



## PEDESTRIAN AND BICYCLE ADVISORY COMMITTEE MEETING AMENDED AGENDA

May 08, 2025 at 11:30 AM

Council Chambers at City Hall - 1123 W. Lake St. Sandpoint, Idaho

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### Call to Order

### Roll Call

### Meeting Minutes Approval

1. Approval of Minutes from the Committee's April 10, 2025, Meeting - **action item**

### Public Comments

**Old/Unfinished Business** - none

### New Business

2. Discussion and Vote for Committee to Adopt Recommendations for Intersection Improvements at Boyer and Highway 2 - **action item**
3. Discussion and Vote for Committee to Adopt Recommendations Related to Various Ongoing Private Development Projects - **action item**
4. Discussion and Vote for Committee to Adopt Recommendations Related to Phase III of the Downtown Revitalization Project – **action item**
5. Discuss Fourth Avenue Sidewalk Project
6. Discuss North Division Crosswalk Location
7. Staff Liaison Private Development Update

### General Announcements/Comments

### Adjourn

### **Public Participation Options and Information**

*Before the meeting, comment in writing: Email [cityclerk@sandpointidaho.gov](mailto:cityclerk@sandpointidaho.gov) or deliver to City Hall.*

*Attend in person: See above for meeting location. Seating available on first-come, first-served basis.*

*Attend remotely: Register at <https://www.sandpointidaho.gov/meetings>.*

*After the meeting, view the recording on YouTube: <https://www.youtube.com/c/CityofSandpoint>*

*For questions or requests for special accommodation: At least 48 hours prior to the meeting, send a message to the email address above or call (208) 263-3310.*



## PEDESTRIAN AND BICYCLE ADVISORY COMMITTEE MEETING MINUTES

April 10, 2025 at 11:30 AM

Council Chambers at City Hall - 1123 W. Lake St. Sandpoint, Idaho

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### Call to Order

Chair Cate Huisman called the meeting of the Pedestrian and Bicycle Advisory Committee Meeting to order at 11:37 a.m. on Thursday, April 10, 2025, in Council Chambers at City Hall, 1123 W Lake Street, Sandpoint, Idaho.

### Roll Call

#### PRESENT

Cate Huisman, Chair  
 Molly O'Reilly, Vice Chair  
 Erin Billings (*late arrival*)  
 Jennifer Heller  
 Julie Perchynski  
 Radley Peterson

#### ABSENT

Evan Lewis  
 Reid Weber

Also present were City Council liaison Deb Ruehle, staff liaison Brandon Staglund, board clerk Samantha Branscome, and Deputy City Clerk Hayley Keys.

### Meeting Minutes Approval

1. The minutes from the Committee's March 13, 2025, meeting were approved as presented.

Motion made by Heller, Seconded by Peterson.

Voting Yea: Huisman, Heller, Perchynski, O'Reilly, Peterson

### Public Comments

No members of the public were present.

### Old/Unfinished Business

2. Mr. Staglund spoke briefly about the process of selecting a presiding officer at a committee meeting if the Committee Chair and Vice Chair are absent, with an understanding that if neither the Chair nor Vice Chair is physically present, the Board members select a member in the meeting room to preside over that meeting.

### New Business

3. Chair Huisman introduced and welcomed the new Committee Clerk Samantha Branscome.
4. Events Subcommittee Report

Chair Huisman shared plans for two events in May and gauged interest in members' volunteering for one or both events. The Pedestrian and Bicycle Advisory Committee plans to

staff a tent at the Bike/Walk to School Day event at the Kaniksu Land Trust Office Building on May 7 and to participate in the Farmin School Bike Bus at Hickory Park Gazebo on May 9.

5. Intersection Subcommittee Report

Ms. Perchynski reported on the Highway 2, Division, and Boyer Intersection. The Committee agreed to review the subcommittee's recommendations and bring to a vote at the next meeting.

6. Multimodal Transportation Master Plan Subcommittee Report

Mr. Peterson reported a few high-level ideas on the City's Phase III plan. The Committee agreed to review the subcommittee's recommendations and bring to a vote at the next meeting.

7. Development Review Subcommittee Report

Ms. Heller reported that the Development Review Subcommittee has not met and had no updates to share.

8. Staff Presentation: Division Avenue Corridor Improvements, Phase II

Mr. Staglund provided an overview of the City's plans to implement Phase II of Division Ave. corridor Improvements. The Committee plans to revisit this item at their next meeting to provide feedback to Mr. Staglund.

9. Discuss Great Northern Commerce Park Subdivision Preliminary Plat

Mr. Staglund presented the proposed Great Northern Commerce Park Subdivision. The Committee delegated further discussion to the Development Review Subcommittee and plan to discuss this again at their next meeting.

**General Announcements/Comments**

Mr. Staglund presented a site plan for the proposed Farmin Flats development on Church Street and discussed parking requirements.

Mr. Peterson asked the Committee to revisit the Fourth Avenue agenda item at the next meeting.

**Adjourn**

With no further business before the Committee, the meeting adjourned at 12:57 p.m.

I presided over this meeting and can confirm that these minutes, prepared by the board clerk, were approved by the Committee during their meeting on \_\_\_\_\_, 2025.

\_\_\_\_\_  
Cate Huisman, Board Chair

\_\_\_\_\_  
Attest: Samantha Branscome, Board Clerk

**Intersection Subcommittee recommendations to the SPBAC for approval and forwarding to the City:**

Recommendations from the Ped-Bike Committee to the council and mayor re Hwy 2 x Boyer:

*Short term:*

- Repaint crosswalk on Hwy 2 between Smokesmith and Dub's
- Provide orange flags for pedestrians (like the ones on 5th Ave.)
- Install mid-street pedestals like the ones on 5th at Poplar (in summer)
- Post a "Watch for Pedestrians" sign on Boyer which flashes when Walk is activated southbound. Have a non flashing "Watch for Pedestrians" sign north of Pine on Boyer, southbound.
- Change existing northbound "watch for pedestrians" sign on Boyer to flashing when activated.
- All left turns at the intersection be Protected only

*Long term:*

- Move the crosswalk between Smokesmith and Dub's to the intersection corners. (A more diagonal crossing, but visible and safer.)
- Move southeast crosswalk across Highway to the intersection corners. (A more diagonal crossing, but visible and safer.)
- Consider 7-second lead time for pedestrian crossing light
- Consider in-pavement lighting

*Also:*

Remove 35 mph posted speed limit signs from Division to Boyer on Highway 2 (these are wrong) and replace them with signs that say 25 mph (this is correct speed limit).

## Notes from Intersection Subcommittee meeting 4/24/25

### Intersection Subcommittee recommendations to the SPBAC for approval and forwarding to the City:

Recommendations from the Ped-Bike Committee to the council and mayor re Hwy 2 x Boyer:

#### *Short term:*

- Repaint crosswalk on Hwy 2 between Smokesmith and Dub's
- Provide orange flags for pedestrians (like the ones on 5th Ave.)
- Install mid-street pedestals like the ones on 5th at Poplar (in summer)
- Post a "Watch for Pedestrians" sign on Boyer which flashes when Walk is activated southbound. Have a non flashing "Watch for Pedestrians" sign north of Pine on Boyer, southbound.
- Change existing northbound "watch for pedestrians" sign on Boyer to flashing when activated.
- All left turns at the intersection be Protected only

#### *Long term:*

- Move the crosswalk between Smokesmith and Dub's to the intersection corners. (A more diagonal crossing, but visible and safer.)
- Move southeast crosswalk across Highway to the intersection corners. (A more diagonal crossing, but visible and safer.)
- Consider 7-second lead time for pedestrian crossing light
- Consider in-pavement lighting

#### *Also:*

Remove 35 mph posted speed limit signs from Division to Boyer on Highway 2 (these are wrong) and replace them with signs that say 25 mph (this is correct speed limit).

In future the subcommittee will examine other signalized intersections for elements of safety for pedestrians and cyclists crossing.

Subcommittee will examine Division and Superior intersection which today is without provision for junior high students to cross Division Ave. safely and accessibly. Students come pouring out and today it is not safe.

Developments evaluated for SPBAC Review and Comment – 4/23 Subcommittee Mtg

Notes

PROJECT	Location/Brief Description	Subcommittee Recommendations to Full SPBAC	Full Committee Action/Date	Potential Ped-Bike issues with this project (original subcommittee discussion notes)	Potential code issues raised (original subcommittee discussion)
PSPR25-0003 Farmin Flats Building 1		Request developer ensure full, well lit, pedestrian access to living units from front sidewalk. For Phase 1 specifically, consider pedestrian access from sidewalk not through the commercial area. Access needs to be and to feel safe day and night. -Bike parking: inside the building, first floor, lockable to building, easily accessible with an outside entry and inside entry. Separate from trash. 1 space per living unit (many people without cars have multiple bikes), and 1 in 5 spaces extra wide to accommodate e-bikes. Consider extra lockers or storage and wide enough doorway entry/access for paniers, baskets, and bike trailers.		Pedestrian access from sidewalk on Phase 1 building is through commercial space. Only when a business is open? Otherwise, have to enter from alley which can be dark and muddy. Will there be adequate (but not garish) lighting for pedestrians in the "canyon" between the two buildings? Will there be secure bike parking required in the interior trash area or under stairs of Phase 1 building, like in Phase 2 plans? How many bike parking spaces? Is the exterior bike parking currently visible outside Phase 1 building just for customers/visitors of commercial space?	
PSPR25-0005 Farmin Flats Building 2	Church St. downtown, two commercial spaces; many small living units without vehicular parking, thus making walk/bike essential.				We can't locate a requirement for bike parking in a development such as this (bike parking is currently tied to parking lot requirements). Nor a requirement that pedestrians have unfettered access to their dwellings from sidewalk.
Brewery on North Boyer	Adding a 2nd industrial building to a 2008 project.	5th document is original "N Boyer Building Permit Review." It has a full paragraph on "Sidewalks" (Page 3, section 5) which spells out sidewalk requirement and the previous PAC's recommendations for ped access. Since this project seems to be continuing off 2008 approvals and plans, we would like to see this paragraph implemented at this time.		Current activity on the plans is not indicating that sidewalk requirement is being upheld. A sidewalk requirement is visible in original 2008 permit review. We would like to see that upheld now that the project is continuing.	West side of N. Boyer is seeing neglect in sidewalk construction (city accepting "in lieu funds" instead). In lieu funds are allowed by code, but we are not certain it is the preferable option. Full pathway/sidewalk was a high priority of city council for at least a decade. Will explore.
PSPR24-0003 Boyer Meadows Industrial Park	Mountain View Dr. off N. Boyer; a number of industrial/commercial spaces in a series of buildings. First phase.	Recommend inclusion of bike parking in plans.		Where is bike parking? This plan has more than 20 cars, though non-contiguous Maybe require that bike parking be shown for approval? Convenient to each building?	Bike parking is required if a nonresident lot holds more than 20 cars. Is that adequate?
PSPR24-0001 Milbrath Multi-Family	Lake St. and Florence Ave.; three buildings, one with 5 townhouse units, 2 duplexes. On the Dover Bike Path and highway 2.	Ensure robust buffer to bike path from shared driveway, with a paved walk/bike entry to/from the bike path from each unit and the driveway. It appears trash area is located adjacent to pathway, and it needs to be moved or well buffered. Need to show doorway connections to frontage street sidewalks -- each unit in building 3 has only driveway and pathway access. Make the latter access robust and direct. Why is sidewalk at S end of Florence connection right on the curb? Snow storage will be a problem and restrict access to bike path. (Pp. 9, 10) Minimize driveway width on Florence for traffic calming. Add amber LED lights at intersections for better visibility, reduced glare, and Dark Sky compliance.		Proposed shared driveway in center of development: ensure robust buffer to bike path, with a paved walk/bike entry to/from the bike path. It appears trash area is located adjacent to pathway (city staff have already remarked on this and tagged for redesign and resubmit in Nov 2024 - see document "Initial Review Comments"). Need to show doorway connections to frontage street sidewalks. E unit in building 3 has only driveway and pathway access. Make the latter robust and direct. Why is sidewalk at S end of Florence connection right on the curb? Snow storage will be a problem there and restrict access to bike path. (Pp. 9, 10) Minimize driveway width on Florence for traffic calming. When walking this area, there are currently no street lights at the intersections.	
<b>CODE AMENDMENT</b>					
Amendments to City Code 10-2-3-D ar	Proposed plan amendment by city regarding sidewalks and small plats.	Holding recommendations while drafting continues (our current understanding of project - Brandon can advise at mtg). First version was already presented to P&Z and approved, could go before council at any time? We would greatly appreciate having a presentation of this at the general committee.		This is a proposed code amendment to the requirement that short plats be required to provide infrastructure improvements, including sidewalks. We want to understand and have opportunity to comment on this.	May result in more spotty sidewalk (or no sidewalk at all in certain neighborhoods). Even isolated chunks of sidewalk get used! Also, hard to retrofit sidewalk after landscaping has happened.

### PSPR25-0003 Farmin Flats Buildings 1 & 2 Now Downtown Church Street Multifamily)

- Request developer ensure full, well lit, pedestrian access to living units from front sidewalk. For Phase 1 specifically, consider pedestrian access from sidewalk not through the commercial area. Access needs to be and to feel safe day and night.
- Bike parking recommendations: add bike parking to building 1 similar to what is visible in building 2 site plans. Optimal bike parking would be inside the building, first floor, lockable to building, separate from trash space, and easily accessible with an outside entry and inside entry. Recommend offering 1 bike space in these parking areas per living unit (many people without cars have multiple bikes); and, furthermore, ensure that 1 in 5 spaces are extra wide to accommodate e-bikes. Encourage space for lockers or storage in bike parking areas, and wide enough doorway entry/access for ancillaries like panniers, baskets, and bike trailers.

### Brewery on North Boyer

- On city's website, 5th document posted is original "N Boyer Building Permit Review." This document has a full paragraph on "Sidewalks" (Page 3, section 5), which spells out Boyer sidewalk requirement and the previous PAC's recommendations for pedestrian access. Since this project seems to be continuing off 2008 approvals and plans, the Bike-Ped Committee would like to see this paragraph implemented and sidewalks built on west side of Boyer now that the project is resuming.

### PSPR24-0003 Boyer Meadows Industrial Park

- Recommend inclusion of bike parking in plans, as this site plan seems to have total parking capacity of more than 20 cars.

### PSPR24-0001 Milbrath Multi-Family *(currently in re-design process?)*

- Ensure robust buffer to bike path from shared driveway, with a paved walk/bike entry to/from the bike path from each unit and the driveway.
- It appears trash area is located adjacent to Dover public pathway. It needs to be moved or well-buffered.
- Need to show doorway connections to frontage street sidewalks -- each unit in building 3 only has driveway and pathway access. Make the latter access robust and direct.
- Sidewalk at south end of Florence connects directly to curb and looks to be a snow storage issue (which will turn into a bike path access issue in certain seasons). Redesign recommended. (Pp. 9, 10)
- Minimize driveway width on Florence for traffic calming.
- Add amber LED lights at intersections for better visibility, reduced glare, and Dark Sky compliance.

### GN Commerce Subdivision - 1266 Woodland Dr

- Require curb ramps in both directions at south side of corner of Woodland and new N/S road.
- Require curb ramps in both directions at two internal T intersections, with the ability to cross the N/S and E/W streets with ramps at the corner and across the street without a corner.
- Provide a minimum 6' wide pedestrian/bicycle connection to the new trunk roadway in the phase two subdivision between lots 6 and 7 in phase 1 and proposed lots 20-21 in phase two, or a nearby couplet.
- Require a 5' planting strip/snow storage between roadway and sidewalk.
- After construction on any lot, which will include driveway cut, ensure sidewalk still meets all city

standards and require repair if not.

- Require buildings have direct connections to sidewalks, not in driveways.
- Require bicycle parking at each building, convenient to main entrance (or inside for employees) not allowing parked bikes to intrude on pedestrian access.

## Proposal for Phase 3 Priorities – Downtown Revitalization Project

Submitted by: Multimodal Transit Master Plan Subcommittee

To: Pedestrian and Bicycle Advisory Committee

Date: 05/08/2025

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### Background

The Multimodal Transit Master Plan (MTMP) Subcommittee of the Pedestrian and Bicycle Advisory Committee (PBAC) has developed a set of priorities for Phase 3 of the Downtown Revitalization Project. These priorities are intended to improve safety, accessibility, and multimodal connectivity in downtown Sandpoint.

Upon approval by PBAC, these recommendations will be formally presented to the Mayor for consideration and potential forwarding to City Staff, contractors, and the City Council as appropriate.

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### Proposed Priorities for Phase 3 Implementation

1. **Safe Multimodal Intersections:** Ensure that intersections safely accommodate bicycle and pedestrian traffic in all directions.
2. **Pilot Temporary Solutions:** Test temporary accommodations for bicycles and pedestrians where feasible before committing permanent resources.
3. **Dedicated Bicycle Lanes:** Install dedicated bicycle lanes in both directions downtown to promote safer and more predictable bike travel.

**Separation of Modes:** Whenever possible, create physical barriers between bicyclists and vehicular traffic to enhance safety.

4. **High ROI Enhancements:** Prioritize improvements that deliver significant benefits for minimal investment, such as lane painting and signage.
5. **Connection to Existing Routes:** Tie new downtown bicycle routes into the existing Oak Street bicycle corridor to create a cohesive network.
6. **Roundabout at 1st & Superior:** Consider installing a single-lane roundabout at the intersection of 1st Avenue and Superior Street, with pedestrian-activated flashing beacons on the north, east, south, and west crossings.
7. **Dedicated Lanes for Each Mode:** Create separate, dedicated lanes for cars, bicycles, and pedestrians wherever feasible.
8. **Tight Turning Radii and accessible crossings:** Strive for consistent curb radii of approximately 8 feet at intersections, aligning with recommendations from the Intersection Committee, to slow vehicle turns and improve pedestrian safety. Install accessible curb ramps in both directions at each corner and at T intersections. People on

foot should not be required to cross traffic multiple times when they want a straightforward single crossing.

9. **Speed Reduction Measures:** Implement traffic calming measures to reduce vehicle speeds downtown, particularly during the summer peak season. Temporary/seasonal measures such as speed bumps should be considered.
10. **Trail and Pathway Connectivity:** Focus on linking existing trails, sidewalks, and bike paths to facilitate continuous non-vehicle access between key destinations including the Long Bridge, City Beach, and the Sand Creek Trail.

# Division Avenue Road Safety Audit

Prepared for:  
Sandpoint, Idaho

December 14, 2020

UT20-2220

FEHR  PEERS

# Table of Contents

---

- Project Description ..... 1
- Data Collection ..... 1
  - Field Review..... 1
  - Crash Data ..... 4
- Summary of Observations..... 8
  - Additional Observations ..... 19
- Summary of Recommendations ..... 21
  - Easy/Short-Term Mitigations ..... 21
  - Medium/Medium-Term Mitigations ..... 23
  - Hard/Long-Term Mitigation ..... 25
- Acknowledgements ..... 29
- Next Steps..... 29

## List of Figures

---

Figure 1: RSA location & AADT map. ....	2
Figure 2: RSA location & commercial truck traffic map. ....	3
Figure 3: Crashes within the RSA study area by severity. ....	5
Figure 4: Crashes within the RSA study area by type. ....	6
Figure 5: Crash density within the RSA study area.....	7
Figure 6: Key issues and observations within the RSA study area.....	20
Figure 7: School Speed Limit Signs with Flashing Beacons.....	22
Figure 8: Recommended spot improvements within the RSA study area.....	24
Figure 9: Proposed Division Avenue cross section near intersections.....	26
Figure 10: Proposed Division Avenue cross section mid-block (away from intersections). ....	28p

## List of Observations

---

Observation 1: Significant traffic peaks at school start and end times (Photo: Fehr & Peers).....	8
Observation 2: Jaywalking between school and church parking lot (Photo: Fehr & Peers). ....	9
Observation 3: Non-ADA-compliant pedestrian ramps and crosswalks (Photo: Fehr & Peers).....	10
Observation 4: School zone signs posted for 20mph with low compliance (Photo: Fehr & Peers). ....	11
Observation 5: Long northbound queues at Division Avenue & Pine Street during school closing peak (Photo: Fehr & Peers).....	12
Observation 6: Street lighting observed to be sparse for pedestrians. (Photo: Fehr & Peers). ....	13
Observation 7: Sight distance obstructed at Division Avenue & Church Street (Street view image: Google Earth, September 2013).....	14
Observation 8: Substandard bus stop (Photo: Fehr & Peers). ....	15
Observation 9: Nonstandard signage near Sandpoint High School (Photo: Fehr & Peers).....	16
Observation 10: Power pole in the center of sidewalk (Photo: Fehr & Peers). ....	17
Observation 11: RRFB at Division Avenue & Spruce Street (Photo: Fehr & Peers). ....	18

## List of Tables

---

Table 1: RSA Area & Statewide Crash Data Summary .....	4
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# Project Description

Fehr & Peers performed a Road Safety Audit (RSA) along Division Avenue from Michigan Street to Cedar Street in Sandpoint, Idaho as requested by the City of Sandpoint and the Local Highway Technical Assistance Council (LHTAC). **Figure 1** shows the RSA location map.

## Data Collection

### Field Review

Prior to observing traffic conditions Fehr & Peers gathered contextual data about the study area. The data included roadway speeds, Average Annual Daily Traffic (AADT), commercial truck traffic, number of travel lanes, intersection traffic controls, the presence of pedestrian and bicycle facilities, and the types of nearby land uses (e.g. business districts, schools, regional growth centers, and mixed use centers).

**Figure 1** outlines the 2019 AADT reported by ITD within the RSA study area and **Figure 2** outlines the commercial truck traffic as a percentage that AADT.

Fehr & Peers visited the site to observe conditions during various times of the day. Fehr & Peers staff were joined by the following RSA team members:

- Amanda Wilson, City of Sandpoint Infrastructure and Development Services,
- Bruce Robertson, City of Sandpoint Infrastructure and Development Services,
- Corey Coon, City of Sandpoint Police Department,
- Steve Klatt, Bonner County,
- Brian Wright, LHTAC,
- Erin Billings, Lake Pend Oreille School District,
- Matt Diel, Lake Pend Oreille School District.

The study area was observed during the following times:

- School opening and morning commuter peak period (7:30 – 9:00 AM)
- School closing peak period (2:00 – 3:30 PM)
- Evening commuter peak period (5:30 – 6:30 PM)
- Nighttime off-peak period (8:30 – 9:30 PM)

As observed by the RSA team, traffic peaked during the 15 minutes immediately before the high school opened for the day from 7:45-8:00 AM, and again during the 15 minutes immediately after the high school closed for the day from 2:30-2:45 PM. Evening commuter traffic appeared more evenly dispersed than the school opening/closing peaks but was still observed to peak from about 5:30-5:45 PM. Minimal traffic was observed during the nighttime visit, and no peaking characteristics were observed during that period.

# DIVISION AVE. RSA • ANNUAL AVERAGE DAILY TRAFFIC (AADT)

Item # 6.

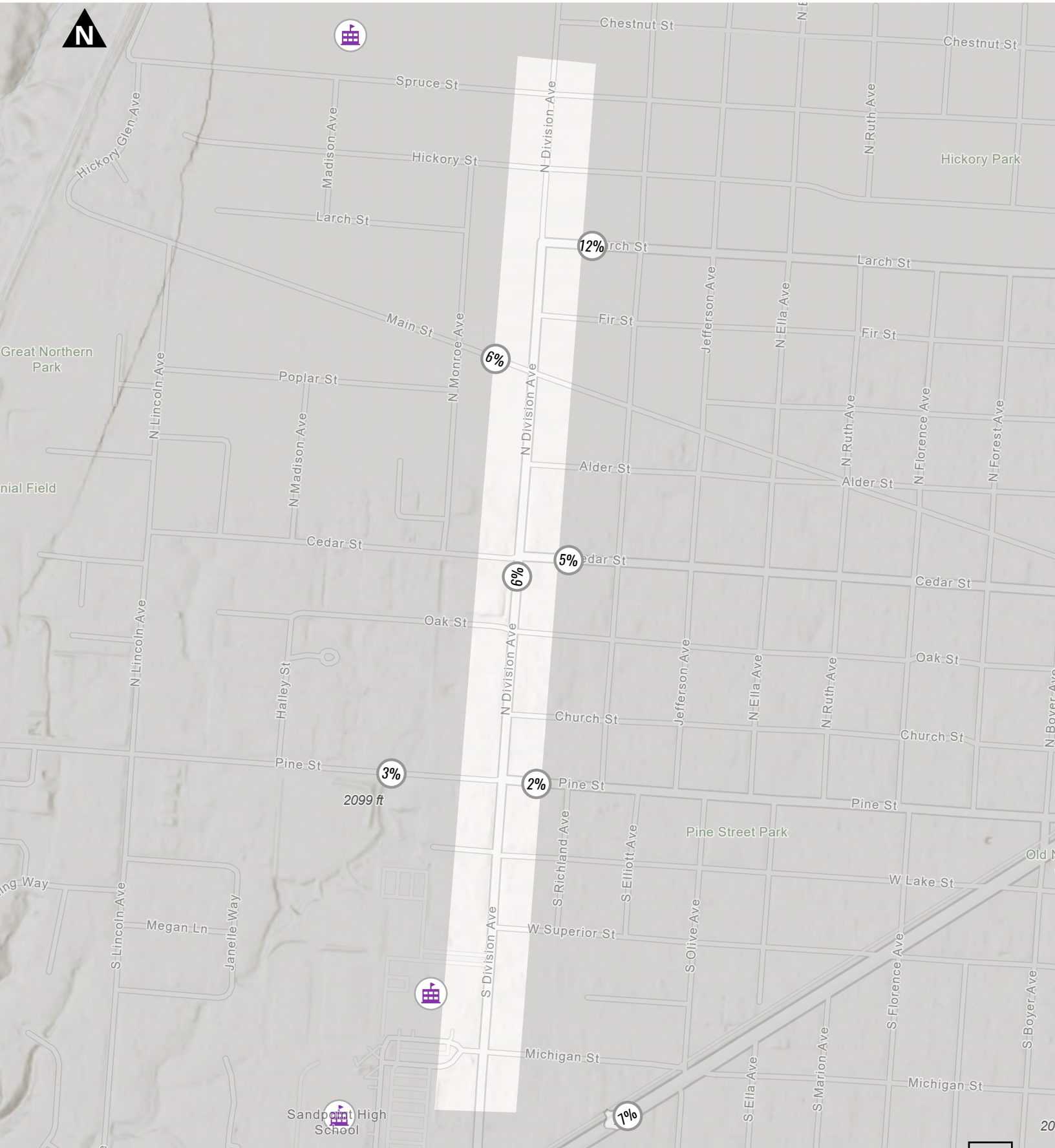


0 0.13 0.25 Miles

Figure 1

# DIVISION AVE. RSA • TRUCK TRAFFIC AS A PERCENT OF AADT

Item # 6.



0 0.13 0.25 Miles

Figure 2

# Crash Data

Fehr & Peers reviewed the available crash data from at 2015-2019 from the LHTAC interactive crash map and the ITD Safety Dashboard. The data was compiled to summarize trends, frequency, and severity of collisions along the study corridor. **Table 1** includes a summary of the crash data for the five study years.

**Figure 3** shows a map of the crashes within the RSA study area by severity, **Figure 4** shows a map of the crashes within the RSA study area by type, and **Figure 5** shows a heatmap of the crashes in the study area to indicate crash density.

**Table 1: RSA Area & Statewide Crash Data Summary**

Crash Statistic	Division Avenue		Statewide	
	Crashes	Percent of Crashes	Crashes	Percent of Crashes
<b>Severity</b>				
Property damage only crashes	26	79%	80,151	63%
Possible injury/complaint crashes	0	0%	25,412	20%
Suspected minor/visible injury crashes	3	10%	15,133	12%
Suspected serious injury crashes	4	12%	5,060	4%
Fatal injury crashes	0	0%	1,069	1%
<b>Lighting Conditions</b>				
Day	28	85%	88,382	70%
Dawn or Dusk	0	0%	5,774	5%
Dark, No Street Lights	1	3%	17,429	14%
Dark, Street Lights On	4	12%	14,245	11%
Dark, Street Lights Off	0	0%	828	1%
<b>Crash Type*</b>				
Angle & Angle Turning	19	58%	26,241	21%
Head-on & Head-on Turning	2	6%	7,227	6%
Pedestrian	2	6%	1,149	1%
Rear-end	5	15%	34,124	27%
Side Swipe Opposite & Side Swipe Same	5	15%	12,196	10%

\* **Table 1** only reports crashes by type that occurred within RSA area. Other crash types occur in Idaho that are not reported here. Source: LHTAC Idaho Crash Data Map, ITD Highway Safety Dashboard.

# DIVISION AVE. RSA • CRASHES

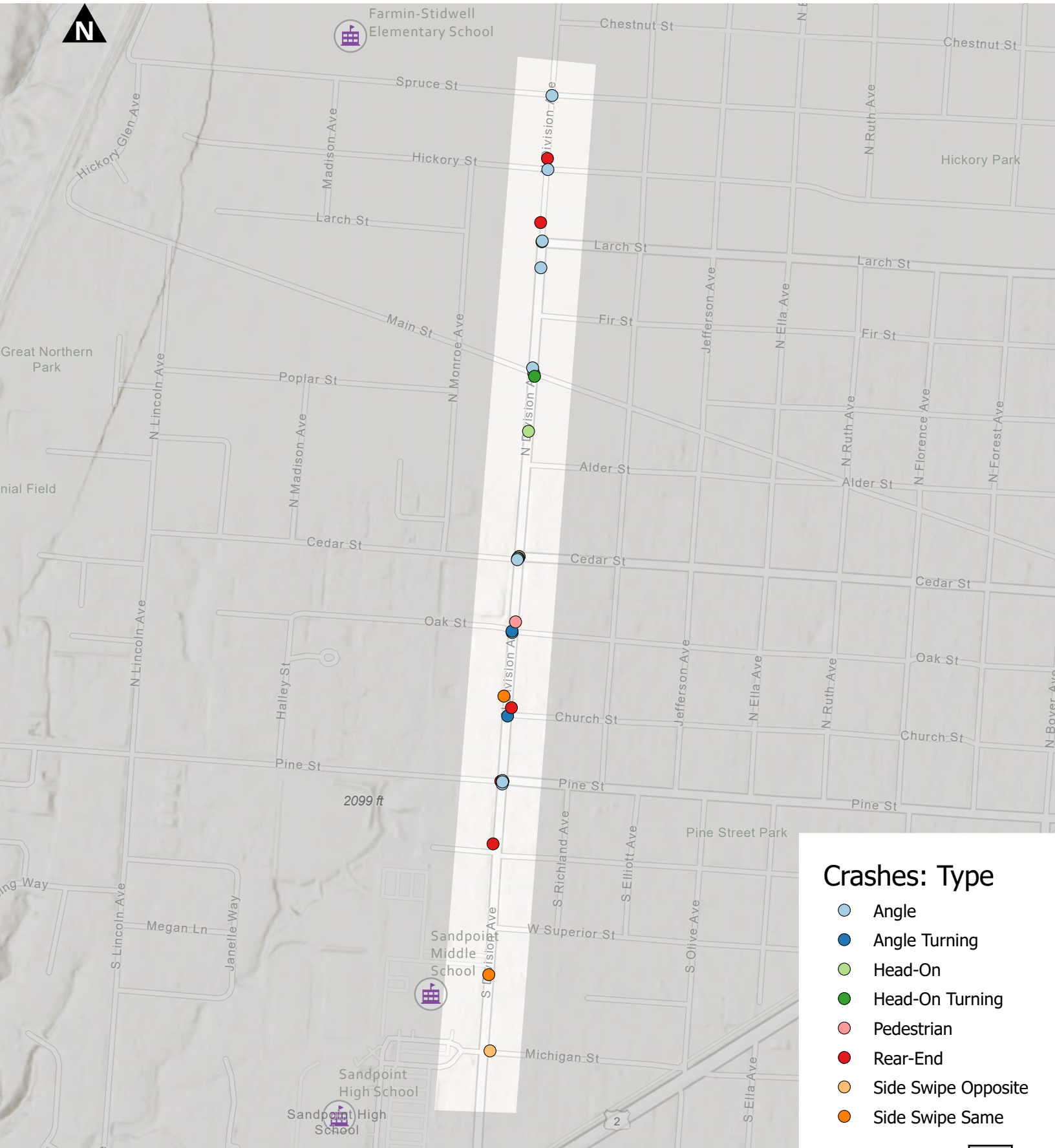
Item # 6.



0 0.13 0.25 Miles

# DIVISION AVE. RSA • CRASHES

Item # 6.



0 0.13 0.25 Miles

# DIVISION AVE. RSA • CRASHES

Item # 6.



**Crash Density**  
Sparse  
Dense

Figure 5

# Summary of Observations

Based on the traffic patterns observed during the field visit and the data gathered from LHTAC and ITD, the RSA team made the following observations:



Observation 1: Significant traffic peaks at school start and end times (Photo: Fehr & Peers).

Traffic was observed to exhibit significant peaking characteristics at school start & end times. The City has made incremental improvements to this area to improve safety and address citizen complaints.

During the school start peak period, a trained crossing guard closes the southbound left turn lane on Division Avenue and directs traffic at Michigan Street. The crossing guard was observed to be effective in managing both vehicle and pedestrian traffic to reduce pedestrian/vehicle conflicts during the AM peak.

No additional traffic or pedestrian control was observed during the school closing peak.



Observation 2: Jaywalking between school and church parking lot (Photo: Fehr & Peers).

Jaywalking was occasionally observed between the high school and the church parking lot on the other side of Division Avenue. As shown in Observation 2, many parents use the church parking lot as a pick-up/drop-off parking lot for the school. The jaywalking at this location was observed to be more frequent during the school closing peak, likely because the crossing guard provided additional protection to pedestrians in the school opening peak.



Observation 3: Non-ADA-compliant pedestrian ramps and crosswalks (Photo: Fehr & Peers).

At the Division Avenue intersections at Michigan Street, Superior Street, and Pine Street, the RSA team observed that pedestrian ramps and crosswalks were not ADA compliant. More specifically, the team observed that the crosswalks did not properly align with the pedestrian ramps. Furthermore, the truncated domes used at the Pine Street intersection were not installed correctly, and the raised curb in the middle of the pedestrian ramps creates an additional hazard for pedestrians.



Observation 4: School zone signs posted for 20mph with low compliance (Photo: Fehr & Peers).

The RSA team observed that school zone signs along Division were posted to reduce the speed limit to 20mph. The speed limit along the rest of Division Avenue is 25mph, so the school zone only reduces the speed limit by 5mph. Furthermore, the school zone speed limit is active from 7:30 AM to 3:30 PM, even during periods when the school is not starting or ending. The law enforcement representative on the RSA team noted that the minor reduction in speed limit and broad time that the school speed limit is in effect has resulted in low compliance, but the increased police presence has helped.



Observation 5: Long northbound queues at Division Avenue & Pine Street during school closing peak (Photo: Fehr & Peers).

Northbound left-turning vehicles were observed driving along the two-way left-turn lane along Division Avenue far in advance of the intersection at Pine Street to illegally get around northbound through/right-turn queue. Law enforcement staff on the RSA team reported that this has caused crashes at this location.



Observation 6: Street lighting observed to be sparse for pedestrians. (Photo: Fehr & Peers).

Division Avenue was not observed to be continuously lit at night. Crosswalks were lit from one corner of each intersection. However, lighting was observed to be sparse in some sections, especially for pedestrians and cyclists on the multi-use path along the west side of Division Avenue.



Observation 7: Sight distance obstructed at Division Avenue & Church Street (Street view image: Google Earth, September 2013).

The RSA team observed that vegetation and a light pole at Church Street obstructs the westbound sight distance. The RSA team also observed several instances of low-hanging tree branches over sidewalks occasionally obstructing the pedestrian walkway. Low-hanging branches were observed along Division Avenue near Alder Street, Church Street, Cedar Street, and Pine Street.



Observation 8: Substandard bus stop (Photo: Fehr & Peers).

The northbound bus stops on Division Avenue were observed to lack pedestrian amenities such as waiting areas, bus shelters and benches.



Observation 9: Nonstandard signage near Sandpoint High School (Photo: Fehr & Peers).

As shown in the Observation 9 photo, the driveway north of the middle school uses nonstandard signs to indicate which driveway is enter only, and which is exit only. These signs may be confusing to drivers since the STOP EXIT ONLY DO NOT ENTER sign is directly adjacent to the ENTER ONLY sign.



Observation 10: Power pole in the center of sidewalk at Division Avenue & Fir Street (Photo: Fehr & Peers).

As shown in Observation 10, a power pole is placed in the center of the sidewalk on the northeast corner of the intersection at Division Avenue & Fir Street. This pole is obstructing the walkway and is preventing the sidewalk from being ADA compliant. Similarly, a pole obstructs the sidewalk the northeast corner of the intersection at Division Avenue & Larch Street.



Observation 11: RRFB at Division Avenue & Spruce Street (Photo: Fehr & Peers).

The City has implemented an RRFB crossing at Spruce which was observed to be effective at managing both vehicle and pedestrian traffic to reduce pedestrian/vehicle conflicts during all observed times of the day. The RRFB was observed to operate well and drivers were observed to comply with the sign during the team's field visits.

## Additional Observations

The RSA team made four additional general observations:

1. Snow buildup along the sidewalk on the east side of Division Avenue has been reported to be an issue due to the lack of snow storage and lack of snow removal. Sandpoint City staff has indicated that a 5 feet minimum park strip buffer between the travel lanes and the sidewalk/multi-use path would be preferred for snow storage.
  - a. It should be noted that the City was reported to successfully keep travel lanes in the road clear of snow along Division Avenue. The multi-use path was also reported to be maintained during snowy months; however, snow storage continues to be a concern along the multi-use path. Implementing the suggestions recommended in the following section will likely improve the conditions of the path and the road during snowy months.
2. Division Avenue is currently designated as a truck route and should continue to accommodate trucks until a different truck route is established.
3. The multi-use path along Division is 8 feet wide, whereas 10 feet is the preferred minimum for a bidirectional multi-use path. Staff also noted that gravel and sand buildup was common along the multi-use path.
4. The reflectivity of current street signs at night appeared to be adequate along Division Avenue.
5. Division Avenue is striped with three-lanes from Michigan Street to Pine Street. The rest of Division Avenue is striped with two-lanes but maintains the same cross-sectional width. In the segments with only two lanes, the lane widths increase to 17 feet. The wider lanes encourage speeding.
6. The RSA team observed that most cyclists avoided the bike lanes along Division Avenue and instead used the multi-use path. Cyclists were also occasionally observed biking northbound in the southbound bike lane to get around other users in the multi-use path. The bike lanes are currently 5 feet wide and utilize the gutter to accomplish this width. However, the AASHTO Guide for the Development of Bicycle Facilities state that “the gutter should not be included in the measurement as usable width, as bicyclists will typically ride well to the left of the joint,” (AASHTO, 2012)

**Figure 6** outlines the key issues and their locations along Division Avenue that were mentioned in this section.

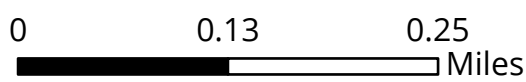
# DIVISION AVE. RSA • KEY ISSUES

Item # 6.



## Key Issues

- 1. Crosswalks do not align with ped ramps.
- 2. NW corner has "desire line" worn in landscaping.
- 3. Crossing guard at Michigan St is required for safe operation. Only available in AM.
- 4. Occasional jaywalking between school and church parking lot. More frequent in PM.
- 5. Vehicles parked on SE corner may obstruct WB LT sight distance.
- 6. Ped ramps & crosswalks are not ADA compliant.
- 7. Cars use NB left turn lane to skip RT queue. Has caused crashes.
- 8. Ped ramps & crosswalks are not ADA compliant.
- 9. Crosswalk on north side is not lit sufficiently at night
- 10. Vegetation obstructs WB sight distance.
- 11. Vegetation & pole obstructs WB sight distance.
- 12. Low hanging branches over sidewalk.
- 13. Low hanging branches over sidewalk.
- 14. Low hanging branches over sidewalk.
- 15. Substandard bus stop.
- 16. Pole placed in center of sidewalk. Not ADA compliant.
- 17. School zone sign is 5mph from the posted speed limit 25 to 20mph. Low compliance.
- 18. Traffic peaks during school start & end times. Frequent vehicle/ped conflicts.
- 19. School zone sign is 5mph from the posted speed limit 25 to 20mph. Low compliance.
- 20. Nonstandard sign configuration



# Summary of Recommendations

Based on the observations outlined in the previous section, the RSA team has outlined several recommendations organized into three levels of mitigation:

1. Easy/Short-Term Mitigations: signing, striping, lighting
2. Medium/Medium-Term Mitigations: sidewalk, pedestrian ramp improvements (flatwork)
3. Hard/Long-Term Mitigation: roadway reconfiguration

## Easy/Short-Term Mitigations

### **Recommendation 1: Coordinate with Lake Pend Oreille School District to Implement Walking School Bus for Elementary School**

A “walking school bus” program organizes a group of children to walk to school with one or more adults. The goal of walking school bus programs is to reduce the number of vehicles traveling to and from a school and provide a safer walking environment for the elementary aged children. A “walking school bus” program could be implemented and maintained by the school district at the elementary school on Spruce Street to encourage children to walk to school and reduce the number of drop-off and pick-up trips to and from the school during the school opening and closing peaks, respectively. By reducing the number of vehicle trips, the number of pedestrian/vehicle conflicts near elementary school would be reduced. Having an organized “walking school bus” program will also provide parents with a sense of security for their children to safely walk to/from school and the leading adult can walk the children along safer routes or the other side of the street when, for instance, the east side sidewalk on Division Avenue is full of snow.

### **Recommendation 2: Implement Crossing Guard During School Closing Peak Period**

Due to the effectiveness of the crossing guard during the school opening peak period, the City is recommended to coordinate with the school district to implement a similar crossing guard in the school closing peak to provide additional protection to pedestrians and reduce pedestrian/vehicle conflicts.

### **Recommendation 3: Add East-West Crosswalk at Fir Street**

Division Avenue currently has no crosswalks between Cedar Street and Spruce Street. Fehr & Peers recommends adding an east-west crosswalk at Fir Street to improve pedestrian connectivity and to reduce the likelihood of jaywalking in areas without a crosswalk.

### **Recommendation 4: Increase School Zone Sign Visibility, Limit Restrict Time Active**

The school zone speed limit is 20mph (5mph lower than the normal speed limit on Division Avenue) and active from 7:30 AM to 3:30 PM. To increase compliance with the school zone speed limit, the RSA team

recommends adding flashing lights to the school zone speed reduction signs (see **Figure 7** for example); the flashing lights should be either automatically scheduled, or manually activated by a crossing guard. The RSA team also recommends that the school zone speed limit reduction should only active during school opening and closing peak periods, when students are likely to be present. If speeds continue to be an issue during the school opening and closing peak period, then reducing the school zone speed limit to 15mph (10mph lower than the normal speed limit) would be appropriate. Police presence to enforce the speeds should continue to be a priority.

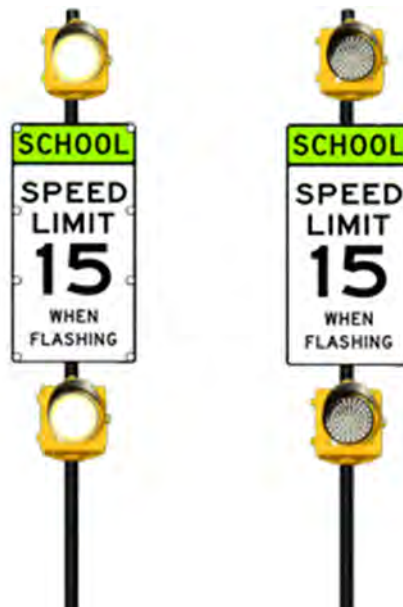


Figure 7: School Speed Limit Signs with Flashing Beacons

**Recommendation 5: Reconfigure Northbound Approach at Division Avenue & Pine Street**

To prevent northbound left-turning vehicles from driving in the two-way left-turn lane along Division Avenue the two-way left-turn striping should end at Lake Street instead of at Pine Street. Standard left north- and southbound left-turn lanes should be striped at Lake Street to provide left turn storage for those movements. Furthermore, the northbound left-turn storage lane at Pine Street should be extended to provide additional queue storage space. If this problem continues, a raised center curb median could be added along the left-turn lane to more fully prohibit this movement from occurring.

**Recommendation 6: Perform a Lighting Inventory and Add Lighting as Needed**

Fehr & Peers recommends performing a lighting study to determine where additional lighting is needed along Division Avenue, especially for pedestrians and cyclists on the multi-use path. The study should aim to provide sufficient lighting for the multi-use path, sidewalk, and crosswalks along Division Avenue.

**Recommendation 7: Remove or Restrict Vegetation in Problem Areas**

Fehr & Peers recommends removing or limiting the vegetation on the northeast corner of Division Avenue & Church Street to prevent the westbound sight distance from being obstructed. Fehr & Peers also recommends managing or limiting the vegetation along the sidewalk and multi-use path to prevent low-hanging tree branches over sidewalks from obstructing the pedestrian walkway.

**Recommendation 8: Relocate or Replace Driveway Signs North of High School**

Fehr & Peers recommends replacing and relocating the nonstandard signs located at the driveway north of the high school. The signs should be replaced with standard MUTCD signs to better indicate which driveway is enter-only and which driveway is exit-only. The sign marked STOP EXIT ONLY DO NOT ENTER is an improper use of a stop sign and should be replaced with an R5-1 (DO NOT ENTER) or R5-1a (WRONG WAY) and relocated closer to the exit-only driveway. The ENTER ONLY post should be relocated to the other side of the entry-only driveway.

**Medium/Medium-Term Mitigations**

**Recommendation 9: Redo the pedestrian ramps**

The pedestrian ramps at Division Avenue & Michigan Street, at Division Avenue & Superior Street, and at Division Avenue & Pine Street are not ADA compliant and should be reconstructed to achieve ADA compliance.

**Recommendation 10: Provide Bus Benches or Bus Shelters at Bus Stops**

Fehr & Peers recommends providing pedestrian amenities such as concrete waiting areas, benches and/or shelters for transit users. The southbound bus stop near the library between Cedar Street and Oak Street currently provides a bus shelter with a bench that could serve as a template for other bus stops along the corridor. Depending on the location of the bus stop, relocating the bus stop poles may also be necessary to adequately provide space for a bench or shelter.

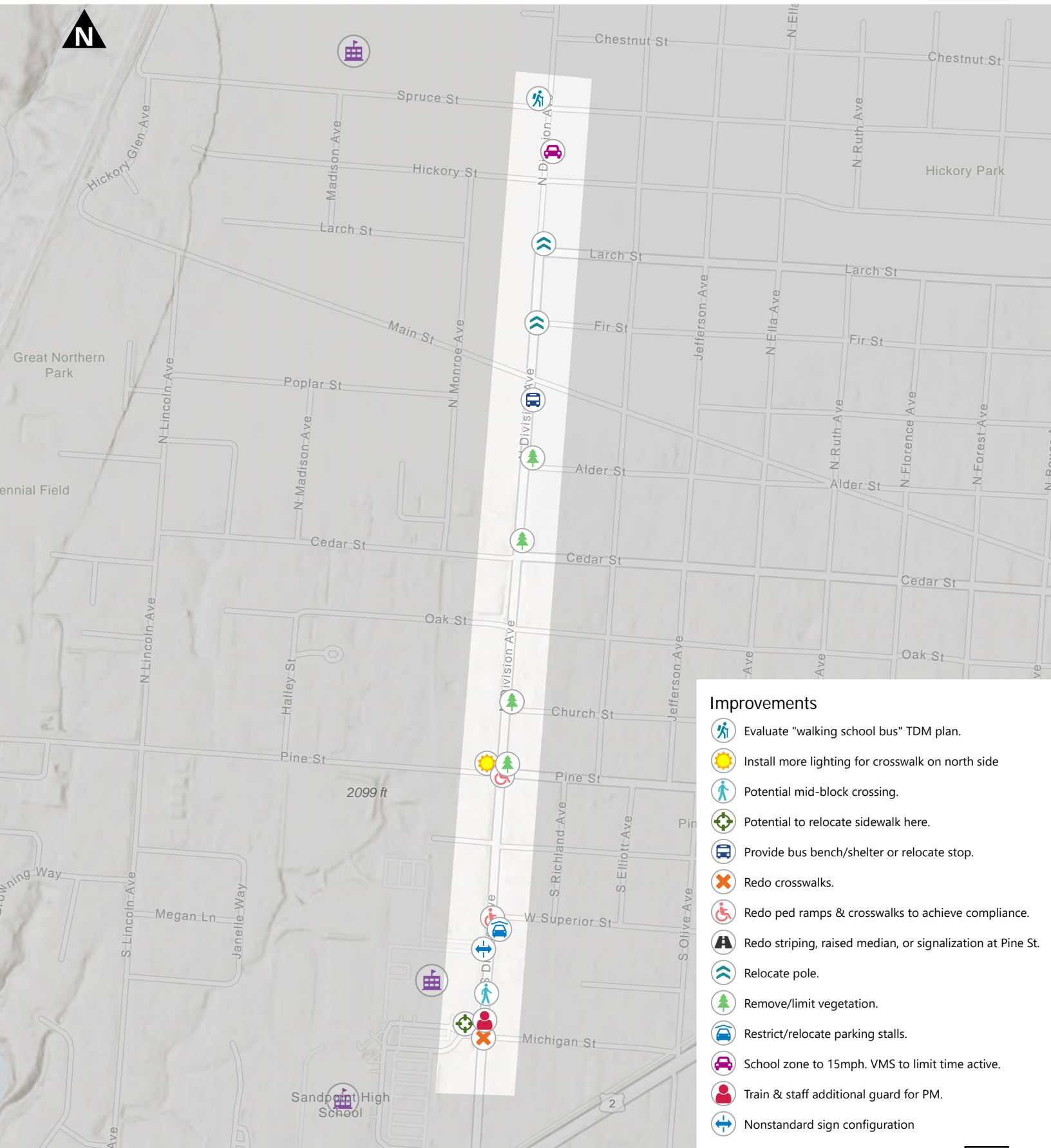
**Recommendation 11: Relocate the Power Poles in the Sidewalk at Division Avenue & Fir Street and at Division Avenue & Larch Street**

The power poles placed in the center of the sidewalk on the northeast corner of the intersections at Division Avenue & Fir Street and at Division Avenue & Larch Street should be relocated to prevent them from blocking the walkway.

**Figure 8** outlines the easy/short-term and medium/medium-term improvements recommended along the RSA study area.

# DIVISION AVE. RSA • CORRIDOR SPOT IMPROVEMENT

Item # 6.



0 0.13 0.25 Miles

Figure 8

## Hard/Long-Term Mitigation

### Recommendation 12: Reconfigure the Cross Section of Division Avenue

To better serve cyclists, pedestrians, and vehicle traffic along Division Avenue, Fehr & Peers recommends reconfiguring the full length of Division Avenue. Fehr & Peers measured the cross section of Division Avenue at multiple locations and found the average cross section to be about 57 feet across. Sandpoint Public Works staff indicated that maintaining the multi-use path was essential. For bi-directional travel, the multi-use path should be expanded to be at a minimum 10 feet wide. The Public Works staff also indicated that landscaped buffers should be included in any reconfiguration to provide space for snow storage during the winter and to serve as a buffer between cyclists, pedestrians, and vehicle traffic. Since Division Avenue is a designated truck route, Fehr & Peers also recommends maintaining at least 11 feet lanes throughout the corridor.

**Figure 9** shows the cross section of Division Avenue near intersections where a left-turn lane is required. In these segments, the bike lanes are recommended to be removed and the vehicle lanes are recommended to be narrowed to 11 feet to accommodate the widened multi-use path, planting strips, and sidewalk. Since bike lanes would no longer be provided along Division Avenue, Fehr & Peers recommends adding “Sharrow” pavement markings to indicate to drivers that cyclists may use the vehicle travel lane. The sharrows will provide an area for the commuting cyclists while the multi-use path provides an area for the other users who are less comfortable sharing the road with motor vehicles. With the additional space, Fehr & Peers recommends widening the multi-use path to 12 feet and marking it for bi-directional travel. The east side sidewalk is also recommended to be widened to 6 feet for the length of the corridor. Planting strips are also recommended to be added to provide snow storage. While Sandpoint City staff expressed that planting strips should be 5 feet wide, due to the limited cross-section space, staff also expressed that a minimum planting strip width of 3 feet would be acceptable near intersections with left-turn lanes.

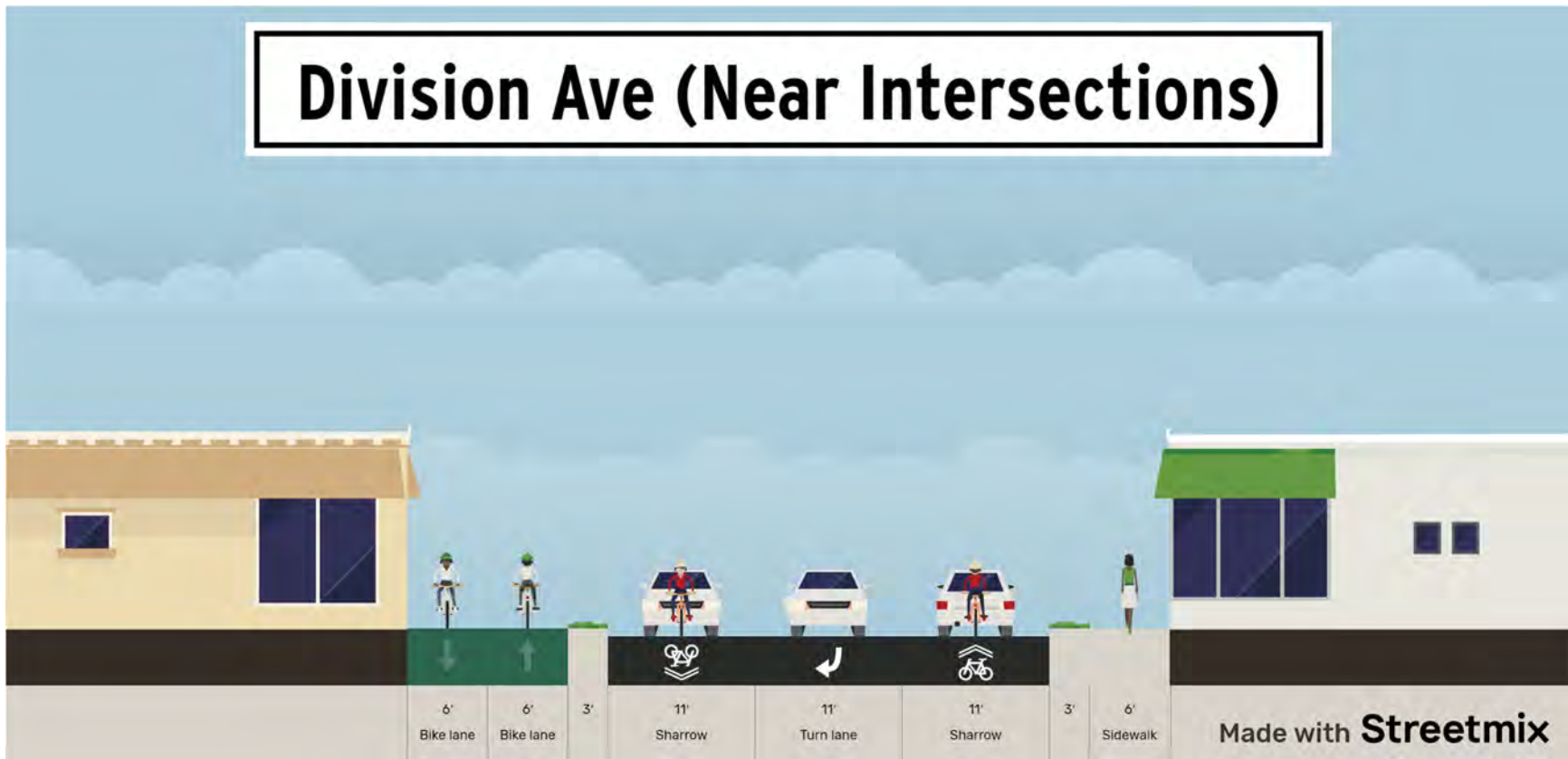


Figure 9: Proposed Division Avenue cross section near intersections.

**Figure 10** shows the cross section of Division Avenue between intersections where no left-turn lane is required. In these segments, the 11 foot turn lane is removed and the planting strips are recommended to be widened to provide additional snow storage. The travel lanes are also widened to 12 feet with an extra 0.5 foot added to the gutter, also for snow storage. The multi-use path and sidewalk widths are also recommended to be maintained to provide a continuous walking/biking trail.

The reconfiguration is recommended for the following reasons:

1. The wider, directionally marked, multi-use path would provide additional space to cyclists and pedestrians.
2. The current 5 feet sidewalk is restrictive for pedestrians with disabilities. Widening to 6 feet provides additional space to make the sidewalk more comfortable.
3. The planting strips between the multi-use path/sidewalk and the vehicle travel lanes serve as snow storage and as additional buffer space between bikes/peds and vehicle traffic.
4. Historically, wide vehicle travel lanes, like the current lanes along Division Avenue encourage speeding. Narrowing the lanes along Division Avenue is recommended to discourage speeding.

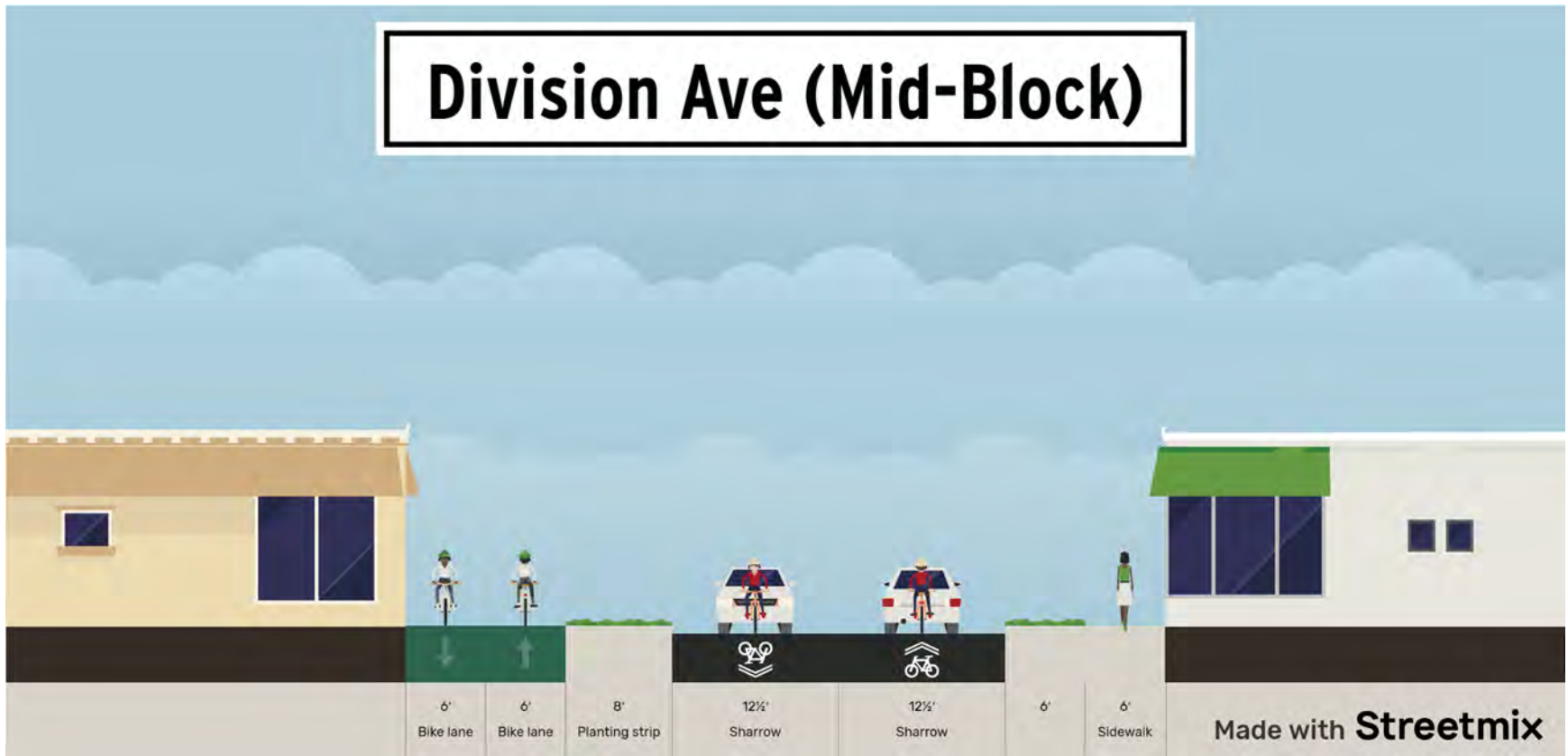


Figure 10: Proposed Division Avenue cross section mid-block (away from intersections).

## Acknowledgements

The RSA team acknowledges the efforts and contributions of the following groups:

- Sandpoint Public Works
- Bonner County
- LHTAC
- Lake Pend Oreille School District
- Sandpoint Police Department

## Next Steps

This RSA is also intended to inform the direction of the Sandpoint MTMP currently in development. This RSA should be consulted when outlining development plans and strategies within the RSA study area.