



PLANNING COMMISSION

Tuesday, December 10, 2024 at 6:00 PM
HYBRID: Council Chambers & Zoom (details below)

AGENDA

6:00 P.M. CALL TO ORDER & FLAG SALUTE

TOPICS FROM THE FLOOR (Not on Public Hearing Agenda): Limited to five minutes per topic

CONSENT AGENDA

- A. Planning Commission Minutes Dated November 12, 2024

PUBLIC HEARING AGENDA (times are earliest start time)

- B. 6:00 p.m. Conditional Use Permit at 58646 McNulty Way at the intersection of McNulty Way and Gable Road - Columbia Community Mental Health
- C. 6:30 p.m. Conditional Use Permit and Variance (x4) at 480 Wyeth Street - Hatfield
- D. 8:30 p.m. Variance (x2) at S. 6th Street - Scholl

DISCUSSION ITEMS

- E. Planning Commission Vacancy Interviews
 - 7:30 p.m. Reid Hermann
 - 7:50 p.m. Diana Weiner
- F. Planning Department Semi-Annual Report

PLANNING DIRECTOR DECISIONS (previously e-mailed to the Commission)

- G. Temporary Sign Permit at 2100 Block of Columbia Blvd - Merchants Toy N Joy
- H. Site Development Review (Scenic Resource) at 405 Riverside Drive - Matt Olson
- I. Conditional Use Permit (Minor) at 1601 Railroad Avenue - Michael Russell
- J. Extension of Time for SUB.1.22 at the current northern termini of N 8th, 9th, and 10th Streets lying north of Deer Island Road; also Tract A and Lot 1 of the Elks Subdivision located along Madrona Court; Tax Assessor Map No. 5N1W-33-00700 and 5N1W-33AD-1900 & 1901 - North 8th Street, LLC
- K. Sign Permit (x2) at 58791 S Columbia River Hwy - ProntoSigns, LLC
- L. Temporary Use Permit at 305 S Columbia River Hwy - Breslin Properties

PLANNING DEPARTMENT ACTIVITY REPORT

- M. Planning Department Activity Report - November

PROACTIVE ITEMS

- N. Architectural Standards

- O. Vacant Storefronts
- P. The Plaza Square

FOR YOUR INFORMATION ITEMS

ADJOURNMENT

NEXT REGULAR MEETING: January 14, 2024

VIRTUAL MEETING DETAILS

Join:

<https://us06web.zoom.us/j/86132911987?pwd=su3eIENkKJ4odtfXzwnocRUg9WXAj.1>

Meeting ID: 861 3291 1987

Passcode: 973867

Dial by your location: +1 253 215 8782 US (Tacoma)

The meeting location is accessible to persons with disabilities. A request for an interpreter for the hearing impaired or for other accommodations for persons with disabilities should be made at least 48 hours before the meeting to City Hall at 503-397-6272.

Be a part of the vision and get involved...volunteer for a City Board or Commission! For more information or for an application, go to www.sthelensoregon.gov or call 503-366-8217.



PLANNING COMMISSION

Tuesday, November 12, 2024, at 6:00 PM

DRAFT MINUTES

- Members Present:** Chair Dan Cary
 Vice Chair Jennifer Shoemaker
 Commissioner Scott Jacobson
 Commissioner Ginny Carlson
 Commissioner David Rosengard
 Commissioner Brooke Sisco
- Members Absent:** Commissioner Charles Castner
- Staff Present:** City Planner Jacob Graichen
 Associate Planner Jenny Dimsho
 Community Development Admin Assistant Christina Sullivan
 City Councilor Mark Gunderson
- Others:** Eric Green
 Cheryl Morrissey
 Vicky Njust
 Heidi Green
 Lucas Green
 Mark Bailets
 Steve Toschi
 Robyn Toschi
 Sheri Melling
 Catherine Ross
 Monica Cade
 Travis Brenner
 Viola Bailets

CALL TO ORDER & FLAG SALUTE

TOPICS FROM THE FLOOR (Not on Public Hearing Agenda): Limited to five minutes per topic

There were no topics from the floor.

CONSENT AGENDA

A. Planning Commission Minutes Dated October 08, 2024

Motion: Upon Vice Chair Shoemaker's motion and Commissioner Jacobson's second, the Planning Commission unanimously approved the Draft Minutes dated October 08, 2024, as written. [AYES: Vice Chair Shoemaker, Commissioner Jacobson, Commissioner Carlson, Commissioner Sisco, Commissioner Rosengard; NAYS: None]

PUBLIC HEARING AGENDA (times are earliest start time)

B. 6:05 p.m. Amendments to the St. Helens Development Code (City of St. Helens)

Chair Dan Cary opened the Public Hearing at 6:01 p.m. There were no ex-parte contacts, conflicts of interest, or bias in this matter.

Steve Toschi said he objected to all the members on the Planning Commission to make a fair decision based on City of St. Helens Resolution Number 1986. He said it prevented the body from being able to engage as a normal decision-making body.

The Planning Commission did not agree that any of them were incapable of making a fair decision, so they moved forward.

City Planner Jacob Graichen shared the staff report dated November 4, 2024. He said there were new bills from the state that required Development Code amendments. He also did some cleaning up of the current code to make it more efficient. He shared this would cover five years of legislation. He mentioned the Planning Commission had met every month since June to discuss the Development Code amendments. He said it was divided into two parts, and they had already met and approved three portions of legislation.

The portion they would discuss for approval would be for four other parts. He started with the Housing Needs Analysis. He said the state of Oregon requires certain cities to have a Housing Needs Analysis that examines the future housing needs in a twenty-year period and whether the city has adequate land available to accomplish it.

He said this was part two of a code amendment that had been started in 2021 and part of the analysis said there should be code around allowing cottage cluster type housing. This means there would be multiple detached homes allowed on a single parcel. He shared that the first part of the multiple dwellings on a single parcel were covered in the amendments made in 2021. He shared the state had implemented legislation that says duplexes could be built with the same rules as a single-family dwelling unit on a single parcel. He shared what two attached and detached dwellings, or a duplex, looked like on a single parcel. He mentioned this new amendment would expand the detached housing on a single parcel for three or more dwelling units.

He said there was another House Bill 4064 regarding Single Room Occupancies (SROs). He said this would be an individual room that would have a shared lavatory or one shared kitchen in the whole house. He said this is not a dwelling unit, but a space for someone to have a lockable room to sleep and keep their items safe for independent use. He said this was not something mandated, but he would like to have tools to help guide when there is a customer who comes in to discuss developing an SRO. He said this means if there are no rules in place, they would be required to use the rules the state allows. He said in a standard single-family dwelling or duplex zone area, this rule would allow up to six SROs in a building. If it is a zoning district that allows apartments, then it would be comparable to the density that allows apartments. This application would still require a land use application such as a Site Design Review where the neighbors are notified, and certain things can be imposed, including parking.

He said there was also a new law from the state that said they had to allow manufactured and prefabricated structures allowed within the city limits. He said there are a few different types of manufactured homes, and it depends on the age. He said all these mobile homes would need to be allowed in city limits but only within mobile home parks. He also mentioned with this new law, single-wide manufactured homes would have to be allowed within city limits. However, he mentioned this type of housing would not be allowed in or adjacent to a Historic District or adjacent to a Historic Landmark.

Vice Chair Jennifer Shoemaker asked about the SRO and if it was just considered a room that could be rented out of a home. Graichen said yes, that was part of the definition. She also asked if there was a clear definition on Cottage Clusters. She was concerned about someone coming in and setting up four sheds on a parcel. Graichen said they would be required to comply with the Building Code and sheds would not comply. She also asked if they had the authority to implement Architectural Standards on this type of housing. Graichen said it would be the same as what is imposed on single-family dwellings.

In Favor

Melling, Sheri. Melling was called to speak. She said she was in favor of these new rules around SROs because she thought it would provide affordable living opportunities for vulnerable populations. She said this would help those who have experienced homelessness or have financial instability to have an opportunity to be sheltered with a more affordable and more accessible place that would provide stability and community living. She said it could also serve as a bridge to a more permanent option.

In Neutral

Ross, Catherine. Ross was called to speak. She was concerned about how tiny homes would be regulated. She also wanted to be sure there would be no increase to their property taxes or affect their property values.

Njust, Vicky. Njust was called to speak. She was concerned that the Cottage Clusters would be built in some of the residential zone districts. She wanted to be sure that the multiple detached single units would be smaller in size and not row homes or apartments. She also wanted to be sure that all easements for properties were met and that the rules for single-family dwellings would apply to this type of housing. She also expressed concern about the parking that would be allowed for this type of housing. She wanted to be sure that the increased number of vehicles did not impact her neighborhood. She also expressed concern about the sewer issue the City is facing and wanted to be sure these would not impact the sewer capacity issues.

Bailets, Mark. Bailets was called to speak. He said that he wanted to see this cottage cluster type of housing be a more permanent structure, including a foundation and connections to city utilities.

Green, Eric. Green was called to speak. He shared his concern over parking and making sure there would still be a development plan for making the parking work in different districts. He said the new rules were taking out the words "catering to motorists," and he wanted to be sure there would still be language that would help when developing a property. He also asked how long it would take to implement these new codes. Graichen said there was a process, but it would likely be by the first of the year.

Bailets, Viola. Bailets was called to speak. She wanted to ask about the size of the lots that would allow multiple units. She wanted to be sure that the houses would fit in with the appropriately sized lots and that there would be enough parking.

Cade, Monica. Cade was called to speak. She asked if the rules would apply only to properties in City Limits. Graichen told her yes. She mentioned that she was right on the outskirts of the City and there were a lot of larger properties that surround her. She wanted to be sure that if those properties were to annex in and wanted to develop this type of housing, she wanted to have the ability to oppose those applications. Graichen said yes, these new rules would make it clear that a land use application would be required with neighbor notice.

In Opposition

Morrissey, Cheryl. Morrissey called to speak. Morrissey did not like the idea of SRO type of housing. She wanted to express concern about potential duplexes or apartments being built in her neighborhood.

She asked if each development was taken case-by-case or if there are just generalized rules that allow them to build what they want wherever. She was concerned about the narrow streets, and the neighborhoods that could not handle more density housing. Graichen said SROs would still be allowed in these neighborhoods, but these new rules would allow for some guidance when these are being developed. She did not agree and said there are neighborhoods that this should not be allowed.

Brenner, Travis. Brenner was called to speak. He was not in support of more density housing brought into his neighborhood due to safety reasons. He felt low-income housing would become an issue with SROs and he was concerned that this would add unsafe occupants to his neighborhood.

Toschi, Steve. Toschi was called to speak. He said he was opposed to the allowance of more high-density housing in the City. He said he believes it invites crime into our neighborhoods. He also said he believed it would cause property values to drop. He expressed concern that there were commercial operations being run in residential neighborhoods for housing of those with mental illnesses. He did not want to see more houses in residential neighborhoods become commercial operations for the homeless populations.

Commissioner David Rosengard said he personally was a citizen who struggled with mental illness and shared that there was no evidence that residents with mental health issues were more likely to be perpetrators of crime. He said that they are more likely to be victims of those crimes. He also said that this community has never expressed that those struggling with these issues were not worth interacting with or talking to. He said this was not reflective of St. Helens or this community's values.

Rebuttal

There was no rebuttal.

End of Oral Testimony

Close of Public Hearing & Record

Deliberations

Commissioner Brooke Sisco mentioned there should be clear and concise code and rules on what is appropriate and not appropriate for SROs. It gives the Planning Department and Commission the ability to help those who are developing projects around this type of housing.

Commissioner Shoemaker said she also agreed there should be guidelines and tools for our community to be able to follow once these state rules are imposed. She also expressed that she would like to see architectural standards applied to the cottage cluster type housing.

There was a small discussion on the necessity of having these tools in place.

There was a discussion on adding SRO definition into the residential zoning districts R7 and R10 to be consistent with the other residential zones.

Motion: Upon Commissioner Sisco's motion and Commissioner Rosengard's second, the Planning Commission unanimously recommended approval to the City Council of the amendments as written by staff with the modifications as described. [AYES: Vice Chair Shoemaker, Commissioner Jacobson, Commissioner Carlson, Commissioner Rosengard, Commissioner Sisco; NAYS: None]

DISCUSSION ITEMS

C. Planning Commission Term Expirations

Graichen reminded the Commission that Chair Cary would be leaving after the December meeting. He also reminded the Commission that they wanted to discuss the attendance before considering the reappointment of Commissioner Rosengard after local elections.

There was a discussion about attendance and the importance of being at meetings in person. Commissioner Rosengard explained why he has missed several of the meetings.

The Commission agreed that Commissioner Rosengard had a valid reason for being absent and wanted to vote to continue his service to the Commission for another term.

Motion: Upon Commissioner Carlson's motion and Commissioner Sisco's second, the Planning Commission unanimously approved the renewal of Commissioner Rosengard term. [AYES: Vice Chair Shoemaker, Commissioner Jacobson, Commissioner Carlson, Commissioner Sisco; NAYS: None]

There was a discussion about whether there should be two commissioners who were related or in the same household on the same commission. The Commission agreed that it was not appropriate for members of the same household or those related to each other should be able to serve on the Planning Commission.

The Planning Commission agreed they had a sufficient interview pool of candidates (three people) and agreed to move forward with a committee. Commissioner Jacobson and Vice Chair Shoemaker volunteered to be on the interview committee with City Councilor Mark Gunderson and staff.

D. 2025 Public Meeting Schedule

Graichen shared the meeting schedule for 2025. He said the only date that would need to be changed is November 11, 2025 because it is Veterans Day. They agreed to move the meeting from November 11, 2025, to November 12, 2025.

There was also a discussion about moving the start time from 6:00 p.m. to 6:30 p.m. This time change would start in January when the new meetings calendar was released.

Motion: Upon Vice Chair Shoemaker's motion and Commissioner Jacobson's second, the Planning Commission unanimously approved to change the start time of the meeting to 6:30 p.m. for 2025. [AYES: Vice Chair Shoemaker, Commissioner Jacobson, Commissioner Carlson, Commissioner Rosengard, Commissioner Sisco; NAYS: None]

PLANNING DEPARTMENT ACTIVITY REPORT

E. Planning Department Activity Report – October

There was no discussion on the Planning Department Activity Report.

PLANNING DIRECTOR DECISIONS (previously e-mailed to the Commission)

- F. Site Design Review at 1465 Columbia Blvd – Riverside Community Outreach
- G. Site Design Review at Property lying at the SE corner of the US30/Howard Street intersection; Block 3 of the Georgetown Subdivision. – Sunset Development, LLC
- H. Lot Line Adjustment (x2) at 1300 Kaster Road. Lots 3 and 4, Block 5, Hawley Addition and Parcel 1 of Partition Plat No. 2020-03
- I. Site Design Review (Amended) at property lying at the SE Corner of the US30/Howard Street intersection; Block 3 of the Georgetown Subdivision
- J. Site Permit (x2) at 745 S Columbia River Hwy – Pacific Stars, LLC
- K. Sign Permit at 299 S Vernonia Road – O'Reilly Auto Parts

There was no discussion on the Planning Director Decisions.

PROACTIVE ITEMS

- L. Architectural Standards
- M. Vacant Storefronts
- N. The Plaza Square

There was no discussion on the Proactive Items.

FOR YOUR INFORMATION ITEMS

Vice Chair Shoemaker shared she was excited to see the new sign being handled at the O'Reilly Auto Parts store. Former commissioner Coen visited, acted as chair, and the other commissioners proceeded to sit upon him. Commission Cary noted he will miss being chair for this reason.

ADJOURNMENT

There being no further business before the Planning Commission, the meeting was adjourned at 8:50 p.m.

Respectfully submitted,

*Christina Sullivan
Community Development Administrative Assistant*

**CITY OF ST. HELENS PLANNING DEPARTMENT
STAFF REPORT**

Conditional Use Permit CUP.1.24

DATE: December 2, 2024
TO: Planning Commission
FROM: Jacob A. Graichen, AICP, City Planner

APPLICANT: AKAAN Architecture + Design, LLC
OWNER: Columbia Community Mental Health (CCMH)

ZONING: General Commercial, GC
LOCATION: 58646 McNulty Way
PROPOSAL: Construct new building for office, intermediate care facility, residential facility, detoxification center and crisis center uses and associated private and public improvements on the existing Columbia Community Mental Health (CCMH) campus

SITE INFORMATION / BACKGROUND

The subject property abuts Gable Road and McNulty Way. McNulty Creek cuts through the southeast portion of the property. There is extensive permitting history:

- 2003** – CCMH received approval with County file (DR 03-09) to develop a 4.1-acre site with a 26,766 sq. ft. main CCMH building. County building permits approved in 2004.
- 2005** – 4.1-acre, original CCMH site, is annexed into the city.
- 2016** – CCMH receives approval with SDRm.5.16 & SL.3.16 for a parking lot expansion partially in the McNulty Way right-of-way, and in close proximity to McNulty Creek. This is associated with the original 4.1 ac. CCMH site within city limits.
- 2017** – CCMH receives approval with County file DR 17-03 to build a 2,505 sq. ft. modular building on a 5.67-acre site (adjacent to the original 4.1 ac. CCMH site) and convert an existing 1,200 sq. ft. detached single-family dwelling into supporting office.
- 2018** – CCMH receives approval with County file DR 18-07 to build two additional modular offices at 1,440 sq. ft. and 560 sq. ft. on the 5.67-acre site.
- 2019** – As part of the County’s DR 18-07 approval, the 5.67-acre lot was annexed into the city based on connected to city water (file A.2.19 and Ordinance No. 3245). Now the entire CCMH campus is within city limits, not just the original 4.1-acre portion annexed in 2005.
- 2023** – The site’s zoning is changed from Light Industrial to General Commercial (file CPZA.1.23 and Ordinance No. 3297) with a vehicle trip cap.
- 2024** – Earlier this year the applicant proactively worked with the city to combine the multiple parcels that make up the subject property today (instrument 2024-1054). So, this does not need to be a condition of this proposal, being already done.

Also, the Planning Commission approved an access variance (file V.7.24) for an access related to a shop on the east side of the subject property and an unlisted use application (file U.1.24) to recognize “intermediate care facility” as a conditional use on the GC zone.

The main/original campus building is connected to city sewer with a private pump station and pressurized system (STEP system). All other structures utilize on-site septic systems. All structures are connected to city water.

PUBLIC HEARING & NOTICE

Public hearing before the Planning Commission: September 10, 2024. The applicant requested the hearing be continued to October 8, 2024 at 6:05pm. The Commission approved a motion to continue the hearing to this date and time at the September hearing. There was no other discussion at the September hearing. At the October 8, 2024 hearing applicant agreed and the Commission continued the matter of Conditional Use Permit CUP.1.24 to a date and time uncertain, so that an expanded application (received September 30, 2024) could be considered with notice published and sent based on the new application. The public hearing for the expanded application before the Planning Commission is December 10, 2024.

Notice of the original proposal was sent to surrounding property owners within 300 feet of the subject property(ies) on August 22, 2024 via first class mail. Notice was sent to agencies by mail or e-mail on the same date. Notice of the updated proposal was sent to surrounding property owners and agencies on November 19, 2024.

Notice of the original was published on August 28, 2024 in The Chronicle newspaper. Notice of the updated proposal was published in the Columbia County Spotlight newspaper on November 29, 2024. The reason for the change on newspapers is The Chronicle closed for business in September.

APPLICATION COMPLETENESS

This application was originally received on August 6, 2024. Staff identified missing information or other aspects that rendered the application incomplete and notified the applicant of the issue pursuant to SHMC 17.24.050 on August 14, 2024. The applicant provided revised or new information and the application was deemed complete on August 23, 2024. The applicant provided a letter dated August 23, 2024 asking for a hearing in October instead of September and noted that this requested delay will not count towards the 120-day rule per ORS 227.178.

In order to continue the Conditional Use Permit with an updated (expanded scope from the original) application, the applicant verbally agreed to waive the 120-day rule per ORS 227.178 at the October 8, 2024 Planning Commission hearing. This was provided in writing on October 23, 2024.

Based on the updated application, staff again identified missing information or other aspects that rendered the application incomplete and notified the applicant on October 14, 2024. The applicant provided revised or new information for the updated application on November 6, 2024.

AGENCY REFERRALS & COMMENTS

City Engineering: See Engineering staff report ([attached](#)).

Columbia County On-site Wastewater Program: See letter dated September 23, 2024 ([attached](#)). Note that the address referenced in that memo—2195 Gable Road—was eliminated in 2019.

Columbia County Land Development Services Director: Per communication about a recent county decision for the adjacent property on the east side, germane to this decision given access aspects, the County’s LDS director noted deficiencies in recent development (from c. 2017-2018) related to the modular buildings and converted dwelling on the CCMH site. Note this development predates annexation of this portion of the CCMH campus site, which occurred in 2019. Thus, all inspections and final approvals were under the County’s jurisdiction.



Left: Photo courtesy of Columbia County LDS Director. The Director’s observation:

As for building N, while it does have a sign on the building with gravel parking it’s in an unrealistic location, as it would be parked behind other parked vehicles.

City staff notes that the plan indicating existing parking here is incorrect and should be labeled as proposed parking.



Left: Photo courtesy of Columbia County LDS Director. The Director’s comment:

CCMH portables with ramp but no ADA parking.

Note that there is a ramp behind Building H and between buildings E and H.



Left: Photo taken by city staff on the opposite side of Building H, where the accessible space is located.

The nearest wheelchair ramp is behind the building and not visible in this photo.

APPLICABLE CRITERIA, ANALYSIS & FINDINGS

Zoning Compliance: The site is zoned General Commercial, GC. The proposed uses of the proposed building are office / intermediate care facility and residential facility (the detox center portion of the proposal).

Office is permitted use and a predominant use of the CCMH campus (subject property). The Planning Commission acknowledged intermediate care facility as a conditional use in the GC zone (see file Unlisted Use U.1.24) at their October 2024 meeting. Residential facility is a listed conditional use. Thus, this CUP application.

As defined in the Development Code “intermediate care facility” means a facility which provides, on a regular basis, health-related care and services to individuals who do not require the degree of care and treatment which a hospital or skilled nursing facility is designated to provide, but who, because of their mental or physical condition, require care and services (above the level of room and board) which can be made available to them only through institutional facilities such as these.

“Residential facility” (applicable to the detox center aspect of this proposal) means a residential care, residential training or residential treatment facility, as defined in and licensed by the Department of Human Services under ORS 443.400 through 443.455 or ORS 418.205 through 418.327, that provides residential care alone or in conjunction with treatment or training or a combination thereof for six to 16 individuals who need not be related. Staff persons required to meet licensing requirements shall not be counted in the number of facility residents, and need not be related to each other or to any resident of the residential facility.

The proposed building is generally <20’ in height, which is within the 45’ maximum allowed.

No outdoor storage is proposed, and the site is well within the maximum 90 percent impervious surface and minimum 10 percent landscaping thresholds.

* * *

Sensitive Lands: The site abuts/contains some of Milton Creek along its southwest side. There is associated riparian area/upland protection zone (Chapter 17.40 SHMC) and floodplain (Chapter 17.46 SHMC) within the subject property, but proposed improvements stay clear of those areas.

There is a wetland area on the northwest side of the property, but it is not “significant” to the city subjecting it to the regulations of the city per Chapter 17.40 SHMC. Its is not in the area of development within the site but could be impacted by improvements to Gable Road.

Wetland Land Use Notification sent to Oregon DSL on September 4, 2024 as required by ORS 227.350.

For any proposed wetland impacts, proof that the impacts are allowed from the applicable agency (DSL and/or USACOE) shall be provided to the city.

* * *

Landscaping/buffering/screening: **Street trees will be required.** The development includes street improvements and has more than 100’ of abutting street frontage. This pertains to Gable Road as the McNulty Way frontage was improved previously.

There are overhead utility lines along the portion of Gable Road that fronts the subject, thus, street trees need to be “small” per this chapter. This requires a 20’ spacing.

Tree location shall also comply with requirements per 17.72.035(2)(d)-(l). And, the city has allowed the “landscape” portion of street improvements to be behind the sidewalk when abutting a wetland, which the plans show.

Street frontage improvements details will be worked out further with civil plans for construction. This is discussed further below.

This chapter requires buffering. Adjacent properties along the east and south sides have a Comprehensive Plan designation of Light Industrial and would be zoned such is annexed. A 10’ buffer is required between commercial uses and light industrial zones. The more than 150’ distance from the building to the east and south property lines satisfies this.

This chapter requires screening. This applies in this case as follows:

Because the parking lot expansion will be greater than three spaces, it is required to be screened. For screening in this case, the city usually requires landscaping along the perimeter that includes

a balance of low lying and vertical shrubbery and trees. No actual landscape plans provided indicating species and such. This shall be required on final plans.

Service facilities and equipment (e.g., HVAC and other mechanical unit) visible from a public street, customer or residential parking area, any public facility or residential area are required to be screened whether they are ground, wall or roof mounted. In addition, rooftop facilities and equipment are required to be screened from street and adjacent properties.

The applicant acknowledges this but provides no details. There is no mention of other facilities. Screening required in all cases.

Refuse container or collection area are required to be screened (e.g., trash enclosure). The applicant mentions trash enclosure but provides no details. Required on final plans.

Interior parking lot landscaping. When off-street parking lots have more than 20 spaces, landscape islands are required with trees. This applies to the new parking proposed as the site has more than 20 spaces. Rows of parking spaces are not to exceed 7 spaces, generally. The “islands” are required to be no less than 48 square feet in area and no dimension less than six feet. They are required to have a combination of groundcover and shrubs in addition to a tree, such that at least 50% of the island will be covered with living plants. They are also required to be protected from vehicular damage by some form of wheel guard or curb that is permanently fixed to the ground.

This is generally shown, but final plans will need further details such as landscaping.

* * *

Visual Clearance: Chapter 17.76 SHMC requires proper sight distances at intersections to reduce traffic hazard potential. The required area to maintain clear vision is greater for arterial streets.

No conflicts observed.

* * *

Off-Street Parking/Loading: For the proposed use, as a mix of offices and sleeping rooms for displaced individuals who will be supervised by CCMH staff, the best fit of parking category is SHMC 17.80.030(1)(d):

(d) Group care – One space per three residential beds plus one space for each employee on largest shift.

Per the applicant for “Phase 1” as identified on the plans, the mix of offices and sleeping rooms will range from 12:8 to 4:16. With each office considered an employee count and each sleeping room as a bed count, this equates to a range of **15 to 10 off-street parking spaces required for “Phase 1.”**

“Phase 2” as identified on the plans include a detox center and crisis center component. The detox center includes 10 beds and two offices, plus a nurse room. This contributes up to 13 off-street parking spaces. The crisis center can serve up to 8 individuals with five additional rooms for assessment or staff offices. At most, this contributes another 13 off-street parking spaces. **Thus, “phase 2” can include as many as 26 off-street parking spaces.**

In total, new off-street parking required based on the proposed use is as many as 36-41 spaces.

Based on the permitting history noted above, the five existing buildings (excluding the maintenance shop on the east side) combined is 32,471 square feet. Using the office category, one space for every 350 square feet, this comes to 102 required parking spaces for the existing use. Applicant notes different numbers and parking requirement basis for this proposal, but in any case, approximately **149 spaces are proposed** and the total off-street parking provided will exceed the minimum requirement.

There appears to be some discrepancy between existing spaces identified on the plans and those actually marked. Spaces that are not designated shall not be included on the plans and count towards the final parking county unless they are proposed to be marked and do not conflict with past approvals. But this number of spaces is not anticipated to reduce the total amount below the minimum required.



Left: Photo taken September 27, 2024 of the modular buildings south of the proposed building. Parking spaces as striped (or not striped such as the parallel spaces) conflict with the plans provided. This conflict continued on updated plans received November 6, 2024. Existing and proposed spaces need to be properly identified. The parallel spaces in front of Building H are not marked and if intended to be should be identified as *proposed*. Also, there are three marked spaces in front of Building B whose length is perpendicular to the front of the building as opposed to being five existing spaces parallel with the front of the building as indicated on the plans.

Dimension and type. All proposed spaces are standard size and meet the normal dimensional requirements.

Location. Parking spaces are required to be within 200 feet of the building served. The the new spaces are.

Accessible (disabled person) spaces. Required to comply with State and Federal Standards.

Also, accessible parking spaces are required to be located on the shortest route to an accessible pedestrian entrance. Though this is a building code issue, it is relevant to site design.

Two accessible spaces are proposed close to the proposed new building.

Bicycle parking. 1 lockable space is required at a rate of 10% of vehicle spaces, with fractions counting as whole spaces. Bicycle spaces are required to be within 50' of primary entrances, under cover when possible, and not located in parking aisles, landscape areas, or pedestrian ways.

Four spaces shall be included on the plans meeting these specifications. They shall be under cover.

Aisle width. Minimum aisle width for two-way vehicular circulation is 24 feet.

This is met for the access aisle proposed now, but a conceptual internal connection to the eastern portion of the property is not. Final plans shall only indicate what is proposed now.

Markings. All interior drives and access aisles are required to be marked and signed to indicate direction flow. This will be require for the final plans.

Surface area. All areas used for parking, storage or maneuvering of vehicles (including things towed by vehicles) shall be paved.

Wheel stops. Wheel stops are required along the boundaries of a parking lot, adjacent to interior landscape area, and along pedestrian ways. Curbing and wheel stops are shown.

Drainage. Drainage plans will be required to prevent ponding, prevent water flow across pedestrian ways and to address pollutants from vehicles (e.g., oil/water separation). The comments from the City's Engineering Manager details this further.

Lighting. Required to be directed to avoid glare from surrounding residences and roads/streets. No lighting plan or information has been provided. Lighting plan will be required.

* * *

Access/egress/circulation:

Public street access. All vehicular access and egress per Chapter 17.84 SHMC is required to directly connect to a public or private street approved by the city for public use. Moreover, vehicular access is required to be within 50' of primary ground floor entrances.

The site abuts the following streets:

Street/Road Name	Public or Private	Street Class (TSP)	Jurisdiction	Improved?
McNulty Way	Public	Collector	City of St. Helens	Yes
Gable Road	Public	Minor Arterial	City of St. Helens	partial; no frontage improvements

The site utilizes these streets for access and brings vehicle access within the statutory distance of the primary entrance.

The portion of the CCMH campus along McNulty Way is not changing or otherwise impacted. Access along Gable Road is discussed in greater detail above for the Access Variance.

Pedestrian access (interior walkways). Walkways shall extend from the ground floor entrances or from the ground floor landing of stairs, ramps, or elevators of all commercial, institutional, and industrial uses, to the streets which provide the required access and egress. Walkways shall provide convenient connections between buildings in multibuilding commercial, institutional, and industrial complexes. Walkways also shall provide access to existing and planned transit stops adjacent to the development site. Unless impractical, walkways should be constructed between a new development and neighboring developments.

The floorplan of the proposed building shows ground floor entrances on most sides, and the plans include a walkway scheme that connects all the new building external doors to the proposed Gable Road sidewalk. Note that the segment of walkway behind the proposed building (east side) utilizes walkway around Building H to get to Gable Road.

Wherever required walkways cross vehicle access driveways or parking lots, such crossings shall be designed and located for pedestrian safety. Required walkways shall be physically separated from motor vehicle traffic and parking by either a minimum six-inch vertical separation (curbed) or a minimum three-foot horizontal separation, except that pedestrian crossings of traffic aisles are permitted for distances no greater than 36 feet if appropriate landscaping, pavement markings, or contrasting pavement materials are used. Walkways shall be a minimum of four feet in width, exclusive of vehicle overhangs and obstructions such as mailboxes, benches, bicycle racks, and sign posts, and shall be in compliance with ADA standards.

An approximate 30' crossing is proposed by Building H.

Walkways on plans appear to meet minimum width. All standards apply.

Required walkways shall be paved with hard-surfaced materials such as concrete, asphalt, stone, brick, etc. Walkways shall be required to be lighted and/or signed as needed for safety purposes. Soft-surfaced public use pathways may be provided only if such pathways are provided in addition to required pathways.

Walkways are proposed to be concrete. Lighting plan will be necessary.

Access requirements based on type and intensity of use. With a parking lot exceeding 100 spaces, at least two access points are required. Two existing accesses servicing the campus will continue to be used for this proposal.

* * *

Signs: No signs are proposed at this time. New signs will require permits per Chapter 17.88 SHMC.

* * *

Solid Waste/Recyclables: Chapter 17.92 SHMC includes provisions for functional and adequate space for on-site storage and efficient collection of mixed solid waste and recyclables subject to pick up and removal by haulers.

This is not addressed in any detail. Both this Chapter and 17.72 include applicable details.

* * *

Site Development Review:

Buildings are required to be located to preserve existing trees and such. Per Chapter 17.96 SHMC trees with a 6" or greater dbh require preservation or replacement.

The existing conditions plan sheet C0.05 identifies trees but provides no species, size or other details to enable a proper honest finding in this regard. Updated tree plan is necessary.

Crime prevention. Lighting plans will be needed. Staff observed that the poles used for overhead power within the boundary of the site include cobra-head lights, which will help, unless they are undergrounded.

The proposed building has good window coverage on all four sides for site observance and crime deterrence.

Potential future development shown on the plans is not approved by this proposal. For example, anything labeled as "possible future," "future...if modulars removed," and "future" in the application materials originally provided require future approval pursuant to SHMC 17.96.070, 17.96.080, 17.100.050 and/or 17.100.060. Final plans for this proposal shall omit any "future" details and only include what is propose now.

* * *

Conditional Use: Pursuant to SHMC 17.100.040:

(1) The planning commission shall approve, approve with conditions, or deny an application for a conditional use or to enlarge or alter a conditional use based on findings of fact with respect to each of the following criteria:

- (a) The site size and dimensions provide adequate area for the needs of the proposed use;
- (b) The characteristics of the site are suitable for the proposed use considering size, shape, location, topography, and natural features;
- (c) All required public facilities have adequate capacity to serve the proposal;
- (d) The applicable requirements of the zoning district are met except as modified by this chapter;
- (e) The supplementary requirements set forth in Chapter 17.88 SHMC, Signs; and Chapter 17.96 SHMC, Site Development Review, if applicable, are met; and
- (f) The use will comply with the applicable policies of the comprehensive plan.

The CUP approval criteria are for the Commission's consideration.

SHMC 17.100.150 has additional requirements for certain conditional use types. The proposal does not include any of these.

SHMC 17.100.040(3) provides "condition of approval guidance" as follows:

- (3) The planning commission may impose conditions on its approval of a conditional use, which it finds are necessary to ensure the use is compatible with other use in the vicinity. These conditions may include, but are not limited to, the following:
 - (a) Limiting the hours, days, place, and manner of operation;
 - (b) Requiring design features which minimize environmental impacts such as noise, vibration, air pollution, glare, odor, and dust;
 - (c) Requiring additional setback areas, lot area, or lot depth or width;
 - (d) Limiting the building height, size or lot coverage, or location on the site;
 - (e) Designating the size, number, location, and design of vehicle access points;
 - (f) Requiring street right-of-way to be dedicated and the street to be improved;
 - (g) Requiring landscaping, screening, drainage and surfacing of parking and loading areas;
 - (h) Limiting the number, size, location, height, and lighting of signs;
 - (i) Limiting or setting standards for the location and intensity of outdoor lighting;
 - (j) Requiring berming, screening or landscaping and the establishment of standards for their installation and maintenance;
 - (k) Requiring and designating the size, height, location, and materials for fences; and
 - (l) Requiring the protection and preservation of existing trees, soils, vegetation, watercourses, habitat areas, and drainage areas.

These are for the Commission's consideration.

Because the off-street parking area is being updated, the Commission should consider requiring updates to all deficient accessible off-street parking spaces and routes to buildings built, placed or modified on the subject property after the original main building was built.

Building letters have emergency response implications and the new building will need letters approved by the city and Fire Marshall. Applicant's plans do not include these letters, despite being critical for site description and emergency response purposes.

* * *

Tree Removal/Preservation: Chapter 17.132 SHMC addresses the preservation of trees with a diameter at breast height (DBH) >12 inches. Protection is preferred over removal per this Chapter and Site Development Review Chapter 17.96 SHMC.

The site is heavily wooded along McNulty Creek along the south side, within/around a wetland area on the northwest side and a grove on the east side, with other scattered trees within the site. Given the quantity on the entire site is basis for the required tree plan.

A tree plan was submitted, though it does not provide much information and lacks arborist input. The plan calls out a redwood tree, but provides scant detail as to other trees within the subject property potentially impacted by development of Gable Road, storm water pond reconfiguration, and proposed utility lines, for example. Based on staff's observations, there are oak trees with a dbh >12" that could be impacted and many trees with white survey tape. Does this survey tape indicate removal? If so, this is not reflected on the plans provided with this application. Amongst other things, the tree plan is supposed to identify the location, size, DBH and species of all existing trees. It is also supposed to identify all trees which are to be removed. There is no minimum or maximum size for such inventorying. Proper tree plan shall be required at least for all trees on the eastern more-or-less half of the site.



Above: Photos of the tree grove on the eastern side of the site. Many trees are marked with white survey tape. The tree plan provided with the applicant provided insufficient detail, such as species, size and fate (i.e., what does the white ribbon mean?).

A protection program by an arborist defining the standards and methods that will be used to protect the existing trees to be preserved is required. This shall be on or with the Building Permit plan set to ensure contractors and others follow the tree protection plan during site development.

Street trees may count as replacement trees.

* * *

Street/Right-of-Way Standards: Gable Road is underdeveloped and the current design standard for the section abutting the subject property is based on the 2019 City's Riverfront Connector

Plan (Ord. 3241), which is a refinement of the City’s 2011 Transportation Systems Plan (Ord. No. 3150 and 3181).

The applicant acknowledges the benefits and need of these improvements by noting these improvements in its narrative and plans. A deviation is being allowed due to the existing wetland at the NW corner of the site. Existing trees may be used as street trees, and this will be the case along the wetland, they will just be behind the other frontage improvements instead of being a green strip between the non-vehicular lanes and vehicle travel lanes. This city has allowed this exception elsewhere such as for the Forest Trail Subdivision along Sykes Road and the new Middle School along N. 16th Streets.

The Riverfront Connector Plan calls for a 14’ wide turn lane or median on Gable Road and how this applies for this proposal has been an discussion point. After receiving comments and testimony on the matter, city staff supports the entire frontage be a 2-way left hand turn lane at this time, for this proposal. See attached Recommended Street Section 4.2 from the 2019 Riverfront Connector Plan and Exhibit “Evaluation of accesses on north side of Gable Road.”

Most accesses have been determined along the north side of Gable Road, except for an approximate 10-acre vacant property directly across Gable Road from most the CCMH property. Though accesses are anticipated to be limited and restricted due to the arterial status of Gable Road, there is much question as to the location of the access(es) allowed when that property develops. Staff believed it is best to utilize the 2-way left hand turn lane now, to be reevaluated for more prescribed turn lanes and/or medians when that property develops. Because of its industrial zoning and size, it is anticipated that the scale of development will warrant street frontage improvements with potentially greater “median improvements.”

* * *

Utility Standards:

Water: The site is served by city water.

Storm Sewer: As per the City’s Engineering Manager, in addition to storm water plans, a 1200-C permit could be required. Storm water plans and other related permitting to be approved and in place prior to building permit issuance.

Sanitary Sewer: Sanitary sewer is more complicated due to a mix of on-site system and city sanitary sewer connection and current limitations of the city’s sanitary sewer infrastructure.

The comments from both the City Engineering Manager and Columbia County Environmental Services Specialist include the sanitary sewer issue.

There is on-site system infrastructure providing sanitary sewer to the area proposed to be developed, with potential challenges and limitations.

Basically, the original portion of the CCMH campus (the 4.1-acre parcel annexed in 2005) utilizes a Septic Tank Effluent Pumping (STEP) system and the remainder (5.67-acres annexed in 2019) into the city on a private on-site system. Per the applicant the intent is to upgrade the STEP system for the new development and abandon the provide on-site system.

Generally, sanitary sewer is not available elsewhere, other then via the STEP system that directs sanitary sewer northwesterly to McNulty Way and Gable Road. There is potential to extend the Sanitary Sewer within McNulty Way about 170' from the subject property, but this would require a crossing of Milton Creek and the extension would not benefit any other properties along McNulty Way. The sanitary sewer within the Gable Road right-of-way is too shallow to extend. Thus, the expansion of the STEP system as the proposed method of sanitary sewer service for the entire site.

Final plans will need to detail existing and proposed STEP system and private on-site system infrastructure and both city and county review and approvals will be necessary for the sanitary sewer service aspects of this proposal.

As it pertains to the current limitations of the city's sanitary sewer infrastructure, the city adopted a new **Wastewater Master Plan (WWMP)** in November 2021 that identifies undersized trunk lines already operating at or above capacity that this development would depend on. The WWMP can be found here:

<https://www.sthelensoregon.gov/engineering/page/public-infrastructure-master-plans>

Sewer pipes are considered "at capacity" when peak flows exceed 85% of the full depth of the pipe in accordance with industry standards. This depth is based on the maximum depth of flow ratio (d/D), where "d" is the depth of flow and "D" is the pipe diameter. The WWMP includes an exhibit—Figure 18—that shows a potential overflow point at Kaster Road and a portion of sewer main by the wastewater treatment plant operating at 0.85-0.99 capacity. This is above 85% and thus above capacity.

Pipeline surcharging occurs as flows exceed the capacity of a full pipe, causing wastewater to back up into manholes and services. In addition to potentially backing up into homes and health risks associated with sanitary sewer overflows, Oregon DEQ prohibits all sanitary sewer overflows and can fine cities for allowing such and has done so to other jurisdictions. Examples of DEQ fines can be found here:

<https://www.oregon.gov/deq/Pages/enforcement-actions.aspx>

Given this issue, SHMC 17.152.090(4) must be considered:

Permits Denied. Development permits may be restricted by the commission or council (i.e., the applicable approval authority) where a deficiency exists in the existing sewer system or portion thereof which cannot be rectified within the development and which if not rectified will result in a threat to public health or safety, surcharging of existing mains, or violations of state or federal standards pertaining to operation of the sewage treatment system.

There is a current deficiency (undersized pipes for existing demand) of a widespread scale within the city per the WWMP including infrastructure this development would need to utilize that could result in surcharging, fines (e.g., for violation of Oregon DEQ standards) and public health risks.

The Commission finds this development can still be approved under these circumstances given this criterion based on the following findings or conditions of approval:

- The deficient conveyance infrastructure this development depends on for sanitary sewer is a priority 1 in the WWMP. Priority rankings include three categories. There is no priority 2 conveyance improvements. The difference between priority 1 and 3, is priority 1 includes areas that have been reported to have overflows or significant surcharging during wet weather events, whereas priority 3 areas are where there have been infrequent or no observations of historical overflows or surcharging.
- City Public Works and Engineering staff have already begun to address the necessary sanitary sewer infrastructure upgrades having already received a State Revolving Fund Program loan (for below market rate loans) from Oregon DEQ to fund both priority 1 projects (in basins 4 and 5) and priority 3 projects in basin 6. This city has also acquired CDBG grant funds for design/engineering. Basin 5 is applicable to this proposal. It is anticipated that these improvements will be constructed in 2027.
- A condition of approval to require a fee per equivalent dwelling unit will be included. This is not a System Development Charge pursuant to ORS 223.299(4)(b); it is a temporary charge by order for development and land divisions proposed under these circumstances until the infrastructure is in order per the WWMP. The nexus is clear as it relates to the sewer conveyance deficiency and an amount has been determined based on calculations to determine fair proportionality—see attached **St. Helens Wastewater Collection System New Sewer Connection Surcharge memo**.

For this project, the fee per equivalent dwelling unit is \$3,200, and this estimated amount is determined to be a fair share quantity for this proposal. It is based on October 2022 dollars, and inflation must be considered.

Other: Code required utilities to be underground. There is overhead utility along the Gable Road right-of-way abutting the subject property and into the property from Gable Road.

This is shown on the existing conditions but is inconsistent on the plans (e.g., absent on sheet SP-1). This detail will be required on all final plans.

The overhead utility infrastructure within the site may remain provided no additional poles are needed. It was approved previously (preexisting) before being within city limits and the poles are also used for site lighting.

Per CRPUD, power needed by CCMH may result in new lines crossing Gable Road. There are currently two lines starting from a pole on the opposite of Gable Road than the subject property,

going into the property. With increased power, this could multiply to four lines. Unless overhead power along Gable Road is undergrounded, requiring this to be undergrounded is not necessary, provided no new poles are needed.

* * *

Traffic Impact Analysis: A traffic impact analysis for the purpose of determining traffic impacts is not warranted. However, in 2023 when the site's zoning was changed from Light Industrial to General Commercial (file CPZA.1.23 and Ordinance No. 3297), a trip cap was established as follows:

Any development and/or redevelopment of the subject property shall not trigger more than 700 daily trips within the TSP planning horizon of 2031 (as adopted in 2011 with Ord. No. 3150).

This is the first development/redevelopment proposal since the adoption of Ordinance No. 3150 on November 15, 2030. The applicant provided a memo dated November 5, 2024 from Jennifer Danziger, PE, of Lancaster Mobley, identifying the trip generation of this proposal to be 108. This leaves 592 daily vehicle trips for future development.

* * * * *

CONCLUSION & RECOMMENDATION

Staff recommends approval of this Conditional Use Permit with the following conditions:

1. This **Conditional Use Permit** approval is valid for a limited time (to establish the use) pursuant to SHMC 17.100.030. It is valid for 1.5 years. A 1-year extension is possible but requires an application and fee. If the approval is not vested within the initial 1.5 year period or an extension (if approved), this is no longer valid and a new application would be required if the proposal is still desired.
2. The following shall be required prior to any development or building permit issuance:
 - a. Final plans as submitted with any development or building permit(s) shall comply with the plans submitted with this CUP/U/VAR with the following additions and/or corrections:
 - i. Final landscaping plans including all necessary details. New parking areas shall be "screened" along the perimeter that using a balance of low lying and vertical shrubbery and trees.
 - ii. As per condition 4.
 - iii. Refuse collection area in accordance with the applicable provisions of Chapters 17.72 and 17.92 SHMC.
 - iv. There appears to be some discrepancy between existing spaces identified on the plans and those actually marked. Existing and proposed spaces shall be properly identified based on actual existing conditions. For example:

- (1) Three striped spaces including an accessible space in front of Building N are not existing parking spaces in this gravel area and this conflicts with the existing conditions plan sheet C0.50.
 - (2) Two parallel spaces in front of Building H are not existing parking spaces. There is no evidence such marking existed. These are not identified on the existing conditions plan sheet C0.50.
 - (3) There are three marked spaces in front of Building B. The site plan incorrectly identifies five existing spaces contrary to actual existing conditions and existing conditions plan sheet C0.50.
- v. Four lockable bicycle spaces within 50' of a primary, under cover, and not located in parking aisles, landscape areas, or pedestrian ways.
 - vi. All interior drives and access aisles are required to be marked and signed to indicate direction flow.
 - vii. Lighting plan illuminating walkways and areas vulnerable to crime such as but not limited to the new/modified parking areas. This shall include whether or not overhead power will be undergrounded and how that impacts existing poles within the site used for lighting.
 - viii. Types of materials used, such as asphalt and concrete shall be identified.
 - ix. A tree plan shall be required pursuant to Chapter 17.132 SHMC for all trees as defined by this Chapter on the eastern more-or-less half of the site providing all necessary information including species, size and whether or not it will be kept. This plan shall both address the tree preservation standards of Chapters 17.96 and 17.132 SHMC. A protection program by a certified arborist defining the standards and methods that will be used to protect the existing trees to be preserved is required. This shall be on or with the Building Permit plan set to ensure contractors and others follow the tree protection plan during site development. It appears there are several trees within the subject property that could be impacted by buildings, paving, utility lines, storm water improvements, Gable Road improvements, etc.
 - x. Building identification letters shall be included on final plans. City and Fire Marshall approval required for letter assignments of new buildings.
 - xi. As per condition 10.
 - xii. **Because the off-street parking area is being updated, the Commission should consider requiring updates to all deficient accessible off-street parking spaces and routes to buildings built, placed or modified on the subject property after the original main building (on the 4.1 ac. parcel) was built.**
- b. Engineering construction plans shall be submitted for review and approval addressing all public improvements including but not limited to:
 - i. This shall include public right-of-way street frontage improvements to Gable Road, and all other public infrastructure proposed or required, such as those related to sanitary sewer. For the center 14' portion of the recommended street section for Gable Road (Segment 4.2) per the Riverfront Connector Plan, city staff supports the entire frontage be a 2-way left hand turn lane at this time.

- ii. All street trees shall be “small” per Chapter 17.72 SHMC due to overhead utility along Gable Road. Tree location shall also comply with requirements per 17.72.035(2)(d)-(l).
 - iii. A drainage and stormwater plan by a certified by a registered professional engineer shall be submitted that addresses any increase in runoff from the site and how the potential impacts will be mitigated.
- c. Before any wetland impacts, proof that the impacts are allowed from the applicable agency (DSL and/or USACOE) shall be provided to the city.
 - d. DEQ 1200-C permit as required by city engineering.
 - e. An additional “fair share” fee shall be paid per equivalent dwelling unit (EDU) based on the portions of the city wastewater collection system between the subject property and the wastewater treatment plant, that this development depends on, that are at or above capacity as identified in the 2021 Wastewater Master Plan. Estimated per EDU cost is \$3,200 based on October 2022 dollars. Inflation adjustment to value at time of building permit issuance shall be included.
 - f. Need approval from Columbia County Land Development Services as it pertains to their jurisdiction over the method of sanitary sewer service and what will happen to existing facilities.
3. The following shall be required **prior to** Certificate of Occupancy by the City Building Official:
 - a. All improvements necessary to address the requirements herein, and in accordance with approved plans, shall be in place.
 - b. Need approval from Columbia County Land Development Services as it pertains to their jurisdiction over the method of sanitary sewer service and what is required for existing facilities such as abandonment.
 - c. Areas where natural vegetation has been removed, and that are not covered by approved landscaping, shall be replanted pursuant to SHMC 17.72.120.
 4. Any new service facilities such as gas meters and air conditioners which would otherwise be visible from a public street, customer or resident parking area, any public facility or any residential area shall be screened, **regardless if such screening is absent on any plan reviewed by the City. This includes but is not limited to ground mounted, roof mounted or building mounted units.** See SHMC 17.72.110(2).
 5. Disabled person parking space(s) shall comply with local, State, and Federal standards.
 6. Any requirement of the Fire Marshall as it applies to this proposal shall be met.
 7. Any new utilities shall be underground. Existing overhead utilities may remain above ground provided there are no new poles.
 8. Owner/Developer shall be solely responsible for obtaining all approvals, permits, licenses, and authorizations from the responsible Federal, State and local authorities, or other entities,

necessary to perform land clearing, construction and improvement of the subject property in the location and manner contemplated by Owner/Developer. City has no duty, responsibility or liability for requesting, obtaining, ensuring, or verifying Owner/Developer compliance with the applicable State and Federal agency permit or other approval requirements. This land use approval shall not be interpreted as a waiver, modification, or grant of any State or Federal agency or other permits or authorizations.

9. No plan submitted to the City for approval shall contradict another. For example, separate plans from the architect and engineer shall not conflict.
10. Potential or any conceptual future development shown on the plans is not approved by this proposal and shall not be on final plans. For example, anything labeled as “possible future,” “future...if modulars removed,” and “future...” in the application materials originally provided are not approved.
11. Owner/applicant and their successors are still responsible to comply with the City Development Code (SHMC Title 17).

Attachment(s):

- Excerpt from Riverfront Connector Plan, Recommended Street Section Segment 4.2
- CUP.1.24 Exhibit: Evaluation of accesses on north side of Gable Road
- St. Helens Wastewater Collection System New Sewer Connection Surcharge memo (excerpts: pgs. 1-6, 13, and 25-26)
- City Engineering staff report
- Letter from Columbia County Land Development Services
- Applicant’s narrative dated November 5, 2024
- Trip generation memo dated November 5, 2024
- Letter from JJJ Earth Movers, LLC dated October 7, 2024
- Letter from Suzie Dahl on behalf of Leila Wilson dated October 7, 2024
- Plans as received November 6, 2024

Note wetland information in record, but not added as an attachment to this report.

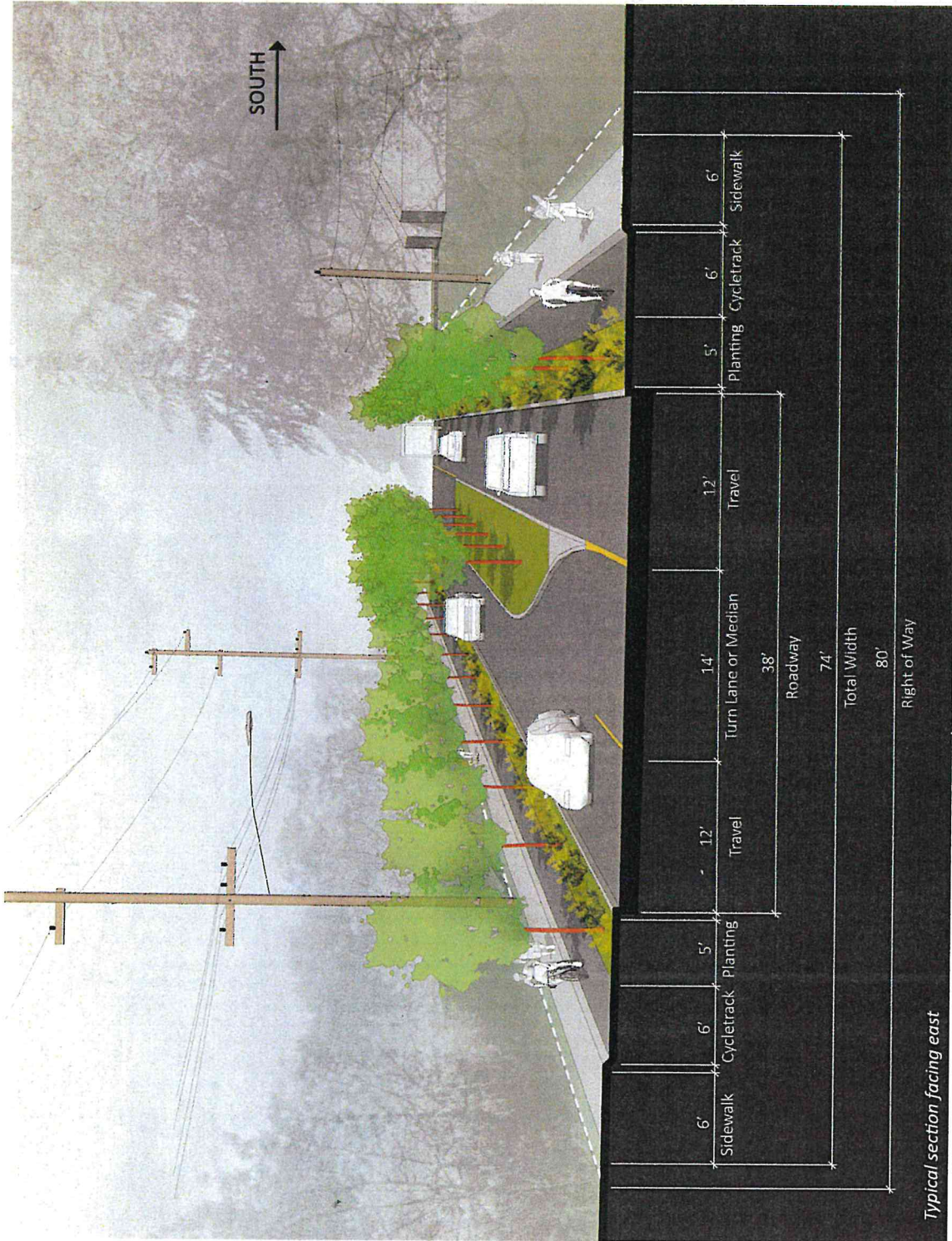
Recommended Street Section

A new preferred option was identified through further discussion and review with members of the advisory committee, Planning Commission, City Council and other community members. It includes one-way cycletracks on both sides of the street.

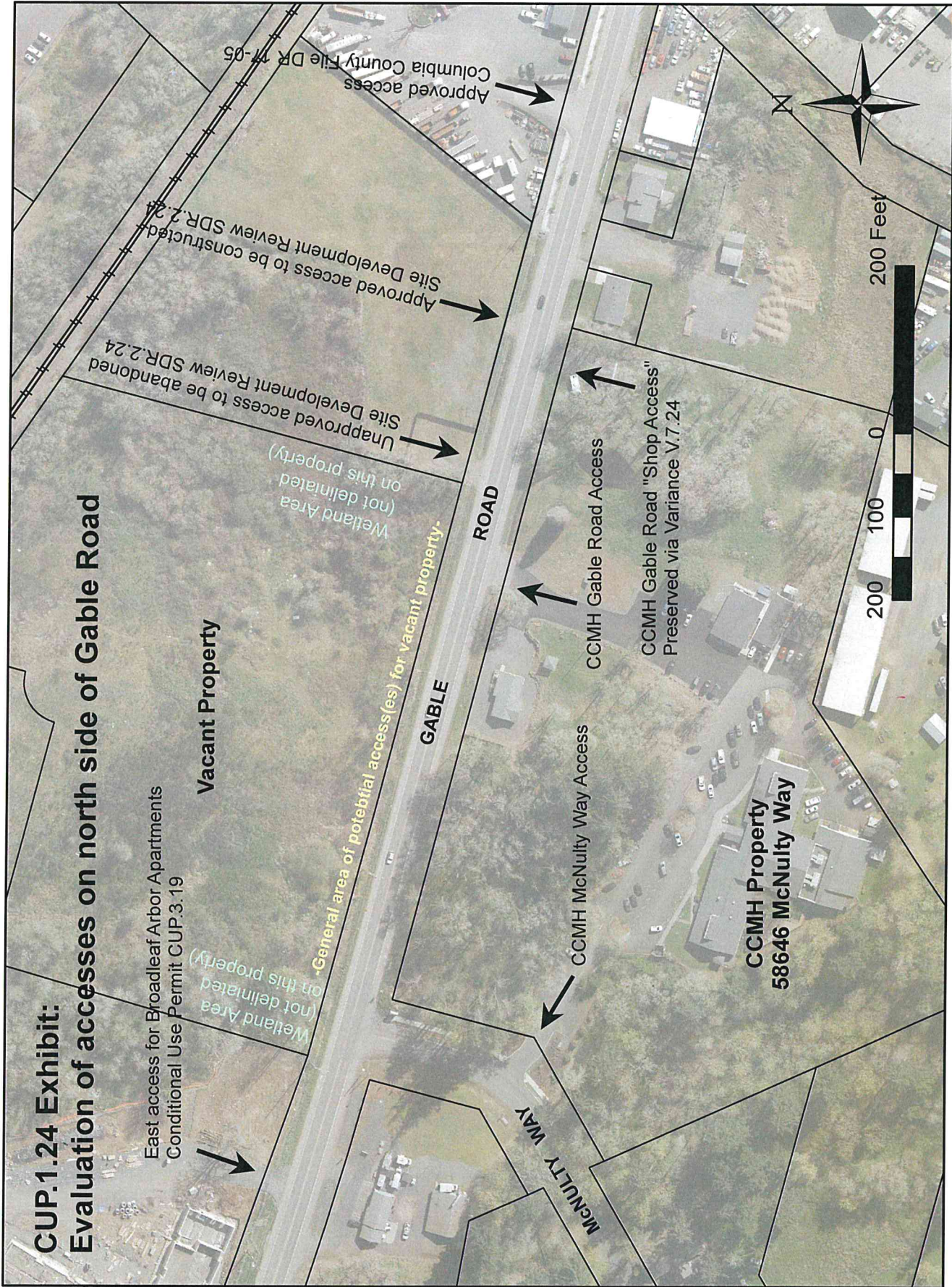
Each cycletrack is separated from the roadway by a landscaping strip, and each will be separated from the adjacent pedestrian walkway by a curb or other means. The new option has the same safety benefits of the original Option C but can be transitioned to adjacent segments more effectively.

Implementation of this option will require careful planning to allow for safe, convenient transitions between this design and parts of the roadway that have undergone recent improvements. Typical transitions are illustrated in the conceptual intersection designs in this document. The final design will also need to ensure that intersections and access points address bicycle and pedestrian safety and minimize conflicts between bicyclists, pedestrians and motor vehicles. Examples of treatments for similar facilities are found in Appendix 8.

This cross-section also includes a center lane to accommodate turn lanes needed at key intersections or other access points to maintain traffic mobility. A landscaped median could be used in short portions of this segment but will need to be located to continue to ensure access to local businesses in this part of the corridor.



SEGMENT 4.2: GABLE ROAD





St. Helens Wastewater Collection System New Sewer Connection Surcharge

*EXCERPT FOR
CUD.1.24 et. al.
PAGES: 1-6, 13, & 25-26*

December 1, 2022
Revision 01

CITY OF ST. HELENS
265 STRAND STREET | ST. HELENS, OREGON 97051
503.397.6272 | WWW.STHELENSOREGON.GOV

TABLE OF CONTENTS

SECTION 1 – BACKGROUND	3
1.1 Wastewater Masterplan 2021 Update	3
1.2 New Development Sewer Surcharge	4
SECTION 2 – ST. HELENS SEWER TRUNKLINE BASINS.....	5
2.1 Sanitary Sewer Trunk Basins Methodology.....	5
2.2 Allendale Sewer Basin	7
2.3 Diversion Trunk.....	8
2.4 Firlock Sewer Basin.....	9
2.5 Gable Sewer Basin	10
2.6 The Interceptor	11
2.7 Matzen Sewer Basin.....	12
2.8 McNulty Sewer Basin	13
2.9 Middle Trunk Sewer Basin	14
2.10 Millard – OPR Sewer Basin	15
2.11 North 11th Sewer Basin	16
2.12 North Willamette Sewer Basin.....	17
2.13 Pittsburg Sewer Basin.....	18
2.14 Port Sewer Basin.....	19
2.15 South Trunk Sewer Basin	20
2.16 Southwest Sewer Basin	21
2.17 Sunset Sewer Basin	22
2.18 Sykes Sewer Basin	23
2.19 Vernonia Road Sewer Basin.....	24
SECTION 3 – SEWER SURCHARGE CHART.....	25
SECTION 4 – EQUIVALENT DWELLING UNIT CONVERSION	26

SECTION 1 – BACKGROUND

1.1 Wastewater Masterplan 2021 Update

The City of St. Helens provides sanitary sewer collection services to businesses and residences within the City limits. The sanitary sewer collection system is a combination of over 60 miles of gravity and force mains, 9 lift stations, and over 1,700 sanitary sewer manholes, vaults, and cleanouts. The sewer pipes in the City range from 6-inches to 48-inches in diameter, with the majority of the pipes being 8-inch. All sewage flows are conveyed to the City's wastewater treatment facility.

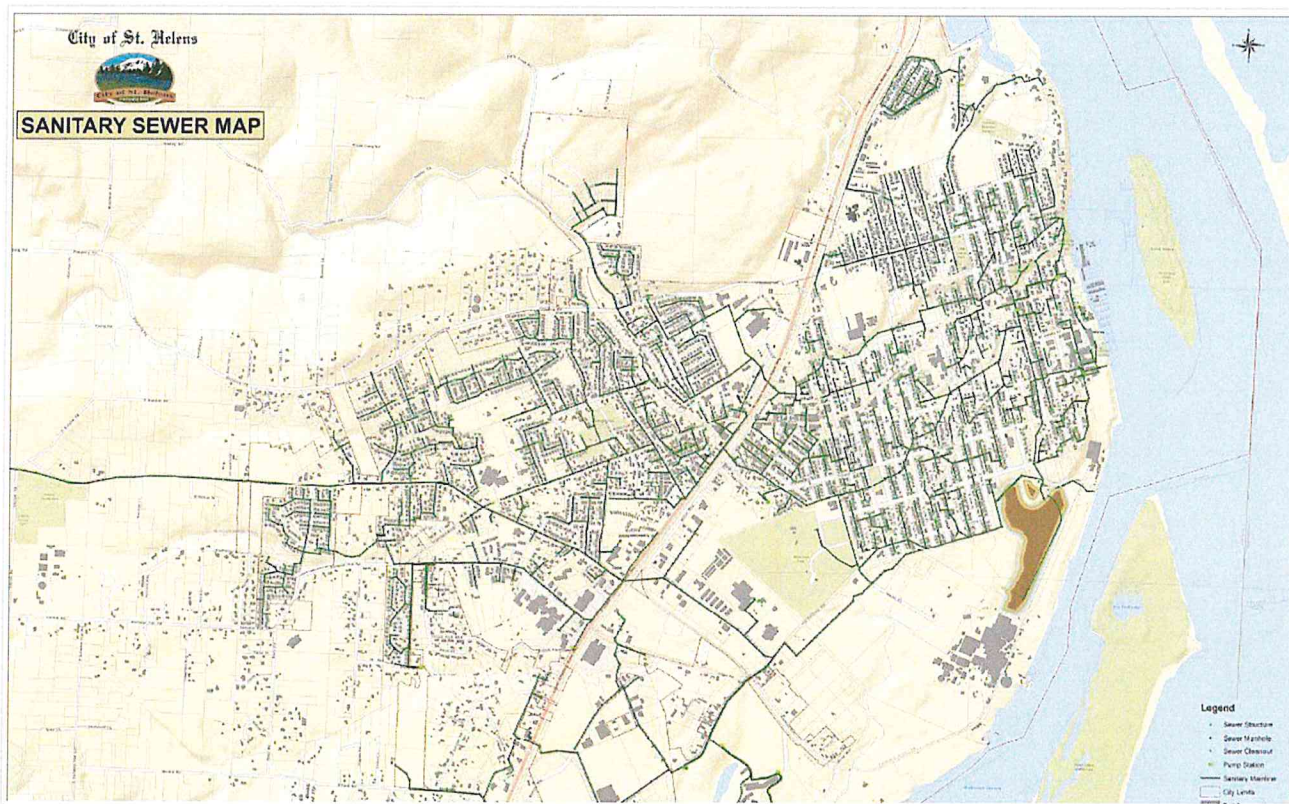


Figure 1.1.A St. Helens Sanitary Sewer Map

On November 17, 2021, the St. Helens City Council adopted the updated Wastewater Master Plan (WWMP) under Resolution No. 1940. This update to the City's WWMP is the first complete study done on the entire sewer collection system since 1989. The population was 7,500 at the time. Since then, the population of St. Helens has grown to over 14,500 – almost double. With this added population, more load is added to the public sewer system. Meanwhile, the size of the sewers have not been increased.

After 33 years of growth, the WWMP revealed that the majority of the City's sewer trunklines are at operating at or above capacity. This means that the greater portion of the City's public sewer system is inadequate to serve a growing population. Without

increasing the sizes of the trunklines, there is an increased risk of sanitary sewer overflows in the collection system.

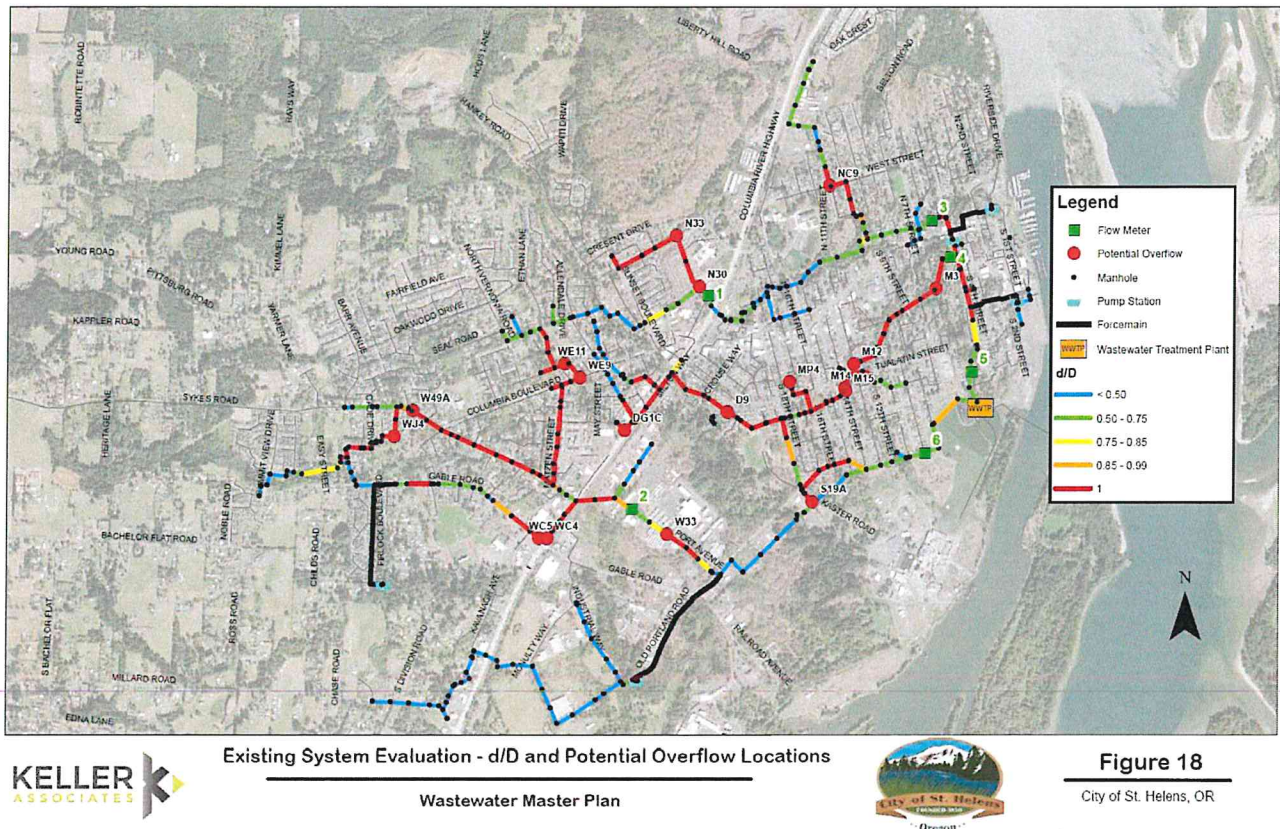


Figure 1.1.B Existing Sanitary Sewer Evaluation Map (2021 WWMP)

1.2 New Development Sewer Surchage

To assess the impacts of future development on the public sewer system and how the City could pay for the costly capital improvements identified in the WWMP, Keller Associates performed an assessment of a sewer charge based on the shared of costs that new upstream Equivalent Dwelling Units (EDUs), as identified in the 2019 Housing Needs Analysis, would pay to complete the downstream CIP improvements along trunk lines that convey their sewage flows. The costs per EDU were based on the CIP project costs broken down by trunkline.

This sewer surcharge assessed per EDU is to fund capacity upgrades to the public sewer system and will be levied on those properties and developments requiring connection to the sewer trunklines identified in the 2021 WWMP update as "at or above" capacity. These fees will allow the City to recover a fair portion of the infrastructure improvements made by the City to accommodate new users and be used solely for public sewer capacity improvements. Equivalent Dwelling Units conversion details for sewer charges for multifamily dwellings, commercial, and industrial land uses may be found in Section 4 – EQUIVALENT DWELLING UNIT CONVERSION.

SECTION 2 – ST. HELENS SEWER TRUNKLINE BASINS

2.1 Sanitary Sewer Trunk Basins Methodology

Sewer basin delineations by trunk lines were created to aid in the proper assessment of the sewer surcharge to ensure costs reflect the actual share of costs that new upstream EDUs, as identified in the 2019 Housing Needs Analysis, would pay based on the downstream sanitary sewer capital improvements along the trunk lines the flows for their property would flow through.

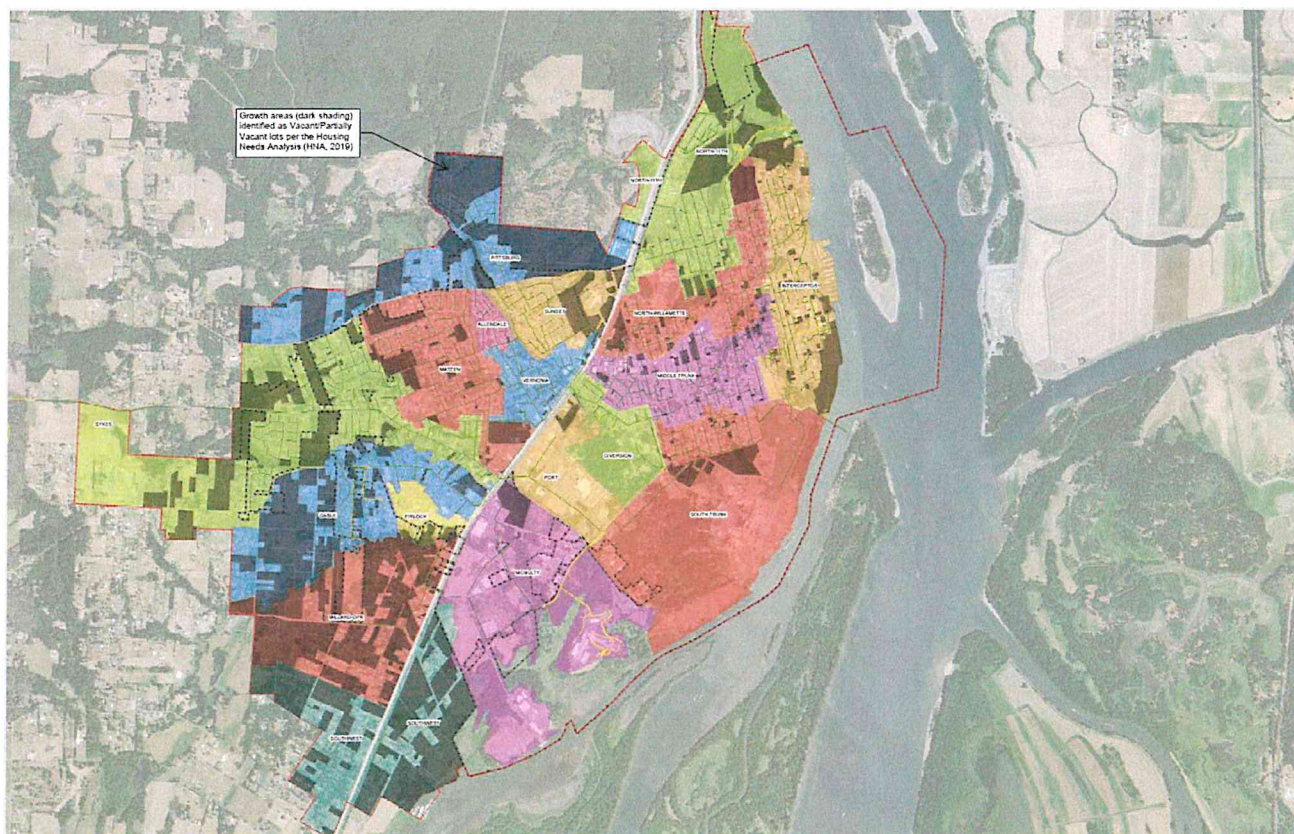


Figure 2.1.A St. Helens Sanitary Sewer Trunkline Basin Delineations

The delineation of CIP projects was simplified and where major portions of a Capital Improvement Project (CIP) spanned more than one basin, projects were split by basin. Basin delineation generally reflects existing conditions, except the Pittsburg basin, which is largely undeveloped and is anticipated to discharge to the North-11th basin.

Costs were calculated by summing CIP costs in and downstream of a basin and summing the EDUs in and upstream of the basin. The downstream CIP costs are then divided by the upstream EDUs. A sewer surcharge cap of \$15,000 per EDU is assumed.

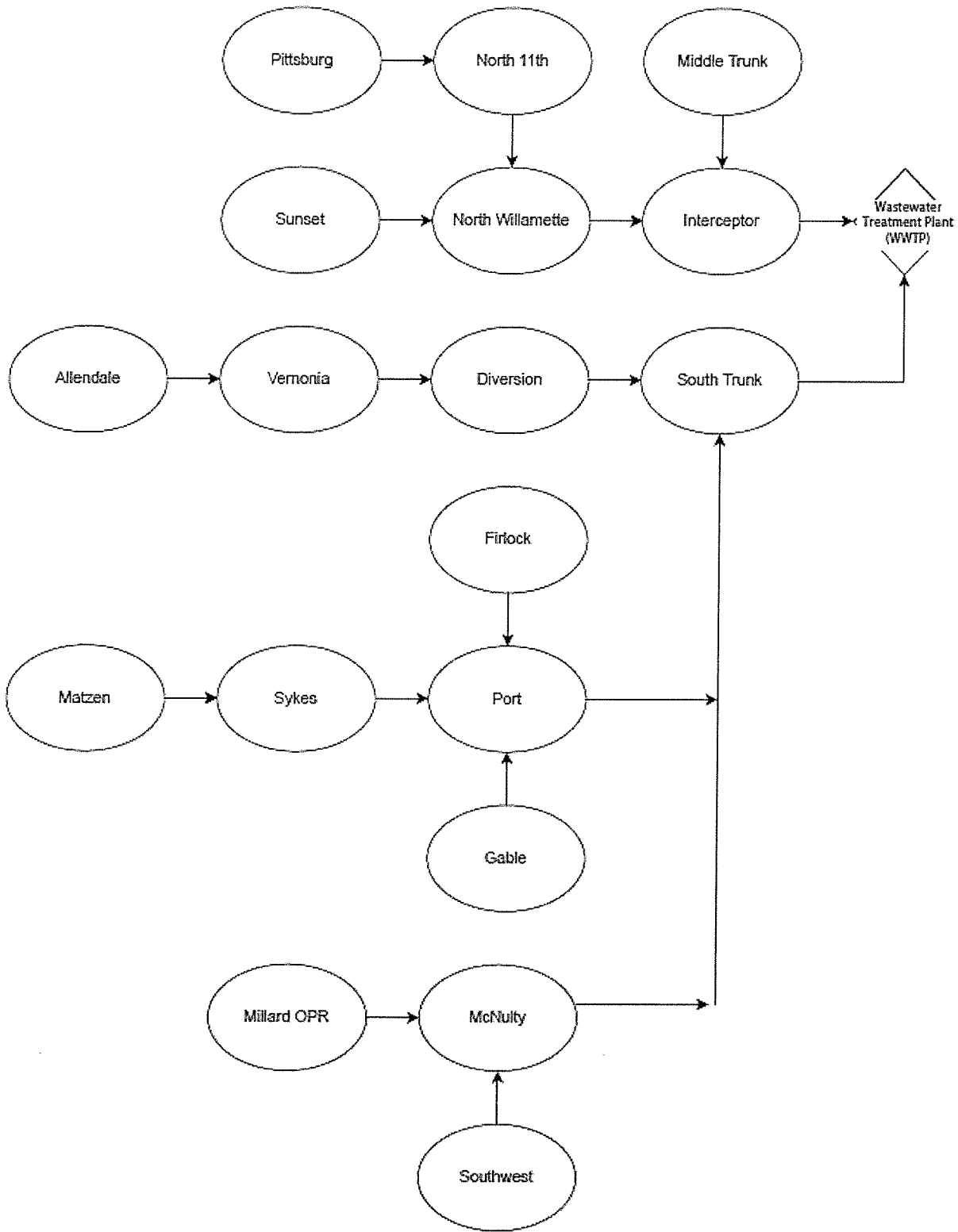


Figure 2.1.B St. Helens Sanitary Sewer Trunkline Basin Flow Paths

2.8 McNulty Sewer Basin

The McNulty sewer basin area has 144 new In-Basin EDUs.

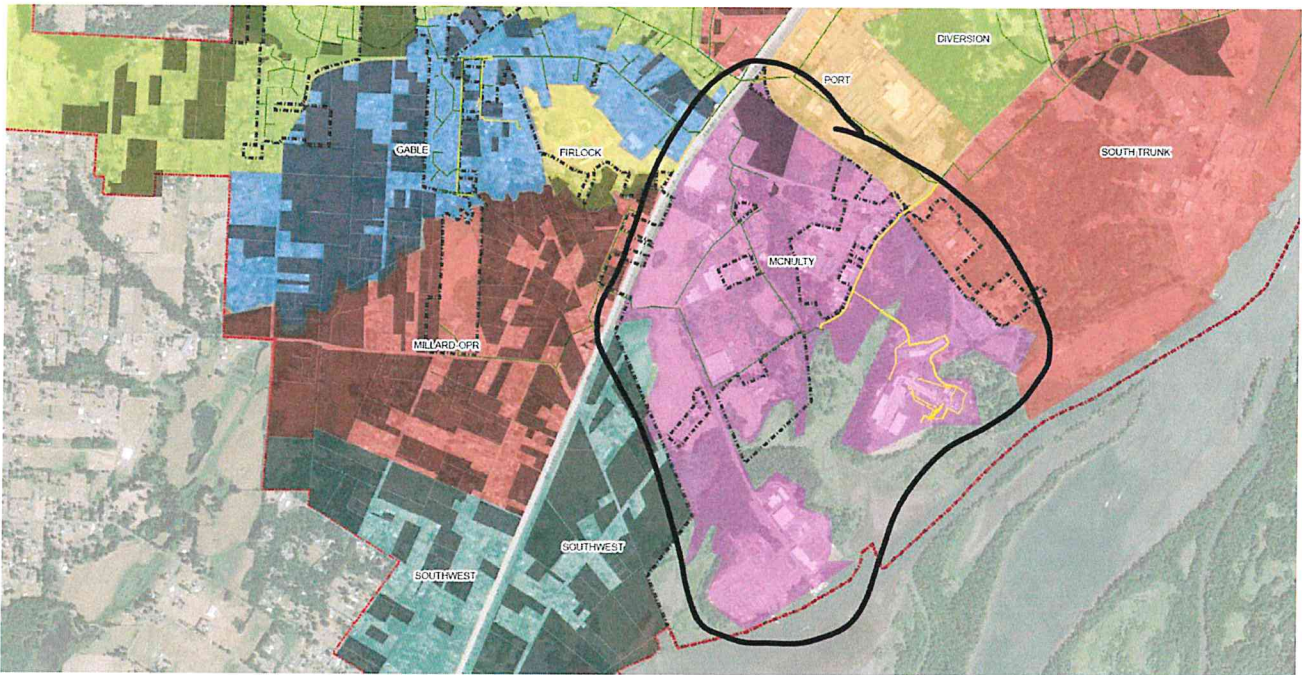


Figure 2.8.A McNulty Sanitary Sewer Basin

The allocation of the McNulty sewer basin's downstream CIP share per new upstream EDU, which consists of the McNulty and South Trunk basins, is \$3,200.

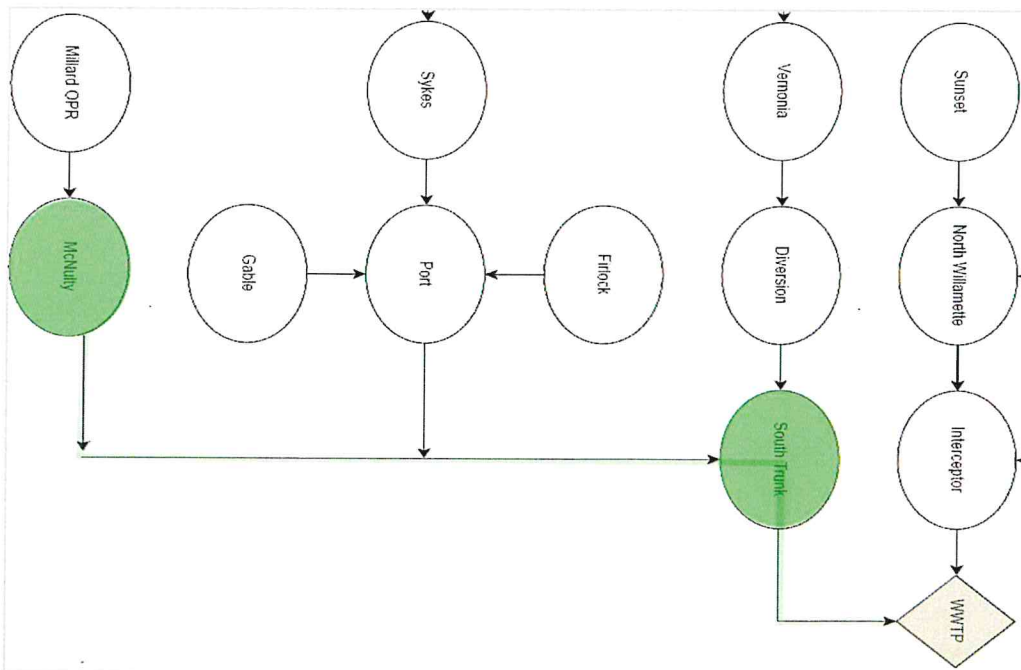


Figure 2.8.B McNulty Sewer Basin Flow Path to WWTP

SECTION 3 – SEWER SURCHARGE CHART

Sewer Trunkline Basin	Downstream CIP Share per New Upstream EDU	New-In Basin EDU	Sewer Surcharge per EDU*
Allendale	\$104,900	1	\$15,000 (max.)
Diversion	\$104,900	1	\$15,000 (max.)
Firlock	\$7,600	0	\$7,600
Gable	\$7,900	589	\$7,900
The Interceptor	\$2,200	512	\$2,200
Matzen	\$12,700	430	\$12,700
McNulty	\$3,200	144	\$3,200
Middle Trunk	\$41,400	91	\$15,000 (max.)
Millard-OPR	\$3,200	806	\$3,200
North 11th	\$3,400	340	\$3,400
North Willamette	\$2,200	134	\$2,200
Pittsburg	\$3,400	731	\$3,400
Port	\$3,800	36	\$3,800
South Trunk	\$1,800	124	\$1,800
Southwest	\$3,200	748	\$3,200
Sunset	\$7,900	321	\$7,900
Sykes	\$6,600	500	\$6,600
Vernonia	\$104,900	30	\$15,000 (max.)

* Estimated Sewer Surcharge cost per EDU is based on the US dollar at the time this document was published. Inflation adjustment to value at time of building permit issuance shall be included.

SECTION 4 – EQUIVALENT DWELLING UNIT CONVERSION

Land Use	EDU Conversion
Single Family Residential	1.00 EDU per unit
Multi Family (Duplex)	0.80 EDU per unit
Multi Family (3 or more Dwelling Units)	0.77 EDU per unit

Residential EDU conversion rate based on the City of St. Helens adopted Sewer Utility Rates and Charges.

EDU conversion rates for sewer surcharges for commercial, industrial, and other land uses not covered under Single Family Residential, Multi Family (Duplex), or Multi Family (3 or more Dwelling Units) shall be based on City of St. Helens wastewater rate classifications for water meter size(s).

3/4-inch meter	1.00 x Sewer Surcharge
1-inch meter	1.67 x Sewer Surcharge
1.5-inch meter	3.33 x Sewer Surcharge
2-inch meter	5.33 x Sewer Surcharge
3-inch meter	10.00 x Sewer Surcharge
4-inch meter	16.67 x Sewer Surcharge
6-inch meter	33.33 x Sewer Surcharge
8-inch meter	53.33 x Sewer Surcharge



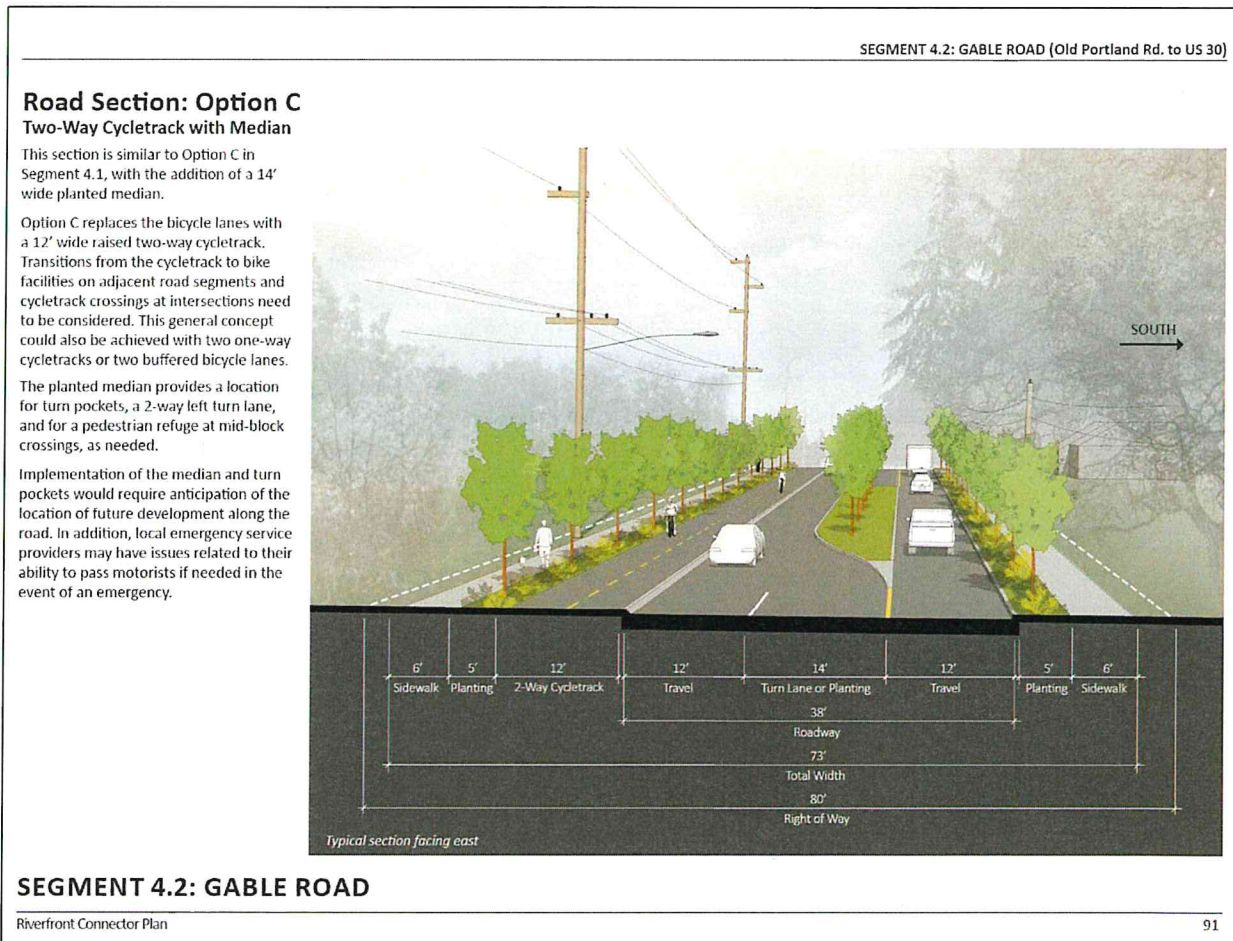
ENGINEERING STAFF REPORT

REPORT DATE	PROJECT	PREPARED BY
11/03/2023 <i>Sept 24</i>	Columbia County Mental Health (CCMH) Improvements At 58646 McNulty Way	Sharon Darroux Engineering Manager

REPORT

STREETS

Half street improvements are required. Frontage improvements along Gable Road and shall follow the City of St. Helens approved Riverfront Connector Master Plan. Along the northwest corner of the property adjacent to wetlands, 12-ft wide curb-tight concrete multi-use bicycle/pedestrian pathway shall be constructed.



WATER

- Water is available. No water main public improvements are anticipated.

SEWER

Item B.

- Sewer capacity surcharge fee will apply for the additional EDUs. Property is located in the McNulty Sewer Basin and fee will be determined based on the St. Helens Wastewater Collection System New Sewer Connection Surcharge document, December 2022 revision for the McNulty Sewer Basin

STORM

- A 1200-C Construction Stormwater General (NPDES) Permit will be required for the site if construction activity and materials or equipment staging and stockpiling will disturb one or more acres of land.
 - * Because of the site's proximity to McNulty Creek, which is one of the impacted waterbodies listed in the City's Mercury TMDL Implementation Plan, the City may require a 1200-C Construction Stormwater General Permit on the basis of DEQ's permit requirement for "Any construction activity that may discharge stormwater to surface waters of the state that may be a significant contributor of pollutants to waters of the state or may cause an exceedance of a water quality standard".
- Site stormwater must be detained. Storm detention facility shall be designed to provide storage using the 25-year event, with the safe overflow conveyance of the 100-year storm. post-development peak release rates shall equal the pre-development release rates for their matching design storm event up to the 10-year design storm. The 25-year storm event peak release rate should not exceed the 10-year pre-development peak release rate. Calculations of site discharge for both the existing and proposed conditions is required.

COLUMBIA COUNTY

Land Development Services



ST. HELENS, OR 97051

230 Strand St.

Direct (503) 397-1501

www.co.columbia.or.us

September 23, 2024

City of St. Helens
Attn: Jacob Grachien

Subject: Columbia Community Mental Health Site Design Review for New Transitional Housing Facility
Subject Map: 4N-1W-09-BB-00300
Situs: 2195 Gable Rd, St. Helens, OR 97051

Re: Columbia County Onsite Wastewater System/Sanitation Comments

These comments are intended to be the formal response to the subject application to be discussed at the October 8th City of St. Helens Planning Commission Meeting. The Site Design review application narrative addresses the proposed use of a 16 bed Transitional Facility. While the occupancy of the building may initially vary, from a wastewater flows stand point the following comments will be based off of the intended final/future use.

The Main CCMH building accessed off McNulty way is currently served by City of St. Helens sewer; connection occurs via a step system to the line on McNulty way. This office does not have direct knowledge of this infrastructure as it is permitted through the City of St. Helens. The subject property at 2195 Gable Rd was approved and permitted for office type uses for youth counseling and includes the main office building off Gable Rd and several portable offices. These structures are connected to an onsite wastewater system installed under a Major Repair permit process. An onsite system was pursued due to the non-existence of an adequate sewer connection point on this side of the property. The Transitional Facility proposal states a plan to simply expand the existing onsite system to support the more intensive use. There are several factors to consider with this and the expansion of the system may not be an option:

- The existing onsite wastewater system is currently at capacity as far as the authorized uses and connections. The existing system has a design flow of 600 gallons per day (gpd).
- While a facility may pursue an Authorization Notice to increase the flows into a system, this is capped at a maximum of 300gpd over the permitted design flow and is also dependent on whether the components can support the increase. This increase would also have requirements to site a replacement system area which may not be feasible.
- The existing building at 2195 Gable Rd has already undergone some modifications through a building remodel that were not reviewed from a sanitation stand point, so it is unknown if the changes resulted in a more intensive use that may have required formal approval that is not captured in the design flow or existing permit information.
- A 16 bed transitional facility would have a design flow of 2000 gallons per day based of OAR 340-071-0220 Table 2 for Institutions other than Hospitals.
- The total design flow would be a combination of the existing system flows and the new proposed flows (2000gpd + 600gpd= 2600gpd) and would exceed the 2,500gpd maximum

threshold that can be permitted under the County. This would require the property owner to pursue a State of Oregon Water Pollution Control Facilities Permit through DEQ.

- The area that the existing onsite system is located was used to its maximum to site the repair Onsite wastewater system. The site has shallow to bedrock soils that are impacted by the water table in the winter. It is not expected that the system could be enlarged to support increasing the size of the system.
- It is expected that based off of flows alone, DEQ may find that connection to City sewer would be applicable prior to even reviewing or considering an expansion to the existing system, which as explained, may not be feasible.
- If it was found that sewer was not outright applicable, and if it was determined that the system could not be expanded as expected, then connection to City sewer would be required as a matter of Septage management to proceed with the proposed use.
- Additionally, the site plan submitted does not actually include any septic system infrastructure and potential future building and parking areas would be in conflict with the septic systems location.

In summary, connection and expansion of the existing onsite wastewater system may not be possible and would be a significant infrastructure upgrade to overcome before the owner could proceed with constructing the new facility. It is recommended that the services of the Onsite wastewater consultant be retained for further contemplation, but it also appears that connection to city sewer is likely to be the most viable option.

Sincerely,



Erin O'Connell
Environmental Services Specialist

CCMH- Site Design Review
Revised Narrative for Phase 1 and 2
Transitional Housing, Detoxification Center, and Crisis Center
Revised Narrative - 11/05/2024

Property Owner:

Columbia Community Mental Health
 2195 Gable Road
 St Helens, OR 97051

Subject Property:

Tax Lot 4109-BB-0300 – 2195 Gable Road, and 58646 McNulty Way
 Formerly two tax lots
 Tax Lot 4108-AD-0200 – 58646 McNulty Way, St Helens
 Tax Lot 4109-BB-0300 – 2195 Gable Road, St Helens

Revised Application for:

- **Conditional Use & Site Design Review**

Updated Submittal Materials:

- **Updated Architectural Drawings (AKAAN)**
 - SP-1 Architectural Site Plan
 - A1.1 Architectural Floor Plan
 - A 2.1 & A 2.2 Architectural Elevations
- **Updated Civil Drawings (Humber Design Group)**
 - C0.50 Existing Conditions Plan
 - C1.00 Layout and Paving Plan
 - C2.00 Grading Plan
 - C3.00 Combined Utilities Plan
- **Updated Transportation Generation Memo (Lancaster Mobley)**

Previously Submitted

- **Wetlands Delineation**
 - Wetlands Report (Wetlands Solutions NW)
 - DSL Concurrence Letter (DSL)

RECEIVED
 NOV 6 2024
 CITY OF ST. HELENS

Introduction

Facility:

The proposed facility will have three (3) components: a transitional housing center for up to 16 displaced individuals (Phase 1), a detoxification center for up to ten (10) individuals, and a crisis center that will be able to serve up to eight individuals (Phase 2).

CCMH initially only had enough funding for the transitional housing component (Phase 1) and therefor originally only applied for site design review for Phase 1. However, CCMH has recently been approved and will soon sign the final contract with the Oregon Health Authority (OHA) to fund the detoxification unit and the crisis center (Phase 2).

This application is revised to include both Phase 1 and Phase 2.

The transitional housing center component will contain 16 single occupancy sleeping rooms, a community room, staff offices, and shared bathing, laundry, and dining areas, and a kitchen.

The Detoxification Center will have five (5) – double occupancy sleeping rooms, and shared bathing, laundry, and dining areas and two (2) staff offices.

The Crisis Center will have a crisis room for up to five (5) adults and a crisis room for up to three (3) minors with a central staff observation room, and five (5) additional rooms that will be a combination of intake/assessment rooms and/or staff offices.

This type of facility is a new building type. This type of care has developed in recent years to deal with the current mental health crisis in Oregon and across the country. There is no specific definition in the City Zoning Code for the Crisis Stabilization Center however based on the type of services provided it falls under the same definition as “Intermediate Care Facility”, approved at the Planning Commission hearing on October 8, 2024.

The Oregon Health Authority is currently in the process of developing OAR rules for this type of facility. Once the rules become final they will fall

under ORS 443.400, the same ORS that regulates residential treatment centers.

Whenever there are patients at the facility they will be supervised on a 24-hour basis by CCMH staff certified or licensed per Oregon standards.

Site Improvements:

CCMH is already connected to city utilities.

Water:

When CCMH constructed its main office building it installed a 2” water meter (and paid required SDC fees) connected to the City’s 8” water main in McNulty Way.

Water meters are sized based on “fixture units” (FU). Based on CCMH’s Master plan inclusive of the existing buildings there are potentially four (4) additional future buildings sites on the campus. The future buildings will mostly be additional office buildings and one could be dedicated to additional transitional housing. Based on building type and size, the estimated total fixture units for current and potential future buildings are outlined in the below table

Buildings (#) indicates possible future buildings)	FU
CCMH Main Office Building	78
Existing Crisis Center	19
Current Proposal Phase 1	50
Current Proposal Phase 2	44
(1) replacement building for existing modular buildings	19
(2) 10,000 sq ft office building	22
(3) 5,000 sq ft office building	19
(4) Possible homeless housing facility**	133
total	384

**NOTE: CCMH Board has pondered an additional homeless housing facility of some kind but does not have concrete plans for such a facility at this time. However, because this type of facility has a higher water potential water demand we have included it in this table for illustration, and “worst case scenario purposes” only.

Per the City of Portland Water Department, a 2" water meter can serve 532 fixture units.

The existing 2" water service is more than sufficient to handle all future water needs on the property. CCMH's on-site water main will be upgraded beginning at the existing 2" water meter and extending across the property to serve the new building and any future buildings.

Water for Fire Protection:

When CCMH constructed its main office building it installed an 8" fire water main and a fire hydrant in front of the main building. The 8" fire main will be extended and connect to the new building's fire sprinkler system and a new fire hydrant will be installed adjacent to the building. The fire main will be extended across the property in anticipation for future development of the rest of the campus.

Sanitary Sewer:

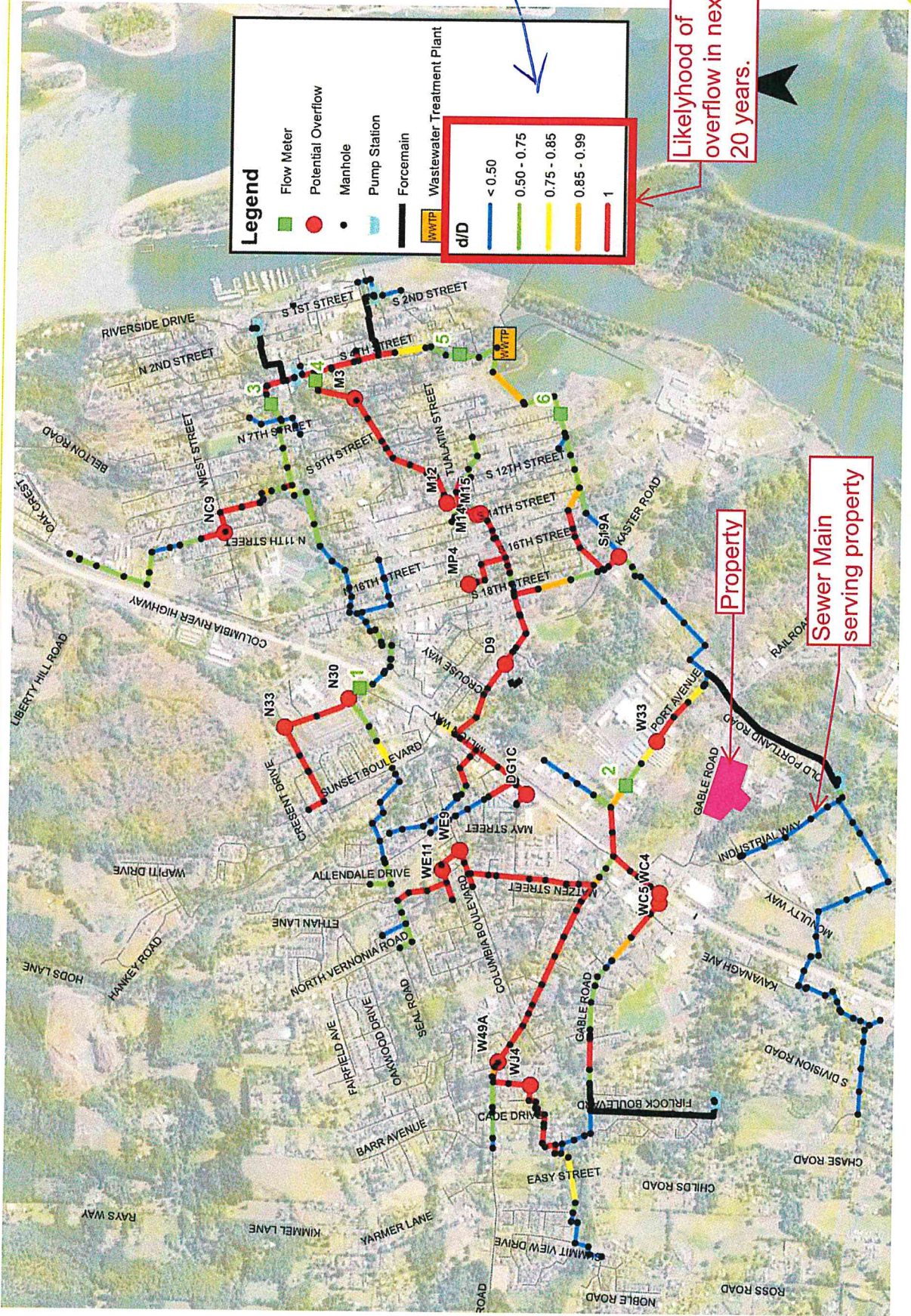
CCMH is already connected to city sewer. When CCMH constructed its main office building it installed a private pump system (known as a "STEP" system) which pumps the sewage to a sanitary manhole in the Gable Road R.O.W. (CCMH paid required SDC sanitary fees at that time as well). CCMH's onsite "STEP" system will be upgraded and extended to serve the new building and any future buildings. (See Civil Utility Plan.) The existing crisis center building will have a new pump installed in the existing septic / holding tank and connect to the upgraded sanitary main at a vault north of the main building parking lot. East of the proposed building another pump station will be installed, the new building and the existing modular buildings will connect to the new pump station. After all the connections are made, the existing septic system will be abandoned. The upgraded system will continue to pump to the same manhole in the Gable Road R.O.W.

The property is indicated on the attached Sanitary Sewer Master Plan Map. (see next page) That map identifies sanitary sewer lines that could overflow in the next 20 years, areas marked with red are likely to overflow, other colors indicate less likelihood of potential overflow. CCMH connects

FIGURE 1-3 ON THE NEXT PAGE IS EXISTING - NOT 20-YEAR

EXISTING - NOT 20 YEAR

FIGURE 1-3: EXISTING SYSTEM EVALUATION - D/D AND POTENTIAL OVERFLOW LOCATIONS



to the sewer main to the south of their property which is colored blue on the map, the least likely of overflow per the St Helens Sanitary Sewer Masterplan. (A 0.5% percent likelihood of overflow in the next 20 years).

Now - NOT 20-YEARS ↘

Other utilities:

Electrical connections will be made to the existing power lines on site. Existing electrical power lines will be upgraded for the additional electrical loads, any new power distribution will be underground.

Any Telecom upgrades will be underground.

The existing gas connection at McNulty Way will be upgraded and extended to serve the new building and future campus development.

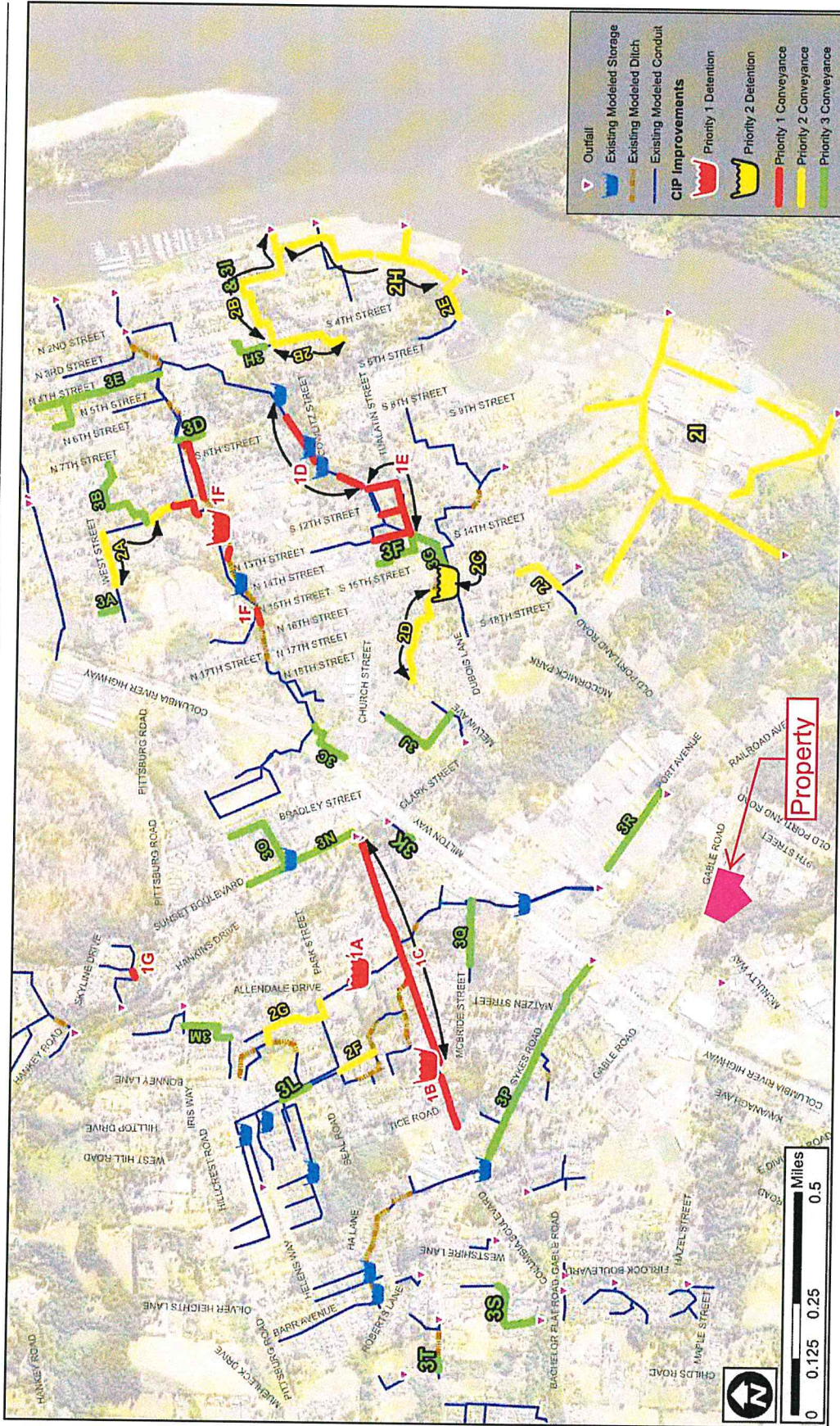
All of these infrastructure upgrades will be installed in anticipation of future campus development to the west side of the property. Included in the site plan are dashed lines that indicate potential building sites and potential parking areas. The current layout of the parking area and the infrastructure upgrades anticipates these potential future buildings; however, any timeline and funding of these facilities is currently unknown and they are not part of this application.

Stormwater:

There is an existing stormwater retention pond on the property. The existing stormwater retention pond will be enlarged to handle the current proposed development as well as anticipated future parking lots and buildings shown as shown (dashed lines) on the site plan.

The city of St Helens Stormwater Master Plan shows deficiencies in the City's stormwater system. No such deficiencies are indicated in the master plan close that relate to this property. (see next page)

FIGURE 1-6: CAPITAL IMPROVEMENT PLAN



Road Improvements:

Improvements to Gable Road will include half street frontage improvements along the entire property frontage, including a curbed landscaped strip with street trees, curbed bicycle lane, pedestrian sidewalk.

The existing driveway for CCMH's existing shop will remain as approved by the Planning Commission at the October 8 2024 hearing.

The Riverfront Connector Plan (RCP) shows a street section that includes a center landscaped median. City staff has told CCMH that the center landscape medians will have to be constructed as part of this project. The civil drawings show the median as directed by city staff. (see Civil drawings).

Note this scenario does not propose turn pockets for the properties on the north side of Gable Road.

A property owner on the north side of Gable Road (see Dahl letter, October 6, 2024) pointed out that the Riverfront Connector Plan says "any medians would require anticipation of the location of future development along the road." (RCP pgs. 89-91). Our architectural site plan (see drawing SP-1) indicates a "striped median" rather than a landscape island, deferring the construction of the center medians until the properties to the north develop and locations of turn pockets can be determined.

Another property owner north of Gable Road that has heavy equipment has told CCMH that the proposed layout (per civil drawings) will block his left turn entrance into the new driveway that was recently approved by the City.

As part of the Site Design Review we propose that the planning commission defer the center landscape islands until development on the north side of Gable Road is established. (Per drawings SP-1) Constructing the center medians without turn pockets (other than for CCMH and McNulty Way) appears to be contradictory to the language in the Riverfront Connector Plan that requires the anticipation of future development..

← A MEDIAN THAT CAN BE LANDSCAPED OR NOT JUST LANDSCAPED
TURN LANE

Wetlands:

CCMH has already performed a wetlands study on the property and the and DSL has already concurred with the delineation (copies previously submitted). There will be a section of the wetlands affected along Gable Road where new sidewalk will be installed. CCMH will apply for a wetlands permit through DSL for that work.

Narrative of relevant SHMC chapters.

17.32.110 General Commercial (GC)

(2) Uses Permitted Outright. In a GC zone, the following uses are permitted outright subject to the provisions of this code and especially the chapter on site development review (Chapter 17.96 SHMC):

(l) Offices – all.

“Office” means a room or group of rooms used for conducting the affairs of a business, profession, service, industry, or government.

(3) Conditional Uses. In the GC zone, the following conditional uses may be permitted upon application, subject to provision of Chapter 17.100 SHMC and other relevant sections of this code:

(y) Residential facility.

“Residential facility” means a residential care, residential training or residential treatment facility, as defined in and licensed by the Department of Human Services under ORS 443.400 through 443.455 or ORS 418.205 through 418.327, that provides residential care alone or in conjunction with treatment or training or a combination thereof for six to 16 individuals who need not be related. Staff persons required to meet licensing requirements shall not be counted in the number of facility residents, and need not be related to each other or to any resident of the residential facility.

Approved by Planning Commission October 8 2023

“Intermediate care facility” means a facility which provides, on a regular basis, health-related care and services to individuals who do not require the degree of care and treatment which a hospital or skilled nursing facility is designated to provide, but who, because of their mental or physical condition, require care and services (above the level of room and board) which can be made available to

them only through institutional facilities such as these. See “long-term care facility” and “nursing or convalescent home.”

The uses in the building will be “Intermediate Care Facility” for the Transitional Housing Component, (approved by the Planning Commission October 8, 2024), a “Residential Facility” for the detoxification component, licensed per ORS 443. The Crisis Center does not have any specific definition in the SHMC but the services provided also fall under the definition of Intermediate Care Facility. We also note that once the OHA finalizes the rules regulating crisis centers, those rules will be incorporated into ORS 443, the same ORS that regulates “Residential Facilities”.

- (4) Standards. In the GC zone the following standards shall apply:
 - (a) The maximum building height shall be 45 feet, except as required in SHMC 17.68.040.

Proposed building height +/- 24'

- (b) Outdoor storage abutting or facing a lot in a residential zone shall comply with Chapter 17.72 SHMC.

Not applicable no abutting residential zones.

- (c) The maximum lot coverage including all impervious surfaces shall be 90 percent.

Property area 431279 sq. ft.

Existing Improvements

Existing Buildings:

Main building	20,500
Modular 1	2500
Modular 2	1500
Modular 3	550
Crisis Center	1400
Shop	605
Storage Sheds	105+175+205 = 485
(behind main building)	

Paved Areas

Existing Paved Area	79,992 sq. ft.
Existing Concrete	5,228 sq. ft.
(walkways etc.)	

Total existing impervious surfaces: 27,540 sq. ft.
 Existing Lot Coverage $27540 / 431279 = 6.3 \%$

New Facility & related improvements

New Building Phase 1	5800 sq. ft.
New building Phase 2	5280 sq ft
New Paved Areas	16,245 sq. ft.
New Concrete	2,297 sq. ft.

Lot Coverage with New improvements = 57,162

New Lot Coverage $57,162 / 431279 = 13.25 \%$

(d) Multi-dwelling units must comply with AR standards and other applicable sections of this code.

Not applicable

(e) The minimum landscaping shall be 10 percent of the gross land area associated with the use.

The property is a campus setting, some areas are natural areas and wetland areas, other areas adjacent to buildings are more “formally landscaped”. It will be the same with this new building, areas adjacent to the building and parking lot will be formally landscaped, and areas not adjacent to the building will be left as natural areas. As noted above the total lot coverage (all impervious areas) is only 12%. In this kind of setting the remainder, or 88%, is either formally landscaped or left as naturally landscaped areas.

17.40 (wetlands):

City of St Helens has determined that there are no significant City wetlands on the property.

CCMH has performed a wetlands delineation survey (attached). The wetlands delineation has been concurred with by the Oregon Department of State Lands (DSL) concurrence letter has been provided to the City. The wetland is identified on the site plan drawings. The wetland abuts and

encroaches into the Gable Road ROW near the intersection of McNulty Way.

CCMH will be required to acquire a wetlands permit from DSL prior to any work or construction of that portion of the city right of way along Gable Road or in any other areas that may affect the designated wetlands.

17.46 (floodplain): Not applicable. The proposed area is outside of any floodplain.

17.72 (landscaping & buffering):

Landscaping & Street Trees

On the south side of Gable Road there are existing overhead utility lines (telecom). The site plan shows street trees along Gable Road, small stature street trees will be required. CCMH will select small stature trees for from City of St Helens Street tree list for approval when Right of Way engineering drawings are submitted for permit.

Per direction from City Staff, along the portion of Gable Road where the wetlands encroach into the ROW, street trees will be eliminated, and the shared pedestrian / bicycle path shall be curb tight. City Staff recommended and will allow the wetlands to serve as the street trees and landscaping in this area. (Similar to what the City did at the improvements to Columbia Blvd between Gable Road and Sykes Road.) Any disturbance to the wetlands will be restored per DSL requirements (see wetlands above).

17.72.070 Buffering and screening

Buffering provisions mandate buffer areas are maintained when different zones abutting each other.

17.72.130 Buffer matrix.

The property is General Commercial (GC), abutting properties are Light Industrial (LI) which requires a min. 10' buffer to adjacent LI properties.

The proposed building is approximately 165' away from the closest property line and no work is anticipated along the property line. The area along the property line has existing trees that will not be disturbed.

Screening of Parking Lots

The parking lot will be landscaped with a mix of groundcover, shrubs, and trees. Per city requirements tree islands will be constructed at a ratio of (1) tree island per (7) parking spaces. The site plan shows the general concept of the landscape, a specific landscape plan will be provided to the city for permit approval.

Screening of Service Facilities:

Gas meters and HVAC units and any storage or garbage areas will be screened per city code, specifics will be provided to the city as part of the plans submitted for permit approval.

There will be other landscaping associated with the building for the privacy of the residential rooms and in landscaped area around the building, the site plan shows the general landscaping areas, specific plans will be submitted for building permit.

17.76 (vision clearance):

Gable Road has a Minor Arterial classification so 35' vision triangle will be maintained for the ROW modifications to the existing driveways.

17.80 Off-Street Parking & Loading

Parking Analysis:

The property works as a campus where the parking lots are shared between all the buildings. Below are listed the amount required parking spaces per previous land use decisions.

Existing Parking

2004 Main Building:	82 spaces required (per 2004 as-built drawings)
---------------------	--

2017 First Modular Building installed: 22 additional spaces required
2018 Additional Modulares installed: 17 additional spaces required

2021 Crisis Center Building 3 spaces*

* no land use documentation found for crisis center, number based on a practical layout of the existing graveled area.

Parking for Existing buildings 124 Spaces

Parking - Current Proposal (Phase 1 and Phase 2)

There is no clear definition in the SHMC for all components of the facility. The closest definition under the parking requirements of development code is "Group Care".

(1) Residential.

(d) Group care – One space per three residential beds plus one space for each employee on largest shift.

Sixteen (16) residential beds (transitional housing)	16
Ten (10) residential beds (detoxification unit)	10
Eight (8) crisis center "couches" (beds)	8
	<hr/>
	34

34 / 3 = (11.3) = 12 spaces (rounded up)

Staff offices = 8 8 spaces

12 + 8 = 20 new required parking spaces

124 existing required spaces (per the above) + 20 required new spaces = 144 total required spaces.

Proposed Site Plan shows 150 total parking spaces, six (6) surplus.
Parking Requirement is met.

Carpool Spaces: 5% of new required spaces required to be car pool spaces, Minimum new car pool spaces = 1. One (1) carpool space will be designated on the permit plans.

Bicycle Parking required to be 10% of new required spaces. 2 bicycle space required based on (20) required new parking spaces (see above). Two (2) bicycle space will be designated on the permit plans.

17.84 Access Variance

Access Variance for existing shop driveway was approved at the Planning Commission's October 8, 2024 hearing.

17.88 (signs):

Not applicable, CCMH is not applying for a sign with this proposal. Any proposed sign will be applied for separately.

17.96 (Site Design Review)

17.96.180 Approval standards.

Many of the Site Design review approval standards relate to Residential Multifamily development. This narrative only addresses those which are applicable to this commercial development

17.96.180(1) To our knowledge all applicable chapters of the development code have been addressed.

17.96.180(2) The buildings have been located to conform to existing topography, the area is not subject to slumping or sliding, as a campus setting there is ample distance between buildings for light and air, the parking and drive lanes are designed to accommodate emergency vehicles.

All trees greater than 6" dbh have been identified and will be either preserved or replaced with new plantings.

(3) refers to residential projects, not applicable

(4) buffering & screening is addressed 17.72 above

(5), (6), & (7) refer to residential projects, not applicable

NO PLAN
SUBSTANTIATES
THIS
COMMENT

(8) The site plan demarcates different areas with different techniques, curbs, landscaping, level changes. Additional techniques will be incorporated into the design as the design progresses and submitted and reviewed during the building permit process.

(9) Crime Prevention & Safety. All of the criteria listed in this section are reviewed during the building permit process and will be reviewed by the city at that time.

(10) The site has been designed with consideration to the existing driveways and access points, it has been designed to accommodate emergency vehicles and contains separate pedestrian paths and clearly identified crosswalks where pedestrian paths and vehicular paths converge.

(11) refers to residential projects, not applicable

(12) parking is addressed in 17.80 above

(13) The project is a campus setting with naturally landscaped areas and more formally landscaped areas near the buildings and parking areas. Specific landscape plans that comply with 17.72 will be provided for approval during the building permit process.

(14) Submitted with this application is a preliminary grading plan prepared by a licensed civil engineer that complies with the city's grading and stormwater engineering guidelines. Final grading and stormwater plans will be submitted for approval during the building permit process.

(15) The project will be designed to the most recently adopted ADA standards.

(16) If CCMH decides to install a sign a separate application will be submitted per 17.88.

(17) To our knowledge all applicable chapters of the development code have been addressed.

17.100 40 (Conditional Use Approval Standards):

(1) The planning commission shall approve, approve with conditions, or deny an application for a conditional use or to enlarge or alter a conditional use based on findings of fact with respect to each of the following criteria:

- (a) The site size and dimensions provide adequate area for the needs of the proposed use;
- (b) The characteristics of the site are suitable for the proposed use considering size, shape, location, topography, and natural features;
- (c) All required public facilities have adequate capacity to serve the proposal;
- (d) The applicable requirements of the zoning district are met except as modified by this chapter;
- (e) The supplementary requirements set forth in Chapter 17.88 SHMC, Signs; and Chapter 17.96 SHMC, Site Development Review, if applicable, are met; and
- (f) The use will comply with the applicable policies of the comprehensive plan.

(a) & (b) The size, dimensions, and characteristics of the site are adequate and suitable for the proposed use.

(c) See introduction, pages 2 - 5 above. Based on discussions with city staff, public facilities are adequate. There are no storm water issues in the area. The 8" water main and existing 2" water meter are sufficient to serve future development on the property. Based on the Sanitary Sewer Master plan the sewer line that the property connects to is one of the least likely to overflow in the next 20 years. The existing on-site facilities will be upgraded for long-term campus development.

(d) With the similar use determination by the Planning Commission on October 8 2024 (see above) the zoning requirements are met.

(e) 17.88 and 17.96 are addressed above.

17.132 tree removal:

Included in the application is an existing conditions survey which identifies all trees on the property larger than 6" in diameter.

NO PLAN INDICATES SIZE!?!

The proposed building and parking lot are in an area of the property where there are few trees. One ornamental maple tree 7" in diameter will be removed (north part of building). One oak tree 16" in diameter will be removed (west side of parking lot). A 66" redwood (south of building) will remain and will be protected during construction.

Multiple trees (not counting required street trees and parking lot trees) will be planted as part of the building landscaping far in excess of city requirements for replacement trees of the two removed trees. (see site plan).

Trees in wetlands area along Gable Road:

Because the street section has not yet been designed it is unknown at this time how many trees will be affected in the area where the wetlands is in the city ROW adjacent to Gable Road. Regardless, because the wetland survey has already been performed and acknowledged by DSL any trees that are affected in that area will have to be mitigated per DSL standards. Part of the wetlands mitigation plan will include a planting plan, including trees.

17.152 (utilities/streets):

Utilities: The drawings show the proposed Right of Way improvements. The proposed improvements follow the City of St Helens Riverfront Connector Plan design guidelines with a landscape strip, and a protected a bicycle lane and sidewalk. The only exception to the above is the area of the ROW where the northern portion of the wetlands encroaches into the ROW. Per city staff direction a curb-tight 12' wide shared bicycle / pedestrian way is proposed so as to protect the wetlands as much as possible. Even with this reduction some of the wetlands that encroach on the right of way will be affected. CCMH will be required to go through DLS's (and possibly the Corps of Engineer's) permit processes to perform work in the wetlands. Any disturbed wetlands will be restored to DSL standards, and if required, other wetland improvements will be performed to mitigate lost wetlands where the street improvements are performed.

Streets:

The Riverfront Connector Plan (RCP) shows a street section that includes a center landscaped median. City staff has told CCMH that the center landscape medians will have to be constructed as part of this project. The civil drawings show the median as directed by city staff. (see Civil drawings).

Note this scenario does not propose turn pockets for the properties on the north side of Gable Road.

A property owner on the north side of Gable Road (see Dahl letter, October 6, 2024) pointed out that the Riverfront Connector Plan says “any medians would require anticipation of the location of future development along the road.” (RCP pgs. 89-91).

Our architectural site plan (see drawing SP-1) indicates a “striped median” rather than a landscape island, deferring the construction of the center medians until the properties to the north develop and locations of turn pockets can be determined.

Another property owner north of Gable Road, an excavation contractor, has told CCMH that the proposed layout (per civil drawings) will block his left turn entrance into the new driveway that was recently approved by the City.

We propose that the planning commission defer the center landscape islands until development on the north side of Gable Road is established. (Per drawings SP-1) Constructing the center medians without turn pockets (other than for CCMH and McNulty Way) appears to be contradictory to the language in the Riverfront Connector Plan that requires the anticipation of future development.

17.156 Traffic Impact Analysis (TIA):

When the property was rezoned the City Council stipulated a “trip-cap” on the property, with a total maximum additional trips of 700 trips per day. (ORD No. 3297).

Included in this application is an updated trip generation memo from Jennifer Danziger, Senior Transportation Engineer from Lancaster Mobley that includes the entire building (Phase 1 and Phase 2). Per Ms. Danziger’s calculations, trip generation is estimated at no more than 108 daily trips per day, significantly lower than the 700 daily trips imposed by ORD No. 3297.

See trip generation memo dated 11/05/2024.

Memorandum

To: Jenny Dimsho, City of St. Helens
Copy: Al Peterson, AKAAN
From: Myla Cross
Jennifer Danziger, PE
Date: November 5, 2024
Subject: Columbia Community Mental Health
Trip Generation Memorandum for Phases 1 & 2

RECEIVED
NOV 6 2024
CITY OF ST. HELENS



RENEWS: 12/31/2025

Introduction

Columbia Community Mental Health (CCMH) is proposing an approximately 11,080-square foot (SF) new building on their site for Phases 1 & 2 of their expansion. Phase 1 includes an approximately 5,800 SF building with transitional housing for individuals in crisis and office space. Phase 2 includes an approximately 5,280-SF addition to the Phase 1 building with detox rooms, crisis stabilization rooms, and office space. The phases were initially planned to be constructed sequentially but are now anticipated to be constructed concurrently.

This memorandum examines the total estimated trip generation of Phases 1 & 2 and provides a comparison with trip cap established with the rezoning of the site from Light Industrial (LI) to General Commercial (GC) in 2023 as established by Ordinance #3297

Project Description & Location

The project site (Map No.4109-BB Tax Lot 300) encompasses approximately 9.84 acres and is located south of Gable Road and east of McNulty Way. The subject property was originally two parcels (Tax Lots 200 & 300) but has since been consolidated into a single parcel owned by CCMH. The existing main CCMH office is located on the southern portion of the site. The proposed 11,080-SF building will include the first two phases of expansion for CCMH.

- Phase 1 will be 5,800 SF including 16 residential rooms, intended to be used for transitional housing, and 4 offices for CCMH staff.
- Phase 2 will be 5,280 SF including 5 detox rooms, 8 crisis stabilization rooms, and 4 additional offices for CCMH staff.

An aerial view of the proposed site and the nearby vicinity is displayed in Figure 1 on the following page. The subject site is shown outlined in yellow. A site plan is attached.



Figure 1: Aerial Photo of Site Vicinity (Image from Columbia County Web Map)

Trip Generation

To estimate the number of trips that could be generated by the proposed development, trip rates from the *Trip Generation Manual*¹ were used.

Phase 1 Trip Generation

For the 5,800 SF included for Phase 1, data from the land use code (LUC) 253, Congregate Care Facility, and LUC 710, General Office, are used to estimate the proposed site's trip generation based on the number of dwelling units and the square footage of the gross floor area, respectively.

Because there is not a specific land use code in the *Trip Generation Manual* for transitional housing, three land use codes were considered. LUC 253, Congregate Care Facility was selected as the most appropriate alternative as opposed to LUC 254, Assisted Living, or LUC 620, Nursing Home, because residents will not require as much personal or medical assistance.

Phase 2 Trip Generation

For the 5,280 SF expansion for Phase 2, data from the land use code (LUC) 620, Nursing Home, and LUC 710, General Office, are used to estimate the proposed site's trip generation based on the number of beds and the square footage of the gross floor area, respectively.

Similar to Phase 1, there is not a specific land use code in the *Trip Generation Manual* for services provided in this part of the building. The same three land use codes were considered. LUC 620, Nursing Home was selected as the most appropriate alternative as opposed to LUC 253, Congregate Care Facility or LUC 254, Assisted

¹ Institute of Transportation Engineers (ITE), *Trip Generation Manual*, 11th Edition, 2021.

Living, because residents of this part of the facility are anticipated to need more personal care and medical assistance.

Trip Generation Estimates

The resulting trip generation estimates for Phases 1 & 2 are summarized in Table 1. Detailed trip generation calculations are included in the appendix.

Table 1: Trip Generation Estimates – Interim Scenario

ITE Code	Intensity	Morning Peak Hour			Evening Peak Hour			Daily Trips
		In	Out	Total	In	Out	Total	
Phase 1								
710 - General Office Building	0.8 KSF	1	0	1	0	1	1	8
253 - Congregate Care Facility	16 DU	1	0	1	1	2	3	36
Subtotal:		2	0	2	1	3	4	44
Phase 2								
710 - General Office Building	0.65 KSF	1	0	1	0	1	1	8
620 – Nursing Home Facility	18 beds	2	1	3	1	2	3	56
Subtotal:		3	1	4	1	3	4	64
Combined Phases 1 & 2								
Total		5	1	6	2	6	8	108

KSF = 1,000 square feet

As shown in Table 1, total trip generation for Phases 1 & 2 is estimated at 6 morning peak hour, 8 evening peak hour, and 108 weekday trips.

Trip Accounting Analysis

With the rezoning of 5.67 acres of Tax Lot 300, a trip cap of 700 new daily trips was established per Ordinance #3297. The Columbia County Community Mental Health Trip Generation Memorandum, dated August 2, 2024, summarized the remaining trips vested in Tax Lot 300 using the trip cap of 1,107 new daily trips established in the CCMH Transportation Planning Rule Analysis, dated July 25, 2023.

Table 2 summarizes the remaining trips vested in Tax Lot 300 following approval of the proposed development application for two phases of development based on the trip cap of 700 new daily trips established by Ordinance #3297



Table 2: Trip Accounting Summary

Development Phase	Daily Trips
Tax Lot 300 Trip Cap	700
Phase 1 – 5,800 SF Residential & Office Building	44
Phase 2 – 5,280 SF Residential & Office Building	64
Remaining Trips	592

The proposed development falls well below the site’s trip cap. After accounting for Phase 1 and Phase 2 development, the site can accommodate future development generating up to 592 daily trips.

Traffic Impact Study Criteria

St. Helen’s Community Development Code (CDC) Section, 17.156.030, establishes the criteria for when a traffic impact study is required. The criteria are listed below with responses following in *italics*.

- (1) A change in zoning or a comprehensive plan amendment designation, except when the change will result in a zone or plan designation that will result in less vehicle trips based on permitted uses (e.g., from a high density residential district to a lower density residential district or from a commercial district to a residential district);

No zone change or comprehensive plan amendment is proposed.

- (2) The site proposes to take access on Highway 30 or on an approach to Highway 30; or

Site access will be taken from Gable Road.

- (3) The development shall cause one or more of the following effects, which can be determined by field counts, site observation, traffic impact analysis or study, field measurements, crash history, Institute of Transportation Engineers Trip Generation, and information and studies provided by the local reviewing jurisdiction(s) and/or ODOT:

- (a) The proposed action is estimated to generate 250 average daily trips (ADT) or more or 25 or more weekday a.m. or p.m. peak hour trips (or as required by the city engineer);

The total trip generation for Phases 1 & 2, estimated at 6 morning peak hour, 8 evening peak hour, and 108 weekday peak hour trips, falls below these thresholds.

- (b) The proposed action is projected to further degrade mobility at the Deer Island Road/Highway 30, Pittsburg Road/Highway 30, Wyeth Street/Highway 30, Gable Road/Highway 30, or Millard Road/Highway 30 intersections;

Due to the low anticipated trip generation, the site is not projected to further degrade any of the specified intersections.

- (c) An increase in use of adjacent streets by vehicles exceeding the 20,000-pound gross vehicle weights by 10 vehicles or more per day;



The proposal will not be a significant generator of truck trips.

- (d) The location of the access driveway does not meet minimum intersection sight distance requirements, or is located where vehicles entering or leaving the property are restricted, or such vehicles queue or hesitate, creating a safety hazard;

The proposal will use the existing access which appears to have adequate sight distance for a 40-mph roadway.

- (e) The location of the access driveway does not meet the access spacing standard of the roadway on which the driveway is located; or

The proposal will use the existing access on Gable Road which meets spacing standards.

- (f) A change in internal traffic patterns that may cause safety problems, such as backup onto the highway or traffic crashes in the approach area. (Ord. 3150 § 3 (Att. B), 2011)

The proposal is not anticipated to cause safety problems due to internal traffic flow patterns.

Based on this assessment, a traffic impact study is not required.

Conclusions

Phases 1 & 2 of the CCMH expansion is estimated to generate no more than 108 total daily trips, which falls well below the site's trip cap. After accounting for the proposed development of Phases 1 & 2, the site can accommodate future development generating up to 592 daily trips. No additional traffic analysis is required.

Attachments:

Site Plan

Trip Generation

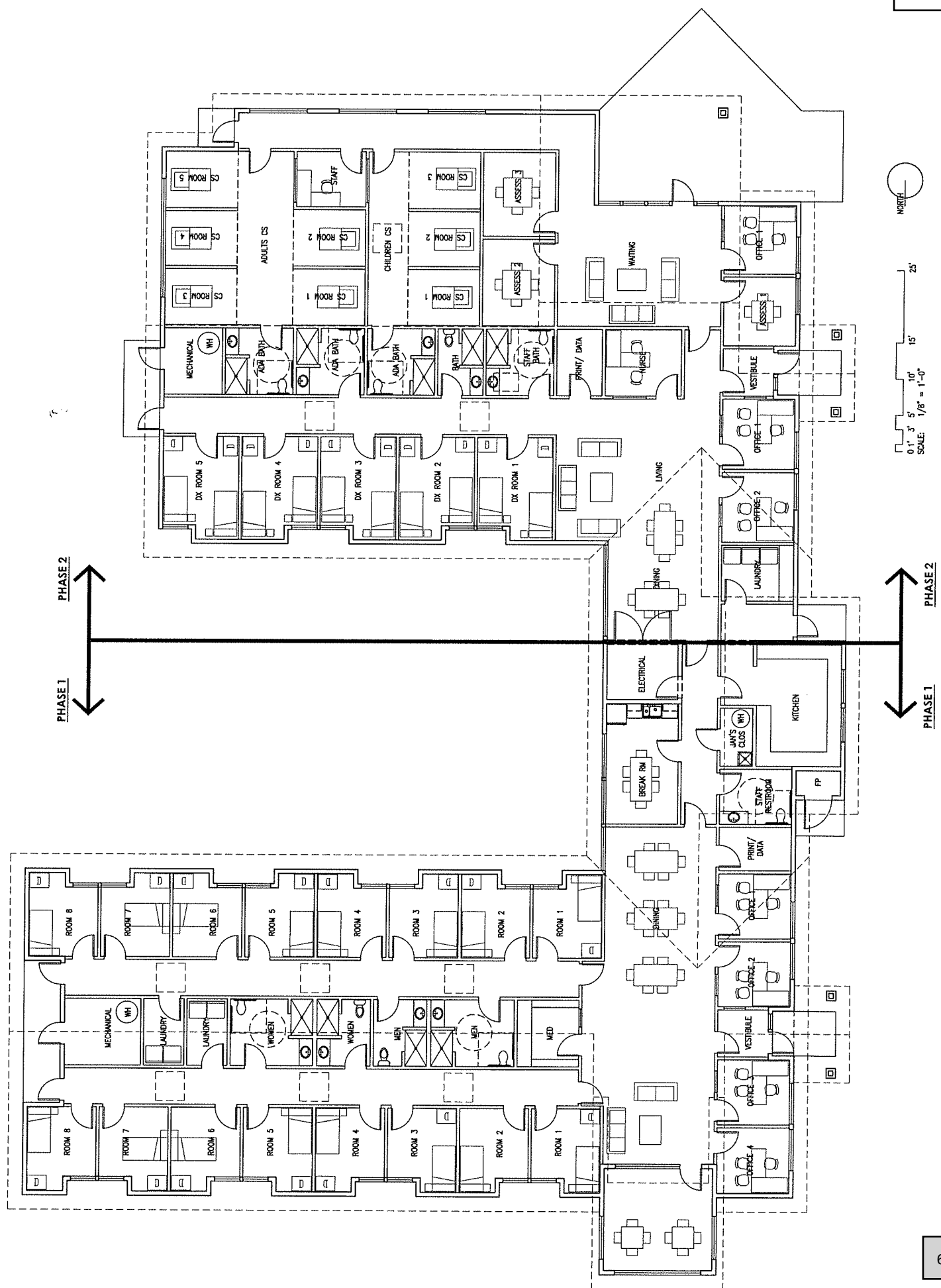




CCMH CAMPUS - PHASE 1 & 2 Building
NOVEMBER 6, 2024
Lot # 4109-BB-00300

PARKING:
 (E) EXISTING PARKING SPAC (SEE NARRATIVE)
 (R) EXISTING PARKING SPACES = 11
 (RN) RECONFIGURED / NEW PARKI
 TOTAL PARKING SPACES = 150
 (S) SURPLUS

SCALE: 1" = 40'
 0' 10' 20' 30' 40' 60'





TRIP GENERATION CALCULATIONS
 Source: Trip Generation Manual, 11th Edition

Land Use: General Office Building
Land Use Code: 710
Land Use Subcategory: All Sites
Setting/Location: General Urban/Suburban
Variable: 1000 SF GFA
Trip Type: Vehicle
Formula Type: Rate
Variable Quantity: **0.8**

WARNING: Variable Quantity is less than Minimum Survey Size for Peak Hours

AM PEAK HOUR

Trip Rate: 1.52

	Enter	Exit	Total
Directional Split	88%	12%	
Trip Ends	1	0	1

PM PEAK HOUR

Trip Rate: 1.44

	Enter	Exit	Total
Directional Split	17%	83%	
Trip Ends	0	1	1

WEEKDAY

Trip Rate: 10.84

	Enter	Exit	Total
Directional Split	50%	50%	
Trip Ends	4	4	8

SATURDAY

Trip Rate: 2.21

	Enter	Exit	Total
Directional Split	50%	50%	
Trip Ends	1	1	2



TRIP GENERATION CALCULATIONS
Source: Trip Generation Manual, 11th Edition

Land Use: Congregate Care Facility
Land Use Code: 253
Land Use Subcategory: All Sites
Setting/Location: General Urban/Suburban
Variable: Dwelling Units
Trip Type: Vehicle
Variable Quantity: **16**

AM PEAK HOUR

Trip Rate: 0.08

	Enter	Exit	Total
Directional Split	58%	42%	
Trip Ends	1	0	1

PM PEAK HOUR

Trip Rate: 0.18

	Enter	Exit	Total
Directional Split	49%	51%	
Trip Ends	1	2	3

WEEKDAY

Trip Rate: 2.21

	Enter	Exit	Total
Directional Split	50%	50%	
Trip Ends	18	18	36

SATURDAY

Trip Rate: 0

	Enter	Exit	Total
Directional Split	50%	50%	
Trip Ends	NA	NA	NA



TRIP GENERATION CALCULATIONS
 Source: Trip Generation Manual, 11th Edition

Land Use: General Office Building
Land Use Code: 710
Land Use Subcategory: All Sites
Setting/Location: General Urban/Suburban
Variable: 1000 SF GFA
Trip Type: Vehicle
Formula Type: Rate
Variable Quantity: **0.65**

WARNING: Variable Quantity is less than Minimum Survey Size for Peak Hours

AM PEAK HOUR

Trip Rate: 1.52

	Enter	Exit	Total
Directional Split	88%	12%	
Trip Ends	1	0	1

PM PEAK HOUR

Trip Rate: 1.44

	Enter	Exit	Total
Directional Split	17%	83%	
Trip Ends	0	1	1

WEEKDAY

Trip Rate: 10.84

	Enter	Exit	Total
Directional Split	50%	50%	
Trip Ends	4	4	8

SATURDAY

Trip Rate: 2.21

	Enter	Exit	Total
Directional Split	50%	50%	
Trip Ends	1	1	2



TRIP GENERATION CALCULATIONS
 Source: Trip Generation Manual, 11th Edition

Land Use: Nursing Home
Land Use Code: 620
Land Use Subcategory: All Sites
Setting/Location: General Urban/Suburban
Variable: Beds
Trip Type: Vehicle
Formula Type: Rate
Variable Quantity: **18**

WARNING: Variable Quantity is less than Minimum Survey Size for Peak Hours

AM PEAK HOUR

Trip Rate: 0.14

	Enter	Exit	Total
Directional Split	72%	28%	
Trip Ends	2	1	3

PM PEAK HOUR

Trip Rate: 0.14

	Enter	Exit	Total
Directional Split	33%	67%	
Trip Ends	1	2	3

WEEKDAY

Trip Rate: 3.06

	Enter	Exit	Total
Directional Split	50%	50%	
Trip Ends	28	28	56

SATURDAY

Trip Rate: 2.32

	Enter	Exit	Total
Directional Split	50%	50%	
Trip Ends	21	21	42

Caution: Small Sample Size

RECEIVED

OCT 7 2024

CITY OF ST. HELENS

J L J Earthmovers, LLC
P.O. BOX 33
SCAPPOOSE, OREGON 97056
Office: (503) 397-9105
Fax: (503) 397-9088



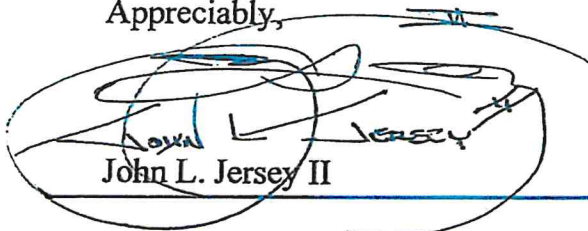
October 7, 2024

Jacob,

I just had the opportunity to meet up with our neighbor down the street on the south side of Gable Road, the Facilities Maintenance Manager at Columbia County Mental Health, Mathew Remsburg. I understand that there is a hearing taking place at your office downtown tomorrow (10/8) evening at 6 pm. I hope to be able to attend this hearing but at a minimum I need to express my major concerns about the improvements that the City of St. Helens are requiring for the center median turn in and turn out lanes built very similar as I understand it from Mathew like and kind as the apartments west of our property. I know their design is showing paint delineation, but Matthew has made it very clear at this point that you folks wishes and requirements to move this project forward. This would intel concrete medians to be installed directly in front of our west side property driveway which would not allow us at this juncture looking at the drawing to turn into our property off of gable road if we were headed east. If we were headed west the turning radius to make an ingress approach to our driveway, we would be hampered with not enough turning radius with both our truck and trailers along with our lowboy tractor and trailer and we would blow that concrete curbing with our lowboy tractor and trailer.

I don't know what the intentions are with Suzie Dahl's family's property for future expansion, but I would think the same thing would apply to that property to what I am seeing on the drawings. I hope to be at tomorrow nights hearing at 6 pm to address these concerns in person because these could become a major impact for our work operations moving forward. I would like to request at this time since this is the first time I am hearing about this, to be kept in the loop as decisions are being made for the proposed development and potential impacts on Gable Road. As you are aware the week before last, I was in to pay for and obtain the Right-of-Way permit for our new location for ingress egress. Jacob, I need to be kept in the loop on this proposed project moving forward.

Appreciably,


John L. Jersey II

October 7, 2024

Please place this in public record comments

RECEIVED
OCT 7 2024
CITY OF ST. HELENS

To: City of St. Helens
From: Suzie Dahl on behalf of Leila Wilson – owner of 10 acre light industrial land
Attn: City Planning Commission and/or City Council
**Re: Conditional Use Permit, Unlisted Use Permit, and Variance at 58646 McNulty Way –
AKAAN Architecture Design, LLC (Continued from September)**

Dear Planning Commission and/or City Council Members,

This letter is to serve as opposition to the City of St. Helens Road Standards in consideration of the future expansion and re-development for the CCMH campus along Gable Road. I am the current owner of the 10-acre light industrial property, directly across Gable Road to the North of the CCMH development.

The St. Helens Riverfront Connector Plan Options A-C (identified on pages 89-91) shows a center median to be landscaped, however in the side notes this plan identified “Implementation of the median and turn pockets would require anticipation of the location of future development along the road. In addition, local emergency service providers may have issues related to their ability to pass motorists if needed in the event of an emergency”. The design from AKAAN Architecture also show this area and blocking access from Gable Road Eastbound to neighboring properties to the North.

A recommended St. Helens Riverfront Connector Plan was approved with support from the city Advisory Committee, Planning Commission and City Council, (page 130) state the following “This cross-section also includes a center lane to accommodate turn lanes needed at key intersections or other access points to maintain traffic mobility. A landscaped median could be used in short portions of this segment but will need to be located to continue to ensure access to local businesses in this part of the corridor”. Please note: this language states could, not shall.

The recommended plan does not include language for local emergency service providers and access. While this may have been an oversight, it should be addressed for this development. With Broadleaf Manor Apartments completed, additional traffic accumulation and lack of an updated TIA, the Gable Road Riverfront Connector Plan should no longer be used for this area due to current traffic patterns and vehicle numbers. Gable Road is no longer a minor arterial street; it has become a major arterial road that connects to downtown St. Helens, (only Gable and Columbia Blvd. provide this access to Hwy 30 for standard traffic patterns).

Emergency services (fire and police) are already at maximum capacity and being called out multiple times each day to the apartment complex. Emergency response has also been called to our undeveloped land on Gable Road next to Broadleaf Manor due to people trespassing and setting fires on our property (3 times in one week). Currently there is access to our 10-acre light industrial land, if a median is in place there is no access on Gable Road eastbound. Gable Road should be widened and only have a center turn lane with street trees by the sidewalk.

Having a median with turns and trees is a recipe for disaster as it will impair sight lines for turning into oncoming vehicles. If a continuous median is in place, truck traffic for the light industrial lands in this location would need to find a place to turn around near Railroad Ave., Port Ave., Kaster Road, etc. in order to obtain access to properties to the North of Gable which in turn creates an unsafe situation and a burden on the more residential areas of Old Portland Road with future truck traffic.

This would increase even longer delays and multiple vehicle stops along North Gable Road when these lands are fully developed. Semi-trucks with trailers need to take a wide sweep for proper turning and as for emergency response, these types of delays and no access to get around vehicles could be the matter of life or death.

Of other concern, the center median improvement should not be completed by the developer for construction since this is a public right of way; developers pay a fee for sidewalk improvements through SDC charges, etc. For the city to put all this responsibility upon the developer creates a burden to construction and growth, therefore the city should increase fees for all development to save funds for this type of public work.

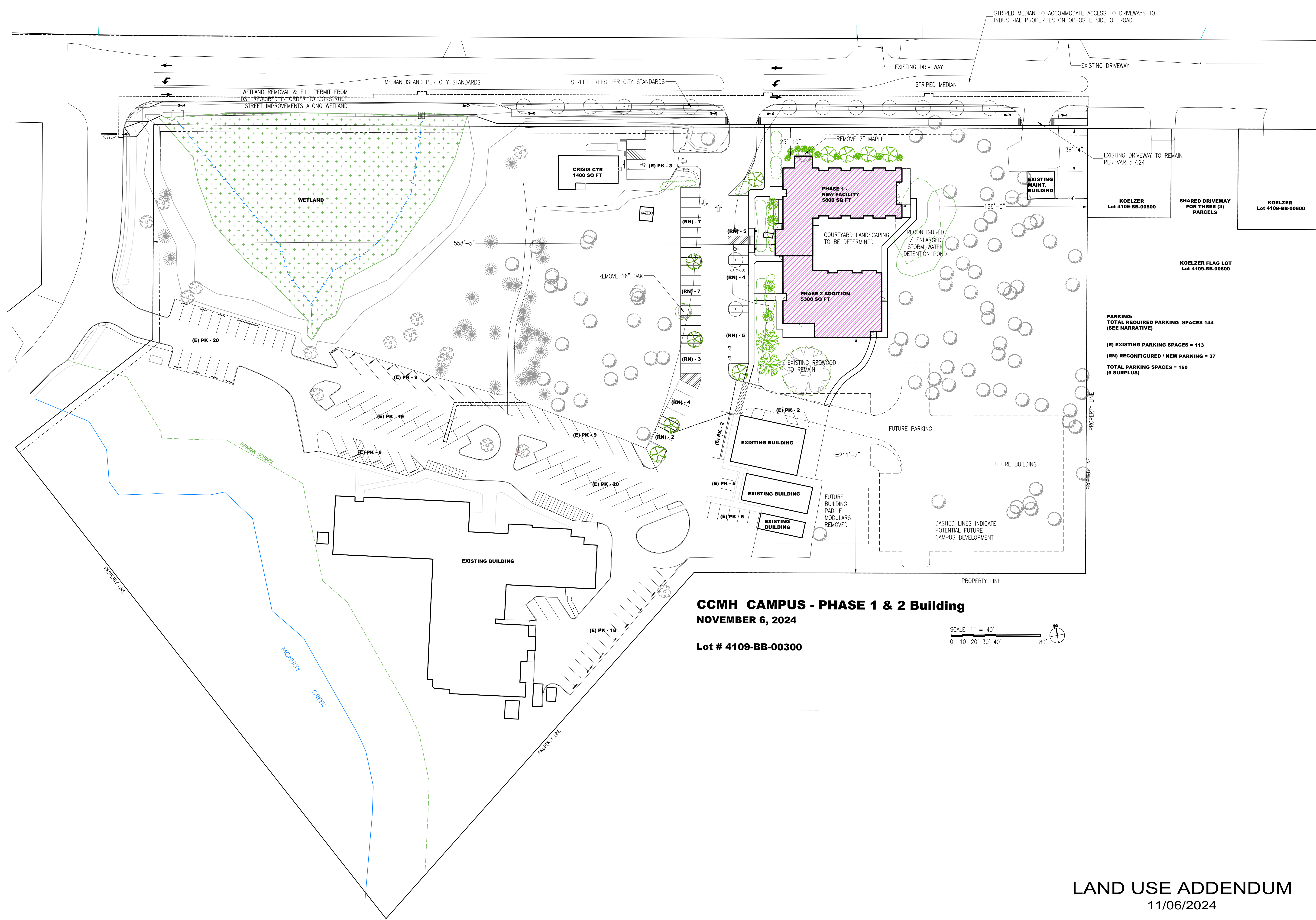
In closing:

A center median placed along Gable Road in an industrial area without consideration for future development or emergency access is not acceptable. This would entail the city require the initial permit and construction costs (burden of developer) for placement of the median and a second permit and construction costs (burden of developer) to remove a portion of that median by a future developer of undeveloped land to gain access.

This plan doesn't make any sense; build it right the first time and widen Gable Road with a center turn that can both be used for turning with clear sight and emergency response vehicles (as shown on page 131 of the adopted plan).

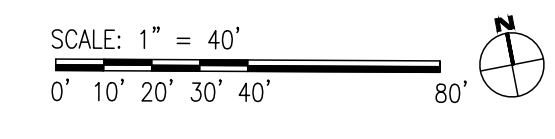
Thank you for your consideration and look forward to a reasonable solution for the safety of St. Helens residents, emergency personnel, business owners and visitors.

Respectfully,
Suzie Dahl on behalf of Leila Wilson – sent via email



CCMH CAMPUS - PHASE 1 & 2 Building
NOVEMBER 6, 2024

Lot # 4109-BB-00300



PARKING:
TOTAL REQUIRED PARKING SPACES 144
 (SEE NARRATIVE)
(E) EXISTING PARKING SPACES = 113
(RN) RECONFIGURED / NEW PARKING = 37
TOTAL PARKING SPACES = 150
(6 SURPLUS)

PROJECT TEAM:

CIVIL ENGINEER:
 HUMBER DESIGN GROUP, Inc.
 110 SE Main Street, Suite 200,
 Portland, OR 97214
 P: 503.488.5711

STRUCTURAL ENGINEER:
 PETERSON STRUCTURAL ENGINEERS
 9400 SW Barnes Road, Suite 100
 Portland, OR 97225
 P: 503.292.1636

MEP ENGINEER:
 NIKE & Associates, Inc.
 6915 SW Macadam Ave, Suite 200
 Portland, OR 97219
 P: 503.892.1188

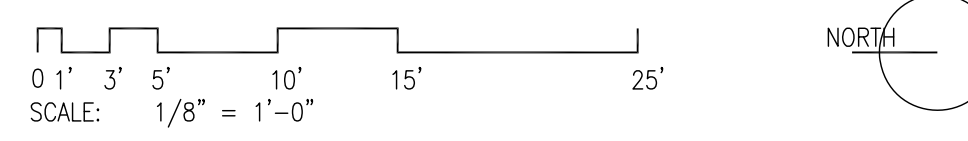
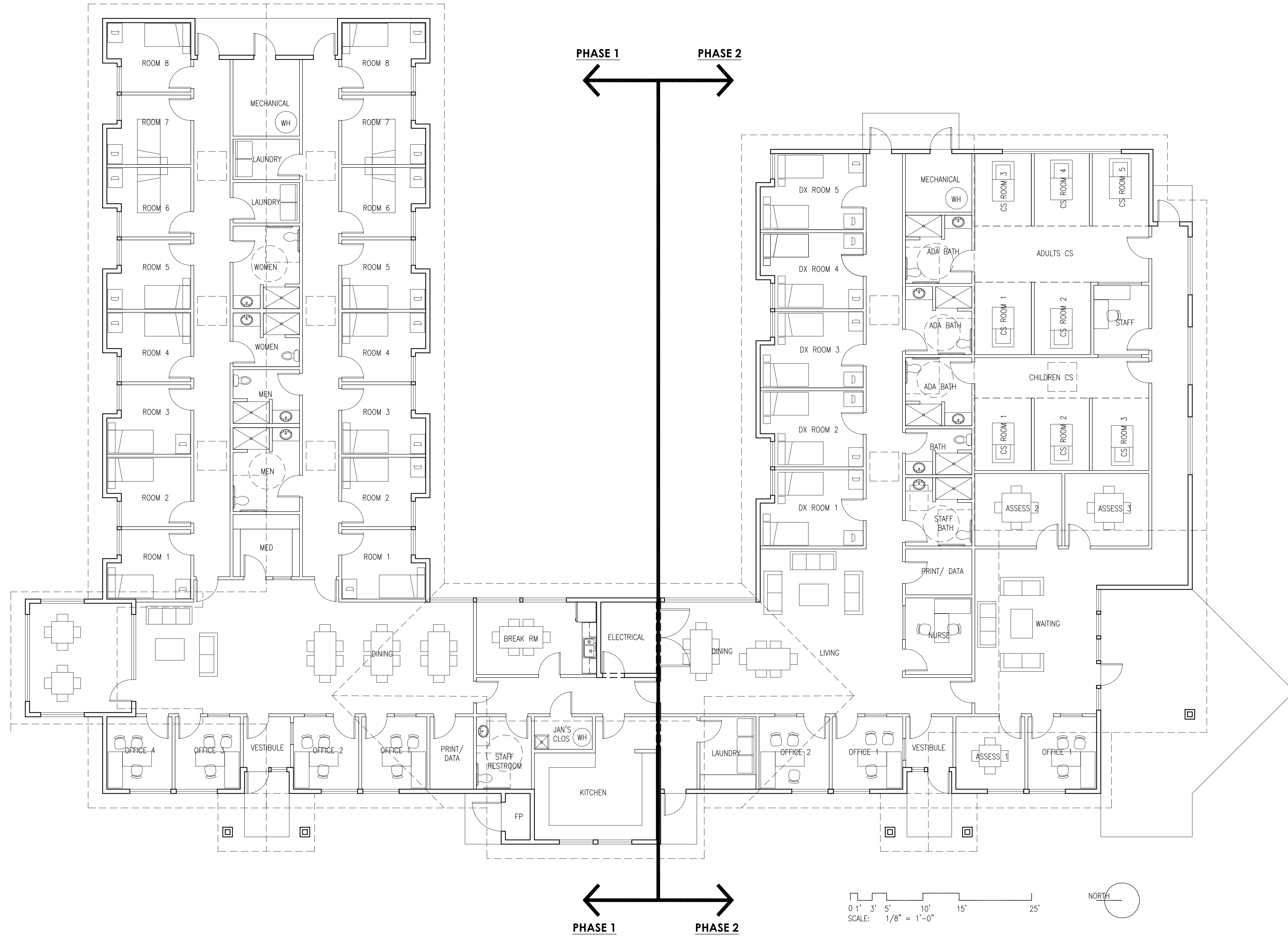
CCMH - NEW FACILITY
 Columbia Community Mental Health
 58646 McNULTY WAY,
 ST HELENS, OREGON 97051

SCALE: AS NOTED
 DRAWN BY: -
 CHECKED BY: -
 CAD FILE: -
 DATE: -

REVISIONS	
△	DESCRIPTION

CONTENTS:
 PHASE 1 & 2
 SITE PLAN

SHEET NO:



PROJECT TEAM:

CIVIL ENGINEER:
HUMBER DESIGN GROUP, Inc.
110 SE Main Street, Suite 200,
Portland, OR 97214
P: 503.488.5711

STRUCTURAL ENGINEER:
PETERSON STRUCTURAL ENGINEERS
9400 SW Barnes Road, Suite 100
Portland, OR 97225
P: 503.292.1635

MEP ENGINEER:
NIKE & Associates, Inc.
6915 SW Macadam Ave, Suite 200
Portland, OR 97219
P: 503.892.1188

CCMH - NEW FACILITY

Columbia Community Mental Health
58646 McNULTY WAY,
ST HELENS, OREGON 97051

SCALE: AS NOTED
 DRAWN BY: ap
 CHECKED BY: ap
 CAD FILE: 1704-plan-opt-d-phase2
 DATE: -

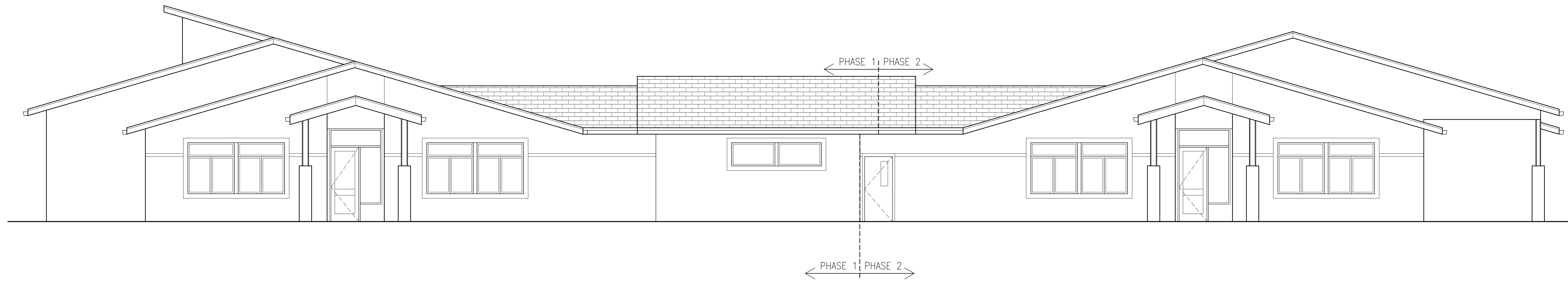
REVISIONS	
△	DESCRIPTION

CONTENTS:
FLOOR PLAN

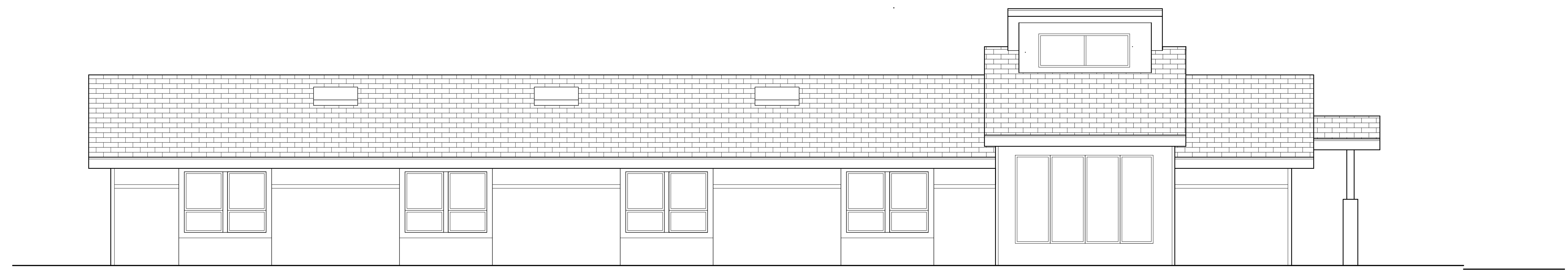
SHEET NO:

LAND USE ADDENDUM
09/30/2024

A1.1



1
A2.1 ELEVATION - WEST
1/4" = 1'-0"



2
A2.1 ELEVATION - NORTH (CABLE ROAD)
1/4" = 1'-0"

PROJECT TEAM:

CIVIL ENGINEER:
HUMBER DESIGN GROUP, Inc.
110 SE Main Street, Suite 200,
Portland, OR 97214
P: 503.488.5711

STRUCTURAL ENGINEER:
PETERSON STRUCTURAL ENGINEERS
9400 SW Barnes Road, Suite 100
Portland, OR 97225
P: 503.292.1636

MEP ENGINEER:
NIKE & Associates, Inc.
6915 SW Macadam Ave, Suite 200
Portland, OR 97219
P: 503.892.1188

CCMH - NEW FACILITY
Columbia Community Mental Health
58646 McNULTY WAY,
ST HELENS, OREGON 97051

SCALE: AS NOTED
DRAWN BY: KP
CHECKED BY: -
CAD FILE: 1704_ELEV
DATE: 4/29/2024

REVISIONS

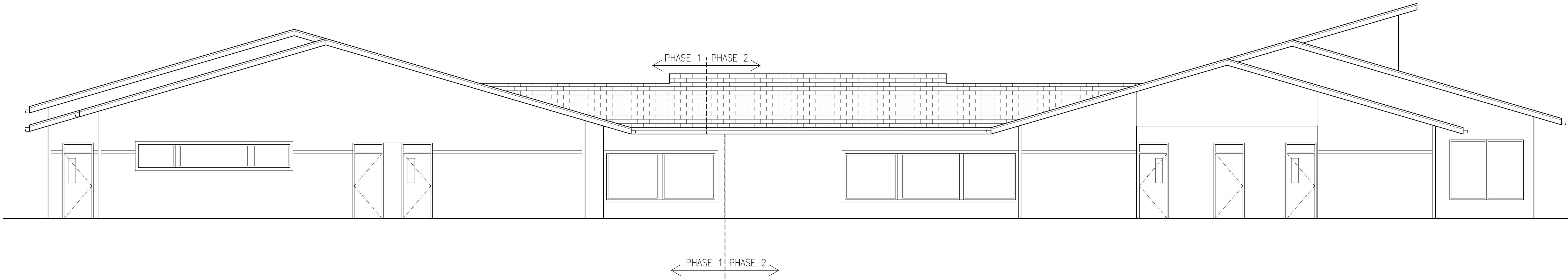
△	DATE	DESCRIPTION

CONTENTS:
EXTERIOR ELEVATIONS

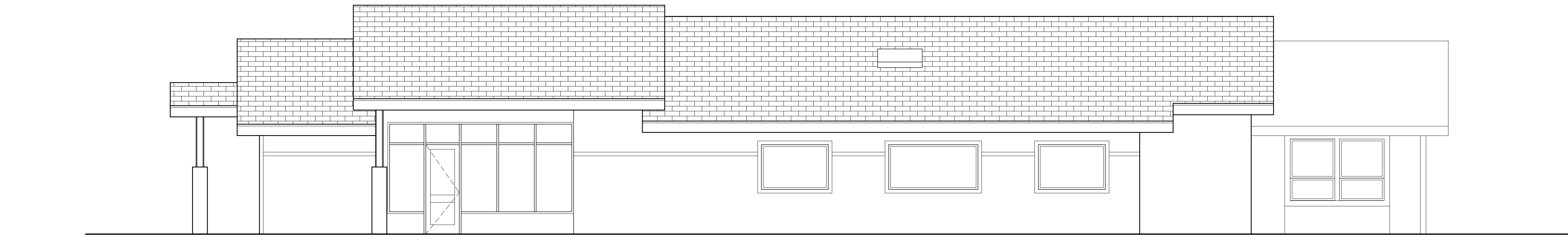
SHEET NO:

LAND USE ADDENDUM
09/30/2024

A2.1



1 ELEVATION - EAST
A2.2 1/4" = 1'-0"



2 ELEVATION - SOUTH
A2.2 1/4" = 1'-0"

PROJECT TEAM:

CIVIL ENGINEER:
HUMBER DESIGN GROUP, Inc.
110 SE Main Street, Suite 200,
Portland, OR 97214
P: 503.488.5711

STRUCTURAL ENGINEER:
PETERSON STRUCTURAL ENGINEERS
9400 SW Barnes Road, Suite 100
Portland, OR 97225
P: 503.292.1636

MEP ENGINEER:
NIKE & Associates, Inc.
6915 SW Macadam Ave, Suite 200
Portland, OR 97219
P: 503.892.1188

CCMH - NEW FACILITY

Columbia Community Mental Health
58646 McNULTY WAY,
ST HELENS, OREGON 97051

SCALE: AS NOTED
DRAWN BY: KP
CHECKED BY: -
CAD FILE: 1704_ELEV
DATE: 4/29/2024

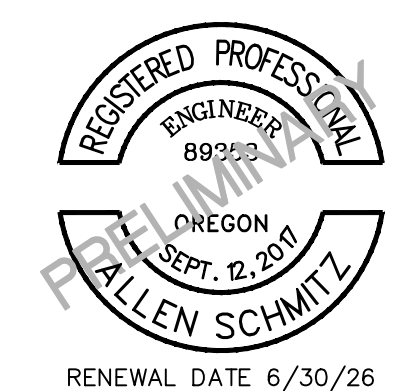
REVISIONS	
△	DESCRIPTION

CONTENTS:
EXTERIOR ELEVATIONS

SHEET NO:

LAND USE ADDENDUM
09/30/2024

A2.2



SHEET LEGEND		
	PRIVATE ASPHALT PAVEMENT	SEE DTL 1 SHT C4.00
	PRIVATE CONCRETE SIDEWALK	SEE DTL 3 SHT C4.00
	PRIVATE CONCRETE PAVEMENT	SEE DTL 2 SHT C4.00
	PUBLIC ASPHALT PAVEMENT	
	PUBLIC CONCRETE SIDEWALK	
	PUBLIC CONCRETE PAVEMENT	
	STORMWATER FACILITY	SEE DTL 1 SHT C4.01
	LANDSCAPING	SEE LANDSCAPE PLANS
	STANDARD CURB	SEE DTL 2 SHT C4.00
	WHEEL STOP	SEE DTL 5 SHT C4.00

PROJECT TEAM:

CIVIL ENGINEER:
HUMBER DESIGN GROUP, Inc.
110 SE Main Street, Suite 200,
Portland, OR 97214
P: 503.488.5711

STRUCTURAL ENGINEER:
PETERSON STRUCTURAL ENGINEERS
9400 SW Barnes Road, Suite 100
Portland, OR 97225
P: 503.292.1635

MEP ENGINEER:
NIKE & Associates, Inc.
6915 SW Macadam Ave, Suite 200
Portland, OR 97219
P: 503.892.1188



CCMH - NEW FACILITY
Columbia Community Mental Health
58646 McNulty Way,
ST HELENS, OREGON 97051

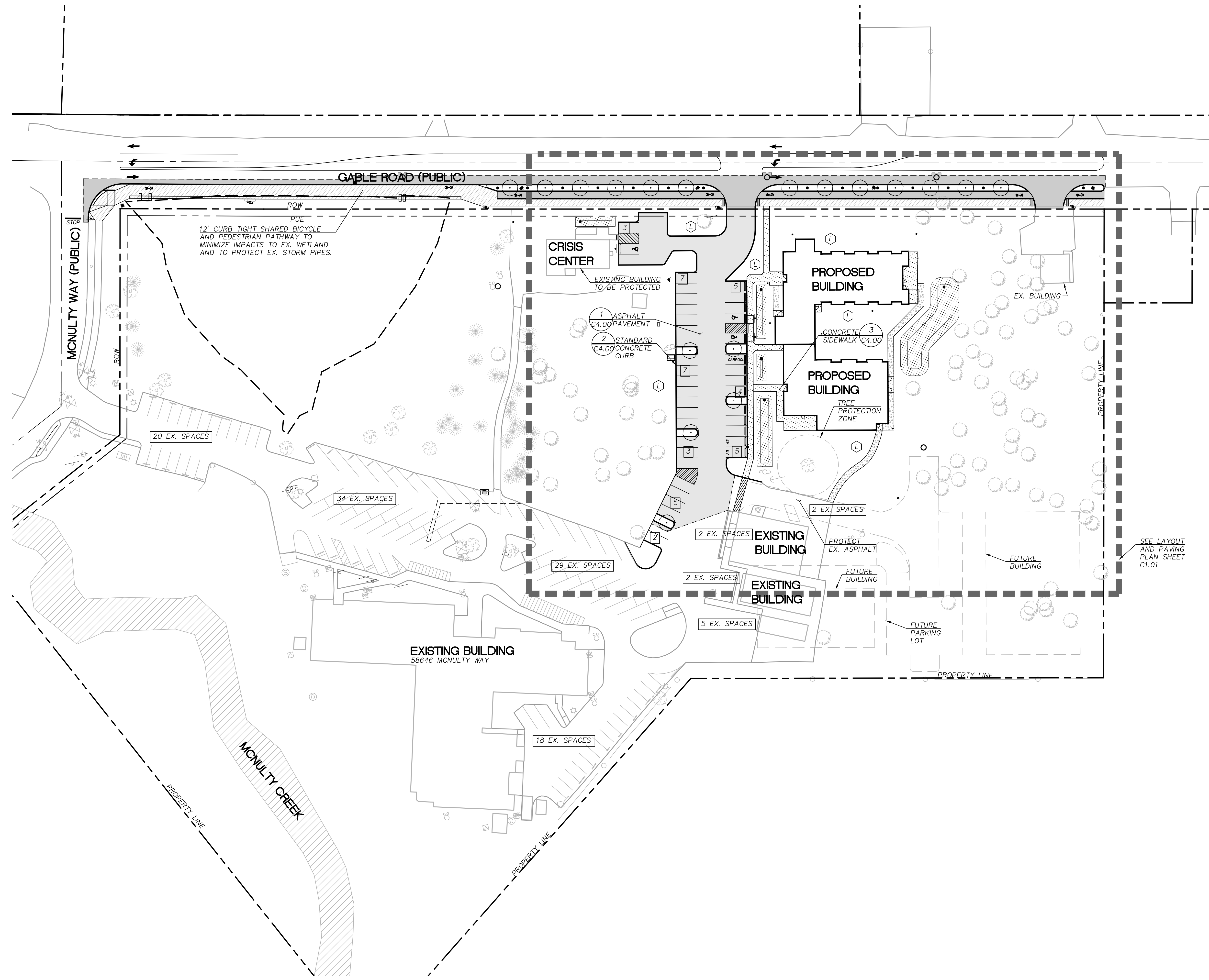
SCALE:
DRAWN BY:
CHECKED BY:
CAD FILE:
DATE: 10/21/2024

REVISIONS	
DATE	DESCRIPTION

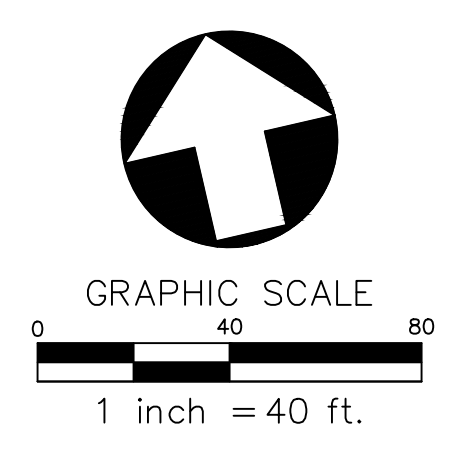
CONTENTS:
OVERALL LAYOUT AND PAVING PLAN

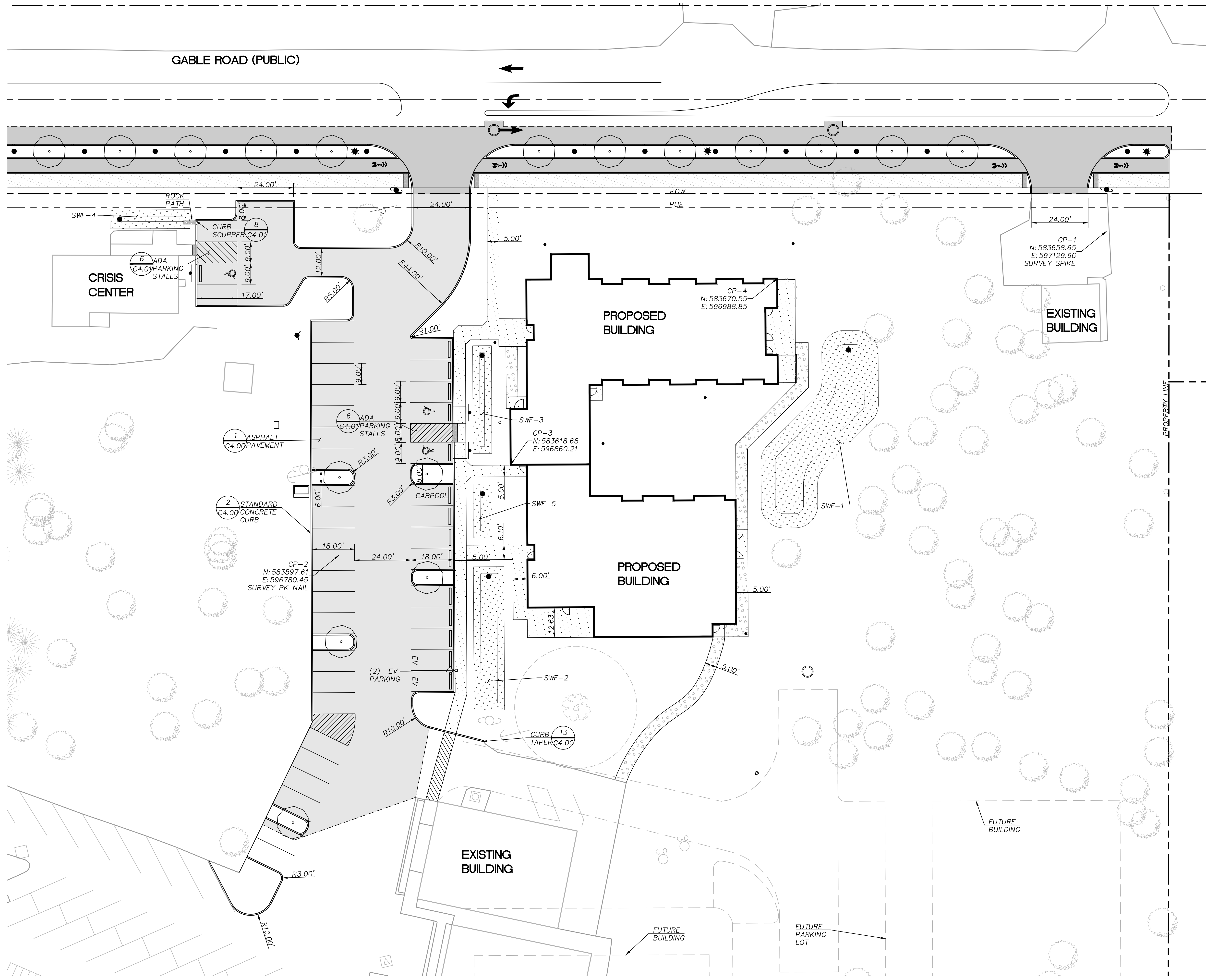
SHEET NO:

C1.00



OVERALL LAYOUT AND PAVING PLAN
SCALE: 1"=40'





SHEET LEGEND		
	PRIVATE ASPHALT PAVEMENT	SEE DTL 1 SHT C4.00
	PRIVATE CONCRETE SIDEWALK	SEE DTL 3 SHT C4.00
	PRIVATE CONCRETE PAVEMENT	SEE DTL 2 SHT C4.00
	PUBLIC ASPHALT PAVEMENT	
	PUBLIC CONCRETE SIDEWALK	
	PUBLIC CONCRETE PAVEMENT	
	STORMWATER POND	SEE DTL 1 SHT C4.01
	LANDSCAPING	SEE LANDSCAPE PLANS
	STANDARD CURB	SEE DTL 2 SHT C4.00
	WHEEL STOP	SEE DTL 5 SHT C4.00

AKAAN
architecture + design llc

REGISTERED PROFESSIONAL ENGINEER
89223
OREGON
SEPT. 2, 2019
ALLEN SCHMITZ
RENEWAL DATE 6/30/26

PROJECT TEAM:
 CIVIL ENGINEER:
 HUMBER DESIGN GROUP, Inc.
 110 SE Main Street, Suite 200,
 Portland, OR 97214
 P. 503.488.5711
 STRUCTURAL ENGINEER:
 PETERSON STRUCTURAL ENGINEERS
 9400 SW Barnes Road, Suite 100
 Portland, OR 97225
 P. 503.292.1636
 MEP ENGINEER:
 NIKE & Associates, Inc.
 6915 SW Macadam Ave, Suite 200
 Portland, OR 97219
 P. 503.892.1188

Humber Design Group, Inc.
Civil Engineering • 503.946.6690 • hdbgdx.com

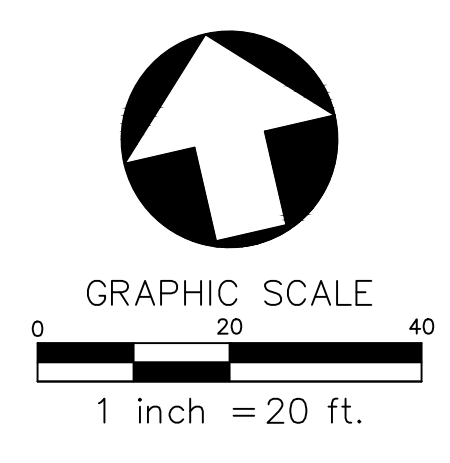
CCMH - NEW FACILITY
 Columbia Community Mental Health
 58646 McNULTY WAY,
 ST HELENS, OREGON 97051

SCALE:
 DRAWN BY:
 CHECKED BY:
 CAD FILE:
 DATE: 10/21/2024

REVISIONS
 Δ DATE DESCRIPTION

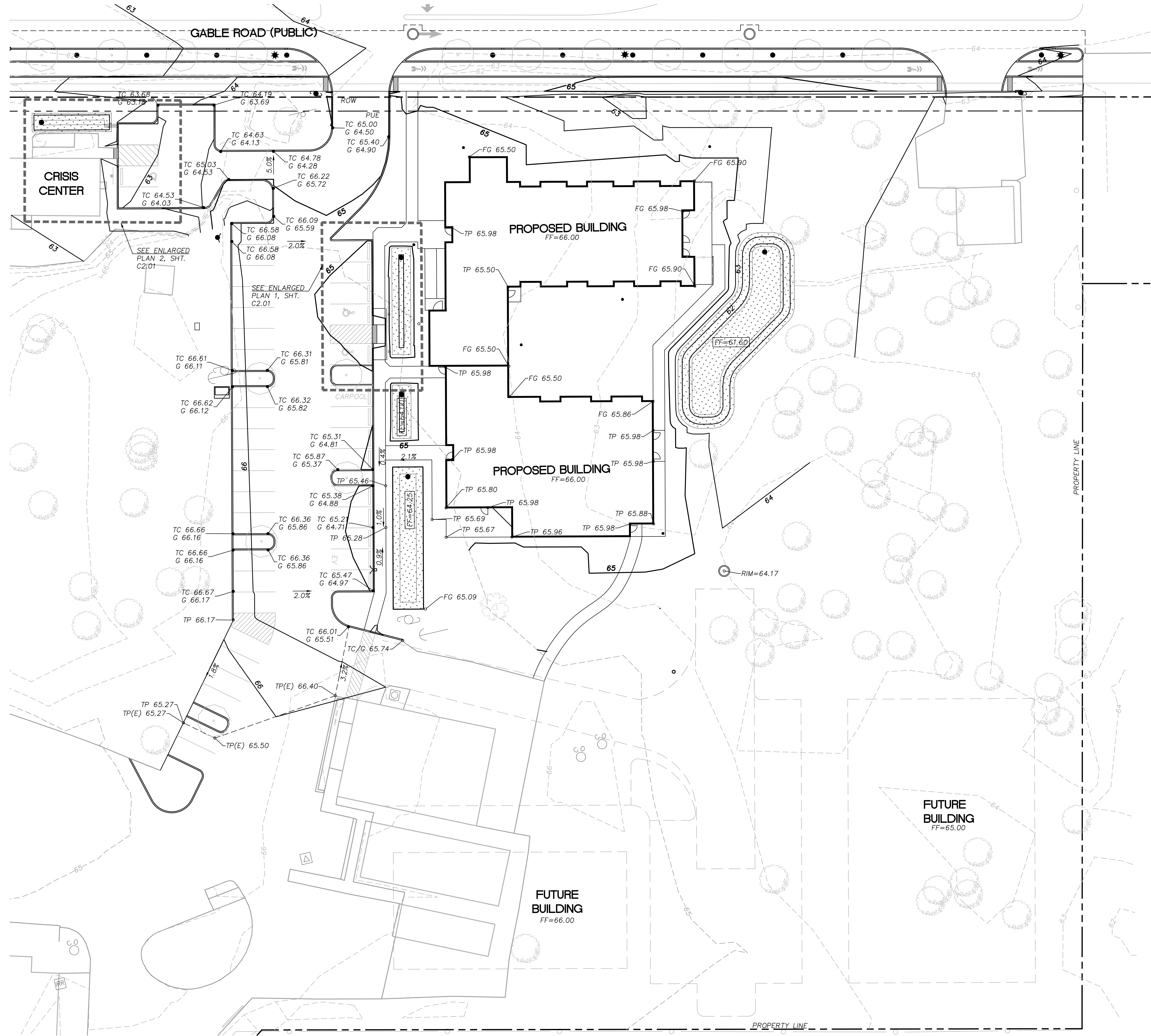
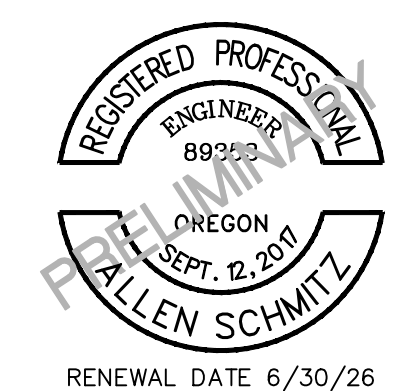
CONTENTS:
 LAYOUT AND PAVING PLAN

SHEET NO:



LAYOUT AND PAVING PLAN
 SCALE: 1"=20'

C1.01



SHEET LEGEND	
---259---	EXISTING CONTOUR
---260---	PROPOSED CONTOUR
G XXX.XX	GRADE AT GUTTER
TC XXX.XX	GRADE AT TOP OF CURB
TP XXX.XX	GRADE AT TOP OF PAVEMENT
TF XXX.XX	GRADE AT TOP OF FOOTING
FF XX.XX	FINISH FLOOR ELEVATION
FG XXX.XX	FINISHED GRADE
TW XXX.XX	GRADE AT TOP OF WALL
EG XXX.XX	EXISTING GRADE
(E)	EXISTING
-X.X%	SLOPE ARROW
GB	GRADE BREAK

PROJECT TEAM:

CIVIL ENGINEER:
HUMBER DESIGN GROUP, Inc.
110 SE Main Street, Suite 200,
Portland, OR 97214
P: 503.488.5711

STRUCTURAL ENGINEER:
PETERSON STRUCTURAL ENGINEERS
9400 SW Barnes Road, Suite 100
Portland, OR 97225
P: 503.292.1635

MEP ENGINEER:
NIKE & Associates, Inc.
6915 SW Macadam Ave, Suite 200
Portland, OR 97219
P: 503.892.1188



CCMH - NEW FACILITY
Columbia Community Mental Health
58646 McNULTY WAY,
ST HELENS, OREGON 97051

SCALE:
DRAWN BY:
CHECKED BY:
CAD FILE:
DATE: 10/21/2024

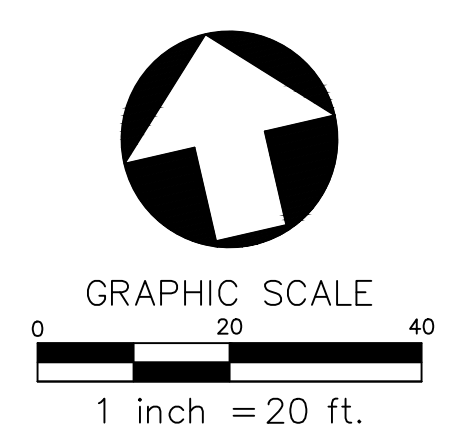
REVISIONS	
Δ	DATE DESCRIPTION

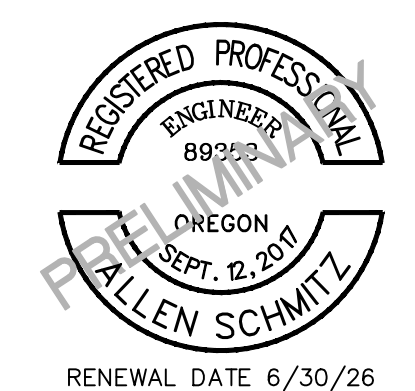
CONTENTS:
GRADING PLAN

SHEET NO:

C2.00

GRADING PLAN
SCALE: 1"=20'





SHEET LEGEND	
--- 259 ---	EXISTING CONTOUR
— 260 —	PROPOSED CONTOUR
G XXX.XX	GRADE AT GUTTER
TC XXX.XX	GRADE AT TOP OF CURB
TP XXX.XX	GRADE AT TOP OF PAVEMENT
TF XXX.XX	GRADE AT TOP OF FOOTING
FF XX.XX	FINISH FLOOR ELEVATION
FG XXX.XX	FINISHED GRADE
TW XXX.XX	GRADE AT TOP OF WALL
EG XXX.XX	EXISTING GRADE
(E)	EXISTING
X.X%	SLOPE ARROW
GB	GRADE BREAK

PROJECT TEAM:

CIVIL ENGINEER:
HUMBER DESIGN GROUP, Inc.
110 SE Main Street, Suite 200,
Portland, OR 97214
P: 503.488.5711

STRUCTURAL ENGINEER:
PETERSON STRUCTURAL ENGINEERS
9400 SW Barnes Road, Suite 100
Portland, OR 97225
P: 503.292.1635

MEP ENGINEER:
NIKE & Associates, Inc.
6915 SW Macadam Ave, Suite 200
Portland, OR 97219
P: 503.892.1188



CCMH - NEW FACILITY
Columbia Community Mental Health
58646 McNULTY WAY,
ST HELENS, OREGON 97051

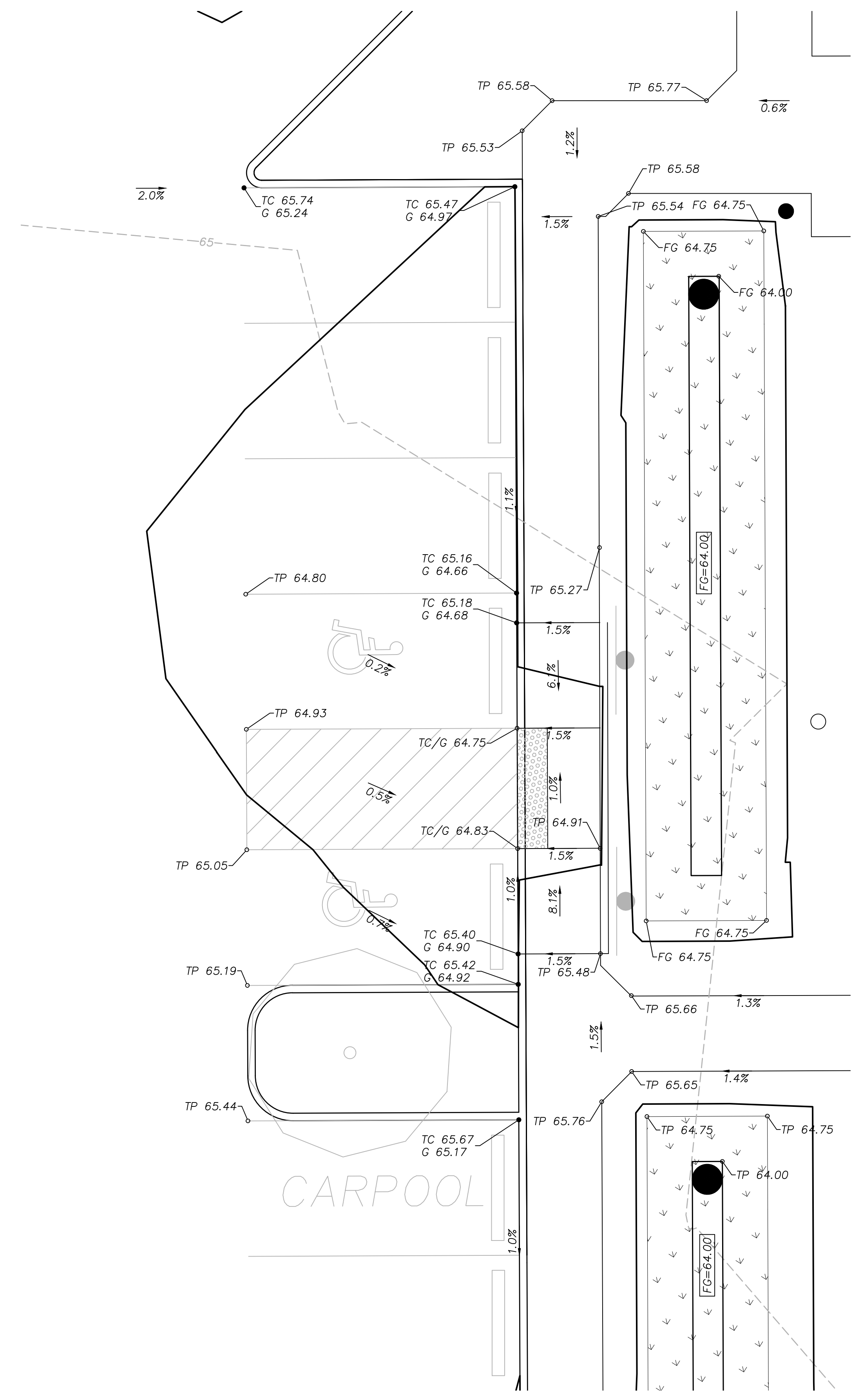
SCALE:
DRAWN BY:
CHECKED BY:
CAD FILE:
DATE: 10/21/2024

REVISIONS		
Δ	DATE	DESCRIPTION

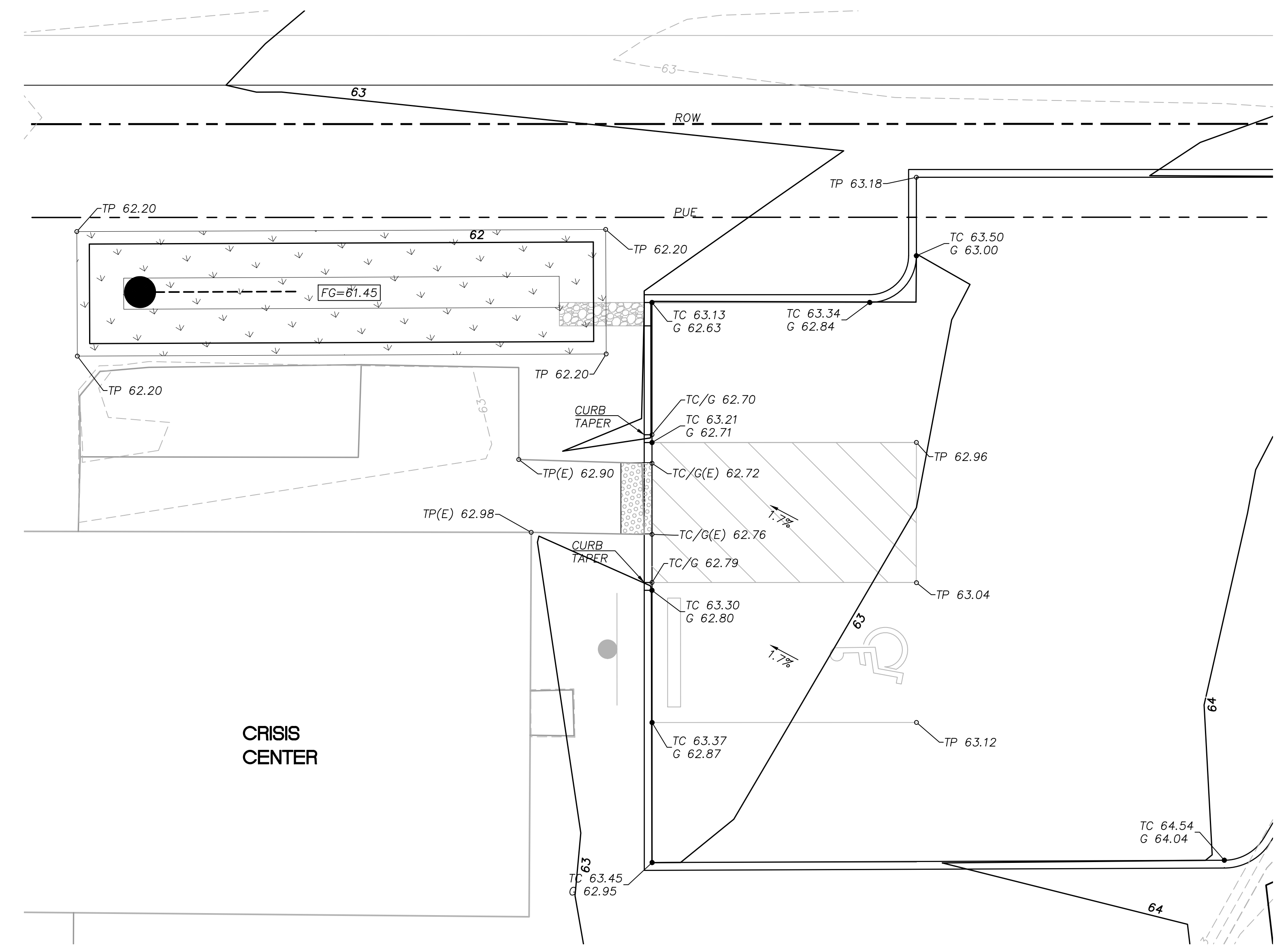
CONTENTS:
GRADING PLAN

SHEET NO:

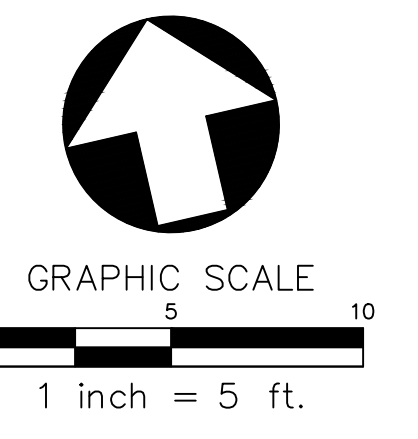
C2.01

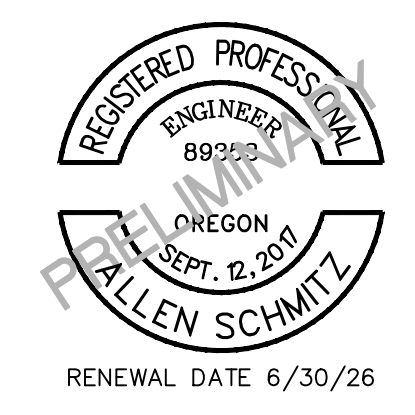


ENLARGED GRADING PLAN - 1
SCALE: 1"=5'



ENLARGED GRADING PLAN - 2
SCALE: 1"=5'





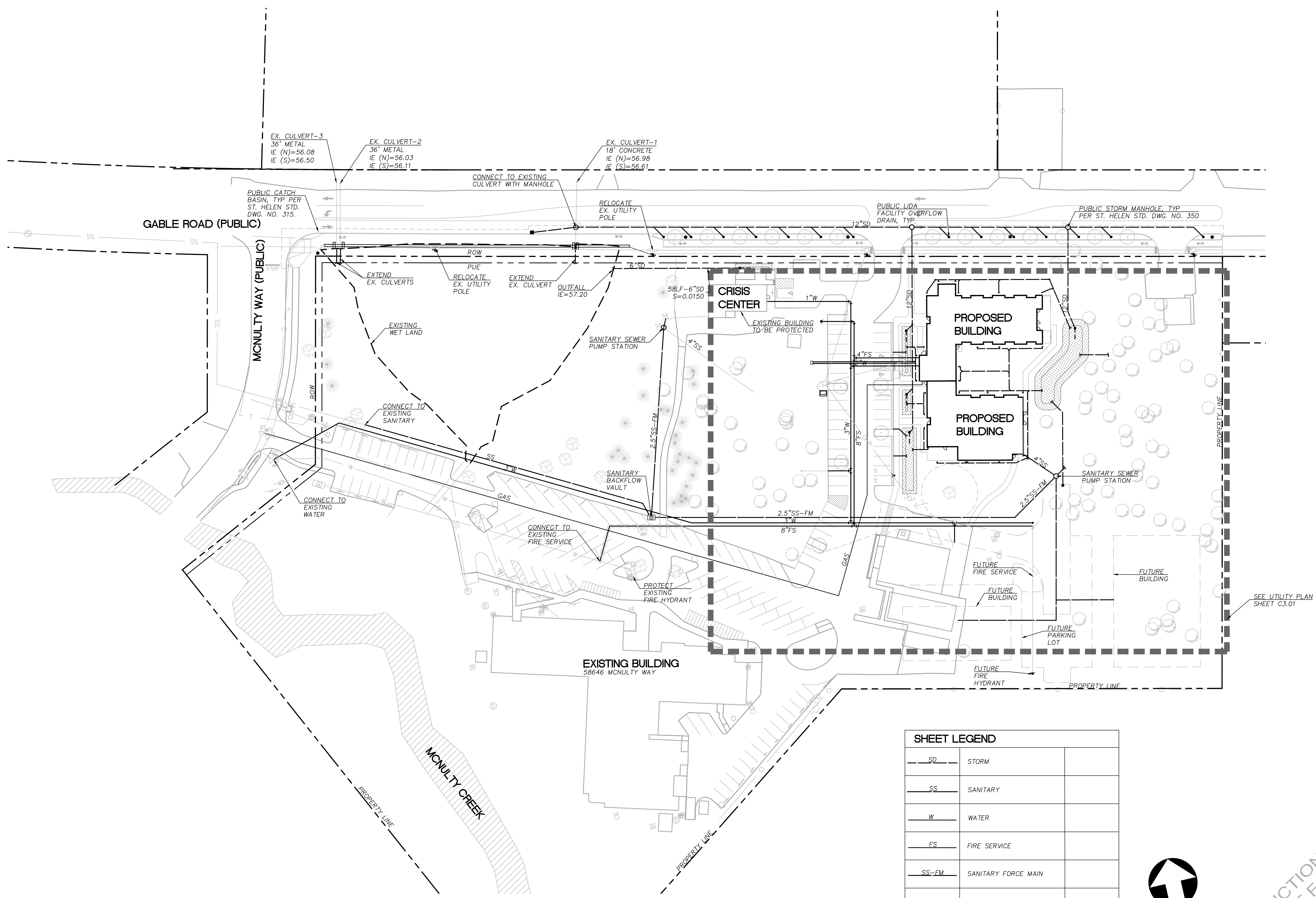
PROJECT TEAM:
CIVIL ENGINEER:
HUMBER DESIGN GROUP, Inc.
110 SE Main Street, Suite 200,
Portland, OR 97214
P: 503.488.5711
STRUCTURAL ENGINEER:
PETERSON STRUCTURAL ENGINEERS
9400 SW Barnes Road, Suite 100
Portland, OR 97225
P: 503.292.1635
MEP ENGINEER:
NIKE & Associates, Inc.
6915 SW Macadam Ave, Suite 200
Portland, OR 97219
P: 503.892.1188



CCMH - NEW FACILITY
Columbia Community Mental Health
58646 McNulty Way,
ST HELENS, OREGON 97051

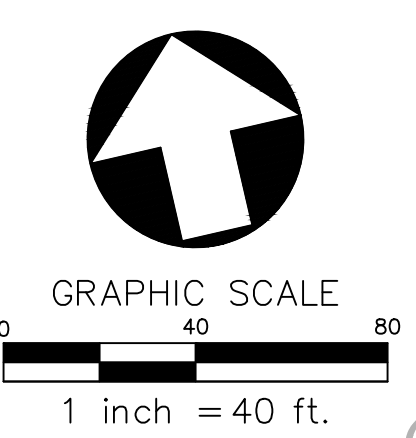
SCALE:
DRAWN BY:
CHECKED BY:
CAD FILE:
DATE: 10/21/2024
REVISIONS
DATE DESCRIPTION
CONTENTS:
OVERALL UTILITY PLAN
SHEET NO:

C3.00

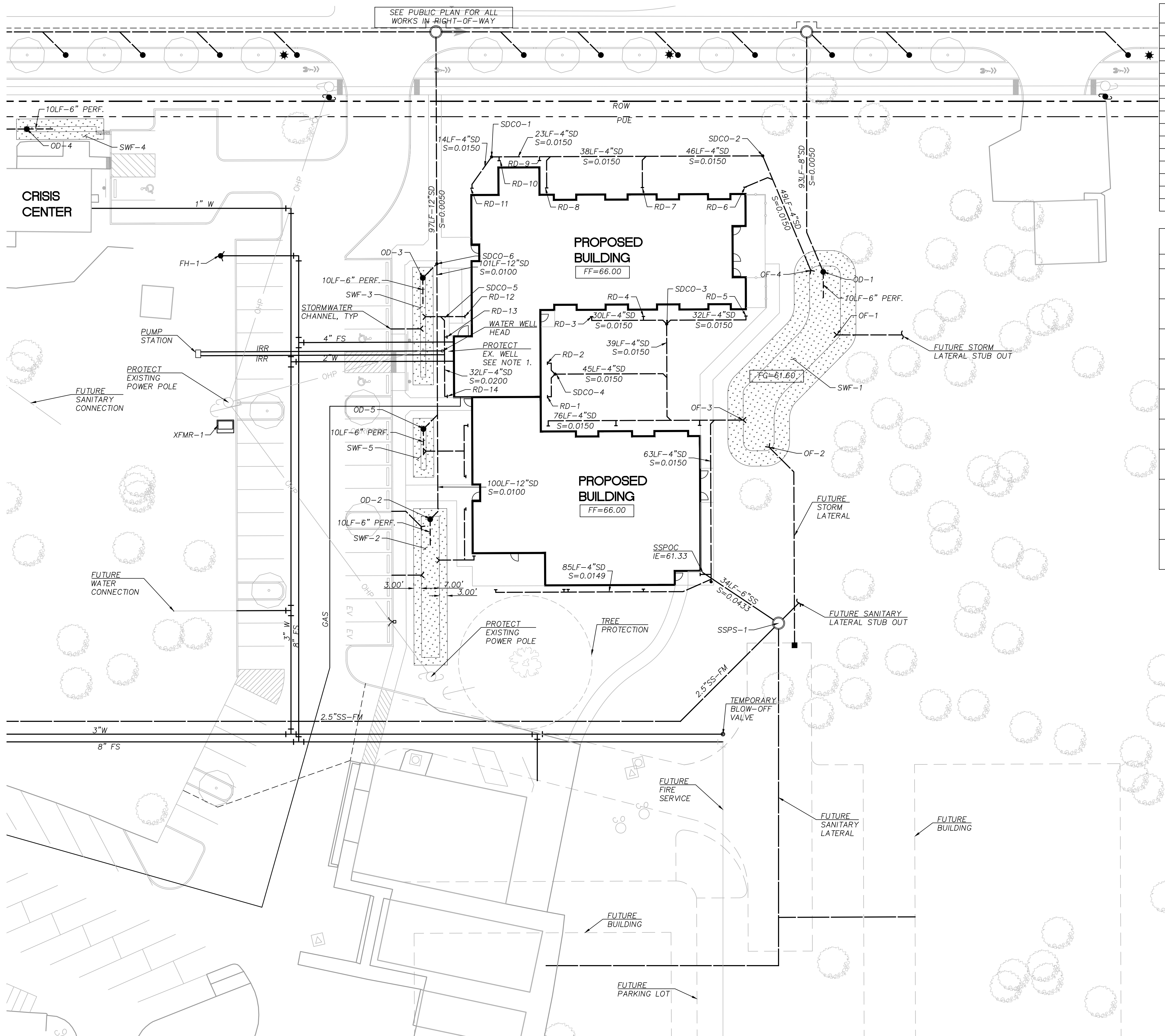


OVERALL UTILITY PLAN
SCALE: 1"=40'

SHEET LEGEND	
SD	STORM
SS	SANITARY
W	WATER
FS	FIRE SERVICE
SS-FM	SANITARY FORCE MAIN
■	CATCH BASIN
●	CLEANOUT



CONSTRUCTION DESIGN
NOT FOR
CONSTRUCTION



ROOF DRAIN SCHEDULE

NO.	SIZE	LATERAL	INVERT
RD-1	4.0"	1.5 LF	IE=63.08
RD-2	4.0"	1.5 LF	IE=63.02
RD-3	4.0"	1.5 LF	IE=63.04
RD-4	4.0"	1.5 LF	IE=62.74
RD-5	4.0"	1.5 LF	IE=63.07
RD-6	4.0"	2 LF	IE=62.35
RD-7	4.0"	12 LF	IE=63.20
RD-8	4.0"	12 LF	IE=63.76
RD-9	4.0"	1.5 LF	IE=63.63
RD-10	4.0"	1.5 LF	IE=63.86
RD-11	4.0"	1.5 LF	IE=64.11
RD-12	4.0"	8 LF	IE=64.31
RD-13	4.0"	1.5 LF	IE=64.29
RD-14	4.0"	1.5 LF	IE=64.75

STORM SEWER SCHEDULE

ITEM	DESCRIPTION	REFERENCE
SDCO-1	CLEANOUT IE=63.88	SEE DTL. 7, SHT C4.00
SDCO-2	CLEANOUT IE=62.41	SEE DTL. 7, SHT C4.00
SDCO-3	CLEANOUT IE=62.07	SEE DTL. 7, SHT C4.00
SDCO-4	CLEANOUT IE=62.47	SEE DTL. 7, SHT C4.00
SDCO-5	CLEANOUT IE=64.51	SEE DTL. 7, SHT C4.00
SDCO-6	CLEANOUT IE=58.75	SEE DTL. 7, SHT C4.00
OF-1	OUTFALL IE=61.60	SEE DTL. 2, SHT C4.01
OF-2	OUTFALL IE=61.60	SEE DTL. 2, SHT C4.01
OF-3	OUTFALL IE=61.60	SEE DTL. 2, SHT C4.01
OF-4	OUTFALL IE=61.60	SEE DTL. 2, SHT C4.01

SHEET LEGEND

SD	STORM	
SS	SANITARY	
W	WATER	
FS	FIRE SERVICE	
SS-FM	SANITARY FORCE MAIN	
■	CATCH BASIN	SEE DTL. 6 SHT C4.00
●	CLEANOUT	SEE DTL. 7 SHT C4.00
▼	FIRE HYDRANT	SEE DTL. 12 SHT C4.00

SHEET NOTES

- PROTECT EXISTING WATER WELL. REMOVE STRUCTURAL OF ABOVE GROUND AND INSTALL WATER WELL HEAD.

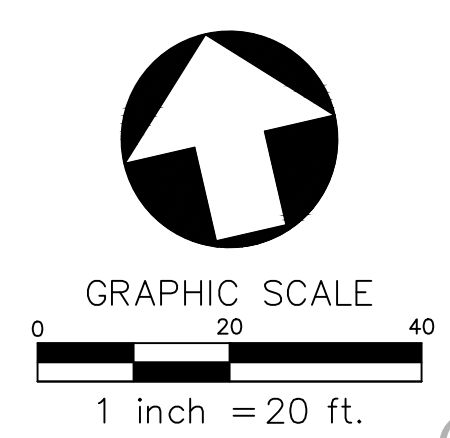
SANITARY SEWER SCHEDULE

ITEM	DESCRIPTION	REFERENCE
SSPS-1	SANITARY SEWER PUMP STATION RIM=64.00 6" IE IN(NE)=59.81 6" IE IN(NW)=59.90 2.5" FM OUT(SW)=62.00 6" IE IN(S)=59.90(FUTURE)	

STORM SEWER SCHEDULE

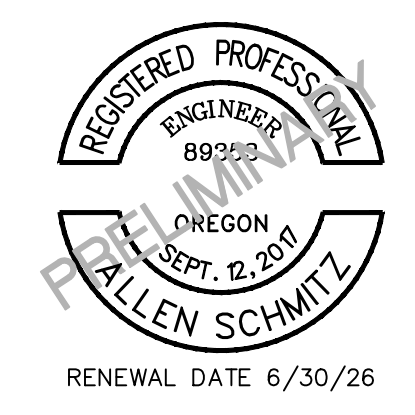
ITEM	DESCRIPTION	REFERENCE
CB-1	CATCH BASIN RIM=64.53 IE=62.04	SEE DTL. 6, SHT C4.00
CB-2	CATCH BASIN RIM=63.08 IE=62.09	SEE DTL. 6, SHT C4.00
SWF-1	STORMWATER FACILITY, PLANTER A=2,135 SF INFILTRATION	SEE DTL. 1, SHT C4.01
SWF-2	STORMWATER FACILITY, SWALE A=792 SF WATER QUALITY ONLY	
SWF-3	STORMWATER FACILITY, SWALE A=367 SF WATER QUALITY ONLY	
SWF-4	STORMWATER FACILITY, SWALE A=272 SF WATER QUALITY ONLY	
SWF-5	STORMWATER FACILITY, SWALE A=184 SF WATER QUALITY ONLY	
OD-1	OVERFLOW DRAIN RIM=62.60 IE=59.46	
OD-2	OVERFLOW DRAIN RIM=64.50 IE=61.75	
OD-3	OVERFLOW DRAIN RIM=64.50 IE=61.75	
OD-4	OVERFLOW DRAIN RIM=61.95 IE=58.95	
OD-5	OVERFLOW DRAIN RIM=64.50 IE=61.75	

UTILITY PLAN
SCALE: 1"=20'



CONSTRUCTION DESIGN
NOT FOR
CONSTRUCTION

AKAAN
architecture + design llc



PROJECT TEAM:
 CIVIL ENGINEER:
 HUMBER DESIGN GROUP, Inc.
 110 SE Main Street, Suite 200,
 Portland, OR 97214
 P: 503.488.5711
 STRUCTURAL ENGINEER:
 PETERSON STRUCTURAL ENGINEERS
 9400 SW Barnes Road, Suite 100
 Portland, OR 97225
 P: 503.292.1635
 MEP ENGINEER:
 NIKE & Associates, Inc.
 6915 SW Macadam Ave, Suite 200
 Portland, OR 97219
 P: 503.892.1188



CCMH - NEW FACILITY
 Columbia Community Mental Health
 58846 McNULLY WAY,
 ST HELENS, OREGON 97051

SCALE:
 DRAWN BY:
 CHECKED BY:
 CAD FILE:
 DATE: 10/21/2024
 REVISIONS
 DATE DESCRIPTION
 CONTENTS:
 UTILITY PLAN
 SHEET NO:

C3.01

**CITY OF ST. HELENS PLANNING DEPARTMENT
STAFF REPORT**

Conditional Use Permit CUP.2.24 & Variances V.8.24 - V.11.24 (four Variances total)

DATE: December 2, 2024
TO: Planning Commission
FROM: Jacob A. Graichen, AICP, City Planner

APPLICANT: Dan Hatfield
OWNER: LAGRAND TOWNHOMES LLC

ZONING: General Residential, R5
LOCATION: 5N1W-34CC-09200; NE corner of the Wyeth and N. 5th Streets intersection
PROPOSAL: Build a triplex on formerly developed lot with multiple Variances

SITE INFORMATION / BACKGROUND

The site was developed with a triplex that utilized on-street parking for decades. Around 2021 there was a fire that destroyed the building, and it was left in a state of disrepair until it was recently demolished; a demo permit to do this was applied for on August 29, 2024 (permit number 749-24-000287-DEMO).

The triplex was an existing non-conforming use as the lot is technically too small to allow three dwelling units and the structure (demolished except for a below grade wall along Wyeth Street) was non-conforming given its close proximity to Wyeth Street, at least. Chapter 17.104 SHMC governs nonconforming structures and when a nonconforming structure is destroyed by more than 60%, **it shall not be reconstructed, except in conformity with the Development Code.**

There is an exception to this per SHMC 17.104.040(1)(b)(ii):

Be occupied by detached single-dwelling unit(s) or a duplex as permitted outright in a residential zoning district, if located in a residential zoning district; the house may be rebuilt on the same size, or smaller, footprint if destroyed by fire or natural disasters over 60 percent of value (any changes to the footprint must meet all setbacks and other regulations of the zone).

However, the applicant does not propose a detached single-family dwelling or duplex. They proposed multi-family development (3 or more dwelling units), in which case this exception does not apply.

This Conditional Use Permit is accompanied by four Variances pertaining to: 1) minimum lot size and the density allowed, 2) reduced exterior side yard (setback) along Wyeth Street, 3) reduction of off-street parking to allow all parking to be street (off site) parking, and 4) relief from the exterior elevation requirements applicable to multifamily development per SHMC17.96.180(3).



Upper left: 2012 Google Earth street view of the subject property viewed from the Wyeth Street / N. 5th Street intersection.
Left: August 2024 photo of the same side of the building. Note the stake in the ground identifying a property corner.
Lower left: November 2024 photo from the same intersection following demolition efforts.





Upper left: June 2022 photo of the subject property from Wyeth Street, showing extent of fire damage.
Left: June 2022 photo of the subject property from N. 5th Street.
Lower left: November 2024 photo of the subject property from N. 5th Street following demolition. Note the remaining retaining wall that was part of the original foundation. This wall parallels and is along Wyeth Street.

PUBLIC HEARING & NOTICE

Public hearing before the Planning Commission: December 10, 2024

Notice of this proposal was sent to surrounding property owners within 300 feet of the subject property(ies) on November 19, 2024 via first class mail. Notice was sent to agencies by mail or e-mail on the same date.

Notice was published on November 29, 2024 in the Columbia County Spotlight newspaper.

APPLICATION COMPLETENESS

This application was originally received on October 31, 2024. The 120-day rule (ORS 227.178) for final action for this land use decision is February 28, 2024.

AGENCY REFERRALS & COMMENTS

CRFR: After reviewing the proposed plans and driving by the site the fire district has the following:

- Due to multiple occupancy – Fire Suppression/Sprinkler system.
- 360 degree fire access to the building. There are several properties that are very close in proximity to the building, for suppression and life safety access the fire district requires the ability to access all sides of the structure.

APPLICABLE CRITERIA, ANALYSIS & FINDINGS

Conditional Use Permit CUP.2.24

Zoning Compliance: The site is zoned R5.

Lot size needs to be 7,500 square feet for triplex. Lot is only 5,800 square feet size. **Need Variance:** see V.8.24.

Min front yard (from N 5th Street) is 20 feet. Site plan shows approximately 30’.

Min rear yard (opposite N 5th Street) is 10 feet. Site plan shows 15 feet.

Min exterior yard (corner lot) along Wyeth is 10 feet. Site plan shows 4.5 feet. **Need Variance:** see V.9.24.

Min side yard (opposite Wyeth) is 10’ for multifamily structures. Site plan shows 12.5 feet.

Maximum building/structure coverage is 50% for multifamily development. ½ of the lot size is 2,900 square feet. The sole building proposed has a footprint of approximately 2,200 square feet.

Minimum landscaping required is 25%. Open space (unpaved/unbuilt area) exceeds 25%, though this will be reduced some, **if the parking variance is not granted.**

Maximum building height is 35 feet. Total height does not exceed 30 feet.

* * *

Addition Yard Setback Reqs & Exceptions: When there is a minimum yard requirement, such as in the case of the subject property, no building, structure or portion thereof, be placed within three feet of a property line.

The significance of this for this case is if the exterior side yard (setback) along Wyeth Street is allowed to be 4.5 feet via the Variance, the building includes a 1' eave, which would still be more than three feet and not conflicting with Chapter 17.64 SHMC.

* * *

Landscaping/buffering/screening: Street trees can be required having more than 100 feet of street frontage. The reduced yard (setback) Variance along Wyeth will impact available space for trees; lack of available space is one of the exemptions for street trees requirements.

Tree spacing will be based on tree size per 17.72.035(2)(a)-(c). Tree location shall also comply with requirements per 17.72.035(2)(d)-(l).

This chapter requires buffering. This applies in this case as follows:

- Site abuts R5 zoned properties occupied by detached single-family dwellings on two sides. The normal requirement is a minimum 10' of buffer plus screening.

The buffering and screening requirements shall be included on revised final plans. Note CRFR comments above; buffer should not conflict with emergency access.

This chapter requires screening (unrelated to buffering above). This applies in this case as follows:

Parking lots greater than three spaces is required to be screened. For screening in this case, the city usually requires landscaping along the perimeter that includes a balance of low lying and vertical shrubbery and trees. **if the Commission grants a Variance that allows no or less off-street parking, this may not apply.**

Service facilities and equipment (e.g., HVAC and other mechanical unit) visible from a public street, customer or residential parking area, any public facility or residential area are required to be screened whether they are ground, wall or roof mounted. In addition, rooftop facilities and equipment are required to be screened from street and adjacent properties.

This is not addressed at this time and will need to be addressed with final plans.

* * *

Visual Clearance: Chapter 17.76 SHMC requires proper sight distances at intersections to reduce traffic hazard potential. The required area to maintain clear vision is greater for arterial streets.

This could potentially affect the final design of the proposal.

* * *

Off-Street Parking/Loading: Off street parking is normally required as new development. However, the applicant is requesting all parking spaces to be on-street. **Need Variance:** see V.10.24.

Normally, 2 spaces per 2-bedroom (or more) dwelling unit requires two spaces each, for a normal minimum requirement of six spaces.

A standard parking space is at least 9' x 18'. If on-street parking is allowed along Wyeth for the parking requirement, the "shoulder" is large enough.

Bicycle parking. Not required for triplexes.

Surface area. If the Commission approved the Variance for on-street parking, it should be paved; this area currently is not.

* * *

Access/egress/circulation:

Public street access. All vehicular access and egress per Chapter 17.84 SHMC is required to directly connect to a public or private street approved by the city for public use. Moreover, vehicular access is required to be within 50' of principle entrances.

The site abuts the following streets:

Street/Road Name	Public or Private	Street Class (TSP)	Jurisdiction	Improved?
Wyeth Street	Public	Collector	City of St. Helens	partial; street paved with old style sidewalk

N. 5 th Street	Public	Local	City of St. Helens	partial; street paved with bike lanes
---------------------------	--------	-------	--------------------	---------------------------------------

The site utilizes these streets for access and the streets are within the statutory distance of the primary entrances.

Pedestrian access (interior walkways). Within all attached housing and multifamily developments, each residential dwelling shall be connected by walkway to the vehicular parking area, and common open space and recreation facilities.

Walkways connecting the front doors facing Wyeth Street to the sidewalk appear to be proposed.

Required walkways shall be paved with hard-surfaced materials such as concrete, asphalt, stone, brick, etc. Walkways shall be required to be lighted and/or signed as needed for safety purposes. Soft-surfaced public use pathways may be provided only if such pathways are provided in addition to required pathways.

Lights will be necessary, associated with each front door.

* * *

Solid Waste/Recyclables: Chapter 17.92 SHMC includes provisions for functional and adequate space for on-site storage and efficient collection of mixed solid waste and recyclables subject to pick up and removal by haulers. However, this does not apply to triplexes.

* * *

Site Development Review: The applicable standards are as follows:

Note that because the subject property is well within a quarter mile via walkways to 6th Street Park, private and shared open space requirements can be exempt.

Per SHMC 17.96.180(3)—Exterior Elevations:

- Along the vertical face of single-dwelling units – attached and multiple-dwelling unit structures, offsets shall occur at a minimum of every 30 feet by providing any two of the following:
 - (a) Recesses (decks, patios, entrances, floor area, etc.) of a minimum depth of eight feet;
 - (b) Extensions (decks, patios, entrances, floor area, etc.) of a minimum depth of eight feet, and maximum length of an overhang shall be 25 feet; and
 - (c) Offsets or breaks in roof elevations of three or more feet in height;

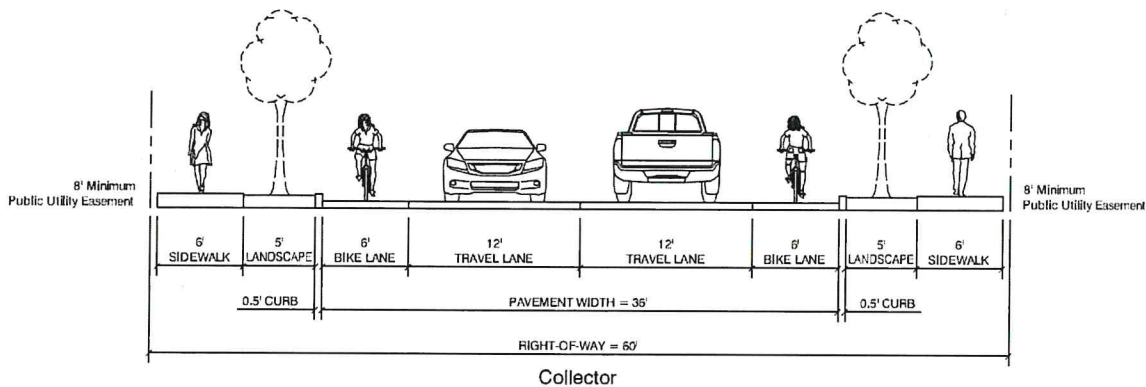
This is not met; **Need Variance:** see V.10.24.

Per SHMC 17.96.180(11)—Distance between Multiple-Family Residential Structure and Other:

- (c) Driveways, parking lots, and common or public walkways shall maintain the following separation for dwelling units within eight feet of the ground level:

- (i) Driveways and parking lots shall be separated from windowed walls by at least eight feet; walkways running parallel to the face of the structures shall be separated by at least five feet; and
- (ii) Driveways and parking lots shall be separated from living room windows by at least 10 feet; walkways running parallel to the face of the structure shall be separated by at least seven feet;

Non-living windowed room areas are along the Wyeth Street side of the building. Provided the existing sidewalk does not move (which is shown incorrectly on the preliminary plans as the sidewalk does not abut the property line), there should be 8' of separation, but maybe not 10 feet. Living room areas are proposed on the opposite side of the building from Wyeth Street.



Because new frontage improvements are possible, that should be examined. The normal cross section shown above has the sidewalk close to the property line. However, it also calls for a 60' wide right-of-way width, and the current width is 80' wide. The improved (asphalt) street is also more-or-less centered in the right-of-way (not skewed to one side). So, it is conceivable the extra right-of-way can accommodate the walkway separation and proposed on-street parking and shall be a condition of approval to ensure design consideration.

* * *

Conditional Use: Pursuant to SHMC 17.100.040:

(1) The planning commission shall approve, approve with conditions, or deny an application for a conditional use or to enlarge or alter a conditional use based on findings of fact with respect to each of the following criteria:

- (a) The site size and dimensions provide adequate area for the needs of the proposed use;
- (b) The characteristics of the site are suitable for the proposed use considering size, shape, location, topography, and natural features;
- (c) All required public facilities have adequate capacity to serve the proposal;
- (d) The applicable requirements of the zoning district are met except as modified by this chapter;
- (e) The supplementary requirements set forth in Chapter 17.88 SHMC, Signs; and Chapter 17.96 SHMC, Site Development Review, if applicable, are met; and
- (f) The use will comply with the applicable policies of the comprehensive plan.

If some of the Variances are denied, this Conditional Use Permit may also warrant denial.

SHMC 17.100.150 has additional requirements for certain conditional use types. The proposal does not include any of these.

SHMC 17.100.040(3) provides “condition of approval guidance” as follows:

(3) The planning commission may impose conditions on its approval of a conditional use, which it finds are necessary to ensure the use is compatible with other use in the vicinity. These conditions may include, but are not limited to, the following:

- (a) Limiting the hours, days, place, and manner of operation;
- (b) Requiring design features which minimize environmental impacts such as noise, vibration, air pollution, glare, odor, and dust;
- (c) Requiring additional setback areas, lot area, or lot depth or width;
- (d) Limiting the building height, size or lot coverage, or location on the site;
- (e) Designating the size, number, location, and design of vehicle access points;
- (f) Requiring street right-of-way to be dedicated and the street to be improved;
- (g) Requiring landscaping, screening, drainage and surfacing of parking and loading areas;
- (h) Limiting the number, size, location, height, and lighting of signs;
- (i) Limiting or setting standards for the location and intensity of outdoor lighting;
- (j) Requiring berming, screening or landscaping and the establishment of standards for their installation and maintenance;
- (k) Requiring and designating the size, height, location, and materials for fences; and
- (l) Requiring the protection and preservation of existing trees, soils, vegetation, watercourses, habitat areas, and drainage areas.

These are for the Commission’s consideration.

* * *

Variance: See Variance section below.

* * *

Tree Removal/Preservation: Chapter 17.132 SHMC addresses the preservation of trees with a diameter at breast height (DBH) >12 inches. Protection is preferred over removal per this Chapter and Site Development Review Chapter 17.96 SHMC.

There is one large tree within the subject property. Staff believes this a walnut tree, which is large for the minimal yard area and building proximity. It could be kept, though if removed, would need to be replaced by two trees.

If proposed to be kept, a protection program by a certified arborist defining the standards and methods that will be used to protect the existing trees to be preserved is required. This shall be on or with the Building Permit plan set to ensure contractors and others follow the tree protection plan during site development.

* * *

Street/Right-of-Way Standards: Both abutting streets are improved with paved travel lanes. Wyeth, a collector street, includes an old style 4’ wide sidewalk on the side of the subject

property and an approximate 18' wide gravel shoulder previously used as on-street parking before the triplex was destroyed. N. 5th Street includes designated bike lanes. Both streets are within 80' wide rights of way.

For frontage improvement considerations, the physical improvements can be required or a minimum 125% fee in lieu per code. Staff feels the physical improvements are warranted given the new construction, especially the Variance request to use of on-street parking.

* * *

Mailboxes: Per SHMC 17.152.030(22) joint mailbox facilities shall be provided in all residential developments. Location is subject to city and postmaster approval. Input from the local postmaster will be required as part of final plans.

* * *

Utility Standards:

Given the site was occupied by a triplex with prior connections to utilities, they are available and this does not pose an increased intensity of use compared to the recent past.

* * * * *

Variances

- Variance V.8.24—dwelling unit/minimum lot size
- Variance V.9.24—reduced yard along the Wyeth Street side of the property
- Variance V.10.24—off-street parking reduction
- Variance V.11.24—noncompliance with Exterior Elevation requirements for multidwelling

CRITERIA:

SHMC 17.108.050 (1) – Criteria for granting a Variance

- (a) The proposed variance will not be significantly detrimental in its consequence to the overall purposes of this code, be in conflict with the applicable policies of the comprehensive plan, to any other applicable policies and standards of this code, and be significantly detrimental in its consequence to other properties in the same zoning district or vicinity;
- (b) There are special circumstances that exist which are peculiar to the lot size or shape, topography or other circumstances over which the applicant has no control, and which are not applicable to other properties in the same zoning district;
- (c) The use proposed will be the same as permitted under this code and city standards will be maintained to the greatest extent that is reasonably possible while permitting some economic use of the land;
- (d) Existing physical and natural systems, such as but not limited to traffic, drainage, dramatic landforms, or parks, will not be adversely affected any more than would occur if the development were located as specified in the code; and

(e) The hardship is not self-imposed and the variance requested is the minimum variance which would alleviate the hardship.

The Commission needs to find all these criteria (a) – (e) are met in order to approve the variance

FINDINGS:

(a) This criterion requires a finding that the variance will not be detrimental.

- Staff comment(s): A triplex with no off-street parking would normally not be allowed today. However, in this case, this occurred at the site for decades until the triplex burned and was left to weather in a state of disrepair for several years. **So does the Commission think this use fits the site now?**

(b) The criterion requires a finding that there are special and unique circumstances.

- See applicant’s narrative. Basis is the historic triplex on the site and topography.

(c) This criterion prohibits a use variance and requires a finding that the applicable standards are maintained to the greatest extent that is reasonably possible.

- See applicant’s narrative. The applicant notes other properties using on-street parking as comparisons.
- Staff comment(s): Regarding density (lot size), there is wider than normal right-of-way to make up the needed lot area. 7,500 square feet for necessary for triplex and the lot is only 5,800 square feet, thus there is a 1,700 square foot deficiency. The extra right-of-way exceeds 1,700 square feet in area.

The remaining foundation wall along Wyeth Street is a reasonable yard reduction consideration.

Regarding parking, if it was a duplex, it could be more reasonable for the requisite amount of off-street parking (2 spaces) to fit via N. 5th access.

Regarding the exterior multidwelling elevation requirements, the building’s length is 55 feet which would require one vertical face offset. In this case, there are more, though the recess and extension are only 4 feet (not the minimum 8 feet) and the break/offset in roof elevation is one foot instead of three feet.

(d) This criterion requires a finding that existing physical and natural systems will not be adversely affected as a result of the requested Variance.

- Staff comment(s): Street frontage improvements will formalize the on-street parking (**if granted**) to include paving and ensure an updated designed sidewalk is the proper distance from windowed walls, and if new, less likely to be replaced and/or relocated for the life of the new triplex.

(e) This criterion requires a finding that the variance issue is not self-imposed and that the variance is the minimum necessary to alleviate the hardship.

- Staff comment(s): The current owner acquired the property after the triplex was built but before the fire that destroyed it.

The Commission needs to find all these criteria (a) – (e) are met in order to approve the variances. If you think one of these is not met, we'll need to address why.

If you approve some of the Variances but not all, please note:

- Variance V.8.24—dwelling unit/minimum lot size. If this is denied, the property can still be developed with single or two dwelling unit development. It is also a question of **does it fit?** That said, this Variance and the off-street parking Variance go hand-in-hand; if one is denied, the other should be denied.
- Variance V.9.24—reduced yard along the Wyeth Street side of the property. This could be approved and still apply to a duplex or detached single-family dwelling and not the triplex as proposed.
- Variance V.10.24—off-street parking reduction. As mentioned, this Variance goes hand-in-hand with the dwelling unit/minimum lot size Variance.
- Variance V.11.24—noncompliance with Exterior Elevation requirements for multidwelling development. If this is denied, multifamily development is still possible, but the proposed building would have to change.

* * * * *

CONCLUSION & RECOMMENDATION

Based on the facts and findings herein, if the Planning Commission approves this Conditional Use Permit and Variances, staff recommends the following conditions:

1. This Conditional Use Permit and Variance approval is valid for a limited time (to establish the use) pursuant to SHMC 17.100.030 and 17.108.040.
2. The following shall be required prior to any development or building permit issuance:
 - a. Final plans as submitted with any development or building permit(s) shall comply with the submitted application materials, with the following additions and/or corrections:
 - A. Buffering and screening along the north and east property lines. Buffering shall not inhibit the fire district’s ability to access all sides of the structure.
 - B. As per condition 4.
 - C. Lighting for/by the doors of each unit along Wyeth Street.
 - D. If the walnut tree (assumed species) is proposed to be kept, a protection program by a certified arborist defining the standards and methods that will be used to protect the existing trees to be preserved is required. This shall be on or with the Building

Permit plan set to ensure contractors and others follow the tree protection plan during site development. If not to be kept, there shall be at least two street trees for replacement purposes.

- E. Joint mailbox facilities shall be provided in all residential developments. Location is subject to city and postmaster approval. Input from the local postmaster will be required as part of final plans.
- F. ...anything needed to cover a variance condition or limitation from the Commission or due to a Variance denial?...

b. Engineering construction plans shall be submitted for review and approval addressing all public improvements including but not limited to:

- A. Street frontage improvements along Wyeth Street and N. 5th Street.
- B. Street trees along N. 15th Street. Note if parking Variance is granted, street trees should be exempted along Wyeth Street and this should be noted in this condition.
- C. On-street parking along Wyeth Street for at least 6 spaces, in improved (paved and marked/striped) form.
- D. Sidewalk based on current standards and placed so it will not be closer than 8' from the windowed walls along Wyeth Street.

3. The following shall be required **prior to** Certificate of Occupancy by the City Building Official / final inspection:

All improvements necessary to address the requirements herein shall be in place.

- 4. Any refuse container or refuse collection area visible from a public street, parking lot, residential or commercial area, or any public facility (e.g., school or park) shall be screened or enclosed from view by a solid wood (or otherwise sight-obscuring) fence, masonry wall or evergreen hedge.
- 5. Owner/applicant and their successors are still responsible to comply with the City Development Code (SHMC Title 17).

Attachment(s): Applicant narrative
Site plan
Building plans

To: City of St. Helens

From: LaGrand Townhomes LLC

Dan Hatfield, Sr.

RE: 470 Wyeth St and N. 5th Street

This is the Responses for land use application Variances to build triplex on the lot address 470 Wyeth Street.

Variance- minimum lot size and density:

The existing structure on the lot was built as triplex and therefore I should be able to build the same footage triplex. I did plan based on the existing foundation. Foundation is 5 feet from the property line. There won't be no porch.

Variance-minimum exterior side yard (along Wyeth)

The existing foundation is only 5 feet from the property line and I am using the existing foundation to build.

Variance – Minimum Side yard (opposite Wyeth)

I want to set 4 feet offset for the center unit. Ane the shed will be removed.

Variance – Off street parking

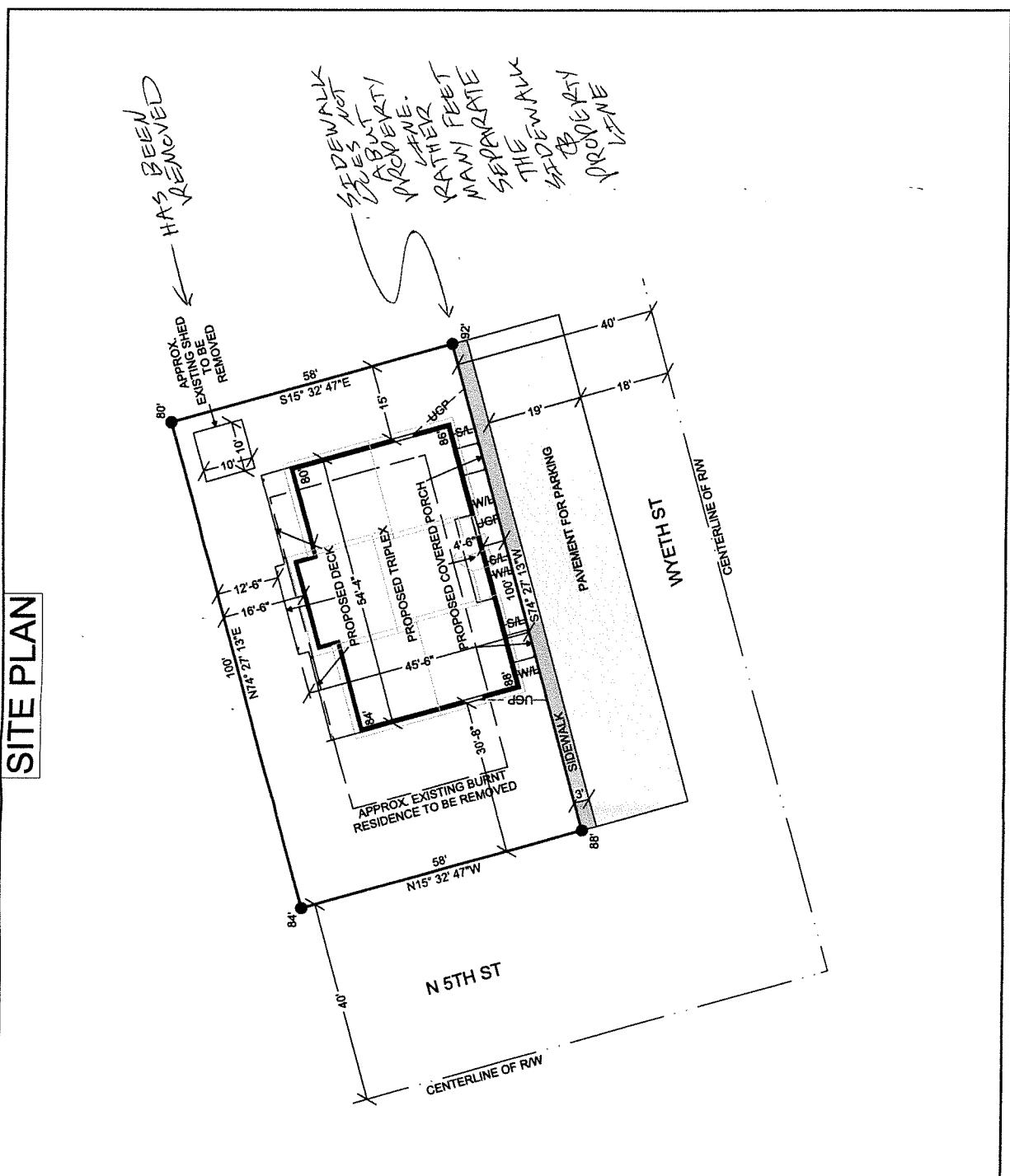
I am not able to put parking on the back or the side of the property. Street is 9 feet higher then the back of the property.

There are many buildings that have parking in the front of the property. For example, 4 plex on 1661 Wyeth street has parking in the front of the building and 4 plex on 1671 Wyeth also has parking in the front of the building. There are parkings in the front of the buildings throughout the Wyeth Street.

SITE PLAN

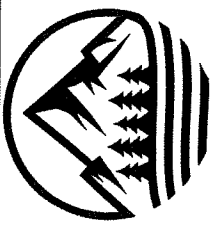
HAS BEEN REMOVED

SIDEWALK DOES NOT GO ABOUT PROPERTY LINE. RATHER MANY FEET SEPARATE THE SIDEWALK FROM PROPERTY LINE



LEGEND

PROPERTY LINE	---
SETBACK LINE	---
EASEMENT	---
CONTOUR LINE	---
SIDEWALK	---
PLANTERS STRIP	---
CENTER LINE OF ROAD	---
ROOF LINE	---
SEWER/SEPTIC LINE (S/L)	---
FENCE	---
WELL	---
WATER LINE (W/L)	---
UG POWER (UGP)	---
OH POWER (OHP)	---
HEAVILY TREED AREA	---
TREE	---
PROPOSED TREE REMOVAL	---
DECIDUOUS TREE	---
DECIDUOUS TREE REMOVAL	---
ELEVATIONS	---



**EVERGREEN
SITE PLANS**
2311 SE 192ND AVE UNIT 103
CANAS, WA 98607
360-521-6272

OWNER:	HATFIELD
CONTRACTOR:	
ADDRESS:	470 WYETH ST
CITY, STATE:	ST HELENS, OR
MAP & TAX LOT:	5134-CC-09200
ACCOUNT NUMBER:	13940
SCALE:	1"=20'
DATE:	8/29/2024



THIS SET OF PLANS IS TO BE USED FOR THE PROJECT AND SITE SPECIFICALLY IDENTIFIED. IT IS NOT TO BE REPRODUCED OR COPIED IN ANY MANNER WITHOUT THE WRITTEN PERMISSION OF DESIGN NORTHWEST ARCHITECTURAL MANAGEMENT. ANY CHANGES TO THESE PLANS MUST BE APPROVED BY DESIGN NORTHWEST ARCHITECTURAL MANAGEMENT. THE USER OF THESE PLANS AGREES TO HOLD DESIGN NORTHWEST ARCHITECTURAL MANAGEMENT HARMLESS FROM AND AGAINST ALL LIABILITY, INCLUDING REASONABLE ATTORNEY'S FEES, ARISING OUT OF OR RESULTING FROM THE USE OF THESE PLANS. THE USER OF THESE PLANS AGREES TO HOLD DESIGN NORTHWEST ARCHITECTURAL MANAGEMENT HARMLESS FROM AND AGAINST ALL LIABILITY, INCLUDING REASONABLE ATTORNEY'S FEES, ARISING OUT OF OR RESULTING FROM THE USE OF THESE PLANS.

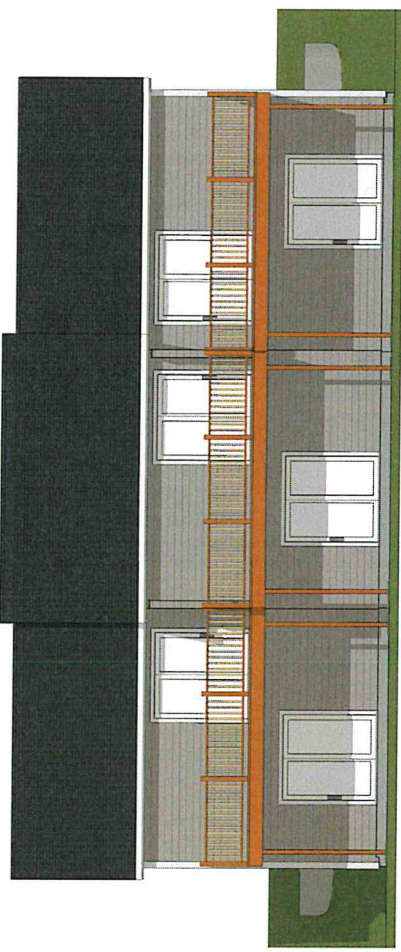
DATE: 02/20/2024
JOB: 21-937
TRIPLEX

1

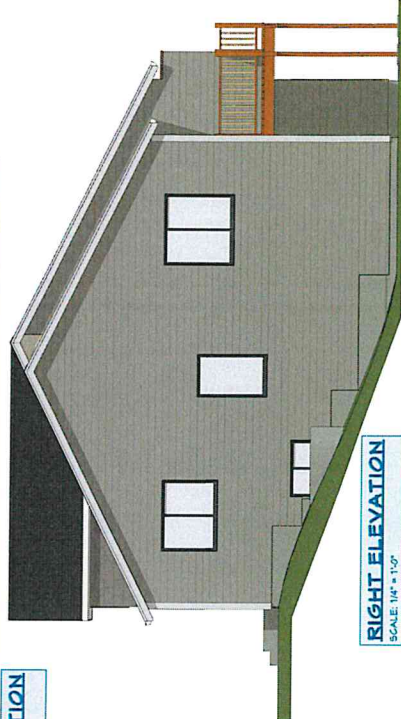
Item C.



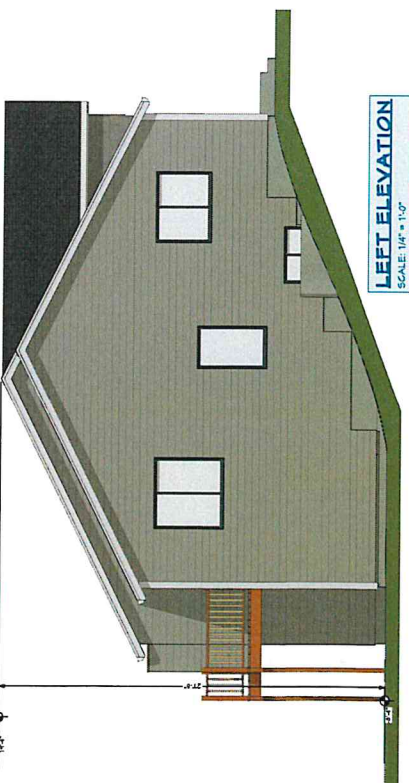
FRONT ELEVATION
SCALE: 1/4" = 1'-0"



REAR ELEVATION
SCALE: 1/4" = 1'-0"



RIGHT ELEVATION
SCALE: 1/4" = 1'-0"



LEFT ELEVATION
SCALE: 1/4" = 1'-0"



FRONT CAMERA VIEW
NTS

COPYRIGHT © DESIGN NW



THIS SET OF PLANS IS THE PROPERTY OF DESIGN NORTHWEST. IT IS TO BE USED ONLY FOR THE PROJECT AND SITE SPECIFICALLY IDENTIFIED HEREON. NO PART OF THIS SET OF PLANS IS TO BE REPRODUCED, COPIED, EITHER WHOLLY OR IN PART, OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, INCLUDING PHOTOCOPYING, RECORDING, OR BY ANY INFORMATION STORAGE AND RETRIEVAL SYSTEM, WITHOUT THE WRITTEN PERMISSION OF DESIGN NORTHWEST. ANY UNAUTHORIZED REPRODUCTION OR TRANSMISSION IS STRICTLY PROHIBITED. DESIGN NORTHWEST SHALL NOT BE RESPONSIBLE FOR ANY DAMAGE TO PERSONS OR PROPERTY ARISING FROM THE USE OF THESE PLANS.

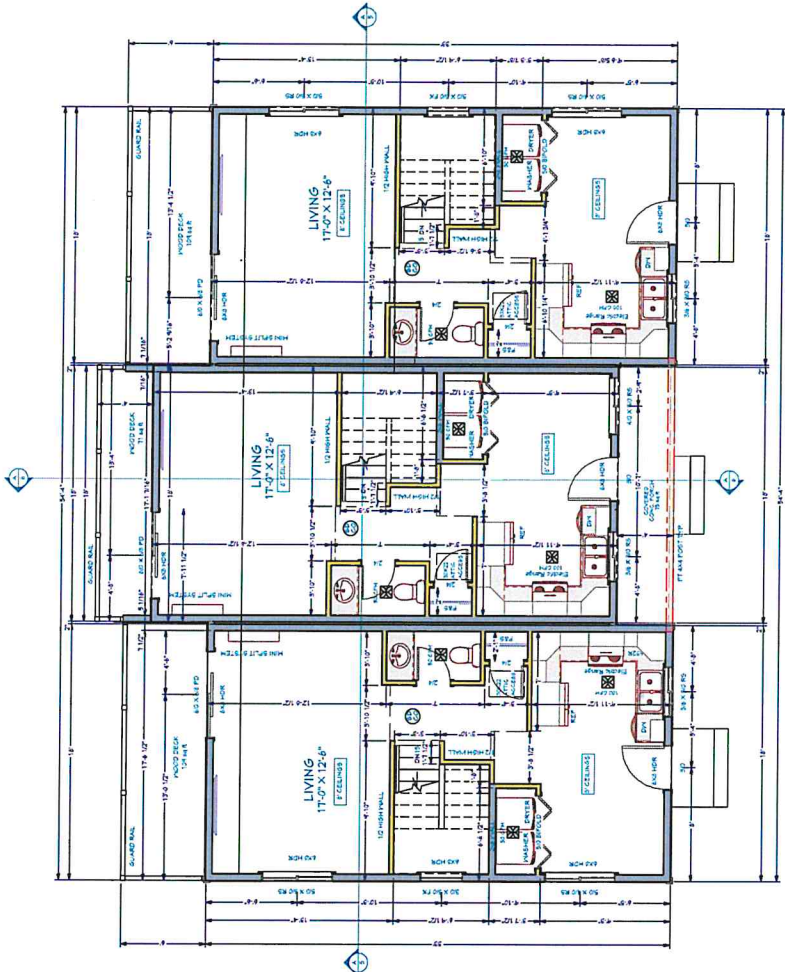
DATE: 01/21/2011
JOB NO: 21-397
SHEET NO: 1 OF 1

2

Item C.

MAIN FLOOR LIVING AREA	846 SF
DAYLIGHT BARBER	854 SF
TOTAL FLOOR AREA PER UNIT	1100 SF
TOTAL FLOOR AREA	3304 SF

1ST FLOOR PLAN
SCALE: 1/4" = 1'-0"



SEEKING TO ENGINEERS FOR REVIEW OF LOCAL ENGINEERING IS REQUIRED ACCORDING TO LOCAL BUILDING CODES AND/OR ZONING REGULATIONS.

NOTES:
1. ALL DIMENSIONS ARE IN FEET AND INCHES.
2. FINISHES ARE TO BE DETERMINED BY THE ARCHITECT.
3. SEE SPECIFICATIONS FOR MATERIALS AND METHODS OF CONSTRUCTION.
4. ALL WORK IS TO BE ACCORDING TO THE LATEST EDITIONS OF THE INTERNATIONAL RESIDENTIAL CODE BOOK (IRC) AND THE INTERNATIONAL MECHANICAL AND PLUMBING CODE (IMC).
5. THE OWNER SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE LOCAL BUILDING DEPARTMENT.
6. THE ARCHITECT SHALL NOT BE RESPONSIBLE FOR ANY DAMAGE TO PERSONS OR PROPERTY ARISING FROM THE USE OF THESE PLANS.

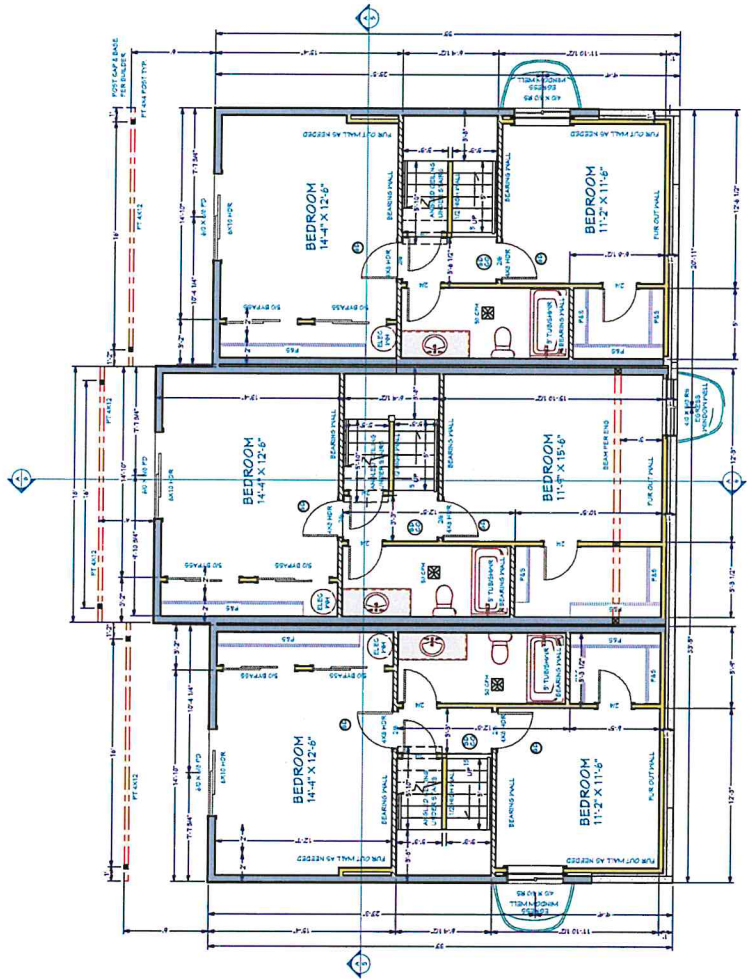


These plans are prepared for the use of the client and are not to be used for any other project without the written consent of the architect. The architect is not responsible for any errors or omissions in these plans. The client is responsible for obtaining all necessary permits and for complying with all applicable building codes and regulations. The architect is not responsible for any construction or material costs. The architect is not responsible for any delays or interruptions in the construction process. The architect is not responsible for any damage to the property or any other loss. The architect is not responsible for any other matters not specifically mentioned in these terms and conditions.

DATE	12/20/2024
JOB NO.	21-397
PROJECT	TRIPLEX

LOWER FLOOR PLAN
SCALE: 1/8" = 1'-0"

COMPILED BY DESIGN NW



REFER TO ENGINEERING FOR SEALS. ALL UTILITY ENGINEERING IS REQUIRED ACCORDING TO LOCAL BUILDING CODES AND/OR ZONING REGULATIONS

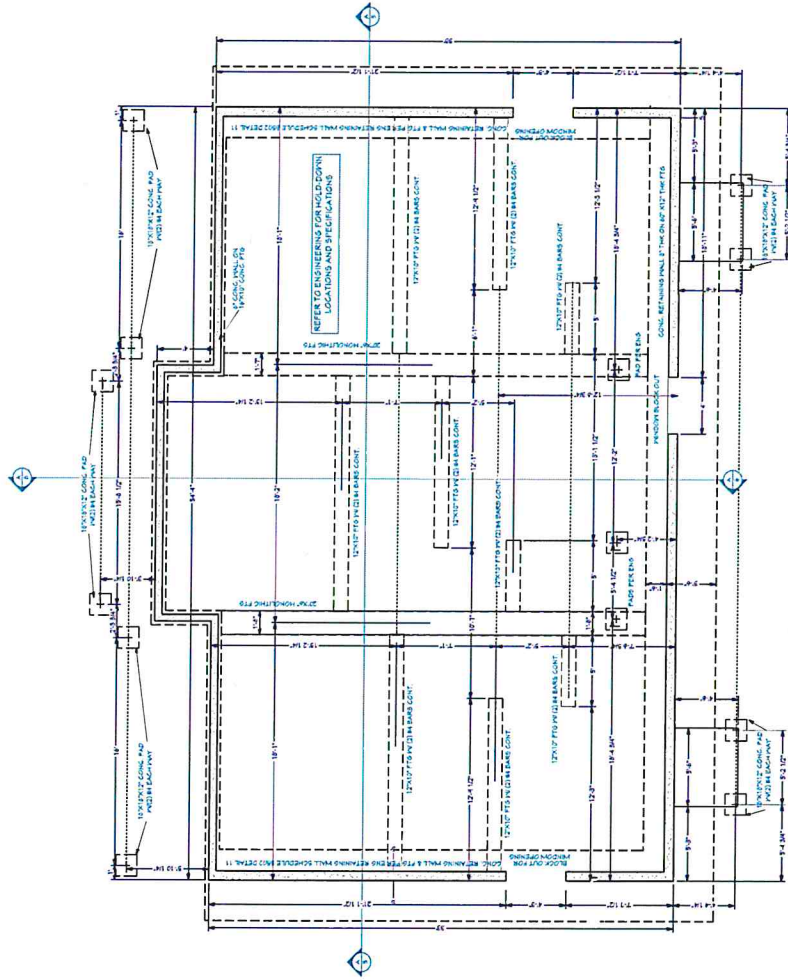


THIS PLAN IS THE PROPERTY OF DESIGN NORTHWEST. IT IS TO BE USED ONLY FOR THE PROJECT AND SITE SPECIFICALLY IDENTIFIED HEREON. ANY REUSE OR MODIFICATION OF THIS PLAN WITHOUT THE WRITTEN CONSENT OF DESIGN NORTHWEST IS STRICTLY PROHIBITED. THE USER ASSUMES ALL LIABILITY FOR THE ACCURACY AND COMPLETENESS OF THIS PLAN. DESIGN NORTHWEST MAKES NO WARRANTY, EXPRESS OR IMPLIED, FOR THE ACCURACY OR COMPLETENESS OF THIS PLAN. THE USER SHALL INDEMNIFY AND HOLD DESIGN NORTHWEST HARMLESS FROM AND AGAINST ALL CLAIMS, DAMAGES, LOSSES AND EXPENSES, INCLUDING REASONABLE ATTORNEY'S FEES, ARISING OUT OF OR FROM THE USE OF THIS PLAN.

DATE: 01-23-2024
JOB: 21-337
PROJECT: TRIPLEX

FOUNDATION PLAN
SCALE: 1/4" = 1'-0"

COMMON DESIGN, INC.



REFER TO ENGINEERING FOR SPECS IF LATERAL LOADS ARE APPLIED TO LOCAL BUILDING CODES AND/OR REGULATIONS.

FOUNDATION PLAN
 THIS PLAN IS THE PROPERTY OF COMMON DESIGN, INC. IT IS TO BE USED ONLY FOR THE PROJECT AND SITE SPECIFICALLY IDENTIFIED HEREON. ANY REUSE OR MODIFICATION OF THIS PLAN WITHOUT THE WRITTEN CONSENT OF COMMON DESIGN, INC. IS STRICTLY PROHIBITED. THE USER ASSUMES ALL LIABILITY FOR THE ACCURACY AND COMPLETENESS OF THIS PLAN. COMMON DESIGN, INC. MAKES NO WARRANTY, EXPRESS OR IMPLIED, FOR THE ACCURACY OR COMPLETENESS OF THIS PLAN. THE USER SHALL INDEMNIFY AND HOLD COMMON DESIGN, INC. HARMLESS FROM AND AGAINST ALL CLAIMS, DAMAGES, LOSSES AND EXPENSES, INCLUDING REASONABLE ATTORNEY'S FEES, ARISING OUT OF OR FROM THE USE OF THIS PLAN.

FOUNDATION PLAN
 THIS PLAN IS THE PROPERTY OF COMMON DESIGN, INC. IT IS TO BE USED ONLY FOR THE PROJECT AND SITE SPECIFICALLY IDENTIFIED HEREON. ANY REUSE OR MODIFICATION OF THIS PLAN WITHOUT THE WRITTEN CONSENT OF COMMON DESIGN, INC. IS STRICTLY PROHIBITED. THE USER ASSUMES ALL LIABILITY FOR THE ACCURACY AND COMPLETENESS OF THIS PLAN. COMMON DESIGN, INC. MAKES NO WARRANTY, EXPRESS OR IMPLIED, FOR THE ACCURACY OR COMPLETENESS OF THIS PLAN. THE USER SHALL INDEMNIFY AND HOLD COMMON DESIGN, INC. HARMLESS FROM AND AGAINST ALL CLAIMS, DAMAGES, LOSSES AND EXPENSES, INCLUDING REASONABLE ATTORNEY'S FEES, ARISING OUT OF OR FROM THE USE OF THIS PLAN.

FOUNDATION PLAN
 THIS PLAN IS THE PROPERTY OF COMMON DESIGN, INC. IT IS TO BE USED ONLY FOR THE PROJECT AND SITE SPECIFICALLY IDENTIFIED HEREON. ANY REUSE OR MODIFICATION OF THIS PLAN WITHOUT THE WRITTEN CONSENT OF COMMON DESIGN, INC. IS STRICTLY PROHIBITED. THE USER ASSUMES ALL LIABILITY FOR THE ACCURACY AND COMPLETENESS OF THIS PLAN. COMMON DESIGN, INC. MAKES NO WARRANTY, EXPRESS OR IMPLIED, FOR THE ACCURACY OR COMPLETENESS OF THIS PLAN. THE USER SHALL INDEMNIFY AND HOLD COMMON DESIGN, INC. HARMLESS FROM AND AGAINST ALL CLAIMS, DAMAGES, LOSSES AND EXPENSES, INCLUDING REASONABLE ATTORNEY'S FEES, ARISING OUT OF OR FROM THE USE OF THIS PLAN.

**CITY OF ST. HELENS PLANNING DEPARTMENT
STAFF REPORT
Variance V.12.24 & V.13.24**

DATE: December 3, 2024
TO: Planning Commission
FROM: Jennifer Dimsho, AICP, Associate Planner

APPLICANT: Rick Scholl
OWNER: Same as applicant

ZONING: General Residential, R5
LOCATION: Vacant lots across from 135 S. 6th Street; 4N1W-3BB-9500
PROPOSAL: Variances (2) for reduced side yard (setback) and reduced front yard (setback)

SITE INFORMATION / BACKGROUND

The subject property is undeveloped and abuts both S. 6th Street and S. 5th Street rights-of-way. S. 6th Street is developed as a street but lacks sidewalks and curb. S. 5th Street is undeveloped. The subject property includes all of Lot 8 and Lot 15 and the northern 29’ of Lot 7, Block 48 of the St. Helens Subdivision. In 2024, a lot line adjustment (LLA.2.24) was approved to adjust the north/south property line to create two relatively flat building envelopes for each of the two buildable lots. The LLA also resulted in a flag lot for one of the lots.

The subject property is incredibly steep, with slopes exceeding 25 percent along the south side. Photos from the applicant are included as an attachment.

PUBLIC HEARING & NOTICE

Public hearing before the Planning Commission: **December 10, 2024**

Notice of this proposal was sent to surrounding property owners within 100’ feet of the subject property(ies) on November 20, 2024, via first class mail. Notice was sent to agencies by mail or e-mail on the same date.

Notice was published on November 28, 2024, in The Spotlight newspaper.

APPLICATION COMPLETENESS

The 120-day rule (ORS 227.178) for final action for this land use decision is **March 19, 2025**.

AGENCY REFERRALS & COMMENTS

As of the date of this staff report, no agency referral comments have been received.

APPLICABLE CRITERIA, ANALYSIS & FINDINGS

DISCUSSION:

Per SHMC 17.140.055 (2) (d), for flag lots, the principal dwelling must have a minimum setback of 10' on all sides. The applicant is requesting a 5' side setback for the northern flag lot for the development of a detached single-family dwelling.

In the R5 zone, the front setback is 20'. The applicant is requesting a 18' front setback for the development of a detached single-family dwelling.

CRITERIA:

SHMC 17.108.050 (1) – Criteria for granting a Variance

- (a) The proposed variance will not be significantly detrimental in its consequence to the overall purposes of this code, be in conflict with the applicable policies of the comprehensive plan, to any other applicable policies and standards of this code, and be significantly detrimental in its consequence to other properties in the same zoning district or vicinity;
- (b) There are special circumstances that exist which are peculiar to the lot size or shape, topography or other circumstances over which the applicant has no control, and which are not applicable to other properties in the same zoning district;
- (c) The use proposed will be the same as permitted under this code and city standards will be maintained to the greatest extent that is reasonably possible while permitting some economic use of the land;
- (d) Existing physical and natural systems, such as but not limited to traffic, drainage, dramatic landforms, or parks, will not be adversely affected any more than would occur if the development were located as specified in the code; and
- (e) The hardship is not self-imposed and the variance requested is the minimum variance which would alleviate the hardship.

The Commission needs to find all these criteria **(a)** – **(e)** are met in order to approve the variances.

FINDINGS:

(a) This criterion requires a finding that the variance will not be detrimental.

- Staff comments: The Commission can note there are no other flag lots in the vicinity of this property which have side yards of 10'. Most of the surrounding properties are standard lots with the more common 5' side setbacks. Therefore, the visual impact would be minimal to the properties within the vicinity.
- There is an extra wide 80' right-of-way along S. 6th Street where both lots access the roadway. Visually, the 2' reduction along the front property line will not be noticeable.

(b) The criterion requires a finding that there are special and unique circumstances.

- Staff comment: The topography of the lot shows an incredibly steep slope (>25%) along the extent of the southern property line according to the attached topography survey. The Commission could find that this is a special circumstance unique to this subject property.

(c) This criterion prohibits a use variance and requires a finding that the applicable standards are maintained to the greatest extent that is reasonably possible.

- Staff comments: The Commission can find this is not a use Variance.
- The Commission can consider that the setback reduction for the front setback is only for only 20% (20' to 18'), which is a relatively small reduction.
- The Commission could consider that the typical side setback for a detached single-family dwelling is 5' in the R5 zoning district. All other lots in the vicinity are regular lots with 5' side setback requirements, not flag lots with 10' side setback requirements.

(d) This criterion requires a finding that existing physical and natural systems will not be adversely affected as a result of the requested Variance.

- Staff comments: It is common for a portion of the 20' front setback to be used for a residential lot's off-street parking requirements. This Variance would not exempt that requirement. The applicant has shown how they would still meet this requirement. This is also a recommended condition of approval.
- The Commission can note there are some trees along the southern steep slope which will be less impacted if the Commission grants a reduced side yard along the north property line.

(e) This criterion requires a finding that the variance issue is not self-imposed and that the variance is the minimum necessary to alleviate the hardship.

- Staff comment: The Commission must find that this variance is not self-imposed and that the variance is the minimum necessary to alleviate the hardship.

The Commission needs to find all these criteria **(a)** – **(e)** are met in order to approve the variances. If you think one of these is not met, we need to address why.

The Commission can find all criteria are met based on the above and/or any other findings, or specify which criteria are not met and why as a basis for Variance denial.

CONCLUSION & RECOMMENDATION

Based upon the facts and findings herein, if the Commission wants to approve the Variances, staff recommends the following conditions:

1. These Variances approvals are valid for a limited time pursuant to SHMC 17.108.040.
2. The off-street parking requirements of the Development Code still apply.

3. Owner/applicant and their successors are still responsible to comply with the City Development Code (SHMC Title 17), except for the Variances granted herein.

Attachments:

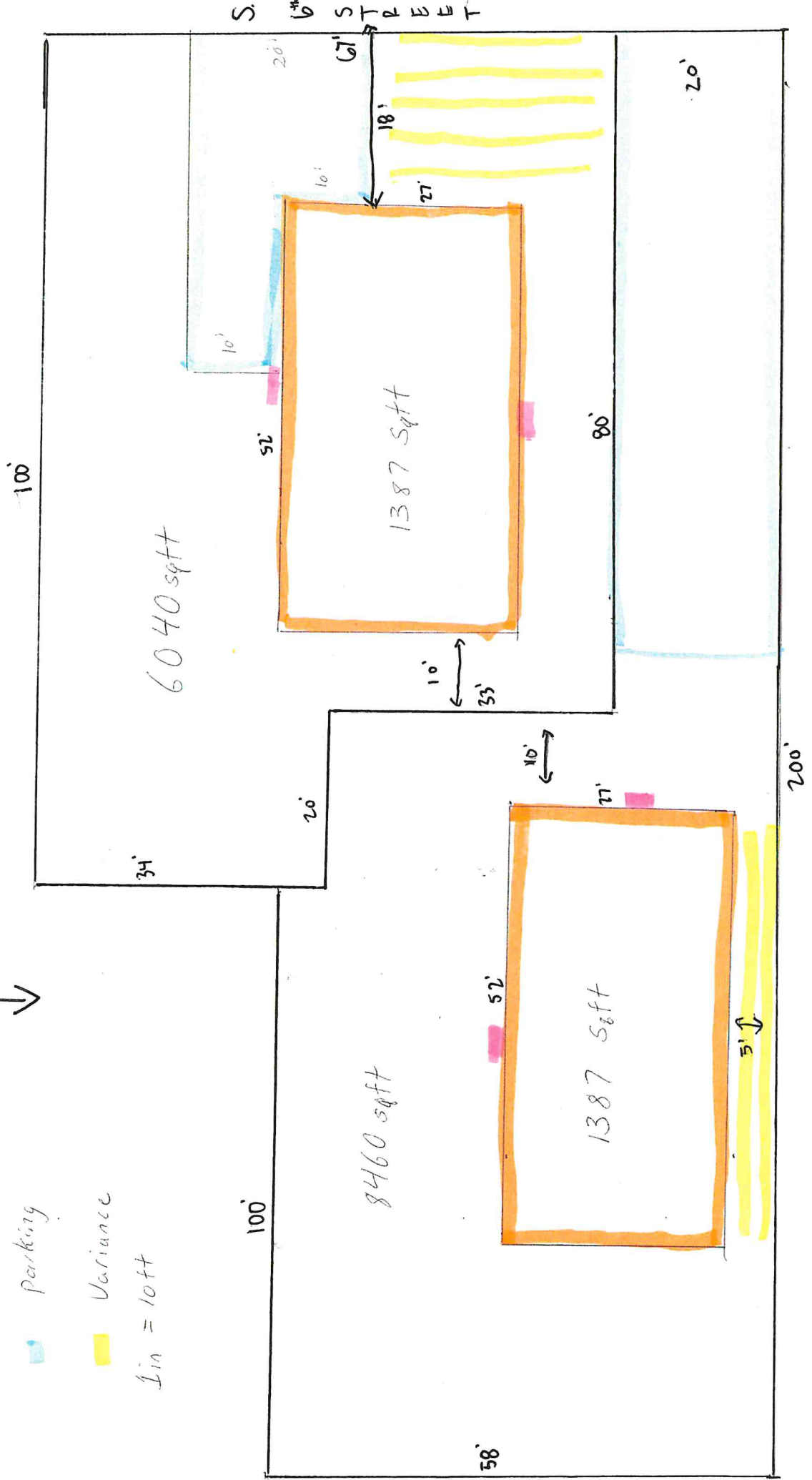
- *Site Plan*
- *LLA*
- *Topography Survey*
- *Applicant's Photos*
- *Proposed Dwelling, Floor Plan*

V.12.24 + V.13.24
Site Plan

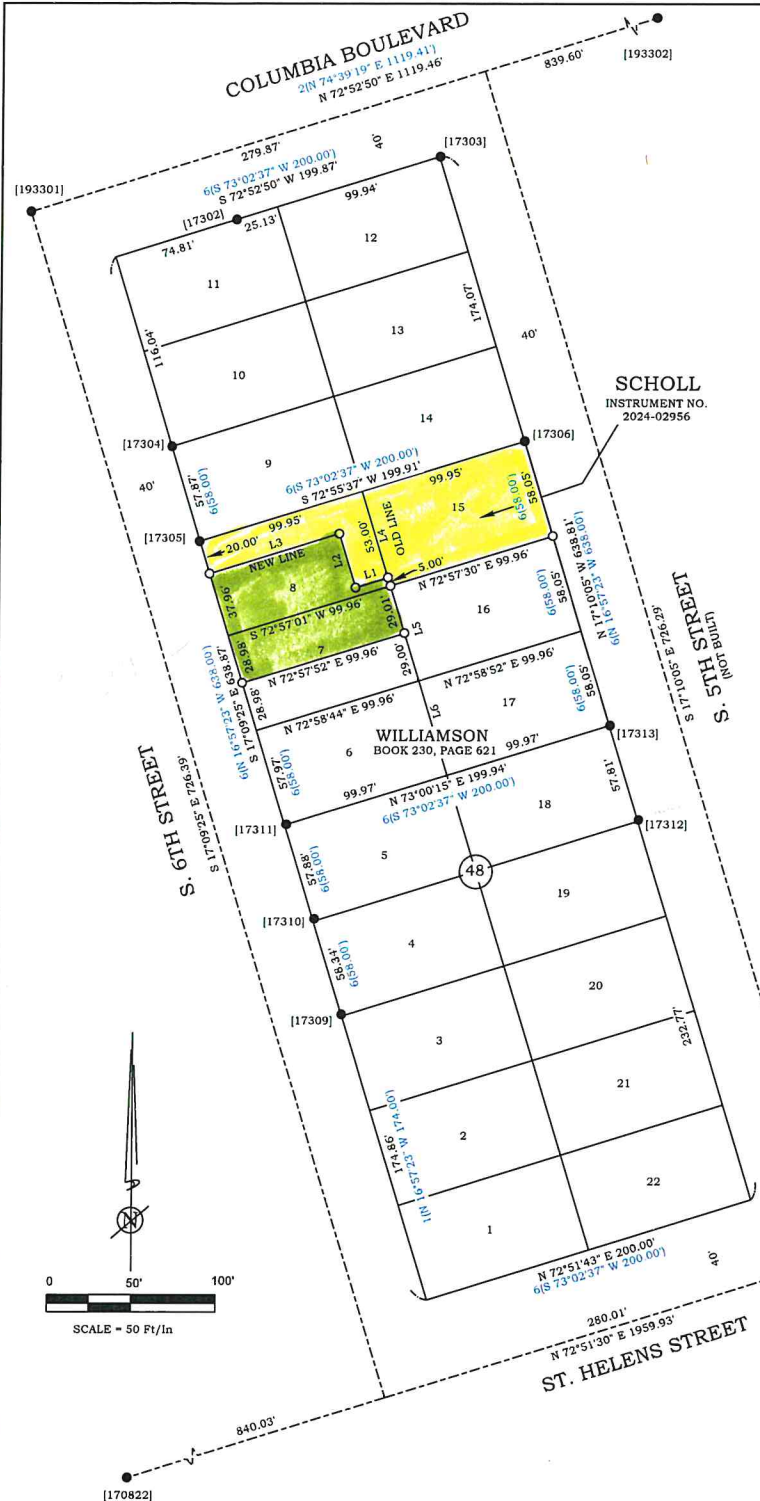
- House
- Entries
- Parking
- Variance

1 in = 10 ft

NORTH
↓



Item D.



MONUMENT NOTES:

- [17302] FOUND A 5/8" IRON ROD PER CS NO. L-1880, MONUMENT BEARS SOUTH 17°07'10" EAST 0.15' FROM NORTH LINE OF BLOCK 48
- [17303] FOUND A 5/8" IRON ROD PER CS NO. L-1880, MONUMENT BEARS NORTH 72°55'38" EAST 0.32' FROM THE NE CORNER OF BLOCK 48
- [17304] FOUND A 5/8" IRON ROD PER CS NO. L-1880, MONUMENT BEARS NORTH 72°50'36" EAST 0.11' FROM EASTERLY RIGHT OF WAY LINE OF S 6TH STREET
- [17305] FOUND A 5/8" IRON ROD WITH A Y.P.C. MARKED "PHIL DEWEY LS 847" PER CS NO. L-1880, MONUMENT BEARS SOUTH 72°55'37" WEST 0.22' FROM EASTERLY RIGHT OF WAY LINE OF S 6TH STREET
- [17306] FOUND A 5/8" IRON ROD WITH A Y.P.C. MARKED "PHIL DEWEY LS 847" PER CS NO. L-1880, (HELD)
- [17309] FOUND A 5/8" IRON ROD WITH A Y.P.C. MARKED "LS 847" PER CS NO. 5072, MONUMENT BEARS NORTH 72°50'35" EAST 0.26' FROM EASTERLY RIGHT OF WAY LINE OF S 6TH STREET
- [17310] FOUND A 5/8" IRON ROD ORIGIN UNKNOWN, MONUMENT BEARS NORTH 72°50'35" EAST 1.16' FROM EASTERLY RIGHT OF WAY LINE OF S 6TH STREET
- [17311] FOUND A PK NAIL WITH A WASHER MARKED "LS 1860" PER CS NO. L-1680, MONUMENT BEARS NORTH 73°00'15" EAST 1.15' FROM EASTERLY RIGHT OF WAY LINE OF S 6TH STREET
- [17312] FOUND A 3/4" IRON PIPE WITH A Y.P.C. MARKED "BRADY LS 1860" PER CS NO. L-1680, MONUMENT BEARS NORTH 72°49'55" EAST 0.63' FROM WESTERLY RIGHT OF WAY LINE OF S 5TH STREET
- [17313] FOUND A 3/4" IRON PIPE WITH A Y.P.C. MARKED "BRADY LS 1860" PER CS NO. L-1680, MONUMENT BEARS NORTH 73°00'15" EAST 0.51' FROM THE WESTERLY RIGHT OF WAY LINE OF S 5TH STREET
- [170822] CENTERLINE OF 9TH STREET & ST. HELENS STREET FOUND 2 1/2" BRASS DISK IN MONUMENT BOX, ORIGIN UNKNOWN, (HELD)
- [193301] CENTERLINE OF COLUMBIA BOULEVARD & 6TH STREET FOUND A 5/8" IRON ROD WITH A 2" ALUMINUM CAP MARKED "AKS ENGR" PER CS NO. 5892, (HELD)
- [193302] FOUND A 5/8" IRON ROD WITH A 2" ALUMINUM CAP MARKED "AKS ENGR" PER CS NO. 5892, (HELD)
- [214363] CENTERLINE OF 2ND STREET & ST HELENS STREET FOUND A 5/8" IRON ROD WITH A Y.P.C. MARKED "MARTINEZ LS 2536" IN A MONUMENT BOX PER CS NO. 4576, (HELD)

NARRATIVE:

--THIS SURVEY WAS COMPLETED AT THE REQUEST OF RICK SCHOLL TO MONUMENT A PROPERTY LINE ADJUSTMENT BETWEEN LOTS 8 & 15, BLOCK 48, ST HELENS, PER CITY OF ST. HELENS PLANNING DEPARTMENT FILE NO. LLA.2.24. THE NEW LINE WAS ESTABLISHED AT THE LOCATION SHOWN PER THE REQUEST OF OUR CLIENT.

--THE BASIS OF BEARINGS IS OREGON COORDINATE REFERENCE SYSTEM COLUMBIA RIVER WEST ZONE, NAD83 (2011) EPOCH 2010.00. DISTANCES SHOWN ARE GROUND DISTANCES.

--FOR CONTROL I HELD THE MONUMENTS AS NOTED IN THE MONUMENT NOTES.

ST. HELENS STREET:
 --I HELD MONUMENT 170822 FOR THE CENTERLINE INTERSECTION OF ST. HELENS STREET AND 9TH STREET. I HELD MONUMENT NO. 214363 FOR THE CENTERLINE INTERSECTION OF ST. HELENS STREET AND 3RD STREET. I COMPUTED THE INTERSECTIONS WITH S 5TH STREET AND S 6TH STREET PROPORTIONALLY BETWEEN MONUMENTS 170822 AND 214363.

COLUMBIA BOULEVARD:
 --I HELD MONUMENT NO. 193301 FOR THE INTERSECTION OF COLUMBIA BOULEVARD AND S. 6TH STREET AND MONUMENT NO. 193302 FOR THE CENTERLINE INTERSECTION OF ST. HELENS STREET AND 3RD STREET. I COMPUTED THE INTERSECTIONS WITH S 5TH STREET AND S 6TH STREET PROPORTIONALLY BETWEEN MONUMENTS 193301 AND 193302.

S. 6TH STREET:
 --I HELD MONUMENT NO. 193301 FOR THE INTERSECTION OF COLUMBIA BOULEVARD AND S. 6TH STREET AND THE COMPUTED CENTERLINE INTERSECTION OF ST. HELENS STREET AND 6TH STREET FOR THE CENTERLINE WITH AN 80' WIDE RIGHT OF WAY.

S. 5TH STREET:
 --I HELD MONUMENT NO. 193301 FOR THE INTERSECTION OF COLUMBIA BOULEVARD AND S. 6TH STREET AND THE COMPUTED CENTERLINE INTERSECTION OF ST. HELENS STREET AND 5TH STREET FOR THE CENTERLINE WITH AN 80' WIDE RIGHT OF WAY.

BLOCK 48 RESOLUTION:
 --I HELD MONUMENTS 17305 & 17306 FOR THE NORTH LINE OF LOTS 8 AND 15. I HELD MONUMENTS 17311, & 17313 FOR THE SOUTH LINE OF LOTS 6 AND 17 AND COMPUTED THE LOT CORNERS AT THE INTERSECTIONS WITH THE WEST RIGHT OF WAY LINE OF 5TH STREET AND THE EAST RIGHT OF WAY LINE OF 6TH STREET. I COMPUTED THE LOT CORNERS OF LOTS 7 & 16 PROPORTIONALLY.

REFERENCES:

- SURVEYS**
 1 = CS NO. 5072
 2 = CS NO. 5892
 3 = CS NO. 7082
 3 = CS NO. L-1680
 5 = CS NO. L-1880
 6 = CS NO. M-0052
- DEEDS**
 1 = INSTRUMENT NO. 2024-02956

Line Table

LINE	BEARING	DISTANCE
L1	S72°55'37"W	20.00'
L2	N17°09'45"W	33.00'
L3	S72°55'37"W	79.96'
L4	N17°09'45"W	58.00'
L5	S17°09'45"E	58.01'
L6	S17°09'45"E	58.01'

LEGEND

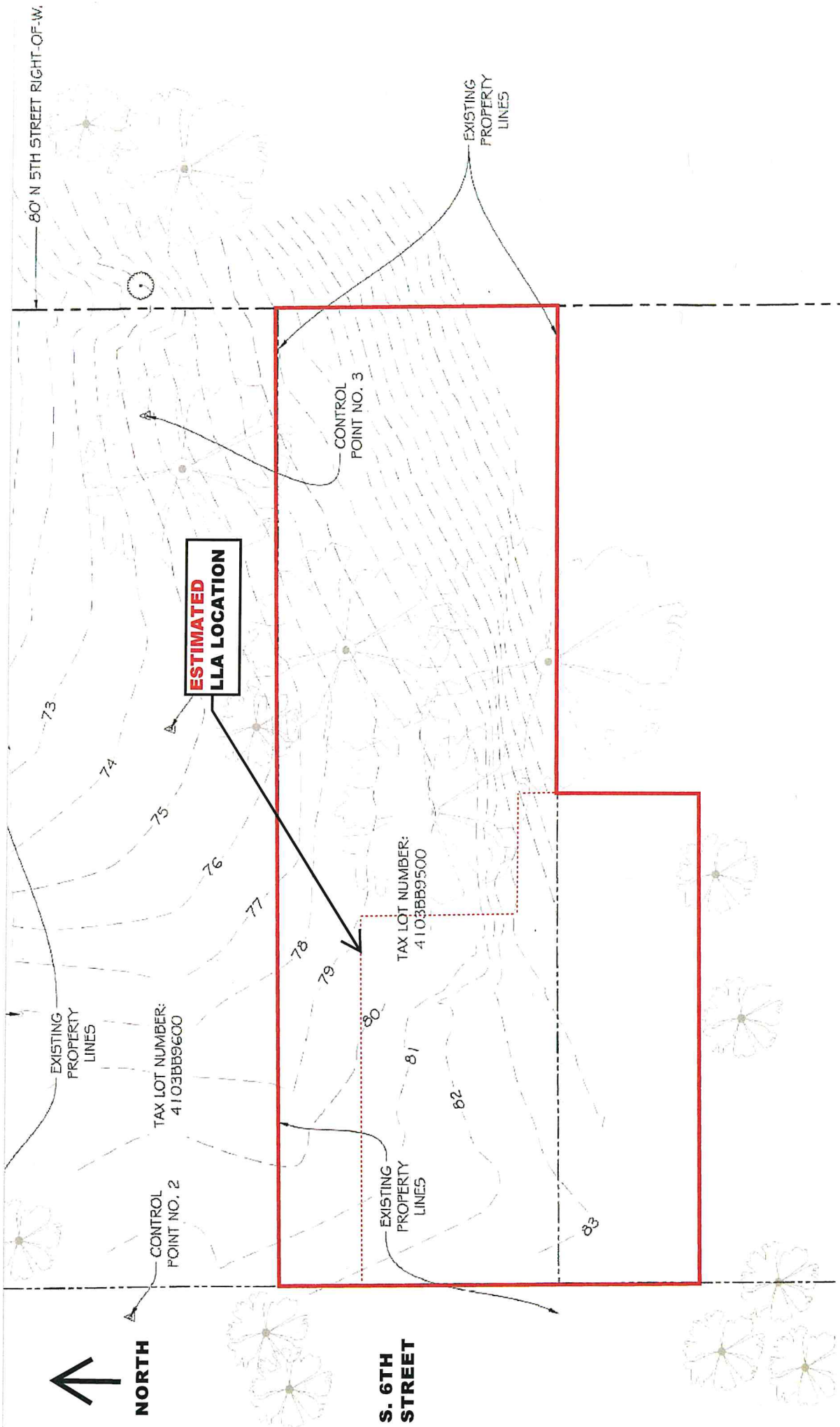
- FOUND MONUMENT AS NOTED
- SET A 5/8" X 30" IRON ROD WITH A Y.P.C. MARKED "KLS SURVEYING INC."
- [X] MONUMENT POINT NO
- () RECORD SURVEY DATA
- [] RECORD DEED DATA
- CS NO. SURVEY NUMBER, COLUMBIA COUNTY SURVEY RECORDS
- Y.P.C. YELLOW PLASTIC CAP

REGISTERED PROFESSIONAL LAND SURVEYOR
 Donald D Wallace
 OREGON
 JANUARY 19, 1993
 DONALD D WALLACE, JR
 2601
 RENEWS 6/30/26

K L S SURVEYING INC.
 1224 ALDER STREET
 VERNOVA, OREGON 97064
 (503) 429-6115

PROPOSED PROPERTY LINE ADJUSTMENT SURVEY FOR
RICK SCHOLL
 IN THE SW 1/4 OF SECTION 4,
 T4N, R1W, W.M.,
 CITY OF ST. HELENS,
 COLUMBIA COUNTY, OREGON
 SEPTEMBER 13, 2024

DRAWN BY: DDW
FIELD: SWM/JR/CAM
EQUIPMENT: NOMAD/NIKON
JOB No. CRW-ST. HELENS
DRAWING NAME: 24-230 ROS
REVISED: 9-19-2024



“Back Lot”





Tempo

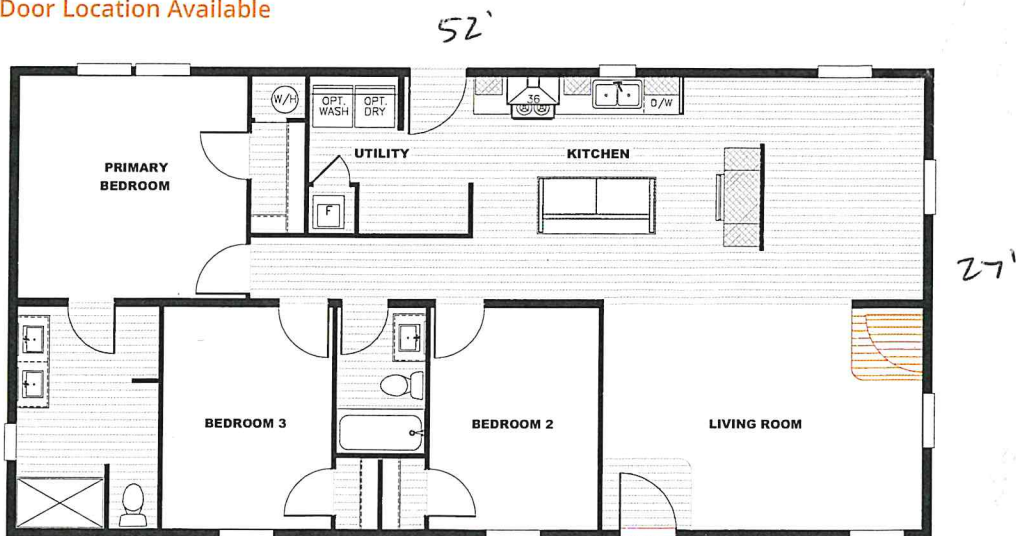
Item D.



Free Bird

55TMP28523AH22 | 1,387 SQ FT | 3 Bedrooms | 2 Baths

 Alternate Door Location Available



- Full Drywall Throughout
- Hardwood Cabinets
- Frigidaire® Stainless Steel Appliance Package
- Craftsman Style Front Door
- Ecobee® Smart Thermostat
- Carrier® Furnace



View This Plan Online

CITY OF ST. HELENS PLANNING DEPARTMENT ACTIVITY REPORT



To: City Council
From: Jacob A. Graichen, AICP, City Planner
cc: Planning Commission

Date: Nov. 25, 2024

This report does not indicate all *current planning* activities over the past report period. These are tasks, processing and administration of the Development Code which are a weekly if not daily responsibility. The Planning Commission agenda, available on the City's website, is a good indicator of *current planning* activities. The number of building permits issued is another good indicator as many require Development Code review prior to Building Official review.

ASSOCIATE PLANNER/PROJECT MANAGER—*In addition to routine tasks, the Associate Planner/Community Development Project Manager has been working on: See attached.*

PLANNING ADMINISTRATION—PREAPPLICATIONS MEETINGS

Conducted a pre-application meeting for a potential multifamily development at the S. 1st Street and Columbia Boulevard intersection.

Had a preliminary Q&A meeting for the city's sanitary sewer upgrades, specifically pump station #7 along Old Portland Road.

PLANNING ADMINISTRATION—MISC.

See **attached** public notice regarding dredging in the Columbia River. Figure 9 includes the St. Helens area.

Prepared for the department's semi-annual report to the City Council and Planning Commission.

Attended workshops for the inclusion of the Endangered Species Act onto the National Flood Insurance Policy.

Assisted property owner in Cherrywood Estates II with a wetland/storm water issue resulting from the c. 2001 development and the 2003 wetland rules the shortly followed. We've been communicating with this person off and on for some time, but some extra correspondence this month.

Sanitary sewage management issue for food trucks continue as staff works to get those within city limits in line with city regulations. This effort started about a year ago. Of the four locations we currently have, two are ok and two need to work towards being ok.

We had our last technical advisory committee meeting for the EOA this month. Good committee with valuable input.

PLANNING COMMISSION (& acting HISTORIC LANDMARKS COMMISSION)

November 12, 2024 meeting (outcome): The Commission held a public hearing for part 2 of the 2024 Development Code amendments. The Commission also discussed Planning Commission term expirations / reappointments and the 2025 public meetings schedule due to a conflict in November for the normal 2nd Tuesday in a month meeting date.

December 10, 2024 meeting (upcoming): The Commission will have three public hearings: (1) for CCMH campus expansion, (2) a Conditional Use Permit + four Variances to rebuild a triplex that burned several years ago, and 3) a pair of Variances for a couple of recently adjusted lots along the 100 block of S. 6th Street.

The Commission will also conduct interviews for three candidates to fill a vacancy starting in January and staff will present the Planning Department's semi-annual report.

GEOGRAPHIC INFORMATION SYSTEMS (GIS)

Quarterly data updates this month.

Several hours spent getting new Engineering employee Bashar Al-Daomi established with their ArcGIS Desktop. This was valuable though, as it reminded me that the GIS software we have been using is being retired in early 2026, so it is imperative that time is spent this year getting familiar with its replacement, ArcGIS Pro.

COUNCIL ACTIONS RELATED TO LAND USE

The Council approved "part 1" of the 2024 Development Code Amendments (file CPZA.1.24). Part 2's hearing will be in December. First reading for this Ordinance has occurred.

From: [Jennifer Dimsho](#)
To: [Jacob Graichen](#)
Subject: November Planning Department Report
Date: Friday, November 22, 2024 4:00:13 PM
Attachments: [image001.png](#)

Here are my additions to the November Planning Department Report.

GRANTS

1. **Riverwalk Project (OPRD Grants x2)** – Contractor is working on final shoreline bank revetment and remaining utilities. Concrete pours will continue to occur weather permitting. Construction timeline looks on track for an early completion. Contract is approximately 50% expended. Submitted our first LWCF grant disbursement of approximately 750k. Coordinated with Communications on E-newsletter content and timing of drone footage. Reviewing/tracking submittals and RFIs. Attending bi-weekly check-ins with contractor and design team.
2. **Community Development Block Grant (CDBG): Sanitary Sewer Improvement Project** – \$2.5 million grant award to fund design/engineering/permitting for 3 sanitary sewer basins identified as deficient in the adopted Wastewater Master Plan. Reviewed 30% design during workshop with consultant team. At approximately 30% of funds expended and between 30% design and 60% design. On track for our 3rd disbursement request in December for work through November.
3. **DLCD Technical Assistance Program** – 60k will fund a new Economic Opportunities Analysis (EOA). TAC meeting #3 held on 11/19 where the comprehensive plan economic goals and policies were reviewed. Preparing for the Joint CC/CC meeting on 12/11 where the final draft will be reviewed.
4. **ODOT Community Paths Program: St. Helens Scappoose Trail Refinement Project** – 405k to study a trail route refinement project (30% design) from St. Helens to Scappoose. ODOT says consultant selection will be soon. Coordinated with County and Scappoose on selection committee. Likely to kickoff early 2025.
5. **2024 Travel Oregon Grant Program: Riverwalk Project** - 100k grant for Riverwalk Project. Anticipated to receive remaining 50k when project is complete by April 2025.
6. **ODOT TGM Program: Transportation Systems Plan** – ODOT says it will likely be early 2025 when we forward with consultant selection. Traffic counts via cameras complete.
7. **2025 Travel Oregon Grant Program:** Attended webinar about the upcoming grant cycle. Awards are up to 150k for accessibility improvements. Considering an application for design and construction of Courthouse Plaza improvements. Letter of Interest available online December 2 when a deadline for December 15. Final grant applications due end of February 27, 2025.

PROJECTS & MISC

8. **Business Oregon – Infrastructure Finance Authority** – Low-interest loan for Streets &

- Utilities Project and Riverwalk improvements. Loan amendment coordinated and approved by Council on 11/20. Submitted Disbursement Request #4 which put us at about 85% complete with expenditures on the loan. Attending regular check-ins. Reviewed regular Waterfront E-newsletter content regarding construction updates, closures, progress, etc. Follow the [City's Waterfront E-newsletter](#) for timely updates.
9. **St. Helens US 30 Entry Sign** – Attended Columbia County Tourism Program Manager to discuss potential funding source for a St. Helens entry sign on US 30. Worked with Ramsay Signs to update our cost estimates for the existing design in both an illuminated and non-illuminated version.
 10. **2024 Certified Local Government Workshop** – Attended the SHPO CLG Workshop on 11/6 in Dallas, Oregon.
 11. **Oregon Government Ethics Commission Training** – Attended training on Public Meetings Law on 11/18 hosted by OGEC who newly administers the rules

Jenny Dimsho, AICP | Community Development Project Manager

City of St. Helens | Planning Department
265 Strand Street, St. Helens, OR 97051 | www.sthelensoregon.gov
P: (503) 366-8207 | jdimsho@sthelensoregon.gov





**US Army Corps
of Engineers**®
Portland District

PUBLIC NOTICE
OF PROPOSED DISCHARGES OF DREDGED AND FILL
MATERIAL FOR THE LOWER COLUMBIA RIVER
CHANNEL MAINTENANCE PLAN, DREDGED
MATERIAL MANAGEMENT PLAN
Oregon and Washington State

Issue Date: 10/7/2024

Expiration Date: 11/6/2024

Interested parties are hereby notified that the U.S. Army Corps of Engineers, Portland District (Corps) proposes to discharge dredged and fill material during implementation of a Dredged Material Management Plan and integrated Environmental Impact Statement (DMMP-EIS) for the purpose of maintaining the congressionally authorized channel dimensions of the Lower Columbia River (LCR) Federal Navigation Channel (FNC), part of the Columbia and Lower Willamette Rivers (C&LW) Project. This public notice is required by the provisions of section 103 of the Ocean Dumping Act (ODA) (aka, Marine Protection, Research, and Sanctuaries Act (MPRSA)) and section 404 of the Clean Water Act (CWA).

LCR FNC maintenance dredging would occur annually utilizing a variety of dredge equipment. The intent of maintenance dredging is to reduce impediments to navigation by dredging to authorized dimensions, plus advanced maintenance dredging (AMD). The overall purpose of the DMMP-EIS is to define the dredged material management practices for the LCR FNC to maintain the authorized dimensions for a minimum of 20 years using the dredged material disposal alternative that represents the least-cost consistent with sound engineering practices and meeting the environmental standards established by the CWA 404(b)(1) evaluation process and ocean dumping criteria. Dredged material is placed in a manner that retains sediment within the Columbia River system to the greatest extent practicable without increasing future dredging requirements. The Corps proposes to transport and place dredged material in deep water, in-water shallow sites, confined aquatic facilities (confined aquatic placement with structures), upland placement sites, beach nourishment (shoreline placement) sites, including the use of temporary in-water transfer sites when needed, and the ocean deep-water site (DWS) ocean dredged material disposal site (ODMDS).

Congress has authorized the Corps to construct and improve Federal navigation projects over time to provide safe, reliable, efficient, and environmentally sustainable waterborne transportation systems for the movement of commerce, national security needs, and recreation. The Congressional authorizations relevant to the LCR FNC have not changed since this channel was last deepened.

Project Description

The forecasted average volume of material needed to be dredged to maintain the authorized dimensions for the next 20 years is expected to be 9.2 million cubic yards annually. The channel is

enrockment only depending on location and river flow velocities. Portions of structures could be completely or periodically inundated with water or designed with crest elevations above high water depending on the local hydraulic conditions. New pile structures would be constructed using barge-mounted cranes that drive timber or steel piles into the river bottom using vibratory or impact hammers. Rock placement would occur using land-based or barge-based excavators and cranes, or specialized placement barges. Sites would be filled incrementally over multiple, similar placement events with each event raising a portion of the site to its estimated design fill elevation. Events may occur within the same dredge season or years apart.

Upland placement may occur on islands in the river or locations on the mainland accessible from the river. Upland sites are defined as sites that initially contain areas of land above ordinary high water. Areas below ordinary high water must first be filled using the shoreline method to create land. The upland placement method is used to place dredged material on top of existing land and fill up to the final site elevation. Sites are filled in layers over multiple, similar placement events with each event raising the fill elevation of the site. Events may occur within the same dredge season or years apart. Containment berms are typically created around the perimeter of the upland placement area. Discharge of water from the final settling pond back into the river is controlled by the use of weirs. The return water discharge pipe is typically submerged in the river 20 feet or deeper.

The crest elevations of shoreline placement sites are at or above the ordinary high water mark (OHWM)(or mean higher high water mark in tidally influenced areas) after placement. Shoreline sites are typically shaped as long, narrow fills. Smaller volume sites may be filled in a single event. Larger volume sites would be filled incrementally over multiple, similar placement events with each event creating new land over a portion of the site. Broader shoreline sites may then be subsequently filled to a higher elevation using the upland placement method. Events may occur within the same dredge season or years apart. During shoreline placement, dredged material is pumped through a floating discharge pipe. As the sand exits the shore pipe, the sand settles out on the shoreline while the water returns to the river. After sufficient sand has settled out and begins to increase in height, it is moved by bulldozers to match the elevation of the existing shoreline at approximately the high-water line. During placement, a temporary sand berm is constructed to retain sand.

A transfer site is an in water holding area created for temporary storage of dredged material. The transfer operation is used only when it is more efficient and less expensive than other methods available to move dredged material directly from a shoal to a permanent placement site. Transfer sites are typically operated between depths of 30 to 43 feet, although some shallower locations may require shallower operating depths (20 to 30 feet), and sites adjacent to the LCR FNC are dredged to the typical advanced maintenance depth of 48 feet. Transfer operations at a given location may be used once per year, more than once per year, or less frequently based on shoaling and dredge availability. Dredge placement operations are similar to in-water deep placement.

Ocean placement: Material dredged from the LCR FNC between RM 3 and RM 30 may also be placed in the ocean at the DWS ODMDS. The Corps only uses the site for Columbia River FNC dredge material if capacity was reached at other placement sites. Dredge placement operations are similar to in-water deep placement.

LCR FNC users may perform dredging to maintain their facilities which is a separate action from the DMMP-EIS. Information is available in Appendix L of the DMMP-EIS Plan which may be downloaded from <https://usace.contentdm.oclc.org/digital/collection/p16021coll7/id/26366>.

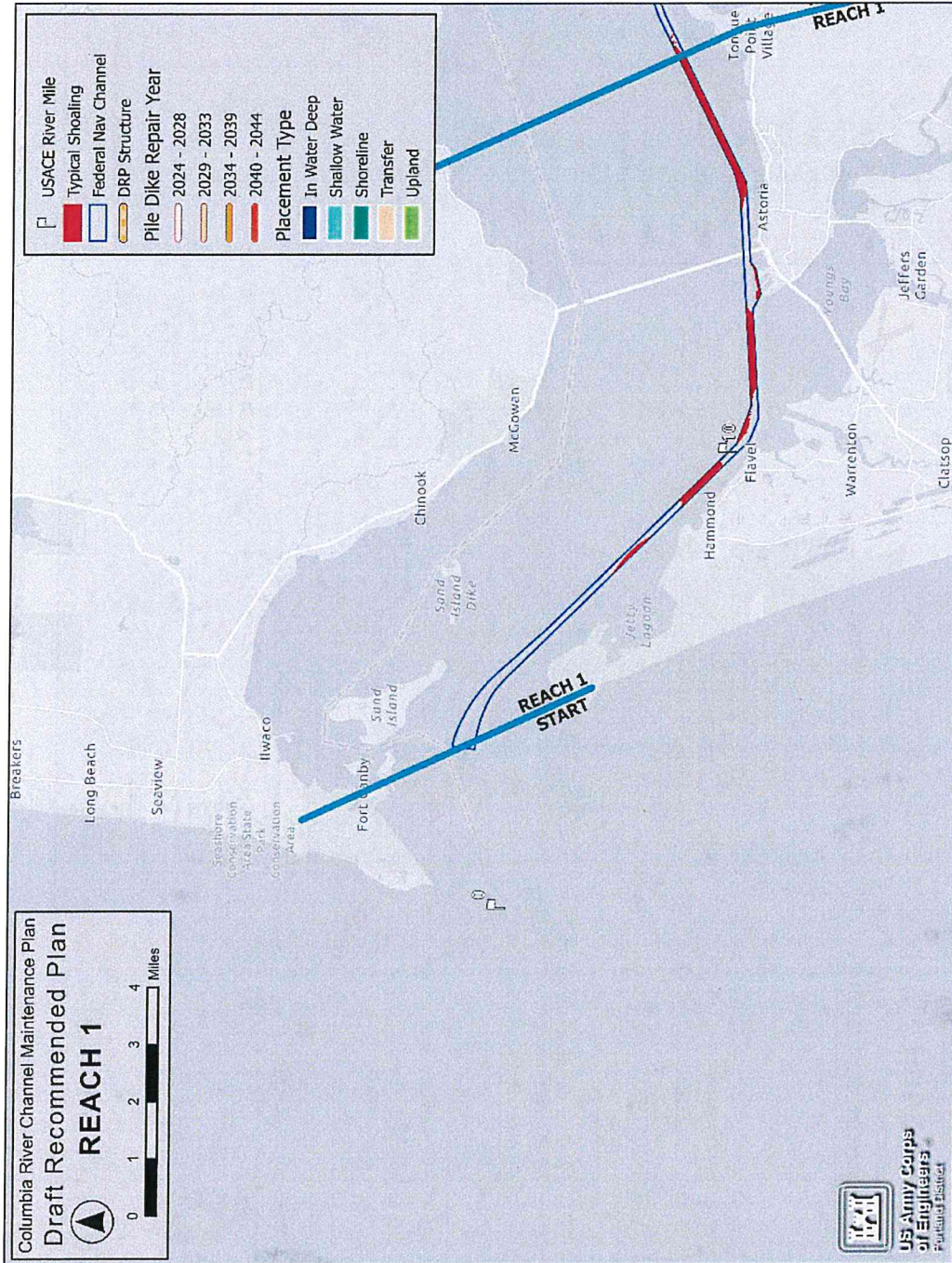


Figure 2. DMMP Recommended Plan Overview Map, Reach 1

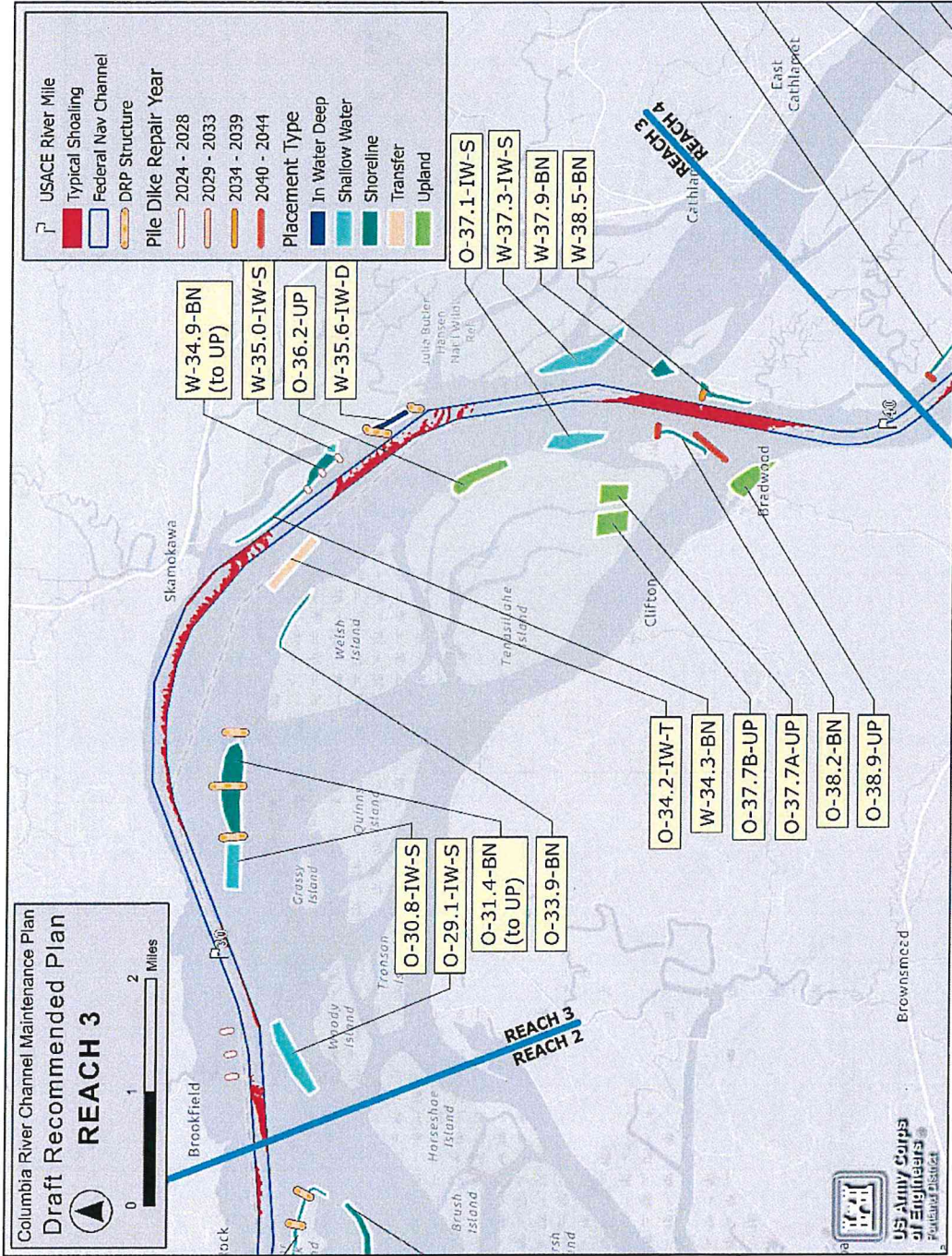


Figure 4. DMMP Recommended Plan Overview Map, Reach 3

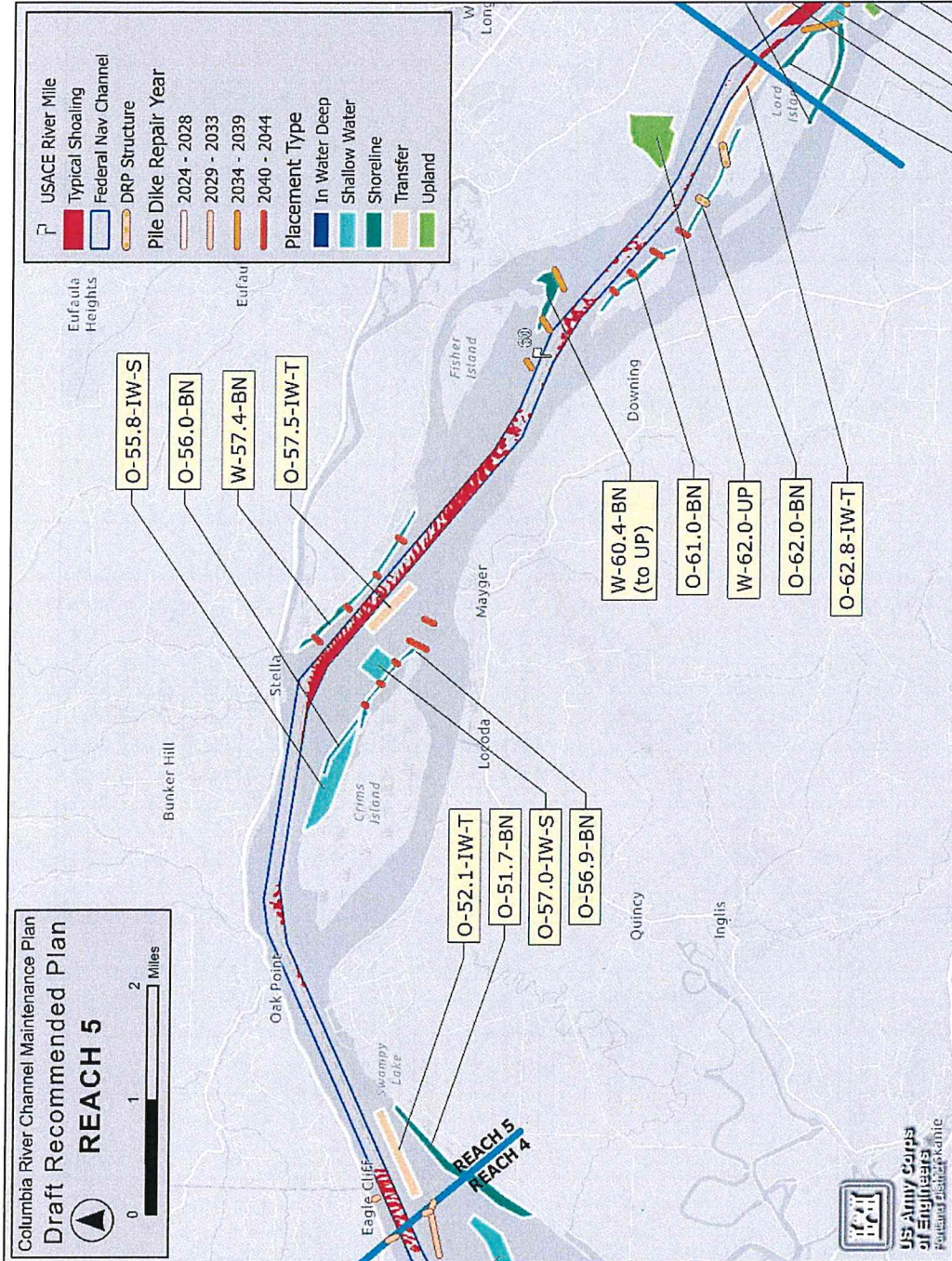


Figure 6. DMMP Recommended Plan Overview Map, Reach 5

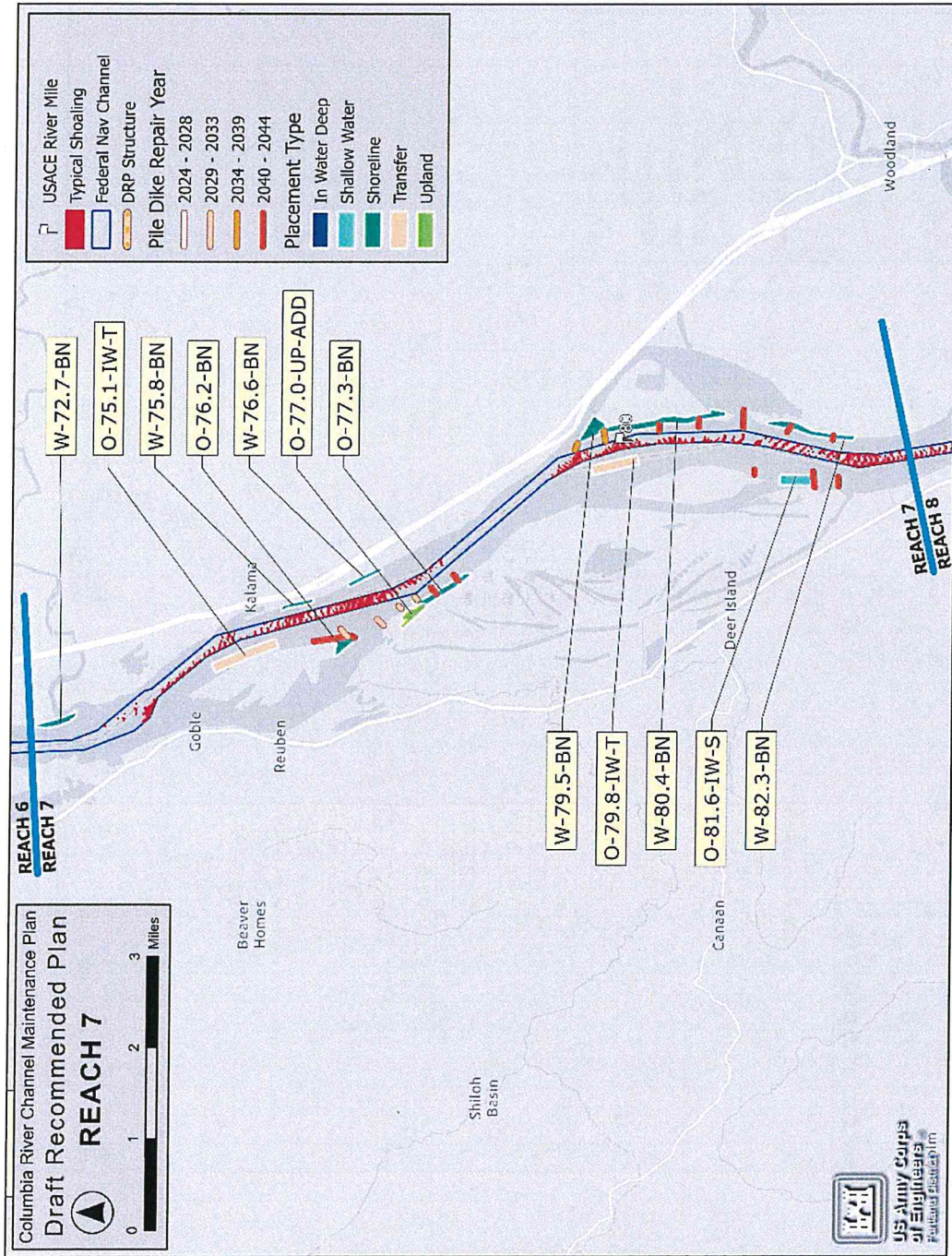


Figure 8. DMMP Recommended Plan Overview Map, Reach 7

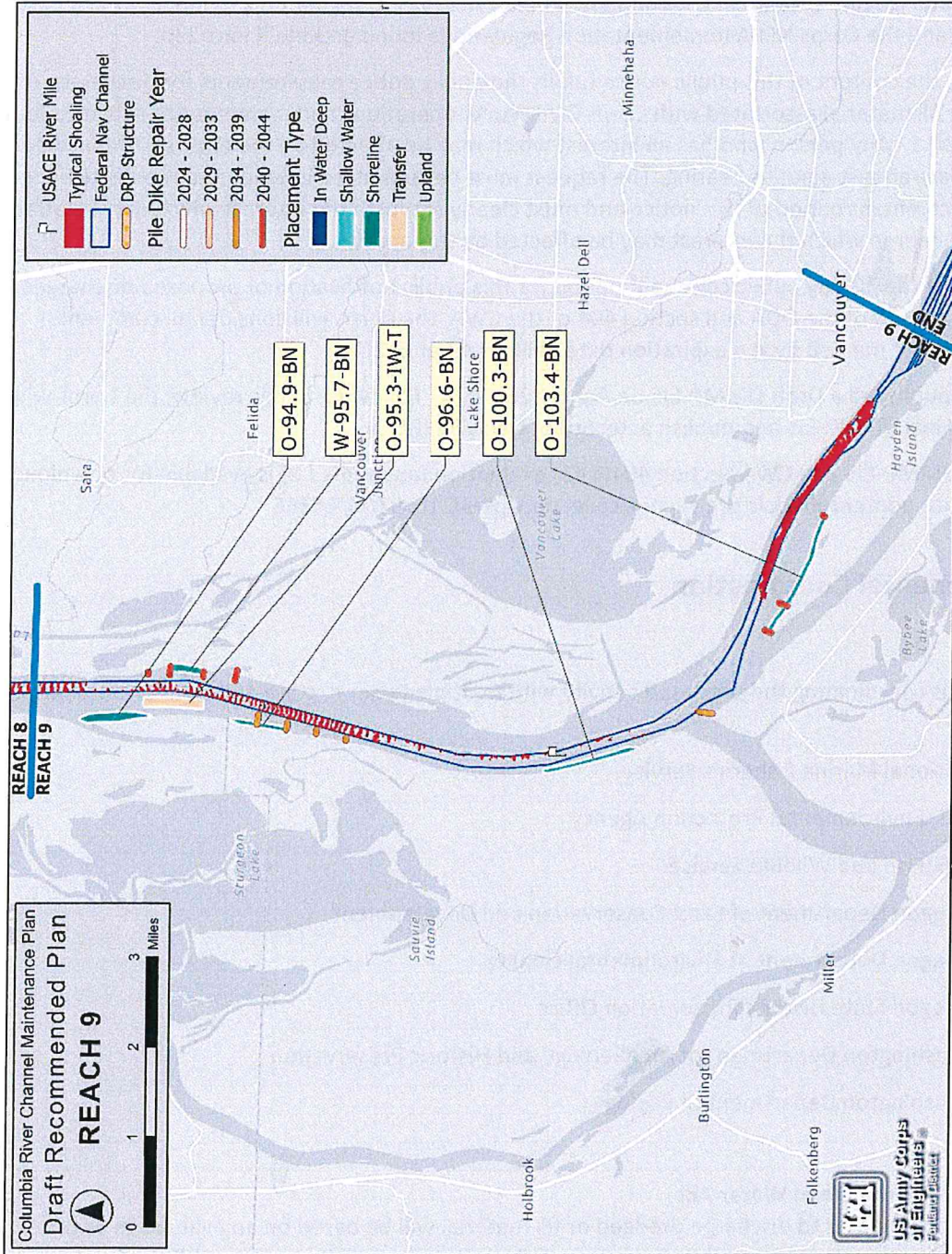


Figure 10. DMMP Recommended Plan Overview Map, Reach 9

including the cumulative effects thereof; among those are conservation, economics, aesthetics, general environmental concerns, wetlands, historic properties, fish and wildlife values, flood hazards, floodplain values, land use, navigation, shoreline erosion and accretion, recreation, water supply and conservation, water quality, energy needs, safety, food and fiber production, mineral needs, considerations of property ownership and, in general, the needs and welfare of the people.

The Corps completed an analysis of the proposed discharge of dredged and fill material pursuant to the guidelines of Section 404(b)(1) of the Clean Water Act (40 CFR 230). This analysis is available at the preceding link under DMMP-EIS Appendix J.2.

The proposed DMMP includes use of the DWS ODMDS. Additional criteria pursuant to the Marine Protection, Research and Sanctuaries Act of 1972 appear in description of environmental compliance under "Section 103 of the Marine Protection, Research and Sanctuaries Act" below.

Section 401 of the Clean Water Act

Following public review of the Draft DMMP-EIS, the Corps will submit certification requests to the Oregon Department of Environmental Quality (DEQ) and Washington Department of Ecology for those discharges associated with the Draft Recommended Plan (DRP) that lack existing certification. Certification, certification with conditions, or denial would occur prior to the agency official signing the Record of Decision for the DMMP-EIS.

Section 106 of the National Historic Preservation Act

The Corps submitted area of potential effects (APE) consultation letters to Oregon State Historic Preservation Office (Oregon SHPO), Washington Department of Archaeology and Historic Preservation (DAHP), and Tribes on March 23, 2020, pursuant to 36 CFR § 800.4[a][1]. Oregon SHPO and DAHP concurred with the APE on July 16, 2020, and June 30, 2020, respectively. Section 106 consultation for DMMP is ongoing. On May 1, 2024, the *Programmatic Agreement Among the U.S. Army Corps of Engineers, Portland District, the Advisory Council on Historic Preservation, the Oregon State Historic Preservation Office, and the Washington State Department of Archaeology and Historic Preservation Regarding the USACE Portland District Lower Columbia River Navigation Program, Oregon and Washington* (Nav Program PA), was executed with Signatories being Advisory Council on Historic Preservation, Oregon SHPO, DAHP, and the Corps. The Nav Program PA allows the Section 106 review for the DMMP implementation to be completed in a phased approach. Prior to any DMMP project location being prepared, work on, or used, the Corps will follow the NHPA Section 106 review process identified in the Nav Program PA to determine effects to historic properties and mitigation, if adverse effects will occur.

Endangered Species Act

The Corps has determined that the DMMP-EIS proposed action would affect listed species under the jurisdiction of USFWS. The Corps initiated formal ESA Section 7 consultation with USFWS for the Continued Operations and Maintenance Dredging Program, which is being substantively modified by the DMMP-EIS proposed action, in a letter emailed on January 26, 2024. The Corps has obtained a Biological Opinion from the USFWS for the proposed action.

The Corps has also determined that the DMMP-EIS proposed action would affect listed species under the jurisdiction of NMFS. As a result, the Corps reinitiated formal ESA Section 7 consultation with NMFS for the Continued Operations and Maintenance Dredging Program, which is being substantively modified by the DMMP-EIS proposed action, on March 11, 2024. Formal consultation will result in a Biological Opinion from NMFS.

Dredged material from the LCR FNC is predominantly composed of clean sand. In the Corps' May 2016 sediment characterization, the average sand content was greater than 97% sand and gravel, and less than 2.6% silt and clay. The total organic carbon content was also low at <0.3% (Corps 2017). Similar results were observed in the 1997 and 2008 characterizations of the LCR FNC sediments. In accordance with the SEF and the MPRSA sediment testing regulations found at 40 CFR § 227.13, chemical analysis was not required, because sand in the LCR FNC is part of the Columbia River bedload and is subject to strong currents. In these types of environments, sand is washed of the fine-grained material and is unable to bind contaminants, because the sand grains are inert. Based on repeated rounds of sediment characterization, the Corps and EPA have jointly determined that dredged material from the LCR FNC is suitable for unconfined, aquatic placement at both beneficial use sites and at ocean dredged material disposal sites. In accordance with the SEF, the Corps would continue to characterize sediments in the LCR FNC every 8 to 10 years to ensure they are suitable for unconfined, aquatic placement.

The MPRSA requires periodic dredged material disposal site monitoring and evaluation of the impact of dredged material disposal (40 CFR §§ 228.9-228.10). At the DWS, baseline surveys of sediment physical and chemical parameters and benthic infauna and epibenthic fauna were performed jointly by the EPA and Corps in 2001 and 2005. Monitoring surveys (led by the EPA with Corps assistance) were performed in 2013, 2014, and 2023. No adverse impacts from past dredged material disposal were observed. The EPA and Corps will continue to monitor the DWS every 8 to 10 years per the MPRSA's monitoring and impact evaluation requirements.

Additional Information and Comments

Additional information and links to download the draft DMMP-EIS can be found at the Lower Columbia River Channel Maintenance website: <http://www.nwp.usace.army.mil/lcrchannelmaintenance/>.

Questions and comments regarding this public notice can be emailed to LCR-CMP-DMMP@usace.army.mil or by calling 503-808-4708. Please add "DMMP Clean Water Act" to the subject line.

Mailed comments on this notice must be postmarked by the above expiration date and sent to:

U.S. Army Corps of Engineers
Attn: CENWP-PME-E / DMMP-EIS
P.O. Box 2946
Portland, OR 97208-2946

In your response, please refer to the above public notice title and date. Should no response be received by the above closing date, a "no comment" response will be assumed.



Amy Gibbons
Chief, Environmental Resources Branch
US Army Corps of Engineers, Portland District