

**CITY COUNCIL REGULAR MEETING AGENDA**City Hall - 10769 W State Street, Star, Idaho
Tuesday, December 17, 2024 at 7:00 PM

PUBLIC NOTICE: THIS MEETING IS RECORDED AND PLACED IN AN ONLINE FORMAT. PERSONS MAY EITHER VIEW OR LISTEN TO VIDEO / AUDIO OF THIS MEETING UNTIL SUCH TIME THE RECORDING IS DESTROYED UNDER THE CITY'S RETENTION POLICY.

1. **CALL TO ORDER** – Welcome/Pledge of Allegiance
2. **INVOCATION** – Associate Pastor Nathan Held, Calvary Star Church
3. **ROLL CALL**
4. **PRESENTATIONS**
 - A. Check Presentation to Warhawk Air Museum from Potato Feed
 - B. Check Presentation to Idaho Veteran's Wreaths
 - C. IIIA Annual Review
 - D. Police Chief Report
 - E. Fire Chief Report
5. **CONSENT AGENDA (ACTION ITEM)** **All matters listed within the Consent Agenda have been distributed to each member of the Star City Council for reading and study, they are considered to be routine and will be enacted by one motion of the Consent Agenda or placed on the Regular Agenda by request.*
 - A. Approval of Claims
 - B. **Approval of Minutes: November 19, 2024**
6. **PUBLIC COMMENT** - Members of the Public may address the Mayor and Council on any item not currently on the Agenda or on items currently or potentially set for a Public Hearing. Comments regarding City Staff, Elected Officials and Private Citizens should be set for a private meeting with the Mayor. **(Three Minute Time Limit)**
7. **PUBLIC HEARINGS with ACTION ITEMS:** (The Council at its option may suspend the rules requiring three separate readings on three separate days for ordinances on the agenda for approval. This may be by a single motion to suspend the rules under Idaho Code 50-902; second of the motion; ROLL CALL VOTE; Title of the Ordinance is read aloud; motion to approve; second of the motion: ROLL CALL VOTE.)
 - A. **PUBLIC HEARING: Legado Subdivision Preliminary Plat Modification (PP-22-05 MOD)** The Applicant is seeking approval of a Preliminary Plat Modification and Private Street with gates for a proposed residential subdivision consisting of 71 single-family residential lots and 12 common lots. The property is located on the west side of Bent Lane in Star, Canyon County, Idaho and consists of 23.77 acres with a proposed density of 2.99 dwelling units per acre. The property is currently zoned Residential R-5. The original preliminary plat was approved by City Council on March 23, 2023. **(tabled from November 19, 2024) - (ACTION ITEM)**
 - B. **PUBLIC HEARING: Pathway Master Plan Adoption.** Kimley-Horn/Logan Simpson Design has been contracted by the City for the creation of the City of Star Pathway Master Plan to consolidate the work done to date, detail the various pathway types, their locations, materials, and outline a system of supportive amenities. These plans have been further envisioned through the included Maintenance Plan and Capital Improvement Plan (CIP) for the implementation and regular maintenance of the trail system. - **(ACTION ITEM)**
 - C. **PUBLIC HEARING: Municipal Code Ordinance Amendment** Request from City Hall of Star to update and amend portions of their Municipal Code, including the current Development Code Title 8, Chapters 1 through 8, and titles 1 through 7 and Title 10. - **(ACTION ITEM)**
8. **ACTION ITEMS:**(The Council at its option may suspend the rules requiring three separate readings on three separate days for ordinances on the agenda for approval. This may be by a single motion to suspend the rules under Idaho Code 50-902; second of the motion; ROLL CALL VOTE; Title of the Ordinance is read aloud; motion to approve; second of the motion: ROLL CALL VOTE.)
 - A. **Amendment to Ada County Sheriff's Office Contract for approval of Code Enforcement Officer and authorize the Mayor to sign amended contract (ACTION ITEM)**
 - B. **Garnet Estates Subdivision Development Agreement Modification** - The new owners of the property located at 6697 Foothill Road are requesting a modification of the current Development Agreement for Garnet Estates Subdivision, removing the approved preliminary plat, and revising conditions of annexation to allow two (2) single-family dwellings on the existing 5-acre parcel. **(ACTION ITEM)**
9. **ADJOURNMENT**

Any person needing special accommodation to participate in the above noticed meeting should contact the City Clerk's Office at 208-286-7247, at least 24 hours in advance of the meeting date.



CITY OF STAR, IDAHO

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The meeting can be viewed via a link posted to the City of Star website at staridaho.org. Information on how to participate in a public hearing remotely will be posted to staridaho.org under the meeting information. The public is always welcomed to submit comments in writing.

Land Use Public Hearing Process

Public signs up to speak at the public hearing

Mayor Opens the Public Hearing

Mayor asks council if there is any Ex Parte Contact

Applicant has up to **20 minutes** to present their project

Council can ask the applicant questions and staff questions

Public Testimony (**3 minutes per person**)

1. Those for the project speak
2. Those against the project speak
3. Those who are neither for or against but wish to speak to the project
4. Council may ask the individual speaking follow-up questions that does not count towards their 3 minutes

Applicant rebuttal (**10 minutes**)

Council can ask the applicant and staff questions

Mayor closes the public hearing

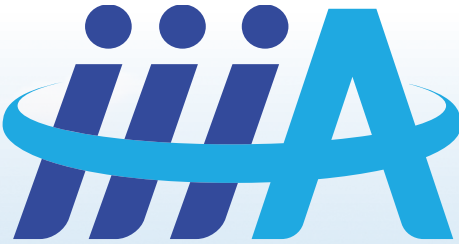
Council deliberates

Motion is made to approve, approve with conditions, deny or table the application to a date certain in the future

Thank you for coming to the Star City Council meeting, public involvement is fantastic and helps in shaping our city for the future. As this is a public hearing, there will be no cheering, clapping, jeering or speaking out during the hearing. Only the person at the podium has the floor to speak during their allotted time. If someone does speak out, cheer, claps, etc. they will be asked to leave the hearing and or escorted out of the hearing. We want to keep these hearings civil so everyone can be heard.

Thank you for your participation.

Mayor Trevor Chadwick

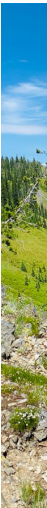


Shared Strength · Trusted Care



2023-24 Annual Report

For Distribution



MISSION

To provide the best quality and structure of health benefits plans and services to our agency’s employees by directly managing the costs and administration of those benefit plans through a cooperative pool in a manner that will be more efficient, economical, and competitive than what the market can offer through traditional insurance providers.



VISION

To provide the best and most affordable health benefit plans possible to local government agencies throughout the State of Idaho. To assist agencies to attract and retain quality employees in the local government sectors through the provision of affordable and quality health benefit plans.

114
Total Agencies
Retention: 100%

2,448
Total Employees
6,440
Total Members

\$37,686,976
Annual Budget

98%
Agency Participation
in Wellness
Screenings

\$2,267,178
Total Savings
from Programs

III-A AGENCIES

IDAHO CITIES

Aberdeen
 American Falls
 Arco
 Athol
 Blackfoot
 Bonners Ferry
 Cascade
 Challis
 Cottonwood*
 Council
 Dalton Gardens
 Declo
 Donnelly
 Dover
 Downey
 Dubois
 Emmett
 Franklin
 Firth
 Fruitland
 Garden City
 Grangeville
 Hayden
 Hayden Lake*
 Homedale
 Kamiah
 Ketchum
 Kooskia
 Kootenai
 Lava Hot Springs
 Malad
 Marsing
 McCall
 Menan
 New Meadows
 New Plymouth
 Nezperce
 Oakley
 Orofino
 Parma
 Paul
 Payette
 Potlatch
 Preston
 Rathdrum*
 Rupert
 Salmon

Shelley
 Star
 St. Anthony
 St. Maries*
 Teton
 Troy
 Victor
 Wilder

HIGHWAY DISTRICTS

Buhl
 East Side
 Filer
 Ferdinand
 Grangeville
 Hillsdale
 Keuterville
 Minidoka
 Power County
 Worley

FIRST RESPONDERS

Bear Lake Co. Fire
 Blackfoot Police & Fire
 Buhl Fire
 Canyon County Ambulance*
 Cascade Rural Fire
 Eagle Fire
 East Side Fire
 Kootenai County EMS
 Kootenai County Sheriff
 Kuna Fire
 Marsing Fire
 Mica Kidd Island Fire*
 McCall Fire
 Meadows Valley Rural Fire
 Middleton Fire
 Minidoka County Fire
 Nampa Fire
 Nampa Police
 Northern Lakes Fire
 Oneida County Sheriff & Probation*
 Sagle Fire
 Sandpoint Fire
 Shoshone County Fire #2
 Spirit Lake Fire
 St. Maries Fire
 Star Fire

Teton County Fire & Rescue
 Weiser Fire*
 Westside Fire
 Worely Fire*

IRRIGATION DISTRICTS

Black Canyon*
 Boise Kuna
 King Hill*
 Lewiston Orchards
 Minidoka

LIBRARY DISTRICT

American Falls

TRANSPORTATION AUTHORITY

Mountain Rides
 S.P.O.T

ABATEMENT DISTRICTS

Payette County Gopher
 Twin Falls Pest

RECREATION DISTRICTS

Middleton Parks
 Payette County

WATER & SEWER DISTRICTS

North Lake Recreation
 Southside
 Star Sewer

CEMETERY DISTRICTS

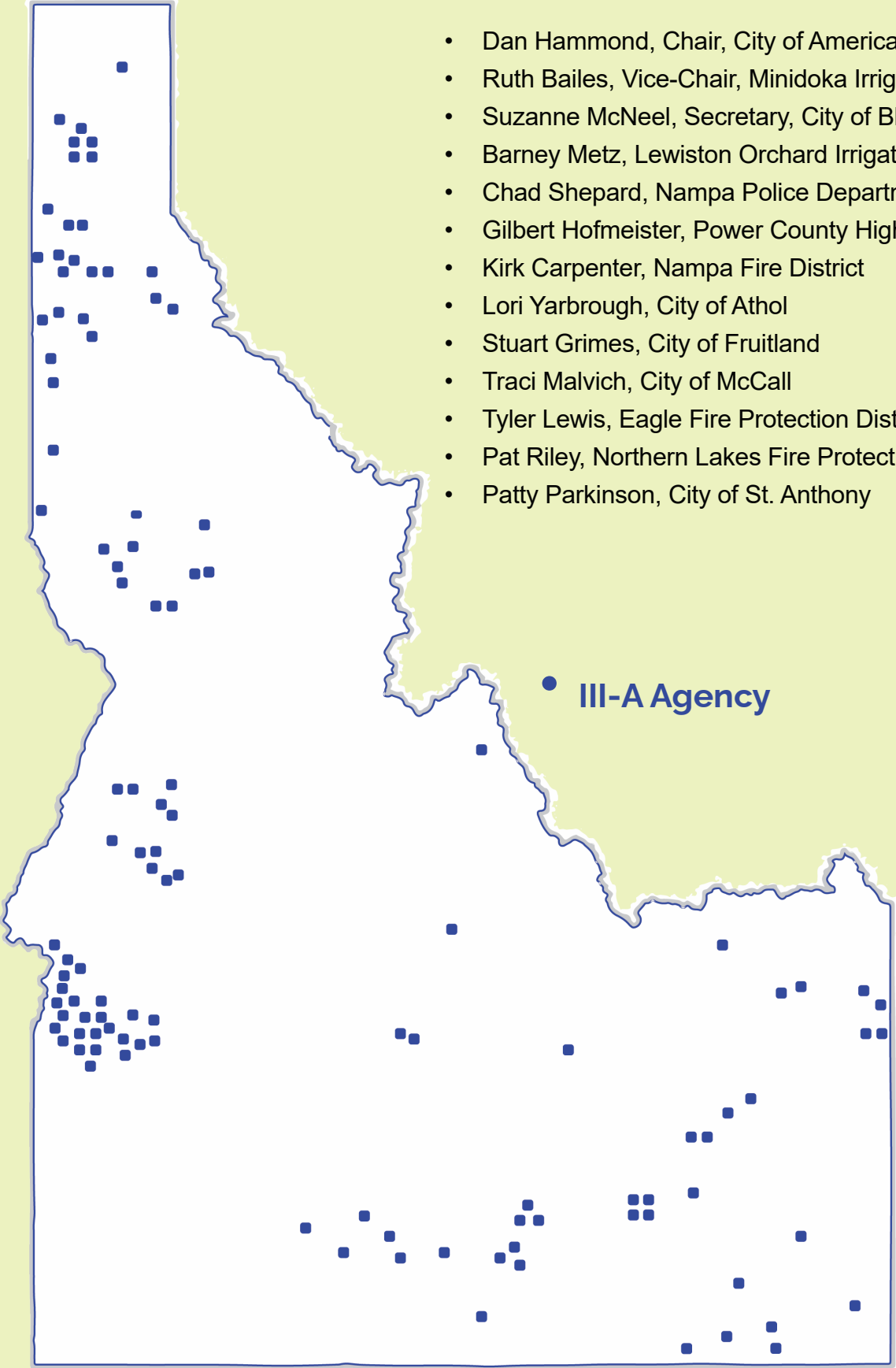
Marsing Homedale
 Star*

OTHER

III-A
 LHTAC

*NEW AGENCIES

BOARD OF TRUSTEES



- Dan Hammond, Chair, City of American Falls
- Ruth Bailes, Vice-Chair, Minidoka Irrigation District
- Suzanne McNeel, Secretary, City of Blackfoot
- Barney Metz, Lewiston Orchard Irrigation District
- Chad Shepard, Nampa Police Department
- Gilbert Hofmeister, Power County Highway District
- Kirk Carpenter, Nampa Fire District
- Lori Yarbrough, City of Athol
- Stuart Grimes, City of Fruitland
- Traci Malvich, City of McCall
- Tyler Lewis, Eagle Fire Protection District
- Pat Riley, Northern Lakes Fire Protection District
- Patty Parkinson, City of St. Anthony

THE III-A TEAM



Amy Manning
Executive Director



Megan Smith
Wellness & Data Director



Susan Lasuen
Operations Director



Alaysia Wallace
Benefits Specialist



Brooke Calton
Administrative Specialist



Hana Waters
Senior Benefits Specialist



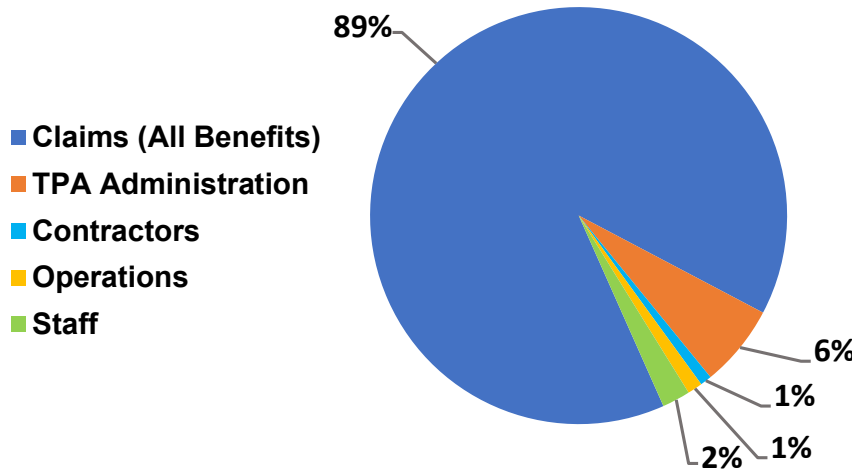
Kandice Dickinson
Marketing & Education Manager



Nicole Tuttle
Benefits Manager



Tami Testa
Benefits Manager



Administration and Operating Expenses:

- 23-24: 2%
- 22-23: -3%
- 21-22: -2%
- 20-21: -9%

MEMBER RELATIONS

- **Benefit Line calls:** 5,536
- **Internally administered benefits:** 11
- **Conference sponsorships:** 11
 - Idaho Water Users Association
 - Idaho Rural Water Association (Spring/Fall)
 - Idaho Fire Chiefs Association
 - Idaho Chiefs of Police Association
- Idaho City Clerks, Treasurers, and Finance Officers Association
- Idaho State Fire Commissioners' Association
- Idaho Association of Highway Districts
- Female Leadership in Policing
- Public Sector HR Association
- Idaho Counseling Association
- Association of Idaho Cities

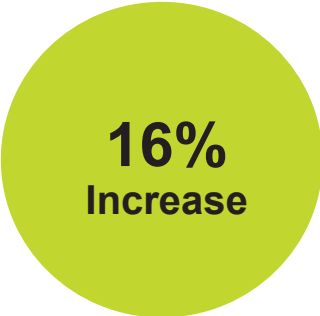
FINANCE



Rx Rebates



Subrogation



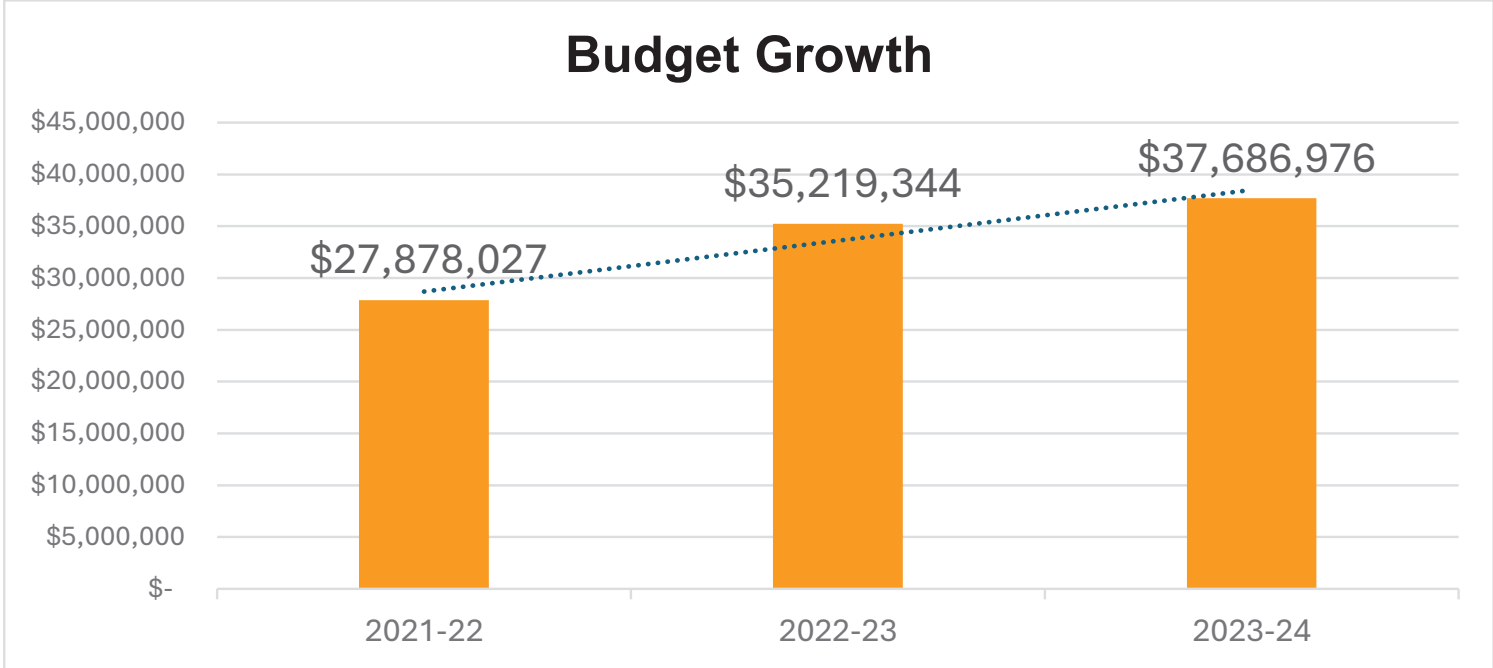
**Stop Loss
Premiums**



Interest Earned



IBNP Reserves



MEDICAL PLAN

46

Provided III-A Info

25

Rated

0

Dropped Benefits

12

Joined

	III-A Renewal	Trend
PY20	2.6%	7.0%
PY21	1.0%	6.0%
PY22	9.5%	6.0%
PY23	12.0%	6.5%
PY24	6.1%	7.1%

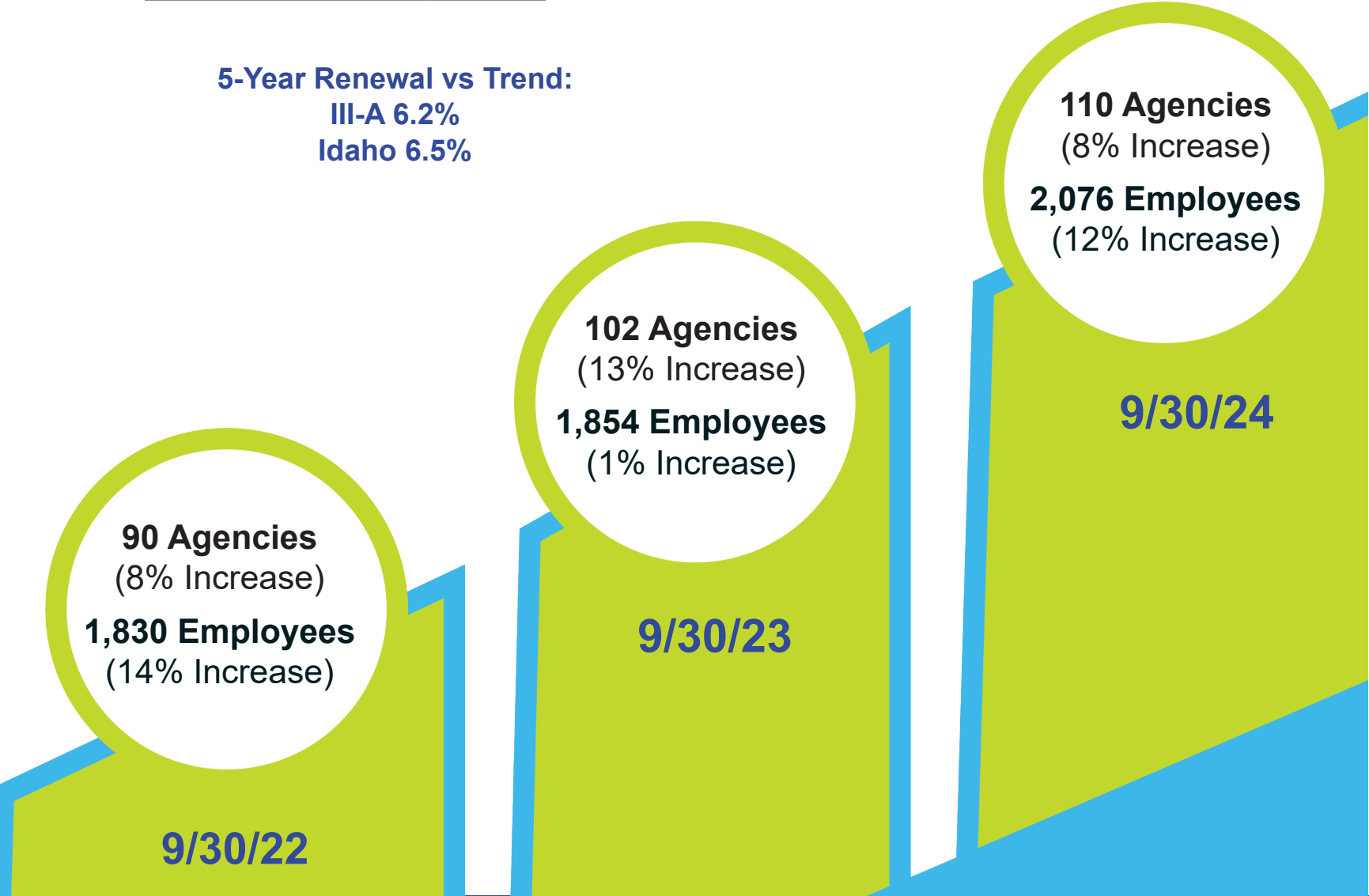
5

Declined Agencies

1

Small Agencies Now Able to Offer Benefits

5-Year Renewal vs Trend:
III-A 6.2%
Idaho 6.5%



HIGH CLAIMANTS & STOP LOSS

Stop Loss Deductible: \$450,000 with \$50,000 Aggregate Corridor

Member	Total Paid	Stop Loss Reimbursement	Pending Reimbursements
1	\$4,183,032	(\$3,407,411)	(\$809,235)
2	\$778,002		(\$328,002)
3	\$516,591		NA: \$600K Laser
4	\$513,030		(\$63,030)
5	\$398,433		
6	\$385,721		
7	\$361,562		
8	\$264,105		
9	\$253,610		
10	\$238,944		
11	\$212,848		
12	\$210,914		
13	\$206,464		
14	\$206,204		
15	\$185,417		
16	\$182,445		
17	\$178,852		
18	\$178,246		
19	\$166,365		
20	\$160,345		
21	\$154,246		
22	\$147,167		
23	\$141,153		
24	\$126,634		
25	\$123,988		
26	\$116,239		
27	\$114,344		
28	\$112,421		
29	\$111,640		
30	\$107,677		
31	\$104,299		
\$11,140,939		(\$3,407,411)	(\$1,200,267)

	Past Plan Year High Claimant
Bold Red	Termed
	Hit Stop Loss (claims >\$450K)

2022-2023 Comparison: 23 members, total paid: \$5,298,646

*This report will not be final until 12/31/24 due to run out claims.

MEDICAL PROGRAMS

Maternity

\$157,232
Estimated Savings

Medication Infusion

\$300,122
Estimated Savings

Telehealth

\$305,505
Estimated Savings
(Calls: 4,185)

Digital Physical Therapy

\$422,940 Estimated Savings
226 Members
8,903 Sessions

Rx Patient Assistance/ Co-Pay Assistance Program

\$1,081,379 Actual Savings
(93% of projection)

Wondr Health

2,010 lbs Lost
Wondr Health: 255 Members
Wondr Advanced: 11 Members
2,256 Sessions

Tobacco Cessation

3 Members

Health Coaching

52 Members

TOTAL ESTIMATED SAVINGS \$2,267,178

WELLNESS SCREENINGS

1,172

Members (56%)
(12% increase from 22-23)

101

Agencies
(98% Participation)

3

Elevated PSA

3

Positive Cologuards

1

Type 2 Diabetes Dx

138

Elevated Lipid Panels



Nurse Practitioners and staff made follow-up calls to all members with elevated results.

Dermatology Skin Checks

427

Members Screened

101

Agencies

4

Possible Melanomas

2

Stage 1 Melanoma
(Lesion removed)

15

Non-Melanoma
Skin Cancers

Dermatologist PA made follow-up calls to all members with higher risk conditions to ensure further evaluations were performed.



DENTAL PLAN

1,075

Total Employees

4

Added

0

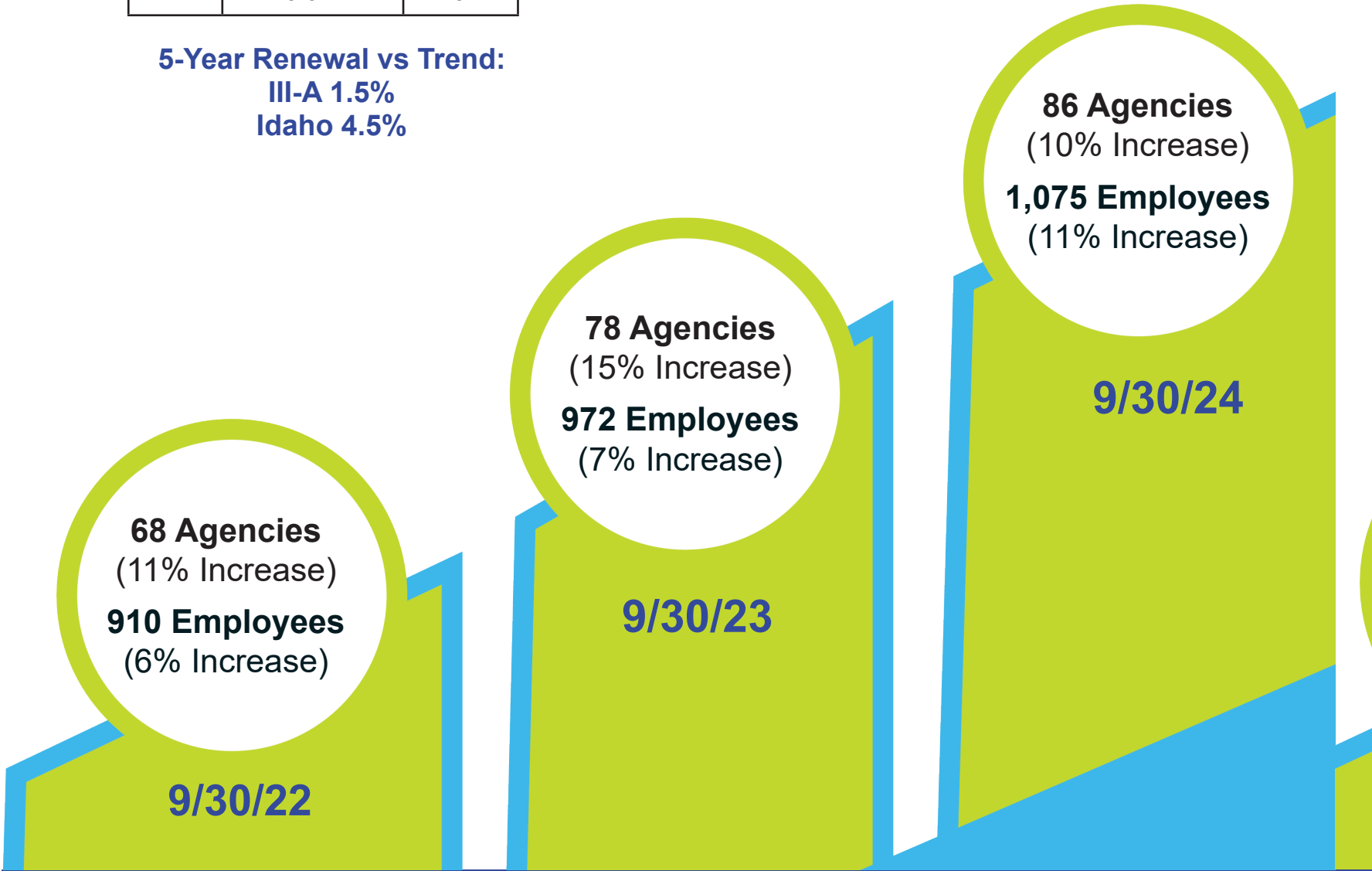
Dropped Benefits

1

Small Agencies Now Able to Offer Benefits

	III-A Renewal	Trend
PY20	3.0%	5.0%
PY21	0.0%	5.0%
PY22	4.4%	3.5%
PY23	0.0%	4.0%
PY24	0.0%	4.9%

5-Year Renewal vs Trend:
III-A 1.5%
Idaho 4.5%



VISION PLAN

1,226

Total Employees

3

Added

0

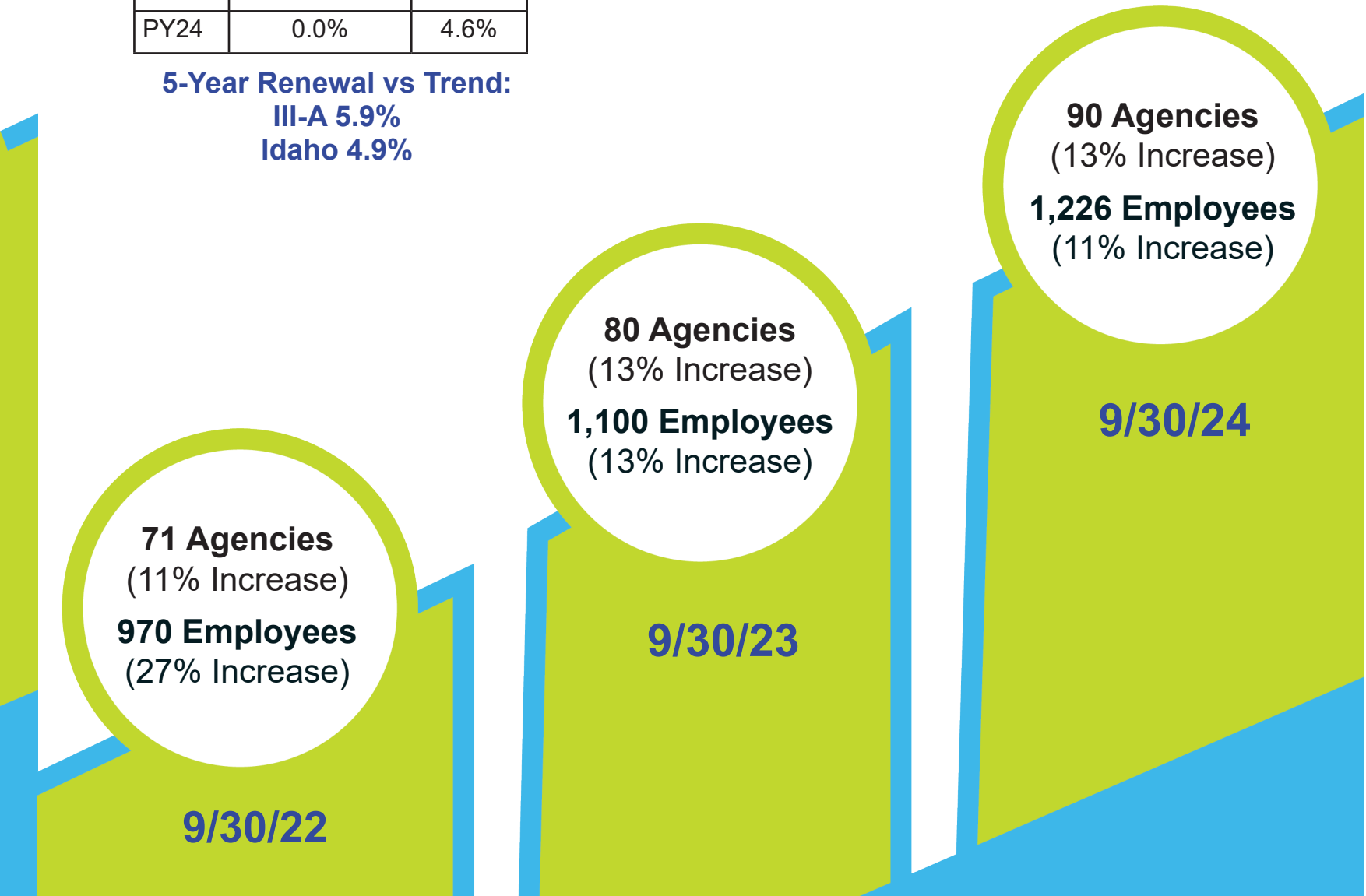
Dropped Benefits

1

Small Agencies Now Able to Offer Benefits

	III-A Renewal	Trend
PY20	15.0%	5.0%
PY21	0.0%	5.0%
PY22	0.0%	5.0%
PY23	14.3%	5.0%
PY24	0.0%	4.6%

5-Year Renewal vs Trend:
III-A 5.9%
Idaho 4.9%



MENTAL HEALTH PLAN

Section 4, Item C.

2,448

Total Employees

5

Provided Ill-A Info

3

Joined

8

Agencies MH for
Volunteers/Elected
Officials

283

EAP
Providers

2,794

EAP Visits

68

Onsite Mental Health
& Wellness Trainings

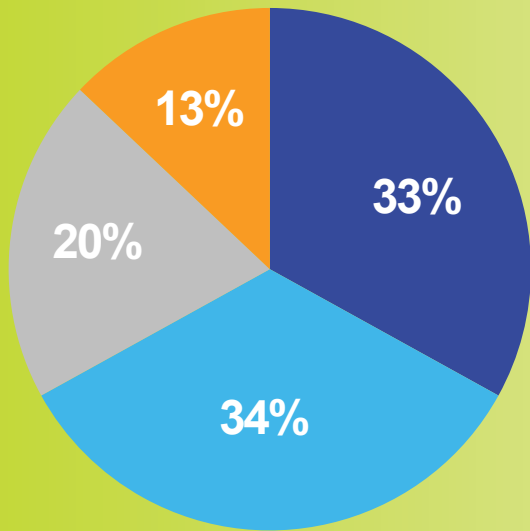
533

Wellness
Wednesday Webinar
Participants

(108% Increase)



MENTAL HEALTH PLAN



Calls by Region:

- Treasure Valley
- Northern Idaho
- Eastern Idaho
- Decline to State/Unknown

224

First Responder Helpline Calls

(4% increase)

5

**First Responder
Crisis Calls**

7

**Critical Incident
Debriefs**

30

Inpatient
(43% increase)
Employee: 11
Spouse: 3
Child (under 26): 16

48

**EAP
First Responder
Providers**





LOOKING FORWARD . . .

- Reduction in Professional/Third Party Fees
- SmithRx - Amazon, Mark Cuban Pharmacy
- 15-Minute Mindful Mondays (first Monday of each month)
- CPAP & BiPAP Program
- New Financial Institution
- New Accounting Software
- Website Redesign
- New Enrollment/Billing Software
- Regional Mental Health Wellness Teams

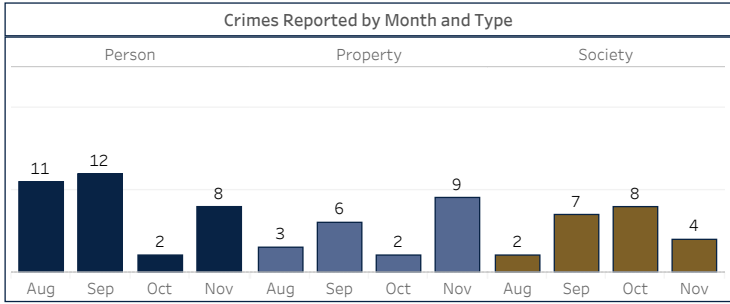


November 2024 Police Report

Release Date 2024-12-06

Offenses Reported¹

Types	2021	2022	2023 ²	2024 YTD
Person	74	110	96	80
Property	59	92	117	61
Society	52	67	134	87
Total Crimes	185	269	347	228
Crimes/1,000 Pop	13.8	17.7	19.6	



Police Activity³

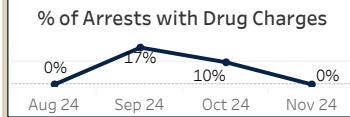
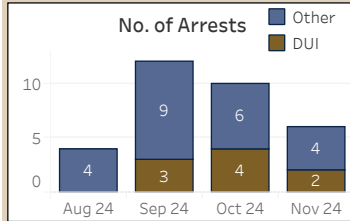
	2024 Monthly Avg ⁴	Aug 24	Sep 24	Oct 24	Nov 24	November 2023
Citizen Calls for Service (CFS)	311	348	350	318	318	276
Proactive Policing	1,795	1,754	1,570	1,825	2,097	1,688

Select Call Types

	2024 Monthly Avg ⁴	Aug 24	Sep 24	Oct 24	Nov 24	November 2023
Crash Response	31	25	38	26	33	26
Crisis/Mental Health ⁵	11	7	8	9	8	16
Domestic Violence	5	10	6	2	6	5
Juvenile Activity	18	18	18	19	19	18
Location Checks ⁶	348	283	263	282	433	351
Property Crime Calls ⁷	17	17	20	18	20	16
School Checks	41	38	44	50	36	43
Traffic Stops	322	275	284	261	315	401
Welfare Checks	21	20	19	16	22	28

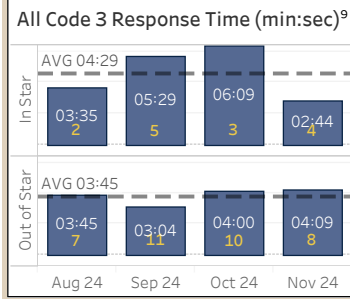
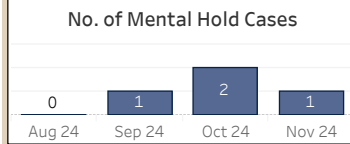
Case Report Types

- Person Crimes** = murder, manslaughter, rape/sodomy, assault, intimidation and kidnapping offenses
- Property Crimes** = robbery, burglary, larceny/theft, arson, destruction of property, counterfeiting, fraud, embezzlement, blackmail and stolen property offenses
- Society Crimes** = drugs/narcotics, gambling, pornography, prostitution and weapons law violations



Citations⁸

	Aug 24	Sep 24	Oct 24	Nov 24
Infraction	46	36	30	47
Misdemeanor	3	13	11	19



¹Offense Reports are compiled from NIBRS RMS. ²Counts updated to reflect NIBRS audit update. ³Police Activity reflects calls in the City of Star and all calls dispatched with a Star deputy. ⁴Monthly averages are based on all prior months of the current year. ⁵Calls are for Crisis/Mental Hold/Suicidal Subject/Attempted Suicide. ⁶Location checks include Construction Site, Property, and Security checks. ⁷Property Crime Calls include Theft, Vandalism, Burglary, Fraud. ⁸Infraction and Misdemeanor Citations issued by a Star deputy as listed in the current Patrol schedule. ⁹Code 3 Calls represent all incidents routed at Priority 3, where Priority 3 calls require an immediate emergency response.





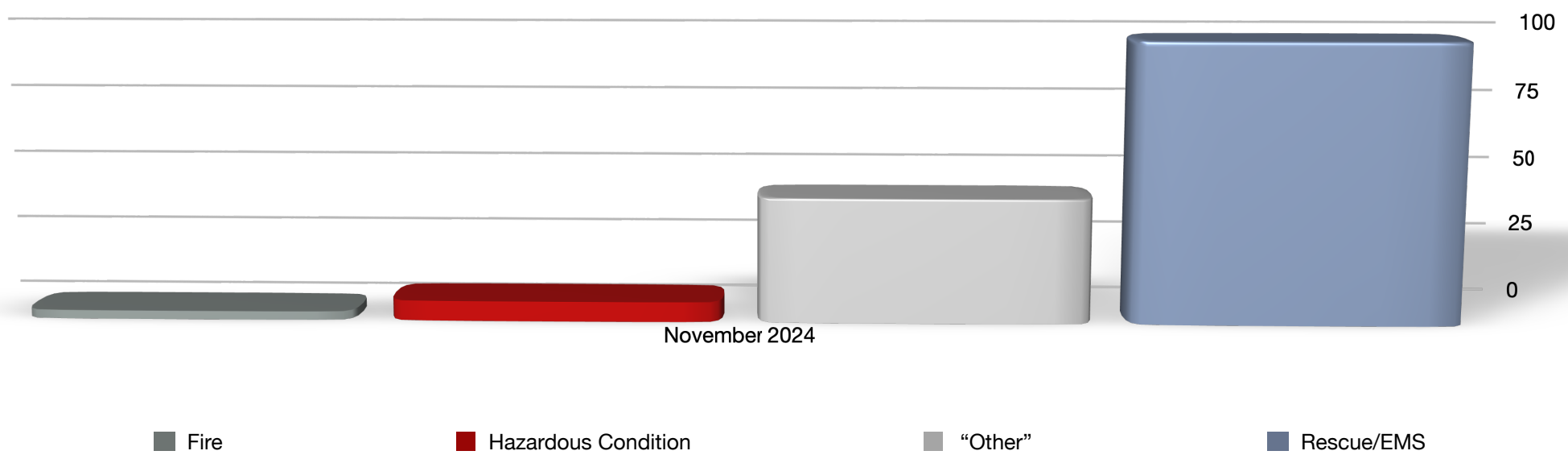
DECEMBER 2024 FIRE REPORT

Release Date: 12/12/24

INCIDENT TYPES

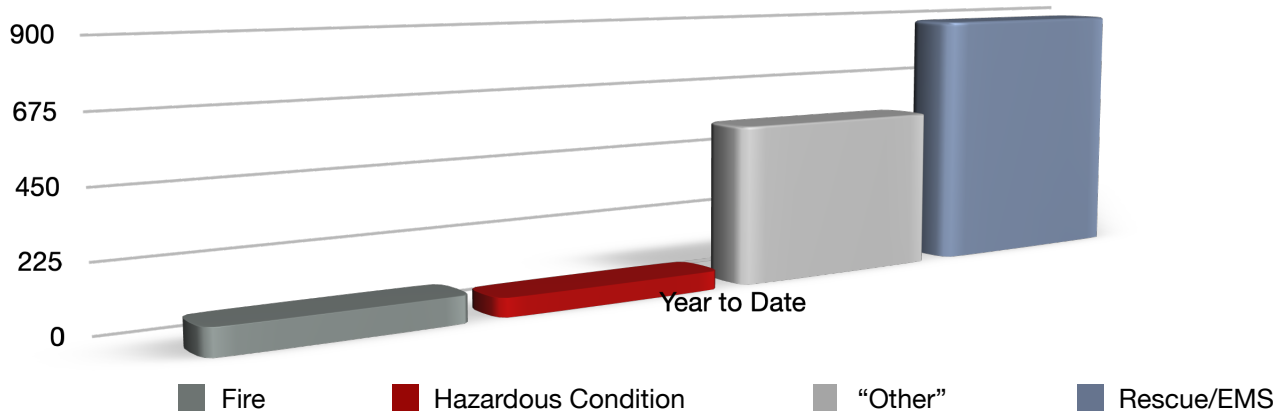
Date	Fire	Hazardous Condition	"Other"	Rescue/EMS	Total
November 2024	3	7	44	99	153
Year to Date	89	63	547	880	1,579

NOVEMBER 2024

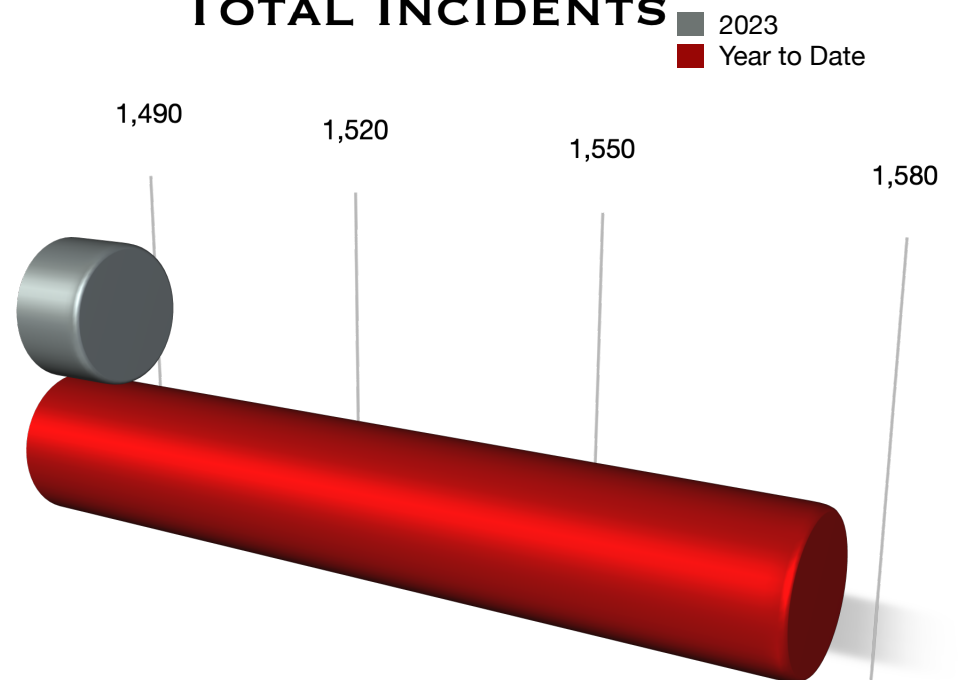


* FIRE call types include: Structure, Grass and Vegetation, Vehicle and other Fire types
 * Rescue & EMS: All EMS calls and vehicle accidents with or without injury
 * Hazardous Conditions: Carbon Monoxide alarm, Gas leaks, Electrical hazards
 * Other call types: Service calls, Good intent calls and false alarms

INCIDENTS YEAR TO DATE



TOTAL INCIDENTS



***2024 Call data is not complete due to a network outage on July 14th - 19th*

TOTAL INCIDENTS

Date	Total
2023	1,499
Year to Date	1,579

Vendor Name	Doc #	Invoice #/Description	Date	Warrant Amount	Inv Date	Period	Fund	Org	Account	Object	
208 CYCLES BIKE SHOP	CL 4389	Quote 87 Bulls Iconic EVO 2 Speed	11/21/24	23676	6,999.98	11/20/24	11/24	10	0	42110	365
A & B LOCK AND KEY	CL 4379	65277 R&R Ignition Chevy TK 15	11/19/24	23679	175.00	10/28/24	10/24	10	557	41540	434
ACTION PLUMBING	CL 4468	3171 Clean Backflow Strainer	12/06/24	23719	300.00	11/21/24	12/24	10	554	41540	434
ACTION PLUMBING	CL 4468	3178 Repair Toilets at Dog Park	12/06/24	23719	400.00	12/03/24	12/24	10	554	41540	434
ADA COUNTY LANDFILL	CL 4356	30767 Landfill Fees	11/15/24	23667	15.00	11/05/24	11/24	10	0	41595	831
ADA COUNTY LANDFILL	CL 4356	30195 Landfill Fees	11/15/24	23667	32.64	11/04/24	11/24	10	0	41595	831
ADA COUNTY LANDFILL	CL 4386	35527 Landfill Fees	11/21/24	23682	15.00	11/12/24	11/24	10	0	41595	831
ADA COUNTY LANDFILL	CL 4386	38185 Landfill Fees	11/21/24	23682	15.00	11/16/24	11/24	10	0	41595	831
ADA COUNTY PROSECUTORS OFFICE	CL 4363	Nov 2024 Prosecutions Contract	11/15/24	-99493	3,541.90	10/28/24	11/24	10	0	41300	327
ADA COUNTY SHERIFF'S OFFICE	CL 4450	122439 Police Services Dec 2024	12/05/24	-99461	287,757.92	12/02/24	12/24	10	0	42110	365
ALLY HOMER	CL 4362	Riverhouse Deposit Refund	11/15/24	23668	500.00	11/05/24	11/24	10	0	40700	882
AMAZON CAPITAL SERVICES	CL 4435	*VGXJ-4GRX Christmas Event Supplies	12/03/24	-99469	277.50	12/01/24	11/24	10	0	47001	594
AMAZON CAPITAL SERVICES	CL 4435	*WRWV-C4DK Christmas Hats/Headbands	12/03/24	-99469	76.15	12/01/24	11/24	10	0	47001	594
AMAZON CAPITAL SERVICES	CL 4435	*Y3R1-9T9V Laminating Sheets	12/03/24	-99469	21.87	12/01/24	11/24	10	0	41810	611
AMAZON CAPITAL SERVICES	CL 4435	*VGXJ-6HGM Door Cover/Porch Sign	12/03/24	-99469	15.96	12/01/24	11/24	10	0	47001	594
AMAZON CAPITAL SERVICES	CL 4435	*TGTR-7MV3 GameTable Covers	12/03/24	-99469	77.98	12/01/24	11/24	10	0	44022	612
AMAZON CAPITAL SERVICES	CL 4435	*G6L3-9CHK RH Christmas Party	12/03/24	-99469	662.61	12/01/24	11/24	10	57	45000	598
AMAZON CAPITAL SERVICES	CL 4435	*NKNK-3731 Office Supplies	12/03/24	-99469	44.78	12/01/24	11/24	10	0	41810	611
AMAZON CAPITAL SERVICES	CL 4435	*174Q-9V39 Switches/Splitter PMR	12/03/24	-99469	630.99	12/01/24	11/24	10	555	41540	434
AMAZON CAPITAL SERVICES	CL 4435	*TGTR-FTL4 Laminator/Flagpole	12/03/24	-99469	113.47	12/01/24	11/24	10	0	41810	611
AMISI IDI	CL 4395	November Rec Clean Up	11/21/24	23683	120.00	11/20/24	11/24	10	0	44022	352
ANALYTICAL LABORATORIES, INC	CL 4376	2407986 Fish Pond Water Testing	11/18/24	-99487	290.00	10/31/24	10/24	10	571	41550	435
ANNIE PEW	CL 4409	Trunk or Treat Supplies	11/22/24	23696	160.13	09/30/24	11/24	10	0	44022	611
ANNIE PEW	CL 4409	Police VS Fire Supplies	11/22/24	23696	15.90	10/07/24	11/24	10	0	47001	594
ANNIE PEW	CL 4409	HALO Event Supplies	11/22/24	23696	37.26	11/18/24	11/24	10	0	47001	594
ANNIE PEW	CL 4409	BSP Halloween Supplies	11/22/24	23696	114.16	10/30/24	11/24	10	101	44022	611
ANNIE PEW	CL 4409	Insurance Ping Pong Table	11/22/24	23696	64.99	11/09/24	11/24	10	0	44022	611
ANNIE PEW	CL 4409	School Day Out Supplies	11/22/24	23696	76.84	11/05/24	11/24	10	109	44022	611
ANNIE PEW	CL 4409	B/A School Program Supplies	11/22/24	23696	272.45	11/01/24	11/24	10	101	44022	611
ANNIE PEW	CL 4409	HALO Event Supplies	11/22/24	23696	14.17	11/18/24	11/24	10	0	47001	594
ANNIE PEW	CL 4409	B/A School Program Supplies	11/22/24	23696	62.65	11/01/24	11/24	10	101	44022	611
ASSOCIATION OF IDAHO CITIES	CL 4383	200012595 ICCTFOA Membership Dues	11/21/24	-99486	90.00	11/20/24	11/24	10	351	48990	570
BILLS MACHINE SHOP	CL 4436	24-1760 Concrete	12/02/24	-99468	12,500.00	11/05/24	11/24	10	0	45100	747
BILLS MACHINE SHOP	CL 4436	24-1760 Crane for Installation	12/02/24	-99468	3,000.00	11/05/24	11/24	10	0	45100	747
BILLS MACHINE SHOP	CL 4436	24-1760 Bridge work	12/02/24	-99468	21,250.00	11/05/24	11/24	10	0	45100	747
BRANDEE MANDIS	CL 4400	Nov 2024 Homeschool/PE	11/21/24	23684	133.00	11/20/24	11/24	10	157	44022	352
CANON FINANCIAL SERVICES, INC	CL 4364	36201363 Leases for 2 Copiers	11/15/24	-99492	256.24	11/11/24	11/24	10	0	42200	374
CANYON COUNTY CLERK	CL 4438	Nov 2024 Prosecution Services	12/02/24	23709	100.00	12/02/24	11/24	10	0	41300	328
CARTER COMFORT SYSTEMS	CL 4445	144920 Run Capacitor Diagnostic	12/05/24	23710	153.00	11/25/24	11/24	10	334	48900	333
CHRISPE MUMBERE	CL 4390	November Rec Clean Up	11/21/24	23685	120.00	11/20/24	11/24	10	0	44022	352
CLEARWATER FINANCIAL LLC	CL 4370	3197 Retainer Renewal	11/15/24	23669	12,000.00	11/27/24	11/24	10	0	48960	593
CONGER GROUP	CL 4384	Bond Refund for Moon Valley #7	11/21/24	23686	30,585.00	11/19/24	11/24	10	0	40600	881
COSTCO	CL 4424	Annual Membership Renewal	11/26/24	23700	65.00	11/26/24	11/24	10	369	48990	570
CRIME STOPPERS OF SOUTHWEST ID	CL 4408	2241 FY 2025 Service Agreement	11/22/24	23697	2,200.00	11/21/24	11/24	10	0	42110	365
DANA PARTRIDGE	CL 4367	Services 11/01/24 to 11/14/24	11/15/24	-99497	3,034.22	11/15/24	11/24	10	0	41740	565
DANA PARTRIDGE	CL 4415	Services 10/04/24 to 10/17/24	11/25/24	23603	2,803.74	11/18/24	10/24	10	0	41740	565
DANA PARTRIDGE	CL 4425	Services 11/15/24 to 11/28/24	11/26/24	-99474	2,987.95	11/26/24	11/24	10	0	41740	565
DAY MANAGEMENT CORPORATION	CL 4385	INV843871 Iridium Extreme Satellite	11/21/24	23677	150.00	09/30/24	9/24	10	0	41540	442
DENIELLE REDONDO	CL 4403	Nov 2024 Barre Instruction	11/21/24	-99481	273.00	11/20/24	11/24	10	156	44022	352
DENIS SUMAILI	CL 4394	November Rec Clean Up	11/21/24	23687	180.00	11/20/24	11/24	10	0	44022	352
DEX IMAGING	CL 4410	AR12289426 Monthly Copier Lease	11/25/24	-99478	196.98	11/15/24	11/24	10	0	42200	374
ECI CONTRACTORS	CL 4433	Nov 2024 Electrical Inspections	12/02/24	-99471	17,360.44	11/30/24	11/24	10	0	40500	454
FACILITRON	CL 4456	*ND74E8F-2 Facility Rental	12/10/24	23726	1,255.00	11/26/24	12/24	10	0	44021	442
FATBEAM LLC	CL 4454	50432 Fiber Optic Internet Service	12/05/24	-99458	3,987.50	12/01/24	12/24	10	0	41100	419
FATBEAM LLC	CL 4466	49459 Fiber Optic Internet Service	12/06/24	-99451	3,987.50	11/01/24	11/24	10	0	41100	419
FIREWORKS & STAGE FX AMERICA I	CL 4407	22474 Fireworks Display NYE	11/22/24	23698	3,000.00	09/18/24	11/24	10	61	45000	598
FISHER'S TECHNOLOGY	CL 4455	1421386 Copier Rec Center	12/05/24	-99457	29.94	12/02/24	12/24	10	0	42200	374
FISHER'S TECHNOLOGY	CL 4455	1421385 Copier City Hall	12/05/24	-99457	227.14	12/02/24	12/24	10	0	42200	374
GAMEFACE ATHLETICS	CL 4372	320630 Building Department Stickers	11/15/24	-99490	240.00	11/11/24	11/24	10	0	41810	611
GAMEFACE ATHLETICS	CL 4372	320645 Carhartt Watch Caps	11/15/24	-99490	280.00	11/01/24	11/24	10	0	45050	585
GAMEFACE ATHLETICS	CL 4396	320748 Shoreline Jacket	11/21/24	-99484	170.00	11/21/24	11/24	10	0	41590	615
GAMEFACE ATHLETICS	CL 4396	320748 Snapback Truckee Hats	11/21/24	-99484	1,200.00	11/21/24	11/24	10	0	45070	590
GEM STATE PAPER & SUPPLY COMPAC	CL 4358	3097851 Upright Vacuum & Hepa	11/15/24	-99495	705.34	11/13/24	11/24	10	553	41540	434
GERARD RAYMOND	CL 4391	November Rec Clean Up	11/21/24	23688	180.00	11/20/24	11/24	10	0	44022	352
GREYLOCK	CL 4371	6254 DMV License Office End Panel	11/15/24	23670	278.84	11/11/24	11/24	10	719	45110	755
HERITAGE LANDSCAPE SUPPLY GROUCL	4464	*1962-001 PVC Conduit	12/06/24	-99453	56.05	11/25/24	12/24	10	0	45100	752
HERITAGE LANDSCAPE SUPPLY GROUCL	4464	*3294-001 Stub Outs Irrigation	12/06/24	-99453	61.05	11/26/24	12/24	10	0	45100	752
HERITAGE LANDSCAPE SUPPLY GROUCL	4464	*4646-001 PVC Stub Outs	12/06/24	-99453	64.01	11/25/24	12/24	10	0	45100	752
HERITAGE LANDSCAPE SUPPLY GROUCL	4464	*9083-001 Christmas Lights	12/06/24	-99453	56.99	12/03/24	12/24	10	57	45000	598
HORIZON	CL 4368	3L240728 Hedge Trimmers	11/18/24	23671	1,044.79	10/25/24	10/24	10	0	41570	619

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HORIZON	CL	4368	3L240471 Sprinkler Organizer C	11/18/24	23671	62.47	10/09/24	10/24	10	0	41570	619
ICON ELECTRIC	CL	4377	Refund - Cancelled Permit	11/18/24	23689	156.00	11/18/24	11/24	10	0	40700	884
IDAHO DEPARTMENT OF LABOR	CL	4430	0007009720 Unemployment Insurance	11/26/24	23701	5,907.20	11/15/24	11/24	10	0	41740	556
IDAHO POWER COMPANY	CL	4420	0033022572 1310 N Little Camas	11/26/24	-99476	102.00	11/15/24	11/24	10	0	41100	412
IDAHO POWER COMPANY	CL	4420	0033075641 Highbrook/Hwy 44 St	11/26/24	-99476	46.31	11/15/24	11/24	10	0	41100	413
IDAHO POWER COMPANY	CL	4420	0033022587 1500 N Star Rd CT	11/26/24	-99476	627.60	11/15/24	11/24	10	0	41100	412
IDAHO POWER COMPANY	CL	4420	0033022589 10769 W State St CT	11/26/24	-99476	621.61	11/15/24	11/24	10	0	41100	412
IDAHO POWER COMPANY	CL	4420	0030204591 Hunters Creek Street	11/26/24	-99476	14.82	11/15/24	11/24	10	0	41100	413
IDAHO POWER COMPANY	CL	4420	0033022594 1000 S Main St Pump	11/26/24	-99476	75.59	11/15/24	11/24	10	0	41100	412
IDAHO POWER COMPANY	CL	4420	0033022576 11225 W Blake Dr Bl	11/26/24	-99476	116.59	11/15/24	11/24	10	0	41100	412
IDAHO POWER COMPANY	CL	4420	0033022563 10775 W State St St	11/26/24	-99476	122.41	11/15/24	11/24	10	0	41100	412
IDAHO POWER COMPANY	CL	4420	0033022592 1300 N Star Rd HC E	11/26/24	-99476	61.80	11/15/24	11/24	10	0	41100	412
IDAHO POWER COMPANY	CL	4420	0033022554 1250 N Star Rd HC P	11/26/24	-99476	443.21	11/15/24	11/24	10	0	41100	412
IDAHO POWER COMPANY	CL	4420	0032863988 960 S Main Street	11/26/24	-99476	28.54	11/13/24	11/24	10	0	41100	412
IDAHO POWER COMPANY	CL	4420	0030968905 W Picket Cr/Pollard	11/26/24	-99476	11.11	11/15/24	11/24	10	0	41100	413
IDAHO POWER COMPANY	CL	4420	0033367599 W Otter Rock Dr Pum	11/26/24	-99476	501.44	10/30/24	11/24	10	0	41100	412
IDAHO POWER COMPANY	CL	4420	0033022596 Blake Park Sprinkle	11/26/24	-99476	3.66	11/15/24	11/24	10	0	41100	412
IDAHO POWER COMPANY	CL	4420	0033372621 10405 W Otter Rock	11/26/24	-99476	258.20	11/15/24	11/24	10	0	41100	412
IDAHO POWER COMPANY	CL	4420	0030980389 W Otter Rock Dr Out	11/26/24	-99476	26.76	11/13/24	11/24	10	0	41100	412
IDAHO POWER COMPANY	CL	4420	Adjustment - one time set up	11/26/24	-99476	30.00	11/13/24	11/24	10	0	41100	412
IDAHO POWER COMPANY	CL	4420	0082547470 W Otter Rock Dr Out	11/26/24	-99476	571.60	11/13/24	11/24	10	0	41100	412
IDAHO POWER COMPANY	CL	4420	0030943198 11665 W State St 41	11/26/24	-99476	9.00	11/15/24	11/24	10	0	41100	413
IDAHO POWER COMPANY	CL	4420	0030980194 Floating Feather/Pl	11/26/24	-99476	12.06	11/15/24	11/24	10	0	41100	413
IDAHO POWER COMPANY	CL	4420	0033022584 Street Lights Star	11/26/24	-99476	379.47	11/15/24	11/24	10	0	41100	413
IDAHO POWER COMPANY	CL	4420	0030885171 11665 W State St C	11/26/24	-99476	151.03	11/15/24	11/24	10	0	41100	412
IDAHO POWER COMPANY	CL	4420	0033022580 State/Plummer Street	11/26/24	-99476	13.45	11/15/24	11/24	10	0	41100	413
IDAHO POWER COMPANY	CL	4420	0030204606 Street Lights Blake	11/26/24	-99476	17.30	11/15/24	11/24	10	0	41100	413
IDAHO POWER COMPANY	CL	4420	0032863989 960 S Main Street	11/26/24	-99476	449.12	11/13/24	11/24	10	0	41100	412
IDAHO POWER COMPANY	CL	4420	0033207655 11665 W State St A1	11/26/24	-99476	179.38	11/15/24	11/24	10	0	41100	412
IDAHO POWER COMPANY	CL	4420	0033157130 11380 W Hidden Broo	11/26/24	-99476	82.98	11/15/24	11/24	10	0	41100	412
IDAHO POWER COMPANY	CL	4420	0033207654 11665 W State St A	11/26/24	-99476	251.91	11/15/24	11/24	10	0	41100	412
IDAHO PRESS	CL	4361	50444 Legal/Pub Notice PH 11/19/24	11/15/24	-99494	111.99	11/02/24	11/24	10	0	41810	530
IDAHO PRESS	CL	4361	50445 Legal/Pub Notice PH 11/19/24	11/15/24	-99494	86.37	11/02/24	11/24	10	0	41810	530
IDAHO PRESS	CL	4446	51282 Legal/Pub Notice PH 12/17/24	12/05/24	-99462	73.56	11/29/24	11/24	10	0	41810	530
IDAHO PRESS	CL	4446	51281 Legal/Pub Notice PH 12/17/24	12/05/24	-99462	121.14	11/29/24	11/24	10	0	41810	530
IDAHO STATE TAX COMMISSION	CL	4473	*681453-09 Tax/Fee and Penalty	12/06/24	23725	421.88	11/29/24	12/24	10	0	41740	557
IDAHO TRACTOR INC	CL	4469	3462 Equipment Service/Maintenance	12/06/24	23720	434.57	12/04/24	12/24	10	0	41580	433
INSPECT LLC	CL	4432	Nov 2024 Plumbing Inspections	12/02/24	-99472	14,603.54	11/30/24	11/24	10	0	40500	453
INTERMOUNTAIN GAS COMPANY	CL	4375	*7251 2 Star Police Station	11/18/24	23690	59.46	10/31/24	11/24	10	0	41100	414
INTERMOUNTAIN GAS COMPANY	CL	4375	*1000 1 Outreach Building	11/18/24	23690	66.90	10/31/24	11/24	10	0	41100	414
INTERMOUNTAIN GAS COMPANY	CL	4375	*3000 2 B&G Shop-1310 N Little	11/18/24	23690	64.61	10/31/24	11/24	10	0	41100	414
INTERMOUNTAIN GAS COMPANY	CL	4375	*3000 1 City Hall	11/18/24	23690	25.16	10/31/24	11/24	10	0	41100	414
J-U-B ENGINEERS	CL	4458	0179410 Freedom Park Annex & Lot Consol	12/05/24	23711	5,720.00	11/26/24	11/24	10	0	45100	747
J-U-B ENGINEERS	CL	4458	0179409 BLM Prop Annex & ROS	12/05/24	23711	10,010.00	11/26/24	11/24	10	0	45100	754
JEAN HENSCHIED	CL	4399	Nov 2024 Intro Yoga Instruction	11/21/24	23691	224.00	11/20/24	11/24	10	153	44022	352
JEAN WOLFORD PHOTOGRAPHY	CL	4366	102193 Trunk or Treat Photography	11/15/24	23672	500.00	11/02/24	11/24	10	55	45000	598
JULEE ELLIOTT	CL	4460	2264 Christmas Light Pole Banners	12/06/24	-99456	1,000.00	12/02/24	12/24	10	0	45070	586
KEELY ELECTRIC	CL	4461	807 Outlet Installation 946 S Main	12/06/24	-99455	5,974.18	11/25/24	12/24	10	0	45100	752
KELLER ASSOCIATES INC	CL	4457	0243520 City Hall Generator Analysis	12/05/24	23712	300.00	11/15/24	11/24	10	0	45210	339
L&W SUPPLY CORPORATION	CL	4447	*60972-001 C/T Office Remodel	12/05/24	23713	504.22	11/15/24	11/24	10	720	45110	755
LARA YOUNGMAN	CL	4402	Nov 2024 Mat Pilates Instruction	11/21/24	-99482	280.00	11/20/24	11/24	10	151	44022	352
LARRY BEARG	CL	4404	Nov 2024 Tai Chi Instruction	11/21/24	-99480	448.00	11/20/24	11/24	10	152	44022	352
LIFESPRIING CHURCH	CL	4369	1008 2024 Veterans Potato Feed	11/15/24	23673	140.00	11/08/24	11/24	10	56	45000	598
LOWES	CL	4411	Veteran's Flag Pole	11/25/24	23675	1,590.00	/ /	11/24	10	130	45070	590
MIDDLETON IRRIGATION ASSOCIATI	CL	4418	8.5 Shares Assessment - Pristine Meadows	11/26/24	23702	333.00	11/15/24	11/24	10	0	41100	417
MIDDLETON IRRIGATION ASSOCIATI	CL	4418	3 Shares Assessment - West Point Park	11/26/24	23702	124.00	11/15/24	11/24	10	0	41100	417
MIDDLETON IRRIGATION ASSOCIATI	CL	4418	4 Shares Assessment - Blake Park	11/26/24	23702	162.00	11/15/24	11/24	10	0	41100	417
MIDDLETON MILL DITCH CO	CL	4417	29.68 shar Assessment - Hunter Creek	11/26/24	23703	1,137.84	11/15/24	11/24	10	0	41100	417
MIDDLETON MILL DITCH CO	CL	4417	7.00 share Assmt - 11665 S & 22585 K	11/26/24	23703	276.00	11/15/24	11/24	10	0	41100	417
MIDDLETON PARKS & RECREATION I	CL	4397	Axe Bowl Medal	11/21/24	23680	130.00	10/30/24	10/24	10	0	44021	531
MOUNT OLYMPUS WATER	CL	4365	24335904 Water Rec Center	11/15/24	-99491	1.00	11/01/24	11/24	10	0	41810	611
MOUNT OLYMPUS WATER	CL	4365	24331811 Water City Hall	11/15/24	-99491	64.50	11/01/24	11/24	10	0	41810	611
MOUNTAIN ALARM	CL	4374	5379411 Security Backup/Monitor	11/15/24	-99488	42.50	11/01/24	11/24	10	327	48900	333
MOUNTAIN ALARM	CL	4374	5379412 Fire Alarm/Monitoring	11/15/24	-99488	92.50	11/01/24	11/24	10	327	48900	333
MOUNTAIN VIEW EQUIPMENT COMPAN	CL	4427	003166 GRA PT 672 ZT Lawnmower	11/26/24	23704	15,328.11	11/20/24	11/24	10	0	45100	747
NAPA AUTO PARTS	CL	4353	114767 Antifreeze	11/15/24	23674	21.56	11/05/24	11/24	10	554	41540	434
NAPA AUTO PARTS	CL	4360	114974 Oil/Filter Change Ford	11/15/24	23674	60.75	11/09/24	11/24	10	0	41580	449
NAPA AUTO PARTS	CL	4419	115572 Dielect Grease Christmas	11/26/24	23705	87.96	11/20/24	11/24	10	57	45000	598
NAPA AUTO PARTS	CL	4472	115836 Dielectric Grease	12/06/24	23721	21.99	11/26/24	12/24	10	0	41580	447
NAPA AUTO PARTS	CL	4472	116152 Dielectric Grease	12/06/24	23721	65.97	12/03/24	12/24	10	0	41580	447
NIKI DEAN	CL	4398	Nov 2024 Gentle Yoga	11/21/24	23692	952.00	11/20/24	11/24	10	153	44022	352
NIKI DEAN	CL	4398	Nov 2024 Restorative Yoga	11/21/24	23692	157.50	11/20/24	11/24	10	153	44022	352
OFFICE SAVERS ONLINE	CL	4439	11461 Copy Paper/Folders	12/03/24	-99466	168.97	11/19/24	11/24	10	0	41810	611
PARFAIT NIYONKURU	CL	4392	November Rec Clean Up	11/21/24	23693	180.00	11/20/24	11/24	10	0	44022	352

PORTAPROS LLC	CL	4357	128708M-1 960 Main Portable Restroom	11/15/24	-99496	488.80	10/03/24	10/24	10	567	41550	435
PORTAPROS LLC	CL	4444	133217E-1 960 S Main Portable	12/05/24	-99463	1,335.20	12/03/24	12/24	10	567	41550	435
PRISTINE MEADOWS	CL	4405	9239 Replace Pump Motor Pavilion	11/21/24	23678	7,724.46	09/11/24	9/24	10	0	45110	738
QUALITY TRAILER SALES	CL	4388	79662 Trailer Hitch	11/21/24	-99485	405.00	10/01/24	11/24	10	0	41570	613
REPUBLIC SERVICES INC	CL	4422	001380026 Hunters Creek Park	11/26/24	-99475	365.03	10/31/24	11/24	10	0	41100	411
REPUBLIC SERVICES INC	CL	4422	001380083 Star River Access 10	11/26/24	-99475	89.39	10/31/24	11/24	10	0	41100	411
REPUBLIC SERVICES INC	CL	4422	001380365 Star City Hall	11/26/24	-99475	220.83	10/31/24	11/24	10	0	41100	411
REPUBLIC SERVICES INC	CL	4422	001380386 Blake Haven Park	11/26/24	-99475	94.59	10/31/24	11/24	10	0	41100	411
RIMI INC	CL	4431	Nov 2024 Mechanical Inspection	12/02/24	-99473	15,282.77	11/30/24	11/24	10	0	40500	455
ROBERT LITTLE	CL	4406	Balance from October	11/22/24	23699	354.16	11/22/24	11/24	10	0	41740	565
ROBERT LITTLE	CL	4406	Life Insur Life Insurance	11/22/24	23699	-285.38	11/22/24	11/24	10	0	41740	565
ROBERT LITTLE	CL	4406	116200 B&G Contracted Services	11/22/24	23699	5,250.00	11/22/24	11/24	10	0	41740	565
ROBERT LITTLE	CL	4414	116199 B&G Contracted Services	11/25/24	23602	4,895.84	10/18/24	10/24	10	0	41740	565
ROBERT LITTLE	CL	4414	Life Insur Life Insurance	11/25/24	23602	-285.38	10/18/24	10/24	10	0	41740	565
ROCKY MOUNTAIN ROLL	CL	4452	37317319 Stage for Christmas Eve	12/05/24	23714	1,035.49	12/03/24	12/24	10	57	45000	598
ROOTS ROCK & BARK YARD	CL	4467	4946 Top Soil 946 S Main Prkng	12/06/24	23722	463.40	10/18/24	12/24	10	0	45100	752
ROYALTY ELECTRIC LLC	CL	4443	24001-12 Phone Service	12/05/24	-99464	1,080.00	12/03/24	12/24	10	0	41100	416
SHERWIN WILLIAMS	CL	4373	0393-0 Paint - Clerks Office	11/15/24	-99489	56.19	11/07/24	11/24	10	720	45110	755
SHERWIN WILLIAMS	CL	4373	3812-3 Paint - Clerks Office	11/15/24	-99489	112.38	11/07/24	11/24	10	720	45110	755
SHERWIN WILLIAMS	CL	4373	3612-7 Paint - Building Maintenance	11/15/24	-99489	410.87	11/01/24	11/24	10	555	41540	434
SHERWIN WILLIAMS	CL	4380	4022-8 Paint - Clerks Office Remodel	11/26/24	-99479	199.25	11/12/24	11/24	10	720	45110	755
SHERWIN WILLIAMS	CL	4380	9261-8 Paint - Clerks Office Remodel	11/26/24	-99479	73.80	11/14/24	11/24	10	720	45110	755
SHERWIN WILLIAMS	CL	4380	9303-8 Paint - Clerks Office Remodel	11/26/24	-99479	17.61	11/15/24	11/24	10	720	45110	755
SKYHAWKS TREASURE VALLEY	CL	4440	Summer Camp Discounts	12/03/24	23715	1,500.00	11/20/24	9/24	10	104	44022	300
STAR FIRE DEPARTMENT	CL	4442	Nov 2024 Star Fire Impact Fees	12/03/24	23716	175,701.52	12/01/24	11/24	10	0	40000	734
STAR MERCANTILE INC	CL	4378	02-47506 Clamps for Christmas	12/06/24	23723	14.43	11/21/24	11/24	10	57	45000	598
STAR MERCANTILE INC	CL	4378	02-47506 Water	12/06/24	23723	241.99	11/21/24	11/24	10	0	41810	611
STAR MERCANTILE INC	CL	4378	02-47734 Clamps for Christmas	12/06/24	23723	17.89	11/21/24	11/24	10	57	45000	598
STAR MERCANTILE INC	CL	4378	02-32707 Hardware	12/06/24	23723	4.99	11/12/24	11/24	10	0	41580	447
STAR MERCANTILE INC	CL	4378	02-26829 Hand Tools	12/06/24	23723	10.98	11/08/24	11/24	10	0	41570	613
STAR MERCANTILE INC	CL	4378	02-36004 Hardware	12/06/24	23723	4.59	11/14/24	11/24	10	555	41540	434
STAR MERCANTILE INC	CL	4378	03-15990 Screws	12/06/24	23723	27.81	11/14/24	11/24	10	0	41580	447
STAR MERCANTILE INC	CL	4378	02-14774 Hardware	12/06/24	23723	26.99	11/01/24	11/24	10	0	41580	447
STAR MERCANTILE INC	CL	4378	03-25915 Hardware Pavilion Shed	12/06/24	23723	14.85	11/27/24	11/24	10	0	45100	738
STAR MERCANTILE INC	CL	4378	02-44794 Cable/Nuts	12/06/24	23723	68.78	11/19/24	11/24	10	0	41580	447
STAR MERCANTILE INC	CL	4378	02-44282 Water	12/06/24	23723	7.00	11/19/24	11/24	10	0	41810	611
STAR MERCANTILE INC	CL	4378	03-20480 Cable for Christmas Tree	12/06/24	23723	19.60	11/20/24	11/24	10	57	45000	598
STAR MERCANTILE INC	CL	4378	02-16238 Wall Sanders	12/06/24	23723	11.68	11/02/24	11/24	10	0	41570	613
STAR MERCANTILE INC	CL	4378	02-46321 Screws for Christmas	12/06/24	23723	9.90	11/20/24	11/24	10	57	45000	598
STAR MERCANTILE INC	CL	4378	02-57525 Drill Bit Pavilion Shed	12/06/24	23723	8.99	11/27/24	11/24	10	0	45100	738
STAR MERCANTILE INC	CL	4378	02-57486 Duct Tape Pavilion Shed	12/06/24	23723	4.99	11/27/24	11/24	10	0	45100	738
STAR MERCANTILE INC	CL	4378	02-37768 Hardware	12/06/24	23723	6.76	11/15/24	11/24	10	0	41580	447
STAR MERCANTILE INC	CL	4378	02-49198 Staples for Christmas	12/06/24	23723	3.99	11/22/24	11/24	10	57	45000	598
STAR MERCANTILE INC	CL	4378	03-10076 Hardware	12/06/24	23723	20.71	11/06/24	11/24	10	0	41580	447
STAR MERCANTILE INC	CL	4378	03-10003 Saw Blades	12/06/24	23723	70.57	11/06/24	11/24	10	0	41570	613
STAR MERCANTILE INC	CL	4378	02-28658 Hardware	12/06/24	23723	12.87	11/09/24	11/24	10	0	41580	447
STAR MERCANTILE INC	CL	4378	02-16244 Paint Remover	12/06/24	23723	14.99	11/02/24	11/24	10	0	41580	447
STAR MERCANTILE INC	CL	4378	02-16770 Wall Sanders/Paper	12/06/24	23723	6.38	11/02/24	11/24	10	555	41540	434
STAR MERCANTILE INC	CL	4378	02-16268 Hose Clamps Christmas	12/06/24	23723	8.98	11/02/24	11/24	10	57	45000	598
STAR STORAGE	CL	4421	Unit C04	11/26/24	23706	175.00	11/26/24	11/24	10	549	41540	434
STAR STORAGE	CL	4421	Unit C08	11/26/24	23706	155.00	11/26/24	11/24	10	549	41540	434
STAR STORAGE	CL	4421	Unit C06	11/26/24	23706	175.00	11/26/24	11/24	10	549	41540	434
STATE INSURANCE FUND	CL	4448	POL 562308 Audit Premium Adjustment	12/05/24	23717	23,003.00	11/15/24	11/24	10	0	41740	555
STEPHANIE COOPER	CL	4387	Refund Riverhouse Deposit	11/21/24	23694	500.00	11/15/24	11/24	10	0	40700	882
STEPHANIE COOPER	CL	4387	Refund Riverhouse Rental	11/21/24	23694	400.00	11/15/24	11/24	10	0	40700	883
TAYLOR HAMMREICH	CL	4401	Nov 2024 Tots Instruction	11/21/24	-99483	25.90	11/20/24	11/24	10	111	44022	352
TAYLOR HAMMREICH	CL	4401	Nov 2024 Tumbling Instruction	11/21/24	-99483	2,000.60	11/20/24	11/24	10	159	44022	352
THOMAS LUMONA	CL	4393	November Rec Clean Up	11/21/24	23695	180.00	11/20/24	11/24	10	0	44022	352
TOLL BROTHERS	CL	4459	Refund Cancld Permit Dbl Pymt	12/05/24	23718	11,887.39	12/05/24	12/24	10	0	40700	884
TREASURE VALLEY COFFEE	CL	4412	10791580 Coffee/Tea/HC B&G	11/25/24	-99477	178.50	11/22/24	11/24	10	0	41810	611
ULINE	CL	4441	185703504 Door Stoppers	12/03/24	-99465	90.00	11/15/24	11/24	10	720	45110	755
ULINE	CL	4441	185703504 Electrical Tape/S&H	12/03/24	-99465	138.28	11/15/24	11/24	10	0	41580	447
ULINE	CL	4441	185703504 Cable/Ties/Tape	12/03/24	-99465	597.00	11/15/24	11/24	10	57	45000	598
VALLEY WIDE COOP	CL	4462	A85245 Fuel	12/06/24	-99454	1,143.13	11/13/24	11/24	10	0	41560	626
VERIZON WIRELESS	CL	4423	9977710404 Monthly Cell Phone	11/26/24	23707	1,031.78	11/01/24	11/24	10	0	41100	373
WESTERN RECORDS DESTRUCTION	CL	4451	0726812 02-64 Gallon City Hall	12/05/24	-99460	62.00	12/01/24	12/24	10	0	41100	411
WESTERN STATES EQUIPMENT CO	CL	4465	003012765 Telehandler Christmas	12/06/24	-99452	2,265.00	12/02/24	12/24	10	57	45000	598
WHITE PETERSON	CL	4382	165712 Keith Hill-First Church	11/19/24	23681	1,797.00	10/31/24	10/24	10	0	41300	323
WILLIAM WRIGHT	CL	4426	Riverhouse Deposit Refund	11/26/24	23708	500.00	11/26/24	11/24	10	0	40700	882
YORGASON LAW OFFICES PLLC	CL	4437	302 City Attorney Monthly Service	12/02/24	-99467	3,500.00	12/02/24	11/24	10	0	41300	322
YORGASON LAW OFFICES PLLC	CL	4437	302 City Attorney Additional Service	12/02/24	-99467	380.00	12/02/24	11/24	10	0	41300	322
YOUNG ELEVATOR INC	CL	4453	1332 Routine Service/Maintenance	12/05/24	-99459	150.00	11/26/24	12/24	10	331	48900	333
ZD FENCING	CL	4470	324 Temporary Fence Rental	12/06/24	23724	5,144.80	12/06/24	12/24	10	0	41570	442

**CITY COUNCIL REGULAR MEETING NOTES**

City Hall - 10769 W State Street, Star, Idaho
Tuesday, November 19, 2024, at 7:00 PM

1. CALL TO ORDER – Welcome/Pledge of Allegiance

Mayor Chadwick called the meeting to order at 7 p.m. and led the Pledge of Allegiance.

2. INVOCATION – Pastor David Ax, Star Calvary Chapel

Pastor Ax led the invocation.

3. ROLL CALL

ELECTED OFFICIALS: Mayor Trevor Chadwick, Council President David Hershey and Council Members Kevan Wheelock, Jennifer Salmonsens and Kevin Nielsen.

STAFF: City Clerk Shelly Tilton; City Planning Administrator Shawn Nickel; Assistant City Planner / Code Officer Ryan Field; Public Information Officer Dana Partridge; City Engineer Ryan Morgan; Star Police Chief Zach Hessing and Star Fire Chief Greg Timinsky.

4. PRESENTATIONS**A. Star Police Chief Monthly Report**

Star Police Chief Zach Hessing made his monthly report. He stated their radios are now encrypted, people can no longer use the scanner apps to hear what they are doing and where they are going. This resolves the safety issues they have had. They are watching to ensure communication is working well; there is a learning curve for officers and dispatchers. Chief Hessing reviewed call volume and offenses for October by type. He pointed out school checks stating they are the highest they have had in the past several months; they visit Star Elementary and Middle Schools. He reviewed staffing, arrests, citations and Code 3 calls. There is a goal of 4-minute response time for Code 3 calls, re-education is being done to ensure officers are being marked when they arrive on-scene. Councilmember Wheelock asked what school checks involve. Chief Hessing said that officers will walk the halls and are often asked by teachers to introduce themselves and talk with students in their classrooms.

B. Star Fire Chief Monthly Report

Star Fire Chief Greg Timinsky made his monthly report. He reviewed incidents/calls by type for the last 30 days, stating calls numbers are running around average. He said the levy election had a large turnout but was unsuccessful. They will continue to get the word out with monthly information meetings at the Star and Middleton stations to ensure voters are receiving accurate information.

5. CONSENT AGENDA (ACTION ITEM) **All matters listed within the Consent Agenda have been distributed to each member of the Star City Council for reading and study, they are considered to be routine and will be enacted by one motion of the Consent Agenda or placed on the Regular Agenda by request.***A. Approval of Claims**

B. Approval of Minutes: May 14, 2024

C. Approval of Minutes: June 4, 2024

**CITY COUNCIL REGULAR MEETING NOTES**

City Hall - 10769 W State Street, Star, Idaho
Tuesday, November 19, 2024, at 7:00 PM

- D. **Approval of Minutes:** June 11, 2024
 - E. **Approval of Minutes:** June 18, 2024
 - F. **Approval of Minutes:** July 2, 2024
 - G. **Approval of Minutes:** July 16, 2024
 - H. **Approval of Minutes:** August 6, 2024
 - I. **Approval of Minutes:** August 20, 2024
 - J. **Approval of Minutes:** September 3, 2024
 - K. **Approval of Minutes:** October 15, 2024
 - L. **Fountain Park Subdivision Preliminary Plat Time Extension (TE-24-09)**
 - M. **Addington Subdivision Preliminary Plat Modification Findings of Fact/Conclusions of Law (PP-22-02 MOD)**
 - N. **Dowdy's Automotive Conditional Use Permit Findings of Fact/Conclusions of Law (CU-24-06)**
 - O. **Oliver Estates Subdivision Final Plat Phase 2 (FP-24-11)**
 - P. **Springtree Estates Subdivision Final Plat (FP-24-13)**
 - Q. **Grace Lane Right of Way Vacation (Recommendation to Highway District #4)**
- Council Member Salmonsens moved to approve the Consent Agenda item 5A. the Approval of Claims, item 5B. the Approval of Minutes for May 14, 2024, 5C. Minutes for June 4, 2024, 5D. Minutes for June 11, 2024, 5E. Minutes for June 18, 2024, 5F. Minutes for July 2, 2024, 5G. Minutes for July 16, 2024, 5H. Minutes for August 6, 2024, 5I. Minutes for August 20, 2024, 5J. Minutes for September 3, 2024, 5K. Minutes for October 15, 2024, item 5L. Fountain Park Subdivision Preliminary Plat Time Extension, item 5M. Addington Subdivision Preliminary Plat Modification Findings of Fact/Conclusions of Law, item 5N. Dowdy's Automotive Conditional Use Permit Findings of Fact/Conclusions of Law, item 5O. Oliver Estates Subdivision Final Plat Phase 2, item 5P. Springtree Estates Subdivision Final Plat and 5Q. Grace Lane Right of Way Vacation. Councilman Nielsen seconded the motion. ROLL CALL VOTE: Hershey – aye; Wheelock – aye; Salmonsens – aye; Nielsen – aye. The motion carried.

6. ACTION ITEMS:

There were no Action Items.

- 7. PUBLIC HEARINGS with ACTION ITEMS:** (The Council at its option may suspend the rules requiring three separate readings on three separate days for ordinances on the agenda for approval. This may be by a single motion to suspend the rules under Idaho Code 50-902; second of the motion; ROLL CALL VOTE; Title of the Ordinance is read aloud; motion to approve; second of the motion: ROLL CALL VOTE.)

- A. **PUBLIC HEARING – Legado Subdivision Preliminary Plat Modification/Private Street (PP-22-15 MOD/PR-24-05)** The applicant is requesting approval of a Preliminary Plat Modification and Private Street with gates for a proposed residential subdivision consisting of 71 single-family residential lots and 12 common lots. The property is located on the west side of Bent Lane in Star, Canyon County, Idaho and

**CITY COUNCIL REGULAR MEETING NOTES**

City Hall - 10769 W State Street, Star, Idaho
Tuesday, November 19, 2024, at 7:00 PM

consists of 23.77 acres with a proposed density of 2.99 dwelling units per acre. The property is currently zoned Residential R-5. The original preliminary plat was previously approved by City Council on March 23, 2023. **(ACTION ITEM)**

Mayor Chadwick opened the public hearing at 7:14 p.m. The property was not properly posted so the hearing will be tabled to December 17, 2024, to allow for site posting.

- Council Member Hershey moved to table the Legado Preliminary Plat Subdivision application to the December 17th meeting. Councilman Wheelock seconded the motion. ROLL CALL VOTE: Hershey – aye; Wheelock – aye; Salmonsens – aye; Nielsen – aye. The motion carried.

8. ADJOURNMENT

Mayor Chadwick adjourned the meeting at 7:15 p.m.

DRAFT



CITY OF STAR
LAND USE STAFF REPORT

TO: Mayor & Council

FROM: Star Planning & Zoning Department *Shen T. Smith*

MEETING DATE: December 17, 2024 – PUBLIC HEARING (Tabled from November 19, 2024)

FILE(S) #: PP-22-05 MOD Preliminary Plat Modification for Star River Ranch North Subdivision – aka Legado Subdivision.
PR-24-05 – Private Road Application

OWNER/APPLICANT/REPRESENTATIVE

Property Owner/Applicant:
Bent Lane, LLC
6152 West Half Moon Lane
Eagle, Idaho 83616

Representative:
Stephanie Hopkins/Connor Lindstrom
KM Engineering, LLP
5725 North Discovery Way
Boise, Idaho 83713

REQUEST

Request: The Applicant is seeking approval of a Preliminary Plat Modification and Private Street with gates for a proposed residential subdivision consisting of 71 single-family residential lots and 12 common lots. The property is located on the west side of Bent Lane in Star, Canyon County, Idaho and consists of 23.77 acres with a proposed density of 2.99 dwelling units per acre. The property is currently zoned Residential R-5. The original preliminary plat was approved by City Council on March 23, 2023.

PROPERTY INFORMATION

Property Location: The subject property is generally located south of Hwy 44 and west of Bent Lane in Star, Idaho. Canyon County Parcel Number R3403500000.

Existing Site Characteristics: The property is now currently vacant land/pasture.
Irrigation/Drainage District(s): - Canyon County Water Company, LTD.
P.O. Box 11
Star, ID 836669

Flood Zone: This property is located in flood hazard Zone AE.
FEMA FIRM Panel: 16027C0259G
FIRM Effective Date: 6/07/2019
Base Flood Elevation: AE 2541.0 feet

Special On-Site Features:

- ✪ Areas of Critical Environmental Concern – None identified.
- ✪ Evidence of Erosion – No known areas.
- ✪ Fish Habitat – No known areas.
- ✪ Mature Trees – None.
- ✪ Riparian Vegetation – No.
- ✪ Steep Slopes – None.
- ✪ Stream/Creek – None.
- ✪ Unique Animal Life – No unique animal life has been identified.
- ✪ Unique Plant Life – No unique plant life has been identified.
- ✪ Unstable Soils – No known issues.
- ✪ Wildlife Habitat – No wildlife habitat has been developed or will be destroyed.
- ✪ Historical Assets – No historical assets have been observed.

APPLICATION REQUIREMENTS

Pre-Application Meeting Held	January 23, 2024
Neighborhood Meeting Held	February 15, 2024
Application Submitted & Fees Paid	August 16, 2024
Application Accepted	August 16, 2024
Residents within 300' Notified	October 30, 2024
Legal Notice Published	November 2, 2024
Property Posted	December 3, 2024

HISTORY

- **May 1, 2007** – Council approved an application for Hidden Meadows Subdivision Annexation and Zoning with Preliminary Plat to R-5. The development consisted of 108 residential lots and 24 common lots on approximately 24 acres.
- **July 19, 2022** – Council tabled application for Preliminary Plat (PP-22-05) for Star River Ranch North Subdivision to September 6, 2022.
- **September 6, 2022** – Council tabled application for Preliminary Plat (PP-22-05) for Star River Ranch North Subdivision to March 7, 2023.

- **March 7, 2023** – Council tabled application for Preliminary Plat (PP-22-05) for Star River Ranch North Subdivision to March 21, 2023.
- **March 21, 2023** – Council approved application for Preliminary Plat/PUD (PP-22-05) for Star River Ranch North Subdivision. The preliminary plat was approved for a maximum of 71 single family residential lots with 12 common lots on 23.77 acres.

SURROUNDING ZONING/COMPREHENSIVE PLAN MAP/LAND USE DESIGNATIONS

	Zoning Designation	Comp Plan Designation	Land Use
Existing	Residential (R-5)	Estate Urban Residential	Vacant
Proposed	Residential (R-5)	Estate Urban Residential	Single Family Residential
North of site	Mixed-Use (MU) AG (Canyon Co)	Estate Urban Residential	Vacant/Agricultural Single family residential
South of site	Residential (R-2)	Estate Urban Residential	Star River Ranch Common Lot/ vacant/agricultural/Single Family Residential
East of site	AG (Canyon Co)	Estate Urban Residential	Vacant/Agricultural
West of site	Mixed Use (MU)	Estate Urban Residential	Old East Canyon Development Vacant/Agricultural

CODE DEFINITIONS / COMPREHENSIVE PLAN

UNIFIED DEVELOPMENT CODE (As of Submittal of Application – Prior to UDC Update):

8-3A-1: ZONING DISTRICTS AND PURPOSE ESTABLISHED:

R RESIDENTIAL DISTRICT: To provide regulations and districts for various residential neighborhoods. Gross density in a Residential (R) district shall be determined according to the numeral following the R. The numeral designates the maximum number of dwelling units per acre. In zoning designations of R-1, R-2, R-3, R-4 and R-5, housing shall be single family detached unless approved with a PUD or development agreement. Connection to municipal water and sewer facilities are required for all subdivision and lot split applications submitted after the effective date hereof in all districts exceeding one dwelling unit per acre. Wells and septic systems may be permitted for larger lots in this land use designation that are not adjacent to municipal services, as determined by the Sewer District, and if approved by the applicable Health Department. Private streets may be approved in this district for access to newly

subdivided or split property. This district does allow for some non-residential uses as specified in 8-3A-3.

(DA) DEVELOPMENT AGREEMENT: This designation, following any zoning designation noted on the official zoning map of the city (i.e., C-2-DA), indicates that the zoning was approved by the city with a development agreement, with specific conditions of zoning.

8-3A-3: USES WITHIN ZONING DISTRICTS

<u>ZONING DISTRICT USES</u>	A	R-R	R
Accessory structure	A	A	A
Dwelling:			
Multi-family 1	N	N	C
Secondary 1	A	A	A
Single-family attached	N	N	C
Single-family detached	P	P	P
Two-family duplex	N	N	P

The following table lists principal permitted (P), accessory uses (A), conditional (C), or prohibited (N) uses.

8-3A-4: ZONING DISTRICT DIMENSIONAL STANDARDS (As of Submittal of Application – Prior to UDC Update):

Zoning District	Maximum Height Note Conditions	Minimum Yard Setbacks Note Conditions			
		Front (1)	Rear	Interior Side	Street Side
R-5	35'	15' to living area/side load garage 20' to garage face	15'	5'	20'

8-3A-4: ZONING DISTRICT DIMENSIONAL STANDARDS (Current UDC Standards as of 6-7-22):

Zoning District	Maximum Height Note Conditions	Minimum Yard Setbacks Note Conditions			
		Front (1)	Rear	Interior Side	Street Side
R-3	35'	15' to living area/side load garage	15'	7.5' (2)	20'
R-5		20' to garage face			

Notes:

1. Front yard setback shall be measured from the face of the garage to the face of the sidewalk, allowing for 20' of parking on the driveway without overhang onto the sidewalk.
2. Zero-Lot-Line and reduced front and rear setback waivers may be requested through the Development Agreement process. All other side yard setback requests for detached structures shall not be granted waivers, unless as part of a Planned Unit Development.
3. All setbacks in the CBD, C-1, C-2, LO, IL, PS, RC and M-U zone shall maintain a minimum 15' when adjacent to a residential use or zone.
4. As approved by the Fire District.

8-4D-3: STANDARDS (PRIVATE STREETS):

All private streets shall be designed and constructed to the following standards:

A. Design Standards:

1. Easement: The private street shall be constructed on a perpetual ingress/egress easement or a single platted lot (with access easement) that provides access to all applicable properties.
2. Connection Point: Where the point of connection of the private street is to a public street, the private street shall be approved by the transportation authority.
3. Emergency Vehicle: The private street shall provide sufficient maneuvering area for emergency vehicles as determined and approved by the Star Fire District.
4. Gates: Gates or other obstacles shall not be allowed, unless approved by Council through a Planned Unit Development or Development Agreement.

B. Construction Standards:

1. Obtain approval from the county street naming committee for a private street name(s);
2. Contact the transportation authority to install an approved street name sign that complies with the regulations of the county street naming ordinance;
3. Roadway and Storm Drainage: The private street shall be constructed in accord with the roadway and storm drainage standards of the transportation authority or as approved by the city of Star based on plans submitted by a certified engineer.
4. Street Width: The private street shall be constructed within the easement and shall have a travel lane that meets ACHD width standards for the City of Star, or as determined by the Council and Star Fire District.
5. Sidewalks: A five foot (5') attached or detached sidewalk shall be provided on one side of the street in commercial districts. This requirement may be waived if the applicant can demonstrate that an alternative pedestrian path exists.
6. Fire Lanes: All drive aisles as determined by the Star Fire District to be fire lanes, shall be posted as fire lanes with no parking allowed. In addition, if a curb exists next to the drive aisle, it shall be painted red.
7. No building permit shall be issued for any structure using a private street for access to a public street until the private street has been approved.

C. The applicant or owner shall establish an on-going maintenance fund through the Owner's association with annual maintenance dues to ensure that funds are available for future repair and maintenance of all private streets. This shall be a requirement in a development agreement

and/or as part of a planned unit development. A reserve account condition shall be included in the recorded CC&R's and shall be provided to the City for review. The condition of approval shall include the following:

1. Private Road Reserve Study Requirements.
 - a. At least once every three years, the board shall cause to be conducted a reasonably competent and diligent visual inspection of the private road components that the association is obligated to repair, replace, restore, or maintain as part of a study of the reserve account requirements of the common interest development, if the current replacement value of the major components is equal to or greater than one-half of the gross budget of the association, excluding the association's reserve account for that period. The board shall review this study, or cause it to be reviewed, annually and shall consider and implement necessary adjustments to the board's analysis of the reserve account requirements as a result of that review.
 - b. The study required by this section shall at a minimum include:
 - i. Identification of the private road components that the association is obligated to repair, replace, restore, or maintain.
 - ii. Identification of the probable remaining useful life of the components identified in paragraph (1) as of the date of the study.
 - iii. An estimate of the cost of repair, replacement, restoration, or maintenance of the components identified in paragraph (1).
 - iv. An estimate of the total annual contribution necessary to defray the cost to repair, replace, restore, or maintain the components identified in paragraph (1) during and at the end of their useful life, after subtracting total reserve funds as of the date of the study.
 - v. A reserve funding plan that indicates how the association plans to fund the contribution identified in paragraph (4) to meet the association's obligation for the repair and replacement of all private road components.
 - c. A copy of all studies and updates shall be provided to the City, to be included in the development application record.

8-4D-4: REQUIRED FINDINGS (PRIVATE STREETS):

In order to approve the application, the administrator and/or Council shall find the following:

- A. The design of the private street meets the requirements of this article;
- B. Granting approval of the private street would not cause damage, hazard, or nuisance, or other detriment to persons, property, or uses in the vicinity; and
- C. The use and location of the private street shall not conflict with the comprehensive plan and/or the regional transportation plan.

8-4E-2: STANDARDS FOR COMMON OPEN SPACE AND SITE AMENITY REQUIREMENTS:

A. Open Space and Site Amenity Requirement (see also Chapter 8 “Architectural Review”):

- 1. The total land area of all common open space shall equal or exceed fifteen percent (15%) of the total gross acreage of land area of the development. A minimum of 10% of the total gross acreage of the development shall be for useable area open space. Open space shall be designated as a total of 15% minimum for residential developments in all zones with densities of R-2 or greater.
- 2. Each development is required to have at least one site amenity.
- 3. One additional site amenity shall be required for each additional twenty (20) acres of development area, plus one additional amenity per 75 residential units.
- 4. Developments with a density of 1 dwelling unit per acre or less may request a waiver of open space and amenities to the Council. Developments with a density of 2 dwelling units per acre or less may request a 50% reduction in total required open space and amenities to the Council.
- 5. For multi-family developments, see Section 8-5-20 for additional standards.

B. Qualified Usable Area Open Space: The following qualifies to meet the useable area open space requirements:

- 1. Any open space that is active or passive in its intended use, and accessible or visible by all residents of the development, including, but not limited to:
 - a. Open grassy area of at least fifty feet by one hundred feet (50' x 100') in area;
 - b. Qualified natural areas, as determined by the Administrator;
 - c. Ponds or water features where active fishing, paddle boarding, or other activities are provided (50% qualifies towards total required usable area open space, must be accessible by all residents to qualify. Ponds must be aerated. All ponds shall be provided with safety floatation devices (rings) located at reasonable distances, as determined by the administrator;
 - d. A plaza.
 - e. Common lots that include a pathway providing local or regional connectivity that is a minimum of 20' in width.
 - f. Irrigation easements/ditches when a pathway is included (to be

measured from the center of the ditch to the property line of the common lot).

2. Additions to a public park or other public open space area.
3. The buffer area along collector and arterial streets may be included in required overall common open space for residential subdivisions.
4. Parkway along local residential streets with detached sidewalks that meet all the following standards may count toward the common open space requirement:
 - a. The parkway is a minimum of eight feet (8') in width from street curb to edge of sidewalk and includes street trees as specified otherwise herein.
 - b. Except for alley accessed dwelling units, the area for curb cuts to each residential lot or common driveway shall be excluded from the open space calculation. For purposes of this calculation, the curb cut area shall be a minimum area of twenty-six feet (26') by the width of the parkway.
 - c. Stormwater detention facilities do not qualify to meet the common area open space requirements, unless all of the following is met:
 1. Must be at least fifty feet by one hundred feet (50' x 100') in area;
 2. Specifically designed as a dual use facility, as determined by the administrator, to include minimal slopes, grass throughout, and guarantee of water percolation within 24 hours of storm event.
 3. Is located in a development that has a second usable open space area that contains a qualified site amenity as herein defined.
5. Visual natural space, including open ditches, wetlands, slopes or other areas that may not be readily accessible to residents, and is provided with open style fencing, may qualify for up to 20% of the required open space total, as determined by the Administrator.

C. Qualified Site Amenities: Qualified site amenities shall include, but not be limited to, the following:

1. Clubhouse;

- 2. Fitness facilities, indoors or outdoors;
- 3. Public art;
- 4. Picnic area; or
- 5. Recreation amenities:
 - a. Swimming pool with an enlarged deck and changing and restroom facility (pools shall count towards 3 required site amenities).
 - b. Children's play structures.
 - c. Sports courts.
 - d. Additional open space in excess of 10% qualified usable space.
 - e. RV parking for the use of the residents within the development.
 - f. School and/or Fire station sites if accepted by the district.
 - g. Pedestrian or bicycle circulation system amenities meeting the following requirements:
 - (1) The system is not required for sidewalks adjacent to public right of way;
 - (2) The system connects to existing or planned pedestrian or bicycle routes outside the development; and
 - (3) The system is designed and constructed in accord with standards set forth by the city of Star;
 - h. Pond and/or waterway amenities including, but not limited to docks, shade structures, ADA access, and fish stocking.

6. Community Gardens.

D. Location: The common open space and site amenities shall be located on a common lot or an area with a common maintenance agreement.

E. Maintenance:

- 1. All common open space and site amenities shall be owned by and be the responsibility of an owners' association for the purpose of maintaining the common area and improvements thereon.

8-3B-3: RESIDENTIAL DISTRICTS: ADDITIONAL RESIDENTIAL DISTRICT STANDARDS

J. Additional residential standards applying to all new residential subdivisions:



a. Residential Elevations:

- i. Building elevations for all residential uses shall be submitted with any development application and will be included as part of any preliminary plat, development agreement and/or any other condition of approval.
- ii. Single-Family Residential Building Front and Side Elevation Minimum Standards. These standards shall be reviewed for compliance with all submitted residential building permits under the Building Zoning Certificate process. Council may adopt these standards as part of a development agreement or preliminary plat approval. The following minimum standards shall be applied to all new residential structure elements in all zones:
 - 1. Exterior finishes shall be primarily horizontal/vertical wood or wood product siding, brick, stucco, stone, or other decorative masonry product. **A minimum of three (3) architectural elements shall be provided for all single-family residential structures.** These elements shall include, but are not limited to, shingled, horizontal or vertical siding, stone or brick highlights, garage door windows or hardware, colored window frames, or other architectural treatments deemed appropriate by the administrator.

8-3B-3 - FIGURE EXTERIOR ARCHITECTURAL ELEMENTS:



- 2. Two-story detached structures should provide a minimum of one, second story side window per side elevation, when appropriate.

3. A minimum one (1) foot overhang shall be provided on all roof overhangs. Administrator may approve deviation from this standard.
 4. Dwellings backing up to collector or arterial streets shall have rear elevations and/or architectural designs that provide depth and dimension, avoiding the flat-wall appearance. These elements must be functional and may not be minimized or created solely for the purpose of compliance with this provision.
 5. Additional landscaping buffers may also be required.
- b. Dwelling Unit Design. Building styles shall be spread throughout the entire development (including all contiguously owned and phased properties). Nowhere within the development shall any fewer than 5 different exterior elevation styles and/or floorplans be located adjacent to each other. The number of different dwelling styles within a development shall be as follows:
- a. 1 to 50 units = minimum of 5 architectural styles and/or floorplans
 - b. 51 to 100 units = minimum of 7 architectural styles and/or floorplans
 - c. 101 and over units = minimum of 10 architectural styles and/or floorplans
 - c. Homeowners Associations. All subdivisions shall be maintained by a Homeowners association with appropriate Conditions, Covenants and Restrictions (CC&R's). CC&R's are not enforceable by the City and are private contracts between the developer and the property owner.
 - d. Irrigation and drainage ditches shall not be covered, tiled or re-routed as part of any new residential development unless specifically approved by Council and the applicable irrigation and/or drainage district. Perforated piping may be considered as an option if tiling is allowed.

8-4A-21: MAILBOXES:

All mailbox clusters shall be approved by the postmaster prior to installation. All clusters shall be covered with an architecturally designed cover, to be approved by the Administrator prior to final plat signature. All covers shall be provided with lighting and shall be stained/painted and kept in good condition at all times. The administrator may issue a letter of violation to the HOA when any mailbox cluster or cover falls into disrepair. Maintenance shall be included in the CC&R's. A turnout shall be installed adjacent to the mailbox cluster

to provide community access, if approved by the transportation authority and postmaster. The design shall be included as part of the preliminary plat submittal.



8-6A-3: PRELIMINARY PLAT PROCESS

A. Preapplication Conference: The applicant shall complete a preapplication conference with the administrator or designee prior to submittal of an application for a preliminary plat. The purpose of this meeting is to discuss early and informally the purpose and effect of this title and the criteria and standards contained herein.

B. Neighborhood Meeting: Applicants are required to hold a neighborhood meeting, in conformance with Section 8-1A-6C to provide an opportunity for public review of the proposed project prior to the submittal of an application. The applicant shall provide a summary of the meeting, including questions and concerns of the neighbors and how the submitted application addresses those issues.

C. Application Requirements: A complete subdivision application form and preliminary plat data as required in this title, together with fees shall be submitted to the administrator. At the discretion of the administrator or city engineer, appropriate supplementary information may also be required to sufficiently detail the proposed development within any special development area, including, but not limited to, hillside, planned unit development, floodplain, cemetery, manufactured home parks, and/or hazardous or unique areas of development. When possible, any unresolved access or traffic generation issues related to ACHD/HD4 or ITD regulated roadways should be resolved by the applicant prior to acceptance of any application. The administrator may require a letter from the appropriate transportation agency or servient property owner to be submitted with the application.

D. Required Information and Data: The contents of the preliminary plat and related information shall be in such a form as stipulated by the City Council, however, additional maps or data deemed necessary by the administrator may also be required. The applicant shall submit to the administrator at least the following:

1. Two (2) copies of the preliminary plat of the proposed subdivision, drawn in accordance with the requirements hereinafter stated. Each copy of the preliminary plat shall be submitted on good quality paper, be professionally drafted, shall have the dimensions of not less than twenty-four inches by thirty-six inches (24" x 36"), and shall be drawn to a scale of not less than one inch to one hundred feet (1"=100') and contain a drafting date and north arrow.
2. For hillside developments, one (1) bound copy of the preliminary engineering plans (not meant to be cross sections or detailed designs) showing streets, water, sewers, sidewalks, and other required public improvements, together with preliminary site grading, drainage and irrigation plans of the proposed subdivision. Such engineering plans shall contain sufficient information and detail to make a determination as to conformance of the proposed improvements to applicable regulations, ordinances and standards.
3. Two (2) copies of a landscape plan showing all open space, common areas, amenities, street trees and development signage;
4. Additional information on the preliminary plat and separately submitted information to include the following:
 - a. The name of the proposed subdivision, as approved in advance by the Ada County Engineering office;
 - b. The name, address, telephone and email of the applicant, developer, engineer and surveyor or drafter who prepared the preliminary plat;
 - c. The names and addresses of all adjoining property owners and recorded plats within three hundred feet (300') of the external boundaries of the land being considered for subdivision.
 - d. The land use and existing and proposed zoning of the proposed subdivision and the adjacent land;
 - e. Streets, street names, rights of ways and roadway widths, including adjoining streets or roadways;
 - f. Lot lines and blocks showing the dimensions and numbers of each, together with area of each lot in acres and square feet;
 - g. Contour lines, shown at five-foot (5') intervals where the land slope is greater than ten percent (10%) and at two-foot (2') intervals where land slope is ten percent (10%) or less, referenced to an established benchmark, including location and elevation;
 - h. Any proposed or existing utilities, including, but not limited to, power poles, storm and sanitary sewers, irrigation laterals, ditches, bridges, culverts, water mains and fire hydrants;
 - i. Any flood zone information including FEMA FIRM panels;
 - j. The legal description of the boundary of the property being subdivided with the seal of the surveyor of record;

- k. Phasing plan showing all proposed phases of the development;
 - l. Preliminary irrigation analysis showing availability of water rights and distribution of irrigation to the lots within the proposed subdivision, or waiver request;
 - m. One (1) copy of a site report of the highest seasonal groundwater elevation prepared by a licensed engineer;
 - n. narrative, signed by the applicant, fully describing the proposed subdivision, including such information as number and type of uses on the lots (residential single, two or multi-family, commercial, etc.), common lots and the proposed uses of those lots (open space, parks, playgrounds, landscaping, or other uses) and any other information deemed necessary to explain the intent of the development including how it relates to other concurrently submitted applications (annexations, rezones, PUD's, CUP's, etc.).
 - o. Neighborhood meeting information including sign-in sheet, copy of meeting letter, copy of mailing labels, and detailed summary of neighbor questions and concerns and how the development has been designed to address those concerns;
 - p. A vicinity map showing the relationship of the proposed plat to the surrounding area (1/2-mile radius);
 - q. Deeds, affidavit of legal interest, address labels, postal service location approval, ACHD traffic study review status, electronic copies, or any other required information deemed necessary by the administrator to allow for proper review of the application;
 - r. A conceptual site plan shall be submitted for any non-residential developments showing building locations, parking and loading areas, traffic access drives and traffic circulation and trash enclosure locations;
 - s. Any additional required information for special area of developments including, but not limited to hillsides, wetlands or as further specified in this Title.
5. Additional information in the application as determined by the administrator may include the following:
- a. Building elevations, including multi-family uses, non-residential uses, clubhouses, well houses, or other elevations deemed necessary to assist the Council in their decision regarding a development;
 - b. Colored site plan and renderings of a subdivision detailing residential lot locations, open space and common areas, buffers, roadways, waterways and irrigation ditches, fencing, signs and landscaping.

E. Acceptance: Upon receipt of the preliminary plat, and compliance with all other requirements as provided for herein, the administrator shall certify the application as complete and shall affix the date of acceptance.

F. Decision: A decision on a preliminary plat for a parcel of land is made by the City Council after receiving a recommendation from the administrator and a public hearing is held.

COMPREHENSIVE PLAN (As of Submittal of Original Application – Under Old Plan):

8.2.3 Land Use Map Designations:

Neighborhood Residential:

Suitable primarily for single family residential use. Densities in the majority of this land use area are to range from 3 units per acre to 5 units per acre. Densities not exceeding 1 to 2 units per acre are to be encouraged in areas of the floodplain, ridgeline developable areas, hillside developable areas, and where new residential lots are proposed adjacent to existing residential lots of one acre and larger where those existing larger lots are not likely to be subdivided in the future.

8.3 Goal:

Encourage the development of a diverse community that provides a mix of land uses, housing types, and a variety of employment options, social and recreational opportunities, and where possible, an assortment of amenities within walking distance of residential development.

8.4 Objectives:

- Manage urban sprawl in order to minimize costs of urban services and to protect rural areas.
- Retain and encourage rural areas where it will not result in increased costs for urban service.
- Encourage land uses that are in harmony with existing resources, scenic areas, natural wildlife areas, and surrounding land uses.

8.5.3 Policies Related Mostly to the Urban Residential Planning Areas:

A. The Neighborhood Residential Land Use is to encourage urban style development densities to limit urban sprawl.

B. Low densities within the Neighborhood Residential Land Use are to be designed within the floodplain, ridgeline developable areas, hillside developable areas and where new residential lots are proposed adjacent to existing residential lots of one acre and larger where those existing larger lots are not likely to be subdivided in the future.

8.5.9 Additional Land Use Component Policies:

- Encourage flexibility in site design and innovative land uses.

- Encourage landscaping to enhance the appearance of subdivisions, structures, and parking areas.
- Require more open space and trees in subdivisions.
- Work with Ada County Highway District (ACHD), Canyon Highway District #4 (CHD4), and Idaho Department of Transportation (ITD) for better coordination of roadway and access needs.
- Support well-planned, pedestrian-friendly developments.
- Dark sky provision should be adopted within the code to assure down style lighting in all developments and Star should consider joining the International Dark Sky Association.
- The City should utilize the 2018 Treasure Valley Tree Selection Guide when requiring trees within developments.

18.4 Implementation Policies:

E. Development Agreements allow the city to enter into a contract with a developer upon rezoning. The Development Agreement may provide the city and the developer with certain assurances regarding the proposed development upon rezoning.

PROJECT OVERVIEW

PRELIMINARY PLAT: (Original Application Review)

The Preliminary Plat submitted contains 71 single family residential lots, and 12 common area lots for a total of 83 lots. The gross density of the proposed development is 2.99 dwelling units/acre. The residential lots range in size from approximately 7,949 square feet to 14,342 square feet with the average buildable lot area of 9,280 square feet. The applicant has indicated in their Open Space Exhibit that the development will contain a total of 4.34 acres (18.3%) of overall open space, which includes 3.38 acres within the proposed plat and .96 acres located within an area in Star River Ranch No. 1. The applicants intent is to improve this .96-acre area for a future dog park that will be utilized by both subdivisions. The total usable open space proposed is 3.63 acres (15.4%) . The Unified Development Code, Section 8-4E-2 states that all developments shall have a total of 15% open space and 10% usable open space. The current preliminary plat, as submitted satisfies these requirements. Streets are proposed to be public throughout the development and will measure 36 ft from back of curb to back of curb and satisfy Section 8-4D-34B(4) of the UDC.

The development will be primarily accessed off Bent Lane via Balto Street. There will also be access via Silverthorne Avenue from Bent Lane along the south. Silverthorne Avenue will connect to the future northern collector road when that property is developed.

The current Unified Development Code, Section 8-4E-2 requires a development of this size to have 2 site amenities. The preliminary plat shows a total of 3 amenities. The applicant is

proposing a large, central open area that will house 2 soccer fields. The applicant is also proposing connected walking paths within the development and a future dog park.

ADDITIONAL DEVELOPMENT FEATURES:

- **Sidewalks:** - Sidewalks are proposed at five-foot (5') widths and will be attached throughout the overall subdivision. **At the time of submittal, detached sidewalks were not required under the UDC that was in affect at the time of submittal of the application. Staff recommends that the Council consider requiring detached sidewalks that meet the current intent of the City for streetscapes.**
- **Lighting:** - Streetlights shall reflect the "Dark Sky" criteria with all lighting. The same streetlight design shall continue throughout the entire development. The applicant has not submitted a streetlight plan, indicating location of lights or a street light design. A condition of approval will be required to receive staff approval of streetlights prior to final plat approval.
- **Landscaping:** - As required by the Unified Development Code, Chapter 8, Section 8-8C-2-M (2) Street Trees; A minimum of one street tree shall be planted for every thirty-five (35) linear feet of street frontage. The applicant shall use "Treasure Valley Tree Selection Guide", as adopted by the Unified Development Code. The landscape plan submitted with the Preliminary Plat shows street trees throughout the large common area and dog park as required per code. The submitted landscape plan appears to satisfy these requirements. Not all street trees are on the landscape plan and appear to be located by the builder. A condition of approval will be to hold certificates of occupancy until trees are planted per code. Common areas along the roads do not contain the necessary number of trees called for by code. A condition of approval will be to submit a new plan showing the correct number of street trees in all the common areas.
- **Setbacks:** – The Applicant is requesting 5-foot side yard setbacks that were allowed in the R-5 zoning district at the time of submittal of the application and prior to the June 7, 2022 Code revision. All other setbacks of the current R-5 zone are consistent with the new R-3 setbacks established by the June 7, 2022 UDC rewrite.
- **Block Length:** - Blocks 2 and 3 are over 750' in length. Therefore, Council must approve the waiver to the block length for these areas.
- **Mailbox Cluster:** - Applicant has provided communication to the Star Postmaster and they are awaiting his response on cluster location.
- **Subdivision Name:** - Applicant has provided documentation showing approval for the subdivision name.

- **Street Names:** - Applicant has provided documentation showing approval for the street names as public streets. New street names will be required to conform to the private road naming conventions.
- **Phasing:** - Applicant is proposing the development will be built out in two phases.

PRELIMINARY PLAT: (Modification Application Review)

The Preliminary Plat submitted contains 71 single family residential lots, and 11 common area lots and 1 common driveway, for a total of 83 lots. The gross density of the proposed development is 2.99 dwelling units/acre. The residential lots range in size from approximately 6,260 square feet to 14,716 square feet with the average buildable lot area of 9,319 square feet. The applicant has indicated in their Open Space Exhibit that the development will contain a total of 4.86 acres (20.45%) of overall open space, which includes 3.81 acres (16.03%) of usable open space. The Unified Development Code, Section 8-4E-2 states that all developments shall have a minimum of 15% open space and 10% usable open space. The current preliminary plat, as submitted satisfies these requirements.

Streets are now proposed to be private throughout the development. The applicant is proposing local streets to be 36 feet from back of curb to back of curb located in a 40-foot Right of Way with an 8-foot-wide planter strip and 5-foot-wide detached sidewalks. These measurements satisfy Section 8-4D-34B(4) of the Unified Development Code. The applicant has also removed the entrance road on the southwest corner of the property and is proposing a shared driveway instead. Under Section 8-6B-2D, common/shared driveways shall not be permitted for residential developments unless an approved, emergency turnaround is provided, and the driveway is approved by the Fire District. Staff recommends that the entrance road be added back to the plan as a shared drive does not meet code and the secondary entrance is necessary for emergency access.

The applicant is proposing that the main entrance to the development be divided by a median, creating 18-foot-wide driving lanes with 7-foot-wide planter strips and 5-foot-wide detached sidewalks. This will require Fire District approval.

The development will be primarily accessed off Bent Lane via Balto Street. There will also be access via Silverthorne Avenue which will connect to a future collector road, Landruff Lane, to the north when that property is developed. **Silverthorne Street was originally connected to Bent Lane on the south, Staff recommends that connection still be made for emergency access and to eliminate the shared drive proposed on the new plan.**

The current Unified Development Code, Section 8-4E-2 requires a development of this size to have 2 site amenities. The original preliminary plat proposed a total of 3 amenities. The revised

preliminary plat is now removing pickleball courts and replacing it with a pool. There will also be a large open, grass area and walking paths throughout the development.

ADDITIONAL DEVELOPMENT FEATURES:

- **Sidewalks:** - Sidewalks are proposed at five-foot (5') widths and will be detached throughout the overall subdivision.
- **Lighting:** - Streetlights shall reflect the "Dark Sky" criteria with all lighting. The same streetlight design shall continue throughout the entire development. The applicant has not submitted a streetlight plan, indicating location of lights or a street light design. A condition of approval will be required to receive staff approval of streetlights prior to final plat approval.
- **Landscaping:** - As required by the Unified Development Code, Chapter 8, Section 8-8C-2-M (2) Street Trees; A minimum of one street tree shall be planted for every thirty-five (35) linear feet of street frontage. The applicant shall use "Treasure Valley Tree Selection Guide", as adopted by the Unified Development Code. The landscape plan submitted with the Preliminary Plat shows street trees throughout the large common area and dog park as required per code. The submitted landscape plan appears to satisfy these requirements.
- **Setbacks:** - The Applicant is requesting 5-foot side yard setbacks that were allowed in the R-5 zoning district at the time of submittal of the application and prior to the June 7, 2022 Code revision. All other setbacks of the current R-5 zone are consistent with the new R-3 setbacks established by the June 7, 2022 UDC rewrite.
- **Block Length:** - Matanuska and Talkeetna streets are over 750' in length. Council will need to approve these private street lengths.
- **Mailbox Cluster:** - Applicant has provided communication from the Star Postmaster depicting the approved location of the mailbox clusters. All clusters shall be covered with an architecturally designed cover, to be approved by the Administrator prior to final plat signature. All covers shall be provided with lighting and shall be stained/painted and kept in good condition at all times
- **Subdivision Name:** - Applicant has provided documentation showing approval for the subdivision name.
- **Street Names:** - Applicant has provided documentation showing approval for the street names and they are showing accurately on the submitted preliminary plat. New street names will be required to conform to the private road naming conventions.

- **Phasing:** - Applicant is proposing the development will be built out in two phases.
- **Gates:** - **The applicant is proposing the private streets to be gated. Unified Development Code section 8-4D-3(4) states that private gates or other obstacles shall not be allowed, unless approved by Council and the Fire District.**

AGENCY RESPONSES

Star Fire District

October 7, 2024

PUBLIC RESPONSES

No public responses have been received.

STAFF ANALYSIS & RECOMMENDATIONS

City staff has received comments about private streets from emergency services, including police and fire, on the increased concerns they are having with the City approving private streets, particularly in higher density subdivisions. These concerns mostly revolve around police enforcement of traffic laws, including speeding, driving under the influence and illegal parking. With private streets, the police department has no jurisdiction to enforce these laws. With the higher the residential density, the greater chance of violation within these subdivisions, and a higher expectation from residents who assume that the police will take care of these violations. This leaves enforcement strictly in the hands of the homeowners associations.

Staff is also concerned that the access in the southwest corner of the development have been removed from the original approved plat, forcing the applicant to propose common driveways to access residential lots in that area. Staff would recommend that Council require this roadway access to be remain, and the common driveway be denied.

Based upon the concerns by the emergency services, staff cannot provide a positive recommendation on this application request to convert the public streets into private streets for this development. The Council should consider the entire record and testimony presented at their scheduled public hearing prior to rendering its decision on the matter. Should the Council vote to approve the request, staff is providing conditions of approval for Council to consider. Council shall direct staff to draft findings of fact and conclusions of law based on their decision for the Council to consider at a future date.

FINDINGS

The Council may **approve, conditionally approve, deny** or **table** this request. In order to approve these applications, the Unified Development Code requires that Council must find the following:

PRELIMINARY PLAT FINDINGS:

- 1. The plat is in compliance with the Comprehensive Plan.
The City must find that this Plat follows designations, spirit and intent of the Comprehensive Plan regarding residential development and meets several of the objectives of the Comprehensive Plan such as:
 - 1. *Designing development projects that minimize impacts on existing adjacent properties, and*
 - 2. *Managing urban sprawl to protect outlying rural areas.*

2. Public Services are available or can be made available and are adequate to accommodate the proposed development.

The City must find that Agencies having jurisdiction on this parcel were notified of this action, and that it has not received notice that public services are not available or cannot be made available for this development.

- 3. There is public financial capability of supporting services for the proposed development;
The City must find that they have not been notified of any deficiencies in public financial capabilities to support this development.

4. The development will not be detrimental to the public health, safety or general welfare;

The City must find that it has not been presented with any facts stating this Preliminary Plat will be materially detrimental to the public health, safety and welfare. Residential uses are a permitted use.

- 5. The development preserves significant natural, scenic or historic features;
The City must find that there are no known natural, scenic, or historic features that have been identified within this Preliminary Plat.

PRIVATE STREET FINDINGS:

- A. The design of the private street meets the requirements of this article;
The City must find that the proposed private streets meets the design standards in the Code.

B. Granting approval of the private street would not cause damage, hazard, or nuisance, or other detriment to persons, property, or uses in the vicinity:

The City must find that it has not been presented with any facts stating this private road will cause damage, hazard or nuisance, or other detriment to persons, property or uses in the vicinity.

C. The use and location of the private street shall not conflict with the comprehensive plan and/or the regional transportation plan.

The City must find that the use is not in conflict with the comprehensive plan and/or regional transportation plan.

Upon granting approval or denial of the application, the Council shall specify:

1. The Ordinance and standards used in evaluating the application;
2. The reasons for recommending approval or denial; and
3. The actions, if any, that the applicant could take to obtain approval.

CONDITIONS OF APPROVAL (IF APPROVED BY COUNCIL)

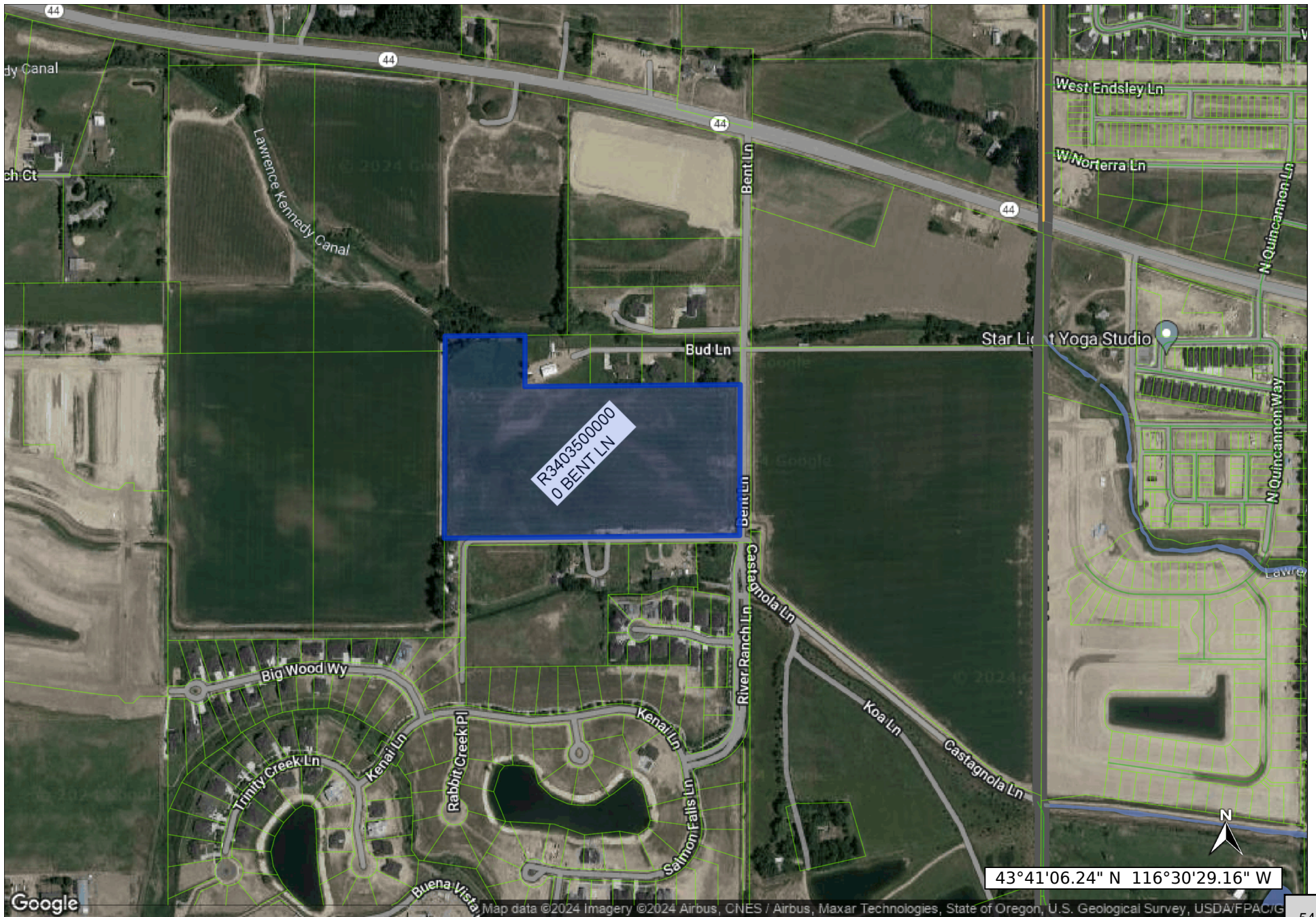
1. The approved Preliminary Plat for the Legado (Star River Ranch North) Subdivision shall comply with all statutory requirements of applicable agencies and districts having jurisdiction in the City of Star.
2. All streets shall have a minimum street width of 36' and shall be constructed to HD4/ACHD standards.
3. **Applicant shall meet all the setback requirements in the Unified Development Code Section 8-3A-4, unless a waiver is applied for and approved by the council. 5-foot side yard setbacks have been approved by Council.**
4. The applicant shall enter into a Development Agreement with the city, agreeing to proportionate share assessment by ITD regarding impacts to the State Highway System within the City of Star and/or City of Star Area of City Impact. **The Developer will pay the \$71,000.00 traffic mitigation fee determined by the Idaho Transportation Department as follows: the Developer will pay the city \$1,000.00 per buildable lot within each phase prior to signature on the final plat for the applicable phase.** The City will allocate the funds to roadway improvements in the vicinity of the project. The Developer shall pay this amount (unless otherwise revised by ITD) directly to the City of Star. The City will maintain this contribution in a specific Development Contributions account, to be distributed to ITD when requested for use with a specific Idaho Transportation Improvement Plan (ITIP) project within the City of Star Area of City Impact or City Limits in accordance with the terms of the Intergovernmental Agreement between the Idaho Transportation Department and the City of Star dated April 22, 2020. These fees will be collected by the City of Star, by phase, prior to final plat signature.
5. **The Applicant/Owner shall submit a private street maintenance plan, including future funding, in compliance with Section 8-4D-3C of the UDC, with the submittal of the final plat application.**

6. **The Applicant/Owner shall provide an emergency access easement allowing access to Fire, Police and Emergency Services on the private roads. The Applicant/Owner shall work with City Staff on the wording/process.**
7. **The applicant shall comply with the Residential Standards for all new houses, as required in Section 8-3B-3 of the UDC.**
8. **The mailbox cluster must be covered and reasonably lit, as required in Section 8-4A-21 of the UDC.**
9. Streetlights shall comply with the Star City Code and shall be of the same design throughout the entire subdivision and shall be maintained by the Homeowners Association. Streetlights shall be installed prior to any building occupancy. Design shall follow Code with requirements for light trespass and "Dark Skies" lighting. **Applicant/Owner shall submit a streetlight plan and design prior to Final Plat approval. Streetlights shall comply with the Star City Code regarding light trespass and "Dark Sky" initiative.**
10. Street trees shall be installed per Chapter 8, including Section 8-8C-2-M(2) Street Trees including one (1) tree per thirty-five (35) linear feet. **For all trees to be located by the builder, the certificate of occupancy will be held until the required street trees are installed.**
11. The property associated with this approved Preliminary Plat, in addition to the property of all future phases shall be satisfactorily weed abated at all times, preventing a public nuisance, per Star City Code Chapter 3, Section 3-1-1 through 3-1-7.
12. The property associated with this approved Preliminary Plat, in addition to the property of all future phases shall be properly maintained at all times, including throughout the construction process to include trash picked up and trash receptacles emptied with regular frequency, streets swept and cleaned weekly, including any streets used to access the property and all debris shall be prevented from accumulating on any adjacent property or public right of way and shall remove all debris from public way at least daily. This shall also include, but is not limited to any trash, junk or disabled vehicles during any portion of the development process. The site shall be properly mitigated from fugitive dust at all times, including during construction, as determined by the Zoning Administrator. Failure to comply with any of the above may result in a stop work order being issued until the violations are remedied, and/or revocation of preliminary plat/final plat approvals.
13. The property associated with this approved Preliminary Plat, in addition to the property of all future phases shall be properly maintained throughout the construction process to include trash picked up and trash receptacles emptied with regular frequency, streets swept and cleaned weekly, or as often as necessary including any streets used to access the property and all debris shall be prevented from accumulating on any adjacent property or public right of way and shall remove all debris from public way at least daily.
14. All signed Irrigation District Agreements with the Irrigation Districts shall be provided to the City of Star with each subsequent Final Plat application.
15. Pressurized irrigation systems shall comply with the Irrigation District(s) and the City of Star Codes. Plans for pressurized irrigation systems shall be submitted to, and approved by the City of Star Engineer, prior to installation.

- 16. A plat note supporting the "Right to Farm Act" as per Idaho Code Title 22, Chapter 45, shall be shown on the Final Plat.
- 17. A copy of the CC&R's shall be submitted to the City of Star at Final Plat.
- 18. A form signed by the Star Sewer & Water District shall be submitted to the City prior to the signature of the Final Plat stating that all conditions of the District have been met, including annexation into the District.
- 19. A plat note shall state that development standards for residential development shall comply with the effective building and zoning requirements at time of building permit issuance, unless amended in the Development Agreement or CUP conditions.
- 20. Development standards for single family residential units shall comply with effective building and zoning requirements at time of building permit issuance, or as approved through the Development Agreement or as stated herein.
- 21. All common areas shall be owned and maintained by the Homeowners Association.
- 22. The applicant shall provide a sign, to be located at all construction entrances, indicating the rules for all contractors that will be working on the property starting at grading and running through home sales that addresses items including but not limited to dust, music, dogs, starting/stopping hours for contractors (7a.m. start time). **Sign shall be approved by the City prior to start of construction.**
- 23. A sign application is required for any subdivision signs.
- 24. Any additional Condition of Approval as required by Staff and City Council.

COUNCIL DECISION

The Star City Council _____ File Number PP-22-05-05 MOD and PR-24-05 for Star River Ranch North Subdivision aka Legado Subdivision on _____, 2024.



August 13, 2024
Project No.: 21-162

Mr. Shawn Nickel
Planning Director and Zoning Administrator
City of Star
10769 West State Street
Star, ID 83669

**RE: Legado (fka Star River Ranch North) Subdivision – Star, ID
Preliminary Plat Modification and Private Road Application**

Dear Shawn:

On behalf of Bent Ln LLC., we are pleased to submit a preliminary plat modification and private road application for the Legado Subdivision, formerly known as the Star River Ranch North Subdivision.

Site Location and History

The property is approximately 23.77 acres located south of State Highway 44 and west of Bent Lane. A preliminary plat for 71 single-family residential lots and 12 common lots served by internal public roadways was approved on March 21, 2023. Since the development received approval in 2023, the Developer has determined that they would like to utilize private roadways within the subdivision. As such, a preliminary plat modification and private road application are now proposed for the subdivision.



Preliminary Plat Modification

The density, number of lots and overall design of the Legado preliminary plat has not changed since the approval of the original preliminary plat in 2023. As noted, a preliminary plat modification is being requested to allow the community to utilize private streets and to be secured with gates in two locations. In revising the preliminary plat and considering private streets, the Developer also decided to pursue a pool with changing rooms instead of the originally proposed pickleball court.

The development encompasses approximately 23.77 acres and consists of 71 single-family detached residential 11 common open space lots, and 1 common drive lot, totaling 83 lots. Residential lot sizes range from approximately 6,260 square feet to 14,716 square feet with an average lot size of approximately 9,319 square feet. The gross density of the subdivision is 2.99 du/acre, which is significantly below the maximum density permitted in the R-5 district and similar to density typical in the R-3 zoning district.

Residential lots have been designed to ensure compatibility with adjacent development and lots within previous phases of Star River Ranch while providing a smaller footprint to transition from larger lots to the mixed use and commercial zones nearby. No variances or deviations from R-5 dimensional standards are requested or needed to accommodate the community planned.

The development is still planned to occur in two phases. The first phase will be comprised of 34 buildable lots, with primary access proposed via Balto Street. A secondary, emergency access is proposed via a common driveway to Bent Lane. Originally, secondary access was proposed via Silverthorne Avenue to Bent Lane. However, since the revised preliminary plat includes private streets with gates, the Developer would like to limit access to Bent Lane to increase the exclusive feel of the development and minimize traffic experienced by existing residents to the south. The second phase will be comprised of 37 buildable lots and will include completion of Silverthorne Avenue to connect with Landruff Lane.

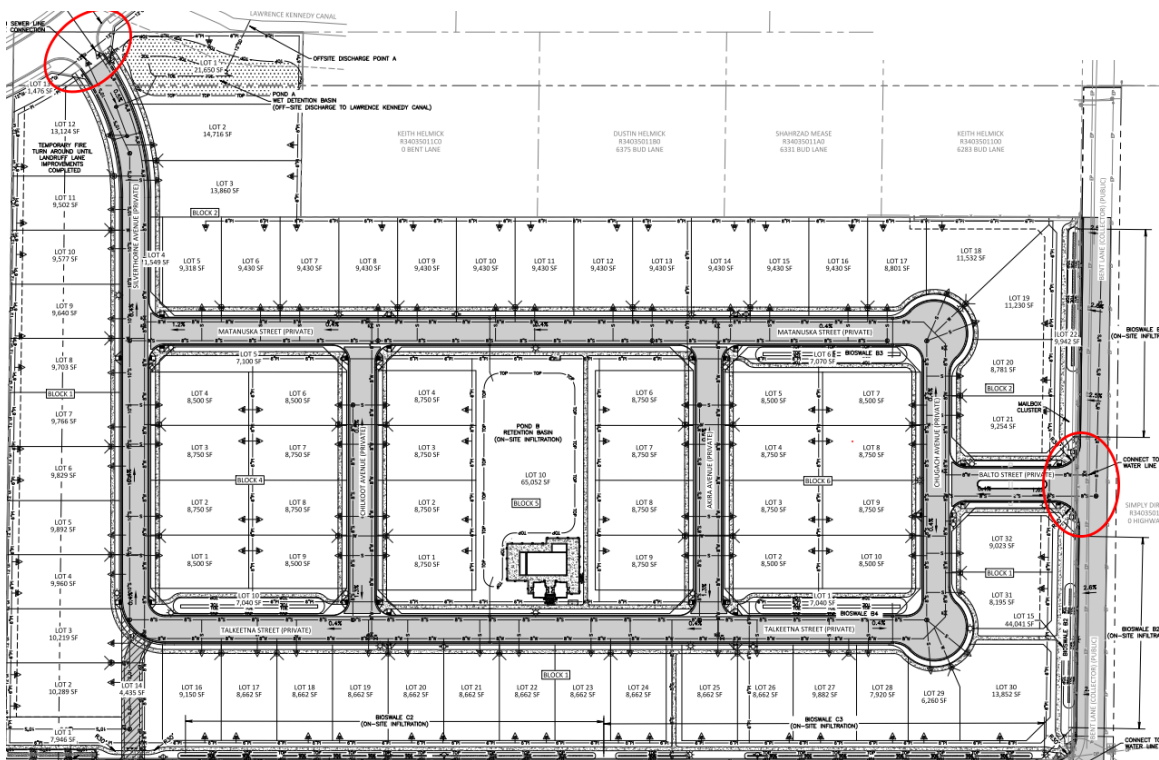
Private Streets

Originally, the preliminary plat had proposed internal public streets to provide access throughout the development. The configuration of roadways remains consistent with the original approval with the proposed private streets, aside from a common driveway in the southwest corner of the site providing emergency access for the first phase of the development. Roadway sections will remain the same width and improvements will conform with Highway District No 4 design standards as required by City Code.

The proposed private streets will include two gates to be located at the development’s entrance points. One gate will be placed at the primary entrance road for the subdivision, Balto Street, a full access point connecting to Bent Lane via State Highway 44 (State Street). Balto Street leads into Chugach Avenue which forks north and south into Matanuska and Talkeetna Streets respectively. Silverthorne Avenue, a north-south local road in the westerly part of the project extends stubs to the north where a future collector roadway (Landruff Lane) is planned. The second gate will be placed at the northwest corner of the site, where Silverthorne



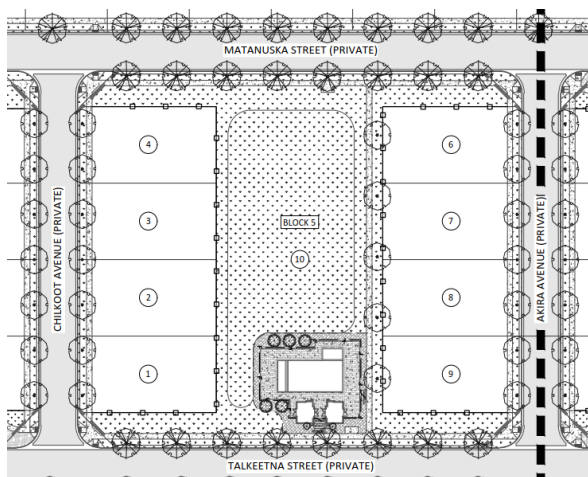
Avenue will eventually connect with Landruff Lane. Gate locations have been circled in the figure below. Schematic examples of the kind of gates contemplated for the development are included within this application.



A traffic impact study (TIS) was submitted to Highway District No 4 (HD4) with the original preliminary plat.

Open Space and Amenities

The approved preliminary plat proposed to include a pickleball court within the central open space lot. With the proposed revision to private streets, the Developer would like to propose that a pool with changing rooms be included for future residents. The development includes pedestrian connectivity throughout to provide easy access to the proposed central amenity and an active amenity for future residents.



Services

In accordance with City Code, Legado will utilize City services. Water is proposed to be connected at two locations in Bent Lane, will be extended throughout the subdivision and stubbed to the south, southwest and northwest corners of the property for future extension. Sanitary sewer will be extended from an existing line in Bent Lane, through the project and will be stubbed to the south for future extension. Exact locations will be determined during final design as we continue to coordinate with the Star Sewer and Water District.

The property is less than 1 ½ miles from Star Elementary School and is about 2 ½ miles from Star Middle School. The Boise River and various parks among other services and amenities are located in close proximity. In addition, the community will have access to open space available in the previous phases of Star River Ranch. Adjacent zoning and the future land use map supports a variety of commercial and residential uses that will complement the proposed development.

Flood Hazard Development

The property is located within the AE Zone floodplain according to the existing FEMA FIRM panel 160236. In accordance with City Code requirements, we are including supplemental information regarding base flood elevations on our preliminary plat. We will continue to coordinate with the City and FEMA to complete any requirements associated with the floodplain development.

Irrigation Districts

The Lawrence Kennedy Canal abuts but does not cross through the northerly portion of the property. The property is currently served by the Canyon County Water Company, Ltd. We will coordinate with them to ensure their standards are being met throughout the project’s duration.

As requested by the application checklist, the following information reflects the contact information for the irrigation districts associated with or adjacent to this project to the best of our knowledge:

- Canyon County Water Company, LTD. : PO Box 11, Star, Idaho 83669-0011

Conclusion

We are excited to bring the revised Legado Subdivision to the City of Star. This community will complement surrounding uses and provide needed housing in this area of Star. The proposed private streets will be compatible with existing and planned development and will allow future residents to live in a secure and beautiful area close to existing development.

Should you have questions or require further information in order to process this application, please feel free to contact me.

Sincerely,
KM Engineering, LLP

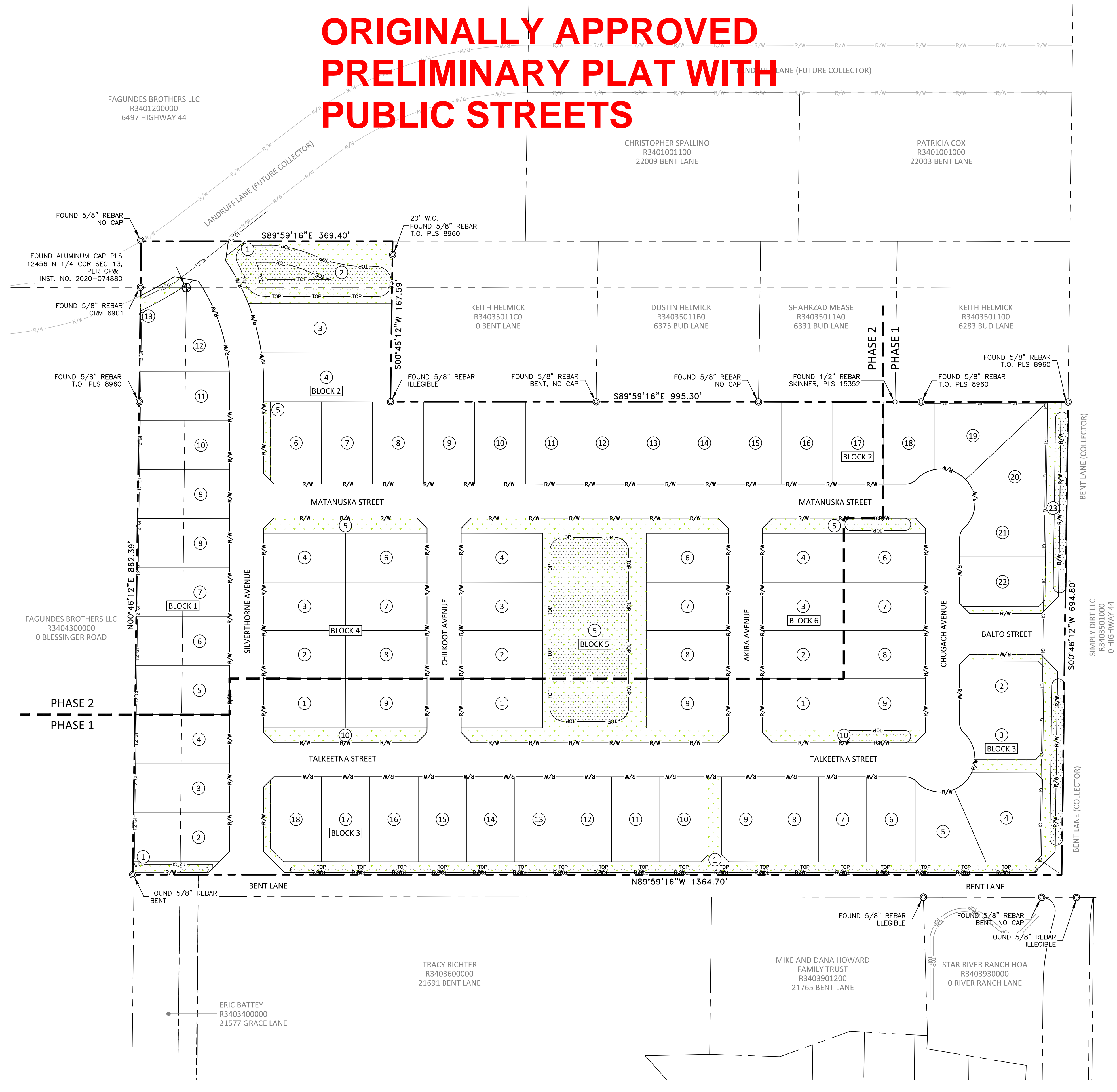


Stephanie Hopkins
Land Planning Manager

PRELIMINARY PLAT SHOWING STAR RIVER RANCH NORTH SUBDIVISION

A PORTION OF LAND IN THE SW 1/4 OF THE SE 1/4 AND THE
SE 1/4 OF THE SW 1/4 OF SECTION 12, ALONG WITH A
PORTION OF THE NW 1/4 OF THE NE 1/4 AND THE NE 1/4
OF SECTION 13, TOWNSHIP 4 NORTH, RANGE 2 WEST,
BOISE MERIDIAN, CITY OF STAR, CANYON COUNTY, IDAHO
FEBRUARY 2023

ORIGINALLY APPROVED PRELIMINARY PLAT WITH PUBLIC STREETS



PRELIMINARY PLAT NOTES

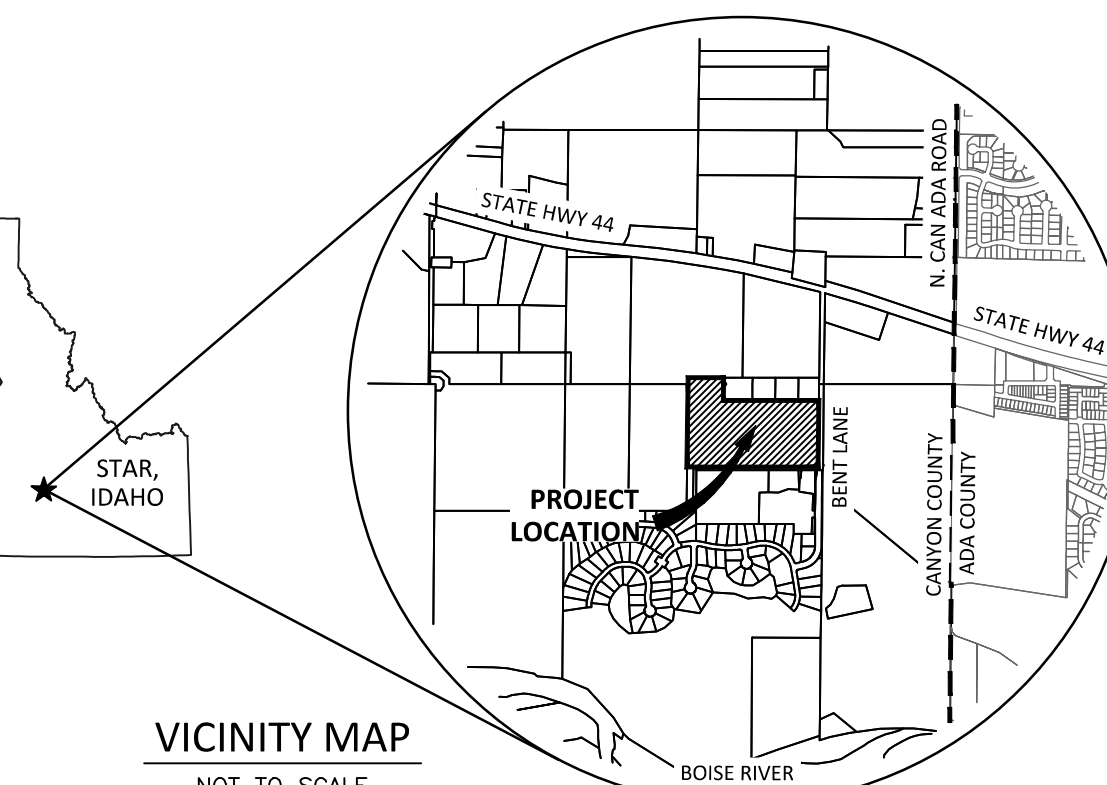
1. ANY RE-SUBDIVISION OF THIS PLAT SHALL COMPLY WITH THE APPLICABLE ZONING REGULATIONS IN EFFECT AT THE TIME OF RE-SUBDIVISION.
2. MINIMUM BUILDING SETBACK LINES SHALL BE IN ACCORDANCE WITH THE APPLICABLE ZONING AND SUBDIVISION REGULATIONS AT THE TIME OF ISSUANCE OF THE BUILDING PERMIT OR AS SPECIFICALLY APPROVED AND/OR REQUIRED.
3. LOTS SHALL NOT BE REDUCED IN SIZE WITHOUT PRIOR APPROVAL FROM THE HEALTH AUTHORITY.
4. THIS DEVELOPMENT IS SUBJECT TO COVENANTS, CONDITIONS, RESTRICTIONS, AND EASEMENTS.
5. DIRECT LOT ACCESS FROM COLLECTORS & ARTERIALS IS PROHIBITED UNLESS APPROVED BY CANYON HIGHWAY DISTRICT 4 AND THE CITY OF STAR.
6. THIS DEVELOPMENT RECOGNIZES 22-4503 OF THE IDAHO CODE, RIGHT TO FARM ACT, WHICH STATES: "NO AGRICULTURAL OPERATION, AGRICULTURAL FACILITY OR EXPANSION THEREOF SHALL BE OR BECOME A NUISANCE, PRIVATE OR PUBLIC, BY ANY CHANGED CONDITIONS IN OR ABOUT THE SURROUNDING NONAGRICULTURAL ACTIVITIES AFTER IT HAS BEEN IN OPERATION FOR MORE THAN ONE (1) YEAR, WHEN THE OPERATION, FACILITY OR EXPANSION WAS NOT A NUISANCE AT THE TIME IT BEGAN OR WAS CONSTRUCTED. THE PROVISIONS OF THIS SECTION SHALL NOT APPLY WHEN A NUISANCE RESULTS FROM THE IMPROPER OR NEGLIGENT OPERATION OF AN AGRICULTURAL OPERATION, AGRICULTURAL FACILITY OR EXPANSION THEREON."
7. ALL LOTS ARE HEREBY DESIGNATED AS HAVING A PERMANENT EASEMENT FOR PUBLIC UTILITIES, IRRIGATION, SIDEWALK AND LOT DRAINAGE OVER THE 10-FOOT ADJACENT TO ANY PUBLIC STREET. THE EASEMENT SHALL NOT PRECLUDE THE CONSTRUCTION OF HARD-SURFACE DRIVEWAYS, WALKWAYS, LANDSCAPE, PARKING, FENCING OR OTHER SUCH NON-PERMANENT STRUCTURES.
8. UNLESS OTHERWISE SHOWN AND DIMENSIONED, ALL LOTS ARE HEREBY DESIGNATED AS HAVING A PERMANENT EASEMENT FOR PUBLIC UTILITIES, IRRIGATION AND LOT DRAINAGE OVER THE 5-FOOT ADJACENT TO ANY INTERIOR SIDE LOT LINE, AND OVER THE 10-FOOT ADJACENT TO ANY REAR LOT LINE OR SUBDIVISION BOUNDARY.
9. LOTS 1 & 13, BLOCK 1, LOTS 1, 2, 5 & 23, BLOCK 2, LOT 1, BLOCK 3, LOTS 5 & 10, BLOCK 4, LOT 5, BLOCK 5, AND LOTS 5 & 10, BLOCK 6 ARE COMMON LOTS WHICH SHALL BE OWNED AND MAINTAINED BY THE STAR RIVER RANCH SUBDIVISION NO. 3 HOMEOWNER'S ASSOCIATION.
10. IRRIGATION WATER HAS BEEN PROVIDED FROM CANAL COMPANIES WHICH INCLUDE CANYON COUNTY WATER CO. LTD. COMPLIANCE WITH IDAHO CODE SECTION 31-3805(1)(b). LOTS WITHIN THIS SUBDIVISION WILL BE ENTITLED TO IRRIGATION WATER RIGHTS AND WILL BE OBLIGATED FOR ASSESSMENTS FROM SAID CANAL COMPANIES.
11. THE HOMEOWNER'S ASSOCIATION, UNDERLYING PROPERTY OWNER, OR ADJACENT PROPERTY OWNER IS RESPONSIBLE FOR ALL STORM DRAINAGE FACILITIES LOCATED OUTSIDE THE PUBLIC RIGHT-OF-WAY, INCLUDING ALL ROUTINE AND HEAVY MAINTENANCE.

PRELIMINARY ENGINEERING NOTES

1. DOMESTIC WATER SERVICE WILL BE PROVIDED BY THE STAR SEWER AND WATER DISTRICT. 8-INCH MAINS WILL BE LOOPED THROUGHOUT THE SUBDIVISION.
2. SANITARY SEWER SERVICE WILL BE PROVIDED BY THE STAR SEWER DISTRICT. THE PROPERTY WILL BE SERVED BY CONNECTING TO EXISTING INFRASTRUCTURE IN BENT LANE. EXACT LOCATION TO BE DETERMINED DURING FINAL DESIGN.
3. WATER AND SEWER LINE SIZES AND LOCATIONS ARE PRELIMINARY AND WILL BE REFINED DURING FINAL DESIGN.
4. ALL DOMESTIC WATER MAINS AND SANITARY SEWER WILL BE INSTALLED IN ACCORDANCE WITH THE IDAHO DEPARTMENT OF ENVIRONMENTAL QUALITY, STAR SEWER AND WATER DISTRICT REQUIREMENTS.
5. STORM DRAINAGE FROM THE ROADWAYS AND LOTS WILL BE COLLECTED IN ROADSIDE CURB AND GUTTER AND ROUTED TO PONDS IN THE COMMON AREAS SHOWN ON THE PLAN. THE PONDS SHOULD BE DESIGNED TO MEET THE IDAHO DEPARTMENT OF ENVIRONMENTAL QUALITY AND CITY OF STAR'S REQUIREMENTS. THE STORM DRAINAGE FACILITIES SHOWN ARE PRELIMINARY AND SHALL BE DETERMINED DURING FINAL DESIGN.
6. THE PRESSURE AND GRAVITY IRRIGATION IMPROVEMENTS SHOWN ARE PRELIMINARY AND SHALL BE DETERMINED DURING FINAL DESIGN.
7. FIBER AND DRY UTILITIES TO BE INSTALLED GENERALLY ADJACENT TO RIGHTS-OF-WAY.
8. TEMPORARY FIRE TURN AROUND TO BE PROVIDED ON LOT 12, BLOCK 1 UNTIL LANDRUFF LANE IMPROVEMENTS ARE COMPLETED.
9. EXISTING FENCE, DRY UTILITIES, POWER POLES, AND IRRIGATION FACILITIES ALONG WEST BENT LANE AND NORTH ALPENGLOW STREET TO BE REMOVED AND/OR RELOCATED DURING CONSTRUCTION. DOWNSTREAM IRRIGATION SHARES WILL BE MAINTAINED WITH THE PROPOSED IMPROVEMENTS.

LEGEND

- BOUNDARY LINE
- ADJACENT PROPERTY LINE
- ROAD CENTERLINE
- SECTION LINE
- RIGHT-OF-WAY LINE
- LOT LINE
- EASEMENT LINE
- COMMON AREA
- R.M.
- FOUND 5/8" REBAR, AS NOTED
- FOUND 1/2" REBAR, AS NOTED
- FOUND BRASS CAP, NORTHEAST CORNER SECTION 12



PRELIMINARY PLAT DATA

SITE DATA	
PARCEL NUMBER(S):	R3403500000
CURRENT ZONING:	R-5-DA (RESIDENTIAL WITH A DA)
REQUESTED ZONING:	R-5 (RESIDENTIAL)
FLOOD ZONE:	ZONE AE (SPECIAL FLOOD HAZARD AREA)
PROJECT AREA:	±23.77 ACRES
DEVELOPABLE LOTS:	±15.13 ACRES
COMMON AREA(LANDSCAPE):	±3.30 ACRES
RIGHT-OF-WAY DEDICATION:	±5.34 ACRES
MAXIMUM LOT SIZE:	14,342 SF
MINIMUM LOT SIZE:	7,949 SF
AVERAGE LOT SIZE:	9,280 SF
NUMBER OF LOTS	
DEVELOPABLE LOTS:	71
COMMON AREA (LANDSCAPE):	12
TOTAL NUMBER OF LOTS:	83
TOTAL NUMBER OF DEVELOPABLE LOTS: 71	
COMMON AREA PROVIDED:	3.30 ACRES (±17.90%)
SEE PPL5.0 FOR OPEN SPACE CALCULATIONS	
DENSITY REQUIREMENTS	
R-5 (SINGLE FAMILY RESIDENTIAL)	5 UNITS PER ACRE MAXIMUM
PROPOSED DENSITY:	R-5 SINGLE FAMILY RESIDENTIAL: ±23.77 ACRES (GROSS) 2.99 U.P.A
BUILDING HEIGHT & LOT AREA REGULATIONS	
MAXIMUM HEIGHT:	R-5 35'
FRONT YARD SETBACK:	15' TO LIVING AREA/SIDE LOAD GARAGE 20' TO GARAGE FACE
REAR YARD SETBACK:	15'
INTERIOR SIDE YARD SETBACK:	5'
STREET SIDE YARD SETBACK:	20'
UTILITIES	
CENTRAL WATER:	STAR SEWER AND WATER DISTRICT
CENTRAL SEWER:	STAR SEWER AND WATER DISTRICT
IRRIGATION:	CANYON COUNTY WATER CO. LTD.
POWER:	IDAHO POWER
NATURAL GAS:	INTERMOUNTAIN GAS CO.
TELEPHONE:	CENTURYLINK
FIRE PROTECTION:	CITY OF STAR FIRE DEPARTMENT
POLICE PROTECTION:	CITY OF STAR POLICE DEPARTMENT
PARKS AND OPEN SPACE:	STAR RIVER RANCH SUB. NO. 3 SUBDIVISION HOMEOWNER'S ASSOCIATION
SCHOOLS:	WEST ADA SCHOOL DISTRICT
SOLID WASTE COLLECTION:	REPUBLIC SERVICES

PROJECT TEAM

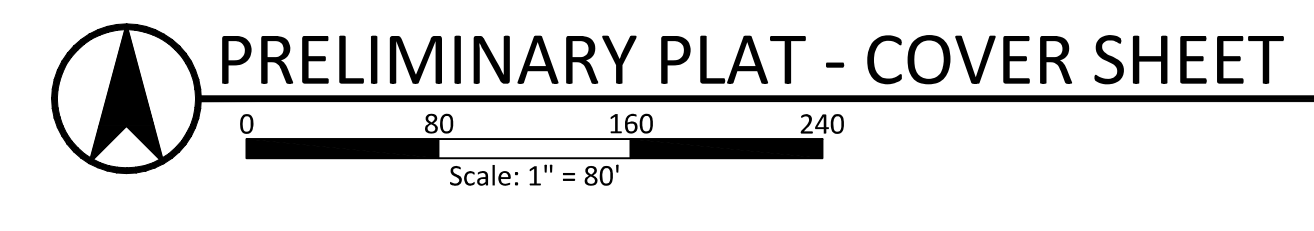
PROPERTY OWNER	
NAME:	BENT LN, LLC.
ADDRESS:	6152 W. HALF MOON LANE
CITY/STATE/ZIP:	EAGLE, IDAHO 83616
ENGINEERING	
NAME:	KM ENGINEERING, LLP.
CONTACT:	KEVIN MCCARTHY, P.E.
ADDRESS:	5725 N. DISCOVERY WAY
CITY/STATE/ZIP:	BOISE, IDAHO 83713
PHONE:	208.639.6939
EMAIL:	kevin@kmengllp.com

INDEX OF DRAWINGS

SHEET NO.	SHEET TITLE
SHEET PP1.0	PRELIMINARY PLAT - COVER SHEET
SHEET PP2.0	PRELIMINARY PLAT - EXISTING CONDITIONS
SHEET PP3.0	PRELIMINARY PLAT - LOT DIMENSIONS
SHEET PP4.0	PRELIMINARY PLAT - PRELIMINARY ENGINEERING
SHEET PP4.1	PRELIMINARY PLAT - PRELIMINARY ENGINEERING
SHEET PPL1.0-5.0	PRELIMINARY PLAT - LANDSCAPE, FENCE, OPEN SPACE

SURVEY CONTROL NOTES

1. ALL SURVEY DATA IS BASED ON THE NAD83 STATE PLANE COORDINATE SYSTEM (IDAHO WEST 1103) AND NAVD 88 VERTICAL DATUM.



**STAR RIVER RANCH NORTH SUB
STAR, IDAHO
PRELIMINARY PLAT**

5725 NORTH DISCOVERY WAY
BOISE, IDAHO 83713
PHONE (208) 639-6939
kmengllp.com

REVISIONS		
NO.	ITEM	DATE
0	INITIAL SUBMITTAL	4/5/22
1	ZONING REQUEST CHANGE	5/26/22
2	CHD4 COMMENT REVISIONS	11/17/22
3	CHD4, CITY COMMENT REVISIONS	3/1/23

DATE:	FEBRUARY 2023
PROJECT:	21-162
SHEET NO.	PP1.0

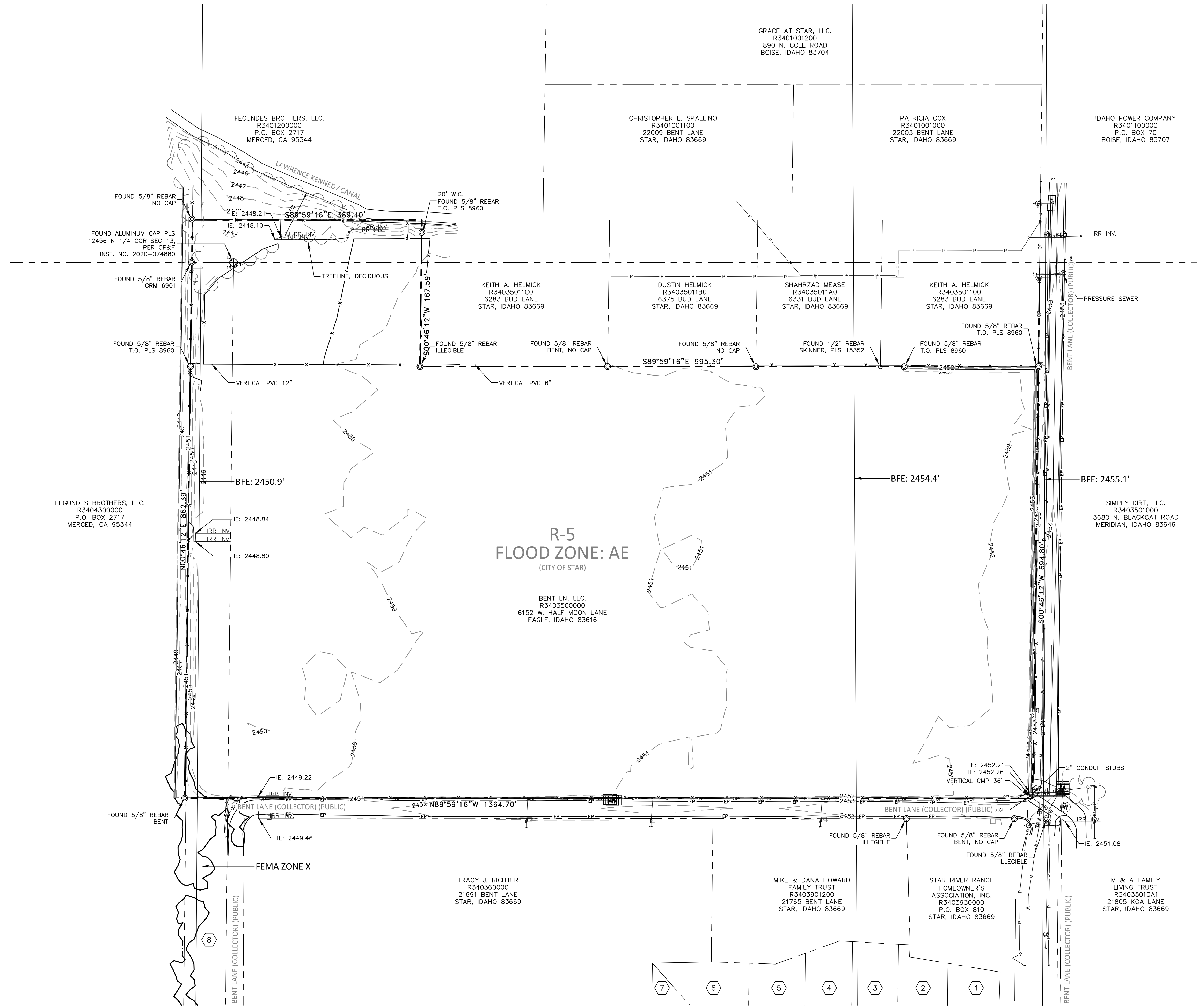
PRELIMINARY PLAT SHOWING LEGADO SUBDIVISION

A PORTION OF LAND IN THE SW 1/4 OF THE SE 1/4 AND THE SE 1/4 OF THE SW 1/4 OF SECTION 12, ALONG WITH A PORTION OF THE NW 1/4 OF THE NE 1/4 AND THE NE 1/4 OF SECTION 13, TOWNSHIP 4 NORTH, RANGE 2 WEST, BOISE MERIDIAN, CITY OF STAR, CANYON COUNTY, IDAHO
MARCH 2024

#	PARCEL NO.	PRIMARY OWNER	MAILING ADDRESS	CITY/STATE/ZIP
1	R3403711300	WPG STAR MEADOWS, LLC.	27271 LAS RAMBLAS	MISSION VIEJO, CA 92691
2	R3403711200	WPG STAR MEADOWS, LLC.	27271 LAS RAMBLAS	MISSION VIEJO, CA 92691
3	R3403711100	OAKMONT SIGNATURE HOMES, LLC.	2502 N. CONSTANCE PLACE	EAGLE, ID 83616
4	R3403711000	OAKMONT SIGNATURE HOMES, LLC.	2502 N. CONSTANCE PLACE	EAGLE, ID 83616
5	R3403710900	OAKMONT SIGNATURE HOMES, LLC.	2502 N. CONSTANCE PLACE	EAGLE, ID 83616
6	R3403710800	OAKMONT SIGNATURE HOMES, LLC.	2502 N. CONSTANCE PLACE	EAGLE, ID 83616
7	R3403710700	HALLMARK ENTERPRISES INC.	1895 N. LAKES PLACE	MERIDIAN, ID 83646
8	R3403400000	ERIC C. BATTAY	2420 MUNGER ROAD	STAR, ID 83669

LEGEND

- BOUNDARY LINE
- ADJACENT PROPERTY LINE
- ROAD CENTERLINE
- SECTION LINE
- WATER LINE (SIZE AS NOTED)
- SANITARY SEWER LINE
- GRAVITY IRRIGATION LINE
- OVERHEAD POWER LINE
- GAS LINE
- EDGE OF PAVEMENT
- EDGE OF DIRT
- REFERENCE MONUMENT
- FOUND 5/8" REBAR AS NOTED
- FOUND 1/2" REBAR AS NOTED
- FOUND BRASS CAP, NORTHEAST CORNER SECTION 12
- SANITARY SEWER MANHOLE
- FIRE HYDRANT
- WATER VALVE
- IRRIGATION CONTROL VALVE
- POWER POLE



FEGUNDES BROTHERS, LLC.
R3404300000
P.O. BOX 2717
MERCED, CA 95344

GRACE AT STAR, LLC.
R3401001200
890 N. COLE ROAD
BOISE, IDAHO 83704

CHRISTOPHER L. SPALLINO
R3401001100
22009 BENT LANE
STAR, IDAHO 83669

PATRICIA COX
R3401001000
22003 BENT LANE
STAR, IDAHO 83669

IDAHO POWER COMPANY
R3401100000
P.O. BOX 70
BOISE, IDAHO 83707

KEITH A. HELMICK
R3403501100
6283 BUD LANE
STAR, IDAHO 83669

DUSTIN HELMICK
R3403501100
6375 BUD LANE
STAR, IDAHO 83669

SHAHRAZAD MEASE
R3403501100
6331 BUD LANE
STAR, IDAHO 83669

KEITH A. HELMICK
R3403501100
6283 BUD LANE
STAR, IDAHO 83669

R-5
FLOOD ZONE: AE
(CITY OF STAR)

BENT LN, LLC.
R3403500000
6152 W. HALF MOON LANE
EAGLE, IDAHO 83616

SIMPLY DIRT, LLC.
R3403501000
3690 N. BLACKCAT ROAD
MERIDIAN, IDAHO 83646

TRACY J. RICHTER
R340360000
21691 BENT LANE
STAR, IDAHO 83669

MIKE & DANA HOWARD
FAMILY TRUST
R3403901200
21765 BENT LANE
STAR, IDAHO 83669

STAR RIVER RANCH
HOMEOWNER'S
ASSOCIATION, INC.
R3403930000
P.O. BOX 810
STAR, IDAHO 83669

M & A FAMILY
LIVING TRUST
R34035010A1
21805 KOA LANE
STAR, IDAHO 83669

PRELIMINARY PLAT - EXISTING CONDITIONS

Scale: 1" = 80'

**LEGADO SUBDIVISION
STAR, IDAHO
PRELIMINARY PLAT**

PROFESSIONAL ENGINEER
LICENSED
21761
03/20/24
STATE OF IDAHO
JEFFREY DOERSCH

km
ENGINEERING
5725 NORTH DISCOVERY WAY
BOISE, IDAHO 83713
PHONE (208) 639-6939
kmengllp.com

REVISIONS		
NO.	ITEM	DATE
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3	CHD4, CITY COMMENT REVISIONS	3/1/23
4	CHD4 COMMENT REVISIONS	5/9/23
5	IRRIGATION POND ADDITION	7/14/23
6	PRIVATE STREETS	3/20/24

DATE: MARCH 2024
PROJECT: 21-162
SHEET NO.
PP2.0

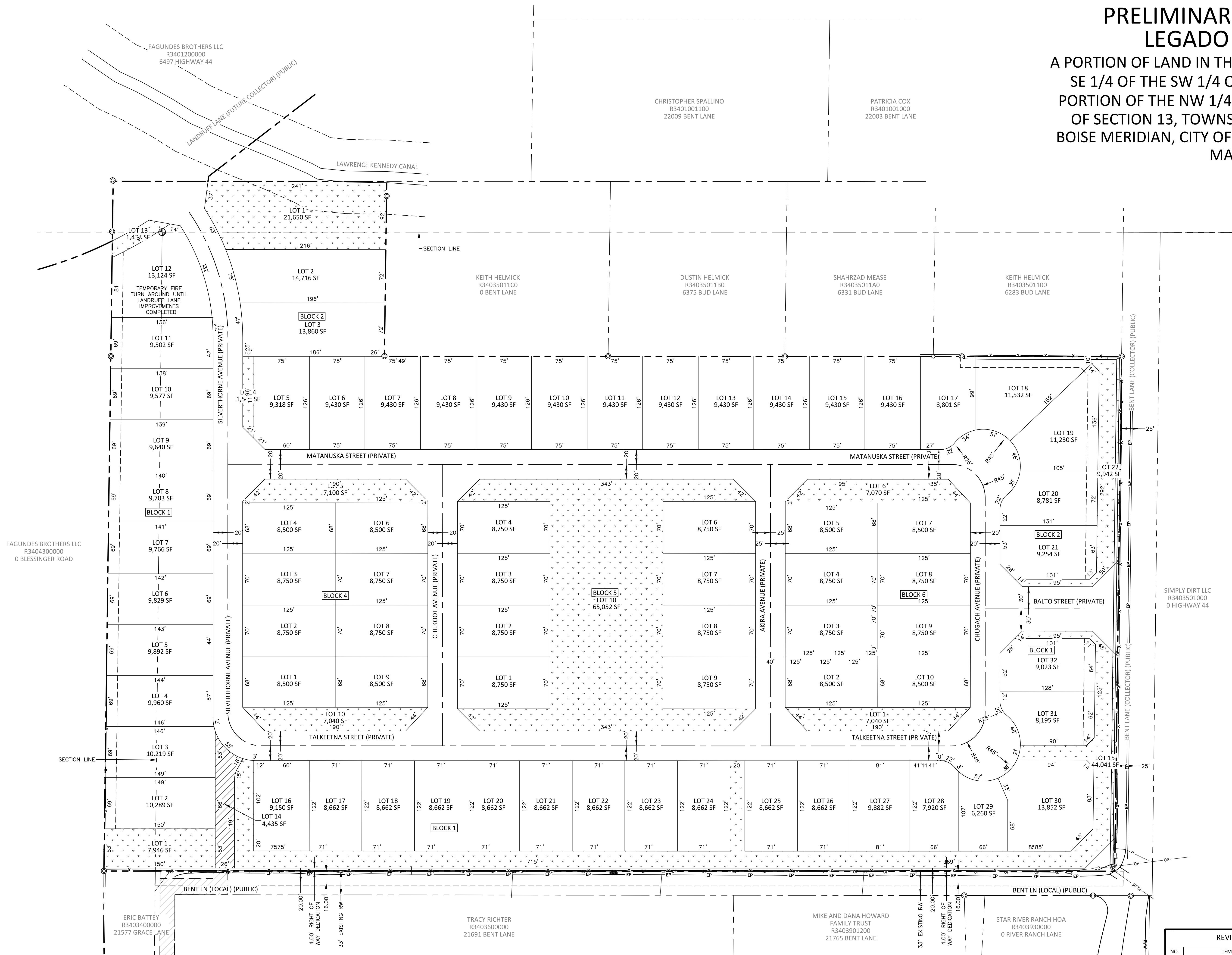
P:\31-LEGADO\PLAT\PRELIMINARY\31-LEGADO-PLAT-EXISTING CONDITIONS.DWG - REF EDDORSH, JDO/2024, CANYON IPTS (HW) PCL, ANKAL LPF

PRELIMINARY PLAT SHOWING LEGADO SUBDIVISION

A PORTION OF LAND IN THE SW 1/4 OF THE SE 1/4 AND THE SE 1/4 OF THE SW 1/4 OF SECTION 12, ALONG WITH A PORTION OF THE NW 1/4 OF THE NE 1/4 AND THE NE 1/4 OF SECTION 13, TOWNSHIP 4 NORTH, RANGE 2 WEST, BOISE MERIDIAN, CITY OF STAR, CANYON COUNTY, IDAHO
MARCH 2024

LEGEND

	BOUNDARY LINE
	ADJACENT PROPERTY LINE
	ROAD CENTERLINE
	SECTION LINE
	RIGHT-OF-WAY LINE
	LOT LINE
	EASEMENT LINE
	COMMON AREA
	REFERENCE MONUMENT
	FOUND 5/8" REBAR (SEE SHEET PP2.0)
	FOUND 1/2" REBAR (SEE SHEET PP2.0)
	FOUND BRASS CAP, NORTHEAST CORNER SECTION 12 (SEE SHEET PP2.0)



PRELIMINARY PLAT - LOT DIMENSIONS

Scale: 1" = 60'

REVISIONS

NO.	ITEM	DATE
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6	PRIVATE STREETS	3/20/24

**LEGADO SUBDIVISION
STAR, IDAHO
PRELIMINARY PLAT**

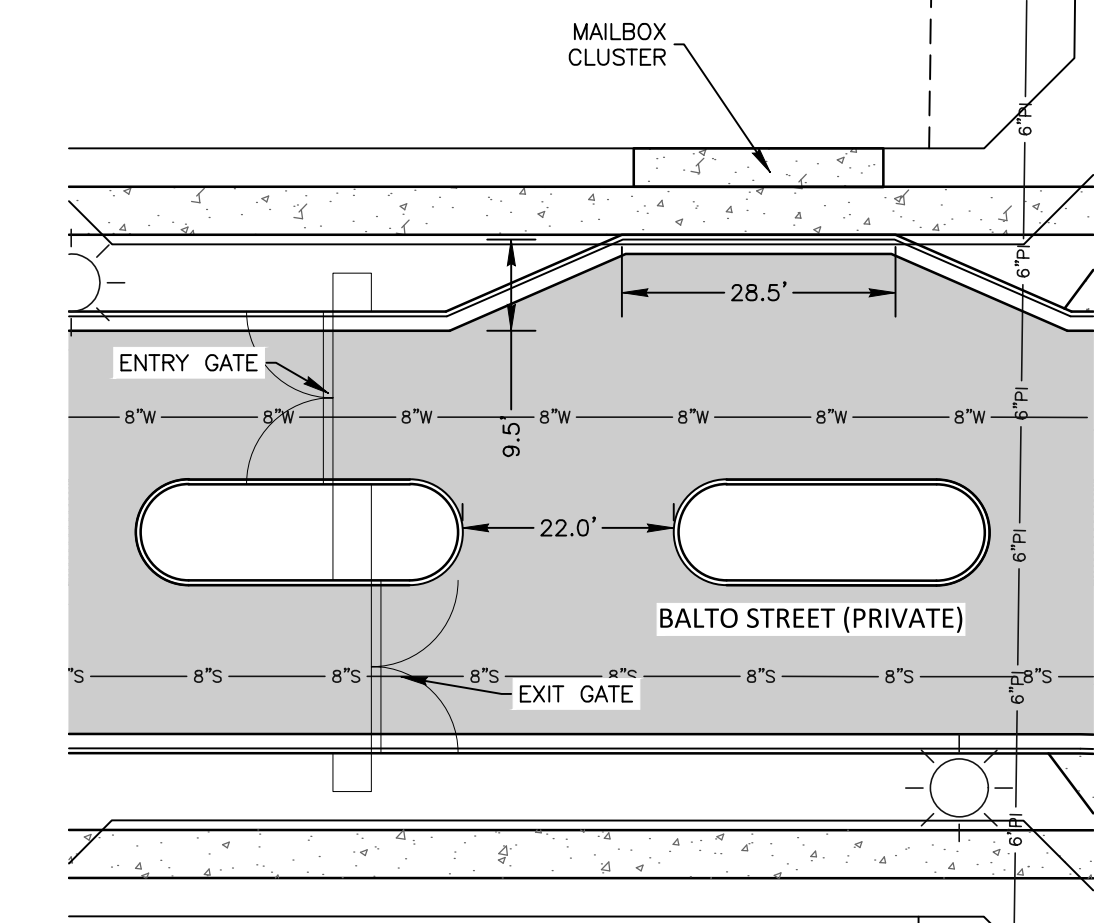
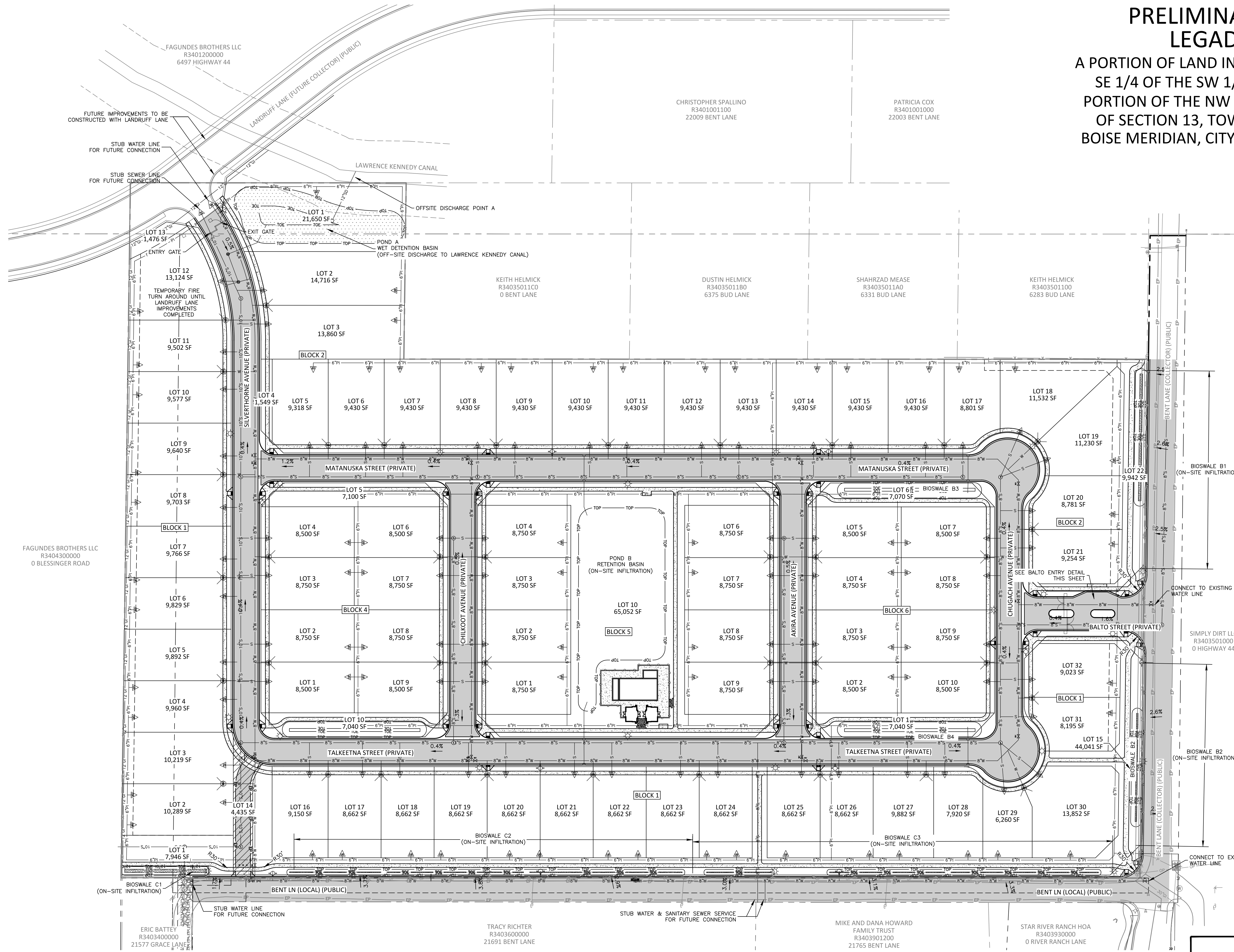
km
ENGINEERING
5725 NORTH DISCOVERY WAY
BOISE, IDAHO 83713
PHONE (208) 639-6939
kmengllp.com

DATE: MARCH 2024
PROJECT: 21-162
SHEET NO. PP3.0

P:\31-REV\ADV\PLAT\PRELIMINARY\31-162 LOT DIMENSIONS.DWG, REF: EDOERSC, 1/20/2024, CANYON 8P755 (R/W) PLS, 2/26/24, LPPH

PRELIMINARY PLAT SHOWING LEGADO SUBDIVISION

A PORTION OF LAND IN THE SW 1/4 OF THE SE 1/4 AND THE SE 1/4 OF THE SW 1/4 OF SECTION 12, ALONG WITH A PORTION OF THE NW 1/4 OF THE NE 1/4 AND THE NE 1/4 OF SECTION 13, TOWNSHIP 4 NORTH, RANGE 2 WEST, BOISE MERIDIAN, CITY OF STAR, CANYON COUNTY, IDAHO
 MARCH 2024



BALTO ENTRY DETAIL
 Scale: 1" = 20'

* SEE SHEET PP4.1 FOR STREET SECTIONS & LEGEND

**LEGADO SUBDIVISION
 STAR, IDAHO
 PRELIMINARY PLAT**

PROFESSIONAL ENGINEER
 LICENSED
 21761
 STATE OF IDAHO
 JEFFREY DOERSCH

km
 ENGINEERING
 5725 NORTH DISCOVERY WAY
 BOISE, IDAHO 83713
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 kmengllp.com

REVISIONS		
NO.	ITEM	DATE
0	INITIAL SUBMITTAL	4/5/22
1	ZONING REQUEST CHANGE	5/26/22
2	CHD4 COMMENT REVISIONS	11/17/22
3	CHD4, CITY COMMENT REVISIONS	3/1/23
4	CHD4 COMMENT REVISIONS	5/9/23
5	IRRIGATION POND ADDITION	7/14/23
6	PRIVATE STREETS	3/20/24

PRELIMINARY PLAT - PRELIMINARY ENGINEERING
 Scale: 1" = 60'

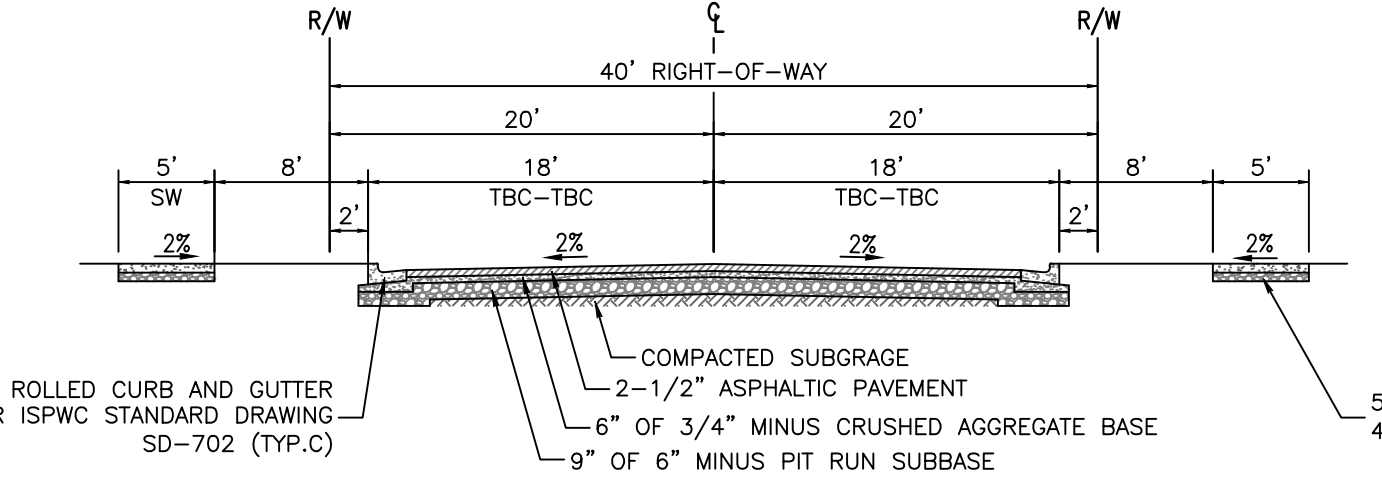
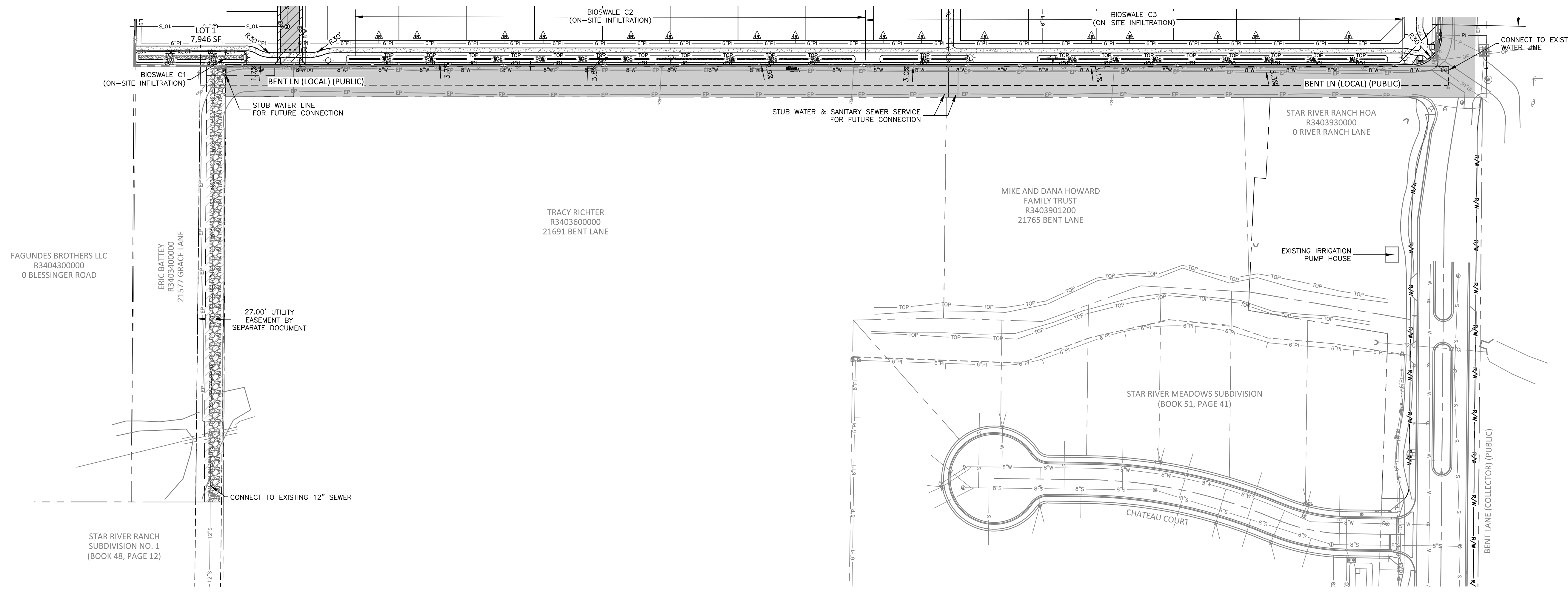
MATCH LINE - SEE SHEET PP4.1

PRELIMINARY PLAT SHOWING LEGADO SUBDIVISION

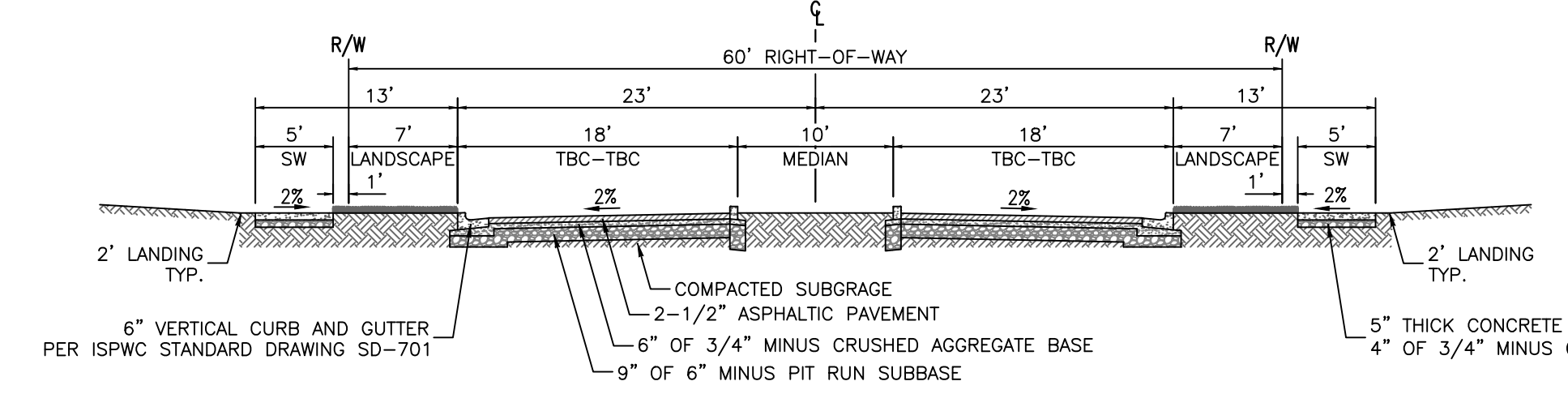
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MARCH 2024

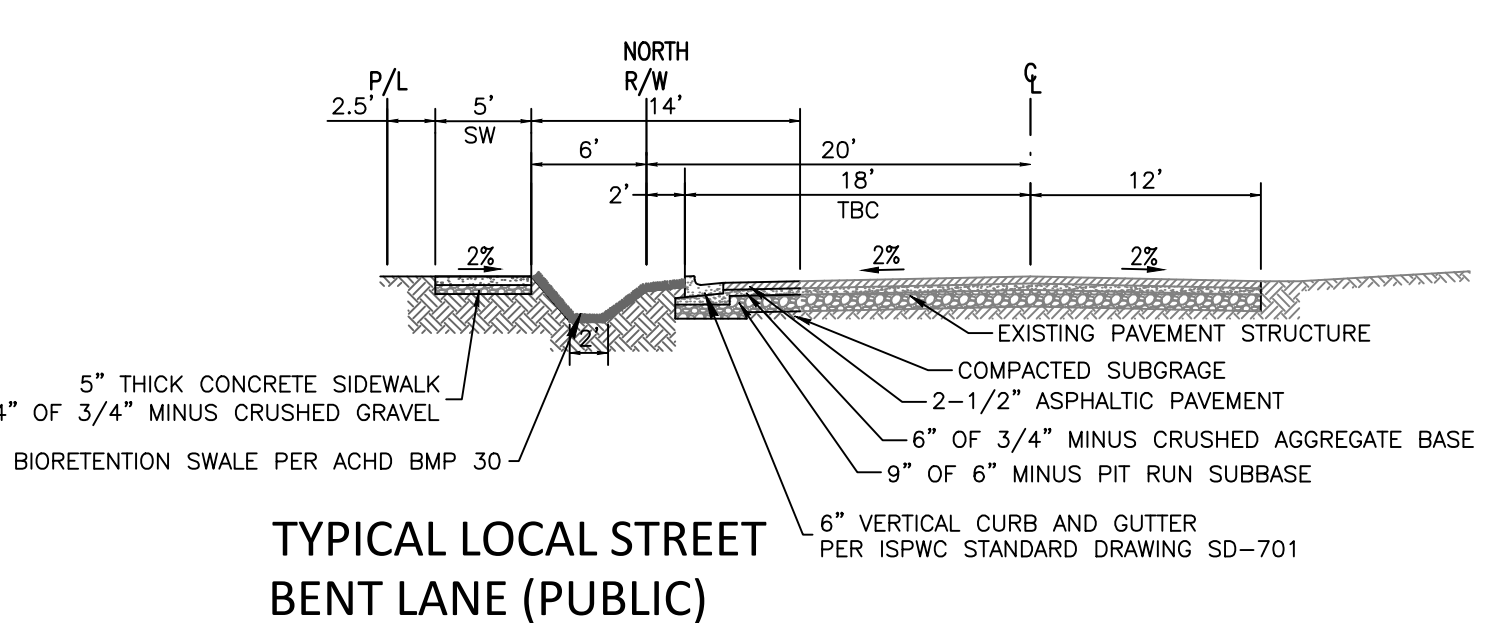
MATCH LINE - SEE SHEET PP4.0



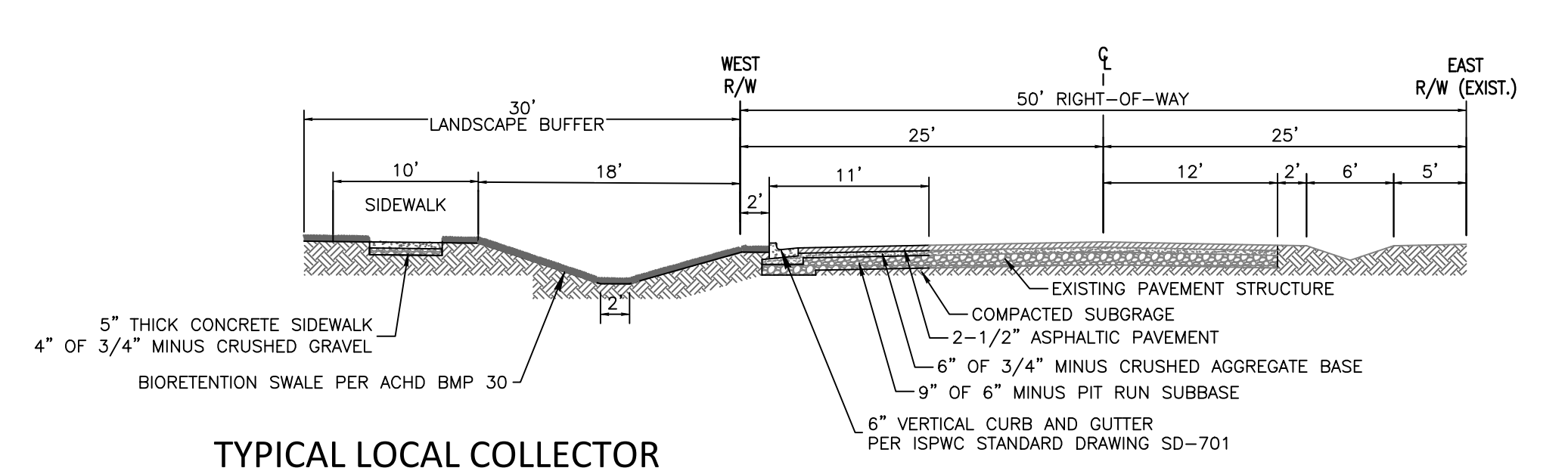
TYPICAL 40' ROW LOCAL STREET
 CHUGACH AVENUE, AKIRA AVENUE, CHILKOOT AVENUE,
 SILVERTHORNE AVENUE, MATANUSKA STREET, TALKETNA STREET
 (PRIVATE)
 NTS



TYPICAL 60' ROW LOCAL STREET
 BALTO STREET (PRIVATE)
 NTS



TYPICAL LOCAL STREET BENT LANE (PUBLIC)
 NTS



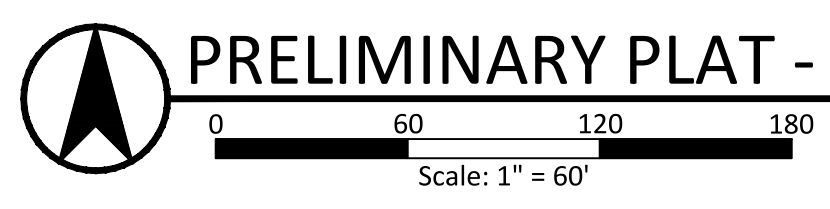
TYPICAL LOCAL COLLECTOR BENT LANE (PUBLIC)
 NTS

LEGEND

- BOUNDARY LINE
- - - ADJACENT PROPERTY LINE
- ROAD CENTERLINE
- - - SECTION LINE
- RIGHT-OF-WAY LINE
- - - LOT LINE
- - - EASEMENT LINE
- - - BUILDING SETBACK LINE
- - - SANITARY SEWER LINE
- - - WATER LINE
- - - STORM DRAIN LINE
- - - GRAVITY IRRIGATION LINE
- - - PRESSURE IRRIGATION LINE
- - - EDGE OF GRAVEL
- - - EDGE OF DIRT
- - - EDGE OF PAVEMENT
- - - OVERHEAD POWER
- - - GAS LINE
- - - SINGLE WATER SERVICE
- - - DOUBLE WATER SERVICE

- ⊙ SEWER MANHOLE
- ⊕ STORM DRAIN MANHOLE
- ⊕ STORM DRAIN INLET
- ⊕ WATER VALVE
- ⊕ WATER METER
- ⊕ FIRE HYDRANT
- ⊕ IRRIGATION BOX
- ⊕ LIGHT POLE

- Common Area
- Asphalt Roadway
- Retention/Detention Basin per Plan



PRELIMINARY PLAT - PRELIMINARY ENGINEERING

LEGADO SUBDIVISION
STAR, IDAHO
PRELIMINARY PLAT

PROFESSIONAL ENGINEER
 LICENSED
 21761
 03/20/24
 STATE OF IDAHO
 JEFFREY DOERSCH

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5	IRRIGATION POND ADDITION	7/14/23
6	PRIVATE STREETS	3/20/24

DATE: MARCH 2024
 PROJECT: 21-162
 SHEET NO.
PP4.1



Star Fire Protection District

DATE: October 7, 2024

TO: City of Star – Planning and Zoning

CC: KM Engineering, LLP – Stephine Hopkins

FROM: Victor Islas, Deputy Chief

SUBJECT: Fire District Comment

PROJECT: Legado Subdivision (Star River Ranch North)
Preliminary Plat Modification and Private Roads

The Star Fire Protection District has reviewed the documents provided by the City of Star for our review on the Preliminary Plat Modification and Private Road applications for Legado Subdivision, comments are as follows.

Original Application Overview – The Applicant is seeking approval of a Preliminary Plat for a proposed residential subdivision consisting of 71 single-family residential lots and 12 common lots. The property is located on the west side of Bent Lane in Star, Canyon County, Idaho and consists of 23.77 acres with a proposed density of 2.99 dwelling units per acre. The property is currently zoned Residential R-5.

New Application Overview- The property is approximately 23.77 acres located south of State Highway 44 and west of Bent Lane. A preliminary plat for 71 single-family residential lots and 12 common lots served by internal public roadways was approved on March 21, 2023. Since the development received approval in 2023, the Developer has determined that they would like to utilize private roadways within the subdivision. As such, a preliminary plat modification and private road application are now proposed for the subdivision.

This development will be serviced by Station 51 located at 11665 W. State St., Star, ID. Station 51 is 1.3 mile(s) from the development entrance with an estimated 3-minute travel time under normal driving conditions.

The Fire District level of service expectation goal is 5 minutes. This development sits within the Fire District service level expectations when resources are available.

Due to the fire district's limited authority to enforce regulations on private roads, there are reservations regarding approval of the request.

In addition to the original review conditions dated March 1, 2023, the following provisions outlined below will also apply if approved the request is approved by the City of Star.



Star Fire Protection District

CONDITIONS OF APPROVAL:

1. Fire Apparatus Access

- a. Access roads shall be provided and maintained following Appendix D and Section 503 of the IFC. Access shall include adequate roadway widths, signage, turnarounds, and turning radius for fire apparatus.
 - a. Access road design shall be designed and constructed to allow for evacuation simultaneously with emergency response operations.
 - b. Emergency Services easement shall be across all access roads.
 - c. Developments of one- or two-family *dwelling units* where the number of *dwelling units* exceeds 30 shall be provided with two separate and *approved* fire apparatus access roads.
 - i. The applicant must demonstrate how they will meet the intent of the code due to the future construction and completion of Landruff lane.
 - d. This project shall comply with the fire districts standards for Gates & Barriers across Fire apparatus access roads. The contractor installing the gate is required to obtain construction permits via the Fire District.

2. Additional Comments

- a. CCNR note shall include that any changes to the road system within the subdivision will need to be approved by the fire district.
- b. CCNR note – It shall be the responsibility of the HOA to maintain all fire apparatus access roads clear of obstruction at all times including snow during winter months.
- c. Any overlooked hazardous condition and/or violation of the International Building and/or Fire Code does not imply approval of such condition or violation.



MIDDLETON RURAL FIRE DISTRICT

STAR FIRE PROTECTION DISTRICT

DATE: March 1, 2023

TO: City of Star – Planning and Zoning

FROM: Victor Islas, Deputy Chief

SUBJECT: Fire District Review (23MS-052)

PROJECT NAME: Star River Ranch North Subdivision
Files: RZ-22-01, DA-22-03, PP-22-05

Fire District Summary Report:

1. **Overview** This development can be serviced by the Star Fire Protection District. This development shall comply with the 2018 International Fire Code (IFC) and any codes set forth by the City of Star, Idaho.
2. **Fire Response Time:** This development will be served by the Star Fire Protection District Station 51, located at 11665 W. State St., Star, Idaho 83669. Station 51 is 1.3 miles with a travel time of 3 minutes under ideal driving conditions to the purposed entrance off Bent Ln.
3. **Side Setback:** *Side Setback for R-3 of 7.5^{'(2)} as per Star City Code with no modification.*
4. **Accessibility:** Roadway Access, Traffic, Radio Coverage
 - a. Access roads shall be provided and maintained following Appendix D and Section 503 of the IFC. Access shall include adequate roadway widths, signage, turnarounds, and turning radius for fire apparatus.
 - b. Access road design shall be designed and constructed to allow for evacuation simultaneously with emergency response operations.
 - c. All access roads in this development shall remain clear and unobstructed during construction of the development. Additional parking restrictions may be required as to always maintain access for emergency vehicles. Hydrants shall always remain unobstructed per city code.
 - d. An unobstructed vertical clearance of no less than 13 feet 6 inches shall be always maintained.
 - e. *Flag Lot (Block 2 Lot 3) Residential structures that sit more than 150ft off the public roadway will require additional emergency turn-around on the property.*
 - f. One- or two-family dwelling residential developments: Developments of one- or two-family dwellings where the number of dwelling units exceeds 30 shall provide with at least two separate and approved fire apparatus access roads.
 - i. *The purposed access into the development meets the intent of the Fire Code for access.*

Project Name: Star River Ranch North



MIDDLETON RURAL FIRE DISTRICT

STAR FIRE PROTECTION DISTRICT

- g. *Temporary turn around will be required at the end of “Silverthorne Avenue”. The temporary turn around can be abandoned once connection is made to the future purposed road. Turn around to follow IFC Fire Apparatus Access Roads Appendix D.*

5. Premises/Site Identification:

- a. All residential, commercial, and industrial buildings within the City shall have approved address numbers, building numbers, or approved building identification placed in a position that is plainly legible and visible from the street or road fronting the property. These numbers shall contrast with their background. When required by the fire code official, address numbers shall be provided in additional approved locations to facilitate emergency response.
- b. The applicant shall work with Ada County, City of Star and Star Fire Protection District to provide an address identification plan and signage which meets the requirements set forth by each agency. Addressing shall be placed in a position that is plainly legible and visible from the street or road fronting the property, as set forth in International Fire Code Section 505.1
- c. Address numbers shall have a minimum stroke width of one-half inch (0.5”), and of a color contrasting with the background. The required height of each address number shall be calculated by the distance of the addressed building from the road.
 - i. *This residential development will be required to have 6” address numbers.*
- d. Upon commencement of initial construction of a new structure, a clear visible freestanding sign or post shall be erected and maintained in place until the permanent address numerals are attached or otherwise displaced upon the premises at completion.

6. Water Supply: Water supply requirements will be followed as described in Appendix B of the 2018 International Fire Code unless agreed upon by the Fire District.

- a. Fire Flow: One- and two-family dwellings not exceeding 3,600 square feet require a fire-flow of 1,000 gallons per minute for a duration of 1 hours to service the entire project. One- and two-family dwellings in excess of 3,600 square feet require a minimum fire flow as specified in Appendix B of the International Fire Code
- b. Water Supply: Acceptance of the water supply for fire protection will be by the Fire District and water quality by Star Sewer & Water for bacteria testing.
- c. Water Supply: Final Approval of the fire hydrant locations shall be by the Star Fire Protection District or their designee in accordance with International Fire Code Section (IFC) 508.5.4 as follows:
 - i. Fire hydrants shall be located along the public right-of-way or along the emergency apparatus access roads, preferably at intersections or on islands separating parking areas which cannot be obstructed by parked vehicles. Hydrants in areas subject to physical damage shall be protected from collision.
 - 1. *Locate the fire hydrant at the intersection of the property line and the roadway right of way or property line.*

Project Name: Star River Ranch North



MIDDLETON RURAL FIRE DISTRICT

STAR FIRE PROTECTION DISTRICT

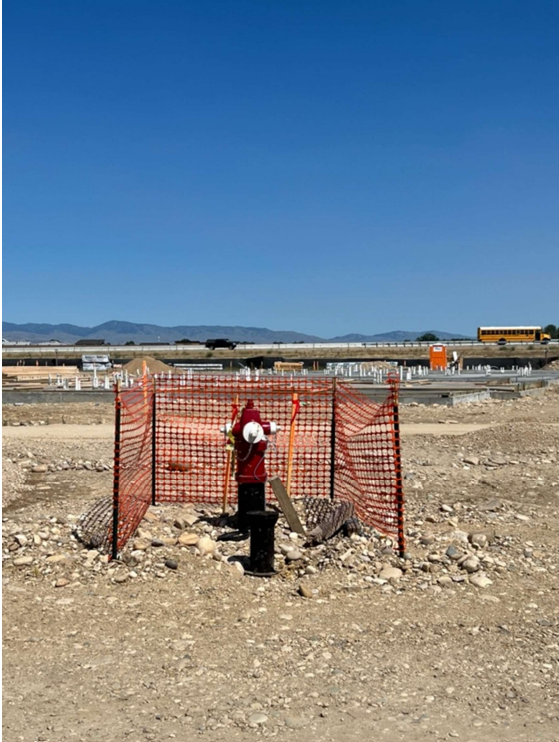
- ii. Fire hydrants shall have a locking Storz LDH connection in place of the 4 ½” outlet. The Storz connection may be integrated into the hydrant, or an approved adapter may be used on the 4 1/2" outlet.
 - iii. Fire hydrants shall have the Storz outlet face the main street or parking lot drive aisle.
 - iv. Fire hydrants shall be placed on corners when spacing permits.
 - v. Fire hydrants shall not have any vertical obstructions to outlets within 10’.
 - vi. Fire hydrants shall be placed 18” above finished grade to the center of the Storz outlet.
 - vii. Fire hydrants shall be provided to meet the requirements of Star Sewer & Water Standards.
 - viii. Show all proposed or existing hydrants for all new construction or additions to existing buildings within 1,000 feet of the project.
 - ix. Fire hydrant relocations to be approved by the Fire District & Star Sewer and Water.
 - x. Hydrants are to always remain clear and unobstructed.
 - xi. Hydrants to be marked with temporary fencing creating a 3 ft clearance around the hydrant and shall remain in place until approved by fire district. *See exhibit 1.*
 - xii. Developer to review landscape plans to ensure landscaping will not obstruct hydrants.
7. **Inspections:** Final inspection by the Fire District of the above listed including hydrant flow must be completed before building permits are issued.
8. **Additional Comments:**
- a. Streetlights shall be turned on once residential building begins, Lighting is essential in assisting first responders with identifying entrances safely while responding to calls for service.



MIDDLETON RURAL FIRE DISTRICT

STAR FIRE PROTECTION DISTRICT

Exabit 1



Project Name: Star River Ranch North

From: Timothy Eck <timothyeck@me.com>
Sent: Wednesday, January 24, 2024 12:52 PM
To: Shawn Nickel <snickel@CityofStar.onmicrosoft.com>
Cc: Ryan Field <rfield@CityofStar.onmicrosoft.com>; McCarthy Kevin <kevin@kmengllp.com>; Stephanie Hopkins <shopkins@kmengllp.com>
Subject: Re: Pre-App

Thank you for the followup. We will proceed accordingly and will be glad to meet with fire and police.

On Jan 24, 2024, at 11:26 AM, Shawn Nickel <snickel@staridaho.org> wrote:

Tim. Ryan is correct. We have been having internal discussions with staff, including police and fire on the negatives associated with private streets, especially within smaller lot subdivisions. I believe I did relay some of these concerns to you yesterday. As I stated, staff and Council have had discussions about this issue and we are working on current Code changes, and private streets will be discussed. That being said, the Code currently does not prohibit private streets from being proposed, although they will ultimately be approved by Council. Therefore, you may request a modification of your previously approved preliminary plat. Until we have time to review the request and get feedback from Fire and Police, along with other staff input, it's hard to say what our final recommendation will be. I understand there are differences in your project from others approved with private streets, and we will consider that as well. My suggestion would be to work with Police and Fire, along with staff, and try to address those concerns prior to the City Council hearing. Also keep in mind, the Code changes are going to be up to the Council for review and approval in the next few months.

Please let me know if you have any further questions.

Thanks.

Shawn L Nickel
City of Star Planning Director

Sent from my Verizon, Samsung Galaxy smartphone
Get [Outlook for Android](#)

From: Timothy Eck <timothyeck@me.com>
Sent: Wednesday, January 24, 2024 7:57:48 AM
To: Shawn Nickel <Snickel@staridaho.org>
Cc: McCarthy Kevin <kevin@kmengllp.com>; Stephanie Hopkins <shopkins@kmengllp.com>
Subject: Pre-App

Sean

Thank you for meeting with me yesterday. As you suggested we will submit an application for Preliminary Plat modification.

We have delayed submitting this application since mid December assuming a pre-application meeting was necessary and have not been able to obtain a date for a pre-app meeting due to the holidays.

I am assuming that our brief meeting yesterday qualifies as the pre-application meeting and we can proceed with the application.

At 6:13 last night Stephanie received an email from Ryan Field (you were copied) that seems to contradict the direction you gave me yesterday. I attended the CHD4 final plat meeting and heard all the plus and minuses of the private roads in Boise Hunter Homes project and understand the concerns of law enforcement on private roads for traffic concerns but theirs is a completely different type of product. They have mid mile collectors including Dandruff Ln that remain public and public open space that requires public access through private roads. Our conditions are completely different than theirs. We are a small 24 acre enclave that has no reason for through traffic. Our plan would be to change the Silverthorn Avenue extension to Alpenglow Street to a bollarded emergency vehicle ingress/egress connection. This will mitigate unnecessary traffic on Alpenglow and keeping Alpenglow Traffic connected to Bent Ln as it currently is and preventing Alpenglow traffic from cutting through Silverthorn Avenue. The Balto Street connection to Bent Lane and Silverthorn Avenue connection to Landruff Lane would be gated. We want to promote a higher end upscale Gated community and with the two accesses gated there would be no through traffic. All traffic would be local residence traffic. With private roads I believe we can post slower speed limits than public roads and actually provide violation fines and enforcement to the HOA. We are fully aware that the road maintenance would then fall on the HOA and will have that addressed in the CC&R's.

Please advise if your direction given yesterday has changed.

Sincerely

Tim Eck



City of Star
Pathway
Master Plan



City of Star Pathway Master Plan

Acknowledgments

The project team would like to acknowledge the following people for their leadership and efforts during the development of the Pathways Master Plan:



Star Pathways Master Plan Project Team

- Ryan Morgan, City Engineer
- Tim Clark, Assistant City Engineer
- Shawn Nickel, Planning Director and Zoning Administrator
- Ryan Field, Assistant City Planner
- Dana Partridge, Public Information Officer

Thank you to all city staff and city departments that contributed to this plan.

Mayor and City Council

- Mayor Trevor Chadwick
- Council Member Jennifer Salmonsén
- Council Member David Hershey
- Council Member Kevin Nielsen
- Council Member Kevan Wheelock

Transportation and Pathway Committee

- John Tensen
- Steve Burton
- Jon Turnipseed
- Richard Girard
- Chris Todd

Consultant Team

Kimley-Horn

- Tim Nicholson
- Nicolette Womack
- Marissa Pellegrini
- Lars Erickson
- Tate Larsen
- Amanda Aiello
- Makena Gove

Logan Simpson Design

- Dan Arseneau

Special Thanks

to all the City of Star residents who shared their unique perspectives on what makes Star great, how the community can connect, and how it can be even better in the future. Thank you for taking the time to attend public meetings, participate in focus groups, complete surveys, and utilize the website feedback tools. Your input and passion for the Pathway Master Plan has directly influenced the recommendations set forth in this plan.

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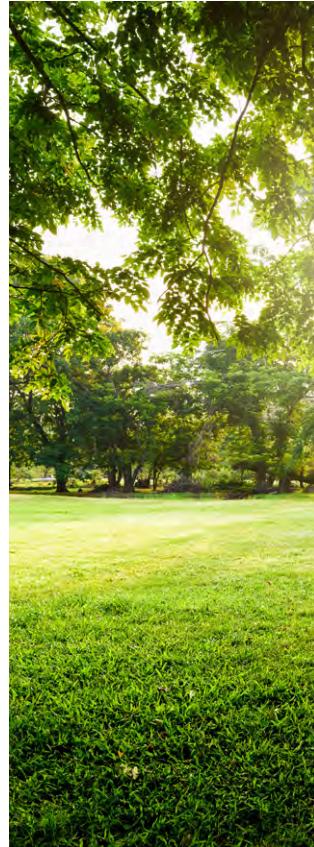
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01

INTRODUCTION



Goals of the Plan:

The City of Star Pathway Master Plan has been created to consolidate the work done to date, detail the various pathway types, their locations, materials, and outline a system of supportive amenities. These plans have been further envisioned through the included Maintenance Plan and Capital Improvement Plan (CIP) for the implementation and regular maintenance of the trail system.





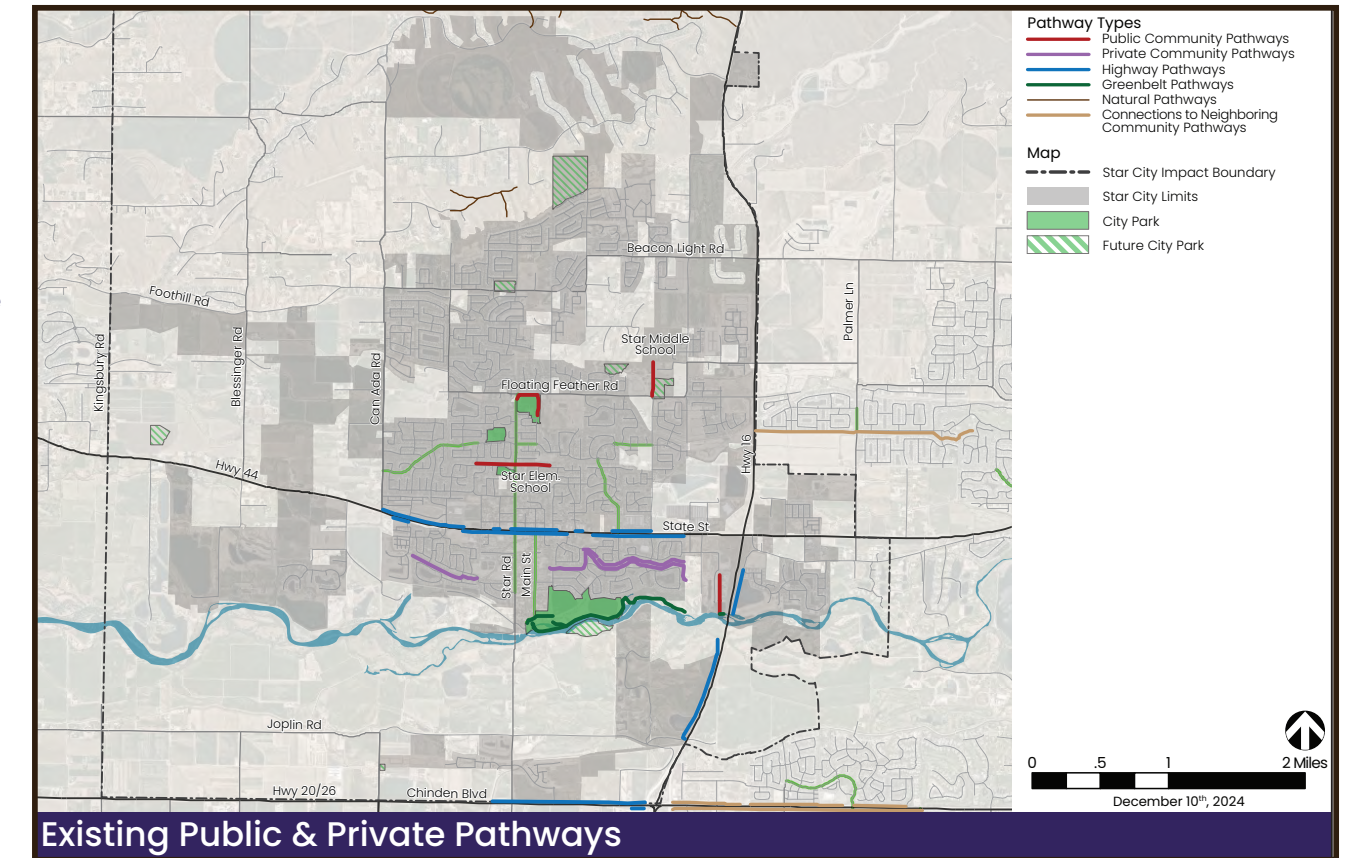
Importance of Pathways & Trails

Pathways and trails are an important asset to cities due to their multifaceted benefits. They contribute to the livability, sustainability, connectivity and vibrancy of a city in several impactful ways:

- **Community:** These spaces serve as communal areas where people can gather, interact, and engage in various activities. They foster a sense of community, promote social interactions, and provide venues for gatherings of any kind. These spaces enhance civic engagement and allow people to feel a sense of place in their communities.
- **Connectivity:** Pathways and trails enhance urban connectivity by linking various neighborhoods, parks, and key locations. Improved connectivity makes it easier for residents to access essential services, cultural sites, and recreational areas, thus fostering greater integration and cohesion within the city and having a positive effect on local economies by boosting tourism and foot traffic to nearby businesses.
- **Health Benefits:** Pathways and trails greatly contribute to public health by promoting physical activity. They provide convenient and accessible areas for exercise, such as walking, jogging, and cycling. Engaging in these activities can lower the risk of chronic illnesses, enhance mental well-being, and support a healthier lifestyle for people of all ages in the city.
- **Transportation Alternatives:** Pathways and trails provide important alternatives to driving, offering safe and convenient routes for walking, biking, and other forms of non-motorized transportation. This shift helps alleviate traffic congestion, contributing to better air quality and a lower overall greenhouse gas emission level. Providing alternative modes of transportation is more equitable and improves access to opportunities by connecting various parts of the city and bridging gaps between different neighborhoods including underserved or economically disadvantaged areas.
- **Environmental Benefits:** Green pathways and trails integrate natural elements into urban settings, which can mitigate the heat island effect, manage stormwater runoff more effectively, and support local flora and fauna. This contributes to a healthier urban ecosystem and promotes sustainability whilst providing an enjoyable way for people to get around.

Existing Pathway Network

The City of Star has contemplated a future city-wide pathway network for several years, the following section includes a summary of the guiding documents and previous plans completed to date. **As of 2024, there are approximately 17.7 miles of public trails within the City trail network, 8.5 miles of hard surface trails and 9.2 miles of natural surface trails.** An additional 2.6 miles of privately owned and maintained trail also exists within city limits. The existing hard surface trails are located beside state and federal highways, spread throughout neighborhoods, and along the Boise River while natural surface trails are concentrated on large, open parcels north of town. Existing trail segments are largely disjointed and lack connection to other trail segments, public amenities like schools and parks, and planned locations of connecting trails from neighboring communities. The continued development of the Star Pathway network aims to fill in these missing connections with the goals of enhancing mobility and safety, promoting sustainability, and fostering community well-being. Another key goal is to ensure existing private pathways and sidewalks are better integrated within the larger public pathway system. By creating well-designed pathways, the City can provide residents with safe and convenient routes for walking, biking, and other forms of active transportation, reducing reliance on motor vehicles and alleviating traffic congestion and emissions. A connected pathway system also encourages social interaction and physical activity, contributing to a healthier and more engaged community. Ultimately, the goal is to create a more accessible, eco-friendly, and vibrant community with an enhanced quality of life for all residents.





Alignment with Key Planning Documents

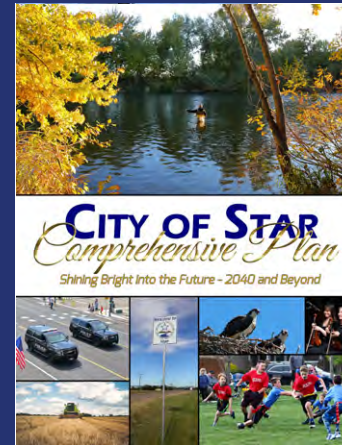
ACHD City of Star Bicycle and Pedestrian Plan (2018)

The ACHD City of Star Bicycle and Pedestrian Plan aimed to create a practical bicycle and pedestrian plan for each city within Ada County's specific community needs. The primary purpose of this plan was to "identify community priorities for future bicycle and pedestrian projects within the planning area." The plan has four specific goals: Increase the safety and convenience of walking and bicycling; improve facilities to meet the needs of people from all age groups; enhance mobility to meet accessibility standards; create economic development opportunities; and enrich the walking and bicycling environment to attract visitors. The plan also provided recommended projects with priority levels based on the Needs Analysis performed for the City of Star. This includes expanding existing bike facilities, adding paved multi-use pathways, and adding sidewalks along roads to connect crossings and developments.

[Link Here](#)

City of Star Comprehensive Plan (2019 Approved, Updated 2020-2022)

The City of Star Comprehensive Plan outlined specific goals for expanding the local and regional pathway system. The first goal was to encourage the development of pathways to provide "basic mobility for some and a viable transportation option of all others", specifying that future subdivisions could be required to provide pathways that connect to facilities open to the public. A second goal was to integrate planned pathways to the SH-44, SH-16, and US-20/26 corridors. Additionally, specifically within the South of the River Subarea, there was an emphasis on promoting connectivity throughout the area by using facilities such as natural resource areas, the Boise Greenbelt extensions, and existing irrigation canals and ditches. Furthermore, the plan outlined a diverse array of pathway descriptions tailored to meet the community's needs and enhance connectivity throughout the city; these included Greenways, Natural Pathways, Community Paths, Highway Paths, Greenbelt Paths, Side-paths, On-Street Bikeways, and All-Terrain Bike Trails, Cross-Country Ski Trail, and Equestrian Trails.



[Link Here](#)



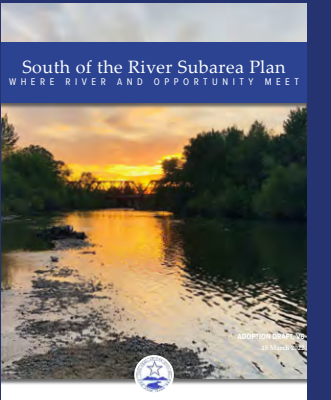
City of Star Pathway Workshop & Master Plan Maps (2021-2024)

The volunteer Transportation and Pathway Committee (TPC) has served as a vital advocate in the community planning for this amenity network. Their presentation given before City Council on December 21, 2021, included an overview of pathway plan locations, purposes, types, legal consideration, order of magnitude costs for construction/maintenance and possible funding sources. The follow up City of Star Pathway Master Plan map featured 66 miles of pathways throughout Star including approximately 31 miles of community pathways within developments and neighborhoods, typically along ditch easements, 13 miles of greenbelt pathways along the Boise River and 22 miles of Highway pathways along US Highway 20/26, State Highway 44, and State Highway 16. The preliminary pathway map has been established and approved by the Star City Council. This plan has been utilized to secure pathway agreements with the Middleton Mill Ditch Company and Middleton Irrigation Association. These agreements include standard language for allowing the City to condition new development to construct, install and maintain pathways and related landscaping and improvements within the Ditch Company's easement or right-of-way subject to their review and approval. They also outline inclusion of the Ditch Companies within the Master Pathway Plan process.

South of the River Subarea Plan (2022)

The City of Star's South of the River Subarea Plan consolidated planning efforts related to the formation of a unique, community-driven space south of the Boise River to Chinden Road and between State Highway 16 (SH 16) and the north extension of Can-Ada Road. Overall, the plan lays out a vision for the area sticking to the goal of "planning for the Boise River, a new riverfront center, focusing on its adjacency to water and the natural environment, creating connected communities, and developing new family-friendly neighborhoods."

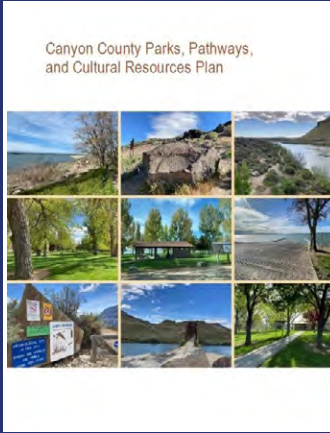
[Link Here](#)



ACHD Roadway to Bikeways Plan (2018 Addendum)

This addendum modernizes the 2009 plan by incorporating recent plans and advances in the state-of-the-practice into the following components: Goals, Objectives, and Performance Measures; Bicycle Program Status Report; Bicycle Facility Selection Matrix and Definitions; Prioritization Criteria; Planned Bicycle Network Maps, which includes the Regional Low-Stress Bikeway Network.

[Link Here](#)



Canyon County Parks, Pathways and Cultural Resources Plan (2022)

This plan aimed to help the County take strategic action to improve park assets and connectivity for all users. Guiding priorities were created to help Canyon County leverage resources and identify opportunities for investment that result in improved services for Canyon County residents. A vision and set of associated goals set the foundation to ensure long-term health and continued improvement of parks, open spaces, trails and cultural assets throughout all of unincorporated Canyon County. Near-term actions and investments were also identified. The Boise River Greenbelt including the section through Star was identified as a Priority Investment Area. Assets and benefits listed included regional connectivity, expanding access to existing fishing/riverside recreational amenities, backbone bicycle/pedestrian infrastructure, connectivity to cultural asset, preservation of habitat and increased river/water protection.

[Link Here](#)



Pre-Concept Report Star Greenbelt (2023)

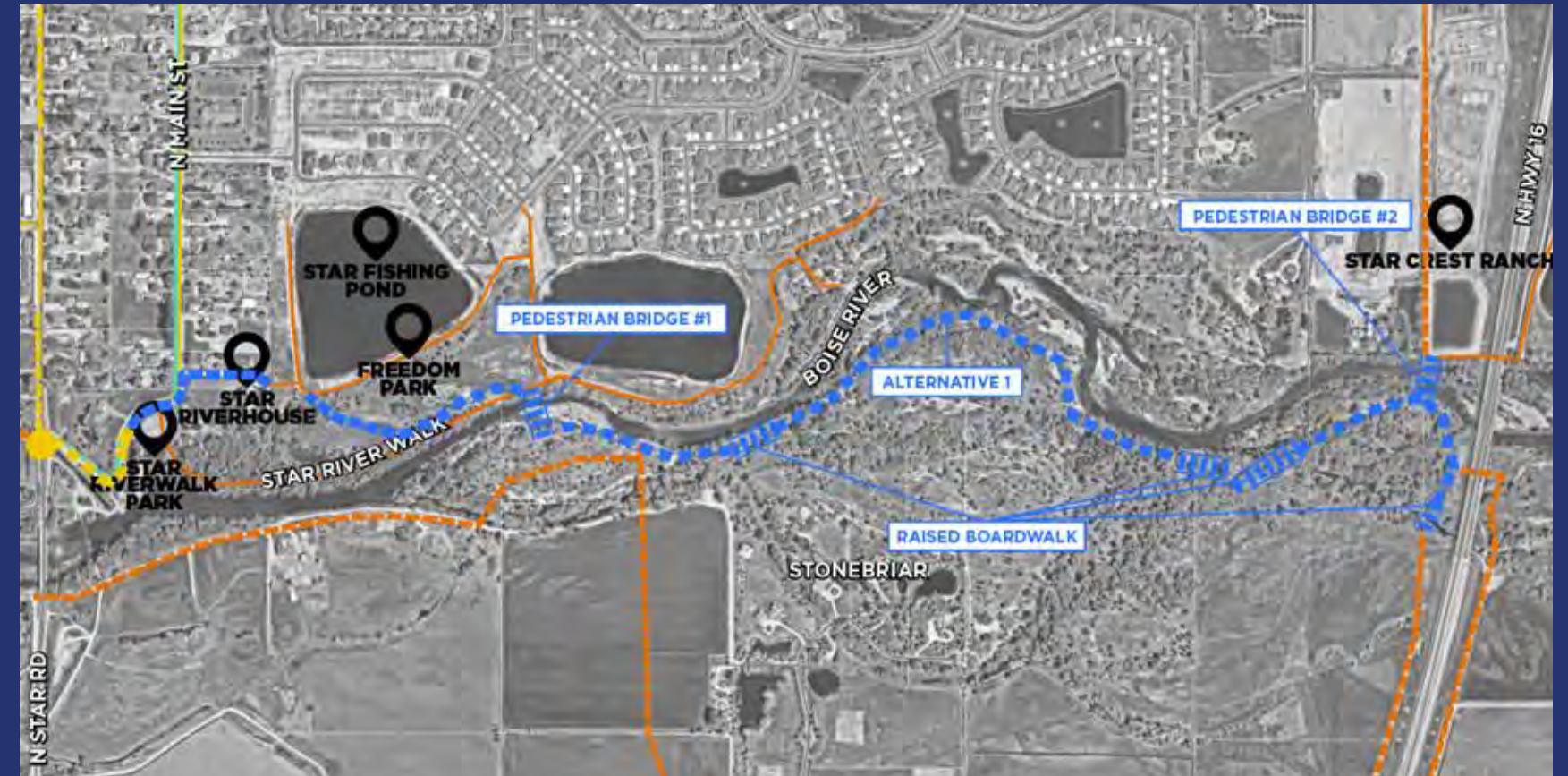
This report was focused on constructing a trail which will establish the first leg of the regional Boise River Greenbelt system within the City limits. The study area included approximately 1.5 miles along the Boise River from Star Road to the west and SH-16 to the east. The trail consisted of a 10-foot wide paved multi-use section with 5-foot shoulders on either side. A preferred Alternative was selected below with guidance given on ROW acquisition/easement needs and estimated costs.

Ada County Parks & Open Space Master Plan (2007, currently updating)

This plan established goals and policies needed to ensure that adequate resources would be available to meet current and future needs for parks/open space land, facilities and services. The document is broken into existing resources, recreation demand, roles and responsibilities, needs assessment and service level standards, recommendations and implementation. Specific to the City of Star it included the Boise River Greenway Extension and connections to the North Foothills area. The plan is currently undergoing an update and has limited information regarding the City of Star Impact Area, it is important the City of Star continue to advocate for future Ridge to Rivers trail connections which will likely come through the City of Eagles access points.



[Link Here](#)

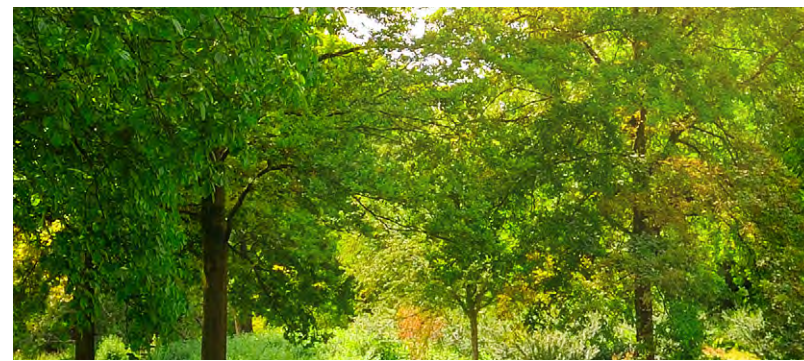


02

COMMUNITY NEEDS

Public Engagement Process

Project timeline

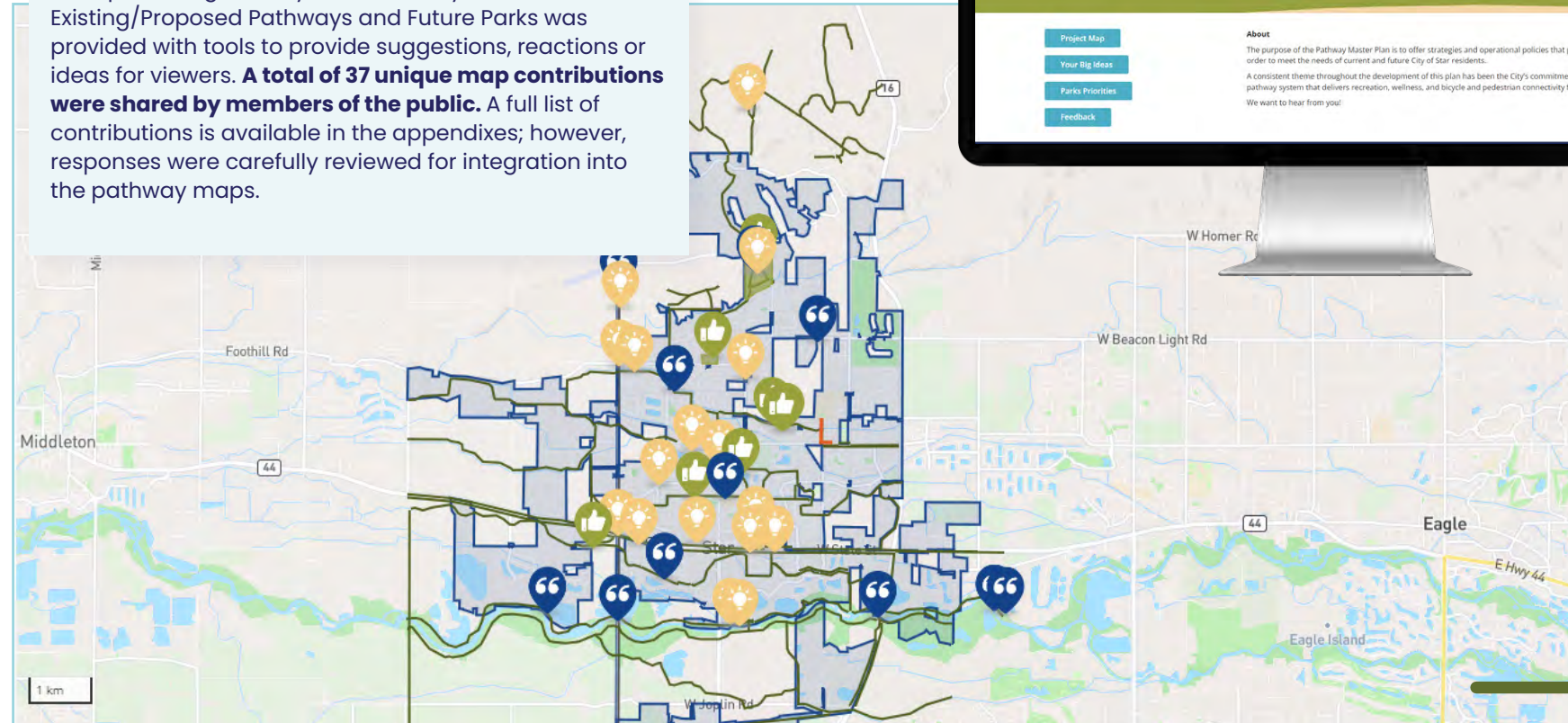
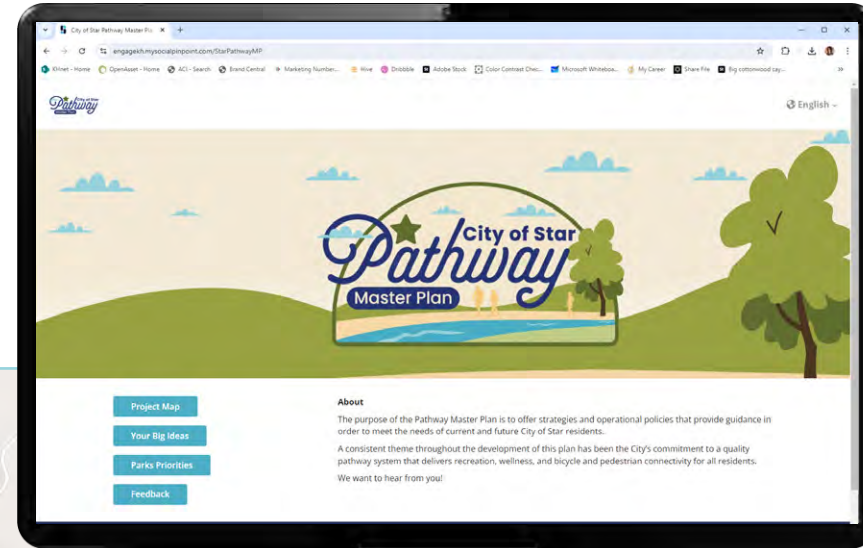


Key Findings

Engagement Results Summary

A project website platform was developed to gather feedback from residents to better inform the plan and future City decision making. The website was made available to the public on the City's website, through QR Code posters and business cards at the Open House and 4th of July Events and shared online through the City's Facebook page. A summary of the tools and results are available below. The draft plan was also shared online with a survey for recording comments, a total of 21 comments were received and addressed where possible.

A map showing the City Limits, County Boundaries, Existing/Proposed Pathways and Future Parks was provided with tools to provide suggestions, reactions or ideas for viewers. **A total of 37 unique map contributions were shared by members of the public.** A full list of contributions is available in the appendixes; however, responses were carefully reviewed for integration into the pathway maps.



Big Ideas

A webpage was provided for broad input of big ideas to be shared by users. **A total of 12 ideas were provided, below is a highlight of select quotes.** A full list of contributions is available in the appendixes.

“ I would love to see a livelier, more walkable downtown in Star, with pedestrian friendly pathways, a plaza for gatherings with shade cover, and more businesses with outdoor dining. – KC

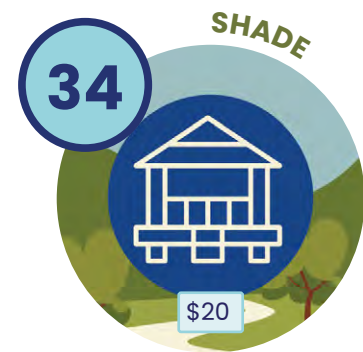
“ A connection from Bent Ln into Star is crucial. We are part of the city, but are on an island with no safe way to access town besides driving. As for parks, shaded play areas for the kids would be a very nice addition. – Evan

“ Having signage about the flora, fauna and birds would be very helpful. – MJM



Park Priorities

A webpage based game was provided to all participants to select items they would most like to see in future parks and recreation system with a total hypothetical budget of \$100 to spend. A summary of responses is available below.



Feedback Survey

A webpage survey was included on the website to gather feedback, below is a summary of results, a full list of responses are available in the appendixes.

Survey Results

98% of respondents are City of Star Residents

40% of respondents use multi-use paths in/around Star multiple times per week

When asked the top 4 trail features most important to them, respondents selected Shade (**92%**), Parking Opportunities (**76%**), Viewpoint/Lookout (**68%**) and Wayfinding/Interpretive Signage (**52%**).

56% of respondents believe the Boise River should be prioritized for new trail projects/connections with the **24%** voting for the Foothills over Main Street or other locations

37% of respondents preferred multi-use path/trail surfaces be a mixture of both hard and soft surfaces, with **21%** preference for concrete, 18% stabilized decomposed granite/millings, 15% asphalt and 7% recycled asphalt.

When asked the 2 most important reasons people would use paths/trails, respondents selected Health & Fitness (**65%**) and Leisure Activities (**61%**).

45% of respondents expressed a clear preference for underpasses that go under the main roadway for street crossings.

When asked the top reasons people have not utilized the local paths/trails more often, respondents selected they did not know the locations (**48%**), the facilities are too far from their home (**29%**) and the lack of shaded areas (**29%**) most frequently.

When given a multi-choice question asking which funding sources people would support using for pathway construction, respondents selected Developer Fees or construction (**90%**), Impact Fees on new development (**77%**), Donations (**72%**), and Grants (**71%**).

Total Participants: 100

81% of respondents said they would support a Local Improvement District to help fund construction of new pathways.

52% of respondents said the project pathway maps had a reasonable amount of pathways, while **48%** said it does not have enough pathways.



Community Engagement

July 15th & 25th, 2024 - 12 attendees.



Importance of Pathways & Trails

Boards with precedent imagery were provided for the trail components below. Participants were each given 8 stickers to spend as votes for any of the features below.

Trail Features



Preference for Multi-use Path/Trail Surface



Preference for Multi-use Path/Trail & Street Crossings

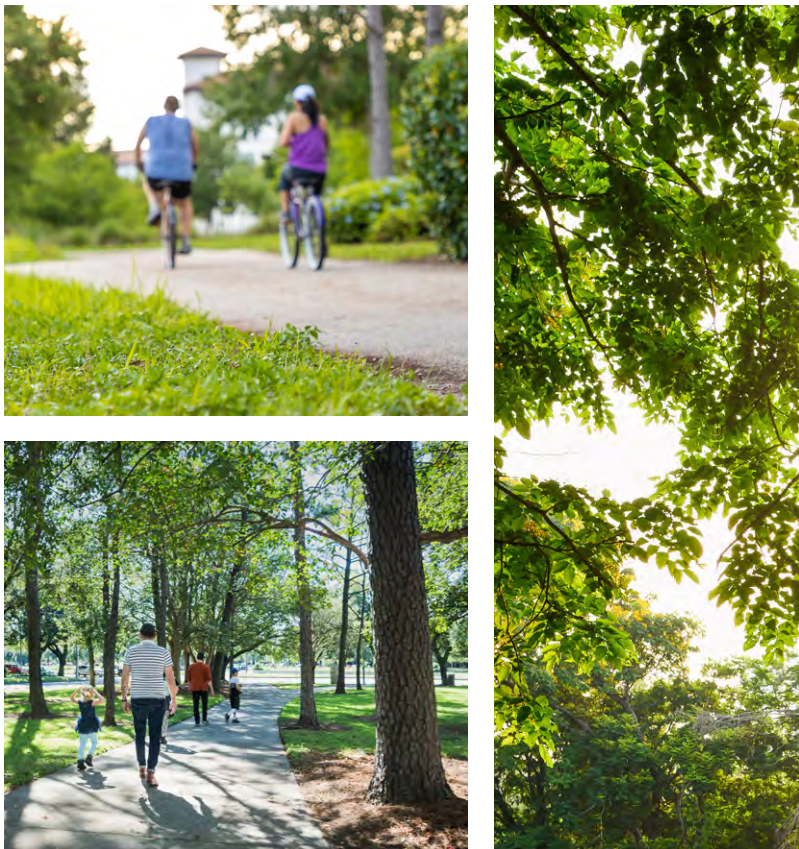


= Votes



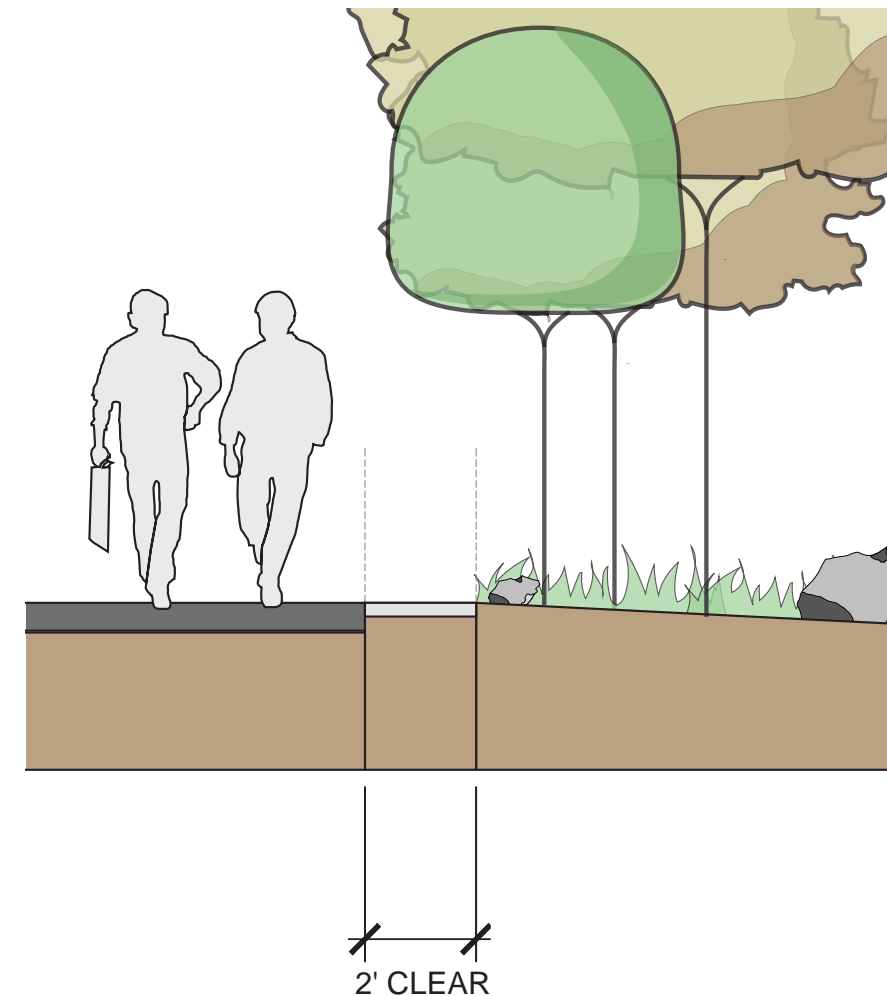
03

PATHWAY CLASSIFICATIONS & SUPPORTING ELEMENTS



Materials

- **Concrete** – durable, higher cost, harder to repair, saw cut joints
- **Asphalt** – lower cost than concrete, low maintenance, shorter lifespan, can heave from adjacent roots
- **Recycled Asphalt** – reprocessed asphalt pavement, a cheaper alternative but reduced durability and increased maintenance over time, and has limitations for accommodating accessibility.
- **Aggregate (crushed rock (gravel) or recycled asphalt)** – preferred for some uses, regular maintenance, limits accessibility
- **Natural Surface (compacted dirt)** – high maintenance, limited use in wet conditions, where required by environmental agencies
- **Permeable Paving** – Asphalt or concrete where required by environmental agencies



2' Clear zone – Shoulder area of compacted aggregate or routinely mowed, kept clear of planting, fencing, light posts or signage. Maximum cross slope 2%

Pathway Classifications

The following pathway classification sections have been provided for the City of Star pathways included within the system maps. It's important to note Highway District facilities should be designed and constructed in accordance with the associated Highway District adopted specifications.

Green Belt Paths

Green Belt Paths are shared hard surface (typically concrete) pathways generally located adjacent to the Boise River. They are a minimum of 12-foot wide. The purpose of the Green Belt Path is for recreation in a natural setting for pedestrian and bike riding (including Class 1 and 2 ebikes). Green Belt paths also service as access to other Community Pathways and to facilitate pedestrian and bike transportation through the community along the river to connect to the greenbelt systems other cities such as Eagle and Boise have constructed. Anticipated to be the highest future used trail. Materials should be environmentally sensitive to the river/floodplain environment.

Examples:

Star River Walk Trail, Greenbelt

Material Recommendation:

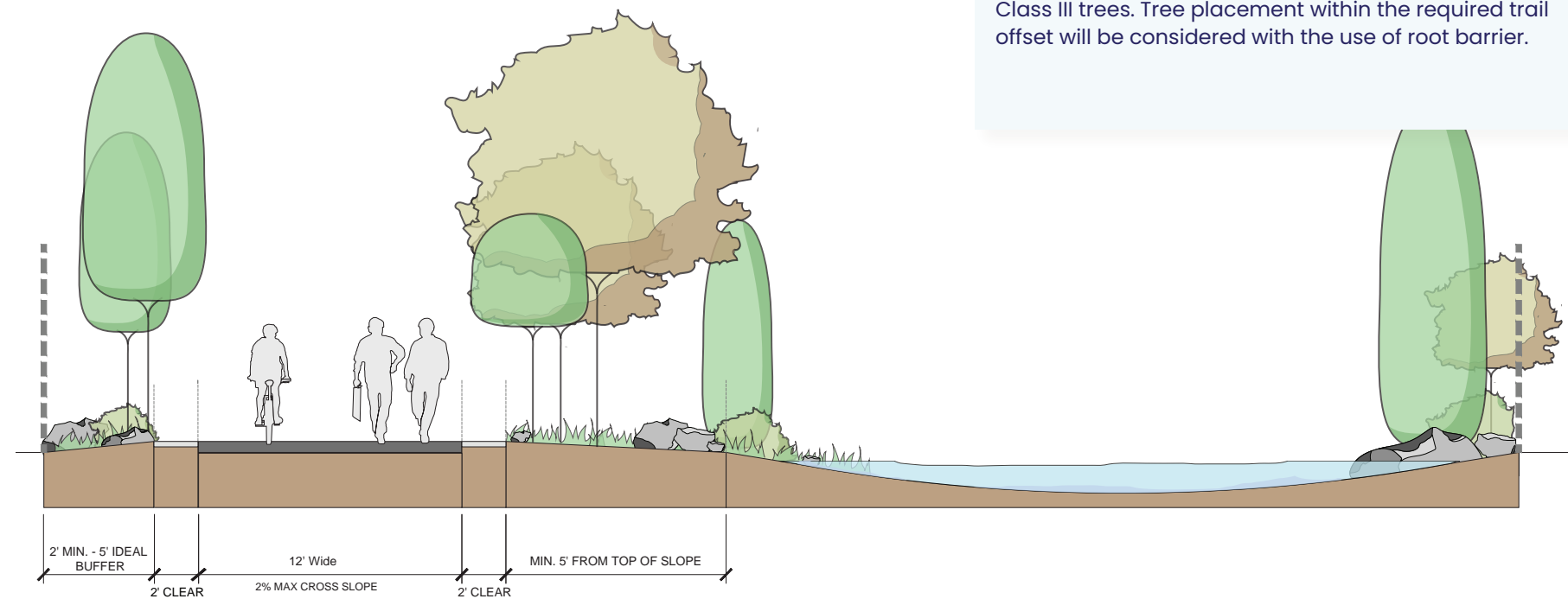
Concrete

Dimension:

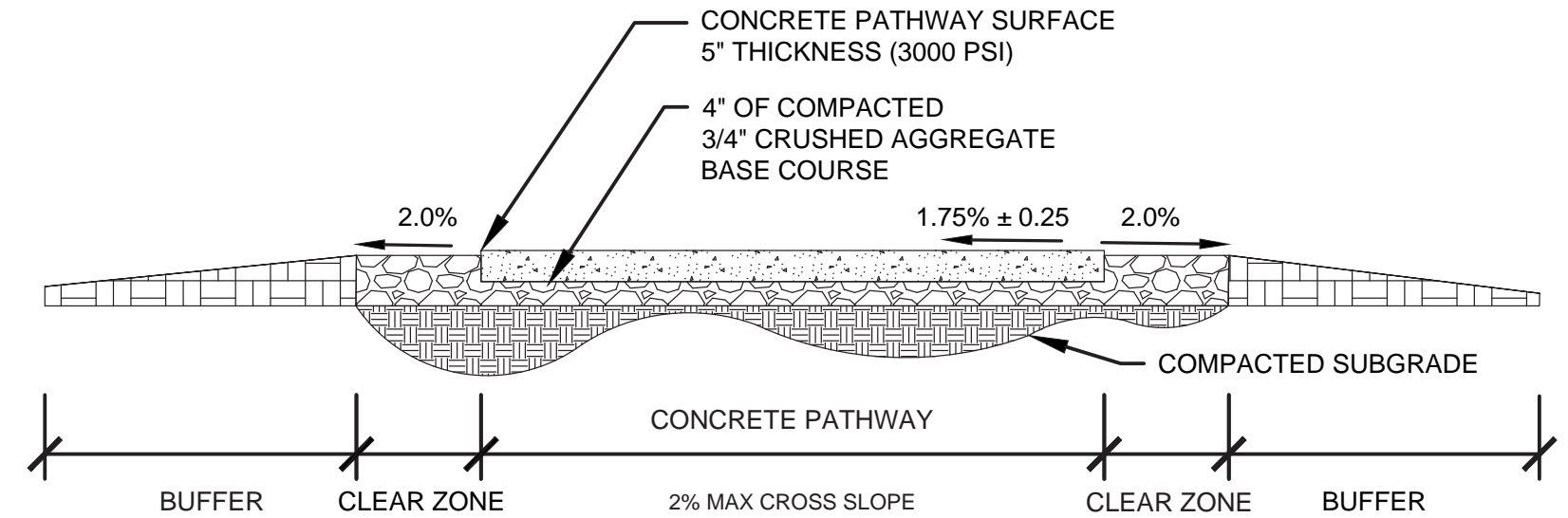
12' Wide

Tree Location:

Trees shall be located no closer than 4 feet from pathway edge for Class I and II trees, and 8 feet for Class III trees. Tree placement within the required trail offset will be considered with the use of root barrier.



Type - Green belt PATHs: Typical section



Drawings are for reference only, not for construction.
A geotechnical report and technical drawings that meet ISPWC standards are required prior to any future trail construction.

Highway Paths

Highway Paths are shared hard surface (typically concrete) pathways adjacent to State Highways 44, 20/26 and 16. They are a minimum of 10-feet wide. The purpose of the Highway is for pedestrian and bike (including Class 1 and 2 ebikes) access to other Community Pathways, greenbelts, schools, parks and businesses and to facilitate pedestrian and bike transportation through the community along the highway system.

Examples:

Main St./HWY 44, HWY 16, Chinden/HWY20/26

Material Recommendation:

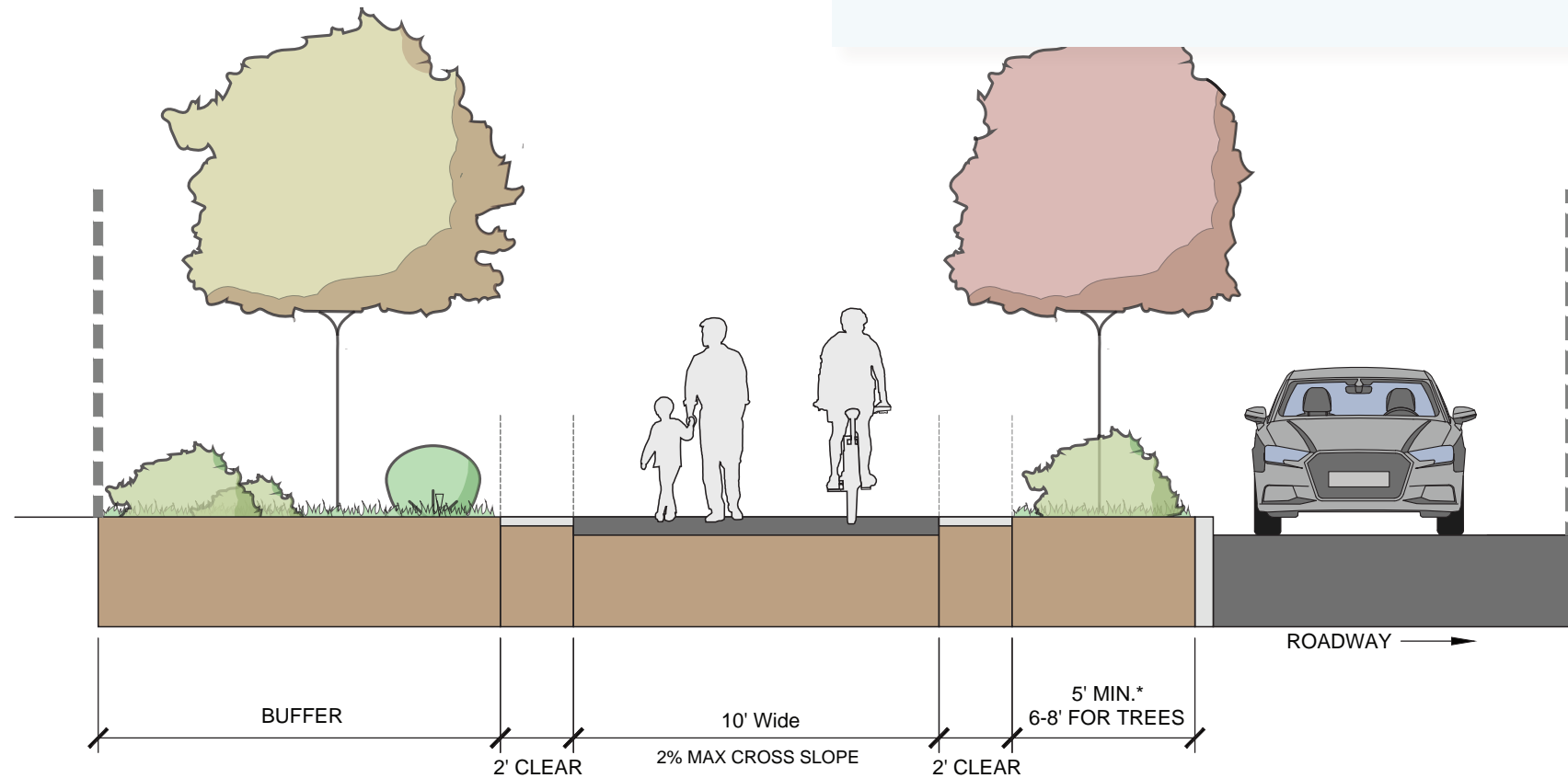
Concrete

Dimension:

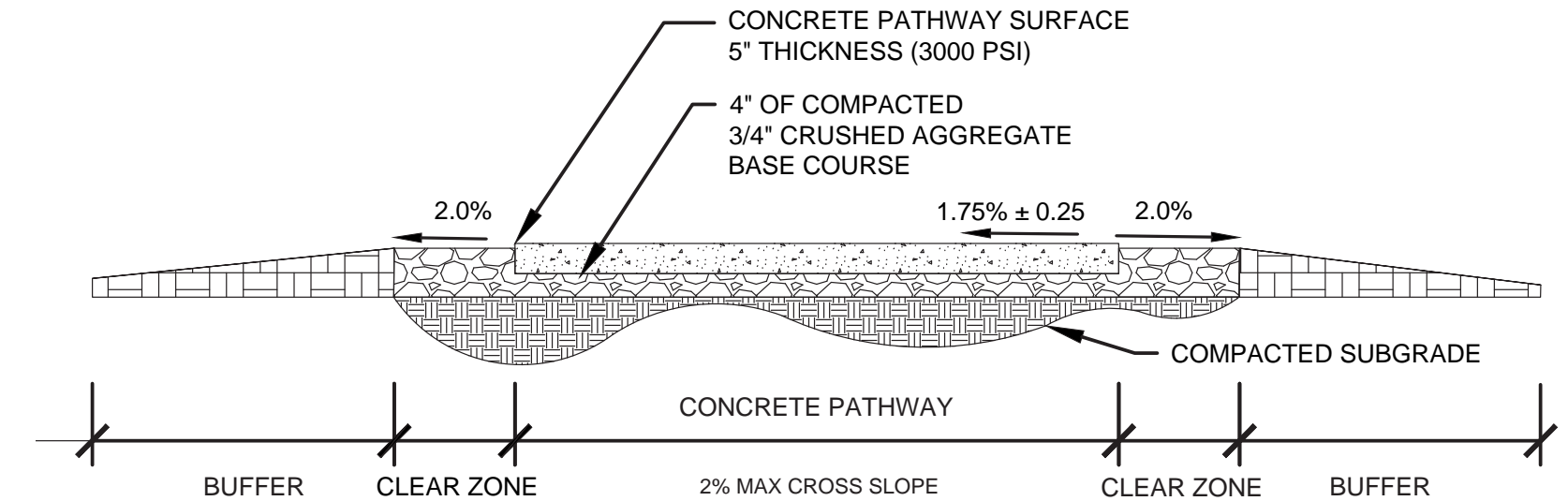
10' Wide

Tree Locations:

Trees shall be located no closer than 4 feet from pathway edge for Class I and II trees, and 8 feet for Class III trees. Tree placement within the required trail offset will be considered with the use of root barrier.



Type - HIGHWAY PATHS: STRUCTURAL DRAWING



Drawings are for reference only, not for construction.
A geotechnical report and technical drawings that meet ISPWC standards are required prior to any future trail construction.

Community Paths

Community Paths are multi-purpose, non-motorized (Class 1 and 2 ebikes allowed) paths that emphasize safe travel for pedestrians to and from parks and schools around the community. The focus is as much on transportation as recreation. Community pathways are commonly located along drain ditches and canals. Community Paths are generally 10-feet wide. They are generally an all-weather hard surface such as recycled asphalt, and asphalt or concrete.

Examples:

Middle School Pathway

Material Recommendation:

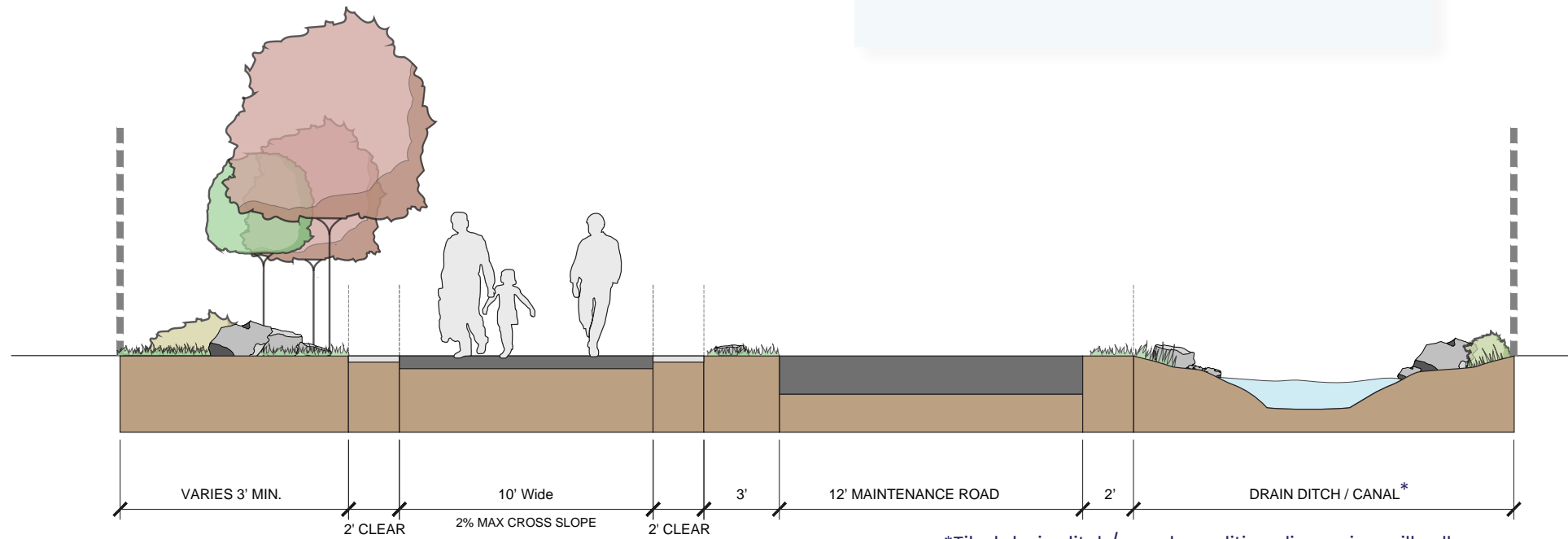
Asphalt or Recycled Asphalt

Dimension:

10' Wide

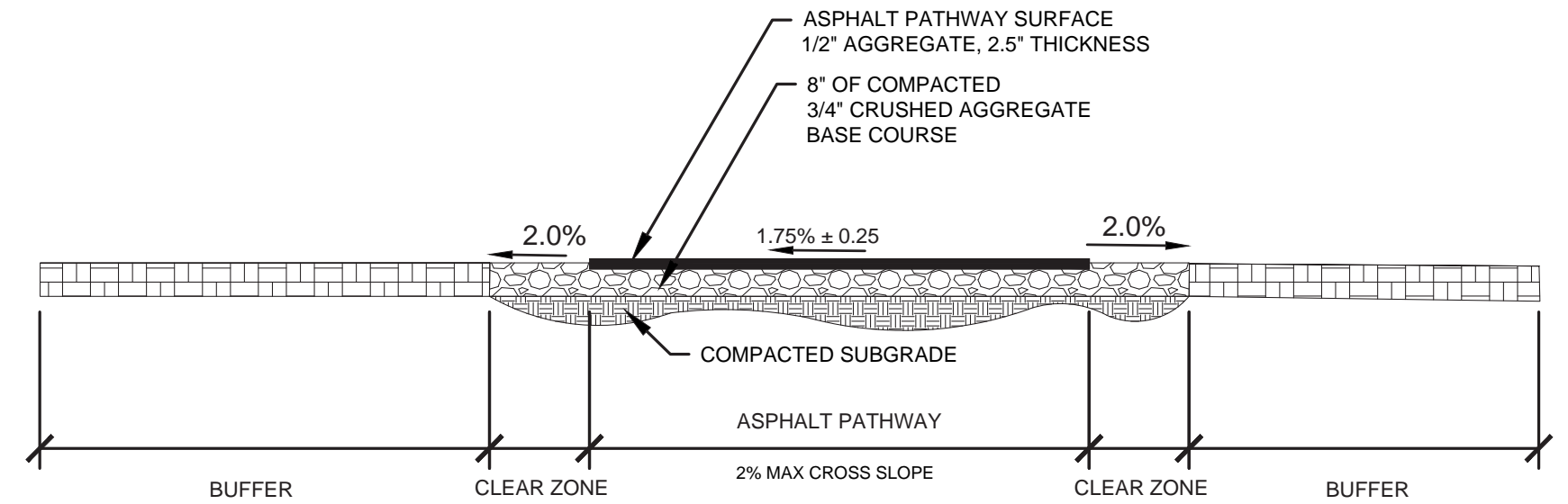
Tree Locations:

Trees shall be located no closer than 4 feet from pathway edge for Class I and II trees, and 8 feet for Class III trees. Tree placement within the required trail offset will be considered with the use of root barrier.



*Tiled drain ditch/canal condition dimension will adhere to owner requirements.

Type - Community PATHs: typical Drawing



Drawings are for reference only, not for construction.

A geotechnical report and technical drawings that meet ISPWC standards are required prior to any future trail construction.

Community Paths (Alternative 1)

Community Paths are multi-purpose, non-motorized (Class 1 and 2 ebikes allowed) paths that emphasize safe travel for pedestrians to and from parks and schools around the community. The focus is as much on transportation as recreation. Community pathways are commonly located along drain ditches and canals. Community Paths are generally 10-foot wide. They are generally an all-weather hard surface such as recycled asphalt, and asphalt or concrete.

Examples:

Middle School Pathway

Material Recommendation:

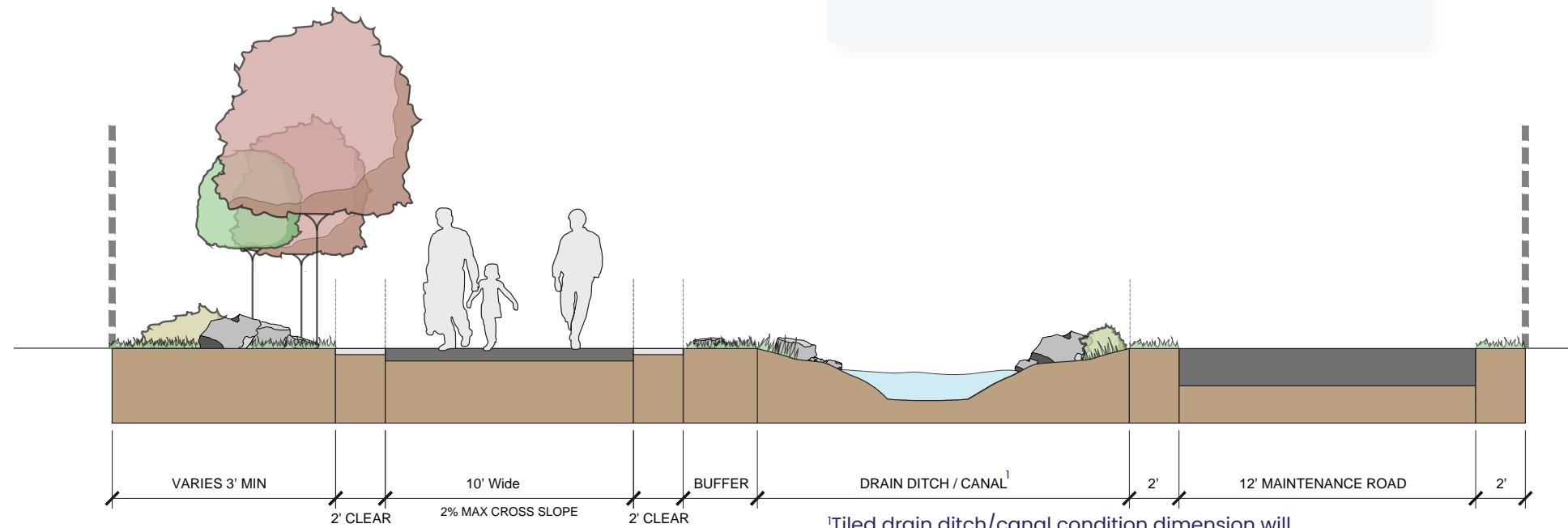
Asphalt or Recycled Asphalt

Dimension:

10' Wide

Tree Locations:

Trees shall be located no closer than 4 feet from pathway edge for Class I and II trees, and 8 feet for Class III trees. Tree placement within the required trail offset will be considered with the use of root barrier.



¹Tiled drain ditch/canal condition dimension will adhere to owner requirements.

*Assumes 50' R.O.W

Community Paths (Alternative 2)

Community Paths are multi-purpose, non-motorized (Class 1 and 2 ebikes allowed) paths that emphasize safe travel for pedestrians to and from parks and schools around the community. The focus is as much on transportation as recreation. Community pathways are commonly located along drain ditches and canals. Community Paths are generally 10-foot wide. They are generally an all-weather hard surface such as recycled asphalt, and asphalt or concrete. Shared path and access road with Ditch Co. Approval.

Examples:

Middle School Pathway

Material Recommendation:

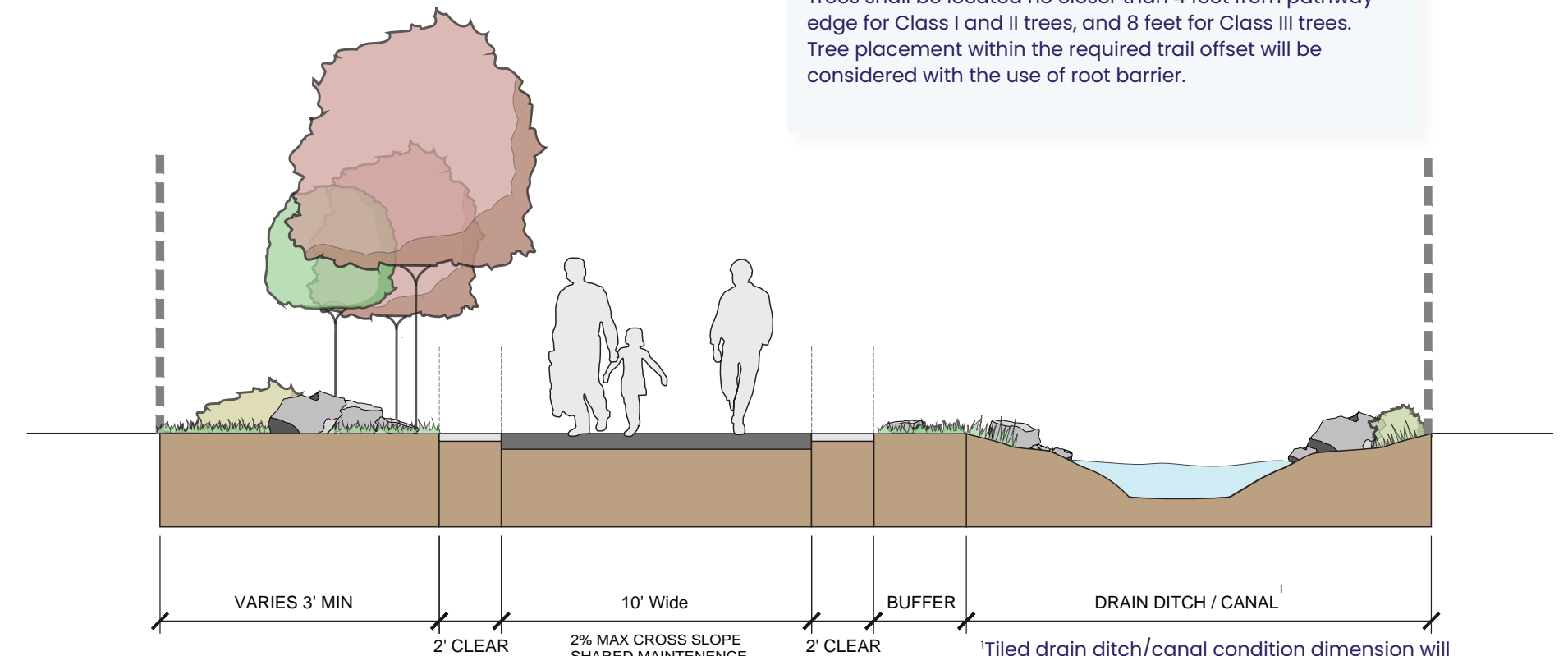
Asphalt or Recycled Asphalt

Dimension:

10' Wide

Tree Locations:

Trees shall be located no closer than 4 feet from pathway edge for Class I and II trees, and 8 feet for Class III trees. Tree placement within the required trail offset will be considered with the use of root barrier.



¹Tiled drain ditch/canal condition dimension will adhere to owner requirements.

*Assumes 50' R.O.W

Natural Paths

Natural Pathways are generally non-motorized pathways located within green ways, parks, foothills and other natural resource areas. The focus is on recreational value and harmony with the natural environment while protecting users from urban development and associated vehicular traffic. Surface material used and width may vary due to the function of the trail. An example of a natural pathway is the paved pedestrian path on the east side of Star River Walk.

Examples:

Star River Walk, Foothills

Material Recommendation:

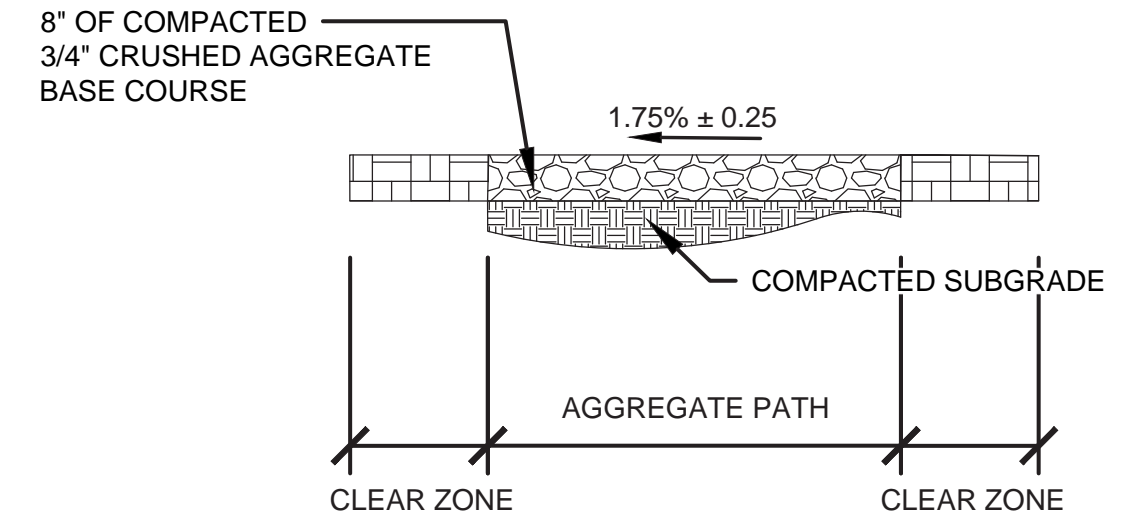
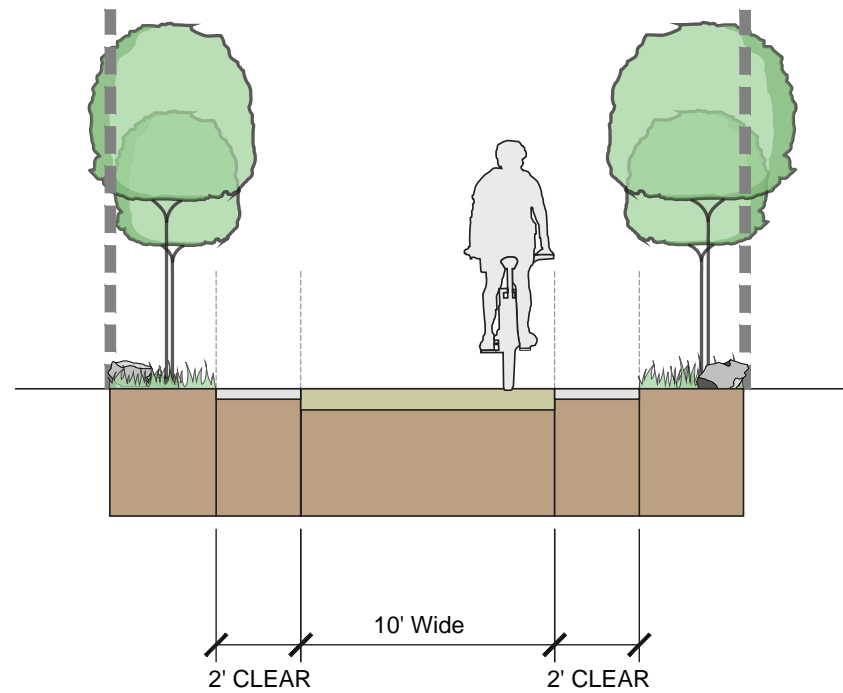
Recycled Asphalt

Dimension:

10' Wide

Tree Locations:

Trees shall be located no closer than 4 feet from pathway edge for Class I and II trees, and 8 feet for Class III trees. Tree placement within the required trail offset will be considered with the use of root barrier.



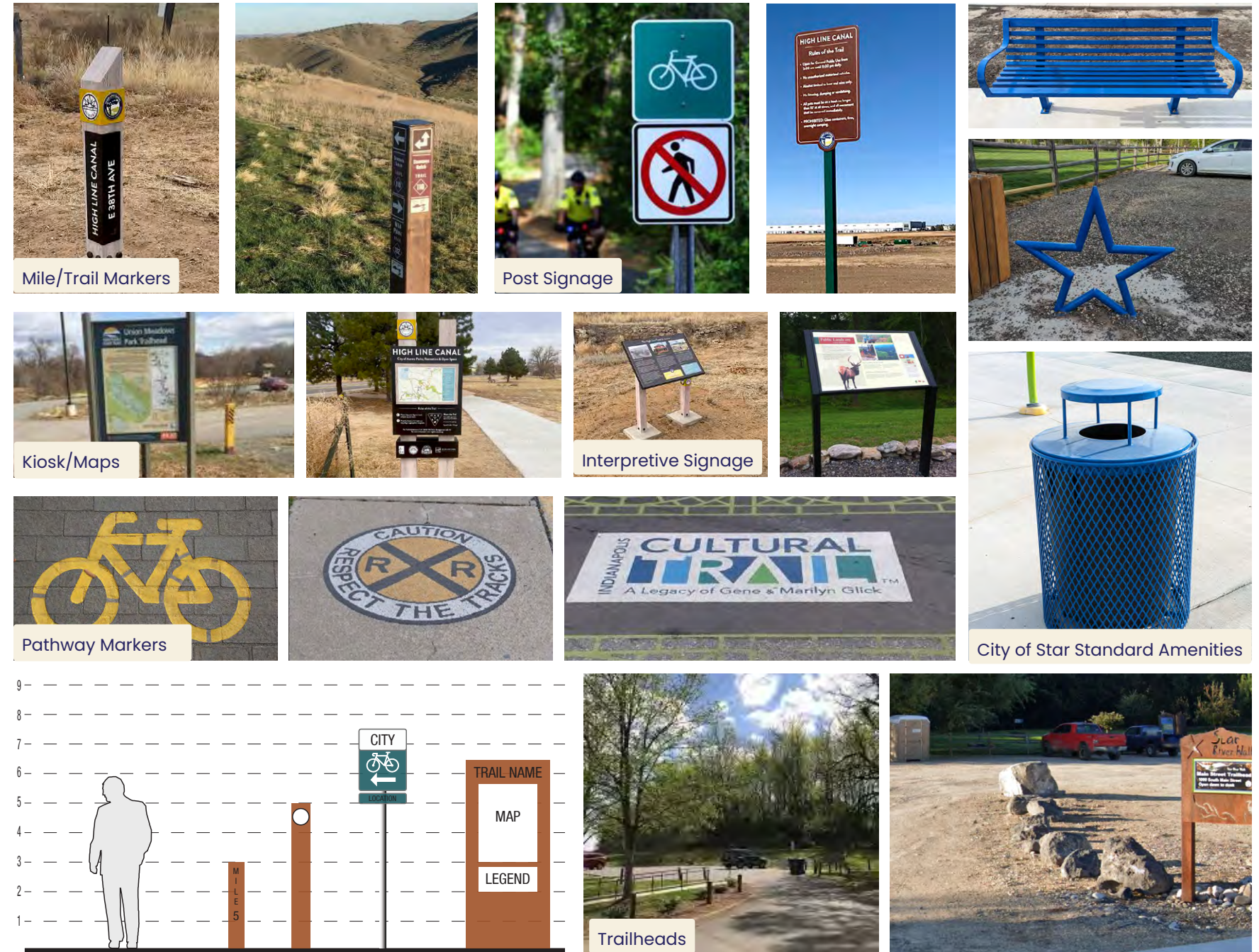
Drawings are for reference only, not for construction.

A geotechnical report and technical drawings that meet ISPWC standards are required prior to any future trail construction.

Pathway Amenities

The table below summarizes the pathway network supportive amenities with detailed descriptions for the City of Star.

Type	Comments
Access Points	Defined point of access for user and emergency services, reference Master Plan Map for locations
Trailhead	Located at major access points, can provide parking, and trailhead map, reference Master Plan Map for locations
Enhanced Trailhead	Located at major access points, can provide parking, restrooms, drinking fountains, bicycle parking, benches and other amenities, reference Master Plan Map for locations
Restrooms	Existing Park Facilities or considered at future trailheads
Parking	Existing Park parking lots or public realm parking at access points
Wayfinding	Directional and information signage used to direct and inform users, with branding and cohesive signage design similar to Ridge to Rivers with Star, ID Blue, signage and wayfinding shall conform to Chapter 9 of the Manual on Uniform Traffic Control Devices (MUTCD). Types include Information Kiosk (with interchangeable graphics capability), Maps, Directional Post Signage, Welcome markers and Mile markers. Locations at trailheads, entry points, decision points/intersections and distances along pathways coordinated with hierarchy of trail types.
Interpretive Signage Viewpoints	Story telling opportunity, view points, scenic lookouts, points of interest, historic marker, water cycle, etc placed at trailheads and points of interest, consideration to be given to Trident Park, Phyllis Canal Trail, Freedom Park, Star Road Bridge over River (east) is City pull out for future river float. Put In at HWY 16
Public Art	Integration of public art, supports wayfinding and interpretive signage at City Parks in coordination with Art and Beautification Committee
Drinking Fountains	Existing or future park facilities/trailheads, typically near restrooms with utility connections, should be all season if possible
Benches	City of Star, ID standard, placed at place of interest (parks, greenspace etc.), anchor mount on concrete base
Bike Racks	City of Star, ID standard, placed at parks or open space use areas, anchor mount on concrete base
Dog stations/Trash Cans	HOA's are encourage to place waste stations at path accesses and near major path intersection, City of Star, ID standard trash cans located at parks/trailheads



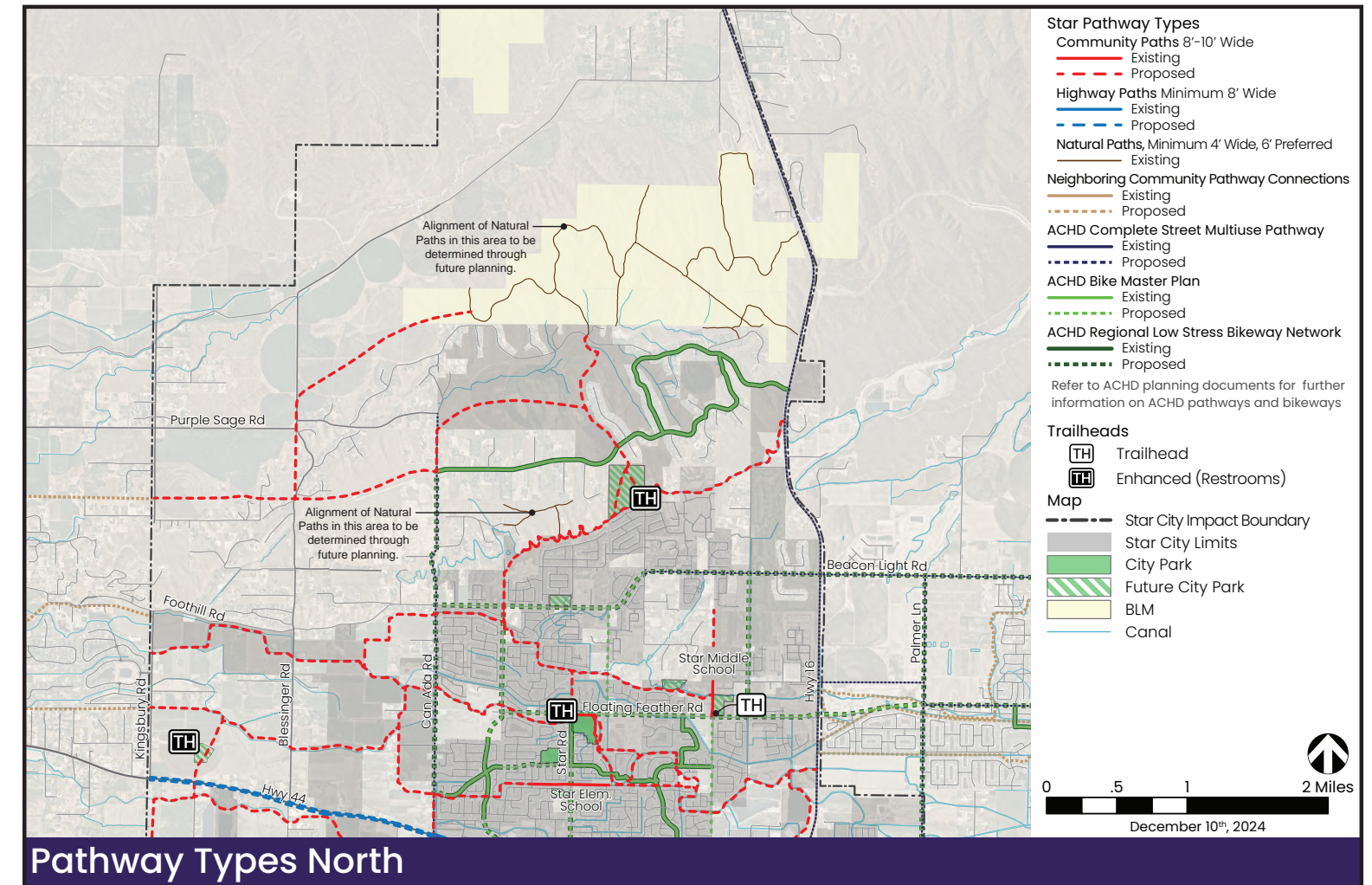
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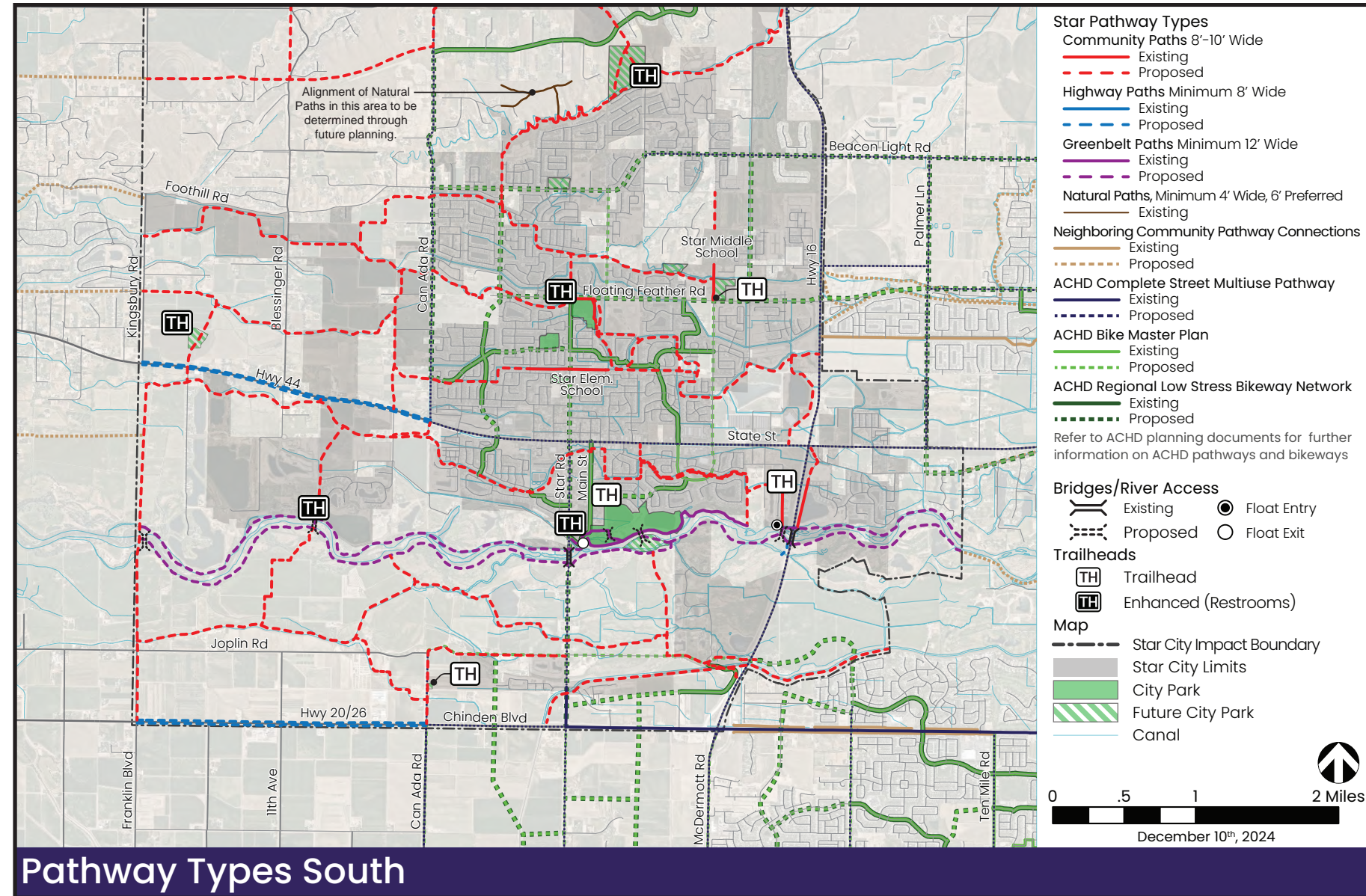
NETWORK RECOMMENDATIONS



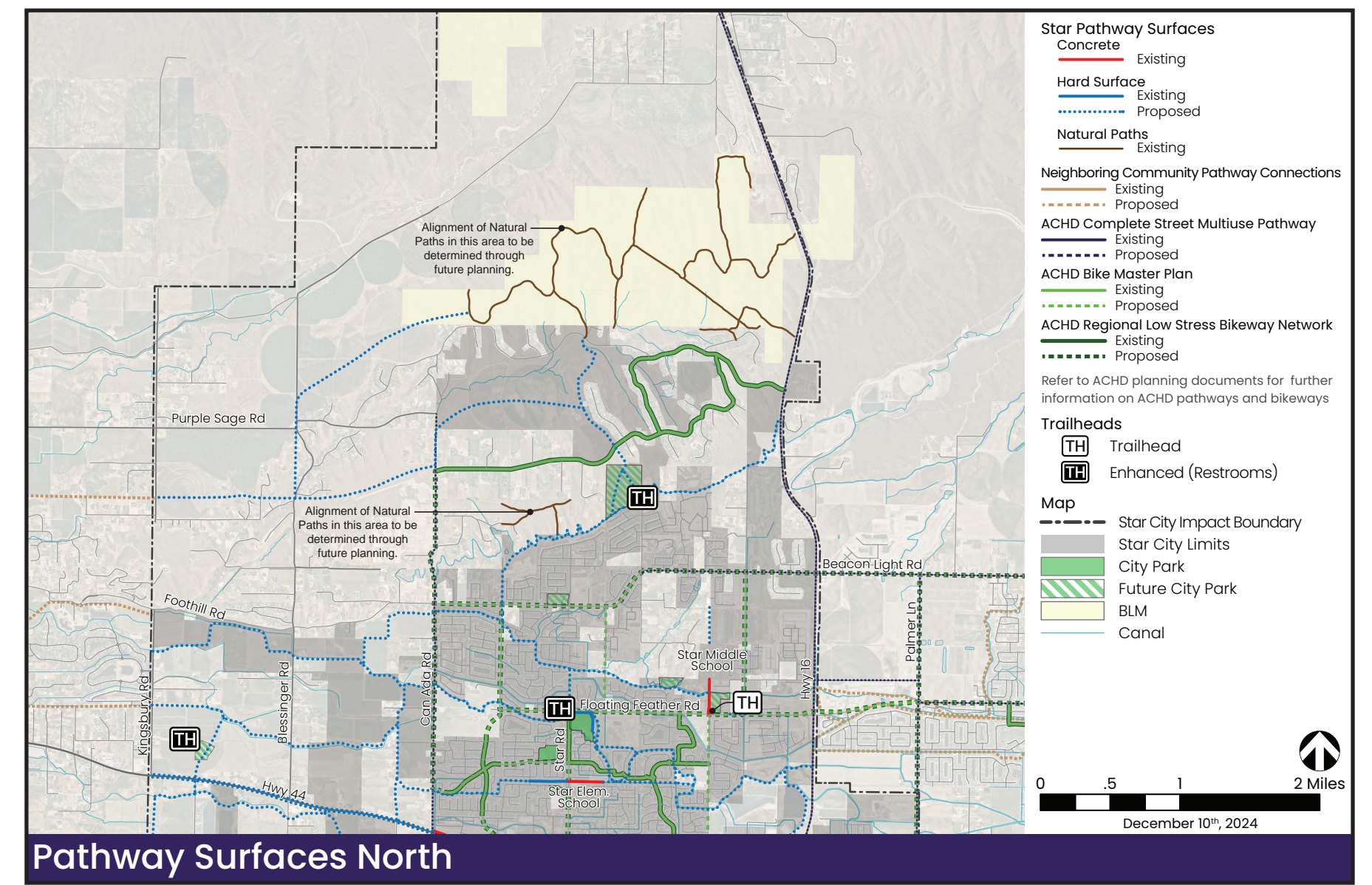
Proposed Pathway Network

Currently only 8% of the planned 102.1-mile hard surface trail network exists. This includes 1.7 miles of Community Pathways; 5.1 miles of Highway Pathways along federal, state, and county highways; and 1.7 miles of Greenbelt Pathway totaling the City's 8.5 miles of existing hard surface pathway. **To complete the pathway network an additional 49.7 miles of community pathway; 31.4 miles of highway pathway, including ACHD Complete Street Multiuse Pathways; and 12.5 miles of greenbelt pathway are proposed. 2.6 miles of the proposed community pathway are currently existing but under private ownership.** No additional natural surface trails are proposed at this time. It's important to note proposed pathways include physically existing pathways/sidewalks today that require future changes to maintenance responsibility, right-of-way and public access being granted. These situations are anticipated to be resolved on a case by case basis. It also important to note that pathways located along highway district rights-of-way are to be designed and constructed in accordance with the associated Highway District adopted specifications.

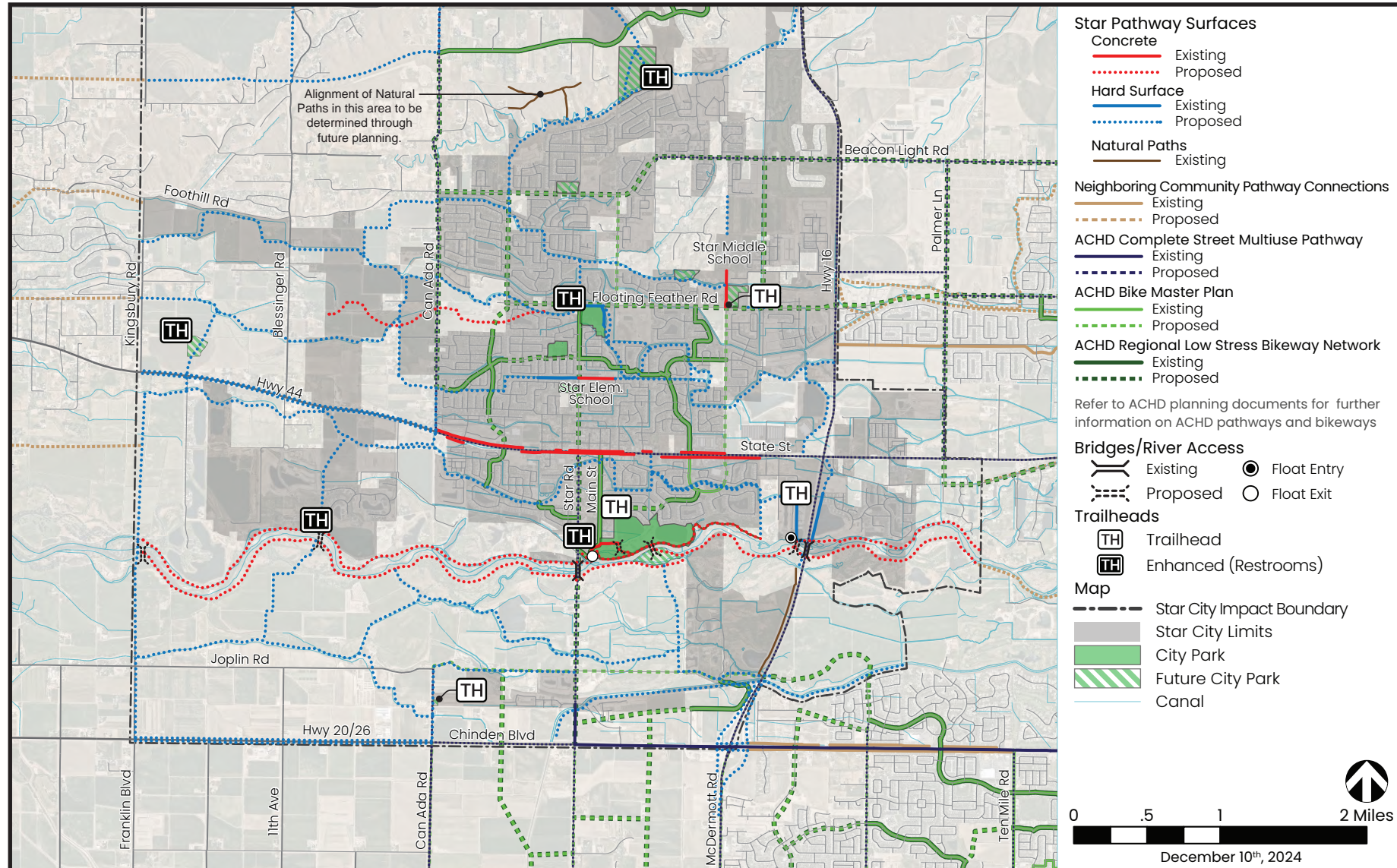




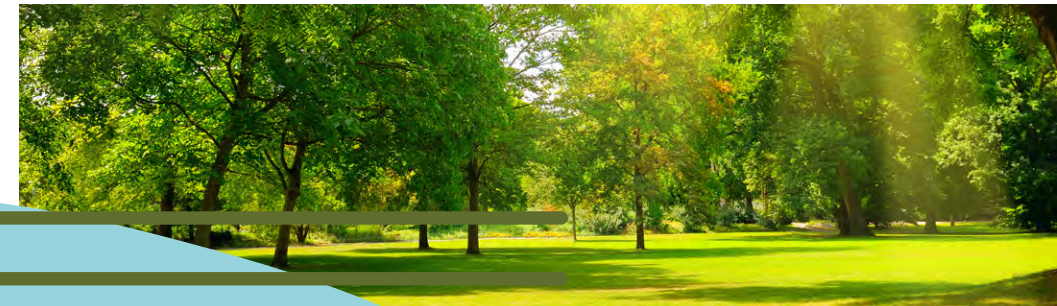
Pathway Types South



Pathway Surfaces North



Pathway Surfaces South



Regional Connectivity

It is critical the Star Master Pathway Plan tie into adjacent jurisdiction pathways to further extend regional connectivity options for City of Star residents. Below are links to the jurisdictions adjacent the City of Star Impact Area boundaries master pathway plans, this has been overlaid with the City of Star Pathway network in the map on the next page.

Perimeter Stubs

City of Eagle

Pathways & Trails Plan – 2019

[Link Here](#)

City of Meridian

Pathways Master Plan – 2010

[Link Here](#)

City of Nampa

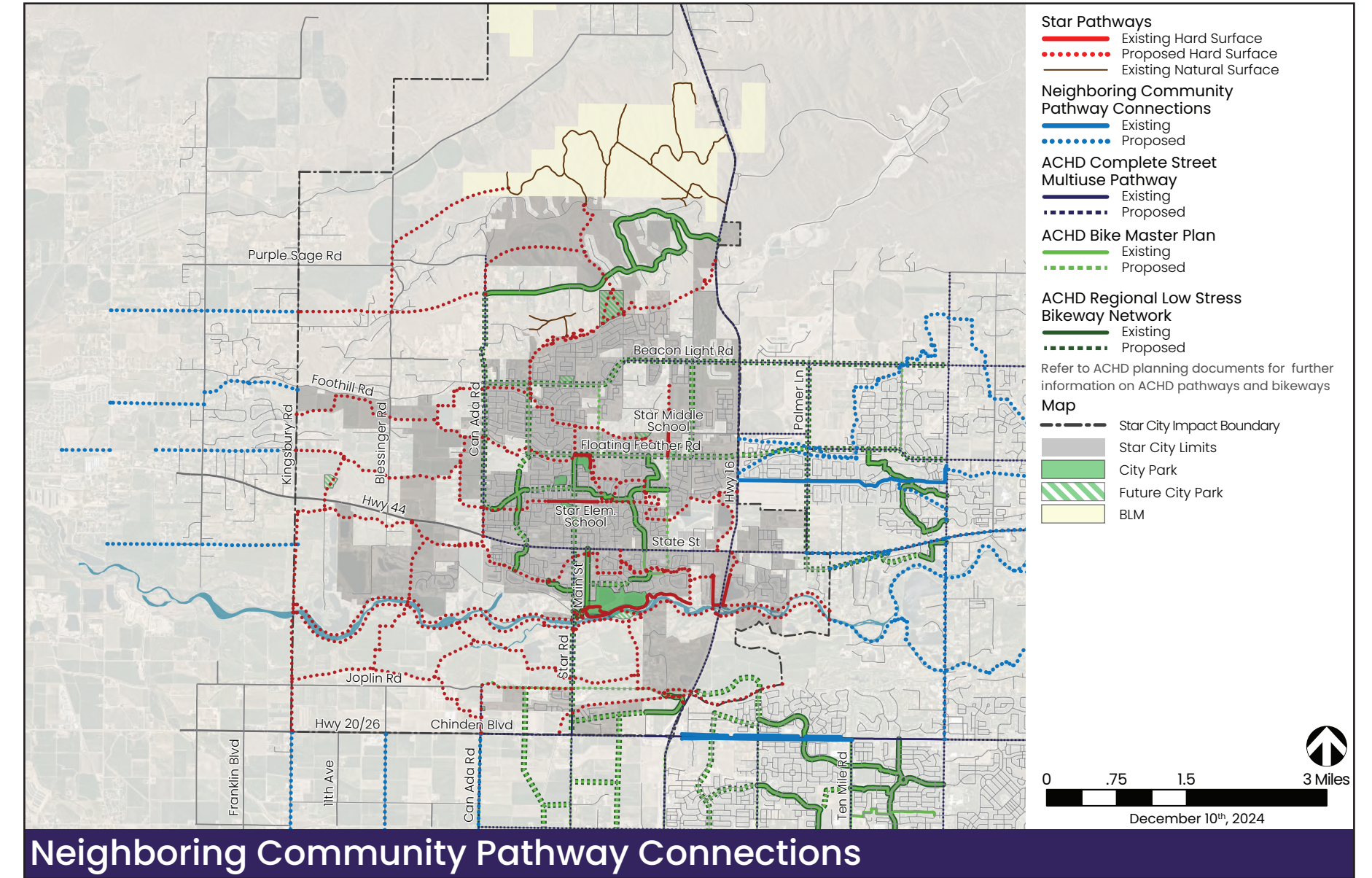
Bike and Pedestrian Master Plan – 2019

[Link Here](#)

City of Middleton

Transportation, Schools and Recreation Map – 2021

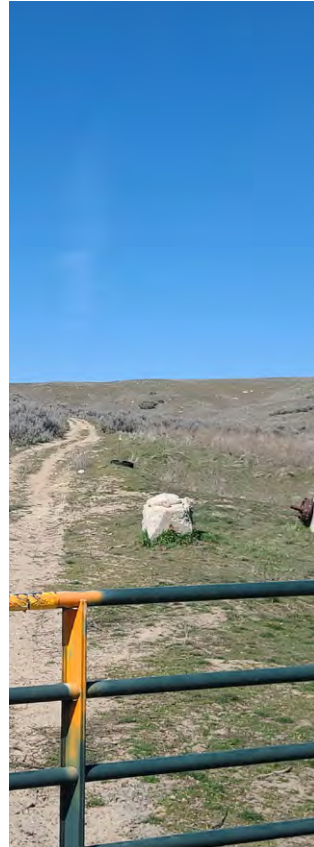
[Link Here](#)



Neighboring Community Pathway Connections

05

PATHWAY MAINTENANCE



Maintenance Guidelines

Introduction

Importance of Maintenance and Purpose of This Plan

Maintenance plays a crucial role in ensuring safe and efficient pedestrian movement within the pathway network. Proper maintenance not only enhances the aesthetics of the city's pathways and promotes a positive user experience, it also helps reduce accidents and negative incidents occurring along the paths. This maintenance management plan aims to provide guidelines for maintenance best practices. This plan is meant to serve as a guideline and does not replace any existing City of Star or governing agency standards, policies, and procedures. This Plan is designed to be a working document and should be reviewed on a regular basis and revised as appropriate. It may be used in conjunction with the city's development of an annual maintenance budget and planning. Maintenance and service levels may fluctuate from time to time due to the availability of resources, special needs or circumstances that may arise. The following maintenance guidelines have been provided for the City of Star pathways included within the system maps. It's important to note Highway District facilities should be maintained and operated in accordance with the associated Highway District adopted specifications.

Quality of Work

The importance of high quality work for pathway maintenance cannot be overstated. Quality work ensures that pathways are safe, functional, and aesthetically pleasing, leading to a positive user experience and long-term sustainability. Here are some key reasons why maintaining a high standard of quality is crucial:

- **Safety:** Pathways are used by pedestrians, cyclists, and other users, and their safety should always be the top priority. High-quality maintenance practices help identify and address potential hazards promptly, such as uneven surfaces, potholes, or obstructions. By ensuring that pathways are in good condition, we can minimize the risk of accidents and injuries.
- **User Experience:** Pathways are designed to facilitate movement and provide an enjoyable experience for users. A well-maintained pathway with smooth surfaces, clear signage, and proper lighting

enhances the user experience and encourages more people to utilize the pathway. Conversely, neglecting maintenance can lead to user dissatisfaction, decreased usage, and a negative perception of the overall environment.

- **Longevity:** Quality maintenance practices contribute to the longevity of pathways. Regular inspections, proactive repairs, and proper cleaning prevent minor issues from escalating into major problems. By addressing maintenance needs promptly and effectively, we can extend the lifespan of pathways, reducing the need for costly repairs or replacements in the future.
- **Cost Savings:** Investing in quality maintenance upfront can result in significant cost savings in the long run. Preventative maintenance and timely repairs help avoid more extensive damage that may require extensive reconstruction or replacement.
- **Community Image:** Pathways are often a reflection of the overall quality and care given to an environment. When pathways are well-maintained, they contribute to a positive community image and create a sense of pride among users and stakeholders. On the other hand, neglected pathways can give the impression of a neglected environment, impacting the overall perception of the area.

Criteria to ensure that an acceptable quality of work is met consistently include:

- All work is consistent with City maintenance standards and complies with any local standards and specifications for the design and construction of public improvements
- All work is consistent with Best Management Practices and industry standards
- All work is compliant with Federal, State and local laws and regulations
- All work is compliant with manufacturer's specifications and standards

Maintenance Standards for Pathways and Trailheads

Overview

The City has adopted maintenance standards to help ensure that all pathways, trailheads, facilities, and equipment are maintained to a specified level of consistent service to produce a safe, clean, functional, and visually appealing multi-modal experience. The goal of these standards is to improve consistency and minimize subjectivity when assessing pathway and trailhead conditions. These standards are consistent with industry standards and best management practices to conserve and protect resources.

The maintenance standards provided in this chapter are intended to be used for the following purposes:

- Inspection and assessment of asset conditions.
- Communicating pathway network conditions effectively
- Developing an asset management plan
- Assisting staff in the prioritization of maintenance resources and allocation

Pathways

- Pathways meet applicable ADA requirements
- Pathways provide a clear path without overgrown landscape material, litter, and debris
- Pathways are free of water collecting depressions and erosion
- Pathways have a uniform surface and positive drainage
- Pathways are free of depressions or trip hazards due to grade changes
- Pathways provide users with warning prior to street or intersection crossings via signage and sensory design

- Signage is clearly visible and not blocked by overgrown landscape materials or other barriers
- Pathway turns or intersections do not occur within 100' of one another
- Sight visibility triangles are considered as part of pathway and landscape design
- Pathways are routinely inspected and cleaned and cleared of litter, debris, snow, and hazards, with frequency determined by assigned maintenance level

Landscape Areas

- Landscape areas are inspected on a weekly basis
- Turf areas are mowed and trimmed at a height of 2.5" to 3"
- Turf areas should only be provided at picnic areas or other spaces utilized for specific programming
- Plant material should be native or adaptive species and be drought tolerant
- Trees that have fruit/seeds that will drop debris onto the pathways should be avoided
- Pathways, trailheads, and facilities are routinely inspected and cleaned and cleared of litter, debris, snow, and hazards, with frequency determined by assigned maintenance level
- Shrubs and grasses are not overgrown or infringing on active pathway areas
- Trees are planted and pruned for the purpose of providing shade to pathway users
- Trees and tree replacements in irrigation district rights-of-way are maintained in accordance with applicable agency standards
- Native grasses are mowed for health, aesthetics, and weed control
- Wildlife habitat is provided, prioritized as applicable
- Noxious weeds are controlled as needed

Irrigation

- Irrigation systems deliver uniform coverage
- Irrigation systems utilize water-efficient technology
- Irrigation systems utilize sensors and communication technology to provide leak detection and remote irrigation system monitoring
- Irrigation systems are annually winterized as needed based on applicable manufacturer recommendations
- Heads and drip irrigation components are routinely inspected for coverage
- Heads are properly distributed with radii and arcs setting adjusted to provide dual coverage and limit overspray
- Irrigation is zoned and timed intentionally to enhance user experience and minimize evapotranspiration rates
- Booster pumps are provided as needed to provide adequate pressure and are regularly inspected and maintained

Drinking Fountains

- Fountains are accessible and operational
- Consider the use of bottle fillers and dog bowls when selecting appropriate drinking fountain models
- Where electricity can be provided, water cooling systems are included
- Drinking fountains are located at trailheads where feasible
- Fountains are checked for debris and cleaned as needed
- Fountains are installed on a solid and ADA compliant surface

Restrooms

- Restrooms are located at enhanced trailheads
- Toilets are clean and sanitary
- Restrooms provide required ADA access
- Restrooms include vandal-resistant partitions and components
- Restrooms are fully operational and regularly maintained
- Restrooms include security system/cameras where allowable

- Trash Receptacles
- Receptacles are clean and routinely emptied
- Receptacles are painted or powder coated
- Receptacles are installed per manufacturer recommendations and properly anchored
- Receptacles are installed on concrete pads
- Trash receptacles are adequately provided at trailheads, picnic areas, shade structures
- Pet waste stations are located and stocked at trailheads as needed
- All receptacles, containers and pet waste stations meet current ADA guidelines for accessibility and height

Benches and Picnic Tables

- Benches are installed on accessible concrete pads and per manufacturer recommendations
- Bench placement considers viewsheds, wildlife habitat, and other areas of interest
- Regular inspections occur and asset management program is in place for site furnishing replacement
- Benches and Tables will not be placed within any canal maintenance easements

Signage

- Signs are clean, legible, and free of vandalism and major sun damage
- Pathway signs are secure and properly installed in a highly visible location free of plant material overgrowth or other barriers
- Pathway network maps and rules signs are secure and properly installed at all trailheads and major points of ingress/egress
- Signage meets ADA requirements
- Trail markers are secure, placed at appropriate intervals, and installed in a highly visible location

Lighting

- All lighting fixtures to be full cutoff at a minimum and should conform to Dark Sky guidance and City standards.
- LED Lighting
- Low level security lighting is provided along trails in urban corridors where allowable
- Where applicable, conduit may be provided for future lighting to be added to pathway corridors

Shade Structures and Pavilions

- Shade structures and pavilions are installed on concrete pads with ADA compliant points of ingress/egress
- Shelters are installed per manufacturer recommendations and footings are designed based on sealed structural calculations provided by a registered structural engineer
- Regular inspection occurs and an asset management program is in place for shade structure and pavilions replacement
- Shelters are clean and sanitary
- LED lighting is provided at all pavilions
- Security lighting is provided in areas adjacent to pavilions
- Electrical receptacles are provided at pavilions
- Quick couplers and hose bibs are provided if utilities are available
- Rules signage, pavilions identification signage, and reservation signage is posted in highly visible locations

Preventative Maintenance

The City follows a comprehensive preventive maintenance plan for all pathways, trailheads, facilities, and equipment that it is responsible for maintaining. Regularly scheduled maintenance and monitoring of the pathway network is critical to meeting user needs. Visual and physical examinations of all pathway network amenities should be conducted weekly through staff evaluation and assessment to ensure compliance, safety, proper operation, and to mitigate liability risks. Annual required formal inspections should be performed by qualified individuals and inspection documentation should be completed and archived in accordance with City document storage practices.

Repairs to all applicable assets should be completed as required upon notification or inspection in a timely manner. Repairs that include an immediate need or a condition that poses a risk to user safety should be considered immediate priorities. Any areas or assets that are considered a safety risk should be closed and off limits to public use until necessary repairs can be made. All applicable maintenance equipment utilized by the City should be locked and tagged out if found to be unsafe to operate until necessary repairs can be made.



Established Service Levels of Maintenance

Overview

Developing maintenance service levels for pathways, trails, trailheads, and related facilities requires evaluating and responding to the path’s level of use, connectivity to city points of interest, type of programming, and provided amenities. Each maintenance service level serves a specific purpose with the ongoing goal of providing a clean and high-quality pathway network for residents and visitors. Maintenance service levels may be adjusted for specific trailheads or pathway segments based on change in programming or increase/decline in use. It’s also important to note some pathways will be managed by the City while others will be managed by existing or future HOA’s and will require maintenance be the responsibility of the HOA, this will be addressed on a case by case basis. The following descriptions outline the expectations for each maintenance service level:

Service Level A - \$ \$ \$ \$ \$

A high level of maintenance associated with well-developed areas with high visitations rates in close proximity to major city landmarks or points of pride. Turf is lush cut two times per week, free from weeds, and cut to a precise level. Turf fertilizing is completed three times annually, and facilities maintenance is performed Monday-Friday. Plants and trees are pruned, trimmed, and shaped to ornamental beauty. Hardscapes are regularly swept, litter control and snow removal is performed five to six times per week. **Requires one park maintenance worker per six to eight linear miles.**

Service Level B - \$ \$ \$ \$

A reasonably high level of maintenance associated with well-developed areas with high visitations rates. This service level is appropriate for areas that are highly programmed and include trailheads. Most greenbelt and community path segments may be associated with Service Level B. One major difference with Service Level B is that turf is not cut as frequently, but still at regular intervals at a precise level. Plants and trees in parks are not pruned and trimmed at the same frequency. Litter control, snow removal and facility maintenance is two to three times per week and hardscape maintenance is less frequent. Mowing frequencies are five to seven days, shrub and tree pruning are completed bi-annually, turf fertilizing is completed three times annually and inspections occur monthly. **Requires one parks maintenance worker per eight to 12 linear miles.**

Service Level C - \$ \$ \$

The lower expected service level for fully developed pathway corridors with a moderate level of maintenance associated with path areas of average development or visitation. Most highway path segments may be associated with Service Level C. Maintenance is accomplished, usually with longer service intervals, to keep the path network safe and minimally serviceable to the community. Mowing frequency is every seven to 14 days, shrub and tree pruning annually, turf fertilization bi-annually, litter control one to two time per week, facilities maintenance performed one time per week and inspections occur monthly. **This level requires one parks maintenance worker per 12 to 18 linear miles.**

Service Level D - \$ \$

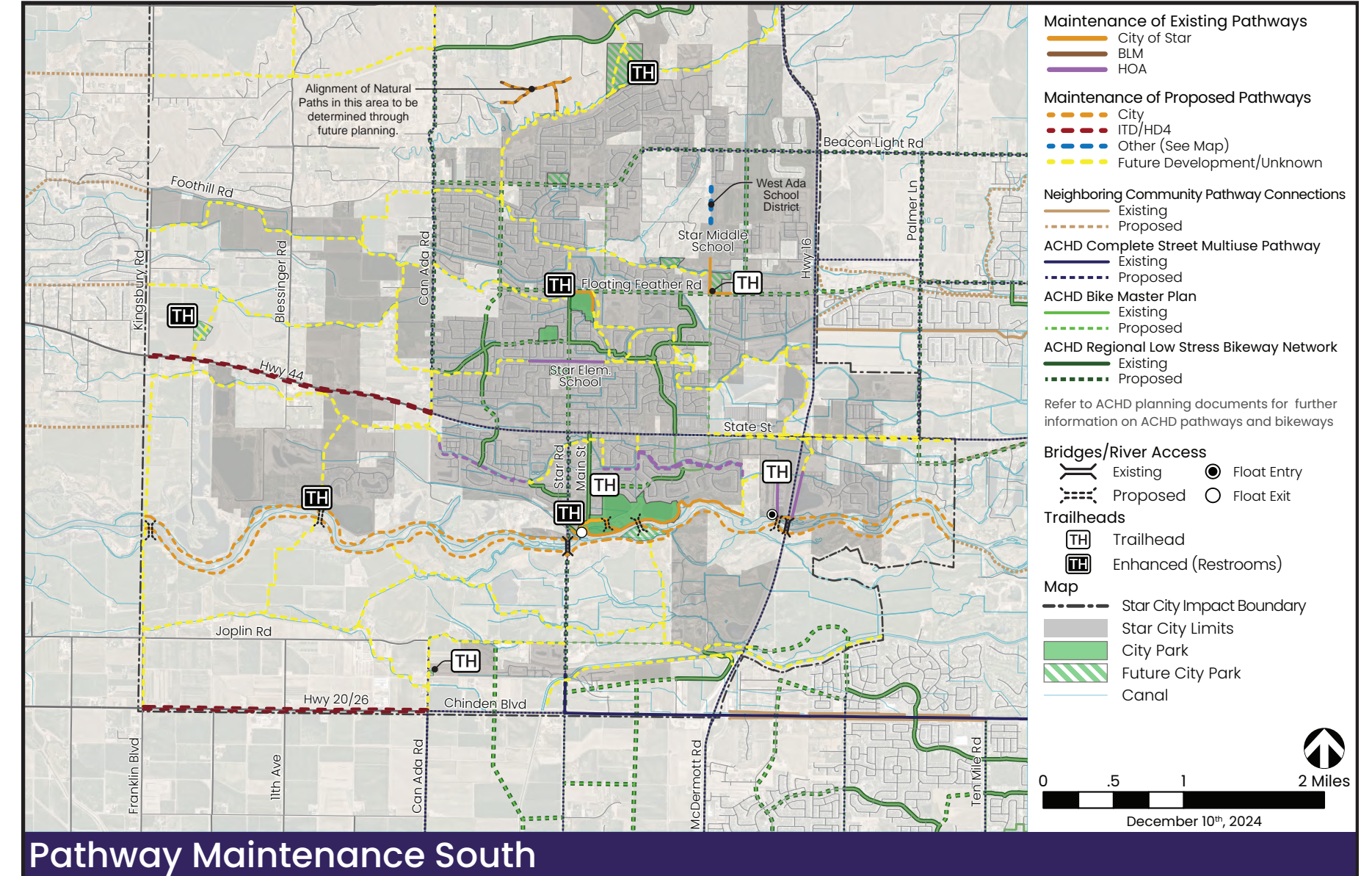
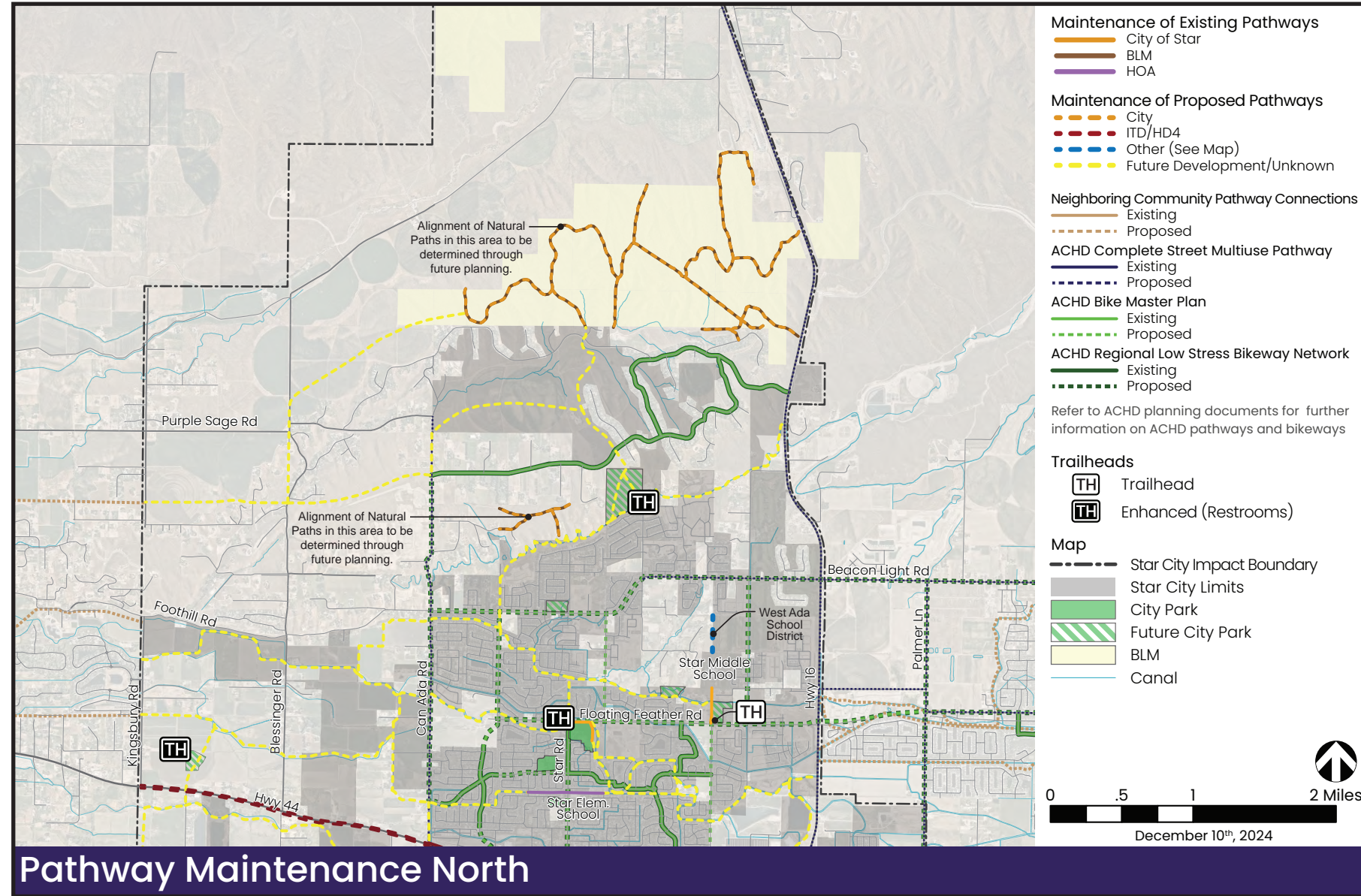
A minimal service level for pathway corridors or open spaces with no facilities with the intent to maintain safe grounds and a natural ambiance. Most natural paths may be associated with Service Level D. Generally inspection services and litter control are conducted on a weekly basis. Mowing frequencies every 14 to 21 days, shrub and tree pruning annually, and fertilization annually. Inspections typically occur monthly. **Usually such services require one park maintenance worker every 50 to 75 linear miles.**

Service Level E - \$

A minimal service level for pathway corridors or open spaces with no facilities with the intent to maintain safe grounds and a natural ambiance. City of Star may not have any planned pathway segments within the Service Level E maintenance classification, however, this classification may be utilized in the future if deemed appropriate. Generally inspection services and litter control are conducted, but on an infrequent basis. **Usually such services and litter control are conducted as “fill-in” work by staff but otherwise one park maintenance worker can cover approximately 200 linear miles.**

Note: Prior to the construction of any city-maintained pathways, the city should assign 0.1 FTE (4 hours per week average) staff person to focus on pathways issues. Tasks would include budget preparation, review of new development for pathway approval conditions and inspection of improvements required; working with HOA’s to construct pathways along canals through existing subdivisions; attending STPC monthly meetings; grant applications; Impact fee projects.

\$ = General cost per mile



06

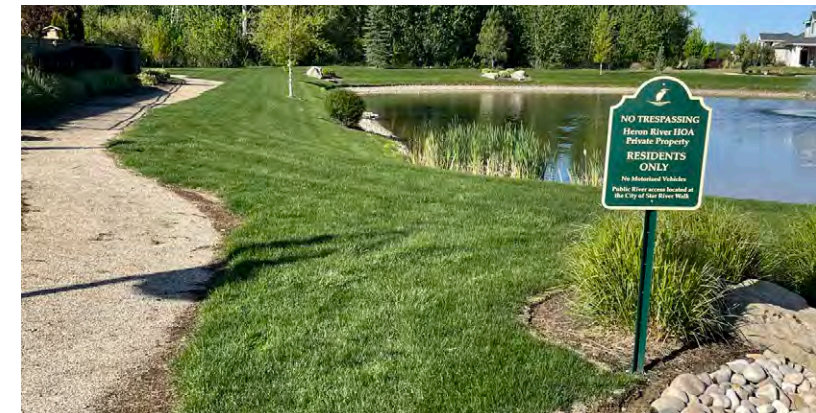
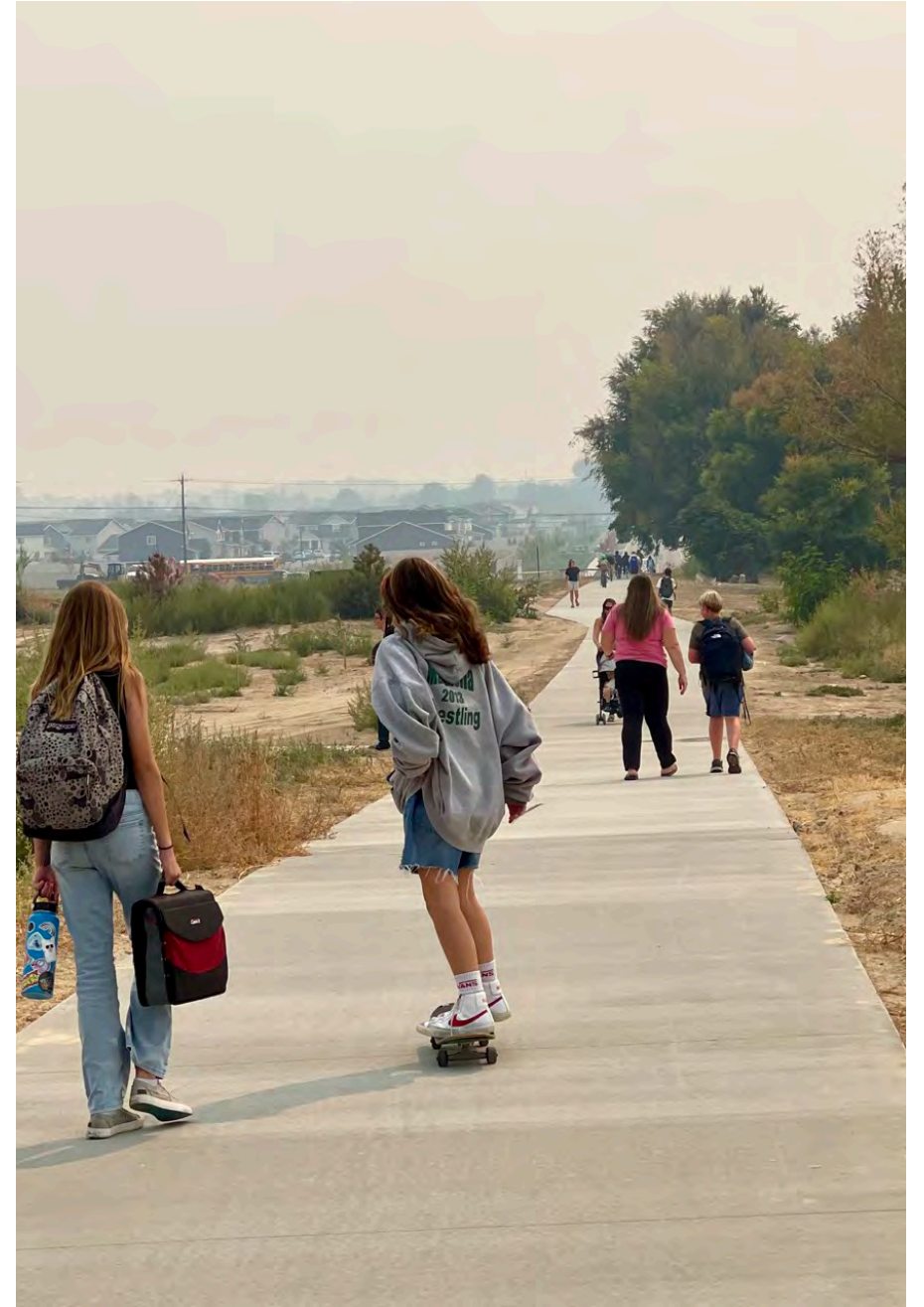
CAPITAL IMPROVEMENT PLAN

Overview

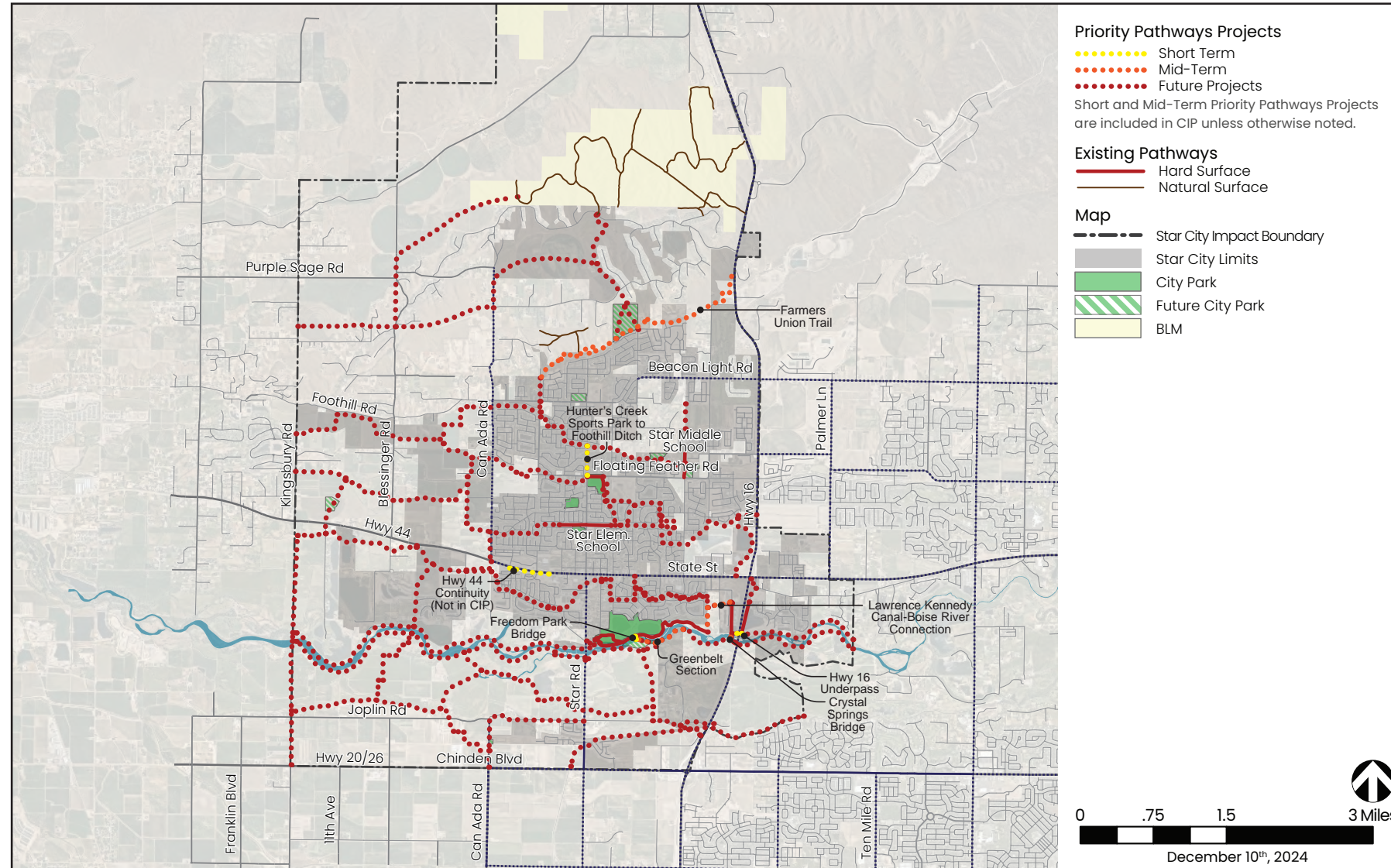
This CIP is intended to focus on new trails and trailhead projects. Most of these capital improvements will also require an increase in annual operations and maintenance costs. The following are the general assumptions utilized in the development of the recommended CIP:

- Lifecycle replacement projects which include the replacement of existing trail assets in the system are included in the cost estimating for the capital improvements plan. These costs have been considered long term.
- Only projects likely to be implemented are included in the plan.
- Projects must be consistent with other planning efforts, where applicable.
- Costs shown are intended to be rough order of magnitude.
- Land acquisition and associated land costs have not been included in this plan.
- Costs shown are intended to reflect the proposed classification section for each trail segment as shown on the plan mapping.
- Grade-separated road crossings, bridges, specific riverfront considerations, or other major structures have not been accounted for in these rough order of magnitude costs.

Please note that the costs shown on the following pages are intended to be rough order of magnitude and based on trail related amenities only. Projected costs do not include additional associated infrastructure related to project implementation. The values shown within this document are based on 2024 dollars and cost escalation has not been added for mid-term (years 6-10) or long term (years 11+). Department staff should continue to evaluate costing information with current market conditions as project funding opportunities arise. It is recommended each identified project undergo a segment-specific master plan prior to final design in order to establish a trail alignment, confirm desired trail amenities, and provide a detailed cost estimate based on specific site conditions.



Capital Improvement Plan Summary



Priority Pathway Projects

Star Pathways Mileage by Type				
Type	Existing Miles	Proposed Miles	Total Miles	% Complete
Community	1.69	49.7	51.39	3%
ACHD	0.18	3.75	3.93	5%
ITD	4.91	27.66	32.57	15%
Greenbelt	1.7	12.49	14.19	12%
Natural	9.23	0	9.23	100%
TOTAL (NATURAL + HARD SURFACE)	17.71	93.6	111.31	16%
TOTAL HARD SURFACE ONLY	8.48	93.6	102.09	8%

Identified Capital Improvement Project Mileage	
Capital Improvement Project Prioritization	Proposed Miles
Short-Term Priority (0-5 Years)	0.38
Mid-Term Priority (6-10 Years)	4.07
Long-Term Priority (11+ Years)	56.21
TOTAL	60.66

Identified Proposed Mileage By Others	Proposed Miles
ACHD/HD4/ITD	31.67

Short-Term Priorities: Capital Improvement Project Breakdown	Proposed Miles	Planning and Design	Soft Surface Trail Cost	Hard Surface Trail Cost	Trail Lighting	Structures	Total Probable Cost Range*
Hunter's Creek to Foothill Ditch	0.3	\$63,648.00	\$31,500.00	\$94,800.00	\$435,600.00	-	\$31,500.00 - \$530,400.00
Highway 16 Underpass	0.05	\$10,608.00	\$5,250.00	\$15,800.00	\$72,600.00	-	\$5,250.00 - \$88,400.00
Freedom Park Boise River Bridge (Approx. 150' Bridge)	0.03	\$548,227.20	-	\$25,000.00	\$43,560.00	\$4,500,000.00	\$4,568,560.00
TOTAL	0.38	\$622,483.20	\$36,750.00	\$135,600.00	\$551,760.00	\$4,500,000.00	\$4,605,310.00 - \$5,187,360.00

Mid-Term Priorities: Capital Improvement Project Breakdown	Proposed Miles	Planning and Design	Soft Surface Trail Cost	Hard Surface Trail Cost	Trail Lighting	Structures	Total Probable Cost Range*
Farmers Union Trail	2.96	\$636,480.00	\$315,000.00	\$948,000.00	\$4,356,000.00	-	\$315,000.00 - \$5,304,000.00
Lawrence Kennedy/Boise River Connection	0.53	\$112,444.80	\$55,650.00	\$167,480.00	\$769,560.00	-	\$55,650.00 - \$937,040.00
Crystal Springs Boise River Bridge (Approx. 210' Bridge)	0.04	\$705,969.60	-	\$25,000.00	\$58,080.00	\$5,800,000.00	\$5,883,080.00
Boise River Greenbelt (South of Freedom Park Bridge)	0.53	\$112,444.80	\$55,650.00	\$167,480.00	\$769,560.00	-	\$55,650.00 - \$937,040.00
TOTAL	4.07	\$1,567,339.20	\$426,300.00	\$1,307,960.00	\$5,953,200.00	\$5,800,000.00	\$6,309,380.00 - \$13,061,160.00

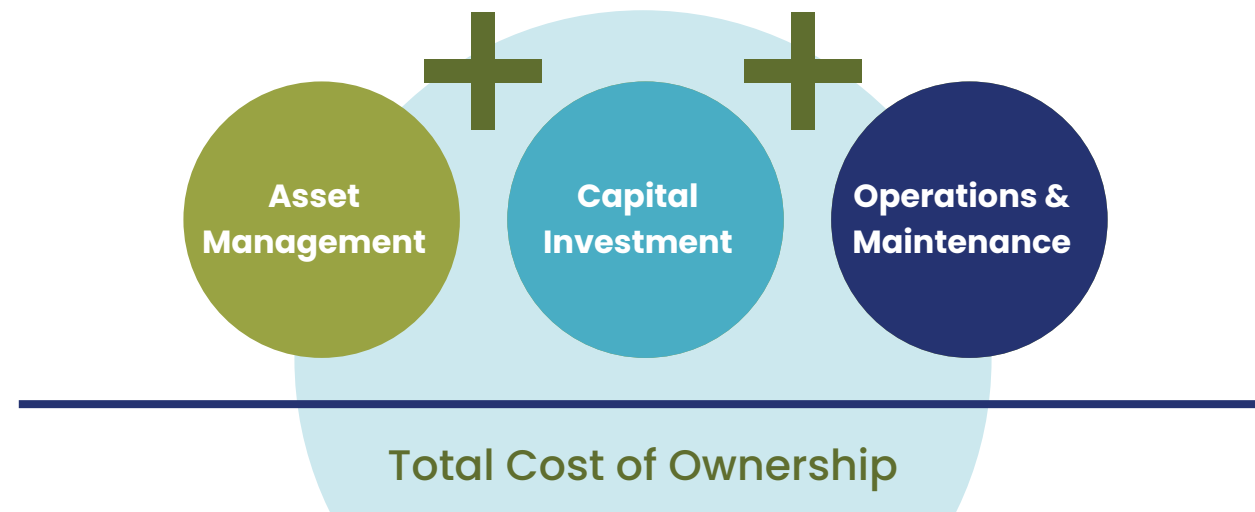
* Costs assume pavement with trail lighting consistent with section classifications| the planning and design will likely be by City staff and could result in a savings to the City.
 **Costs are based on trail mileage and do not consider specific site conditions. It is recommended a project-specific master plan be done for each segment to determine trail alignment and identify site-specific issues prior to design.

Total Cost of the Trails System

The total value of the City's park system includes three items:

- Asset Management: includes preventative maintenance and lifecycle replacement.
- Capital Investment: total cost of the trails system.
- Operations & Maintenance: cost to maintain current trails system.

Inclusion of these three elements provides a true value of the trails system allowing the City to fully view what is required for the full financial sustainability of the trails system.



Asset Management Plan

It is critical to closely monitor the condition of the existing trails system to ensure the safety of patrons. The Recommended Replacement Schedule identifies the recommended trails/amenity replacement schedule based on the identified lifecycle for replacement. It is highly recommended to track the condition and plan for asset replacement based on condition, maintenance, and expected lifecycle. This table is based on best practices within the parks and recreation industry. As the city continues to develop its trail network, it is recommended the city utilizes a comprehensive ten-year asset management plan that is updated annually, identifying conditions and tracking preventative maintenance and lifecycle replacement timelines for the following facilities:

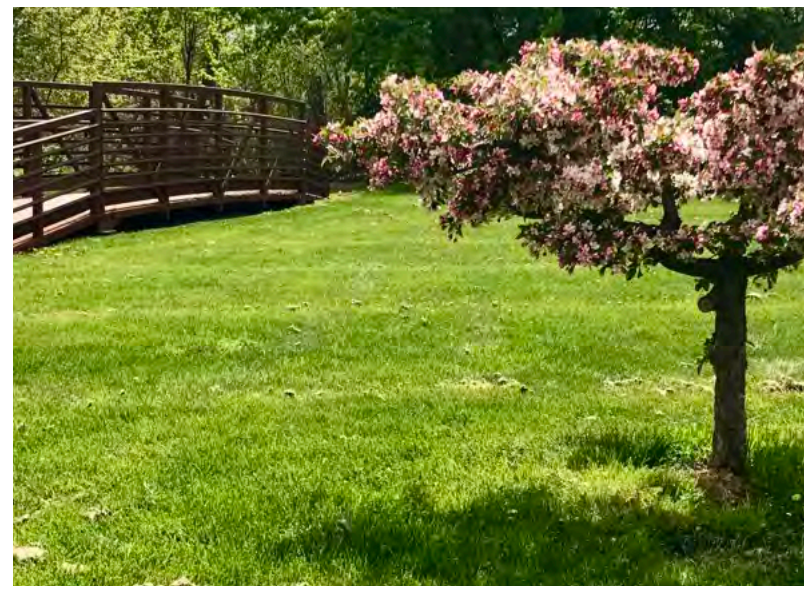
Recommended Replacement Schedule

Facility/Amenity	Lifecycle
BBQ Pit/Grill	10
Bench	15
Bike Rack/Loop	10
Bollards	25
Drinking Fountain	10
Fencing	25
Fitness Station	10
Flagpole	35
Irrigation System	20
Lighting	20
Maintenance Yard/Building	35
Parking Lot	20
Picnic Table	15
Ramada/Pavilion	35
Restroom Building	35
Shade Structure (Fabric)	10
Signage (Monument)	25
Signage (Regulatory/Interpretive)	10
Trail – Non-Paved	25
Trail – Paved	25
Trash Receptacle	15



07

FUNDING STRATEGIES



Introduction

As the Star trails network grows and diversifies its amenities, the city must also identify, develop and diversify their revenue source options. Nationally, a growing number of municipalities have developed strong partnerships that are fair and equitable in the delivery of trails and off-street connections based on who receives the service, for what purpose, for what benefit, and for what costs.

In the City of Star, some of these policies and management practices are in place or being considered and others should be considered for future implementation. Some of these sources can potentially be considered as a revenue option to support the capital and operational needs of the City. The city will need to continually develop and update its business plans for its trails, facilities, and rentable venues it manages. Managing good data is crucial to making good decisions on revenue development.

Primary and Funding Sources for Consideration

Bond Financing

Municipal bonds can be issued to finance trails projects. These bonds are backed by the government and typically repaid using tax revenues or user fees associated with the facility. Bond financing allows for large-scale projects that may require significant upfront investment that address needs that are identified as unmet community priorities.

Impact Fees

Impact fees are charges imposed on new developments to mitigate the impacts they have on the community, such as increased demand for parks and recreation facilities. These fees may be collected by the city and used to fund the construction or expansion of a trails network and infrastructure.

Park/Trail Development Fees

Many municipalities seek developer contributions for parklands and for the development of trails that run through the property being developed. The developer perceives the enhanced value in terms of what the improvements mean for their development. Park or trail dedication as a requirement of subdivision development is a reliable means for maintaining equity of access to parks and trails and keeping pace with the neighborhood and community park needs of the city.

Partnerships

Partnerships are joint-development funding sources or operational funding sources formed between separate agencies, such as two government entities, a non-profit, a public agency, or a private business, local improvement district and a public agency. Partners jointly develop revenue-producing parks and recreation facilities and may share risk, operational costs, responsibilities, and asset management based on the strengths of each partner. In this approach, entities may provide funding or resources in exchange for specific benefits, such as naming rights, advertising opportunities, or revenue-sharing agreements.

The City currently utilizes partnerships and agreements that are joint-development funding sources or operational funding sources between separate agencies. Revisiting the terms of partnerships is important to ensure the agreement's terms are still valid and the need for partnership has not changed. Existing partnership agreements should be updated regularly to ensure the purpose of the partnership, what is being provided, and the terms of the agreement are accurate. New partnerships should be forged as the city continues to evolve and modernize the parks and recreation system to align with community needs. Ensure that partnerships are fair and equitable to the city and to the partner with the best interests of the community prioritized.

Corporate Sponsorships

Trail projects can seek corporate sponsorships to secure funding. In exchange for financial support, sponsors may receive branding opportunities, advertising exposure, or other promotional benefits.

Government Grants

Trails projects can often qualify for government grants from federal, state, and local agencies. These grants are typically awarded based on the project's alignment with specific criteria, such as community impact, environmental sustainability, or economic development. The city may utilize federal grant funding, such as Community Development Block Grant (CDBG) funding, for trail projects and can continue to mine for additional federal, state, and local grant funding opportunities.

Franchise Fee for Utility Right-of-Way

Many agencies have sold the development rights below the ground to utility companies for fiber optic lines, water, sewer, electricity lines, and cable conduits on a linear foot basis.

Maintenance Endowment Fund

This is a fund dedicated exclusively for trail maintenance and is funded by a percentage of user fees from programs, events, and rentals. The fee is paid by users and is added to a dedicated fund for facility and equipment replacement.

Revolving Fund

A revolving fund is a dedicated financial mechanism established to support the ongoing development, enhancement, and maintenance of trails facilities. The primary purpose of these funds is to generate revenue through various means, such as rental fees, sponsorships, donations, and grants. The generated funds are then reinvested back into the parks to improve amenities, infrastructure, programming, and overall trails experiences. Park revolving funds offer a sustainable financial mechanism for supporting trail development and maintenance. By reinvesting generated revenues into related projects, these funds ensure the long-term sustainability and improved amenities of trail facilities. Successful implementation requires careful planning, stakeholder engagement, and effective financial management.

Philanthropic Donations

Non-profit organizations, foundations, and individual donors can contribute funds to support parks and recreation projects. These donations are often made in the form of grants, sponsorships, or endowments. Philanthropic donations can be essential for funding projects that benefit specific communities or have a social or environmental focus.

Volunteerism

This is an indirect revenue source consisting of individuals donating their time to assist the city in providing a product or service on an hourly basis. This reduces the city's cost to provide services, builds department advocacy, and civic involvement.



08

IMPLEMENTATION



Future Implementation Decisions:

The following implementations decisions will need to be addressed by the City in order to effectively implement the Pathway Master Plan into reality.

- a. Adopt the Pathway Master Plan by reference into the City of Star Comprehensive Plan.
- b. Update City GIS database system to include Pathway Plan routes.
- c. Update Development Code regulations to require construction of pathways upon parcel redevelopment consistent with the sections and standards of this plan.
- d. Confirm how much City funding is available and plan pathway construction projects accordingly.
- e. Secure additional funding through grants, bonds, and public-private partnerships to support pathway projects.
- f. Partner with regional authorities and state agencies for cost-sharing transportation projects and inclusion of community projects within transportation improvement programs.
- g. Review Impact Fees for inclusion of pathway development.
- h. Confirm who maintains which pathway types.
- i. Secure pathway agreements with irrigation and drainage districts to allow their right-of-way to be utilized as community pathways.
- j. Coordinate with existing HOA's on conversion of existing private pathways to public pathways.
- k. Apply for SS4A Planning and Demonstration Grant in order to complete area-wide safety action plan resulting in recommended safety improvement projects for all transportation modes. This does not necessarily guarantee a pathway project but if there is a demonstrated risk or crash history it could.
- l. Promote community programs and events that encourage walking, biking and use of shared pathways such as Bike/Walk to School Days, Bike Bus Programs, Bike Repair/Donation Drives, Bike Safety Trainings, 5Ks, or Charity Walk-a-Thon Fundraisers.
- m. Develop a Facilities Maintenance Plan for enhancement and maintenance of City parks and existing pathways.
- n. Conduct necessary property acquisition or easements as needed, specifically along the Boise River.
- o. Update City Code Enforcement and Maintenance responsibilities to include oversight of existing City pathways consistent with the establish level of service guidelines.

Appendix

SOCIAL PINPOINT RESULTS SURVEY FEEDBACK



Closed

City of Star Pathway Survey
City of Star Pathway Master Plan

100
Contributors

105
Contributions

Contribution Summary

1. Are you a City of Star Resident?

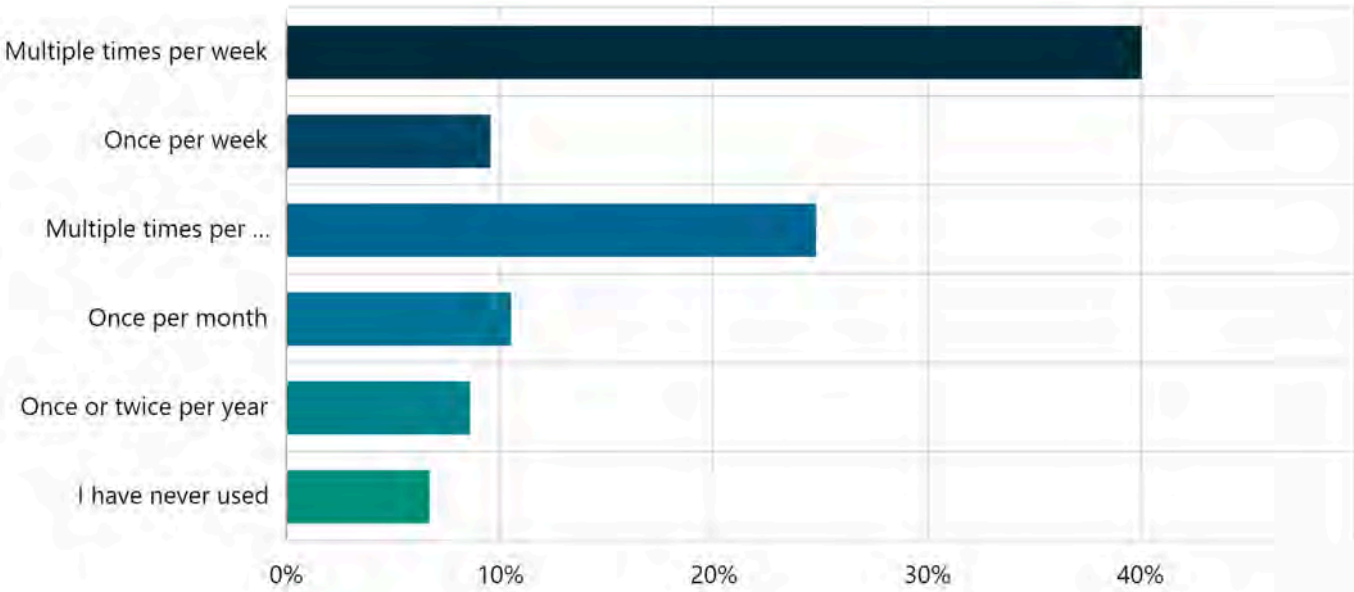
Select Box | Skipped: 2 | Answered: 103 (98.1%)



Answer choices	Percent	Count
Yes	98.06%	101
No	1.94%	2
Total	100.00%	103

2. How often do you use the multi-use paths in and around the City of Star?

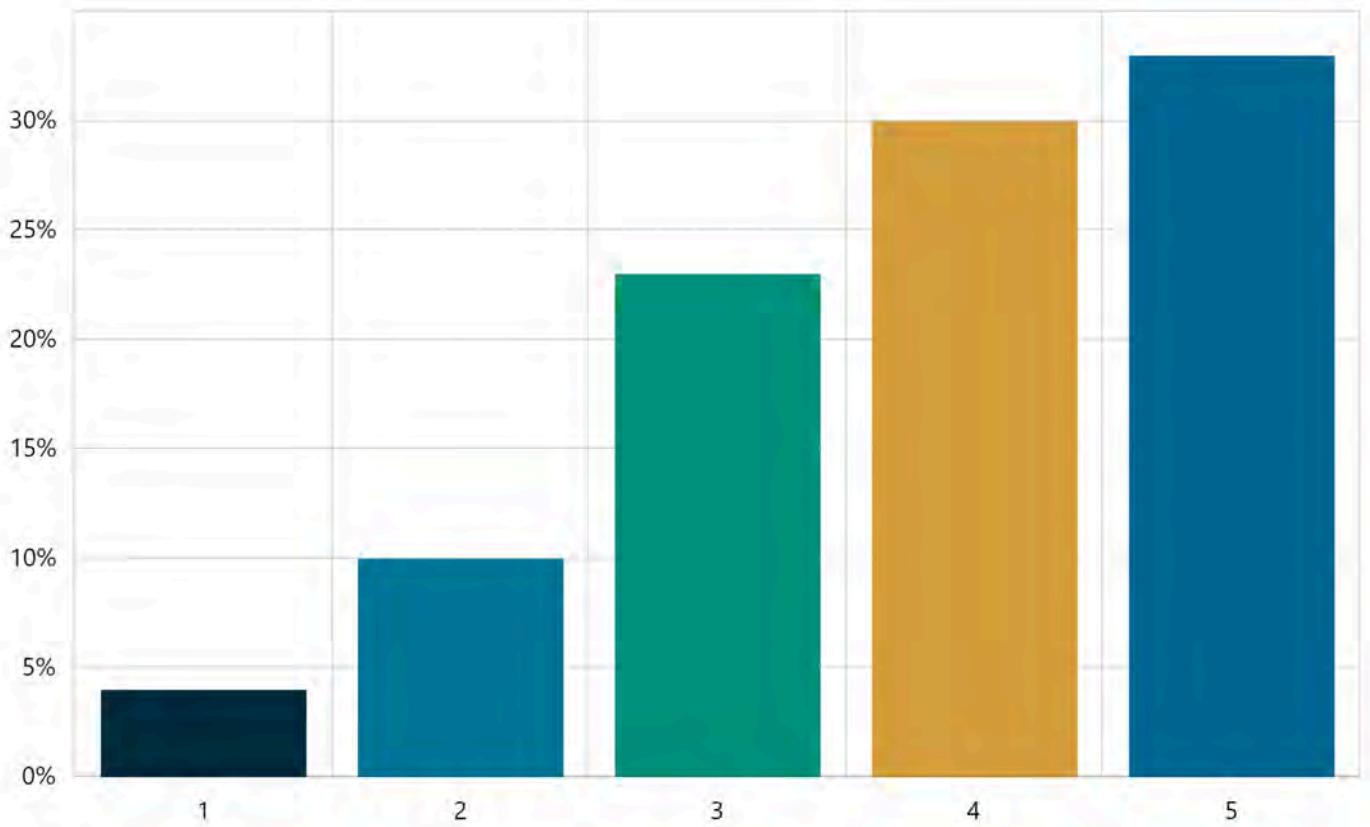
Multi Choice | Skipped: 0 | Answered: 105 (100%)



Answer choices	Percent	Count
Multiple times per week	40.00%	42
Once per week	9.52%	10
Multiple times per month	24.76%	26
Once per month	10.48%	11
Once or twice per year	8.57%	9
I have never used	6.67%	7
Total	100.00%	105

3. Please rate your level of satisfaction with the feeling of safety on multi-use paths.

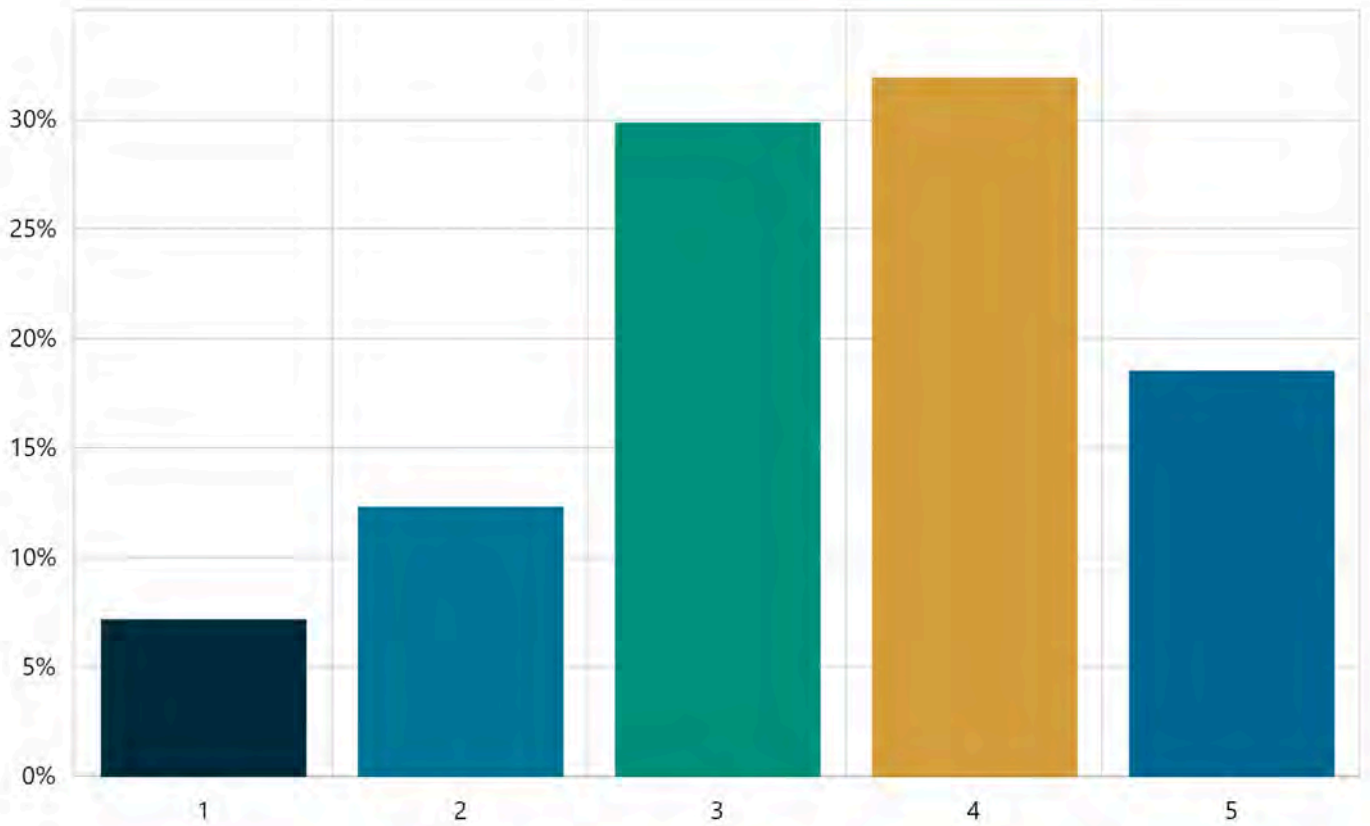
Rating | Skipped: 5 | Answered: 100 (95.2%)



1	2	3	4	5	Count	Weighted average
4.00%	10.00%	23.00%	30.00%	33.00%	100	3.78
4	10	23	30	33		

4. Please rate your level of satisfaction with the number of resting points and benches along the multi-use paths.

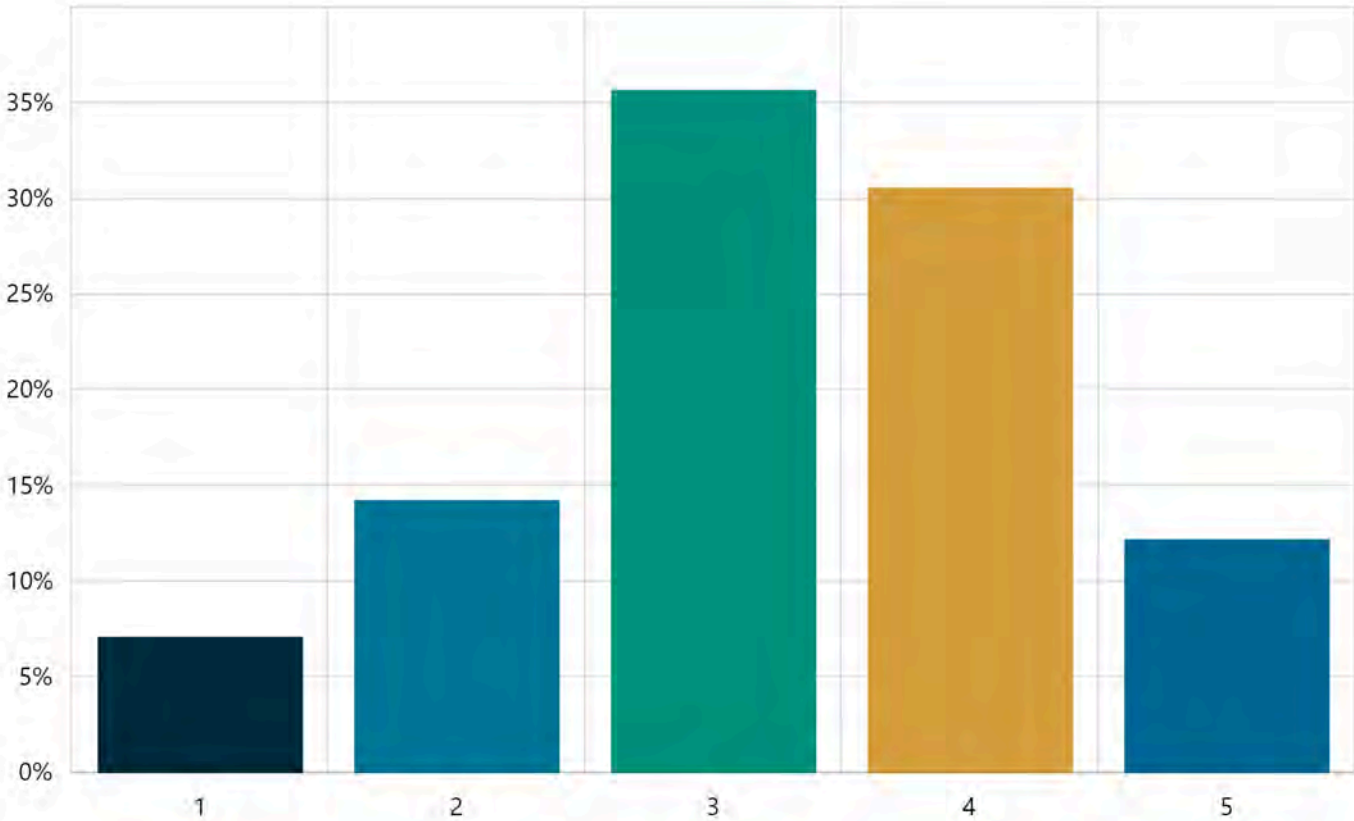
Rating | Skipped: 8 | Answered: 97 (92.4%)



1	2	3	4	5	Count	Weighted average
7.22%	12.37%	29.90%	31.96%	18.56%	97	3.42
7	12	29	31	18		

5. Please rate your level of satisfaction with the multiuse path signage and identification.

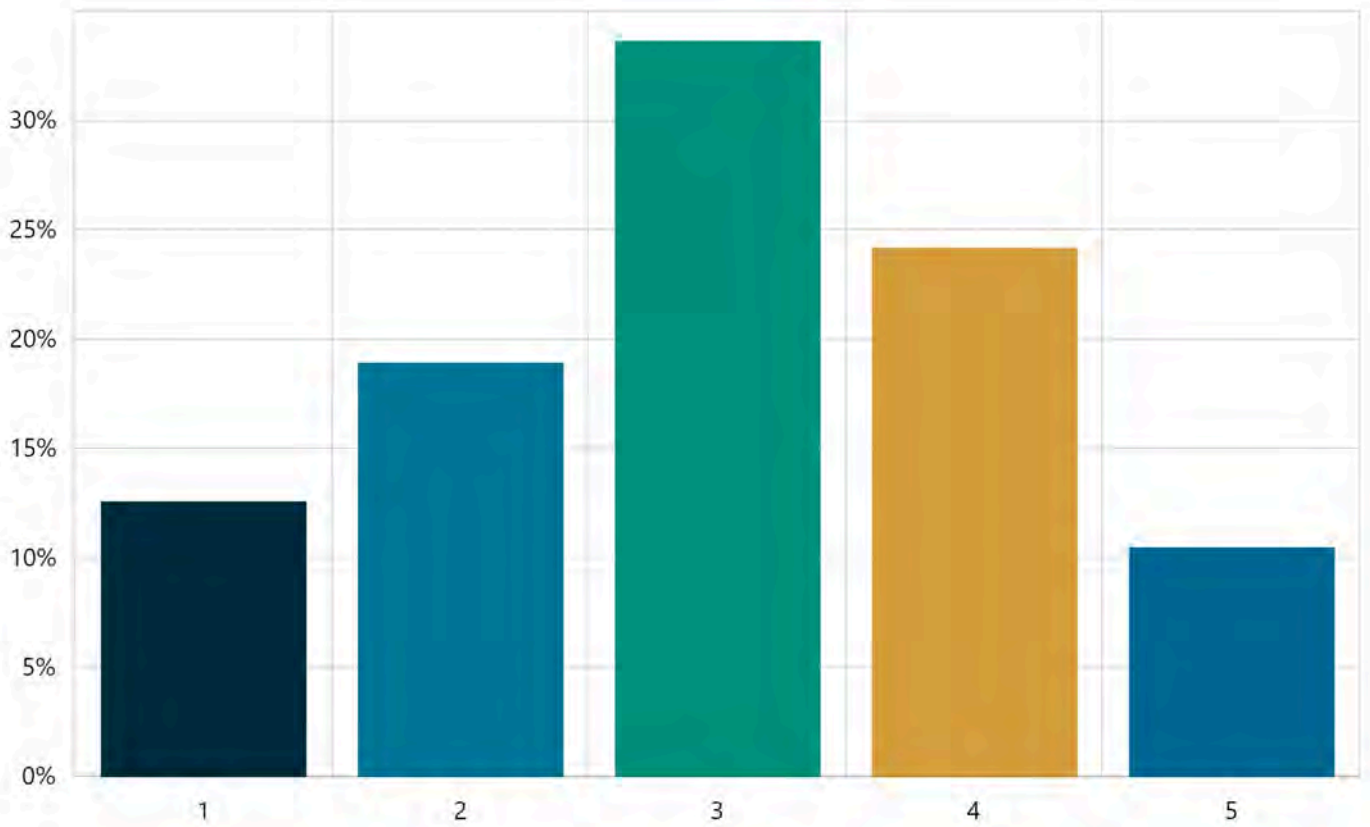
Rating | Skipped: 7 | Answered: 98 (93.3%)



1	2	3	4	5	Count	Weighted average
7.14%	14.29%	35.71%	30.61%	12.24%	98	3.27
7	14	35	30	12		

6. Please rate your level of satisfaction with the adequacy of lighting along multi-use paths.

Rating | Skipped: 10 | Answered: 95 (90.5%)



1	2	3	4	5	Count	Weighted average
12.63%	18.95%	33.68%	24.21%	10.53%	95	3.01
12	18	32	23	10		

7. Would you be willing to participate in future surveys or focus groups for the City of Star? If so, please provide your contact information below. Your information will be kept confidential.

Short Text | Skipped: 68 | Answered: 37 (35.2%)

Sentiment

No sentiment data

Tags

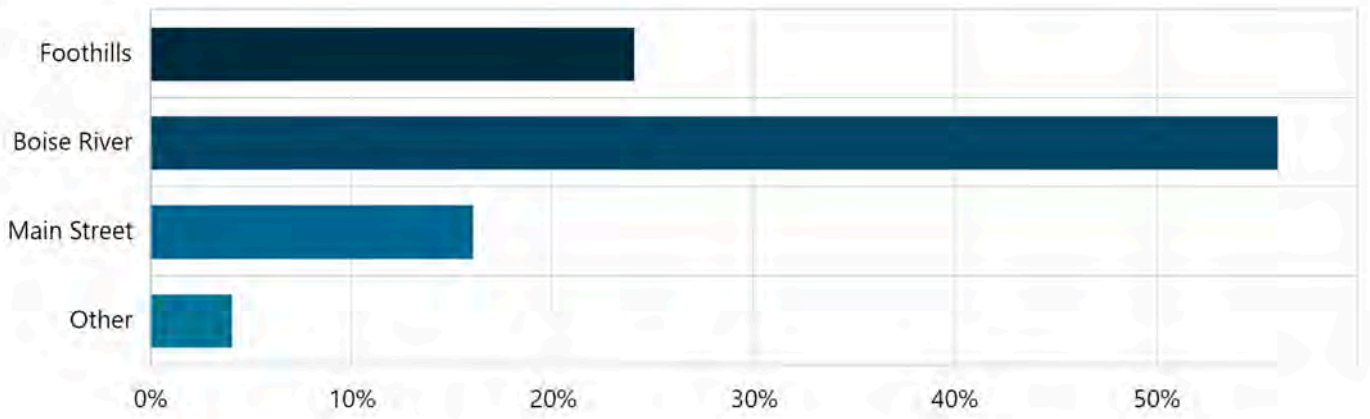
No tag data

Featured Contributions

No featured contributions

8. Which areas of the City should be prioritized for new trails projects and connections?

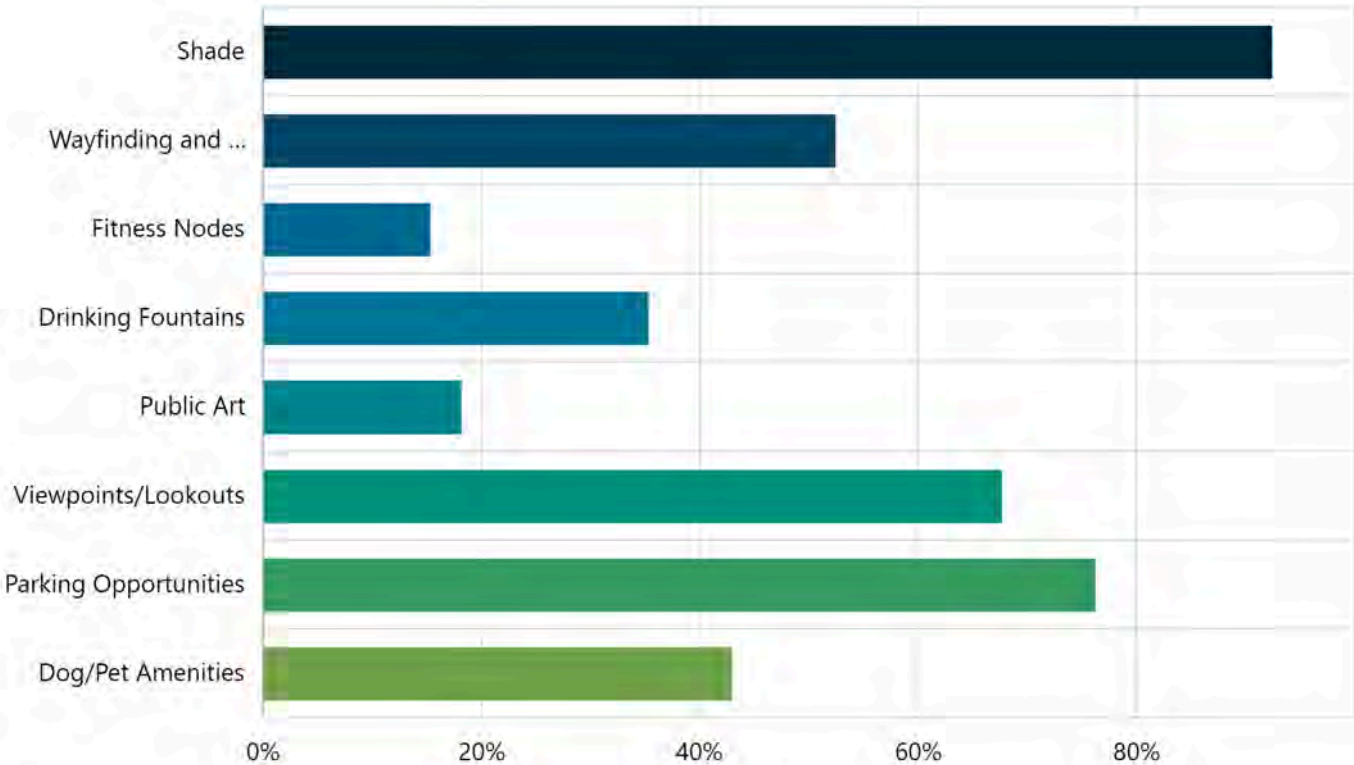
Multi Choice | Skipped: 5 | Answered: 100 (95.2%)



Answer choices	Percent	Count
Foothills	24.00%	24
Boise River	56.00%	56
Main Street	16.00%	16
Other	4.00%	4
Total	100.00%	100

9. Which FOUR trail features are the most important to you? (select four)

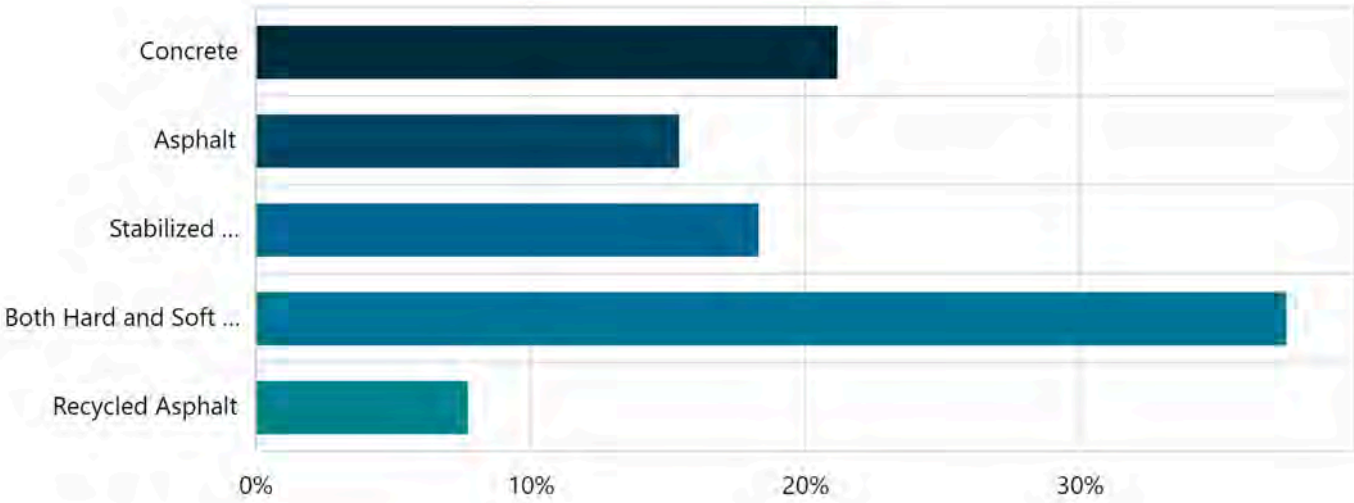
Multi Choice | Skipped: 0 | Answered: 105 (100%)



Answer choices	Percent	Count
Shade	92.38%	97
Wayfinding and Interpretive Signage	52.38%	55
Fitness Nodes	15.24%	16
Drinking Fountains	35.24%	37
Public Art	18.10%	19
Viewpoints/Lookouts	67.62%	71
Parking Opportunities	76.19%	80
Dog/Pet Amenities	42.86%	45

10. What is your preference for the multi-use path and trail surface?

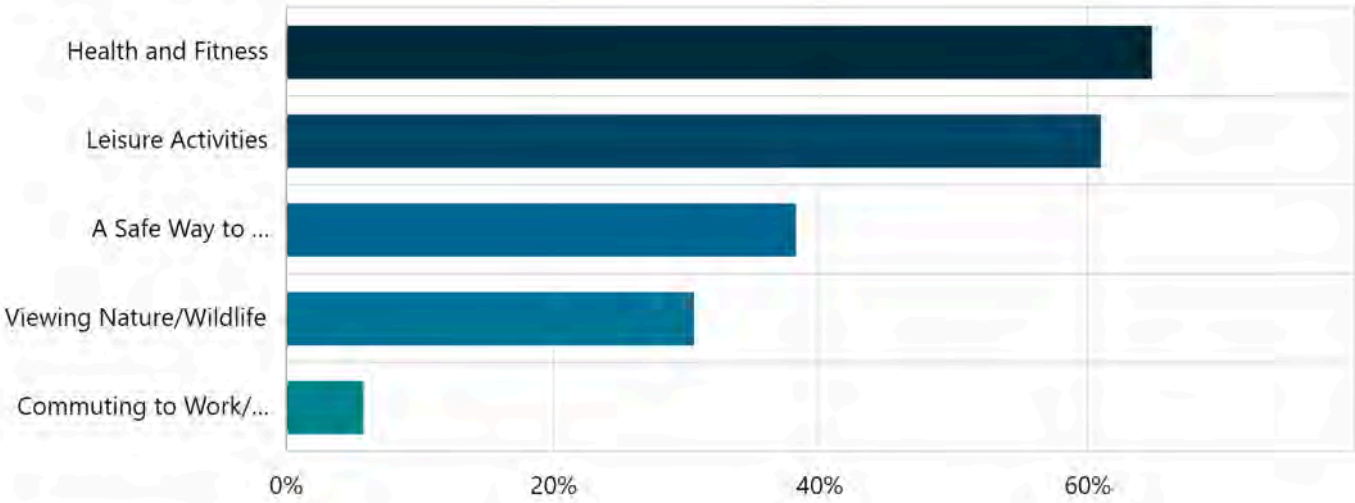
Multi Choice | Skipped: 1 | Answered: 104 (99%)



Answer choices	Percent	Count
Concrete	21.15%	22
Asphalt	15.38%	16
Stabilized Decomposed Granite/Millings	18.27%	19
Both Hard and Soft Surfaces	37.50%	39
Recycled Asphalt	7.69%	8
Total	100.00%	104

11. What are the TWO most important reasons you will use the multi-use paths and trails?

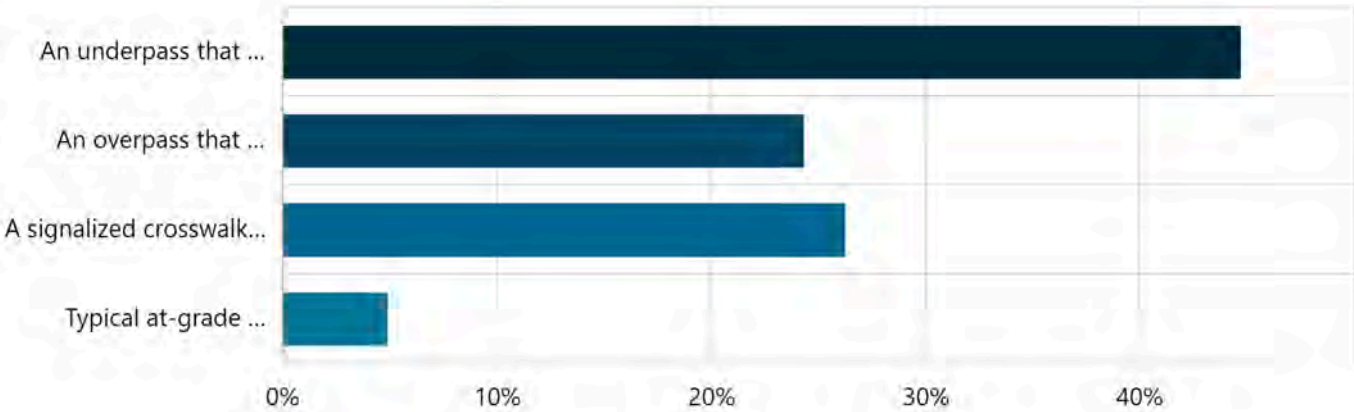
Multi Choice | Skipped: 0 | Answered: 105 (100%)



Answer choices	Percent	Count
Health and Fitness	64.76%	68
Leisure Activities	60.95%	64
A Safe Way to Connect to Surrounding Areas	38.10%	40
Viewing Nature/Wildlife	30.48%	32
Commuting to Work/School/Other Specific Location	5.71%	6

12. What is your preference for multi-use path/trail and street crossings?

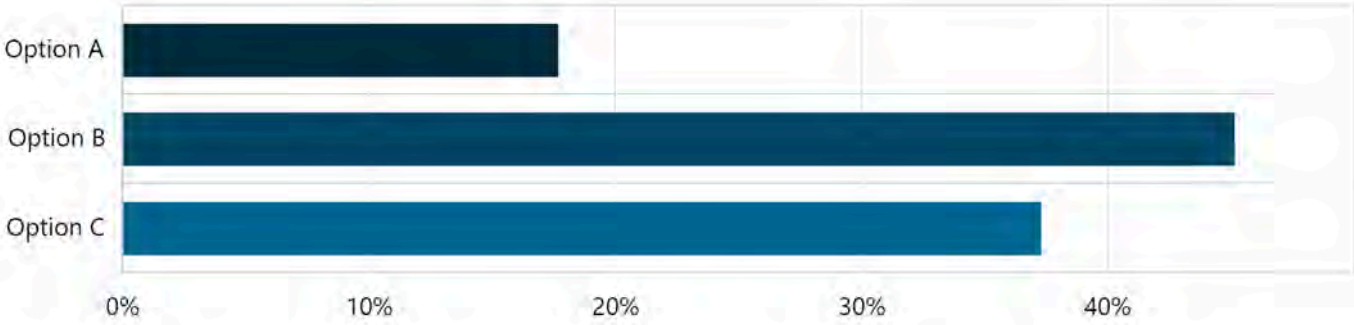
Multi Choice | Skipped: 2 | Answered: 103 (98.1%)



Answer choices	Percent	Count
An underpass that goes under the main roadway	44.66%	46
An overpass that goes over the main roadway	24.27%	25
A signalized crosswalk specifically for path/trail users	26.21%	27
Typical at-grade crossing	4.85%	5
Total	100.00%	103

13. As we look to finalize the branding for the City of Star Path Master Plan, which logo is your preference?

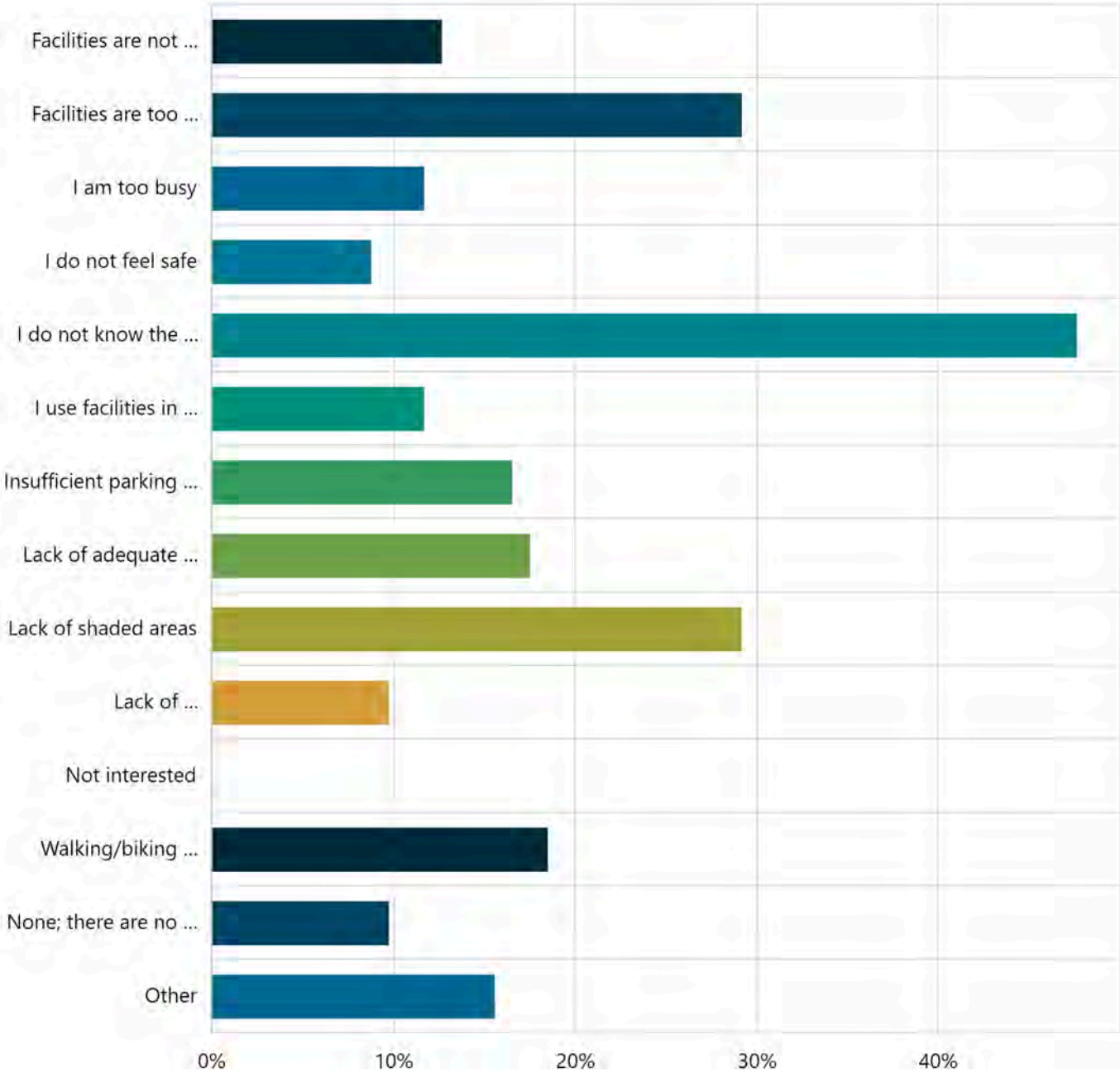
Multi Choice | Skipped: 3 | Answered: 102 (97.1%)



Answer choices	Percent	Count
Option A	17.65%	18
Option B	45.10%	46
Option C	37.25%	38
Total	100.00%	102

14. What are the reasons you have not utilized or have not utilized the local paths/trails more often? (Check ALL that apply)

Multi Choice | Skipped: 2 | Answered: 103 (98.1%)

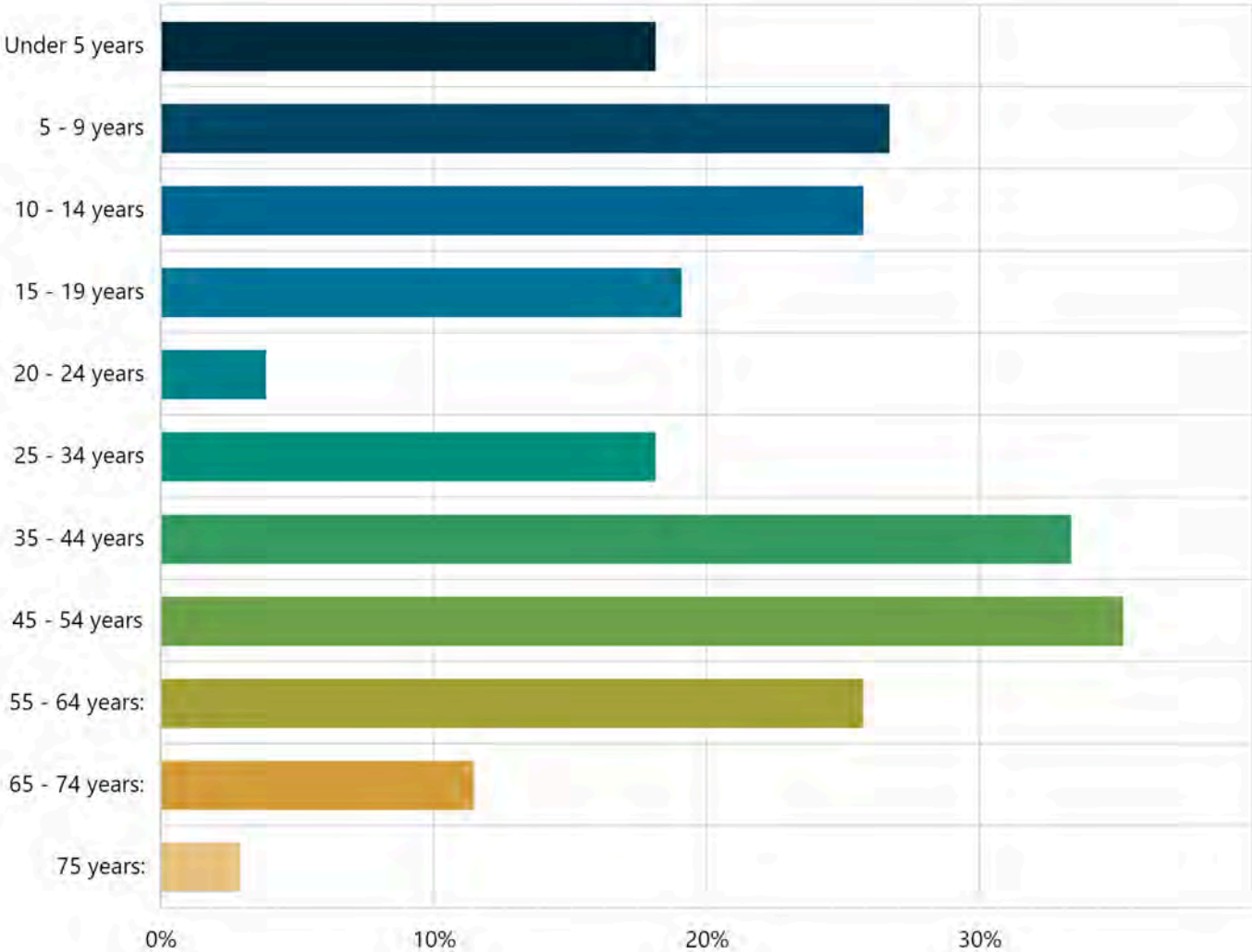


Answer choices	Percent	Count
Facilities are not well maintained	12.62%	13
Facilities are too far from my home	29.13%	30
I am too busy	11.65%	12
I do not feel safe	8.74%	9
I do not know the locations	47.57%	49

I use facilities in other communities	11.65%	12
Insufficient parking opportunities for access	16.50%	17
Lack of adequate amenities that meet my needs	17.48%	18
Lack of shaded areas	29.13%	30
Lack of transportation options to get to multi-use paths/trails	9.71%	10
Not interested	0%	0
Walking/biking routes are not safe	18.45%	19
None; there are no barriers	9.71%	10
Other	15.53%	16

15. Counting yourself, check the age ranges of people in your household: (Check ALL that apply)

Multi Choice | Skipped: 0 | Answered: 105 (100%)

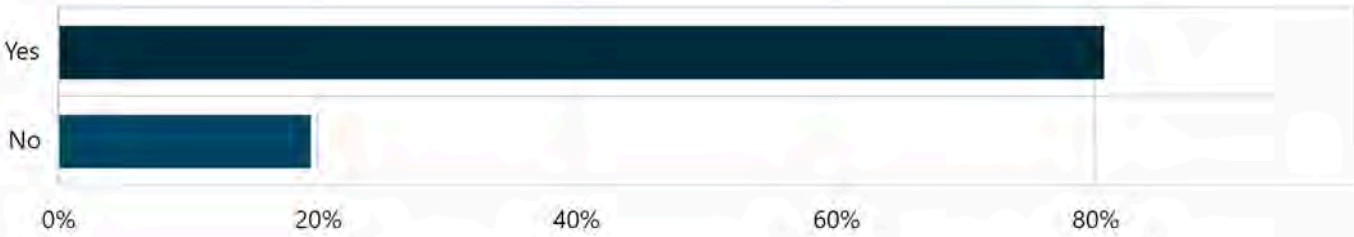


Answer choices	Percent	Count
Under 5 years	18.10%	19
5 - 9 years	26.67%	28
10 - 14 years	25.71%	27
15 - 19 years	19.05%	20
20 - 24 years	3.81%	4
25 - 34 years	18.10%	19
35 - 44 years	33.33%	35
45 - 54 years	35.24%	37
55 - 64 years:	25.71%	27
65 - 74 years:		
75 years:		

65 - 74 years:	11.43%	12
75 years:	2.86%	3

16. Would you support creating a Local Improvement District to help fund construction of new pathways?

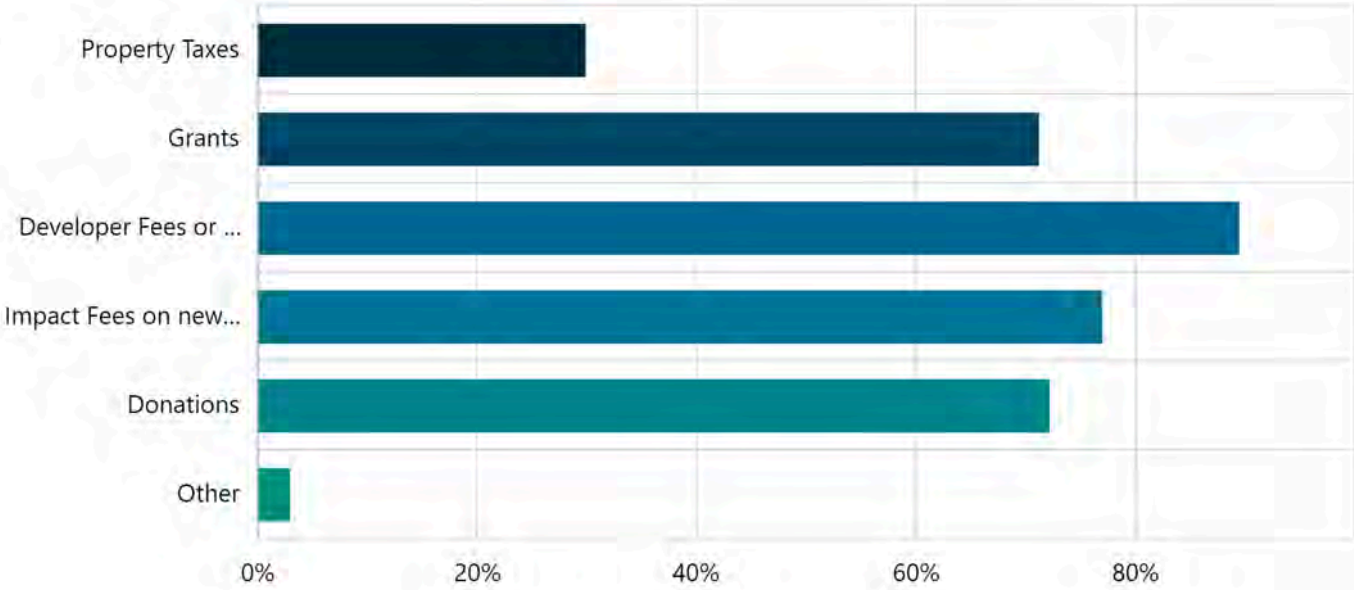
Select Box | Skipped: 7 | Answered: 98 (93.3%)



Answer choices	Percent	Count
Yes	80.61%	79
No	19.39%	19
Total	100.00%	98

17. Which of the following funding sources do you support using for pathway construction? (select all that apply)

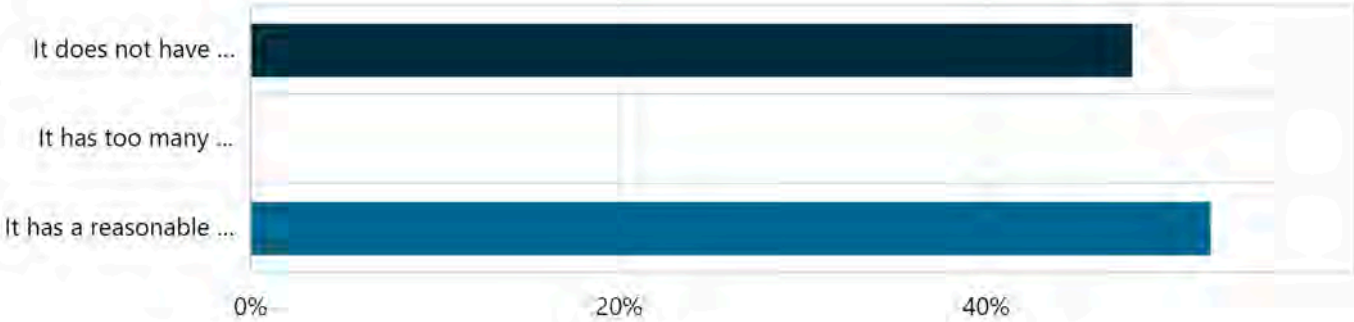
Multi Choice | Skipped: 1 | Answered: 104 (99%)



Answer choices	Percent	Count
Property Taxes	29.81%	31
Grants	71.15%	74
Developer Fees or construction	89.42%	93
Impact Fees on new development	76.92%	80
Donations	72.12%	75
Other	2.88%	3

18. When looking at the project map depicting proposed pathways which of the following do you most agree with?

Multi Choice | Skipped: 11 | Answered: 94 (89.5%)



Answer choices	Percent	Count
It does not have enough pathways	47.87%	45
It has too many pathways	0%	0
It has a reasonable amount of pathways	52.13%	49
Total	100.00%	94


Closed

Help Us Determine Parks Priorities
City of Star Pathway Master Plan


39 Contributors | **40** Contributions

Key Statistics


Top-level information about the activity.



\$99.75
Average Spend
\$100.00 total budget



9
Projects
0 sponsors



208
Total Votes
5.2 avg. projects funded / contribution

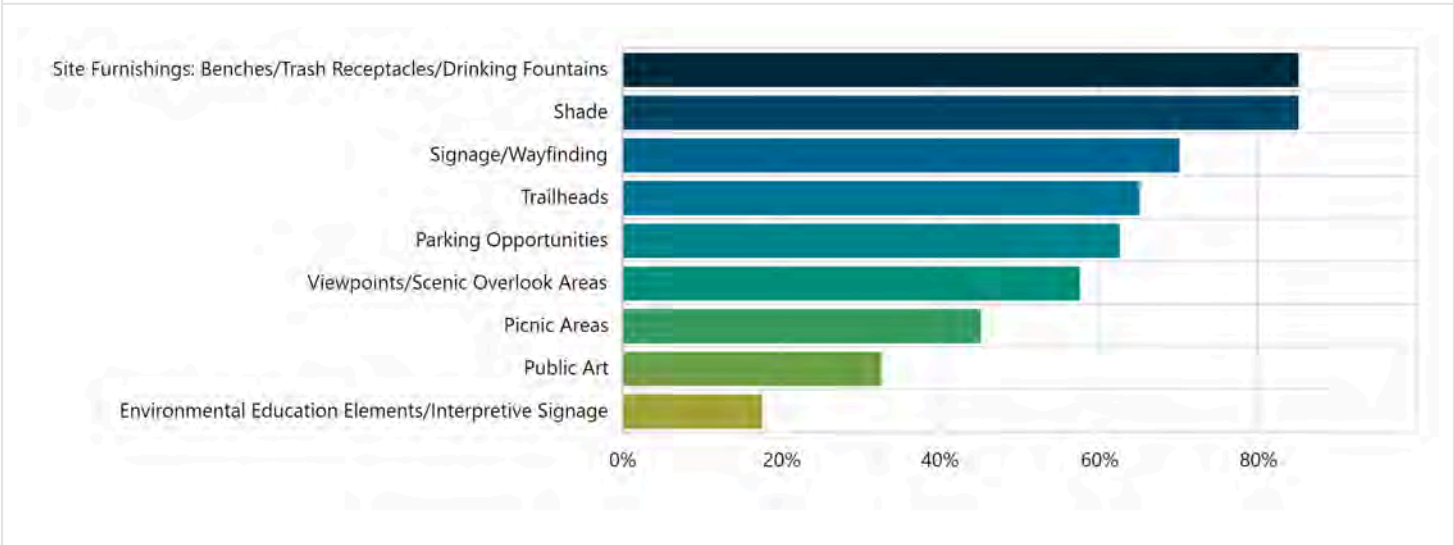
Contribution Summary

Summary of the activity including details of the included projects, voting results and more.

Project Details			
Information on the projects included for potential funding including the name, cost, voting and other details.			
Project Name	Cost	Total Votes	%
Site Furnishings: Benches/Trash Receptacles/Drinking Fountains	\$10.00	34	16.35%
Shade	\$20.00	34	16.35%
Signage/Wayfinding	\$10.00	28	13.46%
Trailheads	\$40.00	26	12.5%
Parking Opportunities	\$20.00	25	12.02%
Viewpoints/Scenic Overlook Areas	\$20.00	23	11.06%
Picnic Areas	\$20.00	18	8.65%
Public Art	\$20.00	13	6.25%
Environmental Education Elements/Interpretive Signage	\$10.00	7	3.37%

Voting Results

Results of participant voting showing which projects were most and least funded (calculated as project votes / total contributions).



Votes by Project Tag

Comparison showing the number of times a project tag was included on a funded project. Vote totals may exceed the total number of project votes.

No Data Available





Votes by Project Sponsor





Comparison showing the number of times a project sponsor was included on a funded project. Vote totals may exceed the total number of project votes.

No Data Available

Suggested Priorities

A prioritised list of projects based on the voting results that maximises the value of the defined budget.

 <p>\$100.00 Total budget for allocated projects</p>	 <p>100% Contributions that include at least one suggested project</p>	 <p>97.5% Contributions that include at least half (50%) of all suggested projects</p>	 <p>12.5% Contributions that include all (100%) of suggested projects</p>
--	--	--	---

	<p>Site Furnishings: Benches/Trash Receptacles/Drinking Fountains</p> <p>Cost: \$10.00</p> <p>34 votes (16.35%)</p>
	<p>Shade</p> <p>Cost: \$20.00</p> <p>34 votes (16.35%)</p>
	<p>Signage/Wayfinding</p> <p>Cost: \$10.00</p> <p>28 votes (13.46%)</p>
	<p>Trailheads</p> <p>Cost: \$40.00</p> <p>26 votes (12.5%)</p>



Parking Opportunities

Cost: \$20.00

25 votes (12.02%)

Project Map

Title/Question: Interactive Map
 Tool Type: Social Map
 Activity ID: 5
 Report Date Range: 3 Jul 2024 - 5 Aug 2024
 Date Exported: 22 Aug 2024 11:01 pm
 Exported By: AAiello

Contribution ID	Date Submitted	Your comment	Screen name	Email address	Category			Address	Latitude	Longitude	Downvote	Upvote	Total Votes	Average Score	IP Address	Moderation Status
					Make a Comment	Something I Like	Ideas/Suggestions									
3475	Jul 30, 2024, 10:31 PM	Bent Lane really needs safe access into Star. The shoulder of the highway doesn't cut it currently.	Evan	elm4486@gmail.com			1	22003 Bent L	43.692207	-116.5173679	0	0	0	0	0F6602cee699d7a9	Approved
3474	Jul 30, 2024, 10:26 PM	Is an easement through this area even possible? If not possible in the foreseeable, any connection from these neighborhoods into Star would be really helpful.	Evan	elm4486@gmail.com	1			Middleton Ri	43.68319479	-116.5261692	0	0	0	0	0F6602cee699d7a9	Approved
3408	Jul 28, 2024, 05:27 PM	An overpass somewhere over State Street would help seniors and all pedestrians and those with bikes to get across more safely. The traffic is just too scary!	Karen					1 10717 West S	43.6916413	-116.4872556	0	0	0	0	0D6c02c94a7855bc	Approved
3402	Jul 27, 2024, 12:16 PM	In order to conserve the quiet neighborhood feeling of the areas on each side of Wing Rd., I would suggest that there are many paths into the park for access by foot and bikes.	Janeen	janeen5311@outlook.com				1 10200 West S	43.73029279	-116.4858547	0	1	1	1	1 0db802d6f858c0e31	Approved
3340	Jul 25, 2024, 12:34 PM	I would love to see connectivity between Pavilion Park and Hunters Creek, with a flashing beaoned crosswalk across Star Rd.	GuahanTriton	steve.abiador@gmail.com				1 1375 North S	43.70360184	-116.4932558	0	0	0	0	0Fcd02efca64886f	Approved
3336	Jul 25, 2024, 09:46 AM	We need pathways in the downtown area. More green space and pathways. That's the way desirable cities manage their growth.	John Pendergast	jpenderg1@yahoo.com				1 11500 West I	43.69296896	-116.497452	0	2	2	2	2 0c8b02a221bacf56	Approved
3335	Jul 25, 2024, 09:44 AM	Deerhaven should be identified as a pathway. It is used quite frequently as such and the current plan include no pathways in this area.	John Pendergast	jpenderg1@yahoo.com				1 West Hidden	43.70097553	-116.5047751	0	0	0	0	0 0c8b02a221bacf56	Approved
3330	Jul 24, 2024, 10:06 PM	This would bring way too much traffic to Wing & cause congestion in what is currently a 100% residential neighborhood.	T	tiffmuller69@gmail.com	1			4075 North P	43.73052151	-116.4863484	1	2	3	1	1 117e0358458cb6b2	Approved
3290	Jul 24, 2024, 01:35 AM	Side walk all the way along Beacon Light. No breaks where you have to go out to the edge of the street to continue walking.	Paul	paul.schaff@yahoo.com	1			9701 West Br	43.72081551	-116.4743147	0	3	3	3	3 10550324d7d5e5e6	Approved
		Please consider a complete pathway starting at the beginning of Brandon Road going all the way to the end. Multiple walkers, runners and people biking that do not have access to a safe pathway on this stretch. Thank you for your consideration!	Erin	erinsummers418@gmail.com				1 2370-2798 N	43.71561776	-116.4881497	0	2	2	2	2 12590356b6f3e79	Approved
3094	Jul 17, 2024, 06:33 PM	Can you install an all weather surface like some of the Ridge to Rivers system trails? It would allow for usage year round and is friendly for the elderly too. It also means less maintenance year round for the city.	Debbie	debbiefugate@yahoo.com				1 1000 South Iv	43.68165282	-116.4909692	0	1	1	1	1 0d4902c5a4e36709	Approved
		These BLM trails are great to have but hardly considered "paths" for walking/biking. They are rutted up and if their is any moisture the trails turn to peanut butter. I suggest a designated walking/biking all weather trail loop through the foothills.	Brandon Bohl	Brandanbohl@outlook.com				1 5975 North H	43.7529369	-116.4877631	0	4	4	4	4 0d7602d2a64b338b	Approved
3079	Jul 17, 2024, 03:08 PM	I suggest this section of N Can Ada Rd have a designated path. I've read this road is getting redeveloped in the near future so we should incorporate a robust path then. This area could be the most beautiful part of Star.	Brandon Bohl	Brandanbohl@outlook.com				1 North Can Ad	43.72559839	-116.5121936	0	3	3	3	3 0d7602d2a64b338b	Approved
3077	Jul 17, 2024, 02:59 PM	To not have a path here connecting to the foothills would be a mistake in my opinion. I suggest a robust bike/walking path with landscape island/median. The landscape median will help protect bikers/pedestrians from traffic and add beauty and shade for the path.	Brandon Bohl	Brandanbohl@outlook.com				1 10334 West S	43.69168847	-116.4825462	0	3	3	3	3 0d7602d2a64b338b	Approved
3074	Jul 17, 2024, 02:26 PM	This is priority as their is no safe way to bike/walk down State street... the heartbeat of our town.	Brandon Bohl	Brandanbohl@outlook.com				1 1000 South Iv	43.68144905	-116.4892858	0	2	2	2	2 0d7602d2a64b338b	Approved
3073	Jul 17, 2024, 02:11 PM	I suggest we start with improving the star river walk. Create an Asphalt wide path (wider the better) as far as possible. Asphalt will attract more of the community including the elderly.	Dina	dinathed@gmail.com				1 22153 North	43.69395294	-116.5126593	0	6	6	6	6 0ec602fdbcc03704	Approved
3048	Jul 17, 2024, 08:23 AM	We need a signal light at N Can Ada with new businesses and housing developments coming to this part of town.	Dina	dinathed@gmail.com				1 South Rippler	43.69272802	-116.5086206	0	2	2	2	2 0ec602fdbcc03704	Approved
3047	Jul 17, 2024, 08:16 AM	We need a walking path on the corner of State and Ripplerock in front of Christian's Brothers. We also need more trees lining that side of the street.	Dina	dinathed@gmail.com												
		Are these pathways shown on the correct side of the canals? Or is this just diagrammatic? Because you are showing a path through property that is NOT within the city boundary!														
		The city needs to take all of their previous building approvals into consideration with the location of these paths. There are multiple houses throughout the city that were approved to be built to all set backs. Meaning the house is really too big for the lot so the back of the house is 15' from the back fence that in many cases are required to be transparent! When these houses back onto a proposed public path you are placing peoples limited private space against an open public space and it makes their homes very uncomfortable and vulnerable to crime! The city of Star is supposed to be a small town, but you have been building it out to dense urban standards for years. Now you want to put public paths through the small areas of private space your voters have. This will likely upset quite a number of your voters when they find out!	MRG	m_r_blish@yahoo.com	1			11900 West T	43.71391634	-116.5017977	0	3	3	3	3 0be00270e90572eb	Approved
2788	Jul 16, 2024, 09:35 AM	Access to the river trail at the south end of Canada would be great for the population living on the west end of star, preventing having to travel through town to access the river, reducing traffic.	Debbie	debbiefugate@yahoo.com	1			Middleton Ri	43.68213391	-116.5126685	0	5	5	5	5 0d4902c5a4e36709	Approved
2562	Jul 13, 2024, 04:29 PM	Being able to cross to the South side of the river would add miles to the trail system, which would be great.	Debbie	debbiefugate@yahoo.com	1			Central Valley	43.68261039	-116.4627682	0	3	3	3	3 0d4902c5a4e36709	Approved
2560	Jul 13, 2024, 04:25 PM	Please develop a path as far east as possible on the river. Star is missing out on a huge recreation asset by not utilizing this prime riverfront land.	Debbie	debbiefugate@yahoo.com	1			976 South Hic	43.68349564	-116.4404253	0	3	3	3	3 0d4902c5a4e36709	Approved
2559	Jul 13, 2024, 11:30 AM	A paved pathway along Middleton Canal to connect to Hunter's Creek park would be appreciated.	Garlick	gorjarlick@gmail.com				1 11624 West I	43.70270925	-116.4984422	0	2	2	2	2 0df302cb050103b8	Approved
2558	Jul 12, 2024, 09:11 PM	This would be a vert nice loop.	JMP	julpip24@gmail.com		1		1162 North K	43.70239944	-116.4890653	0	0	0	0	0 0e6902d3ecb9fd84	Approved
2557	Jul 12, 2024, 09:07 PM	This will be a great pathway!	JMP	julpip24@gmail.com		1		1890 North Ju	43.70947686	-116.4830561	0	0	0	0	0 0e6902d3ecb9fd84	Approved
2555	Jul 12, 2024, 09:02 PM	I agree. We need more walking pathways. This spot would be ideal.	Jer B	jdbmgt@gmail.com		1		10220 West \	43.70841201	-116.4811829	0	0	0	0	0 0e6902d3ecb9fd84	Approved
2501	Jul 10, 2024, 11:37 PM	A walking path/sidewalk to connect Can Ada and New Hope would be great for kids and families. Currently the sidewalk stops suddenly and doesn't connect to this intersection	BR	benjrose1214@gmail.com				1 12576 West \	43.7174613	-116.512428	0	9	9	9	9 12d3038b2501ebf9	Approved
		Cleaning and organizing Foothill ditch walkway to become a real trail and walkway. Currently dirt and trash. No bridge across the creek (have to go around). Would love to see a fully developed and cleaned trail way for locals and possibly connect to a park nearby.	BR	benjrose1214@gmail.com				1 12524 West L	43.71668406	-116.5094759	0	7	7	7	7 12d3038b2501ebf9	Approved
2500	Jul 10, 2024, 11:32 PM	Absolutely need more pathways near Colt Place.	James		1			10183 West \	43.70868006	-116.4804628	0	1	1	1	1 0e6902d3ecb9fd84	Approved
2478	Jul 10, 2024, 09:19 PM	Yes, we need a park on north side! Playground with little kid access would be great	Kim	Kimhoagland.208@gmail.com		1		11304 West \	43.71857667	-116.4944363	0	1	1	1	1 0d1902cbf6915508	Approved
2470	Jul 10, 2024, 08:26 PM	I like the idea of connecting BLM to Star city limits.	MK	Kimhoagland.208@gmail.com		1		4543 North H	43.7321163	-116.4856251	0	1	1	1	1 0d1902cbf6915508	Approved
2469	Jul 10, 2024, 08:22 PM	Need A safer way to ride a bike or walk down Can Ada road	Kim	Kimhoagland.208@gmail.com		1		24005 North	43.7283025	-116.5125222	0	10	10	10	10 0d1902cbf6915508	Approved
2468	Jul 10, 2024, 08:21 PM	Really like the idea of greenbelt to Eagle for commuting	Kim	Kimhoagland.208@gmail.com		1		976 South Hic	43.68317093	-116.4382941	0	7	7	7	7 0d1902cbf6915508	Approved
2467	Jul 10, 2024, 08:19 PM	This should be listed as "Existing Pathway".	Mk	michael.w.keyes@gmail.com		1		Star Element:	43.69906049	-116.492049	0	1	1	1	1 0f1302dbfb365f56	Approved
2422	Jul 10, 2024, 04:07 PM	This should be listed ad "Existing Pathway".	Mk	michael.w.keyes@gmail.com		1		12025 West C	43.68791698	-116.5037531	0	1	1	1	1 0f1302dbfb365f56	Approved
2421	Jul 10, 2024, 04:06 PM	We enjoy this pathway and would like to see the rest of it completed through the neighborhood.	Garlick	gorjarlick@gmail.com		1		11672 West E	43.69915836	-116.4979662	0	2	2	2	2 12770355459435d2	Approved
2419	Jul 10, 2024, 03:56 PM	It would be wonderful if this path connected Mira and Taurus as an alternate, and safer, route for bikers to get across town. It would also be beneficial for schoolchildren.	K. Nellis	KN58@protonmail.com				1 10554 West S	43.69435932	-116.4862203	0	2	2	2	2 126d03885eba9596	Approved
2246	Jul 03, 2024, 11:03 PM	I would like to see sidewalks on both sides of State Street through the main part of Star. The area with the two lane expansion going west is especially dangerous for bikers and pedestrians.	Kathy Nellis	KN58@protonmail.com				1 10580 West S	43.69194346	-116.485612	0	11	11	11	11 126d03885eba9596	Approved



(<https://engagekh.mysocialpinpoint.com/StarPathwayMP>)

Forum Topic

City of Star Pathway Master Plan (<https://engagekh.mysocialpinpoint.com/StarPathwayMP>) / Share Your Big Idea (<https://engagekh.mysocialpinpoint.com/StarPathwayMP/forum>)

We appreciate your participation.

Please provide your input for the future of the City of Star Parks, Recreation, Open Space, and Pathways in the comment box below.

Add Comment

Adding new posts is disabled for this conversation.

12 Comments

Most Recent First



Evan | Posted on Jul 30, 2024

A connection from Bent Ln into Star is crucial. We are part of the city, but are on an island with no safe way to access town besides driving. As for parks, shaded play areas for the kids would be a very nice addition.

|

✕ (<https://twitter.com/intent/tweet?url=https%3A%2F%2Fengagekh.mysocialpinpoint.com%2FStarPathwayMP%2Fforum%2Fwe-appreciate-your-participation-1%23cnv3Message16>)

📘 (<https://www.facebook.com/sharer.php?u=https%3A%2F%2Fengagekh.mysocialpinpoint.com%2FStarPathwayMP%2Fforum%2Fwe-appreciate-your-participation-1%23cnv3Message16>)



KC | Posted on Jul 27, 2024

I would love to see a livelier, more walkable downtown in Star, with pedestrian friendly pathways, a plaza for gatherings with shade cover, and more businesses with outdoor dining.

|

✕ (<https://twitter.com/intent/tweet?url=https%3A%2F%2Fengagekh.mysocialpinpoint.com%2FStarPathwayMP%2Fforum%2Fwe-appreciate-your-participation-1%23cnv3Message11>)

Section 7, Item B.

Respondent ID	Collector ID	Start Date	End Date	IP Address	Email Address	First Name	Last Name	Custom Data 1	After viewing the prioritized trail segments, do you agree with the prioritized trail segments shown on map (pg 37-40)?	Are there any segments missing, you would like to see included?	Are there any particular connections to points of interest you'd like to see added?	Are there any other recommendations you'd like to see included that aren't currently shown?
									Open-Ended Response	Open-Ended Response	Open-Ended Response	Open-Ended Response
1.19E+11	4.58E+08	#####	#####	104.234.190.53					I can't open the map. I guess I'm too late.	Along the Farmer's Union canal above the Trapper Ridge subdivision (and other subdivisions built at the lower part of the foothills off New Hope Rd		
1.19E+11	4.58E+08	#####	#####	172.56.149.53					Yes!!!!	I don't think so?		Adding lunch spots and coffee shops to go to while walking!!! A place to hang out would be amazing
1.19E+11	4.58E+08	#####	#####	154.27.163.73					Yes. I would consider connecting trails and pathways a priority.	Not that I can see.	No if what is proposed would be implemented that would be wonderful.	No, I would be most interested in being able to use the trail system to safely run / walk through the city and connect to other cities through the greenbelt.
1.19E+11	4.58E+08	#####	#####	160.3.201.179					Yes	No	No	Love the idea of a trail system in the north part of Star. This will encourage walking and hiking and an active lifestyle. I believe this will contribute to higher property value and make these areas more desirable to live. As a resident of the Collina Vista community, I would love to see paths to use in the foothills.
1.19E+11	4.58E+08	#####	#####	24.117.185.150					I would like to see more of the foothill development pathways as a priority. That would be north of beacon light Road up by the farmers union canal. We have lots going on down low and by the river but none that we can walk and enjoy Looking out over our beautiful town and Valley. There is much development going on up toward the hills with many people who would appreciate getting those trails in sooner rather than later!	Throughout the treasure Valley, there are many pathways directly adjacent canals. There is a wide road directly next to the farmers union canal as you work your way into the foothills above Star. Is there no way that we can partner with the canal district to allow walking along the canal. All it would take would be signage. The infrastructure would be minimal.		Again, prioritizing some of the trails as you start into the foothills. That would be behind trapper Ridge and the subdivisions east of there. We have lots of trails happening in the flats and I love freedom Park. I would like to see some other options up higher. This is such a beautiful town and valley to look out over.
1.19E+11	4.58E+08	#####	#####	159.118.202.174					No			Proposed pathways appear to cross private property on the Southside of the river and at this time the landowners are not agreeing to allow any pathway system on their property. What is the plan to protect trespassing to private landowners along the Southside of the river? We have already seen an increase in folks trying to access the river across private property a pathway. A bridge from the North to the Southside will only exacerbate the issue.
1.19E+11	4.58E+08	#####	#####	173.207.119.228					I really like he overall plan	Pedestrian bridges over major intersections to minimize vehicle and pedestrian accidents	No	Pedestrian bridges over major streets like State Street and Star Road to minimize accidents
1.19E+11	4.58E+08	#####	#####	174.247.151.18								Bbg pits for families and several restrooms
1.19E+11	4.58E+08	#####	#####	174.201.37.83					Yes	I would like a nonstop path all the way through		
1.19E+11	4.58E+08	#####	#####	154.27.166.16							unclear on how the ITD plan to SH-16 at SH-44 Construct grade- separated interchange works with proposed alternate route for State Highway 44, a main east-west highway passing through the center of downtown Star. (I LOVE the idea of an alternate route around downtown Sta.)	Single-story use buildings should be discouraged within the Riverfront Center, in favor of two- to three-story mixed use buildings. A new City Civic Center could be accommodated as a single-story building - RIVER FRONT CENTER SHOULD NOT BE 2-3 STORIES! THIS WILL RUIN THE FEEL OF THE RIVER AND MAKE IT FEEL MORE LIKE A DOWNTOWN CENTER! SINGLE STORY USE BUILDINGS SHOULD BE ENCOURAGED!!!! For Community Design for Residential, single family homes- lot lines should be a MINIMUM of 10 feet from edge of house to property line. Current 5 feet allowance does not give the "open space" feel and contributes to the claustrophobic skyscraper effect that we want to avoid in STAR.
1.19E+11	4.58E+08	#####	#####	154.27.166.242								How about concentrating on the traffic issue first! There was a bypass the Transportation Dept was looking at 20 years ago, but the mayor at the time nixed it, now look at the mess we have!
1.19E+11	4.58E+08	#####	#####	67.60.113.239								What I loved about the Riverwalk was how wild it was. I don't want the whole thing paved and to become a mini Boise. Can we keep lots of trees and nature and wildlife and not pave everything? Thanks!
1.19E+11	4.58E+08	#####	#####	159.118.231.19					Yes			
1.19E+11	4.58E+08	#####	#####	96.19.76.243					Yes	No	1) Greater clarity on Hwy 44 crossings types - i.e. bridge, underpass, road. 2) Water fountains - they should be all season. As an aside, I relocated from Virginia where there trails system is converting water fountains to all weather to encourage year round usage and it has been well-received.	
1.19E+11	4.58E+08	#####	#####	173.47.1934					Yes.	Continue West across Star River Road behind Pinewood Lakes Reserve to Bent Road!		Please do not plant trees that have fruit/seeds that will dump this debris onto the paths. One example is Honey Locust(old cultivars) that still have large seed pods, use a newer cultivar. Children and dogs can try to eat these pods. Plant more Class III trees, i.e. Oaks, Ginkgo, London Planetree, Kentucky Coffee tree. More conifers, i.e. Limber pine, Bald Cypress, Concolor Fir, Dawn Redwood. Get away from planting Blue Spruce! Be aware that all trees drop stuff and plant them far enough away from paths so we won't be stepping on this debris. Also, plant in groves, i.e. 3-7 trees spaced accordingly. Thanks.
1.19E+11	4.58E+08	#####	#####	173.47.214.222					The proposed plan has a pathway drawn to follow the Lawrence Kennedy canal, that cuts directly through my back yard. We would love to get further details on this, and discuss moving the path from the south side of the canal to the north side of the canal. My address is 11300 W Bridgetown Ln. I love the idea of connecting the community, but I don't love the idea of losing privacy, half of my back yard, and private access to the canal itself. Please contact me at raquelelizabeth77@gmail.com	No, it looks great!	No, looks great!	No. Just want details on the path drawn through my backyard. Thank you.
1.19E+11	4.58E+08	#####	#####	174.27.192.152					Yes, love them! Can't wait!!	No	it would be awesome for a connection point from one side of the highway (north) to the other (south) that connects to the greenbelt trail.	Dog stations would be great

 (<https://www.facebook.com/sharer.php?u=https%3A%2F%2Fengagekh.mysocialpinpoint.com%2FStarPathwayMP%2Fforum%2Fwe-appreciate-your-participation-1%23cnv3Message11>)



Mary Jane Marlow | Posted on Jul 26, 2024

I'm not sure but maybe all paths need to be ADA accessible. In your signage have a spot for a phone # in case of emergency in addition to 911. Going under Hwy 16 overpass would be cheaper than going over it. We need to connect the path from Star Road to Bent lane asap!

|

 (<https://twitter.com/intent/tweet?url=https%3A%2F%2Fengagekh.mysocialpinpoint.com%2FStarPathwayMP%2Fforum%2Fwe-appreciate-your-participation-1%23cnv3Message10>)

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Mary Jane Marlow | Posted on Jul 26, 2024

Having signage about the flora, fauna and birds would be very helpful.

|

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starmomma | Posted on Jul 26, 2024

Coming from a state out East where we had few places to safely ride bikes as a family and NO parks within bike riding distance, we are so grateful to enjoy being outdoors again! Thank you, so much, for planning great outdoor spaces to further encourage this for families. We would definitely benefit from more shade structures at play areas - either trees or canopies. Also, picnic tables are a great way for mommas with kiddos to get outside. Kids love to go on picnics! I also love the idea of harder pathways near the river (maybe recycles asphalt?), as it is really difficult for young kids, wheelchairs, or moms with strollers to enjoy a riverwalk. As the kids get older, it would be great to be able to access the Greenbelt from here. What a fun family ride and day out that would be!! Thanks for all you do!

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Jeff | Posted on Jul 25, 2024

I think we could really benefit from an under or overpass to cross HWY 16 @ Floating Feather. This would connect Star and Eagle in a really awesome way. Better for biking and pedestrians

✂ (https://twitter.com/intent/tweet?url=https%3A%2F%2Fengagekh.mysocialpinpoint.com%2FStarPathwayMP%2Fforum%2Fwe-appreciate-your-participation-1%23cnv3Message7)

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Costa | Posted on Jul 25, 2024

The new park additions have been great but we desperately need shade at Hunter's Creek over and around the playground. Also, more picnic tables at our parks would be great

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Matt | Posted on Jul 25, 2024

*picnic areas with built in bbq,s under shade structures
*water features with music similar to Villag

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Pamm Vuoso | Posted on Jul 25, 2024

I am disappointed in the lack of public transportation in the city of Star. Even Eagle has transportation for public use, both for the disabled and seniors citizens

|

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Debbie | Posted on Jul 17, 2024

We really need more shade! Can you partner with Treasure Valley Canopy Network to put some more trees in areas of high recreation usage

|

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Sue Speer | Posted on Jul 14, 2024

Are sand volleyball courts planned for Freedom Park?

|

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CJ Gibbons | Posted on Jul 11, 2024

Section 7, Item B.

I think we need to add an area dedicated to Archery. Then you can use it for summer camps too.

|

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CITY OF STAR
LAND USE STAFF MEMO

TO: Mayor & Council

FROM: City of Star Planning Department



MEETING DATE: **December 17, 2024 PUBLIC HEARING**

RE: Municipal Code Text Amendment

APPLICANT

City of Star

REQUEST

The following Sections of the Star Municipal Code will be reviewed for amendment:

- **TITLE 3: HEALTH AND SANITATION** CHAPTER 1, NUISANCES. ADD SNOW REMOVAL AND OTHER SIDEWALK AND ROADWAY MAINTENANCE REGULATIONS.
- **TITLE 8: UNIFIED DEVELOPMENT CODE** CHAPTER 1, ARTICLE E, DEFINITIONS, SUBSECTION 1 ARTS, ENTERTAINMENT AND RECREATION FACILITIES. REMOVE FITNESS CLUBS FROM THE USES SPECIFIED. ADD DEFINITION OF HEALTH AND FITNESS CLUBS; CHAPTER 3, ZONING DISTRICT STANDARDS, ARTICLE A, SUBSECTION 3 USES WITHIN ZONING DISTRICT, ADD HEALTH AND FITNESS CLUB USE; CHAPTER 4, ARTICLE A, PERFORMANCE STANDARDS, SUBSECTION 13, PATHWAYS, ADD LANGUAGE FROM NEW PATHWAY MASTER PLAN; CHAPTER 5, SPECIFIC USE STANDARDS, SUBSECTION ADD HEALTH AND FITNESS CLUBS STANDARDS.
- **TITLE 10: FLOOD CONTROL** CHAPTER 1, SUBSECTION 4E, UPDATE FLOODPLAIN VARIANCE PROCEDURES.

APPLICATION REQUIREMENTS, NOTICING & MEETINGS

Legal Notice Published

November 30, 2024

ZONING ORDINANCE STANDARDS

1-1-3: Amendments

Any ordinance amending the city code shall set forth the title, chapter and section number of the section or sections to be amended, and this shall constitute sufficient compliance with any statutory requirement pertaining to the amendment or revision by ordinance of any part of this city code. All such amendments or revisions by ordinance shall be immediately forwarded to the codifiers, and the said ordinance material shall be prepared for insertion in its proper place in each copy of this city code. Each such replacement page shall be properly identified and shall be inserted in each individual copy of the city code.

8-1B-5: UNIFIED DEVELOPMENT CODE TEXT AMENDMENTS:

A. Unified Development Code Amendment Initiated by A Property Owner: The applicant shall complete a pre-application conference with the administrator prior to submittal of an application for a unified development code text amendment. An application and fees shall be submitted to the administrator on forms provided by the city.

B. Unified Development Code Amendment Initiated by the City: The city may initiate a code amendment with public notice as required herein.

C. Required Findings: In order to grant a text amendment to this unified development code, the council shall review the proposed text amendment at a public hearing and make the following findings:

1. The text amendment complies with the applicable provisions of the comprehensive plan;
2. The text amendment shall not be materially detrimental to the public health, safety, and welfare; and
3. The text amendment shall not result in an adverse impact upon the delivery of services by any political subdivision providing public services within the city including.

PROPOSED CODE AMENDMENTS

The following proposed Municipal Code amendments are submitted to Council for review and decision.

TITLE 3 HEALTH AND SANITATION

3-1-2: Definitions

For purposes of this chapter, the following definitions shall apply; and any words or phrases not defined in this section should be given their ordinary, commonly understood and accepted meanings:

PUBLIC NUISANCE: A condition or use of property which is harmful or injurious to, or creates a danger of harm or injury to, the health, safety or welfare of members of the public. The term "public nuisance" is further defined so as to include, by way of example:

1. A condition or use of premises or property which creates a fire hazard or any traffic or safety hazard to members of the public.
2. A condition or use of premises or property which allows the growth of weeds, grasses, bushes or other plant life to such a size (over 1 foot in height) and, in such condition as to cause, or reasonably threaten to cause, a fire hazard because of their dried and unkempt conditions, or a safety hazard because they obstruct sight at intersections or other points at which driveways, lanes or highways come together, or a health hazard because they provide nesting areas for rodents, vermin and/or insects, or the growth of weeds to such a size or in such condition as to interfere with the free and comfortable use of adjacent and neighboring premises and property. "Weeds" are defined as undesirable and nonuseful plant growth, but shall not include noxious weeds as defined in Idaho Code section 22-2402(12).
3. **A condition of snow or ice on any public sidewalk, including private driveway access, abutting or adjoining any privately owned premises.**

(Ord. 9, 6-2-1998; amd. 2006 Code)

3-1-3: Public Nuisances Declared Unlawful

It is unlawful for any person to create, cause, maintain or knowingly allow to exist a public nuisance on any premises or real property which the person owns, rents, leases, occupies or has charge or possession of in the city.

3-1-4: Responsible Party.

- A. Where a nuisance exists upon property that is vacant, abandoned, and/or uninhabited, the owner of record, as reflected on the most recent assessment roll, shall be presumed to be responsible for creating, causing, committing, maintaining, and/or allowing such nuisance. Such owner of record shall be subject to any and all penalties imposed as set forth herein**

and shall be responsible for payment of any and all costs incurred in abating the nuisance. The owner of record shall bear the burden of rebutting this presumption.

B. Where a general nuisance is created by personal property on any portion of a public sidewalk, the owner of the personal property, if such owner can be identified, shall be presumed to be responsible for creating, causing, committing, maintaining, and/or allowing such nuisance. It is also unlawful for any person to create, cause, allow or commit any nuisance upon any public-right-of-way or infrastructure including sidewalks, streets, gutters or storm drains.

TITLE 8: UNIFIED DEVELOPMENT CODE

8-1E-1: TERMS DEFINED:

ARTS, ENTERTAINMENT AND RECREATION FACILITIES: The use of a site or facility for entertainment, spectator sports or recreational activities. The use includes, but is not limited to amusement parks, motion picture and performing arts theaters, racetracks, sports fields, golf courses, ~~fitness clubs~~, museums, zoos, marinas, bowling, video and other games and amusements. For wedding and corporate events see events center.

8-3A-3: USES WITHIN ZONING DISTRICTS

<u>ZONING DISTRICT USES</u>												
USES	A	RR	R	CBD	C-1	C-2	LO	LI	PS	MU	RC	
Arts, entertainment, recreation facility ¹	C	N <u>C</u>	N	P <u>C</u>	C	P <u>C</u>	P <u>C</u>	C	P <u>C</u>	C	P <u>C</u>	
<u>Health and Fitness Clubs</u>	<u>N</u>	<u>N</u>	<u>N</u>	<u>C</u>	<u>C</u>	<u>C</u>	<u>C</u>	<u>C</u>	<u>N</u>	<u>C</u>	<u>C</u>	

8-4B-3: REQUIRED NUMBER OF OFF-STREET PARKING SPACES:

COMMERCIAL	
Health clubs, spas, and weight reduction salons	1 per 250 square feet of gross floor area. <u>Or as otherwise required with conditional use permit</u>

8-4A-13: PATHWAYS:

ADD ADDITIONAL LANGUAGE FROM NEWLY ADOPTED PATHWAY MASTER PLAN

TITLE 10: FLOOD CONTROL

10-1-4: Administration

10-1-4 E.3 Variances as interpreted in the national flood insurance program are based on the general zoning law principle that they pertain to a physical piece of property; variances are not personal in nature and do not pertain to the structure, its inhabitants, economic or financial circumstances. Variances primarily address small lots in densely populated residential neighborhoods. As such, variances from the flood elevations should be quite rare. The applicant shall carefully review FEMA’s Floodplain Management Bulletin entitled “Variances and the National Flood Insurance Program” (FEMA P-993 / July 2014). This document is available from City Staff.

STAFF REVIEW FOR COUNCIL

Staff has provided Council with redlined highlights of the sections of the municipal code proposed for revision. The proposed revisions are a result of Staff review of the existing Titles, and input from the Star Police Chief, Star Fire District and the City Engineer.

Staff has provided Council with the following information:

- 1. Redlined Sections from each Title being revised.**

FY25 City of Star Contract - 4% COLA		POLICE CITY OF STAR	Add FT Code Enf. Jan 1/1/2025
Consolidated Contract City Budget Summary		FY 2025 Base	Option 1
Personnel	\$	3,319,252	\$ 3,376,407
Special Event Overtime			
Equipment / Uniforms	\$	96,865	\$ 98,307
Operational	\$	26,252	\$ 26,297
Vehicles	\$	255,532	\$ 326,795.65
Support	\$	11,340	\$ 11,340
Property & Evidence	\$	-	\$ -
Victim Services Unit	\$	-	\$ -
Extra Operational Items Requested	\$	3,500	\$ 3,500
Total Expenses before credit	\$	3,712,742	\$ 3,842,647
Less Shared Services Credit	\$	(259,647)	\$ (268,740)
New FY25 Contract Amount	\$	3,453,095	\$ 3,573,907
Prior year contract amount	\$	2,894,959	\$ 2,894,959
Net change to contract for FY25	\$	558,136	\$ 678,948

Personnel Costs				
Positions	Base	Opt 1	Total Cost	Opt 1
Chief II	1	1	\$ 200,952	\$ 200,952
Chief I	0	0	\$ -	\$ -
Sergeant	2	2	\$ 376,291	\$ 376,291
Det. Sergeant	1	1	\$ 187,511	\$ 187,511
Detective	3.25	3.25	\$ 490,092	\$ 490,092
Deputy	13	13	\$ 1,990,975	\$ 1,990,975
Code Enforcement	0	0.75	\$ -	\$ 57,155
Community Service Officer	0	0	\$ -	\$ -
Clerk (FT)	1	1	\$ 73,430	\$ 73,430
Clerk (PT)	0	0	\$ -	\$ -
Personnel Subtotal	21.25	22	\$ 3,319,252	\$ 3,376,407

**AMENDED AND RESTATED DEVELOPMENT AGREEMENT
6697 FOOTHILL ROAD ANNEXATION (FORMERLY GARNET SUBDIVISION)**

This Amended and Restated Development Agreement ("Agreement") is entered into by and between the City of Star, a municipal corporation in the State of Idaho, hereinafter referred to as "City", and [REDACTED], hereinafter referred to as "Owner" and "New Owner".

WHEREAS, New owner owns a parcel of land of approximately 5.23 acres in size, currently located within the City of Star, zoned R-1-DA and more particularly described in **Exhibit A** of Ordinance 386-2023, which is attached hereto and incorporated by reference herein (the "Property");

WHEREAS, New owner has recently purchased the Property and is now the applicant in connection with City of Star File Number AZ-23-01/DA-23-01/PP-23-01, seeking a development agreement modification approval for the Property.

WHEREAS, on May 2, 2023, the Property was annexed and zoned R-1-DA and made subject to that certain Development Agreement, dated July 19, 2023 between City and Opus Development, LLC, recorded on August 16, 2023 as Instrument No. 2023-026368 in the office of the Canyon County Recorder ("**Original Development Agreement**");

WHEREAS, the City, pursuant to Section 67-6511A, Idaho Code, and Star City Code at Title 8, Chapter 1, has the authority to enter into a development agreement for the purpose of allowing, by agreement, a specific development to proceed in a specific area and for a specific purpose or use which is appropriate in the area, and for a specific purpose or use and with specific approved design and dimensional standards;

WHEREAS, the City has authority to enter into development agreements, including modifications, to condition annexations and re-zones;

WHEREAS, it is the intent and desire of the parties hereto to proceed with development of the Property in accordance with this Agreement, which the parties agree will fully amend and replace the Original Development Agreement;

WHEREAS, the intent of this Agreement is to protect the rights of Owner's and Developer's use and enjoyment of the Property while at the same time mitigating any adverse impacts of the development upon neighboring properties and the existing community and ensuring the Property is developed in a manner consistent with City Ordinances;

THEREFORE, for and in consideration of the mutual covenants, duties and obligations herein set forth, the parties hereby agree as follows:

Section 1. Legal Authority. This Agreement is made pursuant to and in accordance with the provisions of Idaho Code Section 67-6511A and Star City Code, Title 8, Chapter 1.

Section 2. Development/Uses/Standards.

2.1 Development Acreage and Uses Permitted. As to the Property described on Exhibit A, Owner is allowed to develop the 5.23 acres as follows:

- Zoning Classification: The zoning classification of the Property shall be a R-1-DA.
- The Owner shall comply with all city ordinances relating to the Property except as otherwise provided herein.

2.2 Site Design. ~~The Property shall be developed in substantial conformance with the approved preliminary plat, dated 5/2/23, a copy of which is attached hereto and incorporated by reference herein as Exhibit B. The property is eligible for a one-time parcel division. The owner shall submit all necessary applications for review and approval of a parcel division prior to issuance of a building permit for a second, primary residence on the property.~~

2.3 Uses. The Property is hereby approved for ~~a maximum of 5 residential lots~~ all land uses and operations allowed within the City of Star, subject to compliance with the requirements of the current City of Star Municipal Code.

2.4 Setbacks. The development shall comply with the standard setbacks for the R-1 zone as follows:

Single-family Detached Setbacks:

Max. Height	Min. Front Yard Setback	Min. Rear Yard Setback	Min. Interior Side Setback	Min. Street Side Setback
35'	30'	30'	10'	20'

2.5 Additional Requirements:

- ~~The applicant shall extend the bus stop turnout located to the east into the frontage of the new subdivision.~~
- All homes shall be required to provide fire sprinklers and emergency access in compliance with the Star Fire District.
- ~~The Council approves the revised preliminary plat showing a reversed lot layout as discussed in the public hearing (two lots adjacent to east property line).~~
- ~~The applicant shall provide a streetlight at the entrance to the subdivision.~~
- ~~The Council hereby approves the reduced private street width to a minimum of 24' with no parking or 26' with parking on one side as~~

~~determined by the Fire District. The Council waives the requirement for sidewalks within the subdivision.~~

- ~~Applicant Owner~~ agrees to pay all City Emergency Services Mitigation Fees in effect at the time of building permit for each primary single-family dwelling.
- ~~Applicant shall meet all specific conditions of approval for Preliminary Plat PP-23-01.~~
- The owner shall be required to annex into the Star Sewer & Water District, and meet all District requirements if three (3) or more single-family dwelling lots are developed on the 5-acre parcel.
- The property associated with this annexation shall be satisfactorily weed abated at all times, preventing a public nuisance, per Star City Code Chapter 3, Section 3-1-1 through 3-1-7, and shall be properly maintained at all times, including throughout the construction process to include trash picked up and trash receptacles emptied with regular frequency, streets swept and cleaned weekly, including any streets used to access the property and all debris shall be prevented from accumulating on any adjacent property or public right of way and shall remove all debris from public way at least daily. This shall also include, but is not limited to any trash, junk or disabled vehicles during any portion of the development process. The site shall be properly mitigated from fugitive dust at all times during construction, as determined by the Zoning Administrator. Failure to comply with any of the above may result in a stop work order being issued on any active building permit until the violations are remedied, and/or fines issued by the City.

2.6 Proportionate Share Agreement for ITD Improvements. Developer has agreed to participate in the costs of construction or improvements to the portions of the State Highway System within the City of Star and/or City of Star Area of City Impact. The Developer will pay the ~~\$5,000.00~~ traffic mitigation fee determined, or revised, by the Idaho Transportation Department as follows: the Developer ~~will~~ shall pay the City \$1,000.00 per buildable lot ~~within each phase prior to signature on the final plat for the applicable phase at the time of building permit.~~ The City will allocate the funds to roadway improvements in the vicinity of the project. The Developer shall pay this amount (unless otherwise revised by ITD) directly to the City of Star. The City will maintain this contribution in a specific Development Contributions account, to be distributed to ITD when requested for use with a specific Idaho Transportation Improvement Plan (ITIP) project within the City of Star Area of City Impact or City Limits in accordance with the terms of the Intergovernmental Agreement between the Idaho Transportation Department and the City of Star dated April 22, 2020.

2.7 Changes and Modifications. No change in the use or restrictions specified in this Agreement shall be allowed or changed without modification of this Agreement pursuant to the requirements of the Star City Ordinances. In the event Owner changes or expands the uses permitted by this Agreement or fails to comply with the restrictions without formal modification

of this Agreement as allowed by the Star City Ordinances, Owner shall be in default of this Agreement.

Section 3. Default. The failure of Owner, its heirs or assigns or subsequent owners of the Property or any other person acquiring an interest in the Property, to faithfully comply with any of the terms and conditions of this Agreement shall be deemed a default herein. This Agreement may be modified or terminated by the Star City Council as set forth in the Star City Ordinances. In the event this Agreement is modified, Owner shall comply with the amended terms. Failure to comply with the amended terms shall result in default. In the event the City Council, after compliance with the requirements of the Star City Ordinances, determines that this Agreement shall be terminated, the zoning of the Property or portion thereof that has not been developed in accordance with this Agreement shall revert its prior zoning designation. All uses of such property, which are not consistent with the prior zoning designation, shall cease. A waiver by the City of Star for any default by Owner of any one or more of the covenants or conditions hereof shall apply solely to the breach and breaches waived and shall not bar any other rights or remedies of the City or apply to any subsequent breach of any such or other covenants and conditions. Owner, by entering into this Agreement, do hereby consent to a reversion of the subject property to its prior zoning designation in the event there is a default in the terms and/or conditions of this Agreement.

Section 4. Unenforceable Provisions. If any term, provision, commitment or restriction of this Agreement or the application thereof to any party or circumstances shall, to any extent, be held invalid or unenforceable, the remainder of the instrument shall remain in full force and effect.

Section 5. Assignment and Transfer. After its execution, this Agreement shall be recorded in the office of the County Recorder at the expense of the Applicant. Each commitment and restriction on the development subject to this Agreement, shall be a burden on the Property, shall be appurtenant to and for the benefit of the Property and shall run with the land. This Agreement shall be binding on the City and Owner, and their respective heirs, administrators, executors, agents, legal representatives, successors and assigns: provided, however, that if all or any portion of the Property is divided, each owner of a legal lot shall only be responsible for duties and obligations associated with an owner's parcel and shall not be responsible for duties and obligations or defaults as to other parcels of lots within the Property. The new owner of the Property or any portion thereof (including, without limitation, any owner who acquires its interest by foreclosure, trustee's sale or otherwise) shall be liable for all commitments and other obligations arising under this Agreement with respect only to such owner's lot or parcel.

Section 6. General Matters.

7.1 Amendments. Any alteration or change to this Agreement shall be made only after complying with the notice and hearing provisions of Idaho Code Section 67- 6509, as required by Star City Code.

7.2 Paragraph Headings. This Agreement shall be construed according to its fair meaning and as if prepared by both parties hereto. Titles and captions are for convenience only and shall not constitute a portion of this Agreement. As used in this Agreement, masculine, feminine or neutral gender and the singular or plural number shall each be deemed to include the others wherever and whenever the context so dictates.

7.3 Choice of Law. This Agreement shall be construed in accordance with the laws of the State of Idaho in effect at the time of the execution of this Agreement. Any action brought in connection with this Agreement shall be brought in a court of competent jurisdiction located in Ada County, Idaho.

7.4 Notices. Any notice which a party may desire to give to another party must be in writing and may be given by personal delivery, by mailing the same by registered or certified mail, return receipt requested postage prepaid, or by Federal Express or other reputable overnight delivery service, to the party to whom the notice is directed at the address of such party set forth below.

Star: City of Star
Attn: City Clerk
P.O. Box 130
Star, ID 83669

Owner/Developer: 10206 West State Street LLC
Evan McLaughlin
2001 N. 20th Street
Boise, Idaho 83702

7.5 Effective Date. This Agreement shall be effective after delivery to each of the parties hereto of a fully executed copy of this Agreement.

7.6 Attorney Fees. Should any litigation be commenced between the parties hereto concerning this Agreement, the prevailing party shall be entitled, in addition to any other relief as may be granted, to court costs and reasonable attorney fees as determined by a court of competent jurisdiction. This provision shall be deemed to be a separate contract between the parties and shall survive any default, termination or forfeiture of this Agreement.

[end of text; signatures and exhibits follow]

IN WITNESS WHEREOF, the parties have hereunto caused this Agreement to be executed on the day and year set forth below.

