



PEDESTRIAN AND BICYCLE ADVISORY COMMITTEE MEETING AMENDED AGENDA

September 11, 2025 at 11:30 AM

Council Chambers at City Hall - 1123 W. Lake St. Sandpoint, Idaho

Call to Order

Roll Call

Meeting Minutes Approval

1. Approval of Minutes from the Committee's August 14, 2025, Meeting - ***action item***

Committee Business

2. Discussion: Committee and Staff Review Workflow
3. Discussion: Open House Date
4. Recommendation to Staff: Division Avenue Corridor Improvements, Phase 2 - ***action item***
5. ADDED: Recommendation to Staff: Fourth Avenue Sidewalk Project Preliminary Review - ***action item***
6. Recommendation to Staff: Downtown Bike Parking - ***action item***
7. Committee Review and Concurrence on Comments regarding O'Connell Estates Subdivision - ***action item***
8. Subcommittee Reports
9. General Updates from Staff
10. ~~REMOVED: Recommendation to Planning & Zoning Commission: Amendments to City Code 7-3-10-H, Sidewalk in-lieu Fee Program - ***action item***~~
11. ~~REMOVED: Recommendation to Staff: Ella Cottages - ***action item***~~

Committee Roundtable

Adjourn

Public Participation Options and Information

Before the meeting, comment in writing: Email cityclerk@sandpointidaho.gov or deliver to City Hall.
Attend in person: See above for meeting location. Seating available on first-come, first-served basis.
Attend remotely: Register at <https://www.sandpointidaho.gov/meetings>.
After the meeting, view the recording on YouTube: <https://www.youtube.com/c/CityofSandpoint>.
For questions or requests for special accommodation: At least 48 hours prior to the meeting, send a message to the email address above or call (208) 263-3310.



PEDESTRIAN AND BICYCLE ADVISORY COMMITTEE MEETING MINUTES

August 14, 2025 at 11:30 AM

White Pine Conference Room at City Hall - 1123 W. Lake St. Sandpoint, Idaho

Call to Order

Chair Cate Huisman called the meeting of the Sandpoint Pedestrian and Bicycle Advisory Committee to order at 11:30 a.m. on Thursday, August 14, 2025, in White Pine Conference Room at City Hall, 1123 W. Lake Street, Sandpoint, Idaho.

Roll Call

PRESENT

Cate Huisman, Chair
Molly O'Reilly, Vice Chair
Erin Billings
Jennifer Heller (arrived 11:57 a.m.)
Julie Perchynski
Reid Weber (arrived at 11:33 a.m.)
Katie Stepleton
Sally Lankamer

ABSENT

Radley Peterson

Also present were staff liaison Brandon Staglund, along with Associate Planner Erik Brubaker, and Deputy City Clerk Hayley Keys, serving as board clerk. Council liaison Deb Ruehle was absent.

Meeting Minutes Approval

1. The minutes from the Committee's June 12, 2025, meeting were approved as presented.
Motion made by O'Reilly, Seconded by Billings.
Voting Yea: Huisman, O'Reilly, Billings, Perchynski, Stepleton, Lankamer
2. The minutes from the Committee's July 10, 2025, meeting were approved as presented.
Motion made by Billings, Seconded by O'Reilly
Voting Yea: Huisman, O'Reilly, Billings, Perchynski, Stepleton, Lankamer

Following approval of the minutes, Mr. Staglund announced that this would be his final meeting with the Committee and introduced Mr. Brubaker as the new staff liaison. Before proceeding to Old/Unfinished Business, Ms. Stepleton and Ms. Lankamer each introduced themselves to the Committee.

Old/Unfinished Business

3. Subcommittee Reports

The development subcommittee had no update; the MMTMP subcommittee had no update; the intersections subcommittee will present updates during agenda item #6; the

events subcommittee provided updates on potential open houses in the future in coordination with the Public Works Department.

4. Division Avenue Corridor Improvements, Phase 2 Presentation

Following a brief presentation from Mr. Staglund, questions were fielded by both Mr. Staglund and Mr. Brubaker. The Committee members agreed to bring forward recommendations for the project at the next regularly scheduled meeting.

5. Discussion on the Downtown Bike Parking Survey

Committee members handed in the packets provided at the previous meeting to Jennifer Heller. Ms. Heller will organize and compile information collected on Commercial A district bike racks and will turn over information to Mr. Brubaker before the Committee's next regularly scheduled meeting, where they will discuss the data.

6. Recommendation of Amendments to City Code 7-3-10-H, Sidewalk In-lieu Fee Program

Mr. Staglund provided a presentation and information for the Committee as to why this program is being brought to the attention of the Committee, which was a request by the Sandpoint Planning and Zoning Commission for the Committee's review and recommendation to the Commission to inform the Commission's ultimate recommendation to City Council. Following the presentation, the Committee decided to table and revisit this matter during their next regularly scheduled meeting.

New Business

7. Subcommittee Assignments for New Committee Members

After a brief explanation of each subcommittee functions for the new Committee members, the following subcommittees were formed:

Intersection Subcommittee:

Ms. Perchynski, Ms. O'Reilly, Ms. Heller, Ms. Lankamer

MMTMP Subcommittee:

Mr. Weber, Mr. Peterson, Ms. O'Reilly, Ms. Stepleton

Events Subcommittee:

Ms. Billings, Ms. Huisman, Ms. Lankamer

Development Subcommittee:

Ms. Heller, Ms. O'Reilly, Ms. Stepleton

General Announcements/Comments

Mr. Staglund announced that during the next regularly scheduled meeting, City Planner Bill Dean will provide a presentation on exactions in subdivision review process.

Adjourn

With no further business before the Committee, the meeting adjourned at 12:50 p.m.

These minutes, prepared by the board clerk, were approved by the Committee during their meeting on _____, 2025.

Molly O'Reilly, Board Vice Chair

Attest: Hayley Keys, Deputy City Clerk

SANDPOINT SIDEWALK PROJECT 2025

CIVIL CONSTRUCTION DRAWINGS

PORTIONS OF S 3RD AVE., W PACIFIC ST. AND S 4TH AVE.
 A PORTION OF THE SW¹/₄ OF SEC. 22, T. 57N., R. 02W., B.M.
 SANDPOINT, IDAHO

AUG 2025

GENERAL NOTES:

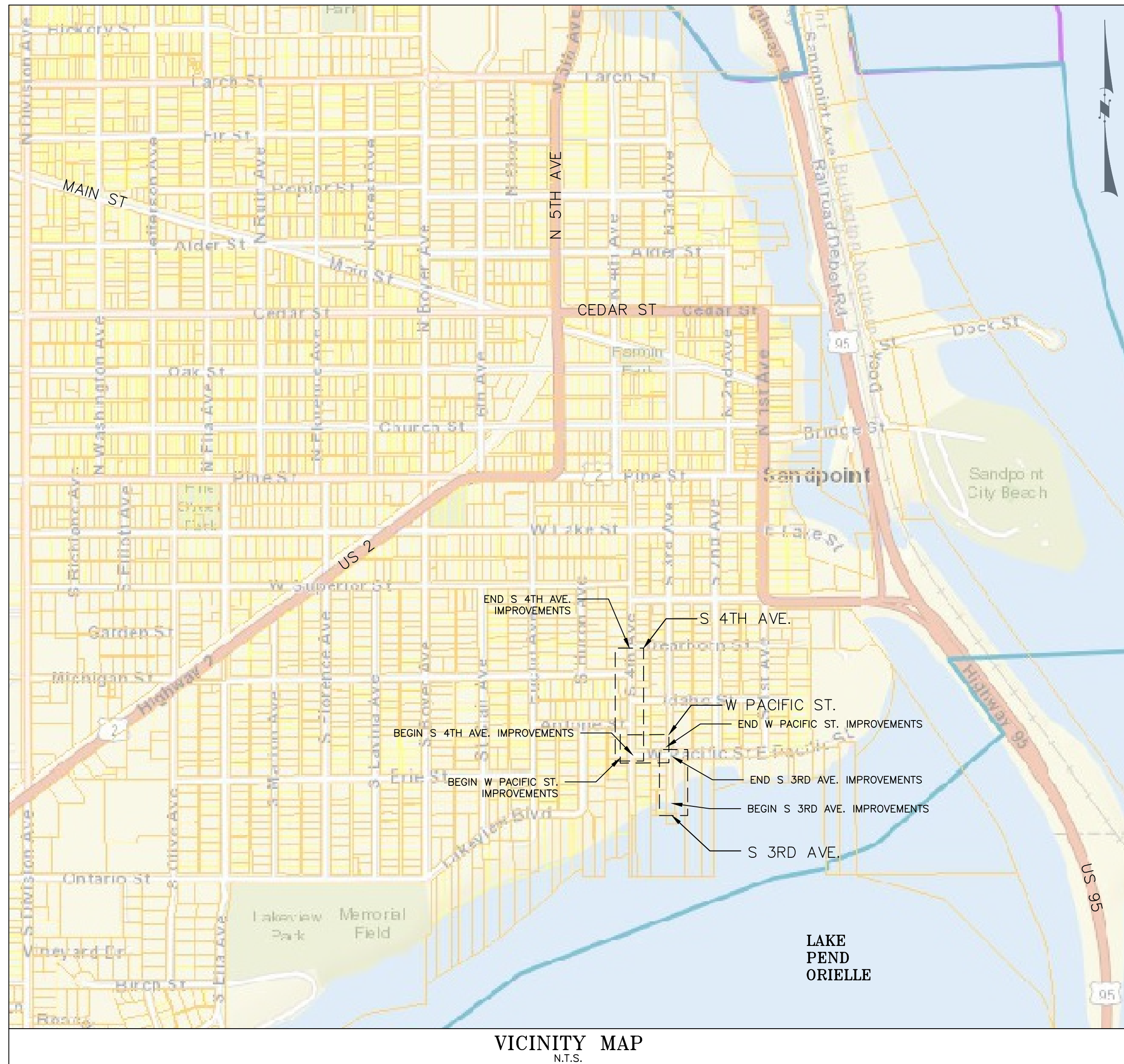
- ALL WORK SHALL CONFORM TO THE MOST CURRENT EDITION OF THE IDAHO STANDARDS FOR PUBLIC WORKS CONSTRUCTION (ISPC) AND AS AMENDED, SUPPLEMENTED OR MODIFIED THEREIN.
- NO GUARANTEE IS MADE OR IMPLIED THAT ALL EXISTING UTILITIES ARE SHOWN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR FIELD VERIFICATION AND LOCATION OF ALL UTILITIES AND UNDERGROUND PIPING. THE CONTRACTOR SHALL CALL THE UTILITY ONE CALL (811) AT LEAST 48 HOURS PRIOR TO COMMENCING WORK. THE CONTRACTOR SHALL PROTECT ALL EXISTING UTILITIES AND STRUCTURES.
- THE CONTRACTOR SHALL NOTIFY THE APPROPRIATE UTILITY COMPANIES PRIOR TO STARTING WORK NEAR ANY FACILITIES AND SHALL COORDINATE THEIR WORK WITH COMPANY REPRESENTATIVES.
- IN THE EVENT THAT ANY UNFORESEEN CONDITIONS NOT COVERED BY THESE CONSTRUCTION DOCUMENTS ARE ENCOUNTERED DURING CONSTRUCTION THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE CITY.
- THE CONTRACTOR SHALL HAVE AN APPROVED SET OF CONSTRUCTION PLANS ON SITE AT ALL TIMES DURING CONSTRUCTION. NO REVISIONS SHALL BE MADE TO THE PLANS WITHOUT THE APPROVAL OF THE CITY. AS-BUILTS SHALL BE SUBMITTED TO THE CITY AT THE COMPLETION OF THE PROJECT.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO COMPLY WITH OSHA SAFETY STANDARDS AND REQUIREMENTS.
- EXISTING PROPERTY CORNERS OR SURVEY MONUMENTS SHALL BE PROTECTED DURING THE COURSE OF CONSTRUCTION. ANY DAMAGED OR OBLITERATED CORNERS OR MONUMENTS SHALL BE RE-ESTABLISHED AT THE CONTRACTORS EXPENSE BY A PROFESSIONAL LAND SURVEYOR, LICENSED IN THE STATE OF IDAHO, PRIOR TO FINAL ACCEPTANCE.
- THE CONTRACTOR SHALL MAINTAIN THE STREETS, SIDEWALKS, AND ALL OTHER PUBLIC AND PRIVATE PROPERTY IN A CLEAN, SAFE AND USABLE CONDITION. ALL SOIL, ROCK, OR CONSTRUCTION DEBRIS SHALL BE PROMPTLY REMOVED DURING CONSTRUCTION AND UPON COMPLETION OF THE PROJECT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR RETURNING ALL DISTURBED AREAS TO PRE-CONSTRUCTION CONDITIONS OR BETTER.
- ALL UNDERGROUND UTILITIES AND CONDUITS SHALL BE INSTALLED BEFORE CONSTRUCTION OF CURBS, SIDEWALKS OR SURFACING OF STREETS.
- LANDSCAPE SLEEVING: EARTHWORK CONTRACTOR SHALL PROVIDE SLEEVING AS REQUIRED UNDER SIDEWALKS, PATHS, CURBING, PAVING, AND OTHER HARD SURFACES AS NEEDED FOR IRRIGATION ACCESS. ALL SLEEVING SHALL BE A 2.5" PVC WITH AT LEAST 2" OF COVER BELOW CONCRETE SIDEWALK. THE CONTRACTOR SHALL INSTALL SLEEVING BEFORE SIDEWALKS ARE INSTALLED.
- CONTRACTOR SHALL RETAIN AND PROTECT ALL EXISTING IRRIGATION SYSTEMS TO THE GREATEST EXTENT POSSIBLE AND REPLACE DAMAGED PORTIONS AT IRRIGATION SYSTEMS WHETHER DAMAGE OCCURS DURING CONSTRUCTION OR NOT.
- DRY UTILITY CONDUIT: CIVIL CONTRACTOR IS RESPONSIBLE FOR COORDINATING ALL ELECTRICAL AND DRY UTILITY CONDUIT NEEDS WITH THE CITY OF SANDPOINT.
- ALL DISTURBED AREAS NOT OTHERWISE COVERED WITH PAVEMENT, STRUCTURES OR LANDSCAPING SHALL BE TOP COATED WITH A MINIMUM 3" OF TOPSOIL AND HYDROSEED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL TRAFFIC CONTROL, IN ACCORDANCE WITH THE M.U.T.C.D. CURRENT EDITION. TRAFFIC CONTROL PLANS SHALL BE PREPARED BY THE CONTRACTOR AND SUBMITTED TO THE CITY OF SANDPOINT FOR APPROVAL. NO WORK SHALL TAKE PLACE UNTIL ALL APPROVED TRAFFIC CONTROL IS IN PLACE.
- ALL SIGNING AND STRIPING SHALL BE IN CONFORMANCE WITH THE M.U.T.C.D., CURRENT EDITION.
- TOPOGRAPHICAL DATA IS BASED ON AERIAL IMAGERY AND MEASUREMENTS TAKEN IN THE FIELD. CONTRACTOR IS RESPONSIBLE FOR FIELD VERIFICATION PRIOR TO BID. VAN HOUTEN CONSULTING & DESIGN TAKES NO RESPONSIBILITY FOR TOPOGRAPHICAL MISHAPS OR UNFORESEEN SUBSURFACE CONDITIONS.
- RIGHT-OF-WAY LINES ARE REPRESENTATIVE ONLY. CONTRACTOR IS RESPONSIBLE FOR IDENTIFYING RIGHT-OF-WAY LIMITS, GRADING, AND ALIGNMENT FOR THE PROJECT.
- CONTRACTOR SHALL COORDINATE WITH ADJACENT RESIDENTS TO ACCOMMODATE DRIVEWAY ACCESS AT ALL TIMES DURING CONSTRUCTION.

STORMWATER & EROSION CONTROL NOTES:

- ALL STORMWATER BMPs SHALL CONFORM TO IDAHO DEPARTMENT OF ENVIRONMENTAL QUALITY (IDEQ) STORM WATER BEST MANAGEMENT PRACTICES CATALOG.
- IN AREAS OUTSIDE THE LIMITS OF DISTURBANCE, RETAIN THE DUFF LAYER, NATIVE TOPSOIL, AND EXISTING VEGETATION IN AN UNDISTURBED STATE TO THE MAXIMUM EXTENT PRACTICAL.
- INSPECT ALL ROADWAYS, AT THE END OF EACH DAY, ADJACENT TO THE CONSTRUCTION ACCESS ROUTE. IF IT IS EVIDENT THAT SEDIMENT HAS BEEN TRACKED OFF SITE AND/OR BEYOND THE ROADWAY APPROACH, CLEANING IS REQUIRED.
- INSPECT SEDIMENT CONTROL BMPs WEEKLY AT A MINIMUM, DAILY DURING A STORM EVENT, AND AFTER ANY DISCHARGE FROM THE SITE (STORMWATER OR NON-STORMWATER). THE INSPECTION FREQUENCY MAY BE REDUCED TO ONCE A MONTH IF THE SITE IS STABILIZED AND INACTIVE.
- CONTROL FUGITIVE DUST FROM CONSTRUCTION ACTIVITY IN ACCORDANCE WITH STATE AND/OR LOCAL AIR QUALITY CONTROL AUTHORITIES WITH JURISDICTION OVER THE PROJECT AREA.
- PROTECT INLETS, DRYWELLS, CATCH BASINS, CULVERTS AND OTHER STORMWATER MANAGEMENT FACILITIES FROM SEDIMENT, WHETHER OR NOT FACILITIES ARE OPERABLE.
- COORDINATE ANY STOCKPILING OF MATERIALS WITH THE CITY OF SANDPOINT PRIOR TO CONSTRUCTION.
- NO AREAS DISTURBED BY CONSTRUCTION SHALL BE LEFT BARE. ALL PEROUS DISTURBED AREAS NOT OTHERWISE COVERED WITH ROCK, MULCH OR LANDSCAPING SHALL BE SEED.
- AREAS RECEIVING SEED SHALL HAVE SOIL SUITABLE FOR VEGETATION GROWTH. IF THE FINISHED SURFACE CANNOT SUPPORT PLANT LIFE, TOPSOIL SHALL BE IMPORTED AND PLACED TO A SUFFICIENT DEPTH FOR PERMANENT REVEGETATION. SHOULD HYDROSEED FAIL TO TAKE ROOT DUE TO INSUFFICIENT SOIL PROFILE, CONTRACTOR SHALL RECTIFY THE PROBLEM BY PERMANENTLY ESTABLISHING VEGETATION AT THEIR OWN EXPENSE.
- CONSTRUCTION SHALL BE CONSIDERED COMPLETE WHEN PERMANENT ESC CONTROLS, WHEN APPLICABLE, HAVE BEEN COMPLETELY INSTALLED; ALL LAND-DISTURBING ACTIVITIES THAT HAVE THE POTENTIAL TO CAUSE EROSION OR SEDIMENTATION PROBLEMS HAVE CEASED; AND, VEGETATION HAS BEEN ESTABLISHED IN THE AREAS NOTED AS REQUIRING VEGETATION ON THE ACCEPTED PLAN ON FILE WITH THE LOCAL JURISDICTION.
- REMOVE TEMPORARY ESC BMPs WITHIN THIRTY DAYS AFTER THE TEMPORARY BMPs ARE NO LONGER NEEDED. PERMANENTLY STABILIZE AREAS THAT ARE DISTURBED DURING THE REMOVAL PROCESS.

GRADING & PAVING NOTES:

- ASPHALT PAVEMENT SURFACE COURSE SHALL CONFORM TO ISPC REQUIREMENTS OR BETTER.
- CONCRETE PAVEMENT SHALL COMPLY WITH ISPC AND ACI 301 REQUIREMENTS FOR MEASURING, MIXING, TRANSPORTING AND PLACING CEMENT CONCRETE PAVEMENT.
- SUBMIT PAVEMENT MIX DESIGN, SUBBASE AND AGGREGATE BASE COURSE INFORMATION FOR CITY APPROVAL PRIOR TO ORDERING THESE MATERIALS.
- CONTRACTOR SHALL CHECK EXPOSED SUBGRADES AND BASE SURFACES FOR COMPLIANCE WITH REQUIREMENTS FOR DIMENSIONAL GRADING, AND ELEVATION TOLERANCES; PREVENT SURFACE WATER AND GROUNDWATER FROM ENTERING EXCAVATIONS, FROM PONDING ON PREPARED SUBGRADES AND BASE SURFACES AND FROM FLOODING PROJECT SITE OR SURROUNDING AREA; AND SHALL PROTECT SUBGRADES AND BASE SURFACES FROM SOFTENING, UNDERMINING, WASHOUT, DAMAGE BY RAIN OR WATER ACCUMULATION AND AGAINST FREEZING TEMPERATURES AND FROST.
- STRIP TOPSOIL PRIOR TO COMPACTING SUBGRADE. SUBGRADE SHALL BE FREE OF ORGANIC MATTER, FROZEN SOIL AND DELETERIOUS DEBRIS.
- CONTRACTOR TO SAWCUT EXISTING PAVEMENT/CONCRETE IN A NEAT LINE AS NEEDED TO FACILITATE THE WORK SHOWN ON THE PLANS. REMOVE AND DISPOSE OF ALL SAWCUT ASPHALT AND UNUSED MATERIALS.



VICINITY MAP
N.T.S.

CONTACT INFORMATION

OWNER
 CITY OF SANDPOINT
 1123 W LAKE ST.
 SANDPOINT, ID 83864
 PHONE: (208) 946-2062

CIVIL ENGINEER
 VAN HOUTEN CONSULTING & DESIGN, LLC
 401 1/2 E. SHERMAN AVE. STE. 211
 COEUR D'ALENE, ID 83814
 PHONE: (208) 930-4000

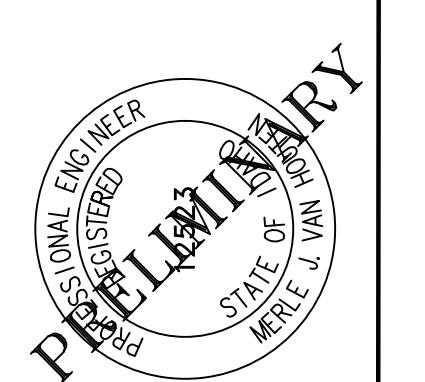
SHEET INDEX

C0.0	COVER SHEET
C1.0	4TH AVE STA: 10+00.00 TO 13+60.00
C1.1	4TH AVE STA: 13+60.00 TO 17+23.00
C1.2	4TH AVE STA: 17+23.00 TO END
C2.0	PACIFIC ST
C3.0	3RD AVE
C4.0	ENLARGED DETAILS
C4.1	DETAILS
C4.2	DETAILS

LEGEND

	EX. STOPLIGHT
	EX. STREET LIGHT
	EX. ROAD SIGN
	EX. DETECTABLE WARNING STRIP
	EX. POWER POLE
	EX. GUY WIRE
	EX. ELECTRICAL BOX
	EX. HYDRANT
	EX. WATER VALVE
	EX. WATER METER
	EX. STORM DRAIN CATCH BASIN
	EX. STORM DRAIN MANHOLE
	EX. SEWER MANHOLE
	EX. OHP
	EX. STORM DRAIN PIPE
	EX. SEWER PIPE
	EX. WATER PIPE
	EX. EOP
	EX. SIDEWALK
	EX. PROPERTY BOUNDARY

Consulting & Design
VH
 MERLE@VANHOUTEND.COM
 401 1/2 E. SHERMAN AVE. STE. #211
 COEUR D'ALENE, ID 83814
 WWW.VANHOUTEND.COM
 VAN HOUTEN CONSULTING & DESIGN, LLC

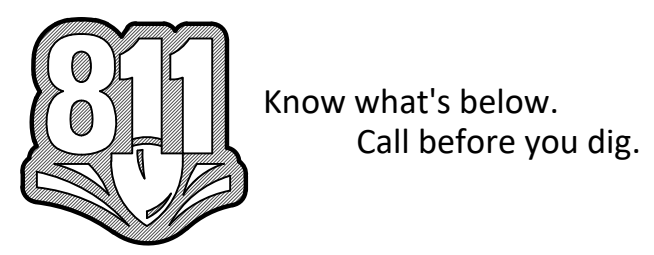


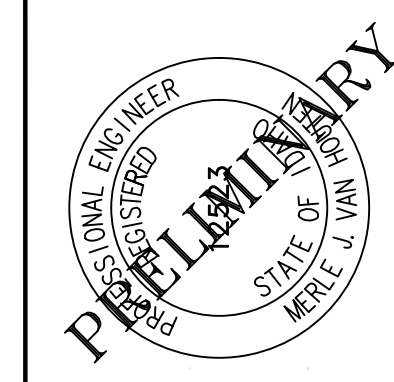
SANDPOINT SIDEWALK
 PROJECT 2025
 COVER SHEET
 SANDPOINT, IDAHO

REVISIONS		BY
NO.	DATE	DESCRIPTION

DATE: 08/07/2025	PROJECT: 25-025	PARCEL: N/A	DRAWN: SRP	CHECKED: WHI	DATE: 08/07/2025	PROJECT: 25-025	PARCEL: N/A	DRAWN: SRP	CHECKED: WHI
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C0.0
 SHEET
 1 OF 9
 UNAUTHORIZED REPRODUCTION OR TRANSLATION OF ANY PART OF THIS WORK WITHOUT THE WRITTEN PERMISSION OF VAN HOUTEN CONSULTING AND DESIGN, LLC IS STRICTLY PROHIBITED.





**SANDPOINT SIDEWALK
 PROJECT 2025**
 4TH AVE STA:10+00 TO 13+60.00
 SANDPOINT, IDAHO

REVISIONS		BY
NO.	DATE	DESCRIPTION

DATE: 08/27/2025	PROJECT: #25-025	PARCEL: N/A	DRAWN: SRP	CHECKED: MWI	DATE: 08/27/2025
PATH: \\S:\PROJECTS\2025\25-0250 - SANDPOINT SIDEWALK\PRODUCTION\2025_VANHOUTECD\CAD\DRAWINGS					

C1.0
SHEET
2 OF 9
 UNAUTHORIZED REPRODUCTION OR TRANSLATION OF ANY PART OF THIS WORK WITHOUT THE WRITTEN PERMISSION OF VAN HOUTEN CONSULTING AND DESIGN, LLC IS STRICTLY PROHIBITED.

S FOURTH AVE
 STA:10+00.00 TO 13+60.00

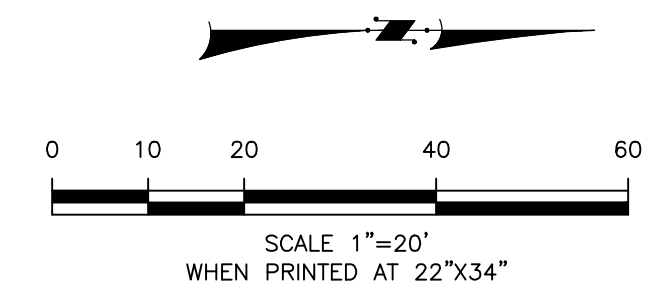


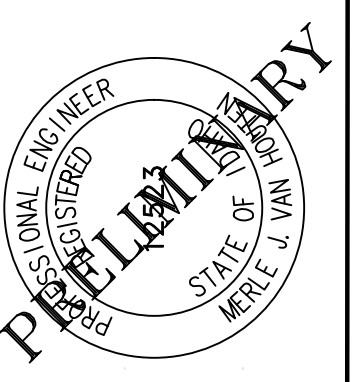
S FOURTH AVE REFERENCE NOTES			
NUMBER	STATION	OFFSET	DESCRIPTION
①	10+19	24.5'R	CONSTRUCT (1) PEDESTRIAN RAMP (TYPE F, ISPWC SD-712F) W/ DETECTABLE WARNING STRIP SAWCUT ASPHALT; REMOVE AND RECONSTRUCT 15 LF OF 6" VERTICAL CURB (NO GUTTER) AND ASPHALT.
②	10+31.30-10+38.30	24.5'R	CONSTRUCT 5'X5' LANDING AT TOP OF RAMP THEN TAPER 8.5 LF OF 5 FT WIDE SIDEWALK TO JOIN BACK OF CURB
③	10+38.30-10+83.50	20.5'R	NO EXISTING SIDEWALK CONSTRUCT 45 LF OF 5 FT WIDE SIDEWALK CONTRACTOR TO COORDINATE WITH CITY FOR RELOCATION/REPLACEMENT OF WATER METER - STA:10+80
④	10+86	20.5'R	REMOVE EXISTING CONCRETE STAIRS LEADING DOWN TO RESIDENCE. CONSTRUCT 5FT WIDE SIDEWALK JOINED TO CURB. RECONSTRUCT CONCRETE STAIRS BEHIND SIDEWALK PER ISPWC SD-713 WITH 12" TREAD AND 6" RISE TO EXISTING WALKWAY.
⑤	10+88-11+27	20.5'R	NO EXISTING SIDEWALK CONSTRUCT 39 LF OF 5 FT WIDE SIDEWALK PLACE IMPORTED FILL TO CONSTRUCT 3:1 MAX BACK SLOPE FROM SIDEWALK TO LANDSCAPE ROCKERY COORDINATE LIMBING OF CEDAR TREES WITH CITY.
⑥	11+27	20.5'R	CONSTRUCT SIDEWALK TAPER TO CREATE 3 FT OF SEPARATION FROM TBC TO FACE OF WALK. 8 FT OF 5 FT SIDEWALK
⑦	11+35-12+26	23.5'R	NO EXISTING SIDEWALK CONSTRUCT 91 LF OF 5 FT WIDE SIDEWALK REMOVE EXISTING LANDSCAPE PAVER WALKWAY TO 431 W FOURTH WHERE CONFLICTS WITH NEW SIDEWALK EXIST. CONTRACTOR TO COORDINATE WITH CITY FOR RELOCATION/REPLACEMENT OF WATER METER- STA 11+54.
⑧	12+45	23.5'R	SEE 421 FOURTH AVE ENLARGED DETAIL, SHEET C4.0
⑨	12+95	23.5'R	REPLACE BROKEN SIDEWALK PANEL
⑩	13+04-13+45	23.5'R	SEE FOURTH AVE AND IDAHO ST ENLARGED INTERSECTION DETAIL, SHEET C4.0

NOTE:
 1. PROTECT AND RETAIN EXISTING CURB TO THE GREATEST EXTENT FEASIBLE UNLESS NOTED OTHERWISE.
 2. CONTRACTOR SHALL CONTACT THE CITY FOR A FORMING INSPECTION PRIOR TO PLACING CONCRETE. ADA TOLERANCES SHALL NOT BE EXCEEDED.
 3. REFER TO SHEET C4.1 FOR HMA ROADWAY PATCHING DETAIL.
 4. REFER TO SHEET C4.1 FOR CITY OF SANDPOINT STANDARD APPROACH DETAIL.
 5. REFER TO ISPWC DWG NO SD-701 & 701A FOR 6" CURB & 6" CURB AND GUTTER DETAILS.
 6. REFER TO ISPWC DWG NO SD-709 FOR CONCRETE SIDEWALK DETAIL.
 7. REFER TO SHEET C4.2 FOR DIVERSION AND RAMPING AT TREES DETAILS.
 8. REFER TO ISPWC SECTION 2030 FOR ALL UTILITY ADJUSTMENTS.

LEGEND

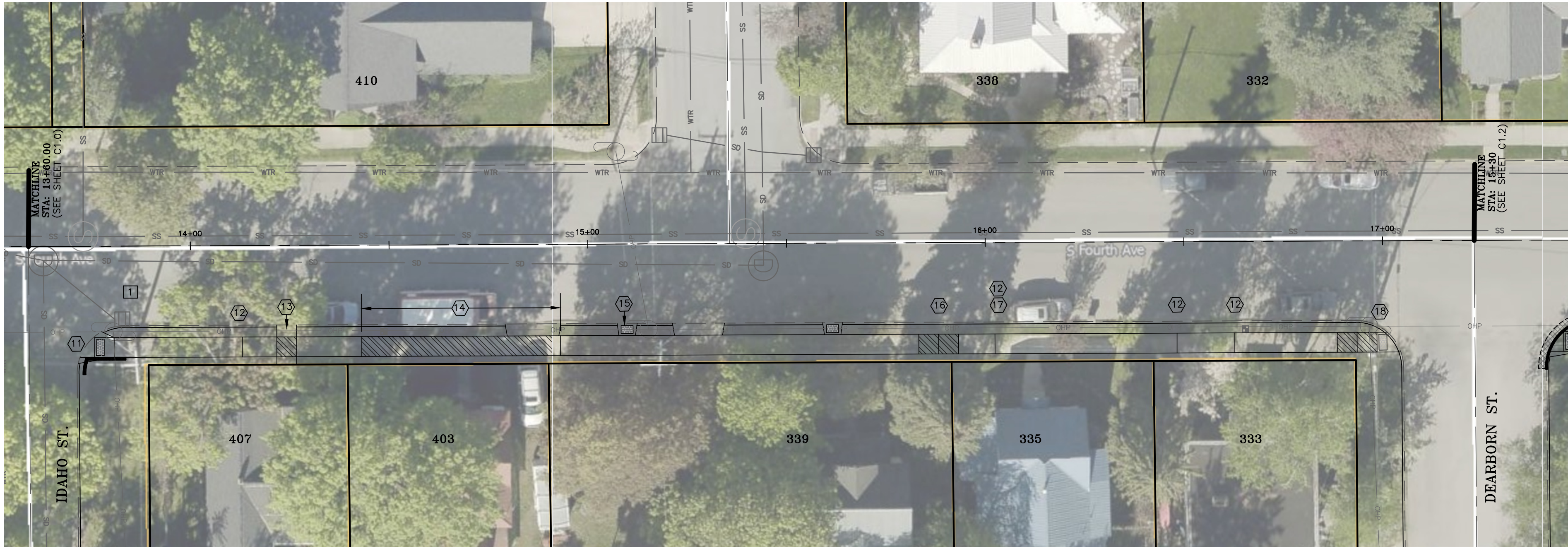
- SIDEWALK CONSTRUCTION
- CURB CONSTRUCTION
- PLANT MIX PAVEMENT





**SANDPOINT SIDEWALK
 PROJECT 2025**
 4TH AVE STA:13+60 TO 17+23
 SANDPOINT, IDAHO

S FOURTH AVE
 STA:13+60.00 TO 17+23.00



S FOURTH AVE REFERENCE NOTES			
NUMBER	STATION	OFFSET	DESCRIPTION
11	13+73	25.0'R	GRIND PEDESTRIAN RAMP JOINT RETAIN AND PROTECT PEDESTRIAN RAMP AND LANDSCAPE BLOCK WALL.
12	SEE DESCRIPTION	28.0'R	GRIND SIDEWALK JOINT STA: 14+13, 16+03, 16+48, 16+63
13	14+24	23.0'R	REMOVE EX. BRICK WALKWAY WITHIN SIDEWALK CORRIDOR AND REPLACE WITH 5 LF OF 5 FT WIDE SIDEWALK.
14	14+43- 14+93	23.0'R	REMOVE AND RECONSTRUCT 50 LF OF 5 FT WIDE SIDEWALK ACROSS 403 S FOURTH AVE CONTRACTOR TO COORDINATE WITH CITY FOR RELOCATION/REPLACEMENT OF WATER METER- STA: 14+49 CONSTRUCT 10FT WIDE CONCRETE DRIVEWAY APPROACH BEHIND EXISTING CURB CUT- STA: 14+83
15	15+10	20.0'R	REBUILD PEDESTRIAN RAMP TO ADA STANDARDS
16	15+88	23.0'R	REMOVE AND RECONSTRUCT (2) TWO SIDEWALK PANELS CENTERED ON TREE SEE SIDEWALK REPAIR RAMPING AT TREES DETAIL, SHEET C4.2 COORDINATE LIMBING OF TREE WITH CITY.
17	16+03	23.0'R	COORDINATE LIMBING OF THE TREE WITH CITY
18	17+00	23.0'R	RETAIN AND PROTECT PEDESTRIAN RAMP REMOVE AND RECONSTRUCT (3) THREE PANELS SOUTH OF RAMP CONSTRUCT 5 FT X 5FT RAMP LANDING AT 1.5% MAX SLOPE IN ANY DIRECTION CONSTRUCT REMAINDER OF SIDEWALK AT 5.0% MAX SLOPE.

NOTE:
 1. PROTECT AND RETAIN EXISTING CURB TO THE GREATEST EXTENT FEASIBLE UNLESS NOTED OTHERWISE.
 2. CONTRACTOR SHALL CONTACT THE CITY FOR A FORMING INSPECTION PRIOR TO PLACING CONCRETE. ADA TOLERANCES SHALL NOT BE EXCEEDED.
 3. REFER TO SHEET C4.1 FOR HMA ROADWAY PATCHING DETAIL.
 4. REFER TO SHEET C4.1 FOR CITY OF SANDPOINT STANDARD APPROACH DETAIL.
 5. REFER TO ISPCW DWG NO SD-701 & 701A FOR 6" CURB & 6" CURB AND GUTTER DETAILS.
 6. REFER TO ISPCW DWG NO SD-709 FOR CONCRETE SIDEWALK DETAIL.
 7. REFER TO SHEET C4.2 FOR DIVERSION AND RAMPING AT TREES DETAILS.
 8. REFER TO ISPCW SECTION 2030 FOR ALL UTILITY ADJUSTMENTS.

EROSION CONTROL REFERENCE NOTES			
NUMBER	STATION	OFFSET	DESCRIPTION
1	13+83.00	18.4'R	STORM DRAIN INLET

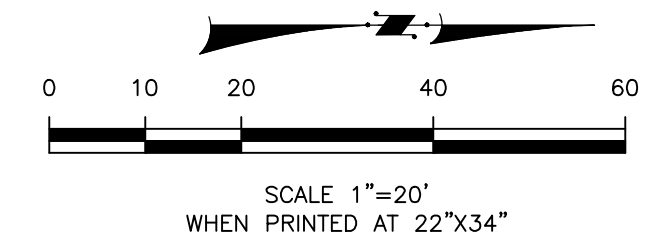
NOTES:
 1. SEE INLET PROTECTION DETAIL, SHEET C4.1.

LEGEND

SIDEWALK CONSTRUCTION

CURB CONSTRUCTION

PLANT MIX PAVEMENT

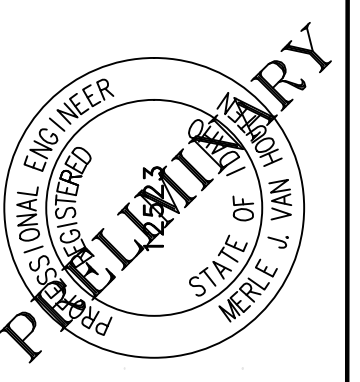


REVISIONS		DATE	BY
NO.	DESCRIPTION		

DATE: 08/07/2025
 PROJECT: #25-025
 PARCEL: N/A
 DRAWN: SRP
 CHECKED: MWI
 PATH: I:\PROJECTS\2025\25-0250 -
 2025 DESIGN\DWG\CIVIL\PRODUCTION -
 20250807.DWG

C1.1
SHEET
3 OF 9

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 STRICTLY PROHIBITED.



**SANDPOINT SIDEWALK
 PROJECT 2025**
 4TH AVE STA:17+23 TO END
 SANDPOINT, IDAHO

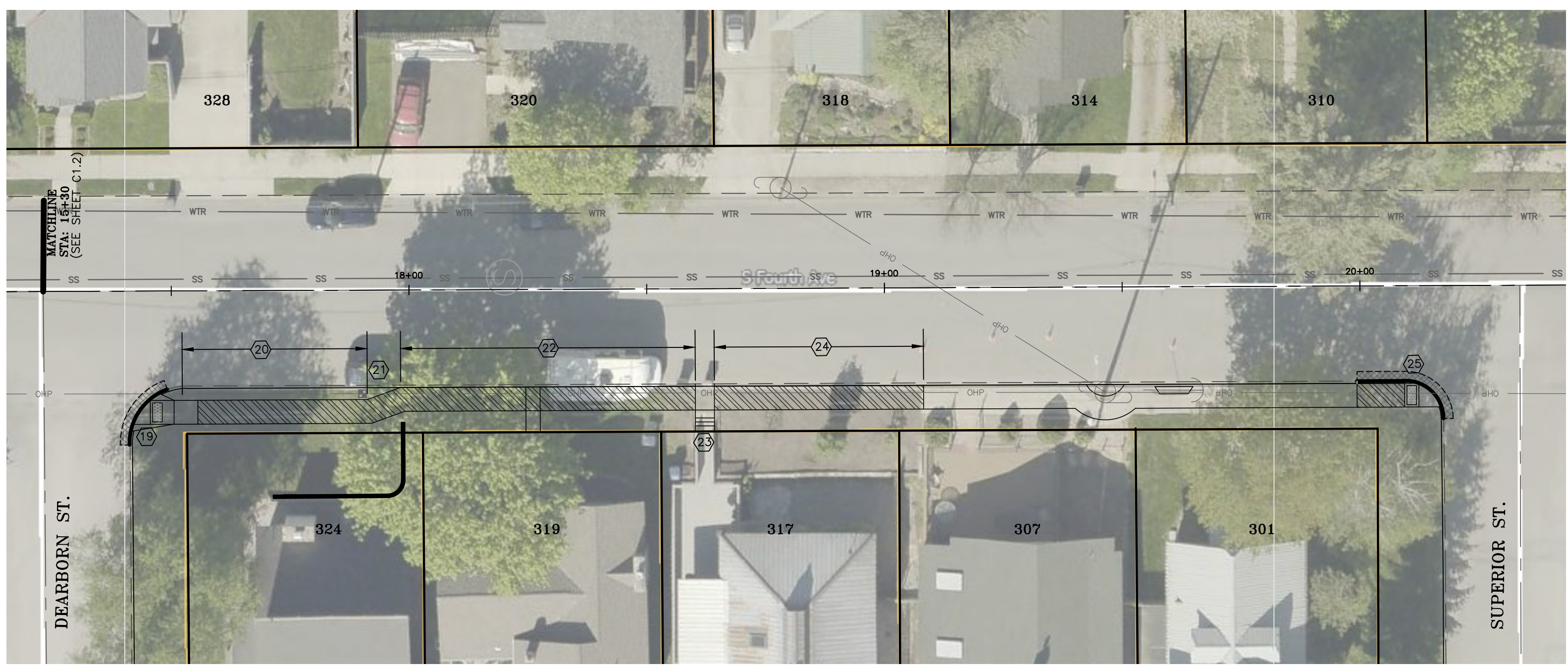
REVISIONS		BY
NO.	DATE	DESCRIPTION

DATE: 08/27/2025	PROJECT: #25-025	PARCEL: N/A	DRAWN: SRP	CHECKED: MWI	DATE: 08/27/2025	PROJECT: #25-025	PARCEL: N/A	DRAWN: SRP	CHECKED: MWI	DATE: 08/27/2025	PROJECT: #25-025	PARCEL: N/A	DRAWN: SRP	CHECKED: MWI
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C1.2
SHEET
4 OF 9

UNAUTHORIZED REPRODUCTION OR TRANSLATION OF ANY PART OF THIS WORK WITHOUT THE WRITTEN PERMISSION OF VAN HOUTEN CONSULTING AND DESIGN, LLC IS STRICTLY PROHIBITED.

S FOURTH AVE
 STA:17+23.00 TO END



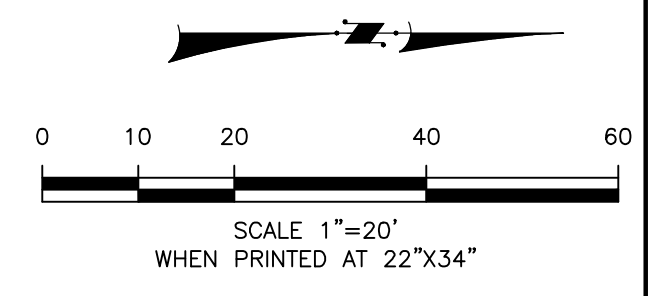
**W FOURTH AVE
 REFERENCE NOTES**

NUMBER	STATION	OFFSET	DESCRIPTION
19	17+45	23.0'R	CONSTRUCT (1) PEDESTRIAN RAMP (TYPE F, ISPCW SD-712F) W/ DETECTABLE WARNING STRIP SAWCUT ASPHALT; REMOVE AND RECONSTRUCT 15 LF OF 6" VERTICAL CURB (NO GUTTER) AND ASPHALT.
20	17+51-17+91	23.0'R	CONSTRUCT 5' X 5' LANDING AT TOP OF RAMP AND THEN 35LF OF 5 FT WIDE SIDEWALK @ 1.5% MAX CROSS SLOPE.
21	17+91-17+98	23.0'R	TAPER 8.5 LF OF 5 FT WIDE SIDEWALK TO JOIN BACK OF CURB RETAIN AND PROTECT EX.WATER METER AND LANDSCAPE BLOCK WALL.
22	17+98-18+60	20.0'R	NO EXISTING SIDEWALK CONSTRUCT 62 LF OF SIDEWALK @1.5% MAX CROSS SLOPE. REMOVE EXISTING LANDSCAPE PAVER WALKWAY TO 431 W FOURTH WHERE CONFLICTS WITH NEW SIDEWALK OCCUR. CONTRACTOR TO COORDINATE WITH CITY FOR RELOCATION/REPLACEMENT OF WATER METER - STA:18+52
23	18+60-18+64	20.0'R	REMOVE EXISTING LANDSCAPE PAVER STAIRS LEADING DOWN TO RESIDENCE. CONSTRUCT 5FT WIDE SIDEWALK JOINED TO CURB. RECONSTRUCT LANDSCAPE PAVER STAIRS BEHIND SIDEWALK WITH 12" TREAD AND 6" RISE TO EXISTING WALKWAY.
24	18+64-19+08	20.0'R	NO EXISTING SIDEWALK CONSTRUCT 44 LF OF 4.5 FT WIDE SIDEWALK @ 1.5% MAX CROSS SLOPE.
25	20+12	20.0'R	REMOVE EXISTING CURB RAMP, (2) TWO SIDEWALK PANELS AND ADJACENT CURB. CONSTRUCT (1) PEDESTRIAN RAMP (TYPE "C3", ISPCW SD-712D) WITH DETECTABLE WARNING STRIP. SAWCUT ASPHALT, RECONSTRUCT 6" VERTICAL CURB (NO GUTTER) AND ASPHALT.

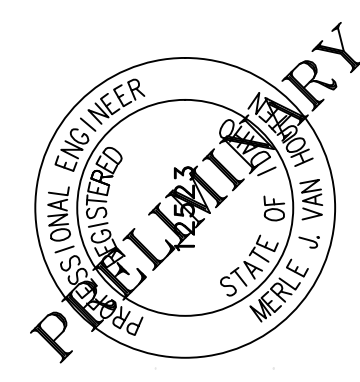
- NOTE:
 1. PROTECT AND RETAIN EXISTING CURB TO THE GREATEST EXTENT FEASIBLE UNLESS NOTED OTHERWISE.
 2. CONTRACTOR SHALL CONTACT THE CITY FOR A FORMING INSPECTION PRIOR TO PLACING CONCRETE. ADA TOLERANCES SHALL NOT BE EXCEEDED.
 3. REFER TO SHEET C4.1 FOR HMA ROADWAY PATCHING DETAIL.
 4. REFER TO SHEET C4.1 FOR CITY OF SANDPOINT STANDARD APPROACH DETAIL.
 5. REFER TO ISPCW DWG NO SD-701 & 701A FOR 6" CURB & 6" CURB AND GUTTER DETAILS.
 6. REFER TO ISPCW DWG NO SD-709 FOR CONCRETE SIDEWALK DETAIL.
 7. REFER TO SHEET C4.2 FOR DIVERSION AND RAMPING AT TREES DETAILS.
 8. REFER TO ISPCW SECTION 2030 FOR ALL UTILITY ADJUSTMENTS.

LEGEND

- SIDEWALK CONSTRUCTION
- CURB CONSTRUCTION
- PLANT MIX PAVEMENT



Know what's below.
 Call before you dig.



**SANDPOINT SIDEWALK
 PROJECT 2025**
PACIFIC ST
SANDPOINT, IDAHO

PACIFIC ST.
 STA:10+00.00 TO END



W PACIFIC AVE REFERENCE NOTES			
NUMBER	STATION	OFFSET	DESCRIPTION
①	10+20-10+42	17.5'R	CONSTRUCT (1) PEDESTRIAN RAMP. ALIGN DETECTABLE WARNING STRIP WITH RAMP ON NORTH SIDE OF PACIFIC AVE. BEGIN RAMP EAST OF DETECTABLE STRIP (6 LF @8.3% MAX TO MATCH 6" CURB HEIGHT) 5' X 5' LANDING AT TOP OF RAMP. CONSTRUCT 6" WIDE CURB WHERE RAMP ABUTS LAWN. THE CURB IS 0" TALL AT TOP OF RAMP AND MATCHES CURB HEIGHT AT STREET. CONTRACTOR SHALL COORDINATE WITH CITY FOR RELOCATION OF IRRIGATION CONTROL BOX.
②	11+42-10+70	17.5'R	CONSTRUCT 23 LF OF 5 FT WIDE SIDEWALK ADJACENT TO CURB THEN TAPER BACK OF SIDEWALK FROM 5 FT TO 3 FT FROM BACK OF CURB OVER A DISTANCE OF 6 FT.
③	10+70-11+00	17.5'R	CONSTRUCT 30 LF OF 3 FT WIDE SIDEWALK ADJACENT TO CURB. RETAIN AND PROTECT EXISTING TREES AND WALKWAY TO 501 4TH ST.
④	11+00-11+21	17.5'R	CONSTRUCT 10 LF SIDEWALK TAPER FROM 3 FT WIDE SIDEWALK ADJACENT TO CURB TO 5 FT WIDE SIDEWALK SEPARATED 4 FT FROM CURB. TAPERING SIDEWALK WIDTH IS 5 FT. CONSTRUCT 5 FT WIDE SIDEWALK FROM TAPER TO DRIVEWAY APPROACH. RETAIN AND PROTECT EX. POWER POLE AND CONCRETE APPROACH.
⑤	12+33	17.5'R	SEE PACIFIC ST AND 3RD AVE ENLARGED INTERSECTION DETAIL, SHEET C4.0

NOTE:
 1. PROTECT AND RETAIN EXISTING CURB TO THE GREATEST EXTENT FEASIBLE UNLESS NOTED OTHERWISE.
 2. CONTRACTOR SHALL CONTACT THE CITY FOR A FORMING INSPECTION PRIOR TO PLACING CONCRETE. ADA TOLERANCES SHALL NOT BE EXCEEDED.
 3. REFER TO SHEET C4.1 FOR HMA ROADWAY PATCHING DETAIL.
 4. REFER TO SHEET C4.1 FOR CITY OF SANDPOINT STANDARD APPROACH DETAIL.
 5. REFER TO ISPWC DWG NO SD-701 & 701A FOR 6" CURB & 6" CURB AND GUTTER DETAILS.
 6. REFER TO ISPWC DWG NO SD-709 FOR CONCRETE SIDEWALK DETAIL.
 7. REFER TO SHEET C4.2 FOR DIVERSION AND RAMPING AT TREES DETAILS.
 8. REFER TO ISPWC SECTION 2030 FOR ALL UTILITY ADJUSTMENTS.

EROSION CONTROL REFERENCE NOTES			
NUMBER	STATION	OFFSET	DESCRIPTION
①	12+33	18.4'R	STORM DRAIN INLET

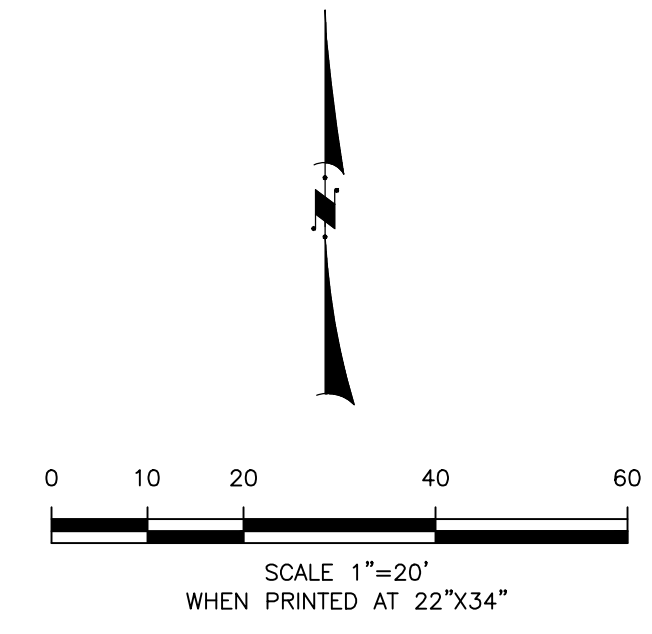
NOTES:
 1. SEE INLET PROTECTION DETAIL, SHEET C4.1.

LEGEND

SIDEWALK CONSTRUCTION

CURB CONSTRUCTION

PLANT MIX PAVEMENT



REVISIONS		BY
NO.	DATE	DESCRIPTION

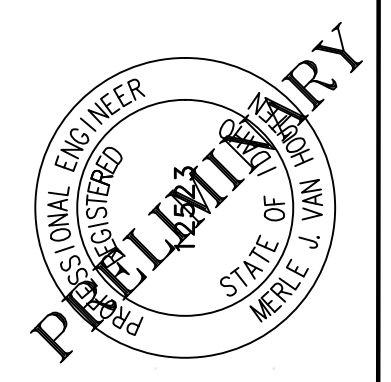
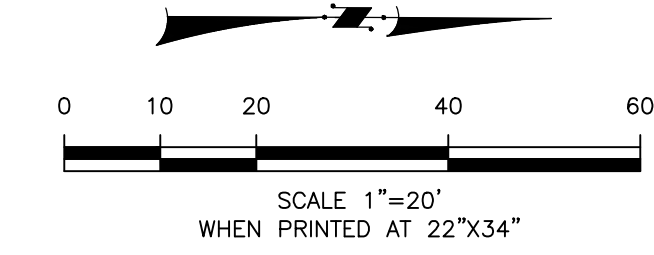
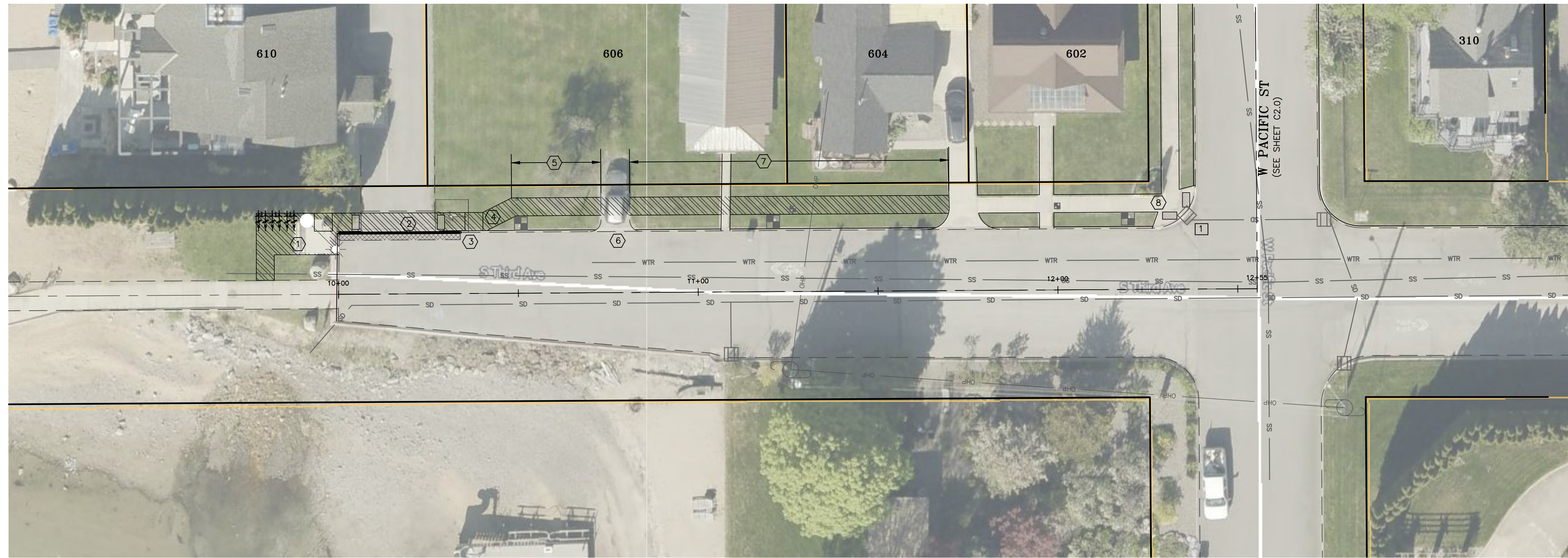
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PATH: \\S:\PROJECTS\2025\25-0250 - SANDPOINT SIDEWALK\2025\25-0250 - SANDPOINT SIDEWALK\DRAWINGS		



C2.0
SHEET
5 OF 9

UNAUTHORIZED REPRODUCTION OR TRANSLATION OF ANY PART OF THIS WORK WITHOUT THE WRITTEN PERMISSION OF VAN HOUTEN CONSULTING AND DESIGN, LLC IS STRICTLY PROHIBITED.

S THIRD AVE
 STA:10+00.00 TO END



SANDPOINT SIDEWALK PROJECT 2025
 3RD AVE
 SANDPOINT, IDAHO

REVISIONS		BY
NO.	DATE	DESCRIPTION

DATE: 08/27/2025	PROJECT: #25-025	PARCEL: N/A	DRAWN: SRP	CHECKED: MWI	DATE: 08/27/2025	PROJECT: #25-025	PARCEL: N/A	DRAWN: SRP	CHECKED: MWI
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C3.0
SHEET
6 OF 9

UNAUTHORIZED REPRODUCTION OR TRANSLATION OF ANY PART OF THIS WORK WITHOUT THE WRITTEN PERMISSION OF VAN HOUTEN CONSULTING AND DESIGN, LLC IS STRICTLY PROHIBITED.

S THIRD AVE REFERENCE NOTES			
NUMBER	STATION	OFFSET	DESCRIPTION
①	10+00	17.5'L	SEE PIER SIDEWALK ENLARGED DETAIL, THIS SHEET.
②	10+06-10+26	17.5'L	REMOVE ALL ASPHALT AND CONCRETE CURB BETWEEN SAWCUT LINES. CONSTRUCT 20 LF CONCRETE SIDEWALK ACROSS DRIVEWAY, 5 FT WIDE AND 6" THICK. MATCH TOP BACK OF WALK TO ASPHALT ELEVATION. 1.5% MAX SIDEWALK CROSS SLOPE.
③	10+26	17.5'L	CONSTRUCT (1) PEDESTRIAN RAMP (TYPE C3, ISPCW SD-712D) W/ DETECTABLE WARNING STRIP SAWCUT ASPHALT; REMOVE AND RECONSTRUCT 15 LF OF 6" VERTICAL CURB (NO GUTTER) AND ASPHALT.
④	10+40	21.5'L	CONSTRUCT TAPER TO SEPARATE SIDEWALK 4.0 FT FROM CURB. 10 LF OF 5 FT WIDE SIDEWALK. COORDINATE WITH CITY FOR RELOCATION OF ELECTRICAL BOX (STA: 10+31) RETAIN AND PROTECT WATER METER (STA: 10+50)
⑤	10+50-10+70	21.5'L	NO EXISTING SIDEWALK CONSTRUCT 20 LF OF 5 FT WIDE SIDEWALK.
⑥	10+70-10+82	21.5'L	CONSTRUCT 12 LF OF 5 FT WIDE AND 6" THICK SIDEWALK ACROSS EXISTING GRAVEL DRIVEWAY. MATCH TOP BACK OF SIDEWALK TO EXISTING GRAVEL ELEVATION. 1.5% MAX SIDEWALK CROSS SLOPE. PLACE AND COMPACT GRAVEL (3"-) TO CREATE A CONTINUOUS GRADE FROM TOP FACE OF SIDEWALK TO 3RD AVE FLOW LINE.
⑦	10+82-11+70	21.5'L	NO EXISTING SIDEWALK CONSTRUCT 88 LF OF 5 FT WIDE SIDEWALK RECONSTRUCT WALKWAY TO 606 3RD AVE TO HAVE 1.5% MAX CROSS SLOPE RETAIN AND PROTECT EX. IRRIGATION BOX STA: 11+21. CONTRACTOR TO COORDINATE WITH CITY FOR RELOCATION/REPLACEMENT OF WATER METER STA: 11+26
⑧	12+30	17.5'L	SEE PACIFIC ST AND 3RD AVE ENLARGED INTERSECTION DETAIL, SHEET C4.0

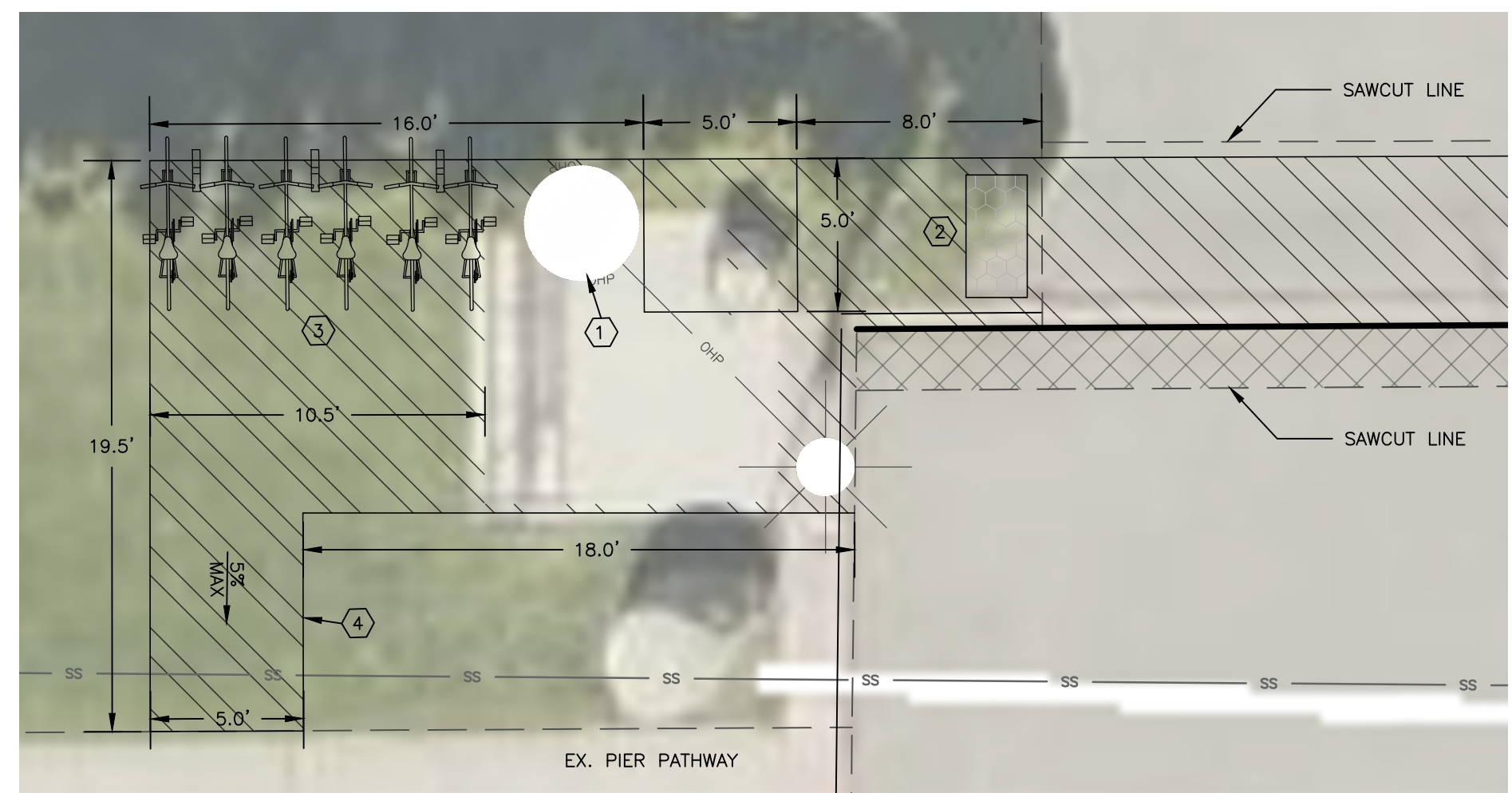
NOTE:
 1. PROTECT AND RETAIN EXISTING CURB TO THE GREATEST EXTENT FEASIBLE UNLESS NOTED OTHERWISE.
 2. CONTRACTOR SHALL CONTACT THE CITY FOR A FORMING INSPECTION PRIOR TO PLACING CONCRETE. ADA TOLERANCES SHALL NOT BE EXCEEDED.
 3. REFER TO SHEET C4.1 FOR HMA ROADWAY PATCHING DETAIL.
 4. REFER TO SHEET C4.1 FOR CITY OF SANDPOINT STANDARD APPROACH DETAIL.
 5. REFER TO ISPCW DWG NO SD-701 & 701A FOR 6" CURB & 6" CURB AND GUTTER DETAILS.
 6. REFER TO ISPCW DWG NO SD-709 FOR CONCRETE SIDEWALK DETAIL.
 7. REFER TO SHEET C4.2 FOR DIVERSION AND RAMPING AT TREES DETAILS.
 8. REFER TO ISPCW SECTION 2030 FOR ALL UTILITY ADJUSTMENTS.

EROSION CONTROL REFERENCE NOTES			
NUMBER	STATION	OFFSET	DESCRIPTION
①	12+36	18.0'L	STORM DRAIN INLET

NOTES:
 1. SEE INLET PROTECTION DETAIL, SHEET C4.1.

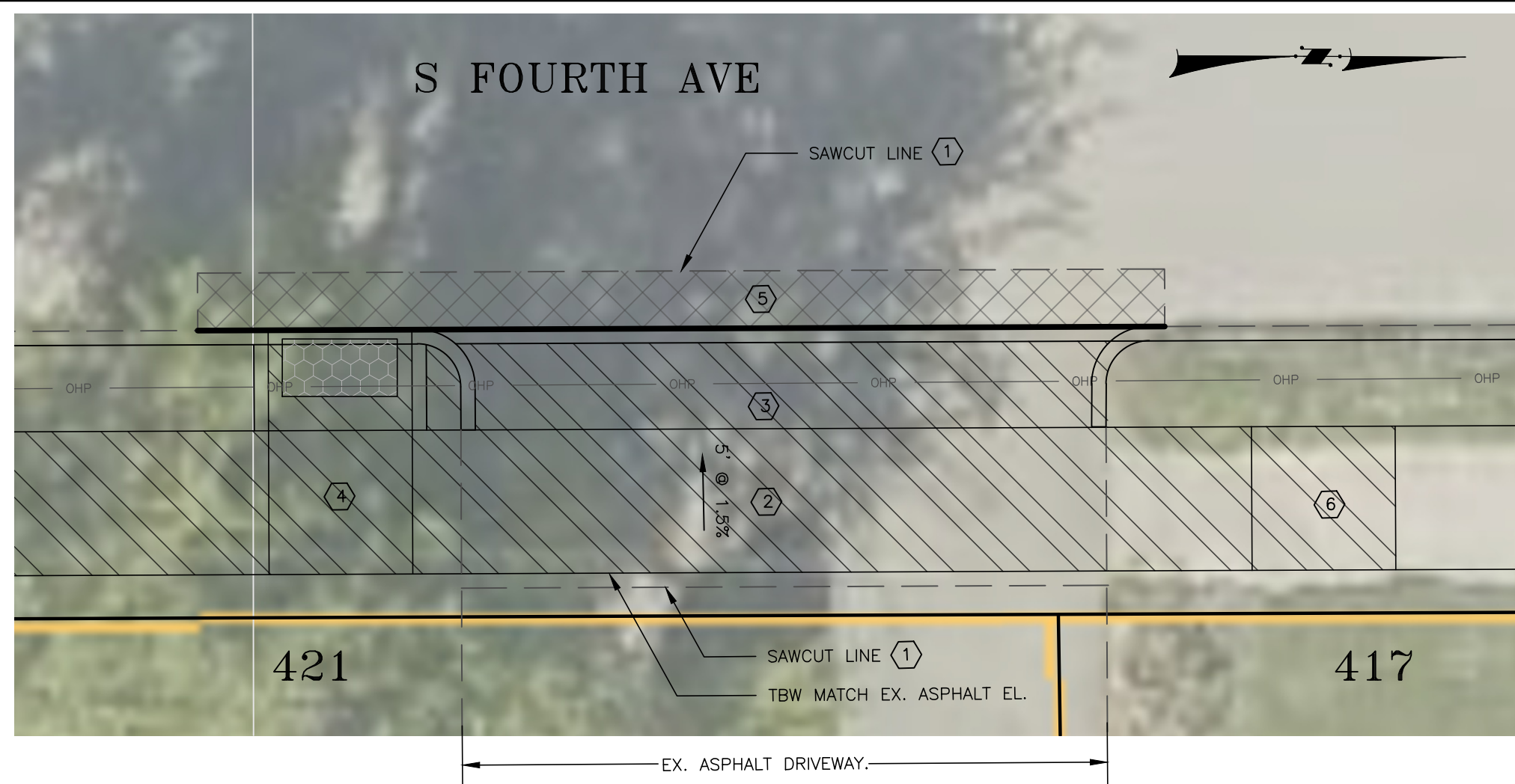
LEGEND

	SIDEWALK CONSTRUCTION
	CURB CONSTRUCTION
	PLANT MIX PAVEMENT



PIER SIDEWALK
S THIRD AVE
 SCALE 1"=5'

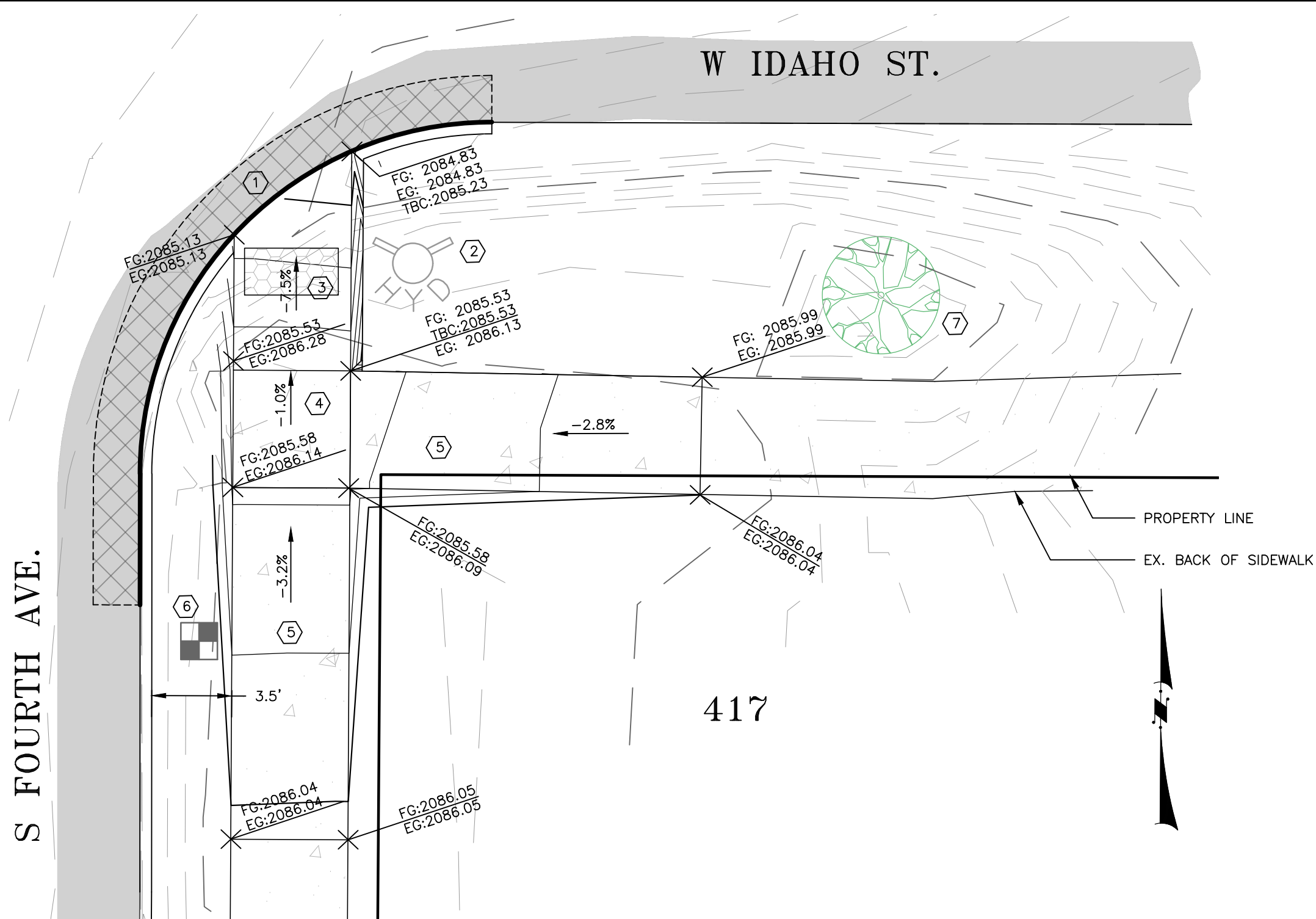
REFERENCE NOTES	
①	RELOCATE STREET LIGHT. CONTRACTOR TO COORDINATE WITH AVISTA.
②	CONSTRUCT (1) PEDESTRIAN RAMP (TYPE C3, ISPCW SD-712D) W/ DETECTABLE WARNING STRIP SAWCUT ASPHALT; REMOVE AND RECONSTRUCT 15 LF OF 6" VERTICAL CURB (NO GUTTER) AND ASPHALT.
③	EXPAND EXISTING CONCRETE PAD TO 5.0' WEST OF TBC AND 10.5' SOUTH OF EXISTING PAD. RELOCATE BIKE RACK AS SHOWN.
④	CONSTRUCT 5' WIDE SIDEWALK WITH 5% MAX RUNNING SLOPE FROM CONCRETE PAD TO PIER PATHWAY.



ENLARGED DETAIL - 421 S FOURTH AVE
REFERENCE NOTES

NUMBER	DESCRIPTION
①	REMOVE ALL ASPHALT AND CONCRETE CURB BETWEEN SAWCUT LINES.
②	CONSTRUCT CONCRETE SIDEWALK ACROSS DRIVEWAY, 5 FT WIDE AND 6" THICK. MATCH TOP BACK OF WALK TO EX. ASPHALT ELEVATION. 1.5% MAX SIDEWALK CROSS SLOPE.
③	CONSTRUCT CONCRETE APPROACH APRON. MATCH ELEVATION AT FACE OF SIDEWALK AND FOURTH AVE FLOW LINE. USE TYPE A CURB ON NORTH AND SOUTH SIDES OF DRIVEWAY FOR 0.0' TO FULL CURB HEIGHT REVEAL. 1.5'± TBC RADIUS.
④	5' X 5' ADA LANDING 1.5% MAX SLOPE IN ANY DIRECTION 5'W X 3.5'L ADA RAMP W/ DETECTABLE WARNING STRIP 8.3% MAX SLOPE 0.0' TO FULL CURB HEIGHT REVEAL ON NORTH AND SOUTH SIDES.
⑤	RECONSTRUCT ASPHALT IN 4TH AVE.
⑥	REMOVE EXISTING SIDEWALK STEP AND TWO (2) NEAREST SIDEWALK PANELS. CONSTRUCT 10 LF OF 5.0% MAX SLOPE SIDEWALK FROM DRIVEWAY ELEVATION TO EX. SIDEWALK ELEVATION.

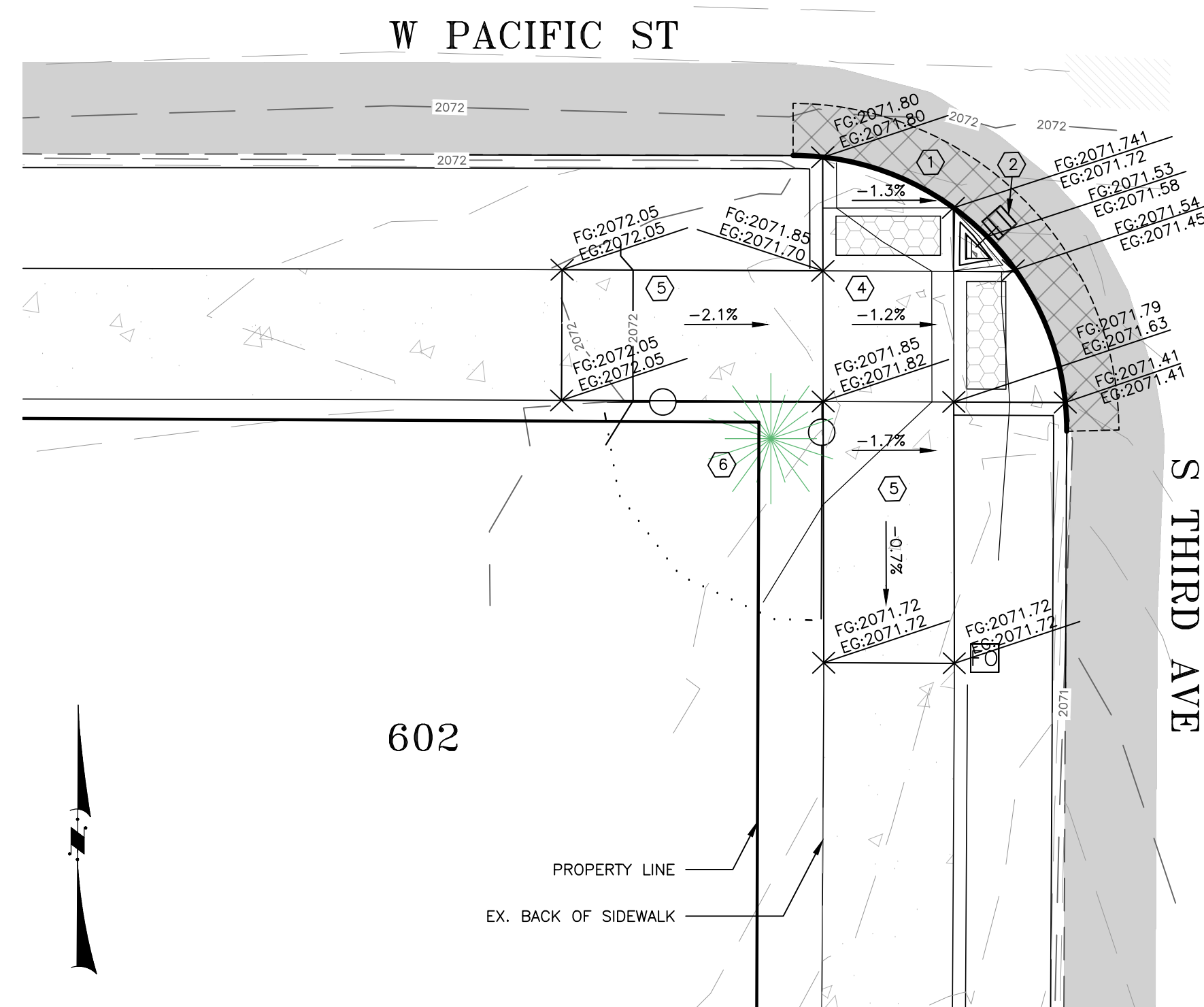
NOTE:
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 4. REFER TO SHEET C4.1 FOR CITY OF SANDPOINT STANDARD APPROACH DETAIL.
 5. REFER TO ISPCW DWG NO SD-701 & 701A FOR 6" CURB & 6" CURB AND GUTTER DETAILS.
 6. REFER TO ISPCW DWG NO SD-709 FOR CONCRETE SIDEWALK DETAIL.
 7. REFER TO SHEET C4.2 FOR DIVERSION AND RAMPING AT TREES DETAILS.
 8. REFER TO ISPCW SECTION 2030 FOR ALL UTILITY ADJUSTMENTS.



ENLARGED DETAIL - 417 S FOURTH AVE
REFERENCE NOTES

NUMBER	DESCRIPTION
①	REMOVE ALL ASPHALT AND CONCRETE CURB BEHIND SAWCUT LINE. REMOVE ALL EXISTING SIDEWALK WITH LIMITS OF DISTURBANCE.
②	RETAIN AND PROTECT EXISTING FIRE HYDRANT
③	CONSTRUCT (1) PEDESTRIAN RAMP (TYPE "C1", ISPCW SD-712D) W/ DETECTABLE WARNING STRIP 8.3% MAX SLOPE FOR GRADING AND SPOT ELEVATIONS SEE THIS SHEET.
④	5' X 5' ADA LANDING 1.0% SLOPE IN ANY DIRECTION
⑤	CONSTRUCT 15LF OF 5' WIDE SIDEWALK TO THE SOUTH AND EAST OF LANDING.
⑥	COORDINATE ADJUSTMENT OF WATER METER BOX WITH CITY. REESTABLISH LAWN AND IRRIGATION SYSTEM TO PRE-CONSTRUCTION CONDITIONS OR BETTER.
⑦	EX TREE - RETAIN AND PROTECT.

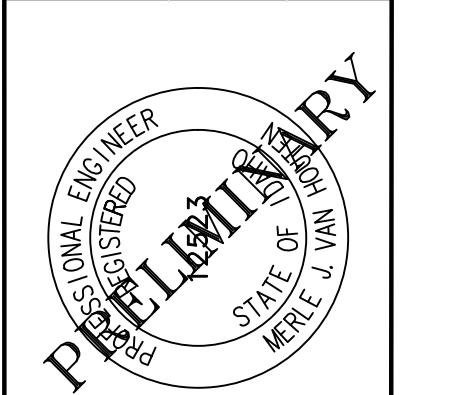
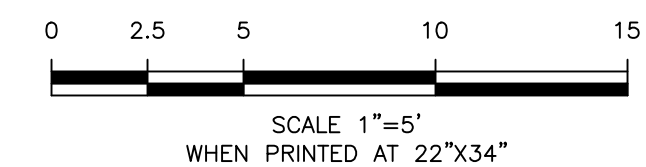
NOTE:
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 3. REFER TO SHEET C4.1 FOR HMA ROADWAY PATCHING DETAIL.
 4. REFER TO SHEET C4.1 FOR CITY OF SANDPOINT STANDARD APPROACH DETAIL.
 5. REFER TO ISPCW DWG NO SD-701 & 701A FOR 6" CURB & 6" CURB AND GUTTER DETAILS.
 6. REFER TO ISPCW DWG NO SD-709 FOR CONCRETE SIDEWALK DETAIL.
 7. REFER TO SHEET C4.2 FOR DIVERSION AND RAMPING AT TREES DETAILS.
 8. REFER TO ISPCW SECTION 2030 FOR ALL UTILITY ADJUSTMENTS.



ENLARGED DETAIL - 602 S 3RD AVE
REFERENCE NOTES

NUMBER	DESCRIPTION
①	REMOVE ALL ASPHALT AND CONCRETE CURB BEHIND SAWCUT LINE. REMOVE ALL EXISTING SIDEWALK WITHIN LIMITS OF DISTURBANCE.
②	RETAIN AND PROTECT EXISTING CATCH BASIN. INSTALL INLET PROTECTION, PER DETAIL SHEET C4.1.
③	CONSTRUCT (2) PEDESTRIAN RAMP (TYPE "F1", ISPCW SD-712E) W/ DETECTABLE WARNING STRIPS
④	5' X 5' ADA LANDING 1.2% SLOPE TOWARDS S 3RD AVE.
⑤	CONSTRUCT 10 LF OF 5FT WIDE SIDEWALK TO THE SOUTH AND WEST OF THE LANDING
⑥	RETAIN AND PROTECT EXISTING FENCE AND LANDSCAPING TO THE GREATEST EXTENT POSSIBLE. ALL DISTURBED AREA SHALL BE RETURNED TO PRE-CONSTRUCTION CONDITION OR BETTER.

NOTE:
 1. PROTECT AND RETAIN EXISTING CURB TO THE GREATEST EXTENT FEASIBLE UNLESS NOTED OTHERWISE.
 2. CONTRACTOR SHALL CONTACT THE CITY FOR A FORMING INSPECTION PRIOR TO PLACING CONCRETE. ADA TOLERANCES SHALL NOT BE EXCEEDED.
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 4. REFER TO SHEET C4.1 FOR CITY OF SANDPOINT STANDARD APPROACH DETAIL.
 5. REFER TO ISPCW DWG NO SD-701 & 701A FOR 6" CURB & 6" CURB AND GUTTER DETAILS.
 6. REFER TO ISPCW DWG NO SD-709 FOR CONCRETE SIDEWALK DETAIL.
 7. REFER TO SHEET C4.2 FOR DIVERSION AND RAMPING AT TREES DETAILS.
 8. REFER TO ISPCW SECTION 2030 FOR ALL UTILITY ADJUSTMENTS.



**SANDPOINT SIDEWALK
PROJECT 2025**

ENLARGED DETAILS

SANDPOINT, IDAHO

REVISIONS		BY
NO.	DATE	DESCRIPTION

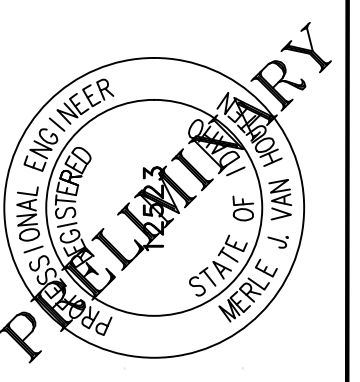
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PARCEL: N/A	DRAWN: SP
CHECKED: MW	DATE: 08/07/2025
2025 DESIGN DWG/CIVIL PRODUCTION	

C4.0

SHEET

7 OF 9

UNAUTHORIZED REPRODUCTION OR TRANSLATION OF ANY PART OF THIS WORK WITHOUT THE WRITTEN PERMISSION OF VAN HOUTEN CONSULTING AND DESIGN, LLC IS STRICTLY PROHIBITED.



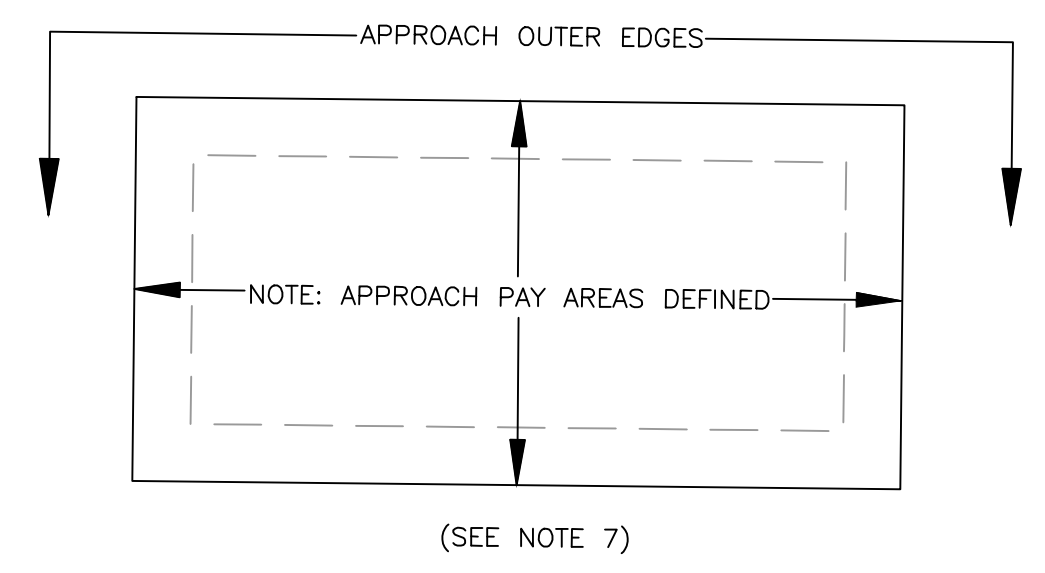
SANDPOINT SIDEWALK
PROJECT 2025
DETAILS
SANDPOINT, IDAHO

REVISIONS		BY
NO.	DATE	DESCRIPTION

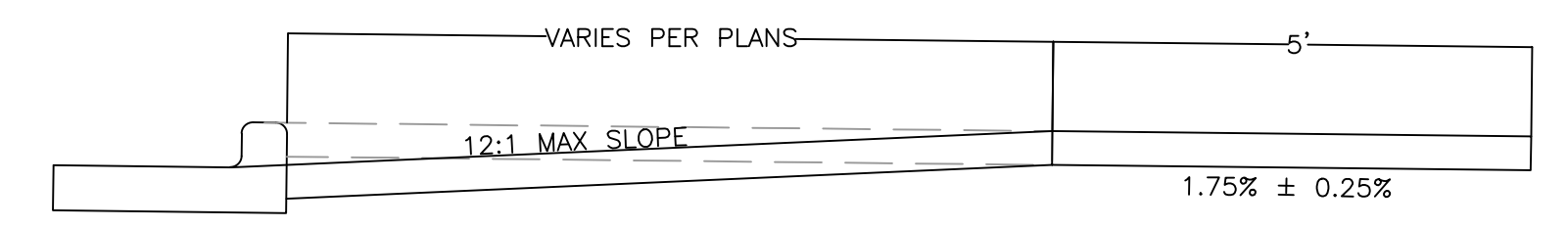
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C4.1
SHEET
8 OF 9

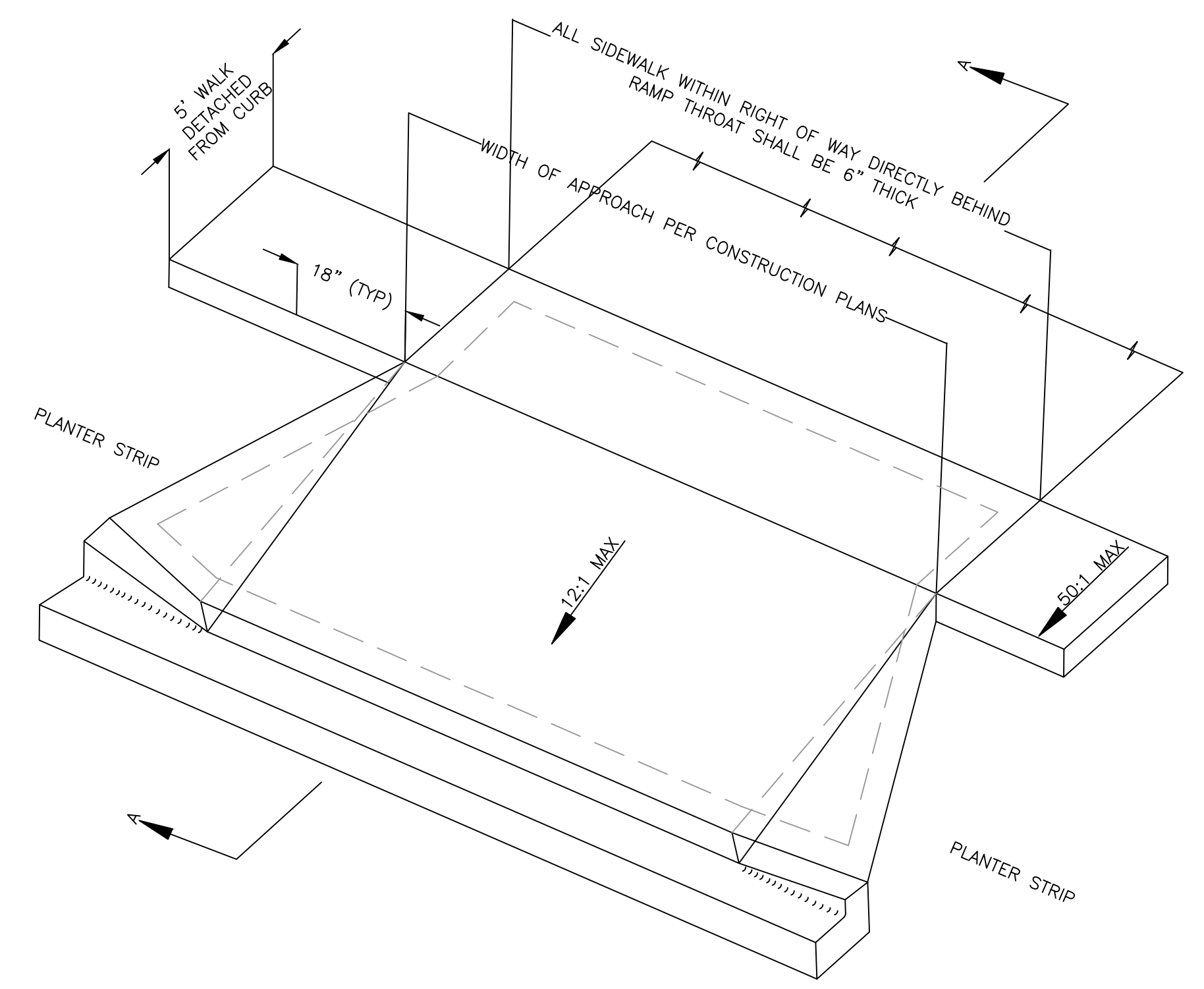
UNAUTHORIZED REPRODUCTION OR TRANSLATION OF ANY PART OF THIS WORK WITHOUT THE WRITTEN PERMISSION OF VAN HOUTEN CONSULTING AND DESIGN, LLC IS STRICTLY PROHIBITED.



PAY QUANTITY DETAIL



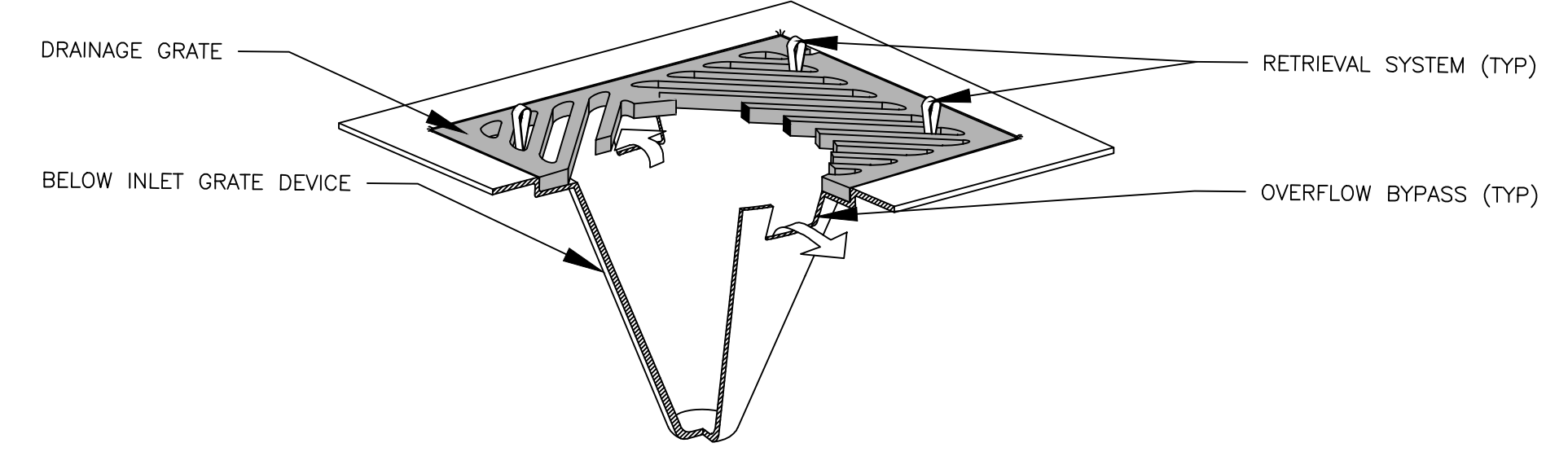
SECTION A-A



- NOTES:**
- APPROACH TO CONFORM THE THE LATEST ADA STANDARDS.
 - INSTALL EXPANSION JOINT AT TIP OF APPROACH WINGS AND WHERE SIDEWALK CHANGES THICKNESS.
 - BASE TO BE A 4" THICKNESS OF 3/4" MINUS CRUSHED AGGREGATE.
 - APPROACH THROAT WIDTHS TO MATCH EXISTING UNLESS NOTED OTHERWISE ON THESE PLANS. ALL CONCRETE TO BE 6" THICK FROM TIP OF WING TO TIP OF WING UP UP TO THE EXPANSION JOINT. WHEN SIDEWALK IS SEPARATE FROM CURB THE SIDEWALK IMMEDIATELY BEHIND THE APPROACH THROAT SHALL BE 6" THICK.
 - ALL CONCRETE SHALL BE CLASS 3000.
 - SIDEWALK WIDTH MAY VARY.
 - PAY QUANTITIES FOR URBAN APPROACHES SHALL INCLUDE THE APPROACH RAMP/DRIVEWAY AREA, THE APPROACH FLARES/WINGS AND SIDEWALK DIRECTLY BEHIND RAMP THROAT.
 - 3/4" REBAR @ 18" O.C. BOTH WAYS IN 6" APRON AND WALK IS REQUIRED, OR EQUIVALENT AS DETERMINED BY CITY.

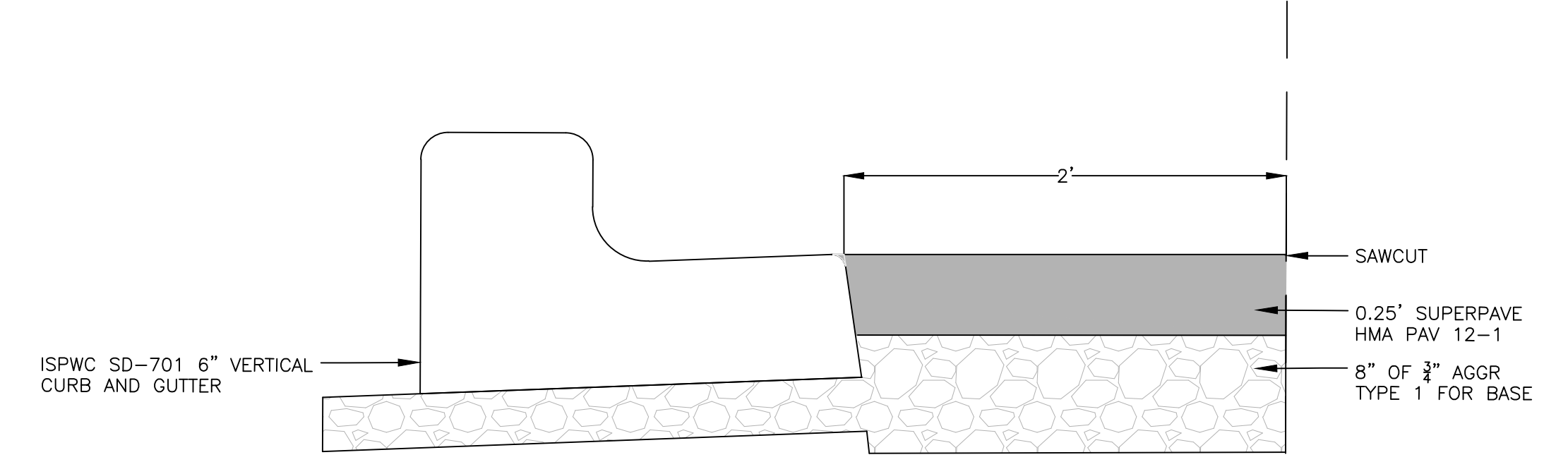
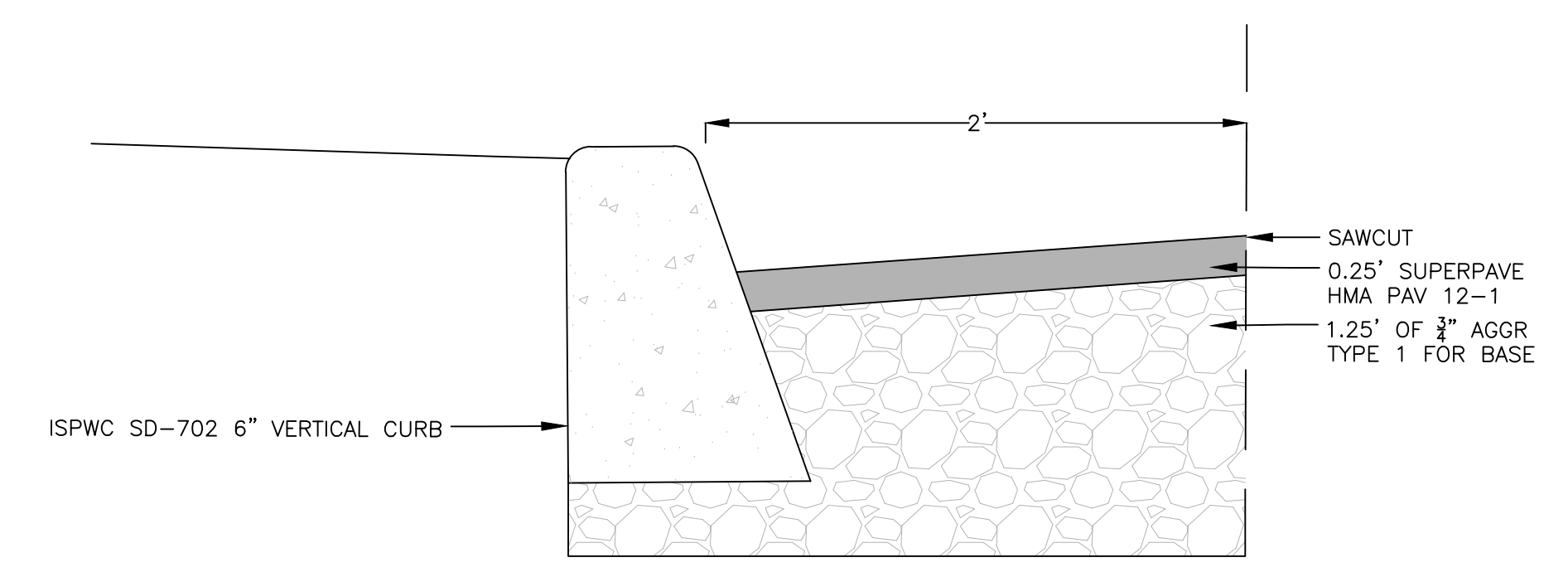
CITY OF SANDPOINT STANDARD APPROACH
N.T.S.

- NOTES:**
- PLACE FILTER FABRIC BELOW THE GRATE WITH 0.5' MINIMUM TUCKED AROUND THE GRATE SIDES TO ENSURE THAT THE FILTER IS SECURE.
 - SIZE THE BELOW INLET GRATE DEVICE (BIGD) FOR THE STORM WATER STRUCTURE IT WILL SERVICE.
 - THE BIGD SHALL HAVE A BUILT-IN HIGH-FLOW RELIEF SYSTEM (OVERFLOW BYPASS).
 - THE RETRIEVAL SYSTEM MUST ALLOW REMOVAL OF THE BIGD WITHOUT SPILLING THE COLLECTED MATERIAL.



BELOW INLET GRATE DEVICE

INLET PROTECTION DETAIL
N.T.S.



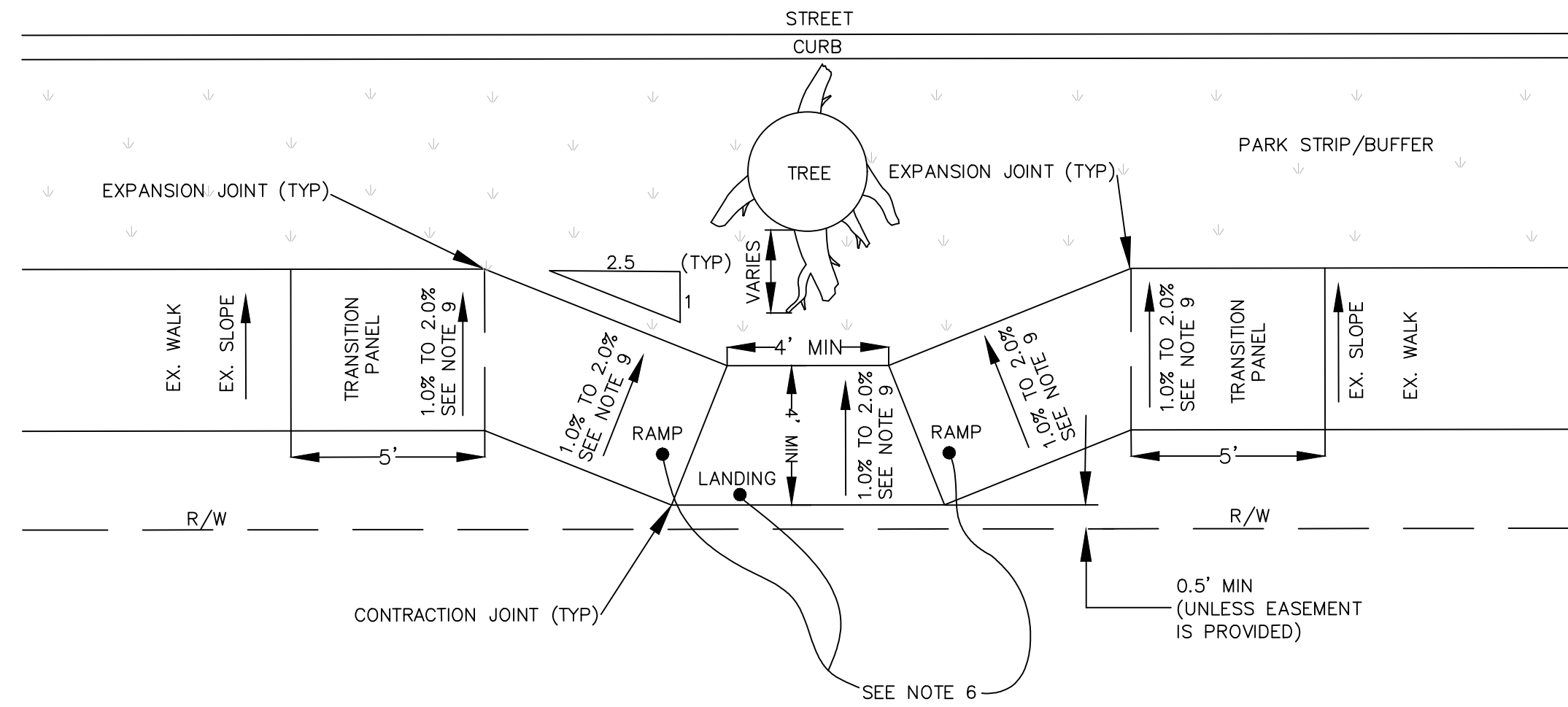
- NOTE:**
- REFER TO ISPWC DWG NO. SD-303 FOR FURTHER DETAIL.

HMA ROADWAY PATCHING
N.T.S.





Know what's below.
Call before you dig.

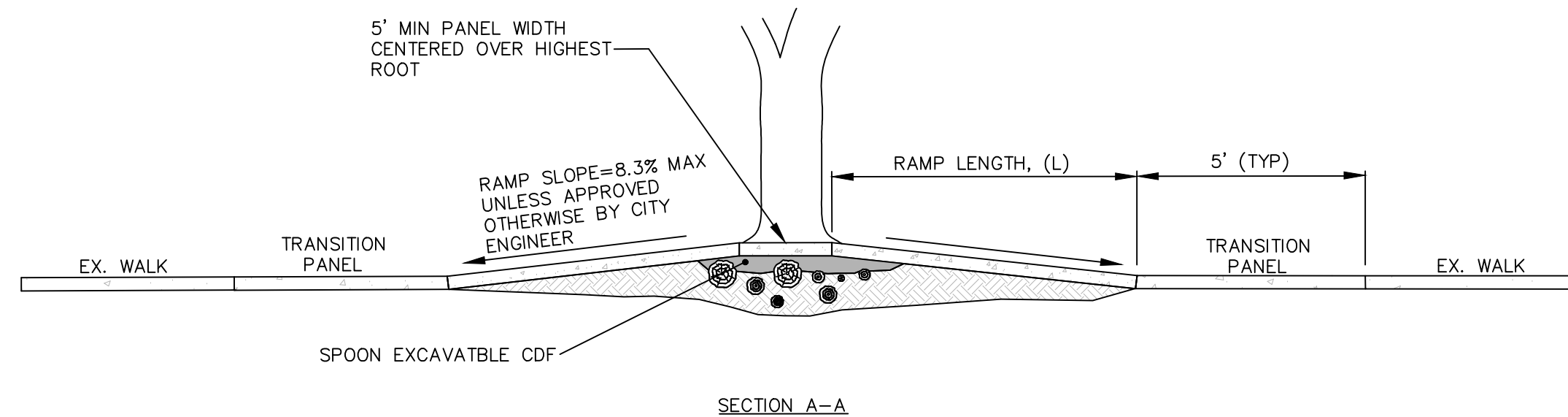


SIDEWALK REPAIR DIVERSION AT TREES

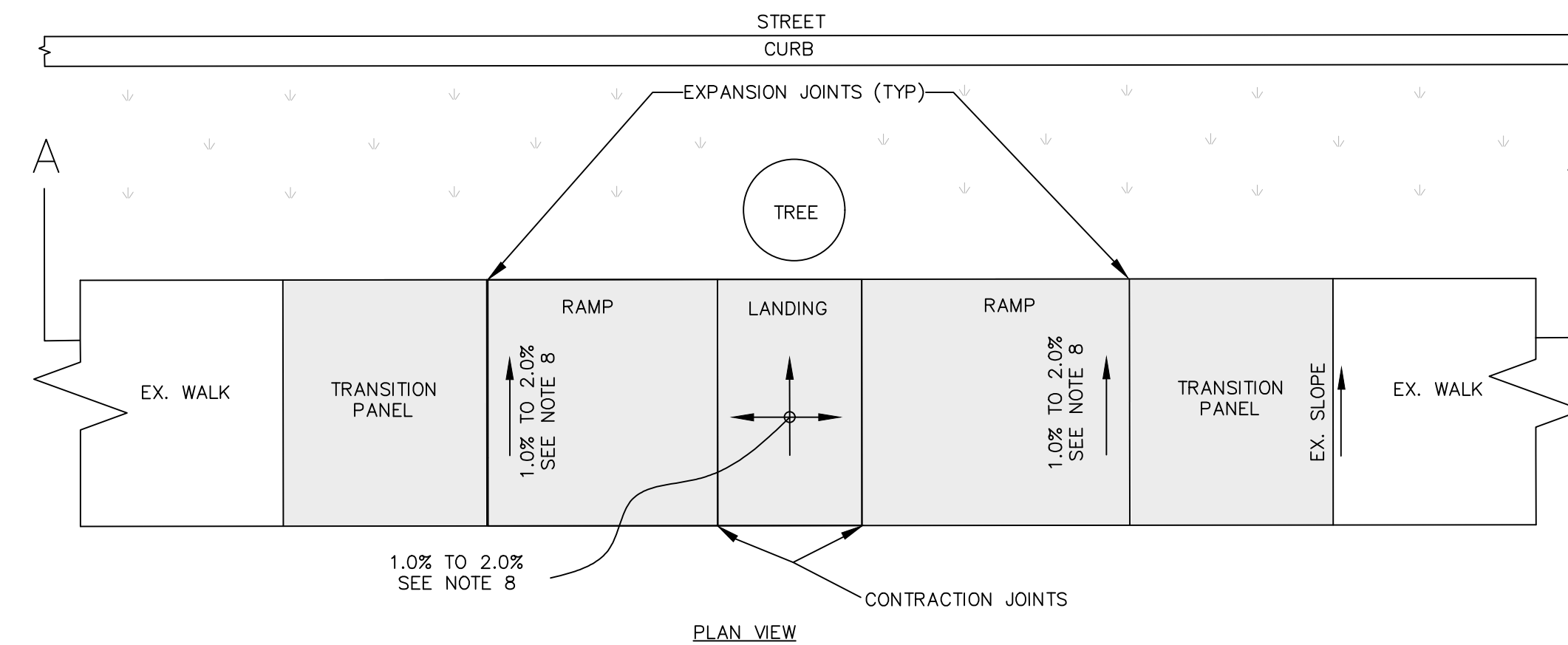
N.T.S.

NOTES:

1. THIS PLAN DOES NOT APPLY FOR A NEW SIDEWALK CONSTRUCTION IN UNDEVELOPED AREAS.
2. 5' TRANSITION PANELS ARE REQUIRED WHEN CROSS SLOPE OF ADJACENT EXISTING WALK EXCEEDS 2.0%.
3. ROOT AREAS SHALL BE UNDISTURBED AS MUCH AS PRACTICAL. LOOSE SOIL SHALL BE LIGHTLY HAND TAMPED. IF ROOT TRIMMING IS NECESSARY, CONTACT THE CITY URBAN FORESTRY DEPARTMENT.
4. TYPICAL SIDEWALK DIVERSION ANGLE SHALL BE 2.5 TO 1. THE DIVERSION ANGLE MAY BE INCREASED TO 1 TO 1 AS DIRECTED BY THE CITY.
5. BACK OF SIDEWALK SHALL BE A MINIMUM OF 0.5' INSIDE OF THE RIGHT-OF-WAY. SIDEWALK WIDTH MAY BE DECREASED TO 3' AS DIRECTED BY THE ENGINEER TO ENSURE SIDEWALK IS WITHIN THE RIGHT-OF-WAY.
6. USE IN CONJUNCTION WITH RAMPING DETAIL, THIS SHEET, WHEN RAMPING OVER AND DIVERTING AROUND TREE ROOTS IS REQUIRED.
7. PLACE TOPSOIL AND HYDROSEED OR SOD AS DIRECTED BY THE CITY.
8. RELOCATE OR REPLACE EXISTING SPRINKLER SYSTEMS AS NEEDED.
9. 1.0% MINIMUM CROSS SLOPE AND 2.0 MAXIMUM CROSS SLOPE. NO ADDITIONAL CONSTRUCTION TOLERANCE IS ALLOWED.



SECTION A-A



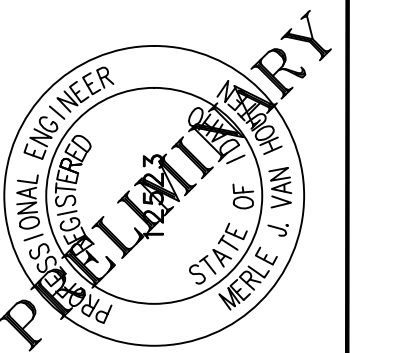
PLAN VIEW

SIDEWALK REPAIR RAMPING AT TREES

N.T.S.

NOTES:

1. THIS PLAN DOES NOT APPLY FOR A NEW SIDEWALK CONSTRUCTION IN UNDEVELOPED AREAS.
2. A 5' TRANSITION PANEL IS REQUIRED WHEN CROSS SLOPE OF ADJACENT EXISTING WALK EXCEEDS 2.0%.
3. THE MAXIMUM RAMP RUNNING SLOPE SHALL NOTE REQUIRE THE RAMP LENGTH (L) TO EXCEED 15' TO AVOID CHASING THE SLOPE INDEFINITELY; INCREASE THE MAXIMUM RUNNING SLOPE AS DIRECTED BY THE ENGINEER. NO ADDITIONAL CONSTRUCTION TOLERANCE IS ALLOWED.
4. ROOTED AREAS SHALL BE UNDISTURBED AS MUCH AS PRACTICAL. LOOSE SOIL SHALL BE LIGHTLY HAND TAMPED. IF ROOT TRIMMING IS NECESSARY CONTACT THE CITY URBAN FORESTRY DEPARTMENT.
5. FILL VOIDS AROUND ROOTS TO PROVIDE SIDEWALK SUPPORT WITH LAYER OF SPOON EXCAVATABLE CONTROLLED DENSITY FILL (CDF). A 1" MINIMUM COVER OVER THE HIGHEST ROOT IS REQUIRED.
6. PLACE TOPSOIL AND HYDROSEED OR SOD AS DIRECTED BY THE CITY TO MATCH EXISTING CONDITIONS.
7. RAISE, RELOCATE, OR REPLACE EXISTING SPRINKLER SYSTEMS AS NEEDED.
8. 1.0% MINIMUM CROSS SLOPE AND 2.0 MAXIMUM CROSS SLOPE. NO ADDITIONAL CONSTRUCTION TOLERANCE IS ALLOWED.



SANDPOINT SIDEWALK
PROJECT 2025
DETAILS
SANDPOINT, IDAHO

REVISIONS		BY
NO.	DATE	DESCRIPTION

DATE: 08/27/2025	PROJECT: #25-025	PARCEL: N/A	DRAWN: SRP	CHECKED: MWI	PATH: A:\PROJECTS\2025\25-0250 - 2025 DESIGN URM\DWG\PRODUCTION DRAWINGS
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C4.2
SHEET
9 OF 9

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Development Review Subcommittee Notes 8-28-25

Present: Katie, Radley, Jen, Molly

Suggested Recommendations for SPBAC to submit on Current Projects

Ella Cottages (418 N. Ella)

Walkway from sidewalk to each cottage should be as direct as possible, not 'wavy.' It is the way people want to walk and is easier for a visually impaired person. Walkway can be further from cottages, to preserve privacy but make someone walking past not feel intrusive. A 5' walkway is recommended.

Walkway from sidewalk to closest building circles a tree. That is consistent with Sandpoint policy.

Does walkway to building closest to alley go to building entrance? Should it continue all the way to the alley to provide a way to take bicycles or walk to alley without going through the gravel?

At street-front, is space between sidewalk and curb sufficient for snow storage?

Excellent that a curb cut that is not needed will be eliminated.

O'Connell Estates (Woodland Dr., Samuelson on west, Boyer on east through Lot 3, which is under same ownership)

A 10'-12' public walk/bike path should be provided from the end of the cul-de-sac to Samuelson.

Sidewalk to be provided on Woodland Dr., with a curb ramp at Beaver Ave.

What is the outlet for the new cul-de-sac? If it is a roadway east of Lots 1 (ie Holstein Place), then sidewalks, curb ramps facing each direction of travel, etc. are required. (Truncated dome panel installation should be metal, not plastic/rubber for durability.)

Provide an RRFB (Rapid Rectangular Flashing Beacon) at Boyer and Woodland. Or provide two RRFBs: mid-block close to Boyer and Jersey, and mid-block near Woodland and Samuelson.

Allshouse Condo Plat – Pine Street Cottages (1211 Pine St.)

Sidewalk on Pine Street is important and needs to be required.

Applicant needs to show how each dwelling unit will be connected to the public sidewalk on Pine Street, as directly as possible. A 5' walkway is recommended. Walkway needs to be separate from driveway.

Should walkway continue to the alley?

GREAT NORTHERN COMMERCE PARK FIRST ADDITION

Curb ramps at each intersection should face directly in both directions of travel.

Sidewalk on Woodland Dr. in addition to within the project.

56 Bridge Street - Extension of Conditional Use Permit & Variance (New hotel on waterfront)

The current extension has been issued. The applicant is not planning to provide sidewalk on Sandpoint Ave., which is a major walkway to the condos north, the Pend Oreille Bay Trail and the City marina. The existing parking lot is used heavily by pedestrian and cyclist through-traffic. SPBAC requests information from the City on when it may comment in favor of a sidewalk.

Subcommittee also submits to full SPBAC its recommendations on *Ordinance 7-3-10 New Construction of Sidewalks; Improvements* in a separate document.

Intersection Committee Notes for meeting 8-25-25
Present: Jen, Sally, Julie, Molly

Unanimous subcommittee agreement to bring its prior curb ramp recommendation to PBAC for vote/approval at next meeting.

Chapter 3, 7-3-10 E-1-c Sidewalks shall be designed and constructed in accordance with the design and construction standards **including curb ramps facing each intersecting roadway when the lot abuts an intersection.**

In lieu discussion:

Subcommittee members are looking forward to staff recommendations on criteria for City Engineer approval, when in lieu by his/her judgement is appropriate.

The subcommittee affirmed the proposal discussed at SPBAC in August to Eliminate 10- G) 1-c.

~~The property owner elects to pay a fee in lieu of construction as an alternative to meeting the construction requirements of this chapter.~~

The subcommittee recommends adding to the list of criteria for in lieu:

Chapter 3, 7-3-G-a.

(6) Always on the last block of dead-end streets

Division Ave. phase 2 – Superior to Pine, east side

Subcommittee recommends to the full SPBAC:

Curb radii of 12' at all intersections. (per NACTO recommendations)

Paired curb ramps at Lake Street crossing Division as well as continuing North-South

Submitted,
Molly O'Reilly

MTMP Subcommittee Meeting Notes 8-29-25

Present: Katie, Reid, Radley, Molly

Subcommittee was unanimous that it should categorize all Sandpoint streets for pedestrian priority with the comprehensive 2004 list as a starting point. All streets are categorized for motor vehicle use, and that categorization affects design, grants, and planning. This categorization should be included in the MTMP and have other uses. When that is completed, establishing bicycle priority streets will be tackled.

The 2004 categorization lists the BID as the downtown unit. Is the BID still relevant? If not, what is in use today that should be cited? What are the boundaries?

The subcommittee started by comparing the High Priority 2004 streets with those listed in the current MTMP, Appendix A, looking at vehicular traffic levels, destinations and origins of pedestrian trips, and SPOT bus stops. Modifications were made to the proposed list.

The subcommittee next looked at the rationale for the 2004 Medium Priority streets, moving some to high priority and vice versa. The subcommittee paid special attention to areas of the city with new development, job growth, SPOT stops, and changes in traffic patterns. The subcommittee assumed that Safe Routes to School streets should remain, as there is always some fluctuation in the location of families with children, but schools are in the same locations.

The subcommittee reviewed Low Priority streets, where no sidewalk exists or would be required. This would seem to be an area of in lieu revenue. The subcommittee is considering whether to add the final block of a cul d'sac where no vehicular extension is possible to the "Low Priority" category.

At its next meeting, the subcommittee plans to review and finalize its work on Pedestrian Priority categorization for submission to the full SPBAC in October.