

CITY OF SOUTH JORDAN
CITY COUNCIL MEETING AGENDA
CITY COUNCIL CHAMBERS
TUESDAY, FEBRUARY 21, 2023 at 6:30 p.m.



Notice is hereby given that the South Jordan City Council will hold a City Council Meeting at 6:30 p.m. on Tuesday, February 21, 2023, in person in the City Council Chambers, located at 1600 W. Towne Center Drive, South Jordan, Utah virtually via Zoom phone and video conferencing. Persons with disabilities requesting assistance should contact the City Recorder at least 24 hours prior to the Meeting. The Agenda may be amended and an Executive Session may be held at the end of the Meeting. Times listed are approximate and may be accelerated or delayed.

In addition to in-person attendance, individuals may join via phone or video, using Zoom. Note, attendees joining virtually may comment during public comment, or a public hearing virtually. To comment during public comment, or public hearing virtually, the individual must have their video on and working during their comments. Attendees who wish to present photos or documents to the City Council must attend in person. Those who join via phone may listen, but not participate in public comment or public hearings.

In the event the Meeting is disrupted in any way that the City in its sole discretion deems inappropriate, the City reserves the right to immediately remove the individual(s) from the Meeting and, if needed, end virtual access to the Meeting. Reasons for removing an individual or ending virtual access to the Meeting include but are not limited to the posting of offensive pictures, remarks, or making offensive statements, disrespectful statements or actions, and other any action deemed inappropriate.

Ability to participate virtually is dependent on an individual's internet connection. To ensure comments are received regardless of technical issues, please have them submitted in writing to the City Recorder, Anna Crookston, at acrookston@sjc.utah.gov by 3:00 p.m. on the day of the meeting. Instructions on how to join virtually are below.

Join South Jordan City Council Meeting Virtually:

- Join on any device that has internet capability.
- Zoom link, Meeting ID and Password will be provided 24 hours prior to meeting start time.
- Zoom instructions are posted <https://ut-southjordan.civicplus.com/241/City-Council>.

Regular Meeting Agenda: 6:30 p.m.

- A. Welcome, Roll Call, and Introduction:** By Mayor, Dawn R. Ramsey
- B. Invocation:** By City Manager, Gary L. Whatcott
- C. Pledge of Allegiance:** Director of Public Works, Jason Rasmussen
- D. Minute Approval:**
 - [D.1.](#) February 7, 2023 City Council Study Meeting
 - [D.2.](#) February 7, 2023 City Council Meeting
- E. Mayor and Council Reports: 6:35 p.m.**

F. Public Comment: 6:50 p.m.

This is the time and place for any person who wishes to comment on items not scheduled on the agenda for public hearing. Any person or group wishing to comment on any item not otherwise scheduled for public hearing on the agenda may address the City Council at this point by stepping to the microphone, or if joining electronically, by raising their hand and giving his or her name for the record. Note, if joining electronically, photos or documents will not be accepted through Zoom and you must attend City Council Meeting in-person. Comments should be limited to not more than three (3) minutes, unless additional time is authorized by the Chair. Groups wishing to comment will be asked to appoint a spokesperson. Items brought forward to the attention of the City Council will be turned over to staff to provide a response outside of the City Council Meeting. Time taken on non-agenda items, interrupts the process of the noticed agenda. In rare cases where it is determined appropriate to address items raised from public comments, these items will be noted and may be brought back at the conclusion of the printed agenda.

G. Public Hearing Item: 7:00 p.m.

[G.1.](#) [Resolution R2023-11](#), Adopting the South Jordan Streetscape Master Plan for the City of South Jordan. *(By Associate Director of Parks, Colby Hill and VODA Landscape & Planning, Mark Morris)*

H. Staff Reports and Calendaring Items: 7:15 p.m.

RECESS CITY COUNCIL MEETING AND MOVE TO EXECUTIVE CLOSED SESSION

I. Executive Closed Session: 7:30 p.m.

I.1. Discussion of the purchase, exchange, or lease of real property.

ADJOURN EXECUTIVE CLOSED SESSION AND RETURN TO CITY COUNCIL MEETING

ADJOURNMENT

CERTIFICATE OF POSTING

STATE OF UTAH)

COUNTY OF SALT LAKE)

I, Anna Crookston, the duly appointed City Recorder of South Jordan City, Utah, certify that the foregoing City Council Agenda was emailed to at least one newspaper of general circulation within the geographic jurisdiction of the public body. The agenda was also posted at the principal office of the public body and also posted on the Utah State Public Notice Website <http://www.utah.gov/pmn/index.html> and on South Jordan City's website at www.sjc.utah.gov. Published and posted February 16, 2023.

SOUTH JORDAN CITY
CITY COUNCIL STUDY MEETING

February 7, 2023

Present: Council Member Tamara Zander, Council Member Patrick Harris, Council Member Don Shelton, Council Member Brad Marlor, Council Member Jason McGuire, City Manager Gary Whatcott, Deputy City Manager Dustin Lewis, CFO Sunil Naidu, Director of Strategy & Budget Don Tingey, Director of Commerce Brian Preece, Director of Public Works Jason Rasmussen, Director of Administrative Services Melinda Seager, City Attorney Ryan Loose, City Recorder Anna Crookston, GIS Coordinator Matt Jarman, Senior IS Tech Phill Brown, IS Specialist Ken Roberts, Director of Planning Steven Schaefermeyer, Deputy City Engineer Jeremy Nielson, Fire Chief Chris Dawson, Police Chief Jeff Carr, Associate Director of Recreation Jacob Druce, Meeting Transcriptionist Diana Baun, HR Director Theresa Cook, Communications Manager Rachael Van Cleave, Planning Commission Chair Michele Hollist

Absent: Mayor Dawn Ramsey

Others: Sam Bishop

4:51 PM
STUDY MEETING

A. Welcome, Roll Call, and Introduction: *By Mayor Pro Tempore Patrick Harris.*

Council Member Marlor made a motion to nominate Council Member Harris as Mayor Pro Tempore for tonight until Mayor Ramsey returns. Council Member McGuire seconded the nomination; vote was unanimous.

Mayor Pro Tempore Patrick Harris welcomed everyone present and introduced the meeting. He excused Mayor Dawn Ramsey who is currently traveling and unable to attend the meeting.

B. Invocation: *By Council Member Don Shelton*

Council Member Shelton offered the invocation.

C. Mayor and Council Coordination

Council Member Shelton reminded the council members of the morning report from the legislators Saturday, February 11, 2023 at 8:00 a.m.

Mayor Pro Tempore Harris asked staff to discuss the impact of the extra snow this year and the resulting potholes.

Deputy City Manager Dustin Lewis said that when we have a lot of moisture and freezing temperatures it tends to break the roads up more than usual, and crews spend their time in-between storms fixing potholes. Some of it can be done with cold patch, but it depends on the days that the plant creating the hot mix has materials available. When they can get the hot mix they go out and fix as many as they can.

Director of Public Works Jason Rasmussen reinforced that the frequent snow storms with freezing and thawing finds those cracks and creates more potholes. One of the limitations has been the asphalt plant, as they haven't been making hot mix as often as they'd like. Hot mix asphalt is the stuff that stays, otherwise when they use the cold patch it usually breaks out during the next snow storm; it is only a temporary fix, or a band-aid. He has spoken with other public works departments and they are experiencing the same issues with not being able to get the right asphalt for winter. However, his department has made some progress, and they are keeping a list of the current potholes that need repair. When they get the hot mix they get both trucks going and fill as many as they can. The plant has only been making the mix about once a week, so that has left them with only one day a week to be able to work on that.

Manager Lewis mentioned a story done last month with KSL News and our Communications Manager Rachael Van Cleave, highlighting potholes around the valley and pointing to South Jordan as one of the cities that was aggressively out there filling potholes and actually doing a good job at it.

Council Member Zander asked to confirm that our main artery roads like 10600 South and 11400 South are state roads, and asked who does those repairs.

Director Rasmussen said they are state roads, and the state maintains those. Much of those roads are concrete, so there are less repairs needed, but west of Bangerter are our roads and we maintain those. When the city sees things that need repair on the state roads staff alerts them, but their response time isn't great. He discussed specific locations with the council where there are issues on the state roads and shared what he knew regarding progress.

D. Discussion/Review of Regular Council Meeting

Action Item

- Resolution R2023-09, Authorizing the Mayor to enter into an Interlocal Cooperation Agreement with Salt Lake City for 2022-2028 UPDES Media Campaign Cost Sharing.

Manager Lewis shared that the city is required under UPDS to do certain things to educate the public about storm water. With those requirements there are two options, one is to do it on our own, and the other is to join with the storm water coalition and pool resources. We will pay our portion, which will go to the team putting that together. That option ends up being more fiscally responsible with city funds to meet the federal and state mandates for storm water education.

Public Hearing Items

- Resolution R2023-08, Amending the Future Land Use Plan Map.
- Zoning Ordinance 2023-02-Z, Rezoning property located at 10828 South 1055 West.
- Ordinance 2023-02, Adopting an amended and updated Impact Fee for Water and Transportation within the “Last Hold Out” properties.
- Ordinance 2023-03, Adopting an amended and updated Impact Fee for Water and Transportation within the “Lark Meadows” properties.

Manager Lewis noted that the first two items are in the area around 1055 West, and we will probably see some of the same residents from the recent meeting on the Rise Development. Director of Strategy & Budget, Don Tingey has been working closely with Lewis Young on the impact fees that need to be charged on some properties on the west side, he and Fred Philpot will be presenting those numbers.

E. Discussion Item:

E.1. Interfacility Transport discussion (*By Fire Chief Chris Dawson*).

Fire Chief Chris Dawson reviewed his prepared presentation (Attachment A).

Council Member Zander asked if the rise in transports was due to our faster population growth, or due to our percentage of calls per population going up.

Chief Dawson answered that it was a combination of things. They tried to narrow that down, but they haven't seen any particular area of calls that has jumped; everything has increased proportionally. He mentioned that elderly care facilities has had a pretty big impact on that, and he also believes because Station 64 has opened they are just more available to handle the calls inside the city and relying less on mutual aid. He continued reviewing Attachment A.

Council Member Marlor asked how much of that growth is outside of the city boundaries.

Chief Dawson referenced Attachment A and showed where the number of calls just inside South Jordan is listed, noting the medics running mutual aid calls outside of the city, along with the ones inside the city.

Mayor Pro Tempore Harris asked if all the EMTs do IFTs.

Chief Dawson responded yes, everybody rotates through them. He continued reviewing Attachment A.

Mayor Pro Tempore Harris asked if they are having to decline IFTs due to the high volumes.

Chief Dawson responded yes, they are having to refuse transfers and call Gold Cross to take those transfers.

Mayor Pro Tempore Harris asked what the total revenue would be if they weren't having to give up those opportunities for transfer due to volume.

Chief Dawson responded that an average of 1.5 additional transports per day would pay for everything being suggested, at more than 100% of the cost. He doesn't know what the number is, but he is confident it would be substantial. He continued reviewing Attachment A.

Council Member McGuire asked what keeps a company like Gold Cross from coming in and trying to take this market over if it's so profitable.

Chief Dawson responded that it's because we hold the license for it. Our fire department is the only one in Salt Lake County that actually holds a license to do this, so another company can't by law come in. We have a contract with Gold Cross to be used as back up, and they are in the works of signing a contract to back them up on the flip side.

City Manager Gary Whatcott added that the ambulance program in the State of Utah isn't a free enterprise system, it is a monopolistic system done by licensing based on geographic boundary. Every license has a boundary with a clear indication of what services can be provided within that boundary. Our license includes not only emergency type calls normally seen in transporting, but we have an interfacility transport license as well which they fought 10 years for with Gold Cross.

City Attorney Ryan Loose discussed a previous issue with Guardian Transport, and how hard the city fought them. That was all an effort to preserve the interfacility part of our license, as they were edging in on that and he knew they were breaking the law and infringing on the city's license.

Manager Whatcott noted that because of that fight, Guardian Transport realized they were in a space they couldn't be and they have moved over to mental health transports. This program is meant to provide five ambulances from showing up at your traffic accident fighting to take you by offering a cheaper price or undercutting each other. Due to this program, the state regulates all the fees that can be charged, we don't control the costs.

Council Member Marlors asked if each city holds a license for this.

Manager Whatcott responded that each license is very specific in terms of the area and what they are allowed to do, and the reason we hold the license we do is because Gold Cross had no infrastructure in the city at any point and the city proved that through a court process that granted the license.

Chief Dawson reiterated that Gold Cross has approached him to work out a mutual aid agreement, as they are typically unavailable to handle calls in this area of the valley and that is another advantage for us, as he has all the confidence that we would be able to pick up those additional 1.5 transports daily to more than cover the costs of this EMS program.

Manager Whatcott added that the helicopters are being removed from Salt Lake County, they hardly ever fly within the county. The actual helicopters are stationed outside of the county due to the proximity of trauma centers, however the critical care ambulance program our city is working on is a piece that fulfills that need on the ground.

Chief Dawson said if they need to call a helicopter they are typically waiting 20 -30 minutes on a good day for that transport, and with recent medical centers opening locally it's just faster to go via ground transport.

Manager Whatcott added that another benefit to this is it isn't taxpayer money, it all comes from third parties. Also, our contractual arrangement with the University of Utah is that they are the payer, we are not going through an insurance provider. Our collection rates, even with Medicaid and Medicare, on the emergency side of the business are very different from these transports. We are being paid up front cash, and it has saved our taxpayers millions in just fire equipment over the life of this program alone that we've paid for.

Council Member Zander asked, if this is approved how many additional transports our crews could facilitate per day.

Chief Dawson said it depends on the open spots they are filling, but it's not unusual for their crews to do 21 transports in a day. That means that any single unit could do 6-8 transfers a day without much difficulty. Some of the savings the city will see with this program are with the Type 1 ambulance which is about half the price to procure, which would cost an estimate of about \$8,400 to run. Just in savings alone, taking the pressure and mileage off the other ambulances will save \$7,420 alone; that leaves the net cost under \$1,000 a year to run this ambulance. He continued reviewing Attachment A.

Manager Whatcott noted they would really like to get started on this program if the council doesn't have any concerns, as it takes time to get the vehicles and get them geared up. They would like to try to get this ready to run by July 1, 2024.

Council Member Shelton asked if this would be part of a budget.

Manager Whatcott said it could be added in, but it's just cash that's currently sitting there in a special fund.

CFO Naidu said this is in the budget as part of the Capital Improvement Funds, but it's not labeled specifically as an enterprise fund.

Council Member Shelton asked if anyone had the current fund balance since it is being tracked.

CFO Naidu replied that he believes it is close to \$2 million or more.

Chief Dawson said his goal is to provide the best care possible to our residents, and we are uniquely positioned to do that with the development of this program and the critical care

program. He is very confident that the EMS system is going to change and evolve to look very different in 5-10 years and this puts us in a position to take advantage of that and be ready to go.

Council Member McGuire asked if this program would run out of Station 64.

Chief Dawson responded yes.

Council Member McGuire asked as the Downtown Daybreak area grows, will that station still be able to serve this program plus the additional battalion the station was designed for.

Chief Dawson responded yes, that everything being looked at with the future addition of Station 65 and potential changes at Station 62 and 64, allow for this program with no significant changes to house this program.

Council Member Marlor asked if the green light is given tonight, would this be brought back for an official vote from the council in the future.

Manager Lewis said he wanted to bring something back after the last discussion where the council asked what could be done to prevent things from impacting service levels. This was the lowest hanging fruit of all the options, and if the council gives the nod he can tell Chief Dawson to start tomorrow morning and make this happen with an anticipated program start date of July 1.

Manager Whatcott noted that in the upcoming budget year there would be changes to approve in terms of the required full time employee positions.

Council Member Marlor asked if there is any talk in the legislative arena regarding any changes that would affect this opportunity.

Attorney Loose discussed someone who was trying to ruin this whole system so he could run things the way he wanted, but that failed because of our city and many other entities. This year there was a little talk and a few things on the edges, but nothing significant. By next year or the year after if they chose to do anything regarding removing the licenses, it would actually probably work to our advantage if we wanted to run a full scale program because we would already be set up and ready to go.

Manager Lewis mentioned a piece of legislation that would change the retirement for the paramedic only, which would significantly help us in the recruiting of people to fill the necessary full time positions. It only applies to those working in the government sector that provide this kind of medical service, and it might be attractive to individuals who don't currently work for a local government to jump across and work here.

Manager Whatcott said there are a number of rural towns whose medics are unable to be in the state retirement system as a public safety employee, and there is a bill to change that for paramedics/EMTs only that are not certified firefighters. This bill now allows them to be in the firefighter retirement system as proposed.

Mayor Pro Tempore Harris asked what happens if a later analysis justifies adding another vehicle, which requires the hiring of six additional paramedics.

Manager Whatcott said it could be brought back as a discussion and added, even if they needed to formally amend the budget that could be done at any time.

Manager Lewis noted that this also adds another tool to the city's toolbox, for instance when we are required to provide an ambulance at a Bingham High Football Game or a standby ambulance at a concert at the equestrian center, Summerfest or the Farmer's Market. This would allow the city to provide that service without taking an ambulance out of service for the event.

Council Member Zander asked if a large sports venue in the city would require an ambulance for their events.

Manager Lewis said it depends on the type of activities, it might be required and we would be available for hire.

The council members all agreed that they were supportive of this proposed program.

Council Member Zander motioned to move from the City Council Study Meeting to Executive Closed Session. Council Member Shelton seconded the motion; vote was unanimous in favor.

RECESS CITY COUNCIL STUDY MEETING AND MOVE TO EXECUTIVE CLOSED SESSION

F. Executive Closed Session

F.1. Discuss pending or reasonably imminent litigation.

ADJOURN EXECUTIVE CLOSED SESSION AND RETURN TO CITY COUNCIL STUDY MEETING

Council Member Marlor motioned to adjourn the Executive Closed Session and return to the City Council Study Meeting. Council Member McGuire seconded the motion; vote was unanimous in favor.

ADJOURNMENT

Council Member McGuire motioned to adjourn the February 7, 2023 City Council Study Meeting. Council Member Shelton seconded the motion; vote was unanimous in favor.

The February 7, 2023 City Council Study meeting adjourned at 6:05 p.m.

SOUTH JORDAN CITY
CITY COUNCIL MEETING

February 7, 2023

Present: Council Member Tamara Zander, Council Member Patrick Harris, Council Member Don Shelton, Council Member Brad Marlor, Council Member Jason McGuire, City Manager Gary Whatcott, Deputy City Manager Dustin Lewis, CFO Sunil Naidu, Director of Strategy & Budget Don Tingey, Director of Commerce Brian Preece, Director of Public Works Jason Rasmussen, Director of Administrative Services Melinda Seager, City Attorney Ryan Loose, City Recorder Anna Crookston, GIS Coordinator Matt Jarman, Senior IS Tech Phill Brown, IS Specialist Ken Roberts, Director of Planning Steven Schaefermeyer, Deputy City Engineer Jeremy Nielson, Fire Chief Chris Dawson, Police Chief Jeff Carr, Associate Director of Recreation Jacob Druce, Meeting Transcriptionist Diana Baun, Communications Manager Rachael VanCleave, Planning Commission Chair Michele Hollist, Strategy & Budget Analyst Abigail Patonai

Absent: Mayor Dawn Ramsey

Others: Seth Drew, Zackary Adams, Bronson, Brian Child, Bennion Gardner, Sandy, Marti Dumas, Katie's iPhone, Kathy Burk, Bryan Farnsworth, Charles Judd, Mike Lorenc, Jen Elizalde, Janalyn Sainsbury, Scott Sainsbury, Kate Sainsbury, Spencer Burkert, Caitlin Burkert, Milo Tenney, Sandra Tenney, Travis & Melanie Hiatt, Resh Jefferies, Kristin Tenney, Lynn Brown, Mitchell Sadowski, Matthew Sadowski, Don Reese, Sam Bishop, Mike West, Daniel Bird, Gary Garner, Karen Christiansen, Bennion Gardner, Bronson Mullen, Carmen Rufner,

6:31 P.M.
REGULAR MEETING

A. Welcome, Roll Call, and Introduction to Electronic Meeting - *By Mayor Pro Tempore Patrick Harris*

Mayor Pro Tempore Patrick Harris welcomed everyone and introduced the meeting. He excused Mayor Ramsey, who was absent from tonight's meeting.

B. Invocation – *By Council Member Tamara Zander*

Council Member Zander offered the invocation.

C. Pledge of Allegiance – *By Assistant Director of Recreation Jake Druce*

Associate Director of Recreation Jake Druce led the audience in the Pledge of Allegiance.

D. Minute Approval

D.1. January 17, 2023 City Council Study Meeting Minutes

D.2. January 17, 2023 City Council Meeting Minutes

D.3. January 31, 2023 City Council Budget Meeting Minutes

Council Member Shelton asked about the possibility of having the attachments included with the draft minutes that are sent to the council.

City Recorder Anna Crookston discussed options with the council and she will include those attachments in the future.

Council Member Shelton motioned to approve the January 17, 2023 City Council Study Session Meeting Minutes, January 17, 2023 City Council Meeting Minutes, and the January 31, 2023 City Council Budget Meeting Minutes as published. Council Member Marlor seconded the motion; vote was unanimous in favor.

E. Mayor and Council Reports

Council Member Jason McGuire attended the Herriman High School Chinese New Year Celebration, the budget meeting last week with the rest of the council, and the circus while in town. He is grateful to have the equestrian park in the city and to Utah State University for continuing to offer programming at the facility, including the circus.

Council Member Tamara Zander sits on the business committee for the South Valley Chamber of Commerce and reported that is going well. Members of the business community that are sitting on that committee have organized an event in March for veterans in the city, recognizing the new VA Clinic in Daybreak. The event is being done in an effort to thank and celebrate the veterans in the city. Dinner will be supplied by volunteers, and there is a wonderful comedian volunteering his time to come and entertain everyone there. This morning she attended the Business Accelerator Class with the South Valley Chamber, which is another service provided by the chamber for anyone in the city with their own business.

Council Member Brad Marlor attended an Architectural Review Committee meeting on January 25 for one new building that has been approved and will be moving forward. He attended the Utah League of Cities and Towns lunch with members of the city's youth council and legislators. All of our city's legislators were invited and attended, sitting with our city at the lunch. He had lunch with a few of our Planning Commission members over the last 2 weeks where they were able to talk about ways to better collaborate between the planning commission and council. He also attended the budget meeting, which was very well done. He very much appreciates the staff and what a great job they always do. He went on a tour with a prominent developer in the city who has some great ideas for things that will enhance the city in years to come; he appreciated that opportunity. On a personal note, he shared his story about recently applying for Social Security and Medicare, noting that he was born in the Philippine Islands which caused some issues proving his citizenship during the application process.

Mayor Pro Tempore Patrick Harris also attended the Utah League of Cities and Town event downtown, it was nice to be able to spend time with our legislators and discuss the issues our city is facing and hear their thoughts. He added that the governor spoke at that event and he always enjoys the governor's positive outlook for the state of Utah. He attended the budget meeting with the other council members, as well as the sewer board meeting where he is pleased to report everything is going well.

Council Member Don Shelton also attended the luncheon and budget meeting described previously. There have been three Legislative Policy Committee meetings since the last council meeting that he has been a part of, along with Council Member McGuire. He attended the Jordan River Commission meeting this last Thursday, where he was elected the Vice Chair of that commission. He also had the chance to take the same tour through the city as Council Member Marlbor with the developer.

F. Public Comment

Mayor Pro Tempore Harris opened the public comment portion of the meeting.

Don Reese (Resident) – My wife and I have resided there for the past 52 years. This is a follow-up on a meeting that I had on January 25 with Gary Whatcott and two city planners. We were talking about 1055 West, and that is still a real concern for us. It was decided in that meeting that Mr. Whatcott would forward the letter from the citizens to the mayor and the city council. I am here for a couple of reasons this evening. Number one, I want to find out if you received that letter from Mr. Whatcott. Number two, did you get a chance to read it. What that represents is 40 members of 1055 West that are very concerned. We are concerned that the options talked about in the letter should be addressed by the city. It should be noted in the minutes tonight that 98% of the people on this street, who reside there, have signed this letter. We have 40 signatures of people that want to be heard. They want to be heard on the idea that you need to look at that street that runs from 1055 West to the new subdivision on our street. There are some real concerns about that, if you would address that we would be happy to address that with you; we have copies if you would like to look at it. I would appreciate you at least answering us back on that, because the 40 citizens are concerned that the safety issues have not been addressed properly.

Sam Bishop (Resident), Mike West (Resident) and Daniel Bird (Lehi Resident)

Sam Bishop – I was up here a few months ago and mentioned that I, along with a few others, had formed a group called Bike Walk Southwest Valley. Since then we have reduced both the geographical scope of our group and the length of our name. We are now Bike Walk SoJo and are here this evening to ask the mayor and council to create an active transportation committee. Of our existing committees, I am picturing something most similar to the Arts Council, where the main focus is on encouraging public good that we tend to overlook otherwise.

Mike West – What we are thinking about is taking ideas from what Provo City has done. They have an active transportation committee that has been in place for years. They do things to try and help encourage people to cycle, walk, and get around by other means without having to drive a car necessarily. Some of the things they have done for example are events like Bike to Work or

Bike to School Day, which can include bike, walk or roll. They can encourage kids to get to school with other means, especially where we can find safe routes to do that. The committee can have a planning guide that can help school administrators as they plan for that as well. May is also Bike Month, and there are a couple of different events you can do for Bike Month, including doing a bike-in instead of a drive-in movie theater, setting up a screen and getting kids and others in the community to bike, walk, or anything else to come to that event. You could do a stroll or roll along the Jordan River Parkway, Provo does that along the Provo River Parkway and we have some options for that along with a Bike to Work type of day. We could hold similar events in May, and even in other parts of the year, and that's a lot of what an Active Transportation Committee could do. We could also take the lead in applying for a bicycle friendly community award, which is what Mr. Bishop has just passed out to you (Attachment A). The American League of Bicyclists has an application that can be submitted with a \$50 fee, and the committee could work on submitting that application. The award levels go all the way from bronze up to platinum, they provide feedback to the city on what's going well as well as ways the city can improve to help with cycling. Specifically, we would like to look holistically at active transportation, but they can give us great advice on cycling. The award designations are good for four years, and what you have in front of you (Attachment A) is a report card that was done for Draper. There is also one other organization that we can look at, called America Walks, and they have a similar program with ratings.

Sam Bishop – I know sometimes it has been challenging to staff committees like the Arts Council, and what we are proposing would take a good number of volunteers. Mr. Lewis has talked a number of times about how members of the HAM Radio Club coordinate with city staff and help, we propose a similar relationship between the Active Transportation Committee and Bike Walk SoJo, or another organization like it.

Gary Garner (Resident) – I wanted to bring up the latest Making the City Great, by the city manager. Frankly, it left me a little cold and I gave it to my wife and it left her a little cold too. There is a lot of rhetoric in here about making the city great, and maybe that's the job of the city manager to take a 35,000 foot overview of what the city is, but I found this lacking specifics. He starts out here saying that the environment is one of the main things that is on South Jordan City minds. I looked at some surveys from the Daily Herald, and from the Dan Jones and Associates, and the environment wasn't mentioned once. The things citizens are worried about are growth and housing costs. On my street, probably half the houses have basement apartments in them now because people can't afford to buy a whole house; they either have to share it to make the payments or make room for their kids and other people. Citizens were also worried about education, crime, jobs, tax reform, water, quality of growth, and inflation as the main things. In the Dan Jones survey, they looked at 10 areas and none of them mentioned anything about environment. I'd like to say that a leader once told me the main thing is to keep the main thing the main thing, and I am thinking that maybe you are missing the mark a little bit. Another thing that gave me pause here is this strategic planning. I haven't seen that document, but back in the eighties I spent 10 years on the planning and consulting commission, and I remember the sweat and tears that were used when we developed a city master plan, building plans, and other kinds of plans that went before the city council and the public. He is talking about a strategic plan that could easily be changed to meet the needs of the community; I think that's a little too loose, and lets people maybe be dictators or make the plan whatever they feel like it is. You talk about your

budget here, and I haven't been able to review the budget, but to me I think if the city council would take a position that we aren't going to allow growth in the budget, I know I paid a little more tax this year to South Jordan City, a zero growth budget would make my heart feel better. He talks about being lean and mean, and I agree with that, I would do that. However, I don't see any freezes in hiring, and I looked at the city hiring and there are four to five positions out there for hiring. I think maybe next time we do one of these, it ought to come down to some more specifics and things we can sink our teeth into.

Janalynn Sainsbury (Resident) – I live directly straight across from the Rise Development. I have written a couple of letters over the last few months addressed to all of the city council members, and I haven't gotten any responses at all so I am not sure if you received them or not. One of them was regarding the historic road and I got up in December and talked about that. I think some of you may have said that you received that, and I know that is all said and done, but the last one that I wrote was just a couple of weeks ago. It was regarding the one way road proposal that Mr. Reese was talking about before. I am just wondering if you have received that, and specifically I would like to know why the one way road into the development is not being considered. I do not accept the reason given by the city manager that it will be too confusing to the residents, or that it can't be enforced. As with any new road, residents will learn the entrance and exits quickly; this is very concerning to us. I know I speak for a lot of the residents as a lot of us feel that decisions with high density and roads are made before City Council meeting and those meetings are just a formality.

Mike Lorenc (Resident) – My concern is that I was on the Utah Water Savers website and plugged in my address to see what I was eligible for in terms of water conservation money, because I saw a lot of money flowing in from the state. South Jordan seems to be the only city that does not allow us access to the new state funds and I can't really seem to understand why. When I plug in an address my size but call it West Jordan, I am eligible for something like \$4,000, where here in South Jordan I might be eligible for \$1,000 or \$1,300 for the stuff that you guys are providing. It does seem like it just needs to be a decision to let us do it, because the money is coming in from the state and the program is being managed by the water district, so you guys really don't have to do anything but just allow it. I would like to see some reconsideration on allowing us access to the new state money for water conservation in our landscapes.

Mayor Pro Tempore Harris asked staff to comment on Mr. Lorenc's comments.

Director of Public Works Jason Rasmussen responded that the city gets grant money from Jordan Valley, and we administer our own program because we feel that is more effective and we are able to do more with it. As far as the level of funding, and all the numbers he has seen, we actually provide more to residents than the Utah Water Savers Website does. There is a lot more he could share, but the city gets a lot more bang for the buck doing our own program and we get more participation through how we spend our money. Our funds do come from Jordan Valley, and we will be receiving some additional funds based on what is being allocated to Salt Lake County for water conservation. The city feels like they are spending their money more effectively, and reaching more people, by having our own program versus going with the state

program. The city's website is watersmartsojo.org and our Water Conservation Coordinator is Connor Oswald, and he can walk through the dollar amounts and all the programs available.

Karen Christiansen (Resident) – My family residence is here in South Jordan, directly south of this concept plan from the Rise Development. I came to the meeting on January 17 when we were to be allowed time to address some of these issues, and I stayed about four hours but I got up at 4:00 a.m. the next day and I had to leave so I just got tidbits from everybody else. Our main concern, is that we are the neighbors right next to the development where my mom's property will be shadowed by all of this many, many units of rentals. I have talked to the city a little bit, and I know that you have had someone talk about your road systems and saying that there is plenty of access for this kind of development to have road access in and out. They are telling me that they think 70% of the road will go down, not on 1055 West, but I have been studying this since that meeting and I cannot imagine that after going down on that lower level with the office buildings. That is such a crowded area and if I were a parent I would never take that road. I know that in the summertime they have soccer and sports down there and that is a nightmare. I just do not believe that this plan is going to be conducive to having the traffic not be a nightmare and will really hinder this 1055 West, I don't know how that is going to handle it. Then, we are going to have developments later on, down the road eventually too, and this is really a very big concern. Another concern I have is that our family property has about a 10 foot or more drop from where that is. I know they are going to be covering the ditches and all of those kinds of things, but I am very much concerned about whether we are going to have any retaining walls that are going to protect us from having the slide and runoff into our property because it slopes so far down. I have talked to a few of the development people but I don't get any answers from them on how they are going to handle that. They tell me they are going to have a wall, but I can't imagine a fence wall can contain what might happen with the runoff of water right next to our family property.

Charles Judd (Resident) – Handout (Attachment B). My grandchildren live on 1055 West, and I am concerned about the road system that is there. I have a Master's Degree from the University of Utah in traffic engineering and I have tried to find out what is going on to be comfortable with what the city is proposing. I have looked at a number of drawings, information, and cannot find enough information to feel comfortable about what is being proposed. Specifically, about the intersection that is proposed out on to 1055 West. I am concerned that if this were approved at this point, with the information that you have, that you will be giving the engineers in your department actually an impossible task. I know that it seems to be that what we want to do is move this forward and then have the engineers figure this out, how it can be done safely and properly, and having been in that position before I would not want to be handed this intersection and told to try and make it work. I have listed here in a simple form (Attachment B), an outline of things that should be considered when you do a simple intersection, and I have highlighted for you some of the concerns that you should look at. There are a lot of them in there, I don't have time to go through them all, but there is one major concern that is clear, you should not build an intersection there. Under #4 in Attachment B, alignment and profile, in the yellow it says "grades in excess of 3% should therefore be avoided" or "those in excess of 6% should not be allowed." I went out there today with my surveying equipment and looked at the slope. I don't know for sure where you are going to put the intersection, but if you put it to the north you are going to run into problems with right of way and the house that is right there. If you move anywhere else on there

the slope is greater than 6% and that is something that is not even supposed to be allowed, and I can see a big problem. If you move forward here, say “go do it,” then I can see the city either getting a huge bill to somehow level that whole hill out so that it is less than 6%, and even that is not really a good idea, but you might get hooked with a lot of money to try and put in a proper intersection. I haven’t seen things about how fire trucks are supposed to get around that corner, right of way, the view to get around trees and other things, crosswalks that are going to be built where they can’t be seen. That is my biggest concern, that my grandkids are on there and with an intersection on a slope like that you are going to have cars flying over that hill and all of a sudden there is going to be a crosswalk. There are just a lot of things, as an engineer, I would not want to see here. I am planning to maybe do a GRAMA request to see if there is information I haven’t seen, because everything I have looked at I haven’t seen the answers to so many questions.

Resh Jeffries (Resident) – I want to echo some of the things that they have said about the intersection there and the issues as far as there not being a one way road. I haven’t heard the initial reporting, until Mrs. Sainsbury indicated that there was a report that it was too confusing for it to be a one way road; I hadn’t heard that was the explanation, but I think that should be looked at again. I don’t know how confusing one way roads are, but that seems kind of insulting to the future residents of that property. I am a personal injury attorney by nature and one of the things I wanted to underscore that is the bus stop location for our kids that we drop off in the morning, just off of 1055 West and I believe the cross street is 10550 South. I have four kids of my own that I drop off there in the morning. There are cars that sort of line the edge of that road that wait in the winter hours for the busses to come, and I think that is maybe something that hasn’t been considered and I wanted to echo that is where we drop off our kids. If we add another 164 residents, with presumably 100-200 cars during the morning commute at that intersection that could present a problem. I think what Mr. Judd indicated, that there would definitely need to be a crosswalk, and the issues with the slope and grading and that road coming up to the crosswalk, does present what seems like an issue.

Kate Sainsbury (Resident) – I am also addressing the traffic flow regarding that intersection. If you are unfamiliar with the sport of pickle ball, which I understand was a gift to the city by the developer, it is very popular among the youth of South Jordan and their families. If you try to go to any pickle ball court in South Jordan or anywhere in any neighboring towns, it is very, very crowded. Having another pickle ball court will just bring more traffic flow, which from what I understand is not expected through that entrance. Also, to go along with that, cemeteries are supposed to be respectful and reverent. I know that because I have lived next to one my whole life. I would just like to invite you to think about how that would affect those mourning, if it is very loud with traffic, and lots of traffic causes accidents as you all know.

Scott Sainsbury (Resident) – You should know me by now, I have been here for the last several months trying to petition some common sense into this situation and the last place I want to be is at a City Council meeting with my daughter before she leaves on her mission in the morning. The way I look at this situation, as you all work for us, the people, we pay your salaries, but I don’t feel like our voices are being heard. I have talked to literally hundreds of people in the City of South Jordan and nobody wants more high density housing in the City of South Jordan. I grew up in Washington State, Seattle is a mess, I have lived in L.A. and L.A. is a bigger mess. The

reason that we moved here is because we don't want to live in high density housing. We picked this specific area of South Jordan because we have the best neighbors, dead ones. We also have a lot of neighbors that have agriculture area, and we know that development is coming. Nobody is against development here, but we would like you to stick with the standard third acre lots that make a lot of sense. It doesn't make any sense to move from agricultural zoning to high density zoning, but apparently if you donate a couple of pickle ball courts you can get what you want with this council. My biggest concern is that I have worked in the river bottom for a while, and I know that at 5:00 p.m. when everybody is getting off work, as soon as they figure out there is another exit out of there, one of the city planners estimated about 5% of that traffic will be coming up through there if it is a two way street, but you are wrong as it is going to be closer to 15%. I know there is a couple of different exits, but there will be more people coming up there than you think. The other big concern is that I sat and watched this council while everybody was shaking their head "no, we haven't made any decisions, we are all trying to figure that out," but it sounds like you have known from the very get go what exactly you are going to do. One of my good neighbors came up with a brilliant idea, a win-win compromise, and we have got basically every signature on our street recommending that we go with a one way street. This is a happy medium, it allows first responders to get into the neighborhood immediately without any restrictions, but it will curb our biggest concern of too much westbound traffic out of that neighborhood. People aren't going to exit from the east, they are going to exit from the west because that is where they are going to go grocery shopping, and dropping their kids off at schools. The only reason you exit to the east is if you are going up to Salt Lake City or down to Utah County or east of the freeway, and that is the minority of the time that people are coming out of that area. I just would like you to highly consider the logic, and the sensibility of having a one way street heading eastbound into that neighborhood; I think it makes a lot of sense. As my daughter politely said, with respect to the kindred dead of previous mayors of this city and their families, let's give the cemetery the peace and respect that our dead deserve. That will go away if we make it a two way street.

Bennion Gardner (Resident) – I am here virtually with the Bike Walk SoJo Group, following Sam Bishop's lead. Mr. Bishop has been great to lead and organize, giving some of us residents who want to see safer streets here in South Jordan some means to organize and work on that. I also wanted to thank my council member, Jason McGuire, for his support and interacting with us as well and supporting that, along with Council Member Shelton who has responded to some issues that I have let him know about in the past and who has worked to improve some of the trail access along Bingham Creek. I just wanted to echo what Mr. Bishop and the rest of us have spoken about. Part of the reason I moved to South Jordan and Daybreak was because it's marketed as a very walkable and safe place to be on foot or bike, and it is, and I think that's great and we should embrace that. I think we can do even better across the city to improve access for active transportation. I think even for people who primarily get around by car, I promise there are people in front of you when you are stuck in traffic who, if there was a safe option to get around on a bike or on foot, they would be out of your way and doing that instead; I know that's my case a lot of times. It is kind of a scary prospect in a lot of places to get around on a bike. You have a lot of residents here that have a lot of energy and enthusiasm, and want to help improve the city, we are just looking for an outlet or a way to do that. One option I wanted to mention, that I'm sure you're all aware of and watching, is the model that Herriman has been implementing with the Friends of Herriman Program. They are using a nonprofit, separate from the city, which gives

them some flexibility and allows them to access some funding and grants that might not be available to the city, and that might be something to look at as well. I actually have volunteered and am on the subcommittee for Friends of Herriman for their Trails Committee because I use those trails on a regular basis for recreation. They are open and taking all the input and help they can get, and I think South Jordan would be wise to follow suit and give residents an option to be more involved and help improve our city.

Bronson Mullen (Resident) – Wanted to say nice job everybody on 1055 West, hopefully it doesn't take that much effort to get my concerns considered. I hope everybody is listening and that gets taken into account. I just wanted to get some direction on how to proceed in getting proper safety measures in place at Aspen Elementary for the children walking to school. We have been in contact with the principal and the police department that is responsible for the crosswalk and crossing guards in the area, unfortunately we are feeling like we are getting a bit of misinformation, getting the run around from the police, and we are facing some excuses and I just want to figure out some solutions. The school has been open for almost two years, and for all that time we have been trying to improve the safety of the crosswalks; one of them in particular has a lot of traffic in the morning and afternoon, and several dozen students that cross. The police have told us that the number of people crossing do indeed merit a crossing guard, however we have yet to get that to happen. We haven't even been able to get a crosswalk painted, which is really frustrating. There is not a single school zone sign anywhere around the entire school with four intersections, let alone any flashing lights that say 20 MPH or anything like that, not even a sign that says it's a school zone. After having extensive conversations with the police department and the principal, we were originally told that the issue was that there wasn't crossing guard availability, so we worked to resolve that. Now that is resolved and the latest reason is that there is a lack of funding. I just want to know what steps to take, to get some direction on how we get proper signage, a crosswalk painted, then where to go for funding for additional crossing guards in all the places that have the traffic meriting a crossing guard.

Mayor Pro Tempore Harris instructed Mr. Mullen to contact our City Manager Gary Whatcott, and he will help with answers to those questions.

Carmen Rufner (Resident) – I am here to talk about that street that is being discussed on the building development that is coming. I, along with others of my neighbors, have signed a petition to ask for a one way street versus a two way street. My house faces that other side of the road, so for me it has a big impact because I would have cars coming and going, and me coming out directly from the driveway into the street when cars are coming in and out; for me that is a safety issue, even though I do still pay attention. I also have my children playing on their bikes, who would still need to pay attention just like in any other community. I think a one way street going into the community, versus coming out, would have less impact. I know it's a smaller width, and I think that it would be a little more doable. I just think that the impact to our community, and right there in front of my house, is pretty big for the people that live there and walk every day. We know that we have to adjust to new and modern things that have to happen in our community, but I wish that you would hear that option and have it open to the developers present here, if there are any. It would be nice if they would take that into account.

Mayor Pro Tempore Harris closed the public comment portion of the meeting. He asked staff to get in touch with Mr. Bishop in regards to his committee proposal. He asked Manager Whatcott to comment on the road.

City Manager Gary Whatcott said Deputy City Engineer Jeremy Nielson is the traffic engineer for the city. Our City Engineer Brad Klavano is not here, but was in the last meeting where residents met with him and Planning Director Steven Schaefermeyer. They did discuss the one way street, and there were concerns with the city engineer and himself with making sure they made a street that worked right. There are other roads inside that subdivision, and they are affected with a street being made one way. They want to make sure this doesn't create more liability for the city with improper or confusing signage that might not work right or create more traffic issues related to safety. There are still more remedies to refine, and the way the council left that direction was for staff to look for more solutions; he doesn't believe they have all those answers yet, but they are still certainly looking. Anyone in the audience is always welcome to contact the city and speak with him or any of the engineering team with questions on the project.

Director of Planning Steven Schaefermeyer noted that there always seems to be some confusion about what was and wasn't approved, what the next steps are. He clarified that the developer has submitted what is called a preliminary plat, the document once recorded at the county recorder's office will actually subdivide the property so they can start building. That preliminary plat goes to the planning commission in a public hearing after staff has had a chance to review those plans, as what was presented before was just a concept. They would then be getting into things like grading and getting very specific about what the property will look like. There will be notices sent out for that meeting, no date has been set yet for that meeting because there are still corrections that need to be made by the developer and developer comments that need to be addressed from staff; part of that review is of course the street. That is the latest update on where they are in the process. Once that preliminary plat is approved by the planning commission, and they have to approve it if it complies with all the city ordinances, then a final plat is recorded and they move forward with building permits.

Council Member Zander asked Police Chief Jeff Carr to comment on the crosswalk by Aspen Elementary.

Police Chief Carr is unsure of the specific concerns, he was just trying to text the lieutenant over that area to see what kind of conversations have already taken place. They will certainly look at that, as the city is responsible for the crossing guards and there is a process they go through to determine whether a crossing meets that threshold. It sounds like the resident has maybe received information that it does meet that threshold, so they would typically look at that. They try not to fund them mid-year, but if the need arises they will try to do that. He will do some follow-up with the lieutenant to find out what has been done there. They do the safe walking routes usually once a year, ahead of the school year, to determine where they want those routes to be. The city is part of that process with the school, and they will look into it. Mr. Mullen can reach out to Chief Carr for additional help.

Council Member Zander noted that in addition to the crossing guard, she would like to know if the city would look into that crosswalk not being painted and why that's the case.

Deputy City Engineer Jeremy Nielson will check into that. He knows the signs were put up as of six months ago, because he checked them, but he hasn't driven out there in a bit and he will confirm they have made it through the winter.

Mayor Pro Tempore Harris would like to understand more about the one way road, but today during public comment is not the time to add an agenda item. He asked staff to provide a bit more information to the council after this meeting to help understand the one way road issues and concerns.

G. Action Item

G.1. Resolution R2023-09, Authorizing the Mayor to enter into an Interlocal Cooperation Agreement with Salt Lake County for 2022-2028 UPDES Media Campaign Cost Sharing (*By Director of Public Works Jason Rasmussen*).

Director of Public Works Jason Rasmussen reviewed background information from the Council Report, as well as his prepared presentation (Attachment C).

Council Member McGuire motioned to approve Resolution R2023-09, Authorizing the Mayor to enter into an Interlocal Cooperation Agreement with Salt Lake County. Council Member Zander seconded the motion. Roll Call vote was 5-0, unanimous in favor.

H. Public Hearing Items

H.1. Resolution R2023-08, Amending the Future Land Use Plan Map of the General Plan of the City of South Jordan from Economic Infill Opportunity (EIO) to Stable Neighborhood (SN) on the property located at 10827 S. 1055 W. and 10828 S. 1055 W.; Devon Hatch (Applicant). RCV (*By Director of Planning Steven Schaefermeyer*)

Planning Director Steven Schaefermeyer reviewed background information the Council Report, as well as his prepared presentation (Attachment D).

Mayor Pro Tempore Harris invited the applicant up to address the council.

Devon Hatch (Applicant) said he personally plans on making his home one of the lots on the top, so he has a personal interest in this development. When he first came here and saw it, he loved the feel of the neighborhood, and that's why he chose to go with the larger lots. He wants to keep this a nice place and stay along with what he fell in love with when he drove through. He thanked staff and the planning commission for their professionalism. He knows that change isn't always welcome, and to the residents around there he wanted to say that he is excited to work with them if they want to reach out to him. They are trying to make good neighbors there, and he plans on living there so he would like to have conversations if they have concerns and work through them. He would like to make it a beautiful community and not go high density, as he could have gone that route, but since he is going to live there he is excited to try and keep it a nice area.

Mayor Pro Tempore Harris opened the public hearing for comments.

Travis Hiatt (Resident) - Asked to have staff display images from Attachments E & F. The first issue I want to bring up is water, it was mentioned a couple weeks ago our concerns about the water. There are 14 homes that access the irrigation line that is being turned into a driveway basically, according to this proposal and images 1 and 2 from Attachments E & F. That is only part of it, it actually runs to the north quite a ways and down. No one has brought forward a workable proposal to address the irrigation water. The proposed driveway is directly over the irrigation line and what they are actually saying is that there is going to be a problem. They admit there is going to be a problem, but no one has really said how we are going to mitigate it so that's concerning for everybody; especially those 14 water users. Known and expected problems should be avoided, at the very least given workable, reasonable solutions, of which there have been none. From South Jordan Planning Commission notes dated November 8 regarding the Rise Townhomes also being built on 1055 West, Deputy City Engineer Jeremy Nielson said "they have a legal responsibility to ensure the water goes where it has historically gone," no part of this plan is going to make that happen so we are obviously concerned about that. The existing water table also changes, as will runoff and flow of the water to the existing homes. He showed the tree line that was abruptly removed in images from Attachments E & F. Those trees to the right of the white building are also gone now too. The existing water table will change, that tree line is going to disrupt that and there was also some springs under those trees that will be disrupted. The Rise Development is adding 155 townhomes which we spoke about earlier, we have heard several comments about that. We had the same concerns about the increased traffic and I think that was pretty well spoken to this evening. We spoke a couple of weeks ago specifically to the negative impact to property value and legal counsel basically told us there was no substantiation. It was a little bit patronizing to be honest, because when we bought the house we had trees and the mountains. Now, we have the view shown in Attachments E & F, and it is almost impossible to imagine that is the same backyard. I think we do have substantiation, and we have been talking to some appraisers and ran out of time. We weren't able to get an appraisal put together in this short amount of time, but we will be doing that and bringing that to the council for the next meeting. We believe that we do have substantiation for property value loss. We are pretty passionate about this.

Melanie Hiatt (Resident) – I want to note that on our public notice we received, I sent the email several times and it bounced, and the phone number is no longer in service. I did get someone nice to help me get that sorted out, but thought you should know that on the letter going out it is not working. Gary Whatcott is popular tonight in his South Jordan newsletter, he said "Most cities in Utah have to cope with rapid growth and urbanization on an unprecedented scale. These challenges are dealing with the fallout of haphazard growth, and planning is the engagement of residents. Our elected officials want everyone engaged, we want to listen to neighborhoods. The story of change is about the engagement of residents." Awesome, here we are, the residents, and the herd that just left are here for the same reason as well. We are against the developer who is requesting an agricultural area of zoning be zoned to residential. This provided plan is extremely unusual, does not integrate environmental thinking, is not cost efficient, and does not make our lives better. The growth is unprecedented, hazardous and unwanted. We were patronized by the zoning commission that losing a tree is difficult, and that change is hard; however, we were not talking about a tree, we were talking about a tree line that was home to a family of Red Tail Hawks, owls and other animals. It was in fact a small forest, hundreds of years old that was literally bulldozed down as shown in Attachments E & F; it will have a detrimental effect on the

neighborhood. Lots has been said about this one lane road with no curb or gutter. My images 8-10 on Attachment F show where this development is actually happening, where it is a one lane road, the road with my vehicle on it. There are no plans to improve the road conditions to manage the more than doubling of traffic to impact that neighborhood. The proposed driveway is our biggest concern, and the cul-de-sac of this development will run through numerous backyards. They are literally proposing a roadway be added to tree line, farmland and pasture. Cars will now be driving through our backyards at approximately a 10%-20% incline, so that headlights will be shining into our houses. The required angle of the proposed driveway would make their suggested fence line completely useless in blocking traffic and light pollution to the existing neighbors. At the zoning meeting, and it was said again this evening, the developers claim to want to be team players and good neighbors who want to work with the neighborhood and just want to be peaceful, coexist, etc., yet there has been zero communication with the existing neighbors. Not one call, not one letter, not one email, not one knock on the door. This is not a developer wanting anything that is best for the neighborhood when 98% of us are saying we don't want this, they are only interested in what's best for their pocketbook. So after zero communication from these peaceful developers they brought in bulldozers and took out a 100 year old tree line and eight fresh water springs; their actions are speaking loud and clear. Just because Utah is experiencing unprecedented growth does not mean that we should be. Maybe some agricultural zoning should be left for the neighborhoods and residents of South Jordan that want it there, maybe every remaining scrap of grass should not be turned into townhomes or luxury homes or density housing, and maybe we like the cemetery road the way that it is, quiet, one lane, unobstructed and undeveloped. Not having solutions for irrigation, not having solutions for roads and traffic, not having solutions for current existing neighbors and homeowners loss of property is simply unacceptable.

Kenyan Clark (Resident) – (Attachment G) submitted a letter brought and read by neighbor, Travis Hiatt.

Charles Judd (Resident) – On the drawing that was shown before, it shows that coming down 1055 West there is now a cul-de-sac in the middle of the road, and then a cul-de-sac up. I'm wondering if the plan is to end the road there because it doesn't seem to make a lot of sense to end the road there. The drawing shows curb and gutter and sidewalk and everything there, and I do know that we need to consider other people that live down that road; that's just something that needs to be considered.

Mayor Pro Tempore closed the public hearing to comments.

Council Member Zander asked for clarification on Mr. Judd's comments regarding the two cul-de-sacs and where they will be placed.

Engineer Nielson said the reason for the two cul-de-sacs is that the road to south is private property. With both cul-de-sacs, it will provide a good turnaround for the snowplows and other city services, like garbage trucks, that use 1055 West.

Council Member Zander said she drove down 1055 West after the last discussion, and was able to see where it became a private lane. However, when proceeding to the very end of 1055 West, she noted there didn't appear to be an outlet and asked for confirmation on that.

Engineer Nielson said it is a dead end to the south, and that has been a problem for the snow plows who have had to turn around on private property, which has caused some problems.

Director Rasmussen said the snowplows have to go partly down so they can turnaround. That area further south that they use to turnaround is not ideal and there have been some incidents of plow trucks hitting things.

Council Member Zander asked to confirm that this proposal with the cul-de-sac would have a through cul-de-sac.

Engineer Nielson noted that it is more of a knuckle, with a road going to the west and drive access to the south for the private lane to continue. There will be a 90 degree bend, or the knuckle that goes to the west, then there will be a formal cul-de-sac at top of the hill.

Council Member Marlor noted that the applicant owns property on the east side, so that shouldn't be a problem as he understands.

Engineer Nielson agreed with Council Member Marlor and said that was his understanding as well.

Council Member McGuire asked regarding the rezoning, if they are basically only looking at what the actual rezoning will be, not dealing with any water issues that would be resolved later in this process.

Director Schaefermeyer said this is hard because we are changing the land use in preparation for actual physical changes. As a result, they are asking the council to think about this in more general terms, while also considering the context of the area and what is around it. The ordinance also requires the developer to provide a concept plan. Things like water and existing canals are always things that, until staff addresses them with the developer, are hard. It is helpful to have that information at this stage, but it's hard to provide a lot of answers. The developer isn't expected to spend all the money to create those solutions when there is a chance the council could vote no on the proposal. That is one of the challenges, staff can only require the developer to provide so much information before there is a determination of whether they can actually proceed, and that's the decision being made tonight. All the issues being presented tonight will be addressed during the subdivision process. Not only is it the subdivision plat that is recorded with the county, but there is a set of construction drawings that talk about everything from grading to water to existing conditions and how to address them.

Council Member McGuire noted that he is very sensitive to the Hiatt's concern over the water table issues. His place of business was built on top of some warm springs and every time there is a construction project it changes the water flow through the area to the area across the street. In turn, when they make a change across the street his area suddenly has water issues, so he wanted to make sure that concern was addressed.

Attorney Loose said there was a quote shared about the historic flows of water that had the correct sentiment, but the legal conclusion being drawn is probably incorrect. It is not that historic flows have to continue to run exactly where they have always ran, the developer or applicant is just required to deal with historic flows. Essentially, the applicant can't require those upstream to turn off the water, they have to allow the water to continue flowing to them and then deal with it. That can be done a number of ways including piping it and taking it in another

direction, but they just can't ignore it by making someone else deal with the problem. That is generally not dealt with 100% at a rezone, it is generally a subdivision issue, but all of that does weigh into the council's decision if the zoning and the amount of density is correct for this area and the future land use being considered.

Mayor Pro Tempore Harris noted at some point the applicant will have to do testing of the water flows to prevent water table issues in the future, and asked what other kinds of testing will be required in conjunction with that.

Engineer Nielson responded they will be doing geotechnical studies, borings on the site to understand the water table, etc., and developing any mitigation needed based on those results. That will all be a part of the site plan approval, which happens after this rezone step.

Mayor Pro Tempore Harris asked if staff felt those studies would address the concerns mentioned tonight.

Engineer Nielson responded yes, as that is the purpose of the geotechnical studies, to look at the water underground and the soils.

Mayor Pro Tempore Harris brought up the concerns about the irrigation canal, now that it will be somehow covered up or blocked, and asked for more information on that from staff.

Engineer Nielson responded yes, as part of that site plan they will have to work with the ditchmaster to ensure those with access previously will continue to have their access to that water. It may not be through the same means, as there may be a turnout from the pipe instead of a ditch, but that will happen as part of the subdivision plan.

Council Member McGuire asked the developer if he had considered any other additional layouts to his proposed site. There is the road wrapping all the way around and he asked if the developer has looked into other options for that.

Mr. Hatch responded yes, they did look into a couple other options. However, the difficulty is that the land has a very steep drop off and a hill there. They have tried to come in from many different ways and places, but this seems to be the only way to get to those other two lots above and still meet some of the requirements the city has for the roads and the sizing there. Regarding the water concerns, they have looked at the ditch there and most likely they have the means to pipe it and cover it up so it still flows through; that will be addressed through the proper process.

Council Member Zander addressed the developer, noting that while looking at the map with regard to slope, one of the neighbors tonight spoke to the grade during public comment. She asked if the northwest corner down to the road is the slope, or if the slope going a different direction like north to south.

Mr. Hatch said the slope is from the west to the east, and in addition there is an existing sewer line running through the property that they are working on locating on the sloped hill. It's a challenging piece of property to develop and they have had to really work hard in figuring out how to get this to work. They feel like they have addressed a lot of those sloping challenges and issues with this current plan. The sewer line and drop off are the reason why they had to maneuver that private road around to get it up here. They do plan on lowering that road quite a

bit, and he knows there was concerns over lights shining in windows. That road will be low enough that with the fence there shouldn't be any issues with lights in homes at that point.

Council Member Zander asked about a possible retaining wall along the west border.

Mr. Hatch said yes, it can be seen on the plan as a solid black line.

Council Member Zander asked to confirm that Lots 1 and 2 will be at a higher elevation than the other four lots, and that they will be level with the neighbors to the west.

Mr. Hatch responded that is all correct.

Council Member Shelton referenced the road that circles around, coming out of the circle on the property, and asked if that will also be a public street.

Director Schaefermeyer responded that road will be a private road or driveway. Regarding the comments on the challenges with 1055 West and the improvements, this is generally how roads like this are improved; as properties come the owners improve the sections they are on. Since this owner owns both sides of this section on 1055 West, they will be improving 1055 West as they develop.

Council Member Shelton asked if this section of 1055 West will be wider with sidewalks, curb, and gutter.

Director Schaefermeyer responded yes.

Council Member Marlbor noted that what they are seeing here, and on the Rise Development, is change, and it is very difficult to deal with change. In his experience on the council, every subdivision, change and road is hard to deal with. It's not fun to see trees that you have grown to love disappear, but it is going to happen throughout that entire corridor, just as we have seen throughout almost all of South Jordan. There is not very much property with large acreages left, so he is not surprised that these are being developed. His heart goes out to the residents that are there, because they would just as soon never see a change happen, but people have a right to develop their property and that is going to happen there and throughout any undeveloped area in our city. His feeling is that everyone has to work together and realize that these individuals do have rights to develop, and those impacts are unfortunate, but they do have the right to move forward with their properties, investments, etc. It is the council's responsibility to make sure that it meets all codes and ordinances, that it will be safe, and all those kinds of things that need to take place from the city's standpoint.

Mayor Pro Tempore Harris asked the council if they would allow one individual comment from someone in the chambers, as a member of the audience indicated they had something to add.

The council members all agreed they were fine with one more comment.

Milo Tenney (Resident) – I live down at the south end of 1055 West. I want to first make what I think is a correction, as I understand that it is not all private land beyond that point. I have researched it, but when I bought the property I was told that the city owned the street in front of my house which is well down that lane. I don't know to what extent we are going to get cut off from snowplows and all that kind of stuff for the rest of that lane, but I don't think it's all private. You are approving a whole lot of change on 1055 West right now, there is a whole lot of very

rapid change going on there and I hope you can consider that we expect change, but it doesn't have to happen all at once.

Council Member Marlor asked staff for more information on the last comment regarding the lane and where it ends in terms of public right of way.

Engineer Nielson thinks there may be pockets of public space along that road, but that is where a large stretch of private lane starts as well. Staff is unable to access the maps for verification of this, currently due to an internet issue.

Council Member Marlor asked if Engineer Nielson is saying that from the base of the proposed cul-de-sac on 1055 West there could be bits and pieces of public right of way, but for the most part it is a private lane.

Engineer Nielson said that is correct. The property line is where the public right of way ends, but further down to the south there are pockets of public right of way.

Council Member McGuire noted he was finally able to pull up the Salt Lake County Assessor's website, and it does show pockets of public right of way further south. It looks like the Rodriguez property is the first section that is private road, and then it opens up to some public right of way after that.

Director Schaefermeyer is unsure of the entire history, but at some point when the Park Place development came in there was discussion about whether or not to connect that road. When that development came in, the city council vacated a sliver that had been dedicated to the city because at some point there was a plan to connect 1055 West all the way down through. As a lot of these subdivisions west of 1055 West and this private drive were developed, the city at the time required them to dedicate portions of their property with the idea that there would be a road going through. He believes that is the most likely reason for those pockets of public right of way.

Engineer Nielson added that he is not aware of the city changing any of their services because of this break in the private lane, especially on those public road sections. There might be an option for a smaller plow truck on the private lane, but it would still be of benefit to have that cul-de-sac there as turnaround.

Council Member Zander asked about Jordan River Drive and where it initially broke through from the west to the east a few years ago.

Director Schaefermeyer said there was an existing stub road connected to the new development that then connected that down through the park.

Council Member Zander noted from the map it looks like 1055 West essentially dead ends about 50 yards from Jordan River Parkway, and asked for confirmation of that.

Director Schaefermeyer responded yes, there were several lots that were part of the Park Place subdivision that cut off that access to Jordan River Drive. In addition, just east of there he speculates that the parcels to the north along that mostly private drive were required during development to dedicate because of potential connection at the time.

Council Member Zander said it looks like Jordan River Drive has a home or two directly at the end of 1055 West, so she assumes that 1055 West will never be a through road.

Director Schaefermeyer said that is correct, unless private property owners get together or the city has a purpose to condemn it; however, that has not been the general practice of this council, to condemn for things like that. Based on his memory, the lot at the end of 1055 West was actually included in the Park Place subdivision but it accesses from 1055 West, not from any of the Park Place roads.

Council Member Marlor motioned to approve Resolution R2023-08, Amending the Future Land Use Plan Map of the General Plan of the City of South Jordan. Council Member Shelton seconded the motion. Roll Call vote was 5-0, unanimous in favor.

H.2. Zoning Ordinance 2023-02-Z, Rezoning property located at 10827 S. 1055 W. and 10828 S. 1055 W. from the A-1, A-5, and P-O Zones to the R-1.8 Zone. RCV *(By Director of Planning Steven Schaefermeyer)*

Mayor Pro Tempore opened the public hearing for comments. There were no comments and the hearing was closed.

Council Member Zander motioned to approve Zoning Ordinance 2023-02-Z, Rezoning property as stated. Council Member Marlor seconded the motion. Roll Call Vote was 5-0, unanimous in favor.

H.3. Ordinance 2023-02, Adopting an amended and updated Impact Fee for Water and Transportation within the “Last Hold Out” properties; establishing certain policies related to Impact Fees for Water and Transportation Facilities; establishing Service Area; and/or other related matters. RCV *(By Director of Strategy & Budget Don Tingey and LYRB Vice President/COO Fred Philpot)*

Director of Strategy & Budget Don Tingey indicated that the next two items (Items H.3. and H.4. on tonight’s agenda) are similar in nature. Item H.3. is dealing with the “Last Hold Out” property, which is in the middle of Daybreak. The other property in Item H.4. is on the west side of Daybreak, just east of the current U-111 alignment. He gave a brief history of both properties from the Council Report.

Fred Philpot reviewed the prepared presentation (Attachment H) and background information from the Council Report.

Mayor Pro Tempore Harris opened the public hearing for comments. There were no comments and the hearing was closed.

Council Member Shelton asked if the facilities, along with the infrastructure had already been built.

Director Tingey said they have in some cases, and some is to be built in the future.

Council Member Shelton asked if the facilities already completed were done by the developer at Daybreak.

Director Tingey responded yes.

Council Member Shelton asked if when those fees are collected the city pays part of them to the developer at Daybreak.

Director Tingey said yes, and there will be an agreement with the developer as soon as they go into place. In addition, part of that fee will be used by the city to build additional infrastructure.

Council Member Zander asked if by adopting this, the city is agreeing to the maximum amounts stated.

Director Tingey responded yes, it would be full reimbursement.

Council Member Shelton asked if the owner of the properties happens to be the same, and if they have been notified.

Director Tingey responded yes, the owner happens to be the same and they received copies of the studies years ago, along with any updated ones.

Attorney Loose noted that at one time the owner asked the city to draft a contract where, instead of going through this process, the owner of those properties would just contractually pay those fees back, commiserate with what is being seen. However, they recently said that it would work better to do it as an impact fee analysis and that is why the city went through this process. That being said, the owner is very well aware and has never objected to the concept of what's happening; they just preferred this route over the contract that was drafted in 2018.

Director Tingey said this is very familiar to the development world. Paying impact fees keeps this straight forward, even though the other way may have been simpler with a contract.

Council Member Marlbor asked if the impact fees are considering when the facilities will actually be built, and that if it takes more time those facilities could be more expensive. He also asked when they are anticipating these will be built.

Director Schaefermeyer said there is some inflation built in.

Mr. Philpot added that if those costs change there may be a need to amend the impact fee analysis to account for those changes in cost.

Director Tingey said City Engineer Brad Klavano will be watching those costs as the projects come online.

Council Member Zander asked for a ballpark of acreage with the Lark Meadow Property and the Bastian Hold Out Property.

Director Tingey responded the Lark Meadows property is 180 acres, the school district piece on the west side of Mountain View Corridor is 103 acres, and the Bastian piece on the east side is about 35 acres.

Council Member Zander asked if the properties are still owned by the Bastians and the school district, and who owns Lark Meadows.

Director Tingey responded that Doug Young has it under contract, but it is still owned by Bastians with an option for Mr. Young.

Council Member McGuire motioned to approve Ordinance 2023-02, Adopting an amended and updated Impact Fee for Water and Transportation within the "Last Hold Out" properties; establishing certain policies related to Impact Fees for Water and

Transportation Facilities; establishing Service Area; and/or other related matters. Council Member Zander seconded the motion. Roll Call vote was 5-0, unanimous in favor.

H.4. Ordinance 2023-03, Adopting an amended and updated Impact Fee for Water and Transportation within the “Lark Meadows” properties; establishing certain policies related to Impact Fees for Water and Transportation Facilities; establishing Service Area; and/or other related matters. RCV *(By Director of Strategy & Budget Don Tingey and LYRB Vice President/COO Fred Philpot)*

Mayor Pro Tempore Harris opened the public hearing for comments. There were no comments and the hearing was closed.

Council Member Shelton motioned to approve Ordinance 2023-03, Adopting an amended and updated Impact Fee for Water and Transportation within the “Lark Meadows” properties; establishing certain policies related to Impact Fees for Water and Transportation Facilities; establishing Service Area; and/or other related matters. Council Member Marlor seconded the motion. Roll Call vote was 5-0, unanimous in favor.

I. Staff Reports and Calendaring Items

Manager Lewis reminded everyone of the upcoming SoJo Date Dash, and due to expected council member attendance it will be noticed as a potential quorum.

Director Rasmussen gave an update on the signage discussed at the December Strategic Planning meeting. Staff has been working on that and they have ordered new border signs that will be installed over the next week. Staff has also been working on all collector street signs to replace the faded ones and straighten out others where needed. The new city welcome signs will also be replaced within the next week. He also wanted to address the public comment regarding the water conservation rebates, noting that the gentleman shared with him afterwards that he is an employee of Jordan Valley, which was not shared during his comments. This has been looked at in the past, and the city really gets so much more by managing the program internally. This allows us to offer the residents more and get them involved in more rebates, rather than having a random employee from Jordan Valley or Utah Water Savers come. Staff can give the residents more attention and feel that they are providing a better service to those residents rather than what Utah Water Savers does. Connor Oswald does a fantastic job and Director Rasmussen feels South Jordan’s program would be ranked as top in state.

Attorney Loose gave legislative updates. Regarding SB 174, Senator Fillmore came out. This is the first of the local land use development revisions, and Representative White came out as well. He gave kudos to Senator Fillmore, from where he started and where he was getting input, the bill is actually very moderate. It’s not exactly where the city and League would want it, but it certainly is more in line with what has been proposed with substantial changes to the subdivision process for cities that still have their city council involved in administrative items. Our city was part of the example showing that the council doesn’t have to deal with all the administrative items, and this council gave that up thanks to involvement from Director Preece around 2006; Council Member Marlor was also on the council at that time. Those are changes going into state law, and the penalties are still being

negotiated, but they are much closer to what the League can live with; they are certainly not where the development groups want them. He thanked the council for speaking with Senator Fillmore, inviting him to sit at their table, as all of those little things help him understand what his constituency and the residents of Utah are feeling. There aren't a lot of other big impact bills right now, but there are currently some regarding retirement and he thanked City Manager Whatcott and Director Seager for their efforts in keeping him updated. They are working on getting costs for the city in regards to some of the current bills, including the public safety retirement bill from Representative Gwynn. That bill specifically would be a direct hit that would have to be poured into public safety retirement, which would then affect how other benefits are done. The League has no allies to kill that bill, so it has a lot stronger chance this year over the previous years that something similar has been attempted.

Council Member McGuire asked if those retirement costs would be yearly and ongoing.

Attorney Loose responded it will be an annual thing that escalates over time. There are a number of unfunded mandates similar to that, along with direct cost revenue reductions with the gas tax, which is a big one. The cities have been told they will be able to make those costs up, however there are two issues at the state level. Firstly, the state has lots of money, but the local levels don't through their revenue streams, and in turn the state is cutting taxes thinking everyone else is flush. The League is continuing to remind them that isn't correct, and he encouraged the council members to remind them of that as well as at the local level that we are having to increase property taxes and doing what needs to be done because the revenue streams just are not there. Secondly, revenue neutral on a large basis, does not mean it is equal to everyone. The Sand and Gravel Bill has that effect, with some cities being winners and others losers; same with the gas tax bill. As was seen here when the taxes went away with the manufacturing bill, those losses of \$1 million a year can really create large deficits. The council needs to reinforce with them that they may not want to change too much at once, and be very thoughtful of how that impacts their constituents.

Council Member Zander noted she wanted to do some follow-up texts and phone calls to some of the elected officials she knows well. She asked if she were to note that as they are looking at the tax revenue stream to please consider the effects it has on the city, who on the staff she can send them to with questions.

Attorney Loose responded she can contact either Manager Whatcott or himself.

Council Member Zander asked about SB 174, Senator Fillmore's Bill.

Attorney Loose said he would send links for both Senator Fillmore and Senator White's bills to the council for review.

Council Member Zander would like to contact those representatives and share her opinion with them, as well as compliment them on progress made.

Attorney Loose reminded the council about the meeting this Saturday at 8:00 a.m. at the hospital, and that there is no virtual option.

Manager Lewis said that should be on everyone's calendars multiple times in case they are interested.

Council Member Marlbor shared he had dinner Saturday night with a friend named Paul Pugmire, who is the Director or CEO of First Tee of Utah. First Tee of Utah's mission is to get youth involved in golf as young as second and third grade for recreation and exercise. Mr. Pugmire noted that he felt his relationship with Assistant Director of Recreation Jake Druce and the other folks at Mulligans is terrific, and that he is grateful for the relationship and feels that things are going very well.

Manager Whatcott mentioned the bill being proposed that would require an SRO in all the charter schools. That language was adjusted from "shall" to "may," so the fiscal impacts are quite a bit different. The substitute draft came out much more favorable for the city from a fiscal standpoint, but the school districts are still on the hook for a lot on that bill.

Council Member McGuire motioned to adjourn the City Council meeting. Council Member Marlbor seconded the motion; vote was unanimous in favor.

ADJOURNMENT

The February 7, 2023 City Council Meeting adjourned at 8:54 p.m.

SOUTH JORDAN CITY CITY COUNCIL REPORT

Council Meeting Date: 2/21/2023

Issue: A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF SOUTH JORDAN, UTAH, ADOPTING THE SOUTH JORDAN STREETScape MASTER PLAN FOR THE CITY OF SOUTH JORDAN

Submitted By: Colby Hill
First Reading Report Date:

Department: Public Works

Staff Recommendation (Motion Ready): Approve Resolution 2023-11 adopting the South Jordan Streetscape Master Plan for the City of South Jordan

BACKGROUND: A streetscape master plan has been prepared by VODA Landscape & Planning and is now ready for adoption by City Council

TEAM FINDINGS, CONCLUSIONS & RECOMMENDATIONS:

FINDINGS: The South Jordan Streetscape Master Plan provides a framework for both beautifying the streets in the city's primary corridors and reducing overall maintenance and water resource requirements. The plan includes a review of existing relevant planning documents and describes the plan implementation process which will be use over time as parks strips and medians are incrementally updated.

CONCLUSIONS: The South Jordan Streetscape Master Plan will ensure that a coordinated, master-planned effort is undertaken to plan for beautifying the streetscapes and maintaining the South Jordan identity while reducing maintenance and water consumption.

RECOMMENDATIONS: For reasons outlined in the South Jordan Streetscape Master Plan and staff presentations, staff recommends that the City Council approve Resolution 2023-11, the South Jordan Streetscape Master Plan for the City of South Jordan.

1. **FISCAL IMPACT:** As outlined in the Streetscape Master Plan

ALTERNATIVES:

1. Adopt Resolution 2023-11
2. Deny Resolution 2023-11

SUPPORT MATERIALS:

2. Resolution 2023-11
3. South Jordan Streetscape Master Plan, dated January, 2023, by VODA Landscape & Planning

City Council Action Requested:


Department Head


Date

RESOLUTION R2023 - 11

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF SOUTH JORDAN, UTAH, ADOPTING THE SOUTH JORDAN STREETSCAPE MASTER PLAN FOR THE CITY OF SOUTH JORDAN.

WHEREAS, the City of South Jordan desires to reduce water use and maintenance resources in park strips and medians maintained by South Jordan City, and the City desires to create attractive landscaped park strips along South Jordan's key travel corridors; and

WHEREAS, A Streetscape Master Plan dated January 2023 has been prepared for the City by VODA Landscape + Planning LLC to help the City meet these goals; and

WHEREAS, The City had adopted a General Plan as required by Utah Code Ann. §10-9a-401 to plan for present and future needs of the municipality and to plan for the growth and development of land within the City; and

WHEREAS, The City desires to add the Streetscape Master Plan as an element of the General Plan to assist the City in meeting its water use and streetscape goals; and

WHEREAS, the City Council finds that adopting the Streetscape Master Plan and incorporating it into the City's General Plan will support the best interests of the City and will promote the public health, safety, and welfare of the residents of the City of South Jordan.

NOW, THEREFORE, BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF SOUTH JORDAN, UTAH:

SECTION 1. Adoption. The document titled South Jordan Streetscape Master Plan prepared by VODA Landscape + Planning LLC, a copy of which is attached, is hereby adopted as the Master Streetscape Plan of the City of South Jordan and shall be included in the City's General Plan.

SECTION 2. Severability. If any section, clause or portion of this Resolution is declared invalid by a court of competent jurisdiction, the remainder shall not be affected thereby and shall remain in full force and effect.

SECTION 3. Effective Date. This Resolution shall become effective immediately upon passage.

[SIGNATURES ON FOLLOWING PAGE]

**APPROVED BY THE CITY COUNCIL OF THE CITY OF SOUTH JORDAN, UTAH,
ON THIS _____ DAY OF _____, 2023 BY THE FOLLOWING VOTE:**

	YES	NO	ABSTAIN	ABSENT
Patrick Harris	_____	_____	_____	_____
Bradley Marlor	_____	_____	_____	_____
Donald Shelton	_____	_____	_____	_____
Tamara Zander	_____	_____	_____	_____
Jason McGuire	_____	_____	_____	_____

Mayor: _____
Dawn R. Ramsey

Attest: _____
City Recorder

Approved as to form:



Gregory M Simonsen (Feb 15, 2023 10:44 MST)

Office of the City Attorney

CITY OF SOUTH JORDAN

STREETSCAPE MASTER PLAN

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APPENDICES

1.0 INTRODUCTION



Figure 1.01 - Refurbished park strip example: 5'-0" planting width

The South Jordan Streetscape Master Plan provides a framework for both beautifying the streets in the city's primary corridors and reducing overall maintenance and water resource requirements.

The plan includes a review of existing relevant planning documents and describes the plan implementation process. This process will be used over time as park strips and medians are incrementally updated. These updates will be made in both street reconstruction projects and park strip only construction projects.

Rather than city-wide site specific planting plans, a planting design algorithm is provided. This algorithm is designed to enable city personnel to create a planting plan unique to each project/corridor using plants and layouts that meet the city's streetscape goals.

This plan is designed to achieve the following tasks and goals:

Task 1: Inventory and Evaluation of Existing Parkstrips and Streetscapes

Task 2: Overall Re-landscape/Refurbishment Plan

Task 3: Goals, Objectives and Policies Development

Task 4: Plan Adoption

Goal 1: Reduce water use in park strips and medians maintained by South Jordan City.

Objective 1: Install new low water use, drought tolerant plants in all park strips and medians which are maintained by the city.

Objective 2: Install new, more efficient irrigation systems that can be adjusted to current conditions.

Goal 2: Reduce maintenance resources required for park strips and medians maintained by South Jordan City.

Objective 1: Transfer park strip maintenance to adjacent property owners where city is not required to maintain the park strip.

Objective 2: Minimize plant and irrigation system maintenance tasks to seasonal and/or annual maintenance requirements.

Objective 3: Eliminate turf grass from all park strips and medians which are maintained by the city.

Goal 3: Create attractive landscaped park strips along South Jordan's key travel corridors.

Objective 1: Use only low-water and low-maintenance plants that will thrive in the city's climate.

Objective 2: Use plants that will beautify and improve the experience and perception of the city.

2.0 PLANNING DOCUMENT REVIEW

South Jordan City has a number of documents and ordinances related to the design and maintenance of their streetscapes and park strips.

2.1 SOUTH JORDAN GENERAL PLAN (2020)

The South Jordan General Plan outlines numerous goals and strategies, several of which are directly or indirectly related to this Streetscape and Park strip Master Plan.

Gather Goal 1: Enhance the public spaces in and around City destinations

1.4 Improve neighborhood identity by encouraging local names for the identification of parks, trails, community facilities, etc. and by using tree species and other urban design features to help distinguish areas of the City

This goal recommends using tree species and other features to highlight different areas of the city.

Gather Goal 4: Encourage the presence of local character and sense of place

4.2 Create a palette of design elements that reflect the City's character to use in redevelopment of key gateway properties

4.3 Consider the following locations for major City

gateways with appropriate signage and landscaping: I-15 at 10000 South, 10600 South, and 11400 South; Bangerter Highway at 9400 South and 11800 South; U-111 at 10200 South and 11800 South, major "entrance" intersections on Redwood Road, Bangerter Highway and Mountain View Corridor; and the Mid-Valley Trax Line entrance to the City at 10200 South

4.4 Consider the following locations for minimal entry signage: 1300 West at 9400 South and 11400 South; 9400 South at 2200, 2700, 3200 and 4000 West; 11800 South at 2700 West [*? typo in general plan*], and 3600 West

4.5 Consider upgrading the following intersections with urban design and landscape features: South Jordan Parkway at Jordan Gateway, 1300 West, Redwood Road, and Mountain View Corridor; 11400 South at Jordan Gateway, 1300 West and Redwood Road; Bangerter Highway at 9800 South, 10400 South and 11400 South; Mountain View Corridor; and 11800 South

4.6 Update the City's Streetscape Plan for major streets within the City and re-adopt the Plan as a reference document that can be updated as tree species types, planting designs and methods evolve; include park strip design, tree planting for development plans and



Figure 2.01 - South Jordan's 2020 General Plan

capital improvement plans, and re-assessment of fencing requirements on local and collector streets

Gather goals 4.2 - 4.5 recommend the creation of a palette of design elements, signage and landscape features for the redevelopment and enhancement of key City gateways and various arterial and collector intersections.

Gather goal 4.6 specifically calls for an update to the City's Streetscape Plan.

Grow Goal 3: Promote energy efficient elements in new and existing development

3.4 Help minimize the urban heat island effect by encouraging lighter color building materials for roof-tops, parking lots, roadways, pathways, planting trees that provide shade, etc.

Well designed streetscapes and park strips can help minimize the urban heat island effect through effective use of street trees and lighter materials.

Grow Goal 5: Reduce waste and excessive water use within the City

5.2 Consider and encourage water saving techniques (i.e. rain water harvesting, re-use of grey water, installing low-flow fixtures)

5.3 Implement rain gardens and other storm water collection and retention systems

5.4 Continue to develop and implement Low Impact Development (LID) standards to improve soil permeability and to avoid costly storm drainage systems

5.7 Maintain an updated water conservation plan

These Grow Goals all promote the reduction of water use, which relates to this Master Plan through the design of more water efficient landscapes within the park strips and streetscapes.

2.2 SOUTH JORDAN SUB AREA PLAN (2021)

This plan, intended to be used together with the general plan, takes a closer look at two sub areas, the Redwood Road Corridor and the Jordan River Gateway. These unique areas were chosen because they are seen as entrances to the city, major transportation corridors, and economic hubs.

For Redwood Road, the plan calls for a focus on the intersections at the South Jordan Towne Center, but also acknowledges that the City has limited control over the street since it is owned by UDOT. Some goals and guidelines of the Sub Area Plan related to this Master Plan include:

GOALS FOR THE REDWOOD ROAD CORRIDOR

Goal 1: Brand Redwood Road area as a corridor

1.1 Install gateway elements at north and south terminus of Redwood Road

1.2 Install unique, consistent accents to street lights and wayfinding in the right-of-way, encourage interactive public art, unique furnishings, and other

valuable placemaking elements in developments along Redwood Road

Goal 4: Promote consistent forms and design elements

4.1 Require all new development or redevelopment to install streetscape elements per the Redwood Road Design Guidelines

4.2 Upgrade major intersections as new development occurs with urban design and gateway features per the Redwood Road Design Guidelines

Develop a palette of acceptable site furnishings

REDWOOD ROAD DESIGN GUIDELINES

Sidewalks & Park Strips

Sidewalks along Redwood Road should have a minimum of 8 feet for a clear walkway

Park strips separating sidewalks from Redwood Road should be 12 feet wide from the back of curb to the sidewalk edge

Park strips should include at least 50% planted or living materials

Plants in park strips should be grouped by hydrozone and use drip irrigation systems to minimize water usage

Park strips should include trees and raised planters to further separate pedestrians from the street

Shade-type street trees should be installed a minimum of every 40 feet in park strips along Redwood Road

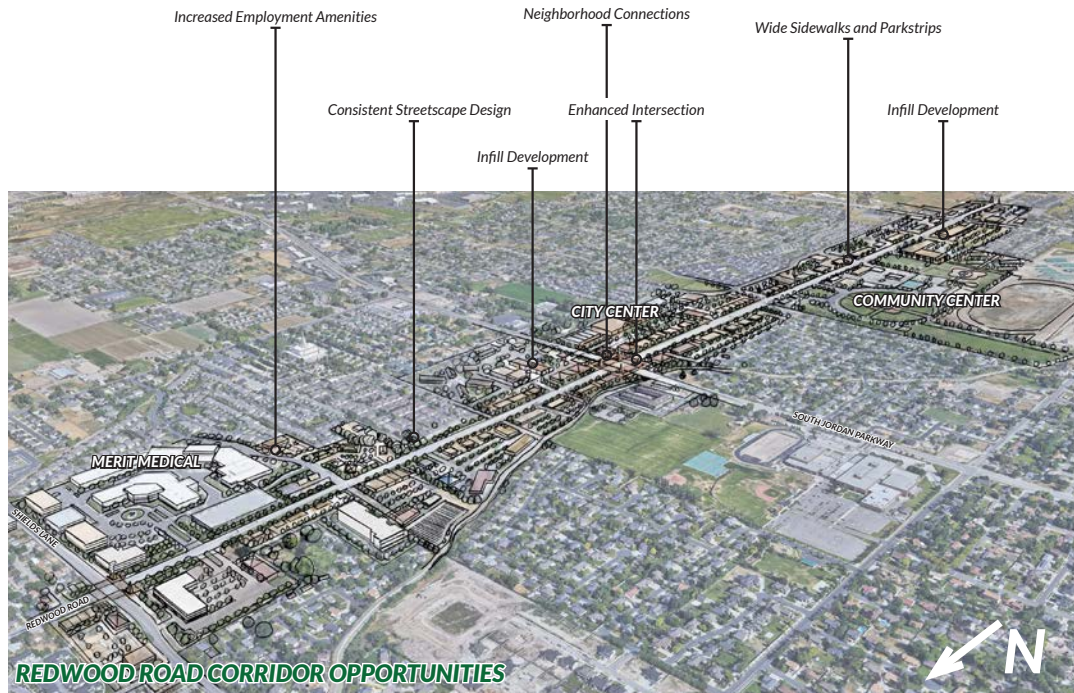


Figure 2.02 - Redwood Road Corridor Opportunities from the South Jordan Sub Area Plan

Site Furnishings

Benches and trash and recycling receptacles should be placed near pedestrian crossings and at transit stops, not to exceed 800 feet between groupings of furnishings

Developments along Redwood Road should provide bike storage for employees, visitors, and residents

Developments along Redwood Road should strive to provide unique and interactive furnishings to compliment district and gateway elements similar to those pictured.

Intersections

Corners at each intersection on Redwood Road should include specialty paving, unique furnishings, and/or unique landscaping

Crosswalks intersecting Redwood Road should be a minimum of 8 feet wide

Where crosswalks stretch across 6 or more lanes, a pedestrian refuge island should be installed in Redwood Road.

These goals and guidelines will require coordination with the South Jordan Streetscape and Park strip Master Plan.

2.3 WATER CONSERVATION PLAN UPDATE (2020)

This update outlines South Jordan City's current water conservation efforts and presents its current conservation goals. In relation to this Streetscape and Park strip Master Plan, the Water Conservation Plan notes that the City has implemented a Residential Drip Conversion and Rock Mulch Program for residential park strip conversions. These programs promote the replacement of a typical lawn park strip to more water-wise plantings and rock mulch. The city can and should promote these programs by doing the same for their city maintained park strips and medians.

The Water Conservation Plan also notes that the city adopted an ordinance for water efficient landscaping on June 18, 2002. The goal of this ordinance is to provide policies for commercial, industrial, multi-family and single family residential developments. The City's ordinance is found under South Jordan City Code: Chapter 16.30 Water efficiency standards.

2.4 CITY CODE: CHAPTER 16.30 WATER EFFICIENCY STANDARDS (2021)

Amended in 2021, this chapter of South Jordan City's code notes that the city desires to promote the design, installation and maintenance of attractive and water

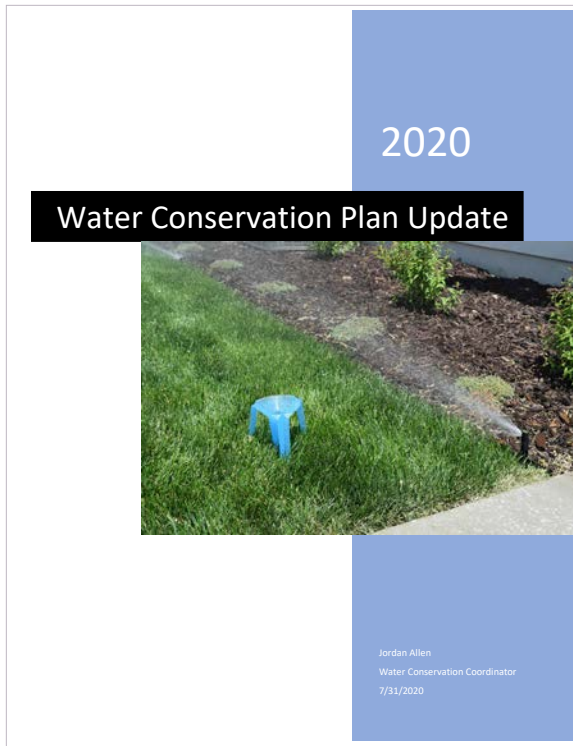


Figure 2.03 - South Jordan's 2020 Water Conservation Plan Update

efficient landscapes, and that it is in the public interest to conserve the public water resources and to promote standards for efficient water use and landscaping by establishing water conservation standards. For all new or rehabilitated landscapes, requirements related to park strips include:

1. Appropriate irrigation for the designated plant material; the use of drip irrigation or bubblers except in lawn areas.
2. Provision of a Watersense labeled smart irrigation controller that responds to changing weather
3. At least 3-4" of mulch in planting beds
4. Minimum 50% vegetation cover at maturity
5. Lawn shall not be installed in park strips

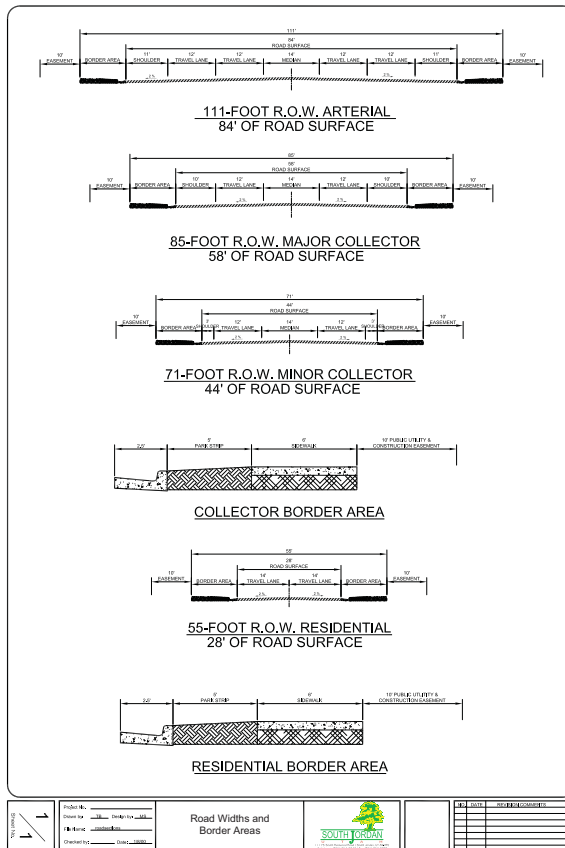
2.5 CITY CODE: CHAPTER 16.04.190 PARKS, PARK STRIPS, WALKWAYS, TRAILS, AND OPEN SPACE

Part E of this chapter outlines requirements for Park strips along both Collectors / Arterials and Residential Streets. Requirements include:

1. Park strips shall be planted with a combination of trees, plants, shrubs or other live vegetation which are low maintenance and commonly found along the Wasatch Front or within similar climates.
2. Lawn shall not be installed in park strips.

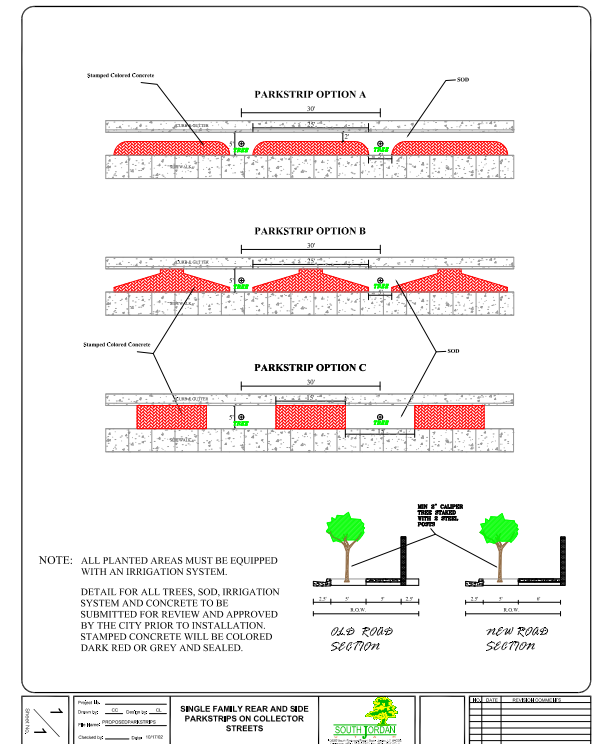
3. Live vegetation will cover a minimum of fifty percent (50%) of the park strip area upon landscape maturity
4. Mulch made of bark or rock material that is one and one-half inches (1.5") or larger may be used in combination with live vegetation, provided that it is fully contained within the park strip at all times.
5. For Collector Streets, up to fifty percent (50%) of the park strip area, per street frontage, may be hardscape, with stamped natural earth tone colored concrete. For Residential Streets, stamped natural earth tone colored concrete or masonry materials such as pavers may cover a maximum of twenty-five percent (25%) of the park strip area per street frontage; for either area, this paving shall not be used in sections greater than fifteen (15) linear feet per section (see figure 2.4)
6. Landscaping And Maintenance:
 - (a) After satisfactory installation of landscaping in collector street park strips..., the City will accept responsibility for maintenance of the park strips along collector streets where collector street fencing has been installed along the rear and side property lines of lots only in single-family residential subdivisions.

Both chapter 16.04 and 16.30 stipulate that lawn shall not be installed in park strips. Also, the city has stipulated that it will accept responsibility for the maintenance of park strips on the side or rear of single family subdivisions.



2.6 SOUTH JORDAN ENGINEERING STANDARD DRAWINGS (VARIOUS DATES)

2.7 2200 WEST LANDSCAPING PROJECT (2022)



2.8 PARK STRIP TREE SPECIES LIST FOR SOUTH JORDAN CITY

3.0 MAINTENANCE RESPONSIBILITY

The city currently maintains park strips and medians that should be maintained by another entity. One way to reduce use of the city's maintenance resources is to transfer maintenance activities to the responsible party. This plan investigates the feasibility of turning over maintenance to private property owners for specific park strips.

3.1 IDENTIFY CURRENT PARK STRIP & MEDIAN MAINTENANCE

The city's GIS database includes park strip and median shape files and a maintenance attribute for each shape. It is assumed that this data is accurate.

GIS data on all park strip and medians was obtained from the city and analyzed to identify a superset of park strips and medians that the city is currently maintaining or should be maintaining. This superset consists of all park strip and median shapes with maintenance attribute values of "SJC" and "unknown." No other maintenance attribute values were included in the superset.

3.2 DETERMINE MAINTENANCE RESPONSIBILITY

Several criteria were applied to the superset in order to differentiate between park strips and medians that are to be maintained by the city and those that are not.

MAINTENANCE CRITERIA

City Code

Two portions of the city code define park strip and median maintenance responsibilities. The city code language is paraphrased here.

1. 12.04.090: Maintenance of Park Strips

Park strip planting and maintenance is the responsibility of the abutting property owner.

2. 16.04.190 E.4: Landscaping and Maintenance

- (a) The city will assume maintenance responsibility for park strips that meet all of the following criteria:
 - (i) have fully installed landscaping
 - (ii) have 100% release of the improvement guarantee for the installed landscaping
 - (iii) have required fencing installed
 - (iv) are along rear or side property lines which are along a collector or arterial street

- (v) are in a single-family subdivision
- (b) Property owners in all other developments are responsible for landscaping and maintenance of park strips abutting their property.

Additional Criteria

1. The city is required to maintain park strips along **all UDOT roads**. Four UDOT roads, or State Routes, pass through South Jordan:
 - (a) State Route 154 - Bangerter Highway. As a high capacity road, Bangerter Highway does not have any park strips or medians to maintain.
 - (b) State Route 68 - Redwood Road
 - (c) State Route 151 - South Jordan Parkway: from Bangerter Highway to the city's eastern boundary
 - (d) State Route 175 - 11400 South: from Bangerter Highway to the city's eastern boundary
2. The city is required to maintain park strips that **abut all city and county owned property**.

APPLY CRITERIA TO GIS SUPERSET

These criteria were applied to the superset of park strip and median shapes in the GIS database. This created **two subsets of park strip and median shape files**: those maintained by the city and those not maintained by the city.

The subset not maintained by the city includes park strips along local or private roads, along front property lines of a single-family subdivision, abutting multi-family developments, commercial, school, or church properties.

Some shape files spanned abutting properties that had different maintenance requirements and were subdivided into multiple shapes.

Any special maintenance agreements that may exist between the city and other property owners are not included in the new GIS data and maps. It is recommended that South Jordan personnel review and validate the updated maintenance attributes and shape files.

3.3 UPDATED MAINTENANCE MAP & TABLE

MAINTENANCE MAPS

The following two maps show all of the park strips and medians in the superset of GIS shape files. Shapes shown in green are the subset of park strips and medians that are the city's responsibility to maintain. The red park strips and medians are the subset that is to be maintained by the owner of the abutting property.

A few park strip shapes are shown in yellow. The maintenance responsibility for these are undetermined and need to be reviewed by city personnel.




MAINTENANCE TABLE

The table in Appendix B lists the shape file data for the subset of park strips that the city should not maintain; the same shapes that are shown in red on the maps.

Complete GIS files with the updated maintenance data subsets will be provided to the city.

South Jordan City Parkstrip Maintenance Map

Legend

-  Municipal Boundary
-  Abutting Property Owner Maintained
-  South Jordan City Maintained

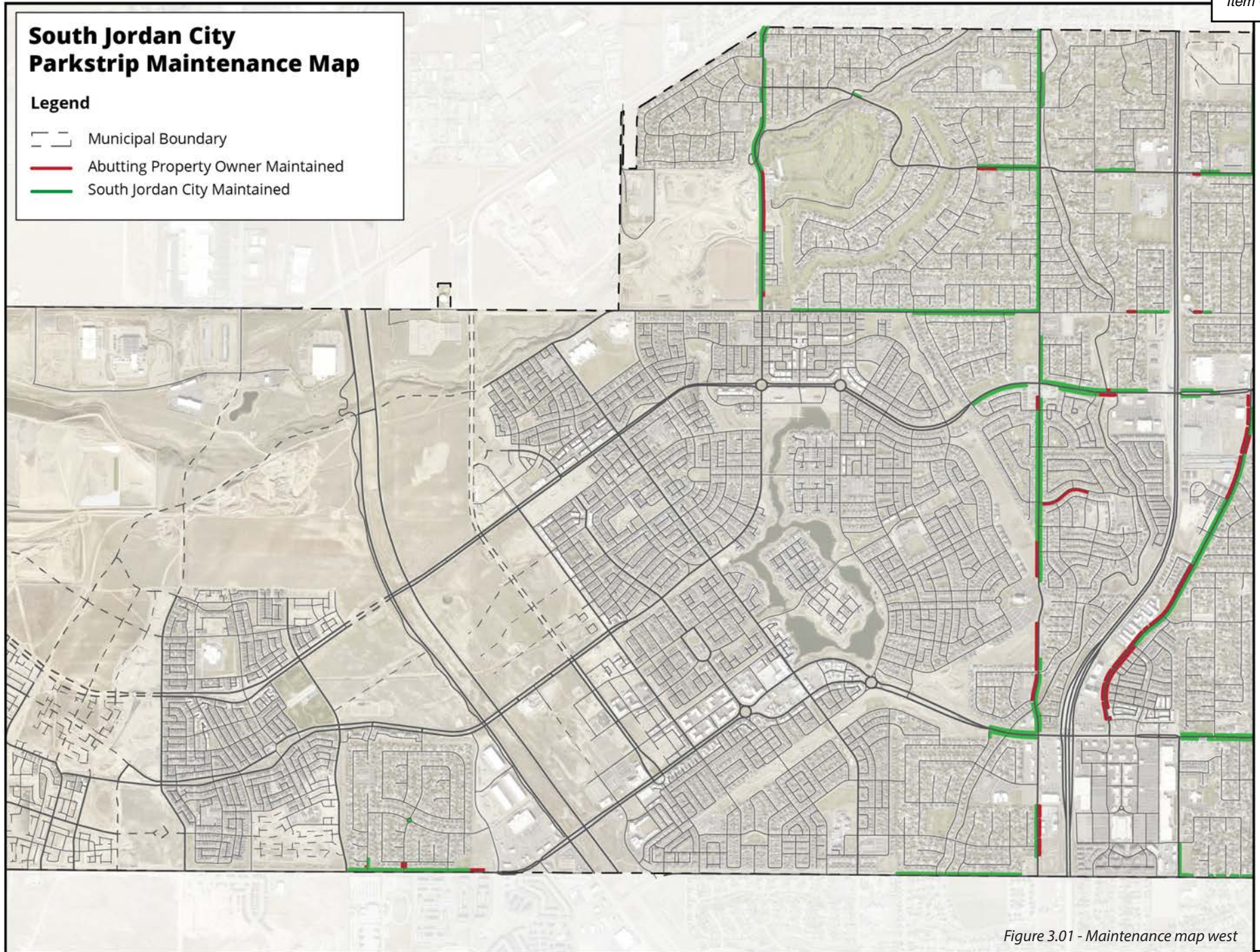


Figure 3.01 - Maintenance map west

South Jordan City Parkstrip Maintenance Map

Legend

- Municipal Boundary
- Abutting Property Owner Maintained
- South Jordan City Maintained
- South Jordan City to Verify

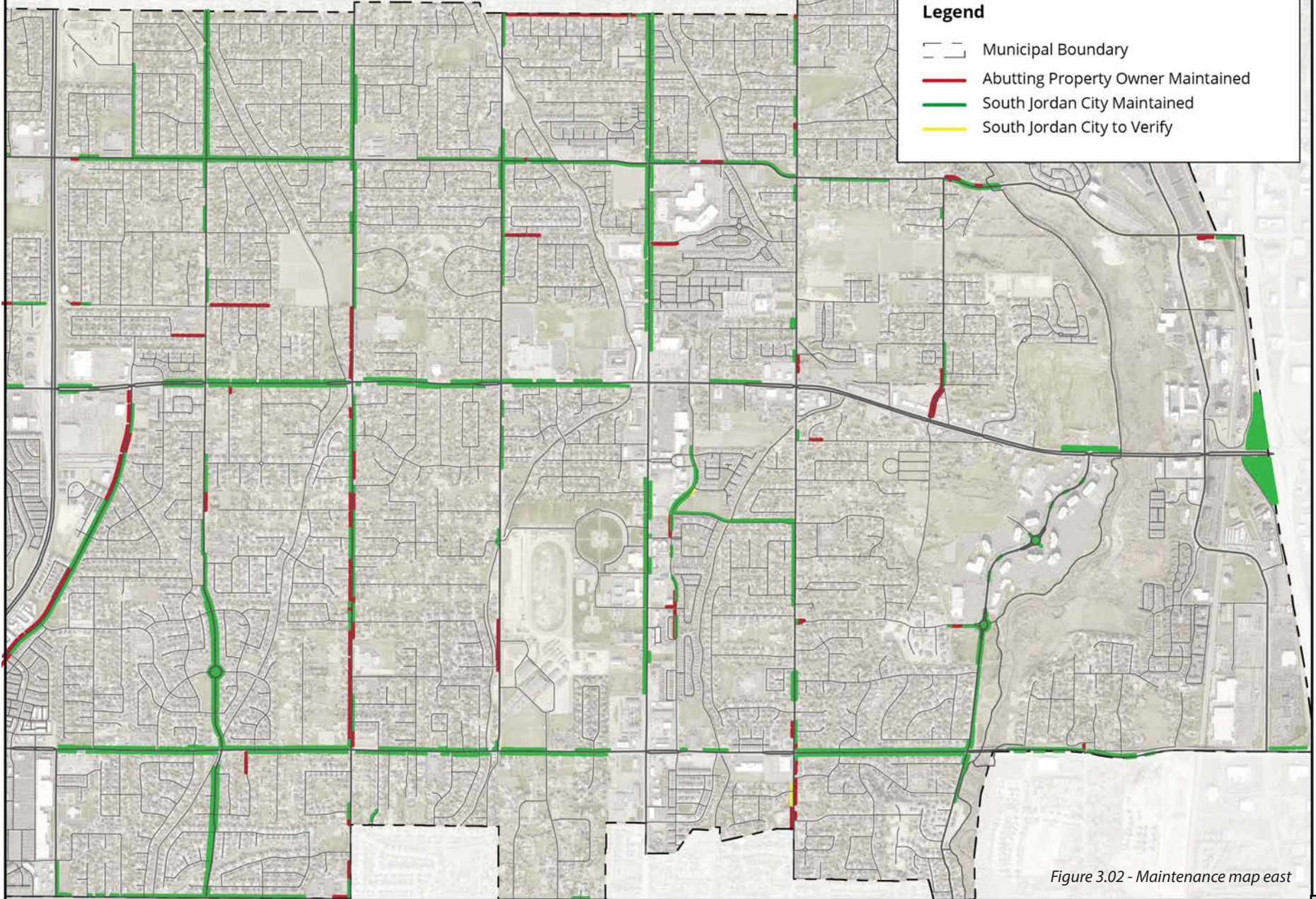


Figure 3.02 - Maintenance map east

4.0 IMPLEMENTATION PROCESS



Figure 4.01 - Parkstrip example for the Residential Drip Conversion and Rock Mulch Program

4.1 GOALS & OBJECTIVES

Implementation of this streetscape master plan will happen over a course of several years as budget and staff time allow.

The intent of this process is to create a clear and easily replicable system for updating segments of the city's streetscapes.

The primary intent is to accomplish the goals established during this process:

Goal 1: Reduce water demands for park strips and medians maintained by South Jordan City.

Objective 1: Implement installation of low water plants throughout all of the city maintained park strips and medians.

Objective 2: Install more efficient and controllable irrigation systems for waste reduction and adjustability during times of drought.

Goal 2: Reduce maintenance needs for city-maintained park strips and medians.

Objective 1: Simplify plant maintenance for park strips and medians to seasonal or annual maintenance tasks.

Objective 2: Eliminate turf grass completely from all city maintained park strips and medians.

Goal 3: Create a lush and attractive landscape along South Jordan's key travel corridors.

Objective 1: Focus city plant installation on a short list of low-water and low-maintenance plants that thrive in our climate.

Objective 2: Create an aesthetic of climate sensitive but lush and attractive plants throughout the city.

This chapter is intended to create a simple and implementable approach to the refurbishment of South Jordan's city maintained park strip and medians.

These design guidelines aim to create a simple but strategic approach to updates that the city can apply to the many various conditions throughout the park strips and medians, as well as apply to new development.

Using the goals in this section as a guide, the aim of these guidelines is to create a 10-year plan for how the city should look and be maintained in some of the key corridors of South Jordan.

Additions and revisions to plant lists are to be expected, but the intent is to keep a short list of easily maintained plants that will add to the city's character, but also recognize the limitations we face in our region related to water and climate.



Figure 4.02 - Park strip refurbishment process

4.2 PROCESS

The park strip refurbishment process has 7 steps:

1. Site Analysis
2. Site Design
3. Site Preparation
4. Install Irrigation
5. Install Plants
6. Establish Plants
7. Maintenance

See Figure 4.02

The steps are listed in their chronological order but the duration of each step will vary. Steps 1 and 2 can be completed well in advance of step 3. Some tasks in steps 3, 4, and 5 will overlap and can be performed at the same time. Step 6 should take 1-2 years, and step 7 continues indefinitely.

This process should be used individually for each street corridor/section.

STEP 1: SITE ANALYSIS

The site analysis both records existing conditions and determines which conditions are to be changed and which are to remain. The site analysis shall be documented on site drawings.

Conditions that might be removed include existing turf, pavement, mulch, dead trees, weeds, irrigation systems, etc.

The following principles shall guide the site analysis:

1. Always consider the specific goals for the given street/corridor.
2. Identify which existing irrigation systems are to be preserved and adapted to the new irrigation scheme.
3. Evaluate any of the previously permitted 15'-0" long paved areas and identify which to keep or remove on a case-by-case basis.
4. Identify which trees are to remain and where trees need to be added or replaced. Preserve existing trees if they are healthy.
5. Identify existing utility poles, lamp posts, street signs, utility vaults, electrical boxes, and other features that will remain in the park strip.

STEP 2: SITE DESIGN

The site design step uses the site analysis drawings to generate site plans. This includes the irrigation system and planting design drawings. The site design should meet the specific goals for the given street/corridor.

The following principles shall guide the site design:

Irrigation Systems

Updated irrigation systems will reduce water use and maintenance while improving plant health. The new irrigation system will have two irrigation zones in each park strip, use smart controllers, and use a consolidated valve box/controller configuration.

Irrigation system plan drawings should be developed on a project basis and meet the following criteria:

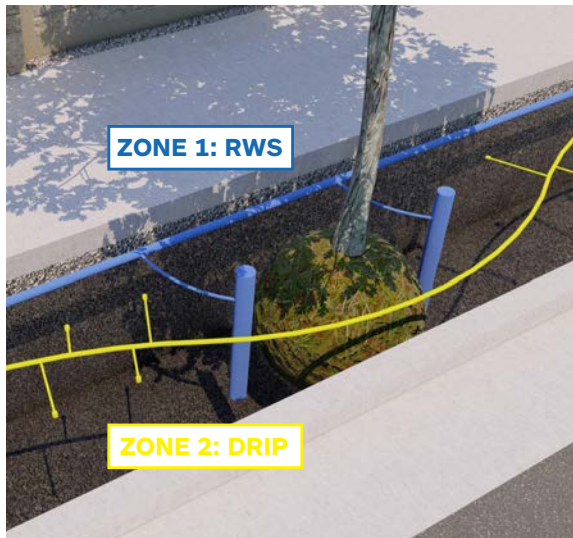


Figure 4.03 - Separate tree and plant irrigation zones

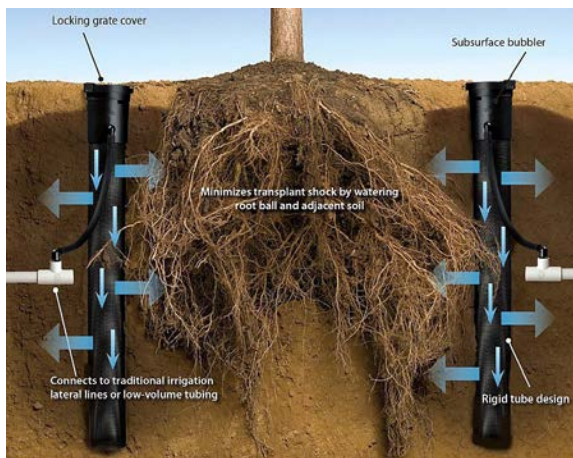


Figure 4.04 - Root watering system diagram

1. Two irrigation zones allow plants and trees to be irrigated independently. This feature is used to customize the amount of water provided for the plants vs the trees. It is useful in establishing new plants and prevents overwatering when the plants and trees have different water demands. It is also useful in drought conditions where trees, the larger investment of time and money, might need to be prioritized over plants.

(a) The tree irrigation zone will use a **Root Watering System (RWS)** to provide water directly to the tree's root system and minimizes surface runoff and evaporation loss. The RWS will be supplied by valves and lateral lines separate from the plant irrigation zones. See Figures 4.03 and 4.04

(i) Each tree shall have a minimum of 2 RWS fixtures evenly spaced around the rootball.

(ii) Each RWS fixture shall be fitted with a cloth sock around its exterior to prevent fine sediments from penetrating and clogging the canister.

(iii) Each RWS canister shall be filled with 0.5" gravel to improve top to bottom water dispersion and prevent canister collapse from side pressure.

(b) The plant irrigation zone will use a drip emitter system. Individual emitters will be placed at each plant and connected to the plant irrigation zone later lines and valves. See Figure 4.03

2. Smart controllers will be installed on both existing and new irrigation systems. These controllers reduce maintenance time and costs because adjustments to watering schedules can be made remotely. This convenience can help reduce water use and increase plant survival by making real-time adjustments based on changing conditions.

(a) Rain Bird brand smart controllers will be installed on all new irrigation systems and retrofitted to existing systems as they are refurbished.

3. Consolidated control systems will minimize the number of controllers and valve boxes. The quantity and location of controllers and valve boxes shall be determined on a project basis with the goal of reducing the number of controllers overall.

Planting Design

Planting design is governed by an algorithm, or decision making structure. Figure 4.05 shows an overview of the process.

The full algorithm and planting design process are described in the next chapter.

STEP 3: SITE PREPARATION

Site preparation includes all site work that needs to be completed before irrigation systems and plants can be installed. This includes the removal or modification of any site features as identified in the site analysis and site design drawings.

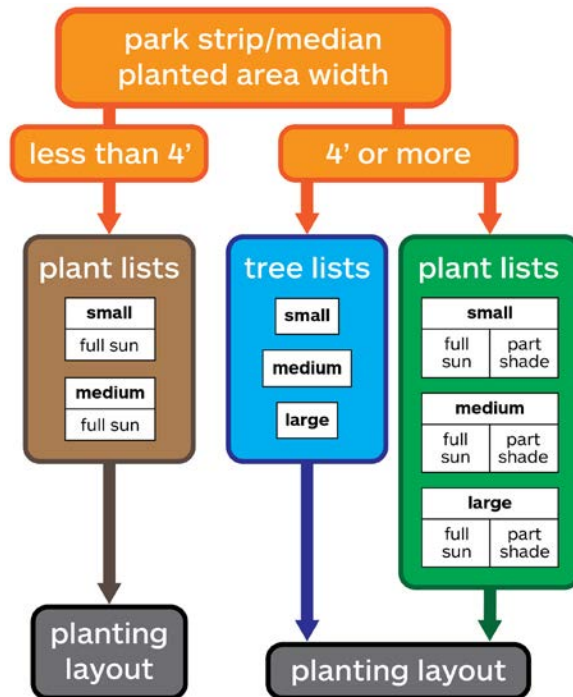


Figure 4.05 - The planting design process is based on the planted area width of each park strip or median

Site preparation activities may coincide with tasks in Step 4 and Step 5 and may be conducted simultaneously to reduce construction time and costs.

1. Existing trees that are to remain shall be protected and preserved during site preparation.

STEP 4: INSTALL IRRIGATION

The two irrigation zones shall be installed per the irrigation plan developed in step 2. Some tasks in this step such as controller, valve box, lateral line, and root barrier installation will be completed before plants are installed.

1. Existing trees that are to remain shall be protected and preserved during irrigation installation.
2. A **30 mil commercial grade weed barrier** shall be installed to cover all exposed soil in all park strips and medians

STEP 5: PREPARE FOR & INSTALL PLANTS

Trees and plants shall be installed per the planting plan developed in step 2. Some irrigation installation tasks such as RWS and drip emitter installation will be completed simultaneously or after plants are installed.

1. Install trees and plants as shown in Figure 5.10
2. Install rock mulch over all weed barrier areas and to cover drip emitters and lines.
 - (a) 1.5" to 2" stone size
 - (b) Neutral color (grey or tan)
 - (c) 3" minimum depth

STEP 6: ESTABLISH PLANTS

Newly installed plants and trees require special care to help them survive the transplant process and establish a new root system. A well-established plant or tree will require less water in the long term.

1. Frequent irrigation in the first year
2. Frequent inspection in the first year

STEP 7: MAINTENANCE

After the plants are established, water use and maintenance is reduced. Ongoing maintenance tasks include:

1. Annual plant evaluation
2. Seasonal maintenance tasks
3. Inspection frequency
4. Controller adjustments
5. Winterizing of irrigation systems

5.0 PLANTING DESIGN PROCESS

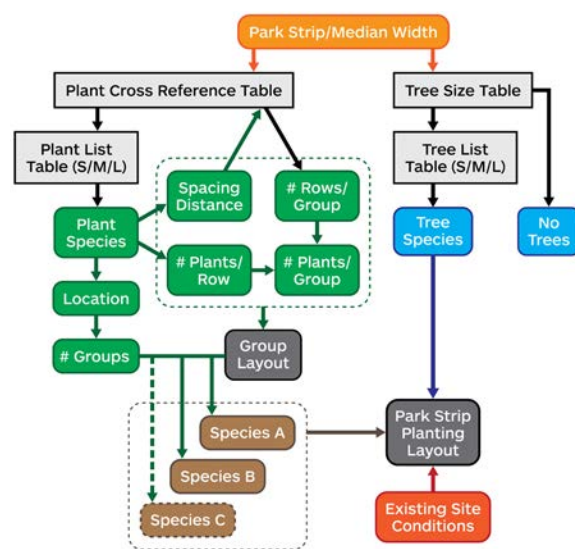


Figure 5.01 - Planting design process flow chart

The planting design process is a tool to enable South Jordan City personnel the flexibility to design their own streetscape planting plans that are customized to the goals for each street/corridor. The species selection and planting layouts generated by the algorithm will develop a planted streetscape that maximizes plant health and resource efficiency.

The planting layouts are designed to achieve 50% coverage of the planted areas at plant maturity. This coverage is concentrated along the center line of the park strips and medians, leaving the side edges less covered. This creates a longer linear layout that accentuates the length of the ornamental planting as it is experienced by people in passing cars.

See Figure 5.01 for a flow chart of the planting design process.

5.1 PLANTING LAYOUT DESIGN

ALGORITHM

The planting layout design process is governed by a simple **algorithm that uses tables and a worksheet**. An example diagram for how to use the algorithm is shown in Appendix C.

Inputs

The algorithm process has **three inputs**: the street, site type, and planted area width. These inputs are written on the top of each worksheet.

1. The **Street** indicates the section of a street to which the planting layout of the given worksheet is applied. The length of this section is used to determine the number of planting modules.
2. **Site type** is either a park strip or median. The presence of overhead power lines is indicated in the site type.
3. **Planted area width** refers to the planted areas in both park strips and medians. It is defined as the actual width of the soil area (measured perpendicular to the street) and does not include the width of any adjacent curbs, gutters, or pavement.
 - (a) The planted area width of 9'-0" for medians is used in reference to South Jordan Engineering Standards Drawing S-1 where the standard 14'-0" median has a planted area width of 9'-0".
 - (b) **Park strip and median widths will vary.** Common widths are included in the tables. When a planted area width is between two listed widths, use the smaller width.

Park Strip Planting Design Algorithm Worksheet
10-Jan-2023 DRAFT

Street _____
From _____ to _____ Linear Feet _____

1 Site Type: park strip / median 2 Overhead Power Lines: Y / N 3 Planted Area Width: _____
4 Planting Module Size: single / double 5 Number of Modules: _____

30'-0" single module 60'-0" double module

Notes:
1) Adjust tree spacing to onsite conditions existing trees, utility structures, driveways, etc.
2) Where no tree is required, combine both "under tree" plant groups into one group.

1, 2, and 3 row group layouts

Tree
6 Tree List: S / M / L / none 7 Tree Species: _____

Plants
Plant Species A
8 Plant List: S / M / L 9 Plant Species: _____
10 Spacing: _____ 11 Rows/Group: _____ 12 Plants/Row: _____ 13 Location: under tree / between trees

Plant Species B
14 Plant List: S / M / L 15 Plant Species: _____
16 Spacing: _____ 17 Rows/Group: _____ 18 Plants/Row: _____ 19 Location: under tree / between trees

Plant Species C
20 Plant List: S / M / L 21 Plant Species: _____
22 Spacing: _____ 23 Rows/Group: _____ 24 Plants/Row: _____ 25 Location: under tree / between trees

Figure 5.02 - Planting design algorithm worksheet. See Appendix A for the full size printable version

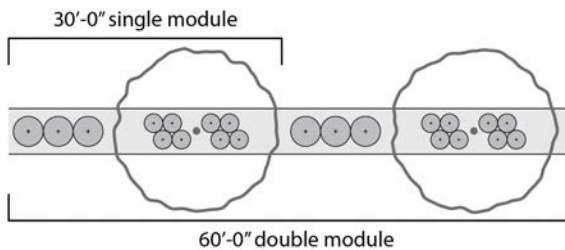


Figure 5.03 - Single and double module sizes

Output

The **output** of the algorithm is a **worksheet** that identifies the species, quantity, and location of each plant and tree for one planting module. See Figure 5.02

PLANTING LAYOUT

The configuration of tree and plant locations within the park strip or median is organized into planting modules. Each module consists of plant groups and tree(s).

Planting Modules

Each worksheet describes one **planting module**. A planting module is the plant layout unit that is repeated along both sides of a street. This repetition unifies the character of the street and reinforces the consistent identity of a corridor.

Planting modules are based on tree spacing and two module sizes are possible. See Figure 5.03

1. **Single modules** are 30'-0" long with 1 tree and 3 groups of 2 plant species.
2. **Double modules** are 60'-0" long with 2 trees and 6 groups of 3 plant species.

Single modules are better suited to shorter sections of streetscape and street frontages that are frequently interrupted by driveways or other non-planted uses of the park strip. Double modules are better suited to longer sections of streetscape and less interrupted street frontages.

After the module size is chosen, the number of modules can be calculated based on the length of the street

indicated on the worksheet. This can be roughly calculated based on linear feet of the street but a finer calculation that includes park strip interruptions, like driveways, will need to be made.

Plant groups

Each plant group within a module contains a single plant species. The species, size, and location of each plant group is determined by the planting design algorithm.

5.2 PLANTING DESIGN ALGORITHM STEPS

The steps of the algorithm are designed to provide the information to fill in the planting design worksheet.

STREET/CORRIDOR INFORMATION

Street name, range, and linear footage

SITE DESCRIPTION

1. Site type
2. Overhead power lines (Y or N)
3. Planted area width
4. Planting module size
5. Number of planting modules

TREE SPECIES SELECTION

Step 1: Tree List Selection

Tree list selection begins with Table 5.01 - Tree List by Planted Area Width. Using the planted area width,

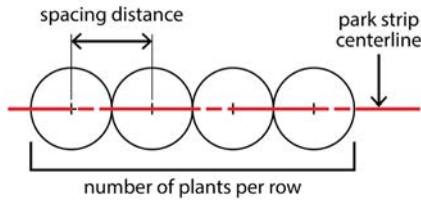


Figure 5.04 - One row plant group layout

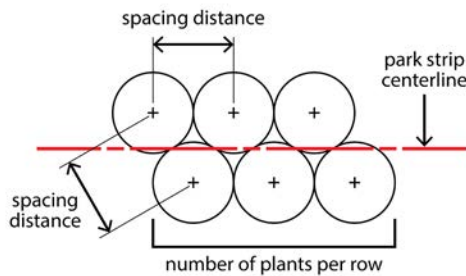


Figure 5.05 - Two row plant group layout

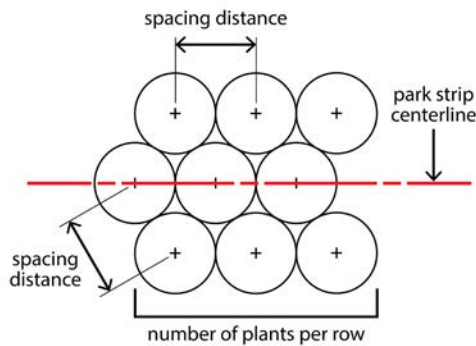


Figure 5.06 - Three row plant group layout

determine which tree list to use. Tree lists are found in Tables 5.03, 5.04, and 5.05

For all planted areas with overhead power lines, and all medians, use the small trees list. Park strips that are less than 4'-0" wide shall not include any trees.

Circle the tree list or no trees option in line 6 on the worksheet.

Step 2: Tree Species Selection

Using the tree list identified in step 1, select one tree species. Choose a species that will help define the character of the given street. Write this tree species on line 7 of the worksheet.

Note: Tree species selection can be made or adjusted alongside plant species selection in order to achieve a coordinated aesthetic.

PLANT SPECIES SELECTION

Single planting modules require 2 plant species. Double planting modules require 3 plant species. Repeat the plant species selection steps for each species in the planting module.

Step 1: Plant List Selection

Plant list selection begins with Table 5.02 - Plant Cross Reference by Planted Area Width. This table contains 3 variables for each plant species and needs to be cross referenced with each selected plant list table to fill in the worksheet. Plant lists are found in Tables 5.06, 5.07, and 5.08

Using the planted area width, determine which plant list Table(s) to use:

- Locate the column with the correct planted area width.
- In cells where the Number of Rows per Plant Group has a numeric value, the corresponding plant list Table(s) in the far left column may be used in the planting module.

Note: Plant list(s) corresponding to cells with an n/a value shall not be used.

Circle the plant list on line 8 in the worksheet.

Step 2: Plant Species Selection

This step requires multiple cross references between Table 5.02 and the plant list table for each species.

- Using the plant list Table(s) identified in step 1, select 1 plant species. Identify the plant spacing for that species and cross reference that spacing value back to the plant spacing value in Table 5.02. Only use plant species that correspond to the spacing values in both the plant species table and Table 5.02. Write the plant species name on line 9 of the worksheet.
- Write the plant spacing value on line 10.
- From Table 5.02, write the number of rows per group on line 11.
- From the selected plant list table, write the number of plants per row on line 12.

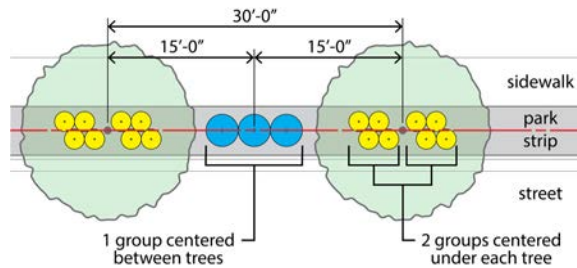


Figure 5.07 - Plant group locations

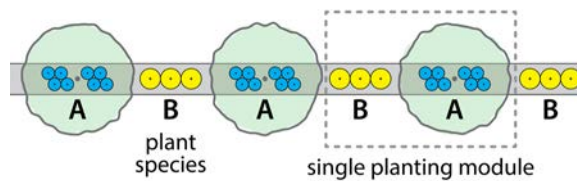


Figure 5.08 - Single planting module has 2 plant species

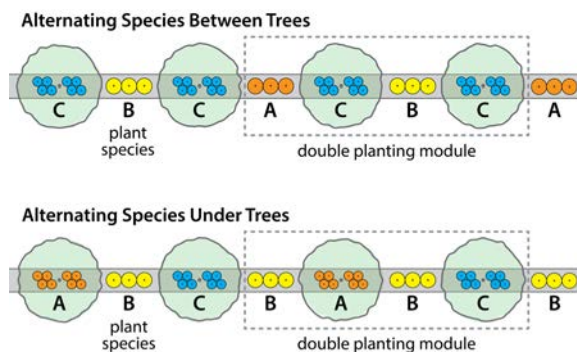


Figure 5.09 - Double planting modules have 3 plant species and 2 alternating species layout options

E. From the selected plant list table, circle the location for the plant species group on line 13. Make sure that the selected location matches each species.

Once each plant species is selected, fill in the boxes in the planting module diagram with the letter of each species.

Double modules will have alternating species. There are two options for alternating species layouts in double modules. See Figure 5.09

5.3 PLANTING DESIGN WORKSHEET

When the planting design worksheet is complete it is combined with the site analysis information to create the planting plan. This will take into account all existing structures in the planting area of each park strip or median. The final number of modules is used to calculate the number of each plant and tree species needed for the given street/corridor length.

The planting design algorithm process can be repeated in order to change plant species, module size, etc. A new worksheet will be prepared each time the process is updated.

5.4 PLANTING DESIGN TABLES, LISTS, AND EXAMPLES

The remainder of this section includes all of the tables for completing the planting design algorithm. Visual plant lists are included and examples of how different park strip and medians can look after the new landscaping is installed.

Table 5.01 - Tree List by Planted Area Width	
Any planted area with overhead power lines	Small
PLANTED AREA WIDTH	TREE LIST
<4'-0"	n/a
4'-0" to 4'-11"	Small
5'-0" to 6'-11"	Medium
7'-0" and larger	Large
9'-0" Median	Small

Table 5.02 - Plant Cross Reference by Planted Area Width							
PLANT LIST	PLANT SPACING	PLANTED AREA WIDTH					MEDIAN
		<3'-0"	3'-0"	4'-0"	5'-0"	8'-0"	9'-0"
		NUMBER OF ROWS PER PLANT GROUP					
Small	1'-6"	1	2	2	3	4	5
	2'-0"	1	1	2	2	3	4
Medium	3'-0"	n/a	n/a	1	1	2	2
Large	4'-0"	n/a	n/a	n/a	1	1	2
	5'-0"	n/a	n/a	n/a	n/a	1	1
	6'-0"	n/a	n/a	n/a	n/a	1	1

TREE LIST TABLES

Table 5.03 - Small Trees List		
BOTANICAL NAME	COMMON NAME	SIZE
Acer ginnala 'Flame'	Flame Amur Maple	6-9'
Malus 'Spring Snow'	Spring Snow Flowering Crabapple	2" cal.
Syringa reticulata	Japanese Tree Lilac	2" cal.
Zelkova serrata 'Wireless'	Wireless Zelkova	2" cal.

Table 5.04 - Medium Trees List		
BOTANICAL NAME	COMMON NAME	SIZE
Carpinus betulus 'Fastigiata'	Columnar Hornbeam	2" cal.
Celtis occidentalis	Common Hackberry	2" cal.
Gleditsia triacanthos var. inermis	Honeylocust	2" cal.
Koelreuteria paniculata	Golden Raintree	2" cal.

Table 5.05 - Large Trees List		
BOTANICAL NAME	COMMON NAME	SIZE
Corylus columna	Turkish Filbert	2" cal.
Gymnocladus dioica 'Espresso'	Kentucky Coffee Tree	2" cal.
Quercus shumardii	Shumard Oak	2" cal.
Tilia tomentosa	Silver Linden	2" cal.
Ulmus x 'Morton'	Accolade Elm	2" cal.
Zelkova serrata 'Village Green'	Village Green Zelkova	2" cal.

SMALL TREES VISUAL LIST



Acer ginnala 'Flame'
FLAME AMUR MAPLE

(Z2) A small rounded tree or large shrub. Green summer foliage turns bright red in the fall. Flowers are very fragrant, appearing in the early spring. Adapts well to a wide range of soils. Grown in a tree or multi-stem form.

Height 20' Spread 20'



Malus 'Spring Snow'
SPRING SNOW CRABAPPLE

(Z4) Full sun.

Profuse white flowers in spring are followed by medium green foliage. Yellow fall color. Fruitless.

H 25' W 22'



Syringa reticulata
JAPANESE TREE LILAC

(Z3) Full sun, waterwise, deer resistant, high elevation.

Small tree with stiff spreading branches developing an oval-rounded to vase shaped crown. Large showy creamy white flowers turn to winter light tan to brown clusters of capsules.

H 20-25' W 15-25'



Zelkova serrata 'Wireless®'
WIRELESS ZELKOVA

(Z5) Full sun, water-wise.

Broadly spreading vase is medium green foliage turns red in fall. Excellent choice for planting under utility lines.

H 24' W 36'

MEDIUM TREES VISUAL LIST



Carpinus betulus 'Fastigiata'
COLUMNAR HORNBEAM

(Z4) Full sun to part shade. Deciduous. Columnar. Low maintenance. Yellow fall color.

This deciduous, upright symmetrical specimen. The foliage is bright green and dense during the summer, and turns yellow in the fall. Great for a tall screen, hedge, or street tree.

H 30-40' W 20-30'



Celtis occidentalis
COMMON HACKBERRY

(Z2) Full sun. Shade tree. Drought tolerant. Low maintenance.

A broad tree with ascending branches, arching with age. Fall color is yellow. Elm-like foliage; upright arching branches. Thick, knobby bark. Tolerates urban air pollution. Good for parking islands.

H 40-50' W 30-40'



Gleditsia triacanthos var. *inermis*
HONEYLOCUST

(Z3) Full sun.

Deciduous, upright tree with a spreading crown and ascending branches. Compound foliage is dark green, turning to yellow in the fall. Great shade tree for filtered shade requirements. Very salt tolerant.

H 45' W 35'



Koelreuteria paniculata
GOLDEN RAIN TREE

(Z5) Full sun. Street tree. Waterwise. Attracts pollinators.

A dense, rounded tree with a spreading habit. Bright yellow flowers in mid-summer are followed by interesting papery seed capsules. Compound foliage is green, then turns to a golden-orange color in the fall.

H 25' W 25'

LARGE TREES VISUAL LIST



Corylus columna
TURKISH FILBERT

(Z4) Full sun.

Pyramidal, deciduous tree. Broad, semi-glossy bright green leaves, with corky, tan to gray bark may flake to reveal orange-brown inner bark. Tolerant of drought once established.

H 50' W 30'



Gymnocladus dioica 'Espresso'
KENTUCKY COFFEE TREE

(Z4) Full sun. Shade tree, Waterwise

Oval to vase shaped tall deciduous tree with rough, scaly gray-brown bark and large bipinnate compound leaves on arching branches. Bluish green leaves with yellow fall color. Seedless selection. Tolerates heat, drought and cold conditions.

H 50' W 35'



Quercus shumardii
SHUMARD OAK

(Z5) Full sun. Shade tree.

Larger leaves and abundant shade producing abilities. Grows more pyramidal in it's younger stages then it will branch out creating a more vase like look. Shiny dark green leaves can reach 8" long. Late fall color turns the tree a vibrant red.

H 40-60' W 30-60'



Tilia tomentosa
SILVER LINDEN

(Z4) Full sun, shade tree, street tree, fall color.

A broadly pyramidal tree. Glossy leaves have dark green tops and are silvery-white underneath. The early summer blooms are fragrant. Tolerant of heat, drought and urban conditions. Green/Yellow fall foliage.

H 60' W 30'



Ulmus x 'Morton' Accolade'
ACCOLADE ELM

(Z4) Full sun. Deciduous Shade Tree

Vase shaped, medium to large, deciduous elm. Glossy dark green foliage and good yellow fall color.

H 50' W 25'



Zelkova serrata 'Village Green'
VILLAGE GREEN ZELKOVA

(Z5) Full Sun. Pest and disease resistant

The Village Green Zelkova is a large shade tree with a graceful shape, clean foliage, attractive bark, and varying autumn color.

H 40-60' W 30-50'

NEW STREET TREE INSTALLATION DETAIL

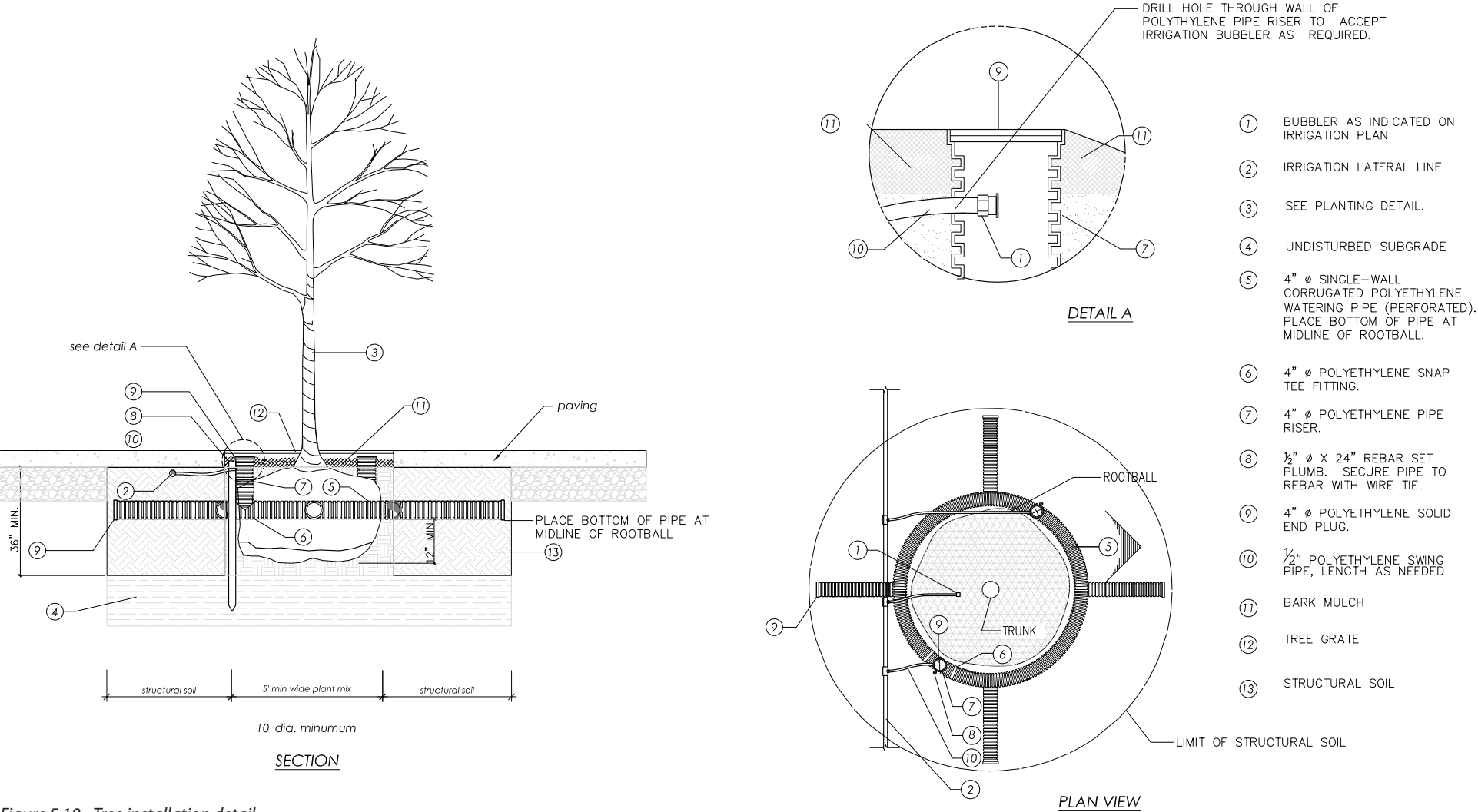


Figure 5.10 - Tree installation detail

PLANT LIST TABLES

Item G.1.

Table 5.06 - Small Plants List

BOTANICAL NAME	COMMON NAME	SPACING	PLANTS/ROW	LOCATION	COLOR
Delosperma cooperi	Hardy Ice Plant	1'-6"	5	between trees	pink-purple
Leucanthemum x superbum 'Snow Lady'	Snow Lady Shasta Daisy	1'-6"	5		white
Leucanthemum superbum PP23181	Banana Cream Shasta Daisy	2'-0"	4		yellow
Sphaeralcea munroana	Munro's Globemallow	2'-0"	4		orange
Schizachyrium scoparium 'Blaze'	Little Bluestem	1'-6"	5		pink-red
Muhlenbergia reverchonii PUND01S	Undaunted Muhly Grass	2'-0"	4		pink
Achillea millefolium 'Paprika'	Paprika Yarrow	1'-6"	5	under tree or between trees	red
Festuca idahoensis 'Siskiyow Blue'	Siskiyow Blue Fescue	1'-6"	5		blue
Penstemon 'Dark Towers' PP20013	Beardtongue	1'-6"	5		pink
Sedum spectabile 'Autumn Joy'	Autumn Joy Stonecrop	2'-0"	4		red
Yucca filamentosa 'Bright Edge'	Bright Edge Yucca	2'-0"	4		white

Table 5.07 - Medium Plants List

BOTANICAL NAME	COMMON NAME	SPACING	PLANTS/ROW	LOCATION	COLOR
Lavandula angustifolia 'Hidcote Blue'	Hidcote Blue English Lavender	3'-0"	3	between trees	violet
Gaura lindheimeri 'Whirling Butterflies'	Beeblossom	3'-0"	3		white
Nepeta x faassenii 'Walkers Low'	Trailing Catmint	3'-0"	3	under tree or between trees	lavender
Artemisia 'Powis Castle'	Wormwood	3'-0"	3		silver
Potentilla fruticosa 'Pink Beauty'	Pink Beauty Cinquefoil	3'-0"	3		pink
Potentilla fruticosa 'Abbotswood'	White Shrubby Cinquefoil	3'-0"	3		white
Achillea 'Firefly Amethyst'	Lavender Yarrow	3'-0"	3		lavender
Amorpha canescens	Leadplant	3'-0"	3		purple

Table 5.08- Large Plants List

BOTANICAL NAME	COMMON NAME	SPACING	PLANTS/ROW	LOCATION	COLOR
Mirabilis multiflora	Desert Four O'Clock	5'-0"	2	between trees	magenta
Potentilla fruticosa 'Fargo' Dakota Sunspot	Yellow Shrubby Cinquefoil	4'-0"	3	under tree or between trees	yellow
Mahonia aquifolium 'Compacta'	Dwarf Oregon Grape	4'-0"	3		yellow
Mahonia repens	Creeping Oregon Grape	6'-0"	2		yellow
Rhus aromatica 'Gro-low'	Grow Low Sumac	6'-0"	2		yellow

SMALL PLANTS VISUAL LIST



Delosperma cooperi
HARDY ICE PLANT

(Z5-9)

Medium to dark green leaves form a low, dense carpet of soft, fleshy foliage, covered with neon, pinkish purple daisy-like flowers from summer to fall. Great for rock gardens.

H 3-6" W 12-36"



Leucanthemum x superbum 'Snow Lady'
SNOW LADY SHASTA DAISY

(Z4) Full sun, deer resistant.

A compact, dwarf form of Shasta Daisy. An ideal choice for the front of any border where a long season of bloom is desired.

H 12" W 12"



Leucanthemum superbum 'PP23181'
BANANA CREAM SHASTA DAISY

(Z5) Full Sun. Perennial.

This shasta daisy's flowers are 4-5" when they open in the summer. Flowers are lemon yellow at time of opening, and become light butter yellow, then creamy white as they mature. An extra row of ray petals gives the flowers a fuller appearance than single-ray varieties. These are award winning as cut flowers, as they last 2-3 weeks once cut.

H 15-18" W 18-24"



Sphaeralcea munroana
MUNRO'S GLOBEMALLOW

(Z4) Full Sun. Waterwise, Native, Summer Blooming

This Globemallow brings a blast of bright orange to the mid-summer garden. A xeric perennial native to the Great Basin of the Western US, this rugged plant thrives in the most challenging hot, sunny dry garden conditions. Drought resistant/drought tolerant plant

H 3.5' W 2'



Schizachyrium scoparium 'Blaze'
BLAZE LITTLE BLUESTEM GRASS

(Z4) Full Sun. Deer Resistant. Drought tolerant.

'Blaze' is a selection of Little Bluestem grass grown for its exceptional fall color. A tall upright warm season grass, 'Blaze' mixes in nicely with flowering perennials and its grass blades turn deep red in the fall maturing to a pink winter color. Makes an excellent transition between landscape and adjacent meadows. Drought resistant/drought tolerant plant.

H 2-3' W 1-2'



Muhlenbergia reverchonii 'PUND01S'
UNDAUNTED MUHLY GRASS

(Z5) Full Sun. Waterwise. Summer and Fall Blooming. Low Maintenance.

Neat clump of light green foliage and in late summer displays airy, ruby-tinged flower spikes. Excellent choice for waterwise gardens as well as dry borders.

H 15-18" W 18-24"



Achillea millefolium 'Paprika'
PAPRIKA YARROW

(Z3) Full sun to part shade, deer resistant, attracts butterflies, good for cut flowers.

Dark green foliage, blooms: Spr/Sum, bloom color: Red with yellow center

H 18-24" W 18-24"

MAINTENANCE: After the flowers have faded, cut the plant back by half. Do not let plant go to seed.



Festuca idahoensis 'Siskiyou Blue'
SISKIYOU BLUE FESCUE

(Z3) Full sun to partial shade. High elevations. Deer resistant.

A modest sized native bunchgrass with beautiful blue-green fine textured foliage. The blades grow eight inches to 1 ft. tall with flower stems 18 inches to 2 ft. tall. Appreciates some protection from hot afternoon sun, especially inland. Drought tolerant, though a little water in hot regions is helpful. Deer resistant.

H 16" W 16"

SMALL PLANTS VISUAL LIST (CONTINUED)



Penstemon 'Dark Towers' PP20013

BEARDTONGUE

(Z3) Perennial. Attracts pollinators. Waterwise. Deer Resistant. High Elevation. Spring Blooming. Low Maintenance.

A showy beardtongue with tall, blooming spikes of dark pink to light pink flowers, with thick, dark purple foliage.

H 24-36" W 12-24"



Sedum spectabile 'Autumn Joy'

AUTUMN JOY STONECROP

(Z3-11)

Its flowers bloom from August into November; they open pink and mature to a copper befitting of autumn. It is 2 feet tall and wide, with succulent stems and leaves. 'Autumn Joy' looks great with ornamental grasses.

H 18-24" W 18-24"



Yucca filamentosa 'Bright Edge'

BRIGHT EDGE YUCCA

(Z4-9) Full Sun. Flowering. Drought tolerant. Low maintenance.

Sword-like leaves, boldly edged in bright gold topped by tall spikes of fragrant, ivory white blooms. Smaller than the species, perfect for adding contrast and texture as a garden accent or container plant. Forms dramatic clumps with age. Drought and heat tolerant. Requires minimal care.

H 2' W 3'

MEDIUM PLANTS VISUAL LIST



Lavandula angustifolia 'Hidcote Blue'
HIDCOTE BLUE ENGLISH LAVENDER

(Z5) Drought tolerant, full sun to partial shade, deer resistant, good for cutting or large pasture plantings. Attracts butterflies.

Thin low growing grass-like leaves with spiked flower stems. Compact deep-purple variety.

H 12-18" W 36"



Gaura lindheimeri 'Whirling Butterflies'
BEEBLOSSOM

(Z5) Full sun, water-wise, attracts pollinators

'Whirling Butterflies' has wispy flowering stems bright white butterfly-shaped flowers. A tall, robust grower.

H 2-3' W 2-3'



Nepeta 'Walkers Low'
CATMINT

(Z4) Full Sun to partial shade, attracts butterflies, water-wise, deer and rabbit resistant.

Moundy grey-green foliage on upright billowy growth that blooms spring through fall.

H 24" W 24-36"

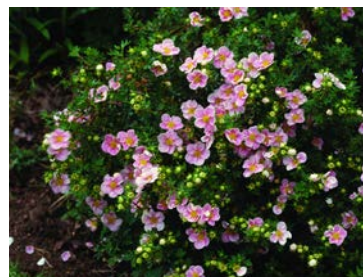


Artemisia 'Powis Castle'
WORMWOOD

(Z4) Full Sun to Part Shade. Native. Waterwise. Deer resistant.

This beautiful, lacy woody perennial has finely dissected, aromatic silver-green foliage and silver-gray flowers. Water use is low once plant is established.

H 34-36" W 24-36"



Potentilla fruticosa 'Pink Beauty'
PINK BEAUTY CINQUEFOIL

(Z2) Full sun, waterwise, deer resistant.

Small compact deciduous shrub that has clear pink flowers from June until first frost in the fall. Fine-textured green foliage is bushy and full making this potentilla an excellent flowering border plant.

H 2-3' W 2-3'



Achillea 'Firefly Amethyst'
LAVENDER YARROW

(Z3) Full Sun to Part Shade. Waterwise. Perennial. Attracts Pollinators. Deer Resistant. Summer Blooming.

The 'Firefly' tend to be taller and hold their color longer without losing their tight habit or sturdy stems. Bright lavender pink flowers age to a soft pink. These are great as a cut flower.

H 18-22" W 30-36"



Potentilla fruticosa 'Abbotswood'
WHITE SHRUBBY CINQUEFOIL

(Z2) Full sun to Part Shade. Waterwise. Deer Resistant. Spring Blooming.

Abbotswood is fine-textured, small, and covered with white flowers. Deciduous, dense, rounded habit. Foliage is green and turns to yellow-brown in the fall. This plant works well in small spaces.

H 3' W 3'



Amorpha canescens
LEADPLANT

(Z4) Full Sun to Part Shade. Waterwise. Attracts Pollinators. Summer Blooming. Low Maintenance.

Spikes of iridescent purple flowers appear atop the delicate silver-gray-green foliage in the heat of mid-summer.

H 24" W 36"

LARGE PLANTS VISUAL LIST



Mirabilis multiflora
WILD FOUR O'CLOCK

(Z4) Full Sun to part shade. Deer & rabbit resistant.

Wild Four O'clock is a magnificent native wildflower that blooms all summer with magenta-pink flowers that open in the afternoon. A perennial with huge, deep roots, it needs no extra water once established. Spreads widely,

H 1.5' W 4-6'



Potentilla fruticosa
SHRUBBY CINQUEFOIL

(Z2) Full sun, waterwise, deer resistant.

Small fine-textured deciduous flowering shrub. Blooms late spring through fall. White, yellow, pink, orange, and red flowering varieties.

H 1-4' W 2-4'



Mahonia aquifolium 'compacta'
DWARF OREGON GRAPE

(Z4) Part shade, and water-wise.

Compact broadleaved evergreen. Holly-like glossy dark green leaves turn red to bronze in the winter. Clusters of bright yellow flowers bloom in the spring followed by blue to black berries. Tolerates drought best in the shade.

H 2-3' W 3-4'



Mahonia repens
CREEPING OREGON GRAPE

(Z2) Full sun to full shade, deer and rabbit resistant, attracts butterflies, native. Utah native.

Small broadleaf evergreen. Multi-colored holly-like leaves turn reddish-purple in fall. Yellow blooms in late spring. Winter interest.

H 12-36" W 36-60"



Rhus aromatica 'Gro-Low'
GROW LOW SUMAC

(Z3) Full sun to part shade, water-wise, deer and rabbit resistant.

Compact habit with glossy green leaves. Excellent for erosion control. Small yellow flowers followed by red berries. Scarlet-orange fall color.

H 2-3' W 6-8'



Figure 5.11 - Park strip example before refurbishment



Figure 5.12 - Park strip example after refurbishment



Figure 5.13 - Refurbished park strip example: 2'-0" planting width



Figure 5.14 - Refurbished park strip example: 3'-0" planting width



Figure 5.15 - Refurbished park strip example: 4'-0" planting width



Figure 5.16 - Refurbished park strip example: 5'-0" planting width



Figure 5.17 - Refurbished park strip example: 8'-0" planting width



Figure 5.18 - Refurbished median example: 9'-0" planting width of standard 14'-0" median

6.0 PRIORITIZATION

A prioritization of corridors and key streetscape updates is necessary to give guidance to future investments in city infrastructure. The intent of a phased approach is to work through a pilot project using this streetscape update process and refine the process before larger projects are tackled.

At each phase of the project, city staff should reach out to property owners along the corridor and determine which segments of the parkstrip project should be turned over to private maintenance once the updates are complete.

Additionally, coordination of these corridors should include an understanding of UDOT road projects for these routes. City staff should coordinate with UDOT staff to plan streetscape improvements and investments only after road updates are completed by UDOT in the near future.

6.1 CORRIDOR PRIORITIZATION

During development of this master plan, it was determined that highly visible corridors in South Jordan would be a higher priority than other corridors.

The map on the following page outlines first, second, and third priority corridors. Some of these corridors are quite lengthy, and should be split into smaller, phased projects within themselves:

1. First Priority Corridors
 - (a) 10600 South
 - (b) 11400 South
 - (c) 1300 West
 - (d) 2700 West (north segment) as pilot project
2. Second Priority Corridors
 - (a) 4800 West
 - (b) 10200 South
 - (c) 2200 West
3. Third Priority Corridors
 - (a) Redwood Road (coordinate with Google Fiber updates)
 - (b) 10000/Shields Lane
 - (c) 2700 West (south segment)
 - (d) 3200 West
 - (e) 4000 West

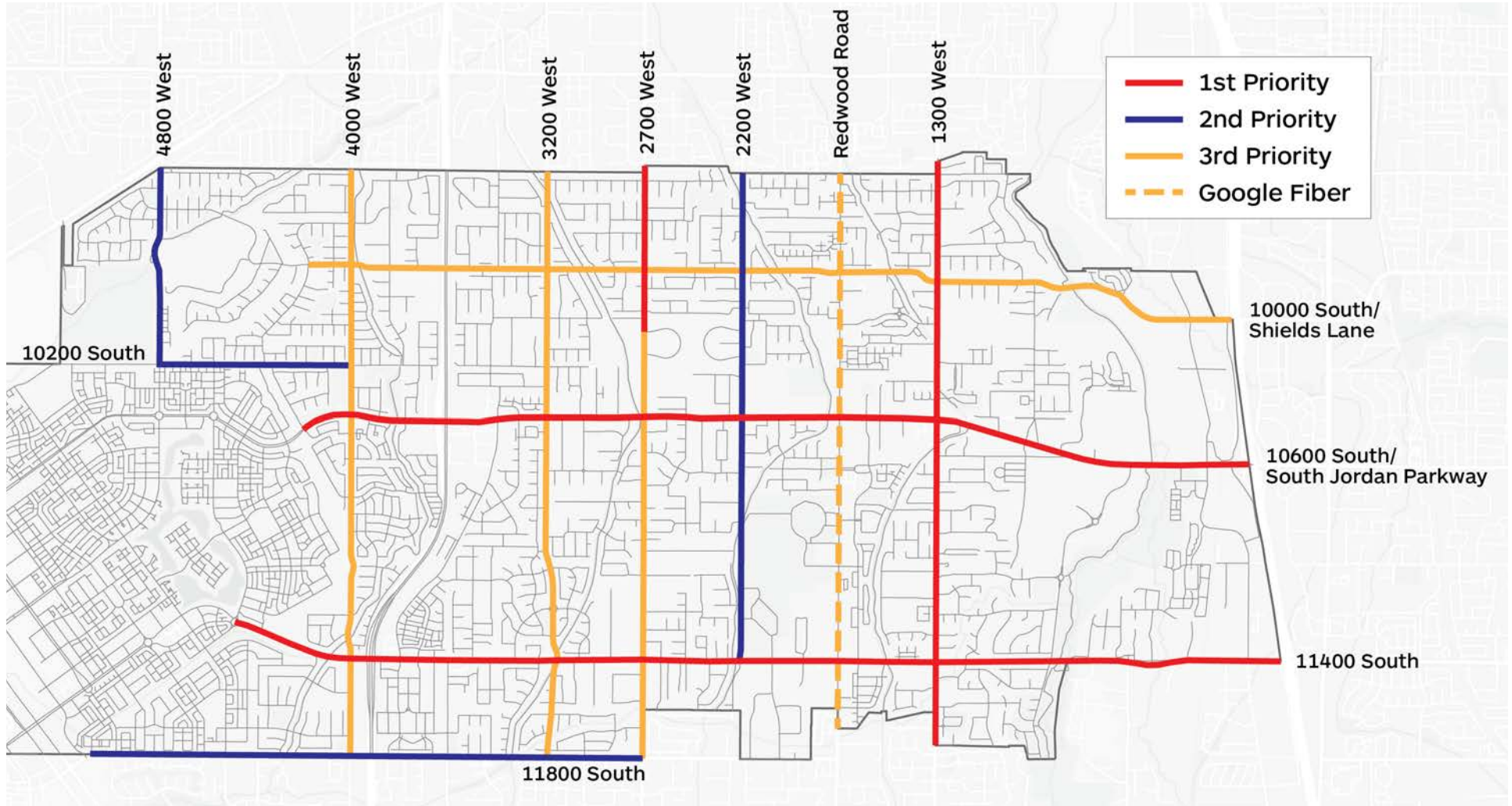


Figure 6.01 - Corridor prioritization map for updates to landscaped park strips currently maintained by South Jordan City

APPENDICES

APPENDIX A: WORKSHEET

The following page is the full-size version of the planting design algorithm worksheet. Use this page for printing the worksheet.

Park Strip Planting Design Algorithm Worksheet

10-Jan-2023 DRAFT

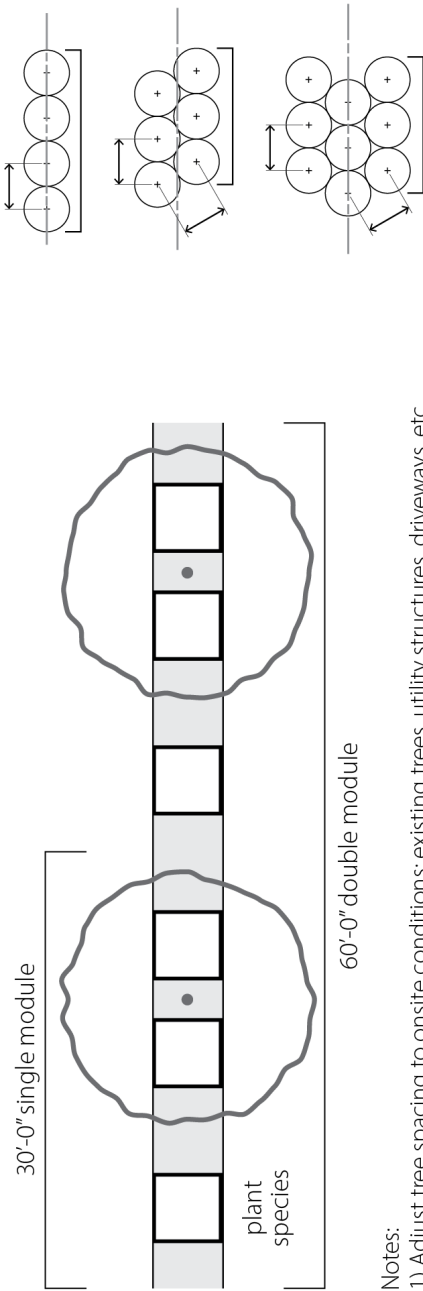


Street _____

From _____ to _____ Linear Feet _____

1 Site Type: park strip / median 2 Overhead Power Lines: Y / N 3 Planted Area Width: _____

4 Planting Module Size: single / double 5 Number of Modules: _____



Notes:
1) Adjust tree spacing to onsite conditions: existing trees, utility structures, driveways, etc.
2) Where no tree is required, combine both "under tree" plant groups into one group

1, 2, and 3 row group layouts

Tree

6 Tree List: S / M / L / none 7 Tree Species: _____

Plants

Plant Species A

8 Plant List: S / M / L 9 Plant Species: _____

10 Spacing: _____ 11 Rows/Group: _____ 12 Plants/Row: _____ 13 Location: under tree / between trees

Plant Species B

14 Plant List: S / M / L 15 Plant Species: _____

16 Spacing: _____ 17 Rows/Group: _____ 18 Plants/Row: _____ 19 Location: under tree / between trees

Plant Species C

20 Plant List: S / M / L 21 Plant Species: _____

22 Spacing: _____ 23 Rows/Group: _____ 24 Plants/Row: _____ 25 Location: under tree / between trees

Item G.1.

APPENDIX B: MAINTENANCE TABLE

This table lists the park strip and median shapes that should not be maintained by the city. These are the shapes indicated in red in the updated maintenance maps. This table is intended to be a quick reference. The actual GIS data will be supplied to the city.

OBJECTID_1	Maintenance	Address
229	Abutting Property Owner	
232	Abutting Property Owner	
233	Abutting Property Owner	
234	Abutting Property Owner	
235	Abutting Property Owner	
57	Abutting Property Owner	10305 S
121	Abutting Property Owner	Windstone Dr
217	Abutting Property Owner	
218	Abutting Property Owner	
219	Abutting Property Owner	
222	Abutting Property Owner	
230	Abutting Property Owner	
61	Abutting Property Owner	2700 W
74	Abutting Property Owner	Jordan River Dr

79	Abutting Property Owner	11800 S	108	Abutting Property Owner	Temple Dr
80	Abutting Property Owner	2700 W	109	Abutting Property Owner	Temple Dr
81	Abutting Property Owner	2700 W	110	Abutting Property Owner	Temple Dr
82	Abutting Property Owner	2700 W	111	Abutting Property Owner	1000 W
83	Abutting Property Owner	2700 W	112	Abutting Property Owner	1000 W
84	Abutting Property Owner	2700 W	113	Abutting Property Owner	1000 W
85	Abutting Property Owner	2700 W	114	Abutting Property Owner	1055 W
86	Abutting Property Owner	2700 W	115	Abutting Property Owner	11800 S
87	Abutting Property Owner	2700 W	116	Abutting Property Owner	11800 S
88	Abutting Property Owner	2700 W	117	Abutting Property Owner	4000 W
89	Abutting Property Owner	2700 W	118	Abutting Property Owner	4000 W
90	Abutting Property Owner	2700 W	119	Abutting Property Owner	Rushton View Dr
91	Abutting Property Owner	2700 W	120	Abutting Property Owner	Rushton View Dr
92	Abutting Property Owner	2700 W	123	Abutting Property Owner	2200 W
93	Abutting Property Owner	2700 W	124	Abutting Property Owner	2700 W
94	Abutting Property Owner	2700 W	125	Abutting Property Owner	2700 W
95	Abutting Property Owner	2700 W	126	Abutting Property Owner	2700 W
96	Abutting Property Owner	2700 W	127	Abutting Property Owner	2700 W
97	Abutting Property Owner	2700 W	128	Abutting Property Owner	2700 W
98	Abutting Property Owner	2700 W	129	Abutting Property Owner	2700 W
99	Abutting Property Owner	2700 W	130	Abutting Property Owner	2700 W
100	Abutting Property Owner	2700 W	131	Abutting Property Owner	2700 W
101	Abutting Property Owner	2700 W	132	Abutting Property Owner	2700 W
107	Abutting Property Owner	10200 S	133	Abutting Property Owner	2700 W

134	Abutting Property Owner	2700 W	163	Abutting Property Owner		198	Abutting Property Owner	4800 S
135	Abutting Property Owner	2700 W	164	Abutting Property Owner	2700 W	199	Abutting Property Owner	4800 S
136	Abutting Property Owner	2700 W	165	Abutting Property Owner	2700 W	200	Abutting Property Owner	2700 W
137	Abutting Property Owner	2700 W	166	Abutting Property Owner		201	Abutting Property Owner	2700 W
138	Abutting Property Owner	2700 W	167	Abutting Property Owner	2700 W	206	Abutting Property Owner	1055 W
139	Abutting Property Owner	2700 W	168	Abutting Property Owner	2700 W	207	Abutting Property Owner	2700 W
140	Abutting Property Owner	2700 W	169	Abutting Property Owner	2700 W	208	Abutting Property Owner	2700 W
141	Abutting Property Owner	2700 W	170	Abutting Property Owner		209	Abutting Property Owner	2700 W
144	Abutting Property Owner	3200 W	171	Abutting Property Owner	2700 W	210	Abutting Property Owner	2700 W
145	Abutting Property Owner	3200 W	172	Abutting Property Owner	2700 W	224	Abutting Property Owner	11010 S
146	Abutting Property Owner	3200 W	173	Abutting Property Owner	2700 W	225	Abutting Property Owner	Beckstead Ln
147	Abutting Property Owner	3200 W	174	Abutting Property Owner	2700 W	226	Abutting Property Owner	Beckstead Ln
150	Abutting Property Owner	Temple Dr	175	Abutting Property Owner	2700 W	227	Abutting Property Owner	
151	Abutting Property Owner	Temple Dr	176	Abutting Property Owner	2700 W	231	Abutting Property Owner	
152	Abutting Property Owner	Temple Dr	177	Abutting Property Owner	Beckstead Ln	228	Abutting Property Owner	
153	Abutting Property Owner	Temple Dr	178	Abutting Property Owner	Beckstead Ln			
154	Abutting Property Owner	Temple Dr	183	Abutting Property Owner	4000 W			
155	Abutting Property Owner	Temple Dr	184	Abutting Property Owner	4000 W			
156	Abutting Property Owner	Temple Dr	185	Abutting Property Owner	2700 W			
157	Abutting Property Owner	Temple Dr	186	Abutting Property Owner	2700 W			
158	Abutting Property Owner	Temple Dr	187	Abutting Property Owner	2700 W			
159	Abutting Property Owner	Temple Dr	194	Abutting Property Owner	2700 W			
160	Abutting Property Owner	Temple Dr	195	Abutting Property Owner	2700 W			
162	Abutting Property Owner		196	Abutting Property Owner	2700 W			

APPENDIX C: PLANTING ALGORITHM TABLE & WORKSHEET EXAMPLE

This diagram illustrates how the different tables are used and cross referenced to populate the values in the planting design worksheet. The numbers in boxes correspond to values on the worksheet. The process is repeated for each plant and tree species.

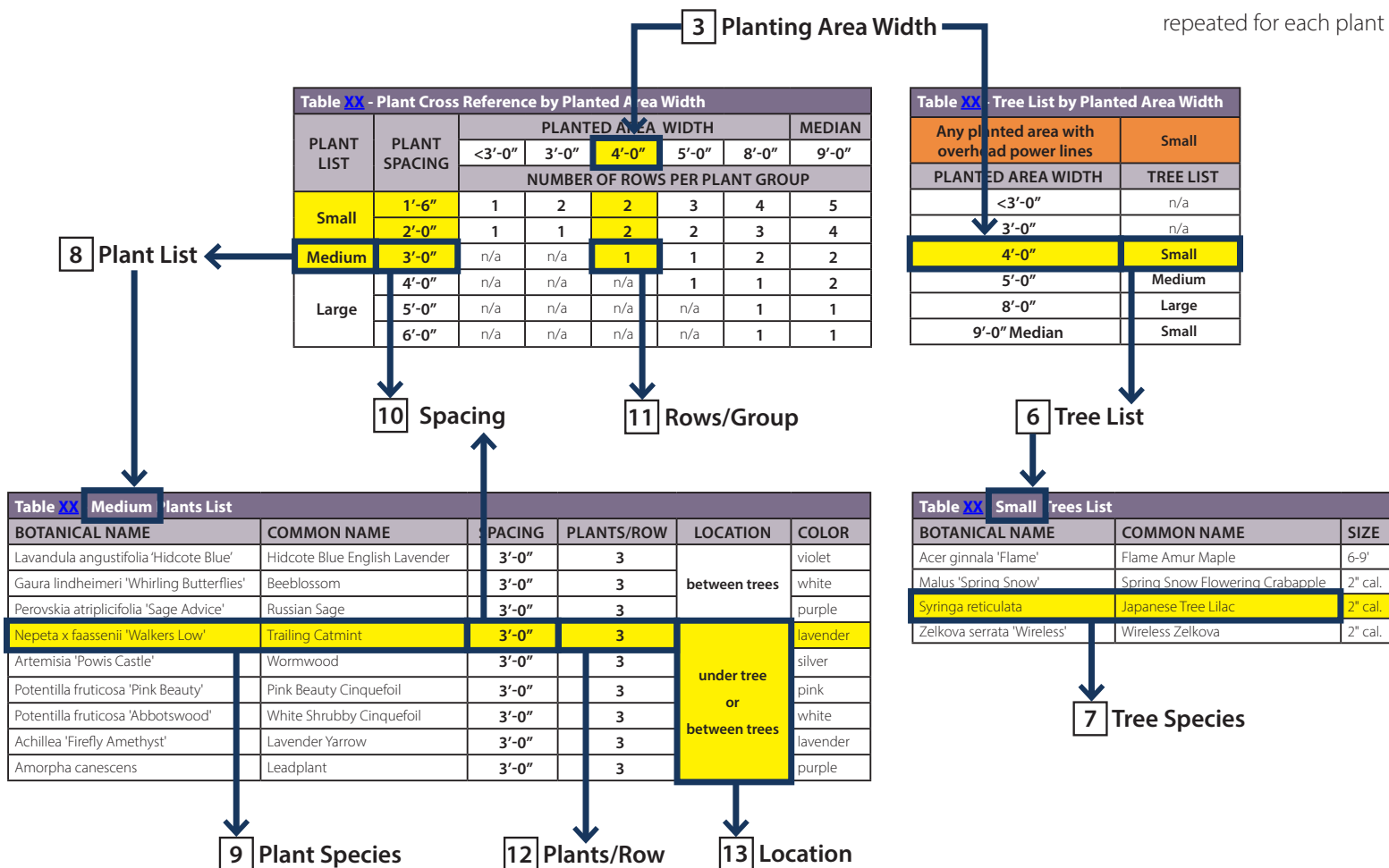




Figure D.01 - Example park strip on a collector street at a single family rear property line with wall

APPENDIX D: EXISTING CONDITIONS

Currently, South Jordan maintains a network of parkstrips and medians throughout the city. These are primarily found along the arterial and collector road corridors running east-west and north-south, but also include some areas around free-way intersections, as well as some smaller residential roads adjacent to residential developments.

A majority of the roadways in South Jordan are constructed and maintained by the City. However, Redwood Road (SR-68), Bangerter Highway (SR-154), South Jordan Parkway (SR-151), 11400 South (SR-175), and Mountain View Corridor (SR-85) are state roadways that are maintained by the Utah Department of Transportation (UDOT). Due agreements with UDOT, South Jordan maintains the parkstrips of large portions of Redwood Road, South Jordan Parkway, and 11400 South.

Table 3.1 gives a summary of the roadways containing parkstrips and medians maintained by the city. Table 3.2 [need to generate] lists other streetscapes maintained by the city, such as the interchanges at 10400 S and 11400 S on the west side of I-15, the park area on 11400 S between Bangerter and 4000 W, and the various gateways around the perimeter of the city [list to be confirmed].

Along these corridors, the areas of parkstrip maintained by the city are often scattered and intermittently spaced between privately maintained parkstrips. This is sometimes due to the city code, which states that *"the City will accept responsibility for maintenance of*

the parkstrips along collector streets where collector street fencing has been installed along the rear and side property lines of lots only in single-family residential subdivisions" (City Code Chapter 16.04.190.6.a). As a result, where properties have frontage on a corridor, the parkstrip is maintained by the city. But where residential developments have side or rear yards with fencing, the City assumes maintenance responsibilities of these parkstrips. The result of these scattered sections of city maintained parkstrip is often an inconsistent and patchwork appearance along many of the city's corridors.

One of the tasks of this Streetscape and Parkstrip Master Plan is it to develop and overall re-landscape and refurbishment plan that will improve the design and character of the city's many corridors. In order to carry out this task, an inventory and analysis of these parkstrips is first required, to guide future steps of the planning process.

The inventory will be presented on a street by street basis, as it is the desire of the city to document these corridors and ultimately to help define a unique streetscape character for each corridor. Interchanges and gateways will also be inventoried and analyzed separately.



Figure D.02 - Park strips and medians that are currently maintained by the city are shown in green

TABLE D.01 - SOUTH JORDAN STREETS WITH CITY MAINTAINED STREETSCAPES AND PARKSTRIPS

The table lists the streets / corridors within the city, starting from north to south, and then from east to west. Also shown is the main direction of the street and the street category according to the South Jordan Transportation Master Plan (2019).

Corridor	Direction	SJ TMP Category	Item G.1.
9400 S	e-w	other	
Shields Lane	e-w	minor collector (3)	
Reunion Avenue	e-w	other	
Silver Streak Drive	e-w	other	
10200 S	e-w	minor collector (2)	
South Jordan Parkway (E of Redwood)	e-w	arterial (7)	
South Jordan Parkway (W of Redwood)	e-w	arterial (5)	
10550 S	e-w	other	
Bison Ridge Road	e-w	other	
Rushton View Dr	e-w	other	
10755 S	e-w	other	
10840 S	e-w	other	
11050 S / Jordan River Drive	e-w	other	
11400 S	e-w	arterial (5)	
11800 S (2700 W to 3600 W)	e-w	minor collector (3)	
11800 S (3600 W to S154)	e-w	major collector (5)	
11800 S (S154 to MVC)	e-w	major collector (3)	
11800 S (Tail Crossing to Kitty Hawk)	e-w	major collector (5)	
Lake Avenue	e-w	minor collector (2)	
Lake Avenue	e-w	Daybreak arterial (4)	
Lake Avenue	e-w	major collector (2)	
Daybreak Parkway	e-w	Daybreak arterial (4)	
River Front Parkway (north of JR Drive)	n-s	major collector (5)	
River Front Parkway (south of JR Drive)	n-s	minor collector (2)	
1000 W	n-s	minor collector (2)	
1300 W	n-s	major collector (3)	
Beckstead Lane / 1600 W	n-s	other	
Redwood Road	n-s	arterial (5)	
2200 W	n-s	minor collector (2)	
2700 W	n-s	minor collector (3)	
3200 W	n-s	minor collector (2)	
3200 W (SJP to 11400 S)	n-s	minor collector (3)	
River Heights Drive	n-s	major collector (3)	
3400 W	n-s	other	
3600 W	n-s	minor collector (3)	
4000 W (north of 11400 S)	n-s	major collector (3)	
4000 W (south of 11400 S)	n-s	major collector (5)	
4800 W	n-s	minor collector (3)	

1300 WEST: INVENTORY AND ANALYSIS

EXISTING CONDITIONS

1300 West is categorized by the South Jordan Transportation Master Plan (SJTMP) as a major collector with 3 lanes. A typical cross section of the street ROW includes sidewalk, parkstrip (3-5' width), parking lane, bicycle lane, driving lane, center lane, driving lane, bicycle lane, parking lane, parkstrip (3-5' width), sidewalk.

There are a variety of parkstrip conditions found along 1300 West, as shown by the inventory key graphic below. Large segments of the streetscape are not maintained by the city, meaning that many parkstrips are maintained by the owner, and the city-maintained parkstrips are scattered and inconsistent. In some cases, the city is maintaining parkstrips that should be maintained by the private owner.

Most of the parkstrips are narrower than the 6' wide city standard. Utility lines run along the west side of the street for the entire length of the street.

Vegetation in the parkstrips range from grass or grass with trees to gravel mulch with a mix of plantings (trees, shrubs, perennials and grasses). Some parkstrips contain mature trees, but the majority of trees along 1300 West are young and small.

ANALYSIS

Currently, 1300 West has a patchwork of parkstrip conditions that lack any cohesion. Some of this is due to the fact that the city only maintains a small percentage of the parkstrips on this street, but the lack of consistency is also found between the city maintained parkstrips themselves. While the majority of the city maintained parkstrips are lawn with trees, this is not always the case. In some cases, the parkstrips are completely vegetated, but in other cases the parkstrips alternate between vegetation and stamped concrete. In addition, tree type and size vary greatly, and some parkstrips have no trees at all. The parkstrips with gravel mulch tend to look very bare, as the amount of plantings within these areas is minimal. Lastly, the privately maintained parkstrips have no consistency at all, as they are not forced to follow any design standards. The result is a street with no identity or character.

The fact that a majority of parkstrips contain lawn as a ground cover is also a concern. The new city code states that lawn should not be installed in parkstrips. Water conservation is becoming an increasingly important issue for the city and the state as a whole. Replacing legacy lawn should be prioritized.

1300 WEST PHOTOS



Example of a parkstrip with alternating lawn and concrete



Example of a parkstrip with gravel, plants and trees



Example of a parkstrip with lawn and trees



Example of a parkstrip with lawn

1300 WEST PHOTOS



Example of a parkstrip lawn with frontage on a private residence



Example of a parkstrip with alternating concrete and unfinished planting



Example of a parkstrip with lawn and trees that is a commercial property



Example of a parkstrip with lawn that is in poor condition

RIVER FRONT PARKWAY: INVENTORY AND ANALYSIS

EXISTING CONDITIONS

River Front Parkway is the main roadway providing access to the River Park business park, and also to River Front Park and Jordan River. The street has two categorizations according to the categorized by the South Jordan Transportation SJTMP. North of River Front Drive, it is a major collector with 5 lanes. South of River Front Drive it is classed as a minor collector with 2 lanes. A typical cross section of the major collector ROW includes sidewalk, parkstrip (3-5' width), two driving lanes, center lane / median, two driving lanes, parkstrip (3-5' width), sidewalk. There are two large roundabouts within the River Park business park area. A typical cross section of the minor collector ROW includes sidewalk, parkstrip (3-5' width), two driving lanes, parkstrip (3-5' width), sidewalk.

There vast majority of parkstrip along this corridor is maintained by the city and consists of lawn, (see the inventory key graphic). There are no trees located within the parkstrips. The medians and roundabouts consist of trees (both deciduous and evergreen), mixed plantings, and lawn. The roundabouts also contain landscaping features including water features, signage, artwork and boulders walls. The majority of parkstrips are narrower than the 6' wide city standard.

There are two smaller side streets that branch off from the roundabouts. These are 10840 South and Jordan River Drive, and both contain narrow parkstrips maintained

by the city within the limits of the business park. The parkstrips along 10840 South consist of lawn and trees. The parkstrips of Jordan River Drive consist of lawn.

ANALYSIS

River Front Parkway has a consistent character along business park portion of its length. This character is lush lawn, with the medians and roundabouts having a more diversified landscape including lawn and other features. While consistent, the character currently emphasizes overuse of water, both within the parkstrips due to lawn, and within the roundabouts due to the water features. Throughout the business park, the parkstrips are narrow, permitting only small trees if they were to be planted in the future.

South of the business park, the parkstrips continue to be lawn where maintained by the city. These are primarily parkstrips adjacent to the River Park and Jordan River. The parkstrips become wider south of the Beckstead Canal trail and could support larger trees in the future. South of 11400 South, much of the city maintained parkstrip is adjacent to natural open space and has not recently been maintained / irrigated. Parkstrips maintained by private owners consist of lawn and trees. A shift to a mix of trees and water wise plantings south of River Front Drive could act as a visual signal that the street type has changed to residential / park.

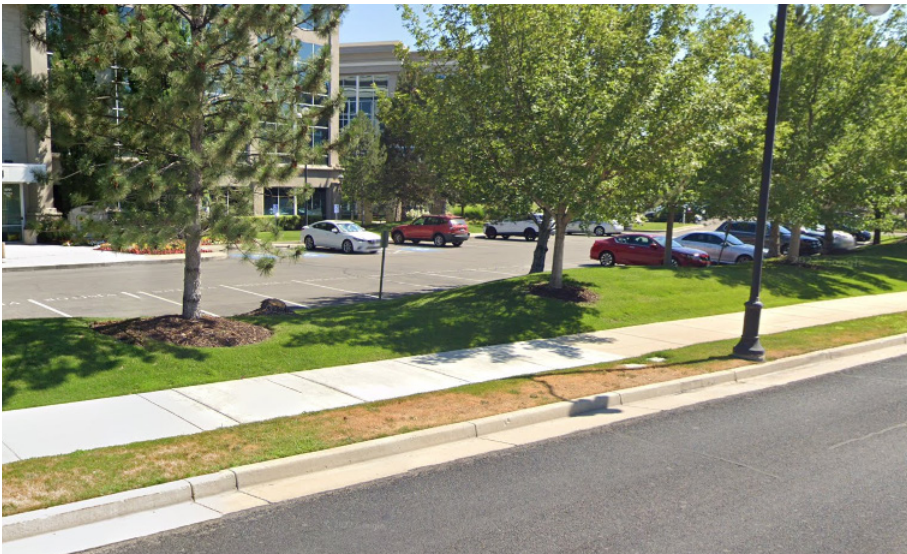
RIVER FRONT PARKWAY PHOTOS



Example of a typical parkstrip with lawn



Example of a median with lawn, trees, mixed plantings and boulders



Example of a parkstrip lawn that is dry



Example of a roundabout with lawn, trees, mixed plantings and landscape features

RIVER FRONT PARKWAY PHOTOS



The River Front Drive parkstrip with lawn



The 10840 S parkstrip with lawn and small streets



Example of a parkstrip with lawn south of River Front Drive. East River Front park at left



Example of a parkstrip with lawn south of 11400 South.

SHIELDS LANE/ 9800 SOUTH: INVENTORY AND ANALYSIS

EXISTING CONDITIONS

Shields Lane is categorized by the South Jordan Transportation Master Plan (SJTMP) as a minor collector with 3 lanes. A typical cross section of the street ROW includes sidewalk, parkstrip (3-5' width), bicycle lane, driving lane, center lane, driving lane, bicycle lane, parking lane, parkstrip (3-5' width), sidewalk.

There are a variety of parkstrip conditions found along 9800 South, as shown by the inventory key graphic below. Large segments of the streetscape are maintained by the City, meaning that few parkstrips are maintained by the owner.

Most of the parkstrips are narrower than the 6' wide city standard. Vegetation in the parkstrips range from grass or grass with trees to gravel mulch with a mix of plantings (trees, shrubs, perennials and grasses). A majority of parkstrips contain mature trees, with some trees along 9800 South that are young and small.

ANALYSIS

Currently, 9800 South has relatively consistent parkstrip design, within the city maintained areas. The majority of the city maintained parkstrips are lawn with trees however, in some cases, the sidewalk lacks a parkstrip and in other cases the parkstrip contains only grass.

Along, 9800 South, the tree type seems to remain the same while the size varies greatly. Most of the parkstrips have grass surrounding the trees. Lastly, the privately maintained parkstrips have no consistency at all, as they are not forced to follow any design standards. The result is a street with no identity or character.

The fact that a majority of parkstrips contain lawn as a ground cover is also a concern. The new city code states that lawn should not be installed in parkstrips. Water conservation is becoming an increasingly important issue for the city and the state as a whole. Replacing legacy lawn should be prioritized.

SHIELDS LANE/ 9800 SOUTH PHOTOS



Example of a sidewalk before installation of landscaping



Example of street trees, parkstrip, and street lighting on north side of Shields Lane



Example of parkstrips on both sides of street with street trees



Example of a parkstrip with mature street trees.

SHIELDS LANE/ 9800 SOUTH PHOTOS



Example of a parkstrip and sound wall along north side of Shields Lane



Example of parkstrip and mature street trees on south side of Shields Lane



Example of parkstrip, wall, and street trees on the north side of Shields Lane



Example of parkstrip with lawn, and young street trees

10600 SOUTH/ SOUTH JORDAN PARKWAY: INVENTORY AND ANALYSIS

EXISTING CONDITIONS

10600 South is categorized by the South Jordan Transportation Master Plan (SJTMP) as an arterial with 7 lanes east of Redwood Road and a arterial with 5 lanes west of Redwood Road. A typical cross section of the street ROW includes sidewalk, parkstrip (3-5' width), parking lane, bicycle lane, 3 driving lanes, center lane, 3 driving lanes, bicycle lane, parking lane, parkstrip (3-5' width), sidewalk.

There are a variety of parkstrip conditions found along 10600 South, as shown by the inventory key graphic below. About half of streetscape along 10600 South are maintained by the city, meaning that the other half of parkstrips are maintained by the owner. The City-maintained parkstrips seem to be concentrated west of Redwood Road. Most of the parkstrips are narrower than the 6' wide city standard.

Vegetation in the parkstrips ranges from just grass to grass with trees. Some parkstrips contain mature trees, but the majority of trees along 10600 South are young and small.

ANALYSIS

Currently, 10600 South has a fairly consistent network of parkstrip conditions predominately west of Redwood Road. This is due to the fact that the city maintains a large percentage of the parkstrips west of Redwood Road, but not east of Redwood Road.

In addition, only about half of the parkstrips contain trees and lawn, while the other half are just lawn.

Lastly, the privately maintained parkstrips have no consistency at all, as they are not forced to follow any design standards. The result is a street with no identity or character.

The fact that a majority of parkstrips contain lawn as a ground cover is also a concern. The new city code states that lawn should not be installed in parkstrips. Water conservation is becoming an increasingly important issue for the city and the state as a whole. Replacing legacy lawn should be prioritized.

10600 SOUTH/ SOUTH JORDAN PARKWAY PHOTOS



Example of a sidewalk without parkstrip near Beckstead Lane



Example of parkstrip and street lighting on south side of 10600 South



Example of large parkstrip, lawn, and street lighting



Example of a parkstrip with lawn

10600 SOUTH/ SOUTH JORDAN PARKWAY PHOTOS



Example of a parkstrip, lawn, and street lighting



Example of roundabout



Example of center median with grasses and street trees



Example of parkstrip with sound wall, lawn, and mature street trees

11400 SOUTH: INVENTORY AND ANALYSIS

EXISTING CONDITIONS

11400 South is categorized by the South Jordan Transportation Master Plan (SJTMP) as an arterial with 5 lanes. A typical cross section of the street ROW includes sidewalk, parkstrip (3-5' width), parking lane, bicycle lane, driving lane, center lane (or center median), driving lane, bicycle lane, parking lane, parkstrip (3-5' width), sidewalk.

There are a variety of parkstrip conditions found along 11400 South, as shown by the inventory key graphic below. A vast majority of the streetscape is maintained by the City, meaning that there are only a few parkstrips that are maintained by the owner. The City maintained parkstrips are grouped together. Most of the parkstrips are narrower than the 6' wide city standard.

Vegetation in the parkstrips range from grass or grass with trees, to gravel mulch with a mix of plantings (trees, shrubs, perennials and grasses). Some parkstrips contain mature trees, but the majority of trees along 11400 South are young and small.

ANALYSIS

Currently, 14000 South has cohesive parkstrip conditions. This is due to the fact that the City maintains a large percentage of the parkstrips and center medians along this street. While the majority of the city maintained parkstrips are lawn with trees, this is not always the case. In some cases, the parkstrips are completely vegetated, but in other cases the parkstrips alternate between vegetation and stamped concrete. In addition, tree type and size vary greatly, and some parkstrips have no trees at all. The parkstrips with gravel mulch tend to look very bare, as the amount of plantings within these areas is minimal.

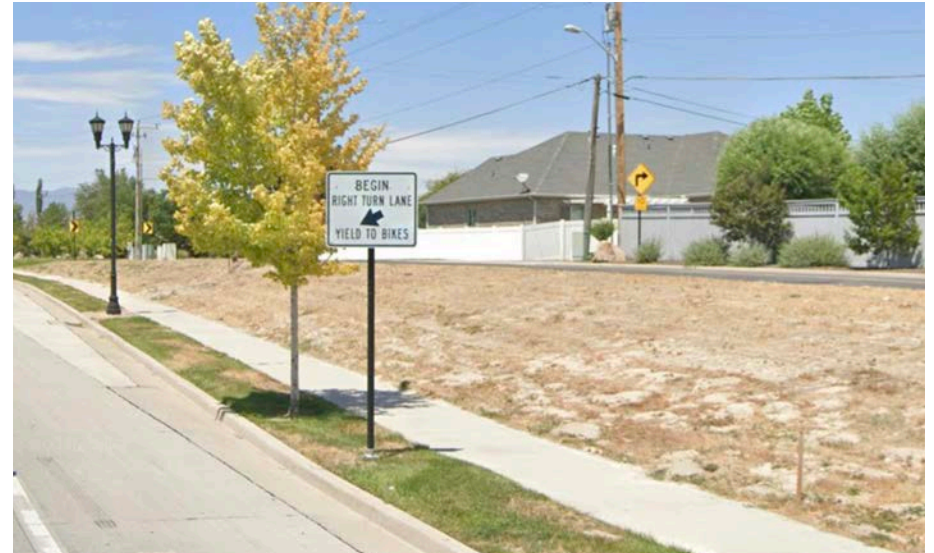
Lastly, the few privately maintained parkstrips have no consistency, as they are not forced to follow any design standards. The result is a street with no identity or character.

The fact that a majority of parkstrips contain lawn as a ground cover is also a concern. The new city code states that lawn should not be installed in parkstrips. Water conservation is becoming an increasingly important issue for the city and the state as a whole. Replacing legacy lawn should be prioritized.

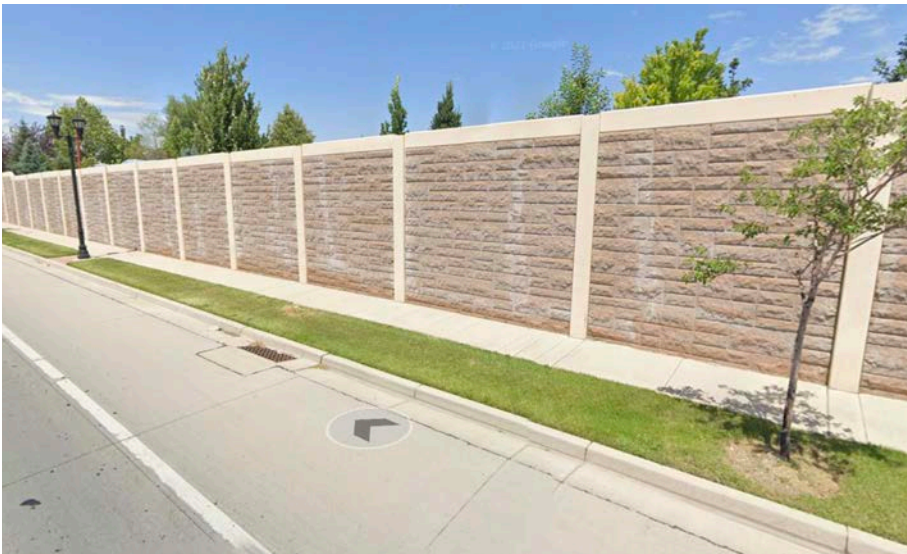
11400 SOUTH PHOTOS



Example of a sidewalk with and unimproved parkstrip and street lighting near I-15



Example of a sidewalk with a parkstrip improved with street trees, grass, and street lights



Example of sidewalk and improved parkstrip with sound wall



Example of a center median with landscaping and street tree improvements

11400 SOUTH PHOTOS



Example of a sidewalk with a gravel and landscaped parkstrip



Example of sidewalk with sound wall, and parkstrip with power poles, and street trees



Example of sidewalk and parkstrip with lawn



Example of sidewalk without parkstrip

DAYBREAK PARKWAY: INVENTORY AND ANALYSIS

EXISTING CONDITIONS

Daybreak Parkway is categorized by the South Jordan Transportation Master Plan (SJTMP) as a Daybreak arterial with 4 lanes. A typical cross section of the street ROW includes sidewalk, parkstrip (3-5' width), parking lane, bicycle lane, driving lanes, center lane (or landscape median), driving lanes, bicycle lane, parking lane, parkstrip (3-5' width), sidewalk.

The parkstrip conditions found along Daybreak Parkway are fairly consistent, as shown by the inventory key graphic below. Most of the streetscape is not maintained by the city, meaning that many parkstrips are maintained by the owner, or have not yet been completed due to current or future construction. However, the parkstrips, roundabouts, and center medians along Daybreak Parkway are relatively cohesive.

Vegetation in the parkstrips range from grass or grass with trees. The majority of trees along Daybreak Parkway are young and small, due to the area have been more recently developed.

ANALYSIS

Currently, Daybreak Parkway has parkstrips and center medians. This is due to the fact that the area was developed at the same time. While the City only maintains a small portion of the streetscape along Daybreak Parkway, the standards in the area still create a consistent streetscape.

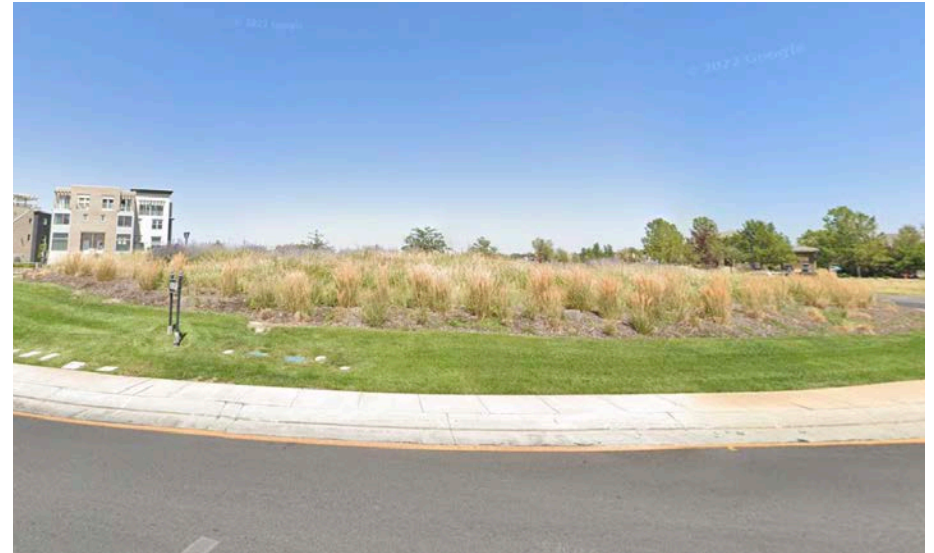
The majority of the parkstrips have either been completed or are near completion. The sidewalks in the area are at times far beyond the parkstrip and are meandering, separated by a berm and large expanse of lawn and other vegetation.

The fact that a majority of parkstrips contain lawn as a ground cover is also a concern. The new city code states that lawn should not be installed in parkstrips. Water conservation is becoming an increasingly important issue for the city and the state as a whole. Replacing legacy lawn should be prioritized.

DAYBREAK PARKWAY PHOTOS



Example of a large center median landscaped with native grass landscape



Example of roundabout



Example of curb and gutter without sidewalk or any streetscape improvements



Example of a streetscape with large landscaped area and meandering walking trail beyond

LAKE AVENUE: INVENTORY AND ANALYSIS

EXISTING CONDITIONS

Lake Avenue is categorized by the South Jordan Transportation Master Plan (SJTMP) as a minor collector with 2 lanes, Daybreak arterial with 4 lanes, and a major collector with 2 lanes. A typical cross section of the street ROW includes sidewalk, parkstrip (3-5' width), parking lane, driving lane, driving lane, parking lane, parkstrip (3-5' width), sidewalk.

The parkstrip conditions found along Lake Avenue are consistent, as shown by the inventory key graphic below. While a large portion of the streetscape is not maintained by the city, meaning that many parkstrips are maintained by the owner, all of the parkstrips are consistent.

Vegetation in the parkstrips ranges from grass or grass with trees. The parkstrips that contain trees and lawn, are a majority of young and small trees.

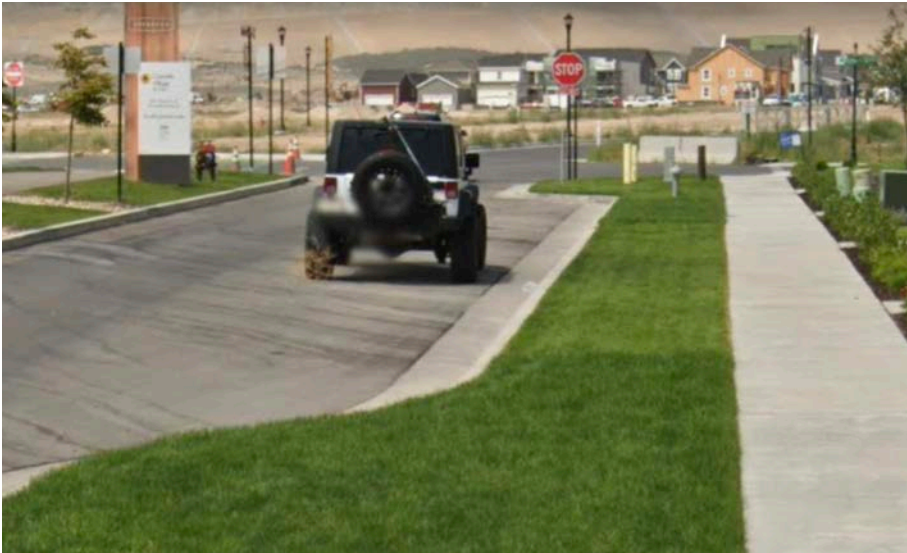
ANALYSIS

Currently, Lake Avenue has parkstrip conditions that are cohesive. Even though the City only maintains a small percentage of the parkstrips on this street, there is still consistency between the City maintained parkstrips and the parkstrips maintained by property owners.

The majority of the city maintained parkstrips are lawn with trees. In addition, tree type and size are consistent throughout Lake Avenue. The roundabouts and other streetscape amenities are also cohesive with the area.

The fact that a majority of parkstrips contain lawn as a ground cover is also a concern. The new city code states that lawn should not be installed in parkstrips. Water conservation is becoming an increasingly important issue for the city and the state as a whole. Replacing legacy lawn should be prioritized.

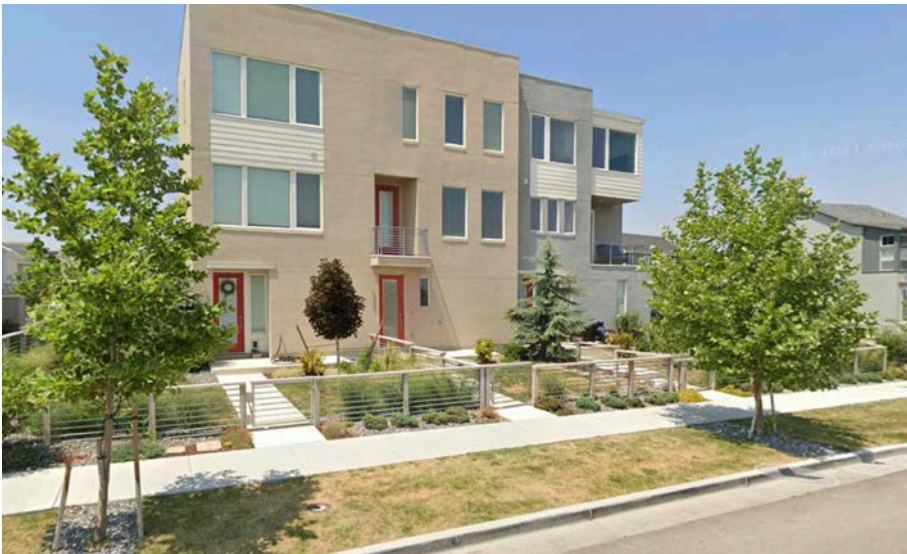
LAKE AVENUE PHOTOS



Example of a sidewalk with a wide grassy parkstrip



Example of sidewalk at Lake Avenue intersection with crosswalk and landscaping



Example of sidewalk and parkstrip with lawn and street trees



Example of sidewalk with parkstrip, landscaping, and benches

4800 WEST: INVENTORY AND ANALYSIS

EXISTING CONDITIONS

4800 West is categorized by the South Jordan Transportation Master Plan (SJTMP) as a minor collector with 3 lanes. A typical cross section of the street ROW includes sidewalk, parkstrip (3-5' width), parking lane, bicycle lane, driving lane, center lane, driving lane, bicycle lane, parking lane, parkstrip (3-5' width), sidewalk.

There are a variety of parkstrip conditions found along 4800 West, as shown by the inventory key graphic below. A vast majority of the streetscape is maintained by the City, meaning that there are only a very few parkstrips that are maintained by the owner. The City maintained parkstrips are grouped together. Most of the parkstrips are narrower than the 6' wide city standard.

Vegetation in the parkstrips range from grass or grass with trees, to stamped concrete. Some parkstrips contain mature trees, and some are young and small, along 4800 West.

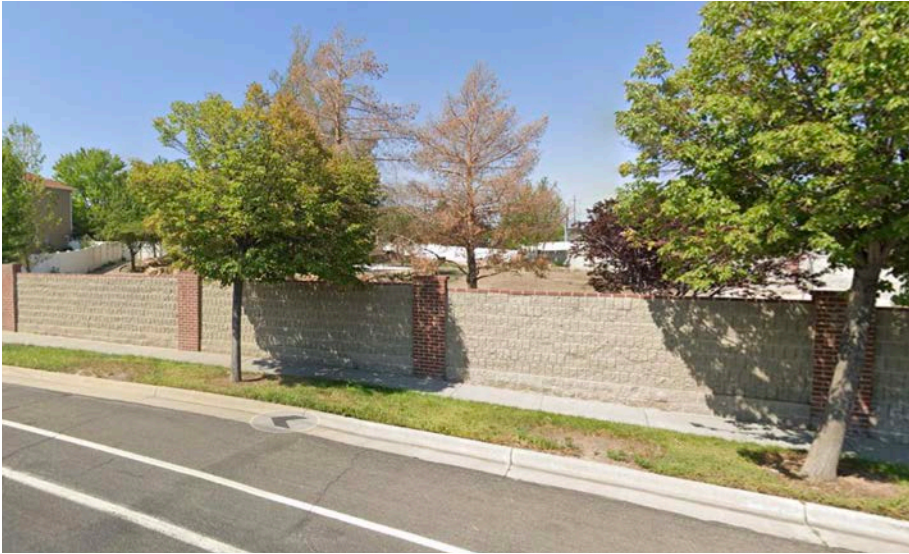
ANALYSIS

Currently, 4800 West has cohesive parkstrip conditions. This is due to the fact that the City maintains a large percentage of the parkstrips along this street. While the majority of the city maintained parkstrips are lawn with trees, this is not always the case. In some cases, the parkstrips are completely vegetated, but in other cases the parkstrips are stamped concrete.

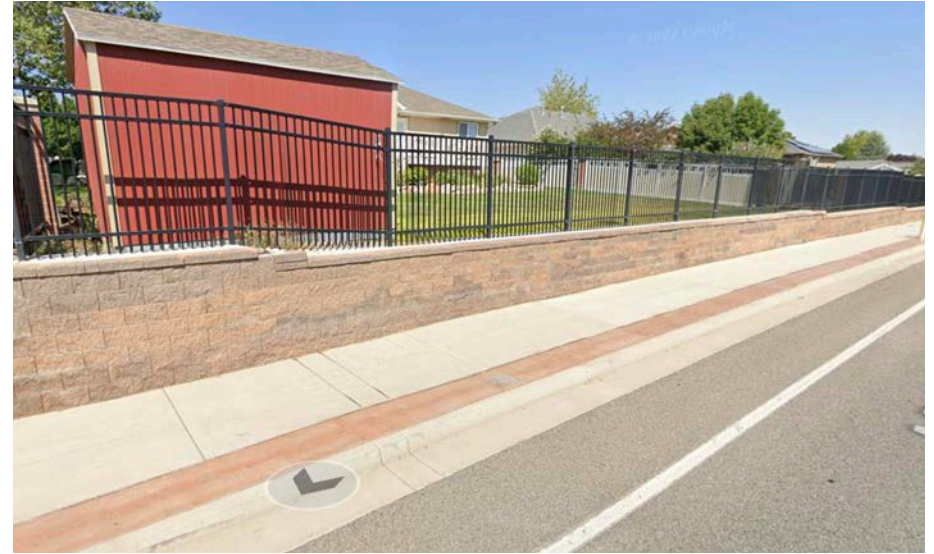
Lastly, the few privately maintained parkstrips have no consistency, as they are not forced to follow any design standards. The result is a street with no identity or character.

The fact that a majority of parkstrips contain lawn as a ground cover is also a concern. The new city code states that lawn should not be installed in parkstrips. Water conservation is becoming an increasingly important issue for the city and the state as a whole. Replacing legacy lawn should be prioritized.

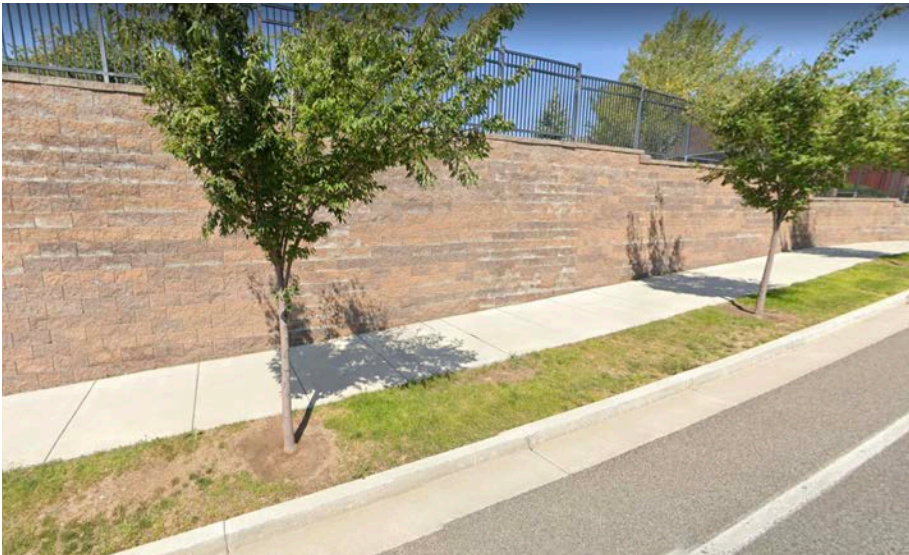
4800 WEST PHOTOS



Example of a sidewalk with a parkstrip that includes mature trees and lawn



Example of a sidewalk with retaining wall



Example of sidewalk and improved parkstrip with retaining wall and young trees



Example of sidewalk with lawn and street trees as parkstrip

RIVER HEIGHTS DRIVE: INVENTORY AND ANALYSIS

EXISTING CONDITIONS

River Heights Drive is categorized by the South Jordan Transportation Master Plan (SJTMP) as a major collector with 3 lanes. A typical cross section of the street ROW includes sidewalk, parkstrip (3-5' width), parking lane, bicycle lane, driving lane, center lane, driving lane, bicycle lane, parking lane, parkstrip (3-5' width), sidewalk.

There are a variety of parkstrip conditions found along River Heights Drive, as shown by the inventory key graphic below. Almost the entirety of the streetscape is maintained by the City, meaning that there are only a very few parkstrips that are maintained by the owner. The City maintained parkstrips are grouped together. Most of the parkstrips are narrower than the 6' wide city standard.

Vegetation in the parkstrips range from grass or grass with trees, to stamped concrete. Some parkstrips contain mature trees, and some are young and small, along River Heights Drive.

ANALYSIS

Currently, River Heights Drive has relatively cohesive parkstrip conditions. This is due to the fact that the City maintains a large percentage of the parkstrips along this street. While the majority of the city maintained parkstrips are lawn with trees, this is not always the case. In some cases, the parkstrips are completely vegetated, but in other cases the parkstrips are gravel with street trees.

The fact that a majority of parkstrips contain lawn as a ground cover is also a concern. The new city code states that lawn should not be installed in parkstrips. Water conservation is becoming an increasingly important issue for the city and the state as a whole. Replacing legacy lawn should be prioritized.

RIVER HEIGHTS DRIVE PHOTOS



Example of sidewalk and parkstrip with lawn



Example of sidewalk and parkstrip with gravel, mature street trees, and street lights



Example of sidewalk and parkstrip with lawn



Example of sidewalk with young trees and lawn in parkstrip

4000 WEST: INVENTORY AND ANALYSIS

EXISTING CONDITIONS

4000 West is categorized by the South Jordan Transportation Master Plan (SJTMP) as a major collector with 3 lanes north of 11400 South, and a major collector with 5 lanes south of 11400 South. A typical cross section of the street ROW includes sidewalk, parkstrip (3-5' width), parking lane, bicycle lane, driving lanes, center lane (or landscape median), driving lane, bicycle lane, parking lane, parkstrip (3-5' width), sidewalk.

The parkstrip conditions found along 4000 West are fairly inconsistent, as shown by the inventory key graphic below. A portion of the streetscape is not maintained by the City, meaning that many parkstrips are maintained by the owner, or have not yet been completed due to current or future construction. However, the parkstrips, along 4000 West are relatively cohesive.

Vegetation in the parkstrips range from grass or grass with trees. The majority of trees along 4000 West are mature.

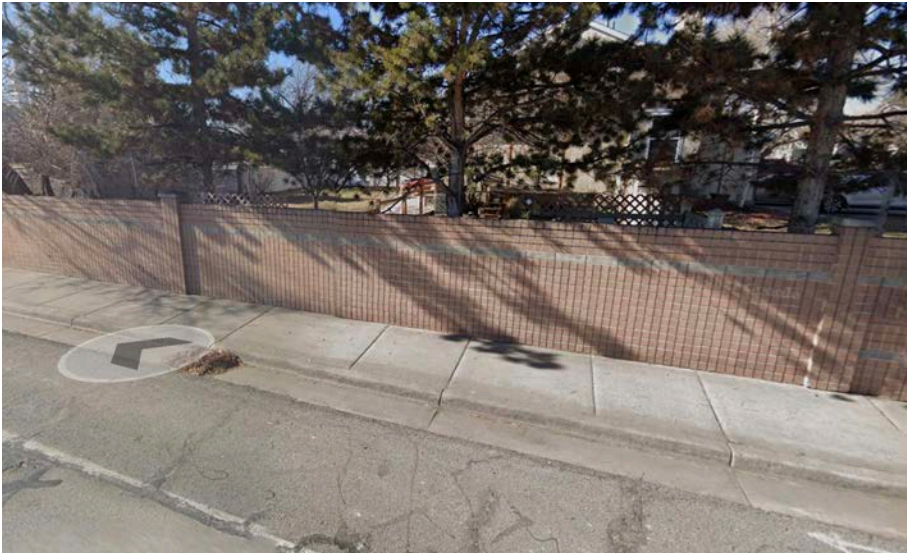
ANALYSIS

Currently, 4000 West has parkstrips along a large percentage of the street. This is due to the City maintaining a large majority of the parkstrips. Some areas that are maintained by the City do not have parkstrips. Some parkstrips are just improved with lawn, and others have lawn and mature trees.

Lastly, the few privately maintained parkstrips have no consistency, as they are not forced to follow any design standards. The result is a street with no identity or character.

The fact that a majority of parkstrips contain lawn as a ground cover is also a concern. The new city code states that lawn should not be installed in parkstrips. Water conservation is becoming an increasingly important issue for the city and the state as a whole. Replacing legacy lawn should be prioritized.

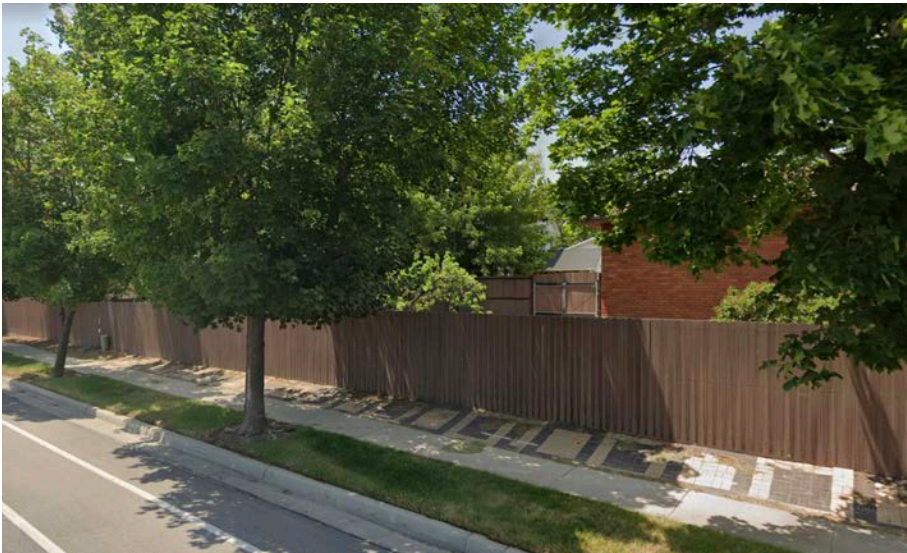
4000 WEST PHOTOS



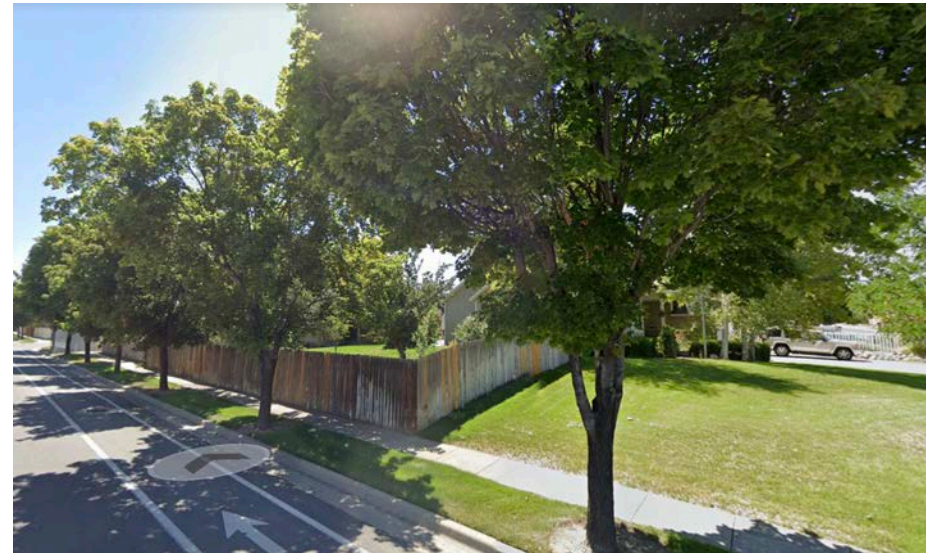
Example of a sidewalk with a sound wall



Example of sidewalk with grassy parkstrip



Example of sidewalk and parkstrip with lawn and street trees



Example of sidewalk with mature trees and lawn in parkstrip

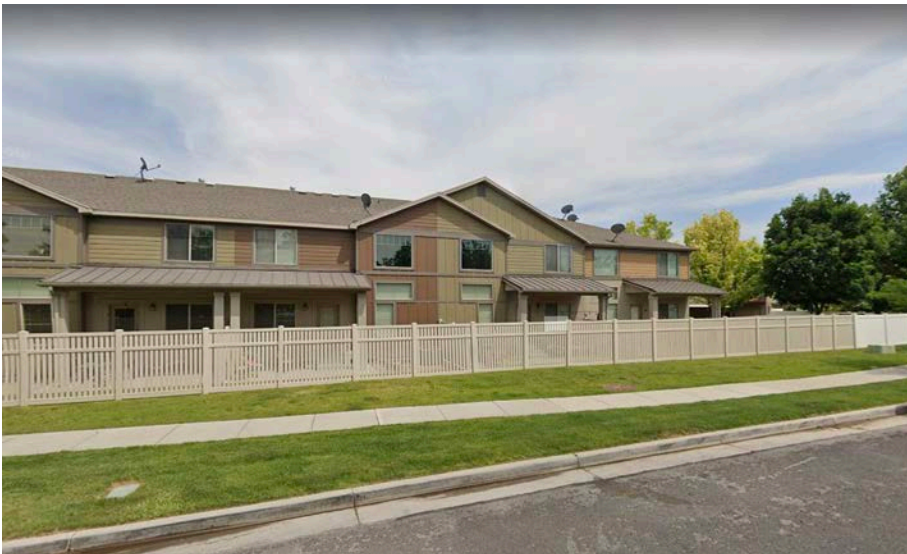
4000 WEST PHOTOS



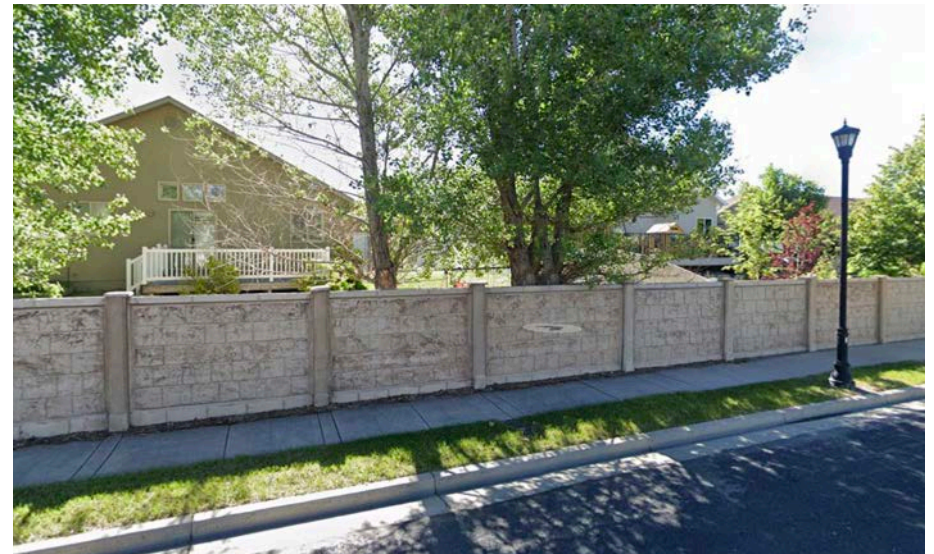
Example of a sidewalk with a parkstrip that includes street trees and grass



Example of a sidewalk with parkstrip containing young street trees and lawn



Example of sidewalk parkstrip improved with lawn on both sides



Example of sidewalk with lawn and street lighting in parkstrip

3200 WEST: INVENTORY AND ANALYSIS

EXISTING CONDITIONS

3200 West is categorized by the South Jordan Transportation Master Plan (SJTMP) as a minor collector with 2 lanes. A typical cross section of the street ROW includes sidewalk, parkstrip (3-5' width), parking lane, bicycle lane, driving lane, center lane, driving lane, bicycle lane, parking lane, parkstrip (3-5' width), sidewalk.

There are a variety of parkstrip conditions found along 3200 West, as shown by the inventory key graphic below. Large segments of the streetscape are not maintained by the city, meaning that many parkstrips are maintained by the Owner. The city-maintained parkstrips are a bit scattered and inconsistent. Most of the parkstrips are narrower than the 6' wide city standard.

Vegetation in the parkstrips range from grass or grass with trees, to gravel mulch with a mix of plantings (trees, shrubs, perennials and grasses). The majority of trees along 3200 West are mature.

ANALYSIS

Currently, 3200 West has a patchwork of parkstrip conditions that lack some cohesion. Some of this is due to the fact that the City only maintains a percentage of the parkstrips on this street, but the lack of consistency is also found between the City maintained parkstrips themselves. While the majority of the City maintained parkstrips are lawn with trees, this is not always the case. In some cases, the parkstrips are completely vegetated, but in other cases the parkstrips alternate between vegetation and gravel. In addition, tree type and size vary greatly, and some parkstrips have no trees at all.

Lastly, the privately maintained parkstrips have no consistency at all, as they are not forced to follow any design standards. The result is a street with no identity or character.

The fact that a majority of parkstrips contain lawn as a ground cover is also a concern. The new city code states that lawn should not be installed in parkstrips. Water conservation is becoming an increasingly important issue for the city and the state as a whole. Replacing legacy lawn should be prioritized.

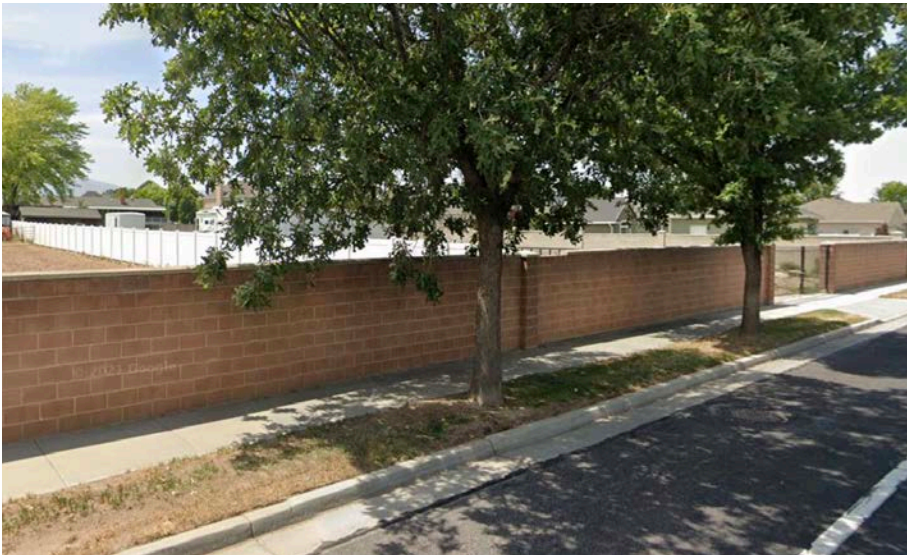
3200 WEST PHOTOS



Example of a sidewalk with a parkstrip with lawn



Example of a sidewalk with retaining wall with lawn and very mature street trees in parkstrip



Example of sidewalk and sidewalk with street trees and poorly maintained lawn



Example of sidewalk without parkstrip

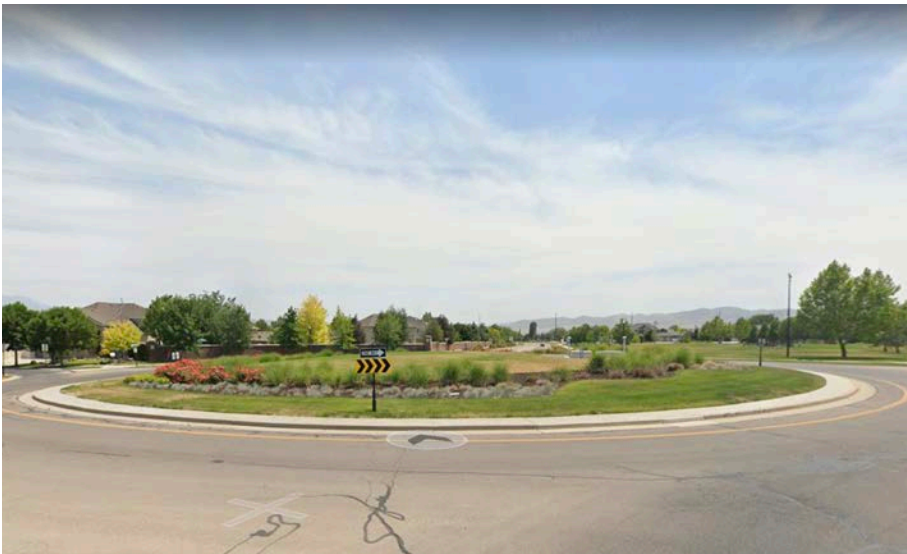
3200 WEST PHOTOS



Example of sidewalk and parkstrip with lawn on both sides and street lighting



Example of sidewalk and parkstrip with street trees and alternating grass and concrete



Example of heavily landscaped roundabout



Example of sidewalk and extra wide parkstrip with lawn and landscaping with rock

2700 WEST: INVENTORY AND ANALYSIS

EXISTING CONDITIONS

2700 West is categorized by the South Jordan Transportation Master Plan (SJTMP) as a minor collector with 3 lanes. A typical cross section of the street ROW includes sidewalk, parkstrip (3-5' width), parking lane, bicycle lane, driving lane, center lane, driving lane, bicycle lane, parking lane, parkstrip (3-5' width), sidewalk.

There are a variety of parkstrip conditions found along 2700 West, as shown by the inventory key graphic below. Large segments of the streetscape are maintained by the City, meaning that just a few parkstrips are maintained by the Owner. The City-maintained parkstrips are relatively consistent. Most of the parkstrips are narrower than the 6' wide city standard.

Vegetation in the parkstrips range from grass or grass with trees, to trees and concrete. The majority of trees along 2700 West are mature.

ANALYSIS

Currently, 2700 West has a patchwork of parkstrip conditions that lack some cohesion. Some of this is due to the fact that the City only maintains a percentage of the parkstrips on this street, but the lack of consistency is mostly found between the City maintained parkstrips themselves. While the majority of the City maintained parkstrips are lawn with trees, this is not always the case. In some cases, the parkstrips are completely vegetated, but in other cases the parkstrips alternate between vegetation and concrete. In addition, tree type varies greatly, and some parkstrips have no trees at all.

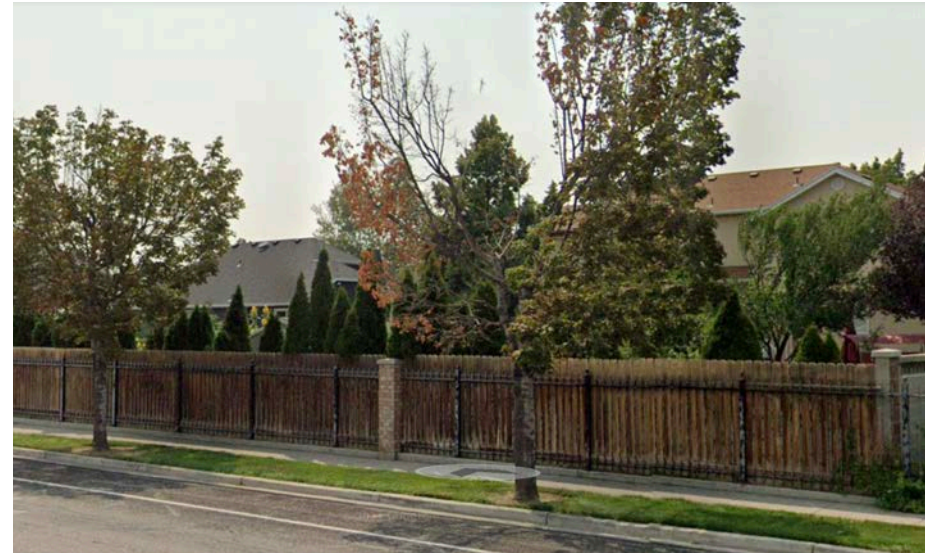
Lastly, the privately maintained parkstrips have no consistency at all, as they are not forced to follow any design standards. The result is a street with no identity or character.

The fact that a majority of parkstrips contain lawn as a ground cover is also a concern. The new city code states that lawn should not be installed in parkstrips. Water conservation is becoming an increasingly important issue for the city and the state as a whole. Replacing legacy lawn should be prioritized.

2700 WEST PHOTOS



Example of sidewalk and parkstrip with lawn



Example of sidewalk and parkstrip with mature trees and lawn

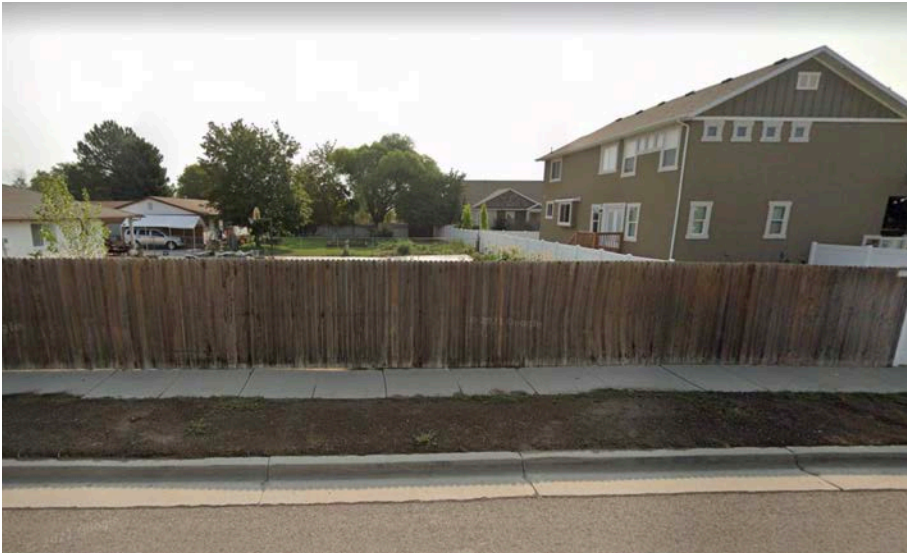


Example of sidewalk with sound wall and park strip with lawn and street trees



Example of sidewalk and concrete parkstrip with young street trees

2700 WEST PHOTOS



Example of sidewalk with unfinished parkstrip



Example of sidewalk with stamped concrete parkstrip



Example of sidewalk with poorly maintained grass parkstrip



Example of sidewalk and parkstrip with lawn

2200 WEST: INVENTORY AND ANALYSIS

EXISTING CONDITIONS

2200 West is categorized by the South Jordan Transportation Master Plan (SJTMP) as a minor collector with 2 lanes. A typical cross section of the street ROW includes sidewalk, parkstrip (3-5' width), parking lane, driving lane, driving lane, parking lane, parkstrip (3-5' width), sidewalk.

There are a variety of parkstrip conditions found along 2200 West, as shown by the inventory key graphic below. Large segments of the streetscape are not maintained by the City, meaning that many parkstrips are maintained by the owner, and the City-maintained parkstrips are scattered and inconsistent. Most of the parkstrips are narrower than the 6' wide city standard.

Vegetation in the parkstrips range from grass or grass with trees to just dirt, or no parkstrip at all. The parkstrips contain a mix of mature trees, and young and small trees.

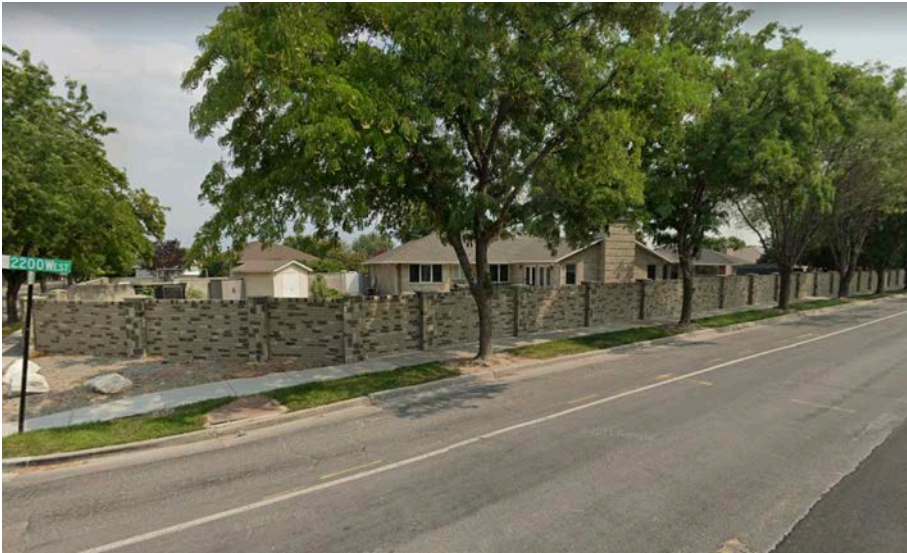
ANALYSIS

Currently, 2200 West has a patchwork of parkstrip conditions that lack any cohesion. Some of this is due to the fact that the City only maintains a small percentage of the parkstrips on this street, but the lack of consistency is also found between the City maintained parkstrips themselves. While the majority of the City maintained parkstrips are lawn with trees, this is not always the case. In some cases, the parkstrips are completely vegetated, but in other cases the parkstrips are dirt or just concrete. In addition, tree type and size vary greatly, and some parkstrips have no trees at all.

Lastly, the privately maintained parkstrips have no consistency at all, as they are not forced to follow any design standards. The result is a street with no identity or character.

The fact that a majority of parkstrips contain lawn as a ground cover is also a concern. The new city code states that lawn should not be installed in parkstrips. Water conservation is becoming an increasingly important issue for the city and the state as a whole. Replacing legacy lawn should be prioritized.

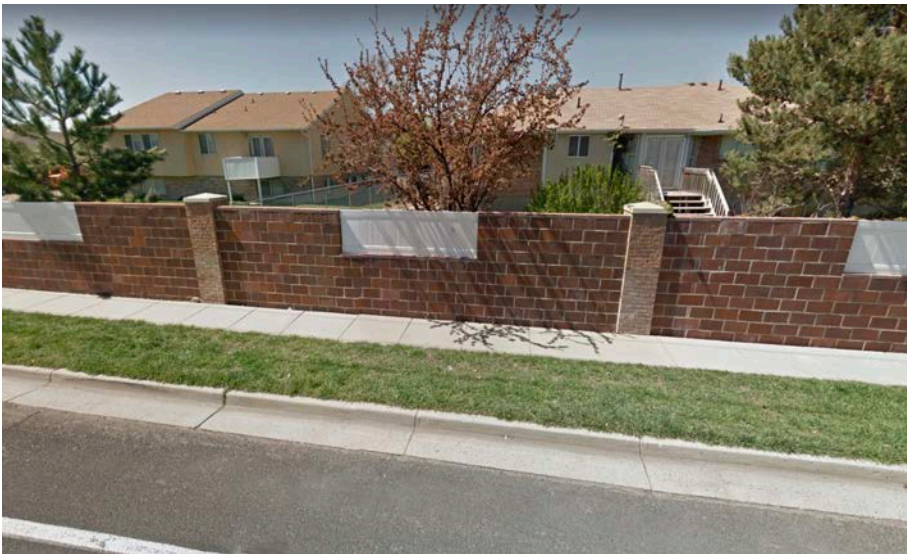
2200 WEST PHOTOS



Example of sidewalk and parkstrip with lawn and very mature trees



Example of sidewalk and parkstrip with street lighting



Example of sidewalk with well established grass parkstrip



Example of sidewalk with parkstrip and young street trees

2200 WEST PHOTOS



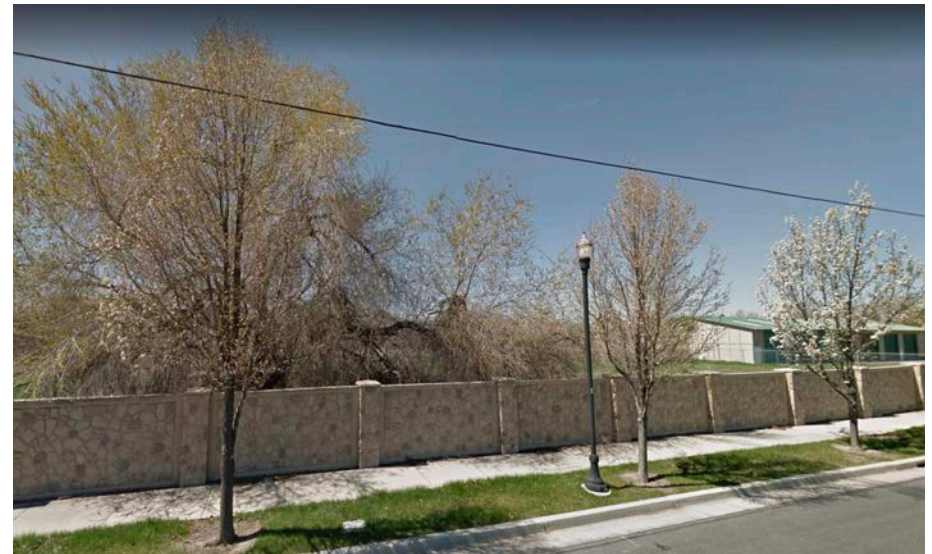
Example of concrete sidewalk with fence and curb and gutter



Example of newly laid sidewalk, young trees, and grass parkstrip



Example of sidewalk in commercial area and parkstrip with young trees and grass



Example of sidewalk, sound wall, and park strip with grass, street trees, and street lights

REDWOOD ROAD: INVENTORY AND ANALYSIS

EXISTING CONDITIONS

Redwood Road is categorized by the South Jordan Transportation Master Plan (SJTMP) as an arterial with 5 lanes. A typical cross section of the street ROW includes sidewalk, parkstrip (3-5' width), parking lane, bicycle lane, driving lanes, center lane, driving lanes, bicycle lane, parking lane, parkstrip (3-5' width), sidewalk.

There are a variety of parkstrip conditions found along Redwood Road, as shown by the inventory key graphic below. Large segments of the streetscape are maintained by the City, meaning that just a few parkstrips are maintained by the Owner. The City-maintained parkstrips are relatively consistent. Most of the parkstrips are narrower than the 6' wide city standard.

Vegetation in the parkstrips range from grass or grass with trees, to trees and concrete, to gravel mulch and trees. The trees along Redwood Road are a combination of young and mature.

ANALYSIS

Currently, Redwood Road has a patchwork of parkstrip conditions that lack some cohesion. Some of this is due to the fact that the City does not maintain the entirety of the parkstrips on this street, but the lack of consistency is mostly found between the City maintained parkstrips themselves. While the majority of the City maintained parkstrips are lawn with trees, this is not always the case. In some cases, the parkstrips are completely vegetated, but in other cases the parkstrips alternate between vegetation and concrete. In addition, tree type varies greatly, and some parkstrips have no trees at all.

Lastly, the privately maintained parkstrips have no consistency at all, as they are not forced to follow any design standards. The result is a street with no identity or character.

The fact that a majority of parkstrips contain lawn as a ground cover is also a concern. The new city code states that lawn should not be installed in parkstrips. Water conservation is becoming an increasingly important issue for the city and the state as a whole. Replacing legacy lawn should be prioritized.

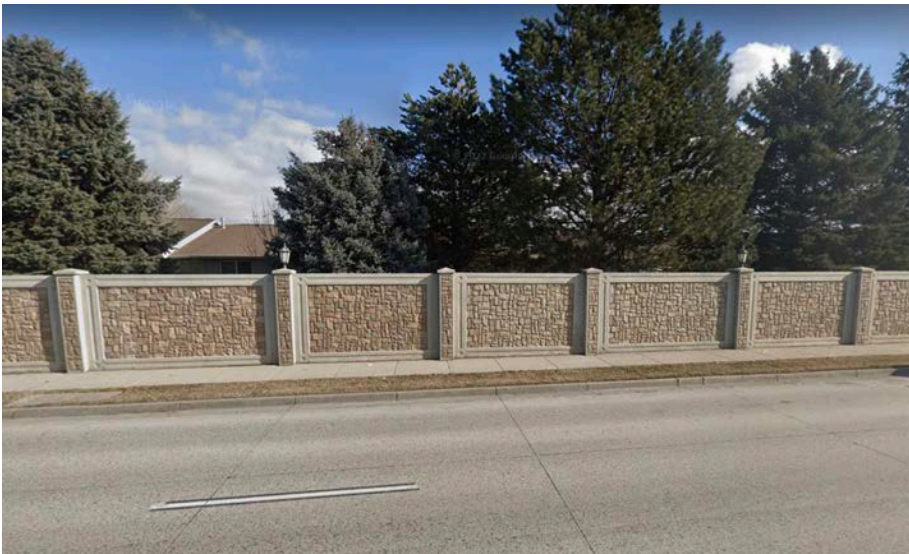
REDWOOD ROAD PHOTOS



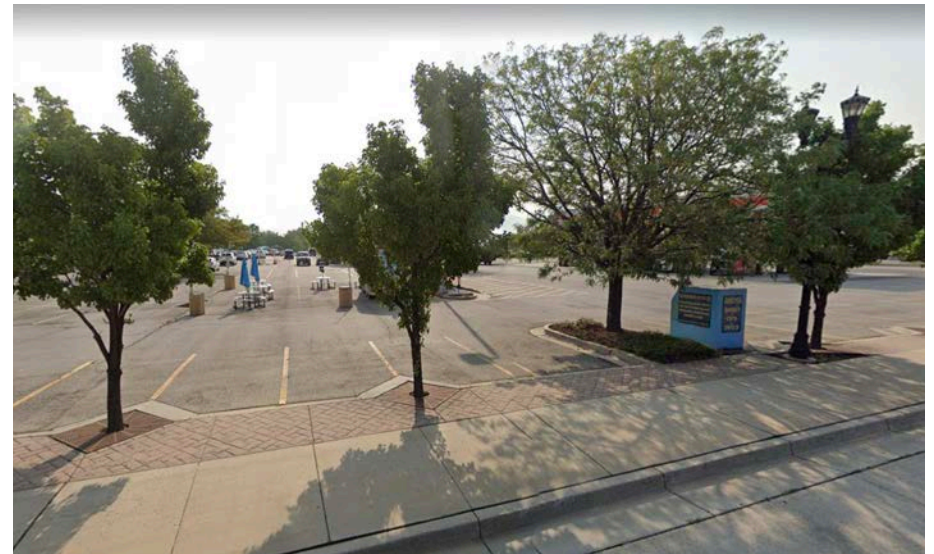
Example of narrow sidewalk and parkstrip with street lighting, grass, and power poles



Example of sidewalk and gravel parkstrip



Example narrow sidewalk, sound wall, and parkstrip with lawn



Example of sidewalk with stamped concrete and mature trees behind

REDWOOD ROAD PHOTOS



Example of a sidewalk, grassy parkstrip, and lawn with trees beyond



Example of sidewalk and unfinished parkstrip with street lighting



Example of sidewalk, parkstrip with grass and street light, and street trees behind



Example of sidewalk and parkstrip with gravel and young street trees

BECKSTEAD LANE: INVENTORY AND ANALYSIS

EXISTING CONDITIONS

Beckstead Lane is categorized by the South Jordan Transportation Master Plan (SJTMP) as other. A typical cross section of the street ROW includes sidewalk, parkstrip (3-5' width), driving lane, center median, driving lane, parkstrip (3-5' width), sidewalk.

There are a variety of parkstrip conditions found along Beckstead Lane, as shown by the inventory key graphic below. A considerable number of segments of the streetscape are not maintained by the City, meaning that just many parkstrips are maintained by the Owner. The City-maintained parkstrips are relatively consistent but have some variation. Most of the parkstrips are narrower than the 6' wide city standard.

Vegetation in the parkstrips range from grass or grass with trees, to trees and concrete, to gravel mulch and trees. The trees along Beckstead Lane are a largely mature, however some are young and Small.

ANALYSIS

Currently, Beckstead Lane has a patchwork of parkstrip conditions that lack cohesion. Some of this is due to the fact that the City does not maintain the entirety of the parkstrips on this street, but the lack of consistency is also found between the City maintained parkstrips themselves. While the majority of the City maintained parkstrips are lawn with trees, this is not always the case. In some cases, the parkstrips contain grass and trees, but in other cases the parkstrips contain just grass. In addition, tree type varies greatly, and some parkstrips have no trees at all.

Lastly, the privately maintained parkstrips have no consistency at all, as they are not forced to follow any design standards. The result is a street with no identity or character.

The fact that a majority of parkstrips contain lawn as a ground cover is also a concern. The new city code states that lawn should not be installed in parkstrips. Water conservation is becoming an increasingly important issue for the city and the state as a whole. Replacing legacy lawn should be prioritized.

BECKSTEAD LANE PHOTOS



Example of sidewalk and parkstrip with healthy lawn and street light



Example of center median with landscaping and small street trees



Example of sidewalk and parkstrip with lawn on both sides and street lighting



Example of sidewalk, sound wall, and parkstrip with lawn and street trees

1000 WEST: INVENTORY AND ANALYSIS

EXISTING CONDITIONS

1000 West is categorized by the South Jordan Transportation Master Plan (SJTMP) as a minor collector with 2 lanes. A typical cross section of the street ROW includes sidewalk, parkstrip (3-5' width), driving lane, driving lane, parkstrip (3-5' width), sidewalk.

The parkstrip conditions found along 1000 West are consistent, as shown by the inventory key graphic below. Large segments of the streetscape are not maintained by the City, meaning that many parkstrips are maintained by the owner, and the City-maintained parkstrips are scattered and inconsistent. Most of the parkstrips are narrower than the 6' wide city standard.

Vegetation in the parkstrips contain grass or grass with trees. Some parkstrips contain mature trees, and others are young and small.

ANALYSIS

Currently, 1000 West has a patchwork of parkstrip conditions that lack any cohesion. This is due to the fact that the city only maintains a small percentage of the parkstrips on this street. The majority of the city maintained parkstrips are lawn with trees.

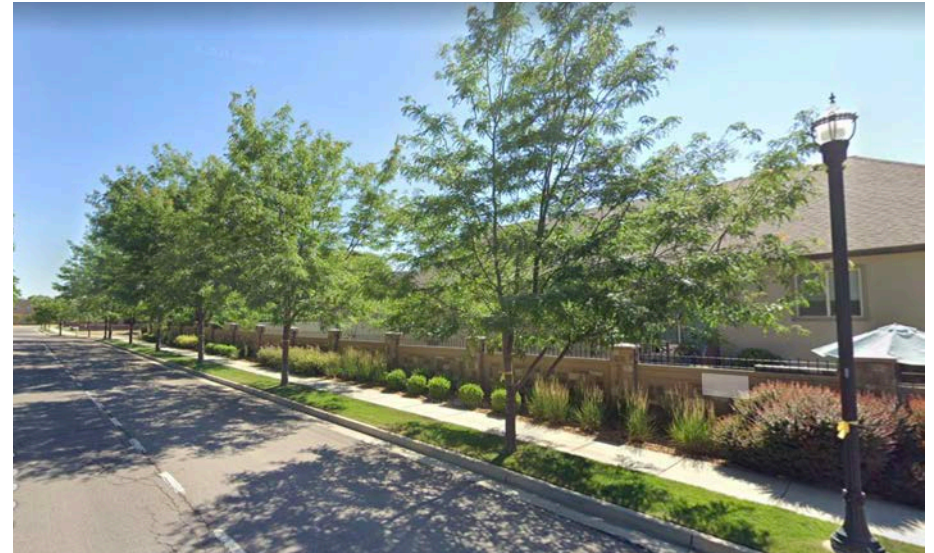
Lastly, the privately maintained parkstrips have no consistency at all, as they are not forced to follow any design standards. The result is a street with no identity or character.

The fact that a majority of parkstrips contain lawn as a ground cover is also a concern. The new city code states that lawn should not be installed in parkstrips. Water conservation is becoming an increasingly important issue for the city and the state as a whole. Replacing legacy lawn should be prioritized.

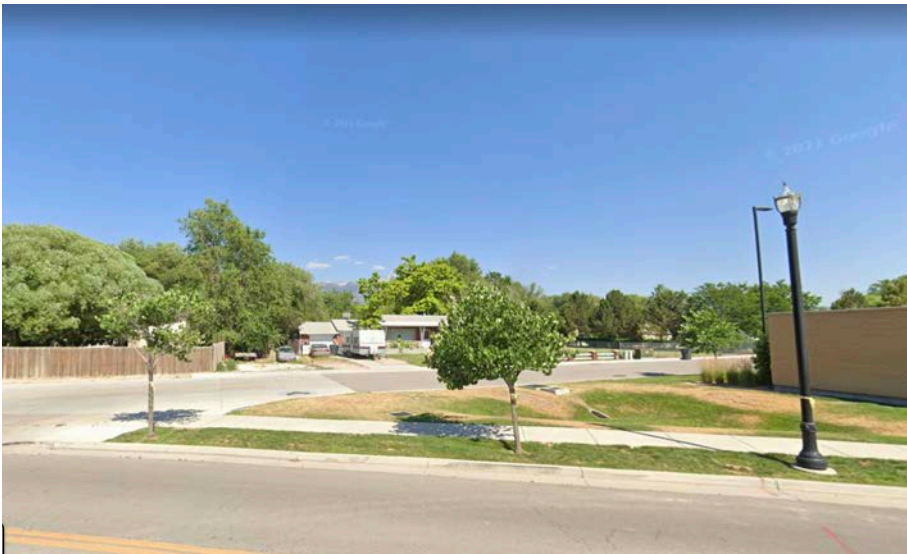
1000 WEST PHOTOS



Example of narrow sidewalk, sound wall, and parkstrip with mature street trees and street lighting



Example of sidewalk, parkstrip with lawn, trees, and lighting and landscaped area behind



Example of sidewalk and parkstrip with small street trees and lighting



Example of sidewalk and parkstrip with new street trees