



PUBLIC WORKS COMMISSION MEETING AGENDA

TUESDAY, MARCH 10, 2026 AT 5:30 PM

**COUNCIL CHAMBERS, SECOND FLOOR, MUNICIPAL BUILDING - 106 JONES STREET,
WATERTOWN, WI 53094**

1. CALL TO ORDER

2. COMMENTS AND SUGGESTIONS FROM CITIZENS PRESENT

Each individual who would like to address the Committee will be permitted up to three minutes for their comments

3. REVIEW AND APPROVE MINUTES

A. Public Works minutes from February 24, 2026

4. BUSINESS

A. Review and take possible action: Recommend authorizing the transfer of \$1,255,000 in excess Water Utility funds to establish and fund the Private Lead Service Financial Assistance Project Fund for private-side lead service line replacement assistance as authorized under Wis. Stat. § 66.0811(2) and (3)

B. Review and take possible action: 2025 Annual Quarry Operations Report

C. Review and take possible action: Adoption of the City-wide Bicycle and Pedestrian Network Plan

D. Review and take possible action: Approval of the 2025 MS4 Permit Annual Report

E. Review and take possible action: Contract with Robert E. Lee & Associates for construction inspection of Rock River Ridge Phases II & III for \$53,650

F. Review and take action: ordinance to amend sections § 446-13 Lead acid batteries, major appliances, waste oil and tires and yard waste, § 446-17 Title to recyclable material, § 457-6 Street and sidewalk openings, § 457-11 Snow and ice removal, § 457-12 Use of streets or sidewalks for sale of merchandise, and § 457-17 Depositing of yard waste in streets or alleys prohibited of the city of Watertown general ordinances

5. ADJOURNMENT

Persons requiring other reasonable accommodations for any of the above meetings, may contact the office of the City Clerk at cityclerk@watertownwi.gov phone 920-262-4000

A quorum of any City of Watertown Council, Committee, Board, Commission, or other body, may be present at this meeting for observing and gathering of information only

PUBLIC WORKS COMMISSION

Tuesday, February 24, 2026

5:30 p.m.

The Public Works Commission met at the above date and time. The following members were present: Alderpersons Brad Blanke, Dan Bartz, and Tony Arnett; Citizen member Pete Thompson. Absent: Myron Moldenhauer (technical issues logging on virtually.) Also present: City Staff Andrew Beyer, Richie Piltz, Stacy Winkelman.

1. **Call to order.** Chairperson Arnett called the meeting to order at 5:30 p.m.
2. **Comments and Suggestions from Citizens Present.**

Stephanie Broere of the Main Street Program encouraged the Commission to not change the street lighting for the Main Street reconstruction project if it would result in the project being delayed.

Alderman Ken Berg thanked city staff and the Commission for resuming an annual program of reviewing city sidewalks.

3. **Review and take possible action. Minutes of PWC meeting of February 10th, 2026.** Mr. Bartz moved to approve the minutes as submitted, seconded by Mr. Thompson. Motion carried by unanimous voice vote.
4. **Review and Discuss: Update on Leaf Collection Program** – Public Works Manager Stacy Winkelman reported back to the Commission on options for a new leaf vacuum truck. Commission members agreed to request a new truck in the 2027 budget.
5. **Review and take possible action: Downtown Main Street Reconstruction Project - Street Lighting Review & Direction** – Public Works Director Andrew Beyer reported back to the Commission on street light options for the Main Street reconstruction project. The existing pedestrian lights cannot be re-used due to their age, poor condition, and lack of replacement parts. Staff did identify an alternate provider for the overhead lighting, Sun Valley, whose unit is nearly identical to the approved fixture, but would cost roughly \$500,000 less. The State DOT has initially indicated changing the light fixture could trigger a review process that could result in the overall delay of the project. Commission members requested staff to go back to the State to press the issue due to the substantial savings, as the actual design change is minimal. Commission members also recommended the City purchase the lighting fixtures directly (instead of through the project contractor), as this would avoid a 35% markup in price. However, this means the City will have purchase the fixtures in 2027 due to the lead time. This will be included in the 2027 budget recommendation.
6. **Review and take possible action: 2026 Sidewalk Repair Program Area** – Engineering Project Manager Richie Piltz reviewed the proposed program area for 2026 sidewalk repairs (mainly centered in the neighborhood near the hospital.) Mr. Blanke moved to approve the program area, seconded by Mr. Thompson. Motion carried by unanimous voice vote.
7. **Review and take possible action: Award Pavement Marking Contract #1-26 Base Bids A & B, and Alternate Bids 1, 2 & 3 to Brickline, Inc. for \$20,609.25** - Richie Piltz presented the bids for this contract. Multiple bids were received and the recommended bid is under budget. Mr. Thompson moved to recommend the selected bid to Council, seconded by Mr. Bartz. Motion carried by unanimous voice vote.

8. **Review and take possible action: Award Rout and Crack Fill Contract #2-26 Base Bids A & B, and Alternate Bids 1 & 2 to Thunder Road, LLC for \$54,380.00** - Richie Piltz presented the bids for this contract. Multiple bids were received and the recommended bid is under budget. Mr. Blanke moved to recommend the selected bid to Council, seconded by Mr. Thompson. There is no federal cost share for this – it is all City-funded. Motion carried by unanimous voice vote.
9. **Review and take possible action: Award Seal Coating Contract #3-26 Base Bid A, and Alternate Bid 1 to the low bidder, Scott Construction, Inc. for \$94,165.95** - Richie Piltz presented the bids for this contract. Multiple bids were received and the recommended bid is under budget. Mr. Bartz moved to recommend the selected bid to Council, seconded by Mr. Blanke. Motion carried by unanimous voice vote.
10. **Review and take possible action: Award 2026 Annual Street and Utility Project Contract #4-26 Base Bids A & B, and Alternate Bids 1, 2 & 5 to Forest Landscaping & Construction, Inc. for \$926,828.00** - Richie Piltz presented the bid for this contract. (There was a corrected handout on the table, making small edits to the distribution of funding across accounts.) There was only 1 bid received. City staff compared the cost to prior years and it was in line with projected inflation for this work. The recommended bid is under budget. Mr. Thompson moved to recommend the selected bid to Council, seconded by Mr. Bartz. Motion carried by unanimous voice vote.
11. **Review and take possible action: Award 2026 Storm Sewer Cleaning & Televising Project Contract #5-26 Base Bid A and Alternate Bids 1, 2 & 3 to Green Bay Pipe & TV, LLC for \$129,288.53** - Richie Piltz presented the bids for this contract. Multiple bids were received and the recommended bid is under budget. Mr. Blanke moved to recommend the selected bid to Council, seconded by Mr. Thompson. Motion carried by unanimous voice vote.
12. **Review and take possible action: Award Bituminous Surfacing Contract #6-26 Base Bids A, B, C, D, E, F, G, & H, and Alternate Bids 1, 2, 3, 4, 5, & O to Wolf Paving Co., Inc. for \$1,141,831.95** - Richie Piltz presented the bids for this contract. (There was a corrected handout on the table, making small edits to the distribution of funding across accounts.) Multiple bids were received and the recommended bid is under budget. Mr. Blanke moved to recommend the selected bid to Council, seconded by Mr. Bartz. Motion carried by unanimous voice vote.
13. **Review and take possible action: Award LRIP 2026/2027 Bituminous Surfacing Project-2026 Market Way Contract #10-26 Base Bid A to Wolf Paving Co., Inc for \$209,919.33** - Richie Piltz presented the bids for this contract. Multiple bids were received. With \$59,000 in State-reimbursed costs, the recommended bid is under budget. Mr. Thompson moved to recommend the selected bid to Council, seconded by Mr. Blanke. Motion carried by unanimous voice vote.
14. **Adjournment.** Mr. Blanke moved to adjourn at 6:15 p.m., seconded by Mr. Bartz. Motion carried by unanimous voice vote.

Respectfully submitted,

Tony Arnett, Chairperson

Note: These minutes are uncorrected, and any corrections made thereto will be noted in the proceedings at which these minutes are approved.

**RESOLUTION FOR
CITY OF WATERTOWN - WATER SYSTEMS**

AUTHORIZING ACCEPTANCE OF EXCESS WATER UTILITY FUNDS

**SPONSOR: MAYOR STOCKS
FROM: FINANCE COMMITTEE**

WHEREAS, the City operates a municipal water utility, under Wis. Stat. §§ 66.0801 to 66.0813, and the Water Utility has accumulated unrestricted net position/fund balance in excess of what is reasonably required for operations, maintenance, depreciation, interest and debt service requirements, local and school tax equivalents, additions and improvements, and other necessary disbursements or indebtedness; and

WHEREAS, Wis. Stat. §§ 66.0811(2) and (3) authorize the City to place such excess funds into the general fund or a special fund for special municipal purposes; and

WHEREAS, the Common Council has determined that it is in the public's interest to establish a Special Private Lead Service Financial Assistance Project Fund ("PLSFAP Fund") to provide financial assistance to property owners for the replacement of private-side lead water service lines, and that such purpose constitutes a special municipal purpose under Wis. Stat. § 66.0811(3); and

WHEREAS, the City of Watertown Public Works Commission and Finance recommend the transfer of \$1,255,000 from the Water Utility Fund (retained earnings / fund balance) to the PLSFAP Fund to support private-side replacement projects; and

WHEREAS, Wis. Stat. §§ 65.90(5)(a) and 66.0607(7) require that all budget amendments and transfers of appropriations be approved by a two-thirds vote of the Common Council; and

WHEREAS, the City has prepared the associated budget amendment, attached as Exhibit A, and recommends that the Common Council formally accept the transferred funds and appropriate them for use in the PLSFAP Fund.

NOW, THEREFORE, BE IT RESOLVED by the Common Council of the City of Watertown that:

- 1.) The City hereby accepts the transfer of \$1,255,000 from the Water Utility Fund to the Special Private Lead Service Financial Assistance Project Fund pursuant to Wis. Stat. §§ 66.0811(2) and (3).
- 2.) The Special Private Lead Service Financial Assistance Project Fund is hereby established to receive and administer funds solely for the purpose of providing grants, reimbursements, and program administration related to private-side lead service line replacement.
- 3.) The 2026 City Budget is hereby amended as shown in Exhibit A, which reflects:
 - a decrease in Water Utility Fund balance in the amount of \$1,255,000, and
 - a corresponding appropriation to the PLSFAP Fund for program expenditures.
- 4.) City staff is authorized and directed to make all necessary accounting entries and financial transfers to implement this Resolution.
- 5.) This Resolution shall take effect upon passage and publication as required by law.

	YES	NO
DAVIS		
LAMPE		
BERG		
BARTZ		
BLANKE		
SMITH		
ARNETT		
WETZEL		
MOLDENHAUER		
MAYOR STOCKS		
TOTAL		

ADOPTED April 6, 2026

CITY CLERK

APPROVED April 6, 2026

MAYOR

EXHIBIT A — Budget Amendment

Account #	Name	Curr Bdgt	+ / -	Modified
Water Utility Fund				
03-99-30-40	Contribution to PLSFAP Fund [Move funds to City Fund]	-	1,255,000	1,255,000
Non-recurring Reserve Account				
24-48-12-44	PLSFAP Revenue [Accept Water Utility funds]	-	(1,255,000)	(1,255,000)
24-58-11-44	PLSFAP Expense [Private-side Project Expenses]	-	1,255,000	1,255,000

**RESOLUTION FOR
CITY OF WATERTOWN - WATER SYSTEMS**

AUTHORIZING TRANSFER OF EXCESS WATER UTILITY FUNDS

**SPONSOR: ALDERPERSON ARNETT
FROM: PUBLIC WORKS COMMISSION**

WHEREAS, the City of Watertown (the “Municipality“) owns and operates a municipal water utility pursuant to Wis. Stat. §§ 66.0801 to 66.0813; and

WHEREAS, Wis. Stat. § 66.0811(2) provides that the income of a municipal public utility shall first be used to make payments to meet operation, maintenance, depreciation, interest and debt service requirements, local and school tax equivalents, additions and improvements, and other necessary disbursements or indebtedness; and

WHEREAS, Wis. Stat. § 66.0811(2) further provides that income in excess of these requirements may be paid into the general fund; and

WHEREAS, Wis. Stat. § 66.0811(3) authorizes the City to place excess funds into a special fund to be used for special municipal purposes; and

WHEREAS, as of December 31, 2025, the Water Utility has excess funds in its unrestricted net position / fund balance in the approximate amount of \$1,255,000, above what is reasonably required for operation, maintenance, depreciation, interest and debt service requirements, local and school tax equivalents, additions and improvements, and other necessary disbursements or indebtedness; and

WHEREAS, the Common Council finds it is in the public interest to transfer \$1,255,000 of such excess funds to a special fund within the City’s General Fund, called the Private Lead Service Financial Assistance Project Fund, as permitted by Wis. Stat. § 66.0811(2) and (3); and

WHEREAS, the purpose of the Private Lead Service Financial Assistance Project Fund is to provide grants, reimbursements, and program administration related to private-side lead service line replacement within the City; and

WHEREAS, the City of Watertown Public Works Commission and Finance Committee considered the proposed transfer on March 10, 2026 and March 23, 2026 respectively, and both recommended the establishment of the Private Lead Service Financial Assistance Project Fund; and

WHEREAS, Wis. Stat. §§ 65.90(5)(a) and 66.0607(7) require that budget amendments and transfers of appropriations be authorized by a two-thirds vote of the membership of the governing body;

NOW, THEREFORE, BE IT RESOLVED BY THE COMMON COUNCIL OF THE CITY OF WATERTOWN, WISCONSIN: The Common Council hereby authorizes the transfer of \$1,255,000 from the Water Utility Fund balance to the General Fund / Private Lead Service Financial Assistance Project Fund pursuant to Wis. Stat. §§ 66.0811 (2) and (3). The 2026 budget is hereby amended to reflect this transfer as shown in **Exhibit A**, attached hereto and incorporated herein by reference. The Finance Director is directed to make all necessary accounting entries to carry out this resolution. This resolution shall take effect upon passage and publication as required by law.

	YES	NO
DAVIS		
LAMPE		
BERG		
BARTZ		
BLANKE		
SMITH		
ARNETT		
WETZEL		
MOLDENHAUER		
MAYOR STOCKS		
TOTAL		

ADOPTED April 6, 2026

CITY CLERK

APPROVED April 6, 2026

MAYOR

EXHIBIT A — Budget Amendment

Account #	Name	Curr Bdgt	+/-	Modified
Water Utility Fund				
03-99-30-40	Contribution to PLSFAP Fund [Move funds to City Fund]	-	1,255,000	1,255,000
Non-recurring Reserve Account				
24-48-12-44	PLSFAP Revenue [Accept Water Utility funds]	-	(1,255,000)	(1,255,000)
24-58-11-44	PLSFAP Expense [Private-side Project Expenses]	-	1,255,000	1,255,000



Water Systems

800 Hoffmann Drive • P.O. Box 477 • Watertown WI 53094-0477
WASTEWATER (920) 262-4085 • WATER (920) 262-4075

To: Chairman Arnett and members of the Public Works Commission
From: Peter Hartz – Water Systems Manager

March 04, 2026

Re: Transfer of excess Water Utility funds and establishment of the Private Lead Service Financial Assistance Project Fund

Review and take possible action: Recommend authorizing the transfer of \$1,255,000 in excess Water Utility funds to establish and fund the Private Lead Service Financial Assistance Project Fund for private-side lead service line replacement assistance as authorized under **Wis. Stat. § 66.0811(2) and (3)**.

Background:

In 2020, the City set a goal of replacing all lead service lines by 2030. The Water Systems Department began implementation in 2021, building on earlier work from 2018–2020, and has made significant progress replacing lead services across the community. Through the end of 2023, the City replaced a total of 708 lead services (150 in 2021, 456 in 2022, and 102 in 2023).

At the end of 2023, the Wisconsin DNR exhausted the prior private lead service replacement funding, and the City pursued new opportunities through the Bipartisan Infrastructure Law-funded Lead Service Line Replacement program administered through the Safe Drinking Water Loan Program.

In 2024, the Public Works Commission was updated on the City's funding applications and approach to include the entire City and all remaining lead services, with a goal of completion within three years. As part of the SFY 2025 funding cycle, Watertown received strong rankings and significant principal forgiveness allocations across three projects (CT1002, CT9620, and City-wide), which positioned the City well to complete the private-side replacement work while minimizing homeowner financial burden. In addition, the program requirements align with City Code 512-87 (Amd. Ord 20-27) and the PSC structure allowing eligible customers to receive grants toward private-side replacement costs.

The attached resolutions are intended to ensure the City has a dedicated and legally supported local funding mechanism to assist with private-side lead service line replacement.

- The PWC resolution authorizes the transfer of **\$1,255,000** of excess Water Utility funds into a special fund within the City's General Fund titled the **Private Lead Service Financial Assistance Project Fund**, as permitted under **Wis. Stat. § 66.0811(3)**.
- The second resolution (for the finance committee) is for the finance committee and recommends formally accepting the transfer, establishes/affirms the special project fund, and amends the 2026 budget to appropriate the funds for program use.

Fiscal Impact

The fiscal impact of these resolutions is the transfer and acceptance of **\$1,255,000** from Water Utility fund balance into the Private Lead Service Financial Assistance Project Fund, with a corresponding 2026 budget amendment. These funds will be restricted for private lead service line financial assistance (grants/reimbursements and program administration), supporting project implementation and helping reduce the risk of unplanned rate impacts or delays.

PSC Coordination

The Public Service Commission oversees all utility operations and rate structures in the state of Wisconsin. City staff discussed this proposal with PSC staff, who provided the following information:

- PSC approval for the transfer and use of Utility funds for private-side lead service lateral replacement assistance is not required at this time. PSC views the proposed transfer and use of the Utility's unrestricted fund balance as a municipal action governed by Home Rule, rather than a Utility action.
- The PSC will review the use of the Utility's fund balance during the City's next simplified rate cases, anticipated in 2030 and 2031.
- Transfers of this nature are uncommon and have not previously been used to support private-side lead service replacement in Wisconsin. The PSC indicated that all such transfers will be subject to strict scrutiny during review in 2030 and 2031.
- If the PSC determines that the transfer adversely affected the Utility's financial position, it may issue remedial orders. However, ratepayers will not be responsible for restoring the Utility's financial health; instead, the Utility and the City would be required to restore the Utility's fund balance.

Recommendation

I recommend the Commission support and approve the transfer of the excess funds for financial assistance needed for the lead service line replacement costs. I recommend this commission approve both the included resolutions; one from this commission for the city council, and the other for the finance committee to review and take possible action regarding accepting the transfer of funds for the financial assistance for the private lead service replacement program funding.

This action provides a dedicated funding structure for private-side lead service line replacement assistance, strengthen long-term program administration, and support the City's overall goal of eliminating lead services while protecting public health and minimizing cost burden to residents.

Sincerely,
Peter Hartz

Water Systems Manager

MEMO

Engineering Division

To: Chairperson Arnett & Commissioners

From: Andrew Beyer, P.E.

Date: March 5, 2026

Subject: 2025 Annual Quarry Operations Report

Background

The Wisconsin Department of Natural Resources (DNR) requires all quarries to submit an annual report as part of their permit requirements. The City of Watertown serves as the regulatory authority for quarries within the City. Currently, the City has one quarry located at 408 Bonner Street, which it owns. The attached report represents the quarry's annual submittal to the City as its local regulator. Information in the report will subsequently be submitted by the City to the DNR in accordance with state requirements.

Budget Goal

No effect on budget

Financial Impact

No financial Impact

Recommendation

Submittal to the DNR in accordance with state requirements.

Andrew Beyer, P.E.
920.262.4050

Maureen McBroom, ENV SP
920.206-4264

Nathan Williams
920-262-4052

Ritchie Section 4, Item B.
920.262.4034

Christopher Newberry
920-390-

Administrative Assistant
Wanda Fredrick 920.262.4060



2025 City of Watertown Annual Nonmetallic Mining Operations Report for Watertown City Quarry

This report is submitted on behalf of the City of Watertown in fulfillment of Wisconsin Administrative Code Chapter DNR 135.36 and the City of Watertown’s permitting process of Chapter 377 Nonmetallic Mining of the Code of Ordinances.

WIDNR Permit Number: WI-0046515-07-0

Current and future uses

The quarry is presently used primarily as a storage area for stockpiles for rock of differing gradation sizes, crushed and uncrushed concrete, and asphalt. The City’s road repairs and construction projects have contributed to the demolition materials of uncrushed concrete, and asphalt into the quarry. These stockpiles of uncrushed concrete and asphalt have grown in the last few years. Lately, very little of the virgin rock materials have been used from the quarry. In the spring of 2025, 15,017 tons of asphalt, 9,750 tons of concrete, and 4,006 tons of virgin material were crushed for use on the City’s future projects. This is a significant increase from past years of crushing. Due to the amount of crushed materials available and cost savings, it is anticipated that crushing would occur every other year annually, or as needed. During winter months the facility is used for storing snow from the downtown area. The top portion of the facility (South of the open excavated quarry) is used to stage supplies for Streets and Parks divisions.

This spring the streets division continued to remove woody vegetation, regrade, and seeded along the top East portions of the quarry and along the stream tributary on the East portion of the property. This work is necessary to maintain the fence around the facility.

The quarry is near its designed horizontal limits of construction. It is estimated that quarry depth can be increased approximately 15 feet below current elevations. Depending on the usage and rate of extraction, the projected quarry life, with consistent mining, could be over 25 years. The current plan after its mining usefulness has ended and reclamation is complete iscomplete for the quarry to revert be converted to a lake for public use. The quarry would then become a part of the Brandt-Quirk Park, which is located directly north of the quarry.

Operators

The City of Watertown is the operator of this quarry. It has been overseen by the City’s Public Works Department – Streets & Solid Waste Division and Engineering Division. The addresses for the divisions are:

Street/Solid Waste Division
811 S. First Street
Watertown, WI 53094

Engineering Division
106 Jones Street
P. O. Box 477
Watertown, WI 53094

Location

The quarry is in the northwestern portion of the City of Watertown and is adjacent to Brandt Quirk Park.

Address: 408 Bonner Street Watertown, WI 53094.

Tax parcel number: 14-291-0915-3231-058

Legal description: That parcel of land commencing at the Northwestern corner of out lot 21, thence Easterly along said Northern Out Lot boundary line, approximately 1,000 feet to a point; thence Southerly along a line parallel with the Western boundary line of Out Lot 21 approximately 1620 feet to a point; thence Southeasterly approximately 1250 feet to a point on the Southern boundary line of Out Lot 21, said point being approximately 920 feet West of the Southeastern corner of Out Lot 21; thence Westerly to the Chicago, Milwaukee, St Paul and Pacific right of way; thence Northwesterly along said right of way to the Western boundary line of Out Lot 21; thence Northerly to the place of beginning.

Also a parcel of land commencing at a point on the Northern boundary line of Out Lot 22, approximately 920 West of the Northeastern corner of Out Lot 22; thence Southerly along a line parallel with the Eastern boundary line of said Out Lot approximately 900 feet to a point; thence Southeasterly approximately 360 feet to a point, said point being on the Westerly line of Bonner Street extended; thence Southerly along a line approximately 135 feet to the Southern boundary line of Out Lot 22; thence Westerly to the Chicago, Milwaukee, St Paul and Pacific right of way; thence Northwesterly along said right of way to a point at which said right of way intersects with the Northern boundary line of Out Lot 22; thence Easterly along said boundary line to the place of beginning.

See attached map for location within the City.

Quarry Size

The current acreage affected by nonmetallic mining is approximately 23.8 acres. No Quarry land has been permanently or temporarily reclaimed. Exhibits showing the City of Watertown Brandt Quirk Park Quarry are attached to this report as references.

I certify that this information is true and accurate, and that the nonmetallic mining site described herein complies with all conditions of the applicable nonmetallic mining permit and Chapter NR 135, Wisconsin Administrative Code.

Date ___/___/___

Andrew M Beyer, P.E.
City Engineer Public Works Director

WATERTOWN CITY QUARRY



MEMO

Engineering Division

To: Chairman Arnett and Public Works Commission Members
From: Andrew Beyer, P.E., Director of Public Works/City Engineer
Date: 3/5/2026
Subject: Bicycle and Pedestrian Network Plan

Background

The City of Watertown, with assistance from HKGi, Inc., has developed a draft city-wide Bicycle and Pedestrian Network Plan (Plan), funded in part with a Transportation Alternatives Program (TAP) Grant from Wisconsin Department of Transportation. The Bicycle and Pedestrian Path Planning Task Force members and partners served as the Advisory Committee for this plan and have reviewed and provided comments during the plan development.

The city-wide Bicycle and Pedestrian Network Plan will guide future installations and connections of bicycle and/or pedestrian routes, initiatives and other measures to promote safe access and travel around the city and connecting to the larger region, via non-vehicular transportation modes. The plan is designed to be an important piece in future grant applications, project selection and budget discussions.

Budget Goal

1. Proactively maintains and improves our parks and infrastructure to ensure safety, quality, and equity
2. Fosters community growth by assessing opportunities, stakeholder input, environmental needs, and modern code and policy priorities
3. Maintains a safe and healthy community, with an eye toward future needs and trends

Financial Impact

The city-wide Bicycle and Pedestrian Network Plan was funded in part with a Transportation Alternatives Program (TAP) Grant through the Wisconsin Department of Transportation. Future implementation of the plan will be considered as opportunities and funding allow, including but not limited to future grant opportunities, budget, program and policy improvements and other



MEMO

location-specific route improvements that can be completed as standalone projects or as part of larger construction projects within the capital improvement program.

Recommendation

Engineering Division staff recommend adoption of the City-wide Bicycle and Pedestrian Network Plan, to be considered as opportunities and funding allow.

The Bicycle and Pedestrian Path Planning Task Force has reviewed the draft plan and recommends adoption.

EXECUTIVE SUMMARY

PLAN AND PURPOSE



The City of Watertown has been incrementally building a network for pedestrians and bicyclists over the last 20+ years. Through collaborative efforts across multiple City departments (Public Works, Parks, Recreation & Forestry), along with coordination with both Jefferson and Dodge Counties, and the Bicycle and Pedestrian Path Task Force, projects such as the Interurban Trail have expanded the City's options for walking and biking, both as a means of transportation and for recreation. Over the last few years, it has become apparent that a long-range plan is needed to clearly create a guide to develop a fully-connected network, which the City can work to implement in phases. A long-term plan like this is ambitious and will require continued collaboration across City departments, support from the City Council, and stewardship from non-profit and advocacy organizations.

The purpose of this planning process is:

- 1 To create a plan that addresses the future of a city-wide bicycle and pedestrian network, building off the existing network.
- 2 To gather and incorporate community feedback and desires to address safety and a future vision for active transportation in Watertown.
- 3 To identify best practices and guide the City in future facility and network investments.
- 4 To guide policy and programming improvements, such as wayfinding, bike parking, and educational and encouragement activities.
- 5 To build awareness of and enthusiasm for expanding multimodal transportation opportunities in and around Watertown.

WHY PLAN FOR BIKING AND WALKING IN WATERTOWN?

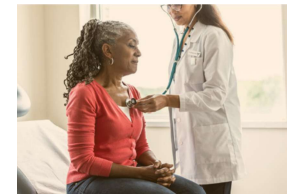
The following are some key reasons for planning for the future of biking and walking in Watertown. To learn more, see **Chapter 1: Introduction, page 6.**



» **Safety**



» **Livability**



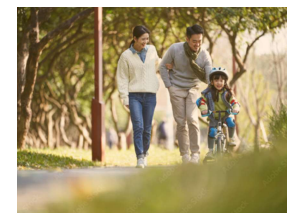
» **Health**



» **Prosperity**



» **Air Quality**



» **Recreation**



» **Parking and Transportation Networks**



» **Tourism**

PLAN OBJECTIVES

Early in the planning process, the following objectives were identified to holistically address bicycle and pedestrian issues within the plan. The Plan Objectives focus on physical infrastructure improvements, programs, and policies.



Address a Variety of Users

Define a cohesive, connected walking and biking network in Watertown that meets the needs of

- » Commuters
- » Recreational bicyclists and hikers
- » People with disabilities
- » People who don't have the ability or interest in driving
- » People who use public transportation
- » Children and young people
- » Seniors
- » Families
- » Visitors



Encourage and Educate

- » Identify programs and initiatives to support biking and walking in Watertown
- » Promote ways to make passive forms of exercise, such as biking and walking, a part of daily routines for residents
- » Develop or identify programs or initiatives to educate bicyclists, pedestrians, and drivers about safety and etiquette



Address Facilities and Infrastructure

- » Fill network gaps and address difficult intersection crossings and barriers to walking and biking circulation within the city
- » Find ways to make biking and walking safe, convenient, and easy
- » Define trail, sidewalk, and facility standards that are reasonable, feasible to build, and sustainable for the City of Watertown



Make Connections

Better connect residents and visitors to:

- » Schools
- » Community Destinations
- » Commercial Centers
- » Employment Centers
- » Neighborhoods
- » Medical Destinations
- » Parks



Prioritize and Implement

- » Define community goals and vision for the future of walking and biking in Watertown
- » Create a plan that will assist with future grant-writing and fund-seeking efforts
- » Identify projects that can be started in the near-term
- » Recommend strategies for phasing and implementation of the plan
- » Identify champions or stewards for projects or initiatives that can't be implemented by the City alone

PLANNING TIMELINE

 Winter 2025
Project Kick-off

 Spring 2025
Information Gathering

 Summer 2025
Public Involvement
• Open Houses
• Pop-up Events
• Advisory Group Meetings

 Fall 2025
Develop Recommendations

 Winter 2025 - 2026
Finalize the Plan

 Implement the Plan

WATERTOWN TODAY

Today, there are 6.4 miles of paved shared use paths, and 97.9 miles of sidewalks within the City of Watertown. As part of the planning process, a thorough review was conducted to build an understanding of the conditions and connectivity of the existing network of facilities (sidewalks, shared use paths, and on-street bikeways). The Downtown Core of Watertown is fairly well connected with a consistent sidewalk network; however many neighborhoods throughout the city do not have sidewalks. The bike network has grown over the last few years with targeted projects throughout the City. These short segments of trail lack connectivity with each other to create a fully linked network that takes people where they need to go.

Along with the physical inventory, a review of demographic information and previous planning efforts were also conducted to understand who will benefit most from the plan, as well as what decisions have already been made. Today, not very many people in Watertown choose to walk or bike to work or school. In contrast, nearly 9% of households in Watertown do not have access to a personal vehicle or cannot drive on their own.

To learn more about the analysis work, see **Chapter 2: Existing Conditions**.

Other Analysis Completed:

- » Roadway Functional Classification
- » Hydrology
- » Slope Analysis
- » Future Land Uses and Development
- » City Ordinance Review

of households in Watertown have no vehicles **Section 4, Item C.**

11%



Carpool to work



Work from home

0.5%

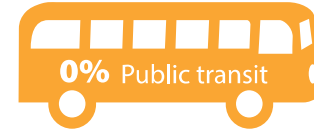


Bike to work



4%

Walk to work



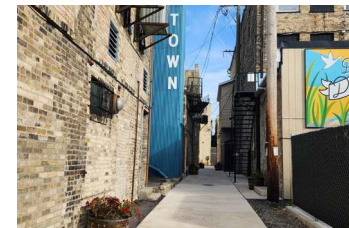
0% Public transit

77%



Drive to work alone

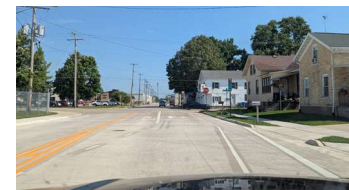
Transportation to Work in Watertown and Household Vehicle Ownership (2023 ACS Data)



Existing alleyway



Riverwalk



Existing bike lane



Existing sidewalk



Interturban Trailhead



Rock River Trail



Glacial River Trail

Figure 0.1 Existing Bicycle and Pedestrian Facilities in Watertown

COMMUNITY INPUT



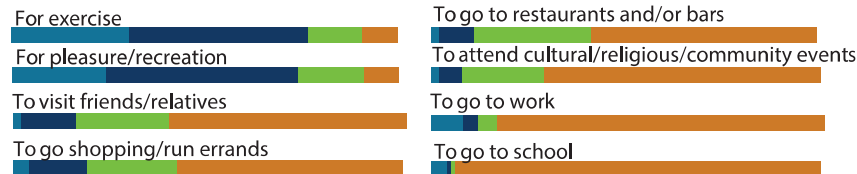
EVENTS

- » Bike Rodeo Pop-up
- » Riverfest Pop-up
- » Community Open House
- » Bike and Pedestrian Path Task Force + Community Advisory Committee Meetings
- » Stakeholder Meetings

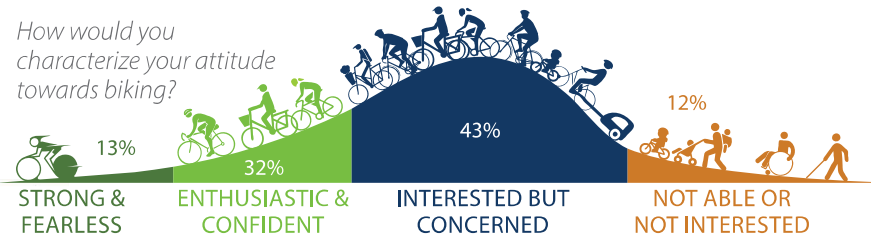
OUTREACH

Gathering feedback, ideas, and concerns from the community was an integral component to the creation of this plan. During the planning process, the City hosted a project website to share information about events, and linked to online survey tools. Information about the plan was shared through the Chamber of Commerce Newsletter, and direct invitations to events were sent via email to a growing list of community stakeholders throughout the project. See Chapter 3 for more details on the process and feedback collected.

During the warmer months of the year, how often do you walk or bike for the following reasons?



How would you characterize your attitude towards biking?



COMMUNITY SURVEY

A community survey was conducted over a 4-month period of time to gather information from the community about how they currently bike and walk, as well as what they'd like to see in the future.

371 responses gathered!

SOCIAL PINPOINT

Social Pinpoint is a map-based interactive online tool that allows participants to leave comments on a map of the city.

⚡ Top Concerns:

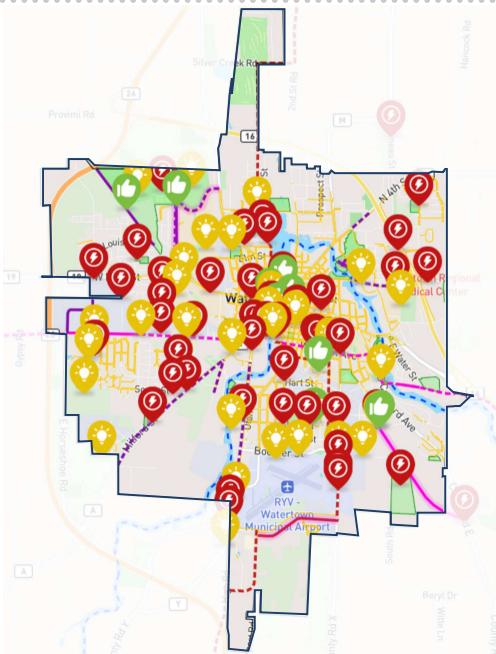
- » Crosswalk and intersection safety
- » Biking and walking along roadway shoulders
- » Need for bike lanes, bollards or separated trails
- » Lack of bike facilities Downtown
- » Biking on narrow bridges
- » Incomplete sidewalk connections

💡 Top Ideas:

- » Fill gaps in the existing network
- » Improve sidewalk conditions
- » Enforcement / etiquette / education for drivers and bikers
- » Do not allow parking in bike lanes
- » Increase amount of separated trails
- » Add protected bike lanes

👍 Places People Like:

- » Storywalk at Brandt-Quirk Park
- » Library
- » Interurban Trail and Trailhead
- » Riverwalk



! BARRIERS AND CHALLENGES

- » Lack of interconnected facilities that link to all areas of Watertown, as well as within the region.
- » Physical barriers, such as rail lines, highways, and the Rock River impact where facilities are easy or convenient to build.
- » Need for upgrades at specific intersections and mid-block crossings for safety.
- » Need for consistent, well-defined facilities for bicyclists to travel across the City.
- » One-way streets in downtown are difficult for bicyclists to navigate and impede connectivity.

★ KEY OPPORTUNITIES

- » Create a network of well-connected bikeways and sidewalks that make choosing to bike or walk to work, school, or other common destinations a viable option for most residents.
- » Celebrate the Rock River with improved connections to parks, the Riverwalk, and water access locations for biking and walking.
- » Create a network that is safe and comfortable with considerations for young people, families, seniors, and people who are visiting or new to Watertown.
- » Promote Watertown as a biking destination by highlighting existing regional trail and route connections that run through Watertown.
- » Develop and support programs for education and awareness around etiquette for all roadway users.

NETWORK PLAN

The Network Plan shows the existing and proposed bicycle and pedestrian facilities to build out a well-connected network in the future. Each facility type in the proposed Network Plan is detailed in the **Facility Design Guidance** Section 4, Item C. 4. Additionally, Chapter 4 contains specific guidance on intersection treatments.

A VISION FOR THE FUTURE

A set of goals were developed to guide the future bicycle and pedestrian network.



Goal 1

Increase the opportunities for residents and visitors of Watertown to choose multimodal transportation to travel throughout the city to get to school, work, or run daily errands.



Goal 2

Provide comfortable routes to connect all areas of Watertown for pedestrians and bicyclists of all ages and abilities, including seniors, young people, and people who depend on mobility devices.



Goal 3

Provide appropriate facilities for pedestrians and bicyclists with consideration for data and context that support improvements.



Goal 4

Prioritize connections for biking and walking to the Rock River, schools, parks, businesses, churches, community centers and libraries, retail, and medical locations, as well as regional destinations outside of Watertown.



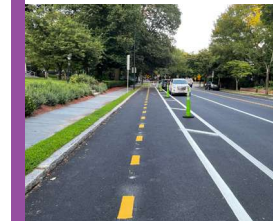
Figure 0.2 Network Plan

Shared Use Paths



Shared use paths are paved trail segments, located off-street and physically separated from vehicle traffic by a boulevard and raised curb. They are generally located in parallel to a roadway but can also be located independently.

Two-Way Protected Bike Lanes



There are three segments within downtown Watertown where **two-way protected bike lanes** are proposed. This facility provides a single area for bicyclists to travel within the road (in both directions) with a physical barrier that separates vehicle traffic from bicycle traffic.

Bike Lanes



Bike lanes are designated areas within the roadway for bicycle travel. Generally, bicycle lanes are located on both sides of the roadway and are designated with a solid white stripe marking with bike lane markings at intersections.

Neighborhood Bikeways



Neighborhood bikeways are proposed areas with shared lane markings (also known as sharrows) to indicate an on-street bicycle facility that overlays with an existing vehicle travel lane.

Sidewalks



Proposed **sidewalks** are shown on the Network Plan in primarily residential areas where none exist today. Sidewalks are concrete walkways intended for use by pedestrians.

SYSTEM RECOMMENDATIONS

Chapter 5: System Recommendations provides detailed guidance on dimensions, materials, applicability, and placement for trailheads and trail access points, bike parking, wayfinding, bridge and railroad crossings, and best management practices for landscaping and stormwater management as the network plan is implemented.

TRAIL ACCESS POINTS

Trail access points are designated public access locations for a trail or path, often located at the endpoints of a path as well as at key locations along the way. The map to the right outlines ten trail access point locations within Watertown, most of which are in alignment with existing trailheads, parks, or open spaces. Many of these access points are within existing City parks that have some on- or off-street parking available currently. As the network is built-out, consideration for upgraded amenities at the trail access points should be folded into the design.

Below are potential amenities to consider at trail access points. **Note that not every amenity listed below is required at every location.** A combination of amenities should be considered that fit the existing context, available space, and need. **Table 5.1 on page 60** outlines suggested amenities at each trail access location.

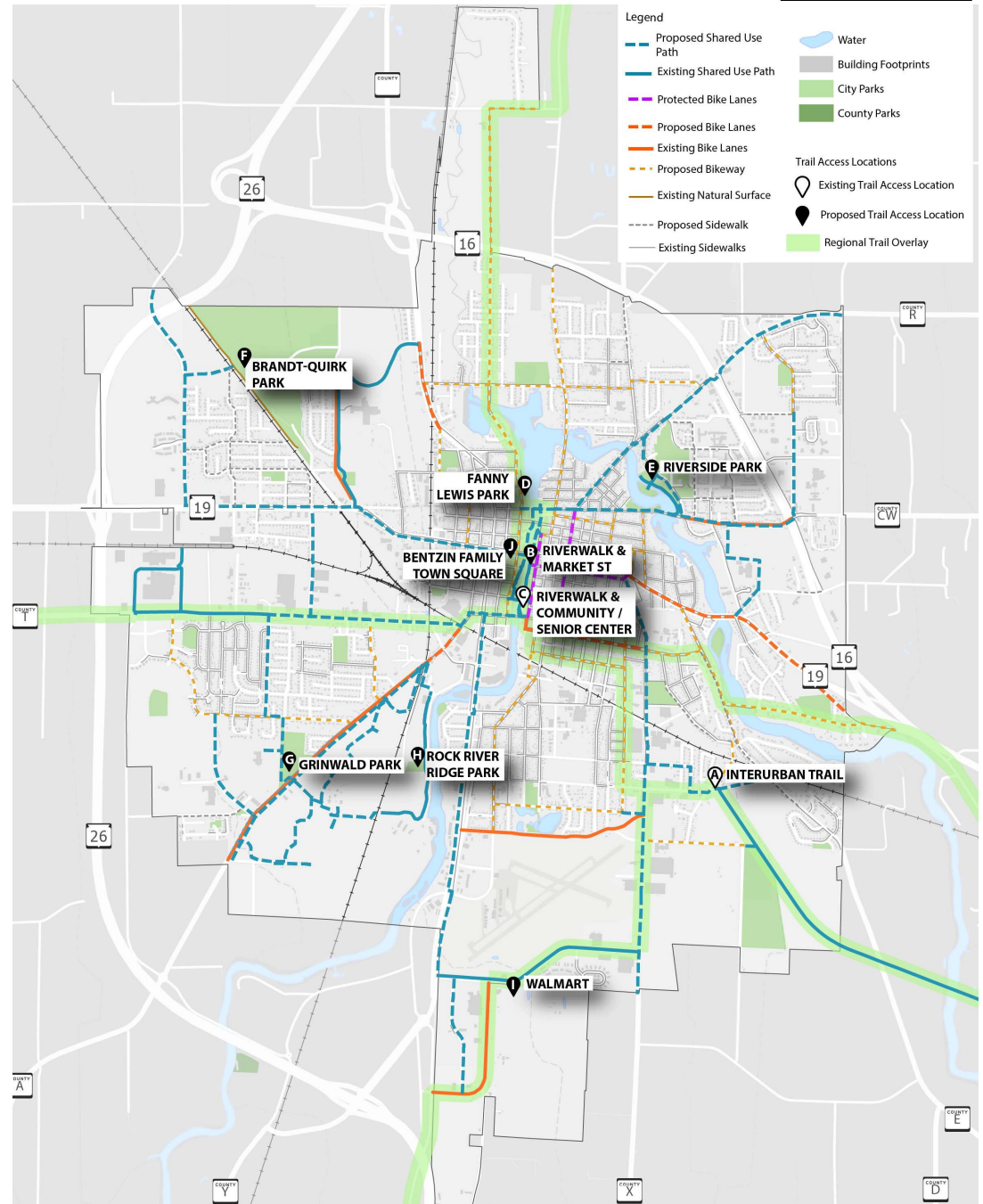
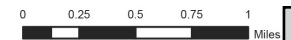


Figure 0.3 Trail Access Plan



WAYFINDING

Clear, consistent, and visible wayfinding is an essential component for a bike and pedestrian network. Wayfinding helps people navigate throughout a system, utilizing a variety of signage types and, in some cases, with online mapping. A Wayfinding Framework has been developed for Watertown, which outlines a series of signage types linked to the facility types shown in the Network Plan. **It is recommended that the City develops a full wayfinding and signage plan with design and construction-level graphics that can be implemented consistently throughout the City as new trail, sidewalk, and bikeway projects are constructed.**

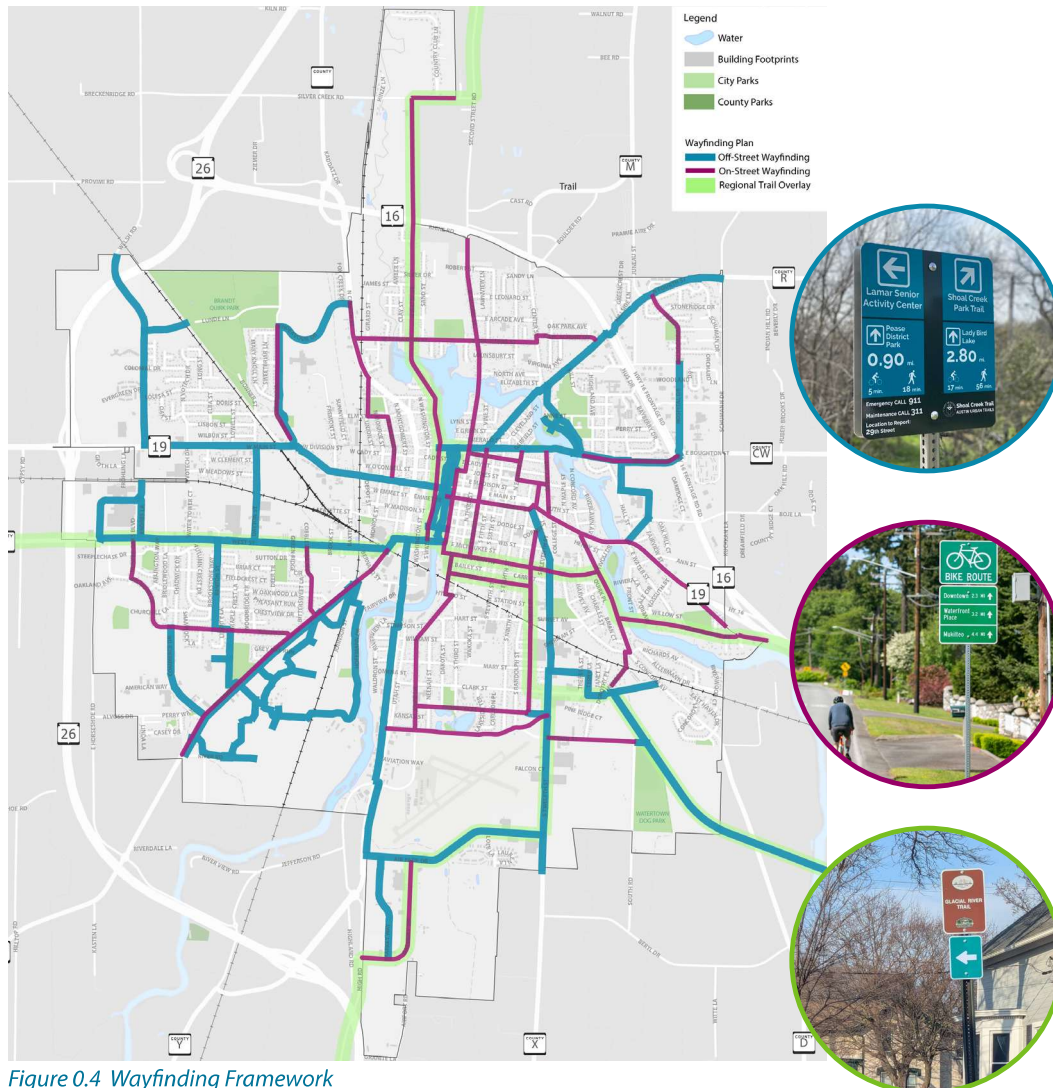


Figure 0.4 Wayfinding Framework

See **Chapter 5: System Recommendations** for additional guidance related to :

- » Bike Parking
- » Railroad Crossing
- » Bridge Crossings and Other Constrained Areas
- » Landscaping and Stormwater Management

See **Chapter 6: Policy Recommendations** for guidance related to:

- » Subdivision Code Considerations
- » E-Bike and E-Scooter Policies

Off-Street Wayfinding (Shared Use Path Network)

- » Directional signage at trail access points and at decision points, with approximate distances called out to nearby community destinations.
- » Trail crossing signage at intersection crossings.
- » Directional signage at alleyways that have been improved for pedestrian use throughout Downtown Watertown.

On-Street Wayfinding (Bikeways and Bike Lanes)

- » BIKE ROUTE and BIKE LANE signage along routes, visible to vehicles and meeting MUTCD standards.
- » SHARE THE ROAD signs, visible to vehicles and meeting MUTCD standards.
- » Directional signage at decision points with approximate distances called out to nearby community destinations.

Regional Trail Overlays

- » Branded signage consistent with County or Regional Trail branding.
- » Additional signs or badges to accompany the On- and Off-Street Wayfinding

IMPLEMENTATING THE PLAN

This plan will require the City and partner organizations to stay connected and focused on the goals, ready as opportunities and funding comes along to implement. Four Implementation Recommendations (below) provide guidance on how to build and keep momentum for the plan. A detailed Phasing Plan has been developed, with prioritized projects identified, alongside lower-lift projects to be implemented incrementally over time. See **Chapter 7: Implementation** for a full set of tables outlining projects, costs, priorities, and costs.

Additional information on Operations and Maintenance and Grant Funding Sources are in Chapter 7.

1 Bicycle & Pedestrian Path Task Force

The Bicycle and Pedestrian Path Task Force should **continue to meet and work in tandem with City staff to support implementation of the plan**, evaluation of projects and priorities on an annual basis, education and community engagement, coordination with regional and agency collaborators, and assisting the City with grant and fund-seeking efforts.

3 Evaluation, Monitoring & Tracking

- The following actions are recommended to keep the City's information up-to-date and accessible as the network develops:
- » Continue to **maintain a GIS geodatabase** with existing bicycle and pedestrian facilities. As facilities are built, update this information, along with reviewing inventory on an annual basis.
 - » Continue to **conduct annual or multi-year condition assessments** of trails, sidewalks, and on-street facilities, in order to budget for annual maintenance.
 - » **Develop a public-facing ArcGIS Online map** that shows existing trails, sidewalks, and bikeways.
 - » **Collect annual user counts** on trails and on future pilot projects.
 - » **Conduct a community-wide survey** on an annual or biennial basis. This way, information can be compared year-over-year to understand if concerns are being addressed and to help prioritize projects for inclusion in the City's CIP.

2 Agency & County Coordination

Implementation of many projects will require ongoing coordination and collaboration with WisDOT, Dodge County, Jefferson County, Watertown School District, and other local agencies and non-profit organizations. **Convening annual or quarterly meetings with City, agency and County staff to review potential project collaborations is recommended.**

4 Encouragement & Education

- Programs and initiatives are recommended to promote a culture of biking and walking in Watertown, as well as to educate and inform residents about best practices:
- » Continue to **support the Watertown Police Department's annual Bicycle Rodeo and educational events** to teach young people (and adults!) how to safely maneuver a bicycle and learn the rules of the road.
 - » Develop a **guidebook of the City's general bicycle, pedestrian and driver etiquette** and rules of the road ordinances.
 - » Develop a program to **share in the cost for installing city-branded bike racks at businesses** throughout Watertown.
 - » **Develop a city-wide wayfinding plan** with specific brand design for directional signage for the On- and Off-Street Network and Regional Overlays.
 - » Re-apply to be certified as a **Bicycle Friendly Community** through the League of American Cyclists.

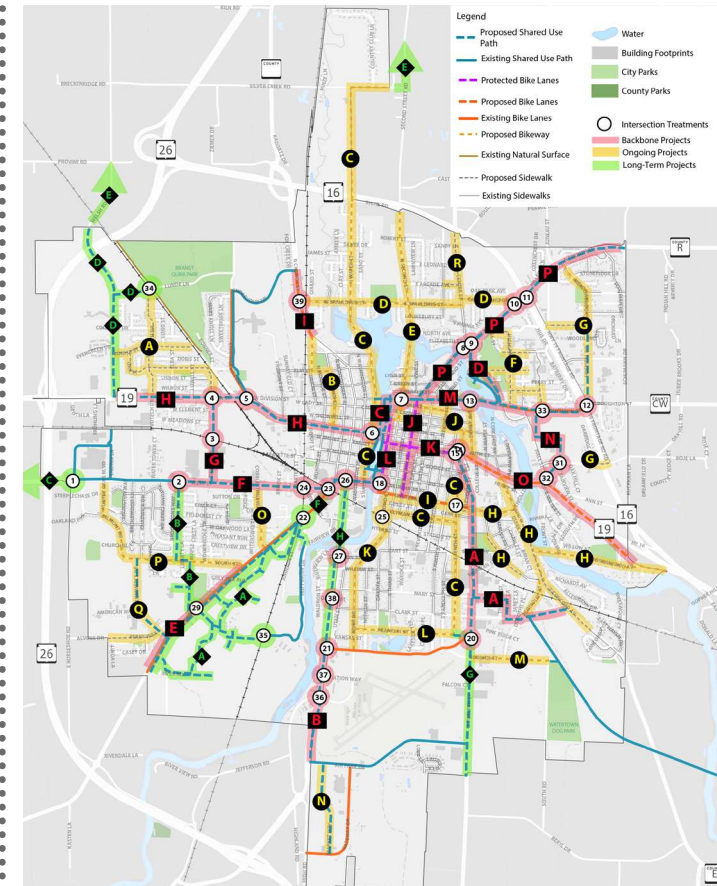


Figure 0.5 Phasing Plan

Backbone Projects are generally larger projects that have been identified as vital to create a well-connected network.

Ongoing Projects are generally smaller, lower-cost projects or projects that can be more easily folded into the City's Capital Improvement Program.

Long-Term Projects will generally require more planning and collaboration with regional agencies to implement or are intended to be initiated and completed as part of future.

Watertown Draft Bicycle and Pedestrian Network Plan

PUBLIC WORKS COMMISSION

3/10/2026



Project Intro

» Plan Purpose

- 1 To create a plan that addresses the future of a city-wide bicycle and pedestrian network, building off the existing network.
- 2 To gather and incorporate community feedback and desires to address safety and a future vision for active transportation in Watertown.
- 3 To identify best practices and guide the City in future facility and network investments.
- 4 To guide policy and programming improvements, such as wayfinding, bike parking, and educational and encouragement activities.
- 5 To build awareness of and enthusiasm for expanding multimodal transportation opportunities in and around Watertown.

» Plan Objectives



Address Facilities and Infrastructure



Encourage and Educate



Address a Variety of Users



Make Connections



Prioritize and Implement

PLAN TIMELINE Section 4, Item C.

- Winter 2025
Project Kick-off
- Spring 2025
Information Gathering
- Summer 2025
Public Involvement
 - Open Houses
 - Pop-up Events
 - Advisory Group Meetings
- Fall 2025
Develop Recommendations
- Winter 2025 - 2026
Finalize the Plan
- Implement the Plan

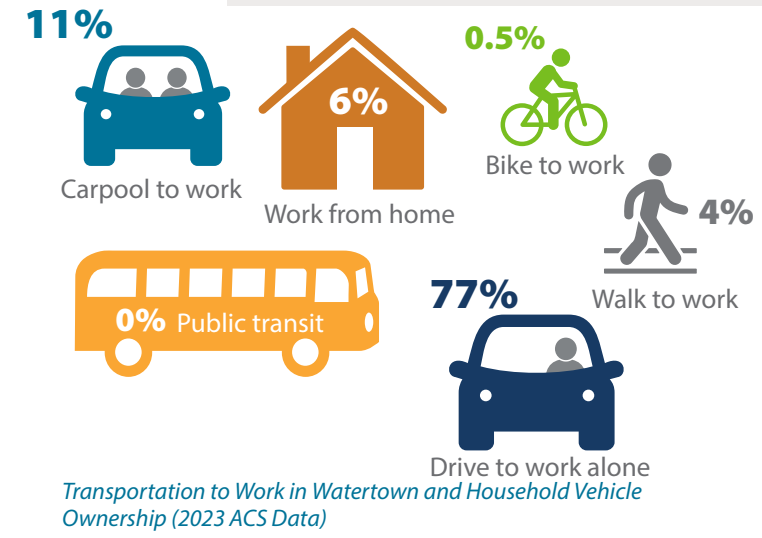
Existing Conditions

REVIEW + ANALYSIS

- » Demographics - Focus on Travel Behavior
- » Relevant Plan Review
- » Existing Facility Inventory and Review for Future Facilities
- » Regional Connections
- » Traffic Safety and Roadway Information
- » Hydrology, Topography, Land Use
- » City Ordinance Review



8.5% of households in Watertown have no vehicles **Section 4, Item C.**



Community Engagement

- » Outreach and Communications
- » Pop-up Events
- » Community Open house
- » Bike & Ped Task Force + Community Advisory Committee
- » Stakeholder Engagement
- » Community Survey
- » Social Pinpoint

WHAT WE HEARD



Section 4, Item C.

BARRIERS AND CHALLENGES

- » Lack of interconnected facilities that link to all areas of Watertown, as well as within the region.
- » Physical barriers, such as rail lines, highways, and the Rock River impact where facilities are easy or convenient to build.
- » Need for upgrades at specific intersections and mid-block crossings for safety.
- » Need for consistent, well-defined facilities for bicyclists to travel across the City.
- » One-way streets in downtown are difficult for bicyclists to navigate and impede connectivity.

KEY OPPORTUNITIES

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- » Celebrate the Rock River with improved connections to parks, the Riverwalk, and water access locations for biking and walking.
- » Create a network that is safe and comfortable with considerations for young people, families, seniors, and people who are visiting or new to Watertown.
- » Promote Watertown as a biking destination by highlighting existing regional trail and route connections that run through Watertown.
- » Develop and support programs for education and awareness around road etiquette for all roadway users.

Network Plan

FUTURE CONNECTIVITY

Neighborhood Bikeways



Neighborhood bikeways are proposed areas with shared lane markings (also known as sharrows) to indicate an on-street bicycle facility that overlays with an existing vehicle travel lane.

Sidewalks



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Shared Use Paths



Shared use paths are paved trails located off-street and physically separated from vehicle traffic by a boulevard and raised curb. They are generally located in parallel to a roadway but can also be located independently.

Section 4, Item C.

Two-Way Protected Bike Lanes



There are three segments within downtown Watertown where **two-way protected bike lanes** are proposed. This facility provides a single area for bicyclists to travel within the road (in both directions) with a physical barrier that separates vehicle traffic from bicycle traffic.

Bike Lanes



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Facility Design Guidance

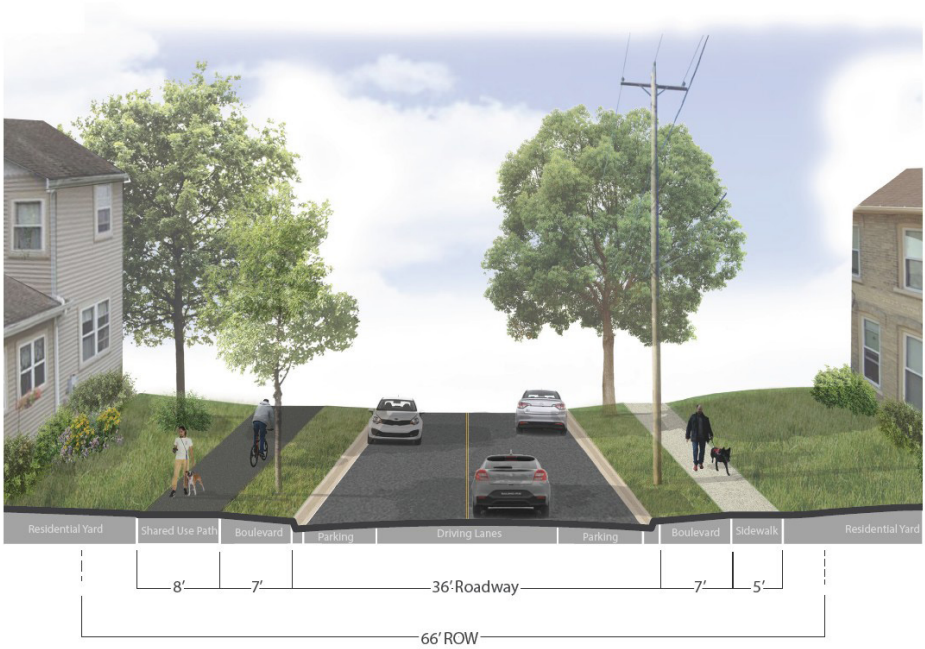


Figure 4.2 Cross section and plan view study of a shared use path along Fourth St.



Figure 4.3 Cross section and plan view study of two-way protected bike lane on Market St.

Intersection Treatment Recommendations

INTERSECTION TREATMENTS



High Visibility Crosswalk Markings: Pavement marking patterns at crosswalks that are visible to both the driver and pedestrian from farther away. Recommended pattern: continental.



Bike Crosswalks: Pavement marking patterns at crosswalks that are specific for bicycle travel and usually painted green.



Raised / Tabletop / Speed Table Crosswalks: Crosswalk design that maintains the sidewalk or path elevation for the pedestrian, providing more visibility to motorists.



Pedestrian Ramps: Concrete ramps designed for ADA accessibility to connect sidewalks and paved paths to roadway intersections. Detectable warning surfaces with truncated domes are recommended to be included in pedestrian ramp design.



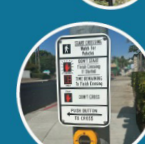
Rapid Flashing Beacon (also known as RRFB – Rectangular Rapid Flashing Beacon): Active warning beacon that uses rapid, irregular flashes of light to alert motorists to yield to pedestrians and bicyclists at crossings and intersections.



Curb Extensions: Sometimes called bump-outs, curb extensions narrow the roadway at intersections and mid-block crossings to shorten crossing distances for pedestrians and make pedestrians more visible to drivers. Curb extensions also provide an opportunity to integrate landscaping, stormwater management, or site furnishings into the streetscape.



Trail Crossing Sign: Diagonal yellow sign alerting motorists that a trail crossing is nearby.



Share the Road Sign: Signage reminding motorists to share the road with bicyclists. This type of signage pairs with on-street bikeways.



Grade Separated Crossing: A bridge or a tunnel dedicated for pedestrian or bicycle crossing of a busy roadway. These treatments require significant space allocation and are expensive to build; however they provide the most separation for pedestrians and bicyclists at crossings.



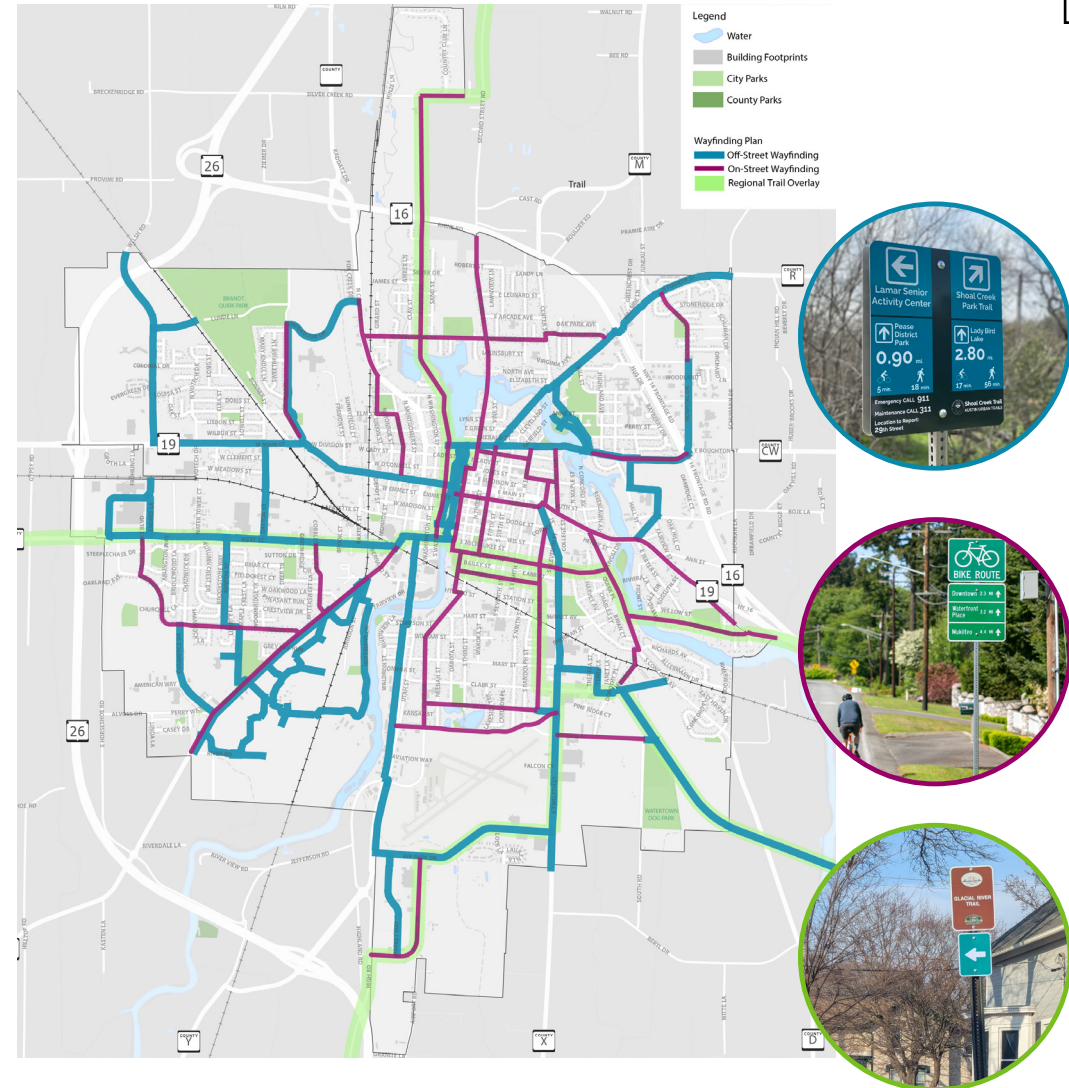
Pedestrian Refuge Island: Protected island, usually made with a raised concrete curb, centered on a crosswalk to shorten the distance that pedestrians are exposed to traffic.



Section 4, Item C.

System Recommendations

- » Trail Access Points and Amenities
- » Bike Parking
- » Wayfinding
- » Railroad Crossings
- » Bridge Crossings and Constrained Areas
- » Landscaping and Stormwater Management



Policy Recommendations

- » Subdivision Code Suggested Revisions
- » Definitions to include Electric Bicycles, Classes, Electric Personal Assistive Mobility Devices, Electric Scooters and Non-Classified Bicycles and Scooters
 - Recommendations for speed limits on sidewalks (15mph) and shared use paths (20mph)
 - Limit all bicycle, e-bike and e-scooter use on sidewalks
 - Consider helmet requirements
 - Case study examples from other WI communities

SECTION 13. 346.806 SPECIAL RULES APPLICABLE TO ELECTRIC BICYCLES.

- An electric bicycle shall be considered a vehicle to the same extent as a bicycle.
- A person operating an electric bicycle is not subject to the provisions relating to financial responsibility, operator's licenses, registration, or certificates of title.
- No person under the age of 16 years may operate a class 3 electric bicycle.
- A person under the age of 16 years may ride as a passenger on a class 3 electric bicycle that is designed to accommodate passengers.

OTHER WISCONSIN STATE STATUTES

- Within the e-bike market, in addition to the three classes of e-bike noted above, there are also unclassified/classless e-bikes available for sale. Other e-bikes that don't fall under any class in the system (for example, those without pedals, those with a throttle that can assist up to 28 mph, and those with motors over 750W). As they fall outside of the three classifications, these bikes are still in use but not considered e-bikes by the state of Wisconsin.
 - With all e-bikes, but especially with unclassified bikes, fires can start when e-bike and e-scooter batteries are stored, charged, or disposed of incorrectly
- Wisconsin allows local municipalities to enact stricter rules concerning e-bike usage.
- Electric bicycles used without the motor engaged are allowed on all bicycle trails unless specifically prohibited.
 - NO e-bike capable of more than 30 MPH is permitted on any state trails that do not specifically allow motorcycles (these would technically not be considered e-bikes by state law).
- Signage is used in many places rather than, or in addition to, policy to identify specific locations where some types of e-bikes are not allowed (ex. sign stating that no e-bikes with a throttle are allowed on a trail)
- E-bike policy can be difficult to enforce due to the difficulty of identifying the type of e-bikes without close inspection.

78 WATERTOWN BICYCLE AND PEDESTRIAN NETWORK PLAN

Implementation

FROM PLAN TO REALITY

» Implementation Recommendations

- Bicycle and Pedestrian Path Task Force
- Evaluation, Monitoring and Tracking
- Agency and County Coordination
- Encouragement and Education

» Prioritized Phasing Plan

» Operations and Maintenance

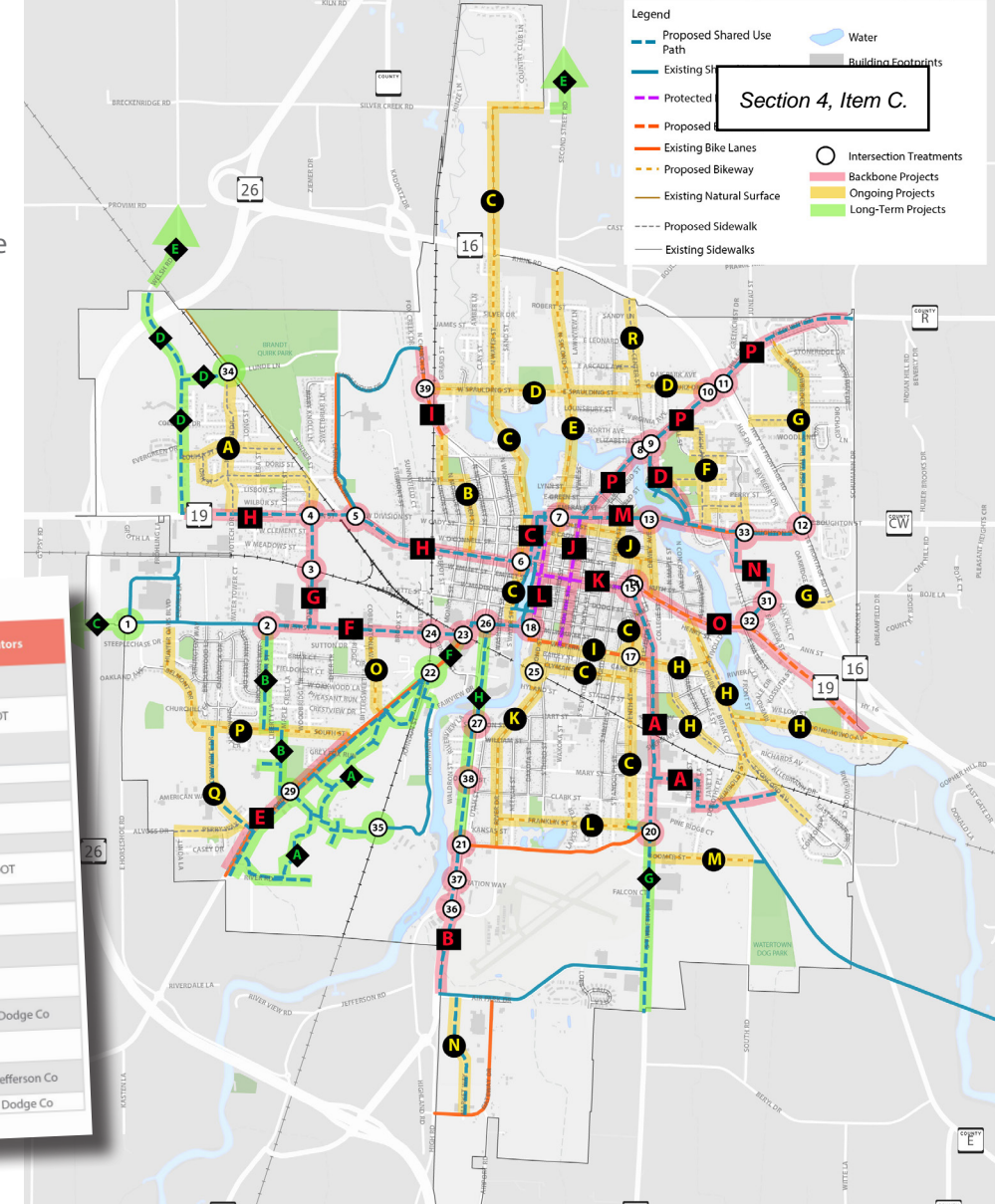
» Funding Sources

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Long-Term Projects will generally require more planning and collaboration with regional agencies to implement or are intended to be initiated and completed as part of future.

BACKBONE PROJECTS								
Tag	Location	Extents	Planned Facility	Project Type	Length	Unit Cost*	Estimated Cost	Collaborators
A	12th St / Interurban Trail	Interurban Trail to Market St	Shared Use Path	New Facility	9961	\$80	\$796,880	
B	S Church St	Air Park Dr to Boomer St	Shared Use Path	New Facility	1840	\$80	\$147,200	WisDOT
C	Riverwalk Extension	Rock River (east and west sides)	Shared Use Path	New Facility	2418	\$80	\$193,440	
D	Labaree St	Fourth St to Division St	Shared Use Path	New Facility	4555	\$35	\$159,425	
E	Millford St	Western City Limits to South St	Sidewalk	New Facility	6354	\$80	\$508,320	
F	West St / Milwaukee St	Benton Ln to Milford, Milwaukee St to 1st St	Shared Use Path	New Facility	2815	\$80	\$225,200	
G	Dayton St	West St to Main St	Shared Use Path	New Facility	9266	\$80	\$741,280	WisDOT
H	W Main St	Welsh St to S 1st St	Sidewalk to SUP Conversion	New Facility	2325	\$20	\$46,500	
I	N Church St	Endeavour Dr to Union St	Bike Lanes	New Facility	3244	\$60	\$194,640	
J	Fourth St	Western Ave to Division St	Two-Way Protected Bike Lanes	New Facility	2617	\$60	\$157,020	
K	Market St	Riverwalk Trailhead to Sharp Corner Park	Two-Way Protected Bike Lanes	New Facility	2172	\$60	\$130,320	
L	First St	Western Ave to Jones St	Two-Way Protected Bike Lanes	New Facility	5834	\$80	\$466,720	WisDOT, Dodge Co
M	Division St / Boughton St	Fourth St to SH 16	Shared Use Path	New Facility	3563	\$80	\$285,040	
N	Riverside Middle School	Main St to Boughton St	Shared Use Path	New Facility	7020	\$20	\$140,400	WisDOT, Jefferson Co
O	E Main St / Summit Ave	Sharp Corner Park to Oconomowoc Ave	Bike Lanes	New Facility	9565	\$80	\$765,200	WisDOT, Dodge Co
P	Fourth St	Division St to Schuman Dr	Shared Use Path	Conversion			\$5,287,585	



THANK YOU!

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jody@hkgi.com

ACKNOWLEDGMENTS

WATERTOWN STAFF

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Nathan Williams, Staff Engineer
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Lisa Famularo, Media and Communications Director
Evan Hisey, Media Productions Manager
Carol Quest, Health Officer/Director
Deb Sybell, Manager of Economic Development and Strategic Initiatives

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Jonathan Lampe, District 2
Ken Berg, District 3
Dan Bartz, District 4
Brad Blanke, District 5
Fred Smith, District 6
Tony Arnett, District 7
Bob Wetzel, District 8
Myron Moldenhauer, District 9

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Jason Widiker
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Carol Quest, Health Officer/Director
Bob Wetzel, Alder
Fred Smith, Alder
Rod Laudenslager
Robert Stocks, Mayor
Andrew Beyer, PW Director / City Engineer
David Brower, Police Chief
Maureen McBroom, Stormwater Project Manager

COMMUNITY ADVISORY GROUP

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Bill Ehlenbeck, Dodge County Director of Land Resources and Parks
Kevin Wiesman, Jefferson County Director of Parks
Jarred Burke, Watertown Unified School District Superintendent

PUBLIC WORKS COMMISSION

Brad Blanke, Alder
Dan Bartz, Alder
Myron Moldenhauer, Alder
Tony Arnett, Alder and Chair
Pete Thompson, Citizen Member

WATERTOWN MAYOR

Robert Stocks

CONSULTANT TEAM



Approved by the City of Watertown on _____.

WATERTOWN BICYCLE + PEDESTRIAN NETWORK PLAN

FINAL DRAFT February 2026



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Watertown Mission:

To provide for, protect, and serve the citizens and businesses of Watertown in an efficient, strategic, and measured manner, while creating a community culture where close knit connections are key, that is rich in small town values balanced with modern conveniences, that is poised for development, and is an idyllic community that leverages location and outdoor opportunity.

Section 4, Item C.



ACKNOWLEDGMENTS

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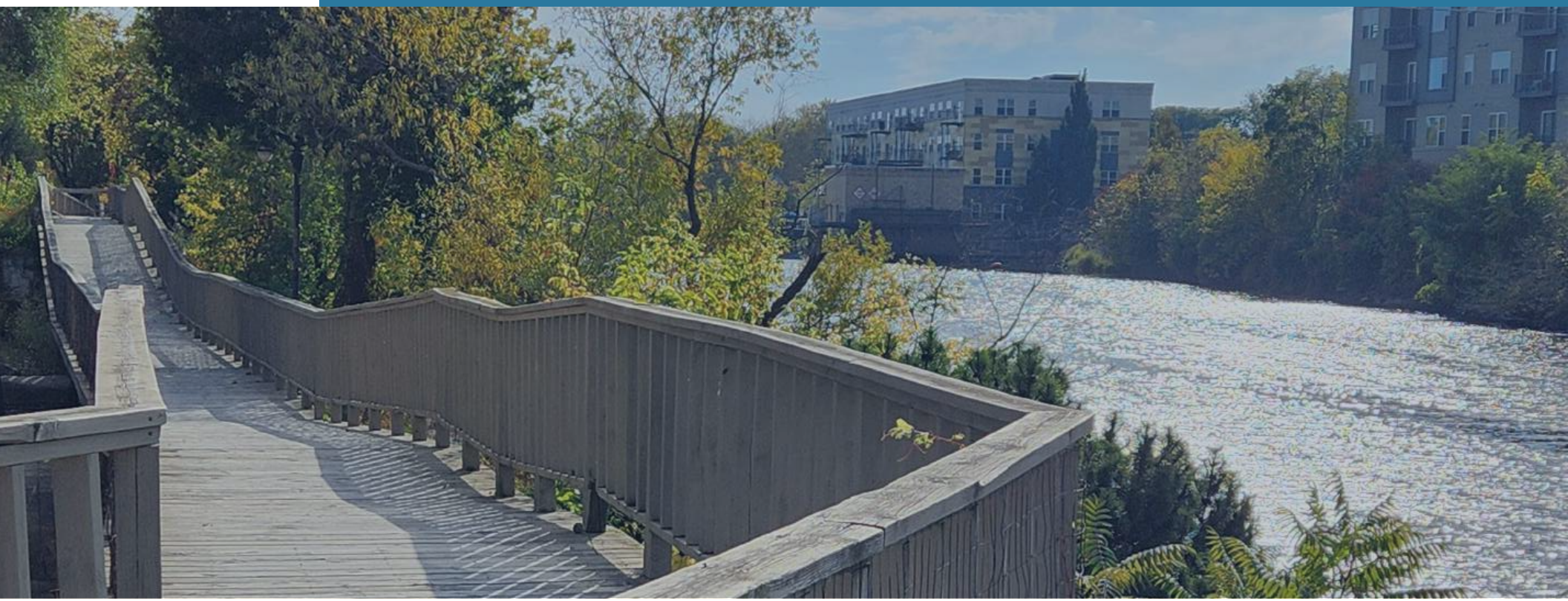
WATERTOWN MAYOR

Robert Stocks

CONSULTANT TEAM



Approved by the City of Watertown on _____.



INTRODUCTION

1

PLAN & PROJECT PURPOSE

The City of Watertown has been incrementally building a network for pedestrians and bicyclists over the last 20+ years. Through collaborative efforts across multiple City departments (Public Works, Parks, Recreation & Forestry), along with coordination with both Jefferson and Dodge Counties, projects such as the Interurban Trail have expanded the City's options for walking and biking, both as a means of transportation and for recreation. Over the last few years, it has become apparent that a long-range plan is needed to clearly create a guide to develop a fully-connected network, which the City can work to implement in phases. A long-term implementation plan like this is ambitious and will require continued collaboration across City departments, support from the City Council, and stewardship from non-profit and advocacy organizations.

The purpose of this planning process is multi-faceted:

- 1 To create a plan that addresses the future of a city-wide bicycle and pedestrian network, building off the existing network.
- 2 To gather and incorporate community feedback and desires to address safety and a future vision for active transportation in Watertown.
- 3 To identify best practices and guide the City in future facility and network investments.
- 4 To guide policy and programming improvements, such as wayfinding, bike parking, and educational and encouragement activities.
- 5 To build awareness of and enthusiasm for expanding multimodal transportation opportunities in and around Watertown.

CREATING CONNECTIONS

In Watertown, **8.5% of residents do not have a vehicle available for their household.** Bicycle and pedestrian networks provide alternative ways for people who do not have access to vehicles to reach destinations.



WHY PLAN FOR BIKING AND WALKING TODAY?

Over time, the purposes for biking and walking have changed evolved. Walking is the original form of transportation and has always been the most economical and environmentally friendly way of getting around (albeit potentially the slowest means of transportation!). The introduction of the bicycle in the early 1800s carried many of the same benefits and allowed people to travel more efficiently, faster, and over longer distances. After the advent of the personal automobile and its increased popularity in the 1920's and 1930's, biking and walking for transportation purposes began to decline as it was seen mostly for sport, exercise, or recreation. As cities became more sprawling with suburban development, distances grew between homes, places of work, schools, and other community destinations. Today, the car is the most dominant means of transportation, and many people depend solely on their personal vehicle for all trips.

Concurrently, today biking and walking have seen a resurgence in popularity as people recognize the importance of living an active and healthy lifestyle. Also, the modern realities of the costs of owning and maintaining a personal vehicle are high and many people cannot afford it. People who cannot or choose not to drive (seniors, people with disabilities, young people) are seeking more ways to independently get from one place to another. Even in communities with great public transportation networks, we have to remember that we are always pedestrians at the beginning and end of any trip we take, even if just walking through a parking lot.



The following are some key reasons for planning for the future of biking and walking in Watertown:



» **Safety:** Evidence suggests that cities with high levels of bicycle mode share are not only safer for bicyclists, but also for all road users. Further, bike facilities themselves can act as “calming” mechanism for traffic, slowing cars and reducing fatalities.



» **Livability + Mobility:** Fifty percent of US residents say that walkability is a top priority or high priority when considering where to live. Bicycling has become the country's fastest growing form of transportation for commuters.



» **Health:** Walking and biking can improve a variety of health outcomes for both mental health and physical health, benefitting individuals and the broader community. For example, **obesity costs the State of Wisconsin around \$598 million in annual healthcare costs**. Improving conditions for biking and walking ensures that all community members have access to low-cost methods for exercise and getting outdoors.



» **Household and Community Prosperity:** Not all households can afford to own a car and some lower-income households who do own a car may be sacrificing other basic needs to pay for insurance, gas, and maintenance. *In Watertown, 8.5% of residents do not have a vehicle available for their household.* Bicycle and pedestrian networks provide alternative ways for people who do not have access to vehicles to reach destinations, including community destinations like the library, swimming pool, or parks, and commercial destinations like movie theaters and shopping centers.



» **Air Quality and Greenhouse Gas Emissions:** *The transportation sector generates a large share of greenhouse gas emissions (28.9% as of 2017).* Walking or cycling could substitute many short car trips (less than 3 miles), significantly reducing CO2 emissions from car travel.



» **Recreation:** Trails, bikeways, and sidewalks provide facilities for recreational purposes that serve a wide range of the community population to connect to parks and natural areas.



» **Parking and Transportation Networks:** As more people bike and walk to retail, restaurants, entertainment, work and school destinations, the demand for vehicle parking decreases, which allows for urban spaces to be developed and utilized more compactly. Encouraging active transportation and providing safe routes for pedestrians and bicyclists alleviates traffic congestion as well.



» **Regional and Economic Competitiveness / Tourism:** Communities with strong and active bicycle and pedestrian networks support local businesses and workers. Employee retention, increasing the draw for residents to the Watertown area, and economic development are all potential outcomes of a successfully implemented plan for biking and walking.



PLAN OBJECTIVES

Early in the planning process, the following objectives were identified to holistically address bicycle and pedestrian issues within the plan. The Plan Objectives focus on physical infrastructure improvements, programs, and policies.



Facilities and Infrastructure

- » Fill network gaps and address difficult intersection crossings and barriers to walking and biking circulation within the city
- » Find ways to make biking and walking safe, convenient, and easy
- » Define trail, sidewalk, and facility standards that are reasonable, feasible to build, and sustainable for the City of Watertown



Address a Variety of Users

Define a cohesive, connected walking and biking network in Watertown that meets the needs of:

- » Commuters
- » Recreational bicyclists and hikers
- » People with disabilities
- » People who don't have the ability or interest in driving
- » People who use public transportation
- » Children and young people
- » Seniors
- » Families
- » Visitors



Make Connections

Better connect residents and visitors to:

- » Schools
- » Community Destinations
- » Commercial Centers
- » Employment Centers
- » Neighborhoods
- » Medical Destinations
- » Parks



Project Timeline

Section 4, Item C.

In 2024, the City of Watertown received a Transportation Alternative Program (TAP) grant from the Wisconsin Department of Transportation (WisDOT) to develop a City-wide Bicycle and Pedestrian Plan. The grant provided 80% of the cost to complete the plan and the City of Watertown matched the remaining 20%. After soliciting proposals from qualified planning firms, the City contracted with HKGi, a private consulting firm with expertise in multimodal planning. The project commenced in January of 2025 and was organized into five major tasks. Public involvement occurred throughout the planning process (see Chapter 3 for more information). The City of Watertown approved the final plan in 2026.



Encourage and Educate

- » Identify programs and initiatives to support biking and walking in Watertown
- » Promote ways to make passive forms of exercise, such as biking and walking, a part of daily routines for residents
- » Develop or identify programs or initiatives to educate bicyclists, pedestrians, and drivers about safety and etiquette



Prioritize and Implement

- » Define community goals and vision for the future of walking and biking in Watertown
- » Create a plan that will assist with future grant-writing and fund-seeking efforts
- » Identify projects that can be started in the near-term
- » Recommend strategies for phasing and implementation of the plan
- » Identify champions or stewards for projects or initiatives that can't be implemented by the City alone

Winter 2025



Project Kick-off

Spring 2025



Information Gathering

Summer 2025



Public Involvement

- Open Houses
- Pop-up Events
- Advisory Group Meetings

Fall 2025



Develop Recommendations

Winter 2025/2026



Finalize the Plan

Implement the Plan





BICYCLE AND PEDESTRIAN PATH TASK FORCE + COMMUNITY ADVISORY GROUP

The City of Watertown has a standing advisory committee that is dedicated to identifying and implementing bicycle and pedestrian programmatic and infrastructure projects, titled the Bicycle and Pedestrian Path Task Force. This group was formed in 2016 and meets quarterly. The committee is comprised of residents who are interested in advocating for biking and walking, along with the City's Health Officer, a few City Alders, the Mayor, the Public Works Director, the Police Chief, City Engineers, and the City's Parks, Recreation & Forestry Director.

For the duration of the planning process to develop the City-wide Bicycle and Pedestrian Plan, the Bicycle and Pedestrian Path Task Force was joined by additional stakeholder representatives, such as Dodge and Jefferson County Parks and Recreation staff, as well as Watertown School District administrators. The broader committee was named the Community Advisory Group, although colloquially the committee was referred to as the 'Task Force' throughout the planning process.

The Community Advisory Group served as the lead committee to steer the direction of the plan and met five times over the course of the project.

Member roles included:

- » Provide technical and organizational guidance
- » Serve as decision-makers for the plan
- » Provide guidance on preferred recommendations
- » Review draft materials
- » Recommend the plan to the City's Public Works Commission
- » Support engagement and outreach efforts

EXISTING CONDITIONS

2

WATERTOWN IN CONTEXT

LOCATION AND DESCRIPTION OF AREA

Watertown is located in southern Wisconsin and straddles the border of Jefferson County and Dodge County. It is approximately 35 miles east of Madison and 45 miles west of Milwaukee. The city has an area of approximately 12.5 square miles. A key feature shaping the city's physical geography and historical development patterns is the Rock River, which runs through the city. The river first flows north where it crosses into the city from the southeastern side, then flows south as it passes through downtown and out the southwestern boundary of the city.

The City of Watertown developed due to the Rock River and its potential for power. Early industry evolved around sawmills that used the river's power, later the construction of two hydroelectric dams along the Rock River and construction of rail lines through Watertown cemented early economic development in the City.





American Community Survey

ACS DATA

HOW IT WORKS

The American Community Survey (ACS) is an on-going survey that provides data every year, providing information between decennial census years. The 5-year ACS estimates are based on larger data samples than the 1-year ACS estimates and are more accurate for smaller areas, which is why this report references the 2023 5-year estimates, which is the most recent 5-year estimate at the time of writing. On the Map is a census tool that provides location-based data on where people live and work, providing insights on how far and to/from where people travel for work.



EXISTING TRAVEL BEHAVIOR AND CHARACTERISTICS

The following section paints a picture of how Watertown residents are currently getting around the city and to other destinations. Analyzing data that signifies potential for walking and biking, such as zero-car households or households in poverty, also helps Watertown understand how much demand may exist for better walking and biking facilities. The data summarized below is from the US Census, including the 2020 decennial census, the 2023 5-year American Community Survey (ACS), and On the Map census tool, unless otherwise stated.

It is also important to consider the high number of schools in Watertown. In addition to the public school system and multiple private grade schools serving local residents, the presence of a boarding school and a college in Watertown is note worthy. These schools serve populations that are less likely to have access to personal vehicles (due to age and school policies) and thus rely on alternative modes of transportation.

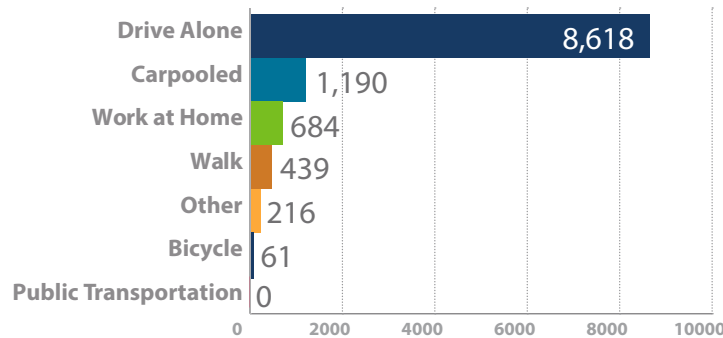


Figure 2.1 Bike and Walk Mode Share in Watertown

BIKING AND WALKING MODE SHARE

Because trips to work make up a large majority of daily transportation, analyzing how residents get to work provides a good snapshot of typical mode share in Watertown. Mode share is the percentage of travelers using a particular type of transportation (e.g. a personal vehicle, public transit, bicycle, or walking). Figure 2.1 highlights how Watertown residents get to work most days of the year.

In most American cities, many residents drive to work alone. According to the 2023 5-year ACS, an estimated **77% of Watertown residents aged 16 or older drive to work alone**, which is slightly higher than the rate of 75% for the State of Wisconsin. **Approximately 11% of Watertown residents carpool to work, and an additional 4% walk to work. 0.5% of residents bicycle to work.** No residents reported using public transportation to get to work.

The rates of carpooling and walking to work in Watertown are slightly higher than the statewide rates, which are 7.6% and 2.5% respectively (see figure 2.2). Six percent of residents work from home.

	Watertown	Wisconsin
Drove Alone	77%	76%
Carpooled	11%	7%
Public Transit	0%	1%
Walked	4%	3%
Other Means	2%	2%
Work at Home	6%	12%

Table 2.1 Transportation to Work in Watertown and Wisconsin (2023 ACS Data)



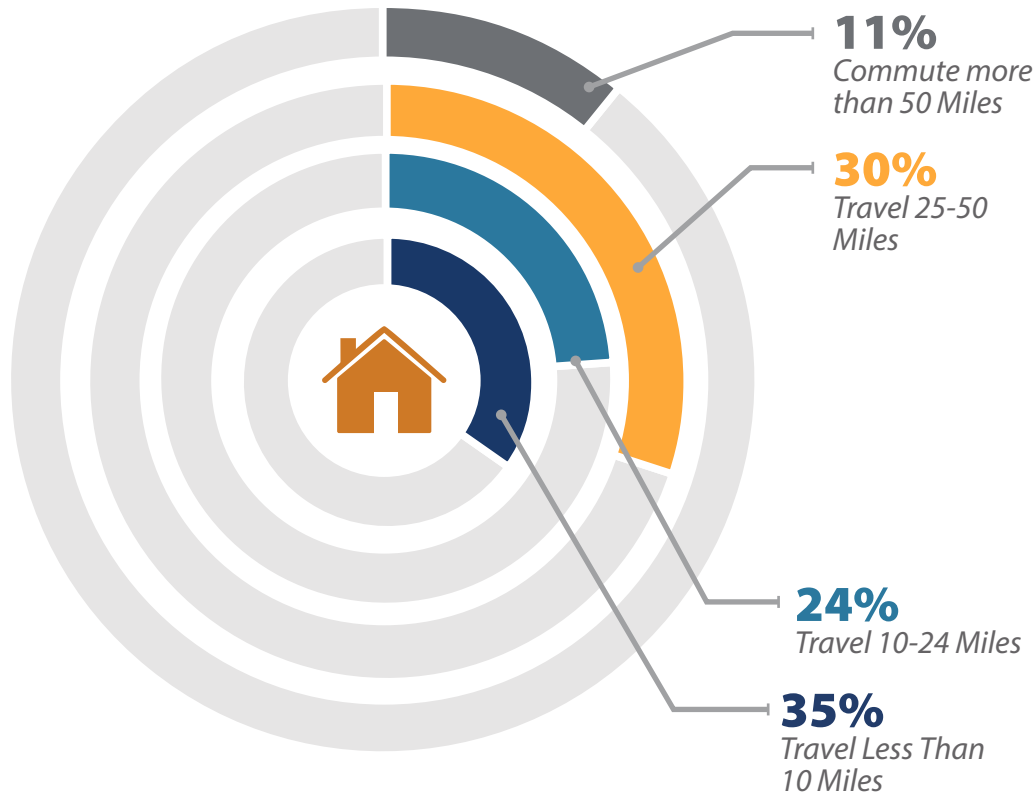
*Public transit is not available in Watertown

One significant note about travel is that every person, regardless of their main travel mode, is a pedestrian at one point in time whether it's walking (or rolling) from their car to their work entrance or walking to a neighbor's house to carpool to work.

As seen in Table 2.2, modes of transportation to work have shifted over time in Watertown, with the number of people walking and biking to work decreasing overall. Efforts to promote walking and biking will require efforts from a variety of partners, including the City, schools, organizations, and employers through investments in infrastructure and facilities along with education and programmatic elements.

	2015	2020	2023
Bicycle	0.8%	0.3%	0.5%
Walk	4.4%	5.1%	3.9%

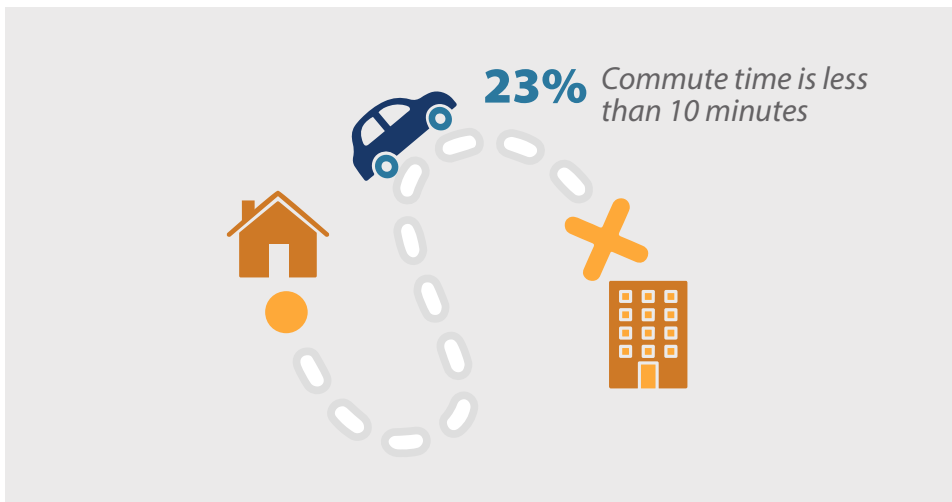
Table 2.2 Transportation to Work in Watertown Over the Years



COMMUTE DISTANCES AND TIME

When a person is less than three miles from their destination, the chance that they choose to bike to their destination is more likely. Similarly, when a person is less than a half mile away from their destination, they are more likely to walk. Even if individuals don't choose to bike or walk to their destination, they are more likely to consider it as an option if the distance is short, and high-quality biking and walking facilities are present.

*Of residents who commute to work in Watertown, **35% travel less than 10 miles, 24% travel 10 to 24 miles, and 30% of residents travel 25 to 50 miles. 11% of Watertown residents commute more than 50 miles to work** (2023 5-year ACS). While Watertown residents, workers, and visitors may not currently be choosing to bike or walk for all their trips, there is great potential to change behaviors because distances are generally short.



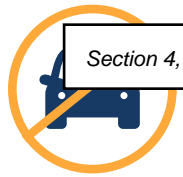
Approximately 2,600 (23%) Watertown residents have a commute that takes them less than 10 minutes to get to work (2023 5-year ACS). According to the [On the Map US census tool](#), of the 12,000 workers that live in Watertown, there are around 3,100 that work within the city limits. These numbers show us that there is great potential to increase the non-motorized transportation mode share with improved facilities, encouragement and education.

*Source for data in this paragraph: U.S. Census Bureau, U.S. Department of Commerce. "Means of Transportation to Work." American Community Survey, ACS 5-Year Estimates Detailed Tables, Table B08301

8.5%

Of households in Watertown have no vehicles

Section 4, Item C.



HOUSEHOLD CHARACTERISTICS

Vehicle competitive households are defined as those having more people than vehicles available within a household. When faced with conflicting travel situations, some people decide to work alternative work schedules, while others shift to a different means of transportation such as carpooling, public transit (where available), walking, or biking. Figure 2.2 highlights the number of vehicle competitive households in Watertown. This data indicates how many workers could potentially benefit from a more connected bicycle and pedestrian network. It also highlights the fact that not every eligible driver within a household in Watertown needs to own a car, if they can supplement some trips by walking or biking to nearby destinations.

There are an estimated 627 households with more workers than vehicles in Watertown, and an estimated **758 households with no vehicles. Zero-vehicle households account for 8.5% of all Watertown households** (2023 5-year ACS). These households must rely on other modes of transportation, such as walking, biking, public transit (where available), or carpooling to get around.

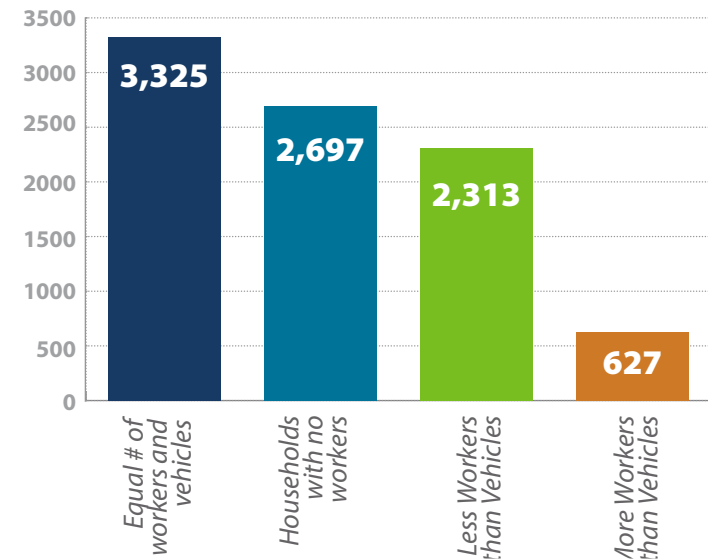
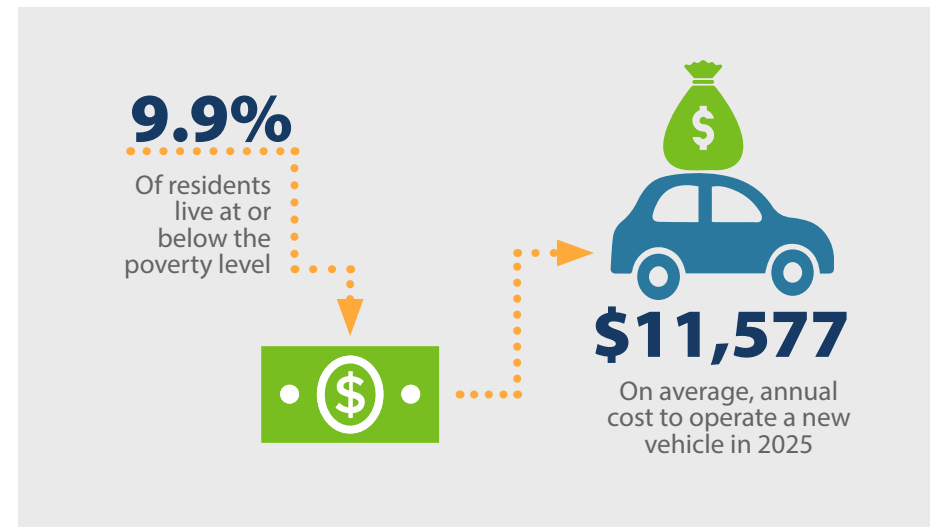


Figure 2.2 Number of Vehicle Competitive Households in Watertown

In Watertown, **9.9% of residents live at or below poverty level** (2023 5-year ACS). A disproportionate level of minority populations live below the poverty level in Watertown. According to AAA, **it costs an average of \$11,577 annually to operate a new car in 2025**, or about \$965 per month. This amount factors in all costs of auto ownership, including depreciation, finance, fuel, insurance, maintenance, and taxes and registration. For those living at or below the poverty level, vehicle ownership can be a significant cost burden. This cost burden can be eliminated or significantly lowered if a safe and convenient non-motorized transportation network exists.

Source citation: U.S. Census Bureau, U.S. Department of Commerce. "Selected Economic Characteristics." American Community Survey, ACS 5-Year Estimates Data Profiles, Table DP03.

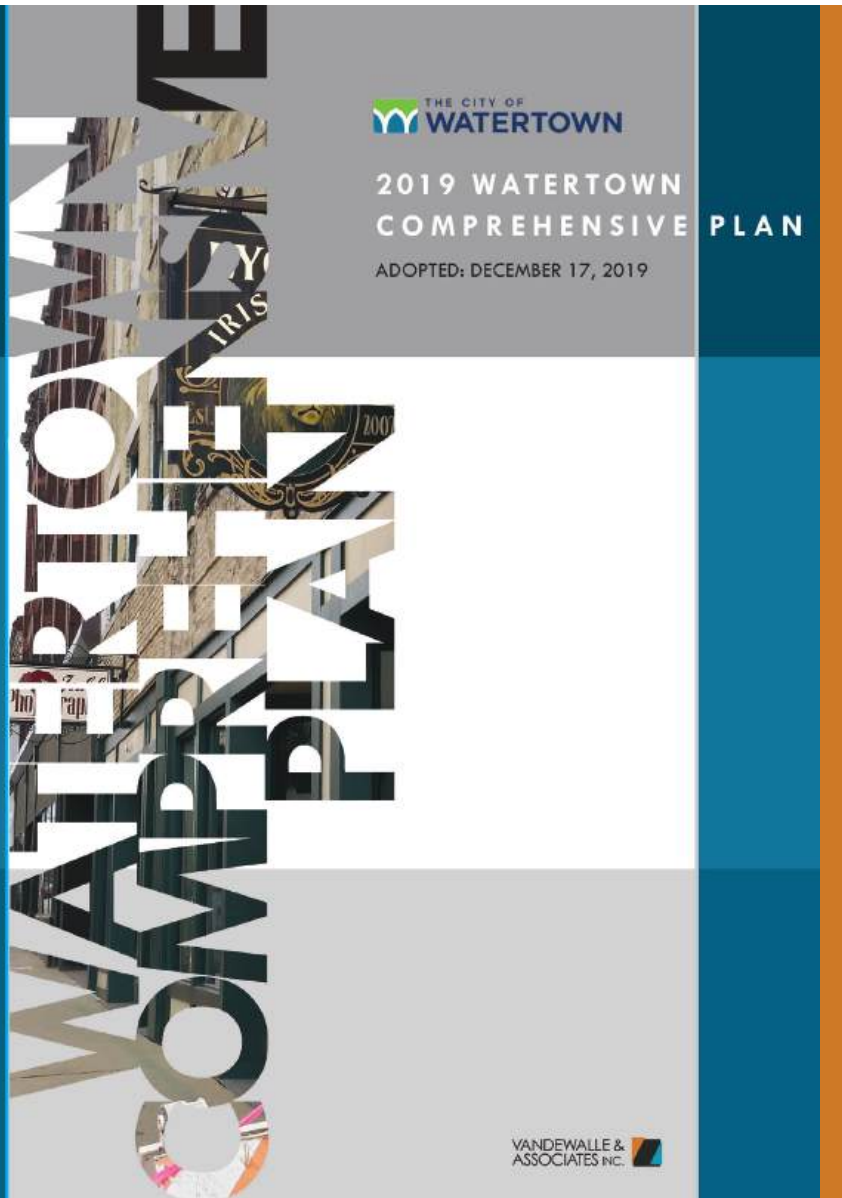




RELEVANT PLAN REVIEW

Section 4, Item C.

Numerous existing planning documents support the continued development and enhancement of the city's bicycle and pedestrian network. Some of the most recent and relevant documents are highlighted in the following pages.



CITY OF WATERTOWN COMPREHENSIVE PLAN (2019)

The Watertown Comprehensive Plan, adopted in 2019, provides the City with a vision and a plan for the next 20 years. The Comprehensive Plan offers guidance on sustainable growth and development and recommendations on how to bring the city towards its vision across a variety of topic areas. The areas covered in the plan include agricultural resources, natural resources, historic and cultural resources, community character and design, land use, transportation, utilities and community facilities, housing and neighborhood development, economic development, and intergovernmental cooperation.

The transportation section of the plan is of particular relevance to this plan as it identifies goals and policies that prioritize the development of bicycle and pedestrian facilities, encourage collaborative bicycle and pedestrian planning, and prioritize aesthetic improvements that improve the experience of walking and biking in the City.

TRANSPORTATION

Goals

- » Provide a safe and efficient multi-modal transportation system that meets the needs of pedestrians, bicyclists, motorists, trucks, trains, and transit users.
- » Develop and maintain a comprehensive system of on-street and off-street bicycle and pedestrian facilities in the Watertown area.

Objectives:

- » Provide safe and convenient access between neighborhoods, employment centers, schools, service centers, and recreational centers
- » Consider pedestrian and bicycle accessibility when selecting new sites for public facilities such as schools, parks, libraries, and community centers.

Policies:

- » Prioritize the future interconnection of the City's on-street bicycle facilities system within the City, in addition to connections to the Downtown Riverwalk, Interurban Trail, Glacial River Trail, and Wild Goose State Trail.
- » Partner with the Watertown Bicycle Task Force and local public health partners to better plan and implement a comprehensive and safe network of sidewalks and bicycle routes to connect neighborhoods with schools, parks, and shopping.
- » Use the Jefferson County Bicycle and Pedestrian Plan and Dodge County Bicycle and Pedestrian Plan as the foundation to create a customized City-wide Bicycle and Pedestrian Plan to further prioritize active transportation facilities, connections, and utilization.
- » Actively participate and engage in the update of the Jefferson County Bicycle and Pedestrian Plan and Dodge County Bicycle and Pedestrian Plan.
- » Require all new developments to be served with sidewalks or pedestrian/bicycle paths.
- » Support a comprehensive community-wide wayfinding signage system in order to better direct travelers to key destinations in the community.
- » Encourage aesthetic improvements such as canopy shade trees along terraces, pedestrian scale theme lighting, landscaped boulevards, traffic circles, banners, or benches in all arterial and collector street construction and reconstruction projects.
- » Adopt a Complete Streets Policy that requires multi-modal elements as part of every roadway infrastructure project. Integrate United States Department of Transportation (USDOT), Federal Highway Administration (FHWA), National Association of City Transportation Officials (NACTO), American Association of State Highway and Transportation Officials (AASHTO), and Wisconsin Department of Transportation (WisDOT) guidelines, best practices, and performance measures into the policy to facilitate true complete streets.
- » Continue bicycle and pedestrian educational programs like the Police Department's bicycle rodeo and other trainings to increase safety and utilization of trails, paths, sidewalks, and streets.
- » Continue to enforce the Sidewalk Infill Policy.

HOUSING AND NEIGHBORHOOD DEVELOPMENT**Objectives:**

- » Encourage pedestrian and bicycle-oriented neighborhood designs as new developments are platted and existing neighborhoods are revitalized.

Policies:

- » Design livable neighborhoods that are well-served by sidewalks, bicycle routes, and other non-motorized transportation facilities and located within a ten-minute walk (approximately 1/3 mile) of a public park or open space area.
- » Continue to enforce the City Sidewalk Infill Policy to increase the interconnectedness of the pedestrian network within existing neighborhoods.



WATERTOWN PARKS & OPEN SPACE PLAN

DRAFT JUNE 2025



CITY OF WATERTOWN COMPREHENSIVE PARKS & OPEN SPACE PLAN

(2019 AND 2025)

Watertown adopts a Parks and Open Space Plan every 5 years that serves as a guiding document for Watertown’s Parks, Recreation, and Forestry Department. The most recent plan was adopted in 2025, during the planning process for this Bicycle and Pedestrian Network Plan, so both the 2025 and previous 2019 Parks and Open Space Plan were reviewed. Generally, the Parks and Open Space Plan provides an overview of existing conditions, analysis of future parks and recreation needs, and recommendations on how the City can meet these future needs through improvements to existing facilities and acquisition or development of new facilities. The Plan also provides calculations and information regarding Watertown’s park dedication fees.

2025 PARKS AND OPEN SPACE PLAN

The 2025 Plan update identified four overarching goals, which were modified from the 2019 Plan:

- » Goal #1: Provide inclusive, safe, and active places for all. Provide a sufficient number of parks, recreational facilities, and open space areas to support active living, and to enhance the health and quality of life for City of Watertown residents and visitors. Such facilities should be diverse in accommodating people of many different ages and abilities, such as the elderly, people with disabilities, adults, youth, and children. Parks should be welcoming, safe, and accessible for all.
- » Goal #2: Maintain and improve parks with community support. Continue to manage and maintain existing parks, open space facilities, and recreational programming in response to community interest and needs. Leverage partnerships, grants, and agency programs to implement initiatives and to secure funding for projects.
- » Goal #3: Foster a resilient future. Preserve, manage, and plan for the City’s natural resources and outdoor amenities to create a resilient, sustainable park system that works for the benefit of current and future residents.
- » Goal #4: Connect parks and open spaces to the City’s multi-modal network. Ensure that parks, recreational facilities, and open space areas are connected to the City’s multi-modal network for driving, biking, walking, and rolling.

Recommendations in the plan relating to bicycle and pedestrian facilities include:

- » Utilize the sidewalk and trail system to better connect people to parks
- » Continue to expand and maintain trails within parks to create longer routes and more loop trails
- » Clear trails for use throughout the winter months
- » Improve safety and the perception of safety along trail facilities, including through improving lighting
- » Maintain the trail networks regularly
- » Consider how trails are used beyond walking, running, and biking.
- » Continue to work with county and regional partners to connect into regional trail systems and routes

DOWNTOWN MAIN STREET RECONSTRUCTION TASK FORCE RECOMMENDATION REPORT (2022)

The Downtown Main Street Reconstruction Task Force Recommendation Report details the recommendations from the Task Force, which was convened in response to the anticipated replacement of the Cole Bridge (completed in 2025) and reconstruction of 11 blocks of Main Street (currently planned for 2028). The report provides design guidelines based on community feedback for the Main Street reconstruction and wider downtown area. There are two sets of recommendations – one specific to the Main Street reconstruction project, and one set applicable to the entire downtown area. The recommendations fall into four categories: Aesthetic Improvements; Infrastructure Improvements; Pedestrian-oriented Improvements; and Planning Recommendation; and three timeframes: before, during, or after reconstruction.

RELEVANT POINTS

Main Street Recommendations:

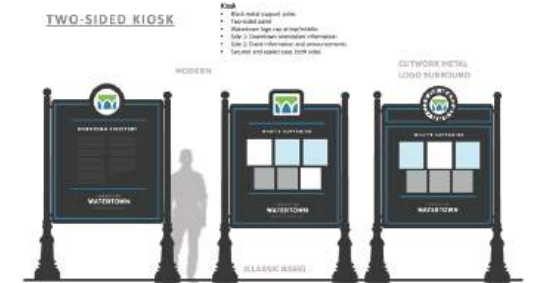
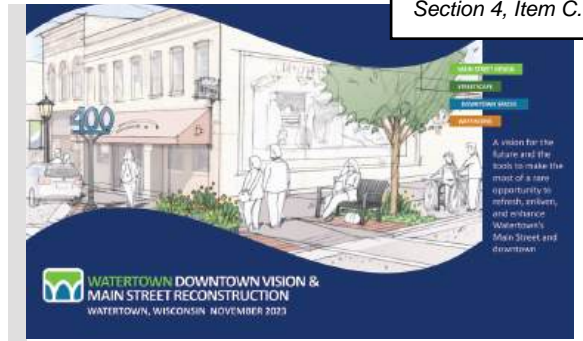
Recommendations related to pedestrian travel along Main Street include streetscape improvements to create a pedestrian-scale environment, address safety and experiences.

Below are a few key recommendations:

- » Install traffic lights that allow for pedestrians to stop traffic.
- » Widen sidewalks.
- » Install well-designed wayfinding signage.
- » Install curb bump-outs to improve traffic calming measures.
- » Relocate crosswalks for improved pedestrian flow and safety.
- » Incorporate features for the blind and deaf into crosswalks.
- » Create continental crosswalks for improved pedestrian safety.

Downtown Recommendations:

- » Perform transportation network evaluation in the Downtown, with the intention of redesigning one-way streets and planning for bike usage.
- » Create implementation plan for developing the Riverwalk north of the Cole Bridge.



CITY OF WATERTOWN PARKING STUDY

The City of Watertown conducted a parking study in 2022 (an update to the parking study completed in 2018), which considered the parking utilization rates of on- and off-street public and private parking. The main objectives of this study were to review the existing parking utilization within the project area, document any supply issues that exist, and plan for potential future parking opportunities as redevelopment is expected to occur. The study identified low, medium, and high-cost options for implementing the study's objectives and recommendations.

RELEVANT POINTS

Conclusions

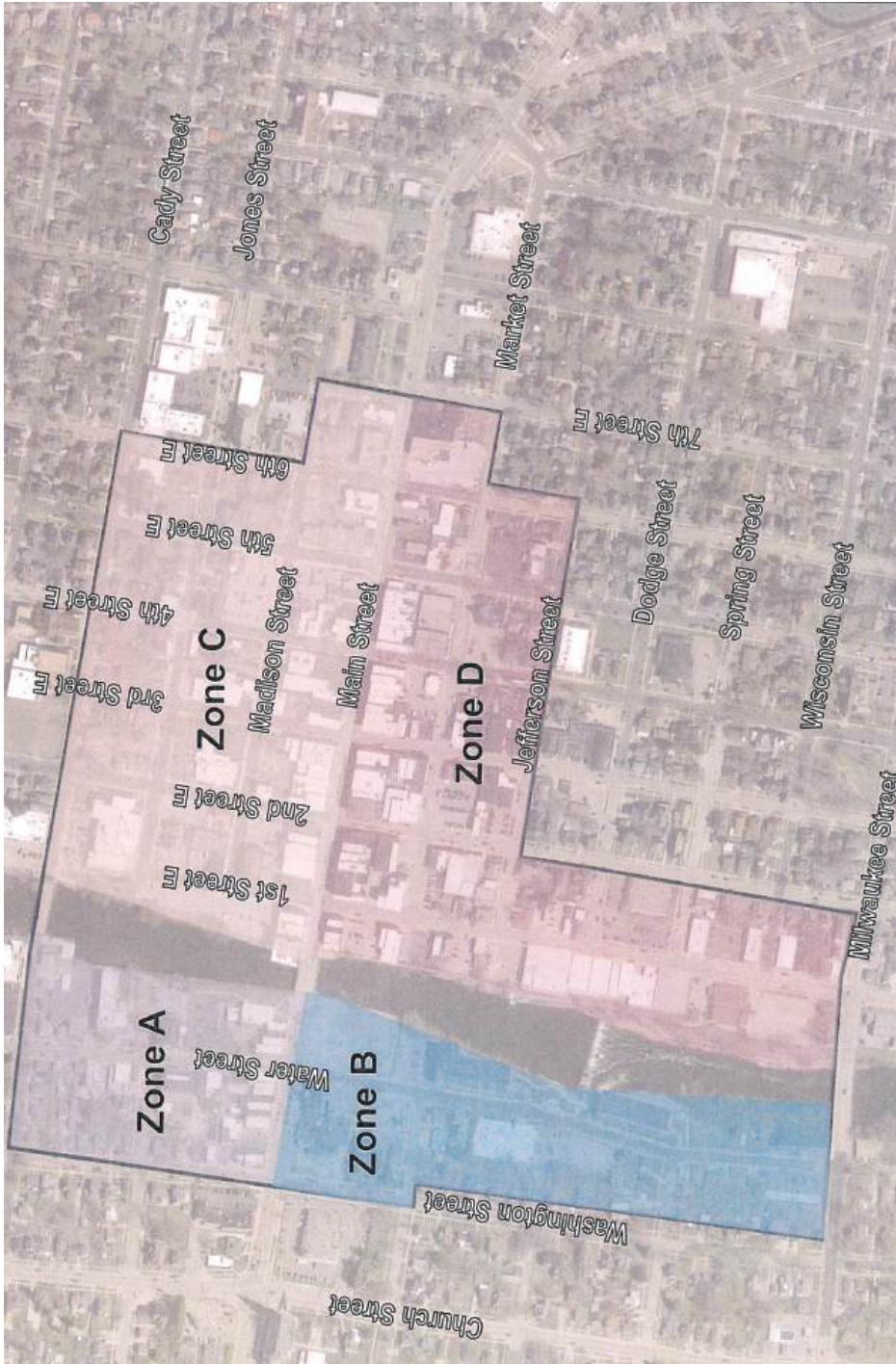
- » The peak parking period in downtown Watertown is generally identified as Saturday afternoons, with a peak utilization of approximately 45% of all spaces in the study area.
- » **Overall, there is a public parking surplus of 597 spaces.**
- » Potential deficits may occur in Zones A and B (west side of the Rock River) during programmed events on Saturday afternoons.

Medium-Cost Implementation

- » Promoting Walking/Biking Downtown: To encourage higher usage of public parking lots on the fringe of the Downtown core, the City can encourage walking through the core and encourage residents who live within a bikeable distance (typically within 1 - 2 miles) to bike to Downtown to reduce reliance on automobiles.

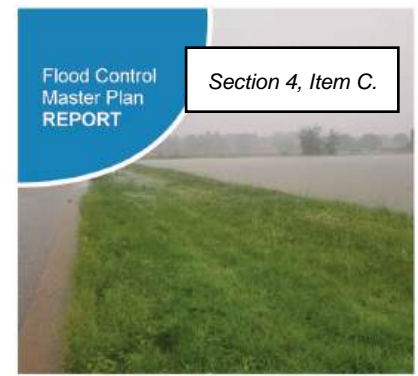
The plan identifies strategies to encourage walking and biking, including:

- » Installing bike racks and bike lanes
- » Designating bike routes
- » Ensuring sidewalks are appropriate widths, maintained in good condition, and clear of snow and ice
- » Ensure adequate lighting for safety
- » Install wayfinding oriented to pedestrians and cyclists, including distances and walk times and information about nearby destinations

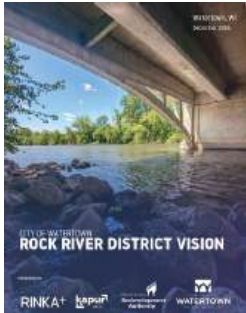


CITY-WIDE FLOOD STUDY (2024)

The 2024 Watertown Flood Study Report provides flood risk projections for 10-, 100-, and 500- year flood events and details mitigation and management practices to decrease flooding risks in Watertown. The focus of the plan is on flooding in low-lying developed areas, primarily related to older undersized storm sewer systems, as opposed to riverine flooding. The plan identifies focus areas with tailored recommendations to reduce flooding and provides regional best management practices for stormwater management.



Flood Control Master Plan Report
February 14, 2024



ROCK RIVER DISTRICT VISION REPORT (2025)

This report outlines a vision and accompanying principles to guide redevelopment and shape the future of the Downtown River Corridor. The plan states desired outcomes and includes an overview of public participation and existing conditions analysis that offer direction for the vision in this corridor. The purpose of the plan is to promote cohesive and thoughtful redevelopment that expands public access and activates public spaces along the riverfront in downtown Watertown.

RELEVANT POINTS

Desired Outcomes

- » Establish publicly accessible spaces on both sides of the river for use as gathering spaces, recreational opportunities (walking, running, biking), and for fostering social interaction
- » The Riverwalk seeks to enhance walkability, connect key public nodes (including South First Street Park, Watertown Dam pocket park, Bentzin Family Town Square, and Fannie P Lewis Park), and connect into newly developed and planned areas, including the Globe multifamily housing development.
- » The Riverwalk is intended to be a key link in a cohesive network of parks and public spaces in downtown, which include planned and recently developed areas and trails
- » Public Participation

Framework

- » Pedestrian Connectivity: the plan recognizes that adding to existing pedestrian infrastructure and connecting into the Glacial River Trail and Interurban Trail provides an opportunity to create a pedestrian hub in downtown.
- » Parks & Open Space: the plan proposes integrating existing green spaces along the riverfront through improved pedestrian connections and green infrastructure – the vision is to offer an expanded recreational network that offers active and passive recreational opportunities; integrating these green spaces also promotes ecological preservation, another goal of the existing parks and open space plan.

Placemaking Strategy

- » The goal of a continuous Riverwalk is to integrate existing parks and other fragmented community spaces.
- » Streetscape design should include complete streets.
- » Riverwalk should integrate infrastructure improvements such as bike-friendly amenities and signage to enhance accessibility and wayfinding.

RIVERSIDE MIDDLE SCHOOL TRAFFIC AND PEDESTRIAN SAFETY STUDY (2016)

The Riverside Middle School Traffic and Pedestrian Safety Study reviewed crashes near the middle school between 2011-2016, conducted a survey of school parents, and engaged staff and local stakeholders to identify safety and traffic flow issues near the school. Key problems identified include inadequate pedestrian facilities, insufficient signage and pavement markings, and uncontrolled traffic flow and operation. Recommendations identified in the plan covered short- and long-term recommendations that address both the area immediately around Riverside Middle School and the broader city streets around the school.

RELEVANT POINTS

Short-term recommendations that focus on pedestrian safety include:

- » Constructing additional sidewalks
- » Improving intersections for pedestrians using:
 - Raised crosswalks
 - Additional school crossing signage and pavement markings
 - Rectangular Rapid Flashing Beacons (RRFBs) at intersections
 - Adding midblock crossings with crossing islands
- » Adding additional signage and pavement markings throughout the area, including speed feedback signs and school crossing signs

Long-term recommendations include:

- » Redesigning the intersection of E Main Street and the school driveway and cemetery roadway to improve pedestrian access and safety

JEFFERSON COUNTY BICYCLE AND PEDESTRIAN PLAN (2010)

This document was an update to the 1996 Jefferson County Bikeway and Pedestrianway Plan and was meant to build upon the 1996 plan while reassessing the current bicycling culture and the extent to which conditions for pedestrians and bicyclists have changed since the adoption of the original plan.

RELEVANT POINTS

This plan provides strategies intended to increase transportation safety for pedestrians, bicyclists, and motorists. Infrastructure improvements such as sidewalks, marked crosswalks, designated bikeways, bike lanes, paved shoulders, multi-use trails, and informational signs are among the types of facilities recommended to improve conditions for the non-motorized public. Opportunities to educate bicyclists about safety and promote bicycling as a viable mode of transportation are also discussed. Additionally, recommendations to improve enforcement and education regarding traffic laws affecting bicyclists and pedestrians and to promote bicycling and walking as viable modes of transportation are also included. This plan focuses on multi-modalism and the idea that transportation systems should offer not only choices among travel modes for specific trips, but more importantly, present these options so that they are viable choices that meet the needs of individuals and society as a whole.



41. Rank of any of the following translated bicycle route by approximately ranked by per mile cost about the city:

Route	Route	Cost	Rank	Cost	Rank	Cost	Rank	Cost	Rank
Loop 1 - Whittier Lake Blvd	10.00	10.00	10.00	10.00	10.00	10.00	10.00	10.00	10.00
Loop 2 - Hamilton Station Circle	10.00	10.00	10.00	10.00	10.00	10.00	10.00	10.00	10.00
Loop 3 - Road Highway	10.00	10.00	10.00	10.00	10.00	10.00	10.00	10.00	10.00
Loop 4 - Cambridge Lake Blvd	10.00	10.00	10.00	10.00	10.00	10.00	10.00	10.00	10.00
Loop 5 - Park Avenue/Whittier Blvd	10.00	10.00	10.00	10.00	10.00	10.00	10.00	10.00	10.00
Loop 6 - Main St	10.00	10.00	10.00	10.00	10.00	10.00	10.00	10.00	10.00
Loop 7 - Hamilton Station Circle	10.00	10.00	10.00	10.00	10.00	10.00	10.00	10.00	10.00
Loop 8 - Colfax/Kemp	10.00	10.00	10.00	10.00	10.00	10.00	10.00	10.00	10.00
Loop 9 - Van der Post Blvd/Flour	10.00	10.00	10.00	10.00	10.00	10.00	10.00	10.00	10.00
Loop 10 - Van der Post Blvd/Flour	10.00	10.00	10.00	10.00	10.00	10.00	10.00	10.00	10.00
Loop 11 - Main St	10.00	10.00	10.00	10.00	10.00	10.00	10.00	10.00	10.00
Loop 12 - Davis Street/Allen St	10.00	10.00	10.00	10.00	10.00	10.00	10.00	10.00	10.00

DODGE COUNTY PARKS, OUTDOOR RECREATION AND OPEN SPACE PLAN (2023)

This plan provides an overview of existing parks and open space facilities, projects future parkland needed to serve county residents, and provides recommendations for improving existing park facilities, developing new parks, and recommendations for trails, open space areas, and special use facilities. The plan identifies a number of recommendations for existing county and regional trails, these recommendations are relevant to Watertown's Bicycle and Pedestrian plan as they are critical for building regional connections. Below are selected points that pertain specifically to improvements within or immediately adjacent to Watertown that tie into city infrastructure or programming.

RELEVANT POINTS

- » The plan provides a map of recommended trail connections to future planned parks
- » Glacial River Trail Recommended Site Improvements and Expansion Opportunities
 - Install bike route signage along trail and road connections to Watertown and the Wild Goose State Trail
 - Seek opportunities for off-road trail connections to replace the current road routes where feasible. The railroad corridor from Clyman Junction to Watertown may hold some opportunities in some sections.
 - Acquire lands that may become available and suitable for development of an off-road path to close the gap between the Wild Goose State Trail and the City of Watertown. The railroad corridor from Clyman Junction to Watertown could hold opportunities for trail development upon abandonment or upon consideration of a trail alongside the active line in some areas.

WATERTOWN SOUTHWEST SIDE CONCEPT NEIGHBORHOOD PLAN

The purpose of this plan is to provide a framework for developing the Southwest Side neighborhood in Watertown with a focus on providing diverse housing types. In addition to centering housing as a key principle, the plan emphasizes developing a sustainable neighborhood that incorporates multi-modal transportation, employs a walkable community design, and preserves environmental systems.

RELEVANT POINTS

The plan proposes using traditional neighborhood design principles. Relevant principles of this design style include:

- » Provide a variety of land uses and walkability: integrate neighborhood-scaled destinations within walking distance of residents such as parks and community institutions
- » Blend multi-modal transportation options into neighborhood design: facilitate pedestrian neighborhood within and between neighborhoods; provide sidewalks along all streets and multi-use trails in environmental corridors and parks; Interconnect streets within neighborhood and to existing and future adjoining neighborhoods

The plan also includes a draft conceptual neighborhood plan that identifies a potential trail network. The trail network would include key shared use paths providing access throughout the development with smaller feeder trails and/or pedestrian and bicycle infrastructure providing access to the shared use paths from all parts of the development.



EXISTING BICYCLE AND PEDESTRIAN NETWORK

EXISTING FACILITIES

The existing bicycle and pedestrian network in Watertown is comprised of a combination of sidewalks, off-street bikeways (shared use paths), and on-street bikeways, as shown in Figure 2.3. This network includes components developed and maintained by the City of Watertown, agency collaborations, Jefferson and Dodge Counties, and the State of Wisconsin. The City currently maintains 5.4 miles of on-street bike lanes, 6.4 miles of shared use paths, and 9.8 miles of trails within City parks, a majority of which are soft surface trails. There are 97.9 miles of sidewalk in the City, most of which are maintained by residential property owners.

While there are many high-quality trails in Watertown today, facility segments are not connected to each other. This can make it difficult and stressful for bicyclists or pedestrians to get around town or travel through the city.

OFF-STREET NETWORK FACILITIES

SHARED USE PATHS (BIKEWAYS)

Sometimes referred to as bikeways, trails, off-road trails, separated paved trails, shared-use trails or shared-use paths, these facilities are physically separated from roadway traffic and are intended to be used by bicyclists, pedestrians, and other non-motorized travel.

Shared use path is the term preferred for the purposes of this plan, as it indicates intended users (bicyclists and pedestrians), the type of facility (paved) and location (off-street). City code uses the term 'bikeways' to refer to these facility types. Standard City Specification dictates bikeways shall be constructed of bituminous pavement with a minimum width of 8'.

The recently constructed Interurban Trail is an example of a shared use path. Segments of the Glacial River Trail and a few City-owned trails make up the remainder of shared use paths today in Watertown. The current network of shared use paths is disconnected, with many gaps in the overall network.

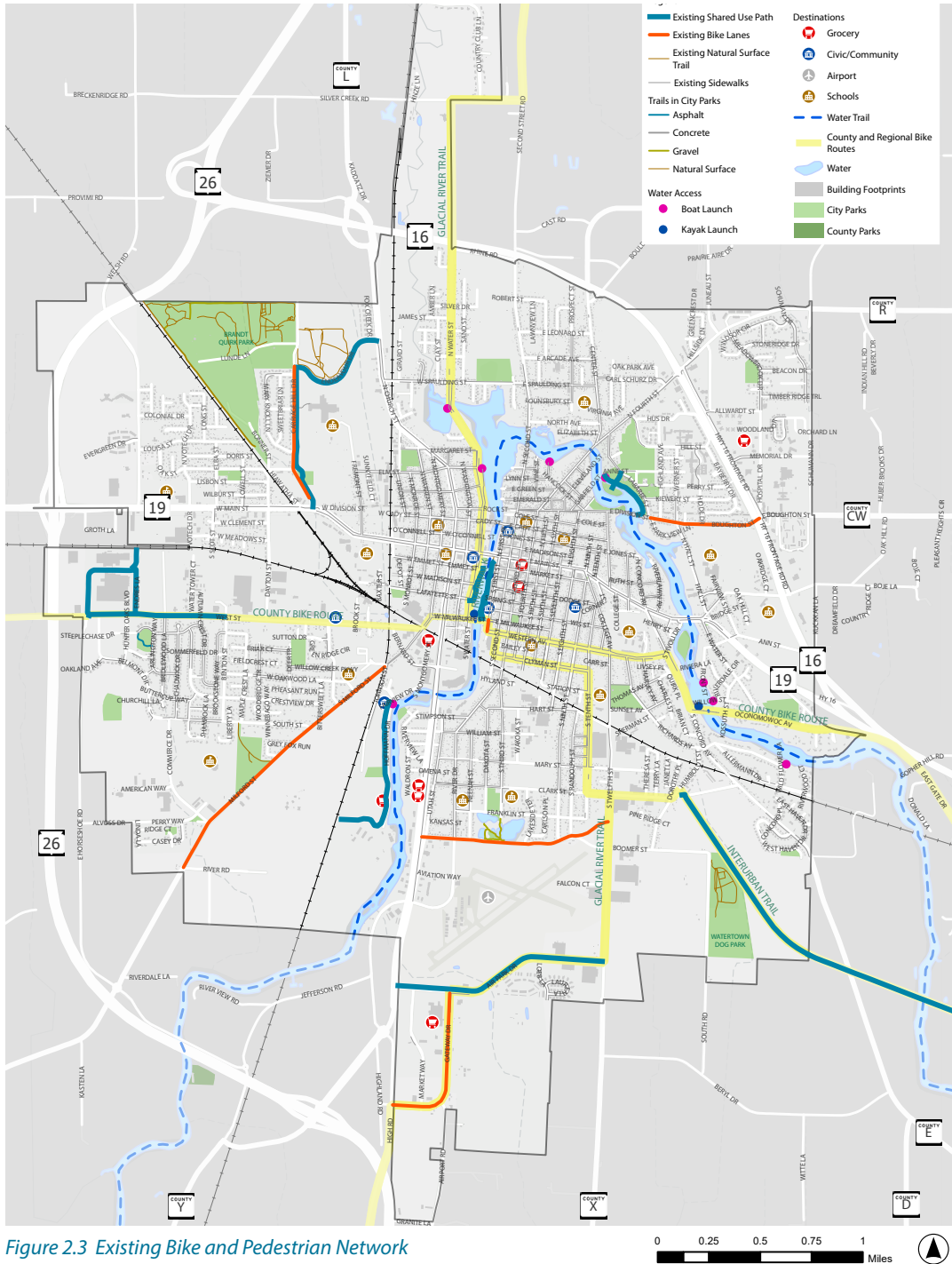
WATERTOWN BIKING AND WALKING FACILITIES TODAY

- » **5.4 Miles** of On-Street Bike Lanes
- » **6.4 Miles** of Shared Use Paths
- » **9.8 Miles** of Trails within City Parks
- » **97.9 miles** of sidewalk in the city

119.5 miles of total facilities!



Interurban Trail



Existing trail



Existing alleyway



Existing sidewalk

Figure 2.3 Existing Bike and Pedestrian Network

RIVERWALK

The Riverwalk is a shared use path and boardwalk that loops around the Rock River in downtown Watertown. As planned in the Rock River District Plan, the Riverwalk will extend from Milwaukee Street to the south and Fannie Lewis Park (west side of river) and Division Street (east side of river) to the north. The Riverwalk links parks and public spaces in downtown, which includes planned and recently developed areas and trails. Future connections will include South First Street Park, Watertown Dam pocket park, Bentzin Family Town Square, and Fannie P Lewis Park. Other planned amenities will expand the Riverwalk further north along the Rock River, improve stormwater management, and increase amenities along the Riverwalk.



Riverwalk

SIDEWALKS

Sidewalks are paved paths primarily intended for pedestrians.

According to City Code: Sidewalks are constructed of concrete and should be at least five feet wide. Sidewalks shall be located as far from the traffic lane as is possible but not closer than six inches within the right-of-way line.

Sidewalks in downtown Watertown are typically wider, some up to 10' in width, and provide walk-up access to business entrances. Sometimes decorative concrete or other urban design elements are included along the pathway. Neighborhood sidewalks are generally 4-6 feet wide, made of concrete, and are separated from the roadway by a planted boulevard.

Watertown has a consistent and connected network of sidewalks throughout downtown and the adjacent residential neighborhood. The network of sidewalks is inconsistent outside of the downtown core, with some neighborhoods (primarily in the south, west, and northeast areas of the city) lacking connected sidewalks.



Existing sidewalks

ON-STREET NETWORK FACILITIES

BIKE LANES

Bike lanes are designated marked lanes intended solely for use by bicyclists. They are generally marked with a solid white line on one or both sides of a roadway, sometimes with additional 'bike lane' symbols.

Watertown has added a number of bike lanes in the last 10 years, such as those along West St, Milford St, Church St, Boughton St, and Fourth St. There are a few areas in the City where bike lanes and on-street parking are allowed in the same area of the roadway. This has been identified as a conflict to be addressed as the future shared use path and on-street network is implemented.

SHARED LANE MARKINGS

Sometimes referred to as 'sharrows,' shared lane markings indicate routes where bicyclists and vehicles share a lane of traffic. It should be noted that bicyclists are allowed on all local roadways and by state law can take a full lane of traffic, even if shared lane markings or bike lanes are not provided.

REGIONAL TRAILS

Regional trails refer to shared use paths and bike routes that connect users across a region. They are often maintained by the county or through partnerships across multiple jurisdictions, including counties and cities. There are several regional trails that connect through Watertown. These routes are largely on-street or overlay shared use paths and rely on branded signage to indicate the route. The Glacial River Trail and the Rock River Trail are examples of Regional Trails in Watertown.



Existing bike lane



Glacial River Trail in Watertown



Intertuban Trail

INTERURBAN RECREATION TRAIL

The Interurban Recreation Trail is a planned 11-mile recreation trail between Oconomowoc and Watertown, built on an old rail corridor. The trailhead in Watertown is in the southeast quadrant of the City along Clark Street. The Interurban Trail is currently open with 7 miles of paved bike trail completed from Watertown to Ixonia. The final phase of the project has a planned completion of fall 2025 and includes a bridge over the Rock River and an additional 3 miles of paved trail connecting to Oconomowoc.

ROCK RIVER TRAIL

The Rock River Trail is a 320-mile system of recreational trails on and along the Rock River that travel through 11 counties in Wisconsin and Illinois. This route can be hiked, bicycled, paddled, or driven and begins at the headwaters of the Rock River in Fond du Lac County, Wisconsin, going all the way to the Mississippi River at Rock Island, Illinois. The route runs through the center of Watertown north/south and is primarily an on-road bike route with occasional paved segments. The Rock River Trail and the Glacial River Trail follow the same route through Watertown, coming into town on Water Street from the north, and exiting town on High Road to the south.

GLACIAL RIVER TRAIL

The Glacial River Trail spans 52 miles, featuring a mix of separated paved bike paths and on-road connections between Janesville and the Wild Goose Trail in Dodge County. The trail includes 15.9 miles of paved, off-road paths and 17 miles of marked on-road sections, primarily running alongside Hwy 26.

JEFFERSON COUNTY BIKE ROUTE

Jefferson County has identified a primarily on-road bike route going east/west through the City. To the west, this route connects to Waterloo. The County has identified a connection trail between Waterloo and Watertown as Priority #4 in their Bicycle and Pedestrian Plan from 2010.

STATE TRAILS

There are no state trails within the City. However, the Glacial River Bike Route, which runs north/south through the City, connects Watertown to the Glacial Drumlin State Trail to the south and the Wild Goose State Trail to the north. The Glacial Drumlin State Trail connects to the metro areas of Madison and Milwaukee and the Wild Goose State Trail runs northeast to Fond du Lac.

EXISTING TRAIL REVIEW

An existing trail review evaluated the potential for integrating bikeways within the City of Watertown, using established best practices to assess feasibility and constraints. The evaluation considered spatial attributes such as location, connections to community destinations, right of way limitations, and the presence of physical barriers. It also incorporated quantitative data including roadway speeds, existing facility types, and AADT to help determine potential comfort levels for bicycle users, as well as qualitative characteristics such as urban or rural context. Together, these factors informed system wide recommendations for future bikeway facilities. The network was further segmented to identify appropriate facility types for each corridor, and more detailed guidance was developed for individual segments.

Click here to view the Existing Trail Review Map Online



Figure 2.4 Screenshot of the Existing Trail Review Map



Intertuban Trailhead



Rock River Trail



Glacial River Trail



TRAFFIC, SAFETY, AND ROADWAY INFORMATION

ROADWAY FUNCTIONAL CLASSIFICATION

Traditional definitions for roadways in a city refer to arterials (principal and minor), collectors, and local roads. These terms come from National Functional Classification (NFC), a transportation planning tool. All public roads are classified according to the function they serve within the overall roadway network.

Arterials serve as major traffic routes within and through the community and see heavier traffic usage compared to other road types. Arterials are primarily designed for the movement of traffic, with a secondary goal of serving adjacent land uses. In Watertown, examples of principal arterial roadways include Church St, W Main St and Summit Ave, and State Road 16. Due to traffic speeds and volumes, arterial roadways are usually high-stress areas for bicycles and pedestrians.

Collector roads serve key community destinations and provide a way for community traffic to access the regional roadway system. Examples of collector roads in Watertown include N Second St, Water St, Concord Ave, and Clark St. These roads provide a greater level of access to individual property and will carry more moderate levels of traffic. Depending on roadway speeds, these roads may create high or moderately high stress experiences for pedestrians and bicyclists. Any on-street bicycle facilities on collector roads should generally be buffered or protected with bollards, curbs, or other physical separation, especially where posted speeds are higher than 35mph.

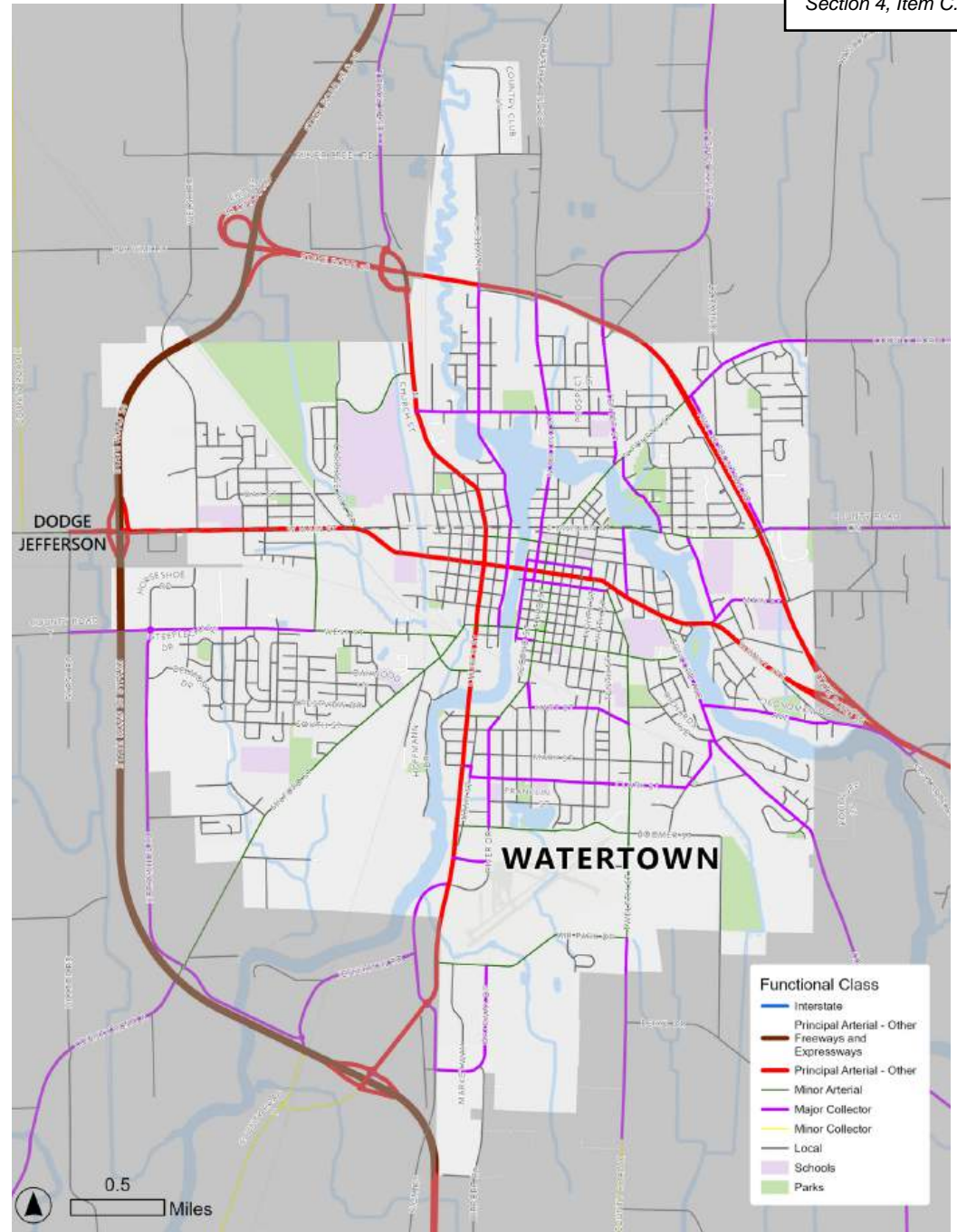


Figure 2.5 Roadway Functional Classifications

Local streets provide the greatest degree of access to private property and generally carry the lowest traffic volume. Because they provide the greatest level of access, local streets make up the most street mileage in the system. Local roadways are often lower stress roadways for pedestrians and bicyclists as traffic speeds are lower, there are often traffic calming interventions or parked cars that slow cars down. Lower traffic volumes also reduce stress for pedestrians and bicyclists.



TRAFFIC VOLUMES

Traffic volumes, measured using average annual daily trips (AADT), are highest on the main north-south and east-west throughfares through Watertown. Church St has the highest traffic volumes (more than 10,000 AADT), followed by Main St (5,001 to 10,000 AADT). Highway 26, in the northwestern corner of the city, and State Highway 16 in the northern part of the city also have high traffic volumes. Collector roads within Watertown have lower traffic volumes, with AADT counts between 1,501 and 5,000.

Generally, on-street facilities (such as bike lanes and on-street bikeways) are best paired with roadways that have lower traffic volumes (less than 5,000 per day). Off-street facilities (shared use paths) are recommended along roadways with higher traffic volumes.

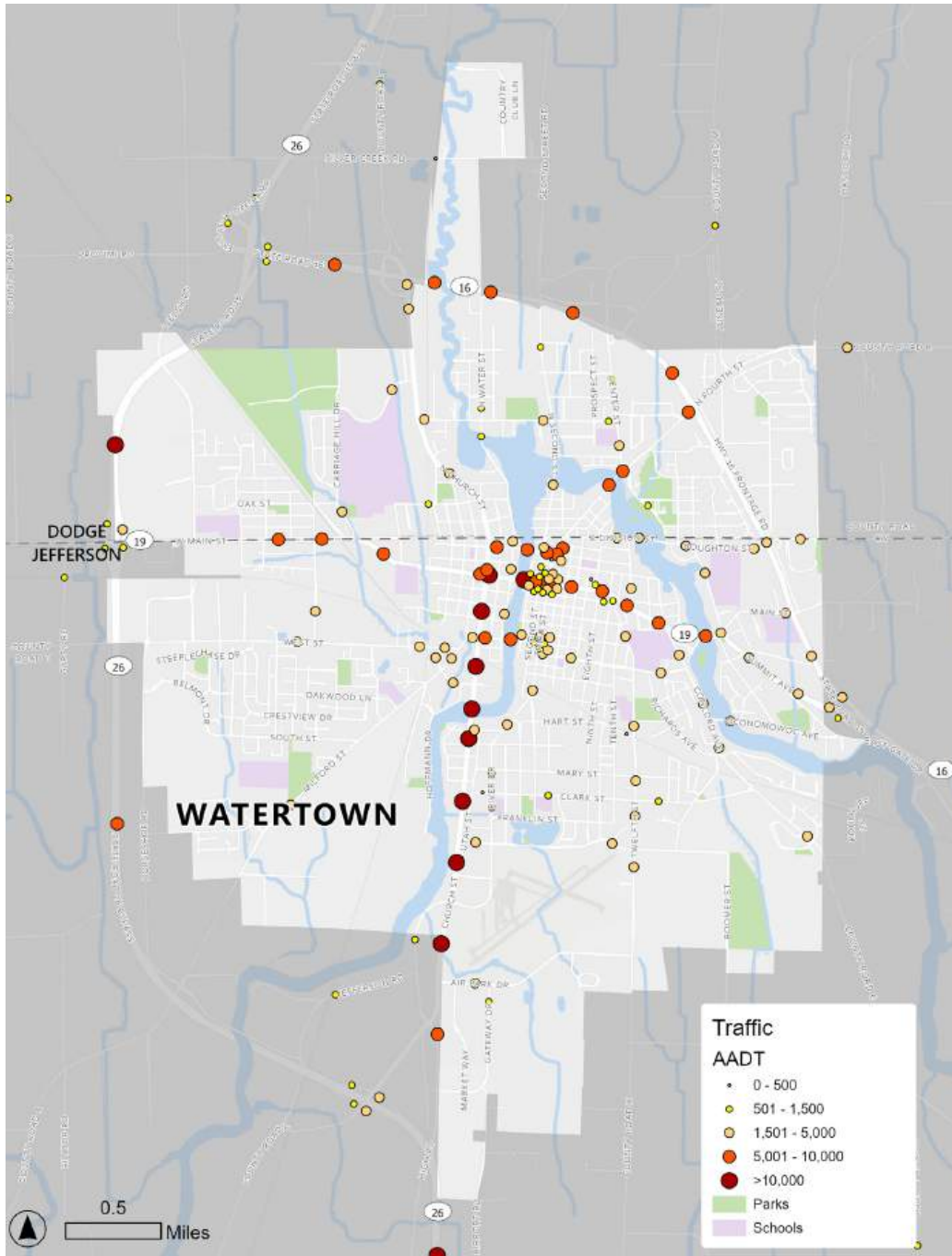


Figure 2.6 AADT Map

BIKE AND PEDESTRIAN CRASHES

Between 2019 and 2025, there have been 49 crashes involving a bicycle or pedestrian. Of the 49 crashes, 28 have involved pedestrians and 21 have involved bicyclists. The majority of crashes have occurred in the downtown area along or close to Main St. Figure 2.7 shows the locations and types of crashes. Many of the identified crashes occur at intersections. One fatality and eight suspected serious injuries were results of these crashes, with an additional 24 incidents with suspected minor injury.

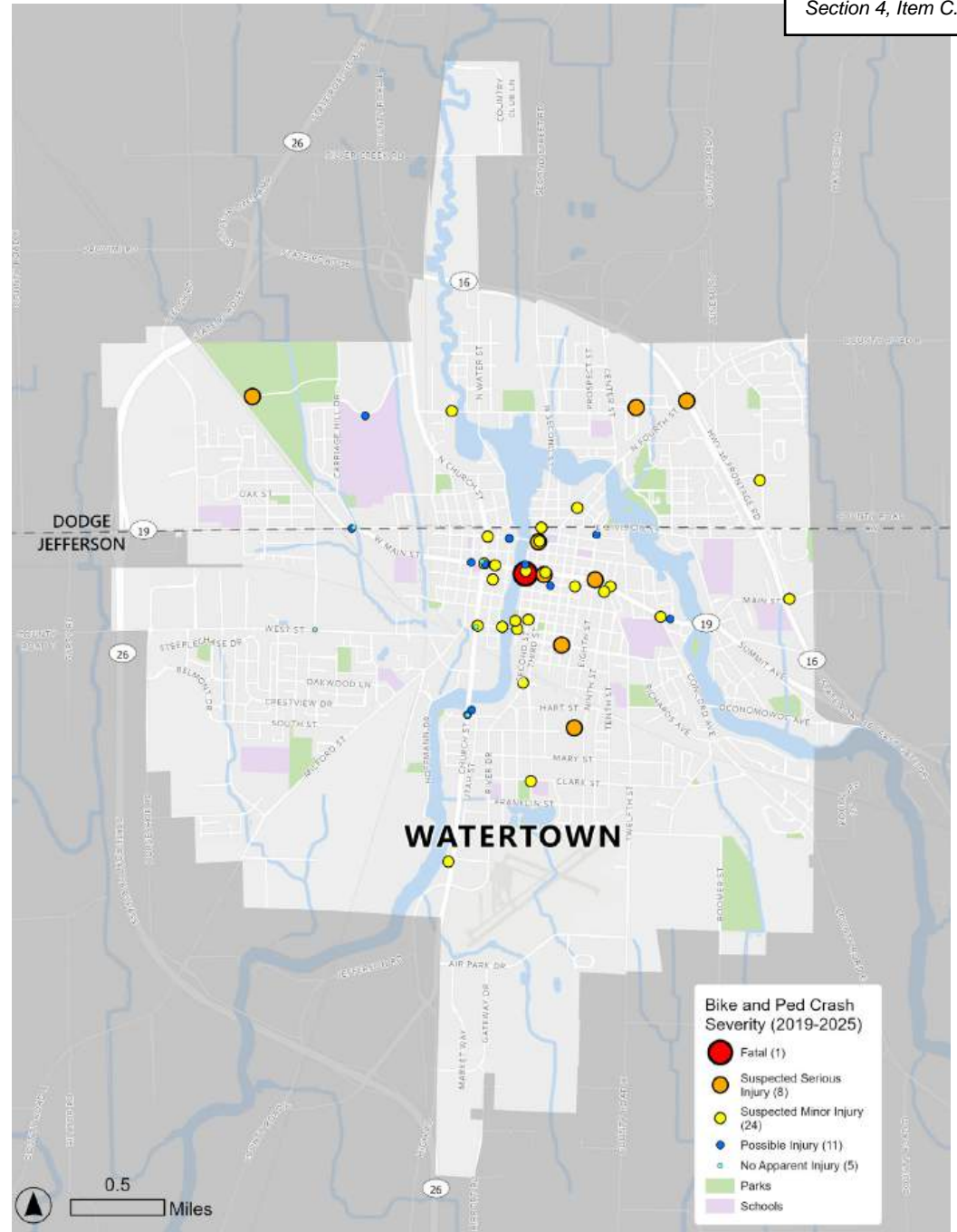


Figure 2.7 Crash History and Severity (Wisconsin DOT)

HYDROLOGY

Figure 2.8 shows the slope analysis for Watertown. The entire detailed planning area is located within the Rock River Basin portion of the Mississippi River system. The dominant feature of the area is the Rock River corridor. The Rock River runs from the east side of the planning area, through the City, and exits the planning area to the southwest. Silver Creek, which empties southward into the Rock River on the north side of the City, drains the majority of the lands to the north and northwest. Minor tributaries of the Rock drain south to north on the west, northeast, and east side of the planning area.



Rock River - Lower Dam

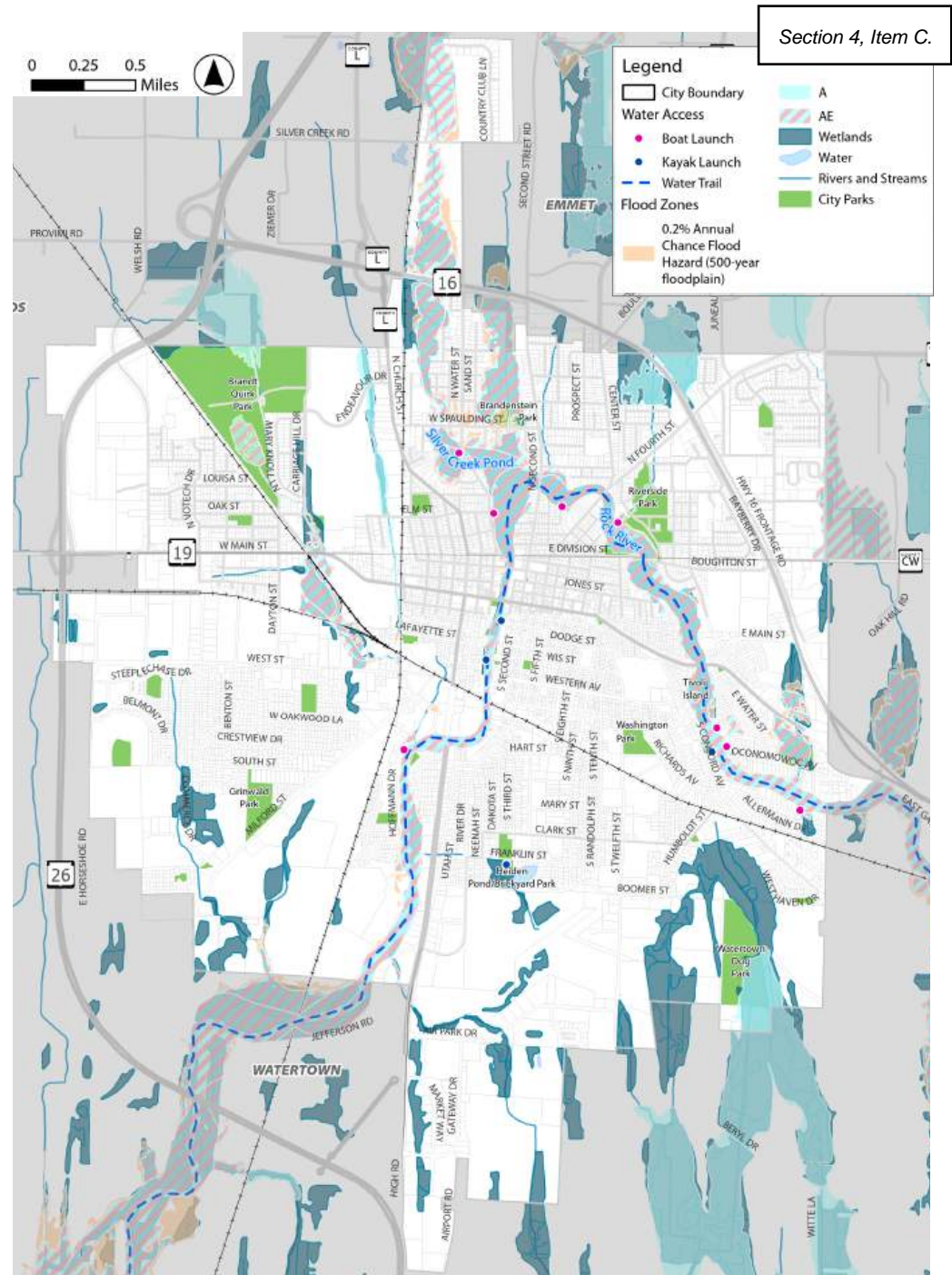


Figure 2.8 Hydrology

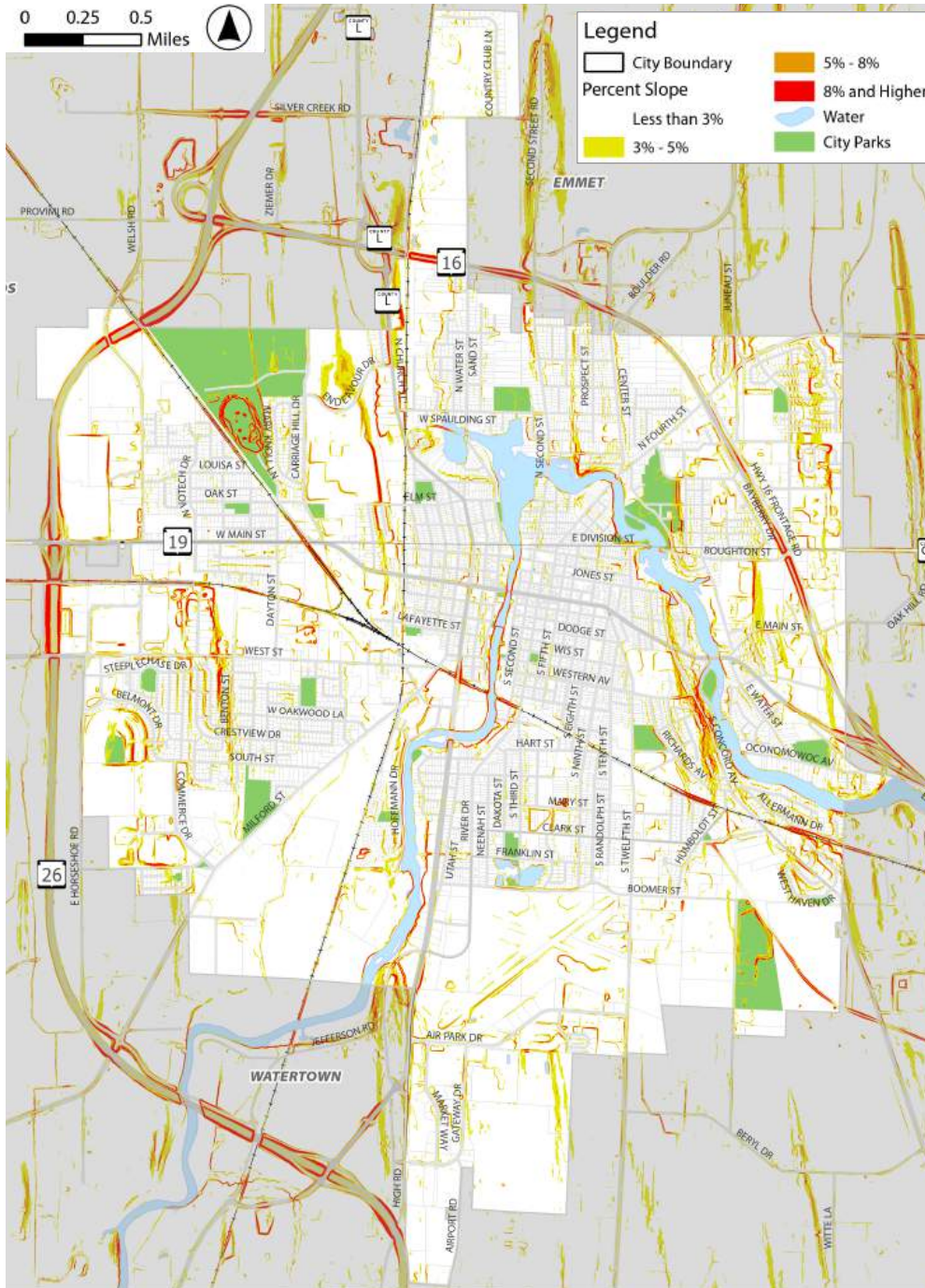


Figure 2.9 Slope Analysis

Finally, the headwaters of Johnson Creek drain southeast and southern parts of the planning area. The Rock River creates a significant challenge in planning a connected pedestrian and bicycle network as river crossing infrastructure is costly. Combining bicycle and pedestrian facilities with existing road bridges is an option, but often results in on-street facilities or limited space for bicycles and pedestrians.

SLOPE ANALYSIS

Watertown is relatively flat with no major topographical challenges (Figure 2.8). There are some riverbank areas with slopes of 8% or higher and a few parks contain more steep areas than other areas of the city.



Riverbanks at Heron View Park

FUTURE LAND USES AND DEVELOPMENT

Watertown's 2019 Comprehensive Plan provides detailed information on projected land needs, growth, and development in Watertown. The plan identifies a projected need for 980 acres of new development over the life of the plan, which runs through 2040. The future land use map identifies a number of residential and land use categories. Many of the identified expansion areas within and beyond the City's current boundaries are planned for residential uses. As these areas develop, incorporating bicycle and pedestrian infrastructure will be critical for ensuring well-connected, multi-modal transportation networks.

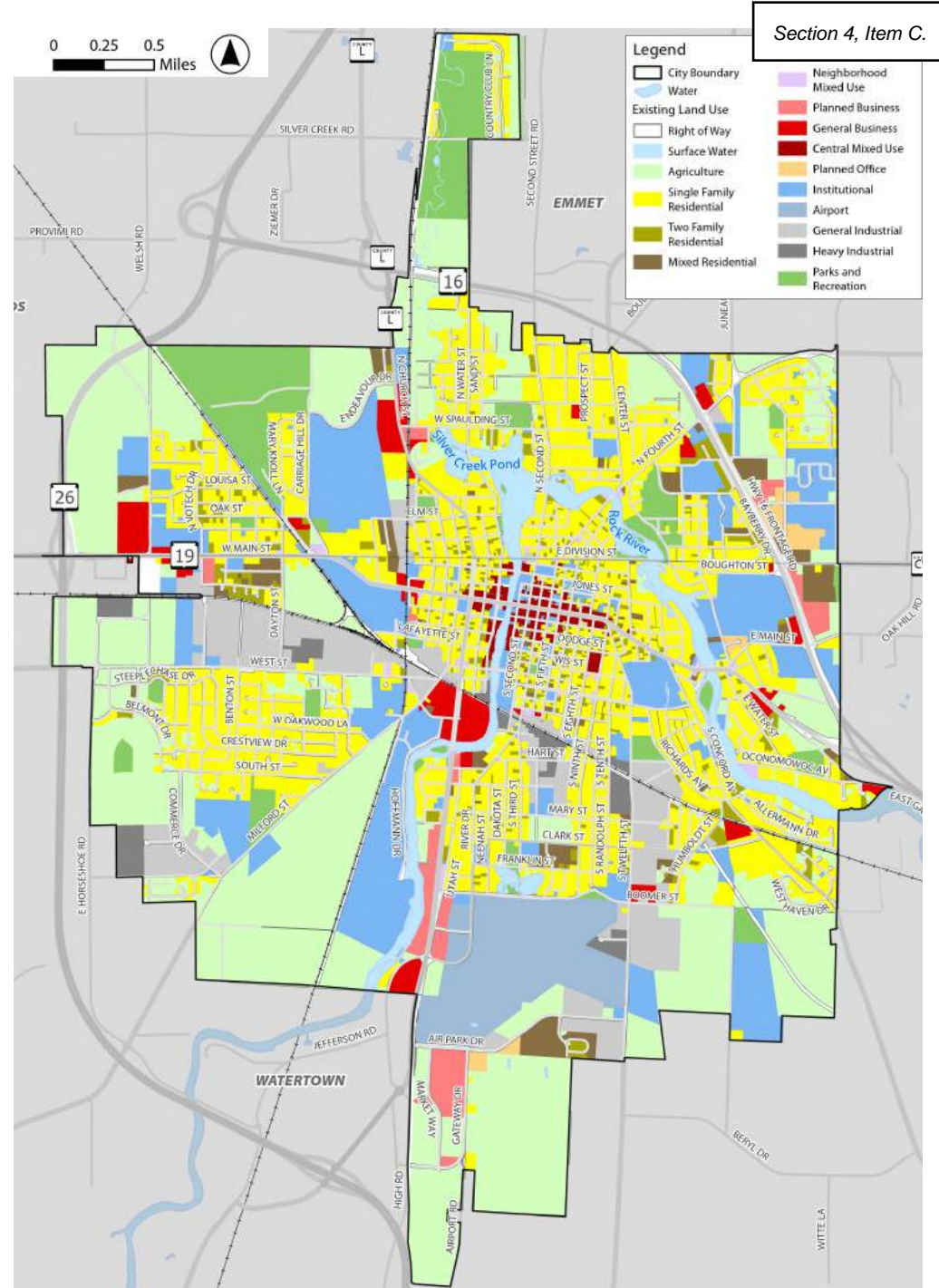


Figure 2.10 Future Land Use Map

CITY ORDINANCE REVIEW

Sec. 545 of the Watertown Subdivision Code lists the general requirements for subdivisions. Per the code, subdivisions shall conform to the policies agreed to in the Comprehensive Plan, Parks and Open Space Plan, Zoning Code and other planning documents. Specifically, the subdivision code identifies several policies relating to multi-modal transportation and pedestrian and bicycle infrastructure, including:

- » To develop a system of interior open spaces within existing environmental corridors to delineate neighborhoods, control stormwater drainage, and provide circulation for pedestrian and bicycle traffic.
- » To ensure that development locates and coordinates safely and efficiently with transportation facilities.
- » To favor development intensities and patterns that are supportive of alternative modes of transportation.



Section 4, Item C.

Article III of the Watertown Subdivision Code outlines the required improvements and design standards for subdivisions in Watertown. As part of this section ([Sec. 545-37](#)), the code requires developers to install sidewalks on both sides of all major and arterial streets and on local streets that provide access to businesses, schools churches, neighborhood parks, multi-family residential, restaurants, and other destinations. The code also identifies the requirements for design and construction of sidewalks. Section 545-37 also provides the following definition for bikeways:

“*Bikeways shall serve both pedestrian and bicycle traffic in areas where the majority of the adjoining lots do not have frontage or access to a street. In general, those lots which do not front or have access on the street in question are not the generating or terminating point for the pedestrian or bicycle traffic. Bikeways shall be designed to transport the majority of pedestrian or bike traffic through the area as opposed to serving the adjoining lots as a sidewalk does. Bikeways shall be constructed of bituminous pavement at least eight feet in width according to City specifications.*”

[Watertown Code Section 545-37](#)

[Section 545-29\(M\)](#) states that in the design of the plat, the developer shall make provisions for pedestrian ways and bikeways for transport and recreation as required by the Plan Commission on recommendation of the Park, Recreation and Forestry Commission and the Public Works Commission, based upon recommendations contained within the Comprehensive Plan and formally adopted sub-plans and policies. Required pedestrian ways and bikeways shall adhere to city specifications as required in Chapter 545.

Community Engagement

3

INTRODUCTION

Public engagement relies on community voices to influence and shape the planning process. A robust community engagement process paves the way for community support for the final plan and makes implementation of plan elements easier. It also ensures the plan reflects the challenges, ideas, and experiences of community members. The Bicycle and Pedestrian Network Plan process included a variety of engagement methods, including online and in-person events. This chapter highlights the comments of community members as they participated in the variety of community engagement opportunities throughout the planning

OUTREACH AND COMMUNICATIONS

Communication is a key component of good engagement, and providing consistent information about the project timeline, process, and outcomes throughout the process goes a long way. During the planning process, the City hosted a webpage dedicated to the project that was updated periodically with input opportunities, findings, and the project timeline. Social media posts shared information about upcoming community events and linked to the survey and interactive mapping tools. Information about the plan was included in the Chamber of Commerce newsletter and direct invitations to the open house, stakeholder meetings, and other engagement events were sent via email.



EVENTS

Engagement events included pop-up events (the team shares project information and gathers input at existing community events), a community-wide open house, meetings with the Bicycle and Pedestrian Path Task Force and meetings with different stakeholder groups, such as educators in Watertown. Below is a summary of the engagement events conducted over the course of the planning process.

POP-UP EVENTS

BIKE RODEO

The Watertown Bike Rodeo is an annual event that teaches kids bike riding skills and safety, including hand signals and bicycle courses to practice on. It is a community event with prizes and raffles for bicycle helmets, and kids' bicycles. At the June 21, 2025 bike rodeo, the project team set up a booth with information and asked for input. The team interacted with approximately 20 people and heard:

- » Concerns about sidewalk gaps, specifically along Water Street
- » General safety concerns
- » Interest in expanding the Riverwalk
- » Interest in additional bike lanes downtown



RIVERFEST

Riverfest is an annual community festival featuring activities and music hosted in Watertown every August. The project team attended on August 8, 2025 and spoke with approximately 25 people. Most conversations were with families, many of whom were visiting Watertown from out of town, rather than residents of the city. Conversations with people who stopped by the table centered on:

- » Interest in regional trail connections
- » Appreciation that the City was engaging with and seeking input
- » Popularity of the Interurban Trail



COMMUNITY OPEN HOUSE

A community open house was held on August 7, 2025. Between 20 and 30 people attended. Most attendees were enthusiastic or frequent bicyclists, runners, and hikers. Themes and topics covered in comments and conversations at the open house included:

- » A need for trail connections, specifically in areas around 12th Street, the Interurban Trail, and Western & Concord
- » Concern about crossings, specifically crossing Hwy 16, Meadowbrook Drive (the hospital area)
- » Barriers to cyclists posed by one-way streets downtown
- » Lack of bike parking
- » Interest in making downtown better for biking (business draw, Riverwalk)
- » Soft shoulders on newer paved roads are difficult for bicyclists
- » Lots of interest in regional trail connections to the west and finishing the Interurban Trail
- » Interest in bike lanes, neighborhood sidewalks, natural surface trails, and shared use paths
- » Wayfinding for the Glacial River Trail bike route



COMMUNITY ADVISORY COMMITTEE

Watertown has a standing Bicycle and Pedestrian Path Task Force that was created in 2016 to oversee, delineate, and update the plans to reflect the most advantageous locations for bicycle and pedestrian paths that will bring the biggest benefit to the City. Building off the existing Task Force, the Community Advisory Committee was formed as a steering committee for the Bicycle and Pedestrian Plan, providing input and direction throughout the planning process. The Community Advisory Committee included the Bicycle and Pedestrian Task Force members and representatives from Dodge and Jefferson Counties and the school district. Over the course of the project, there were five Community Advisory Committee meetings held to share findings and seek direction on plan goals and recommendations.

STAKEHOLDER ENGAGEMENT

Stakeholder engagement involved outreach to specific groups organized around individual topics. Outreach included a virtual meeting with stakeholders from the schools in Watertown, including the superintendent of the Watertown School District and a representative from Luther Preparatory School, a boarding school; and outreach to the business community through the Chamber of Commerce.

COMMUNITY SURVEY

A community-wide survey was available online from April 22 – August 28, 2025. The survey was advertised on the City website, through social media posts, and at in-person community events (such as the Bike Rodeo and Riverfest). The survey consisted of 11 questions focused on individual preferences regarding biking and walking as well as what respondents thought of Watertown’s current and future bike/walk network.



The survey *was open for four months* from April 22 – August 28, 2025

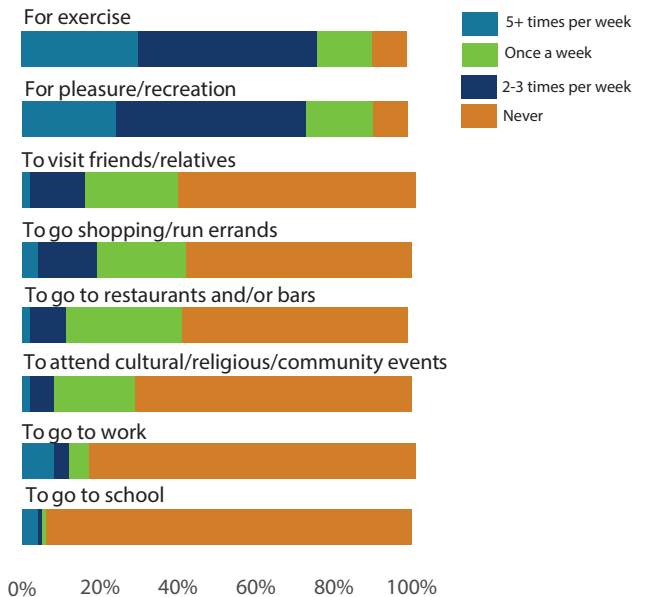


371 total responses recorded

SUMMARY OF RESULTS

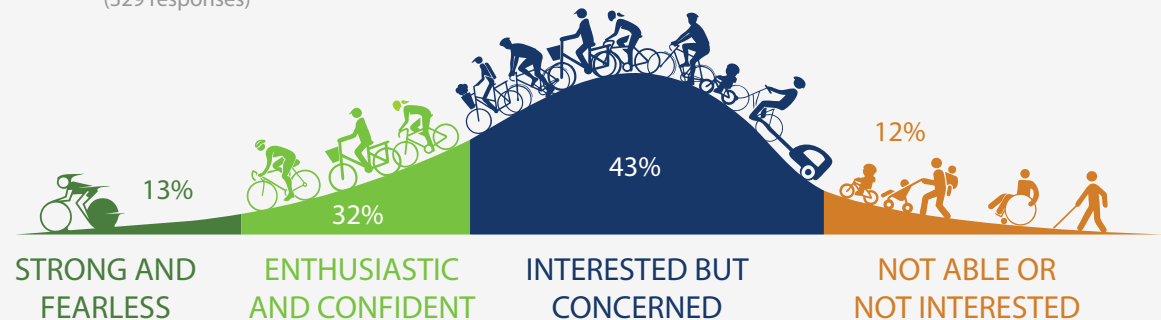
- » When asked how frequently they bike for different reasons, 50% of respondents said they walk or bike for pleasure/recreation, 2-3 times per week.
- » Additionally, 47% of respondents walk or bike for exercise 2-3 times per week.
- » 60% of respondents say they never bike or walk to go shopping/run errands or to visit friends or relatives.
- » 8% of respondents stated they bike or walk to work 5 or more times per week and 4% say they bike or walk to work 2-3 times per week. 83% of respondents say they never bike or walk to work.
- » 83% of respondents said that parks and trails are the most important destinations to be able to walk or bike to, with residential areas ranked second most important.

During the warmer months of the year, how often do you walk or bike for the following reasons? (329 responses)



Respondents were provided with descriptions of attitudes towards biking. 43% of respondents identified their attitude as “interested but concerned” and 32% said they were “enthusiastic and confident.”

How would you characterize your attitude towards biking? (329 responses)



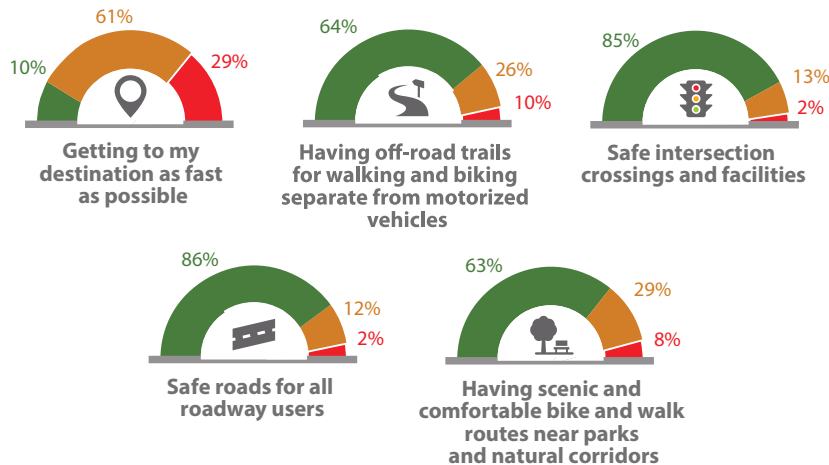
The top three responses to the question “what keeps you from walking or biking more in Watertown?” were:

- » Lack of bicycle and pedestrian facilities (trails and sidewalks)
- » I don't feel safe walking or biking near motorized traffic
- » I don't feel safe crossing busy intersections

Similarly, 86% of respondents said that safe roads for all roadway users is very important when planning for the future of Watertown's bicycle and pedestrian network and 85% of respondents said safe intersection crossings and facilities are very important.

How important are each of the following to you when planning for the future bicycle and pedestrian network?
(322 responses)

● **Very important** ● **Somewhat important** ● **Not important**



There were 177 responses to the open-ended question for ideas to improve conditions for biking, walking, and rolling in Watertown. Many comments showed strong support for protected bike lanes, with suggestions for bike lanes along Dewey Ave, Main St, 12th St, Dayton Rd, and connections to the Interurban Trail, Walmart, and the KwikTrip on Church St. Similarly, there was significant concern around safety crossing busy intersections, with several intersections identified as priority areas, including Church and Main St around the high school, KwikTrip on 4th St, Western Ave, and Hwy 26 and Church St Downtown. Additional ideas shared in the open response section of the survey included:

- 1 Interurban Trail Connections
- 2 Wayfinding to local destinations from the Interurban Trail
- 3 Wayfinding at Riverside Park
- 4 Extend Riverwalk and make it more bike-friendly
- 5 Mountain bike trails at Brandt-Quirk and Watertown Dog Park
- 6 Fill sidewalk gaps where needed
- 7 Replace on-street trails with separated paved trails

"I wish that there were more natural areas in Watertown. Watertown is really short on them and natural areas are not encouraged in Watertown- it seems that local people don't care about trees, nature paths, etc. Watertown is really lacking in concern for the environment."

"I wish the parks that are biked to were in better shape. Like the Skate Park. I see so many kids that bike there in the summer but that park is in need of an update."

"One of my major concerns has always been the lack of sidewalks in certain parts of town, or sidewalks that discontinue in areas."

"There should be more dedicated, separated bike lanes. Major roads such as Main are dangerous to bike on"

"With how wide our roads are in many areas, particularly the one-way streets, it would be very easy to reduce traffic down to one lane (helps with speeding) that has a protected bike lane."



SOCIAL PINPOINT

Social Pinpoint is a map-based online engagement tool that allows community members to leave comments on specific areas of the city using a map. Participants are then able to interact with each other, by 'liking' or 'disliking' other comments, or replying directly into a comment thread. Social Pinpoint was used in conjunction with the community survey to learn more about places like, ideas, and concerns respondents have related to biking and walking in Watertown.

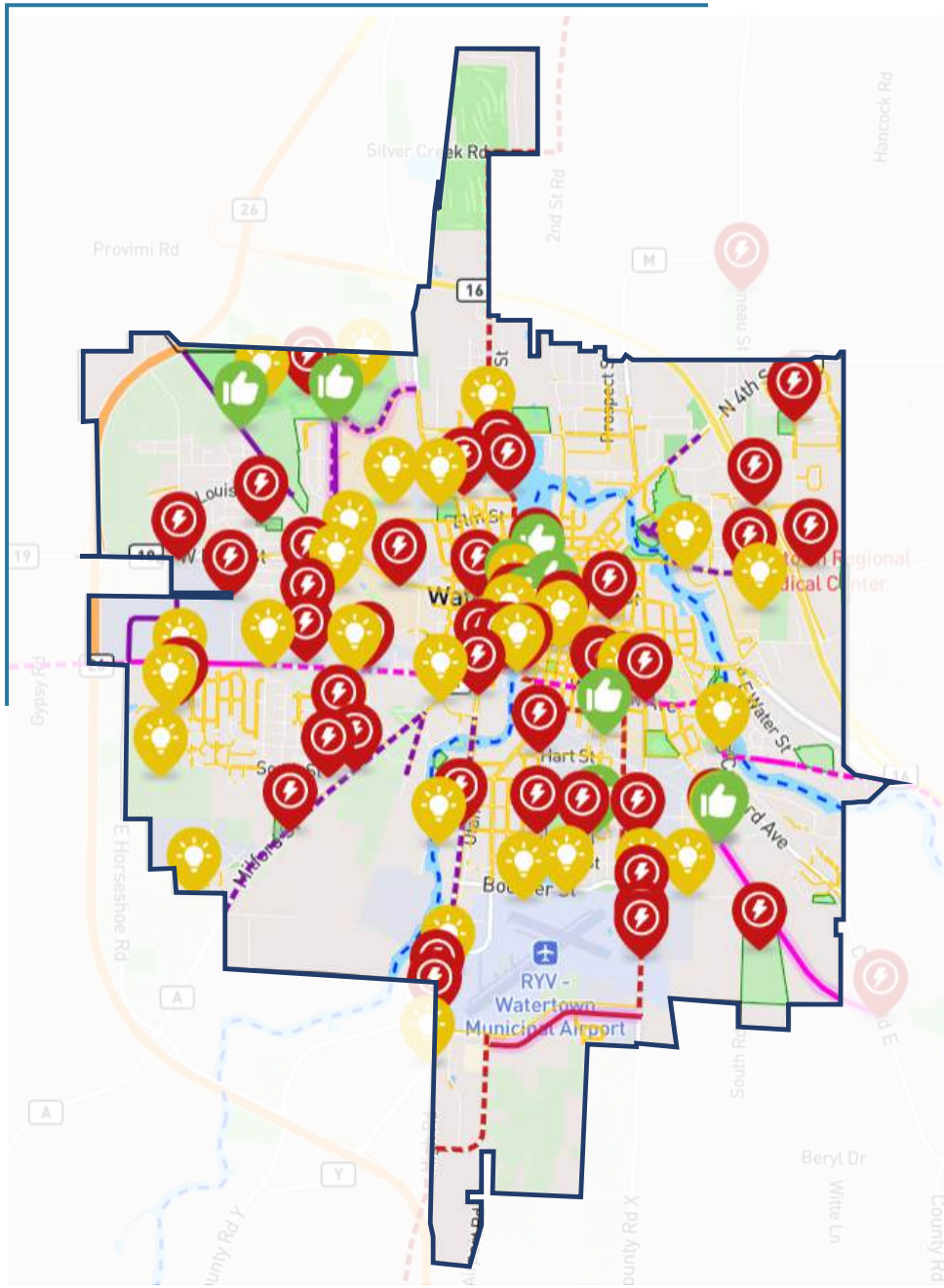
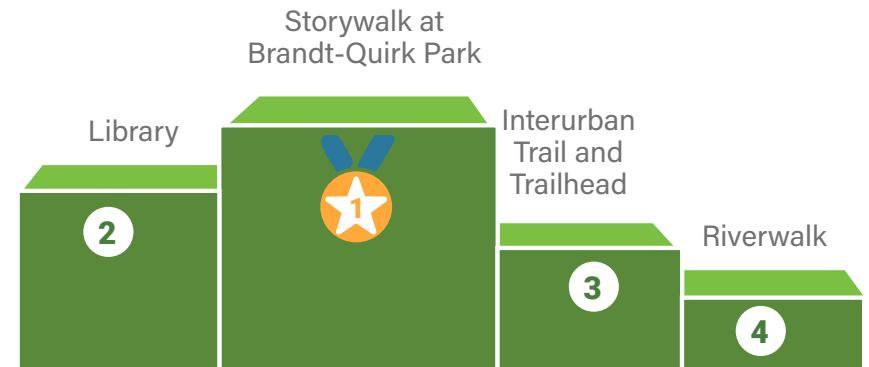


Figure 3.1 Social Pinpoint Results



Places People Like:



Top Concerns:

- » Crosswalk and intersection safety
- » Biking and walking along roadway shoulders
- » Need for bike lanes, bollards or separated trails
- » Lack of bike facilities downtown
- » Biking on narrow bridges
- » Incomplete sidewalk connections throughout Watertown



Top Ideas:

- » Fill gaps in the existing network
- » Improve sidewalk conditions
- » Enforcement / etiquette / education for drivers and bikers
- » Do not allow parking in bike lanes
- » Increase amount of separated trails
- » Add protected bike lanes



ISSUES & OPPORTUNITIES

Identifying the issues and opportunities within Watertown's bicycle and pedestrian network is an essential step in crafting goals and recommendations and developing an implementation strategy that meets the needs of the community and builds on where Watertown is today. The issues and opportunities listed below were identified based on the existing conditions analysis and community input, which is critical in understanding the lived experience of the residents and visitors who use Watertown's bicycle and pedestrian network.



KEY BARRIERS AND CHALLENGES

- » Lack of interconnected facilities that connect to all areas of Watertown.
- » Physical barriers, such as rail lines, highways, and the Rock River impact where facilities are easy or convenient to build. These barriers have shaped the placement of the existing facilities today.
- » Lack of or low visibility for amenities that support biking and walking in Watertown.
- » Need for upgrades at specific intersections and mid-block crossings for better visibility of pedestrians and bicyclists.
- » Need for consistent facilities for bicyclists to travel across the City that are more defined.
- » Lack of facilities to connect to destinations beyond Watertown.
- » One-way streets downtown are difficult for bicyclists to navigate and impede connectivity.



KEY OPPORTUNITIES

- » Create a network of well-connected bikeways and sidewalks that make choosing to bike or walk to work, school, or other common destinations a viable option for most residents.
- » Celebrate the Rock River with improved connections to parks, the Riverwalk, and water access locations for biking and walking.
- » Create a network that is safe and comfortable with considerations for young people, families, seniors, and people who are visiting or new to Watertown.
- » Promote Watertown as a biking destination by highlighting existing regional trail and route connections that run through Watertown.
- » Develop and support programs for education and awareness around road etiquette for all roadway users

Network Plan

4

GOALS: A VISION FOR THE FUTURE

INTRODUCTION

A set of goals were developed to guide the future bicycle and pedestrian system in Watertown. These goals were developed based on feedback collected from the community and vetted through the Bicycle and Pedestrian Plan Task Force and staff review. These goals are broad statements that express public priorities and are intended to guide decision-making in the coming years as projects and conditions change. The goals can be considered as a reminder of the overall vision of this plan and planning process.



GOAL 1

Increase the opportunities for residents and visitors of Watertown to choose multimodal transportation to travel throughout the city to get to school, work, or run daily errands. This means prioritizing safety for pedestrians and bicyclists through facility design, as well as implementing education and etiquette messaging to build a culture that is welcoming to multimodal transportation.



GOAL 2

Provide comfortable routes to connect all areas of Watertown for pedestrians and bicyclists of all ages and abilities, including seniors, young people, and people who depend on mobility devices.



GOAL 3

Provide appropriate facilities for pedestrians and bicyclists with consideration for data and context that support improvements. This information could include traffic volumes, traffic speeds, cost to implement, available right-of-way, parking, roadway circulation, opportunities to collaborate with agencies or funding sources, and community feedback.



GOAL 4

Prioritize connections for biking and walking to the Rock River, schools, parks, businesses, churches, community centers and libraries, retail, and medical locations, as well as regional destinations outside of Watertown.

NETWORK PLAN

The Network Plan in Figure 4.1 shows corridors with recommended future improvements and additions to the bicycle and pedestrian network. The Network Plan is based on a list of objectives that were developed over the course of the planning process, influenced by feedback collected through community engagement. Each facility type shown in the proposed Network Plan is detailed in the Facility Design Guidance section.

Facilities included in the Network Plan:

SHARED USE PATHS



TWO-WAY PROTECTED BIKE LANES



BIKE LANES



NEIGHBORHOOD BIKEWAYS



SIDEWALKS



NETWORK PLAN OBJECTIVES

- » Focus on connections to and through downtown Watertown, with additional use and consideration for the future of the Riverwalk.
- » If bike lanes are to be installed, find ways to show that they are protected bike lanes as much as possible (bike lanes with painted buffers or physical barriers such as concrete curb or flexible bollards).
- » Seek ways to connect to employment destinations (Walmart, KwikTrip, hospital, industrial areas).
- » Seek ways to create 'loop' trails – routes that circle back for recreational bike riding or walking.
- » Connect to the Rock River.
- » Connect the Interurban Trail to downtown Watertown.
- » Make the existing designated regional trails more visible.
- » Address lack of connection across Hwy 16 in the Meadowbrook Area.

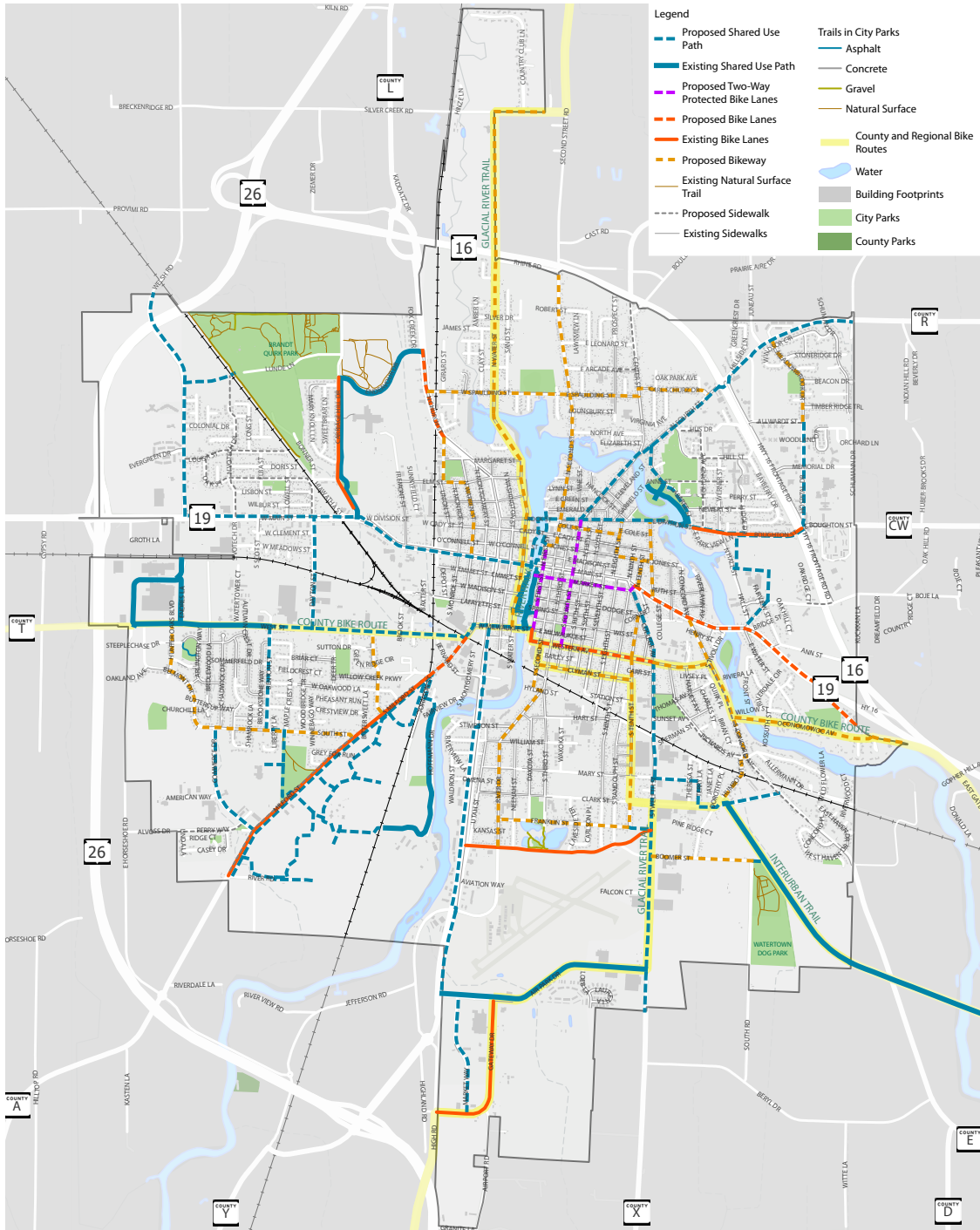
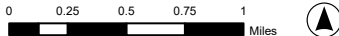


Figure 4.1 Network Plan



FUTURE OF THE RIVERWALK

The Riverwalk in Watertown today is a path that consists of a combination of shared use path (asphalt surfacing), concrete, and elevated boardwalk (wood deck surfacing). The Rock River District Plan proposed elevating the design and expanding the Riverwalk to the north and south. Watertown’s Redevelopment Authority (RDA) is taking the lead on implementation of the Rock River District Plan. Future redevelopment sites will have expanded access and pathways for the Riverwalk.

The goal is to provide a continuous, enjoyable circuit for walking and biking. Clear access points, widening of the path to accommodate bicycle traffic, and ensuring continuity with the path alignment are all goals for the future of the Riverwalk. As future segments of the Riverwalk are added, it is recommended to build the Riverwalk to a width of no less than 8’, with 10’ or 12’ as a preferred width. There are some preliminary plans to widen the existing Riverwalk path near the Community and Senior Center. Other ideas include integrating wayfinding specific to the Riverwalk to direct downtown visitors to the destination walk, or including interpretive signage or a storywalk along the loop (similar to what exists at Brandt-Quirk Park today).

In addition to providing a riverfront destination and leisurely place to stroll, the Riverwalk can also serve as an important connection within the overall bicycle and pedestrian network. The fully built-out network may take years to implement; in the interim, the Riverwalk can serve as a north-south connector for longer-distance biking and walking across the city. Pilot or temporary projects to encourage one-way travel can alleviate conflicts between bicyclists and pedestrians in the near-term. Future re-routing of the Glacial River Trail should consider use of the Riverwalk as the trail is expanded.

FACILITY DESIGN GUIDANCE

The following facility design guidance describes the facility types recommended within the Network Plan, along with guidance on design and applicability. Overall, the City should work towards implementation of consistent facility design through Standard Specifications within each facility type.

SHARED USE PATHS

Proposed shared use paths are paved trail segments, located off-street and physically separated from vehicle traffic by a boulevard and raised curb. They are generally located in parallel to a roadway but can also be located independently.

- » Preferred min. width: 8'; preferred width of 10' - 12' where possible
 - » Preferred surface: concrete or bituminous/asphalt surface
 - » Preferred separation from roadway: 4'-0" minimum
 - » Generally located along roadways with higher vehicle volumes (AADT 3,000 or above) or higher speed limits (above 35mph)
 - » Intended for bicyclists and pedestrians to share the path
 - In areas with heavy traffic, striping to indicate direction or separation of bicyclists from pedestrians may be necessary. Widening of the path may also alleviate congestion.
 - » E-bicycles, E-scooters, and personal mobility devices are allowed, with recommended speed limits
 - Faster-moving users should yield to pedestrians and slower-moving users
 - » Unless an alternate agreement is in place, property owners adjacent to shared use paths are responsible for keeping the path clear of debris, ice and snow (similar to a sidewalk)
- » In newly developed areas of Watertown, shared use paths should be constructed by the developers as shown on the Network Plan and/or as required by local ordinance
 - In developed neighborhoods with existing sidewalks, a shared use path can replace an existing sidewalk
 - » Where possible, shade trees should be planted or protected along shared use paths, with consideration for species selection and available soil volume to ensure adequate area for tree root zones
 - » There may need to be additional right-of-way allocated or an easement granted to construct a new shared use path
 - » This is the most preferred facility for biking and walking, according to feedback collected through community engagement
- » Shared use paths are more expensive to build and maintain than other on-street facilities (such as bike lanes), but also provide the most comfortable facility for most users
 - » Where shared use paths cross roadways, there should be:
 - Concrete pedestrian ramps with truncated dome detection surface
 - Green or white high visibility crosswalk markings
 - Rapid Flashing Beacons at mid-block crossings across high volume roadways (above 3,000 ADT)
 - Mid-block crossings are not recommended to be paired with this facility type in areas with speed limits over 35mph

Examples of Shared Use Paths



Shared Use Paths



The figure to the right depicts a design concept to show what a potential shared use path could look like, if implemented along Fourth St in Watertown. As shown, the existing sidewalk along the west side of Fourth St has been converted to an 8' wide shared use path. Construction of the shared use path is within the existing right-of-way (ROW).

Note that these are concept-level explorations and implementation of this type of facility will require further engineering analysis to determine specific dimensions and feasibility, along with impacts to trees, yards, and utilities.

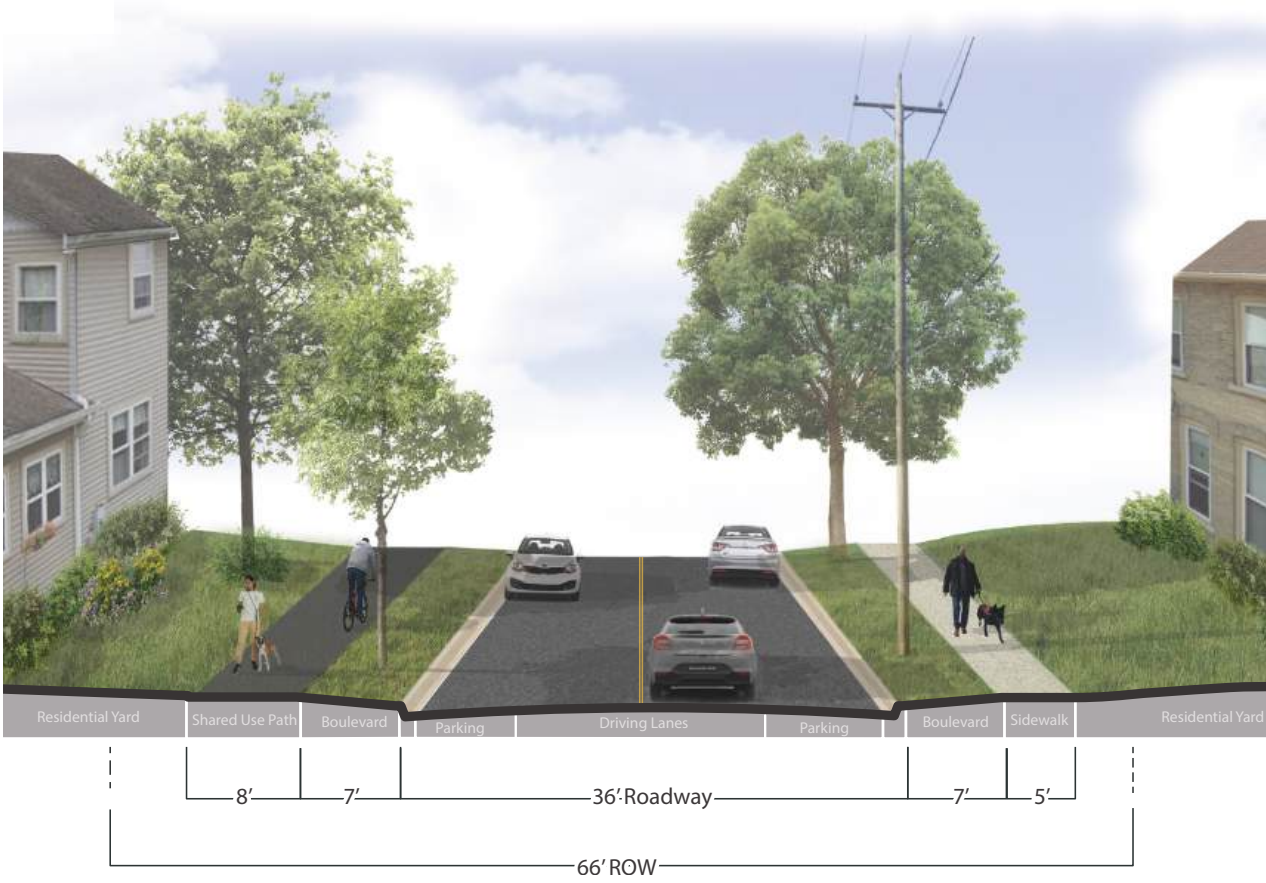


Figure 4.2 Cross section and plan view study of a shared use path along Fourth St.



Examples of Two-Way Protected Bike Lanes



TWO-WAY PROTECTED BIKE LANES

There are three segments within downtown Watertown where two-way protected bike lanes are proposed. This facility provides a single area for bicyclists to travel within the road (in both directions) with a physical barrier that separates vehicle traffic from bicycle traffic. A physical barrier can be a raised curb, jersey barrier, or flexible bollard.

- » Preferred width (total): 8 - 10' (where possible)
 - Width may be determined by the width of snow plowing equipment
- » Preferred surface: concrete or bituminous/asphalt surface as part of the roadway
- » Preferred separation from vehicles: 1 – 2' minimum (where possible)
- » Generally located along roadways with higher vehicle volumes (ADT 3,000 or above) or higher speed limits (above 35mph)
- » Intended for bicyclists only, although future expansion of this type of facility could be considered for shared use by pedestrians in lower-volume roadway areas
- » All state and local rules of the road pertain to two-way protected bike lane users
- » Snow and ice to be cleared as part of roadway plowing
- » Two-way protected bike lanes are proposed for areas where space is too constrained to build a shared use path
- » Preferred location along two-way vehicle roadways
- » Markings should follow MUTCD guidance
 - Solid green paint enhances visibility
- » Two-way protected bike lanes may be used in combination with shared use paths where there space is constrained (for example: along Fourth St at the Cemetery)
- » Where proposed in Watertown, this facility will most likely require removal of on-street parking along the side of the road where the two-way protected bike lane is proposed
 - This type of facility still allows for on-street parking to remain on the opposite side of the road, serving as a compromise for space allocation of different modes of transportation
- Count of parking spaces affected by the proposed Network Plan, as shown in Figure 4.1:
 - First St: approximately 44 on-street spaces (east side of street)
 - Fourth St: approximately 32 on-street spaces (west side of street)
 - Market St: approximately 61 spaces (either north or south side of street)
 - Note that the 2022 Parking Study showed capacity for on-street parking within downtown (see page 20)
- » A raised curb to separate the two-way bike lane from on-coming traffic is preferred at the intersection approach with solid green paint
- » Wide green high-visibility crosswalk markings are recommended at crossings

Examples of Bike Lanes



Bike Lane Example



Bike Lane Example with buffered stripe markings

BIKE LANES

Bike lanes are designated areas within the roadway for bicycle travel. Generally, bicycle lanes are located on both sides of the roadway and are designated with a solid white stripe marking with bike lane markings at intersections.

- » Preferred width (total): 3' minimum, 5' preferred (where possible)
- » Preferred surface: concrete or bituminous/asphalt surface as part of the roadway
- » Preferred separation from vehicles: solid white stripe
 - Where possible, an additional white diagonal striped buffer 1 – 2' wide is preferred to further separate vehicle traffic from bicyclists
- » Generally located along roadways with low to medium (ADT 3,000 – 6,000) with speed limits not to exceed 45mph
- » Intended exclusively for bicyclists only; on-street parking should be prohibited within bike lanes
- » All state and local rules of the road pertain to bike lane users
- » At intersections:
 - Green high visibility crosswalk markings
 - Bike Lane markings at block ends and intermittently along long block segments
- » Lower cost to implement than a shared use path
 - Could be implemented as part of a mill/overlay roadway improvement
- » Need to consider availability of boulevard or ditch alongside the roadway for snow storage and/or stormwater management
- » Additional coordination will be required with Jefferson and Dodge counties, and WisDOT, on a handful of proposed segments



NEIGHBORHOOD BIKEWAYS

Neighborhood bikeways are proposed areas with shared lane markings (also known as sharrows) to indicate an on-street bicycle facility that overlays with an existing vehicle travel lane.

- » Generally located along lower volume roadways (ADT 3,000 and below) with low speed limits not to exceed 25mph
- » Intended for bicyclists
- » All state and local rules of the road pertain to neighborhood bikeway users
- » Existing on-street parking can remain along these roadways
- » Marked with stand-alone bikeway pavement markings at intersections and 'SHARE THE ROAD' signage per MUTCD
- » Bicyclists are encouraged to 'take the lane' on bikeways
- » Neighborhood bikeways have low costs to implement and could be accomplished as a stand-alone project or as part of a mill/overlay project
- » Additional coordination will be required with Jefferson County on a few proposed segments
- » See wayfinding guidance in Chapter 5



Examples of Sidewalks



Downtown sidewalk



Curb ramp with crosswalk markings



Neighborhood sidewalk

SIDEWALKS

Proposed sidewalks are shown on the Network Plan primarily in residential areas where none exist today. Sidewalks are concrete walkways intended for use by pedestrians.

- » Preferred width: 4' minimum, 6' (where possible);
- » Preferred surface: concrete
- » Preferred separation from roadway: 4'-0" minimum boulevard
- » Generally located along roadways of all vehicle volumes and speed limits
 - With higher vehicle volumes (ADT 3,000 or above) or higher speed limits (above 35mph), a wider separation is recommended.
- » Intended for pedestrians; children younger than 12 are also permitted to ride bicycles on sidewalks (yielding to pedestrians)
- » E-bikes, E-scooters should not be permitted on sidewalks
- » Personal mobility devices travelling less than 15mph should be permitted on sidewalks
- » Sidewalks on both sides of the roadway should be built as part of all new developments within Watertown, unless shown as a future shared use path (in which case, the shared use path should be constructed as part of the development in lieu of one side of sidewalk along roadway)
- » ADA accessibility is required as part of the design (slope, pedestrian ramps, minimum widths and surface design should comply with US Accessibility Board standards)
 - Watertown is currently focusing on upgrading curb ramps throughout the city to meet WisDOT and ADA standards. Sites near schools and parks are priority projects.
- » Unless an alternate agreement is in place, property owners adjacent to sidewalks are responsible for keeping the path clear of debris, ice and snow
- » Where possible, shade trees should be planted or protected along sidewalks, with consideration for species selection and available soil volume to ensure adequate area for tree root zones
- » The proposed sidewalk locations on the Network Plan are focused on existing neighborhoods where gaps in the overall network exist today for pedestrians
 - Future sidewalk improvements have been located along streets that connect to collector or arterial roads (through streets) and have been informed through input from Watertown residents

INTERSECTION AND CROSSWALK TREATMENTS

Through the planning and community engagement process, many intersections in Watertown were identified as opportunity sites for safety and infrastructure upgrades. The map to the right keys the locations of these intersections with the accompanying table, which provides recommended treatments at each location. Treatment recommendations are based on best practices provided through state and national resources (NACTO, AASHTO, FHWA, Wisconsin Bicycle Facility Design Handbook, etc.). As segments of future sidewalks, trails, and bikeways are constructed in Watertown, these treatments should be considered with final design of improvements.

Definitions and example images of crossing treatments are detailed on page 58.

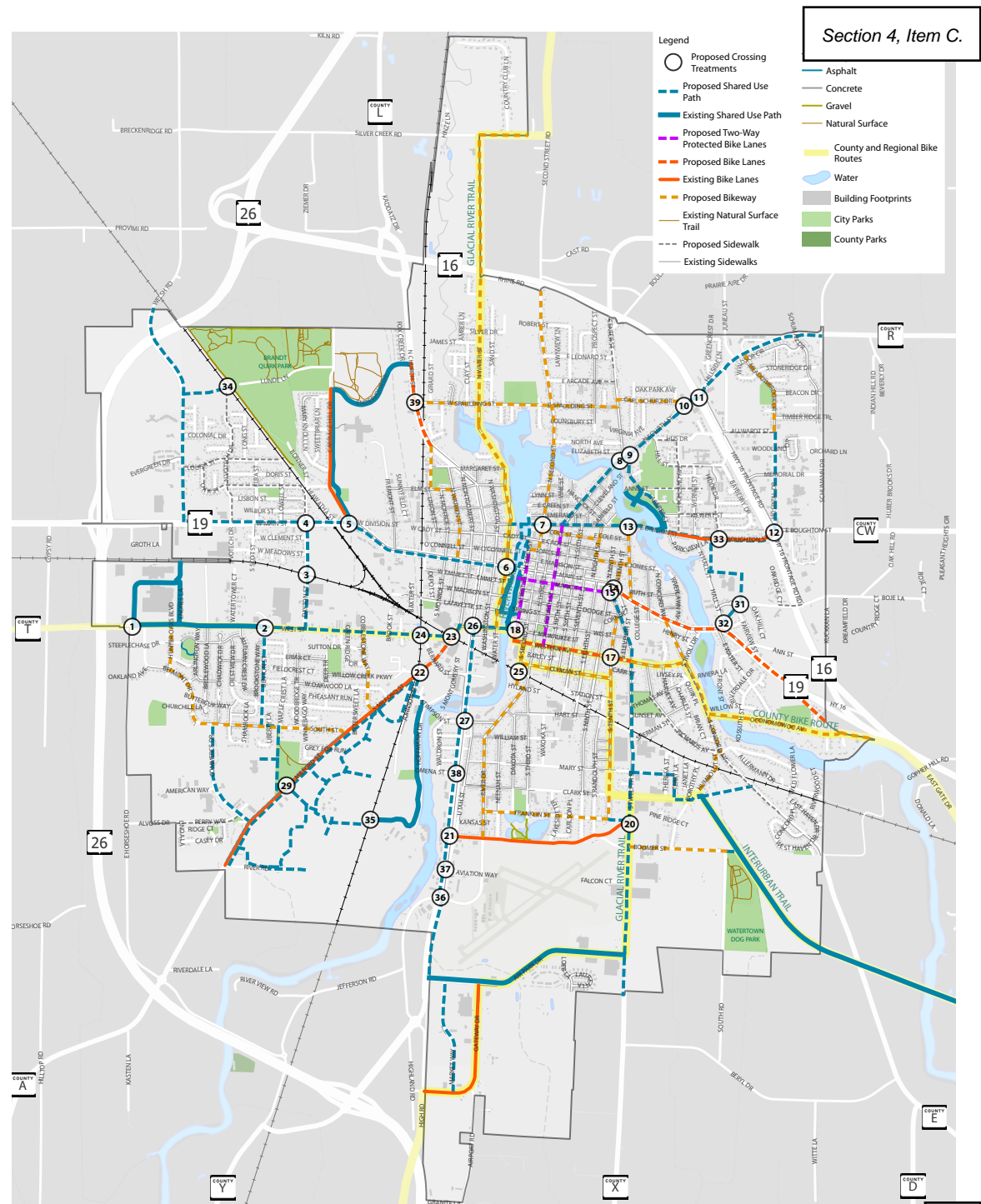


Figure 4.4 Network Plan with Crosswalk Treatment Locations

RECOMMENDED TREATMENTS

#	Location	High Visibility Crosswalk Markings	Bike Crosswalks	Ped Ramps	RRFB	Curb Extension	Trail Xing Sign	Share the Road Sign	Signal Timing/ LPI	Grade Separated Crossing (Tunnel)	Ped Refuge Island	Raised Crosswalk	RR Crossing
1	West St and Horseshoe Rd	x		x			x						
2	West St and Benton St	x		x	x	x	x						
3	Dayton St RR Xing north of West St.												x
4	Dayton St and W Main St	x		x	x	x							
5	W Main St and W Division St	x		x	x								
6	Water St and W Main St	x				x			x				
7	N Second St and E Division St	x					x						
8	N Fourth St at Center St	x		x	x	x							
9	Labaree St at 4th St	x	x	x									
10	N Fourth St at Carl Schurz Dr	x	x	x	x	x	x					x	
11	N Fourth St at STH 16	x							x	x	x		
12	Boughton St and STH 16 Frontage Rd	x		x	x						x		
13	Dewey Ave and E Division St	x	x	x									
14	Market St and Main St and 10th St	x	x	x									
15	10th St and Market St	x	x	x									
17	10th St and Western Ave	x	x	x									
18	1st St and Milwaukee St	x	x	x									

Table 4.1 Recommended Intersection and Crosswalk Treatments

RECOMMENDED TREATMENTS

#	Location	High Visibility Crosswalk Markings	Bike Crosswalks	Ped Ramps	RRFB	Curb Extension	Trail Xing Sign	Share the Road Sign	Signal Timing/ LPI	Grade Separated Crossing (Tunnel)	Ped Refuge Island	Raised Crosswalk	RR Crossing
20	12th St and Boomer St	x	x	x								x	
21	Church St and Boomer St	x	x	x		x	x		x		x		
22	Milford St and Johnson St	x	x	x									x
23	Milford St and West St												x
24	West St near Bernard St												x
25	2nd St under RR							x					
26	Church St and Milwaukee St	x	x	x		x			x				
27	Church St and Simpson St	x	x						x				
29	Milford St at Grinwald Park	x	x		x	x						x	
31	E Main St and Riverside MS	x											
32	E Main and Hall St	x											
33	Boughton and Hidde Dr	x	x		x	x						x	
34	Lunde Ln / Brandt-Quirk Park												x
35	Hoffman Dr and RR												x
36	Church St and Jefferson Rd	x		x		x	x		x		x		
37	Church St and Aviation Way	x		x	x	x	x				x		
38	Church St and Omena St	x		x	x	x	x				x		
39	Church St and Spaulding St	x		x	x	x		x					

Table 4.1 Recommended Intersection and Crosswalk Treatments (continued)

INTERSECTION TREATMENTS



High Visibility Crosswalk Markings: Pavement marking patterns at crosswalks that are visible to both the driver and pedestrian from farther away. Recommended pattern: continental.



Bike Crosswalks: Pavement marking patterns at crosswalks that are specific for bicycle travel and usually painted green.



Raised / Tabletop / Speed Table Crosswalks: Crosswalk design that maintains the sidewalk or path elevation for the pedestrian, providing more visibility to motorists.



Pedestrian Ramps: Concrete ramps designed for ADA accessibility to connect sidewalks and paved paths to roadway intersections. Detectable warning surfaces with truncated domes are recommended to be included in pedestrian ramp design.



Rapid Flashing Beacon (also known as RRFB – Rectangular Rapid Flashing Beacon): Active warning beacon that uses rapid, irregular flashes of light to alert motorists to yield to pedestrians and bicyclists at crossings and intersections.



Curb Extensions: Sometimes called bump-outs, curb extensions narrow the roadway at intersections and mid-block crossings to shorten crossing distances for pedestrians and make pedestrians more visible to drivers. Curb extensions also provide an opportunity to integrate landscaping, stormwater management, or site furnishings into the streetscape.



Trail Crossing Sign: Diagonal yellow sign alerting motorists that a trail crossing is nearby.



Share the Road Sign: Signage reminding motorists to share the road with bicyclists. This type of signage pairs with on-street bikeways.



Signal Timing / Leading Pedestrian Interval: Adjustment of pedestrian crosswalks signal timing that provides a head start for pedestrians to cross an intersection while all vehicles are at a stop. This type of signal timing works well paired with 'no turn on red' signage.



Grade Separated Crossing: A bridge or a tunnel dedicated for pedestrian or bicycle crossing of a busy roadway. These treatments require significant space allocation and are expensive to build; however they provide the most separation for pedestrians and bicyclists at crossings.



Pedestrian Refuge Island: Protected island, usually made with a raised concrete curb, centered on a crosswalk to shorten the distance that pedestrians are exposed to traffic.

System Recommendations

5

INTRODUCTION

To support the bicycle and pedestrian network, a series of system recommendations are provided in this section that address facility amenities. The following recommendations provide guidance on dimensions, materials, applicability, and placement for trailheads and trail access points, bike parking, wayfinding, bridge and railroad crossings, and best management practices for landscaping and stormwater management as the network plan is implemented.



TRAIL ACCESS POINTS

Trail access points are designated public access locations for a trail or path, often located at the endpoints of a path as well as at key locations along the way. The trail access points, sometimes referred to as trailheads, provide a spot for people to meet when heading out to use a trail, or provide a place to rest with park-like amenities to support longer-distance trail users. The map below outlines ten trail access point locations within Watertown, most of which are in alignment with existing trailheads, parks, or open spaces. Many of these access points are within existing City parks that have some on- or off-street parking available currently. As the shared use path network is built-out, consideration for upgraded amenities at the trail access points should be folded into the design.

The following lists recommended amenities and definitions. Note that not every amenity listed below is required at every trail access point. A combination of amenities should be considered that fit the existing context, available space, and need. Table 5.1 outlines suggested amenities at each location shown on Figure 5.1

Tag	Location	Amenities									Public Art	Electricity
		Vehicle Parking	Bike Parking	Bike Repair Station	Kiosk / Wayfinding	Restrooms	Drinking Fountain	Seating & Shade	Interpretive Elements			
A	Interurban Trail	Existing	Existing	Existing	Existing	Existing	Existing	Existing	Existing	Existing	Existing	Existing
B	Riverwalk and Market St	Existing: Designate for Trail	Recommended	Recommended	Recommended		Recommended	Recommended	Recommended: Storywalk	Recommended	Recommended	Recommended
C	Riverwalk and Community /Senior Center	Existing: Designate for Trail	Existing	Recommended	Recommended	Existing Access	Existing Access	Existing	Recommended: Storywalk	Recommended	Existing Access	
D	Fanny Lewis Park	Existing: Designate for Trail	Recommended	Recommended	Recommended	Existing: Portable	Recommended	Existing	Recommended	Recommended	Recommended	
E	Riverside Park	Existing: Designate for Trail	Recommended	Recommended	Recommended	Existing	Existing	Existing	Recommended	Recommended	Existing	
F	Brandt-Quirk Park	Existing: Designate for Trail	Recommended	Recommended	Existing: Update	Existing	Recommended	Recommended	Existing: Storywalk	Recommended		
G	Grinwald Park	Existing: Designate for Trail	Recommended	Recommended	Recommended		Recommended	Recommended	Recommended	Recommended		
H	Rock River Ridge Park		Recommended	Recommended	Recommended		Recommended	Recommended	Recommended	Recommended		
I	Walmart	Existing: Designate for Trail	Recommended	Recommended	Recommended	Existing	Recommended	Recommended	Recommended	Recommended	Recommended	Recommended

Table 5.1 Recommended Trail Access Points and Amenities

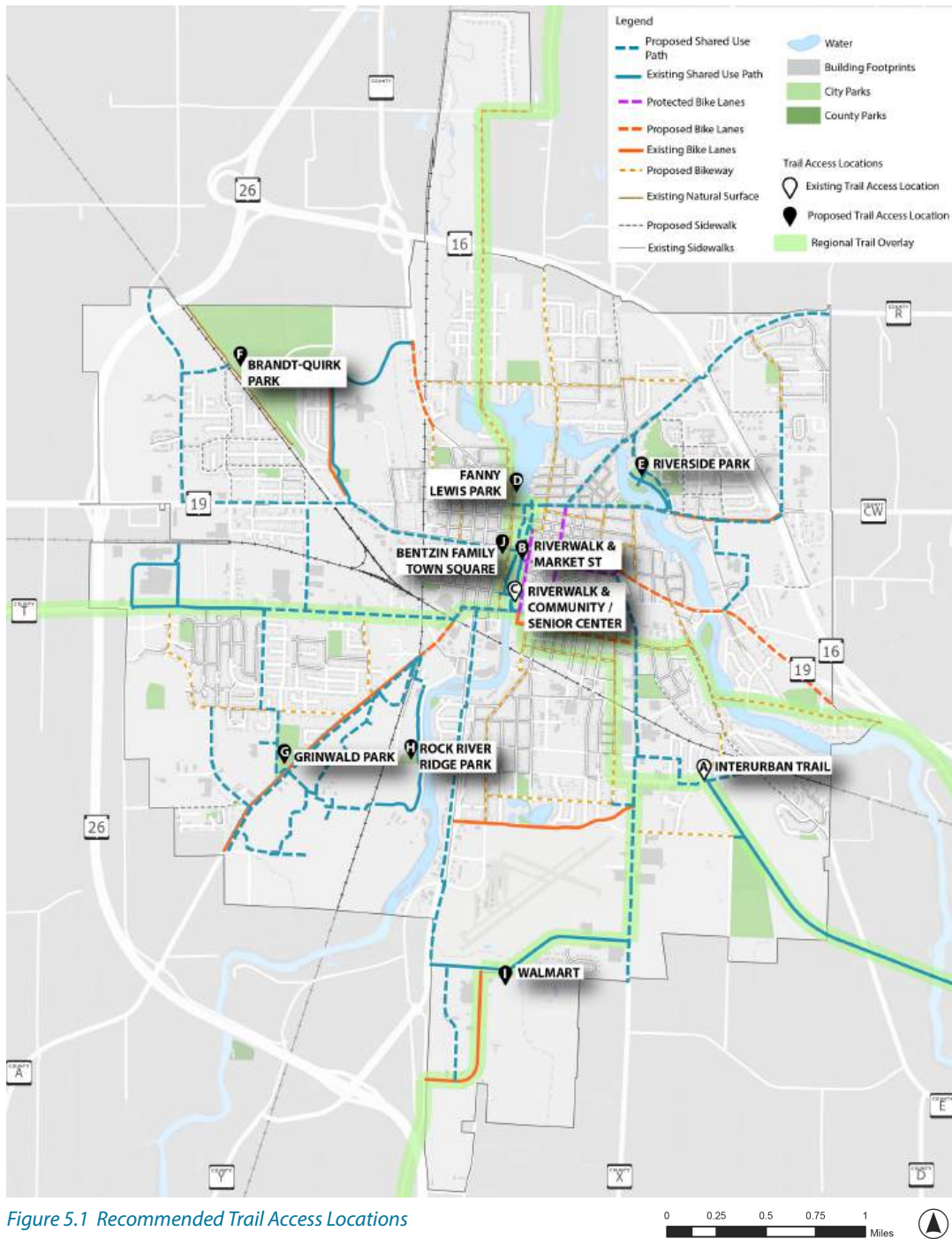


Figure 5.1 Recommended Trail Access Locations





VEHICLE PARKING

Parking stalls provide a means for visitors travelling from further regional or statewide distances away to park and access the trail network. Additionally, people who rely on mobility devices or who have disabilities can use nearby accessible parking stalls to easily access the trail network. For trail access points, it is recommended to provide 1 to 2 ADA accessible parking stalls at a minimum.



BIKE RACKS AND BIKE REPAIR STATIONS

Bike racks are useful to provide a means to dismount and walk to a nearby destination or use other trailhead facilities. Bike repair stations (sometimes called Fix-it Stations) can be handy if one needs to pump up tires or make minor repairs to their bicycle while using a trail. It is recommended to provide bike racks at all trail access points. See the section on Bike Parking for bike parking design recommendations.



KIOSK / WAYFINDING

Directional signage and maps are commonly found at trailhead locations to orient trail users who are exiting or entering the trail network. Wayfinding signage can also be placed in the vicinity of a trail or route to direct people to the trail corridor, or to other nearby destinations. A stand-alone kiosk with a map of the city is a useful tool for visitors coming to Watertown. Today, the Interurban Trailhead has a kiosk sign with maps and posted community information. Other potential locations for a kiosk include Fanny Lewis Park, Bentzin Family Town Square, Riverwalk & Market St, and Walmart. It should be noted that a long-term project identified in the Watertown Parks and Open Space Plan is to implement kiosks in all City parks.



RESTROOMS

At heavily used trailheads, or in combination with other recreational facilities, restrooms can be a great amenity along a trail, providing individuals and families an option for making a day out of biking, walking, or rolling along the City's network. Future restroom facilities require consideration for regular maintenance and security. Many of the identified future trail access locations already have public access permanent or portable restrooms (Interurban Trailhead, Fanny P Lewis Park, Grinwald Park, Riverside Park and Brandt-Quirk Park) or public restrooms very near (Bentzin Family Town Square near the Watertown Library and the Riverwalk location near the Community and Senior Center) and may just require simple wayfinding signage at the trail access location to direct people to the facilities. Future permanent or portable restroom facilities should be considered at the Walmart and Riverwalk/Market St locations, both of which will require coordination with private property owners.



DRINKING FOUNTAINS

Staying hydrated is an important part of a comfortable trail user experience, and drinking fountains should be considered where possible at locations with utility access. Today, there are drinking fountains at the Interurban Trailhead, Benzin Family Town Square, and Riverside Park.

SEATING AND SHADE

Placing benches and other seating at trail access points is a great way to provide amenities to serve elderly people. Seating that is durable, protected from the sun, and distanced from vehicle traffic is generally preferred. Picnic tables and places to gather and eat food are also encouraged at trailhead locations. Shade trees near trails are vital for providing a cool and comfortable trail experience. Where possible, incorporate shade trees on the south side of trail access areas and rest locations.



INTERPRETIVE ELEMENTS AND/OR PUBLIC ART

Beyond serving a primary function of providing access to a trail network, a trail access point can also serve as a means for placemaking or celebrating the local character and identity of the City of Watertown. Small areas planted with native, hardy plants or integrated public art showcasing local artists can activate these small public spaces and serve as landmarks within the network. Interpretive elements describing historical, cultural, or ecological information about the area can also be incorporated into a trailhead location.

ELECTRICITY / CHARGING STATION

Electrical receptacles, either as a part of a lighting fixture, or as a stand-alone solar-powered station, can provide a place for people to charge cell phones and batteries for power-assisted mobility devices. This amenity can make it possible for people who use mobility devices to take longer trips via the network. City-operated charging stations for electrical vehicle at trailhead parking areas is also an amenity to consider.





BIKE PARKING

Bike Parking is an essential component for people wanting to use bicycling as a means of transportation to school, work, community destinations, shopping and errands. Bicycle parking for short-term use is also a low-cost outdoor furnishing that can be easily installed in most locations throughout Watertown. The best locations for bike parking are along streets in commercial areas (Main Street, for example) and near business entry locations. Single, stand-alone racks are best when located along a street, while corals or racks with the ability to store multiple bicycles together are better paired with parking lots at business entries. Most racks need to be installed on a concrete, level surface and require adequate clear space around them for maneuvering a bicycle. Other considerations are sightlines and overall visibility, ADA pathways connecting from sidewalks and trails, and lighting.

Currently, the City of Watertown has metal-coated City-branded bicycle racks located throughout downtown and within City parks. Overall, these bike racks have not been very successful: many people do not recognize them as bicycle parking, they are difficult to attach a lock to for many bicycle styles, and many are rusting and deteriorating from salt spray. This plan recommends selecting a new style for a City-branded bike rack and installing / replacing new racks incrementally throughout the city over the next 5 years. Additionally, the City could consider implementing a cost-share program with businesses to provide bike racks at low-cost for participation in the program. Below are examples of bike rack styles recommended for this application [From APBP Essentials of Bike Parking, September 2015].



INVERTED U

Common style appropriate for many uses; two points of ground contact. Can be installed in a series on rails to create a free-standing parking area in variable quantities. Available in many variations.



POST & RING

Common style appropriate for many uses; one point of ground contact. Compared to inverted-U racks, these are less prone to unintended perpendicular parking. Products exist for converting unused parking meter posts.



WHEELWELL - SECURE

Includes an element that cradles one wheel. Design and performance vary by manufacturer; typically contains bikes well, which is desirable for long-term parking and in large-scale installations (e.g. campus); accommodates fewer bicycle types and attachments than the two styles above.



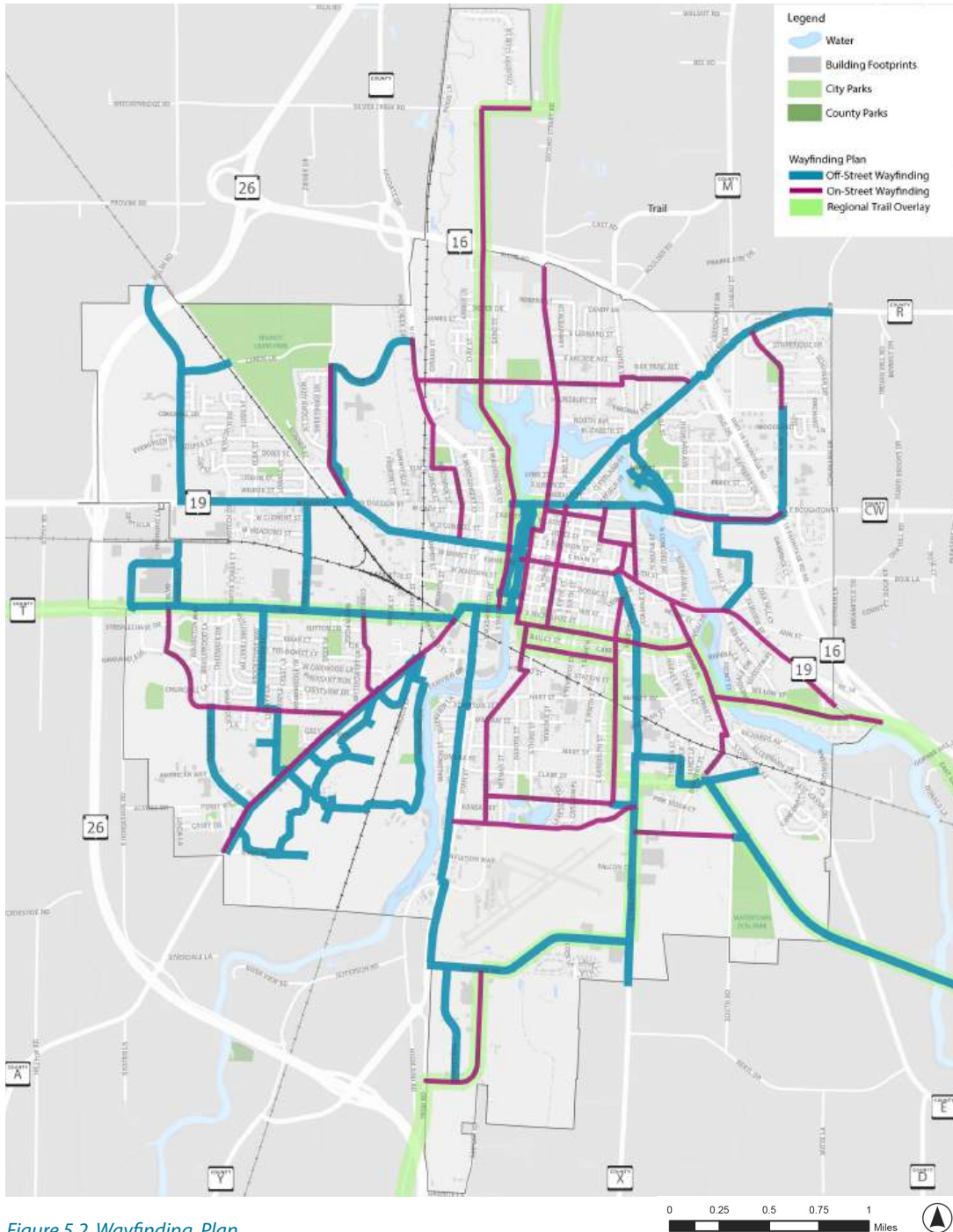


Figure 5.2 Wayfinding Plan

WAYFINDING

Clear, consistent, and easy to understand wayfinding is an essential component for a bike and pedestrian network. Wayfinding helps people navigate throughout a system, utilizing a variety of signage types and sometimes in combination with online mapping. Basic principles of wayfinding are listed below (based on the NACTO Urban Bikeway Design Guide):

- » Clarity and Consistency: Wayfinding should be easy to read and understand, even briefly, and for everyone, including people with limited literacy or whose primary language is not English.
- » Consistency: Wayfinding should use consistent symbols, colors, and fonts.
- » Visibility: Wayfinding should be visible to pedestrians and bicyclists, which may require locating it in different places, depending on the situation.

A Wayfinding Framework has been developed for Watertown, which outlines a series of signage types linked to the facility types shown in the Network Plan. **It is recommended that the City develops a full wayfinding and signage plan with design and construction-level graphics that can be implemented consistently throughout the City as new trail, sidewalk, and bikeway projects are constructed.**

OFF-STREET WAYFINDING (SHARED USE PATH NETWORK)

- » Directional signage at trail access points and at decision points, with approximate distances called out to nearby community destinations.
- » Trail crossing signage at intersection crossings.
- » Directional signage at alleyways that have been improved for pedestrian use throughout Downtown Watertown.



ON-STREET WAYFINDING (BIKEWAYS AND BIKE LANES)

- » BIKE ROUTE and BIKE LANE signage along routes, visible to vehicles and meeting MUTCD standards.
- » SHARE THE ROAD signs, visible to vehicles and meeting MUTCD standards.
- » Directional signage at decision points with approximate distances called out to nearby community destinations.



REGIONAL TRAIL OVERLAYS

- » Branded signage consistent with County or Regional Trail branding.
- » Additional signs or badges to accompany the On- and Off-Street Wayfinding



RAILROAD CROSSINGS

A safe at-grade rail trail crossing depends on clear signs, strong safety messages, and a surface that lets people walking or biking move comfortably across the rails and flangeways. It also requires selecting crossing designs that match the specific type of crossing and the surrounding environment, ensuring the treatment fits the conditions on the ground. The strategies in this section are common approaches used in typical situations. Before any of them can move forward, they'll need more detailed engineering review and coordination with the railroad. These ideas are meant to offer options and help support those conversations.

Figure 8E-5. Example of a Crossbuck Assembly at a Pathway or Sidewalk Grade Crossing

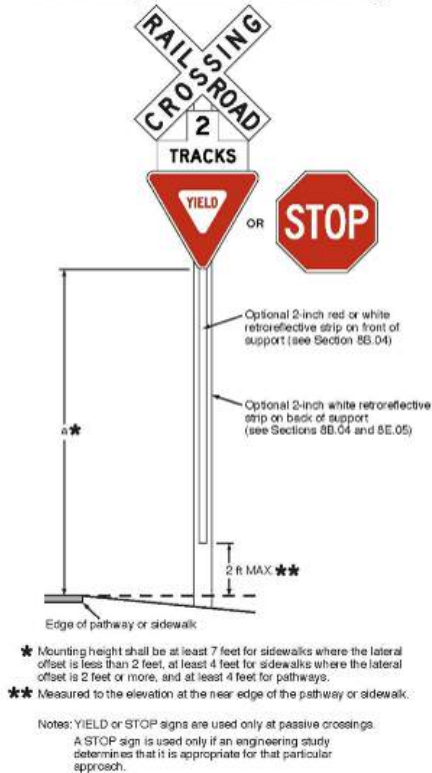


Figure 5.3 WMUTCD, Fig 8E-5

PASSIVE WARNING DEVICES

Passive warning signs for railroad crossings are non-electronic, non-active signs and markings that alert people to approach the tracks, but don't use lights, gates, or bells. They rely entirely on the trail user or driver noticing the sign and acting accordingly. When considering passive signs, the Wisconsin Manual on Uniform Traffic Control Devices (WMUTCD) should be used as a reference since it provides clear examples of regulatory and warning signs and plaques for crossings, along with short explanations of when each is intended to be used. In general, the signs follow standard WMUTCD guidance.

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Trail users being cautioned at a trail crossing in Chattanooga, Tennessee.



Signage and trail surface shown, along with an angled crossing in Chattanooga, Tennessee



Signage that warns trail users of the railroad angle crossing the trail, Minneapolis Cedar Lake Regional Trail.

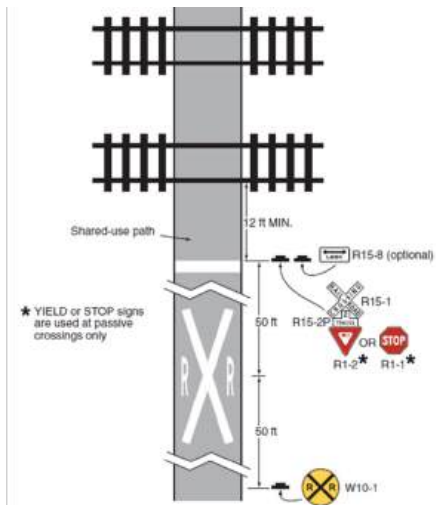


Figure 5.4 WMUTCD, Fig 8D-1

PAVEMENT MARKINGS

Pavement markings are another form of passive signage. However, the WMUTCD does not require pavement markings at trail-rail crossings, but states if users travel faster than pedestrians (such as bicyclists, scooter-users, skaters), using warning signs and pavement markings in advance of the pathway grade crossing should be considered.

Pavement markings may include an "X", the letters "RR", and/or a stop bar line. For unpaved trails, consider paving the crossing approach to install the appropriate pavement marking and to provide a smooth crossing. If paving the approach is not possible, installing additional devices is appropriate.

The WMUTCD also includes additional notes on standard pavement markings (see Figure 5.4), specifically for situations where LOOK (R15 8) signs aren't used or where a stop line is used instead of a yield line.

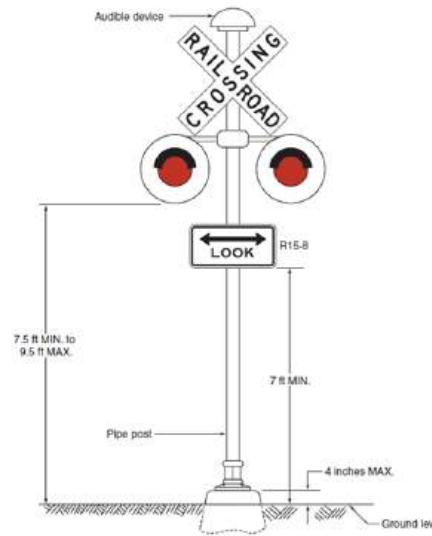


Figure 5.5 Flashing light signal for pedestrian crossings, WMUTCD Fig 8C-4

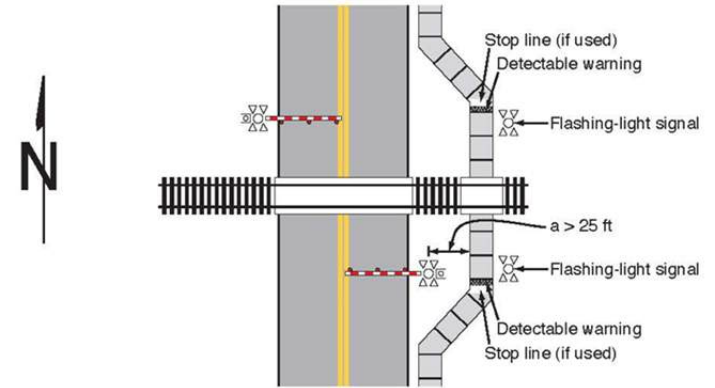
ACTIVE WARNING DEVICES

Active warning devices differ from passive ones because they change their appearance when a train is approaching. This gives trail users a clear indication that a train is near or already present. Examples include bells, flashing light signals, automatic gates, and similar equipment.

The WMUTCD offers guidance for pedestrian and bicycle signals at pathway and rail crossings, although the recommendations are not highly detailed. The FHWA Railroad-Highway Grade Crossing Handbook provides additional information. For example, flashing light signals paired with swing gates are generally suitable when trains operate at high speeds, when sight distance is limited, when multiple tracks are present, or when temporary obstructions such as stopped freight cars affect visibility.

The WMUTCD also notes that sidewalks and recreational trails typically function as two way facilities. Because of this, gates can trap users within the track zone if no escape route is available. Thus, automatic gates should not be placed across sidewalks, multi use trails, or bike paths unless they are specifically designed to allow users to exit safely, as stated in WMUTCD section 8E.09.

Figure 8E-2. Example of an Active Traffic Control System for a Sidewalk or Pathway Grade Crossing



Notes: The dimension "a" is the distance from the edge of the sidewalk grade crossing to the center of the traffic control warning devices at the grade crossing. If $a \leq 25$ feet, flashing-light signals may be omitted at a pathway or sidewalk grade crossing (see Section 8E.07).

Figure 5.6 WMUTCD Fig 8E-2



LIGHTING

Lighting can help improve visibility at crossings, but it should be aimed at the trail surface rather than toward a train engineer's line of sight. While lighting is not strictly required, it can be added along the approach to the crossing or at the crossing itself if it would enhance visibility or user safety.

Figure 8E-1. Illustration of the Difference between a Pathway Grade Crossing and a Sidewalk Grade Crossing

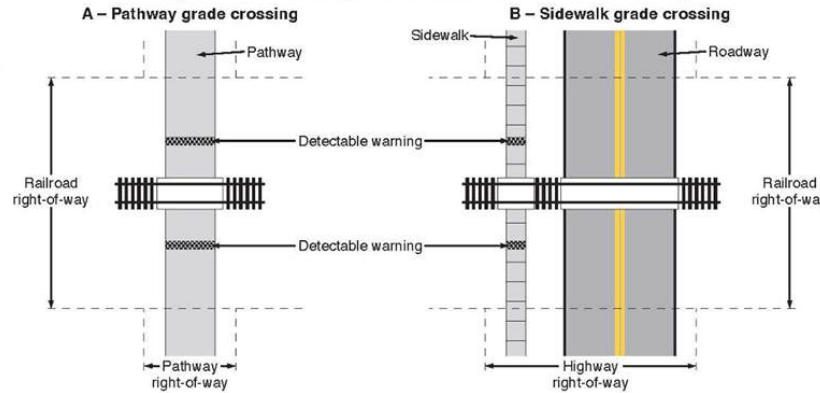


Figure 5.7 WMUTCD Fig 8E-1

APPROACH GRADES AND ANGLED CROSSINGS

When designing trail approaches at rail crossings, both grade and crossing angles play an important role in user safety and comfort.

General guidance calls for grades of five percent or less, but greater grades are allowed for short distances and in specific circumstances. The trail approach should be at the same elevation as the track, steep grades on either side could cause bicyclists to lose control, may distract trail users from crossing conditions, and block sight lines.

Another issue, particularly for bicyclists and people with disabilities, is the angle of a crossing. Crossing at right angles to the tracks avoids the hazard of bike tires getting caught in the flangeway grooves. The AASHTO Guide states that the likelihood of falls decreases when the roadway or shared use path crosses the tracks at 90 degrees (Rails with Trails, AASHTO).

Figure 5.8 A 45-degree rail-trail crossing, Rails with Trails

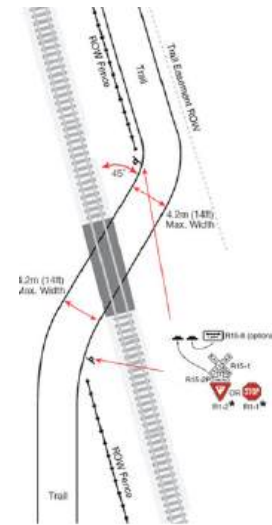


Figure 5.9 A 45-degree rail-trail crossing, Rails with Trails

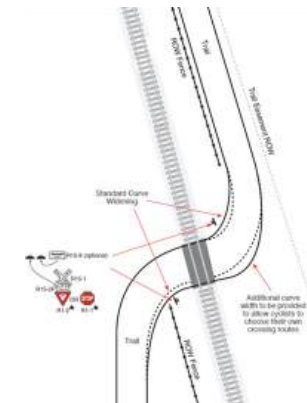
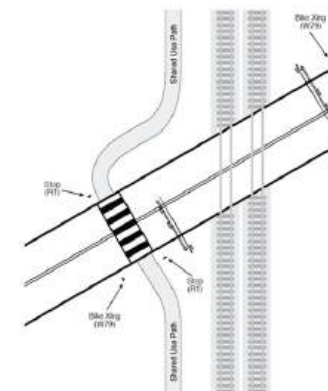


Figure 5.10 Angled intersection of a rails-trails-road crossing, Transportation Research Board, 2018



SIGHT DISTANCE

Clear visibility of a rail crossing is essential to help trail users recognize and react to it in time. Maintaining advance notice of the crossing with adequate sight distance is crucial. In highway intersection design, the stopping sight distance is usually twelve feet clear of the nearest rail. On trails, these points are typically marked with pavement markings placed in advance of the crossing. Traffic control devices such as crossbuck signs also need to be installed ahead of the crossing, following WMUTCD standards.



Quiet Zone Signage



Quiet Zone Signage

QUIET ZONES

Federal Rail Administration (FRA) is committed to reducing grade crossings while establishing a consistent standard for communities that opt to maintain quality of life for residents by establishing quiet zones where routine use of train horns at crossings is prohibited. Communities must comply with FRA standards and safety measures, which often means substantial infrastructure upgrades such as installing additional gates, medians, and signage to maintain public safety. This is especially important for pedestrian and bicycle crossings, because removing train horns requires additional safety measures to ensure users are properly warned when a train is approaching.

Minimum standards include:

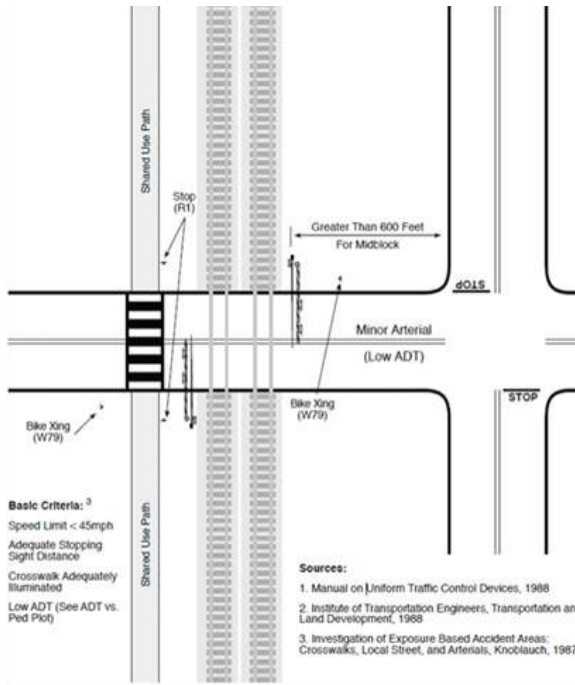
- » Each crossing within the desired Quiet Zone must have gates, flashing lights and bells, constant warning time devices, and power-out indicators.
- » The Quiet Zone can include one crossing or multiple crossings but must be at least ½ mile long.
- » Warning signs of the crossing alert drivers that they're entering a quiet zone.

The WMUTCD requires a "NO TRAIN HORN" sign or plaque in each direction at highway-rail grade crossings in a quiet zone, this feature should be considered at trail-rail grade crossings too. In Quiet Zones, adequate approach angle and sight distances are critically important, and lighting may be recommended more at these crossings.

TRAIL CROSSING SURFACE MATERIALS

The smoothness of the crossing surface plays a major role in the safety and comfort of trail users. Uneven surfaces or bumps can cause cyclists to lose control and can be especially challenging for wheelchair users. When the surface is in poor condition, users often focus on finding the smoothest path through the crossing, which can distract them from warning devices or oncoming trains.

According to AASHTO, the four most common materials used at grade crossings are concrete, rubber, asphalt, and timber. Concrete generally performs the best, even in wet conditions, because it provides the smoothest ride. Rubber surfaces can be comfortable when new, but they tend to become slippery when wet and degrade over time. Asphalt offers a smooth surface initially, but it can heave or settle and requires maintenance to prevent buildup along the tracks. Timber wears down quickly and becomes slick when wet, making it the least reliable option.



Unsignalized crossing type, Transportation Research Board.



Crushed rock is used between the rails as trail surface, which could be a cheaper option. Poudre River Trail, Colorado, Transportation Research Board.



An example of a pedestrian trail-rail crossing with sufficient surface materials, Transportation Research Board.

BRIDGE CROSSINGS AND CONSTRAINED AREAS

Designing new pedestrian or bicycle facilities can be challenging when space is limited. Too often, pedestrian or bicycle elements are the first to be reduced or eliminated within a roadway project, resulting in no meaningful improvements for bicyclists and pedestrians. Although these decisions can be difficult, a range of design strategies can help ensure streets support all modes and users of all abilities. Many of these design strategies are explained in the MUTCD and other resources.

When space constraints prevent full width bicycle lanes or fully separated pathways, several adaptable treatments can still be applied to enhance safety, visibility, and user comfort. The strategies that follow offer practical options for improving multimodal access within tight right of way conditions.

- » Convert existing sidewalk to serve as a narrow shared use path where there is not enough room for separate on-street facilities. Removal of sidewalk on one side and widening opposite side sidewalk to create a shared use path will depend on the structural components of the bridge.
 - This is a prevalent scenario today in Watertown along the (11) bridges that cross the Rock River within the City limits. Most bridges today have sidewalks installed on both sides of the roadway crossing.
 - To convert an existing sidewalk to accommodate pedestrians and bicyclists for short segments, curb ramps (from on-street bike lanes) will need to be installed.
- » Signage is needed to direct route for bicyclists.
- » Potential for a 'dismount' zone for bicyclists in areas with heavy pedestrian traffic.
- » Solid white striped bike lane is recommended, with flexible bollards and/or diagonal striped buffer (if there is room) within the roadway.
- » Future bridge reconstruction or renovations should include width for separated bicycle and pedestrian facilities.
- » Additional pedestrian bridges have been recommended as part of the Rock River District Plan, which will enhance pedestrian and bicycle connectivity throughout downtown Watertown. The planned pedestrian and bicycle bridge linking Division St (east) and Rock St (west) coincides with the Network Plan in this chapter.

Precedent Images

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Bike and pedestrian bridge addition



Curb ramp from on-street bike lane



"Walk Your Wheels" dismount zone markings

Precedent Imagery



Rainwater garden and curb cuts



Native plants as a protected bike lane buffer



Permeable paver paths

LANDSCAPING AND STORMWATER MANAGEMENT

As new shared use paths, sidewalks, and intersection improvements are implemented, there are opportunities to incorporate landscaping and Best Management Practices to address stormwater management. Each project should be considered for pairing with treatments such as:

- » Native, hardy and salt-tolerant landscaping at crosswalk extensions and medians, potentially with curb inlets and integrated catch basins.
- » Native, hardy and salt-tolerant landscaping as a potential vertical element for a protected bike lane.
- » Linear landscaping with native, hardy, and salt-tolerant species and shade trees along walkways for interest, comfort, and pollinator habitat.
- » Native landscaping featured as part of the site design for trail access locations, paired with public art or interpretive elements.
- » Permeable pavers along pedestrian walkways and potentially as surfacing for shared use paths.
- » Raingardens and biofilters can be integrated into linear landscapes along shared use paths, sidewalks, and adjacent to parking lots.

For more information about how to integrate green infrastructure into the future network, see the City of Watertown's Green Infrastructure Practices for Low Impact Development – a guide for planning and installing stormwater features that mimic natural processes within the urban environment.

Policy Recommendations

6

INTRODUCTION

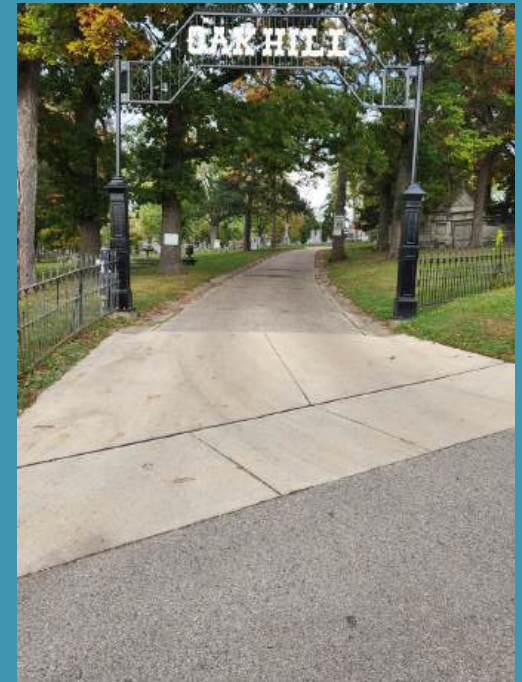
Over the course of the development of this plan, the project team reviewed the City's existing ordinances related to community development, street development, and electric bicycle use. Suggested updates to ordinance language have been provided to City staff for further review. Below is a summary of suggested ordinance and policy changes to explore.

STANDARD CITY SPECIFICATIONS

Standard City Specifications should reflect the guidance provided in this plan for shared use paths. **It is recommended to update the Standard City Specifications as needed to reflect preferred width and placement of shared use paths, as well as work towards a consistently implemented design for sidewalks, shared use paths, and on-street bike facilities.**

SUBDIVISION CODE

Today, the subdivision code (§ 545-29 Streets.) within Part III: Land Use Legislation of Watertown's Code requires developers to make provisions for pedestrian ways and bikeways for transport and recreation as required by the Planning Commission, based on recommendations from the Park, Recreation and Forestry Commission and the Public Works Commission, which base their recommendations on the formally adopted subplans and policies, including this Bicycle and Pedestrian Plan at adoption.





SIDEWALKS

As currently written within the City's subdivision code ([§ 545-37 Sidewalks and bikeways.](#)), sidewalks are required along both sides of all major streets and arterial streets and those local streets where access needs are further defined and described. To simplify the requirement and remove the potential for misinterpretation of where sidewalks are needed or not needed for access, **it is recommended that the language be amended to exclude the additional provisions and simply require sidewalks along both sides of all roadways unless otherwise recommended by the Planning Commission or Public Works Commission.**

Other suggested edits include addition of written policy language to require that sidewalks are to be installed for each lot prior to occupancy, with entire sidewalk network within a development to be installed within two years of final plat record (the two-year requirement exists already in the code language as part of the building permit process). This will ensure a complete sidewalk network at the onset of development.



BICYCLES

Today, City policies regarding bicycle use and rules of the road are found in [Part II: General Legislation Ch 240](#) of the Watertown City Code. To align with this plan, the State of Wisconsin and with accepted national standards, the following revisions are suggested.

RULES OF THE ROAD

In [§ 240-8 Rules of road](#), it is stated that the rider of a bicycle upon a roadway carrying two-way traffic shall ride as near as practicable to the right edge of the unobstructed traveled roadway. **It is recommended to add 'unless an alternate on-street bicycle facility, such as a bike lane or shared lane marking, is provided.' This aligns more closely with this plan's network recommendations where bikeways and bike lanes are to be implemented.**

Additionally, it is recommended to remove section D from the code language, which prohibits anyone riding a bicycle upon any public roadway abreast of another person operating a bicycle except when passing such bicycle. Wisconsin Statute 346.80 (3) (a) allows for bicycles (along with electric scooters, and personal assistive mobility devices) may ride 2 abreast if it does not impede the normal and reasonable movement of traffic. Removing the Watertown prohibition of riding abreast aligns more closely with the implementation of the Network Plan, as shared lane markings and bikeways are facilities that encourage bicyclists to ride within the roadway and there is not a safety reason to limit the number that can ride abreast.

ELECTRIC BICYCLES, SCOOTERS, AND ASSISTIVE MOBILITY DEVICES

Within [Chapter 240](#) of the City's ordinances, there are no definitions listed for Electric Bicycles, Scooters or Electric Assistive Mobility Devices. **It is recommended to amend the City code to include a set of definitions for Electric Bicycles, including the state-aligned definition of Class 1, 2, and 3 types, as well as Electric Personal Assistive Mobility Device, Electric Scooters, and Non-Classed Bicycles and Scooters (also known as E-Motos).**

It is also recommended to:

- » **Consider prohibiting the use of non-classed bicycles and scooters on City sidewalks and shared use paths or trails, as these vehicles are not regulated for speed. At a minimum, consider prohibiting their use on sidewalks.**
- » **Require owners of electric bicycles and scooters to register with the City under the same regulations set forth in the code relating to bicycles.**
- » **Prohibit the operation of any bicycle, electric bicycle or electric scooter on any sidewalk within the City, except bicycles operated by children 12 years or less, or bicycles operated by a responsible person over the age of 12 accompanying a child.**
- » **Continue to allow all personal assistive mobility devices on all City sidewalks but prohibit speeds over 15mph.**
- » **Prohibit operation of any vehicle--bicycle, electric scooter, personal assistive mobility device, non-classed bicycles or scooters (if allowed) above 20mph on any shared use path or trail within the City limits.**
- » **Consider requiring helmets for all bicycle, electric bicycle and electric scooter users.**

Note that the above are recommendations. A full process to amend City ordinances will be necessary to enact any of these policies.

ELECTRIC BICYCLES IN WISCONSIN

The Wisconsin State legislature defines an electric bicycle (also known as an e-bike) as follows:

WISCONSIN STATE STATUTE

2019 WISCONSIN ACT 34 SECTION 4. 340.01

"Electric bicycle" means a bicycle that is equipped with fully operative pedals for propulsion by human power and an electric motor of 750 watts or less and that meets the requirements of any of the following classifications:

- Class 1 electric bicycle is an electric bicycle equipped with a motor that provides assistance only when the rider is pedaling and that ceases to provide assistance when the bicycle reaches the speed of 20 miles per hour.
- Class 2 electric bicycle is an electric bicycle that may be powered solely by the motor and is not capable of providing assistance when the bicycle reaches the speed of 20 miles per hour.
- Class 3 electric bicycle is an electric bicycle equipped with a motor that provides assistance only when the rider is pedaling and that ceases to provide assistance when the bicycle reaches the speed of 28 miles per hour.

"Electric scooter" means a device weighing less than 100 pounds that has handlebars and an electric motor, is powered solely by the electric motor and human power, and has a maximum speed of not more than 20 miles per hour on a paved level surface when powered solely by the electric motor. "Electric scooter" does not include an electric personal assistive mobility device, motorcycle, motor bicycle, electric bicycle, or moped.

"Electric personal assistive mobility device" means a self-balancing, 2-nontandem-wheeled device that is designed to transport only one person and that has an electric propulsion system that limits the maximum speed of the device to 15 miles per hour or less.



SECTION 13. 346.806 SPECIAL RULES APPLICABLE TO ELECTRIC BICYCLES.

- An electric bicycle shall be considered a vehicle to the same extent as a bicycle.
- A person operating an electric bicycle is not subject to the provisions relating to financial responsibility, operator's licenses, registration, or certificates of title.
- No person under the age of 16 years may operate a class 3 electric bicycle.
- A person under the age of 16 years may ride as a passenger on a class 3 electric bicycle that is designed to accommodate passengers.



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OTHER WISCONSIN STATE STATUTES

- Within the e-bike market, in addition to the three classes of e-bike noted above, there are also unclassified/classless e-bikes available for sale. Other e-bikes that don't fall under any class in the system (for example, those without pedals, those with a throttle that can assist up to 28 mph, and those with motors over 750W). As they fall outside of the three classifications, these bikes are still in use but not considered e-bikes by the state of Wisconsin.
 - With all e-bikes, but especially with unclassified bikes, fires can start when e-bike and e-scooter batteries are stored, charged, or disposed of incorrectly.
- Wisconsin allows local municipalities to enact stricter rules concerning e-bike usage.
- Electric bicycles used without the motor engaged are allowed on all bicycle trails unless specifically prohibited.
 - NO e-bike capable of more than 30 MPH is permitted on any state trails that do not specifically allow motorcycles (these would technically not be considered e-bikes by state law).
- Signage is used in many places rather than, or in addition to, policy to identify specific locations where some types of e-bikes are not allowed (ex. sign stating that no e-bikes with a throttle are allowed on a trail)
- E-bike policy can be difficult to enforce due to the difficulty of identifying the type of e-bikes without close inspection.



STATE TRAILS AND THE WISCONSIN DNR

State Trails in Wisconsin are regulated by the Wisconsin DNR. The following are policies that affect these trails or areas within Wisconsin DNR jurisdiction:

- Class 1 and Class 3 electric bicycles (defined in s. 340.01 (15ph), Wis. Stats. [exit DNR]) are allowed on many bicycle trails, but must observe a 15-mile-per-hour speed limit. Electric bicycles are not allowed on any other bicycle trails other than a few designate trails. Electric bicycles used without the motor engaged are allowed on all bicycle trails.
- There are currently two hard surface trails in state parks, forests, and wildlife areas where e-bikes are not allowed: Kettle Moraine State Forest - Northern Unit and Point Beach State Forest.
- E-bikes are not allowed on the majority of soft-surface (unsurfaced or woodchip surfaced) trails.



POLICIES IN OTHER WISCONSIN COMMUNITIES

"Many communities do not regulate e-bikes beyond how they are regulated by state law. The following Wisconsin communities have additional ordinances of policies enacted to address safety concerns:

- Milwaukee: e-Bikes are allowed on most trails and bike lanes, but Class 3 e-Bikes may face restrictions in certain parks or on nature trails.
- Madison: e-Bikes, including Class 3, are allowed on city streets and bike lanes, but may not be permitted on all recreational trails due to the potential for higher speeds.
- Manawa: City code states that while riders do not need a driver's license to operate e-bikes, riders 16 and under are only allowed to operate e-bikes at speeds no higher than 20 miles an hour.
- Wausau: All three classes of e-bikes are allowed on roads, bike lanes, and most shared-use paths. They are not allowed on sidewalks unless there are signs that specifically state otherwise. Only class 1 e-bikes are allowed on park trails.
- Johnson's Creek: Does not address e-bikes specifically but does require registration for all bikes and electric motor scooters
- Oconomowoc: does not make any mention of e-bikes in their current City Code or website; they have a business in town that rents e-bikes



(POLICIES IN OTHER WISCONSIN COMMUNITIES CONTINUED)

Section 4, Item C.

- Fort Atkinson: All bikes, e-bikes, and e-scooters owned by residents of the City of Fort Atkinson are required to be registered with the City. Generally, the City applies the same regulations to bicycles as e-bikes and scooters; however, these regulations are more specific and exhaustive than they are in many communities regarding riding etiquette. City definitions:
 - E-Scooter means a device weighing less than 100 pounds that has handlebars and an electric motor, is powered solely by the electric motor and human power, and has a maximum speed of not more than 20 miles per hour on a paved level surface when powered solely by the electric motor. "Electric scooter" does not include an electric personal assistive mobility device, motorcycle, motor bicycle, electric bicycle, or moped.
 - e-Bike means a bicycle with a motor attached. It must have pedals that are operational, an electric motor that is less than 750w and have a maximum speed of 20 mph on level ground.
- Jefferson County: The County defines e-bikes as a bicycle that is equipped with fully operative pedals for propulsion by human power and an electric motor of 750 watts in addition to the definition for e-bikes from Wisconsin Statute. All Jefferson County bike paths are considered multi-use paths and open to pedestrians, bicycles, e-bicycles, inline skates, play vehicles, and electric scooters
- Dodge County: The County does not reference e-bikes or other e-devices in its code or ordinances
- Waukesha County: Allows the Operation Of Class 1 Electric Bicycles On All Waukesha County Recreational Trails And Designated Waukesha County Park Mountain Bike Trails, all other motor vehicles are prohibited.

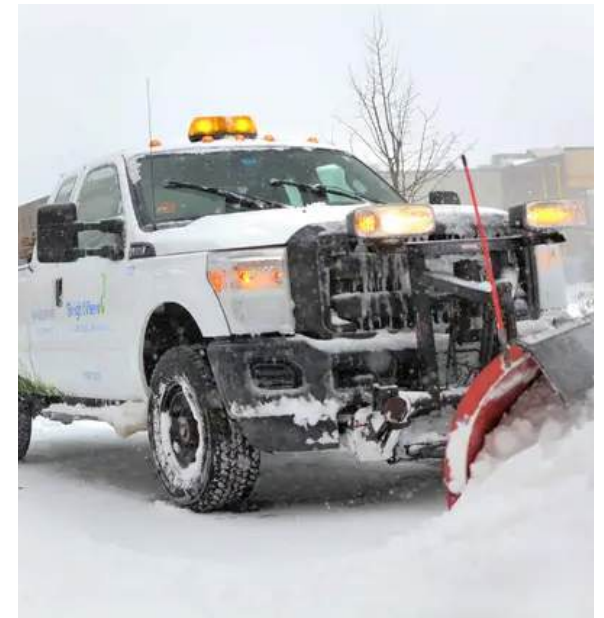


Implementation

INTRODUCTION

The Watertown Bicycle and Pedestrian Plan outlines goals, projects, and recommendations for improving the biking and walking network in the City for the next few generations. This chapter focuses on how to transform the plan components into reality. First, a series of general Implementation Recommendations are listed; these are intended to provide guidance for policy and coordination of implementation efforts. Next, a phasing strategy is outlined in a table, which breaks projects down into Backbone Projects, Ongoing Projects, and Long-Term projects. The table provides guidance on anticipated costs, extents, and related projects that could be implemented together.

As community goals and other projects are implemented as cross-departmental initiatives, it is imperative to keep this plan in discussions, to identify projects that could support realizing components of this plan. This chapter also provides recommendations for future evaluation and monitoring to keep the plan relevant as conditions change. Additional guidance on maintenance, pavement preservation, and potential grant funding sources are provided at the conclusion of the chapter.



IMPLEMENTATION RECOMMENDATIONS

The following recommendations have been developed to assist with guiding policy and implementation of the plan:

1

BICYCLE & PEDESTRIAN PATH TASK FORCE

Since its inception in 2016, the City-wide Bicycle and Pedestrian Path Task Force has been tasked with identifying and advancing the goals of improving conditions for biking and walking in Watertown. This plan recommends that the City continues to convene as a committee and should continue to provide recommendations to the Public Works Commission (PWC), along with continuing to forward meeting minutes to the PWC.

The role of this committee will change slightly moving forward into the future. With the approval of this Plan, there is now a clear road map for upcoming projects. The primary role of the Task Force will be to support implementation of the plan, evaluation of projects and priorities on an annual basis, education and community engagement, coordination with regional and agency collaborators, and assisting the City with grant and fund-seeking efforts.

The make-up of the Bicycle and Pedestrian Task Force may need to expand to include residents and advocates that are more representative of network users (youth, seniors, families), as well as include agency representatives and people who are enthusiastic and excited to connect with community members and serve as champions for the Bicycle and Pedestrian Plan.

2

AGENCY & COUNTY COORDINATION

Many of the Network Plan and facility recommendations are located along roadways that are County or State-owned or located on property owned by the Watertown School District. Implementation of these projects will require ongoing coordination and collaboration with WisDOT, Dodge County, Jefferson County, Watertown School District, and other local agencies and non-profit organizations (Main Street or Chamber of Commerce are examples). With City staff as a lead for this effort, convening annual or quarterly meetings with agency and County staff to review potential project collaborations is recommended. Additionally, inclusion of representatives from these entities as part of the Bicycle and Pedestrian Task Force is also recommended.

The Network Plan shown in Chapter 4 shows a few locations where regional trail alignments, such as the Rock River Trail and the Glacial River Trail, may need to be modified within Watertown as shared use paths and other segments of the future network are constructed. Specifically, the City should work together with Jefferson and Dodge County to update regional trail maps.

Expansion of the regional network, such as constructing a trail west towards Waterloo, or continuing to expand regional trails north, should also include the participation of Watertown City staff and/or members of the Bicycle and Pedestrian Path Task Force.

3 EVALUATION, MONITORING & TRACKING

Consistent and documented data collection plays an increasingly vital role in understanding baseline inventory of City trail and multimodal facilities, as well as serve as information that is often required or useful in grant writing or fund-seeking efforts. The following actions are recommended to keep the City's information up-to-date and accessible as the network develops:

- » Continue to maintain a GIS geodatabase with shapefiles of roadway information, along with existing bicycle and pedestrian facilities. As facilities are built, update this information, along with reviewing inventory on an annual basis.
- » Continue to conduct an annual or multi-year condition assessments of trails, sidewalks, and on-street facilities, in order to budget for annual maintenance.
- » Develop a public-facing ArcGIS Online map that shows existing trails, sidewalks, and bikeways, or integrate this data into the existing City of Watertown Public GIS Viewer. Update this data with future/planned/funded projects as they come on board and include trail access points and other network features.
- » Collect annual user counts on trails and on future pilot projects. Examples include conducting manual or automated counting on the Interurban Trail or along other regional routes to understand how trails are used seasonally. Visitor intercept surveys could also be conducted to understand where people are coming from, as well as potential amenities or concerns to address.
- » Conduct a community-wide survey on an annual or biennial basis, similar (or identical) to the community survey used for the development of the Bicycle and Pedestrian Plan. This way, information can be compared year-over-year to understand if concerns are being addressed and to help prioritize projects for inclusion in the City's CIP.

4 ENCOURAGEMENT & EDUCATION

The following programs and initiatives are recommended to promote a culture of biking and walking in Watertown, as well as to educate and inform residents about best practices:

- » Continue to support the Watertown Police Department's annual Bicycle Rodeo and educational events to teach young people (and adults!) how to safely maneuver a bicycle and learn the rules of the road.
- » Develop a guidebook of the City's general bicycle, pedestrian and driver etiquette and rules of the road ordinances or state statutes. This guidebook could be in the form of a StoryMap or other interactive website to serve as a resource for residents and educators in the community, as well as to communicate potential upcoming changes in the City's policies regarding E-bikes and other multimodal topics.
- » Develop a program to share in the cost for installing city-branded bike racks at businesses throughout Watertown.
- » Develop a city-wide wayfinding plan with specific brand design for directional signage for the On- and Off-Street Network and Regional Overlays.
- » Re-apply to become certified as a Bicycle Friendly Community through the League of American Cyclists.
 - In 2017, the City of Watertown received an Honorable Mention for their application to be recognized as one of five levels of award (diamond, platinum, gold, silver and bronze). The award designation recognizes communities that are making demonstrated progress towards the goals of Bicycle Friendly Communities through planning, policies, systems, and partnerships.



PHASING PLAN FOR IMPLEMENTATION

Implementation of the Network Plan and Intersection Treatments detailed in Chapter 4, and with further guidance provided in Chapter 5, will need to be accomplished in phases based on community-identified priorities, balanced with available funding, feasibility of construction and alignment with related projects (such as roadway improvements).

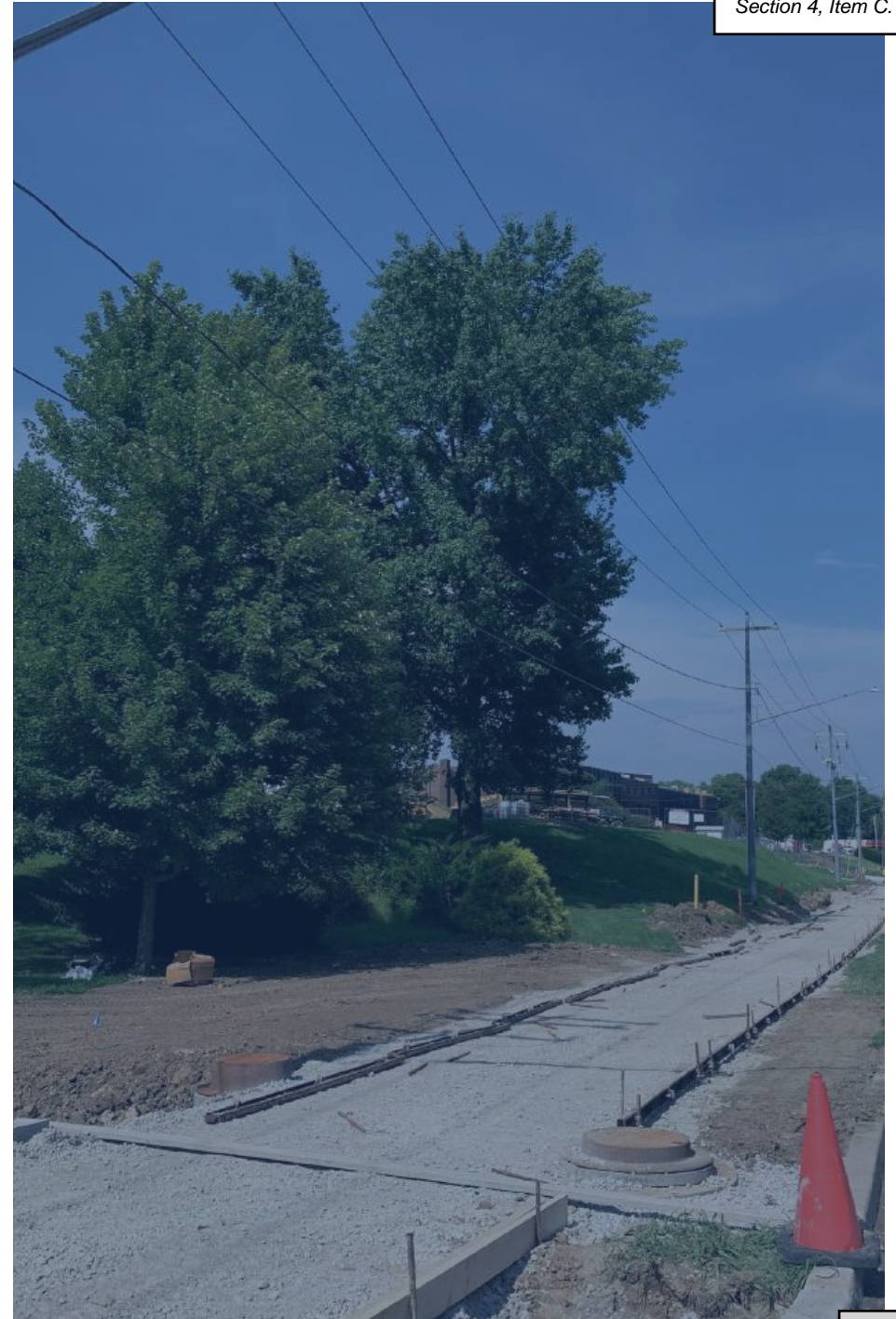
The projects have been split into three categories:

- » Backbone Projects
- » Ongoing Projects
- » Long-Term Projects.

Each project listed in Tables 7.3, 7.4, and 7.5 lists the project name, location, project type (New Facility, Facility Conversion, Facility Upgrade), estimated cost, collaborating agency, associated intersection treatments, associated projects, and wayfinding strategies.

The following are steps that will need to be completed prior to construction or implementation of any improvement can occur (not all steps may be necessary for every project):

- » Feasibility and engineering analysis, including potential analysis for economic impact, archaeological/historical/natural/cultural resource review with other agencies with jurisdiction.
- » Funding sources will need to be identified and secured.
- » Potential land surveying, negotiation with any property owners (as necessary) for easements if additional right-of-way is needed. Note that state statute prohibits the use of eminent domain to establish or extend recreational trails, bicycle ways, bicycle lanes or pedestrian ways (Act 59, 2017).
- » Detailed design and engineering of new facilities.
- » Coordination with other roadway, trail, or capital improvement project.



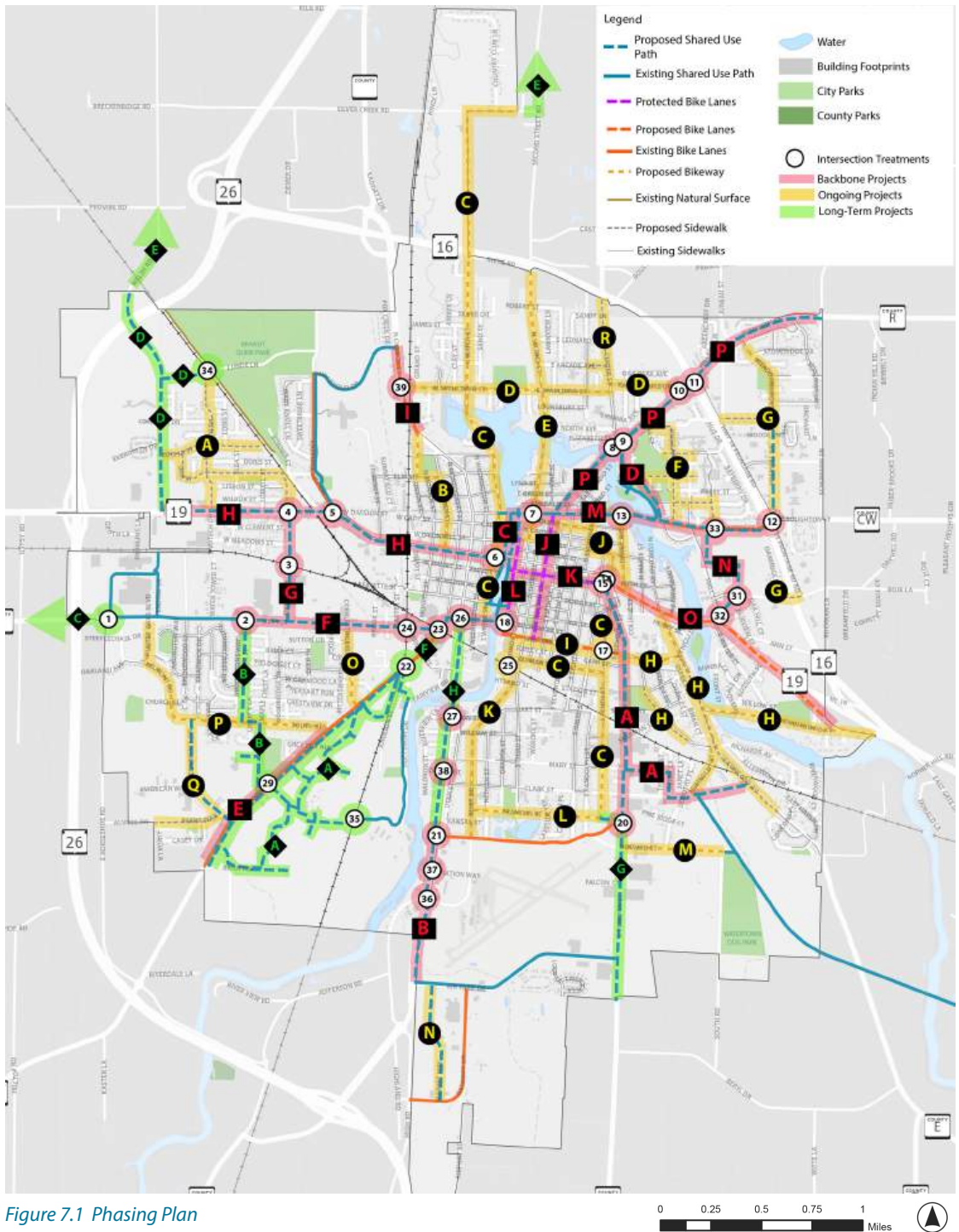


Figure 7.1 Phasing Plan



Backbone Projects are generally larger projects that have been identified as vital to create a well-connected network. You can think of these projects as the ‘spine’ of the City’s network. Many of these projects involve the construction of shared use paths as new facilities and will require time and significant effort to secure funding through grants and collaborative efforts. These projects should be the primary focus for grant seeking and implementation for the coming years.

Ongoing Projects are generally smaller, lower-cost projects or projects that can be more easily folded into the City’s Capital Improvement Program, although this does not prevent the City from collaborating or seeking grant-funding for implementation. These projects are primarily sidewalk and on-street bikeway projects.

Long-Term Projects will generally require more planning and collaboration with regional agencies to implement or are intended to be initiated and completed as part of future.

BACKBONE PROJECTS

Tag	Location	Extents	Planned Facility	Project Type	Length	Unit Cost*	Estimated Cost	Collaborators
A	12th St / Interurban Trail	Interurban Trail to Market St	Shared Use Path	New Facility	9961	\$80	\$796,880	
B	S Church St	Air Park Dr to Boomer St	Shared Use Path	New Facility	1840	\$80	\$147,200	WisDOT
C	Riverwalk Extension	Rock River (east and west sides) Main St to Division St	Shared Use Path	New Facility	4125	\$80	\$330,000	
D	Labaree St	Fourth St to Division St	Shared Use Path	New Facility	2418	\$80	\$193,440	
E	Milford St	Western City Limits to South St	Sidewalk	New Facility	4555	\$35	\$159,425	
F	West St / Milwaukee St	Benton Ln to Milford, Milwaukee St to 1st St	Shared Use Path	New Facility	6354	\$80	\$508,320	
G	Dayton St	West St to Main St	Shared Use Path	New Facility	2815	\$80	\$225,200	
H	W Main St	Welsh St to S 1st St	Shared Use Path	Sidewalk to SUP Conversion	9266	\$80	\$741,280	WisDOT
I	N Church St	Endeavour Dr to Union St	Bike Lanes	New Facility	2325	\$20	\$46,500	
J	Fourth St	Western Ave to Division St	Two-Way Protected Bike Lanes	New Facility	3244	\$60	\$194,640	
K	Market St	Riverwalk Trailhead to Sharp Corner Park	Two-Way Protected Bike Lanes	New Facility	2617	\$60	\$157,020	
L	First St	Western Ave to Jones St	Two-Way Protected Bike Lanes	New Facility	2172	\$60	\$130,320	
M	Division St / Boughton St	Fourth St to SH 16	Shared Use Path	New Facility	5834	\$80	\$466,720	WisDOT, Dodge Co
N	Riverside Middle School	Main St to Boughton St	Shared Use Path	New Facility	3563	\$80	\$285,040	
O	E Main St / Summit Ave	Sharp Corner Park to Oconomowoc Ave	Bike Lanes	New Facility	7020	\$20	\$140,400	WisDOT, Jefferson Co
P	Fourth St	Division St to Schuman Dr	Shared Use Path	Conversion	9565	\$80	\$765,200	WisDOT, Dodge Co
							\$5,287,585	

Table 7.3 Backbone Projects (generally listed in order of priority)

*Unit costs do not include costs related to engineering, mobilization, contingency, or other extenuating site conditions that may require retaining walls or earthwork to significantly change grades. Unit costs are based on 2026 pricing of typical projects in similarly-sized communities in the Upper Midwest.

Tag	Location	Implementation Notes	Associated Intersection Treatments	Other Associated Projects	Wayfinding
A	12th St / Interurban Trail		20, 15		Off-Street
B	S Church St	Air Park Dr to Boomer St is already under design and funded	36, 37, 21,		Off-Street
C	Riverwalk Extension	Rock River District Plan	7	Trailheads at Riverwalk and Market St, Bentzin Family Town Square, Fanny Lewis Park	Off-Street / Regional Trail Branding
D	Labaree St	In design (2026)	9	Riverside Park Trailhead	Off-Street
E	Milford St				
F	West St / Milwaukee St		2, 24, 23, 26, 18	Bridge Crossing	Off-Street - County Trail
G	Dayton St		3, 4		Off-Street
H	W Main St		4, 5, 6	Bentzin Family Town Square Trailhead	Off-Street
I	N Church St		39		On-Street
J	Fourth St				On-Street
K	Market St			Riverwalk Trailhead	On-Street
L	First St		18	Riverwalk Trailhead	On-Street
M	Division St / Boughton St		7, 13, 33, 12		Off-Street
N	Riverside Middle School	Collaboration with Watertown School District	32, 31		Off-Street
O	E Main St / Summit Ave	Collaboration with WisDOT as part of upcoming roadway improvements	14, 15		Off-Street
P	Fourth St	Will require collaboration with WisDOT	8,9, 10, 11	Grade Separated Crossing at SH 16	Off-Street

ONGOING PROJECTS

Section 4, Item C.

Tag	Location	Extents	Planned Facility	Project Type	Length	Unit Cost*	Estimated Cost	Collaborators
A	Northwest Neighborhood	Louisa St, Votech Dr, Oak St	Sidewalks	New Facility	9925	\$35	\$347,375	
B	Lincoln Park Area	Union St / Elm St / Warren St	Bikeway	New Facility	3610	\$6	\$21,660	
C	Glacial River Trail	S Tenth St, Clyman St, 2nd St, S Water St, N Water St, Silver Creek Rd	Bikeway - Regional Trail Network	Facility Upgrade	22200	\$10	\$222,000	
D	Spaulding St / Carl Schurz Dr	N Church St to Center St, Carl Schurz Dr	Bikeway	New Facility	7487	\$6	\$44,922	
E	N Second St	Division St to Robert St	Bikeway	New Facility	7282	\$6	\$43,692	
F	Riverside Neighborhood	Hus Dr, Highland Ave, Hill St, Anne St, Kiewert St, Hidde Dr	Sidewalks	New Facility	7032	\$35	\$246,120	
G	Meadowbrook / Mary Rose Park Neighborhood	Meadowbrook Dr, Hospital Dr, Allwardt St, E Main St	Sidewalk, Bikeway, Shared Use Path	New Facility	n/a	n/a	n/a	
H	Concord Ave Neighborhood	S Concord Ave, Oconomowoc Ave, Western Ave east of Twelfth St, Richards Ave	Sidewalk, Bikeway	New Facility	n/a	n/a	n/a	Jefferson Co (Western Ave)
I	Western Ave	First St to Twelfth St	Bike Lanes	New Facility	3184	\$20	\$63,680	Jefferson Co
J	Downtown Bikeways	Cole St, Eighth St, Jones St, Dewey Ave, 10th St	Bikeway	New Facility	5996	\$6	\$35,976	
K	River Dr / Second St	Boomer St to Second St to Western Ave	Bikeway	New Facility	5255	\$6	\$31,530	
L	Franklin St	River Dr to Tenth St	Bikeway	New Facility	3369	\$6	\$20,214	
M	Boomer St	Twelfth St to Watertown Dog Park	Bikeway	New Facility	2648	\$6	\$15,888	
N	Market Way	Gateway Dr to Air Park Dr	Shared Use Path	New Facility	3016	\$80	\$241,280	
O	Cobblestone Way	Milford St to West St	Bikeway	New Facility	2124	\$6	\$12,744	
P	South St/Belmont Dr / Hunter Oaks Rd	South St/Belmont Dr / Hunter Oaks Rd	Bikeway	New Facility	6888	\$6	\$41,328	
Q	Commerce Dr / Perry Way	Commerce Dr and Perry Way	Shared Use Path and Sidewalk	New Facility	n/a	n/a	n/a	
R	Center St and East Arcade Ave	Arcade Ave to City Limits	Sidewalk	New Facility	1891	\$35	\$66,185	
							\$1,454,594	

Table 7.4 Ongoing Projects

Tag	Location	Implementation Notes	Associated Intersection Treatments	Other Associated Projects	Wayfinding
A	Northwest Neighborhood	Implement with roadway improvements	34 - future railroad crossing at Lunde Ln / Brandt QuirkPark		
B	Lincoln Park Area				On-Street
C	Glacial River Trail				On-Street Glacial River Trail
D	Spaulding St / Carl Schurz Dr		39, 10		On-Street
E	N Second St		7		On-Street
F	Riverside Neighborhood		33		
G	Meadowbrook / Mary Rose Park Neighborhood		12		On- Street and Off-Street
H	Concord Ave Neighborhood			Priority Project A (Interurban Trail Connections)	On-Street
I	Western Ave		17		On-Street
J	Downtown Bikeways		13, 7, 14		On-Street
K	River Dr / Second St		25 - Railroad Crossing		On-Street
L	Franklin St			Priority Project A (Interurban Trail Connections)	On-Street
M	Boomer St			Shared Use Path connection from Watertown Dog Park Trailhead to Interurban Trail	On-Street
N	Market Way				Off-Street
O	Cobblestone Way				On-Street
P	South St/Belmont Dr / Hunter Oaks Rd				On-Street
Q	Commerce Dr / Perry Way				Off-Street
R	Center St and East Arcade Ave				

*Unit costs do not include costs related to engineering, mobilization, contingency, or other extenuating site conditions that may require retaining walls or earthwork to significantly change grades. Unit costs are based on 2026 pricing of typical projects in similarly-sized communities in the Upper Midwest

Tag	Location	Extents	Planned Facility	Project Type	Length	Unit Cost*	Estimated Cost	Collaborators
A	Southwest Side Neighborhood	Southwest Side Neighborhood Phase II	Shared Use Path	New Facility	22675	\$80	\$1,814,000	Developer
B	Grinwald Park and Hunter Oaks Neighborhood	Milford St to South St, Benton St to West St	Shared Use Path	New Facility	5080	\$80	\$406,400	
C	County T	Regional Trail Connection to Waterloo	Shared Use Path	New Facility	n/a	\$80		Jefferson Co
D	Main St / Welsh Rd	Main St west of Dayton St to Welsh Rd to northern City limits	Shared Use Path	New Facility	7248	\$80	\$579,840	WisDOT, Dodge Co
E	Welsh Rd / Second St	Regional Trail Connection	Shared Use Path	New Facility	n/a	\$80		Dodge Co
F	Milford St	Johnson St to West St	Bike Lanes	New Facility	1357	\$20	\$27,140	
G	12th St (south of Boomer St)	South of Boomer St to City Limits	Shared Use Path	New Facility	4652	\$80	\$372,160	Jefferson Co
H	S Church St	Boomer St to Milwaukee St	Shared Use Path	New Facility	5752	\$80	\$460,160	WisDOT
							\$3,199,540	

Table 7.5 Long-Term Projects

*Unit costs do not include costs related to engineering, mobilization, contingency, or other extenuating site conditions that may require retaining walls or earthwork to significantly change grades. Unit costs are based on 2026 pricing of typical projects in similarly-sized communities in the Upper Midwest.

Tag	Location	Implementation Notes	Associated Intersection Treatments	Other Associated Projects	Wayfinding
A	Southwest Side Neighborhood	Trails to be implemented as neighborhood is developed, including Shared Use Path along Milford St	22, 29, 35	H - Bike Lane extension on Milford St to West St	Off-Street
B	Grinwald Park and Hunter Oaks Neighborhood		2, 29		Off-Street
C	County T	Long-term collaboration to plan and build a regional trail connection per Jefferson County long-range plans.	1		Off-Street / Regional Trail Branding
D	Main St / Welsh Rd		34	Railroad crossing at Lunde Ln	Off-Street
E	Welsh Rd / Second St	Long-term collaboration to build a regional trail connection to a destination north of Watertown.			Off Street / Regional Trail Branding
F	Milford St	Implement with Southwest Side Neighborhood Phase II trails	22, 23	A - Southwest Side Neighborhood Phase II	On-Street
G	12th St (south of Boomer St)		20		Off-Street / Regional Trail Branding
H	S Church St	Boomer St to Milwaukee St	21, 38, 27, 26		Off-Street



OPERATIONS AND MAINTENANCE

SNOW/ICE REMOVAL

Snow and ice removal are key to providing safe access to trails, sidewalks, and on-street facilities year-round. In Watertown, residents are responsible for maintaining clear paths for sidewalks adjacent to or within their property that are used by the public. The City of Watertown Parks, Recreation, and Forestry Department clear some shared use paths, some trail segments within parks, and a handful of sidewalks as part of development agreements.

This plan recommends the following for clearing responsibilities:

- » The City should continue to clear existing and future shared use paths and sidewalks that are not adjacent to residential units, with an emphasis on paths that are near schools.
- » Residents should be responsible for clearing sidewalks adjacent to their property.
- » The City should clear bike lanes and protected bike lanes as part of existing roadway clearing protocol.

With the addition of more shared use paths, sidewalks, and on-street facilities, the City's Park, Recreation and Forestry Department may need to increase staffing levels to accommodate the additional mileage for clearing. It should be noted that some grants may require that the City continues to clear new facilities that are funded through their sources.



PAVEMENT MANAGEMENT

Regular maintenance of trails and sidewalks will extend the life of the surface and reduce the overall long-term costs of total reconstruction of facilities. Regular maintenance also keeps pedestrians and cyclists safe from hazards, allows the facilities to be used by people with a wide variety of abilities, and provides a welcoming and high-quality experience. People are more likely to use facilities that are in good physical condition, and having a maintenance plan in place is sometimes a requirement to receive state or federally funded grants.

A typical asphalt surfaced trail has a life expectancy of 20 – 25 years if constructed correctly with adequate depth of aggregate base and regular maintenance. Concrete sidewalks tend to have more variable durability, with a life expectancy of upwards of 30 years. Generally, less annual/ongoing maintenance is required for concrete sidewalks, as the cost and effort to replace segments as needed is lower. Factors that affect the longevity of facilities include seasonal or annual maintenance, weather and climate, exposure to ultraviolet light, use by heavy vehicles, poor soils, and drainage. Annual condition inventory and budgeting for maintenance will extend the life of any trail or sidewalk surface.

According to the Local Roadway Research Board, deferred maintenance can lead to a shorter service life and result in earlier needs for major rehabilitation or replacement needs. Preventative maintenance actions are typically applied to trails while they are in “good” or “fair” condition to keep them in a usable state.



- » **Crack Filling:** Crack filling is a common practice applied to asphalt trails when cracks are wider than 3/8". Crack sealing is a flexible latex product that is pumped into cracks to prevent water intrusion and damage to the sub-grade. Crack filling provides safety benefits for all users but does not fully address pavement quality or distress issues. Crack filling should be viewed as a short-term preventative maintenance technique.
- » **Thin Overlay:** An overlay consists of a thin layer of asphalt that is applied to the top of the surface of the trail. Application of a thin overlay is a costly technique but can extend the service life of the pavement by 5 to 10 years and improve riding quality.
- » **Mill and Overlay:** A mill and overlay is also a common technique for major rehabilitation and reconstruction projects. This technique builds structural capacity by milling the existing pavement and adding a new layer of asphalt. A mill and overlay can extend the life of a trail by 10 to 25 years, depending on the trail's structural base and depth of the mill and overlay.
- » **Mastic Products:** Mastic products are seal coating materials that are applied to larger cracks that cannot be addressed with traditional crack filling treatments. Mastic products serve as a joint sealer without jeopardizing the ride quality of the trail. These products are designed to dry quickly (within one hour) and can extend the life of a trail by 3 – 5 years. This technique is both a preventative maintenance measure and a minor rehabilitation practice.
- » **Slurry Seal:** A slurry seal is a preventative maintenance procedure that provides a protective surface to the existing asphalt trail. More advanced applications are known as micro-surfacing. This treatment includes a blend of oil and small aggregate that is applied to the trail in 1/8" depths. By sealing the trail, the base is protected from water damage, and the surface is protected from weather and wear. This treatment can extend a trail's life by 3 – 5 years, while a micro-surfacing treatment can last more than 7 years. Slurry sealing and micro-surfacing require a longer period to dry (6 – 8 hours).

Table 7.6 outlines recommended maintenance activities and suggested frequencies for application.

Maintenance Activity	OPTIMAL FREQUENCY							Notes
	Weekly	Monthly	Quarterly	Annually	Spring/Fall	After Storm	Other	
General Maintenance								
1	Safety inspection	X					X	
2	General debris and trash pickup	X					X	
3	Vandalism inspection	X						
4	Encroachments							Ongoing
Pavement								
1	Pavement survey					X		Conduct Spring and Fall surveys
2	Crack sealing							Reactionary
3	Fog seal							As Needed
4	Sealcoat							As Needed Lifespan approximately 4-6 years
5	Slurry seal							As Needed Lifespan approximately 6-10 years
6	Overlay							As Needed Lifespan approximately 8-10 years
7	Reconstruct							As Needed Lifespan approximately 20 years
8	Inspect pavement markings				X			As Needed
9	Repaint pavement markings							As Needed
Vegetation								
1	Mowing- clear zones, trailhead areas	X	X					
2	Brush trimming/ overhead trimming				X			Spring activity
3	Clear zone weed control							As Needed Noxious weed removal
4	Sight line trimming at intersections		X					Roads, other trails, driveways, etc.
5	Tree removal						X	As Needed Storm cleanup
6	Rain garden maintenance		X				X	
7	Trail sweeping/ blowing					X	X	As Needed Up to weekly frequency in Fall
8	Seeding				X	X		Spring activity
9	Root cutting							Monitor root activity along trail

Table 7.6 Recommended Maintenance

Maintenance Activity	OPTIMAL FREQUENCY							Notes
	Weekly	Monthly	Quarterly	Annually	Spring/Fall	After Storm	Other	
Drainage								
1	Erosion repair			X		X	X	After spring snow melt, storm cleanup
2	Culvert/ catch basin clearing			X			X	Storm cleanup
3	Ditch maintenance (clear of debris, trash, branches)				X		X	Spring activity
4	Standing water repair				X		X	
Structures								
1	Bridge inspection (non-structural inspection)				X			
2	Tunnel inspection (non-structural inspection)				X			
3	Boardwalk inspection				X			
4	Railroad crossing inspection				X			Notify owner (railroad) of problems
5	Retaining walls				X			

MAINTENANCE COSTS

The table below outlines the existing and proposed shared use path and sidewalk network in Watertown, along with anticipated annual operations and maintenance costs, as well as the capital costs associated with replacement of facilities. Anticipated costs are based on consultant experience developing facilities in similar communities across the Upper Midwest. Local costs may vary and are subject to inflation or other changes over time.

	Qty (Miles)	Qty (LF)	Annual O+ M Cost	Total O+M Annual Cost	Capital Cost	Total Projected Capital / Replacement Costs	Estimated Life
Existing Shared Use Paths (Asphalt)	6.4	33,792	\$1,500 / mi	\$9,600	\$80/LF	\$2,703,360	20 - 25 years
Proposed Shared Use Paths (Asphalt)	22.1	116,795	\$1,500 / mi	\$33,180	\$80/LF	\$9,343,600	20 - 25 years
Existing Sidewalks (Concrete)	97.9	516,912	\$400/ mi	\$39,160	\$35/LF	\$18,091,920	30+ years
Proposed Sidewalks (Concrete)	6.0	31,778	\$400/ mi	\$2,407	\$35/LF	\$1,112,230	30+ years

Table 7.7 Estimated Maintenance Operation Costs

FUNDING SOURCES

Achieving the vision for a well-connected bicycle and pedestrian network in Watertown will take significant funding, not only for construction of new facilities, but also for ongoing maintenance and operations of existing and future facilities.

There are several potential funding sources available to help finance implementation, including state and federal grant programs. Funds from any of these grant programs are subject to change due to fluctuations in federal, state, and local budgets and legislation. The network improvements recommended in this plan should be incorporated into the City-wide Capital Improvement Plans and Programs, with Ongoing Projects and matching funds for projects with grant funding as a priority. Additionally, the city of Watertown should continue to utilize the existing framework through the City's subdivision ordinance to ensure that new developments include bike and pedestrian facilities as planned.

The City of Watertown should also consider coordinating efforts with other units of government, such as Dodge and Jefferson Counties, as well as other public agencies and non-profit organizations to help fund projects and pursue grant funding. The following table outlines potential state and federal funding sources that could be used to implement projects in Watertown.



Grant Name	Source	Grant Goal/Purpose	Eligible Applicants	Eligible Projects/ Example Projects	Grant Amount/ Range/Limit	Deadlines/ Grant Cycle	Watertown Projects	
Wisconsin DNR Grants								
Knowles-Nelson Stewardship Local Assistance Grant Program	Wisconsin DNR	The Knowles-Nelson Stewardship Fund was created in 1989 to preserve important natural communities, protect water quality and fisheries, and expand opportunities for outdoor recreation.		[NEED TO VERIFY - RECENT 2026 LEGISLATION OMITTED LAND ACQUISITION]				https://dnr.wisconsin.gov/topic/Stewardship
Aids for the Acquisition and Development of Local Parks (ADLP)	Wisconsin DNR	Helps communities and organizations acquire and develop land for public nature-based outdoor recreation and improve community recreation areas.	Towns, villages, cities, counties, tribal governments, and NCOs.	Land acquisition and development projects that provide opportunities for nature-based outdoor recreation are eligible for ADLP grants.	50% match; Funds are allocated on regional basis proportional to population, projects compete against other projects in their region	Annual May 1 deadline	Riverwalk Extension, Riverside Park, Trailhead development	https://dnr.wisconsin.gov/topic/Stewardship/ApplyLUG
Urban Green Space Program (UGS)	Wisconsin DNR	Supports acquisition of land for open natural areas and community gardens within or near urban areas.	"Towns, villages, cities, counties, tribal governments, NCOs, sanitary districts, and public inland lake protection and rehabilitation districts"	Projects that acquire land within or in proximity to urban areas that provides open space, protects unique natural features, and/or provides opportunities for community gardening are eligible to apply for UGS grants. Development projects are not eligible for UGS grants.	50% match; projects compete state wide	Annual May 1 deadline	Shared Use Paths that have associated pollinator pathways, landscaping or best management stormwater infrastructure included in the scope, Trailhead development.	https://dnr.wisconsin.gov/topic/Stewardship/ApplyLUG
Urban Rivers Grant Program (URGP)	Wisconsin DNR	Helps restore and preserve the character of urban river corridors through the acquisition and development of land adjacent to rivers.	Towns, villages, cities, counties, tribal governments, and NCOs	Projects that acquire land within or in proximity to urban areas that provides open space, protects unique natural features, and/or provides opportunities for community gardening are eligible to apply for UGS grants. Development projects are not eligible for UGS grants.	50% match; no applicant can receive more than 20% of allocated funds in any fiscal year; projects compete statewide	Annual May 1 deadline	Riverwalk Extension, Riverside Park, Trailhead development	https://dnr.wisconsin.gov/topic/Stewardship/ApplyLUG
Acquisition of Development Rights	Wisconsin DNR	Protect natural, agricultural, and forest lands that provide nature-based outdoor recreation by purchasing development rights and compensating landowners for limited future development on their land.	Local/tribal government, lake sanitary districts/public lake inland protection and rehab districts; conservation non-profits	Projects that acquire development rights to support nature-based outdoor recreation.	50% match; projects compete state wide	Annual May 1 deadline		https://dnr.wisconsin.gov/topic/Stewardship/ApplyLUG

Table 7.8 Grants - Wisconsin DNR

Grant Name	Source	Grant Goal/Purpose	Eligible Applicants	Eligible Projects/ Example Projects	Grant Amount/ Range/Limit	Deadlines/ Grant Cycle	Watertown Projects	Section 4, Item C.
Wisconsin DOT								
Local Roads Improvement Project (LRIP)	WisDOT	The Local Roads Improvement Program (LRIP) assists local governments in improving seriously deteriorating county highways, town roads, and city and village streets.	Local units of government		Reimbursement program, 50% match for entitlement and discretionary projects; 10% match for supplemental projects	LRIP projects are awarded every two years on a biennial budget cycle. The current biennial program cycle is from July 1, 2025, through June 30, 2027.		https://wisconsindot.gov/Pages/doing-bus/local-gov/astnce-pgms/highway/lrip.aspx
Surface Transportation Program - Urban	WisDOT	The objective of the STP-Urban Program is to improve transportation on Wisconsin's federal-aid eligible roads and streets in urban areas.	"Counties, towns, cities, villages and certain public authorities located within urban and urbanized areas..."	Federal funding is provided for a wide range of transportation-related activities, including projects on higher function local roads not on the State Trunk Highway system, and local safety improvements.		TBD		https://wisconsindot.gov/Pages/doing-bus/local-gov/astnce-pgms/highway/stp-urban.aspx
2026-2030 Transportation Alternative Program (TAP)	WisDOT	The Transportation Alternatives Program (TAP) is the State of Wisconsin's program for what is now the federal Transportation Alternatives (TA) set-aside Program. Wisconsin used to have separate Bicycle and Pedestrian Facilities Program and Safe Routes to School Program, now any projects eligible under these programs are funded through the TAP program.	Local governments, Regional transportation authorities, Transit agencies, Natural resource or public land agencies (see description below), School districts, local education agencies, or schools, Tribal governments, etc.	Projects that upgrade the condition of streets, highways, and bridges making them safe for all users, while simultaneously modernizing them so that the transportation network is accessible for all users.	Projects are reimbursed up to 80%	2026-2030 program applications were due in 2025; keep an eye on website for future funding cycles	Intersection treatments that address safety concerns, shared use paths, on-street facilities, sidewalk improvements.	https://wisconsindot.gov/Pages/doing-bus/local-gov/astnce-pgms/aid/tap.aspx
Highway Safety Improvement Project (HSIP)		The Highway Safety Improvement Program (HSIP) is a core Federal-aid program with an objective to achieve a significant reduction in traffic fatalities and serious injuries on all public roads, including non-State-owned roads and roads on Tribal lands	local government units	The HSIP requires a data-driven and strategic approach to fund highway safety projects at sites that have experienced a high crash history. The program has an emphasis on low-cost projects that can be implemented quickly, ideally within four years.	90% federal funds, 10% local match required	Mid-cycle deadline was 2025.		https://wisconsindot.gov/Pages/doing-bus/local-gov/astnce-pgms/highway/hsip.aspx

Table 7.9 Grants - Wisconsin DOT

Grant Name	Source	Grant Goal/Purpose	Eligible Applicants	Eligible Projects/ Example Projects	Grant Amount/ Range/Limit	Deadlines/ Grant Cycle	Watertown Project	Section 4, Item C.
Federal Grants								
Recreational Trails Program (RTP)	Federal program administered by Wisconsin DNR	The RTP program provides funds to develop and maintain recreational trails and trail related facilities for both motorized and non-motorized recreational trail uses. RTP grants are to be used on trail projects that further a specific goal or are included in the State Comprehensive Recreation Plan (SCORP) or a local plan referenced in the SCORP.	Towns, villages, cities, counties, tribal governing bodies, school districts, state agencies, federal agencies or incorporated organizations are eligible to apply for funds.	Eligible RTP projects, in order of priority, are: • Rehabilitation and maintenance of existing trails and trailhead facilities; • Development of new trails, trail linkages and trailhead facilities (some restrictions apply on federal lands); and • Acquisition of property for trails in areas with limited trail opportunities. Note: applications for acquisition projects are not being accepted for State Fiscal Year 2025. "	Applicants can receive up to 80% reimbursement; individual projects cannot exceed \$100,000 (for 2025-2026 application cycle)	March 1, 2026 deadline (March 1 every year)	Regional trail connections, shared use paths (links to the Interurban Trail and other longer segments of shared use paths as shown on the network plan).	https://dnr.wisconsin.gov/aid/RTP.html
Land and Water Conservation Fund (LWCF) Program	Federal program administered by Wisconsin DNR	The federal LWCF program supports land acquisition and development of high-quality outdoor recreation amenities in local communities.	Towns, villages, cities, counties, tribal governments, school districts or other state political subdivisions	Generally, all acquisition and development projects that are eligible under the ADLP, UGS, and UR Stewardship subprograms are also eligible for LWCF grants. In addition, projects that provide outdoor recreation facilities that are not exclusively nature-based, such as active sports facilities, are eligible for LWCF grants.	Funds allocated proportional to population; compete against projects within your region		Riverwalk Extension, Riverside Park, Trailhead development, shared use path improvements.	https://dnr.wisconsin.gov/topic/Stewardship/FederalLUG
Better Utilizing Investments to Leverage Development (BUILD) Grant Program	US DOT	The U.S. Department of Transportation's (USDOT) Better Utilizing Investments to Leverage Development (BUILD) grant program provides grants for surface transportation infrastructure projects with significant local or regional impact.	States, local units of government, public agencies, public transportation authority, transit agency, multi-jurisdictional group of entities that are separately eligible	Highway or bridge projects, public transportation projects, passenger and freight rail transportation projects, projects to replace or rehabilitate a culvert or prevent stormwater runoff for the purpose of improving habitat, inter modal projects	Minimum award: \$1 million for rural capital projects; \$5 million for urban capital projects; Planning projects do not have a minimum award size; Maximum award: \$25 million; 20% match	Feb 24 2026 (annual notice of funding opportunity released, that determines deadline)		https://www.transportation.gov/BUILDgrants
Safe Streets and Roads for All (SS4A)	US DOT	The SS4A program funds regional, local, and Tribal initiatives through grants to prevent roadway fatalities and serious injuries.	Metropolitan planning organizations; local units of government; transit agencies; tribal governments	Planning and Demonstration Grants, Implementation Grants, Action Plans to address a roadway safety problem. Eligible projects and strategies can be infrastructural, behavioral, and/or operational activities.	Approximately \$1 billion is still available for the next funding round.	Notice of Funding Opportunity has not been released for FY 2026 yet; 2025 was due in June		https://www.transportation.gov/grants/SS4A

Table 7.10 Grants - Federal Grants

Grant Name	Source	Grant Goal/ Purpose	Eligible Applicants	Eligible Projects/ Example Projects	Grant Amount/ Range/Limit	Deadlines/ Grant Cycle	Watertown Projects	UR Section 4, Item C.
Non-Profit and Private Grants								
Community Change Grants	America Walks and CDC	This program funds projects related to creating healthy, active, and engaged places to live, work, and play.		Projects that demonstrate increased physical activity and active transportation	Funds 6 projects at \$2,000 each	Feb 10 2026		https://docs.google.com/forms/d/e/1FAIpQLSfl3LYjU0Uai9xueDUudIP_cPWR5z-8ur0fa1XqBH-QCTeyg/viewform
AARP Community Change Grants	AARP	A grant program to make communities more livable for people of all ages with tangible improvements that jump-start long-term change	Government entities, non-profit organizations	Projects that expand or improve safety of transportation and mobility options, including biking and walking. May be tangible (crosswalks, benches, bike lanes, etc.) or used to leverage additional support from other sources	Flagship grant average is between \$10,000-12,000; Capacity-building grant is up to \$2,500; Demonstration grants are typically between \$10,000-20,000	March 4 2026		https://www.aarp.org/livable-communities/community-challenge/info-2026/2026-challenge.html
Greater Watertown Community Foundation		The Foundation equips community change leaders with the tools they need to build strong families and thriving communities.		Not a defined grant program, but a community organization that offers funding -- write-in with a grant application				https://watertownhealthfoundation.com/GetInvolved.html#ApplyForFunding

Table 7.11 Grants - Non-Profit and Private Grants

**DRAFT RESOLUTION TO
ADOPT CITY-WIDE BICYCLE AND PEDESTRIAN NETWORK PLAN**

**SPONSOR: ALDERPERSON ARNETT, CHAIR
FROM: PUBLIC WORKS COMMISSION**

WHEREAS, the City of Watertown has many segments of bicycle and pedestrian routes but does not have a comprehensive bicycle and pedestrian path network plan; and,

WHEREAS, the City of Watertown Engineering Division applied for and was awarded an \$80,000 Transportation Alternatives Program Grant by the Wisconsin Department of Transportation for the development of a city-wide bicycle and pedestrian network plan; and,

WHEREAS, the City of Watertown Engineering Division, with support from HKGi, Inc., and with input and recommendations from the Bicycle and Pedestrian Path Task Force, Dodge County, Jefferson County, Watertown Unified School District, Luther Preparatory School and interested members of the public, has developed a comprehensive bicycle and pedestrian network plan, Public Works Commission has reviewed said plan and recommends adoption by the Common Council.

NOW, THEREFORE, BE IT RESOLVED BY THE COMMON COUNCIL OF THE CITY OF WATERTOWN, WISCONSIN:

That the Watertown City-wide Bicycle and Pedestrian Network Plan be adopted, for the City to consider and use as a guide to developing, connecting and improving bicycle and pedestrian routes throughout the City as appropriate when opportunities and funding allow.

	YES	NO
DAVIS		
LAMPE		
BERG		
BARTZ		
BLANKE		
SMITH		
ARNETT		
WETZEL		
MOLDENHAUER		
MAYOR STOCKS		
TOTAL		

ADOPTED March 17, 2026

CITY CLERK

APPROVED March 17, 2026

MAYOR

MEMO

Engineering Division of the Public Works Department

To: Chairman Arnett and Public Works Commission Members
From: Andrew Beyer, P.E., Director of Public Works/City Engineer
Date: 3/4/2026
Subject: 2025 MS4 Permit Annual Report

Background

The Wisconsin Department of Natural Resources requires an Annual Report be submitted summarizing actions taken to comply with the WPDES Municipal Separate Storm Sewer System (MS4) Permit during 2025. The report provides information on the following permit categories:

- Public Education and Outreach
- Public Involvement
- Illicit Discharge Detection and Elimination
- Construction Site Pollutant Control
- Post-Construction Storm Water Management
- Pollution Prevention (Municipal Operations)
- MS4 Map
- Total Maximum Daily Load (TMDL) Implementation

Detailed information for each permit category is included in the report.

The 2019-2024 MS4 Permit remains in effect until the next MS4 Permit is reissued.

Budget Goal

1. Proactively maintains and improves our parks and infrastructure to ensure safety, quality, and equity
2. Fosters community growth by assessing opportunities, stakeholder input, environmental needs, and modern code and policy priorities
3. Maintains a safe and healthy community, with an eye toward future needs and trends

Financial Impact

Engineering Division tracks expenditures for MS4 Permit compliance. Partnerships and grants have helped minimize program implementation costs over time.

Recommendation

Engineering Division staff recommend approval of the 2025 MS4 Permit Annual Report, to be submitted to Wisconsin Department of Natural Resources by the March 31st, 2026 deadline.

2026 Operational Goals

Present a budget that (Department select the relevant goals, and delete those not relevant):

1. Invests in the strategic planning and maintenance of our city buildings
2. Proactively maintains and improves our parks and infrastructure to ensure safety, quality, and equity
3. Supports employee retention and growth, while also evaluating operations and the associated staffing
4. Fosters community growth by assessing opportunities, stakeholder input, environmental needs, and modern code and policy priorities
5. Maintains a safe and healthy community, with an eye toward future needs and trends



2025 MS4 PERMIT ANNUAL REPORT

MARCH 2026

TODAY'S DISCUSSION

- Stormwater Program Goals
- MS4 Permit Annual Report Review
 - MS4 Programs in 2025
- Next Permit: 2026 – 2031 (?)



STORMWATER PROGRAM



- Flood Control – to minimize flooding impacts to public infrastructure and public/private properties
- **Pollutant Control – to protect quality of life for residents, property owners and meet Wisconsin Department of Natural Resources Stormwater (MS4) Permit**
- Maintenance so the systems work.



OUTREACH & EDUCATION / PUBLIC INVOLVEMENT

- City Connection Newsletter
- Social Media & Website
- Brochures @ City Hall
- Tables at Lights & Sirens, Boo Bash, Farmer's Markets
- 4th Annual Rain Barrel Workshop
- Citizen Stream Monitoring Training (2 sessions)
- Streambank Clean-up
- WI Stormwater Awareness Week
- Riverside Park Creek Streambank Plantings

*Add PWW,
RRC partner
logos*



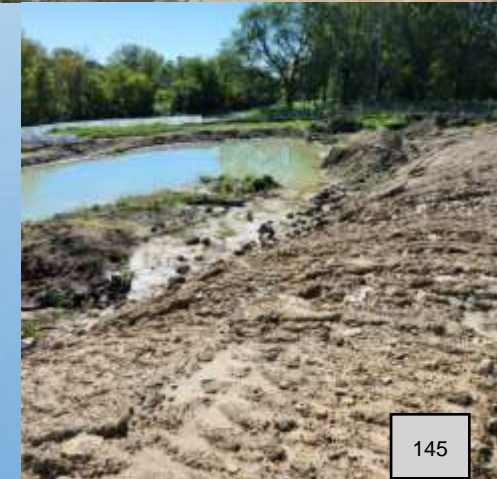
ILLICIT DISCHARGE DETECTION AND ELIMINATION (IDDE) / GIS UPDATES

- 408 Storm Sewer Outfalls to Rock River, creeks, wetlands
- 49 IDDE Inspections Completed; 13 Tested In-Field
- 0 “hits” detected
- Added recent storm pipe, BMPs & As-Built Data
- Completed Re-naming of Storm Structures in New Stormwater Layer in GIS



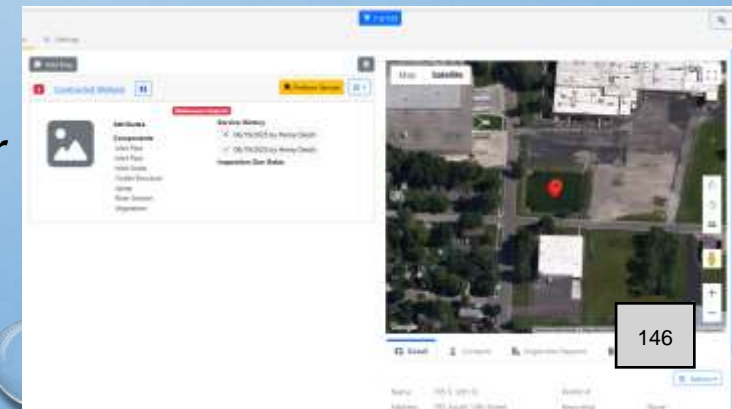
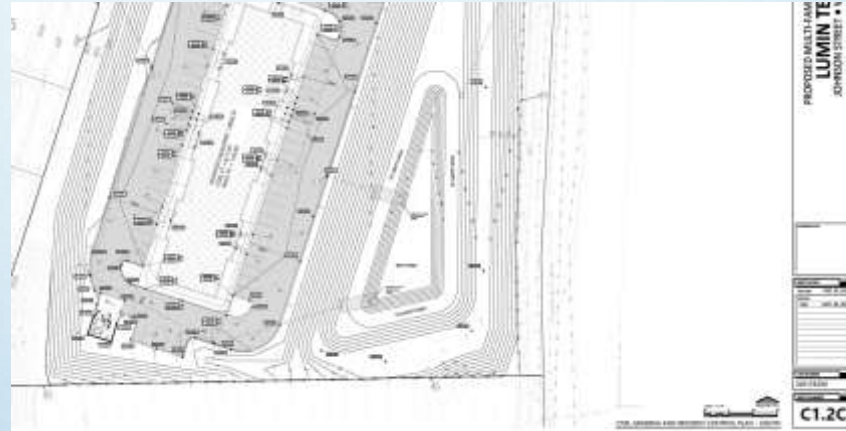
CONSTRUCTION SITE POLLUTANT CONTROL

- 139 Erosion Control Inspections completed in 2025
 - 14 Sites; 10 Sites over 1 Acre
 - Monthly EC Inspections
 - Weekly if there are problems on site
 - Bi-monthly for Inactive Sites
- Try to meet with site project manager at beginning of project, and again if there are concerns or noncompliance issues.



POST-CONSTRUCTION STORMWATER MANAGEMENT

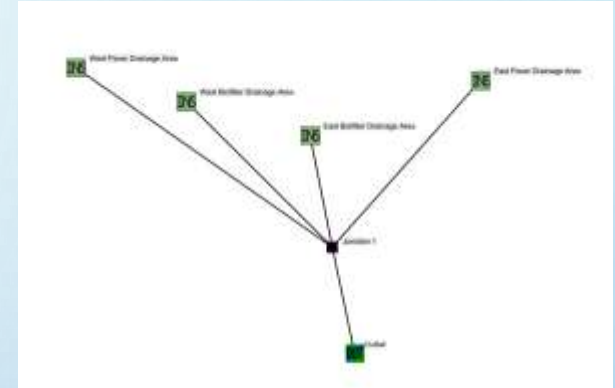
- New Stormwater BMPs in 2025:
 - Lumin Terrace – 1 Biofilter
 - Johnsonville – 1 biofilter (retrofit)
 - Hiawatha Street Bioswale
- Issued 12 EC & SW Permits; Completed 17 Plan Reviews & Revisions
- 62 BMPs Inspected
- MetaBMP Inspection & Maintenance Program Pilot Project
- W. Main Street Kwik Trip Swales and Ponds Maintained by Owner





POLLUTION PREVENTION

- Municipal Operations 2025 Stormwater Data:
 - 232 Lane Miles Swept
 - 450 yards of sediment and debris collected from street sweeper and catch basins
 - 2,996 tons of salt; 5,723 gallons of brine* applied to streets
 - WinSLAMM Training for 3 Engineering Div. Staff
 - SaltWise Training for 6 Parks Staff



SLAMM for Windows Version 10.5.0
 (c) Copyright Robert Pitt and John Vorhees 2019. All Rights Reserved

Data file name: C:\Nathan\As-Built\79482\James Street Lot Proposed.mdb
 WinSLAMM Version 10.5.0
 Run file name: C:\WinSLAMM Files\Rain\FastRain\FastRainReg - Madison WI 1981.RAN
 Particulate Solids Concentration file name: C:\WinSLAMM Files\10.1\WV_AW011.prc
 Runoff Coefficient file name: C:\WinSLAMM Files\10.1\SL06 Dec04.rvs
 Pollutant Relative Concentration file name: C:\WinSLAMM Files\10.1\UEC00.prc
 Residential Street Delivery file name: C:\WinSLAMM Files\10.1\Res and Other Urban Dec04.tbl
 Institutional Street Delivery file name: C:\WinSLAMM Files\10.1\Com Inst Urban Dec04.tbl
 Commercial Street Delivery file name: C:\WinSLAMM Files\10.1\Com Inst Indust Dec04.tbl
 Industrial Street Delivery file name: C:\WinSLAMM Files\10.1\Com Inst Indust Dec04.tbl
 Other Urban Street Delivery file name: C:\WinSLAMM Files\10.1\Res and Other Urban Dec04.tbl
 Freeway Street Delivery file name: C:\WinSLAMM Files\Freeway Dec04.tbl
 Apply Street Delivery Files to Adjust the After Event Load Street Dirt Mass Reason: False
 Source Area PSD and Peak to Average Flow Ratio File: C:\WinSLAMM Files\NWRP Source Area PSD Files.cbr
 Cost Data file name:
 Seed for random number generator: -42
 Study period starting date: 01/01/81 Study period ending date: 12/31/81
 Start of Winter Season: 12/01 End of Winter Season: 03/12
 Model Run Start Date: 01/01/81 Model Run End Date: 12/31/81
 Date of run: 11-10-2025 Time of run: 11:34:51
 Total Area Modeled (acres): 0.150
 Years in Model Run: 1.00

	Runoff Volume (cu ft)	Percent Runoff Reduction	Particulate Solids Conc. (mg/L)	Particulate Solids Yield (lb)	Percent Particulate Solids Reduction
Total of all Land Uses without Controls	11703		115.0	94.45	
Catchall Total with Controls	3711	68.40%	\$1.40	14.23	85.15%
Annualized Total After Catchall Controls	3723		\$1.40	14.26	

Pollutant	Conc. No Controls	Conc. With Controls	Conc. Units	Pollutant Yield No Controls	Pollutant Yield With Controls	Pol. Yield Units	Percent Reduction
Particulate Solids	115.0	\$1.40	mg/L	94.45	14.23	lb	85.15%
Total Phosphorus	0.2148	\$1.204	mg/L	0.1578	0.02636	lb	82.82%

Detritifier # 1 is expected to clog in 10.4 years... Percent Solids Reduction due to Engineered Media Not Used
 Detritifier # 2 is expected to clog in 9.70 years... Percent Solids Reduction due to Engineered Media Not Used



*Brine = 77% water, 23% salt mixture

TMDL / WATER QUALITY TRADING



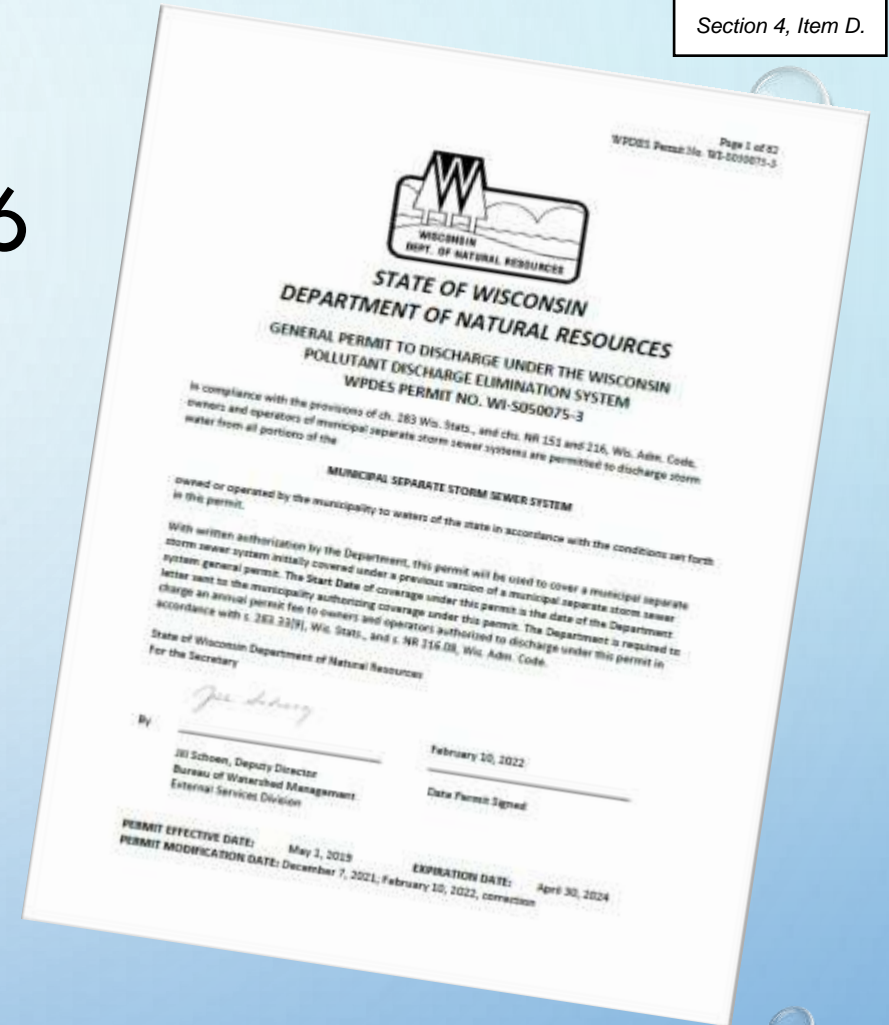
- 2022-2025 Total Pollutant Reductions:
 - **TSS: 26,791.50 Lbs.; TP: 79.9159 Lbs.** Reduced
- Next MS4 Permit TMDL Goals (20%TSS, 10%TP of Remainder)
 - TSS: **65,391 Lbs.**
 - Phosphorus: **152 Lbs.**

City-owned BMPs 2022-2025											
BMP Number	BMP Name	Site Name	NC TSS (lbs)	NC TP (lbs)	Proposed TSS (lbs)	Proposed TP (lbs)	Percent TSS Reduction	Percent TP Reduction	TSS reduction (lbs)	TP reduction (lbs)	
79	Permeable Pavers	Jones St Lot	64.12	0.1242	5.06	0.01009	92.11%	91.87%	59.06	0.1141	
80	Biofilters	Jones St Lot	20.33	0.0336	9.17	0.01827	54.92%	45.69%	11.17	0.0154	
81	Permeable Pavers	Cady St Lot	33.43	0.0553	3.61	0.00790	89.19%	85.71%	29.82	0.0474	
88	Biofilter	Yard Waste Site	6609	26.4900	1515.00	17.85000	77.10%	32.60%	5,094.00	8.6400	
92	South Biofilter	Reflection Park	5.79	0.0344	0.85	0.00531	85.41%	84.55%	4.95	0.0291	
93	West Biofilter	Reflection Park	15.46	0.0992	2.69	0.01689	82.63%	82.97%	12.77	0.0823	
94	Permeable Pavers	Senior Center	362.8	0.6000	242.90	0.40690	33.05%	32.18%	119.90	0.1931	
95	Biofilter	Watertown Court	3299	5.6000	1354.00	2.90000	58.96%	48.21%	1,945.00	2.7000	
96	Biofilter	Boomer St.-West	360.3		139.80		61.20%	46.60%	220.50	0.0000	
97	Biofilter	Boomer St. - East	375.4		145.66		61.20%	46.60%	229.74	0.0000	
102	Biofilter	Fire Station-North	1346	2.8890	269.10	0.57780	80.01%	80.00%	1,076.93	2.3112	
103	Biofilter	Fire Station-Middle									
104	Biofilter	Fire Station-South									
106	Biofilter	Hiawatha St. ROW	664	1.8440	662.54	0.38245	85.49%	79.26%	567.65	1.4616	
						0.00000			0.00	0.0000	
Total			13155.63	37.7697	4350.36	4.26715			9,371.50	6.665	
WQT #1	Buffer								17,420.00	73.2500	
Total with WQT Practice									26,791.50	79.9159	



COMING IN 2026

- Next 5-year MS4 Permit Anticipated in 2026
 - Develop Plans for Impaired Waterways
 - Updates to Written MS4 Permit Programs
 - IDDE Analysis, Implementation & Protocols
 - Greater Focus on Maintenance: Private & Public BMPs
 - Enhanced SW Pollution Prevention Plans (SWPPPs)
 - Salt Use Reporting, Calibration of Equipment
 - TMDL Reductions: 20% TSS, 10% Phosphorus (of remainder needed to meet TMDL)



**DRAFT RESOLUTION TO
AUTHORIZE SUBMITTAL OF 2025 ANNUAL STORMWATER REPORT**

**SPONSOR: ALDERPERSON ARNETT, CHAIR
FROM: PUBLIC WORKS COMMISSION**

WHEREAS, the City of Watertown is required to submit an annual stormwater report to the Wisconsin Department of Natural Resources (WDNR); and,

WHEREAS, the City of Watertown Engineering Division has completed said report and included all necessary information to be submitted with said report; and,

WHEREAS, the City of Watertown Public Works Commission has reviewed said report and recommends submittal to the WDNR.

NOW, THEREFORE, BE IT RESOLVED BY THE COMMON COUNCIL OF THE CITY OF WATERTOWN, WISCONSIN:

That the proper City officials are hereby authorized to submit the attached 2025 Annual Stormwater Report under Municipal Separate Storm Sewer System (MS4) General Permit Number WI-S050075-3 to the WDNR.

	YES	NO
DAVIS		
LAMPE		
BERG		
BARTZ		
BLANKE		
SMITH		
ARNETT		
WETZEL		
MOLDENHAUER		
MAYOR STOCKS		
TOTAL		

ADOPTED March 17, 2026

CITY CLERK

APPROVED March 17, 2026

MAYOR

Notice: Pursuant to s. NR 216.07(8), Wis. Adm. Code, an owner or operator of a Municipal Separate Storm Sewer System (MS4) is required to submit an annual report to the Department of Natural Resources (Department) by March 31 of each year to report on activities for the previous calendar year ("reporting year"). This form is being provided by the Department for the user's convenience for reporting on activities undertaken in each reporting year of the permit term. Personal information collected will be used for administrative purposes and may be provided to the extent required by Wisconsin's Public Records Law [ss. 19.31-19.39, Wis. Stats.].

Note: Compliance items must be submitted separately from this report to the Department.

Under s. 283.53(3)(a), a general MS4 permittee is required to reapply for permit coverage at least 180 days prior to the expiration date of the permit. In order to acknowledge that you are reapplying for permit coverage, please check the following box.

Part I. Municipal Contact Information

This form covers the activities during calendar year 2025

Name of Municipality City of Watertown	Facility ID No. (FIN) 31435	<input type="checkbox"/> Check to update mailing address information	
Mailing Address	City	State	ZIP Code

Primary Municipal Contact Person ([Authorized Representative](#) for MS4 Permit) Check to update contact information

Name	Title		
Mailing Address	City	State	ZIP Code
Phone Number (include area code)	Email		

Municipal Billing Contact Person ([Authorized Representative](#) for MS4 Permit) Check to update billing contact information

Name	Title		
Mailing Address	City	State	ZIP Code
Phone Number (include area code)	Email		

[+] Additional Contact Information (optional)

Individual with responsibility for (check all that apply):

- I&E Program IDDE Program IDDE Response Procedure Manual Municipal-wide Water Quality Plan
 Ordinances Pollution Prevention Program Post-Construction Program Winter Roadway Maintenance

First & Last Name Maureen McBroom	Title Stormwater Project Manager		
Mailing Address 106 Jones Street	City Watertown	State WI	ZIP Code 53094
Phone Number (include area code) (920) 206-4264	Email mmcbroom@watertownwi.gov		

[+] Additional Contact Information (optional)

Individual with responsibility for (check all that apply):

- I&E Program IDDE Program IDDE Response Procedure Manual Municipal-wide Water Quality Plan
 Ordinances Pollution Prevention Program Post-Construction Program Winter Roadway Maintenance

First & Last Name Stacy Winkelman	Title Operations Manager		
Mailing Address 811 S. First Street	City Watertown	State WI	ZIP Code 53094
Phone Number (include area code) (920) 262-4047	Email swinkelman@watertownwi.gov		

1. Does the municipality rely on another entity to satisfy any of the permit requirements? Yes No

Public Education and Outreach - Rock River Stormwater Group

Public Involvement and Participation - Rock River Stormwater Group

Illicit Discharge Detection and Elimination - _____

Construction Site Pollutant Control - _____

Post-Construction Storm Water Management - _____

Pollution Prevention

2. Has there been any changes to the municipality's participation in group efforts towards permit compliances (i.e., the municipality has added or dropped consortium membership)? Yes No Unsure

Part II. Storm Water Program Evaluation – Minimum Control Measures

1. Public Education and Outreach

a. Does MS4 conduct any educational efforts or events independently (not with a group) Yes No

b. How many total educational events were held during the reporting year:

c. Were any of the public education and outreach delivery mechanisms conducted during the reporting year active or interactive? Yes No

d. Please select all storm water topics, target audiences, and delivery mechanisms used in the reporting year.
 Use the [+] to add multiple Mechanisms.

Public Education and Outreach Delivery Mechanisms (Active and Passive)

Active/Interactive Mechanisms	Passive Mechanisms
<input type="checkbox"/> Education activities(school presentation, summer camps) <input type="checkbox"/> Information booth at event <input type="checkbox"/> Targeted group training (contractors, consultants, etc.) <input type="checkbox"/> Government event (public hearing, council meeting) <input type="checkbox"/> Workshops <input type="checkbox"/> Tours <input checked="" type="checkbox"/> Other: Co-presented WI SW Week webinar on Illicit Discharges	<input type="checkbox"/> Passive print media (brochures at front desk, posters, etc.) <input type="checkbox"/> Distribution of print media (mailings, newsletters, etc.) via mail or email. <input type="checkbox"/> Media offerings (radio and TV ads, press release, etc.) <input checked="" type="checkbox"/> Social media posts <input type="checkbox"/> Signage <input checked="" type="checkbox"/> Website <input checked="" type="checkbox"/> Other: webinar presentation on illicit discharges
<input type="checkbox"/> Education activities(school presentation, summer camps) <input type="checkbox"/> Information booth at event <input type="checkbox"/> Targeted group training (contractors, consultants, etc.) <input type="checkbox"/> Government event (public hearing, council meeting) <input checked="" type="checkbox"/> Workshops <input type="checkbox"/> Tours <input checked="" type="checkbox"/> Other: Stormwater presentation/Q&A at Rock River Coalition Rain Barrel Workshop	<input checked="" type="checkbox"/> Passive print media (brochures at front desk, posters, etc.) <input type="checkbox"/> Distribution of print media (mailings, newsletters, etc.) via mail or email. <input type="checkbox"/> Media offerings (radio and TV ads, press release, etc.) <input checked="" type="checkbox"/> Social media posts <input type="checkbox"/> Signage <input checked="" type="checkbox"/> Website <input checked="" type="checkbox"/> Other: Stormwater presentation at Rock River Coalition Rain Barrel Workshop
<input checked="" type="checkbox"/> Education activities(school presentation, summer camps) <input checked="" type="checkbox"/> Information booth at event <input checked="" type="checkbox"/> Targeted group training (contractors, consultants, etc.) <input checked="" type="checkbox"/> Government event (public hearing, council meeting) <input checked="" type="checkbox"/> Workshops <input type="checkbox"/> Tours	<input checked="" type="checkbox"/> Passive print media (brochures at front desk, posters, etc.) <input checked="" type="checkbox"/> Distribution of print media (mailings, newsletters, etc.) via mail or email. <input checked="" type="checkbox"/> Media offerings (radio and TV ads, press release, etc.) <input checked="" type="checkbox"/> Social media posts <input checked="" type="checkbox"/> Signage <input checked="" type="checkbox"/> Website

Active/Interactive Mechanisms	Passive Mechanisms
<input type="checkbox"/> Other:	<input type="checkbox"/> Other:
<input type="checkbox"/> Education activities(school presentation, summer camps) <input type="checkbox"/> Information booth at event <input checked="" type="checkbox"/> Targeted group training (contractors, consultants, etc.) <input type="checkbox"/> Government event (public hearing, council meeting) <input checked="" type="checkbox"/> Workshops <input type="checkbox"/> Tours <input checked="" type="checkbox"/> Other: North Central WI stormwater Consortium meeting presentation/discussion; Sweet Water Clean Rivers/Clean Lake Conference presentation/ Q&A	<input type="checkbox"/> Passive print media (brochures at front desk, posters, etc.) <input type="checkbox"/> Distribution of print media (mailings, newsletters, etc.) via mail or email. <input type="checkbox"/> Media offerings (radio and TV ads, press release, etc.) <input type="checkbox"/> Social media posts <input type="checkbox"/> Signage <input type="checkbox"/> Website <input checked="" type="checkbox"/> Other: Presentations on local water quality trading program (WWIP)
<input type="checkbox"/> Education activities(school presentation, summer camps) <input type="checkbox"/> Information booth at event <input checked="" type="checkbox"/> Targeted group training (contractors, consultants, etc.) <input type="checkbox"/> Government event (public hearing, council meeting) <input checked="" type="checkbox"/> Workshops <input type="checkbox"/> Tours <input checked="" type="checkbox"/> Other: Waukesha County Stormwater Workshop presentation/Q&A	<input type="checkbox"/> Passive print media (brochures at front desk, posters, etc.) <input type="checkbox"/> Distribution of print media (mailings, newsletters, etc.) via mail or email. <input type="checkbox"/> Media offerings (radio and TV ads, press release, etc.) <input type="checkbox"/> Social media posts <input type="checkbox"/> Signage <input type="checkbox"/> Website <input checked="" type="checkbox"/> Other: Presentation on local water quality trading program (WWIP)
Topics Covered	Target Audience
<input checked="" type="checkbox"/> Illicit discharge detection and elimination <input checked="" type="checkbox"/> Household hazardous waste disposal/pet waste management/ vehicle washing <input checked="" type="checkbox"/> Yard waste management/pesticide and fertilizer application <input checked="" type="checkbox"/> Stream and shoreline management <input checked="" type="checkbox"/> Residential infiltration <input checked="" type="checkbox"/> Construction sites and post-construction storm water management <input checked="" type="checkbox"/> Pollution prevention <input checked="" type="checkbox"/> Green infrastructure/low impact development <input checked="" type="checkbox"/> Other: <u>Describe</u>	<input checked="" type="checkbox"/> General Public <input checked="" type="checkbox"/> Public Employees <input checked="" type="checkbox"/> Residents <input checked="" type="checkbox"/> Businesses <input checked="" type="checkbox"/> Contractors <input checked="" type="checkbox"/> Developers <input checked="" type="checkbox"/> Industries <input checked="" type="checkbox"/> Public Officials <input type="checkbox"/> Other
<p>e. Will additional information/summary of education events be attached to the annual report? <input checked="" type="radio"/> Yes <input type="radio"/> No</p> <p>If no, please provide additional comment in the brief explanation box below. <i>Limit response to 250 characters and/or attach supplemental information in the attachments page.</i></p>	
<p>f. Brief explanation on Public Education and Outreach reporting. <i>Limit response to 250 characters and/or attach supplemental information on the attachments page.</i></p> <p>See City of Watertown 2025 Annual Stormwater Report Supplemental MS4 Program Information for additional details.</p>	

Minimum Control Measurements - Section 2: Has Missing Items

2. Public Involvement and Participation

a. Permit Activities. Select all of the following topics the Permittee did to engage public participation and involvement. Use the [+] to add multiple Mechanisms.

Topics Covered	Target Audience	Estimated People Reached (optional)	Regional Effort? (optional)	Topic
<input type="checkbox"/> MS4 Annual Report <input type="checkbox"/> Storm Water Management Program <input type="checkbox"/> Storm water related ordinance <input checked="" type="checkbox"/> Other: Describe SW WI Building Inspector Training - stormwater presentation	<input type="checkbox"/> General Public <input checked="" type="checkbox"/> Public Employees <input type="checkbox"/> Residents <input type="checkbox"/> Businesses <input type="checkbox"/> Contractors <input type="checkbox"/> Developers <input type="checkbox"/> Industries <input type="checkbox"/> Public Officials <input type="checkbox"/> Other	11-50	<input checked="" type="radio"/> Yes <input type="radio"/> No	<input type="checkbox"/> N <input type="checkbox"/> S <input type="checkbox"/> C <input checked="" type="checkbox"/> P

b. Volunteer Activities. Select all of the following audiences targeted for volunteer involvement and participation related to storm water. Use the [+] to add multiple Mechanisms.
 NA (Individual Permittee)

Topics Covered	Target Audience	Estimated People Reached (optional)	Regional Effort? (optional)	Topic
<input checked="" type="checkbox"/> Volunteer Opportunity	<input checked="" type="checkbox"/> General Public <input type="checkbox"/> Public Employees <input checked="" type="checkbox"/> Residents <input type="checkbox"/> Businesses <input type="checkbox"/> Contractors <input type="checkbox"/> Developers <input type="checkbox"/> Industries <input type="checkbox"/> Public Officials <input type="checkbox"/> Other	1-10	<input checked="" type="radio"/> Yes <input type="radio"/> No	<input checked="" type="checkbox"/>

c. Brief explanation on Public Involvement and Participation reporting. *Limit response to 250 characters and/or attach supplemental information on the attachments page.*
 RRSg worked with WRA to provide a presentation to building inspectors in Fitchburg on 5/15/25.
 See City of Watertown 2025 Annual Stormwater Report Supplemental MS4 Program Information for additional details.

3. Illicit Discharge Detection and Elimination

a. How many total outfalls does the municipality have?	408
b. How many major outfalls does the municipality have?	37
c. How many outfalls did the municipality evaluate as part of their routine ongoing field screening program?	49
d. From the municipality's routine screening, how many were confirmed illicit discharges?	0
e. How many illicit discharge complaints did the municipality receive?	0
f. From the complaints received, how many were confirmed illicit discharges?	0
g. How many of the identified illicit discharges did the municipality eliminate in the reporting year (from both routine screening and complaints)? (If the sum of 3.d. and 3.f. does not equal 3.g., please explain below.)	0

h. What types of regulatory mechanisms does the municipality have available to compel compliance with this program? Check all that are available and how many times each were used in the reporting year.

- Verbal Warning 0
- Written Warning (including email) 0
- Notice of Violation 0
- Civil Penalty/Citation 0

Additional information:

j. Brief explanation on Illicit Discharge Detection and Elimination reporting. *Limit response to 250 characters and/or attach supplemental information on the attachments page.*

See City of Watertown 2025 Annual Stormwater Report Supplemental MS4 Program Information for additional details.

4. Construction Site Pollutant Control

a. How many total construction sites with one acre or more of land disturbing construction activity were active at any point in the reporting year?

10

b. How many construction sites with one acre or more of land disturbing construction activity did the municipality issue permits for in the reporting year?

3

c. How many erosion control inspections did the municipality complete in the reporting year (at sites with one acre or more of land disturbing construction activity)?

46

d. What types of regulatory mechanisms does the municipality have available to compel compliance with this program? Check all that are available and how many times each were used in the reporting year.

- Verbal Warning 4
- Written Warning (including email) 7
- Notice of Violation 2
- Civil Penalty/Citation 0
- Stop Work Order 0
- Forfeiture of Deposit
- Other – Describe below 0

Building Permits and Occupancy Permits are typically issued after other permit and approval conditions have been met.

e. Brief explanation on Construction Site Pollutant Control reporting. *Limit response to 250 characters and/or attach supplemental information on the attachments page.*

See City of Watertown 2025 Annual Stormwater Report Supplemental MS4 Program Information for additional details.

5. Post-Construction Storm Water Management

- a. How many new structural storm water management Best Management Practice (BMP) have received local approval? 5
 - * Engineered and constructed systems that are designed to provide storm water quality control such as wet detention ponds, constructed wetlands, infiltration basins, grassed swales, permeable pavement, etc.
- b. Does the MS4 have procedures for inspecting and maintaining private storm water facilities? Yes No
- c. If Yes, how many privately owned storm water management facilities were inspected in the reporting year? 32
 - inspections completed by private landowners should be included in the reported number.
- d. Does the municipality utilize privately owned storm water management BMP in its pollutant reduction analysis? Yes No
- e. Does MS4 have maintenance authority on these privately owned BMPs? Yes No
- f. What types of enforcement actions does the municipality have available to compel compliance with the regulatory mechanism? Check all that apply and enter the number of each used in the reporting year.

<input checked="" type="checkbox"/> Verbal Warning	
<input checked="" type="checkbox"/> Written Warning (including email)	
<input checked="" type="checkbox"/> Notice of Violation	
<input checked="" type="checkbox"/> Civil Penalty/Citation	0
<input type="checkbox"/> Forfeiture of Deposit	
<input checked="" type="checkbox"/> Complete Maintenance	0
<input checked="" type="checkbox"/> Bill Responsible Party	0
<input type="checkbox"/> Other – Describe below	

g. Brief explanation on Post-Construction Storm Water Management reporting. *Limit response to 250 characters and/or attach supplemental information on the attachments page.*
 Building Permits and/or Occupancy Permits are typically issued after other permit and approval conditions have been met.
 See City of Watertown 2025 Annual Stormwater Report Supplemental MS4 Program Information for additional details.

6. Pollution Prevention

- Storm Water Management Best Management Practice Inspections** Not Applicable
- a. Enter the total number of "municipally owned" (i.e., publicly owned BMPs) or operated (i.e., privately owned BMPs) structural storm water management best management practices. 32
 - b. How many new municipally owned storm water management best management practices were installed in the reporting year? 1
 - c. How many municipally owned (public) storm water management best management practices were inspected in the reporting year? 27
 - d. What elements are looked at during inspections (250 character limit)?
 Vegetation, erosion, inlets, outlets, riprap, water depth, emergency overflows, clogging between pavers, etc.
 - e. How many of these facilities required maintenance? 18
 - f. Brief explanation on Storm Water Management Best Management Practice inspection reporting. *Limit response to 250 characters and/or attach supplemental information on the attachments page.*
 See City of Watertown 2025 Annual Stormwater Report Supplemental MS4 Program Information for additional details.

Public Works Yards & Other Municipally Owned Properties that require a stormwater pollution prevention plan (SWPPP) Not Applicable

g. How many municipal properties require a SWPPP?

h. How many inspections of municipal properties have been conducted in the reporting year?

i. Have amendments to the SWPPPs been made? Yes No

j. If yes, describe what changes have been made. *Limit response to 250 characters and/or attach supplemental information on the attachment page:*
 A second street sweepings storage area was created at upper City Quarry site.

k. Brief explanation on Storm Water Pollution Prevention Plan reporting. *Limit response to 250 characters and/or attach supplemental information on the attachments page.*

*Any municipally owned property that has the potential to generate stormwater pollution should have a SWPPP. For example, if a municipal property stores compost piles, material storage, yard wastes, etc., outside and can contaminate stormwater runoff-a SWPPP is required.

Collection Services - Street Sweeping Program Not Applicable

l. Did the municipality conduct street sweeping during the reporting year? Yes No

m. If known, how many tons of material was removed?

n. Does the municipality have a low hazard exemption for this material? Yes No

o. If street sweeping is identified as a storm water best management practice in the pollutant loading analysis, was street cleaning completed at the assumed frequency?
 Yes - Explain frequency _____
 No- Explain _____
 Not Applicable _____

Collection Services - Catch Basin Sump Cleaning Program Not Applicable

p. Did the municipality conduct catch basin sump cleaning during the reporting year? Yes No

q. How many catch basin sumps were cleaned in the reporting year?

r. If known, how many tons of material was collected?

s. Does the municipality have a low hazard exemption for this material? Yes No

t. If catch basin sump cleaning is identified as a storm water best management practice in the pollutant loading analysis, was cleaning completed at the assumed frequency?
 Yes - Explain frequency _____
 No-Explain _____
 Not Applicable _____

Collection Services - Leaf Collection Program Not Applicable

u. Does the municipality conduct curbside leaf collection? Yes No

v. Does the municipality notify homeowners about pickup? Yes No

w. Where are the residents directed to store the leaves for collection?
 Pile on terrace Pile in street Bags on terrace
 Other - Describe Drop off at City Yard Waste Site.

x. What is the frequency of collection? Minimum of once per month. Weather permitting, usually 2-3 times month.

y. Is collection followed by street sweeping/cleaning? Yes No

z. Brief explanation on Collection Services reporting. *Limit response to 250 characters and/or attach supplemental information on the attachments page.*

Winter Road Management Not Applicable

*Note: We are requesting information that goes beyond the reporting year, answer the best you can.

aa. How many lane-miles of roadway is the municipality responsible for doing snow and ice control? (One mile of a two-way road equals two lane miles.)

120

ab. Provide amount of de-icing products used by month last winter season.

Solids (tons) (ex. sand, or salt-sand)

Product	Oct	Nov	Dec	Jan	Feb	Mar
Salt		280	1,183	320	813	400

Liquids (gallons) (ex. brine)

Product	Oct	Nov	Dec	Jan	Feb	Mar
Brine		1,200	0	2,200	2,323	0

ac. Was salt applying machinery calibrated in the reporting year? Yes No

ad. Have municipal personnel attended salt reduction strategy training in the reporting year? Yes No

Date	Training Name	How many attended
11/12/2025	WI Salt Wise Training - Waukesha	6

ae. Brief explanation on Winter Road Management reporting. *Limit response to 250 characters and/or attach supplemental information on the attachments page.*

See City of Watertown 2025 Annual Stormwater Report Supplemental MS4 Program Information for additional details.

Internal (Staff) Education & Communication

af. Has the municipality provided an opportunity for internal training or education to staff implementing the municipality's procedures for each of the pollution prevention program element? Yes No

If yes, describe what training was provided (250 character limit):

WinSLAMM training (3 engineering staff); small site erosion control training/building inspectors training; Salt Awareness Week webinars, WI Stormwater Week webinars, Sweet Water Clean Rivers/Clean Lake Conference, Waukesha County Stormwater Workshop

ag. Describe how the municipality has kept local officials and municipal staff aware of the municipal storm water discharge permit programs, procedures and pollution prevention program requirements.

Elected officials: Public Works Commission, Finance Committee, Committee of the Whole (Common Council)

Municipal officials: Mayor Stocks

Appropriate staff (such as operators, Department heads, and those that interact with the public):

Director of Public Works/City Engineer & Engineering Div staff, Streets & Operations Management and staff, Parks Director and staff

ah. Brief explanation on Internal Education reporting. *Limit response to 250 characters and/or attach supplemental information on the attachments page.*

See City of Watertown 2025 Annual Stormwater Report Supplemental MS4 Program Information for additional details.

7. Storm Sewer System Map

a. Did the municipality update their storm sewer map this year? Yes No

If yes, check the areas the map items that got updated or changed:

- Storm water treatment facilities
- Storm pipes
- Vegetated swales
- Outfalls
- Other - Describe below

b. Brief explanation on Storm Sewer System Map reporting. *Limit response to 250 characters and/or attach supplemental information on the attachments page.*

See City of Watertown 2025 Annual Stormwater Report Supplemental MS4 Program Information for additional details.

Part III. Final Evaluation

1. **Fiscal Analysis** Complete the fiscal analysis table provided below. For municipalities that do not break out funding into permit program elements, please enter the monetary amount to your best estimate of what funding may be going towards these programs.

Program Element	Annual Expenditure Reporting Year	Budget Reporting Year	Budget Upcoming Year	Source of Funds
Public Education and Outreach	\$9,789.00	\$11,000.00	\$8,500.00	Storm water utility
Public Involvement and Participation	\$6,580.00	\$14,000.00	\$2,500.00	Storm water utility
Illicit Discharge Detection and Elimination	\$225.00	\$1,500.00	\$1,500.00	Storm water utility
Construction Site Pollutant Control	\$16,320.00	\$16,500.00	\$15,200.00	Storm water utility
Post-Construction Storm Water Management	\$13,300.00	\$55,000.00	\$60,000.00	Storm water utility
Pollution Prevention	\$23,670.00	\$35,000.00	\$35,000.00	Storm water utility
Storm Water Quality Management	\$0.00	\$0.00	\$0.00	Storm water utility
Storm Sewer System Map	\$16,770.00	\$16,000.00	\$21,834.00	Storm water utility
Other: Water Quality Trading program	\$14,980.00	\$58,000.00	\$65,000.00	Storm water utility
Other: MS4 Permit Fee	\$5,000.00	\$5,000.00	\$5,000.00	Storm water utility

Please provide a justification for any zeros ("0") entered in the Fiscal Analysis. *Limit response to 250 characters.*
 Storm water quality management analysis was completed in 2023 under UNPS Planning Grant project.

2. Water Quality

a. Were there any known water quality improvements in the receiving waters to which the municipality's storm sewer system directly discharges to? Yes No Unsure

If so, explain: (additional stormwater BMPs, WQT practice)

b. Were there any known water quality degradation in the receiving waters to which the municipality's storm sewer system directly discharges to? Yes No Unsure

If so, explain:

c. Have any of the receiving waters that the municipality discharges to been added to the impaired waters list during the reporting year? Yes No Unsure

d. Has the municipality evaluated their storm water practices to reduce the pollutants of concern? Yes No Unsure

3. Storm Water Quality Management

a. Has the municipality completed or updated modeling in the reporting year (relating to developed urban area performance standards of s. NR 151.13(2)(b)1., Wis. Adm. Code)? Yes No

b. If yes, enter percent reduction in the annual average mass discharging from the entire MS4 to surface waters of the state as compared to implementing no storm water management controls:

Total suspended solids (TSS) _____

Total phosphorus (TP) _____

4. Total Maximum Daily Loads

a. For permittees covered under the MS4 individual permits only, does the municipality discharge to any of the following approved TMDLs? (Select all that apply.)

- Rock River Basin and/or Beaver Dam Lake
 Lower Fox River Basin and Lower Green Bay
 Lake St. Croix
 Tainter Lake and Lake Menomin Milwaukee River
 Wisconsin River
 Upper Fox and Wolf River Basin
 Other:
 Does not apply

b. Status of TMDL implementation.

The permittee City of Watertown is subject to the following approved TMDLS: [autopopulated].

Auto-populates from DNR database based on past reporting.

WQT and other urban stormwater practices

The permittee intends to comply with the following permit requirements to show progress towards meeting the TMDL: [autopopulated]

Auto-populates from DNR database based on past reporting.

WQT and other urban stormwater practices

[A.2] The Permittee requested and received department concurrence that the TMDL pollutant reductions is currently being met in all applicable reachsheds.

The permittee is confirming that they are maintaining all storm water management facilities, continuing street sweeping, and any other actions to continue maintenance of pollution control. Agree Disagree

[A.3.1] The Permittee is following the TMDL Compliance Plan, which received department concurrence prior to April 30, 2019.

The permittee is confirming that all planned efforts are on schedule. Agree Disagree

[A.3.2] The Permittee is participating in an approved Adaptive Management Project.

Attach a summary of adaptive management implementation actions for the reporting year, including:

- Most recent estimated pollutant of concern percent reduction levels (i.e. total phosphorus and total suspended solids/ sediment), as compared to no controls by reachshed, within the permittee's MS4 permitted area.
- Pollutant of concern percent reduction levels, as compared to no controls by reachshed, which the permittee intends to ultimately achieve within its own MS4 permitted area (not associated with AM buy-in).
- The financial dollar value contributed to an AM program for the reporting year.
- Identify any additional storm water measures that were initially implemented in the reporting year, which reduce the discharge of pollutants of concern from its MS4 permitted area (not associated with AM buy-in). If available, identify the incremental percent reduction gained by such measures relative to the MS4 permitted area.

[A.4] The Permittee will demonstrate that the TMDL pollutant reductions will be met in all applicable reachsheds by October 31, 2023.

The permittee is confirming that all planned efforts are on schedule. Agree Disagree

[A.5.3] The Permittee will demonstrate an optimization of measures defined in the permit by October 31, 2023.

The permittee is confirming that all planned efforts are on schedule.

Agree Disagree

[A.6.3] Final Documentation.

The permittee is confirming that all planned efforts are on schedule to submit the final documentation materials [updates to mapping, modeling, tabular summary, and Implementation Plan] under section A.6.3 by October 31, 2023.

Agree Disagree

[B.3-4] The permittee is confirming that the appropriate documents, due March 31, 2022, has already been submitted or is being submitted with this annual report.

- For an Adaptive Management project, a plan is required.
- For TMDL Implementation, updates to mapping, modeling, tabular summary, and Implementation Plan documents are required.

Agree Disagree

[B.5.2] Bacteria sources map and inventory.

The permittee is confirming that the appropriate documents, due March 31, 2022, has already been submitted or is being submitted with this annual report.

Agree Disagree

[B.5.2.b] The Permittee will be submitting a bacteria source elimination plan.

The permittee is confirming that all planned efforts are on schedule to submit the required information by October 31, 2023.

Agree Disagree

[B.5.3] The Permittee will be adopting local ordinances to address potential sources of bacteria entering the MS4.

The permittee is confirming that all planned efforts are on schedule to submit the required information by March 31, 2023.

Agree Disagree

[B.6.3] Final Documentation.

The permittee is confirming that all planned efforts are on schedule to submit the required information by March 31, 2023.

Agree Disagree

[C.3-4] The Permittee is confirming that all planned efforts are on schedule to meet requirements due to the department.

- For an Adaptive Management project, a plan is required within 36 months of the TMDL approval date.
- For TMDL Implementation, updates to mapping, modeling, tabular summary, and Implementation Plan documents are required within 48 months of the TMDL approval date.)

Agree Disagree

5. Additional Information Based on the municipality's storm water program evaluation in Part II, describe any proposed changes to the municipality's storm water program. If your response exceeds 250 characters, attach supplemental information on the attachments page.
See City of Watertown 2025 Annual Stormwater Report Supplemental MS4 Program Information for additional details.

Part IV. Request for Assistance on Understanding Permit Programs (optional)

Would the municipality like the Department to contact them about providing more information on understanding any of the Municipal Separate Storm Sewer Permit programs? Please select from the options below.

- | | |
|--|---|
| <input type="checkbox"/> Public Education and Outreach | <input type="checkbox"/> Public Involvement and Participation |
| <input type="checkbox"/> Illicit Discharge Detection and Elimination | <input type="checkbox"/> Construction Site Pollutant Control |
| <input type="checkbox"/> Post-Construction Storm Water Management | <input type="checkbox"/> Pollution Prevention |
| <input type="checkbox"/> Storm Water Quality Management | <input type="checkbox"/> Water Quality Concerns |
| <input type="checkbox"/> Storm Sewer System Map | <input type="checkbox"/> Compliance Schedule Items Due |
| | <input type="checkbox"/> MS4 Program Evaluation |

Certification

I hereby certify that I am an authorized representative of the municipality covered under City of Watertown's MS4 Permit for which this annual report is being submitted and that the information contained in this document and all attachments were gathered and prepared under my direction or supervision. Based on my inquiry of the person or persons under my direction or supervision involved in the preparation of this document, to the best of my knowledge, the information is true, accurate, and complete. I further certify that the municipality's governing body or delegated representatives have reviewed or been apprised of the contents of this annual report. I understand that Wisconsin law provides severe penalties for submitting false information.

Authorized Representative Printed Name	Authorized Representative Printed Title
Email abeyer@watertownwi.gov	Phone Number (920) 262-4060

Signature of Authorized Representative

Date Signed (mm/dd/yyyy)

City of Watertown
2025 Annual Stormwater Report
Supplemental MS4 Program Information
February 2026

To meet the requirements the Wisconsin Department of Natural Resources WPDES
Municipal Separate Storm Sewer System (MS4) Permit Annual Report (Section 2.9)

Public Education and Outreach

The City of Watertown (City) participates in the Rock River Stormwater Group's "Protect Wisconsin Waterways" campaign and other stormwater programs (see RRSB annual report).

City staff helped to plan WI Stormwater Week and also co-presented a webinar on Illicit Discharges during this week. City staff co-presented at multiple professional conferences on the City's local water quality trading program to meet the MS4 Permit requirements of the Rock River Basin Total Maximum Daily Load (TMDL) study. Additional stormwater information is shared via the City's website, in social media posts, and in person conversations with citizens, property owners, and other parties interested in stormwater issues.

Public Involvement and Participation

The City partnered with the Rock River Coalition to host a rain barrel workshop, a citizen stream monitoring training event (spring) and a streambank planting event (fall) in 2025.

The RRSB coordinated 10 stream clean-up sites with over 200 volunteers on September 20, 2025.

RRSB worked with WRA to provide a presentation to building inspectors in Fitchburg on 5/15/25.

The Rock River Stormwater Group contracts with the UW-Whitewater chapter of the American Marketing Association, who runs Creative Marketing Unlimited (CMU). <https://amawhitewater.org/aboutcmu/> CMU students learn about stormwater runoff issues and share that message with targeted audiences (usually the general public) via the Protect Wisconsin Waterways campaign, which includes developing messaging for social media posts, talking to event attendees at in-person tables and handing out promotional items, coordinating and attending stream clean-up events, and more.

Illicit Discharge Detection and Elimination (IDDE)

Outfall Screening

City staff screened 49 priority storm sewer outfalls for illicit discharges in 2025. Of the 49 outfalls, 14 outfalls had flow, and 13 were sampled and tested in the field for pollutants. No pollutants above acceptable action levels were detected in 2025.

One (1) outfall had flow, but was not sampled for pollutants, due to the presence of groundwater from sump pump discharges upstream. These outfalls had previously been tested for 5 years or more with no pollutants being detected above action limits. In 2022, WDNR approved visually screening these outfalls but not running tests on the water due to the long-term presence of groundwater (sump pump discharges) in the storm sewer system.

The City's total number of outfalls has been updated to 408 (previously reported as 553) following corrections and updates to the City's GIS system.

Spills

City Stormwater Utility staff were added to the DNR Spills Notification list on 9/30/2025.

Local spills that were called in to the DNR prior to staff knowledge included:

- 9/30/25 WI SPILL ID 20250930SC28-1 – JET FUEL
 - A small plane went off the runway and into a swale. No one was hurt. The fire department determined there was no fuel spill, so no clean-up actions were implemented. The City's airport has a mix of storm pipe, swales and then a small creek on the south end of the property. Most of the storm pipe is up near the fueling area, which is covered by a WDNR industrial stormwater permit. The rest of the airport, including the swales that run along the runways, the creek, etc., are covered by the MS4 Permit.

- 8/11/2025 WI SPILL ID 20250811SC28-1 – WASTEWATER SLUDGE (WASTEWATER)
 - A vehicle or vessel collision resulted in a discharge of wastewater sludge from an industrial facility. This facility is covered under a DNR industrial stormwater runoff permit FIN # 7832, FID # 128008870.

Construction Site Pollutant Control

The erosion control portion of the City’s Erosion Control & Storm Water Runoff Permit and Chapter 288, Municipal Code, applies to construction sites with 3,000 square feet or more of land disturbance. City staff completed a total of 138 erosion control inspections in 2025 (55 erosion control inspections specifically to meet the MS4 Permit; an additional 83 erosion control inspections were completed on city projects to meet NOI/City Erosion Control Permit/MS4 Permit requirements.)

Post-Construction Stormwater Management

City staff completed 62 stormwater BMP inspections during 2025.

The City partnered with Nathan Minor of Drainage Doctors on stormwater BMP inspections and maintenance tracking in MetaBMP. Thirty City-owned BMPs were entered into MetaBMP and inspected through that system as a pilot project. 122 culverts were also entered into MetaBMP to track inspections and maintenance of those structures, as part of the pilot project. City staff continue to work with N. Minor on system recommendations from a municipal perspective, desired reports, and more.

Chapter 288, municipal code was updated to address edits from the 2022 update, developments that drain directly to the Rock River (a river segment drainage more than 500 square miles), the minor road reconstruction definition, and to clarify the City’s citation authority.

Pollution Prevention

Internal (Staff) Training:

- Parks Department staff who clear snow and ice from parks and other municipal properties attended the WI Salt Wise training in November 2025.
- RRSB hosted two training dates on erosion control in September 2025 targeting public works employees.
- Six (6) Parks Department staff attended the WI Salt Wise training in Waukesha in November 2026.

- Public Works Commission presentation re: MS4 Permit Annual Report, SW Utility Rate Study presentation(s)
- Finance Committee, Committee of the Whole (Common Council) re: stream assessment grant (re: erosion, creek conditions and flooding)

Storm Sewer System Map

The City's stormwater system (catch basins, inlets, outfalls, manholes, etc.) was inventoried in the field in 2021-2022. This information was compiled in GIS format, and transferred into a new stormwater layer on the City's internal GIS site. Further confirmation of complicated and/or historic sites within the system have been addressed throughout the following field seasons. Jefferson County GIS is now managing the City's GIS, and adding features such as flow arrows, color-coded pipes for different size pipes, improved query features, and more.

Rock River TMDL

The City's water quality trading program, Watertown Waterways Improvement Program, is implemented by Jefferson County Land and Water Conservation Department. Multiple sites were considered in 2025, but ultimately an agreement with the property owner could not be reached. Inspections and the annual payment to the owner of the 1st approved water quality trading project were completed in 2025.

City crews installed a bioswale along Hiawatha Street in the ROW to manage stormwater that was previously running onto private property. Street reconstruction projects include catch basin retrofits with 36-inch deep sumps where stormwater does not flow to an existing stormwater BMP.

Private new and redevelopment projects resulted in a new biofilter and a retrofitted biofilter in 2025.

Additional urban stormwater BMPs will be considered and installed as appropriate to meet requirements and maximize stormwater treatment, most likely with future flood control practices.

City-owned BMPs 2022-2025										
BMP Number	BMP Name	Site Name	NC TSS (lbs)	NC TP (lbs)	Proposed TSS (lbs)	Proposed TP (lbs)	Percent TSS Reduction	Percent TP Reduction	TSS reduction (lbs)	TP reduction (lbs)
79	Permeable Pavers	Jones St Lot	64.12	0.1242	5.06	0.01009	92.11%	91.87%	59.06	0.1141
80	Biofilters	Jones St Lot	20.33	0.0336	9.17	0.01827	54.92%	45.69%	11.17	0.0154
81	Permeable Pavers	Cady St Lot	33.43	0.0553	3.61	0.00790	89.19%	85.71%	29.82	0.0474
88	Biofilter	Yard Waste Site	6609	26.4900	1515.00	17.85000	77.10%	32.60%	5,094.00	8.6400
92	South Biofilter	Reflection Park	5.79	0.0344	0.85	0.00531	85.41%	84.55%	4.95	0.0291
93	West Biofilter	Reflection Park	15.46	0.0992	2.69	0.01689	82.63%	82.97%	12.77	0.0823
94	Permeable Pavers	Senior Center	362.8	0.6000	242.90	0.40690	33.05%	32.18%	119.90	0.1931
95	Biofilter	Watertower Court	3299	5.6000	1354.00	2.90000	58.96%	48.21%	1,945.00	2.7000
96	Biofilter	Boomer St.-West	360.3		139.80		61.20%	46.60%	220.50	0.0000
97	Biofilter	Boomer St. - East	375.4		145.66		61.20%	46.60%	229.74	0.0000
102	Biofilter	Fire Station-North	1346	2.8890	269.10	0.57780	80.01%	80.00%	1,076.93	2.3112
103	Biofilter	Fire Station-Middle								
104	Biofilter	Fire Station-South								
106	Biofilter	Hiawatha St. ROW	664	1.8440	662.54	0.38245	85.49%	79.26%	567.65	1.4616
						0.00000			0.00	0.0000
Total			13155.63	37.7697	4350.36	4.26715			9,371.50	6.6659
WQT #1	Buffer								17,420.00	73.2500
Total with WQT Practice									26,791.50	79.9159

Figure 1: TSS & TP Reductions, City-Owned BMPs & WQT Practice, 2022-2025.



2025 Final Report

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2025 Highlights

Annual Report 2025 Metrics Highlight

In 2025, the Protect Wisconsin Waterways (Rock River Stormwater Group) focused on organic and partner-driven digital reach and had an **in-person presence at 37 events** (including the clean-ups). Tabling and other activities at in-person events **engaged 2,014+ individuals**. The Protect Wisconsin Waterways annual cleanup held in September included **213 volunteers** across **10 locations**. Combined with Protect Wisconsin Waterways’ digital outreach efforts (website, email, social media), the RRSG + community partner outreach resulted in over **301,000 digital impressions** (excluding the statewide WI Stormwater Week efforts).

The 2026 outreach efforts include continuing an active presence at in-person events to recruit additional Storm Drain Protectors (adopt-a-storm drain program). RRSG also intends to continue offering mini-grants while expanding support and collaboration with key partners (e.g., Rock River Coalition, SaltWise, Statewide Stormwater Consortium) to implement additional stormwater-related projects and outreach in RRSG member communities.

Total Digital Outreach Summary Statistics – Year Over Year

Combined Digital Outreach	2025	2024	2023	2022	2021
Total Impressions	301,000+	297,000+	289,000+	277,900+	228,733+

**Note: Combined impressions include RRSG metrics + data provided by community partners related to social media posts, email messages, etc. (e.g., chambers of commerce, partner alliances, and others)*

Website & Storm Drain Protector Summary Statistics – Year Over Year

Website Metrics	2025	2024	2023	2022	2021
Total Views (RRSG site)	13,354	33,776	17,996	8,412	8,010
Total Views (Stormwater Week site)	5,432	5,735	7,139	n/a	n/a
Storm Drain Protector Program	220	295	125	218	241

**Note: 2023 and 2024 included paid ads that drove additional website visits.*

Event Summary Statistics – Year Over Year

Event Metrics	2025	2024	2023	2022	2021*
Total Events	37	38	37	32	20
Total Event Reach/Impressions	2,014+	1,529+	1,441+	1,697+	2,360+

Clean-Up Summary Statistics – Year Over Year

Clean-Up Metrics	2025	2024	2023	2022	2021	2020*	2019	2018
Total Volunteers	213	295	257	201	187	-	196	130
Total Trash Collected	161	194	166+ bags + other items	80+ bags + other items	200+ bags + other items	-	151+ bags + other items	37+ bags + other items

**Note: The 2020 clean-up was canceled due to COVID. Other items include tires, large pieces of metal, or other debris that is too large or heavy to fit inside a trash bag.*

Facebook Summary Statistics – Year Over Year

	2025	2024	2023	2022	2021
Posts	243	267	235	104	119
Followers	1,392	1,139	1,083	1,007	897
Page Reach (# unique accounts reached)	25,099	28,122	26,535	3,201	n/a
Facebook Page Visits (# of times profile page visited)	3,364	4,200	2,258	652	n/a
Content Views (# of times played or displayed)	51,022	n/a	n/a	n/a	n/a
Content Viewers (# of unique accounts who viewed once+)	13,292	n/a	n/a	n/a	n/a
Interaction Rate	2.16%	n/a	n/a	n/a	n/a

**Note: FB added new “viewer” metrics for content published after July 31, 2025. RivalIQ.com reported a benchmark average across industries of a 0.046% FB engagement rate compared to our 2.16% interaction rate.*

Instagram Summary Statistics – Year Over Year

	2025	2024	2023	2022	2021
Posts	292	213	189	129	89
Followers	1,461	1,201	1,167	1,078	1,019
Instagram Reach (# unique accounts reached)	24,288	8,335	4,806	1,890	n/a
Instagram Profile Visits (# of times profile page visited)	1,179	704	1,136	1,064	n/a
Instagram Views (# of times played or displayed)	62,149	n/a	n/a	n/a	n/a
Interaction Rate	2.97%	n/a	n/a	n/a	n/a

**Note: RivalIQ.com reported a benchmark average across industries of a 0.36% IG engagement rate compared to our 2.97% interaction rate.*

2025 Year-in-Review

Introduction

The following document provides an overview of the Rock River Stormwater Group’s (RRSG) public education and outreach activities (branded as Protect Wisconsin Waterways) as part of regional stormwater public education and outreach during the 2025 calendar year.

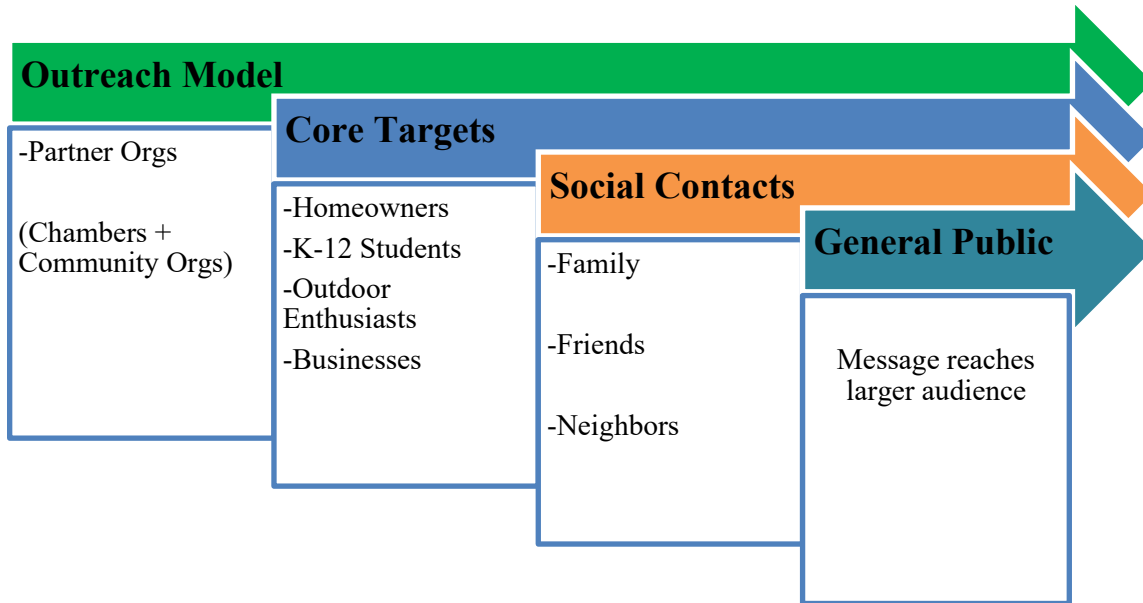
The Protect Wisconsin Waterways regional stormwater public education efforts had a presence at **27 in-person events** in 2025 (not including clean ups). Community-based events included farmers’ markets, festivals near downtown areas or along waterways, and other events organized by community partners. Protect Wisconsin Waterways also sponsored nine waterway clean-up events on September 20th, 2025 (**10 total waterway clean-ups**) that engaged **213 volunteers**. Each event contributed to active education efforts for the general public. We also funded **one mini-grant program** in Waupun that resulted in the installation of a permanent sculpture “Eddies and Etchings.” The RRSG also contributed to the strategic planning and implementation of the **2025 statewide Wisconsin Stormwater Week**. This includes funding and support for the Stormwater Week website and securing the Governor’s Proclamation.

On a digital front, RRSG maintained partnership efforts through various community-based organizations. Stormwater-focused communications shared through these organizations, plus Protect Wisconsin Waterways’ social media efforts, resulted in over **301,000+ digital impressions**. In combination, the efforts helped the Protect Wisconsin Waterways brand increase the reach of public education efforts compared to the 2024 efforts.



Target Audiences & Outreach Communication Model

Protect Wisconsin Waterways’ outreach and communication plan in 2025 focused on leveraging partnerships with community-based organizations to help engage our core targets, their social contacts, and, by extension, the general public.



Initiative #1: Digital Brand Awareness & Community Partnerships

Community Partners: Expanding on existing brand awareness through establishing and maintaining community partnerships is a crucial strategy for Protect Wisconsin Waterways to enhance its outreach and impact. By building strong relationships with local organizations, environmental groups, schools, and businesses, PWW can strengthen its presence within communities across Wisconsin. These partnerships help raise awareness about the importance of preserving water resources and foster collaborative efforts in environmental education, advocacy, and clean-up initiatives. Maintaining these partnerships will ensure that PWW remains a trusted and visible leader in waterway protection, amplifying its message and mobilizing more individuals to take action to safeguard Wisconsin's precious waterways for future generations.



In addition to maintaining ongoing sponsorships/partnerships with WI SaltWise and the Rock River Coalition, RRSg digital outreach efforts occurred via chambers, community organizations, and other local groups. Their outreach through social media is a crucial aspect of this advocacy, enabling them to reach a broader and more diverse audience. Using platforms like Facebook and Instagram, RRSg shares educational content, updates on local water quality issues, and details of upcoming events or clean-up initiatives. Social media also facilitates real-time communication, allowing RRSg to engage directly with community members, answer questions, and encourage participation in waterway protection efforts. Moreover, it helps create a sense of community, rallying support from individuals who might not otherwise have been involved. By maintaining an active and engaging presence online, RRSg amplifies its impact and fosters a network of informed citizens who are committed to protecting and preserving local water resources.




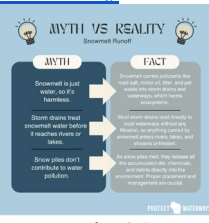




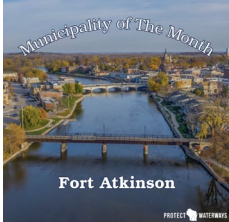
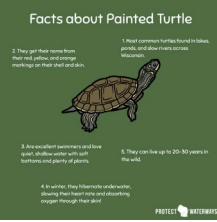




Social Media: In 2025, we continued our Protect Wisconsin Waterways social media campaign on Facebook and Instagram. Content highlighted MS4-related topics, Myth/Facts, “Featured Municipality of the Month” to showcase the waterways within the cities/townships in the Rock River watershed, and other content series. Throughout 2025, the Facebook and Instagram pages gained a total of 513 new followers. Social media efforts on the Protect Wisconsin Waterways’ Facebook and Instagram pages included over **113,171 impressions** across a total of **535 posts**.

Monthly Content Calendar

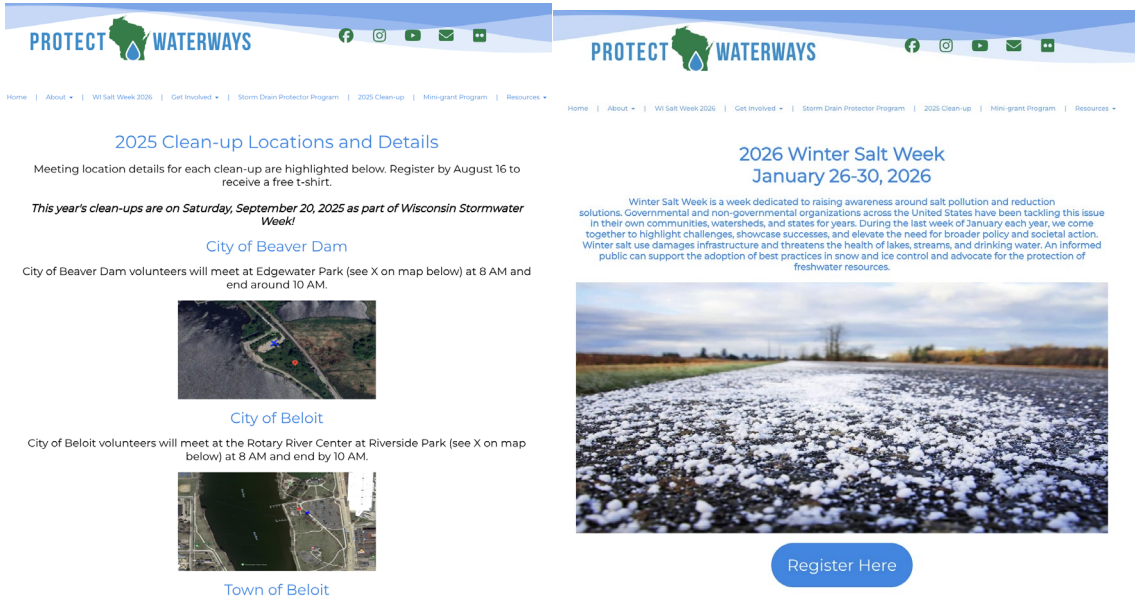
Month	Theme	Municipality
January	Pollution Prevention Education	Town of Beloit
February	Snow Melt Runoff	Whitewater
March	Construction Site and Post-Construction Stormwater Management	Jefferson
April	Green Infrastructure & Low Impact Development	Beaver Dam
May	Fertilizer Pesticide Application	Monroe
June	Residential Infiltration	City of Beloit
July	Yard and pet waste management	Fort Atkinson
August	Household and Hazardous Waste Disposal	Townships of Janesville, Rock, Turtle, Harmony
September	Vehicle Washing	Janesville
October	Stream and Shoreline Management	Watertown
November	Illicit Discharge Detection and Elimination	Waupun
December	Salt Use	Milton

Example social media posts related to RRSB activities.

Top post from every month in 2025

<p>January</p>  <p>Outreach: 366 accounts</p>	<p>February</p>  <p>Outreach: 317 accounts</p>	<p>March</p>  <p>Outreach: 859 accounts</p>
<p>April</p>  <p>Outreach: 1,377 accounts</p>	<p>May</p>  <p>Outreach: 269 accounts</p>	<p>June</p>  <p>Outreach: 808 accounts</p>
<p>July</p>  <p>Outreach: 2,690 accounts</p>	<p>August</p>  <p>Outreach: 374 accounts</p>	<p>September</p>  <p>Outreach: 2,225 accounts</p>
<p>October</p>  <p>Outreach: 544 accounts</p>	<p>November</p>  <p>Outreach: 1,751 accounts</p>	<p>December</p>  <p>Outreach: 2,923 accounts</p>

Website: Overall, the website reached 13,354. While down from the previous year, we didn't pay for digital ads in 2025 that had led to an increase in 2024.




2025 Clean-up Locations and Details

Meeting location details for each clean-up are highlighted below. Register by August 16 to receive a free t-shirt.

This year's clean-ups are on Saturday, September 20, 2025 as part of Wisconsin Stormwater Week!


City of Beaver Dam

City of Beaver Dam volunteers will meet at Edgewater Park (see X on map below) at 8 AM and end around 10 AM.




City of Beloit

City of Beloit volunteers will meet at the Rotary River Center at Riverside Park (see X on map below) at 8 AM and end by 10 AM.



2026 Winter Salt Week
January 26-30, 2026

Winter Salt Week is a week dedicated to raising awareness around salt pollution and reduction solutions. Governmental and non-governmental organizations across the United States have been tackling this issue in their own communities, watersheds, and states for years. During the last week of January each year, we come together to highlight challenges, showcase successes, and elevate the need for broader policy and societal action. Winter salt use damages infrastructure and threatens the health of lakes, streams, and drinking water. An informed public can support the adoption of best practices in snow and ice control and advocate for the protection of freshwater resources.



[Register Here](#)

Initiative #2: Outreach & Engagement via Community Events

Storm Drain Protector Program:

The Storm Drain Protector Program was initially launched in 2019 to engage homeowners with storm drains adjacent to their property. The program aims to raise awareness among municipal residents of the nature of stormwater and what they should look for to preserve water quality. We ask homeowners to sign up to become “storm drain protectors” and pledge to keep their storm drains clear of leaves, grass clippings, and other debris. The 2025 efforts included online sign-ups at our tabling events and social media content.

2025 Storm Drain Protector Sign-ups: 201 sign ups

Community Events:

Volunteers represented the Protect Wisconsin Waterways brand at **37 total community events** (including the 10 clean-ups). Tabling included distributing brochures and information about different MS4-related topics, highlighting the storm drain protector program and clean-ups, and activities like the Enviroscape and Stormwater Around Your Home demonstrations. In combination, the brand ambassador volunteers **engaged 2,014+ individuals** and the 213 volunteers at the clean-ups across the 2025 events as outlined in the table.

Annual Waterway Clean-Up: RRSB municipalities hosted **ten waterway clean-up events** in parks across the area on Saturday, September 20, 2025, during Wisconsin Stormwater Week. We had **213 volunteers** participate in the 2025 clean-up events.

Community Engagement Event Reports

Event	City	Date	# of People Engaged
Story Time with Splash – Beaver Dam Library	Beaver Dam	4/21/25	12
Beaver Dam Earth Day Event Clean-up	Beaver Dam	4/26/25	50
Janesville Sustainability Fair (Library)	Janesville	4/26/25	81
Jefferson Earth Day Festival	Jefferson	4/26/25	70
Rotary Botanical Garden Earth Day Event	Janesville	4/27/25	60
Earth Day Celebration	Beloit	4/26/25	18
Invitation to Wonder the Waubesa Wetlands	Watertown	4/30/25	13
Story Time with Splash	Whitewater	5/9/25	15
Milton Community Days	Milton	5/9/25	55
Midtown Market	Waupun	6/14/25	10
Janesville Farmers Market	Janesville	6/7/25	35
Beloit Farmers Market	Beloit	6/21/25	30
Story Time with Splash - UWW Children’s Center	Whitewater	6/27/25	20
Watertown Kids Festival	Watertown	7/13/25	25
Beloit Farmers Market	Beloit	7/19/25	12
Janesville Farmers Market	Janesville	8/16/25	105
Whitewater Farmers Market	Whitewater	9/9/25	15
Clean-ups (see table below)	10 locations	9/20/25	213
Milton Days	Milton	10/10/25	20
Whitewater Farmers Market	Whitewater	10/14/25	200+
Downtown Beloit Halloween Parade and Party	Beloit	10/25/25	200+

Watertown Boo Bash	Watertown	10/25/25	500+
Trunk or Treat	Whitewater	10/30/25	100+
Tabling UC at UW-Whitewater	Whitewater	11/4/25	100+
UWW Bookstore Tabling	Whitewater	11/8/25	20
Story Time with Splash - UWW Children's Center	Whitewater	11/18/25	12
Story Time with Splash - UWW Children's Center	Whitewater	11/20/25	12
Story Time with Splash - UWW Children's Center	Whitewater	11/21/25	11
TOTAL			2,014+

2025 Waterway Clean-Ups

City	Time	Location	Volunteers	Trash Bags
Milton	8 am - 10 am	Schiller Park	9	13
Waupun	8 am - 10 am	Shaler Park	49	20
Beaver Dam	8 am - 10 am	Edgewater Park	9	7
Watertown	9 am - 11 am	Riverside Park	25	15
Fort Atkinson	9 am - 12 pm	Bicentennial Park	20	8
Janesville	10 am - 12 pm	Wilson Elementary Lot	27	25
Whitewater	8 am - 10 am	Cravath Lakefront	33	25
Town of Beloit	10 am - 12 pm	Preservation Park	11	22
City of Beloit	8 am - 10 am	Rotary River Center	25	22
City of Monroe	10 - 12 pm	Twinning Park	5	4
TOTAL			213 volunteers	161 trash bags

Initiative #3: RRSB Mini-Grant Program

Mini-Grant 2025:

In 2022, Protect Wisconsin Waterways launched the Mini-Grant program to engage community organizations and help spread awareness of protecting and keeping our local waterways clean. The program encourages community organizations and other eligible groups to apply for project funding of up to \$5,000 related to stormwater public education efforts.

In addition to promoting on the Protect Wisconsin Waterways website and social media, outreach was conducted with community organizations across RRSB communities. In 2025, we funded one new applicant (Create Waupun). The mini grant helped pay for the permanent installation of the “Eddies & Etchings” sculpture placed in Harris Mill Park. The sculpture was created by Minnesota-based sculpture artists James and Ryan Pedersen and represents the curves of the Rock River as it flows through Waupun. A ribbon cutting was held immediately following the Waupun cleanup event held September 20, 2025.

“Eddies and Etchings” 2025



Initiative #4: Municipal Worker & Other Trainings

The RRSg provides all members access to an online library of training resources (i.e., videos and other materials) on different stormwater topics. Each January, we encourage members to provide DPW and other employees with time to complete relevant training. Our continued partnership and sponsorship of Salt Wise also offer additional training opportunities and workshops specific to salt use, equipment calibration, and related topics. We also promoted SaltWise workshops to the business community through our Chamber of Commerce connections. Municipal members also send employees to other training.

Municipal training resources include coverage of the following topics.

- Spill Prevention Control Countermeasure (SPCC)
- Stormwater MS4 ‘Rain Check’
- Stormwater Construction ‘Ground Control’
- Stormwater ‘Storm Watch’ Municipal
- IDDE ‘A Grate Concern’ Employee Training

RRSG hosted municipal staff training programs on erosion control run by Water Resources & Associates in Watertown (9/9/25 – 12 attendees) and Janesville (9/16/25).

The Rock River Coalition conducted volunteer stream monitoring training and other events not reported earlier.

Monitoring Sites

County	Baseline Monitoring Sites	Total Volunteers	Total Nutrient Testing Sites	Total Sites with Thermistors
Dodge County	15	31	4	6
Fond du Lac County	2	4	0	2
Jefferson County	19	29	2	4
Rock County	11	18	4	0
Walworth County	12	8	10	5
Total	60	90	20	17

Monitoring Sites Within 10 Miles of RRSg Member Municipalities

Municipality	Number of Sites	Waterbodies Monitored
Beaver Dam	5	Beaver Dam River, Beaver Creek, Mill Creek, Unnamed Tributary to Lake Sinissippi (WBIC 5031431)
Beloit	2	Spring Brook, Turtle Creek
Fort Atkinson	4	Allen Creek, Bark River, Unnamed Trib to Rock River @ CTH J, Unnamed (809000) north of Rockdale Rd
Janesville	3	Blackhawk Creek, Spring Brook
Jefferson	5	Rock Creek, Johnson Creek
Milton	2	Otter Creek, Saunders Creek
Watertown	3	Silver Creek, Riverside Park Creek
Waupun	2	South and West Branches of the Rock River
Whitewater	4	Bark River, Whitewater Creek, Spring Brook Creek, Bluff Creek

Waupun Open House: Held on October 6th, 2025. Over 100 community and municipal members attended.

Beloit Rain Barrel Workshops: Two workshops were held on May 10th at the City of Beloit Utilities and Engineering Facility. The Rock River Coalition staff led 45-minute water conservation and stormwater management presentations, followed by rain barrel assembly demonstrations. 54 participants attended, with 27 rain barrels distributed.

Waupun Rain Barrel Workshop: Held on April 12, 2025, at The Waupun Community Center, the workshop included similar presentations and hands-on assembly sessions with 38 participants and 23 rain barrels.

Stream Monitoring Training: Held in multiple locations in Rock County. There were 12 participants, 10 of which were new.

Watertown Riverside Park Native Planting Report: Saturday, Sept. 20, a group of 26 volunteers helped improve Watertown's Riverside Park Creek by planting native plugs at several sites along the creek. This was Phase II of the Creekside Restoration Planting. RRC Staff and 2-3 volunteers continued to install plants on Sept. 21, 23, 27, 29, 30, and Oct. 1. More than 2,000 native plants were installed on the banks of Riverside Park Creek to help reduce runoff into the creek, prevent soil erosion and flooding.

Initiative #5: Library Package

Protect Wisconsin Waterways continued a *Splash into Learning Library Package* for the municipality libraries. The program included an informative display and a donation of 11 books to the libraries for residents to check out, bookmarks featuring a community waterway photo, children’s activities, and the opportunity to make “raindrop” pledges to keep our waterways clean. Libraries also had the opportunity to schedule an appearance by our Splash mascot for a storytime! The program is designed to get kids excited about reading, learn more about stormwater pollution, and discover things they can do around their homes to make a positive impact on Wisconsin’s waterways. List of books provided to each library included:

- Landscaping with Native Plants of Wisconsin
- We are Water Protectors
- The Great Big Water Cycle Adventure
- All the Way To the Ocean
- Hello from Renn Lake
- Saving Tally
- Let’s Build a Rain Garden
- Me and Marvin Gardens
- Two Little Raindrops
- Zoey and Sassafras Merhorses and Bubbles
- Plasticus Maritimus and Invasive Species



2025 Activities & RRSB’s Public Education & Outreach Goals

The following section outlines the relationship between RRSB’s specific activities and accomplishments to the group’s public education and outreach goals.

Goal 1 - Illicit Discharge Detection and Elimination: Promote detection and elimination of illicit discharges and water quality impacts associated with such discharges from municipal separate storm sewer systems.

1. Illicit Discharge was the November monthly theme for social media and the e-newsletter, the Runoff Rundown. Posts were made identifying illicit discharge and what to do if they see it happening.
2. A “Report a Violation” tab was added to the website, allowing users to report illicit discharge violations in 2017. No “violations” were reported via the website in 2025.

Goal 2 - Household Hazardous Waste Disposal/Pet Waste/Management/Vehicle Washing: Inform and educate the public about the proper management of materials that may cause stormwater pollution from sources including automobiles, pet waste, household hazardous waste and household practices.

1. The social media monthly theme in May, June, July, August, and September aligns with the discussion of proper management for automobiles, pet waste, and household practices. Most other monthly themes also included information on how community members could improve their household practices.
2. Our informational brochures for the Storm Drain Protector Program included information on how to protect the waterways from one’s home. We have continued our Be Wise campaigns surrounding this content.
3. Clean-up events were held on September 20th, 2025 (ten locations).

Goal 3 - Yard Waste Management/Pesticide and Fertilizer Application: Promote beneficial onsite reuse of leaves and grass clippings and proper use of lawn and garden fertilizers and pesticides.

1. Our monthly themes in May and July were fertilizer/pesticide application and pet/yard waste such as lawn clippings, waste disposal, and leaves, respectively.
2. “Lawn Wise” and “Yard Wise” digital content were created to share key tips and best practices that help homeowners “Be Wise” and Protect Wisconsin Waterways.
3. A new “Lawn Wise” demonstration model and educational materials were created in collaboration with the Rock River Coalition.

Goal 4 - Stream and Shoreline Management: Promote the management of streambanks and shorelines by riparian landowners to minimize erosion and restore and enhance the ecological value of waterways.

1. Our monthly theme in October was stream and shoreline management where we discussed the best practices to benefit and help stream and shorelines.

Goal 5 - Residential Infiltration: Promote infiltration of residential stormwater runoff from rooftop downspouts, driveways, and sidewalks.

1. The monthly theme in June promoted better systems to allow more infiltration of residential stormwater.
2. The “Yard Wise” or “Lawn Wise” graphics were created to expand on this concept, during the fall months when leaves can be prevalent.
3. A new “Lawn Wise” demonstration model and educational materials were created in collaboration with the Rock River Coalition.

Goal 6 - Construction Sites and Post-Construction Storm Water Management: Inform and educate those responsible for the design, installation, and maintenance of construction site erosion control practices and stormwater management facilities on how to design, install and maintain the practices.

1. Our monthly theme for March was Construction Sites and Post Construction Storm Water Management.
2. By working with municipal representatives, we gathered information about different erosion control practices.

Goal 7 - Pollution Prevention: Identify businesses and activities that may pose a stormwater contamination concern, and educate those specific audiences on methods of stormwater pollution prevention.

1. Pollution Preventions was our theme for January.
2. Educational efforts focused on community members and homeowners discussing various possible pollutants that they can help prevent from entering the waterways.
3. RRSB members had municipal/DPW employees complete online training via our Excal video library, Salt Wise, and Fortin virtual training.

Goal 8 - Green Infrastructure/Low Impact Development: Promote environmentally sensitive land development designs by developers and designers, including green infrastructure and low-impact development.

1. Green Infrastructure/Low Impact Development was April’s monthly theme.
2. By promoting things like rain gardens and educating people on erosion control, we promoted environmentally sensitive land development.
3. Rain barrel workshops were held in collaboration with the Rock River Coalition.

Maureen McBroom

From: Maureen McBroom
Sent: Monday, March 24, 2025 10:37 AM
To: Joe Radocay
Cc: Andrew Beyer
Subject: RE: River gage

Good morning, Joe-

FYI, my contact at DNR provided the following link for info from the gauge south of Milwaukee Street: [Rock River at Watertown, WI - USGS Water Data for the Nation](#)

I know the drawdown last fall impacted some of the continuous monitoring info that is collected along the Rock, but monitoring should be back to normal now for 2025.

Have a great day!
-McB

Maureen McBroom

Stormwater Project Manager
City of Watertown
106 Jones Street
Watertown, WI 53094
Office: (920) 206-4264

From: Joe Radocay <joemichael1952@gmail.com>
Sent: Saturday, March 15, 2025 11:36 AM
To: Maureen McBroom <MMcBroom@watertownwi.gov>
Subject: Re: River gage

Thanks for the update.
Joe Radocay

On Fri, Mar 14, 2025 at 10:53 AM Maureen McBroom <MMcBroom@watertownwi.gov> wrote:

Thanks Joe. I'll confess that I'm not sure what's up with this, but will look into it.

There doesn't appear to be data from this gauge since October - ??

Thanks for the heads up!

-McB

This gage is cooperatively operated by the U.S. Army Corps of Engineers - Rock Island District, the Wisconsin Department of Natural Resources, and the U.S. Geological Survey - Wisconsin District.

For official flow data, please visit the USGS website listed in the Additional Links for this station. The National Weather Service information is also linked in the Additional Links for this station.

2024 Stage (Ft)

Day	JAN	FEB	MAR	APR	MAY	JUN	JULY	AUG	SEP	OCT	NOV	DEC
1	2.06	2.60	2.34	3.16	3.39	3.68	4.32	3.44	1.81	1.79	M	M
2	2.05	2.68	2.32	3.20	3.26	3.83	4.33	3.70	1.78	1.73	M	M
3	2.03	2.68	2.29	3.98	3.20	3.91	4.37	4.02	1.76	1.70	M	M
4	1.99	2.70	2.22	4.20	3.13	4.36	4.43	3.68	1.73	1.58	M	M
5	1.97	2.67	2.32	4.13	3.10	4.70	4.40	3.53	1.69	1.59	M	M
6	1.93	2.60	2.41	3.99	3.05	4.53	4.38	3.58	1.63	1.56	M	M
7	1.94	2.58	2.36	3.93	2.98	4.49	4.39	3.56	1.61	1.53	M	M
8	1.93	2.57	2.40	4.02	3.06	4.56	4.40	3.54	1.62	1.51	M	M
9	1.93	2.63	2.53	4.08	3.04	4.69	4.36	3.49	1.61	1.48	M	M
10	1.94	2.69	2.63	4.16	2.97	4.79	4.38	3.44	1.60	1.45	M	M
11	1.98	2.70	2.61	4.21	2.91	4.84	4.34	3.38	1.56	1.45	M	M
12	2.01	2.72	2.59	4.23	2.87	4.81	4.28	3.31	1.51	1.58	M	M
13	1.63	2.72	2.58	4.25	2.84	4.75	4.26	3.20	1.49	1.59	M	M
14	1.86	2.74	2.56	4.26	2.81	4.66	4.34	3.06	1.46	1.54	M	M
15	2.30	2.81	2.88	4.25	2.80	4.55	4.37	2.94	1.44	1.55	M	M
16	2.51	2.84	2.90	4.21	2.77	4.43	4.32	2.85	1.44	1.52	M	M
17	2.70	2.80	2.84	4.22	2.76	4.36	4.26	2.77	1.37	1.52	M	M
18	2.18	2.79	2.86	4.18	2.72	4.28	4.22	2.60	1.39	1.50	M	M
19	2.11	2.80	2.86	4.14	2.65	4.12	4.19	2.48	1.40	1.58	M	M
20	2.32	2.78	2.90	4.08	2.57	4.00	4.16	2.39	1.41	1.55	M	M
21	2.34	2.75	2.89	4.02	2.47	3.90	4.12	2.29	1.38	1.51	M	M
22	1.93	2.74	2.88	3.97	2.48	4.02	4.10	2.17	1.52	1.48	M	M
23	1.90	2.68	2.86	3.90	2.53	4.45	4.05	2.10	1.78	M	M	M
24	1.87	2.63	2.79	3.83	2.50	4.57	4.00	2.02	1.98	M	M	M
25	1.89	2.56	2.78	3.76	2.86	4.28	3.95	1.95	2.14	M	M	M
26	1.96	2.52	3.08	3.65	3.00	4.17	3.88	1.86	2.28	M	M	M
27	2.10	2.47	3.43	3.60	3.16	4.16	3.80	1.80	2.27	M	M	M
28	2.26	2.41	3.36	3.58	3.37	4.14	3.74	1.76	2.18	M	M	M
29	2.40	2.40	3.24	3.55	3.50	4.36	3.68	1.77	2.02	M	M	M
30	2.48		3.22	3.48	3.59	4.37	3.61	1.84	1.88	M	M	M
31	2.57		3.21		3.61		3.53	1.84		M		M
MIN	1.63	2.40	2.22	3.16	2.47	3.68	3.53	1.76	1.37	1.45		
MAX	2.70	2.84	3.43	4.26	3.61	4.84	4.43	4.02	2.28	1.79		
MEAN	2.10	2.66	2.75	3.94	2.97	4.36	4.16	2.79	1.69	1.56		

Rock River at Watertown, WI
 Gage Zero - 792.58 Ft. NGVD29
 Flood Stage - 5.5 Ft.
 Record High Stage - 7.81 Ft. (08/13/2008)
 River Mile -
 Location of Gage -

Located in the SW 1/4 sec.4, T.8 N., R. 15 E., Jefferson County, on left bank, 700 ft downstream from Milwaukee Street bridge, 1.1 mi

Maureen McBroom

Stormwater Project Manager

City of Watertown

106 Jones Street

Watertown, WI 53094

Office: (920) 206-4264

Section 4, Item D.

From: Joe Radocay <joemichael1952@gmail.com>

Sent: Friday, March 14, 2025 9:54 AM

To: Andrew Beyer <ABeyer@watertownwi.gov>; Ritchie Piltz <RPiltz@watertownwi.gov>; Maureen McBroom <MMcBroom@watertownwi.gov>

Subject: River gage

Hi all

FYI

The river gage by the US Army Corp is not working/reporting for Watertown WI

Rivergages.com

Rock River Basin

Is there a problem?

Check it out.

Good information

Thanks

Joe Radocay

Maureen McBroom

From: WI Land+Water <info-wisconsinlandwater.org@shared1.ccsend.com>
Sent: Monday, November 10, 2025 10:02 AM
To: Maureen McBroom
Subject: [*Maybe SPAM*] Cooler Days, Cleaner Ways



TRAINING SPOTLIGHT

FEATURED TRAINING

Bioengineering Field Demonstration

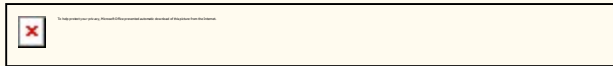
December 16 - Peshtigo, WI

The Marinette County LWCD is working with the WDNR and private landowners to control erosion and restore habitat along nearly 1,600 feet of the Peshtigo River. The grant-funded project will remove concrete rubble from 900 feet of shoreline and repair more than 600 feet of severely eroding shoreline. The entire streambank will be restored using a nature-based solution designed by SmithGroup and LimnoTech.

They will meet in Peshtigo to go over project details, then carpool to the site to view the partially completed project. The contractor (Geomorphic Restoration Inc.) will demonstrate the installation of root wad revetments and discuss other aspects of the project.

[More Information Here](#)

PAST TRAINING



Stormwater Week 2025 Presents: "What is an illicit discharge?"

Recorded September 2025

Speakers Adelle Keppers, City of Superior, and Maureen McBroom, City of Watertown, define an illicit discharge vs. large spill, best practices for dry weather screening at outfalls, and steps WI residents can take when they see an illicit discharge.

View a curated selection of trainings anytime in the on-demand training library.

[Browse On-Demand Trainings](#)



UPCOMING TRAININGS

[Visit our website for training information.](#)

November 10-15

Native American Heritage Webinar Series: Menominee Agriculture from Past to Future - Thursday, November 13 (12:00 PM – 1:00 PM, Virtual, Personal & Professional Skills)

The Flow of Life - The Mysterious Wonder of Carbon - Thursday, November 13 (7:00 PM – 8:00 PM, Virtual, Climate Resiliency)

Fall Conservation Partnership Training - Friday, November 14 (9:00 AM – 3:30 PM, Green Bay, WI, Personal & Professional Skills)

Wetland Coffee Break - Introduction to wetland sedges (Carex) - Friday, November 14 (10:30 AM – 11:30 AM, Virtual, Habitat, Plants, Wildlife)

November 16-23

Clean Lakes 101: Teejop (Four Lakes) Trails & Tales - Tuesday, November 18 (4:00 PM – 5:00 PM, Madison, WI, Habitat, Plants, Wildlife/Personal & Professional Skills)

Navigating Nitrogen: Yields, Optimization, and Water Quality - Wednesday, November 19 (9:30 AM – 12:00 PM, Darlington, WI, Agriculture/Water Resources)

Midwest Vegetable Systems Trial—Utilizing No-Till and Biochar to Improve Sustainability of Organic Vegetable Production - Wednesday, November 19 (1:00 PM – 2:00 PM, Virtual, Agriculture)

Harnessing AI for Invasive Species Detection: Smart Traps, Drones, and Machine Learning in Action - Wednesday, November 19 (1:00 PM – 2:30 PM, Virtual, Computer-based Skills/Habitat, Plants, Wildlife)

Low-cost sensor networks for pluvial flash flood detection and early warning in urban areas - Thursday, November 20 (10:00 AM – 12:00 PM, Virtual, Computer-based Skills/Water Resources)

Native American Heritage Webinar Series: The Menominee Forest - Thursday, November 20 (12:00 PM – 1:00 PM, Virtual, Personal & Professional Skills)

Green Thumb Gardening Fall 2025 - Gardening With Climate Change in Mind - Thursday, November 20 (5:30 PM – 7:30 PM, Virtual, Habitat, Plants, Wildlife/Personal & Professional Skills)

View Calendar

To view more upcoming trainings, visit the Conservation Training Calendar under the events tab.



UPCOMING CONFERENCES

[Visit our website for conference information.](#)

November Conferences

Sustainable Agriculture Summit - November 19-20, Anaheim, CA, Agriculture

December Conferences

2025 Eco-Ag Conference - December 1-4, Madison, WI, Agriculture

Premier Soil Health Event - December 2-3, Mankato, MN, Agriculture/Sediment & Erosion

The Big Soil Health Event - December 8-9, Cedar Falls, IA, Agriculture/Sediment & Erosion

Great Lakes EXPO - December 9-11, Grand Rapids, MI, Water Resources

95th WFU Annual State Convention - December 12-14, Agriculture

Wisconsin Water and Soil Health Conference - December 16-17, Water Resources/Sediment & Erosion



EVENTS CALENDAR

EVENT LOCATION

To help navigate the trainings, use the location filtering function. The areas are based on [Wisconsin Land + Water's eight area associations](#). When using this filter, results will display in-person events occurring within the corresponding area. Virtual events are not included in this filter.

EVENT TOPICS

To help navigate the calendar, use the topic filtering function. Topic categories include:

- *Agriculture*
- *Climate Resiliency*
- *Computer-based Skills*

EVENT TYPE

To help navigate the calendar, use the event type filtering function. Event types include:

- *Trainings*
- *Area Association Meetings*
- *Tours and Field Days*

- *Engineering*
- *Environmental Education*
- *Forestry*
- *Habitat, Plants, Wildlife*
- *Personal & Professional Skills*
- *Sediment & Erosion*
- *Water Resources*
- *Grazing*

- *Conferences*
- *Committee Meetings*
- *Professional Development*
- *Networking*

Partner and Other Training Websites

<u>Conservation Professional Training Program</u>	<u>DATCP</u>	<u>EPA Watershed Academy</u>
<u>Science & Technology Training Library</u>	<u>Wetland Training Institute, Inc</u>	<u>WI School for Beginning Dairy and Livestock Farmers</u>
NRCS <ul style="list-style-type: none"> • <u>On-the-Job Training Modules</u> • <u>Upcoming Trainings and Events</u> • <u>Conservation at Work Video Series</u> 	<u>Wisconsin DNR</u> <ul style="list-style-type: none"> • <u>Surface Water Data Viewer (SMDV)</u> • <u>Water Condition Viewer (WCV)</u> 	Wisconsin Wetlands Assoc. <ul style="list-style-type: none"> • <u>County Fact Sheets</u> • <u>Video Series</u> • <u>Coffee Break</u>

SITCOM Training E-newsletters are usually distributed bi-weekly. If you have a training opportunity to share, please contact Peyton Mueller at peyton@wisconsinlandwater.org. These trainings are provided for your information and do not necessarily reflect the view of WI Land+Water unless provided by our association.



WI Land+Water | 121 S. Pinckney Street, Suite 420 | Madison, WI 53703 US

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Water Systems
800 Hoffmann Drive • P.O. Box 477
Watertown WI 53094-0477
WATER: (920) 262-4075 • WASTEWATER: (920) 262-4085
FAX: (920) 262-4087 • Email: wtnwater@watertownwi.gov

March 31, 2025

Dear Customer:

Due to increasing meter prices along with the manual labor and time that goes into crediting the usage from the sewer deduct meters, the qualifications of the outside water metering program have changed for the 2025 watering season as follows:

1) Grass seed, sod, and landscaping:

- a. New construction houses – eligible for one meter
- b. Existing houses – eligible for one meter if at least 50% of the yard needs to be regraded and replanted with seed or sod. The homeowner will need to provide photo evidence of yard condition.
- c. A refundable deposit of \$100.00 will be required for rental which will be credited back to your account if the meter is found to be in good working condition after testing.
- d. The meter will need to be returned at the end of the season. Any meter not returned by 11/30 each year will result in the loss of the \$100.00 deposit.
- e. An annual, non-refundable rental fee of **\$20.00** will be charged for use.
- f. Credit will be received for the sewer portion only, at a rate of \$3.51 per unit (748 gallons). However, you will be responsible for paying the water charges on the usage through this meter, billed at \$3.80 per unit (748 gallons). Please note that rates are subject to change.
- g. The customer will be billed for any damage resulting from negligence.

2) Pool / hot tub fills:

- a. Not eligible for a meter.
- b. Customers will be allowed ONE credit per year for a pool and/or hot tub fill.
 - i. Read your meter inside before and after filling your pool.
(Please note: if you have a Badger Meter Read-O-Matic register on the outside of your house, these are not attached to your meter and cannot be used for reading your meter).
 - ii. Note the date of the pool fill.
 - iii. Email our office with your address, the date of the pool fill, and the meter readings at wtnwater@watertownwi.gov or call 920-262-4075 within 48 hours of the pool fill.
- c. Credits **will not** be given without meter readings.

Deduct Meter Information

Page -2-

3) Gardens & flower beds:

- a. Not eligible for a meter.
- b. Customers will be eligible for a one-time rebate on a rain barrel purchase through the Stormwater Utility. For more information, please visit our website at https://www.watertownwi.gov/departments/stormwater_utility/Rain_barrel_rebate.php, email MMcBroom@watertownwi.gov, or call 920-206-4264.

4) For all other uses, eligibility will be determined on an individual basis. The water department reserves the right to deny eligibility.

- **See additional meter section of the rate file**

Feel free to contact our office with any questions or concerns you may have.

Sincerely,

Watertown Water Department

RATE FILE

Sheet No. 1 of 1

Public Service Commission of Wisconsin

Schedule No. Am-1

Amendment No. 45

Watertown Water Department

Additional Meter Rental Charge

Upon request, the utility shall furnish and install additional meters to:

- A. Water service customers for the purpose of measuring the volume of water used that is not discharged into the sanitary sewer system; and
- B. Sewerage service customers who are not customers of the water utility for the purpose of determining the volume of sewage that is discharged into the sanitary sewer system.

The utility shall charge a meter installation charge of \$60.00 and a monthly rental fee for the use of this additional meter.

Monthly Additional Meter Rental Charges:

3/8 - inch meter:	\$ 4.50
3/4 - inch meter:	\$ 4.50
1 - inch meter:	\$ 7.00
1 1/4 - inch meter:	\$ 9.50
1 1/2 - inch meter:	\$ 12.00
2 - inch meter:	\$ 17.50

This schedule applies only if the additional meter is installed on the same service lateral as the primary meter and either:

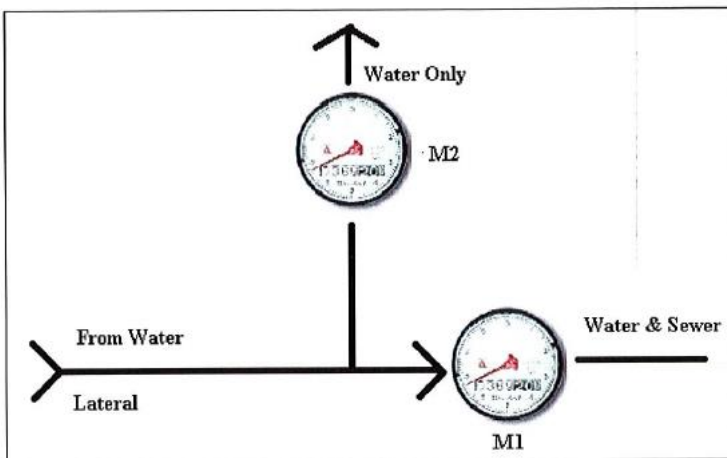
- A. The additional meter is 3/4-inch or smaller if the metering configuration is the Addition Method; or
- B. The additional meter is 2-inch or smaller for all other metering configurations.

If the additional meter is larger than 2-inch or larger than 3/4-inch and installed in the Addition Method, each meter shall be treated as a separate account and Schedule Mg-1 rates shall apply.

Billing: Same as Schedule Mg-1.

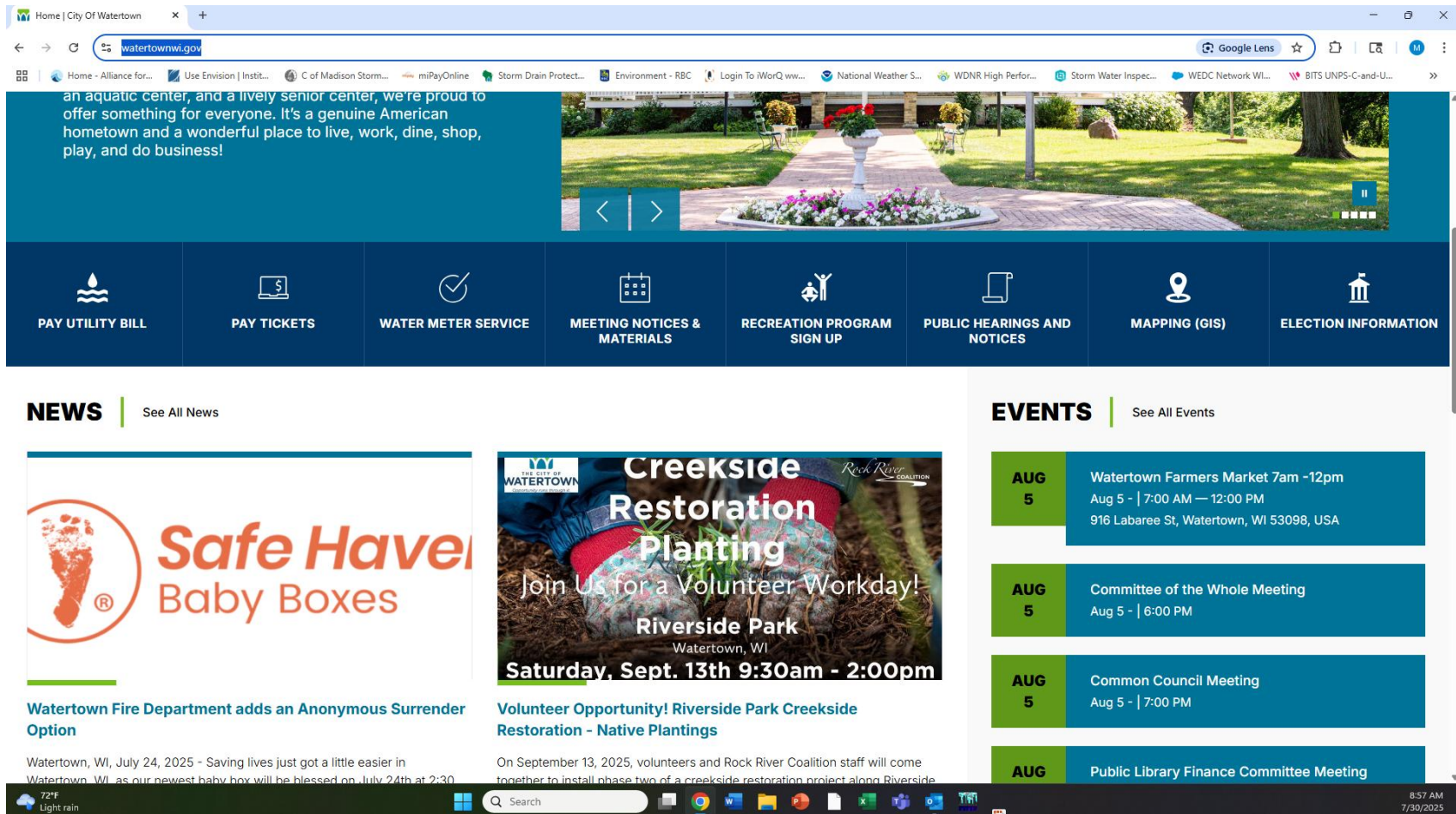
Meter setting to be as shown in figure 3 below.

FIGURE 3 – ADDITION METHOD



Water Billing - No Irrigation Class

General water service is billed according to Schedule Mg-1 based on the M1 meter size and the combined M1 and M2 readings. An additional meter rental charge under Schedule Am-1 may apply, depending on the M2 meter size (see Notes).



City website: 7/30/25

**ORDINANCE TO
AMEND CHAPTER 288, EROSION AND SEDIMENT CONTROL OF THE
CITY OF WATERTOWN GENERAL ORDINANCES**

**SPONSOR: ALDERPERSON BOARD
FROM: PUBLIC WORKS COMMISSION**

WHEREAS, the City of Watertown is required to adopt and implement an ordinance to control construction site runoff and post-construction stormwater management per the Wisconsin Department of Natural Resources (WDNR) Municipal Separate Storm Sewer System (MS4) Permit No. WI-S050075; and,

WHEREAS, the City of Watertown Chapter 288 Erosion and Sediment Control ordinance requires updates to include stormwater management language in the WDNR NR 151, Wisconsin Administrative Code and NR 216, Wisconsin Administrative Code to effectively implement the City of Watertown stormwater management program; and,

WHEREAS, the Public Works Commission reviewed the proposed amendments to Chapter 288 at its February 11, 2025 meeting and recommends adoption of said revised Chapter 288.

THE COMMON COUNCIL OF THE CITY OF WATERTOWN DOES ORDAIN AS FOLLOWS:

SECTION 1. Chapter 288 is hereby amended as follows:

Chapter 288. Erosion and Sediment Control

**Chapter 288
Erosion and Sediment Control**

**ARTICLE I
Erosion Control and Stormwater Runoff**

[Adopted by Ord. No. 08-26 (§ 20.16 of the former City Code); amended in its entirety 4-3-2023 by Ord. No. 23-05]

§ 288-1. Authority.

- A. This article is adopted under the authority granted by § 62.234, Wis. Stats. This article supersedes all provisions of an ordinance previously enacted under § 62.23, Wis. Stats., that relates to construction site erosion control. Except as otherwise specified in § 62.234, Wis. Stats., § 62.23, Wis. Stats., applies to this article and to any amendments to this article.
- B. The provisions of this article are deemed not to limit any other lawful regulatory powers of the same governing body.
- C. The Common Council hereby authorizes the Public Works Director/City Engineer and

(February 17, 2025) Ord. 25-03

its designees to administer and enforce the provisions of this article.

- D. The requirements of this article do not preempt more stringent erosion and sediment control requirements that may be imposed by any of the following:
- (1) Wisconsin Department of Natural Resources administrative rules, permits or approvals including those authorized under §§ 281.16 and 283.33, Wis. Stats.
 - (2) Targeted nonagricultural performance standards promulgated in rules by the Wisconsin Department of Natural Resources under § NR 151.004, Wis. Adm. Code.

§ 288-2. Findings of fact.

The Common Council finds that runoff from land-disturbing construction activity carries a significant amount of sediment and other pollutants to the waters of the state in the City of Watertown.

§ 288-3. Purpose.

It is the purpose of this article to further the maintenance of safe and healthful conditions; prevent and control water pollution; prevent and control soil erosion; protect spawning grounds, fish and aquatic life; control building sites, placement of structures and land uses; preserve ground cover and scenic beauty; and promote sound economic growth, by minimizing the amount of sediment and other pollutants carried by runoff or discharged from land-disturbing construction activity to waters of the state in the City of Watertown.

§ 288-4. Applicability and jurisdiction.

A. Applicability.

- (1) This article applies to the following land-disturbing construction activities except as provided under Subsection A(2):
 - (a) A construction site, which has 3,000 or more square feet of land-disturbing construction activity.
- (2) This article does not apply to the following:
 - (a) Land-disturbing construction activity that includes the construction of a one- or two- family residential site less than one acre and is otherwise regulated by the Wisconsin Department of Safety and Professional Services.
 - (b) A construction project that is exempted by federal statutes or regulations from the requirement to have a national pollutant discharge elimination system permit issued under Chapter 40, Code of Federal Regulations, Part 122, for land-disturbing construction activity.
 - (c) Nonpoint discharges from agricultural facilities and practices.

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- (d) Nonpoint discharges from silviculture activities.
 - (e) Construction projects that do not result in land-disturbing activity including mill and crush operations that do not have soil disturbance, filling or road shoulder grading.
 - (f) Routine maintenance for project sites under five acres of land disturbance if performed to maintain the original line and grade, hydraulic capacity or original purpose of the facility.
- (3) Notwithstanding the applicability requirements in Subsection A(1), this article applies to construction sites of any size that, in the opinion of the City, are likely to result in runoff that exceeds the safe capacity of the existing drainage facilities or receiving body of water, that causes undue channel erosion, that increases water pollution by scouring or the transportation of particulate matter or that endangers property or public safety.
- B. Jurisdiction. This article applies to land-disturbing construction activity on construction sites located within the boundaries and jurisdiction of the City of Watertown.
- C. Exclusions. This article is not applicable to activities conducted by a state agency, as defined under

§ 227.01(1), Wis. Stats., but also including the office of District Attorney, which is subject to the state plan promulgated or a memorandum of the understanding entered into under § 281.33(2), Wis. Stats.

§ 288-5. Definitions.

As used in this article, the following terms shall have the meanings indicated:

ADMINISTERING AUTHORITY — A governmental employee or his/her designee that is designated by the City of Watertown to administer this article.

AGRICULTURAL FACILITIES AND PRACTICES — Has the meaning in § 281.16(1), Wis. Stats.

AVERAGE ANNUAL RAINFALL — A typical calendar year of precipitation as determined by the Wisconsin Department of Natural Resources for users of models such as WlnSLAMM, P8 or equivalent methodology. The average annual rainfall is chosen from a department publication for the location closest to the City.

BEST MANAGEMENT PRACTICE or BMP — Structural or nonstructural measures, practices, techniques or devices employed to avoid or minimize soil, sediment or pollutants carried in runoff to waters of the state.

BUSINESS DAY — A day the City Hall is routinely and customarily open for business.

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CEASE AND DESIST ORDER — A court-issued order to halt land-disturbing construction activity that is being conducted without the required permit.

CONSTRUCTION SITE — An area upon which one or more land-disturbing construction activities occur, including areas that are part of a larger common plan of development or sale where multiple separate and distinct land-disturbing construction activities may be taking place at different times on different schedules but under one plan.

DESIGN STORM — A hypothetical discrete rainstorm characterized by a specific duration, temporal distribution, rainfall intensity, return frequency and total depth of rainfall.

DIVISION OF LAND — The creation from one parcel of four or fewer parcels or building sites of one or fewer acres each in area where such creation occurs at one time or through the successive partition within a five-year period.

EROSION — The process by which the land's surface is worn away by the action of wind, water, ice or gravity.

EROSION AND SEDIMENT CONTROL PLAN — A comprehensive plan developed to address pollution caused by erosion and sedimentation of soil particles or rock fragments during construction.

EXTRATERRITORIAL — The unincorporated area within three miles of the corporate limits of a first, second, or third class city, or within 1.5 miles of a fourth class city or village.

FINAL STABILIZATION — That all land-disturbing construction activities at the construction site have completed and that a uniform perennial vegetative cover has been established, with a density of at least 70% of the cover, for the unpaved areas and areas not covered by permanent structures, or that employ equivalent permanent stabilization measures.

GOVERNING BODY — The City Public Works Commission or the City Council.

LAND-DISTURBING CONSTRUCTION ACTIVITY — Any man-made alteration of the land surface resulting in a change in the topography or existing vegetative or nonvegetative soil cover, that may result in runoff and lead to an increase in soil erosion and movement of sediment into waters of the state. Land-disturbing construction

(February 17, 2025) Ord. 25-03

activity includes clearing and grubbing, demolition, excavating, pit trench dewatering, filling and grading activities.

MEP or MAXIMUM EXTENT PRACTICABLE — The highest level of performance that is achievable but is not equivalent to a performance standard identified in this article as determined in accordance with § 288-6 of this article.

PERFORMANCE STANDARD — A narrative or measurable number specifying the minimum acceptable outcome for a facility or practice.

PERMIT — A written authorization made by the City of Watertown to the applicant to conduct land-disturbing construction activity or to discharge post-construction runoff to waters of the state.

POLLUTANT — Has the meaning given in § 283.01(13), Wis. Stats.

POLLUTION — Has the meaning given in § 281.01(10), Wis. Stats.

PUBLIC WORKS DIRECTOR/CITY ENGINEER — The individual holding the Public Works Director/ City Engineer title or his/her designees within the City of Watertown.

RESPONSIBLE PARTY — The landowner or any other entity performing services to meet the requirements of this article through a contract or other agreement.

RUNOFF — Stormwater or precipitation including rain, snow or ice melt or similar water that moves on the land surface via sheet or channelized flow.

SEDIMENT — Settleable solid material that is transported by runoff, suspended within runoff or deposited by runoff away from its original location.

SEPARATE STORM SEWER — A conveyance or system of conveyances including roads with drainage systems, streets, catch basins, curbs, gutters, ditches, constructed channels or storm drains, which meets all of the following criteria:

- A. Is designed or used for collecting water or conveying runoff.
- B. Is not part of a combined sewer system.
- C. Is not draining to a stormwater treatment device or system.

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D. Discharges directly or indirectly to waters of the state.

SILVICULTURE ACTIVITY — Activities including tree nursery operations, tree harvesting operations, reforestation, tree thinning, prescribed burning, and pest and fire control. Clearing and grubbing of an area of a construction site is not a silviculture activity.

SITE — The entire area included in the legal description of the land on which the land-disturbing construction activity is proposed in the permit application.

STOP-WORK ORDER — An order issued by the City which requires that all construction activity on the site be stopped.

TECHNICAL STANDARD — A document that specifies design, predicted performance and operation and maintenance specifications for a material, device or method.

WATERS OF THE STATE — Includes those portions of Lake Michigan and Lake Superior within the boundaries of this state, and all lakes, bays, rivers, streams, springs, ponds, wells, impounding reservoirs, marshes, watercourses, drainage systems and other surface water or groundwater, natural or artificial, public or private, within this state or its jurisdiction.

§ 288-6. Applicability of maximum extent practicable.

Maximum extent practicable applies when a person who is subject to a performance standard of this article demonstrates to the City's satisfaction that a performance standard is not achievable and that a lower level of performance is appropriate. In making the assertion that a performance standard is not achievable and that a level of performance different from the performance standard is the maximum extent practicable, the responsible party shall take into account the best available technology, cost effectiveness, geographic features, and other competing interests such as protection of public safety and welfare, protection of endangered and threatened resources, and preservation of historic properties.

§ 288-7. Technical standards.

- A. Design criteria, standards and specifications. All BMPs required to comply with this article shall meet the design criteria, standards and specifications based on any of the following:
- (1) Design guidance and technical standards identified or developed by the Wisconsin Department of Natural Resources under Subchapter V of Chapter NR 151, Wis. Adm. Code.
 - (2) Soil loss prediction tools [such as the Universal Soil Loss Equation (USLE)] when using an appropriate rainfall or runoff factor (also referred to as the R

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factor) or an appropriate design storm and precipitation distribution, and when considering the geographic location of the site and the period of disturbance.

- B. Other standards. Other technical standards not identified or developed in Subsection A may be used provided that the methods have been approved by the City.

§ 288-8. Performance standards.

- A. Responsible party. The responsible party shall implement an erosion and sediment control plan, developed in accordance with § 288-10 that incorporates the requirements of this section.
- B. Plan. A written plan shall be developed in accordance with § 288-10 and implemented for each construction site. Simplified plans may be completed for sites with less than one acre of land-disturbing construction activity in accordance with the requirements of this article.
- C. Erosion and other pollutant control requirements. The plan required under Subsection B shall include the following:
- (1) Erosion and sediment control practices. Erosion and sediment control practices shall be used at each site where more than 3,000 square feet of land-disturbing construction activity is to occur, and shall be used to prevent or reduce all of the following:
 - (a) The deposition of soil from being tracked onto streets by vehicles.
 - (b) The discharge of sediment from disturbed areas into on-site stormwater inlets.
 - (c) The discharge of sediment from disturbed areas into adjacent waters of the state.
 - (d) The discharge of sediment from drainageways that flow off the site.
 - (e) The discharge of sediment by dewatering activities.
 - (f) The discharge of sediment eroding from soil stockpiles existing for more than seven days.
 - (g) The discharge of sediment from erosive flows at outlets and in downstream channels.
 - (h) The transport by runoff into waters of the state of chemicals, cement, and other building compounds and materials on the construction site during the construction period. However, projects that require the placement of these materials in waters of the state, such as constructing bridge footings or BMP installations, are not prohibited by this subsection.
 - (i) The transport by runoff into waters of the state of untreated wash water from vehicle and wheel washing.
 - (2) Sediment performance standards. In addition to the erosion and sediment control practices under Subsection C(1), the following erosion and sediment control

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practices shall be employed for all construction sites with more than one acre of land-disturbing construction activity:

- (a) BMPs that, by design, discharge no more than five tons per acre per year, or to the maximum extent practicable, of the sediment load carried in runoff from initial grading to final stabilization.
 - (b) No person shall be required to employ more BMPs than are needed to meet a performance standard in order to comply with maximum extent practicable. Erosion and sediment control BMPs may be combined to meet the requirements of this subsection. Credit may be given toward meeting the sediment performance standard of this subsection for limiting the duration or area, or both, of land-disturbing construction activity, or for other appropriate mechanisms.
 - (c) Notwithstanding Subsection C(2)(a), if BMPs cannot be designed and implemented to meet the sediment performance standard, the erosion and sediment control plan shall include a written, site-specific explanation of why the sediment performance standard cannot be met and how the sediment load will be reduced to the maximum extent practicable.
- (3) Preventive measures. The erosion and sediment control plan shall incorporate all of the following:
- (a) Maintenance of existing vegetation, especially adjacent to surface waters whenever possible.
 - (b) Minimization of soil compaction and preservation of topsoil.
 - (c) Minimization of land-disturbing construction activity on slopes of 20% or more.
 - (d) Development of spill prevention and response procedures.
- D. Location. The BMPs used to comply with this section shall be located prior to runoff entering waters of the state.
- E. Implementation. The BMPs used to comply with this section shall be implemented as follows:
- (1) Erosion and sediment control practices shall be constructed or installed before land-disturbing construction activities begin in accordance with the erosion and sediment control plan developed in § 288-10.
 - (2) Erosion and sediment control practices shall be maintained until final stabilization.
 - (3) Final stabilization activity shall commence when land-disturbing activities cease and final grade has been reached on any portion of the site.
 - (4) Temporary stabilization activity shall commence when land-disturbing activities have temporarily ceased and will not resume for a period exceeding 14 calendar days.

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(5) BMPs that are no longer necessary for erosion and sediment control shall be removed by the responsible party.

F. Alternate requirements. The City may establish stormwater management requirements more stringent than those set forth in this section if the City determines that an added level of protection is needed for sensitive resources.

§ 288-9. Permitting requirements, procedures and fees.

A. Permit required. No responsible party may commence a land-disturbing construction activity subject to this article without receiving prior approval of an erosion and sediment control plan for the site and a permit from the City.

B. Permit application and fees. At least one responsible party desiring to undertake a land-disturbing construction activity subject to this article shall submit an application for a permit and an erosion and sediment control plan that meets the requirements of § 288-10 and shall pay an application fee to the City of Watertown. By submitting an application, the applicant is authorizing the City of Watertown to enter the site to obtain information required for the review of the erosion and sediment control plan.

C. Review and approval of permit application. The City shall review any permit application that is submitted with an erosion and sediment control plan, and the required fee. The following approval procedure shall be used:

(1) Within 20 business days of the receipt of a complete permit application, as required by Subsection B, the City shall inform the applicant whether the application and plan are approved or disapproved based on the requirements of this article.

(2) If the permit application and plan are approved, the City shall issue the permit.

(3) If the permit application or plan is disapproved, the City shall state in writing the reasons for disapproval.

(4) The City may request additional information from the applicant. If additional information is submitted, the City shall have 15 business days from the date the additional information is received to inform the applicant that the plan is either approved or disapproved.

D. Financial guarantee. As a condition of approval and issuance of the permit, the City may require the applicant to deposit a surety bond, irrevocable letter of credit or other financial guarantee to guarantee a good faith execution of the approved erosion control plan and any permit conditions. The financial guarantee shall be an amount up to 120% of the estimated cost of the improvements.

E. Permit requirements. All permits shall require the responsible party to:

(1) Notify the City within 48 hours of commencing any land-disturbing construction activity.

(2) Notify the City of completion of any BMPs within three days after their installation.

(3) Obtain permission in writing from the City prior to any modification pursuant to

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§ 288-10C of the erosion and sediment control plan.

- (4) Install all BMPs as identified in the approved erosion and sediment control plan.
 - (5) Maintain all road drainage systems, stormwater drainage systems, BMPs and other facilities identified in the erosion and sediment control plan.
 - (6) Repair any siltation or erosion damage to adjoining surfaces and drainageways resulting from land-disturbing construction activities and document repairs in a site erosion control log.
 - (7) Inspect the BMPs within 24 hours after each rain of 0.5 inch or more which results in runoff during active construction periods, and at least once each week. Document the findings of the inspections in a site erosion control log with the date of inspection, the name of the person conducting the inspection, and a description of the present phase of the construction at the site. Repair or replace erosion and sediment control best management practices as necessary within 24 hours of an inspection or by the date agreed to between the permittee and the Public Works Director/City Engineer or the appropriate designee. Inspections are only required for construction sites with more than one acre of land-disturbing construction activity.
 - (8) Allow the City to enter the site for the purpose of inspecting compliance with the erosion and sediment control plan or for performing any work necessary to bring the site into compliance with the control plan. Keep a copy of the erosion and sediment control plan at the construction site.
 - (9) Keep a copy of the inspection reports on the site at all times.
- F. Permit conditions. Permits issued under this section may include conditions established by the City in addition to the requirements set forth in Subsection E, where needed to assure compliance with the performance standards in § 288-8.
- G. Permit duration. Permits issued under this section shall be valid for a period of three years from the date of issuance. The City may extend the period once for up to an additional three years. The City may require additional BMPs as a condition of the extension if they are necessary to meet the requirements of this article.
- H. Maintenance. The responsible party throughout the duration of the construction activities shall maintain all BMPs necessary to meet the requirements of this article until the site has undergone final stabilization.

§ 288-10. Erosion and sediment control plan, statement, and amendments.

- A. Erosion and sediment control plan.
- (1) An erosion and sediment control plan shall be prepared and submitted to the City.
 - (2) The erosion and sediment control plan shall be designed to meet the performance standards in § 288-8 and other requirements of this article. Simplified plans may be completed for sites with less than one acre of land-disturbing construction activity.

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- (3) The erosion and sediment control plan shall address pollution caused by soil erosion and sedimentation during construction and up to final stabilization of the site. The erosion and sediment control plan shall include, at a minimum, the following items:
- (a) The name(s) and address(es) of the owner or developer of the site, and of any consulting firm retained by the applicant, together with the name of the applicant's principal contact at such firm. The application shall also include start and end dates for construction.
 - (b) Description of the site and the nature of the construction activity, including representation of the limits of land disturbance on a United States Geological Survey 7.5-minute series topographic map.
 - (c) A sequence of construction of the development site, including stripping and clearing; rough grading; construction of utilities, infrastructure, and buildings; and final grading and landscaping. Sequencing shall identify the expected date on which clearing will begin, the estimated duration of exposure of cleared areas, areas of clearing, installation of temporary erosion and sediment control measures, and establishment of permanent vegetation.
 - (d) Estimates of the total area of the site and the total area of the site that is expected to be disturbed by construction activities.
 - (e) Estimates, including calculations, if any, of the runoff coefficient of the site before and after construction activities are completed.
 - (f) Calculations to show the expected percent reduction in the average annual sediment load carried in runoff as compared to no sediment or erosion controls.
 - (g) Existing data describing the surface soil as well as subsoils.
 - (h) Depth to groundwater, as indicated by on-site soil borings or Natural Resources Conservation Service soil information where available.
 - (i) Name of the immediate named receiving water from the United States Geological Survey 7.5-minute series topographic maps.
- (4) The erosion and sediment control plan shall include a site map. The site map shall include the following items and shall be at a scale not greater than 100 feet per inch and at a contour interval not to exceed five feet.
- (a) Existing topography, vegetative cover, natural and engineered drainage systems, roads and surface waters. Lakes, streams, wetlands, channels, ditches and other watercourses on and immediately adjacent to the site shall be shown. Any identified 100-year floodplains, flood fringes and floodways shall also be shown.
 - (b) Boundaries of the construction site.
 - (c) Drainage patterns and approximate slopes anticipated after major grading

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activities.

- (d) Areas of soil disturbance.
 - (e) Location of major structural and nonstructural controls identified in the plan.
 - (f) Location of areas where stabilization practices will be employed.
 - (g) Areas which will be vegetated following construction.
 - (h) Area and location of wetland acreage on the site and locations where stormwater is discharged to a surface water or wetland within one-quarter mile downstream of the construction site.
 - (i) Locations of all surface waters and wetlands within one mile of the construction site.
 - (j) Areas used for infiltration of post-construction stormwater runoff.
 - (k) An alphanumeric or equivalent grid overlying the entire construction site map.
- (5) Each erosion and sediment control plan shall include a description of appropriate erosion and sediment control best management practices that will be installed and maintained at the site to prevent pollutants from reaching waters of the state. The plan shall clearly describe the appropriate erosion and sediment control measures for each major land-disturbing construction activity and the timing during the construction process that the measures will be implemented. The description of erosion and sediment controls shall include, when appropriate, the following minimum requirements:
- (a) Description of interim and permanent stabilization practices, including an implementation schedule. Site plans shall ensure that existing vegetation is preserved where attainable and that disturbed portions of the site are stabilized.
 - (b) Description of structural practices to divert flow away from exposed soils, store flows or otherwise limit runoff and the discharge of pollutants from the site. Unless otherwise specifically approved in writing by the City of Watertown, structural measures shall be installed on upland soils.
 - (c) Management of overland flow at all sites, unless otherwise controlled by outfall controls.
 - (d) Trapping of sediment in channelized flow.
 - (e) Staging construction to limit exposed soil areas subject to erosion.
 - (f) Protection of downslope drainage inlets where they occur.
 - (g) Minimization of tracking via installation of tracking pads at all vehicle and equipment entry and exit locations of the construction site.
 - (h) Clean up of off-site sediment deposits.

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- (i) Proper disposal of building and waste materials at all sites.
 - (j) Stabilization of drainageways.
 - (k) Control of soil erosion from dirt stockpiles.
 - (l) Installation of permanent stabilization practices as soon as possible after final grading.
 - (m) Minimization of dust to the maximum extent practicable.
- (6) The erosion and sediment control plan shall require that velocity dissipation devices be placed at discharge locations and along the length of any outfall channel, as necessary, to provide a nonerosive flow from the structure to a watercourse so that the natural physical and biological characteristics and functions are maintained and protected.
- B. Erosion and sediment control plan statement. For each construction site identified under § 288-4A with more than one acre of land-disturbing construction activity, an erosion and sediment control plan statement shall be prepared. This statement shall be submitted to the City. The control plan statement shall briefly describe the site, including a site map. Further, it shall also include the best management practices that will be used to meet the requirements of the article, including the site development schedule.
- C. Amendments. The applicant shall amend the plan if any of the following occur:
- (1) There is a change in design, construction, operation or maintenance at the site which has the reasonable potential for the discharge of pollutants to waters of the state and which has not otherwise been addressed in the plan.
 - (2) The actions required by the plan fail to reduce the impacts of pollutants carried by construction site runoff.
 - (3) The City notifies the applicant of changes needed in the plan.

§ 288-11. Fee schedule.

The fees referred to in other sections of this article shall be established by the Common Council and may from time to time be modified by resolution. A schedule of the fees established by the Common Council shall be available for review in City Hall. The fee shall cover all City and consultant costs to review the permit application and perform the required site inspections.

§ 288-11.1. Inspection.

If land-disturbing construction activities are being carried out without a permit required by this article, the City may enter the land pursuant to the provisions of § 66.0119(1), (2) and (3), Wis. Stats. The City will inspect any construction site with more than ~~one acre~~ **three thousand square** feet of land-disturbing construction activity that holds a permit under this chapter **or where construction work began prior to obtaining a permit** as required by the current Wisconsin Department of Natural Resources Municipal Separate

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Storm Sewer System (MS4) Permit or within the first two weeks of construction, at least once a month, and again at the end of construction to ensure compliance with the approved sediment and erosion control plan. If erosion and/or sediment control best management practices (BMPs) are out of compliance during inspections, the City may conduct follow-up inspections within seven days, unless corrections are made and observed by the inspector or verified via photographs submitted to the inspector. The costs of these inspections shall be billed to the responsible party.

§ 288-11.2. Enforcement.

- A. The City may post a stop-work order if any of the following occurs:
 - (1) Any land-disturbing construction activity regulated under this article is being undertaken without a permit.
 - (2) The erosion and sediment control plan is not being implemented in a good faith manner.
 - (3) The conditions of the permit are not being met.
- B. If the responsible party does not cease activity as required in a stop-work order posted under this section or fails to comply with the erosion and sediment control plan or permit conditions, the City may revoke the permit.
- C. If the responsible party, where no permit has been issued, does not cease the activity after being notified by the City, or if a responsible party violates a stop-work order posted under Subsection A, the City may issue a citation and/or request the City Attorney to obtain a cease and desist order in any court with jurisdiction.
- D. The City may retract the stop-work order issued under Subsection A or the permit revocation under Subsection B.
- E. After posting a stop-work order under Subsection A, the City may issue a notice of intent to the responsible party of its intent to perform work necessary to comply with this article. The City may go on the land and commence the work after issuing the notice of intent. The costs of the work performed under this subsection by the City, plus interest at the rate authorized by City shall be billed to the responsible party. In the event a responsible party fails to pay the amount due, the Clerk shall enter the amount due on the tax rolls and collect as a special assessment against the property pursuant to Subch. VII of Ch. 66, Wis. Stats.
- F. Any person violating any of the provisions of this article shall be subject to a forfeiture of not less than \$100 nor more than \$1,000 and the costs of prosecution for each violation. Each day a violation exists shall constitute a separate offense.
- G. Compliance with the provisions of this article may also be enforced by injunction in any court with jurisdiction. It shall not be necessary to prosecute for forfeiture or a cease and desist order before resorting to injunctive proceedings.

§ 288-11.3. Appeals.

- A. Public works commission. The Public Works Commission shall act as the review

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and appeal agency and:

- (1) Shall hear and decide appeals where it is alleged that there is error in any order, decision or determination in administering this article except for cease and desist orders obtained under § 288-11.2C.
 - (2) Upon appeal, may authorize variances from the provisions of this article which are not contrary to the public interest and where owing to special conditions a literal enforcement of the provisions of the article will result in unnecessary hardship; and
 - (3) Shall use the rules, procedures, duties and powers authorized by statute in hearing and deciding appeals and authorizing variances.
- B. Who may appeal. Appeals to the Public Works Commission may be taken by any aggrieved person or by any office, department, board, or bureau of the City of Watertown affected by any decision of the City.

§ 288-11.4. Severability.

If a court of competent jurisdiction judges any section, clause, provision or portion of this article unconstitutional or invalid, the remainder of the article shall remain in force and not be affected by such judgment.

ARTICLE II

Control of Post-Construction Stormwater Management

[Adopted by Ord. No. 08-27 (§ 20.17 of the former City Code); amended in its entirety 4-3-2023 by Ord. No. 23-05]

§ 288-12. Authority.

- A. This article is adopted by the Common Council under the authority granted by § 62.234, Wis. Stats. This article supersedes all provisions of an ordinance previously enacted under § 62.23, Wis. Stats., that relate to stormwater management regulations. Except as otherwise specified in § 62.234, Wis. Stats., § 62.23, Wis. Stats., applies to this article and to any amendments to this article.
- B. The provisions of this article are deemed not to limit any other lawful regulatory powers of the same governing body.
- C. The Common Council hereby authorizes the City and its designees to administer and enforce the provisions of this article.
- D. The requirements of this article do not preempt more stringent stormwater management requirements that may be imposed by any of the following:
 - (1) Wisconsin Department of Natural Resources administrative rules, permits or approvals including those authorized under §§ 281.16 and 283.33, Wis. Stats.
 - (2) Targeted nonagricultural performance standards promulgated in rules by the Wisconsin Department of Natural Resources under § NR 151.004, Wis.

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Adm. Code.

§ 288-13. Findings of fact.

The Common Council finds that uncontrolled, post-construction runoff has a significant impact upon water resources and the health, safety and general welfare of the community and diminishes the public enjoyment and use of natural resources. Specifically, uncontrolled post-construction runoff can:

- A. Degrade physical stream habitat by increasing stream bank erosion, increasing streambed scour, diminishing groundwater recharge, diminishing stream base flows and increasing stream temperature.
- B. Diminish the capacity of lakes and streams to support fish, aquatic life, recreational and water supply uses by increasing pollutant loading of sediment, suspended solids, nutrients, heavy metals, bacteria, pathogens and other urban pollutants.
- C. Alter wetland communities by changing wetland hydrology and by increasing pollutant loads.
- D. Reduce the quality of groundwater by increasing pollutant loading.
- E. Threaten public health, safety, property and general welfare by overtaxing storm sewers, drainageways, and other minor drainage facilities.
- F. Threaten public health, safety, property and general welfare by increasing major flood peaks and volumes.
- G. Undermine floodplain management efforts by increasing the incidence and levels of flooding.

§ 288-14. Purpose and intent.

- A. Purpose. The general purpose of this article is to establish long-term, post-construction runoff management requirements that will diminish the threats to public health, safety, welfare and the aquatic environment. Specific purposes are to:
 - (1) Further the maintenance of safe and healthful conditions.
 - (2) Prevent and control the adverse effects of stormwater; prevent and control soil erosion; prevent and control water pollution; protect spawning grounds, fish and aquatic life; control building sites, placement of structures and land uses; preserve ground cover and scenic beauty; and promote sound economic growth.
 - (3) Control exceedance of the safe capacity of existing drainage facilities and receiving water bodies; prevent undue channel erosion; control increases in the scouring and transportation of particulate matter; and prevent conditions that endanger downstream property.
 - (4) Minimize the amount of pollutants discharged from the separate storm sewer to protect waters of the state.
- B. Intent. It is the intent of the Common Council that this article regulates post-

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construction stormwater discharges to waters of the state. This article may be applied on a site-by-site basis. The Common Council recognizes, however, that the preferred method of achieving the stormwater performance standards set forth in this article is through the preparation and implementation of comprehensive, systems-level stormwater management plans that cover hydrologic units, such as watersheds, on a municipal and regional scale. Such plans may prescribe regional stormwater devices, practices or systems, any of which may be designed to treat runoff from more than one site prior to discharge to waters of the state. Where such plans are in conformance with the performance standards developed under § 281.16, Wis. Stats., for regional stormwater management measures and have been approved by the Common Council, it is the intent of this article that the approved plan be used to identify post-construction management measures acceptable for the community.

§ 288-15. Applicability and jurisdiction.

A. Applicability.

- (1) Where not otherwise limited by law, this article applies to a post-construction site which has 21,780 or more square feet of new impervious surface, unless the site is otherwise exempt under Subsection A(2).
- (2) A site that meets any of the criteria in this Subsection is exempt from the requirements of this article:
 - (a) Land-disturbing construction activity that includes the construction of a one- or two- family residential site less than one acre and is otherwise regulated by the Wisconsin Department of Safety and Professional Services.
 - (b) A post-construction site with less than 10% connected imperviousness based on complete development of the post-construction site, provided the cumulative area of all parking lots and rooftops is less than one acre.
 - (c) Nonpoint discharges from agricultural facilities and practices.
 - (d) Nonpoint discharges from silviculture activities.
 - (e) Routine maintenance for project sites under five acres of land disturbance if performed to maintain the original line and grade, hydraulic capacity or original purpose of the facility.
 - ~~(f)~~ (f) Underground utility construction such as water, sewer and fiberoptic lines. This exemption does not apply to the construction of any aboveground structures associated with utility construction.
 - ~~(g)~~ (g) Minor reconstruction of a highway. Notwithstanding the exemption under this paragraph, the protective areas requirements in §288.18 apply to minor reconstruction of a highway.
 - ~~(h)~~ (h) The requirements of this article do not preempt more stringent stormwater management requirements that may be imposed by any of the following:

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- [1] Wisconsin Department of Natural Resources administrative rules, permits or approvals including those authorized under § 288.16, Wis. Stats., for nonpoint sources, and § 283.33, Wis. Stats., for stormwater discharge.
 - [2] Targeted nonagricultural performance standards promulgated in rules by the Wisconsin Department of Natural Resources under § NR 151.004, Wis. Adm. Code.
- (3) Notwithstanding the applicability requirements in Subsection A(1), this article applies to post- construction sites of any size that, in the opinion of the City, are likely to result in runoff that exceeds the safe capacity of the existing drainage facilities or receiving body of water, that causes undue channel erosion, that increases water pollution by scouring or the transportation of particulate matter or that endangers property or public safety.
- B. Jurisdiction. This article applies to land-disturbing activities within the boundaries of the City of Watertown, and that portion of the Town of Emmet, Dodge County, Wisconsin, that is subject to the City's Plat Review Jurisdiction as set forth in Resolution Exhibit No. 6152 and recorded on September 25, 1997, in Volume 937 on Page 86 as Document No. 851436 in the Dodge County Office of the Register of Deeds and all subsequent amendments.
- C. Exclusions. This article is not applicable to activities conducted by a state agency, as defined under § 227.01(1), Wis. Stats., but also including the office of the District Attorney, which is subject to the state plan promulgated or a memorandum of understanding entered into under § 281.33(2), Wis. Stats.

§ 288-16. Definitions.

As used in this article, the following terms shall have the meanings indicated:

ADEQUATE SOD, OR SELF-SUSTAINING VEGETATIVE COVER — Maintenance of sufficient vegetation types and densities such that the physical integrity of the stream bank or lakeshore is preserved. Self-sustaining vegetative cover includes grasses, forbs, sedges and duff layers of fallen and woody debris.

ADMINISTERING AUTHORITY — The Public Works Director/City Engineer, the City Public Works Commission, the City Council or other entity empowered under § 62.234, Wis. Stats., that is designated by the City of Watertown to administer this article.

AGRICULTURAL FACILITIES AND PRACTICES — Has the meaning given in § 281.16, Wis. Stats.

ATLAS 14 — The National Oceanic and Atmospheric Administration (NOAA) Atlas 14 Precipitation- Frequency Atlas of the United States, Volume 8 (Midwestern States), published in 2013.

AVERAGE ANNUAL RAINFALL — A typical calendar year of precipitation as determined by the Wisconsin Department of Natural Resources for users of models such

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as WlnSLAMM, P8 or equivalent methodology. The average annual rainfall is chosen from a department publication for the location closest to the City.

BEST MANAGEMENT PRACTICE or BMP — Structural or nonstructural measures, practices, techniques or devices employed to avoid or minimize sediment or pollutants carried in runoff to waters of the state.

BUSINESS DAY — A day the City Hall is routinely and customarily open for business.

CEASE AND DESIST ORDER — A court-issued order to halt land-disturbing construction activity that is being conducted without the required permit or in violation of a permit issued by the City of Watertown.

COMBINED SEWER SYSTEM — A system for conveying both sanitary sewage and stormwater runoff.

CONNECTED IMPERVIOUSNESS — An impervious surface connected to waters of the state via a separate storm sewer, an impervious flow path, or a minimally pervious flow path.

DESIGN STORM — A hypothetical discrete rainstorm characterized by a specific duration, temporal distribution, rainfall intensity, return frequency, and total depth of rainfall.

DEVELOPMENT — Residential, commercial, industrial or institutional land uses and associated roads.

DIRECT CONDUITS TO GROUNDWATER — Wells, sinkholes, swallets, fractured bedrock at the surface, mine shafts, nonmetallic mines, tile inlets discharging to groundwater, quarries, or depressional groundwater recharge areas over shallow fractured bedrock.

DIVISION OF LAND — The creation from one parcel of four or fewer parcels or building sites of one or fewer acres each in area where such creation occurs at one time or through the successive partition within a five-year period.

EFFECTIVE INFILTRATION AREA — The area of the infiltration system that is used to infiltrate runoff and does not include the area used for site access, berms or pretreatment.

EROSION — The process by which the land's surface is worn away by the action of wind, water, ice or gravity.

EXCEPTIONAL RESOURCE WATERS — Waters listed in § NR 102.11, Wis. Adm. Code.

EXTRATERRITORIAL — The unincorporated area within three miles of the corporate limits of a first, second, or third class city, or within 1.5 miles of a fourth class city or village.

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FILTERING LAYER — Soil that has at least a three-foot-deep layer with at least 20% fines; or at least a five-foot-deep layer with at least 10% fines; or an engineered soil with an equivalent level of protection as determined by the regulatory authority for the site.

FINAL STABILIZATION — That all land-disturbing construction activities at the construction site have been completed and that a uniform, perennial, vegetative cover has been established, with a density of at least 70% of the cover, for the unpaved areas and areas not covered by permanent structures, or employment of equivalent permanent stabilization measures.

FINANCIAL GUARANTEE — A performance bond, maintenance bond, surety bond, irrevocable letter of credit, or similar guarantees submitted to the City by the responsible party to assure that requirements of the article are carried out in compliance with the stormwater management plan.

GOVERNING BODY — The City Public Works Commission or the City Council.

IMPERVIOUS SURFACE — An area that releases as runoff all or a large portion of the precipitation that falls on it, except for frozen soil. Rooftops, sidewalks, driveways, parking lots and streets are examples of areas that typically are impervious.

IN-FILL AREA — An undeveloped area of land located within an existing urban sewer service area, surrounded by development or natural or man-made features where development cannot occur.

INFILTRATION — The entry of precipitation or runoff into or through the soil.

INFILTRATION SYSTEM — A device or practice such as a basin, trench, rain garden or swale designed specifically to encourage infiltration, but does not include natural infiltration in previous surfaces such as lawns, redirecting of rooftop downspouts onto lawns or minimal infiltration from practices, such as swales or roadside channels designed for conveyance and pollutant removal only.

KARST FEATURE — An area or surficial geologic feature subject to bedrock dissolution so that it is likely to provide a conduit to groundwater, and may include caves, enlarged fractures, mine features, exposed bedrock surfaces, sinkholes, springs, seeps or swallets.

LAND-DISTURBING CONSTRUCTION ACTIVITY — Any man-made alteration of the land surface resulting in a change in the topography or existing vegetative or nonvegetative soil cover, that may result in runoff and lead to an increase in soil erosion and movement of sediment into waters of the state. Land-disturbing construction activity includes clearing and grubbing, demolition, excavating, pit trench dewatering, filling and grading activities.

MAINTENANCE AGREEMENT — A legal document that provides for long-term maintenance of stormwater management practices.

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MEP or MAXIMUM EXTENT PRACTICABLE — The highest level of performance that is achievable but is not equivalent to a performance standard identified in this article. Maximum extent practicable applies when a person who is subject to a performance standard of this article demonstrates to the City's satisfaction that a performance standard is not achievable and that a lower level of performance is appropriate. In making the assertion that a performance standard is not achievable and that a level of performance different from the performance standard is the maximum extent practicable, the responsible party shall take into account the best available technology, cost effectiveness, geographic features, and other competing interests such as protection of public safety and welfare, protection of endangered and threatened resources, and preservation of historic properties.

MINOR RECONSTRUCTION

A transportation facility construction site where a bid is advertised, a construction contract signed where no bid is advertised or a notice of intent is submitted in accordance with NR216, Wisconsin Administrative Code, reconstruction that is limited to 1.5 miles in continuous or aggregate total length of realignment and that does not exceed 100 feet in width of roadbed widening, and that does not include replacement of a vegetated drainage system with a non-vegetated drainage system except where necessary to convey runoff under a highway or private road or driveway.

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NEW DEVELOPMENT — Development resulting from the conversion of previously undeveloped land or agricultural land uses.

NRCS MSE3 DISTRIBUTION — A specific precipitation distribution developed by the United States Department of Agriculture, Natural Resources Conservation Service, using precipitation data from Atlas 14.

OFF SITE — Located outside the property boundary described in the permit application.

ON SITE — Located within the property boundary described in the permit application.

ORDINARY HIGH WATER MARK — Has the meaning given in § NR 115.03(6), Wis. Adm. Code. OUTSTANDING RESOURCE WATERS — Waters listed in § NR 102.10, Wis. Adm. Code.

PERCENT FINES — The percentage of a given sample of soil which passes through a No. 200 sieve.

PERFORMANCE STANDARD — A narrative or measurable number specifying the minimum acceptable outcome for a facility or practice.

PERMIT — A written authorization made by the City to the applicant to conduct land-disturbing construction activity or to discharge post-construction runoff to waters of the state.

PERMIT ADMINISTRATION FEE — A sum of money paid to the City by the

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permit applicant for the purpose of recouping the expenses incurred by the authority in administering the permit.

PERVIOUS SURFACE — An area that releases as runoff a small portion of the precipitation that falls on it. Lawns, gardens, parks, forests or other similar vegetated areas are examples of surfaces that typically are pervious.

POLLUTANT — Has the meaning given in § 283.01(13),
Wis. Stats.

POLLUTION — Has the meaning given in § 281.01(10),
Wis. Stats.

POST-CONSTRUCTION SITE — A construction site following the completion of land-disturbing construction activity and final site stabilization.

PREDEVELOPMENT CONDITION — The extent and distribution of land cover types present before the initiation of land-disturbing construction activity, assuming that all land uses prior to development activity are managed in an environmentally sound manner.

PREVENTIVE ACTION LIMIT — Has the meaning given in § NR 140.05(17), Wis. Adm. Code.

PROTECTIVE AREA — An area of land that commences at the top of the channel of lakes, streams and rivers, or at the delineated boundary of wetlands, and that is the greatest of those widths, as measured horizontally from the top of the channel or delineated wetland boundary to the closest impervious surface.

PUBLIC WORKS DIRECTOR/CITY ENGINEER — The individual holding the Public Works Director/ City Engineer title or his/her designees within the City of Watertown.

REDEVELOPMENT — Areas where development is replacing older development.

RESPONSIBLE PARTY — The landowner or any other entity performing services to meet the requirements of this article through a contract or other agreement.

RUNOFF — Stormwater or precipitation including rain, snow or ice melt or similar water that moves on the land surface via sheet or channelized flow.

SEPARATE STORM SEWER — A conveyance or system of conveyances including roads with drainage systems, streets, catch basins, curbs, gutters, ditches, constructed channels or storm drains, which meets all of the following criteria:

- A. Is designed or used for collecting water or conveying runoff.
- B. Is not part of a combined sewer system.
- C. Is not draining to a stormwater treatment device or system.
- D. Discharges directly or indirectly to waters of the state.

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SILVICULTURE ACTIVITY — Activities including tree nursery operations, tree harvesting operations, reforestation, tree thinning, prescribed burning, and pest and fire control. Clearing and grubbing of an area of a construction site is not a silviculture activity.

SITE — The entire area included in the legal description of the land on which the land-disturbing construction activity occurred.

STOP-WORK ORDER — An order issued by the Public Works Director/City Engineer which requires that all construction activity on the site be stopped.

STORMWATER MANAGEMENT PLAN — A comprehensive plan designed to reduce the discharge of pollutants from stormwater after the site has undergone final stabilization following completion of the construction activity.

STORMWATER MANAGEMENT SYSTEM PLAN — A comprehensive plan designed to reduce the discharge of runoff and pollutants from hydrologic units on a regional or municipal scale.

TECHNICAL STANDARD — A document that specifies design, predicted performance and operation and maintenance specifications for a material, device or method.

TOP OF THE CHANNEL — An edge, or point on the landscape, landward from the ordinary high water mark of a surface water of the state, where the slope of the land begins to be less than 12% continually for at least 50 feet. If the slope of the land is 12% or less continually for the initial 50 feet, landward from the ordinary high water mark, the top of the channel is the ordinary high water mark.

TOTAL MAXIMUM DAILY LOAD or TMDL — The amount of pollutants specified as a function of one or more water quality parameters, that can be discharged per day into a water quality limited segment and still ensure attainment of the applicable water quality standard.

TP-40 — Technical Paper No. 40, Rainfall Frequency Atlas of the United States, published in 1961.

TR-55 — The United States Department of Agriculture, Natural Resources Conservation Service (previously Soil Conservation Service), Urban Hydrology for Small Watersheds, Second Edition, Technical Release 55, June 1986.

TRANSPORTATION FACILITY — A highway, a railroad, a public mass transit facility, a public-use airport, a public trail or any other public work for transportation purposes such as harbor improvements under § 85.095(1)(b), Wis. Stats. "Transportation facility" does not include building sites for the construction of public buildings and buildings that are places of employment that are regulated by the Department pursuant to § 281.33, Wis. Stats.

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TSS — Total suspended solids.

TYPE II DISTRIBUTION — A rainfall type curve as established in the United States Department of Agriculture, Soil Conservation Service, Technical Paper 149, published 1973.

WATERS OF THE STATE — Includes those portions of Lake Michigan and Lake Superior within the boundaries of this state, and all lakes, bays, rivers, streams, springs, ponds, wells, impounding reservoirs, marshes, watercourses, drainage systems and other surface water or groundwater, natural or artificial, public or private, within this state or its jurisdiction.

§ 288-17. Technical standards.

The following methods shall be used in designing the water quality, peak flow shaving and infiltration components of stormwater practices needed to meet the water quality standards of this article:

- A. Technical standards identified, developed or disseminated by the Wisconsin Department of Natural Resources under Subchapter V of Chapter NR 151, Wis. Adm. Code.
- B. Where technical standards have not been identified or developed by the Wisconsin Department of Natural Resources, other technical standards may be used provided that the methods have been approved by the City.
- C. In this article, the following year and location has been selected as the average annual rainfall for water quality modeling purposes: Madison, 1981 (Mar. 12-Dec. 2).

§ 288-18. Performance standards.

- A. Responsible party. The responsible party shall implement a post-construction stormwater management plan that incorporates the requirements of this section.
- B. Plan. A written stormwater management plan in accordance with § 288-20 shall be developed and implemented for each post-construction site. Simplified plans may be completed for sites with less than one acre of land-disturbing construction activity in accordance with the requirements of this chapter.
- C. Maintenance of effort. For redevelopment sites where the redevelopment will be replacing older development that was subject to post-construction performance standards of Ch. NR 151, Wis. Adm. Code, in effect on or after October 1, 2004, the responsible party shall meet the total suspended solids reduction, peak flow control, infiltration, and protective areas standards applicable to the older development or meet the redevelopment standards of this article, whichever is more stringent.
- D. Requirements. The plan required under Subsection B shall include the following:
 - (1) Pollutant control. BMPs shall be designed, installed and maintained to control total suspended solids and phosphorus carried in runoff from the post-construction site as follows:
 - (a) BMPs shall be designed in accordance with Table 1 or to the maximum extent practicable as provided in Subsection D(1)(b). The design shall be based on an average annual rainfall, as compared to no runoff management

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controls.

Table 1

Development Type	Pollutant Reduction Standards	
	TSS Reduction	Phosphorus
New development	80%	30%
In-fill development	80%	30%
Redevelopment	60% of load from parking areas and roads	30% of load from parking areas and roads

- (b) Maximum extent practicable. If the design cannot meet a total suspended solids or phosphorus reduction performance standard of Table 1, the stormwater management plan shall include a written, site-specific explanation of why the total suspended solids or phosphorus reduction performance standard cannot be met and why the total pollutant loads will be reduced only to the maximum extent practicable.
 - (c) Off-site drainage. When designing BMPs, runoff draining to the BMP from off-site shall be taken into account in determining the treatment efficiency of the practice. Any impact on the efficiency shall be compensated for by increasing the size of the BMP accordingly.
- (2) Peak discharge.
- (a) Unless otherwise provided for in this section, all land development activities subject to this section shall establish on-site management practices to control peak flow rates of stormwater discharged from the site. On-site management practices shall be used to meet the following minimum performance standards:
 - [1] The peak flow rates of stormwater runoff from the development shall not exceed those calculated for the series of design storms specified in Subsection D(2)(a)[2] occurring under development conditions specified in Subsection D(2)(a)[4]. Discharge velocities must be nonerosive to discharge locations, outfall channels, and receiving streams. Safe overland conveyance must be provided for discharges from the development.
 - [2] The stormwater management facilities shall contain sufficient storage to contain the runoff from the 100-year, twenty-four-hour rainfall event under developed conditions, while utilizing a peak discharge rate from the developed site which does not exceed the peak runoff rate from the site for a two-year, twenty-four-hour rainfall event under predevelopment conditions.
 - [3] By design, BMPs shall be employed to maintain or reduce the one-year, twenty-four-hour post-construction peak runoff discharge rates to the

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one-year, twenty-four-hour predevelopment peak runoff discharge rate, or to the maximum extent practicable.

- [4] Predevelopment conditions shall assume "good hydrologic conditions" for appropriate land covers as identified in TR-55 or an equivalent methodology. The meaning of "hydrologic soil group" and "runoff curve number" are as determined in TR-55. However, when predevelopment land cover is woodland, grassland, or cropland, rather than using TR-55 values for these land use types, the runoff curve numbers in Table 2 shall be used. Peak discharges shall be calculated using TR-55 runoff curve number methodology, Atlas 14 precipitation depths, and the appropriate NRCS Wisconsin MSE3 precipitation distribution. On a case-by-case basis, the Public Works Director/City Engineer may allow the use of TP-40 precipitation depths and the Type II distribution.

Table 2
Maximum Predevelopment Runoff Curve Numbers

Hydrologic Soil Group	A	B	C	D
Woodland curve number	30	55	70	77
Grassland curve number	39	61	71	78
Cropland curve number	55	69	78	83

(b) This subsection of the section does not apply to any of the following:

[1] A redevelopment post-construction site.

[2] An in-fill development area less than one acre.

[2][3] A post-construction site where the discharge is directly into a lake over 5,000 acres or a stream or river segment draining more than 500 square miles.

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(3) Infiltration. BMPs shall be designed, installed, and maintained to infiltrate runoff to the maximum extent practicable in accordance with the following, except as provided in Subsection D(3)(f) through (i).

(a) Low imperviousness. For development up to 40% connected imperviousness, such as parks, cemeteries, and low-density residential development, infiltrate sufficient runoff volume so that the post-development infiltration volume shall be at least 90% of the predevelopment infiltration volume, based on an average annual rainfall. However, when designing appropriate infiltration systems to meet this requirement, no more than 1% of the post-construction site is required as an effective infiltration area.

(b) Moderate imperviousness. For development with more than 40% and up to 80% connected imperviousness, such as medium- and high-density

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residential, multifamily development, industrial and institutional development, and office parks, infiltrate sufficient runoff volume so that the post-development infiltration volume shall be at least 75% of the predevelopment infiltration volume, based on an average annual rainfall. However, when designing appropriate infiltration systems to meet this requirement, no more than 2% of the post-construction site is required as an effective infiltration area.

- (c) High imperviousness. For development with more than 80% connected imperviousness, such as commercial strip malls, shopping centers, and commercial downtowns, infiltrate sufficient runoff volume so that the post-development infiltration volume shall be at least 60% of the predevelopment infiltration volume, based on an average annual rainfall. However, when designing appropriate infiltration systems to meet this requirement, no more than 2% of the post-construction site is required as an effective infiltration area.
- (d) Predevelopment condition shall be the same as in Table 2 of the peak discharge section of this article.
- (e) Before infiltrating runoff, pretreatment shall be required for parking lot runoff and for runoff from new road construction in commercial, industrial and institutional areas that will enter an infiltration system. The pretreatment shall be designed to protect the infiltration system from clogging prior to scheduled maintenance and to protect groundwater quality in accordance with Subsection D(3)(k). Pretreatment options may include, but are not limited to, oil/grease separation, sedimentation, biofiltration, filtration, swales or filter strips.
- (f) Exclusions. Runoff from the following areas may not be infiltrated and do not qualify as contributing to meeting the requirements of this section unless demonstrated to meet the conditions of Subsection D(3)(k):
 - [1] Areas associated with Tier 1 industrial facilities identified in § NR 216.21(2)(a), Wis. Adm. Code, including storage, loading, rooftop and parking.
 - [2] Storage and loading areas of Tier 2 industrial facilities identified in § NR 216.21(2)(b), Wis. Adm. Code.
 - [3] Fueling and vehicle maintenance areas. Runoff from rooftops of fueling and vehicle maintenance areas may be infiltrated with the concurrence of the regulatory authorities.
- (g) Location of practices. Infiltration practices may not be located in the following areas:
 - [1] Areas within 1,000 feet upgradient or within 100 feet downgradient of direct conduits to groundwater.
 - [2] Areas within 400 feet of a community water system well as specified

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in § NR 811.12(5)(d)6, Wis. Adm. Code, or within the separation distances listed in § NR 812.08, Wis. Adm. Code, for any private well or noncommunity well for runoff infiltrated from commercial (including multifamily residential), industrial and institutional land uses or regional devices for one- and two-family residential development.

- [3] Areas where contaminants of concern, as defined in § NR 720.03(2), Wis. Adm. Code are present in the soil through which infiltration will occur.

(h) Separation distances.

- [1] Infiltration practices shall be located so that the characteristics of the soil and the separation distance between the bottom of the infiltration system and the elevation of seasonal high groundwater or the top of bedrock are in accordance with Table 3:

**Table 3
Separation Distances and Soil Characteristics**

Source Area	Separation Distance	Soil Characteristics
Industrial, commercial, institutional parking lots and roads	5 feet or more	Filtering layer
Residential arterial roads	5 feet or more	Filtering layer
Roofs draining to subsurface infiltration practices	1 foot or more	Native or engineered soil with particles finer than coarse sand
Roofs draining to surface infiltration practices	Not applicable	Not applicable
All other impervious source areas	3 feet or more	Filtering layer

- [2] Notwithstanding Subsection D(3)(h), applicable requirements for injection wells classified under Ch. NR 815, Wis. Adm. Code, shall be followed.

(i) Exemptions. Infiltration practices located in runoff from the following areas may be credited towards meeting the requirements when infiltrated, but the decision to infiltrate under these conditions is optional:

- [1] Areas where the infiltration rate of the soil measured at the proposed bottom of the infiltration system is less than 0.6 inch/hour using a scientifically credible field test method.
- [2] Areas where the least permeable soil horizon to five feet below the proposed bottom of the infiltration system using the U.S. Department of Agriculture method of soils analysis is one of the following: sandy clay loam, clay loam, silty clay loam, sandy clay, silty clay, or clay.

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- [3] Parking areas and access roads less than 5,000 square feet for commercial development.
 - [4] Parking areas and access roads less than 5,000 square feet for industrial development not subject to the prohibitions/exclusions under Subsection D(3)(f).
 - [5] Redevelopment post-construction sites except as provided under § 288-18C.
 - [6] In-fill development areas less than one acre.
 - [7] Roads in commercial, industrial and institutional land uses, and arterial residential roads.
- (j) Where alternate uses of runoff are employed, such as for toilet flushing, laundry or irrigation, such alternate use shall be given equal credit toward the infiltration volume required by this section.
- (k) Groundwater standards.
- [1] Infiltration systems designed in accordance with this section shall, to the extent technically and economically feasible, minimize the level of pollutants in filtration to groundwater and shall maintain compliance with the preventive action limit at a point of standards application in accordance with Ch. NR 140. However, if site- specific information indicates that compliance with a preventive action limit is not achievable, the infiltration BMP may not be installed or shall be modified to prevent infiltration to the maximum extent practicable.
 - [2] Notwithstanding Subsection D(3)(k)[1], the discharge from BMPs shall remain below the enforcement standard at the point of standards application.
- (l) Maximum extent practicable. Where the conditions of Subsection D(3)(f) through (i) limit or restrict the use of infiltration practices, the infiltration performance standard of § 288-18D(3) shall be met to the maximum extent practicable.
- (4) Protective areas.
- (a) "Protective area" means an area of land that commences at the top of the channel of lakes, streams and rivers, or at the delineated boundary of wetlands, and that is the greatest of the following widths, as measured horizontally from the top of the channel or delineated wetland boundary to the closest impervious surface. However, in this subsection, protective area does not include any area of land adjacent to any stream enclosed within a pipe or culvert, such that runoff cannot enter the enclosure at this location.
 - [1] For outstanding resource waters and exceptional resource waters: 75 feet.
 - [2] For perennial and intermittent streams identified on a United States

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Geological Survey 7.5-minute series topographic map, or a county soil survey map, whichever is more current: 50 feet.

- [3] For lakes: 50 feet.
 - [4] For wetlands not subject to Subsection D(4)(a)[5] or [6]: 50 feet.
 - [5] For highly susceptible wetlands: 75 feet. Highly susceptible wetlands include the following types: calcareous fens, sedge meadows, open and coniferous bogs, low prairies, coniferous swamps, lowland hardwood swamps and ephemeral ponds.
 - [6] For less susceptible wetlands: 10% of the average wetland width, but no less than 10 feet nor more than 30 feet. Less susceptible wetlands include degraded wetlands dominated by invasive species such as reed canary grass, cultivated hydric soils; and any gravel pits, or dredged material or fill material disposal sites that take on the attributes of a wetland.
 - [7] In Subsection D(4)(a)[4] through [6], determinations of the extent of the protective area adjacent to wetlands shall be made on the basis of the sensitivity and runoff susceptibility of the wetland in accordance with the standards and criteria in § NR 103.03, Wis. Adm. Code.
 - [8] Wetland boundary delineations shall be made in accordance with § NR 103.08(1m), Wis. Adm. Code. This subsection does not apply to wetlands that have been completely filled in accordance with all applicable state and federal regulations. The protective area for wetlands that have been partially filled in accordance with all applicable state and federal regulations shall be measured from the wetland boundary delineation after fill has been placed. Where there is a legally authorized wetland fill, the protective area standard need not be met in that location.
 - [9] For concentrated flow channels with drainage areas greater than 130 acres: 10 feet.
 - [10] Notwithstanding Subsection D(4)(a)[1] to [9], the greatest protective area width shall apply where rivers, streams, lakes and wetlands are contiguous.
- (b) This subsection applies to post-construction sites located within a protective area, except those areas exempted pursuant to Subsection D(4)(d).
 - (c) The following requirements shall be met:
 - [1] Impervious surfaces shall be kept out of the protective area entirely or to the maximum extent practicable. If there is no practical alternative to locating an impervious surface in the protective area, the stormwater management plan shall contain a written site-specific explanation.

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- [2] Where land-disturbing construction activity occurs within a protective area, and where no impervious surface is present, adequate sod or self-sustaining vegetative cover of 70% or greater shall be established and maintained. The adequate sod or self-sustaining vegetative cover shall be sufficient to provide for bank stability, maintenance of fish habitat and filtering of pollutants from upslope overland flow areas under sheet flow conditions. Nonvegetative materials, such as rock riprap, may be employed on the bank as necessary to prevent erosion, such as on steep slopes or where high-velocity flows occur.
 - [3] Best management practices such as filter strips, swales, or wet detention basins that are designed to control pollutants from nonpoint sources may be located in the protective area.
- (d) This subsection does not apply to:
- [1] Except as provided under § 288-18C, redevelopment post-construction sites.
 - [2] In-fill development areas less than one acre.
 - [3] Structures that cross or access surface waters such as boat landings, bridges and culverts.
 - [4] Structures constructed in accordance with § 59.692(1v), Wis. Stats.
 - [5] Areas of post-construction sites from which runoff does not enter the surface water, including wetlands, without first being treated by a BMP to meet the local ordinance requirements for total suspended solids and peak flow reduction, except to the extent that vegetative ground cover is necessary to maintain bank stability.
- (5) Fueling and vehicle maintenance areas. Fueling and vehicle maintenance areas shall, to the maximum extent practicable, have BMPs designed, installed and maintained to reduce petroleum within runoff, such that the runoff that enters waters of the state contains no visible petroleum sheen.
- (6) Site drainage. Measures shall be implemented to ensure proper site drainage, prevent property damage and protect public health and safety, including the following minimum requirements:
- (a) Drainage easement. Perpetual drainage easements or other deed restrictions shall be recorded on the property to preserve major stormwater flow paths and permanent stormwater BMP locations. Covenants in these areas shall not allow buildings or other structures and shall prevent any grading, filling or other activities that interrupt or obstruct flows in any way. Covenants shall also specify maintenance responsibilities and authorities in accordance with § 288-21.
 - (b) Site grading. Site grading shall ensure positive flows away from all buildings, roads, driveways and septic systems, be coordinated with the

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general stormwater drainage patterns for the area, and minimize adverse impacts on adjacent properties.

- (c) Street drainage. All street drainage shall be designed to prevent concentrated flows from crossing the traffic lanes to the maximum extent practicable. Design flow depths at the road center line for on-street drainage shall not exceed six inches during the peak flows generated by the 100-year, twenty-four-hour design storm, using planned land use conditions for the entire contributing watershed area.
- (d) Bridges and cross-culverts. All new or modified bridges and cross-culverts shall comply with applicable design standards and regulations, facilitate fish passage and prevent increased flooding or channel erosion upstream or downstream from the structure. All bridges and cross-culverts on collector and arterial roadways shall be designed to convey the 100-year, twenty-four-hour design storm. All bridges and cross-culverts on local roadways shall be designed to convey ten-year, twenty-four-hour design storm, while providing an overland flow path that does not impact any structures for the 100-year, twenty-four-hour design storm. A floodplain analysis shall be required for all developments impacting a navigable waterway. This analysis must demonstrate no adverse off-site impacts, in accordance with state and federal regulations and may require larger structures than those specified above. Design flow depths at the road center line for all crossings shall not exceed six inches during peak flows generated by the 100-year, twenty-four-hour design storm, using planned land use conditions for the entire contributing watershed area. All predevelopment runoff storage areas within the flow path upstream of bridges and cross-culverts shall be preserved and designated as drainage easements, unless compensatory storage is provided and accounted for in modeling. As-built documentation shall be submitted for all new or modified structures that are located within a mapped floodplain or that the City determines to be necessary to maintain floodplain modeling for the applicable watershed.
- (e) Subsurface drainage. To avoid property and other damages from groundwater, all buildings planned for human occupation on a regular basis shall meet all of the following:
 - [1] Basement floor surfaces shall be built a minimum of one foot above the highest groundwater table elevation, as documented in the submitted soil evaluations in accordance with City standards. On sloped sites, basements may be allowed partially below the highest groundwater table only on the upslope side if they meet City drainage system standards for design, discharge, engineering oversight, and long-term maintenance. For these sites, the one-foot groundwater separation will be enforced at the furthest downslope point of the basement.
 - [2] Avoid hydric soils as much as possible.
 - [3] The City shall be notified of any drain tiles that are uncovered during

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construction, which the City may require to be restored or connected to other drainage systems.

- [4] No discharge of groundwater from tile lines, sump pumps or other means shall be allowed onto another person's land or any public space without the written approval of the City and the property owner.
 - [a] Where storm sewer is available, all cistern overflows, drain tile, downspouts, roof leaders, surface or area drains may be connected to it with the appropriate City permit.
 - [b] Where storm sewer is not available, all cistern overflows, drain tile, downspouts, roof leaders, surface or area drains or other clean water may be piped separately to the street curb or other place of disposal with the appropriate City permit.
- (f) Open channels. All open channel drainage systems shall at a minimum be designed to carry the peak flows from a 100-year, twenty-four-hour design storm using planned land use for the entire contributing watershed area. Side slopes shall be no steeper than 4h:1v unless otherwise approved by the City for unique site conditions. Water surface elevations for the 100-year, twenty-four-hour design storm shall be calculated for all existing and proposed open channels.
- (g) Storm sewers. All storm sewers shall be designed to convey the ten-year, twenty-four-hour design storm while providing an overland flow path that does not impact any structures for the 100-year, twenty-four-hour design storm, unless otherwise modified by the City.
- (h) Changes to stormwater discharges. For sites where the City determines the post-development stormwater discharge flow paths will be significantly different than predevelopment conditions, or where proposed stormwater discharges may otherwise have a significant negative impact on downstream property owner(s), the City may require the applicant to submit written authorization, record a drainage easement, or complete other legal arrangements with the affected property owner(s) prior to permit issuance.
- (i) Structure protection and safety. Flows generated by the 100-year, twenty-four-hour design storm under the planned land use conditions may exceed the design capacity of conveyance systems, but shall not come in contact with any buildings. For buildings designed for human occupation on a regular basis, the following additional requirements shall apply:
 - [1] The lowest elevation of the structure that is exposed to the ground surface shall be a minimum of two feet above the maximum water surface elevation produced by the 100-year, twenty-four-hour design storm, including flows through any stormwater BMP that may temporarily or permanently store water at a depth of greater than one foot; and
 - [2] The structure shall be set back at least 50 feet from any stormwater

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BMP that may temporarily or permanently store water at a depth of greater than one foot, including any internally drained area with a significant contributing watershed and/or limited runoff storage capacity, as determined by the City. Setback distance shall be measured from the closest edge of water at the elevation produced by the 100-year, twenty-four-hour design storm. The City may exempt existing structures and structures with no basement from this requirement if the City determines other site risks are minimal based on soil and site conditions.

- (7) Swale treatment for transportation facilities.
- (a) Applicability. Except as provided in Subsection D(7)(b), transportation facilities that use swales for runoff conveyance and pollutant removal meet all of the water quality requirements of this section, if the swales are designed to the maximum extent practicable to do all of the following:
- [1] Be vegetated. However, where appropriate, nonvegetative measures may be employed to prevent erosion or provide for runoff treatment, such as rock riprap stabilization or check dams.
 - [2] Swales shall comply with Wisconsin Department of Natural Resources Technical Standard 1005, "Vegetated Infiltration Swales." Transportation facility swale treatment does not have to comply with other sections of Technical Standard 1005.
- (b) Exemptions. The City may, consistent with water quality standards, require that other provisions be met on a transportation facility with an average daily travel of vehicles greater than 2,500 and where the initial surface water of the state that the runoff directly enters is any of the following:
- [1] An outstanding resource water.
 - [2] An exceptional resource water.
 - [3] Waters listed in § 303(d) of the federal Clean Water Act that are identified as impaired in whole or in part, due to nonpoint source impacts.
 - [4] Waters where targeted performance standards are developed under § NR 151.004, Wis. Adm. Code, to meet water quality standards.
- (c) The transportation facility authority shall contact the City to determine if additional BMPs beyond a water quality swale are needed under this subsection.
- E. General considerations for on-site and off-site stormwater management measures. The following considerations shall be observed in managing runoff:
- (1) Natural topography and land cover features such as natural swales, natural depressions, native soil infiltrating capacity, and natural groundwater recharge areas shall be preserved and used, to the extent possible, to meet the requirements

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of this section.

- (2) Emergency overland flow for all stormwater facilities shall be provided to prevent exceeding the safe capacity of downstream drainage facilities and prevent endangerment of downstream property or public safety.
- (3) Unless deemed not possible by City staff, stormwater facilities may be located on outlots with direct access to adjacent public streets.

F. Location and regional treatment option.

- (1) BMPs may be located on site or off site as part of a regional stormwater device, practice or system, but shall be in accordance with § NR 151.003, Wis. Adm. Code.
- (2) The City may approve off-site management measures provided that all of the following conditions are met:
 - (a) The City determines that the post-construction runoff is covered by a stormwater management system plan that is approved by the City of Watertown and that contains management requirements consistent with the purpose and intent of this article.
 - (b) The off-site facility meets all of the following conditions:
 - [1] The facility is in place.
 - [2] The facility is designed and adequately sized to provide a level of stormwater control equal to or greater than that which would be afforded by on-site practices meeting the performance standards of this article.
 - [3] The facility has a legally obligated entity responsible for its long-term operation and maintenance.
- (3) Where a regional treatment option exists such that the City exempts the applicant from all or part of the minimum on-site stormwater management requirements, the applicant shall be required to pay a fee in an amount determined in negotiation with the City. In determining the fee for post-construction runoff, the City shall consider an equitable distribution of the cost for land, engineering design, construction, and maintenance of the regional treatment option.

G. Alternate requirements. The City may establish stormwater management requirements more stringent than those set forth in this section if the City determines that an added level of protection is needed to protect sensitive resources, to control stormwater quantity or control flooding, comply with federally approved total maximum daily load requirements, or control pollutants associated with existing development or redevelopment.

§ 288-19. Permitting requirements, procedures and fees.

A. Permit required. No responsible party may undertake a land-disturbing construction

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activity without receiving a post-construction runoff permit from the City prior to commencing the proposed activity.

- B. Permit application and fees. Unless specifically excluded by this article, any responsible party desiring a permit shall submit to the City a permit application made on a form provided by the City for that purpose.
- (1) Unless otherwise excepted by this article, a permit application must be accompanied by a stormwater management plan, a maintenance agreement and a nonrefundable permit administration fee.
 - (2) The stormwater management plan shall be prepared to meet the requirements of §§ 288-18 and 288-20, the maintenance agreement shall be prepared to meet the requirements of § 288-21, the financial guarantee shall meet the requirements of § 288-22, and fees shall be those established by the City of Watertown as set forth in § 288-23.
- C. Review and approval of permit application. The City shall review any permit application that is submitted with a stormwater management plan, maintenance agreement, and the required fee. The following approval procedure shall be used:
- (1) Within 20 business days of the receipt of a complete permit application, including all items as required by Subsection B, the City shall inform the applicant whether the application, plan and maintenance agreement are approved or disapproved based on the requirements of this article.
 - (2) If the stormwater permit application, plan and maintenance agreement are approved, or if an agreed upon payment of fees in lieu of stormwater management practices is made, the City shall issue the permit.
 - (3) If the stormwater permit application, plan or maintenance agreement is disapproved, the City shall detail in writing the reasons for disapproval.
 - (4) The City may request additional information from the applicant. If additional information is submitted, the City shall have 20 business days from the date the additional information is received to inform the applicant that the plan and maintenance agreement are either approved or disapproved.
- D. Permit requirements. All permits issued under this article shall be subject to the following conditions, and holders of permits issued under this article shall be deemed to have accepted these conditions. The City may suspend or revoke a permit for violation of a permit condition, following written notification of the responsible party. An action by the City to suspend or revoke this permit may be appealed in accordance with § 288-25.
- (1) Compliance with this permit does not relieve the responsible party of the responsibility to comply with other applicable federal, state, and local laws and regulations.
 - (2) The responsible party shall design and install all structural and nonstructural stormwater management measures in accordance with the approved stormwater management plan and this permit.

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- (3) The responsible party shall notify the City at least three business days before commencing any work in conjunction with the stormwater management plan, three days prior to commencing work on the stormwater management practices, and within three business days upon completion of the stormwater management practices. If required as a special condition under Subsection E, the responsible party shall make additional notification according to a schedule set forth by the City so that practice installations can be inspected during construction.
- (4) Practice installations required as part of this article shall be certified "as built" by a licensed professional engineer and furnished to the City in digital AutoCad format (.dwg or .dxf file format), in Adobe PDF format, and in ArcGIS shapefile format (.shp or FileGDB format) or other format as approved by Public Works Director/City Engineer. Files shall be tied to a coordinate system approved by the Public Works Director/City Engineer. Completed stormwater management practices must pass a final inspection by the City or its designee to determine if they are in accordance with the approved stormwater management plan and ordinance. The City or its designee shall notify the responsible party in writing of any changes required in such practices to bring them into compliance with the conditions of this permit.
- (5) The responsible party shall notify the City of any significant modifications it intends to make to an approved stormwater management plan. The City may require that the proposed modifications be submitted for approval prior to incorporation into the stormwater management plan and execution by the responsible party.
- (6) The responsible party shall maintain all stormwater management practices in perpetuity in accordance with the stormwater management plan until the practices either become the responsibility of the City of Watertown, or are transferred to subsequent private owners as specified in the approved maintenance agreement.
- (7) The responsible party authorizes the City to perform any work or operations necessary to bring stormwater management measures into conformance with the approved stormwater management plan, and consents to a special assessment or charge against the property as authorized under Subch. VII of Ch. 66, Wis. Stats., or to charging such costs against the financial guarantee posted under § 288-22.
- (8) If so directed by the City, the responsible party shall repair at the responsible party's own expense all damage to adjoining municipal facilities and drainageways caused by runoff, where such damage is caused by activities that are not in compliance with the approved stormwater management plan.
- (9) The responsible party shall permit property access to the City or its designee for the purpose of inspecting the property for compliance with the approved stormwater management plan and this permit.
- (10) Where site development or redevelopment involves changes in direction, increases in peak rate and/or total volume of runoff from a site, the City may require the responsible party to make appropriate legal arrangements with

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affected property owners concerning the prevention of endangerment to property or public safety.

- (11) The responsible party shall provide a five-year guarantee on all facilities installed as part of the stormwater plan.
- (12) The responsible party is subject to the enforcement actions and penalties detailed in § 288-24, if the responsible party fails to comply with the terms of this permit.
- E. Permit conditions. Permits issued under this subsection may include conditions established by the City in addition to the requirements needed to meet the performance standards in § 288-18 or a financial guarantee as provided for in § 288-22.
- F. Permit duration. Permits issued under this section shall be valid for a period of three years from the date of issuance. The City may extend the period once for up to an additional three years or until all stormwater management practices have passed the final inspection required under Subsection D(4).

§ 288-20. Stormwater management plan.

- A. Plan requirements. The stormwater management plan required under § 288-18B shall contain any such information the City may need to evaluate the environmental characteristics of the area affected by land development activity, the potential impacts of the proposed development upon the quality and quantity of stormwater discharges, the potential impacts upon water resources and drainage utilities, and the effectiveness and acceptability of proposed stormwater management measures in meeting the performance standards set forth in this section. Unless specified otherwise by this section, stormwater management plans shall contain, at a minimum, the following information:
 - (1) Name, address, and telephone number for the following or their designees: landowner; developer; project engineer for practice design and certification; person(s) responsible for installation of stormwater management practices; and person(s) responsible for maintenance of stormwater management practices prior to the transfer, if any, of maintenance responsibility to another party.
 - (2) A proper legal description of the property proposed to be developed, referenced to the U.S. Public Land Survey system or to block and lot numbers within a recorded land subdivision plat.
 - (3) Predevelopment site conditions, including:
 - (a) One or more site maps at a scale of not greater than one inch equals 50 feet. The site maps shall show the following: site location and legal property description; predominant soil types and hydrologic soil groups; existing cover type and condition; topographic contours of the site at a scale not to exceed two feet; topography and drainage network including enough of the contiguous properties to show runoff patterns onto, through, and from the site; watercourses that may affect or be affected by runoff from the site; flow path and direction for all stormwater conveyance sections; watershed boundaries used in hydrology determinations to show compliance with

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performance standards; lakes, streams, wetlands, channels, ditches, and other watercourses on and immediately adjacent to the site; limits of the 100-year floodplain; location of wells and wellhead protection areas covering the project area and delineated pursuant to § NR 811.16, Wis. Adm. Code.

- (b) Hydrology and pollutant loading computations as needed to show compliance with performance standards. Computations of the peak flow discharge rates and discharge volumes from each discharge point in the development. At a minimum, computations must be made for the following storms: one-, two-, ten-, and 100-year. All major assumptions used in developing input parameters shall be clearly stated. The geographic areas used in making the calculations shall be clearly cross-referenced to the required map(s).
- (4) Post-development site conditions, including:
- (a) Explanation of the provisions to preserve and use natural topography and land cover features to minimize changes in peak flow runoff rates and volumes to surface waters and wetlands.
 - (b) Explanation of any restrictions on stormwater management measures in the development area imposed by wellhead protection plans and ordinances.
 - (c) One or more site maps at a scale of not greater than one inch equals 50 feet showing the following: post-construction pervious areas including vegetative cover type and condition; impervious surfaces including all buildings, structures, and pavement; post-construction topographic contours of the site at a scale not to exceed two feet; post-construction drainage network including enough of the contiguous properties to show runoff patterns onto, through, and from the site; locations and dimensions of drainage easements; locations of maintenance easements specified in the maintenance agreement; flow path and direction for all stormwater conveyance sections; location and type of all stormwater management conveyance and treatment practices, including the on-site and off-site tributary drainage area; location and type of conveyance system that will carry runoff from the drainage and treatment practices to the nearest adequate outlet such as a curbed street, storm drain, or natural drainageway; watershed boundaries used in hydrology and pollutant loading calculations and any changes to lakes, streams, wetlands, channels, ditches, and other watercourses on and immediately adjacent to the site.
 - (d) Hydrology and pollutant loading computations as needed to show compliance with performance standards. The computations shall be made for each discharge point in the development, and the geographic areas used in making the calculations shall be clearly cross-referenced to the required map(s). Computations of the peak flow discharge rates and discharge volumes from each discharge point in the development including analysis of the safe capacity of downstream conveyance systems. At a minimum, computations must be made for the following storms: one-, two-, ten-, and

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100-year. All major assumptions used in developing input parameters shall be clearly stated.

- (e) Results of investigations of soils and groundwater required for the placement and design of stormwater management measures. Detailed drawings including cross-sections and profiles of all permanent stormwater conveyance and treatment practices.
 - (5) A description and installation schedule for the stormwater management practices needed to meet the performance standards in § 288-18.
 - (6) A maintenance plan developed for the life of each stormwater management practice including a map showing the BMP, access routes, easements and corresponding streets and water resources, the required maintenance activities and maintenance activity schedule. A vegetation plan should be included if applicable.
 - (7) Cost estimates for the construction, operation, and maintenance of each stormwater management practice.
 - (8) Results of impact assessments on wetland functional values, as applicable.
 - (9) Design computations and all applicable assumptions for stormwater conveyance (open channel, closed pipe) and stormwater treatment practices (sedimentation type, filtrations, infiltration type) as needed to show that practices are appropriately sized and capable of meeting the discharge performance standards of this section.
 - (10) Other information requested in writing by the City to determine compliance of the proposed stormwater management measures with the provisions of this article.
 - (11) All site investigations, plans, designs, computations, and drawings shall be certified by a licensed professional engineer to be prepared in accordance with accepted engineering practice and requirements of this article.
 - (12) Total amount of new/revised impervious area on property in square feet.
- B. Simplified plans. The City may allow simplified stormwater management plans for sites with less than one acre of land-disturbing construction activity.
- (1) Erosion Control Plans are required for construction sites with 3,000 square feet or more of land disturbance.
 - (2) Stormwater management plans including modeling or other calculations accepted for review by the Public Works Director/City Engineer detailed construction plans and stormwater maintenance agreements and are required for construction sites with 21,780 square feet or more of new impervious surface.
- C. Alternate requirements. The City may prescribe alternative submittal requirements for applicants seeking an exemption to on-site stormwater management performance standards under § 288-18D.

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§ 288-21. Maintenance agreement.

A. Maintenance agreement required. The maintenance agreement required under § 288-19B for stormwater management practices shall be an agreement between the City and the responsible party to provide for maintenance of stormwater practices beyond the duration period of this permit. The maintenance agreement shall be filed with the County Register of Deeds as a property deed restriction so that it is binding upon all subsequent owners of the land served by the stormwater management practices.

- B. Agreement provisions. The maintenance agreement shall contain the following information and provisions and be consistent with the maintenance plan required by § 288-20A(6):
- (1) Identification of the stormwater facilities and designation of the drainage area served by the facilities.
 - (2) A schedule for regular maintenance of each aspect of the stormwater management system consistent with the stormwater management plan required under § 288-19B.
 - (3) Identification of the party(s) responsible for long-term maintenance of the stormwater management practices identified in the stormwater management plan required under § 288-19B.
 - (4) Requirement that the responsible party(s) shall maintain stormwater management practices in accordance with the schedule included in Subsection B(2) and shall submit an inspection and maintenance summary report to the City per the inspection frequency described in the maintenance plan and at least once every three years.
 - (5) Authorization for the City to access the property to conduct inspections of stormwater management practices as necessary to ascertain that the practices are being maintained and operated in accordance with the agreement.
 - (6) A requirement of the City to maintain public records of the results of the site inspections, to inform the responsible party responsible for maintenance of the inspection results, and to specifically indicate any corrective actions required to bring the stormwater management practice into proper working condition.
 - (7) Agreement that the party designated under Subsection B(3), as responsible for long-term maintenance of the stormwater management practices, shall be notified by the City of maintenance problems which require correction. The specified corrective actions shall be undertaken within a reasonable time frame as set by the City.
 - (8) Authorization of the City to perform the corrected actions identified in the inspection report if the responsible party designated under Subsection B(3) does not make the required corrections in the specified time period. The City shall enter the amount due on the tax rolls and collect the money as a special charge against the property pursuant to Subch. VII of Ch. 66, Wis. Stats.

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§ 288-22. Financial guarantee.

- A. Establishment of the guarantee. The City may require the submittal of a financial guarantee, the form and type of which shall be acceptable to the City. The financial guarantee shall be up to an amount determined by the City to be 120% of the estimated cost of construction and the estimated cost of maintenance of the stormwater management practices during the period which the designated party in the maintenance agreement has maintenance responsibility. The financial guarantee shall give the City the authorization to use the funds to complete the stormwater management practices if the responsible party defaults or does not properly implement the approved stormwater management plan, upon written notice to the responsible party by the City that the requirements of this article have not been met.
- B. Conditions for release. Conditions for the release of the financial guarantee are as follows:
 - (1) The City shall release the portion of the financial guarantee established under this section, less any costs incurred by the City of Watertown to complete installation of practices, upon submission of as-built plans by a licensed professional engineer. The City may make provisions for a partial pro-rata release of the financial guarantee based on the completion of various development stages.
 - (2) The City shall release the portion of the financial guarantee established under this section to assure maintenance of stormwater practices, less any costs incurred by the City, at such time that the responsibility for practice maintenance is passed onto another entity via an approved maintenance agreement.

§ 288-23. Fee schedule.

The fees referred to in other sections of this article shall be established by the Common Council and may from time to time be modified by resolution. A schedule of the fees established by the Common Council shall be available for review in City Hall. The fee shall cover all City and consultant costs to review the permit application.

§ 288-24. Enforcement.

- A. Any land-disturbing construction activity or post-construction runoff initiated after the effective date of this article by any person, firm, association, or corporation subject to the article provisions shall be deemed a violation unless conducted in accordance with the requirements of this article.
- B. The City shall notify the responsible party by certified mail of any noncomplying land-disturbing construction activity or post-construction runoff. The notice shall describe the nature of the violation, remedial actions needed, a schedule for remedial action, and additional enforcement action which may be taken.
- C. Upon receipt of written notification from the City under Subsection B, the responsible party shall correct work that does not comply with the stormwater management plan or other provisions of this permit. The responsible party shall make corrections as

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necessary to meet the specifications and schedule set forth by the City in the notice.

- D. If the violations to a permit issued pursuant to this article or violations to the requirements of this article on a site where work began prior to obtaining a permit are likely to result in damage to properties, public facilities, or waters of the state, the City may enter the land and take emergency actions necessary to prevent such damage. The costs incurred by the City plus interest, consultant and legal costs shall be billed to the responsible party.
- E. The City is authorized to post a stop-work order on all land-disturbing construction activity that is in violation of this article, issue a citation, or to request the City Attorney to obtain a cease and desist order in any court with jurisdiction.
- F. The City may revoke a permit issued under this article for noncompliance with article provisions.
- G. Any permit revocation, stop-work order, or cease and desist order shall remain in effect unless retracted by the City or by a court with jurisdiction.
- H. The City is authorized to refer any violation of this article, or of a stop-work order or cease and desist order issued pursuant to this article, to the City Attorney for the commencement of further legal proceedings in any court with jurisdiction.
- I. Any person, firm, association, or corporation who does not comply with the provisions of this article shall be subject to a forfeiture of not less than \$100 nor more than \$1,000 per offense, together with the costs of prosecution. Each day that the violation exists shall constitute a separate offense.
- J. Compliance with the provisions of this article may also be enforced by injunction in any court with jurisdiction. It shall not be necessary to prosecute for forfeiture or a cease and desist order before resorting to injunctive proceedings.
- K. When the City determines that the holder of a permit issued pursuant to this article has failed to follow practices set forth in the stormwater management plan, or has failed to comply with schedules set forth in said stormwater management plan, or a permit was not obtained prior to the start of work on site, the City or a party designated by the City may enter upon the land and perform the work or other operations necessary to bring the condition of said lands into conformance with requirements of the approved plan. The City shall keep a detailed accounting of the costs and expenses of performing this work. These costs and expenses shall be deducted from any financial security posted pursuant to § 288-22 of this article. Where such a security has not been established, or where such a security is insufficient to cover these costs, the costs and expenses shall be entered on the tax roll as a special charge against the property and collected with any other taxes levied thereon for the year in which the work is completed.

§ 288-25. Appeals.

- A. Public works commission. The Public Works Commission shall act as the review and appeal agency and:

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- (1) Shall hear and decide appeals where it is alleged that there is error in any order, decision or determination made in administering this article except for citations and cease and desist orders obtained under § 288-24E;
- (2) Upon appeal, may authorize variances from the provisions of this article which are not contrary to the public interest and where owing to special conditions a literal enforcement of the provisions of the article will result in unnecessary hardship; and
- (3) Shall use the rules, procedures, duties and powers authorized by statute in hearing and deciding appeals and authorizing variances.

B. Who may appeal. Appeals to the Public Works Commission may be taken by any aggrieved person or by any office, department, board, or bureau of the City of Watertown affected by any decision of the City.

§ 288-26. Severability.

If any section, clause, provision or portion of this article is judged unconstitutional or invalid by a court of competent jurisdiction, the remainder of the article shall remain in force and not be affected by such judgment.

SECTION 2. All ordinances or parts of ordinances inconsistent with the provisions of this ordinance are hereby repealed.

SECTION 3. This ordinance shall take effect and be in force the day after its passage and publication.

Adopted March 4, 2025
Signed - Megan Dunneisen-City Clerk
Approved March 4, 2025
Signed - Emily McFarland-Mayor
Ord. 25-03

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Maureen McBroom

From: Stacy Winkelman
Sent: Thursday, October 9, 2025 2:56 PM
To: Maureen McBroom; Mark Pitterle
Cc: Andrew Beyer
Subject: RE: MS4 Permit Leaf Collection Requirements

Yes, when I say “leaf collection” I mean with the actual leaf suckers. The street sweeper has been out in the designated heavy leaf areas.

Stacy Winkelman

Operations Manager
DPW – Street/Solid Waste Division
Phone: 920-262-4047
Cell: 920-342-8885
swinkelman@watertownwi.gov



From: Maureen McBroom <MMcBroom@watertownwi.gov>
Sent: Thursday, October 9, 2025 2:50 PM
To: Stacy Winkelman <SWinkelman@watertownwi.gov>; Mark Pitterle <MPitterle@watertownwi.gov>
Cc: Andrew Beyer <ABeyer@watertownwi.gov>
Subject: RE: MS4 Permit Leaf Collection Requirements

Hi again-

I noticed today’s construction meeting notes from Wanda say “**Leaf collection? Perhaps one machine.**”

Can you confirm that the street sweeper is being used in early October in the yellow highlighted areas on the R/M map? This is to meet both Section 2.6.5 of the MS4 Permit and the additional TMDL requirements in Appendix A of the MS4 Permit.

Thank you!
-McB

Maureen McBroom

Stormwater Project Manager
City of Watertown
106 Jones Street
Watertown, WI 53094

From: Maureen McBroom
Sent: Tuesday, September 30, 2025 9:29 AM
To: Stacy Winkelman <SWinkelman@watertownwi.gov>; Mark Pitterle <MPitterle@watertownwi.gov>
Cc: Andrew Beyer <ABeyer@watertownwi.gov>
Subject: MS4 Permit Leaf Collection Requirements
Importance: High

Hi Stacy & Mark-

Happy Fall! Here is a map focusing in on the areas (in yellow) that we are required to do enhanced leaf collection activities per the MS4 Permit starting October 1st each year.

This map was developed by Ruekert & Mielke as part of the 2022-2023 grant we received to update the MS4 Permit-TMDL compliance programs. The results of this study was used to justify purchasing and operating a 2nd street sweeper.

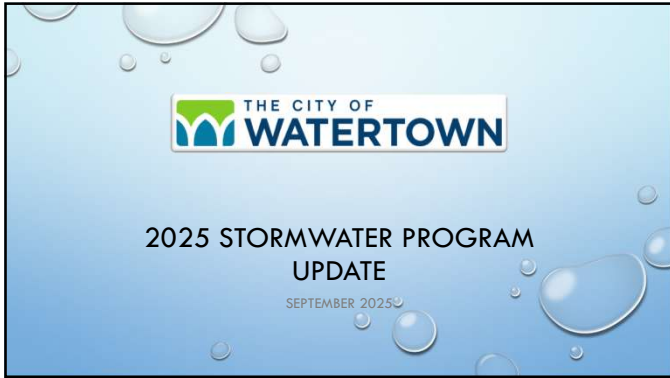
We are able to capture over 230 pounds of phosphorus that would otherwise get into the Rock River and tributary waterways through the annual leaf collection program. **This is huge.** It is very difficult and costly to capture that amount of phosphorus through traditional stormwater BMPs and even water quality trading.

The DNR's guidance doc re: this program is also attached.

Let me know if you would like to review or discuss more details about the analysis Ruekert & Mielke did on this program.

Thank you!
-McB

Maureen McBroom
Stormwater Project Manager
City of Watertown
106 Jones Street
Watertown, WI 53094
Office: (920) 206-4264



1



2



3



4





5




6

MS4: ILLICIT DISCHARGE DETECTION AND ELIMINATION (IDDE) (3)

- 553 Storm Sewer Outfalls to Rock River, creeks, wetlands
- 49 IDDE Inspections Completed Annually
 - ~2 dozen in-field tests completed annually
- 0 "hits" detected in last 3 years
- Mini-Storm Program to capture water from foundation drains



* In-house Inspections reduced \$3,000 annual fee to cost of supplies.

7

MS4: CONSTRUCTION SITE POLLUTANT CONTROL (4)

- 76 Erosion Control Inspections completed in 2024
 - Monthly EC Inspections
 - Weekly if there are problems on site
 - Bi-monthly for Inactive Sites
- Try to meet with site project manager at beginning of project, and again if there are concerns or noncompliance issues.







*In-house inspections reduces cost developer pays for 3rd party inspector.

8

MS4: POST-CONSTRUCTION STORMWATER MANAGEMENT (5)

- New Stormwater BMPs in 2024:
 - YMCA – 1 Biofilter
 - Rock River Ridge – 2 Wet Ponds
 - New Fire Station – 3 Biofilters
 - Yard Waste Site Sandy Biofilter (DNR Grant)
 - Boomer Street – 2 Biofilters
- Permits/Plan Reviews: Issued 19 EC & SW Permits; 13 Plan Reviews & Revisions
- 34 BMPs Inspected in 2024; 7 Maintenance Letters sent + Work Orders for City Crews









*In-house plan reviews reduces cost to developer for 3rd party reviewer; also ensures comprehensive, integrated multi-departmental review of City.

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RECENT STORMWATER BMPs/DEVELOPMENTS

- YMCA – 1 Biofilter
- Rock River Ridge – 2 Wet Ponds
- New Fire Station – 3 Biofilters
- Yard Waste Site Sandy Biofilter (DNR Grant)
- Current requirements – state, city, WQ, quantity;
 - Managing Expectations: 10-year event piping, 6 inches stormwater in road, prioritize EMT/truck routes, & other flood plan goals








10

MS4: POLLUTION PREVENTION (6)

- Municipal Operations Stormwater Data:
 - 232 Lane Miles Swept (2024)
 - 450 yards of sediment and debris collected from street sweeper and catch basins (2024)
 - 166 bags + 3,320 yards of leaves collected curbside (2024)
 - 1,635 tons of salt, 3,719 gallons of brine* applied to streets (2024)





*Brine = 77% water, 23% salt mixture

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MS4: MS4 MAP (GIS UPDATES)

- Recent Updates to Stormwater Layers
 - Added Culverts, Swales, Updated Wetlands, & 6 Sub-Basins
 - Added/Improved Labeling, Query and Export Features
 - Added Project Area Selection Feature
 - Provided Guidance for Staff GIS Updates, As-Builts, etc.
 - Coordinated Updates in FieldMaps & Stormwater Editor (desktop)
 - Joint meeting with Fire Dept., County (GIS) & Engineering (SW)






12

MS4: TMDL / WATER QUALITY TRADING

- Watertown Waterways Improvement Program (WWIP)
 - 1st Practice Completed & Installed
- New Stormwater BMPs – City & Private
- Leaf Collection + Increased Street Sweeping during Fall
- Streambank Stabilization –currently evaluating





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WATER QUALITY TRADING

- 1st Practice Completed in 2024
 - 10-year Agreement signed with Owner
 - Buffers have been Installed
 - DNR reviewed and conditionally approved practice
- 2nd Practice: Develop in 2025, Install in 2026
- Based on 1st practice, about 24 more practices needed

Pollutant	Interim Credit	Long-Term Credit
Total Phosphorus	14.8 lbs/year	58.45 lbs/year
Total Suspended Solids	2.18 tons/year	6.53 tons/year

**Innovative water quality trading program, developed with partners and with cooperation from WDNR, reduces cost of TMDL compliance from \$13 Million + to \$1.5 Million (current projected estimate).*



14

COMING IN 2025

- Next 5-year MS4 Permit Anticipated in 2025
 - Develop Plans for Impaired Waterways
 - Updates to Written MS4 Permit Programs
 - IDDE Analysis, Implementation & Protocols
 - Greater Focus on Maintenance: Private & Public BMPs
 - Enhanced SWPPPs
 - Salt Use Reporting, Calibration of Equipment
 - TMDL Reductions: 20% TSS, 10% Phosphorus (of remainder needed to meet TMDL)
 - ??????




15

FLOOD CONTROL PLAN

- 2024 City-wide Flood Control Master Plan
 - 15 Priority Flooding Areas
 - Detailed, Site-specific Drainage Study and Design needed for each area & solution
 - Hart Street – Next Steps
 - Westside Creek System Study
 - WEM Grant Application

Goal is to minimize flooding, and to recover quickly afterwards.

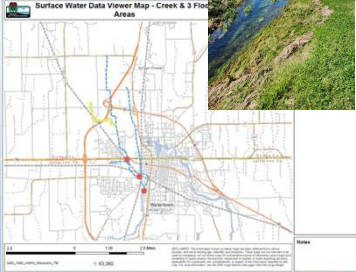
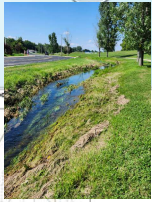





16

WESTSIDE CREEK SYSTEM STUDY

- 5 Major Flooding Areas Drain To Westside Creek System
- Drainage Area is Outside/Upstream of City & along west of Church Street
- Can receiving creek handle more water from new flood control projects?
- WEM Grant? 75% Cost, City= 25%

17

MAINTENANCE

- Annual Storm Sewer Cleaning and Televising
 - Heavy Cleaning Needed in Specific Areas
- Storm Sewer Repairs
 - City Crews Completing Discovered Maintenance
- BMP Maintenance: Routine and Occasional Tasks
 - Routine Examples: Vegetation Clearing, Minor Inlet/Outlet Repairs,
 - Occasional Examples: Dredging, Re-shaping BMPs, Re-stabilization





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2024 CLEANING & TELEVISION

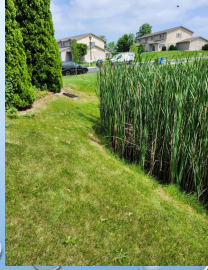



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BMP MAINTENANCE

- 105 Stormwater Best Management Practices (BMPs) (Public and Private)
 - ~30 are City-owned BMPs, Inspected Annually
 - ~25 Private BMPs are Inspected Once every 3 years
 - Underground systems are inspected by 3rd parties; reports submitted to City



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RIVERSIDE PARK CREEK

- 2022-2023: Dodge County Land & Water Conservation Department: Plan Development and Cost-Share Program
- 2024: City Crews Dredged Invasive Plants/Installed Riprap in Critical Areas
- 2025: Rock River Coalition Organizing Native Planting Installation
 - Saturday, 9/13/25 9:00-2:00 Volunteer Workday




THE CITY OF WATERTOWN

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GRANTS

TRIPS & Storm Water Planning	Storm Water Planning Updates	2022-2023	WDNR	\$70,000.00	Complete
LRIP	Market Way	2023-2024	WisDOT	\$49,785.00	Complete
TRIPS & Storm Water Construction	2023 Streets Catch Basins	2023-2024	WDNR	\$150,000.00	Complete
TRIPS & Storm Water Construction	Yard Waste Site Biofilters	2023-2024	WDNR	\$150,000.00	Complete
TAP Grant	Tivoli Island Historic Bridge Study	2023-2027	WisDOT	\$25,600.00	Active
TAP Grant	City-wide Bike & Ped Network Plan	2023-2027	WisDOT	\$80,000.00	Active
HSP	Welsh Road	2023	WisDOT	\$171,643.50	Complete
BIL-STP Urban	Dewey Ave.		WisDOT	\$1,425,760.00	Active
BIL-STP Local	S. First St. & Western Ave.		WisDOT	\$500,000.00	Active
BIL	N. Fourth Street Resurfacing	2023	WisDOT	\$426,882.40	Complete
Local Bridge	Cady Street Bridge Rehab	2023	WisDOT	\$363,400.00	Complete
Dodge County LWCD	Riverside Park Creek Improvements	2024	WisDOT	\$13,900.00	Complete
STP Urban	Labaree Street	2025-2029	WisDOT	\$923,776.00	Active
TAP Grant	S. Church Street Shared-use Path	2024-2028	WisDOT	\$1,213,256.66	Active
Targeted Runoff Management Grant	Fannie Lewis Park Shoreline Stabilization	2025-2026	WDNR	\$113,943.00	Active
	Total Grant Awards:			\$5,444,046.56	

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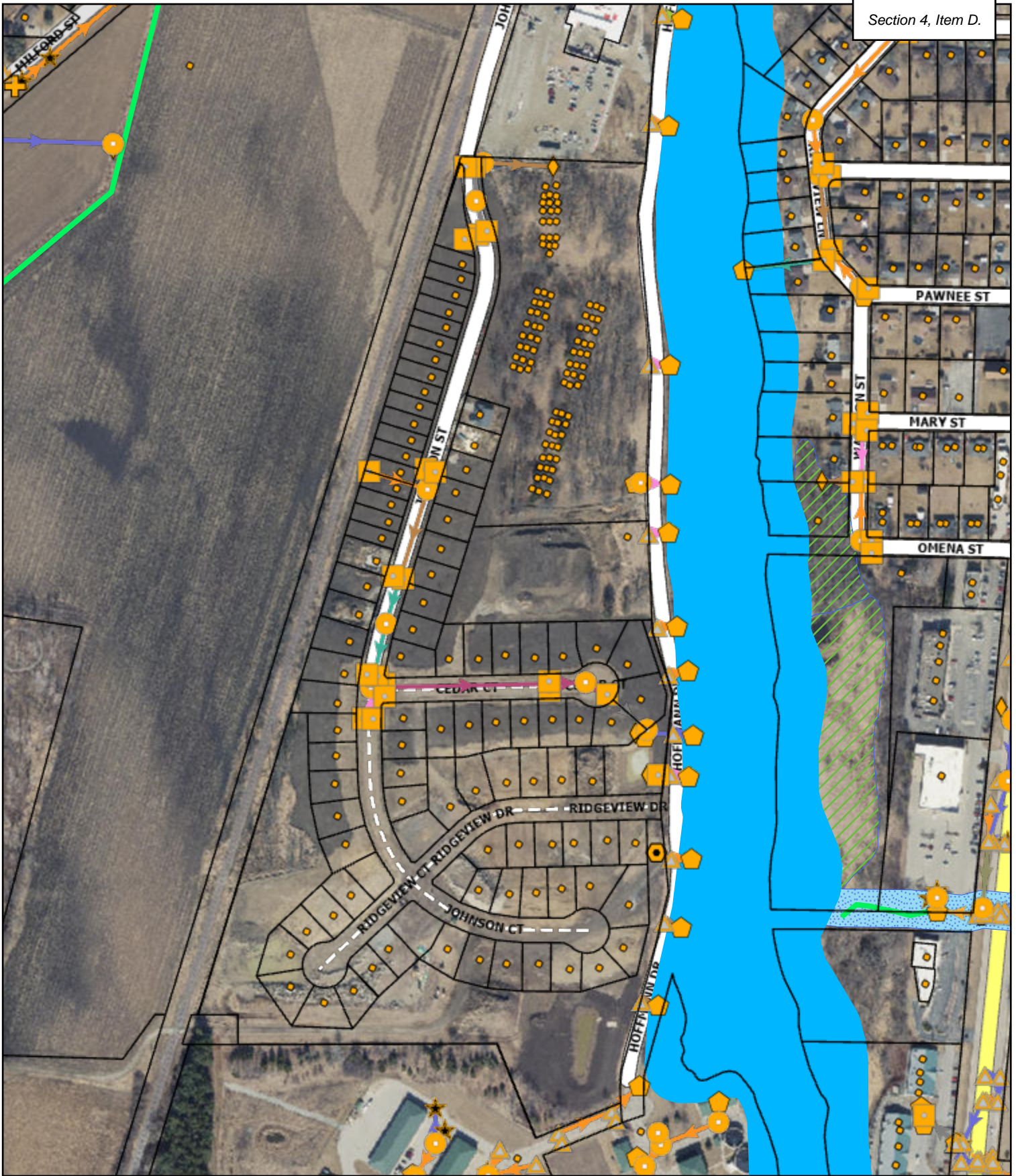
22

QUESTIONS & DISCUSSION




THE CITY OF WATERTOWN

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Municipal Boundary	End Section	6	21	42	68	Open Channel
Parcels Boundaries	Outfall	8	24	45	76	City
Common Areas	Tee	10	27	48	76	County
Addresses	Culverts	12	30	49	84	Federal
Catchbasins	Null Features	14	33	53	96	Private
Manholes	Stormwater Pipes by Diameter	15	36	54	98	State
Inlets	Pipe Diameter	18	38	60	Other	Roadside Swale
		4				

THE CITY OF WATERTOWN
Opportunity runs through it.

City of Watertown Geographic Information System

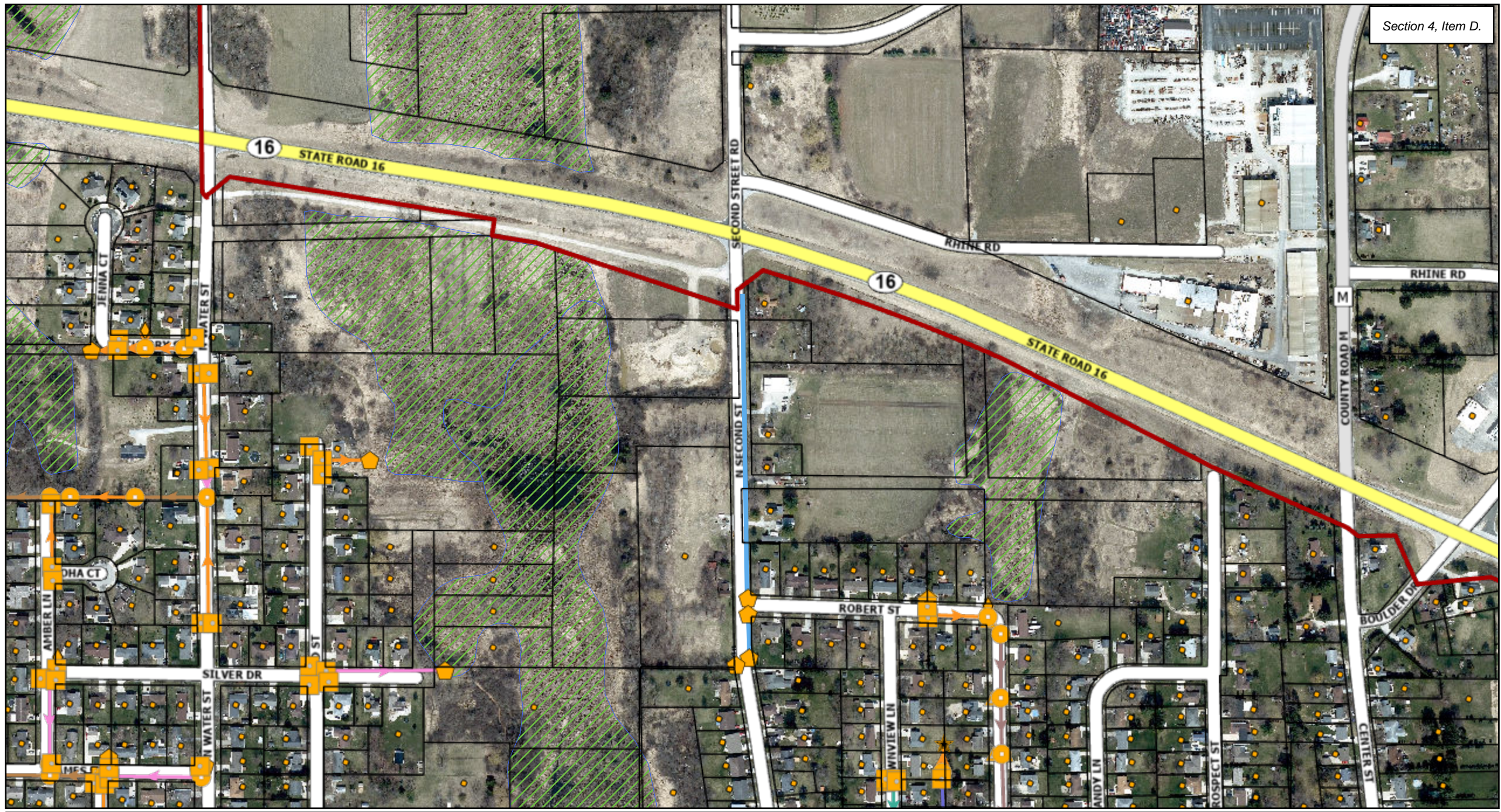
Scale: 1:4,123 Printed on: February 2014
 SCALE BAR = 1" Author: [Redacted]

249

DISCLAIMER: This map is not a substitute for an actual field survey or onsite investigation. The accuracy of this map is limited to the quality of the records from which it was assembled. Other inherent inaccuracies occur during the compilation process. City of Watertown makes no warranty whatsoever concerning this information.

City of Watertown-New Streets and Storm 2

Section 4, Item D.



Municipal Boundary	Manholes	Culverts	8	18	33	48	68	98	Federal	DNR Wetlands
Parcels Boundaries	Inlets	Null Features	10	21	36	49	76	State	Private	DodgeCo
Common Areas	End Section	Stormwater Pipes by Diameter	12	24	38	53	76	City	Other	RGB
Addresses	Outfall	Pipe Diameter	14	27	42	54	84	County	Red: Band_1	Green: Band_2
Catchbasins	Tee	4	15	30	45	60	96	Roadside Swale	Blue: Band_3	
		6								

THE CITY OF WATERTOWN
 Opportunity runs through it.
 City of Watertown Geographic Information System

Scale: 1:4,428
 SCALE BAR = 1"

Printed on: February 25, 2025
 Author:

DISCLAIMER: This map is not a substitute for an actual field survey or onsite inspection. The accuracy of this map is limited to the quality of the records from which it was assembled. Other inherent inaccuracies occur during the compilation process. City of Watertown makes no warranty whatsoever concerning this information.

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MS4 Permit No. WI-S050075-3

2025 MS4 Permit Annual Report

Section A.6.2: Annual Update on TMDL Implementation

MS4 Permit No. WI-S050075-3, Section A.6.2 Annual Reporting. For compliance options outlined under sections A.3, A.4, and A.5, the permittee shall include a description and the status of progress toward implementing the identified actions and activities in their MS4 annual reports due by March 31 of each year.

The City of Watertown has made progress in implementing the Rock River TMDL in 2025 with the following tasks:

1. Continued to implement the Watertown Waterways Improvement Program, a Quality Trading Program developed with Jefferson County & Rock River Coalition.

Inspections were completed and payment was made in 2025 on the first practice, a set of 2 harvestable buffers, which was installed in 2024. The water quality plan describing the overall local water quality trading program and this particular trade was conditionally approved by WDNR in October 2024. A 10-year agreement was signed by the property owner and the City. This practice will control 73.25 pounds of Phosphorus and 8.71 tons of Total Suspended Solids (TSS) over those 10 years. Additional practices were considered but did not result in a new agreement with property owners. Alternative sites/practices will be considered in 2026.

The City, Jefferson County & the Rock River Coalition have come together to develop a Water Quality Trading (WQT) program. This program will assist the City's goals of meeting the TSS & TP reduction requirements in the Rock River TMDL reachsheds 28 & 29. This partnership incorporates the nonpoint source experience and expertise of the Jefferson County staff and the relationships and outreach experience and expertise of the Rock River Coalition with the City's funding and need to meet the TMDL requirements. DNR staff have been very helpful in providing clarifications and MS4 Permit program guidance in developing a watershed-style WQT program.

2. Plan and Implement New Stormwater BMPs.

The City completed installation of a biofilter in the right-of-way along Hiawatha Street to improve stormwater management in reachshed 29. The three new biofilters at the new Fire Station site were also completed in 2025. Planning is underway for future stormwater management facilities to control both flooding and pollutants.

WinSLAMM modeling was completed and compiled for recently installed city-owned BMPs.

City-owned BMPs 2022-2025					Proposed	Proposed	Percent TSS	Percent TP	TSS reduction	TP reduction
BMP Number	BMP Name	Site Name	NC TSS (lbs)	NC TP (lbs)	TSS (lbs)	TP (lbs)	Reduction	Reduction	(lbs)	(lbs)
79	Permeable Pavers	Jones St Lot	64.12	0.1242	5.06	0.01009	92.11%	91.87%	59.06	0.1141
80	Biofilters	Jones St Lot	20.33	0.0336	9.17	0.01827	54.92%	45.69%	11.17	0.0154
81	Permeable Pavers	Cady St Lot	33.43	0.0553	3.61	0.00790	89.19%	85.71%	29.82	0.0474
92	South Biofilter	Reflection Park	5.79	0.0344	0.85	0.00531	85.41%	84.55%	4.95	0.0291
93	West Biofilter	Reflection Park	15.46	0.0992	2.69	0.01689	82.63%	82.97%	12.77	0.0823
94	Permeable Pavers	Senior Center	362.8	0.6000	242.90	0.40690	33.05%	32.18%	119.90	0.1931
95	Biofilter	Watertown Court	3299	5.6000	1354.00	2.90000	58.96%	48.21%	1945.00	2.7000
96	Biofilter	Boomer St.-West	360.3		139.80		61.20%	46.60%	220.50	0.0000
	Biofilter	Boomer St. - East	375.4		145.66		61.20%	46.60%	229.74	0.0000
102	Biofilter	Fire Station-North	1346	2.8890	269.10	0.57780	80.01%	80.00%	1076.93	2.3112
103	Biofilter	Fire Station-Middle								
104	Biofilter	Fire Station-South								
106	Biofilter	Hiawatha St. ROW	664	1.8440	662.54	0.38245	85.49%	79.26%	567.65	1.4616
						0.00000			0.00	0.0000
Total			6546.63	11.2797	2835.36	4.26715			4277.50	6.6659

Program Contact

Maureen McBroom, Stormwater Project Manager

mmcbroom@watertownwi.gov

920-206-4264



2025 Year-End Report

2025 was the third year of the Watertown Waterways Improvement Program (WWIP) since the initial public meeting was held August 30, 2023. Jefferson County Land and Water Conservation Department (LWCD) continued to work with landowners from the volunteer list within Reachshed 28 to install one of four of the provided conservation practices: harvestable buffers, filter strips, grassed waterways, and roof runoff structures. Though much work was performed communicating to landowners and modeling practices, it was determined that the potential trades were not beneficial to the City of Watertown. This was both due to the high price per pound of phosphorus and the farmer unwilling to change their operations due to field conditions, Snap+ deficiencies, and present farming operations. Therefore, there were no credits generated by a new conservation practice in 2025 and no trade was completed.

The modelling completed on one landowner's land in 2025 found that no-till fields already have minimal runoff since there's no tillage happening throughout the fields. Fields that have no tillage generally have much less erosion and associated pollutant loading of phosphorus and total suspended solids. This was proven when modelling various scenarios using Snap+ in a no-till system to add a filter strip or harvestable buffer to the downslope side of the field. A thirty foot wide, 2.9 acre filter strip would have been the least expensive option and would cost roughly \$1,500 per pound of phosphorus reduction.

The other landowner and their proposed field would have worked well within program requirements but didn't work out due to DNR guidelines and Snap+ deficiencies. The proposed field was an 80 acre field with multiple aspects (slopes facing multiple directions) and complied with all NR151 standards. However, DNR and Snap+ technical team said the whole field must be broken down into separate fields that splits the different drainage aspects which created 3-4 different fields. Due to the predominant soils and predefined attributes in Snap+, the fields could not meet the "tolerable soil loss," which is defined as the amount of tolerable soil losses as determined by the soil type. After splitting the 80-acre field into smaller fields, the only way to achieve the tolerable soil loss would be to change the farming operation's tillage practice and introduce contour farming. These changes to the farmer's operations were unfortunately too drastic for them to consider moving forward with a trade. Also, to make this trade financially viable for Watertown, the



harvestable buffer size would have to be less acreage (lower width) which made it less appealing for the farmer to move forward with a trade.

2025 Highlights:

- Continued collaborative efforts between City of Watertown, Jefferson County LWCD, and Rock River Coalition to discuss program features and work through challenges. Several meetings throughout 2025 were held.
- An article was written by Garrett Hopkins of the Rock River Coalition and was printed in their newsletter along with DNR's MS4 newsletter. The article was written to showcase the 2024 trade and discuss program features.
- Continued discussions with DNR to help consider various trades, Snap+ issues, and various WQT Guidance questions that arise.
- Watertown and LWCD worked with DNR on various WQT Guidance components and offered comments to the proposed new guidance draft.
- Discussed program future of using volunteers versus using a targeted approach. LWCD will continue to work with volunteers, but they have developed a plan to locate good projects (by looking at variables such as fields adjacent to waterways, field slope, tillage) and initiate conversations with the landowners.
- LWCD wrote a letter to all interested participants to give program updates and let potential participants know the process is moving slower than anticipated and to reach out with any questions.
- Presentations on the program were given at the following: Northcentral Wisconsin Stormwater Coalition meeting, Southeastern Wisconsin Watersheds Trust, Inc conference, and Waukesha County Stormwater Workshop.
- Several inspections were conducted for the practice installed in 2024 to confirm the harvestable buffer is functioning and the landowner is approved for annual payment.
- Watertown paid LWCD a total of \$10,941.48 to implement the program, which was less than budgeted for 2025. This is due to no trade being completed hence no updates needed to the existing WQT Plan.



CONSERVATION PRACTICE HARVESTABLE BUFFER INSPECTION AND VERIFICATION FORM

The intended use of this form is for annual inspections, practice installation verification, and practice establishment guidelines for harvestable buffers. It will be used as a method for LWCD to show Watertown the practice is functioning, and the landowner is approved for payment of installation costs and annual payments.

GENERAL INFORMATION

Landowner Name, Address, and Contract Number: Ted Mueller, W2117 North Side Drive, Watertown, WI 53094. WWIP-RR28-001-2024

Practice Type and Install Date: Harvestable Buffer. Installed 3-15-24.

Site Visit Date(s): 8/14/25 after significant rainfall events in the region to make sure buffer is intact. 11/6/2025 for annual inspection.

PRACTICE INSTALLATION AND ESTABLISHMENT GUIDELINES

Practice installed according to specs, is correct size/dimension, installed in proper location, and no signs of encroachment? YES or NO

If no, explain:

Practice installed/established in accordance with Conservation Practice Agreement, Exhibit C Installation and Maintenance Plan? YES or NO

If no, explain:

Was practice checked after severe storms and verified it remained intact? YES or NO

If no, explain:

Are there any areas void of vegetation and need replanting? YES or NO

If yes, explain:

POST-ESTABLISHMENT STANDARDS AND GUIDELINES

Do plant densities achieve successful planting performance standards? YES or NO

- Checked several areas for densities within one square foot and found buffers to be in compliance.

Is the % stand canopy and stem densities per square foot in accordance with Code 393? YES or NO

If no, explain:

Are any areas void of vegetation and need replanting? YES or NO

If yes, explain:

Harvested plants have at least 4 inches of growth after cutting? YES or NO

If no, explain:

Is there overland sheet flow entering the buffer? YES or NO

- The known concentrated flow area to the north of the west buffer had planted corn in field, but the area showed no signs of any concentrated flows, rilling, or gullying.

Concentrated flows dissipating prior to upslope edge of buffer? YES or NO

If no, explain:

Evidence of buffers being used as travel lanes for equipment or livestock? YES or NO

If yes, explain:

Any grading of buffer needed due to sediment deposition reducing buffer effectiveness? YES or NO

If yes, explain:

Any removal of unevenly deposited sediment accumulation needed that disrupts sheet flow? YES or NO

If yes, explain:

List any weed management needs or maintenance activities conducted on practice: See below.

Other Observations and Verification Status:

Pennycress (*Thlaspi arvense*) and a weedy mustard (likely *Erysimum cheiranthoides*) found in areas of buffer and is affecting grassy-alfalfa seed from establishing in small areas (less than 5 sq. ft.). Buffer vegetation was found growing throughout weed stands, but either mowing prior to seed development or an herbicide application may be needed if not out competed by late next year. Timely harvesting of crop could help combat seed dispersal.

LANDOWNER MAINTENANCE NEEDS:

List of any management activities needed:


- Mow or spray weed patches when applicable to prevent further distribution of pennycress and mustard weed seeds.
- Continue to follow Conservation Practice Agreement, particularly Exhibit C, Maintenance and Installation Plan.

I certify that installed conservation practice is performing in accordance with standards set forth in landowner's Conservation Practice Agreement, Exhibit C, and landowner is eligible for payments.

LWCD Staff Member: _____



Date: 11-6-2025



2025 MS4 PERMIT ANNUAL REPORT
MARCH 2026

1

TODAY'S DISCUSSION



- Stormwater Program Goals
- MS4 Permit Annual Report Review
 - MS4 Programs in 2025
- Next Permit: 2026 – 2031 (?)




2

STORMWATER PROGRAM

- Flood Control – to minimize flooding impacts to public infrastructure and public/private properties
- Pollutant Control – to protect quality of life for residents, property owners and meet Wisconsin Department of Natural Resources Stormwater (MS4) Permit
- Maintenance so the systems work.

3

OUTREACH & EDUCATION / PUBLIC INVOLVEMENT

- City Connection Newsletter
- Social Media & Website
- Brochures @ City Hall
- Tables at Lights & Sirens, Boo Bash, Farmer's Markets
- 4th Annual Rain Barrel Workshop
- Citizen Stream Monitoring Training (2 sessions)
- Streambank Clean-up
- WI Stormwater Awareness Week
- Riverside Park Creek Streambank Plantings



Add PWW, RRC partner logos




4

ILLICIT DISCHARGE DETECTION AND ELIMINATION (IDDE) / GIS UPDATES

- 408 Storm Sewer Outfalls to Rock River, creeks, wetlands
- 49 IDDE Inspections Completed; 13 Tested In-Field
- 0 "hits" detected
- Added recent storm pipe, BMPs & As-Built Data
- Completed Re-naming of Storm Structures in New Stormwater Layer in GIS

5

CONSTRUCTION SITE POLLUTANT CONTROL

- 139 Erosion Control Inspections completed in 2025
 - 14 Sites; 10 Sites over 1 Acre
 - Monthly EC Inspections
 - Weekly if there are problems on site
 - Bi-monthly for Inactive Sites
- Try to meet with site project manager at beginning of project, and again if there are concerns or noncompliance issues.




6

POST-CONSTRUCTION STORMWATER MANAGEMENT

- New Stormwater BMPs in 2025:
 - Lumin Terrace – 1 Biofilter
 - Johnsonville – 1 biofilter (retrofit)
 - Hiawatha Street Bioswale
- Issued 12 EC & SW Permits; Completed 17 Plan Reviews & Revisions
- 62 BMPs Inspected
- MetaBMP Inspection & Maintenance Program Pilot Project
- W. Main Street Kwik Trip Swales and Ponds Maintained by Owner




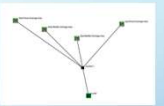



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POLLUTION PREVENTION

- Municipal Operations 2025 Stormwater Data:
 - 232 Lane Miles Swept
 - 450 yards of sediment and debris collected from street sweeper and catch basins
 - 2,996 tons of salt; 5,723 gallons of brine* applied to streets
 - WinSLAMM Training for 3 Engineering Div. Staff
 - SaltWise Training for 6 Parks Staff


*Brine = 77% water, 23% salt mixture

THE CITY OF WATERTOWN

8

TMDL / WATER QUALITY TRADING

- 2022-2025 Total Pollutant Reductions:
 - TSS: 26,791.50 Lbs.; TP: 79.9159 Lbs. Reduced
- Next MS4 Permit TMDL Goals (20%TSS, 10%TP of Remainder)
 - TSS: 65,391 Lbs.
 - Phosphorus: 152 Lbs.



BMP Number	BMP Name	Site Name	NC TSS (lbs)	NC TP (lbs)	Proposed Reduction (%)	Proposed Reduction (lbs)	Percent TSS Reduction	Percent TP Reduction	TP reduction (lbs)
76	Stormwater Practice	James St SW	48.12	0.2012	0.00	0.0000	0.00%	0.00%	0.0000
80	Biofilter	James St SW	28.33	0.0334	9.37	0.0167	54.53%	48.89%	13.17
81	Stormwater Practice	James St SW	12.00	0.0213	0.00	0.0000	0.00%	0.00%	0.0000
82	Biofilter	East Main St SW	5829	38.2528	153.70	17.0000	77.35%	52.26%	5,724.00
84	Stormwater Practice	Washington Park	0.78	0.0044	0.00	0.0000	0.00%	0.00%	0.0000
93	Stormwater Practice	Washington Park	15.48	0.0912	0.00	0.0000	0.00%	0.00%	0.0000
94	Stormwater Practice	Wagon Wheel	98.8	0.4940	242.78	2.4500	12.42%	12.00%	232.80
95	Biofilter	Washington Court	1289	0.4850	134.00	2.8000	10.39%	48.41%	1,540.00
96	Biofilter	Wagon Wheel	98.8	0.4940	242.78	2.4500	12.42%	12.00%	232.80
97	Biofilter	Wagon Wheel - East	133.4	0.4445	144.00	1.0000	48.80%	48.80%	229.74
100	Biofilter	Franklin Highway	1368	2.3818	300.00	1.5750	80.45%	90.00%	1,078.50
106	Biofilter	Franklin Highway	1368	2.3818	300.00	1.5750	80.45%	90.00%	1,078.50
108	Biofilter	Hiawatha St. North	804	1.4840	462.00	0.2025	55.46%	79.26%	367.35
109	Biofilter	Franklin Highway	1368	2.3818	300.00	1.5750	80.45%	90.00%	1,078.50
Total			5703.63	37.2897	4068.48	4.2625			5,573.50
2025 TSS									17,400.00
TSS with MS4 Practice									65,391.00

THE CITY OF WATERTOWN

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COMING IN 2026

- Next 5-year MS4 Permit Anticipated in 2026
 - Develop Plans for Impaired Waterways
 - Updates to Written MS4 Permit Programs
 - IDDE Analysis, Implementation & Protocols
 - Greater Focus on Maintenance: Private & Public BMPs
 - Enhanced SW Pollution Prevention Plans (SWPPPs)
 - Salt Use Reporting, Calibration of Equipment
 - TMDL Reductions: 20% TSS, 10% Phosphorus (of remainder needed to meet TMDL)



THE CITY OF WATERTOWN

10

MEMO

Engineering Division of the Public Works Department

To: Chairperson Arnett and Commission Members
From: Andrew Beyer, P.E., Public Works Director
Date: March 5, 2026
Subject: Public Works Commission Meeting of March 5, 2026

Review and take possible action: Contract with Robert E. Lee & Associates for construction inspection of Rock River Ridge Phases II & III for \$53,650

Background

The Greater Watertown Community Health Foundation is moving forward with Phases II and III of the Rock River Ridge Subdivision located on the City's southwest side. These phases include the construction of public infrastructure improvements that are anticipated to be dedicated to the City upon completion.

The Public Works Department is seeking approval to contract with Robert E. Lee & Associates to perform construction inspection services related to the installation of sanitary sewer and storm sewer within the public right-of-way. City staff are scheduled to perform construction inspection of water main and roadway construction items.

Construction inspection services help ensure that infrastructure improvements intended for public ownership are constructed in accordance with City standards and specifications and perform as intended over the long term.

Quotes were solicited from engineering consultants who have performed construction inspection services for the City in recent years. Based on experience, familiarity with City standards, and availability, the Public Works Department is recommending contracting with Robert E. Lee & Associates of Hobart, Wisconsin for construction inspection services for a total cost of \$53,650.

Budget Goal

2. Proactively maintains and improves our parks and infrastructure to ensure safety, quality, and equity



MEMO

4. Fosters community growth by assessing opportunities, stakeholder input, environmental needs, and modern code and policy priorities
5. Maintains a safe and healthy community, with an eye toward future needs and trends

Financial Impact

The total cost of construction inspection services is \$53,650. Of this amount, \$25,750 will be charged to the Stormwater Utility and \$27,900 will be charged to the Wastewater Utility.

Consistent with discussions during the 2026 budgeting process and current practice in 2026, 50 percent of the inspection costs will be invoiced to the developer as supported by the Development Agreement and City code.

Recommendation

The Public Works Department recommends approving the contract with Robert E. Lee & Associates for construction inspection services related to Rock River Ridge Phases II and III for the amount of \$53,650 with resolution to be forwarded to the Common Council for review and action.

Attachments:

- Robert E. Lee & Associates Fee Proposal

February 16, 2026

Andrew Beyer, P.E., Director of Public Works/City Engineer
CITY OF WATERTOWN
106 Jones Street
Watertown, WI 53094

RE: Request for Proposal (RFP) for Utility Construction Services of the Rock River Ridge, Phases 2 and 3

Dear Mr. Beyer:

Robert E. Lee & Associates, Inc. (REL) is pleased to submit our proposal for utility construction observation services for Rock River Ridge, Phases 2 and 3. We understand the City of Watertown's need for comprehensive observation services during the installation of storm sewer and sanitary sewer mains and laterals. The project will be divided into multiple phases. Phase 1 is sanitary sewer installation (estimated 21 working days, March 5 - April 2, 2026). The second phase is storm sewer installations (estimated 19 working days, April 29 to May 25, 2026). REL will ensure daily observation logs are maintained and that utility infrastructure is installed in compliance with City specifications.

Our assigned lead inspector, Zane Zoromski, brings experience in utility construction observations and will lead the on-site efforts. He will be supported by Ryan Trzinski, P.E., who is available to assist throughout the various phases of the project. Our team will be on-site during contractor working hours (Monday-Friday, 7:00 am to 5:00 pm) to ensure thorough oversight of utility installations. Detailed daily observation logs will be provided to document project progress and address concerns related to the observed utility install.

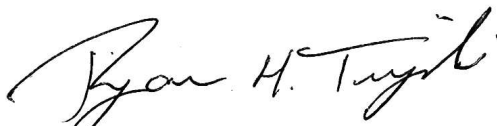
Our approach emphasizes full-time on-site observation and likely coordination with the City for issues that arise. The cost proposal is attached, based on a time and expense format with estimated hours for each phase.

Robert E. Lee & Associates is confident in our ability to provide high-quality inspection services for the Rock River Ridge, Phases 2 and 3, helping ensure adherence to the City's standards and timely completion. We appreciate the opportunity to submit this proposal and are available for any follow-up questions.

Thank you for your consideration.

Sincerely,

ROBERT E. LEE & ASSOCIATES, INC.



Ryan H. Trzinski, P.E.
Construction Services Manager

RHT/JGS/LAR

ENC.



Jared G. Schmidt, P.E.
President



Construction Engineering Technician

Zane Zoromski

Northeast WI Technical College

Associates
Civil Engineering Technician, 2024

Experience and Expertise

Zane is a full-time construction engineering technician who previously interned with our team for two summers. His hands-on experience during his internship coupled with his full-time experience gives him a strong foundation in construction processes and field operations. Zane supports project managers, contractors, and team members by conducting with site observations, interpreting plans, and tracking project progress. He helps document daily job site activities, gathers field data, and assists with tasks such as field orders and change orders. With his growing expertise and familiarity with our workflows, Zane is well-prepared to contribute to the success of any construction projects.

Key Areas of Expertise

2 years of experience specializing in the following areas:

- Resident project representative
- Construction observation services

Roles and Responsibilities

As a Construction Engineering Technician supporting construction administration, Zane plays a key role in monitoring daily site activity to help keep projects on schedule and meeting quality expectations. Building on the knowledge gained during his two summer internships with our team, he regularly visits job sites to observe progress, verify that work aligns with project plans and specifications, and document daily activities such as weather conditions, contractor personnel, and completed tasks.

Zane serves as a communication link between contractors, project managers, and owners, helping ensure everyone stays informed and coordinated throughout the construction process. He assists in interpreting construction drawings, answering contractor questions, and supporting field coordination. Additionally, he helps prepare and process field orders and change orders, tracks quantities for payment, and participates in progress meetings. His familiarity with our processes, combined with his hands-on approach, makes him a valuable contributor to successful project execution and effective problem-solving.

Construction Observation Services

Observe construction for conformance with the plans and specifications. Record construction activities in the daily construction report. Assist the project manager in preparation of field orders, change orders, and contract related correspondence.

- STH 49/11 Resurfacing Project, Waupaca County
- STH 96, Resurfacing Project, Waupaca County
- CTH PP, Resurfacing Project, Brown County
- STH 54, Reconditioning Project, Waupaca County
- Village of Hobart Utility and Streets
- Green Bay Water Utility 2024 & 2025
- City of De Pere Water Reservoir Rehabilitation



Construction Services Manager

Ryan Trzinski, P.E., V.P.

UW - Platteville

Bachelor of Science
Civil Engineering, 2007

Experience and Expertise

Ryan's versatile experience in Highway Transportation Construction and design showcases his versatility and expertise in highway construction.

With a proven construction background, he is able to complete constructability reviews, assist as a construction engineer and offer construction observation and administration guidance to staff, with his long-standing working relationship with the City and understanding of City specifications.

Professional Engineer

Wisconsin, Michigan

Certified Bridge Inspector - WI

Key Areas of Expertise

18 years of experience in Civil Engineering, specializing in the following areas:

- Certified Bridge Inspector
- Complex Traffic Staging
- Rural Roadway Design
- Urban Roadway Design
- Safety Improvement Projects
- Pavement Design
- Construction Project Engineer/Leader
- Agency Permitting for Local, State, Federal
- Recreational Trail Design

Roles and Responsibilities

As the Construction Engineer, Ryan plays a crucial role in construction administration. He utilizes his expertise in sanitary sewer, water main, and storm sewer systems to assist effectively with construction observation. His responsibilities include providing daily reports, tracking daily quantities, and reviewing pay requests. Ryan is proactive in anticipating future work and potential issues, facilitating or collaborating with the City and contractor on work change directives and change orders as necessary. He maintains constant communication with the contractor and City staff to ensure smooth coordination and timely resolution of any concerns.

- Resident project representative
- Construction observation services
- Project management
- City Specification knowledge
- History of working with the City

Main Street Water Utility - Watertown

Ryan led the design engineering for the W Main Street water utility relocation between N Water and 1st Street in anticipation for bridge and approach reconstruction over the Rock River.

- Field Survey
- Utility Coordination
- Water Main / Lateral Design
- Bidding Support

North Forth Street

Ryan led design engineering for this Resurfacing Project; a half-mile urban resurfacing, which included milling existing concrete pavement, placing a pavement geotextile interlayer, and overlaying the HMA.

- Staging Accommodations for Hospital Route
- Public Involvement
- Multiproject Coordination
- Expedited Bil Funded Schedule

Total Project

Investment

Our assigned lead observer is Zane Zoromski, and will be supported by Ryan Trzinski, P.E. who is available to assist through-out the various phases of the project. Our team will be on-site during contractor working hours (Monday-Friday, 7:00 am to 5:00 pm) to ensure thorough oversight of utility installations.

REL proposes to complete these efforts on a time and expense basis at our standard hourly rates, estimated at \$27,900.00 for the Sanitary Sewer Item and \$25,750.00 for the Storm Sewer item.



Phase	Estimated Hours	Staff	Hourly Rate	Estimated Cost*
Sanitary Sewer Main and Lateral Installation	21 days	Zane Zoromski & Ryan Trzinski	\$84 \$175	\$27,900
Storm Sewer Main and Installation	19 days	Zane Zoromski & Ryan Trzinski	\$84 \$175	\$25,750

Base Bid, Est. Cost \$53,650

*Total estimate cost includes labor and expenses.

**ORDINANCE TO
AMEND SECTIONS 446-13, 446-17, 457-6(A), 457-6(B), 457-6(D), 457-6(F),
457-11(B), 457-12(D)(4), 457-17(A), 457-27(E)(1) AND 484-5(B) OF THE
CITY OF WATERTOWN GENERAL ORDINANCES**

**SPONSOR: ALD. TONY ARNETT
FROM: PUBLIC WORKS COMMISSION**

THE COMMON COUNCIL OF THE CITY OF WATERTOWN DOES ORDAIN AS FOLLOWS:

SECTION 1. §446-13 Lead acid batteries, major appliances, waste oil and tires and yard waste of the Watertown Code of Ordinances is hereby amended to read as follows:

§ 446-13 Lead acid batteries, major appliances, waste oil and tires and yard waste.

Occupants of single-family and two-to-four-unit residences, multiple-family dwellings and nonresidential facilities and properties shall manage lead acid batteries, major appliances, waste oil, waste tires and yard waste as follows:

~~A. Lead acid batteries will not be accepted by the City. All lead acid batteries must be taken to area retailers who sell vehicle batteries or to a salvage yard.~~

~~B. Major appliances with compressors will not be accepted by the City. Residents desiring to dispose of refrigerators, freezers, dehumidifiers, or air conditioners must have the freon removed from those appliances by a state-certified contractor before the City will accept the appliance for pickup. If a resident places these items curbside for pickup, all doors on the equipment shall be removed and placed on the side of the equipment to prevent children from being injured by crawling into the equipment. Failure to remove the doors when placing those items on the curb shall be a violation of this Code and shall be punishable by a forfeiture of not less than \$25 nor more than \$100.~~

~~C.A.~~ Waste oil and tires shall not be picked up by the City in its curbside collection system. Waste oil may be delivered by residents to the City's ~~Recycling Center~~ Street/Solid Waste Division in sealed, disposable containers, not larger than one gallon in capacity. ~~These containers will be accepted by the Recycling Center.~~ Residents desiring to dispose of tires shall do so through a tire dealer or private contractor. The City will not accept tires at its ~~Recycling Center~~ Street/Solid Waste Division.

~~D.B.~~ Yard waste consisting of grass clippings, leaves, twigs and branches less than six inches in diameter and under three feet in length and other organic yard materials shall be separated from refuse. Grass clippings, leaves, twigs and branches less than six inches in diameter and under three feet in length and other organic yard materials shall not be collected by the City, except during the fall season when leaves only may be placed out for collection in the tree border in a loose, neatly piled state. The City shall make a dropoff site available to City residents during posted hours from April to December of each year, depending upon weather and temperature conditions, at the discretion of the department head. The City will compost these materials south of the old City

(Type meeting date of the FIRST meeting the ordinance will be considered) Ord. 26-XX

landfill on Boomer Street. All boxes or other containers used by City residents to transport the yard waste shall be removed by said persons immediately after depositing the contents thereof at the dropoff site. Plastic bags only may be disposed of at the dropoff site. If a citizen of Watertown is physically unable to deliver the materials referred to hereinabove at the dropoff site, due to a physical disability or due to lack of transportation, he or she may obtain a yearly permit from the City, to be issued between March 1 and April 1 of each year, at a cost as set by the Common Council and provided under separate fee schedule. The City will grant an exemption when no person in that individual household has a vehicle and/or a driver's license, or if the person presents a certification from his or her personal physician that he or she is unable to transport the material due to physical disability. Application for the exemption permit shall be obtained from the ~~Street Department~~ **Street/Solid Waste Division** and, when returned, shall be granted by the ~~Street Department~~ **Street/Solid Waste Division** after being approved by the ~~Street Superintendent~~ **Public Works Manager** or ~~his~~ **their** designee.

SECTION 2. §446-17 Title to recyclable material of the Watertown Code of Ordinances is hereby amended to read as follows:

§ 446-17 Title to recyclable material.

The title to recyclable material placed curbside for collection by the citizens of the City of Watertown or delivered to the ~~Recycling Center~~ **Street/Solid Waste Division** within the City of Watertown shall transfer to the City of Watertown upon placement. Any person not authorized by the proper City officials to pick up said materials and who removes said materials from the residential recycling bins or from the ~~Recycling Center~~ **Street/Solid Waste Division** shall be guilty of theft and subject to a fine of not less than \$200 nor more than \$500.

SECTION 3. §457-6(A), §457-6(B), §457-6(D) and §457-6(F) Street and sidewalk openings of the Watertown Code of Ordinances is hereby amended to read as follows:

§ 457-6 Street and sidewalk openings.

A. Permit required. No person shall make or cause to be made any excavation or opening in any street, alley, sidewalk or other public way within the City without first obtaining a permit from the ~~Street Superintendent~~ **Public Works Manager of the Street/Solid Waste Division**. All openings to be made in improved streets shall be reported to the ~~Street Superintendent~~ **Public Works Manager** before work is begun. A duplicate copy of the permit shall be deposited with the Public Works Director/City Engineer.

B. Liability insurance required.

(1) No street opening permit shall be issued hereunder until the applicant files with the ~~Street Superintendent~~ **Public Works Manager** public liability insurance written by a company licensed to do business in the State of Wisconsin, for the principal sum of not less than \$1,000,000 bodily injury per occurrence and \$500,000 property damage per occurrence, such policy to be written according to a standard form now in general use. A duplicate of such policy shall be deposited with the Public Works Director/City Engineer. Aggregate coverage for bodily injury and property damage may be in any amount in excess of the per-occurrence amounts set forth hereinabove. Such policy shall have an endorsement protecting the City as its interests may appear as the result of any accident or injury for which the City may otherwise become liable.

D. Backfilling.

(1) All trenches made in any street shall be backfilled with sand or gravel and the old excavated material carried away and disposed of. The material moved in excavating for the trenches may be used to backfill the same if it is a granular material and approval to do so is obtained from the ~~Street Superintendent~~ **Public Works Manager or their designee.**

(2) The plumber, drain layer or contractor shall exercise every possible precaution to compact all backfill in such a manner as prescribed by the ~~Street Superintendent~~ **Public Works Manager or his their** agent so the surface of the ditch or trench after backfilling will not settle in excess of three inches within six months after backfilling. The plumber, drain layer or contractor who did the backfilling shall be liable to the City for any and all costs incurred in restoring the ditch or trench surface to proper grade.

F. Relaying of pavement.

(1) The relaying of street surface following opening hereunder shall be done by the ~~Street Department~~ **Division** or by a contractor under the supervision and control of the ~~Street Superintendent~~ **Public Works Manager** and shall be paid for by the applicant at the time the permit is issued at the rates established by Common Council.

SECTION 4. §457-11(B) Snow and ice removal of the Watertown Code of Ordinances is hereby amended to read as follows:

§ 457-11 Snow and ice removal.

~~B. Street Superintendent to cause removal. If the owner, occupant or person in charge of each lot or part of lot does not remove or cover all snow or ice from the sidewalks abutting the premises owned, occupied or cared for by him, as provided in Subsection A herein, and continues to refuse to remove the snow and ice after a complaint has been issued, and the party is found to be in violation of Subsection A hereinabove, then the Street Superintendent shall cause all sidewalks which shall not have been cleaned or sprinkled with sand, salt or an ice melting compound as above prescribed to be so cleaned or sprinkled in his discretion immediately upon the default of the person whose duty it is to clean or sprinkle the same. He shall annually render to the City Treasurer an itemized report of all sums disbursed by him for such cleaning or sprinkling, the description of the premises affected thereby, and the names of the owners thereof, and the sum so disbursed shall annually be levied and collected as a special tax upon said premises unless paid.~~

B. Removal.

(1) ~~Notice for removal. It shall be the duty of the Police Chief or designee to serve or cause to be served a notice upon the owner, lessee, occupant, or any agent, servant, representative, or employee of any such owner, lessee, or occupant of any premises on which snow or ice are permitted to accumulate in violation of the provisions of this chapter and to demand the abatement of the said snow or ice. Such notice shall be provided by the City through door hangers and a letter mailed to the property owner of record. Notification will only be made on the first violation during the annual snow season (typically November 1 to April 1).~~

(2) ~~Abatement after notice. If the owner, lessee, occupant, or agent so served with a notice of a snow and/or ice removal violation does not abate such in accordance with the notice, the Police Chief or designee may proceed to abate the violation, keeping an account of the expense of the abatement, with such expense being charged to and paid by the property owner. Notice of the bill for abatement of the violation shall be mailed to the owner of the premises and shall be payable~~

within 10 calendar days from receipt thereof. Within 60 days after such costs and expenses are incurred and remain unpaid, the Finance Director shall enter those charges onto the tax roll as a special tax as provided by the State statutes.

SECTION 5. §457-12(D)(4) Use of streets or sidewalks for sale of merchandise of the Watertown Code of Ordinances is hereby amended to read as follows:

§ 457-12 Use of streets or sidewalks for sale of merchandise.

D. (4) The Watertown Street ~~Department~~ **Division** shall provide and remove such barricades and warning devices as are deemed necessary by the Chief of Police to allow for the safe flow of traffic northbound and southbound on the intersecting streets within the Central Business District and to allow for emergency vehicle access to the closed portion of Main Street.

SECTION 6. §457-17(A) Depositing of yard waste in streets or alleys prohibited of the Watertown Code of Ordinances is hereby amended to read as follows:

§ 457-17 Depositing of yard waste in streets or alleys prohibited.

A. The depositing or placement of any type of yard waste, including grass clippings, leaves or tree branches or any other yard waste, in the paved portion of any road or alley is hereby prohibited. The Watertown Public Works Commission shall be responsible for establishing a written policy regarding the enforcement of this section. The Watertown ~~Street Superintendent~~ **Public Works Manager** and Assistant ~~Street Superintendent~~ **Public Works Manager** shall have the authority to issue citations to persons who violate this section.

SECTION 7. §457-27(E)(1) Environmental conditions of the Watertown Code of Ordinances is hereby amended to read as follows:

E. (1) If the sites poses a possible health or safety risk, the City of Watertown Police and Fire Departments shall be notified immediately and the utility shall take the necessary steps to provide for the safety of people and property in the area. After suspended operations, the utility shall contact the offices listed in Table A depending upon the type of conditions discovered.

Table A: Notification Table (Note: Call all that apply)	
Utility Discovers Environmental Condition within City of Watertown Right-of-Way	
Category	Please Call
Archeological Sites or Historic Structure	
Historic structure	State Historic Preservation Office (608) 264-6512
Archeological site	State Historic Preservation Office (608) 264-6507
Burial	Burial Site Preservation Office (608) 264-6507 or (800) 342-7834
Utility project but no City of Watertown project	City of Watertown Street Department Engineering Division (920) 262- 4080 -4060

Table A: Notification Table (Note: Call all that apply)	
Utility Discovers Environmental Condition within City of Watertown Right-of-Way	
Category	Please Call
City of Watertown projects	City of Watertown Street Department Engineering Division (920) 262- 4080 4060
Contaminated Soils, USTs, LUSTs, etc.	City of Watertown Street Department Division (920) 262-4080
Local DNR Office ¹	DNR Regional Office (608) 275-3266 or (608) 266-2621 or (608) 743-4800 or (608) 935-3368
Utility projects but no City of Watertown project	City of Watertown Street Department (920) 262-4080
Department project	City of Watertown Street Department Division (920) 262-4080

¹ Required under Wisconsin law.

SECTION 8. §484-5(B) Tree trimming of the Watertown Code of Ordinances is hereby amended to read as follows:

§ 484-5 Tree trimming.

B. All trees and shrubs on private or public property shall be trimmed and kept trimmed so as to maintain a clear space of not less than ~~12~~ 14 feet above the roadway, except the Public Works Commission may make special provisions in exceptional cases.

SECTION 9. All ordinances or parts of ordinances inconsistent with the provisions of this ordinance are hereby repealed.

SECTION 10. This ordinance shall take effect and be in force the day after its passage and publication.

DATE:	First meeting date		Second meeting date	
READING:	1ST		2ND	
	YES	NO	YES	NO
DAVIS				
LAMPE				
BERG				
BARTZ				
BLANKE				
SMITH				

ADOPTED Type second meeting date

CITY CLERK

APPROVED Type second meeting date

ARNETT				
WETZEL				
MOLDENHAUER				
MAYOR STOCKS				
TOTAL				

MAYOR