



CITY OF WESTWOOD, KANSAS

PLANNING COMMISSION MEETING

4700 RAINBOW BLVD. WESTWOOD, KS 66205

Monday, September 11, 2023 at 7:00 PM

AGENDA

Welcome to your Westwood City Council meeting. This meeting may be attended remotely via Zoom:

Access Online: <https://us02web.zoom.us/j/89009964959>

Access by Phone: (312) 626-6799 / **Webinar ID:** 890 0996 4959

[Note: This agenda is subject to changes, additions, or deletions at the discretion of the Governing Body]

REGULAR MEETING AGENDA

I. CALL TO ORDER

II. APPROVAL OF THE AGENDA AND MEETING MINUTES

A. Consider approval of August 7, 2023 Planning Commission meeting minutes

B. Consider approval of September 11, 2023 Planning Commission meeting agenda

III. PUBLIC HEARINGS

Public Comment Procedure

The Planning Commission Chair has elected to allow a second public comment opportunity on the items on tonight's agenda pursuant to the following instructions and guidelines:

Those wishing to make public comment via Zoom should use Zoom's "Raise Hand" feature and then wait to be called on by the meeting host. Once called on, the meeting host will lower the commenter's hand in Zoom and unmute them, at which time the commenter will be able to address the Planning Commission.

Those wishing to make public comment in person at City Hall should approach the podium in an orderly way upon invitation by the meeting Chair to the public present.

All commenters must first state their name and address for the record.

Commenters are permitted up to five (5) minutes to make a statement.

Commenters should not expect to engage in a dialogue with the Planning Commission and so should not expect responses to any questions posed by commenters during this time.

Commenters will be permitted only one (1) turn to speak.

Comments may also be submitted in writing to City Hall or via the email address provided below to be included in the meeting minutes/official record of the meeting.

- A. RZ-2023-01 Consider application of Karbank Holdings, LLC, on behalf of owner City of Westwood, KS to rezone property at 5000 and 5050 Rainbow Blvd., Westwood, KS 66205 from R-1 (Single-Family Residential) to PD (Planned Development)
- B. RZ-2023-02 Consider application of Karbank Holdings, LLC, on behalf of owner Shawnee Mission School District to rezone property at 2511 W. 50th Street, et al., Westwood, KS 66205 from R-1 (Single-Family Residential) to PD (Planned Development)
- C. PDP-2023-01 Consider application of Karbank Holdings, LLC on behalf of owners Shawnee Mission School District and City of Westwood, KS, jointly, for approval of a preliminary development plan at 2511 W. 50th St., 5000 Rainbow Blvd., and 5050 Rainbow Blvd, Westwood, KS 66205

IV. PRESENTATIONS

V. OLD BUSINESS

VI. NEW BUSINESS

VII. ANNOUNCEMENTS/PLANNING COMMISSIONER COMMENTS

VIII. STAFF REPORTS

- A. City Administrator Report (Leslie Herring)
- B. Public Works Director Report (John Sullivan)
- C. Codes Administrator/Building Official Report (Eddie McNeil)

IX. UPCOMING ITEMS

- A. FDP-2023-01 Consider application of Karbank Holdings, LLC on behalf of owners Shawnee Mission School District and City of Westwood, KS, jointly, for approval of a final development plan at 2511 W. 50th St., 5000 Rainbow Blvd., and 5050 Rainbow Blvd, Westwood, KS 66205
- B. FP-2023-01 - Consider application of Karbank Holdings, LLC on behalf of owner City of Westwood to replat property at 5000 and 5050 Rainbow Blvd.
- C. FP-2023-02 - Consider application of Karbank Holdings, LLC on behalf of owner Shawnee Mission School District to replat property at 2511 W. 50th St., et. al.

X. ADJOURNMENT

UPCOMING MEETINGS

Regular meetings of the Westwood Planning Commission are held at 7:00 PM on the first Monday of each month. The next regular meeting of the Westwood Planning Commission will be held October 2, 2023, at 7:00 PM at Westwood City Hall or virtually, depending on current public health protocols in place. The City Calendar may be accessed at www.westwoodks.org. To receive further updates and communications, please see or sign up for the following:

Westwood Buzz Email: <https://bit.ly/3wA4DWx>

Facebook: [City of Westwood Kansas-Government](#)
[Westwood, KS Police Department](#)

**City of Westwood, Kansas
Planning Commission Meeting
4700 Rainbow Boulevard
August 7, 2023 – 7:00 PM**

Commissioners Present: Kevin Breer, Vice Chair
Clay Fulghum
Ann Holliday
Samantha Kaiser
David Kelman
Mark Neibling
Sarah Page, Chair
M. Scott Weaver

Commissioners Absent: Matt Prout

Staff Present: Leslie Herring, City Administrator
John Sullivan, Public Works Director
Spencer Low, City Attorney Co-Counsel

Call to Order

Chair Page called the meeting to order at 7:00 PM on August 7, 2023.

Approval of Agenda and Meeting Minutes

Chair Page called for modifications or discussion of the August 7, 2023 agenda and July 10, 2023 meeting minutes. Commissioner Breer moved to approve both as presented. Commissioner Weaver seconded. Motion passed unanimously.

Public Hearings

Consider proposed directional parking signs at The Westwood Plaza Towers (primarily occupied by Midwest Transplant Network), a property that is zoned C-O and located at 1900 W 47th Place, Westwood, KS 66205.

Chair Page invited the applicant to address the Planning Commission. The applicant was not present at the meeting.

Chair Page called for City Administrator Herring to present the item. City Administrator Herring presented the staff report included in the meeting packet. Commissioner Neibling asked for clarification from staff as to the sign materials. City Administrator Herring shared that the plans call for an aluminum cabinet.

Chair Page called for public comment on the application. No public comment was received. Chair Page closed the public comment portion of the meeting.

Chair Page called for discussion amongst the Planning Commission. Commissioner Weaver voiced support for this signage package as it helps people get onto, out of, and around the site.

Chair Page called for additional comments or a motion to be made. None heard.

Motion by Commissioner Breer to approve the application of Midwest Transplant Network, Inc. to allow the installation of parking lot directional parking signs as presented at property at 1900 W. 47th Pl., Westwood, KS 66205. Second by Commissioner Kelman. Motion passed unanimously.

RZ-2023-01 Consider application of Karbank Holdings, LLC, on behalf of owner City of Westwood, KS to rezone property at 5000 and 5050 Rainbow Blvd., Westwood, KS 66205 from R-1 (Single-Family Residential) to PD (Planned Development); and

RZ-2023-02 Consider application of Karbank Holdings, LLC, on behalf of owner Shawnee Mission School District to rezone property at 2511 W. 50th Street, et al., Westwood, KS 66205 from R-1 (Single-Family Residential) to PD (Planned Development); and

PDP-2023-01 Consider application of Karbank Holdings, LLC on behalf of owners Shawnee Mission School District and City of Westwood, KS, jointly, for approval of a preliminary development plan at 2511 W. 50th St., 5000 Rainbow Blvd., and 5050 Rainbow Blvd, Westwood, KS 66205.

Chair Page set out the order of activities for the public hearing and presentation on this item for the benefit of the public in attendance and also shared additional opportunities for the public to share comments on the applications.

Chair Page invited the applicant to address the Planning Commission. Steve Karbank, Chairman of Karbank Real Estate Company, addressed the Planning Commission and presented the preliminary development plan and elevations for the proposed development at 50th & Rainbow Blvd.

Chair Page invited questions of the applicant from the Planning Commissioners.

Commissioner Kelman asked about the exterior materials selected for the primary structure, especially related to the louvers shown on the renderings in July and not shown on the current, updated plans. Karbank responded that whether or not to use louvers now that stained terra cotta has been selected as the exterior material has not been finally determined but that they are evaluating still using them.

Chair Page asked about the plan to evaluate and protect as many mature trees as possible. Karbank responded that they are working on a process to do just that. Commissioner Neibling noted that the plans appear to show a grading plan for the City Park site and expressed his concern about the proposed grading up to and around the mature trees on the west of the City Park site. Austin Lage, Engineer for BHC, responded that he will review their work but that the intent is to preserve as many trees as possible.

Commissioner Neibling asked for clarification as to the placement of the proposed underground detention facility.

Chair Page asked why Karbank believes the colors of the exterior materials selected for the primary structures fit within the neighborhood. Karbank shared several photos of single-family houses in Westwood and Westwood Hills that are painted a variety of colors. Commissioner Neibling noted that the building material selected by Karbank has a different texture and sheen than the paint used on single-family houses and may appear and feel different. He further noted that the stained terra cotta does come in neutral tones. Karbank responded that they are open to feedback from the Planning Commission on the type and color of the materials used. Commissioner Kelman stated his preference for

a less bold look and feel of the exterior materials. Karbank responded that they would incorporate feedback to the extent they can. Commissioner Breer recalled that the plan presented by Karbank on July 10th was noted at that time by the applicant to show a placeholder material because the materials had not yet been selected. He noted that he finds the colors of the material to be refreshing and likes that they are a deviation from Johnson County beige. He also acknowledged that building materials will elicit diverging opinions.

Commissioner Breer asked about the status of the stormwater runoff study. Karbank confirmed that a full study would be provided as part of the final development plan submittal.

Commissioner Breer asked whether an evaluation of the impact of the development on water pressure to surrounding homes has been performed. Karbank responded that the conversation has not occurred.

Chair Page asked whether there will be parking for park users at the development. Karbank requested City Administrator Herring respond as to the status of that conversation. Herring responded that the current conversation is for ten (10) dedicated parking spots on the Karbank surface lot just to the south of the South Pavilion building. Karbank elaborated on this point and also that the plans currently show an accommodation for siting park restrooms on the City Park property just to the west of the North Pavilion building. Karbank showed renderings of the development.

Commissioner Kelman asked about the location of the generator. Karbank stated that the generator is a back-up generator and won't be used continually and that it sits below the grade of 50th St. Commissioner Kelman asked whether it would be acoustically treated so as to not disturb neighbors. Karbank responded that the generator would only turn on if and when power fails during storms and that they would do what's need to ensure the sound isn't a nuisance to neighbors.

Commissioner Kelman asked about the traffic impact study. Kevin Pinkowski of BHC explained that additional traffic counts will take place after school resumes and that they are working with City staff to find an agreeable approach to modeling the impact of traffic given that there are temporarily two schools operating here now.

Chair Page asked about Karbank's confidence to lease out the space when vacancies are observed in other nearby offices and retail spaces. Karbank responded that they are seasoned at leasing and that their product will appeal to area residents who would like to relocate or open their office within walking or biking distance of their home. Chair Page asked about what types of retail tenants are anticipated. Karbank responded that high-end retailers are the target audience; smaller boutiques. Commissioner Kelman expressed his concern that retailers that are too boutique might not feel welcoming to Westwood residents. Karbank responded that they will target retailers who are more accessible and provide a casual enough environment for Westwood residents.

Chair Page asked for elaboration on green design features. Karbank responded that they have LEED-certified projects and that they are very familiar with both LEED and other green building standards and that they are planning to incorporate those standards in the project. Commissioner Kelman asked whether a LEED certification level has been identified as a minimum standard for the project. Karbank responded that Certified is the minimum standard.

Commissioner Kaiser asked whether adding residential uses has been evaluated. Karbank responded that their impression is that the surrounding community does not desire multifamily development and that they have not designed this project to provide residential.

Commissioner Neibling asked about the ability of the development to accommodate fire apparatus. Karbank responded that they have designed the project in accordance with the Fire Marshal's requirements.

Commissioner Weaver asked how Karbank believes its project conforms to the City's comprehensive plan (2017 Master Plan). Karbank shared that they feel the comprehensive plan calls for redevelopment on this site and that their proposal advances the comprehensive plan.

Commissioner Neibling asked about ceiling heights and massing. Mike Paxton, Architect at Perspective, responded that their construction type calls for higher ceilings due to their design preferences to construct a single, concrete structure vs. steel. He also responded that the site grade provides for the building height and number of stories to look, feel, and be different at different points on the site; the development will be only two and three stories on certain points on the site.

Commissioner Kelman asked about ventilation within the parking structure and expressed his preference for natural vs. mechanical ventilation. Karbank responded that will depend on the location and width of the easements.

Chair Page invited City Staff to address the Planning Commission. City Administrator Herring provided an overview of the various application processes related to this project and how they relate to one another and how both the Planning Commission and City Council are involved in the process. Herring also read from the 2017 Master Plan identified potential uses for the subject parcels, citing that a low-intensity development, including some retail services to nearby residents and businesses along with residential uses is listed among the possibilities for 5050 Rainbow Blvd. Herring noted that the City Park is not part of this application process and that planning for it will come later but only if this proposal is approved; if this proposal is not approved, park planning and park improvements as being currently discussed and proposed will not take place. Herring continued to highlight aspects of City Staff review of the applications as outlined in the staff report on this item.

Commissioner Weaver asked whether this proposal is the only opportunity for the City park to be developed. Herring stated that this is not the only opportunity for an improved City park but that a substantial park overhaul is not currently included in the City Council's facilities or capital planning; that the Karbank proposal has accelerated discussions of park improvements because it provides a means to do those improvements.

Chair Page called for public comment on the application.

Marsa Swatzell, 4958 Rainbow Blvd., Westwood, addressed the Planning Commission. Swatzell expressed concern that the property values of immediately surrounding owners will be harmed by this development. She expressed her concern for the scale of the building and the exterior materials selected. She also expressed concern about the ability of Karbank to lease the spaces. She stated that she likes the idea of a nearby improved park and walkable retail.

Rod Bell, 2820 W. 50th St., Westwood, addressed the Planning Commission. He expressed his support for the revenue that this proposal would produce for the City to care for critical infrastructure. He believes this project would have a positive impact on the community for generations, including to immediately surrounding property owners.

Tara Chamberlain, 2018 W. 49th Ter., Westwood Hills, addressed the Planning Commission. She is curious to understand how office space produces revenue for a city. She is also curious to know what types of businesses are even in the market for office space, lease rates, and is concerned that the office spaces will be leased as temporary offices.

Karen Johnson, 4950 Adams St., Westwood, addressed the Planning Commission. Johnson does not feel that the proposal conforms to the 2017 Master Plan and that single-family residential homes are the only appropriate use for the site. She is also concerned about the height of the buildings and feels its excessive. Further, she is concerned about the ability of the developer to lease the space and is concerned that potential tenants could have drive-up accommodations, which she opposes. She is also concerned about light spill from the development and about the protection of mature trees.

Ellen Marsee, 4957 Adams St., Westwood, addressed the Planning Commission. She does not feel the proposal conforms to the 2017 Master Plan and cited multiple survey responses from residents received during the Master Plan creation and adoption process. She further shared her concerns about traffic impacts and the ability of the developer to lease office and retail space in the current market. She stated it's her understanding that 50th St. would need to be widened for this project, easements taken by the City for additional sidewalk, and on-street parking.

Nicki Dupont, 1930 W. 50th Ter., Westwood Hills, addressed the Planning Commission. She does not feel that the proposal conforms to the 2017 Master Plan and cited that the scale of the project is not low-intensity and does not respect the neighborhood scale of the surrounding area. She also expressed concern at the potential loss of mature trees resulting from the construction of underground detention, as proposed. She also expressed concern for the ability of the developer to lease the space. She further questions the motive of the City of Westwood to consider this proposal to begin with.

Dennis Dupont, 1930 W. 50th Ter., Westwood Hills, addressed the Planning Commission. He expressed concern about the development and is skeptical about the preliminary traffic counts and analysis performed. He is concerned about traffic generated by the development taking 50th Terrace from State Line Rd. He does not feel the proposal fits with the character of the neighborhood and cited language from the 2017 Master Plan to support his position.

Jessica Peterson, 4831 Belinder Ct., Westwood, addressed the Planning Commission. She encourages the City to require LEED certification for the project.

Jermy Morris, 2340 W. 51st St., Westwood, addressed the Planning Commission. He expressed concern about the development having a negative impact on his property value. He is concerned for the transitional buffers between the development and the residential properties as well as concern about traffic impacts on the residential streets. He likes the prospect of adding walkable retail but doesn't like this proposal.

Ben Hobert, 2208 W. 49th St., Westwood Hills, addressed the Planning Commission. He doesn't feel that the size and scale of this project respects the immediate residential neighbors. He also doesn't feel that

a new park on the site proposed cannot replace the feel of the existing park. He also doesn't believe the traffic impact analysis.

Beth Ciperson, on 46th St. between Rainbow Blvd. and State Line in Kansas City, Kansas, addressed the Planning Commission. She is concerned that the project does not incorporate enough environmentally-friendly design features. She is also concerned about the exterior material and the size of the parking area.

Kent Peterson, 4831 Belinder Ct., Westwood, addressed the Planning Commission. He thanked the City and Planning Commission for their work and efforts to bring this project to the community. He expressed concern if the development isn't LEED certified. He appreciates the revenue this project could bring to the City for infrastructure and other needs and likes the idea of a new and improved City park. He expressed concern that the project is not addressing the harmful legacy of residential racial redlining.

Russ Waitman, 2201 W. 49th St., Westwood Hills, addressed the Planning Commission. He stated he likes Karbank and their existing projects. He is concerned that the development in the proposed location will pose safety issues for children walking to and from elementary school.

Tammy Carter, 2323 W. 51st St., Westwood, addressed the Planning Commission. She expressed concern about losing the existing park and about increased traffic.

Brandon Joiner, 2016 W. 47th Ter., Westwood, addressed the Planning Commission. He expressed support for redevelopment, generally. He expressed a concern that the existing City Park is inadequate and outdated. He also expressed interest in the office use at the site, sharing that he would like to have his office there and he likes the idea of additional walkable retail and restaurants for his family to frequent.

Becky Beilharz, 2903 W. 51st Ter., Westwood, addressed the Planning Commission. She asked for clarification on what financial incentives the developer is requesting. She also expressed an interest in the development being at least LEED Silver certified.

Laura Bowell, 2301 W. 51st St., Westwood, addressed the Planning Commission. She expressed her concern about the scale of the development and the location of the vehicular access to the site. She also expressed concern about the size of the proposed new park not being adequate.

Steve Platt, 4910 Glendale Rd., Westwood Hills, addressed the Planning Commission. He asked why Johnson County Parks & Recreation District is not a part of this application so that they could provide a benefit to this part of the County for the tax dollars generated by area residents. He also expressed concern about the height and scale of the buildings and questions whether the City really needs this development.

Sara Keehn, 4957 Booth St., Westwood, addressed the Planning Commission. She expressed her belief that this proposal doesn't conform to the 2017 Master Plan and that commercial belongs at 47th & Rainbow Blvd., not at this site. She also expressed concern about traffic impacts on the residential streets. She further expressed concern about the architectural style and scale of the structure.

Peter Pantz, Woodside North Apartments, Westwood, addressed the Planning Commission. He noted that City officials have done a good job so far managing and reviewing this project. He expressed

support for the project and doesn't believe that it will harm the community and will only make it better and Westwood a more attractive place to live.

Chair Page asked for any additional commenters to come forward; seeing none, Page closed the public comment portion of the meeting.

Chair Page invited the applicant to address the Planning Commission again to share responses to any questions posed during the public comment portion of the hearing. Karbank stated that they are not seeking any financial tax incentives for the project and that they plan to be a tax payer. Karbank stated that retail tenants with drive-thrus will not be included in the development and that the traffic impacts are being studied but that traffic issues are not anticipated to be an issue. Karbank further shared that they take environmental design seriously and plan to incorporate those elements. Further, he shared that there are multiple examples of retail and commercial uses near elementary schools.

Chair Page invited City Administrator Herring to address the Planning Commission again to share responses to any questions posed during the public comment portion of the hearing. Herring clarified that the City has no intent to widen any streets, take any easements, nor is on-street parking part of the project as proposed. Herring also stated that previous conversation between the City and Johnson County Parks & Recreation District/Department have resulted in JCPRD sharing their position that properties of this size are not of interest to them to incorporate into their park portfolio.

Chair Page invited discussion amongst the Planning Commissioners. Commissioner Kelman asked for clarification on whether a non-profit group (including City Hall) could be a tenant in the development given the City's current negotiation of the Development Agreement. City Attorney Ryan Denk responded that the financial analysis for the project is currently only contemplating users who pay property taxes and that the City prefers tax-paying users since so much of the land in Westwood is already occupied by tax-exempt entities and that Karbank has so far expressed interest in retaining ownership of the development and not selling off any part of it to individual tenants.

Commissioner Breer stated that he, as an individual Planning Commissioner, is not considering the potential revenue generated as a factor in his evaluation of the application.

Commissioner Weaver observed that Entercom was, for years, located at the corner of 50th St. and Belinder and was a commercial use in the middle of a residential area.

Chair Page stated that the public hearing will remain open and will be back on the agenda on September 11th and that another opportunity for public comment will be afforded at that time.

Presentations

None.

Old Business

None.

New Business

FP-2023-01 - Consider application of Karbank Holdings, LLC on behalf of owner City of Westwood to replat property at 5000 and 5050 Rainbow Blvd.; and

FP-2023-02 - Consider application of Karbank Holdings, LLC on behalf of owner Shawnee Mission School District to replat property at 2511 W. 50th St., et. al.

Chair Page asked that City Administrator Herring provide the staff report. City Administrator Herring presented the report included in the meeting packet. Herring stated that these applications are on the agenda just to track with the applications for rezoning and the preliminary development plan being heard tonight. Herring stated that there is no action recommended on the plat applications until and only if the Planning Commission approves the rezoning and preliminary development plan for this project.

Conduct annual review of Capital Improvement Plan (2024 – 2028)

Chair Page asked that City Administrator Herring provide the staff report. City Administrator Herring presented the report included in the meeting packet.

Review item only. No action taken.

Announcements/Planning Commissioner Comments

Commissioner Neibling requested that staff publish with the agenda rules of procedure for the upcoming public hearings. City Administrator Herring confirmed she would work with Chair Page to draft those rules and would communicate them ahead of the public hearings.

Staff Reports

City Administrator Herring provided an update on the following items:

- Rainbow Blvd. Planning Sustainable Places Planning Status

Public Works Director John Sullivan provided an update on the following items:

- 47th Street Complete Street implementation project

Upcoming Items

- FDP-2023-01 Consider application of Karbank Holdings, LLC on behalf of owners Shawnee Mission School District and City of Westwood, KS, jointly, for approval of a final development plan at 2511 W. 50th St., 5000 Rainbow Blvd., and 5050 Rainbow Blvd, Westwood, KS 66205

Adjournment

Motion by Commissioner Kelman to adjourn the meeting. Second by Commissioner Weaver. Motion passed unanimously. The meeting adjourned at 10:03 PM.

APPROVED: _____
Sarah Page, Chair

ATTEST: _____
Leslie Herring, Secretary

WESTWOOD PLANNING COMMISSION

Staff Report

Meeting Date: September 11, 2023

Staff Contact: Leslie Herring, City Administrator

RZ-2023-01 – Consider application of Karbank Holdings, LLC, on behalf of owner City of Westwood, KS to rezone property at 5000 and 5050 Rainbow Blvd., Westwood, KS 66205 from R-1 (Single-Family Residential) to PD (Planned Development)

RZ-2023-02 – Consider application of Karbank Holdings, LLC, on behalf of owner Shawnee Mission School District to rezone property at 2511 W. 50th St., et al., Westwood, KS 66205 from R-1 (Single-Family Residential) to PD (Planned Development)

PDP-2023-01 – Consider application of Karbank Holdings, LLC on behalf of owners Shawnee Mission School District and City of Westwood, KS, jointly, for approval of a preliminary development plan at 2511 W. 50th St., 5000 Rainbow Blvd., and 5050 Rainbow Blvd, Westwood, KS 66205

OWNER OF RECORD:

- 5000 Rainbow Blvd.: City of Westwood, Kansas
- 5050 Rainbow Blvd.: City of Westwood, Kansas
- 2511 W. 50th St.: Unified School District No. 512 (Shawnee Mission School District)

APPLICANT: Karbank Holdings, LLC, agent for property owners City of Westwood, Kansas and Unified School District No. 512, jointly.

LOCATION: The property is located on the southwest corner of W. 50th St. and Rainbow Blvd.

EXISTING ZONING: The property is currently zoned R-1 (D): single-family residential.

PROPOSED PROJECT: Build a mixed-use office and retail development (an adjoining future City Park to the west of the subject site but located on the western portion of 2511 W. 50th St. outside of the rezoning subject site).

BACKGROUND: The applicant is requesting approval to rezone a portion of the subject site and for approval of a preliminary development plan to build a mixed-use office and retail development on property currently under contract at 5000 Rainbow Blvd., 5050 Rainbow Blvd., and 2511 W. 50th St.

Pursuant to Page Nos. 4-15 – 4-18 of the 2017 Westwood Master Plan, the following have been identified as potential uses for the subject site:

- 5050 Rainbow Blvd. – a low-intensity development, including some retail services to nearby residents and businesses along with residential uses.
- 2511 W. 50th St. (former) Westwood View Elementary – park and open green spaces; civic uses

As such, the proposed use for the subject site has been contemplated by the Westwood Master Plan, which serves as the City's Comprehensive Plan for guiding land use within the City of Westwood.

Although a park is a permitted use by right within the R-1 zone, a mixed-use commercial use is not permitted within the R-1 zone and requires a rezoning to accommodate the proposed development. The applicant desires to rezone to PD – Planned Development, Westwood Zoning Ordinance 1.6.24 requires that the preliminary development plan shall be considered and approved as part of the rezoning application.

As such, this application is subject to the conditions and criteria for:

- Rezoning approval, provided for in Section 1.6.1-18 of the Westwood Zoning Ordinance; and
- Preliminary development (site) plan approval provided for in Section 1.6.20-24 of the Westwood Zoning Ordinance.

The rezoning and preliminary development plan are subject to public hearing before the Planning Commission, at the conclusion of which the Planning Commission is to make a recommendation to the Governing Body, which body then takes the ultimate action on the applications¹.

The Planning Commission opened the public hearing on these items on August 7, 2023 and, at that time, continued the public hearing to today's agenda to allow Karbank an opportunity to integrate feedback from the public hearing into its preliminary development plan.

City Park Not Included in this Application

The proposed City Park is not included in this application, as its existence in the location proposed on the west side of the subject site is wholly dependent on the outcome of the applications filed by Karbank for its proposal on the east side of the subject site. The park planning process – including final grading and features/amenities – will take place only if and when approvals for the Karbank proposal have been obtained, lot lines determined and platted², and the successful sale and purchase of the various property parcels within the subject site has closed³.

Application Subject to Final Development Plan Approval

This application is subject to both preliminary and a final development plan approval. The preliminary development plan is the subject of the current meeting, with the final development plan submittal and review process to take place shortly hereafter. Unlike the preliminary development plan, which must in this case run concurrently with the rezoning application and which are subject to public hearing, the final development plan is not subject to public hearing so long as it is not substantially changed from the preliminary development plan and so long as the Planning Commission finds that it satisfies the requirements for site planning, landscaping, and other technical studies.

The purpose of the final development plan is to provide for more detailed technical plans that cannot be finalized until the site layout and general elements of the site are agreed upon by the parties (i.e. the

¹ Pursuant to Westwood Zoning Ordinance 1.6.13.

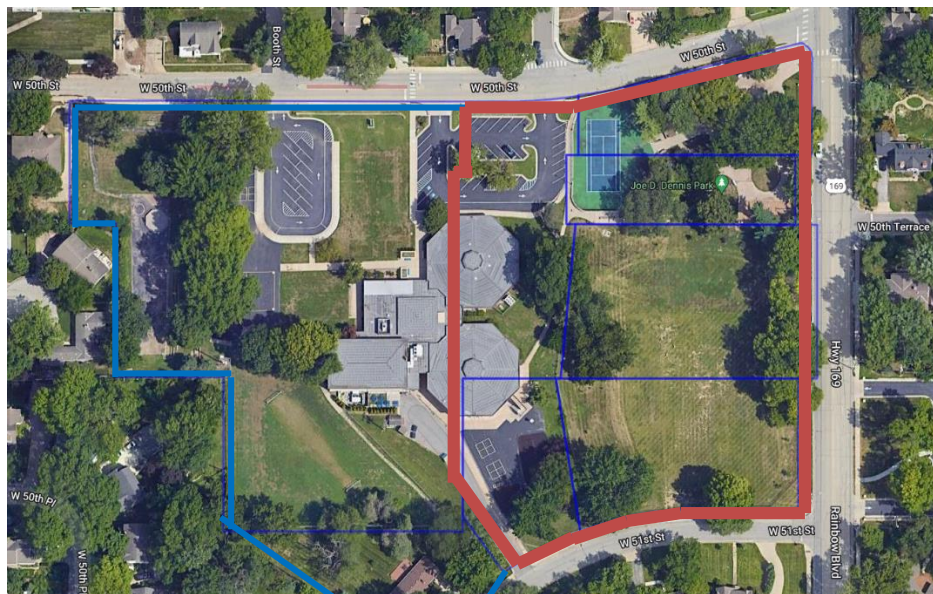
² Subject to separate but related process being run commensurate with the rezoning and preliminary development plan applications.

³ Pursuant to Purchase Agreements executed jointly by and between USD 512, the City of Westwood, and Karbank Holdings, LLC dated June 8, 2023.

City via Staff and the Planning Commission/Governing Body and the applicant). The final development plan is anticipated to be considered by the Planning Commission at the October 2, 2023 regular meeting.

EXISTING CONDITIONS: This application is to rezone 4.124 acres of the total 7.624 acres of the combined project area. The red outline in the illustration below indicates the area subject to the rezoning request. The area to the west (outlined in blue) indicates the area of the proposed City Park, which is to remain zoned R-1 and which would be subject to a later planning process in the event this rezoning application and preliminary development plan is approved and the sale and purchase of the various properties closes.

The area subject to this rezoning request (outlined in red) currently contains: a portion of an elementary school building and parking lot(s) (to be decommissioned and sold by USD 512 in 2024, regardless of the outcome of these joint applications), a City tennis court, a City playground, and a vacant parcel.



REQUESTED ACTION:

The applicant is requesting approval of a rezoning and a preliminary development plan to construct a mixed-use office and retail development.

REVIEW CRITERIA:

City staff – with the assistance of outside professionals retained by staff – reviewed the submitted applications pursuant to the following articles of the Westwood Zoning Ordinance, which solely govern this submittal:

- Article 1: Purpose, Responsibilities, and Procedures
- Article 2: Definitions
- Article 5: Commercial Zoning Districts
- Article 6: Planned Zoning Districts
- Article 9: Signs

The submittal is also reviewed to ensure compliance with the 2018 ICC code edition and APWA 5600, as amended.

APPROVAL CRITERIA:

Rezoning

Pursuant to Section 1.6.17 of the Westwood Zoning Ordinance:

In considering any application for rezoning request, the Planning Commission and the Governing Body may give consideration to the criteria stated below, to the extent they are pertinent to the particular application. In addition, the Planning Commission and Governing Body may consider other factors which may be relevant to a particular application⁴.

- A. The conformance of the proposed use to the City's Comprehensive Plan and other adopted planning policies.*
- B. The character of the neighborhood including, but not limited to: land use, zoning, density (residential), architectural style, building materials, height, structural mass, siting, open space, and floor-to-area ratio (commercial and industrial).*
- C. The zonings and uses of nearby properties, and the extent to which the proposed use would be in harmony with such zonings and uses.*
- D. The suitability of the property for the uses to which it has been restricted under the applicable zoning district regulations.*
- E. The length of time the property has remained vacant as zoned.*
- F. The extent to which approval of the application would detrimentally affect nearby properties.*
- G. The extent to which the proposed use would substantially harm the value of nearby properties.*
- H. The extent to which the proposed use would adversely affect the capacity or safety of that portion of the road network influenced by the use, or present parking problems in the vicinity of the property.*
- I. The extent to which the proposed use would create excessive air pollution, water pollution, noise pollution, or other environmental harm.*
- J. The economic impact of the proposed use on the community.*
- K. The gain, if any, to the public health, safety, and welfare due to denial of the application as compared to the hardship imposed upon the landowner, if any, as a result of denial of the application.*
- L. The recommendation of professional staff.*

Preliminary Development Plan

Per Section 1.6.20 of the Westwood Zoning Ordinance:

The purpose and intent of requiring site plan approval is to encourage the compatible arrangement of buildings, off-street parking, lighting, landscaping, pedestrian walkways and sidewalks, ingress and egress, and drainage on the site and from the site, any or all of these, in a

manner that will promote safety and convenience for the public and will preserve property values of surrounding properties.

Section 1.6.21 of the Westwood Zoning Ordinance provides the conditions and criteria for site plan approval.

The Planning Commission shall review the site plan to determine if it demonstrates a satisfactory quality of design in the individual buildings and in its site, the appropriateness of the building or buildings to the intended use, and the aesthetic integration of the development into its surroundings. Satisfactory design quality and harmony will involve among other things:

- A. The site is capable of accommodating the building(s), parking areas and drives with appropriate open space.*
- B. The plan is consistent with good land planning, good site engineering design principles, and good landscape architectural principles.*
- C. An appropriate use of quality materials and harmony and proportion of the overall design.*
- D. The architectural style should be appropriate for the project in question and compatible with the overall character of the neighborhood.*
- E. The siting of the structure on the property, as compared to the siting of other structures in the immediate neighborhood.*
- F. The bulk, height and color of the proposed structure as compared to the bulk, height and color of other structures in the immediate neighborhood.*
- G. Landscaping to City standards shall be required on the site and shall be in keeping with the character or design of the site.*
- H. Ingress, egress, internal traffic circulation, off-street parking facilities and pedestrian ways shall be so designed as to promote safety and convenience, and shall conform to City standards.*
- I. The plan represents an overall development pattern that is consistent with the City's Comprehensive Plan, the official street map, and other adopted planning policies.*

STAFF ANALYSIS: Staff review resulted in identification of the following more noteworthy elements of the preliminary development plan that remain unresolved through the administrative review process:

Setbacks

Sections 5.3.1 and 5.3.7 of the Westwood Zoning Ordinance provide for, among other criteria, the following:

- Consistent with the City's Comprehensive Plan, the C-1 Commercial Mixed-Use District is intended to provide redevelopment or investment opportunities for existing or planned commercial centers within the pg. 67 City. Developments in Commercial Mixed-Use districts are to follow the characteristics of traditional "Main Street" commercial neighborhood developments, and to encourage pedestrian use through connections to adjacent neighborhoods and the construction of mixed-use buildings. The purpose of this District is to allow for the development of fully integrated, mixed-used, pedestrian-oriented neighborhoods. (5.3.1.A)

- The streets and sidewalks [to be] the main pedestrian activity centers in Commercial Mixed-Use Districts. Minimal setbacks bring buildings close to the street and the pedestrians. (5.3.1.C)
- Unless otherwise indicated in a specific Overlay District, the facade of buildings in the C-1 District shall be constructed directly on the build-to line along at least seventy percent of the length of the building. (5.3.7.A)
- Parking areas and parking garages shall be recessed or placed to the rear of buildings. (5.3.7.C)
- Larger setbacks may be permitted for street-side outdoor cafes and patios. (5.3.7.D)
- Buildings on such C-1 District lots shall have no setback from at least one side lot line. (5.3.7.E)
- The required setbacks shall be... as follows:
 - 1. front and side setbacks: Ø feet
 - 2. rear setback: 20 feet minimum (5.3.7.H)

As the requested rezoning is to PD – Planned Development, deviation and flexibility from the strict criteria of the underlying zoning district (in this case C-1) is anticipated and permitted. A deviation from the setbacks required by the C-1 zoning district is requested by the applicant. The compatibility/applicability of these criteria and the setbacks proposed by the applicant should be weighed by the Planning Commission in consideration of the uses of neighboring properties and the larger criteria for reviewing rezoning requests and preliminary development (site) plans, as set forth in the Approval Criteria set forth above.

City staff does not object to the setbacks as proposed as allowing larger setbacks on this site allows for more dense tree canopy planting, which enhances the site while also serving as a transitional buffer to neighboring residential properties.

Trees

It is the stated intent of the applicant that “the majority of existing trees will be preserved at the perimeter of the site to create a large multistory landscape buffer from surrounding neighbors and preserve the existing quality of the streetscape.” However, administrative review concludes that the proposed underground stormwater management system is too close to the trees lining Rainbow and will not allow for installation of the underground detention or, if detention is installed as proposed, mature trees will likely be lost due to severe cutting of the root system and will have a profound affect for them to remain upright.

Additionally, City staff recognizes the value of a dense tree canopy and also the value of redevelopment and of proper stormwater management. As such, City staff recommends that certain conditions for approval of the application be met, which are identified as follows:

- a. Karbank to perform at its sole expense a study prior to any demolition permits being issued, such study to include the following scope of work:
 1. provide an inventory of all existing trees, identifying by location each tree’s:
 - i. specie type;
 - ii. estimated age;

- iii. condition (and to the extent possible an estimate on remaining lifespan);
 - iv. ability to be relocated elsewhere on the development site or at the City Park (with emphasis on relocating as many as possible on the development site); and
 - v. if proposed to be kept, a tree preservation and protection plan for use during the demolition and construction period; and
- b. Karbank to mitigate the removal of mature trees from 5000 and 5050 Rainbow Blvd. by providing one (1) new tree for every tree less than 12" caliper removed and for trees over 12" caliper, replace at 2:1. Preference for new tree plantings is for them to be located in the north and south side yard of the of the development; however, exact location will be determined at a later date following the conclusion of the study defined above.

Traffic Impact

Although a preliminary Traffic Memo has been provided as part of the submittal, a full Traffic Impact Study (TIS) is anticipated in the coming weeks. There are several outstanding items to be resolved to ensure that the applicant is appropriately forecasting and evaluating the traffic impacts of the proposed mixed-use development in relation to existing conditions. Further, City staff has requested and is awaiting from the applicant documentation of approval from the Kansas Department of Transportation (KDOT) as to the applicant's proposed new access point onto Highway 169/Rainbow Blvd., aligned with 50th Terrace. Further still, City staff has directed the applicant to conduct traffic counts around the site once all surrounding schools have commenced their Fall 2023 sessions (on or after August 24, 2023); these traffic counts were conducted the week of September 5th, with counts and analysis expected in an addendum to the Traffic Impact Study to be provided with the final development plan submittal.

Given the status of evaluation of the traffic impact related to the proposed development, there are still two items outstanding, which will need to be provided and resolved during the final development plan review and prior to its approval. Those items are:

- a. A conclusion to the Traffic Impact Study by way of including as an addendum to it, the in-school dates of traffic counts, and an assessment and review of those counts; and
- b. A conclusion to KDOT's review of the access of the development directly onto Rainbow Blvd.

It is worth noting that although there is a current regional study of Rainbow Blvd./Highway 169/7th St. taking place, that process is unrelated to this application; however, the two processes will be connected where possible. The Mid-America Regional Council (MARC) Planning Sustainable Places (PSP) Rainbow Blvd./7th Street/Hwy. 169 Complete Streets Traffic Management Plan study project was contemplated by the City of Westwood following the Urban Land Institute's (ULI) 2021 Technical Assistance Panel (TAP) and was initiated prior to the Karbank proposal being received by the City.

Notably, one of the main stated objectives of the MARC PSP work is to:

Integrate into the design recommendations [for Rainbow Blvd.] strategies and solutions that reduce – or at a minimum don't contribute to or cause higher levels of – non-resident vehicular traffic on adjacent residential streets.

Although this MARC PSP hopes to produce a recommendation that would result in a more pedestrian and transit-oriented Rainbow Blvd. from Shawnee Mission Pkwy. to I-35, the timelines for the projects will not result in integration of the MARC PSP findings into this particular application process.

Nonetheless, the conversations and results from this application process will be integrated into the MARC PSP process.

Stormwater Management

The stormwater management plans are, in theory and method, acceptable. Additional detail and information (including the MARC level of service calculation worksheets showing LOS required and proposed) will be reviewed in depth once it is provided prior to approval of a Final Development Plan.

Signage Guidelines

City staff sees no issues or areas of noncompliance within the submitted Comprehensive Signage Guidelines. As no tenants have yet been identified, exact signs cannot be contemplated nor reviewed by City staff. As tenants are named and signage is designed for their use, it must conform with the Comprehensive Signage Guidelines as approved during this process. Any deviations would come before the Planning Commission for review and approval.

Rezoning

City staff analysis of this application related to the *REZONING* is as follows:

Pursuant to Section 1.6.17 of the Westwood Zoning Ordinance:

In considering any application for rezoning request, the Planning Commission and the Governing Body may give consideration to the criteria stated below, to the extent they are pertinent to the particular application. In addition, the Planning Commission and Governing Body may consider other factors which may be relevant to a particular application⁵.

- A. The conformance of the proposed use to the City's Comprehensive Plan and other adopted planning policies.*

The 2017 Master Plan provides for low-intensity development with retail uses as a potential use for the site at 5050 Rainbow Blvd. The Master Plan defines a term low-scale within this context as development generally no more than two stories. The proposed buildings are one, two, and three stories looking east from the future City Park; the buildings are three and four stories looking from Rainbow Blvd. west; and are three and one story buildings looking from the north and south of the site.

City staff finds that the proposed project meets the spirit of the 2017 Master Plan for proposed use for this site as the project.

- B. The character of the neighborhood including, but not limited to: land use, zoning, density (residential), architectural style, building materials, height, structural mass, siting, open space, and floor-to-area ratio (commercial and industrial).*

City staff finds that Rainbow Blvd. is a mixed-use corridor with a greater amount of commercial than single-family residential lot frontage from Shawnee Mission Pkwy. to

County Line Rd. at 47th Street.; there are only three (3) residential properties between the subject site and KU Cancer Center on Shawnee Mission Pkwy. Given this proximity to existing commercial/office land uses and the potential use of the specific site for commercial purposes in the 2017 Master Plan, City staff find that this land use and zoning category fit within the Rainbow Blvd. neighborhood context. The architectural style, height, massing, and siting are not out-of-line with the existing KU Cancer Center offices, located just 500 feet to the south of the project site.

- C. *The zonings and uses of nearby properties, and the extent to which the proposed use would be in harmony with such zonings and uses.*

As stated above, City staff finds that the zoning and use of the proposed project is in harmony with uses of nearby properties along Rainbow Blvd., including KU Cancer Center just 500 feet to the south of the project site.

- D. *The suitability of the property for the uses to which it has been restricted under the applicable zoning district regulations.*

City Staff does not find that the property is ill-suited to R-1 residential uses; however, the location of Joe Dennis Park directly on Rainbow Blvd. – a State Highway – is not ideal given the opportunity to relocate the park nearer Westwood View Elementary School and with a structural buffer between it and Rainbow Blvd.

- E. *The length of time the property has remained vacant as zoned.*

5050 Rainbow Blvd., specifically, has been vacant for 10 years.

- F. *The extent to which approval of the application would detrimentally affect nearby properties.*

The 2017 Master Plan, 2021 ULI (Urban Land Institute) TAP (Technical Assistance Panel), and the 2022 City Facilities Assessment and Feasibility Analysis resulted in direct community input that walkable retail and restaurants are a high priority for residents. Residents also expressed interest in having a more walkable community, generally, and the addition of office space would provide more opportunities for residents to work within walking distance of their homes. As such, City staff finds that this application would not detrimentally affect nearby properties.

- G. *The extent to which the proposed use would substantially harm the value of nearby properties.*

City staff cannot estimate the extent to which the proposed use would harm the value of nearby property; however, anecdotally, several examples of single-family residential homes next or close to commercial development exist in our immediate community. These include but are not limited to:

- The Fairway Shops and KU Cancer Center, which back up to houses on W. 51st Ter.

- Commercial and industrial development along 47th St., which backs up to houses on 47th Terrace from Mission Rd. to Belinder Ct.
- Woodside Club and State Line 47 office complex on 47th Pl., which backs up to houses on 47th Ter. between Rainbow & State Line

County property value data indicates that the owners of single-family residential homes on these streets close to commercial development have seen the same – if not higher – average increases over the past 10 years in value than homes in Westwood not immediately abutting commercial uses. In fact, the homes on 47th Ter. between Rainbow and State Line (backing up to Woodside and the State Line 47 office complex) have averaged 19.5% higher property value growth compared to the average of the rest of the City as a whole over the past 10 years. Great parks, good schools, walkable retail and restaurants, and good transportation options are all factors positively impacting property values. Homes immediately surrounding this project are likely to see positive impacts to their value from all of those contributing factors.

- H. The extent to which the proposed use would adversely affect the capacity or safety of that portion of the road network influenced by the use, or present parking problems in the vicinity of the property.*

City staff continues to work with the Karbank team to ensure comprehensive traffic evaluation and analysis and recommends a condition of approval of the final development plan be evidence through the Traffic Impact Study that capacity and safety in the vicinity of the development will not be adversely affected by the development. Further, City staff finds that the on-site parking is adequate to accommodate the proposed mix of uses for the site.

- I. The extent to which the proposed use would create excessive air pollution, water pollution, noise pollution, or other environmental harm.*

City staff has not evaluated these impacts directly nor has an environmental impact study been required for this project; however, proper stormwater management is required to be provided on-site and the photometrics plan provided provides for adequate light containment on-site. Noise pollution from mechanical equipment will be evaluated during the construction design phase prior to the issuance of building permits for the project.

- J. The economic impact of the proposed use on the community.*

Karbank is supportive of the City creating a TIF District covering both its development and the proposed City Park to the west of the development. Karbank is requesting none of the TIF proceeds – incremental value created by the construction and improvements made by Karbank – for its own use. The TIF proceeds would flow directly and entirely to the City for the 20-year period the TIF Project (kicked off by Karbank’s developing their project) and would primarily be used to plan and build the new City Park on the approximately four acres to the west of the mixed-use development. Additionally, all Karbank’s tenants will pay property taxes and sales taxes will be assessed by its retail tenants, which all provides revenue to the City. Further, the economic impact to surrounding property owners is likely to be positive (for reasons cited above in G) and in

that this proposal provides for a larger and more modern park, which absolutely creates positive economic impact for the community.

- K. *The gain, if any, to the public health, safety, and welfare due to denial of the application as compared to the hardship imposed upon the landowner, if any, as a result of denial of the application.*

City staff finds no gains to the public health, safety, and welfare should the application be denied. As the City of Westwood is the landowner in this matter, denial of the application would create a hardship upon it in that it cannot feasibly improve the park amenities with current revenue levels and competing City infrastructure needs. Further, a hardship would be imposed on the City as such denial would likely signal to other interested potential partners that redevelopment of the sight is unlikely to be politically feasible.

- L. *The recommendation of professional staff.*

City staff's recommendation is provided below.

Site (Development) Plan

City staff analysis of this application related to the *SITE (DEVELOPMENT) PLAN* is as follows:

Section 1.6.21 of the Westwood Zoning Ordinance provides the conditions and criteria for site plan approval.

The Planning Commission shall review the site plan to determine if it demonstrates a satisfactory quality of design in the individual buildings and in its site, the appropriateness of the building or buildings to the intended use, and the aesthetic integration of the development into its surroundings. Satisfactory design quality and harmony will involve among other things:

- A. *The site is capable of accommodating the building(s), parking areas and drives with appropriate open space.*

City staff finds this criterion to be met by the current preliminary development plan.

- B. *The plan is consistent with good land planning, good site engineering design principles, and good landscape architectural principles.*

City staff finds this criterion to be met by the current preliminary development plan.

- C. *An appropriate use of quality materials and harmony and proportion of the overall design.*

City staff finds this criterion to be met by the current preliminary development plan.

- D. *The architectural style should be appropriate for the project in question and compatible with the overall character of the neighborhood.*

City staff finds this criterion to be met by the current preliminary development plan as relates the overall character of the Rainbow Blvd. mixed-use corridor neighborhood.

- E. The siting of the structure on the property, as compared to the siting of other structures in the immediate neighborhood.*

City staff finds this criterion to be met by the current preliminary development plan as the site layout calls for enlarged front and side yard setbacks to complement the front yard setbacks of typically single-family homes in the vicinity.

- F. The bulk, height and color of the proposed structure as compared to the bulk, height and color of other structures in the immediate neighborhood.*

City staff finds this criterion to be met – specifically as relates to bulk and height – by the current preliminary development plan as relates the overall character of the Rainbow Blvd. mixed-use corridor neighborhood.

- G. Landscaping to City standards shall be required on the site and shall be in keeping with the character or design of the site.*

City staff finds this criterion to be met by the current preliminary development plan.

- H. Ingress, egress, internal traffic circulation, off-street parking facilities and pedestrian ways shall be so designed as to promote safety and convenience, and shall conform to City standards.*

City staff continues to work with the Karbank team and KDOT to ensure ingress, egress, and pedestrian ways promote safety and convenience. City staff finds the criteria of internal traffic circulation and off-street parking facilities to be met by the current preliminary development plan.

- I. The plan represents an overall development pattern that is consistent with the City's Comprehensive Plan, the official street map, and other adopted planning policies.*

City staff finds this criterion to be met by the current preliminary development plan.

STAFF RECOMMENDATION: Staff recommends that the Planning Commission conditionally approve all three of the applications under concurrent consideration, identified as follows:

RZ-2023-01 – Consider application of Karbank Holdings, LLC, on behalf of owner City of Westwood, KS to rezone property at 5000 and 5050 Rainbow Blvd., Westwood, KS 66205 from R-1 (Single-Family Residential) to PD (Planned Development)

RZ-2023-02 – Consider application of Karbank Holdings, LLC, on behalf of owner Shawnee Mission School District to rezone property at 2511 W. 50th St., et al., Westwood, KS 66205 from R-1 (Single-Family Residential) to PD (Planned Development)

PDP-2023-01 – Consider application of Karbank Holdings, LLC on behalf of owners Shawnee Mission School District and City of Westwood, KS, jointly, for approval of a preliminary development plan at 2511 W. 50th St., 5000 Rainbow Blvd., and 5050 Rainbow Blvd, Westwood, KS 66205

City staff recommends the following conditions of approval:

- a. Karbank to perform at its sole expense a study prior to any demolition permits being issued, such study to include the following scope of work:
 1. provide an inventory of all existing trees, identifying by location each tree's:
 - i. specie type;
 - ii. estimated age;
 - iii. condition (and to the extent possible an estimate on remaining lifespan);
 - iv. ability to be relocated elsewhere on the development site or at the City Park (with emphasis on relocating as many as possible on the development site); and
 - v. if proposed to be kept, a tree preservation and protection plan for use during the demolition and construction period;
- b. Karbank to mitigate the removal of mature trees from 5000 and 5050 Rainbow Blvd. by providing one (1) new tree for every tree less than 12" caliper removed and for trees over 12" caliper, replace at 2:1. Preference for new tree plantings is for them to be located in the north and south side yard of the of the development; however, exact location will be determined at a later date following the conclusion of the study defined above;
- c. Karbank to provide as part of the final development plan consideration sufficient and acceptable in-school traffic counts and traffic modeling and any necessary resulting modifications to the site access to ensure levels of service do not worsen as a result of the development; and
- d. Karbank, prior to final development plan approval, conclude with KDOT its review of the site access as applicable to KDOT and its jurisdiction on Rainbow Blvd.

Suggested Motion:

I move to recommend to the City Council approval of the following applications:

RZ-2023-01 – Application of Karbank Holdings, LLC, on behalf of owner City of Westwood, KS to rezone property at 5000 and 5050 Rainbow Blvd., Westwood, KS 66205 from R-1 (Single-Family Residential) to PD (Planned Development);

RZ-2023-02 – Application of Karbank Holdings, LLC, on behalf of owner Shawnee Mission School District to rezone property at 2511 W. 50th St., et al., Westwood, KS 66205 from R-1 (Single-Family Residential) to PD (Planned Development); and

PDP-2023-01 – Application of Karbank Holdings, LLC on behalf of owners Shawnee Mission School District and City of Westwood, KS, jointly, for approval of a preliminary development plan at 2511 W. 50th St., 5000 Rainbow Blvd., and 5050 Rainbow Blvd, Westwood, KS 66205; with the following conditions:

(Wording of conditions recommended by City staff provided above) Planning Commission is welcome to recommend additional or alternate conditions.

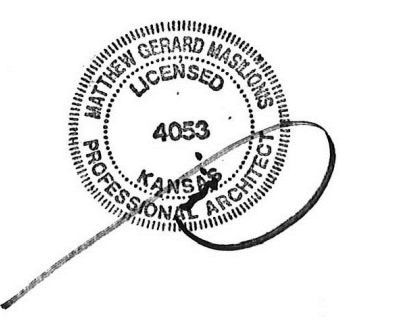
50TH & RAINBOW DEVELOPMENT

W 50TH STREET & RAINBOW BLVD
WESTWOOD, KANSAS 66205

Perspective

ND1930 + 36N1C31HC9V

2000 SHAWNEE MISSION PKWY
SUITE 100
MISSION WOODS, KS 66205
816 502 1500
WWW.PAD.STUDIO



MATTHEW MASILIONIS - ARCHITECT
KANSAS # 4053

I have prepared the drawings and assume responsibility for the sheets numbered with an "A" prefix for the project named below.

Other drawings and specifications attached for the above-mentioned project have been by and are the responsibility of the licensed engineer whose stamp and firm appear on that sheet.

The Architect is not responsible for the design of the mechanical, electrical, plumbing, civil, landscaping, structural, signage (not specified), fire sprinkler or fire suppression systems, and does not take responsibility for the compliance of these areas with the laws of the above governmental entities. The architect is not responsible for materials, components or equipment, as well as the method in which they are installed on the project by others. The architect is not liable or responsible for verification, during construction or upon completion of construction. The architect is not responsible for improper operation due to faulty installation or product failure during construction or after completion of construction when operation has begun by the landlord or tenant.

The licensed professional whose stamp appears on sheets other than those specifically noted above shall be responsible for those items in paragraph three.



MAIN BUILDING PERSPECTIVE RENDERING L19

The 50th and Rainbow Development is a proposed Planned Development District (PD) located on the west side of Rainbow Boulevard between 50th Street and 51st Street in the City of Westwood, Kansas. The proposed project will feature approximately 19,498 leasable square feet of retail space on the 1st floor of the building and approximately 78,351 leasable square feet of office space on 3 floors above the retail. Adjacent to the main building is a smaller 8,750 leasable square foot single story office / retail building broken into two masses.

Because of the mixed-use nature of the proposed project, it is anticipated that portions of the first and second floors of the buildings may be a mix of retail and office uses in lieu of being strictly office or strictly retail. An underground parking structure is located to the west and north of the main building with 167 covered parking spaces. 123 surface parking spaces are also provided on the east and west sides of the main building.

With regard to the 2017 Westwood Comprehensive Use Plan, the land use map in Section 3.3 identifies the proposed development location as Public / Semi-Public and Open Space. This usage type per Figure 3.2, accounts for only 4.4% and 0.8% of the overall city land use. Given the small percentage of space allocated to these uses, it seems vital to the community to maintain these land use types. The proposed development would offer a mixture of Public and Semi-Public spaces with office and retail buildings, and maintain the Open Space components with the proposed City Park. Alternatively, if the plan reverted to the R-1 Single Family Residence Zoning of the surrounding neighborhood, the location would become private space which would seem in conflict with the Comprehensive Use Plan.

The specific stated desired outcomes of the 5050 Rainbow Site, and the Westwood View Elementary site per the Westwood Comprehensive Use Plan are to:

- Protect and minimize impacts to adjacent residential property.
- Offer indoor and/or outdoor community activity/gathering spaces.
- Attract and keep residents.
- Retain and grow property values.
- Support local / small business development

The proposed project would provide a vital fully integrated, mixed-use, pedestrian oriented commercial neighborhood that would meet all of the desired outcomes as listed in the Comprehensive Use Plan.

Specifically related to Section 1.6.17 of the Zoning Ordinance Criteria for Considering Applications for a Rezoning Request:

- A. The conformance of the proposed use to the City's Comprehensive Plan and other adopted planning policies. **Per Section 6.2.2, a PD Planned Development district is equivalent to C-1 Zoning. The proposed development would be in substantial compliance with current C-1 Zoning. Minor deviations are anticipated and included in the deviations list on the cover sheet.**
- B. The character of the neighborhood including, but not limited to: Land use, zoning, density (residential), architectural style, building materials, height, structural mass, siting, open space, and floor-to-area ration (commercial and industrial). **The proposed buildings have intentionally been pushed back from Rainbow Boulevard and the tallest of the buildings are at the center of the site to best fit in with the scale and character of the surrounding neighborhood. Additionally, the majority of existing trees will be preserved at the perimeter of the site to create a large multi-story landscape buffer from surrounding neighbors and preserve the existing quality of the streetscape. Building massing has been broken down in plan and elevation to create smaller scaled facades of varying colors to also complement and blend into the surrounding residential neighborhood.**
- C. The zonings and uses of nearby properties, and the extent to which the proposed use would be in harmony with such zonings and uses. **All nearby properties are Zoned R-1 Single Family Residential. The proposed development would bring a mix of office and retail uses to the project compatible with the surrounding neighborhood providing residents walkable places to work and shop fulfilling the purpose of a fully integrated, mixed-use, pedestrian oriented neighborhood.**
- D. The suitability of the property for the uses to which it has been restricted under the applicable zoning district regulations. **While the current property could be utilized for single family residential, adding a mixture of office, retail and park functions would serve to provide additional amenities to the residents in the area.**
- E. The length of time the property has remained vacant as zoned. **The former church site property along Rainbow Boulevard was acquired by the City of Westwood and the church was demolished to accommodate the city's plan for future development The school, while still in use, is scheduled to be vacated in August 2024.**
- F. The extent to which approval of the application would detrimentally affect nearby properties. **Approval of the application would have no detrimental effect on nearby properties.**
- G. The extent to which the proposed use would substantially harm the value of nearby properties. **The proposed use will not harm and may enhance the value of nearby properties.**
- H. The extent to which the proposed use would adversely affect the capacity or safety of the portion of the road network influenced by the use, or present parking problems in the vicinity of the property. **The existing site functions as both a school and a park, creating peak traffic times as well as various visitor traffic throughout the day. Traffic flow for the proposed development would be of a similar nature with peak traffic times at the beginning and end of the office work day, and various visitor traffic to the retail shops throughout the day. Parking for the proposed development would be handled internally on the site so there would be minimal overflow to the surrounding area.**
- I. The extent to which the proposed use would create excessive air pollution, water pollution, noise pollution, or other environmental harm. **No excess air, water or noise pollution or other environmental harm would occur.**
- J. The economic impact of the proposed use on the community. **The Economic impact of the proposed project would result in a net increase to the sales and property taxes received by the City of Westwood. No tax incentives are being requested by the developer as part of this project apart from a sales tax waiver on construction materials.**
- K. The gain, if any, to the public health, safety, and welfare due to denial of the application as compared to the hardship imposed upon the landowner, if any, as a result of denial of the application. **No gain to the public health, safety and welfare would occur due to denial of the application, however if the application is denied, a substantial portion of the site would remain vacant and the future of city park would be in jeopardy.**

PROJECT NARRATIVE M01

The following Deviations from the equivalent C-1 Zoning are requested as part of the proposed PD Planned Development District:

5.3.7 A Unless otherwise indicated in a specific Overlay District, the facade of building in the C-1 District shall be constructed directly on the build-to-line along at least seventy percent of the length of the building.

The proposed development is surrounded by residential housing of 2 story scale set back typically 35- 60 feet from Rainbow Boulevard. Placing the buildings at property line is inconsistent with surrounding scale and development. Placing the buildings back from Rainbow allows a softening of the perimeter to create a better scale with the neighboring houses as well as opportunity to create more habitable spaces for pedestrians, restaurants and retail. In effect it allows a place to be created versus a space directly adjacent to a major boulevard.

Additionally, 5.5.1 B states that in C-1 Districts, off-street parking of no less than 75% of the parking places shall be to the rear or side of the building. As proposed 85% of the parking spaces will be .

Underground rock is currently located at elevation 936 only a few feet below grade. Due to the natural slope of the site from a high on the northwest to a low on the southeast, placing all parking to the rear of buildings would create a primary retail entrance via an underground parking garage.

5.3.7 C Parking areas and parking garages shall be recessed or placed to the rear of buildings.

5.3.7.H.2 The required setbacks shall be as indicated in each Overlay District, or, in the absence of such criteria, as follows:
2. rear setback: 20 feet minimum.

Due to site constraints and in effort to align the drive isle on the upper level of the parking deck with Adams street we would ask for a deviation on the 20' setback requirement in the rear yard adjacent to the future park.

5.5.1 B states that in C-1 Districts, off-street parking of no less than 75% of the parking places shall be to the rear or side of the building. The proposed development has 85% of parking areas and parking garages at the rear or side of the building, hidden from view. The natural topography of the site sets itself up to place a recessed garage at the rear of the buildings with a small surface parking lot in front of the buildings to allow visible access to the retail and office entries.

5.3.8 A B C One-, two-, and three-story buildings are permitted in the C-1 Mixed-use Districts.

The proposed four-story portions remain in compliance with the zoning requirements and maximum allowable height restrictions of a "mid-rise building". Portions of the building at the center of the project are 4 stories along Rainbow Boulevard. The additional story creates a stronger mass at the center of the site, allowing for rooftop terraces and a stronger sense of hierarchy and visual interest toward the center of the site. Because the project is set into a hillside, the four-story portions of the building allow for a more varied appearance in keeping with the massing and variety of the neighboring residential area. Moreover, on the west and north sides of the site the buildings will only have 3 stories visible above grade.

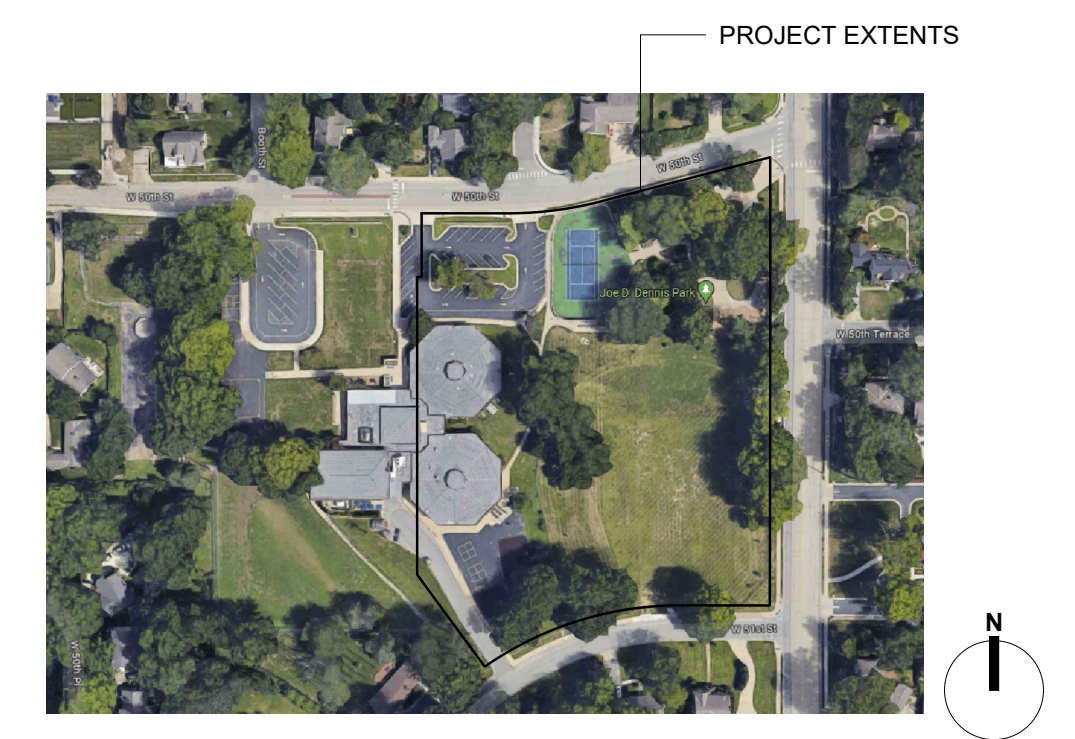
5.4.2.1 Minimum Parking Spaces Per Use

Based on the developer's experience with the local market and retail and office uses of a similar type to those of the proposed development, the amount of parking proposed is less than the minimum requirement and in keeping with market expectations and standards in this location.

DEVIATION REQUESTS F01



PAVILION PERSPECTIVE RENDERING A19



PROPERTY LOCATION MAP A11

PROJECT EXTENTS

PROPERTY DEVELOPER
ADAM FELDMAN
KARBANK REAL ESTATE COMPANY
2000 SHAWNEE MISSION PARKWAY
SUITE 400
MISSION WOODS, KANSAS 66205
816 221 4488
af@karbank.com

CIVIL ENGINEER
AUSTIN LAGE
BHC
7101 COLLEGE BLVD.
SUITE 400
OVERLAND PARK, KANSAS 66210
913.663.1900
austin.lage@bhc.com

ARCHITECT
KENNY MILLER
PERSPECTIVE ARCHITECTURE
& DESIGN
2000 SHAWNEE MISSION PKWY
SUITE 100
MISSION WOODS, KS 66205
816.842.1549
kenny@pad.studio

CONTACT INFORMATION A06

GENERAL	COVER SHEET
A000	COVER SHEET
A001	PROJECT LOCATION PLAN
CIVIL	
C1.0	OVERALL SITE PLAN
C1.1	SITE PLAN - GROUND LEVEL
C1.2	SITE PLAN - SECOND LEVEL
C1.3	SIGHT DISTANCE PLAN
C1.4	FIRE TRUCK MANEUVERABILITY
C2.0	GRADING PLAN
C3.0	BMP PLAN
L1.1	TREE PRESERVATION PLAN
L1.0	LANDSCAPE PLAN - GROUND LEVEL
L1.1	LANDSCAPE PLAN - SECOND LEVEL
PH1.1	PHOTOMETRICS PLAN
ARCHITECTURAL	
AS101	ARCHITECTURAL SITE PLAN
AS102	ARCHITECTURAL SITE PLAN
AS103	ARCHITECTURAL SITE PLAN
A201	BUILDING ELEVATIONS
A202	BUILDING ELEVATIONS
A203	BUILDING ELEVATIONS

SHEET INDEX A01

CONSULTANT



PROJECT
50TH AND RAINBOW DEVELOPMENT

W 50TH ST & RAINBOW BLVD.
WESTWOOD KS, 66205

MARK	DATE	DESCRIPTION
1	08/03/23	CITY COMMENTS
3	09/06/23	CITY COMMENTS

SHEET TITLE

COVER SHEET

PROJECT NUMBER
2021073.000

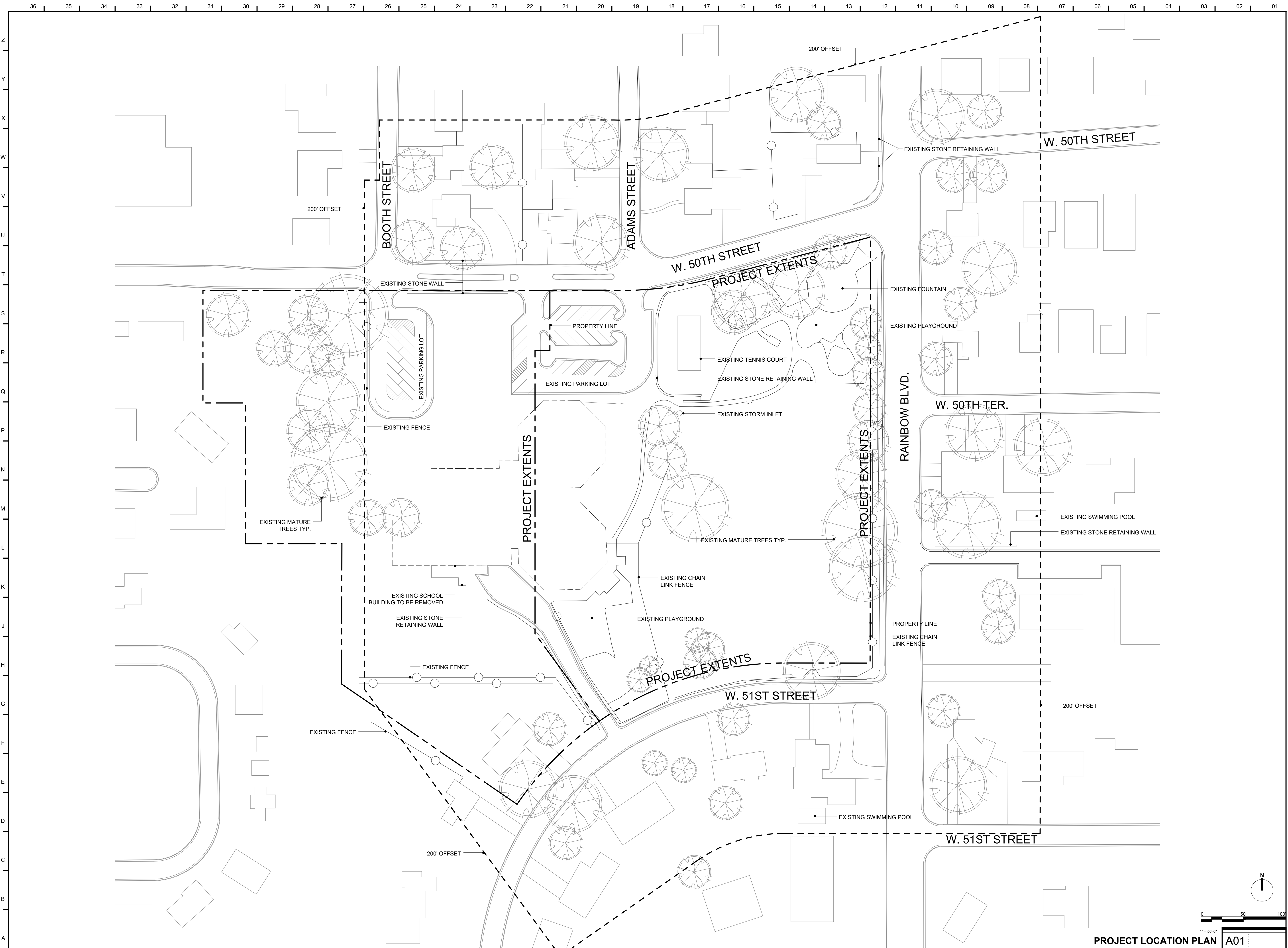
SHEET AUTHOR

CHECKED BY

DATE
7.20.23

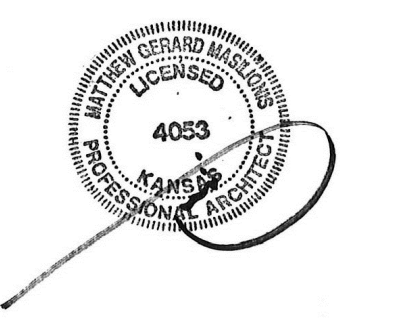
PDP SUBMITTAL

SHEET NUMBER
A000



Perspective

2000 SHAWNEE MISSION PKWY
SUITE 100
MISSION WOODS, KS 66205
816 502 1500
WWW.PAD.STUDIO



MATTHEW MASILIONIS - ARCHITECT
KANSAS # 4053

I have prepared the drawings and assume responsibility for the sheets numbered with an "A" prefix for the project named below.

Other drawings and specifications attached for the above-mentioned project have been by and are the responsibility of the licensed engineer whose stamp and firm appear on that sheet.

The Architect is not responsible for the design of the mechanical, electrical, plumbing, civil, landscaping, structural, signage (not specified), fire sprinkler or fire suppression systems, and does not take responsibility for the compliance of these areas with the laws of the above governmental entities. The architect is not responsible for materials, components or equipment, as well as the method in which they are installed on the project by others. The architect is not liable or responsible for verification, during construction or upon completion of construction. The architect is not responsible for improper operation due to faulty installation or product failure during construction or after completion of construction when operation has begun by the landlord or tenant.

The licensed professional whose stamp appears on sheets other than those specifically noted above shall be responsible for those items in paragraph three.

CONSULTANT



PROJECT
50TH AND RAINBOW DEVELOPMENT

50TH ST. & RAINBOW BLVD.
WESTWOOD KS, 66205

MARK	DATE	DESCRIPTION
△	08/03/23	CITY COMMENTS

SHEET TITLE

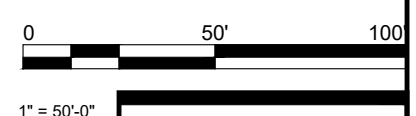
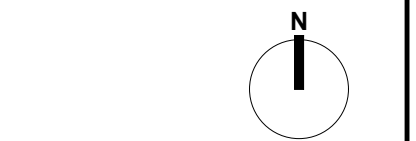
PROJECT LOCATION PLAN

PROJECT NUMBER
2021073.000
SHEET AUTHOR
K. MILLER

CHECKED BY
M. PAXTON
DATE
7.20.23

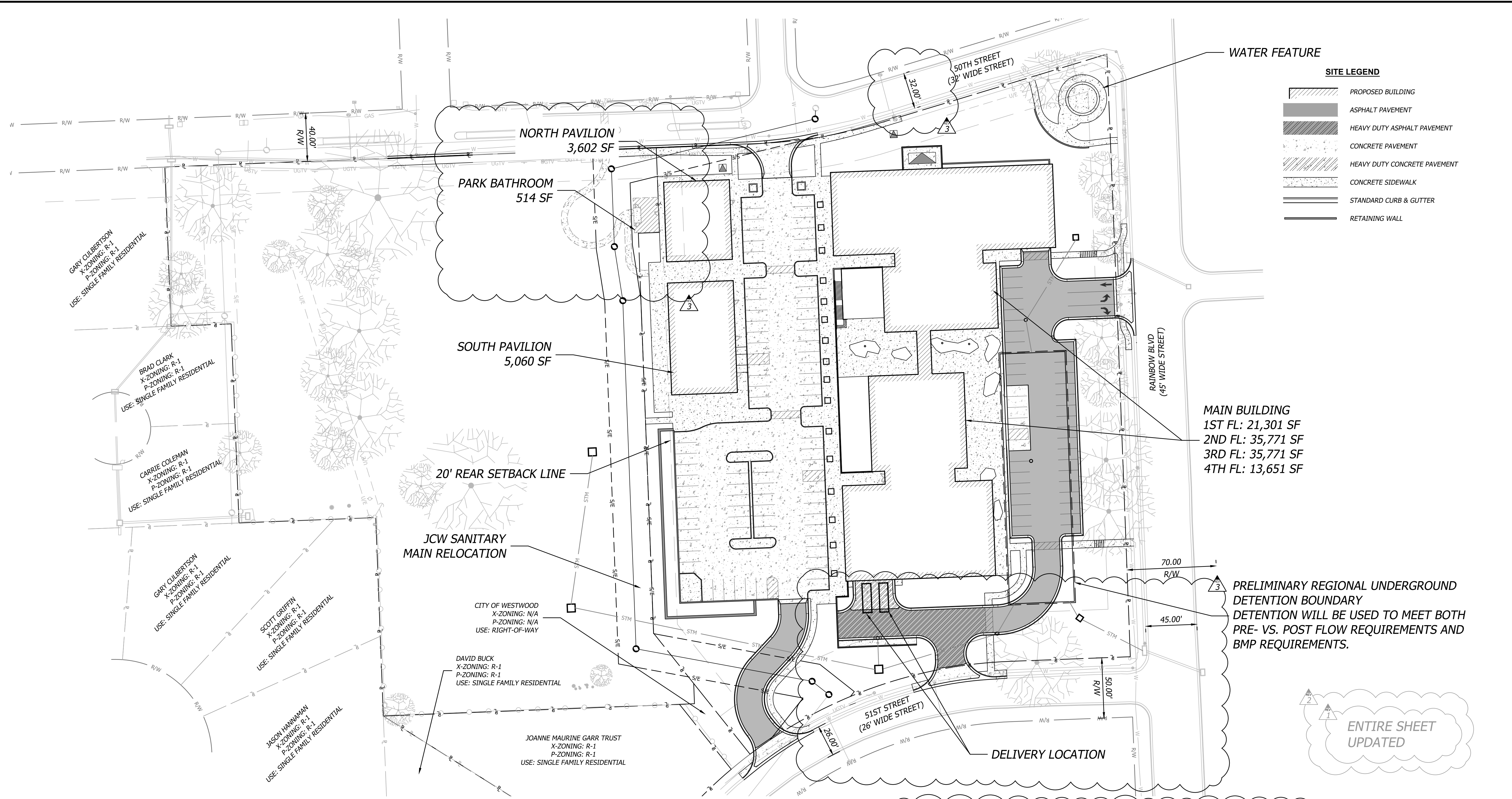
PDP SUBMITTAL

SHEET NUMBER
A001 △



PROJECT LOCATION PLAN A01

Sep. 07, 2023 - 9:15am Plotted By: austin.lage V:\037920-Westwood\04-DWG\Eng\Sheet\037920-SRFS-SITE.dwg Layout: OVERALL SITE PLAN



SITE LEGEND

	PROPOSED BUILDING
	ASPHALT PAVEMENT
	HEAVY DUTY ASPHALT PAVEMENT
	CONCRETE PAVEMENT
	HEAVY DUTY CONCRETE PAVEMENT
	CONCRETE SIDEWALK
	STANDARD CURB & GUTTER
	RETAINING WALL

MAIN BUILDING
 1ST FL: 21,301 SF
 2ND FL: 35,771 SF
 3RD FL: 35,771 SF
 4TH FL: 13,651 SF

PRELIMINARY REGIONAL UNDERGROUND DETENTION WILL BE USED TO MEET BOTH PRE- VS. POST FLOW REQUIREMENTS AND BMP REQUIREMENTS.

ENTIRE SHEET UPDATED

SITE DATA

OVERALL SITE	
SITE AREA:	7.62 AC 332,129 SF
IMPERVIOUS AREA:	
EXISTING:	121,314 SF (36.5%)
PROPOSED:	133,114 SF (40.1%)
WEST SITE	
SITE AREA:	3.38 AC 147,412 SF
IMPERVIOUS AREA:	
EXISTING:	55,142 SF (37.4%)
PROPOSED:	3,758 SF (2.55%)
EAST SITE	
SITE AREA:	4.24 AC 184,717 SF
IMPERVIOUS AREA:	
EXISTING:	66,158 SF (35.8%)
PROPOSED:	129,356 SF (70.0%)

BUILDING DATA - GROSS AREA

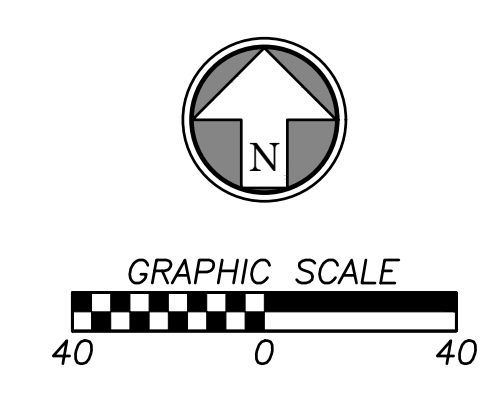
WEST SITE	
PUBLIC BATHROOM:	514 SF (0.35%)
EAST SITE	
TOTAL BUILDING FOOTPRINT:	44,433 SF (24.1%)
TOTAL BUILDING SF:	115,156 SF
MAIN BUILDING:	
FOOTPRINT:	35,771 SF (19.4%)
TOTAL SF (4 FLOORS):	106,494 SF
PAVILION BUILDINGS:	
NORTH FOOTPRINT:	3,602 SF (1.95%)
SOUTH FOOTPRINT:	5,060 SF (2.74%)
LOT COVERAGE	
BUILDING/STRUCTURE (100% TOTAL AREA)	106,906 SF (57.0%)
OVERHANG ABOVE OPEN AIR (50% TOTAL AREA)	4,125 SF (2.20%)
TOTAL	111,031 SF (59.2%)

PARKING DATA

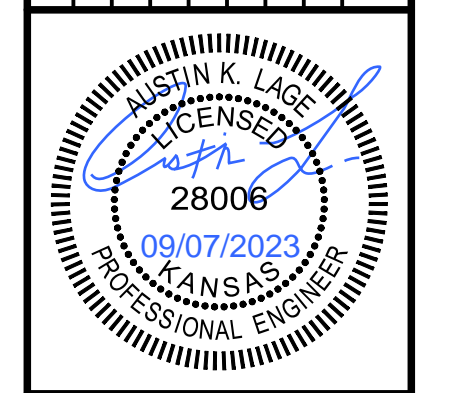
PARKING PROVIDED:	291 STANDARD 8 ADA ACCESSIBLE (4 VAN)
PARKING REQUIRED:	STANDARD PARKING STALLS: REFER TO ARCHITECTURAL SHEETS FOR CALCULATIONS
ADA ACCESSIBLE STALLS:	7 ADA ACCESSIBLE (2 VAN) (BASED ON 290 PROVIDED PARKING)

EXISTING ZONING
 R-1 (SINGLE FAMILY RESIDENTIAL)

PROPOSED ZONING
 CP-1 (PLANNED COMMERCIAL)

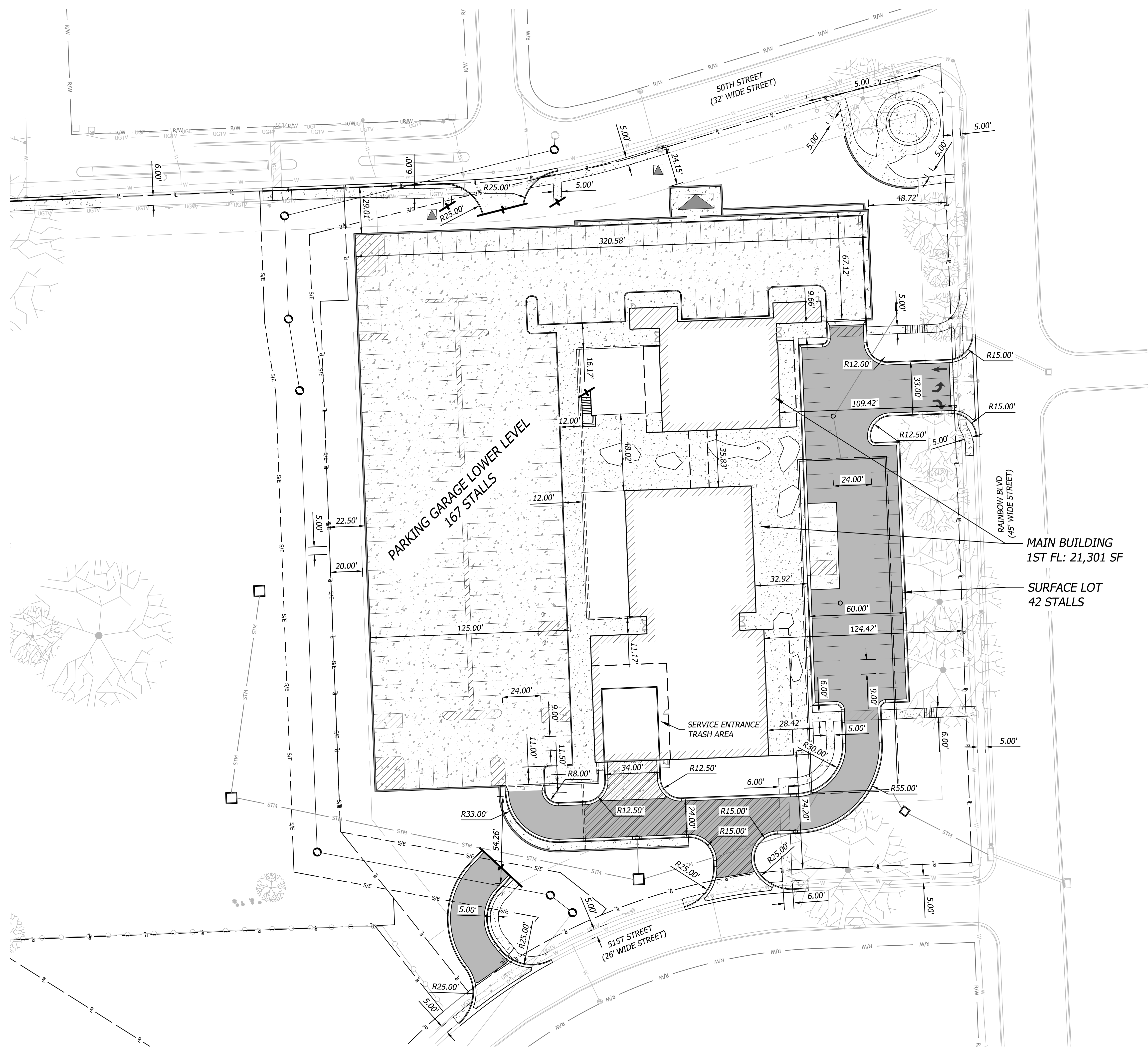


<p>Prepared For: KARIBANK REAL ESTATE COMPANY ADAM FELDMAN 20000 SH PARKWAY, SUITE 400 MISSIONS WOODS, KS 66205 816-221-4488</p>	<p>Design: DSN Drawn: DRN Checked: CHK Issue Date: 07/20/2023 Project Number: 037920</p>
<p>PDP SUBMITTAL 50TH & RAINBOW DEVELOPMENT WESTWOOD, KS 66205</p> <p>OVERALL SITE PLAN</p>	
<p>C1.0</p>	



Rev.	Date	Description
3	09/06/23	CITY COMMENTS
2	08/25/23	PDP UPDATE
1	08/03/23	CITY COMMENTS

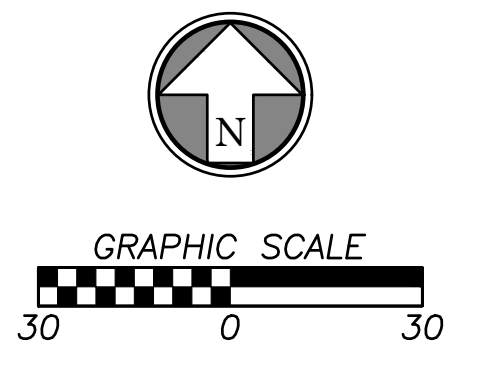
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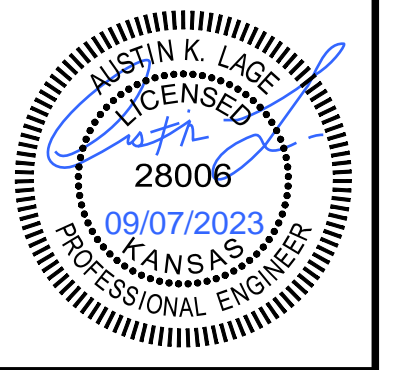
SITE LEGEND

	PROPOSED BUILDING
	ASPHALT PAVEMENT
	HEAVY DUTY ASPHALT PAVEMENT
	CONCRETE PAVEMENT
	HEAVY DUTY CONCRETE PAVEMENT
	CONCRETE SIDEWALK
	STANDARD CURB & GUTTER
	RETAINING WALL

ENTIRE SHEET
 UPDATED



Rev.	Date	Description	By	App.
3	09/06/23	CITY COMMENTS	AKL	AKL
2	08/25/23	PDP UPDATE	AKL	AKL
1	08/03/23	CITY COMMENTS	AKL	AKL



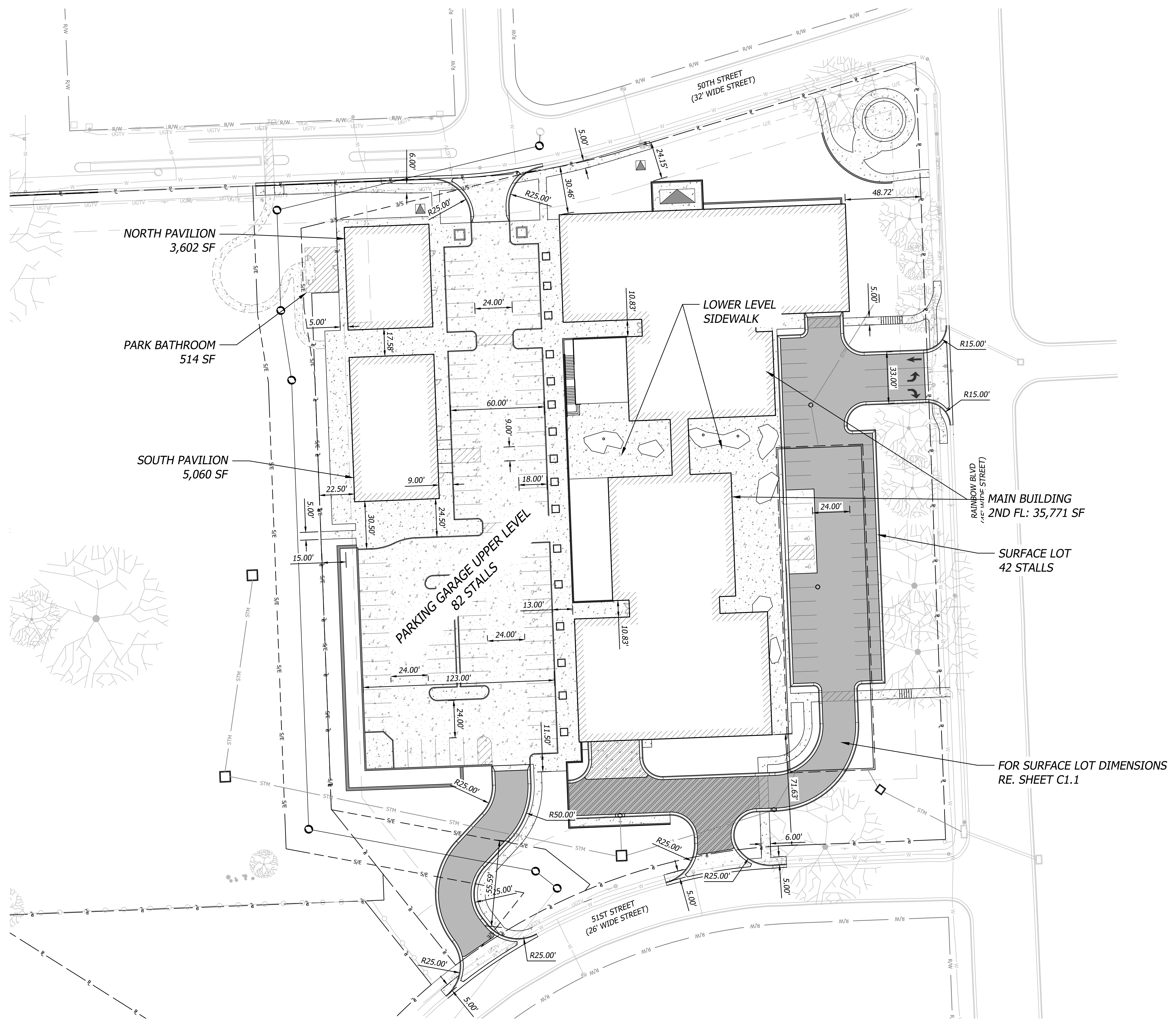
Prepared For:
 KARBANK REAL ESTATE COMPANY
 ADAM FELDMAN
 20000 SHY PKWY, SUITE 400
 MISSIONS WOODS, KS 66205
 816-221-4488

PDP SUBMITTAL
50TH & RAINBOW DEVELOPMENT
WESTWOOD, KS 66205
SITE PLAN-GROUND LEVEL

Design: DSN | Drawn: DRN
 Checked: CHK
 Issue Date: 07/20/2023
 Project Number: 037920

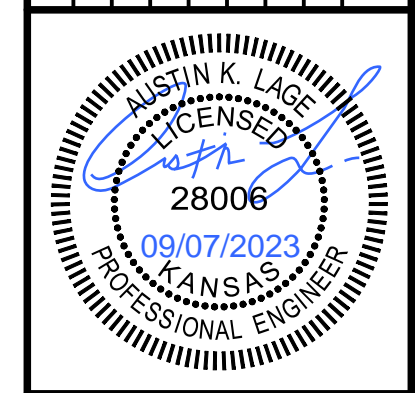
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SITE LEGEND

	PROPOSED BUILDING
	ASPHALT PAVEMENT
	HEAVY DUTY ASPHALT PAVEMENT
	CONCRETE PAVEMENT
	HEAVY DUTY CONCRETE PAVEMENT
	CONCRETE SIDEWALK
	STANDARD CURB & GUTTER
	RETAINING WALL



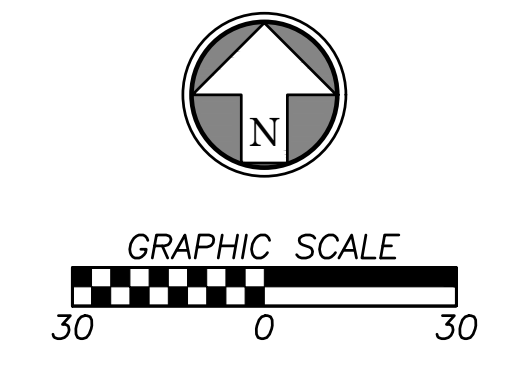
Prepared For:
 KARBANK REAL ESTATE COMPANY
 ADAM FELDMAN
 20000 SHI PKWY, SUITE 400
 MISSIONS WOODS, KS 66205
 816-221-4488

PDP SUBMITTAL
50TH & RAINBOW DEVELOPMENT
WESTWOOD, KS 66205
SITE PLAN-SECOND LEVEL

Design: DSN | Drawn: DRN
 Checked: CHK
 Issue Date: 07/20/2023
 Project Number: 037920

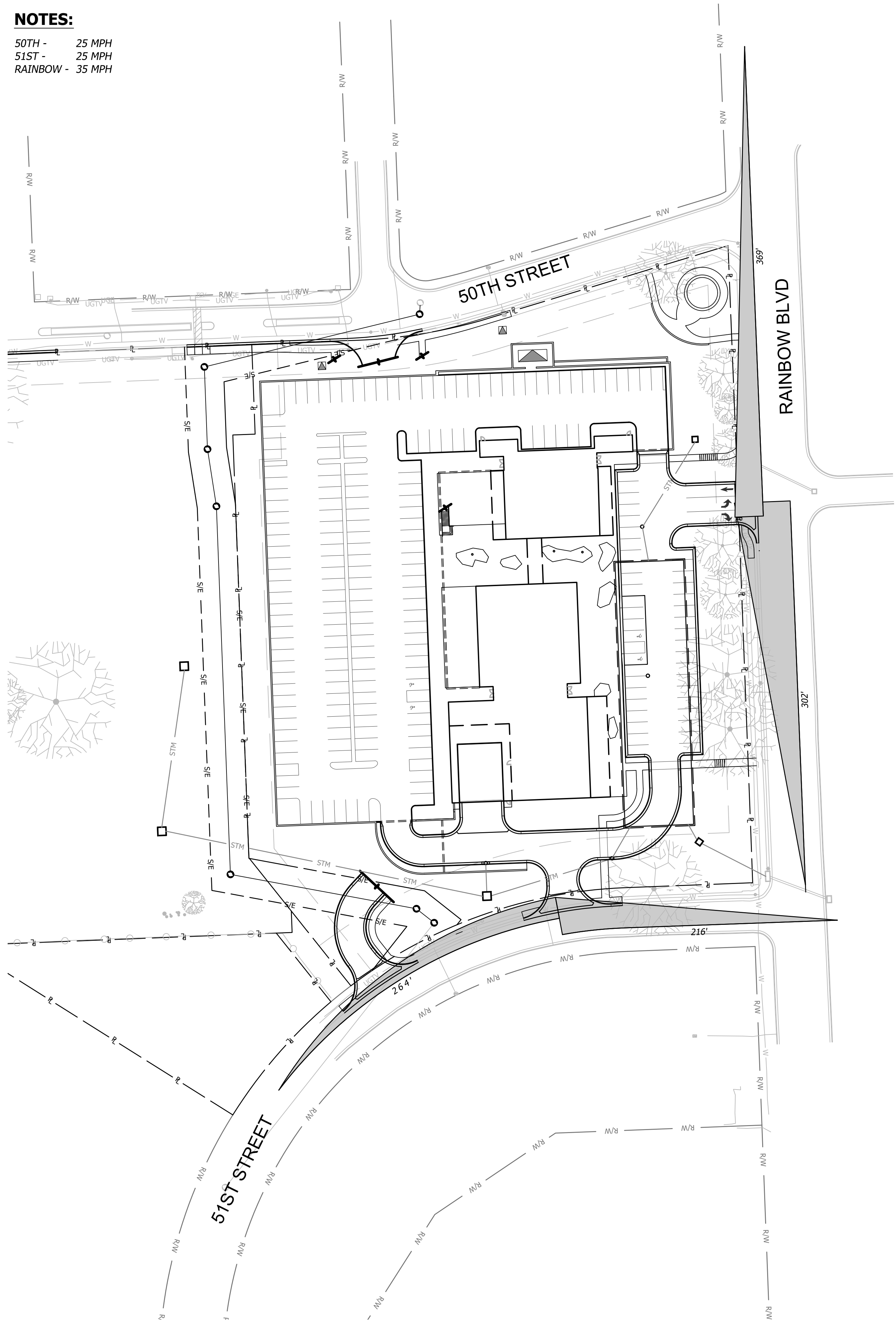
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3
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**ENTIRE SHEET
 UPDATED**

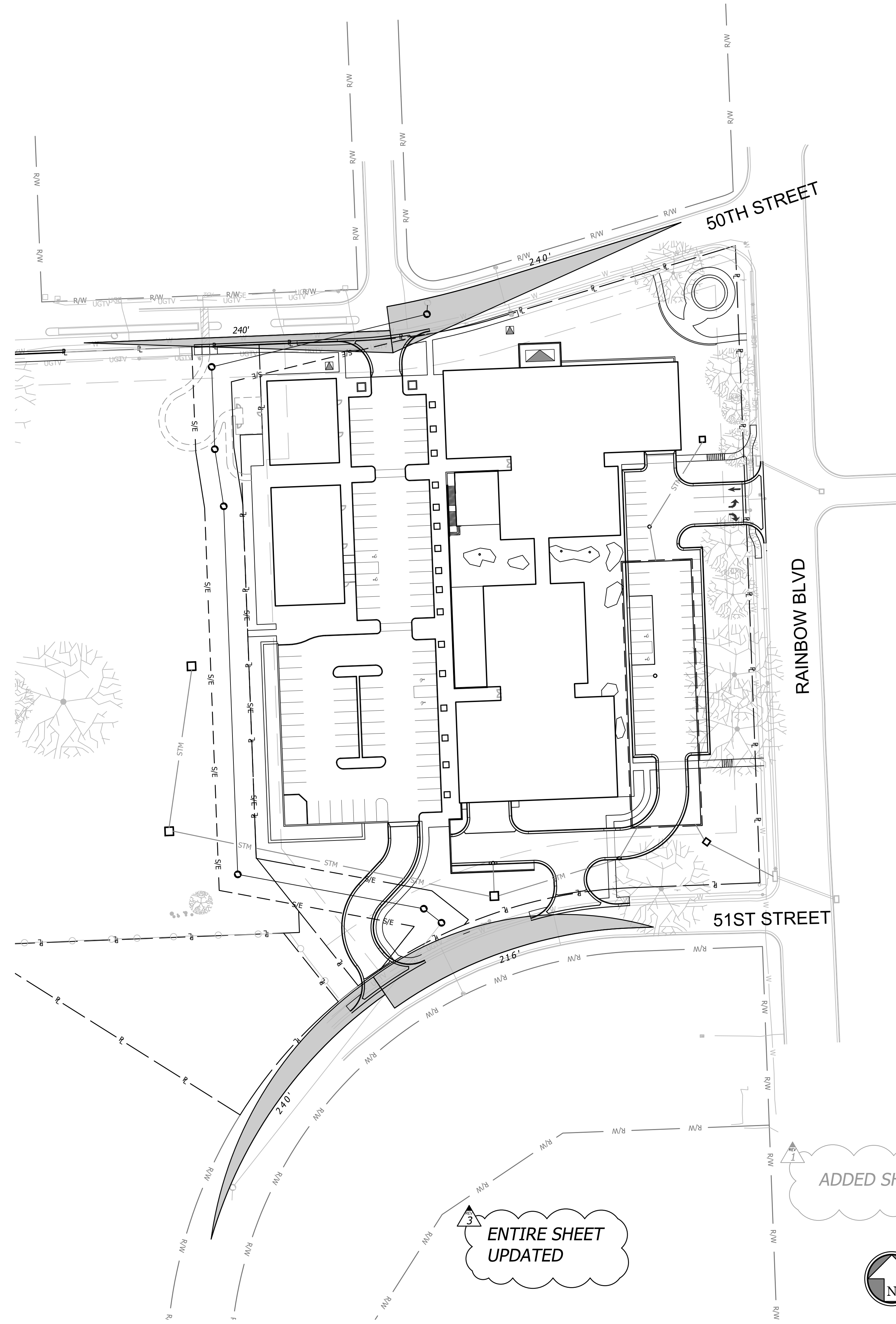


Rev.	Date	Description	By	App.
1	09/06/23	CITY COMMENTS	AKL	
2	08/25/23	PDP UPDATE	AKL	
3			AKL	

NOTES:
 50TH - 25 MPH
 51ST - 25 MPH
 RAINBOW - 35 MPH



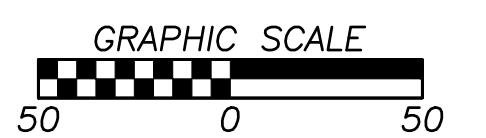
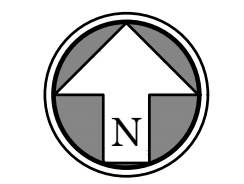
GROUND LEVEL



SECOND LEVEL

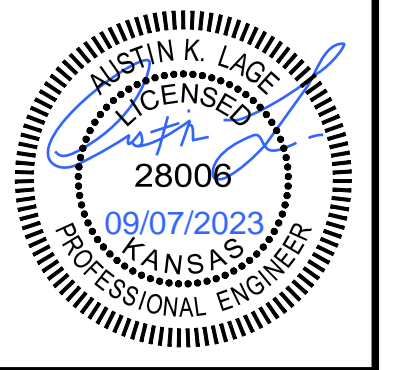
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UPDATED

ADDED SHEET



Sep. 07, 2023 - 9:15am Plotted By: austin.lage V:\037920-Westwood\04-DWG\Eng\Sheet\037920-SFIS-SITE.dwg Layout: SIGHT DISTANCE PLAN

Rev.	Date	Description	By	App.
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2	08/25/23	PDP UPDATE	AKL	
1	08/03/23	CITY COMMENTS	AKL	



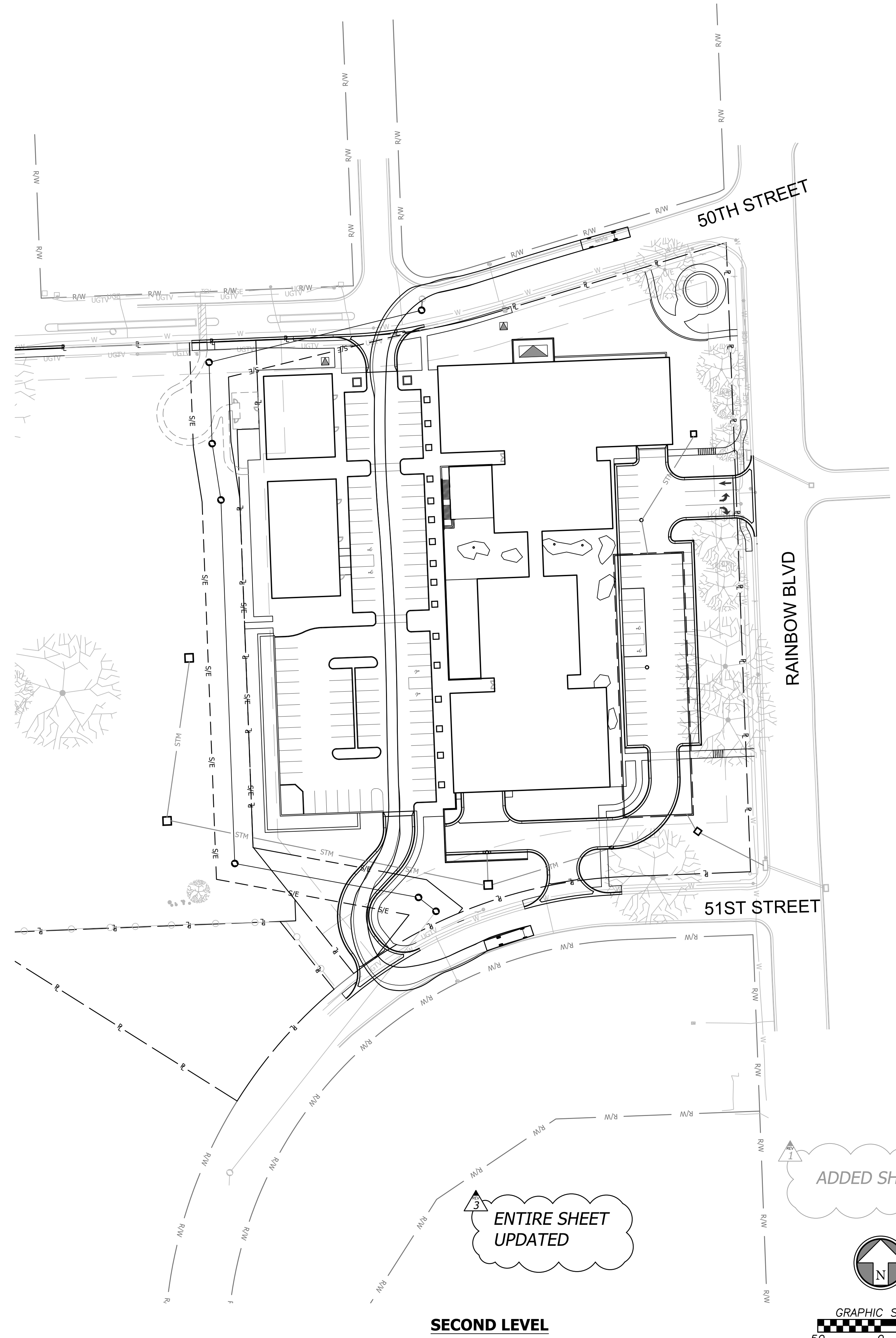
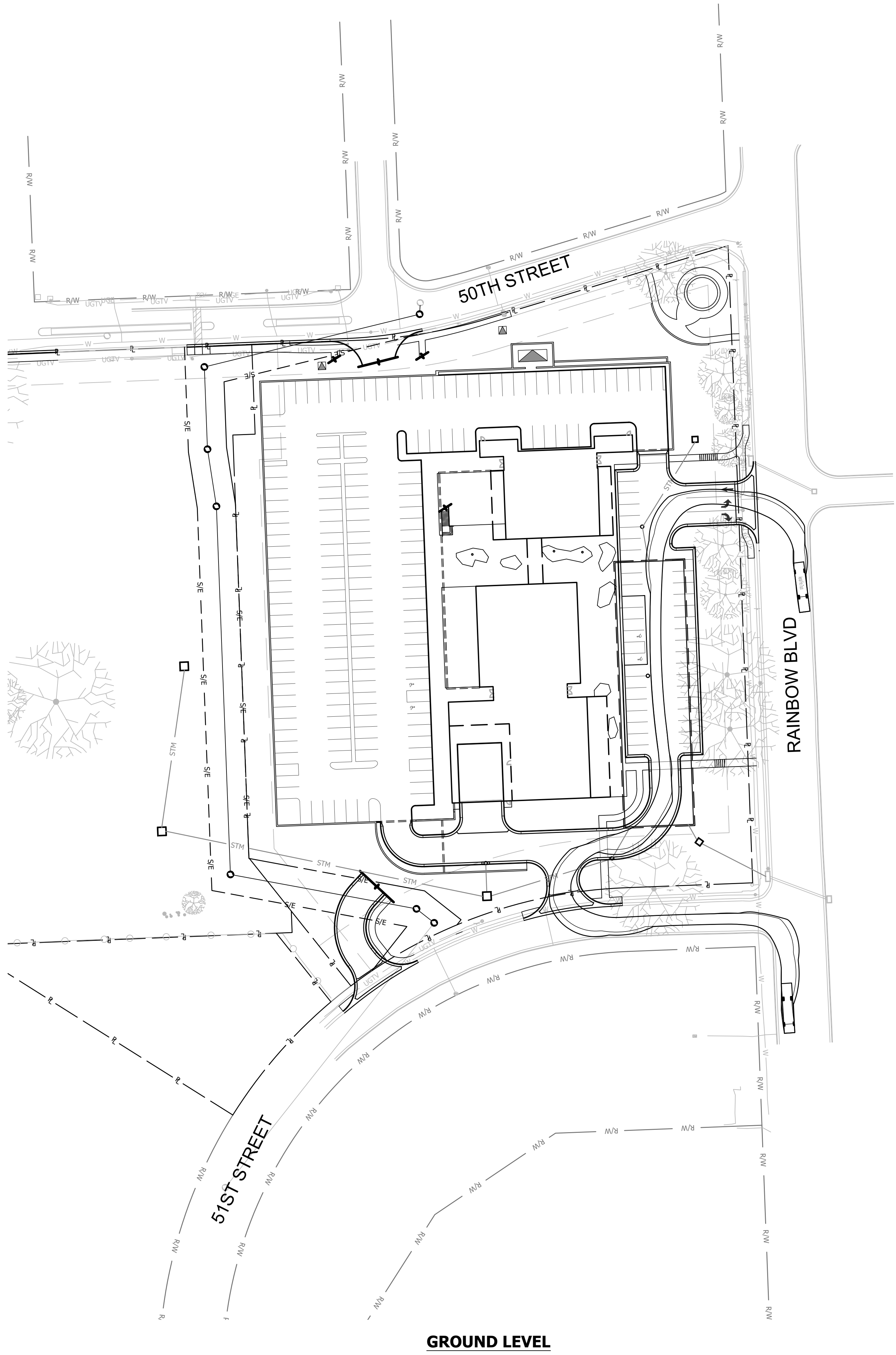
Prepared For:
 KARBANK REAL ESTATE COMPANY
 ADAM FELDMAN
 20000 SH PARKWAY, SUITE 400
 MISSIONS WOODS, KS 66205
 816-221-4488

PDP SUBMITTAL
50TH & RAINBOW DEVELOPMENT
WESTWOOD, KS 66205
SIGHT DISTANCE PLAN

Design: DSN | Drawn: DRN
 Checked: CHK
 Issue Date: 07/20/2023
 Project Number: 037920

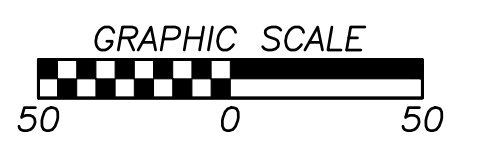
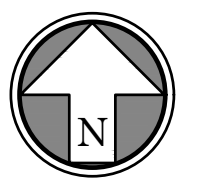
C1.3

Sep. 07, 2023 - 9:15am Plotted By: austin.lage V:\037920-Westwood\04-DWG\Eng\Sheet\037920-SFIS-SITE.dwg Layout: FIRE TRUCK MANEUVERABILITY

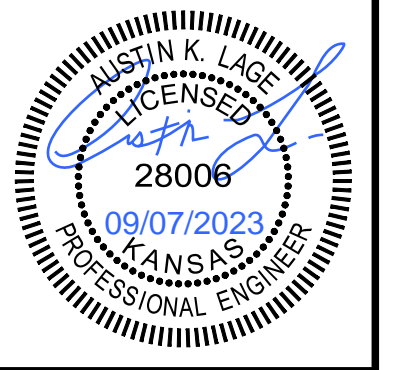


ENTIRE SHEET
UPDATED

ADDED SHEET



Rev.	Date	Description	By	App.
3	09/06/23	CITY COMMENTS	AKL	
1	08/03/23	CITY COMMENTS	AKL	

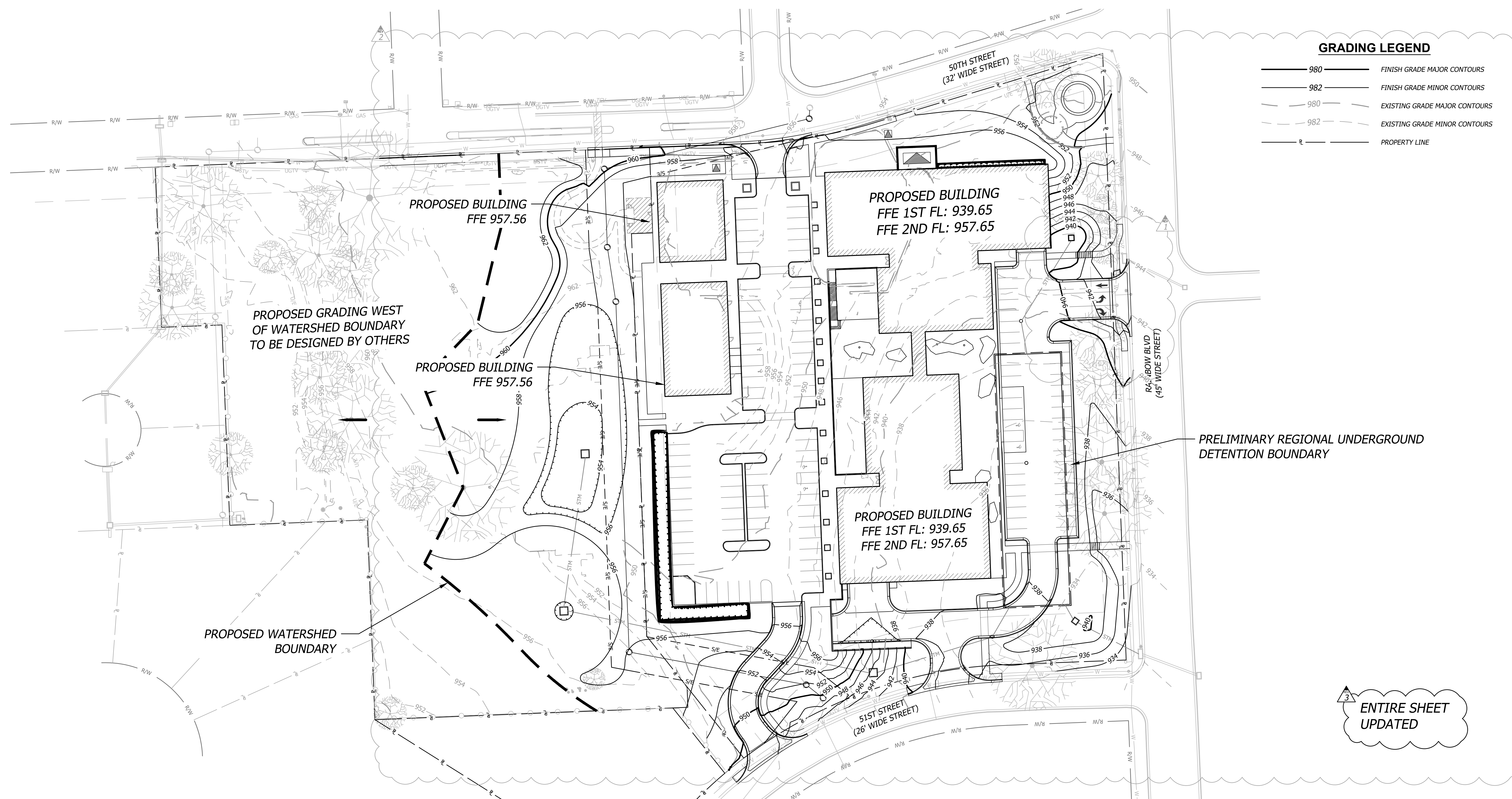


Prepared For:
 KARBANK REAL ESTATE COMPANY
 ADAM FELDMAN
 20000 SH PARKWAY, SUITE 400
 MISSIONS WOODS, KS 66205
 816-221-4488

PDP SUBMITTAL
50TH & RAINBOW DEVELOPMENT
WESTWOOD, KS 66205
FIRE TRUCK MANEUVERABILITY

Design: DSN | Drawn: DRN
 Checked: CHK
 Issue Date: 07/20/2023
 Project Number: 037920

C1.4



GRADING LEGEND

- 980 — FINISH GRADE MAJOR CONTOURS
- 982 — FINISH GRADE MINOR CONTOURS
- - - 980 - - - EXISTING GRADE MAJOR CONTOURS
- - - 982 - - - EXISTING GRADE MINOR CONTOURS
- - - R - - - PROPERTY LINE

PROPOSED GRADING WEST OF WATERSHED BOUNDARY TO BE DESIGNED BY OTHERS

PROPOSED WATERSHED BOUNDARY

PRELIMINARY REGIONAL UNDERGROUND DETENTION BOUNDARY

ENTIRE SHEET UPDATED

GRADING NOTES

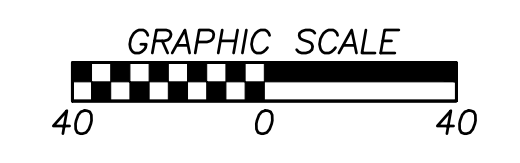
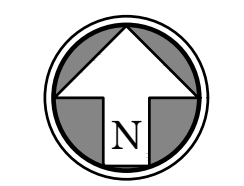
1. Contractor shall obtain a copy of the Geotechnical Services Report for the project and be familiar with the existing conditions and recommendations contained in the report if such a report has been prepared.
2. Contractor is responsible for any over excavation of existing unsuitable soils will be required under building and pavement areas. Contractor shall perform over excavation of unsuitable soils as a part of this work.
3. Contractor shall obtain soils suitable as structural fill from off-site sources. All borrow materials must be tested and approved by the Geotechnical Engineer prior to importing the soils to the project site.
4. Contractor shall operate under the terms and permits included in the Stormwater Pollution Prevention Plan (SWPPP) prepared for this project and permitted through the State of Kansas. Contractor shall employ a qualified person to conduct regular inspections of the site erosion control measures and document such inspections in the SWPPP document maintained by the Contractor.
5. All topsoil, vegetation, root structures, and deleterious materials shall be stripped from the ground surface prior to the placement of embankments. Contractor shall obtain the on-site geotechnical representative's acceptance of the existing ground surface materials and the proposed fill material prior to the placement of fill.
6. All proposed contour lines and spot elevations shown are finish ground elevations. Contractor shall account for pavement depths, building pads, topsoil, etc when grading the site.
7. All disturbed areas that are not to be paved (green spaces) shall be finish graded with a minimum of six inches of topsoil.
8. All excavation and embankments shall comply with the recommendations provided by the geotechnical engineer.
9. Prior to placing any concrete or asphalt pavement the contractor shall perform a proof roll of the pavement sub-grade with a fully loaded tandem axle dump truck. The proof roll shall be conducted in the presence of the on-site geotechnical representative. Areas that display rutting or pumping that are unsatisfactory to the geotechnical representative shall be re-worked and a follow-up proof roll shall be conducted prior to acceptance of the sub-grade for paving. The contractor may, at its own expense, stabilize the sub-grade using Class C fly ash or quicklime, as approved by the geotechnical engineer.
10. Finished grades shall not be steeper than 3:1.
11. All grading work shall be considered unclassified. No additional payments shall be made for rock excavation. Contractor shall satisfy himself as to any rock excavation required to accomplish the improvements shown hereon.
12. A 2.0% maximum cross slope shall be maintained on all pedestrian sidewalks and paths.

FLOOD STATEMENT

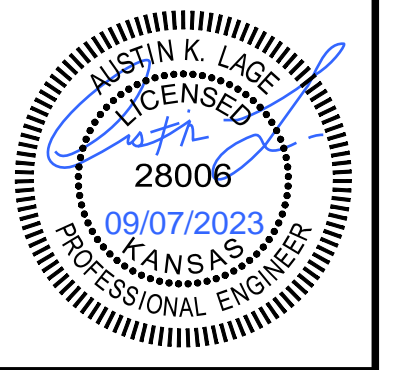
The subject property lies within Flood Zone "X" (unshaded) (Areas determined to be outside the 0.2% annual chance floodplain.), as shown on the Johnson County, Kansas and Incorporated Areas Flood Insurance Rate Map (F.I.R.M.).
 Map Number: 20091C0010G
 Panel No: 10 of 161
 Map Revised Date: August 3, 2009
NOTE: This statement is provided for informational purposes only and shall in no way constitute a basis for a flood certificate. No field work was performed to establish the boundaries of this zone. The information was derived by scaling the subject property on the above referenced map.

BENCHMARKS (DATUM: NAVD88)

JOHNSON COUNTY VERTICAL CONTROL POINT BENCHMARK
 BENCHMARK NUMBER: 901
 ELEVATION= 883.46
 BERNTSEN ALUMINUM DISK STAMPED BM 901. TOP 24 INCH CONCRETE CURB IN LINE WITH WEST FACE OF RCB, 20 FEET SOUTH OF RCB ON THE NORTH SIDE 50TH TERR.



Rev.	Date	Description
3	09/06/23	CITY COMMENTS PDP UPDATE
2	08/25/23	CITY COMMENTS
1	08/03/23	CITY COMMENTS



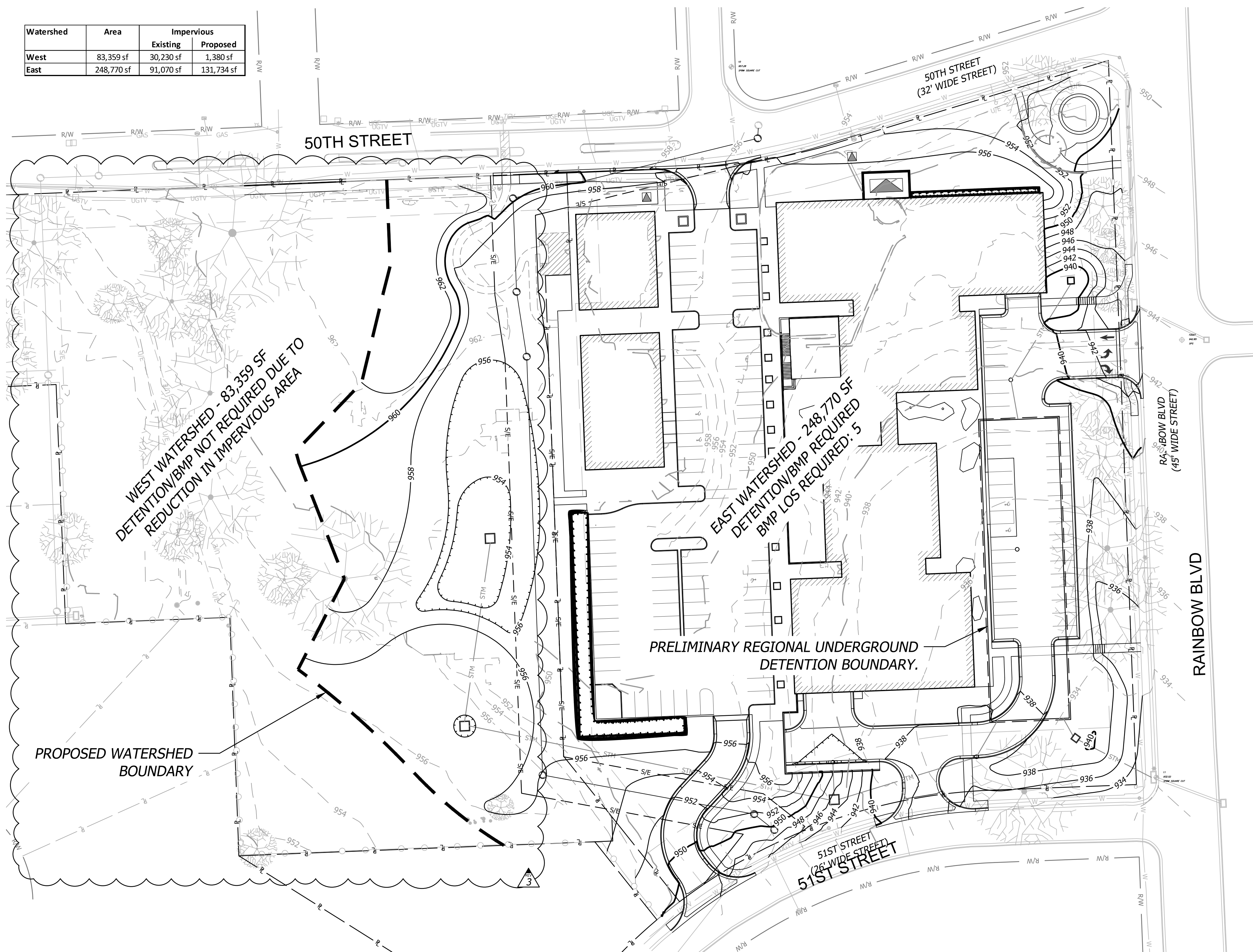
Prepared For:
 KARBANK REAL ESTATE COMPANY
 ADAM FELDMAN
 20000 SHI PKWY, SUITE 400
 MISSIONS WOODS, KS 66205
 816-221-4488

PDP SUBMITTAL
50TH & RAINBOW DEVELOPMENT
WESTWOOD, KS 66205
GRADING PLAN

Design: DSN | Drawn: DRN
 Checked: CHK
 Issue Date: 07/20/2023
 Project Number: 037920

C2.0

Watershed	Area	Impervious	
		Existing	Proposed
West	83,359 sf	30,230 sf	1,380 sf
East	248,770 sf	91,070 sf	131,734 sf



WEST WATERSHED - 83,359 SF
DETENTION/BMP NOT REQUIRED DUE TO
REDUCTION IN IMPERVIOUS AREA

EAST WATERSHED - 248,770 SF
DETENTION/BMP REQUIRED
BMP LOS REQUIRED: 5

PRELIMINARY REGIONAL UNDERGROUND
DETENTION BOUNDARY.

PROPOSED WATERSHED
BOUNDARY

NOTE:

FINAL SIZING, LOCATION, ROUTING OF STORM AND
DETENTION WILL BE INCLUDED IN THE FINAL
DEVELOPMENT PLANS

EAST WATERSHED IS TREATED AS A DEVELOPED SITE
WEST WATERSHED IS TREATED AS AN UNDEVELOPED SITE

GRADING LEGEND

- 980 — FINISH GRADE MAJOR CONTOURS
- 982 — FINISH GRADE MINOR CONTOURS
- - 980 - - EXISTING GRADE MAJOR CONTOURS
- - 982 - - EXISTING GRADE MINOR CONTOURS
- - - - PROPERTY LINE

1. Runoff Curve Number - East Watershed

A. Predevelopment CN

Cover Description	Soil HSG	CN From		Product of	
		Table 1	Area (sf)	Area (ac.)	CN x Area
Open Space (turf), Good	D	80	157700	3.62	289.6
Impervious	D	98	91070	2.09	204.9
				0.00	0.0
				0.00	0.0
				0.00	0.0
				0.00	0.0
				0.00	0.0
				0.00	0.0
Totals:				5.71	494.5

Area-Weighted CN = total product/total area = **87** (Round to integer)

B. Postdevelopment CN

Cover Description	Soil HSG	CN From		Product of	
		Table 1	Area (sf)	Area (ac.)	CN x Area
Open Space (turf), Good	D	80	117036	2.69	214.9
Impervious	D	98	131734	3.02	296.4
				0.00	0.0
				0.00	0.0
				0.00	0.0
				0.00	0.0
				0.00	0.0
				0.00	0.0
				0.00	0.0
Totals:				5.71	511.3

1 Postdevelopment CN is one HSG higher for all cover types except preserved vegetation, absent documentation showing how postdevelopment soil structure will be preserved.

Area-Weighted CN = total product/total area = **90** (Round to integer)

C.

Level of Service Calculation		Change in CN	LS
Predevelopment CN:	87	17+	8
Post Development CN:	90	7 to 16	7
Difference:	3	4 to 6	6
		1 to 3	5
		0	4
		-7 to -1	3
		-8 to -17	2
		-18 to -21	1
		-22 -	0
LS Required (see scale at right):	5		

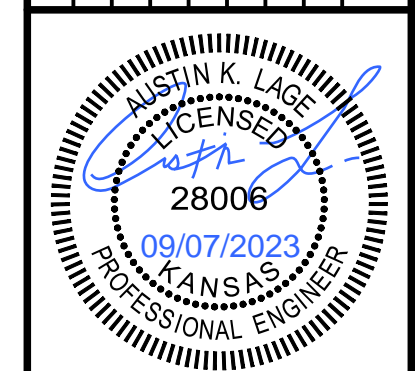
2. Proposed BMP Option Package No. 1

BMP ID	Cover/BMP Description	Treatment Area	VR from Table 4.4 or 4.6	Product of VR x Area
1	ADS Isolator Row	3.25	9.0	29.3
2	No BMP	2.46	0.0	0.0
3				
4				
Total:		5.71	Total:	5.1

- 1 VR calculated for final BMP only in Treatment Train
- 2 Total treatment area cannot exceed 100 percent of the actual site area.
- * Blank in Redevelopment

Meets required LS (Yes/No)? **Yes** (If No, or if additional options are being tested, proceed below.)

Rev.	Date	Description	By	App.
3	09/06/23	CITY COMMENTS	AKL	AKL
2	08/25/23	PDP UPDATE	AKL	AKL
1	08/03/23	CITY COMMENTS	AKL	AKL



Prepared For:
KARBANK REAL ESTATE COMPANY
ADAM FELDMAN
20000 SH PARKWAY, SUITE 400
MISSIONS WOODS, KS 66205
816-221-4488

PDP SUBMITTAL
50TH & RAINBOW DEVELOPMENT
WESTWOOD, KS 66205
BMP PLAN

Design: DSN | Drawn: DRN
Checked: CHK
Issue Date: 07/20/2023
Project Number: 037920

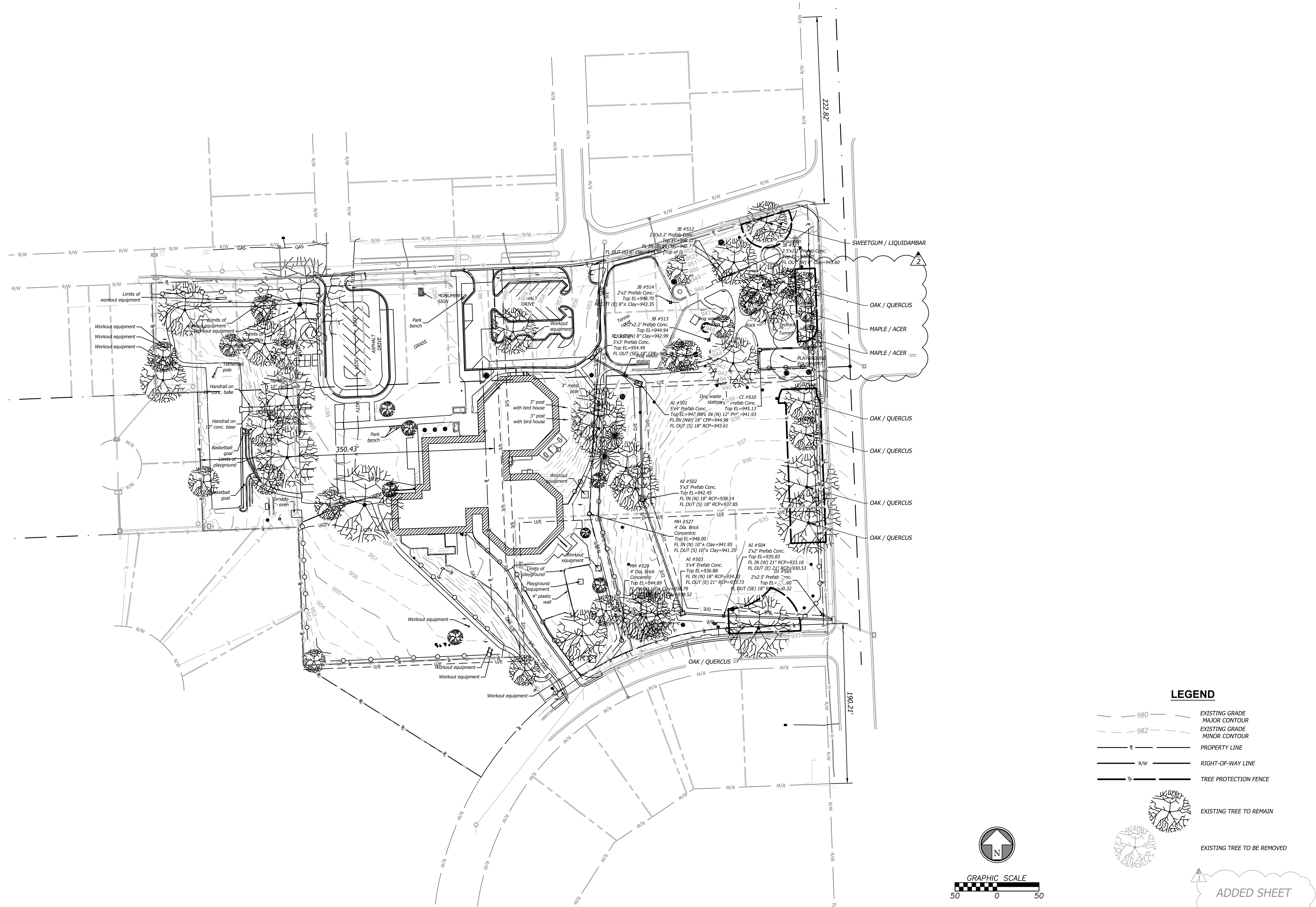
C3.0

Sep. 07, 2023 - 9:15am Plotted By: austin.lag... V:\037920-Westwood\04-DWG\Eng\Sheet\037920-SRFS-6660.dwg Layout: BMP PLAN

ADDED SHEET

GRAPHIC SCALE
40 0 40

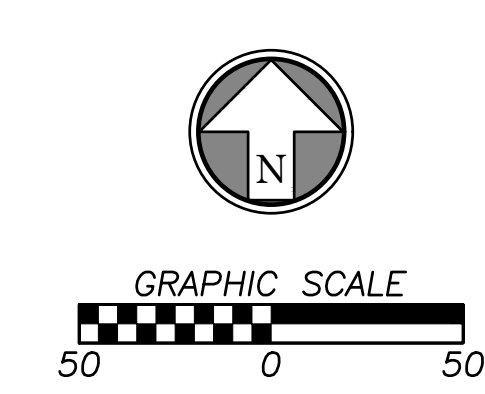
Sep. 07, 2023 - 9:26am Plotted By: angela.moyer V:\037920- Westwood\04-DWG\Eng\Sheet\037920-SHTS-LASC-TREE.dwg Layout: Tree Pres Plan



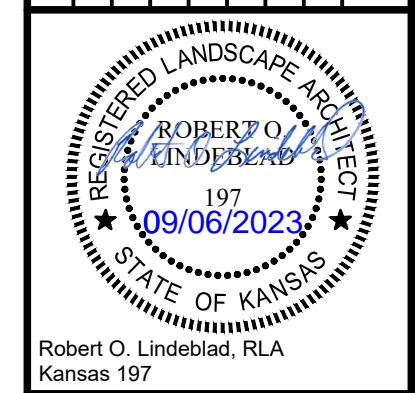
LEGEND

- 980 EXISTING GRADE MAJOR CONTOUR
- 982 EXISTING GRADE MINOR CONTOUR
- PROPERTY LINE
- RIGHT-OF-WAY LINE
- TREE PROTECTION FENCE
- EXISTING TREE TO REMAIN
- EXISTING TREE TO BE REMOVED

ADDED SHEET



Rev.	Date	Description	By	App.
3	09/06/23	CITY COMMENTS	ROL	ROL
2	08/25/23	PDP UPDATE	ROL	ROL
1	08/03/23	CITY COMMENTS	ROL	ROL



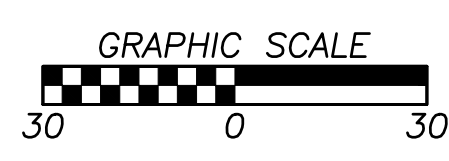
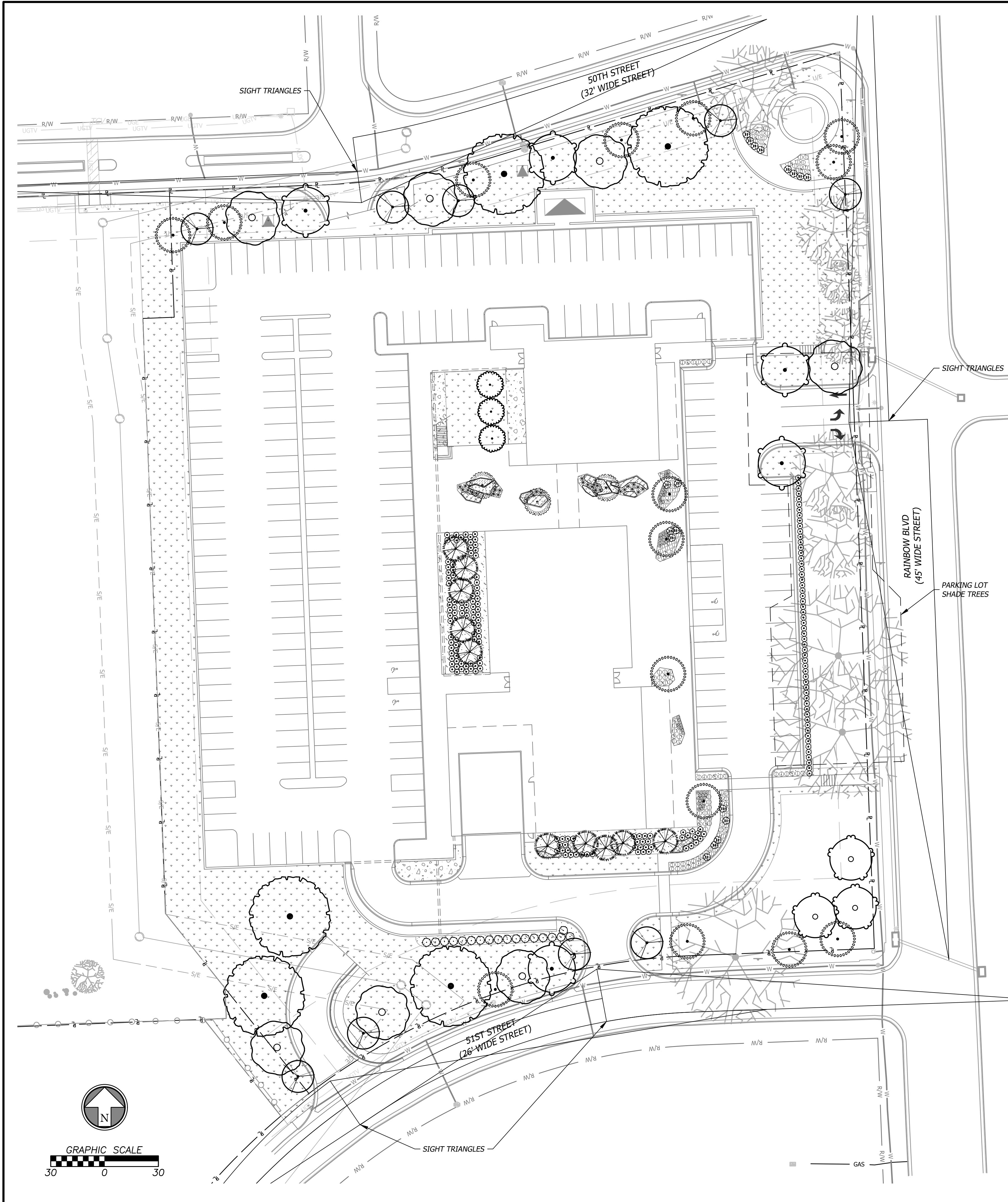
Prepared For:
 KARIBANK REAL ESTATE COMPANY
 ADAM FELDMAN
 20000 SHI PKWY, SUITE 400
 MISSIONS WOODS, KS 66205
 816-221-4488

**PDP SUBMITTAL
 50TH & RAINBOW DEVELOPMENT
 WESTWOOD, KS 66205
 EXISTING FEATURES AND
 TREE PRESERVATION PLAN**

Design: ADM | Drawn: ADM
 Checked: ROL
 Issue Date: 07/20/2023
 Project Number: 037920

L1.0

Sep. 07, 2023 - 9:27am Plotted By: angela.moyer V:\037920-Westwood\04-DWG\Eng\Sheet\037920-SHTS-LNSC.dwg Layout: Landscape Plan-Ground Level



PLANT SCHEDULE

TREES	QTY	BOTANICAL / COMMON NAME	CONT	CAL	HEIGHT
	7	Celtis occidentalis 'Prairie Pride' / Prairie Pride Hackberry	B & B	2"	cal
	3	Ginkgo biloba 'PNI 2720' / Princeton Sentry® Maidenhair Tree	B & B	2"	cal
	5	Gymnocladus dioica 'Espresso' / Kentucky Coffeetree	B & B	2"	cal
	3	Nyssa sylvatica 'Wildfire' / Wildfire Tupelo	B & B	2"	cal
	5	Quercus imbricaria / Shingle Oak	B & B	2.5"	cal
	9	Zelkova serrata 'JFS-KW1' / City Sprite® Japanese Zelkova	B & B	2"	cal
EVERGREEN TREES	QTY	BOTANICAL / COMMON NAME	CONT	CAL	HEIGHT
	10	Pinus thunbergii / Japanese Black Pine	B & B		6' - 8' H
ORNAMENTAL TREES	QTY	BOTANICAL / COMMON NAME	CONT	CAL	HEIGHT
	15	Amelanchier x grandiflora 'Autumn Brilliance' / Autumn Brilliance Apple Serviceberry	B & B	1.5"	cal
	3	Chionanthus virginicus 'Dirr' / Dirr White Fringetree	B & B	1.5"	cal
EVERGREEN SHRUBS	QTY	BOTANICAL / COMMON NAME	SPEC	HEIGHT	
	18	Juniperus chinensis 'Sea Green' / Sea Green Juniper	5 gal		
PERENNIALS	QTY	BOTANICAL / COMMON NAME	SPEC	HEIGHT	
	28	Amsonia hubrichtii / Arkansas Bluestar	1 gal		
	18	Baptisia x 'Purple Smoke' / Purple Smoke Wild Indigo	1 gal		
	136	Bouteloua gracilis 'Blonde Ambition' / Blonde Ambition Blue Grama	1 gal		
	59	Heuchera x 'Plum Pudding' / Plum Pudding Coral Bells	1 gal		
	184	Muhlenbergia capillaris / Pink Muhly Grass	1 gal		
SHRUB AREAS	QTY	BOTANICAL / COMMON NAME	CONT		
	100	Asclepias tuberosa / Butterfly Milkweed	quart		
	27	Carex grayi / Gray's Sedge	quart		
	199	Carex muskingumensis / Palm Sedge	quart		
	107	Chelone obliqua 'Arntipp02' / Tiny Tortuga Turtlehead	quart		
GROUND COVERS	QTY	BOTANICAL / COMMON NAME	SPEC		
	42,903 sf	Festuca arundinacea 'Watersaver Blend' / Watersaving Blend of Tall Fescue	sod		

MULCH SCHEDULE

	WOOD MULCH Double ground hardwood mulch. 3" depth.	6,980 sf
	ROCK MULCH 3" depth.	1,158 sf

SITE DATA

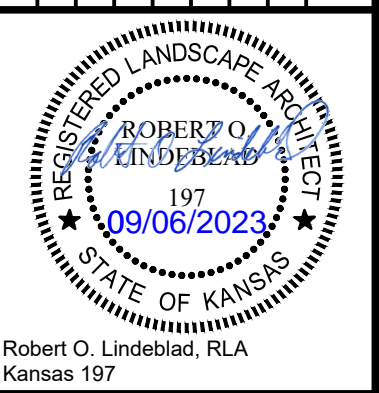
	Quantity	Required	Existing	Provided	Total
Transitional Buffer					
West 50th Street: 1 tree / 20LF	325	16.25	1	16	17
West 51st Street: 1 tree / 20LF	289	14.45	1	14	15
Perimeter Parking Landscape					
continuous screen of 3' shrubs		Y		Y	
Interior Parking Landscape					
surface lot: 1 tree / 8 spaces	42	5.25	4	3	7*

*called out on plan as "parking lot shade trees"

LEGEND

- 980 PROPOSED FINISH GRADE MAJOR CONTOUR
- 982 PROPOSED FINISH GRADE MINOR CONTOUR
- 980 EXISTING GRADE MAJOR CONTOUR
- 982 EXISTING GRADE MINOR CONTOUR
- PROPOSED STORM SEWER LINE
- RD PROPOSED ROOF LINE DRAIN
- PROPERTY LINE
- R/W RIGHT-OF-WAY LINE
- TP TREE PROTECTION
- EXISTING TREE

ENTIRE SHEET
UPDATED



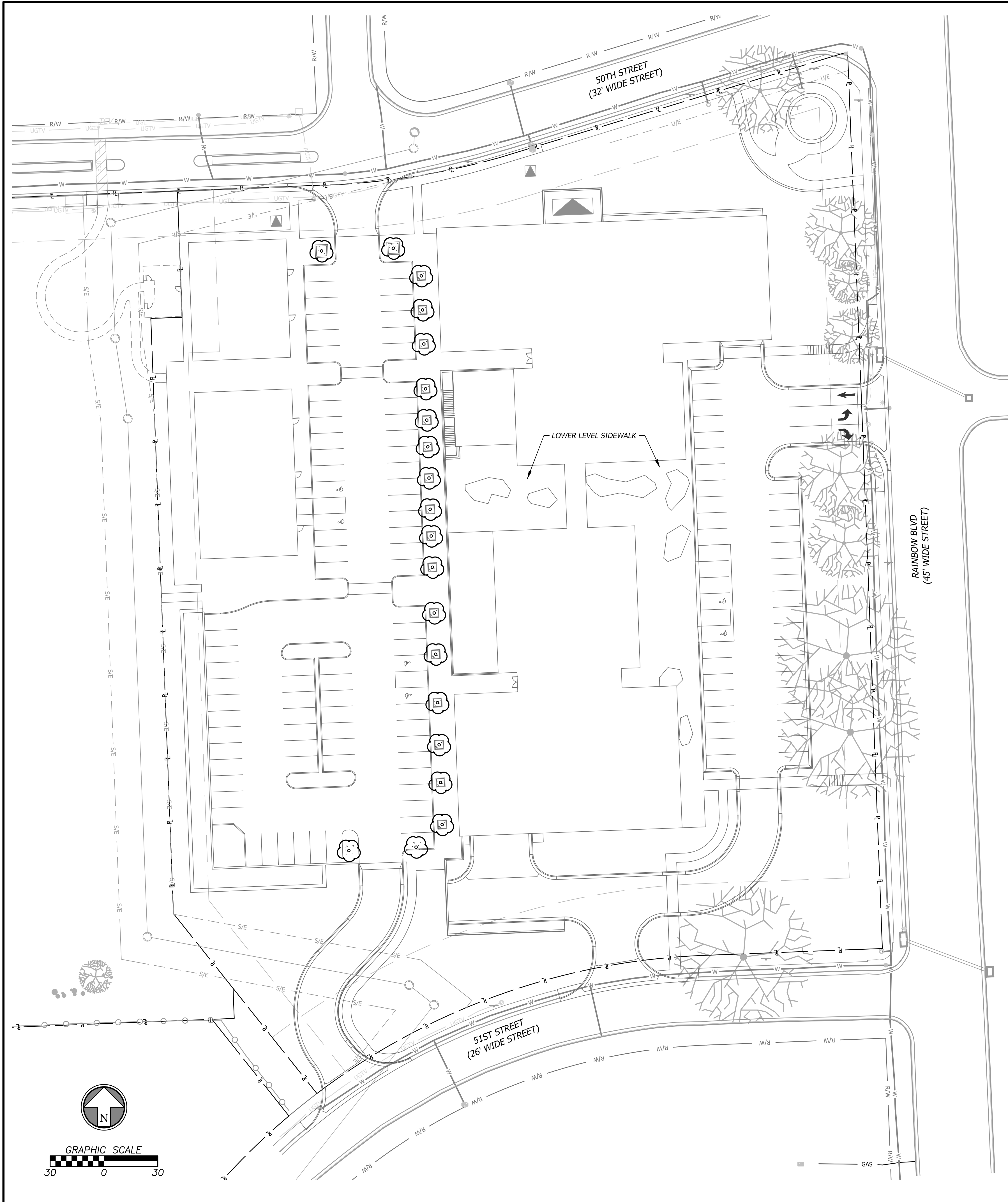
Prepared For:
KARBANK REAL ESTATE COMPANY
 ADAM FELDMAN
 20000 SHY PARKWAY, SUITE 400
 MISSIONS WOODS, KS 66205
 816-221-4488

PDP SUBMITTAL
50TH & RAINBOW DEVELOPMENT
WESTWOOD, KS 66205
LANDSCAPE PLAN-GROUND LEVEL

Design: ADM | Drawn: ADM
 Checked: ADM
 Issue Date: 07/20/2023
 Project Number: 037920

1.1

Sep. 07, 2023 - 9:27am Plotted By: angela.moyer V:\037920-Westwood\04-DWG\Eng\Sheet\037920-SHFS-LNSC.dwg Layout: Landscape Plan-Second Level



PLANT SCHEDULE

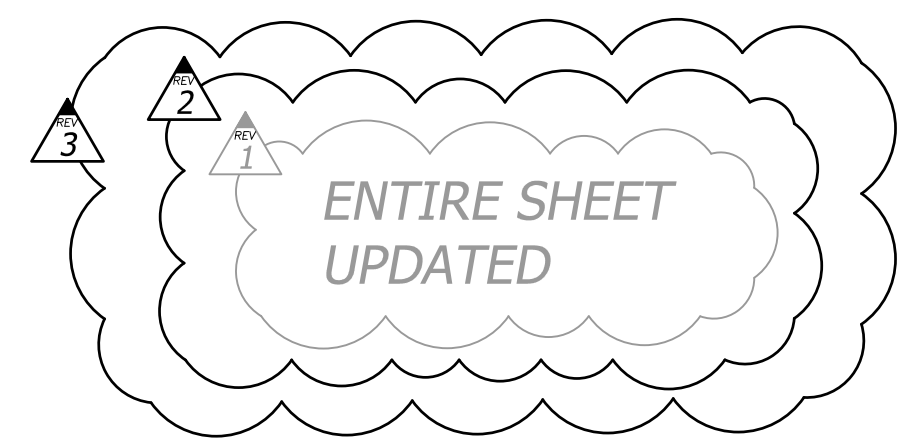
ORNAMENTAL TREES	QTY	BOTANICAL / COMMON NAME	CONT	CAL
	20	Malus x 'Royal Raindrops' / Royal Raindrops Crabapple	B & B	1.5" cal

MULCH SCHEDULE

	WOOD MULCH Double ground hardwood mulch. 3" depth.	429 sf
--	---	--------

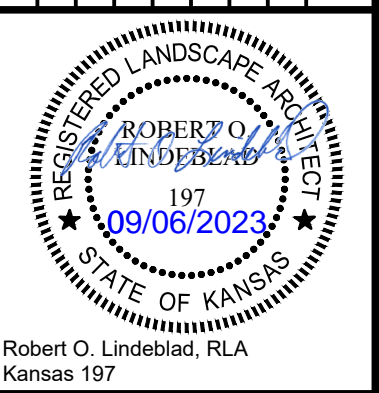
LEGEND

	980	PROPOSED FINISH GRADE MAJOR CONTOUR
	982	PROPOSED FINISH GRADE MINOR CONTOUR
	980	EXISTING GRADE MAJOR CONTOUR
	982	EXISTING GRADE MINOR CONTOUR
		PROPOSED STORM SEWER LINE
	RD	PROPOSED ROOF LINE DRAIN
	R	PROPERTY LINE
	R/W	RIGHT-OF-WAY LINE
	Tp	TREE PROTECTION



GENERAL LANDSCAPE NOTES

- The Contractor shall verify and coordinate all final grades with the Landscape Architect and or design team prior to completion.
- Location and placement of all plant material shall be coordinated with the Landscape Architect prior to installation.
- Location of all utilities are approximate, the Contractor shall field verify locations prior to commencement of construction operations.
- Refer to Civil Drawings for all grading and berming, erosion control, storm drainage, utilities and site layout.
- The Contractor shall arrange and conduct a pre-construction meeting onsite with Landscape Architect prior to work.
- Plant quantities are for information only, drawing shall prevail if conflict occurs. Contractor is responsible for calculating own quantities and bid accordingly. Minimum quantities for each category of planting required by City Code must be maintained.
- The Contractor is to notify Landscape Architect after staking is complete and before plant pits are excavated.
- Tree locations in areas adjacent to drives, walks, walls and light fixtures may be field adjusted as approved by Landscape Architect.
- The Contractor shall report subsurface soil or drainage problems to the Landscape Architect.
- The plan is subject to changes based on plant size and material availability. All changes or substitutions must be approved by the City of Westwood, Kansas and the Landscape Architect.
- Aluminum landscape edging to be used on all landscape beds adjoining turf areas as noted on landscape plans. Edging shall not be used between pavement and landscape beds.
- Landscape Contractor shall be responsible for watering all plant material until the time that a permanent water source is ready.
- The Contractor shall provide a submittal to show proof of procurement, sources, quantities, and varieties for all shrubs, perennials, ornamental grasses, and annuals within 21 days following the award of the contract.
- Contractor shall provide full maintenance for newly landscaped areas for a period of 30 days after the date of final acceptance. At the end of the maintenance period, a healthy, well-rooted, even-colored, viable turf and landscaped area must be established. The landscaped areas shall be free of weeds, open joints, bare areas, and surface irregularities.
- Landscape Contractor shall provide rock mulch sample to owner for approval.



Prepared For:
 KARBANK REAL ESTATE COMPANY
 ADAM FELDMAN
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 MISSIONS WOODS, KS 66205
 816-221-4488

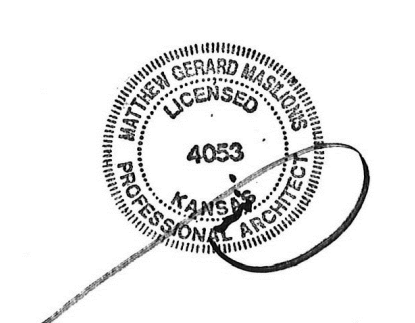
PDP SUBMITTAL
50TH & RAINBOW DEVELOPMENT
WESTWOOD, KS 66205
LANDSCAPE PLAN-SECOND LEVEL

Design: ADM | Drawn: ADM
 Checked: ADM
 Issue Date: 07/20/2023
 Project Number: 037920

L1.2

Perspective

2000 SHAWNEE MISSION PKWY
SUITE 100
MISSION WOODS, KS 66205
816 502 1500
WWW.PAD.STUDIO



MATTHEW MASILIONIS - ARCHITECT
KANSAS # 4053

I have prepared the drawings and assume responsibility for the sheets numbered with an "A" prefix for the project named below.

Other drawings and specifications attached for the above-mentioned project have been by and are the responsibility of the licensed engineer whose stamp and firm appear on that sheet.

The Architect is not responsible for the design of the mechanical, electrical, plumbing, civil, landscaping, structural, signage (not specified), fire sprinkler or fire suppression systems, and does not take responsibility for the compliance of these areas with the laws of the above governmental entities. The architect is not responsible for materials, components or equipment, as well as the method in which they are installed on the project by others. The architect is not responsible for proper operation due to faulty installation or product failure during construction or after completion of construction when operation has begun by the landlord or tenant.

The licensed professional whose stamp appears on sheets other than those specifically noted above shall be responsible for those items in paragraph three.

CONSULTANT



PROJECT 50TH AND RAINBOW DEVELOPMENT

50TH ST. & RAINBOW BLVD.
WESTWOOD KS, 66205

MARK	DATE	DESCRIPTION
2	08/25/23	PDP Update
3	09/06/23	CITY COMMENTS

SHEET TITLE

ARCHITECTURAL SITE PLAN

PROJECT NUMBER
2021073.000

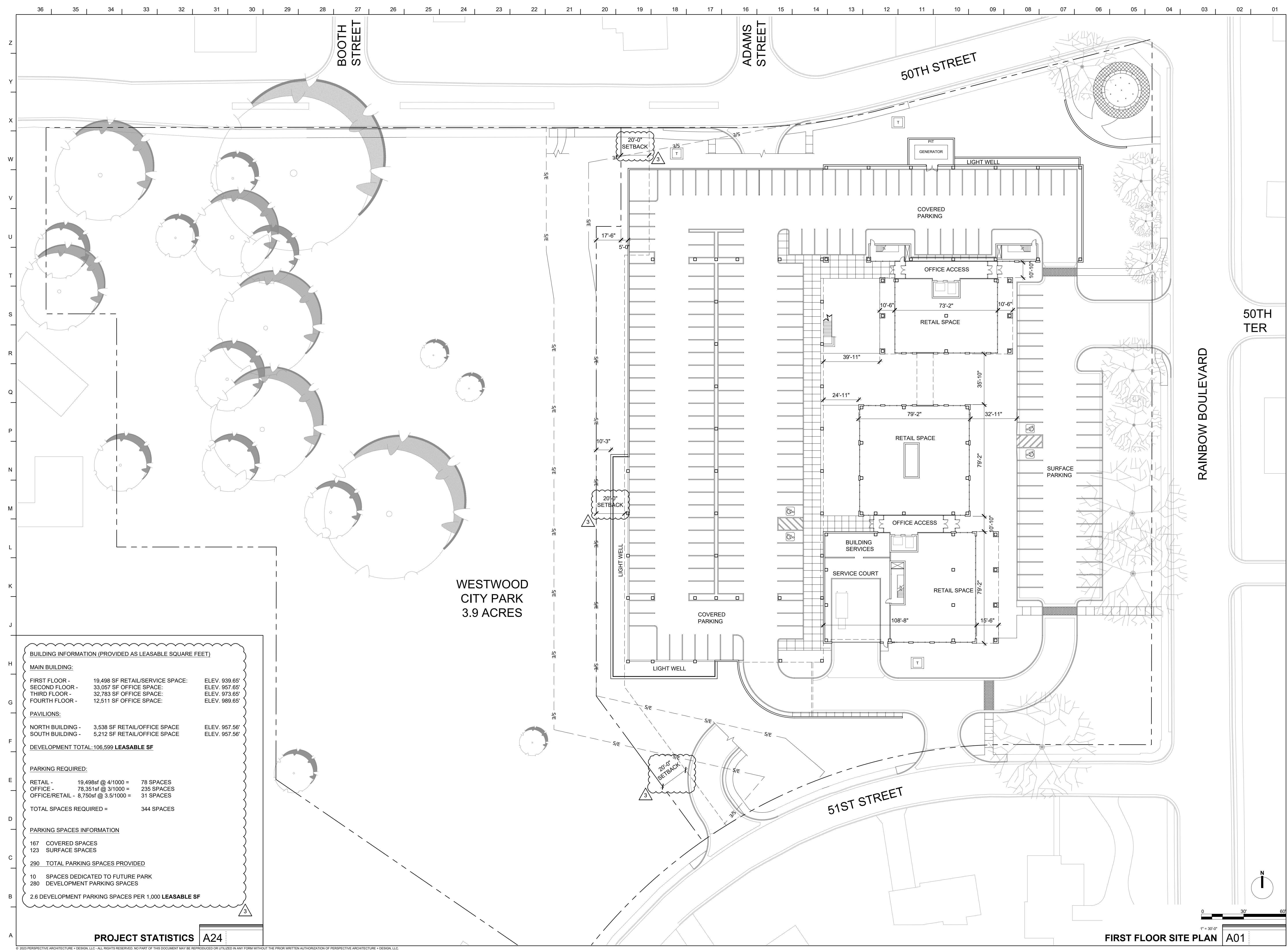
SHEET AUTHOR
K. MILLER

CHECKED BY
M. PAXTON

DATE
7.20.23

PDP SUBMITTAL

SHEET NUMBER
AS101



BUILDING INFORMATION (PROVIDED AS LEASABLE SQUARE FEET)

MAIN BUILDING:

FIRST FLOOR -	19,498 SF RETAIL/SERVICE SPACE:	ELEV. 939.65'
SECOND FLOOR -	33,057 SF OFFICE SPACE:	ELEV. 957.65'
THIRD FLOOR -	32,783 SF OFFICE SPACE:	ELEV. 973.65'
FOURTH FLOOR -	12,511 SF OFFICE SPACE:	ELEV. 989.65'

PAVILIONS:

NORTH BUILDING -	3,538 SF RETAIL/OFFICE SPACE	ELEV. 957.56'
SOUTH BUILDING -	5,212 SF RETAIL/OFFICE SPACE	ELEV. 957.56'

DEVELOPMENT TOTAL: 106,599 LEASABLE SF

PARKING REQUIRED:

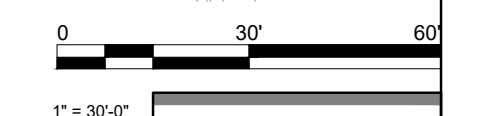
RETAIL -	19,498sf @ 4/1000 =	78 SPACES
OFFICE -	78,351sf @ 3/1000 =	235 SPACES
OFFICE/RETAIL -	8,750sf @ 3.5/1000 =	31 SPACES
TOTAL SPACES REQUIRED =		344 SPACES

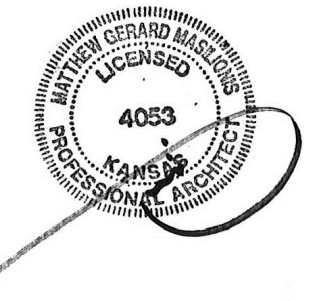
PARKING SPACES INFORMATION

167 COVERED SPACES
123 SURFACE SPACES
290 TOTAL PARKING SPACES PROVIDED
10 SPACES DEDICATED TO FUTURE PARK
280 DEVELOPMENT PARKING SPACES
2.6 DEVELOPMENT PARKING SPACES PER 1,000 LEASABLE SF

PROJECT STATISTICS A24

FIRST FLOOR SITE PLAN A01





MATTHEW MASILIONIS - ARCHITECT
 KANSAS # 4053

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PROJECT
50TH AND RAINBOW DEVELOPMENT

50TH ST. & RAINBOW BLVD.
 WESTWOOD KS, 66205

MARK	DATE	DESCRIPTION
2	08/25/23	PDP Update
3	09/06/23	CITY COMMENTS

SHEET TITLE

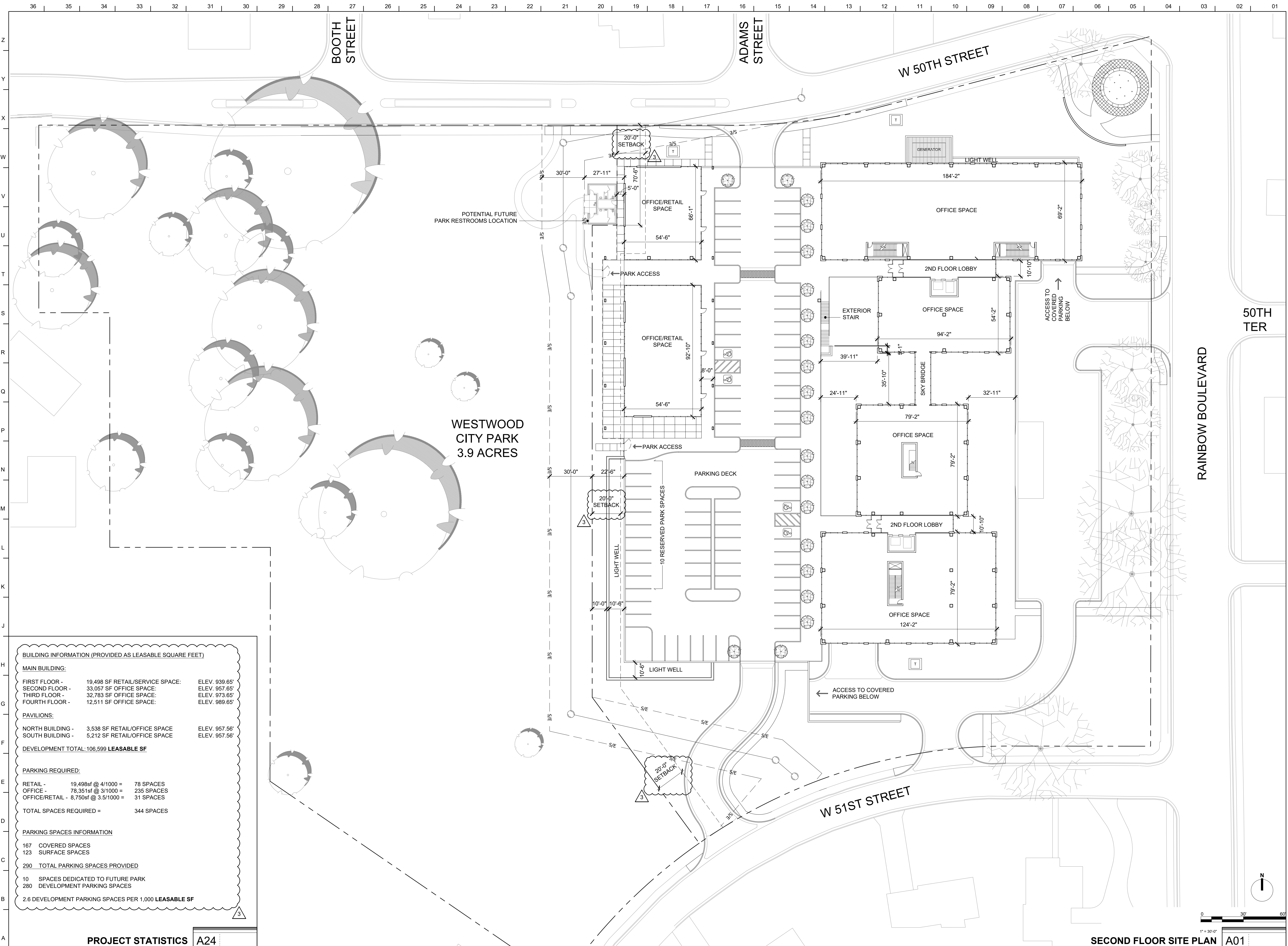
ARCHITECTURAL
 SITE PLAN

PROJECT NUMBER
 2021073.000
 SHEET AUTHOR
 K. MILLER

CHECKED BY
 M. PAXTON
 DATE
 7.20.23

PDP SUBMITTAL

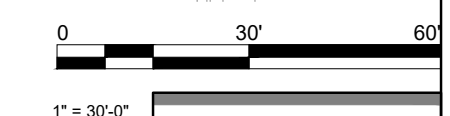
SHEET NUMBER
AS102



BUILDING INFORMATION (PROVIDED AS LEASABLE SQUARE FEET)		
MAIN BUILDING:		
FIRST FLOOR -	19,498 SF RETAIL/SERVICE SPACE:	ELEV. 939.65'
SECOND FLOOR -	33,057 SF OFFICE SPACE:	ELEV. 957.65'
THIRD FLOOR -	32,783 SF OFFICE SPACE:	ELEV. 973.65'
FOURTH FLOOR -	12,511 SF OFFICE SPACE:	ELEV. 989.65'
PAVILIONS:		
NORTH BUILDING -	3,538 SF RETAIL/OFFICE SPACE	ELEV. 957.56'
SOUTH BUILDING -	5,212 SF RETAIL/OFFICE SPACE	ELEV. 957.56'
DEVELOPMENT TOTAL: 106,599 LEASABLE SF		
PARKING REQUIRED:		
RETAIL -	19,498sf @ 4/1000 =	78 SPACES
OFFICE -	78,351sf @ 3/1000 =	235 SPACES
OFFICE/RETAIL -	8,750sf @ 3.5/1000 =	31 SPACES
TOTAL SPACES REQUIRED = 344 SPACES		
PARKING SPACES INFORMATION		
167	COVERED SPACES	
123	SURFACE SPACES	
290 TOTAL PARKING SPACES PROVIDED		
10	SPACES DEDICATED TO FUTURE PARK	
280	DEVELOPMENT PARKING SPACES	
2.6 DEVELOPMENT PARKING SPACES PER 1,000 LEASABLE SF		

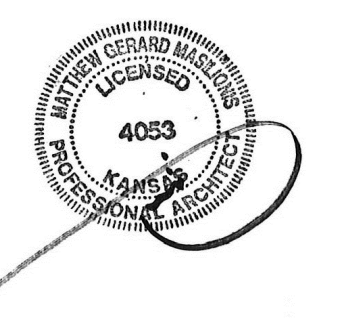
PROJECT STATISTICS **A24**

SECOND FLOOR SITE PLAN **A01**



Perspective

2000 SHAWNEE MISSION PKWY
SUITE 100
MISSION WOODS, KS 66205
816 502 1500
WWW.PAD.STUDIO



MATTHEW MASILIONIS - ARCHITECT
KANSAS # 4053

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CONSULTANT



PROJECT
50TH AND RAINBOW DEVELOPMENT

50TH ST. & RAINBOW BLVD.
WESTWOOD KS, 66205

MARK	DATE	DESCRIPTION
2	08/25/23	PDP Update
3	09/06/23	CITY COMMENTS

SHEET TITLE

ARCHITECTURAL SITE PLAN

PROJECT NUMBER
2021073.000

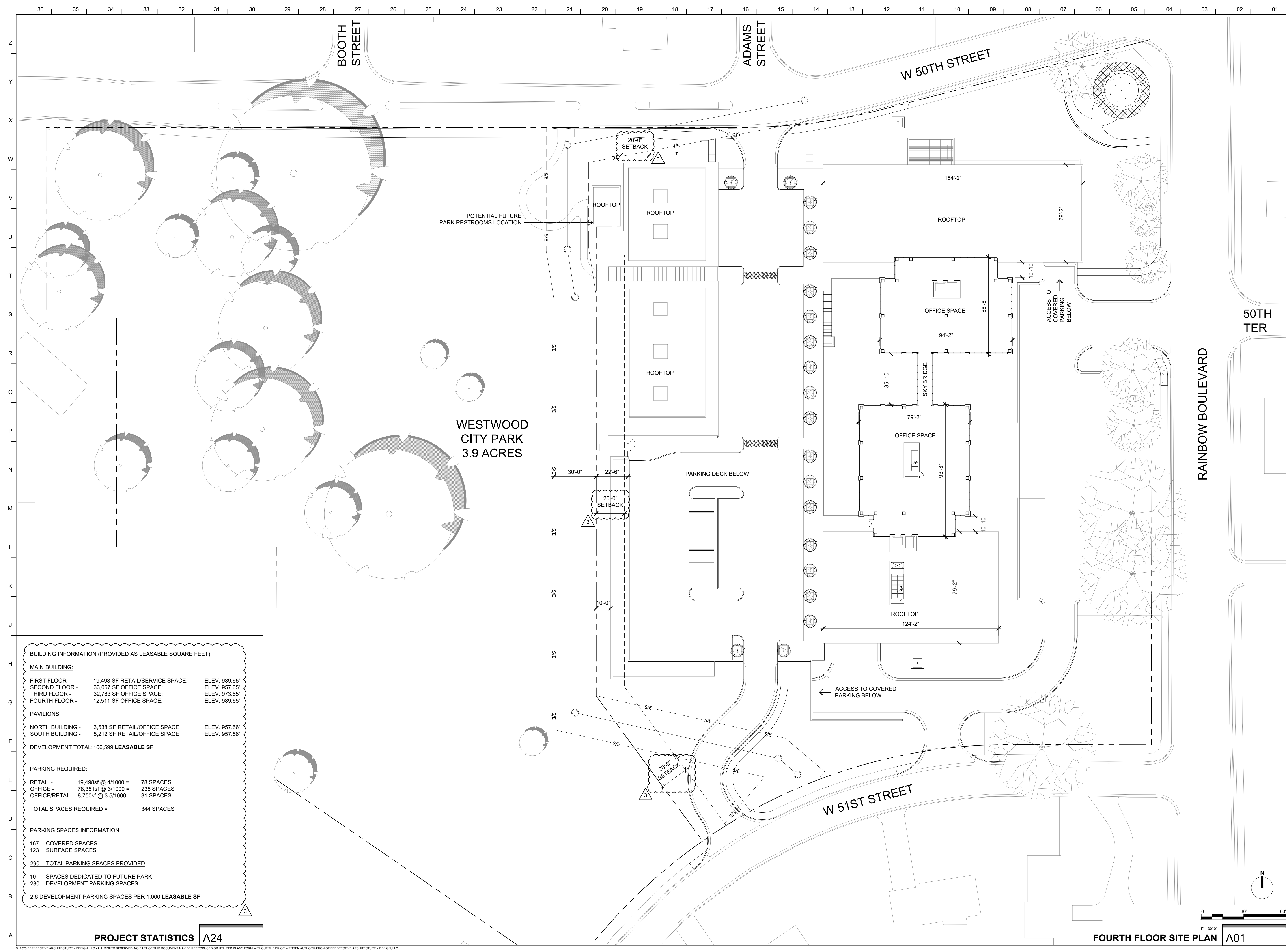
SHEET AUTHOR
K. MILLER

CHECKED BY
M. PAXTON

DATE
7.20.23

PDP SUBMITTAL

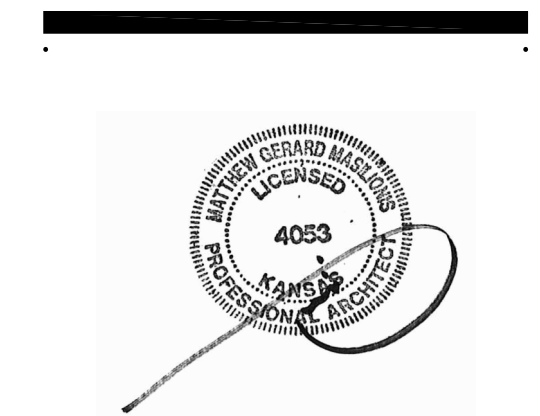
SHEET NUMBER
AS103



BUILDING INFORMATION (PROVIDED AS LEASABLE SQUARE FEET)		
MAIN BUILDING:		
FIRST FLOOR -	19,498 SF RETAIL/SERVICE SPACE:	ELEV. 939.65'
SECOND FLOOR -	33,057 SF OFFICE SPACE:	ELEV. 957.65'
THIRD FLOOR -	32,783 SF OFFICE SPACE:	ELEV. 973.65'
FOURTH FLOOR -	12,511 SF OFFICE SPACE:	ELEV. 989.65'
PAVILIONS:		
NORTH BUILDING -	3,538 SF RETAIL/OFFICE SPACE	ELEV. 957.56'
SOUTH BUILDING -	5,212 SF RETAIL/OFFICE SPACE	ELEV. 957.56'
DEVELOPMENT TOTAL: 106,599 LEASABLE SF		
PARKING REQUIRED:		
RETAIL -	19,498sf @ 4/1000 =	78 SPACES
OFFICE -	78,351sf @ 3/1000 =	235 SPACES
OFFICE/RETAIL -	8,750sf @ 3.5/1000 =	31 SPACES
TOTAL SPACES REQUIRED = 344 SPACES		
PARKING SPACES INFORMATION		
167	COVERED SPACES	
123	SURFACE SPACES	
290	TOTAL PARKING SPACES PROVIDED	
10	SPACES DEDICATED TO FUTURE PARK	
280	DEVELOPMENT PARKING SPACES	
2.6 DEVELOPMENT PARKING SPACES PER 1,000 LEASABLE SF		

PROJECT STATISTICS A24

FOURTH FLOOR SITE PLAN A01



MATTHEW MASILIONIS - ARCHITECT
KANSAS # 4053

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CONSULTANT



PROJECT
50TH AND RAINBOW DEVELOPMENT

50TH ST. & RAINBOW BLVD.
WESTWOOD KS, 66205

MARK	DATE	DESCRIPTION
2	8.25.23	PDP UPDATE
3	9.06.23	CITY COMMENTS

SHEET TITLE

ELEVATIONS

PROJECT NUMBER
2021073.000
SHEET AUTHOR
K. MILLER
CHECKED BY
M. PAXTON
DATE
7.20.23

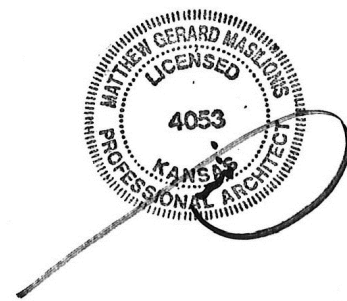
PDP SUBMITTAL

SHEET NUMBER
A201



1/16" = 1'-0"
MAIN BUILDING - WEST ELEVATION N01

1/16" = 1'-0"
MAIN BUILDING - EAST ELEVATION A01



MATTHEW MASILIONIS - ARCHITECT
KANSAS # 4053

I have prepared the drawings and assume responsibility for the sheets numbered with an "X" prefix for the project named below.

Other drawings and specifications attached for the above-mentioned project have been by and are the responsibility of the licensed engineer whose stamp and firm appear on this sheet.

The Architect is not responsible for the design of the mechanical, electrical, plumbing, civil, landscaping, structural, signage (not specified), fire sprinkler or fire suppression systems, and does not take responsibility for the compliance of these areas with the laws of the above governmental entities. The architect is not responsible for materials, components or equipment, as well as the method in which they are installed on the project by others. The architect is not hired or responsible for certification, during construction or upon completion of construction. The architect is not responsible for employer operation due to faulty installation or product failure during construction or after completion of construction when operation has begun by the landlord or tenant.

The licensed professional whose stamp appears on sheets other than those specifically noted above shall be responsible for those items in paragraph three.

CONSULTANT



PROJECT

50TH AND RAINBOW DEVELOPMENT

50TH ST. & RAINBOW BLVD.
WESTWOOD KS, 66205

MARK	DATE	DESCRIPTION
△	8.25.23	PDP UPDATE

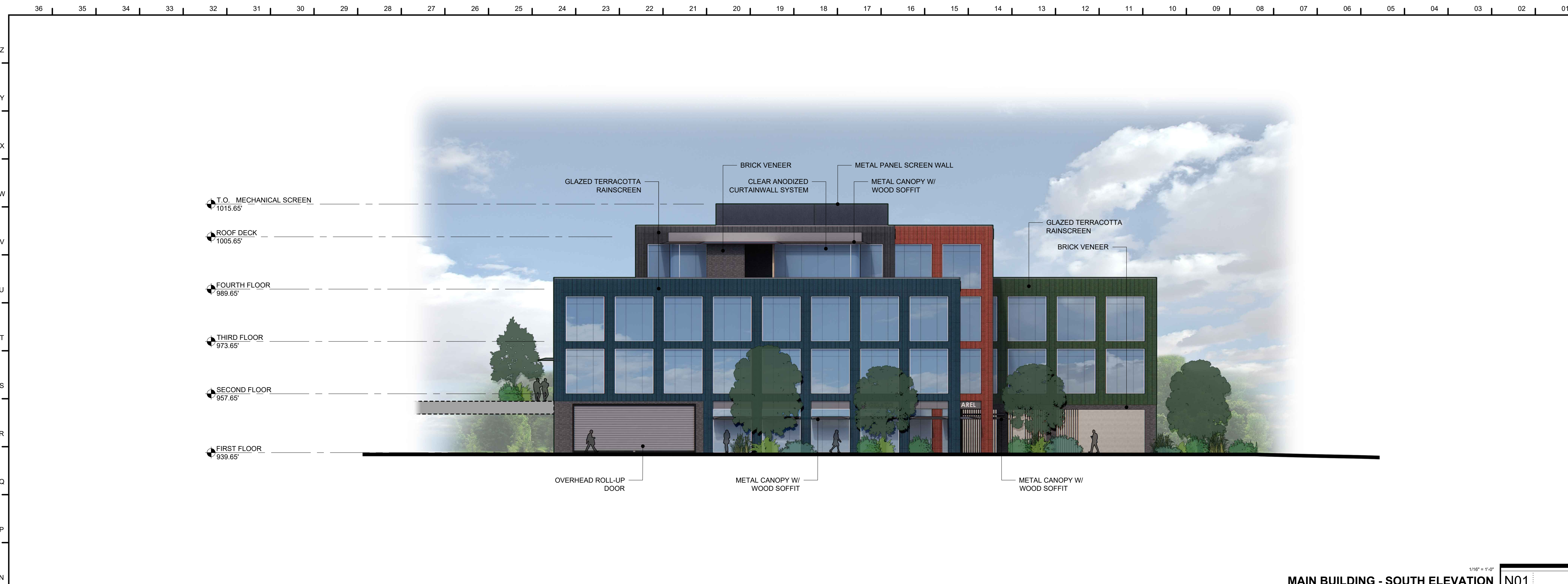
SHEET TITLE

ELEVATIONS

PROJECT NUMBER
2021073.000
SHEET AUTHOR
K. MILLER
CHECKED BY
M. PAXTON
DATE
7.20.23

PDP SUBMITTAL

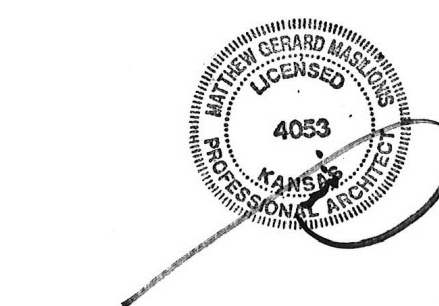
SHEET NUMBER
A202



1/16" = 1'-0"
MAIN BUILDING - SOUTH ELEVATION N01



1/16" = 1'-0"
MAIN BUILDING - NORTH ELEVATION A01



MATTHEW MASILIONIS - ARCHITECT
KANSAS # 4053

I have prepared the drawings and assume responsibility for the sheets numbered with an "X" prefix for the project named below.

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PROJECT

50TH AND RAINBOW DEVELOPMENT

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WESTWOOD KS, 66205

MARK	DATE	DESCRIPTION
	8.25.23	PDP UPDATE

SHEET TITLE

ELEVATIONS

PROJECT NUMBER

2021073.000

SHEET AUTHOR

K. MILLER

CHECKED BY

M. PAXTON

DATE

7.20.23

PDP SUBMITTAL

SHEET NUMBER

A203





August 15, 2023

Leslie Herring
City Administrator
City of Westwood, KS

Re: Traffic Impact Study for 50th and Rainbow Development

BHC has been asked to review the traffic impact of a proposed redevelopment located in the southwest corner of 50th Street and Rainbow Boulevard. The site includes Joe D. Dennis Park and the former Westwood View Elementary School.

Westwood View Elementary School operations have relocated to the northeast corner of 50th Street and Belinder Avenue, approximately 500 feet west of this site. For the 2023-2024 school year, Rushton Elementary School operations will utilize the original Westwood View Elementary School while their school is being rebuilt. After the school year, the proposed development would replace the site for a proposed mixed-use site consisting of 98,750 square feet of general office buildings and 36,300 square feet of retail.

This traffic study provides existing traffic counts, a traffic distribution, trip generation and intersection capacity/queuing analyses for the proposed development for the AM and PM peak hour traffic volumes. The traffic data was collected in the summer without school in session.

A follow-up Traffic Study Addendum will be provided in September after school traffic patterns have settled and new counts can be obtained. That information will be used to update the existing+proposed conditions of this study, as well as a future condition where a 0.5% annual traffic growth rate is applied for a 20-year period.

EXISTING CONDITIONS

The location currently includes Joe D. Dennis Park and the former Westwood View Elementary School. Rainbow Boulevard (169 Highway) is a 35mph 4-lane road that runs along the eastern side of the site. Rainbow Boulevard provides access to Shawnee Mission Parkway approximately 1000 feet to the south, and I-35 approximately 2.5 miles to the north.

The intersection of Rainbow Boulevard and 50th Street is a signalized 4-leg intersection with 50th Street being offset by approximately 70 feet. 50th Street runs along the northern side of the site and is a 25-mph 2-lane minor collector street connecting Mission Road to State Line Road.

51st Street is a 25-mph 2-lane residential street along the southern side of the site that connects Rainbow Boulevard to 51st Terrace. 51st Street forms a T-intersection with Rainbow Boulevard that is Stop-sign controlled for 51st Street.

The existing street network along with AM and PM traffic counts were taken on Tuesday, July 18th may be seen in Figure 1.



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EXISTING CONDITIONS (continued)

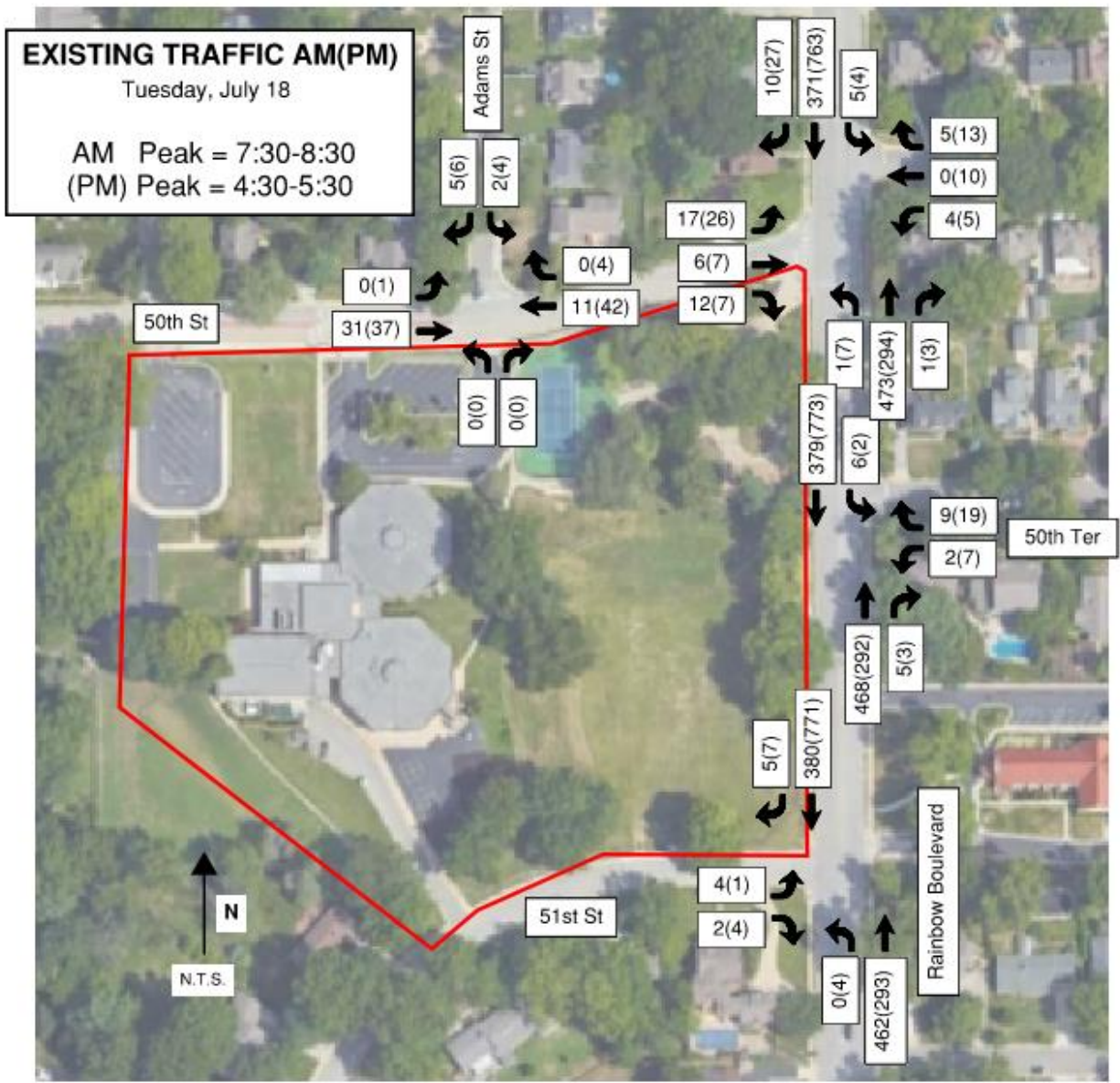


Figure 1: Existing Conditions

PROPOSED CONDITIONS

The proposed mixed-use site will consist of 98,750 square feet of general office building and 36,300 square feet of retail.

Along the eastern side of the site (Rainbow Boulevard), a new access driveway is proposed that would align itself across from 50th Terrace. The access driveway will provide access to the ground level of a parking garage with 215 parking spaces.

Along the northern side of the site (50th Street), a single proposed driveway that aligns with Adams Street would provide access to the second level of the parking garage with 80 parking spaces.

Along the southern side of the site (51st Street), the eastern of two proposed driveways approximately 150 feet west of Rainbow Boulevard will provide a second access point to the ground level of the parking garage. The western proposed driveway will provide a second access point the second level of the parking garage. There is no internal connectivity of the two levels of the parking garage.

The proposed site layout may be seen in Figure 2. Intersection site triangles have been provided on the plans and may also be seen in Figure 2.

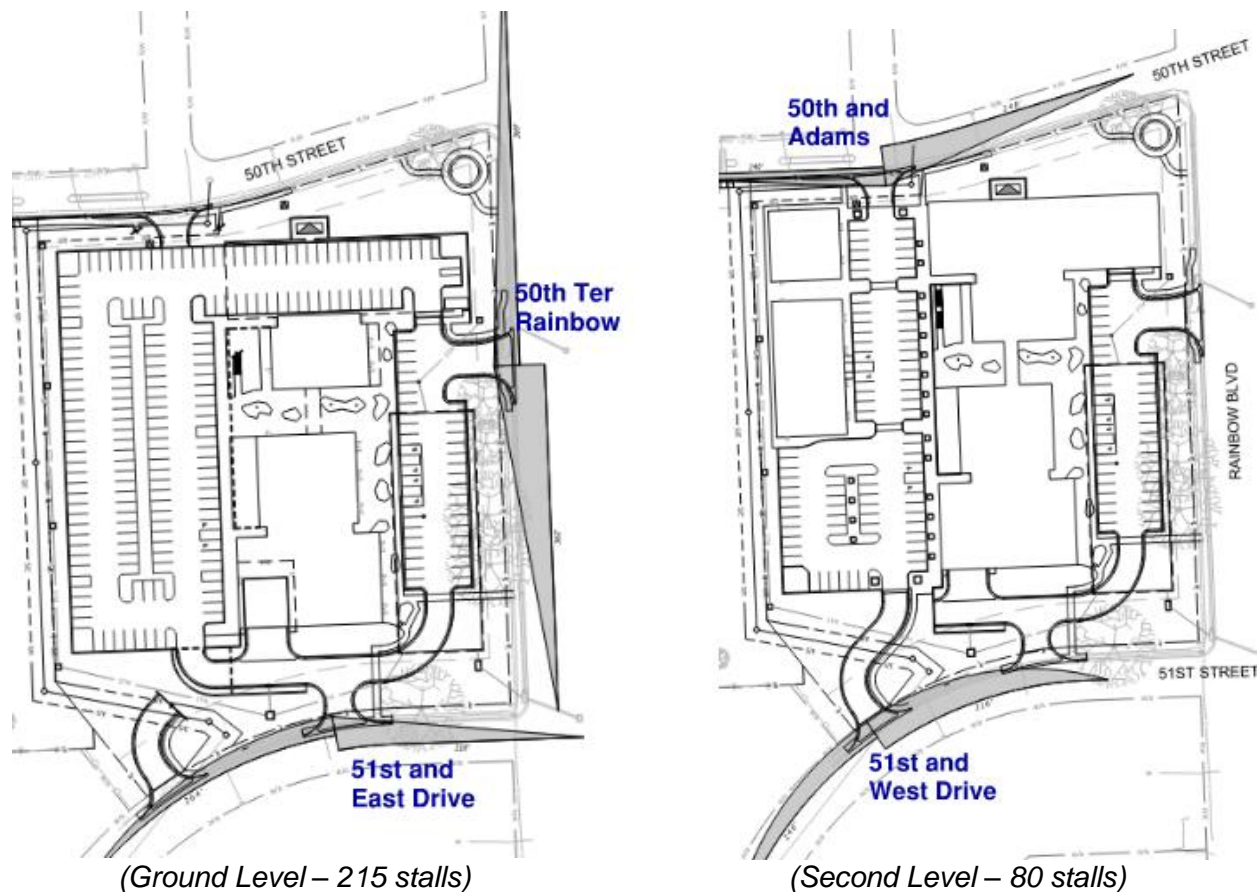


Figure 2: Proposed Site Layout



TRIP DISTRIBUTION

The project is situated within a well-established neighborhood. Rainbow Boulevard is anticipated to carry a larger percentage of the proposed site-related traffic due to the nature of a mixed-use site as opposed to a centrally located community elementary school. It is also assumed that a notable percentage of the retail traffic is expected to be pass-by and/or internal capture trips already on the surrounding roadway network.

The fact that Rushton Elementary will (and has) operated in the former Westwood View Elementary school for the 2023-2024 complicates existing traffic counts, traffic patterns, and the future trip distribution. Traffic distributions for a neighborhood school and a mixed-use development are expected to operate differently. Figure 3 illustrates where Rushton Elementary is relative to the project and where its traffic would be expected to originate.

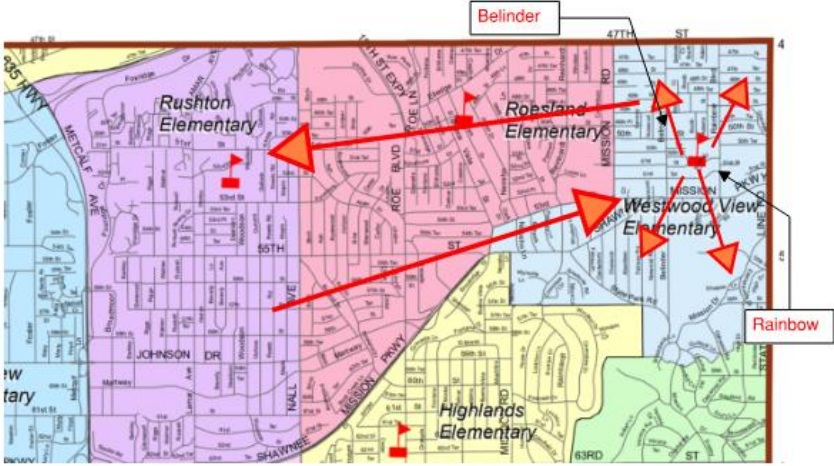


Figure 3: Rushton Elementary Traffic Flow

Figure 4 illustrates where it is anticipated the proposed site office and retail traffic will originate. General office employees would be expected to draw from a much larger population radius within the metropolitan area and less likely to use the residential street networks in their commute. The trip distribution assumptions utilized for distributing the proposed traffic are explained on the following page.

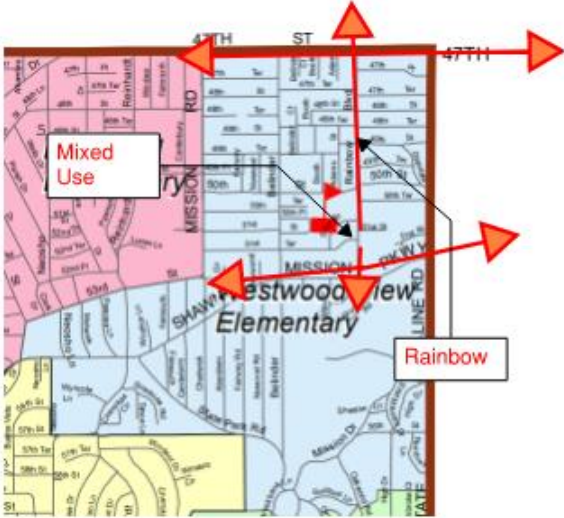


Figure 4: Proposed Mixed-Use Traffic Flow

TRIP DISTRIBUTION (continued)

A review of the surrounding population centers, existing roadway network, and July traffic counts along Rainbow Boulevard was completed to develop a trip distribution. Several assumptions were made for the distribution and are summarized below:

- 1) 20% of site generated traffic will be assigned to filter through the surrounding neighborhoods via 50th Street, 50th Terrace, and 51st Street. While existing traffic counts in the summer do not support this high percentage (no school in session), 20% has been selected to conservatively account for side-street traffic concerns.
- 2) 80% of the site generated traffic will be assigned to Rainbow Boulevard with a 50%/50% northbound/southbound directional split. Consideration was given towards a less balanced directional split as Rainbow Boulevard has a definitive 60%/40% north/southbound directional split that reverses in the AM and PM peak hours. However, the location of City of Westwood and this project relative to the surrounding population centers in the metro has lead us to a 50%/50% split.

Figure 5 illustrates the entering (blue numbers) and exiting (red numbers) trip distribution percentage selected based on these assumptions. The numbers in orange represent the directional distributions from each entering street/direction. The sum of the red numbers leaving the site boundary, as well as the sum of the blue numbers entering the site boundary total 100% and represent where the trip generation numbers will be assigned.



TRIP DISTRIBUTION (continued)

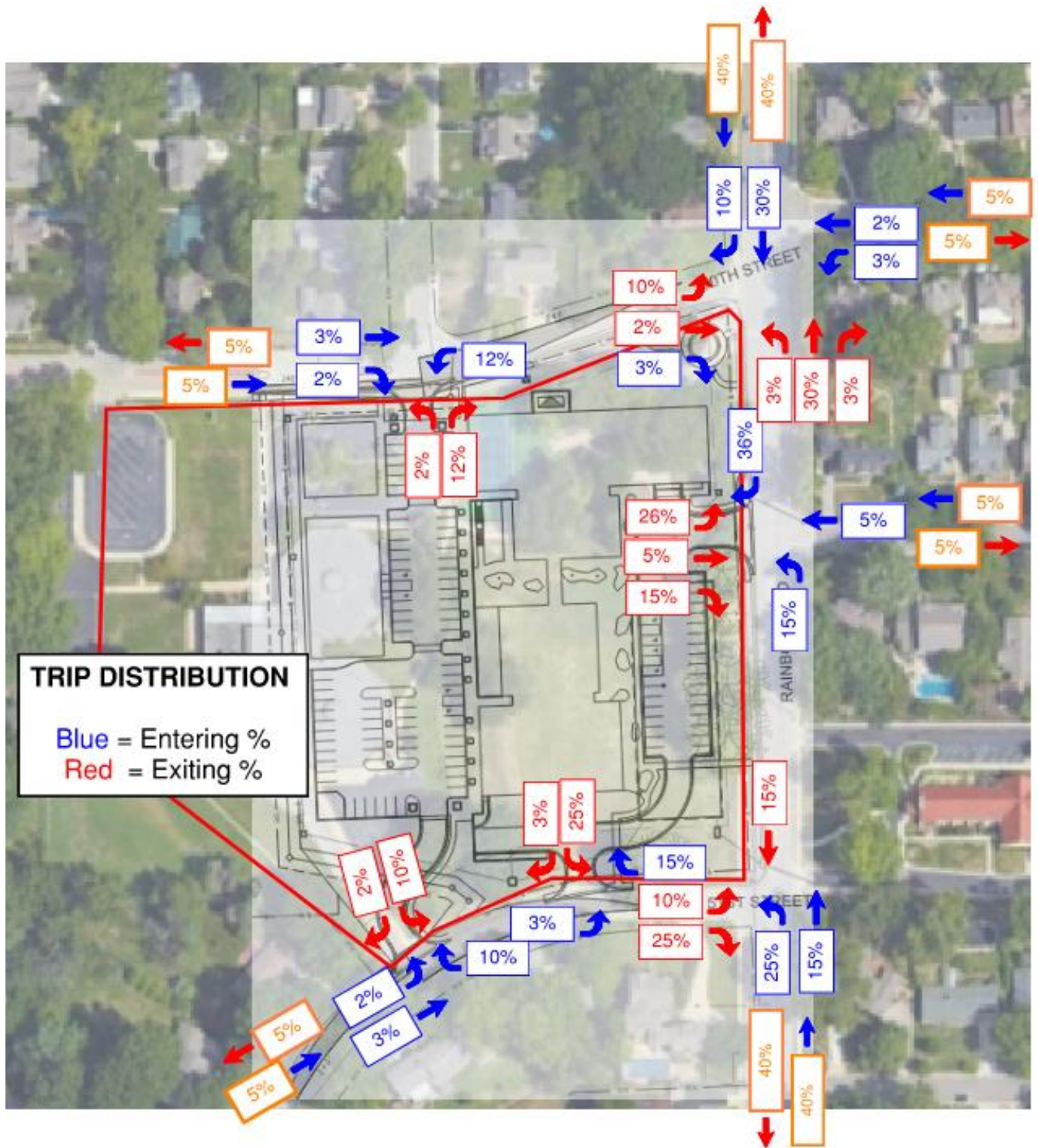


Figure 5: Proposed Trip Distribution

TRIP GENERATION

A trip generation analysis was performed using the Institute of Transportation Engineers (ITE) TripGen web-based app. The 11th edition of the Trip Generation Manual was used. The land use codes used for the proposed site were 710 – General Office Building, and 822 – Strip Retail Plaza.

The ITE Average Rate was used for General Office Building, and the ITE Fitted Curve Equation was used for the Strip Retail Plaza. The fitted curve equation was chosen as a better fit for the ITE data points collected for a Strip Retail Plaza site (the proposed retail is 36,300 square feet which is close to the 40,000 square foot threshold). See ITE Trip Gen plots in Appendix. The number of trips generated may be seen in Table 1 for the AM peak hour, PM peak hour, and weekday total.

Table 1 – Trip Generation						
ITE Code	Land Use	1000 SF	Avg. Rate	Trips Generated		
				Total	Enter	Exit
AM Peak Hour (7-9 AM)						
710	General Office Building	98.75	1.52	150	132	18
822	Strip Retail Plaza (<40k)	36.3	2.36*	67	40	27
Total AM Peak Hour				217	172	45
PM Peak Hour (4-6 PM)						
710	General Office Building	98.75	1.44	142	24	118
822	Strip Retail Plaza (<40k)	36.3	6.59*	194	97	97
Total PM Peak Hour				336	121	215
Weekday Total						
710	General Office Building	98.75	10.84	1070	535	535
822	Strip Retail Plaza (<40k)	36.3	54.45*	1762	881	881
Total Weekday				2832	1416	1416

* ITE Average Rate shown, ITE Fitted Curve Equation used for Strip Retail Plaza <40k

Pass-By Assumption

Not all traffic entering or exiting a site driveway is necessarily new traffic added to the roadway network. The actual amount of new traffic is dependent upon the purpose of the trip and route used from its origin to its destination. For example, retail-oriented developments such as shopping centers, restaurants, service stations, and convenience markets are often located adjacent to busy roads with the intent of attracting motorists already on the roadway network. These developments attract a portion of their trips from existing traffic passing the site. Thus, these “pass-by” trips do not add new traffic and may be reduced from the total external trips generated by a study site.

Considering the proposed Strip Retail Plaza land use, an average pass-by percentage reduction of 30% is an acceptable practice. ITE indicates that the average pass-by rate for a Shopping Plaza is 40%. This study will stay conservative by not using any pass-by percentage which overestimates the mixed-use traffic generation lowering intersection levels of service. If a pass-by of 30% were applied to the retail plaza this study would decrease those trips by 30%.



TRIP GENERATION (continued)

Figure 6 illustrates the Trip Generations provided in Table 1 and distributes them to the proposed site and surrounding street network to the percentages provided in Figure 5.

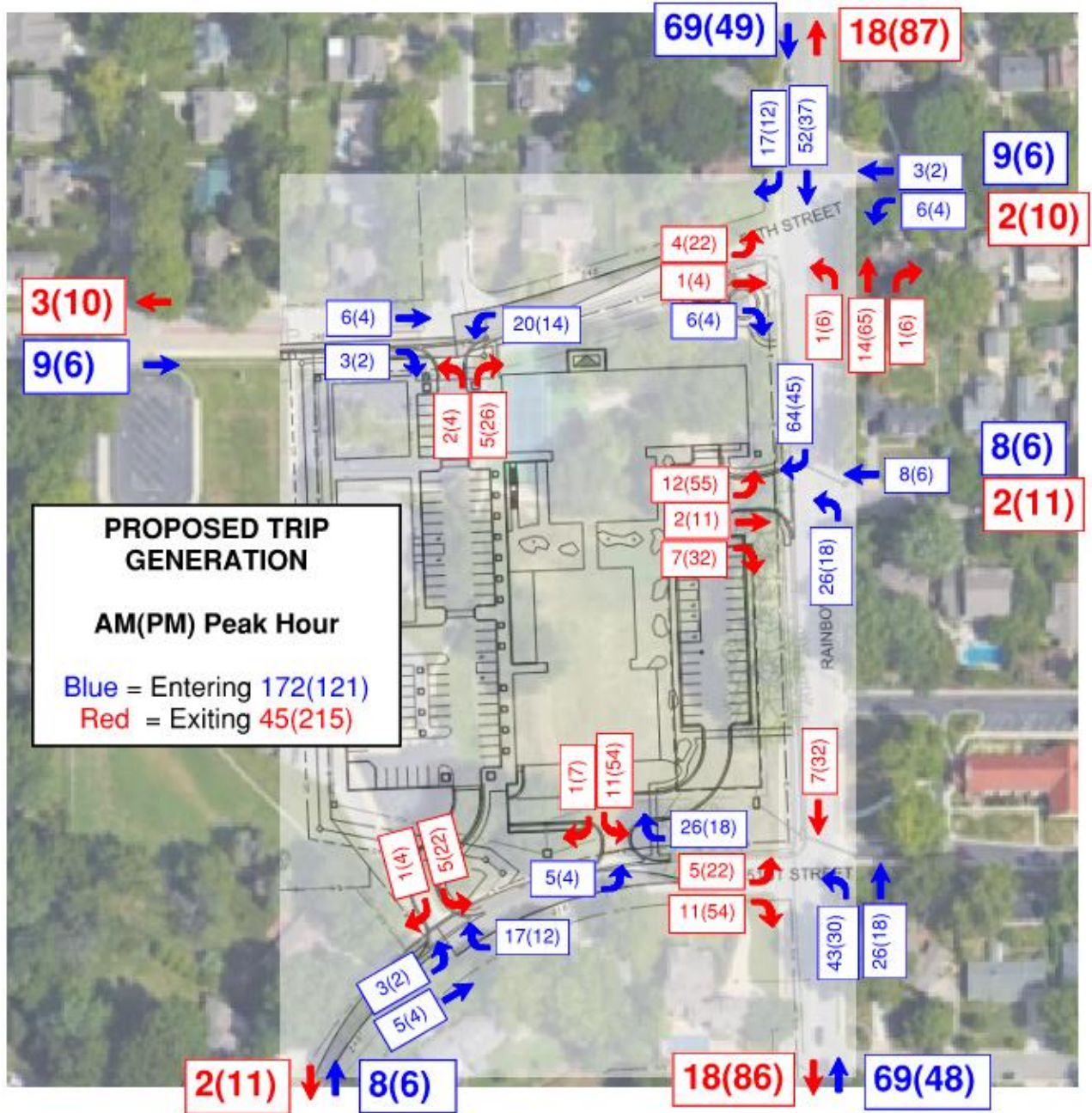


Figure 6: Proposed Trip Generation

TRIP GENERATION (continued)

Figure 6 represents the peak hour traffic increases associated with the site and the trip distribution assumptions. The information in Figure 6 helps identify intersections where projected left-turn movement increases could impact intersection operations.

The highest left-turn volume increase in Figure 6 is the westbound left-turn from the proposed site driveway onto Rainbow Boulevard in the PM peak with 55 vehicles (vehicle queues associated with this movement would occur internal to the site). The second highest left-turn volume increase is the southbound left-turn from the site's eastern driveway onto 51st Street in the PM peak with 54 vehicles (vehicles queues associated with this movement would also occur internal to the site). The highest left-turn increase on Rainbow Boulevard is projected to occur on northbound Rainbow at 51st Street with 43 additional vehicles in the AM peak.

EXISTING + PROPOSED CONDITIONS

The existing traffic volumes in Figure 1 from July have been added to the proposed site traffic volumes in Figure 6 to determine the existing+proposed volumes in Figure 7. These volumes will be used in the intersection capacity analyses for existing+proposed conditions.

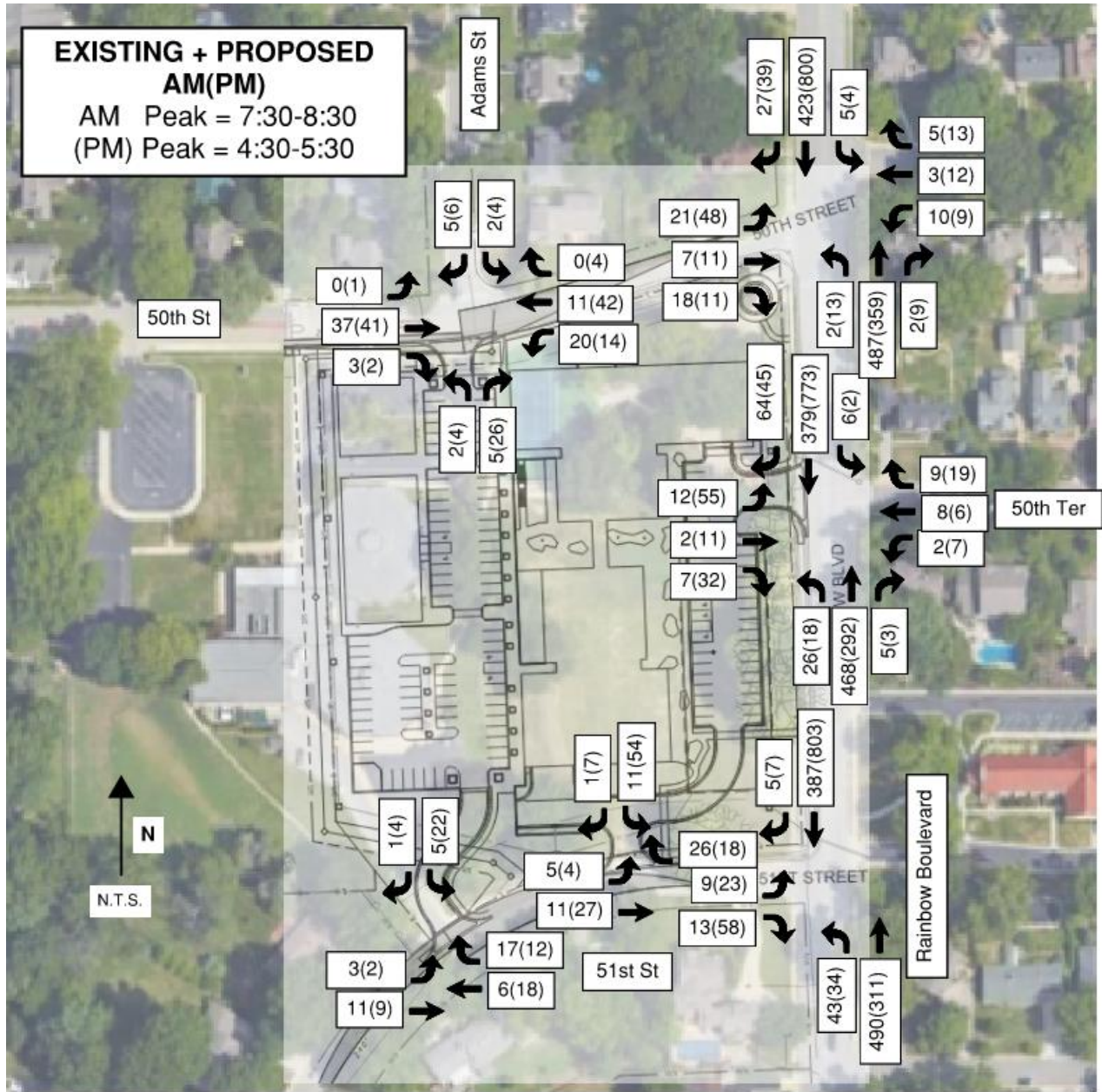


Figure 7: Existing + Proposed Traffic

INTERSECTION CAPACITY ANALYSES

Intersection capacity analyses were performed using the Highway Capacity Manual (HCM) 6th Edition Methodology provided in Synchro v11. The amount of delay is equated to a Level of Service (LOS) based on defined thresholds. A grade of A through F is assigned, with LOS A representing the best intersection operation. Table 2 shows the LOS associated with intersection approach delays, in seconds per vehicle (sec/veh), for signalized and unsignalized intersection cases.

Table 2 – Level of Service Criteria		
Level of Service (LOS)	Stop Control Approach Delay (sec/veh)	Signal Control Approach Delay (sec/veh)
A	≤ 10	≤ 10
B	> 10 and ≤ 15	> 10 and ≤ 20
C	> 15 and ≤ 25	> 20 and ≤ 35
D	> 25 and ≤ 35	> 35 and ≤ 55
E	> 35 and ≤ 50	> 55 and ≤ 80
F	> 50	> 80

Existing traffic signal timings for 50th Street and Rainbow Boulevard was provided by the City of Westwood. The timings were entered into the Synchro v11 program along with the existing AM and PM peak hour traffic volumes from Figure 1. Analyses were also performed for the existing + proposed peak hour volumes in Figure 7. The results of the analyses for the project intersections may be viewed in Table 3 on the next page.



INTERSECTION CAPACITY ANALYSES (continued)

Table 3 – Intersection Capacity Analyses										
Intersection	Existing Conditions					Existing + Proposed Conditions				
	AM		PM			AM		PM		
	Avg. Delay (sec)	LOS	Avg. Delay (sec)	LOS		Avg. Delay (sec)	LOS	Avg. Delay (sec)	LOS	
50th and Adams Street (Two-Way Stop)										
	NB	0.0	A	0.0	A	8.7	A	8.8	A	
	SB	8.5	A	8.8	A	8.7	A	9.0	A	
	EB	0.0	A	0.2	A	0.0	A	0.2	A	
	WB	0.0	A	0.0	A	4.7	A	1.7	A	
50th and Rainbow Boulevard (Signalized Intersection)										
	NB	5.2	A	8.4	A	6.7	A	10.0	B	
	SB	5.1	A	9.8	A	6.6	A	12.0	B	
	EB	22.3	C	26.9	C	22.1	C	29.6	C	
	WB	0.1	A	22.5	C	25.8	C	23.6	C	
50th Terrace and Rainbow Boulevard (Two-Way Stop)										
	NB	0.0	A	0.0	A	0.4	A	0.6	A	
	SB	0.1	A	0.0	A	0.1	A	0.0	A	
	EB	-	-	-	-	16.7	C	38.0	E	
	WB	11.2	B	11.6	B	16.6	C	15.9	C	
51st and Rainbow Boulevard (Eastbound One-Way Stop)										
	NB	0.0	A	0.0	A	0.9	A	1.2	A	
	SB	0.0	A	0.0	A	0.0	A	0.0	A	
	EB	12.7	B	13.1	B	12.8	B	18.2	C	
51st Street and East Drive (Southbound One-Way Stop)										
	SB	-	-	-	-	8.8	A	9.1	A	
	EB	-	-	-	-	2.3	A	0.9	A	
	WB	-	-	-	-	0.0	A	0.0	A	
51st Street and West Drive (Southbound One-Way Stop)										
	SB	0.0	A	0.0	A	8.7	A	8.8	A	
	EB	0.0	A	0.0	A	1.6	A	1.3	A	
	WB	0.0	A	0.0	A	0.0	A	0.0	A	

For the existing conditions (July traffic counts) all intersections and lane movements operate at LOS of C or better. For the existing+proposed conditions, all intersections and lane movements are expected to operate at LOS of C or better with one exception. The eastbound lane movement from the ground level parking garage to Rainbow Boulevard at 50th Terrace is estimated to operate at LOS E during the PM peak hour. Vehicle delays and queues associated with this movement would occur internal to the site.

Note: A pass-by reduction was not applied to the mixed-use component of the trip generation used for these analyses. If applied, average delays would slightly improve.



INTERSECTION CAPACITY ANALYSES (continued)

Synchro signalized intersection queuing analyses were performed using Highway Capacity Manual 6th Edition methodology in Synchro v11. The results of the analyses may be seen for the study intersections in Table 4.

Table 4 – Intersection 95% Queues						
Intersection	Storage Provided	Existing Conditions		Existing + Proposed Conditions		
		95% Queue (feet)		95% Queue (feet)		
		AM	PM	AM	PM	
50th Street and Adams Street						
Approach Lane						
NB	30'	0'	0'	20'	20'	
SB	20'	0'	0'	0'	0'	
50th Street and Rainbow Boulevard						
Approach Lane						
NB	300'	105'	73'	117'	97'	
SB	450'	85'	205'	107'	237'	
EB	250'	35'	42'	42'	64'	
WB	150'	0'	30'	24'	36'	
50th Terrace and Rainbow Boulevard						
Approach Lane						
EB	55'	0'	0'	20'	60'	
WB	200'	20'	20'	20'	20'	
NBL	280'	N/A	N/A	20'	20'	
51st Street and Rainbow Boulevard						
Approach Lane						
EB	140'	0'	0'	20'	20'	
NBL	130'	N/A	N/A	20'	20'	
51st Street and East Drive						
Approach Lane						
SB	40'	0'	0'	0'	20'	
51st Street and West Drive						
Approach Lane						
SB	160'	0'	0'	0'	20'	

All intersections lane movements are expected to have calculated 95% queues within their existing or proposed storage areas with the exception of the eastbound movement leaving the ground level of the parking garage to Rainbow Boulevard. Vehicle queues associated with this movement would occur internal to the site and may not be desirable to the operation of the garage.

Note: A pass-by reduction was not applied to the mixed-use component of the trip generation used for these analyses.

CRASH ANALYSES

The Kansas Department of Transportation (KDOT) provided accident data for Rainbow Boulevard from 50th Street to 51st Street during the 5-year period between 2018 and 2022.

Based on the provided data, no intersections reported an average of more than one accident per year during the reporting period (this is less than the requirement for an accident-based traffic signal warrant). A summary of the data can be seen in Table 5.

Table 5 - Crash Analysis Summary						
Rainbow Boulevard Intersection	Five Year (2018-2022) Accident Totals					
	PDO	Injury	Fatal	Total	Reported Acc. / Year	Reported Acc. / MEV
50 th Street	2	2	0	4	0.8	0.127
50 th Terrace	1	0	0	1	0.2	0.032
51 st Street	1	1	0	1	0.2	0.032

The KDOT Traffic Count map indicates a 24-hour traffic volume of 17,200 vehicles per day on Rainbow Boulevard just north of Shawnee Mission Parkway in Year 2017. The Accident Rate per Million Entering Vehicles @ 50th and Rainbow is calculated as:

$$\frac{(4 \text{ accidents}) * (1,000,000)}{(17,200 \text{ entering vehicles per day}) * (365 \text{ days/year}) * (5 \text{ years})} = 0.127 \text{ Acc./MEV}$$

Three of the six reported accidents over the period were fixed object related.



ACCESS MANAGEMENT

According to KDOT standards, Rainbow Boulevard is best classified as a Class B roadway, as it is located on the National Highway System. 50th Street, 50th Terrace, and 51st Street are best classified as Class E roadways, as they provide local service only for very short trips.

50th Terrace is situated 220 feet south of 50th Street and currently forms a T-intersection with Rainbow Boulevard. The proposed site adds a fourth leg to the existing intersection. See Figure 8.



Figure 8: Access Points

With Rainbow being a 35-mph Class B roadway, Table 4-6 of the KDOT Access Management Policy was reviewed to determine unsignalized access spacing. With 50th Terrace already existing 220 feet south of 50th Street, a case for an area type of central business district (CBD) can be made. CBD indicates a 205-foot spacing criteria.

Table 4-6. Unsignalized access spacing criteria

Access Route Classification	Area Type	Posted Speed Limit (mph)										
		20	25	30	35	40	45	50	55	60	65	70
B	Undeveloped				350	420	515	610	720	825	955	1075
	Developed	115	170	225	295	365	450	535	640	740		
	CBD	85	120	155	205							

Figure 9: KDOT Unsignalized Access Spacing

ACCESS MANAGEMENT (continued)

The southbound right-turn volumes for Rainbow Boulevard into the proposed site driveway at 50th Terrace is necessary to determine if there is merit for a southbound right-turn auxiliary lane into the site. Figure 6 indicates that the existing+proposed condition is anticipated to have 64 southbound right-turns in the AM peak with 385 thru/left-turns. Figure 6 also indicates that the PM peak is anticipated to have 45 southbound right-turns with 775 thru/left-turns.

KDOT does not provide guidelines for right-turn treatments for 35 mph roadways. Table 4-25 of the KDOT Access Management Policy can be reviewed for 40 mph, in which case the volumes for this development remain under the values to warrant an auxiliary lane.

Table 4-25. Right-turn treatment guidelines for two-lane highways

Highway DDHV (vph)	Highway Operating Speed (mph)												
	40		45		50		55		60		65		
	Lane	Taper	Lane	Taper	Lane	Taper	Lane	Taper	Lane	Taper	Lane	Taper	
200				83	73	30	35	14	20	8	15	7	
AM = 385	300		120	40	41	19	24	9	15	7	12	6	
400	200	85	52	27	30	14	19	8	12	6	11	5	
PM = 775	600	50	27	26	13	20	9	14	6	10	5	9	4
800	25	12	16	8	15	7	11	5	9	4	8	3	
1000	14	8	12	5	11	5	9	4	8	3	7	3	
1200	10	6	9	4	9	4	8	4	7	3	7	3	

Source: "Guidelines for right-turn treatments at unsignalized intersections and driveways," K-Tran:KSU-95-5, Kansas Department of Transportation, Kansas State University, Tanweer Hasan, Dr. Robert W. Stokes

- Turning speed is 15 mph (right-turn)
- The values presented in this table represent minimum right-turn design hour volumes (vph) required to warrant right-turn treatments (lane or taper)
- DDHV = directional design hourly volumes

Table 4-25 does not provide guidelines for 35 mph Operating Speeds

Figure 10: KDOT Right-turn Guidelines

CONCLUSION

This traffic study has conducted traffic counts, provided a traffic distribution, trip generation and analyses for the proposed Westwood Village development.

The analyses contained in this study are based on summer traffic counts. Unfortunately the project schedule did not allow for existing traffic counts to be taken in the fall while Westwood View Elementary School was in session. This will be rectified in September with an addendum to this study that replaces the existing summer traffic counts with existing fall school counts. While there will be additional traffic on Rainbow Boulevard and the side streets, there is not much change in intersection level of service and queues anticipated.

Tables 3 and 4 provide the AM and PM intersection capacity and queuing analyses for existing and existing+proposed conditions for the following intersections:

- 50th Street and Adams Street
- 50th Street and Rainbow Boulevard (signalized)
- 50th Terrace and Rainbow Boulevard
- 51st Street and Rainbow Boulevard
- 51st Street and East Proposed Driveway
- 51st Street and West Proposed Driveway

In all cases, the total intersection level of service is LOS C or better. Only one movement, the eastbound approach at 50th Terrace and Rainbow Boulevard operates below LOS C at LOS E. That same movement is also the only approach that indicates a 95% queue that exceeds available storage.

A pass-by reduction was not applied to the mixed-use component of the trip generation used for these analyses. If applied, average delays and queues would improve slightly.

This study has two recommendations on the following page.



RECOMMENDATIONS

Recommendation 1: Increase the width of the eastbound drive approach at 50th Terrace and Rainbow Boulevard to accommodate 3-lanes. One receiving lane, a left-turn only lane, and a right-turn only lane. Sign the outbound approaches as Left-Turn Only and Right-Turn Only to discourage any thru traffic. This modification would be expected to help with capacity and queueing.

Recommendation 2: Schedule traffic counts to be conducted a minimum of two-weeks after the first day of school at the study intersections and any other driveway in use by Rushton Elementary (in the former Westwood View Elementary). Use the driveway counts to back out the Rushton Elementary traffic to obtain baseline traffic count data for the existing conditions while school in session. With that information, prepare an Addendum to this study that recalculates the existing, and existing+proposed conditions for the AM, PM School Peak, and PM peak periods.

This traffic impact study and its Addendum will be presented to KDOT for review.

If there are any questions regarding this traffic memo, please contact me at your convenience at 913-663-1900 or mark.sherfy@ibhc.com.

Sincerely,



Mark Sherfy, P.E., PTOE
Traffic Engineer
BHC



APPENDICES

APPENDIX A - TRAFFIC COUNTS

- 50th Street and Adams Street
- 50th Street and Rainbow Boulevard
- 50th Terrace and Rainbow Boulevard
- 51st Street and Rainbow Boulevard

APPENDIX B – ITE TRIP GENERATION REPORTS

APPENDIX C - CAPACITY AND QUEUING ANALYSES

- 50th Street and Adams Street
 - AM Existing
 - PM Existing
 - AM Existing+Proposed
 - PM Existing+Proposed
- 50th Street and Rainbow Boulevard
 - AM Existing
 - PM Existing
 - AM Existing+Proposed
 - PM Existing+Proposed
- 50th Terrace and Rainbow Boulevard
 - AM Existing
 - PM Existing
 - AM Existing+Proposed
 - PM Existing+Proposed
- 51st Street and Rainbow Boulevard
 - AM Existing
 - PM Existing
 - AM Existing+Proposed
 - PM Existing+Proposed
- 51st Street and East Drive
 - AM Existing+Proposed
 - PM Existing+Proposed
- 51st and West Drive
 - AM Existing
 - PM Existing
 - AM Existing+Proposed
 - PM Existing+Proposed



APPENDIX A - TRAFFIC COUNTS

- 50th Street and Adams Street
- 50th Street and Rainbow Boulevard
- 50th Terrace and Rainbow Boulevard
- 51st Street and Rainbow Boulevard

West 50th Street & Adams St - TMC

Tue Jul 18, 2023

Full Length (7 AM-9 AM, 2 PM-6 PM)

All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks)

All Movements

ID: 1090499, Location: 39.037394, -94.612952



Item A, Section III, Item

Provided by: Gewalt Hamilton Associates Inc.

625 Forest Edge Drive, Vernon Hills, IL, 60061, US

Leg Direction	Adams St Southbound				50th St Westbound				Exit Access Northbound				50th St Eastbound				Int
	R	L	U	App	R	T	U	App	R	T	L	App	T	L	U	App	
2023-07-18 7:00AM	0	0	0	0	1	3	0	4	0	0	0	0	4	0	0	4	8
7:15AM	1	0	0	1	0	2	0	2	0	0	0	0	6	0	0	6	9
7:30AM	0	0	0	0	0	1	0	1	0	0	0	0	6	0	0	6	7
7:45AM	3	1	0	4	0	4	0	4	0	0	0	0	8	0	0	8	16
Hourly Total	4	1	0	5	1	10	0	11	0	0	0	0	24	0	0	24	40
8:00AM	2	1	0	3	0	3	0	3	0	0	0	0	9	0	0	9	15
8:15AM	0	0	0	0	0	3	0	3	0	0	0	0	8	0	0	8	11
8:30AM	1	3	0	4	0	8	0	8	0	0	0	0	5	0	0	5	17
8:45AM	0	0	0	0	0	7	0	7	0	0	0	0	10	0	0	10	17
Hourly Total	3	4	0	7	0	21	0	21	0	0	0	0	32	0	0	32	60
2:00PM	1	0	0	1	1	6	0	7	0	0	0	0	10	0	0	10	18
2:15PM	1	1	0	2	1	7	0	8	2	0	1	3	7	0	0	7	20
2:30PM	2	1	0	3	0	5	0	5	0	0	0	0	4	3	0	7	15
2:45PM	1	0	0	1	1	1	0	2	0	0	0	0	3	0	0	3	6
Hourly Total	5	2	0	7	3	19	0	22	2	0	1	3	24	3	0	27	59
3:00PM	1	0	0	1	0	4	0	4	0	0	1	1	6	0	0	6	12
3:15PM	0	0	0	0	0	3	0	3	0	0	0	0	9	0	0	9	12
3:30PM	1	0	0	1	1	7	0	8	0	0	0	0	1	3	0	4	13
3:45PM	0	0	0	0	0	7	0	7	1	0	0	1	8	1	0	9	17
Hourly Total	2	0	0	2	1	21	0	22	1	0	1	2	24	4	0	28	54
4:00PM	0	0	0	0	0	9	0	9	1	0	0	1	15	1	0	16	26
4:15PM	0	0	0	0	0	6	0	6	0	0	0	0	11	0	0	11	17
4:30PM	2	2	0	4	1	7	0	8	0	0	0	0	3	0	0	3	15
4:45PM	2	1	0	3	3	9	0	12	0	0	0	0	14	1	0	15	30
Hourly Total	4	3	0	7	4	31	0	35	1	0	0	1	43	2	0	45	88
5:00PM	1	1	0	2	0	8	0	8	0	0	0	0	12	0	0	12	22
5:15PM	1	0	0	1	0	18	0	18	0	0	0	0	8	0	0	8	27
5:30PM	0	0	0	0	0	16	0	16	0	0	0	0	6	2	0	8	24
5:45PM	0	0	0	0	1	6	0	7	0	0	0	0	5	0	0	5	12
Hourly Total	2	1	0	3	1	48	0	49	0	0	0	0	31	2	0	33	85
Total	20	11	0	31	10	150	0	160	4	0	2	6	178	11	0	189	386
% Approach	64.5%	35.5%	0%	-	6.3%	93.8%	0%	-	66.7%	0%	33.3%	-	94.2%	5.8%	0%	-	-
% Total	5.2%	2.8%	0%	8.0%	2.6%	38.9%	0%	41.5%	1.0%	0%	0.5%	1.6%	46.1%	2.8%	0%	49.0%	-
Lights	20	11	0	31	10	149	0	159	4	0	2	6	176	11	0	187	383
% Lights	100%	100%	0%	100%	100%	99.3%	0%	99.4%	100%	0%	100%	100%	98.9%	100%	0%	98.9%	99.2%
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Articulated Trucks	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%
Buses and Single-Unit Trucks	0	0	0	0	0	1	0	1	0	0	0	0	2	0	0	2	3
% Buses and Single-Unit Trucks	0%	0%	0%	0%	0%	0.7%	0%	0.6%	0%	0%	0%	0%	1.1%	0%	0%	1.1%	0.8%

* L: Left, R: Right, T: Thru, U: U-Turn

West 50th Street & Adams St - TMC

Tue Jul 18, 2023

Full Length (7 AM-9 AM, 2 PM-6 PM)

All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks)

All Movements

ID: 1090499, Location: 39.037394, -94.612952

[N] Adams St

Total: 52

In: 31 Out: 21

20 11

[W] 50th St

Total: 361

In: 189 Out: 172

11 178

10 150

Out: 193 In: 160

Total: 353

[E] 50th St

2 4

Out: 0 In: 6
Total: 6

[S] Exit Access

West 50th Street & Adams St - TMC

Tue Jul 18, 2023

AM Peak (8 AM - 9 AM)

All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks)

All Movements

ID: 1090499, Location: 39.037394, -94.612952



Item A, Section III, Item

Provided by: Gewalt Hamilton Associates Inc.
625 Forest Edge Drive, Vernon Hills, IL, 60061, US

Leg Direction	Adams St Southbound				50th St Westbound				Exit Access Northbound				50th St Eastbound				
Time	R	L	U	App	R	T	U	App	R	T	L	App	T	L	U	App	Int
2023-07-18 8:00AM	2	1	0	3	0	3	0	3	0	0	0	0	9	0	0	9	15
8:15AM	0	0	0	0	0	3	0	3	0	0	0	0	8	0	0	8	11
8:30AM	1	3	0	4	0	8	0	8	0	0	0	0	5	0	0	5	17
8:45AM	0	0	0	0	0	7	0	7	0	0	0	0	10	0	0	10	17
Total	3	4	0	7	0	21	0	21	0	0	0	0	32	0	0	32	60
% Approach	42.9%	57.1%	0%	-	0%	100%	0%	-	0%	0%	0%	-	100%	0%	0%	-	-
% Total	5.0%	6.7%	0%	11.7%	0%	35.0%	0%	35.0%	0%	0%	0%	0%	53.3%	0%	0%	53.3%	-
PHF	0.375	0.333	-	0.438	-	0.656	-	0.656	-	-	-	-	0.800	-	-	0.800	0.882
Lights	3	4	0	7	0	21	0	21	0	0	0	0	32	0	0	32	60
% Lights	100%	100%	0%	100%	0%	100%	0%	100%	0%	0%	0%	-	100%	0%	0%	100%	100%
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Articulated Trucks	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	-	0%	0%	0%	0%	0%
Buses and Single-Unit Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Buses and Single-Unit Trucks	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	-	0%	0%	0%	0%	0%

* L: Left, R: Right, T: Thru, U: U-Turn

West 50th Street & Adams St - TMC

Tue Jul 18, 2023

AM Peak (8 AM - 9 AM)

All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks)

All Movements

ID: 1090499, Location: 39.037394, -94.612952



Item A, Section III, Item

Provided by: Gewalt Hamilton Associates Inc.
625 Forest Edge Drive, Vernon Hills, IL, 60061, US

[N] Adams St

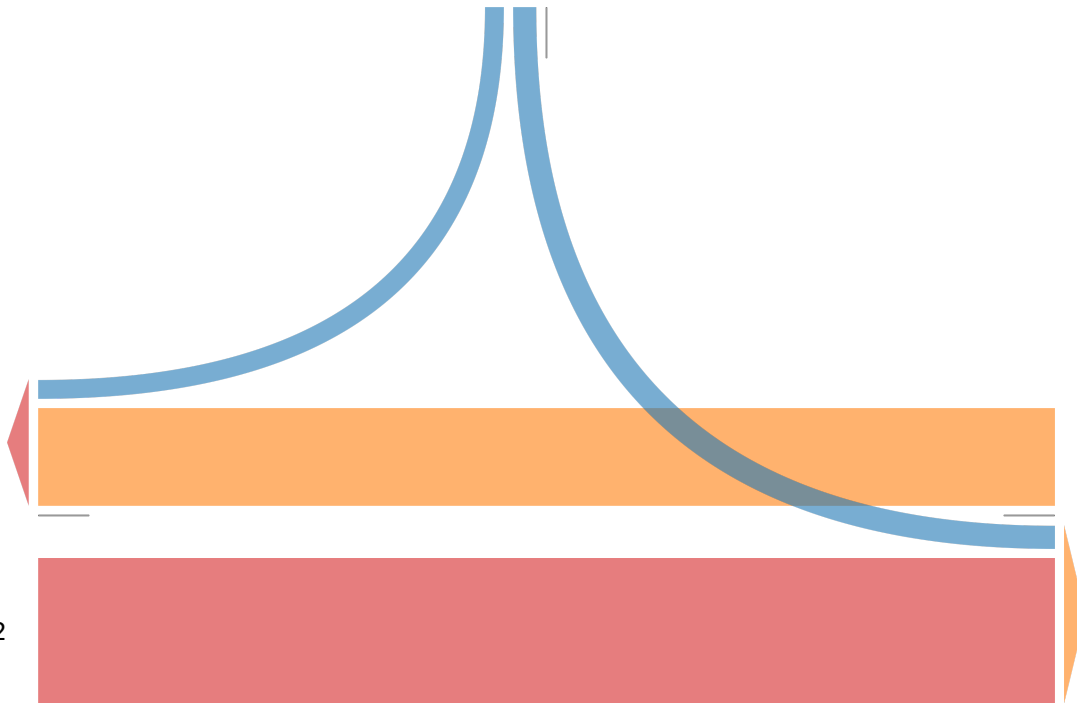
Total: 7

In: 7 Out: 0

3 4

[W] 50th St
Total: 56
In: 32 Out: 24

32



21

Out: 36 In: 21
Total: 57
[E] 50th St

West 50th Street & Adams St - TMC

Tue Jul 18, 2023

PM Peak (4:45 PM - 5:45 PM) - Overall Peak Hour

All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks)

All Movements

ID: 1090499, Location: 39.037394, -94.612952



Item A, Section III, Item

Provided by: Gewalt Hamilton Associates Inc.
625 Forest Edge Drive, Vernon Hills, IL, 60061, US

Leg Direction	Adams St Southbound				50th St Westbound				Exit Access Northbound				50th St Eastbound				Int
	R	L	U	App	R	T	U	App	R	T	L	App	T	L	U	App	
2023-07-18 4:45PM	2	1	0	3	3	9	0	12	0	0	0	0	14	1	0	15	30
5:00PM	1	1	0	2	0	8	0	8	0	0	0	0	12	0	0	12	22
5:15PM	1	0	0	1	0	18	0	18	0	0	0	0	8	0	0	8	27
5:30PM	0	0	0	0	0	16	0	16	0	0	0	0	6	2	0	8	24
Total	4	2	0	6	3	51	0	54	0	0	0	0	40	3	0	43	103
% Approach	66.7%	33.3%	0%	-	5.6%	94.4%	0%	-	0%	0%	0%	-	93.0%	7.0%	0%	-	-
% Total	3.9%	1.9%	0%	5.8%	2.9%	49.5%	0%	52.4%	0%	0%	0%	0%	38.8%	2.9%	0%	41.7%	-
PHF	0.500	0.500	-	0.500	0.250	0.708	-	0.750	-	-	-	-	0.714	0.375	-	0.717	0.858
Lights	4	2	0	6	3	51	0	54	0	0	0	0	40	3	0	43	103
% Lights	100%	100%	0%	100%	100%	100%	0%	100%	0%	0%	0%	-	100%	100%	0%	100%	100%
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Articulated Trucks	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	-	0%	0%	0%	0%	0%
Buses and Single-Unit Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Buses and Single-Unit Trucks	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	-	0%	0%	0%	0%	0%

* L: Left, R: Right, T: Thru, U: U-Turn

West 50th Street & Adams St - TMC

Tue Jul 18, 2023

PM Peak (4:45 PM - 5:45 PM) - Overall Peak Hour

All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks)

All Movements

ID: 1090499, Location: 39.037394, -94.612952

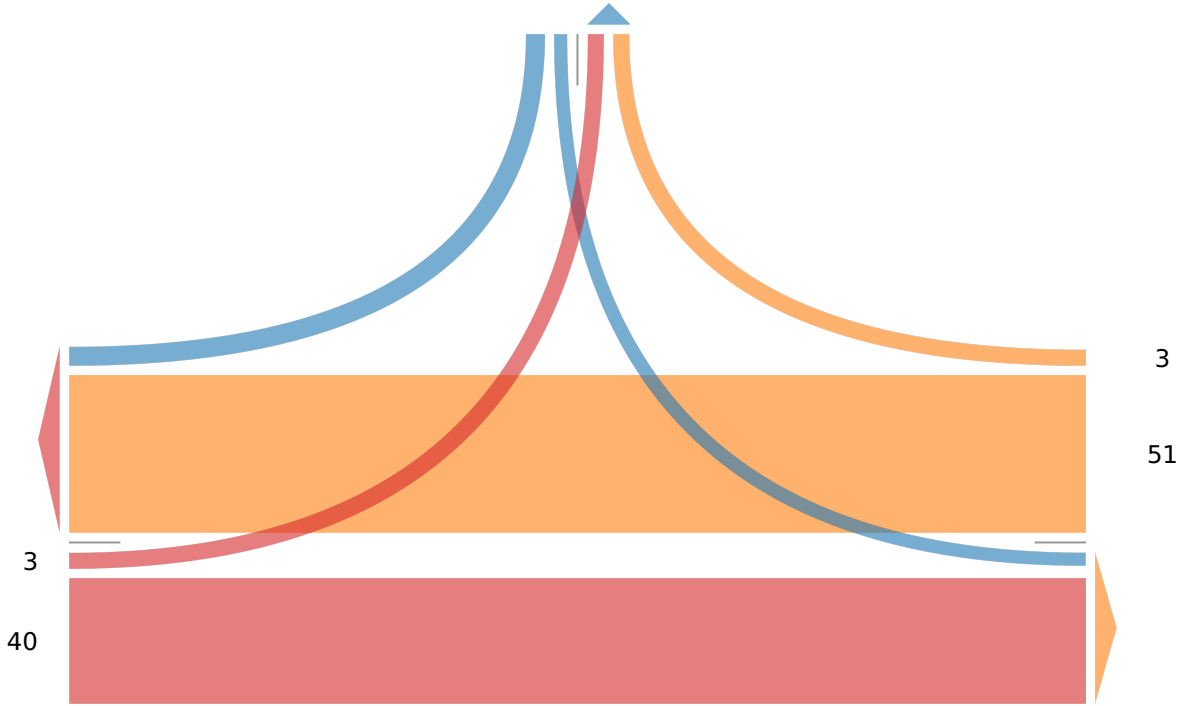
[N] Adams St

Total: 12

In: 6 Out: 6

4 2

[W] 50th St
 Total: 98
 In: 43 Out: 55



Out: 42 In: 54
 Total: 96
 [E] 50th St

Rainbow Boulevard & West 50th Street - TMC

Tue Jul 18, 2023

Full Length (7 AM-9 AM, 2 PM-6 PM)

All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks)

All Movements

ID: 1090495, Location: 39.037585, -94.611889



Item A, Section III, Item

Provided by: Gewalt Hamilton Associates Inc.

625 Forest Edge Drive, Vernon Hills, IL, 60061, US

Leg Direction	Rainbow Blvd Southbound					50th St Westbound					Rainbow Blvd Northbound					50th St Eastbound					Int
	R	T	L	U	App	R	T	L	U	App	R	T	L	U	App	R	T	L	U	App	
2023-07-18 7:00AM	0	60	1	0	61	0	0	1	0	1	1	94	4	0	99	2	1	1	0	4	165
7:15AM	0	71	1	0	72	0	1	0	0	1	1	103	1	0	105	0	0	6	0	6	184
7:30AM	1	91	0	0	92	2	0	0	0	2	0	124	0	0	124	2	0	3	0	5	223
7:45AM	4	100	1	0	105	1	0	2	0	3	0	126	0	0	126	2	1	6	0	9	243
Hourly Total	5	322	3	0	330	3	1	3	0	7	2	447	5	0	454	6	2	16	0	24	815
8:00AM	3	96	0	0	99	0	0	1	0	1	1	108	1	0	110	3	2	4	0	9	219
8:15AM	2	84	4	0	90	2	0	1	0	3	0	115	0	0	115	5	3	4	0	12	220
8:30AM	5	88	2	0	95	1	2	0	0	3	1	82	1	0	84	4	1	3	0	8	190
8:45AM	3	104	2	0	109	1	1	0	0	2	0	121	3	0	124	4	1	5	0	10	245
Hourly Total	13	372	8	0	393	4	3	2	0	9	2	426	5	0	433	16	7	16	0	39	874
2:00PM	6	89	2	0	97	0	1	0	0	1	4	62	0	0	66	1	1	7	0	9	173
2:15PM	3	106	1	0	110	3	2	2	0	7	0	76	2	0	78	2	0	7	0	9	204
2:30PM	3	102	1	0	106	1	1	2	0	4	0	72	1	0	73	1	1	3	0	5	188
2:45PM	0	85	2	0	87	1	1	1	0	3	0	62	1	0	63	1	1	1	0	3	156
Hourly Total	12	382	6	0	400	5	5	5	0	15	4	272	4	0	280	5	3	18	0	26	721
3:00PM	3	137	1	0	141	1	1	1	0	3	0	79	0	0	79	1	2	3	0	6	229
3:15PM	2	111	2	0	115	1	1	2	0	4	1	55	0	0	56	0	4	4	0	8	183
3:30PM	6	114	1	0	121	1	1	1	0	3	0	57	2	0	59	0	0	2	0	2	185
3:45PM	4	126	3	0	133	2	2	0	0	4	2	89	0	0	91	2	2	5	0	9	237
Hourly Total	15	488	7	0	510	5	5	4	0	14	3	280	2	0	285	3	8	14	0	25	834
4:00PM	5	150	5	0	160	2	3	0	0	5	2	81	1	0	84	7	4	5	0	16	265
4:15PM	3	137	0	0	140	5	3	0	0	8	0	85	1	0	86	2	2	7	0	11	245
4:30PM	5	171	0	0	176	2	1	1	0	4	0	67	1	0	68	2	0	3	0	5	253
4:45PM	6	196	1	0	203	2	3	1	0	6	1	64	3	0	68	2	3	8	0	13	290
Hourly Total	19	654	6	0	679	11	10	2	0	23	3	297	6	0	306	13	9	23	0	45	1053
5:00PM	5	217	1	0	223	4	2	3	0	9	0	74	1	0	75	2	1	10	0	13	320
5:15PM	11	179	2	0	192	5	4	0	0	9	2	89	2	0	93	1	3	5	0	9	303
5:30PM	8	154	1	0	163	7	5	0	0	12	2	75	3	0	80	1	0	5	0	6	261
5:45PM	0	131	2	0	133	1	4	0	0	5	1	89	3	0	93	0	1	4	0	5	236
Hourly Total	24	681	6	0	711	17	15	3	0	35	5	327	9	0	341	4	5	24	0	33	1120
Total	88	2899	36	0	3023	45	39	19	0	103	19	2049	31	0	2099	47	34	111	0	192	5417
% Approach	2.9%	95.9%	1.2%	0%	-	43.7%	37.9%	18.4%	0%	-	0.9%	97.6%	1.5%	0%	-	24.5%	17.7%	57.8%	0%	-	-
% Total	1.6%	53.5%	0.7%	0%	55.8%	0.8%	0.7%	0.4%	0%	1.9%	0.4%	37.8%	0.6%	0%	38.7%	0.9%	0.6%	2.0%	0%	3.5%	-
Lights	88	2827	34	0	2949	44	38	19	0	101	19	2001	31	0	2051	47	34	110	0	191	5292
% Lights	100%	97.5%	94.4%	0%	97.6%	97.8%	97.4%	100%	0%	98.1%	100%	97.7%	100%	0%	97.7%	100%	100%	99.1%	0%	99.5%	97.7%
Articulated Trucks	0	7	0	0	7	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	8
% Articulated Trucks	0%	0.2%	0%	0%	0.2%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0.1%
Buses and Single-Unit Trucks	0	65	2	0	67	1	1	0	0	2	0	47	0	0	47	0	0	1	0	1	117
% Buses and Single-Unit Trucks	0%	2.2%	5.6%	0%	2.2%	2.2%	2.6%	0%	0%	1.9%	0%	2.3%	0%	0%	2.2%	0%	0%	0.9%	0%	0.5%	2.2%

*L: Left, R: Right, T: Thru, U: U-Turn

Rainbow Boulevard & West 50th Street - TMC

Tue Jul 18, 2023

Full Length (7 AM-9 AM, 2 PM-6 PM)

All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks)

All Movements

ID: 1090495, Location: 39.037585, -94.611889



Item A, Section III, Item

Provided by: Gewalt Hamilton Associates Inc.
625 Forest Edge Drive, Vernon Hills, IL, 60061, US

[N] Rainbow Blvd

Total: 5228

In: 3023

Out: 2205

[W] 50th St
Total: 350
In: 192 Out: 158

111
34
47

88

2899

36

45
39
19

Out: 89 In: 103
Total: 192
[E] 50th St

31

2049

19

Out: 2965

In: 2099

Total: 5064

[S] Rainbow Blvd

Rainbow Boulevard & West 50th Street - TMC

Tue Jul 18, 2023

AM Peak (7:30 AM - 8:30 AM)

All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks)

All Movements

ID: 1090495, Location: 39.037585, -94.611889



Item A, Section III, Item

Provided by: Gewalt Hamilton Associates Inc.

625 Forest Edge Drive, Vernon Hills, IL, 60061, US

Leg Direction	Rainbow Blvd Southbound					50th St Westbound					Rainbow Blvd Northbound					50th St Eastbound					Int
	R	T	L	U	App	R	T	L	U	App	R	T	L	U	App	R	T	L	U	App	
2023-07-18 7:30AM	1	91	0	0	92	2	0	0	0	2	0	124	0	0	124	2	0	3	0	5	223
7:45AM	4	100	1	0	105	1	0	2	0	3	0	126	0	0	126	2	1	6	0	9	243
8:00AM	3	96	0	0	99	0	0	1	0	1	1	108	1	0	110	3	2	4	0	9	219
8:15AM	2	84	4	0	90	2	0	1	0	3	0	115	0	0	115	5	3	4	0	12	220
Total	10	371	5	0	386	5	0	4	0	9	1	473	1	0	475	12	6	17	0	35	905
% Approach	2.6%	96.1%	1.3%	0%	-	55.6%	0%	44.4%	0%	-	0.2%	99.6%	0.2%	0%	-	34.3%	17.1%	48.6%	0%	-	-
% Total	1.1%	41.0%	0.6%	0%	42.7%	0.6%	0%	0.4%	0%	1.0%	0.1%	52.3%	0.1%	0%	52.5%	1.3%	0.7%	1.9%	0%	3.9%	-
PHF	0.625	0.928	0.313	-	0.919	0.625	-	0.500	-	0.750	0.250	0.938	0.250	-	0.942	0.600	0.500	0.708	-	0.729	0.931
Lights	10	354	5	0	369	5	0	4	0	9	1	465	1	0	467	12	6	17	0	35	880
% Lights	100%	95.4%	100%	0%	95.6%	100%	0%	100%	0%	100%	100%	98.3%	100%	0%	98.3%	100%	100%	100%	0%	100%	97.2%
Articulated Trucks	0	5	0	0	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5
% Articulated Trucks	0%	1.3%	0%	0%	1.3%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0.6%
Buses and Single-Unit Trucks	0	12	0	0	12	0	0	0	0	0	0	8	0	0	8	0	0	0	0	0	20
% Buses and Single-Unit Trucks	0%	3.2%	0%	0%	3.1%	0%	0%	0%	0%	0%	0%	1.7%	0%	0%	1.7%	0%	0%	0%	0%	0%	2.2%

*L: Left, R: Right, T: Thru, U: U-Turn

Rainbow Boulevard & West 50th Street - TMC

Tue Jul 18, 2023

AM Peak (7:30 AM - 8:30 AM)

All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks)

All Movements

ID: 1090495, Location: 39.037585, -94.611889



Item A, Section III, Item

Provided by: Gewalt Hamilton Associates Inc.
625 Forest Edge Drive, Vernon Hills, IL, 60061, US

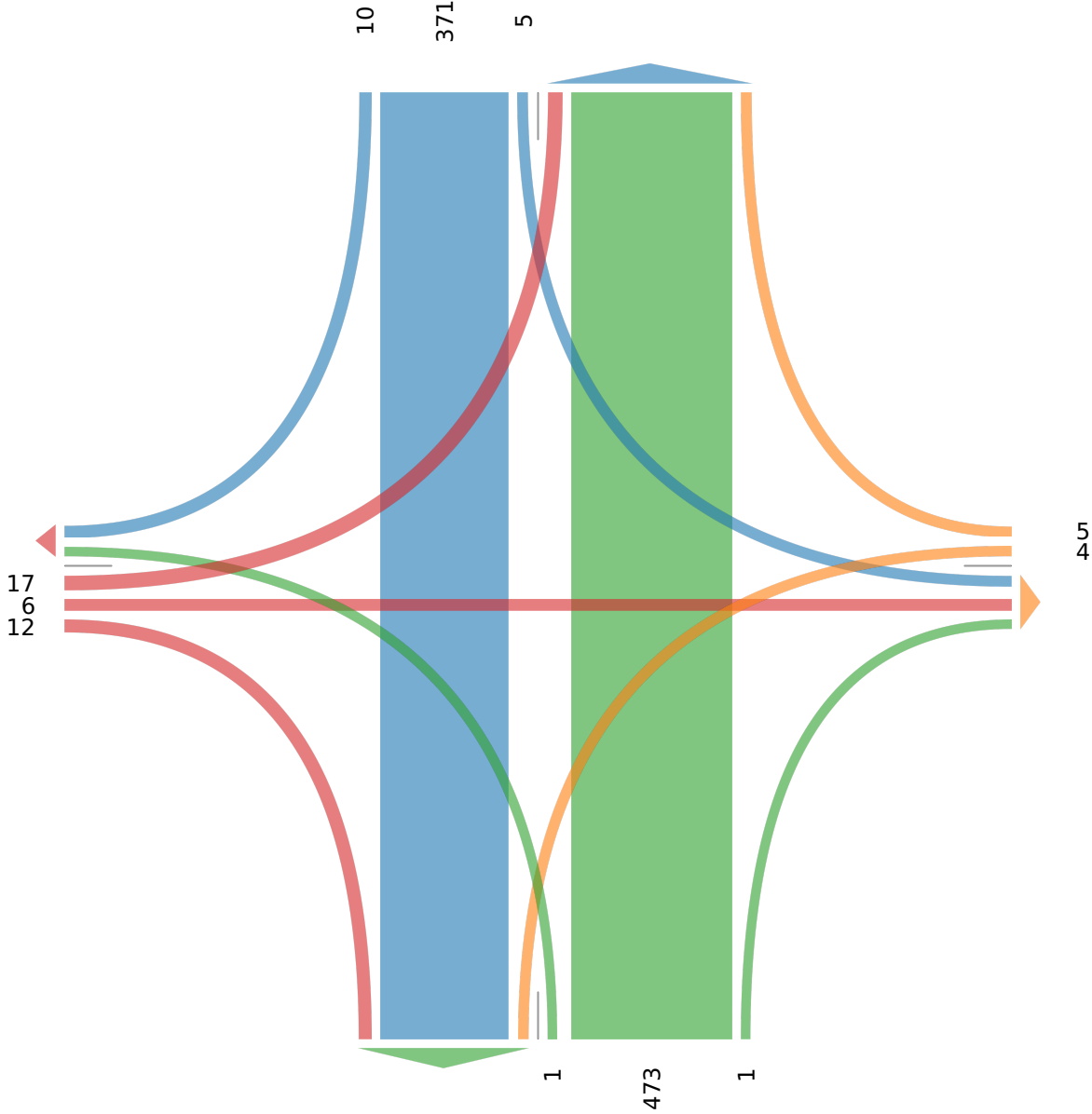
[N] Rainbow Blvd

Total: 881

In: 386

Out: 495

[W] 50th St
Total: 46
In: 35 Out: 11



Out: 387

In: 475

Total: 862

[S] Rainbow Blvd

Rainbow Boulevard & West 50th Street - TMC

Tue Jul 18, 2023

PM Peak (4:45 PM - 5:45 PM) - Overall Peak Hour

All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks)

All Movements

ID: 1090495, Location: 39.037585, -94.611889



Item A, Section III, Item

Provided by: Gewalt Hamilton Associates Inc.

625 Forest Edge Drive, Vernon Hills, IL, 60061, US

Leg Direction	Rainbow Blvd Southbound					50th St Westbound					Rainbow Blvd Northbound					50th St Eastbound					Int	
	R	T	L	U	App	R	T	L	U	App	R	T	L	U	App	R	T	L	U	App		
2023-07-18 4:45PM	6	196	1	0	203	2	3	1	0	6	1	64	3	0	68	2	3	8	0	13	290	
5:00PM	5	217	1	0	223	4	2	3	0	9	0	74	1	0	75	2	1	10	0	13	320	
5:15PM	11	179	2	0	192	5	4	0	0	9	2	89	2	0	93	1	3	5	0	9	303	
5:30PM	8	154	1	0	163	7	5	0	0	12	2	75	3	0	80	1	0	5	0	6	261	
Total	30	746	5	0	781	18	14	4	0	36	5	302	9	0	316	6	7	28	0	41	1174	
% Approach	3.8%	95.5%	0.6%	0%	-	50.0%	38.9%	11.1%	0%	-	1.6%	95.6%	2.8%	0%	-	14.6%	17.1%	68.3%	0%	-	-	
% Total	2.6%	63.5%	0.4%	0%	66.5%	1.5%	1.2%	0.3%	0%	3.1%	0.4%	25.7%	0.8%	0%	26.9%	0.5%	0.6%	2.4%	0%	3.5%	-	
PHF	0.682	0.859	0.625	-	0.876	0.643	0.700	0.333	-	0.750	0.625	0.848	0.750	-	0.849	0.750	0.583	0.700	-	0.788	0.917	
Lights	30	738	4	0	772	18	14	4	0	36	5	297	9	0	311	6	7	28	0	41	1160	
% Lights	100%	98.9%	80.0%	0%	98.8%	100%	100%	100%	0%	100%	100%	98.3%	100%	0%	98.4%	100%	100%	100%	0%	100%	98.8%	
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Articulated Trucks	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%
Buses and Single-Unit Trucks	0	8	1	0	9	0	0	0	0	0	0	5	0	0	5	0	0	0	0	0	14	
% Buses and Single-Unit Trucks	0%	1.1%	20.0%	0%	1.2%	0%	0%	0%	0%	0%	0%	1.7%	0%	0%	1.6%	0%	0%	0%	0%	0%	1.2%	

* L: Left, R: Right, T: Thru, U: U-Turn

Rainbow Boulevard & West 50th Street - TMC

Tue Jul 18, 2023

PM Peak (4:45 PM - 5:45 PM) - Overall Peak Hour

All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks)

All Movements

ID: 1090495, Location: 39.037585, -94.611889



Item A, Section III, Item

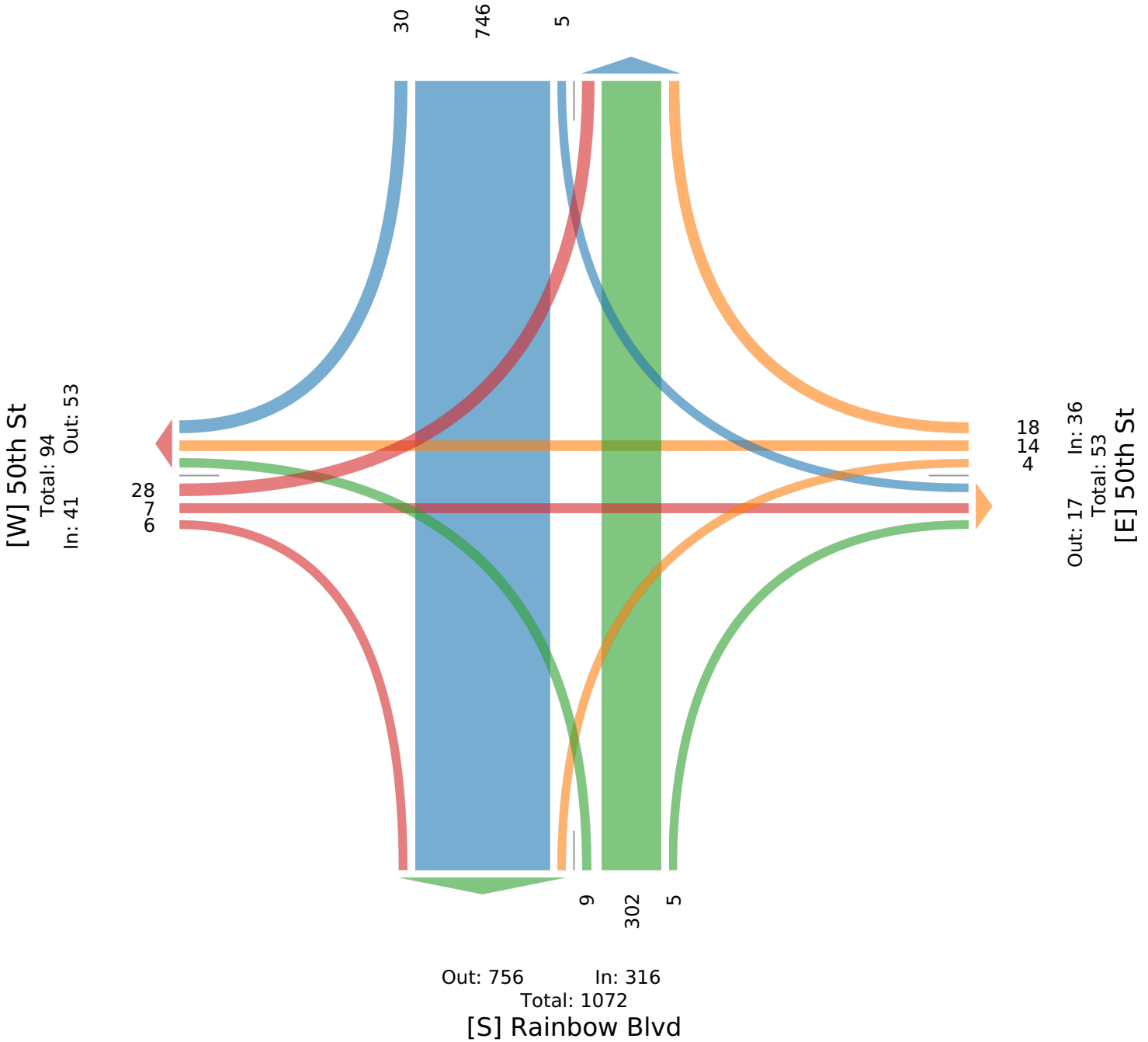
Provided by: Gewalt Hamilton Associates Inc.
625 Forest Edge Drive, Vernon Hills, IL, 60061, US

[N] Rainbow Blvd

Total: 1129

In: 781

Out: 348



Rainbow Boulevard & 50th Terrace - TMC

Tue Jul 18, 2023

Full Length (7 AM-9 AM, 2 PM-6 PM)

All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks)

All Movements

ID: 1090498, Location: 39.036944, -94.611879



Item A, Section III, Item

Provided by: Gewalt Hamilton Associates Inc.

625 Forest Edge Drive, Vernon Hills, IL, 60061, US

Leg Direction	Rainbow Blvd Southbound				50th Ter Westbound				Rainbow Blvd Northbound				
Time	T	L	U	App	R	L	U	App	R	T	U	App	Int
2023-07-18 7:00AM	62	2	0	64	1	2	0	3	4	99	0	103	170
7:15AM	70	0	0	70	0	1	0	1	1	105	0	106	177
7:30AM	91	1	0	92	1	1	0	2	3	125	0	128	222
7:45AM	101	2	0	103	1	0	0	1	0	125	0	125	229
Hourly Total	324	5	0	329	3	4	0	7	8	454	0	462	798
8:00AM	98	0	0	98	6	0	0	6	1	105	0	106	210
8:15AM	89	3	0	92	1	1	0	2	1	113	0	114	208
8:30AM	91	2	0	93	3	3	0	6	3	82	0	85	184
8:45AM	104	4	0	108	2	1	0	3	0	126	0	126	237
Hourly Total	382	9	0	391	12	5	0	17	5	426	0	431	839
2:00PM	91	0	0	91	1	0	0	1	3	68	0	71	163
2:15PM	111	1	0	112	0	2	0	2	2	80	0	82	196
2:30PM	103	1	0	104	0	1	0	1	2	72	0	74	179
2:45PM	85	1	0	86	2	1	0	3	0	62	0	62	151
Hourly Total	390	3	0	393	3	4	0	7	7	282	0	289	689
3:00PM	136	2	0	138	4	1	0	5	0	77	0	77	220
3:15PM	110	2	0	112	0	3	0	3	3	57	0	60	175
3:30PM	116	0	0	116	1	1	0	2	3	59	0	62	180
3:45PM	133	3	0	136	1	3	0	4	0	93	0	93	233
Hourly Total	495	7	0	502	6	8	0	14	6	286	0	292	808
4:00PM	158	1	0	159	1	2	0	3	2	82	0	84	246
4:15PM	139	0	0	139	3	1	0	4	1	87	0	88	231
4:30PM	170	2	0	172	4	1	0	5	0	66	0	66	243
4:45PM	199	0	0	199	7	3	0	10	2	68	0	70	279
Hourly Total	666	3	0	669	15	7	0	22	5	303	0	308	999
5:00PM	223	0	0	223	2	2	0	4	1	74	0	75	302
5:15PM	181	0	0	181	6	1	0	7	0	84	0	84	272
5:30PM	153	1	0	154	3	0	0	3	0	77	0	77	234
5:45PM	130	1	0	131	3	1	0	4	0	90	0	90	225
Hourly Total	687	2	0	689	14	4	0	18	1	325	0	326	1033
Total	2944	29	0	2973	53	32	0	85	32	2076	0	2108	5166
% Approach	99.0%	1.0%	0%	-	62.4%	37.6%	0%	-	1.5%	98.5%	0%	-	-
% Total	57.0%	0.6%	0%	57.5%	1.0%	0.6%	0%	1.6%	0.6%	40.2%	0%	40.8%	-
Lights	2876	29	0	2905	52	32	0	84	32	2028	0	2060	5049
% Lights	97.7%	100%	0%	97.7%	98.1%	100%	0%	98.8%	100%	97.7%	0%	97.7%	97.7%
Articulated Trucks	5	0	0	5	0	0	0	0	0	1	0	1	6
% Articulated Trucks	0.2%	0%	0%	0.2%	0%	0%	0%	0%	0%	0%	0%	0%	0.1%
Buses and Single-Unit Trucks	63	0	0	63	1	0	0	1	0	47	0	47	111
% Buses and Single-Unit Trucks	2.1%	0%	0%	2.1%	1.9%	0%	0%	1.2%	0%	2.3%	0%	2.2%	2.1%

* L: Left, R: Right, T: Thru, U: U-Turn

Rainbow Boulevard & 50th Terrace - TMC

Tue Jul 18, 2023

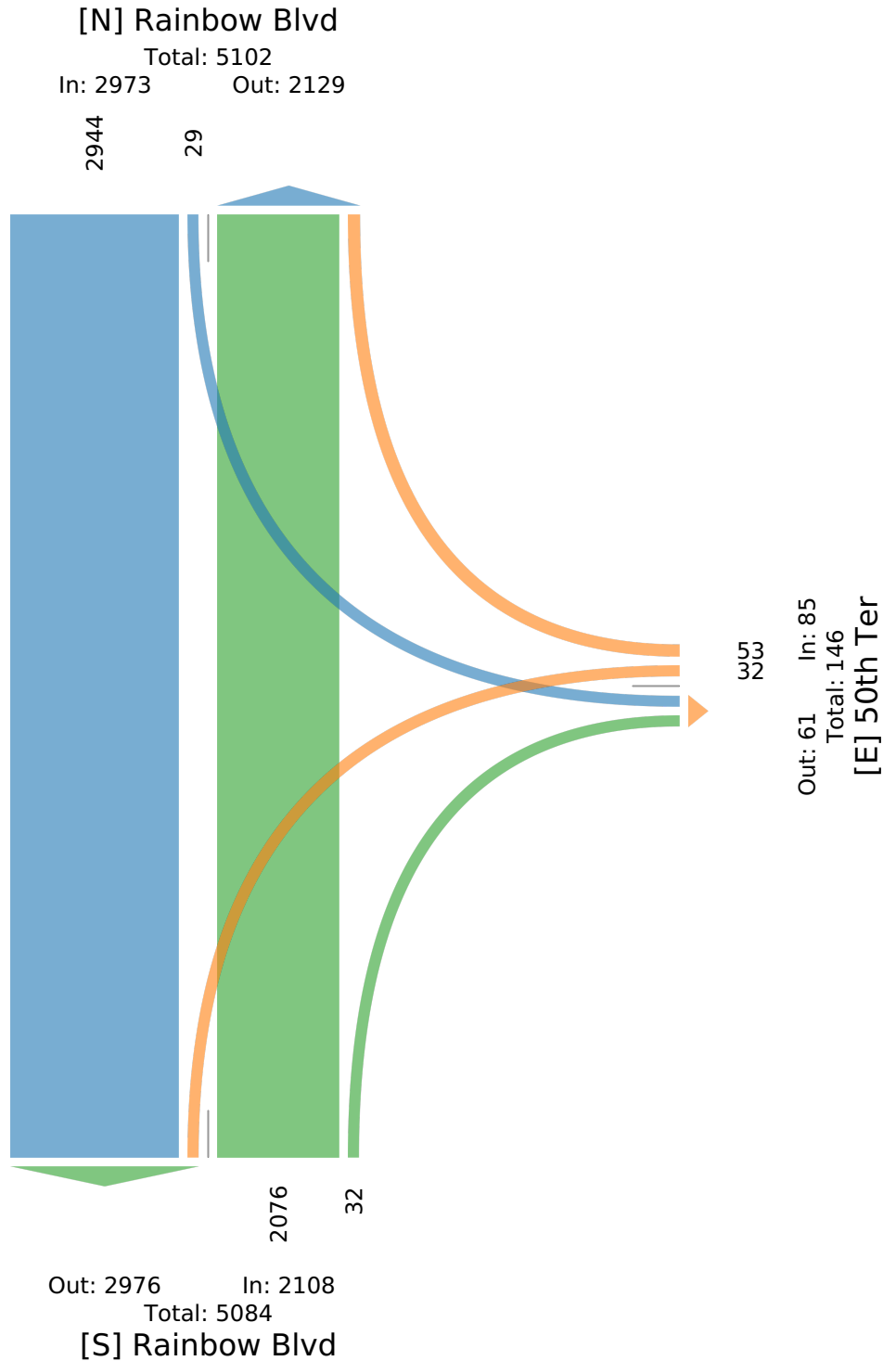
Full Length (7 AM-9 AM, 2 PM-6 PM)

All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks)

All Movements

ID: 1090498, Location: 39.036944, -94.611879

Provided by: Gewalt Hamilton Associates Inc.
625 Forest Edge Drive, Vernon Hills, IL, 60061, US



Rainbow Boulevard & 50th Terrace - TMC

Tue Jul 18, 2023

AM Peak (7:30 AM - 8:30 AM)

All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks)

All Movements

ID: 1090498, Location: 39.036944, -94.611879



Item A, Section III, Item

Provided by: Gewalt Hamilton Associates Inc.

625 Forest Edge Drive, Vernon Hills, IL, 60061, US

Leg Direction	Rainbow Blvd Southbound				50th Ter Westbound				Rainbow Blvd Northbound				
Time	T	L	U	App	R	L	U	App	R	T	U	App	Int
2023-07-18 7:30AM	91	1	0	92	1	1	0	2	3	125	0	128	222
7:45AM	101	2	0	103	1	0	0	1	0	125	0	125	229
8:00AM	98	0	0	98	6	0	0	6	1	105	0	106	210
8:15AM	89	3	0	92	1	1	0	2	1	113	0	114	208
Total	379	6	0	385	9	2	0	11	5	468	0	473	869
% Approach	98.4%	1.6%	0%	-	81.8%	18.2%	0%	-	1.1%	98.9%	0%	-	-
% Total	43.6%	0.7%	0%	44.3%	1.0%	0.2%	0%	1.3%	0.6%	53.9%	0%	54.4%	-
PHF	0.938	0.500	-	0.934	0.375	0.500	-	0.458	0.417	0.936	-	0.924	0.949
Lights	364	6	0	370	9	2	0	11	5	460	0	465	846
% Lights	96.0%	100%	0%	96.1%	100%	100%	0%	100%	100%	98.3%	0%	98.3%	97.4%
Articulated Trucks	3	0	0	3	0	0	0	0	0	0	0	0	3
% Articulated Trucks	0.8%	0%	0%	0.8%	0%	0%	0%	0%	0%	0%	0%	0%	0.3%
Buses and Single-Unit Trucks	12	0	0	12	0	0	0	0	0	8	0	8	20
% Buses and Single-Unit Trucks	3.2%	0%	0%	3.1%	0%	0%	0%	0%	0%	1.7%	0%	1.7%	2.3%

* L: Left, R: Right, T: Thru, U: U-Turn

Rainbow Boulevard & 50th Terrace - TMC

Tue Jul 18, 2023

AM Peak (7:30 AM - 8:30 AM)

All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks)

All Movements

ID: 1090498, Location: 39.036944, -94.611879

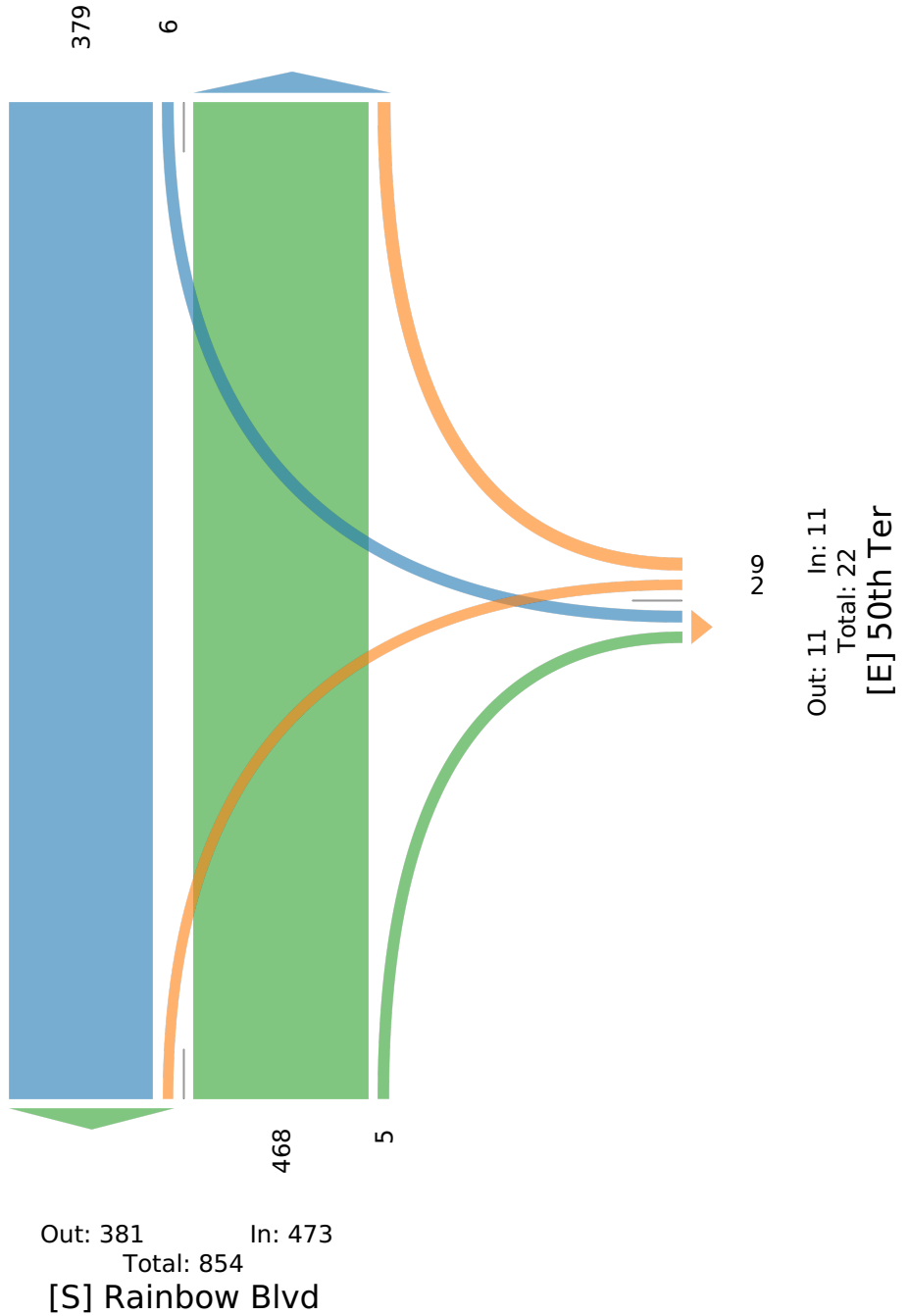
Provided by: Gewalt Hamilton Associates Inc.
625 Forest Edge Drive, Vernon Hills, IL, 60061, US

[N] Rainbow Blvd

Total: 862

In: 385

Out: 477



Rainbow Boulevard & 50th Terrace - TMC

Tue Jul 18, 2023

PM Peak (4:30 PM - 5:30 PM) - Overall Peak Hour

All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks)

All Movements

ID: 1090498, Location: 39.036944, -94.611879



Item A, Section III, Item

Provided by: Gewalt Hamilton Associates Inc.

625 Forest Edge Drive, Vernon Hills, IL, 60061, US

Leg Direction	Rainbow Blvd Southbound				50th Ter Westbound				Rainbow Blvd Northbound				
Time	T	L	U	App	R	L	U	App	R	T	U	App	Int
2023-07-18 4:30PM	170	2	0	172	4	1	0	5	0	66	0	66	243
4:45PM	199	0	0	199	7	3	0	10	2	68	0	70	279
5:00PM	223	0	0	223	2	2	0	4	1	74	0	75	302
5:15PM	181	0	0	181	6	1	0	7	0	84	0	84	272
Total	773	2	0	775	19	7	0	26	3	292	0	295	1096
% Approach	99.7%	0.3%	0%	-	73.1%	26.9%	0%	-	1.0%	99.0%	0%	-	-
% Total	70.5%	0.2%	0%	70.7%	1.7%	0.6%	0%	2.4%	0.3%	26.6%	0%	26.9%	-
PHF	0.867	0.250	-	0.869	0.679	0.583	-	0.650	0.375	0.869	-	0.878	0.907
Lights	764	2	0	766	18	7	0	25	3	288	0	291	1082
% Lights	98.8%	100%	0%	98.8%	94.7%	100%	0%	96.2%	100%	98.6%	0%	98.6%	98.7%
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0
% Articulated Trucks	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%
Buses and Single-Unit Trucks	9	0	0	9	1	0	0	1	0	4	0	4	14
% Buses and Single-Unit Trucks	1.2%	0%	0%	1.2%	5.3%	0%	0%	3.8%	0%	1.4%	0%	1.4%	1.3%

* L: Left, R: Right, T: Thru, U: U-Turn

Rainbow Boulevard & 50th Terrace - TMC

Tue Jul 18, 2023

PM Peak (4:30 PM - 5:30 PM) - Overall Peak Hour

All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks)

All Movements

ID: 1090498, Location: 39.036944, -94.611879

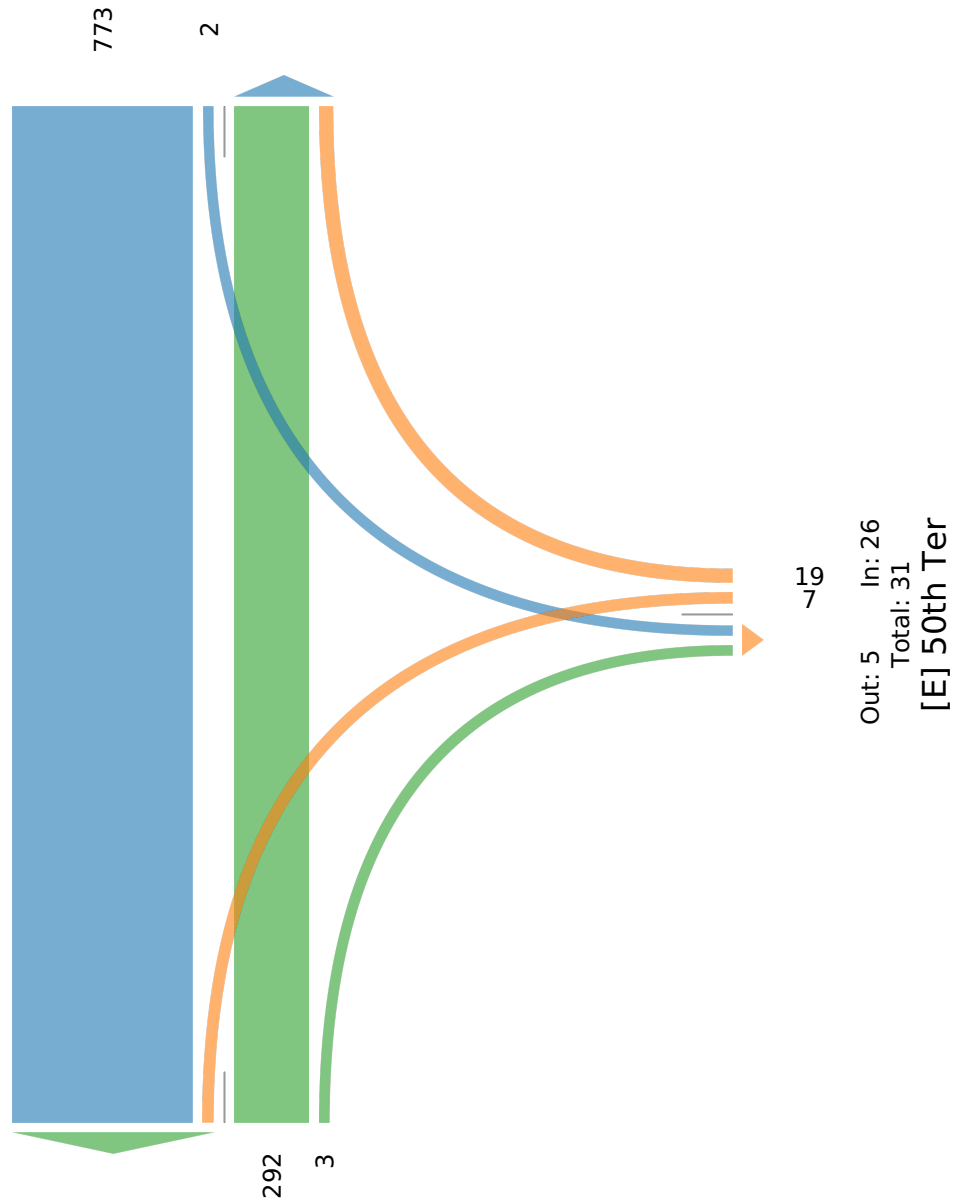
Provided by: Gewalt Hamilton Associates Inc.
625 Forest Edge Drive, Vernon Hills, IL, 60061, US

[N] Rainbow Blvd

Total: 1086

In: 775

Out: 311



Out: 5 In: 26
Total: 31
[E] 50th Ter

Out: 780

In: 295

Total: 1075

[S] Rainbow Blvd

Rainbow Boulevard & West 51st Street - TMC

Tue Jul 18, 2023

Full Length (7 AM-9 AM, 2 PM-6 PM)

All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks)

All Movements

ID: 1090496, Location: 39.036048, -94.611877



Item A, Section III, Item

Provided by: Gewalt Hamilton Associates Inc.

625 Forest Edge Drive, Vernon Hills, IL, 60061, US

Leg Direction	Rainbow Blvd Southbound					Access Westbound					Rainbow Blvd Northbound					51st St Eastbound					Int
	R	T	L	U	App	R	T	L	U	App	R	T	L	U	App	R	T	L	U	App	
2023-07-18 7:00AM	1	68	0	0	69	0	0	0	0	0	0	97	0	0	97	0	0	0	0	0	166
7:15AM	0	83	0	0	83	0	0	0	0	0	0	102	0	0	102	0	0	1	0	1	186
7:30AM	2	93	0	0	95	0	0	0	0	0	0	125	0	0	125	2	0	1	0	3	223
7:45AM	2	99	0	0	101	0	0	0	0	0	1	122	0	0	123	0	0	0	0	0	224
Hourly Total	5	343	0	0	348	0	0	0	0	0	1	446	0	0	447	2	0	2	0	4	799
8:00AM	1	99	1	0	101	0	0	0	0	0	0	104	0	0	104	0	0	0	0	0	205
8:15AM	0	89	1	0	90	0	0	0	0	0	0	111	0	0	111	0	0	3	0	3	204
8:30AM	1	93	0	0	94	0	0	0	0	0	1	81	0	0	82	0	0	1	0	1	177
8:45AM	0	103	1	0	104	0	0	0	0	0	0	125	0	0	125	1	0	0	0	1	230
Hourly Total	2	384	3	0	389	0	0	0	0	0	1	421	0	0	422	1	0	4	0	5	816
2:00PM	2	90	0	0	92	0	0	0	0	0	0	71	1	0	72	0	0	0	0	0	164
2:15PM	3	107	0	0	110	0	0	0	0	0	0	83	1	0	84	0	0	0	0	0	194
2:30PM	2	107	0	0	109	0	0	0	0	0	0	74	0	0	74	0	0	1	0	1	184
2:45PM	1	84	1	0	86	0	0	0	0	0	0	62	0	0	62	0	0	0	0	0	148
Hourly Total	8	388	1	0	397	0	0	0	0	0	0	290	2	0	292	0	0	1	0	1	690
3:00PM	1	133	0	0	134	0	0	0	0	0	0	77	1	0	78	0	0	0	0	0	212
3:15PM	0	114	0	0	114	0	0	0	0	0	0	59	0	0	59	1	0	1	0	2	175
3:30PM	0	104	0	0	104	0	0	0	0	0	0	61	0	0	61	1	0	1	0	2	167
3:45PM	2	125	1	0	128	0	0	0	0	0	0	93	0	0	93	1	0	0	0	1	222
Hourly Total	3	476	1	0	480	0	0	0	0	0	0	290	1	0	291	3	0	2	0	5	776
4:00PM	4	159	0	0	163	0	0	0	0	0	0	84	0	0	84	1	0	0	0	1	248
4:15PM	2	140	0	0	142	0	0	0	0	0	0	87	0	0	87	0	0	0	0	0	229
4:30PM	2	167	0	0	169	0	0	0	0	0	0	66	0	0	66	0	0	0	0	0	235
4:45PM	3	201	0	0	204	0	0	0	0	0	0	69	2	0	71	0	0	0	0	0	275
Hourly Total	11	667	0	0	678	0	0	0	0	0	0	306	2	0	308	1	0	0	0	1	987
5:00PM	1	216	0	0	217	0	0	0	0	0	0	73	1	0	74	3	0	1	0	4	295
5:15PM	1	187	0	0	188	0	0	0	0	0	0	85	1	0	86	1	0	0	0	1	275
5:30PM	1	153	0	0	154	0	0	0	0	0	0	77	0	0	77	1	0	0	0	1	232
5:45PM	0	133	0	0	133	0	0	0	0	0	0	90	2	0	92	0	0	0	0	0	225
Hourly Total	3	689	0	0	692	0	0	0	0	0	0	325	4	0	329	5	0	1	0	6	1027
Total	32	2947	5	0	2984	0	0	0	0	0	2	2078	9	0	2089	12	0	10	0	22	5095
% Approach	1.1%	98.8%	0.2%	0%	-	0%	0%	0%	0%	-	0.1%	99.5%	0.4%	0%	-	54.5%	0%	45.5%	0%	-	-
% Total	0.6%	57.8%	0.1%	0%	58.6%	0%	0%	0%	0%	0%	0%	40.8%	0.2%	0%	41.0%	0.2%	0%	0.2%	0%	0.4%	-
Lights	32	2880	5	0	2917	0	0	0	0	0	2	2031	8	0	2041	11	0	10	0	21	4979
% Lights	100%	97.7%	100%	0%	97.8%	0%	0%	0%	0%	-	100%	97.7%	88.9%	0%	97.7%	91.7%	0%	100%	0%	95.5%	97.7%
Articulated Trucks	0	6	0	0	6	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	7
% Articulated Trucks	0%	0.2%	0%	0%	0.2%	0%	0%	0%	0%	-	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0.1%
Buses and Single-Unit Trucks	0	61	0	0	61	0	0	0	0	0	0	46	1	0	47	1	0	0	0	1	109
% Buses and Single-Unit Trucks	0%	2.1%	0%	0%	2.0%	0%	0%	0%	0%	-	0%	2.2%	11.1%	0%	2.2%	8.3%	0%	0%	0%	4.5%	2.1%

*L: Left, R: Right, T: Thru, U: U-Turn

Rainbow Boulevard & West 51st Street - TMC

Tue Jul 18, 2023

Full Length (7 AM-9 AM, 2 PM-6 PM)

All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks)

All Movements

ID: 1090496, Location: 39.036048, -94.611877



Item A, Section III, Item

Provided by: Gewalt Hamilton Associates Inc.
625 Forest Edge Drive, Vernon Hills, IL, 60061, US

[N] Rainbow Blvd

Total: 5072

In: 2984

Out: 2088

[W] 51st St

Total: 63

In: 22 Out: 41

110
20

32

2947

5

9

2078

2

Out: 2959

In: 2089

Total: 5048

[S] Rainbow Blvd

Out: 7 In: 0

Total: 7

[E] Access

Rainbow Boulevard & West 51st Street - TMC

Tue Jul 18, 2023

AM Peak (7:30 AM - 8:30 AM)

All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks)

All Movements

ID: 1090496, Location: 39.036048, -94.611877



Item A, Section III, Item

Provided by: Gewalt Hamilton Associates Inc.
625 Forest Edge Drive, Vernon Hills, IL, 60061, US

Leg Direction	Rainbow Blvd Southbound					Access Westbound					Rainbow Blvd Northbound					51st St Eastbound					Int
	R	T	L	U	App	R	T	L	U	App	R	T	L	U	App	R	T	L	U	App	
2023-07-18 7:30AM	2	93	0	0	95	0	0	0	0	0	0	125	0	0	125	2	0	1	0	3	223
7:45AM	2	99	0	0	101	0	0	0	0	0	1	122	0	0	123	0	0	0	0	0	224
8:00AM	1	99	1	0	101	0	0	0	0	0	0	104	0	0	104	0	0	0	0	0	205
8:15AM	0	89	1	0	90	0	0	0	0	0	0	111	0	0	111	0	0	3	0	3	204
Total	5	380	2	0	387	0	0	0	0	0	1	462	0	0	463	2	0	4	0	6	856
% Approach	1.3%	98.2%	0.5%	0%	-	0%	0%	0%	0%	-	0.2%	99.8%	0%	0%	-	33.3%	0%	66.7%	0%	-	-
% Total	0.6%	44.4%	0.2%	0%	45.2%	0%	0%	0%	0%	0%	0.1%	54.0%	0%	0%	54.1%	0.2%	0%	0.5%	0%	0.7%	-
PHF	0.625	0.960	0.500	-	0.958	-	-	-	-	-	0.250	0.924	-	-	0.926	0.250	-	0.333	-	0.500	0.955
Lights	5	365	2	0	372	0	0	0	0	0	1	455	0	0	456	2	0	4	0	6	834
% Lights	100%	96.1%	100%	0%	96.1%	0%	0%	0%	0%	-	100%	98.5%	0%	0%	98.5%	100%	0%	100%	0%	100%	97.4%
Articulated Trucks	0	5	0	0	5	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	5
% Articulated Trucks	0%	1.3%	0%	0%	1.3%	0%	0%	0%	0%	-	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0.6%
Buses and Single-Unit Trucks	0	10	0	0	10	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	17
% Buses and Single-Unit Trucks	0%	2.6%	0%	0%	2.6%	0%	0%	0%	0%	-	0%	1.5%	0%	0%	1.5%	0%	0%	0%	0%	0%	2.0%

* L: Left, R: Right, T: Thru, U: U-Turn

Rainbow Boulevard & West 51st Street - TMC

Tue Jul 18, 2023

AM Peak (7:30 AM - 8:30 AM)

All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks)

All Movements

ID: 1090496, Location: 39.036048, -94.611877



Item A, Section III, Item

Provided by: Gewalt Hamilton Associates Inc.
625 Forest Edge Drive, Vernon Hills, IL, 60061, US

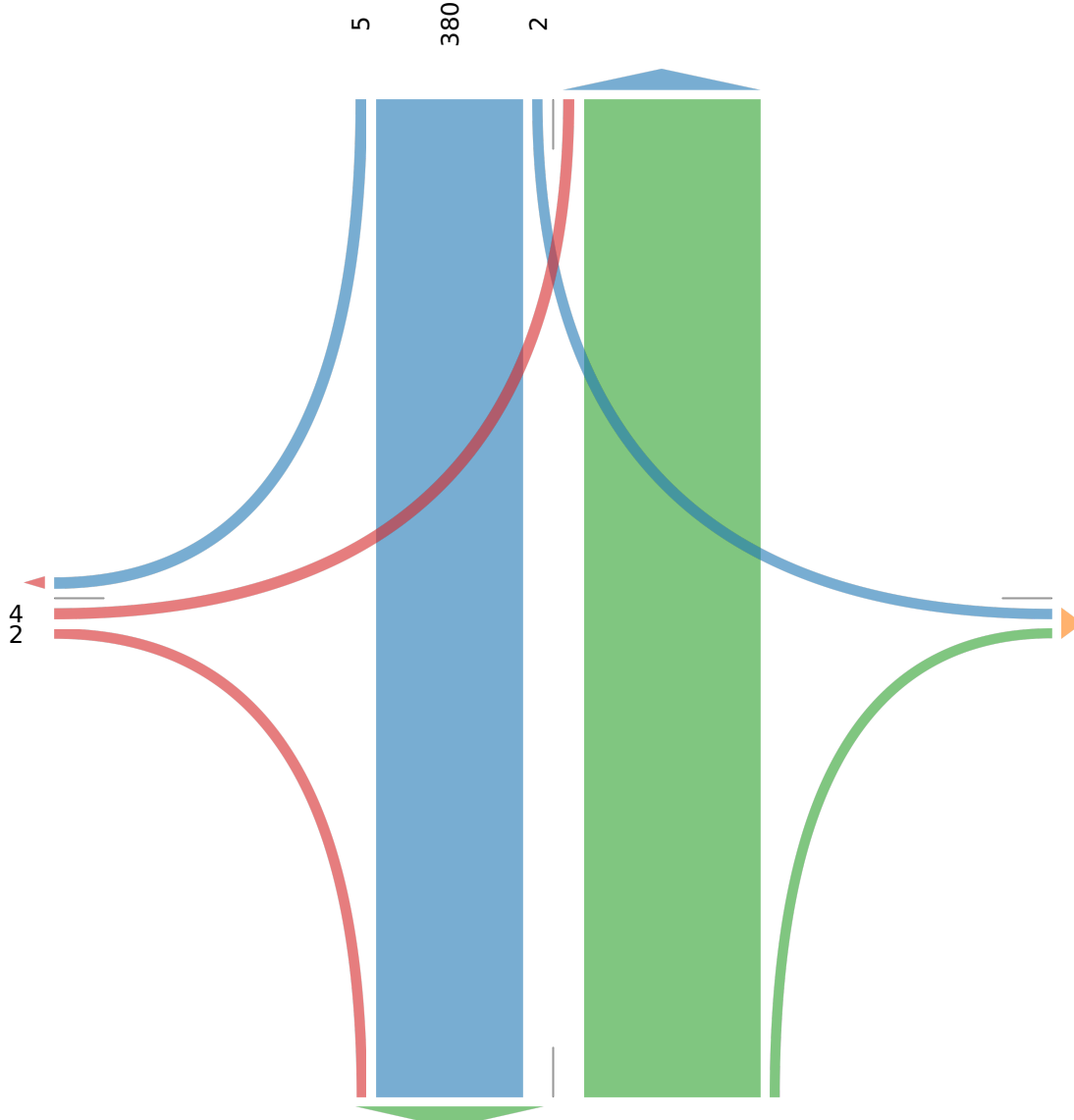
[N] Rainbow Blvd

Total: 853

In: 387

Out: 466

[W] 51st St
Total: 11
In: 6 Out: 5



Out: 3 In: 0
Total: 3
[E] Access

Out: 382

In: 463

Total: 845

[S] Rainbow Blvd

Rainbow Boulevard & West 51st Street - TMC

Tue Jul 18, 2023

PM Peak (4:30 PM - 5:30 PM) - Overall Peak Hour

All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks)

All Movements

ID: 1090496, Location: 39.036048, -94.611877



Item A, Section III, Item

Provided by: Gewalt Hamilton Associates Inc.

625 Forest Edge Drive, Vernon Hills, IL, 60061, US

Leg Direction	Rainbow Blvd Southbound					Access Westbound					Rainbow Blvd Northbound					51st St Eastbound					Int
	R	T	L	U	App	R	T	L	U	App	R	T	L	U	App	R	T	L	U	App	
2023-07-18 4:30PM	2	167	0	0	169	0	0	0	0	0	0	66	0	0	66	0	0	0	0	0	235
4:45PM	3	201	0	0	204	0	0	0	0	0	0	69	2	0	71	0	0	0	0	0	275
5:00PM	1	216	0	0	217	0	0	0	0	0	0	73	1	0	74	3	0	1	0	4	295
5:15PM	1	187	0	0	188	0	0	0	0	0	0	85	1	0	86	1	0	0	0	1	275
Total	7	771	0	0	778	0	0	0	0	0	0	293	4	0	297	4	0	1	0	5	1080
% Approach	0.9%	99.1%	0%	0%	-	0%	0%	0%	0%	-	0%	98.7%	1.3%	0%	-	80.0%	0%	20.0%	0%	-	-
% Total	0.6%	71.4%	0%	0%	72.0%	0%	0%	0%	0%	0%	0%	27.1%	0.4%	0%	27.5%	0.4%	0%	0.1%	0%	0.5%	-
PHF	0.583	0.892	-	-	0.896	-	-	-	-	-	-	0.862	0.500	-	0.863	0.333	-	0.250	-	0.313	0.915
Lights	7	762	0	0	769	0	0	0	0	0	0	289	4	0	293	4	0	1	0	5	1067
% Lights	100%	98.8%	0%	0%	98.8%	0%	0%	0%	0%	-	0%	98.6%	100%	0%	98.7%	100%	0%	100%	0%	100%	98.8%
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Articulated Trucks	0%	0%	0%	0%	0%	0%	0%	0%	0%	-	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%
Buses and Single-Unit Trucks	0	9	0	0	9	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	13
% Buses and Single-Unit Trucks	0%	1.2%	0%	0%	1.2%	0%	0%	0%	0%	-	0%	1.4%	0%	0%	1.3%	0%	0%	0%	0%	0%	1.2%

* L: Left, R: Right, T: Thru, U: U-Turn

Rainbow Boulevard & West 51st Street - TMC

Tue Jul 18, 2023

PM Peak (4:30 PM - 5:30 PM) - Overall Peak Hour

All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks)

All Movements

ID: 1090496, Location: 39.036048, -94.611877

[N] Rainbow Blvd

Total: 1072

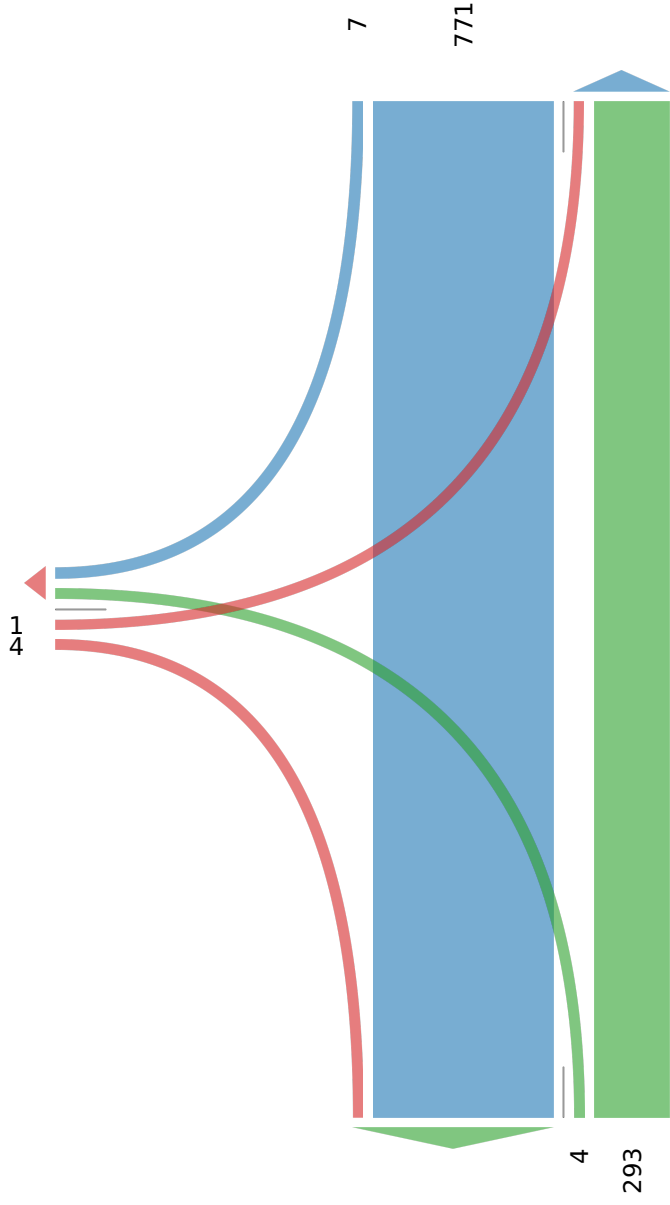
In: 778

Out: 294

[W] 51st St

Total: 16

In: 5 Out: 11



Out: 775

In: 297

Total: 1072

[S] Rainbow Blvd

APPENDIX B – ITE TRIP GENERATION PLOTS

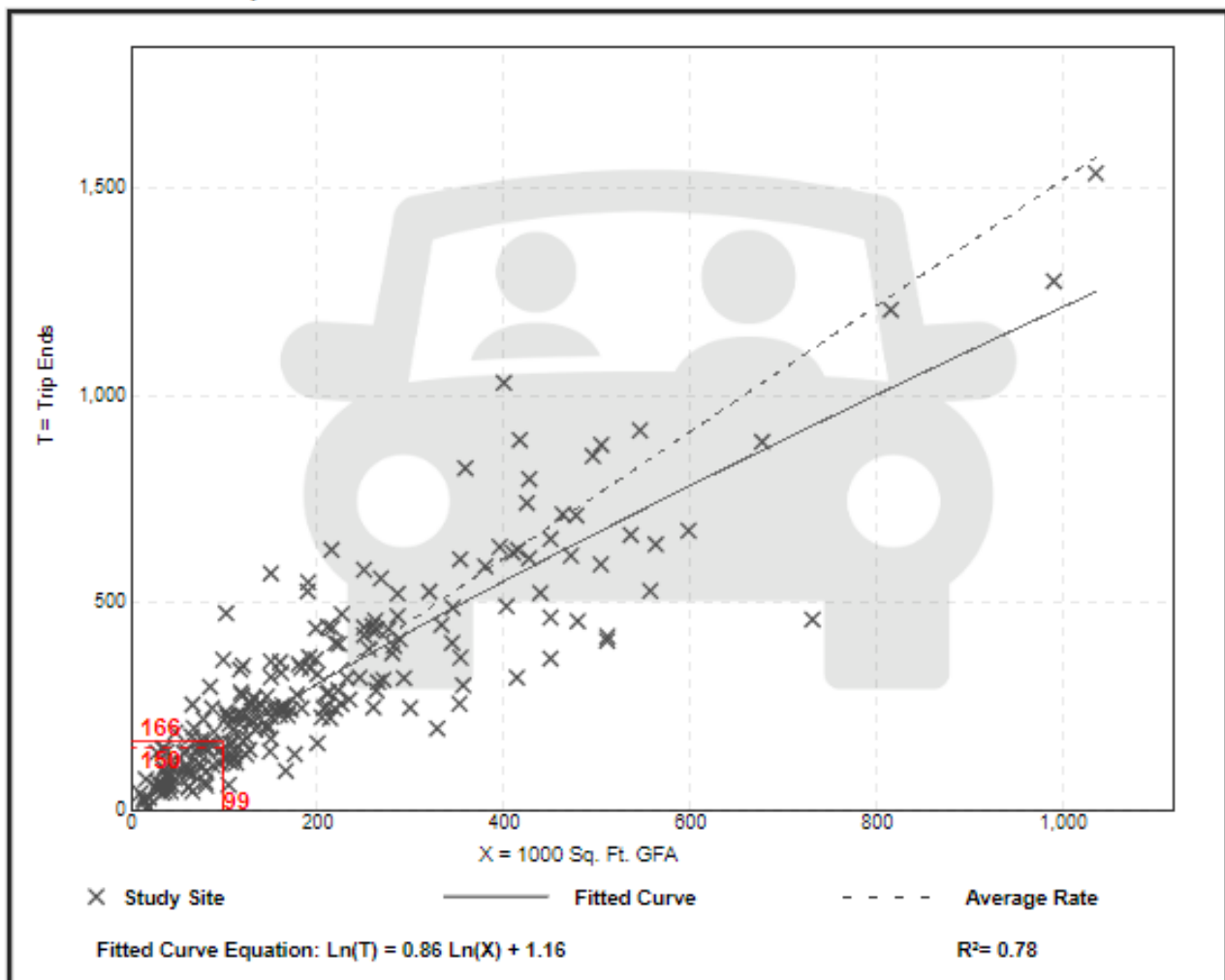
General Office Building (710)

Vehicle Trip Ends vs: 1000 Sq. Ft. GFA
 On a: Weekday,
 Peak Hour of Adjacent Street Traffic,
 One Hour Between 7 and 9 a.m.
 Setting/Location: General Urban/Suburban
 Number of Studies: 221
 Avg. 1000 Sq. Ft. GFA: 201
 Directional Distribution: 88% entering, 12% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
1.52	0.32 - 4.93	0.58

Data Plot and Equation



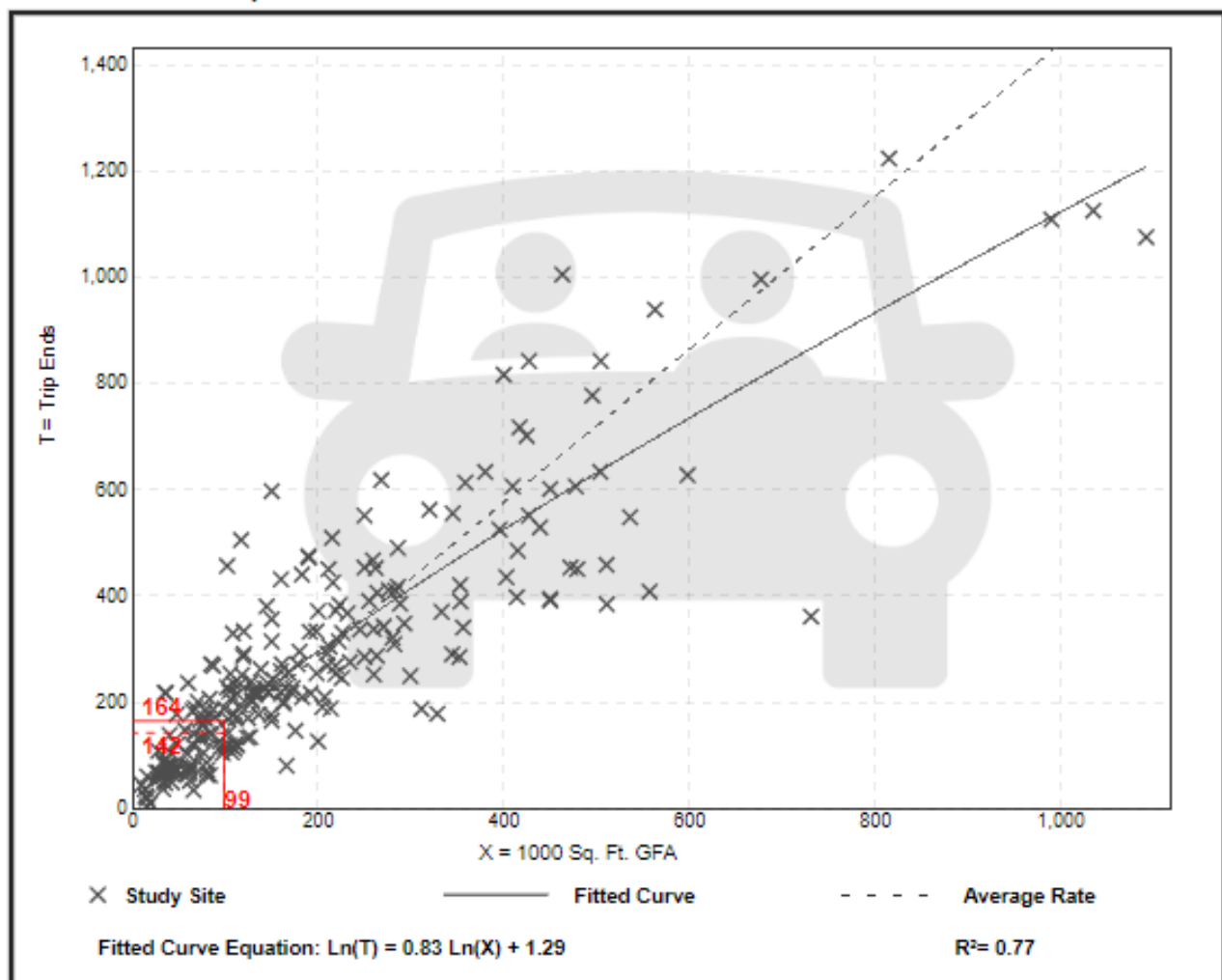
General Office Building (710)

Vehicle Trip Ends vs: 1000 Sq. Ft. GFA
 On a: Weekday,
 Peak Hour of Adjacent Street Traffic,
 One Hour Between 4 and 6 p.m.
 Setting/Location: General Urban/Suburban
 Number of Studies: 232
 Avg. 1000 Sq. Ft. GFA: 199
 Directional Distribution: 17% entering, 83% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
1.44	0.26 - 6.20	0.60

Data Plot and Equation



Strip Retail Plaza (<40k) (822)

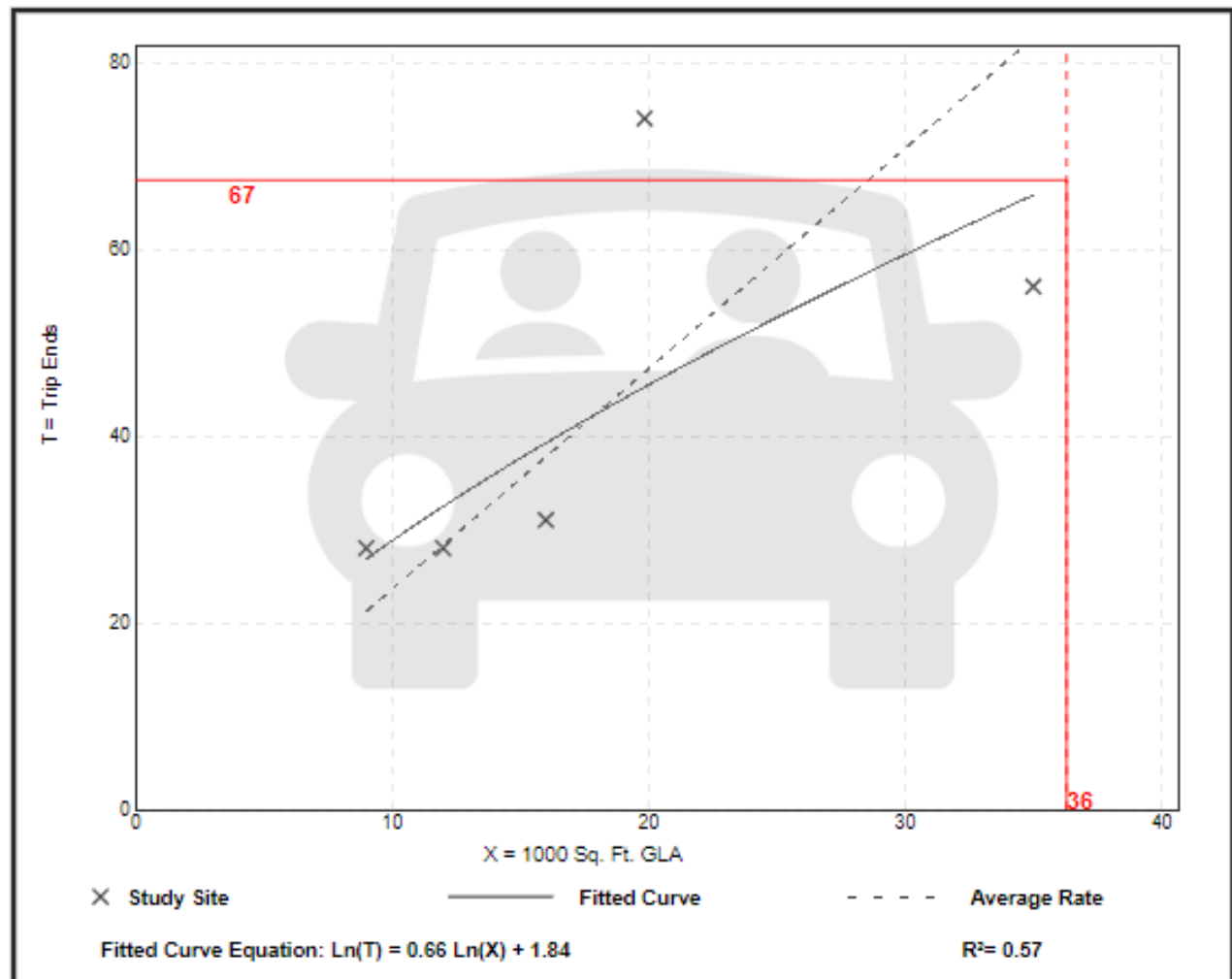
Vehicle Trip Ends vs: 1000 Sq. Ft. GLA
 On a: Weekday,
 Peak Hour of Adjacent Street Traffic,
 One Hour Between 7 and 9 a.m.
 Setting/Location: General Urban/Suburban
 Number of Studies: 5
 Avg. 1000 Sq. Ft. GLA: 18
 Directional Distribution: 60% entering, 40% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GLA

Average Rate	Range of Rates	Standard Deviation
2.36	1.60 - 3.73	0.94

Data Plot and Equation

Caution – Small Sample Size



Strip Retail Plaza (<40k) (822)

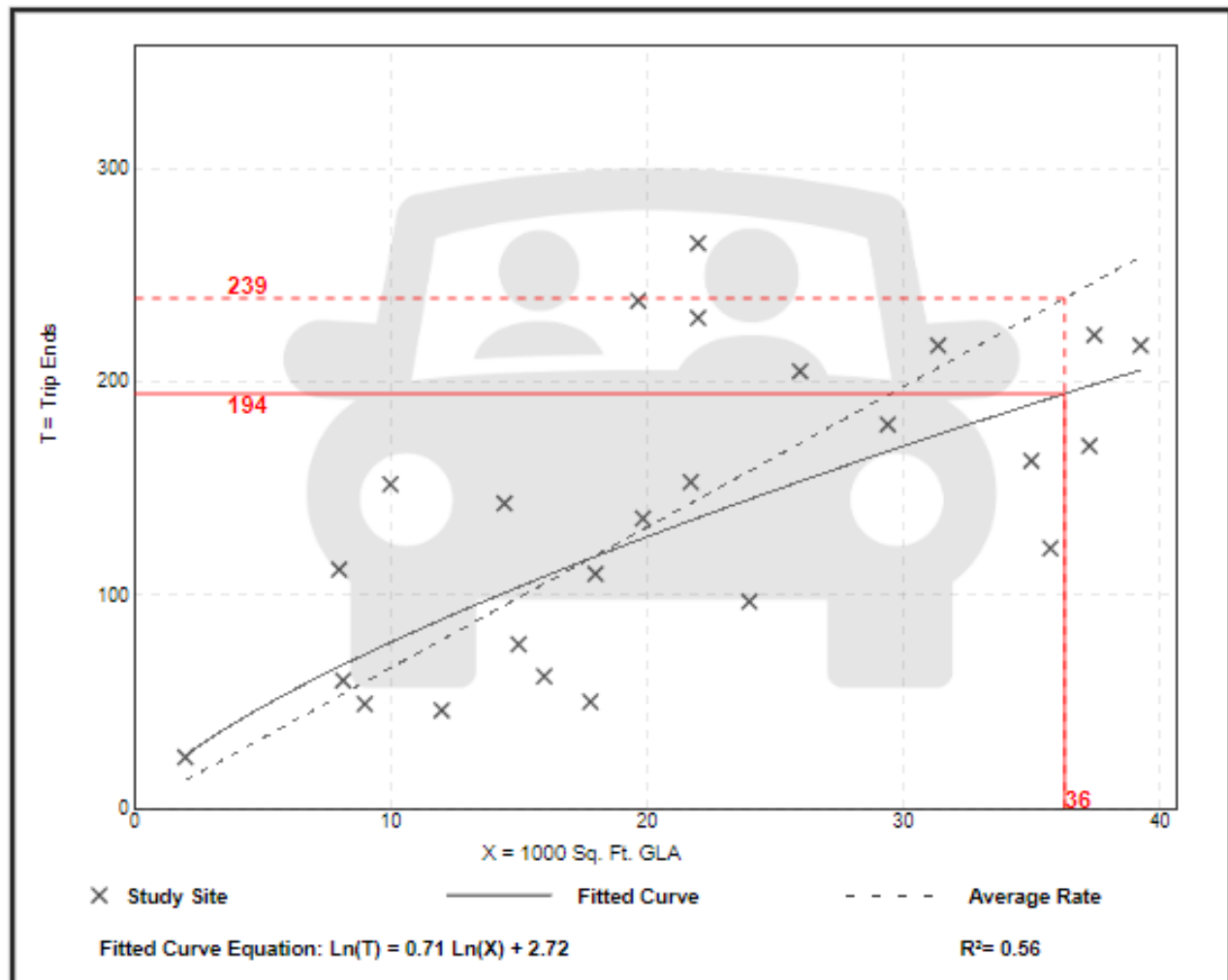
Vehicle Trip Ends vs: 1000 Sq. Ft. GLA
 On a: Weekday,
 Peak Hour of Adjacent Street Traffic,
 One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban
 Number of Studies: 25
 Avg. 1000 Sq. Ft. GLA: 21
 Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GLA

Average Rate	Range of Rates	Standard Deviation
6.59	2.81 - 15.20	2.94

Data Plot and Equation



APPENDIX C - CAPACITY AND QUEUING ANALYSES

- 50th Street and Adams Street
 - AM Existing
 - PM Existing
 - AM Existing+Proposed
 - PM Existing+Proposed
- 50th Street and Rainbow Boulevard
 - AM Existing
 - AM Existing Queues
 - PM Existing
 - PM Existing Queues
 - AM Existing+Proposed
 - AM Existing+Proposed Queues
 - PM Existing+Proposed
 - PM Existing+Proposed Queues
- 50th Terrace and Rainbow Boulevard
 - AM Existing
 - PM Existing
 - AM Existing+Proposed
 - PM Existing+Proposed
- 51st Street and Rainbow Boulevard
 - AM Existing
 - PM Existing
 - AM Existing+Proposed
 - PM Existing+Proposed
- 51st Street and East Drive
 - AM Existing+Proposed
 - PM Existing+Proposed
- 51st and West Drive
 - AM Existing
 - PM Existing
 - AM Existing+Proposed
 - PM Existing+Proposed

Intersection

Int Delay, s/veh 1.2

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	0	31	0	0	11	0	0	0	0	2	0	5
Future Vol, veh/h	0	31	0	0	11	0	0	0	0	2	0	5
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	0	34	0	0	12	0	0	0	0	2	0	5

Major/Minor	Major1	Major2	Minor1	Minor2
Conflicting Flow All	12	0	0	34
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Critical Hdwy	4.12	-	-	4.12
Critical Hdwy Stg 1	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-
Follow-up Hdwy	2.218	-	-	2.218
Pot Cap-1 Maneuver	1607	-	-	1578
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Platoon blocked, %	-	-	-	-
Mov Cap-1 Maneuver	1607	-	-	1578
Mov Cap-2 Maneuver	-	-	-	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	0	0	0	8.5
HCM LOS			A	A

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	-	1607	-	-	1578	-	-	1034
HCM Lane V/C Ratio	-	-	-	-	-	-	-	0.007
HCM Control Delay (s)	0	0	-	-	0	-	-	8.5
HCM Lane LOS	A	A	-	-	A	-	-	A
HCM 95th %tile Q(veh)	-	0	-	-	0	-	-	0

Intersection												
Int Delay, s/veh	1											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	1	37	0	0	42	4	0	0	0	4	0	6
Future Vol, veh/h	1	37	0	0	42	4	0	0	0	4	0	6
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	1	40	0	0	46	4	0	0	0	4	0	7

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	50	0	0	40	0	0	94	92	40	90	90	48
Stage 1	-	-	-	-	-	-	42	42	-	48	48	-
Stage 2	-	-	-	-	-	-	52	50	-	42	42	-
Critical Hdwy	4.12	-	-	4.12	-	-	7.12	6.52	6.22	7.12	6.52	6.22
Critical Hdwy Stg 1	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.12	5.52	-	6.12	5.52	-
Follow-up Hdwy	2.218	-	-	2.218	-	-	3.518	4.018	3.318	3.518	4.018	3.318
Pot Cap-1 Maneuver	1557	-	-	1570	-	-	889	798	1031	895	800	1021
Stage 1	-	-	-	-	-	-	972	860	-	965	855	-
Stage 2	-	-	-	-	-	-	961	853	-	972	860	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	1557	-	-	1570	-	-	883	797	1031	894	799	1021
Mov Cap-2 Maneuver	-	-	-	-	-	-	883	797	-	894	799	-
Stage 1	-	-	-	-	-	-	971	859	-	964	855	-
Stage 2	-	-	-	-	-	-	955	853	-	971	859	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	0.2	0	0	8.8
HCM LOS			A	A

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	-	1557	-	-	1570	-	-	966
HCM Lane V/C Ratio	-	0.001	-	-	-	-	-	0.011
HCM Control Delay (s)	0	7.3	0	-	0	-	-	8.8
HCM Lane LOS	A	A	A	-	A	-	-	A
HCM 95th %tile Q(veh)	-	0	-	-	0	-	-	0

Intersection

Int Delay, s/veh 4.3

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	0	37	3	20	11	0	4	0	26	2	0	5
Future Vol, veh/h	0	37	3	20	11	0	4	0	26	2	0	5
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	0	40	3	22	12	0	4	0	28	2	0	5

Major/Minor	Major1	Major2	Minor1	Minor2
Conflicting Flow All	12	0	0	43
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Critical Hdwy	4.12	-	-	4.12
Critical Hdwy Stg 1	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-
Follow-up Hdwy	2.218	-	-	2.218
Pot Cap-1 Maneuver	1607	-	-	1566
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Platoon blocked, %	-	-	-	-
Mov Cap-1 Maneuver	1607	-	-	1566
Mov Cap-2 Maneuver	-	-	-	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	0	4.7	8.7	8.7
HCM LOS			A	A

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	1004	1607	-	-	1566	-	-	989
HCM Lane V/C Ratio	0.032	-	-	-	0.014	-	-	0.008
HCM Control Delay (s)	8.7	0	-	-	7.3	0	-	8.7
HCM Lane LOS	A	A	-	-	A	A	-	A
HCM 95th %tile Q(veh)	0.1	0	-	-	0	-	-	0

Intersection

Int Delay, s/veh 3.2

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	1	41	2	14	42	4	4	0	26	4	0	6
Future Vol, veh/h	1	41	2	14	42	4	4	0	26	4	0	6
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	1	45	2	15	46	4	4	0	28	4	0	7

Major/Minor	Major1	Major2	Minor1	Minor2
Conflicting Flow All	50	0	0	47
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Critical Hdwy	4.12	-	-	4.12
Critical Hdwy Stg 1	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-
Follow-up Hdwy	2.218	-	-	2.218
Pot Cap-1 Maneuver	1557	-	-	1560
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Platoon blocked, %	-	-	-	-
Mov Cap-1 Maneuver	1557	-	-	1560
Mov Cap-2 Maneuver	-	-	-	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	0.2	1.7	8.8	9
HCM LOS			A	A

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1
Capacity (veh/h)	992	1557	-	-	1560	-	-	919
HCM Lane V/C Ratio	0.033	0.001	-	-	0.01	-	-	0.012
HCM Control Delay (s)	8.8	7.3	0	-	7.3	0	-	9
HCM Lane LOS	A	A	A	-	A	A	-	A
HCM 95th %tile Q(veh)	0.1	0	-	-	0	-	-	0

4: Rainbow & 50th St
Timings

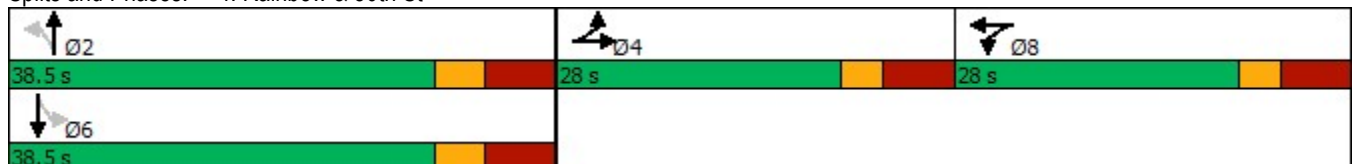


Lane Group	EBT	WBT	NBL	NBT	SBL	SBT
Lane Configurations	↕	↕		↕		↕
Traffic Volume (vph)	6	0	1	473	5	371
Future Volume (vph)	6	0	1	473	5	371
Turn Type	NA	NA	Perm	NA	Perm	NA
Protected Phases	4	8		2		6
Permitted Phases			2		6	
Detector Phase	4	8	2	2	6	6
Switch Phase						
Minimum Initial (s)	4.0	4.0	4.0	4.0	4.0	4.0
Minimum Split (s)	24.0	24.0	24.5	24.5	24.5	24.5
Total Split (s)	28.0	28.0	38.5	38.5	38.5	38.5
Total Split (%)	29.6%	29.6%	40.7%	40.7%	40.7%	40.7%
Yellow Time (s)	3.0	3.0	3.5	3.5	3.5	3.5
All-Red Time (s)	5.0	5.0	5.0	5.0	5.0	5.0
Lost Time Adjust (s)	0.0	0.0		0.0		0.0
Total Lost Time (s)	8.0	8.0		8.5		8.5
Lead/Lag						
Lead-Lag Optimize?						
Recall Mode	None	None	Max	Max	Max	Max
Act Effect Green (s)	6.6	5.6		48.7		48.7
Actuated g/C Ratio	0.11	0.09		0.82		0.82
v/c Ratio	0.19	0.03		0.19		0.15
Control Delay	22.3	0.1		5.2		5.1
Queue Delay	0.0	0.0		0.0		0.0
Total Delay	22.3	0.1		5.2		5.1
LOS	C	A		A		A
Approach Delay	22.3	0.1		5.2		5.1
Approach LOS	C	A		A		A

Intersection Summary

Cycle Length: 94.5
 Actuated Cycle Length: 59.5
 Natural Cycle: 75
 Control Type: Actuated-Uncoordinated
 Maximum v/c Ratio: 0.19
 Intersection Signal Delay: 5.8
 Intersection LOS: A
 Intersection Capacity Utilization 31.2%
 ICU Level of Service A
 Analysis Period (min) 15

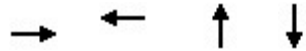
Splits and Phases: 4: Rainbow & 50th St



4: Rainbow & 50th St
 HCM 2010 Signalized Intersection Summary

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	17	6	12	4	0	5	1	473	1	5	371	10
Future Volume (veh/h)	17	6	12	4	0	5	1	473	1	5	371	10
Number	7	4	14	3	8	18	5	2	12	1	6	16
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Adj Sat Flow, veh/h/ln	1900	1863	1900	1900	1863	1900	1900	1863	1900	1900	1863	1900
Adj Flow Rate, veh/h	18	7	13	4	0	5	1	514	1	5	403	11
Adj No. of Lanes	0	1	0	0	1	0	0	2	0	0	2	0
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	26	10	19	7	0	9	64	1873	4	69	1811	49
Arrive On Green	0.03	0.03	0.03	0.01	0.00	0.01	0.53	0.53	0.53	0.53	0.53	0.53
Sat Flow, veh/h	814	317	588	739	0	924	1	3548	7	8	3431	93
Grp Volume(v), veh/h	38	0	0	9	0	0	270	0	246	220	0	199
Grp Sat Flow(s),veh/h/ln	1718	0	0	1663	0	0	1862	0	1694	1854	0	1679
Q Serve(g_s), s	1.2	0.0	0.0	0.3	0.0	0.0	0.0	0.0	4.6	0.0	0.0	3.6
Cycle Q Clear(g_c), s	1.2	0.0	0.0	0.3	0.0	0.0	4.6	0.0	4.6	3.6	0.0	3.6
Prop In Lane	0.47		0.34	0.44		0.56	0.00		0.00	0.02		0.06
Lane Grp Cap(c), veh/h	55	0	0	16	0	0	1046	0	894	1043	0	886
V/C Ratio(X)	0.70	0.00	0.00	0.58	0.00	0.00	0.26	0.00	0.27	0.21	0.00	0.22
Avail Cap(c_a), veh/h	605	0	0	585	0	0	1046	0	894	1043	0	886
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	0.00	1.00	0.00	0.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	27.2	0.0	0.0	28.0	0.0	0.0	7.4	0.0	7.4	7.2	0.0	7.2
Incr Delay (d2), s/veh	14.8	0.0	0.0	30.0	0.0	0.0	0.6	0.0	0.8	0.5	0.0	0.6
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.8	0.0	0.0	0.3	0.0	0.0	2.5	0.0	2.3	2.0	0.0	1.8
LnGrp Delay(d),s/veh	42.0	0.0	0.0	58.0	0.0	0.0	8.0	0.0	8.2	7.6	0.0	7.8
LnGrp LOS	D			E			A		A	A		A
Approach Vol, veh/h		38			9			516			419	
Approach Delay, s/veh		42.0			58.0			8.1			7.7	
Approach LOS		D			E			A			A	
Timer	1	2	3	4	5	6	7	8				
Assigned Phs		2		4		6		8				
Phs Duration (G+Y+Rc), s		38.5		9.8		38.5		8.5				
Change Period (Y+Rc), s		8.5		8.0		8.5		8.0				
Max Green Setting (Gmax), s		30.0		20.0		30.0		20.0				
Max Q Clear Time (g_c+I1), s		6.6		3.2		5.6		2.3				
Green Ext Time (p_c), s		3.0		0.1		2.4		0.0				
Intersection Summary												
HCM 2010 Ctrl Delay			9.7									
HCM 2010 LOS			A									

4: Rainbow & 50th St
Queues



Lane Group	EBT	WBT	NBT	SBT
Lane Group Flow (vph)	38	9	516	419
v/c Ratio	0.19	0.03	0.19	0.15
Control Delay	22.3	0.1	5.2	5.1
Queue Delay	0.0	0.0	0.0	0.0
Total Delay	22.3	0.1	5.2	5.1
Queue Length 50th (ft)	7	0	0	0
Queue Length 95th (ft)	35	0	105	85
Internal Link Dist (ft)	236	436	184	566
Turn Bay Length (ft)				
Base Capacity (vph)	600	677	2764	2739
Starvation Cap Reductn	0	0	0	0
Spillback Cap Reductn	0	0	0	0
Storage Cap Reductn	0	0	0	0
Reduced v/c Ratio	0.06	0.01	0.19	0.15
Intersection Summary				

4: Rainbow & 50th St
 HCM 2010 Signalized Intersection Summary

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	17	6	12	4	0	5	1	473	1	5	371	10
Future Volume (veh/h)	17	6	12	4	0	5	1	473	1	5	371	10
Number	7	4	14	3	8	18	5	2	12	1	6	16
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Adj Sat Flow, veh/h/ln	1900	1863	1900	1900	1863	1900	1900	1863	1900	1900	1863	1900
Adj Flow Rate, veh/h	18	7	13	4	0	5	1	514	1	5	403	11
Adj No. of Lanes	0	1	0	0	1	0	0	2	0	0	2	0
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	26	10	19	7	0	9	64	1873	4	69	1811	49
Arrive On Green	0.03	0.03	0.03	0.01	0.00	0.01	0.53	0.53	0.53	0.53	0.53	0.53
Sat Flow, veh/h	814	317	588	739	0	924	1	3548	7	8	3431	93
Grp Volume(v), veh/h	38	0	0	9	0	0	270	0	246	220	0	199
Grp Sat Flow(s),veh/h/ln	1718	0	0	1663	0	0	1862	0	1694	1854	0	1679
Q Serve(g_s), s	1.2	0.0	0.0	0.3	0.0	0.0	0.0	0.0	4.6	0.0	0.0	3.6
Cycle Q Clear(g_c), s	1.2	0.0	0.0	0.3	0.0	0.0	4.6	0.0	4.6	3.6	0.0	3.6
Prop In Lane	0.47		0.34	0.44		0.56	0.00		0.00	0.02		0.06
Lane Grp Cap(c), veh/h	55	0	0	16	0	0	1046	0	894	1043	0	886
V/C Ratio(X)	0.70	0.00	0.00	0.58	0.00	0.00	0.26	0.00	0.27	0.21	0.00	0.22
Avail Cap(c_a), veh/h	605	0	0	585	0	0	1046	0	894	1043	0	886
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	0.00	1.00	0.00	0.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	27.2	0.0	0.0	28.0	0.0	0.0	7.4	0.0	7.4	7.2	0.0	7.2
Incr Delay (d2), s/veh	14.8	0.0	0.0	30.0	0.0	0.0	0.6	0.0	0.8	0.5	0.0	0.6
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.8	0.0	0.0	0.3	0.0	0.0	2.5	0.0	2.3	2.0	0.0	1.8
LnGrp Delay(d),s/veh	42.0	0.0	0.0	58.0	0.0	0.0	8.0	0.0	8.2	7.6	0.0	7.8
LnGrp LOS	D			E			A		A	A		A
Approach Vol, veh/h		38			9			516				419
Approach Delay, s/veh		42.0			58.0			8.1				7.7
Approach LOS		D			E			A				A
Timer	1	2	3	4	5	6	7	8				
Assigned Phs		2		4		6		8				
Phs Duration (G+Y+Rc), s		38.5		9.8		38.5		8.5				
Change Period (Y+Rc), s		8.5		8.0		8.5		8.0				
Max Green Setting (Gmax), s		30.0		20.0		30.0		20.0				
Max Q Clear Time (g_c+I1), s		6.6		3.2		5.6		2.3				
Green Ext Time (p_c), s		3.0		0.1		2.4		0.0				
Intersection Summary												
HCM 2010 Ctrl Delay				9.7								
HCM 2010 LOS				A								

4: Rainbow & 50th St
Timings

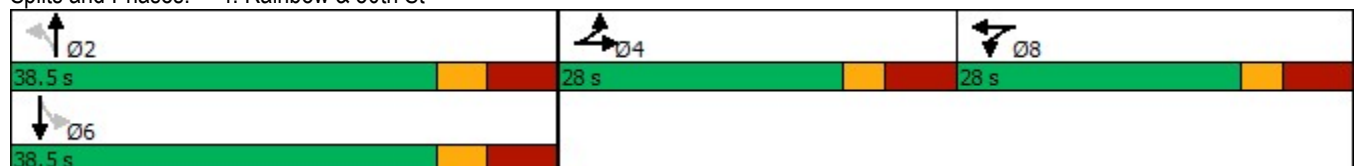


Lane Group	EBT	WBT	NBL	NBT	SBL	SBT
Lane Configurations	↕	↕		↕		↕
Traffic Volume (vph)	7	10	7	294	4	763
Future Volume (vph)	7	10	7	294	4	763
Turn Type	NA	NA	Perm	NA	Perm	NA
Protected Phases	4	8		2		6
Permitted Phases			2		6	
Detector Phase	4	8	2	2	6	6
Switch Phase						
Minimum Initial (s)	4.0	4.0	4.0	4.0	4.0	4.0
Minimum Split (s)	24.0	24.0	24.5	24.5	24.5	24.5
Total Split (s)	28.0	28.0	38.5	38.5	38.5	38.5
Total Split (%)	29.6%	29.6%	40.7%	40.7%	40.7%	40.7%
Yellow Time (s)	3.0	3.0	3.5	3.5	3.5	3.5
All-Red Time (s)	5.0	5.0	5.0	5.0	5.0	5.0
Lost Time Adjust (s)	0.0	0.0		0.0		0.0
Total Lost Time (s)	8.0	8.0		8.5		8.5
Lead/Lag						
Lead-Lag Optimize?						
Recall Mode	None	None	Max	Max	Max	Max
Act Effect Green (s)	6.9	6.3		44.4		44.4
Actuated g/C Ratio	0.11	0.10		0.70		0.70
v/c Ratio	0.22	0.16		0.14		0.37
Control Delay	26.9	22.5		8.4		9.8
Queue Delay	0.0	0.0		0.0		0.0
Total Delay	26.9	22.5		8.4		9.8
LOS	C	C		A		A
Approach Delay	26.9	22.5		8.4		9.8
Approach LOS	C	C		A		A

Intersection Summary

Cycle Length: 94.5
 Actuated Cycle Length: 63.1
 Natural Cycle: 80
 Control Type: Actuated-Uncoordinated
 Maximum v/c Ratio: 0.37
 Intersection Signal Delay: 10.3
 Intersection Capacity Utilization 44.8%
 Analysis Period (min) 15
 Intersection LOS: B
 ICU Level of Service A

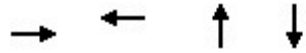
Splits and Phases: 4: Rainbow & 50th St



4: Rainbow & 50th St
 HCM 2010 Signalized Intersection Summary

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	26	7	7	5	10	13	7	294	3	4	763	27
Future Volume (veh/h)	26	7	7	5	10	13	7	294	3	4	763	27
Number	7	4	14	3	8	18	5	2	12	1	6	16
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Adj Sat Flow, veh/h/ln	1900	1863	1900	1900	1863	1900	1900	1863	1900	1900	1863	1900
Adj Flow Rate, veh/h	28	8	8	5	11	14	8	320	3	4	829	29
Adj No. of Lanes	0	1	0	0	1	0	0	2	0	0	2	0
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	39	11	11	8	17	21	77	1783	17	64	1763	61
Arrive On Green	0.04	0.04	0.04	0.03	0.03	0.03	0.52	0.52	0.52	0.52	0.52	0.52
Sat Flow, veh/h	1114	318	318	285	626	797	24	3451	32	3	3413	119
Grp Volume(v), veh/h	44	0	0	30	0	0	173	0	158	454	0	408
Grp Sat Flow(s),veh/h/ln	1751	0	0	1708	0	0	1817	0	1689	1860	0	1674
Q Serve(g_s), s	1.4	0.0	0.0	1.0	0.0	0.0	0.0	0.0	2.9	0.0	0.0	9.1
Cycle Q Clear(g_c), s	1.4	0.0	0.0	1.0	0.0	0.0	2.9	0.0	2.9	9.0	0.0	9.1
Prop In Lane	0.64		0.18	0.17		0.47	0.05		0.02	0.01		0.07
Lane Grp Cap(c), veh/h	61	0	0	45	0	0	1004	0	873	1024	0	865
V/C Ratio(X)	0.72	0.00	0.00	0.66	0.00	0.00	0.17	0.00	0.18	0.44	0.00	0.47
Avail Cap(c_a), veh/h	603	0	0	588	0	0	1004	0	873	1024	0	865
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	0.00	1.00	0.00	0.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	27.7	0.0	0.0	28.0	0.0	0.0	7.5	0.0	7.5	9.0	0.0	9.0
Incr Delay (d2), s/veh	14.5	0.0	0.0	15.5	0.0	0.0	0.4	0.0	0.5	1.4	0.0	1.8
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.9	0.0	0.0	0.7	0.0	0.0	1.5	0.0	1.4	4.9	0.0	4.5
LnGrp Delay(d),s/veh	42.2	0.0	0.0	43.5	0.0	0.0	7.8	0.0	7.9	10.4	0.0	10.8
LnGrp LOS	D			D			A		A	B		B
Approach Vol, veh/h		44			30			331				862
Approach Delay, s/veh		42.2			43.5			7.9				10.6
Approach LOS		D			D			A				B
Timer	1	2	3	4	5	6	7	8				
Assigned Phs		2		4		6		8				
Phs Duration (G+Y+Rc), s		38.5		10.0		38.5		9.5				
Change Period (Y+Rc), s		8.5		8.0		8.5		8.0				
Max Green Setting (Gmax), s		30.0		20.0		30.0		20.0				
Max Q Clear Time (g_c+I1), s		4.9		3.4		11.1		3.0				
Green Ext Time (p_c), s		1.9		0.1		5.2		0.1				
Intersection Summary												
HCM 2010 Ctrl Delay				11.8								
HCM 2010 LOS				B								

4: Rainbow & 50th St
Queues



Lane Group	EBT	WBT	NBT	SBT
Lane Group Flow (vph)	44	30	331	862
v/c Ratio	0.22	0.16	0.14	0.37
Control Delay	26.9	22.5	8.4	9.8
Queue Delay	0.0	0.0	0.0	0.0
Total Delay	26.9	22.5	8.4	9.8
Queue Length 50th (ft)	14	6	23	73
Queue Length 95th (ft)	42	30	73	205
Internal Link Dist (ft)	236	436	178	566
Turn Bay Length (ft)				
Base Capacity (vph)	572	567	2315	2361
Starvation Cap Reductn	0	0	0	0
Spillback Cap Reductn	0	0	0	0
Storage Cap Reductn	0	0	0	0
Reduced v/c Ratio	0.08	0.05	0.14	0.37
Intersection Summary				

4: Rainbow & 50th St
Timings

AM_Existing+P

Item A, Section III, Item

08/13/2023

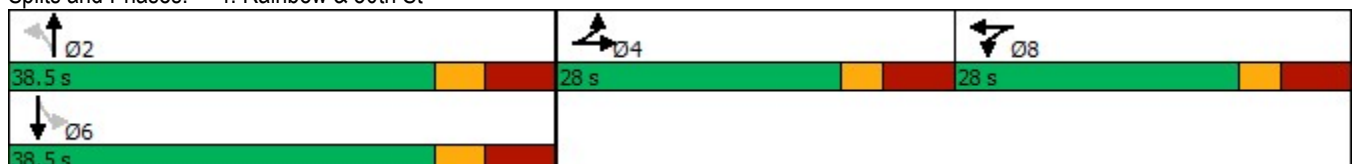


Lane Group	EBT	WBT	NBL	NBT	SBL	SBT
Lane Configurations	↔	↔		↕		↕
Traffic Volume (vph)	7	3	2	487	5	423
Future Volume (vph)	7	3	2	487	5	423
Turn Type	NA	NA	Perm	NA	Perm	NA
Protected Phases	4	8		2		6
Permitted Phases			2		6	
Detector Phase	4	8	2	2	6	6
Switch Phase						
Minimum Initial (s)	4.0	4.0	4.0	4.0	4.0	4.0
Minimum Split (s)	24.0	24.0	24.5	24.5	24.5	24.5
Total Split (s)	28.0	28.0	38.5	38.5	38.5	38.5
Total Split (%)	29.6%	29.6%	40.7%	40.7%	40.7%	40.7%
Yellow Time (s)	3.0	3.0	3.5	3.5	3.5	3.5
All-Red Time (s)	5.0	5.0	5.0	5.0	5.0	5.0
Lost Time Adjust (s)	0.0	0.0		0.0		0.0
Total Lost Time (s)	8.0	8.0		8.5		8.5
Lead/Lag						
Lead-Lag Optimize?						
Recall Mode	None	None	Max	Max	Max	Max
Act Effect Green (s)	6.8	6.2		46.0		46.0
Actuated g/C Ratio	0.11	0.10		0.74		0.74
v/c Ratio	0.25	0.11		0.21		0.20
Control Delay	22.1	25.8		6.7		6.6
Queue Delay	0.0	0.0		0.0		0.0
Total Delay	22.1	25.8		6.7		6.6
LOS	C	C		A		A
Approach Delay	22.1	25.8		6.7		6.6
Approach LOS	C	C		A		A

Intersection Summary

Cycle Length: 94.5
 Actuated Cycle Length: 62
 Natural Cycle: 75
 Control Type: Actuated-Uncoordinated
 Maximum v/c Ratio: 0.25
 Intersection Signal Delay: 7.7
 Intersection LOS: A
 Intersection Capacity Utilization 33.2%
 ICU Level of Service A
 Analysis Period (min) 15

Splits and Phases: 4: Rainbow & 50th St



4: Rainbow & 50th St
 HCM 2010 Signalized Intersection Summary

AM_Existing+P

Item A, Section III, Item

08/13/2023

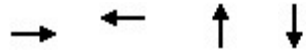
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	21	7	18	10	3	5	2	487	2	5	423	27
Future Volume (veh/h)	21	7	18	10	3	5	2	487	2	5	423	27
Number	7	4	14	3	8	18	5	2	12	1	6	16
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Adj Sat Flow, veh/h/ln	1900	1863	1900	1900	1863	1900	1900	1863	1900	1900	1863	1900
Adj Flow Rate, veh/h	23	8	20	11	3	5	2	529	2	5	460	29
Adj No. of Lanes	0	1	0	0	1	0	0	2	0	0	2	0
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	30	10	26	18	5	8	64	1837	7	67	1713	107
Arrive On Green	0.04	0.04	0.04	0.02	0.02	0.02	0.52	0.52	0.52	0.52	0.52	0.52
Sat Flow, veh/h	769	268	669	1003	274	456	2	3538	13	7	3300	206
Grp Volume(v), veh/h	51	0	0	19	0	0	279	0	254	260	0	234
Grp Sat Flow(s),veh/h/ln	1706	0	0	1732	0	0	1861	0	1693	1855	0	1659
Q Serve(g_s), s	1.7	0.0	0.0	0.6	0.0	0.0	0.0	0.0	4.9	0.0	0.0	4.6
Cycle Q Clear(g_c), s	1.7	0.0	0.0	0.6	0.0	0.0	4.9	0.0	4.9	4.5	0.0	4.6
Prop In Lane	0.45		0.39	0.58		0.26	0.01		0.01	0.02		0.12
Lane Grp Cap(c), veh/h	66	0	0	32	0	0	1029	0	879	1026	0	861
V/C Ratio(X)	0.77	0.00	0.00	0.60	0.00	0.00	0.27	0.00	0.29	0.25	0.00	0.27
Avail Cap(c_a), veh/h	591	0	0	599	0	0	1029	0	879	1026	0	861
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	0.00	1.00	0.00	0.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	27.5	0.0	0.0	28.2	0.0	0.0	7.9	0.0	7.9	7.8	0.0	7.8
Incr Delay (d2), s/veh	17.1	0.0	0.0	17.1	0.0	0.0	0.7	0.0	0.8	0.6	0.0	0.8
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	1.1	0.0	0.0	0.4	0.0	0.0	2.7	0.0	2.5	2.5	0.0	2.3
LnGrp Delay(d),s/veh	44.7	0.0	0.0	45.3	0.0	0.0	8.5	0.0	8.7	8.4	0.0	8.6
LnGrp LOS	D			D			A		A	A		A
Approach Vol, veh/h		51			19			533			494	
Approach Delay, s/veh		44.7			45.3			8.6			8.5	
Approach LOS		D			D			A			A	
Timer	1	2	3	4	5	6	7	8				
Assigned Phs		2		4		6		8				
Phs Duration (G+Y+Rc), s		38.5		10.2		38.5		9.1				
Change Period (Y+Rc), s		8.5		8.0		8.5		8.0				
Max Green Setting (Gmax), s		30.0		20.0		30.0		20.0				
Max Q Clear Time (g_c+I1), s		6.9		3.7		6.6		2.6				
Green Ext Time (p_c), s		3.1		0.2		2.9		0.0				
Intersection Summary												
HCM 2010 Ctrl Delay				10.8								
HCM 2010 LOS				B								

4: Rainbow & 50th St
Queues

AM_Existing+P

Item A, Section III, Item

08/15/2023



Lane Group	EBT	WBT	NBT	SBT
Lane Group Flow (vph)	51	19	533	494
v/c Ratio	0.25	0.11	0.21	0.20
Control Delay	22.1	25.8	6.7	6.6
Queue Delay	0.0	0.0	0.0	0.0
Total Delay	22.1	25.8	6.7	6.6
Queue Length 50th (ft)	12	6	39	35
Queue Length 95th (ft)	42	24	117	107
Internal Link Dist (ft)	236	436	184	566
Turn Bay Length (ft)				
Base Capacity (vph)	578	574	2502	2473
Starvation Cap Reductn	0	0	0	0
Spillback Cap Reductn	0	0	0	0
Storage Cap Reductn	0	0	0	0
Reduced v/c Ratio	0.09	0.03	0.21	0.20
Intersection Summary				

4: Rainbow & 50th St
Timings

PM_Existing+P

Item A, Section III, Item

08/13/2023



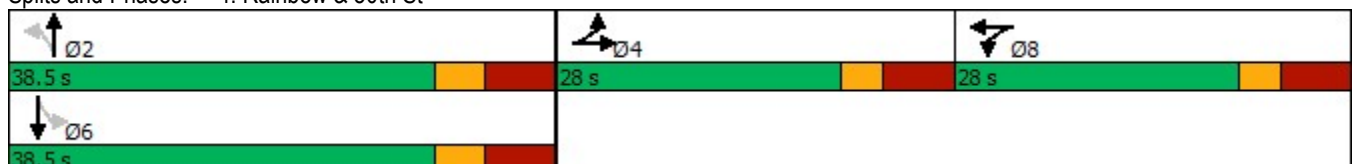
Lane Group	EBT	WBT	NBL	NBT	SBL	SBT
Lane Configurations	↔	↔		↕		↕
Traffic Volume (vph)	11	12	13	359	4	800
Future Volume (vph)	11	12	13	359	4	800
Turn Type	NA	NA	Perm	NA	Perm	NA
Protected Phases	4	8		2		6
Permitted Phases			2		6	
Detector Phase	4	8	2	2	6	6
Switch Phase						
Minimum Initial (s)	4.0	4.0	4.0	4.0	4.0	4.0
Minimum Split (s)	24.0	24.0	24.5	24.5	24.5	24.5
Total Split (s)	28.0	28.0	38.5	38.5	38.5	38.5
Total Split (%)	29.6%	29.6%	40.7%	40.7%	40.7%	40.7%
Yellow Time (s)	3.0	3.0	3.5	3.5	3.5	3.5
All-Red Time (s)	5.0	5.0	5.0	5.0	5.0	5.0
Lost Time Adjust (s)	0.0	0.0		0.0		0.0
Total Lost Time (s)	8.0	8.0		8.5		8.5
Lead/Lag						
Lead-Lag Optimize?						
Recall Mode	None	None	Max	Max	Max	Max
Act Effect Green (s)	8.0	6.6		40.9		40.9
Actuated g/C Ratio	0.12	0.10		0.62		0.62
v/c Ratio	0.34	0.20		0.21		0.44
Control Delay	29.6	23.6		10.0		12.0
Queue Delay	0.0	0.0		0.0		0.0
Total Delay	29.6	23.6		10.0		12.0
LOS	C	C		B		B
Approach Delay	29.6	23.6		10.0		12.0
Approach LOS	C	C		B		B

Intersection Summary

Cycle Length: 94.5
 Actuated Cycle Length: 66
 Natural Cycle: 80
 Control Type: Actuated-Uncoordinated
 Maximum v/c Ratio: 0.44
 Intersection Signal Delay: 12.7
 Intersection Capacity Utilization 48.7%
 Analysis Period (min) 15

Intersection LOS: B
 ICU Level of Service A

Splits and Phases: 4: Rainbow & 50th St



4: Rainbow & 50th St
 HCM 2010 Signalized Intersection Summary

PM_Existing+P

Item A, Section III, Item

08/13/2023

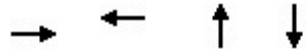
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	48	11	11	9	12	13	13	359	9	4	800	39
Future Volume (veh/h)	48	11	11	9	12	13	13	359	9	4	800	39
Number	7	4	14	3	8	18	5	2	12	1	6	16
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Adj Sat Flow, veh/h/ln	1900	1863	1900	1900	1863	1900	1900	1863	1900	1900	1863	1900
Adj Flow Rate, veh/h	52	12	12	10	13	14	14	390	10	4	870	42
Adj No. of Lanes	0	1	0	0	1	0	0	2	0	0	2	0
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	69	16	16	14	19	20	85	1671	42	62	1687	81
Arrive On Green	0.06	0.06	0.06	0.03	0.03	0.03	0.50	0.50	0.50	0.50	0.50	0.50
Sat Flow, veh/h	1200	277	277	466	606	652	42	3330	84	3	3362	162
Grp Volume(v), veh/h	76	0	0	37	0	0	214	0	200	483	0	433
Grp Sat Flow(s),veh/h/ln	1754	0	0	1724	0	0	1776	0	1680	1860	0	1667
Q Serve(g_s), s	2.6	0.0	0.0	1.3	0.0	0.0	0.0	0.0	4.0	0.0	0.0	10.5
Cycle Q Clear(g_c), s	2.6	0.0	0.0	1.3	0.0	0.0	3.9	0.0	4.0	10.4	0.0	10.5
Prop In Lane	0.68		0.16	0.27		0.38	0.07		0.05	0.01		0.10
Lane Grp Cap(c), veh/h	101	0	0	53	0	0	955	0	843	994	0	836
V/C Ratio(X)	0.75	0.00	0.00	0.70	0.00	0.00	0.22	0.00	0.24	0.49	0.00	0.52
Avail Cap(c_a), veh/h	587	0	0	577	0	0	955	0	843	994	0	836
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	0.00	1.00	0.00	0.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	27.7	0.0	0.0	28.7	0.0	0.0	8.4	0.0	8.4	10.0	0.0	10.0
Incr Delay (d2), s/veh	10.5	0.0	0.0	15.3	0.0	0.0	0.5	0.0	0.7	1.7	0.0	2.3
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	1.5	0.0	0.0	0.8	0.0	0.0	2.1	0.0	2.0	5.7	0.0	5.2
LnGrp Delay(d),s/veh	38.2	0.0	0.0	44.0	0.0	0.0	8.9	0.0	9.1	11.7	0.0	12.3
LnGrp LOS	D			D			A		A	B		B
Approach Vol, veh/h		76			37			414				916
Approach Delay, s/veh		38.2			44.0			9.0				12.0
Approach LOS		D			D			A				B
Timer	1	2	3	4	5	6	7	8				
Assigned Phs		2		4		6		8				
Phs Duration (G+Y+Rc), s		38.5		11.5		38.5		9.8				
Change Period (Y+Rc), s		8.5		8.0		8.5		8.0				
Max Green Setting (Gmax), s		30.0		20.0		30.0		20.0				
Max Q Clear Time (g_c+I1), s		6.0		4.6		12.5		3.3				
Green Ext Time (p_c), s		2.4		0.3		5.4		0.1				
Intersection Summary												
HCM 2010 Ctrl Delay				13.3								
HCM 2010 LOS				B								

4: Rainbow & 50th St
Queues

PM_Existing+P

Item A. Section III, Item

08/15/2023



Lane Group	EBT	WBT	NBT	SBT
Lane Group Flow (vph)	76	37	414	916
v/c Ratio	0.34	0.20	0.21	0.44
Control Delay	29.6	23.6	10.0	12.0
Queue Delay	0.0	0.0	0.0	0.0
Total Delay	29.6	23.6	10.0	12.0
Queue Length 50th (ft)	24	8	32	84
Queue Length 95th (ft)	64	36	97	237
Internal Link Dist (ft)	236	436	178	566
Turn Bay Length (ft)				
Base Capacity (vph)	547	545	1991	2079
Starvation Cap Reductn	0	0	0	0
Spillback Cap Reductn	0	0	0	0
Storage Cap Reductn	0	0	0	0
Reduced v/c Ratio	0.14	0.07	0.21	0.44
Intersection Summary				

Intersection												
Int Delay, s/veh	0.2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	0	0	0	2	0	9	0	468	5	6	379	0
Future Vol, veh/h	0	0	0	2	0	9	0	468	5	6	379	0
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	0	0	0	2	0	10	0	509	5	7	412	0

Major/Minor	Minor2		Minor1			Major1			Major2			
Conflicting Flow All	681	940	206	732	938	257	-	0	0	514	0	0
Stage 1	426	426	-	512	512	-	-	-	-	-	-	-
Stage 2	255	514	-	220	426	-	-	-	-	-	-	-
Critical Hdwy	7.54	6.54	6.94	7.54	6.54	6.94	-	-	-	4.14	-	-
Critical Hdwy Stg 1	6.54	5.54	-	6.54	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.54	5.54	-	6.54	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.52	4.02	3.32	3.52	4.02	3.32	-	-	-	2.22	-	-
Pot Cap-1 Maneuver	336	262	800	309	263	742	0	-	-	1048	-	0
Stage 1	577	584	-	513	535	-	0	-	-	-	-	0
Stage 2	727	534	-	762	584	-	0	-	-	-	-	0
Platoon blocked, %												
Mov Cap-1 Maneuver	329	260	800	307	261	742	-	-	-	1048	-	-
Mov Cap-2 Maneuver	329	260	-	307	261	-	-	-	-	-	-	-
Stage 1	577	579	-	513	535	-	-	-	-	-	-	-
Stage 2	717	534	-	755	579	-	-	-	-	-	-	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	0	11.2	0	0.1
HCM LOS	A	B		

Minor Lane/Major Mvmt	NBT	NBR	EBLn1	WBLn1	SBL	SBT
Capacity (veh/h)	-	-	-	590	1048	-
HCM Lane V/C Ratio	-	-	-	0.02	0.006	-
HCM Control Delay (s)	-	-	0	11.2	8.5	0
HCM Lane LOS	-	-	A	B	A	A
HCM 95th %tile Q(veh)	-	-	-	0.1	0	-

Intersection						
Int Delay, s/veh	0.3					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	7	19	292	3	2	773
Future Vol, veh/h	7	19	292	3	2	773
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	8	21	317	3	2	840

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	743	160	0	0	320
Stage 1	319	-	-	-	-
Stage 2	424	-	-	-	-
Critical Hdwy	6.84	6.94	-	-	4.14
Critical Hdwy Stg 1	5.84	-	-	-	-
Critical Hdwy Stg 2	5.84	-	-	-	-
Follow-up Hdwy	3.52	3.32	-	-	2.22
Pot Cap-1 Maneuver	351	857	-	-	1237
Stage 1	710	-	-	-	-
Stage 2	628	-	-	-	-
Platoon blocked, %					
Mov Cap-1 Maneuver	350	857	-	-	1237
Mov Cap-2 Maneuver	350	-	-	-	-
Stage 1	710	-	-	-	-
Stage 2	626	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	11.1	0	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	617	1237
HCM Lane V/C Ratio	-	-	0.046	0.002
HCM Control Delay (s)	-	-	11.1	7.9
HCM Lane LOS	-	-	B	A
HCM 95th %tile Q(veh)	-	-	0.1	0

Intersection												
Int Delay, s/veh	0.9											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	12	2	7	2	8	9	26	468	5	6	379	64
Future Vol, veh/h	12	2	7	2	8	9	26	468	5	6	379	64
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	13	2	8	2	9	10	28	509	5	7	412	70

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	776	1031	241	789	1064	257	482	0	0	514	0	0
Stage 1	461	461	-	568	568	-	-	-	-	-	-	-
Stage 2	315	570	-	221	496	-	-	-	-	-	-	-
Critical Hdwy	7.54	6.54	6.94	7.54	6.54	6.94	4.14	-	-	4.14	-	-
Critical Hdwy Stg 1	6.54	5.54	-	6.54	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.54	5.54	-	6.54	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.52	4.02	3.32	3.52	4.02	3.32	2.22	-	-	2.22	-	-
Pot Cap-1 Maneuver	287	232	760	281	221	742	1077	-	-	1048	-	-
Stage 1	550	564	-	475	505	-	-	-	-	-	-	-
Stage 2	671	504	-	761	544	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	265	222	760	267	211	742	1077	-	-	1048	-	-
Mov Cap-2 Maneuver	265	222	-	267	211	-	-	-	-	-	-	-
Stage 1	530	559	-	458	487	-	-	-	-	-	-	-
Stage 2	627	486	-	744	539	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s	16.7		16.6		0.4		0.1	
HCM LOS	C		C					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	1077	-	-	331	330	1048	-	-
HCM Lane V/C Ratio	0.026	-	-	0.069	0.063	0.006	-	-
HCM Control Delay (s)	8.4	-	-	16.7	16.6	8.5	0	-
HCM Lane LOS	A	-	-	C	C	A	A	-
HCM 95th %tile Q(veh)	0.1	-	-	0.2	0.2	0	-	-

Intersection												
Int Delay, s/veh	3.5											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	55	11	32	7	6	19	18	292	3	2	773	45
Future Vol, veh/h	55	11	32	7	6	19	18	292	3	2	773	45
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2	2	2	2	2	2	2
Mvmt Flow	60	12	35	8	7	21	20	317	3	2	840	49

Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	1071	1229	445	789	1252	160	889	0	0	320	0	0
Stage 1	869	869	-	359	359	-	-	-	-	-	-	-
Stage 2	202	360	-	430	893	-	-	-	-	-	-	-
Critical Hdwy	7.54	6.54	6.94	7.54	6.54	6.94	4.14	-	-	4.14	-	-
Critical Hdwy Stg 1	6.54	5.54	-	6.54	5.54	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.54	5.54	-	6.54	5.54	-	-	-	-	-	-	-
Follow-up Hdwy	3.52	4.02	3.32	3.52	4.02	3.32	2.22	-	-	2.22	-	-
Pot Cap-1 Maneuver	175	177	561	281	171	857	758	-	-	1237	-	-
Stage 1	313	367	-	632	626	-	-	-	-	-	-	-
Stage 2	781	625	-	574	358	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	161	171	561	243	165	857	758	-	-	1237	-	-
Mov Cap-2 Maneuver	161	171	-	243	165	-	-	-	-	-	-	-
Stage 1	303	366	-	612	606	-	-	-	-	-	-	-
Stage 2	730	605	-	519	357	-	-	-	-	-	-	-

Approach	EB		WB		NB		SB	
HCM Control Delay, s	38		15.9		0.6		0	
HCM LOS	E		C					

Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR
Capacity (veh/h)	758	-	-	212	366	1237	-	-
HCM Lane V/C Ratio	0.026	-	-	0.502	0.095	0.002	-	-
HCM Control Delay (s)	9.9	-	-	38	15.9	7.9	0	-
HCM Lane LOS	A	-	-	E	C	A	A	-
HCM 95th %tile Q(veh)	0.1	-	-	2.5	0.3	0	-	-

Intersection						
Int Delay, s/veh	0.1					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T		
Traffic Vol, veh/h	4	2	0	462	380	5
Future Vol, veh/h	4	2	0	462	380	5
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	4	2	0	502	413	5

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	667	209	418	0	-	0
Stage 1	416	-	-	-	-	-
Stage 2	251	-	-	-	-	-
Critical Hdwy	6.84	6.94	4.14	-	-	-
Critical Hdwy Stg 1	5.84	-	-	-	-	-
Critical Hdwy Stg 2	5.84	-	-	-	-	-
Follow-up Hdwy	3.52	3.32	2.22	-	-	-
Pot Cap-1 Maneuver	392	797	1138	-	-	-
Stage 1	634	-	-	-	-	-
Stage 2	768	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	392	797	1138	-	-	-
Mov Cap-2 Maneuver	392	-	-	-	-	-
Stage 1	634	-	-	-	-	-
Stage 2	768	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	12.7	0	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1138	-	472	-	-
HCM Lane V/C Ratio	-	-	0.014	-	-
HCM Control Delay (s)	0	-	12.7	-	-
HCM Lane LOS	A	-	B	-	-
HCM 95th %tile Q(veh)	0	-	0	-	-

Intersection						
Int Delay, s/veh	0.1					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T		
Traffic Vol, veh/h	1	4	0	293	771	7
Future Vol, veh/h	1	4	0	293	771	7
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	1	4	0	318	838	8

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	1001	423	846	0	-	0
Stage 1	842	-	-	-	-	-
Stage 2	159	-	-	-	-	-
Critical Hdwy	6.84	6.94	4.14	-	-	-
Critical Hdwy Stg 1	5.84	-	-	-	-	-
Critical Hdwy Stg 2	5.84	-	-	-	-	-
Follow-up Hdwy	3.52	3.32	2.22	-	-	-
Pot Cap-1 Maneuver	239	579	787	-	-	-
Stage 1	383	-	-	-	-	-
Stage 2	853	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	239	579	787	-	-	-
Mov Cap-2 Maneuver	239	-	-	-	-	-
Stage 1	383	-	-	-	-	-
Stage 2	853	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	13.1	0	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	787	-	451	-	-
HCM Lane V/C Ratio	-	-	0.012	-	-
HCM Control Delay (s)	0	-	13.1	-	-
HCM Lane LOS	A	-	B	-	-
HCM 95th %tile Q(veh)	0	-	0	-	-

Intersection						
Int Delay, s/veh	0.8					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T		
Traffic Vol, veh/h	9	13	43	490	387	5
Future Vol, veh/h	9	13	43	490	387	5
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	10	14	47	533	421	5

Major/Minor	Minor2	Major1		Major2	
Conflicting Flow All	785	213	426	0	-
Stage 1	424	-	-	-	-
Stage 2	361	-	-	-	-
Critical Hdwy	6.84	6.94	4.14	-	-
Critical Hdwy Stg 1	5.84	-	-	-	-
Critical Hdwy Stg 2	5.84	-	-	-	-
Follow-up Hdwy	3.52	3.32	2.22	-	-
Pot Cap-1 Maneuver	330	792	1130	-	-
Stage 1	628	-	-	-	-
Stage 2	676	-	-	-	-
Platoon blocked, %				-	-
Mov Cap-1 Maneuver	311	792	1130	-	-
Mov Cap-2 Maneuver	311	-	-	-	-
Stage 1	591	-	-	-	-
Stage 2	676	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	12.8	0.9	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1130	-	485	-	-
HCM Lane V/C Ratio	0.041	-	0.049	-	-
HCM Control Delay (s)	8.3	0.2	12.8	-	-
HCM Lane LOS	A	A	B	-	-
HCM 95th %tile Q(veh)	0.1	-	0.2	-	-

Intersection						
Int Delay, s/veh	1.5					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T		T		T	
Traffic Vol, veh/h	23	58	34	311	803	7
Future Vol, veh/h	23	58	34	311	803	7
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	25	63	37	338	873	8

Major/Minor	Minor2	Major1		Major2	
Conflicting Flow All	1120	441	881	0	0
Stage 1	877	-	-	-	-
Stage 2	243	-	-	-	-
Critical Hdwy	6.84	6.94	4.14	-	-
Critical Hdwy Stg 1	5.84	-	-	-	-
Critical Hdwy Stg 2	5.84	-	-	-	-
Follow-up Hdwy	3.52	3.32	2.22	-	-
Pot Cap-1 Maneuver	200	564	763	-	-
Stage 1	367	-	-	-	-
Stage 2	775	-	-	-	-
Platoon blocked, %				-	-
Mov Cap-1 Maneuver	188	564	763	-	-
Mov Cap-2 Maneuver	188	-	-	-	-
Stage 1	345	-	-	-	-
Stage 2	775	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	18.2	1.2	0
HCM LOS	C		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	763	-	360	-	-
HCM Lane V/C Ratio	0.048	-	0.245	-	-
HCM Control Delay (s)	10	0.2	18.2	-	-
HCM Lane LOS	A	A	C	-	-
HCM 95th %tile Q(veh)	0.2	-	0.9	-	-

Intersection

Int Delay, s/veh 1.9

Movement EBL EBT WBT WBR SBL SBR

Lane Configurations		↶	↷		↶	
Traffic Vol, veh/h	5	11	22	26	11	1
Future Vol, veh/h	5	11	22	26	11	1
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	5	12	24	28	12	1

Major/Minor Major1 Major2 Minor2

Conflicting Flow All	52	0	-	0	60	38
Stage 1	-	-	-	-	38	-
Stage 2	-	-	-	-	22	-
Critical Hdwy	4.12	-	-	-	6.42	6.22
Critical Hdwy Stg 1	-	-	-	-	5.42	-
Critical Hdwy Stg 2	-	-	-	-	5.42	-
Follow-up Hdwy	2.218	-	-	-	3.518	3.318
Pot Cap-1 Maneuver	1554	-	-	-	947	1034
Stage 1	-	-	-	-	984	-
Stage 2	-	-	-	-	1001	-
Platoon blocked, %		-	-	-		
Mov Cap-1 Maneuver	1554	-	-	-	944	1034
Mov Cap-2 Maneuver	-	-	-	-	944	-
Stage 1	-	-	-	-	981	-
Stage 2	-	-	-	-	1001	-

Approach EB WB SB

HCM Control Delay, s	2.3	0	8.8
HCM LOS			A

Minor Lane/Major Mvmt EBL EBT WBT WBR SBLn1

Capacity (veh/h)	1554	-	-	-	951
HCM Lane V/C Ratio	0.003	-	-	-	0.014
HCM Control Delay (s)	7.3	0	-	-	8.8
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0	-	-	-	0

Intersection

Int Delay, s/veh 5.1

Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↶	↷		↶	
Traffic Vol, veh/h	4	27	23	0	54	7
Future Vol, veh/h	4	27	23	0	54	7
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	4	29	25	0	59	8

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	25	0	-	0	62 25
Stage 1	-	-	-	-	25 -
Stage 2	-	-	-	-	37 -
Critical Hdwy	4.12	-	-	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	2.218	-	-	-	3.518 3.318
Pot Cap-1 Maneuver	1589	-	-	-	944 1051
Stage 1	-	-	-	-	998 -
Stage 2	-	-	-	-	985 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1589	-	-	-	941 1051
Mov Cap-2 Maneuver	-	-	-	-	941 -
Stage 1	-	-	-	-	995 -
Stage 2	-	-	-	-	985 -

Approach	EB	WB	SB
HCM Control Delay, s	0.9	0	9.1
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1589	-	-	-	952
HCM Lane V/C Ratio	0.003	-	-	-	0.07
HCM Control Delay (s)	7.3	0	-	-	9.1
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0	-	-	-	0.2

Intersection						
Int Delay, s/veh	0					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↶	↷		↶	
Traffic Vol, veh/h	0	6	5	0	0	0
Future Vol, veh/h	0	6	5	0	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	7	5	0	0	0
Major/Minor	Major1	Major2	Minor2			
Conflicting Flow All	5	0	-	0	12	5
Stage 1	-	-	-	-	5	-
Stage 2	-	-	-	-	7	-
Critical Hdwy	4.12	-	-	-	6.42	6.22
Critical Hdwy Stg 1	-	-	-	-	5.42	-
Critical Hdwy Stg 2	-	-	-	-	5.42	-
Follow-up Hdwy	2.218	-	-	-	3.518	3.318
Pot Cap-1 Maneuver	1616	-	-	-	1008	1078
Stage 1	-	-	-	-	1018	-
Stage 2	-	-	-	-	1016	-
Platoon blocked, %		-	-	-		
Mov Cap-1 Maneuver	1616	-	-	-	1008	1078
Mov Cap-2 Maneuver	-	-	-	-	1008	-
Stage 1	-	-	-	-	1018	-
Stage 2	-	-	-	-	1016	-
Approach	EB	WB		SB		
HCM Control Delay, s	0	0		0		
HCM LOS				A		
Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1	
Capacity (veh/h)	1616	-	-	-	-	
HCM Lane V/C Ratio	-	-	-	-	-	
HCM Control Delay (s)	0	-	-	-	0	
HCM Lane LOS	A	-	-	-	A	
HCM 95th %tile Q(veh)	0	-	-	-	-	

Intersection						
Int Delay, s/veh	0					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↶	↷		↶	
Traffic Vol, veh/h	0	5	11	0	0	0
Future Vol, veh/h	0	5	11	0	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	5	12	0	0	0

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	12	0	-	0	17 12
Stage 1	-	-	-	-	12 -
Stage 2	-	-	-	-	5 -
Critical Hdwy	4.12	-	-	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	2.218	-	-	-	3.518 3.318
Pot Cap-1 Maneuver	1607	-	-	-	1001 1069
Stage 1	-	-	-	-	1011 -
Stage 2	-	-	-	-	1018 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1607	-	-	-	1001 1069
Mov Cap-2 Maneuver	-	-	-	-	1001 -
Stage 1	-	-	-	-	1011 -
Stage 2	-	-	-	-	1018 -

Approach	EB	WB	SB
HCM Control Delay, s	0	0	0
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1607	-	-	-	-
HCM Lane V/C Ratio	-	-	-	-	-
HCM Control Delay (s)	0	-	-	-	0
HCM Lane LOS	A	-	-	-	A
HCM 95th %tile Q(veh)	0	-	-	-	-

Intersection

Int Delay, s/veh 1.7

Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↶	↷		↶	
Traffic Vol, veh/h	3	11	6	17	5	1
Future Vol, veh/h	3	11	6	17	5	1
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	3	12	7	18	5	1

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	25	0	-	0	34 16
Stage 1	-	-	-	-	16 -
Stage 2	-	-	-	-	18 -
Critical Hdwy	4.12	-	-	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	2.218	-	-	-	3.518 3.318
Pot Cap-1 Maneuver	1589	-	-	-	979 1063
Stage 1	-	-	-	-	1007 -
Stage 2	-	-	-	-	1005 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1589	-	-	-	977 1063
Mov Cap-2 Maneuver	-	-	-	-	977 -
Stage 1	-	-	-	-	1005 -
Stage 2	-	-	-	-	1005 -

Approach	EB	WB	SB
HCM Control Delay, s	1.6	0	8.7
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1589	-	-	-	990
HCM Lane V/C Ratio	0.002	-	-	-	0.007
HCM Control Delay (s)	7.3	0	-	-	8.7
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0	-	-	-	0

Intersection						
Int Delay, s/veh	3.6					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↕	↕		↕	
Traffic Vol, veh/h	2	9	18	12	22	4
Future Vol, veh/h	2	9	18	12	22	4
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	2	10	20	13	24	4

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	33	0	-	0	41 27
Stage 1	-	-	-	-	27 -
Stage 2	-	-	-	-	14 -
Critical Hdwy	4.12	-	-	-	6.42 6.22
Critical Hdwy Stg 1	-	-	-	-	5.42 -
Critical Hdwy Stg 2	-	-	-	-	5.42 -
Follow-up Hdwy	2.218	-	-	-	3.518 3.318
Pot Cap-1 Maneuver	1579	-	-	-	970 1048
Stage 1	-	-	-	-	996 -
Stage 2	-	-	-	-	1009 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1579	-	-	-	969 1048
Mov Cap-2 Maneuver	-	-	-	-	969 -
Stage 1	-	-	-	-	995 -
Stage 2	-	-	-	-	1009 -

Approach	EB	WB	SB
HCM Control Delay, s	1.3	0	8.8
HCM LOS			A

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1579	-	-	-	980
HCM Lane V/C Ratio	0.001	-	-	-	0.029
HCM Control Delay (s)	7.3	0	-	-	8.8
HCM Lane LOS	A	A	-	-	A
HCM 95th %tile Q(veh)	0	-	-	-	0.1

July 20, 2023

Leslie Herring
City Administrator
City of Westwood, Kansas

RE: Storm Memo for Westwood Village Redevelopment

Dear Ms. Herring:

The existing sites at Joe Dennis Park and the adjacent former Westwood View Elementary School at the southwest corner of W. 50th Street and Rainbow Blvd. are intended to be redeveloped as a public park (on the former elementary school site) and office/retail space (on the existing park/old church site). A concept plan of the proposed development is included below.

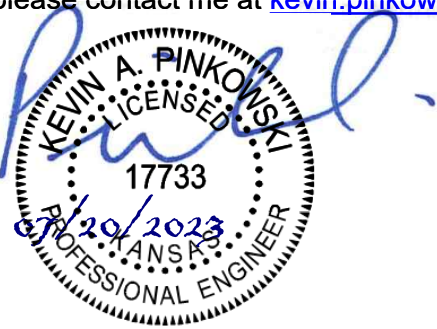
The total combined area of the park and school sites is 7.62 acres. The proposed park will be 3.5 acres and the proposed office/retail development site will be 4.12. The overall impervious area percentage will go from 36.5% (excluding the old church impervious area) in the existing condition to 42.3% in the proposed condition. Additional stormwater runoff will be generated due to this increase in impervious area. The majority of runoff generated by the proposed site will be captured in a proposed underground detention system planned for the southeast corner of the property. The discharge rate from the proposed underground detention will be designed in a manner that, when combined with the stormwater runoff rates from site areas that don't drain to the detention, will meet or reduce the total runoff rate from the existing site.

Any water quality requirements for the stormwater runoff from the site will primarily be met by an isolator row designed as a part of the underground detention system. Additional, smaller water quality measures will be provided around the site as required to meet the necessary level of service.

A final stormwater management report will be provided with the final development plan package. If you have any questions, please contact me at kevin.pinkowski@ibhc.com or 913-663-1900.

Sincerely,

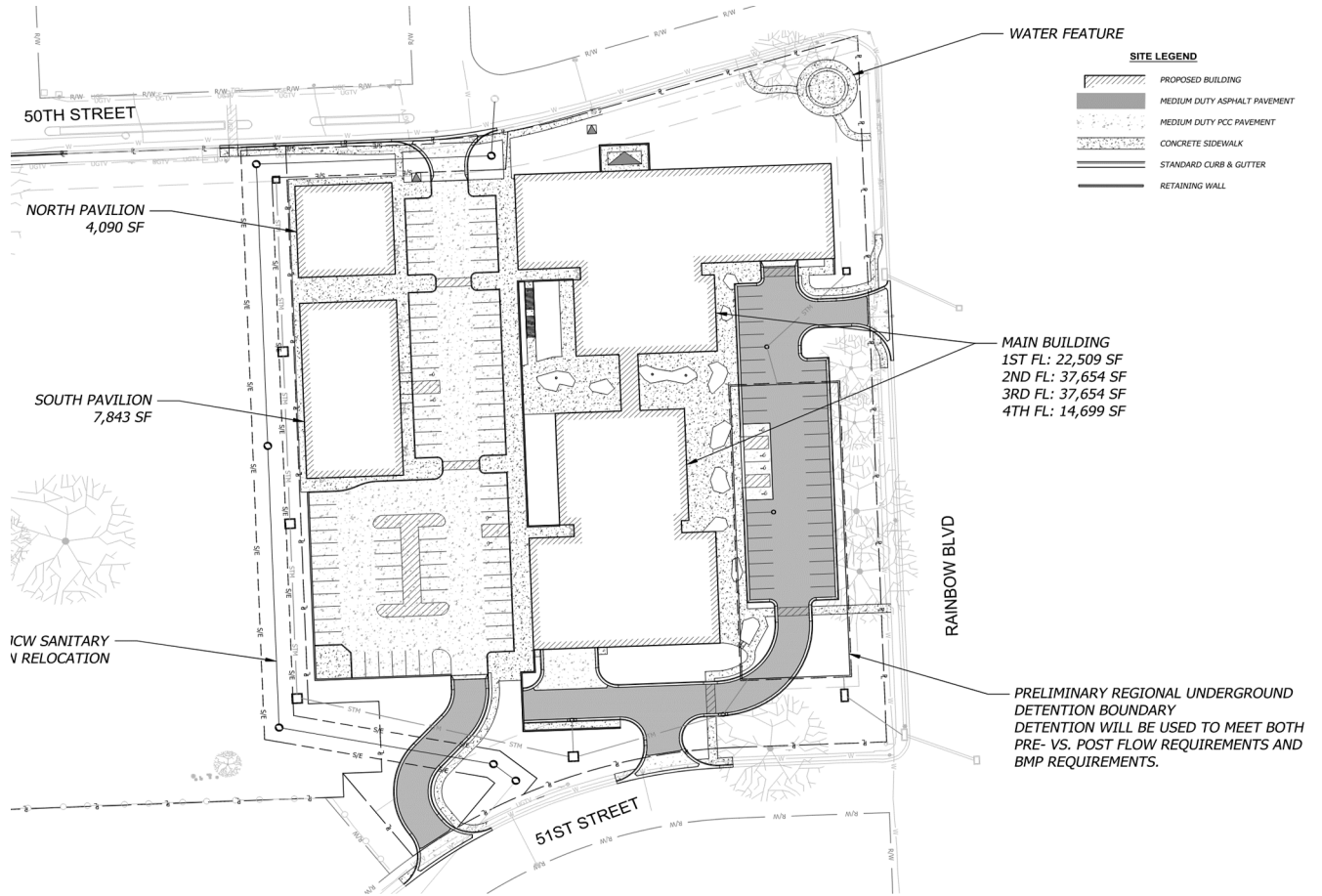
Kevin Pinkowski, P.E.
Senior Project Manager



Existing Site



Proposed Site





913.663.1900



ibhc.com



7101 College Blvd., Ste. 400
Overland Park, KS 66210

KARBANK
REAL ESTATE COMPANY



Perspective
ARCHITECTURE + DESIGN

50th AND RAINBOW DEVELOPMENT
Comprehensive Signage Guidelines | July 2023

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INTRODUCTION

A. ORGANIZATION OF DOCUMENT

This document begins with this Introduction which describes this document's intent and identifies the Regulatory Framework—the language drawn from previous documents and criteria that form the basis of this Comprehensive Sign Plan. The Introduction also identifies what is not regulated by this document.

The second section is the Building Signage Design Guidelines which applies to the building identification and to all businesses operating within the boundaries of the project. This section describes the Design Principles, the different Sign Types—their maximum size and quantity, and suggested Materials & Illumination.

The third section is the Approvals Process as outlined for each sign plan applicant.

B. DOCUMENT INTENT

The purpose of this document is to create a policy for a comprehensive and balanced system of signs for the 50th and Rainbow Development in the City of Westwood, Kansas. The standards are intended to set out a coordinated program for retail/tenant signage.

These criteria were developed to aid the tenant in the development of a retail design that emphasizes the merchandise, enhances the product or service, and reinforces the design quality of the building as a whole. It is hoped that tenants will generate imaginative designs for their space with integral, creative graphics and quality merchandising. Tenants are encouraged to express their own unique design statement within the parameters of the criteria outlined in this document.

All tenants must adhere to these criteria and all applicable state and local sign and building codes. Tenants are encouraged to understand the criteria prior to beginning design for their space. At that time, questions should be raised with Landlord and the City of Westwood to avoid delays later in the design process.

This Comprehensive Sign Plan (CSP) is intended to support the creation of a unified, integrated and enhanced character for the 50th and Rainbow Development, zoned as a Planned Development District (PD), through signage rules and regulations that respond to the unique attributes of the area. This CSP is also intended to ensure that all signage will contribute to the vitality and interest of the project, creating a lively and provocative atmosphere.

The requirements of this Comprehensive Sign Plan shall be supplemented by the City of Westwood regulations and codes in all areas which are not addressed in this document. All City of Westwood ordinances and regulations remain in full effect except as varied by this Comprehensive Sign Plan.

C. REGULATORY FRAMEWORK

This project is located in a planned development district as established by the City of Westwood Zoning Code.

ZONING

In accordance with the City of Westwood, Article Nine shall govern and control the erection, remodeling, enlarging, moving, operation and maintenance of all signs by permitted uses within all zoning districts. Nothing in the CSP shall be deemed a waiver of the provisions of any other ordinance or regulation applicable to signs. Signs located in areas governed by several ordinances and/or applicable regulations shall comply with all such ordinances and regulations.

DESIGN STANDARDS

1. All building signs shall conform to the City of Westwood Zoning Code.
2. Buildings with ground floor uses shall provide a uniform zone for signage over the ground floor.
3. All signs shall be measured in conformance with the City of Westwood Zoning Code.
4. The signage zone shall be provided with electrical power to enable the installation of illuminated signs.
5. All building signs shall be constructed of durable materials suited to the urban environment and climate of Kansas.
6. All conduits, junction boxes, and other functional elements shall be completely hidden from view and safely concealed once the sign is installed.
7. No flashing signs shall be permitted.

BUILDING SIGNAGE INTENT

1. To integrate private business signage in a manner that facilitates commerce, enlivens the public realm, and respects the character of the 50th and Rainbow Development and surrounding area.
2. To ensure that the signs of individual buildings and businesses can express a unique identity, while not detracting from the more important wayfinding and identification signage.
3. To encourage creative sign design.

D. CRITERIA FOR REVIEW

- a. The sign plan allows flexibility in the size, type and location of signs identifying the use(s) and location of a large facility, structure, or building group.
- b. The sign plan shall exhibit design excellence, inventiveness and sensitivity to the context.
- c. Signs shall not be oriented or illuminated so that they adversely affect the surrounding area, particularly existing nearby residential uses or structures. Examples of adverse effects are glare from intense illumination, and large signs or structures which visually dominate and area.
- d. Roof signs shall not be allowed. Portable roof signs, flashing signs, and animated signs are prohibited.
- e. Signs shall be professionally designed and fabricated from quality, durable materials.

BUILDING SIGNAGE - DESIGN PRINCIPLES

This section is intended to be used by all Owners, Developers, Tenants and their Designers who will be operating within the 50th and Rainbow Development.

All exterior signage should address both the communicative functions of a sign and its aesthetic integration with the overall retail concept. The building's architecture sets the tone for the signage program and the Landlord has established standards for identity signage as outlined in these criteria. Engaging an environmental graphic designer to work with the retail designer will assure a coordinated design program. Their knowledge of typography, materials, and fabrication contribute to design success. Experienced designers are aware of the interplay between aesthetics and function, and possess the skills necessary to achieve a synthesis of these conflicting factors.

National and regional "standard" storefront concepts and signage are respected; however, some concept modification may be necessary for compliance. Tenant signs and related logo graphics located along the 50th and Rainbow Development perimeter should express a refined urban sophistication through the use of clean and contemporary shapes and forms. The use of similar architectural materials used throughout the building are encouraged so as to create a seamless transition between the building and the tenant space.

All tenant signage should be appropriate to and expressive of the tenant business activity for which they communicate. Tenant sign designs shall be compatible with and be an enhancement of the architectural character of the 50th and Rainbow Development building(s), expressing scale, color, materials and lighting levels. The Landlord reserves the right to disapprove any sign design which is not compatible with these criteria and the aesthetics of this project. Exceptions to these specifications are rare but will be considered if, in the Landlord's opinion, the sign design is of exceptional merit and architectural quality. Such exceptions must be approved in advance by the Landlord.

As with all undertakings, the ultimate success of the 50th and Rainbow Development depends on the positive contributions of all participants.

Tenants should take advantage of the opportunity to use unique two-and three-dimensional forms/shapes, profiles and iconography that reflect both the personality of the tenant, product/service and the surrounding building architecture. The tenant is encouraged to use color, typography, pattern, texture and materials to create a dynamic interface with the streetscape. Designs which simply maximize size and volume in rectangular form are not acceptable. The Signage Design Criteria should act as a guide for the design of the tenant's signage in conjunction with the provisions of the tenant's lease with the Landlord. Furthermore, these criteria are subject to revision by the Landlord, and the Landlord's interpretation of these criteria are final and governing.

All signage designed for exterior identification of a retail store shall be designed for total compatibility with building finishes, color scheme and lighting levels, in order to maintain a design standard throughout the building. All primary signage will be limited to trade names and shall not include specification of merchandise sold or services rendered, regardless of the tenant's legal name. Corporate crests, logos or insignias may be acceptable pending the Landlord's approval and provided they are part of the tenant's name.

All signage requires review by the City of Westwood Zoning Administration and issuance of a Sign Permit.

BUILDING SIGNAGE: SIGNAGE DEFINITIONS

PERMITTED SIGN TYPES

Exterior signs for tenants and businesses operating covered by this section of the Comprehensive Sign Plan are:

Identification Signs: Project or Tenant

- Wall and Canopy Signs
- Window Signs
- Monument Signs
- Blade Signs

Parking/Traffic Directional Wayfinding

- Projecting Signs
- Wall Signs
- Ground Wayfinding Signs

Wayfinding: Pedestrian Directories

- Ground Signs
- Wall Signs

Refer to pages 10–16 of this document for further definitions of each sign type.

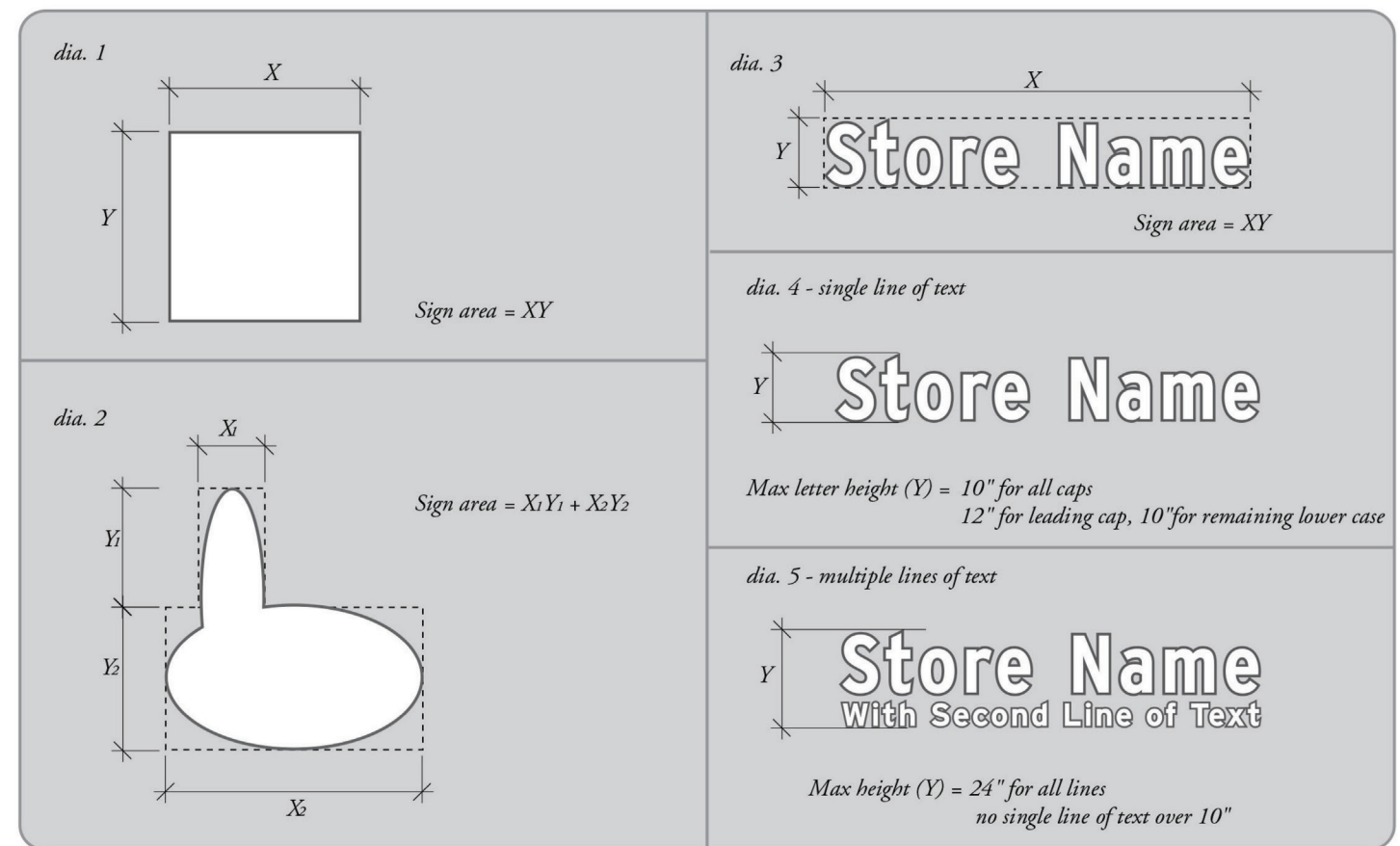
CALCULATING SIGNAGE AREA

The area of a sign is determined by the sum of all areas or portion of each triangle, parallelogram, circle, ellipses or any combination thereof which creates the smallest single continuous perimeter enclosing the extreme limits of decorative sign elements; this includes all words, letters, logos, frames, backing, face plates, non-structural trim or other components not used for support.

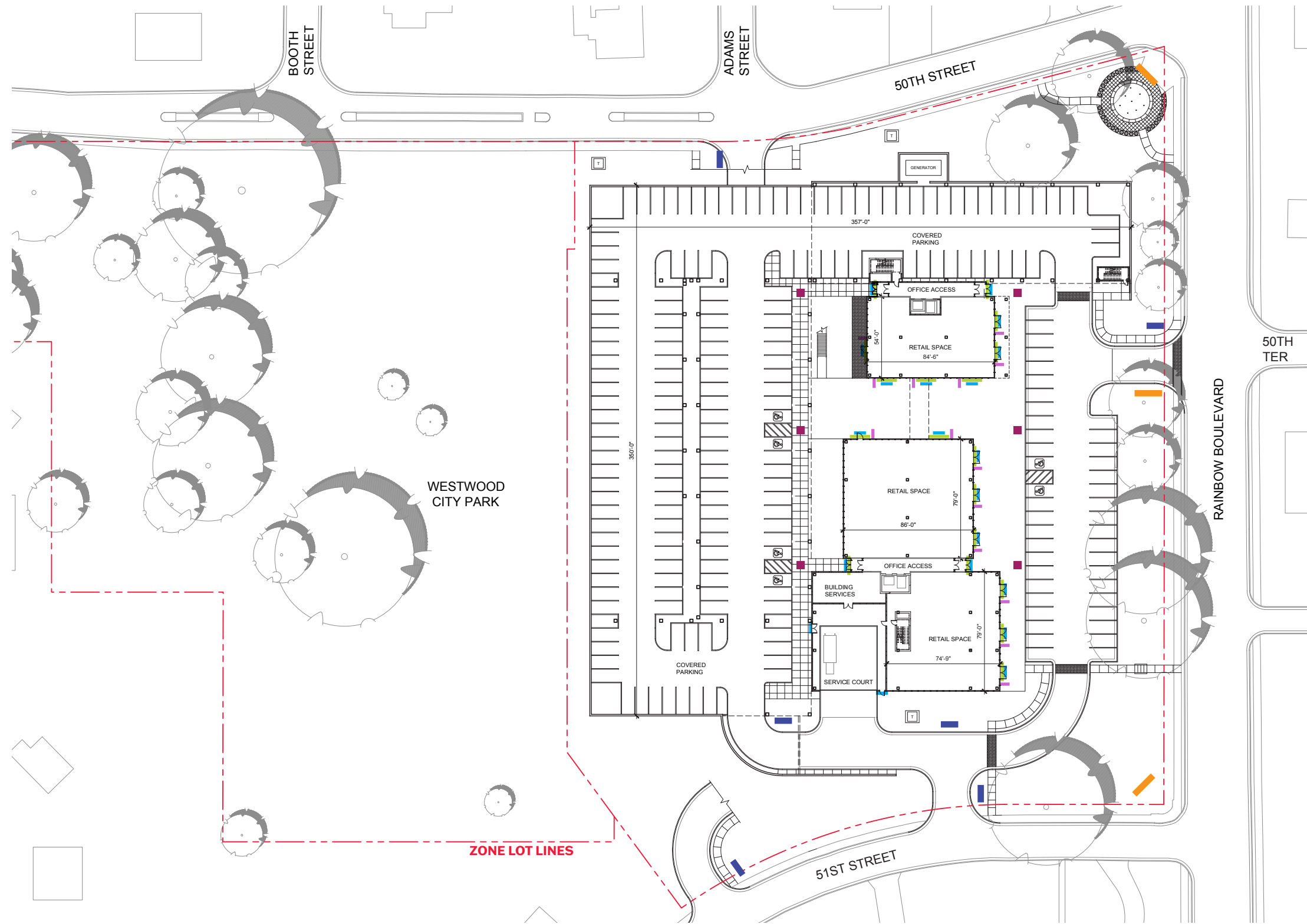
Sign armature or bracing shall not be included in the sign area measurement unless it is made part of the message or face of the sign. Where a sign has two (2) or more display faces, the area of all faces shall be included in the calculation unless the display faces are back to back and parallel to each other and not more than twenty four inches (24") apart, or form a "V" type angle of less than ninety degrees (90°).

For regular shaped signs the area of the sign will be computed by using standard mathematical formulas for regular geometric shapes, including, without limitation, triangles, parallelograms, circles, ellipses, or combinations thereof.

In the case of an irregularly shaped sign or a sign with letters or symbols directly affixed or painted on the wall of a building, the area of the sign is the entire area within a single continuous rectilinear perimeter of not more than eight straight lines enclosing the extreme limits of any writing, representation, emblem, or any figure of similar character, together with any material or color forming an integral part or background of the display if used to differentiate such sign from the backdrop of structure against which it is placed, but if a freestanding sign structure is not a fence which functions as such, the sign area shall be the area of the entire structure.

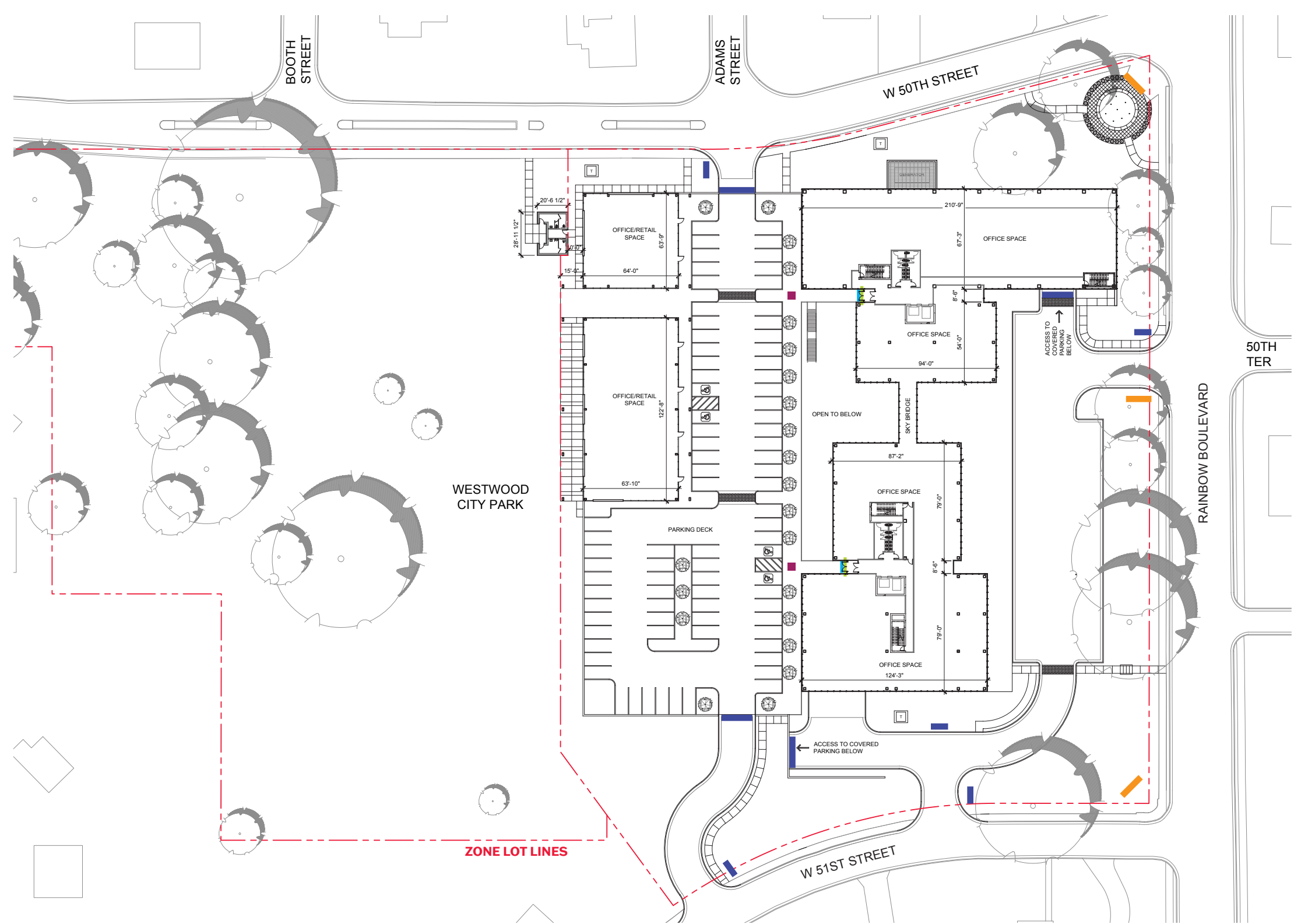


SITE LOCATION PLAN, LEVEL 1



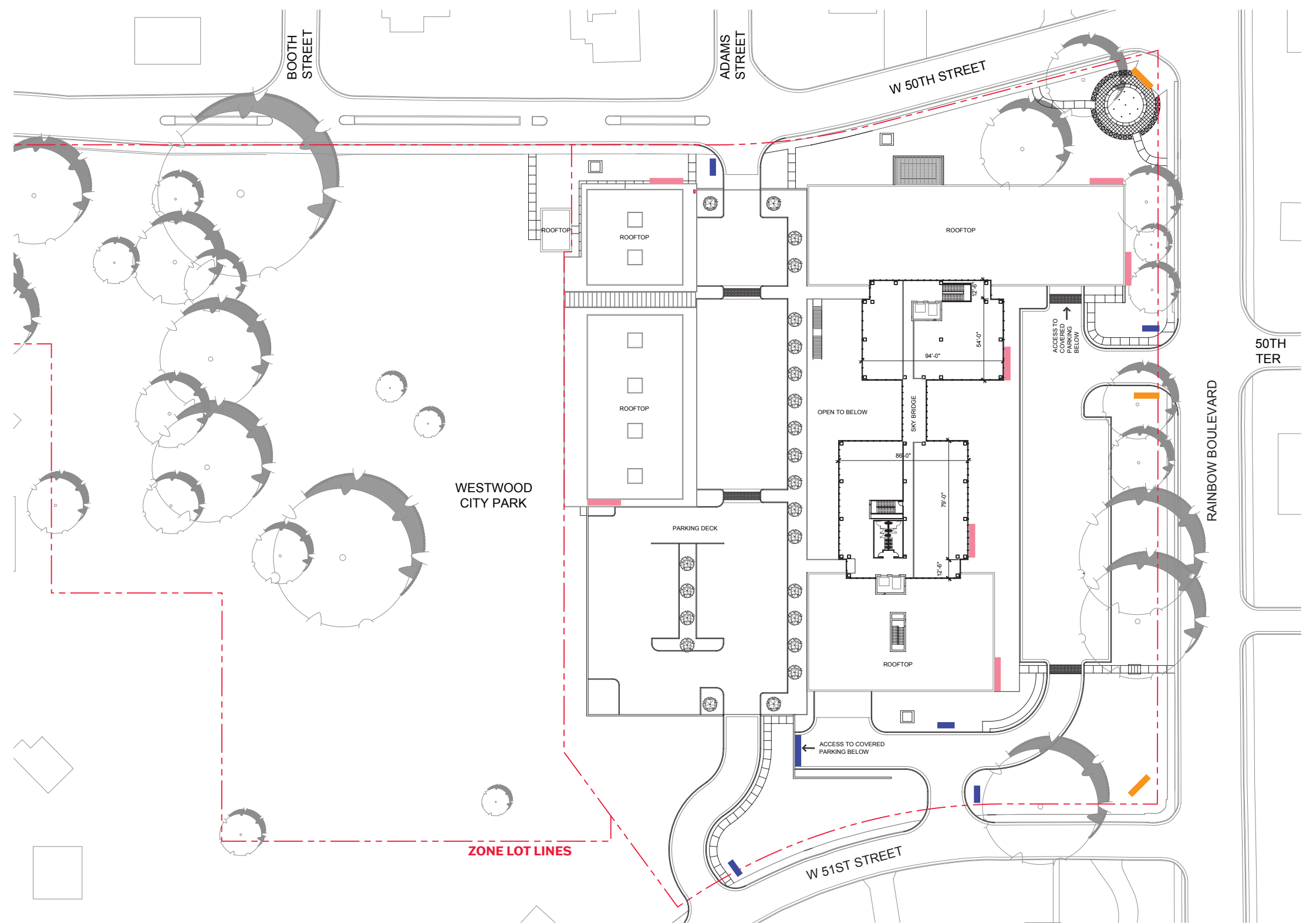
SIGN TYPE LEGEND	
A1	Wall Sign - Tenant or Project
A2	Wall Sign - Upper Story
B	Window Sign - Tenant or Project
C	Monument Sign - Tenant or Project
D	Blade Signs - Tenant or Project
E	Parking/Traffic Directional Sign
F	Wayfinding Sign - Pedestrian

SITE LOCATION PLAN, LEVEL 2



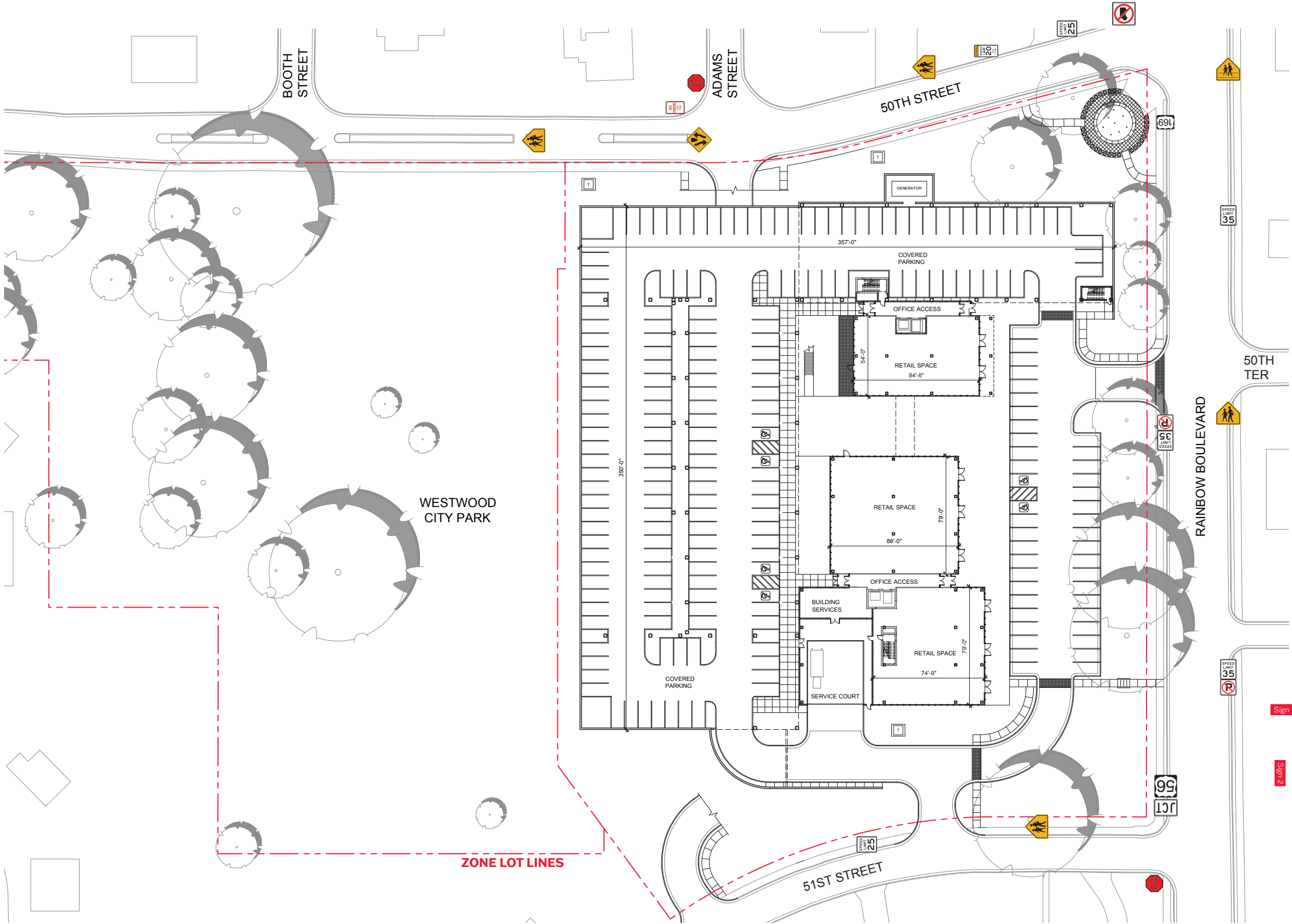
SIGN TYPE LEGEND	
A1	Wall Sign - Tenant or Project
A2	Wall Sign - Upper Story
B	Window Sign - Tenant or Project
C	Monument Sign - Tenant or Project
D	Blade Signs - Tenant or Project
E	Parking/Traffic Directional Sign
F	Wayfinding Sign - Pedestrian

SITE LOCATION PLAN, LEVEL 4



SIGN TYPE LEGEND	
A1	Wall Sign - Tenant or Project
A2	Wall Sign - Upper Story
B	Window Sign - Tenant or Project
C	Monument Sign - Tenant or Project
D	Blade Signs - Tenant or Project
E	Parking/Traffic Directional Sign
F	Wayfinding Sign - Pedestrian

SITE LOCATION PLAN, EXISTING NEIGHBORHOOD SIGNAGE



Sign 1: St. Rose Philippine Duchesne Church



Sign 2: St. Rose Philippine Duchesne Church

A1: WALL & CANOPY SIGNS, TENANT IDENTIFICATION

INTENT:

Wall and Canopy Signs are integrated with the building walls where available or above main entries of tenant lease spaces and/or entrances into the project. These signs are mounted to the wall of the building; all connecting hardware should not be visible unless it is an integral part of the sign design.

LOCATION:

Wall and Canopy Signs are integrated with the building walls where available or on canopies above main entries of tenant lease spaces. All connecting hardware should not be visible unless it is an integral part of the sign design. In lieu of the wall, signs may be applied to entry canopies or awnings where applicable, and with approval of the Landlord. When using an existing sign band, provide space between the sign and the sign band border or edge to follow a traditional application. When using an existing sign band, keep signage flush to the wall surface. Do not design wall signs that project in front of adjacent architectural details, such as a wall band frame.

ALLOWANCE:

The maximum allowable is one (1) sign per each tenant on exterior wall/wall frontage. No more than 10% of total Tenant's elevation area may be used, and no more than 100 square feet total area per tenant.

ILLUMINATION:

Wall Signs may be illuminated. If illuminated, signs are to have static, unobtrusive illumination. Face-lit or back-lit letters (halo), neon or a shielded lamp or goose-neck located at top of the sign is allowed.

All direct illumination shall not exceed 25 watts per bulb. Flashing signs are prohibited.

ENCOURAGED APPROACHES:

The following are approaches commonly encouraged:

- Tenant logo/logo type to be fabricated or flat cut-out and layered to achieve a 3-dimensional form to the signage components.
- Tenant logo/logo type should not fill the entire designated sign zone.
- Paint and metal finishes that connect to the building wall should reflect the overall architectural color palette.
- Sign/sign armature attachment to building wall with custom designed metal armature/brackets.
- External and or internal illumination integrated into sign canopy recommended.
- All designs subject to review for scale and proportion relative to the building architectural context.
- Three-dimensional logo or iconography are encouraged.
- Cut-out letters with fabricated returns and shapes encouraged.

ENCOURAGED EXAMPLES:



Flat cut out pin-mounted halo-lit letters



Deep single-mounted letters with high contrast



Fabricated letters integrated into architecture



Face-lit flush-mounted logo



Cabinet sign box



Canopy Signage

A2: WALL SIGNS, UPPER STORY

INTENT:

The intent for Upper Story Wall Signs is to bring greater hierarchy and large-scale vehicular wayfinding opportunity for said-tenant.

LOCATION:

Upper Story Wall Signs to be located near top of building, not to extend above the roof line on front facade of building.

ALLOWANCE:

Upper Story Wall Signs are not to exceed 100 sq-ft total per allowed location.

ILLUMINATION:

Upper Story Wall Signs may be illuminated. If illuminated, signs are to have static, unobtrusive illumination. Face-lit or back-lit letters (halo), neon or a shielded lamp or goose-neck located at top of the sign is allowed.

All direct illumination shall not exceed 25 watts per bulb. Flashing signs are prohibited.

ENCOURAGED APPROACHES:

The following are approaches commonly encouraged:

- Tenant logo/logo type to be fabricated or flat cut-out and layered to achieve a 3-dimensional form to the signage components.
- Tenant logo/logo type should not fill the entire designated sign zone.
- Paint and metal finishes that connect to the building facade should reflect the overall architectural color palette.
- Sign/sign armature attachment to building facade with custom designed metal armature/brackets.
- External and or internal illumination integrated into sign canopy recommended.
- All designs subject to review for scale and proportion relative to the building architectural context.
- Three-dimensional logo or iconography are encouraged.
- Cut-out letters with fabricated returns and shapes encouraged.
- No back-lit sign boxes will be allowed.

ENCOURAGED EXAMPLES:



Cut-out logo, halo-lit



High contrast color internally-illuminated letters

B: WINDOW SIGNS, TENANT

INTENT:

To identify the entrance and hours of operation, identify the tenants storefront and display windows and create visual interest.

LOCATION:

If any window signs are located on the door, the following design standards must be met: Use door signage to identify business name, address, hours of operation and a possible logo if needed.

Window graphics are typically located at eye level on doors or adjacent to entrances for door signs or in the lower 20% of storefront windows for window signs for each tenant. Patterns/graphics may be installed at transoms, but shall not contain any text. Limit opaque and solid materials to no more than 10 percent of a window's area, and place appropriately to avoid blocking visibility in and out of a window.

ALLOWANCE:

Any element that is attached to or located within 36 inches of a window is considered to be a window sign. A window sign should not exceed 20% of the total window area with 10% allowed to be opaque or solid. All words or pictures located on a window or door shall be considered signs and shall meet all criteria for signage defined herein and shall be permitted as signs by the City of Westwood.

ATTACHMENTS:

Install directly inside (second surface) to tenant glass.

ENCOURAGED APPROACHES:

The following are approaches commonly encouraged:

- Maximum graphic image area not to exceed 20% of total window area.
- Message height is recommended to be at eye level for door signs or in the lower 20% of storefronts for window signs.
- Digitally cut vinyl, silk-screened, gold leaf, hand painted or neon graphics should be applied to the interior surface of the window (second surface if single paned glass, fourth surface if double paned glass).
- Avoid repeating business wording and tenant ID's in every window when this information already exists on other signage.
- Provide secondary information on products, services, etc. that are not available on other signs.
- Plan window signage to draw the pedestrian's eye into a business and to create additional interest.
- Use door signage to identify business name, address, hours of operation and a possible ID if needed.
- Vinyl, silk-screened or gold leaf signage on doors is

ENCOURAGED EXAMPLES:



Logo & Hours



Logo, Tag-line & Hours



Retail ID and less than 20% window coverage brand graphic

C: MONUMENT SIGN, TENANT AND PROJECT

INTENT:

Monument Signs are for the purpose of identifying the Project as well as the tenants occupying space within it and is used for wayfinding.

LOCATION:

The permitted location shall be set in at least five (5) feet from every boundary line of the zone lot. Locate in a high traffic area out of the public right of way.

ALLOWANCE:

A maximum of three (3) monument signs are allowed on the site.

Monument signs for the site have a maximum height of 14 ft above finished grade. No more than 100 square feet total area per location.

ILLUMINATION:

Monument Signs may be illuminated. If illuminated, signs are to have static, unobtrusive illumination. Internal illumination of channel letters, back-lit letters (halo), push through letters, shielded, concealed or external, shielded, downward facing fixtures are allowed.

May be illuminated and all direct illumination shall not exceed 25 watts per bulb. Flashing signs are prohibited.

ENCOURAGED APPROACHES:

The following are approaches commonly encouraged:

- Signs must be consistent with or complement the building, hardscape and existing signage material palettes.
- Use permanent, durable materials such as metals, metal composites, and other high quality materials. Do not use signs with plastic or acrylic.

ENCOURAGED EXAMPLES:



Contemporary materials & cut out logo



Industrial solution with contrast logo and background



Simple solution with small project ID



Complimentary material usage

D: BLADE SIGN, TENANT

INTENT:

Blade Signs are signs that attach and project from the building facade.

LOCATION:

All Blade Signs are to project perpendicular to the building facade and will be a minimum of 18" wide and be no greater than 36" wide in their horizontal dimension. Blade Signs must maintain a minimum clearance of eight feet (8'-0") above the sidewalk.

ALLOWANCE:

Blade Signs are limited to the Landlord or Tenants whose entry is on the exterior of the building, or those who have limited wall surface at their storefront. Eligible tenants are allowed one (1) Blade Sign.

ILLUMINATION:

Any sign configuration by multi-storefront tenants shall not exceed the allowance for total square feet area. Static, unobtrusive illumination allowed. Internal illumination of channel letters, Back-lit letters (halo), push through acrylic letters, or a shielded spot light located at top of the sign is allowed. May be illuminated and all direct illumination shall not exceed 25 watts per bulb. Flashing signs are prohibited.

ATTACHMENT DETAILS:

Blade Signs are attached to the building facades at main entries of tenant lease spaces. These signs are mounted directly to the exterior wall of the building; all connecting hardware should not be visible unless it is an integral part of the sign design. No more than 10% of total wall area may be used per tenant.

ENCOURAGED APPROACHES:

The following are approaches commonly encouraged:

1. Artistic, three-dimensional object signs of logo or primary sales product(s) fabricated/sculpted from suitable materials.
2. Router-cut or dimensional letters/ logos attached to or pushed through sign panels or cabinet construction boxes.
3. Painted, screen printed or gilded sign panels or cabinet construction boxes.
 - A combination of materials. Cut-out, layered, built up or pinned-off metal or wood borders or graphics.
 - Raceways, conduits and transformers must be concealed within the sign assembly.
 - Flush, discreet attachment of the acrylic faces to the metal channel letters without typical trim cap edging.
 - The acrylic face of the letter forms must have a matte finish to avoid reflections in the letter face when not illuminated.

Blade Signs for businesses and retail tenants are encouraged to have internal, integral, edge, halo or external illumination, or any combination thereof.

ENCOURAGED EXAMPLES:



Box sign



Custom shape



Cabinet sign box with push-through letters



Pushed through sign panel

E: PARKING/TRAFFIC DIRECTIONAL SIGNAGE

INTENT:

The intent of Parking/Traffic Directional Signage is to provide identification, information and direction to guests, residents and workers visiting and interacting with the project. These signs provide a general understanding of the project which allows users to move about with confidence and ease. These signs assist the guests, from well positioned and highly visible identification of the vehicular garage entries, to navigating within the garage, to emerging into the street-level public realm.

LOCATION:

To be mounted above the garage entrance as clear messaging for vehicular arrival. Wall signs are integrated with the building walls where available or above main parking entrances and vehicular flow routes.

ALLOWANCE:

Maximum of two (2) parking identification sign per garage entrance.

ILLUMINATION:

Parking/Traffic Directional signs may be illuminated. If illuminated, signs are to have static, unobtrusive illumination. Halo, shielded, concealed or external, shielded, downward facing fixtures are allowed.

ATTACHMENT DETAILS:

Wall signs are integrated with the building walls where available or above main parking entrances and vehicular flow routes. All connecting hardware should not be visible unless it is an integral part of the sign design.

ENCOURAGED APPROACHES:

The following are approaches commonly encouraged:

- Signs must be consistent with or complement the building, hardscape and existing signage material palettes.
- Use permanent, durable materials such as metals metal composites, and other high quality materials. Do not use signs with plastic or acrylic faces.

ENCOURAGED EXAMPLES:



Signage integrated into building architecture



Parking projecting ID



Cabinet sign box



Cabinet sign box

F: WAYFINDING, PEDESTRIAN DIRECTORIES

INTENT:

Wayfinding Signs are a specific type of sign that are intended for pedestrian wayfinding purposes, presenting multiple Tenants and public use destinations in a single display.

LOCATION:

Locate in a high traffic area out of the public right of way. Wayfinding Signs to be permitted location shall be set in at least five (5) feet from every boundary line of the zone lot. Wayfinding Signs may integrate with the building walls where available or above main entries of tenant(s) entrances into the project. All connecting hardware should not be visible unless it is an integral part of the sign design.

ALLOWANCE:

Wayfinding Signs for the complex may have a maximum height of 8 ft above finished grade. The maximum allowable is one (1) sign housing three or more tenant logos on exterior wall/wall frontage. Thirty-six (36) square feet maximum total area per sign.

ILLUMINATION:

Wayfinding Signs may be illuminated. If illuminated, signs are to have static, unobtrusive illumination. Halo, shielded, concealed or external, shielded, downward facing fixtures are allowed.

ENCOURAGED APPROACHES:

The following are approaches commonly encouraged:

- Fabricated, weather protective enclosures.
- Changeable graphics may be silk-screened or digitally printed. Units may be installed to be flush with wall (column) surface or may be secured to column faces. Illumination, if included, must be externally washed by concealed fixtures.
- Framed panel or open pan construction with expose or concealed external illumination sources. Changeable graphics may be silk-screened or digitally printed.
- Layered sandwich construction with protective glass face, changeable content and rear panel surface of appropriate materials and finishes all secured by vandal-resistant mechanical fasteners.
- Changeable cut-out metal letters or panels secured in a frame or track. External or halo illumination.

ENCOURAGED EXAMPLES:



Sleek solution with clear wayfinding messaging and Project ID logo



Contemporary materials high contrast letters



Simple, modern solution



Complimentary material usage with tenant wayfinding.

MATERIAL & PERFORMANCE SPECIFICATIONS

In keeping with the high standards of design being applied to the overall project, all signage must utilize the highest quality materials and fabrication methods. The following minimum quality standards shall apply to all the 50th and Rainbow Development signs:

GENERAL

The environmental graphic designers and their project teams shall be responsible for verifying and ensuring compliance of the signage with all ADA, OSHA, MUTCD, environmental regulations and all other applicable governing code requirements.

Should there be a conflict between these documents and federal, state or local code requirements, code shall take precedence unless a specific agreement has been established with the City of Westwood providing a variance to the local codes.

FABRICATION & INSTALLATION REQUIREMENTS

A. STRUCTURAL REQUIREMENTS

The designer shall follow this document for exterior visual appearance. The internal structure, engineered connections, mounting assemblies and foundations shall be developed by the sign fabricator as required for each sign type. The structural design shall utilize self-supportive framing and prevent irregularities in exposed surfaces.

B. ELECTRICAL REQUIREMENTS

All transformers and electrical hardware shall be concealed (i.e. non-audible and non-visible to vehicular and pedestrian traffic) but easily accessible for maintenance and servicing. All connections must be in compliance with the requirements of the NEC and all other applicable governing code requirements. All necessary electrical components and assemblies are to be UL listed, or approved by a nationally recognized testing lab and shall be warranted by the manufacturer against failure for at least ninety days. All conduit, junction boxes and races shall be concealed within the sign or the building.

C. LIGHTING

All lighting components must be easily accessible for maintenance and servicing. All lighting components shall be constructed per recognized national standards, and/or specific manufacturer's recommendations. It is strongly encouraged that all illumination shall be provided by LED light sources for longevity, ease of maintenance and life-cycle cost purposes. Unless otherwise noted, the interior of all illuminated enclosures shall be painted bright white to increase reflectivity. Should exposed neon components be used, they shall be warranted against failure for at least three years, and all other lighting components shall be warranted for at least ninety days.

D. LABELING

Manufacturer's or testing laboratory labels shall clearly appear on all completed elements, as required by code but shall be located on secondary or less obvious surfaces.

MATERIAL & WORKMANSHIP STANDARDS

When selecting final materials to be used for the signage, the design must bear in mind the unsecured, urban environment in which these signs will be installed and the high potential for their abuse. Furthermore, these signs will likely be in place for an extended period of time, materials should be selected for longevity

A. PAINTS & FINISHES

Given the potential for abuse, painted finishes should be used sparingly or located at a height less susceptible to abuse. All pretreats, primers, coatings, and finishes shall be applied in strict accordance with the paint manufacturer's specifications to provide the highest level of ultraviolet light resistance, weatherability and overall longevity for both the materials indicated and the environmental conditions of the final install locations. Paints and finishes shall be warranted against color fading, UV damage, cracking, peeling, blistering and other defects in materials or workmanship for a minimum of five years.

B. METALS

Metals shall be the best commercial quality for the purposes specified and free from defects impairing strength, durability or appearance. Unless specifically designed otherwise as a feature element, all visible seams are to be continuously welded, filled and ground smooth. All sheet metal shall have brake formed edges with radii not greater than sheet thickness. All metals must be treated to prevent corrosion and staining of other finishes.

C. FASTENERS

Unless specifically designed otherwise as a feature element, all exposed fasteners shall be tamper-proof, resistant to oxidation and other corrosion and of a finish to match adjacent surfaces. Concealed fasteners must be resistant to oxidation and corrosion to prevent staining of other finishes.

D. VINYL

All vinyl products shall be specified and installed in strict accordance with the manufacturer's recommendations to provide the highest level of ultraviolet light resistance, weatherability and overall longevity for both the materials

indicated and the environmental conditions of the final installation locations. All vinyl material shall be warranted against color fading, UV damage, de-lamination and peeling for a period of five years.

E. DIGITAL PRINTS

Technological advances in digital printing make this medium ideal for easily updatable content. As such, this material must be periodically refreshed, whether the content has changed or not. All digital prints must provide the highest level of ultraviolet light resistance, weatherability and overall longevity for both the materials indicated and the environmental conditions of the Kansas region. Unless specifically designed otherwise as a feature element, digital prints shall have a minimum resolution of 200 dpi. Printed products shall be warranted against color fading, UV damage, delamination or peeling for a minimum of five years.

F. GENERAL ASSEMBLY

Unless otherwise stated above, all installed elements shall be warranted against manufacturer defects for a minimum of one year and all installed elements shall be warranted against defects in installation or workmanship for a minimum of three years.

G. NEWLY CREATED MATERIALS

Newly created materials meeting the intent of the CSP may be considered for approval based upon the guidelines set forth in this document.

ACCEPTABLE BUILDING SIGN MATERIAL EXAMPLES

Selected materials should reflect their use and the anticipated longevity of the sign. Materials should be urban in character, durable, easily maintained and of the highest quality. Elements such as acrylic and wood should be used selectively and their location should be considered to minimize the potential for damage. The materials used for all freestanding signage shall be designed and constructed to be durable enough to withstand the equipment to be used for snow removal and other maintenance.



ACCEPTABLE WALL SIGN ILLUMINATION EXAMPLES

Signage illumination should be chosen based upon the purpose of the sign, the required legibility and visibility, the anticipated ambient lighting and the competing signage elements in the area(s) in which the signs will be located. All illuminated signs must be controlled by a central timer or photosensitive switch (photo cell) to regulate the hours of operation.

It is additionally encouraged that the controls for the lighting allow for dimming during the late night/early morning hours.



PROHIBITED BUILDING SIGN TYPES

PROHIBITED SIGN TYPES

In order to maintain a high level of quality and a character appropriate to the 50th and Rainbow Development project, the sign types and fabrication methods described below will not be permitted for any businesses or developments within its limits. All signs are subject to the review and approval of the Landlord and the City of Westwood Zoning Administration.

The following identity sign types are prohibited:

1. Internally illuminated signs with vacuum formed plastic faces.
2. Internally illuminated box signs with exposed acrylic or stretched vinyl sheet faces without additional materiality and layering.
3. Internally illuminated awnings.
4. Parked motor vehicles and/or trailers intentionally located so as to serve as a sign or advertising device.
5. Signs with exposed raceways.
6. Signs with individual changeable plastic letters.
7. Sign boards using explicitly inexpensive materials.
8. Painted or printed window graphics which cover more than twenty (20) percent of a tenant's glazing area.
9. Off the shelf portable signs that do not reflect the quality demanded of this district.
10. Inflatable signs.



Internally illuminated signs with plastic faces



Exposed raceways, heavy trim cap



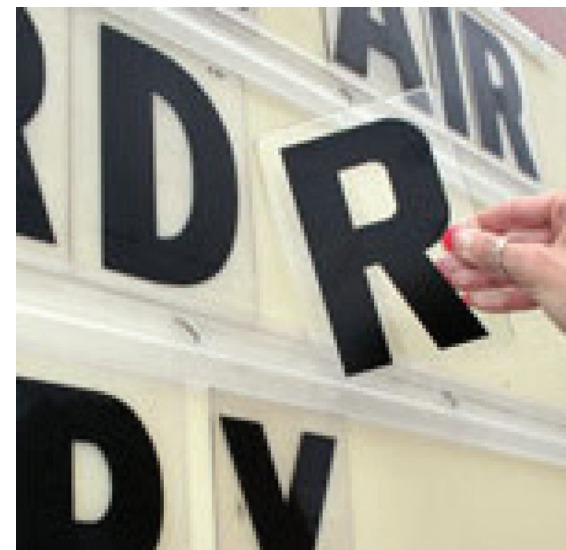
More than 20% of window area with graphics



Low quality off-the-shelf portable signs



Temporary inflatables, of any kind



Signs with individual changeable plastic letters



Internally illuminated signs with vacuum formed plastic faces

50th & Rainbow Development

Project Narrative

The 50th and Rainbow Development is a proposed Planned Development District (PD) located on the west side of Rainbow Boulevard between 50th Street and 51st Street in the City of Westwood, Kansas. The proposed project will feature approximately 22,509 square feet of retail space on the 1st floor of the building and approximately 90,007 square feet of office space on 3 floors above the retail. Adjacent to the main building is a smaller 11,933 square foot single story office / retail building broken into two masses.

Because of the mixed-use nature of the proposed project, it is anticipated that portions of the first and second floors of the buildings may be a mix of retail and office uses in lieu of being strictly office or strictly retail. An underground parking structure is located to the west and north of the main building with 171 covered parking spaces. 125 surface parking spaces are also provided on the east and west sides of the main building.

With regard to the 2017 Westwood Comprehensive Use Plan, the land use map in Section 3.3 identifies the proposed development location as Public / Semi-Public and Open Space. This usage type per Figure 3.2 accounts for only 4.4% and 0.8% of the overall city land use. Given the small percentage of space allocated to these uses, it seems vital to the community to maintain these land use types. The proposed development would offer a mixture of Public and Semi-Public spaces with office and retail buildings, and maintain the Open Space components with the proposed City Park. Alternatively, if the plan reverted to the R-1 Single Family Residence Zoning of the surrounding neighborhood, the location would become private space which would seem in conflict with the Comprehensive Use Plan.

The specific stated desired outcomes of the 5050 Rainbow Site, and the Westwood View Elementary site per the Westwood Comprehensive Use Plan are to:

- Protect and minimize impacts to adjacent residential property.
- Offer indoor and/or outdoor community activity/gathering spaces.
- Attract and keep residents.
- Retain and grow property values.
- Support local / small business development

The proposed project would provide a vital fully integrated, mixed-use, pedestrian oriented commercial neighborhood that would meet all of the desired outcomes as listed in the Comprehensive Use Plan.



Specifically related to Section 1.6.17 of the Zoning Ordinance Criteria for Considering Applications for a Rezoning Request:

- A. The conformance of the proposed use to the City's Comprehensive Plan and other adopted planning policies. **Per Section 6.2.2, a PD Planned Development district is equivalent to C-1 Zoning. The proposed development would be in substantial compliance with current C-1 Zoning. Minor deviations are anticipated and included in the deviations list on the cover sheet.**
- B. The character of the neighborhood including, but not limited to: Land use, zoning, density (residential), architectural style, building materials, height, structural mass, siting, open space, and floor-to-area ration (commercial and industrial). **The proposed buildings have intentionally been pushed back from Rainbow Boulevard and the tallest of the buildings are at the center of the site to best fit in with the scale and character of the surrounding neighborhood. Additionally, the majority of existing trees will be preserved at the perimeter of the site to create a large multistory landscape buffer from surrounding neighbors and preserve the existing quality of the streetscape. Building massing has been broken down in plan and elevation to create smaller scaled facades of varying colors to also complement and blend into the surrounding residential neighborhood.**
- C. The zonings and uses of nearby properties, and the extent to which the proposed use would be in harmony with such zonings and uses. **All nearby properties are Zoned R-1 Single Family Residential. The proposed Development would bring a mix of office and retail uses to the project compatible with the surrounding neighborhood providing residents walkable places to work and shop fulfilling the purpose of a fully integrated, mixed-use, pedestrian oriented neighborhood.**
- D. The suitability of the property for the uses to which it has been restricted under the applicable zoning district regulations. **While the current property could be utilized for single family residential, adding a mixture of office, retail and park functions would serve to provide additional amenities to the residents in the area.**
- E. The length of time the property has remained vacant as zoned. **The former church site property along Rainbow Boulevard was acquired by the City of Westwood and the church was demolished to accommodate the city's plan for future development The school, while still in use, is scheduled to be vacated in August 2024.**
- F. The extent to which approval of the application would detrimentally affect nearby properties. **Approval of the application would have no detrimental effect on nearby properties.**
- G. The extent to which the proposed use would substantially harm the value of nearby properties. **The proposed use will not harm and may enhance the value of nearby properties.**
- H. The extent to which the proposed use would adversely affect the capacity or safety of the portion of the road network influenced by the use, or present parking problems in the vicinity of the property. **The existing site functions as both a school and a park, creating peak traffic times as well as various visitor traffic throughout the day. Traffic flow for the proposed development would be of a similar nature with peak traffic times at the beginning and end of the office work day, and various visitor traffic to the retail shops throughout the day. Parking for the proposed development would be handled internally on the site so there would be minimal overflow to the surrounding area.**
- I. The extent to which the proposed use would create excessive air pollution, water pollution, noise pollution, or other environmental harm. **No excess air, water or noise pollution or other environmental harm would occur.**
- J. The economic impact of the proposed use on the community. **The Economic Impact of the proposed project would result in a net increase to the sales and property taxes received by the City of Westwood. No tax incentives are being requested by the developer as part of this project apart from a sales tax waiver on construction materials.**

The gain, if any, to the public health, safety, and welfare due to denial of the application as compared to the hardship imposed upon the landowner, if any, as a result of denial of the application. **No gain to the public health, safety and welfare would occur due to denial of the application, however if the application is denied, a substantial portion of the site would remain vacant and the future of city park would be in jeopardy.**



**MESSAGE FROM THE MAYOR
KARBANK AND PARK REDEVELOPMENT PROJECT**

August 28, 2023

Friends and Neighbors,

As you know, on March 9, 2023, the City Council received a report from the Karbank Real Estate Company with a proposal that could provide a pathway for the City to acquire the former Westwood View site, develop it as a feature park and green space for our community, and also utilize our frontage on Rainbow for a supporting development. At that meeting, the City Council approved a “funding and exclusivity agreement” that, while not approving any specific plans, established a general timeline for discussing and negotiating real estate contracts, considering plans, and allowing for initial due diligence by both the City and Karbank. That funding and exclusivity agreement also required that Karbank deposit funds with the City to pay for the City’s costs in working with financial consultants and other specialists to evaluate Karbank’s proposal.



Since that time, we have been diligently working with both the Shawnee Mission School District and Karbank to further refine the legal issues involved, establish timelines and calendars, negotiate draft agreements, and consider preliminary financial information. Your team in this work has included me, City Administrator Leslie Herring, City Attorney Ryan Denk, Financial Advisor Jeff White (Columbia Capital Management), and Bond Counsel Kevin Wempe (Gilmore & Bell).

On June 8, 2023, the City Council approved real estate contracts with the Shawnee Mission School District and with Karbank. On August 7, 2023, the Planning Commission held a public hearing to consider Karbank's request to rezone a portion of the property along 50th and Rainbow for its development. The City currently contemplates that the Planning Commission will make a recommendation to the City Council at its September 11, 2023, meeting, and that the City Council will consider the rezoning and other related development documents at its October 12, 2023, meeting. Should matters not be ready for decision by that date, then they may be made at the November 9, 2023, City Council meeting.

To be sure, your elected officials, volunteers, and City Staff are fielding numerous questions and comments regarding this proposal. Please know, your input is always welcome and necessary. As of the writing of this message, no firm decisions have been made one way or the other as to whether the City will accept Karbank's proposal or rezone any properties. Residents should also know that, in rezoning matters, there are Kansas legal principles that do not allow your elected officials to expressly promise to vote one way or another on such applications. That is, the City cannot "pre-judge" these types of applications. I understand that may be frustrating to residents who want certainty, at this moment, as to what decisions will be made. I do hope residents can understand that this is a process—one of receiving applications, reviewing applications, receiving public input, asking questions, considering the answers to those questions, and only then making decisions.

However, I thought it might be helpful to share with you some information that might provide the full context in which any decisions may likely be made. With this message—it is long, I know—I hope to speak to the City's past planning, our residents' wishes which have previously been expressed, the framework within which decisions will be made, and the impact of this project (or a similar project) on the City's future.

SUMMARY OF KARBANK PROPOSAL

Although the exact details of the plans and the proposal must still be discussed and negotiated further, the following are the broad elements of the proposal, as further refined since Karbank's initial March 9 proposal:

- Utilizing the City's "right of first offer" with the Shawnee Mission School District, the City would acquire the former Westwood View site from the School District, at Karbank's cost.
 - *Per the 2022 City Facilities Assessment and Feasibility Analysis prepared by Multistudio, those acquisition costs were estimated to be \$3 million. The City has since been able to negotiate a purchase price with the School District of \$2.65 million.*
 - *Karbank or related foundations plan to make a donation to the City in the amount of the purchase price, allowing the City to acquire title to the former Westwood View site.*
 - *On May 22, 2023, the Board of Education for the School District approved a contract to sell the former Westwood View site to the City. The City approved this contract on June 8, 2023. Following periods of due diligence, and only if the project is approved, closing on the purchase is anticipated to occur in early 2024.*
- The City would lease the former Westwood View site back to the School District through July 2024, allowing the School District to continue to operate the site for Rushton Elementary students through the remainder of the school year, and otherwise allow the School District to remove educational materials and the like.
- The City would demolish the existing school building, also at Karbank's cost.
 - *Multistudio estimated that the total cost of demolishing the school and parking, plus bringing the property to finish grade and sodding to be between \$1,700,000.00 to \$2,000,000.00. These costs will need to be examined further, as they may have included other park grading costs which would be the responsibility of the City.*
 - *Karbank or related foundations plan to make a donation to the City in the amount of the demolition costs, allowing the City to perform the actual demolition.*

- The City would convey to Karbank the City-owned property currently consisting of Dennis Park and the 5050 Rainbow property. The City would also convey a portion of the former Westwood View site to Karbank to create a larger developable parcel for Karbank (referred to in Planning Commission materials as the “Lot Line Adjustment”).
- As the City understands it, Karbank has also negotiated to acquire a residential property along West 51st Street adjacent to the former Westwood View site, the entirety of which would also be conveyed to the City, at Karbank’s cost. To be clear, the City did not ask for this parcel and was not involved in this transaction. Should Karbank close on that separate parcel, the size of the City park area would be approximately **3.9 acres**. The addition of this property has not yet been brought to the Planning Commission for review.
 - *The Dennis Park site is less than one acre (0.959 acres). Accordingly, the City would gain over 2.9 acres of park space, an increase of more than **three times** as much as the City has currently.*
 - *The 5050 Rainbow parcel is not officially part of the City park and, until recently, was property on which there was development (the Westwood Christian Church), with parking, and which was acquired primarily to provide flexibility to the School District for potential future use and growth. After demolition, the property was fenced and seeded. Even if this green space is taken into account, the City park and green space would increase by over one (1) acre, or over a 38% increase.*
- Karbank would further agree to pay off the remaining balance of the City’s note on the 5050 Rainbow property, which amount is approximately \$275,000.00.
- Karbank proposes developing three to five buildings (they may be connected, such that one building may appear to be two buildings) along Rainbow, primarily for office and limited retail or restaurant purposes (see picture of latest proposed renderings above). The buildings would be served by an underground parking garage with some surface parking. The City is currently negotiating how best to provide a park restroom facility that would serve the new City park.
- Karbank’s proposal does not require that the City locate City Hall to its development. At this time, the City is not considering locating City Hall to this area, given the City’s goal in maximizing green space and also given certain legal and tax issues associated with a city being a tenant in a for-profit development.
- Karbank would work with the City to engage a landscape architect to develop the park. Programming and development of the park would be at the City’s cost. Karbank would require that the City adopt certain restrictive covenants preventing the new park property from being used for anything but a park.
 - *Per the 2022 City Facilities Assessment and Feasibility Analysis prepared by Multistudio, the costs of developing a 3.5-acre park would be between \$2,000,000.00 and \$2,500,000.00 (features factoring into this estimate are identified in Multistudio’s report).*

The City has not yet begun any planning for park layout or amenities, as that would be putting the cart before the horse, as any consideration of developing a park on the former Westwood View site would be predicated on the Karbank proposal (in some form) being approved.

- Karbank has stated that it would not be requesting tax increment financing (TIF) or community improvement district (CID) incentives for its project (both of which are in place for Woodside Village). However, Karbank supports the City pursuing a “public benefit” tax increment financing (TIF) district, the revenues from which would go only to park development and public infrastructure costs, and not to Karbank’s own private development costs. Per state law, the School District’s state levy and capital outlay levies would not be captured by the TIF, if created by the City.
- Karbank may request certain industrial revenue bond (IRB) approvals which would allow Karbank to be exempt from sales tax on construction materials. Last year, the City approved similar IRBs to assist with the remodel of the Woodside South Club. Such IRBs have also previously been approved in Westwood for the Midwest Transplant Network.

WESTWOOD FACILITIES OPEN SURVEY

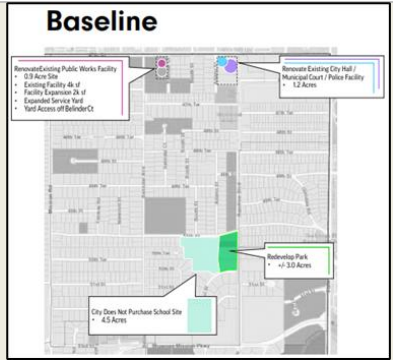
Following the 2022 City Facilities Assessment and Feasibility Analysis, the City conducted resident surveys and held an open house, soliciting and obtaining very good feedback from our residents on options the City might consider that would allow Westwood to acquire the former Westwood View Elementary site, develop a feature park for our community, and also pursue development that can enable the City to continue to provide the services and amenities our residents expect. Per the survey team the City engaged for this process, the levels of participation and the responses received did make the survey results “statistically valid” (that is, the City can rely on the conclusions derived from the survey as accurately representing the views of the community as a whole; there were proper sampling methods, adequate sample sizes, and unbiased data collection methods). The City received 245 responses to the online survey, 25 written comment cards, several emails, and over 60 people attended our open house event. Highlights from those survey results are as follows:

Question: How would you prefer to pay for repair, renovation, and expansion of City facilities and amenities?

- 10.13% Do nothing, pay for repairs and maintenance from the City budget only on an emergency basis.
- 14.35% Add money to the City budget by increasing property taxes.
- 65.82% Add more money to the City budget by leveraging available property and collecting taxes from future development projects.
- 9.70% Other

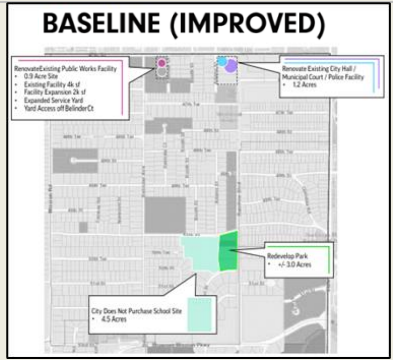
By a large majority, survey respondents felt that the best approach to improving City facilities and amenities was to leverage available property to generate additional revenues from future development projects.

Question: Having reviewed the October 2022 Facilities Assessment and Feasibility Analysis, which scenario from a LAND USE perspective, best aligns with Westwood’s vision? Rank your responses from 1-6.



Repair “as-is” existing City Hall (with police) and refresh finishes; repair “as-is” existing public works facility and refresh finishes; redevelop existing park; City does not acquire the former Westwood View site.

Survey not taken on this (not recommended by Multistudio) for land use purposes, but it was included for the later “financial perspective” question.

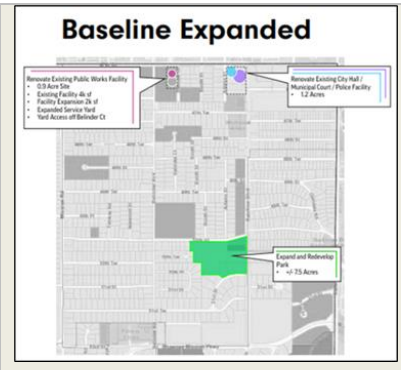


More full renovation of existing City Hall (with police); expansion and renovation of existing public works facility; redevelop existing park; City does not acquire the former Westwood View Site.

First Choice:	13.49%
Second Choice:	15.87%
Third Choice:	7.14%
Fourth Choice:	7.14%
Fifth Choice:	10.32%
Sixth Choice:	46.03%

Note: Labeled as “Baseline Scenario” on survey results.

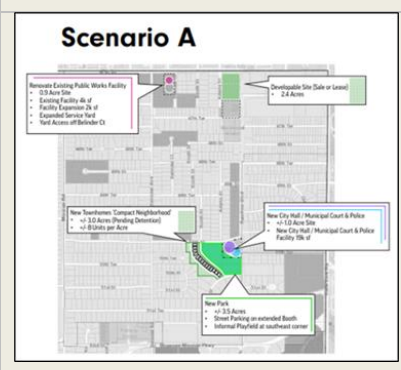
For only 36.5%, this was Top 3; for 63.5%, this was Bottom 3.



More full renovation of existing City Hall (with police); expansion and renovation of existing public works facility; redevelop existing park; City **does** acquire the former Westwood View site; develop and expand the park areas.

First Choice:	27.05%
Second Choice:	16.39%
Third Choice:	5.74%
Fourth Choice:	9.84%
Fifth Choice:	33.61%
Sixth Choice:	7.38%

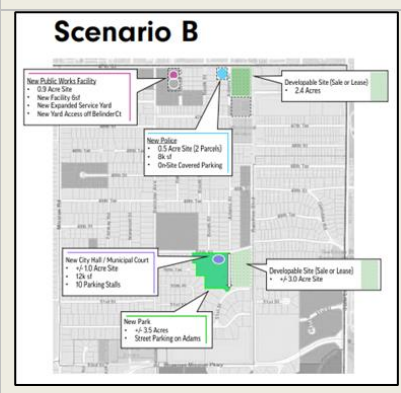
For 49.18%, this was Top 3; for 50.82%, this was Bottom 3.



Locate new City Hall (with police) near 50th and Rainbow; utilize existing City Hall site for development; renovate existing public works facility; City **does** acquire the former Westwood View site; develop and expand the park areas; provide housing options adjacent to the expanded park.

First Choice:	20.97%
Second Choice:	12.90%
Third Choice:	20.97%
Fourth Choice:	19.35%
Fifth Choice:	12.10%
Sixth Choice:	13.71%

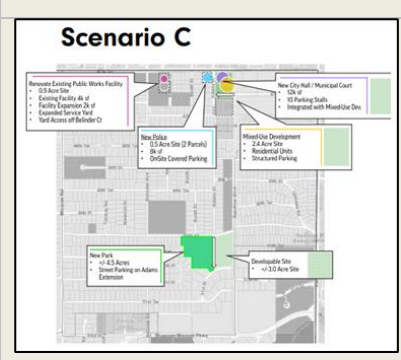
For 54.84%, this was Top 3; for 45.16%, this was Bottom 3.



City **does** acquire the former Westwood View site; locate new City Hall there and expand the park areas; utilize existing City Hall site for development; build new public works facility; locate police on Foundation-owned property at 47th and Adams; utilize 50th and Rainbow frontage for development.

First Choice:	12.90%
Second Choice:	22.58%
Third Choice:	20.97%
Fourth Choice:	24.19%
Fifth Choice:	16.94%
Sixth Choice:	2.42%

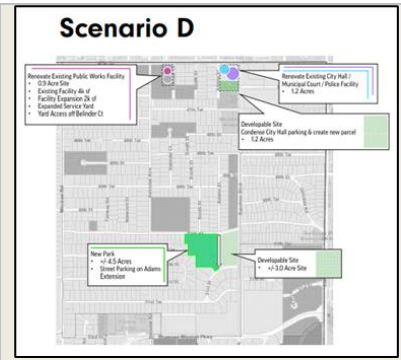
For 56.45%, this was Top 3; for 43.55%, this was Bottom 3.



Integrate a new City Hall at its current location, as part of a mixed-use development; City **does** acquire the former Westwood View site; develop and expand the park areas; renovate existing public works facility; locate police on Foundation-owned property at 47th and Adams; utilize 50th and Rainbow frontage for development.

First Choice:	16.94%
Second Choice:	20.16%
Third Choice:	23.39%
Fourth Choice:	13.71%
Fifth Choice:	16.94%
Sixth Choice:	8.87%

For 60.49%, this was Top 3; for 39.51%, this was Bottom 3.



More full renovation of existing City Hall (with police); create a developable parcel to the south; renovate existing public works facility; City **does** acquire the former Westwood View site; develop and expand the park areas; utilize 50th and Rainbow frontage for development.

First Choice:	12.10%
Second Choice:	12.10%
Third Choice:	20.16%
Fourth Choice:	25.00%
Fifth Choice:	9.68%
Sixth Choice:	20.97%

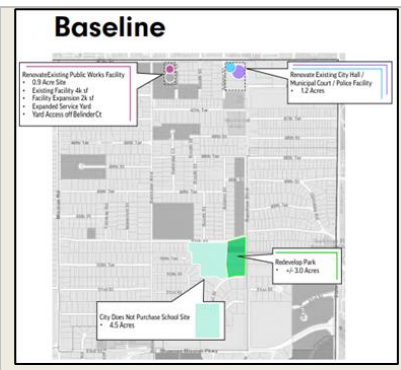
For 44.36%, this was Top 3; for 55.64%, this was Bottom 3.

In terms of the Top 3, where a majority of respondents were in favor of a scenario, options were ranked in the following order:

- Scenario C (which includes new park and development along Rainbow), at 60.49%;
- Scenario B (which includes new park and development along Rainbow), at 56.45%; and
- Scenario A (housing in park area, but new City Hall development along Rainbow), at 54.85%.¹

The City will likely be keeping this in mind when considering the Karbank proposal, and the extent to which it does (or does not) align with the scenarios supported by a majority of survey respondents, from a land use perspective.

Question: Having reviewed the October 2022 Facilities Assessment and Feasibility Analysis, which scenario from a FINANCIAL perspective, provides the best approach for Westwood? Rank your responses from 1-6. Note: Some of the scenarios produce revenue over time; others do not.



Total estimated cost, per Multistudio’s report, would be between **\$4,050,000** and **\$5,075,000**.

No additional revenue for the City to help pay for these expenses, outside of potential property taxes from future unknown development of the former Westwood View site.

First Choice:	11.21%
Second Choice:	13.79%
Third Choice:	9.48%
Fourth Choice:	5.17%
Fifth Choice:	18.10%
Sixth Choice:	42.24%

For only 34.48%, this was Top 3; for 65.52%, this was Bottom 3.

Repair “as-is” existing City Hall (with police) and refresh finishes; repair “as-is” existing public works facility and refresh finishes; redevelop existing park; City does not acquire the former Westwood View site.

¹ Note: By weighted score, the survey results resulted in Scenario B ranked highest (3.83), followed by Scenario C (3.80), and then by Baseline Expanded (3.71).



More full renovation of existing City Hall (with police); expansion and renovation of existing public works facility; redevelop existing park; City does **not** acquire the former Westwood View Site.

Note: Mislabeled as "Baseline Expanded" on survey results (but described similar to "Baseline Improved" land-use question).

Total estimated cost, per Multistudio's report, would be between **\$7,275,000** and **\$9,350,000**.

No additional revenue for the City to help pay for these expenses, outside of potential property taxes from future unknown development of the former Westwood View site.

- First Choice: 17.39%
- Second Choice: 13.91%
- Third Choice: 6.09%
- Fourth Choice: 10.43%
- Fifth Choice: 33.91%
- Sixth Choice: 18.26%

For only 37.39%, this was Top 3; for 62.61%, this was Bottom 3.

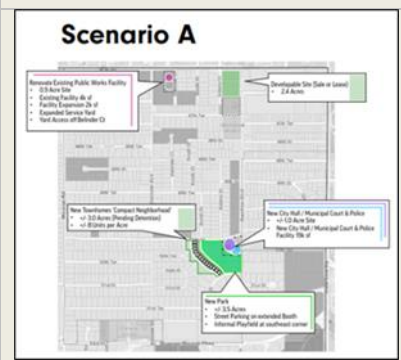


More full renovation of existing City Hall (with police); expansion and renovation of existing public works facility; redevelop existing park; City **does** acquire the former Westwood View site; develop and expand the park areas.

Total estimated cost, per Multistudio's report, would be between **\$14,475,000** and **\$17,350,000**.

No additional revenue for the City to help pay for these expenses.

Note: Survey not taken on this for financial perspective purposes, and the "Baseline Improved" was labeled as this by mistake. This option was included in the "land use" question.



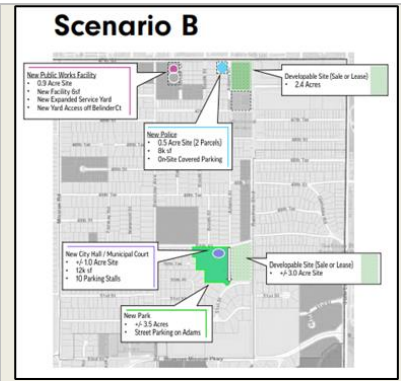
Locate new City Hall (with police) near 50th and Rainbow; utilize existing City Hall site for development; renovate existing public works facility; City **does** acquire the former Westwood View site; develop and expand the park areas; provide housing options adjacent to the expanded park.

Total estimated cost, per Multistudio's report, would be between **\$20,800,000** and **\$24,625,000**. Total estimated revenues would be between **\$14,000,000** and **\$17,100,000**. Total estimated net cost would be between **\$6,800,000** and **\$7,525,000**.

With revenues, the total City costs are estimated to be **less** than doing the "Baseline Improved" repairs only, between **(-\$475,000)** and **(-\$1,825,000)**.

- First Choice: 21.74%
- Second Choice: 13.04%
- Third Choice: 17.39%
- Fourth Choice: 25.22%
- Fifth Choice: 10.43%
- Sixth Choice: 12.17%

For 52.17%, this was Top 3; for 47.83%, this was Bottom 3.



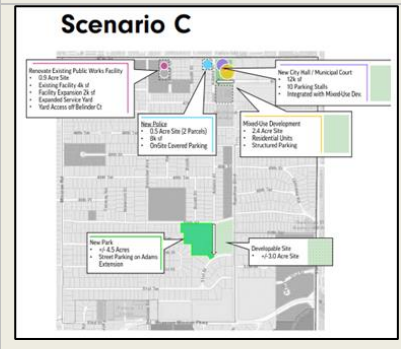
City **does** acquire the former Westwood View site; locate new City Hall there and develop and expand the park areas; utilize existing City Hall site for development; build new public works facility; locate police on Foundation-owned property at 47th and Adams; utilize 50th and Rainbow frontage for development.

Total estimated cost, per Multistudio's report, would be between **\$22,775,000** and **\$26,925,000**. Total estimated revenues would be between **\$19,800,000** and **\$24,300,000**. Total estimated net cost would be between **\$2,625,000** and **\$2,975,000**.

With revenues, the total project costs are estimated to be less than doing "Baseline Improved" repairs only, between **(-\$4,300,000)** and **(-\$6,725,000)**.

First Choice:	14.16%
Second Choice:	24.78%
Third Choice:	27.43%
Fourth Choice:	18.58%
Fifth Choice:	10.62%
Sixth Choice:	4.42%

For 66.37%, this was Top 3; for 33.63%, this was Bottom 3.



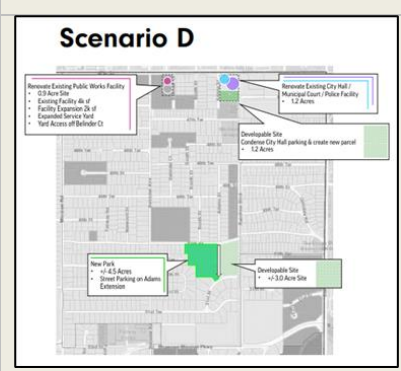
Integrate a new City Hall at its current location, as part of a mixed-use development; City **does** acquire the former Westwood View site; develop and expand the park areas; renovate existing public works facility; locate police on Foundation-owned property at 47th and Adams; utilize 50th and Rainbow frontage for development.

Total estimated cost, per Multistudio's report, would be between **\$23,260,000** and **\$27,715,000**. Total estimated revenues would be between **\$19,800,000** and **\$24,300,000**. Total estimated net cost would be between **\$3,415,000** and **\$3,460,000**.

With revenues, the total project costs are estimated to be **less** than doing "Baseline Improved" recommended repairs only, between **(-\$3,815,000)** and **(-\$5,935,000)**.

First Choice:	15.04%
Second Choice:	23.01%
Third Choice:	24.78%
Fourth Choice:	18.58%
Fifth Choice:	14.16%
Sixth Choice:	4.42%

For 62.83%, this was Top 3; for 37.17%, this was Bottom 3.



More full renovation of existing City Hall (with police); create a developable parcel to the south; renovate existing public works facility; City **does** acquire the former Westwood View site; develop and expand the park areas; utilize 50th and Rainbow frontage for development.

Total estimated cost, per Multistudio's report, would be between **\$13,075,000** and **\$15,750,000**. Total estimated revenues would be between **\$15,500,000** and **\$19,100,000**. This scenario would result in total estimated savings to the City of between **(-\$2,425,000)** and **(-\$3,350,000)**.

With revenues, the total project costs are estimated to be less than doing "Baseline Improved" recommended repairs only, between **(-\$9,700,000)** and **(-\$12,700,000)**.

First Choice:	24.58%
Second Choice:	13.56%
Third Choice:	11.86%
Fourth Choice:	21.19%
Fifth Choice:	10.17%
Sixth Choice:	18.64%

For 50%, this was Top 3; for 50%, this was Bottom 3.

In terms of the Top 3, where a majority of respondents were in favor of a scenario, options were ranked in the following order:

- Scenario B (which includes new park and development along Rainbow), at 66.37%;
- Scenario C (which includes new park and development along Rainbow), at 62.83%; and
- Scenario A (housing in park area, but new City Hall development along Rainbow), at 52.17%.²

Accordingly, Scenarios B and C both finished in both poll questions as the Top 2 preferred scenarios, both of which contemplate a new park at the former Westwood View site, and both of which contemplate development of some kind on the City-owned parcels along Rainbow. This is also consistent with the 65.82% of survey respondents who indicated they would prefer to fund improvements to City facilities (City Hall, our Park, and Public Works) by leveraging available property and generating additional revenue from future development on those sites.

It is difficult to make a financial comparison of Karbank's proposal to the scenarios discussed above, as it would be somewhat be "apples to oranges". The City has engaged a financial advisor (at Karbank's cost) to assist with a financial analysis. It is extremely important to note that the City's own financial advisor is preparing updated estimates, using the Karbank proposal as its model, and that these numbers may be quite different from Multistudio's when the financial study is presented to the City Council. To be sure, multiple scenarios can be constructed, likely in multiple ways to show degrees of value, benefit, or burden and, ultimately, the value proposition will have to be established with firmer figures. Other factors that may influence these analyses include:

- Whether or not the City actually imposes an additional sales tax of 1% within the City, as modeled by Multistudio;
- Multistudio's proposed use of the property as mixed-use with residential (as opposed to Karbank's proposal with mostly office and mixed-use); and
- The impact of a TIF plan (not modeled by Multistudio) on City costs and reimbursements (discussed further below).

Even so, our City decision-makers will also consider the extent to which the Karbank proposal does (or does not) accomplish other goals of the City:

- Increasing park and green space (including ADA compliance, restrooms for park users);
- Putting tax-exempt properties to productive use, especially along a major corridor;
- Generating increased revenue for general City operations (helping to either finance additional City work such as facility needs at City Hall or Public Works, or reducing the property tax burden on Westwood residents);
- Eliminating debt on 5050 Rainbow; and
- Providing commercial opportunities and other amenities desired by residents.

² Note: By weighted score, the survey results resulted in Scenario B also ranked highest (4.00), followed by Scenario C (3.93), and then by Scenario A (3.74).

BIG PICTURE vs. PROJECT-SPECIFIC CONSIDERATIONS

To be sure, there are (and will be) many important things to consider as this process continues. You, as residents will have questions about, and want to discuss, specific “programming” elements, which may include the following:

- Square footage (+/-) of space to be provided or used;
- Parking and traffic flow;
- Desired park amenities (sport courts or fields, walking paths, water features, types of playground equipment, and the like);
- Building materials and design; and
- Types of users and tenants.

Many of those matters will be discussed and addressed during the process, as the City and Karbank move from concept to design development, to schematic design, to site plans, to final engineering and construction documents. That process does take time, and I would encourage residents to not “lose the forest for the trees” at the earliest of stages. Over the next few pages, I wish to bring up a few of these big-picture considerations, which should always be kept in mind.

Budgetary Considerations.

The City currently operates on an approximately \$5 million budget. Although much of the public focus can be on the property tax mill levy, only 25% of the City’s general operating budget is funded through property taxes. Sales and use taxes make up approximately 34.5% of the City’s operating budget revenue. Accordingly, the City must be smart in its planning to ensure that Westwood maintains a healthy mix of revenue sources, and that Westwood looks for opportunities to generate revenue from new sources.

For example, there is a large amount of property within our City, and especially on our primary commercial corridors—47th Street, Rainbow, and Shawnee Mission Parkway—that is tax-exempt (or nearly so, or possibly eligible for exemption now or in the future, based on current uses), as shown in blue below:



These parcels make up 35.06 acres, or 13.67% of Westwood’s total land area. This is not to say that any of these uses are not valuable or welcome, but merely to say that the tax-exempt status of those properties must go into our current and future budget planning, to ensure that the City has reliable and varied sources of commercial revenue and does not have to rely too heavily on resident property taxes.

Therefore, in considering Karbank’s proposal, the City should ask itself what impact any proposal might have—positively or negatively—on: our City budget; our ability to ensure that we can continue to provide the City services our residents expect; our ability to provide even more services our community desires; our ability to maintain and improve streets and roads; our ability to provide good police protection; our employees’ needs for adequate and competitive wages; and more. There are purely financial reasons why the City must look at its budget, plan for the future, consider opportunities, and work to ensure our City remains viable in the long term.

Growth and Needs of Westwood in the Future

When engaging in planning, the City must consider both its current and its future residents. Our City has had times where not looking ahead, or not considering future resident needs, has led to decreases in population, especially at our local elementary school, causing Westwood View to—at one time—be on a list for possible closure. Certainly, the wishes and desires of our current city residents must be paramount. However, when considering zoning and planning decisions, the “public” welfare means the community as a whole, and not just immediate neighbors. It appears that the Westwood community does support proposals that would provide for larger green space and development along Rainbow, given the responses to the survey questions discussed above, and the work of our past planning studies (discussed more below).

As to our future residents, the City should ask, what will they look for when moving to a community? Examples may include: the ability to buy a home; the opportunity to attend good schools; conveniently-located shopping and restaurants; perhaps opportunities for employment; well-kept roads and infrastructure; public safety; quality parks and other amenities. I daresay when current residents someday hope to sell their home, they will hope that there is a market for a home in Westwood that will attract buyers. In that sense, planning for our future ensures a continued good quality of life for our current residents.

My sense of the community is that—whether through the Karbank plan or another plan—Westwood would appreciate more green space and park space. Improved park offerings may also help retain residents and attract new ones (especially for our new elementary school, to ensure its continued viability).

Past Community Input; Studies, Plans, and Recommendations

In considering these issues, and in preparing to make decisions on these issues, the City Council and Planning Commission have been and will be guided by the work that has come before this moment—all with community input. These include the following:

- 2015 Urban Land Institute (ULI) Technical Assistance Panel Study (available at <https://westwood.govoffice2.com/uli2021tap>);
- 2017 Comprehensive Plan (available at https://westwood.govoffice2.com/comprehensive_plan);
- 2021 Urban Land Institute Technical Assistance Panel Study (available at <https://westwood.govoffice2.com/uli2021tap>); and
- 2022 City Facilities Assessment and Feasibility Analysis (available at <https://westwood.govoffice2.com/facilitiesassessment>).

Although this message cannot lay out every detail of these studies and plans, on the following pages I have included excerpts from them, to show how they may relate to the Karbank proposal.

2015 Urban Land Institute Technical Assistance Panel Study

• Dennis Park, while well loved by all, is in need of updating to be ADA compliant, a redesign for better use of the tennis court/ice rink, more field space, and may not be located in the safest location due to the adjacent busy road.

• With the loss of Sprint's property tax, there is a need for intelligent planning to increase the sales and property tax base.

The panel agreed that the best way to help the community create certainty about their identity would be to establish an overall goal to attract more young families and to keep this in mind when creating alternatives for the community.



The core driver to achieve the community's vision is to ensure future viability of the school. To accomplish this goal, the City of Westwood needs to:

- Increase housing choices that appeal to young families to boost enrollment.
- Increase and improve park space. Additional usable park space will attract young families.
- Increase tax base by facilitating development in appropriate locations. In order to produce additional revenue, land that is owned by the City of Westwood should be considered for redevelopment as a public-private partnership.

2017 Comprehensive Plan

Promote and protect the neighborhood character of existing single-family detached homes

Residents highly value the character of the single-family detached homes located throughout the majority of the community. The Plan seeks to protect and enhance these residential areas by focusing and encouraging commercial redevelopment activity only on to two exterior corridors of the community.

Community civic spaces & additional open green space areas

Communities with a high quality of living component connect residents to each other through the built environment. Community civic spaces and outdoor open green space areas function as social settings that encourage people to walk and gather within the community. The Plan outlines the desire and strategies for the development of community connection areas - additional open green space areas and community civic space opportunities.

Commercial redevelopment activity should be focused along the 47th Street and Rainbow Boulevard corridors, with smartly-designed transitions into the adjacent residential areas. This includes developments with a distinctive character that is compatible with a walkable, livable place.

Local businesses, patrons and visitors

Businesses in Westwood, like residents, have an interest in the overall well-being of the community. It is essential for Westwood to strive for future development projects that work within a unified framework fostering civic life and economic vitality. The Plan demonstrates the commitment to the commercial corridors along 47th Street and Rainbow Boulevard. Also, these businesses will benefit from the policies that focus on capital investments such as sidewalk improvements, street lighting, and parking.

2017 Comprehensive Plan

WESTWOOD VIEW ELEMENTARY

Potential uses for this site include:

- New or replacement elementary school facility
- Park and open green space
- Civic uses
- Residential property for single-family homes

5050 RAINBOW

The possibilities for this site include:

- Park and open space. Joe D. Dennis Park can be expanded onto this site which would allow for additional park features and recreational opportunities in the community.
- A low-intensity development could include some retail services to nearby residents and businesses along with residential uses. There is also an opportunity to include a civic use as part of any redevelopment of this location.

2021 Urban Land Institute Technical Assistance Panel Study

Leveraging and expanding Joe D. Dennis Park into a larger, more significant City and civic asset presents a transformational opportunity for the City of Westwood. The park is owned and maintained by the City and today features a small gathering space, formal playground, a playscape that uses the site's topography, and a tennis court. It is bordered by the current elementary school site to the west, and to the south is open land that was once the location for the Westwood Christian Church. The northern border is W. 50th Street, and Rainbow Boulevard borders it to the east.



Park Redevelopment Scenario 1
Civic Park & Plaza on 50th Street

- Relocate City Hall to the existing school site
- Maintain the mature tree canopy present in Joe D. Dennis Park
- Maintain and improve playing fields to the south of Joe D. Dennis Park for flexible play
- Expand park design across the remainder of the site

Density along the Edges. With W. 47th Avenue and Rainbow Boulevard edging the city to the north and east, stakeholders would like to see more dense uses along those edges of the community.

Local Amenities (parks, open space, trails, retail). Westwood residents love their parks and shops and want more of them.

Tax Base. It is important to consider the current and future tax base for the City to help ensure that it remains a financially viable and thriving community into the future.



Park Redevelopment Scenario 2
Civic Park & Plaza on Rainbow

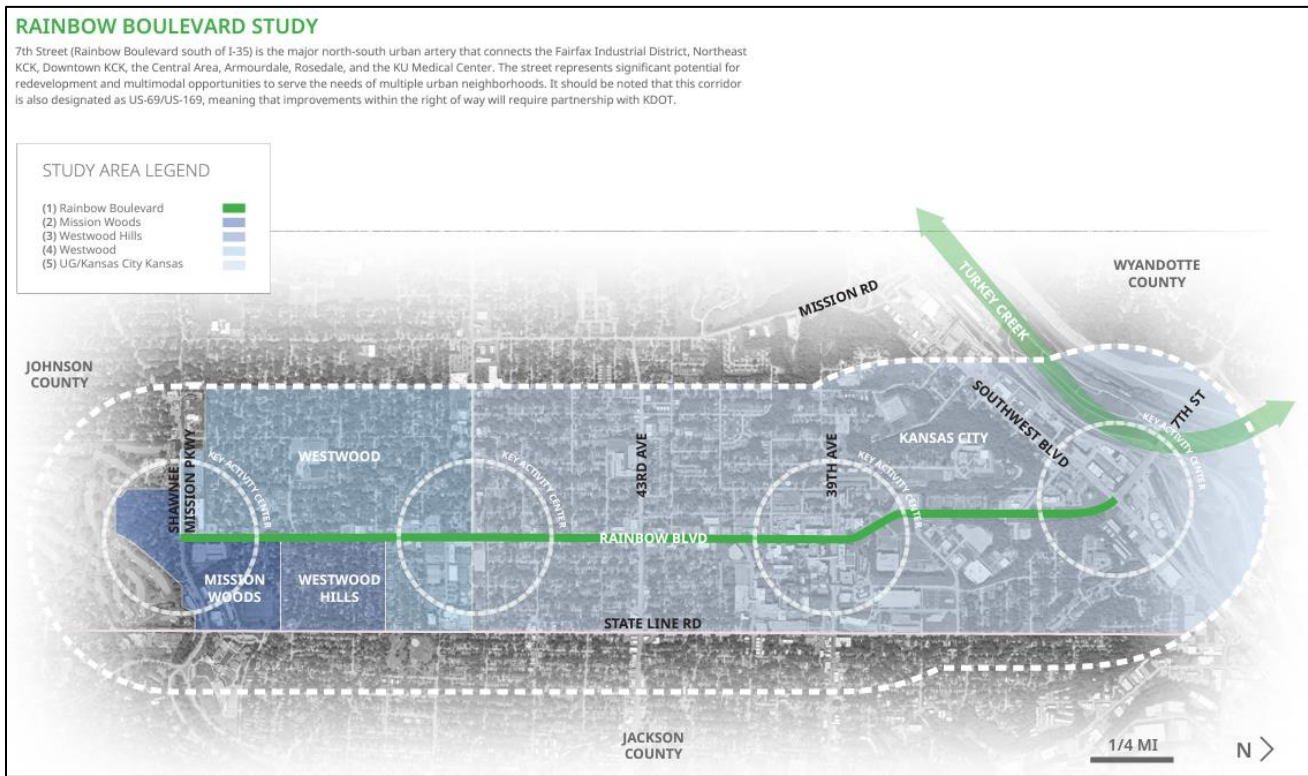
- Relocate City Hall to the existing school site and face Rainbow Boulevard
- Maintain the mature tree canopy of existing Joe D. Dennis Park and buffer the playground with a civic plaza
- Relocate the playing fields/flexible play area to the central part of site
- Expand the park across the remainder of the site

A significant amount of work and community input went into analyzing these issues and developing recommendations for future implementation. Consideration of Karbank's proposal (and, again, any proposal that may come before the City) should always keep these broad policy goals and planning directions in mind.

RAINBOW BOULEVARD COMPLETE STREETS TRAFFIC MANAGEMENT PLAN

Along with this potential project, residents should know that the City is also exploring options for ensuring that Rainbow Boulevard remains a safe corridor for the City or, to the extent it is not, making needed improvements for our community. This would include any crossings which may be impacted by a potential project along Rainbow.

In early 2022, the City began discussions with other municipalities and agencies (including the Kansas Department of Transportation, the City of Mission Woods, the Rosedale Development Association, the Unified Government of Wyandotte County and Kansas City, Kansas, KU Health Systems, and the Kansas City Area Transportation Authority) to begin evaluation possible improvements to Rainbow Boulevard, as contemplated by the City’s Comprehensive Plan, its 2020 Complete Streets Plan, and the 2021 Urban Land Institute Technical Assistance Panel Study. Options to be studied include, but may not necessarily be limited to, reduction of lanes, improved sidewalks and crossing, landscaping, and facilities for alternative modes of transportation (such as bicycles).



After presenting the Rainbow corridor’s needs, the Mid-America Regional Council (MARC), through its Sustainable Places Policy Committee, recommended that this project receive the full amount of federal funding available for projects such as this. Both the City of Westwood and the City of Mission Woods (as well as other of the groups mentioned above) agreed to contribute financially to this project as well.

In January of this year, MARC issued a request for proposals for a “Rainbow Blvd./7th Street/Hwy. 169 Complete Streets Traffic Management Plan” as part of MARC’s “Planning Sustainable Places” program. Since that time, the MARC board of directors approved the scope of work negotiated with the firm selected by the project steering/advisory committee of stakeholders, and a notice to proceed was issued in May of this year.

A project kick-off with WSP Consulting, MARC, and the local project stakeholders took place in June, and public engagement is scheduled to begin here at the end of August. An opinion survey and additional public engagement activities and opportunities will take place from August through November, with final reporting out of recommendations and findings to take place in the first quarter of 2024. Please follow along with, and participate in this process, by signing up for City communications and following the City’s social media channels (links to which are provided below).

ISSUES PERTAINING TO THE SHAWNEE MISSION SCHOOL DISTRICT

Now that students have moved into the new Westwood View, the Shawnee Mission School District—which owns the former Westwood View site—has relocated students from Rushton Elementary in Mission, Kansas, into the former site. This is being done so that the School District can tear down and rebuild the current Rushton Elementary in its current location. Construction (including demolition) at Rushton Elementary will likely conclude in a period of time such that the new Rushton Elementary could open in Fall 2024. That means, for about the next year, the former Westwood View site will likely be filled with students and used by the School District.

As most of you know, several years ago the City negotiated a “right of first offer” agreement with the School District. This was set to expire very soon, on February 2, 2024. Therefore, if Westwood desired to control the future of development on the former Westwood View site (outside of utilizing zoning controls), then the City would likely need to acquire the property itself, or otherwise find a path working with others (such as Karbank) to acquire and develop the property. With the building likely being empty in less a year, and with the City’s option agreement expiring in just a few months, time became of the essence.

On May 22, 2023, the School District approved a form of real estate contract with the City. The City Council approved this contract on June 8, 2023. With that, the City’s option has been exercised and is no longer in place. That is, the City does not have a second option should the Karbank project not ultimately be approved. This is the City’s opportunity to purchase the former Westwood View, and especially for only **\$2.65 million** (which is below its current appraised value).

Of course, the City does not have \$2.65 million in cash-on-hand to acquire the former Westwood View site. If the City does not pursue, or ultimately rejects, a development opportunity that would (a) not involve a development partner (such as Karbank) financing the City’s acquisition of the site, or (b) not generate sufficient new revenues through new development that would allow the City to finance an acquisition, then the City would really only have two options:

Option 1: Allow the School District to sell the former Westwood View site to another party.

The property could possibly be marketed and sold to anyone. The property is currently zoned “R-1” (Single-Family Residential) which would—as a matter of right—allow the property to be used for single-family homes, accessory dwelling units, parks or playgrounds, churches, public or parochial schools, city halls, police stations, group homes, and various accessory uses.

A future owner of the property could request that the property be rezoned for other uses as well (such as commercial, office, mixed-use, planned residential cluster housing, etc.). Any rezoning would require a public hearing before the Planning Commission and consideration by the City Council. Outside of direct ownership, zoning is the primary control the City would have on future use of the property; however, Kansas law does not allow the City to exercise that control in an arbitrary or capricious manner.

The former Westwood View property would not likely be used for a larger City park. It could certainly be developed for single-family homes. However, such a development would not likely generate nearly the same levels of additional tax revenue to the City as would development along Rainbow, and it is likely that the current Dennis Park and 5050 Rainbow would remain undeveloped as a park and green space. Future improvements to those properties would require looking elsewhere for additional revenue. Additional analysis on this point is provided below.

Option 2: Borrow Money (through bonds) and raise property taxes to acquire the site.

In 2022, one mill in Westwood generated approximately \$37,500.00 in property taxes. The current mill levy rate is 21.198. Assuming Westwood could “bond” (borrow) monies for acquisition of the former Westwood View over a 20-year period (at an acquisition cost of \$2.65 million), and assuming the City did not pursue any development opportunities that would generate other revenues (using only property taxes to fund debt service), and at an assumed

current municipal bond interest rate of 4.15%, and not taking into account any premiums or additional closing costs, that would result in a debt service requirement of approximately \$195,225.00 per year.

The City would have to increase its mill rate by **5.206** mills (and maintain this increased level) to pay that annual debt service, a **24.56%** mill levy increase. For the average Westwood home (with an appraised value of \$357,510 for 2023, and assessed at 11.5% per Kansas law, providing an assessed value of \$41,113.65), a single-family-home resident’s property taxes—just for Westwood—would increase from **\$871.53** to **\$1,085.57** per year. This would be on top of an average increase in Westwood from 2022 to 2023 of \$105.47 per year, based solely on higher appraised values. I would have serious concerns over imposing such a drastic increase on Westwood residents, especially at a time when the City is also considering housing affordability issues.

Furthermore, such a mill levy increase would only cover the costs (over 20 years) for park acquisition. Demolition costs (estimated by Multistudio to be an additional \$1.7 million to \$2 million) and park improvement costs (estimated by Multistudio to be between \$2.6 million and \$3.2 million for a 4.5-acre park on all of the property) would **not** be included. Additional City needs, including improvements to other City facilities, would also not be met by this increase.

The combined costs of all of the foregoing, at the high-end estimates (\$2.65 million for acquisition; \$2 million for demolition, site work, and reseeded; and \$3.2 million for development of a larger 4.5-acre park) equals **\$7.85 million**. Using the above calculations, if Westwood were not to have a development partner, not have additional development to help finance costs, and go it alone, using only increases in property taxes, the calculations would be as follows:

- Total Cost:	\$7,850,000.00	
- Annual Debt Service:	\$578,306.52	(4.15%, 20 years)
- Annual Debt Service Divided by \$37,500.00 (amount generated from 1 mill in 2022):	15.422 mills	
- New Total Mill Levy:	36.62 mills	(21.198 + 15.422)
- Percentage Mill Levy Increase:	72.75%	
- Average Annual Home Property Tax Bill: (Westwood only):	\$1,505.58	(72.75% increase)

This calculation also does not include the cost of paying off the balance of the City’s note on 5050 Rainbow, \$275,000.00.

If the City were to **not** demolish the school building, and perhaps use it for a community center or other municipal purposes, the costs of doing so are not exactly known, but the Multistudio report provides at least some direction. At the high-end, Multistudio estimated that the costs of renovating our current City Hall building would be \$4.425 million (Baseline Expanded), and the cost of a new City Hall (without police, an additional cost) would be \$8.6 million. Even assuming the lower Baseline Expanded scenario would apply to the former Westwood View school (which is not likely a good assumption, given the completely different uses between the two buildings), total costs (including acquisition) could be approximately **\$7.075 million**, and likely much more, raising the same issues as to mill levy increases. Again, this would also does not include the cost of paying off the balance of the City’s note on 5050 Rainbow, \$275,000.00.

Simply put, parks and other amenities do not pay for themselves. The City cannot “just” turn property into a park or other type of civic asset without a consideration of these costs and revenues. If the City wishes to pursue its goals in terms of increasing parks and green space, updating those amenities, ensuring compatible developments, controlling its future as to the Westwood View site, and developing a budget that does not overwhelm residents—if these are indeed the goals of the City (and they seem to be, as reflected in the above plans)—then the City must likely look for partnerships, opportunities to raise revenue in other ways, and leverage City-owned properties that are currently tax-exempt and do not provide revenue to our City (the solution most-approved by survey respondents).

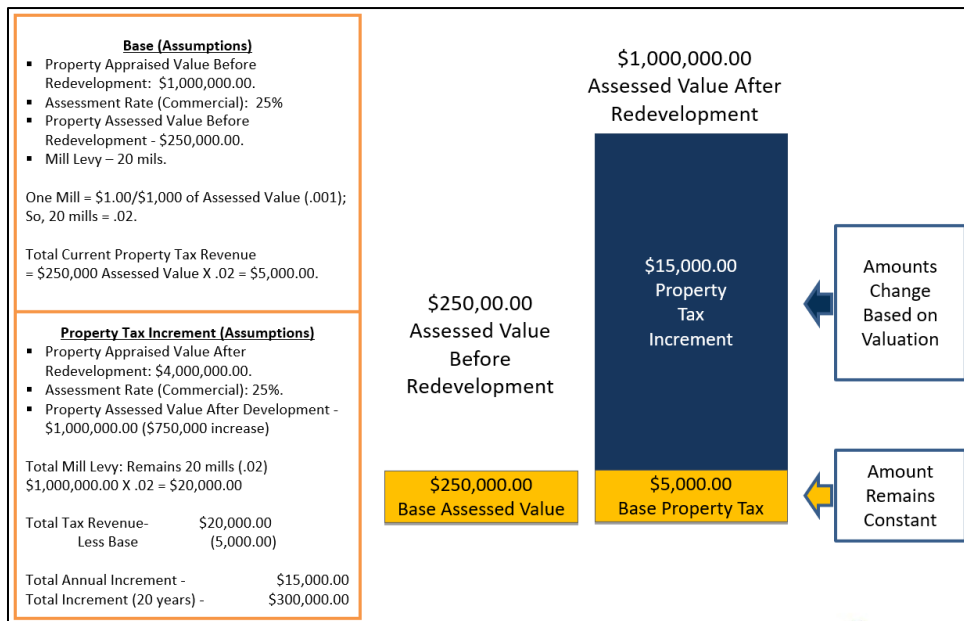
PUBLIC BENEFIT TAX INCREMENT FINANCING (TIF)

Should the City ultimately approve the Karbank proposal, it would be the City’s responsibility to develop a new City park, at the City’s cost. Multistudio estimated those costs would be (for a 3.5-acre park, and on the high end) at \$2.5 million. To help the City pay for those costs, an agreement-in-principle has been reached with Karbank regarding establishment of a “public benefit TIF” (tax increment financing) district (note, Karbank’s approval would not be necessary for the City to establish such a TIF; this option presents itself solely because Karbank has stated that it would not require TIF incentives itself for its development).

Here, I would like to make an important caveat. The establishment of a TIF “redevelopment district” and adoption of a “redevelopment project plan” are also public processes, requiring public hearings, requiring an analysis of whether certain factors have been met, and reviewing financial estimates and projections prepared by the City’s financial advisor. As of the date of this message, that modeling has not yet been presented to the City Council. The City’s public hearing on whether or not to create a TIF district will be held at the City Council’s regular meeting on **Thursday, September 14, 2023**. In anticipation of that meeting, a general overview of TIF for our residents is important.

Under a traditional TIF (of the kind requested from developers to help finance private development projects), certain property tax and sales tax revenues can be “captured” and, instead of those revenues going to local units of government (the County, Johnson County Community College, the City, etc.), they are redirected back into the project itself (primarily for acquisition costs, site work, infrastructure, utilities, and other improvements; “vertical” buildings are not TIF-eligible, but parking garages and structures are). Under a public benefit TIF, instead of those TIF revenues being directed back to Karbank (or another developer) for its project, the revenues could all go to the City for use in public infrastructure and park development.

At the risk of oversimplifying the TIF process, it would start by creating a “TIF redevelopment district” which would likely contain, at a minimum, the current Westwood View site and all of the property intended to be developed by Karbank. A key reason for this is that, as a park, the former Westwood View site would not generate any additional property or sales taxes, so the intended Karbank site, as developed, would be the only revenue generator. The following chart shows how a public benefit TIF could generally work. (Note, for this first graphic, I am using example values only of a hypothetical piece of property, with hypothetical numbers, just to make the math easier to follow).



At \$15,000.00 per year in increment, over the 20-year life of a TIF, the above project would generate \$300,000.00 toward redevelopment expenses (this assumes, of course, property values do not go up at all during that 20-year span, which they likely would). (Note, TIF may also be used to capture incremental sales taxes; again, for simplicity, I have not included that possibility here.)

Again, as part of a public benefit TIF analysis, the City has engaged a professional financial advisor. However, until those analyses are completed and presented to the City Council, below is an example (prepared by me) only of how such a TIF might work in the Karbank case. For this, I am using Karbank’s three-building development in Mission Woods (the “1900 Building” development), merely as a guide. The below figures may not reflect the actual estimated values of a project in Westwood (and are likely lower than what they would be in Westwood), but I use this project simply because it is one with which most residents are familiar.

- **Base Assessed Valuation within TIF District at time of Formation: \$0.00**
 - *The City-owned parcels on Rainbow and the School District-owned former Westwood View site are tax-exempt, and have no assessed value for tax purposes.*

- **Appraised Value of Karbank Development after Completion: \$15,542,000.00**
 - *This is based on the combined 2023 value of Karbank’s office development project in Mission Woods.*

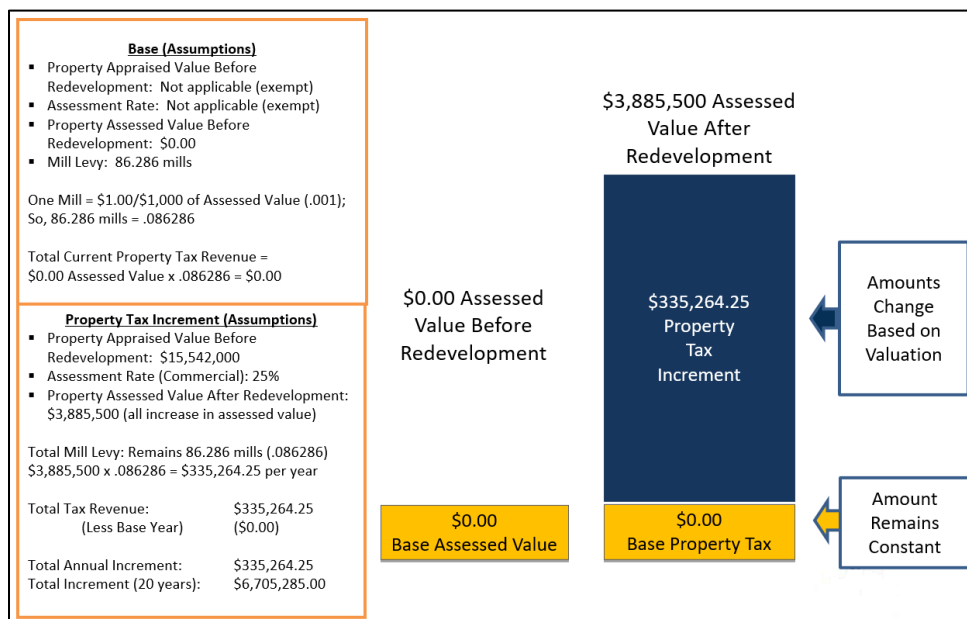
- **Assessed Value of Karbank Development after Completion: \$3,885,500.00**
 - *This value is achieved by taking the appraised value (\$15,542,000) and multiplying it by the Kansas assessment rate for commercial properties (25%).*

- **Mill Levy Rates (2023):**

	<u>Total</u>	<u>TIF Eligible</u>	
- USD 512 Bond:	7.453	7.453	
- USD 512 Uniform:	20.000	Excluded	
- USD 512 Other	21.933	13.933	(8.000 mills for USD capital outlay excluded)
- JCCC:	8.617	8.617	
- Fire District:	10.477	10.477	
- Library:	3.815	3.815	
- JoCo Parks:	3.021	3.021	
- JoCo:	17.772	17.772	
- Kansas:	1.500	Excluded	
- <u>Westwood:</u>	<u>21.198</u>	<u>21.198</u>	
TOTAL:	115.786	86.286	= .086286

- **Assessed Value (\$3,885,500) times (x) Mill Levy (0.086286) = \$335,264.25**
 - *This is the amount of TIF revenue the Karbank project—again, using the Mission Woods project as a model—could generate per year, not accounting for any increases in property values.*

- **TIF Revenue over 20 years (maximum life of a TIF plan): \$6,705,285.00**
 - *\$335,264.25 per year, for 20 years, the maximum life of a TIF project plan (again, without accounting for any increases in property values over those 20 years).*



Under these assumptions, if the Karbank project in Westwood was valued similarly to the 1900 Building development, a public benefit TIF could generate for Westwood—over 20 years—**\$6,705,285.00** in incremental property taxes (this would not include possible incremental sales taxes as well). These numbers could be much higher depending on the ultimate value of the Karbank development.

At the end of the day, this type of proposal with a public benefit TIF would produce significantly more revenue for the City than a comparable Multistudio model would (if elements of their models related just to this area of the City were broken out). Structured correctly, and accounting for reimbursements from these TIF funds for acquiring the property, demolishing the existing school, and developing a new park, and also removing the 5050 Rainbow debt from the City's books, this structure could free up a significant amount of general fund dollars that could then be used for:

- Additional street repairs, street lighting, and capital improvements;
- Renovating and expanding the Public Works building and expanding the yard (\$2.125 million on the low end of Multistudio's estimate; \$2.8 million on the high end);
- Providing funds for a City Hall renovation or relocation;
- Assisting with costs in long-term maintenance of the park (e.g., additional employees);
- Continuing to remain competitive with employee salaries and benefits.

In addition to the financial benefits, the City would have a new, larger park and supporting commercial development bringing amenities to the community. If a scenario such as the above were to play out, that would also allow the City to perhaps accomplish some of these things without additional mill levy increases.

To be sure, the above scenarios use "static" dollars, and do not account for either increases in property values (on the positive) or costs of interest-carry (on the negative). However, other revenues not included in these scenarios include new sales tax revenues from any development, the City's stormwater utility fee, utility franchise fees, and one-time building permit fees.

Again, I caution that these are only preliminary numbers based on the assumptions I use above. The City's financial advisor will be able to provide more detailed figures that account for growth and the impact of future dollars on current expenses. Those presentations will be made at a future City Council meeting.

WHAT IF THE CITY DOES NOTHING (OR JUST REDEVELOPS DENNIS PARK AND 5050 RAINBOW)?

Multistudio has already provided several options for the City to consider, and I will not repeat those here. There may be other ways to mix-and-match elements of Multistudio’s proposals. However, I would again point to the survey results and our previous planning work as guides for how the overall community feels about the proper direction for the City, from both a planning and a financial perspective.

That being said, below I attempt to run a scenario that breaks down just the former Westwood View site, Dennis Park, and 5050 Rainbow. My assumption below is that the City would nevertheless still invest in developing our current park and 5050 Rainbow into a nicer park, and that the City would allow the former Westwood View site to be sold to another party. I also make the assumption that the former Westwood View site would be developed for single family homes, and that the single family lots would be approximately the same size and value as those newest lots on the west side of Booth Street, adjacent to the new Westwood View.

- Size of Westwood View Property: 4.97 acres
- Less new ROW (30 ft x 465 ft): 0.32 acres (13,950 sf)
- Remaining Westwood View Property: 4.65 acres
- Divided by Average New Lot Acreage: .20 acres
- Number of Possible New Lots: 23.25 new homes (23 new homes)

It is extremely unlikely that 23 new lots could actually be placed on the former Westwood View site, but I use these maximum figures (and high values) simply for purposes of showing a higher-end financial analysis (more affordable residential development would perhaps lower these figures).

- Number of New Homes: 23
- Average Appraised Value of Each: \$877,480 (average of 5 new homes on Booth)
- Average Assessed Value (11.5% Kansas rate): \$100,910.20
- Average Assessed Value times 23 homes: \$2,320,934.60
- Times Westwood Mill Levy (21.198 mills) \$49,199.17 in property taxes per year
- Total Property Taxes over 20 years: **\$983,983.43** (not including increases in value)

The above calculation does not include consideration that values will likely increase over 20 years. However, under this scenario, a cost/revenue model could look like the following:

City Revenues—Single Family Development		City Expenses—Redevelop Existing Green Space	
Estimated property taxes (20 years):	\$983,983.43	Balance due for 5050 Rainbow:	\$275,000.00
Estimated sales taxes (20 years):	\$0.00	Develop a 3.0-acre park (from Multistudio):	\$1,800,000.00
TOTAL:	\$983,983.43	TOTAL:	\$2,075,000.00
		Revenues Less Expenses:	(-\$1,091,016.57)

It would not appear that even a high-end residential development alone on the Westwood View site would be adequate to financially support redevelopment of our current park and 5050 Rainbow—there would be close to \$1.1 million that the City would have to find elsewhere.

Accordingly, the City would have to continue to develop other sources of revenue (or make cuts in other programs) in order to cover these estimated costs. Furthermore, this would not account for other needs (such as City Hall, Public Works, or other streets or project needs, the costs for which are also in the millions of dollars). To be sure, scenarios presented by Multistudio presented other options that could generate revenue (such as utilizing the existing City Hall site), but (1) there would be expenses associated with those proposals as well, and (2) those scenarios may not have as much public support, based on survey responses and the other planning work done by the City in the past.

Of course, another option would be to not incur park development costs at all and leave the existing park and 5050 Rainbow spaces as they are. However, that would not seem to be a direction which our community supports.

PROCESS FOR CONSIDERATION

The City will continue to keep residents updated, as it has been through our City Newsletter, weekly "Upcoming Meetings" emails, the Westwood Buzz, and other social media channels. The City has also published an FAQ and a brochure, which I have attached at the end of this message. Many more materials and information are available to you at <https://www.westwoodks.org/home-page/page/new-feature-park-development-consideration>.

I would remind our residents, again, that no decisions have yet been made as to this proposal. There are certainly good questions and valid concerns that have been raised regarding scope, size, height, traffic, and the like. Review of these matters will initially fall to the Planning Commission, who will then make a recommendation to the City Council. Again, we welcome input from our residents and encourage you to read the full reports summarized and linked to above. Below are some additional resources to help residents share their thoughts with the City, and also for you to keep up with the latest City news:

Governing Body Email Addresses: <https://www.westwoodks.org/mayor-and-city-council>

Planning Commission Contact: info@westwoodks.org

Westwood Email Notifications: <https://www.westwoodks.org/government-resources/page/stay-informed>

Westwood Facebook Page: <https://www.facebook.com/profile.php?id=100064359892945>
(find us under "City of Westwood Kansas-Government")

Thank you for taking the time to read this message. As I stated before in a previous Message from the Mayor, I understand that these are complex issues, and my goal here is to share with you the relationships among these issues and the balance of considerations which we, as a City, must undertake. I greatly appreciate all of your time and efforts in reviewing, considering, and working through these important matters for our City.

Sincerely,



David E. Waters, Mayor

We need your input!

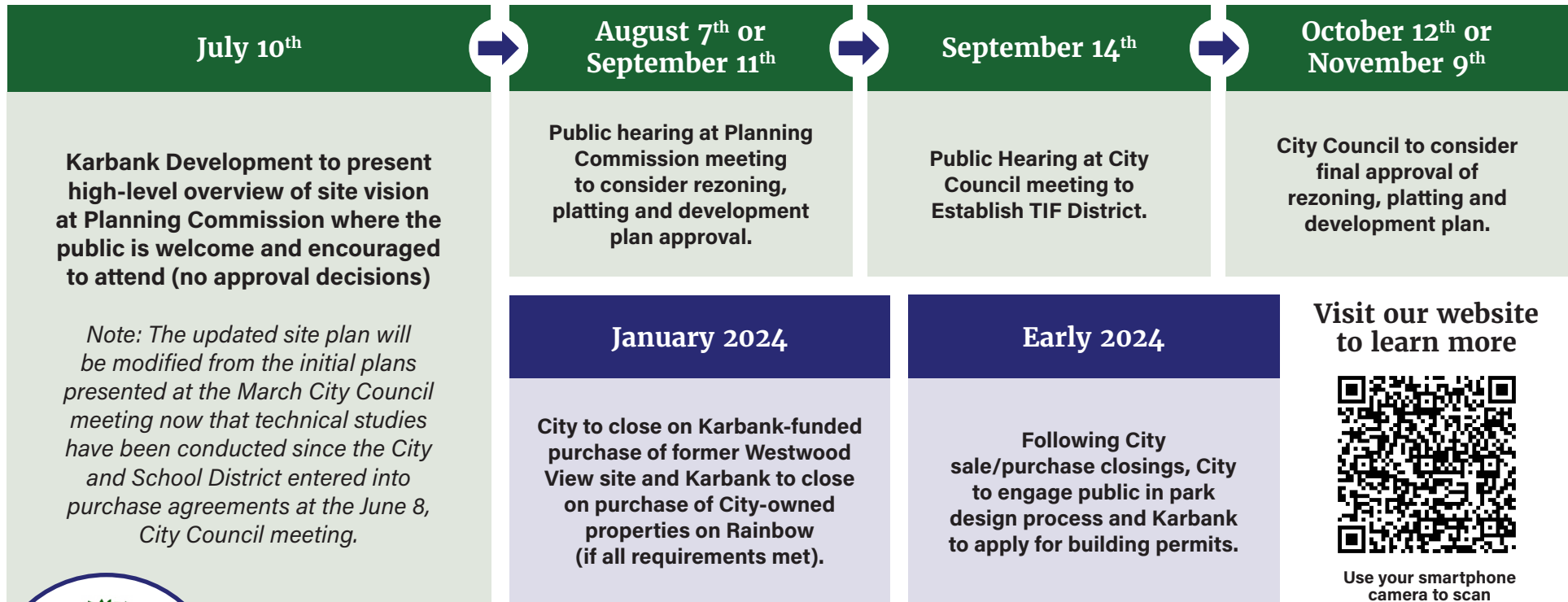
The Karbank and City Park proposal is an opportunity for the community to actualize the vision articulated in the 2022 Facilities Assessment and Feasibility Analysis, the 2021 Urban Land Institute Technical Assistance Panel, and the 2017 Comprehensive Plan. It's critical that the specifics of this development are shaped by public input and aligned to community vision and goals.

In the coming months, there are several key steps in the approval process where public input is essential. The public is welcome and encouraged to attend all Planning Commission and City Council meetings.

Karbank Development

New Park and Retail Proposed in the Heart of Westwood

On June 8th, the City Council approved both a purchase agreement with the Shawnee Mission School District for the old Westwood View Elementary School and a separate but related purchase agreement with Karbank for the City-owned property along Rainbow Blvd. between 50th & 51st St. The proposed development includes mixed use office and retail and a new park over double the size of Dennis Park. The period of due diligence began with the approval of these agreements and will continue until the purchase and sale close in January 2024.



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City of Westwood, Kansas Karbank Development Timeline

City of Westwood Kansas

How is the acquisition and demolition of the old Westwood View site being funded?

The City is selling its property along Rainbow Blvd. to Karbank for an amount sufficient to cover not only the cost of the purchase of the school building and all associated demolition costs, but also the City’s remaining debt on the property at 5050 Rainbow, approximately \$275,000.

How will the costs to develop the approximately 3.5 acre City Park be funded?

The City will issue GO TIF Bonds – General Obligation Tax Increment Financing Bonds – to fund the construction of the new park. The City will create a TIF District including both the Rainbow Blvd. properties and the Shawnee Mission School District properties. When new development happens in the TIF district (i.e. the Karbank development) and a property is improved, the value of that property increases. The taxes on the assessed value of the property at the time the TIF District is created (the “base”) continue to go to the applicable taxing entities (County, City, School District, Fire District, etc.). The additional property tax dollars between the base amount and the new property assessment is called the tax increment. Since this site is currently a school, no property tax is collected. This TIF will be a new revenue source for the City and will be used to make the debt payments on the GO Bonds.

Will this development add housing units to Westwood?

As currently proposed, the two to four building development will include office and limited retail spaces. However, the developers are open to including a few housing units if that is the desire of the community. To-date, Karbank has not included residential use in its tenant mix.

What’s a preliminary and final development plan?

A preliminary development plan (PDP) is a document that outlines the proposed development of an area of land. It serves as an initial framework for a development project and is submitted to the Planning Commission and City Council for review and approval.

The primary purpose of a preliminary development plan is to provide an overview of the proposed project, including its design, layout, infrastructure, and intended land use. It helps stakeholders, including government officials, planners, and community members, understand the project’s scope, impacts, and compliance with zoning regulations and development guidelines.

A final development plan (FDP) is a comprehensive document that provides specific and precise information about the design, layout, and implementation of a development project. It is typically prepared after the preliminary development plan (PDP) has been approved by the Planning Commission and City Council.

The purpose of a final development plan is to provide the necessary technical details and specifications for the construction and development of a project. It serves as a blueprint or guide for developers, architects, engineers, and contractors involved in the actual implementation of the project.

What’s replatting?

Replatting refers to the process of dividing or subdividing, or combining land into new lots or parcels. It typically involves changing the boundaries, configurations, or sizes of existing lots within a particular area of a city. Replatting is expected to occur during this development process to combine the Westwood View site and the City-owned property on Rainbow into two larger developable lots. The replat will combine six property parcels (two SMSD and four City) and break that back down into three property parcels (one City and two Karbank). The second Karbank parcel will be a piece of the eastern side of the current SMSD property (i.e. Karbank needs slightly more property for its development than just the City’s Rainbow parcels, so it needs to retain a small amount of the SMSD property). In the legal documents, this piece is called the Lot Line Adjustment.



What’s rezoning?

Rezoning is the process of changing how land is used in a city. The Karbank project proposes rezoning a residential area to a planned development area with an underlying commercial district. A planned development zoning designation is an area where certain rules and guidelines are in place to control what can be built or done there. This helps ensure that the development is organized, efficient, and meets specific goals or requirements set by the City Council and Planning Commission. An underlying commercial district designation means that certain business activities – as outlined in the planned development district guidelines – are allowed.

In the City of Westwood, rezoning can only occur through an application, notice of all property owners within 200 feet of the proposed rezone, a public hearing, recommendation of the Planning Commission, and final approval by the City Council.

Will the removal of mature trees be considered?

It’s crucial to strike a balance between development needs and preserving the valuable ecological and social benefits provided by mature trees. If the removal of mature trees is proposed as part of the development plans, the City will follow a comprehensive evaluation process that consists of a tree inventory and assessment, project requirements and constraints, and alternative designs and modifications.

How will parking, entering and exiting the development, and traffic impacts be evaluated?

Karbank will be required to submit a comprehensive traffic study as part of their final development plan. The traffic study will be conducted by a third-party transportation engineer and is a systematic evaluation of the potential impacts that their proposed project may have on the existing transportation infrastructure and traffic conditions in Westwood. The study will assess the project’s effects on traffic flow, safety, parking and congestion.

The findings will help City officials understand the potential effects on the transportation system and make informed decisions regarding project approvals and required infrastructure upgrades.

What’s a development agreement?

A development agreement is a legal contract between a developer and the City. It outlines the terms and conditions for a specific development project. This agreement typically includes details such as the scope of the project, the timeline for completion, the responsibilities of the developer, and any financial obligations or incentives involved. It helps establish a clear understanding between the developer and the government regarding the development process, regulations, and mutual expectations.

In this development, the agreement will also include the requirement of dedicated parkland and certain expectations around its use and features.

When will construction start and how will it affect residents?

If the plans are approved and the purchase agreements close, demolition could begin as early as Spring 2024. Karbank will be required to get permits for all demolition and building work. The City will work closely with the developer to ensure minimal interruption to daily life for Westwood residents. We will communicate proactively via our newsletter, website and social media channels.

When and how will the features of the new City Park be determined?

If the purchase agreement closes and the development is approved in early 2024, the City will begin the process of park design. The City will actively involve the public in the design of the park, prioritizing a collaborative effort that fosters a sense of ownership and pride among community members. Engaging residents throughout the process helps ensure that the park reflects their aspirations, promotes inclusivity, and meets the diverse recreational and social needs of the community.

Possible avenues for public engagement include public meetings and workshops, online surveys and focus groups/stakeholder interviews. The City will advertise all opportunities on its website, newsblasts and social media.



Karbank Development

Frequently Asked Questions 2.0



GENERAL OVERVIEW

What's happening now?

The City is negotiating with Karbank to finalize a [development agreement](#) that meets the needs of both parties. Among other things, the City is advocating for maximum green space, public access restrooms for park visitors, and ample parking for park visitors.

What's next in the process?

On August 7th, the Planning Commission discussed Karbank's application for rezoning – which is when the land use of a certain area is changed – and held a public hearing as part of that application. The next steps in the process are:

September 11th: The Planning Commission will meet to make a recommendation to Council to either approve the [rezoning application](#) and [preliminary development plan](#), deny the rezoning application, or grant a conditional approval, which means that the applicant must meet certain requirements in order to get approval.

September 14th: The City Council will discuss the creation of a [Tax Increment Financing \(TIF\) district](#), which is the structure that the City is proposing to allow money from the development to pay for the creation of the park. For more information on TIF, see our glossary of development terms [here](#).

October 2nd: The Planning Commission will meet to discuss the final development plan. For more information on the difference between a preliminary and final development plan, see our glossary of development terms [here](#).

October 12th: The City Council will meet to make the final decision on all components of the proposed development.

For more information on development terms, see our [glossary](#).

If needed, the City could shift these dates or hold special called meetings. Sign up for updates [here](#) to stay informed about the next steps of the process.

PUBLIC INPUT OPPORTUNITIES

PLANNING COMMISSION: SEPTEMBER 11TH, OCTOBER 2ND

CITY COUNCIL: SEPTEMBER 14TH, OCTOBER 12TH

Karbank Development

Frequently Asked Questions 2.0



SAFETY AND COMMUNITY COHESION

How are pedestrian safety concerns of nearby residents being addressed?

Karbank prioritizes low-traffic tenants in its developments. Karbank historically leases to a tenant mix of professional services, boutique firms and unique retail. A common characteristic of these types of tenants is low traffic and minimum employees.

Additionally, the City is requiring that the traffic circulation for the proposed development be designed with the safety of children and pedestrians in mind. The entrance to the development on 50th Street has no access to the parking garage (where most if not all of the office employees will park), so there will be minimal impact of development-related traffic to the children walking to and from school on 50th Street. It is likely that the only traffic entering and exiting the development on 50th Street will be for patrons of the pavilion tenants, limited retail accessible from the west side of the main mixed-use buildings, and the City Park, which will have 10 dedicated parking spaces in Karbank's parking lot just south of the pavilion(s).

Karbank has completed a traffic study, which will be included in the Planning Commission's September 11th meeting packet. The City will work with its traffic engineers to determine and mitigate any areas of concern. The Kansas Department of Transportation (KDOT) is also reviewing the traffic study for pedestrian safety.

The City Council and Police Department will continually monitor impacts to traffic and work to mitigate any concerns. Residents are encouraged, as always, to reach out to their Council Members or the Police Department with any and all safety concerns.

PUBLIC INPUT OPPORTUNITIES

PLANNING COMMISSION: SEPTEMBER 11TH, OCTOBER 2ND

CITY COUNCIL: SEPTEMBER 14TH, OCTOBER 12TH

Karbank Development

Frequently Asked Questions 2.0

Item A, Section III, Item



RAINBOW BOULEVARD

How will this proposed development address the overall safety and connectivity concerns along Rainbow Boulevard?

Specific safety improvements at 50th & Rainbow Blvd. were recommended by the resident-led 2021 Westwood Complete Streets Task Force, including a sidewalk on the north side of 50th Street. This development will accelerate and fund most if not all of the construction of this sidewalk.

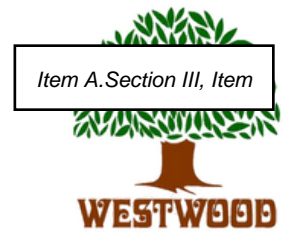


In a separate process, the Cities of Westwood, Westwood Hills, and Mission Woods are working with the Kansas Department of Transportation (KDOT) and several other partners through the Mid America Regional Council's (MARC) Planning Sustainable Places (PSP) program to determine safety issues on Rainbow Boulevard and develop recommendations to enhance pedestrian safety. Full details of this separate process are available via this link. The current Karbank proposal is being included in the PSP study; however, recommendations for design improvements to Rainbow Blvd. at 50th St. resulting from the PSP won't be included in Karbank's final proposal, due to differing timelines, stakeholders and requirements. For more information about the PSP process, visit the project site [here](#).

MORE QUESTIONS? EMAIL US AT INFO@WESTWOODKS.ORG

Karbank Development

Frequently Asked Questions 2.0



RAINBOW BOULEVARD

Why is the City considering a proposal for redevelopment of the corner of 50th & Rainbow Blvd. prior to working on redevelopment opportunities on 47th and Rainbow?

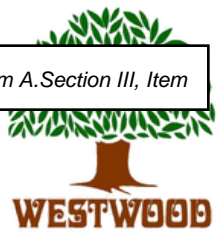
The Shawnee Mission School District has a certain timeline for selling the old Westwood View Elementary School property. In order to gain public benefits desired by the community – such as increased park space and unique retail – from both the proposed private investment and the ability to control the redevelopment on the southwest corner of 50th and Rainbow, the City must act now. This proposed private development is unique – the developers are asking for no tax incentives from the City and instead are diverting the tax dollars from the increased property value to the City for public infrastructure, particularly for a public park.

Commercial redevelopment opportunities at the southwest corner of 47th & Rainbow Blvd. would likely bring additional money to the City and help fund the City's strategic priorities of facility improvements; street, streetlight, and sidewalk improvements; and maintaining great City staff and services. City staff and elected officials will continue to pursue these redevelopment opportunities.

MORE QUESTIONS? EMAIL US AT INFO@WESTWOODKS.ORG

Karbank Development

Frequently Asked Questions 2.0



QUALITY OF LIFE

What impact to my property value could I expect if this project is built?

Property valuation is complex and ultimately beyond the direct control of the City. However, anecdotally, several examples of single-family residential homes next or close to commercial development exist in our immediate community. These include but are not limited to:

- The Fairway Shops and KU Cancer Center, which back up to houses on W. 51st Ter.
- Commercial and industrial development along 47th St., which backs up to houses on 47th Terrace from Mission Rd. to Belinder Ct.
- Woodside Club and State Line 47 office complex on 47th Pl., which backs up to houses on 47th Ter. between Rainbow & State Line

County property value data indicates that the owners of single-family residential homes on these streets close to commercial development have seen the same – if not higher – average increases over the past 10 years in value than homes in Westwood not next to commercial uses.

In fact, the homes on 47th Ter. between Rainbow and State Line (backing up to Woodside and the State Line 47 office complex) have averaged **19.5% higher property value growth** compared to the average of the rest of the City as a whole over the past 10 years. Great parks, good schools, walkable retail and restaurants, and good transportation options are all factors positively impacting property values. Homes immediately surrounding this project are likely to see positive impacts to their value from all of those contributing factors.

For more information about property valuation and appraisals in Johnson County, visit the County Appraiser's website linked [here](#).

FOR MORE INFORMATION,
VISIT THE CITY OF
WESTWOOD'S WEBSITE



Karbank Development

Frequently Asked Questions 2.0



QUALITY OF LIFE

What influence could the City have on the office or retail tenants of the development?

While specific leases have not yet been signed, Karbank is prioritizing similar tenants for this development as they have in their existing developments, including The 1900 Building at Shawnee Mission Parkway & State Line. It will likely be a curated mix of boutique professional services, restaurants, and retail. Karbank is a private company and its buildings and financial pro forma will determine the size and style of leasable areas and lease rates.

The City has limited authority on to whom private developers lease space and has no authority on lease rates between a private developer and a private business tenant.

Tenants will be required to comply with the existing City noise ordinance which requires quiet hours from 10 pm - 7 am.

How is the tree canopy being protected?

The City and the developer share a commitment to preserving mature trees. The City has recommended that a comprehensive tree inventory be conducted as part of the development process. This will catalog the species, estimated age, health and condition, and anticipated remaining life expectancy of any mature trees that might be impacted by the development. This information will allow decision makers to weigh the cost and benefits of removing mature trees. Further, a tree protection plan with the aim of protecting trees during construction activities will be recommended as a condition of any approval of the rezoning or development plan.

Additionally, the City is recommending that the developer offset any removal of mature trees by planting new trees (with an established minimum caliper/size) elsewhere at the development. City staff is exploring the possibility of creating a demonstration arboretum in the new feature park, however the specifics of this program will be determined with residents as a part of the park-planning process.

PUBLIC INPUT OPPORTUNITIES

PLANNING COMMISSION: SEPTEMBER 11TH, OCTOBER 2ND

CITY COUNCIL: SEPTEMBER 14TH, OCTOBER 12TH

Karbank Development

Frequently Asked Questions 2.0



COMPATIBILITY AND STRATEGIC ALIGNMENT

How is this proposed development compatible with the City's past land use planning efforts?

In the August 2023 Message from the Mayor (linked [here](#)), a full review of the City's past planning efforts relevant to the current Karbank proposal are outlined.

In summary, Rainbow Blvd. is considered a mixed-use corridor where commercial redevelopment activity with smartly-designed transitions into the adjacent residential areas is allowed. Specific to 5050 Rainbow Blvd., which is the site of the former Westwood Christian Church, the plan says that **"Joe D. Dennis Park can be expanded onto this site which would allow for additional park features and recreational opportunities in the community"** and that the site could be used for **"low-intensity development which could include some retail services to nearby residents and businesses along with residential uses."** See the entire comprehensive plan [here](#).

Why is the City pursuing ownership of the former Westwood View Elementary School and not just allowing it to be sold by SMSD?

In the August 2023 Message from the Mayor (linked [here](#)), an in-depth answer to this question is provided. For more information about right of first offer, see our development terms glossary [here](#).

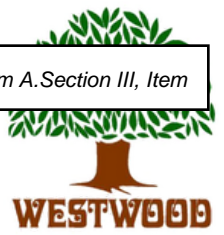
In summary, the Karbank proposal provides a way to purchase the School site, demolish the building, significantly increase park space, and create a modern, ADA-accessible park based on community input, all at no cost to Westwood residents. All of this would be paid for by Karbank – either through direct donation or by the value created and received by the City through the property taxes on the Karbank development. If the City does not exercise its right of first offer on the School site, it is unlikely – given the City's competing needs of improving City buildings and improving City streets and installing new streetlights and sidewalks – that the City would have the money to modernize the current Joe Dennis Park or add amenities to 5050 Rainbow Blvd. in the near future.

If the Karbank proposal is approved, the new City Park would be built within the next two to three years.

MORE QUESTIONS? EMAIL US AT INFO@WESTWOODKS.ORG

Karbank Development

Frequently Asked Questions 2.0



What alternatives to this proposal have been or could be considered?

Since the Shawnee Mission School District decided that the Westwood View Elementary would move to 50th and Belinder and vacate the old building in 2021, the City has worked diligently to re-evaluate strategic priorities to determine whether it should exercise its right of first offer with the Shawnee Mission School District on the former Westwood View Elementary School, which expires in February 2024. Rather than issuing a call for development proposals, the City conducted an Urban Land Institute Technical Assistance Panel in 2021 and a City Facilities Assessment and Feasibility Analysis in 2022 to determine whether the right of first offer should be exercised. The goal of these studies was to engage all community stakeholders and technical experts – not just private developers – in this important decision. More information about these studies can be found [here](#).

These activities (as well as the demolition of the Entercom radio station and the demolition of the Westwood Christian Church on 5050 Rainbow Blvd.) signaled to the development community that the City was considering redevelopment around 50th & Rainbow Blvd.

As a result, Karbank approached the City with its proposal, which was determined by the Mayor and City Council at the March 2023 City Council meeting to be aligned with the City's long-term land use planning and strategic direction. No other development proposals for this site have been presented to the City and, due to the provisions of the Funding and Exclusivity Agreement signed by the City in March 2023, no other proposals may be considered unless and until that Agreement terminates.

For more context on the evaluation of alternatives, please see the August 2023 Message from the Mayor, linked [here](#).

Could the plan be changed now or in the future to include residential uses?

Technically, yes. Karbank has said that they are not residential developers. They considered – but have not pursued – adding an additional level to their proposed mixed-use buildings to construct a limited number of luxury residential units. Townhomes, patio homes, and apartments are not included in Karbank's proposal. If ever there was a request to add or convert to residential use to any of the buildings, that request would have to come back to the Planning Commission for a public hearing in order to be allowed. This would be the case even if Karbank were to sell the property to another owner.

FOR MORE INFORMATION,
VISIT THE CITY OF
WESTWOOD'S WEBSITE



Karbank Development

Frequently Asked Questions 2.0



Why is the City considering allowing a project that develops new office in this current market?

Karbank is a tenured private developer that has a reputation for building developments and holding them for longer than the average developer. These factors, along with the current high occupancy / low vacancy rates at Karbank's other office developments in Johnson County, and the relatively small scale of the leasable area of office space in the proposed development, influenced the City's comfort with allowing this proposal to be heard by the Planning Commission and City Council during the current public process. Karbank is expected to address this question by providing its use case in its next appearance before the Planning Commission on September 11th.

What role does the City play in the developer's choice of architectural style and building materials?

When reviewing applications for rezoning, the Planning Commission and the Governing Body can refer to specific criteria as set out in the City's Zoning Ordinance. These criteria include, among others, architectural style and building materials, which influence how massive a structure feels (its massing).

The City's Zoning Ordinance does not define what type of building materials must be used in the Commercial Zoning District and there are not additional specific requirements on this site, the Planning Commission has the authority to influence these elements through its direction to the developer and recommendation to the City Council.

What does the City stand to gain from this proposed development?

Because Karbank is not requesting any tax incentives for this project, and instead are diverting the tax dollars from the increased property value to the City, the City stands to gain an enlarged public park, fully funded by private development. Without this private investment in the City, the funds for a new public park would need to come from the City. A project of this scale wouldn't be possible without finding new sources of revenue, like increased property tax or debt issuance.

In past planning efforts, residents have expressed a desire for more unique retail options within City limits, a desire that would be realized in this proposed development.

PUBLIC INPUT OPPORTUNITIES

PLANNING COMMISSION: SEPTEMBER 11TH, OCTOBER 2ND

CITY COUNCIL: SEPTEMBER 14TH, OCTOBER 12TH

Karbank Development

Frequently Asked Questions 2.0



Why is the park space to the west of the proposed development not showing any park features in the renderings being considered by Planning Commission?

The City can only begin the park planning process once the sale and purchase of the property related to this proposal closes. Karbank's original proposal showed 3.5 acres of public park space. After hearing feedback from the community about the desire to maximize park space in this development, Karbank independently negotiated with a resident to purchase property to add 0.5 acres to the park (noted in the graphic below by the black diagonal lines). The City was not involved in this transaction.

The City is committed to a robust public engagement process to determine park features, layout and amenities. The City understands that inclusive, equitable engagement is necessary to ensure that all voices are heard and all perspectives are considered. To stay updated on the park process, sign up for the City e-newsletter [here](#).



PUBLIC INPUT OPPORTUNITIES

PLANNING COMMISSION: SEPTEMBER 11TH, OCTOBER 2ND

CITY COUNCIL: SEPTEMBER 14TH, OCTOBER 12TH

Trenton Dandel
3020 W. 50th Ter
Westwood, KS 66205
tdandel@gmail.com
785-565-3353
September 7, 2023

City of Westwood
Attn: Planning Commission
4700 Rainbow Blvd.
Westwood, KS 66205

Subject: Support for Karbank's redevelopment of Joe Dennis Park

Dear Commission Members,

I hope this letter finds you well. I am writing as a proud resident of Westwood to express my enthusiastic support for the proposed Joe Dennis Park and the redevelopment of the old Westwood View Elementary School site. Having moved to Westwood in July of 2020, my family and I were immediately drawn to the town's unique walkability, vibrant atmosphere, and exceptional amenities. The remarkable character of our town is a testament to the hard work and dedication of the residents, staff, and elected officials who have contributed to its evolution over the years. I am grateful for your collective efforts that have shaped Westwood into the welcoming community we now call home.

Change and progress, as I understand, come with their fair share of challenges, including criticism and resistance. I commend your dedication to steering our town towards a brighter future despite these hurdles. It is within this context that I find myself excited to share my thoughts and aspirations for the evolution of Joe Dennis Park and the former Westwood View Elementary School site.

I wish to extend my sincere gratitude for the remarkable transparency and open communication that has characterized the planning process thus far. Your timely and detailed responses to my inquiries, coupled with proactive updates through emails, mailings, and postings, exemplify the engagement between our government and its citizens. This level of transparency fosters trust and unity within our community.

Addressing the concerns of those who oppose the project due to potential impacts on property values and quality of life, I propose a different perspective. Access to amenities and retail establishments enrich the desirability of residential areas, as my own family's decision to reside here exemplifies. The blend of convenience and recreational opportunities inherent in such projects contributes positively to our daily lives. I also would like to note that in the September 5th article covering this conversation, I did not feel that the new park, which is a part of the project, received adequate coverage. The loss of Dennis Park as it currently exists was covered in detail, but the fact that a new, larger, more appropriately sized park for

the surrounding community seemed to be glossed over. This new park is one of the most appealing aspects of the proposed development for my family and me.

Furthermore, I recognize the project's potential to enhance the city's financial stability through increased tax revenue, a prospect I wholeheartedly support. As a parent to two young children, aged 3 and 6, I am excited about the prospect of new amenities tailored to families. This offers us an opportunity to actively engage in the planning and programming of a park that will be enjoyed for generations.

My vision for Westwood encompasses vibrancy and variety. I envision a mix of retail establishments, including restaurants and recreational venues that seamlessly combine sports with indoor and outdoor dining spaces for people of all ages. Moreover, I hope to see provisions for hosting community events, such as KC Symphony in the Park, public book readings, and neighborhood-friendly music shows.

Density is an essential aspect of modern urban planning, and I am in favor of this aspect within the proposed project. However, I also advocate for the inclusion of a residential component. Residents living within the project would foster a sense of community and continuity that transient users may not provide. I understand that the developer's expertise may not lie in residential projects, and I suggest exploring potential partnerships with those who can contribute in this regard.

In light of the unique dynamic in our small-city setting, particularly with the consideration of planning and rezoning notifications, I kindly request that the perspective of residency be taken into account. The impact of this project on tax-paying citizens differs from its impact on non-citizens, and I encourage this differentiation in the decision-making process.

As a devoted resident invested in the long-term prosperity of Westwood, I offer my heartfelt support for the Joe Dennis Park and Westwood View Elementary School project. I am excited about the opportunity to contribute to the growth and enhancement of our community. Thank you for your time, dedication, and consideration of my thoughts.

Sincerely,

Trenton Dansel

Handout for Planning Commission, September 11, 2023
City of Westwood Master Plan; Comprehensive Land Use Plan, June 2017

On page 4-15 titled **Westwood View Elementary**, we find this:

“The Possibilities

“Several opportunities have been identified for this site. Combined with the adjacent Joe D. Dennis Park and 5050 Rainbow Property, this area has a lot of potential for redevelopment, including additional single-family homes and a larger park space, among other options.”

“Potential uses for this site include:

- New or replacement elementary school facility
- Park and open green space
- Civic uses
- Residential property for single-family homes.

And on Page 4-17 titled **5050 Rainbow**, we find this:

“The Possibilities

“Possibilities for this site have maximum impact when combined with potential opportunities for the Westwood View Elementary school site, discussed previously. The surrounding land uses (park and residential) would conflict with most commercial development, but there is an opportunity for mixed-use and/or civic use development along Rainbow Boulevard. **The scale of the buildings should be low-scale, generally no more than two stories, (“Low-Scale”).**”

(Pictured below this comment is a photo of the small businesses in Westwood Hills at the corner of 50th and State Line Road as one example.)

On Page 4-10 titled **Commercial Development Policies**, we find this prohibition:

“2.5. Prohibit isolated commercial rezoning in established residential developments.

Moving on from the Comprehensive Plan to the Urban Land Institute 2021 Report:
 Redevelopment Opportunities for the City of Westwood

On page 20 we find this: **Re-Imagining Joe D. Dennis Park**

“Commercial uses at the site seem to make sense, given its presence along the commercial corridor. Yet, this section of Rainbow Boulevard on the east, and the blocks immediately north and south are primarily residential, **leaving any new commercial uses on the park site isolated from other commercial uses.**”

This Report reaffirms the prohibition from the Master Plan—no isolated commercial rezoning!

(Submitted by Karen I. Johnson, 4950 Adams Street, Westwood, Kansas)

From: [David Waters](#)
To: [Leslie Herring](#)
Subject: Fwd: Plan for Dennis Park and the old WWV
Date: Thursday, August 31, 2023 9:28:24 PM

-David E. Waters

Begin forwarded message:

From: Stephanie Weaver <essayweaver@gmail.com>
Date: August 31, 2023 at 9:09:28 PM CDT
To: jeff.harris@westwoodks.org, andrew.buckman@westwoodks.org,
jason.hannaman@westwoodks.org, laura.steele@westwoodks.org,
david.waters@westwoodks.org
Subject: Plan for Dennis Park and the old WWV

Good evening,

I wanted to write and express my support for the new development plans, as well as the concerns I have.

I grew up in Westwood, and am in the process of buying my mom's house here. We moved here a year ago, and my son attends Westwood View as a 1st grader.

I love this neighborhood, and one of the things I love so much about it is how walkable it is! And walkable from the perspective of going places. I live within walking distance of the elementary school, two grocery stores, several restaurants in Westwood, Fairway and KCK, and other shopping and services, and within biking distance of even more.

I love the idea of having more options within walking distance, especially restaurants. I also love the idea of a bigger park, especially if it includes restrooms. I considered having my son's birthday party at the mini park, but ultimately decided against it, due to the lack of restrooms. Considering that there used to be a radio station at the new WWV site and a church in that neighborhood, I do not think additional traffic is a major concern.

In fact, I would support apartments, townhouses or duplexes (the "missing middle" housing) in that area. It seems to me that the apartments by Woodside have only been a good thing.

As far as traffic goes, an excellent enhancement would be to include pedestrian bumpouts at the intersections on Rainbow, which decrease the distance for walkers and cyclists and slow cars naturally. Leaving grassy strips and trees will also be helpful at slowing traffic. I would love to see these types of designs incorporated throughout our very pedestrian-friendly neighborhoods.

My main concern with the development is simply making sure that we can support additional businesses. The fairway shops have at least two empty storefronts, and the shops in KCK along County Line Road have two empty restaurants.

My main concern with the park is keeping all of the lovely old trees. It would be an absolute shame to terraform the park and remove beautiful and historic trees. I also wish that we could return the dedication plaques for the trees planted in memorial, such as the tree planted for Ms. Sheradon, a teacher who died the year before I reached her grade. My class was there for the dedication of the tree.

Thank you for all of your work on this project.

-Stephanie Weaver

Sent from my iPhone

Leslie Herring

To: Mindy McEwen
Subject: RE: Karbank project proposal concerns

Hi, Leslie.

I have spoken with Sarah Page and Holly Wimer about my concerns regarding the Karbank project and Holly suggested that I email you so you could pass my concerns along to the members of the Westwood city council and planning commission.

We have lived at 2208 West 50th Terrace in Westwood Hills since 2006 and, as you know, have a child who attends Westwood View. My biggest concerns about this project are the following:

1. Traffic/pedestrian safety - we walk to/from school every day. There is a lot of traffic on Rainbow, particularly in the mornings, and I have seen so many cars run the red light at 50th Street and Rainbow (I was communicating with the chief of police about this last year as well). I am aware that traffic is currently increased due to Rushton using the old Westwood View building; however, the traffic issues on Rainbow were issues prior to Rushton moving in. Putting commercial buildings at/near this corner is clearly going to cause additional traffic, particularly during the 8 am walk to/from school, and I am very concerned about pedestrian safety on Rainbow as well as north and south of the site.

Side note - regardless of what happens with this project, would it be possible to add a sidewalk to the north side of 50th Street between Rainbow and Adams (and possibly add/move the crosswalk there)?

2. General safety - I walk past this area on a regular basis, often times alone. Walking past buildings with underground parking where I cannot see who may be around is concerning to me as a female walking alone, especially if it's getting dark.

3. Lights and noise - living two houses off of Rainbow, we are concerned about lights shining into our home/onto our property and noise from any restaurant/retail establishment that is open early/late as well as noise from hvac systems, generators, and any other mechanical equipment. Sarah mentioned that there are ordinances for these items, but they are still a concern we wanted to share.

4. Property values - have there been any studies done to understand the impact on property values when you turn a park into commercial real estate?

5. Visual appeal - the current proposal, in my opinion, does not fit with the neighborhood.

6. Occupancy - with so many commercial vacancies in the area, and across the country, how will they fill the buildings and keep them filled?

I would be thrilled to have a new park where the old school building sits and feel that something new getting built on Rainbow is likely going to be better than looking at the old, dilapidated church that used to be there; however, I am concerned that the proposed office buildings will bring more issues than value.

Thank you for taking the time to read my message. I am happy to speak with anyone who would like to discuss this in more detail.

From: [Jamie N Johnson](#)
To: info@westwoodks.org
Subject: Rainbow blvd development
Date: Wednesday, August 16, 2023 11:11:29 AM

In regards to public input for the development project on Rainbow Blvd, I am in support of the project overall. The aesthetics of the external facade and landscaping is a priority for maintaining an attractive addition versus a commercial eyesore.

The city will benefit through the builder absorbing the costs of acquiring and demolishing the former elementary school and dedicating land for development of the park and green space as well as dedicating the TIF proceeds to covering the costs of the new park and future city developments.

Jamie Johnson
2505 W 50th Place

AUG 10 2023

Westwood City Council and citizens:

My name is Mike Coffman and I live at 2217 W 50th St, Westwood Hills, KS. My wife and I moved from our home in Westwood to our present house in the early 80's. We have witnessed the changes in the area for the last 40 plus years. Traffic at the corner of 50th and Rainbow has continuously increased but has substantially increased in the last 5-7 years. I became aware of the office park that is planned for that corner and feel that would be a terribly wrong placement of such construction in the short and long term of the community. I can't understand how commercial property, bringing more daily traffic, can successfully improve a corner that has been residential since the beginning of these communities and has seen substantial increase in traffic.

Issues:

- Much more traffic around the children at the new school
- More traffic at the 50th and Rainbow corner, once again increasing traffic right in front of the school crossing. Having lived at that corner since 1982, we've seen how dangerous that corner can be. We've witnessed so many issues on that corner Westwood Hills petitioned your public works to increase the stoplight delay to 5 seconds delay due to drivers running red lights at that corner. Just speak to the crossing guard to verify. Moving the crossing North across Rainbow will not alter the safety.
- The 50th and Rainbow corner is the only guarded crossing for children going from the west side of Rainbow to the East side of Rainbow. The intersection has caused problems due to the hill peak and the swerve at the corner on 50th.
- 50th is one of the few streets running from the Ward Parkway to Roeland Park and therefore sees increased congestion during "rush hours".
- The proposed Karbank architectural plan in no way fits the style and feel of the area. The proposed development does not fit the architectural style of any of the surrounding residential houses
- The project is over development for a residential area.s.
- There is a lack of "breathing room" or green space/greenery between the Rainbow side and the development.

The Proposed Plan does not fit with the goals and desires of the Westwood 2015 or 2017 Westwood Master Plans.

2017 Page 51 Section 2.1 Use architecture that is in scale and harmony with nearby structures.

2017 Page 51 Section 2.5. Prohibit isolated commercial rezoning in established residential developments.

The 2017 Plan clearly shows that no more than 2 story commercial development should be placed outside of the 47th and Rainbow area and that the 2 story development should maintain neighborhood character. Whereas the so-called Gateway Area at 47th and Rainbow can have a "scale and pattern" of greater than 2 stories.

The 2021 Final TAP - Recommended moving city hall to allow for Higher Rise Commercial expansion including apartments and townhomes so that there would be more attainable housing choices.

I could foresee Karbank development on the southeast corner of Rainbow and 47th where commercial property is already established on the other 3 sides as per the 2021 TAP recommendations. Maybe even a maximal 2 story commercial development at the site of the old Westwood View school thus in keeping with the neighborhood.

Finally, it's true that we are concerned about our property value but it's also true that for over 40 years we've been able to look out on the fountain, the flag, children at play, and the school that our children attended. To trade the current pleasant view for a two/three story steel and glass development and parking lot seems adherent. Would you, after 40 years of living in Westwood, want a 2/3 story glass office park built across the street from your house. We knew when we bought our house that living next to Rainbow there would be traffic, but we had a beautiful park just across the street. I'm embarrassed for Westwood that they would scar that corner with the plan that Karbank has proposed and is opposed by the citizens of the 3 cities affected.

Please keep residential areas residential and commercial areas commercial and reconsider the proposal for the long term health of our beautiful cities.

From: [Rachel DeSchepper](mailto:Rachel.DeSchepper@westwoodks.org)
To: leslie.herring@westwoodks.org
Cc: info@westwoodks.org
Subject: Public Comment on Karbank proposal
Date: Wednesday, August 9, 2023 11:47:54 AM

Hi Leslie -- I hope that I'm not too late to submit public comment regarding the Karbank proposal that was presented earlier this week to the Planning Commission. I attended the meeting via Zoom. I live at 4919 Adams St.

I want to first make clear that I'm not anti-development in Westwood (despite it making me so sad every time I now drive by our sweet Joe D. Dennis park and imagine it gone). I understand that we need the revenue, and I am very much on board with creating a walkable neighborhood that is in character with our area. I love the idea of being able to walk to a coffee shop in the morning, mingle with my neighbors, have a close place to grab a bite to eat, and also have a more modern park.

That said, the rezoning proposal and design by Karbank that we saw on Monday could not be further from the character of Westwood or fulfilling what I imagine for our city. I'm not going to say much that wasn't already said during the meeting, but I want to also voice my disapproval on the following points:

- 1) The office space concerns me for the same reasons others mentioned. What tenants are still looking for office space given that remote work is not going away? Is there something in the works with the health system? Why offices?
- 2) The design of the buildings is, to be transparent, abhorrent. The colors are clownish, and the buildings look like rail cars. Can we not have a design that's more in line with the character of our neighborhood? If the proposal looked more like the Fairway shops, the Brookside area, even Prairie Village I think you would see a lot more support from our neighbors.
- 3) The support and infrastructure for cars and parking is out of character for a walkable neighborhood.
- 4) The gentleman from Karbank stressed that the retail space would be filled by boutique tenants, which frankly, felt elite and exclusive. That is not in line with our neighborhood. He then immediately flip flopped when one of our commissioners pushed back on that, and pointed out how non inclusive that would be. We do not need boutique tenants. I think I speak for my neighbors when I say that we want tenants that are friendly and accessible for all of us.
- 5) Another neighbor made the point that it would make much more sense to move City Hall to this parcel, and develop the current area where City Hall is. That seems so much more logical. For one, it is directly across the street from the Woodside development, retail, and restaurants; and two, it would preserve the area where Joe D. Dennis park and the school are for non-retail/parking/traffic. We would be able to reserve much more of that space for leisure, *not* capital.

Final question: Is the commission/council open to additional proposals other than what Karbank presented? I think we *absolutely* need to see better ideas.

Thank you for giving us the opportunity to submit these comments.

Rachel DeSchepper

--

Rachel DeSchepper

racheldeschepper.com

From: [Rod Bell](#)
To: [Leslie Herring](#)
Subject: Karbank proposal
Date: Tuesday, August 8, 2023 4:15:55 PM

I attended the meeting yesterday afternoon.

I found the Karbank proposal compelling. This is moving forward for Westwood in a very competitive world where revenue for communities is extremely tight. Westwood is a very attractive community for young families with the new Westwood View elementary school. It may have an impact

on the traffic on Rainbow, mo I believe it will be negligible. The vast majority of residents will won't feel a thing. As we all know property values in Westwood are on the rise in a big way. Again primarily because of the new school. We need to make sure these values are sustainable with the infrastructure needed in the neighborhood.

For businesses residents could walk to work. Shops and restaurants are also a big plus. Just like in Prairie Village we are seeing renovations and new building of homes. This is also positive for the community.

We don't live in a static world where nothing changes. This will effective homeowners for generations to come.

I do certainly hope the Karbank proposal is adopted

Thank you

Rod Bell

2820 w 50th st

RECEIVED

Item A, Section III, Item

AUG 08 2023

City of Westwood KS

Ellen Marsee
4957 Adams
Westwood, KS 66205

~~Dear Neighbor,~~ *City of Westwood*

My name is Ellen Marsee. I have been a resident of Westwood Kansas for almost 60 years. I love our city and the neighborly small town feel an engaged citizenry has maintained. I am writing with my concerns about a proposed development project. I hope you will join me and attend the public meeting at city hall on August 7th at 7 p.m.

Admittedly, I am steadfastly opposed to the project. I live in the house on the corner of 50th and Adams directly across from the park and tennis courts. How lucky my late husband and I have been to raise our family in such a safe and connected community. We have always enjoyed a beautiful view of Westwood's only park and school. My objection is foremost due to our darling park being replaced by office and retail space within 200 feet of my property.

However, my concerns are not only self serving. I am equally concerned that our city might move forward with a development project that is not in the best interest of our city, either in the short or long term, and in fact, conflicts with citizens' desires as expressed in recent surveys and recommendations given in the 2017 City of Westwood Master Plan (this can be found on the city's website).

My major concerns are as follows:

In 2017 the city published its Master Plan and paid \$70,000 for the research and creation of this plan. The Karbank development proposal conflicts with this Master plan in many ways. Some of those are:

- Includes development which is not focused only on the corridors of 47th Street and Rainbow Blvd. as recommended (pg. 27). In fact the proposed development encroaches and disrupts purely residential areas on both 50th and 51st Streets.
- Includes commercial development of more than two-stories with great negative impact on adjacent residential areas (pg. 47).
- Ignores the survey responses and priorities citizens volunteered throughout the creation of the Master Plan (pg. 17). Here are a few excerpts: Survey respondents desire: • Livable neighborhoods • Schools • Appearance of the community • Public safety • Small-town character. Survey respondents said they prefer to see residential (single-family) land uses in the community (64 percent of respondents), followed by mixed use businesses that include residential and commercial (38 percent of respondents). Survey respondents expressed concern about: • Potential conflict between businesses and residential areas • Development of apartment and senior living units. Most important issues to address in the next ten years: • Aging infrastructure • Balancing what people want versus spending/capacity • Aging housing stock In addition, survey respondents recommended focusing on preserving and enhancing the existing

residential areas rather than building new. A majority of respondents did not want to grow the community, but rather protect and enhance the existing community character.

The proposed development will bring major traffic into residential areas and most importantly into a proposed new park and in close proximity to our new school. Citizens often cite the safety and walkability of Westwood, and specifically, the ability of children and families to walk to school as a priority. Instead of a plan to divert traffic away from our only school and park, this design will draw it in.

Likely you have followed news of communities strained by the challenges to fill office and retail spaces in a post covid era. Much has been reported on this national trend and the negative economic impacts it has had. Locally we have seen this in our own Country Club Plaza with reporting on the historic number of vacancies. Our neighboring city of Fairway currently has vacancies in their small commercial area including a restaurant space that has remained vacant since 2019. I shudder to think of the disaster of the Mission Gateway development and the impact such an experience could have on Westwood.

Finally, with only one plan for citizens and the Westwood City Council to consider, along with an exclusivity agreement with Karbank (the timeline can be found on the city's site), interesting and innovative proposals that may exist will not even be considered. How do we hold developers accountable to the contributions of citizens in the City Master Plan when we have eliminated any competition? Instead, we should be fostering an arena in which the best plan for Westwood is able to be drawn out through a commitment to citizens' priorities and desires for the future.

I hope you find this helpful as you prepare to consider your own position on the development. Wishing you a safe and happy rest of the summer.

Your neighbor,
Ellen

*I submit this letter as my petition
of protest. August 8, 2013*

Ent	Name	Acct No	Invoice	Date	P.O. Num	Reference	Amount	
090000	KARBANK HOLDING	1350-001	REZONEAPP2	6/27/2023		School Dist Parcels	500.00	Item A. Section III, Item 0

Payor: KARBANK HOLDINGS LLC	Date: 6/28/2023	Check No.: 108986	Check Amount: 500.00
Payee: CITY OF WESTWOOD			

Retain this statement for your records



FEE SCHEDULE		Office Use Only Fee Paid:
<u> </u> RESIDENTIAL	\$ 250.00	\$ <u>500-</u> Date Paid:
<u> x </u> OFFICE/COMMERCIAL/INDUSTRIAL	\$ 500.00	<u>6/29/23</u> Case No.:
		<u>22-2023-02</u> Meeting Date:

Name of Project: 50th and Rainbow Development

Requested Zoning: PD - Planned Development District

Existing Zoning: R-1 Single Family Residential

PROPERTY DESCRIPTION

Location/Address: 2511 W 50th Street et. al.

Approximate Size of Property (in square feet or acres): Approximately 4.8 Acres

Note: If property is not in a recorded subdivision, an electronic legal description of the property must be submitted with this application in Word format.

Property Tax ID Number: RP30000001 0012A and RP27000000 0008

Present Use of Property: Education

Proposed Use of Property: Mixed Use - Office / Retail / Residential

List improvements on the property: Existing - 26,257 square foot school building.

Proposed - Up to 140,000 square feet of buildings, with surface lots and parking garages as required.

APPLICANT INFORMATION Note: If Agent/Contact Person is anyone other than the fee simple owner, signature of the fee simple owner designating the Agent/Contact Person as the authorized agent for all matters concerning this request, must submit the attached affidavit with the application.

Owner of Record:

Agent/Contact Person:

Name: Unified School District #512 Name: Karbank Holdings LLC (Adam Feldman, Authorized Agent)

Address: 8200 W 71st Street Address: 2000 Shawnee Mission Parkway, Suite 400

City/State Shawnee Mission, Kansas Zip: 66204 City/State Mission Woods, Kansas Zip: 66205

Phone: 913-993-6462 Phone: 816-221-4488

Email: joegilhaus@smsd.org Email: af@karbank.com



I, Adam Feldman, (Contact Person's Name; Please Print), hereby certify the attached and completed application contains the information as specified below in accordance with the Unified Development Code. I understand the submission of incomplete or inaccurate information may result in a delay in processing and action on this application and may be subject to other penalties provided by law.

Adam Feldman (handwritten signature)

6/29/23 (handwritten date)

Signature of Contact Person

Date

- 1. Developer participates in a pre-application conference with City staff to present a conceptual plan for the development if development is more than three acres.
2. The Secretary of the Planning Commission checks rezoning applications for completeness and receives the filing fee, which shall be made by the deadline stipulated on the prepared schedule prior to the associated Planning Commission meeting date.
3. For rezoning applications the City shall mail Notice of Public Hearing to all surrounding property owners within 200' (1000' if property is adjacent to extraterritorial property) of all property boundaries. Mailings shall be made at least 20 days prior to the hearing date and shall include the time and place of the hearing, a general description of the proposal, a general street location of the property subject to the proposed change, and a statement explaining that the public may be heard at the public hearing. The notice shall also contain a statement explaining that property owners required to be notified by this section shall have the opportunity to submit a protest petition, in conformance with the Westwood Zoning Ordinance, to be filed with the office of the City Clerk within 14 days after the conclusion of the public hearing. If a valid petition is filed, then an affirmative vote of three-fourths (3/4) of the City Council members must occur to adopt the proposed rezoning.
4. The applicant is responsible for posting signs, furnished by the City, 20 days prior to the Planning Commission public hearing, on the property so that they are visible from the public street. The signs must be placed so as to face each of the streets abutting thereto within five feet of the street right-of-way line in a central position on the lot, tract, or parcel of land so that the sign is free of any visual obstructions surrounding the sign. The applicant shall file an affidavit with the City Clerk at the time of the public hearing verifying that the sign has been maintained and posted as required by this title and applicable resolutions. Failure to submit the affidavit prior to the hearing may result in a continuance of the hearing. The sign may be removed at the conclusion of the public hearing and must be removed at the end of all proceedings on the application or upon withdrawal of the application.
5. The City shall be responsible for a public advertisement in the official newspaper of the City of Westwood.
6. By the deadline stipulated on the prepared schedule, an applicant must submit for approval four (4) collated, bound and rolled copies of a concept or preliminary site plan to the Secretary of the Planning Commission. As part of the rezoning application, a detailed description of the proposed project and how the request complies with applicable zoning and comprehensive planning criteria must be submitted. The narrative should also address the criteria within Section 1.6.17 of the Westwood Zoning Ordinance.
7. After reviewing the plan, City Staff will prepare comments which will be e-mailed to the contact person and must be subsequently addressed and resubmitted by the date stipulated.
8. The Planning Commission conducts public hearings on the rezoning and plan applications and forwards a recommendation to the City Council.
9. The City Council approves the application, in whole or in part, with or without modifications and conditions, or denies the application. Upon review of rezoning applications the City Council also has the option to remand the application back to the Planning Commission. In the event of a denial by the Governing Body, the application may not be resubmitted for one year.
10. Full application and requirements and procedures are identified in Section 1.6 of the Westwood Zoning Ordinance. Applicants bear the onus of understanding all requirements and procedures and asking City staff if and when clarification is needed.



AFFIDAVIT

STATE OF KANSAS)
) §
COUNTY OF JOHNSON)

I, Unified School District #512, being duly sworn upon our oath and being of
Name of Property Owner

sound mind and legal age, depose and state that:

1. Unified School District #512 is the owner of property located at approximately
Name of Property Owner
2511 W 50th Street (RP30000001 0012A and RP27000000 0008), in the City of Westwood, Kansas, Johnson County.
Address or Vicinity of Property

2. I have the legal authority to bind Karbank Holdings LLC, as the Applicant, the
Authorized Person
authority to authorize the filing of land use applications on the Property.

3. I have authorized Karbank Holdings LLC to file an application for
Authorized Person
a Rezoning on the property described above, and
Rezoning/Special Use Permit
do affirm that I have the authority necessary to grant such.

Unified School District #512

Signature of Property Owner

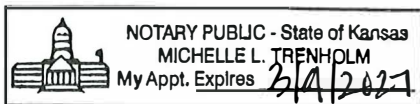
Name: Unified School District #512

Print name of Property Owner

Title: Dr. Joe Gilhaus, Authorized Agent

Subscribed and sworn to before me this 26 day of June, 2023.

My Commission Expires:



Michelle L. Trenholm
Notary Public

EXHIBIT "A"
LEGAL DESCRIPTION OF THE LAND

Legal Description to the Developed by Survey

TRACT 1:

THE WEST TWO HUNDRED FIFTY-EIGHT AND ONE-TENTH (258.1) FEET OF THE SOUTH HALF (1/2) OF LOT EIGHT (8), HOLMESLAND, A SUBDIVISION IN THE CITY OF WESTWOOD, IN JOHNSON COUNTY, KANSAS, ACCORDING TO THE RECORDED PLAT THEREOF.

AND ALSO:

THE NORTH HALF OF LOT 8, HOLMESLAND, A SUBDIVISION IN THE CITY OF WESTWOOD, JOHNSON COUNTY, KANSAS EXCEPT THE EAST 286.58 FEET THEREOF, AND THE WEST 258.1 THEREOF.

AND ALSO:

ALL THAT PART OF THE EAST 286.58 FEET OF THE NORTH 1/2 OF LOT 8, HOLMESLAND, A SUBDIVISION IN THE CITY OF WESTWOOD, JOHNSON COUNTY, KANSAS, MORE PARTICULARLY DESCRIBED AS FOLLOWS: BEGINNING AT A POINT ON THE NORTH LINE AND 286.58 FEET WEST OF THE NORTHEAST CORNER OF SAID LOT 8; THENCE SOUTH, ALONG A LINE 286.58 FEET WEST OF AND PARALLEL TO THE EAST LINE OF SAID LOT 8, A DISTANCE OF 165.39 FEET, TO THE SOUTH LINE OF THE N 1/2 OF SAID LOT 8; THENCE EAST, ALONG THE SOUTH LINE OF THE N 1/2 OF SAID LOT 8, A DISTANCE OF 128 FEET, TO THE NORTHEAST CORNER OF LOT 12, BLOCK 1, KLASSEN PLACE, A SUBDIVISION IN THE CITY OF WESTWOOD, JOHNSON COUNTY, KANSAS; THENCE NORTHEASTERLY, TO A POINT ON THE NORTH LINE AND 271.28 FEET WEST OF THE NORTHEAST CORNER OF SAID LOT 8; THENCE WEST, ALONG THE NORTH LINE OF SAID LOT 8, A DISTANCE OF 15.30 FEET, TO THE POINT OF BEGINNING.

EXCEPT ANY PART USED OR DEDICATED FOR STREETS, ROADS AND PUBLIC RIGHTS

OF WAY. TRACT 2:

ALL OF LOTS 4 THROUGH 14, BOTH INCLUSIVE, BLOCK 1, SWATZELL ADDITION, A SUBDIVISION IN THE CITY OF WESTWOOD, JOHNSON COUNTY, KANSAS.

TRACT 3:

THE EAST 112.3 FEET OF THE NORTH HALF OF LOT 9, HOLMESLAND, A SUBDIVISION IN THE CITY OF WESTWOOD, JOHNSON COUNTY, KANSAS.

Corrected
2013

TRACT 4:

ALL OF LOT 12, BLOCK 1, KLASSEN PLACE, A SUBDIVISION IN THE CITY OF WESTWOOD, JOHNSON COUNTY, KANSAS, EXCEPT THE FOLLOWING TRACT OF LAND:

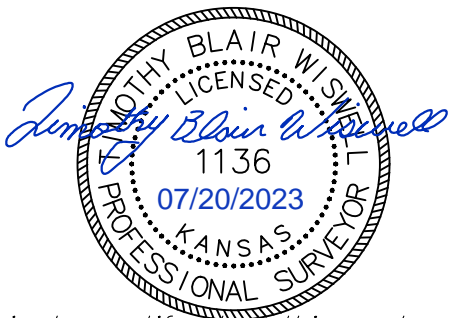
ALL THAT PART OF LOT 12, BLOCK 1, KLASSEN PLACE, A SUBDIVISION OF LAND IN THE CITY OF WESTWOOD, JOHNSON COUNTY, KANSAS, DESCRIBED AS FOLLOWS: BEGINNING AT THE NORTHEAST CORNER OF SAID LOT 12; THENCE SOUTHEASTERLY, ALONG THE EASTERLY LINE OF SAID LOT 12, TO THE SOUTHEAST CORNER THEREOF; THENCE SOUTHWESTERLY, ALONG THE SOUTHERLY LINE OF SAID LOT 12, A DISTANCE OF 16.87 FEET THENCE NORTHWESTERLY, TO THE POINT OF BEGINNING.

Corrected 7/20/23


PROPOSED LOT 1 DESCRIPTION:

A tract of land being all of Lots 13 and 14, and part of Lot 12, Block 1, KLASSEN PLACE, a subdivision recorded in Bk. 12, Pg. 50; all of Lots 1, 2, and 3, Block 1, SWATZELL ADDITION, a subdivision recorded in Bk. 7, Pg. 35; part of Lot 8, HOLMESLAND, a subdivision recorded in Bk. 3, Pg. 11; and a portion of unplatted land, recorded in Book 661, Page 184, and lying in the Northeast Quarter of Section 3, Township 12 South, Range 25 East of the 6th Principal Meridian, in the City of Westwood, Johnson County, Kansas, being more particularly described by Timothy Blair Wiswell, LS 1136, of George Butler Associates Inc., CLS 8, on July 20, 2023, as follows:

Commencing at the Southeast Corner of said Northeast Quarter; thence South 87°43'46" West, on the South line of said Northeast Quarter, a distance of 43.00 feet, to a point on the West Right-of-Way line of Highway 169, also known as Rainbow Boulevard, as now established per Condemnation No. 14610, recorded in Bk. 26, Pg. 79, said point also being the Point of Beginning; thence South 02°10'40" East, departing said South line, on said West Right-of-Way line, a distance of 305.31 feet, to the Southeast corner of said Lot 14, KLASSEN PLACE; thence South 87°47'33" West, departing said West Right-of-Way line, on the Northerly Right-of-Way line of W. 51st Street, as now established per said KLASSEN PLACE, a distance of 100.09 feet (100.0 feet per plat), to a point of curvature, said point also being the Southwest corner of said Lot 14; thence Southwesterly, continuing on said Northerly Right-of-Way line, and on a curve to the left, having a radius of 380.20 feet, a central angle of 19°53'46", and an arc length of 132.03 feet; thence North 14°02'17" West, departing said Northerly Right-of-Way line, a distance of 166.00 feet; thence North 87°43'46" East, a distance of 1.25 feet; thence North 02°40'54" East, a distance of 165.87 feet; thence North 87°43'46" East, a distance of 1.29 feet; thence North 02°16'38" West, a distance of 75.00 feet; thence North 87°43'22" East, a distance of 13.15 feet; thence North 02°16'38" West, a distance of 61.34 feet, to a point on the Southerly Right-of-Way line of W. 50th Street, as now established per said SWATZELL ADDITION, said point also being a point on a non-tangent curve; thence Northeasterly, on said Southerly Right-of-Way line, and on a curve to the left, having an initial tangent bearing of North 79°30'06" East, a radius of 420.00 feet, a central angle of 05°10'42", and an arc length of 37.96 feet; thence North 73°06'49" East, continuing on said Southerly Right-of-Way line, a distance of 203.11 feet, to a point on the West Right-of-Way line of said Highway 169; thence South 02°17'01" East, departing said Southerly Right-of-Way line, on said West Right-of-Way line, a distance of 194.71 feet, to the Point of Beginning, containing 117,463.55 square feet, or 2.697 acres, more or less.



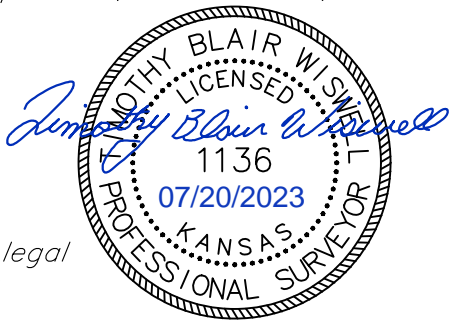
This is to certify that this real property legal description has been prepared by me or under my direct supervision.

	9801 Renner Boulevard Lenexa, Kansas 66219 913.492.0400 www.gbateam.com twiswell@gbateam.com	PROJECT NUMBER 14031.15	Proposed Lot Description Lot 1 HENRY'S ADDITION Westwood, Jo.Co., KS	SHEET NUMBER 1 of 3
	© George Butler Associates, Inc. 2023 C.O.A. # CLS-8	DATE 07/20/2023		

PROPOSED LOT 2 DESCRIPTION:

A tract of land being part of Lot 12, Block 1, KLASSEN PLACE, a subdivision recorded in Bk. 12, Pg. 50; all of Lots 4, 5, and 6, Block 1, SWATZELL ADDITION, a subdivision recorded in Bk. 7, Pg. 35; part of Lot 8, HOLMESLAND, a subdivision recorded in Bk. 3, Pg. 11, in the City of Westwood, Johnson County, Kansas, being more particularly described by Timothy Blair Wiswell, LS 1136, of George Butler Associates Inc., CLS 8, on July 20, 2023, as follows:

Commencing at the Southeast Corner of the Northeast Quarter of Section 3, Township 12 South, Range 25 East of the 6th Principal Meridian; thence South 87°43'46" West, on the South line of said Northeast Quarter, a distance of 43.00 feet, to a point on the West Right-of-Way line of Highway 169, also known as Rainbow Boulevard, as now established per Condemnation No. 14610, recorded in Bk. 26, Pg. 79; thence South 02°10'40" East, departing said South line, on said West Right-of-Way line, a distance of 305.31 feet, to the Southeast corner of said Lot 14, KLASSEN PLACE; thence South 87°47'33" West, departing said West Right-of-Way line, on the Northerly Right-of-Way line of W. 51st Street, as now established per said KLASSEN PLACE, a distance of 100.09 feet (100.0 feet per plat), to a point of curvature, said point also being the Southwest corner of said Lot 14; thence Southwesterly, continuing on said Northerly Right-of-Way line, and on a curve to the left, having a radius of 380.20 feet, a central angle of 19°53'46", and an arc length of 132.03 feet, to the Point of Beginning; thence Southwesterly, continuing on said Northerly Right-of-Way line, and on a curve to the left, having a radius of 380.20 feet, a central angle of 15°04'03", and an arc length of 99.98 feet; thence North 38°33'02" West, continuing on said Northerly Right-of-Way line, a distance of 74.96 feet (74.30 feet per plat), to a point on the West line of said Lot 12; thence North 87°43'46" East, departing said Northerly Right-of-Way line, a distance of 26.00 feet; thence North 02°16'19" West, a distance of 50.00 feet; thence South 87°43'46" West, a distance of 47.04 feet; thence North 02°10'40" West, a distance of 395.28 feet, to a point on the Southerly Right-of-Way line of W. 50th Street, as now established per said SWATZELL ADDITION; thence North 87°43'22" East, on said Southerly Right-of-Way line, a distance of 79.43 feet, to a point on a non-tangent curve; thence Northeasterly, continuing on said Southerly Right-of-Way line, and on a curve to the left, having an initial tangent bearing of North 89°04'45" East, a radius of 420.00 feet, a central angle of 09°34'39", and an arc length of 70.21 feet; thence South 02°16'38" East, departing said Southerly Right-of-Way line, a distance of 61.34 feet; thence South 87°43'22" West, a distance of 13.15 feet; thence South 02°16'38" East, a distance of 75.00 feet; thence South 87°43'46" West, a distance of 1.29 feet; thence South 02°40'54" West, a distance of 165.87 feet; thence South 87°43'46" West, a distance of 1.25 feet; thence South 14°02'17" East, a distance of 166.00 feet, to the Point of Beginning, containing 62,136.40 square feet, or 1.427 acres, more or less.



This is to certify that this real property legal description has been prepared by me or under my direct supervision.

GBA
architects
engineers

9801 Renner Boulevard
Lenexa, Kansas 66219
913.492.0400
www.gbateam.com
twiswell@gbateam.com

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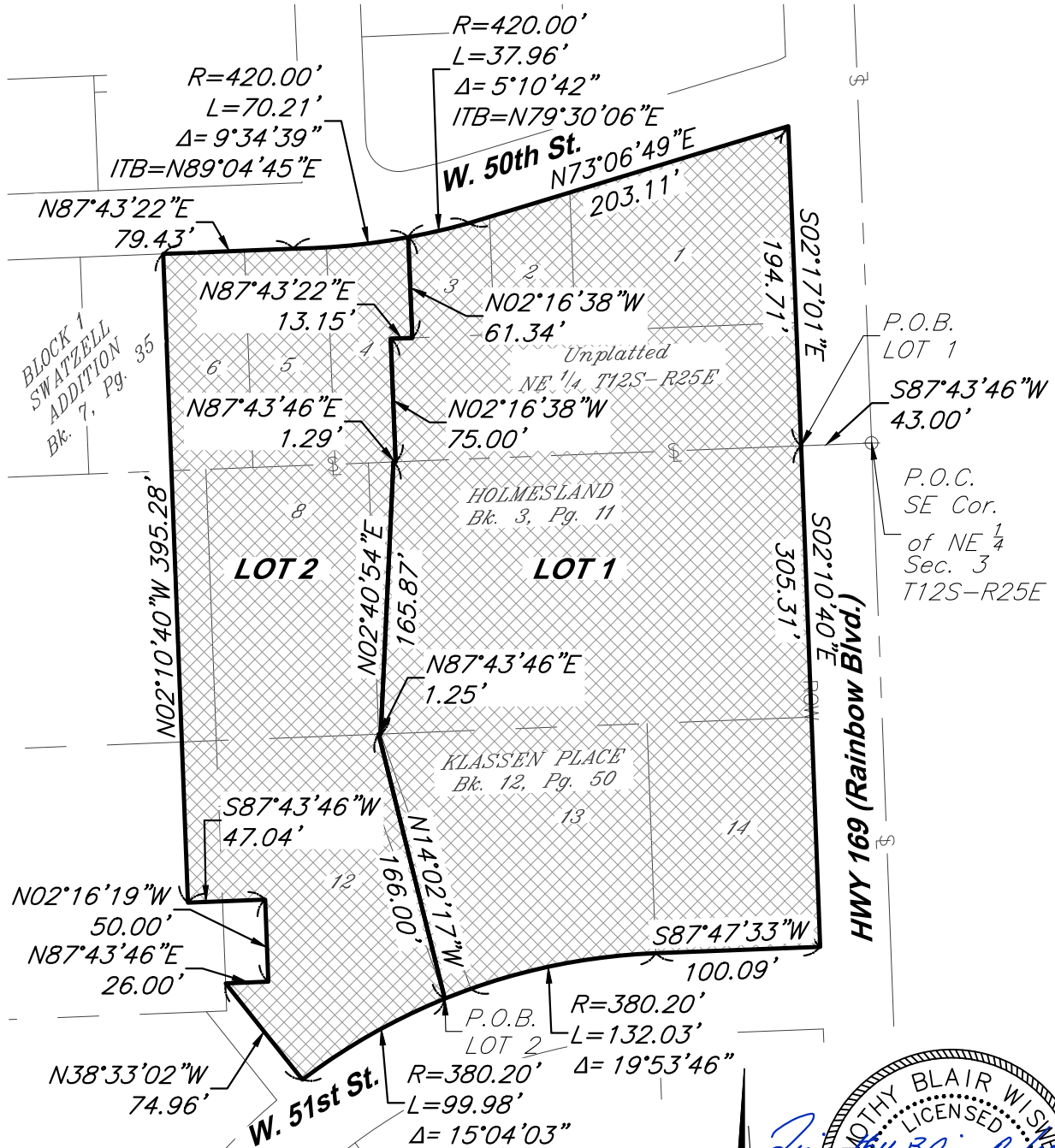
PROJECT NUMBER	14031.15
DATE	07/20/2023

Proposed Lot Description

Lot 2
HENRY'S ADDITION
Westwood, Jo. Co., KS

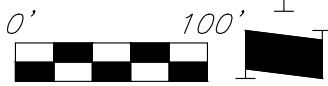
SHEET NUMBER

2 of 3



This is to certify that this real property exhibit was prepared by me or under my direct supervision. This exhibit does not warrant that a boundary survey was performed upon the hereon shown tract of land.

SCALE:
1 INCH =
100 FEET



- LOT 1 117,463.55 Square Feet
- LOT 2 62,136.40 Square Feet

GBA
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9801 Renner Boulevard
Lenexa, Kansas 66219
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PROJECT NUMBER
14031.15
DATE
07/20/2023

Proposed Lot Description
Lot 1 and Lot 2
HENRY'S ADDITION
Westwood, Jo. Co., KS

SHEET NUMBER
3 of 3

Ent	Name	Acct No	Invoice	Date	P.O. Num	Reference	Amount	Discount	Check Amt
090000	KARBANK HOLDING	1350-001	REZONEAPP1	6/27/2023		Westwood CityParcels	500.00		0

Item A. Section III, Item

Payor: KARBANK HOLDINGS LLC
 Payee: CITY OF WESTWOOD

Date: 6/28/2023
 Check No.: 108985

Check Amount: 500.00

Retain this statement for your records

ORIGINAL CHECK HAS A COLORED BACKGROUND, VOID PANTO & A HEAT SENSITIVE ICON - SEE BACK FOR DETAILS

VOID
 KARBANK HOLDINGS LLC
 2000 SHAWNEE MISSION PARKWAY
 SUITE 400
 MISSION WOODS, KS 66205

VOID
 COMMERCE BANK
 1100 WALNUT
 KANSAS CITY, MO 64106

VOID

18-1
 1010
 Date: 6/28/2023
 Check No.: 108985
 Check Amount: 500.00

Five Hundred AND 00/100 Dollars

Pay to the order of:

CITY OF WESTWOOD
 4700 RAINBOW BLVD
 WESTWOOD, KS 66205

VOID IF NOT CASHED WITHIN 120 DAYS WITHIN DATE OF ISSUE

GUH *[Signature]*



FEE SCHEDULE		Office Use Only Fee Paid:
<u> </u> RESIDENTIAL	\$ 250.00	\$ <u>500.-</u> Date Paid:
<u> x </u> OFFICE/COMMERCIAL/INDUSTRIAL	\$ 500.00	<u>6/29/23</u> Case No.:
		<u>RE-2023-01</u> Meeting Date:

Name of Project: 50th and Rainbow Development

Requested Zoning: PD - Planned Development District

Existing Zoning: R-1 Single Family Residential

PROPERTY DESCRIPTION

Location/Address: 5000 - 5050 Rainbow et. al.

Approximate Size of Property (in square feet or acres): Approximately 2.81 Acres

Note: If property is not in a recorded subdivision, an electronic legal description of the property must be submitted with this application in Word format.

Property Tax ID Number: RP63000001 0001; RF251203-3060; RP27000000 0008 D; RP30000001 0012B

Present Use of Property: Park (5000 Rainbow) and Vacant Land (5050 Rainbow et. al.)

Proposed Use of Property: Mixed Use - Office / Retail / Residential

List improvements on the property: N/A

Proposed - Up to 140,000 square feet of buildings, with surface lots and parking garages as required.

APPLICANT INFORMATION Note: *If Agent/Contact Person is anyone other than the fee simple owner, signature of the fee simple owner designating the Agent/Contact Person as the authorized agent for all matters concerning this request, must submit the attached affidavit with the application.*

Owner of Record:

Agent/Contact Person:

Name: City of Westwood, Kansas Name: Karbank Holdings LLC (Adam Feldman, Authorized Agent)

Address: 4700 Rainbow Blvd Address: 2000 Shawnee Mission Parkway, Suite 400

City/State Westwood, KS Zip: 66205 City/State Mission Woods, Kansas Zip: 66205

Phone: 913-362-1550 Phone: 816-221-4488

Email: david.waters@westwoodks.org Email: af@karbank.com



REZONING APPLICATION & CHECKLIST

I, Adam Feldman (Contact Person's Name; Please Print), hereby certify the attached and completed application contains the information as specified below in accordance with the Unified Development Code. I understand the submission of incomplete or inaccurate information may result in a delay in processing and action on this application and may be subject to other penalties provided by law.

[Handwritten signature]
Signature of Contact Person

6/29/23
Date

- 1. Developer participates in a pre-application conference with City staff to present a conceptual plan for the development if development is more than three acres.
2. The Secretary of the Planning Commission checks rezoning applications for completeness and receives the filing fee, which shall be made by the deadline stipulated on the prepared schedule prior to the associated Planning Commission meeting date.
3. For rezoning applications the City shall mail Notice of Public Hearing to all surrounding property owners within 200' (1000' if property is adjacent to extraterritorial property) of all property boundaries. Mailings shall be made at least 20 days prior to the hearing date and shall include the time and place of the hearing, a general description of the proposal, a general street location of the property subject to the proposed change, and a statement explaining that the public may be heard at the public hearing. The notice shall also contain a statement explaining that property owners required to be notified by this section shall have the opportunity to submit a protest petition, in conformance with the Westwood Zoning Ordinance, to be filed with the office of the City Clerk within 14 days after the conclusion of the public hearing. If a valid petition is filed, then an affirmative vote of three-fourths (3/4) of the City Council members must occur to adopt the proposed rezoning.
4. The applicant is responsible for posting signs, furnished by the City, 20 days prior to the Planning Commission public hearing, on the property so that they are visible from the public street. The signs must be placed so as to face each of the streets abutting thereto within five feet of the street right-of-way line in a central position on the lot, tract, or parcel of land so that the sign is free of any visual obstructions surrounding the sign. The applicant shall file an affidavit with the City Clerk at the time of the public hearing verifying that the sign has been maintained and posted as required by this title and applicable resolutions. Failure to submit the affidavit prior to the hearing may result in a continuance of the hearing. The sign may be removed at the conclusion of the public hearing and must be removed at the end of all proceedings on the application or upon withdrawal of the application.
5. The City shall be responsible for a public advertisement in the official newspaper of the City of Westwood.
6. By the deadline stipulated on the prepared schedule, an applicant must submit for approval four (4) collated, bound and rolled copies of a concept or preliminary site plan to the Secretary of the Planning Commission. As part of the rezoning application, a detailed description of the proposed project and how the request complies with applicable zoning and comprehensive planning criteria must be submitted. The narrative should also address the criteria within Section 1.6.17 of the Westwood Zoning Ordinance.
7. After reviewing the plan, City Staff will prepare comments which will be e-mailed to the contact person and must be subsequently addressed and resubmitted by the date stipulated.
8. The Planning Commission conducts public hearings on the rezoning and plan applications and forwards a recommendation to the City Council.
9. The City Council approves the application, in whole or in part, with or without modifications and conditions, or denies the application. Upon review of rezoning applications the City Council also has the option to remand the application back to the Planning Commission. In the event of a denial by the Governing Body, the application may not be resubmitted for one year.
10. Full application and requirements and procedures are identified in Section 1.6 of the Westwood Zoning Ordinance. Applicants bear the onus of understanding all requirements and procedures and asking City staff if and when clarification is needed.



REZONING APPLICATION & CHECKLIST

AFFIDAVIT

STATE OF KANSAS)
) §
COUNTY OF JOHNSON)

David E. Waters (Mayor and Authorized Agent)

I _____, being duly sworn upon our oath and being of
Name of Property Owner

sound mind and legal age, depose and state that:

1. City of Westwood, Kansas is the owner of property located at approximately
Name of Property Owner
RP63000001 0001; RF251203-3080; RP27000000 0008D; RP30000001 0012B
Address or Vicinity of Property

2. I have the legal authority to bind Karbank Holdings LLC, as the Applicant, the
Authorized Person
authority to authorize the filing of land use applications on the Property.

3. I have authorized Karbank Holdings LLC to file an application for
Authorized Person
a Rezoning on the property described above, and
Rezoning/Special Use Permit
do affirm that I have the authority necessary to grant such.

City of Westwood, Kansas

[Signature]
Signature of Property Owner

Name: City of Westwood, Kansas
Print name of Property Owner

Title: David E. Waters, Mayor and Authorized Agent

Subscribed and sworn to before me this 29 day of June, 2023.

[Signature]

My Commission Expires: 2/5/2025



Notary Public

EXHIBIT A

Legal description for the Land:

Lots 13 and 14, Block 1, KLASSEN PLACE, a subdivision in Johnson County, Kansas;

And,

All that part of Lot 12, Block 1, KLASSEN PLACE, a subdivision of land in Johnson County, Kansas described as follows: Beginning at the Northeast corner of said Lot 12; thence Southeasterly, along the Easterly line of said Lot 12, to the Southeast corner thereof; thence Southwesterly, along the Southerly line of said Lot 12, a distance of 16.87 feet; thence Northwesterly, to the Point of Beginning.

And,

East 286.58 feet of the North one-half of Lot Eight (8), HOLMESLAND, a subdivision in Johnson County, Kansas, the Easterly 23 feet thereof being subject to rights of the State Highway Commission under condemnation for road purposes; EXCEPT All that part of the East 286.58 feet of the North half of Lot 8, HOLMESLAND, a subdivision of land in Johnson County, Kansas, more particularly described as follows: Beginning at a point on the North line and 286.58 feet West of the Northeast corner of said Lot 8; thence South, along a line 286.58 feet West of and parallel to the East line of said Lot 8, a distance of 165.39 feet, to the South line of the North half of said Lot 8; thence East, along the South line of the North half of said Lot 8, a distance of 1.28 feet, to the Northeast corner of Lot 12, Block 1, KLASSEN PLACE, a subdivision of land in Johnson County, Kansas; thence Northeasterly, to a point on the North line and 271.28 feet West of the Northeast corner of said Lot 8; thence West, along the North line of said Lot 8, a distance of 15.30 feet, to tie Point of Beginning;

And,

Lots 1, 2 and 3, block 1, Swatzell Addition, a subdivision in the City of Westwood, Johnson County, Kansas;

And,

All that part of the Northeast 1/4 of Section 3, Township 12, Range 25 in the City of Westwood, Johnson County, Kansas, being more particularly described as follows:

Beginning at the Southeast corner of the Northeast quarter of Section 3, Township 12, Range 25; thence West along the East-West center lines of said Section 3; 290.40 feet to the Southeast corner of Lot 4, Block 1, Swatzell Addition; thence North along the East

line of Lot 4, 75 feet; thence East along the South line of a portion of Block 1, Swatzell Addition 290.50 feet to the East line of Section 3; thence South 75 feet to the point of beginning, less the East 43 feet taken for public road, all in the City of Westwood, Johnson County, Kansas,

All except any other part used or dedicated for streets, roads and public rights of way.

Legal description for the Lot Line Adjustment Parcel:

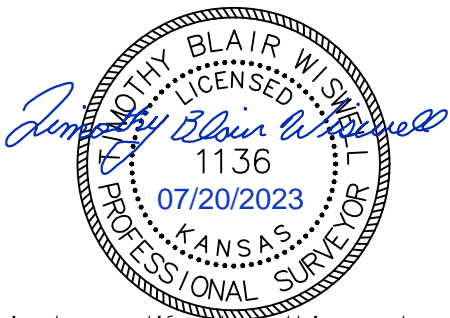
To be developed by survey.

Corrected 7/20/23

PROPOSED LOT 1 DESCRIPTION:

A tract of land being all of Lots 13 and 14, and part of Lot 12, Block 1, KLASSEN PLACE, a subdivision recorded in Bk. 12, Pg. 50; all of Lots 1, 2, and 3, Block 1, SWATZELL ADDITION, a subdivision recorded in Bk. 7, Pg. 35; part of Lot 8, HOLMESLAND, a subdivision recorded in Bk. 3, Pg. 11; and a portion of unplatted land, recorded in Book 661, Page 184, and lying in the Northeast Quarter of Section 3, Township 12 South, Range 25 East of the 6th Principal Meridian, in the City of Westwood, Johnson County, Kansas, being more particularly described by Timothy Blair Wiswell, LS 1136, of George Butler Associates Inc., CLS 8, on July 20, 2023, as follows:

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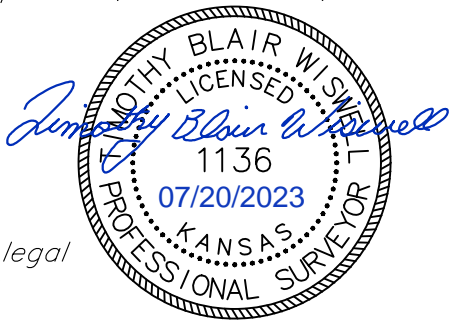
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GBA architects engineers 9801 Renner Boulevard Lenexa, Kansas 66219 913.492.0400 www.gbateam.com twiswell@gbateam.com	PROJECT NUMBER 14031.15	Proposed Lot Description Lot 1 HENRY'S ADDITION Westwood, Jo. Co., KS	SHEET NUMBER 1 of 3
	DATE 07/20/2023		

PROPOSED LOT 2 DESCRIPTION:

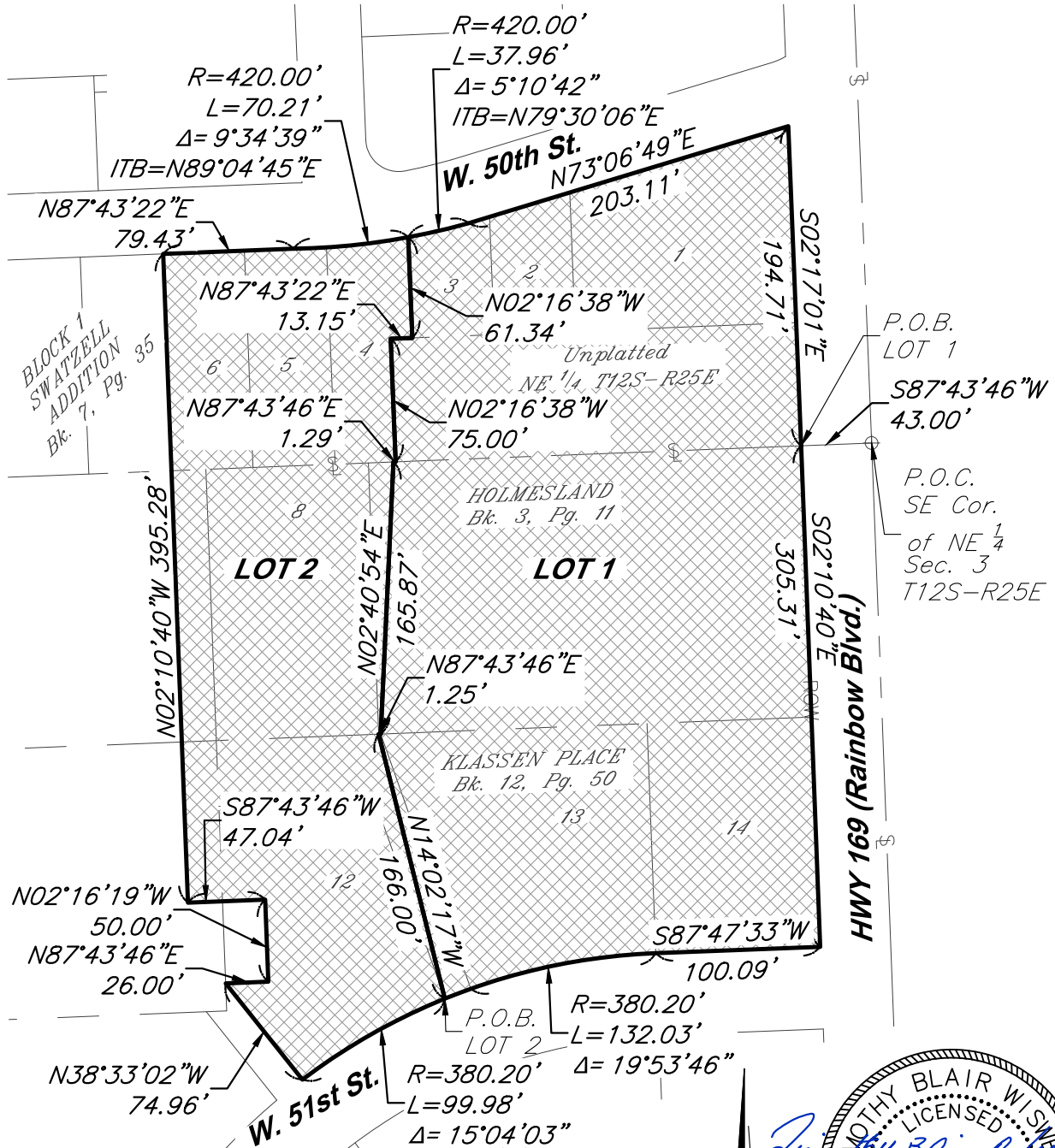
A tract of land being part of Lot 12, Block 1, KLASSEN PLACE, a subdivision recorded in Bk. 12, Pg. 50; all of Lots 4, 5, and 6, Block 1, SWATZELL ADDITION, a subdivision recorded in Bk. 7, Pg. 35; part of Lot 8, HOLMESLAND, a subdivision recorded in Bk. 3, Pg. 11, in the City of Westwood, Johnson County, Kansas, being more particularly described by Timothy Blair Wiswell, LS 1136, of George Butler Associates Inc., CLS 8, on July 20, 2023, as follows:

Commencing at the Southeast Corner of the Northeast Quarter of Section 3, Township 12 South, Range 25 East of the 6th Principal Meridian; thence South 87°43'46" West, on the South line of said Northeast Quarter, a distance of 43.00 feet, to a point on the West Right-of-Way line of Highway 169, also known as Rainbow Boulevard, as now established per Condemnation No. 14610, recorded in Bk. 26, Pg. 79; thence South 02°10'40" East, departing said South line, on said West Right-of-Way line, a distance of 305.31 feet, to the Southeast corner of said Lot 14, KLASSEN PLACE; thence South 87°47'33" West, departing said West Right-of-Way line, on the Northerly Right-of-Way line of W. 51st Street, as now established per said KLASSEN PLACE, a distance of 100.09 feet (100.0 feet per plat), to a point of curvature, said point also being the Southwest corner of said Lot 14; thence Southwesterly, continuing on said Northerly Right-of-Way line, and on a curve to the left, having a radius of 380.20 feet, a central angle of 19°53'46", and an arc length of 132.03 feet, to the Point of Beginning; thence Southwesterly, continuing on said Northerly Right-of-Way line, and on a curve to the left, having a radius of 380.20 feet, a central angle of 15°04'03", and an arc length of 99.98 feet; thence North 38°33'02" West, continuing on said Northerly Right-of-Way line, a distance of 74.96 feet (74.30 feet per plat), to a point on the West line of said Lot 12; thence North 87°43'46" East, departing said Northerly Right-of-Way line, a distance of 26.00 feet; thence North 02°16'19" West, a distance of 50.00 feet; thence South 87°43'46" West, a distance of 47.04 feet; thence North 02°10'40" West, a distance of 395.28 feet, to a point on the Southerly Right-of-Way line of W. 50th Street, as now established per said SWATZELL ADDITION; thence North 87°43'22" East, on said Southerly Right-of-Way line, a distance of 79.43 feet, to a point on a non-tangent curve; thence Northeasterly, continuing on said Southerly Right-of-Way line, and on a curve to the left, having an initial tangent bearing of North 89°04'45" East, a radius of 420.00 feet, a central angle of 09°34'39", and an arc length of 70.21 feet; thence South 02°16'38" East, departing said Southerly Right-of-Way line, a distance of 61.34 feet; thence South 87°43'22" West, a distance of 13.15 feet; thence South 02°16'38" East, a distance of 75.00 feet; thence South 87°43'46" West, a distance of 1.29 feet; thence South 02°40'54" West, a distance of 165.87 feet; thence South 87°43'46" West, a distance of 1.25 feet; thence South 14°02'17" East, a distance of 166.00 feet, to the Point of Beginning, containing 62,136.40 square feet, or 1.427 acres, more or less.



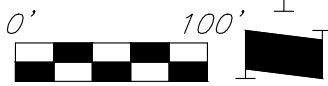
This is to certify that this real property legal description has been prepared by me or under my direct supervision.

GBA architects engineers 9801 Renner Boulevard Lenexa, Kansas 66219 913.492.0400 www.gbateam.com twiswell@gbateam.com	PROJECT NUMBER 14031.15	Proposed Lot Description Lot 2 HENRY'S ADDITION Westwood, Jo. Co., KS	SHEET NUMBER 2 of 3
	DATE 07/20/2023		



This is to certify that this real property exhibit was prepared by me or under my direct supervision. This exhibit does not warrant that a boundary survey was performed upon the hereon shown tract of land.

SCALE:
1 INCH =
100 FEET



GBA
architects
engineers
9801 Renner Boulevard
Lenexa, Kansas 66219
913.492.0400
www.gbateam.com
twiswell@gbateam.com
© George Butler Associates, Inc. 2023 C.O.A. # CLS-8

PROJECT NUMBER
14031.15
DATE
07/20/2023

Proposed Lot Description
Lot 1 and Lot 2
HENRY'S ADDITION
Westwood, Jo. Co., KS

SHEET NUMBER
3 of 3

Ent	Name	Acct No	Invoice	Date	P.O. Num	Reference	Amount	Discount	Check Amt
090000	KARBANK HOLDING	1350-001	_IMDEVPLAN1	6/27/2023		Westwood CityParcels	300.00		Item A, Section III, Item 0

Payor: KARBANK HOLDINGS LLC **Date:** 6/28/2023 **Check No.:** 108981 **Check Amount:** 300.00
Payee: CITY OF WESTWOOD

Retain this statement for your records

ORIGINAL CHECK HAS A COLORED BACKGROUND, VOID PANTO & A HEAT SENSITIVE ICON - SEE BACK FOR DETAILS

KARBANK HOLDINGS LLC
2000 SHAWNEE MISSION PARKWAY
SUITE 400
MISSION WOODS, KS 66205

COMMERCE BANK
1100 WALNUT
KANSAS CITY, MO 64106

18-1
1010

Date: 6/28/2023 **Check No.:** 108981 **Check Amount:** 300.00

Three Hundred AND 00/100 Dollars

Pay to the order of:

CITY OF WESTWOOD
4700 RAINBOW BLVD
WESTWOOD, KS 66205

VOID IF NOT CASHED WITHIN 120 DAYS WITHIN DATE OF ISSUE





Preliminary Plan Application and Checklist

Item A, Section III, Item

Preliminary Plan _____ **PRELIMINARY / REVISED PRELIMINARY PLAN**

\$300.00

Office Use Only	
Fee Paid:	
\$ 300-	
Date Paid:	
6/29/23	
Case No.:	
PDP-2023-014	
Meeting Date:	

NAME OF DEVELOPMENT 50th and Rainbow Development

ADDRESS / VICINITY / PARCEL ID # RP63000001 0001; RF251203-3060; RP27000000 0008D; RP30000001 0012B

CURRENT ZONING R-1 Single Family Residential

APPLICANT

FIRM Karbank Holdings LLC CONTACT Adam Feldman
 ADDRESS 2000 Shawnee Mission Parkway, Suite 400
 CITY Mission Woods STATE KS ZIP 66205
 PHONE 816-221-4488 E-MAIL af@karbank.com

OWNER

NAME City of Westwood, Kansas CONTACT David E. Waters
 ADDRESS 4700 Rainbow Blvd
 CITY Westwood STATE KS ZIP 66205
 PHONE 913-362-1550 E-MAIL david.waters@westwoodks.org

ARCHITECT

FIRM Perspective Architecture + Design CONTACT Mike Paxton
 ADDRESS 2000 Shawnee Mission Parkway, Suite 100
 CITY Mission Woods STATE KS ZIP 66205
 PHONE 816-502-1500 E-MAIL mike@pad.studio

ENGINEER

FIRM BHC CONTACT Austin Lage
 ADDRESS 7101 College Blvd # 400
 CITY Overland Park STATE KS ZIP 66210
 PHONE 913-663-1900 E-MAIL austin.lage@ibhc.com



Preliminary Plan Application and Checklist

I, Adam Feldman (Contact Person's Name; Type or Print), hereby certify the attached and completed application contains the information as specified below in accordance with the City of Westwood Zoning Ordinance. I understand the submission of incomplete or inaccurate information may result in a delay in processing and action on this application.

6/29/23

Signature of Contact Person

Date

Note: The following items apply to all applications for preliminary plan approval (except R-1 zoning district). Some plans, because of their scale and complexity, may require additional information. The Applicant is strongly encouraged to work closely with Staff in advance of an actual application submittal. Please submit ONLY THOSE DRAWINGS necessary to provide information required by this checklist. Do not submit construction drawings or other nonessential drawings as they may delay the review process.

General Requirements for all Preliminary Plan Applications

1. Plan application form, filled out completely and accurately with all required contact information, signatures, etc.
2. All files must be electronic. A PDF file shown as 24 x 36 pages to include all general site development data, landscape and fence data, engineering data, utility plans, floor plans and building elevations, and any ancillary information shall be provided with sufficient information included as to allow for an appropriate review by the City. **All plan sets must be in one document; individual pages will not be accepted.** Three (3) full-sized and three (3) half-sized full plan sets are required.
3. Please label documents, i.e.: Preliminary or Final Plan Set, Stormwater Report, Project Narrative/Business Description, Cross Access Easement, Cut Sheets for Lighting, Cut Sheets for Major Site Furnishings (benches, fountains, planters, statuary), etc. All plans must be sealed by a licensed architect or professional engineer as required.
4. Include a PDF of the Stormwater Report, all pages must be in one document.
5. Where tenants are known, a project narrative to consist of: a business description, hours/days of operation, number of employees, goods or services rendered, products sold at wholesale or retail on site or distributed off-site, any flammable products or chemicals with method of storage, etc. (MSDS sheets will be required at time of Building Permit application).
6. All Planned districts are to provide a description of the overall development concept and also conceptual architectural drawings of units, type of roofing materials proposed, and a color palette for the development.
7. The plan shall address the approval criteria identified in Section 1.6.21 of the Westwood Zoning Ordinance in a manner in which the Planning Commission and Governing Body can review the plan to determine if it demonstrates a satisfactory quality of design.

General Information required for all Preliminary Plan submittals

1. A cover sheet, clearly identifying the project associated with the submittal.
2. North arrow.
3. A scale appropriate to clearly express the design intent for the project, but not less than 1" = 100'
4. A project location map at a scale of not less than 1" = 2000', with the site location clearly marked.
5. Dates of plan preparation and or plan revisions.
6. Owner's name, zoning and present use of adjoining tracts.

Project plans must include the following information:

General Site Development Data

1. Boundary survey tied to established section lines.
2. Section, Township, and Range.
3. Gross and net acreage of the site.
4. Location, width and names of all existing (or proposed) streets and water courses.
5. Location and dimensions of all existing buildings and location and dimension of all proposed buildings.
6. Location of all existing wells, cisterns, septic systems, etc. and whether they are to remain or be removed.
7. Plans for residential structures containing more than 2 units must also provide a unit count by number of bedrooms.
8. Current use of each existing structure and proposed use of each proposed structure.



Preliminary Plan Application and Checklist

9. A survey showing legal description of tract, physical features of property, including contours at vertical intervals of not more than two (2) feet. Elevations shall be marked on such contours based on the existing datum plane established by the U.S. Coast and Geodetic Survey. Benchmark elevations used shall be described on the plan.
10. Adjacent development including lot lines, building footprint, access points and parking.
11. Location, type, and width of sidewalks and walkways.
12. Location of all existing (or proposed) easements.
13. All parcels of land proposed to be dedicated to public use and the conditions of such dedication, if any.
14. Provide a project schedule. Where multiple phases are proposed, a phasing plan reflecting project phases and anticipated construction dates of each phase is required.
15. Multiple building projects must reflect intent to provide shared access and/or parking agreements as necessary.

Parking

1. Location and dimensions of off-street parking including spaces for the disabled, curb cuts, ramps, and location of all loading area.
2. Angle of parking stalls.
3. Dimensioned width of parking aisles, islands, and drives.
4. Curb radii.
5. Parking schedule to show the number of spaces provided and required on all existing buildings; and all spaces required for proposed buildings, including number of ADA stalls proposed.

Landscape Plan and Fence Data

Site improvement and landscape plans must be prepared to clearly describe proposed improvements within the buffer areas, internal parking lot landscape areas, pedestrian-oriented public open space, stormwater management tracts and other common open space areas. Plans must include the following information:

1. A plan to show current physical features (both natural and manmade) of the property and adjacent land within 200 feet of the described property.
2. Existing and proposed contours [Minimum of two foot (2') intervals]. Include detail and cross-sections for all proposed berming. Elevations shall be marked on such contours based on the existing datum plane established by the U.S. Coast and Geodetic Survey. Include dates of topography and permanent and temporary benchmark locations and elevations, if appropriate.
3. Plans must show how stormwater detention basins and BMP's (Best Management Practices) are intended to be integrated into the overall landscape design.
4. Dimensions must be shown for all existing and proposed structures. Plans must show building dimensions, number of floors, height above grade, gross floor area per floor, and total building area.
5. Location and description of all easements and utility locations.
6. Plans must reflect pedestrian access to, as well as circulation within, common open areas. Pedestrian links to the public walks and trails within a planned development must also be reflected on the plan.
7. Identify areas of tree or natural vegetation preservation as well as trees to be removed (trees 10" caliper or less may be located by using aerial photo with site verification or by survey; trees greater than 10" caliper must be located by survey).
8. A plant schedule to identify type, (*tree, shrub, deciduous, evergreen, etc*) and quantity of new plant material and a general description of plant material for proposed stormwater BMP's.
9. Calculations for planting areas.
10. Sight distance information at intersections of public streets and private drives along public and private street right of way.



Preliminary Plan Application and Checklist

11. Location and general details must be submitted for all proposed fences, retaining walls and architectural screens.
12. Location of roof and grounded mechanical units and general screening details.
13. Trash dumpster locations and screening provisions.
14. With regard to native grass and wildflower areas, the landscape plan must include a general proposal describing the intent for establishing and maintaining any such areas.
15. Plans should reflect that all turf areas are to be established with the use of sod unless seed is approved by City staff at the pre-application meeting.
16. Other information may be determined necessary by the Applicant/city to address site specific details.

Lighting

Exterior lighting information must be submitted to include a description of fixtures and a photometric layout. Digital cut sheets must be provided for all exterior fixtures to clearly describe equipment type, location and mounting height. **Light spillage must be shown on photometric layout.**

Signs

A digital copy of conceptual sign criteria must be provided at this time. The criteria must include a general description of the proposed sign program and general locations for signs to be included on the building elevations. The location, size, type of materials, and message of all proposed signs on the subject property, and a written and graphic description of all other existing signs located within 1,000 feet of the property which is the subject of the application, in order to determine compatibility of design is required. Monument signs must be located in plan and shown with proposed landscape.

Building Plans

1. Complete floor plans of existing buildings and conceptual floor plans for new buildings must be submitted to include dimensions and a description of use areas.
2. Building elevation plans are required to illustrate the proposed architectural quality and character. Plans must include a general description of finish materials and colors and show basic architectural detailing. Material and color samples, if available, should be included at this time. Elevations of all sides of proposed buildings are required.
3. Plans must reflect suggested location of wall-mounted meters and other service equipment.

Engineering Information

Streets & Access

1. Location, type and size of access points, driveways, curb cuts to the proposed site and all adjacent sites.
2. Existing street network.
3. Proposed street network, including horizontal and vertical curvature data (use of direction arrows and percent of grade is permitted at preliminary for vertical curve data, unless otherwise specified / required.)
4. Show, label, and dimension all existing and proposed right of way.
5. Provide intersection site distance analysis.
6. Provide traffic lane markings and regulatory signs where applicable.
7. Street light plan. Where existing street lights must be relocated, said street lights must be noted as "to be relocated" on the plans along with the name and mailing address of the party who will assume relocation costs.
8. Vehicle maneuvering / turning templates reflecting the site can accommodate a minimum SU-30 class vehicle (for emergency access to all areas of the site), and the appropriate site design vehicle for any other special areas of the site (such as delivery or dock areas, etc.).
9. A traffic study is required with scope of work to be determined by the City Engineer.



Preliminary Plan Application and Checklist

Stormwater / Watershed

1. Existing and proposed storm drainage, indicating location and connections to existing drainage system.
2. Existing topography with contours at vertical intervals of not more than five (5) feet where the slope is greater than ten percent (10%); and not more than two (2) feet where the slope is less than ten percent.
3. Proposed preliminary grading by contours at vertical intervals of not more than five (5) feet where the slope is greater than ten percent; and not more than two (2) feet where the slope is less than ten percent, supplemented by spot elevations where necessary.
4. Provide at a scale appropriate for clear readability the drainage basins, but not less than 1" = 100', both on-site and off-site drainage sub-basins coming to the subject site, including all points at which it leaves the site. Each sub-basin should be clearly labeled with a designation letter or number, acreage of the sub-basin, and CN value of the sub-basin.
5. Limits of the 100 year flood plain and floodway of all existing water courses that would impact this development.
6. Impervious area calculations.
7. Level of service calculations.
8. Proposed BMP (Best Management Practices) types and locations in plan, profile, and detail form.
9. Such additional information as may reasonably be required in writing by the City Engineer.
10. All engineering plans must be sealed by a Kansas Registered Professional Engineers.
11. Plans must adhere to APWA 5600 as amended.

Ancillary Information (as necessary)

1. Letters of approval for encroachment from easement holders (e.g. utilities) for which site development encroaches upon the utility holder's easement.
2. Such other information as the Planning Commission shall require.
3. Any other information the applicant believes will support the request.
4. At the time of complete plan revision submission (for Agenda presentation) the following must be submitted:
 - a. PDF files must include one full size plan set as one document; **individual pages will not be accepted.**
 - b. A digital copy of all stormwater components must accompany revisions.

Ent	Name	Acct No	Invoice	Date	P.O. Num	Reference	Amount	Discount	Check Amt
090000	KARBANK HOLDING	1350-001	IMDEVPLAN2	6/27/2023		School Dist Parcels	300.00	0.00	300.00

Payor: KARBANK HOLDINGS LLC
Payee: CITY OF WESTWOOD

Date: 6/28/2023
Check No.: 108982
Check Amount: 300.00

Retain this statement for your records



Preliminary Plan Application and Checklist

Item A, Section III, Item

Preliminary Plan _____ **PRELIMINARY / REVISED PRELIMINARY PLAN** \$300.00

Office Use Only
Fee Paid:
\$ 300 -
Date Paid:
6/29/23
Case No.:
PDP-2023-01
Meeting Date:

NAME OF DEVELOPMENT 50th and Rainbow Development

ADDRESS / VICINITY / PARCEL ID # RP30000001 0012A and RP27000000 0008

CURRENT ZONING R-1 Single Family Residential

APPLICANT

FIRM	Karbank Holdings LLC	CONTACT	Adam Feldman
ADDRESS	2000 Shawnee Mission Parkway, Suite 400		
CITY	Mission Woods	STATE	KS ZIP 66205
PHONE	816-221-4488	E-MAIL	af@karbank.com

OWNER

NAME	Unified School District #512	CONTACT	Dr. Joe Gilhaus
ADDRESS	8200 W 71st Street		
CITY	Shawnee Mission	STATE	KS ZIP 66204
PHONE	913-957-2560	E-MAIL	joegilhaus@smsd.org

ARCHITECT

FIRM	Perspective Architecture + Design	CONTACT	Mike Paxton
ADDRESS	2000 Shawnee Mission Parkway, Suite 100		
CITY	Shawnee Mission	STATE	KS ZIP 66205
PHONE	816-502-1500	E-MAIL	mike@pad.studio

ENGINEER

FIRM	BHC	CONTACT	Austin Lage
ADDRESS	7101 College Blvd # 400		
CITY	Overland Park	STATE	KS ZIP 66210
PHONE	913-663-1900	E-MAIL	austin.lage@ibhc.com



I, Adam Feldman (Contact Person's Name; Type or Print), hereby certify the attached and completed application contains the information as specified below in accordance with the City of Westwood Zoning Ordinance. I understand the submission of incomplete or inaccurate information may result in a delay in processing and action on this application.

Signature of Contact Person [Handwritten Signature]

Date 6/29/23

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5. Where tenants are known, a project narrative to consist of: a business description, hours/days of operation, number of employees, goods or services rendered, products sold at wholesale or retail on site or distributed off-site, any flammable products or chemicals with method of storage, etc. (MSDS sheets will be required at time of Building Permit application).
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3. Proposed street network, including horizontal and vertical curvature data (use of direction arrows and percent of grade is permitted at preliminary for vertical curve data, unless otherwise specified / required.)
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5. Provide intersection site distance analysis.
6. Provide traffic lane markings and regulatory signs where applicable.
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9. A traffic study is required with scope of work to be determined by the City Engineer.



Stormwater / Watershed

1. Existing and proposed storm drainage, indicating location and connections to existing drainage system.
2. Existing topography with contours at vertical intervals of not more than five (5) feet where the slope is greater than ten percent (10%); and not more than two (2) feet where the slope is less than ten percent.
3. Proposed preliminary grading by contours at vertical intervals of not more than five (5) feet where the slope is greater than ten percent; and not more than two (2) feet where the slope is less than ten percent, supplemented by spot elevations where necessary.
4. Provide at a scale appropriate for clear readability the drainage basins, but not less than 1" = 100', both on-site and off-site drainage sub-basins coming to the subject site, including all points at which it leaves the site. Each sub-basin should be clearly labeled with a designation letter or number, acreage of the sub-basin, and CN value of the sub-basin.
5. Limits of the 100 year flood plain and floodway of all existing water courses that would impact this development.
6. Impervious area calculations.
7. Level of service calculations.
8. Proposed BMP (Best Management Practices) types and locations in plan, profile, and detail form.
9. Such additional information as may reasonably be required in writing by the City Engineer.
10. All engineering plans must be sealed by a Kansas Registered Professional Engineers.
11. Plans must adhere to APWA 5600 as amended.

Ancillary Information (as necessary)

1. Letters of approval for encroachment from easement holders (e.g. utilities) for which site development encroaches upon the utility holder's easement.
2. Such other information as the Planning Commission shall require.
3. Any other information the applicant believes will support the request.
4. At the time of complete plan revision submission (for Agenda presentation) the following must be submitted:
 - a. PDF files must include one full size plan set as one document; **individual pages will not be accepted.**
 - b. A digital copy of all stormwater components must accompany revisions.

First published in The Legal Record, Tuesday, July 18, 2023.
CITY OF WESTWOOD, KANSAS
NOTICE OF PUBLIC HEARING

The Legal Record

1701 E. Cedar St., Ste. 111
Olathe, KS 66062-1775
(913) 780-5790

The Planning Commission of the City of Westwood, Kansas will hold a public hearing on Monday, August 7, 2023, at 7:00 PM at Westwood City Hall located at 4700 Rainbow Boulevard. Details for attending the meeting and speaking during the public hearing can be found at www.westwoodks.org or by calling 913-362-1550. Written comments may also be submitted ahead of the public hearing to info@westwoodks.org. The subject of the public hearing is:

RZ-2023-01
An application of Karbank Holdings, LLC, on behalf of owner City of Westwood, KS to rezone property at 5000 and 5050 Rainbow Blvd., Westwood, KS 66205 from R-1 (Single-Family Residential) to PD (Planned Development).

RZ-2023-02
An application of Karbank Holdings, LLC, on behalf of owner Shawnee Mission School District to rezone property at 2511 W. 50th Street, et al., Westwood, KS 66205 from R-1 (Single-Family Residential) to PD (Planned Development).

PDP-2023-01
An application of Karbank Holdings, LLC on behalf of owners Shawnee Mission School District and City of Westwood, KS, jointly, for approval of a preliminary development plan at 2511 W. 50th St., 5000 Rainbow Blvd., and 5050 Rainbow Blvd, Westwood, KS 66205.

/s/ Leslie Herring, City Administrator
7/18

ATTN: LESLIE HERRING
CITY OF WESTWOOD
4700 RAINBOW BLVD
WESTWOOD KS 66205-1831

Proof of Publication

STATE OF KANSAS, JOHNSON COUNTY, SS;
Maureen Gillespie, of lawful age, being first duly sworn, deposes and says that she is Legal Notices Clerk for The Legal Record which is a newspaper printed in the State of Kansas, published in and of general paid circulation on a weekly, monthly or yearly basis in Johnson County, Kansas, is not a trade, religious or fraternal publication, is published at least weekly fifty (50) times a year, has been so published continuously and uninterrupted in said County and State for a period of more than one year prior to the first publication of the notice attached, and has been entered at the post office as Periodicals Class mail matter. That a notice was published in all editions of the regular and entire issue for the following subject matter (also identified by the following case number, if any) for 1 consecutive week(s), as follows:

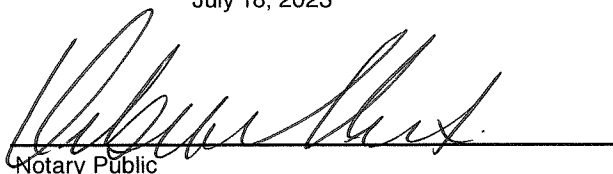
HEARING - PLANNING COMMISSION, AUGUST 7, 2023, RZ-2023-01, RZ-2023-02, PDP-2023-01
7/18/23



Maureen Gillespie, Legal Notices Billing Clerk

Subscribed and sworn to before me on this date:

July 18, 2023


Notary Public

DEBRA VALENTI
Notary Public-State of Kansas
My Appt. Expires Aug. 21, 2023

L13837
Publication Fees: \$15.83



July 17, 2023

Dear neighbor:

As a Westwood property owner within 200 feet of property located at:

2511 W. 50th St., Westwood, Kansas 66205;

5000 Rainbow Blvd., Westwood, Kansas 66205; and/or

5050 Rainbow Blvd. Westwood, Kansas 66205.

you are being notified of the following zoning applications for a proposed new mixed-use commercial development on that real property identified by the addresses above:

RZ-2023-01

An application of Karbank Holdings, LLC, on behalf of owner City of Westwood, KS to rezone property at 5000 and 5050 Rainbow Blvd., Westwood, KS 66205 from R-1 (Single-Family Residential) to PD (Planned Development).

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PDP-2023-01

An application of Karbank Holdings, LLC on behalf of owners Shawnee Mission School District and City of Westwood, KS, jointly, for approval of a preliminary development plan at 2511 W. 50th St., 5000 Rainbow Blvd., and 5050 Rainbow Blvd, Westwood, KS 66205.

A comprehensive description of the project under consideration is available:

- online at: <https://www.westwoodks.org/home-page/page/new-feature-park-development-consideration>; or
- by calling: Leslie Herring, City Administrator, at 913-362-1550;

and a complete legal description of the properties under consideration for rezoning is available at:

Westwood City Hall
4700 Rainbow Blvd.
Westwood, Kansas 66205.

This letter is to inform you that the Westwood Planning Commission will hold a public hearing for consideration of these applications where the general public may provide comments. **The purpose of a**

4700 Rainbow Boulevard Westwood, Kansas 66205 (913) 362-1550 www.westwoodks.org

public hearing is to allow the applicant and all other interested parties a reasonable and fair opportunity to be heard and to present evidence relevant to the application.

The public hearing will be held on and at:

**Monday, August 7, 2023
7:00 PM
Westwood City Hall
4700 Rainbow Blvd.
Westwood, KS 66205.**

The meeting may be attended either in person at Westwood City Hall or virtually, by using the following instructions:

- online: <https://us02web.zoom.us/j/89009964959>; or
- by phone: (312) 626-6799 (Webinar ID: 890 0996 4959).

In addition to live oral comments, written comments may be submitted ahead of the public hearing to info@westwoodks.org and will be included in the public meeting record.

Protest Petition Procedures

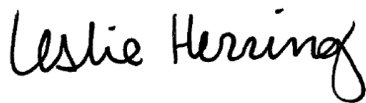
This matter is subject to State law governing the process of land rezoning in Kansas. Pursuant to K.S.A. 12-757, if a protest petition against such rezoning is filed in the office of the Westwood City Clerk within fourteen (14) days after the date of the conclusion of the public hearing pursuant to this notice, signed by the owners of record of 20% or more of the total real property within the area required to be notified by this act of the proposed rezoning of a specific property, excluding streets and public ways, the ordinance or resolution adopting such rezoning shall not be passed except by at least a ¾ vote of all of the members of the Westwood Governing Body.

By way of receipt of this notice, you are a property owner required to be notified and you shall have the opportunity to submit a protest petition.

Pursuant to Westwood Zoning Ordinance 1.6.11 – Public Hearing Process:

If an item which is subject to a public hearing is continued or otherwise carried over to a subsequent date and the public hearing has been opened, then the public hearing shall not be deemed concluded until the date on which the hearing is formally closed and the Planning Commission has taken action on the application. No additional notices shall be required once the public hearing is opened.

Kind regards,



Leslie Herring
City Administrator, Westwood, KS
913.942.2128
leslie.herring@westwoodks.org



July 17, 2023

Dear neighbor:

As a property owner outside of the jurisdictional boundaries of Westwood, Kansas but within 1,000 feet of the properties at:

2511 W. 50th St., Westwood, Kansas 66205;

5000 Rainbow Blvd., Westwood, Kansas 66205; and/or

5050 Rainbow Blvd. Westwood, Kansas 66205.

you are being notified of the following zoning applications for a proposed new mixed-use commercial development on that real property identified by the addresses above:

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4700 Rainbow Blvd.
Westwood, Kansas 66205.

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4700 Rainbow Boulevard Westwood, Kansas 66205 (913) 362-1550 www.westwoodks.org

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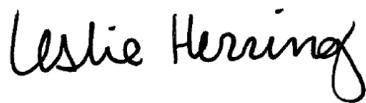
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Kind regards,



Leslie Herring
City Administrator, Westwood, KS
913.942.2128
leslie.herring@westwoodks.org