

CITY OF SOUTH JORDAN
PLANNING COMMISSION MEETING AGENDA
CITY COUNCIL CHAMBERS
TUESDAY, JUNE 24, 2025 at 6:30 PM



Notice is hereby given that the South Jordan City Planning Commission will hold a Planning Commission Meeting on Tuesday, June 24, 2025, in the City Council Chambers, located at 1600 W. Towne Center Drive, South Jordan, Utah with an electronic option via Zoom phone and video conferencing. Persons with disabilities who may need assistance should contact the City Recorder at least 24 hours prior to this meeting.

In addition to in-person attendance, individuals may join via phone or video using Zoom. Please note that attendees joining virtually or by phone may not comment during public comment or a public hearing; to comment, individuals must attend in person.

If the Meeting is disrupted in any way that the City in its sole discretion deems inappropriate, the City reserves the right to immediately remove the individual(s) from the Meeting and, if needed, end virtual access to the Meeting. Reasons for removing an individual or ending virtual access to the Meeting include, but are not limited to, the posting of offensive pictures, remarks, or making offensive statements, disrespectful statements or actions, and any other action deemed inappropriate. The ability to participate virtually is dependent on an individual's internet connection.

To ensure comments are received, please have them submitted in writing to City Planner, Greg Schindler, at gschindler@sjc.utah.gov by 12:00 p.m. on the day of the meeting.

Instructions on how to join the meeting virtually are below.

Join South Jordan Planning Commission Electronic Meeting June 24, 2025 at 6:30 p.m.

- Join on any device that has internet capability.
- Zoom link, Meeting ID and Meeting Password will be provided 24 hours prior to meeting start time.
- Zoom instructions are posted <https://www.sjc.utah.gov/254/Planning-Commission>

THE MEETING WILL BEGIN AT 6:30 P.M. AND THE AGENDA IS AS FOLLOWS:

- A. **WELCOME AND ROLL CALL – Commission Chair Nathan Gedge**
- B. **MOTION TO APPROVE AGENDA**
- C. **APPROVAL OF THE MINUTES**
 - [C.1.](#) June 10, 2025 - Planning Commission Meeting Minutes
- D. **STAFF BUSINESS**
- E. **COMMENTS FROM PLANNING COMMISSION MEMBERS**
- F. **SUMMARY ACTION**
- G. **ACTION**

H. ADMINISTRATIVE PUBLIC HEARINGS

H.1. DAYBREAK VILLAGE 11B PLAT 2

Address: Generally located on the west side of Bingham Rim Road between 11800 S and Meadow Grass Drive
File No: PLPP202500089
Applicant: Perigee Consulting on behalf of LHM Real Estate

H.2. DAYBREAK SOUTH STATION PLAT 5

Address: Generally located at 11234 S Grandville Avenue
File No: PLPP202500034
Applicant: Perigee Consulting on behalf of LHM Real Estate

H.3. SOJO OFFICE CONDOMINIUM PRELIMINARY SUBDIVISION

Address: 10509 S River Heights Dr.
File No: PLPP202500055
Applicant: Zach Olson

I. LEGISLATIVE PUBLIC HEARINGS

I.1. FRONTRUNNER STATION AREA PLAN

File No: Resolution R2025-24

J. OTHER BUSINESS

J.1. Planning Commission Discussion/Adoption regarding Commission Rules for 2025.

ADJOURNMENT

CERTIFICATE OF POSTING

STATE OF UTAH)

: §

COUNTY OF SALT LAKE)

I, Cindy Valdez, certify that I am the duly appointed City Deputy Recorder of South Jordan City, State of Utah, and that the foregoing Planning Commission Agenda was faxed or emailed to the media at least 24 hours prior to such meeting, specifically the Deseret News, Salt Lake Tribune and the South Valley Journal. The Agenda was also posted at City Hall, on the City's website www.sjc.utah.gov and on the Utah Public Notice Website www.pmn.utah.gov.

Dated this 19th day of June, 2025.
Cindy Valdez
South Jordan City Deputy Recorder

**CITY OF SOUTH JORDAN
PLANNING COMMISSION MEETING
COUNCIL CHAMBERS
June 10, 2025**

Present: Chair Nathan Gedge, Commissioner Michele Hollist, Commissioner Laurel, Commissioner Bevens, Steven Catmull, Assistant City Attorney Greg Simonson, City Planner Greg Schindler, Planner Damir Drozdek, Associate Engineer Jared Francis, Deputy Recorder Cindy Valdez, Deputy City Engineer Jeremy Nielsen, IT Director Matt Davis, GIS Coordinator Matt Jarman

Absent: Commissioner Lori Harding, Commissioner Sam Bishop

**6:32 P.M.
REGULAR MEETING**

A. WELCOME AND ROLL CALL –Chair Nathan Gedge

Chair Gedge welcomed everyone to the Planning Commission Meeting and noted that (4) of the Planning Commissioner's are present. Commissioner Bishop and Commissioner Harding are excused from tonight's meeting.

B. MOTION TO APPROVE AGENDA

Commissioner Hollist motioned to approve the June 10, 2025 Planning Commission Agenda. Chair Gedge seconded the motion. Vote was 4-0; Commissioner Bishop and Commissioner Harding were absent from the vote.

C. APPROVAL OF THE MINUTES

C.1. Approval of the May 27, 2025 - Planning Commission Meeting Minutes.

Commissioner Bevens motioned to approve the May 27, 2025 Planning Minutes with corrections. Commissioner Gedge seconded the motion. Vote was 4-0; Commissioner Bishop and Commissioner Harding were absent from the vote.

D. STAFF BUSINESS

Director Brian Preece – (Attachment A) said with the subject at last meeting with the weaver application the planning commission made that motion subject to the all legal requirements for the location. As I reviewed that under the authority granted to me by the ordinance, I determined that it was in the front yard and was not able to be built there. I issued the report, and then sent it to the planning commission members and to the Weavers. And so if you have any questions, I am happy to answer those. The letter will be attached to the these minutes, and that will clarify everything.

Chair Gedge I said really appreciate the detailed list of your memo lining that up. It really helped us understand.

Director Preece said it is subject to appeal, so the Weavers, or any other party with standing could appeal it 10 days after yesterday's date.

Commissioner Bevans said I just want to clarify that in the memo, it sounds like our ruling on the extra square footage stands as long as they put it somewhere appropriate.

Director Preece said I saw no reason to have to come re-hear that if they find another location that meets the requirements.

Commissioner Hollist said I have another question on this particular item. We were told by staff that this had been reviewed and found acceptable. On what grounds do we now review it and find it unacceptable?

Assistant City Attorney Simonson said under our code, the planning director makes the final interpretation of the code, and we've had a change in planning directors. I haven't talked to him about the details, but he obviously he felt that it was legal, and it came true to you that way. Director Preece has reviewed it, and he holds that mantle now, and I find his logic solid, and so that's that explains the change.

Director Preece said I just want to add for education purposes, had I determined the other direction, someone else could have appealed it as well. It's appealable either way, one of the neighbors or something could have appealed it.

Commissioner Hollist said I have a tiny bit of heartburn with respect to this, simply because it sounds like we made an appropriate decision. At what point is that decision and that review final? Is it because now we've gone through it and we asked city to review, and the person with that authority determined that it no longer complies, or it did not actually comply?

Assistant City Attorney Simonson said in this circumstance, I think the Planning Commission left the door open. There was enough concern after the response that we were going to review this for legality on that very issue, and and that's exactly what Director Preece did.

Director Preece said it is little unprecedented that there was a change in between, but as we discussed it, I was the one that was in place when you made that motion.

E. COMMENTS FROM PLANNING COMMISSION MEMBERS

Chair Gedge said I'll just say, the city staff who put together the volunteers for the social Summer Fest this past weekend, it was a well organized event. When I attended, it was well run so good job on the city. On our next meeting in two weeks, I'll be in attendance so, let us know for sure if you will be in attendance so we will have a quorum.

Commissioner Bevans said I will be in attendance and it will be my last meeting.

F. SUMMARY ACTION

G. ACTION

H. ADMINISTRATIVE PUBLIC HEARINGS

H.1. CABCO JORDAN SUBDIVISION AMENDED

Address: 10138 S. Jordan Gateway

File No: PLPLA202500048

Applicant: Krisel Travis, DAI Utah

Planner Damir Drozdek reviewed background information on this item from the staff report.

Commissioner Catmull said so the access is going to go right behind that building on the north, so if I'm looking at the building on the north, there's a curb and gutter that goes right to the edge of the building.

Planner Drozdek said that's not being changed, it will remain as is.

Chriselle Travis (Applicant) said I am with DAI and we are the applicant. I think Planner Drozdek did a great job reviewing the staff report. I'm happy to answer any questions you may have for me, but it's pretty straightforward of what we're trying to accomplish here. We do own the parcel, and so this will just make it a legal subdivision.

Chair Gedge opened the Public Hearing to comments. There were none. He closed the Public Hearing.

Assistant City Attorney Simonson said I just wanted to make sure that we were clear on the record. You mentioned that you had heard from some neighbors, and I want to make sure that there is not comment from neighbors that have not made it onto the record that are being considered as evidence one way or the other.

Chair Gedge said the items were about the overall Rise Development and I've directed them to the city council study session last meeting with the area plan that was discussed in the study session.

Commissioner Bevans said I don't quite know how to phrase it, but after we saw this the last time we did start to see emails and received letters from the northern property owner about their access. Does this resolve all of their concerns, and are they okay with the way this road is happening now, or are we still trying to navigate those concerns from the northern property owner.

Assistant City Attorney Simonson said there was a letter sent, I believe it was actually addressed to you the Planning Commissioner's on March 28, 2025. I sent an email back in response. I think

the basic gist of the my responding email was that you'll get all notices and you're welcome to come to the meeting, but it really didn't address the merits. If there was concerns, I have not heard anything more from that attorney or that property owner. I assume if there were concerns that they would be here at the hearing.

Commissioner Bevans said we haven't heard anything from the northern property owner pertaining to this, and they obviously were correctly noticed, but nobody has received anything for the public record.

Commissioner Hollist said will this area have on street parking, or will that be marked as a no parking section of road along Jordan gateway? I am referring to the little piece that we're talking about.

Planner Drozdek said there will not be any on street parking.

Commissioner Bevans said this section that's blue is where we're adding to it, and that is what will be used to access the Rise Development, should it continue and move forward. And the piece that's not highlighted, that is the existing road will be what the northern property owner uses to access their property.

Planner Drozdek said yes, that's correct.

Chair Hollist said I have a clarifying question, will those two things together be the road, or will there be some delineation?

Planner Drozdek said the north end will be striped, but the north parcel will not be used for access.

Associate Engineer Jared Francis said on the blue section it will have a center line going down it to delineate both travel lanes, and then on the outside edges of those lanes, and a white stripe to help direct traffic.

Chair Hollist said there will just be a white line between the northern edge of that right of way and then the northern property or businesses private access.

Associate Engineer Jared Francis said yeah, it's tracked.

Commissioner Hollist said knowing what's going in behind this and the volume of traffic, I hope that's very clearly marked. I think it could be a safety issue otherwise, especially if that northern business is traveling both directions within that that piece. Does the blue section that we're talking about conform to standard road access says standard width?

Associate Engineer Jared Francis said so the blue section will provide 210 foot travel lanes, one in each direction.

Commissioner Hollist said is that smaller than what is typically required of a road.

Associate Engineer Jared Francis said slightly, in our regular residential streets it's 28 feet, but does meet fire code which is 10 feet.

Commissioner Hollist said does staff feel that sufficient and appropriate for the size of development that we'll be using it as their only point of access?

Planner Schindler said is this the only point of access for the for the future project?

Commissioner Hollist said I misspoke, I think they may have another one too.

Associate Engineer Jared Francis said this is the second point of access, so this is the secondary access to the project.

Commissioner Bevens said is there a curb and gutter adding to that 10 feet, or is it 10 feet total? Or are we adding curb gutter onto the 10 feet on the south side.

Associate Engineer Jared Francis said there's curb and gutter on the south side.

Commissioner Hollist motioned to approve File No PLPLA202500048 CABCO Jordan Subdivision Amended. Chair Gedge seconded the motion. Roll Call Vote was 4-0 in favor; Commissioner Bishop and Commissioner Harding were absent from the vote.

H.2. UTAH BLACK DIAMONDS SITE PLAN

Address: 272 W. 11000 S.

File No: PLSPR202500059

Applicant: Ryan Naylor, Nichols Naylor Architects

Planner Damir Drozdek reviewed background information on this item from the staff report and (Attachment B)

Chair Gedge said I did see a sign and notarizing that fully executed development agreement in our packet, so it's been to city council?

Planner Drozdek said, yes.

Chair Gedge said you guys probably know why I asked that question. Can you remind us of the parking that's required for the pickleball only that we're reviewing this evening for the use, and is there a standard of parking stalls for its intended use?

Planner Drozdek said the proposed meets the code. If you remember, it's in the agreement for events which are going to bring bigger crowds, they're going to do some off site parking, and they're going to be organizing that away from here.

Commissioner Hollist said I am following up on what you just said. What is the standard parking required for a use like this? So, I saw the parking called out for the housing, but I didn't see it for this. So is it a square footage or number of courts?

Planner Drozdek said it is based on the square footage, and it's in the agreement. It's in the table that is in the agreement, I'm pretty sure there was a traffic study, and the traffic study addressed the apartments parking situation. I may have to go check, but I am pretty sure that one of the exhibits has a table that shows parking ratios. It's broken down on different traffic for tournament days.

Chair Gedge said I drive this road daily and have to cross over the Union Pacific and front runner tracks daily, and feel that my tires were going to pop every single time I drive over that crossing. Are there any plans for the Jordan Parkway road? Does UDOT or the City have any improvements? Especially where there's going to be a new development coming in right there at that corner.

Planner Drozdek said it is a city road, but we don't have any plans for improvements in this specific spot.

Russ Naylor (Nichols and Architect) said obviously, we're here just for a site plan approval, now that we have the city council's approval for the zoning and and basically the site plan. I'm just following up on what Damir said. We physically broke the building down into each one of the components and then applied what we understood the city's zoning requirement was based upon each use, so the fitness center and pickleball courts. I think it was two or three per court, allowing for some people to have four players. All that was approved by the city council, and we did go to architectural committee meeting a couple of weeks ago and presented the material to the board and so forth. We had some pretty good discussion about the materials that we were using. We don't make formal recommendations with that committee, but my general sense was that everybody looked at it favorably. A couple of things that I do want to talk about is that we mentioned off site parking, and we did present that to the city council. We've identified a dozen public facilities around that are within less than two miles that we could shuttle people from. And once we have an event schedule, then we will contact these parties and negotiate a weekend rental for parking stalls. We've identified half a dozen different common carriers that have enough buses that they would be happy to work with us and shuttle people back and forth from the facility to the off site parking. So we think we've got that in hand. The other thing is, when we came to the planning commission there were a couple of representatives here from Hewlett and Associates which is the accounting firm that owns the building right on the bend on the northeast corner of this expanded property. They indicated that they had half a dozen concerns about the development and it included the traffic and so forth. They recommended we have no parking signs on the south side all the way around on South Jordan Gateway. They had some concerns about the decorative aluminum wrought iron looking fence on the south property line, but it doesn't extend all the way across their parking on the west side. They were also concerned about people cutting through their parking lot to get to our facility. We told them that we would be willing to extend their fence so that it joins with the fence around our Pickleball Course, so people can't cut through their. Most of these were were pretty simple, but the one that I want to

talk about is they would not like us to implement the solution that would have a drive through. They indicated that they have a second level deck, and they would not like people to be parked in a drive through, pumping out exhaust. That is the only one that they proposed of the five that I don't feel like at this point we can say to just forget about a drive through on either of those uses, those pad uses on the front of the property. We haven't got a tenant, we haven't designed it, so we know that once we have something solid, then we're going to have to go back to the ARC Committee and come back to the planning commission to get those uses approved. That would give them another opportunity to come and provide public comment. So we're certainly aware of what their concerns are, and we are doing our best to try to resolve those that have big economic impact. But, we don't feel like at this point we can say, we'll give up the drive through. I guess, other than that we are hoping after we do this and subject to your approval, then that would allow us to go ahead and submit plans for a building permit. We would hope to do that just shortly after or just before the fourth of July holiday. So we are moving forward and excited about being able to get started. We did get a construction estimate and we're like \$2 million over budget, so we may be doing some value engineering and trying to reduce our costs by about 10% but we are committed to this project, so we're moving forward.

Commissioner Hollist said does development agreement require you to move forward with the pickleball facility first?

Russ Naylor said I think the way it's worded in the agreement, we have to get a building permit, pay all the permit fees and bonds and so forth, and physically start construction before we could do anything at all with the apartments. The City Council was pretty committed, it's the pickleball they want, and they'll tolerate the apartments to get the pickleball, is what it really amounted to.

Commissioner Catmull said so I haven't seen how is the signage going to happen on this building? I haven't seen that visually.

Russ Naylor said I think we did show signage on the site plan. I would anticipate we will have a sign on the building and then I'm hoping we probably would want to maybe have a monument sign out on close to the entry on South Jordan gateway.

Commissioner Catmull said I know that's not in our title 16 or title 17, as we look at the site plan, and being closer to a freeway, I was just wondering what that was going to look like. I didn't see it on any rendering, so I think okay, and it's not in the agreement.

Russ Naylor said we would still have to submit drawings and so forth, and I don't know whether that'd have to come back for that.

Planner Drozdek said it will be reviewed and approved by staff, but it wouldn't be a big sign, like one of those pylons.

Russ Naylor said we're not doing the big freeway signs or anything like that.

Chair Gedge said, as Mr. Naylor mentioned we did receive a letter from Mark Hewlett of Hewlett and Associates. They are the neighboring property. We have all received it and we have reviewed that as part of the public record. Mr. Naylor did address many of the concerns in there. I just want it to be known that it is part of the public record.

Chair Gedge opened the Public Hearing to comments.

Darren Hawes (CFO for Western App Credit) said we will also be a neighbor to Black Diamonds. We will be on the southeast corner, and we actually are in favor of this development. This piece of the land we owned and sold to the development company to do that. We are in favor of this plan as drawn, as long as the outdoor courts remain in that that far south northwest corner, then we're okay, just because concern over or sound, but that puts that away from us. We are in favor of this and look forward to a a good neighbor there, after having about 10 to 15 years of empty fields.

Chair Gedge closed the Public Hearing.

Chair Gedge said the letter from the resident brought up the sound. I know the sound might be an issue for the neighboring businesses, but from my personal observation, it's next to a front runner Union Pacific rail line, and also next to the I-15 freeway, so there's going to be noise anyway. I don't know if that's obviously a habit depth tournament that we can review, and they try to mitigate that as much as possible for the neighbors request. So I think they're being very accommodating, and of course, the one concern in the letter is the drive through that is allowed in the development agreement. And so unfortunately, landowners have rights as well, and it's been agreed to by the city council. I think the applicant has been open to the other items that have been raised. Again, my big oncern for the rezone, is event parking, especially where this is going to have a national caliber cord and broadcast facility that pickleball is very popular. I'm just still concerned about transit, public transit to get people to this event. Obviously, there's some neighboring fields to the south, but I'm just a little worried about people parking on Jordan Parkway, backing up and trying to cross 10400 S, which I believe is 45 miles an hour. And I don't think anybody, including myself goes 45 miles an hour right there getting off the freeway once the light turns green. So just some public safety concerns. So hopefully, those can be remedied for a large event. Were going to hit our second professional franchise here in South Jordan. So I hope it goes well.

Commissioner Bevans said I am excited for this and for the city. I think it's going to be a great addition. I don't have any other concerns other than parking, which we've shared multiple times, so I'm not going to go over it again, but I think it's going to be a good addition.

Commissioner Hollist said there is development agreement in place, so that's how I reviewed it, because that's now what governs this property. I just wanted to ask the staff what ensures that the pickle ball development gets completed. I'm a little concerned when I hear that it's already over budget, and is there any protection in place to ensure that it's completed?

Planner Drozdek said we wouldn't cover any costs in the case they come up short. So it would be up to them, but we wouldn't jump in and pay for anything.

Commissioner Hollist said what happens if they would they have to come back before city council if they decided with their outdoor courts, to raise the funds to build the building for pickleball courts, and they tried to potentially leverage that as something else to meet the shortfall?

Planner Drozdek said if they're making any changes to the agreement, depending on how big the changes are and the number of changes, they would have to go back to city council. If it's just a minor change, those minor changes can be approved by city staff, but if it's anything major, or they're making plenty of changes it would go back to city council.

Commissioner Hollist said I know that this came up before, but I'm just going to say it so that it's on the record. I do think it's unusual to subsidize commercial with residential, but as the applicant even said, one item is wanted and the other is tolerated. So it is what it is. Again, the development agreement governs what we're looking at tonight. I also just wanted to say again, I do have concerns with the amount of residential parking being required, but as long as the 240 spots are provided for the 210 units, it's in compliance.

Commissioner Catmull said on the architecture side, the applicant was talking about mischaracterizing or was mentioning that it was favorable towards the requirements. I don't know if there'll be anyone made favorable or unfavorable towards the specific like or dislike of a particular material as far as the group or the committee. The other thing I would add there around noise is there are noise ordinances at the county level and pretty sure, at the city level as well. I don't know how they apply to this particular thing, but there's ways to manage the impact at property lines and everything else. But otherwise, I can't see anything that is out of alignment with the with the development agreement.

Chair Gedge said just to confirm you did attend the ARC meeting, correct?

Commissioner Catmull said yes, I did attend.

Chair Gedge said I just want to confirm that so we can amend the minutes from the ARC Committee, Mr. Catmull is not listed as an attendee as our planning commission representative. So can we just get that done just as we finalize this this evening?

Planner Drozdek said yes, we will do that.

Commissioner Bevans motioned to approve File No. PLSPR202500059 Utah Black Diamonds Site Plan. Chair Gedge seconded the motion. Roll Call Vote is 4-0 in favor; Commissioner Bishop and Commissioner Harding were absent from the vote.

I. LEGISLATIVE PUBLIC HEARINGS

J. OTHER BUSINESS

J.1. Planning Commission Discussion/Adoption regarding Commission Rules for 2025 (Attachment C).

Assitant City Attorney Greg Simonson said one of the things I appreciate about this planning commission, and as you know I have worked over the years with many planning commissions. I'm always very complimentary of this commission and how hard it works and how it tries to improve itself, and how it works at its training and tries to understand its duties. I appreciate that, and that's kind of how I view this effort. As far as these rules, I have had a chance to go through this pretty much line by line carefully in between the last meeting and this meeting, and I'd like to keep in mind that our ordinance? I'm looking at the planning commission ordinance specifically says: that the planning commission shall adopt such rules and regulations for the conduct of business before the Planning Commission as they deem appropriate, which rules and regulations may be modified and amended at any time by the planning commission at any of its regular meetings, so there's, there's quite a bit of discretion there. I've got a few comments, and I have focused on, not surprisingly, legal issues. And one, of the things I'll bring up first of all, is actually at the end. I had our legal assistant, Emily Fitton go through and retype them all, and I will circulate that to you. She didn't change anything as far as the wording, but she changed some of the formatting

The Planning Commissioner's and staff went through the bylaws and discussed new changes.

Chair Gedge said thank you Mr. Simonson and your assistant for going through the bylaws for us, we appreciate that. Our goal is so Commissioner Bevins last vote will be to approve the minutes, or not to the bylaws.

Chair Gedge said Mr. Simonson will draft revisions that he'll be making, we would like to have this noticed and put into our agenda under administrative for next meeting.

ADJOURNMENT

Commissioner Hollist motioned to adjourn. Chair Gedge seconded the motion.

The Planning Commission Meeting adjourned at 7:48 p.m.

SOUTH JORDAN CITY PLANNING COMMISSION STAFF REPORT

MEETING DATE: JUNE 24, 2025

FILE OVERVIEW

Item Name	Daybreak Village 11B Plat 2
Address	Generally located on the west side of Bingham Rim Road between 11800 S and Meadow Grass Drive.
File Number	PLPP202500089
Applicant	Perigee Consulting on behalf of LHM Real Estate
Property Owner	LHM Real Estate
Staff Author	Greg Schindler
Presenter	Greg Schindler

PROPERTY OVERVIEW

Acreage	12.731 Acres		
Current Zone	P-C (Planned Community)		
Current Land Use	Vacant		
General Plan Designation	Mixed Use Transit Oriented Development (MU-TOD)		
Neighboring Properties	<i>Zone</i>		<i>Land Use</i>
	<i>North</i>	P-C	RDO
	<i>East</i>	P-C	RDO
	<i>South</i>	Herriman City	
	<i>West</i>	CC(PD) A-1	MU

ITEM SUMMARY

A complete preliminary subdivision application for Daybreak Village 11b Plat 2 was submitted on May 1, 2025. The proposed subdivision will divide 12,731 acres into 91 residential lots, 5 park lots and associated public right-of-way.

TIMELINE

- **On April 30, 2025**, the applicant submitted an incomplete application to Staff for review. The application was initially rejected and a revised application was submitted and deemed complete on May 5, 2025. Staff reviewed the application and worked with the applicant to revise the preliminary subdivision plat to conform to applicable city regulations. Multiple reviews and re-reviews were completed by staff with all required corrections completed on June 11, 2025. The application was reviewed by the following departments:
 - Planning:
 - Engineering:
 - Building:
 - Fire:
 - Public Works, Stormwater, Streets, Parks and Water Divisions

REPORT ANALYSIS

Perigee Consulting, on behalf of applicant Larry H. Miller Real Estate has filed an application that will divide 12,731 acres into 91 residential lots (includes 59 single family detached and 32 townhomes), 5 park lots and associated public right-of-way.

The property is located along the west side of Bingham Rim Road between 11800 South and Meadow Grass Drive.

The Daybreak Community Structure Plan designates the area where the subdivision is located, as Village. Section 17.72.020 describes the Village Land Use Designation as follows: “This category is designed for medium density mixed use development that includes residential (single and multi-family), office, commercial, industrial, public/semipublic and recreation/open space uses, without a predetermined emphasis on any single use. This category may accommodate a gross residential density of twenty five (25) units per acre.”

The gross density of the proposed subdivision is 7.1 units per acre and the net density is 13.9 units per acre.

Findings:

- The proposed subdivision is consistent with the City General Plan.
- All State and Local subdivision review requirements have been followed.
- The proposal complies with all City ordinances, the Daybreak Master Development Agreement and the Daybreak Master Subdivision requirements.

Conclusions:

- The application is in conformance with the minimum requirements of South Jordan Municipal Codes [§16.10.040](#) and [§17.72.110](#) and the General Plan of South Jordan.

Planning Staff Recommendation:

Staff recommends approval of the application based on the report analysis, findings, and conclusions listed above.

PLANNING COMMISSION ACTION

Required Action:

Final Decision

Scope of Decision:

This is an administrative decision to be decided by the Planning Commission.

Standard of Approval:

The Planning Commission shall receive comment at a public hearing regarding the proposed subdivision. The Planning Commission may approve, approve with conditions or if the proposed subdivision does not meet City ordinances or sanitary sewer or culinary water requirements, deny the preliminary subdivision plat application.

Motion Ready:

I move that the Planning Commission approve:

1. File PLPP202500089, Daybreak Village 11B Plat 2 Preliminary Subdivision

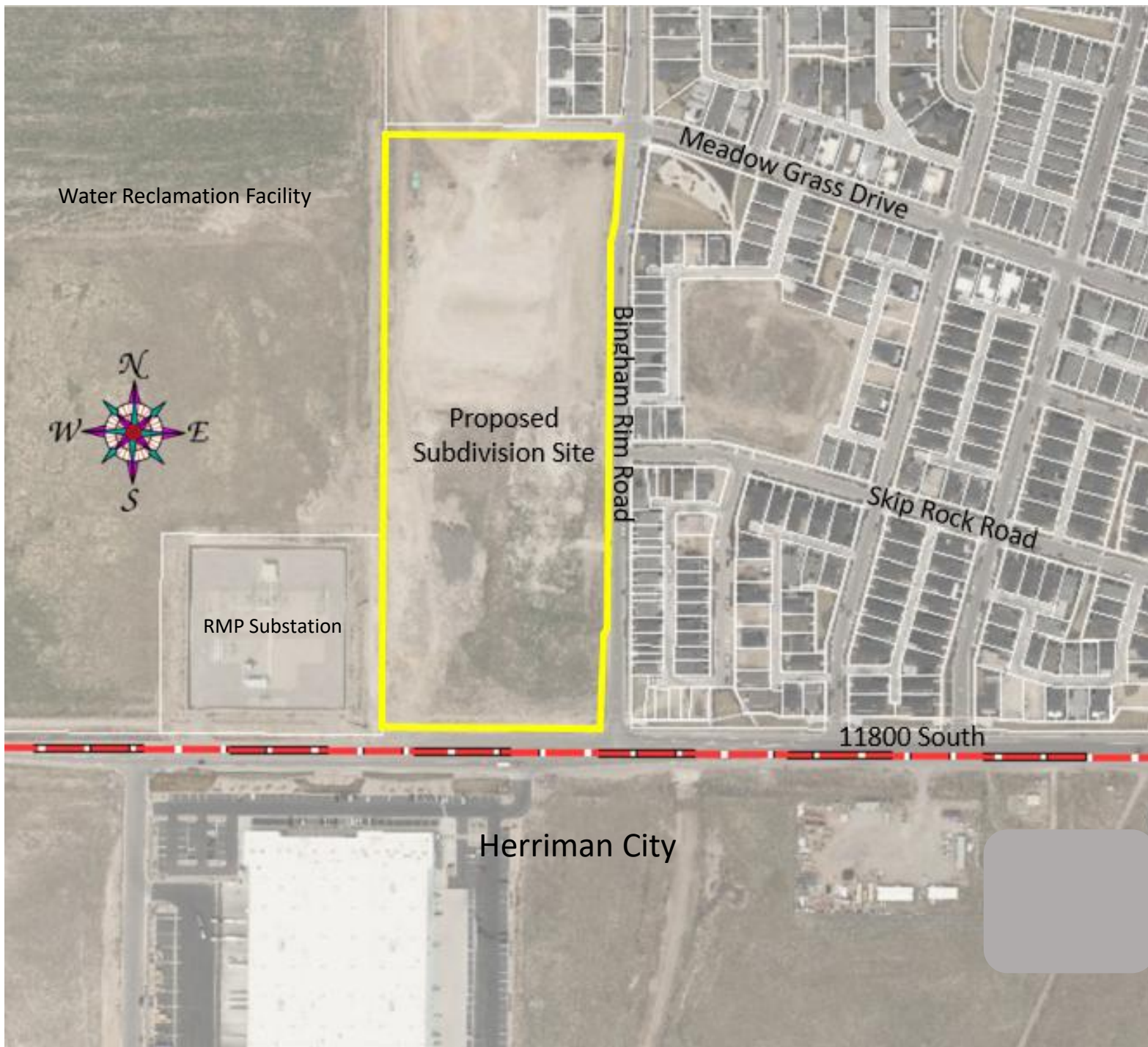
Alternatives:

1. Recommend denial of the application.
2. Schedule the application for a decision at some future date.

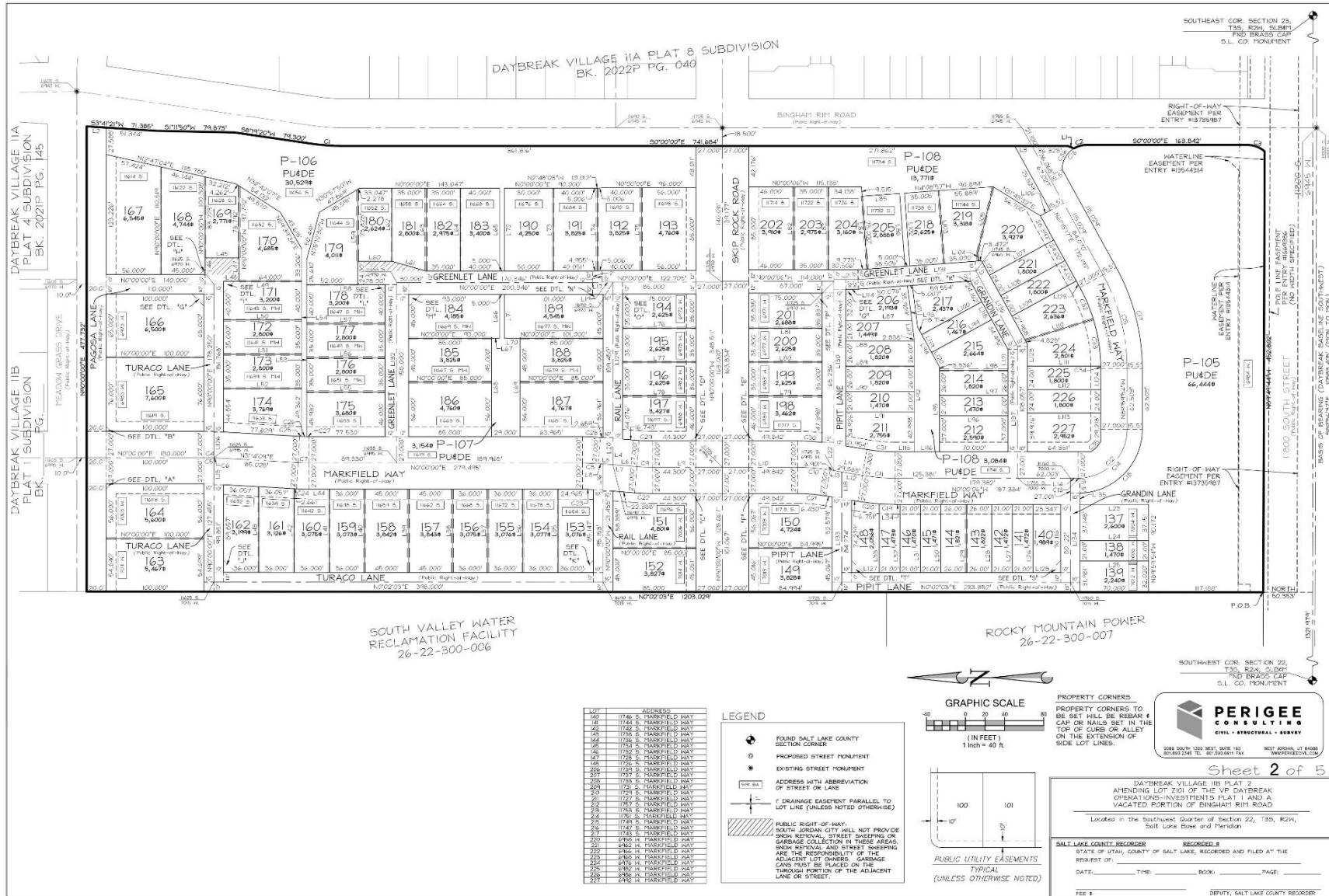
SUPPORTING MATERIALS

1. Attachments (Location Map, Proposed Preliminary Subdivision)

Location Map



Item H.1.



SOUTH JORDAN CITY PLANNING COMMISSION STAFF REPORT

MEETING DATE: JUNE 24, 2025

FILE OVERVIEW

Item Name	Daybreak South Station Plat 5
Address	Generally located at 11234 S Grandville Avenue
File Number	PLPP202500034
Applicant	Perigee Consulting on behalf of LHM Real Estate
Property Owner	LHM Real Estate
Staff Author	Greg Schindler
Presenter	Greg Schindler

PROPERTY OVERVIEW

Acreage	6.308 Acres		
Current Zone	P-C (Planned Community)		
Current Land Use	Vacant		
General Plan Designation	Mixed Use Transit Oriented Development (MU-TOD)		
Neighboring Properties	<i>Zone</i>		<i>Land Use</i>
	<i>North</i>	P-C	MU-TOD
	<i>East</i>	P-C	MU-TOD
	<i>South</i>	P-C	MU-TOD
	<i>West</i>	P-C	MU-TOD

ITEM SUMMARY

A complete preliminary subdivision application for Daybreak South Station Plat 5 was submitted on February 25, 2025. The proposed subdivision will divide 6.308 acres into two Civic/Commercial lots and associated public right-of-way.

TIMELINE

- **On February 20, 2025**, the applicant submitted an incomplete application to Staff for review. The application was initially rejected and a revised application was submitted and deemed complete on February 25, 2025. Staff reviewed the application and worked with the applicant to revise the preliminary subdivision plat to conform to applicable city regulations. Multiple reviews and re-reviews were completed by staff with all required corrections completed on June 11, 2025. The application was reviewed by the following departments:
 - Planning:
 - Engineering:
 - Building:
 - Fire:
 - Public Works, Stormwater, Streets, Parks and Water Divisions

REPORT ANALYSIS

Perigee Consulting, on behalf of applicant Larry H. Miller Real Estate has filed an application that will divide 6.308 acres into two Civic/Commercial lots and associated public right-of-way.

The property is located immediately north of the Salt Lake County Daybreak Library.

The subdivision is located within the boundaries of the Daybreak Town Center. Section 17.72.020 describes the Town Center designation as “designed for high density mixed use development that emphasizes office, commercial and recreational uses, but also includes residential (single- and multi-family), public/semipublic, industrial and open space uses. This category may accommodate gross residential density of fifty (50) units per acre.”

The future land use designation for the property is Mixed Use Transit Oriented Development (MU-TOD). “MU-TOD identifies active areas that are within ¼ mile of transit hubs. These areas support a vertical or horizontal mix of commercial, office, and higher density residential uses with entertainment, restaurants, bars, cafes, and businesses that do not required automotive transportation. These areas shall be located adjacent to regional transit hubs and provide accommodation for active transportation such as bike racks.”

The anticipated use of the property includes the Larry H. and Gail Miller Family Arts Center on Lot –C-101 and mixed use (likely a mix of residential and retail) on lot C-102.

FINDINGS AND RECOMMENDATION

Findings:

- The proposed subdivision is consistent with the City General Plan.

- All State and Local subdivision review requirements have been followed.
- The proposal complies with all City ordinances, the Daybreak Master Development Agreement and the Daybreak Master Subdivision requirements.

Conclusions:

- The application is in conformance with the minimum requirements of South Jordan Municipal Codes [§16.10.040](#) and [§17.72.110](#) and the General Plan of South Jordan.

Planning Staff Recommendation:

Staff recommends approval of the application based on the report analysis, findings, and conclusions listed above.

PLANNING COMMISSION ACTION

Required Action:

Final Decision

Scope of Decision:

This is an administrative decision to be decided by the Planning Commission.

Standard of Approval:

The Planning Commission shall receive comment at a public hearing regarding the proposed subdivision. The Planning Commission may approve, approve with conditions or if the proposed subdivision does not meet City ordinances or sanitary sewer or culinary water requirements, deny the preliminary subdivision plat application.

Motion Ready:

I move that the Planning Commission approve:

1. File PLPP202500034, Daybreak South Station Plat 5 Preliminary Subdivision

Alternatives:

1. Recommend denial of the application.
2. Schedule the application for a decision at some future date.

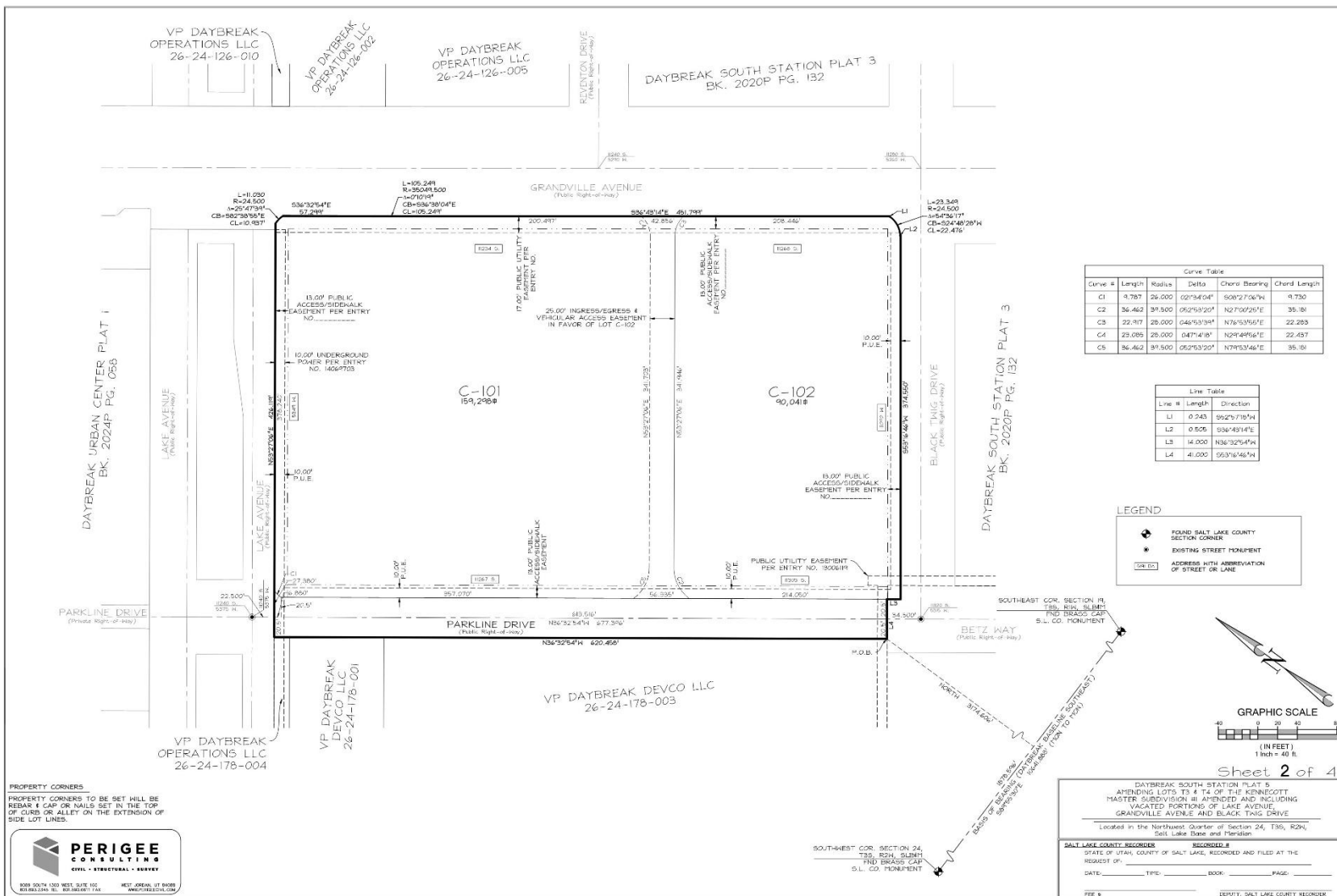
SUPPORTING MATERIALS

1. Attachments (Location Map, Proposed Preliminary Subdivision)

Location Map

Item H.2.





SOUTH JORDAN CITY PLANNING COMMISSION STAFF REPORT

MEETING DATE: June 24, 2025

FILE OVERVIEW

Item Name	SOJO Office Condominium Preliminary Subdivision
Address	10509 S River Heights Dr, South Jordan, UT 84095
File Number	PLPP202500055
Applicant	Zach Olson
Property Owner	SOJO PROFESSIONAL OFFICES, LLC
Staff Author	Miguel Aguilera, Planner I

PROPERTY OVERVIEW

Acreage	2.16 acres		
Recorded Subdivision	Harvest Village at South Jordan 2		
Current Zone	Professional Office (P-O)		
Current Land Use	Office Building		
General Plan Designation	Economic Center		
Neighboring Properties	<i>Zone</i>		<i>Land Use</i>
	<i>North</i>	[P-O]	[EC]
	<i>East</i>	[C-C]	[SN]
	<i>South</i>	[P-O]	[EC]
	<i>West</i>	[R-2.5]	[EC]

ITEM SUMMARY

The applicant is requesting the Planning Commission approve this condominium preliminary subdivision for the new office building located at 10509 S River Heights Drive. The property is zoned Professional Office and has one building.

- **March 31, 2025**, the applicant submitted a complete condominium preliminary subdivision application to Staff for review. The application was revised a total of 2 times to address all staff comments. The application was reviewed by the following departments:
 - Planning: Staff reviewed the application and worked with the applicant to revise the preliminary subdivision to conform to applicable city regulations. Resubmitted materials addressed staff comments.
 - Engineering: Staff reviewed the application and worked with the applicant to revise the preliminary subdivision to conform to applicable city regulations. Resubmitted materials addressed staff comments.
 - Building: Staff reviewed the application and worked with the applicant to revise the preliminary subdivision to conform to applicable city regulations. Resubmitted materials addressed staff comments.
 - Fire: Staff reviewed the application and worked with the applicant to revise the preliminary subdivision to conform to applicable city regulations. Resubmitted materials addressed staff comments.

REPORT ANALYSIS

Overview: The proposed subdivision will reflect the internal division of the building. The new Sojo Professional Office building is finishing up construction and has two stories with four units per story. Units range in size from 2,568 square feet to 4,012 square feet.

The site plan application for the building was approved on June 5, 2023. Sojo Professional Office building is nearing completion and hosts office suites to be occupied by various tenants. The City Manager recently approved an upsized single meter to serve all the condominium lots on the new plat.

FINDINGS AND RECOMMENDATION

Findings:

- There is no development agreement associated with this application.
- This preliminary subdivision will reflect existing units within the new building
- The P-O Zone allows for any size condominium in the zone. There are no specific requirements for either density or condominium size in the Zone

Conclusions:

- The application is in conformance with the minimum requirements of South Jordan Municipal Code [§16.14](#).

Planning Staff Recommendation:

Staff recommends approval of the application based on the report analysis, findings, and conclusions listed above.

Item H.3.

PLANNING COMMISSION ACTION

Required Action:

Final Decision

Scope of Decision:

This is an administrative decision to be decided by the Planning Commission.

Standard of Approval:

The Planning Commission shall receive comment at a public hearing regarding the proposed subdivision amendment. The Planning Commission may approve the amendment if it finds good cause to amend the subdivision, and the amendment complies with South Jordan Municipal Code [§16.14](#), other City ordinances, and sanitary sewer and culinary water requirements. The Planning Commission may only deny the amendment if there is no good cause for amending the subdivision and the proposed amendment does not meet all provisions of South Jordan Municipal Code [§16.14](#), other City ordinances, and sanitary sewer and culinary water requirements.

Motion Ready:

I move that the Planning Commission approves:

1. File number [PLPP202500055], Sojo Office Condominium Preliminary Subdivision

Alternatives:

1. Approval with conditions.
2. Denial of the application.
3. Schedule the application for a decision at some future date.

SUPPORTING MATERIALS

1. Attachment A, Location Map
2. Attachment B, Zoning Map
3. Attachment C, Proposed Condominium subdivision
4. Attachment D, SOJO Office Site Plan and Plat

Brad Klavano

Brad Klavano (Jun 17, 2025 18:36 MDT)

Miguel Aguilera

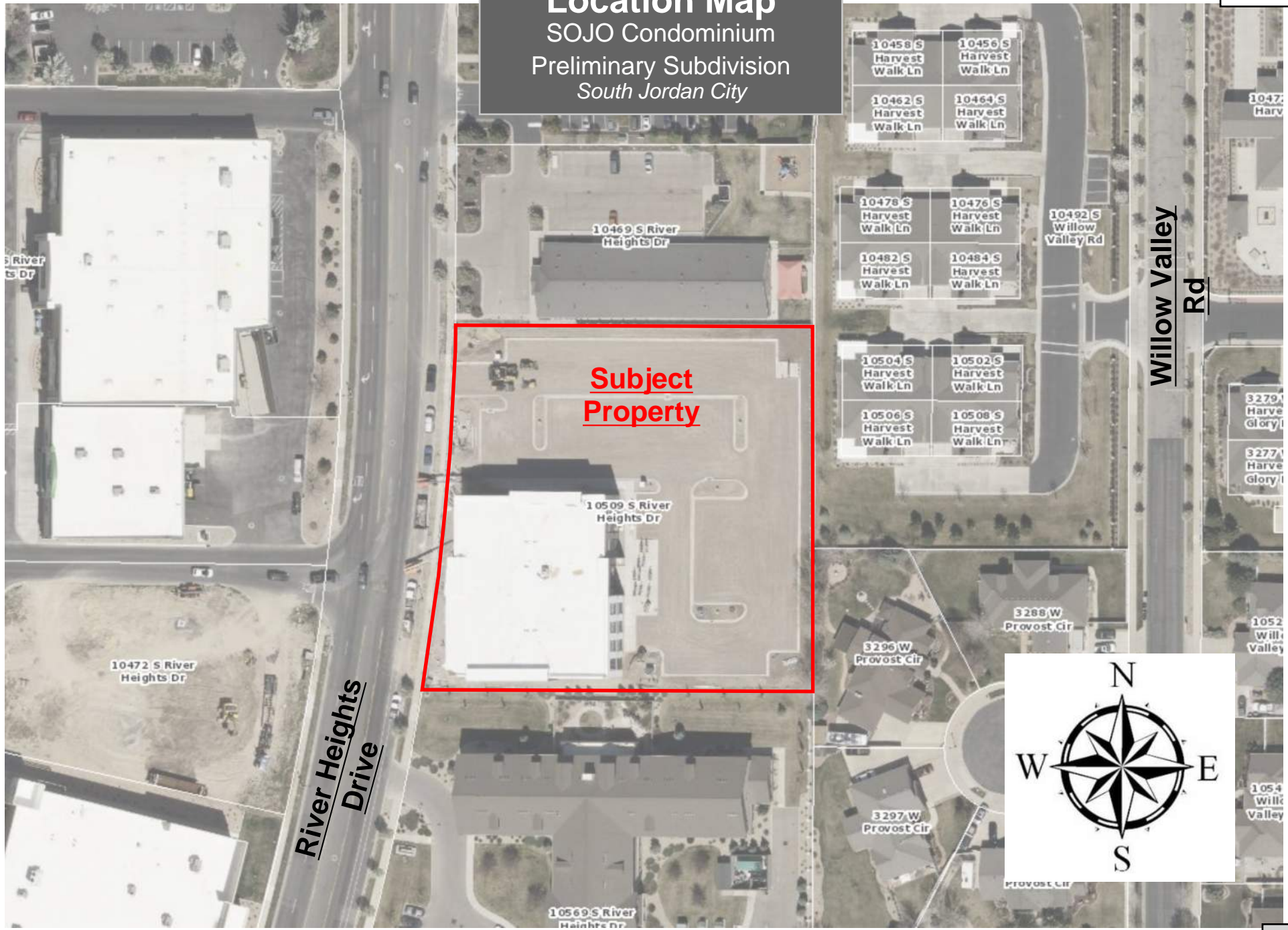
Miguel Aguilera (Jun 17, 2025 17:36 MDT)

Location Map

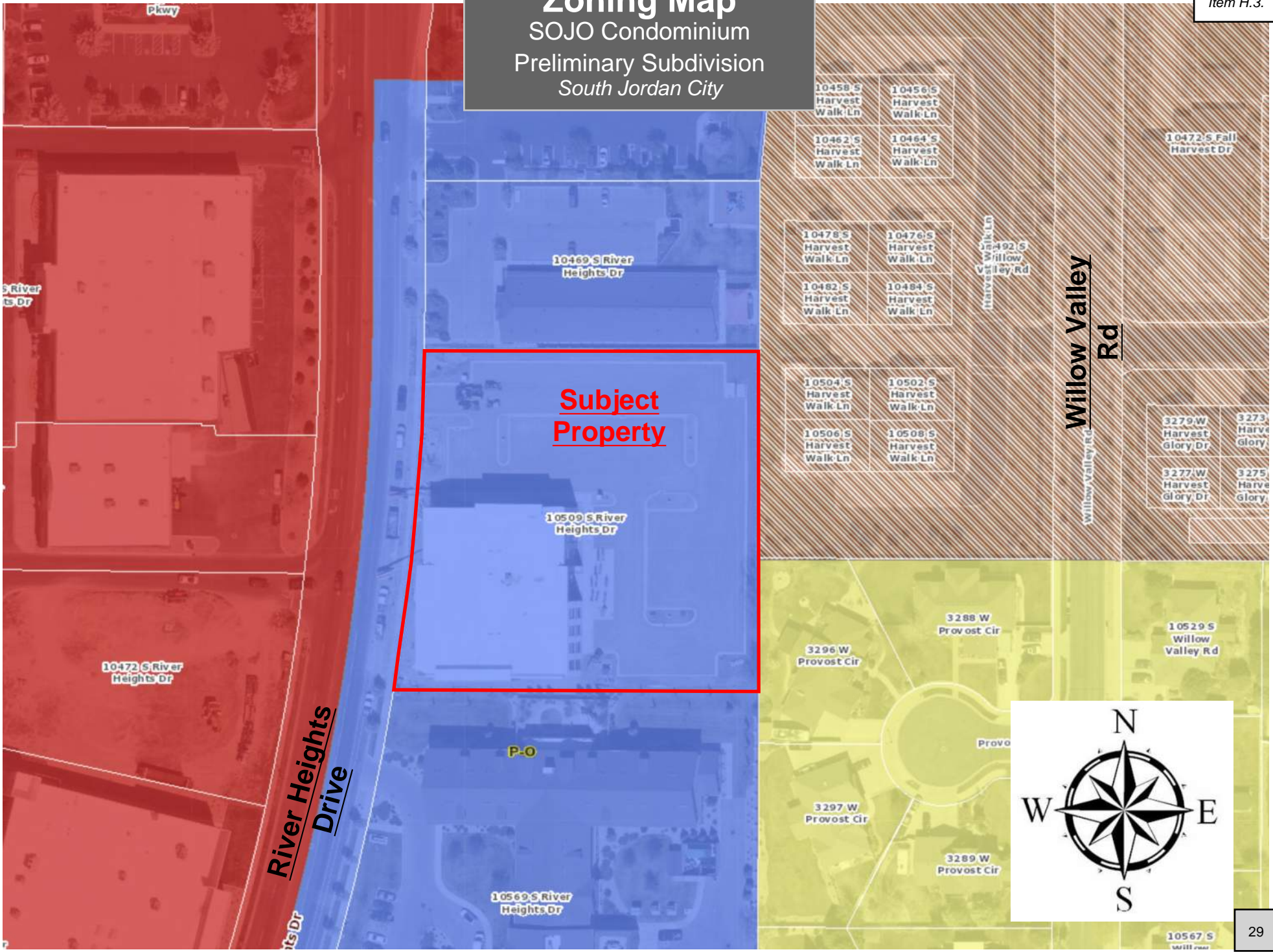
SOJO Condominium

Preliminary Subdivision

South Jordan City



Zoning Map
SOJO Condominium
Preliminary Subdivision
South Jordan City



SOJO OFFICE CONDOMINIUM

AMENDING LOT 4A OF HARVEST VILLAGE AT SOUTH JORDAN SUBDIVISION PHASE II AMENDED
LOCATED IN THE NORTHWEST QUARTER OF SECTION 17,
TOWNSHIP 3 SOUTH, RANGE 1 WEST, SALT LAKE BASE AND MERIDIAN
SOUTH JORDAN CITY, SALT LAKE COUNTY, UTAH

FOSSY HOLDINGS, LLC
LOT 3A
27-17-278-021

HARVEST VILLAGE AT SOUTH
JORDAN SUB PH II AMO
BK 2016P, PG 190

N 89°55'14" E 274.00'

NORTHWEST CORNER
SECTION 17, T3S, R 1W, SLB&M
(FOUND 2.5" BRASS CAP)

BASE OF BEARING N 0°08'50" W 2655.60'
2196.84'

468.76'

CENTER OF SECTION 17,
T3S, R1W, SLB&M
(FOUND 3" BRASS CAP)
BM ELEV: 4685.21'

RIVER HEIGHTS DRIVE
(PUBLIC DRIVE)

D=7°59'01"
R=1999.00
L=278.55'
CB=N 4°49'12" E
C=278.32'

POINT OF
BEGINNING

- ROCKY MOUNTAIN POWER NOTES:
- PURSUANT TO UTAH CODE ANN. § 54-3-27 THIS PLAT CONVEYS TO THE OWNER(S) OR OPERATORS OF UTILITY FACILITIES A PUBLIC UTILITY EASEMENT ALONG WITH ALL THE RIGHTS AND DUTIES DESCRIBED THEREIN.
 - PURSUANT TO UTAH CODE ANN. § 17-27A-603(4)(C)(III) ROCKY MOUNTAIN POWER ACCEPTS DELIVERY OF THE PUE, AS DESCRIBED IN THIS PLAT AND APPROVES THIS PLAT SOLELY FOR THE PURPOSE OF CONFIRMING THAT THE PLAT CONTAINS PUBLIC UTILITY EASEMENTS AND APPROXIMATES THE LOCATION OF THE PUBLIC UTILITY EASEMENTS, BUT DOES NOT WARRANT THEIR PRECISE LOCATION. ROCKY MOUNTAIN POWER MAY REQUIRE OTHER EASEMENTS IN ORDER TO SERVE THIS DEVELOPMENT. THIS APPROVAL DOES NOT AFFECT ANY RIGHT THAT ROCKY MOUNTAIN POWER HAS UNDER:
 - A RECORDED EASEMENT OR RIGHT-OF-WAY
 - THE LAW APPLICABLE TO PRESCRIPTIVE RIGHTS
 - TITLE 54, CHAPTER 8A, DAMAGE TO UNDERGROUND UTILITY FACILITIES OR
 - ANY OTHER PROVISION OF LAW.

ENBRIDGE GAS UTAH - NOTE
Questar Gas Company, dba Enbridge Gas Utah ("Enbridge"), approves this plat solely to confirm that the plat contains public utility easements. This approval does not (a) affect any right Enbridge has under Title 54, Chapter 8a, a recorded easement or right-of-way, prescriptive rights, or any provision of law, (b) constitute acceptance of any terms contained in any portion of the plat, and (c) guarantee any terms or waive Enbridge's right to require additional easements for gas service.



SANDY
45 W 10000 S, Suite 500
Sandy, UT 84070
Phone: 801.255.0529
WWW.ENSIGNENG.COM

LAYTON
Phone: 801.547.1100
TOOELE
Phone: 435.343.3590
CELEBRITY CITY
Phone: 435.865.1453
RICHFIELD
Phone: 435.896.2983

SHEET 1 OF 2

PROJECT NUMBER : 11820

MANAGER : BDM

DRAWN BY : KFW

CHECKED BY : PMH

DATE : 2025-06-11

JORDAN BASIN IMPROVEMENT DISTRICT

APPROVED THIS _____ DAY OF _____
20____ BY THE JORDAN BASIN IMPROVEMENT DISTRICT

JORDAN BASIN IMPROVEMENT DISTRICT MANAGER

BOARD OF HEALTH APPROVAL

APPROVED THIS _____ DAY OF _____
20____ BY THE BOARD OF HEALTH

SALT LAKE COUNTY HEALTH DEPARTMENT

CITY PLANNER

APPROVED THIS _____ DAY OF _____
20____ BY THE CITY PLANNER

CITY PLANNER

CITY ENGINEER

I HEREBY CERTIFY THAT THIS OFFICE HAS EXAMINED THIS PLAT AND
IT IS CORRECT IN ACCORDANCE WITH INFORMATION ON FILE IN
THIS OFFICE AND IS HEREBY APPROVED.

SOUTH JORDAN CITY ENGINEER DATE

OFFICE OF THE CITY ATTORNEY

APPROVED AS TO FORM THIS _____ DAY OF _____ A.D. 20____

ATTORNEY FOR SOUTH JORDAN CITY

DEVELOPER

THRIVE DEVELOPMENT
7585 S. UNION PARK AVE, STE 200
MIDVALE, UT 84047
PHONE: 801.948.8800

RECORD OF SURVEY

ROS NO.: _____
S-_____

COUNTY SURVEYOR REVIEWER DATE

EASEMENT APPROVAL

CENTURYLINK	DATE
ROCKY MOUNTAIN POWER	DATE
ENBRIDGE GAS UTAH	DATE
COMCAST	DATE

SOUTH JORDAN CITY MAYOR

APPROVED AS TO FORM THIS _____ DAY OF _____ A.D. 20____

ATTEST: CITY CLERK MAYOR

SURVEYOR'S CERTIFICATE

I, **PATRICK M. HARRIS**, do hereby certify that I am a Professional Land Surveyor, and that I hold License No. **286882** as prescribed under laws of the State of Utah. I further certify that by authority of the Owners, I have made a survey of the tract of land and the following description correctly describes the land surface upon which has been or will be constructed, hereafter to be known as **SOJO OFFICE CONDOMINIUM**, a Utah condominium project. In accordance with the Utah Condominium Ownership Act, I further certify that the condominium plat for said project is accurate and complies with the provision of Section 57-8-13 (1) of the Utah Condominium Ownership Act. Reference markers as shown on this plat are located as shown and are sufficient to readily retrace or re-establish this survey.

BOUNDARY DESCRIPTION

All of Lot 4A, Harvest Village at South Jordan Subdivision Phase II Amended, recorded August 11, 2016 in Book 2016P at Page 190 in the Office of the Salt Lake County Recorder, being more particularly described as follows:

Beginning at a point being North 00°08'50" West 458.76 feet along the section line and East 1,330.62 feet from the Center of Section 17, Township 3 South, Range 1 Wes, Salt Lake Base and Meridian; and running

thence Northeasterly 278.55 feet along the arc of a 1,999.00 foot radius curve to the left (center bears North 81°11'17" West and the chord bears North 04°49'12" East 278.32 feet with a central angle of 07°59'01");
thence North 89°55'14" East 274.00 feet;
thence South 00°06'27" East 169.79 feet;
thence South 00°04'46" East 107.51 feet;
thence South 89°55'14" West 297.85 feet to the point of beginning.

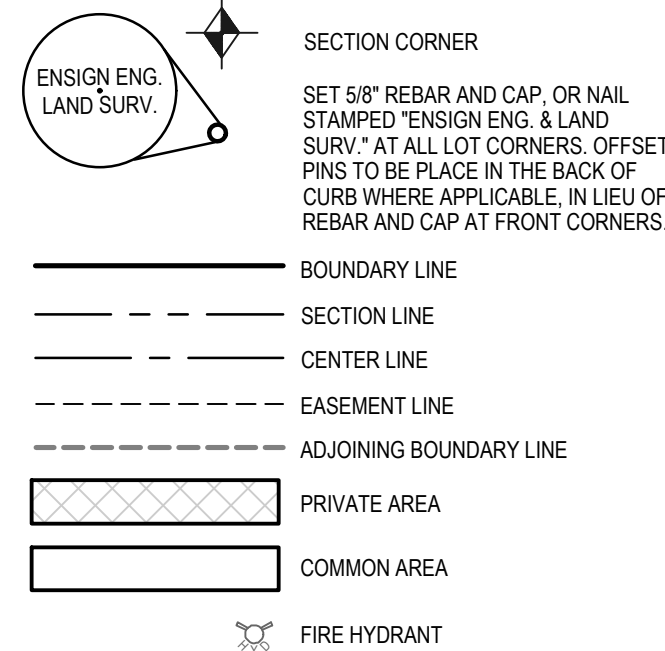
Contains 78,393 Square Feet or 1.800 Acres



DATE

PATRICK M. HARRIS
P.L.S. NO. 286882

LEGEND



South Jordan City Plat Notes

- Owners and potential purchasers of property legally described by this plat (the "property") are advised to familiarize themselves with all notes, lot information, easements and other pertinent information contained with this plat and also with any conditions, covenants and restrictions (CC&R) documents recorded against the property. Failure to adhere to these notes, easements, CC&R's or other documents recorded against the land could result in financial losses to or changes in expected property use of the property owner. Property owners and purchasers are responsible to review and be in compliance with all notes, easements, CC&R's, and other recorded documents related to this plat, as currently existing or as may from time to time be changed and/or amended.
- Many areas in South Jordan City have ground water problems due to high or fluctuating water table. Approval of this plan does not constitute representation by the city that building at any specified elevation will solve groundwater problems, in any.
- The owner certifies that the title report dated _____, which was prepared by _____, was provided to owner's surveyor and that the plat shows all easements and encumbrances listed in said title report.
- Finish floor elevation on each lot will not exceed 4' above the tbc elevation across the frontage of the lot.
- Approval of this plat by South Jordan City does not mean that individual lot drainage to a road or retention facility is assured. Development and grading may necessitate swales and other drainage facilities to protect individual properties. Approval of this plat also does not constitute representation by the City that swales or other drainage facilities are appropriate and maintained nor that drainage from adjacent properties is prevented.
- A single water meter serves all of the condominium lots within this plat.

JAMES AND NIKEM WILLIS
JONES MEADOWS PHASE 3
LOT 341
27-17-278-023
HARVEST VILLAGE AT SOUTH JORDAN
BK 2003P, PG 227

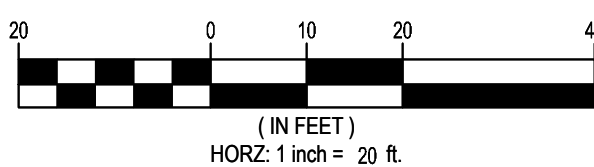
UNDERGROUND RIGHT
OF WAY EASEMENT
ENTRY NO. 14379402
BK 11568, PG 4183

CA SENIOR SOUTH JORDAN
UT PROPERTY OWNER
LOT 5A
27-17-278-023

HARVEST VILLAGE AT SOUTH
JORDAN SUB PH II AMO
BK 2016P, PG 190



HORIZONTAL GRAPHIC SCALE



SOJO OFFICE CONDOMINIUM

AMENDING LOT 4A OF HARVEST VILLAGE AT SOUTH
JORDAN SUBDIVISION PHASE II AMENDED
LOCATED IN THE NORTHWEST QUARTER OF SECTION 17,
TOWNSHIP 3 SOUTH, RANGE 1 WEST, SALT LAKE BASE AND MERIDIAN
SOUTH JORDAN CITY, SALT LAKE COUNTY, UTAH

SALT LAKE COUNTY RECORDER

RECORDED #

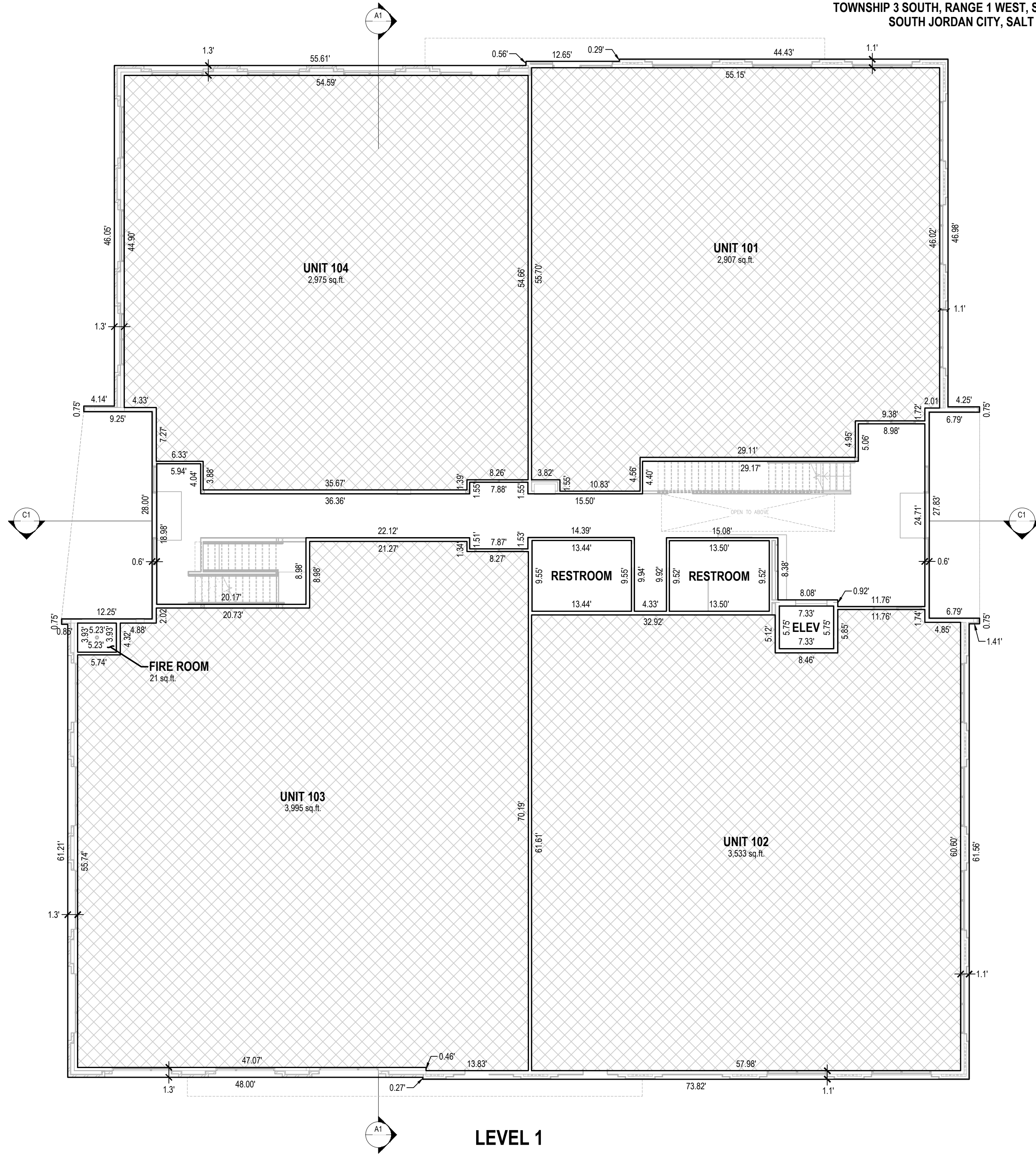
STATE OF UTAH, COUNTY OF SALT LAKE, RECORDED AND FILED AT THE
REQUEST OF :

DATE: _____ TIME: _____ BOOK: _____ PAGE: _____

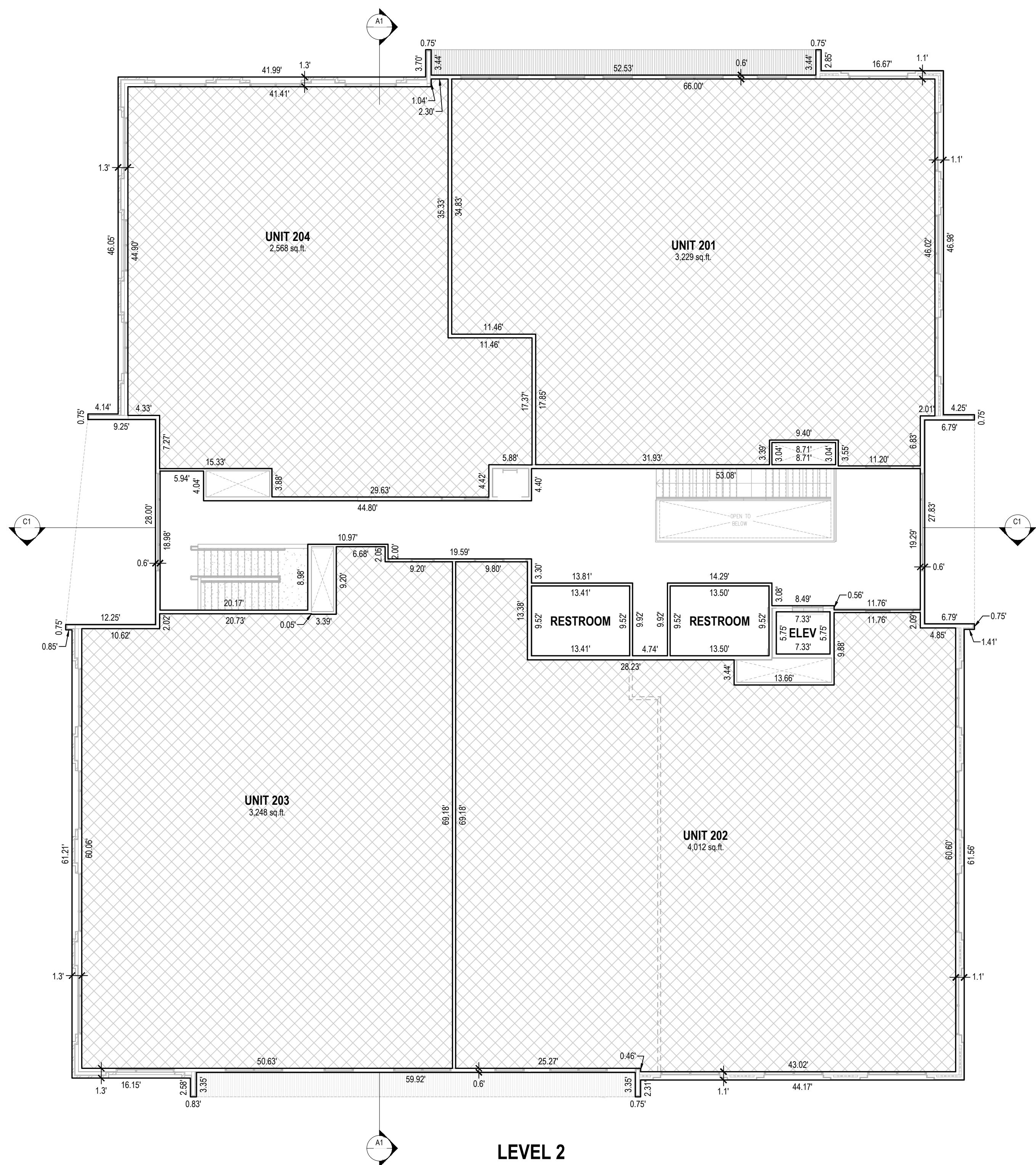
FEES _____ DEPUTY SALT LAKE COUNTY RECORDER

SOJO OFFICE CONDOMINIUM

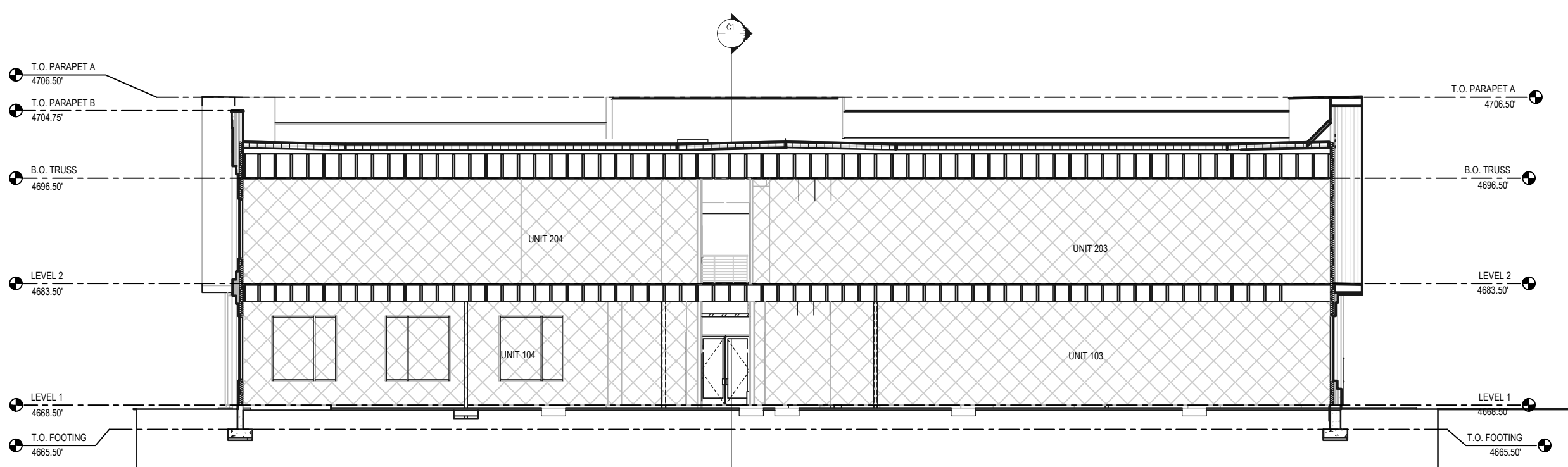
AMENDING LOT 4A OF HARVEST VILLAGE AT SOUTH JORDAN SUBDIVISION PHASE II AMENDED
LOCATED IN THE NORTHWEST QUARTER OF SECTION 17,
TOWNSHIP 3 SOUTH, RANGE 1 WEST, SALT LAKE BASE AND MERIDIAN
SOUTH JORDAN CITY, SALT LAKE COUNTY, UTAH



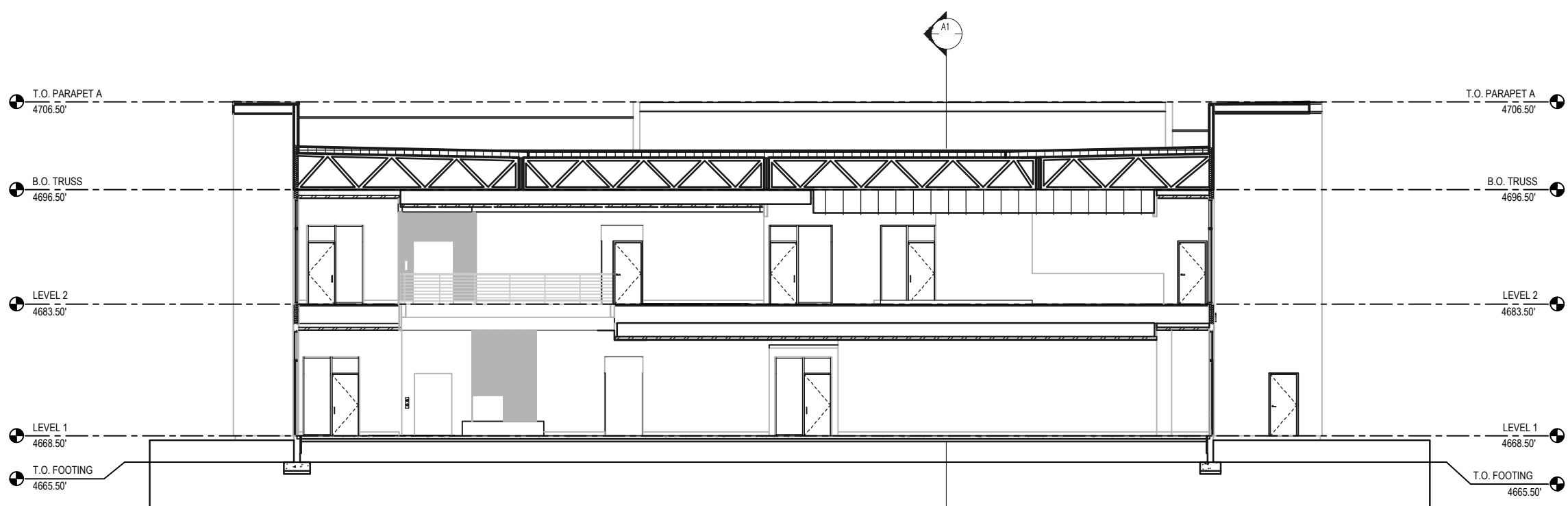
LEVEL 1



LEVEL 2



BUILDING SECTION - A1



BUILDING SECTION - C1

LEGEND

- PRIVATE AREA
- COMMON AREA



DEVELOPER
THRIVE DEVELOPMENT
7585 S. UNION PARK AVE, STE 200
MIDVALE, UT 84047
PHONE: 801.948.8800



SANDY
45 W 10000 S, Suite 500
Sandy, UT 84070
Phone: 801.255.0529
WWW.ENSGNENG.COM

LAYTON
Phone: 801.547.1100
TODDLE
Phone: 435.843.3590
CEDAR CITY
Phone: 435.855.1453
RICHFIELD
Phone: 435.895.2983

SHEET 2 OF 2

PROJECT NUMBER: 11820
MANAGER: BDM
DRAWN BY: KFW
CHECKED BY: PMH
DATE: 2025-06-11

SOJO OFFICE CONDOMINIUM

AMENDING LOT 4A OF HARVEST VILLAGE AT SOUTH
JORDAN SUBDIVISION PHASE II AMENDED
LOCATED IN THE NORTHWEST QUARTER OF SECTION 17,
TOWNSHIP 3 SOUTH, RANGE 1 WEST, SALT LAKE BASE AND MERIDIAN
SOUTH JORDAN CITY, SALT LAKE COUNTY, UTAH

SALT LAKE COUNTY RECORDER

RECORDED #
STATE OF UTAH, COUNTY OF SALT LAKE, RECORDED AND FILED AT THE
REQUEST OF :
DATE: TIME: BOOK: PAGE:
FEES DEPUTY SALT LAKE COUNTY RECORDER

811

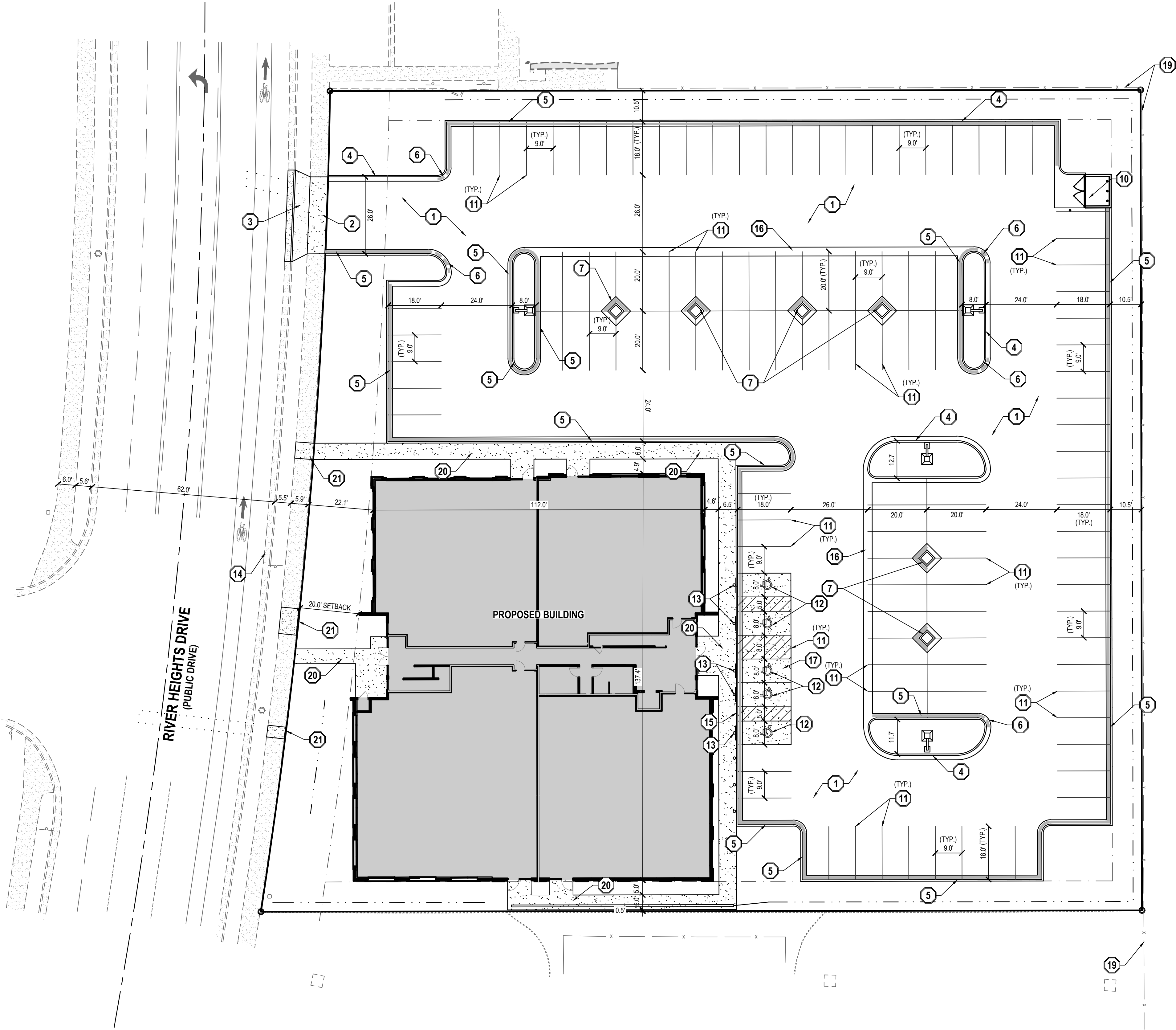
Know what's below.
Call before you dig.

CALL BLUESTAKES
@ 811 AT LEAST 48 HOURS
PRIOR TO THE
COMMENCEMENT OF ANY
CONSTRUCTION.

BENCHMARK

CENTER OF SECTION 17,
TOWNSHIP 3 WEST, RANGE 1 SOUTH
SALT LAKE BASE AND MERIDIAN

ELEV = 4685.21'



- GENERAL NOTES**
- ALL WORK TO COMPLY WITH SOUTH JORDAN CITY'S STANDARDS AND SPECIFICATIONS.
 - ALL IMPROVEMENTS MUST COMPLY WITH ADA STANDARDS AND RECOMMENDATIONS.
 - SEE LANDSCAPE/ARCHITECTURAL PLANS FOR CONCRETE MATERIAL, COLOR, FINISH, AND SCORE PATTERNS THROUGHOUT SITE.
 - ALL PAVEMENT MARKINGS SHALL CONFORM TO THE LATEST EDITION OF THE M.U.T.C.D. (MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES).
 - ALL SURFACE IMPROVEMENTS DISTURBED BY CONSTRUCTION SHALL BE RESTORED OR REPLACED, INCLUDING TREES AND DECORATIVE SHRUBS, SOD, FENCES, WALLS AND STRUCTURES, WHETHER OR NOT THEY ARE SPECIFICALLY SHOWN ON THE CONTRACT DOCUMENTS.
 - NOTIFY ENGINEER OF ANY DISCREPANCIES IN DESIGN OR STAKING BEFORE PLACING CONCRETE OR ASPHALT.
 - THE CONTRACTOR IS TO PROTECT AND PRESERVE ALL EXISTING IMPROVEMENTS, UTILITIES, AND SIGNS, ETC. UNLESS OTHERWISE NOTED ON THESE PLANS.

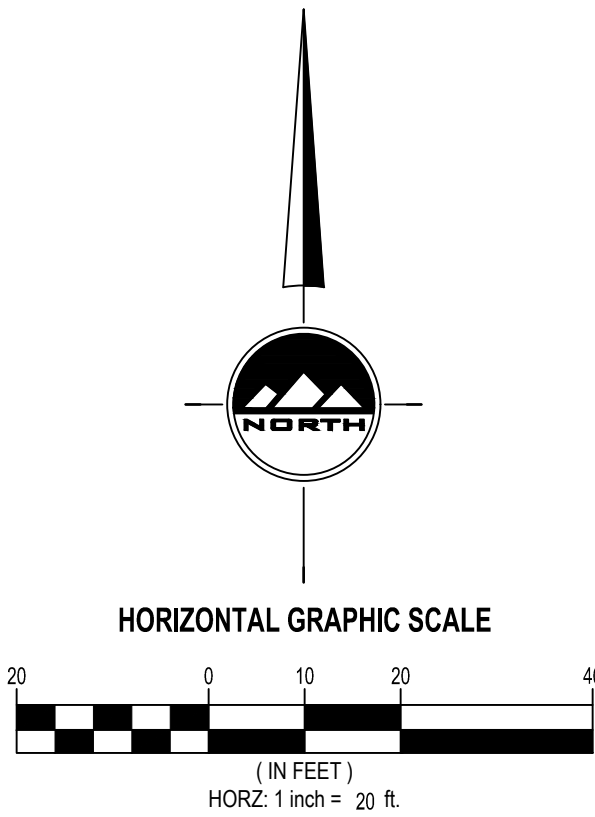
- SCOPE OF WORK:**
PROVIDE, INSTALL AND/OR CONSTRUCT THE FOLLOWING PER THE SPECIFICATIONS GIVEN OR REFERENCED, THE DETAILS NOTED, AND/OR AS SHOWN ON THE CONSTRUCTION DRAWINGS:
- ASPHALT PAVEMENT: 3" THICK ASPHALTIC CONCRETE WITH 8" UNTREATED BASE COURSE PER GEOTECHNICAL REPORT AND DETAIL 6/C-600.
 - 8" THICK CONCRETE SIDEWALK W/ 8" BASE COURSE PER APWA STANDARD PLAN NO. 231 AND SPECIFICATIONS.
 - 8" THICK TYPE 'A' FLARED DRIVEWAY APPROACH W/ 8" BASE COURSE PER APWA STANDARD PLAN NO. 221.1 AND SPECIFICATIONS.
 - 24" COLLECTION CURB AND GUTTER PER DETAIL 7/C-600.
 - 24" REVERSE PAN CURB AND GUTTER PER DETAIL 8/C-600.
 - TRANSITION BETWEEN COLLECTION CURB AND GUTTER AND REVERSE PAN CURB AND GUTTER.
 - 4.0' TREE ISLAND WITH 6" CURB WALL PER APWA STANDARD PLAN NO. 209 TYPE P AND SPECIFICATIONS.
 - LIGHT. SEE ELECTRICAL PLANS FOR INFORMATION.
 - *STOP* SIGN PER M.U.T.C.D. STANDARD PLANS.
 - TRASH ENCLOSURE: WITH 6" THICK CONCRETE APRON WITH #4 REBAR @ 24" O.C. E.W. ON 6" GRAVEL BASE PER DETAIL 6/C-600.
 - 4" WIDE SOLID WHITE PAVEMENT MARKING PER M.U.T.C.D. STANDARD PLANS.
 - PAINTED ADA SYMBOL AND ASSOCIATED HATCHING PER M.U.T.C.D. STANDARD PLANS.
 - *HANDICAP PARKING* SIGN PER DETAIL 4/C-600.
 - 30" TYPE 'A' CURB AND GUTTER PER APWA STANDARD PLAN NO. 205.1 AND SPECIFICATIONS.
 - 24" CONCRETE CURB AND GUTTER WITH 0" CURB FACE.
 - 3" WATERWAY PER DETAIL 9/C-600.
 - CONCRETE PAVEMENT: 6 1/2" THICK CONCRETE PAVEMENT OVER 4" BASE COURSE PER GEOTECHNICAL REPORT AND DETAIL 12/C-600.
 - ACCESSIBLE ROUTE FROM BUILDING ENTRANCE TO ROW.
 - EXISTING FENCE TO REMAIN ALONG THE NORTH AND EAST PROPERTY LINES.
 - 4" THICK CONCRETE SIDEWALK PER APWA STANDARD PLAN NO. 231 AND SPECIFICATIONS.
 - REPLACE EXISTING SIDEWALK. MATCH EXISTING SIDEWALK THICKNESS PER SOUTH JORDAN STANDARDS AND SPECIFICATIONS.

SITE SUMMARY TABLE		
DESCRIPTION	AREA (SF)	PERCENTAGE
PAVEMENT / HARDSCAPE	45,776	58%
BUILDING FOOTPRINT	15,474	20%
LANDSCAPING	17,138	22%
TOTAL SITE	78,388	100%

- FLOOR AREA CALCULATION (SQ. FT.):**
- LEVEL 1: 15,474 SQ. FT.
 - LEVEL 2: 15,031 SQ. FT.
 - TOTAL: 30,505 SQ. FT.

- PARKING CALCULATIONS:**
- 1 STALL / 300 SQ. FT. GENERAL OFFICE
 - 30,505 SQ. FT. / 300 = 102 STALLS REQUIRED
 - 123 STALLS AVAILABLE

PARKING DATA TABLE	
SURFACE STANDARD STALLS	118
SURFACE HANDICAP-ACCESSIBLE STALLS	5
TOTAL STALLS PROVIDED	123



City Engineer
City of South Jordan
Approved 05/30/2023
Brad Klauer City Engineer

Item H.3.

EN SIGN

THE STANDARD IN ENGINEERING

SALT LAKE CITY

45 W. 10000 S., Suite 500
Sandy, UT 84070
Phone: 801.255.0529

LAYTON

Phone: 801.547.1100

TOOELE

Phone: 435.843.3590

CEDAR CITY

Phone: 435.865.1453

RICHFIELD

Phone: 435.896.2983

WWW.ENSIGNENG.COM

FOR:
THRIVE DEVELOPMENT, LLC
7585 SOUTH UNION PARK AVE. STE 200
MIDVALE, UTAH 84047
CONTACT:
ZACHARY OLSON
PHONE: 801-948-8800

SOJO PROFESSIONAL
OFFICE BUILDING
10509 S RIVER HEIGHTS DR
SOUTH JORDAN, UTAH



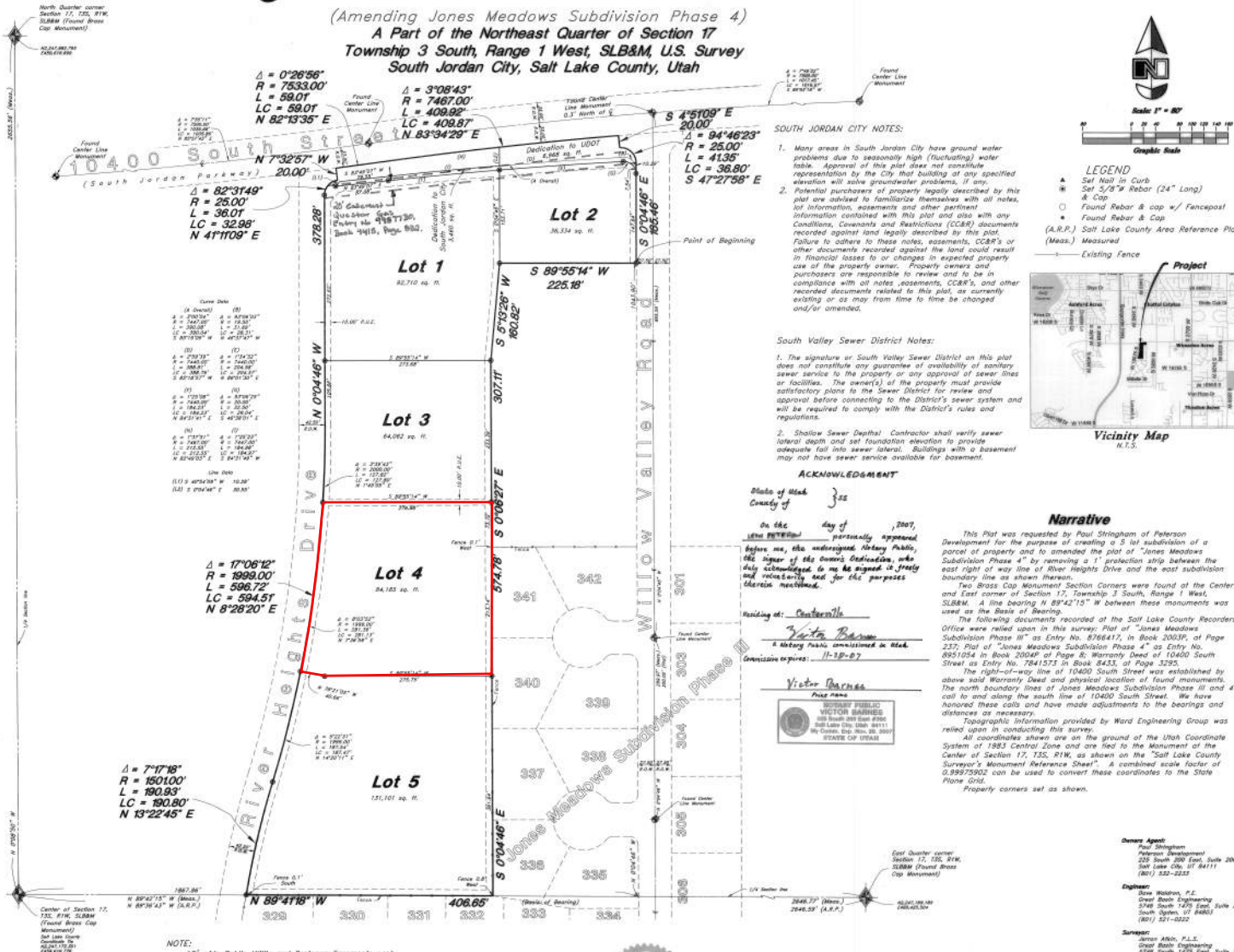
SITE PLAN

PROJECT NUMBER
11820
PRINT DATE
5/11/23
DRAWN BY
CHECKED BY
PROJECT MANAGER

C-200

Harvest Village at South Jordan Subdivision Phase II

(Amending Jones Meadows Subdivision Phase 4)
A Part of the Northeast Quarter of Section 17
Township 3 South, Range 1 West, SLB&M, U.S. Survey
South Jordan City, Salt Lake County, Utah



NOTE: 10' wide Public Utility and Drainage Easements each side of property line as indicated by dashed lines, unless otherwise shown

Surveyor's Certificate

I, Jerron R. Atkins II, do hereby certify that I am a Professional Land Surveyor, and that I hold Certificate No. 5892544 as prescribed under the Laws of the State of Utah. I further certify that by the authority of the Owners, I have made a survey of the tract of land shown on this plat and described below, and have subdivided said tract of land into lots hereinafter to be known as the Harvest Village at South Jordan Subdivision Phase II.

And that the same has been correctly surveyed and staked on the ground as shown on this plat.

Boundary Description

A part of the Northeast Quarter of Section 17, Township 3 South, Range 1 West, Salt Lake Base & Meridian, U.S. Survey:

Beginning of a point on the west right of way line of Willow Valley Road which is 1887.86 feet South 89°42'15" East along the south section line of said Northeast Quarter to said west right of way line of Willow Valley Road and 1043.90 feet North 0°04'46" West along said right of way line from the Center of said Section 17; thence thence South 89°53'14" West 225.18 feet; thence South 89°11'28" West 180.82 feet; thence South 0°06'27" East 307.11 feet to a point on the northeast corner of lot 341 of Jones Meadows Subdivision Phase II as recorded in the Salt Lake County Records Office as Entry No. 8766417 in Book 20039 of Page 237; thence along said subdivision (2) two courses as follows: North 0°04'46" East 574.78 feet and North 89°41'18" East 408.65 feet to the east right of way line of River Heights Drive; thence along said right of way (3) five courses as follows: northwesterly along the arc of a 1501.00 foot radius curve to the right a distance of 180.83 feet (central angle equals 7°17'18" and long chord bears North 17°06'12" East 190.80 feet) to a point of reverse curvature, northwesterly along the arc of a 1899.00 foot radius curve to the left a distance of 596.72 feet (central angle equals 17°06'12" and long chord bears North 0°28'20" East 594.51 feet); North 0°04'46" West 578.28 feet; northwesterly along the arc of a 25.00 foot radius curve to the right a distance of 36.01 feet (central angle equals 82°11'48" and long chord bears North 41°11'09" East 32.98 feet); North 71°25'27" West 20.00 feet to the south right of way line of 10400 South Street (South Jordan Parkway); thence along said right of way (2) two courses as follows: northwesterly along the arc of a 7533.00 foot radius curve to the left a distance of 58.01 feet (central angle equals 0°26'36" and long chord bears North 82°13'35" East 59.01 feet) to a point of reverse curvature, northwesterly along the arc of a 7487.00 foot radius curve to the right a distance of 408.32 feet (central angle equals 0°08'43" and long chord bears North 83°24'29" East 408.81 feet) to a point on the said west right of way line of Willow Valley Road; thence along said west right of way (3) three courses as follows: 451.90 feet East 30.00 feet, southeasterly along the arc of a 25.00 foot radius curve to the right a distance of 41.35 feet (central angle equals 84°46'25" and long chord bears South 47°27'58" East 38.80 feet) and South 0°04'46" East 163.46 feet to the point of beginning.

Contains 418,820 sq. ft. or 9.615 acres.

Contains 5 Lots

LEGEND

- Set Nail in Curb
- Set 2" x 8" Rebar (24" Long) & 100
- Found Rebar & cap w/ Fencepost
- Found Rebar & cap
- (A.R.C.) Salt Lake County Area Reference Plot (Mass.)
- Measured
- Existing Fence



SOUTH JORDAN CITY NOTES:

1. Many areas in South Jordan City have ground water problems due to seasonally high (fluctuating) water table. Approval of this plat does not constitute representation by the City that building at any specified elevation will solve groundwater problems, if any.
2. Potential purchasers of property legally described by this plat are advised to familiarize themselves with all rules, lot information, easements and other pertinent information contained with this plat and also with any Conditions, Covenants and Restrictions (CC&R's) documents recorded against land legally described by this plat. Failure to adhere to these rules, easements, CC&R's or other documents recorded against the land could result in financial losses to or changes in expected property use of the property owner. Property owners and purchasers are responsible to review and to be in compliance with all rules, easements, CC&R's and other recorded documents related to this plat, as currently existing or as may from time to time be changed and/or amended.

South Valley Sewer District Notes:

1. The signature or South Valley Sewer District on this plat does not constitute any guarantee of availability of sanitary sewer facilities to the property or any approval of sewer lines or services. The owner(s) of the property must provide satisfactory plans to the Sewer District for review and approval before connecting to the District's sewer system and will be required to comply with the District's rules and regulations.
2. Shallow Sewer Depth: Contractor shall verify sewer lateral depth and set foundation elevation to provide adequate fall into sewer lateral. Buildings with a basement may not have sewer service available for basement.

ACKNOWLEDGMENT

State of Utah
County of Salt Lake
On the 21st day of January, 2007,
I, Jerron R. Atkins II, personally appeared before me, the undersigned Notary Public, the signer of this document, who is acknowledged to me to be signed, freely and voluntarily and for the purposes therein mentioned.

Residing at: Centerville
Jerron R. Atkins II
A Notary Public, commissioned in Utah.
Commission expires: 11-18-07



Narrative

This Plat was requested by Paul Stringham of Peterson Development for the purpose of creating a 5 lot subdivision of a parcel of property and to amend the plat of "Jones Meadows Subdivision Phase II" by removing a 1" protection strip between the east right of way line of River Heights Drive and the east subdivision boundary line as shown thereon.

Two Brass Cap Monument Section Corners were found at the Center and East corner of Section 17, Township 3 South, Range 1 West, SLB&M. A line bearing N 89°42'15" W between these monuments was used as the Basis of Bearing.

The following documents recorded at the Salt Lake County Records Office were relied upon in this survey: Plat of "Jones Meadows Subdivision Phase II" as Entry No. 8766417, in Book 20039, of Page 237; Plat of "Jones Meadows Subdivision Phase II" as Entry No. 8951054 in Book 20047 of Page 8; Warranty Deed of 10400 South Street as Entry No. 7841072 in Book 8433, of Page 329.

The right-of-way line of 10400 South Street was established by above said Warranty Deed and physical location of found monuments. The north boundary lines of Jones Meadows Subdivision Phase II and 4 call to and along the south line of 10400 South Street. We have honored these calls and have made adjustments to the bearings and distances as necessary.

Topographic information provided by Ward Engineering Group was relied upon in conducting this survey.

All coordinates shown are on the ground of the Utah Coordinate System of 1983 Central Zone and are tied to the Monument of the Center of Section 17, T3S, R1W, as shown on the "Salt Lake County Surveyor's Monument Reference Sheet". A combined scale factor of 0.99975903 can be used to convert these coordinates to the State Plane Grid.

Property corners set as shown.

Surveyor:
Paul Stringham
Peterson Development
225 South 200 East, Suite 200
Salt Lake City, UT 84111
(801) 332-2232

Engineer:
Steve Robinson, P.E.
Great Salt Engineering
3745 South 1475 East, Suite 200
Salt Lake City, UT 84143
(801) 521-2232

Surveyor:
Jerron Atkins, P.L.S.
Great Salt Engineering
3745 South 1475 East, Suite 200
Salt Lake City, UT 84143
(801) 521-2232

SALT LAKE COUNTY BOARD OF HEALTH
Approved this 21st day of January, 2007.
Chris J. Jorgensen
SALT LAKE COUNTY BOARD OF HEALTH

SOUTH VALLEY SEWER DISTRICT
Approved this 21st day of January, 2007.
[Signature]
South Valley Sewer District

SOUTH JORDAN CITY MAYOR
Approved this 21st day of January, 2007.
[Signature]
South Jordan City Mayor

PLANNING COMMISSION APPROVAL
Approved this 21st day of January, 2007.
[Signature]
Planning Commission

SOUTH JORDAN CITY ENGINEER
Approved this 21st day of January, 2007.
[Signature]
City Engineer

SOUTH JORDAN CITY ATTORNEY
Approved this 21st day of January, 2007.
[Signature]
City Attorney

HARVEST VILLAGE at SOUTH JORDAN SUBDIVISION PHASE II

A Part of the Northeast Quarter of Section 17
T3S, R1W, SLB&M, U.S. Survey
South Jordan City, Salt Lake County, Utah

Recorded # 10022799
State of Utah, County of Salt Lake, Recorded and Filed at the Request of
Jerron R. Atkins II
Date 1/21/07 Time 11:11 AM Book 2007 Page 23
Jerron R. Atkins II
Salt Lake County Recorder











Final Report - SOJO Condo Plat

Final Audit Report

2025-06-18

Created:	2025-06-17
By:	Becky Messer (rmesser@sjc.utah.gov)
Status:	Signed
Transaction ID:	CBJCHBCAABAAky1yHZAgd5brTKpnxfebCxE2SGJ6Tv79

"Final Report - SOJO Condo Plat" History

-  Document created by Becky Messer (rmesser@sjc.utah.gov)
2025-06-17 - 11:26:14 PM GMT- IP address: 63.226.77.126
-  Document emailed to maguilera@sjc.utah.gov for signature
2025-06-17 - 11:27:06 PM GMT
-  Email viewed by maguilera@sjc.utah.gov
2025-06-17 - 11:36:08 PM GMT- IP address: 35.95.19.1
-  Signer maguilera@sjc.utah.gov entered name at signing as Miguel Aguilera
2025-06-17 - 11:36:26 PM GMT- IP address: 63.226.77.126
-  Document e-signed by Miguel Aguilera (maguilera@sjc.utah.gov)
Signature Date: 2025-06-17 - 11:36:28 PM GMT - Time Source: server- IP address: 63.226.77.126
-  Document emailed to bklavano@sjc.utah.gov for signature
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2025-06-18 - 0:35:10 AM GMT- IP address: 35.95.19.1
-  Signer bklavano@sjc.utah.gov entered name at signing as Brad Klavano
2025-06-18 - 0:36:02 AM GMT- IP address: 63.226.77.126
-  Document e-signed by Brad Klavano (bklavano@sjc.utah.gov)
Signature Date: 2025-06-18 - 0:36:04 AM GMT - Time Source: server- IP address: 63.226.77.126
-  Agreement completed.
2025-06-18 - 0:36:04 AM GMT

SOUTH JORDAN CITY PLANNING COMMISSION STAFF MEMO

MEETING DATE: JUNE 24, 2025

FILE OVERVIEW

Item Name	FrontRunner Station Area Plan
File Number	Resolution R2025-24
Staff Author	Joe Moss, Long Range Planner
Presenter	Joe Moss, Shawn Seager (WCG)

ITEM SUMMARY

Utah Code [§ 10-9a-403.1](#) requires South Jordan to adopt a Station Area Plan (SAP) for any area in the city that is within one half mile of a Frontrunner, TRAX, or within one quarter mile of a bus rapid transit station before December of 2025. The City received a grant from Wasatch Front Regional Council (WFRC) to work with a consultant team to comply with the requirements of state law. South Jordan adopted SAP's for the TRAX stations in December of 2024. Since that Time the City has been working with WFRC, UTA, and the consultant team to complete a SAP for the FrontRunner Station, which is the last remaining SAP needed for South Jordan to comply with these requirements.

The SAP is required to look at the following components:

- 1- Increasing the availability and affordability of housing, including moderate income housing.
- 2- Promotes sustainable environmental conditions.
- 3- Enhances access to opportunities.
- 4- Increases transportation choices and connections.

The draft SAP (attached to this memo) addresses these components and provides a comprehensive vision for the station area's future. It also includes an implementation table. The City will be required to report the plan's implementation progress in five years.

Staff requests the Planning Commission provide a positive recommendation of the item for the City Council to ensure that the City remains in compliance with legislative requirements.



Required Action:

Recommendation for City Council

Scope of Decision:

This is a legislative item that will be decided by the City Council.

Standard of Approval:

Utah Code [§ 10-9a-403.1](#) requires a municipality to “develop and adopt” a station area plan for any fixed guided railway stations. A resolution for such plan should “substantially promote” each of the following objectives:

- 1- Increasing the availability and affordability of housing, including moderate income housing;
- 2- Promoting sustainable environmental conditions;
- 3- Enhancing access to opportunities; and
- 4- Increasing transportation choices and connections.

Motion Ready:

I move that the Planning Commission recommends that the City Council approve:

1. Resolution R2025-24, FrontRunner Station Area Plan.

Alternatives:

1. Recommend approval with conditions.
2. Recommend denial.
3. Schedule the item for a recommendation at some future date.

SUPPORTING MATERIALS

1. Resolution R2025-24
 - a. Exhibit A, FrontRunner Station Area Plan



RESOLUTION NO. 2025 - 24**A RESOLUTION OF THE CITY OF SOUTH JORDAN, UTAH, ADOPTING THE SOUTH JORDAN FRONTRUNNER STATION AREA PLAN**

WHEREAS, pursuant to Utah Code §10-9a-403.1, each city with a fixed guideway transit station is required to adopt a “Station Area Plan”; and

WHEREAS, the City of South Jordan staff and Wasatch Front Regional Council (“WFRC”) staff recommend that the City of South Jordan City Council (the “City Council”) adopt the attached South Jordan FrontRunner Station Area Plan (the “FrontRunner SAP”); and

WHEREAS, the City Council determines that it is in the best interest of South Jordan City to adopt the FrontRunner SAP; and

WHEREAS, adoption of the FrontRunner SAP allows the City to forward the plan to the metropolitan planning organization (WFRC) and the public transit district (the Utah Transit Authority) for certification and demonstration of compliance with Utah Code §10-9a-403.1.

NOW, THEREFORE, BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF SOUTH JORDAN, UTAH:

SECTION 1. Adoption. The City Council hereby adopts the FrontRunner SAP, attached as Exhibit A, as required by Utah Code §10-9a-403.1.

SECTION 2. Severability. If any section, part or provision of this Resolution is held invalid or unenforceable, such invalidity or unenforceability shall not affect any other portion of this Resolution and all sections, parts, provisions and words of this Resolution shall be servable.

SECTION 3. Effective Date. This Resolution shall become effective immediately upon passage.

APPROVED BY THE CITY COUNCIL OF THE CITY OF SOUTH JORDAN, UTAH, ON THIS _____ DAY OF _____, 2025 BY THE FOLLOWING VOTE:

	YES	NO	ABSTAIN	ABSENT
Patrick Harris	_____	_____	_____	_____
Kathie L. Johnson	_____	_____	_____	_____
Donald Shelton	_____	_____	_____	_____
Tamara Zander	_____	_____	_____	_____
Jason McGuire	_____	_____	_____	_____

Mayor: _____

Dawn R. Ramsey

Attest: _____

City Recorder

Approved as to form:

Gregory Simonsen
Gregory Simonsen (Jun 10, 2025 15:40 MDT)

Office of the City Attorney

SOUTH JORDAN FRONTRUNNER STATION AREA PLAN

May 2025



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EXECUTIVE SUMMARY

South Jordan's FrontRunner Station area has been in active development for nearly 25 years—beginning just before Salt Lake City hosted the 2002 Winter Olympics Games. FrontRunner commuter rail service began operating this station along with the rest of FrontRunner South project on December 10, 2012, following UTA's FrontRunner North Project in 2008, establishing commuter rail in Utah only seventeen years ago.

Significant economic development growth has occurred over the past 25 years within the Station Area; growth potential is flattening west of I-15 as land availability is approaching buildout. Most of the buildable property is already constructed and occupied by manufacturing, auto sales, freeway oriented hotels, job centers and some limited residential apartments.

This area is characterized as mostly privately-owned business property with the exception of the Jordan River Corridor, a majority of which is managed as habitat protection, mitigation and preservation. The Jordan River Trail is the one notable public space and is heavily used as a bicycle and pedestrian shared-use path and kayaking water trail. A boat launch and trail head with parking is located on the north end of this site at South Jordan's Shields Lane Park. Most notable is a lack of housing and restaurant services within a half mile radius of

the station. Sandy City has future plans to develop additional housing on the site of the South Town mall including a Housing and Transit Reinvestment Zone. South Jordan City has recently received interest in developing some additional housing on the north end of this site.

The Station Area has 30-minute UTA FrontRunner (750) Commuter Rail service in peak periods and 60 minute service mid-day. With only approximately 465 riders boarding per day from this station, it lies below-average in terms of use—accounting for about four percent of the 13,213 total average weekday boardings on the entire FrontRunner system.

The station acts as an important main line I-15 / commuter rail transfer opportunity as it is surrounded with office jobs and manufacturing. It is across and adjacent to the Sandy Cairns and South Town Mall site that is undergoing redevelopment by the Smith Entertainment Group with housing and entertainment opportunities. The station area benefits from excellent freeway access via I-15, which carries an Annual Average Daily Traffic (AADT) of approximately 200,000 vehicles. Additionally, 10600 South / Jordan Parkway (SR-151) experiences nearly 47,000 AADT, while the five-lane Jordan Gateway roadway, running north-south, accommodates approximately 18,000 AADT.

House Bill 462 ("HB462") was passed and signed into law in 2022, which requires cities with a fixed-guideway public transit station to develop a Station Area Plan (SAP) for that station. This SAP covers roughly the half mile radius surrounding the FrontRunner station in South Jordan City. Sandy City has developed a separate SAP for this station in its jurisdiction east of I-15 which was adopted in 2025.

Barriers in the South Jordan Station Area include the Jordan River floodplain, I-15, railroad lines, and a power line easement, all of which limit connectivity and development.

This SAP meets the requirements described in HB462 by promoting the following objectives:

- ◆ **Increasing the availability and affordability of housing, including moderate income housing by proposing to:**
 - Update the General Plan to include future land use policies identified in the Station Area Plan,
 - Incentivise the development of underutilized or vacant land into owner occupied residential, and
 - Incentivise Mixed-Use infill development.

- ◆ **Promotes sustainable environmental conditions by proposing to:**
 - Facilitate shared parking implementation to reduce unproductive surface parking,
 - Continue Jordan River Corridor preservation area north to the end of the Study Area while allowing increasing housing development,
 - Avoid vertical development under the power line corridor due to utility constraints while encouraging placemaking and beautification,
 - Continue transit-oriented development around FrontRunner station to reduce trips, improve travel times, and improve air quality.

- ◆ **Enhances access to opportunities by proposing to:**
 - Incentivize mixed-use infill development with public spaces,
 - Encourage pedestrian bridge over I-15 to access South Town site,
 - Encourage public use, marketing, and engagement of public facilities, and
 - Add wayfinding to and from FrontRunner stations and destinations.

- ◆ **Increases transportation choices and connections by proposing to:**
 - Continue to encourage and partner with UTA and WFRC to expand transit options,
 - Building the Jordan River Pedestrian Bridge,
 - Facilitating active transportation network cohesion and visibility,
 - Implement shared parking to increase UTA station parking.



FrontRunner Bus Bay

I. INTRODUCTION

A Station Area Plan is intended to promote the State of Utah's investment in transit with goals and objectives to increase housing availability, improve access to job opportunities, protect the natural environment, and increase transportation options.

As a result of the 2022 Utah State Legislative session, [HB462 \(titled Housing Affordability Amendments](#), (a summary of which is located [on the Wasatch Front Regional Council \(WFRC\) website](#), passed and was signed into law. It requires a city that has a fixed-guideway public transit station (rail or Bus Rapid Transit) to develop an SAP for that station and update its general plan and zoning to implement the SAP if needed. HB462 requires that SAPs promote the following principles:

- ◆ Increase the availability and affordability of housing, including moderate income housing
- ◆ Promote sustainable environmental conditions
- ◆ Enhance access to opportunities
- ◆ Increase transportation choices and connections

Much of this FrontRunner Station area development has already been done through Developer Agreements and use of the site's Commercial Freeway Zone, Transit-Oriented Development Mixed-Use Zone, and Light Industrial Freeway Zone. The Jordan River Corridor has been successfully preserved with the Agricultural A1 Zone and by ownership of the United States Federal Government as mitigation property for the larger Central Utah Water Project (CUP).

The South Jordan City (City) applied for assistance from WFRC in 2024 and a consultant was hired to facilitate the planning process for the TRAX Red Line station areas located in Daybreak (in a separate document) and this FrontRunner Station near I-15 and 10600 South.

This SAP covers the half mile radius surrounding the South Jordan FrontRunner Station (located in South Jordan City boundaries). WFRC, UTA, and the City worked together to establish the formal Study Area boundary, which is shown below in **Figure 1**.

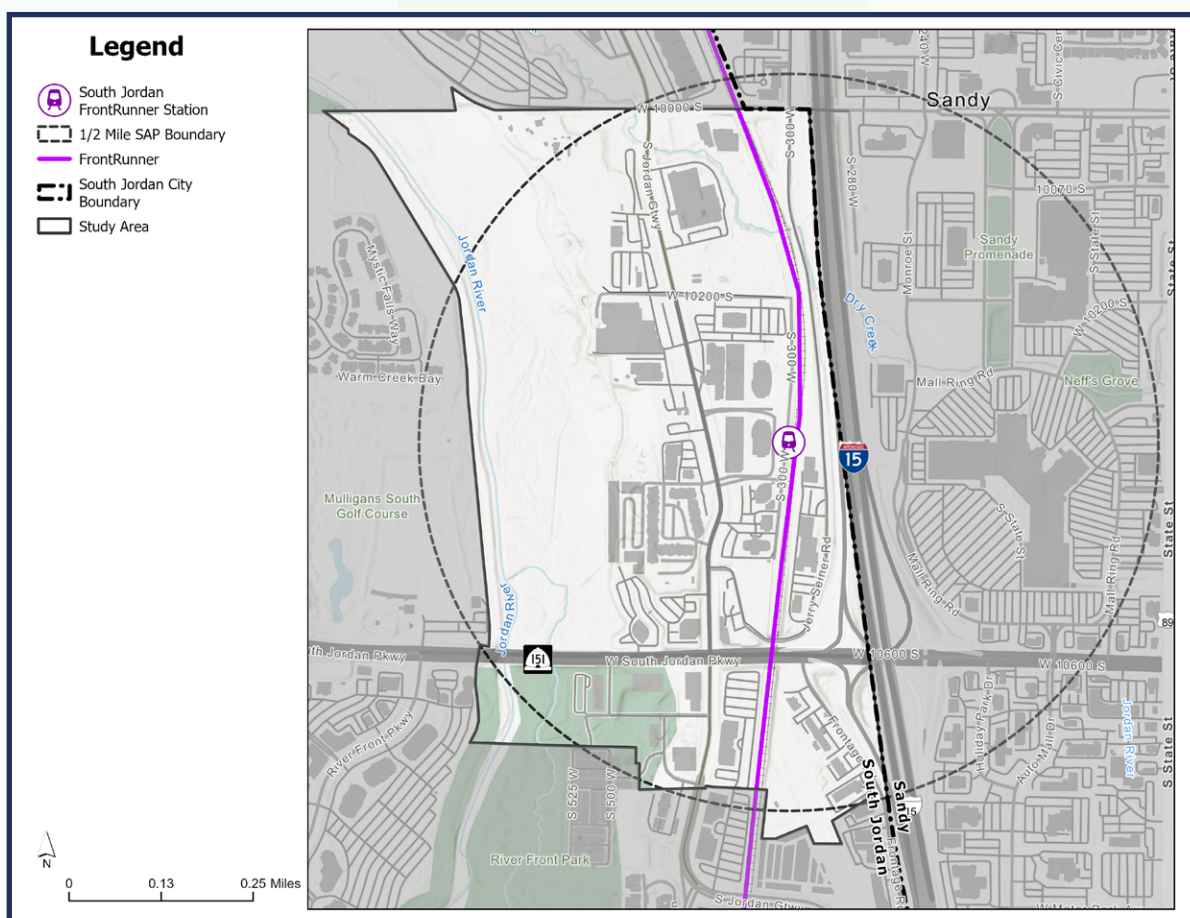


FIGURE 1: Study Area

II. EXISTING CONDITIONS

KEY TAKEAWAY

- ◆ More than twenty previous plans and studies have been completed in this area.

The existing conditions section documents demographics, existing plans, and existing land use. This section includes a multimodal transportation assessment documenting existing transit, active transportation, and vehicle networks.

A. Previous Plans and Studies

A review of previous and ongoing planning efforts focused on the FrontRunner SAP area provides a baseline understanding of needs and possible solutions for the Study Area. All relevant studies and plans are summarized in **Table 1** below with a hyperlink to the document. A brief description for each plan or study is provided in the Appendix.

TABLE 1: PREVIOUS PLANS AND STUDIES

Study Name	Date Completed
WFRC 2023 The Point-Area West Transit Fresh Look Study	In Progress
UTA 2025–2029 Five-Year Service Plan	2025
Moderate-Income Housing Plan	2025
South Jordan Transportation Master Plan	2024
Wasatch Front Regional Council Transportation Improvement Program	2024
Wasatch Choice Vision	2024
Sandy Station Area Plan - South Jordan Frontrunner	2024
UTA Moves 2050 Long-Range Transit Plan	2023
WFRC Regional Transportation Plan (RTP)	2023
WFRC Access to Opportunities	2023
WFRC SAP Guidance	2022
Blueprint Jordan River	2022
SLCo West General Plan	2021
Southwest Salt Lake County Transportation Analysis and Solutions Development	2021
South Jordan Sub Area Plan - Jordan River Gateway	2021
Utah Reclamation Mitigation and Conservation Commission	2021
South Jordan City General Plan	2020
Housing Needs Study	2014
South Jordan Gateway Study	2013
Jordan River Corridor Open Space and Habitat Conservation Master Plan and Management Guidelines	2011
South Jordan Economic Strategic Plan	2011

B. Demographic and Socioeconomic Data

KEY TAKEAWAYS

- ◆ 933 people are living in the Study Area in 2025.
- ◆ There is a younger population than the city-wide average age: 31 years compared to 34 years. They earn more than other renters in the City, have smaller average household sizes, and are more educated than the city as a whole.
- ◆ Employment is dominated by industrial manufacturing at Ultradent, and there is a high level of office employment with minimal retail sales (with the exception of the two automobile dealerships).

Population

According to Salt Lake County parcel data (verified by field inspection), there are two single-family units within the Study Area, with the remaining addresses attributed to multi-family units. After contacting both residential complexes and incorporating the single family homes, the total number of units within the Study Area is 578. For purposes of determining the average household (HH) size, Census tract-level data was used. The estimated HH size for single-family units is 3.03, and 1.61 for multi-family. The average HH size information was then multiplied by the total units to determine the population. The current population is estimated at 933 persons.

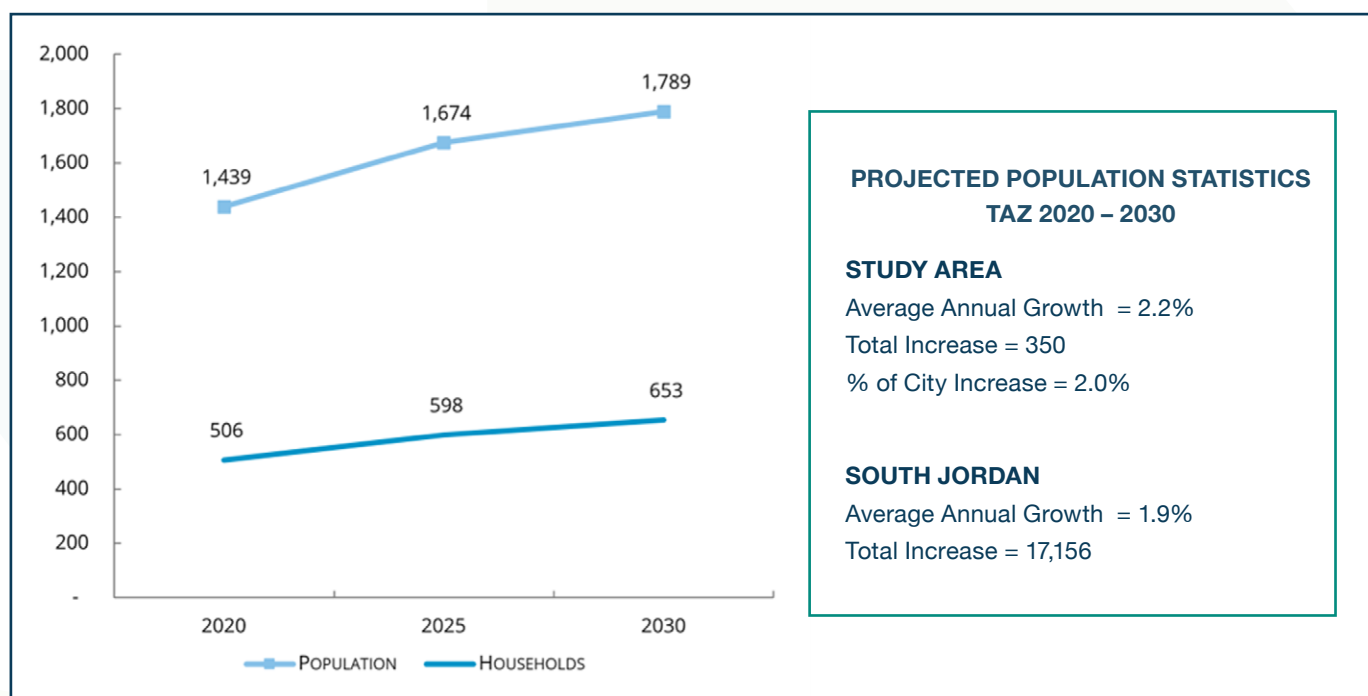


FIGURE 2: TAZ Population and Household Projections (Study Area)¹

¹ It is important to note that future population projections would be created using the 2010-2020 Annual Average Growth Rate (AAGR) for the DEC populations from those years. This method was unable to be utilized, as historic address point data was unavailable for use at the time of calculation. The projections provided in the figure utilize Traffic Analysis Zone (TAZ) data from the Wasatch Front Regional Council, which provides recent, current, and projected population estimates for the Study Area and South Jordan City. These estimates show growth for both geographies through 2030, but population growth percentage for the Study Area is estimated at 2.2 percent. Additionally, household growth percentage is estimated to outpace population growth percentage across both geographies, potentially indicating a demographic shift toward smaller households.

It is also important to note that TAZ data estimates a higher population than the methodology utilizing a mix of information including parcel data, address points, and Census data. In 2025, TAZ estimates show a population of 1,674, a difference of 741 from 933. However, TAZ households are estimated at 598 in 2025, a difference of 20 from 578.

Age

Utilizing 2020 Census block-level proportions as the base for the Study Area, **Figure 3** displays the distribution of population by age category for the Study Area and the City. The City's information was retrieved using 2023 ACS estimates. Where the City is comparatively balanced among categories, the Study Area has more specific concentrations within particular categories.

Notably, the Study Area has greater proportions of persons aged 25 to 29 but lower proportions of persons aged 0 to 24, indicating a continuance of the existing trend of a younger population in the Study Area. The median age for the Study Area is estimated at 30.5, while the estimate for the City is 34.4.

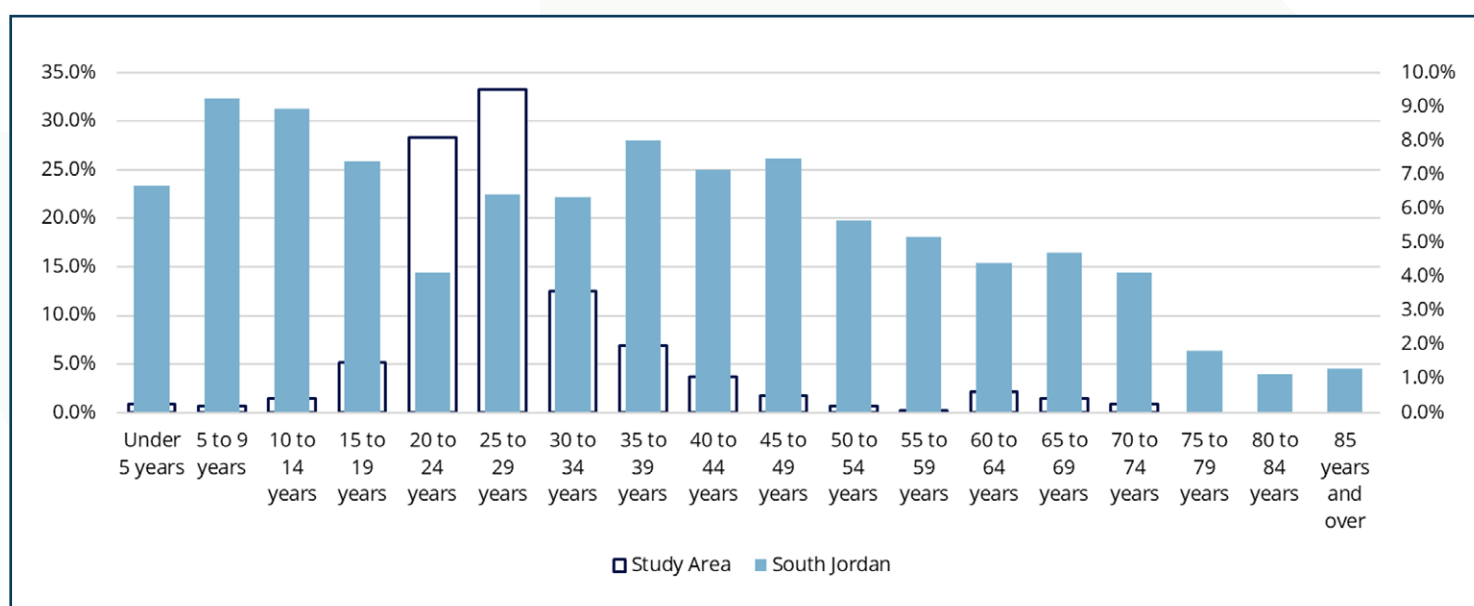


FIGURE 3: Age Distribution as a Percent of Total

TABLE 2: SUMMARY OF HOUSING STOCK

	Study Area		South Jordan	
	Count	Percent	Count	Percent
Single Family	2	0.4%	22,389	86.8%
2 to 4 Units	0	0.0%	292	1.1%
5 to 9 Units	0	0.0%	327	1.3%
10 or more Units	576	99.6%	2,709	10.5%
Mobile Home & Other	-	0.0%	84	0.3%
Total Units	578		25,801	

Source: Salt Lake County Parcel Data US Census Bureau, American Community Survey 5-Year Estimates (B25032), & Field Research

Table 2 displays the distribution of housing structure occupancy by unit count. Parcel data indicates there are two single-family units within the Study Area, with the remaining distribution being 10 or more units. The Study Area has lower proportions of persons living in single-family units than the City and higher proportions of persons living in two or more unit structures. Additionally, there are no mobile home or other structure types represented in the Study Area.

Income and Housing Costs

Table 3 compares housing affordability for both renters and homeowners between the Study Area and City using the available Census tract-level data. The ratio of the Study Area's median rent to renter income is 26.9 percent. The ratio of the Study Area's owner-occupied median income to median mortgage is 23.6 percent. Ratios greater than 30 percent suggest the average renter or household owner is burdened by housing costs. Ratios greater than 50 percent suggest a severe burden.

TABLE 3: HOUSING COST RATIO

	Study Area	South Jordan
Median Household Income in the past 12 months	\$90,1792	\$126,400
Owner-occupied Median Income	\$133,611	\$143,153
Renter-occupied Median Income	\$81,181	\$78,864
Median Gross Rent	\$1,817	\$1,806
Owner-occupied w/Mortgage Cost	\$2,628	\$2,423
Median Rent to Renter Income	26.9%	27.5%
Median Mortgage to Owner Income	23.6%	20.3%

Source: US Census Bureau, American Community Survey 5-Year Estimates (B25119, B25088, B25064)

Education

Utilizing 2023 Census tract-level proportions as the base for the Study Area, **Figure 4** displays educational attainment. Generally, residents of the Study Area have greater

educational attainment, with 61.8 percent of residents of the age of 25 or over holding an associate's degree or higher compared to the City at 57.5 percent.

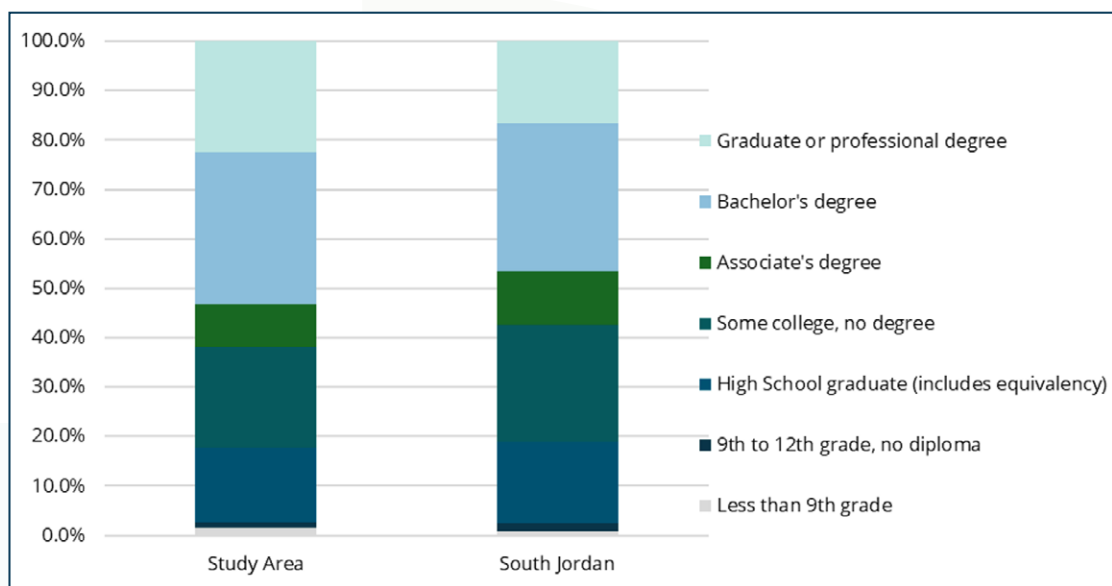


FIGURE 4: Educational Attainment

Employment

Figure 5 displays recent, current, and projected employment counts by industry type for the Study Area. These counts are provided through 2050. Through each 10-year period, percent growth of total employment is greater in the City than the Study Area. This extends to employment in the retail, office, and industrial job sectors. While non-typical

employment is expected to grow at a rate outpacing that of the City through 2030, it is anticipated to slow afterward through 2050. For the Study Area, the 10-year period of 2020 to 2030 is estimated to show the greatest percent growth for most sectors

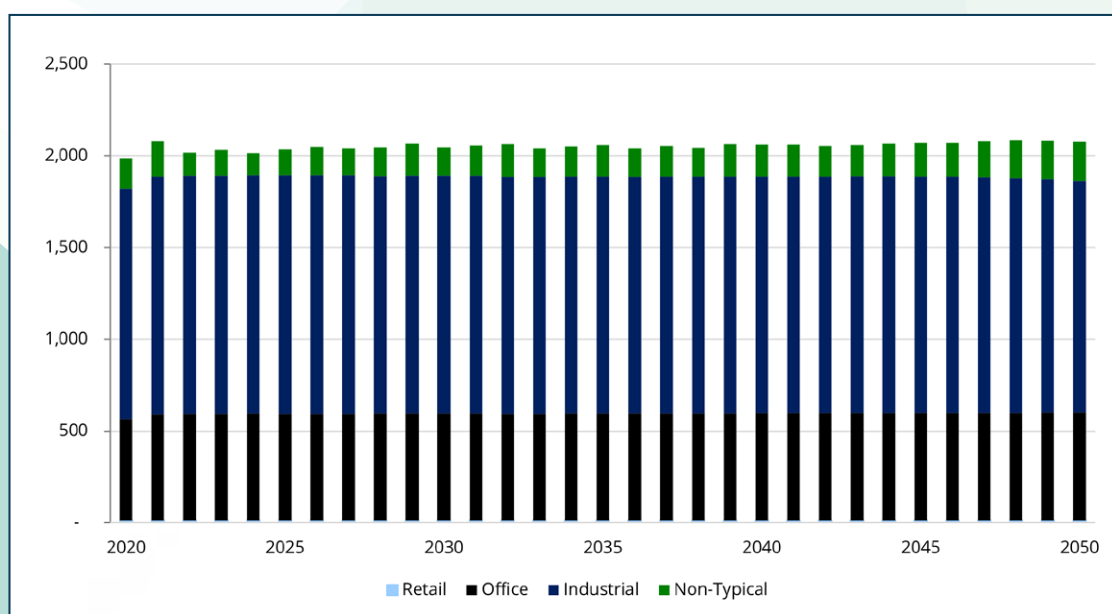


FIGURE 5: Projected Employment within Study Area

C. Land Use

KEY TAKEAWAYS

- ◆ Residential land use is very limited in the Study Area.
- ◆ A significant portion of the land for employment is used for manufacturing by Ultradent.
- ◆ There are only four food and beverage establishments for thousands of employees.
- ◆ The vast majority of buildable property is already developed.
- ◆ Jordan River Corridor is a prominent public space and managed for habitat protection.
- ◆ The majority of existing development has occurred with development agreements.

The area surrounding the South Jordan FrontRunner station is primarily characterized by employment-related land uses. A significant portion of the land within a half-mile radius is owned and occupied by Ultradent, a large employer in the City with a focus on manufacturing products and equipment. The remainder of the developed properties consist of office buildings, auto sales, freeway-oriented

hotels, preserved open space, some attached residential units, and the powerline corridor and facility. Buildable property in the immediate vicinity of the station is largely developed and occupied by these uses. There are a few undeveloped parcels that are earmarked for development shown in the existing land use map below.



Ultradent

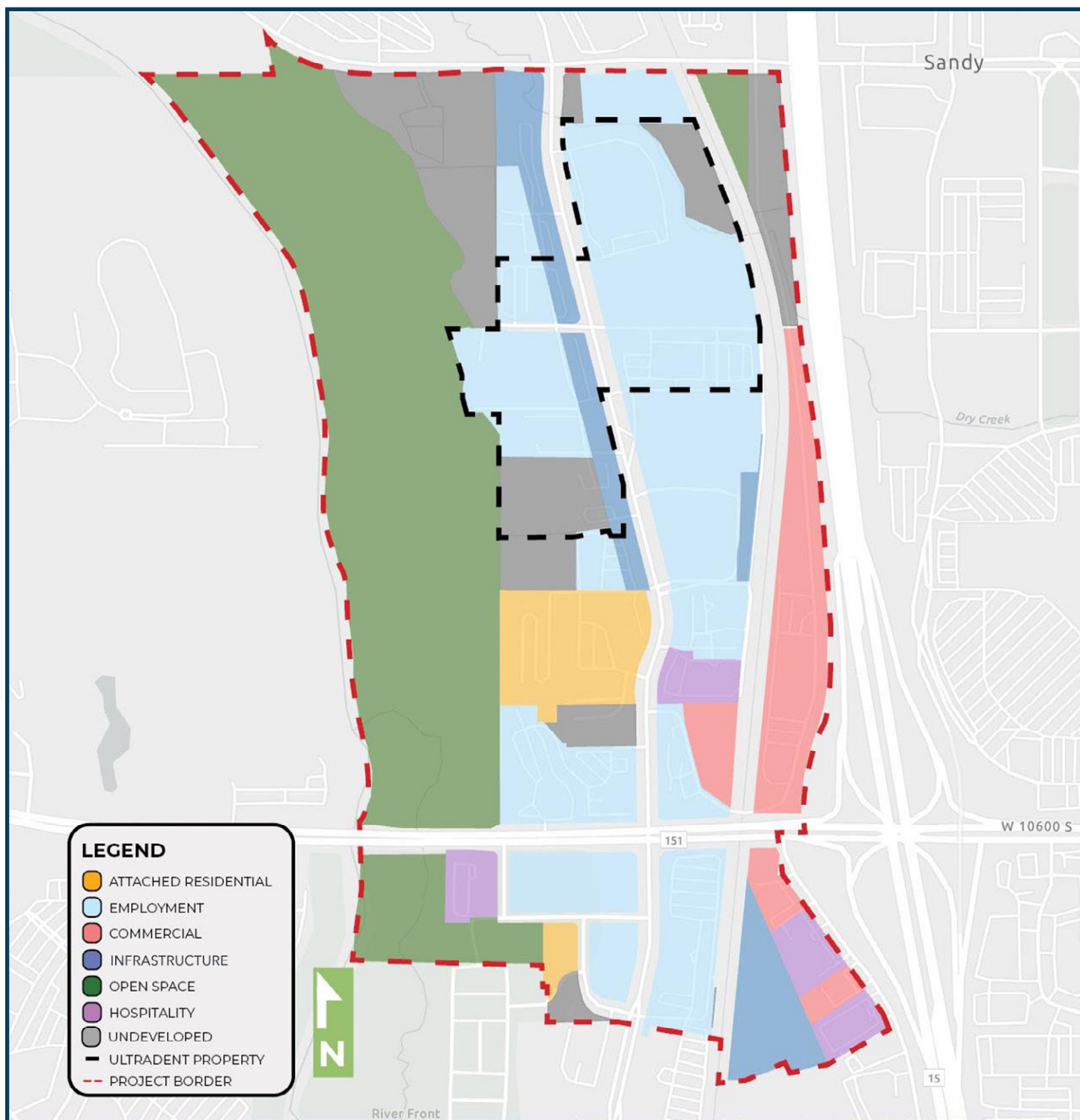


FIGURE 6: Existing Land Use

Given the station's proximity to the Jordan River and the existing floodway and floodplain, a significant portion of the Station Area, particularly on the west end, is dedicated to open space. The Jordan River Trail, a popular shared-use path for bicycles and pedestrians, traverses this area but is currently disconnected from the core of the Station

Area. The Jordan River Corridor itself is a prominent public space, managed primarily for habitat protection, mitigation, and preservation. This natural corridor provides valuable ecological benefits and may have the potential for enhanced recreational opportunities. Integration of this amenity with the surrounding developed areas is limited at present.



Jordan River looking east

Residential land use within the defined station area is also limited, with some potential for continued residential infill on vacant parcels. Despite the limited space within the South Jordan Station Area, other locations within the slightly broader area could potentially accommodate additional residential development in the future. Existing units within the station area are almost exclusively for rent and include two owner-occupied, single-family residences, the Jordan Station Apartments (536 apartment units) and South Ridge Townhomes and Apartments (40 apartment units with an additional 145 townhomes just outside of the station area). As residential infill continues within the station area, the addition of for sale units would be beneficial.

There are a few commercial, food, and retail uses on the west side of I-15 and maintain some synergy to the station area's uses. There are four food and beverage establishments

to support the 2,036 employees in the area, an appliance store, and a few vehicle sales establishments. Additional retail and restaurants exist on the east side of I-15 within and around The Shops at South Town. Opportunities for temporary uses like food trucks should be explored. The addition of these types of uses and additional residential units would enhance the vibrancy, walkability, and overall functionality of the station area, potentially attracting more transit riders and creating a more integrated environment.

Within the immediate station area, development has largely been guided by Development Agreements and the application of various zoning designations, including the Commercial Freeway Zone, Transit Oriented Development Mixed-Use Zone, and Light Industrial Freeway Zone.

Much of the development within the Station Area has little to no potential to be redeveloped in the next 20 years as most buildings are relatively newer construction. Those areas indicated in yellow on the redevelopment potential map below (**Figure 7**) have significant constraints with some ability for infill or open space improvements, within the next 5-20 years. The yellow swath on the northwestern edge

of the Study Area sits within the floodway and floodplain. There is opportunity to make some open space amenity improvements on the northern piece, whereas the southern half of the flood area north of 10600 South is highly restrictive towards open space improvements. The areas in green either have current development proposals or are more available to be developed in the near term (0-5 years).

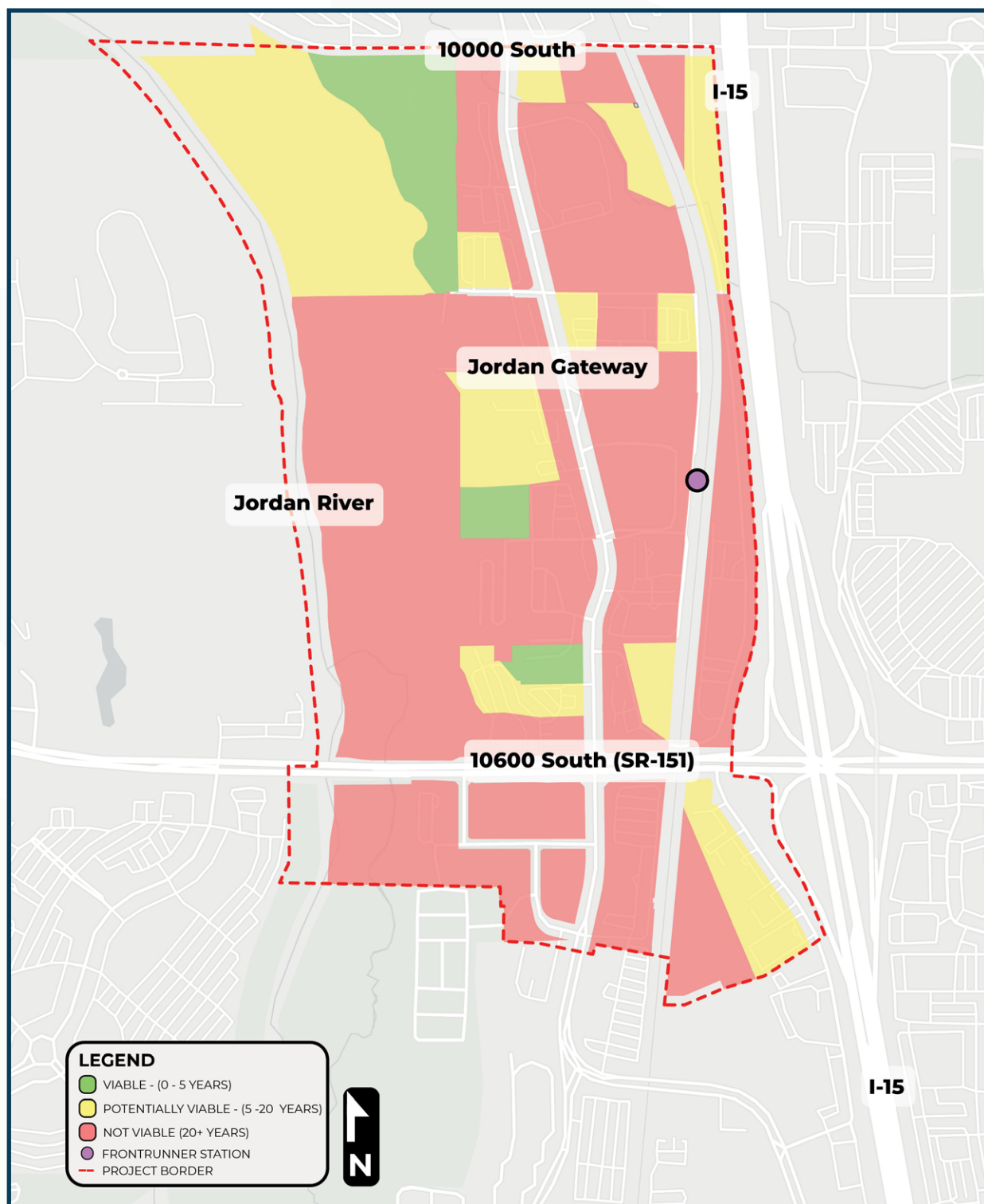
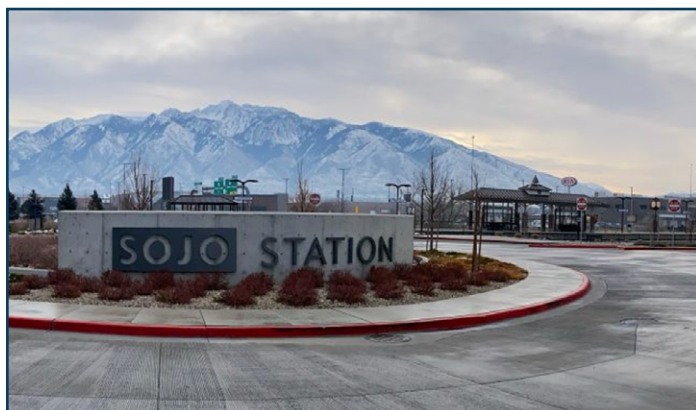


FIGURE 7: Study Area Development Potential

D. Public Space and Urban Design



South Jordan FrontRunner Station

The South Jordan Station Area presents a unique context for urban design. The area is predominantly non-residential uses, with some commercial, office, and industrial uses alongside natural open spaces. The area's urban framework currently reflects a functional arrangement of employment centers and transportation infrastructure. Jordan Gateway has eight-foot sidewalks running on both sides of the street, and the Jordan River Trail (on the west side of the Jordan River) is a 10-foot multi-use path offering north-south pedestrian collector routes. East-west connectivity is very limited with the sidewalks on 10600 South and Shields Lane being the only true east-west connector paths. There are also only five crossings on Jordan Gateway within the approximately 5,000 linear inside of the Study Area. Two of the crossings are at the northern and southern most parts of the Study Area. That leaves three crossings for 5,000 linear feet, or a crossing approximately every 1,250 ft. / quarter mile.

Smaller pockets of pedestrian-oriented spaces exist, but with the minimal connectivity and existing land uses, the area is relatively auto-oriented and auto-dependent despite having a FrontRunner Station and lots of employment. Adding to some of the difficulty in the area, the powerline corridor runs along Jordan Gateway restricting larger types of shade trees allowed in the park strips. Minimization of these shade trees along the predominant pathway hurts the comfort level and perceived safety of non-motorized users walking or biking along the corridor. These power corridors also require larger setbacks for buildings along this corridor, adding to the walking and biking distance for various uses and destinations within the area. These larger setbacks have been filled with surface parking lots further giving priority to

automotive access.

One positive of the powerline corridors which helps balance the lack of shade, distance, and visual barriers of the higher volume roadway, are the wider park strips beneath them. These park strips are landscaped and well-kept and maintained. They also contain lighting that is appropriately scaled for pedestrians. This level of detail adds to the area's overall pedestrian aesthetic, making it more inviting and safe for pedestrians than if the buffer didn't exist.

Despite the spread in connection points, clustering the east-west routes around heavier areas of foot traffic could enhance the functional interconnectedness of the public spaces, and various destinations within the Station Area. Additional opportunities also exist to enhance the pedestrian realm with additional pathways, pedestrian-scaled signage, furniture, temporary uses, art, and other elements which provide visual interest and visually compartmentalize the space for non-vehicle users. Key to these opportunities would be a bridge across the Jordan River with a strong connection between the FrontRunner Station and the Jordan River Trail. This would add to the accessible recreation opportunities for those who live and work within the Station Area.

The limited residential land use within the immediate Station Area contributes to the difficulty of viability of permanent restaurant and retail users outside of typical business hours. Hospitality uses can help, but infusing additional long-term residents will add to the range of services in the area helping create a more balanced mixed-use environment.

Future urban design efforts should prioritize the creation of stronger linkages between the natural and built environments, accessible open space, mixed-use infill with an emphasis on residential units, and pedestrian-scaled design elements.

E. Existing Environmental Conditions

A high voltage transmission line runs parallel and over the top of 400 West / Jordan Gateway road through the Study Area. This large electrical transmission power corridor is owned and operated by Rocky Mountain Power (RMP). Development is currently restricted here to avoid any impacts to and from this primary power transmission line for Salt Lake valley. Roadway, open space, and surface parking lots are currently being developed in this corridor by the City and private property owners in consultation with the Utility. RMP is a private utility company regulated by the public utility commission of Utah and is the primary electric utility in this area serving more than 1.2 million customers in Utah, Wyoming, and Idaho.

The Jordan River Corridor within the FrontRunner SAP Study Area has acres of delineated wetlands by the US Fish and Wildlife Service and is an important protected natural habitat for fish, birds, insects, and wildlife. During this stakeholder interview process, it was noted that several of

the property owners regularly observed mule deer, various fox species, large eagles, small mammals, and other wildlife utilizing the corridor. The Jordan River connects Utah Lake to the Great Salt Lake, and it is considered a Riverine System that continuously contains moving water.

The National Wetlands Inventory (NWI) was established by the US Fish and Wildlife Service (FWS) to conduct a nationwide inventory of U.S. wetlands to provide biologists and others with information on the distribution and type of wetlands to aid in conservation efforts. The Jordan River Corridor has the following National Wetland Inventory systems: River System Riverine, Subsystem Lower Perennials, Unconsolidated Bottoms, Water Regimes Permanently Flooded, Riparian Forested Shrub Wetlands, Freshwater Emergent Wetlands, Freshwater Forested/Shrub Wetlands, Freshwater Ponds, and Herbaceous Riparians.



National Wetlands Inventory

F. Multimodal Transportation Assessment

KEY TAKEAWAYS

- ◆ South Jordan Station ranks low in daily ridership at 465 compared to other FrontRunner stations.
- ◆ Some active transportation facilities exist in the Study Area with limited connectivity to the station.
- ◆ The Study Area has excellent vehicle access with inadequate parking availability.
- ◆ The public reports a desire to use UTA VIA service, but it is often not available.

Public Transit

Public transit typically includes buses, light rail, and shuttle routes. Public transit in South Jordan City is served by UTA. According to the ACS (2022), 2.1 percent of South Jordan

residents reported using public transit as their transportation mode to commute to work. This is compared to 1.8 percent of Utahns and 3.8 percent of people in the United States who use public transit as their transportation mode to commute to work.

[Existing transit service](#) in the Study Area is shown below in **Figure 8** and includes UTA On Demand, FrontRunner, and bus routes 201, F202, 218, and F514.

According to UTA's most recent ridership data from August to December 2024, the average weekday boardings and alightings at the South Jordan Frontrunner Station was 227 and 238, respectively. Shown below in the table are the surrounding FrontRunner station average daily boardings and alightings for comparison. As shown in the table, the South Jordan station has lower ridership than most stations.

TABLE 4: EXISTING TRANSIT SERVICE

Approximate frequency in minutes


Regular service		WEEKDAY			SATURDAY		SUNDAY	
Route		Peak	Day	Night	Day	Night	Day	Night
 FrontRunner		30	60	60	60	60	—	—
UTA On Demand		WEEKDAY			SATURDAY		SUNDAY	
Zone		Hours			Hours		Hours	
A South Valley		4 am - 12:15 am			4 am - 1:15 am		—	

TABLE 5: FRONTRUNNER EXISTING AVERAGE DAILY BOARDINGS AND ALIGHTINGS

FrontRunner Station	Provo Central	North Temple	Ogden Central	Murray Central	Salt Lake Central	Orem Central	Lehi	Layton	Farmington	Clearfield	Woods Cross	South Jordan	American Fork	Roy	Draper	Vineyard
Average Boardings & Alightings	1,814	1,668	1,551	1,281	1,148	892	663	616	586	508	469	465	427	404	378	343

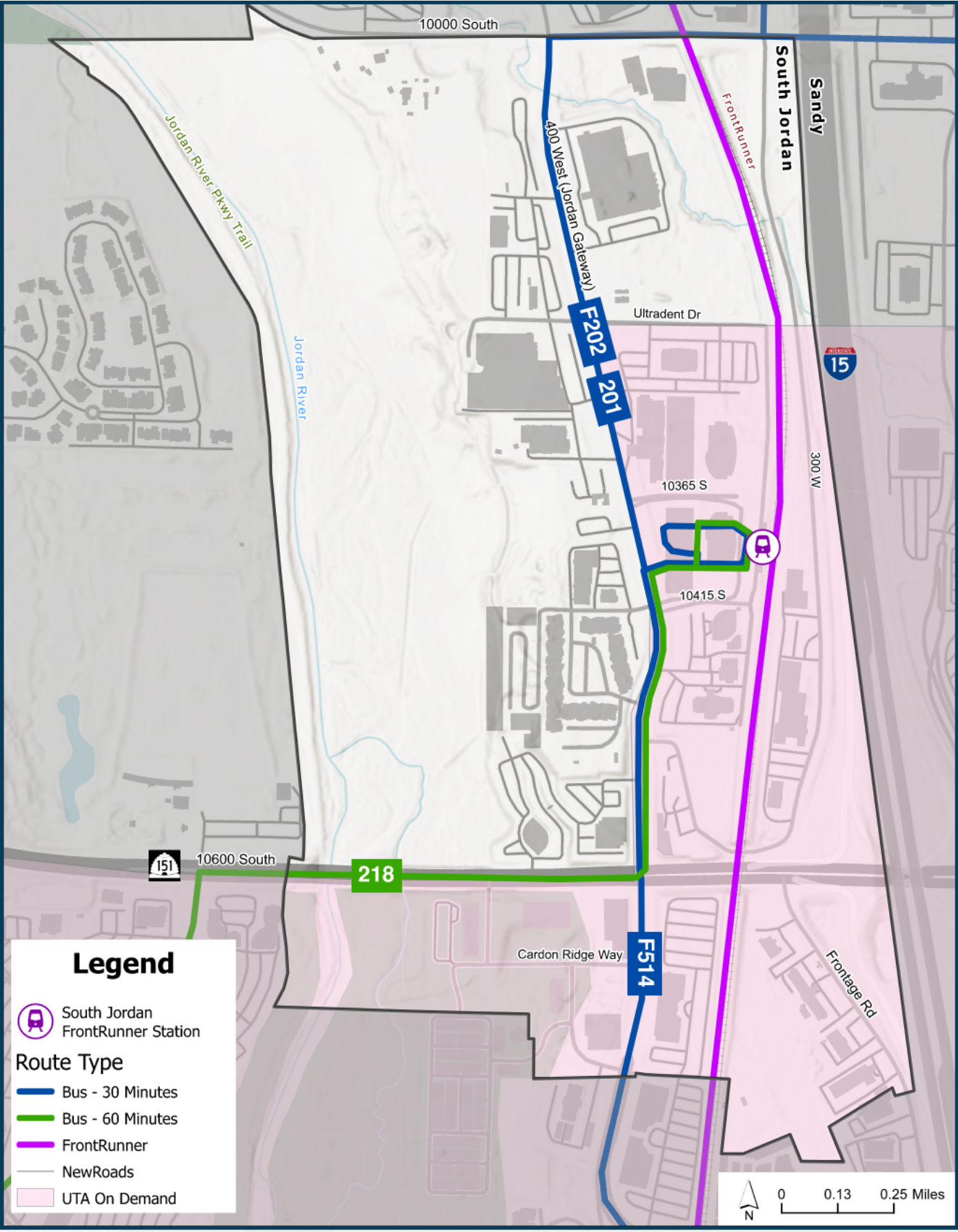


FIGURE 8: Study Area Existing Transit Service

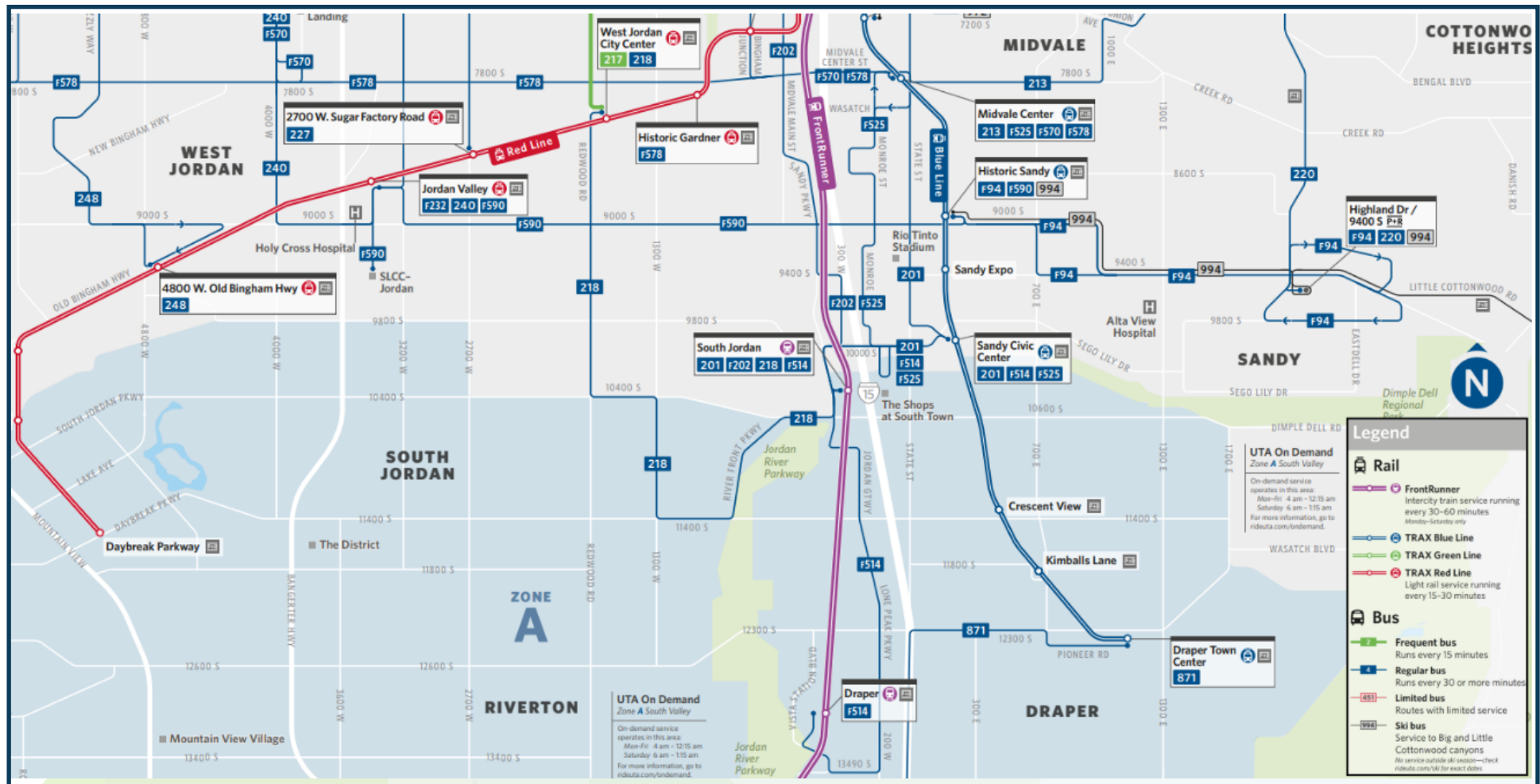


FIGURE 9: Regional Existing Transit Service

UTA On Demand Zone A is an innovative form of transportation that connects riders with other transit services like TRAX, FrontRunner, or bus as well as to other destinations in the community. The app-based technology is managed by [Via](#). Via technology matches multiple riders headed in a similar direction

and groups them into a single vehicle, allowing for quick and efficient shared trips. The UTA On Demand South Valley Zone and the FrontRunner operation schedule is shown in **Table 4**.

Active Transportation

Transit ridership relies on adequate bike and pedestrian facilities. An inventory of the existing active transportation facilities within the Study Area was conducted to find gaps in the network and opportunities for improvement. **Figure 10** shows the existing active transportation facilities. The Preferred Vision Plan will discuss plans that already exist to fill these and other gaps and make further recommendations as needed.

Vehicle Access

Preserving adequate vehicle access in the Station Area is essential for providing good transportation options for residents whose needs are not yet met by the transit or active transportation network in the first and last mile of service to the FrontRunner station. A majority of transit trips in Utah start and end with an automobile trip. Vehicle parking at the station is currently served by a shared parking agreement between adjacent businesses and UTA. **Figure 11** shows the existing roadway network with functional classifications and intersection control.

Public outreach efforts, including a recent community survey, have highlighted a recurring concern regarding parking availability at the FrontRunner station. Many respondents noted that the existing parking supply is limited and frequently operates at or near full capacity during peak travel times. Notably, 20% of survey participants specifically identified the need for additional parking as a top improvement priority. This feedback underscores the importance of addressing parking constraints to support current demand and accommodate future ridership growth.

In response to these findings, it is recommended that the City continue to collaborate on shared parking partners such as UTA, Ultradent, and other users where feasible with additional development. It is recommended the City consider a future parking study for the station to consider a transition from surface parking to structured parking. UTA is currently working on a study to look at a range of solutions to track utilization at the park and rides. The City may consider collaborating with UTA on this study to develop additional potential solutions.



FrontRunner Parking Lot

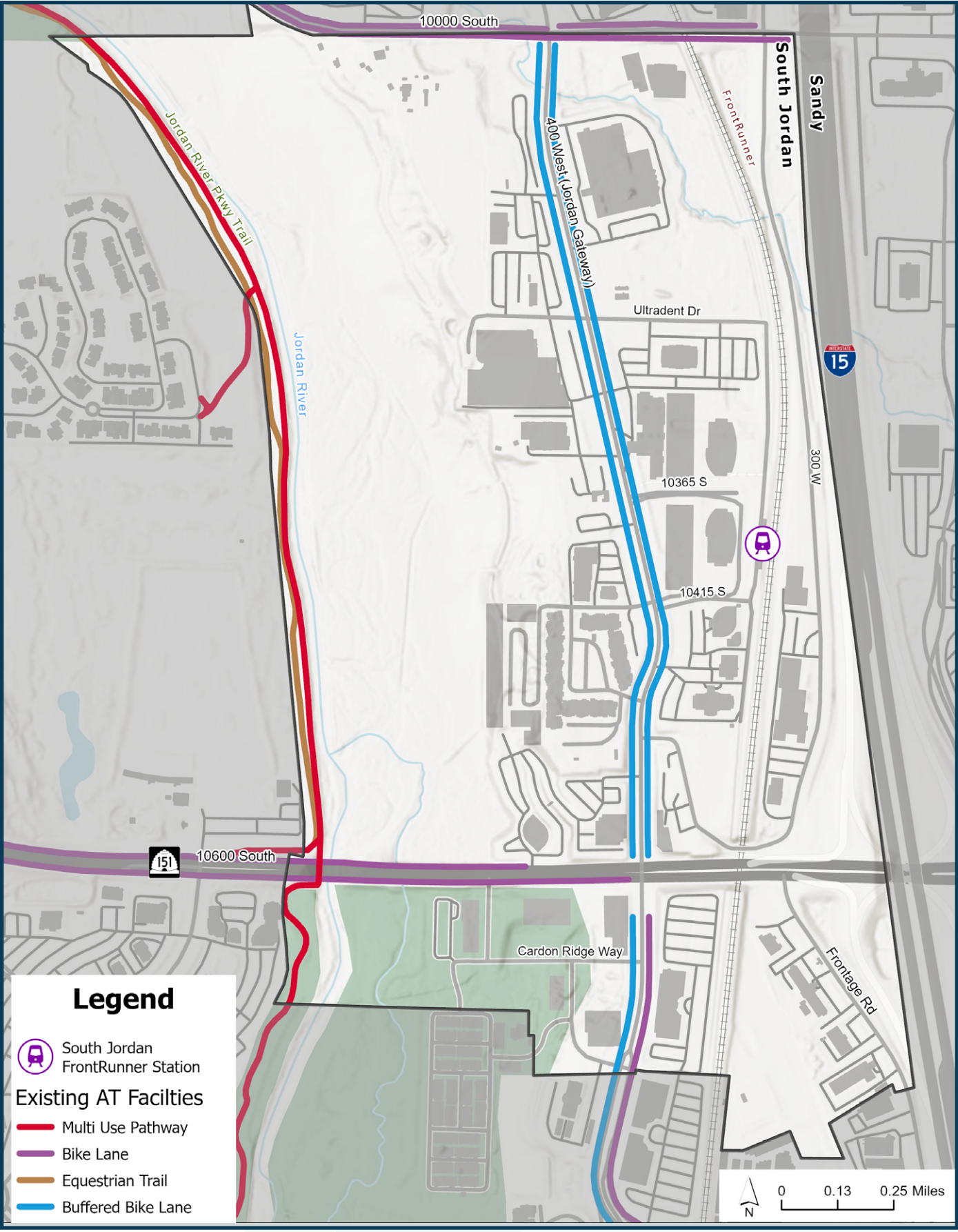


FIGURE 10: Existing Active Transportation Facilities

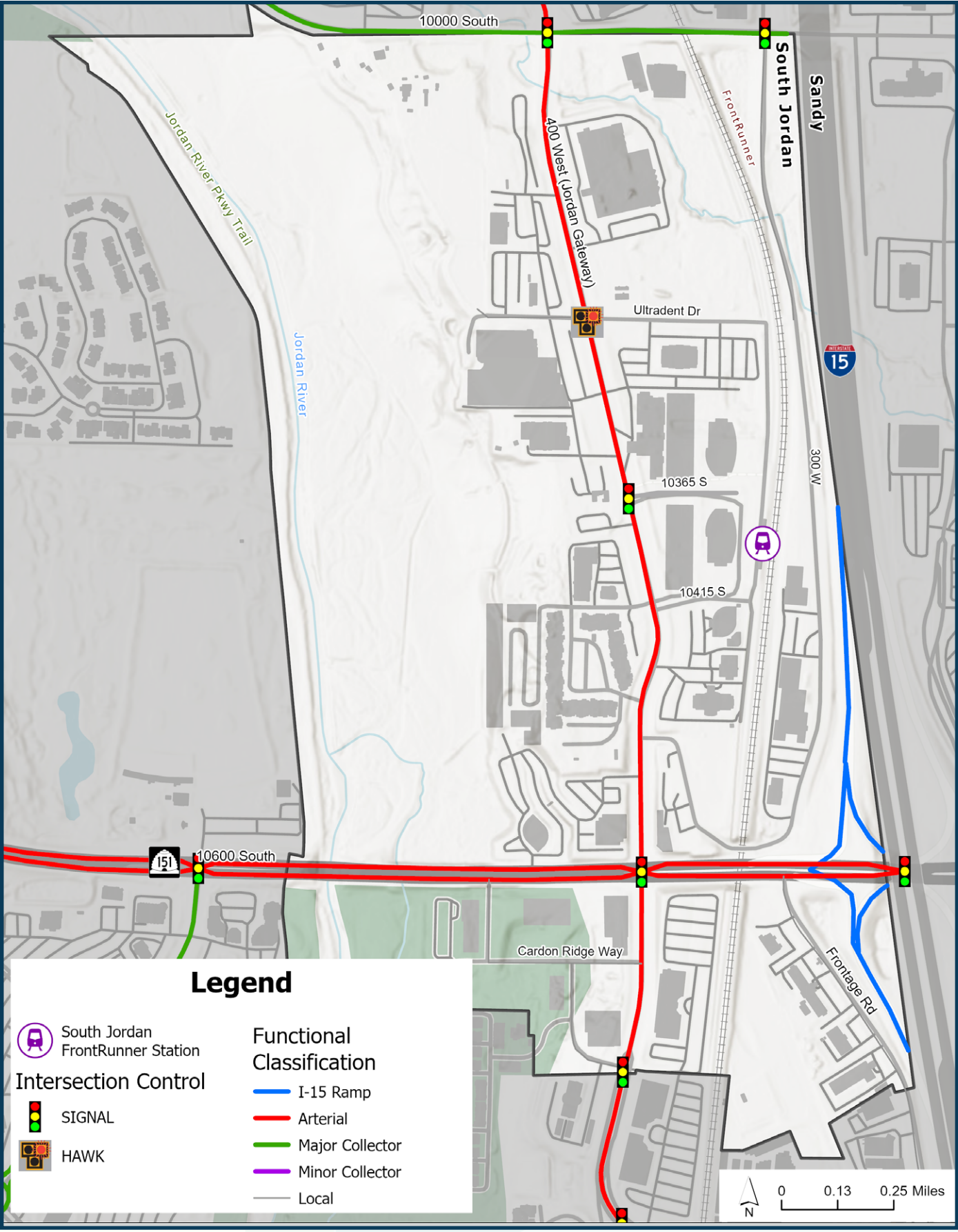


FIGURE 11: Existing Functional Class and Intersection Control

Pedestrian, Bicycle, and Street Connectivity

A roadway system with excellent connectivity allows people multiple options when traveling between points within a city. Strong collector and arterial road connectivity distributes traffic between corridors, and a well-connected local street network allows short trips to be completed on local roadways rather than relying on regional collectors and arterials. A connected road network improves access and reduces travel times for all users and can reduce the need for future roadway widening. Good network connectivity also improves emergency access and response times and allows multiple exit routes in the event of emergencies.

WFRC maps [connectivity scores](#) across the Wasatch Front. The polygons depict a point system based on the number of four-way and three-way intersections within each polygon (one point for four-way, 0.5 points for three-way). As shown in **Figure 12**, the Study Area scores poorly when compared to Salt Lake County. Several transportation barriers exist within the Study Area including I-15, the rail lines, and the Jordan River. Any opportunity to provide additional pedestrian, bicycle or street connections in the Study Area should be taken. Recommendations for improved transportation connections are covered in the Preferred Vision Plan.

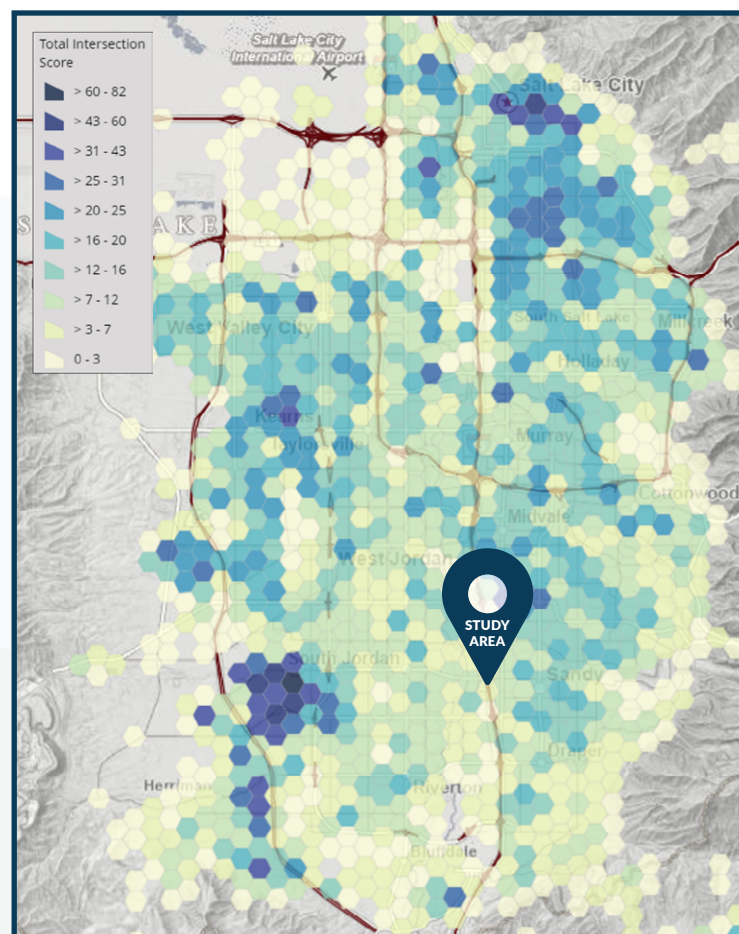


FIGURE 12: Salt Lake County Connectivity Scores

Safety

Safety is a crucial consideration for road users when considering whether to walk or bike to a destination. If the route to a transit station provides too little separation from traffic or active transportation crossings feel too exposed, fewer people will utilize the transit stations. A review of crash data in and around the Study Area was conducted to ascertain where concerning trends may be and to develop recommendations to improve the safety of the road network. The most recent five full years of available crash data (January 1, 2020 to December 31, 2024) from UDOT Traffic & Safety were used to perform a safety analysis. While severe crashes (those resulting in fatalities or serious injuries) were the primary focus of this analysis, all reported crashes are mapped in **Figure 13** to make trends more apparent.

Historic crash patterns of concern within the Study Area are listed below:

- ◆ **There were no fatalities and three serious injury crashes in the Study Area from January of 2020 to December of 2024.**
- ◆ The majority of crashes occurred along 10600 South (SR-151) and near the I-15 interchange.
- ◆ There was one pedestrian involved in a serious injury crash at the Jordan Gateway and 10600 South intersection. This intersection is a UDOT facility near a freeway interchange that experiences high traffic volumes and high speeds, and the crossing distance is long (130+ feet). Care should be taken to provide a more comfortable and safe crossing environment for pedestrians: It is recommended the City work with UDOT to consider a pedestrian leading interval at the intersection or prohibit right-turns on red from the side streets. It is also recommended that under-mast lights are installed to provide increased nighttime pedestrian visibility.

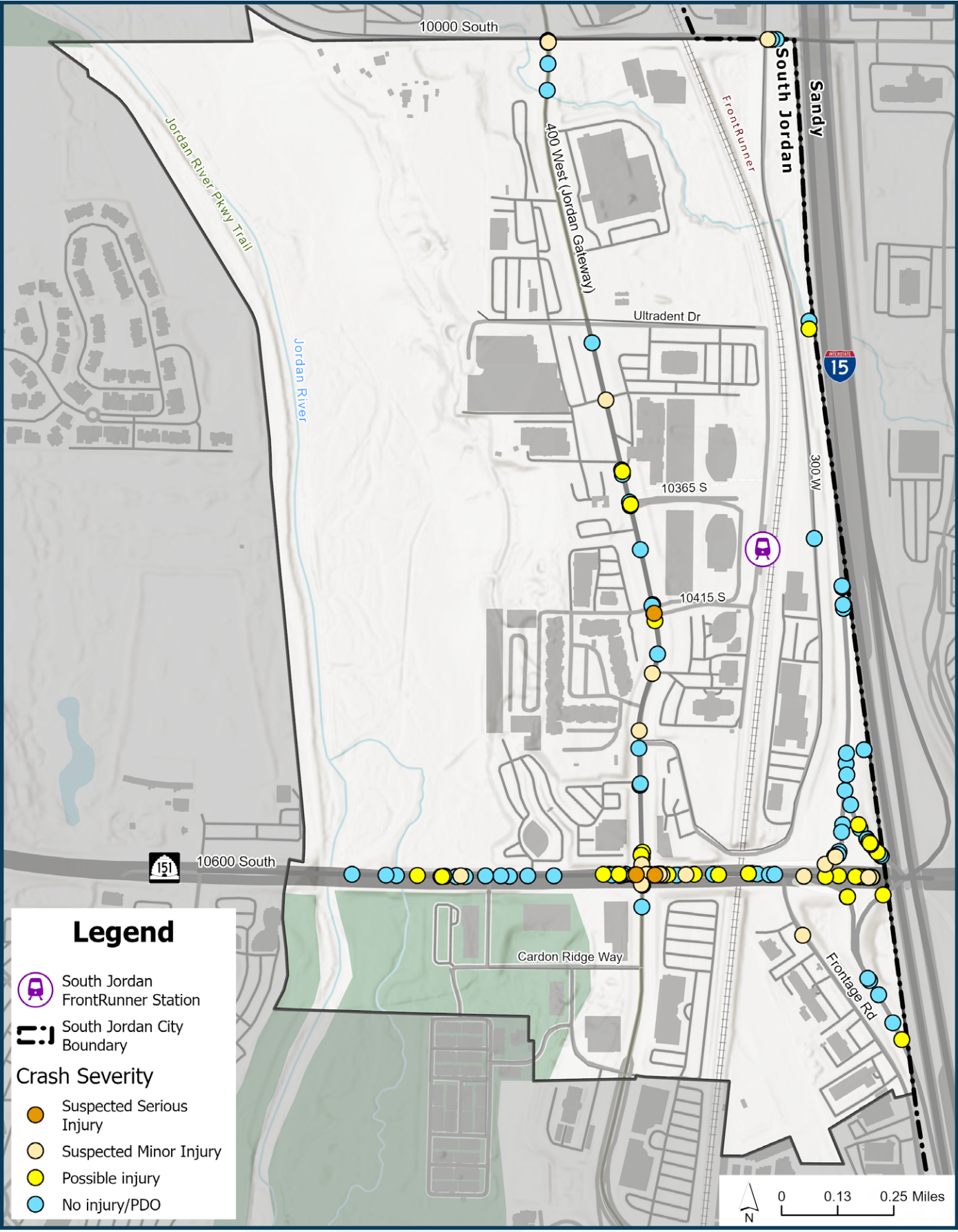


FIGURE 13: January 2020 - December 2024 Crash History

III. STAKEHOLDER AND COMMUNITY ENGAGEMENT

A. Stakeholder Engagement

KEY TAKEAWAYS

- ◆ Two large employment stakeholders are supportive of transit use and purchase corporate passes for employees as they recover from the pandemic.
- ◆ I-15 automobile access is very convenient with a high level of service.
- ◆ Public engagement shows the need for improved connectivity for walking and biking to and from the station—particularly to the east side of I-15.
- ◆ Public feedback also indicated a desire for additional food and beverage services in the broader station area.

Stakeholders were identified through Salt Lake County property ownership data in the area and obtaining critical resource management agencies' contact information. One-on-one meetings were arranged to understand the stakeholders' interest in the Study Area, establish the area's existing conditions, understand future development possibilities, and establish key recommendations for this Station Area Plan.

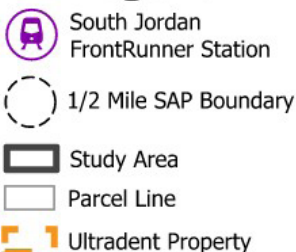
Property Ownership

Within the Study Area, approximately seventy percent of the land is privately owned. Of this, Ultradent Products Inc. owns about twenty percent. Other large private property owners include Pacificorp / Rocky Mountain Power, the Jerry Seiner Dealership group, the Marjean H Nelson; Harrison Family Trust, and the Todd Harrison Trust. For a comprehensive list of property owners, see Appendix.

Publicly-owned land includes nearly 49.2 acres of land controlled by the Federal Government, known as the Jordan River conservation area. State entities include UDOT, UTA, and the Department of Natural Resources which own other portions of the Jordan River corridor north and south through the Study Area. South Jordan City's ownership in the area includes the right-of-way where the Jordan Gateway exists at the core of the Station Area.



Jordan River Trail



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Rocky Mountain Power/PacifiCorp Interview - March 2025



The Rocky Mountain Power / PacifiCorp electric utility company is currently using all of the parcels that it owns in the station area. Future plans include the potential increase in substation capacity on the southern end of the planning area in a 6.4 acre parcel. These expansion activities include constructing additional infrastructure such as transformer banks and distribution capacity. The utility company is also planning upgrades to their north-south utility transmission line by increasing capacity from 46KV to 138KV in certain areas. In the future, there may also be a need for additional transmission lines in the existing north-south corridor if an increased capacity need arises.

The utility company does not plan to expand outside of their current property ownership footprint, and any future design plans under or adjacent to the utility requires a review by multiple departments in the company. Parking lots are considered appropriate and can be developed under transmission lines as long as they enable truck clearance and maintenance access for heavy vehicles.

Great Salt Lake Audubon, Chapter of the National Audubon Society - March 2025



Several existing master plans designate the Jordan river corridor as a high-priority conservation area. These include:

- ◆ Jordan River Open Space & Habitat Conservation Plan
- ◆ Jordan River Natural Conservation Corridor Report
- ◆ Jordan River Commission's Blueprint Jordan River: Refresh (2022)
- ◆ Utah Division of Natural Resources' Jordan River Comprehensive Management Plan and others

These plans are reflected in the City's General Plan now. Some of these restrict public access to preserve critical habitat areas to protect nesting bird species and other wildlife.

The Great Salt Lake Audubon Chapter recommends the City consider mitigating impacts to the river corridor through strategic habitat conservation, establishing additional buffer zones, preserving land in the floodplains and acquiring additional private lands in the river corridor for preservation.

They recommend that new development in the SAP include bird-friendly building design elements such as minimization of light pollution, noise pollution, minimized nighttime construction, bird-friendly glass, solar shading on windows, and planting of native vegetation. They recommend monitoring and removal of invasive plants to improve habitat function and encourage habitat restoration.

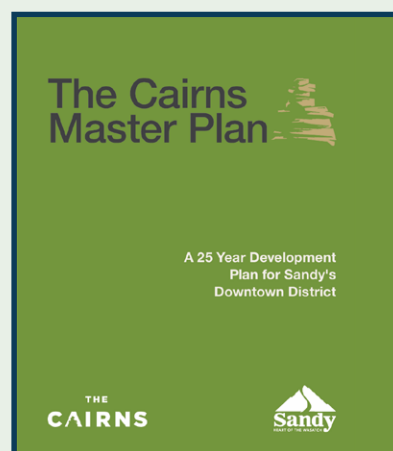
The chapter also encourages public recreation, including educational signage and trail connectivity, that is consistent with habitat preservation to support the recreational goals of the Jordan River Commission and its outdoor education and programs.

Sandy City Planning Department - January 2025



Sandy City sees the South Jordan FrontRunner Station as a key aspect of the Cairns Development District - tied into the city's downtown vision. Sandy City is planning for significant development near the South Towne Mall site, adding between 1100- 2800 additional housing units and potentially moving the HTRZ from the Target Department Store area further south into the South Towne Mall site. The current HTRZ near Target has four approved projects which total:

- ◆ 706 housing units
- ◆ A hotel and parking structure



Sandy City Planning Department - January 2025 (cont.)

Smith Entertainment Group recently acquired the South Town Mall property and is working on the south end of the mall site to build an ice hockey practice facility for the state's new NHL team. This in-progress development places an emphasis on community accessibility and benefit—providing recreation opportunities and youth activities in addition to the offices, locker rooms, and a practice ice rink for the NHL team. The project has an expected completion date in September 2025.



The Mall Site has existing zoning to support mixed-use development, including:

- ◆ A hotel
- ◆ Restaurant space
- ◆ 1,000 residential units

Each of these proposed land uses are currently approved. With new ownership, there is a strong desire for rebranding the site to look and function more like a mixed use and community-focused space to increase shopping, dining, and recreation offerings in Sandy.

To support future growth and mobility, Sandy City is preserving the right-of-way at 10200 South for a potential vehicular bridge, but acknowledges that South Jordan's Transportation Master Plan does not currently call for a

bridge at this location. Of more immediate interest in Sandy is improving pedestrian and bicycle connectivity as city officials see strong benefit in the following projects:

- ◆ Active transportation bridge over the freeway and railroad
- ◆ A potential extension of the AT connection to South Jordan FrontRunner further east, connecting to Dimple Dell Park
- ◆ An extension of the Shields Lane Shared Use Path to link with the North-South trail east of I-15 and into the Cairns District

Sandy also recently completed improvements to Monroe Street enhancing access into the South Towne Mall area. Sandy would like to see additional connections across I-15 to increase access to the South Jordan FrontRunner Station. Additional ideas and solutions include a bus or shuttle circulator route between South Jordan FrontRunner and Sandy's TRAX stations in partnership with UTA.

Utah Department of Natural Resources, Division of Forestry, Fire & State Lands, Jordan River Restoration Program - February 2025



This resource management agency is charged with invasive weed control, and revegetation work in the Jordan River corridor. It manages programs to control invasive Phragmites, invasive Russian Olive and Tamarisk and recommends best practices for revegetation along the Jordan River following control of invasive plant species. This includes replacement with native riparian trees, shrubs, forbs (flowering Herbaceous species), and graminoids (grasses and grass-like species).

See Appendix E for Best Practice documents that provide additional details and specific recommendations for the Jordan River's ecological management. This State of Utah Department and Division work directly with the Jordan River Commission, the Utah Reclamation Conservation Commission, the Great Salt Lake Audubon Society, South Jordan City and others to build partnerships and work towards common ecological management goals.

Jerry Seiner Buick, GMC, and Kia Auto Dealership of South Jordan - February 2025



The Jerry Seiner dealerships are a significant sales-tax-contributing entity in South Jordan City. The dealerships are situated between I-15 and the UTA and Union Pacific Railroad tracks. They are accessed from 300 West / Jerry Seiner Road.



Photo courtesy of Jerry Seiner Group

The dealerships don't have any existing plans in place to expand on their site, however, a future need to store additional inventory on site may be accommodated with a potential parking development. If a pedestrian bridge over I-15 is constructed, close coordination with Jerry Seiner will be necessary in order to avoid any operational interruptions and/or damage to their vehicle inventory.

Ultradent Products, Inc. - February 2025



Ultradent is a global oral health manufacturing and training company. They are the largest single property owner and employer in the station area. Ultradent is a family owned business—committed to long-term growth and development within their existing site. Ultradent leadership has a high level of engagement and personal ownership in the Station Area. The adjacent Jordan River corridor and wildlife presence is a key and desirable asset to their employee base and its protection should remain a high priority.

Following a few near collisions between pedestrians and automobiles, Ultradent has expressed strong support for a range of improvements to enhance pedestrian safety near its campus—particularly along the Jordan Gateway as employees frequently travel between buildings on the east and west sides of the roadway. Potential enhancements mentioned are improving visibility of the pedestrian hybrid beacon signal such as adding high-visibility road painting.

Additional safety enhancements on Ultradent's property include potential solutions such as ornamental fencing and controlled access gates with respect to the occasional transient population that occupies portions of land along the Jordan River Parkway Trail.



Photo courtesy of STO Building Group

As a major employer that supplies discount transit passes to its employees, Ultradent is also supportive of enhanced access to the trailhead at the Jordan River, provided adequate access control measures are in place. High importance items include balancing the need for enhanced pedestrian safety while maintaining proper ingress and egress for large delivery vehicles. In a stakeholder discussion with Ultradent, company leadership identified the following initiatives and potential opportunities:

Ultradent's Future Growth and Opportunities

- ◆ Seven to eight acres of property is allocated for future expansion activities in existing parking lots.
- ◆ There is consideration of a proposal to lease out a section of property for a year-round recreation facility that would serve employees, nearby offices, and hotels with pickleball and additional outdoor amenities.
- ◆ Southwest parking lot is currently barricaded for liability concerns and if a need arises, this may be made into a functioning parking lot.
- ◆ Dedicated space on site for seasonal food truck accommodations has support.
- ◆ There is support for a potential food venue space near building six.
- ◆ There is exploration of enhanced branding and visual recognition of Ultradent at the gateway to South Jordan.
- ◆ Freeway-facing signage
 - Digital signage displays
 - Visually-appealing flags on existing light poles
 - Campus-oriented signage to improve wayfinding

Utah Reclamation & Conservation Commission - February 2025



The Commission's responsibility under Central Utah Project Completion Act is to mitigate adverse effects to fish and wildlife resources caused by Central Utah Project's Bonneville Unit. A major impetus for environmental programs established under CUPCA was awareness that prior mitigation efforts had lagged behind CUP construction or were inadequate when measured against modern environmental Standards.

In 1997 the Commission started purchasing parcels in this area including a 44-acre parcel on the north side of 10600 South. In 1999, a second 17-acre parcel on the south side of 10600 South in South Jordan. In 2000, a conservation easement was donated to the Commission on about 35 acres along the Jordan River and adjacent to the second parcel. Other parcels were acquired to link with other undeveloped parcels.

In 2018 The Commission donated about 3.8 acres to Sandy City for their use and maintenance as open space in 2018 and supported the construction of an access road to Sandy Pond and other components of the Jordan River Parkway.

The Commission's Jordan River Watershed Program will pursue permanent transfer of its 49 acres parcel in the Study Area near 10600 South to suitable entities in order to address immediate and long-term management needs as they have done in Sandy.

Jordan River Commission - February 2025



The Jordan River Commission is a Utah Interlocal government agency of cities, counties, state agencies, and special districts working together to improve, restore and revitalize the Jordan River Parkway. The Jordan River Parkway Trail is a paved active transportation route running parallel to the Jordan River on the west side for the entire length of the station area. Guiding documents include 2022 Blueprint Jordan River Refresh, 2017 Jordan River Comprehensive Management Plan and the Best Practices for Riverfront Communities.

The Jordan River Commission is involved in a wide range of responsibilities pertaining to the Jordan River corridor including:

- ◆ Habitat and Natural Systems
- ◆ Water and the River
- ◆ Comfort and Inclusivity
- ◆ Recreation
- ◆ Safety and Wellbeing
- ◆ Development and Transportation
- ◆ Communications
- ◆ Education
- ◆ Art activities

The Jordan River Commission is the primary point of contact for the Jordan River Trail which includes providing consistent signage along the river and at trailheads, formalized boat launch and take-off points, and safety interventions regarding signage near boat hazards. The Commission is supportive of the proposed trail connection from the existing trail east across the Jordan River near 10200 South and connecting to the Jordan Gateway roadway.

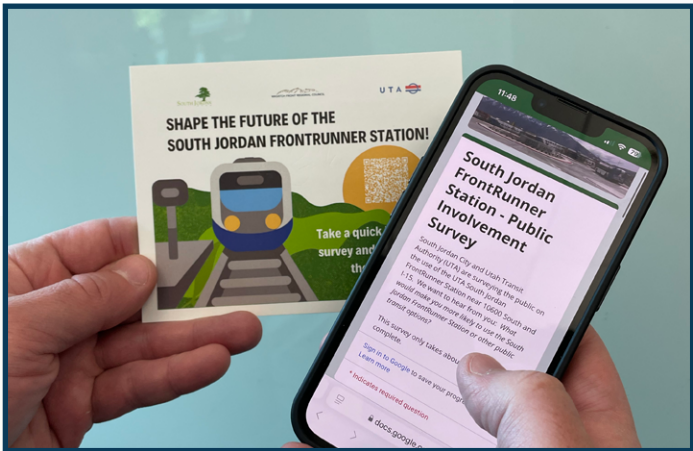
Altitude Residential Project (Proposed) by DAI Utah, Todd Harrison Property

This proposed 18-acre project is currently before the South Jordan City Council. It is located in the north west quadrant of the study area. As of April of 2025 this proposed project consists of a multi-unit residential development with a mix of townhomes and condominiums for a total proposed 222 equivalent residential units. The proposed project includes a Jordan River Corridor wetland expansion, future Jordan River Parkway Trail Bridge over the Jordan River and new trail connection up to Ultradent Drive.






B. Community Engagement

The planning process utilized a mix of in-person and online engagement opportunities to maximize input, informing the vision for the future of the station area. This approach kept the process inclusive and responsive to needs and aspirations of area stakeholders and the community. Residents and stakeholders provided their feedback and recommendations on land use and transportation in the station area, the station itself, and using public transit more broadly.



QR Code to Survey

SHAPE THE FUTURE OF THE SOUTH JORDAN FRONTRUNNER STATION!



Take a quick 5-minute survey and share your thoughts!

HOW CAN THE SOUTH JORDAN FRONTRUNNER STATION WORK BETTER FOR YOU?

The City of South Jordan and Utah Transit Authority (UTA) want to hear your thoughts on how to make FrontRunner and bus service at the South Jordan Station more convenient, comfortable, and accessible.

☒


WHAT WOULD MAKE YOU MORE LIKELY TO USE TRANSIT?

☒

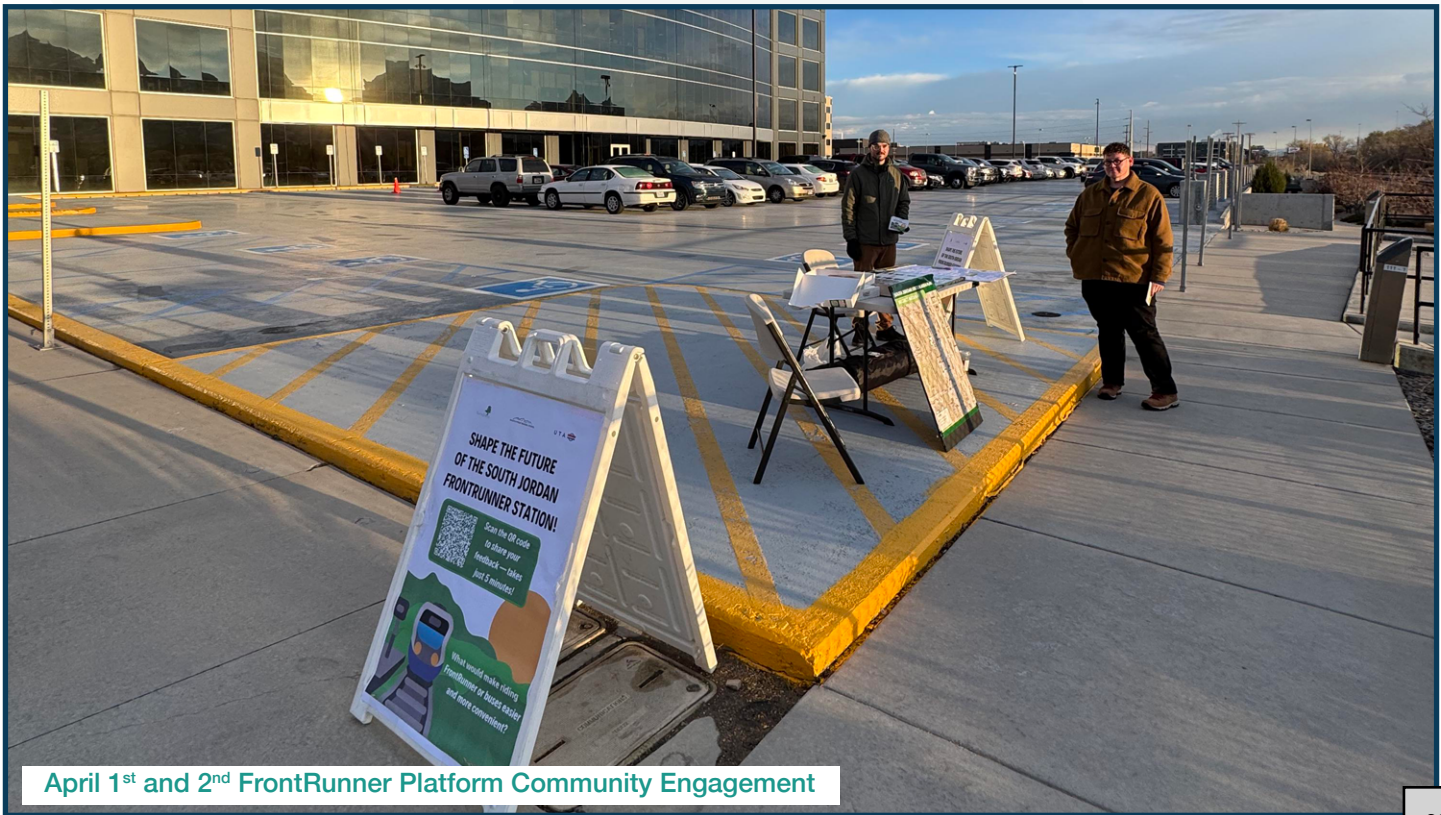
WHAT AMENITIES OR IMPROVEMENTS WOULD MAKE YOUR TRIP EASIER?



SCAN THE QR CODE TO TAKE A QUICK 5-MINUTE SURVEY!



Digital Post Card Handout



April 1st and 2nd FrontRunner Platform Community Engagement

Digital Survey and Postcard

March-April 2025

A brief community survey was developed to ask residents, businesses and transit riders for their ideas on ways to enhance the overall station area and suggestions for making UTA FrontRunner and bus service a more convenient and desirable transportation option. South Jordan City and UTA assisted in the creation of the project survey, utilizing both entity's prior best practices in gathering public input. The City distributed the survey through their E-Newsletter and promoted it on their social media platforms. Staff distributed 500 postcard handouts with informational material and a QR code to all businesses and residential buildings in the Study Area that could accept the handouts (ie. Social Security Administration does not allow distribution of this type of material, etc.) Postcard handouts were distributed to the following:

- ◆ Lucid Software
- ◆ Employee Navigator
- ◆ Embassy Suites
- ◆ Ultradent
- ◆ Holiday Inn
- ◆ Pinned Coffee Shop
- ◆ Jordan Station Apartments
- ◆ Evolve Wellness
- ◆ Momentum Development
- ◆ Cypress
- ◆ Motiv Health
- ◆ UBIC
- ◆ Jerry Seiner GMC and Kia Offices
- ◆ Presidio Real Estate
- ◆ Marshal Gates foundation
- ◆ BLOC pharmacy
- ◆ RCG Law Group
- ◆ Merrick Bank
- ◆ Sleep Inn
- ◆ La Quinta
- ◆ Phoenix Recovery
- ◆ NFP

- ◆ Luxury Executive Suites
- ◆ Clear Vision Institute
- ◆ XIMA
- ◆ York Howell & Gymon
- ◆ Integrated Financial Group
- ◆ Arbor Real Estate
- ◆ Presidio Real Estate
- ◆ 10656 Windsor Ridge Way Apartments
- ◆ Bryan S Finkie Building

These materials were also emailed to property owners and stakeholders in the area as a follow up to the one on one discussions held previously in the process with the project team.

Station Platform Engagement

On April 1st and 2nd 2025, South Jordan City and UTA personnel staffed a public engagement booth near the FrontRunner platform during peak AM and PM commute times each day (7:00 AM-9:00 AM and 4:00 PM-6:00 PM). A postcard with a QR Code to the survey, flyers, area maps with potential improvements and a freeform comment board were included, providing a more interactive engagement for transit riders.



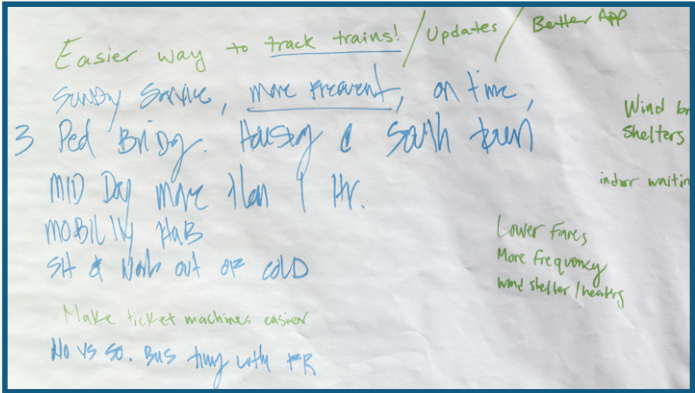
Platform Setup

Public Input and Key Messages

263 total survey responses were received and analyzed by the project team. Community input gathered both in-person and online found recurring sentiments of a desire for enhanced pedestrian and bicycle connections, improved transit connectivity and schedule alignment across various transit modes that converge at the station: FrontRunner, TRAX, bus service, and UTA On-Demand. Other key messages and recommendations from transit riders and area stakeholders included:

1. Increased commuter rail frequency and expanded service hours
2. Additional food options in the station area
3. First and last-mile pedestrian and bicycle connectivity across I-15 and into Sandy
4. Increased connections to the Jordan River Parkway Trail
5. Additional station amenities and services–warming shelters, restrooms, food and beverage offerings

Of the 263 responses received, 38.2% of respondents ride transit daily or a couple of times a week. 46.9% of respondents rarely or never ride transit. Over half of the survey respondents selected “Traveling to Work” as their primary purpose for using the South Jordan FrontRunner Station. This insight underscores the importance of tailoring future improvements to better support both regular commuters and potential new transit users.



What would make riding transit easier?

Increased transit, pedestrian, and bicycle connections to Sandy on the east side of I-15 were the most frequently cited improvement by respondents to the survey. Other desired improvements include increased shade and tree canopy in the station area as well as enhanced accessibility to the Jordan River Trail.

1) How often do you ride FrontRunner to or from South Jordan? Choose one.

241 responses

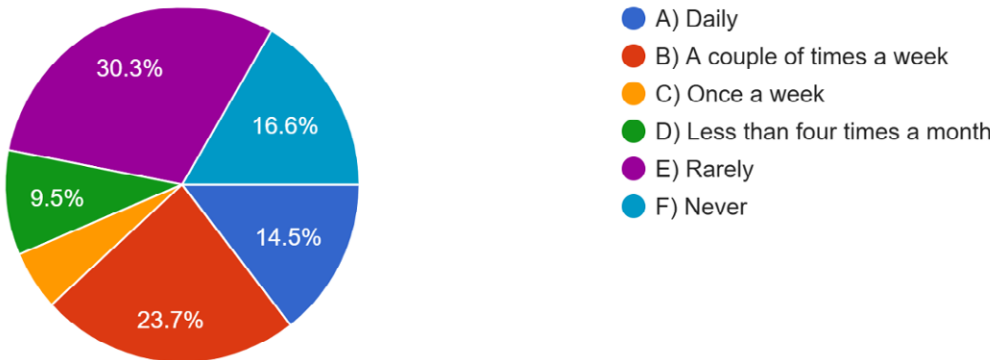


FIGURE 15: Station Usage - Survey Results

Question seven of the survey asked respondents to choose five improvements based on a list of twelve potential improvements for the station area. Results found that Increased transit connections to South Towne and TRAX was the most frequently selected improvement desired.

TABLE 6: STATION AREA IMPROVEMENTS - SURVEY RESULTS

Rank	Improvement
1	Increased transit connection to South Towne and TRAX
2	Increased shade with trees or canopies
3	Potential pedestrian bridge over freeway
4	Connect the Jordan River Parkway Trail to the FrontRunner Station
5	Increased Jordan Parkway crossing safety and bike lanes
6	Pop up food truck plaza
7	Increased open space and public recreation opportunities
8	Increased access to Jordan River, bird refuge, passive recreation opportunities
9	Additional parking
10	Increased residential homeownership opportunities
11	Increased directional and wayfinding signage
12	Expanded employment opportunities

Together, the community and area stakeholders priorities reflect a strong desire for a more connected, comfortable, and accessible station area that accommodates active transportation options and adds to the quality of the public transit experience and desirability of supporting land uses. Additional enhancements such as facilities,

increased shade, and tree canopy would improve the daily functionality of the station and contribute to a stronger sense of place at the station, creating long-term community value. Safe, intuitive, and attractive connections are key to unlocking the station area's full potential.

Pedestrian access to FrontRunner platform

IV. MARKET ANALYSIS

KEY TAKEAWAYS

- ◆ Support and encourage office campus infill and development
- ◆ Short-term office development is stagnant in the broader Salt Lake County market with vacancies up from year previous, long-term forecast is positive for office development at this site
- ◆ Support and encourage multi-family development, demand is high
- ◆ Retail demand is low due to lack of roof tops and competition nearby

A. Parcel Data Evaluation

Parcel characteristics for the Study Area and City were calculated and sorted between land use categories as outlined below. The distribution of land uses within the Study Area is shown below, with the greatest number of parcels being attributed to the “other” category. The “other” category generally comprises of government buildings and other exempt properties. The second most common property type is office, with 38 percent of the taxable value and 44 percent of the market value attributed to office property types. Commercial and industrial parcels also contribute significantly to total square footage and taxable and market values. Agricultural land, which may be converted for other uses in the future, comprises 13 percent of total acreage.

TABLE 7: STUDY AREA PARCEL CHARACTERISTICS

Category	Count	Acreage	Square Footage	Taxable Value	Market Value
Single-Family	1	0.50	2,718	\$444,600	\$244,530
Multifamily	3	11	609,639	\$145,862,780	\$80,346,992
Commercial	11	29	496,242	\$81,274,000	\$81,274,000
Office	19	48	999,107	\$220,826,800	\$217,582,320
Industrial	3	15	464,465	\$100,924,900	\$100,924,900
Vacant	17	41	-	\$12,060,900	\$11,181,245
Agricultural	2	37	1,480	\$5,476,690	\$120,020
Other	52	109	300	\$20,684,600	\$2,429,400
Total	108	291	2,573,951	\$587,555,270	\$494,103,407

Source: Salt Lake County Parcel Data

Commercial land use types include Restaurants, automotive related (dealerships), and hotels.

Industrial consists of light manufacturing, and industrial R&E.

TABLE 8: SOUTH JORDAN PARCEL CHARACTERISTICS

Category	Count	Acreage	Square Footage	Taxable Value	Market Value
Single-Family	23,902	6,06	78,205,923	\$8,604,905,533	\$15,730,310,470
Multifamily	910	204	5,271,468	\$707,402,825	\$1,351,316,960
Commercial	286	724	5,662,083	\$1,327,606,535	\$1,422,963,980
Office	328	504	5,258,929	\$1,006,998,050	\$1,289,312,990
Industrial	28	175	2,735,300	\$471,212,169	\$472,197,950
Vacant	1,344	1,353	167,401	\$388,780,033	\$543,537,780
Agricultural	101	2,740	80,291	\$23,104,983	\$261,225,860
Other	2,502	2,633	3,858,279	\$150,327,096	\$1,307,851,780
Total	29,401	14,396	101,239,674	\$12,680,337,229	\$22,378,717,770

Source: Salt Lake County Parcel Data

As shown in **Table 8** and **Figure 16**, the City's overall parcel count, square footage, and taxable and market values illustrate a concentration of single-family development. At this scale, multifamily, commercial, and office uses

represent similar proportions of square footage and taxable and market values. Vacant and agricultural land in the City respectively represent roughly nine and 19 percent of total acreage.

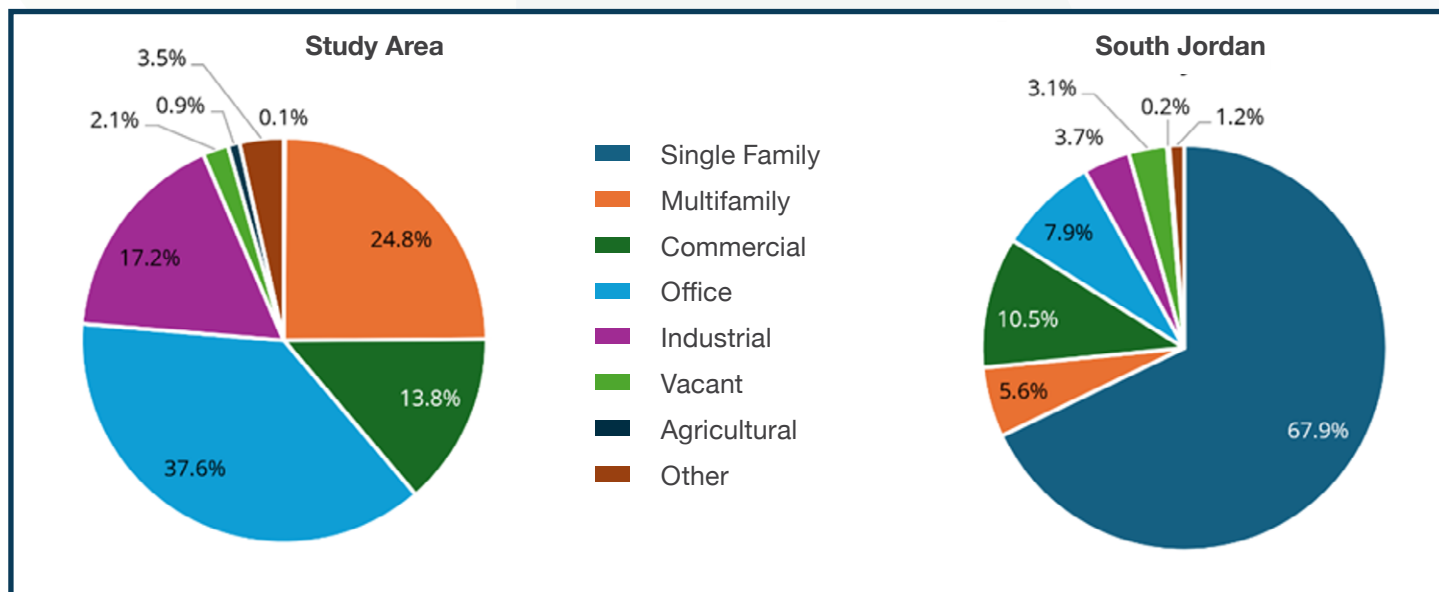


FIGURE 16: Total Taxable Value Distribution by Land Type

B. Sales Leakage

Utilizing 2023 taxable sales collection data from the Utah State Tax Commission, a sales leakage analysis was completed for the City to provide an overview of leakage and retention by commercial category in comparison to the State of Utah.

TABLE 9: SOUTH JORDAN SALES LEAKAGE

	South Jordan Direct Taxable Sales	Per Capita Spending	Utah Income Adjusted Per Capita Spending	Capture Rate	Per Capita	Total Leakage
RETAIL						
Building Material & Garden Equip	\$17,445,442	\$206	\$2,104	10%	(\$1,898)	(\$160,369,346)
Clothing & Accessories	\$86,704,186	\$1,026	\$937	110%	\$89	\$7,562,165
Electrical & Appliance	\$54,555,675	\$646	\$561	115%	\$85	\$7,152,386
Food & Beverage	\$157,951,160	\$1,869	\$2,468	76%	(\$599)	(\$50,610,579)
Furniture & Home Furnishing	\$11,738,847	\$139	\$495	28%	(\$357)	(\$30,130,435)
Gas Station	\$25,500,000	\$302	\$728	41%	(\$426)	(\$36,011,200)
General Merchandise	\$430,593,931	\$5,095	\$3,574	143%	\$1,522	\$128,598,559
Health & Personal	\$15,099,393	\$179	\$358	50%	(\$180)	(\$15,171,812)
Miscellaneous Retail Trade	\$45,477,702	\$538	\$993	54%	(\$455)	(\$38,411,430)
Motor Vehicle	\$467,611,596	\$5,533	\$3,841	144%	\$1,692	\$142,998,332
Nonstore Retailers	\$231,507,425	\$2,739	\$2,933	93%	(\$194)	(\$16,366,955)
Sporting Good	\$19,739,196	\$234	\$578	40%	(\$345)	(\$29,120,222)
Wholesale Trade-Durable Goods	\$94,984,697	\$1,124	\$2,859	39%	(\$1,735)	(\$146,623,377)
Wholesale Trade-Electronic Markets	\$10,384,280	\$123	\$478	26%	(\$355)	(\$30,015,480)
Wholesale Trade-Nondurable Goods	\$1,632,228	\$19	\$66	29%	(\$47)	(\$3,940,608)
Total Retail	\$1,670,925,758	\$19,772	\$22,973	86%	(\$3,200)	(\$270,460,003)

TABLE 9: SOUTH JORDAN SALES LEAKAGE (Continued)

	South Jordan Direct taxable Sales	Per Capita Spending	Utah Income Adjusted Per Capita Spending	Capture Rate	Per Capita	Total Leakage
INDUSTRY						
Agriculture, Forestry, Fishing & Hunting	\$35,995	\$0	\$17	3%	(\$16)	(\$1,360,181)
Construction	\$7,743,235	\$92	\$676	14%	(\$585)	(\$49,425,904)
Information	\$111,241,490	\$1,316	\$1,353	97%	(\$36)	(\$3,058,397)
Manufacturing	\$38,967,712	\$461	\$1,727	27%	(\$1,265)	(\$106,942,941)
Mining, Quarrying, & Oil & Gas Extraction	\$6,492,286	\$77	\$243	32%	(\$166)	(\$14,002,368)
Transportation & Warehousing	\$2,888,626	\$34	\$135	25%	(\$101)	(\$8,531,492)
Utilities	\$86,624,581	\$1,025	\$1,303	79%	(\$278)	(\$23,510,656)
Total Industry	\$253,993,925	\$3,006	\$5,453	55%	(\$2,447)	(\$206,831,939)
SERVICES						
Accommodation	\$27,237,338	\$322	\$1,231	\$0	(\$909)	(\$76,824,497)
Admin. & Sup & Waste Man.& Remed. Services	\$7,628,045	\$90	\$165	55%	(\$75)	(\$6,336,631)
Arts, Entertainment, and Recreation	\$11,435,904	\$135	\$523	26%	(\$388)	(\$32,776,128)
Educational Services	\$5,173,506	\$61	\$85	72%	(\$24)	(\$1,990,231)
Finance & Insurance	\$11,605,965	\$137	\$145	94%	(\$8)	(\$678,601)
Food Services & Drinking Places	\$194,041,780	\$2,296	\$2,874	80%	(\$578)	(\$48,857,296)
Health Care & Social Assistance	\$5,090,342	\$60	\$71	85%	(\$11)	(\$903,007)
Management Of Companies & Enterprises	\$350,000	\$4	\$13	32%	(\$9)	(\$757,935)
Other Services-Except Public Administration	\$45,117,694	\$534	\$915	58%	(\$381)	(\$32,236,042)
Professional, Scientific, & Technical Services	\$76,831,996	\$909	\$801	113%	\$108	\$9,114,324
Public Administration	\$2,500,000	\$30	\$133	22%	(\$103)	(\$8,717,042)
Real Estate, Rental, & Leasing	\$32,150,762	\$380	\$931	41%	(\$551)	(\$46,545,944)
Total Services	\$419,163,332	\$4,960	\$7,889	63%	(\$2,929)	(\$247,509,030)

TABLE 9: SOUTH JORDAN SALES LEAKAGE (Continued)

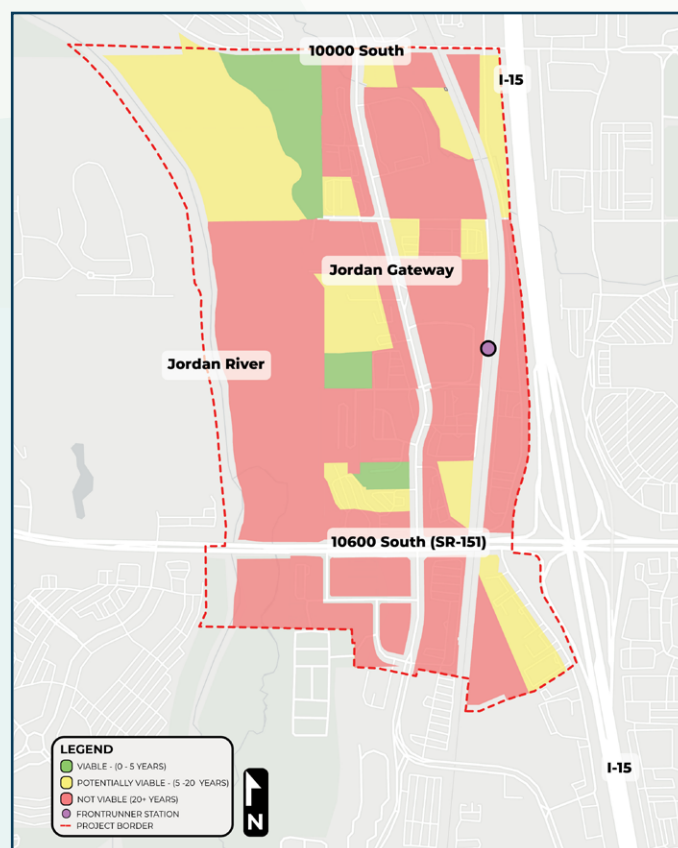
	South Jordan Direct taxable Sales	Per Capita Spending	Utah Income Adjusted Per Capita Spending	Capture Rate	Per Capita	Total Leakage
OTHER						
Private Motor Vehicle Sales	\$54,500,000	\$645	\$849	\$1	(\$204)	(\$17,221,156)
Special Event Sales	\$741,726	\$9	\$38	23%	(\$29)	(\$2,472,080)
Unknown/Non-classifiable	(\$14,000)	(\$0)	\$1	-24%	(\$1)	(\$72,532)
Prior-Period Payments & Refunds	\$626,450	\$7	\$364	2%	(\$357)	(\$30,158,066)
Total Other	\$55,854,176	\$661	\$1,252	53%	(\$591)	(\$49,923,834)
TOTAL COMBINED	\$2,399,937,191	\$28,399	\$37,566	76%	(\$9,167)	(\$774,724,806)

The City is leaking in all major categories in comparison to State average spending. The per capita spending in South Jordan is \$28,399, a figure 24.4 percent lower than the State's average of \$37,566. The total taxable sales leaking to other communities is estimated at just below \$775M. Assuming a sales tax levy of 0.5 percent based on point of sale, this equates to a loss of \$7.3M in annual sales tax revenues. In comparison, the per capita spending in West Jordan is \$23,266 and has a capture rate of 82 percent when compared to the State. Factors influencing a community's capture rate include total population, proximity to major freeways or surface streets, the population within a 360-degree trade area, geographic isolation, and competitive market sites.

Supportable Zoning

The South Jordan Gateway Study (2013) assesses transportation, land use, and future development in the City's Gateway District, which surrounds the FrontRunner Station. The Study primarily focuses on the opportunity for transit-oriented development around the FrontRunner Station. Transit-oriented development aims to use existing transit infrastructure to contribute to dense and diverse development use near public transportation. With limited retail demand in the area, the Study Area can benefit from planning for infill development, primarily of office and residential land use for the area.

The map below illustrates development and redevelopment opportunities within the Study Area. The map identifies four areas that are currently available for infill office, hospitality, or residential development.



Development and Redevelopment Opportunities

Market factors at the county-level indicate Salt Lake County has experienced stagnant office construction in the second quarter of 2024. Office vacancies are at 17.94 percent (Q2 2024), an increase of 2.5 percent from the previous year.

SL County Development Highlights: Office

- ◆ Permitting of office structures has been stagnant.
- ◆ Direct office vacancy is at 17.94 percent (Q2 2024), an increase of 2.5 percent from 2023.
- ◆ Increased vacancy rates and decreased sublease availability indicates a cautious market.
- ◆ It is expected that lower vacancy will allow developers to initiate new projects in the future.

To determine the supportable commercial zoning within South Jordan and the Study Area, this analysis evaluates future taxable sales growth, per capita spending by sector, and general retail and office zoning ratios from the City as a whole. Using two different methodologies, this analysis provides an estimate of supportable acreage by the following categories: general retail, industry, services, and total commercial acreage. The following table provides a summary of South Jordan's sales leakage (See **Table 9** for more details).

TABLE 10: SUMMARY OF SOUTH JORDAN SALES LEAKAGE

	South Jordan Direct Taxable Sales	Per Capita Spending	Utah Income Adjusted Per Capita Spending	Capture Rate	Per Capita	Total Leakage
Retail Subtotal	\$1,670,925,758	\$19,772	\$22,973	86%	(\$3,200)	(\$270,460,003)
Industry Subtotal	\$253,993,925	\$3,006	\$5,453	55%	(\$2,447)	(\$206,831,939)
Services Subtotal	\$419,163,332	\$4,960	\$7,889	63%	(\$2,929)	(\$247,509,030)
Other Subtotal	\$55,854,176	\$661	\$1,252	53%	(\$591)	(\$49,923,834)
Total	\$2,399,937,191	\$28,399	\$37,566	76%	(\$9,167)	(\$774,724,806)

The first methodology employed in this analysis utilizes estimated per capita spending of \$28,399 in South Jordan. Assuming a new population of 48,639 residents by 2050 in the City using TAZ estimates, the total supportable zoning is estimated at 5M square feet. This assumes a median sales volume of \$275 per square foot of gross leasable area.

TABLE 11: SOUTH JORDAN SUPPORTABLE COMMERCIAL ZONING BASED ON PER CAPITA SPENDING

	General Retail	Industry	Services	Other	Total
Per Capita Spending (State Income Adjusted)	\$19,772	\$3,006	\$4,960	\$661	\$28,399
New Growth in City (2050)	48,639	48,639	48,639	48,639	48,639
Total Spending	\$961,717,348	\$146,188,640	\$241,253,476	\$32,147,407	\$1,381,306,870
Median Sales Volume Per Sq. Ft. of GLA	\$275	\$275	\$275	\$275	\$275
Supportable Sq. Ft.	3,497,154	531,595	877,285	116,900	5,022,934

Source: WFRC TAZ Estimates

Employing an alternative methodology produces a slightly higher supportable square footage estimate as illustrated in the following tables. Using the 2024 City employment as a benchmark, an average of SF per job is calculated at 192.1 for office and 710.8 for commercial. Based on new

job growth in South Jordan using TAZ estimates and the average SF per job, the supportable commercial square footage results in 5.6M SF for office and 1.6M for retail, totaling a combined 7.3M SF.

TABLE 12: SOUTH JORDAN COMMERCIAL SQ. FT. PER JOB

	South Jordan Employment (2024)	Zoned SF	SF Per Job	New Job Growth in South Jordan (2024-2050)	Supportable SF
Office Use	27,376	5,258,929	192.1	29,589	5,684,199
Retail Use	7,966	5,662,083	710.8	2,373	1,686,347

Economic development research and concept plans have been completed by the City, developers, and other stakeholders due to an existing station area consisting of Downtown Daybreak, an HTRZ, and the Daybreak Field at America First Square, a new ballpark coming to South Jordan in 2025. The table below provides the current planned square footage applicable to the Daybreak Area.

While the City shows supportable square footage under both methodologies, existing plans envision that future retail and office development will occur Daybreak. As a result, development and redevelopment opportunities in the FrontRunner Study Area will primarily focus on infill development consisting of office, residential, and dining.

TABLE 13: DAYBREAK URBAN CENTER PROGRAM AND ABSORPTION FOR USE IN TAZ ZONES (2025 – 2050)

	Total Projected Units	Total Projected Retail SF	Total Projected Office SF
SS CDA /Urban Center Core	434	307,034	870,652
Urban Center Core	4,815	1,491,066	3,603,108
North Station	2,666	150,000	2,318,524
TOTAL	7,915	1,948,100	6,792,283

Source: WCG and Hodges Design

The supportable zoning analysis indicates that the Study Area can benefit from planning for infill development for the area due to limited retail demand in the area. Infill developments of focus in the area include office (i.e., healthcare, dental, technology sectors) and multi-family.

The City should support and encourage development and redevelopment opportunities that are currently available, as shown in the map above. The following table summarizes the opportunities and challenges in the Study Area regarding office, retail, hospitality, and residential development.

TABLE 14: SUMMARY OF ECONOMIC OPPORTUNITIES AND CHALLENGES IN THE STUDY AREA

Category	Demand	Opportunities	Challenges	Target Industry
Office	High	Corporate office expansion Office campus development Infill development	Property owner's willingness to expand Land availability Site stability and grading issues	Healthcare Dental Technology Public Administration
Retail	Low	Eating establishments *Storage units *Automotive	Land availability Site stability and grading issues Competition from adjacent retail (in City and out of City)	Food Trucks, Restaurants *High End Storage (vertical) *Car Dealerships
Hospitality	Moderate	Hospitality	Property owner's willingness to redevelop Land availability	Hotel Hotel Redevelopment
Multifamily	High	Infill development Mixed use development	Willingness from property owners Land availability Site stability and grading issues Quality of product	Vertical high density

*This use should be limited to areas not easily accessible on foot from the station.



C. Potential Barriers

In addition to the challenges provided above in **Table 14**, some development may be impacted by factors that serve as barriers toward unconstrained development growth within the community. Proximity to a regional transportation network such as I-15 allows communities to attract larger developments like distribution centers or industrial centers which in turn stimulate job growth and spending. However, the lack of immediate east-west connections across I-15 and traffic concerns may limit access to the Study Area.

Additionally, online sales will continue to impact the traditional brick-and-mortar approach. The US Census Bureau's estimate of retail e-commerce sales as percent of total quarterly retail sales continues to rise, increasing from nearly 5.6 percent in 2013 to 15.1 percent during the first quarter in 2023. Data on retail sales is provided below in **Figure 17**.

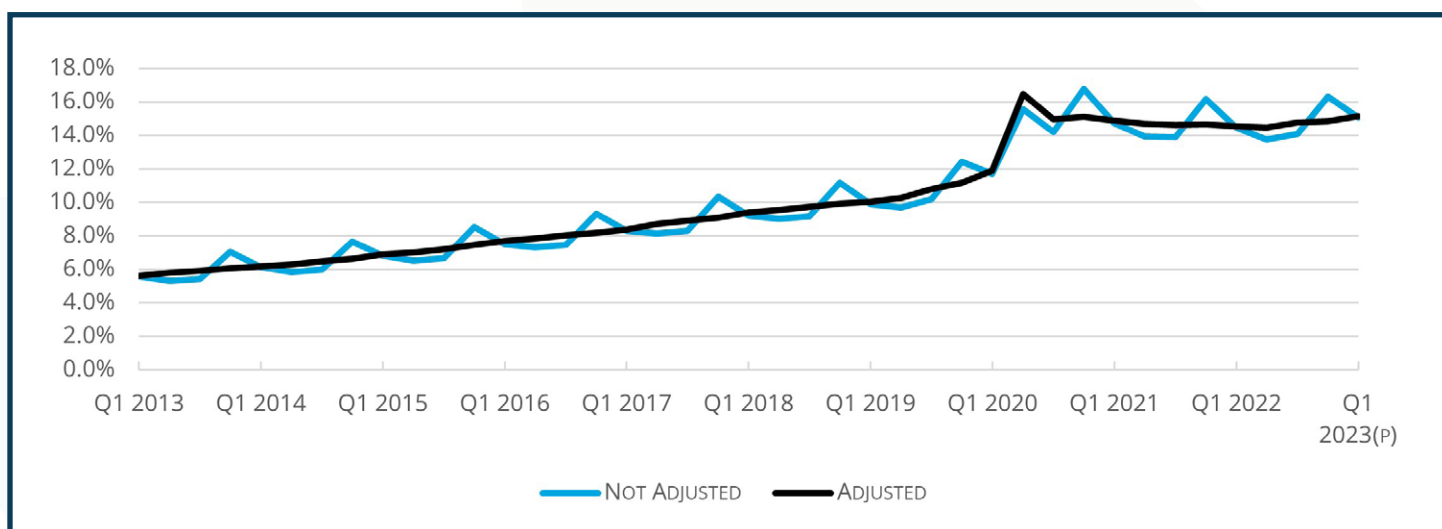


FIGURE 17: Quarterly U.S. Retail E-commerce Sales as a % of Total Quarterly Retail Sales

Monthly retail sales numbers by the Census Bureau show sales from non-store retailers like Amazon, eBay, QVC, and Alibaba increased 6.5 percent from 2022 to 2023. Given this trend, additional measures must be taken to ensure profitability of location-based retail. Methods to promote increased commercial development include the following:

- ◆ Allow for more residential development and population growth.
- ◆ Provide development incentives.
- ◆ Promote niche markets that will capture sales from surrounding communities.
- ◆ Remove barriers to entry.
- ◆ Promote other types of commercial development (industrial, tech, office, etc.).

While the analysis illustrates the potential for substantial office development, increased vacancy rates resulting in cautious developers may highlight challenges for maximizing this land use. However, market trends suggest office vacancy rates may decrease, allowing developers to initiate new projects.

V. PREFERRED VISION PLAN

This Preferred Vision Plan for the South Jordan FrontRunner Station SAP has been created over the past 20 years from previous planning efforts, stakeholder coordination, City Council meetings, Planning Commission meetings, public meetings, and this Station Area Planning exercise.

The Preferred Vision shows how station areas will meet the principles as described in HB462. The transportation facilities and land use laid out in the City's development code and SAP will build a beautiful area that provides easy access to jobs, increases in affordable housing and attainable housing, and enhances access to recreation, entertainment, and sporting events within the station area and across I-15 at the South Town Mall Site.

A. Land Use

KEY TAKEAWAYS

- ◆ Establish a vibrant mixed-use area with increased access to housing and services (such as more restaurants or Pop-Up Food Truck Plaza) to activate the site.
- ◆ Promote residential infill on the remaining buildable parcels of for sale units to support restaurant and food services.
- ◆ Provide additional multimodal connectivity: recreation, employment, and retail opportunities should be considered to enhance and improve the urban environment and community character.

The land use vision for the South Jordan Station Area focuses on creating a connected and vibrant mixed-use community that leverages the proximity to the FrontRunner station and the surrounding amenities. Due to the highly-developed nature of the station area, this plan focuses on adding the missing details and uses that would better allow this area to function internally as a community while still supporting the region as a whole. The recommended future land use map below shows proposed land uses for the Station Area.

After reviewing the existing conditions and following the discussions with the property owners and City representatives, the project team developed this future land use map to guide the development within the station area. These land uses support the City's vision to establish a vibrant mixed-use area with increased access to a range of housing types and services. They are synergistic and enhance the existing Station Area uses and provide additional opportunities for housing, employment,

recreation, and shopping. These uses are also consistent with the state's desires of increased development intensity around major infrastructure developments.

Each parcel was reviewed to better understand their near (0 - 5 year), medium (5 - 20 year), and long-term (20+ years) development potential. Those with near-term development potential should be considered for attached residential uses with a focus on owner-occupied for sale units with some percentage of attainable units. These additional residential units will better support and facilitate the vibrancy of the area outside of business hours. Extending the hours of the area's functional use while adding owner-occupied units will add to the potential viability of additional commercial and retail services within the area. This may also lead to an increase in ridership and use of the FrontRunner Station.

Sustainable transit-oriented communities significant residential units to create a culture of walkability and vibrancy to non-residential uses. The balanced mix of uses is critical in maintaining a community long-term. It is imperative to find a better balance of jobs to residents within the area to add a heightened level of self-sufficiency and sustainability to the station area.

Given the total number of square feet in office, commercial, and industrial uses above (**Table 7**) divided by the jobs per square foot (**Table 12** and utilizing a conservative 1,500 square feet per industrial job, there are approximately 6,230 office, commercial, and manufacturing jobs within the Study Area. If we factor in the estimated population of 933 people (without accounting for individuals of working age), there are 6.7 jobs per resident in the area. A healthy balance for a mixed-use community is 1.3 per household. That number increases in strictly multifamily residential communities. An estimated 440-750 additional housing units are to be constructed on build-out. This healthier balance between households and jobs promotes walkability, increases the demand for commercial service, and supports the City's vision.

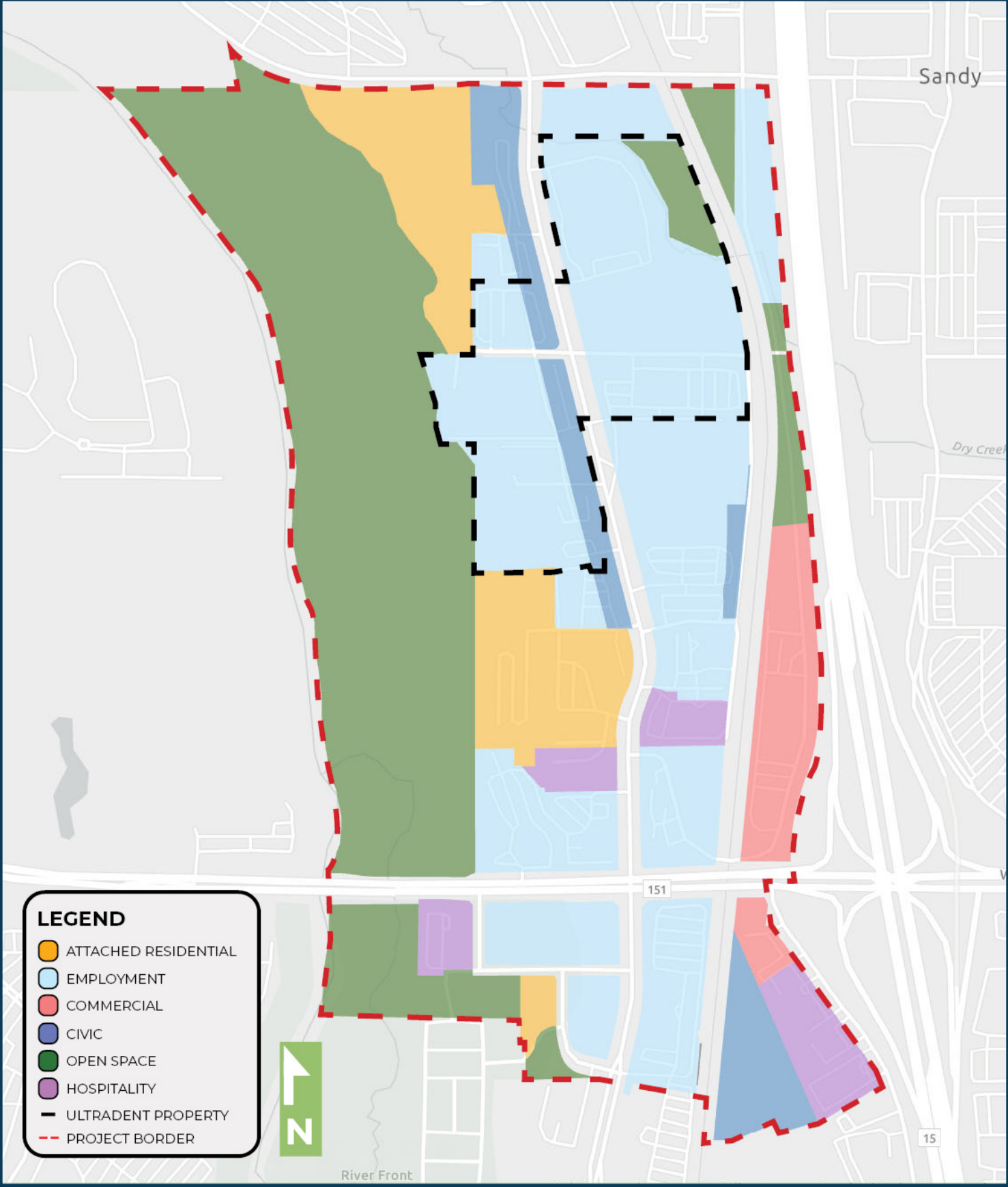


FIGURE 18: Recommended Future Land Use

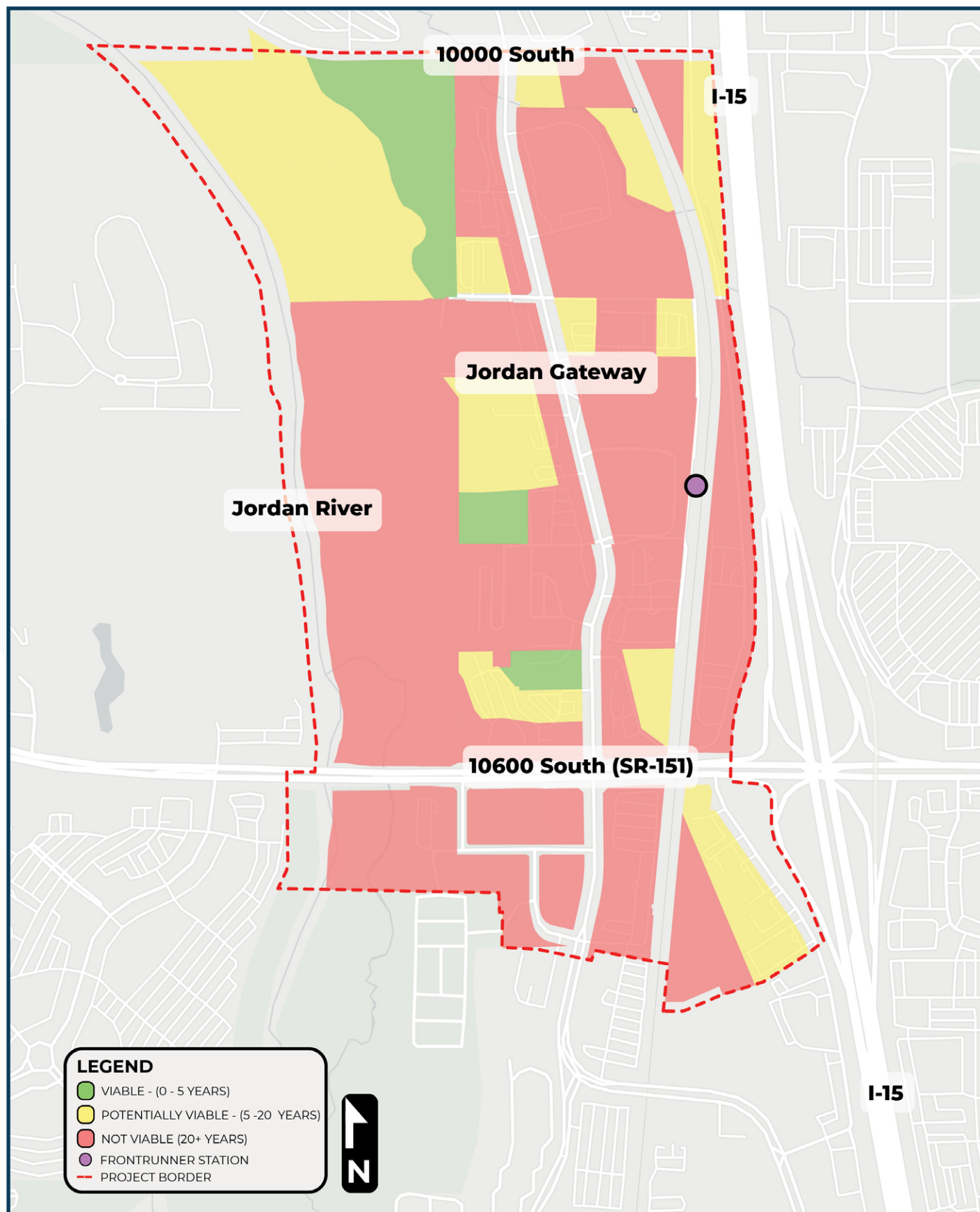


FIGURE 19: Study Area Development Potential

To further support the City's vision, the following elements should be considered to enhance and improve the urban environment, community character, multi-modal connectivity, and recreation, employment, and retail opportunities:

Enhance Residential Infill Development

- ◆ **Objective:** Increase the supply of owner-occupied housing with a percentage focused on attainable residential units to meet the growing demand and promote sustainable urban development.
- ◆ **Goal:** Promote residential infill that is accessible to a diverse population, and incorporates street facing pedestrian-scaled design elements to improve long-term character and vibrancy of the community.
- ◆ **Strategy:** Encourage residential development on vacant parcels, particularly on the west side of the Study Area, and integrate design standards for public-facing facades only. Look for opportunities to reduce parking ratios, requirements for completely enclosed parking, and encourage residential square foot maximums.

Promote Infill Development and Expanded Employment Opportunities

- ◆ **Objective:** Encourage the development of underutilized or vacant land to create a mix of office, residential, and commercial spaces that support local employment, economic growth, and community character.
- ◆ **Goal:** Foster a dynamic and diverse land use pattern that integrates residential, commercial, and recreational spaces, enhancing the overall livability of the area.
- ◆ **Strategy:** Identify key infill sites for development, including areas along the power corridor, surface parking lots, and on or near the Ultradent Campus, to create new employment opportunities and support existing industries.

Develop Recreational Opportunities

- ◆ **Objective:** Create a multi-purpose sports complex to serve as a recreational hub for the community, addressing the area's need for amenities that support both residents and the workforce population of approximately 6,230 employees.
- ◆ **Goal:** Enhance recreational opportunities and promote active lifestyles within the Study Area while creating community gathering spaces that activate the area outside traditional business hours, supporting the 24-hour vibrancy identified as lacking in the current assessment.
- ◆ **Strategy:** Identify suitable locations near existing employment centers for recreational facilities that

could include indoor and outdoor amenities like pickleball courts, fitness trails, and community gathering spaces. These facilities could serve as anchors for evening and weekend activity, complementing the predominantly daytime employment uses in the area.

Activate the Site

- ◆ **Objective:** Create, promote and establish activity / vibrancy near and around the station to activate the site with areas of interest and public gathering opportunities by creating reasons to linger and participate in the site rather than only passing through it.
- ◆ **Goal:** Create a vibrant and dynamic space that attracts residents and visitors during business hours and evenings, supporting local businesses and extending activity beyond the current daytime employment dominated periods.
- ◆ **Strategy:** Engage with local businesses, establish public space for events and activities like a Pop-Up Food Truck Plaza that designate areas for food trucks and provide necessary utility infrastructure to support their operation. Program the site with activities such as music, sporting activities, cultural/art events, special interest events like bicycle maintenance clinics, bird-watching lectures etc.
- ◆ **Consider locations** near the FrontRunner station platform, employment and hospitality land uses to maximize foot traffic. Incorporate seating, shade structures, and lighting to create a welcoming environment in currently underutilized surface parking areas. The market analysis and stakeholder feedback identified the limited food options, protection from the weather conditions and restrooms at the site.

Implement Community-Centric Street Improvements

- ◆ **Objective:** Enhance the streetscape and promote active transportation through street improvements that address the connectivity and safety concerns identified in the transportation assessment.
- ◆ **Goal:** Create a safe and attractive environment for pedestrians and cyclists that improves the quarter -mile spacing between existing crossings and enhances the comfort of east-west connectivity.
- ◆ **Strategy:** Implement patterned road segments, bike lanes, and intersection enhancements along major corridors. Key improvements should include the proposed 10-foot, two-way cycle track with a four-foot buffer on the west side of Jordan Gateway, enhanced mid-block crossings with pedestrian hybrid beacons, and under-mast arm lighting at signalized intersections to increase nighttime visibility and safety. These enhancements are explained in further detail in the Transportation section below.

B. Future Environmental Conditions

KEY TAKEAWAYS

- ◆ Transit-oriented development around FrontRunner the stations reduces trips, travel times and carries multiple positive environmental impacts such as air quality.
- ◆ Avoid, minimize, and mitigate future adverse impacts to the Jordan River Parkway corridor.

South Jordan City is proud of its sustainable environmental efforts and are actively promoting these by continuing the sensitive lands preservation policy that restricts development in the Jordan River Corridor while allowing the few remaining undeveloped private property parcels on the north end of the site (between 100 - 102 South and between Jordan Gateway Road and the Jordan River) to develop their property that is outside of the steep riparian corridor.

The National Wetlands Inventory (NWI) was established by the US Fish and Wildlife Service (FWS) to conduct a nationwide inventory of U.S. wetlands to provide biologists and others with information on the distribution and type of wetlands to aid in conservation efforts. The Jordan River in this area is designated forested shrub wetland riparian corridor.

Preserving the remaining land in the Jordan River Corridor from development through the development process will result in a win-win outcome for the Jordan River Corridor and the remaining private property owners. Acquiring or restricting future development on additional private lands in the river corridor for preservation in accordance with the City's development policies will convert private lands into permanent habitat conservation consistent with the Jordan River Commission.

Continue to Preserve Existing Jordan River Corridor

- ◆ **Objective:** Preserve natural habitats and provide passive recreational amenities through the promotion of the nature preserve that complements the Jordan River's ecological significance as identified by conservation stakeholders.
- ◆ **Goal:** Enhance community awareness and buy-in of the recreational opportunities within the Study Area while protecting the significant wetlands identified in the National Wetlands Inventory and supporting the wildlife habitat documented in the area.
- ◆ **Strategy:** Develop additional educational signage that promotes conservation and outdoor activities as appropriate including do not enter signage. Encourage future bird-friendly design elements such as minimization of light pollution, native vegetation plantings, and invasive species removal efforts. Coordinate with the Jordan River Commission to align with broader regional habitat preservation goals.



Jordan River Trail

C. Economics

The market analysis suggests that while the City has supportable square footage, existing City plans envision that future retail and office development will occur at Daybreak. As a result, development and redevelopment opportunities in the FrontRunner Study Area will primarily focus on infill development consisting of office and residential land use. The following policies serve as a strategic response to the market analysis:

Foster Economic Growth and Job Creation

- ◆ **Objective:** Stimulate local economic growth by encouraging the development of office spaces that cater to existing industries, such as health, technology, dental, and public administration, while promoting flexible work environments to encourage utilization of public transportation.
- ◆ **Goal:** Promote office development that complements existing businesses in key sectors, promoting a sustainable economic base in the Study Area.
- ◆ **Strategy:** Encourage expansion of office development at key sites including Ultradent Campus, infill locations within the power corridor, and vacant parcels.

Strengthen Hospitality Offerings

- ◆ **Objective:** Promote growth in the hospitality sector by allowing development of new hotels and leisure facilities to attract regional visitation.
- ◆ **Goal:** Increase the development of high-quality hospitality infrastructure that focuses on providing new offerings and revitalizing existing space.
- ◆ **Strategy:** Develop hotels at key parcels, with a focus on revitalization of aging sites. Areas of focus include the south-east quadrant of Study Area and vacant parcels.

Address Housing Demand

- ◆ **Objective:** Meet the growing demand for housing by increasing the supply of attainable and market-rate residential units while promoting sustainable urban development.
- ◆ **Goal:** Increase the housing stock to ensure housing accessibility for a diverse population.
- ◆ **Strategy:** Encourage the development of underutilized or vacant land, particularly on the west-side of the Study Area, to build higher-density housing and incorporate sustainable building practices and green design elements in residential projects to reduce energy consumption, improve long-term affordability, and promote environmental responsibility.



D. Transportation

A primary objective of the SAP is to facilitate the increase of transportation choices and connections. This section summarizes the current transportation plans in the Station Area and identifies opportunities for additional improvements to enhance access for transit users.

KEY TAKEAWAYS

- ◆ Improve wayfinding to transit and AT facilities/services
- ◆ Study a Circulator Route Connecting Key Destinations
- ◆ Transition Surface Parking to Structured Parking & Screen
- ◆ Construct a Jordan River Pedestrian Bridge
- ◆ Construct parallel trail north from the Station to Shields Lane to more directly connect business north of 10000 South
- ◆ Construct pedestrian bridge over I-15 to the South Town site near 10200 south
- ◆ Re-evaluate transit needs with growth in Sandy

Public Transit

Several transit plans have been completed previously in the Study Area and are summarized below and in **Figures 20 and 21**. This station area plan reaffirms and supports the transit recommendations made in the WFRC RTP, UTA Five-Year plan, and UTA Moves 2050: LRTP 2023-2050 Plan.

UTA Moves RTP 2050

- ◆ South Jordan / Sandy Connector Core Route (15 minute frequency) - Sandy Expo TRAX Station to South Jordan FrontRunner Station, planned in Phase 2 (2032-2042).

UTA 5-Year Service Plan (2025-2029)

- ◆ **Route 219 (60-minute frequency)** - Route 219 will be a new north-south route on Redwood Rd between Sandy Civic Station and Bluffdale with connections to Sandy City Hall, South Jordan Station, Riverpark Business Park, South Jordan Towne Center, Riverton City Center, and Bluffdale City Hall. The route will initially operate Weekdays at 60-minute frequency planned in 2025.
- ◆ **Route 218 (30-minute frequency)** - Headways on Route 218 will be increased to 30 min from 60 min in 2025.

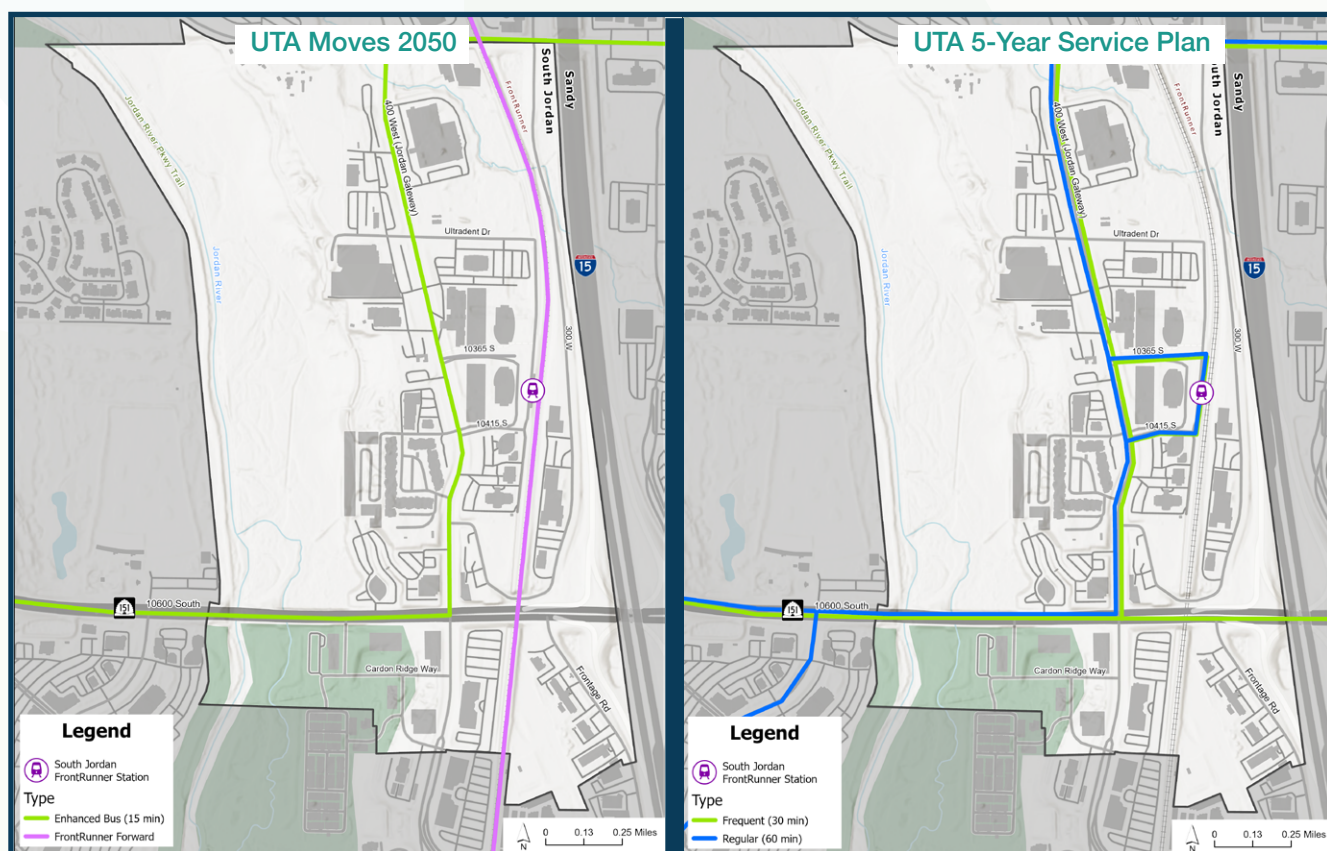


FIGURE 20: UTA Transit Plans

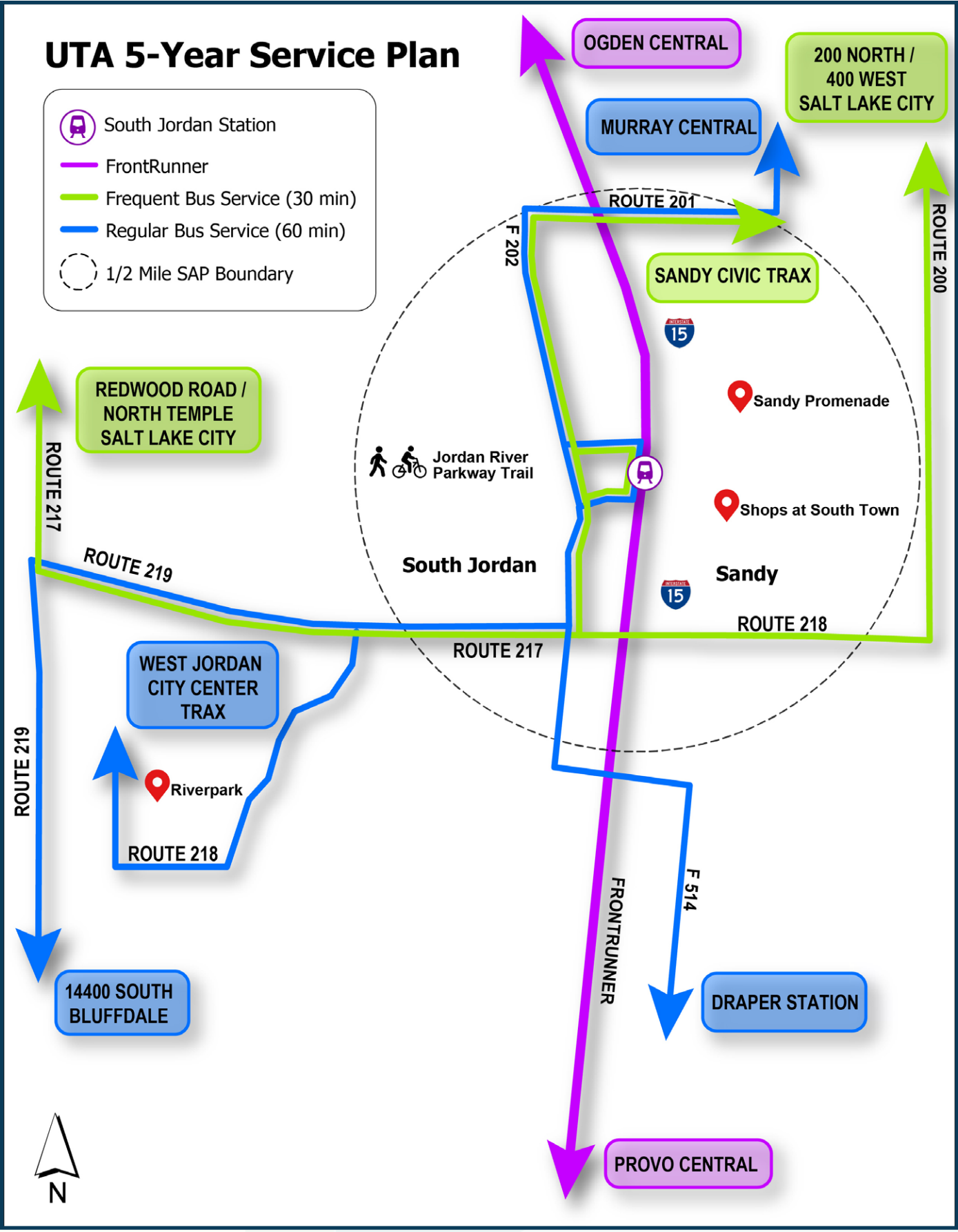
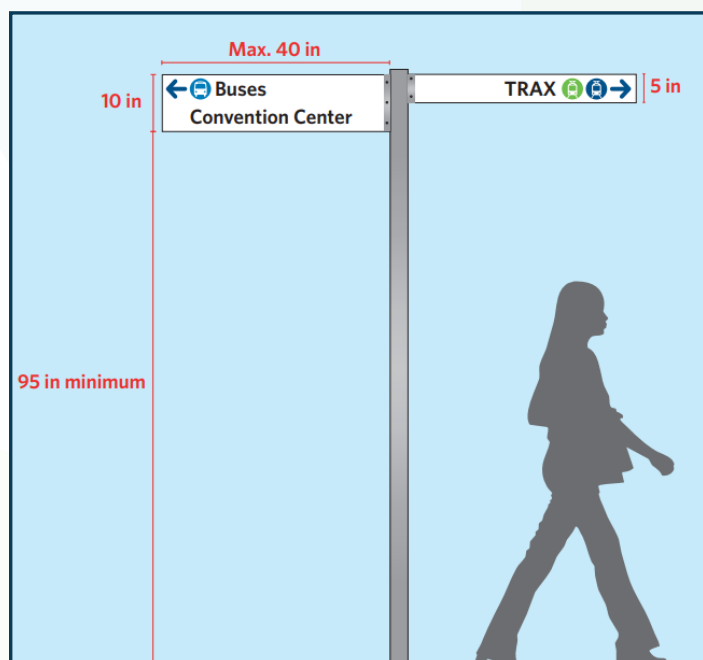


FIGURE 21: UTA 5-Year Service Plan

Wayfinding Signage

It is essential that transit users have a convenient and comfortable transit experience. Special care should be taken to accommodate disadvantaged transit users (i.e. people with disabilities) and families at the stations. Adequate lighting should be provided at the stations and wayfinding should allow for simple and easy navigation. A wayfinding system will direct people from point to point and confirm their progress along a route. Wayfinding also serves as an additional marketing source: signage builds brand recognition and equity. They elevate a brand story and create a sense of place. It is recommended the City work with UTA to make wayfinding consistent with the standards in the UTA Wayfinding & Signage Sign Schedule and Drawing Package. Signage should reflect the area's character and guide residents and visitors to key destinations, including the FrontRunner. As an example, UltraDent has branded their buildings and grounds with Utah specific theming (Arches, Bryce Canyon, and Zions National Parks). Signage within the area could utilize this brand or the existing SOJO Station branding.



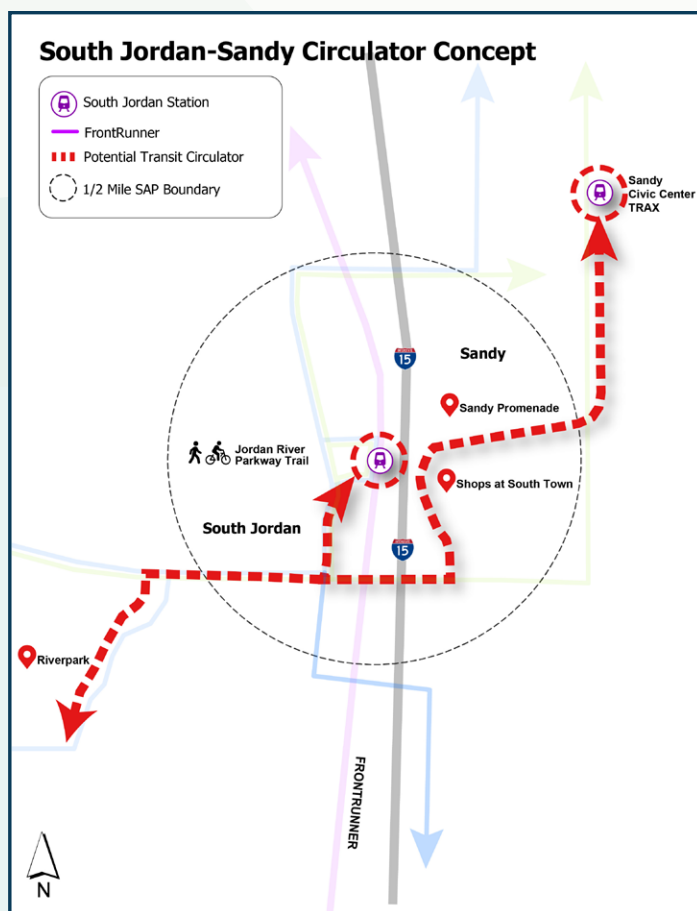
Source: UTA Wayfinding & Signage Sign Schedule and Drawing Package

East Side Transit Connectivity

Considering the challenge of getting to the east side of the rail line and I-15, there is a critical need for transit connections that link with the Sandy Civic Center TRAX station in Sandy. This concept has been studied previously and demand for this connection is anticipated to grow as Sandy City is expected to add over 2,000 additional residential units and other commercial/office uses in their

boundary on the east side of I-15. In an effort to connect the South Jordan FrontRunner Station to the east side of I-15 and Sandy, this circulator concept route (shown below) could be incorporated into existing and future UTA lines to provide frequent service that connects key destinations. Some of the major destinations nearby this proposed circulator route include:

- ◆ SOJO Station Office Complex
- ◆ Riverpark Corporate Center
- ◆ The Shops at South Towne
- ◆ Sandy Civic Center TRAX
- ◆ Jordan High School
- ◆ Towne Ridge Office Park
- ◆ Mountain America Expo Center
- ◆ Jordan Commons
- ◆ Real Salt Lake Stadium
- ◆ Mountain America Office Building
- ◆ Hale Center Theatre
- ◆ Sandy City Hall
- ◆ Ultradent Campus



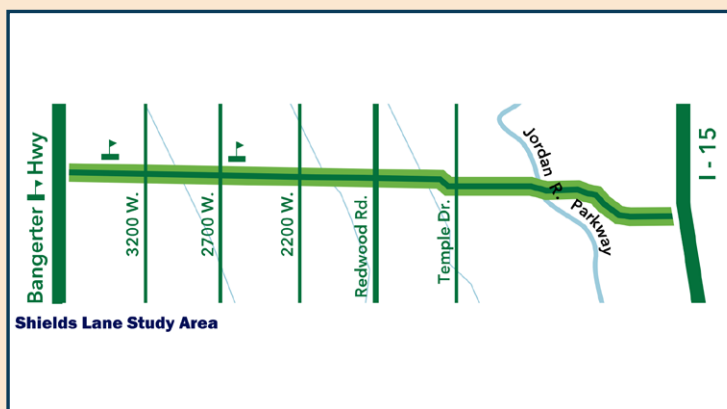
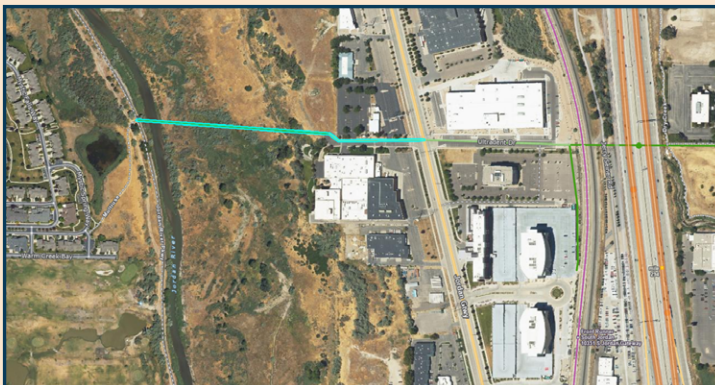
Active Transportation

There are several active transportation improvement projects in the planning phase within the Study Area including a Jordan River Bridge, the 9800 South (Shields Lane) active transportation improvements, and a pedestrian bridge over I-15.

Active Transportation Projects Currently Underway

Jordan River Bridge

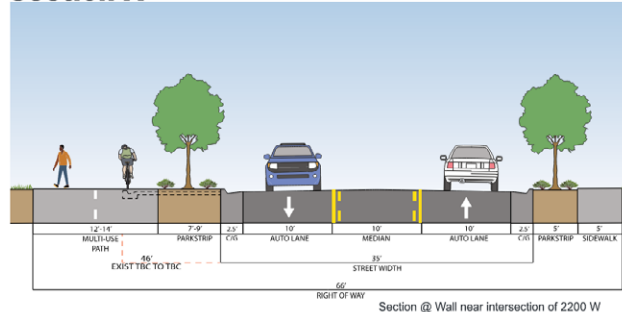
The Jordan River Trail (JRT) Connector to the FrontRunner Station is a proposed 0.2-mile trail and bridge connection, providing pedestrians with a safer and more direct route from the JRT to the South Jordan FrontRunner Station. The project is identified in the 2023 Regional Transportation Plan and was awarded funding from the Congestion Mitigation and Air Quality Program (CMAQ) in October 2024. This project has an estimated cost of \$1.55 million with \$1.45 million in federal aid funds and the remaining \$135,000 coming from local government and other sources.



9800 South (Shields Lane) Active Transportation Improvement

Shields Lane serves as one of South Jordan City's primary east-west corridors and currently functions as a key active transportation route, providing pedestrian and bicycle access to two elementary schools and one middle school. While future trail development in the City focuses on north-south connections, there are no existing or planned east-west trail facilities. The Shields Lane Solutions Development Study aims to identify future improvements that preserve and enhance this corridor's vital role in the City's active transportation network.

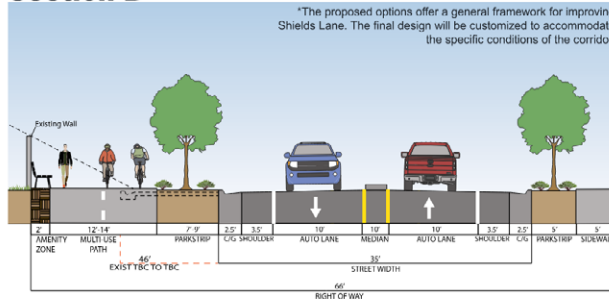
Section A



Project Description

This proposal prioritizes pedestrian and bike safety, ease of maintenance, park strip impacts considerations, cost, and smooth traffic flow while preserving a center median for emergency access.

Section B



Versatile path for all users, improved safety, and enhanced community connection.

Pedestrian Bridge over I-15

As a part of a long-term vision to enhance multimodal connectivity and improve access to regional transit, a potential pedestrian bridge has been identified in Sandy providing a direct connection to the South Jordan FrontRunner Station. While still conceptual and dependent on additional coordination and funding resources, the structure could be similar in scale to the pedestrian bridge in Orem that links the Orem FrontRunner Station to Utah Valley University and surrounding high-density housing. As the Cairns downtown area in Sandy continues to grow with additional housing developments and other land uses with high visitation, so too may the demand and desire for a more direct pedestrian connection to the South Jordan FrontRunner Station. This potential link aligns with both communities' aspirations in South Jordan and Sandy for improved first-last mile options, safer active transportation routes, and a more integrated transit network.



UVU Pedestrian Bridge

Several additional improvements are recommended to the active transportation network as a part of this station area plan are explained in more detail below, and are shown in **Figure 22**.

1

Jordan Gateway Cycle Track

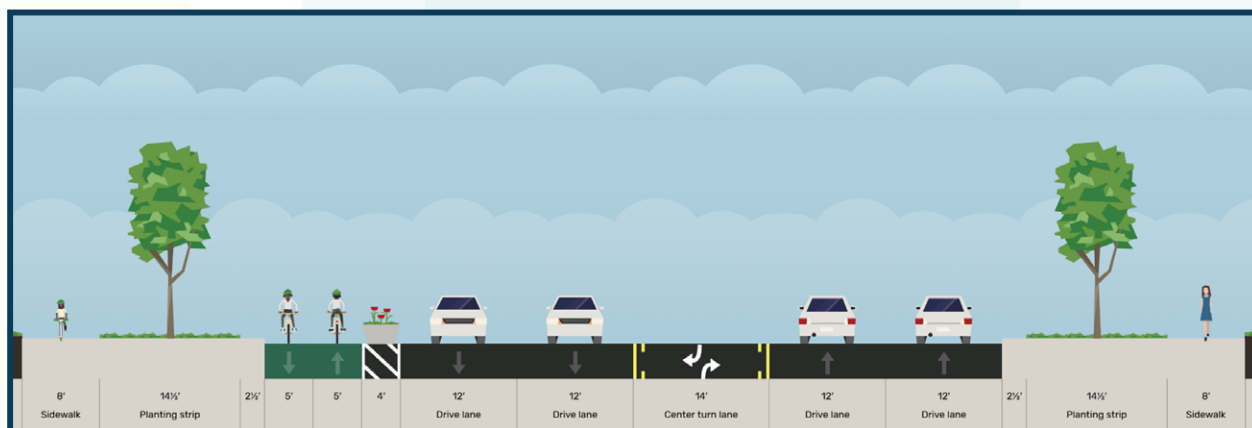
As a part of this station area plan, a north-south cycle track is recommended for Jordan Gateway. One conceptual design features a 10-foot, two-way travel lane for cyclists with a four-foot buffer, fitting within the existing asphalt width while preserving current vehicle travel lane dimensions. It is proposed that the cycle track be constructed on the west side of Jordan Gateway as there are fewer side street accesses and less dense development compared to the east side. The evaluation on final design should leave room for temporary implementation designs.

Beyond providing a dedicated space for cyclists, a well-designed cycle track offers multiple benefits. It serves as a

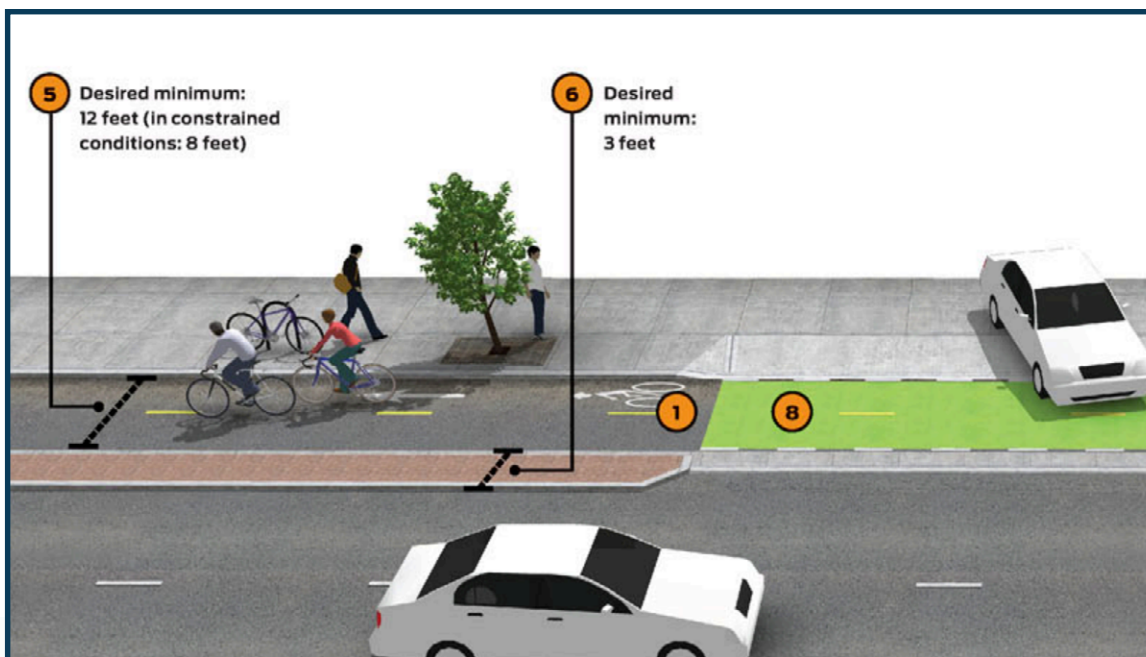
traffic-calming measure, encouraging safer vehicle speeds. By reallocating space, it shortens pedestrian crossing distances at mid-block crossings, improving walkability. Additionally, the cycle track contributes to the aesthetic enhancement of the corridor, creating a more inviting and vibrant streetscape that supports active transportation.

Furthermore, a cycle track on Jordan Gateway would also provide a connection to the 9800 South (Shield's Lane) proposed active transportation facility from the FrontRunner station promoting seamless multimodal transportation to/from the station.

One alternative to a cycle track on Jordan Gateway would be a lane width reduction strategy which could be implemented the next time the roadway is re-stripped and would include 4 x 11-foot travel lanes and a 12-foot, two-way left turn median. The extra asphalt width could be used for additional bicycle lane buffer or lane width.



Example Jordan Gateway Cycle Track Cross Section

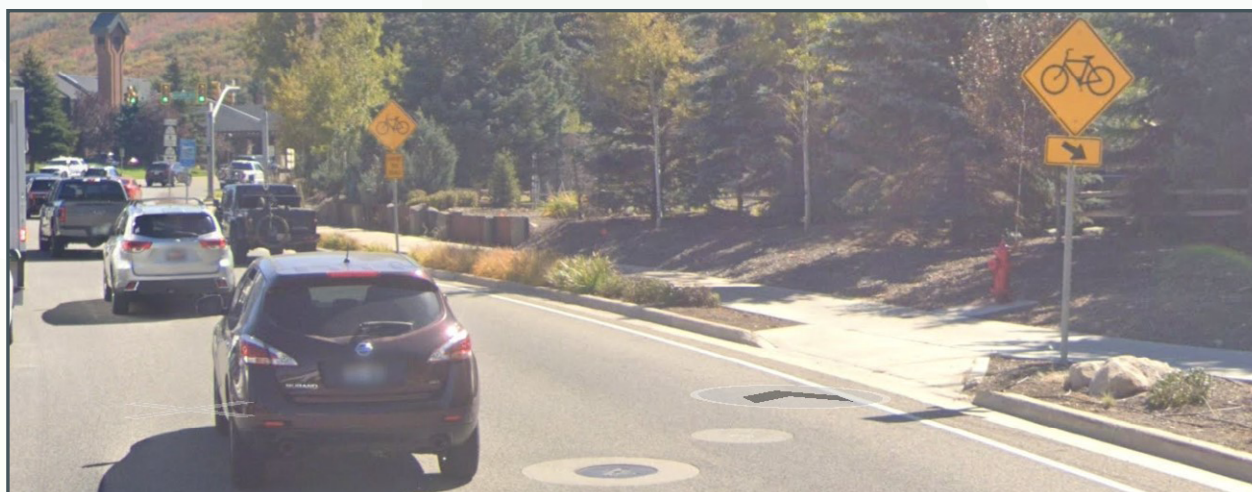


Source: NACTO

2 Existing Bike Lane Enhancement

The existing bike lanes on 10600 South and on Jordan Gateway south of 10600 South end abruptly before reaching the intersection at 10600 South and Jordan Gateway. There is no clear signage or transition for bicyclists, creating confusion between bicyclists and drivers and posing a

significant safety risk. The City should implement clear and consistent transitions that guide bicyclists to either safely exit the roadway or merge into vehicle traffic in a predictable manner.



On-Street Bike Transition - Park Avenue, Park City

3 Enhance Pedestrian Experience

There are multiple opportunities to enhance the existing pedestrian hybrid beacon (PHB) at the intersection of Jordan Gateway and Ultradent Drive. Planned infrastructure investments—including a pedestrian bridge over I-15 and a multi-use bridge over the Jordan River—will significantly increase east-west active transportation demand through this intersection. As a result, Jordan Gateway and Ultradent Drive are expected to become a critical active transportation crossing, warranting improvements to ensure pedestrian safety and comfort.

Two primary improvement options are recommended, each with associated trade-offs:

1. Full Signalization:

Upgrading the intersection to a fully signalized configuration would offer the highest level of control for pedestrian crossings and provide a predictable traffic environment. However, it may introduce an increased risk of vehicular collisions due to added intersection conflicts, potentially reducing overall intersection safety.

2. Enhanced Pedestrian Hybrid Beacon:

Enhancing the existing PHB could balance safety and operational performance. Potential improvements include expanding the system to incorporate two mast arms with an additional center-mounted beacon, increasing the total number of signal heads from two to three per direction. Installing beacons at ground level makes it easier for turning movements to see the beacons. Installation of a raised median could further support pedestrian safety by serving as a refuge and providing a visual cue to motorists, promoting traffic calming. A conceptual example of an enhanced PHB treatment is provided below.

Additional improvements at the intersection include constructing directional pedestrian ramps on the side streets with intersection bulb outs.



Pedestrian Hybrid Beacon in Tucson, Arizona

It is also recommended that the City explore under-mast arm lighting at signalized crosswalks within the station area where traditional lighting installations are challenging to enhance nighttime pedestrian crossing visibility.

4 Encourage UDOT to Enhance Transportation Facilities at the 10600 South Interchange

It is recommended that the City encourage UDOT to increase the user comfort of east-west active transportation facilities at the 10600 South Interchange such as improved lighting, enhanced striping, and other strategies to enhance the experience for vulnerable users.

5 Jerry Seiner Road Sidewalk

When the adjacent property develops, city policy will require developer to complete sidewalk at the following location, thus completing the sidewalk network in the Study Area.



Proposed Jerry Seiner Sidewalk Improvement

6 Bicycle and Storage Maintenance Amenities Promotion

It is recommended that the City work with UTA to increase the visibility and awareness of bicycle storage and bicycle maintenance amenities through signage and platform promotional materials.

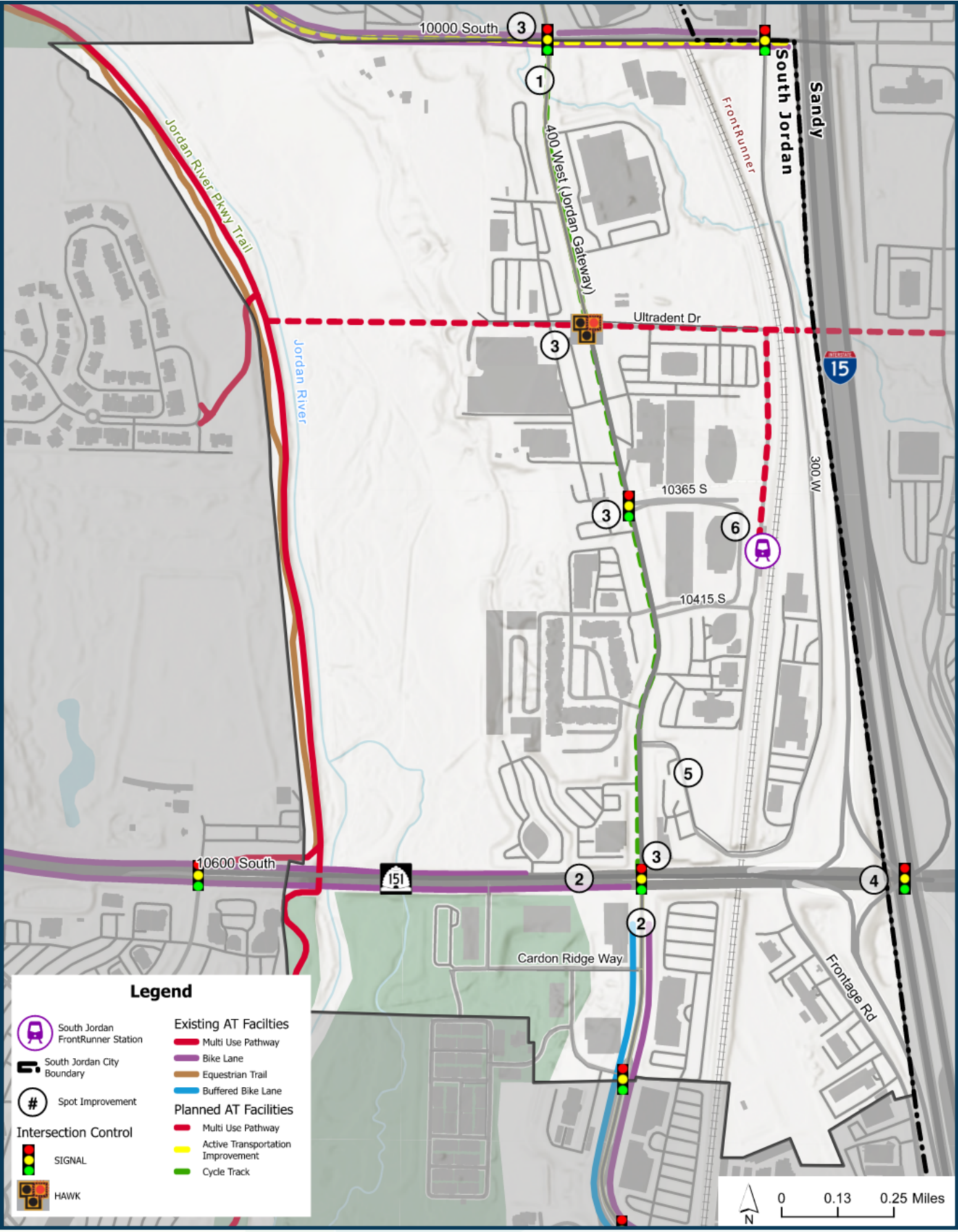


FIGURE 22: Active Transportation Preferred Vision Plan

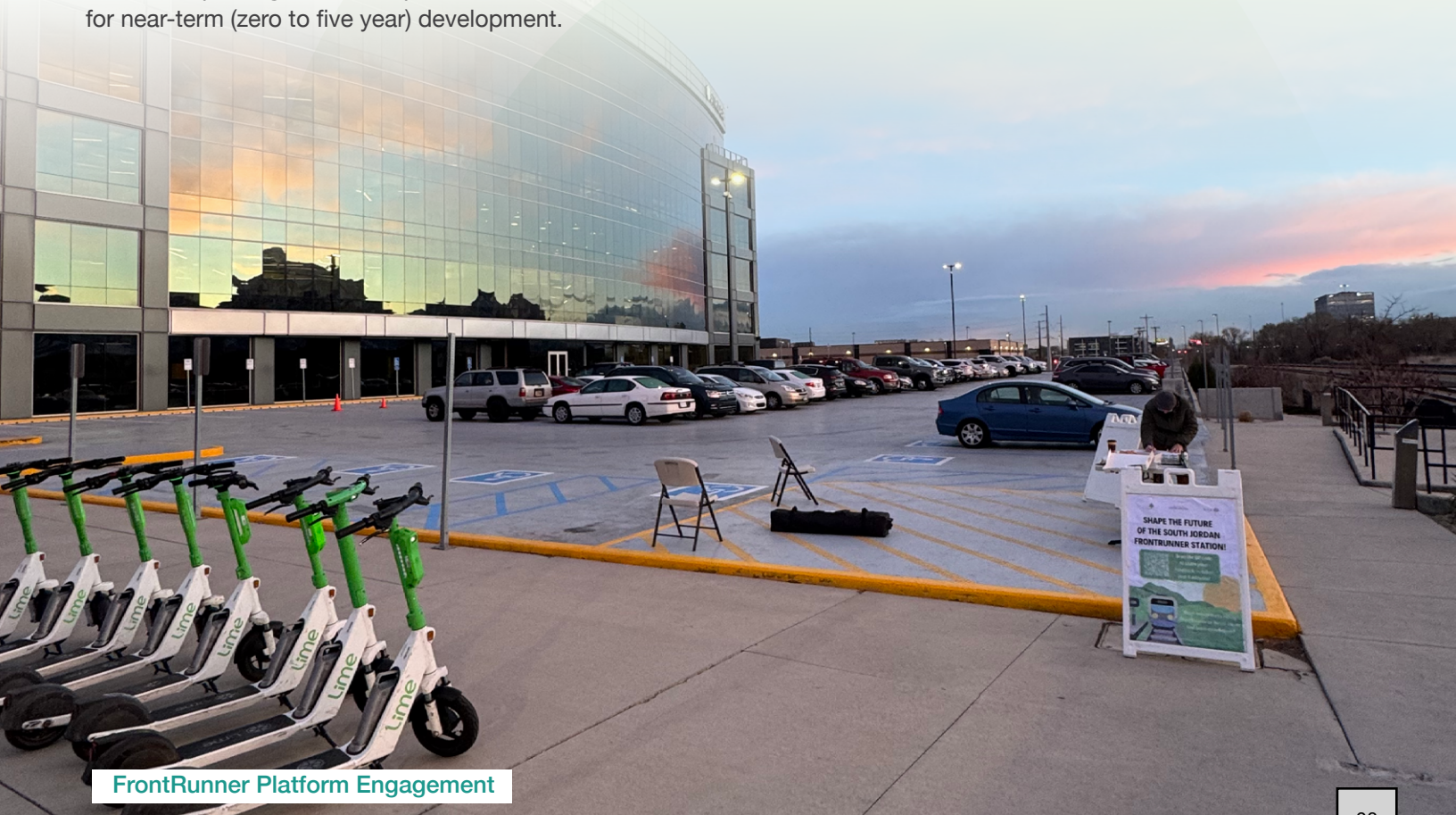
Vehicles

No roadway projects are currently planned in the Study Area. An intersection improvement project is scheduled for phase #2 (2033-2042) at Jordan Gateway and 10000 South. Public outreach has indicated that parking at the FrontRunner station is limited and frequently operates at or near capacity, with 20% of responses in the survey mentioning additional parking as a needed improvement. Therefore it is recommended that the City, UTA, and adjacent businesses collaborate on an expanded shared parking agreement.

To minimize the visual impact of structured parking along I-15 and maximize land use in this high-value transit-oriented corridor, it is recommended to transition surface parking to structured parking while implementing effective screening and design strategies. The objective is to create an aesthetically pleasing environment that seamlessly integrates parking facilities into the urban landscape, supporting both economic development and improved transit accessibility. This can be achieved through the use of landscaping, architectural elements, and facade treatments that reflect the station area's unique identity. Additionally, opportunities for shared parking between businesses with complementary peak hours should be explored, and structured parking should be prioritized in areas identified for near-term (zero to five year) development.

“ UTA Frontrunner riders take up our work parking stalls. There needs to be more UTA parking at this particular station. ”

— Comment from public on site April 2nd 2025



FrontRunner Platform Engagement

VI. 5-YEAR IMPLEMENTATION PLAN

Successful implementation of the South Jordan FrontRunner Station Area Plan will continue to occur in phases through multi-agency cooperation. **Table 15** and **Figure 23** below summarizes the recommended implementation plan.

Implementation actions can include projects, programs, policies, and plans for transportation, land use, regulatory framework, urban design, and environmental enhancements.

Actions the City can take, in association with other public sector partners to support private sector stakeholders to induce land owners to act in alignment with the plan to collectively achieve the vision for development in the station area.

The recommendations have a description, recommended phases, rough order of magnitude costs, and range of impact and indicates if the implementation item is related to the State of Utah HB462 and Station Area Planning principles:

- ◆ Affordability of housing, including moderate income housing
- ◆ Promote sustainable environmental conditions
- ◆ Enhance access to opportunities
- ◆ Increase transportation choices and connections



TABLE 15: SOUTH JORDAN FRONTRUNNER STATION IMPLEMENTATION PLAN												
Project ID	Category	Project	Cost, Potential Funding (\$=Less than \$1M, \$\$=\$1M-\$10M, \$\$\$=\$10M+)	Housing	Environment	Access to Opportunity	Transportation	Priority	Project Champions	Near-term (1-2 years)	Mid-term (3-5 years)	Long-term (5+years)
1	Policy	General Plan Alignment - Update South Jordan General Plan to include future land use policies identified in the Station Area Plan	\$, South Jordan City	Y	Y	Y		High	City	City staff implement		
2	Policy, Program	East-West Transit Connectivity - Continue to encourage and partner with UTA and WFRC to expand transit options such as additional service routes and circulators to nearby employment centers and housing.	\$\$, UTA, WFRC, City and developer agreements.			Y	Y	Medium	South Jordan City, UTA and Sandy City	Continue to work with UTA to refine future Transit service in the annual UTA five Year plan development cycle	Implementation	
3	Project	Active Transportation Network Cohesion - Enhance active transportation wayfinding and markings with themed signage around the FrontRunner station, along Jordan River Parkway and Shields lane improvements.	\$, Developer agreements, UTA, City, WFRC, Salt Lake County			Y	Y	Low	City, UTA	City to initiate a collaboration with UTA to make wayfinding consistent with the standards in the UTA Wayfinding & Signage	City Develops Sign Schedule and Drawing Package.	Installed as development occurs and opportunity arise
4	Project	Jordan River Trail Network Expansion - Expand the trail network connecting the Jordan River Trail with a bridge across the Jordan River connecting to key destinations within the station area, creating a comprehensive active transportation system linking residential, employment, and recreational uses.	\$\$, Funding in place, Work with Jordan River Commission and Private Property owners to gain an easement up the slope to Ultradent Drive			Y	Y	High	State of Utah, City, Salt Lake County	During private development negotiations work with private property owner to secure easement	Jointly work with Jordan River Commission to secure permits and develop construction Plans/Package	
5	Project	Jordan Gateway Cycle Track - Evaluate feasibility of including a Cycle Track on Jordan Gateway to the Active Transportation Plan in the next modification. (Active Transportation Improvement #1)	\$, City			Y	Y	Low	City	City initiate a planning study to identify issues and opportunities		Implementation
6	Project	Bicycle Visibility - Incorporate high visibility bike lane paint at intersections along Jordan Parkway, Shields Lane, and 10600 South near the station to enhance existing bike lanes (Active Transportation Improvement #2)	\$, City				Y	Medium	City, UDOT	City initiate discussion with UDOT Region 2 to develop a striping and paint plan	Implementation	
7	Project	Enhance Pedestrian Experience - Enhance pedestrian experience through evaluation of improvements of crossings and lighting opportunities such as under-arm mast lighting and improvements to existing intersections and crossing signals. (Active Transportation Improvement #3)	\$, City, UDOT				Y	Low	City, UDOT	City coordinate with UDOT Region 2 and Implement		
8	Project	10600 South Active Transportation Improvements - Encourage UDOT to evaluate ways to increase visibility and safety of pedestrian and bicycle crossings along 10600 South and at the I-15 interchange through improved lighting and enhanced striping. (Active Transportation Improvement #4)	\$, UDOT, City, WFRC, Salt Lake County			Y	Y	Medium	City, UDOT	City coordinate with UDOT Region 2 and implement		
9	Project	Jordan Gateway Pedestrian Accessibility - Complete sidewalk connection to Jordan Gateway by completing missing links, and construct crosswalk on the south leg of 10365 South and Jordan Gateway. (Active Transportation Improvement #5)	\$, City			Y	Y	Low	City	City to require completion of sidewalk when adjacent property develops	Ongoing	Ongoing
10	Project/Policy	FrontRunner Station Platform Improvements - Increase visibility of existing bike storage and maintenance amenities through signage and promotional materials. Consider adding cold weather shelters, transit/ orientation maps, and other passenger amenities (Active Transportation Improvement #6)	\$, UTA and City				Y	High	UTA	City Coordinate with UTA and UTA implements		
11	Project	Pedestrian Bridge Over I-15 - Work with Sandy City as they take the lead in constructing a pedestrian bridge connecting the FrontRunner station area to Sandy City's Cairns District (including South Towne Mall redevelopment) to enhance regional connectivity and provide direct access to entertainment, retail, and future housing.	\$\$\$, Sandy City, Federal Government, State of Utah, WFRC, Salt Lake County and South Jordan City			Y	Y	Low	Sandy City	South Jordan City reach out to Sandy City to coordinate	Sandy City and South Jordan City Coordination on Funding, Design and Permitting	Implementation

TABLE 15: SOUTH JORDAN FRONTRUNNER STATION IMPLEMENTATION PLAN (continued)												
Project ID	Category	Project	Cost, Potential Funding (\$=Less than \$1M, \$\$=\$1M-\$10M, \$\$\$=\$10M+)	Housing	Environment	Access to Opportunity	Transportation	Priority	Project Champions	Near-term (1-2 years)	Mid-term (3-5 years)	Long-term (5+years)
12	Policy	Jordan River Corridor Preservation - Continue Jordan River preservation area north to the end of the study area while balancing with increasing housing development needs. Continue natural restoration projects and programs lead by the Utah Department of Natural Resources, Jordan River Commission, and others to revegetate eroded river banks and mitigate invasive weed species	\$, City developer agreements, State of Utah, Utah Reclamation Conservation Commission, Utah Sovereign Lands, Jordan River Commission, Great Salt Lake Audubon Society	Y	Y			High	City, Salt Lake County, Jordan River Commission, Developers	City looks for opportunities to partner with Jordan River Commission and Utah DNR	Implementation	
13	Project	Shared Parking Implementation and Parking Improvements - Continue to collaborate on shared parking partners such as UTA, Ultradent, and other users where feasible with additional development. Consider a future parking study for the station to consider a transition from surface parking to structured parking.	\$\$, South Jordan City, UTA, WFRC, and Private stakeholders				Y	High	City , UTA, Developers	City and UTA reach out and work with partners / stakeholders	City leads facilitation	Implementation
14	Policy	Establish Activity/Vibrancy - Create, promote and establish activity / vibrancy near and around the station to activate the site with areas of interest and public gathering opportunities by creating reasons to linger and participate in the site rather than only passing through it.	\$, Developers, City			Y		High	City, Developers	City works with property owners to gauge interest discuss options	City Economic Development Department facilitates discussions	Implementation
15	Project/Policy	Mixed-Use Infill Development - Evaluate land use code to remove barriers for higher-density and mixed-use infill and redevelopment on underutilized parcels, focusing on existing industries, such as health, technology, dental, and public administration a more balanced mix of uses with both employment and housing opportunities near transit.	\$, Developers, City	Y		Y		Medium	City, Developers	City works to promote sites in conjunction with landowners/developers	Ongoing	Ongoing
16	Project/Policy	Office Infill Development - Encourage expansion of office and hospitality development at key sites including Ultradent Campus, infill locations within the power corridor, and at vacant parcels.	\$, Developers, City, Ultradent			Y		Medium	City, Developers	City works to promote sites in conjunction with landowners/developers	Ongoing	Ongoing
17	Policy	Owner-Occupied Residential Infill Development - Establish incentives for owner-occupied residential development on available parcels with a percentage as attainable units to better balance the station area's jobs-to-housing ratio and support greater buy-in and transit ridership	\$, Developers, City	Y		Y		High	City, Developers	City works to promote sites in conjunction with landowners/developers	Ongoing	Ongoing
18	Project/Policy	Attainable Housing Development - Encourage the development of underutilized or vacant land—particularly west of Jordan Gateway Road—for higher-density, attainable housing to improve long-term affordability and better utilize existing infrastructure.	\$, Developers, City	Y	Y	Y		High	City, Developers	City works to promote sites in conjunction with landowners/developers	Ongoing	Ongoing
19	Project/Policy	Incorporate Environmentally Friendly Design - Incorporate sustainable building practices, green design elements, and Jordan River protection measures into new development to reduce energy consumption and promote environmental preservation of the existing Jordan River Corridor.	\$, Developers, City		Y			Medium	City, Developers	City works with developers and Jordan River Commission	Ongoing	Ongoing
20	Project/Policy	"Community Identity and Sense of Place - Support placemaking improvements on Jordan Gateway consistent with Ultradent's improvements to enhance the public realm. Community-centric street improvements promote placemaking and boost pride in the community, wayfinding signage creates a sense of place within the station area, establishing a themed neighborhood feel."	\$, Developers, City			Y		High	City, Developers, Ultradent	City looks for opportunities along Jordan Gateway as infill happens to establish a theme and allocate space to celebrate growing neighborhood ownership	Ongoing support from City	Ongoing support from City

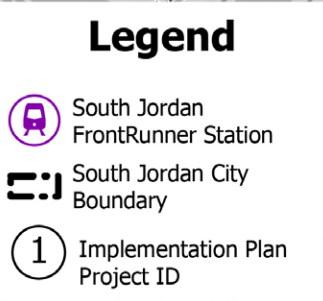


FIGURE 23: Project Implementation Plan Map

VII. APPENDICES

Appendix A – Survey Questions

Appendix B – Survey Results

Appendix C – Previous Plans and Studies

Appendix D – Property Ownership

Appendix E – Jordan River Best Practices Resources

| Appendix A – Survey Questions

A. Survey Questions



South Jordan FrontRunner Station - Public Involvement Survey

South Jordan City and Utah Transit Authority (UTA) are surveying the public on the use of the UTA South Jordan FrontRunner Station near 10600 South and I-15. We want to hear from you: *What would make you more likely to use the South Jordan FrontRunner Station or other public transit options?*

This survey only takes about 5 minutes to complete.

shawn.seager@wcg.us [Switch account](#)

Not shared

* Indicates required question



1) How often do you ride FrontRunner to or from South Jordan? Choose one. *

- ☐ A) Daily
- ☐ B) A couple of times a week
- ☐ C) Once a week
- ☐ D) Less than four times a month
- ☐ E) Rarely
- ☐ F) Never

2) What is currently preventing you from riding public transportation more frequently? Choose any that apply. *

- ☐ A) Driving my own car
- ☐ B) Service hours don't match my schedule
- ☐ C) Trains/buses don't come often enough
- ☐ D) I already use it frequently
- ☐ Other: _____

3) What purpose do you use the South Jordan FrontRunner Station for? Choose any that apply. *

- ☐ A) Travel to work
- ☐ B) Travel to school
- ☐ C) Travel to my home
- ☐ D) Leisure/social (e.g., fun, shopping, dining)
- ☐ E) Travel for recreation (e.g. Jordan River Trail)
- ☐ F) General errands or appointments (e.g., medical visit, grocery shopping)
- ☐ G) N/A
- ☐ Other: _____

4) What do you like best about riding FrontRunner to or from South Jordan? Choose up to three. *

- ☐ A) Avoid vehicle congestion during commuting hours
- ☐ B) I don't own a personal vehicle and rely on transit service
- ☐ C) Improve air quality
- ☐ D) Economic benefits - reducing my travel costs
- ☐ E) Proximity to Jordan River Trail
- ☐ F) Proximity to Shopping at South Towne Mall
- ☐ G) Proximity to my office
- ☐ H) Proximity to my home
- ☐ I) Environmental benefit
- ☐ J) N/A
- ☐ Other: _____

5) What would encourage you to ride FrontRunner / Transit more? Pick your top 5. *

- ☐ A) Increase transit frequency (more buses and/or trains per hour)
- ☐ B) Add later evening service or earlier morning service
- ☐ C) Make transit routes faster / shorter travel time between destinations
- ☐ D) Add more weekend service, including Sunday service
- ☐ E) Additional amenities and services at the station - restrooms, concessions, lockers etc.
- ☐ F) Provide more connections between regional transit services
- ☐ G) Connection to Jordan River Trail
- ☐ H) Connection to South Towne Mall
- ☐ I) Improved Safety / Security
- ☐ J) Employer reimbursement program

6) What other means of transportation do you use to get to the station? Choose any that apply. *

- ☐ A) Driving
- ☐ B) Walking
- ☐ C) Biking
- ☐ D) Scooter, skateboard, or other personal wheeled device
- ☐ E) Rideshare - Uber, Lyft
- ☐ F) Bus
- ☐ G) UTA On Demand
- ☐ H) N/A
- ☐ Other: _____

7) Please select the improvements you think would be most beneficial:

- ☐ A) Expanded employment opportunities
- ☐ B) Additional parking
- ☐ C) Potential pedestrian bridge over the freeway
- ☐ D) Increased residential homeownership opportunities
- ☐ E) Increased transit connections to South Towne and TRAX
- ☐ F) Increased Jordan Parkway crossing safety and bike lanes
- ☐ G) Increased open space and public recreation opportunities
- ☐ H) Pop up food truck plaza
- ☐ I) Increased access to Jordan River, bird refuge, passive recreation opportunities
- ☐ J) Connect the Jordan River Parkway Trail to the FrontRunner Station
- ☐ K) Increased directional and wayfinding signage
- ☐ L) Increased shade with trees or canopies

8) What other ideas do you have for this area and where?

Your answer

Submit

Page 1 of 1

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| Appendix B – Survey Results

Timestamp	1) How often do you ride FrontRunner?	2) What is currently preventing you from riding public transportation more frequently? Choose any that apply.	3) What purpose do you use the South Jordan FrontRunner Station for? Choose any that apply.	4) What do you like best about riding FrontRunner to or from South Jordan? Choose up to three.	5) What would encourage you to ride FrontRunner / Transit more? Pick your top 5.	6) What other means of transportation do you use to get to the station? Choose any that apply.	7) Please select the improvements you think would be most beneficial.	8) What other ideas do you have for this area and where?
3/25/2025 12:56:59	F) Never	F) Trains/buses don't come often enough	Na	A) Avoid vehicle congestion during commuting hours, C) Improve air quality / environmental benefit - reducing my carbon footprint, D) Economic benefits - reducing my travel costs	A) Increase transit frequency (more buses and/or trains per hour), F) Provide more connections between regional transit services, G) Connection to Jordan River Trail	A) Driving, C) Biking	4 - Increased residential ownership opportunities, 5 - Increase transit connections to South Towne and TRAX, 6 - Increase Jordan Parkway crossing safety and bike lanes, 7 - Increase open space and public recreation opportunities, 9 - increase access to Jordan River, bird refuge, passive recreation opportunities, 10 - Connect the Jordan River Parkway Trail to the FrontRunner Station	
3/25/2025 12:57:11	D) Less than four times a month	G) I already use it frequently	D) Leisure/social (e.g., fun, shopping, dining), E) Travel for recreation (e.g. Jordan River Trail)	B) I don't own a personal vehicle and rely on transit service, Don't feel like I'm going out of my way like Murray Station	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., F) Provide more connections between regional transit services, G) Connection to Jordan River Trail	C) Biking	1 - Expanded employment opportunities, 5 - Increase transit connections to South Towne and TRAX, 6 - Increase Jordan Parkway crossing safety and bike lanes, 9 - Increase access to Jordan River, bird refuge, passive recreation opportunities, 10 - Connect the Jordan River Parkway Trail to the FrontRunner Station, 12 - Increase shade with trees or canopies	A fast bus down 114th South or South Jordan Parkway would make me way more likely to use this station vs Murray.
3/25/2025 12:58:01	E) Rarely	F) Trains/buses don't come often enough, Station too far from my work	A) Travel to work	A) Avoid vehicle congestion during commuting hours, H) Proximity to my home	A) Increase transit frequency (more buses and/or trains per hour), F) Provide more connections between regional transit services, G) Connection to Jordan River Trail, J) Discounted fee program / employer incentives	A) Driving, B) Walking	6 - Increase Jordan Parkway crossing safety and bike lanes, 10 - Connect the Jordan River Parkway Trail to the FrontRunner Station, 11 - Increase directional and wayfinding signage	More transportation connections to west side of South Jordan
3/25/2025 13:13:57	F) Never	F) Trains/buses don't come often enough	A) Travel to work, D) Leisure/social (e.g., fun, shopping, dining)	N/A - I don't ride it	C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, F) Provide more connections between regional transit services	A) Driving	1 - Expanded employment opportunities, 4 - Increased residential ownership opportunities, 5 - Increase transit connections to South Towne and TRAX, 6 - Increase Jordan Parkway crossing safety and bike lanes, 7 - Increase open space and public recreation opportunities, 10 - Connect the Jordan River Parkway Trail to the FrontRunner Station, 12 - Increase shade with trees or canopies	We need bus routes in South Jordan that connect to FrontRunner. We can't go anywhere in South Jordan via the bus so the FrontRunner and TRAX stations are underutilized.
3/25/2025 13:15:02	F) Never	D) Driving my own car	A) Travel to work	D) Economic benefits - reducing my travel costs	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, C) Make transit routes faster / shorter travel time between destinations, I) Improved Safety / Security, J) Discounted fee program / employer incentives	A) Driving	1 - Expanded employment opportunities, 3 - Potential pedestrian bridge over the freeway, 5 - Increase transit connections to South Towne and TRAX, 6 - Increase Jordan Parkway crossing safety and bike lanes, 11 - Increase directional and wayfinding signage, 12 - Increase shade with trees or canopies	
3/25/2025 13:15:57	D) Less than four times a month	G) I already use it frequently	D) Leisure/social (e.g., fun, shopping, dining), F) General errands or appointments (e.g., medical visit, grocery shopping), I use the busses more often than FrontRunner	A) Avoid vehicle congestion during commuting hours, B) I don't own a personal vehicle and rely on transit service, H) Proximity to my home	A) Increase transit frequency (more buses and/or trains per hour), D) Add more weekend service, including Sunday service, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., J) Discounted fee program / employer incentives	F) Bus	5 - Increase transit connections to South Towne and TRAX, 7 - Increase open space and public recreation opportunities, 8 - Pop up food truck plaza, 11 - Increase directional and wayfinding signage	
3/25/2025 13:19:03	E) Rarely	F) Trains/buses don't come often enough	A) Travel to work	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs	C) Make transit routes faster / shorter travel time between destinations, F) Provide more connections between regional transit services	A) Driving	2 - Additional parking, 9 - Increase access to Jordan River, bird refuge, passive recreation opportunities	
3/25/2025 13:20:07	E) Rarely	D) Driving my own car	D) Leisure/social (e.g., fun, shopping, dining), E) Travel for recreation (e.g. Jordan River Trail)	A) Avoid vehicle congestion during commuting hours, C) Improve air quality / environmental benefit - reducing my carbon footprint, D) Economic benefits - reducing my travel costs	D) Add more weekend service, including Sunday service, F) Provide more connections between regional transit services, G) Connection to Jordan River Trail, I) Improved Safety / Security, J) Discounted fee program / employer incentives	A) Driving, B) Walking, C) Biking	3 - Potential pedestrian bridge over the freeway, 6 - Increase Jordan Parkway crossing safety and bike lanes, 7 - Increase open space and public recreation opportunities, 8 - Pop up food truck plaza, 9 - Increase access to Jordan River, bird refuge, passive recreation opportunities, 10 - Connect the Jordan River Parkway Trail to the FrontRunner Station, 12 - Increase shade with trees or canopies	
3/25/2025 13:21:25	D) Less than four times a month	D) Driving my own car, G) I already use it frequently	D) Leisure/social (e.g., fun, shopping, dining), E) Travel for recreation (e.g. Jordan River Trail), Travel downtown/ avoiding parking and congestion	A) Avoid vehicle congestion during commuting hours, C) Improve air quality / environmental benefit - reducing my carbon footprint, D) Economic benefits - reducing my travel costs	B) Add later evening service or earlier morning service, D) Add more weekend service, including Sunday service, F) Provide more connections between regional transit services, J) Discounted fee program / employer incentives	A) Driving	4 - Increased residential ownership opportunities, 5 - Increase transit connections to South Towne and TRAX	
3/25/2025 13:24:34	E) Rarely	D) Driving my own car	D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours	C) Make transit routes faster / shorter travel time between destinations, H) Connection to South Towne Mall	A) Driving, B) Walking	5 - Increase transit connections to South Towne and TRAX, 8 - Pop up food truck plaza, 12 - Increase shade with trees or canopies	
3/25/2025 13:25:05	D) Less than four times a month	D) Driving my own car	D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, G) Proximity to my office	D) Add more weekend service, including Sunday service	A) Driving, B) Walking	6 - Increase Jordan Parkway crossing safety and bike lanes, 8 - Pop up food truck plaza, 9 - Increase access to Jordan River, bird refuge, passive recreation opportunities, 12 - Increase shade with trees or canopies	
3/25/2025 13:25:45	E) Rarely	D) Driving my own car, E) Service hours don't match my schedule, F) Trains/buses don't come often enough	D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours	C) Make transit routes faster / shorter travel time between destinations, F) Provide more connections between regional transit services, J) Discounted fee program / employer incentives	A) Driving, B) Walking, C) Biking, D) Scooter, skateboard, or other personal wheeled device	3 - Potential pedestrian bridge over the freeway, 6 - Increase Jordan Parkway crossing safety and bike lanes, 7 - Increase open space and public recreation opportunities, 12 - Increase shade with trees or canopies	
3/25/2025 13:40:53	E) Rarely	E) Service hours don't match my schedule, F) Trains/buses don't come often enough	D) Leisure/social (e.g., fun, shopping, dining), F) General errands or appointments (e.g., medical visit, grocery shopping)	A) Avoid vehicle congestion during commuting hours, B) I don't own a personal vehicle and rely on transit service	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, F) Provide more connections between regional transit services	E) Rideshare - Uber, Lyft, UTA On Demand	2 - Additional parking, 3 - Potential pedestrian bridge over the freeway, 5 - Increase transit connections to South Towne and TRAX, 8 - Pop up food truck plaza, 12 - Increase shade with trees or canopies	
3/25/2025 13:41:33	E) Rarely	D) Driving my own car	E) Travel for recreation (e.g. Jordan River Trail)	A) Avoid vehicle congestion during commuting hours	D) Add more weekend service, including Sunday service	A) Driving	2 - Additional parking, 3 - Potential pedestrian bridge over the freeway, 5 - Increase transit connections to South Towne and TRAX, 11 - Increase directional and wayfinding signage	

Timestamp	1) How often do you ride FrontRunner?	2) What is currently preventing you from riding public transportation more frequently? Choose any that apply.	3) What purpose do you use the South Jordan FrontRunner Station for? Choose any that apply.	4) What do you like best about riding FrontRunner to or from South Jordan? Choose up to three.	5) What would encourage you to ride FrontRunner / Transit more? Pick your top 5.	6) What other means of transportation do you use to get to the station? Choose any that apply.	7) Please select the improvements you think would be most beneficial.	8) What other ideas do you have for this area and where?
3/25/2025 13:41:37	E) Rarely	I'd like to go to the airport from Daybreak on a direct line.	D) Leisure/social (e.g., fun, shopping, dining), Airport	C) Improve air quality / environmental benefit - reducing my carbon footprint, H) Proximity to my home	C) Make transit routes faster / shorter travel time between destinations	A) Driving, B) Walking	4 - Increased residential ownership opportunities, 5 - Increase transit connections to South Towne and TRAX, 7 - Increase open space and public recreation opportunities, 9 - Increase access to Jordan River, bird refuge, passive recreation opportunities, 10 - Connect the Jordan River Parkway Trail to the FrontRunner Station, 12 - Increase shade with trees or canopies	Please enhance the tools for planning trips. I always use public transportation in foreign cities, places where I don't even speak their language, but each time I consult the TRAKS/UTA site, I'm disappointed by the confusion.
3/25/2025 13:47:36	E) Rarely	D) Driving my own car	D) Leisure/social (e.g., fun, shopping, dining), E) Travel for recreation (e.g. Jordan River Trail)	A) Avoid vehicle congestion during commuting hours, C) Improve air quality / environmental benefit - reducing my carbon footprint	C) Make transit routes faster / shorter travel time between destinations, F) Provide more connections between regional transit services	A) Driving	5 - Increase transit connections to South Towne and TRAX, 8 - Pop up food truck plaza, 10 - Connect the Jordan River Parkway Trail to the FrontRunner Station, 11 - Increase directional and wayfinding signage	
3/25/2025 13:49:33	E) Rarely	F) Trains/buses don't come often enough	A) Travel to work, C) Travel to my home	A) Avoid vehicle congestion during commuting hours, H) Proximity to my home	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations	A) Driving	5 - Increase transit connections to South Towne and TRAX, 6 - Increase Jordan Parkway crossing safety and bike lanes, 12 - Increase shade with trees or canopies	
3/25/2025 13:54:59	E) Rarely	D) Driving my own car	I try to ride it when going downtown, but my assignments often end around 9:00 to 10:00. Late evening service is not good.	A) Avoid vehicle congestion during commuting hours	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, D) Add more weekend service, including Sunday service	A) Driving		The announced double-tracking and more frequent service will be a blessing.
3/25/2025 13:57:23	F) Never	D) Driving my own car, locations	D) Leisure/social (e.g., fun, shopping, dining), E) Travel for recreation (e.g. Jordan River Trail)	A) Avoid vehicle congestion during commuting hours, C) Improve air quality / environmental benefit - reducing my carbon footprint, D) Economic benefits - reducing my travel costs	C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, F) Provide more connections between regional transit services, G) Connection to Jordan River Trail, J) Discounted fee program / employer incentives	A) Driving, B) Walking, E) Rideshare - Uber, Lyft, UTA On Demand	3 - Potential pedestrian bridge over the freeway, 6 - Increase Jordan Parkway crossing safety and bike lanes, 7 - Increase open space and public recreation opportunities, 8 - Pop up food truck plaza, 9 - Increase access to Jordan River, bird refuge, passive recreation opportunities, 10 - Connect the Jordan River Parkway Trail to the FrontRunner Station	
3/25/2025 13:59:00	F) Never	I work by the station but I leave West of here, so can't use FrontRunner. I used to use it a lot before moving	don't use	A) Avoid vehicle congestion during commuting hours	A) Increase transit frequency (more buses and/or trains per hour), G) Connection to Jordan River Trail, H) Connection to South Towne Mall, J) Discounted fee program / employer incentives	A) Driving	5 - Increase transit connections to South Towne and TRAX, 6 - Increase Jordan Parkway crossing safety and bike lanes, 12 - Increase shade with trees or canopies	This has nothing to do with FrontRunner but with buses: it would be nice to have an Express bus from way West in Daybreak to the FrontRunner station. Just a thought...
3/25/2025 14:00:49	E) Rarely	F) Trains/buses don't come often enough	D) Leisure/social (e.g., fun, shopping, dining)	Jazz games	J) Discounted fee program / employer incentives	A) Driving	2 - Additional parking	Security patrols
3/25/2025 14:09:06	E) Rarely	G) I already use it frequently.	D) Leisure/social (e.g., fun, shopping, dining)	D) Economic benefits - reducing my travel costs	B) Add later evening service or earlier morning service, D) Add more weekend service, including Sunday service, H) Connection to South Towne Mall, I) Improved Safety / Security	A) Driving, B) Walking	5 - Increase transit connections to South Towne and TRAX, 7 - Increase open space and public recreation opportunities, 8 - Pop up food truck plaza, 9 - Increase access to Jordan River, bird refuge, passive recreation opportunities	
3/25/2025 14:11:16	E) Rarely	Work from home; us when heading downtown	D) Leisure/social (e.g., fun, shopping, dining), E) Travel for recreation (e.g. Jordan River Trail)	A) Avoid vehicle congestion during commuting hours, C) Improve air quality / environmental benefit - reducing my carbon footprint, I) Its fun!	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., F) Provide more connections between regional transit services	A) Driving, C) Biking	1 - Expanded employment opportunities, 3 - Potential pedestrian bridge over the freeway, 5 - Increase transit connections to South Towne and TRAX, 6 - Increase Jordan Parkway crossing safety and bike lanes, 9 - Increase access to Jordan River, bird refuge, passive recreation opportunities, 10 - Connect the Jordan River Parkway Trail to the FrontRunner Station	Cover the station (protect from weather), Clearer signage for parking and getting to the station.
3/25/2025 14:15:23	E) Rarely	Train trips take too long. Trains should be faster/similar to driving time.	D) Leisure/social (e.g., fun, shopping, dining), E) Travel for recreation (e.g. Jordan River Trail)	A) Avoid vehicle congestion during commuting hours, C) Improve air quality / environmental benefit - reducing my carbon footprint, D) Economic benefits - reducing my travel costs	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations	A) Driving, C) Biking, E) Rideshare - Uber, Lyft, UTA On Demand	3 - Potential pedestrian bridge over the freeway, 4 - Increased residential ownership opportunities, 6 - Increase Jordan Parkway crossing safety and bike lanes, 10 - Connect the Jordan River Parkway Trail to the FrontRunner Station	Have trains stop at every third location. Have first train stop at station 1, 4, 7, 10, etc. Have second train stop at 2, 5, 8, 11, etc. This will decrease stops and increase speed of service.
3/25/2025 14:24:40	F) Never	D) Driving my own car, E) Service hours don't match my schedule, F) Trains/buses don't come often enough	D) Leisure/social (e.g., fun, shopping, dining), E) Travel for recreation (e.g. Jordan River Trail), F) General errands or appointments (e.g., medical visit, grocery shopping)	C) Improve air quality / environmental benefit - reducing my carbon footprint	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, D) Add more weekend service, including Sunday service, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., I) Improved Safety / Security	A) Driving, B) Walking, C) Biking, D) Scooter, skateboard, or other personal wheeled device, E) Rideshare - Uber, Lyft, UTA On Demand	1 - Expanded employment opportunities, 2 - Additional parking, 3 - Potential pedestrian bridge over the freeway, 4 - Increased residential ownership opportunities, 5 - Increase transit connections to South Towne and TRAX, 7 - Increase open space and public recreation opportunities, 11 - Increase directional and wayfinding signage, 12 - Increase shade with trees or canopies	
3/25/2025 14:27:21	E) Rarely	F) Trains/buses don't come often enough	B) Travel to school	B) I don't own a personal vehicle and rely on transit service, C) Improve air quality / environmental benefit - reducing my carbon footprint, D) Economic benefits - reducing my travel costs	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., I) Improved Safety / Security, J) Discounted fee program / employer incentives	A) Driving	3 - Potential pedestrian bridge over the freeway, 5 - Increase transit connections to South Towne and TRAX, 6 - Increase Jordan Parkway crossing safety and bike lanes, 8 - Pop up food truck plaza	
3/25/2025 14:30:07	F) Never	D) Driving my own car, Late to work when I have ridden in the past	A) Travel to work, Visit downtown SLC	Nothing	C) Make transit routes faster / shorter travel time between destinations, J) Discounted fee program / employer incentives	A) Driving	12 - Increase shade with trees or canopies	Less costly uta travel and more on time trains
3/25/2025 14:39:45	E) Rarely	D) Driving my own car	A) Travel to work, C) Travel to my home, D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service	A) Driving	2 - Additional parking, 8 - Pop up food truck plaza	
3/25/2025 14:48:40	E) Rarely	E) Service hours don't match my schedule	D) Leisure/social (e.g., fun, shopping, dining), E) Travel for recreation (e.g. Jordan River Trail)	A) Avoid vehicle congestion during commuting hours, C) Improve air quality / environmental benefit - reducing my carbon footprint	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, D) Add more weekend service, including Sunday service	A) Driving	2 - Additional parking, 3 - Potential pedestrian bridge over the freeway, 5 - Increase transit connections to South Towne and TRAX, 12 - Increase shade with trees or canopies	
3/25/2025 14:53:04	F) Never	I live on the west side of Mountain View Corridor, so I ride the Red Line to Murray Central, and catch Front Runner from there. The lack of bus service in western South Jordan is a problem	I wish that I could use it, but I would use to to ride Front Runner to work. Too bad there are no buses where I live	I can't use it, it is too far. No buses to western South Jordan. Of course if there were, it might be quicker to ride the Red Line	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, F) Provide more connections between regional transit services	A) Driving, E) Rideshare - Uber, Lyft, UTA On Demand, Red Line (Daybreak station), Front Runner (Murray Central Station)	5 - Increase transit connections to South Towne and TRAX	Sorry, I really don't have any. South Jorday City is to big to feel connected. It is like everything West of Bangenter Highway is an afterthought. It might as well be its own city

Timestamp	1) How often do you ride FrontRunner?	2) What is currently preventing you from riding public transportation more frequently? Choose any that apply.	3) What purpose do you use the South Jordan FrontRunner Station for? Choose any that apply.	4) What do you like best about riding FrontRunner to or from South Jordan? Choose up to three.	5) What would encourage you to ride FrontRunner / Transit more? Pick your top 5.	6) What other means of transportation do you use to get to the station? Choose any that apply.	7) Please select the improvements you think would be most beneficial.	8) What other ideas do you have for this area and where?
3/25/2025 15:01:40	D) Less than four times a month	E) Service hours don't match my schedule	A) Travel to work	A) Avoid vehicle congestion during commuting hours	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., F) Provide more connections between regional transit services, I) Improved Safety / Security	A) Driving	5 - Increase transit connections to South Towne and TRAX	
3/25/2025 15:05:04	C) Once a week	They do not go where I need to travel. Doctors, shopping (grocery).	B) Travel to school	A) Avoid vehicle congestion during commuting hours	F) Provide more connections between regional transit services	A) Driving	2 - Additional parking, 3 - Potential pedestrian bridge over the freeway, 12 - Increase shade with trees or canopies	
3/25/2025 15:14:01	E) Rarely	I do not work. But I would like the choice.	A) Travel to work, C) Travel to my home, D) Leisure/social (e.g., fun, shopping, dining), F) General errands or appointments (e.g., medical visit, grocery shopping)	A) Avoid vehicle congestion during commuting hours, C) Improve air quality / environmental benefit - reducing my carbon footprint, H) Proximity to my home	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, C) Make transit routes faster / shorter travel time between destinations, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., F) Provide more connections between regional transit services	A) Driving	2 - Additional parking, 3 - Potential pedestrian bridge over the freeway, 5 - Increase transit connections to South Towne and TRAX, 6 - Increase Jordan Parkway crossing safety and bike lanes, 7 - Increase open space and public recreation opportunities, 11 - Increase directional and wayfinding signage, 12 - Increase shade with trees or canopies	
3/25/2025 15:20:31	B) A couple of times a week	D) Driving my own car	D) Leisure/social (e.g., fun, shopping, dining), E) Travel for recreation (e.g., Jordan River Trail), F) General errands or appointments (e.g., medical visit, grocery shopping)	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, F) Proximity to Shopping at South Towne Mall	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, D) Add more weekend service, including Sunday service, E) Additional amenities and services at the station - restrooms, concessions, lockers etc.	A) Driving, F) Bus	5 - Increase transit connections to South Towne and TRAX, 6 - Increase Jordan Parkway crossing safety and bike lanes, 7 - Increase open space and public recreation opportunities, 8 - Pop up food truck plaza, 10 - Connect the Jordan River Parkway Trail to the FrontRunner Station, 12 - Increase shade with trees or canopies	
3/25/2025 15:31:43	B) A couple of times a week	G) I already use it frequently	F) General errands or appointments (e.g., medical visit, grocery shopping)	A) Avoid vehicle congestion during commuting hours	A) Increase transit frequency (more buses and/or trains per hour)	A) Driving	7 - Increase open space and public recreation opportunities	
3/25/2025 15:32:06	F) Never	F) Trains/buses don't come often enough	D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours, C) Improve air quality / environmental benefit - reducing my carbon footprint	C) Make transit routes faster / shorter travel time between destinations	A) Driving		
3/25/2025 15:43:01	D) Less than four times a month	D) Driving my own car, E) Service hours don't match my schedule	D) Leisure/social (e.g., fun, shopping, dining), F) General errands or appointments (e.g., medical visit, grocery shopping)	A) Avoid vehicle congestion during commuting hours, C) Improve air quality / environmental benefit - reducing my carbon footprint, H) Proximity to my home	F) Provide more connections between regional transit services, I) Improved Safety / Security	B) Walking, C) Biking, E) Rideshare - Uber, Lyft, UTA On Demand	3 - Potential pedestrian bridge over the freeway, 5 - Increase transit connections to South Towne and TRAX, 8 - Pop up food truck plaza, 10 - Connect the Jordan River Parkway Trail to the FrontRunner Station	
3/25/2025 15:52:42	F) Never	D) Driving my own car	G) N/A	A) Avoid vehicle congestion during commuting hours, C) Improve air quality	C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, E) Additional amenities and services at the station - restrooms, concessions, lockers etc.	A) Driving	8 - Pop up food truck plaza, 9 - Increase access to Jordan River, bird refuge, passive recreation opportunities, 10 - Connect the Jordan River Parkway Trail to the FrontRunner Station, 12 - Increase shade with trees or canopies	
3/25/2025 15:54:55	F) Never	G) I already use it frequently	G) N/A	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, H) Proximity to my home	A) Increase transit frequency (more buses and/or trains per hour), G) Add later evening service or earlier morning service, C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, F) Provide more connections between regional transit services	N/A	5 - Increase transit connections to South Towne and TRAX	
3/25/2025 16:20:30	F) Never	D) Driving my own car	G) N/A	A) Avoid vehicle congestion during commuting hours, C) Improve air quality	A) Increase transit frequency (more buses and/or trains per hour), E) Additional amenities and services at the station - restrooms, concessions, lockers etc.	A) Driving	C) Potential pedestrian bridge over the freeway, F) Increased Jordan Parkway crossing safety and bike lanes	
3/25/2025 16:20:52	F) Never	D) Driving my own car	G) N/A	I don't use public transportation	B) Add later evening service or earlier morning service, C) Make transit routes faster / shorter travel time between destinations	H) N/A		
3/25/2025 16:21:18	A) Daily	E) Service hours don't match my schedule, F) Trains/buses don't come often enough, G) I already use it frequently	A) Travel to work, C) Travel to my home	B) I don't own a personal vehicle and rely on transit service, G) Proximity to my office, H) Proximity to my home	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, F) Provide more connections between regional transit services	B) Walking, F) Bus	E) Increased transit connections to South Towne and TRAX	
3/25/2025 16:24:33	D) Less than four times a month	G) I already use it frequently	D) Leisure/social (e.g., fun, shopping, dining), E) Travel for recreation (e.g., Jordan River Trail), F) General errands or appointments (e.g., medical visit, grocery shopping)	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, So I don't have to pay for parking	B) Add later evening service or earlier morning service, D) Add more weekend service, including Sunday service, I) Improved Safety / Security	A) Driving, E) Rideshare - Uber, Lyft	E) Increased transit connections to South Towne and TRAX, G) Increased open space and public recreation opportunities, I) Increased access to Jordan River, bird refuge, passive recreation opportunities	nothing comes to mind
3/25/2025 16:34:52	F) Never	safety/security	G) N/A	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, H) Proximity to my home	C) Make transit routes faster / shorter travel time between destinations, F) Provide more connections between regional transit services, I) Improved Safety / Security	H) N/A	D) Increased residential homeownership opportunities, G) Increased open space and public recreation opportunities, H) Pop up food truck plaza, L) Increased shade with trees or canopies	Finish new senior center
3/25/2025 16:35:38	E) Rarely	F) Trains/buses don't come often enough	A) Travel to work, F) General errands or appointments (e.g., medical visit, grocery shopping)	A) Avoid vehicle congestion during commuting hours	A) Increase transit frequency (more buses and/or trains per hour), E) Additional amenities and services at the station - restrooms, concessions, lockers etc., G) Connection to Jordan River Trail	A) Driving	C) Potential pedestrian bridge over the freeway, D) Increased residential homeownership opportunities, E) Increased transit connections to South Towne and TRAX, F) Increased Jordan Parkway crossing safety and bike lanes, G) Increased open space and public recreation opportunities, J) Increased access to Jordan River, bird refuge, passive recreation opportunities, J) Connect the Jordan River Parkway Trail to the FrontRunner Station	
3/25/2025 16:39:28	E) Rarely	D) Driving my own car, F) Trains/buses don't come often enough	D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours	D) Add more weekend service, including Sunday service	A) Driving	L) Increased shade with trees or canopies	
3/25/2025 16:45:32	F) Never	F) Trains/buses don't come often enough	D) Leisure/social (e.g., fun, shopping, dining), E) Travel for recreation (e.g., Jordan River Trail), F) General errands or appointments (e.g., medical visit, grocery shopping)	E) Proximity to Jordan River Trail, F) Proximity to Shopping at South Towne Mall, H) Proximity to my home	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, F) Provide more connections between regional transit services, G) Connection to Jordan River Trail, H) Connection to South Towne Mall	B) Walking, C) Biking	C) Potential pedestrian bridge over the freeway, E) Increased transit connections to South Towne and TRAX, F) Increased Jordan Parkway crossing safety and bike lanes, G) Increased open space and public recreation opportunities, H) Pop up food truck plaza, I) Increased access to Jordan River, bird refuge, passive recreation opportunities, J) Connect the Jordan River Parkway Trail to the FrontRunner Station	

Timestamp	1) How often do you ride FrontRunner?	2) What is currently preventing you from riding public transportation more frequently? Choose any that apply.	3) What purpose do you use the South Jordan FrontRunner Station for? Choose any that apply.	4) What do you like best about riding FrontRunner to or from South Jordan? Choose up to three.	5) What would encourage you to ride FrontRunner / Transit more? Pick your top 5.	6) What other means of transportation do you use to get to the station? Choose any that apply.	7) Please select the improvements you think would be most beneficial.	8) What other ideas do you have for this area and where?
3/25/2025 16:46:08	E) Rarely	D) Driving my own car, F) Trains/buses don't come often enough	D) Leisure/social (e.g., fun, shopping, dining), E) Travel for recreation (e.g., Jordan River Trail), F) General errands or appointments (e.g., medical visit, grocery shopping)	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs	D) Add more weekend service, including Sunday service, F) Provide more connections between regional transit services	A) Driving	C) Potential pedestrian bridge over the freeway, E) Increased transit connections to South Towne and TRAX, I) Increased access to Jordan River, bird refuge, passive recreation opportunities, J) Connect the Jordan River Parkway Trail to the FrontRunner Station	
3/25/2025 16:52:54	E) Rarely	D) Driving my own car	D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, Someone else does the driving	D) Add more weekend service, including Sunday service	A) Driving	F) Increased Jordan Parkway crossing safety and bike lanes	Please add Sunday routes to FrontRunner
3/25/2025 16:59:43	B) A couple of times a week	Cleanliness/The Type of People sometimes on	A) Travel to work, B) Travel to school, C) Travel to my home	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, H) Proximity to my home	A) Increase transit frequency (more buses and/or trains per hour), D) Add more weekend service, including Sunday service, I) Improved Safety / Security, J) Employer reimbursement program	A) Driving	G) Increased open space and public recreation opportunities, H) Pop up food truck plaza, I) Increased shade with trees or canopies	
3/25/2025 17:06:41	E) Rarely	D) Driving my own car, I work from home and do not use public transit often	E) Travel for recreation (e.g., Jordan River Trail)	A) Avoid vehicle congestion during commuting hours	F) Provide more connections between regional transit services	A) Driving	G) Increased open space and public recreation opportunities, K) Increased directional and wayfinding signage, B) Additional parking, C) Potential pedestrian bridge over the freeway, F) Increased Jordan Parkway crossing safety and bike lanes, H) Pop up food truck plaza, I) Increased access to Jordan River, bird refuge, passive recreation opportunities, J) Connect the Jordan River Parkway Trail to the FrontRunner Station, L) Increased shade with trees or canopies	
3/25/2025 18:52:13	F) Never	My work commute is on the west side only.	G) N/A	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, I) Environmental benefit	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, D) Add more weekend service, including Sunday service, G) Connection to Jordan River Trail, H) Connection to South Towne Mall	A) Driving		The east/west commute is the worst for where we live. We rarely take tax downtown on the weekends. But there are no worthwhile UTA connections going east/west.
3/25/2025 19:16:27	D) Less than four times a mo	D) Driving my own car	Visiting family	D) Economic benefits - reducing my travel costs	E) Additional amenities and services at the station - restrooms, concessions, lockers etc., F) Provide more connections between regional transit services, J) Employer reimbursement program	A) Driving, E) Rideshare - Uber, Lyft	E) Increased transit connections to South Towne and TRAX, L) Increased shade with trees or canopies	More offices or residential nearby
3/25/2025 19:18:13	E) Rarely	E) Service hours don't match my schedule, F) Trains/buses don't come often enough	A) Travel to work, B) Travel to school, D) Leisure/social (e.g., fun, shopping, dining), E) Travel for recreation (e.g., Jordan River Trail)	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, D) Economic benefits - reducing my travel costs	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, F) Provide more connections between regional transit services	A) Driving	A) Expanded employment opportunities, E) Increased transit connections to South Towne and TRAX, I) Increased access to Jordan River, bird refuge, passive recreation opportunities, J) Connect the Jordan River Parkway Trail to the FrontRunner Station	
3/25/2025 19:40:10	E) Rarely	D) Driving my own car, E) Service hours don't match my schedule, F) Trains/buses don't come often enough	G) N/A	A) Avoid vehicle congestion during commuting hours, I) Environmental benefit	E) Additional amenities and services at the station - restrooms, concessions, lockers etc.	A) Driving	H) Pop up food truck plaza, K) Increased directional and wayfinding signage	
3/25/2025 19:57:27	E) Rarely	D) Driving my own car, E) Service hours don't match my schedule, F) Trains/buses don't come often enough, No parking at station	A) Travel to work, C) Travel to my home, D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, D) Economic benefits - reducing my travel costs	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, D) Add more weekend service, including Sunday service, F) Provide more connections between regional transit services, I) Improved Safety / Security	A) Driving, E) Rideshare - Uber, Lyft	B) Additional parking, E) Increased transit connections to South Towne and TRAX	
3/25/2025 20:03:20	F) Never	D) Driving my own car, F) Trains/buses don't come often enough	D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours	F) Provide more connections between regional transit services, G) Connection to Jordan River Trail	A) Driving	B) Additional parking, E) Increased transit connections to South Towne and TRAX, F) Increased Jordan Parkway crossing safety and bike lanes	
3/25/2025 21:16:46	E) Rarely	G) I already use it frequently	E) Travel for recreation (e.g., Jordan River Trail)	A) Avoid vehicle congestion during commuting hours, B) I don't own a personal vehicle and rely on transit service, I) Environmental benefit	A) Increase transit frequency (more buses and/or trains per hour), D) Add more weekend service, including Sunday service, F) Provide more connections between regional transit services	B) Walking, C) Biking, F) Bus, G) UTA On Demand	E) Increased transit connections to South Towne and TRAX	When I take the Front-runner, I take Trax to the Murray Central station. I would love to have a bus option that goes all along the South Jordan Pkwy. If that existed, I would probably use the South Jordan station more often.
3/25/2025 22:22:47	F) Never	D) Driving my own car	G) N/A	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, I) Environmental benefit	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations	A) Driving, B) Walking	B) Additional parking	Express trains from Daybreak to downtown Would definitely increase ridership
3/25/2025 22:30:19	E) Rarely	D) Driving my own car	D) Leisure/social (e.g., fun, shopping, dining)	H) Proximity to my home	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, C) Make transit routes faster / shorter travel time between destinations	A) Driving		
3/25/2025 22:49:27	E) Rarely	D) Driving my own car	D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, I) I ride it downtown so I don't have to find parking.	A) Increase transit frequency (more buses and/or trains per hour), F) Provide more connections between regional transit services	A) Driving	B) Additional parking, C) Potential pedestrian bridge over the freeway, D) Increased residential homeownership opportunities, F) Increased Jordan Parkway crossing safety and bike lanes	
3/25/2025 22:50:02	E) Rarely	D) Driving my own car, E) Service hours don't match my schedule, No service on Sundays	D) Leisure/social (e.g., fun, shopping, dining), E) Travel for recreation (e.g., Jordan River Trail)	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, Safer than driving in bad weather	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, F) Provide more connections between regional transit services, I) Improved Safety / Security	A) Driving	B) Additional parking, E) Increased transit connections to South Towne and TRAX, K) Increased directional and wayfinding signage, L) Increased shade with trees or canopies	
3/26/2025 0:44:14	E) Rarely	F) Trains/buses don't come often enough, Connecting trains don't go close enough to where we are going	D) Leisure/social (e.g., fun, shopping, dining)	Avoid congestion and parking downtown for sports games or shows	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, D) Add more weekend service, including Sunday service, F) Provide more connections between regional transit services	A) Driving, Tracks	E) Increased transit connections to South Towne and TRAX, F) Increased Jordan Parkway crossing safety and bike lanes, G) Increased open space and public recreation opportunities, J) Connect the Jordan River Parkway Trail to the FrontRunner Station	We really enjoy taking public transit when we visit other cities because it is convenient. Public transit in Utah is very inconvenient because it takes way longer and connections don't go through to the right places at the right times. Just figuring out a way to make public transit feel like it's not an afterthought in city planning and have it planned around the city better.
3/26/2025 0:53:34	F) Never	D) Driving my own car	G) N/A	I don't currently use it.	I) Improved Safety / Security	A) Driving		
3/26/2025 4:22:29	D) Less than four times a mo	D) Driving my own car	D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, H) Proximity to my home	A) Increase transit frequency (more buses and/or trains per hour), D) Add more weekend service, including Sunday service, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., G) Connection to Jordan River Trail, I) Improved Safety / Security	A) Driving	B) Additional parking, G) Increased open space and public recreation opportunities, J) Connect the Jordan River Parkway Trail to the FrontRunner Station, K) Increased directional and wayfinding signage, L) Increased shade with trees or canopies	

Timestamp	1) How often do you ride FrontRunner?	2) What is currently preventing you from riding public transportation more frequently? Choose any that apply.	3) What purpose do you use the South Jordan FrontRunner Station for? Choose any that apply.	4) What do you like best about riding FrontRunner to or from South Jordan? Choose up to three.	5) What would encourage you to ride FrontRunner / Transit more? Pick your top 5.	6) What other means of transportation do you use to get to the station? Choose any that apply.	7) Please select the improvements you think would be most beneficial.	8) What other ideas do you have for this area and where?
3/26/2025 6:04:16	E) Rarely	F) Trains/buses don't come often enough	D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., F) Provide more connections between regional transit services	A) Driving	E) Increased transit connections to South Towne and TRAX, L) Increased shade with trees or canopies	More bus routes Daybreak to front runner
3/26/2025 6:30:17	F) Never	I don't know how to use it.	G) N/A	I don't use it.	A) Increase transit frequency (more buses and/or trains per hour), I) Improved Safety / Security	A) Driving	B) Additional parking, G) Increased open space and public recreation opportunities, I) Increased access to Jordan River, bird refuge, passive recreation opportunities, L) Increased shade with trees or canopies	
3/26/2025 7:30:03	E) Rarely	Buses don't go where I want to go	D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours	F) Provide more connections between regional transit services	A) Driving	B) Additional parking, C) Potential pedestrian bridge over the freeway	How about a bus that passes in front of the Jordan River temple
3/26/2025 8:33:45	E) Rarely	F) Trains/buses don't come often enough	G) N/A	A) Avoid vehicle congestion during commuting hours, C) Improve air quality	C) Make transit routes faster / shorter travel time between destinations	A) Driving	E) Increased transit connections to South Towne and TRAX	
3/26/2025 13:01:49	F) Never	A) Driving my own car	G) N/A	J) N/A	G) Connection to Jordan River Trail	H) N/A	J) Connect the Jordan River Parkway Trail to the FrontRunner Station	
3/26/2025 13:57:42	E) Rarely	B) Service hours don't match my schedule	A) Travel to work, D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, I) Environmental benefit	B) Add later evening service or earlier morning service, D) Add more weekend service, including Sunday service, E) Additional amenities and services at the station - restrooms, concessions, lockers etc.	A) Driving	L) Increased shade with trees or canopies	make sure the surrounding roadways and infrastructure can handle the additional congestion
3/26/2025 15:56:07	E) Rarely	A) Driving my own car	D) Leisure/social (e.g., fun, shopping, dining)	J) N/A	B) Add later evening service or earlier morning service, C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, F) Provide more connections between regional transit services, I) Improved Safety / Security	A) Driving	C) Potential pedestrian bridge over the freeway, E) Increased transit connections to South Towne and TRAX	
3/26/2025 16:21:41	E) Rarely	Schedules are confusing	D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours	C) Make transit routes faster / shorter travel time between destinations	A) Driving	K) Increased directional and wayfinding signage, L) Increased shade with trees or canopies	
3/26/2025 16:30:04	D) Less than four times a month	B) Service hours don't match my schedule, C) Trains/buses don't come often enough	A) Travel to work, D) Leisure/social (e.g., fun, shopping, dining), E) Travel for recreation (e.g. Jordan River Trail), Access to transportation to ski resorts	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, I) Environmental benefit	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, F) Provide more connections between regional transit services	A) Driving, E) Rideshare - Uber, Lyft	B) Additional parking, G) Increased open space and public recreation opportunities	
3/26/2025 17:32:32	D) Less than four times a month	C) Trains/buses don't come often enough	D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, H) Proximity to my home	A) Increase transit frequency (more buses and/or trains per hour), D) Add more weekend service, including Sunday service, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., G) Connection to Jordan River Trail, H) Connection to South Towne Mall	A) Driving, B) Walking, D) Scooter, skateboard, or other personal wheeled device	C) Potential pedestrian bridge over the freeway, F) Increased Jordan Parkway crossing safety and bike lanes, G) Increased open space and public recreation opportunities, I) Increased access to Jordan River, bird refuge, passive recreation opportunities, J) Connect the Jordan River Parkway Trail to the FrontRunner Station, K) Increased directional and wayfinding signage, L) Increased shade with trees or canopies	
3/26/2025 18:36:54	F) Never	B) Service hours don't match my schedule, I have to drive my car to the station and from my final destination so it is not convenient.	G) N/A	J) N/A	D) Add more weekend service, including Sunday service, F) Provide more connections between regional transit services, I) Improved Safety / Security	A) Driving	C) Potential pedestrian bridge over the freeway, F) Increased Jordan Parkway crossing safety and bike lanes, J) Connect the Jordan River Parkway Trail to the FrontRunner Station, K) Increased directional and wayfinding signage	
3/26/2025 21:07:17	E) Rarely	There's no way for me to get to the station from my house besides driving a car which is not convenient when my husband and I want to go out for drinks	D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours	F) Provide more connections between regional transit services	A) Driving	E) Increased transit connections to South Towne and TRAX, F) Increased Jordan Parkway crossing safety and bike lanes	
3/27/2025 9:29:03	E) Rarely	A) Driving my own car	D) Leisure/social (e.g., fun, shopping, dining)	fun / adventure of rail	C) Make transit routes faster / shorter travel time between destinations, H) Connection to South Towne Mall	A) Driving	B) Additional parking, C) Potential pedestrian bridge over the freeway	
3/27/2025 12:38:58	E) Rarely	A) Driving my own car, cost	A) Travel to work	A) Avoid vehicle congestion during commuting hours	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, C) Make transit routes faster / shorter travel time between destinations, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., G) Connection to Jordan River Trail	E) Rideshare - Uber, Lyft	A) Expanded employment opportunities, F) Increased Jordan Parkway crossing safety and bike lanes, G) Increased open space and public recreation opportunities, H) Pop up food truck plaza, J) Connect the Jordan River Parkway Trail to the FrontRunner Station, L) Increased shade with trees or canopies	Good Work! We love SoJo and UTA
3/27/2025 13:58:24	B) A couple of times a week	D) I already use it frequently, Unsafe to access the station - discourages me from accessing the station by bike	A) Travel to work, C) Travel to my home, D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, Do my part to reduce traffic congestion	A) Increase transit frequency (more buses and/or trains per hour), F) Provide more connections between regional transit services, G) Connection to Jordan River Trail	A) Driving, C) Biking	D) Increased residential homeownership opportunities, F) Increased Jordan Parkway crossing safety and bike lanes, J) Connect the Jordan River Parkway Trail to the FrontRunner Station	Create a community with a greater mix of uses, gathering spaces, businesses, and more housing and housing types in the area. Currently it mostly feels like an office park surrounded by parking lots.
3/27/2025 15:53:18	F) Never	don't want to drive to station because I live in Daybreak- night as well use own car	G) N/A	J) N/A	F) Provide more connections between regional transit services, H) Connection to South Towne Mall	H) N/A		Access to and from the station without a car would be huge to improve on. There aren't good east-west transit options, you take your life in your hands biking up SoJo Parkway, and if you drive then you are stuck in traffic and add to the traffic problem. We instead need convenient and safe ways to travel without a car that encourage people to be a part of the traffic solution. More widening of vehicle lanes on SoJo Parkway won't solve the traffic problem, it ultimately compounds it.
3/27/2025 20:09:45	E) Rarely	A) Driving my own car, C) Trains/buses don't come often enough	A) Travel to work	A) Avoid vehicle congestion during commuting hours, C) Improve air quality	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, F) Provide more connections between regional transit services	A) Driving, B) Walking	E) Increased transit connections to South Towne and TRAX	
3/28/2025 8:14:13	E) Rarely	D) I already use it frequently	A) Travel to work, D) Leisure/social (e.g., fun, shopping, dining), F) General errands or appointments (e.g., medical visit, grocery shopping)	A) Avoid vehicle congestion during commuting hours	A) Increase transit frequency (more buses and/or trains per hour), E) Additional amenities and services at the station - restrooms, concessions, lockers etc., F) Provide more connections between regional transit services	A) Driving, B) Walking	F) Increased Jordan Parkway crossing safety and bike lanes, G) Increased open space and public recreation opportunities, I) Increased access to Jordan River, bird refuge, passive recreation opportunities, L) Increased shade with trees or canopies	

Timestamp	1) How often do you ride FrontRunner?	2) What is currently preventing you from riding public transportation more frequently? Choose any that apply.	3) What purpose do you use the South Jordan FrontRunner Station for? Choose any that apply.	4) What do you like best about riding FrontRunner to or from South Jordan? Choose up to three.	5) What would encourage you to ride FrontRunner / Transit more? Pick your top 5.	6) What other means of transportation do you use to get to the station? Choose any that apply.	7) Please select the improvements you think would be most beneficial:	8) What other ideas do you have for this area and where?
3/28/2025 9:54:23	F) Never	B) Service hours don't match my schedule, C) Trains/buses don't come often enough	E) Travel for recreation (e.g. Jordan River Trail)	C) Improve air quality, D) Economic benefits - reducing my travel costs, I) Environmental benefit	A) Increase transit frequency (more buses and/or trains per hour), D) Add more weekend service, including Sunday service, G) Connection to Jordan River Trail	H) N/A	C) Potential pedestrian bridge over the freeway, F) Increased Jordan Parkway crossing safety and bike lanes, G) Increased open space and public recreation opportunities, I) Increased access to Jordan River, bird refuge, passive recreation opportunities	Connectivity is everything. If the station is standalone and you have to have a car to access any other amenities, it's next to useless. Add pedestrian crossings, bike travel, and buses that run regularly to nearby destinations and suddenly you've got a thriving transportation corridor that people are excited to visit. Thanks!
3/28/2025 11:40:29	E) Rarely	A) Driving my own car, C) Trains/buses don't come often enough	C) Travel to my home, D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, I) Environmental benefit	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, F) Provide more connections between regional transit services	A) Driving	C) Potential pedestrian bridge over the freeway, J) Connect the Jordan River Parkway Trail to the FrontRunner Station	
3/28/2025 11:43:08	E) Rarely	A) Driving my own car	D) Leisure/social (e.g., fun, shopping, dining)	take visitors, guests and grandchildren to various places	D) Add more weekend service, including Sunday service, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., F) Provide more connections between regional transit services, G) Connection to Jordan River Trail, H) Connection to South Towne Mall	A) Driving	E) Increased transit connections to South Towne and TRAX, J) Connect the Jordan River Parkway Trail to the FrontRunner Station	
3/28/2025 13:14:03	B) A couple of times a week	A) Driving my own car, C) Trains/buses don't come often enough	A) Travel to work, C) Travel to my home	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, F) Provide more connections between regional transit services	A) Driving	D) Increased residential homeownership opportunities, G) Increased open space and public recreation opportunities	
3/28/2025 13:14:16	A) Daily	D) I already use it frequently	A) Travel to work, E) Travel for recreation (e.g. Jordan River Trail)	B) I don't own a personal vehicle and rely on transit service, F) Proximity to Shopping at South Towne Mall, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, H) Connection to South Towne Mall	A) Driving, C) Biking	E) Increased transit connections to South Towne and TRAX	I heard about a pedestrian bridge over the highway to facilitate access to the hockey arena. That would be amazing!
3/28/2025 13:14:31	B) A couple of times a week	station not close to my home.	A) Travel to work	A) Avoid vehicle congestion during commuting hours	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, C) Make transit routes faster / shorter travel time between destinations	A) Driving	B) Additional parking	UTA Fronrunner riders take up our work parking stalls. There needs to be more UTA parking at this particular station
3/28/2025 13:14:33	B) A couple of times a week	I can work from home and the commute is long even with the FrontRunner. Also, sometimes the FrontRunner has delays which add to my commute.	A) Travel to work, D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, G) Proximity to my office	B) Add later evening service or earlier morning service, C) Make transit routes faster / shorter travel time between destinations, E) Additional amenities and services at the station - restrooms, concessions, lockers etc.	A) Driving	D) Increased residential homeownership opportunities, L) Increased shade with trees or canopies	
3/28/2025 13:14:58	F) Never	A) Driving my own car, The FrontRunner track does not go to Eagle Mountain	G) N/A	J) N/A	F) Provide more connections between regional transit services	A) Driving	D) Increased residential homeownership opportunities	
3/28/2025 13:15:00	B) A couple of times a week	D) I already use it frequently	A) Travel to work, C) Travel to my home	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, H) Connection to South Towne Mall, J) Employer reimbursement program	A) Driving, C) Biking	B) Additional parking, C) Potential pedestrian bridge over the freeway, E) Increased transit connections to South Towne and TRAX, F) Increased Jordan Parkway crossing safety and bike lanes, H) Pop up food truck plaza	Front Runner is constantly late/not on time. We need consistent train schedule AND a clear communication platform for when the train is delayed, etc. There is no good updates for it. A UTA app allowing people to follow updates and purchase/tap on/off in app would be great.
3/28/2025 13:15:16	B) A couple of times a week	D) I already use it frequently	A) Travel to work	A) Avoid vehicle congestion during commuting hours, G) Proximity to my office	E) Additional amenities and services at the station - restrooms, concessions, lockers etc., G) Connection to Jordan River Trail, H) Connection to South Towne Mall	A) Driving, B) Walking	A) Expanded employment opportunities, B) Additional parking, C) Potential pedestrian bridge over the freeway, D) Increased residential homeownership opportunities, F) Increased Jordan Parkway crossing safety and bike lanes, L) Increased shade with trees or canopies	
3/28/2025 13:15:31	B) A couple of times a week	I only go into work twice a week	A) Travel to work	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), E) Additional amenities and services at the station - restrooms, concessions, lockers etc.	A) Driving	C) Potential pedestrian bridge over the freeway, H) Pop up food truck plaza, L) Increased shade with trees or canopies	
3/28/2025 13:15:36	B) A couple of times a week	A) Driving my own car, B) Service hours don't match my schedule, C) Trains/buses don't come often enough	A) Travel to work, C) Travel to my home	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, F) Provide more connections between regional transit services	A) Driving, B) Walking, F) Bus	C) Potential pedestrian bridge over the freeway, E) Increased transit connections to South Towne and TRAX, G) Increased open space and public recreation opportunities, H) Pop up food truck plaza, I) Increased access to Jordan River, bird refuge, passive recreation opportunities, J) Connect the Jordan River Parkway Trail to the FrontRunner Station, L) Increased shade with trees or canopies	
3/28/2025 13:16:50	F) Never	B) Service hours don't match my schedule, C) Trains/buses don't come often enough	G) N/A	J) N/A	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, C) Make transit routes faster / shorter travel time between destinations, F) Provide more connections between regional transit services	A) Driving	B) Additional parking, E) Increased transit connections to South Towne and TRAX, I) Increased access to Jordan River, bird refuge, passive recreation opportunities, J) Connect the Jordan River Parkway Trail to the FrontRunner Station	Expanding the network with more station locations and improving efficiency would make a big difference.
3/28/2025 13:18:05	C) Once a week	B) Service hours don't match my schedule, C) Trains/buses don't come often enough, Lack of public transit going east/west	A) Travel to work, C) Travel to my home	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), D) Add more weekend service, including Sunday service, F) Provide more connections between regional transit services	A) Driving, B) Walking	C) Potential pedestrian bridge over the freeway, E) Increased transit connections to South Towne and TRAX, J) Connect the Jordan River Parkway Trail to the FrontRunner Station	I'd love to use public transportation, but the current system is too inconvenient. I live in Pleasant Grove and commute to South Jordan twice a week. To take public transport, I first have to drive to the AF station, which adds up to a 40-minute commute. In comparison, driving myself takes only 30 minutes, even with traffic.
3/28/2025 13:18:22	E) Rarely	A) Driving my own car	A) Travel to work	A) Avoid vehicle congestion during commuting hours, G) Proximity to my office	C) Make transit routes faster / shorter travel time between destinations	A) Driving	D) Increased residential homeownership opportunities	
3/28/2025 13:19:10	B) A couple of times a week	A) Driving my own car, Wish it was faster and didn't have as many stops and slow points between Farmington and South Jordan	A) Travel to work	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, G) Proximity to my office	C) Make transit routes faster / shorter travel time between destinations	A) Driving, B) Walking	H) Pop up food truck plaza, L) Increased shade with trees or canopies	LOVE this stop, super convenient. Just wish it didn't take so long to get there from Farmington, wish there was an express option, then I'd ride it ALL the time.
3/28/2025 13:20:15	B) A couple of times a week	A) Driving my own car	A) Travel to work	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, G) Proximity to my office	C) Make transit routes faster / shorter travel time between destinations, E) Additional amenities and services at the station - restrooms, concessions, lockers etc.	A) Driving, C) Biking	C) Potential pedestrian bridge over the freeway	

Timestamp	1) How often do you ride FrontRunner?	2) What is currently preventing you from riding public transportation more frequently? Choose any that apply.	3) What purpose do you use the South Jordan FrontRunner Station for? Choose any that apply.	4) What do you like best about riding FrontRunner to or from South Jordan? Choose up to three.	5) What would encourage you to ride FrontRunner / Transit more? Pick your top 3.	6) What other means of transportation do you use to get to the station? Choose any that apply.	7) Please select the improvements you think would be most beneficial.	8) What other ideas do you have for this area and where?
3/28/2025 13:20:26	D) Less than four times a month	The FrontRunner trains are outdated and have terrible wi-fi. Air conditioning sometimes doesn't work well in the summer.	A) Travel to work	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), E) Additional amenities and services at the station - restrooms, concessions, lockers etc.	A) Driving, C) Biking	L) Increased shade with trees or canopies	
3/28/2025 13:20:41	B) A couple of times a week	A) Driving my own car	A) Travel to work, C) Travel to my home	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., H) Connection to South Towne Mall	A) Driving	B) Additional parking, C) Potential pedestrian bridge over the freeway, E) Increased transit connections to South Towne and TRAX, H) Pop up food truck plaza	
3/28/2025 13:22:40	E) Rarely	A) Driving my own car	A) Travel to work	A) Avoid vehicle congestion during commuting hours, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), F) Provide more connections between regional transit services	A) Driving	G) Increased open space and public recreation opportunities, I) Increased access to Jordan River, bird refuge, passive recreation opportunities, J) Connect the Jordan River Parkway Trail to the FrontRunner Station, L) Increased shade with trees or canopies	
3/28/2025 13:23:18	A) Daily	C) Trains/buses don't come often enough, D) I already use it frequently	A) Travel to work, E) Travel for recreation (e.g. Jordan River Trail)	B) I don't own a personal vehicle and rely on transit service, C) Improve air quality, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, H) Connection to South Towne Mall	A) Driving, C) Biking	C) Potential pedestrian bridge over the freeway	
3/28/2025 13:23:50	B) A couple of times a week	D) I already use it frequently	A) Travel to work, C) Travel to my home	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, G) Proximity to my office	C) Make transit routes faster / shorter travel time between destinations, J) Employer reimbursement program	B) Walking	C) Potential pedestrian bridge over the freeway	
3/28/2025 13:24:07	E) Rarely	C) Trains/buses don't come often enough	A) Travel to work	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, C) Make transit routes faster / shorter travel time between destinations	A) Driving, B) Walking	C) Potential pedestrian bridge over the freeway, D) Increased residential homeownership opportunities, F) Increased Jordan Parkway crossing safety and bike lanes, G) Increased open space and public recreation opportunities, L) Increased shade with trees or canopies	
3/28/2025 13:24:14	B) A couple of times a week	D) I already use it frequently	A) Travel to work	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, D) Economic benefits - reducing my travel costs	A) Increase transit frequency (more buses and/or trains per hour), E) Additional amenities and services at the station - restrooms, concessions, lockers etc., F) Provide more connections between regional transit services	A) Driving, B) Walking	E) Increased transit connections to South Towne and TRAX, G) Increased open space and public recreation opportunities, I) Increased access to Jordan River, bird refuge, passive recreation opportunities, L) Increased shade with trees or canopies	
3/28/2025 13:24:41	D) Less than four times a month	A) Driving my own car	A) Travel to work	A) Avoid vehicle congestion during commuting hours, G) Proximity to my office, I) Environmental benefit	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations	B) Walking, C) Biking, G) UTA On Demand	B) Additional parking, F) Increased Jordan Parkway crossing safety and bike lanes	
3/28/2025 13:25:16	E) Rarely	Station is too far from house	A) Travel to work, C) Travel to my home	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, G) Proximity to my office	F) Provide more connections between regional transit services	A) Driving		It's a great station - I'd use it every day if there were a Spanish Fork frontrunner station
3/28/2025 13:25:18	D) Less than four times a month	Vineyard station has zero parking from apartments parking there and just limited parking in general	A) Travel to work, D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour)	A) Driving	B) Additional parking	
3/28/2025 13:25:25	C) Once a week	B) Service hours don't match my schedule	A) Travel to work, C) Travel to my home	A) Avoid vehicle congestion during commuting hours, G) Proximity to my office, I) Environmental benefit	A) Increase transit frequency (more buses and/or trains per hour), G) Add later evening service or earlier morning service, D) Add more weekend service, including Sunday service, F) Provide more connections between regional transit services, I) Improved Safety / Security	A) Driving	B) Additional parking	
3/28/2025 13:25:28	E) Rarely	It's expensive	A) Travel to work	D) Economic benefits - reducing my travel costs, Able to do other things while commuting	C) Make transit routes faster / shorter travel time between destinations, I) Improved Safety / Security	A) Driving	D) Increased residential homeownership opportunities	
3/28/2025 13:25:44	E) Rarely	C) Trains/buses don't come often enough, There are no buses in the west most area of south Jordan. It would take me a long time to reach the front runner station via public transport	D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours, G) Proximity to my office, I) Environmental benefit	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, F) Provide more connections between regional transit services, J) Employer reimbursement program	A) Driving, E) Rideshare - Uber, Lyft	C) Potential pedestrian bridge over the freeway, D) Increased residential homeownership opportunities, F) Increased Jordan Parkway crossing safety and bike lanes, L) Increased shade with trees or canopies	
3/28/2025 13:27:01	F) Never	A) Driving my own car, B) Service hours don't match my schedule, C) Trains/buses don't come often enough, Route	G) N/A	C) Improve air quality, D) Economic benefits - reducing my travel costs, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, C) Make transit routes faster / shorter travel time between destinations, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., F) Provide more connections between regional transit services	A) Driving, C) Biking	B) Additional parking, C) Potential pedestrian bridge over the freeway, I) Increased access to Jordan River, bird refuge, passive recreation opportunities, J) Connect the Jordan River Parkway Trail to the FrontRunner Station, L) Increased shade with trees or canopies	
3/28/2025 13:31:36	D) Less than four times a month	A) Driving my own car, Closest train/bus station takes the same time to get to as driving directly.	A) Travel to work	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, D) Economic benefits - reducing my travel costs	H) Connection to South Towne Mall	A) Driving, C) Biking	C) Potential pedestrian bridge over the freeway	
3/28/2025 13:43:07	B) A couple of times a week	D) I already use it frequently	A) Travel to work	G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations	A) Driving	L) Increased shade with trees or canopies	I wish there was a stop at the SoJo station at 10a. It currently skips a stop that time.
3/28/2025 13:45:32	D) Less than four times a month	its too cold to wait outside in the winter for me	A) Travel to work	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, Proximity to my home, Proximity to my office, Economic benefits - reducing my travel costs	E) Additional amenities and services at the station - restrooms, concessions, lockers etc.	A) Driving, B) Walking, C) Biking, D) Scooter, skateboard, or other personal wheeled device	L) Increased shade with trees or canopies	i want areas to not be freezing in during the winter lol
3/28/2025 13:45:33	C) Once a week	A) Driving my own car	A) Travel to work, E) Travel for recreation (e.g. Jordan River Trail)	A) Avoid vehicle congestion during commuting hours, E) Proximity to Jordan River Trail, G) Proximity to my office	G) Connection to Jordan River Trail	A) Driving, C) Biking	C) Potential pedestrian bridge over the freeway, F) Increased Jordan Parkway crossing safety and bike lanes, I) Increased access to Jordan River, bird refuge, passive recreation opportunities, J) Connect the Jordan River Parkway Trail to the FrontRunner Station	
3/28/2025 13:46:45	E) Rarely	No close stop by my house in Draper	A) Travel to work	A) Avoid vehicle congestion during commuting hours, G) Proximity to my office, I) Environmental benefit	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations	B) Walking	D) Increased residential homeownership opportunities, E) Increased transit connections to South Towne and TRAX	
3/28/2025 13:52:18	C) Once a week	D) I already use it frequently	A) Travel to work, Airport	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations	A) Driving	B) Additional parking	

Timestamp	1) How often do you ride FrontRunner?	2) What is currently preventing you from riding public transportation more frequently? Choose any that apply.	3) What purpose do you use the South Jordan FrontRunner Station for? Choose any that apply.	4) What do you like best about riding FrontRunner to or from South Jordan? Choose up to three.	5) What would encourage you to ride FrontRunner / Transit more? Pick your top 5.	6) What other means of transportation do you use to get to the station? Choose any that apply.	7) Please select the improvements you think would be most beneficial.	8) What other ideas do you have for this area and where?
3/28/2025 14:00:17	E) Rarely	A) Driving my own car, Other passengers are tweakin	D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours, I) Environmental benefit, Economic benefits for the public, not personal	C) Make transit routes faster / shorter travel time between destinations, I) Improved Safety / Security	A) Driving, B) Walking	C) Potential pedestrian bridge over the freeway, G) Increased open space and public recreation opportunities, L) Increased shade with trees or canopies	
3/28/2025 14:00:23	B) A couple of times a week	D) I already use it frequently	A) Travel to work	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, I) Environmental benefit	A) Increase transit frequency (more buses and/or trains per hour), D) Add more weekend service, including Sunday service	A) Driving, B) Walking	L) Increased shade with trees or canopies	
3/28/2025 14:04:52	B) A couple of times a week	D) I already use it frequently	A) Travel to work	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, I) Improved Safety / Security, J) Employer reimbursement program	F) Bus	C) Potential pedestrian bridge over the freeway, H) Pop up food truck plaza	
3/28/2025 14:11:32	B) A couple of times a week	D) I already use it frequently	A) Travel to work, C) Travel to my home	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, Ability to work while commuting	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service	A) Driving	E) Increased transit connections to South Towne and TRAX	Increased reliability (fewer delays).
3/28/2025 14:12:38	A) Daily	D) I already use it frequently	A) Travel to work, C) Travel to my home	A) Avoid vehicle congestion during commuting hours, E) Proximity to Jordan River Trail, G) Proximity to my office	B) Add later evening service or earlier morning service, C) Make transit routes faster / shorter travel time between destinations, G) Connection to Jordan River Trail	C) Biking	C) Potential pedestrian bridge over the freeway, F) Increased Jordan Parkway crossing safety and bike lanes, J) Connect the Jordan River Parkway Trail to the FrontRunner Station, L) Increased shade with trees or canopies	
3/28/2025 14:15:44	B) A couple of times a week	C) Trains/buses don't come often enough, D) I already use it frequently	A) Travel to work, C) Travel to my home	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service	B) Walking	C) Potential pedestrian bridge over the freeway, D) Increased residential homeownership opportunities, F) Increased Jordan Parkway crossing safety and bike lanes, G) Increased open space and public recreation opportunities, I) Increased access to Jordan River, bird refuge, passive recreation opportunities, J) Connect the Jordan River Parkway Trail to the FrontRunner Station	
3/28/2025 14:19:02	B) A couple of times a week	D) I already use it frequently	A) Travel to work	D) Economic benefits - reducing my travel costs, G) Proximity to my office, Work during commute	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations	A) Driving		
3/28/2025 14:20:30	B) A couple of times a week	D) I already use it frequently	A) Travel to work, D) Leisure/social (e.g., fun, shopping, dining)	C) Improve air quality, G) Proximity to my office, I) Proximity to my home	A) Increase transit frequency (more buses and/or trains per hour), D) Add more weekend service, including Sunday service, F) Provide more connections between regional transit services	B) Walking, C) Biking, F) Bus	C) Potential pedestrian bridge over the freeway, E) Increased transit connections to South Towne and TRAX, F) Increased Jordan Parkway crossing safety and bike lanes, I) Increased access to Jordan River, bird refuge, passive recreation opportunities	
3/28/2025 14:22:09	F) Never	C) Trains/buses don't come often enough, Even if they came more frequently, they take indirect routes or make so many stops that the public transit options usually take 3-4 times longer than driving, I just don't have that flexibility in my schedule to devote to transit.	G) N/A	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, I) Environmental benefit	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, F) Provide more connections between regional transit services	A) Driving, C) Biking, E) Rideshare - Uber, Lyft	E) Increased transit connections to South Towne and TRAX, G) Increased open space and public recreation opportunities	Express train from Provo to Ogden, with limited stops (eg Lehi, SLC) to make it a legitimately faster way to travel. Right now it's just not faster unless where you live and where you're going are very close to the train line.
3/28/2025 14:29:37	F) Never	A) Driving my own car, B) Service hours don't match my schedule	B) Travel to school, D) Leisure/social (e.g., fun, shopping, dining), E) Travel for recreation (e.g., Jordan River Trail)	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, E) Proximity to Jordan River Trail	G) Connection to Jordan River Trail, I) Improved Safety / Security	A) Driving, B) Walking, D) Scooter, skateboard, or other personal wheeled device	C) Potential pedestrian bridge over the freeway, E) Increased transit connections to South Towne and TRAX, F) Increased Jordan Parkway crossing safety and bike lanes, G) Increased open space and public recreation opportunities, I) Increased access to Jordan River, bird refuge, passive recreation opportunities	
3/28/2025 14:43:43	E) Rarely	Carpooling with someone who doesn't work near a station, so he has to drive anyways	A) Travel to work	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, G) Proximity to my office	F) Provide more connections between regional transit services, J) Employer reimbursement program	A) Driving		
3/28/2025 14:45:41	F) Never	A) Driving my own car	G) N/A	J) N/A	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, F) Provide more connections between regional transit services	A) Driving	D) Increased residential homeownership opportunities, F) Increased Jordan Parkway crossing safety and bike lanes, J) Connect the Jordan River Parkway Trail to the FrontRunner Station	
3/28/2025 14:47:41	B) A couple of times a week	D) I already use it frequently	A) Travel to work, D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours, G) Proximity to my office, I) Environmental benefit	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, E) Additional amenities and services at the station - restrooms, concessions, lockers etc.	B) Walking, C) Biking	C) Potential pedestrian bridge over the freeway, E) Increased transit connections to South Towne and TRAX, I) Increased access to Jordan River, bird refuge, passive recreation opportunities	
3/28/2025 14:52:53	B) A couple of times a week	D) I already use it frequently	A) Travel to work	B) I don't own a personal vehicle and rely on transit service, G) Proximity to my office, I) Environmental benefit	D) Add more weekend service, including Sunday service, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., H) Connection to South Towne Mall	A) Driving, C) Biking, D) Scooter, skateboard, or other personal wheeled device	C) Potential pedestrian bridge over the freeway, E) Increased transit connections to South Towne and TRAX, H) Pop up food truck plaza, J) Connect the Jordan River Parkway Trail to the FrontRunner Station	
3/28/2025 15:06:32	B) A couple of times a week	D) I already use it frequently	A) Travel to work	A) Avoid vehicle congestion during commuting hours, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations	A) Driving		
3/28/2025 15:40:52	B) A couple of times a week	D) I already use it frequently	A) Travel to work	A) Avoid vehicle congestion during commuting hours, G) Proximity to my office, I) Environmental benefit	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, F) Provide more connections between regional transit services, H) Connection to South Towne Mall	E) Rideshare - Uber, Lyft	C) Potential pedestrian bridge over the freeway, E) Increased transit connections to South Towne and TRAX, H) Pop up food truck plaza	
3/28/2025 16:08:26	E) Rarely	B) Service hours don't match my schedule, C) Trains/buses don't come often enough	D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours, G) Proximity to my office, H) Proximity to my home	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, D) Add more weekend service, including Sunday service, F) Provide more connections between regional transit services, J) Employer reimbursement program	A) Driving, F) Bus	C) Potential pedestrian bridge over the freeway, E) Increased transit connections to South Towne and TRAX, H) Pop up food truck plaza, J) Connect the Jordan River Parkway Trail to the FrontRunner Station, L) Increased shade with trees or canopies	The main thing I need is more direct/often bus service from Redwood Road to the station

Timestamp	1) How often do you ride FrontRunner?	2) What is currently preventing you from riding public transportation more frequently? Choose any that apply.	3) What purpose do you use the South Jordan FrontRunner Station for? Choose any that apply.	4) What do you like best about riding FrontRunner to or from South Jordan? Choose up to three.	5) What would encourage you to ride FrontRunner / Transit more? Pick your top 5.	6) What other means of transportation do you use to get to the station? Choose any that apply.	7) Please select the improvements you think would be most beneficial.	8) What other ideas do you have for this area and where?
3/28/2025 16:13:34	B) A couple of times a week	D) I already use it frequently	A) Travel to work	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour)	A) Driving		
3/28/2025 17:01:02	F) Never	A) Driving my own car, C) Trains/buses don't come often enough. There's a bus stop right by my house that could take me from Keams to the Murray station to go to Sojo for work but the commute is 1 hour during normal hours and sometimes up to an hour and half during peak times. My commute is typically 25-40 minutes regardless of traffic.	A) Travel to work	C) Improve air quality, D) Economic benefits - reducing my travel costs, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, E) Additional amenities and services at the station - restrooms, concessions, lockers etc.	A) Driving, E) Rideshare - Uber, Lyft	C) Potential pedestrian bridge over the freeway, E) Increased transit connections to South Towne and TRAX, H) Pop up food truck plaza	I work in the buildings that share parking with Sojo station and the ability to more easily and quickly get to and from work along with more options for food/recreation that I could take advantage of immediately before/during/immediately after work are the biggest draws for me. The more flexibility and optionality I have there, the more likely I am to use it. My employer offers a fully comped transit pass but my commute is 2-3x just driving. If it could be closer to 1-1.5x (or perhaps even as much as 2x but it was consistent and reliable) then I'd have to consider it.
3/28/2025 18:17:47	F) Never	C) Trains/buses don't come often enough	F) General errands or appointments (e.g., medical visit, grocery shopping)	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, D) Economic benefits - reducing my travel costs	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, F) Provide more connections between regional transit services, J) Employer reimbursement program	A) Driving		
3/29/2025 3:14:58	C) Once a week	C) Trains/buses don't come often enough	A) Travel to work	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service	C) Biking	C) Potential pedestrian bridge over the freeway, E) Increased transit connections to South Towne and TRAX	
3/29/2025 10:02:07	E) Rarely	The time difference between driving and public transit is not doable with my schedule - it's too time consuming for me to make it work.	D) Leisure/social (e.g., fun, shopping, dining), E) Travel for recreation (e.g., Jordan River Trail)	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, E) Proximity to Jordan River Trail	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, G) Connection to Jordan River Trail, J) Employer reimbursement program	A) Driving, C) Biking, E) Rideshare - Uber, Lyft, F) Bus	C) Potential pedestrian bridge over the freeway, E) Increased transit connections to South Towne and TRAX, F) Increased Jordan Parkway crossing safety and bike lanes, G) Increased open space and public recreation opportunities, I) Increased access to Jordan River, bird refuge, passive recreation opportunities, J) Connect the Jordan River Parkway Trail to the FrontRunner Station, K) Increased directional and wayfinding signage, L) Increased shade with trees or canopies	In general, improving safety and comfort for pedestrians and cyclists through enhancing and adding active transportation infrastructure.
3/29/2025 15:41:57	E) Rarely	A) Driving my own car	D) Leisure/social (e.g., fun, shopping, dining)	C) Improve air quality, I) Environmental benefit	F) Provide more connections between regional transit services	A) Driving	E) Increased transit connections to South Towne and TRAX	I live pretty far west in Daybreak, so in order to use frontrunner, I would want a TRAX line to meet up at either the Draper station or the South Jordan station. Because going north to Murray just to then go south on the Frontrunner wastes a lot of time.
3/29/2025 20:01:12	E) Rarely	No stations near house - driving to station or multiple transfers make it really inconvenient	D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, I) Environmental benefit	F) Provide more connections between regional transit services, G) Connection to Jordan River Trail	H) N/A	E) Increased transit connections to South Towne and TRAX, J) Connect the Jordan River Parkway Trail to the FrontRunner Station	It would be help to extend trax lines into more places in the city. I saw a imagined route that takes the Red line extended curve towards 118 - having trax go east west and connecting to frontrunner could really reduce the road traffic. I feel like everything - the trax lines, front runner, i-15 - all go north and south, but there's not a good way to get off that corridor.
3/30/2025 6:49:29	F) Never	A) Driving my own car, B) Service hours don't match my schedule, C) Trains/buses don't come often enough, No bus service in Herriman; a 20-min drive to the nearest Trax stop, it takes almost twice as long to get downtown than driving the whole way.	G) N/A	J) N/A	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, G) Connection to Jordan River Trail	H) N/A	F) Increased Jordan Parkway crossing safety and bike lanes, J) Connect the Jordan River Parkway Trail to the FrontRunner Station	
3/30/2025 7:37:53	E) Rarely	I work from home	D) Leisure/social (e.g., fun, shopping, dining), E) Travel for recreation (e.g., Jordan River Trail), F) General errands or appointments (e.g., medical visit, grocery shopping)	C) Improve air quality, E) Proximity to Jordan River Trail, F) Proximity to Shopping at South Towne Mall	G) Connection to Jordan River Trail, H) Connection to South Towne Mall	C) Biking, F) Bus	C) Potential pedestrian bridge over the freeway, I) Increased access to Jordan River, bird refuge, passive recreation opportunities, J) Connect the Jordan River Parkway Trail to the FrontRunner Station	
3/30/2025 11:20:00	D) Less than four times a mo	too cold or hot waiting on platform to transfer	D) Leisure/social (e.g., fun, shopping, dining), E) Travel for recreation (e.g., Jordan River Trail)	A) Avoid vehicle congestion during commuting hours	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, E) Additional amenities and services at the station - restrooms, concessions, lockers etc.	A) Driving	I) Increased access to Jordan River, bird refuge, passive recreation opportunities, J) Connect the Jordan River Parkway Trail to the FrontRunner Station, L) Increased shade with trees or canopies	bus from daybreak pkwy trax, new ballpark to SJ frontrunner, heated/air conditioned waiting area for transfers
3/31/2025 7:59:00	D) Less than four times a mo	B) Service hours don't match my schedule, C) Trains/buses don't come often enough	A) Travel to work, C) Travel to my home, D) Leisure/social (e.g., fun, shopping, dining)	G) Proximity to my office, H) Proximity to my home, I) Environmental benefit	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, F) Provide more connections between regional transit services, G) Connection to Jordan River Trail	A) Driving, C) Biking	C) Potential pedestrian bridge over the freeway, E) Increased transit connections to South Towne and TRAX, F) Increased Jordan Parkway crossing safety and bike lanes, I) Increased access to Jordan River, bird refuge, passive recreation opportunities, J) Connect the Jordan River Parkway Trail to the FrontRunner Station	Double tracking will likely have massive improvements across the board, at least if it can help when trains are down/not working. One of the biggest concerns both me and my husband have is that the transit isn't reliable enough. We've had enough situations where we planned on using transit and then train outages/issues made it so we were not going to arrive to our destination on time. This was a severe disruption (had to pay lots of \$\$\$ for an Uber, partner had to leave work to drive etc.) so it's safer to just "not" plan on using the frontrunner because the risk of it not working when you need it is too high/too frequent.
3/31/2025 8:43:03	E) Rarely	I live between the Murray and SoJo stations and work at Lucid, so my public transit commute would be really inefficient	When the car is unavailable and I need to get somewhere other than my home	A) Avoid vehicle congestion during commuting hours, G) Proximity to my office, I) Environmental benefit	C) Make transit routes faster / shorter travel time between destinations	A) Driving, C) Biking	E) Increased transit connections to South Towne and TRAX, H) Pop up food truck plaza, I) Increased access to Jordan River, bird refuge, passive recreation opportunities, J) Connect the Jordan River Parkway Trail to the FrontRunner Station	
3/31/2025 9:21:18	B) A couple of times a week	A) Driving my own car, B) Service hours don't match my schedule	A) Travel to work, C) Travel to my home	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, G) Proximity to my office	C) Make transit routes faster / shorter travel time between destinations, F) Provide more connections between regional transit services	A) Driving, B) Walking, C) Biking	B) Additional parking, C) Potential pedestrian bridge over the freeway	
3/31/2025 9:29:45	C) Once a week	B) Service hours don't match my schedule	A) Travel to work, C) Travel to my home	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, D) Add more weekend service, including Sunday service, I) Improved Safety / Security, J) Employer reimbursement program	A) Driving, B) Walking	C) Potential pedestrian bridge over the freeway, J) Connect the Jordan River Parkway Trail to the FrontRunner Station	South Jordan station is actually great. Farmington Station, however... is really frustrating and takes far too long from parking to train access. Biggest issues with getting on Frontrunner more regularly are that there should be trains every 15 mins during peak hours and the trains need to move faster. I know this is slated for investment, but it kills current utilization.
3/31/2025 9:46:41	D) Less than four times a mo	B) Service hours don't match my schedule, UTA police presence has disappeared. Trax is a homeless shelter all day every day. There is limited seating due to homeless people and all of their belongings and pets and it smells disgusting.	A) Travel to work, travel to airport	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, I) Environmental benefit	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, D) Add more weekend service, including Sunday service, I) Improved Safety / Security, J) Employer reimbursement program	A) Driving, F) Bus		

Timestamp	1) How often do you ride Front Runner?	2) What is currently preventing you from riding public transportation more frequently? Choose any that apply.	3) What purpose do you use the South Jordan FrontRunner Station for? Choose any that apply.	4) What do you like best about riding FrontRunner to or from South Jordan? Choose up to three.	5) What would encourage you to ride FrontRunner / Transit more? Pick your top 5.	6) What other means of transportation do you use to get to the station? Choose any that apply.	7) Please select the improvements you think would be most beneficial.	8) What other ideas do you have for this area and where?
3/31/2025 10:26:33	B) A couple of times a week	C) Trains/buses don't come often enough	A) Travel to work, D) Leisure/social (e.g., fun, shopping, dining), F) General errands or appointments (e.g., medical visit, grocery shopping)	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, D) Economic benefits - reducing my travel costs	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., F) Provide more connections between regional transit services	A) Driving	A) Expanded employment opportunities, D) Increased residential homeownership opportunities, E) Increased transit connections to South Towne and TRAX, L) Increased shade with trees or canopies	
3/31/2025 12:29:54	B) A couple of times a week	C) Trains/buses don't come often enough	A) Travel to work, C) Travel to my home	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), D) Add more weekend service, including Sunday service	A) Driving, C) Biking	J) Connect the Jordan River Parkway Trail to the FrontRunner Station, L) Increased shade with trees or canopies	
4/1/2025 4:02:42	D) Less than four times a month	A) Driving my own car	D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours, H) Proximity to my home	E) Additional amenities and services at the station - restrooms, concessions, lockers etc., I) Improved Safety / Security	A) Driving, E) Rideshare - Uber, Lyft	B) Additional parking, E) Increased transit connections to South Towne and TRAX, G) Increased open space and public recreation opportunities, L) Increased shade with trees or canopies	
4/1/2025 7:33:36	B) A couple of times a week	A) Driving my own car, C) Trains/buses don't come often enough	B) Travel to school	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, Proximity to Roseman university	C) Make transit routes faster / shorter travel time between destinations, E) Additional amenities and services at the station - restrooms, concessions, lockers etc.	A) Driving, B) Walking, F) Bus	L) Increased shade with trees or canopies	Restrooms would be great. Student pass including Roseman university
4/1/2025 7:34:20	A) Daily	D) I already use it frequently	A) Travel to work, B) Travel to school, C) Travel to my home, D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, D) Economic benefits - reducing my travel costs	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, E) Additional amenities and services at the station - restrooms, concessions, lockers etc.	A) Driving	C) Potential pedestrian bridge over the freeway, E) Increased transit connections to South Towne and TRAX, G) Increased open space and public recreation opportunities, I) Increased access to Jordan River, bird refuge, passive recreation opportunities, J) Connect the Jordan River Parkway Trail to the FrontRunner Station, K) Increased directional and wayfinding signage, L) Increased shade with trees or canopies	
4/1/2025 7:36:00	B) A couple of times a week	D) I already use it frequently	A) Travel to work, C) Travel to my home, D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), D) Add more weekend service, including Sunday service, I) Improved Safety / Security, J) Employer reimbursement program	A) Driving	F) Increased Jordan Parkway crossing safety and bike lanes, I) Increased access to Jordan River, bird refuge, passive recreation opportunities, J) Connect the Jordan River Parkway Trail to the FrontRunner Station, K) Increased directional and wayfinding signage	
4/1/2025 8:04:59	B) A couple of times a week	C) Trains/buses don't come often enough	B) Travel to school	B) I don't own a personal vehicle and rely on transit service	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, E) Additional amenities and services at the station - restrooms, concessions, lockers etc.	F) Bus	C) Potential pedestrian bridge over the freeway, E) Increased transit connections to South Towne and TRAX, H) Pop up food truck plaza, J) Connect the Jordan River Parkway Trail to the FrontRunner Station, L) Increased shade with trees or canopies	Please ask bus drivers to never be early!! Because they pass up to 8 minutes early, it takes another 45-60 minutes longer for me to get where I need to go!! It is better they be late than early. I spend most of my time waiting for buses than I do riding them because they pass by earlier than scheduled. Also, bus 218 should come more often than once an hour. Thank you.
4/1/2025 8:28:42	B) A couple of times a week	C) Trains/buses don't come often enough, D) I already use it frequently	A) Travel to work, D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours, G) Proximity to my office, H) Proximity to my home	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., F) Provide more connections between regional transit services	A) Driving, B) Walking, D) Scooter, skateboard, or other personal wheeled device, E) Rideshare - Uber, Lyft	C) Potential pedestrian bridge over the freeway, E) Increased transit connections to South Towne and TRAX, G) Increased open space and public recreation opportunities, H) Pop up food truck plaza, L) Increased shade with trees or canopies	
4/1/2025 8:31:11	B) A couple of times a week	C) Trains/buses don't come often enough	A) Travel to work	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., F) Provide more connections between regional transit services, J) Employer reimbursement program	A) Driving, C) Biking, F) Bus, G) UTA On Demand	J) Connect the Jordan River Parkway Trail to the FrontRunner Station, K) Increased directional and wayfinding signage	
4/1/2025 8:40:32	A) Daily	D) I already use it frequently	A) Travel to work, C) Travel to my home	A) Avoid vehicle congestion during commuting hours, G) Proximity to my office, I) Environmental benefit	C) Make transit routes faster / shorter travel time between destinations	A) Driving, B) Walking	H) Pop up food truck plaza, I) Increased access to Jordan River, bird refuge, passive recreation opportunities	
4/1/2025 9:35:05	B) A couple of times a week	D) I already use it frequently	A) Travel to work	A) Avoid vehicle congestion during commuting hours	A) Increase transit frequency (more buses and/or trains per hour)	A) Driving	F) Increased Jordan Parkway crossing safety and bike lanes, H) Pop up food truck plaza	
4/1/2025 10:07:17	B) A couple of times a week	B) Service hours don't match my schedule, C) Trains/buses don't come often enough, D) I already use it frequently	A) Travel to work, C) Travel to my home	A) Avoid vehicle congestion during commuting hours, B) I don't own a personal vehicle and rely on transit service, I) Environmental benefit	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., J) Employer reimbursement program	B) Walking, F) Bus	A) Expanded employment opportunities, C) Potential pedestrian bridge over the freeway, D) Increased residential homeownership opportunities, J) Connect the Jordan River Parkway Trail to the FrontRunner Station	
4/1/2025 11:52:46	E) Rarely	C) Trains/buses don't come often enough, Much longer commute when using public transit	A) Travel to work	C) Improve air quality, G) Proximity to my office, I) Environmental benefit	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service	B) Walking, C) Biking, F) Bus	C) Potential pedestrian bridge over the freeway, H) Pop up food truck plaza, L) Increased shade with trees or canopies	
4/1/2025 13:04:02	E) Rarely	A) Driving my own car	I haven't yet had the need to use the FrontRunner, but I use the TRAX Redline frequently.	J) N/A	D) Add more weekend service, including Sunday service	A) Driving, E) Rideshare - Uber, Lyft	E) Increased transit connections to South Towne and TRAX	
4/1/2025 13:13:17	F) Never	there is no service in the areas I need to go	just drive by and wish that it connected to the places I need/want to go	I would ride it for all of the above if it was possible to use it effectively	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, F) Provide more connections between regional transit services	H) N/A, its not worth the effort to get there for most trips I need to make	F) Increased Jordan Parkway crossing safety and bike lanes, I) Increased access to Jordan River, bird refuge, passive recreation opportunities, J) Connect the Jordan River Parkway Trail to the FrontRunner Station	more bus service to go west into South Jordan and connect through to Herriman
4/1/2025 16:11:36	B) A couple of times a week	A) Driving my own car, C) Trains/buses don't come often enough	A) Travel to work, C) Travel to my home	A) Avoid vehicle congestion during commuting hours, G) Proximity to my office, I) Environmental benefit	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, F) Provide more connections between regional transit services, G) Connection to Jordan River Trail	C) Biking	A) Expanded employment opportunities, C) Potential pedestrian bridge over the freeway, D) Increased residential homeownership opportunities, E) Increased transit connections to South Towne and TRAX, H) Pop up food truck plaza, J) Connect the Jordan River Parkway Trail to the FrontRunner Station	In the near term having any kind of food options would be amazing. Longer term I would love to see more residential units near the station so more people can enjoy the wonderful service that FrontRunner provides for our valley.

Timestamp	1) How often do you ride FrontRunner?	2) What is currently preventing you from riding public transportation more frequently? Choose any that apply.	3) What purpose do you use the South Jordan FrontRunner Station for? Choose any that apply.	4) What do you like best about riding FrontRunner to or from South Jordan? Choose up to three.	5) What would encourage you to ride FrontRunner / Transit more? Pick your top 5.	6) What other means of transportation do you use to get to the station? Choose any that apply.	7) Please select the improvements you think would be most beneficial.	8) What other ideas do you have for this area and where?
4/1/2025 16:19:18	B) A couple of times a week	C) Trains/buses don't come often enough, D) I already use it frequently	A) Travel to work, C) Travel to my home	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, D) Add more weekend service, including Sunday service, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., F) Provide more connections between regional transit services	B) Walking, F) Bus	C) Potential pedestrian bridge over the freeway, E) Increased transit connections to South Towne and TRAX, F) Increased Jordan Parkway crossing safety and bike lanes, G) Increased open space and public recreation opportunities, H) Pop up food truck plaza, J) Connect the Jordan River Parkway Trail to the FrontRunner Station	I would much appreciate public bathrooms and signage about their accessibility
4/1/2025 16:23:21	B) A couple of times a week	C) Trains/buses don't come often enough, D) I already use it frequently	A) Travel to work, C) Travel to my home, D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours, G) Proximity to my office, H) Proximity to my home	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., F) Provide more connections between regional transit services	A) Driving, B) Walking, D) Scooter, skateboard, or other personal wheeled device, E) Rideshare - Uber, Lyft	C) Potential pedestrian bridge over the freeway, E) Increased transit connections to South Towne and TRAX, G) Increased open space and public recreation opportunities, H) Pop up food truck plaza, L) Increased shade with trees or canopies	
4/1/2025 16:52:11	E) Rarely	A) Driving my own car, Don't need a monthly pass and would take frontrunner 2 days a week but need a discounted punch card or the ability to buy a specific number of rides each month or quarter	A) Travel to work	A) Avoid vehicle congestion during commuting hours, G) Proximity to my office	B) Add later evening service or earlier morning service, D) Add more weekend service, including Sunday service, J) Employer reimbursement program	A) Driving	H) Pop up food truck plaza, I) Increased access to Jordan River, bird refuge, passive recreation opportunities, J) Connect the Jordan River Parkway Trail to the FrontRunner Station	
4/1/2025 16:53:58	B) A couple of times a week	D) I already use it frequently	A) Travel to work	A) Avoid vehicle congestion during commuting hours, G) Proximity to my office, H) Proximity to my home	C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., J) Employer reimbursement program	A) Driving, B) Walking	A) Expanded employment opportunities, E) Increased transit connections to South Towne and TRAX, L) Increased shade with trees or canopies	I think the train wifi needs a major boost
4/1/2025 17:12:46	E) Rarely	C) Trains/buses don't come often enough	C) Travel to my home	H) Proximity to my home	C) Make transit routes faster / shorter travel time between destinations	B) Walking, D) Scooter, skateboard, or other personal wheeled device	A) Expanded employment opportunities, J) Connect the Jordan River Parkway Trail to the FrontRunner Station	
4/1/2025 17:13:26	E) Rarely	A) Driving my own car	B) Travel to school, C) Travel to my home	A) Avoid vehicle congestion during commuting hours, B) I don't own a personal vehicle and rely on transit service	A) Increase transit frequency (more buses and/or trains per hour), I) Improved Safety / Security	B) Walking, G) UTA On Demand	A) Expanded employment opportunities, H) Pop up food truck plaza	Have security inside the train and on the train station for safety
4/1/2025 17:21:30	A) Daily	D) I already use it frequently	A) Travel to work, C) Travel to my home	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), H) Connection to South Towne Mall	B) Walking, F) Bus	C) Potential pedestrian bridge over the freeway, E) Increased transit connections to South Towne and TRAX	Pedestrian bridge over the freeway would cover it
4/1/2025 17:25:37	C) Once a week	C) Trains/buses don't come often enough	A) Travel to work	I can use when I don't have my car	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, J) Employer reimbursement program	A) Driving, B) Walking, E) Rideshare - Uber, Lyft, F) Bus, G) UTA On Demand	E) Increased transit connections to South Towne and TRAX, J) Connect the Jordan River Parkway Trail to the FrontRunner Station	
4/2/2025 7:00:05	A) Daily	D) I already use it frequently	A) Travel to work, B) Travel to school, C) Travel to my home, D) Leisure/social (e.g., fun, shopping, dining), E) Travel for recreation (e.g., Jordan River Trail)	A) Avoid vehicle congestion during commuting hours	A) Increase transit frequency (more buses and/or trains per hour)	D) Scooter, skateboard, or other personal wheeled device, E) Rideshare - Uber, Lyft, F) Bus, G) UTA On Demand		
4/2/2025 7:04:57	A) Daily	C) Trains/buses don't come often enough, D) I already use it frequently	A) Travel to work, C) Travel to my home	D) Economic benefits - reducing my travel costs, G) Proximity to my office, H) Proximity to my home	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., F) Provide more connections between regional transit services, I) Improved Safety / Security	A) Driving	E) Increased transit connections to South Towne and TRAX, L) Increased shade with trees or canopies	Additional train cars during busy hours for more sitting or educate riders to be willing to share seats when they take up two spots, something to help keep us cool or warm while waiting for the train (Especially when there are delays).
4/2/2025 7:29:25	A) Daily	D) I already use it frequently	A) Travel to work, Visiting a friend near the Orem station	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, H) Proximity to my home	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, C) Make transit routes faster / shorter travel time between destinations, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., J) Employer reimbursement program	A) Driving	B) Additional parking, G) Increased open space and public recreation opportunities, L) Increased shade with trees or canopies	Heaters in winter. Better App/way to track trains and get updates.
4/2/2025 7:30:24	A) Daily	C) Trains/buses don't come often enough, D) I already use it frequently	B) Travel to school, D) Leisure/social (e.g., fun, shopping, dining)	B) I don't own a personal vehicle and rely on transit service, D) Economic benefits - reducing my travel costs, F) Proximity to Shopping at South Towne Mall	A) Increase transit frequency (more buses and/or trains per hour), D) Add more weekend service, including Sunday service, F) Provide more connections between regional transit services, H) Connection to South Towne Mall	A) Driving, B) Walking, F) Bus	C) Potential pedestrian bridge over the freeway, E) Increased transit connections to South Towne and TRAX, H) Pop up food truck plaza	
4/2/2025 7:31:02	B) A couple of times a week	A) Driving my own car, Remote work every other week	A) Travel to work	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, Employer offers commuter benefit free to us AND less stress in the morning... like getting chauffeured to work.	C) Make transit routes faster / shorter travel time between destinations	A) Driving		What if train transportation could be dramatically improved and personalized for timing. You arrive at the train station when you want, there are two tracks, coming and going, that are always ready to receive passengers. The stations have acceleration (like the Superman ride at Magic Mountain) tracks that get the pods to speed before merging onto the main coming or going tracks. I don't like waiting at the stations in the cold and I don't like having waiting too long and stopping at stations along the way.
4/2/2025 7:32:37	A) Daily	D) I already use it frequently	B) Travel to school	B) I don't own a personal vehicle and rely on transit service, D) Economic benefits - reducing my travel costs, H) Proximity to my home	C) Make transit routes faster / shorter travel time between destinations, E) Additional amenities and services at the station - restrooms, concessions, lockers etc.	A) Driving	G) Increased open space and public recreation opportunities, H) Pop up food truck plaza, L) Increased shade with trees or canopies	
4/2/2025 7:32:45	A) Daily	A) Driving my own car	B) Travel to school	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, H) Proximity to my home	A) Increase transit frequency (more buses and/or trains per hour), E) Additional amenities and services at the station - restrooms, concessions, lockers etc.	A) Driving	E) Increased transit connections to South Towne and TRAX	
4/2/2025 7:33:48	A) Daily	D) I already use it frequently	B) Travel to school	B) I don't own a personal vehicle and rely on transit service, D) Economic benefits - reducing my travel costs	E) Additional amenities and services at the station - restrooms, concessions, lockers etc.	A) Driving	G) Increased open space and public recreation opportunities, H) Pop up food truck plaza, L) Increased shade with trees or canopies	
4/2/2025 7:47:13	A) Daily	D) I already use it frequently	A) Travel to work	B) I don't own a personal vehicle and rely on transit service	D) Add more weekend service, including Sunday service	B) Walking, F) Bus		
4/2/2025 7:57:44	A) Daily	D) I already use it frequently	B) Travel to school	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs	E) Additional amenities and services at the station - restrooms, concessions, lockers etc., H) Connection to South Towne Mall, I) Improved Safety / Security	A) Driving, B) Walking	C) Potential pedestrian bridge over the freeway, I) Increased access to Jordan River, bird refuge, passive recreation opportunities, K) Increased directional and wayfinding signage, L) Increased shade with trees or canopies	
4/2/2025 8:04:33	B) A couple of times a week	B) Service hours don't match my schedule, C) Trains/buses don't come often enough	A) Travel to work	Usually able to relax on the train	A) Increase transit frequency (more buses and/or trains per hour)	A) Driving	L) Increased shade with trees or canopies	More shelter from the elements.

Timestamp	1) How often do you ride FrontRunner / Transit more frequently? Choose any that apply.	2) What is currently preventing you from riding public transportation more frequently? Choose any that apply.	3) What purpose do you use the South Jordan FrontRunner Station for? Choose any that apply.	4) What do you like best about riding FrontRunner / or from South Jordan? Choose up to three.	5) What would encourage you to ride FrontRunner / Transit more? Pick your top 5.	6) What other means of transportation do you use to get to the station? Choose any that apply.	7) Please select the improvements you think would be most beneficial.	8) What other ideas do you have for this area and where?
4/2/2025 8:11:44	B) A couple of times a week	I only use it when I need to go to the office in Salt Lake. Two times a weeks or sometimes more often.	A) Travel to work	A) Avoid vehicle congestion during commuting hours, B) I don't own a personal vehicle and rely on transit service, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), D) Add more weekend service, including Sunday service, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., F) Provide more connections between regional transit services, G) Connection to Jordan River Trail	A) Driving, F) Bus	C) Potential pedestrian bridge over the freeway, F) Increased Jordan Parkway crossing safety and bike lanes, G) Increased open space and public recreation opportunities, J) Connect the Jordan River Parkway Trail to the FrontRunner Station, L) Increased shade with trees or canopies	
4/2/2025 8:16:28	A) Daily	D) I already use it frequently	B) Travel to school, C) Travel to my home	C) Improve air quality, D) Economic benefits - reducing my travel costs, I) Environmental benefit	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., F) Provide more connections between regional transit services	A) Driving, F) Bus	C) Potential pedestrian bridge over the freeway, E) Increased transit connections to South Towne and TRAX, H) Pop up food truck plaza, L) Increased shade with trees or canopies	
4/2/2025 8:23:47	A) Daily	B) Service hours don't match my schedule, C) Trains/buses don't come often enough	A) Travel to work, C) Travel to my home	A) Avoid vehicle congestion during commuting hours, G) Proximity to my office, I) Environmental benefit	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, F) Provide more connections between regional transit services, J) Employer reimbursement program	A) Driving	C) Potential pedestrian bridge over the freeway, D) Increased residential homeownership opportunities, E) Increased transit connections to South Towne and TRAX, F) Increased Jordan Parkway crossing safety and bike lanes, J) Connect the Jordan River Parkway Trail to the FrontRunner Station	Really appreciate the free parking at all front runner stations
4/2/2025 8:28:51	A) Daily	D) I already use it frequently	B) Travel to school, C) Travel to my home, D) Leisure/social (e.g., fun, shopping, dining), E) Travel for recreation (e.g., Jordan River Trail), F) General errands or appointments (e.g., medical visit, grocery shopping)	B) I don't own a personal vehicle and rely on transit service, C) Improve air quality, I) Environmental benefit	A) Increase transit frequency (more buses and/or trains per hour), D) Add more weekend service, including Sunday service, F) Provide more connections between regional transit services, G) Connection to Jordan River Trail, H) Connection to South Towne Mall	A) Driving, B) Walking, E) Rideshare - Uber, Lyft, F) Bus, G) UTA On Demand	C) Potential pedestrian bridge over the freeway, E) Increased transit connections to South Towne and TRAX, F) Increased Jordan Parkway crossing safety and bike lanes, G) Increased open space and public recreation opportunities, H) Pop up food truck plaza, I) Increased access to Jordan River, bird refuge, passive recreation opportunities, J) Connect the Jordan River Parkway Trail to the FrontRunner Station	I'd love more bus stops in the South Jordan area similar to how Orem/Provo transit is structured. Specifically, transit to The District and South Town Mall (or the various shopping districts/recreational areas nearby) would help make the area more accessible to more people, and would bring all of the environmental/economical benefits public transit provides.
4/2/2025 8:31:43	A) Daily	D) I already use it frequently	B) Travel to school, D) Leisure/social (e.g., fun, shopping, dining), E) Travel for recreation (e.g., Jordan River Trail), F) General errands or appointments (e.g., medical visit, grocery shopping)	A) Avoid vehicle congestion during commuting hours, F) Proximity to Shopping at South Towne Mall, I) Environmental benefit	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, D) Add more weekend service, including Sunday service	B) Walking, C) Biking, D) Scooter, skateboard, or other personal wheeled device, F) Bus	C) Potential pedestrian bridge over the freeway, D) Increased residential homeownership opportunities, E) Increased transit connections to South Towne and TRAX, F) Increased Jordan Parkway crossing safety and bike lanes, G) Increased open space and public recreation opportunities, I) Increased access to Jordan River, bird refuge, passive recreation opportunities, J) Connect the Jordan River Parkway Trail to the FrontRunner Station, K) Increased directional and wayfinding signage, L) Increased shade with trees or canopies	A crosswalk at the south end of the station would have my walk
4/2/2025 8:34:18	A) Daily	D) I already use it frequently	A) Travel to work, C) Travel to my home	B) I don't own a personal vehicle and rely on transit service, C) Improve air quality, G) Proximity to my office	F) Provide more connections between regional transit services, I) Improved Safety / Security	E) Rideshare - Uber, Lyft, G) UTA On Demand, UTA on demand was not upto the mark because they are very rare to connect and we need to spend around 20 minutes on the app continuously	E) Increased transit connections to South Towne and TRAX, L) Increased shade with trees or canopies	UTA on demand was not upto the mark because they are very rare to connect and we need to spend around 20 minutes on the app continuously And we're not sure the cab will connect. I was completely dependent on the UTA demand because there is no connection to south jordan frontrunner station from my home (420 w cadbury dr.)
4/2/2025 8:41:08	B) A couple of times a week	A) Driving my own car, Alternating work schedule	A) Travel to work, C) Travel to my home	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), E) Additional amenities and services at the station - restrooms, concessions, lockers etc., F) Provide more connections between regional transit services, I) Improved Safety / Security	A) Driving, D) Scooter, skateboard, or other personal wheeled device, F) Bus	C) Potential pedestrian bridge over the freeway, H) Pop up food truck plaza	Increase Buses to the Jordan River Parkway. Maybe every 30 minutes but an hourly bus it hard and inconvenient
4/2/2025 8:49:28	A) Daily	A) Driving my own car, B) Service hours don't match my schedule, C) Trains/buses don't come often enough, D) I already use it frequently	B) Travel to school	A) Avoid vehicle congestion during commuting hours, B) I don't own a personal vehicle and rely on transit service, D) Economic benefits - reducing my travel costs	A) Increase transit frequency (more buses and/or trains per hour), D) Add more weekend service, including Sunday service, E) Additional amenities and services at the station - restrooms, concessions, lockers etc.	A) Driving, E) Rideshare - Uber, Lyft	B) Additional parking, L) Increased shade with trees or canopies	
4/2/2025 8:53:04	B) A couple of times a week	B) Service hours don't match my schedule, C) Trains/buses don't come often enough	A) Travel to work, C) Travel to my home	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, H) Proximity to my home	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, F) Provide more connections between regional transit services, I) Improved Safety / Security	A) Driving, B) Walking, F) Bus	A) Expanded employment opportunities, C) Potential pedestrian bridge over the freeway, G) Increased open space and public recreation opportunities, L) Increased shade with trees or canopies	More bus routes and have employers promote riding public transportation as part of their benefits package.
4/2/2025 8:53:31	A) Daily	D) I already use it frequently	B) Travel to school	A) Avoid vehicle congestion during commuting hours, H) Proximity to my home	C) Make transit routes faster / shorter travel time between destinations	A) Driving		
4/2/2025 9:00:52	A) Daily	D) I already use it frequently	A) Travel to work, B) Travel to school	A) Avoid vehicle congestion during commuting hours, B) I don't own a personal vehicle and rely on transit service, C) Improve air quality	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, H) Connection to South Towne Mall	A) Driving, F) Bus	B) Additional parking, E) Increased transit connections to South Towne and TRAX	
4/2/2025 9:07:13	B) A couple of times a week	C) Trains/buses don't come often enough. Needs more frequent connection transportation to places of interest like grocery shopping or malls	A) Travel to work, D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours, H) Proximity to my home, I) Environmental benefit	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., F) Provide more connections between regional transit services, I) Improved Safety / Security	B) Walking	A) Expanded employment opportunities, E) Increased transit connections to South Towne and TRAX, F) Increased Jordan Parkway crossing safety and bike lanes, G) Increased open space and public recreation opportunities, H) Pop up food truck plaza, I) Increased access to Jordan River, bird refuge, passive recreation opportunities, J) Connect the Jordan River Parkway Trail to the FrontRunner Station, L) Increased shade with trees or canopies	As someone who loves learning about urban planning in Europe and Japan, it'd be great if we could have light rails on main roads that branch off of the front runner station. Like eliminate 1 or 2 lanes middle lanes for a light rail that goes on 10400 S heading west. There's so much congestion there during rush hour that if we had more options of transportation, it would spread out how people travel and hopefully reduce car traffic as well. Then maybe off of that light rail route, there could be bus stations for busses with their own lanes to take people through smaller streets. If this was as fast, convenient, and reliable as driving, I think it could work. And of course, expanding the front runner east and west and having more light rails branch off of those would be amazing too!
4/2/2025 9:09:23	B) A couple of times a week	B) Service hours don't match my schedule, C) Trains/buses don't come often enough	A) Travel to work, C) Travel to my home	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, E) Additional amenities and services at the station - restrooms, concessions, lockers etc.	B) Walking, D) Scooter, skateboard, or other personal wheeled device, F) Bus	D) Increased residential homeownership opportunities, E) Increased transit connections to South Towne and TRAX, L) Increased shade with trees or canopies	Double track would be huge, but that's more of a frontrunner wide thing and not a south jordan station thing. It would also help with consistency of trains which would be huge
4/2/2025 13:51:52	A) Daily	D) I already use it frequently	A) Travel to work, C) Travel to my home	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), J) Employer reimbursement program	C) Biking	C) Potential pedestrian bridge over the freeway	

Timestamp	1) How often do you ride FrontRunner?	2) What is currently preventing you from riding public transportation more frequently? Choose any that apply.	3) What purpose do you use the South Jordan FrontRunner Station for? Choose any that apply.	4) What do you like best about riding FrontRunner to or from South Jordan? Choose up to three.	5) What would encourage you to ride FrontRunner / Transit more? Pick your top 5.	6) What other means of transportation do you use to get to the station? Choose any that apply.	7) Please select the improvements you think would be most beneficial.	8) What other ideas do you have for this area and where?
4/2/2025 16:22:44	A) Daily	B) Service hours don't match my schedule, C) Trains/buses don't come often enough, the train speed isn't fast enough sometimes	A) Travel to work, B) Travel to school, C) Travel to my home, D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, D) Economic benefits - reducing my travel costs	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, C) Make transit routes faster / shorter travel time between destinations, F) Provide more connections between regional transit services, J) Employer reimbursement program	A) Driving, B) Walking, F) Bus	B) Additional parking, C) Potential pedestrian bridge over the freeway, E) Increased transit connections to South Towne and TRAX, G) Increased open space and public recreation opportunities, L) Increased shade with trees or canopies	Increased parking here, but also shuttles that go to and from Morgan Stanley. We have hundreds of employees but it's a 21 minute walk for us if we take the front runner like me. but mainly in Vineyard! I come from vineyard every day and parking is absolutely horrible and there is only one entrance!
4/2/2025 16:25:23	B) A couple of times a week	B) Service hours don't match my schedule, C) Trains/buses don't come often enough, Price to include family members in travel (I am on a monthly pass, they are not)	A) Travel to work, C) Travel to my home	D) Economic benefits - reducing my travel costs, E) Proximity to Jordan River Trail, Internet access and power stations allow me to work on my commute. It allows me to be with my family sooner. (Also, safety against accidents)	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., F) Provide more connections between regional transit services, G) Connection to Jordan River Trail	A) Driving, B) Walking, C) Biking, Friend/other ride	B) Additional parking, D) Increased residential homeownership opportunities, E) Increased transit connections to South Towne and TRAX, F) Increased Jordan Parkway crossing safety and bike lanes, I) Increased access to Jordan River, bird refuge, passive recreation opportunities, J) Connect the Jordan River Parkway Trail to the FrontRunner Station, L) Increased shade with trees or canopies	Windbreaks/heating or cooling options during uncomfortable temperatures would make me want to ride the train more.
4/2/2025 16:57:59	C) Once a week	No need do due to work from home schedule.	A) Travel to work	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, G) Proximity to my office	J) Employer reimbursement program	B) Walking, D) Scooter, skateboard, or other personal wheeled device	A) Expanded employment opportunities	
4/2/2025 16:58:49	A) Daily	D) I already use it frequently	A) Travel to work, C) Travel to my home, D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), D) Add more weekend service, including Sunday service, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., F) Provide more connections between regional transit services, J) Employer reimbursement program	A) Driving, E) Rideshare - Uber, Lyft, G) UTA On Demand	C) Potential pedestrian bridge over the freeway, D) Increased residential homeownership opportunities, E) Increased transit connections to South Towne and TRAX	
4/2/2025 17:03:57	A) Daily	D) I already use it frequently	A) Travel to work, C) Travel to my home	A) Avoid vehicle congestion during commuting hours, H) Proximity to my home, The South Jordan Station has one of the better UTA parking lots and better/quicker freeway access than Draper's	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations	A) Driving	B) Additional parking, L) Increased shade with trees or canopies	
4/2/2025 17:13:57	C) Once a week	Buses and other things are not free for BYU students	C) Travel to my home	B) I don't own a personal vehicle and rely on transit service	A) Increase transit frequency (more buses and/or trains per hour)	G) UTA On Demand		
4/2/2025 17:18:40	D) Less than four times a month	I don't need to	D) Leisure/social (e.g., fun, shopping, dining)	B) I don't own a personal vehicle and rely on transit service, more chill and fun	F) Provide more connections between regional transit services	B) Walking, C) Biking	E) Increased transit connections to South Towne and TRAX, H) Pop up food truck plaza, L) Increased shade with trees or canopies	you should make there be rooms on the train like in Harry Potter
4/2/2025 17:20:57	B) A couple of times a week	D) I already use it frequently	A) Travel to work, C) Travel to my home	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, H) Proximity to my home	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., F) Provide more connections between regional transit services, J) Employer reimbursement program	A) Driving		
4/2/2025 17:22:40	C) Once a week	C) Trains/buses don't come often enough	A) Travel to work	A) Avoid vehicle congestion during commuting hours, G) Proximity to my office, I) Environmental benefit	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., J) Employer reimbursement program	A) Driving, E) Rideshare - Uber, Lyft	C) Potential pedestrian bridge over the freeway, K) Increased directional and wayfinding signage	The lack of clear connection from the local mall to the train station is a barrier. Another barrier is the lack of signage on main road near the station the lack of reasonable connection times from buses away from the South Jordan station. EG neighboring offices are too far for walking from this location yet many bus routes are hourly and are not in sync with the Station meaning you generally wait an hour to catch a connecting bus Creates the need for either paying for rideshare or waiting. This creates people trying to avoid the train station here and find alternative ways to their locations that are more convenient and timely.
4/2/2025 17:43:02	C) Once a week	B) Service hours don't match my schedule	B) Travel to school, C) Travel to my home	B) I don't own a personal vehicle and rely on transit service, I) Environmental benefit	D) Add more weekend service, including Sunday service, J) Employer reimbursement program	A) Driving, F) Bus	E) Increased transit connections to South Towne and TRAX	
4/2/2025 17:48:53	E) Rarely	C) Trains/buses don't come often enough	D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours, B) I don't own a personal vehicle and rely on transit service, F) Proximity to Shopping at South Towne Mall	A) Increase transit frequency (more buses and/or trains per hour), E) Additional amenities and services at the station - restrooms, concessions, lockers etc., F) Provide more connections between regional transit services	A) Driving, E) Rideshare - Uber, Lyft, F) Bus	C) Potential pedestrian bridge over the freeway, E) Increased transit connections to South Towne and TRAX, L) Increased shade with trees or canopies	I find that compared to other stations, this station is isolated and has fewer connections and the wait time is longer. There is no incentive to ride the bus to get to SLCC because the wait is long even though the campus is nearby. It's easier to walk. Getting to South Towne is easier, but there is still a wait time for the bus to leave.
4/2/2025 18:08:25	B) A couple of times a week	B) Service hours don't match my schedule, C) Trains/buses don't come often enough	A) Travel to work	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, D) Economic benefits - reducing my travel costs	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, J) Employer reimbursement program	A) Driving	C) Potential pedestrian bridge over the freeway, F) Increased Jordan Parkway crossing safety and bike lanes, L) Increased shade with trees or canopies	
4/2/2025 18:27:25	A) Daily	D) I already use it frequently	A) Travel to work, C) Travel to my home, D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours, G) Proximity to my office, I) Environmental benefit	E) Additional amenities and services at the station - restrooms, concessions, lockers etc., H) Connection to South Towne Mall	A) Driving	E) Increased transit connections to South Towne and TRAX, L) Increased shade with trees or canopies	
4/2/2025 20:36:30	A) Daily	B) Service hours don't match my schedule, D) I already use it frequently	A) Travel to work, B) Travel to school, C) Travel to my home	A) Avoid vehicle congestion during commuting hours, G) Proximity to my office, H) Proximity to my home	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, F) Provide more connections between regional transit services, J) Employer reimbursement program	A) Driving, C) Biking, D) Scooter, skateboard, or other personal wheeled device, F) Bus, G) UTA On Demand	B) Additional parking, J) Connect the Jordan River Parkway Trail to the FrontRunner Station	
4/2/2025 22:58:24	A) Daily	D) I already use it frequently	A) Travel to work	A) Avoid vehicle congestion during commuting hours	D) Add more weekend service, including Sunday service	G) UTA On Demand	I) Increased access to Jordan River, bird refuge, passive recreation opportunities	I would leave the area west of the station the open land. There is dear that come through
4/3/2025 7:41:34	A) Daily	D) I already use it frequently	A) Travel to work, D) Leisure/social (e.g., fun, shopping, dining), E) Travel for recreation (e.g., Jordan River Trail)	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, I) Environmental benefit	A) Increase transit frequency (more buses and/or trains per hour), F) Provide more connections between regional transit services, J) Employer reimbursement program	A) Driving, C) Biking, F) Bus	A) Expanded employment opportunities, D) Increased residential homeownership opportunities, F) Increased Jordan Parkway crossing safety and bike lanes, L) Increased shade with trees or canopies	The entrance for parking is difficult to find if you don't know where you are going. Some clearer, safe marking, or a dedicated entrance that doesn't interfere with businesses could be helpful. Perhaps a sign along the stoplight pole to direct traffic?
4/3/2025 8:35:10	B) A couple of times a week	D) I already use it frequently	A) Travel to work, C) Travel to my home	A) Avoid vehicle congestion during commuting hours, F) Proximity to Shopping at South Towne Mall, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, E) Additional amenities and services at the station - restrooms, concessions, lockers etc.	A) Driving	D) Increased residential homeownership opportunities, G) Increased open space and public recreation opportunities, H) Pop up food truck plaza, I) Increased access to Jordan River, bird refuge, passive recreation opportunities, J) Connect the Jordan River Parkway Trail to the FrontRunner Station, K) Increased directional and wayfinding signage, L) Increased shade with trees or canopies	Please improve the parking lot coating. I have slipped on the ice many times because in the winter because it freezes and is more slick than regular concrete or asphalt.

Timestamp	1) How often do you ride FrontRunner?	2) What is currently preventing you from riding public transportation more frequently? Choose any that apply.	3) What purpose do you use the South Jordan FrontRunner Station for? Choose any that apply.	4) What do you like best about riding FrontRunner to or from South Jordan? Choose up to three.	5) What would encourage you to ride FrontRunner / Transit more? Pick your top 5.	6) What other means of transportation do you use to get to the station? Choose any that apply.	7) Please select the improvements you think would be most beneficial.	8) What other ideas do you have for this area and where?
4/3/2025 9:15:46	F) Never	B) Service hours don't match my schedule. C) Trains/buses don't come often enough. There's not an easy close bus stop from the train station either to my work	A) Travel to work, G) N/A	A) Avoid vehicle congestion during commuting hours, G) Proximity to my office, H) Proximity to my home	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, E) Additional amenities and services at the station - restrooms, concessions, lockers etc.	A) Driving	B) Additional parking, C) Potential pedestrian bridge over the freeway, D) Increased residential homeownership opportunities, E) Increased transit connections to South Towne and TRAX, J) Connect the Jordan River Parkway Trail to the FrontRunner Station, K) Increased directional and wayfinding signage, L) Increased shade with trees or canopies	Cheaper fare programs for workers and non student people.
4/3/2025 11:19:35	B) A couple of times a week	D) I already use it frequently, I don't feel safe taking it at night.	B) Travel to school	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, C) Make transit routes faster / shorter travel time between destinations, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., I) Improved Safety / Security	A) Driving	B) Additional parking, K) Increased directional and wayfinding signage, L) Increased shade with trees or canopies	
4/3/2025 18:26:41	E) Rarely	More frequently I use Trax as the trains are where I want to go. If I want to go to Ogden I do take the Frontrunner.	D) Leisure/social (e.g., fun, shopping, dining)	C) Improve air quality, I) Environmental benefit, I prefer it to driving if I can take to where I want to go	E) Additional amenities and services at the station - restrooms, concessions, lockers etc., F) Provide more connections between regional transit services	A) Driving	D) Increased residential homeownership opportunities, E) Increased transit connections to South Towne and TRAX, G) Increased open space and public recreation opportunities, J) Connect the Jordan River Parkway Trail to the FrontRunner Station, K) Increased directional and wayfinding signage, L) Increased shade with trees or canopies	Better signage. I don't know where to park. Is the parking structure for Frontrunner? It's not clear. I parked at the SJ station in the open pkg lot that's probably for the office tower.
4/3/2025 19:12:14	B) A couple of times a week	Front runner has been notoriously slow and late way too much. I also work a hybrid schedule so I work remotely some days each week	A) Travel to work, C) Travel to my home	D) Economic benefits - reducing my travel costs	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., I) Improved Safety / Security, J) Employer reimbursement program	A) Driving	B) Additional parking, C) Potential pedestrian bridge over the freeway, G) Increased open space and public recreation opportunities, H) Pop up food truck plaza, K) Increased directional and wayfinding signage	
4/4/2025 13:01:00	E) Rarely	A) Driving my own car, C) Trains/buses don't come often enough	D) Leisure/social (e.g., fun, shopping, dining)	J) N/A	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations	H) N/A	C) Potential pedestrian bridge over the freeway, L) Increased shade with trees or canopies	
4/4/2025 14:10:56	D) Less than four times a mo	C) Trains/buses don't come often enough. Sometime the train is late so it is hard to plan my schedule, and because of so many stops it talks a while to get where I am going.	A) Travel to work, C) Travel to my home	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, I) Improved Safety / Security, J) Employer reimbursement program	A) Driving		
4/4/2025 17:51:07	A) Daily	A) Driving my own car, D) I already use it frequently. Bus connections not in my area (The District in South Jordan)	A) Travel to work, Travel to the airport	A) Avoid vehicle congestion during commuting hours, G) Improve air quality, I) Environmental benefit	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, F) Provide more connections between regional transit services, H) Connection to South Towne Mall, I) Improved Safety / Security	A) Driving	C) Potential pedestrian bridge over the freeway, E) Increased transit connections to South Towne and TRAX, I) Increased access to Jordan River, bird refuge, passive recreation opportunities, L) Increased shade with trees or canopies	
4/4/2025 18:54:04	B) A couple of times a week	C) Trains/buses don't come often enough, D) I already use it frequently	A) Travel to work, B) Travel to school, C) Travel to my home, D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours, B) I don't own a personal vehicle and rely on transit service, H) Proximity to my home	A) Increase transit frequency (more buses and/or trains per hour), D) Add more weekend service, including Sunday service, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., F) Provide more connections between regional transit services, J) Employer reimbursement program	A) Driving, B) Walking, F) Bus	A) Expanded employment opportunities, B) Additional parking, D) Increased residential homeownership opportunities, E) Increased transit connections to South Towne and TRAX, G) Increased open space and public recreation opportunities, J) Connect the Jordan River Parkway Trail to the FrontRunner Station, L) Increased shade with trees or canopies	More bus routes through the South Jordan area. An idea is between Daybreak and the Frontrunner station.
4/5/2025 10:52:45	D) Less than four times a mo	A) Driving my own car, Distance and horrible access to the American Fork Station.	A) Travel to work	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, G) Proximity to my office	C) Make transit routes faster / shorter travel time between destinations	A) Driving, B) Walking, C) Biking	C) Potential pedestrian bridge over the freeway, J) Connect the Jordan River Parkway Trail to the FrontRunner Station	Just work with American Fork to improve the safe and ease of access to the AF station. It is without any sidewalk or bike lane. It is dangerous to ride or walk there.
4/5/2025 14:46:14	F) Never	TRAX is much easier to get to from Daybreak	G) N/A	J) N/A	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, D) Add more weekend service, including Sunday service, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., F) Provide more connections between regional transit services	A) Driving	C) Potential pedestrian bridge over the freeway, E) Increased transit connections to South Towne and TRAX, F) Increased Jordan Parkway crossing safety and bike lanes, H) Pop up food truck plaza, K) Increased directional and wayfinding signage, L) Increased shade with trees or canopies	
4/6/2025 9:35:13	A) Daily	C) Trains/buses don't come often enough, D) I already use it frequently	B) Travel to school, D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, D) Economic benefits - reducing my travel costs	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, F) Provide more connections between regional transit services	A) Driving	E) Increased transit connections to South Towne and TRAX, J) Connect the Jordan River Parkway Trail to the FrontRunner Station, L) Increased shade with trees or canopies	
4/6/2025 10:32:08	B) A couple of times a week	D) I already use it frequently	A) Travel to work	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), I) Improved Safety / Security, J) Employer reimbursement program	A) Driving, B) Walking	C) Potential pedestrian bridge over the freeway, L) Increased shade with trees or canopies	
4/7/2025 5:50:38	E) Rarely	Hard to come by public transportation	A) Travel to work	B) I don't own a personal vehicle and rely on transit service, D) Economic benefits - reducing my travel costs, I) Environmental benefit	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, C) Make transit routes faster / shorter travel time between destinations, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., I) Improved Safety / Security	E) Rideshare - Uber, Lyft	C) Potential pedestrian bridge over the freeway, E) Increased transit connections to South Towne and TRAX, G) Increased open space and public recreation opportunities, J) Connect the Jordan River Parkway Trail to the FrontRunner Station	
4/7/2025 7:14:35	E) Rarely	A) Driving my own car, I live in Eagle Mountain and the traffic from the Lehi station to my house doesn't reduce my travel time	A) Travel to work	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs	C) Make transit routes faster / shorter travel time between destinations	A) Driving		add a better way to get from Eagle Mountain to Lehi or AF station
4/7/2025 7:30:04	F) Never	A) Driving my own car, B) Service hours don't match my schedule, C) Trains/buses don't come often enough	G) N/A	J) N/A	A) Increase transit frequency (more buses and/or trains per hour), F) Provide more connections between regional transit services	H) N/A		
4/7/2025 7:38:41	E) Rarely	A) Driving my own car	D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours	D) Add more weekend service, including Sunday service	A) Driving	E) Increased transit connections to South Towne and TRAX, H) Pop up food truck plaza	

Timestamp	1) How often do you ride FrontRunner?	2) What is currently preventing you from riding public transportation more frequently? Choose any that apply.	3) What purpose do you use the South Jordan FrontRunner Station for? Choose any that apply.	4) What do you like best about riding FrontRunner to or from South Jordan? Choose up to three.	5) What would encourage you to ride FrontRunner / Transit more? Pick your top 5.	6) What other means of transportation do you use to get to the station? Choose any that apply.	7) Please select the improvements you think would be most beneficial.	8) What other ideas do you have for this area and where?
4/7/2025 7:53:26	E) Rarely	A) Driving my own car, C) Trains/buses don't come often enough	A) Travel to work, D) Leisure/social (e.g., fun, shopping, dining)	G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), F) Provide more connections between regional transit services	A) Driving	C) Potential pedestrian bridge over the freeway, E) Increased transit connections to South Towne and TRAX, G) Increased open space and public recreation opportunities, H) Pop up food truck plaza, I) Increased access to Jordan River, bird refuge, passive recreation opportunities, L) Increased shade with trees or canopies	
4/7/2025 8:14:01	F) Never	The connection from daybreak to the south Jordan front runner is not in the area covered by my pass. I connects in Murray and my pass does not work at this location. The UTA app says I need to connect via 2 busses and does not give the front runner option	A) Travel to work	A) Avoid vehicle congestion during commuting hours	F) Provide more connections between regional transit services	A) Driving	E) Increased transit connections to South Towne and TRAX, J) Connect the Jordan River Parkway Trail to the FrontRunner Station	Allow daybreak to south Jordan front runner with eco pass
4/7/2025 8:25:25	F) Never	A) Driving my own car, It's hard to get to a station. It's slower than driving my own car. There are sketchy people.	G) N/A	J) N/A	I) Improved Safety / Security	H) N/A		
4/7/2025 8:29:29	C) Once a week	I no longer live near a Front Runner Station. I used to use it once a week.	A) Travel to work	A) Avoid vehicle congestion during commuting hours, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations	A) Driving	C) Potential pedestrian bridge over the freeway, D) Increased residential homeownership opportunities	I would like to suggest improvements to enhance the cleanliness, comfort, and overall experience of the Front Runners. First, providing disinfecting wipe stations on each level of the train would allow passengers to clean their area before use, promoting a cleaner environment. Additionally, adding more tables would greatly benefit working professionals commuting to and from work. I recommend installing tables at all seats on the top level to better accommodate these needs. The afternoon sun can also be quite blinding, so adding window shades would significantly improve comfort. Another idea is to introduce designated quiet zones for passengers who prefer a more peaceful environment during their commute. This would allow those who need to work, relax, or focus to do so without disturbances.
4/7/2025 8:34:52	A) Daily	C) Trains/buses don't come often enough	A) Travel to work, C) Travel to my home, D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, I) Improved Safety / Security, J) Employer reimbursement program	A) Driving, B) Walking	A) Expanded employment opportunities, H) Pop up food truck plaza, L) Increased shade with trees or canopies	
4/7/2025 8:52:55	E) Rarely	C) Trains/buses don't come often enough	D) Leisure/social (e.g., fun, shopping, dining), E) Travel for recreation (e.g., Jordan River Trail)	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, H) Proximity to my home	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., F) Provide more connections between regional transit services, J) Employer reimbursement program	A) Driving, E) Rideshare - Uber, Lyft	E) Increased transit connections to South Towne and TRAX	if the train was as fast or almost as fast I would ride. I do not like adding hours to my day commuting.
4/7/2025 9:13:33	F) Never	It is inconvenient. I live in Payson, Utah.	G) N/A	A) Avoid vehicle congestion during commuting hours	F) Provide more connections between regional transit services	A) Driving, B) Walking	C) Potential pedestrian bridge over the freeway, L) Increased shade with trees or canopies	
4/7/2025 9:17:16	E) Rarely	A) Driving my own car	A) Travel to work	C) Improve air quality, G) Proximity to my office, I) Environmental benefit	F) Provide more connections between regional transit services, J) Employer reimbursement program	A) Driving, B) Walking, F) Bus	E) Increased transit connections to South Towne and TRAX, H) Pop up food truck plaza, J) Connect the Jordan River Parkway Trail to the FrontRunner Station, K) Increased directional and wayfinding signage, L) Increased shade with trees or canopies	
4/7/2025 9:59:00	E) Rarely	A) Driving my own car	D) Leisure/social (e.g., fun, shopping, dining)	I) Environmental benefit	E) Additional amenities and services at the station - restrooms, concessions, lockers etc., I) Improved Safety / Security	A) Driving	K) Increased directional and wayfinding signage, L) Increased shade with trees or canopies	
4/7/2025 10:25:45	A) Daily	D) I already use it frequently	A) Travel to work, C) Travel to my home	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, J) Employer reimbursement program	A) Driving	A) Expanded employment opportunities, D) Increased residential homeownership opportunities, L) Increased shade with trees or canopies	
4/7/2025 10:39:58	F) Never	A) Driving my own car, Location compared to where I live and work is not workable for me	G) N/A	A) Avoid vehicle congestion during commuting hours	H) Connection to South Towne Mall, J) Employer reimbursement program	A) Driving	C) Potential pedestrian bridge over the freeway, E) Increased transit connections to South Towne and TRAX, I) Increased access to Jordan River, bird refuge, passive recreation opportunities, J) Connect the Jordan River Parkway Trail to the FrontRunner Station, L) Increased shade with trees or canopies	
4/7/2025 11:46:09	D) Less than four times a month	I used to ride frequently, but recently changed jobs and can now walk to work	D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, D) Economic benefits - reducing my travel costs	A) Increase transit frequency (more buses and/or trains per hour), D) Add more weekend service, including Sunday service, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., J) Employer reimbursement program	B) Walking	D) Increased residential homeownership opportunities, E) Increased transit connections to South Towne and TRAX, F) Increased Jordan Parkway crossing safety and bike lanes, J) Connect the Jordan River Parkway Trail to the FrontRunner Station	I used to ride the FrontRunner regularly for my old job in SLC. It was fantastic. However, I ran into issues when I tried to get anywhere in South Jordan. Not a ton of bus options out into the city toward Daybreak. I also love the idea of connecting the FrontRunner station to the Jordan River trail. I do see how you could connect via existing roadways, but I would be curious what connections UTA could make!
4/7/2025 13:25:10	A) Daily	D) I already use it frequently	A) Travel to work, C) Travel to my home	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, C) Make transit routes faster / shorter travel time between destinations, H) Connection to South Towne Mall, J) Employer reimbursement program	A) Driving, B) Walking, C) Biking	C) Potential pedestrian bridge over the freeway, L) Increased shade with trees or canopies	
4/7/2025 14:02:21	A) Daily	B) Service hours don't match my schedule, C) Trains/buses don't come often enough	A) Travel to work	D) Economic benefits - reducing my travel costs, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, I) Improved Safety / Security	A) Driving, B) Walking		The frontrunner only comes once every hour in the early afternoon and the cars are crowded which makes it very hard to ride in the afternoon.
4/7/2025 15:48:30	E) Rarely	C) Trains/buses don't come often enough	D) Leisure/social (e.g., fun, shopping, dining), F) General errands or appointments (e.g., medical visit, grocery shopping)	C) Improve air quality, D) Economic benefits - reducing my travel costs, H) Proximity to my home	D) Add more weekend service, including Sunday service, F) Provide more connections between regional transit services	A) Driving	C) Potential pedestrian bridge over the freeway, E) Increased transit connections to South Towne and TRAX, H) Pop up food truck plaza, I) Increased access to Jordan River, bird refuge, passive recreation opportunities, L) Increased shade with trees or canopies	
4/7/2025 16:57:05	D) Less than four times a month	A) Driving my own car, C) Trains/buses don't come often enough	A) Travel to work	A) Avoid vehicle congestion during commuting hours, G) Proximity to my office, I) Environmental benefit	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, J) Employer reimbursement program	A) Driving	C) Potential pedestrian bridge over the freeway, L) Increased shade with trees or canopies	
4/7/2025 22:57:33	B) A couple of times a week	C) Trains/buses don't come often enough, D) I already use it frequently	C) Travel to my home, D) Leisure/social (e.g., fun, shopping, dining), F) General errands or appointments (e.g., medical visit, grocery shopping)	A) Avoid vehicle congestion during commuting hours, B) I don't own a personal vehicle and rely on transit service, D) Economic benefits - reducing my travel costs	B) Add later evening service or earlier morning service, D) Add more weekend service, including Sunday service, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., F) Provide more connections between regional transit services, I) Improved Safety / Security	A) Driving, B) Walking, F) Bus	A) Expanded employment opportunities, B) Additional parking, C) Potential pedestrian bridge over the freeway, D) Increased open space and public recreation opportunities, L) Increased shade with trees or canopies	Close spaces to cover when it's winter, older people we can't be to exposed to coldness

Timestamp	1) How often do you ride FrontRunner?	2) What is currently preventing you from riding public transportation more frequently? Choose any that apply.	3) What purpose do you use the South Jordan FrontRunner Station for? Choose any that apply.	4) What do you like best about riding FrontRunner to or from South Jordan? Choose up to three.	5) What would encourage you to ride FrontRunner / Transit more? Pick your top 5.	6) What other means of transportation do you use to get to the station? Choose any that apply.	7) Please select the improvements you think would be most beneficial.	8) What other ideas do you have for this area and where?
4/8/2025 9:04:21	F) Never	A) Driving my own car. From Riverton I will need to drive 10 minutes to the Red Line, ride 27 minutes to Murray Central, wait for a Front Runner train to take me south to South Jordan. In order to get to work by 7am, I would need to leave my house at 5:50am. If I drive the commute takes 25 minutes maximum.	G) N/A	J) N/A	F) Provide more connections between regional transit services, J) Employer reimbursement program	A) Driving	E) Increased transit connections to South Towne and TRAX, I) Increased access to Jordan River, bird refuge, passive recreation opportunities	
4/8/2025 12:44:29	E) Rarely	B) Service hours don't match my schedule, the work stop (10600 s) is convenient. The home stop (Eagle Mountain City Center) is not	D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours, G) Proximity to my office	B) Add later evening service or earlier morning service, C) Make transit routes faster / shorter travel time between destinations, F) Provide more connections between regional transit services	A) Driving	C) Potential pedestrian bridge over the freeway	Would use a TRAX or FrontRunner option if in Eagle Mountain City Center
4/8/2025 13:04:32	E) Rarely	Ticket prices are more expensive than fuel costs to drive into work.	A) Travel to work, C) Travel to my home, D) Leisure/social (e.g., fun, shopping, dining)	A) Avoid vehicle congestion during commuting hours, It is currently a great service if/when it becomes financially beneficial.	J) Employer reimbursement program	A) Driving		If your service costs the end user less money than for them to drive, they will use it. If your costs are the same or more expensive (as they currently are) people will continue to drive. Where is the benefit?
4/9/2025 7:25:09	C) Once a week	C) Trains/buses don't come often enough	A) Travel to work	C) Improve air quality, G) Proximity to my office, I) Environmental benefit	A) Increase transit frequency (more buses and/or trains per hour), E) Additional amenities and services at the station - restrooms, concessions, lockers etc., J) Employer reimbursement program	A) Driving, B) Walking, F) Bus	C) Potential pedestrian bridge over the freeway, D) Increased residential homeownership opportunities, H) Pop up food truck plaza	
4/9/2025 9:15:03	E) Rarely	A) Driving my own car	D) Leisure/social (e.g., fun, shopping, dining)	J) N/A	D) Add more weekend service, including Sunday service, F) Provide more connections between regional transit services	A) Driving	E) Increased transit connections to South Towne and TRAX, F) Increased Jordan Parkway crossing safety and bike lanes, J) Connect the Jordan River Parkway Trail to the FrontRunner Station	I live in Daybreak. If there were a way to get from Daybreak to this station it would make using a car something I would never have to worry about. Creating a TRAX connection to this station would be amazing and decrease traffic from Daybreak to this station.
4/9/2025 11:17:27	E) Rarely	C) Trains/buses don't come often enough	A) Travel to work	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, F) Provide more connections between regional transit services	A) Driving, C) Biking, F) Bus	E) Increased transit connections to South Towne and TRAX, F) Increased Jordan Parkway crossing safety and bike lanes	Need more ways to get to the front runner stations from other areas of the city like sugar house, Millicreek, Olympus cove
4/9/2025 12:28:33	A) Daily	D) I already use it frequently	A) Travel to work	A) Avoid vehicle congestion during commuting hours, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, D) Add more weekend service, including Sunday service, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., J) Employer reimbursement program	C) Biking	C) Potential pedestrian bridge over the freeway, L) Increased shade with trees or canopies	Increased frequency
4/9/2025 21:47:31	B) A couple of times a week	A) Driving my own car, C) Trains/buses don't come often enough. It is cheaper to drive my family than purchase individual tickets for even 1-2 of them on top of my own ticket.	A) Travel to work, C) Travel to my home, E) Travel for recreation (e.g. Jordan River Trail)	A) Avoid vehicle congestion during commuting hours, E) Proximity to Jordan River Trail, I can save time to be with my family sooner by working on the train with Wi-Fi and tables on board.	A) Increase transit frequency (more buses and/or trains per hour), C) Make transit routes faster / shorter travel time between destinations, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., F) Provide more connections between regional transit services, J) Employer reimbursement program	A) Driving, B) Walking, C) Biking, Friend	B) Additional parking, E) Increased transit connections to South Towne and TRAX, F) Increased Jordan Parkway crossing safety and bike lanes, G) Increased open space and public recreation opportunities, H) Pop up food truck plaza, I) Increased access to Jordan River, bird refuge, passive recreation opportunities, J) Connect the Jordan River Parkway Trail to the FrontRunner Station, K) Increased directional and wayfinding signage, L) Increased shade with trees or canopies	Tiered pay options to encourage more family travel.
4/10/2025 14:38:46	E) Rarely	A) Driving my own car, B) Service hours don't match my schedule, C) Trains/buses don't come often enough. Not as many routes needed for me to get within comfortable walking distance of my work or home.	A) Travel to work, C) Travel to my home, D) Leisure/social (e.g., fun, shopping, dining), E) Travel for recreation (e.g. Jordan River Trail), To the airport	A) Avoid vehicle congestion during commuting hours, C) Improve air quality, I) Environmental benefit	B) Add later evening service or earlier morning service, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., F) Provide more connections between regional transit services, I) Improved Safety / Security, J) Employer reimbursement program	A) Driving, B) Walking, C) Biking	C) Potential pedestrian bridge over the freeway, F) Increased Jordan Parkway crossing safety and bike lanes, L) Increased shade with trees or canopies	I am all for public transit, I wish we had more options available!!!! I would use it everyday if there were more stops and more frequent trains. Thank you for having this service.
4/11/2025 7:36:01	B) A couple of times a week	C) Trains/buses don't come often enough	A) Travel to work	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, H) Proximity to my home	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, H) Connection to South Towne Mall	A) Driving, D) Scooter, skateboard, or other personal wheeled device, E) Rideshare - Uber, Lyft, F) Bus	C) Potential pedestrian bridge over the freeway, L) Increased shade with trees or canopies	Make restroom available to public. Reduce the scheduled time between transfers. (I normally have to wait 20 to 25 minutes after my bus arrives for the train to arrive [and visa versa]. That's a lot of extra commute time per day when you do it both ways).
4/14/2025 13:57:24	C) Once a week	B) Service hours don't match my schedule, C) Trains/buses don't come often enough	A) Travel to work, B) Travel to school, Work and drop off kids tip school downtown they go to Catholic school and the school bus picks them up from north temple	A) Avoid vehicle congestion during commuting hours, D) Economic benefits - reducing my travel costs, G) Proximity to my office	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, C) Make transit routes faster / shorter travel time between destinations, E) Additional amenities and services at the station - restrooms, concessions, lockers etc., F) Provide more connections between regional transit services	A) Driving	D) Increased residential homeownership opportunities, E) Increased transit connections to South Towne and TRAX, F) Increased Jordan Parkway crossing safety and bike lanes, H) Pop up food truck plaza, I) Increased access to Jordan River, bird refuge, passive recreation opportunities, J) Connect the Jordan River Parkway Trail to the FrontRunner Station, L) Increased shade with trees or canopies	Heat lamps for the cold winter days
4/18/2025 7:47:20	D) Less than four times a mo	B) Service hours don't match my schedule, C) Trains/buses don't come often enough	C) Travel to my home	G) Proximity to my office, I don't like much about Front Runner	A) Increase transit frequency (more buses and/or trains per hour), B) Add later evening service or earlier morning service, J) Employer reimbursement program	B) Walking	L) Increased shade with trees or canopies	

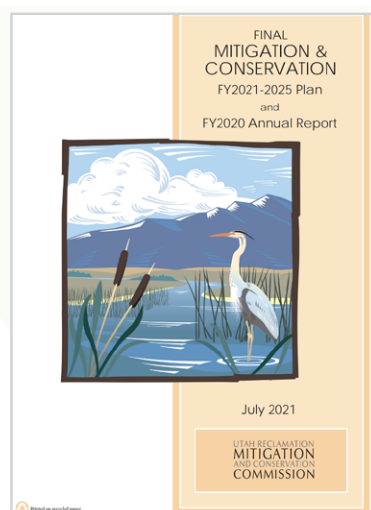
| Appendix C – Previous Plans and Studies

C. Previous Plans and Studies

PREVIOUS PLANS AND STUDIES	
Study Name	Date Completed
WFRC 2023 The Point-Area West Transit Fresh Look Study	In Progress
UTA 2025–2029 Five-Year Service Plan	2025
Moderate-Income Housing Plan	2025
South Jordan Transportation Master Plan	2024
Wasatch Front Regional Council Transportation Improvement Program	2024
Wasatch Choice Vision	2024
Sandy Station Area Plan - South Jordan Frontrunner	2024
UTA Moves 2050 Long-Range Transit Plan	2023
WFRC Regional Transportation Plan (RTP)	2023
WFRC Access to Opportunities	2023
WFRC SAP Guidance	2022
Blueprint Jordan River	2022
SLCo West General Plan	2021
Southwest Salt Lake County Transportation Analysis and Solutions Development	2021
South Jordan Sub Area Plan - Jordan River Gateway	2021
Utah Reclamation Mitigation and Conservation Commission	2021
South Jordan City General Plan	2020
Housing Needs Study	2014
South Jordan Gateway Study	2013
Jordan River Corridor Open Space and Habitat Conservation Master Plan and Management Guidelines	2011
South Jordan Economic Strategic Plan	2011

Utah Reclamation Mitigation and Conservation Commission

Final Mitigation & Conservation Plan July 2021 is a federal program for restoring, protecting and conserving fish, wildlife and related recreation resources in Utah that were impacted by federal water development projects authorized under the Reclamation Act of 1902 particularly the Central Utah Project's Bonneville Unit.



This Final Mitigation & Conservation Plan dated July 2021 summarizes the history of the Commission's activities and outlines the future plans of the parcels they manage.

In 1997 the 44-acre parcel on the north side of 10600 South in South Jordan was acquired by the Commission. In 1999, a second parcel (about 17 acres) was acquired by the Commission on the south side of 10600 South in South Jordan. In 2000, a conservation easement was donated to the Commission on about 35 acres along the Jordan River and adjacent to the second parcel. In 2001, about 17 acres were purchased along the Jordan River between 9800 South and 10000 South. These parcels link with other undeveloped parcels. In 2008, the Commission issued a license agreement to Sandy City to construct an access road to the city's park and fishing pond and other components of the Jordan River Parkway in Sandy City. The Commission donated the underlying fee plus adjacent acreage of approximately 3.8 acres to Sandy City for their use and maintenance as open space in 2018.

The Commission will continue property management while entering discussions with several parties with the intent of identifying a suitable entity to transfer the federally owned property for ownership and management. The Commission will continue providing support to the Jordan River Commission and Technical Advisory Committee.

Sandy Station Area Plan - South Jordan Frontrunner (2024)

Sandy's portion of the South Jordan Frontrunner Station Area is a key redevelopment zone east of I-15, serving as the southern gateway to The Cairns, Sandy's downtown district. Anchored by the Sandy Promenade and South Towne Mall site, the area is positioned for significant transformation with a mix of four- to eight-story commercial and residential buildings and multimodal connectivity enhancements. At full build-out, this area is projected to add 1,100-2,800 new housing units.

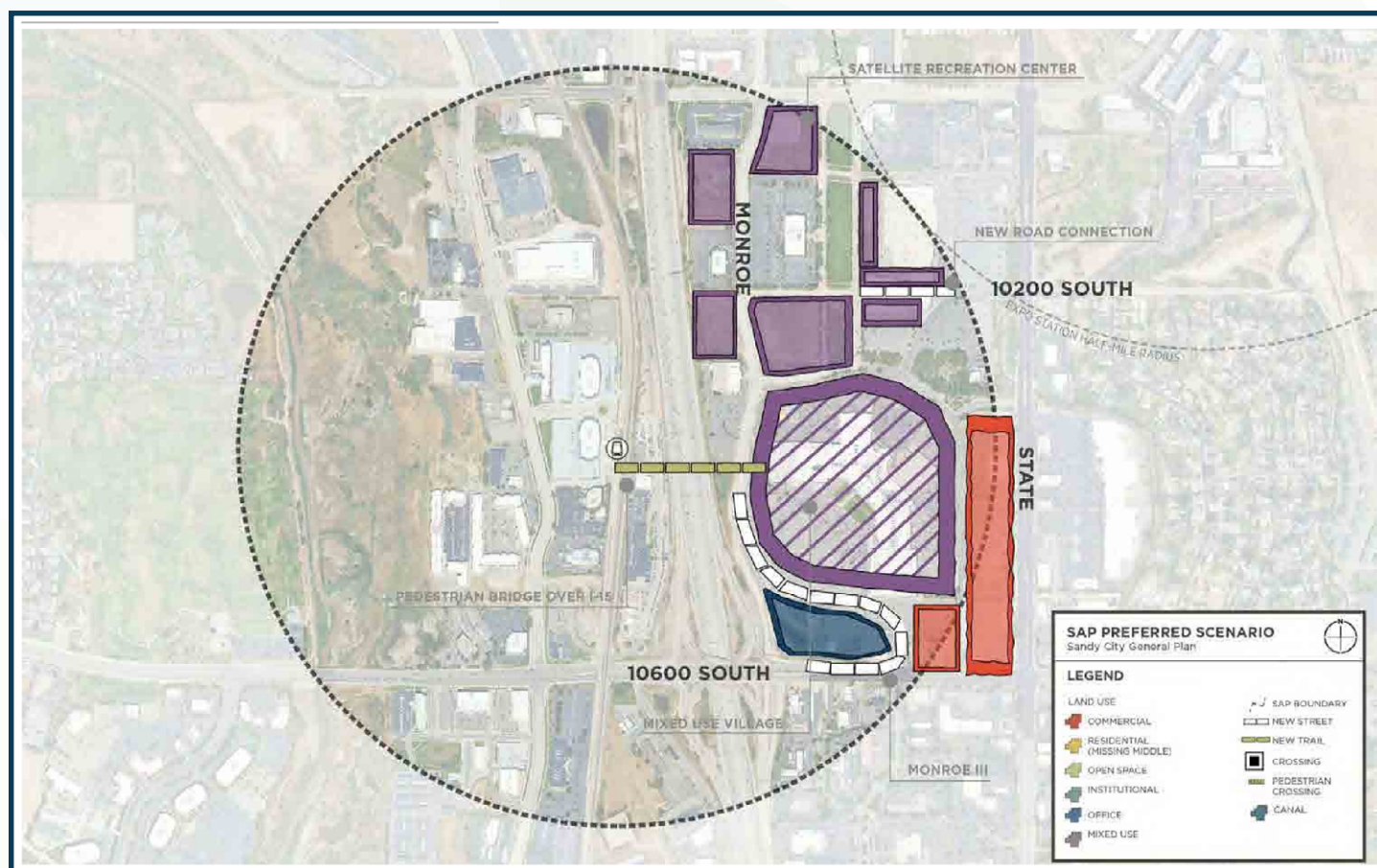
Notable Development Projects:

- ◆ South Towne Mall site redevelopment - Future home of the Utah NHL team's practice facility and new community-focused amenities - expected completion date in late 2025

- ◆ Mixed-use developments with preliminary approvals - Red Sky, Summit, and Shulsen

Key Recommendations from the plan:

- ◆ Develop a pedestrian-cyclist bridge over I-15 and Monroe Street near 10200 South to enhance connectivity
- ◆ Explore funding sources (grants, general funds, UDOT, WFRC) for pedestrian bridge feasibility studies
- ◆ Streamline UTA transit service between South Jordan Station and Civic Center TRAX to improve regional mobility
- ◆ Establish an HTRZ over a portion of South Town's west end for residential uses to meet the HTRZ State Statute requirements ($\frac{1}{3}$ mile radius)



South Jordan Gateway Study (2013)

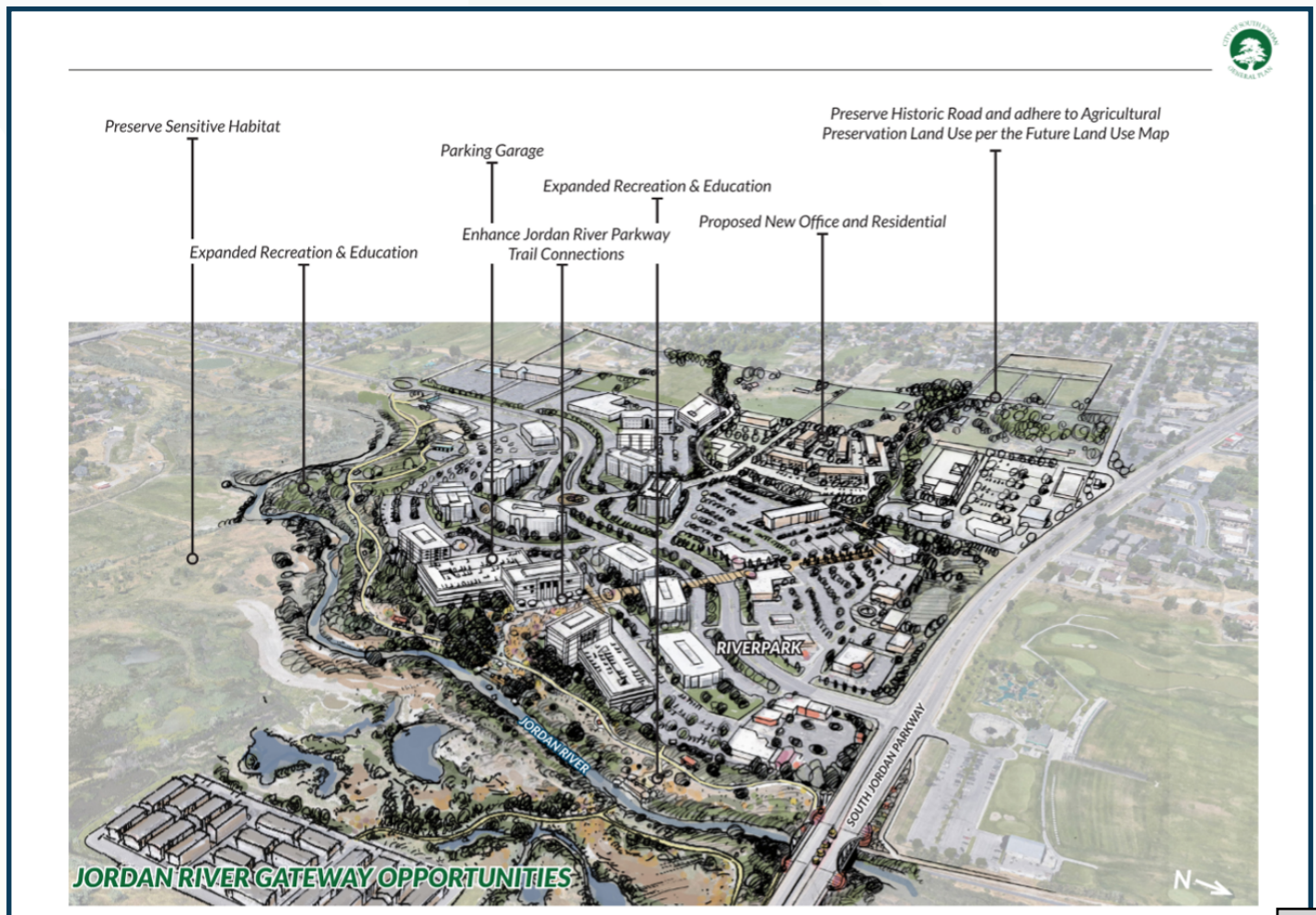
The South Jordan Gateway Study assesses transportation, land use, and future development opportunities within the Gateway district—the area centered around the FrontRunner Station. The assessment identifies the gateway district as an area with significant untapped economic potential and social value. The framework outlined in the study aims to:

- ◆ Unlock economic value by attracting high-wage employers and expanding commercial offerings
- ◆ Lead in developing state of the art mixed use projects that serve South Jordan's growth and identity
- ◆ Incentivize market-driven transit-oriented land use and development
- ◆ Work with Sandy to establish a Joint Circulator Bus Route linking Sandy Civic TRAX station and South Jordan FrontRunner Station
- ◆ Assess the future of the Mulligans Golf and Games site and establish three scenarios

South Jordan Sub Area Plan - Jordan River Gateway (2021)

Building off of the 2013 South Jordan Gateway Study, the Sub Area Plan, Jordan River Gateway, provides additional visioning for design and land use scenarios and establishes specific goals and policies. The Jordan River Gateway is a key entry point into South Jordan, located adjacent to the Frontrunner Station and serves as a critical regional connection via I-15, the Jordan River Parkway Trail, and commuter rail. The plan aims to align with the Blueprint Jordan River Plan, emphasizing a balanced approach to urban growth and ecological preservation of the river.

Vision: “The Jordan Gateway is where South Jordan’s rural heritage and urban lifestyle meet. with public transit, freeway access, and regional trail connections, this area is accessible for all, making it a City-wide gathering destination. This area is a careful balance of natural open space preservation and economic amenities that highlights our unique blend of land use.”



Key Recommendations

- ◆ Mixed-use development encouraged west of the River Park development
- ◆ Office and retail expansion in River Park development
- ◆ Conserve Jordan River open spaces, ensuring access to recreation while protecting natural habitat
- ◆ Enhance connectivity between River Park development and South Jordan station through public transit and active transportation improvements
- ◆ Promote overlapping land uses near the Jordan River to extend visitor activity throughout the day

Blueprint Jordan River

The Blueprint Jordan River Plan highlights a regional vision for a continuous, 50-mile “blue-green” corridor connecting Utah Lake to the Great Salt Lake. The Plan emphasizes ecological restoration, access to recreation, and deepening community integration along the river’s corridor. In South Jordan, the Plan highlights opportunities for enhancing trail connectivity, restoring the riparian habitat and improving public access to the river corridor in a responsible manner. The South Jordan FrontRunner Station is a key amenity in drawing recreational users to the Jordan River Trail for day-long adventures. These opportunities make the Blueprint Jordan River Plan a useful document for integrating ecological and recreational goals with the transit-oriented development around South Jordan FrontRunner Station.

Key Recommendations

- ◆ Optimize habitats and natural ecosystems
- ◆ Use a coordinated watershed approach to improve water quality
- ◆ Manage river flows for natural river function
- ◆ Enhance the hydrological function of the river
- ◆ Make the Parkway a welcoming place for visitors of all abilities, backgrounds, and circumstances
- ◆ Mitigate homelessness in the Jordan River Parkway
- ◆ Enhance opportunities for recreational uses and active transportation throughout the Parkway
- ◆ Improve access to the Parkway and enhance the Parkway’s role as a regional transportation corridor
- ◆ Ensure that development highlights the Jordan River and enhances the Parkway

WFRC SAP Guidance

The Wasatch Front Regional Council Station Area Plan Certification and Technical Assistance website projects information as to what an SAP is meant to accomplish. An SAP is intended to promote shared objectives such as housing availability and affordability, access to opportunities, sustainable environmental conditions, and transportation choices and connections. As a result of the 2022 state Legislative session, HB462 (titled Housing Affordability Amendments, a summary of which is located here) passed and was signed into law. It requires a city that has a fixed-guideway public transit station (rail or bus rapid transit) to develop an SAP for that station and update its general plan and zoning to implement the SAP. HB462 also requires that SAPs promote the following principles:

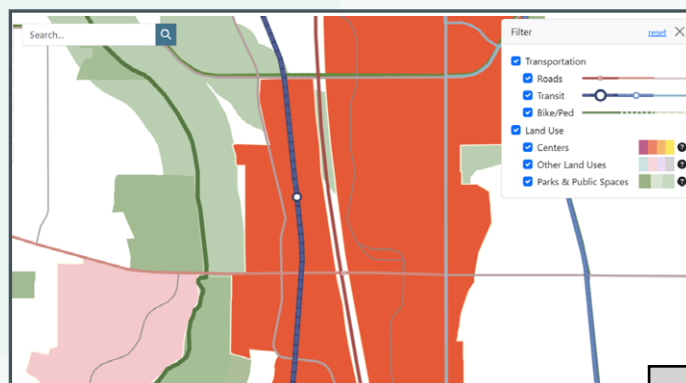
- Increase the availability and affordability of housing, including moderate-income housing
- Promote sustainable environmental conditions
- Enhance access to opportunities
- Increase transportation choices and connections

Wasatch Choice Vision

The Wasatch Choice Vision illustrates a voluntarily implemented regional vision of how changes could occur between now and 2050. Its goal is to support the economy, improve air quality, and enhance the quality of life for Utahns living across the Wasatch Front.

This vision was developed by WFRCI, Utah Department of Transportation (UDOT), Utah Transit Authority (UTA), Utah League of Cities and Towns, Utah Association of Counties, Governor’s Office of Planning & Budget, Kem C. Gardner Policy Institute and the Salt Lake Chamber & Local Chambers of Commerce.

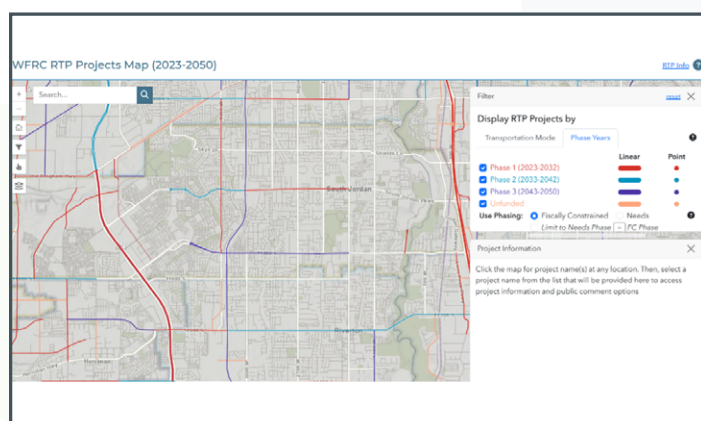
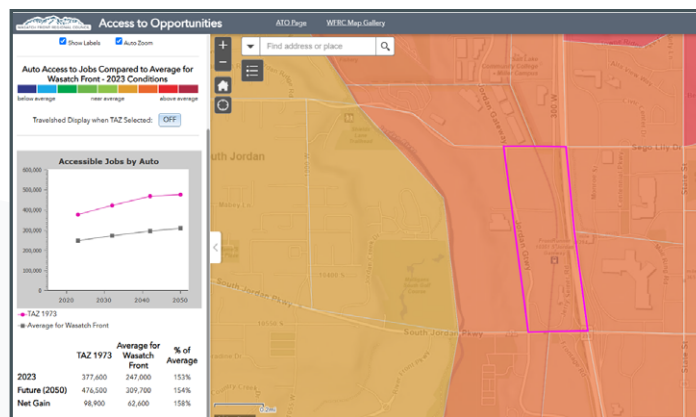
The South Jordan Station Area FrontRunner planning area is designated by the Wasatch Choice Vision as an urban center with significant commerce, a regional destination retail center with the intersection of major freeways and arterials along and a junction point of high capacity transit network.



WFRC Access to Opportunities

The Access to Opportunity (ATO) score for the SAP is calculated at 73 out of 120 for South Jordan City. Access to jobs and community necessities were the weakest elements in this score. Access to Opportunities is a way to measure how well people can connect to basic needs and amenities including jobs, schools, grocery and other retail, parks, community centers, recreation, and entertainment.

This GIS-based tool can be utilized to understand future access opportunities for the project area, and help identify weak points for different destinations (jobs, households) and by different modes (auto, transit).



WFRC Regional Transportation Plan (RTP)

The Regional Transportation Plan (RTP) sets the regional long-term strategy for Salt Lake County's future transportation system. The 2023–2050 RTP outlines investments for all modes of transportation and forms the transportation element of the Wasatch Choice Vision.

Utah is experiencing rapid growth which amplifies the weight of the decisions we make now and over the next few decades. The RTP addresses growth patterns and the transportation of people and goods which impact growth-related issues such as poor air quality, high costs of living, and congested roadways. The RTP looks to the future to consider how transportation infrastructure can work with land and economic development decisions to maximize overall quality of life.



UTA 2025–2029 Five-Year Service Plan

The 2025-2029 Five-Year Service Plan is a dynamic guide for UTA’s near-term future. Our region is growing rapidly, and where and how we grow affects the transportation network. This vision for the future of UTA works to address these effects through improvements to the transit system. This plan, like a route map, is a snapshot in time. It reflects UTA’s intended service based on the best information available. This plan is updated every two years and considers both the UTA Long-Range Transit Plan as well as other regional and local transportation plans in its development.

UTA Moves 2050 Long-Range Transit Plan

Where and how we grow affects the transportation network. UTA has developed a Long-Range Transit Plan for the next 30 years as a vision for the future of public transportation. This plan, “UTA Moves 2050”, focuses on understanding and responding to the needs of the community we serve today, tomorrow, and beyond.



Wasatch Front Regional Council Transportation Improvement Program

WFRC develops and manages the Transportation Improvement Program (TIP). The TIP is a six-year program of roadway, transit, and active transportation projects for the Salt Lake-West Valley and the Ogden-Layton urbanized areas. It is updated once a year and provides a carefully reviewed prioritization of roadway, transit, bicycle, and pedestrian projects in the region, including operation and maintenance of the existing transportation system. The TIP is developed in close collaboration with UDOT, UTA, and cities and counties along the Wasatch Front and aligns with the Wasatch Choice Vision.

Southwest Salt Lake County Transportation Analysis and Solutions Development

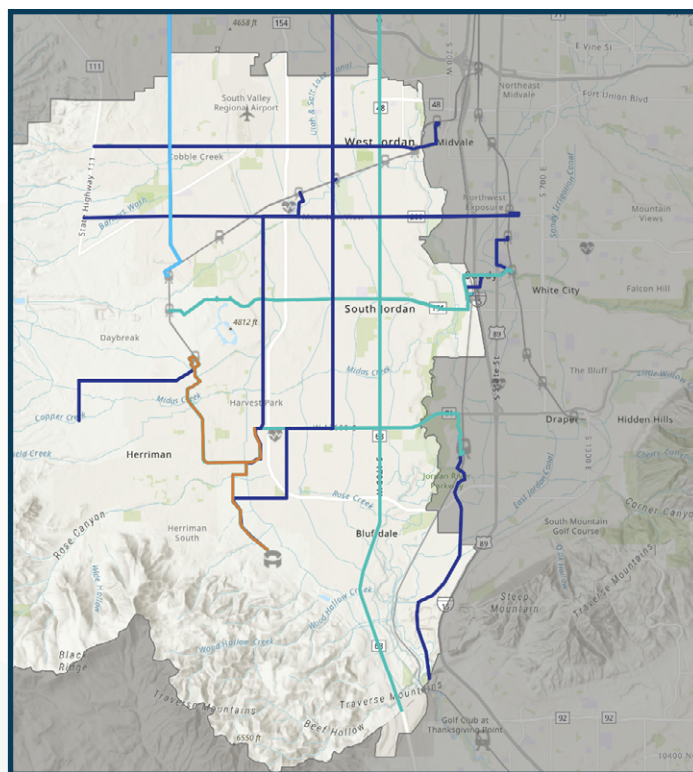
The purpose of this 2021 study was to identify solutions to mobility concerns in southwest Salt Lake County, focusing on east-west mobility. It explores short-, mid-, and long-term solutions that improve mobility. Partners included Bluffdale, Copperton, Herriman, Riverton, South Jordan, West Jordan, representatives from Salt Lake County, and three partner agencies: UTA, UDOT, and WFRC.

WFRC 2023 The Point-Area West Transit Fresh Look Study

The Point-Area West (Southwest Salt Lake County and Northwest Utah County) is one of the fastest-growing areas in the United States. Local leaders are interested in the degree to which transportation plans and current construction are meeting the needs of this growth. The study's hypothesis: With all the growth and land use changes, what could be the future of transit in this area? The study is in progress now and expected to deliver recommendations in 2025.

South Jordan City General Plan

The South Jordan General Plan serves as a guide for future growth and investment while preserving the City's high quality of life and unique character. The 2020 South Jordan General Plan includes existing conditions, a vision statement, framework maps, and goals and strategies for South Jordan as it continues to develop.



South Jordan Economic Strategic Plan

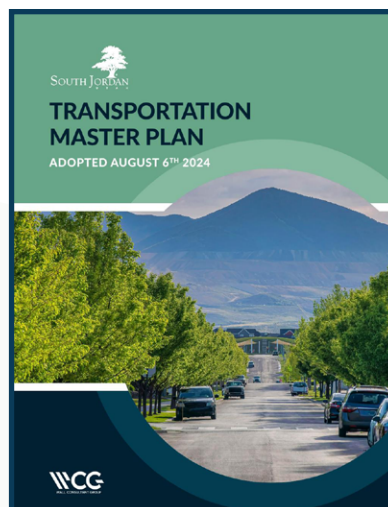
The South Jordan Economic Development Strategic Plan was prepared in November of 2011. The plan provides objectives and implementation strategies for seven focus areas including:

- ◆ Promote Transportation Improvements.
- ◆ Foster a Regional Retail Perspective.
- ◆ Advance Neighborhood/Community Retail to Capture Incremental Growth.
- ◆ Encourage Specialized Commercial Development.
- ◆ Support the Development of Highway Commercial and Transit Oriented Development.
- ◆ Expand Upon the Existing Class A Office Strengths.
- ◆ Build Economic Diversity through Premier Research and Business Parks.

[Jordan River Corridor Open Space and Habitat Conservation Master Plan and Management Guidelines](#)

The Jordan River Corridor Open Space and Habitat Conservation Master Plan and Management Guidelines document, completed in 2011, outlines the active river channel, the 100 year floodplain, and the 500 year floodplain. A significant portion of the FrontRunner Station area is affected by these designations. As such the plan establishes “instructions for the long-term preservation and management of open space and natural areas.” A portion of the station area is shown as a conservation area which protects against the natural course of the river and avoids flood damage here and at other locations along the river. Several zoning ordinances are recommended to regulate specific sections of the river, and are outlined as follows:

1. “Natural Area Open Space” Zoning and Ordinance which “preserves and protects natural areas which are of community, regional, federal, and international importance.”
2. “Public Safety Federal Emergency Management Agency (FEMA) 100- and 500-year Floodplain Zone No-Build” Ordinance which protects against major flooding and costly damage while preserving the ecological functions and habitat areas
3. “Stream Setback No-Build” Zoning and Ordinance establishes measures and best management practices consistent with major federal, state, and other local codes and “improve[s] stream health and water quality, preserve[s] riparian buffers and public safety floodplain areas, and preserve[s] wildlife habitat.”
4. “Wetland, Special Aquatic Site (i.e., Seeps and Springs), Perennial and Intermittent Stream Channel, and Terrace Slope Ephemeral Drainage Channel Protection” Ordinance which establishes bounds and guidance on how to address these locations.
5. “Green Space Site Design” Ordinance “sustains community and regionally important natural area passive recreation experiences and wildlife habitat values.”
6. “Steep Slope/Hillside Protection No Disturbance Setback” Ordinance provides setbacks to protect against long-term degradation of the water way and slopes.
7. “No Wildlife Hunting or Trapping” Ordinance leaves the area open for public use.



[South Jordan Transportation Master Plan](#)

The South Jordan City TMP guides transportation infrastructure investments for the future by documenting the roadway network’s existing operation and forecasting roadway conditions to future years 2033 and 2050. This allows planners to quantify the effects that land development and the resulting population growth may have on the transportation system, in order to assess future roadway project needs. Included in this discussion is an update to the 2020 Active Transportation Plan and recommendations for transit improvements, to be implemented in partnership with UTA. These recommendations were provided with the aim of improving transportation efficiency across each available mode of transportation, which in turn benefits all modes. The TMP also covers City transportation management-related best practices, such as access management standards, safety analyses, identifying policy and ordinance changes, truck routes, traffic calming, and livable street standards.

[SLCo West General Plan](#)

The Salt Lake County West General Plan is intended to give guidance on unincorporated land that falls under the county’s jurisdiction as of 2021. It “envision[s] safe and beautiful places for current and future generations to live, work, and recreate.” Although this plan does not overlap with the Study Area, several synergistic points should be noted. These synergistic points fall under the transportation, active transportation, and public transportation maps and align with the city’s general plan, trails plan, and transportation master plan outlined above. The Study Area also serves as a major town center for much of Central Oquirrh’s future growth

Moderate-Income Housing Plan

With a population projected to reach nearly 97,000 by 2030, the City is experiencing strong demand for housing—especially affordable options—as rising home prices and a tight housing market put pressure on both renters and homeowners. While most residents commute out of the city for work, South Jordan continues to attract families and professionals with its walkable neighborhoods, parks, and strong local economy. This report examines current housing conditions, demographic trends, and future needs, highlighting a growing gap in affordable housing and the City’s efforts to support inclusive, balanced development.

Housing Needs Study

The Housing Needs Study was completed in 2014. At that time, the City was “short on affordable units for the low- (LIH) and very low-income households (VLIH) and held an excess of moderate-income household (MIH) units.” The moderate-income level ranged from \$117,135–\$216,279 at the time of the study. The study also showed a need for 2,265 MIH units but provided over 5,000 MIH units. It also showed the need for 763 LIH and 825 VLIH but provided 28 and 5 units respectively.

| Appendix D – Property Ownership

D. Property Ownership

South Jordan FrontRunner SAP Study Area Property Ownership

RANK	OWNER NAME	OWNER TYPE	ACRES
1	UNITED STATES OF AMERICA	PUBLIC	59.84
2	ULTRADENT PRODUCTS INC	PRIVATE	37.09
3	PACIFICORP / UTAH POWER & LIGHT CO.	PRIVATE	19.71
4	MARJEAN H NIELSON	PRIVATE	18.95
5	TODD HARRISON TRUST	PRIVATE	18.45
6	JERRY SEINER DEALERSHIPS	PRIVATE	16.82
7	SOUTH JORDAN CITY	PUBLIC	14.66
8	SOJO STATION NORTH AND SOUTH, LLC	PRIVATE	9.74
9	SOUTH RIDGE APARTMENTS LLC	PRIVATE	8.15
10	JGW INVESTORS LLC	PRIVATE	6.93
11	DESERET GENERATION & TRANSMISSION CO-OPERATIVE	PRIVATE	6.49
12	UTAH DEPARTMENT OF TRANSPORTATION	PUBLIC	6.45
13	SJ UTAH LLC	PRIVATE	5.72
14	GREENHORNET SPACE AGENCY II, LLC	PRIVATE	5.09
15	UTAH DEPT OF NATURAL RESOURCES DIV OF PARKS & RECREATION	PUBLIC	4.97
16	UTAH DEPARTMENT OF NATURAL RESOURCES DIV PARK & REC	PUBLIC	4.89
17	TOWERS AT SOUTH TOWNE, LLC	PRIVATE	4.76
18	STORAGE WORLD II, LLC	PRIVATE	4.61
19	MDL TIMP CAPITAL, LLC	PRIVATE	4.29
20	GMC TURTLE CREEK LLC	PRIVATE	4.01
21	SOUTH JORDAN HOSPITALITY, LLC	PRIVATE	3.57
22	UTAH TRANSIT AUTHORITY	PUBLIC	3.56
23	PERRY DICE LLC	PRIVATE	2.70
24	106TH SOUTH BUSINESS PARK	PRIVATE	2.62
25	106 EXCHANGE II, LLC	PRIVATE	2.50
26	SJ UTAH COMMERCIAL LLC	PRIVATE	2.48
27	PARKWAY TOWER, LLC	PRIVATE	2.46
28	T AND S WATKINS FAMILY II LLC	PRIVATE	2.31
29	SANDY HOSPITALITY GROUP, LLC	PRIVATE	2.18
30	APATITE SOUTH JORDAN LLC	PRIVATE	2.09
31	SPORTS MEDICINE RESEARCH AND TESTING LABORATORY	PRIVATE	2.00
32	PROTEAN PROPERTIES INC	PRIVATE	1.99
33	SHREE VEER HOSPITALITY LLC	PRIVATE	1.94
34	TREJO INVESTMENTS LLLP ET AL	PRIVATE	1.84
35	5TH AVE RETIREMENT LLC	PRIVATE	1.79
36	APPLE HOSPITALITY SOUTH JORDAN 7144, LLC	PRIVATE	1.75
37	GATEWAY RETAIL PARTNERS LLC	PRIVATE	1.48
38	ATHLETIC PERFORMANCE INSTITUTE LLC	PRIVATE	1.30
39	SOUH RIDGE APARTMENTS II, LLC	PRIVATE	1.19
40	B&C WONG UTAH LLC	PRIVATE	1.12
41	671 POPLAR, LLC	PRIVATE	1.12
42	10702 PROPERTY, LLC	PRIVATE	1.06
43	HOLDINGS10 LLC	PRIVATE	0.75
44	RICHARD G NIELSON	PRIVATE	0.59
45	SUMMIT GROUP OF PARK CITY, LLC	PRIVATE	0.51
46	RIVERPARK LLC	PRIVATE	0.20

| Appendix E – Jordan River Best Practices Resources

Best practices for revegetation along the Jordan River (Utah, USA) following control of invasive plant species

Item 1.1.

Rae Robinson, Keith Hambrecht, and Eric McCulley

Version 2 | January 2024

This document was informed by the experience of practitioners working in wetland and riparian areas in the region, including those working with the Jordan River Commission and Division of Forestry, Fire and State Lands.

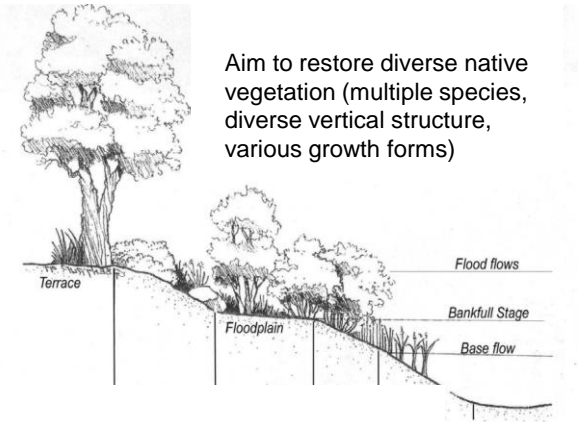
Introduction

A common goal for land managers along the Jordan River is to control *Phragmites* (*Phragmites australis* subsp. *australis*), Russian olive (*Elaeagnus angustifolia*), tamarisk (*Tamarix* species), and puncturevine (*Tribulus terrestris*) and restore diverse plant communities (with numerous native tree, shrub, grass, and forb species). We summarize best practices for revegetation from our experience and those of practitioners working on the Jordan River in UT.



Why pursue active revegetation

Removing large populations of invasive species may result in the loss of important ecosystem functions (e.g., temperature regulation/shade, nutrient cycling, bank stabilization, habitat). Ideally this loss is temporary and diverse native plants replace (and improve) these ecosystem functions. But often native plants fail to quickly recover, especially if environmental conditions are not favorable, secondary invasions threaten the restoration site, or surrounding native plant populations (in the seedbank and adjacent areas) are insufficient. **Active revegetation can accelerate native species establishment and prevent reinvasion of invasive species.**



Source: Hoag, et al., 2001. Riparian Planting Zones in the Intermountain West. Information Series #16. NRCS - Plant Material Center, Aberdeen, ID.

When to revegetate

Revegetation can be implemented at a restoration site before, during, or after invasive plant control, depending on the site. The timing of revegetation will depend on restoration goals, the extent of the invasion, and how well the species has been controlled. Multiple years of revegetation may be necessary to achieve the desired cover or density of species.

Revegetation prior to invasive species control

Example: Planting trees earlier in the restoration process gives planted trees a “head start”. Caution— be sure not to plant or seed in areas that will be in the way of future invasive species control treatments.

Revegetation during invasive species control

Example: If removing Russian olive and tamarisk trees patch by patch at a site, consider revegetating newly barren patches using a phased approach.

Revegetation after effective invasive species control

Example: When revegetating an area that was previously occupied by a dense *Phragmites* stand, be sure *Phragmites* cover and biomass is minimal (after many years of treatment) before proceeding with revegetation.

Revegetation after a disturbance of earth (e.g., utilities, trails, boat ramps)

Revegetate within two weeks of the disturbance, no matter the time of year. Follow up with additional revegetation treatments in the fall and spring.





Important considerations

- The quick recovery of native plant cover to an area will provide resistance to future reinvasion.
- Mark and protect revegetation areas. Do not let herbicide drift damage native plant communities. Be sure to protect revegetation areas from herbivory (e.g., use at least 14-gauge welded wire fencing around trees).
- Invasive plant seeds are transported on shoes and equipment. Be sure to properly clean equipment before moving to a new site.



Revegetation methods

Moisture is key to successful revegetation, especially in the first few years following planting or seeding. Knowledge of water availability at your site (e.g., depth to ground water, timing and duration of inundation) will be valuable to your restoration planning. Supplemental irrigation or selection of drought-tolerant species may be required for drier sites. Generally, revegetation should take place in the fall or spring to capitalize on favorable conditions. Below are short descriptions of revegetation methods that may be useful along the Jordan River.

Plant materials	Brief methods and equipment	Notes
 <p>Seed</p>	<ul style="list-style-type: none"> • Sow a species-diverse seed mix (the species in the mix will depend on your restoration goals) • Use a handheld seed spreader and rake, Argo or UTV with hopper and harrow, or hydroseeder 	<ul style="list-style-type: none"> • Harrow or rake seeds into soil to ensure good seed to soil contact. • Many species exhibit dormancy. Fall seedings will allow for greater seed dormancy break. If seeding in the spring, consider a cold stratification treatment. • Reach out to the Jordan River Commission for seed mix examples and guidance.
 <p>Potted plants (i.e., plugs, containerized plants of all sizes)</p>	<ul style="list-style-type: none"> • Use shovels or machine-powered augers for digging holes • Water the hole prior to planting • When planting the tree, create a shallow depression around the tree at the soil surface • Water the tree immediately after planting 	<ul style="list-style-type: none"> • Deep-rooted plants (e.g., 30" tall potted plants) may do better than plants with shallower roots when ground water is less available. • Knowledge of site hydrology is key. Supplemental irrigation may be needed in the first two to three years.
 <p>Cuttings/pole plantings</p>	<ul style="list-style-type: none"> • Some wetland plants (e.g., willows, cottonwoods) can be propagated from cuttings. • See "A Guide for Harvesting, Storing and Planting Dormant Willow Cuttings" (Wildlands Restoration Volunteers, 2008) for more information. 	<ul style="list-style-type: none"> • Soak cuttings in water for 2-10 days prior to a dormant planting. • Rebar, an auger, or a water stinger can be used to create deep holes (a majority of the cutting will be buried).
 <p>Sod mats</p>	<ul style="list-style-type: none"> • Use stakes to secure mats to the ground • If hydrology is insufficient to keep roots wet, supplemental irrigation may be needed following installation 	<ul style="list-style-type: none"> • Sod mats may be useful in areas with consistent hydrology and a 3:1 slope or less. • Native plant nurseries usually need a long lead time to grow sod mats for projects.

Plant material source — It is best practice to source local plant materials (e.g., from within the watershed, state, region) whenever possible. However, sourcing materials from further away (but still within the West) is acceptable for species that have a high degree of plasticity (e.g., many bulrushes, rushes, sedges) or those that are not available otherwise.

Species to consider for revegetation

Functional group	Common name	Scientific name	Wetland indicator status*
Riparian trees	Box elder	<i>Acer negundo</i>	FACW
	Black hawthorn	<i>Crataegus douglasii</i>	FAC
	Fremont cottonwood	<i>Populus fremontii</i>	FACW
	Peachleaf willow	<i>Salix amygdaloides</i>	FACW
	Whiplash willow	<i>Salix lasiandra</i>	FACW
Shrubs	Big sagebrush	<i>Artemisia tridentata</i>	FACU
	Fourwing saltbush	<i>Atriplex canescens</i>	UPL
	Rubber rabbitbrush	<i>Ericameria nauseosa</i>	UPL
	Broom snakeweed	<i>Gutierrezia sarothrae</i>	NI
	Chokecherry	<i>Prunus virginiana</i>	FAC
	Skunkbush sumac	<i>Rhus trilobata</i>	FACU
	Golden currant	<i>Ribes aureum</i>	FAC
	Woods' rose	<i>Rosa woodsii</i>	FACU
	Sandbar/coyote willow	<i>Salix exigua</i>	FACW
	Greasewood	<i>Sarcobatus vermiculatus</i>	FAC
	Silver buffaloberry	<i>Shepherdia argentea</i>	FACU
	Common yarrow	<i>Achillea millefolium</i>	NI
Forbs (flowering herbaceous species)	White sagebrush	<i>Artemisia ludoviciana</i>	FACU
	Swamp milkweed	<i>Asclepias incarnata</i>	OBL
	Showy milkweed	<i>Asclepias speciosa</i>	FAC
	Hairy false goldenaster	<i>Chrysopsis villosa</i>	NI
	Rocky Mountain beeplant	<i>Cleome serrulata</i>	FACU
	Blanket flower species	<i>Gaillardia spp.</i>	FACU
	Curly cup gumweed	<i>Grindelia squarrosa</i>	FACU
	Annual sunflower	<i>Helianthus annuus</i>	FACU
	Nuttall's sunflower	<i>Helianthus nuttallii</i>	FACW
	Lewis flax	<i>Linum lewisii</i>	NI
	Canada goldenrod	<i>Solidago canadensis</i>	FACU
	Saltgrass	<i>Distichlis spicata</i>	FAC
Graminoids (grasses and grass-like species)	Nebraska sedge	<i>Carex nebrascensis</i>	OBL
	Common spikerush	<i>Eleocharis palustris</i>	OBL
	Common field sedge	<i>Carex praegracilis</i>	FACW
	Arctic rush	<i>Juncus arcticus</i>	FACW
	Torrey's rush	<i>Juncus torreyi</i>	FACW
	Western wheatgrass	<i>Pascopyrum smithii</i>	FAC
	Sandberg bluegrass	<i>Poa secunda</i>	FACU
	Nuttall's alkaligrass	<i>Puccinellia nuttalliana</i>	FACW
	Hardstem bulrush	<i>Schoenoplectus acutus</i>	OBL
	Chairmaker's bulrush	<i>Schoenoplectus americanus</i>	OBL
	Common threesquare bulrush	<i>Schoenoplectus pungens</i>	OBL
	Alkali sacaton	<i>Sporobolus airoides</i>	FAC
	Sand dropseed	<i>Sporobolus cryptandrus</i>	FACU

* Wetland indicator status abbreviations

UPL = upland (almost never occur in wetlands)

FACU = facultative upland (usually occur in non-wetlands, but may occur in wetlands)

FAC = facultative (occur in wetlands and non-wetlands)

FACW = facultative wetland (usually occur in wetlands, but may occur in non-wetlands)

OBL = obligate (almost always occur in wetlands)

NI = non-indicator

Note: Some of the species in this table may not be commercially available. Local collection of seed or cuttings may be possible by some nurseries, seed collectors, students, or volunteers.

For more information about the Jordan River Cooperative Weed Management Area please contact Rae Robinson, Jordan River Vegetation Project Coordinator, at raerobinson@utah.gov.

Best practices to control non-native, invasive Russian olive and tamarisk within the Jordan River Corridor (Utah, US)

Item 1.1.

Rae Robinson, Keith Hambrecht, and Eric McCulley

Version 2 | January 2024

This document was informed by the experience of practitioners working in wetland and riparian areas in the region, including those working with the Jordan River Commission and Division of Forestry, Fire and State Lands.

Introduction

Russian olive (*Elaeagnus angustifolia*) and tamarisk (*Tamarix* species) are non-native, invasive tree species that are common in the Jordan River system and are on the State of Utah Noxious Weed List. These species outcompete native plant species, reduce the complexity of the ecosystem, and make it difficult for wildlife and humans to access the river. A common goal for land managers along the Jordan River is to control these two species and support the recovery of a diverse riparian plant community (native trees, shrubs, grasses, and forbs). We summarize best practices for controlling these species from our experience and those of practitioners working on the Jordan River in UT.



Important considerations for Russian olive and tamarisk control

In general, plan to remove invasive trees and revegetate with native plants in phases. This will help to mitigate the dramatic and quick loss of structure, habitat, and shade.

Bird habitat

- If there are known high use areas by birds, especially sensitive species, map and monitor these locations. Avoid removals in these areas or wait until nearby higher quality habitat is established and see if birds shift over to these new spots.
- When possible, use a frill cut treatment on large diameter, tall, and straight Russian olive trees to provide perches.
- Typically, it is fine to remove lone Russian olive and tamarisk trees, as they do not provide frequently utilized habitat.

Shade

- In some situations, it may be appropriate to revegetate the project site with native plants prior to removing all of the tall shade-providing invasive trees. Once tall native vegetation is established, the rest of the invasive trees can be removed.



Erosion

- Invasive trees growing on and near the river's edge assist in stabilizing the bank. Removing these trees may lead to bank erosion over time. Consider these effects before removing invasive trees along the river's edge.

Revegetation

Many restoration sites will need active revegetation to restore ecosystem functions and fulfill restoration goals. For more information, see "Best practices for revegetation along the Jordan River (Utah, USA) following control of invasive plant species".

Selection of techniques

There are several different ways to control Russian olive and tamarisk. We highlight the most effective strategies below. The cut stump method will kill the tree and allow for the biomass to be chipped or piled. The frill cut method will kill the tree slowly and will result in standing dead branches that may create desirable bird habitat. For small project areas, hand-pulling first-year seedlings works well for preventing infestations. These control treatments can occur any time of year except for when the ground is frozen.

Cut stump for Russian olive and tamarisk

- For tall trees, cut the stumps a few feet tall and clear the biomass away from the tree. Once the biomass is cleared from the area, cut stumps low and **immediately** apply herbicide to the cambium layer, the layer of living cells underneath the bark of the tree. No need to apply herbicide to the bark or inner core wood.
- Before applying herbicide, be sure to wipe away any dirt or sawdust from the stump that would inhibit herbicide uptake.
- Any untreated stumps or whips will vigorously regrow.**
- Consider cutting stumps to ~4" above the ground to allow for retreatment if regrowth occurs.
- Chip or pile the biomass. Piles should be densely packed and be no bigger than 6 x 6 ft. Piles can be burned or left to provide habitat for insects, birds, and small mammals. Do not pile in areas that could flood—intact branches have the potential to take root.
- While chainsaws or a skid steer with a mastication head attachment can be used for large trees, heavy duty loppers work well for smaller stems.



Cut stump herbicide application set up

Frill cut for Russian olive

- With a hatchet or ax, chop through the bark into the tree at a downward angle so the frill cut will hold the chemical.
- Immediately** inject or pour 1ml of herbicide into each frill cut.
- Space frill cuts around the stem(s) of the tree, one frill for every 2–3 inches of the tree diameter at breast height. Leave space in between the cuts—the tree must be able to move the chemical via intact phloem.
- Multiple stems should be treated separately.
- Leave the tree in place for at least a year to be sure the tree has been killed.



A frill cut

Recommended herbicide mixtures

You must follow all labels of the products applied.

- Russian olive – for cut stump and frill cut, undiluted glyphosate
- Tamarisk – for cut stump, undiluted triclopyr or imazapyr at labeled rate
- Penetrant/surfactant for cut stump treatments (e.g., LI-700 at ~2 oz per gallon)
- Dye (at the labeled rate)

Use caution when applying herbicide near native vegetation. Imazapyr herbicides persist in the soil (potentially affecting desirable seedlings) and have more label restrictions.

For more information about the Jordan River Cooperative Weed Management Area please contact Rae Robinson, Jordan River Vegetation Project Coordinator, at raerobinson@utah.gov.

Best practices to control non-native, invasive *Phragmites* within the Jordan River Corridor (Utah, USA)

Item 1.1.

Rae Robinson, Keith Hambrecht, and Eric McCulley

Version 2 | January 2024

This document was informed by the experience of practitioners working in wetland and riparian areas in the region, including those working with the Jordan River Commission and Division of Forestry, Fire and State Lands.

Introduction

Controlling *Phragmites* (*Phragmites australis* subsp. *australis*) is a common goal of many land managers working in wetland and riparian areas in North America. In this document we summarize best practices for controlling *Phragmites* from our experience and those of practitioners working on the Jordan River in UT. We plan to update this document as we make progress treating *Phragmites* on public and private lands along the Jordan River so that it will be a relevant resource for those working to control *Phragmites* on the Jordan River.

Non-native, invasive *Phragmites australis* subsp. *australis*

The *Phragmites* species we reference in this document is the non-native, invasive subspecies that is not originally from North America, has aggressive characteristics, and is on the State of Utah Noxious Weed List. Almost all of the *Phragmites* in northern Utah is *Phragmites australis* subsp. *australis*. This subspecies outcompetes and replaces diverse native plant communities which sustain diverse resident and migratory wildlife species. *Phragmites* encroaches into the river channel and up the banks of the Jordan River, impeding human and wildlife access along and to the river. The native subspecies, *Phragmites australis* subsp. *americanus*, does not grow as densely and may have subtle morphological differences (e.g., leaf sheath falls off easily in the native subspecies; it stays tightly attached to the dead culm in the non-native subspecies).



How to prioritize sites

The criteria below may be useful in identifying areas to prioritize *Phragmites* control and restoration.

These criteria were developed by the *Phragmites Working Group for the Jordan River*.

Critical need — areas where ecosystem functions and services are at great risk and demand immediate attention

Protection — areas where ecosystem functions and services are of high quality but need management to maintain this status

Leveraged efforts — areas where control efforts would build upon other investments along the Jordan River

How to control *Phragmites*

- We recommend integrating mechanical (i.e., mowing), chemical (i.e., herbicide), and revegetation strategies to control *Phragmites* on the Jordan River. Research shows that the application of these three strategies is much more effective than one of these techniques alone.
- Typically, *Phragmites* will need to be controlled with mechanical and chemical treatments within the same year, for 3–5 years. Revegetation will likely be needed to assist in the recovery of native plants.
- Other strategies to control *Phragmites*, such as grazing and water control, may apply in some places on the Jordan River, however due the nature of the Jordan River system (e.g., highly developed, regulated flows), these strategies may be less applicable.

Treatment timeline for Phragmites control

June – July	July – September	October – April
Mowing ^a	Herbicide ^b	Mowing ^c
		Revegetation ^d

The above table shows a coarse timeline for controlling Phragmites. **This timeline should be repeated each year until Phragmites is controlled (typically this will take 3–5 years).** The timing of these treatments may vary due to the weather from year to year and the location of the restoration site.

a: Mowing in June or July can improve access for spraying and reduce seed production. This step is optional. Take precautions to avoid disrupting nesting birds.

b: Appropriate timing for spraying may vary within a given site. Drier areas may need to be sprayed in July before plants are drought-stressed (see note below about drought stress). Phragmites growing in wet areas likely can be sprayed in August and September. Spraying later in the season has been shown to yield better long term control.

c: This mowing treatment will break up the standing dead Phragmites and will allow light to reach the soil surface (important for germination of native plants).

d: Revegetation (e.g., sowing seeds, planting trees) adjacent to small patches of Phragmites can be pursued in the first year of control. Revegetation within dense Phragmites stands should be pursued only when significant progress has been made in reducing Phragmites cover and biomass. Care should be taken to avoid spraying or mowing newly revegetated areas. Generally, the best time to revegetate is in the fall or spring when conditions are cool and moist.

Important considerations for the timing of treatments

- **Spray Phragmites with herbicide before it becomes drought-stressed.** When the plant is stressed it has a weakened ability to metabolize the herbicide. Drought stress can present as obvious or subtle yellowing of the leaves. Context clues, like the current drought status, or distance to water can help determine if the plant is drought-stressed. Be aware that there may be a shorter window to spray Phragmites that is growing further from water.
- **Do not mow Phragmites within one month of an herbicide treatment, before or after.**
- **Be sure to spray Phragmites before the first frost.**



Yellowing at the tips of Phragmites leaves can indicate drought stress.

Recommended herbicide mixture

You must follow all labels of the products applied and calibrate your spray equipment.

- Aquatic-approved glyphosate; use the high end of the labeled rate (e.g., RoundUp Custom at 3 Qt/acre, or 1.5% solution for handheld spot spray)
- Aquatic-approved, non-ionic surfactant (e.g., LI-700 at labeled rate)
- Ammonium sulfate (8 to 17 lbs per 100 gallon spray solution)
- Clean water
- Dye (at the labeled rate)

Aquatic-approved imazapyr herbicides are also an option, but they are more expensive, persist in the soil (potentially affecting desirable seedlings), and have more label restrictions. Consider using imazapyr, if the label allows, in areas where glyphosate has been used for many years to prevent herbicide resistance from developing.

Other considerations

- Create work corridors to allow vehicles and people to efficiently access the restoration site. This may involve removing some Russian olive, tamarisk, and Phragmites.
- Consider bank erosion—in some places it may not be appropriate to remove Phragmites if its removal will accelerate erosion that will negatively affect infrastructure.
- To prevent clonal expansion of small Phragmites stands, consider hand pulling Phragmites runners.

Phragmites treatment methods, equipment, and materials

Mowing — to reduce seed production, improve access, and remove dead biomass

Equipment	Useful when...
Brushcutter (e.g., Stihl FS 561 C-EM with tri-bladed brush knife attachment)	Small area; poor vehicle access
Marsh Master or other wetland tractor with mower	Large area; mostly flat, wet ground
Skid steer with mowing attachment	Patches of all sizes; dry ground
Mini excavator with mowing attachment	Steep slopes



Herbicide spraying — to kill the plant

Equipment	Useful when...
Backpack sprayer	Small area; short or sparse Phragmites; poor vehicle access
Argo (amphibious side-by-side)	Wet ground (but avoid deep standing water); tall Phragmites
UTV (side-by-side)	Dry ground
Drone	High vantage point nearby; poor access by foot and vehicle
Marsh Master or other wetland tractor	Large area; mostly flat, wet ground
Boat (e.g., Jon boat)	Poor access from land



Revegetation — to assist in the recovery of native plant species and prevent Phragmites reinvasion

Plant materials

Seed

Potted trees, shrubs, grasses, forbs

Cuttings

Sod mats

For more information about revegetation see “Best practices for revegetation along the Jordan River (Utah, USA) following control of invasive plant species”.

For more information about the Jordan River Cooperative Weed Management Area please contact Rae Robinson, Jordan River Vegetation Project Coordinator, at raerobinson@utah.gov.

City of South Jordan Planning Commission

By-laws and Rules of Procedure

A. ORGANIZATION

1. Selection of the Chair and Officers.

- a. At the first regularly scheduled meeting of each year, the Commission shall have an agenda time to select a Chair and officers for the next calendar year.
- b. Selection of the Chair and officers shall be made separately through open nomination for selection from any Commissioner. If only one Commissioner is nominated for either position, an approval vote of the Commission shall determine the selection. If more than one Commissioner is nominated for an office, a simple majority vote of the Commissioners present shall determine the office holder. Any commissioner may serve more than one term in a Commission office.
- c. In the event of any officer vacancy, the commission will follow the process described in Section A.1.b.

2. Duties of the Chair.

- a. Planning Commission meetings shall be held at the call of the Chairman. (City Code 17.16.010 A)
- b. To call meetings of the Commission to order on the day and time scheduled and proceed with the order of business.
- c. To receive and submit, in the proper manner, all motions, recommendations, and propositions presented by the Commissioners.
- d. To put to vote all issues and items which are properly moved upon, or necessarily arise in the course of the proceedings and to announce the result of any such actions taken.
- e. To inform the Commission, when necessary, of any point of order or practice. In the course of discharge of this duty, the Chair shall have the right to call upon legal counsel or staff for advice.
- f. To maintain order at all meetings of the Commission including the maintenance of a respectful, orderly, appropriate, and professional atmosphere.
- g. To move the agenda along, reduce redundancy by limiting time allowed for comment, whenever possible, set guidelines for public input, and reference handouts and procedures during meetings.
- h. Recognize speakers and Commissioners prior to receiving comments and presentations.
- i. Ensure the legal due process to all items and people involved in each item of each meeting agenda; and
- j. Ensure that a quorum will be present prior to public notice for a commission meeting being posted.
- k. Coordinates with City Staff to ensure that mandatory training is completed.

3. Duties of the Vice Chair. The Vice Chair Shall have and perform all of the duties and functions of the Chair in the absence of the Chair.

4. Temporary Chair.

- a. In the event of the absence or disability of the Chair and Vice Chair, and with a proper quorum present, a temporary Chair shall be selected to conduct business prior to any agenda item to serve as the temporary Chair until the return of the either the Chair or Vice Chair.
 - b. The temporary Chair shall be nominated and selected in the same manner as the regular nomination and selection of the Chair.
5. Quorum. The Commission follows City code 17.16.010a as the definition of a quorum. At least 3 Commissioners must be in attendance. No action of the Commission shall be official or have any effect except when a quorum of the Commissioners are present. While City of South Jordan encourages the physical attendance of all of its Commissioners at the meeting anchor location, there is, from time to time, a need for Commissioners to connect to meetings via the electronic meeting method includeding in the meeting notice. At a minimum, one Commissioner must be physically in attendance at the meeting anchor site. †
6. Placing Items on the Agenda. A request for an item to be on a Commission meeting agenda may be initiated by the Chair, two Commissioners, the staff, the Mayor, or the City Council. Agenda items must be in compliance with all public meeting requirements including noticing.

B. MEETINGS

1. Meeting Schedule. Meetings shall be scheduled in accordance with Sections 17.16.010 of the City Code
2. General Order of Business. The business of the Commission at its meetings shall generally be conducted in accordance with the following order, unless otherwise specified.
 - a. Welcome and Roll Call
 - b. Motion to Approve Agenda
 - c. Approval of the Minutes
 - d. Staff Business
 - e. Comments from Planning Commission Members
 - f. Summary Action
 - g. Action
 - h. Administrative Public Hearings
 - Conditional Use Permit Applications
 - Subdivision Applications
 - Site Plan Reviews
 - i. Legislative Public Hearings
 - General Plan and Amendments
 - Rezoning Applications
 - Annexation Applications
 - Code Amendments
 - j. Other business
 - k. Agenda Items and Public Hearings
 - l. Adjournment
3. Public Forum. The Commission agenda generally does not allow members of the general public to address the Commission regarding issues which are not listed on

the posted agenda. However, upon majority vote of the Commission, Public Forum may be allowed and shall adhere to the following parameters of Section 5 b:

4. Procedure of Consideration of Agenda Items. The following procedure will normally be observed for all agenda items, however, it may be rearranged at the discretion of the Chair for an individual item, for the expeditious conduct of business:
 - a. Introduction of the item by the Chair;
 - b. Staff presentation and recommendation;
 - c. Presentation of the proposal by the applicant(s);
 - d. Public Hearing to be conducted according to paragraph B5 below;
 - e. Commission Discussion. The discussion shall be confined to the Commission unless any Commissioner requests specific additional information from staff, the applicant(s), or via a majority vote of the Commission a member of the general public present who is or may be directly involved or impacted by the issue under consideration; and
 - f. Motion and Vote. The Chair may outline the possible actions of approval, approval with conditions, tabling, or denial prior to entertaining a motion. Voting shall be in accordance with section F below.
5. Public Hearings.
 - a. *Procedure*
 - Opening of the public hearing by the Chair.
 - Testimony by citizens present to discuss the item through comment or testimony of facts or arguments.
 - Closure of the public hearing by the Chair.
 - b. *Public Comment During Public Hearings.* Comments, testimony, and presentations from the public shall be respectful, pertinent, relevant, concise, and confined within a three-minute timeframe. A spokesman for a group of citizens wishing to make similar comments regarding the agenda item should be allowed up to five minutes. Public comments should be non-repetitious. The Commission Chair shall have the ability to discontinue the receipt of public comment at his/her discretion if the commentary and testimony becomes repetitious, disrespectful, rude, or otherwise rowdy. All public comment shall be directed toward the Commission and may only be received in writing or from the speaker's podium, with the exclusion of recognized exceptions due to a disability of the speaker, within the location of the meeting. Public comment not originated from the podium shall not be accepted or considered by the Commission.
 - i. *Time Limit.* This portion of the agenda should not last more than approximately 30 minutes combined for all speakers. Speakers should limit their comments, under the jurisdiction of the Chair, to approximately two minutes. Speakers declaring their representation of a group of the general public present may be allotted an extended amount of time to address the Commission, at the discretion of the Chair, so long as the extension does not limit the ability or time for other members of the general public to address the Commission.

- ii. **Speakers to be Heard.** Speakers wishing to be heard shall make their comments from the speaker's podium within the City Council chambers, or other locations of the meeting, with the exclusion of recognized exceptions due to a disability of the speaker. All comments shall be directed towards the Commission and should be made in a respectful and concise manner.
- iii. **Written Comments.** Members of the general public may, and are encouraged to submit written comments relating to any item of City business to the City Planning Department, 12:00 pm local time on the day of a Commission meeting, and said comments may be read and distributed to the Commission for their review. The Commission may conduct discussion as deemed appropriate regarding any comments submitted.
- iv. **Repetitious Comments.** Members of the general public addressing the Commission shall not present the same, or substantially same items or arguments to those provided by other members of the general public. Once an item has been heard by the Commission and determined to necessitate an action or necessitate no action the same, or substantially same, item may not be presented by the same member of the general public or any other.
- c. **Commission Action.** Comments from the public shall be received and weighted but not responded to by the Commission during the public hearing.
- d. **Timeframe.** Public comment may be limited to 30 minutes at the Chair's discretion for each agenda item requiring public hearing
- 6. **Adjournment.** No Commission meeting shall be permitted to extend past 10:00 p.m. unless a majority vote of the Commissioners present determines an extension of the meeting past 10:00 p.m. With such a vote, the motion for extension shall include a specific amount of time for the extension of the meeting and a determination of which remaining agenda items are to be considered in the extension. Remaining agenda items for any meeting not extended past 10:00 p.m., or otherwise not fully addressed within the extended time, shall have a motion for continuance made upon them, either to the next regularly scheduled meeting or to a specially called meeting, prior to the meeting's adjournment. No item on a noticed agenda shall be left without action being taken regarding that item, through formal action or continuance, prior to the adjournment of any meeting.

C. RIGHTS AND DUTIES OF THE COMMISSIONERS

- 1. **Meeting Attendance.** Every Commissioner shall attend each meeting of the Commission unless excused or unable to attend due to extenuating circumstances. Any Commissioner expecting to be absent from a meeting of the Commission shall notify the City Planning Department and/or the Chair.
- 2. **Conflict of Interest.**
 - a. If any Commissioner has a conflict of interest with an item on an agenda, the Commissioner shall declare the conflict before any testimony for that item is heard. The Commissioner may then choose to step down from the Commission table and withdraw from discussion and voting on the agenda

item. Following action by the Commission on the agenda item, the Commissioner may return to the Commission dais and resume his/her duties as a Commissioner. In the event the commissioner declines his/her option to step down after declaring a conflict of interest, the Chair shall ask if any Commissioner wishes to request a vote on the conflict.

- If a request is made, the Commissioner making the request shall detail his/her request including any request to have the Commissioners step down and/or withdraw from discussion or voting. The remaining Commissioners, other than the Commissioner declaring the conflict of interest, shall vote as to the request. A simple majority of the remaining Commissioners voting in favor of the request shall approve the request. The Commissioner declaring the conflict of interest shall then comply with the approved request. If an approval is not achieved, the Commissioner declaring a conflict of interest may proceed under his/her own recognizance.
 - If no such request is made, the Commissioner declaring a conflict of interest may proceed under his/her own recognizance.
- b. Any Commissioner who feels he/she, or another Commissioner, may have an actual, apparent, or reasonably foreseeable conflict of interest shall declare such feelings. Such declarations shall be determined and handled as in Section C-2-a herein.
 - c. No Commissioner with an actual, or possible, conflict of interest shall, at any time before, during, or after the meeting, attempt to use his/her influence with another Commissioner with regard to the agenda item in question.
 - d. Conflicts of interest are determined to be any personal, familial, or financial tie between the Commissioner and the applicant or the item of any manner of business.
 - e. A Commissioner may appear before the Commission through his/her employment or as an advocate or agent for a proponent, or as the applicant, only after declaring his/her conflict of interest and stepping down from the Commission table.
 - f. A Commissioner shall not sell or offer to sell services, or solicit prospective clients or employment, by stating an ability to influence Commission decisions or on the basis of being a Commissioner.
 - g. A Commissioner must not use the power of his/her appointed office as a Commissioner to seek or obtain any special advantage.
3. Not to Vote Unless Present. No Commissioner shall be permitted to vote on any question, matter of business, or agenda item unless the Commissioner is present at the meeting in which the vote is taken and the result is announced regarding the issue. No Commissioner shall give his/her proxy to any other person to vote on any issue.
 4. Special Meetings. A Special meeting may be requested by any Commissioner to hear matters of Commission business. It shall be at the discretion of the Chair to grant or deny such a request and schedule such a meeting, as necessary.

5. Quorum. A simple majority or a minimum of 3 Commissioners, whichever is greater of the total of the Commissioners present shall constitute a quorum for the transaction of business. Any Commissioner choosing to abstain from a vote on an agenda item shall be included when in consideration of a quorum. Any Commissioner disqualified under the terms of a conflict of interest shall not be included when considering the presence of a quorum. When a conflict of interest results in a lack of Commissioners present to approve a motion, the issue, at the discretion of the Chair, may be heard by the Commission but may not have action taken upon it until an adequate amount of Commissioners can be present to properly vote on the issue.
6. Lack of a Quorum. No matters of business shall be heard, unless caused by a conflict of interest disqualification, unless a proper quorum is present. In the event a quorum is not present for a scheduled meeting of the Commission, the meeting shall be canceled, only after a reasonable allowance of time for a quorum to arrive through a motion for adjournment. No second shall be required for such motion.
7. Commissioner Decorum.
 - a. *Appearance.* Commissioners in attendance at each meeting shall portray an appropriate appearance as a representative of City of South Jordan to conduct the meeting in a serious, respectful, and sincere manner.
 - b. *Actions.* At no time shall any Commissioner make any comments, gestures, or other similar actions which is or can be portrayed to be demeaning, insulting, or disrespectful of the other Commissioners, staff, applicant(s), or any member of the general public.

D. ORDER AND DECORUM

1. General Decorum. The atmosphere of a Commission meeting shall be conducted with the utmost respect for and by all parties. All those in attendance shall conduct themselves in a courteous, mindful, professional, sincere, and appropriate manner for the nature of the proceedings. Things and actions such as booing, hissing, cheering, clapping, throwing objects, obscene gestures, harassing comments, or other similar or obnoxious behavior shall not be tolerated.
2. Due Process. The Commission Chair shall conduct each meeting in such a manner so as to afford due process throughout the proceedings.
3. Chair's Authority. It shall be the authority and discretion of the Commission Chair to order the removal of any individual member of the general public present at a Commission meeting, groups of the general public, or the removal of the general public present in its entirety, for the duration of the discussion and consideration regarding any single agenda item, or for the remainder of the meeting, if said person or groups of people become rude, disrespectful, disruptive, or otherwise rowdy towards the Commission, any single Commissioner, staff, the applicant, or other members of the general public.
4. Chair's Adjournment. In the event of refusal to leave the meeting by any member of the general public under the authority granted under Section D-4 herein, it shall be the authority and discretion of the Commission Chair to promptly adjourn any meeting of the Commission when said meeting becomes out of hand, unruly, overly disruptive, or otherwise inhibitive to the conduct of the City's business until such time as business can be conducted in an appropriate manner.

E. MOTIONS

1. Making a Motion. Any Commissioner, including the Chair, may make or second any motion.
2. Findings. Motions for approval, denial, or approval with conditions should state reasons, issues, and facts leading to the motion within the motion.
3. Motions Repeated. Motions may be repeated for clarification, further understanding, or consideration at the request of any Commissioner, or by staff for the purpose of clarification for the official record.
4. Legal Counsel. Any Commissioner may request legal advice from the City legal counsel in the preparation, discussion, and/or deliberation of any motion.
5. Second Required. Each motion of the Commission must be seconded with the exception of a motion to adjourn the Commission meeting.
6. Withdrawing a Motion. After a motion has been stated, the motion resides in the possession of the Commission but may be withdrawn by the author of the motion at any time prior to the motion being put to vote. Withdrawal of a second shall become automatic with the withdrawal of the motion.
7. Motion to Table. A motion to table an agenda item shall be accompanied by the reasoning and rationale for the tabling, such as further study or pending further information or review, and whenever possible, a specific date for which the issue would be reheard.
8. Amending Motions. When a motion is pending before the Commission, any Commissioner may suggest a motion amendment to the author of the motion at any time prior to the motion being put to vote. The amendment must be accepted by the author of the motion and the author of the second in order to amend a motion. Amendments to an amended motion shall be handled in the same manner.
9. Reconsideration of Motions. Any Commissioner who voted in favor of any approved motion may offer a motion to reconsider the motion at any time during the meeting in which the vote took place or during the review of the minutes of the meeting in which the vote took place. A motion to reconsider must pass an approval vote in order to reconsider the action taken. If reconsideration finds that the previously approved motion should stand, no formal vote shall be necessary. If the former motion is to be amended or made void, the reconsideration motion shall be put to a formal vote of the Commission.
10. Motion to Recess. Any Commissioner may offer a motion for recess, and have said motion considered and voted upon, at any point during a Commission meeting. Any such motion shall contain a specific time to reconvene the meeting. No such recess shall be made for a period of time greater than one hour.
11. Dead Motions and New Motions. In the event that a seconded motion does not carry due to the lack of an adequate sustaining vote, the motion is determined to be dead and the discussion and consideration of the agenda item shall continue until another motion and second are offered and approved.
12. Motion to Adjourn. A motion to adjourn shall be required to conclude every Planning Commission meeting, following the addressing of each item on the agenda for that specific meeting. No second shall be required for a motion to adjourn so long as the Chair declares the meeting closed.

F. VOTING

1. Changing a Vote. No Commissioner shall be permitted to change his/her vote once the Chair has declared the result of the decision.
2. Abstention. Any Commissioner may choose to abstain from voting on any agenda item if the Commissioner perceives a personal legal implication or other conflict. Commissioners wishing to abstain may remain at the Commission table and participate in the discussion. Reasoning for abstention is not required to be revealed prior to the vote being taken, but must be disclosed as a part of the Commissioner's vote for abstention in order to ensure that no conflict of interest has occurred. Without limitation, Commissioners may abstain from voting to approve the Commission minutes for any Commission meeting at which that Commissioner was not present.
3. Process of Voting. Any agenda item requiring a vote of the Commission to determine a decision or a recommendation, with the exception of those items requiring a roll call vote, shall be conducted follow the Chair's recognition of a motion and a second regarding the agenda item and the allowance for Commission discussion, by:
 - a. The Chair's request for "All those in favor of the motion regarding...";
 - b. The simultaneous vocalization of "Aye" or "Yes" by the Commissioners voting in favor of the motion;
 - c. The Chair's request for "All those opposed to the motion regarding...";
 - d. The simultaneous vocalization of "Nay" or "No" by the Commissioners voting in opposition to the motion;
 - e. The Chair's request for "All those wishing to abstain from voting on the motion regarding...";
 - f. The simultaneous vocalization of "Abstain" by the Commissioners wishing to abstain from voting on the motion; and
 - g. The Chair's declaration of the voting results.
5. Roll Call Vote. A roll call vote shall be held to vote on motions made regarding:
 - a. The approval of an agenda item where the Commission is the designated approving body;
 - b. Recommendations to the City Council regarding ordinances, amendments to the City Code, adoption or amendment of the General Plan, or the adoption or amendment of any City master plan; or
 - c. The adoption of any Commission resolution.
6. Conducting a Roll Call Vote. When a roll call vote is necessary, the Planning Commission Chair shall, following a motion, second, and Commission discussion, request a vote individually from each Commissioner present. Each Commissioner shall, when called upon, declare their vote orally by stating "Aye" or "Yes" if voting in favor of the motion being considered. "Nay" or "No" if voting in opposition of the motion being considered, or "I abstain from voting" due to When a roll call vote is necessary, each Commissioner's vote shall be recorded individually in the official minutes of the meeting.

G. SUSPENSION OF RULES

1. Non-Exclusive Rules. The rules set forth are not exclusive and do not limit the inherent power or general legal authority of the Commission, or its Chairperson,

to govern the conduct of Commission meetings as may be considered appropriate from time to time or in particular circumstances for the purposes of orderly and effective conduct of the affairs of the City.

2. Amendment and adoption of Commission by-laws to supersede those contained herein shall follow the same procedure as the adoption of these by-laws. A motion may be made by any Commissioner to review the by-laws for amendment at any time following their inception. A sustaining vote shall be required to open the review of the by-laws for amendment.
 3. The adoption of these by-laws, and any successors, shall be done only after the publication of a notice of review and adoption of these by-laws in a format(s) consistent with State law within the City at least 10 days prior to the meeting in which the adoption will be considered.
 4. The general public shall have the opportunity to review and comment upon these by-laws prior to the adoption by the Commission.
- H. These Commission by-laws will be superseded by any State of Utah law or City of South Jordan code that are in conflict.

RECORDING OF BY-LAWS.

These by-laws, and all subsequent amendments, shall be recorded by the City Recorder and copies of which shall be distributed to each Commissioner and the Planning Department.

Approved by the City of South Jordan Planning Commission this xx day of month, 2025
first name last name, Planning Commission Chair