



## **AGENDA**

### **CITY COUNCIL, NORMAN UTILITIES AUTHORITY, NORMAN MUNICIPAL AUTHORITY, AND NORMAN TAX INCREMENT FINANCE AUTHORITY**

*It is the policy of the City of Norman that no person or groups of persons shall on the grounds of race, color, religion, ancestry, national origin, age, place of birth, sex, sexual orientation, gender identity or expression, familial status, marital status, including marriage to a person of the same sex, disability, retaliation, or genetic information, be excluded from participation in, be denied the benefits of, or otherwise subjected to discrimination in employment activities or in all programs, services, or activities administered by the City, its recipients, sub-recipients, and contractors. In the event of any comments, complaints, modifications, accommodations, alternative formats, and auxiliary aids and services regarding accessibility or inclusion, please contact the ADA Technician at 405-366-5424, Relay Service: 711. To better serve you, five (5) business days' advance notice is preferred.*

You are required to sign up in advance of the meeting on the City's webpage, by calling the City Clerk's Office (405-366-5406), or at the Council Chambers prior to the start of the meeting with your name, ward, and item you wish to speak to including whether you are a proponent or opponent. When the time comes for public comments, the Clerk will call your name and you can make your way to the podium. Comments may be limited on items of higher interest, if so, the Mayor will announce that at the beginning of the meeting. Participants may speak one time only up to 3 minutes per person per item. There will be no yielding of time to another person. Sign up does not guarantee you will get to speak if the allotted time for that item has already been exhausted. If there is time remaining after those registered to speak have spoken, persons not previously signed up may have the opportunity to speak. Comments received must be limited to the motion on the floor only.

### **CALL TO ORDER**

### **ROLL CALL**

### **PLEDGE OF ALLEGIANCE**



## **AWARDS AND PRESENTATIONS**

PRESENTATION OF AWARDS FROM THE OKLAHOMA MUNICIPAL LEAGUE TO THE FOLLOWING CITY OF NORMAN EMPLOYEES WHO HAVE COMPLETED 25 YEARS OF CONTINUOUS SERVICE:

1. DOUGLAS BIRKHIMER, TRAFFIC SIGNAL TECHNICIAN, PUBLIC WORKS DEPARTMENT
2. MILES COTTEN, CONSTRUCTION INSPECTOR, PUBLIC WORKS DEPARTMENT
3. JACQUELINE CRUMRINE, TRAINING AND DEVELOPMENT MANAGER, HUMAN RESOURCES DEPARTMENT
4. JOEL FORMBY, SERGEANT, POLICE DEPARTMENT
5. ANTHONY FRANCISCO, DIRECTOR OF FINANCE, FINANCE DEPARTMENT
6. PATRICK GOLDSBY, STREETS DIVISION CREW CHIEF, PUBLIC WORKS DEPARTMENT
7. HOMER HARVEY, FIRE DRIVER ENGINEER, FIRE DEPARTMENT
8. RICHARD KING, STORMWATER MAINTENANCE. WORKER I, PUBLIC WORKS DEPARTMENT
9. TRAVIS KING, FIRE CHIEF, FIRE DEPARTMENT
10. SHANE RAINES, FIRE DRIVER ENGINEER, FIRE DEPARTMENT
11. VERACHAI SANITPHAN, FLEET MECHANIC II, PUBLIC WORKS DEPARTMENT
12. GARY SCHMIDT, DETECTIVE, POLICE DEPARTMENT
13. KEITH SCOTT, FIRE DRIVER ENGINEER, FIRE DEPARTMENT
14. JERALD SMALL, PARK MAINTENANCE HEAVY EQUIPMENT OPERATOR, PARKS AND RECREATION DEPARTMENT
15. JONATHAN STAMPER, FIREFIGHTER, FIRE DEPARTMENT
16. GAY WEBB, ADMINISTRATIVE TECHNICIAN IV, UTILITIES DEPARTMENT
17. MIKE WHITE, FLEET PROGRAM MANAGER, PUBLIC WORKS DEPARTMENT

## **PROCLAMATIONS**

2. CONSIDERATION OF ACKNOWLEDGEMENT, ACCEPTANCE, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF PROCLAMATION P-2122-19: A PROCLAMATION OF THE MAYOR OF THE CITY OF NORMAN, OKLAHOMA, PROCLAIMING THE MONTH OF JANUARY, 2022, AS NATIONAL MENTORING MONTH IN THE CITY OF NORMAN.

## **COUNCIL ANNOUNCEMENTS**

### **CONSENT DOCKET**

This item is placed on the agenda so that the City Council, by unanimous consent, can designate those routine agenda items that they wish to be approved or acknowledged by one motion. If any item proposed does not meet with approval of all Councilmembers, that item will be heard in regular order. Staff recommends that Item 3 through Item 10 be placed on the consent docket.

## **Reports/Communications**

- [3.](#) CONSIDERATION OF ACKNOWLEDGEMENT, APPROVAL, ACCEPTANCE, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF RECEIPT OF THE FINANCE DIRECTOR'S INVESTMENT REPORT AS OF DECEMBER 31, 2021, AND DIRECTING THE FILING THEREOF.
- [4.](#) CONSIDERATION OF ACKNOWLEDGEMENT, APPROVAL, ACCEPTANCE, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF THE MONTHLY DEPARTMENTAL REPORT FOR THE MONTH OF DECEMBER, 2021.

## **Surplus/Obsolete Equipment**

- [5.](#) CONSIDERATION OF DECLARATION, APPROVAL, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF SURPLUS AND/OR OBSOLETE EQUIPMENT AND MATERIALS AND AUTHORIZING THE SALE OR DISPOSAL THEREOF.

## **Final Plat**

- [6.](#) CONSIDERATION FOR APPROVAL, ACCEPTANCE, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF A FINAL SITE DEVELOPMENT PLAN AND FINAL PLAT FOR THE YOUNG FAMILY ATHLETIC COMPLEX, (YFAC), A PLANNED UNIT DEVELOPMENT, AND ACCEPTANCE OF PUBLIC DEDICATIONS CONTAINED THEREIN. (LOCATED 525 FEET EAST OF 24TH AVENUE N.W. AND SOUTH OF ROCK CREEK ROAD)

## **Contracts**

- [7.](#) CONSIDERATION OF AWARDING, ACCEPTANCE, APPROVAL, ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF BID 2122-35, CONTRACT K-2122-75 BY AND BETWEEN THE CITY OF NORMAN, OKLAHOMA, AND SILVER STAR CONSTRUCTION COMPANY, INC., IN THE AMOUNT OF \$3,616,910.70, PERFORMANCE BOND B-2122-52, STATUTORY BOND B-2122-53, AND MAINTENANCE BOND MB-2122-41 FOR THE ALAMEDA STREET WIDENING CONSTRUCTION PROJECT FROM RIDGE LAKE BOULEVARD TO 48TH AVENUE S.E., RESOLUTION R-2122-66 GRANTING TAX EXEMPT STATUS, AND BUDGET TRANSFER AS OUTLINED IN THE STAFF REPORT.
- [8.](#) CONSIDERATION OF APPROVAL, ACCEPTANCE, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF CHANGE ORDER NO. FOUR TO CONTRACT K-2021-35 BY AND BETWEEN THE CITY OF NORMAN, OKLAHOMA, AND FLINTCO L.L.C., INCREASING THE CONTRACT AMOUNT BY \$93,249.12 FOR A REVISED CONTRACT AMOUNT OF \$8,983,752.96 AND ADDING 41 CALENDAR DAYS FOR THE NORTH BASE COMPLEX PROJECT, PHASE 1, IN ORDER TO MAKE VARIOUS CHANGES TO THE PROJECT DESIGN AND BUDGET APPROPRIATION AS OUTLINED IN THE STAFF REPORT.

9. CONSIDERATION OF APPROVAL, ACCEPTANCE, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF CONTRACT K-2122-91: A CONTRACT BY AND BETWEEN THE CITY OF NORMAN, OKLAHOMA, AND H.W. LOCHNER, INC., IN THE AMOUNT OF \$61,400 TO PROVIDE CONSULTANT SERVICES FOR THE BRIDGE MAINTENANCE PROGRAM SITE DISCOVERY PROJECT AND BUDGET TRANSFER AS OUTLINED IN THE STAFF REPORT.

### **Resolutions**

10. CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT AND/OR POSTPONEMENT OF RESOLUTION R-2122-82: A RESOLUTION OF THE COUNCIL OF THE CITY OF NORMAN APPROPRIATING \$83,778 FROM THE CAPITAL FUND BALANCE TO ADJUST THE LOCAL SHARE DEPOSIT PAYMENT TO THE OKLAHOMA DEPARTMENT OF TRANSPORTATION FOR THE CONSTITUTION STREET MULTIMODAL PATH PROJECT BETWEEN JENKINS AVENUE AND CLASSEN BOULEVARD.

### **NON-CONSENT ITEMS**

11. CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF RESOLUTION R-2122-57: A RESOLUTION OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, AMENDING THE NORMAN 2025 LAND USE AND TRANSPORTATION PLAN SO AS TO REMOVE PART OF THE NORTHWEST QUARTER (NW/4) OF SECTION TWENTY-NINE (29), TOWNSHIP NINE (9) NORTH, RANGE TWO (2) WEST OF THE INDIAN MERIDIAN (I.M.), CLEVELAND COUNTY, OKLAHOMA, FROM THE INSTITUTIONAL DESIGNATION AND PLACE THE SAME IN THE MIXED USE DESIGNATION. (NRHS PORTER CAMPUS)
12. CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF ORDINANCE O-2122-29 UPON SECOND AND FINAL READING: AN ORDINANCE OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, AMENDING SECTION 460 OF CHAPTER 22 OF THE CODE OF THE CITY OF NORMAN SO AS TO REMOVE PART OF THE NORTHWEST QUARTER (NW/4) OF SECTION TWENTY-NINE (29), TOWNSHIP NINE (9) NORTH, RANGE TWO (2) WEST OF THE INDIAN MERIDIAN, TO NORMAN, CLEVELAND COUNTY, OKLAHOMA, FROM THE R-1, SINGLE FAMILY DWELLING DISTRICT, THE C-3, INTENSIVE COMMERCIAL DISTRICT, AND THE O-1, OFFICE-INSTITUTIONAL DISTRICT, AND PLACE SAME IN THE PUD, PLANNED UNIT DEVELOPMENT DISTRICT; AND PROVIDING FOR THE SEVERABILITY THEREOF. (NORMAN REGIONAL HEALTH SYSTEM PORTER CAMPUS)
13. CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF ORDINANCE O-2122-30 UPON SECOND AND FINAL READING: AN ORDINANCE OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, CLOSING CERTAIN PUBLIC INTERESTS IN AN ALLEY AND RIGHTS-OF-WAY LOCATED AT THE NORMAN REGIONAL HEALTH SYSTEM PORTER CAMPUS SITE; AND PROVIDING FOR THE SEVERABILITY THEREOF.

14. CONSIDERATION OF APPROVAL, ACCEPTANCE, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF A PRELIMINARY PLAT FOR NORMAN REGIONAL HEALTH SYSTEM PORTER CAMPUS ADDITION, A PLANNED UNIT DEVELOPMENT. (GENERALLY LOCATED SOUTH OF ROBINSON STREET BETWEEN PORTER AVENUE AND FINDLAY AVENUE).
15. CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF RESOLUTION R-2122-31: A RESOLUTION OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, AMENDING THE NORMAN 2025 LAND USE AND TRANSPORTATION PLAN SO AS TO REMOVE THE NORTH ONE HUNDRED FIFTY (150) FEET OF LOT TWO (2), BLOCK TEN (10), OF PICKARD ACRES ADDITION, TO NORMAN, CLEVELAND COUNTY, OKLAHOMA, FROM THE LOW DENSITY RESIDENTIAL DESIGNATION, AND THE SOUTH ONE HUNDRED FIFTY-TWO AND ONE-HALF (152.5) FEET OF LOT TWO (2), BLOCK TEN (10), OF PICKARD ACRES ADDITION, TO NORMAN, CLEVELAND COUNTY, OKLAHOMA, FROM THE OFFICE DESIGNATION AND PLACE THE SAME IN THE COMMERCIAL DESIGNATION. (1027 AND 1035 SOUTH BERRY ROAD)
16. CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF ORDINANCE O-2122-15 UPON SECOND AND FINAL READING: AN ORDINANCE OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, AMENDING SECTION 460 OF CHAPTER 22 OF THE CODE OF THE CITY OF NORMAN SO AS TO REMOVE THE NORTH ONE HUNDRED FIFTY (150) FEET OF LOT TWO (2), BLOCK TEN (10), OF PICKARD ACRES ADDITION, TO NORMAN, CLEVELAND COUNTY, OKLAHOMA, FROM THE R-1, SINGLE FAMILY DWELLING DISTRICT, AND PLACE SAME IN THE SPUD, SIMPLE PLANNED UNIT DEVELOPMENT DISTRICT, AND TO REMOVE THE SOUTH ONE HUNDRED FIFTY-TWO AND ONE-HALF (152.5) FEET OF LOT TWO (2), BLOCK TEN (10), OF PICKARD ACRES ADDITION, TO NORMAN, CLEVELAND COUNTY, OKLAHOMA, FROM THE CO, SUBURBAN OFFICE COMMERCIAL DISTRICT, AND PLACE SAME IN THE SPUD, SIMPLE PLANNED UNIT DEVELOPMENT DISTRICT; AND PROVIDING FOR THE SEVERABILITY THEREOF. (1027 AND 1035 SOUTH BERRY ROAD)
17. CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF RESOLUTION R-2122-64: A RESOLUTION OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, AMENDING THE NORMAN 2025 LAND USE AND TRANSPORTATION PLAN SO AS TO REMOVE PART OF THE WEST HALF (W/2) OF THE SOUTHWEST QUARTER (SW/4) OF SECTION TWENTY-NINE (29), TOWNSHIP NINE (9) NORTH, RANGE ONE (1) WEST, OF THE INDIAN MERIDIAN (I.M.), CLEVELAND COUNTY, OKLAHOMA, FROM THE COUNTRY RESIDENTIAL DESIGNATION AND PLACE THE SAME IN THE COMMERCIAL DESIGNATION. (WEST OF OLIPHANT AVENUE BETWEEN ALAMEDA DRIVE AND ALAMEDA STREET)

18. CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF ORDINANCE O-2122-22 UPON SECOND AND FINAL READING: AN ORDINANCE OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, AMENDING SECTION 460 OF CHAPTER 22 OF THE CODE OF THE CITY OF NORMAN SO AS TO REMOVE PART OF THE WEST HALF (W/2) OF THE SOUTHWEST QUARTER (SW/4) OF SECTION TWENTY-NINE (29), TOWNSHIP NINE (9) NORTH, RANGE ONE (1) WEST OF THE INDIAN MERIDIAN, TO NORMAN, CLEVELAND COUNTY, OKLAHOMA, FROM THE RE, RESIDENTIAL ESTATES DWELLING DISTRICT AND PLACE THE SAME IN THE CR, RURAL COMMERCIAL DISTRICT, OF SAID CITY; AND PROVIDING FOR THE SEVERABILITY THEREOF. (WEST OF OLIPHANT AVENUE BETWEEN ALAMEDA DRIVE AND ALAMEDA STREET)
19. CONSIDERATION OF APPROVAL, ACCEPTANCE, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF A PRELIMINARY PLAT FOR ALAMEDA GARDEN CENTER AND A WAIVER FOR DRIVEWAY SPACING. (GENERALLY LOCATED WEST OF OLIPHANT AVENUE BETWEEN ALAMEDA DRIVE AND ALAMEDA STREET).
20. CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF ORDINANCE O-2122-27 UPON SECOND AND FINAL READING: AN ORDINANCE OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, AMENDING SECTION 460 OF CHAPTER 22 OF THE CODE OF THE CITY OF NORMAN SO AS TO GRANT SPECIAL USE FOR MEDICAL MARIJUANA PROCESSING (TIER III) IN THE C-2, GENERAL COMMERCIAL DISTRICT FOR LOT 3C IN BLOCK TWO (2) OF EAST LINDSEY PLAZA SECTION 2, TO NORMAN, CLEVELAND COUNTY, OKLAHOMA; AND PROVIDING FOR THE SEVERABILITY THEREOF. (1228 LINDSEY PLAZA DRIVE)
21. CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF RESOLUTION R-2122-58: A RESOLUTION OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, ADOPTING THE HISTORIC PRESERVATION GUIDELINES TO BE USED BY THE NORMAN HISTORIC DISTRICT COMMISSION IN REVIEWING PROPOSED ADDITIONS, ALTERATIONS, AND DEMOLITIONS TO STRUCTURES LOCATED IN THE HISTORIC DISTRICTS.
22. CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF ORDINANCE O-2122-31 UPON SECOND AND FINAL READING: AN ORDINANCE OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA AMENDING CHAPTER 22 (ZONING ORDINANCE), ARTICLE XI, SPECIFIC DISTRICT REGULATIONS, SECTION 429.3 IN DEFINITIONS DELETING CONSERVATION AND ADDING ORIGINAL, PERIOD OF SIGNIFICANCE, RELOCATION, AND SECRETARY OF THE INTERIOR STANDARDS OF HISTORIC BUILDINGS; EDITS FOR CONSISTENCY OR CORRECTION PURPOSES; ADDING WRITTEN DENIAL REQUIREMENT; ADDING SEVEN DAY NOTICE; EXTENDING TIME LIMIT OF CERTIFICATE OF APPROPRIATENESS (COA) FROM SIX MONTHS TO TWELVE MONTHS; AND RESTRUCTURING EXCEPTIONS TO ADMINISTRATIVE BYPASS; AND PROVIDING FOR THE SEVERABILITY THEREOF.

23. CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT AND/OR POSTPONEMENT OF ORDINANCE O-2122-34 UPON SECOND AND FINAL READING: AN ORDINANCE OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA AMENDING SECTION 21-113 OF CHAPTER 21 OF THE CODE OF ORDINANCES OF THE CITY OF NORMAN, OKLAHOMA, CONTINGENT ON VOTER APPROVAL OF ORDINANCE NO. O-2122-35, INCREASING THE MONTHLY BASE FEE FOR RESIDENTIAL METERED USERS FROM SIX DOLLARS (\$6.00) TO SEVEN DOLLARS AND EIGHTY CENTS (\$7.80); INCREASING THE MONTHLY WATER RATES FOR RESIDENTIAL METERED USERS FROM THREE DOLLARS AND THIRTY-FIVE CENTS (\$3.35) TO FOUR DOLLARS AND TWENTY CENTS (\$4.20) PER THOUSAND GALLONS FOR THE FIRST 5,000 GALLONS OF WATER, FROM FOUR DOLLARS AND TEN CENTS (\$4.10) TO FIVE DOLLARS AND FIFTEEN CENTS (\$5.15) PER THOUSAND GALLONS FOR WATER USAGE BETWEEN 5,001 AND 15,000 GALLONS, FROM FIVE DOLLARS AND TWENTY CENTS (\$5.20) TO SIX DOLLARS AND FIFTY CENTS (\$6.50) PER THOUSAND GALLONS FOR WATER USAGE BETWEEN 15,001 AND 20,000 GALLONS, AND FROM SIX DOLLARS AND EIGHTY CENTS (\$6.80) TO EIGHT DOLLARS AND FIFTY CENTS (\$8.50) PER THOUSAND GALLONS FOR WATER USAGE OVER 20,000 GALLONS; INCREASING THE MONTHLY BASE FEE FOR NON-RESIDENTIAL WATER SERVICE FROM SIX DOLLARS (\$6.00) TO SEVEN DOLLARS AND EIGHTY CENTS (\$7.80); INCREASING THE WATER RATES FOR NON-RESIDENTIAL METERED USERS FROM THREE DOLLARS AND EIGHTY CENTS (\$3.80) TO FOUR DOLLARS AND THIRTY-FIVE CENTS (\$4.35) PER THOUSAND GALLONS FOR WATER USAGE UP TO SUCH CUSTOMER'S AVERAGE WINTER CONSUMPTION AS DEFINED HEREIN AND FROM FOUR DOLLARS AND TWENTY CENTS (\$4.20) TO FOUR DOLLARS AND EIGHTY CENTS (\$4.80) PER THOUSAND GALLONS FOR WATER USAGE EXCEEDING SUCH NON-RESIDENTIAL METERED USER'S AVERAGE WINTER CONSUMPTION AS DEFINED HEREIN; PROVIDING AN EFFECTIVE DATE FOR SAID INCREASE SUBJECT TO VOTER APPROVAL; AND PROVIDING FOR THE SEVERABILITY THEREOF.
24. CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT AND/OR POSTPONEMENT OF ORDINANCE O-2122-35 UPON SECOND AND FINAL READING: AN ORDINANCE OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, AUTHORIZING THE CALLING AND HOLDING OF A SPECIAL ELECTION IN THE CITY OF NORMAN, COUNTY OF CLEVELAND, STATE OF OKLAHOMA, ON THE 5TH DAY OF APRIL, 2022, FOR THE PURPOSE OF SUBMITTING TO THE REGISTERED VOTERS OF THE CITY OF NORMAN THE QUESTION OF APPROVING OR REJECTING ORDINANCE O-2122-34, WHICH ORDINANCE AMENDING SECTION 21-113 OF CHAPTER 21 OF THE CODE OF ORDINANCES OF THE CITY OF NORMAN, OKLAHOMA, CONTINGENT ON VOTER APPROVAL OF ORDINANCE O-2122-35, INCREASING THE MONTHLY BASE FEE FOR RESIDENTIAL METERED USERS FROM SIX DOLLARS (\$6.00) TO SEVEN DOLLARS AND EIGHTY CENTS (\$7.80); INCREASING THE MONTHLY WATER RATES FOR RESIDENTIAL METERED USERS FROM THREE DOLLARS AND THIRTY-FIVE CENTS (\$3.35) TO FOUR DOLLARS AND TWENTY CENTS (\$4.20) PER THOUSAND GALLONS FOR THE FIRST 5,000 GALLONS OF WATER, FROM FOUR DOLLARS AND TEN CENTS (\$4.10) TO FIVE DOLLARS AND FIFTEEN CENTS (\$5.15) PER THOUSAND GALLONS FOR WATER USAGE BETWEEN 5,001 AND 15,000 GALLONS, FROM FIVE DOLLARS AND TWENTY



CENTS (\$5.20) TO SIX DOLLARS AND FIFTY CENTS (\$6.50) PER THOUSAND GALLONS FOR WATER USAGE BETWEEN 15,001 AND 20,000 GALLONS, AND FROM SIX DOLLARS AND EIGHTY CENTS (\$6.80) TO EIGHT DOLLARS AND FIFTY CENTS (\$8.50) PER THOUSAND GALLONS FOR WATER USAGE OVER 20,000 GALLONS; INCREASING THE MONTHLY BASE FEE FOR NON-RESIDENTIAL WATER SERVICE FROM SIX DOLLARS (\$6.00) TO SEVEN DOLLARS AND EIGHTY CENTS (\$7.80); INCREASING THE WATER RATES FOR NON-RESIDENTIAL METERED USERS FROM THREE DOLLARS AND EIGHTY CENTS (\$3.80) TO FOUR DOLLARS AND THIRTY-FIVE CENTS (\$4.35) PER THOUSAND GALLONS FOR WATER USAGE UP TO SUCH CUSTOMER'S AVERAGE WINTER CONSUMPTION AS DEFINED HEREIN AND FROM FOUR DOLLARS AND TWENTY CENTS (\$4.20) TO FOUR DOLLARS AND EIGHTY CENTS (\$4.80) PER THOUSAND GALLONS FOR WATER USAGE EXCEEDING SUCH NON-RESIDENTIAL METERED USER'S AVERAGE WINTER CONSUMPTION AS DEFINED HEREIN; PROVIDING AN EFFECTIVE DATE FOR SAID INCREASE SUBJECT TO VOTER APPROVAL; AND PROVIDING FOR THE SEVERABILITY THEREOF.

## **MISCELLANEOUS COMMENTS**

*This is an opportunity for citizens to address City Council. Due to Open Meeting Act regulations, Council is not able to participate in discussion during miscellaneous comments. Remarks should be directed to the Council as a whole and limited to three minutes or less.*

## **ADJOURNMENT**

**File Attachments for Item:**

1. PRESENTATION OF AWARDS FROM THE OKLAHOMA MUNICIPAL LEAGUE TO THE FOLLOWING CITY OF NORMAN EMPLOYEES WHO HAVE COMPLETED 25 YEARS OF CONTINUOUS SERVICE:

DOUGLAS BIRKHIMER, TRAFFIC SIGNAL TECHNICIAN, PUBLIC WORKS DEPARTMENT

MILES COTTEN, CONSTRUCTION INSPECTOR, PUBLIC WORKS DEPARTMENT

JACQUELINE CRUMRINE, TRAINING AND DEVELOPMENT MANAGER, HUMAN RESOURCES DEPARTMENT

JOEL FORMBY, SERGEANT, POLICE DEPARTMENT

ANTHONY FRANCISCO, DIRECTOR OF FINANCE, FINANCE DEPARTMENT

PATRICK GOLDSBY, STREETS DIVISION CREW CHIEF, PUBLIC WORKS DEPARTMENT

HOMER HARVEY, FIRE DRIVER ENGINEER, FIRE DEPARTMENT

RICHARD KING, STORMWATER MAINTENANCE. WORKER I, PUBLIC WORKS DEPARTMENT

TRAVIS KING, FIRE CHIEF, FIRE DEPARTMENT

SHANE RAINES, FIRE DRIVER ENGINEER, FIRE DEPARTMENT

VERACHAI SANITPHAN, FLEET MECHANIC II, PUBLIC WORKS DEPARTMENT

GARY SCHMIDT, DETECTIVE, POLICE DEPARTMENT

KEITH SCOTT, FIRE DRIVER ENGINEER, FIRE DEPARTMENT

JERALD SMALL, PARK MAINTENANCE HEAVY EQUIPMENT OPERATOR, PARKS AND RECREATION DEPARTMENT

JONATHAN STAMPER, FIREFIGHTER, FIRE DEPARTMENT

GAY WEBB, ADMINISTRATIVE TECHNICIAN IV, UTILITIES DEPARTMENT

MIKE WHITE, FLEET PROGRAM MANAGER, PUBLIC WORKS DEPARTMENT



## CITY OF NORMAN, OK STAFF REPORT

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**MEETING DATE:** 01/25/2022

**REQUESTER:** Brenda Hall, City Clerk

**PRESENTER:** Mayor Breea Clark

**ITEM TITLE:** PRESENTATION OF AWARDS FROM THE OKLAHOMA MUNICIPAL LEAGUE TO THE FOLLOWING CITY OF NORMAN EMPLOYEES WHO HAVE COMPLETED 25 YEARS OF CONTINUOUS SERVICE:

1. DOUGLAS BIRKHIMER, TRAFFIC SIGNAL TECHNICIAN, PUBLIC WORKS DEPARTMENT
  2. MILES COTTEN, CONSTRUCTION INSPECTOR, PUBLIC WORKS DEPARTMENT
  3. JACQUELINE CRUMRINE, TRAINING AND DEVELOPMENT MANAGER, HUMAN RESOURCES DEPARTMENT
  4. JOEL FORMBY, SERGEANT, POLICE DEPARTMENT
  5. ANTHONY FRANCISCO, DIRECTOR OF FINANCE, FINANCE DEPARTMENT
  6. PATRICK GOLDSBY, STREETS DIVISION CREW CHIEF, PUBLIC WORKS DEPARTMENT
  7. HOMER HARVEY, FIRE DRIVER ENGINEER, FIRE DEPARTMENT
  8. RICHARD KING, STORMWATER MAINTENANCE. WORKER I, PUBLIC WORKS DEPARTMENT
  9. TRAVIS KING, FIRE CHIEF, FIRE DEPARTMENT
  10. SHANE RAINES, FIRE DRIVER ENGINEER, FIRE DEPARTMENT
  11. VERACHAI SANITPHAN, FLEET MECHANIC II, PUBLIC WORKS DEPARTMENT
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  13. KEITH SCOTT, FIRE DRIVER ENGINEER, FIRE DEPARTMENT
  14. JERALD SMALL, PARK MAINTENANCE HEAVY EQUIPMENT OPERATOR, PARKS AND RECREATION DEPARTMENT
  15. JONATHAN STAMPER, FIREFIGHTER, FIRE DEPARTMENT
  16. GAY WEBB, ADMINISTRATIVE TECHNICIAN IV, UTILITIES DEPARTMENT
  17. MIKE WHITE, FLEET PROGRAM MANAGER, PUBLIC WORKS DEPARTMENT
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**File Attachments for Item:**

2. CONSIDERATION OF ACKNOWLEDGEMENT, ACCEPTANCE, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF PROCLAMATION P-2122-19: A PROCLAMATION OF THE MAYOR OF THE CITY OF NORMAN, OKLAHOMA, PROCLAIMING THE MONTH OF JANUARY, 2022, AS NATIONAL MENTORING MONTH IN THE CITY OF NORMAN.



## CITY OF NORMAN, OK STAFF REPORT

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**MEETING DATE:** 01/25/2022

**REQUESTER:** Brenda Hall

**PRESENTER:** Brenda Hall, City Clerk

**ITEM TITLE:** CONSIDERATION OF ACKNOWLEDGEMENT, ACCEPTANCE, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF PROCLAMATION P-2122-19: A PROCLAMATION OF THE MAYOR OF THE CITY OF NORMAN, OKLAHOMA, PROCLAIMING THE MONTH OF JANUARY, 2022, AS NATIONAL MENTORING MONTH IN THE CITY OF NORMAN.

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## Proclamation

P-2122-19

A PROCLAMATION OF THE MAYOR OF THE CITY  
OF NORMAN, OKLAHOMA, PROCLAIMING THE  
MONTH OF JANUARY, 2022, AS NATIONAL  
MENTORING MONTH IN THE CITY OF NORMAN.

- § 1. WHEREAS, January 2022 marks the 20th Anniversary of National Mentoring Month which honors volunteer mentors who support young people by showing up for them every day and demonstrating their commitment to helping them thrive; and
- § 2. WHEREAS, mentoring programs like Alameda Reading Tutoring Program; Big Brothers Big Sisters Norman; Center for Children and Families Boys and Girls Club of Norman; Bridges of Norman Mentoring Program; Chevron Phillips Scholar-Mentor Program; and Dean's Leadership Council make our city stronger by driving impactful relationships that increase social capital for young people and provide invaluable support networks; and
- § 3. WHEREAS, during the COVID-19 pandemic, mentoring programs have stepped up to fill gaps for young people and families, connecting them with resources and ensuring that mentoring relationships continue virtually to ensure that physical distancing does not mean social disconnection; and
- § 4. WHEREAS, students who meet regularly with their mentors are more than 52% less likely than their peers to skip a day of school and youth who face an opportunity gap but have a mentor are 55 percent more likely to be enrolled in college than those who did not have a mentor; and
- § 5. WHEREAS, quality mentoring promotes healthy relationships and communication, positive self-esteem, emotional well-being, and growth of a young person and their relationships with other adults; and

NOW, THEREFORE, I, MAYOR OF THE CITY OF NORMAN, OKLAHOMA:

- § 6. Do hereby proclaim the month of January, 2022, as National Mentoring Month in the City of Norman, Oklahoma, and encourage all citizens, businesses, public and private agencies, and religious and educational institutions to celebrate, elevate, and encourage mentoring in the City of Norman and recruit adult mentors.

PASSED AND APPROVED this 18th day of January, 2022.

ATTEST:

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
City Clerk





**File Attachments for Item:**

3. CONSIDERATION OF ACKNOWLEDGEMENT, APPROVAL, ACCEPTANCE, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF RECEIPT OF THE FINANCE DIRECTOR'S INVESTMENT REPORT AS OF DECEMBER 31, 2021, AND DIRECTING THE FILING THEREOF.



## CITY OF NORMAN, OK STAFF REPORT

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**MEETING DATE:** 01/25/2022

**REQUESTER:** Anthony Francisco, Director of Finance

**PRESENTER:** Anthony Francisco, Director of Finance

**ITEM TITLE:** CONSIDERATION OF ACKNOWLEDGEMENT, APPROVAL, ACCEPTANCE, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF RECEIPT OF THE FINANCE DIRECTOR'S INVESTMENT REPORT AS OF DECEMBER 31, 2021, AND DIRECTING THE FILING THEREOF.

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DATE: January 7, 2022  
 TO: City Council  
 FROM: Anthony Francisco, Director of Finance  
 REVIEWED BY: Clint Mercer, Chief Accountant  
 PREPARED BY: Debbie Whitaker, Municipal Accountant III  
 SUBJECT: Breakdown of Interest Earnings by Fund

FUND	MONTHLY COMPARISON						ANNUAL COMPARISON			
	MONTHLY BUDGETED INTEREST EARNINGS FYE22	MONTHLY INTEREST EARNINGS December 2021	MONTHLY % INCREASE (DECREASE)	MONTHLY INTEREST % OF PORTFOLIO HOLDINGS	MONTH-END BALANCE December 2021	MONTHLY % OF PORTFOLIO HOLDINGS	ANNUAL BUDGETED INTEREST EARNINGS FYE22-YTD	INTEREST EARNINGS YTD FYE22	YTD % INCREASE (DECREASE)	YTD % PORTFOLIO HOLDINGS
GENERAL FUND	\$15,476	\$1,978	-87.23%	4.77%	7,892,270	2.81%	\$91,035	\$10,155	-88.85%	3.43%
NET REVENUE STABILIZATION	\$4,187	\$1,016	-75.61%	2.45%	4,003,579	1.42%	\$25,000	\$7,418	-70.34%	2.50%
PUBLIC SAFETY SALES TAX FUND	\$4,167	\$403	-90.34%	0.97%	9,489,306	3.37%	\$25,000	\$1,309	-94.76%	0.44%
HOUSING	N/A	\$5	100.00%	0.01%	3,844,722	1.37%	N/A	\$9	100.00%	0.00%
SPECIAL GRANTS FUND	N/A	\$474	100.00%	1.14%	13,591,179	4.83%	N/A	\$2,811	100.00%	0.95%
ROOM TAX FUND	\$208	\$132	-36.75%	0.32%	4,348,106	1.55%	\$1,250	\$747	-40.25%	0.25%
SEIZURES	\$42	\$257	517.71%	0.62%	1,151,285	0.41%	\$250	\$1,814	625.41%	0.61%
CLEET FUND	N/A	\$0	100.00%	0.00%	(3,289)	0.00%	N/A	\$0	100.00%	0.00%
TRANSIT & PARKING FUND	\$0	\$225	0.00%	0.54%	781,343	0.28%	\$0	\$648	0.00%	0.22%
ART IN PUBLIC PLACES FUND	N/A	\$1	100.00%	0.00%	2,348	0.00%	N/A	\$3	100.00%	0.00%
WESTWOOD FUND	\$625	\$175	-71.93%	0.42%	1,118,911	0.40%	\$3,750	\$929	-75.22%	0.31%
WATER FUND	\$10,000	\$8,849	-11.51%	21.35%	39,540,101	14.06%	\$60,000	\$83,474	5.79%	21.42%
WASTEWATER FUND	\$4,167	\$2,425	-41.80%	5.85%	10,099,673	3.59%	\$25,000	\$17,889	-28.44%	6.04%
SEWER MAINTENANCE FUND	N/A	\$3,131	100.00%	7.56%	12,262,311	4.36%	N/A	\$21,785	100.00%	7.35%
DEVELOPMENT EXCISE	\$5,833	\$1,655	-71.63%	3.99%	6,974,056	2.48%	\$35,000	\$12,554	-64.13%	4.24%
SANITATION FUND	\$25,000	\$4,150	-83.40%	10.01%	16,229,347	5.77%	\$150,000	\$30,819	-79.59%	10.33%
RISK MANAGEMENT FUND	N/A	\$609	100.00%	1.47%	1,974,073	0.70%	N/A	\$5,316	100.00%	1.79%
CAPITAL PROJECTS FUND	\$58,333	\$8,549	-88.77%	15.80%	65,600,918	23.33%	\$200,000	\$50,466	-74.77%	17.03%
NORMAN FORWARD SALES TAX	\$1,250	\$7,977	538.16%	19.25%	64,825,089	23.05%	\$7,500	\$55,789	643.85%	18.82%
PARKLAND FUND	\$833	\$198	-76.23%	0.48%	783,906	0.28%	\$5,000	\$1,643	-67.13%	0.55%
UNP TAX INCREMENT DISTRICT	\$2,000	\$106	-94.70%	0.26%	10,731,281	3.82%	\$240,000	\$544	100.00%	0.18%
CENTER CITY TAX INCREMENT DIST	N/A	\$120	0.00%	0.29%	471,518	0.17%	N/A	\$853	100.00%	0.29%
SINKING FUND	\$2,083	\$785	-63.26%	1.85%	4,554,183	1.62%	\$12,500	\$7,835	-37.32%	2.64%
SITE IMPROVEMENT FUND	N/A	\$13	100.00%	0.03%	50,218	0.02%	N/A	\$93	100.00%	0.03%
TRUST & AGENCY FUNDS	N/A	\$2	100.00%	0.01%	9,675	0.00%	N/A	\$18	100.00%	0.01%
ARTERIAL ROAD FUND	N/A	\$231	100.00%	0.56%	915,284	0.33%	N/A	\$1,669	100.00%	0.56%
	\$134,184	\$41,446	-69.11%	100.00%	281,221,388	100.00%	\$861,285	\$296,387	-66.37%	100.00%

City funds are invested in interest bearing accounts and investment securities, as directed by the City's Investment Policy. Rates of return on these investments relate directly to current Treasury and Money Market rates. Total funds on deposit of \$281.22 million as of 12/31/21 are represented by working capital cash balances of all City funds of approximately \$97.13 million, outstanding encumbrances of \$69.02 million, General Obligation Bond proceeds of \$45.85 million, NUA revenue bond proceeds of \$6.85 million, NMA bond proceeds of \$49.43 million, and UNP TIF reserve amounts of \$12.94 million.

## INVESTMENT BY TYPE

December 31, 2021					EARNED		
LIST BY TYPE	SEC. NO.	PURCHASED	MATURITY	YIELD	INTEREST	COST	MARKET
<b>**Checking</b>							
BANK OF OKLAHOMA	GEN'L DEP.			0.05%	\$1,164.51	\$33,498,869.59	\$33,498,869.59
BANK OF OKLAHOMA	WARRANTS PAYABLE					(\$1,588,829.23)	(\$1,588,829.23)
BANK OF OKLAHOMA	PAYROLL					(\$3,135,152.08)	(\$3,135,152.08)
BANK OF OKLAHOMA	COURT BOND REFUNDS					\$111,994.83	\$111,994.83
BANK OF OKLAHOMA	INSURANCE CLAIMS					(\$241,820.86)	(\$241,820.86)
BANK OF OKLAHOMA	LOCK BOX					\$1,695,016.40	\$1,695,016.40
BANK OF OKLAHOMA	RETURN CHECKS					(\$23,390.35)	(\$23,390.35)
BANK OF OKLAHOMA	PARKS					\$0.00	\$0.00
BANK OF OKLAHOMA	FLEXIBLE SPENDING					(\$8,223.71)	(\$8,223.71)
BANK OF OKLAHOMA	CDBG-CV			0.05%	\$5.14	(\$16,256.10)	(\$16,256.10)
<b>**Subtotal</b>					<b>\$1,169.65</b>	<b>\$30,296,408.71</b>	<b>\$30,296,408.71</b>
<b>**Money Market</b>							
BANCFIRST-NUA	MONEY MKT.			0.37%	\$4.40	\$563,698.29	\$563,698.29
BANCFIRST-NMA Room Tax	MONEY MKT.			0.06%	\$17.87	\$3,911,698.55	\$3,911,698.55
BANCFIRST-NUA Water	MONEY MKT.			0.37%	\$104.08	\$5,838,312.60	\$5,838,312.60
BANCFIRST-NUA Clean Water	MONEY MKT.			0.06%	\$3.17	\$452,481.29	\$452,481.29
BANCFIRST-NMA PSST	MONEY MKT.			0.37%	\$156.42	\$8,705,753.72	\$8,705,753.72
BANCFIRST-NMA Norman Forward	MONEY MKT.			0.37%	\$328.93	\$34,840,969.29	\$34,840,969.29
BANCFIRST-NMA ERP Financing	MONEY MKT.			0.37%	\$35.12	\$1,987,417.17	\$1,987,417.17
BANK OF OKLAHOMA UNP TIF	MONEY MKT.			0.01%	\$106.00	\$12,942,886.91	\$12,942,886.91
BANK OF OKLAHOMA ARPA	MONEY MKT.			0.05%	\$472.46	\$11,126,230.18	\$11,126,230.18
BANK OF OKLAHOMA-Westwood	MONEY MKT.			0.05%	\$175.41	\$0.00	\$0.00
BANK OF OKLAHOMA-CW	MONEY MKT.			0.05%	\$0.51	\$12,423.02	\$12,423.02
BANK OF OKLAHOMA	MONEY MKT.			0.05%	\$1.34	\$32,975.22	\$32,975.22
BANK OF OKLAHOMA	MONEY MKT.			0.05%	\$6.87	\$163,539.94	\$163,539.94
BANK OF OKLAHOMA-SW	MONEY MKT.			0.05%	\$0.00	\$6.77	\$6.77
BANK OF OKLAHOMA-Misty Lake	MONEY MKT.			0.05%	\$25.27	\$595,066.02	\$595,066.02
BANK OF OKLAHOMA-2015	MONEY MKT.			0.05%	\$79.16	\$1,941,716.25	\$1,941,716.25
BANK OF OKLAHOMA-2019A	MONEY MKT.			0.05%	\$257.88	\$6,282,110.31	\$6,282,110.31
BANK OF OKLAHOMA-2019B	MONEY MKT.			0.05%	\$648.90	\$15,914,195.80	\$15,914,195.80
BANK OF OKLAHOMA-2020A	MONEY MKT.			0.05%	\$343.52	\$8,058,604.43	\$8,058,604.43
BANK OF OKLAHOMA-2021	MONEY MKT.			0.05%	\$579.66	\$13,650,632.20	\$13,650,632.20
<b>**Subtotal</b>					<b>\$3,346.77</b>	<b>\$127,000,717.96</b>	<b>\$127,000,717.96</b>
<b>**Sweep/Overnight</b>							
BANK OF OKLAHOMA	ICS ACCT			0.03%	\$822.53	\$32,323,266.69	\$32,323,266.69
BANK OF OKLAHOMA PORTFOLIO	SHORT TERM			0.01%	\$175.25	\$22,255,994.94	\$22,255,994.94
<b>**Certificate of Deposit</b>							
FIRST FIDELITY BANK	CD	09/30/21	09/30/22	0.45%	\$31.25	\$250,000.00	\$250,000.00
GREAT NATIONS BANK	CD	09/30/21	09/30/22	0.45%	\$114.58	\$250,000.00	\$250,000.00
VALLANCE BANK	CD	11/30/21	11/30/22	0.15%	\$31.25	\$250,000.00	\$250,000.00
FIRST NATIONAL BANK	CD	12/28/21	12/28/22	0.25%	\$52.08	\$250,000.00	\$250,000.00
<b>**Subtotal</b>					<b>\$229.16</b>	<b>\$1,000,000.00</b>	<b>\$1,000,000.00</b>
<b>**U.S. Treasury Securities/Agency Securities</b>							
FFCB	3133EH2T9	12/31/19	12/13/21	1.58%	288.26		
FFCB	3133EMNU2	01/29/21	04/27/22	0.08%	521.90	7,500,000.00	\$7,498,575.00
US T-Note	912828XR6	05/31/19	05/31/22	2.04%	5,077.68	3,000,000.00	\$3,019,620.00
FHLB	313381BR5	12/31/19	12/09/22	1.81%	10,879.28	8,100,000.00	\$8,210,808.00
FHLMC	3134GXGQ1	01/29/21	12/15/22	0.08%	897.62	8,000,000.00	\$7,989,840.00
FFCB	3133EMKU5	01/29/21	12/14/22	0.11%	453.11	5,005,000.00	\$5,005,750.75
US T-Note	9128284X5	06/29/21	08/31/23	2.75%	1,492.47	8,500,000.00	\$8,792,910.00
FHLB	3130A3VC5	12/31/19	12/08/23	1.82%	8,824.50	6,500,000.00	\$6,690,385.00
FNMA	3135G05T6	11/30/21	08/18/23	0.48%	2,832.95	6,500,000.00	\$6,463,730.00
US T-Note	91282CBN0	06/30/21	12/28/23	0.13%	1,367.70	9,000,000.00	\$8,964,540.00
FHLB	3130A8HK2	11/30/21	06/14/24	0.70%	3,266.82	6,240,000.00	\$6,376,032.00
<b>**Subtotal</b>					<b>35,702.29</b>	<b>68,345,000.00</b>	<b>\$69,012,190.75</b>
<b>**TOTAL**</b>					<b>41,445.65</b>	<b>281,221,388.30</b>	<b>\$249,565,312.36</b>

The Governmental Accounting Standards Board requires the reporting of market values of investment securities. These market values represent the amount of money the security would sell for on the open market, if cash flow demands were such that the security had to be sold. The City of Norman purchases investment securities with the intent of holding them to maturity, as stated in the City's Investment Policy. Only in exceptional circumstances would securities be sold before their maturity, due to cash flow demands or favorable market conditions.

**File Attachments for Item:**

4. CONSIDERATION OF ACKNOWLEDGEMENT, APPROVAL, ACCEPTANCE, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF THE MONTHLY DEPARTMENTAL REPORT FOR THE MONTH OF DECEMBER, 2021.



## CITY OF NORMAN, OK STAFF REPORT

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**MEETING DATE:** 01/25/2022

**REQUESTER:** Stacey Parker, Executive Assistant

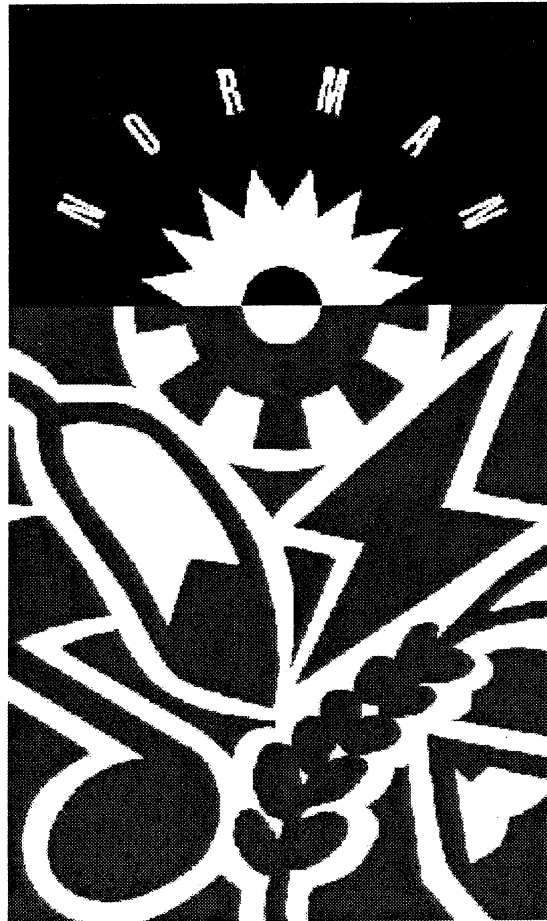
**PRESENTER:** Stacey Parker, Executive Assistant

**ITEM TITLE:** CONSIDERATION OF ACKNOWLEDGEMENT, APPROVAL, ACCEPTANCE, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF THE MONTHLY DEPARTMENTAL REPORT FOR THE MONTH OF DECEMBER, 2021.

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# City of Norman



## Monthly Departmental Report **December 2021**

## **MONTHLY PROGRESS**

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**CITY CLERK      1**

**CITY CLERK**  
**MONTHLY PROGRESS REPORT**  
**December 2021**

<b>ACTION CENTER</b>				
<b>DEPARTMENT</b>	<b>CALLS</b>	<b>CALLS YTD</b>	<b>ADDITIONAL CONTACTS</b>	<b>ADDITIONAL CONTACTS YTD</b>
Animal Welfare	3	25	0	13
Bus Service	0	3	0	0
CDBG	13	52	2	9
City Clerk	34	416	1	56
City Manager/Mayor	5	34	5	21
City Wide Garage Sale	0	205	0	6
Code Enforcement	28	240	4	27
Finance	4	104	0	3
Fire/Civil Defense	1	11	0	1
Human Resources	3	17	0	0
I.T.	9	41	0	0
Legal	5	28	1	5
Line Maintenance	12	77	0	7
Municipal Court	5	26	0	0
Noise Complaint	0	0	0	0
Norman Forward Questions	0	0	0	0
Parks & Recreation	13	99	3	13
Permits/Inspections	26	193	0	14
Planning	13	85	1	2
Police/Parking	15	117	5	66
Public Works	2	73	2	9
Recycling	0	0	0	0
Sanitation	29	217	2	16
Sidewalks	0	5	0	2
Storm Debris	0	0	0	0
Storm Water	6	45	2	11
Streets	27	117	1	11
Street Lights	0	40	0	0
Traffic	12	90	0	4
Utilities	38	142	2	13
WC Questions	0	0	0	0
WC Violations	0	0	0	0
<b>December Total: 334</b>	<b>303</b>	<b>2502</b>	<b>31</b>	<b>311</b>

## LICENSES

Eleven New licenses and Zero Renewals were issued during the month of December. Following is a list of each license type and the number issued for that specific type:

LICENSE TYPE	NUMBER	FYE	LICENSE TYPE	NUMBER	FYE
	ISSUED	YTD		ISSUED	YTD
Bee Keeper	0	0	Retail Beer	0	6
Brewer	0	0	Retail Spirits Store	0	1
Coin-Operated Devices	0	1	Retail Wine	0	6
Distiller	0	0	Salvage Yard	0	0
Food	2	26	Sidewalk Dining	0	0
Game Machines	8	8	Solicitor/Peddler (30 day)	0	7
Impoundment Yard	0	0	Solicitor/Peddler (60 day)	0	12
Kennel	0	0	Solicitor/Peddler (one day)	0	2
Medical Marijuana Dispensary	0	9	Special Event	0	3
Medical Marijuana Grower	1	18	Strong Beer & Wine/Winemaker	0	1
Medical Marijuana Processor	0	4	Taxi/Motorbus/Limousine	0	3
Medical Marijuana Testing Laboratory	0	0		0	0
Mixed Beverage	0	1	Temp Food (one day)	0	8
Mixed Beverage/Caterer	0	5	Temp Food (30 day)	0	10
Pawnbroker	0	0	Temp Food (180 day)	0	11
Pedicab	0	0	Transient Amusement	0	0
<b>YTD License Total: 142</b>	<b>11</b>	<b>72</b>		<b>0</b>	<b>70</b>

## **NEW ESTABLISHMENT LICENSES**

NAME	ADDRESS	LICENSE TYPE(S)
HeyDay Entertainment (Games)	3201 Market Place	Game Machines/Pool tables (8)
Marco's Pizza 5247	2620 Classen Blvd Suite 102	Food Service
Rock Creek Cultivation	2605 72 <sup>nd</sup> Ave NE	Medical Marijuana Grower
Wendy's	3251 Classen Blvd.	Food Service

## CLAIMS FILED

DATE FILED	NAME	JUSTIFICATION	AMOUNT
12/07/21	State Farm Mutual Automobile Insurance Co./ A/S/O Jim Andrew	State Farm insured alleges that a City employee pulled up to the gate at Boulevard De Lac in Norman and was struggling to get in. Insured drove up to the right of the City vehicle to help. City employee then drove forward slightly to the right allegedly striking the insured's vehicle, causing damage to the front driver's side.	\$2,793.00
12/09/21	Noah Royse	Mr. Royse adopted a pet from the Norman Animal Welfare Center on September 2, 2021. Mr. Royse alleges he was not informed that the pet was neutered the day before, nor was he given any post-op instructions. Claimant took the pet to Sooner Veterinary Hospital on September 8, 2021, After finding blood in the feces of said pet. The Veterinarian at Sooner Veterinary Hospital allegedly found three ruptured abscesses, caused from the "worst neuter job	\$173.84



		they had ever seen". Mr. Royse is requesting a refund of adoption & neutering fees as well as the Veterinary bill for post-op care due to the alleged poor neutering practices performed at the Norman Animal Welfare Center.	
12/29/21	Carroline Webb/ Laird Hammons Laird Trail Lawyers	Alleges that a Norman Police Officer caused a collision with her automobile on July 22, 2021. She alleges this caused her bodily injury that resulted in medical bills of \$973.00 and auto repair of \$3601.08. She is demanding \$9000.00 from the City of Norman in return, Ms. Webb would agree to waive any further action relating to this incident against the City of Norman.	\$9,000.00
12/30/21	OG&E	Alleged damage to OGE property on February 12, 2021. City of Norman was doing water work and damaged an OG&E cable in the area of Monnett Ave. and E. Duffy Ave.	\$1,217.13

### **STUDY SESSION**

On December 7, 2021, City Council met in Study Session for discussion regarding an election for a water rate increase.

### **BUSINESS AND COMMUNITY AFFAIRS COMMITTEE**

On December 2, 2021, the Business and Community Affairs Committee met and discussed neighborhood commercial and accessory commercial units in neighborhoods.

### **COMMUNITY PLANNING & TRANSPORTATION COMMITTEE**

On December 30, 2021, the Community Planning & Transportation Committee met for Public Transit update. Discussion regarding Campus Corner Parking and Commercial loading zones, the presentation of the 2021 Americans with Disabilities Act transit transition plan. Discussion regarding loud music subwoofers in residential areas.

### **OVERSIGHT COMMITTEE**

On December 9, 2021, the Oversight Committee met for discussion regarding the Center City Form based code ordinance and reestablishment of the Center City Form based code AD HOC Committee. Discussed the potential purchase of a van for the proposed Mobile Crisis Response Unit and staff report on Homeless activity.

**CITY MANAGER                      2**

## **NORMAN FORWARD 2A**



# Memorandum

**To:** Jason Olsen, The City of Norman Parks and Recreation

**From:** Randy Hill, ADG

**ADG Project Number:** 16-003

**ADG Project Name:** Norman FORWARD

**Date:** 01.18.2022

**Re:** December 2021 Monthly Report

**REPORT PERIOD:** December 3 through December 31, 2021

## WORK THIS MONTH

1. Wednesday, December 1, 2021 | 9:30 a.m. | North Base Complex Bi-Monthly Update Call
  - a. Discussion of project schedule, budgets, and critical issues
2. Wednesday, December 1, 2021 | 10:00 a.m. | Norman Development Center - OAC Meeting
  - a. Bi-weekly discussion of project schedules, budgets, and critical issues
3. Thursday, December 2, 2021 | 10:00 a.m. | Ruby Grant Park Phase 1 - Warranty Walk
  - a. Walk to verify any warranty work required
4. Thursday, December 2, 2021 | 1:00 p.m. | Indoor Aquatic and Multi-Sport Weekly Programming Meeting
  - a. Discussion of programming issues on the Indoor Aquatic and Multi-Sport Project
5. Thursday, December 2, 2021 | 2:30 p.m. | Senior Wellness Center Bi-Weekly Programming Meeting
  - a. Discussion of programming issues on the Senior Wellness Center Project
6. Friday, December 3, 2021 | 8:30 p.m. | Pref for Griffin CMAR Interviews and Deliberations
  - a. Discussion of interview standards
7. Friday, December 3, 2021 | 1:00 p.m. | Griffin CMAR Interviews and Deliberations
  - a. CMAR Interviews
8. Monday, December 6, 2021 | 11:00 a.m. | YFAC – Monthly Updates
  - a. Monthly discussion of project schedules, budgets, and critical issues
9. Tuesday, December 7, 2021 | 2:00 p.m. | Bid Opening - Norman Senior Wellness Center
  - a. Bid opening for GMP #1
10. Tuesday, December 7, 2021 | 3:30 p.m. | CFOB Meeting
  - a. Update CFOB committee, presented graphic report
11. Wednesday, December 8, 2021 | 10:45 a.m. | Development Center Updates
  - a. Discussion of project schedule, budgets, and critical issues
12. Thursday, December 9, 2021 | 8:00 a.m. | Norman ECOC Kick-Off Meeting
  - a. Introductions of architect, program manager and CMAR
13. Tuesday, December 9, 2021 | 10:00 a.m. | YFAC – Review Meeting
  - a. Discussions with developer
14. Tuesday, December 9, 2021 | 10:00 a.m. | Norman Development Center - OAC Meeting
  - a. Bi-weekly discussion of project schedules, budgets, and critical issues
15. Thursday, December 9, 2021 | 1:00 p.m. YFAC OAC Meeting
  - a. Bi-weekly discussion of project schedules, budgets, and critical issues

Memorandum

01.18.2022

To: Jason Olsen, The City of Norman Parks and Recreation

Page 2 of 4

Re: December 2021 Monthly Report

ADG Project No. 16-003

16. Thursday, December 9, 2021 | 3:30 p.m. | ECOC: CMAR Proposal Review
  - a. Review and discuss proposals recieved
17. Friday, December 10, 2021 | 9:00 a.m. | Reaves OAC Meeting
  - a. Bi-weekly discussion of project schedules, budgets, and critical issues
18. Monday, December 13, 2021 | 10:30 a.m. | Norman Forward Weekly Staff-ADG Coordination
  - a. Weekly discussion of project schedules, budgets, and critical issues
19. Monday, December 13, 2021 | 1:00 p.m. | NNB - Completion Discussion
  - a. Discussion of completion timeline
20. Tuesday, December 14, 2021 | 10:00 a.m. | YFAC - ADG/FSB Coordination
  - a. Meeting with FSB to facilitate coordination of contract documents
21. Wednesday, December 15, 2021 | 9:30 a.m. | North Base Maintenance Complex Update Call
  - a. Weekly discussion of project schedule, budgets, and critical issues
22. Thursday, December 16, 2021 | 1:00 p.m. | YFAC Programming
  - a. Discussion of programming issues on the YFAC Project
23. Thursday, December 16, 2021 | 2:30 p.m. | Senior Wellness Center Bi-Weekly Programming Meeting
  - a. Discussion of programming issues on the Senior Wellness Center Project
24. Thursday, December 16, 2021 | 3:30 p.m. | ECOC CMAR Interview Prep
  - a. Pre-Interview meeting to prepare questions
25. Thursday, December 16, 2021 | 4:00 p.m. | NF Griffin Park Ad Hoc Meeting
  - a. Presentation to Ad-Hoc current progress on the project
26. Friday, December 17, 2021 | 9:00 a.m. | ECOC CMAR Interview: Crossland Construction
  - a. Interview with Crossland Construction
27. Friday, December 17, 2021 | 10:30 a.m. | ECOC CMAR Interview: GE Johnson
  - a. Interview with GE Johnson
28. Monday, December 20, 2021 | 10:30 a.m. | Norman Forward Weekly Staff-ADG Coordination
  - a. Weekly discussion of project schedules, budgets, and critical issues
29. Tuesday, December 21, 2021 | 10:00 a.m. | FSB/ADG YFAC Coordination
  - a. Meeting with FSB to facilitate coordination of contract documents
30. Tuesday, December 21, 2021 | 1:00 p.m. | NNB - Exterior Building Punch Walk
  - a. Punchlist walk
31. Wednesday, December 22, 2021 | 10:00 a.m. | Norman Development Center OAC
  - a. Bi-weekly discussion of project schedules, budgets, and critical issues

## Construction Observation Site Visits:

- a. Municipal Complex, Development Center: 8
- b. North Base: 10
- c. Young Family Athletic Center: 6

Memorandum

01.18.2022

To: Jason Olsen, The City of Norman Parks and Recreation

Page 3 of 4

Re: December 2021 Monthly Report

ADG Project No. 16-003

**WORK ANTICIPATED THE UPCOMING MONTH (January 2022)**

- Griffin Park Ph. 5
  - Out for bid
- Central Library
  - Contractor working on open warranty items and leaks.
- Reaves Park
  - Construction in progress
- Ruby Grant Park
  - Close out final completion
- North Base Complex
  - On-site interviews for Davis Bacon compliance
  - Proceed to Substantial Completion
- Indoor Aquatic and Multi-Sport Facility
  - Bid GMP #3
  - Recurring weekly OAC meetings
- Senior Wellness Center
  - Bid GMP #2
  - Recurring bi-weekly programming meetings underway
- Municipal Complex
  - Development Center: Construction underway
  - Municipal Courts: CD's underway
  - Building 201: Phase 1 & 2 complete; construction underway
  - FF&E inventory, selection and layout underway
- Continued Development of Standardized Contracts for Use with Architectural, Design, and Construction Consultants

**PROJECT STATUS**

- East Library
  - a. Schedule: Opening Celebration on July 20, 2018
  - b. Budget: Within budget
  - c. In operation
- Central Library
  - a. Schedule: Warranty work in progress
  - b. Budget: Within budget
  - c. In operation
- Westwood Family Aquatic Center
  - a. Schedule: Opening Celebration on May 26, 2018
  - b. Budget: Final Pay App approved on July 24, 2018
  - c. In operation
- Griffin Park
  - a. Schedule: Phase V upcoming
  - b. Budget: Within budget
  - a. Issues: No known issues
- Reaves Park Phase I
  - a. Schedule: Construction Schedule updated
  - b. Budget: Within budget
  - c. Issues: No known issues

Memorandum

01.18.2022

To: Jason Olsen, The City of Norman Parks and Recreation

Page 4 of 4

Re: December 2021 Monthly Report

ADG Project No. 16-003

- Westwood Indoor Tennis Facility
  - a. Schedule: Opening Celebration on May 24, 2019
  - b. Budget: Within budget
  - c. In operation: Court paint warranty extended another 12 months from 9.22.2021
- Ruby Grant
  - a. Schedule: Complete
  - b. Budget: In budget
  - c. Issues: No known issues
- Indoor Aquatic and Multi-Sport Facility
  - a. Schedule: CDs in progress,
  - b. Budget: Design Development estimate in progress
  - c. Issues: No known issues
- Senior Wellness Center
  - a. Schedule: Design development VE process ongoing
  - b. Budget: Budget alignment in progress
  - c. Issues: NRHS Porter Campus Master Plan
- Norman Forward Program Management
  - a. Schedule: Proceeding per Implementation Plan
  - b. Budget: Proceeding per Implementation Plan
  - c. Implementation Plan: Revised/Updated December 30, 2019
  - d. Issues: None

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SUBMITTED BY: ADG – Randy W. Hill

## **FINANCE      3**



## CITY OF NORMAN

### Department of Finance Monthly Report – December 2021

Statistics on outputs from the various divisions of the Department of Finance (DOF) are presented on the following pages. Major projects which were completed or initiated by the DOF in December are discussed below:

#### Treasury Division:

In the month of December, the Treasury Division processed 40,173 payments in person and over the phone, a decrease of -3.9% from last month. Paymentus (the City's 3<sup>rd</sup> party processor of online and automated telephone payments) processed 10,574 payments in December, a decrease of -7.3% from last month.

#### Utility Services Division:

The Meter Reading Division read 41,140 meters. Out of 77 meter reading routes, 44 (57%) were read within the targeted 30-day reading cycle. 57 routes (74%) were read by the 31st day, and all routes were read by the 36th day.

#### General Fund Revenues & Expenditures:

When comparing General Fund revenue sources versus budgeted levels, revenues are above target for the month of December by 5.4%. Revenues from the City's largest single source of revenue, sales tax, are above target by 19.7% for the year to date and 24.3% above last fiscal year. Following is a summary table regarding General Fund revenues and expenditures to-date.

	FYE 22 Budget To Date	FYE 22 Actual To Date	FYE 21 Actual To Date	FYE 20 Actual To Date
Sales Tax Revenue	\$23,078,854	\$27,635,417	\$22,226,197	\$20,706,007
General Fund Revenue	\$44,418,127	\$46,835,659	\$49,983,497	\$38,919,504
General Fund Expenses	\$45,870,107	\$40,741,533	\$46,149,066	\$45,258,811

## Administration Division

	FYE 22		FYE 21	
	December	YTD	December	YTD
PERSONNEL HOURS - FULL TIME				
Total Regular Hours Available	320.00	2,080.00	320.00	2,080.00
Total Comp Time Available	11.75	53.75	0.50	14.75
Total Overtime Hours	0.00	0.50	0.00	0.00
Total Bonus Hours	0.00	0.00	0.00	0.00
Total Furlough Hours	0.00	0.00	0.00	0.00
 TOTAL HOURS AVAILABLE	 331.75	 2,134.25	 320.50	 2,094.75
Benefit Hours Taken	58.50	317.75	129.00	450.00
 TOTAL ACCOUNTABLE STAFF HOURS	 273.25	 1,816.50	 191.50	 1,644.75
 PERMANENT PART-TIME				
Total Regular Hours Available	0.00	0.00	0.00	0.00
Total Comp Time Available	0.00	0.00	0.00	0.00
Total Overtime Hours	0.00	0.00	0.00	0.00
Total Bonus Hours	0.00	0.00	0.00	0.00
 TOTAL HOURS AVAILABLE	 0.00	 0.00	 0.00	 0.00
Benefit Hours Taken	0.00	0.00	0.00	0.00
 TOTAL ACCOUNTABLE STAFF HOURS	 0.00	 0.00	 0.00	 0.00
 TEMPORARY				
Total Regular Hours Available	0.00	0.00	0.00	0.00
Total Overtime Hours	0.00	0.00	0.00	0.00
 TOTAL HOURS AVAILABLE	 0.00	 0.00	 0.00	 0.00

## ACCOUNTING 3A

## Accounting Division

	FYE 22		FYE 21	
	December	YTD	December	YTD
Total Regular Hours Available	1,120.00	6,464.00	960.00	6,240.00
Total Comp Time Available	0.50	26.50	0.50	13.50
Total Overtime Hours	15.25	112.50	21.75	87.00
Total Bonus Hours	0.00	0.00	0.00	0.00
Total Furlough Hours	0.00	0.00	0.00	0.00
 TOTAL HOURS AVAILABLE	 1,135.75	 6,603.00	 982.25	 6,340.50
Benefit Hours Taken	237.00	956.50	169.25	795.75
 TOTAL ACCOUNTABLE STAFF HOURS	 898.75	 5,646.50	 813.00	 5,544.75
 PERMANENT PART-TIME				
Total Regular Hours Available	0.00	0.00	0.00	0.00
Total Comp Time Available	0.00	0.00	0.00	0.00
Total Overtime Hours	0.00	0.00	0.00	0.00
Total Bonus Hours	0.00	0.00	0.00	0.00
 TOTAL HOURS AVAILABLE	 0.00	 0.00	 0.00	 0.00
Benefit Hours Taken	0.00	0.00	0.00	0.00
 TOTAL ACCOUNTABLE STAFF HOURS	 0.00	 0.00	 0.00	 0.00
 TEMPORARY				
Total Regular Hours Available	0.00	0.00	0.00	0.00
Total Overtime Hours	0.00	0.00	0.00	0.00
 TOTAL HOURS AVAILABLE	 0.00	 0.00	 0.00	 0.00

## **CITY REVENUE REPORTS**

**3B**

City Revenue Report			
	FYE 22	FYE 22	
	November	December	
Total Revenue Received (\$)	\$5,544,709	\$5,272,047	\$272,662
Utility Payments - Office (#)	41,784	40,173	1,611
Utility Payments - Office (\$)	\$4,664,076	\$4,124,553	\$539,523
Lockbox (#)	12,732	11,767	965
Lockbox (\$)	\$1,223,866	\$1,134,657	\$89,209
IVR Credit Card (#)	0	0	0
IVR Credit Card (\$)	\$0	\$0	\$0
Click to Gov (#)	0	0	0
Click to Gov (\$)	\$0	\$0	\$0
Paymentus (#)	\$11,403	\$10,574	\$829
Paymentus (\$)	\$1,576,035	\$1,310,813	\$265,222
UT Credit Card Payments (#)	0	0	0
UT Credit Card Payments (\$)	\$0	\$0	\$0
Art Donations (#)	0	0	0
Art Donations (\$)	\$0	\$0	\$0
Bank Draft Payments (#)	14,633	14,146	487
Bank Draft Payments (\$)	\$1,541,551	\$1,326,070	215,481
Utility Deposits (#)	0	0	0
Utility Deposits (\$)	\$0	\$0	\$0
Fix Payments (#)	0	0	0
Fix Payments (\$)	\$0	\$0	\$0
Processed Return Checks (#)	72	111	(39)
Processed Return Checks (\$)	(\$9,390)	(\$21,693)	\$12,303
Other Revenue Transactions (#)	0	0	0
Other Revenue Received (\$)	\$0	\$0	\$0
Accounts Receivable Payments (\$)	\$666,757	\$797,010	(\$130,253)
Accounts Receivable - Credit Card #	0	0	0
Accounts Receivable - Credit Card \$	\$0	\$0	\$0
Municipal Court - Fines/Bonds (\$)	\$104,967	\$100,177	\$4,790
Municipal Court - Credit Card (#)	318	277	41
Municipal Court - Credit Card (\$)	\$55,305	\$50,130	\$5,175
Municipal Court - C2G (#)	0	0	0
Municipal Court - C2G (\$)	\$0	\$0	\$0
Building Permits Cash Report (\$)	\$195,582	\$375,978	(\$180,396)
Building Permits Credit Card (#)	329	325	4
Building Permits Credit Card (\$)	\$143,000	\$202,481	(\$59,481)
Building Permits C2G (#)	0	0	0
Building Permits C2G (\$)	\$0	\$0	\$0
Occupational License - Bldg Insp. (\$)	\$2,830	\$1,974	\$856
Occupational License - Bldg Insp. CC (#)	14	17	(3)
Occupational License - Bldg Insp. CC (\$)	\$1,330	\$1,824	(\$494)
Business License - City Clerk (\$)	\$3,808	\$1,050	\$2,758
Business License - City Clerk CR CD (#)	0	0	0
Business License - City Clerk CR CD (\$)	\$0	\$0	\$0
Convenience Fees - All Payments (#)	0	0	0
Convenience Fees - All Payments (\$)	\$0	\$0	\$0
Bank Drafts Billed (#)	0	0	0
Bank Drafts Billed (\$)	\$0	\$0	\$0
Interdepartmental Billing (#)	0	0	0
Interdepartmental Billing (\$)	\$0	\$0	\$0
Accounts Receivable Billed (\$)	\$70,607	\$626,758	(\$556,151)

## Budget Services Division

	FYE 22		FYE 21	
	December	YTD	December	YTD
PERSONNEL HOURS - FULL TIME				
Total Regular Hours Available	320.00	2,079.50	320.00	2,080.00
Total Comp Time Available	0.00	6.00	0.00	4.00
Total Overtime Hours	0.00	0.50	0.00	1.00
Total Bonus Hours	0.00	0.00	0.00	0.00
Total Furlough Hours	0.00	0.00	0.00	0.00
 TOTAL HOURS AVAILABLE	 320.00	 2,086.00	 320.00	 2,085.00
Benefit Hours Taken	53.25	312.00	84.00	279.50
 TOTAL ACCOUNTABLE STAFF HOURS	 266.75	 1,774.00	 236.00	 1,805.50
 PERMANENT PART-TIME				
Total Regular Hours Available	0.00	0.00	0.00	0.00
Total Comp Time Available	0.00	0.00	0.00	0.00
Total Overtime Hours	0.00	0.00	0.00	0.00
Total Bonus Hours	0.00	0.00	0.00	0.00
 TOTAL HOURS AVAILABLE	 0.00	 0.00	 0.00	 0.00
Benefit Hours Taken	0.00	0.00	0.00	0.00
 TOTAL ACCOUNTABLE STAFF HOURS	 0.00	 0.00	 0.00	 0.00
 TEMPORARY				
Total Regular Hours Available	0.00	0.00	0.00	0.00
Total Overtime Hours	0.00	0.00	0.00	0.00
 TOTAL HOURS AVAILABLE	 0.00	 0.00	 0.00	 0.00

## Treasury Division

	FYE 22		FYE 21	
	December	YTD	December	YTD
PERSONNEL HOURS - FULL TIME				
Total Regular Hours Available	800.00	4,768.25	800.00	5,200.00
Total Comp Time Available	5.75	74.50	19.50	134.25
Total Overtime Hours	42.00	242.00	30.50	122.50
Total Bonus Hours	0.00	0.00	0.00	0.00
Total Furlough Hours	0.00	0.00	0.00	0.00
TOTAL HOURS AVAILABLE	847.75	5,084.75	850.00	5,456.75
Benefit Hours Taken	181.50	755.75	169.50	823.25
TOTAL ACCOUNTABLE STAFF HOURS	666.25	4,329.00	680.50	4,633.50
PERMANENT PART-TIME				
Total Regular Hours Available	0.00	0.00	0.00	0.00
Total Comp Time Available	0.00	0.00	0.00	0.00
Total Overtime Hours	0.00	0.00	0.00	0.00
Total Bonus Hours	0.00	0.00	0.00	0.00
TOTAL HOURS AVAILABLE	0.00	0.00	0.00	0.00
Benefit Hours Taken	0.00	0.00	0.00	0.00
TOTAL ACCOUNTABLE STAFF HOURS	0.00	0.00	0.00	0.00
TEMPORARY				
Total Regular Hours Available	0.00	0.00	0.00	0.00
Total Overtime Hours	0.00	0.00	0.00	0.00
TOTAL HOURS AVAILABLE	0.00	0.00	0.00	0.00



**UTILITY      3C**

## Utility Division

	FYE 22		FYE 21	
	December	YTD	December	YTD
PERSONNEL HOURS - FULL TIME				
Total Regular Hours Available	2,442.00	15,488.00	2,560.00	17,317.00
Total Comp Time Available	13.00	173.25	3.25	148.25
Total Overtime Hours	54.75	570.25	213.75	1,461.50
Total Bonus Hours	0.00	0.00	0.00	0.00
Total Furlough Hours	0.00	0.00	0.00	0.00
TOTAL HOURS AVAILABLE	2,509.75	16,231.50	2,777.00	18,926.75
Benefit Hours Taken	325.25	2,478.25	630.75	3,180.25
TOTAL ACCOUNTABLE STAFF HOURS	2,184.50	13,753.25	2,146.25	15,746.50
PERMANENT PART-TIME				
Total Regular Hours Available	0.00	0.00	0.00	0.00
Total Comp Time Available	0.00	0.00	0.00	0.00
Total Overtime Hours	0.00	0.00	0.00	0.00
Total Bonus Hours	0.00	0.00	0.00	0.00
TOTAL HOURS AVAILABLE	0.00	0.00	0.00	0.00
Benefit Hours Taken	0.00	0.00	0.00	0.00
TOTAL ACCOUNTABLE STAFF HOURS	0.00	0.00	0.00	0.00
TEMPORARY				
Total Regular Hours Available	0.00	0.00	0.00	0.00
Total Overtime Hours	0.00	0.00	0.00	0.00
TOTAL HOURS AVAILABLE	0.00	0.00	0.00	0.00

## Office Services

	FYE 22		FYE 21	
	December	YTD	December	YTD
PERSONNEL HOURS - FULL TIME				
Total Regular Hours Available	320.00	2,077.75	320.00	2,080.00
Total Comp Time Available	0.00	0.00	0.00	0.00
Total Overtime Hours	26.50	197.25	30.00	199.00
Total Bonus Hours	0.00	0.00	0.00	0.00
Total Furlough Hours	0.00	0.00	0.00	0.00
TOTAL HOURS AVAILABLE	346.50	2,275.00	350.00	2,279.00
Benefit Hours Taken	73.50	349.75	83.00	214.25
TOTAL ACCOUNTABLE STAFF HOURS	273.00	1,925.25	267.00	2,064.75
PERMANENT PART-TIME				
Total Regular Hours Available	0.00	0.00	0.00	0.00
Total Comp Time Available	0.00	0.00	0.00	0.00
Total Overtime Hours	0.00	0.00	0.00	0.00
Total Bonus Hours	0.00	0.00	0.00	0.00
TOTAL HOURS AVAILABLE	0.00	0.00	0.00	0.00
Benefit Hours Taken	0.00	0.00	0.00	0.00
TOTAL ACCOUNTABLE STAFF HOURS	0.00	0.00	0.00	0.00
TEMPORARY				
Total Regular Hours Available	0.00	0.00	0.00	0.00
Total Overtime Hours	0.00	0.00	0.00	0.00
TOTAL HOURS AVAILABLE	0.00	0.00	0.00	0.00

### Drive-up Window and Mail Payments

	FYE 22 December	FYE 22 November
Mail Payments - Lockbox	14,275	15,041
Mail Payments - Office	91	23
<b>Mail Payments - Subtotal</b>	<b>14,366</b>	<b>15,064</b>
Night Deposit	195	216
Click-to-Gov Payments	0	0
Paymentus Payments	11,767	12,732
IVR Payments	0	0
<b>Without assistance payments - Subtotal</b>	<b>11,962</b>	<b>12,948</b>
Drive-up window & inside counter	2,334	2,535
Credit Card machine payments (swipe)	0	0
Credit Card machine payments (phone)	0	0
<b>With assistance payments - Subtotal</b>	<b>2,334</b>	<b>2,535</b>
<b>Total Payments Processed - Subtotal</b>	<b>28,662</b>	<b>30,547</b>
Bank Draft (ACH) Payments	10,445	10,995
<b>Total Payments (Utility)</b>	<b>39,107</b>	<b>41,542</b>
Total Convenience Fees - all Payments	0	0
<b>Grand Total Payments</b>	<b>39,107</b>	<b>41,542</b>

### Traffic Counter at Drive-up Facility

Night Drop *	0	0
8-5 Drive-up Window Customers *	0	0
<b>Total Traffic Counter</b>	<b>0</b>	<b>0</b>

\* These figures are included in the above Total Customer Contact Payments.

## Meter Reading Division

	FYE 22		FYE 21	
	December	YTD	December	YTD
Number of Meters Read	38,506	231,269	41,140	184,653
New Service	753	4,627	666	4,562
Request for Termination	779	4,573	686	4,466
Delinquent On(s)	252	1,390	242	1,278
Delinquent Offs	243	1,433	257	1,490
Collect Deposit Tags Hung	0	0	0	60
Collect Deposit Cut Offs	0	0	0	12
Blue Tags	0	0	0	77
Number of Meters Re-read	1,258	6,900	479	3,773
Meters Cleaned	2	87	0	291
Customer Assists	0	0	0	277
Meters Pulled	0	6	0	0
Meters Re-set	0	0	0	0
Meter Exchanges	63	254	53	342
TOTAL	41,856	250,539	43,523	201,281

## Utility Division Activity Report

	FYE 22		FYE 21	
	December	YTD	December	YTD
STATUS REPORT				
Regular Utility Accounts Billed	44,173	261,961	44,140	262,454
New Ons	661	4,682	699	5,959
Final Accounts Billed	366	3,763	737	4,251
TOTAL ACCOUNTS BILLED	45,200	270,406	45,576	272,664

## **FIRE DEPARTMENT**

**4**

## NFD Monthly Progress Report December 2021

### Incident Response Type Summary

Incident Type	Total	% of Total
1 - Fire	46	2.86%
2 - Overpressure Rupture, explosion, Overheat - No Fire	0	0.00%
3 - Rescue & emergency	947	58.89%
4 - Hazardous Conditions (No Fire)	33	2.05%
5 - Service Call	141	8.77%
6 - Good Intent Call	344	21.39%
7 - False Alarm & False Call	74	4.60%
8 - Severe Weather & Natural Disaster	0	0.00%
9 - Special Incident Type	2	0.12%
Incomplete Reports	21	1.31%
<b>Total Incident Count (Unique Calls)</b>	<b>1608</b>	<b>100.00%</b>
<b>Number of Total Unit Responses</b>	<b>2195</b>	

Total Fire Loss    \$494,600.00

	Number of First-In Calls	Average Time/Seconds	Average Time/Minutes
<b>Station #1</b>	305	276	0:04:36
<b>Station #2</b>	246	317	0:05:17
<b>Station #3</b>	280	341	0:05:41
<b>Station #4</b>	197	320	0:05:20
<b>Station #5</b>	67	560	0:09:20
<b>Station #6</b>	59	611	0:10:11
<b>Station #7</b>	164	333	0:05:33
<b>Station #8</b>	110	314	0:05:14
<b>Station #9</b>	178	358	0:05:58

### Community Outreach

Tours and Special Events	7	Christmas Parade, Winterfest & Station Tours
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### Burn Permits

Burn Permits Issued	89	Total of 5 burn days
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### Training

Total Personnel Training Hours	1690	Mgmt/Supvrsr, Hazmat, Wildland, Special Healthcare, etc.
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# NFD Monthly Progress Report

December 2021

Item 4.

## Total Calls By Unit

	Total Number of Responses	District 1	District 2	District 3	District 4	District 5	District 6	District 7	District 8	District 9
Chief 301	19	2	1	4	3	1		6	1	1
Chief 302	18	3	4		6		2	1	1	1
Chief 303	20	2	7	4	3			1	1	2
Chief 304	17	2	3	1	1		3	2	3	2
Chief 401	14		1	5		3	2	1		2
Chief 402	32	4	5	1	5	3	5	2	3	4
Chief 403	10	1		3		3		1	1	1
Chief 404	9	1	1			1	3	1		2
Engine 1	332	299	1	8	2		6	8		8
Brush 1	11	3		1			5			2
Ladder 1	25	6	3	6	4			2	1	3
Engine 2	272	6	239	5	12			7	2	1
Brush 2	14		12		1				1	
Ladder 2	38	2	15	6	7			4	1	3
Engine 3	301	7	4	278		1	1	2	1	7
Brush 3	6		2	2		1				1
Engine 4	217		12	1	194			5	5	
Brush 4	9		5		3				1	
Tanker 4	3		1			1			1	
Engine 5	25					23	2			
Brush 5	69					68	1			
Brush 52	1					1				
Engine 6	34	1			2	6	21			4
Brush 6	76	2			2	8	59			5
Rescue 7	5		1					4		
Squad 7	211	12	14	10	8		1	156	6	4
Brush 7	6	1					2	1	1	1
Engine 8	126				8		2	6	110	
Brush 8	4				1		1		2	
Tanker 8	5				1		2	1	1	
Engine 9	214	9		10	3	5	8	2		177
Brush 9	16	1		1		3	5			6
Tanker 9	16			1		4	7			4
HAZMAT	1	1								
Gator 1	1					1				
EMS1	9		1	2	3		1	1		1
Fire Marshal 5	4		1				2			1
Fire Marshal 6	5			1		3		1		
	2195	365	333	350	269	136	141	215	143	243

**NORMAN FIRE DEPARTMENT  
MONTHLY PROGRESS REPORT  
December 2021**



**Fire Review Coordinator Activities**

<b>Activites</b>	<b>Notes</b>	<b>Number</b>	<b>Staff Hours</b>
Building Permits/Development Review Team		28	73
Inspections		12	16
Citizen Calls for Information		4	3
Training	CFI Investigation and EMS	3	16
Meetings		6	6
Fire Investigations	Report and Research	3	17
<b>Totals</b>		<b>56</b>	<b>131</b>

## EMERGENCY MANAGEMENT DIVISION ACTIVITIES

Emergency Management Division	
Comments: December 2021	
Regular Monthly Scheduled Activities	Unless otherwise noted all meetings are held at the Norman Fire Training Center 2207 Goddard Ave, Norman, OK
Each morning at 7:00 am, a silent test of the outdoor warning system is conducted	The test provides an operational snapshot of the status of the system. This information provides information if a unit needs maintenance and if it is operating properly
Each Monday morning at 10:00 am, the National Weather Service conducts a video call regarding the upcoming weekly weather.	This call has the option for video participation and telephone call in. It is primarily for the Emergency Management of jurisdictions, school, State offices involved in EM, Tribes and other entities tasked with severe weather operations. It is not intended for the general public nor is this just a weather forecast. This time allows for interaction with the NWS about concerns that directly affect the local jurisdiction so they may better prepare for incoming weather.
Each Tuesday evening at 6:30 pm, ELMER night with the Amateur radio club (SCARS, <a href="http://www.w5nor.org">www.w5nor.org</a> ) at the Fire Training Center (South Canadian Amateur Radio Society)	The club mentors other HAMS, works on projects and equipment, provides general support to the City and Public on Amateur operations
Each Wednesday Morning 9:15 am	Radio test with State Emergency Management. This tests the local and statewide capability for voice communication to the State office and to other jurisdictions.
Each first Thursday evening of the month is amateur radio testing night at 6:00 pm	Open to the public, the club provides the opportunity for the community to test for their Amateur license or upgrade a license. Note: the FCC has been directed to start charging for testing. Effective date is TBD

Each Saturday 12:00 Outdoor Warning audible test. This test is supported by the Amateur radio club to assist in identifying and verifying units needing maintenance. Residents can assist by “Adopting a Pole” and reporting the siren status they adopt at the website <a href="http://www.w5nor.org">www.w5nor.org</a>	Audible test of the outdoor warning system is conducted for 60 seconds. Three units are sounded for 20 seconds due to being a public park venue.
Second Thursday of each month is the Norman Emergency Response Team Volunteers and the Medical Reserve CORPS members to meet, network and discuss preparedness support and collaboration with the Cleveland County Health Department on use of the Medical Reserve Corps	The Volunteer meeting at 6:30 pm. At 6 pm we meet for social time before the meeting. Usually we have snacks and drinks. This networking allows for camaraderie and building relationships during different organizations. City staff is always welcome.
Each Third Thursday of the month is reserved for Division Staff duties and collaboration with the Disaster Assistance Teams (DAT) of the American Red Cross	Meetings are conducted at the Fire Training Center and usually held on an as needed basis.
Local Emergency Preparedness Committee	Meets quarterly (normally at the Norman Regional hospital on Porter) under the management of the Cleveland County Emergency Management office. The public is welcome to address any concerns regarding emergency planning or SARA Title III information
<b>Other Emergency Management Activities</b>	
<b>Local Response</b>	
Red Cross Coordination for burn outs. In December there were five fires responded to in order to assist families with immediate needs. One of these was a motel. Note: the management of the Motel (Travel Lodge of Norman) did an outstanding job of aiding the victims of the fire in relocating to other motels and helping them to recover vital medicines like insulin.	With the robust reorganization in the Red Cross and the turmoil of having a in home fire, the volunteers or I, when called, will respond to the scene, (physically or by phone and assist the family in coordination with the Red Cross to provide immediate assistance.
<b>Youth Camp</b>	
Planning of the 2022 Youth Preparedness Camp to be held at Northeastern University in Tahlequah, OK is underway. Dates are July 16-22, 2022. In January 2022, active recruitment	In 2019, the first youth preparedness camp was held at NSU in Tahlequah. 35 students from around the state participated and supported by 25 adults. This program

<p>will begin for students and volunteer adults. The initial walk through and planning meeting was conducted on November 4, 2022</p>	<p>trained students in two primary areas. 1: The Citizens Emergency Response Training program and 2: A course of Community Preparedness Training. Students and staff were housed in the dorms and logistic support was provided by NSU emergency Management. The course was very robust and kept the students and staff highly engaged in training and learning hands on skills critical in disaster operations. The FEMA Region 6 representative indicated the camp was the best he had seen so far. The camp was nationally recognized by Bill Bischof, FEMA Region 6, National Preparedness Division</p>
 <p>Camp Poster Rough Draft Edit (DW).doc</p>	 <p><a href="https://community.fema.gov/story/Summer-Camps-Turn-Teens-into-Preparedness-Leaders?lang=es%2F">https://community.fema.gov/story/Summer-Camps-Turn-Teens-into-Preparedness-Leaders?lang=es%2F</a></p> <p>Camp Poster Rough Draft Edit (DW).doc</p>
<p>The Norman Art Walk conducted each second Friday contacted the EM Division for support of a cooling station.</p>	<p>In August of 2021, Norman response volunteers established 2 mister fans opposite corners of each other which provided a quick relief for numerous attendees at the Art walk. Since then volunteers have been present during the evening providing preparedness literature and answer questions regarding severe weather operations.</p>
<p>Norman EM coordinates the activities of the Cleveland County Medical Reserve CORPS Unit 0333 in collaboration with the Cleveland County Health Department. During the COVID response by the health department the MRC was vital in operating numerous points of distribution for the vaccine injections. More than 3000 volunteer hours were provided during the height of the response.</p>	<p>The Emergency Management Coordinator met with the State Coordinator and the Cleveland County Local Response Coordinator (LRC) to discuss the reorganization of the health departments and the management of the MRC Unit. Cleveland County is now assigned to take care of solely Cleveland County. Previously they supported 5 counties. The reassignment identifies the health department as Region 10, Cleveland County. This will assist in the collaboration of the MRC unit and will continue to enhance the Norman Emergency Volunteer response.</p>

Norman Annual Christmas Dinner	On December 23 and 24, 2021 members of the Norman Response Team, Medical Reserve CORPS and Envision Success for Veterans supported the annual Christmas dinner and assisted in establishing the gift area. Norman EM provided 6000 masks to the organization for the public to continue the fight against COVID. Thousands of gifts were donated and sorted by age group for Santa to provide to the resident's children. More than 1000 residents enjoyed a Christmas meal.
<b>Disaster Reimbursement Status</b>	
FEMA has instituted a new process for reimbursement claims. As with any new process there are many issues to be worked through	It is vital for Volunteers to ensure their time has been recorded. Volunteer hours are dollars for a jurisdiction. The hours go to meeting cost share and having a value added impact for the local jurisdictions.
Disaster DR-4222 2015	Federal portion paid, State portion of \$91,808.32 is pending
Disaster Dr-4575 October 2021	The initial FEMA review has been completed and forwarded to the State EM for review. The State has requested additional information and the request was given to the Division (12-09-2021) to provide their portion of the requested information.
Disaster 4587 February 2021	The initial application to FEMA was opened. The process of gathering, reviewing and submitting documentation is ongoing and should have the first FEMA review done by end of January 2022.
<b>Mitigation Grant Status</b>	
Norman Safe Room grant program	The State Office of Emergency Management has under gone several personnel changes. The COVID pandemic compounded the state's office and ability to continue work to close out this grant. They have hired a new grants manager and the EM Division will continue to work with the state to finish the grant.

<b>Severe Weather Issues</b>	
National Weather Service Storm Spotter Training	<a href="#">NWS Norman Spotter Schedule (weather.gov)</a>

## **HUMAN RESOURCES 5**



## HUMAN RESOURCES

### Monthly Report

December 2021

Item 4.

#### ADMINISTRATION

##### A. Administrative Support

- Processed Monthly Department Report
- Processed invoices and reconciled expense accounts
- Began 2023 Budget Preparation
- Met with Chief Diversity & Equity Officer regarding personnel manual
- Assembled Employee Recognition packets for department distribution in January

#### BENEFITS

- Conducted ten (10) new hire orientations and one (1) PPT
- Processed enrollment forms, terminations, and changes to health, life, and supplemental products for general health/dental changes
- Reconciliation of health, dental, and supplemental products
- Continued education of plan documents
- Fielded approximately 150 phone calls discussing benefits, claims, and open enrollment changes
- ACA reporting
- Working with Healthy Merits to open registration window in January for City's Wellness Screenings
- Webex Meeting with EHealth Biometric Screening 2022 Wellness Program
- Continued correspondence with EHealth - fine tuning details regarding set up of on-site biometric screenings for 2022 Wellness Program

#### COMPENSATION

##### New Hires – 14

Dept./Div.	Position	Number of Employees
Finance	Meter Reader	2
Public Works/Stormwater	Stormwater Program Manager	1
Public Works/Fleet	Mechanic II	1
Public Works/Fleet	Fleet Service Tech	1
Public Works/Transit	Maintenance Worker I	1
Utilities/Water Line Maint.	Utility Distribution Worker I	1
Utilities/Sanitation	Sanitation Worker I	2
Police/Emerg. Comm.	Communications Officer I	1
Utilities/Water Recl.	Custodian (PPT)	1
Planning/CDBG	Emergency Shelter Case Mgr. (PT)	1
Police/Investigations	Victim Advocate Field Rep (PT)	1
Parks/Recreation	Recreation Center Specialist (PT)	1

**Separations – 11**

Item 4.

Dept./Div.	Position	Number of Employees
Police/Patrol	Parking Service Officer	1
Police/Patrol	Police Officer	1
Police/Emerg. Comm.	Communications Officer II	1
Finance/Utilities	Meter Reader	1
Utilities/Water Treatment	Maintenance Worker I	1
Utilities/Water Line Maint.	Utilities Supervisor	1
Parks/Westwood Golf	Golf Course Attendant (PT)	2
Parks/Recreation	Tennis Shop Attendant (PT)	3

**Promotions – 5**

Dept./Div.	Position	Number of Employees
Utilities/Water Line Maint	Line Maint. Division Mgr.	1
Utilities/Water Line Maint.	Utilities Supervisor	1
Utilities/Water Reclamation	Household Hazardous Waste Coord.	1
Utilities/Sanitation	Sanitation Worker II	1
Utilities/Water Treatment	Utilities Supervisor	1

**RECRUITMENT****Accepted applications for the following positions:**

Department/Division	Position
Fire Department	Fire Training Officer
Finance/Meter Services	Meter Reader
Finance/Meter Services	Meter Services Rep
Human Resources	Director of Human Resources
Information Technology	Systems Support Technician
Parks & Recreation/Facility Maintenance	Custodian (PPT)
Parks & Recreation/Facility Maintenance	Facility Maintenance Supervisor
Parks & Recreation/Recreation-Irving	Recreation Center Specialist (PT)
Parks & Recreation/Recreation-Little Axe	Recreation Center Specialist (PT)
Parks & Recreation/Recreation-Whittier	Recreation Center Specialist (PT)
Parks & Recreation/Westwood Golf Course	Golf Course Attendant (PT)
Parks & Recreation/Westwood Tennis Center	Tennis Shop Attendant (PT)
Planning & Community Dev./CDBG	Emergency Shelter Case Manager (PT)
Police/Animal Welfare	Animal Welfare Center Manager
Police/Animal Welfare	Shelter Veterinarian
Police/Animal Welfare	Veterinary Technician
Police/Emergency Communications Bureau	Communications Officer I
Police/Emergency Communications Bureau	Communications Officer II
Police/Staff Services	Victim Advocate Field Rep
Public Works/Engineering	Capital Projects Engineer
Public Works/Engineering	City Surveyor
Public Works/Fleet	Fleet Service Technician
Public Works/Fleet	Maintenance Worker I
Public Works/Fleet	Mechanic I
Public Works/Stormwater	Maintenance Worker I
Public Works/Stormwater	Maintenance Worker II
Public Works/Stormwater	Stormwater Compliance Inspector
Public Works/Streets	Maintenance Worker I
Public Works/Streets	Maintenance Worker II

Public Works/Transit		Transit Support Technician (PPT)		Item 4.
Utilities/Sanitation		Sanitation Worker I		
Utilities/Sanitation		Sanitation Worker II		
Utilities/Water Line Maintenance		Utility Distribution Worker I		
Utilities/Water Line Maintenance		Utilities Supervisor		
Utilities/ Water Line Maintenance		Water Maintenance Specialist		
Utilities/Water Treatment Plant		Maintenance Worker I		
Utilities/Water Treatment Plant		Plant Operator D		
Utilities/Water Treatment Plant		Temporary Laborer (PT)		
Utilities/Water Treatment Plant		Utilities Supervisor		
Hiring Statistics		Recruitment Statistics		
Pre-Employment Background Investigations	12	Advertisements Placed	28	
Pre-Employment Drug Screens	18	Applications Received	374	
Pre-Employment Physicals	15	Job Announcements Emailed	51	
Pre-Employment OSBI	6	Job Announcements to CON Depts.	720	

#### Recruitment & Hiring Statistics:

Contacts/Inquiries		Selection Process Elements	
In Person	310	Written Exams	1
Phone	405	Practical Testing/Assessment Center	0
Mail	215	Panel Board Interviews	10
Email	200	Promotions	7
Total Subscribers on E-mail Vacancy List	1590	Oral Interviews	0
Total Page Views for HR Website	4,973	Hiring/Promotion Board	0

### TRAINING AND DEVELOPMENT

Conducted training for ten (10) new employees on the topics of Customer Service, Workplace Harassment, Workplace Violence, CON Code of Ethics and Conduct, Computer Networks and Communications, IT Acceptable Use and Social Media policies.

Provided Advanced Supervisory Academy Session 3 on the topic of Improvement Plans for Employee Development and Remediation for 30 employees in management, supervisory, lead and crew chief roles from various departments. The course covered methods for measuring employee development.

The Computer Training Lab was the site for ERP project management for IT and EAM training for various departments.

### SAFETY

- Safety material documents were sent to divisions each week
- Conducted one (1) Fitness for Duty Meeting for Fire
- Conducted ten (10) new employee orientations
- Conducted certification in Forklift Safety Training for five (5) employees

#### Recordable Injuries – 4

Dept./Division	Nature of the Injury	Activity	Prognosis
----------------	----------------------	----------	-----------

Finance/ Meter Services	Right shoulder strain	Felt sharp pain after digging out a meter box	Work Restrictions	Item 4.
Fire/ Suppression	Strained head, neck & upper back	Wet ceiling debris fell on top of fireman	Work restrictions	
Utilities/ Line Maint.	Bite to right hand	Dog attacked service technician during maintenance in back yard	Work restrictions	
Utilities/ Line Maint.	Bite to right hand	Dog attacked service technician during maintenance in back yard	Work restrictions	

**Recordable Injuries per calendar year. CY 2021 is current year to date:**

2021	2020	2019	2018	2017	2016
64	57	65	71	59	69

**Vehicle Collisions: 0**

Division	Description of Collision	Status

**Current number of “at fault” Vehicle Collisions per fiscal year:**

2022	2021	2020	2019	2018	2017
1	10	3	8	5	17

## INFORMATION TECHNOLOGY 6

## CITY OF NORMAN

### Information Technology Department Monthly Report – December 2021.

Working projects for the IT Department are as follows:

Project	Description/improvement anticipated	Status
ERP Replacement Project	Process improvements for finance, purchasing, AR/AP, Courts, HR, Payroll, Personnel, and Permits Management through newer technology, software, and business review processes.	In Progress –Project team has completed implementation work on Incode for Municipal Court, Munis for Financials, Advanced Utilities for utility billing, Munis for Payroll, and is currently in the implementation phase and Tyler EAM for Work Orders, as well as Intellitime for Time and Attendance. Munis for HR Module has completed. CityView for Planning and Permits began May 2021.
Main Site data center upgrades	Necessary upgrades to current infrastructure are needed to enhance capabilities and continue with power saving and cooling efforts by reducing the amount of physical servers through virtualization.	Ongoing: FYE18 funding of new core switching has been approved and switches have been installed. IT Network staff are in implementation and testing of networking and security appliances Q1 of 2021.

Water tower and mono-pole contract negotiations.	Increased Revenue and compliance for water tower/cellular mono-pole leases. Repair requirements.	Ongoing
Fiber Optic Installation for redundant loop at WTP and EOC	IT and the Utilites Department will be using capital funds to connect a microwave antenna from Fire station 9 to the Water treatment plant and connect to the Emergency Operations Center once that building is complete. This will create a redundant loop for the WTP and EOC incase of lost service from the main connection.	Awaiting Approval possible launch in FYE22
New Building construction support.	The IT Department is involved in all new building construction planning and will be responsible to map our networking infrastructure, building air for network closets, power for equipment and battery backups, network drops, wireless network coverage, hardware installation, and physical moves of staffs computer systems in all building projects (Norman Forward Driven)	In Planning
Main Street Roadway Lighting and Signal Interconnect (Main at Santa Fe, Peters, Crawford, and Porter)	Increase the coverage of traffic signals and the IT Fiber infrastructure which will require IP addresses for viewing in Centrac	In Progress
IT Security training efforts for all network and email users at the City of Norman.	Increase the knowledge and stay current on all new and rapidly changing cyber-attack methods so that the CoN network is protected by every employee who uses the network and email.	Ongoing

Installation of fiber optic cable along 36th Avenue NW from Robinson Street to Main Street with switches at Brookhollow Road, Quail Drive, River Oaks Drive, and a new signal at Havenbrook Street.	Increase the coverage of traffic signals and the IT Fiber infrastructure--will require IP addresses	In Progress
Installation of fiber optic cable along 12th Avenue SE from Highway 9 to Cedar Lane Road with a switch at a new signal at Campus Crest Drive.	Increase the coverage of traffic signals and the IT Fiber infrastructure--will require IP addresses	Expected completion in Q1 or Q2 2019
Installation of fiber optic cable along 12th Avenue NE from Robinson Street to Alameda Street with switches at Main Street, Norman Public Schools Bus Barn, and Morren Drive.	Increase the coverage of traffic signals and the IT Fiber infrastructure--will require IP addresses	In Progress
Installation of fiber optic cable along 12th Avenue NE from Rock Creek Road to Robinson Street with switches at Rock Creek Road and a modified traffic signal at High Meadows Drive	Increase the coverage of traffic signals and the IT Fiber infrastructure--will require IP addresses	
Installation of fiber optic cable along Classen Boulevard from Highway 9 to Cedar Lane Road with switches at new signals at Ann Branden Boulevard, Renaissance Drive, and Southlake Boulevard.	Increase the coverage of traffic signals and the IT Fiber infrastructure--will require IP addresses	



### Support Tickets:

The IT department tracks work requests with a software package called Spiceworks. **IT Table 1** below represents the number of support tickets closed by the IT Support Staff and those remaining open at the end of December 2021.

### Mass Communications:

The following statistics represent email space and resource savings. In the attached table, you will see that 12 emails from the groups shown were sent from city servers using city resources – of those 14,560 were delivered to outside mailboxes for the month of December 2021. The city servers generated mass communications to Norman citizens of 16,560 messages from only 12 sent (see **IT Table 2**).

### Email Security Appliance:

The City of Norman's IT Department has an email filter that has enhanced reporting and filtering capabilities that protect the City's systems from malicious attacks from the outside. Email is one of the preferred methods of the delivery of malicious software and viruses. The IT department monitored 366,434 attempted incoming and 93,770 outgoing messages for the month of December 2021. Incoming messages totaling 190,427 were considered Spam or hazardous e-mails by our email-filtering appliance, and were quarantined or filtered (see **IT Table 3**). This number represents nearly 52% of our inbound mail. This percentage continues to be the norm for malicious email/spam. The IT Department has made the decision to block inbound traffic from known malicious countries and we continue to monitor and analyze the situation on a daily basis. Inbound email messages of this nature means increased vulnerabilities and attack vectors into the city. Without the email filter appliance, our email server would have received more mail, which increases the opportunity for entrance of a virus into the network. It also creates waste and decreases valuable storage space. Additionally, the email filter appliance kept city employees from bombardment of a significant amount of spam, phishing, and other types of potentially harmful emails.

### Web Site:

The City of Norman's web site is hosted, updated, and maintained by the IT Department. In the month of December 2021, the City of Norman's web site had 77,778 individual web sessions access the web site for 167,270 total page views. Of those sessions, 43,197 were identified as New Users to view content on the City web site (see **IT Table 4**). In July of 2019, the IT Department kicked off a project with Interpersonal Frequency to overhaul the City website. Since its completion in June 2020, the site has reduced and more relevant information as well as the ability to function seamlessly from mobile devices and tablets.

## ERP Project Implementation Progress:

The chart below shows the current progress of the ERP Implementation Project. The project began in January of 2018 and is expected to run through the second quarter of 2022. The City Council has approved approximately \$6 Million to replace the outdated software systems that run our daily business operations. Once complete the city will have enhanced automated services and web services for our citizen base as well as the employees who use and track their daily work with these business systems. The IT Department has completed implementation of Parks and Rec software (Vermont Systems), Municipal Court software (Incode), Finance software (Munis), Utility Billing software (CIS Infinity), and Payroll (Munis). Human Resource Management (HRM), and EAM for Work Orders is our focus for the Months of Feb through July/August 2021. Daily work continues on these systems as well as additional training and configuration on the Utility Billing software. Server installation and configuration for Planning and Community Services (CityView) application began in May of 2021. The COVID-19 pandemic had an adverse effect on the completion of CIS Infinity and the starting of CityView because of vendor resource availability and travel restrictions in Canada (both companies are Harris owned and based in Canada). The final phase of our software upgrades, City View, is expected to be completed by the end of FYE22 or the start of FYE23.

		2018												2019												2020												2021													
Community / Phase	Legacy	Jan	Feb	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	Nov	Dec	Jan	Feb	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	Nov	Dec	Jan	Feb	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	Nov	Dec														
City of Norman Implementation Plan																																																			
Parks & Rec: Vermont	Manual																																																		
Municipal Court: Tyler Incode	Custom																																																		
Phase 1: Financial Management	HTE																																																		
Utility Billing: Advanced Utilities	HTE																																																		
Phase 2: EAM: Facilities, Line Maint, Storm water	HTE																																																		
Planning and Community Dev.: Cityview	HTE																																																		
Phase 3: Human Resource Management	HTE																																																		
Time & Attendance: IntelliTime	Manual																																																		
Website Re-Design	Custom																																																		
Phase Months:		0	1	2	2	1	3	3	3	2	2	2	2	2	3	2	2	3	3	4	4	4	4	4	4	5	5	6	6	5	7	7	5	5	4	4	3	2	2	0	0	1	0	1	0	1	0	0	1	1	1

Table 1

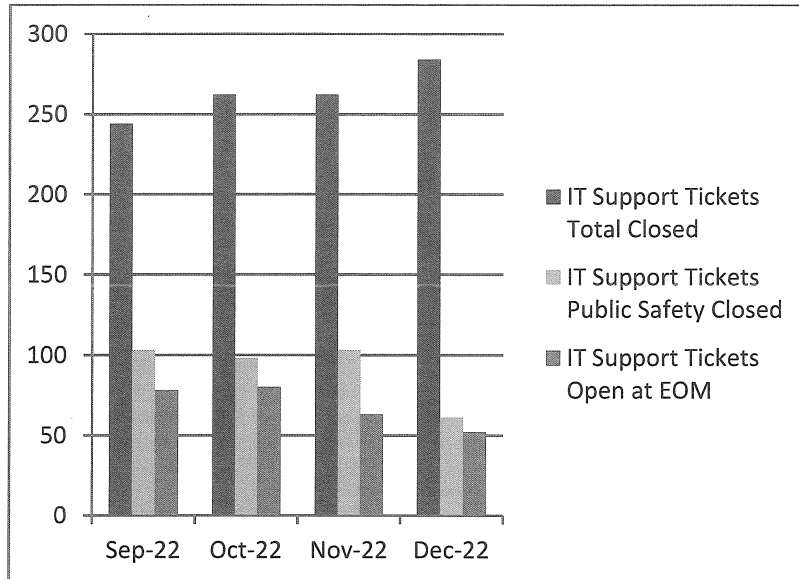


Table 2

DEC 2021 LIST SERVER REPORT			
Group	Active Members	Mailings	Total Delivered
Affirmative Action Group	17	3	51
Job Posting	1587	3	4761
Norman News	1958	6	11748
<b>Totals</b>	<b>3562</b>	<b>12</b>	<b>16560</b>



## SECURE EMAIL GATEWAY

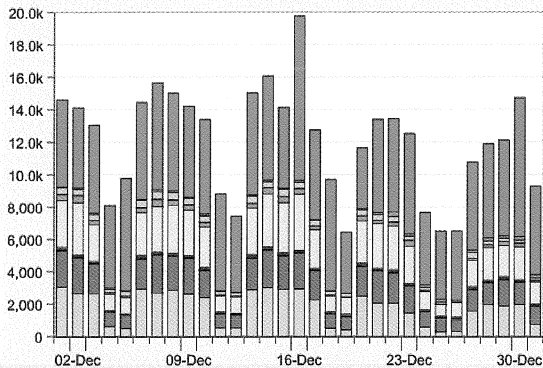
## Executive Summary

ironport.example.com

01 Dec 2021 00:00 to 31 Dec 2021 23:59 (GMT -06:00)

Data in time range: 100.0 % complete

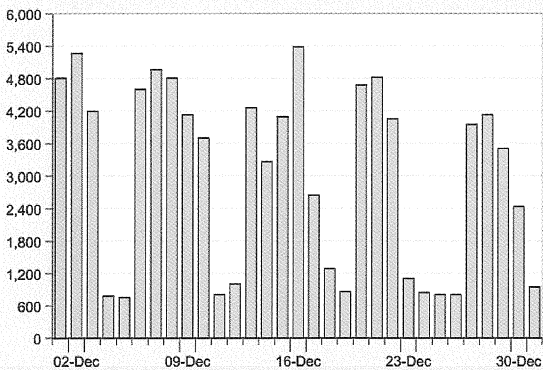
Incoming Mail Graph



Incoming Mail Summary

Message Category	%	Messages
Stopped by IP Reputation Filtering	48.5%	177,602
Stopped by Domain Reputation Filtering	0.9%	3,217
Stopped as Invalid Recipients	0.0%	1
Spam Detected	2.6%	9,389
Virus Detected	0.0%	28
Detected by Advanced Malware Protection	0.0%	33
Messages with Malicious URLs	0.0%	55
Stopped by Content Filter	0.0%	102
Stopped by DMARC	1.9%	6,951
S/MIME Verification/Decryption Failed	0.0%	0
<b>Total Threat Messages:</b>	<b>52.0%</b>	<b>190,427</b>
Marketing Messages	18.0%	65,826
Social Networking Messages	1.2%	4,433
Bulk Messages	13.3%	48,657
<b>Total Graymails:</b>	<b>32.4%</b>	<b>118,916</b>
S/MIME Verification/Decryption Successful	0.0%	0
Clean Messages	15.6%	57,207
<b>Total Attempted Messages:</b>		<b>366,550</b>

Outgoing Mail Graph



Outgoing Mail Summary

Message Processing	%	Messages
Spam Detected	0.0%	0
Virus Detected	0.0%	0
Detected by Advanced Malware Protection	0.0%	0
Messages with Malicious URLs	0.0%	0
Stopped by Content Filter	0.1%	58
Stopped by DLP	0.0%	0
Clean Messages	99.9%	93,757
<b>Total Messages Processed:</b>		<b>93,815</b>

Message Delivery	%	Messages
Hard Bounces	1.8%	1,641
Delivered	98.2%	92,129
<b>Total Messages Delivered:</b>		<b>93,770</b>

ironport.example.com - 01 Jan 2022 01:00 (GMT -06:00)

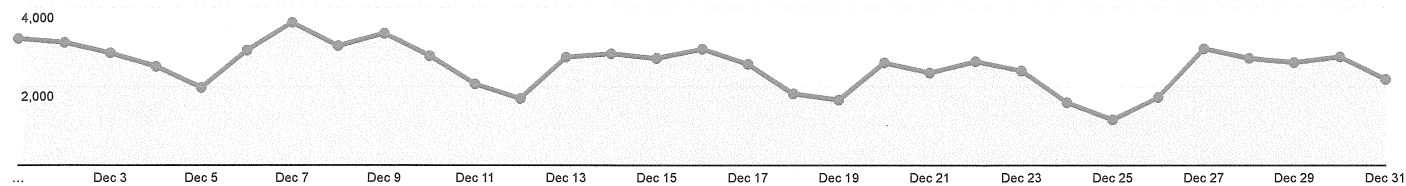
## Site Traffic

Dec 1, 2021 - Dec 31, 2021

All Users  
100.00% Sessions

Report Tab

Sessions



Day of the month	Sessions	Pages / Session	Pageviews	Users	New Users	Bounce Rate	Avg. Time on Page
	77,778 % of Total: 100.00% (77,778)	2.15 Avg for View: 2.15 (0.00%)	167,270 % of Total: 100.00% (167,270)	50,560 % of Total: 100.00% (50,560)	43,197 % of Total: 100.06% (43,169)	42.46% Avg for View: 42.46% (0.00%)	00:01:31 Avg for View: 00:01:31 (0.00%)
1. 07	3,618 (4.65%)	2.07	7,473 (4.47%)	3,199 (4.70%)	2,236 (5.18%)	50.14%	00:01:35
2. 09	3,346 (4.30%)	2.02	6,746 (4.03%)	2,953 (4.34%)	2,038 (4.72%)	51.85%	00:01:29
3. 01	3,211 (4.13%)	2.21	7,106 (4.25%)	2,791 (4.10%)	1,709 (3.96%)	39.74%	00:01:31
4. 02	3,110 (4.00%)	2.32	7,208 (4.31%)	2,689 (3.95%)	1,682 (3.89%)	40.55%	00:01:25
5. 08	3,027 (3.89%)	2.23	6,763 (4.04%)	2,657 (3.90%)	1,741 (4.03%)	47.14%	00:01:25
6. 27	2,942 (3.78%)	2.15	6,318 (3.78%)	2,544 (3.74%)	1,652 (3.82%)	38.95%	00:01:38
7. 16	2,940 (3.78%)	2.13	6,262 (3.74%)	2,576 (3.78%)	1,705 (3.95%)	44.80%	00:01:39
8. 06	2,912 (3.74%)	2.12	6,159 (3.68%)	2,577 (3.79%)	1,591 (3.68%)	43.44%	00:01:35
9. 03	2,850 (3.66%)	2.25	6,422 (3.84%)	2,476 (3.64%)	1,481 (3.43%)	39.96%	00:01:32
10. 14	2,825 (3.63%)	2.32	6,560 (3.92%)	2,431 (3.57%)	1,458 (3.38%)	37.27%	00:01:31

Rows 1 - 10 of 31

**LEGAL 7**

**MONTHLY REPORT - LEGAL DEPARTMENT**  
**December 2021 Report**  
**(Submitted January 14, 2022)**

**MONTHLY HIGHLIGHTS:**

Easley v. City of Norman, CJ-2015-304 T; Case No. 117292 (Convenience Fee) (K)

This case is a class action that challenges the City's authority to assess a convenience fee on certain credit card transactions. On December 17, 2021, the plaintiff dismissed this case without prejudice. Under 12 O.S. § 100, he has until December 17, 2022, to refile. This case will no longer appear on the Monthly Report.

**LIST OF PENDING CASES:**

**UNITED STATES COURT OF APPEALS FOR THE TENTH CIRCUIT**

**UNITED STATES DISTRICT COURT, EASTERN DISTRICT OF NEW YORK**

In re Payment Card Interchange Fee and Merchant Discount Antitrust Litigation, Case No. 05-MD-01720 (JG)(JO) (K)

**UNITED STATES DISTRICT COURT, WESTERN DISTRICT OF OKLAHOMA**

Doughty v. CentralSquare Technologies LLC, et al., CJ-2020-451; CIV-2020-451 (K)

Thompson v. City of Norman, et al., CJ-2019-71; CIV-19-13 (K)

Harmon, Toby, Shane Dodson, Tammi Dodson v. City of Norman, Jeff Robertson, Case No. 18-6187; CIV-18-688-HE (K)

**OKLAHOMA SUPREME COURT / COURT OF CIVIL APPEALS**

AMF Development v. City of Norman, et al., CJ-2018-1134; SD 119,677 (K)

Fleske Holding Company LLC v. City of Norman, CV-2018-956; SD 119,649

Kevin Easley v. City of Norman, CV-2012-346; DF-115811 & SD-119536

Golden Tribe LLC v. City of Norman, CV-2018-1142, DF-119107

Magnum Energy, Inc. v. Board of Adjustment for the City of Norman, SD-117912 (M)

**COURT OF CRIMINAL APPEALS**

None

**CLEVELAND COUNTY DISTRICT COURT**

**A. *General Lawsuits***

Armstrong v. City of Norman, CJ-2012-1638 (K)

City v. Haddock, CV-2010-357 TS (K, S)

City v. IAFF, CV-2011-48 L; DF-109447 (K)

City v. Komiske, Cobblestone Creek Management Company, et al., CV-2012-748 (K, W)

City v. Stachmus, Aaron & Anglin, Bryson, CJ-2021-445

FOP/IAFF/AFSCME v. Okla. Dept. of Labor and City of Norman, CJ-2005-1170 L (K)

FOP v. City of Norman, CV-2011-876 L (K)  
Martin Florez v. City of Norman & John Doe, CJ-2021-1051  
Caleb Fulton v. Loyal Reich, Reich Dozer Services, LLC, City of Norman, Board of County Commissioners of Cleveland County, State of Oklahoma, CJ-2020-797 (K)  
Kevin Hahn v. Norman Police Department, City of Norman, CJ-2021-210 (K)  
Henderson, et al. v. City of Norman, et al., Case No. CJ-2016-610 (K)  
City v. Lonnie Hodges, CV-2020-2922  
The Norman Petition Initiative No. 2021-1, Case No. CV-2020-2384 (K)  
McCarver v. City of Norman, CJ-2013-128 TS (K)  
Vahe Patatanyan v. City of Norman, CP-2021-28  
Remy v. Hall, et al., Case No. CV-2017-1853 (K, S)  
Shaz Investment Group, LLC v City of Norman, CJ-2021-1044 (K)  
Walling v. Norman Regional Health System, et al, CJ-2014-874 (K)

***B. Condemnation Proceedings***

City of Norman v Chastain Oil Company, a Corporation, et al., CV-2015-677 (M)  
City of Norman v. West Lindsey Center Investors, LLC, et al., CV-2015-671 (M)  
City of Norman v. Tietsort Revocable Trust, et al., CJ-2013-775 (M)  
City of Norman v Apex Properties, LLC, et al., CJ-2021-221 (M)  
City of Norman v. The Uplands Development Co., LLC, et al., CJ-2021-227 (M)  
City of Norman v. Hallbrooke Development Group One, LLC, et al., CJ-2021-228 (M)  
City of Norman v. Ronald A. Ashley, et al., CJ-2021-337 (M)

***C. Lawsuits involving a City claim/interest in Property, Foreclosure Actions, and Applications to Vacate***

City of Norman v. Legacy Property Partners, LLC, CV-2018-249 (K, S)  
Mortgage Clearing Corporation v. Ricky Joe Butler, et al., CJ-2016-219 (M)  
Mortgage Clearing Corporation v. Doiron, et al., CJ-2014-1459 (M)

***D. Municipal Court Appeals***

***E. Small Claims Court***

Almost Home Investment, et al. v. City of Norman, SC-2014-3027 (K)

***F. Board of Adjustment Appeals***

**LABOR / ADMINISTRATIVE PROCEEDINGS**

***A. Grievance & Arbitration Proceedings (K)***

This office has assisted with the following grievances:

AFSCME Grievance FYE 21-02 – (COVID-19 Leave)



AFSCME Grievance FYE 21-05 – (Brooks & Stephens)  
AFSCME Grievance FYE 21-06 – (Parks HEOs and MWIIIs)  
AFSCME Grievance FYE 22-02 – (Jerry Younts and Bennie Gilmore – COVID-19 Leave)  
AFSCME Grievance FYE 22-02 – (Tara Klepper – COVID-19 Leave)

IAFF Grievance FYE 21 – (Carl Smith – Improper Compensation)

IAFF Grievance FYE 22 – (Battalion Chief)

This grievance has been resolved and will no longer appear on the Monthly Report.

IAFF Grievance FYE 22 – (Intellitime)

This grievance has been resolved and will no longer appear on the Monthly Report.

IAFF Grievance FYE 22 – (Chris Koscinski Termination for Disability)

This grievance has been resolved. The grievant has advised that he will retire on January 31, 2022. This grievance will no longer appear on the Monthly Report.

IAFF Grievance FYE 22 – (Jesse Baldwin – Written Reprimand)

The time for the grievant to request arbitration has expired. This grievance will no longer appear on the monthly report.

IAFF Grievance FYE 22 – (Jonathan Wilk – Written Reprimand)

The time for the grievant to request arbitration has expired and the grievant has resigned. This case will no longer appear on the Monthly Report.

**B. *Equal Employment Opportunity Commission (EEOC)***

None

**C. *Contested Unemployment Claims (OESC)***

None

**MUNICIPAL COURT PROSECUTIONS**

This chart represents the cases prosecuted by the City Attorney's Office in the Municipal Criminal Court through November 2021 (numbers were unavailable for December 2021 due to staffing issues). The chart does not represent those cases disposed of prior to Court through actions of the City Attorney and the Court.

Month	<u>ADULT CASES</u>			<u>JUVENILE CASES</u>			<u>COURT SESSIONS</u>		
	FYE 20	FYE 21	FYE 22	FYE 20	FYE 21	FYE 22	FYE 20	FYE 21	FYE 22
JULY	640	545	*275	35	23	11	15	16	7
AUG	683	444	236	10	11	9	15	14	5
SEPT	497	520	263	17	10	9	14	13	5
OCT	581	325	269	23	4	12	18	7	6
NOV	390	259	228	9	0	2	11	6	6
DEC	444	279		25	6		12	7	
JAN	522	134		32	3		15	0	
FEB	597	178		22	1		13	0	

	<u>ADULT CASES</u>			<u>JUVENILE CASES</u>			<u>COURT SESSIONS</u>		
MAR	420	270		22	6		7	5	
APR	104	420		0	6		0	13	
MAY	137	507		2	10		0	13	
JUNE	528	422		25	0		9	11	
TOTALS / YTD	5,543	4,303	1,271	222	80	43	129	105	29

\* Correction

### WORKERS' COMPENSATION COURT

The total number cases pending as of November 2021 are 17. During the month of November, there were no new workers compensation cases filed. One settlement was approved by Council on November 9, 2021. The remaining cases are proceeding in active litigation in the Oklahoma Workers' Compensation Commission. The current breakdown of pending Workers' Compensation cases by work area have been reviewed and updated for accuracy is as follows:

DEPARTMENT	DIVISION	PENDING CASES	FYE 22 CASES	FYE21 CASES	FYE20 CASES	FYE19 CASES
Fire	Suppression	3	1	2	2	4
Parks/Rec.	Park Maintenance	1				1
Parks/Rec	Westwood Pool	1	1			
Planning	Development Services					
Police	Animal Welfare	3		2	1	
Police	Patrol	7	3	1	2	1
Police	Administration					
Public Works	Street Maintenance	1		1	1	3
Public Works	Vehicle Maintenance					
Public Works	Storm Water					
Utilities	Line Maintenance	1			1	
Utilities	Sanitation					
TOTALS		17	5	6	7	9

### *List of Pending Cases*

Adams, Malia Jessie v. City of Norman, CM-2020-01069 Q

(Police, Animal Welfare, Animal Welfare Officer, R. Hip, Low Back with Radicular Symptoms, Mid-Back Consequential; + Thoracic Spine)

Adams, Malia Jessie v. City of Norman, CM-2021-02000 A

(Police, Animal Welfare, Animal Welfare Officer, R. Ankle, Mid/Low Back)

Amason, Amber v. City of Norman, WCC 2012-12306 K

(Police, Patrol, MPO, Intestinal/Parasite/Infection)

Clement, Stacey v. City of Norman, CM-2020-04580 R

(Police, Investigations/Captain, Head, Face, Neck, Back, Both Shoulders, Arms, Hands, Legs)

Crews, William "Will" Bryan v. City of Norman, CM-2021-04764 A

(Police, Patrol, Sergeant, Neck, L Shldr., L. Arm, L. Hand)

Crews, William "Will" Bryan v. City of Norman, CM-2021-04762 Q

(Police, Patrol, Sergeant, Back, Neck, L. Shldr, L. Arm, L. Hand)

Crews, William “Will” Bryan v. City of Norman, CM-2021-04763 X

(Police, Patrol, Sergeant, Back, Neck, Both Shldr., Both Arms, Both Hands)

Harris, Reagan v. City of Norman, CM-04817 K

(P&R, Westwood Pool, Life Guard, L. Wrist)

Kizzia, Derrald v. City of Norman, WCC 2014-06995 K

(Parks & Rec, Park Maintenance, HEO, R. Knee/Reopen Request)

Koscinski, Christopher v. City of Norman, CM-2020-06955 J

(Fire, Suppression, Firefighter, R. Shoulder, R. Arm)

Koscinski, Christopher v. City of Norman, CM-2021-04927 L

(Fire, Suppression, Firefighter, Back)

Landrum, Sean v. City of Norman, CM-2019-05618 L

(Utilities, Water Maintenance, Utility Distribution Worker II, Head, Headaches, Face, Jaw, Teeth, Tongue, Neck, and R. Shoulder)

Mosley, Kent v. City of Norman, CM-2020-00585 X

(Police, Patrol, Sergeant (Mid & Low Back, Radicular Pain Down L. & R. Leg, R. Big Toe, R. & L. Buttocks + Neck, Headaches + Consequential Erectile Dysfunction) + Disfigurement to Stomach

Robertson, Kellee v. City of Norman, WCC 2010-13896 F

(Police, Narcotics, MPO, Respiratory System/Lungs, Circulatory System Organs of the Body and Whole Person)

Wilkins, Levi v. City of Norman, CM-2019-05323 X

(Fire, Suppression, Fire Driver Engineer, BAW, Cancer)

Younts, Jerry Wayne v. City of Norman, CM-2020-06911 F

(Public Works/Street Maintenance/HEO, L Shoulder, Neck)

## TORT CLAIMS

The following is a breakdown of the Tort Claims activity through November 2021.

DEPARTMENT	FYE 22 Month	FYE 22 YTD	FYE 21	FYE 20	FYE 19
Animal Control	1	1	1		
Finance – IT			1		
Fire		1	1	4	
Legal			2		
Other		2	11	10	9
Parks		1	4	6	6
Planning		1			
Police	1	4	3	5	10
Public Works – other	1	2	2	3	
Public Works – Stormwater				2	
Public Works – Engineering			1	2	
Public Works – Streets		7	9	11	6
Utilities – Water		5	11	11	12
Utilities – Sanitation			12	12	10
Utilities – Sewer		1	5	5	3
<b>TOTAL CLAIMS</b>	<b>3</b>	<b>25</b>	<b>63</b>	<b>71</b>	<b>56</b>

CURRENT CLAIM STATUS	FYE 22 TO DATE	FYE 21	FYE 20	FYE 19
Claims Filed	25	63	71	56
Claims Open and Under Consideration	11	0	0	0
Claims Not Accepted Under Statute/Other	2	10	11	8
Claims Paid Administratively	3	11	13	10
Claims Paid Through Council Approval	1	7	14	12
Claims Resulting in a Lawsuit for FY	0	3	1	4
Claims Barred by Statute (No Further Action Allowed)	0	23	32	22
Claims in Denied Status (Still Subject to Lawsuit)	8	9	0	0

## MUNICIPAL COURT

8

**MUNICIPAL COURT  
MONTHLY REPORT  
DECEMBER - FY '22**

**CASES FILED**

	<b><u>DECEMBER</u></b>	<b><u>FY22</u></b>	<b><u>Y-T-D</u></b>	<b><u>DECEMBER</u></b>	<b><u>FY21</u></b>	<b><u>Y-T-D</u></b>
Traffic	280		1932	445		4411
Non-Traffic	204		1177	148		1437
SUB TOTAL	484		3109	593		5848
Parking	303		3532	315		2872
GRAND TOTAL	787		6641	908		8720

**CASES DISPOSED**

	<b><u>DECEMBER</u></b>	<b><u>FY22</u></b>	<b><u>Y-T-D</u></b>	<b><u>DECEMBER</u></b>	<b><u>FY21</u></b>	<b><u>Y-T-D</u></b>
Traffic	294		2429	610		5167
Non-Traffic	137		1020	175		1299
SUB TOTAL	431		3449	785		6466
Parking	377		4540	255		1915
GRAND TOTAL	808		7989	1040		8381

**REVENUE**

	<b><u>DECEMBER</u></b>	<b><u>FY22</u></b>	<b><u>Y-T-D</u></b>	<b><u>DECEMBER</u></b>	<b><u>FY21</u></b>	<b><u>Y-T-D</u></b>
Traffic	\$ 33,831.70		\$ 248,246.91	\$ 64,155.85		\$ 525,660.59
Non-Traffic	\$ 18,246.72		\$ 124,632.25	\$ 19,315.28		\$ 143,033.27
SUB TOTAL	\$ 52,078.42		\$ 372,879.16	\$ 83,471.13		\$ 668,693.86
Parking	\$ 12,445.00		\$ 150,667.26	\$ 7,526.00		\$ 46,889.00
GRAND TOTAL	\$ 64,523.42		\$ 523,546.42	\$ 90,997.13		\$ 715,582.86

## **MUNICIPAL COURT - MONTHLY REPORT**

### **December 2021**

#### **JUVENILE COMMUNITY SERVICE PROGRAM**

Due to a vacancy in program staff, juveniles located and worked community service projects on their own.

#### **MEDIATION PROGRAM**

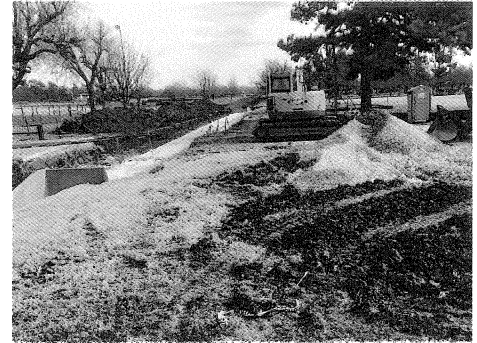
The Early Settlement – Norman Mediation Program accepted 33 new cases and closed 38 cases during the month of December 2021. No Mediations were held.

**PARKS AND RECREATION 9**



**NORMAN FORWARD Reaves Park:**

Work is continuing on the park renovation that will result in moving all of the youth baseball/softball to Reaves, so those leagues will no longer be split between Griffin and Reaves Parks. Flintco Construction (our construction manager) has been working to complete improvements to the northeast parking areas by the old adult softball fields, while they also continue working on construction of the interior park road and parking lots' asphalt and concrete. We will be working this winter to make field improvements to the 6 softball fields in the north part of the park and other upgrades to the older buildings, bleachers and support facilities. Once we are able to move our Park Maintenance shop and equipment to their new home at North Base, we will demolish the existing structures in that part of the park and build new T-Ball fields in that area, along with additional parking, restroom/concession service and all associated utilities and landscape beginning in the spring of 2022. We also met last month with the director of the Medieval Fair to help plan for that event to take place in 2022 in the park, while construction is ongoing around them on the ball fields. The project is expected to be complete in winter/spring of 2022/23.



**NORMAN FORWARD Neighborhood Parks:**

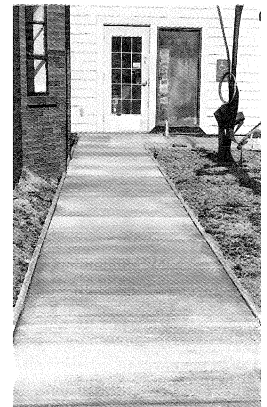
We received bids on December 30<sup>th</sup> for the playground replacement at Cherry Creek Park. This is one of the last of our parks with obsolete play equipment that needs full replacement. The new equipment will be placed in an area of synthetic turf surfacing and include a shade structure over the majority of the play area, as we have done at the other playground replacement projects in Norman Forward. We will prepare a contract with the successful bidder for City Council; and expect the work to occur in the spring. We are also awaiting delivery of the new playground equipment for Vineyard Park, which was awarded and ordered earlier this year. That project will replace the playground that was destroyed by vandalism/fire early this year. Other projects are planned to improve the landscape and furnishings at Earl Sneed Park and construction of more park picnic shelters and basketball courts in various parks around town.

**Lions Park:**

We are preparing a bid package for renovation of the restroom building and sidewalks in Lions Park to address accessibility issues at that popular central Norman park. We are working with the City's ADA Coordinator to fund the renovation work as part of the program designed to address issues with public land and buildings to bring them into compliance with ADA standards. Evaluations were done by a consulting design group; and the building improvements were designed by The McKinney Partnership, Architects earlier this year. We will improve the restroom access and functionality, along with adding an accessible ramp up to the park gazebo and make improvements to several sections of the park sidewalks. Work will be done this winter, while the restrooms are shut down and the park experiences much lower daily use.

**Cultural Facilities:**

Crews worked to replace the walkways on the south side of the Firehouse Art Center. The old "Trex" plastic lumber decking walkway had become unsafe due to wood rot in the sub-frame over the past year. That system was completely removed and replaced with a concrete walkway approach to the sales gallery entry. We are also having the entry doors on the south and north side of the gallery replaced after they were found to be rusting and hard to lock after decades of maintenance repairs. We also have completed all renovations of the sweet shop at the Sooner Theatre ahead of their run of Irving Berlin's "White Christmas" musical. The show ran December 10-19.



DECEMBER 2021  
RECREATION DIVISION  
MONTHLY REPORT

Item 4.

**Little Axe Community Center:** The Head Start program currently has 11 children enrolled and the after school program has six students enrolled. Meals on Wheels is available citizens in the Little Axe Community and Norman Regional Hospital prepares the meals daily. End of year rental revenue was \$1,980 and year end after school revenue was \$1,683.00.

**12th Avenue Recreation Center: Child Care:** The 12th Avenue Recreation Center's After School Program ran three weeks in December, averaging 27 students per day. Students got to complete Thanksgiving-themed holiday crafts and participate in the after-schoolroom festive decorating. Students got winter break holiday bags that included candy and toys. The center's adult basketball league concluded the Fall league with the Oklahoma Motorcars winning the championship game 88 to 86 against the Supreme team. The Zoom with Santa Program allowed residents to visit Santa over a Zoom video call safely. Over 250 children got to talk to Santa and get their pictures taken. Parks and Recreation Staff gathered at 12th Avenue for the Department Winter Holiday Party for food and celebration of a great year working for the City of Norman Parks and Recreation Department. A local church began renting the facility on Sundays to play basketball.

**Irving Recreation Center:** Irving Recreation Center had 3 facility rentals this month and hosted a Winter Break Camp for youth ages 5-11 on December 20<sup>th</sup>-23<sup>rd</sup>, 27<sup>th</sup>-31<sup>st</sup> and January 3<sup>rd</sup>. Overall, there were 40 total children signed up for camp. Each day campers participated in games, crafts, and sports & recreational activities. Campers also participated in other fun and educational activities such as: art instruction with Heart Studios, STEM activities with Bricks 4 Kidz (Lego Robotics), a science show from Mad Science, learning about water conservation with the Department of Environmental Quality and enrichment activities with the Pioneer Library System.

**Whittier Recreation Center:**

The 2022 winter league remained open for registration this month online and in person. The league will begin at the end of January. Whittier Middle School basketball games held three games this month at the Center. The schools booster club is running the concession this season for the first time. The staff is present during the games to assist with any issues in the building. The after school program continued this month and participants were offered homework time, gym games, STEAM learning activities, board games, arts & crafts, movies, free meals/snacks and much more! The City of Norman Parks & Recreation Department has officially become a licensed DHS Community Hope Center and are now accepting DHS subsidy payments. The program is currently full with 30 students and there is a current waiting list. The clogging class was held on Tuesdays and Thursdays this month along with Tippi Toes who offered dance classes on Wednesday nights.

FACILITY ATTENDANCE:	Month	Year to Date
Little Axe Community Center	432	1,961
12th Avenue Recreation Center	1,512	8,072
Irving Recreation Center	1,243	8,390
Whittier Recreation Center	2,335	8,325
Reaves Center	300	1,800
Tennis Center	2,129	18,705

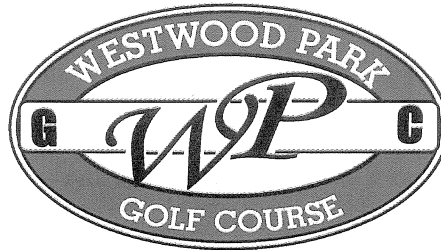
	July	Aug	Sept	Oct	Nov	Dec	Jan	Feb	Mar	Apr	May	June	
Senior	925	873	785	759	713								4,055
Axe	75	69	95	768	622	432							1,961
twelveth	1,024	868	1,067	1,512	2,089	1,512							8,072
Irving	628	634	851	2,727	2,307	1,243							8,390
Whittier	500	255	1,320	1,730	2,185	2,335							8,325
Reaves	300	300	300	300	300	300							1,800
Tennis	3,844	3,038	3,570	3,280	2,844	2,129							18705

**DECEMBER 2021  
PARK MAINTENANCE DIVISION**

Park Maintenance crews performed routine trash removal and leaf clean-up in City Parks as well as restroom/shelter cleaning & landscape maintenance in City Parks. Crews finished with the installation of the holiday decorations along Main Street and the Cleveland County Historical House.

<b>SAFETY REPORT</b>	<b>FYE-22MTD</b>	<b>FYE-22YTD</b>		<b>FYE-21MTD</b>	<b>FYE-21YTD</b>
On-The-Job Injuries	0	0		1	4
Vehicle Accidents	0	0		0	0
Employee responsible	0	0		0	0
<b>ROUTINE ACTIVITIES</b>	<b>Total Man Hours</b>	<b>Hours YEAR-TO-DATE</b>		<b>Total Man</b>	<b>Hours YEAR-TO-DATE</b>
Mowing	0.00	460.00		0.00	358.00
Trim Mowing	0.00	2763.50		59.00	2891.50
Chemical Spraying	82.00	290.00		30.00	223.00
Fertilization	0.00	16.00		0.00	12.00
Tree Planting	0.00	0.00		0.00	1.00
Tree & Stump Removal	0.00	51.00		0.00	36.00
Tree Trimming/Limb Pick-Up	12.00	352.00		847.00	2408.00
Restroom/Trash Maintenance	0.00	40.00		16.00	779.50
Play Equipment Maintenance	8.00	261.00		0.00	114.00
Sprinkler Maintenance	58.00	415.00		0.00	129.00
Watering	0.00	0.00		0.00	0.00
Grounds/Building Maintenance	0.00	0.00		0.00	189.00
Painting	0.00	0.00		0.00	0.00
Planning Design	102.00	571.00		41.50	77.50
Park Development	0.00	104.00		0.00	0.00
Special Projects	0.00	3.00		0.00	268.50
Nursery Maintenance	0.00	0.00		0.00	0.00
Flower/Shrub Bed Maintenance	3.00	397.25		94.25	587.25
Seeding/Sodding	0.00	59.27		5.25	35.25
Ballfield Maintenance/Marking	0.00	0.00		50.00	50.00
Fence Repairs	28.00	28.00		80.00	358.00
Equipment Repairs/Maintenance	0.00	138.00		56.00	832.75
Material Pick-Up	0.00	0.00		0.00	42.50
Miscellaneous	0.00	0.00		0.00	461.00
Shop Time	2.00	278.00		0.00	111.00
Snow/Ice Removal	40.50	594.00		42.50	165.50
Christmas Lights	0.00	278.00		0.00	0.00
Close to Home Fishing	0.00	0.00		0.00	0.00
Forestry	32.00	554.00		16.00	70.00
Graffiti Clean-Up	0.00	552.75		72.50	147.50
Water Fountains	6.00	156.00		32.25	42.25
Inground Trash	0.00	0.00		0.00	0.00
Vector Control	0.00	150.00		0.00	144.00

## **WESTWOOD/NORMAN MUNICIPAL AUTHORITY 9A**



## DECEMBER 2021

### Westwood Golf Course Division Monthly Progress Report

ACTIVITY	DEC FYE'22	DEC FYE'21
Regular Green Fees	406	363
Senior Green Fees	359	215
Junior Fees	52	37
School Fees ( high school golf team players)	0	0
Advanced Fees (high school golf team pre-pay)	0	0
Annual Fees (Regular, Senior & Junior Members)	634	285
Employee Comp Rounds	301	128
Golf Passport Rounds	0	0
9-Hole Green Fee	68	52
2:00 Fees	3	1
4:00 Fees	201	141
Dusk Fees or 6:00 Fees	95	54
PGA Comp Rounds	2	1
*Rainchecks (not counted in total round count)	12	3
Misc Promo Fees (birthday, players cards, OU student)	410	291
Green Fee Adjustments (fee difference on rainchecks)	6	5
<b>Total Rounds</b> (*not included in total round count)	<b>2537</b>	<b>1573</b>
% change from FY '20	61.28%	
<b>Range Tokens</b>	<b>1628</b>	<b>1298</b>
% change from FY '20	25.42%	
18 - Hole Carts	92	87
9 - Hole Carts	43	17
½ / 18 - Hole Carts	837	559
½ / 9 - Hole Carts	188	109
<b>Total Carts</b>	<b>1160</b>	<b>772</b>
% change from FY '20	50.26%	
18 - Hole Trail Fees	1	0
9 - Hole Trail Fees	0	0
18 - Hole Senior Trail Fees	2	2
9-Hole Senior Trail Fees	0	0
<b>Total Trail Fees</b>	<b>3</b>	<b>2</b>
% change from FY '20	50.00%	
<b>TOTAL REVENUE</b>	<b>\$75,575.84</b>	<b>\$60,005.66</b>
% change from FY '20	25.95%	

**DECEMBER 2021  
WESTWOOD GOLF DIVISION  
MONTHLY PROGRESS REPORT**

<b>SAFETY REPORT</b>	<b>FY 2022 MTD</b>	<b>FY 2022 YTD</b>	<b>FY 2021 MTD MTD</b>	<b>FY 2021 YTD YTD</b>
Injuries On The Job	0	0	1	2
City Vehicles Damaged	0	0	0	0
Vehicle Accidents Reviewed	0	0	0	0

**FINANCIAL INFORMATION**

	<b>FYE 2022</b>	<b>FYE 2022</b>	<b>FY 2021</b>	<b>FYE 2022</b>
	<b>MTD</b>	<b>YTD</b>	<b>MTD</b>	<b>YTD</b>
Green Fees	\$35,042.86	\$307,000.41	\$28,846.88	\$320,278.75
Driving Range	\$6,580.72	\$68,679.77	\$5,311.57	\$84,404.54
Cart Rental	\$20,040.27	\$174,419.74	\$14,667.71	\$184,860.07
Restaurant	\$13,736.58	\$105,767.41	\$11,025.82	\$84,332.73
Insufficient Check Charge	\$0.00	\$0.00	\$0.00	\$158.91
Interest Earnings	\$175.41	\$929.42	\$153.68	\$931.52
<b>TOTAL INCOME</b>	<b>\$75,575.84</b>	<b>\$656,796.75</b>	<b>\$60,005.66</b>	<b>\$674,966.52</b>
Expenditures	\$115,745.67	\$721,445.60	\$76,271.42	\$520,690.11
Income vs Expenditures	<b>(\$40,169.83)</b>	<b>(\$64,648.85)</b>	<b>(\$16,265.76)</b>	<b>\$154,276.41</b>
Rounds of Golf	2537	20348	1573	21520

All of the turfgrass on the golf course is dormant. Off season maintenance to the golf course is now projects. We sealed and winterized the irrigation pump station with insulation and weather stripping. The main pumps manifold is failing and will need repaired or replacement. This will be determined by dismantling and inspection. The shed we recovered from the old detention center is now located at our maintenance area and will be fitted with electric service so that a space heater will allow us to store all the turfgrass chemicals in it year round. The #3 drainage pipe project has been extended to out of play areas and the catch basin area of this drain is being graded for optimum drainage. This required the demolition and removal of the old clock-house building. We have started the winter maintenance of mowing equipment and golf course accessories.

DECEMBER 2021  
WESTWOOD POOL  
MONTHLY REPORT

Item 4.

FINANCIAL INFORMATION

	<b>FY2022 MTD</b>	<b>FY2022 YTD</b>	<b>FY2021 TOTAL</b>
Swim Pool Passes	\$0.00	\$1,810.00	\$191,747.00
Swim Pool Gate Admission	\$0.00	\$191,042.00	\$284,993.00
Swim Lesson Fees	\$105.00	\$3,511.00	\$33,547.50
Pool Rental	\$0.00	\$20,906.00	\$28,128.00
Locker Fees	\$0.00	\$0.00	\$0.00
Pool Classes	\$0.00	\$910.00	\$6,495.22
Pool Merchandise Sales	\$0.00	\$0.00	\$0.00
Concessions	\$0.00	\$106,835.76	\$127,066.89
<b>TOTAL INCOME</b>	<b>\$105.00</b>	<b>\$325,014.76</b>	<b>\$671,977.61</b>
Expenditures	\$12,281.88	\$384,681.94	\$624,044.12
<b>Income verses Expenditures</b>	<b>(\$12,176.88)</b>	<b>(\$59,667.18)</b>	<b>\$84,856.65</b>

ATTENDANCE INFORMATION

	<b>Season to Date Dec FYE 2022</b>	<b>2021 YTD May 2020 - Oct 2020</b>	<b>2020 YTD May 2019-Oct 2019</b>
a. Pool Attendance	0	43,187	68,202
b. Adult Lap Swim Morning/Night	0	581	282
c. Water Walkers	0	2,990	1,607
d. Toddler Time	0	2,723	2,314
e. Water Fitness	0		
f. Swim Team	0	1,221	3,167
g. Scuba Rentals	0		
h. Scuba Participants	0		
i. Swim Lessons	0	579	1,214
j. Private Swim Lessons	0		
g. Movie Night/Special Events	0	0	3,391
h. Party / Rentals	0	91	323
<b>TOTAL ATTENDANCE</b>	<b>0</b>	<b>51,372</b>	<b>80,500</b>



**FACILITY MAINTENANCE 9B**

**City of Norman Facility Maintenance  
December 2021 Monthly Hourly Materials Cost Report**

Item 4.

	Location	Labor Hrs	Labor Cost	Materials Cost	Total
<b>Misc</b>					
	Facility Maint	0.00	\$0.00	\$4.00	\$4.00
	Bldg A	0.00	\$0.00	\$550.00	\$550.00
	Bldg B	0.00	\$0.00	\$515.50	\$515.50
<b>Total</b>		<b>0.00</b>	<b>\$0.00</b>	<b>\$1,069.50</b>	<b>\$1,069.50</b>
<b>Electrical</b>					
	Facility Maint	11.50	\$382.86	\$49.61	\$432.47
	City Hall	147.00	\$4,762.20	\$4,254.84	\$9,017.08
	Bldg A	10.00	\$322.40	\$0.00	\$322.40
	Bldg B	5.00	\$165.30	\$0.00	\$165.30
	Bldg C	4.00	\$129.78	\$0.00	\$129.78
	Library	6.00	\$196.72	\$0.00	\$196.72
	Pistol Range	1.00	\$31.42	\$0.00	\$31.42
	NIC	1.00	\$31.42	\$0.00	\$31.42
	Fire Stations	15.00	\$487.70	\$0.00	\$487.70
	Parks	29.75	\$964.98	\$213.56	\$1,178.54
	Rec Centers	4.00	\$133.88	\$0.00	\$133.88
	Senior Center	9.00	\$299.18	\$0.00	\$299.18
	FHA	2.00	\$66.94	\$0.00	\$66.94
	WW Tennis	2.00	\$62.84	\$0.00	\$62.84
	Sanitation	6.00	\$200.82	\$0.00	\$200.82
	Fleet	11.00	\$345.62	\$0.00	\$345.62
	WRF	25.00	\$785.50	\$75.26	\$860.76
<b>Total</b>		<b>289.25</b>	<b>\$9,369.60</b>	<b>\$4,593.27</b>	<b>\$13,962.87</b>

**City of Norman Facility Maintenance  
December 2021 Monthly Hourly Materials Cost Report**

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**HVAC**

Facility Maint	2.00	\$60.40	\$0.00	\$60.40
City Hall	43.00	\$1,386.10	\$0.00	\$1,386.10
Bldg A	15.00	\$480.50	\$753.82	\$1,234.32
Bldg B	34.00	\$821.80	\$0.00	\$821.80
Bldg C	11.00	\$339.70	\$0.00	\$339.70
Library	6.00	\$181.20	\$0.00	\$181.20
Animal Welfare	4.00	\$120.80	\$25,341.00	\$25,461.80
Shooting Range	10.00	\$322.00	\$0.00	\$322.00
NIC	20.00	\$644.00	\$0.00	\$644.00
Fire Admin	2.00	\$60.40	\$0.00	\$60.40
Fire Stations	21.00	\$661.70	\$0.00	\$661.70
Rec Centers	26.00	\$795.10	\$0.00	\$795.10
Senior Center	12.00	\$382.40	\$0.00	\$382.40
Sooner Theater	6.00	\$181.20	\$0.00	\$181.20
Firehouse Art	10.00	\$322.00	\$0.00	\$322.00
WW Golf	4.00	\$120.80	\$0.00	\$120.80
WW Tennis	6.00	\$181.20	\$0.00	\$181.20
Sanitation	4.00	\$120.00	\$0.00	\$120.00
Traffic	8.00	\$251.60	\$0.00	\$251.60
Stormwater	8.00	\$251.60	\$0.00	\$251.60
Streets	12.00	\$372.40	\$0.00	\$372.40
Line Maint	4.00	\$120.80	\$0.00	\$120.80
Fleet	10.00	\$302.00	\$0.00	\$302.00
WTP	12.00	\$362.40	\$0.00	\$362.40
WRF	10.00	\$302.00	\$0.00	\$302.00

<b>Total</b>	<b>300.00</b>	<b>\$9,144.90</b>	<b>\$26,094.82</b>	<b>\$35,239.72</b>
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**City of Norman Facility Maintenance  
December 2021 Monthly Hourly Materials Cost Report**

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**Plumbing**

Facility Maint	100.00	\$3,344.95	\$1,196.57	\$4,541.52
City Hall	7.00	\$226.09	\$0.00	\$226.09
Bldg A	24.00	\$760.23	\$0.00	\$760.23
Bldg B	18.00	\$581.96	\$15.66	\$597.60
Bldg C	11.00	\$345.62	\$0.00	\$345.62
Bldg D	2.00	\$66.94	\$0.00	\$66.94
Library	21.00	\$676.22	\$10.80	\$687.02
Animal Welfare	29.00	\$921.43	\$37.26	\$958.69
NIC	1.00	\$31.42	\$0.00	\$31.42
Fire Stations	16.00	\$502.72	\$213.68	\$716.40
Parks	14.00	\$454.23	\$329.10	\$783.33
Rec Centers	14.00	\$450.13	\$68.93	\$519.06
WW Golf	21.00	\$674.17	\$74.18	\$748.35
WW Tennis	2.00	\$66.94	\$0.00	\$66.94
Sanitation	17.00	\$548.49	\$334.04	\$882.53
Streets	2.00	\$62.84	\$0.00	\$62.84
Fleet	1.00	\$31.42	\$0.00	\$31.42

<b>Total</b>	<b>300.00</b>	<b>\$9,745.80</b>	<b>\$2,280.22</b>	<b>\$12,026.02</b>
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**City of Norman Facility Maintenance  
December 2021 Monthly Hourly Materials Cost Report**

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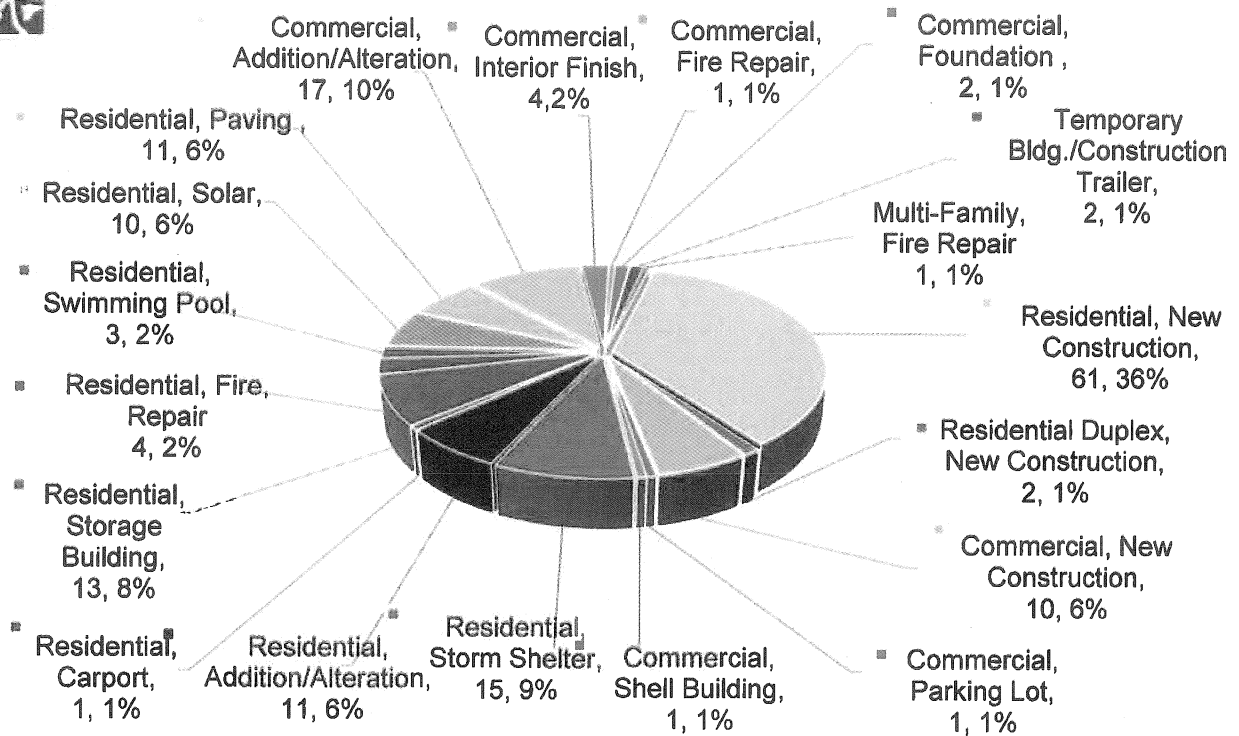
**Custodial**

<b>City Hall</b>	<b>118.00</b>	<b>\$2,278.74</b>	<b>\$22.80</b>	<b>\$2,301.54</b>
<b>Bldg A</b>	<b>118.00</b>	<b>\$2,278.74</b>	<b>\$22.80</b>	<b>\$2,301.54</b>
<b>Bldg B</b>	<b>167.00</b>	<b>\$3,027.27</b>	<b>\$22.80</b>	<b>\$3,050.07</b>
<b>Bldg C</b>	<b>118.00</b>	<b>\$2,278.74</b>	<b>\$22.80</b>	<b>\$2,301.54</b>
<b>Fire Stations</b>	<b>16.00</b>	<b>\$407.84</b>	<b>\$2,244.92</b>	<b>\$2,652.76</b>
<b>Library</b>	<b>12.00</b>	<b>\$305.88</b>	<b>\$1,441.80</b>	<b>\$1,747.68</b>
<b>Total</b>	<b>549.00</b>	<b>\$10,577.21</b>	<b>\$3,777.92</b>	<b>\$14,355.13</b>
<b>Total</b>	<b>1438.25</b>	<b>\$38,837.51</b>	<b>\$37,815.73</b>	<b>\$76,653.24</b>

**PLANNING AND COMMUNITY DEVELOPMENT 10**



**CITY OF NORMAN  
DEVELOPMENT SERVICES DIVISION PERMIT ACTIVITY  
DECEMBER 2021 REPORT**



Permit Type	Count	Valuation
Residential, New Construction	61	\$ 14,832,076
Residential Duplex, New Construction	2	\$ 400,000
Residential, New Manufactured Home	0	\$ -
Commercial, New Construction	10	\$ 3,514,948
Commercial, Parking Lot	1	\$ 90,000
Commercial, Shell Building	1	\$ 735,000
Residential, Storm Shelter	15	\$ 66,900
Residential, Addition/Alteration	11	\$ 1,411,578
Residential, Carport	1	\$ 65,000
Residential, Storage Building	13	\$ 299,949
Residential, Fire Repair	4	\$ 90,000
Residential, Swimming Pool	3	\$ 195,000
Residential, Manufactured Home Repl	0	\$ -
Residential, Solar	10	\$ 234,217
Residential, Paving	11	\$ 362,850
Commercial, Addition/Alteration	17	\$ 3,300,768
Commercial, Interior Finish	4	\$ 277,000
Commercial, Fire Repair	1	\$ 200,000
Commercial, Foundation	2	\$ 5,010,000
Temporary Bldg./Construction Trailer	2	\$ 23,000
Multi-Family, New	0	\$ -
Multi-Family, Addition/Alteration	0	\$ -
Multi-Family, Foundation	0	\$ -
Multi-Family, Fire Repair	1	\$ 20,000
Group Quarters	0	\$ -
<b>Total</b>	<b>170</b>	<b>\$ 31,128,280</b>

**CITY OF NORMAN**  
**Building Permit Activity-DECEMBER 2021**

		2021 YEAR TO-DATE	VALUATION	2020 TOTALS	2020 TOTAL VALUATION
	Residential, New Construction.....	559	\$ 171,447,259	537	\$ 155,417,525
	Residential, New Dwelling Unit Attached.....	0	\$ -	-	\$ -
	Residential, New Manufactured Home.....	2	\$ 65,000	1	\$ 97,500
	Residential, New Non Dwelling Unit.....	0	\$ -	0	\$ -
	Residential Duplex, New Construction.....	2	\$ 400,000	8	\$ 1,880,000
	Residential, Garage Apartment.....	0	\$ -	0	\$ -
	Multi-Family, New Construction 3-4 DU.....	1	\$ 750,000	0	\$ -
	Multi-Family, New Construction 5+ DU.....	9	\$ 22,230,000	25	\$ 17,432,000
	Multi-Family, Fire Repair.....	13	\$ 284,798	3	\$ 63,128
	Multi-Family, Foundation.....	3	\$ 170,000	27	\$ 924,930
	Multi-Family, Addition/Alteration.....	2	\$ 16,000	0	\$ -
	Residential, Addition/Alteration.....	185	\$ 11,219,201	159	\$ 6,979,022
	Residential, Carport.....	4	\$ 73,710	10	\$ 37,344
	Residential, Storm Shelter.....	307	\$ 1,174,221	364	\$ 1,155,682
	Residential, Storage Building.....	141	\$ 4,708,996	156	\$ 4,819,011
	Residential, Fire Repair.....	31	\$ 1,558,116	23	\$ 1,241,786
	Residential, Swimming Pool.....	145	\$ 9,332,054	118	\$ 6,436,083
	Residential, Manufactured Home Replacement	7	\$ 582,151	7	\$ 493,288
	Residential, Solar.....	16	\$ 461,303		
	Residential, Paving.....	111	\$ 1,737,557	110	\$ 1,026,455
	Group Quarters.....	0	\$ -	3	\$ 27,809,773
	<b>TOTAL</b>	<b>1538</b>	<b>\$ 226,210,366</b>	<b>1551</b>	<b>\$ 225,813,527</b>
<b>NON-RESIDENTIAL</b>	Commercial, New Construction.....	51	\$ 263,453,985	62	\$ 59,513,823
	Commercial, New Shell Building.....	11	\$ 9,242,000	11	\$ 5,141,000
	Commercial, Addition/Alteration.....	143	\$ 45,783,076	150	\$ 50,146,843
	Commercial, Interior Finish.....	40	\$ 3,374,700	41	\$ 4,025,413
	Commercial, New Foundation.....	7	\$ 10,740,000	11	\$ 2,246,353
	Commercial, Fire Repair.....	8	\$ 888,000	4	\$ 1,050,000
	Commercial, Parking Lot.....	7	\$ 682,640	8	\$ 495,452
	Commercial, Temporary Bldg./Const Trailer....	29	\$ 351,391	24	\$ 690,229
	<b>TOTAL</b>	<b>296</b>	<b>\$ 334,515,792</b>	<b>311</b>	<b>\$ 123,309,113</b>
<b>OTHER ACTIVITY</b>	Electrical Permits.....	1663		1489	
	Heat/Air/Refrigeration Permits.....	1405		1381	
	Plumbing and Gas Permits.....	1891		1775	
	Sign Permits.....	475		357	
	Water Well Permits.....	44		31	
	Garage Sale Permits.....	782		631	
	Structure Moving Permits.....	16		15	
	Demo-Residential Permits.....	31		49	
	Demo-Non-Residential Permits.....	10		2	
	Temp. Const. Bldgs. & Roll-off Permits.....	173		148	
	Lot Line Adjustments Filed.....	25		12	
	Certificate of Occupancy (CO).....	1070		1102	
	All Field Inspections.....	27583		25135	
	Net Residential Demos & Removals.....	-28		-47	
<b>TOTAL VALUATION</b>			<b>\$ 560,726,158</b>		<b>\$ 349,122,640</b>

Development Services Division | 201-A West Gray Street | Norman, OK 73069 | (405) 366-5339



City of Norman BUILDING PERMITS AND INSPECTIONS																					
NON-RESIDENTIAL BUILDING PERMITS Issued December 2021 - Sorted by Permit Type																					
Permit Type	Contractor	Tenant Name	Permit #	Issued	Street #	Dir	Street Name	Street Type	Lot	Block	Subdivision	Zoning	Valuation	Project Area							
COMMERCIAL ADD/ALT	MANHATTAN CONSTRUCTION	NPS-LINCOLN ELEMENTARY LIBRARY ADDITION	3440	12/1/2021	915		CLASSEN	BLVD		15	CLASSEN-MILLER ADDITION	R1	\$ 1,873,160	8002							
COMMERCIAL ADD/ALT	MANHATTAN CONSTRUCTION	NPS-LINCOLN ELEMENTARY INTERIOR CLASSROOM RENOVATION	3442	12/1/2021	915		CLASSEN	BLVD		15	CLASSEN-MILLER ADDITION	R1	\$ 143,608	4378							
COMMERCIAL ADD/ALT	TC GRISOM BUILDING CO.	H INDUSTRIAL-WHITE BOX #130	4867	12/22/2021	5744		HUETTNER	CT	6	1	HUETTNER ADDITION SEC 3	H	\$ 60,000	1297							
COMMERCIAL ADD/ALT	MANHATTAN CONSTRUCTION	MOORE NORMAN TECH CENTER NEW WELLNESS CENTER	4888	12/2/2021	5744		HUETTNER	CT	6	1	HUETTNER ADDITION SEC 3	H	\$ 85,000	2534							
COMMERCIAL ADD/ALT	ZERNCO, INC.	GREEN BUFFALO CANOPY ADDITION	4888	12/1/2021	4701		12TH	AVE	7	2W	NOT SUBDIVIDED	A2	\$ 500,000	5102							
COMMERCIAL ADD/ALT	WILSON, BILL	GREEN BUFFALO CANOPY ADDITION	4960	12/22/2021	3024		CLASSEN	BLVD	4A	1	EMPIRE ADDITION	C2	\$ 466,000	3999							
COMMERCIAL ADD/ALT	OWNER	EMERALD GEMINI MM GROW	4965	12/16/2021	3001		36TH	AVE	1	1	MASTERCRAFT PROPERTIES	H	\$ 75,000	709							
COMMERCIAL ADD/ALT	SABRE INDUSTRIES	T MOBILE NEW ANTENNAS	5086	12/1/2021	7006		LAGO RANCHERO	DR	16	2	LAGO RANCHERO	A2	\$ 10,000	709							
COMMERCIAL ADD/ALT	SABRE INDUSTRIES	T MOBILE REPLACE ANTENNAS	5124	12/14/2021	598		24TH	AVE	33	2W	NOT SUBDIVIDED	A2	\$ 25,000	15							
COMMERCIAL ADD/ALT	AMERICAN TOWER CORPORATION	T MOBILE REPLACE ANTENNAS	5127	12/14/2021	3099		36TH	AVE	1	1	MASTERCRAFT PROPERTIES	H	\$ 50,000	15							
COMMERCIAL ADD/ALT	AMERICAN TOWER CORPORATION	T MOBILE REPLACE ANTENNAS	5160	12/8/2021	3198 S		BERRY	RD	12	3W	NOT SUBDIVIDED	C1	\$ 35,000	15							
COMMERCIAL ADD/ALT	OWNER	OK ORGANIC HEALTH REMODEL	5161	12/8/2021	3099 E		IMHOFF	RD	3	2W	NOT SUBDIVIDED	C0	\$ 35,000	15							
COMMERCIAL ADD/ALT	OWNER	PRIME ARCHITECTS AWNING	5168	12/2/2021	2596 W		TECUMSEH	RD	1	1	SHOPS AT TECUMSEH	C2	\$ 30,000	2500							
COMMERCIAL ADD/ALT	PRECISION BUILDERS, LLC.	CLASSEN LANDING WHITE BOX #123	5254	12/7/2021	212 N		CRAWFORD	AVE	17	15	NORMAN, ORIGINAL TOWNSHIP	C3	\$ 3,000	60							
COMMERCIAL ADD/ALT	PRECISION BUILDERS, LLC.	CLASSEN LANDING WHITE BOX #121	5559	12/16/2021	1915		CLASSEN	BLVD	32	2W	CLASSEN LANDING ADDITION	C2	\$ 10,000	1538							
COMMERCIAL ADD/ALT	CHARDON BUILDERS, INC.	ST THOMAS MORE ORGAN INSTALLATION	5567	12/16/2021	1915		CLASSEN	BLVD	32	2W	CLASSEN LANDING ADDITION	C2	\$ 10,000	1381							
COMMERCIAL FIRE REPAIR	OWNER	TRAVEL LODGE FIRE REPAIR	5660	12/30/2021	100		STINSON	ST	1A	1	ST THOMAS MORE CHURCH SEC 2	R1	\$ 100,000	100							
COMMERCIAL FOUNDATION PERMIT	GE JOHNSON CONSTRUCTION	YOUNG FAMILY ATHLETIC CENTER	5425	12/8/2021	225		INTERSTATE	DR	1	1	HEDDON ADD	C2	\$ 200,000	500							
COMMERCIAL FOUNDATION PERMIT	WILSON, BILL	GREEN BUFFALO FOUNDATION	4615	12/9/2021	2201		TRAE YOUNG	DR	1	1	YFAC ADDITION	PUD	\$ 5,000,000	109601							
COMMERCIAL INTERIOR FINISH	PRECISION BUILDERS, LLC.	FIRST AMERICAN TITLE #117	5083	12/1/2021	3001		36TH	AVE	1	1	MASTERCRAFT PROPERTIES	H	\$ 10,000	2120							
COMMERCIAL INTERIOR FINISH	TC GRISOM BUILDING CO.	MACBAX LAND SURVEYING	3892	12/14/2021	2751		36TH	AVE	1	1	36TH NORTH BUSINESS PARK	PUD	\$ 100,000	3879							
COMMERCIAL INTERIOR FINISH	PRECISION BUILDERS, LLC.	STOIC STRENGTH & FITNESS	4889	12/2/2021	5744		HUETTNER	CT	6	1	HUETTNER ADDITION SEC 3	H	\$ 80,000	1780							
COMMERCIAL INTERIOR FINISH	TCS CONSTRUCTION	PURE BARRE EXERCISE	5323	12/16/2021	2751		36TH	AVE	1	1	36TH NORTH BUSINESS PARK	PUD	\$ 50,000	2434							
COMMERCIAL NEW CONSTRUCTION	COIL CONSTRUCTION	CLUB CAR WASH	5757	12/29/2021	1908		24TH	AVE	1	1	UNIVERSITY NORTH PARK 4 REPLAT	PUD	\$ 47,000	1400							
COMMERCIAL NEW CONSTRUCTION	STORAGE OKLAHOMA, LLC.	STORAGE OKLAHOMA BLDG. #18	3667	12/9/2021	3451		CLASSEN	BLVD	16	2W	CLUB CAR WASH	C2	\$ 2,000,000	5871							
COMMERCIAL NEW CONSTRUCTION	STORAGE OKLAHOMA, LLC.	STORAGE OKLAHOMA BLDG. #19	3746	12/3/2021	3451		12TH	AVE	16	2W	PARK HILL ADDITION SEC 4	PUD	\$ 109,360	3100							
COMMERCIAL NEW CONSTRUCTION	STORAGE OKLAHOMA, LLC.	STORAGE OKLAHOMA BLDG. #20	3747	12/3/2021	3451		12TH	AVE	16	2W	PARK HILL ADDITION SEC 4	PUD	\$ 193,200	7000							
COMMERCIAL NEW CONSTRUCTION	STORAGE OKLAHOMA, LLC.	STORAGE OKLAHOMA BLDG. #21	3748	12/3/2021	3451		12TH	AVE	16	2W	PARK HILL ADDITION SEC 4	PUD	\$ 281,888	5888							
COMMERCIAL NEW CONSTRUCTION	STORAGE OKLAHOMA, LLC.	STORAGE OKLAHOMA BLDG. #22	3749	12/3/2021	3451		12TH	AVE	16	2W	PARK HILL ADDITION SEC 4	PUD	\$ 48,300	1750							
COMMERCIAL NEW CONSTRUCTION	SOONER TRADITIONS, LLC.	PRESS AND PLOW RESTAURANT	3750	12/3/2021	3451		12TH	AVE	16	2W	PARK HILL ADDITION SEC 4	PUD	\$ 163,740	4750							
COMMERCIAL NEW CONSTRUCTION	LAGERE, DAVID	NEW START FARMS GREENHOUSE #1	4847	12/10/2021	800 W		LINDSEY	ST	1	1	OAK RIDGE ADD	PUD	\$ 662,910	2328							
COMMERCIAL NEW CONSTRUCTION	LAGERE, DAVID	NEW START FARMS GREENHOUSE #2	5310	12/21/2021	5501		BROWN	ST	5	1E	NOT SUBDIVIDED	A2	\$ 3,700	960							
COMMERCIAL NEW CONSTRUCTION	TYLER SIGN COMPANY	CITY OF NORMAN BUS SHELTER	5311	12/21/2021	5501		BROWN	ST	5	1E	NOT SUBDIVIDED	A2	\$ 41,850	2100							
COMMERCIAL NEW SHELL BLDG	TC GRISOM BUILDING CO.	EQUITY REALTY BLDG. #3	5331	12/7/2021	108 W		ACRES	ST	30	2W	CITY PROPERTY	C3	\$ 10,000	35							
COMMERCIAL PARKING LOT	SUN CONSTRUCTION SERVICES	ROSE ROCK VET PARKING EXPANSION	2659	12/2/2021	1910		RESEARCH PARK	BLVD	33B	3	NORMAN RESEARCH PARK	H	\$ 735,000	10500							
TEMPORARY BLDG/CONST TRAILER	CAPSULE MANUFACTURING	GO BRANDS INC. FOOD TCTL	4859	12/2/2021	400		24TH	AVE	1	2	TWENTY-FOURTH AVE IND PARK	H	\$ 90,000	9000							
TEMPORARY BLDG/CONST TRAILER	TC GRISOM BUILDING CO.	TC GRISOM CONSTRUCTION OFFICE	5139	12/8/2021	2000		RESEARCH PARK	BLVD	33B	3	NORMAN RESEARCH PARK	H	\$ 20,000	237							
			5385	12/9/2021	3321 W		TECUMSEH	RD	1	1	CARROLL FARM SEC. 4	PUD	\$ 3,000	180							
TOTAL PERMITS			38	AVERAGE VALUATION		\$346,071		AVERAGE PROJECT AREA		5,542		TOTAL PROJECT AREA		210,620							
				TOTAL VALUATION		\$13,150,716															
Permit Type	Permit Counts	Valuation	New Construction Business Information (New Construction and New Shell Building)																		
COMMERCIAL ADD/ALT	17	\$ 3,300,766	Building Size (SF)		Use/Classification		Business														
COMMERCIAL FOUNDATION PERMIT	2	\$ 5,010,000	5,671		COMMERCIAL		CLUB CAR WASH														
COMMERCIAL FIRE REPAIR	1	\$ 200,000	26,188		INDUSTRIAL		STORAGE OKLAHOMA BUILDINGS 18-21														
COMMERCIAL INTERIOR FINISH	4	\$ 277,000	2,326		RETAIL		PRESS AND PLOW RESTAURANT														
COMMERCIAL NEW CONSTRUCTION	10	\$ 3,514,948	3,060		INDUSTRIAL		NEW START FARMS GREENHOUSE #1, AND #2														
COMMERCIAL NEW SHELL BLDG	1	\$ 735,000																			
COMMERCIAL PARKING LOT	1	\$ 90,000																			
TEMPORARY BLDG/CONST TRAILER	2	\$ 23,000																			
TOTAL			38	\$	13,150,716																

City of Norman BUILDING PERMITS AND INSPECTIONS														RESIDENTIAL BUILDING PERMITS Issued December 2021 - Sorted by Permit Type			
Permit Type	Contractor	Permit #	Date Issued	Lot	Block	Subdivision	Zoning	Valuation	Project Area	RW							
1 & 2 FAMILY STORM SHELTER	GROUND ZERO STORM SHELTERS	5238	12/8/2021	609		RIVERWALK CT	3	3	SHADOWRIDGE ADD #4	R1	\$	4,095	35				
1 & 2 FAMILY STORM SHELTER	OZ SAFEROOMS	5280	12/22/2021	425		TECUMSEH RIDGE CT	35	4	BROOKHAVEN #36	R1	\$	11,025	33				
1 & 2 FAMILY STORM SHELTER	OZ SAFEROOMS	5282	12/7/2021	817		KETTLE RD	8	3	GREENLEAF TRAILS ADD #0	PUD	\$	11,025	33				
1 & 2 FAMILY STORM SHELTER	TORNADO SAFE SHELTERS	5346	12/8/2021	501 E		BROOKS ST	37	10	SOUTHDRIDGE ADD	R1	\$	3,586	46				
1 & 2 FAMILY STORM SHELTER	GROUND ZERO STORM SHELTERS	5349	12/8/2021	4305		BROWNWOOD LN	1	5	CARRINGTON PLACE ADD #3	R1	\$	3,800	32				
1 & 2 FAMILY STORM SHELTER	STORM SAFE	5438	12/8/2021	2616		TRENTON RD	5	4	NORMANDY PARK	R1	\$	3,700	35				
1 & 2 FAMILY STORM SHELTER	GROUND ZERO SHELTERS	5445	12/9/2021	4605		FLINT RIDGE DR	4	1	BROOKHAVEN #33	R1	\$	3,500	24				
1 & 2 FAMILY STORM SHELTER	GROUND ZERO SHELTERS	5589	12/20/2021	1511		SUNSET DR	7	1	EDGEEMERE 4TH ADD	R1	\$	5,600	48				
1 & 2 FAMILY STORM SHELTER	GROUND ZERO SHELTERS	5590	12/20/2021	4100		BRIARCREST DR	2	1	BROOKHAVEN #03	R1	\$	3,500	24				
1 & 2 FAMILY STORM SHELTER	PREFERRED SHELTERS	5595	12/20/2021	437		FOREMAN AVE	15	1	TOWN & COUNTRY ESTATES #2	R1	\$	3,300	48				
1 & 2 FAMILY STORM SHELTER	GROUND ZERO SHELTERS	5803	12/17/2021	3008		CRUDEN DR	11	4	CASCADE ESTATES PUD #5	PUD	\$	2,190	15				
1 & 2 FAMILY STORM SHELTER	GROUND ZERO SHELTERS	5804	12/17/2021	3114		TARA LN	14	1	HIGHLAND VILLAGE ADD SEC 2	R1	\$	2,190	15				
1 & 2 FAMILY STORM SHELTER	GROUND ZERO SHELTERS	5805	12/17/2021	2900		SOUTHAMPTON DR	24	2	ST JAMES PARK ADD 6	R1	\$	2,190	15				
1 & 2 FAMILY STORM SHELTER	TORNADO SAFE, LLC	5812	12/17/2021	3528		CRAMPTON GAP WAY	1	2	SPRINGS AT GREENLEAF TRAILS #2	PUD	\$	3,789	15				
1 & 2 FAMILY STORM SHELTER	GROUND ZERO SHELTERS	5851	12/21/2021	3603		TIMBERBROOK DR	13	12	VINTAGE CREEK ADDITION	PUD	\$	3,400	24				
1 & 2 FAMILY, ADD OR ALTER	LAW CONSTRUCTION	5910	12/6/2021	518 S		LAHOMA AVE	17	3	ROSS ADDITION	R1	\$	900,000	3066				
1 & 2 FAMILY, ADD OR ALTER	APEX INVESTMENTS, LLC	5181	12/10/2021	1901		TEXAS ST	6	8	FACULTY HEIGHTS ADD	R1	\$	30,000	1198				
1 & 2 FAMILY, ADD OR ALTER	OWNER	5220	12/7/2021	514		MILLER AVE	7	4	CLASSEN-MILLER ADDITION	R1	\$	50,000	1430				
1 & 2 FAMILY, ADD OR ALTER	J. HOWELL CONSTRUCTION, INC.	5225	12/7/2021	4004		GRANGE HILL WAY	6	2	CARRINGTON PLACE ADD #4	R1	\$	200,000	1732				
1 & 2 FAMILY, ADD OR ALTER	OWNER	5268	12/3/2021	2305		INGELS PL	7	1	HALLBROOKE ADD #6	R1	\$	55,000	360				
1 & 2 FAMILY, ADD OR ALTER	OWNER	5309	12/3/2021	801		IOWA ST	16	5	PARK ADD	R1	\$	62,000	1115				
1 & 2 FAMILY, ADD OR ALTER	OWNER	5319	12/6/2021	334		GEORGE L CROSS DR	3	2	JENNINGS ESTATES #1	R1	\$	2,500	300				
1 & 2 FAMILY, ADD OR ALTER	OWNER	5329	12/6/2021	1314		SUNSET DR	7	3	FUZZELLS ADD #2	R1	\$	30,000	2269				
1 & 2 FAMILY, ADD OR ALTER	R & A CONCRETE CONSTRUCTION	5479	12/16/2021	3908 N		FLOYD COX DR	11	1	PRIDE #130	RE	\$	37,000	460				
1 & 2 FAMILY, ADD OR ALTER	OWNER	5487	12/10/2021	1009		WHISPERING PINES DR	1	1	WHISPERING PINES ADD	R1	\$	10,676	1200				
1 & 2 FAMILY, ADD OR ALTER	OWNER	5568	12/22/2021	9109		BENNY BRUCE ST	9	5	PRIDE #331	RE	\$	15,000	200				
1 & 2 FAMILY, CARPORT	MOHR CONSTRUCTION, LLC	5568	12/21/2021	503		TULSA ST	29	3	SOUTHDRIDGE ADD	R1	\$	65,000	695				
1 & 2 FAMILY, FIRE REPAIR	WELCO ELECTRIC(A/R)	5258	12/1/2021	424		SUNRISE ST	8	6	SUNRISE HEIGHTS SEC #2	R1	\$	60,000	700				
1 & 2 FAMILY, FIRE REPAIR	OWNER	5506	12/14/2021	1318		WHIPPOORWILL DR	6	1	OAKHURST ADD	R1	\$	20,000	300				
1 & 2 FAMILY, FIRE REPAIR	OWNER	5555	12/14/2021	1400		ED NOBLE PKY			TURTLE CREEK MOBILE HOME PARK	RM4	\$	3,000	200				
1 & 2 FAMILY, FIRE REPAIR	OWNER	5663	12/22/2021	1612		MOHAWK RD	14		INDIAN HILLS ESTATES #2	RE	\$	7,000	400				
1 & 2 FAMILY, PAVING	STEPHEN KING CONSTRUCTION LLC	4198	12/14/2021	1613		CHAMBLEE DR	14	7	SHILOH HEIGHTS #3	NA	\$	308,000	248				
1 & 2 FAMILY, PAVING	CHARLEYS CONCRETE & DRAINAGE	5153	12/6/2021	2420		LOVE OAK DR	12	2	ROYAL OAKS ADD #8	R1	\$	1,450	144				
1 & 2 FAMILY, PAVING	A & L CONCRETE	5257	12/22/2021	1002		ROBINHOOD LN	12	1	SHERWOOD FOREST #1	R1	\$	4,800	416				
1 & 2 FAMILY, PAVING	GIBBS, JEFF	5284	12/7/2021	4101		48TH AVE	10	3W	NOT SUBDIVIDED	RE	\$	2,500	50				
1 & 2 FAMILY, PAVING	BARCENAS CONCRETE	5288	12/3/2021	524 E		BOYD ST	10	3	SOUTHDRIDGE ADD	R1	\$	5,200	1616				
1 & 2 FAMILY, PAVING	OWNER	5322	12/7/2021	425		CHAUTAUQUA AVE	12	1	HOWES ADD	R1	\$	9,000	455				
1 & 2 FAMILY, PAVING	HAWKINS CONCRETE	5343	12/8/2021	861		KANSAS ST	10	11	PARK ADD	R1	\$	5,000	358				
1 & 2 FAMILY, PAVING	SHEPPARD CONCRETE	5443	12/13/2021	2501		SMOKING OAK RD	6	1	SMOKING OAK #2	R1	\$	7,500	744				
1 & 2 FAMILY, PAVING	SHEPPARD CONCRETE	5489	12/13/2021	1115		LOUISE LN	6	5	NOT SUBDIVIDED	R1	\$	3,500	300				
1 & 2 FAMILY, PAVING	SHEPPARD CONCRETE	5501	12/16/2021	530 S		FLOOD AVE	5	1	WESTBROOK ADD	R1	\$	12,000	1450				
1 & 2 FAMILY, PAVING	KATY CONSTRUCTION CO	5536	12/20/2021	810		MONNETT AVE	20	4	STATE UNIVERSITY ADD	R1	\$	3,900	976				
1 & 2 FAMILY, SOLAR	SHINE SOLAR DBA SHINE AIR	4791	12/2/2021	3602		TRUMAN DR	10	2	INDEPENDENCE VALLEY SEC. #1	R1	\$	20,460					
1 & 2 FAMILY, SOLAR	DKIE SOLAR	4891	12/10/2021	509		LEOPARD LILY DR	5	2	POPPY GROVE SEC 2	R1A	\$	18,500					
1 & 2 FAMILY, SOLAR	SHINE SOLAR DBA SHINE AIR	5271	12/3/2021	1913		LAKEHURST DR	3	19	OAKHURST ADD #06	R1	\$	32,037					
1 & 2 FAMILY, SOLAR	SOLAR POWER OF OKLAHOMA	5293	12/3/2021	419 N		FINCLAY AVE	2	3	COLLEYS #2	R1	\$	4,950					
1 & 2 FAMILY, SOLAR	PRO ELECTRIC	5434	12/10/2021	3901		ABINGDON DR	1	14	BELLATONA SEC. #1	R1	\$	18,380					
1 & 2 FAMILY, SOLAR	GREEN LIGHT SOLAR	5504	12/13/2021	7202		SPRING VIEW DR	1	2	VISTA SPRINGS ESTATES ADD 2	RE	\$	25,160					
1 & 2 FAMILY, SOLAR	REX VALOUCH ELEC(A/R)	5520	12/17/2021	927		MCCALL DR	10	4	WILLOW BROOK ADD	R1	\$	35,250					
1 & 2 FAMILY, SOLAR	MASON PAGE ELECTRICAL SERVICES	5624	12/28/2021	1408		TEAKWOOD DR	7	1	WOODCREEK ADD #8	R1	\$	28,000					
1 & 2 FAMILY, SOLAR	MASON PAGE ELECTRICAL SERVICES	5625	12/28/2021	1913		VANESSA DR	15	4	CRESTLAND ESTATES #1	R1	\$	31,500					
1 & 2 FAMILY, SOLAR	PRO ELECTRIC	5649	12/28/2021	4621		OSPREY DR	12	2	EAGLE CLIFF SOUTH ADD #7	R1	\$	20,000					
1 & 2 FAMILY, STORAGE BLDG	BETTER BARNS	5154	12/1/2021	11001		RED FERN LN	11	1W	WOODLAND HILLS (SURVEY)	A2	\$	23,863	910				
1 & 2 FAMILY, STORAGE BLDG	DERKSEN PORTABLE BUILDINGS	5256	12/3/2021	2201		DONNA DR	19	5	BOYD VIEW #1	R1	\$	5,500	160				
1 & 2 FAMILY, STORAGE BLDG	OWNER	5330	12/7/2021	4201		36TH AVE	11	2W	NOT SUBDIVIDED	A2	\$	25,000	1200				
1 & 2 FAMILY, STORAGE BLDG	AAA CONSTRUCTION	5349	12/7/2021	2201		78TH AVE	20	1W	NOT SUBDIVIDED	A2	\$	52,297	1200				
1 & 2 FAMILY, STORAGE BLDG	TUFF SHED	5458	12/13/2021	1004		VILLAVARDE CIR	17	1	MONITOR RIDGE SEC. #2	PUD	\$	6,166	140				
1 & 2 FAMILY, STORAGE BLDG	OWNER	5459	12/9/2021	2798		WOODBRIAR DR	4	4	WOODCREST EST #4	R1	\$	904	160				
1 & 2 FAMILY, STORAGE BLDG	SCISSORTAIL ROOFING & CONST.	5505	12/22/2021	2601		SMOKING OAK RD	4	12	FOREST HILLS ADD	R1	\$	25,000	600				
1 & 2 FAMILY, STORAGE BLDG	HARALSON RENOVATIONS	5581	12/17/2021	4501		72ND AVE	6	3W	NOT SUBDIVIDED	A2	\$	50,000	1300				
1 & 2 FAMILY, STORAGE BLDG	A&W CONSTRUCTION	5582	12/17/2021	3050		84TH AVE	8	1W	NOT SUBDIVIDED	A2	\$	50,000	2400				
1 & 2 FAMILY, STORAGE BLDG	TUFF SHED	5593	12/20/2021	4016		WORTHINGTON DR	3	5	CASTLEROCK ADD #5	R1	\$	5,600	120				
1 & 2 FAMILY, STORAGE BLDG	TUFF SHED INC	5682	12/28/2021	2917		SUMMIT TERRACE DR	2	3	SUMMIT LAKES ADD #6	R1	\$	5,619	140				
1 & 2 FAMILY, STORAGE BLDG	OWNER	5706	12/30/2021	212 E		HUGHBERT ST	13	8	J A JONES ADDITION	NA	\$	5,000	176				
1 & 2 FAMILY, STORAGE BLDG	RICE, TRAVIS	5719	12/30/2021	2716		MEADOWBROOK DR	3	5	BROAD ACRES SEC 1	R1	\$	45,000	384				
1 & 2 FAMILY, SWIMMING POOL	S & S POOLS	5255	12/2/2021	213		THIBURY RD	1	4	THE VINEYARD PHASE II	PUD	\$	50,000	800				
1 & 2 FAMILY, SWIMMING POOL	SPARTAN POOLS & PATIO	5575	12/17/2021	1421		SKYLER WAY	65	3	CEDAR LANE SEC #1	R1	\$	70,000	928				
1 & 2 FAMILY, SWIMMING POOL	SPARTAN POOLS & PATIO	5579	12/17/2021	529		YARMOUTH RD	12	12	VINEYARD PHASE III	R1	\$	75,000	1032				

1 FAMILY, NEW CONSTRUCTION	HOME CREATIONS, INC.	186	12/22/2021	2324	NORWOOD	DR	3	6	BELLATONA SEC. #2	R1	\$	138,780	2057
1 FAMILY, NEW CONSTRUCTION	WOGGS, WILLIAM D.	3354	12/27/2021	501	SANTA FE	AVE	17	3	LARSH ADD #1	CCFB	\$	950,000	2852
1 FAMILY, NEW CONSTRUCTION	OWNER	5178	12/28/2021	201	72ND	AVE	29	1W	ALAMEDA 80 COS 1101-1	A2	\$	72,000	3200
1 FAMILY, NEW CONSTRUCTION	RED LEAF CONSTRUCTION, INC.	5179	12/21/2021	3431	ENCLAVE	PL	13	2	FOUNTAIN VIEW NORTH	PUD	\$	224,625	5739
1 FAMILY, NEW CONSTRUCTION	SMITH, MARTIN	5206	12/13/2021	5401	COTTONWOOD CREEK	DR	4	2	COTTONWOOD CREEK	R1	\$	800,000	6919
1 FAMILY, NEW CONSTRUCTION	HOMESTEAD CUSTOM HOMES & CONSR	5209	12/6/2021	1935	BURNING TREE	RD	2	4	HALLBROOKE ADD #6	R1	\$	250,000	4305
1 FAMILY, NEW CONSTRUCTION	HAMPTON HOMES, LLC	5267	12/6/2021	2505	SUMMIT CROSSING PKY	RD	2	2	SUMMIT LAKES ADD #11	R1	\$	325,260	3019
1 FAMILY, NEW CONSTRUCTION	ARMSTRONG HOMES	5278	12/10/2021	1824	BURNING TREE	RD	11	5	HALLBROOKE ADD #6	R1	\$	400,000	4434
1 FAMILY, NEW CONSTRUCTION	BYRD BUILDING	5288	12/29/2021	2700	CRITTENDEN LINK	RD	2	4	ASHTON GROVE ADD SEC 2	PUD	\$	1,200,000	6990
1 FAMILY, NEW CONSTRUCTION	HAMPTON HOMES, LLC	5308	12/6/2021	2904	SUMMIT CROSSING PKY	RD	7	4	SUMMIT LAKES ADD #11	R1	\$	326,000	3050
1 FAMILY, NEW CONSTRUCTION	GENESIS FINE HOMES, LLC.	5342	12/10/2021	3061	HUMMINGBIRD	CT	4	2	FROST CREEK ADD.	R1	\$	395,500	5029
1 FAMILY, NEW CONSTRUCTION	OWNER	5367	12/10/2021	6001	SAND HILL	CT	34	1W	NOT SUBDIVIDED	A2	\$	80,000	2300
1 FAMILY, NEW CONSTRUCTION	ARMSTRONG HOMES	5446	12/28/2021	2011	TURTLE CREEK	WAY	3	1	HALLBROOKE ADD #6	PUD	\$	270,000	2693
1 FAMILY, NEW CONSTRUCTION	ARMSTRONG HOMES	5447	12/22/2021	2012	TURTLE CREEK	WAY	3	2	HALLBROOKE ADD #6	PUD	\$	270,000	2816
1 FAMILY, NEW CONSTRUCTION	ARMSTRONG HOMES	5448	12/28/2021	2044	TURTLE CREEK	WAY	9	2	HALLBROOKE ADD #6	PUD	\$	270,000	2644
1 FAMILY, NEW CONSTRUCTION	RICHARDSON HOMES	5482	12/17/2021	6111	ROSE ROCK HILL	RD	19	1W	MILLER PINES NCOS	A2	\$	435,600	4409
1 FAMILY, NEW CONSTRUCTION	COLONY FINE HOMES, LLC.	5236	12/3/2021	1219	TORTOISE WOOD	RD	1	5	TURTLE CROSSING	PUD	\$	132,759	1753
1 FAMILY, NEW CONSTRUCTION	COLONY FINE HOMES, LLC.	5237	12/3/2021	1223	TORTOISE WOOD	RD	6	1	TURTLE CROSSING	PUD	\$	138,733	1837
1 FAMILY, NEW CONSTRUCTION	HOME CREATIONS, INC.	5359	12/13/2021	1140	OSPREY	DR	2	1	EAGLE CLIFF SOUTH ADD #7	R1	\$	163,000	2292
1 FAMILY, NEW CONSTRUCTION	COLONY FINE HOMES, LLC.	5461	12/18/2021	1203	TORTOISE WOOD	RD	1	1	TURTLE CROSSING	PUD	\$	138,733	1735
1 FAMILY, NEW CONSTRUCTION	COLONY FINE HOMES, LLC.	5462	12/18/2021	1207	TORTOISE WOOD	RD	2	1	TURTLE CROSSING	PUD	\$	132,759	1726
1 FAMILY, NEW CONSTRUCTION	COLONY FINE HOMES, LLC.	5463	12/18/2021	1211	TORTOISE WOOD	RD	3	1	TURTLE CROSSING	PUD	\$	138,254	1787
1 FAMILY, NEW CONSTRUCTION	COLONY FINE HOMES, LLC.	5464	12/18/2021	1215	TORTOISE WOOD	RD	4	1	TURTLE CROSSING	PUD	\$	138,733	1793
1 FAMILY, NEW CONSTRUCTION	IDEAL HOMES OF NORMAN	5494	12/20/2021	3916	BLACK MESA	RD	17	1	RED CANYON RANCH SEC 7	PUD	\$	275,310	3059
1 FAMILY, NEW CONSTRUCTION	IDEAL HOMES OF NORMAN	5495	12/20/2021	3048	WISTER	RD	5	2	GREENLEAF TRAILS ADD 11	PUD	\$	147,000	2280
1 FAMILY, NEW CONSTRUCTION	IDEAL HOMES OF NORMAN	5507	12/20/2021	3040	WISTER	RD	3	2	GREENLEAF TRAILS ADD 11	PUD	\$	179,010	2658
1 FAMILY, NEW CONSTRUCTION	IDEAL HOMES OF NORMAN	5508	12/20/2021	3044	WISTER	RD	4	2	GREENLEAF TRAILS ADD 11	PUD	\$	193,230	2696
1 FAMILY, NEW CONSTRUCTION	IDEAL HOMES OF NORMAN	5509	12/20/2021	3051	WISTER	RD	1	3	GREENLEAF TRAILS ADD 11	PUD	\$	167,130	2452
1 FAMILY, NEW CONSTRUCTION	IDEAL HOMES OF NORMAN	5510	12/20/2021	3106	WISTER	RD	7	2	GREENLEAF TRAILS ADD 11	PUD	\$	159,120	2411
1 FAMILY, NEW CONSTRUCTION	IDEAL HOMES OF NORMAN	5511	12/20/2021	3110	WISTER	RD	8	2	GREENLEAF TRAILS ADD 11	PUD	\$	176,760	2676
1 FAMILY, NEW CONSTRUCTION	IDEAL HOMES OF NORMAN	5512	12/20/2021	3122	WISTER	RD	10	2	GREENLEAF TRAILS ADD 11	PUD	\$	179,010	2746
1 FAMILY, NEW CONSTRUCTION	IDEAL HOMES OF NORMAN	5513	12/20/2021	3125	WISTER	RD	11	2	GREENLEAF TRAILS ADD 11	PUD	\$	150,840	2257
1 FAMILY, NEW CONSTRUCTION	IDEAL HOMES OF NORMAN	5514	12/20/2021	3130	WISTER	RD	12	2	GREENLEAF TRAILS ADD 11	PUD	\$	151,000	2241
1 FAMILY, NEW CONSTRUCTION	IDEAL HOMES OF NORMAN	5515	12/20/2021	3134	WISTER	RD	13	2	GREENLEAF TRAILS ADD 11	PUD	\$	170,000	2504
1 FAMILY, NEW CONSTRUCTION	IDEAL HOMES OF NORMAN	5524	12/20/2021	3150	WISTER	RD	17	2	GREENLEAF TRAILS ADD 11	PUD	\$	148,200	2143
1 FAMILY, NEW CONSTRUCTION	IDEAL HOMES OF NORMAN	5525	12/20/2021	3117	WISTER	RD	5	1	GREENLEAF TRAILS ADD 11	PUD	\$	178,000	2786
1 FAMILY, NEW CONSTRUCTION	IDEAL HOMES OF NORMAN	5527	12/20/2021	3138	WISTER	RD	14	2	GREENLEAF TRAILS ADD 11	PUD	\$	158,400	2439
1 FAMILY, NEW CONSTRUCTION	IDEAL HOMES OF NORMAN	5528	12/20/2021	3148	WISTER	RD	9	2	GREENLEAF TRAILS ADD 11	PUD	\$	160,400	2521
1 FAMILY, NEW CONSTRUCTION	IDEAL HOMES OF NORMAN	5529	12/27/2021	2948	BLUE FISH	RD	1	5	GREENLEAF TRAILS ADD 11	PUD	\$	158,300	2233
1 FAMILY, NEW CONSTRUCTION	IDEAL HOMES OF NORMAN	5530	12/27/2021	603	RED CEDAR	WAY	1	4	GREENLEAF TRAILS ADD 11	PUD	\$	154,700	2397
1 FAMILY, NEW CONSTRUCTION	IDEAL HOMES OF NORMAN	5559	12/29/2021	2902	GULLIVER	ST	5	3	TRAILWOODS SEC 12	PUD	\$	159,480	1772
1 FAMILY, NEW CONSTRUCTION	IDEAL HOMES OF NORMAN	5561	12/29/2021	2912	ZARA	ST	1	3	TRAILWOODS SEC 12	PUD	\$	210,330	2337
1 FAMILY, NEW CONSTRUCTION	IDEAL HOMES OF NORMAN	5565	12/29/2021	2947	WISTER	RD	14	3	GREENLEAF TRAILS ADD 11	PUD	\$	150,800	2480
1 FAMILY, NEW CONSTRUCTION	IDEAL HOMES OF NORMAN	5590	12/29/2021	2908	ZARA	ST	2	2	TRAILWOODS SEC 12	PUD	\$	198,810	2209
1 FAMILY, NEW CONSTRUCTION	IDEAL HOMES OF NORMAN	5591	12/29/2021	2900	ZARA	ST	4	2	TRAILWOODS SEC 12	PUD	\$	166,590	1851
1 FAMILY, NEW CONSTRUCTION	IDEAL HOMES OF NORMAN	5594	12/29/2021	1129	PIPER	ST	2	1	TRAILWOODS SEC 12	PUD	\$	171,270	1903
1 FAMILY, NEW CONSTRUCTION	IDEAL HOMES OF NORMAN	5585	12/29/2021	2918	GULLIVER	ST	9	3	TRAILWOODS SEC 12	PUD	\$	166,860	1854
1 FAMILY, NEW CONSTRUCTION	IDEAL HOMES OF NORMAN	5586	12/29/2021	2903	GULLIVER	ST	5	4	TRAILWOODS SEC 12	PUD	\$	182,340	2026
1 FAMILY, NEW CONSTRUCTION	IDEAL HOMES OF NORMAN	5591	12/29/2021	2919	GULLIVER	ST	1	4	TRAILWOODS SEC 12	PUD	\$	160,650	1785
1 FAMILY, NEW CONSTRUCTION	IDEAL HOMES OF NORMAN	5592	12/29/2021	1121	PIPER	ST	4	1	TRAILWOODS SEC 12	PUD	\$	175,230	1947
1 FAMILY, NEW CONSTRUCTION	IDEAL HOMES OF NORMAN	5593	12/29/2021	1109	PIPER	ST	7	1	TRAILWOODS SEC 12	PUD	\$	157,770	1753
1 FAMILY, NEW CONSTRUCTION	IDEAL HOMES OF NORMAN	5594	12/29/2021	1125	PIPER	ST	3	1	TRAILWOODS SEC 12	PUD	\$	162,270	1803
1 FAMILY, NEW CONSTRUCTION	IDEAL HOMES OF NORMAN	5595	12/29/2021	2917	ZARA	ST	1	3	TRAILWOODS SEC 12	PUD	\$	192,600	2140
1 FAMILY, NEW CONSTRUCTION	IDEAL HOMES OF NORMAN	5596	12/29/2021	1133	PIPER	ST	1	1	TRAILWOODS SEC 12	PUD	\$	187,200	2080
1 FAMILY, NEW CONSTRUCTION	IDEAL HOMES OF NORMAN	5597	12/29/2021	2905	ZARA	ST	3	4	TRAILWOODS SEC 12	PUD	\$	164,160	1824
1 FAMILY, NEW CONSTRUCTION	IDEAL HOMES OF NORMAN	5598	12/29/2021	2913	ZARA	ST	2	3	TRAILWOODS SEC 12	PUD	\$	159,350	2215
1 FAMILY, NEW CONSTRUCTION	IDEAL HOMES OF NORMAN	5599	12/29/2021	2909	ZARA	ST	3	3	TRAILWOODS SEC 12	PUD	\$	172,989	1922
1 FAMILY, NEW CONSTRUCTION	IDEAL HOMES OF NORMAN	5700	12/29/2021	2904	ZARA	ST	3	2	TRAILWOODS SEC 12	PUD	\$	166,120	2058
1 FAMILY, NEW CONSTRUCTION	IDEAL HOMES OF NORMAN	5702	12/29/2021	1117	PIPER	ST	5	1	TRAILWOODS SEC 12	PUD	\$	170,100	1890
1 FAMILY, NEW CONSTRUCTION	TUCKER HOUSE, LLC	5542	12/22/2021	1241	HONEYBEE	CT	36	2	FROST CREEK ADD.	PUD	\$	600,000	5198
1 FAMILY, NEW CONSTRUCTION	TUCKER HOUSE, LLC	5544	12/22/2021	3410	DRAGONFLY	RD	38	2	FROST CREEK ADD.	PUD	\$	600,000	4986
2 FAMILY, NEW CONSTRUCTION	MURFIELD HOMES	5210	12/21/2021	217	MCCULLOUGH	RD	19	2	STATE UNIVERSITY ADD	CCFB	\$	200,000	2226
2 FAMILY, NEW CONSTRUCTION	MURFIELD HOMES	5211	12/21/2021	215	MCCULLOUGH	RD	19	2	STATE UNIVERSITY ADD	CCFB	\$	200,000	2226
3+ FAMILY, FIRE REPAIR	REO CONSTRUCTION	5711	12/30/2021	1303	TECUMSEH	RD	12	3W	ARTISAN CROSSING ADDIT	PUD	\$	20,000	100
TEMPORARY ROLL-OFF, OTHER	PATEL, SARESHBHAI (SAM)	5428	12/8/2021	225	INTERSTATE	DR	1	1	HIBDON ADD	C2	\$	-	40
TEMPORARY ROLL-OFF, OTHER	THOMPSON, BOB	5578	12/15/2021	2179	INDUSTRIAL	BLVD	29	3	NORMAN INDUSTRIAL TRACT	I2	\$	-	0
TEMPORARY ROLL-OFF RESIDENTIAL	WARD, JEANETTE	5294	12/1/2021	1525	EISENHOWER	DR	7	4	NORTH ACRES	NA	\$	-	0
TEMPORARY ROLL-OFF RESIDENTIAL	GARDER, ROBERT L	5327	12/6/2021	4331	WILLOWPOINT	DR	16	7	WILLOWWAY ESTATES	RM2	\$	-	10
TEMPORARY ROLL-OFF RESIDENTIAL	HUSKEY, BENJAMIN	5336	12/3/2021	710	EUFALLA	ST	4	56	NORMAN, ORIGINAL TOWNSHIP	R2	\$	-	0
TEMPORARY ROLL-OFF RESIDENTIAL	INDIAN LAND	5338	12/3/2021	17974	TECUMSEH	RD	15	1E	INDIAN LAND	A2	\$	-	0
TEMPORARY ROLL-OFF RESIDENTIAL	MCALLISTER, LISA	5340	12/3/2021	7555	LETT	CR	25	1W	LETT FARMS COS	A2	\$	-	0
TEMPORARY ROLL-OFF RESIDENTIAL	WEATHERHOLT, MICHAEL AUBREY &	5500	12/13/2021	2449	SMOKING OAK	RD	1	1	SMOKING OAK #1	R1	\$	-	40
TEMPORARY ROLL-OFF RESIDENTIAL	SKINNER, CYNTHIA	5577	12/15/2021	13400	CHAPARRAL	RD	2	12	THUNDERBIRD HILLS	RE	\$	-	0
TEMPORARY ROLL-OFF RESIDENTIAL	HENSEN, CAROLYN	5609	12/17/2021	1904	SUNFLOWER	CR	5	8	PRAIRIE CREEK	R1	\$	-	0
TEMPORARY ROLL-OFF RESIDENTIAL	SAGEBIE, BRYAN K	5638	12/20/2021	3413	BRIGHT	ST	7	2	CASCADE ADDITION	R1	\$	-	0
TEMPORARY ROLL-OFF RESIDENTIAL	BARTMESS, LEVI	5708	12/29/2021	2501	WEYMOUTH	PL	5	1	WOODLAKE ESTATES #2	R1	\$	-	0
TOTAL PERMITS (EXCLUDING TEMP ROLL-OFF)		132	AVERAGE VALUATION		\$	136,194		AVERAGE PROJECT AREA		\$	1,698		
			TOTAL VALUATION		\$	17,977,570		TOTAL PROJECT AREA		\$	207,100		

Permit Type	Permit Counts	Unit Count	Valuation		Permit Type	Permit Counts
1 & 2 FAMILY, STORM SHELTER	15		\$ 68,900		RESIDENTIAL STORAGE CONTAINER	0
1 & 2 FAMILY, ADD OR ALT	11		\$ 1,411,578		TEMPORARY ROLL-OFF, RESIDENTIAL	10
1 & 2 FAMILY, CARPORT	1		\$ 65,000		TEMPORARY ROLL-OFF, OTHER	2
1 & 2 FAMILY, FIRE REPAIR	4		\$ 90,000		SEASONAL STORAGE CONTAINER	0
1 & 2 FAMILY, PAVING	11		\$ 362,850			
1 & 2 FAMILY, SOLAR	10		\$ 234,217			
1 & 2 FAMILY, STORAGE BLDG	13		\$ 298,949			
1 & 2 FAMILY, SWIMMING POOL	3		\$ 195,000			
1 FAMILY, MANUFACTURED HOME REPLACEMENT	0		\$ -			
1 FAMILY, MANUFACTURED HOME NEW	0		\$ -			
1 FAMILY, NEW CONSTRUCTION	61		\$ 14,832,076			
2-FAMILY, NEW CONSTRUCTION	2		\$ 400,000			
3-FAMILY, NEW CONSTRUCTION	0		\$ -			
3-FAMILY, FIRE REPAIR	1		\$ 20,000			
3-FAMILY, FOUNDATION	0		\$ -			
3-FAMILY, ADD/ALT	0		\$ -			
GROUP QUARTERS	0		\$ -			
GROUP QUARTERS	0		\$ -			
GROUP QUARTERS	0		\$ -			
<b>TOTAL</b>	<b>132</b>		<b>\$ 17,977,570</b>		<b>TOTAL DEMO-NET DWELLING UNITS</b>	<b>-2</b>

**POLICE 11**



# Administrative Summary

## December 2021 Summary

1/3/2022



### Operations

Part I Crimes	2021	Current	2020	2019	2021	Year-To-Date	2020	2019
		MONTH 5YR AVG				YTD 5YR AVG		
Murder	0	1	1	1	2	4	5	3
Rape	1	4	4	3	61	60	38	62
Robbery	0	6	5	3	31	56	52	46
Agg. Assault	17	15	21	13	249	184	251	183
Burglary	52	53	43	44	521	654	529	536
Larceny	205	229	243	219	2,864	2,882	2,655	2,616
Motor Vehicle Theft	31	31	45	23	517	355	457	363
Arson	0	1	0	0	6	6	7	11
Part I Totals:	306		362	306	4,251		3,994	3,820
<b>Part II Crimes</b>								
DUI/APC	30	35	20	47	299	431	358	534
Drunkenness	34	44	24	35	438	632	414	581
Drug Violations	33	64	33	67	387	1,001	507	991
Forgery	9	11	19	10	134	212	135	175
Vandalism	71	81	70	90	1,102	944	899	962
Others	358	NA	350	423	4,602	NA	4,869	4,673
Part II Totals:	535		516	672	6,962		7,182	7,916
<b>Total Reported Crime:</b>	<b>841</b>		<b>878</b>	<b>978</b>	<b>11,213</b>		<b>11,176</b>	<b>11,736</b>
<b>Other Reported Activity</b>								
Public Peace Reports	201	189	163	221	2,366	2,253	2,296	2,382
Warrants Served	81	105	97	109	994	1,474	1,126	1,711
Other Reports Totals:	282		260	330	3,360		3,422	4,093
<b>Total Case Reports:</b>	<b>1,123</b>		<b>1,138</b>	<b>1,308</b>	<b>14,573</b>		<b>14,598</b>	<b>15,829</b>
<b>Collisions</b>								
Fatality	1	0	2	0	7	7	11	4
Injury	59	63	32	50	692	703	553	667
Non-Injury	143	134	57	134	1,431	1,653	1,187	1,539
Total Collisions:	203		91	184	2,130		1,751	2,210
<b>Call for Service</b>								
CAD Activity (All Other CFS)	3,636		3,439	3,327	41,662		36,892	38,541
Calls for Service (Only Police)	6,316		5,639	7,440	76,971		82,161	101,297
Total CFS:	9,952		9,078	10,767	118,633		119,053	139,838
<b>Police Only CFS</b>								
CFS - Citizen Initiated	4,949	4,582	4,227	4,551	59,077	60,090	56,335	59,319
CFS - Officer Initiated	1,367	2,741	1,412	2,889	17,894	35,846	25,826	41,978
Total Police Only CFS:	6,316		5,639	7,440	76,971		82,161	101,297
<b>Citations &amp; Warnings:</b>								
Citations	246		425	967	5,832		9,950	15,869
Warnings	614		819	1,695	9,168		15,378	24,467
Total Citations & Warnings:	860		1,244	2,662	15,000		25,328	40,336

\*\* Public Peace reports include: Animal Bite, Found Property, Recovery/Other Agency Vehicles, Mental Case, Unattended Death and Other

\*\* Five Year Average based on 2015 to 2019

\*\* 2020 Data not used in 5YR Average due to Covid influences

## ANIMAL CONTROL 11A

# Norman Animal Welfare Monthly Statistical Report December 2021



## IN SHELTER ANIMAL COUNTS

	2020			2021			Comparisons	
	Canine	Feline	Total	Canine	Feline	Total	Difference	Percent
Beginning	31	23	54	67	47	114	60	111%
Ending	46	20	66	78	35	113	47	71%

## ANIMAL INTAKES

	2020			2021			Comparisons	
	Canine	Feline	Total	Canine	Feline	Total	Difference	Percent
Stray at Large	64	25	89	83	41	124	35	39%
Owner Relinquish	11	11	22	24	34	58	36	164%
Owner Intended Euth	2	0	2	0	0	0	(2)	-100%
Transfer In	0	13	13	0	0	0	(13)	-100%
Other Intakes*	18	3	21	6	2	8	(13)	-62%
Returned Animal	12	3	15	8	5	13	(2)	-13%
<b>TOTAL LIVE INTAKES</b>	<b>107</b>	<b>55</b>	<b>162</b>	<b>121</b>	<b>82</b>	<b>203</b>	<b>41</b>	<b>25%</b>

\*Confiscate, Protective Custody, Born in Shelter, and all other infrequent entries

## OTHER STATISTICS

	2020		2021		Comparisons	
	Total		Total		Difference	Percent
Wildlife Collected (DOA)	5	5	1	1	(4)	-80%
Dog Collected (DOA)	1	1	2	2	1	100%
Cat Collected (DOA)	0	0	0	0	0	
Wildlife Transferred	0	0	0	0	0	
Intake Horses	0	0	0	0	0	
Intake Cows	0	0	0	0	0	
Intake Goats	18	18	0	0	(18)	-100%
Intake Sheep	0	0	0	0	0	
Intake Rabbits	0	0	1	1	1	
Intake Pigs	0	0	0	0	0	
Intake Other	0	0	0	0	0	
<b>TOTAL OTHER ITEMS</b>	<b>24</b>	<b>24</b>	<b>4</b>	<b>4</b>	<b>(20)</b>	<b>-83%</b>

## LENGTH OF STAY (DAYS)

	2020	2021
Dog	12.4	19.5
Puppy	3.8	7.8
Cat	13.4	13.1
Kitten	3.5	5.6

## OWNER SURRENDER PENDING INTAKE

	Canine	Feline	Other	Total
Animals	101	31	0	132



# Norman Animal Welfare Monthly Statistical Report December 2021



## LIVE ANIMAL OUTCOMES

	2020			2021			Comparisons	
	Canine	Feline	Total	Canine	Feline	Total	Difference	Percent
Adoption	39	39	78	51	78	129	51	65%
Return To Owner	35	2	37	38	4	42	5	14%
Transferred Out	14	11	25	16	7	23	(2)	-8%
Returned in Field	0	3	3	0	3	3	0	0%
Other Outcome	0	0	0	0	0	0	0	0%
<b>TOTAL LIVE OUTCOMES</b>	<b>88</b>	<b>55</b>	<b>143</b>	<b>105</b>	<b>92</b>	<b>197</b>	<b>54</b>	<b>38%</b>

## OTHER ANIMAL OUTCOMES

	2020			2021			Comparisons	
	Canine	Feline	Total	Canine	Feline	Total	Difference	Percent
Died in Care	1	0	1	0	0	0	(1)	-100%
Lost in Care	0	0	0	0	0	0	0	0%
Shelter Euth	3	3	6	5	2	7	1	17%
Owner Intended Euth	2	0	2	0	0	0	(2)	-100%
<b>TOTAL OTHER OUTCOMES</b>	<b>6</b>	<b>3</b>	<b>9</b>	<b>5</b>	<b>2</b>	<b>7</b>	<b>(2)</b>	<b>-22%</b>

## TOTAL OUTCOMES

	2020			2021			Comparisons	
	Canine	Feline	Total	Canine	Feline	Total	Difference	Percent
Total Live Outcomes	88	55	143	105	92	197	54	38%
Total Other Outcomes	6	3	9	5	2	7	(2)	-22%
<b>TOTAL OUTCOMES</b>	<b>94</b>	<b>58</b>	<b>152</b>	<b>110</b>	<b>94</b>	<b>204</b>	<b>52</b>	<b>34%</b>

## SHELTER EUTHANASIA DATA

	Canine	Feline	Other	Total	Percentage
Medical - Sick	0	1	0	1	14%
Medical - Injured	0	1	0	1	14%
Behavior - Aggressive	5	0	0	5	71%
Behavior - Other	0	0	0	0	0%
<b>TOTAL EUTHANASIA</b>	<b>5</b>	<b>2</b>	<b>0</b>	<b>7</b>	

## MONTHLY LIVE RELEASE RATE

2020	2021
<b>95.3%</b>	<b>96.6%</b>

*Live Outcomes / (Total Outcomes - Owner Int Euth)*



# Monthly Service By Assignment

August 2021 to December 2021

Norman Animal Welfare Volunteers (ALL)

Place	Assignment	Aug 2021 Hours	Sep 2021 Hours	Oct 2021 Hours	Nov 2021 Hours	Dec 2021 Hours	Total Hours
Norman Animal Welfare Center	Community Services-NAWC	415:45	199:00	234:00	187:00	0:00	1,035:45
	NAWC-Bather / Groomer	5:46	2:00	1:40	3:58	0:00	13:24
	NAWC-Beautification Volunteer	0:00	0:00	0:00	0:00	0:00	0:00
	NAWC-Cat Socializer	22:47	28:22	30:01	23:44	28:25	133:19
	NAWC-Community Outreach Volunteer	0:00	0:00	0:00	0:00	0:00	0:00
	NAWC-Dog Handler	52:38	42:02	66:23	60:53	69:12	291:08
	NAWC-Foster Program	72:00	58:00	53:00	21:00	19:00	223:00
	NAWC-Kennel Assistant	0:00	0:00	0:00	0:00	0:00	0:00
	NAWC-Laundry	6:04	8:02	11:01	7:31	2:34	35:12
	NAWC-Lobby Greeter	0:00	0:00	0:00	0:00	0:00	0:00
	NAWC-Orientation	2:00	12:00	3:00	5:00	6:00	28:00
	NAWC-Photographer	0:00	0:00	0:00	0:00	0:50	0:50
	Other Volunteer Services	0:00	0:00	0:00	0:00	0:00	0:00
	Veterinarian Assistant Tech	0:00	0:00	0:00	0:00	0:00	0:00
	Total	577:00	349:26	399:05	309:06	126:01	1,760:38
Grand total		577:00	349:26	399:05	309:06	126:01	1,760:38

**PUBLIC WORKS                      12**

DEPARTMENT OF PUBLIC WORKS  
MONTHLY PROGRESS REPORT  
CITY OF NORMAN, OKLAHOMA  
December 2021

**ENGINEERING DIVISION**

**DEVELOPMENT**

The Development Manager processed two (2) Final Plats to the Development Committee; one (1) Rural Certificate of Survey and four (4) Preliminary Plats for Planning Commission and one (1) Final Plat to City Council. The Development Engineer reviewed 21 sets of construction plans and 4 punch lists. There were 158 permits reviewed and/or issued. Fees were collected in the amount of \$5,297.71.

**CAPITAL PROJECTS:**

**Robinson Street West of I-35 Widening Project:**

The Oklahoma Department of Transportation (ODOT) conducted a bid opening on November 19, 2020, for the Robinson Street West of I-35 Project, located from I-35 to west of Rambling Oaks Drive/Cross Roads Boulevard intersection. The low bidder was Redlands Contracting, L.L.C. of Warr Acres, Oklahoma in the amount of \$5,025,867.62. ODOT awarded this project at the December 7, 2020, Transportation Commission Meeting. Redlands started construction on Monday, April 5, 2021. This project has a 270-calendar day construction schedule. Taking into account weather days, staff estimates a March 2022 completion. ODOT is administering the construction of this project because federal transportation funds are being used.

The project involves the following items:

- Widen Robinson Street for the addition of right turn and left turn lanes
- Relocate & reconfigure Interstate Drive/Robinson Street intersection east of current location
- Intersection improvements to Crossroads Blvd./Rambling Oaks Dr./Robinson St. intersection
- New street lights, traffic signals, street signs and traffic signal interconnect along the project
- Interstate 35 on and off ramp reconstruction south of Robinson Street
- Continuous sidewalks and accessibility
- Stormwater improvements

The contractor's activities this month were as follows:

- Completed pavement removal, grading subgrade stabilization and asphalt base installation in Phase 4A, the south leg of Rambling Oaks Drive.
- Finished the reinforced concrete box installation on the south side of Robinson Street between Interstate Drive and Rambling Oaks Drive.
- ONG began addressing a utility conflict with a relocated gas line that is in conflict with the project.

**Porter Avenue and Acres Street Intersection 2019 Bond Project:**

The City of Norman conducted a bid opening on October 1, 2020, for the Porter Avenue and Acres Street Intersection 2019 Bond Project. The low bidder was Rudy Construction Co. of Oklahoma City, Oklahoma in the amount of \$2,600,996.65. The Norman City Council awarded the project at the October 27, 2020 City Council Meeting. A pre-construction meeting occurred November 12, 2020 and construction began on November 17, 2020. This project has a 270-calendar day construction schedule. Taking into account weather days, staff estimates a December 2021 completion. The City of Norman Public Works Department is administering the construction of this project with the aid of inspectors from the Norman Utilities Authority.

The project involves the following items:

- Revised geometry of the intersection to enhance traffic operations and pedestrian safety
- New traffic signals with ADA compliant ramps and crossings.
- New storm water structures meeting current City of Norman codes and ordinances
- Dedicated left turn lanes to enhance traffic operations
- Reconfigured Daws Street to accommodate intersection improvements at Porter and Acres
- Waterline utility relocations as needed to accommodate intersection improvements
- New ADA compliant pedestrian sidewalks adjacent to the roadway
- Streetscape elements including decorative sidewalks, lighting, and landscaping

The contractor's activities this month were as follows:

- *Began installation of traffic signals*
- *Finished installation of sod*
- *Awaiting delivery of pedestrian push button poles to complete signal installation*
- Roadway is currently open with temporary two-way stop condition on Acres.

*Monthly Progress Report  
Public Works (December 2021)*

**Sidewalk Programs:**

**FYE 2022 Annual Sidewalk Program.** Bids were received on June 3, 2021. City Council awarded the contract to Nash Construction Co. in the amount of \$313,109.00, on July 13, 2021. Change Order #1 increasing the Citywide contract amount of \$56,665.00 by \$45,000.00 was approved by City Council on September 28, 2021. Construction began August 10, 2021. The Schools and Arterials, Sidewalks & Trails, Sidewalk Accessibility and Downtown Sidewalk and Curbs projects are complete. To date, we've expended an estimated \$103,529.04 on 51 Citywide projects, which includes the "50/50 program," and have another 14 projects in queue at an estimated cost of \$24,529.51. Proposed Change Order #2 adding an additional \$80,000.00 to the Citywide program is projected to meet City Council on January 18, 2022. If approved, this will extend the program until the end of FYE 2022, or until funds are depleted.

Citywide Sidewalk Reconstruction				
FYE 22 Projects Completed	FYE 22 Citizen Contributions	Total Open Projects	Open Projects Estimate	Open Projects Scheduled: City Responsibility/Resident Participation
51	\$21,918.48	14	\$24,529.51	0

**FYE 2022 Capital Improvement Project – 24th Avenue NW.** Bids were received on August 5, 2021 and the project was awarded to Parathon Construction by City Council on September 14, 2021. This project addresses a significant portion of the City of Norman's 2018 Americans with Disabilities Act Transition Plan. The 24th Avenue project area is large and is broken down into four phases. This construction addresses Phase I and includes significant repairs along a 1.1-mile sidewalk corridor on 24th Avenue NW, from Main Street to Robinson Street (east side) and comprises 1365 square yards of sidewalk repairs, 800 square yards of driveway approach reconstruction and 216 square yards of ramps and curb cuts. The project began November 8, 2021 and is currently 55% complete. Change Order #1 was approved by Council on December 14, 2021, which increased the contract amount by \$120,066.00 enabling a continuation into Phase II of the project. Phase II consists of the same stretch as Phase I, but along the west side of 24th Avenue NW and is projected to begin March 1, 2022. Phase III and IV, which continues along the east and west sides of 24th Avenue SW, from Main Street to Lindsey Street will be programmed for future years.

**STREET MAINTENANCE BOND PROJECTS:**

**2022 Urban Concrete**

Bid 1

During the month of December, EMC completed work on Bid 1.

Bid 2

During the month of December, A-Tech Paving began select concrete panel replacement in the Westfield Manor Addition on Berry Road between Boyd Street and Main Street between Wards 2 and 4.

**Imhoff Bridge Emergency Repairs Project**

On Thursday, July 29, 2021, City Staff were made aware of the failure of the southeast wing wall attached to the West Imhoff Road Bridge, NBI No. 18958. Upon initial inspections performed by City Staff, the condition of the bridge was found to be severe enough to warrant immediate closure.

On August 10, 2021, Haskell Lemon Construction Co., mobilized to the West Imhoff Road Bridge to begin removing the debris from the channel that was restricting the flow of Imhoff Creek which was part of the original scope of work listed in the FYE 2022 Bridge Maintenance Program contract. On August 11, 2021, Haskell Lemon Construction Co., investigated spalling on the northwest wing wall that was also identified in the FYE 2022 Bridge Maintenance Program contract scope. During their investigations a large portion of concrete came loose exposing the joint between the wing wall and the bridge structure. The wing wall was found to have approximately 1.5" of separation from the bridge structure with no reinforcing tie-ins. This wing wall has three (3) utility lines routed through it: a sanitary sewer line, a 4" gas line, and a potable water line. The wing wall is in danger of a full-scale failure, which failure would affect these three utilities as well as the structure's stability.

*Monthly Progress Report  
Public Works (December 2021)*

On August 14, 2021, City Council declared the situation at the Bridge to be an emergency.

During the month of December, Haskell Lemon Construction Co. completed all concrete work on the South side of the bridge including both wing walls, apron, and curtain wall. Haskell Lemon also began excavation on the North side of the bridge as well as backfilling operations on the South side.

## **PUBLIC TRANSIT**

### **Public Transit Response to COVID-19 (coronavirus)**

Below are actions that have continued to be taken by City and EMBARK staff altering transit service in response to COVID-19.

- Enhanced cleaning of vehicles.
- Suspended operations of route 144-Social Security.
- Limited capacity on fixed route and paratransit busses. (increased fixed route on December 5)
- Mandatory face coverings while using transit services, a federal requirement on public transit (expiration date extended from January 18, 2022 to March 18, 2022).

### **Battery Electric Bus Purchases**

The City is currently in the process of purchasing 2 battery electric busses. A group of transit staff members visited the manufacture's facility at the beginning of November to perform a pre-production meeting. Staff anticipates receiving these vehicles in August/September 2022. Below is background information on both battery electric bus projects:

- An authorization to purchase the City's first battery electric vehicle, a transit bus, was approved at Council's May 25, 2021 meeting. A purchase order was issued on May 27, 2021. Approximately 50% of the vehicle purchase price will be reimbursed through a grant received from Oklahoma Department of Environmental Quality through the Volkswagen Settlement Fund.
- An authorization to purchase the City's second battery electric transit bus was approved at Council's August 10, 2021 meeting. A purchase order was issued on August 13, 2021 to the manufacturer. Approximately 70% of the vehicle purchase price will be reimbursed through a grant received from the FTA's 2021 Low- or No-Emission Vehicle Program. The City's project was 1 of 49 projects selected in the nation.
- Council granted approval for additional funds to be allocated to both bus builds on December 14, 2021. These additional funds were used add charge rails to the top of the busses so that in the future an overhead pantograph charging system could be utilized.

### **Go Norman Transit Plan (City of Norman Transit Long Range Strategic Plan Update)**

The Go Norman Transit Plan was approved by resolution by Council at its June 22<sup>nd</sup>, 2021 meeting. Staff are continuing exploratory work on the next steps as recommended in the plan. Tasks this month included:

- Continuing to explore the site of the City's Transit Center.
- Staff submitted an ACOG Air Quality Small Grant application to fund the construction of 80 new bus stops that align with the recommended route network of the Go Norman Transit Plan. Council supported this application by adopting a programming resolution at its November 30, 2021 meeting. Staff are awaiting notification from ACOG on its application.

### **Construction of the Transit Operations and Maintenance Facility**

The North Base Complex, Phase 1 project was awarded to Flintco, LLC in the amount of \$8,648,000 on October 13, 2020. This project includes construction of the Transit/Fire Maintenance Facility and Parks Maintenance Facility at the North Base Complex. The Notice to Proceed was issued on November 2, 2020, and a groundbreaking ceremony was held on the same day. It is estimated that the project will be completed in December 2021.

The project involves the following items:

- Construction of New Transit/Fire Maintenance Building and Parks Maintenance Building
- Utility Extension to serve the new complex
- New storm water structures meeting current City of Norman codes and ordinances
- Construction of paved parking & storage areas to serve the Operations and Maintenance Facilities
- Revisions to secured access to the North Base Facility.

*Monthly Progress Report  
Public Works (December 2021)*

The contractor's activities this month were as follows:

- Completed east entry and gate
- Completed detention pond excavation and structures
- Continued startup of building MEP
- Installed Networking equipment
- Began construction of west entry and gate on Da Vinci Street
- Plumbing and bathroom fixtures installation completed
- Counter top construction complete
- LVT flooring continued in Transit Building
- Drop ceilings complete
- Polished concrete flooring in both buildings complete
- Interior door installation is complete
- Fence construction continued
- Security and Access system installation is in progress
- Office furniture installed
- NAPA Parts room shelving installed
- TV's installed in conference rooms
- EV and CNG equipment delivered to site
- Gas Meters installed
- All tile and interior finishes complete
- Bathroom fixtures installed

**Transit Monthly Performance Report**

Attached is the transit performance report for November 2021.

**STREETS DIVISION**

**CAPITAL PROJECTS:**

**72<sup>ND</sup> AVENUE NE: ALAMEDA STREET TO ROBINSON STREET**

Streets crews worked an overlay at 72<sup>nd</sup> Avenue NE: Alameda Street to Robinson Street and required 2,980.09 tons of asphalt for the repair.

**72<sup>ND</sup> AVENUE NW: FRANKLIN ROAD TO SOUTH END**

Streets crews worked an overlay at 72<sup>nd</sup> Avenue NW: Franklin Road to South End and required 809.85 tons of asphalt for the repair.

**TOWN AND COUNTRY ESTATES-NORTH WESTCHESTER AVENUE TO NORTH SHERRY AVENUE**

Streets crews replaced damaged concrete panels on Town and Country Estates-North Westchester Avenue to North Sherry Avenue. This repair required 147.50 cubic yards of concrete and resulted in over 400 square yards repaired.

**1213 DUSTIN DRIVE (VALLEY GUTTER PROGRAM)**

Streets crews worked valley gutter repair at 1213 Dustin Drive and required 30.82 tons of asphalt for the repair.

**ASPHALT OPERATIONS:**

**2919 MEADOW AVENUE – DEEP PATCH**

Streets crews worked deep patch at 2919 Meadow Avenue and required 19.25 tons of asphalt for the repair.

**CHAUTAUQUA AVENUE AND BROOKS STREET-DEEP PATCH**

Streets crews worked deep patch at Chautauqua Ave and Brooks Street and required 47.92 tons of asphalt for the repair.

*Monthly Progress Report  
Public Works (December 2021)*

**CONCRETE OPERATIONS:**

**2919 MEADOW AVENUE**

Streets crews replaced damaged concrete panels on 2919 Meadow Avenue. This repair required 8.50 cubic yards of concrete and resulted in over 17.50 square yards repaired.

**ROUTINE POTHOLE PATCHING OPERATIONS**

This month approximately 4.01 tons of asphalt was utilized in routine pothole patching operations.

**STORMWATER**

**WORK ORDER RESPONSE**

Stormwater Division received 11 work order requests and closed 10 work orders.

**INFRASTRUCTURE MAINTENANCE**

The Channel Maintenance crew removed drifts and cleared debris from several locations along Imhoff, Bishop and Hollywood Chanel, which resulted in 17 tons of debris removed. The Channel Maintenance Crew removed debris at a flume on Glenn Oaks Drive totaling 2 tons. The Channel Maintenance crew also removed 22 tons of debris and sediment from Merkle creek north of Iowa street. The Maintenance crew reshaped the ditch line at two separate locations on the east side, one at Lindsey and Deerfield and the other at 4100 36<sup>th</sup> AVE NE, removing 3 tons of dirt to reestablish the ditch line to prevent water from freezing on road. The crew checked 30 inlets and cleaned 12 inlets totaling .5 tons of debris removed.

**CHANNEL MAINTENANCE**

The Channel Maintenance crew removed drifts and cleared debris from several locations along Colonial Estates and Hollywood Chanel, which resulted in 10 tons of debris removed. The Channel Maintenance Crew removed debris from Telstar channel totaling 140 tons. The Channel Maintenance crew cleaned the OU and HWY 9 grate crossing, removing 45 tons of debris. The Maintenance crew added 75 tons of rip rap for bank stabilization at Meadow Ave. The Maintenance crew reshaped the ditch line on the north side of Robison St. west of Firefly, removing 430 tons of dirt to reestablish the ditch line to prevent water from freezing on the road. The crew checked 579 inlets and cleaned 320 inlets, removing 3.5 tons of debris.

**URBAN STREET SWEEPING/CAMERA VAN OPERATIONS**

A total of 209 lane miles were swept in December resulting in the removal of approximately 86.74 tons of debris from various curb lined streets throughout the city. The crew also checked 250 linear feet of pipe with camera truck at the Porter reconstruct project and 905 linear feet at Woodland and Burton streets. The crew checked 106 inlets and cleaned 51 inlets totaling 1.5 tons of debris removed.

**STORMWATER OKIE LOCATES**

During the month of December, 2976 Call 811 Okie Spots were received. Of those requests, 90 were stormwater pipe locates, 48 were marked, and 346 were referred to other departments.

**CONSTRUCTION SITE STORMWATER MANAGEMENT**

Performed 53 inspections of 122 active sites.

Issued 0 citations and 0 NOV to active sites.

Issued 2 Earth Change Permits to new projects.

**MS4 OPERATIONS**

Received and responded to 9 citizen calls.

Conducted 0 outfall inspections.

Conducted 0 detention/retention pond inspections.

On December 4, Michelle Chao facilitated a clean-up event at Griffin Community Park.



*Monthly Progress Report  
Public Works (December 2021)*

On December 8, Michelle Chao and Stuart Shumate presented the EnviroScape watershed model at Eisenhower Elementary School.

On December 18, clean-up supplies were provided for a clean-up event at Lions Park.

### **FLEET DIVISION**

The Fleet Management Division Activity Report shows a comprehensive summary of the activity during the month, broken down into 3 subgroups: Fuel Report, Maintenance Report, and Productivity Report.

#### **FUEL REPORT**

Purchases: The Inventory fuel and Outside fuel purchases are added together for each category of fuel - Unleaded gasoline, Diesel fuel, and CNG.

Amount Sold: The amount of Inventory fuel and Outside fuel disbursed to city divisions are shown.

Price Per Gallon: For Inventory Purchases, each time a purchase is made the invoice information, such as quantity and total price is receipted into the Faster system. The Faster program then tallies the information and decides on a price-per-gallon for that purchase. The monthly high and the monthly low price-per-gallon for unleaded gasoline and diesel fuel are shown.

#### **MAINTENANCE REPORT**

Repair Parts Sold: This shows the amount of money spent on repair parts for vehicles during the month.

Tires Sold: This shows the amount of money spent on tires for city vehicles during the month.

Total Parts Sold: This is the sum of Repair Parts and Tires Sold added together.

Sublet Repairs: This is the amount spent on outside repairs during the month.

Road Calls: This is the amount of times Fleet was called out to retrieve/repair a vehicle.

Preventative Maintenance Services: This is the amount of times a vehicle failed to make the appointed preventative maintenance service and had to be rescheduled.

Total Work Orders: This is the amount of work orders for the entire month.

Year to Date Work Order Total: This is the amount of work orders for the entire year.

#### **PRODUCTIVITY REPORT**

Direct Labor Hours: Each mechanic's total direct labor hours are shown. Then the direct labor hours are tallied together. After that the total available hours are shown to assess productivity.

Productivity Goal: When mechanics are productive at 70%, meaning that 70% of their day was spent actually working on vehicles, the City of Norman is in equilibrium. We are able to use the money generated from their direct labor to pay wages, benefits and the utilities.

Actual Productivity: This is the average percent of all the mechanics' total productivity during the available working hours for the month.

**December 2021**  
**DEVELOPMENT COORDINATION, ENGINEERING**  
**AND PERMIT REVIEW**

**Subdivision Development:**

FYE 2022 Associated Fees

**Planning Commission/Dev Comm Review:**      This Month      Last Month      FY Total

\*Norman Rural Cert of Survey... 1  
 \*Final Plats..... 2  
 \*Preliminary Plats..... 4  
 \*Short Form Plat..... 0  
 \*Center City Form Based Code.. 0  
 \*Concurrent Constr. Request..... 0

**City Council Review:**

Certificate of Survey..... 0  
 Preliminary Plat..... 0  
 Final Plats ..... 1  
 Certificate of Plat Correction..... 0  
 Encroachment..... 0  
 Easements..... 0  
 Closure..... 0  
 Release of Deferral..... 0

\$ 1,420.00

**Development Committee:**

Final Plats..... 0

**Fee-In-Lieu of Detention..... 0**

\$0.00

**Subtotal:**

\$1,420.00

\$11,200.00

\$35,330.00

**Permits Reviewed/Issued:**

(includes Offsite Construction fees)

\*\*Single Family..... 60  
 \*\*\*Commercial..... 12  
 Multi-Family..... 0  
 Addition/Alteration..... 24  
 House Moving..... 0  
 Paving Only..... 10  
 Storage Building..... 12  
 Swimming Pool..... 5  
 Storm Shelters..... 13  
 Public Improvements..... 2  
 Temporary Encroachments..... 0  
 Fire Line Pits/Misc..... 0  
 Franchise Utilities ..... 20  
 Other revenue..... 0  
 Flood Plain (@\$100.00 each)..... 0

**Total Permits.....****Grand Total.....****\*\*\*\*Construction Plan Review Occurrences****\*\*\*\*\*Punch Lists Prepared.....**

	\$0.00	\$10.00	
	\$0.00	\$300.00	\$1,200.00
	\$3,877.71	\$9,038.37	\$24,475.30
	\$5,297.71	\$20,548.37	\$61,015.30
	21	29	139
	4	6	30

\* All Final Plat review completed within ten days..... PI # 13

\*\* All Single Family Permits were reviewed and completed within three days.....PI # 10

\*\*\* All Commercial Permits were reviewed and completed within seven days..... PI # 11

\*\*\*\* All Construction Plans were reviewed within ten days.....PI # 12

\*\*\*\*\*All Punch Lists prepared within one day of Final Inspection.....PI # 8

**December 2021**

**DEVELOPMENT COORDINATION,  
ENGINEERING, AND PERMIT REVIEW**

**KEN DANNER/TODD McLELLAN/JACK BURDETT**

	NUMBER OF INSTANCES	PERCENTAGE ACHIEVED
<i>PI #8</i> PREPARE DEVELOPMENT PUNCH LIST WITHIN 1 DAY OF FINAL INSPECTION	4	100%
<i>PI #10</i> SINGLE FAMILY BLDG PERMIT REVIEW W/I 3 DAYS	60	100%
<i>PI #11</i> COMMERCIAL BLDG PERMIT REVIEW W/I 7 DAYS	12	100%
<i>PI #12</i> CONSTRUCTION PLAN REVIEW W/I 10 DAYS	21	100%
<i>PI #13</i> FINAL PLAT REVIEW COMPLETED WITHIN 10 DAYS	2	100%

# EMBARK NORMAN

## PERFORMANCE REPORT



### Summary of Services Table: November 2021

The table below provides daily averages for the number of passengers carried by many of the services offered by EMBARK Norman. The year-to-date (YTD) figures are cumulative totals.

EMBARK Norman Service Summary	ADP Nov FY22	FY22 YTD	FY21 YTD	Service Profile	Nov FY22	Nov FY21	Oct FY22
Fixed Routes (M-F)	952	96,175	77,150	Weekdays	21	20	21
Fixed Routes (Sat)	393	8,118	4,847	Saturdays	4	4	5
PLUS (M-F)	81	8,338	6,560	Gamedays	1	1	2
-Zone 1*	65	6,593	5,228	Holidays	0	2	0
-Zone 2**	16	1,745	1,332	Weather	1	0	1
PLUS (Sat)***	19	319	208	Fiscal YTD Days	128	123	103
				Cal. YTD Days	281	251	256

\* Requires ¾ mile

\*\*Operates only on Weekdays until 7:00 pm

\*\*\*Operates only in Zone 1

### Strategic Performance Measures

MEASURE	FY 22 YTD	FY 22 Targets	
# of Norman fixed-route passenger trips provided	104,293	265,054	▲
# of Norman paratransit trips provided	8,657	19,000	●
% of on-time Norman paratransit pick-ups	98.31%	95.00%	●
# of Norman bus passengers per service hour, cumulative	13.15	13.14	●
# of Norman bus passengers per day, average	816	N/A*	N/A*
% of Norman required paratransit pick-ups denied due to capacity	0.02%	N/A*	N/A*

\*These LFR targets are unavailable for this fiscal year. We hope to have them for FY23.

# SERVICE EFFORTS AND ACCOMPLISHMENTS FYE 2022

Item 4.

STREET DIVISION					
	FYE 2022 December 2021	FYE 2022 December 2021	Year to Date	Year to Date	FYE 2022
PERFORMANCE INDICATORS	ACTUAL	PERCENT	ACTUAL	PERCENT	PROJECTED
Distribute work order requests to field personnel within one day.	99%	99%	99%	100%	100%
Patch potholes smaller than one cubic foot within 24 hours (tons of material used)	100% 4.01	100%	100% 49.49	100%	95%
Overlay/pave 10 miles per year.	1.00	10%	11.25	113%	100%
Replace 2,000 square yards of concrete pavement panels	400.00	20%	1,777.00	89%	100%
Grade all unpaved alleys two (2) times per year. (approximately 210 blocks)	14.50	3%	79.50	19%	100%
Mow 15 ROW-miles (1,584,000 sf) of Urban right-of-way, eight times per year	-	0%	12,839,148.50	101%	100%
Mow 148 miles of Rural Right-of-way three times per year	-	0%	1,064.00	240%	100%
Debris Removal – pre-positioned contractor on notice 24 hours prior to storm event	-	0%	-	0%	0%
Debris Removal - Issue Notice to Proceed/Task Order with 48 hours of storm event	-	0%	-	0%	0%
Bridge - Maintain 5 non-deficient bridges in a year	-	0%	-	0%	0%
Bridge - Rehab 7 structurally deficient bridges per year through outside contract	-	0%	-	0%	0%
Bridge - Replace one functionally obsolete bridge per year	-	0%	-	0%	0%
Bond Program - Complete all selected projects for the bond year within the same fiscal year	-	0%	-	0%	0%

## SERVICE EFFORTS AND ACCOMPLISHMENTS FYE 2022

STORMWATER DIVISION					
	FYE 2022 December, 2021	FYE 2022 December, 2021	Year to Date	Year to Date	FYE 2022
PERFORMANCE INDICATORS	ACTUAL	PER CENT	ACTUAL	PER CENT	PROJECTED
Respond to stormwater complaints and drainage concerns within 24 hours of the time reported.	99%	99%	99%	99%	99%
Mechanically sweep 500 curb miles per month (lane miles)	209.00	42%	2,758.00	46%	50%
Inspect and clean 100% of the urban drainage inlets three times per year. (approximately 5,000 locations)	134.00	1%	4,997.00	50%	70%
Mow 2,271,548 sq.feet of open drainage ways, six times per year	5,800.00	0%	3,929,531.00	29%	90%
Apply chemical vegetative control to open drainage channels, one time per year.	-	0%	-	0%	90%
Permit all earth disturbing operations over 1 acre in size.	2.00	95%	15.00		95%
Permit all floodplain activities as appropriate.	4.00	4%	13.00		100%
Submit all necessary reports and documentation as required to comply with state stormwater regulations within 15 days of deadlines. (1)	-	0%	1.00		100%
Perform erosion control inspections of permitted sites within 30 days.	53.00	43%	615.00		100%
Respond to stormwater complaints within 24 hours of the time reported	9.00	100%	110.00		100%
Inspect City facilities identified as potential stormwater pollution sources.	-	0%	-		50%
Inspect stormwater outfalls.	-	0%	5.00		100%

**PUBLIC WORKS  
FLEET DIVISION  
ACTIVITY REPORT**

Item 4.

December 2021

IN GALLONS	FYE 2022	FUEL REPORT	
	<u>UNLEADED PURCHASED</u>	<u>DIESEL PURCHASED</u>	<u>CNG PURCHASED</u>
Internal pumps	19,215.00	17,513.00	19,124.95
Outside - sublet	1,619.00	963.00	4,449.23
<b>TOTAL</b>	<b>20,834.00</b>	<b>18,476.00</b>	<b>23,574.18</b>
	<u>UNLEADED CONSUMED</u>	<u>DIESEL CONSUMED</u>	<u>CITY CNG CONSUMED</u>
<b>TOTAL Consumption</b>	<b>18,178.85</b>	<b>17,065.28</b>	<b>4,449.23</b>

FYE 2022 TO DATE CONSUMPTION				
<b>TOTAL Consumption</b>	<u>UNLEADED CONSUMED</u>	<u>DIESEL CONSUMED</u>	<u>CITY CNG CONSUMED</u>	<u>PUBLIC CNG CONSUMED</u>
	117,528.80	127,008.50	201,166.26	25,851.25

INTERNAL PRICE PER GALLON:					EXTERNAL PRICE PER GALLON:				
UNLEADED	High	\$2.35	Low	\$1.96	UNLEADED	High	\$2.35	Low	\$1.96
DIESEL	High	\$2.44	Low	\$2.06	DIESEL	High	\$2.44	Low	\$2.06
CNG	High	\$0.84	Low	\$0.84	CNG	High	\$1.49	Low	\$1.49

FASTER CONSUMABLE PARTS PURCHASED					PUBLIC CNG SALES	
REPAIR PARTS	\$84,646.95				Month Total Public CNG Sales	\$6,315
BATTERIES	\$2,714.70				FYE 2022 To Date Public Sales	\$38,210
OILS/FLUIDS	\$5,510.64				LIFE TO DATE CNG GAS GALLON EQUIVALENT	
TIRES	\$29,048.60				Total Sold Gallons Life To Date	989,487
SUBLET REPAIRS	\$43,445.16				Total Gross Sales Life To Date	\$1,419,816
<b>TOTAL SPENT ALL parts/sublet</b>	<b>\$165,366.05</b>				Life To Date CNG Gas Gallon Equivalent	
					Total Public/City Through-Put CNG Gallons @ Statio	2,706,177

Light Shop	CURRENT MONTH	LAST MONTH	Two Months Ago	YEAR TO DATE
ROAD SERVICE	1	1	4	39
EMERGENCY ROAD CALLS	3	1	5	42
PM SERVICES	93	85	96	690
INCLEMENT WEATHER	2	4	13	20
WORK ORDERS	231	237	300	1,883
SCHEDULED REPAIRS	107	114	118	776
NON SCHEDULED REPAIRS	69	73	77	522

Heavy Shop	CURRENT MONTH	LAST MONTH	Two Months Ago	YEAR TO DATE
ROAD SERVICE	3	4	12	51
EMERGENCY ROAD CALLS	23	14	15	145
PM SERVICES	41	39	59	313
INCLEMENT WEATHER	0	0	0	1
WORK ORDERS	203	208	256	1,754
SCHEDULED REPAIRS	57	58	89	432
NON SCHEDULED REPAIRS	121	120	135	1,003

Transit Shop	CURRENT MONTH	LAST MONTH	Two Months Ago	YEAR TO DATE
ROAD SERVICE	1	2	1	20
EMERGENCY ROAD CALLS	0	0	0	1
PM SERVICES	12	17	10	78
INCLEMENT WEATHER	1	0	2	2
WORK ORDERS	78	73	80	508
SCHEDULED REPAIRS	12	17	10	67
NON SCHEDULED REPAIRS	60	54	69	389

COMBINED SHOPS	CURRENT MONTH	LAST MONTH	TWO MONTHS AGO	YEAR TO DATE
ROAD SERVICE	5	10	17	101
EMERGENCY ROAD CALLS	26	16	20	178
PM SERVICES	146	143	166	1074
INCLEMENT WEATHER	3	5	15	24
WORK ORDERS	512	544	653	4236
SCHEDULED REPAIRS	176	196	219	1289
NON SCHEDULED REPAIRS	250	259	285	1946

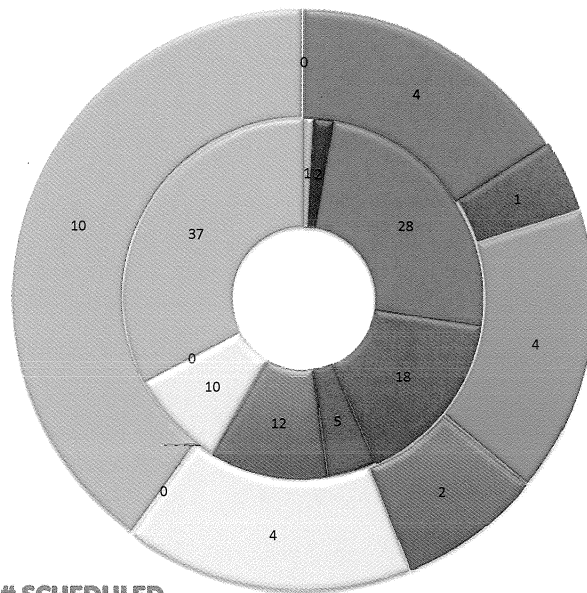
**PUBLIC WORKS FLEET DIVISION**  
**PM COMPLIANCE REPORT**  
 December FYE 2022

Item 4.

Currently Past Due:

Unit #	Unit Description	Department Division	Current Odometer Reading	Meter or scheduled date	Meter Past		ORIGINAL Scheduled DATE	SHOP	Type of SERVICE	LAST PM DONE
<b>POLICE</b>										
LED1	2000 Ford Crown Vic	PD Patrol	1/1/2022	11/9/2021	-53	Days	11/23/2021	Light Repair	PM-C	11/9/2020
1186	2013 Ford Interceptor	PD Patrol	163101	159921	-3180	miles	11/8/2021	Light Repair	PM-C	4/26/2021
<b>PSST</b>										
1220	2018 Ford Interceptor	PSST Patrol	60061	11/16/2063	-205	Days	12/16/2021	Light Repair	PM-D,G, & T	
LED2	2010 Ford Crown Vic	PSST Patrol	1/1/2022	11/17/2021	-45	Days	11/23/2021	Light Repair	PM-C	9/6/2019
<b>UTILITIES</b>										
0337	2012 Internatnl 7600 Dumptruck	Waterline Maintenance	6209	6207	-2	Hours	12/17/2021	Heavy Repair	PM-C	6/11/2021
0319	2018 Ford F250	Waterline Maintenance	28281	28000	-281	Miles	12/17/2021	Light Repair	PM-D & G	3/29/2021
307H	2015 Indeco	Waterline Maintenance	1/1/2022	3/2/2021	-305	days	7/9/2021	Heavy Repair	PM-C	3/2/2021
309A	2020 Champion Compressor	Waterline Maintenance	1/1/2022	6/25/2021	-190	days	8/2/2021	Heavy Repair	PM-C	N/A
333A	2015 Vanair Air Compressor	Waterline Maintenance	1/1/2022	11/18/2021	-44	Days	11/30/2021	Light Repair	PM-C	5/18/2021
<b>SANITATION</b>										
0209	2019 Ford F150	Commercial	13254	11262	-1992	Miles	11/1/2021	Light Repair	PM-C & N	9/23/2020
0261	2010 Peterbilt Sideload	Residential	19926	19455	-471	hours	10/13/2021	Heavy Repair	PM-C & S	1/20/2021
281T	2019 Holt Trailer	Commercial	1/1/2021	1/28/2021	27	hours	3/19/2021	Heavy Repair	PM-A	1/27/2020
0208	2019 Ford F150	Residential	17265	15593	-1672	Miles	7/22/2021	Light Repair	PM-C	6/10/2021
0256	2014 Peterbilt Rearloader	Yard Waste	227	221	-6	Hours	12/28/2021	Heavy Repair	PM-C	6/29/2021
<b>PARK MAINTENANCE</b>										
5408	2001 Dodge 2500 P/U	Park Maintenance	109533	108217	-1316	Miles	10/20/2021	Light Shop	PM-C	1/2/2019
0437	2015 Pheonix 1800HD	Park Maintenance	1/1/2022	6/24/2021	-191	days	5/7/2021	Light Repair	PM-B	6/24/2020
0428	2007 Phoenix	Park Maintenance	1/1/2022	6/19/2021	-196	days	7/23/2021	Light Repair	PM-C	6/19/2020
<b>PUBLIC WORKS</b>										
680B	2012 John Deere Brushhog	Streets	1/1/2022	6/8/2021	-207	days	8/13/2021	Heavy Repair	PM-B	6/8/2020

**PM Compliance Report**  
**December FYE 2022**



**INNER RING - MONTHLY # SCHEDULED**  
**OUTER RING = MONTHLY # MISSED/LATE**

Department	Scheduled	Missed/Late	% Late
Finance	1	0	0.0%
Planning	2	0	0.0%
Public Works	28	4	14.3%
Police	18	1	5.6%
Fire	5	4	80.0%
Parks & Rec.	12	2	16.7%
PSST	10	4	40.0%
Human Resources	0	0	0.0%
Utilities	37	10	27.0%
<b>Citywide Total</b>	<b>113</b>	<b>25</b>	<b>22.1%</b>



**PUBLIC WORKS  
FLEET DIVISION**  
Technician Productivity  
Report

FYE 2022

December 2021

MECHANIC	DIRECT LABOR HOURS	PRODUCTIVITY		INDIVIDUAL PRODUCTIVITY	
		GOAL		ACTUAL	DIFFERENCE
# 001	138.65	72%		97.0%	25.0%
# 002	151.24	72%		105.8%	33.8%
# 003	111.38	72%		77.9%	5.9%
# 004	93.61	72%		65.5%	-6.5%
# 006	104.58	72%		73.1%	1.1%
# 007	110.88	72%		77.5%	5.5%
# 008	111.53	72%		78.0%	6.0%
# 009	110.56	72%		77.3%	5.3%
# 010	124.70	72%		87.2%	15.2%
# 011	132.06	72%		92.3%	20.3%
# 012	106.55	72%		74.5%	2.5%
# 013	105.34	72%		73.7%	1.7%
# 018	97.18	72%		68.0%	-4.0%
# 021	97.44	72%		68.1%	-3.9%
# 028	118.51	72%		82.9%	10.9%
# 031	46.90	72%		32.8%	-39.2%
# 033	134.29	72%		93.9%	21.9%
# 034	63.72	72%		44.6%	-27.4%
# 035	46.28	72%		32.4%	-39.6%

DIRECT LABOR HOURS	2005.40
TOTAL AVAILABLE HOURS	2613.00
PRODUCTIVITY GOAL	72.0%
ACTUAL PRODUCTIVITY	76.7%

**PUBLIC WORKS FLEET DIVISION  
PM COMPLIANCE REPORT**

**December FYE 2022**

**Industry Standard Compliance: Not To Exceed 5%**

**Item 4.**

Department/Division	Number of PMs Scheduled	Number of PMs Completed On Time	Number of PMs Completed LATE	Number of PMs Still Past Due	Current % PENDING	YearToDate Non-Compliance Trend
<b>CITY CLERK</b>						
CITY COUNCIL					0%	0%
BUILDING ADMINISTRATION					0%	0%
<b>MUNICIPAL COURT</b>						
MUNICIPAL COURT					0%	0%
<b>INFORMATION TECHNOLOGY</b>						
INFORMATION TECHNOLOGY					0%	0%
<b>HUMAN RESOURCES</b>						
HUMAN RESOURCES					0%	0%
					0%	50%
					0%	0%
<b>FINANCE</b>						
METER SERVICES	1	1			0%	0%
<b>PLANNING</b>						
PLANNING	2	2			0%	0%
BUILDING INSPECTIONS					0%	0%
CODE COMPLIANCE					0%	67%
<b>PUBLIC WORKS</b>						
ENGINEERING					0%	10%
STREETS	8	11	3		0%	26%
STORMWATER	2	2			0%	6%
TRAFFIC	6	7			0%	13%
STORMWATER QUALITY					0%	0%
FLEET	12	11		1	8%	8%
TRANSIT					0%	40%
<b>POLICE</b>						
ANIMAL CONTROL	1	2			0%	31%
POLICE ADMINISTRATION					0%	31%
POLICE STAFF SERVICES	1	2			0%	19%
POLICE CRIMINAL INVESTIGATIONS	1	2	1		0%	44%
POLICE PATROL	14	17			0%	20%
POLICE SPECIAL INVESTIGATIONS	1	1			0%	0%
POLICE EMERGENCY COMMUNICATIONS					0%	0%
<b>FIRE</b>						
FIRE ADMINISTRATION					0%	62%
FIRE TRAINING	1	1			0%	50%
FIRE PREVENTION	1	1	1		0%	25%
FIRE SUPPRESSION	2	2	3		0%	100%
FIRE DISASTER PREPAREDNESS	1	1			0%	31%
<b>PARKS &amp; RECREATION</b>						
PARK MAINTENANCE	11	9		2	18%	19%
PARKS & RECREATION					0%	0%
CUSTODIAL					0%	0%
FACILITY MAINTENANCE	1	1			0%	39%
<b>PUBLIC SAFETY SALES TAX (PSST)</b>						
PSST POLICE PATROL	9	7	2	2	22%	44%
PSST POLICE CRIMINAL INVESTIGATIONS					0%	0%
PSST FIRE SUPPRESION	1	2			0%	0%
<b>CDBG</b>						
PLANNING CDBG					0%	0%
<b>UTILITIES WATER</b>						
UTILITIES ADMINISTRATION					0%	0%
WATER TREATMENT PLANT					0%	0%
WATER PLANT					0%	0%
WATER PLANT WELLS					0%	50%
WATER PLANT LAB					0%	0%
LINE MAINTENANCE ADMIN.					0%	0%
WATER LINE MAINTENANCE	8	6		3	38%	38%
UTILITIES INSPECTOR					0%	0%
<b>UTILITIES WRF</b>						
WRF ADMIN					0%	14%
WRF INDUSTRIAL					0%	17%
WRF BIOSOLIDS					0%	0%
WRF OPERATIONS					0%	0%
SEWER LINE MAINTENANCE	7	7			0%	0%
<b>UTILITIES SANITATION</b>						
SANITATION ADMINISTRATION					0%	22%
SANITATION RESIDENTIAL	7	4	3		0%	84%
SANITATION COMMERCIAL	2	2		1	50%	67%
SANITATION TRANSFER	5	4	2		0%	65%
SANITATION COMPOST					0%	33%
SANITATION RECYCLE	2	2			0%	0%
SANITATION YARD WASTE	6	4		1	17%	17%
<b>CITYWIDE TOTAL</b>	<b>113</b>	<b>111</b>	<b>15</b>	<b>10</b>	<b>9%</b>	<b>32%</b>

**FLEET MANAGEMENT  
INVENTORY  
December 2021**

**FUEL**

WESTWOOD GOLF	391.8	gallons	DIESEL	@	2.320	\$	908.98
WESTWOOD GOLF	576.4	gallons	UNLEADED	@	2.390	\$	1,377.60
NORTH BASE	3,026.9	gallons	UNLEADED	@	2.250	\$	6,810.43
NORTH BASE	996.3	gallons	DIESEL	@	2.420	\$	2,411.01
FIRE STATION #5	355.1	gallons	UNLEADED	@	2.350	\$	834.56
FIRE STATION #5	207.1	gallons	DIESEL	@	2.440	\$	505.32
FIRE STATION #6	210.9	gallons	DIESEL	@	2.440	\$	514.60
FIRE STATION #6	76.1	gallons	UNLEADED	@	2.350	\$	178.84
BULK TANKS	1,200.0	gallons	DIESEL	@	2.420	\$	2,904.00

TOTAL	GALLONS:	DOLLAR:
UNLEADED	4,034.5	\$ 9,201.41
DIESEL	3,006.1	\$ 7,243.90

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**CITY OF NORMAN**  
**DEPARTMENT OF PUBLIC WORKS-TRAFFIC CONTROL DIVISION**  
**MONTHLY PROGRESS REPORT**

<b>DECEMBER 2021</b>	<b>PROJECTED GOAL</b>	<b>THIS MONTH</b>			<b>YEAR TO DATE</b>		
	<b>Percentage</b>	<b>Number of Requests</b>	<b>Goal Met</b>	<b>Percentage Met</b>	<b>Number of Requests</b>	<b>Goal Met</b>	<b>Percentage Met</b>
Provide initial response to citizen inquiries within 2 days	100%	73	73	100%	427	427	100%
Provide information requested by citizens within 7 days	95%	73	73	100%	427	427	100%
Complete traffic engineering studies within 45 days.	99%	2	2	100%	15	15	100%
Review subdivision plats, construction traffic control plans, traffic impact statements, and other transportation improvement plans within 7 days.	95%	39	39	100%	292	292	100%
Worker Hours Per Gallon of Paint Installed.	0.80	<b>Gallons</b>	<b>Worker Hours</b>	<b>Percentage</b>	<b>Gallons</b>	<b>Worker Hours</b>	<b>Percentage</b>
		0	0	100%	1659	547.75	0.33
Thermoplastic legend, arrows, stop bars & crosswalks installed.	4-6 Installations per day per 2 person crew. 100%	<b>Crew Work Days</b>	<b>Total Installations</b>	<b>Average</b>	<b>Crew Work Days</b>	<b>Total Installations</b>	<b>Average</b>
		0	0	#DIV/0!	1.66	8	4.82
Preventative Maintenance on each traffic signal once a year. Approximately 11 will be performed each month.	100%	<b>Number Performed</b>	<b>Goal Met</b>	<b>Percentage Met</b>	<b>Number Performed</b>	<b>Goal Met</b>	<b>Percentage Met</b>
		12	12	100%	81	81	100%
Response to reports on traffic signal malfunctions within one hour.	99%	<b>Number of Reports</b>	<b>Goal Met</b>	<b>Percentage Met</b>	<b>Number of Reports</b>	<b>Goal Met</b>	<b>Percentage Met</b>
		14	14	100%	104	104	100%
Response to reports of sign damage:	<b>Percentage</b>						
<b>High Priority</b> Stop or Yield Signs within one hour	99%	9	9	100%	36	36	100%
<b>Lower Priority</b> all other signs within one day	90%	54	54	100%	190	190	100%
<b>Street Name Signs</b> within two weeks	90%	11	11	100%	89	89	100%
Percent of work hours lost due to on the job injuries.	<.01%	<b>Total Work Hours</b>	<b>Work Hours Lost</b>	<b>Percentage Met</b>	<b>Total Work Hours</b>	<b>Work Hours Lost</b>	<b>Percentage Met</b>
		3200	0	0.00	19400	16	0.00

**UTILITIES 13**

## **LINE MAINTENANCE:**

### Waterline Capital Projects

- Beaumont Drive – 0%
- 1357 12<sup>th</sup> NE Avenue – 0%
- Crest Court – 0%
- Barb Court – 5% - Materials ordered

Barb Court: WA0358: - Staff replaced 570 feet of six-inch Ductile Iron with 6" C-900 PVC on Barb Court from Morren Drive to Crestland Drive. Construction started this week.

### Water Line Breaks – 7 in December

### Sewer Line Data

- Total obstruction service requests - 19
  - Private Plumbing: 18
  - City Infrastructure: 1
  - Sanitary Sewer Overflows: 0

### Lift Station D Flows:

- Days - 31
- Average daily flow: 1.12 MGD
- Total Monthly flow: 38.13 MG

## **UTILITIES ENGINEERING:**

Line Maintenance Building (WA/WW0329)/WRF Storage Building (WW0318): Project will construct new building for Line Maintenance Division consisting of 49 staff; the existing 11,000 SF building is currently located on North Base property adjacent to the Fleet offices and vehicle servicing areas. This project assumes construction of a new 20,000 SF facility NW of the water treatment plant to house administrative offices, the employee breakroom, fire hydrant/meter repair area as well as areas for numerous water and sewer line repair parts, pump repair parts and a work area for pump repairs and testing. A preliminary meeting was held with City staff to discuss the project and what variances may be required. Additional funding will also be necessary due to the increased cost for the project to meet actual division needs now and into the future. Due to other department priorities, funding for the project was pushed back to FYE 23 so the project has essentially been pushed back one year.

Solar Arrays at WRF and WTP: Project will utilize areas in the vicinity of the Water Reclamation Facility and the Water Treatment Plant to install new solar arrays that will offset electrical use from the grid. This project moves the Utilities Department forward and closer to meeting the *Mayors Climate Protection Agreement* and the *Ready for 100* initiatives. This performance guaranty contract has requirements such that the City will be reimbursed if sufficient energy is not generated.

Design and permitting work is currently ongoing for the project. The Special Use Zoning was recommended for approval by Planning Commission. Floodplain permits were also obtained for both site locations. Council approved the zoning in November 2021. The next step is to complete the interconnection agreements with the utility companies. The required paperwork for the interconnections will be submitted after Council approval of items on January 18, 2022.

In December 2019, NUA learned that Bureau of Reclamation (BOR) had approved a grant of up to \$700,000 to assist funding project. Grant contract was fully executed during September 2021. In early October 2021, NUA learned that it has been awarded a second grant from BOR, a green infrastructure grant to evaluate what role a constructed wetland could play in further improving quality of stormwater and/or effluent water entering Lake Thunderbird as part of a future reuse program. This grant includes \$209,824 in funds for 2022 and can be extended with additional funds added in future years as conclusions are reached and recommendations made based on the work done during the initial years' funding. A kick-off meeting for the Green Infrastructure grant convened in early November 2021 and initial work (mostly research at this point) is ongoing.

Engineer: Garver LLC (Michael "Cole" Niblett)

WRF Dewatering Centrifuge Replacement (WW0326) – Project will replace dewatering centrifuges at WRF. Current centrifuges are approaching the end of their useful lives, and thus require excessive maintenance and repairs, and, due to poor service provided by manufacturer, often require extended downtime before necessary repairs can be completed to keep them operational. NUA approved a Contract with Garver, LLC on December 8, 2020, and design for this project commenced immediately thereafter. During the first half of 2021, manufacturers of several different dewatering technologies ran demonstration tests that were observed by Garver, WRF and NUA Engineering staff. WRF and NUA Engineering staff also made several site visits at plants in the region to observe other manufacturers/technologies in operation. These demonstrations and observations generally showed that the various screw press technologies could not meet the minimum requirement of consistently producing 20% solids sludge. In response, one manufacturer, PW Tech using their Volute Dewatering Press performed a followup demonstration with a new mixing technology that was supposed to increase percent solids and reduce polymer demand. However, the demonstration failed to show either. Based on these results, Garver issued a Final Version of the Preliminary Engineering Report in November 2021 recommending hydraulic drive centrifuges as the technology for this project. During December 2021 and early January 2022, Garver and NUA met with several reputable to centrifuge manufactures to learn more about their equipment and, just as importantly, to learn about their manufacturing and service infrastructure. Based on these meetings, four (4) manufacturers have been approved for inclusion in the project specifications.

Based on feedback from the meetings with centrifuge manufacturers, it appears that the current lead time on centrifuge manufacture is approximately 7 months (1.5 months to produce shop drawings + 0.5 months to review and approve shop drawings + 5 months to manufacture). Based on this long lead time and its likely impact on a traditional design-bid-build construction schedule, the project appears to be a good candidate for Construction Manager at Risk (CMaR) project delivery in which the CM could commence procurement of centrifuges immediately after award of Contract while Garver is concurrently completing final design and thus minimize time lost during construction while awaiting manufacture of centrifuges. Garver is currently preparing schematic drawings and a specification for centrifuges which along with Norman's standard CMaR front end documents should be sufficient to complete a CMaR RFP. Therefore, the CMaR RFP should be ready for release in February 2022. A CM would then be selected and contract awarded in March 2022. Procurement of centrifuges should be complete, shop drawings approved and manufacture commenced by May 2022. Final Design would then be complete in June 2022 and a Guaranteed Maximum Price (GMP) negotiated with CM in July 2022. Construction would commence in August 2022 with ample time to complete preliminary work before Centrifuge delivery in November 2022. Construction should be complete in Spring 2023.

Engineer: Garver LLC (Michael "Cole" Niblett)

SE Norman Lift Station Payback (WW0306): Staff has recently updated the wastewater model to project flows generated from full build-out of the Destin Landing Development in SE Norman. A series of interceptors as well as one large lift station with flow equalization can eliminate one existing and three proposed lift stations in southeast Norman. This project will estimate project costs, assign wastewater generation estimates to undeveloped properties to be serviced, and prorate payback costs per parcel based on wastewater generation projections. Developers might initially fund the lift station and/or the NUA with a portion of the funding paid back as additional areas develop. RFP issued 06/12/18 for this work with proposals due 07/15/18. On 08/07/18, staff selected Search, Inc. to prepare the sewer service area study and evaluate its potential as a payback project. Staff expects to bring a contract forward to NUA in August, 2021.

Bishop Creek Interceptor Project (WW0174): Project will replace or parallel approximately 20,600 feet of existing sewer interceptors in the in the Bishop Creek wastewater basin to accommodate the full build-out wastewater flows. The project

area generally lies between Highway 9 and Constitution and between Jenkins and Classen Boulevard. Staff will soon prepare an RFP to select design consultant for this and other water/sewer projects.

Lift Station D Force Main Replacement (WW0091): Another portion of the Lift Station D 16" ductile iron force main along 12th Ave N.E. is severely deteriorated and in need of replacement. Approximately 3,500 feet of this force main to just south of Rock Creek Road was replaced with 20-inch PVC in 2009. The area needing replacement because of continued breaks is on the west side of 12th from the end of the previous project to the point where it begins to gravity flow near the 12th Avenue Recreation Center. The project is expected to require 2,300 feet of new 20-inch piping, three air release vaults, and a new receiving manhole. After discussions with the Parks Department, the City of Norman will continue to lease the property needed for this project and will not purchase the property outright. Therefore, the project was kicked back off to identify the best alignment and then move forward to final design, easement acquisition (as necessary), bidding and construction. Design is progressing based on field investigations and 90% plans were submitted in November. Staff has reviewed the plans and there are constructability concerns regarding the proposed alignment. Staff have reviewed the alternative alignment on the east side of 12<sup>th</sup> Avenue NE and this corridor appears to be more constructible due to reduced utility conflicts. Staff is working with the consultant for an amendment to the contract to complete this work.

Engineer: PEC, Inc. (Chris Grizer)

Class A Sludge / Co-Composting (WW0312): This Project includes evaluation of biosolids co-composting via the windrow method to achieve Class A Biosolids for the Norman Utilities Authority (Owner) in Norman, Oklahoma. An evaluation of sites near the Water Reclamation Facility (WRF) will be evaluated as well. This scope of services also includes modification and update to Norman Water Reclamation Facility's existing Sludge Management Plan. Technologies were screened and Windrow Composting was determined by the Engineer to be the best alternative due to capital cost and operational familiarity considerations. The project was put on hold pending further discussions with landowners for land application of bio-solids that is a lower cost option than co-composting.

Engineer: Garver, Inc. (Steve Rice)

Septage Receiving Facility (WW0319): The Water Reclamation Facility (WRF) is often contacted regarding the possibility of taking trucked wastewater from hauling companies. Delivery of trucked wastes is not typically approved due to the undocumented quality of the trucked waste. Oklahoma City is currently the only municipal location in the metropolitan area for haulers to dispose of septage. The WRF could potentially generate revenue from companies hauling septage (septic tank clean-outs) and other acceptable wastes. Additionally, City of Norman residents utilizing a private sewage system could benefit through their private haulers having a closer disposal solution.

To allow for the new facility, the current WRF treatment processes must be protected which may require temporary storage of the trucked wastes delivered to the WRF while water quality testing is performed to verify its acceptability. Preparation of an engineering study and design is budgeted for FYE19 while construction is budgeted for FYE20. This project is not a high priority for service reliability and has been deferred due to funding availability.

Engineer: Olsson, Inc. (Kevin Rood)

WRF New Maintenance Building (WW0318) and WRF Main Control Building Renovation (WW0325): - These two projects are being designed under a single design contract and are anticipated to be bid as a single project, either traditional design/bid/build or Construction Manager at Risk (CMaR) project so they will be covered as a single project here. Due to recent plant improvements projects, space formerly used for spare part storage has been incorporated into plant operations space, leaving a shortage of viable storage space. Project WW0318 will cover the construction of a new pre-engineered Maintenance Building for spare part storage and other critical maintenance activities to offset space lost in existing facilities. The Main Control Building at the WRF was constructed in 1982 and, while some building systems have been replaced and/or upgraded since then, many of the original interior and exterior finishes as well as the main laboratory have not been replaced or renovated since original construction and are now nearing the end of their useful lives. Project WW0325 will renovate existing building and update layout and building systems as well as expand and renovate the laboratory.



Greeley Hansen was selected as the Architect for these two projects, and their Contract was approved on June 8, 2022. A design kickoff meeting convened in late June. Greeley Hansen submitted a draft Preliminary Engineering Report in November 2022 and they are currently incorporating comments into a final report. The November review meeting included a discussion of project delivery methods, and all parties agreed that Construction Manager at Risk (CMaR) was the best option for this project. First, it is believed that the phasing and scheduling of the work could open up several possible value engineering opportunities and having the Construction Manager on board during final design would ensure that these opportunities are properly and fully evaluated and, where costs savings are confirmed, quickly implemented. Moreover, over the past year or so, lead times on seemingly random construction materials and pieces of equipment have gotten extremely long. Having a Contractor on the project team would allow those items with long lead times to be identified and for advance procurements to be scheduled as needed so as to minimize delays to construction.

If CMaR is to be used: Greeley Hansen will submit their final Preliminary Engineering Report in January 2022. This report along with Norman standard CMaR front end documents will serve as the RFP, which would then be advertised in February 2022 and CM selected by March 2022. CMaR Contract would be awarded in April 2022 and, along with value engineering reviews, CM would immediately commence identifying equipment and construction materials with long lead times. The Procurement process for these materials would then commence in May 2022 with the intention of having equipment and materials delivered starting in July 2022 or whenever thereafter, they are needed to keep work on schedule. Final Design, with value engineering input from CM, would continue in the same time frame and should be complete in May 2022. Guaranteed Minimum Price (GMP) would be negotiated during June 2022, and Contract Amendment with approved GMP executed in late June 2022 or early July 2023 with construction commencing immediately thereafter. Construction will be complete in June 2023.

If traditional design/bid/build project delivery is to be used: Project would be advertised in late May 2022, bids opened in June 2022 and awarded in July 2022. Construction would then continue until August 2023 (and possibly longer depending on impact of current supply chain issues).

Engineer: Greely and Hansen LLC (Ana Stagg)

## **WATER PROJECTS:**

Waterline Relocation: I-35 Widening (WA0196): Unbudgeted project necessitated by ongoing widening of I-35. Total budget of about \$500,000 estimated for replacing three waterlines associated with the I-35 project (24-inch at Briggs Street, 12-inch at Brooks Street and 12-inch at 24th Avenue SW and Highway 9). Team of SAIC/Poe and Associates will prepare design plans as an amendment to the Lindsey Street widening project. ODOT expected to reimburse waterline relocation costs at end of project. NUA approved design contract with SAIC and ODOT relocation agreements on 10/22/13. ODOT required I-35 crossing at Briggs Street to be deepened to 28 feet below grade; construction costs will increase approximately \$450,000. Project advertised 04/29/14 and three bids were opened 05/22/14; Matthews Trenching was the low bidder at \$997,875. NUA approved contract on 06/10/14; work began 06/11/14. \$840,780 has been previously reimbursed by ODOT for this project. Staff requested reimbursement for the rest of the construction portion in December 2021 and received payment in the amount of \$215,320 from ODOT in February 2021. Staff received Audit Reports from ODOT in April 2021 for reimbursements in the amount of \$36,300 and \$8,700 for design agreements. A rebuttal letter with invoices were sent to ODOT on April 21, 2021 in order to receive reimbursement. However, after review, ODOT explained that they need the invoices to show how much of each invoice went towards each agreement. None of the invoices were broken up showing this distinction, so staff reached out to Benham (who took over SAIC) on June 9, 2021 so that they could provide us with that information. Staff has followed up with Benham and they are still in the process of getting the information needed.

Waterline Replacement: Interstate Drive (WA0245): Project will replace approximately 8,600 feet of failing ductile iron water lines with PVC pipe to reduce service disruptions to customers and damage to driveways and streets. The project is located along North Interstate Drive from Sooner Fashion Mall north to Northwest Boulevard and along Copperfield Drive from Northwest Blvd. to Interstate Drive with an extension west into Springbrook Addition. New piping will include

about 8,600 feet of 6, 8, and 12-inch PVC pipe. Easements are still being acquired for the project with five of the parcels received. The last easement is in final negotiations so the project was advertised and bids were opened on December 3, 2020. Cimarron Construction Co. was the lowest, responsible bidder and the contract was awarded on January 12, 2021. Public Notice letters were sent to home and business owners on February 24, 2021 and the contract started on March 3, 2021. Contractors started mobilization on March 10, 2021. Since the installation of the line on Parkway Drive there has been a line break and two leaks on the adjacent line along 26<sup>th</sup> Street. Cimarron could not perform the additional replacement on 26<sup>th</sup> and Parkway Drive following the breaks and leaks recognized recently, so the Line Maintenance Division started replacement internally. Contractors finished final punch list items this month and were sent the inspectors final quantities to review. A change order will be completed for additional items and adjustments to the contract price.

Engineer: Cardinal Engineering.

Waterline Transmission: Robinson Avenue Water Line Replacement (Phase III) – 24<sup>th</sup> Avenue NW to Flood Ave (WA0242): Project will install approximately 6,800 feet of 30-inch PVC C900 along the north side of Robinson Avenue from just east of the 24<sup>th</sup> Avenue NW to Flood Avenue. The new 30-inch line will replace the existing 16-inch line. This project will also replace approximately 3,000 feet of 12-inch water line on the south side of Robinson in the same vicinity. The project was bid and the contract was awarded to Garney Companies, Inc. (Garney) in the amount of \$1,978,875.00 on July 14, 2020. As of the end of May 2021, all 12" and 30" water line was installed, tested, and placed into service. On July 22, 2021, a final inspection convened with representatives from Garney, City of Norman and OU Grounds staff in attendance, and a preliminary punchlist was generated. On October 8, 2021, a final punchlist was forwarded to Garney, and all work was deemed complete on November 2, 2021. A final change order has been negotiated and a final pay request is being prepared by Garney. Project should be final accepted by City Council at either the last Council Meeting of January 2022 or the first one in February 2022.

Engineer: Alan Plummer and Associates, Inc. (Chris Ferguson)

Flood Avenue Waterline Replacement (WA0328):

Project will replace approximately 12,700 feet of 16-inch water line from Robinson Street to Tecumseh Road, and about 2,900 feet of 12-inch from Tecumseh to Venture Drive with 16-inch C900 PVC pipe. The existing ductile iron pipe is not compatible with the corrosive clay soils. It is failing, causing service disruptions to customers and damage to driveways, streets, and yards. In combination with this project, the potential for using existing wells along Flood for blending back into the water system using the existing pipe after rehabilitation will be evaluated. Design is funded in FYE19, with construction funded in FYE20. Based on detailed field survey, the alignment appears to require the removal of the existing 16-inch line and installation of the new line in the same corridor. Water model simulations have indicated that this will not impact service levels but this work is going to be scheduled during the non-demand seasons to reduce the impacts of this outage. Comments on preliminary plans were provided to Engineer on April 3, 2020. Plans at 65 percent level were received and reviewed and comments were returned back to the consultant. Utility verification and potholing were completed to allow for the design to be finalized. Plans were reviewed on February 23, 2021 and a permit was sent to BNSF Railroad for the Lexington Street crossing on March 1, 2021. Staff sent a final Addendum on August 23, 2021 and held a bid opening on August 26, 2021. Total bids ranged from \$2.9 million to \$3.8 million. Southwest Waterworks was determined to be the lowest responsible bidder at \$2,854,724.50. City Council approved the contract with Southwest Water Works on September 14, 2021 in the amount of \$2,854,724.50. A pre-construction meeting was held on October 6, 2021. Construction is set to begin on November 3, 2021 and will continue for 330 calendar days. Contractors completed the Goddard Ave line and are waiting on safe bacteriological results before placing in service. Contractors have started marking existing utilities near the water tower in order to lay that line next. Staff is still in negotiation with the property owner for the Lexington street easement.

Advanced Metering Infrastructure (WA0351): The City of Norman has an aged water meter population and current and improvement technology have improved such that advanced metering infrastructure would provide significant benefits for the City and its customers. The implementation of this technology will reduce staff requirements for the reading of meters and will ensure more timely and accurate readings. With daily water usage information accessible for staff and

the customer, customers will be able to be notified of leaks and better understand how water is used at their property. This will also help with water conservation efforts and billing resolutions. In addition to water metering improvements, the system and technology will also be leveraged to the maximum extent possible for monitoring the water system and other City needs. The consultant has completed the assessment phase of the project. Staff are currently reviewing the draft report prior to finalizing the report. The procurement phase, specifically the generation of the Request for Proposal, will begin in November. The bid package will be let in April 2022.

Consultant: E Source (Nicole Pennington)

FYE15 Water Wells and Supply Lines: Staff issued RFP 08/18/15 for water rights permitting, well field design, test and production well design, well house design and distribution system improvements. 14 proposals were received 09/14/15; staff selected several consultants and followed with contract negotiations. On 02/09/16 the NUA approved APAI contract K-1516-85 to update of our city wide water model; NUA approved contract with Cowan to perform water right permitting through the OWRB on 02/26/16 and approved contract with Carollo to perform well field design, and test/production well design on 06/14/16. On 2/14/17 NUA approved contract with Garver to study pros and cons for utilizing horizontal well technologies for one of our wells. Each project is broken out below.

Well Field Blending and Future Groundwater Treatment Site (WA0214):

This project will determine the best location, layout, and treatment processes for blending and treating the 32 active groundwater wells utilized by the City of Norman. All active wells are currently in compliance with the standards set forth in the Safe Drinking Water Act and Public Water Supply Operations are not required to provide treatment and residual disinfectant under Oklahoma Administrative Code 252:631. However, the NUA also treats and distributes surface water from Lake Thunderbird. Since the water from the surface water source and the groundwater wells is blended in the distribution system piping, ODEQ has indicated that the system will need to be modified such that a minimum disinfectant residual of 1.0 mg/L of total chloramines (NHCL<sub>2</sub>) should be found throughout all parts of the system in the future. In addition, maximum contaminant levels (MCLs) of total chromium and arsenic may be lowered by EPA, and a new MCL for hexavalent chromium may be established in the future, thus requiring additional treatment for the groundwater wells. NUA entered into a contract with Carollo Engineering, Inc. on June 22, 2021 in the amount of \$95,090 to develop preliminary layouts for the future build-out of the facility including immediate needs and future treatment processes. The Notice to Proceed date was set for June 29, 2021 and a kick-off meeting and site field investigations are scheduled to be held on July 21, 2021. Staff met with SRB, LLC this month to obtain assistance for acquiring the land needed for the blending location which includes review of property records, survey and map, and negotiation assistance. A Purchase Order was created for \$16,200 for these services.

Wellfield - Carollo and staff held a coordination meeting with ACOG the morning of July 18th and also held a kickoff meeting the afternoon of July 18, 2016. ACOG prepared a map showing potential thick sands that will be targeted for future wells. Carollo used this information along with GIS data and composed a more robust map to better define future well locations to pursue. Additionally, it was decided to stay with ½ mile spacing so that maximum yields can be achieved. Carollo received modeling information from APAI which allowed them to proceed with optimizing the best well sites based on proposed groundwater treatment plant sites. Carollo prepared a Draft Technical Memorandum (TM) and staff provided comments and 45 potential well sites were selected. Carollo performed an optimization model and 20 of the 45 well sites stand out as sites to begin with. Carollo and staff prepared an addendum to their contract and it was approved by NUA 2/13/18. The next step is to approach land owners to negotiate test well sites. A Frequently Asked Questions sheet has been drafted and will be utilized with potential well site land owners. Test wells are expected to begin in September 2018. Staff is working with Carollo and their sub-consultant to obtain necessary access agreements for potential well sites. Approximately 8 landowners are interested and staff is moving forward with securing documents for 5 of them. Well Construction bids were opened and read aloud October 18, 2018. Layne Christiansen was low bidder and a contract in the amount of \$4,714,421.72 was approved November 27, 2018 at NUA meeting. Held kick off meeting January 23, 2019 and again met February 27, 2019.

Layne has drilled all test holes and plan to begin final well drilling January 2020. All 11 well site easements and deeds were approved by NUA on October 22, 2019. The NUA also approved CO#1 December 10, 2019 for Layne's contract to

add 3 additional wells to the original 6 as shown in the contract. This change order also added days to contract and completion date is now December 2020. Held kick off meeting January 23, 2019 and since Layne had drilled all test holes, the plan was to begin final well drilling January 2020. Layne's rig was under repair so final well drilling didn't begin until February 18, 2020. Crews have completed all wells and are now complete with all 6 monitoring wells. 1 deep monitor well failed and was re-drilled in September. Well house construction is complete and all wells are ready for pumping. Contractor finalized disinfection on well house lines and are in the final clean up stages. Project is substantially complete and final payment will be made in January or February of 2022.

Engineer: Carollo Engineers (Rebecca Poole)

Master Meter Project – Bids to install 10 master meters were opened 7/24/14. An agenda item was taken to Council but suspended until written documentation was obtained from USPS, MNTC, JD McCarty, and Veterans Center. On 6/19/17, staff received approval from USPS to install one meter and vault to accommodate a water line project they had underway. This project was completed 7/27/17. The permission letter they provided allows staff to complete the two other sites for USPS. Staff has made contact with MNTC and is scheduling a meeting in April 2018 to obtain similar written documentation to allow for the 3 new meter vaults to be installed on their property. Staff contacted the State of Oklahoma to obtain similar letter for JD McCarty and Veterans Center which have 2 new meter vaults each to be installed. Staff met with State of Oklahoma Real Estate officials and they requested additional information but seem amenable to our project. Additional documents were sent for their review. They emailed they are currently reviewing documents as of 6/6/18. Staff is designing a project along Robinson that will front the J.D. McCarty Center and the Veteran's Center. Staff is planning to meet with State officials as part of that effort. Staff has been unable to establish a meeting with MNTC. Though MNTC, JD McCarty, and Veterans Center are not onboard with installing master meters at this time, staff will work with USPS to install 2 additional meters at their facility. Our consultants for the Robinson water line project 24<sup>th</sup> NE to 12<sup>th</sup> NE have begun discussion with OMES and Department of Mental health regarding easements for both water line and meter vaults. Staff anticipates having an agreement established by December 2021.

Water Metering / Billing Audit Project – This project began in March 2018. Staff is working along with a meter consultant to randomly check approximately 130 domestic meters for accuracy. Meters were sent April 10, 2018 to a third party testing facility that will be used to verify accuracy of measurements. The consultant will compile findings within a report for staff review. Report was received October 2018 and after reviewing results, the ROI was not favorable on the meter inaccuracies alone but staff has contracted with E Source to lead Norman through the best path of obtaining Advanced Metering Infrastructure (AMI). This is the last report for this project and all reporting will be covered under the AMI project.

Robinson Water Line: 24<sup>th</sup> Ave NE to 12<sup>th</sup> Ave NE – Jacobs Engineering was selected as the consultant for the 30-inch water line project from 24<sup>th</sup> Ave NE to 12<sup>th</sup> Ave NE. The contract was approved by NUA on November 26, 2019, and project kickoff meeting was held January 14<sup>th</sup>, 2020. On May 6, 2020, a preliminary plan review meeting convened with NUA and Jacobs staff in attendance, and updated preliminary plans were approved in August 2020. In February 2021, 65% plans and specifications were submitted for NUA review, and, during March 2021, several Zoom calls convened to review NUA comments and address questions that arose from the comments. Based on these discussions and further review of existing conditions, Jacobs has proposed a fairly substantial revision to the alignment, which includes an open cut crossing of 12<sup>th</sup> Ave N.E. and relocating approximately 900 LF of the line into the traffic lanes of Robinson. NUA staff and Public Works staff have reviewed the alignment and deemed it acceptable with some minor conditions. NUA is compiling these conditions and will forward to Jacobs along with a request to immediately proceed with final design and easement acquisition on the basis of the realignment. Two property owners with whom easement agreements must be negotiated, J.D. McCarty and U.S. Department of Veterans Affairs, are also customers with multiple meters with whom NUA intends to negotiate an agreement on master metering. The current intent is to negotiate both concurrently and to include master meter installation for both into this project. Assuming no unexpected delays in easement acquisition, project should be ready for bidding in June 2022 with Contract Award in July 2022 and construction starting immediately thereafter. Project completion would then be anticipated in July 2023.

Engineer: Jacobs Engineering (Lars Ostervold)

Water Line Replacement, Parsons Addition (WA0246): A contract (K-1819-87) with Cabbiness Engineering, LLC was approved by NUA 12/11/18 to design the replacement of approximately 4,500 lineal feet of water line in the Parsons addition. Due to the project's close proximity to campus, the design process included public input from University of Oklahoma (OU), OU's sororities and fraternities, homeowners and landlords, and, based on that input and discussions with consultant and potential contractors, it was determined that the best of course of action would be to divide the project into two phases that would proceed generally during summer months when OU is not in regular session. As part of this process, it was also decided to install replacement mains in the streets rather than sidewalks wherever possible. Phase I was scheduled to begin July 1, 2019, and Phase II was originally scheduled for the summer of 2020.

Bids were opened for Phase I on June 13, 2019 and SW Water Works was low bidder at \$828,112. NUA approved contract June 25, 2019 and construction began July 8, 2019. Crews completed last concrete pours November 5, 2019. A final walk through by staff was performed on November 8, 2019, and NUA approved final as-bid to as-built quantities change order on January 28, 2020. In March 2020, Phase II was delayed until Spring of 2021. In March 2021, Phase II was delayed again until Spring of 2022. Assuming the project is not deferred again for budgetary reasons, it is now scheduled to advertise during the late Winter of 2022 in order for Low Bidder to start ordering long-lead materials in March and April 2022 (to offset potential supply chain-related delays), ensuring that Contractor can start pipe installation work promptly when Notice to Proceed is issued immediately upon the end of OU's spring 2022 semester on May 15, 2022. Assuming this schedule is maintained, it is anticipated that construction would continue through summer and major work would be completed before the start of the fall semester in August 2022.

Engineer: Cabbiness Engineering, LLC (Sean Price)

Water Wells Water Line: 108<sup>th</sup> & Tecumseh – Cardinal Engineering was selected as the consultant for one of the well water line projects, and NUA approved their contract November 12, 2019. Project design was completed during the first half of 2020, and project was advertised for bid on July 23 and July 30, 2020. Bids were opened August 13, 2020 and Hammer Construction was deemed lowest and best bidder. NUA approved Contract K-2021-11 with Hammer on September 8, 2020. Notice to Proceed with construction was issued on October 10, 2020. On October 6, 2021, a final inspection convened and all pipe, appurtenances and restoration on the project were deemed complete except for final testing of pipelines, which was ongoing as of the inspection date. The final pipeline section was successfully bacteriologically-tested on November 30, 2021 and all project work has been deemed complete as of that date. A final change order has since been negotiated and Hammer has submitted their final payment request. Project should be final accepted by City Council on either January 25, 2022 or, at the latest, February 8, 2022.

Engineer: Cardinal Engineering (Josh Risley)

Water Wells Water Line: 60<sup>th</sup> and Franklin – Garver Engineers were selected as the consultant for one of the well water line projects, and NUA approved their contract November 26, 2019. Design was completed during the first half of 2020, and project was advertised for bid on July 16 and July 23, 2020. Bids were opened August 6, 2020, and SMC Utility Construction was deemed lowest and best bidder at \$1,082,550. NUA approved Contract K-2021-12 with SMC on August 25, 2020, and Notice to Proceed with construction was issued on September 10, 2020. As of June 18, 2021, all 12" diameter well water lines had been installed, tested and were ready for service when needed by NUA. During June and July 2021, SMC worked on restoration of the project. Informal punchlist inspections were made in July 2021 and November 2021, with partial inspections regularly occurring in between. In January 2022, SMC advised NUA that, in their opinion, all project work was complete, and a final inspection to confirm this is scheduled for January 18, 2022. A final change order is also currently being negotiated with SMC. Assuming project work is deemed complete on January 18, 2022, project should be submitted to City Council for Final Acceptance on February 8, 2022.

Engineer: Garver Engineers (Jeff Chavez)

Lindsey Water Tower Rehabilitation and Non-Potable Water Expansion Study - Lindsey Tower was decommissioned several years ago because it is not high enough to act as viable potable water system storage at current system operating pressures. Recently the coatings on the tower have begun to show signs of deterioration. For this reason, Dunham Engineering evaluated the condition of the tank in the Fall of 2020, and they determined that the underlying steel was in good condition and the tank could be repainted and recommissioned for many years of additional service if needed. In July 2021, NUA requested that Dunham provide a proposed scope and fee for Lindsey Water Tower rehabilitation.

Dunham submitted their proposed scope and fee in early October 2021. Scope and fees were negotiated, However, before submitting Contract for City Council approval, NUA will await outcome of ongoing study being performed by Plummer Engineering, which is described in the following paragraphs and will directly impact future usage, if any, for Lindsey Tank.

In October 2021, NUA requested that Plummer Engineering furnish a proposed scope and fee to analyze the possibility of extending a non-potable reuse system from the Norman Water Reclamation Facility (WRF) to Reaves Park on the University of Oklahoma (OU) campus and to 10 to 15 other potential customers to the east of the WRF and Reaves Park, generally on or near Highway 9, as described Norman's 2060 Strategic Water Supply Plan. This reuse system would necessarily run in close proximity to Lindsey Tower so, as part of their analysis, Plummer has also been directed to assess the possibility of repurposing Lindsey Tower as dedicated storage for the new reuse system. Plummer's scope and fee was finalized in November 2021 and their Contract was approved by City Council on December 14, 2021. A kickoff meeting convened on January 6, 2022 and Plummer has commenced work on the study. The study should be completed by April 2022.

Assuming the Plummer study affirms repurposing Lindsey Tower as dedicated non-potable storage, a Contract for Dunham to prepare bidding documents for, and to inspect construction of, rehabilitation work on Lindsey Tower would be immediately submitted to Council in April 2022. Bidding Documents would then be completed and project advertised in May 2022, and Bids would be opened, Contracts awarded, and Notice to Proceed issued in June 2022. Rehabilitation work would then proceed through summer (which, given the tank's proximity to OU's campus, is the recommended time frame for the work). Project would then be complete by September 2022.

If Plummer's report does not recommend repurposing Lindsey Tank for non-potable storage, tank will be further evaluated to determine if it can be recommissioned and viably used for potable water storage. Depending on the outcome of this final evaluation, there is a range of possibilities for the future of Lindsey Tank. If no viable use can be found for Lindsey Tower at this point, it may be recommended for demolition. If it is determined that construction of additional infrastructure (for example a booster station) could make Lindsey Tower viable as potable water storage for the Norman system again, it could be part of a future project that combines rehabilitation of Lindsey Tower with construction of the identified new infrastructure that would see the rehabilitated tower returned to service upon completion of construction.

Engineer: Dunham Engineering for Lindsey Tower (Joe Seiter) and Plummer (Dexter May) for Non-potable Water System Study.

Water Treatment Plant Filter Effluent Pipe Improvements – During construction of the Phase II improvements at the Water Treatment Plant, staff discovered that the existing filter effluent line at the plant did not include an isolation valve, and, as a result, any future inspections or repair work on the 60+ year old line would require shutting down the entire treatment plant. To address this, a valve and inspection port will be cut into the existing filter effluent line that would allow portions of the plant to remain in service even if filter effluent line must be inspected and/or repaired in the future. NUA approved a Contract with Carollo Engineers on November 24, 2020. Bids were opened on December 18, 2020, and Contract was awarded to W.E.B. Construction (W.E.B.) on January 12, 2021. Notice to Proceed was issued on January 25, 2021 and W.E.B. mobilized to the project site in February 2021. Final inspection convened on July 14, 2021 with representatives from W.E.B., Carollo and NUA in attendance, and all parties agreed work was complete. Water Treatment Filter Effluent Pipe Improvements project is complete and was final accepted by NUA on August 24, 2021.

Engineer: Carollo Engineers (Tom Crowley)

Water Distribution System Sampling Stations (WA0350): Project will install 43 sampling stations throughout the water distribution system in order to meet federal and state requirements of the Revised Total Coliform Rule. The number of sites and location of each site is approved by the Oklahoma Department of Environmental Quality (ODEQ). These stations will allow staff to collect samples representing the entire distribution system at all times, rather than staff collecting samples from homes and businesses which may not be accessible at certain times of the year. In addition, each station will be tapped directly into a main water line, which will give us a better representation of the water supplied by the City. Each station will be constructed of stainless steel, surrounded by a cover with Norman logo and cyber lock, with a concrete pad. NUA will purchase stations directly from vendor, who will send the stations directly to the approved contractor for storage and installation. Staff obtained 4 quotes for stations and Ferguson Waterworks was the low bidder. Invitation to bid was published on March 4 and March 11, 2021. Staff received 8 bids that ranged from \$85,100 –



\$264,400; the low bidder being Sooner Sight Utility & Construction. City Council approved the contract with Sooner Sight Utility on April 27, 2021. The contract transmittal was sent on May 27, 2021 with a start date of June 29, 2021. Contractors started work on August 4, 2021. Contractors completed the first set of punch list items, but there are still valve issues and restoration needed on a few stations that will need to be fixed before the project is complete. Line Maintenance has completed bacteriological sampling from each station.

As-Built Linking Project: Project is an engineering effort to develop a system for linking as-built records to a GIS interface to allow for staff to more efficiently find as-built records. Contract K-2021-72 was awarded to Meshek & Associates on December 1, 2020. The project will allow users to click on a water or sanitary sewer asset in a GIS viewer which will then provide the as-built record document in a new window for viewing or downloading. Additional internal staff discussion was required to determine naming conventions for the consultant to use that would create a standardized system that could be used for this project and any future projects. As such, the project deadlines were set back a couple of months. All existing as-built information was provided to the consultant for their use. Pilot area information has been received and staff are currently reviewing the deliverable.

Blending of Wells 5, 6, and 52 at the Water Treatment Plant (WA0369): This project will blend wells 5, 6, and 52, at the Water Treatment Plant (WTP) before entering the distribution system. Blending these wells will only require samples be taken at the blending site as opposed to each well, which will minimize time and money spent on sampling each month. This project will require approximately 2,200 feet of 8-inch waterline to bring well 52 to the 36<sup>th</sup> Avenue NE and Robinson Street intersection where it will combine with the line coming from well 5. An existing 12-inch waterline will then be converted to a non-potable line as it moves west on Robinson Street before it combines with well 6 and the distribution system at the WTP. In addition, two bores consisting of approximately 260 linear feet of 8-inch and 6-inch waterline under Robinson Street will be required to connect the neighborhoods on the north side of Robinson Street with the existing high pressure line on the south side of Robinson Street. An existing 10-inch waterline and old 24-inch concrete line at the WTP will be abandoned as part of this project. As part of a previous project, Freese and Nichols, Inc. recently designed the project to extend water transmission lines from the WTP to 24<sup>th</sup> Avenue NE. Since they are familiar with the lines in the area and the interconnection requirements at the WTP and would be able to provide design services for this project, staff determined that it would be in the best interest of the NUA to contract with Freese and Nichols, Inc. for this project as well. City Council approved the contract with Freese and Nichols, Inc. on May 11, 2021 for \$47,850. Staff had a project kick-off meeting with engineers on May 27, 2021. Engineers are working on revisions to the 50% plans in order to submit 90% plans at the end of this month.

Jenkins Avenue Waterline Replacement (WA0353): This project will replace approximately 2,500 feet of existing 6-inch waterline with new 12-inch waterline in concert with the planned widening of Jenkins Avenue through the City of Norman Public Works Department. This project will also design a 1,000-foot extension of the Segment D transmission line recommended by the 2003 water master plan. In addition, this project will install a non-potable reuse line from Imhoff Road to Constitution Street. Freese and Nichols, Inc. is currently under contract with Public Works to design the widening on Jenkins Avenue and also the intersection improvements at Jenkins Avenue/Constitution Street/Imhoff Street, so staff determined that it would be in the best interest of the NUA to contract with Freese and Nichols, Inc. for this waterline project in order to ensure a cohesive design for both street improvements and the new water lines in this area. The contract with Freese and Nichols was approved by council on April 27, 2021 in the amount of \$95,740. Staff had a project kick-off meeting with engineers on May 27, 2021. Engineers are waiting for the roadway plans to be finished before sending 50% plans for this project so that the roadway and waterlines don't conflict with each other.

James Garner Ave Waterline Replacement from Main to Duffy (WA0338): This project will replace the aging 6, 8, 12, and 16-inch waterlines between Main Street and Duffy Street along James Garner Avenue in conjunction with the Public Works Department James Garner Avenue Streetscape project currently being designed by Cowan Group Engineering, LLC. The waterlines in this area are over 50 years old and have experienced failures that disrupt water service and traffic flows. Replacement of these waterlines prior to the surface improvements would ensure good infrastructure and reduce the likelihood that the surface improvements have to be removed and replaced for future water line replacements work. NUA entered into a contract with Cowan Group Engineering, LLC in the amount of \$93,800 on June 22, 2021. A contract transmittal was sent with a start date of June 29, 2021. Changes are currently being made to the streetscape project and

a kick-off meeting will be held once these changes are finalized to better understand the roadway corridor along Jam Garner. Item 4.

Phase II – Porter Avenue Waterline Replacement (WA0354):

This project will replace approximately 1,500 feet of aging 8-inch and 5,000 feet of 12-inch waterline along Porter Avenue from Robinson Street to Alameda Street in conjunction with the Public Works Department Porter and Avenue Streetscape project. Part of this project (Main Street to Rich Street) was already completed as part of Public Work's ODOT funded Transportation Bond project. Replacement of these waterlines prior to the surface improvements will ensure good infrastructure and reduce the likelihood that the surface improvements have to be removed and replaced for future water line replacement work. NUA entered into a contract with Cabiness Engineering, LLC (Garver Engineering) in February 2021 in the amount of \$58,100. Staff received final design plans and sent an invitation to bid to the *Norman Transcript* for publication on December 23, 2021 and December 30, 2021. A mandatory pre-bid was held on January 4, 2022 and the bid opening will be held on January 20, 2022.

Engineer: Garver Engineering (Bret Cabiness)

Water Treatment Plant Carbon Dioxide Tank Replacement Tank (WA0374):

The existing carbon dioxide tank at the City's Water Treatment Plant is aging and vendors are not able to supply replacement parts to the model any longer. Carbon Dioxide is used to adjust the pH of the drinking water which effects its scaling and corrosivity potential, so a new tank is necessary. This project will involve removal of the existing 30-ton carbon dioxide tank at the water treatment plant, complete installation of new tank 30-ton tank of correct make and model specified or approved equal, demonstrating correct operation and dosages both manually and remotely, and training water treatment staff on operation and maintenance. Staff awarded the removal and installation to Wynn Construction Co. for \$75,000. Staff awarded the purchase and delivery of the tank to Tomco Systems in the amount of \$228,975 in December. The purchase authorization is set to be approved by Council on January 18, 2021.

Southlake Addition Waterline Replacement (WA0352):

This project will replace approximately 7,500 linear feet of aging waterlines within the Southlake Addition, which is located between Cedar Lane and State Highway 9, just east of Classen Blvd (SH 77). The existing lines are ductile iron pipe that were installed in the 1980s and have experienced a significant amount of corrosion, causing a significant number of breaks impacting water service to the neighborhood. This project will install replacement 8-inch mains to replace the existing lines, along with minimal amounts of 6-inch for small streets and short dead-end lines, and will then reconnect services for the residents. A Request for Proposals (RFP) for this project was published in the *Norman Transcript* on July 15, 2021. Staff received 18 proposals on August 5, 2021. Staff reviewed each proposal and selected a firm using the ranking criteria listed in the RFP. Smith Roberts Baldischwiler (SRB) ranked the highest and was selected for the project. Staff held a kick-off meeting with Engineers on November 30, 2021 to discuss the preliminary design of the project. 65% will be received after the beginning of the new year.

Engineer: SRB (Bryan Mitchell)

Water Treatment Plant Clarifier 1 and 2 Rehabilitation (WA0375):

Clarifiers 1 and 2 at the Water Treatment Plant (WTP) were installed in 1965 and have experienced deterioration warranting replacement of gearboxes and motors, in addition to new coats of paint. After 55 years in service, it's expected that the structural integrity of these clarifiers will have decreased. In July 2021, staff hired Suez Water Technology Solutions Services, Inc., who currently own the "Accelator" technology employed by these two clarifiers, to perform a visual inspection on one of the clarifiers. They found pitting and corrosion of steel and indicated the need to repair/replace the launder and outer wall brackets, in addition to realignment for the hood structure and circular deckplate, and re-painting the structure. This project will first entail a more in-depth condition assessment of both clarifiers 1 and 2 by Carollo Engineers, Inc, including visual and non-destructive testing of the concrete to determine areas of possible concern and ultrasonic thickness testing for metal structures. A final technical memorandum will be provided with their findings in addition to estimated costs for needed repairs in order to obtain reliable capacity from the clarifiers for the next 20-40 years. A contract with schedule, scope, and fee of \$73,991.00 was obtained with Carollo and was awarded on December 14, 2021. Staff held a kickoff meeting with Carollo on December 22, 2021. It was determined that inspection of the two tanks will occur in late January 2022.



**SANITATION CAPITAL PROJECTS:**

Truck Wash Facility (SA0015)/ Container Maintenance Facility (SA0009) / Household Hazardous Waste Facility (SA0012): Project will provide an automated truck wash facility at the sanitation storage yard north of the WRF and will provide a new enlarged maintenance, and regulatory compliant welding and painting facility for solid waste collection containers. The Truck Wash Facility will be available to all City vehicles, but designed primarily for large equipment. For the container maintenance facility, the newly proposed location is on the site of the Transfer Station. Upon learning of the potential bus wash facility to be constructed at the North Base Campus, Utilities and Public Works have begun discussions regarding coordination of the facilities and the potential to combine efforts into facility capable of meeting needs for both departments and the City.

A Household Hazardous Waste Facility will also be included within this project. This Facility is necessary to allow for City residents a timely disposal option for household wastes that cannot be disposed using their normal polycart service. Currently, an annual collection day is held for City residents to dispose of their items but this has proven to be challenging in recent years.

Preliminary design efforts have identified potential improvements to the final product that warrant modifications to the project scope that required a contract amendment with the Architect. Amendment No. 1 was approved by City Council in February 2020 and included changes to the site for the Household Hazardous Waste and Container Maintenance facilities and entry drive modifications to the Transfer Station facility to improve accessibility and safety for vehicles entering and leaving the facility.

The property has been rezoned to add Municipal Use for the property to meet current code requirements. The final plans were completed and advertised. Bids were opened and contract K-2021-10 was awarded to the Landmark Construction Group on January 12, 2021. Work continues at both sites. Both buildings are 99% complete and minor items remain to obtain Certificates of Occupancy.

Architect: Studio Architects, LLC (George Winters)

New Sanitation Office Building: The Sanitation Department has outgrown their current office space. Because their existing facility cannot be readily expanded and because it is not laid out in a way that would allow for renovations to create a more efficient workspace, a new facility must be constructed. On January 12, 2021, Norman Municipal Utilities Authority (NMUA) approved a design contract with The McKinney Partnership Architects PC (TMP) to design this new facility, and design commenced immediately thereafter. TMP has finalized Design Development level plans and specifications, and, upon seeing their completeness during preliminary review, NUA has decided to proceed with traditional design/bid/build project delivery for this project. A design review meeting convened for design development plans on November 23, 2021. There were only minor comments, and TMP is proceeding with final design. Bidding Documents should be complete by the end of January 2022. Project will advertise immediately thereafter, with bids opening in late February 2022 and Contract Award in March 2022. Construction should be complete by March 2023.

Architect: The McKinney Partnership Architects PC (Toni Bragg)

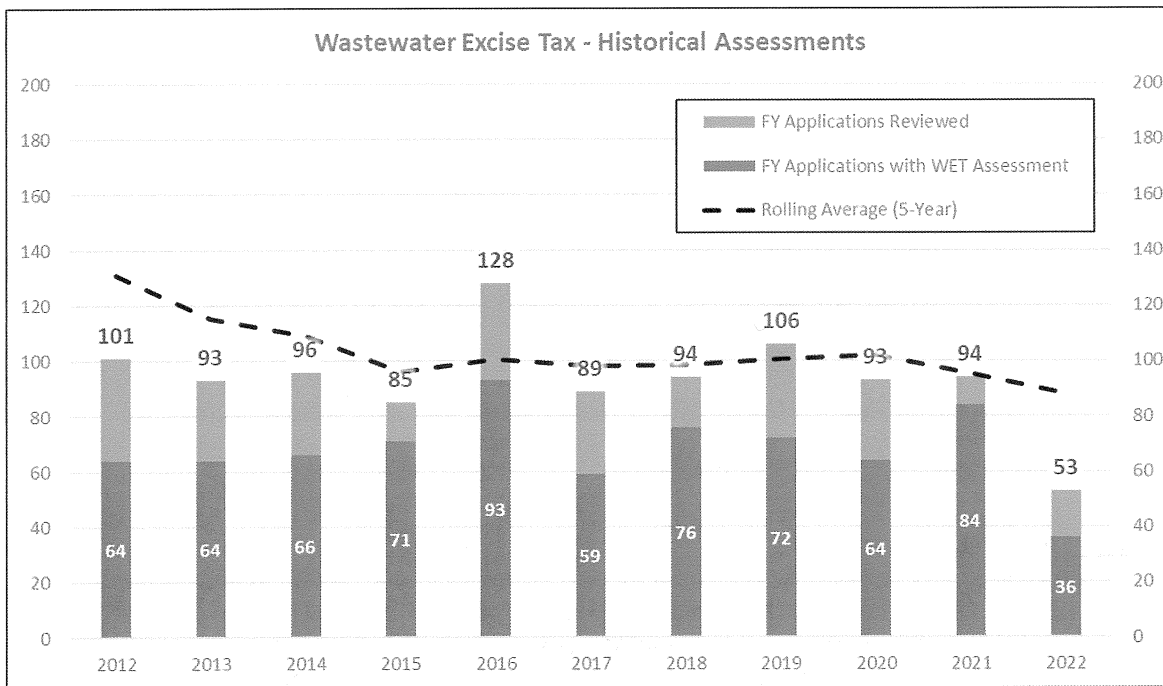
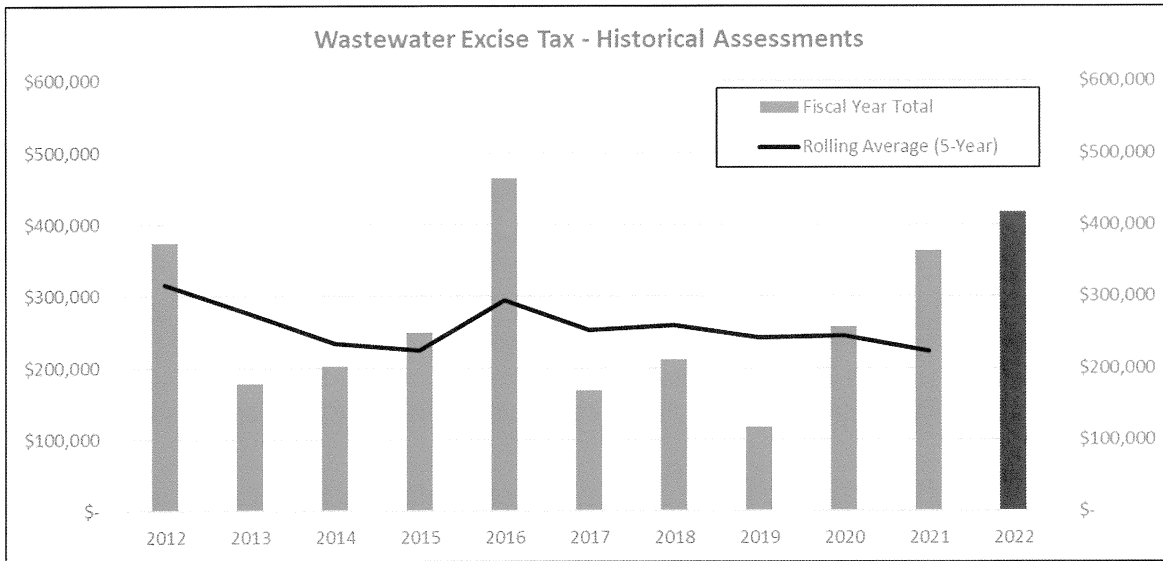
**Compost Facility Scale House (SA0019):**

This project will modify the existing City compost facility layout located at Bratcher Minor Road, west of Jenkins, to facilitate a more efficient operation for the public and facility, install scales used for weighing large loads of compost, and construction of a modular building with potable water and sanitary sewer for staff in charge of coordinating with customers. This building will also replace the prefabricated building purchased in 2003 that has become severely deteriorated and inadequate. Based on the project scope, staff appointed TriCore Group, LLC as the engineer responsible for design and bidding services. City Council approved the contract with TriCore Group in the amount of \$30,500. Staff met with the Engineer on June 16, 2021 for the kickoff meeting.

Engineer: TriCore Group, LLC

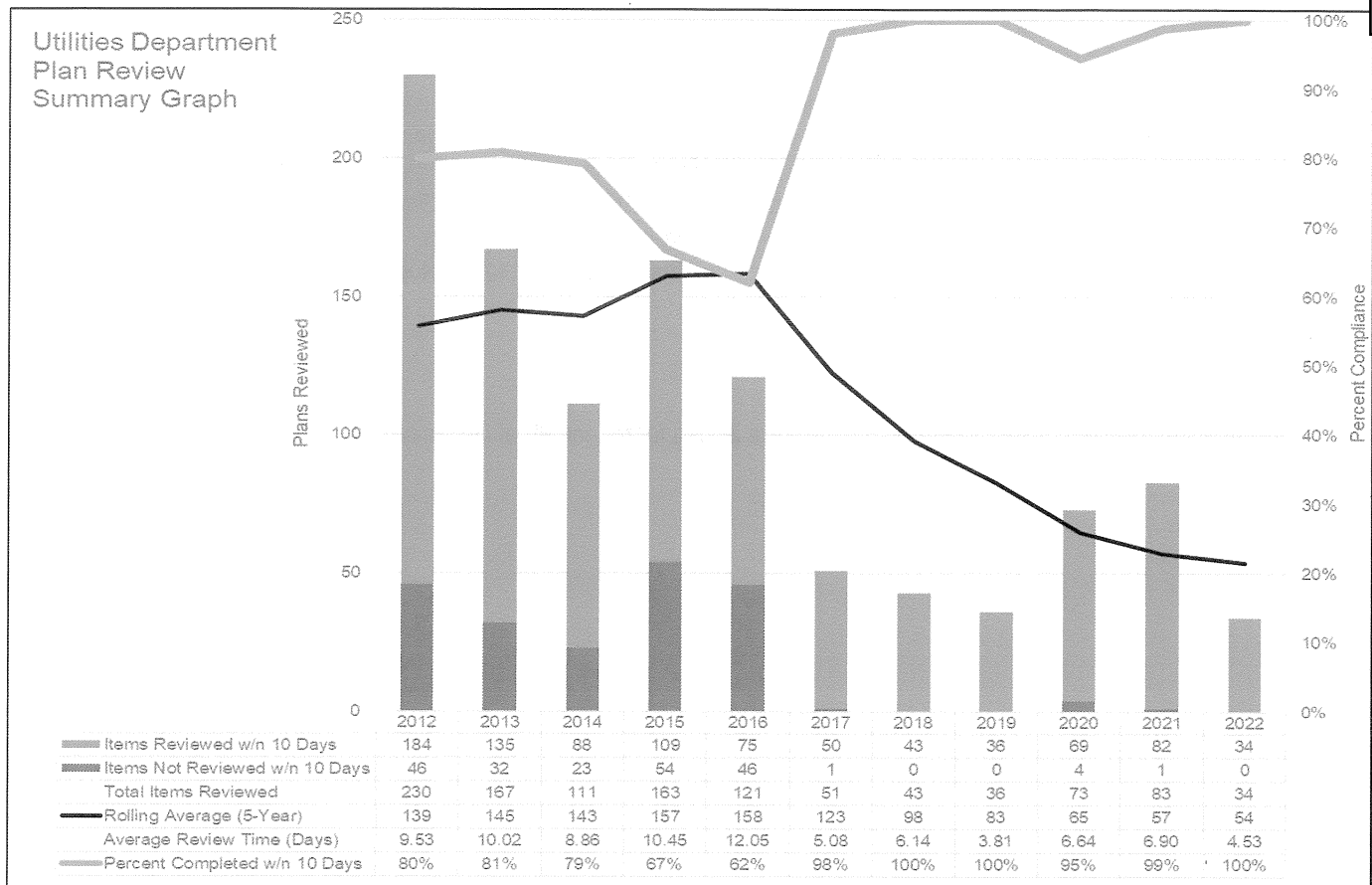
### Wastewater Excise Tax – Non-Residential:

WRF Investment Fee/Wastewater Excise Tax: Staff evaluated the Wastewater Excise Tax on 6 commercial entities last month. Of the 6 applications, 2 applications were assessed since the applications were determined to increase wastewater flows over the previous use of the site. Through December, 49 commercial properties were reviewed and a total of \$414,337.22 was assessed to the 34 entities that will increase wastewater flows for the respective property. Below are graphs showing the amount assessed and the number reviewed.



### PLAN REVIEW:

Eleven plan sets was reviewed during December. Staff have reviewed 34 plans for FYE2022 with an average review time of 4.5 days and with 100 percent of plans reviewed within 10 days.



## RECOUPMENT PROJECTS:

1. NW Sewer Study: 36th Interceptor & Force Main Payback projects established in 1998/1999: Because of abandonment of Carrington LS, two resolutions reducing number of parcels requiring payback approved 01/10/12. Releases for many properties now served by North interceptor system projects were filed of record in 2012. NUA approved appropriation of payback funds on 12/05/17. Collected payback fees of \$697 for Jolley Addition on 03/23/18. NUA approved appropriation of payback funds 12/10/19 allowing staff to issue payback checks to developers in late December 2019.
2. Sewer Service Area 5 Payback: Payback project established by R-0304-13 for NUA share of sewer improvements along Highway 9 from the Summit Valley Lift Station to the USPS.
3. North Porter Waterline Payback: Payback project established 04/12/05 for 12-inch waterline constructed by Calvary Free Will Baptist Church along Porter Avenue from Tecumseh Road north. Total payback to date is \$0.00 of potential \$61,177.
4. 36<sup>th</sup> Avenue NW Waterline Payback: Payback project established 08/24/99 for 24-inch waterline along 36th Avenue NW from Tecumseh Road to SE 34th in Moore. Total payback to date is \$65,123.
5. 24th Avenue NW Waterline Payback: Payback project established 04/22/08 for 24-inch waterline along 24th Avenue NW from convention center to Tecumseh Road. Medcore billed \$27,212 on 04/15/20; total payback to date is \$87,074 of original project cost of \$346,134.
6. Post Oak Lift Station Payback: Payback project approved 04/14/09 for sewer and lift station improvements to serve the Links development and other properties in SE Norman. Construction complete and final payback costs approved 01/25/11. Parcel 5 payback of \$15,717.09 paid 12/15/15; total of \$15,717.09 paid to date and will be returned to Links at end of fiscal year. Payback funds returned to Links in January 2018. Links check reissued in July 2019 as previous check was never cashed.
7. Interstate Drive Waterline Payback: NUA approved payback project on 04/22/14 for waterline improvements in University North Park in conjunction with the extension of Interstate Drive. Construction was complete in late 2015. Staff has

finalized project costs, payback amounts and the Final Payback resolution approved 12/10/19. Hudiburg Subaru billed \$28,540 on 04/24/20 and UNP was billed \$32,963 for detention pond on 04/24/20.

8. Ruby Grant Waterline Payback: NUA will soon consider a new waterline payback project for waterline improvements along Franklin Road in conjunction with the Ruby Grant Park Improvements. Norman Forward through the Parks Department will fund 50% and the NUA will fund the remaining costs to be paid back over 20 years as property to the north develops. Design plans are complete and project will bid 07/25/19. Ruby Grant Waterline Payback project approved by Council 12/10/19; construction of 12-inch waterline is nearing completion. Battison Honda is considering new development along Interstate Drive north of Franklin Road that will connect to the Ruby Grant Waterline.

#### **Private Water Well Permits Issued**

3 Water Well Permits 21-5481, 5533, and 5709 were issued for the month of December.

December 2021  
ENVIRONMENTAL SERVICES DIVISION  
MONTHLY REPORT

**INSPECTIONS**

	December	Year to date
Fats, oil and grease (FOG) program	29	390
Food license approval	0	11
Significant Industrial Users	0	21
<b>Total inspections</b>	<b>29</b>	<b>422</b>

**ROUTINE ACTIVITIES**

	December	Year to date
Significant Industrial User sites sampled	0	22
Required annual 40 CFR Part 122, Appendix D, Table II monitoring completed (%)	0%	100%
Required quarterly 40 CFR Part 122, Appendix D, Table III monitoring completed (%)	0%	100%
Household hazardous waste disposal calls	24	445
Immediate assistance requested	0	7

**REVENUE**

	December	Year to date
FOG Program	\$0.00	\$34,950.00
Surcharge	\$9,571.51	\$116,452.89
Lab Analysis Recovery	\$0.00	\$2,996.07
Industrial Discharge Permit	\$0.00	\$4,000.00
<b>Total revenue</b>	<b>\$9,571.51</b>	<b>\$158,398.96</b>

**ENVIRONMENTAL CONTROL ADVISORY BOARD (ECAB)**

- 1 Provided staff liaison support including agenda setting, meeting attendance, minutes preparation, and issue research.
- 2 Developing an seasonal eating calendar
- 3 Facilitating Yard by Yard Program
- 4 ECAB will have educational stations during the installation of Artificial Inlets
- 5 Created the ECAB James O. Harp Environmental Recognition Award

**MISCELLANEOUS ACTIVITIES**

- 1 Staff maintain and loan out recycling containers for special events. Containers are routinely used at the OKC Memorial Marathon, Earth Day, May Fair, Summer Breeze concerts, Midsummer Nights Fair, Groovefest, and 2nd Friday Art Walks
- 2 Working on Phase II of the Fats, Oils and Grease Program (FOG) - to include OU businesses, fraternities, sororities, and long-term care facilities
- 3 Staff is working with other departments to increase energy efficiency - i.e. turning off computers, duplex printing, anti-idling, methane recovery, and retrofitting pumps with variable speed drives.
- 4 Completed paint event on November 13, 2021
- 5 Completed and submitted 2021 Pretreatment Annual Report to DEQ on 12/30/21
- 6 Issued NOV to OU Chem Annex for Cu, Hg, Pb and Zn violations
- 7 Acts as Interim President of LTWA providing support including agenda setting, issue research and collaboration
- 8 Newly permitted SW Wire is in compliance
- 9 All staff completed online version of HAZWOPER
- 10 Spoke to Norman Area Land Conservancy to look for projects for Phase II BOR funding through LTWA
- 11 Attended SW AWWA
- 12 Collaborated on grant application for EV chargers at City Hall-looks to be successful
- 13 Facilitated one LTWA Board Meeting (November 22, 2021).
- 14 Spoke to OU Landscape Architecture class about the proposed Matton Property Stormwater Park
- 15 Hired HHW Facility Coordinator and are continuing to work on the opening of the facility
- 16 Planning and Coordinating with Parks and Stormwater for Great American Cleanup
- 17 Facilitated NE Lions Park Cleanup for NNHS Stucco on December 18, 2021
- 18 Facilitated a Clean-up with Garver in Russell Bates Park on 11/19/21
- 19 For FYE 2022, the FOG Program was responsible for removing 226954 gallons of grease from entering the POTW

CITY OF NORMAN				
DEPARTMENT OF UTILITIES				
LINE MAINTENANCE DIVISION				
MONTHLY PROGRESS REPORT				
SEWER MAINTENANCE				
	FYE 22		FYE 21	
December, 2021	MONTH	YTD	MONTH	YTD
Obstructions:				
City Responsibility	0	7	1	4
Property Owner Responsibility	19	93	19	140
TOTAL	19	100	20	144
Number of Feet of Sewer Cleaned:				
Cleaned	104,770	704,421	69,895	491,959
Rodded	5,395	86,015	3,868	25,693
Foamed	0	74,476	0	81,695
SL-RAT	0	0	14,028	110,509
TOTAL	110,165	864,912	87,791	709,856
Sewer Overflows:				
Rainwater	0	0	0	0
Grease/Paper/Roots	0	0	0	0
Obstruction	0	3	1	2
Private	0	6	1	5
Other (Lift Station, Line Break, etc.)	0	0	0	0
Total Overflows	0	9	2	7
Feet of Sewer Lines Televised	24,581	147,157	17,441	116,023
Locates Completed	287	1,535	274	1,592
Manholes:				
Inspected	999	6,389	1,096	6,568
New	0	0	0	0
Rebuilt	0	0	0	0
Repaired	0	0	0	0
Feet of Sewer Lines Replaced/Repaired	0.00	49.00	8	17
Hours Worked at Lift Station	100.50	554.34	105	977
Hours Worked for Other Departments	17.25	230.25	3.25	434.91
OJI Percentage	1.00	0.17	0.00	0.00
Square Feet of Concrete	0	0	0	324
Average Response Time (Hours)	0.51	0.44	0.53	0.33
Claims Paid Per 10,000 People	0	0.0000	0	0.0000

CITY OF NORMAN				
DEPARTMENT OF UTILITIES				
LINE MAINTENANCE DIVISION				
MONTHLY PROGRESS REPORT				
WATER MAINTENANCE				
	FYE 22		FYE 21	
December, 2021	MONTH	YTD	MONTH	YTD
New Meter Sets:	51	256	75	330
Number Short Sets	51	250	75	328
Number Long Sets	0	5	0	2
Average Meter Set Time	2.94	3.92	3.77	4.75
Number of Work Orders:				
Service Calls	466	2,399	345	2,366
Meter Resets	0	2	1	5
Meter Removals	6	12	1	12
Meter Changes	55	233	52	319
Locates Completed	723	6,141	866	3,115
Number of Water Main Breaks	7	82	11	91
Average Time Water Off	2.00	1.91	1.65	2.06
Fire Hydrants:				
New	0	0	0	2
Replaced	1	1	2	5
Maintained	145	834	124	570
Number of Valves Exercised	189	1,056	162	1,122
Feet of Main Construction	447	447	0	1,795
Hours of Main Construction	369	531	157	1,929
Meter Changeovers	0	0	0	32
OJI Percentage	0.00	0.00	3.17	0.53
Hours Flushing/Testing New Mains	90.00	499	45	377
Hours Worked Outside of Division	0.75	226.25	15	564

City of Norman, Oklahoma  
Department of Utilities

Monthly Progress Report  
Water Reclamation Facility  
December 1-31, 2021  
**Flow Statistics**

	<b>FYE 2022</b>		<b>FYE 2021</b>	
	<u>This Month</u>	<u>YTD</u>	<u>This Month</u>	<u>YTD</u>
Total Influent Flow (M.G.)	283.1	2002.8	325.5	1982.0
Total Effluent Flow (M.G.)	262.7	1879.9	313.0	1945.1
Influent Peak Flow (MGD)	10.2	25.5	17.1	20.1
Effluent Peak Flow (MGD)	9.3	25.5	16.9	20.1
Daily Avg. Influent Flow (MGD)	9.4	11.1	10.5	10.8
Daily Avg. Effluent Flow (MGD)	8.5	10.1	10.1	10.6
Precipitation (inches)	0.3	4.8	3.2	13.5

**Discharge Monitoring Report Stats**

EPA minimum percentage removal 85%

5 day BOD:	Avg.	Avg.
Influent Total (mg/l)	164	134
Effluent Carbonaceous Total	2	2
Percent Removal	98.8	98.5
Total Suspended Solids:		
Influent (mg/L)	262	282
Effluent (mg/L)	5	4
Percent Removal	98.1	98.6
Dissolved Oxygen:		
Influent (min)	0.7	1.1
Effluent (min)	6.4	6.7
pH		
Influent (Low)	6.7	6.8
(High)	7.0	7.2
Effluent (Low)	6.7	6.9
(High)	7.1	7.1
Ammonia Nitrogen		
Influent (mg/L)	31.2	26.3
Effluent (mg/L)	0.1	0.3
Percent Removal	99.7	98.9

**Utilities****Electrical**

Total kWh Used (Plant wide)	547,720	3,111,300	574,840	2,965,020
Aeration Blowers	194,100	1,309,680	161,400	1,074,500
UV Facility	34,200	411,100	60,000	382,400

**Natural Gas**

Total cubic feet/day (plant wide)	355,000	2,168,000	749,000	2,669,000
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Public Education (Tours)	1	5	0	0
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Total Attendees for FYE 22	58		32	
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Reclaimed Water System (MG)	0.0	0.0	0.0	0.0
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OU Golf Course	2.6	47.7	0.9	42.1
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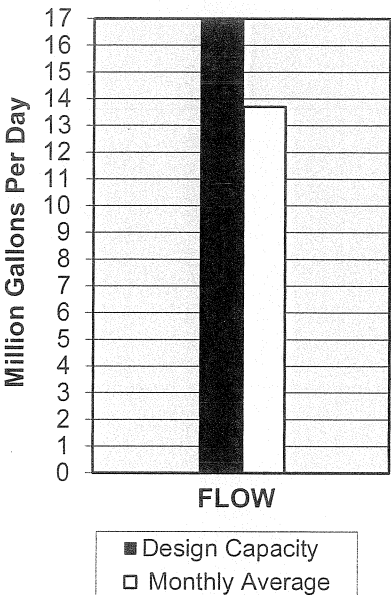
E.coli geometric mean average for December 2021 30 MPN (Limit is 630)



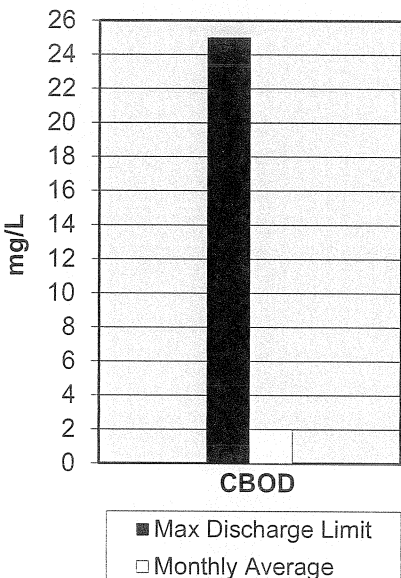
CITY OF NORMAN  
WATER RECLAMATION FACILITY  
December 2021

Item 4.

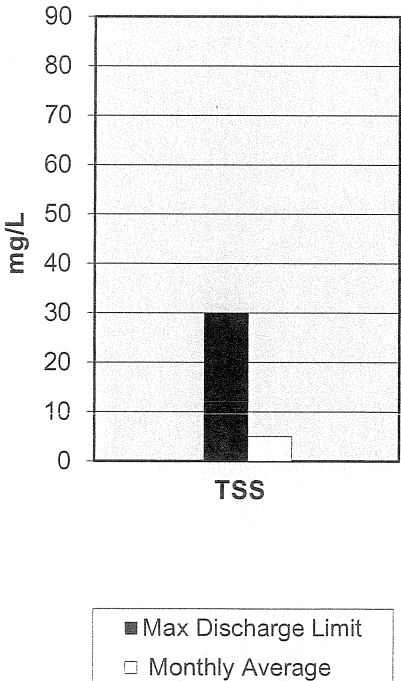
**Influent Flow  
Volume Treated**



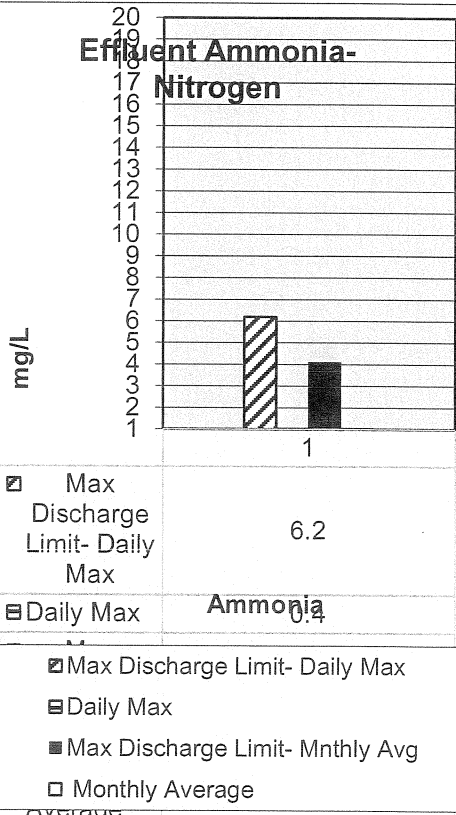
**Effluent CBOD:  
Strength of Wastewater**



**Effluent  
Total Suspended Solids**



**Effluent Ammonia-  
Nitrogen**



Comments here

**CITY OF NORMAN, OKLAHOMA  
DEPARTMENT OF UTILITIES  
MONTHLY PROGRESS REPORT**

**WATER TREATMENT DIVISION**

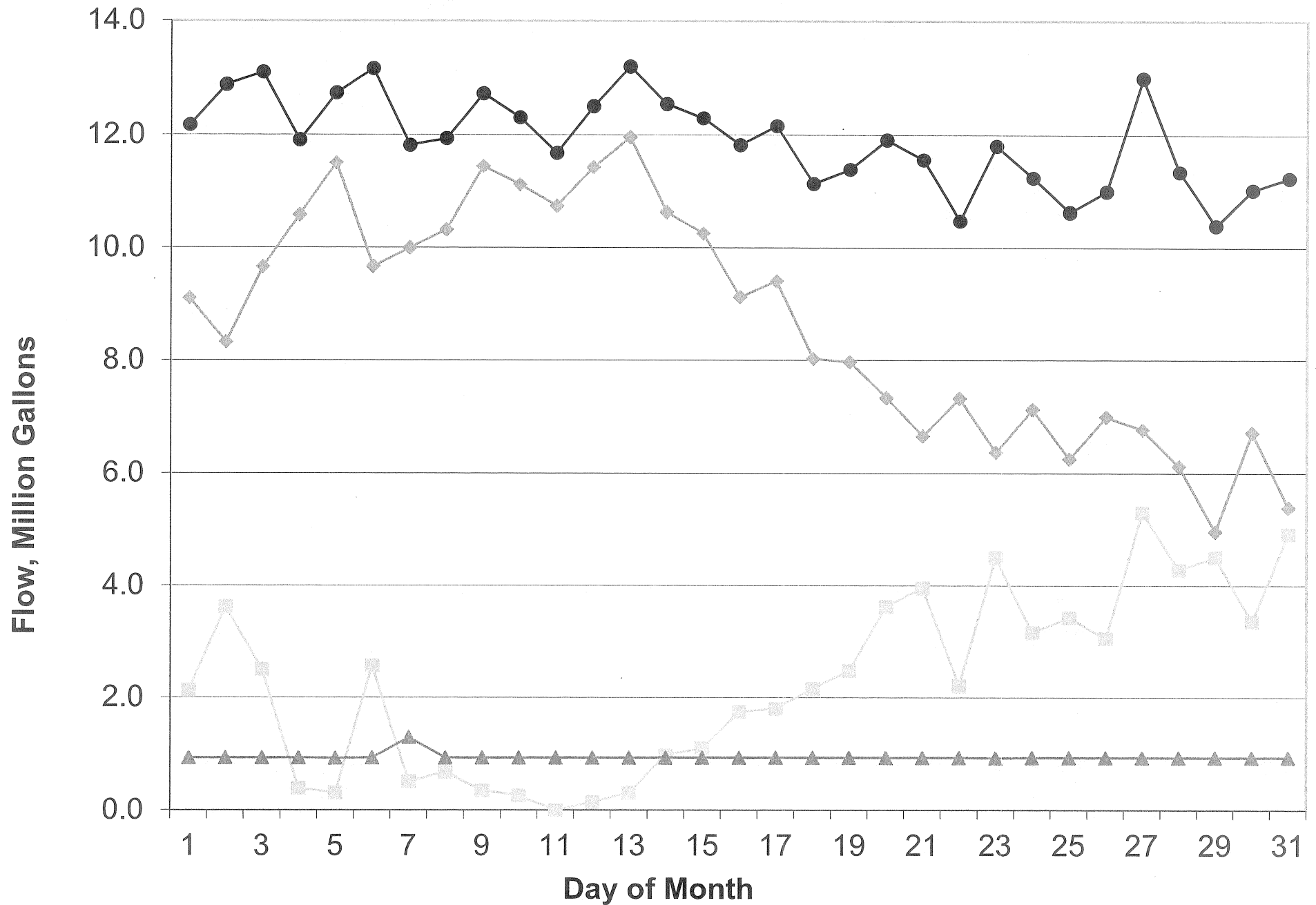
MONTH: December-2021

	<u>FYE 2022</u>		<u>FYE 2021</u>	
	<u>This month</u>	<u>Year to date</u>	<u>This month</u>	<u>Year to date</u>
<b>Water Supply</b>				
Plant Production (MG)	269.31	2128.31	197.01	2101.02
Well Production (MG)	70.35	513.03	97.84	474.98
Oklahoma City Water Used (MG)	29.39	193.58	30.80	185.93
Total Water Produced (MG)	369.05	2834.92	325.66	2761.92
Average Daily Production	11.90	15.41	10.51	15.01
<b>Peak Day Demand</b>				
Million Gallons	13.21	26.00	12.61	26.00
Date	12/13/2021	8/23/2020	12/7/2020	8/23/2020
System Capacity (see note 1)	23.35	23.35	23.35	23.35
Demand Above Capacity (Peak Day)	0.00	2.65	0.00	2.65
Note 1: Beginning June 2016 the System Capacity includes the Oklahoma City water line. (Plant + Wells + OKC)				
<b>Costs</b>				
Plant	\$667,449.35	\$4,134,309.18	\$759,408.26	\$4,417,913.38
Wells	\$206,713.95	\$1,321,533.75	\$213,136.82	\$1,290,436.30
OKC	\$76,794.05	\$454,508.75	\$77,493.65	\$505,370.52
Total	\$950,957.35	\$5,910,351.68	\$1,050,038.73	\$6,213,720.20
<b>Cost per Million Gallons</b>				
Plant	\$2,478.33	\$1,942.53	\$3,854.59	\$2,102.75
Wells	\$2,938.49	\$2,575.95	\$2,178.36	\$2,716.85
OKC	\$2,612.93	\$2,347.92	\$2,516.03	\$2,718.08
Total	\$2,576.76	\$2,084.84	\$3,224.37	\$2,249.78
<b>Water Quality</b>				
Total Number of Bacterial Samples	90	579	90	533
Bacterial Samples out of Compliance	0	7	0	1
Total number of inquiries (Note 2)	1	21	0	13
Total number of complaints (Note 2)	1	23	1	26
Number of complaints per 1000 service connections	0.02	0.57	0.02	0.64
Note 2: Prior to April 2016 complaints and inquiries were grouped together, listed as complaints, and not distinguished.				
<b>Safety</b>				
Hours lost to OJI	0	0	0	0
Hours lost to TTD	0	0	0	0
Total Hours Lost	0	0	0	0
Safety Training Sessions Held	0	3	0	0
<b>Public Education</b>				
Number of tours conducted	1	9	0	0
Number of people on tours	1	110	0	0

**Notes:**

Received solinoids for SCC 1&2 influent valves, scheduled to be installed.  
 Staff working with Utility engineers on clarifier 1 &2 study for rehab work and CO2 tank replacement  
 Staff working with Chart Industries to resolve LOX leak and venting issue

# Water Production for December 2021



Plant (MG) Well (MG) OKC (MG) Total (MG)

**CURBSIDE MONTHLY RECYCLING REPORT****DECEMBER****PROGRAM STATISTICS**

	<b>AVERAGE</b>
	<b>MONTH</b>
SET OUT/PARTICIPATION RATE:	91%
AVERAGE TONS PER DAY :	12.55
POUNDS PER HOME:	22.35

**COMMODITY BY TON**

	<b>% of Total</b>	<b>TONS</b>
ALUMINUM BEVERAGE CAN	1.96%	7.38
#1 PET	4.08%	15.36
NEWS	0.00%	0
GLASS CONTAINERS	10.07%	37.91
MIX PAPER	29.67%	111.71
PLASTIC FILM	0.60%	2.26
#2 NATURAL	1.11%	4.18
#2 COLOR	1.66%	6.25
#3-#7	0.00%	0
METAL	0.30%	1.13
RIGIDS	0.26%	0.98
TIN-STEEL SCRAP	2.14%	8.06
TRASH	27.91%	105.08
OCC	20.24%	76.21
<b>TOTAL</b>	<b>100.00%</b>	<b>376.51</b>

	<b>MONTH</b>
SERVICE CALLS (MISSES)	35
HOUSESIDE	3
REMINDER	5
SCATTERED	1
MISC.	1
REPAIR	11
NEW	44
ADD	7
MISSING	7
EXCHANGE	0
REPLACE	7
PICK UP	7
<b>TOTAL CALLS</b>	<b>128.00</b>

	<b>MONTH</b>
<b>LANDFILL COST AVOIDANCE</b>	<b>\$7,436.07</b>

**SANITATION DIVISION PROGRESS REPORT**  
**SUMMARY 2022**

	FYE 21		FYE 22	
	MONTH	YR-TO-DATE	MONTH	YR-TO-DATE
<u>Vehicle Accidents</u>	0	4	1	4
<u>On The Job Injuries</u>	0	8	0	2
<u>Bulk Pickups</u>	28	144	37	184
<u>Refuse Complaints</u>	120	598	100	550
<u>New Polycarts Requests</u>	70	409	66	348
<u>Polycarts Exchanges</u>	5	51	5	68
<u>Additional Polycart Requests</u>	59	387	79	535
<u>Replaced Stolen Polycarts</u>	21	142	44	160
<u>Replaced Damaged Polycarts</u>	79	558	62	548
<u>Polycarts Repaired</u>	29	228	50	283

**COMPOST MONTHLY REPORT**  
**DECEMBER**

	MONTH
TONS BROUGHT IN BY COMPOST CREWS:	303.87
LANDFILL TIPPING FEE'S	\$ 20.88
SAVINGS FROM NOT DUMPING AT LANDFILL:	\$ 6,344.81
TONS BROUGHT IN BY PUBLIC:	700.00
TONS BROUGHT IN BY CONTRACTORS :	1,900.00
TONS BROUGHT IN BY OTHER CITY DEPARTMENTS:	75.00
LANDFILL TIPPING FEE'S	\$ 20.88
SAVINGS FROM NOT DUMPING AT LANDFILL:	\$ 55,854.00
TOTAL SAVINGS FROM NOT DUMPING AT LANDFILL:	\$ 62,198.81
REVENUE COLLECTED FROM COMPOST SALES:	\$650.00
REVENUE COLLECTED FROM MULCH SALES:	\$0.00
TOTAL TONS COLLECTED	2,978.87

	MULCH CUBIC YDS MONTH	COMPOST CUBIC YDS MONTH
PARKS DEPT.		
ROAD & CHANNEL		
LINE MAINTENANCE		
STREET DEPT.		
WATER TREATMENT		
MURPHY PRODUCTS OKC		
SELF LOADING BIN		9
DRYING BEDS		
COMPOST SOLD BY CUBIC YARDS		195
MULCH LOADED BY CUBIC YARDS	135	
TOTAL:	135	204

## MONTHLY TRANSFER STATION REPORT

### December 2021

	TONS PER MONTH	REVENUE PER MONTH
O.U. TONS	223.20	\$11,261.20
STANDARD TONS	1,834.18	\$107,474.46
CASH TONS:	403.00	\$17,298.40
TOTALS:	2,460.38	\$136,034.06

	MONTH
# OF LOADS TRANSPORTED TO OKC LANDFILL BY TRANSFER STATION TRUCKS.	469.00

# OF TONS TRANSPORTED TO OKC LANDFILL BY TRANSFER STATION TRUCKS.	9017.19
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# OF LOADS TRANSPORTED TO OKC LANDFILL BY INDIVIDUAL SANITATION TRUCKS.	0.00
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# OF TONS TRANSPORTED TO OKC LANDFILL BY INDIVIDUAL SANITATION TRUCKS:	0.00
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TOTAL LOADS BROUGHT TO LANDFILLS:	469.00
GRAND TOTAL TONS TO LANDFILLS	9,017.19

DISPOSAL COST PER TON (OKC)	\$20.88
TIPPING FEE'S FOR DUMPING AT OKC:	\$188,278.93
GRAND TOTAL TIPPING FEE'S	\$188,278.93

# OF LOADS BROUGHT TO TRANSFER STATION BY COMMERCIAL SANITATION TRUCKS:	688.00
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# OF TONS BROUGHT TO TRANSFER STATION BY COMMERCIAL SANITATION TRUCKS:	3845.39
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# OF LOADS BROUGHT TO TRANSFER STATION BY RESIDENTIAL SANITATION TRUCKS:	454.00
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# OF TONS BROUGHT TO TRANSFER STATION BY RESIDENTIAL SANITATION TRUCKS:	2904.02
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TOTAL LOADS BROUGHT TO TRANSFER STATION:	1142.00
--	---------

TOTAL TONS BROUGHT TO TRANSFER STATION:	6749.41
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MISCELLANEOUS TONS BROUGHT BY OTHER DEPTS.:	130.09
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TOTAL TONS RECEIVED AT TRANSFER STATION	18357.07
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**File Attachments for Item:**

5. CONSIDERATION OF DECLARATION, APPROVAL, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF SURPLUS AND/OR OBSOLETE EQUIPMENT AND MATERIALS AND AUTHORIZING THE SALE OR DISPOSAL THEREOF.



## CITY OF NORMAN, OK STAFF REPORT

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**MEETING DATE:** 01/25/2022

**REQUESTER:** Frederick Duke

**PRESENTER:** Frederick Duke, Procurement Analyst

**ITEM TITLE:** CONSIDERATION OF DECLARATION, APPROVAL, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF SURPLUS AND/OR OBSOLETE EQUIPMENT AND MATERIALS AND AUTHORIZING THE SALE OR DISPOSAL THEREOF.

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**BACKGROUND:**

Section 8-301 of the Code of Ordinances of the City of Norman provides that “The City Council must declare surplus or obsolete any supplies, materials, or equipment where the valuation exceeds one thousand dollars (\$1,000) prior to the selling of such supplies, materials, or equipment.” The Auction will be conducted by Big Iron. Big Iron upon sale of any item sold will retain as total commission and compensation 7% of gross sales less sales tax. All items sold at auction are understood to be sold as is. The auction date is to be determined.

**DISCUSSION:**

The attached list is the supplies, materials, or equipment identified as surplus or obsolete, and is therefore being prepared for sale at auction.

**RECOMMENDATION:**

It is recommended that the attached list of supplies, materials, or equipment be declared surplus or obsolete, and approved for sale at public auction.



## INVENTORY OF GOODS TO BE AUCTIONED

Department Public Works - FleetSignature M. WhiteApproved by Fred Duke 11-16-24

Purchasing Agent

Condition	Year	Mileage/Hrs	Detailed Description	Serial Number/VIN	City Unit No.	Asset Tag No.	Dept/Div
Fair	2006	4066	ICX Sky Watch Tower	A060870396	1230SKY	N/A	PD
Good	2013	2000	CT660 Heavy Wrecker	1HSJKTKTDXJ405443	81	22711	PW Fleet
Fair	2008	4338	JD 5525	LV5525R454491	691	N/A	PW Streets
Fair	2012	66550	Chevy 3500	1GB3CZCG1CF101829	313	17765	Utilities/Water Line
Good	N/A	N/A	HP Printer Toner X6	N/A	N/A	N/A	PW Fleet
Good	N/A	N/A	PetroVend Key Encoder	1308558	N/A	N/A	PW Fleet
Fair	N/A	N/A	Frigidaire a/c window unit 5,000BTU Model:FRA052XT7	KK22144837	N/A	N/A	PW Fleet
Good	N/A	N/A	Wooden 6 drawer desk and side file cabinet	N/A	N/A	N/A	PW Fleet
Good	N/A	N/A	Bissell cordless vacuum cleaner w/battery and charger	Model:15203100N	N/A	N/A	PW Fleet
Fair	2010	5135	IHC 7600 Dump truck	1HTWYSJTXAJ279687	100	15300	PW Streets
Fair	2010	N/A	Monroe Plow	10/4/2747	100P		PW Streets
Good	2008	74946	Chevrolet Kodiak Patch Truck	1GBE4C3G78F408089	360	15078	Utilities/Sewer Line
Fair	2010	4760	John Deere 310SJ	1T0310SJKAA0190284	5327	N/A	Utilities/Water Line
Fair	2007	91487	Dodge Charger	2B3KA43G57H708421	1031	3-62879	PD criminal invest.
Fair	2013	161338	Chevrolet Tahoe	1GNLC2E0XDR130627	1158	19853	PD spec. Invest.
Fair	2006	90865	Dodge Charger	2B3KA43H26H335099	1161AA	14923	PD patrol
Poor	1996	4293	Ransomes Greensmower	96009060	777	N/A	Parks Westwood
Poor	2014	N/A	Jacobsen Greensmower	6222903973	712	N/A	Parks Westwood
Poor	2000	N/A	Diamond Greensmower	DT5245	715	N/A	Parks Westwood
Poor	1998	1584	Toro Triplex	3422	796	N/A	Parks Westwood
Poor	N/A	N/A	Rubbermaid Shed Kit	N/A	N/A	N/A	PW Fleet
Poor	N/A	N/A	Cushman Bed	N/A	N/A	N/A	Parks Westwood
Fair	N/A	N/A	Skid Steer Bucket	N/A	N/A	N/A	Parks Westwood
Poor	N/A	N/A	Marshall Saw Skid Steer Tree Cutter	N/A	N/A	N/A	Parks Westwood
Poor	2014	N/A	Red Ford F250 Tailgate	N/A	N/A	N/A	Fire Admin
Fair	N/A	N/A	Gravely Brush Cutter 1045 Pro	N/A	N/A	653	Parks Westwood
Fair	1996	169301	Dodge 1500	3B7HF13Z8TG173363	5000A	14994	City Clerk
Fair	2012	66550	Chevrolet 3500	1GB3CZCG1CF101829	313	17765	Utilities/Water Line
Fair	2010	6556	IHC 7600 Dump truck	1HTWYSJT6AJ244516	167	126981	PW Streets

Good	2006	126986	GMC Yukon	1GKEC13T66J118389	4	14826	Fire Prevention
Poor	2008	154720	Ford Crown Victoria	2FAFP71V68X176840	LEDT4	14932	PD Patrol
Poor	2019	1045	Vermeer TG5000	1VR240208K1001033	206	N/A	Sanitation Compost
Good	2018	N/A	Frigidaire 110V Window Unit A/C	KK81825820	N/A	N/A	PW Fleet
Good	2018	N/A	Frigidaire 110V Window Unit A/C	KK81825813	N/A	N/A	PW Fleet
Good	N/A	N/A	Wooden Desk	N/A	N/A	N/A	PW Fleet
Poor	2011	N/A	Chevrolet Tahoe Rear Differential	N/A	N/A	N/A	PD Patrol
Fair	N/A	N/A	Canon Image Runner Printer	(21)DRL55837	N/A	N/A	Fleet Admin
Fair	N/A	N/A	HP Color Laser Jet Printer	CNBC84N1L9	N/A	N/A	Fleet Admin

## INVENTORY OF GOODS TO BE AUCTIONED

Department Police  
Signature (S.T. G) (STGE)

Reviewed by Accounting Fred Voke  
11-16-21

[illegible]

\* Items stored by boiler room under overhang.

## INVENTORY OF GOODS TO BE AUCTIONED

Department Public Works, Traffic Control Division

Signature Dennis W. Davis 10/26/21

Approved by:

Fred Duke 11-16-21  
Purchasing Agent

[illegible]

**\*GOOD – FAIR – POOR – Please indicate if vehicles have been wrecked, burned or are in working order.**

## INVENTORY OF GOODS TO BE AUCTIONED

Department Public Works / Traffic Control

Signature J. Garcia

Reviewed by Accounting: Sped Webb 11-6-24

[illegible]

**File Attachments for Item:**

6. CONSIDERATION FOR APPROVAL, ACCEPTANCE, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF A FINAL SITE DEVELOPMENT PLAN AND FINAL PLAT FOR THE YOUNG FAMILY ATHLETIC COMPLEX, (YFAC), A PLANNED UNIT DEVELOPMENT, AND ACCEPTANCE OF PUBLIC DEDICATIONS CONTAINED THEREIN. (LOCATED 525 FEET EAST OF 24TH AVENUE N.W. AND SOUTH OF ROCK CREEK ROAD)



## CITY OF NORMAN, OK STAFF REPORT

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**MEETING DATE:** 01/18/2022

**REQUESTER:** Ken Danner, Subdivision Development Manager

**PRESENTER:** Shawn O'Leary, Director of Public Works

**TITLE:** CONSIDERATION FOR APPROVAL, ACCEPTANCE, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF A FINAL SITE DEVELOPMENT PLAN AND FINAL PLAT FOR THE YOUNG FAMILY ATHLETIC COMPLEX, (YFAC), A PLANNED UNIT DEVELOPMENT, AND ACCEPTANCE OF PUBLIC DEDICATIONS CONTAINED THEREIN. (LOCATED 525 FEET EAST OF 24TH AVENUE N.W. AND SOUTH OF ROCK CREEK ROAD)

---

### **BACKGROUND:**

This item is a final site development plan and final plat for YFAC, a Planned Unit Development located 525-feet east of 24<sup>th</sup> Avenue N.W. and south of Rock Creek Road. The property consists of 12.5 acres and one lot. The proposal is the Young Family Athletic Complex for the City of Norman.

City Council, at its meeting of December 12, 2006, adopted Ordinance No. O-0607-13 amending Planned Unit Development Narrative for University North Park.

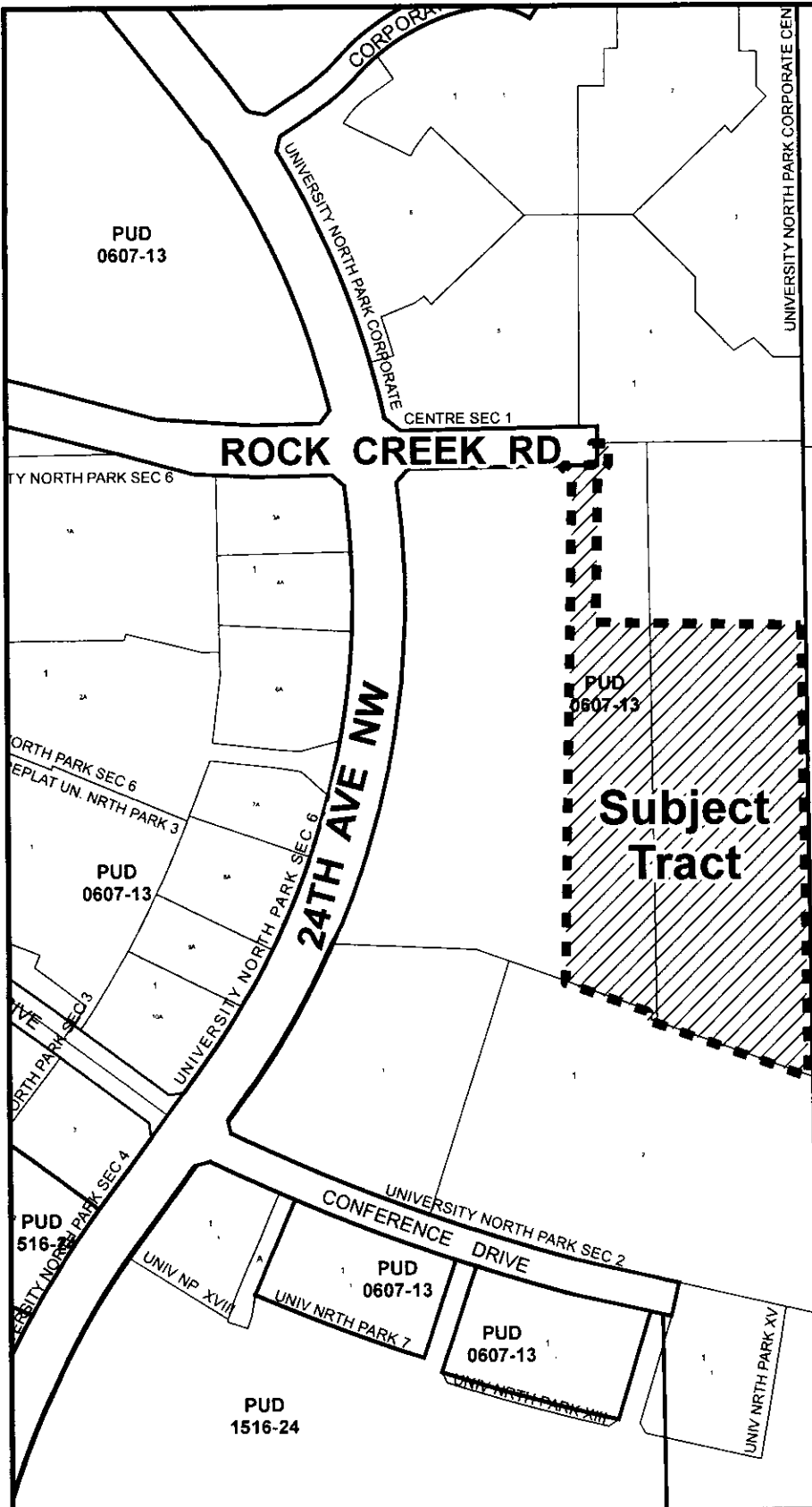
The Norman Development Committee, on December 7, 2021, approved the program of improvements, final site development plan and final plat and recommended the final site development plan and final plat for YFAC, a Planned Unit Development be submitted to City Council for consideration.

### **DISCUSSION:**

Construction plans have been reviewed for the required public improvements for this development. These improvements consist of water mains with fire hydrants, street paving and sidewalk improvements. Stormwater will be conveyed to an existing off-plat detention facility.

### **STAFF RECOMMENDATION:**

Based on the above information, staff recommends acceptance of the public dedications, approval of the final site development plan and final plat and filing of the final site development plan and final plat.



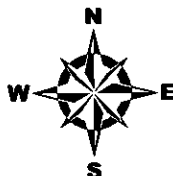
A-2  
1339

**Subject Tract**

# Location Map




Map Produced by the City of Norman  
Geographic Information System.  
The City of Norman assumes no  
responsibility for errors or omissions  
in the information presented.



December 3, 2021

0 200 400 Ft.

 Subject Tract

 Zoning





# FINAL PLAT OF YFAC ADDITION, A PLANNED UNIT DEVELOPMENT A PART OF THE N.E. 1/4 OF SEC. 23 AND THE N.W. 1/4 OF SECTION 24, T9N, R3W, I.M. NORMAN, CLEVELAND COUNTY, OKLAHOMA

**LEGEND OF ABBREVIATIONS**  
 U.P. UNPLATTED  
 U.P. UNPLATTED FOR THIS PLAT  
 U.P. UNPLATTED FOR THIS PLAT  
 U.P. UNPLATTED FOR THIS PLAT  
 U.P. UNPLATTED FOR THIS PLAT  
 U.P. UNPLATTED FOR THIS PLAT

**BENCHMARK:**  
 SET BRASS CAP  
 AT END OF CURVE AT  
 CORNER OF SECTION 23  
 OF PROPERTY.  
 ELEV. = 1181.74

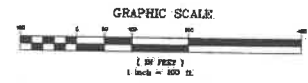
**NOTES:**

- All improvements within utility easements may be removed by the city to perform maintenance, and replacement of such improvements is the responsibility of the property owner.
- Easements shown herein by specific recording information are shown for reference purposes only and are not dedicated pursuant to this final plat.
- The basis of bearings for this survey was the Oklahoma State Plane Coordinate System (NAD83-South Zone) using a bearing of South 89°07'00" West on the north line of the Northeast Quarter of Section 23, Township 9 North, Range 3 West of the 10th Meridian.
- Unless otherwise stated all property corners are 1/2" iron pins with C&G301 caps or may note with C&G301 tags.

Line #	Direction	Length	Line #	Direction	Length
L1	S 89°57'17" E	7.50	L23	S 01°02'54" W	43.12
L2	S 89°57'17" E	15.00	L24	N 41°34'47" E	26.82
L3	S 01°02'54" W	176.89	L25	N 41°57'15" W	17.82
L4	N 89°57'17" W	22.50	L26	N 89°57'17" W	74.63
L5	S 01°02'54" W	345.88	L27	N 01°02'54" E	38.38
L6	N 01°02'54" E	67.87	L28	S 89°57'17" E	15.00
L7	S 78°25'58" E	15.82	L29	S 01°01'35" W	38.29
L8	S 48°18'44" E	66.40	L30	S 89°57'17" E	428.18
L9	S 78°25'58" E	156.80	L31	N 89°57'17" W	335.72
L10	S 28°34'01" W	38.18	L32	S 01°02'54" W	4.44
L11	S 84°24'02" W	5.28	L33	S 89°57'17" E	15.00
L12	S 78°25'58" E	22.52	L34	N 01°02'54" E	1.98
L13	S 30°11'00" E	235.10	L35	N 79°11'25" E	27.37
L14	N 87°18'25" E	45.10	L36	N 02°28'00" W	19.81
L15	N 47°18'25" E	24.79	L37	S 79°11'25" W	46.37
L16	N 02°28'00" W	105.51	L38	N 44°55'28" W	34.75
L17	S 89°21'58" W	37.80	L39	N 44°55'28" W	13.89
L18	N 02°28'00" W	197.28	L40	N 89°57'17" E	72.12
L19	N 44°55'28" W	32.42	L41	N 02°28'00" W	50.00
L20	N 89°57'17" W	429.22	L42	N 89°57'17" E	38.80
L21	S 01°02'54" W	5.99	L43	S 01°02'54" W	33.03
L22	S 01°02'54" W	18.21	L44	S 43°02'57" W	36.12

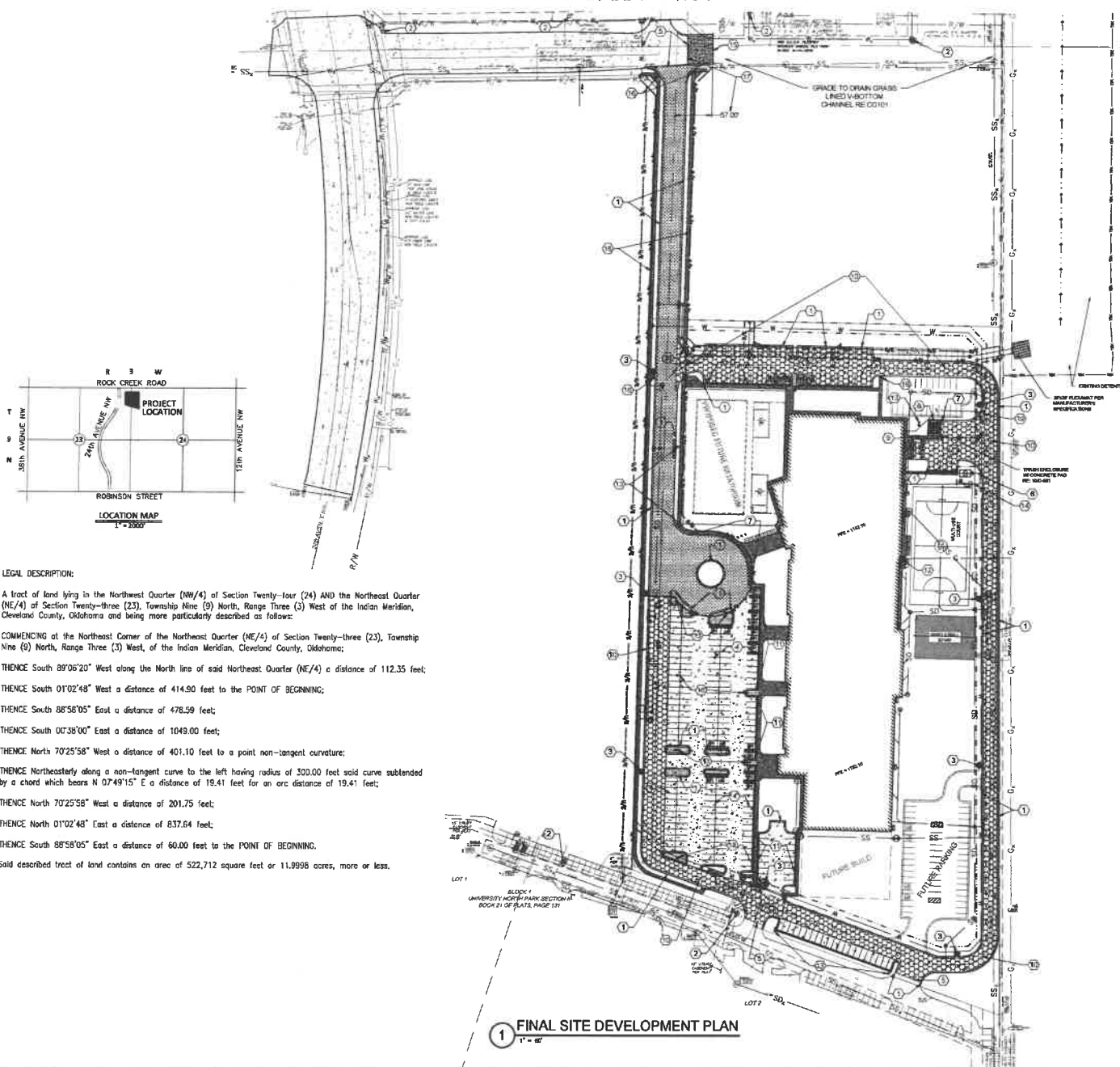
Curve #	Radius	Length	Delta	Tangent	Chord Bearing	Chord Length
C1	300.00'	18.41'	43°42'30"	9.71'	N 07°49'15" E	19.41'

NOTE: THE UNIT OF MEASUREMENT SHOWN FOR ALL DIMENSIONS ON THE FINAL PLAT IS FEET.



**DODSON - THOMPSON - MANSFIELD, PLLC**  
 20 W. Main Street, Norman, Oklahoma 73061  
 Telephone: 847-1000, 847-1001, 847-1002  
 Fax: 847-1003  
 Surveying - Engineering - Earthwork  
 CERTIFICATE OF AUTHORIZATION NO. 4201, EXPIRES JAN. 31, 2022

FINAL SITE DEVELOPMENT PLAN  
YFAC ADDITION, A PLANNED UNIT DEVELOPMENT  
A PART OF THE N.E. 1/4 OF SEC. 23 AND THE N.W. 1/4 OF SECTION 24, T24N, R3W, 14M,  
NORMAN, CLEVELAND COUNTY, OKLAHOMA



## GENERAL NOTES

- A. REFER TO SHEET VAD01 FOR BASIS OF VERTICAL AND HORIZONTAL CONTROL.  
B. REFER TO SHEET C-001 FOR ADDITIONAL NOTES AND REQUIREMENTS.  
C. DIMENSIONS SHOWN ARE TO FACE OF CURBSIDE OF PAVEMENT UNLESS OTHERWISE NOTED.  
D. CONTRACTOR SHALL EXERCISE CARE TO AVOID DAMAGE TO EXISTING PAVEMENT TO REMAIN. ANY DAMAGE TO EXISTING PAVEMENT TO REMAIN SHALL BE REPAIRED TO THE OWNER'S SATISFACTION AT THE CONTRACTOR'S EXPENSE.  
E. CONSTRUCTION FENCE SHALL BE PLACED ALONG THE PROJECT LIMIT LINE EXCEPT WHERE SHOWN OTHERWISE ON THIS PLAN. AREAS OUTSIDE THE CONSTRUCTION FENCE BUT INSIDE THE PROJECT LIMIT LINE SHALL BE COORDINATED IN CONSTRUCTION PHASING SO AS TO MINIMIZE OBSTRUCTION OF TRAFFIC AND PEDESTRIAN ACCESS. WORK AREAS SHOWN OUTSIDE THE LIMITS OF CONSTRUCTION FENCE SHOWN ON THIS PLAN SHALL BE PROTECTED BY CONSTRUCTION FENCE DURING THOSE TIMES WHEN WORK IS TAKING PLACE IN THE AFFECTED AREAS.  
F. THE CONTRACTOR SHALL PHASE AND COORDINATE THE WORK SO AS TO MINIMIZE THE OBSTRUCTION OF TRAFFIC AND PEDESTRIAN ACCESS TO ADJACENT FACILITIES.  
G. FOR CLARITY, JOINTING IS NOT SHOWN ON THIS PLAN.  
H. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH ADA REQUIREMENTS FOR ACCESS.  
I. REFER TO LANDSCAPING FOR IRRIGATION SLEEVE LOCATIONS.

## SHEET KEYNOTES

1. 6" CONCRETE CURB (TYP)
2. EXISTING FIRE HYDRANT
3. PROPOSED FIRE HYDRANT
4. PARKING LOT STRIPING, 4" WHITE (TYP.)
5. SAW CUT EXISTING PAVEMENT. REMOVE EXISTING PAVEMENT AND EXISTING CURB & GUTTER. PROTECT EDGE TO REMAIN. MEET & MATCH EXISTING PAVEMENT PER CITY OF NORMAN STD. RE: 16C-501, RE: 16C-501
6. 4" WATER METER VAULT, RE: CUI-101
7. CONCRETE BOLLARD
8. DUMPSTER ENCLOSURE RE: 10C-501
9. 8" FIRE LINE DOUBLE CHECK DETECTOR VAULT, RE: CUI-101
10. FIRE LANE W/STRIPING
11. CONCRETE WHEEL STOPS (TYP.) RE: 12C-501
12. GAS METER, RE: CUI-101
13. EXTERIOR LIGHT POLES, RE: E-101
14. 2" BRIGGATION LINE W/ METER, BACKFLOW PREVENTER W/ VAULT, AND IRRIGATION PUMP PAD.
15. ROAD CLOSED BARRICADES RE: C-506
16. ADA RAMPS PER CITY OF NORMAN STANDARDS
17. CONSTRUCT STREET TO PROPOSED DEAD END.
18. SIDEWALK INSIDE RIGHT-OF-WAY PER CITY OF NORMAN STANDARD.
19. FOG CONNECTION

## LEGEND

	EXISTING ASPHALT PAVING
	EXISTING CONCRETE PAVING
	ASPHALT ROAD DESIGN PER CITY OF NORMAN RE: 11C-501
	NEW LIGHT DUTY ASPHALT PAVING RE: 11C-501
	NEW HEAVY DUTY ASPHALT PAVING RE: 12C-501
	NEW CONCRETE SIDEWALK RE: 13C-501
	NEW CONCRETE STREET SECTION RE: 14C-501

City of Norman  
Young Family Athletics Center  
Norman, Oklahoma



NO.	DESCRIPTION	DATE
1	DESIGNED BY: RPK	
2	DRAWN BY: PSB	
3	REVIEWED BY: JOD	
4	PROJECT MANAGER: JOD	
5	PROJECT NUMBER: 2018-003-01	
6	SHEET NUMBER: 1	
7	FINAL SITE DEVELOPMENT PLAN	

DESIGNED BY:	RPK
DRAWN BY:	PSB
REVIEWED BY:	JOD
PROJECT MANAGER:	JOD
PROJECT NUMBER:	2018-003-01
SHEET NUMBER:	1
FINAL SITE DEVELOPMENT PLAN	

APPROVED DATE:	08/22/2021
APPROVED BY:	
CS101	

## DEVELOPMENT COMMITTEE

FINAL PLAT  
FP-2122-9

DATE:  
December 7, 2021----

### STAFF REPORT

**ITEM:** Consideration of a Final Plat for YFAC ADDITION, A PLANNED UNIT DEVELOPMENT.

**LOCATION:** Generally located 525-feet east of 24<sup>th</sup> Avenue N.W. on the south side of Rock Creek Road.

**INFORMATION:**

1. Owners. City of Norman and University Town Center L.L.C.
2. Developer. City of Norman.
3. Engineer. FSB Engineering.
4. Surveyor. Dodson-Thompson-Mansfield, PLLC

**HISTORY:**

1. October 18, 1961. City Council adopted Ordinance No. 1313 annexing this property into the Norman Corporate City limits without zoning.
2. December 19, 1961. Planning Commission recommended to City Council that this property be placed in A-2, Rural Agricultural District.
3. January 23, 1962. City Council adopted Ordinance No. 1339 placing this property in the A-2, Rural Agricultural District.
4. July 11, 2002. Planning Commission, on a vote of 5-0-2, recommended to City Council that the NORMAN 2020 Land Use and Transportation Plan be amended by designating this property as a Special Planning Area Designation and changing it from Industrial Designation.
5. July 11, 2002. Planning Commission, on a vote of 5-0-2, recommended to City Council that this property be placed in the PUD, Planned Unit Development and removed from A-2, Rural Agricultural District.
6. July 11, 2002. Planning Commission, on a vote of 5-0-2, recommended to City Council that the preliminary plat for University North Park Addition, a Planned Unit Development be approved.

7. August 13, 2002. City Council amended the NORMAN 2020 Land Use and Transportation Plan designating this property as a Special Planning Area.
8. August 13, 2002. City Council adopted Ordinance No. O-0203-2 placing this property in the PUD, Planned Unit Development, and removing it from A-2, Rural Agricultural District. The approval included the preliminary plat for University North Park Addition.
9. September 14, 2006. Planning Commission, on a vote of 7-0, recommended to City Council the amending of the Planned Unit Development Narrative.
10. September 14, 2006. Planning Commission, on a vote of 7-0, recommended to City Council that the preliminary plat for University North Park Addition, a Planned Unit Development be approved.
11. October 24, 2006. City Council postponed indefinitely Ordinance No. O-0607-13, amending the Planned Unit Development Narrative and Site Development Plan for University North Park Addition, a Planned Unit Development.
12. October 24, 2006. City Council postponed indefinitely the revised Preliminary Plat for University North Park Addition, a Planned Unit Development.
13. December 12, 2006. City Council approved Ordinance No. O-0607-13 amending the Planned Unit Development Narrative and Site Development Plan and approved the revised Preliminary Plat for University North Park Addition, a Planned Unit Development.

#### **IMPROVEMENT PROGRAM:**

1. Fire Hydrants. Fire hydrants will be installed in accordance with approved plans. Their locations have been approved by the Fire Department.
2. Permanent Markers. Permanent markers will be installed prior to the filing of the final plat.
3. Sanitary Sewers. A sanitary sewer main is existing.
4. Sidewalks. Sidewalks are required adjacent to Trae Young Court.
5. Storm Sewers. Storm water and appurtenant drainage structures will be installed in accordance with approved plans and City drainage standards. An off plat privately maintained detention pond will be utilized.
6. Streets. Trae Young Drive will be constructed in accordance with City paving standards.

**IMPROVEMENT PROGRAM (CON'T):**

7. Water Mains. Water lines will be installed in accordance with City and Oklahoma Department of Environmental Quality standards.

**PUBLIC DEDICATIONS:**

1. Easements. All required easements are dedicated to the City on the final plat.
2. Rights-of-Way. All street rights-of-way are dedicated to the City on the final plat.

**SUPPLEMENTAL MATERIAL:** Copies of a location map, final site development plan and final plat are attached.

**STAFF COMMENTS AND RECOMMENDATION:** The engineer for the developer has requested the Development Committee review and approve the program of public improvements, final site development plan and final plat for YFAC Addition, a Planned Unit Development and submit them to City Council for consideration.

This property consists of 12.5 acres and one (1) lot. The proposed use will be a Young Family Athletic Complex for the City of Norman.

APPLICATION FOR Final Plat  
DEVELOPMENT COMMITTEE ACTION

Date: 10/26/2021

Part I: To be Completed by Applicant:

1. Applicant(s): City of Norman

Signature of Applicant(s):

Telephone Number and Address: 405-366-5406, 201 W. Gray St, Norman, Oklahoma 73069

2. Project Name and Legal Description: YFAC ADDITION, A PLANNED UNIT DEVELOPMENT, part of the NE ¼ of Section 23 and the NW ¼ of Section 24, T9N, R3W, I.M., Norman, Cleveland County, Oklahoma
3. Action Request of Development Committee: Review and Approval  
Request the Development Committee approve the program of public improvements final site development plan and final plat for YFAC Addition, a Planned Unit Development.

Part II: To Be Completed by Development Committee:

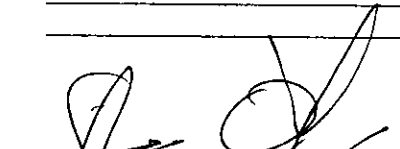
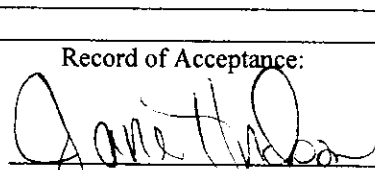
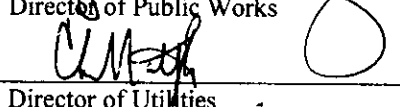
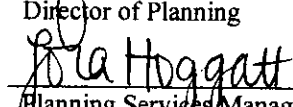
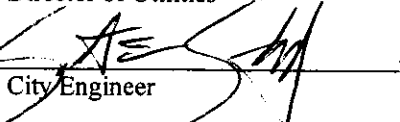
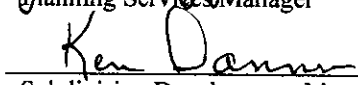
Development Committee Met on: December 7, 2021

Development Committee Findings:

Staff has requested the Development Committee approve the program of public improvements, final site development plan and final plat and submit the final site development plan and final plat to City Council for consideration.

Development Committee Recommendations:

Recommend submitting the final site development plan and final plat for YFAC a Planned Unit Development to City Council for consideration.

		Record of Acceptance:			
	Yes	No		Yes	No
 Director of Public Works	<input checked="" type="checkbox"/>	<input type="checkbox"/>	 Director of Planning	<input checked="" type="checkbox"/>	<input type="checkbox"/>
 Director of Utilities	<input checked="" type="checkbox"/>	<input type="checkbox"/>	 Planning Services Manager	<input checked="" type="checkbox"/>	<input type="checkbox"/>
 City Engineer	<input checked="" type="checkbox"/>	<input type="checkbox"/>	 Subdivision Development Manager	<input checked="" type="checkbox"/>	<input type="checkbox"/>

**File Attachments for Item:**

7. CONSIDERATION OF AWARDING, ACCEPTANCE, APPROVAL, ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF BID 2122-35, CONTRACT K-2122-75 BY AND BETWEEN THE CITY OF NORMAN, OKLAHOMA, AND SILVER STAR CONSTRUCTION COMPANY, INC., IN THE AMOUNT OF \$3,616,910.70, PERFORMANCE BOND B-2122-52, STATUTORY BOND B-2122-53, AND MAINTENANCE BOND MB-2122-41 FOR THE ALAMEDA STREET WIDENING CONSTRUCTION PROJECT FROM RIDGE LAKE BOULEVARD TO 48TH AVENUE S.E., RESOLUTION R-2122-66 GRANTING TAX EXEMPT STATUS, AND BUDGET TRANSFER AS OUTLINED IN THE STAFF REPORT.



## CITY OF NORMAN, OK STAFF REPORT

**MEETING DATE:** 1/25/2022

**REQUESTER:** Tim Miles, Capital Projects Manager

**PRESENTER:** Shawn O'Leary, Public Works Director

**ITEM TITLE:** CONSIDERATION OF AWARDING, ACCEPTANCE, APPROVAL, ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF BID 2122-35, CONTRACT K-2122-75 BY AND BETWEEN THE CITY OF NORMAN, OKLAHOMA, AND SILVER STAR CONSTRUCTION COMPANY, INC., IN THE AMOUNT OF \$3,616,910.70, PERFORMANCE BOND B-2122-52, STATUTORY BOND B-2122-53, AND MAINTENANCE BOND MB-2122-41 FOR THE ALAMEDA STREET WIDENING CONSTRUCTION PROJECT FROM RIDGE LAKE BOULEVARD TO 48TH AVENUE S.E., RESOLUTION R-2122-66 GRANTING TAX EXEMPT STATUS, AND BUDGET TRANSFER AS OUTLINED IN THE STAFF REPORT.

### BACKGROUND:

On August 28, 2012, the citizens of Norman voted in favor of a General Obligation Bond Issue to finance the local share of eight transportation/storm water improvement projects. One of the eight 2012 bond projects is the East Alameda Street Widening Project from Ridge Lake Boulevard to 48<sup>th</sup> Avenue S.E. (attached is a location map showing the project boundaries).

On December 11, 2012, the Norman City Council approved Programming Resolution R-1213-78 requesting federal funds for the East Alameda Street Project. This resolution states the City's commitment to adhere to the terms and conditions of a federally funded project including engineering design, acquisition of all necessary rights-of-way and relocation of utilities and encroachments at 100% the City's cost. In return, the Association of Central Oklahoma Governments (ACOG), through the Oklahoma Department of Transportation (ODOT), agrees to provide 80% of the construction cost, up to \$3,641,000, and administration of the construction with the matching share from the City of Norman.

Proposed improvements for the East Alameda Street Project include:

1. Widening East Alameda Street from 2 lanes to 5 lanes between Ridge Lake Boulevard and 36<sup>th</sup> Avenue S.E.
2. Add 10-foot shoulders to East Alameda Street between 36<sup>th</sup> Avenue S.E. and 48<sup>th</sup> Avenue S.E.



3. Intersection improvements at East Alameda Street/36<sup>th</sup> Avenue S.E. and East Alameda Street/48<sup>th</sup> Avenue S.E.
4. Continuous sidewalks on the south side of East Alameda Street to 36<sup>th</sup> Avenue S.E.
5. Storm water drainage improvements

On February 26, 2013, City Council approved engineering services Contract K-1213-160 with Cabbiness Engineering for the design of the Alameda Street Widening Project from Ridge Lake Boulevard to 48<sup>th</sup> Avenue S.E. in the amount of \$196,975.

On November 30, 2020, City Council approved Amendment One to Contract K-1213-160 with Cabbiness Engineering increasing the contract amount by \$22,500 for a revised contract amount of \$219,475, to provide additional design services on the Alameda Street Widening Project from Ridge Lake Boulevard to 48<sup>th</sup> Avenue S.E. Cabbiness Engineering was acquired by Garver, LLC in 2021 so Garver, LLC is now administering this contract.

## **DISCUSSION:**

Bid documents and specifications for the construction of the Alameda Street Widening Project from Ridge Lake Boulevard to 48<sup>th</sup> Avenue S.E. were advertised according to State Law. Four (4) potential contractors and sub-contractors acquired plans and specifications. Two (2) bids were received on Thursday, December 16, 2021.

The bid consisted of three parts including the Base Bid, Add Alternate No. 1 and Add Alternate No. 2. The Base Bid consisted of all bid items associated with the reconstruction of the urban portion of the Alameda Street Widening Project located from Ridge Lake Boulevard to east of 36<sup>th</sup> Avenue S.E. Add Alternate No. 1 consisted all bid items except for the asphalt pavement bid items of the rural portion of the Alameda Street Widening Project from east of 36<sup>th</sup> Avenue S.E. to east of 48<sup>th</sup> Avenue S.E. Add Alternate No. 2 consisted of the pavement bid items for the rural portion of the Alameda Street Widening Project from east of 36<sup>th</sup> Avenue S.E. to east of 48<sup>th</sup> Avenue S.E. The project bids were prepared this way because of limited budget availability.

The low bidder is Silver Star Construction, Inc., of Moore, Oklahoma, with a Base Bid of \$2,171,000, Add Alternate No. 1 Bid of \$1,445,910.70 and Add Alternate No. 2 Bid of \$1,192,289.30, for a total bid of \$4,809,200. This total bid is \$278,729 or 5.48% under the Engineer's Estimate with an estimated Base Bid of \$2,400,593; estimated Add Alternate No. 1 Bid of \$1,253,080.50; and estimated Add Alternate No. 2 Bid of \$1,434,255.50, for a total engineer's estimated bid of \$5,087,929. City staff has done a comparative analysis of these bids and believes the low bid is competitive and represents a fair bid. Silver Star Construction, Inc. has completed several similar construction projects in Norman and surrounding cities and has met the bidding requirements set forth in the bid documents.

City staff recommends accepting Silver Star's Base Bid of \$2,171,000 and Add Alternate No. 1 Bid of \$1,445,910.70 for a total of \$3,616,910.70. We also recommend rejecting the Add

Alternate No. 2 Bid of \$1,192,289.30. The City can save approximately \$342,000 if the Public Works/Streets Division completes the paving on the rural portion of this project. They routinely pave rural roads within city limits and have the capacity to complete this work.

This project is one of two remaining 2012 Bond Projects yet to be completed. Since the federal funding never materialized on this project through the Association of Central Oklahoma Governments (ACOG) or other federal programs, City staff is being resourceful to come up with the other 80% of the construction cost, including completing the rural paving with City forces.

Funding the construction portion of this project requires transferring \$2,452,000 from the 2016 Street Maintenance Bond Program Fund Balance and \$216,480 from the 2021 Street Maintenance Bond Program Fund Balance into the 2012 Alameda Street Widening Bond Project, Construction (Construction Account 50595552-46101; Project BP0190). All 2016 Street Bond Projects have now been completed, so \$2,452,000 of the excess funds are proposed to be used on the Alameda Street Project. The \$216,480 coming from the 2021 Street Fund Balance was specifically programmed for this project in the bond language. The remaining \$948,430.70 for this contract is already located in the 2012 Alameda Street Widening Bond Construction account (Account 50595552-46101; Project BP0190).

Per the contract documents, construction is anticipated to start on or near, March 1, 2022, with completion anticipated for the urban project in August, 2022, and the rural project in April, 2023.

#### **RECOMMENDATION No. 1:**

City staff recommends the transfer of \$2,452,000 from the 2016 Street Maintenance Bond Program Fund Balance (50-29000) and \$216,480 from the 2021 Street Maintenance Bond Program Fund Balance (50-29000) into the 2012 Alameda Street Widening Bond Project, Construction Account (50595552-46101; Project BP0190).

#### **RECOMMENDATION No. 2:**

City staff further recommends that the Base Bid and Add Alternate Bid No. 1 of Bid 2122-35 for the Alameda Street Widening Project from Ridge Lake Boulevard to 48<sup>th</sup> Avenue S.E. be awarded to the low bidder, Silver Star Construction, Inc. of Moore, Oklahoma in the amount of \$3,616,910.70.

#### **RECOMMENDATION No. 3:**

City Staff further recommends that Add Alternate Bid No. 2 of Bid 2122-35 for the surfacing portion of the rural Alameda Street Widening Project from east of 36<sup>th</sup> Avenue S.E. to east of 48<sup>th</sup> Avenue S.E. in the amount of \$1,192,289.30 be rejected.

#### **RECOMMENDATION No. 4:**

City Staff further recommends that, upon approval of Base Bid and Add Alternate No. 1 for Bid 2122-35, the following contracts and bonds be approved:

Contract K-2122-75  
Performance Bond B-2122-52  
Statutory Bond B-2122-53

Maintenance Bond MB-2122-41

**RECOMMENDTION No. 5:**

City Staff further recommends that, upon approval of Base Bid and Add Alternate No.1 for Bid 2122-35, Silver Star Construction, Inc. be authorized and appointed as Project Agent via Resolution R-2122-66 granting tax exempt status.



The City of  
**NORMAN**

201 West Gray, Bldg. A • P.O. Box 370  
Norman, Oklahoma 73069 • 73070

**PUBLIC WORKS DEPARTMENT**

Item 7.

**ENGINEERING DIVISION**

**Phone 405.366.5452**

**Fax 405.366.5418**

January 3, 2022

City of Norman

Mr. Shawn O'Leary, Director of Public Works

201 West Gray Street, Building A

Norman, OK 73069

**Re: Alameda Street Widening Project- Ridge Lake Boulevard to 48<sup>th</sup> Avenue S.E.  
Bid Opening**

Dear Mr. O'Leary,

Regarding the Alameda Street Widening Project, bids were opened on Thursday, December 16, 2021 at 2:00 pm at the City of Norman offices. Two bids were received and are summarized below along with the Engineer's Estimate:

Bidder Name	Location	Base Bid	Add Alternate No. 1	Add Alternate No. 2	Total Bid
Engineer's Estimate	N/A	\$2,400,593.00	\$1,253,080.50	\$1,434,255.50	\$5,087,929.00
Silver Star Construction	Moore, Oklahoma	\$2,171,000.00	\$1,445,910.70	\$1,192,289.30	\$4,809,200.00
Ellsworth Construction	Tulsa, Oklahoma	\$2,316,212.82	\$1,343,562.46	\$1,278,861.90	\$4,938,637.18

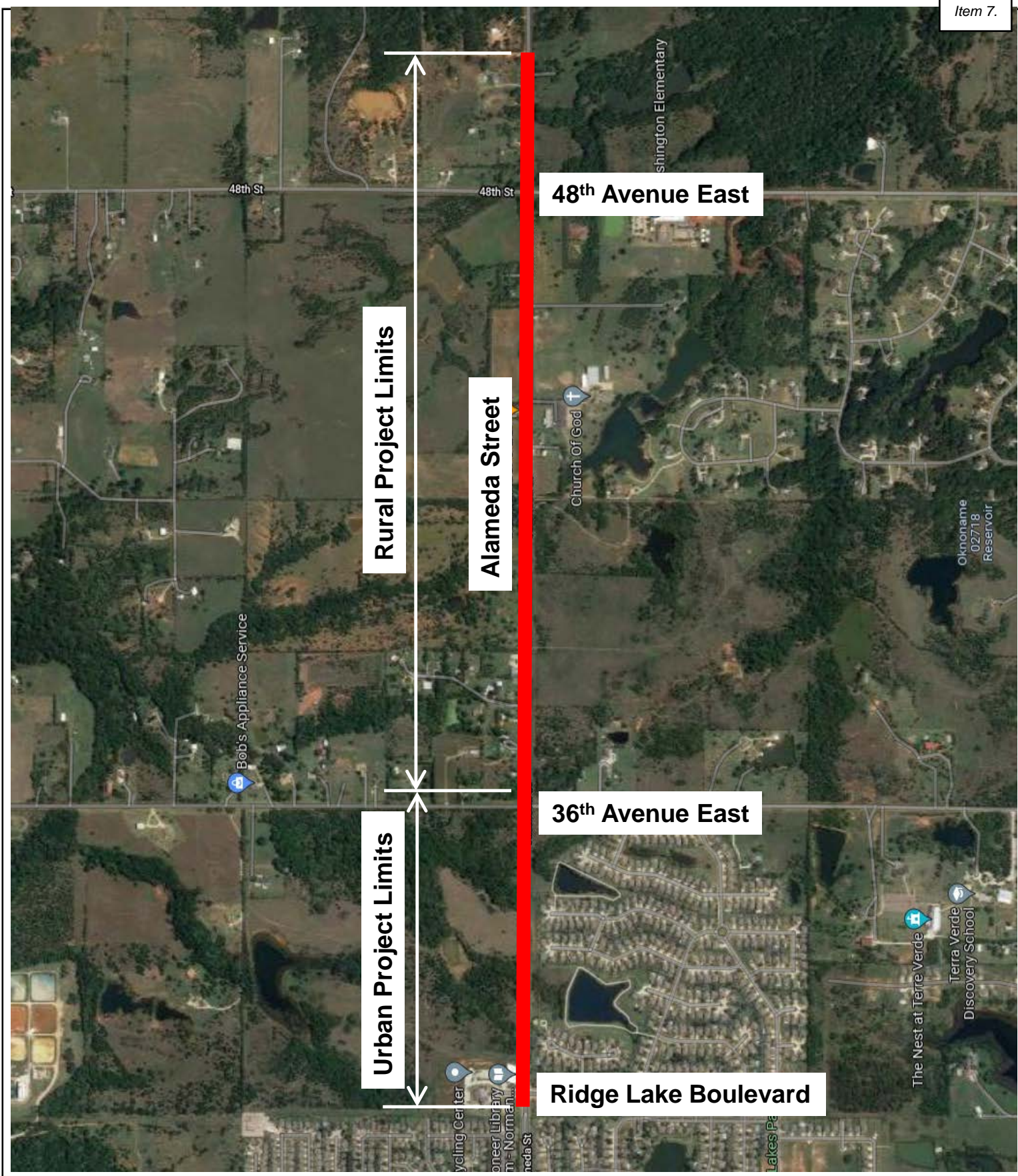
During the bid opening, Silver Star Construction, Inc. submitted the apparent low total bid in the amount of \$4,809,200.00, which is \$278,729.00 or 5.48% below the engineer's estimate. This bid has been reviewed for mathematical accuracy and evaluated to identify any potential irregularities, to which none were found.

After further evaluation of the Streets Department availability and costs to complete the Add Alternate No. 2 work, City staff recommends awarding only the Base Bid and Add Alternate No. 1 to Silver Star Construction, Inc. and having the Streets Department complete Add Alternate No. 2. This approach saves the City approximately \$342,000. As a result, City staff recommends awarding Contract No. K-2122-75 to Silver Star Construction, Inc. in the amount of \$2,171,000.00 for the Base Bid and \$1,445,910.70 for Add Alternate No.1 for a total amount of \$3,616,910.70, which is \$36,762.80 or 1.01% below the engineer's estimate.

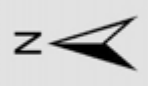
Respectfully,  
**City of Norman**

Tim Miles, PE  
Capital Projects Manager





# Alameda Street Widening Project Location Map





# C O N T R A C T

THIS CONTRACT made and entered into this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_, by and between Silver Star Construction Company, Inc. as Party of the First Part, hereinafter designated as the CONTRACTOR, and the City of Norman, a municipal corporation, hereinafter designated as the CITY, Party of the Second Part.

## W I T N E S S E T H

WHEREAS, the CITY has caused to be prepared in accordance with law, specifications, and other bidding documents for the work hereinafter described and has approved and adopted all of said bidding documents, and has caused Notice to Bidders to be given and advertised as required by law, and has received sealed proposals for the furnishing of all labor and materials for the following projects:

### **BID 2122-35 ALAMEDA STREET WIDENING PROJECT** **(RIDGE LAKE BOULEVARD TO 48TH AVENUE SE)**

as outlined and set out in the bidding documents and in accordance with the terms and provisions of said CONTRACT; and,

WHEREAS, the CONTRACTOR in response to said Notice to Bidders, has submitted to the CITY in the manner and at the time specified, a sealed proposal in accordance with the terms of this Contract; and,

WHEREAS, the CITY, in the manner provided by law, has publicly opened, examined, and canvassed the proposals submitted and has determined and declared the above-named CONTRACTOR to be the lowest and best Bidder on the above-prepared project, and has duly awarded this CONTRACT to said CONTRACTOR, for the sum named in the proposal, to wit:

(WRITTEN) Three Million Six Hundred Sixteen Nine Hundred Ten and 70/100 (DOLLARS):

(NUMERALS) (\$ 3,616,910.70 ).

NOW, THEREFORE, for and in consideration of the mutual agreements and covenants herein contained, the parties to this CONTRACT have agreed, and hereby agree, as follows:

1) The CONTRACTOR shall, in a good and first-class, workman-like manner at his own cost and expense, furnish all labor, materials, tools, and equipment required to perform and complete said work in strict accordance with this CONTRACT and the following CONTRACT Documents: The Bid Notice published in the Norman Transcript, the Notice to Bidders, Instructions to Bidders, the Contractor's Bid or Proposal, the Construction Drawings, Specifications, Provisions, and Bonds thereto, all of which documents are on file in the Office of the Purchasing Agent of the City of Norman, and are made a part of this CONTRACT as fully as if the same were set out at length.

2) The CITY shall make payments as stipulated in the contract documents to the CONTRACTOR in the following manner: On or about the first day of each month, the project engineer, or other appropriate person, will make accurate estimates of the value, based on CONTRACT prices, or work done, and materials incorporated in the work and of materials suitably stored at the site thereof during the preceding calendar month. The CONTRACTOR shall furnish to the project engineer, or other appropriate person, such detailed information as he may request to aid him as a guide in the preparation of the monthly estimates.

Contract No. K-2122-75  
Page 1 of 4

Each monthly estimate for payment must contain or have attached an affidavit in accordance with the Constitution of the State of Oklahoma, Title 62, Section 310.9.

On completion of the work, but prior to the acceptance thereof by the CITY, it shall be the duty of the project engineer, or other appropriate person, to determine that said work has been completely and fully performed in accordance with said CONTRACT Documents; and upon making such determinations, said official shall make his final certificate to the CITY.

The CONTRACTOR shall furnish proof that all claims and obligations incurred by him in connection with the performance of said work have been fully paid and settled; said information shall be in the form of an affidavit, which shall bear the approval of the surety on the CONTRACT Bonds for payment of the final estimate to the CONTRACTOR; thereupon, the final estimate (including retainages) will be approved and paid.

3) It is further agreed that the CONTRACTOR will commence said work within 10 days following receipt of a NOTICE-TO-PROCEED, and prosecute the same vigorously and continuously. Any suspension of work must be approved by the engineer or the engineer's representative. The contract period is as follows:

**BID 2122-35 ALAMEDA STREET WIDENING PROJECT**  
**(RIDGE LAKE BOULEVARD TO 48<sup>TH</sup> AVENUE SE)**

1. Base Bid contract time: 157 Calendar Days
2. Add Alternate Nos. 1 and 2 contract time: 265 Calendar Days

Notes: Calendar Days do not include weather days. Weather days to be determined by the engineer or construction manager. Work cannot begin on Add Alternate No.1 until the Base Bid Project is completed and open to traffic. **THIS PROJECT WILL HAVE A DELAYED START DATE OF TUESDAY, MARCH 1, 2022, TO ALLOW FOR UTILITY RELOCATIONS TO BE COMPLETED.**

- 4) That the CITY shall pay the CONTRACTOR for the work performed as follows:
- a. Payment for unit price items shall be at the unit price bid for actual construction quantities.
  - b. Construction items specified but not included as bid items shall be considered incidental and shall not be paid for directly, but shall be included in the bid price for any or all of the pay quantities. Should any defective work or materials be discovered or should a reasonable doubt rise as to the quality of any work completed, there will be deducted from the next estimate an amount equal to the value of the defective or questionable work and shall not be paid until the defects are remedied.

And that the CONTRACTOR'S bid is hereby made a part of this Agreement.

- 5) The amount of retainage with respect to progress payments will be 5%.
- 6) That the CONTRACTOR will not undertake to furnish any materials or to perform any work not specifically authorized under the terms of this Agreement unless additional materials or work are authorized by written Change Order, executed by the CITY; and that in the event any additional are provided by the CONTRACTOR without such authorization, the CONTRACTOR shall not be entitled to any compensation therefore whatsoever.

Contract No. K-2122-75  
Page 2 of 4



7) That if any additional work is performed or additional materials provided by the CONTRACTOR upon authorization by the CITY, the CONTRACTOR shall be compensated therefore at the unit price and as agreed to by both parties in the execution of the Change Order.

8) That the CONTRACTOR shall perform the work and provide the materials strictly in accordance with the specifications as to quality and kind, and all work and materials shall be subject to rejection by the CITY through its authorized representatives for failure to meet such requirements, and in the event of such rejection, the CONTRACTOR shall replace the work and materials without compensation therefore by the CITY.

9) The CONTRACTOR shall complete the work in accordance with the terms of this Agreement. The CONTRACTOR further agrees to pay liquidated damages, as stipulated in the contract document and the General Conditions included in the City of Norman Standard Specifications and Construction Drawings, for each calendar day thereafter.

10) The CONTRACTOR shall furnish surety bonds and certificate of insurance as specified herein which bonds and insurance must be approved by the CITY prior to issuance of the Work Order and commencement of work on the project. The CONTRACTOR shall provide written documentation from the Maintenance Bond Company that all work, including Change Orders, is covered by the Maintenance Bond before final acceptance of the project.

11) IN WITNESS WHEREOF, the parties hereto have caused this instrument to be executed, in four (4) duplicate originals, the day and year first above written.

12) To that end, no provision of this CONTRACT or of any such aforementioned document shall be interpreted or given legal effect to create an obligation on the part of the CITY to third persons, including, by way of illustration but not exclusion, sureties upon performance bonds, payment bonds or other bonds, assignees of CONTRACTOR, subcontractors, and persons performing labor, furnishing material or in any other way contributing to or assisting in the performance of the obligations of the CONTRACTOR; nor shall any such provisions be interpreted or given legal effect to afford a defense against any obligation owed or assumed by such third person to the CITY or in any way to restrict the freedom of the third person to the CITY or in any way to restrict the freedom of the CITY to exercise full discretion in its dealing with the Contractor.

13) The sworn, notarized statement below must be signed and notarized before this Contract will become effective.

STATE OF Oklahoma )  
COUNTY OF Cleveland ) ss:

Craig Parker, of lawful age, being first duly sworn, on oath says that (s)he is the agent authorized by CONTRACTOR to submit the above CONTRACT to the CITY. Affidavit further states that CONTRACTOR has not paid, given or donated or agreed to pay, give, or donate to any officer or employee of the CITY any money or other thing of value, either directly or indirectly, in the procuring of the CONTRACT.

  
Craig Parker Contractor President

Contract No. K-2122-75  
Page 3 of 4



Subscribed and sworn to before me this 5<sup>th</sup> day of January, 2022



T. Madden  
Notary Public

IN WITNESS WHEREOF, the said parties of the First and Second Part have hereunto set their hands and seals respectively the \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_, and the \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_.

(Corporate Seal) (where applicable)

ATTEST:  
Authorized Representative

[Signature]  
Corporate Secretary (where applicable)  
*Exec. Vice President*

Principal Silver Star Construction Company, Inc.

Signed: [Signature]

Title: President

Address 2401 S. Broadway, Moore, Ok 73160

Telephone: 405-793-1725

CITY OF NORMAN:

Approved as to form and legality this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_.

\_\_\_\_\_  
City Attorney

Approved by the Council of the City of Norman, this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_.

ATTEST:

\_\_\_\_\_  
City Clerk

\_\_\_\_\_  
Mayor

CONTRACT AFFIDAVIT

STATE OF Oklahoma )  
COUNTY OF Cleveland ) ss:

Craig Parker, of lawful age, being first duly sworn, on oath says that (s)he is the Agent authorized by the Firm of Silver Star Construction Company, Inc. to submit the above Contract to the City of Norman, Oklahoma.

Affidavit further states that such firm has not paid, given or donated or agreed to pay, give, or donate to any officer or employee of the City of Norman, Oklahoma, any money or other thing of value, either directly or indirectly, in the procuring of the Contract.

Craig Parker Contractor President

Subscribed and sworn to before me this 5<sup>th</sup> day of January, 2022

Terri Madden  
Notary Public

My Commission Expires:

03/05/2023



Know all men by these presents, that Silver Star Construction Company, Inc. as PRINCIPAL, and The Ohio Casualty Insurance Company Corporation organized under the laws of the State of New Hampshire and authorized to transact business in the State of Oklahoma, as SURETY, are held and firmly bound unto THE CITY OF NORMAN, a Municipal Corporation of the State of Oklahoma, herein called CITY, in the sum of Three Million Six Hundred Sixteen Thousand Nine Hundred Ten and 70/100 DOLLARS, (\$3,616,910.70 ), for the payment of which sum PRINCIPAL and SURETY bind themselves, their heirs, executors, administrators, successors and assigns jointly and severally.

WHEREAS, the conditions of this obligation are such that the PRINCIPAL, being the lowest and best Bidder on the following PROJECT:

**BID 2122-35 ALAMEDA STREET WIDENING PROJECT**  
**(RIDGE LAKE BOULEVARD TO 48<sup>TH</sup> AVENUE SE)**

has entered into a written CONTRACT (K-2122-75) with THE CITY OF NORMAN, dated this 4th day of January, 20 22 for the erection and construction of this PROJECT, that CONTRACT being incorporated herein by reference as if fully set forth.

NOW, THEREFORE, if PRINCIPAL shall, in all particulars, well and truly perform and abide by said CONTRACT and all specifications and covenants thereto; and if the PRINCIPAL shall promptly pay or cause to be paid all indebtedness incurred for labor and materials and repairs to and parts for equipment furnished in the making of this PROJECT, whether incurred by the PRINCIPAL or subcontractors; and if the PRINCIPAL shall protect and hold harmless the CITY from all loss, damage, and expense to life or property suffered or sustained by any person, firm, or corporation caused by PRINCIPAL or his or its agents, servants, or employees in the construction of the PROJECT, or by or in consequence of any negligence, carelessness or misconduct in guarding and protecting the same, or from any act or omission of PRINCIPAL of his or its agents, servants, or employees; and if the PRINCIPAL shall protect and save the CITY harmless from all suits and claims of infringement or alleged infringement or patent rights or processes, then this obligation shall be null and void. Otherwise, this obligation shall remain in full force and effect.

It is further expressly agreed and understood by the parties hereto that no changes or alterations in the CONTRACT and no deviations from the plan or mode of procedure herein fixed shall have the effect of releasing the sureties, or any of them, from the obligations of this Bond.

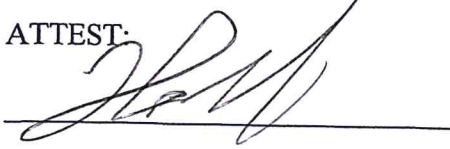
It is further expressly agreed that the Principal's obligations under this Bond include payment of not less than the prevailing hourly rate of wages as established by the Commissioner of Labor of the State of Oklahoma and by the Secretary of the U.S. Department of Labor or as determined by a court on appeal.



IN WITNESS WHEREOF, the PRINCIPAL has caused these presents to be executed in its name and its corporate seal (where applicable) to be hereunto affixed by its duly authorized representative(s), and the 04 day of January, 2022 and the SURETY has caused these presents to be executed in its name and its corporate seal to be hereunto affixed by its authorized representative(s) on the day of January 04, 2022.

(Corporate Seal) (where applicable)

ATTEST:

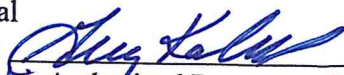


Corporate Secretary (where applicable)  
Exec. Vice President  
Tim Caudle

(Corporate Seal) (where applicable)

ATTEST:



Silver Star Construction Co.  
Inc.  
Principal  
Signed:   
Greg Kalicki Authorized Representative  
Title: Vice President

Address: 2401 S. Broadway  
Moore, OK 73160

Telephone: 405-793-1725

Surety: The Ohio Casualty Insurance Company

Signed:   
Authorized Representative

Printed: Russell Hollingsworth  
Authorized Representative

Title: Attorney-In-Fact

Address: 2402 W Willow Enid, OK 73703

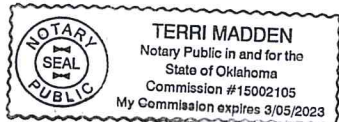
Telephone: 580-233-2000

**CORPORATE ACKNOWLEDGEMENT**

STATE OF Oklahoma )  
 ) ss:  
 COUNTY OF Cleveland )

The foregoing instrument was acknowledged before me this 5<sup>th</sup> day of January, 20 22 by Greg Kahaki - Vice President (Name and Title), of Silver Star Construction Co. Inc, a(n) corporation, on behalf of the corporation.

WITNESS my hand and seal this 5<sup>th</sup> day of January, 20 22.



T. Madden  
 Notary Public

My Commission Expires: 03/05/2023

**INDIVIDUAL ACKNOWLEDGEMENT**

STATE OF \_\_\_\_\_ )  
 ) ss:  
 COUNTY OF \_\_\_\_\_ )

The foregoing instrument was acknowledge before me this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_, by \_\_\_\_\_ (Name and Title) of \_\_\_\_\_, a(n) corporation.

WITNESS my hand and seal this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_.

\_\_\_\_\_  
 Notary Public

My Commission Expires:  
 \_\_\_\_\_

**PARTNERSHIP ACKNOWLEDGEMENT**

STATE OF \_\_\_\_\_ )  
 ) ss:  
 COUNTY OF \_\_\_\_\_ )

The foregoing instrument was acknowledge before me this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_,  
 by \_\_\_\_\_ (Name and Title) \_\_\_\_\_  
 (partner/agent) on behalf of \_\_\_\_\_, a partnership.

WITNESS my hand and seal this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_.

\_\_\_\_\_  
 Notary Public

My Commission Expires:  
 \_\_\_\_\_

**CITY OF NORMAN**

Approved as to form and legality this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_.

\_\_\_\_\_  
 City Attorney

Approved by the Council of the City of Norman this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_.

ATTEST:

\_\_\_\_\_  
 City Clerk

\_\_\_\_\_  
 Mayor





This Power of Attorney limits the acts of those named herein, and they have no authority to bind the Company except in the manner and to the extent herein stated.

Item 7.

Liberty Mutual Insurance Company  
The Ohio Casualty Insurance Company  
West American Insurance Company

Certificate No: **8203689 - 986746**

## POWER OF ATTORNEY

KNOWN ALL PERSONS BY THESE PRESENTS: That The Ohio Casualty Insurance Company is a corporation duly organized under the laws of the State of New Hampshire, that Liberty Mutual Insurance Company is a corporation duly organized under the laws of the State of Massachusetts, and West American Insurance Company is a corporation duly organized under the laws of the State of Indiana (herein collectively called the "Companies"), pursuant to and by authority herein set forth, does hereby name, constitute and appoint, Jason Blair, Debra Cornelsen, John E. Dillingham, Mike Haselden, Russell Hollingsworth, Amber Jarman, Tara L. Ramos

all of the city of Enid state of OK each individually if there be more than one named, its true and lawful attorney-in-fact to make, execute, seal, acknowledge and deliver, for and on its behalf as surety and as its act and deed, any and all undertakings, bonds, recognizances and other surety obligations, in pursuance of these presents and shall be as binding upon the Companies as if they have been duly signed by the president and attested by the secretary of the Companies in their own proper persons.

IN WITNESS WHEREOF, this Power of Attorney has been subscribed by an authorized officer or official of the Companies and the corporate seals of the Companies have been affixed thereto this 11th day of May, 2020.



Liberty Mutual Insurance Company  
The Ohio Casualty Insurance Company  
West American Insurance Company

By: David M. Carey  
David M. Carey, Assistant Secretary

State of PENNSYLVANIA ss  
County of MONTGOMERY

On this 11th day of May, 2020 before me personally appeared David M. Carey, who acknowledged himself to be the Assistant Secretary of Liberty Mutual Insurance Company, The Ohio Casualty Company, and West American Insurance Company, and that he, as such, being authorized so to do, execute the foregoing instrument for the purposes therein contained by signing on behalf of the corporations by himself as a duly authorized officer.

IN WITNESS WHEREOF, I have hereunto subscribed my name and affixed my notarial seal at King of Prussia, Pennsylvania, on the day and year first above written.



COMMONWEALTH OF PENNSYLVANIA  
Notarial Seal  
Teresa Pastella, Notary Public  
Upper Merion Twp., Montgomery County  
My Commission Expires March 28, 2021  
Member, Pennsylvania Association of Notaries

By: Teresa Pastella  
Teresa Pastella, Notary Public

This Power of Attorney is made and executed pursuant to and by authority of the following By-laws and Authorizations of The Ohio Casualty Insurance Company, Liberty Mutual Insurance Company, and West American Insurance Company which resolutions are now in full force and effect reading as follows:

### ARTICLE IV – OFFICERS: Section 12. Power of Attorney.

Any officer or other official of the Corporation authorized for that purpose in writing by the Chairman or the President, and subject to such limitation as the Chairman or the President may prescribe, shall appoint such attorneys-in-fact, as may be necessary to act in behalf of the Corporation to make, execute, seal, acknowledge and deliver as surety any and all undertakings, bonds, recognizances and other surety obligations. Such attorneys-in-fact, subject to the limitations set forth in their respective powers of attorney, shall have full power to bind the Corporation by their signature and execution of any such instruments and to attach thereto the seal of the Corporation. When so executed, such instruments shall be as binding as if signed by the President and attested to by the Secretary. Any power or authority granted to any representative or attorney-in-fact under the provisions of this article may be revoked at any time by the Board, the Chairman, the President or by the officer or officers granting such power or authority.

### ARTICLE XIII – Execution of Contracts: Section 5. Surety Bonds and Undertakings.

Any officer of the Company authorized for that purpose in writing by the chairman or the president, and subject to such limitations as the chairman or the president may prescribe, shall appoint such attorneys-in-fact, as may be necessary to act in behalf of the Company to make, execute, seal, acknowledge and deliver as surety any and all undertakings, bonds, recognizances and other surety obligations. Such attorneys-in-fact subject to the limitations set forth in their respective powers of attorney, shall have full power to bind the Company by their signature and execution of any such instruments and to attach thereto the seal of the Company. When so executed such instruments shall be as binding as if signed by the president and attested by the secretary.

**Certificate of Designation** – The President of the Company, acting pursuant to the Bylaws of the Company, authorizes David M. Carey, Assistant Secretary to appoint such attorneys-in-fact as may be necessary to act on behalf of the Company to make, execute, seal, acknowledge and deliver as surety any and all undertakings, bonds, recognizances and other surety obligations.

**Authorization** – By unanimous consent of the Company's Board of Directors, the Company consents that facsimile or mechanically reproduced signature of any assistant secretary of the Company, wherever appearing upon a certified copy of any power of attorney issued by the Company in connection with surety bonds, shall be valid and binding upon the Company with the same force and effect as though manually affixed.

I, Renee C. Llewellyn, the undersigned, Assistant Secretary, The Ohio Casualty Insurance Company, Liberty Mutual Insurance Company, and West American Insurance Company do hereby certify that the original power of attorney of which the foregoing is a full, true and correct copy of the Power of Attorney executed by said Companies, is in full force and effect and has not been revoked.

IN TESTIMONY WHEREOF, I have hereunto set my hand and affixed the seals of said Companies this 4th day of January, 2022.



By: Renee C. Llewellyn  
Renee C. Llewellyn, Assistant Secretary

Not valid for mortgage, note, loan, letter of credit, currency rate, interest rate or residual value guarantees.

To confirm the validity of this Power of Attorney call 1-610-832-8240 between 9:00 am and 4:30 pm EST on any business day.



Know all men by these presents that Silver Star Construction Company, Inc. as PRINCIPAL, and The Ohio Casualty Insurance Company, a corporation organized under the laws of the State of New Hampshire, and authorized to transact business in the State of Oklahoma, as Surety, are held and firmly bound unto the State of Oklahoma in the sum of Three Million Six Hundred Sixteen Thousand Nine Hundred Ten and 70/100 DOLLARS (\$3,616,910.70), or the payment of which sum PRINCIPAL and SURETY bind themselves, their heirs executors, administrators, successors and assigns jointly and severally.

WHEREAS, the conditions of this obligation are such, that the PRINCIPAL, being the lowest and best Bidder on the following PROJECT:

**BID 2122-35 ALAMEDA STREET WIDENING PROJECT**  
**(RIDGE LAKE BOULEVARD TO 48<sup>TH</sup> AVENUE SE)**

has entered into a written CONTRACT (K-2122-75) with THE CITY OF NORMAN, dated this 4<sup>th</sup> day of January, 2022, for the erection and construction of this PROJECT, that CONTRACT being incorporated herein by reference as if fully set forth.

NOW, THEREFORE, if the PRINCIPAL, shall properly and promptly complete the work on this PROJECT in accordance with the CONTRACT, and shall well and truly pay all indebtedness incurred for labor and materials and repairs to and parts for equipment furnished in the making of the PROJECT, whether incurred by the PRINCIPAL, his subcontractors, or any material men, then this obligation shall be void. Otherwise this obligation shall remain in full force and effect. If debts are not paid within thirty (30) days after the same becomes due and payable, the person, firm, or corporation entitled thereto may sue and recover on this Bond, subject to the provisions of 61 O.S. S2, for the amount so due and unpaid.

It is further expressly agreed and understood by the parties hereto that no changes or alterations in said CONTRACT and no deviations from the plan or mode of procedure herein fixed shall have the effect of releasing the SURETIES, or any of them, from the obligation of this Bond.

It is further expressly agreed that the Principal's obligations under this Bond include payment of not less than the prevailing hourly rate of wages as established by the Commissioner of Labor of the State of Oklahoma and by the Secretary of the U.S. Department of Labor or as determined by a court on appeal.

IN WITNESS WHEREOF, the PRINCIPAL has caused these presents to be executed in its name and its corporate seal (where applicable) to be hereunto affixed by its duly authorized representative(s), on the 4 day of January, 2022, and the SURETY has caused these presents to be executed in its name and its corporate seal to be hereunto affixed by its authorized representative on the 4 day of January, 2022.

(Corporate Seal) (where applicable)

ATTEST

Corporate Secretary (where applicable)

Exec. Vice President  
Tim Caughie



Silver Star Construction, Co Inc.  
Principal  
Signed: [Signature]  
Greg Kulicki Authorized Representative  
Title: Vice President  
Address: 2401 S. Broadway  
Moore, OK 73160  
Telephone: 405-793-1725

Statutory Bond No. B-2122-53

Page 1 of 3

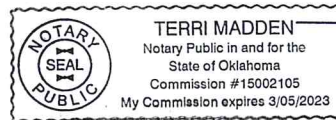


(Corporate Seal) (where applicable)

ATTEST:

Surety: The Ohio Casualty Insurance CompanySigned: *Russell Hollingsworth*  
Authorized RepresentativePrinted: Russell Hollingsworth  
Authorized RepresentativeTitle: Attorney-In-FactAddress: 2402 W Willow Enid, OK 73703Telephone: 580-233-2000**CORPORATE ACKNOWLEDGEMENT**STATE OF Oklahoma )  
 ) ss:  
COUNTY OF Cleveland )The foregoing instrument was acknowledge before me this 5<sup>th</sup> day of January, 2022 by Greg Kubiak - Vice President (Name and Title), of Silver Star Construction Co. Inc. a(n) corporation, on behalf of the corporation.WITNESS my hand and seal this 5<sup>th</sup> day of January, 2022.

My Commission Expires:

03/05/2023*T. Madden*  
Notary Public**INDIVIDUAL ACKNOWLEDGEMENT**STATE OF \_\_\_\_\_ )  
 ) ss:  
COUNTY OF \_\_\_\_\_ )

The foregoing instrument was acknowledge before me this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_, by \_\_\_\_\_ (Name and Title) of \_\_\_\_\_,

a(n) corporation.

WITNESS my hand and seal this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_.

\_\_\_\_\_  
Notary PublicMy Commission Expires:  
\_\_\_\_\_

Statutory Bond No. B-2122-53

Page 2 of 3

**PARTNERSHIP ACKNOWLEDGEMENT**

STATE OF \_\_\_\_\_ )  
 ) ss:  
 COUNTY OF \_\_\_\_\_ )

The foregoing instrument was acknowledge before me this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_,  
 by \_\_\_\_\_ (Name and Title) \_\_\_\_\_  
 (partner/agent) on behalf of \_\_\_\_\_, a partnership.

WITNESS my hand and seal this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_.

\_\_\_\_\_  
 Notary Public

My Commission Expires:  
 \_\_\_\_\_

**CITY OF NORMAN**

Approved as to form and legality this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_.

\_\_\_\_\_  
 City Attorney

Approved by the Council of the City of Norman this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_.

ATTEST:

\_\_\_\_\_  
 City Clerk

\_\_\_\_\_  
 Mayor

Statutory Bond No. B-2122-53  
 Page 3 of 3





This Power of Attorney limits the acts of those named herein, and they have no authority to bind the Company except in the manner and to the extent herein stated.

Item 7.

Liberty Mutual Insurance Company  
The Ohio Casualty Insurance Company  
West American Insurance Company

Certificate No: **8203689 - 986746**

## POWER OF ATTORNEY

**KNOWN ALL PERSONS BY THESE PRESENTS:** That The Ohio Casualty Insurance Company is a corporation duly organized under the laws of the State of New Hampshire, that Liberty Mutual Insurance Company is a corporation duly organized under the laws of the State of Massachusetts, and West American Insurance Company is a corporation duly organized under the laws of the State of Indiana (herein collectively called the "Companies"), pursuant to and by authority herein set forth, does hereby name, constitute and appoint, Jason Blair, Debra Cornelisen, John E. Dillingham, Mike Haselden, Russell Hollingsworth, Amber Jarman, Tara L. Ramos

all of the city of Enid state of OK each individually if there be more than one named, its true and lawful attorney-in-fact to make, execute, seal, acknowledge and deliver, for and on its behalf as surety and as its act and deed, any and all undertakings, bonds, recognizances and other surety obligations, in pursuance of these presents and shall be as binding upon the Companies as if they have been duly signed by the president and attested by the secretary of the Companies in their own proper persons.

IN WITNESS WHEREOF, this Power of Attorney has been subscribed by an authorized officer or official of the Companies and the corporate seals of the Companies have been affixed thereto this 11th day of May, 2020.



Liberty Mutual Insurance Company  
The Ohio Casualty Insurance Company  
West American Insurance Company

By: David M. Carey  
David M. Carey, Assistant Secretary

State of PENNSYLVANIA  
County of MONTGOMERY ss

On this 11th day of May, 2020 before me personally appeared David M. Carey, who acknowledged himself to be the Assistant Secretary of Liberty Mutual Insurance Company, The Ohio Casualty Company, and West American Insurance Company, and that he, as such, being authorized so to do, execute the foregoing instrument for the purposes therein contained by signing on behalf of the corporations by himself as a duly authorized officer.

IN WITNESS WHEREOF, I have hereunto subscribed my name and affixed my notarial seal at King of Prussia, Pennsylvania, on the day and year first above written.



COMMONWEALTH OF PENNSYLVANIA  
Notarial Seal  
Teresa Pastella, Notary Public  
Upper Merion Twp., Montgomery County  
My Commission Expires March 28, 2021  
Member, Pennsylvania Association of Notaries

By: Teresa Pastella  
Teresa Pastella, Notary Public

This Power of Attorney is made and executed pursuant to and by authority of the following By-laws and Authorizations of The Ohio Casualty Insurance Company, Liberty Mutual Insurance Company, and West American Insurance Company which resolutions are now in full force and effect reading as follows:

### ARTICLE IV – OFFICERS: Section 12. Power of Attorney.

Any officer or other official of the Corporation authorized for that purpose in writing by the Chairman or the President, and subject to such limitation as the Chairman or the President may prescribe, shall appoint such attorneys-in-fact, as may be necessary to act in behalf of the Corporation to make, execute, seal, acknowledge and deliver as surety any and all undertakings, bonds, recognizances and other surety obligations. Such attorneys-in-fact, subject to the limitations set forth in their respective powers of attorney, shall have full power to bind the Corporation by their signature and execution of any such instruments and to attach thereto the seal of the Corporation. When so executed, such instruments shall be as binding as if signed by the President and attested to by the Secretary. Any power or authority granted to any representative or attorney-in-fact under the provisions of this article may be revoked at any time by the Board, the Chairman, the President or by the officer or officers granting such power or authority.

### ARTICLE XIII – Execution of Contracts: Section 5. Surety Bonds and Undertakings.

Any officer of the Company authorized for that purpose in writing by the chairman or the president, and subject to such limitations as the chairman or the president may prescribe, shall appoint such attorneys-in-fact, as may be necessary to act in behalf of the Company to make, execute, seal, acknowledge and deliver as surety any and all undertakings, bonds, recognizances and other surety obligations. Such attorneys-in-fact subject to the limitations set forth in their respective powers of attorney, shall have full power to bind the Company by their signature and execution of any such instruments and to attach thereto the seal of the Company. When so executed such instruments shall be as binding as if signed by the president and attested by the secretary.

**Certificate of Designation** – The President of the Company, acting pursuant to the Bylaws of the Company, authorizes David M. Carey, Assistant Secretary to appoint such attorneys-in-fact as may be necessary to act on behalf of the Company to make, execute, seal, acknowledge and deliver as surety any and all undertakings, bonds, recognizances and other surety obligations.

**Authorization** – By unanimous consent of the Company's Board of Directors, the Company consents that facsimile or mechanically reproduced signature of any assistant secretary of the Company, wherever appearing upon a certified copy of any power of attorney issued by the Company in connection with surety bonds, shall be valid and binding upon the Company with the same force and effect as though manually affixed.

I, Renee C. Llewellyn, the undersigned, Assistant Secretary, The Ohio Casualty Insurance Company, Liberty Mutual Insurance Company, and West American Insurance Company do hereby certify that the original power of attorney of which the foregoing is a full, true and correct copy of the Power of Attorney executed by said Companies, is in full force and effect and has not been revoked.

IN TESTIMONY WHEREOF, I have hereunto set my hand and affixed the seals of said Companies this 4th day of January, 2022.



By: Renee C. Llewellyn  
Renee C. Llewellyn, Assistant Secretary

Not valid for mortgage, note, loan, letter of credit, currency rate, interest rate or residual value guarantees.

To confirm the validity of this Power of Attorney call 1-610-832-8240 between 9:00 am and 4:30 pm EST on any business day.



## MAINTENANCE BOND

Know all men by these presents that Silver Star Construction Company, Inc., as Principal, and The Ohio Casualty Insurance Company, a corporation organized under the laws of the State of New Hampshire, and authorized to transact business in the State of Oklahoma, as SURETY, are held and firmly bound unto THE CITY OF NORMAN, a Municipal Corporation of the State of Oklahoma, herein called CITY, in the sum of Three Million Six Hundred Sixteen Thousand Nine Hundred Ten and 70/100 DOLLARS (\$), such sum being equal to the contract price and being in force for a period of one year from the date of the acceptance of the below described improvements by the City Council, and thereafter for the sum of \$542,536.61 DOLLARS(\$), such sum being not less than fifteen percent (15%) of the total contract price of said improvements for a period of four years thereafter, for the payment of which sum PRINCIPAL and SURETY bind themselves, their heirs, executors, administrators, successors and assigns, jointly and severally.

WHEREAS, the conditions of this obligation are such that the PRINCIPAL, being the lowest and best bidder on the following project:

**BID 2122-35 ALAMEDA STREET WIDENING PROJECT**  
**(RIDGE LAKE BOULEVARD TO 48<sup>TH</sup> AVENUE SE)**

has entered into a written CONTRACT (K-2122-75) with the CITY OF NORMAN, dated this 4th day of January, 20 22 for the erection and construction of this PROJECT, that CONTRACT being incorporated herein by references as if fully set forth; and,

WHEREAS, under the ordinances of the CITY the PRINCIPAL is required to furnish to the CITY a maintenance bond covering said construction of this PROJECT, the bond to include the terms and provisions hereinafter set forth, as a condition precedent to final acceptance of the PROJECT.

NOW, THEREFORE, if the PRINCIPAL shall keep and maintain, subject to normal wear and tear, the construction, except for defects not occasioned by improper workmanship, materials, or failure to protect new work until it is accepted, and if the PRINCIPAL shall promptly repair, without notice from the CITY or expense to the CITY any and all defects arising from improper workmanship, materials, or failure to protect new work until it is accepted; all for a period of five (5) years from the date of the written final acceptance by the CITY, then this obligation shall be null and void. The amount of the Maintenance Bond shall be 100 % of the contract amount. Otherwise, this obligation shall remain in full force and effect at all times.

Provided further, however, that upon neglect, failure or refusal of the PRINCIPAL to maintain or make any needed repairs upon the construction on the PROJECT, as set out in the preceding paragraph, within ten (10) days after the mailing of notice to the PRINCIPAL by letter deposited in the United States Post Office at Norman, Oklahoma, addressed to the PRINCIPAL at the address set forth below, then the PRINCIPAL and SURETY shall jointly and severally be liable to the CITY for the cost and expense for making such repair, or otherwise maintaining the said construction.

It is further expressly agreed and understood by the parties hereto that no changes or alterations in said CONTRACT and no deviations from the plan or mode of procedure herein fixed shall have the effect of releasing the sureties, or any of them, from the obligations of this Bond.

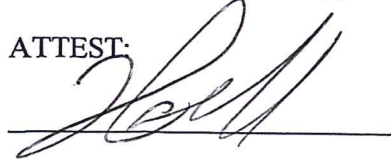
Maintenance Bond No. MB-2122-41

Page 1 of 3

IN WITNESS WHEREOF, the said PRINCIPAL has caused these presents to be executed in its name and its corporate seal (where applicable) to be hereunto affixed by its duly authorized representative(s), on the 04 day of January, 2022, and the SURETY has caused these presents to be executed in its name its corporate seal to be hereunto affixed by its authorized representative(s) on the 04 day of January, 2022.

(Corporate Seal) (where applicable)

ATTEST:



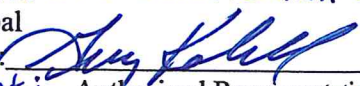
(Corporate Secretary (where applicable))

Exec. Vice President  
Tim Canale

(Corporate Seal) (where applicable)

ATTEST:



Silver Star Construction Co., Inc.  
Principal  
Signed:   
Greg Kulicki: Authorized Representative  
Title: Vice President

Address: 2401 S. Broadway  
Moore, OK 73160

Telephone: 405-793-1725

Surety: The Ohio Casualty Insurance Company

Signed:   
Authorized Representative

Printed: Russell Hollingsworth  
Authorized Representative

Title: Attorney-In-Fact

Address: 2402 W Willow Enid, OK 73703

Telephone: 580-233-2000

### CORPORATE ACKNOWLEDGEMENT

STATE OF Oklahoma )  
 ) ss:  
COUNTY OF Cleveland )

The foregoing instrument was acknowledge before me this 5<sup>th</sup> day of January, 2022 by Greg Kulicki - Vice President (Name and Title), of Silver Star Construction Co., Inc. a(n) corporation, on behalf of the corporation.

WITNESS my hand and seal this 5<sup>th</sup> day of January, 2022

  
Notary Public

My Commission Expires:

03/05/2023



Maintenance Bond No. MB-2122-41

Page 2 of 3



**INDIVIDUAL ACKNOWLEDGEMENT**

Item 7.

STATE OF \_\_\_\_\_ )  
 ) ss:  
COUNTY OF \_\_\_\_\_ )

The foregoing instrument was acknowledge before me this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_, by  
\_\_\_\_\_, (Name and Title) of \_\_\_\_\_,  
a(n) corporation.

WITNESS my hand and seal this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_.

\_\_\_\_\_  
Notary Public

My Commission Expires:  
\_\_\_\_\_

**PARTNERSHIP ACKNOWLEDGEMENT**

STATE OF \_\_\_\_\_ )  
 ) ss:  
COUNTY OF \_\_\_\_\_ )

The foregoing instrument was acknowledge before me this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_, by  
\_\_\_\_\_, (Name and Title) \_\_\_\_\_ (partner/agent)  
on behalf of \_\_\_\_\_, a partnership.

WITNESS my hand and seal this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_.

\_\_\_\_\_  
Notary Public

My Commission Expires:  
\_\_\_\_\_

**CITY OF NORMAN**

Approved as to form and legality this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_.

\_\_\_\_\_  
City Attorney

Approved by the Council of the City of Norman this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_.

ATTEST:

\_\_\_\_\_  
City Clerk

\_\_\_\_\_  
Mayor

Maintenance Bond No. MB-2122-41  
Page 3 of 3



This Power of Attorney limits the acts of those named herein, and they have no authority to bind the Company except in the manner and to the extent herein stated.

Item 7.

Liberty Mutual Insurance Company  
The Ohio Casualty Insurance Company  
West American Insurance Company

Certificate No: **8203689 - 986746**

## POWER OF ATTORNEY

KNOWN ALL PERSONS BY THESE PRESENTS: That The Ohio Casualty Insurance Company is a corporation duly organized under the laws of the State of New Hampshire, that Liberty Mutual Insurance Company is a corporation duly organized under the laws of the State of Massachusetts, and West American Insurance Company is a corporation duly organized under the laws of the State of Indiana (herein collectively called the "Companies"), pursuant to and by authority herein set forth, does hereby name, constitute and appoint, Jason Blair, Debra Cornelsen, John E. Dillingham, Mike Haselden, Russell Hollingsworth, Amber Jarman, Tara L. Ramos

all of the city of Enid state of OK each individually if there be more than one named, its true and lawful attorney-in-fact to make, execute, seal, acknowledge and deliver, for and on its behalf as surety and as its act and deed, any and all undertakings, bonds, recognizances and other surety obligations, in pursuance of these presents and shall be as binding upon the Companies as if they have been duly signed by the president and attested by the secretary of the Companies in their own proper persons.

IN WITNESS WHEREOF, this Power of Attorney has been subscribed by an authorized officer or official of the Companies and the corporate seals of the Companies have been affixed thereto this 11th day of May, 2020.



Liberty Mutual Insurance Company  
The Ohio Casualty Insurance Company  
West American Insurance Company

By: David M. Carey  
David M. Carey, Assistant Secretary

State of PENNSYLVANIA ss  
County of MONTGOMERY

On this 11th day of May, 2020 before me personally appeared David M. Carey, who acknowledged himself to be the Assistant Secretary of Liberty Mutual Insurance Company, The Ohio Casualty Company, and West American Insurance Company, and that he, as such, being authorized so to do, execute the foregoing instrument for the purposes therein contained by signing on behalf of the corporations by himself as a duly authorized officer.

IN WITNESS WHEREOF, I have hereunto subscribed my name and affixed my notarial seal at King of Prussia, Pennsylvania, on the day and year first above written.



COMMONWEALTH OF PENNSYLVANIA  
Notarial Seal  
Teresa Pastella, Notary Public  
Upper Merion Twp., Montgomery County  
My Commission Expires March 28, 2021  
Member, Pennsylvania Association of Notaries

By: Teresa Pastella  
Teresa Pastella, Notary Public

This Power of Attorney is made and executed pursuant to and by authority of the following By-laws and Authorizations of The Ohio Casualty Insurance Company, Liberty Mutual Insurance Company, and West American Insurance Company which resolutions are now in full force and effect reading as follows:

### ARTICLE IV – OFFICERS: Section 12. Power of Attorney.

Any officer or other official of the Corporation authorized for that purpose in writing by the Chairman or the President, and subject to such limitation as the Chairman or the President may prescribe, shall appoint such attorneys-in-fact, as may be necessary to act in behalf of the Corporation to make, execute, seal, acknowledge and deliver as surety any and all undertakings, bonds, recognizances and other surety obligations. Such attorneys-in-fact, subject to the limitations set forth in their respective powers of attorney, shall have full power to bind the Corporation by their signature and execution of any such instruments and to attach thereto the seal of the Corporation. When so executed, such instruments shall be as binding as if signed by the President and attested to by the Secretary. Any power or authority granted to any representative or attorney-in-fact under the provisions of this article may be revoked at any time by the Board, the Chairman, the President or by the officer or officers granting such power or authority.

### ARTICLE XIII – Execution of Contracts: Section 5. Surety Bonds and Undertakings.

Any officer of the Company authorized for that purpose in writing by the chairman or the president, and subject to such limitations as the chairman or the president may prescribe, shall appoint such attorneys-in-fact, as may be necessary to act in behalf of the Company to make, execute, seal, acknowledge and deliver as surety any and all undertakings, bonds, recognizances and other surety obligations. Such attorneys-in-fact subject to the limitations set forth in their respective powers of attorney, shall have full power to bind the Company by their signature and execution of any such instruments and to attach thereto the seal of the Company. When so executed such instruments shall be as binding as if signed by the president and attested by the secretary.

**Certificate of Designation** – The President of the Company, acting pursuant to the Bylaws of the Company, authorizes David M. Carey, Assistant Secretary to appoint such attorneys-in-fact as may be necessary to act on behalf of the Company to make, execute, seal, acknowledge and deliver as surety any and all undertakings, bonds, recognizances and other surety obligations.

**Authorization** – By unanimous consent of the Company's Board of Directors, the Company consents that facsimile or mechanically reproduced signature of any assistant secretary of the Company, wherever appearing upon a certified copy of any power of attorney issued by the Company in connection with surety bonds, shall be valid and binding upon the Company with the same force and effect as though manually affixed.

I, Renee C. Llewellyn, the undersigned, Assistant Secretary, The Ohio Casualty Insurance Company, Liberty Mutual Insurance Company, and West American Insurance Company do hereby certify that the original power of attorney of which the foregoing is a full, true and correct copy of the Power of Attorney executed by said Companies, is in full force and effect and has not been revoked.

IN TESTIMONY WHEREOF, I have hereunto set my hand and affixed the seals of said Companies this 4th day of January, 2022.



By: Renee C. Llewellyn  
Renee C. Llewellyn, Assistant Secretary

Not valid for mortgage, note, loan, letter of credit, currency rate, interest rate or residual value guarantees.

To confirm the validity of this Power of Attorney call 1-610-832-8240 between 9:00 am and 4:30 pm EST on any business day.



# Resolution

R-2122-66

A RESOLUTION OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, AUTHORIZING AND APPOINTING SILVER STAR CONSTRUCTION COMPANY, INC., AS PROJECT AGENT FOR THE ALAMEDA STREET WIDENING PROJECT (RIDGE LAKE BOULEVARD TO 48TH AVENUE S.E.) FOR THE CITY OF NORMAN.

- § 1. WHEREAS, the City of Norman, Oklahoma, does hereby acknowledge that the tax-exempt status of this political subdivision is a significant factor in determining the agreed contract price bid by Silver Star Construction Company, Inc., for the Alameda Street Widening Project (Ridge Lake Boulevard to 48th Avenue S.E. for the City of Norman; and
- § 2. WHEREAS, the City of Norman, Oklahoma, in compliance with State law, desires to confer on Silver Star Construction Company, Inc., its special State and Federal sales tax exemptions and in order to achieve such end, finds it necessary to appoint as its direct purchasing agent, Silver Star Construction Company, Inc., to purchase materials which are in fact used for the Alameda Street Widening Project (Ridge Lake Boulevard to 48th Avenue S.E. for the City of Norman; and
- § 3. WHEREAS, this limited agent status is conferred with the express understanding that Silver Star Construction Company, Inc., shall appoint employees and subcontractors as subagents who shall be authorized to make purchases on their behalf.

NOW, THEREFORE, BE IT RESOLVED BY THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA:

- § 4. That the City of Norman, Oklahoma, on the 25th day of January, 2022, did appoint Silver Star Construction Company, Inc., who is involved with the Alameda Street Widening Project (Ridge Lake Boulevard to 48th Avenue S.E.), an agent of the City of Norman, Oklahoma, solely for the purpose of purchasing, on a tax-exempt basis, materials and tangible personal property to be used exclusively for the Alameda Street Widening Project (Ridge Lake Boulevard to 48th Avenue S.E.) for the City of Norman.

PASSED AND ADOPTED THIS \_\_\_\_ day of \_\_\_\_\_, 2022.

\_\_\_\_\_  
Mayor

ATTEST:

\_\_\_\_\_  
City Clerk





**File Attachments for Item:**

8. CONSIDERATION OF APPROVAL, ACCEPTANCE, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF CHANGE ORDER NO. FOUR TO CONTRACT K-2021-35 BY AND BETWEEN THE CITY OF NORMAN, OKLAHOMA, AND FLINTCO L.L.C., INCREASING THE CONTRACT AMOUNT BY \$93,249.12 FOR A REVISED CONTRACT AMOUNT OF \$8,983,752.96 AND ADDING 41 CALENDAR DAYS FOR THE NORTH BASE COMPLEX PROJECT, PHASE 1, IN ORDER TO MAKE VARIOUS CHANGES TO THE PROJECT DESIGN AND BUDGET APPROPRIATION AS OUTLINED IN THE STAFF REPORT.



## CITY OF NORMAN, OK STAFF REPORT

**MEETING DATE:** 1/25/2022

**REQUESTER:** Paul D'Andrea, Capital Projects Engineer

**PRESENTER:** Shawn O'Leary, Public Works Director

**ITEM TITLE:** CONSIDERATION OF APPROVAL, ACCEPTANCE, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF CHANGE ORDER NO. FOUR TO CONTRACT K-2021-35 BY AND BETWEEN THE CITY OF NORMAN, OKLAHOMA, AND FLINTCO L.L.C., INCREASING THE CONTRACT AMOUNT BY \$93,249.12 FOR A REVISED CONTRACT AMOUNT OF \$8,983,752.96 AND ADDING 41 CALENDAR DAYS FOR THE NORTH BASE COMPLEX PROJECT, PHASE 1, IN ORDER TO MAKE VARIOUS CHANGES TO THE PROJECT DESIGN AND BUDGET APPROPRIATION AS OUTLINED IN THE STAFF REPORT.

### BACKGROUND:

**Parks Maintenance Facility:** On October 13, 2015, the citizens of Norman voted to fund the Norman Forward Program with a limited term ½ percent sales tax increase for 15 years. This initiative included a number of high-priority projects outlined in the 2014 Library Master Plan Update, the 2009 Norman Parks and Recreation Master Plan and additional projects designed to provide recreational opportunities for Norman residents. One such project was the construction of the Griffin Park Sports Complex.

The Norman Forward Program includes a Maintenance Facility for the City of Norman Parks and Recreation Department. This facility was originally to be relocated from Reaves Park to Griffin Park; however, the City now intends to construct a new Parks, and City Transit/Public Safety Maintenance Facility adjacent to the City's current Fleet Maintenance Facility at the "North Base" Complex, approximately located at 1310 Da Vinci Street, near the Norman Municipal Airport. The proposed multi-departmental facility will provide maintenance for City Parks and Recreation vehicles and equipment, City Transit System buses, and Public Safety vehicles and equipment.

**Transit Maintenance Facility:** In August of 2018, University of Oklahoma officials advised City of Norman staff of their desire to transfer non-campus public transportation services in Norman to another operator by the end of the 2019 fiscal year. On July 1, 2019, operation of non-campus public transportation services in Norman were transferred from the University to the City.

On July 30, 2019, the City Council approved Contract K-1920-32, leasing a portion of University-owned property located at 510 Chesapeake, commonly referred to as the OU Transportation Operations Center, for office space, maintenance bays, and fleet storage, until December 31,

2019. On December 10, 2019, the City and the University extended that lease agreement until June 30, 2020. On April 28, 2020, the City and the University extended that lease agreement until December 31, 2020. Another extension of this agreement to December 31, 2021 was entered on August 24, 2021. The last extension was entered on December 14, 2021, extending the lease to January 31, 2022 or until substantial completion of North Base, whichever occurred first.

**North Base Complex Design:** On March 14, 2017, the Norman City Council approved Contract K-1617-114 between the City of Norman and PDG, LLC d.b.a. Planning Design Group, in the amount of \$761,000 for Professional Architectural Design Services for the Griffin Park Sports Complex.

Because PDG, LLC was performing the professional architectural design services for the Griffin Park Sports Complex, City staff recommended amending their contract to include design of the new Parks Maintenance Facility. These services were over and above those included in contract K-1617-114. Due to the desire to co-locate the Parks, City Transit and Public Safety Maintenance Facilities at the North Base Campus, staff further recommended including master planning and design of the City Transit and Public Safety Maintenance Facilities in the amended contract.

On August 27, 2019, the Norman City Council approved Amendment No. 1 to Contract K-1617-114 between the City of Norman and PDG, LLC d.b.a. Planning Design Group, in the amount of \$430,280 for additional Professional Architectural Design Services to add the City Park Maintenance Facility and Transit/Public Safety Maintenance Facility to the existing contract for the Griffin Park Sports Complex.

On April 14, 2020, the Norman City Council approved Amendment No. 3 to Contract K-1617-114 with PDG, LLC d.b.a. Planning Design Group, in the amount of \$316,370 for additional Professional Architectural Design Services to provide for an increased project scope and to add bidding services and construction administration services for the City Park Maintenance Facility and Transit/Public Safety Maintenance Facility to the existing contract for the Griffin Park Sports Complex.

On September 8, 2020, the Norman City Council approved Amendment No. 5 to Contract K 1617-114 with PDG, LLC d.b.a. Planning Design Group, in the amount of \$49,500 for additional architectural design services for the proposed City Parks and Transit/Public Safety Maintenance Facility to include proposed building and parking revisions, project phasing, renderings, and inclusion of federal third party contracting requirements needed for FTA Grant funding.

On September 8, 2020, the Norman City Council approved Amendment No. 4 to Contract K-1516-110 with City of Norman, The Norman Municipal Authority, and ADG, P.C., in the amount of \$157,180.00 adding Construction Phase Services for the North Base Complex, Phase 1 Project to the scope of projects receiving program management services.

On October 13, 2020, the Norman City Council awarded Contract K-2021-35 with Flintco LLC, in the amount of \$8,648,000, for the construction of the City Park Maintenance Facility and the Transit/Public Safety Maintenance Facility.

On March 9, 2021, the Norman City Council approved Change Order No. 1 to Contract K-2021-35 between the City of Norman, Oklahoma, and Flintco LLC, in the amount of \$64,737.65 and 20 Calendar Days, for the construction of the City Park Maintenance Facility and the Transit/Public Safety Maintenance Facility.

On July 13, 2021, the Norman City Council approved Change Order No. 2 to Contract K-2021-35 between the City of Norman, Oklahoma, and Flintco LLC, in the amount of \$76,156.98 and adding 39 Calendar Days, for the construction of the City Park Maintenance Facility and the Transit/Public Safety Maintenance Facility.

On October 12, 2021, the Norman City Council approved Change Order No. 3 to Contract K-2021-35 with Flintco LLC, in the amount of \$102,609.21 and adding 58 Calendar Days, for the construction of the City Park Maintenance Facility and the Transit/Public Safety Maintenance Facility.

## **DISCUSSION:**

As construction progressed on the North Base Phase 1 project, design revisions were submitted to the contractor to cover elements of the design and conditions of the project not originally accounted for in the bid documents, but which were required for the successful completion of the project scope and to avoid structural or equipment conflicts. Furthermore, the City requested several changes to the original design to provide for better use of the finished facility. Finally, weather and supply chain issues have caused delays outside of the contractor's control, prompting the need to amend the contract completion date. These changes include the following items:

- Revisions to HVAC Ductwork due to interference with structural steel members;
- Relocation of heaters and addition of heat shields to protect lubrication equipment from heat damage;
- Additional power supply, data supply, and security equipment to office areas, entry gates, and flagpole;
- Addition of a ladder to Transit roof for safe access to lower roof level;
- Relocation of CNG evacuation ducts to accommodate other equipment;
- Change of lubrication equipment and controller due to unavailability of the specified system;
- Various revisions to condensate lines, vent routing and other mechanical and utility systems;
- Addition of sliding windows to Parts Room in Transit Building;
- Addition of conference room televisions and wall mounts for both buildings;
- Addition of removable sections of second story handrails for ease of loading materials into the storage areas in both buildings;
- Deduction for the removal of decorative box frames around maintenance bay doors for both buildings;
- Weather delay days;

- Additional delay days for supply issues outside of the contractor's control.

Change Order No. 4 results in an increase of the lump sum contract amount of \$93,249.12 or 1.08% of the original contract amount. Of that amount, \$86,336.24 would apply to the Base Bid (Transit/Public Safety Maintenance Facility), and \$6,912.88 would apply to Bid Alternate No. 1 (Parks Maintenance Facility). If approved, the overall contract amount will have increased from \$8,648,000 to \$8,983,752.96 for an overall project increase to date of 3.88%.

In addition to the cost increase, an additional 41 construction days are required to account for the project weather and supply chain delays. The additional days would result in a completion date of January 7, 2022.

The change order will require a budget appropriation from the Capital Fund Balance (Account 50-29000), to Transit/Fire Maintenance Facility, Construction (Account 50193365-46101; Project BG0252), to cover these additional costs.

### **RECOMMENDATIONS:**

Staff recommends the following:

1. Approval of Change Order No. 4 for the North Base Complex, Phase 1, Contract K-2021-35 with Flintco, LLC in the amount of \$93,249.12 plus 41 additional calendar days, for the Base Bid and Bid Alternate No. 1.
2. Approval of a budget appropriation in the amount of \$93,249.12 from the Capital Fund Balance (Acct. No. 50-29000), to Transit/Fire Maintenance Facility, Construction (Account 50193365-46101; Project BG0252).



## Owner Change Order

Change Order Number: 004

## City of Norman - North Base Complex Phase 1

1310-1380 DA Vinci Street  
Norman, Oklahoma 73069

Flintco LLC (40-000)

Project # 20136

Date: 12/22/2021

## To Owner:

City of Norman  
201-C West Gray  
Norman, Oklahoma 73069

Contract Number: K-2021-35

Contract Date: 10/13/2020

## The Contract is hereby revised by the following items:

## Details

Includes Costs for the Following:

- RFIs Nos. 076, 077, 078, 084, 085/086, 092, 095, 097, 098, 100, 103, 106, 109, 111, & 113
- Landscaping/Irrigation Reduction
- Additional Roof Ladder
- Additional Electrical for Flag Pole
- Additional Glazing
- Added TV/Mount
- Delays Days\*\*\*
- Weather Delay Days Thru 12/22/2021\*\*\*

Items with \*\*\* have no cost impact.

PCO #	Title	Amount
004	Change Order No. 004	\$93,249.12
<b>PCO Total:</b>		<b>\$93,249.12</b>

The original Contract Value	\$8,648,000.00
Sum of changes by prior Prime Contract Change Orders	\$242,503.84
The Contract Value prior to this Prime Contract Change Order	\$8,890,503.84
The Contract Value will be changed by this Prime Contract Change Order in the amount of	\$93,249.12
The new Contract Value including this Prime Contract Change Order will be	<b>\$8,983,752.96</b>
The Contract duration will be changed by	41 days
The revised Substantial Completion date as of this Prime Contract Change Order	1/7/2022

**Flintco, LLC**

CONTRACTOR

2302 S. Prospect  
Oklahoma City, Oklahoma 73129

Address

By

SIGNATURE

DATE

**City of Norman**

OWNER

201-C West Gray  
Norman, Oklahoma 73069

Address

By

SIGNATURE

DATE

**GSB, Inc. Architects & Planners**

ARCHITECT

3555 NW 58TH Street, Suite 700W  
Oklahoma City, OK 73112

Address

By

SIGNATURE

DATE

**PROPOSED CHANGE ORDER SUMMARY--- CONSTRUCTION MANAGER****Construction Manager:**

Flintco, LLC

**THE CITY OF NORMAN**

Proj Name/ #: Norman North Base - Phase 1 / 20136

PCO No.: #004

RFI #076, RFI #077, RFI #078, RFI #084, RFI #085/#086, RFI #092, RFI #095, RFI #097,  
RFI #100, RFI #103, RFI #106, RFI #109, RFI #111, RFI #113, Landscaping/Irrigation  
Reduction, Additional Roof Ladder, Additional Electrical for Flag Pole and Additional  
Glazing & Added TV/Mount

PCO Title:

PCO Date: 12/22/2021

<b>1.</b>	<b>Subcontractors' Work</b> (enter amounts from Subcontractor Summary Sheets)		
DIV 23	Orcutt: RFI #077 - Relocation of HVAC Duct at Overhead Doors at Fleet	\$	9,000.00
DIV 23	Orcutt: RFI #077 - Ductwork Revision Around Structural Steel at Fleet	\$	7,084.00
DIV 23	Orcutt: RFI #078 - Lowering of Heaters and Addition of Heat Shields at Fleet	\$	6,388.00
DIV 32	Green Shade: Reduction of Landscaping & Irrigation	\$	(9,299.71)
DIV 26	DSE: RFI #084 - Added Power and Data at Fleet	\$	2,101.00
DIV 27	H&H: RFI #085 & #086 - Swale at Da Vinci & Inlet Modifications at Flood	\$	14,575.00
DIV 26	DSE/Turning Point/Tipton: RFI #092 Converging Gate Access Controls	\$	12,356.20
DIV 22	Orcutt: RFI #095 - MAU Condensate Routing	\$	1,593.00
DIV 22	Orcutt: RFI #097 - Antifreeze Tank Venting	\$	1,186.00
DIV 26	DSE: RFI #100 - Central Vac System Power Discrepancies	\$	2,183.00
DIV 9/23	Orcutt/Naff/Wiljo: RFI #103 - CNG Duct Relocation @ Tool Room F131	\$	4,614.00
DIV 05	K&E: RFI #106 Fleet - Removable Handrails	\$	4,556.00
DIV 22	Orcutt: RFI #111 - Waste Oil Pump Venting	\$	1,718.00
DIV 22/23	Orcutt: RFI #113 - Pressure Washer Venting	\$	6,269.00
DIV 05	Additional Ladder to Lower Roof	\$	1,233.77
DIV 26	Additional Electrical to Flag Pole	\$	950.00
DIV 08	Additional Glazing at Tire/Parts Pass Thru Windows	\$	2,612.00
	(1) 65" TV and Wall Mount per Owner's Request	\$	582.19
	Subtotal for Subcontractors' Work	\$	69,701.45
<b>2.</b>	<b>Itemized Direct Project Management Cost Increases (if any)</b>		
	Field Engineer (Layout): 3 Days @ \$70/HR	\$	1,680.00
	Labor Foreman: 0 Days @ \$65/HR	\$	-
	Burden Rate (58% DPM Costs)	\$	974.40
	Subtotal for Direct Project Management Cost Increases (if any)	\$	2,654.40
	Subtotal 1. + 2.	\$	72,355.85
<b>3.</b>	<b>Construction Manager's Self Performed Work</b>		
DIV 13	RFI #076 - Addition of Simple Saver Above Tire Carousel (\$45/hr x 48 hrs) Note: Extra Material and Lifts Onsite	\$	2,160.00
DIV 13	RFI #109 Fleet - Box Frame Canopies	\$	(10,840.00)
DIV 05	Flintco Cost to Install Temp Poles at Da Vinci	\$	1,883.24
DIV 05	Flintco Cost to Install Removable Handrails at Fleet	\$	1,825.74
DIV 05	Flintco Cost to Install Additional Ladder to Low Roof	\$	5,132.22
	Clean-Up: 0 Days @ \$45/HR (Incl. Burden Rate @ 58%)	\$	-
	Subtotal for Construction Manager's Self Perfrom Work	\$	161.20
	<b>Subtotal, Cost of Work and Direct Proj Mgmt Cost</b>	\$	72,517.05
<b>4.</b>	<b>Construction Manager Fee</b> (Percent of Subtotal 1. and 2. above)	10.00%	\$ 7,177.37
<b>5.</b>	<b>Construction Manager Overhead</b> (Percent of Subtotal 1. and 2. above)	5.00%	\$ 3,588.68
<b>6.</b>	<b>CM's Liability Insurance</b>	0.65%	\$ 541.34
<b>7.</b>	<b>Subcontractor Default Insurance, if authorized (% of Subcontractor Subtotal)</b>	1.60%	\$ 1,341.19
<b>8.</b>	<b>CM's Builders Risk Insurance</b>	0.55%	\$ 468.41
<b>9.</b>	<b>Project Bonds</b>	0.82%	\$ 702.20
<b>Total Cost of Change</b>			\$ 86,336.24

Approved for Construction – Any cost component may be adjusted in accordance with the Agreement for At-Risk Construction Management Services.

Construction Manager

Architect/Engineer

Owner



**PROPOSED CHANGE ORDER SUMMARY--- CONSTRUCTION MANAGER****Construction Manager:**

Flintco, LLC

**THE CITY OF NORMAN**

Proj Name/ #: Norman North Base - Phase 1 / 20146

PCO No.: #004

PCO Title: RFI #077, RFI #084, RFI #095, RFI #098, RFI #106 &amp; Added TV/Mount

PCO Date: 12/22/2021

<b>1.</b>	<b>Subcontractors' Work</b> (enter amounts from Subcontractor Summary Sheets)		
DIV 23	Orcutt: RFI #077 - Relocation of HVAC Duct at Overhead Doors at Parks	\$	3,370.00
DIV 23	Orcutt: RFI #077 - Lowering of Heaters at Parks	\$	3,658.00
DIV 26	DSE: RFI #084 - Added Power and Data at Parks	\$	105.00
DIV 22	Orcutt: RFI #095 - MAU Condensate Routing	\$	1,593.00
DIV 22	Orcutt: RFI #098 - MAU-P-01 Gas Line Sizing Discrepancy	\$	461.00
DIV 05	K&E: RFI #106 Parks - Mezzanine Handrail Requested Change	\$	2,278.00
	(1) 65" TV and Wall Mount per Owner's Request	\$	582.19
	Subtotal for Subcontractors' Work	\$	12,047.19
<b>2.</b>	<b>Itemized Direct Project Management Cost Increases (if any)</b>		
	Field Engineer (Layout): 0 Days @ \$70/HR	\$	-
	Labor Foreman: 0 Days @ \$65/HR	\$	-
	Burden Rate (58% DPM Costs)	\$	-
	Subtotal for Direct Project Management Cost Increases (if any)	\$	-
	Subtotal 1. + 2.	\$	12,047.19
<b>3.</b>	<b>Construction Manager's Self Performed Work</b>		
DIV 13	RFI #109 Parks - Box Frame Canopies	\$	(10,520.00)
DIV 05	Flintco Cost to Install Removable Handrail at Parks	\$	3,421.48
	Clean-Up: 0 Days @ \$45/HR (Incl. Burden Rate @ 58%)	\$	-
	Subtotal for Construction Manager's Self Perfrom Work	\$	(7,098.52)
	<b>Subtotal, Cost of Work and Direct Proj Mgmt Cost</b>	\$	4,948.67
<b>4.</b>	<b>Construction Manager Fee</b> (Percent of Subtotal 1. and 2. above)	10.00%	\$ 1,146.50
<b>5.</b>	<b>Construction Manager Overhead</b> (Percent of Subtotal 1. and 2. above)	5.00%	\$ 573.25
<b>6.</b>	<b>CM's Liability Insurance</b>	0.65%	\$ 43.34
<b>7.</b>	<b>Subcontractor Default Insurance, if authorized (% of Subcontractor Subtotal)</b>	1.60%	\$ 107.39
<b>8.</b>	<b>CM's Builders Risk Insurance</b>	0.55%	\$ 37.51
<b>9.</b>	<b>Project Bonds</b>	0.82%	\$ 56.22
<b>Total Cost of Change</b>			\$ 6,912.88

Approved for Construction -- Any cost component may be adjusted in accordance with the Agreement for At-Risk Construction Management Services.

Construction Manager

Architect/Engineer

Owner

## **Current Delay Days Requested:**

**Weather: 4 Working Days (7 Calendar Days) from 9/15/2021 - 12/22/2021**

**Additional Days Associated with CO# 004: 25 Working Days (34 Calendar Days)\*\*\***

**Total Working Days Requested: 29 Working Days (41 Calendar Days)  
Requested Revised Substantial Completion Date: 01/07/2022**

**\*\*\* Note: The cause for the 25 Working Days (34 Calendar Days) delay has been a direct result of:**

**Our suppliers being unable to receive materials due to the current market shortages. Flintco provided notified the Owner of these delays and has worked diligently with suppliers to expedite manufacturing and delivery, if possible, and help minimize this impact.**

**ONG Gas line conflict on Da Vinci.**

**Unforeseen Epoxy Flooring issues.**

**Additional time required associated with RFI Nos. 077, 084, 085, 086, 092, 096, 097, 100, 103, 111R1 & 113.**

CHANGE ORDER SUMMARY  
CITY OF NORMAN  
CLEVELAND COUNTY, OKLAHOMA

CHANGE ORDER NO. 004

DATE: January 25, 2021

CONTRACT NO.: K-2021-35

SUBMITTED BY: Paul D'Andrea

PROJECT: North Base Complex, Phase 1

CONTRACTOR: Flintco, LLC  
2302 S. Prospect  
Oklahoma City, Oklahoma 73129

Original Completion Date: July 30, 2021

Previous Completion Date: November 24, 2021 ORIGINAL CONTRACT AMOUNT \$8,648,000.00

(Increase) this change order 41 Calendar days

New Completion Date: January 7, 2022 PRESENT CONTRACT AMOUNT \$ 8,890,503.84

DESCRIPTION	DECREASE	INCREASE
Change in Pay Quantities	\$0.00	\$93,249.12

NET CHANGE \$93,249.12

REVISED CONTRACT AMOUNT \$8,983,752.96

See Detailed Quantity Change Summary on Page 2 of 2.

[illegible]

<b>Totals</b>	<b>\$93,249.12</b>	<b>\$0.00</b>
<b>Difference</b>	<b>\$93,249.12</b>	

CONTRACTOR:\_\_\_\_\_

DATE: \_\_\_\_\_

ENGINEER: \_\_\_\_\_

DATE: \_\_\_\_\_

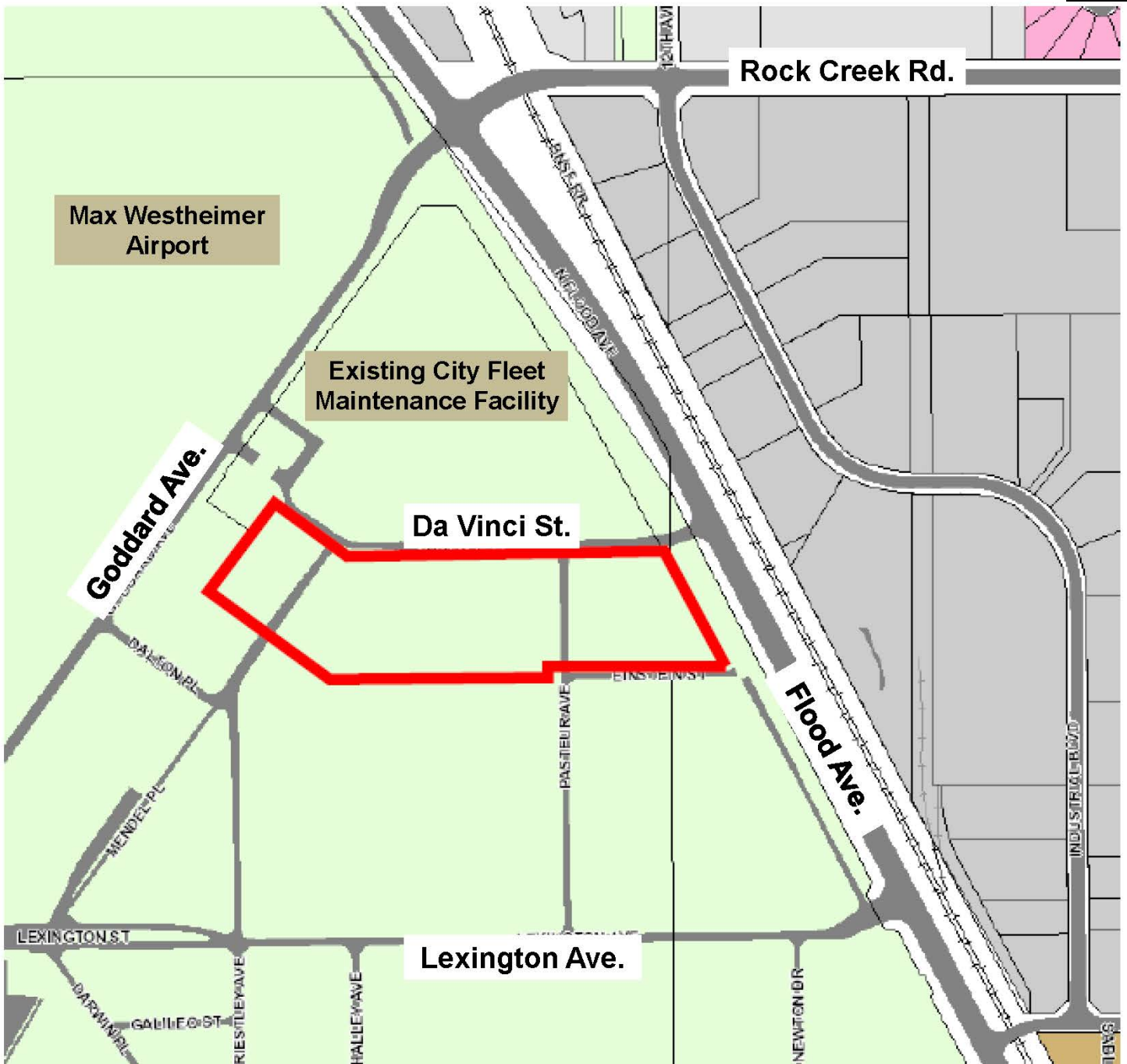
CITY ATTORNEY: \_\_\_\_\_

DATE:

ACCEPTED BY: \_\_\_\_\_

DATE:

(Mayor)



## North Base Complex, Phase 1 Location Map



**File Attachments for Item:**

9. CONSIDERATION OF APPROVAL, ACCEPTANCE, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF CONTRACT K-2122-91: A CONTRACT BY AND BETWEEN THE CITY OF NORMAN, OKLAHOMA, AND H.W. LOCHNER, INC., IN THE AMOUNT OF \$61,400 TO PROVIDE CONSULTANT SERVICES FOR THE BRIDGE MAINTENANCE PROGRAM SITE DISCOVERY PROJECT AND BUDGET TRANSFER AS OUTLINED IN THE STAFF REPORT.



## CITY OF NORMAN, OK STAFF REPORT

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**MEETING DATE:** 01/25/2022

**REQUESTER:** Joseph Hill, Streets Program Manager

**PRESENTER:** Shawn O'Leary, Director of Public Works

**ITEM TITLE:** CONSIDERATION OF APPROVAL, ACCEPTANCE, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF CONTRACT K-2122-91: A CONTRACT BY AND BETWEEN THE CITY OF NORMAN, OKLAHOMA, AND H.W. LOCHNER, INC., IN THE AMOUNT OF \$61,400 TO PROVIDE CONSULTANT SERVICES FOR THE BRIDGE MAINTENANCE PROGRAM SITE DISCOVERY PROJECT AND BUDGET TRANSFER AS OUTLINED IN THE STAFF REPORT.

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### BACKGROUND:

The Oklahoma Department of Transportation (ODOT) is tasked by the Federal Highway Administration to perform Bridge Safety Inspections for all publicly owned structures greater than 20 feet in length. Inspections are performed at least once every two years to identify any critical findings, document the bridge condition, and make repair recommendations if necessary. ODOT compiles a list of pre-qualified candidates from which the City interviews at least three (3). Upon completion of the interviews, City Staff selects the candidate they feel is most qualified.

On November 12, 2019, City Council approved Resolution R-1920-53 selecting H.W. Lochner, Inc., as the City's appointed Bridge Safety Inspection Consultant to ODOT for compliance with the National Bridge Inspection Standards under ODOT Contract Number CI-2105G. The contract duration was 2 years.

In 2021, a new round of interviews was conducted to select a firm for the 2021-2023 Off-System Bridge Inspections. On October 12, 2021, City Council approved Resolution R-2122-45 selecting H.W. Lochner, Inc., as the City's appointed Bridge Safety Inspection Consultant to ODOT for compliance with the National Bridge Inspection Standards.

The 2019 Bridge Safety Inspection results identified 13 structurally deficient bridges, 8 functionally obsolete bridges, and 8 bridges at risk of becoming structurally deficient. Based on this data, City Staff identified and ranked bridges in need of maintenance activities. The four bridges that were deemed to be the highest priority for maintenance in this cycle are listed below:

- NBI No. 05645 – 48th Ave. SE (0.8 miles South of HWY 9)
- NBI No. 18958 – W. Imhoff Rd. (0.2 miles East of S. Berry Rd.)
- NBI No. 20034 – Cedar Lane Rd. (0.8 miles East of Indian Meridian Ave.)
- NBI No. 09189 – 60th Ave. NE (0.5 miles North of Rock Creek Rd.)

On July 13, 2021, City Council approved Contract K-2122-6 with Haskell Lemon Construction Co. to perform maintenance activities on the aforementioned bridge locations as part of the City's annual Bridge Maintenance Program.

On Thursday, July 29, 2021, City Staff were made aware of the failure of the southeast wing wall attached to the West Imhoff Road Bridge, NBI No. 18958. Upon initial inspections performed by City Staff, the condition of the bridge was found to be severe enough to warrant immediate closure.

On August 10, 2021, Haskell Lemon Construction Co., mobilized to the West Imhoff Road Bridge to begin removing the debris from the channel that was restricting the flow of Imhoff Creek which was part of the original scope of work listed in the FYE 2022 Bridge Maintenance Program contract. On August 11, 2021, Haskell Lemon Construction Co., investigated spalling on the northwest wing wall that was also identified in the FYE 2022 Bridge Maintenance Program contract scope. During their investigations a large portion of concrete came loose exposing the joint between the wing wall and the bridge structure. The wing wall was found to have approximately 1.5" of separation from the bridge structure with no reinforcing tie-ins. This wing wall has three (3) utility lines routed through it: a sanitary sewer line, a 4" gas line, and a potable water line. The wing wall is in danger of a full-scale failure; which failure would affect these three utilities as well as the structure's stability.

Due to the need for an urgent response, City Staff requested H.W. Lochner, Inc., travel to the location and provide a preliminary engineering assessment of the bridge and develop plans for the repair. H.W. Lochner, Inc., has previously been selected using a Qualifications-Based Selection process. H.W. Lochner, Inc. provided a proposal, which was accepted and executed by the City Manager. On September 9, 2021, H.W. Lochner, Inc. submitted the finalized plans for the repair of the West Imhoff Road Bridge.

On September 14, 2021, City Council approved Contract K-2122-59 with Haskell Lemon Construction Co., in the amount of \$1,440,695 to perform emergency repair activities on the Imhoff Road Bridge. The above described emergency response is the third such response that has occurred in the last five years. The other two locations were Halbrook Drive over Brookhaven Creek in 2016 and Main Street over Brookhaven Creek in 2018.

## **DISCUSSION:**

The Bridge Maintenance Program has been very successful in identifying the highest-risk bridge structures in the City. Since the 2019 Off-System Bridge Inspections, performed by H.W. Lochner, Inc. the City has seen a reduction in the number of structurally deficient bridges; from thirteen (13) in 2019 to four (4) today. To date, the method for selecting project locations for the Bridge Maintenance Program is based on the inspections performed by H.W. Lochner, Inc. and the subsequent list of structurally deficient bridge locations. Desiring to shift from a reactive approach to a proactive approach, Staff began development of an inventory ranking system.



In December of 2021, Staff reached out to H.W. Lochner, Inc., having been previously selected using a Qualifications-Based Selection process and having familiarity with the City's bridge inventory, to request a quote for consultation on a Bridge Maintenance Program Site Discovery project that would provide detailed cost estimates on locations identified by the inventory ranking system within the Bridge Maintenance Program. The list below identifies what Staff has identified as the top 20 priority locations to be evaluated by this Bridge Discovery phase utilizing the inventory ranking system and recent inspection data received in late December 2021.

- NBI 05274 – North Porter Avenue 0.6 Miles South of Franklin Road (Replace)
- NBI 06106 – 72nd Avenue NE 0.1 Miles South of Tecumseh Road (Replace)
- NBI 18958 – Imhoff Road 0.2 Miles East of Berry Road (Replace)
- NBI 09189 – 60th Avenue NE 0.5 Miles North of Rock Creek Road (Replace)
- NBI 05850 – East Robinson Street 0.6 Miles East of 72nd Avenue NE (Replace)
- NBI 09991 – Franklin Road 0.1 Miles West of 36th Avenue NE (Replace)
- NBI 18802 – 24th Avenue NE 0.9 Miles North of Robinson Street (Replace)
- NBI 05634 – 36th Avenue NE 0.2 Miles South of Robinson Street (Replace)
- NBI 07545 – 36th Avenue NE 0.5 Miles North of Franklin Road (Replace)
- NBI 26914 – East Post Oak Road 0.2 Miles East of 96th Avenue SE (Replace)
- NBI 07923 – Lindsey Street 0.1 Miles East of 48th Avenue SE (Replace)
- NBI 19451 – West Robinson Street 0.2 Miles East of 36th Avenue NW (Rehabilitate)
- NBI 19418 – Indian Hills Road 0.4 Miles East of Porter Avenue (Rehabilitate)
- NBI 12549 – Lindsey Street 100' West of Classen Boulevard (Rehabilitate)
- NBI 12330 – Boyd Street 500' East of Oklahoma Avenue (Rehabilitate)
- NBI 08335 – 24th Avenue SW 0.4 Miles south of Main Street (Rehabilitate)
- NBI 09865 – Robinson Street 0.5 Miles East of I-35 (Rehabilitate)
- NBI 10801 – Iowa Street 150' East of Merkle Drive (Rehabilitate)
- NBI 19348 – Franklin Road 0.2 Miles West of 72nd Avenue NE (Rehabilitate)
- NBI 18911 – Main Street 0.3 Miles East of 24th Avenue SW (Rehabilitate)

A map of the locations listed above is attached. The work performed by H.W. Lochner, Inc. will be necessary to ensure an additional measure of accuracy for future large-scale bridge improvement activities.

On Wednesday January 12, 2022 City Staff received a quote from H.W. Lochner, Inc. with a total project fee of \$61,400.

Funds are available in the Bridge Maintenance Program Design Account (TC0254; 50596687-46201). A transfer of \$61,400 is needed to the Bridge Maintenance Program Site Discovery Design Account (TC0282; Org 50596687; Object 46201).

#### **RECOMMENDATION:**

Staff recommends the approval of Contract K-2122-91 with H.W. Lochner, Inc. in the amount of \$61,400. Also, Staff recommends a transfer of \$61,400 from TC0254, Design to TC0282, Design as noted above.

## AGREEMENT FOR PROFESSIONAL SERVICES

THIS AGREEMENT is entered into between The City of Norman (OWNER) and H.W. Lochner, Inc. (CONSULTANT) for the following reasons:

1. The OWNER intends to perform a bridge discovery whereby detailed cost estimates for bridge replacement or rehabilitation are compiled for twenty (20) bridge locations within the City limits; and,
2. The OWNER requires certain professional administrative, technical, and analytical services in connection with the PROJECT (the SERVICES); and,
3. The CONSULTANT is prepared to provide the SERVICES.

In consideration of the promises contained in this AGREEMENT, the OWNER and the CONSULTANT agree as follows:

### ARTICLE 1 - EFFECTIVE DATE

The effective date of this AGREEMENT shall be the 26th day of January, 2022.

### ARTICLE 2 - GOVERNING LAW

This AGREEMENT shall be governed by the laws of the State of Oklahoma.

### ARTICLE 3 - SCOPE OF SERVICES

The CONSULTANT shall provide the SERVICES described in **Attachment A, Scope of Services**.

### ARTICLE 4 - SCHEDULE

The CONSULTANT shall exercise its reasonable efforts to perform the SERVICES described in **Attachment A, Scope of Services** to be completed by the 26th day of April, 2022, or 90 days from receipt of Notice to Proceed.

### ARTICLE 5 - COMPENSATION

The OWNER shall pay the CONSULTANT in accordance with **Attachment B, Compensation**. Invoices shall be due and payable upon receipt. The OWNER shall give prompt written notice of any disputed amount and shall pay the remaining amount.

### ARTICLE 6 - OWNER'S RESPONSIBILITIES

The OWNER shall be responsible for all matters described in **Attachment C, Owner's Responsibilities**. The OWNER hereby represents that it owns the intellectual property rights in any plans, documents or other materials provided by the OWNER to the CONSULTANT. If the OWNER does not own the intellectual property rights in such plans, documents or other materials, prior to providing same to the CONSULTANT, the OWNER shall obtain a license or right to use, including the right to sublicense to the CONSULTANT. The OWNER hereby grants the CONSULTANT the right to use the intellectual property associated with plans, documents or other materials it owns or has the right to use for the limited purpose of performing the SERVICES. The OWNER represents that the CONSULTANT's use of such documents will not infringe upon any third parties' rights.

#### **ARTICLE 7 - STANDARD OF CARE**

The same degree of care, skill, and diligence shall be exercised in the performance of the SERVICES as is ordinarily possessed and exercised by a member of the same profession, currently practicing, under similar circumstances. No other warranty, express or implied, is included in this AGREEMENT or in any drawing, specification, report, opinion, or other instrument of service, in any form or media, produced in connection with the SERVICES.

#### **ARTICLE 8 - INDEMNIFICATION AND LIABILITY**

Indemnification. To the extent permitted by law, including the Constitution of the State of Oklahoma, the CONSULTANT and the OWNER each hereby agree to defend, indemnify, and hold harmless the other party, its officers, servants, and employees, from and against any and all liability, loss, damage, cost, and expense (including attorneys' fees and accountants' fees) caused by an error, omission, or negligent act of the indemnifying party in the performance of SERVICES under this AGREEMENT. The CONSULTANT and the OWNER each agree to promptly serve notice on the other party of any claims arising hereunder, and shall cooperate in the defense of any such claims. This indemnity is not limited except as provided by applicable Oklahoma law. The acceptance by OWNER or its representatives of any certification of insurance providing for coverage other than as required in this Agreement to be furnished by the CONSULTANT shall in no event be deemed a waiver of any of the provisions of this indemnity provision. None of the foregoing provisions shall deprive the OWNER of any action, right, or remedy otherwise available to the OWNER at common law.

Survival. The terms and conditions of this ARTICLE shall survive completion of the SERVICES, or any termination of this Agreement.

#### **ARTICLE 9 - INSURANCE**

During the performance of the SERVICES under this Agreement, the CONSULTANT shall maintain the following insurance:

1. General Liability Workmen's Compensation and Auto Liability insurance in accordance with the Political Subdivision Tort Claims Act of the State of Oklahoma.
2. Adequate worker's compensation insurance coverage to comply with State laws and employer's liability coverage in the minimum amount of \$100,000.
3. Comprehensive general liability insurance coverage with a minimum of \$100,000 per person and \$300,000 per accident for bodily injury or death and \$100,000 for property damage.
4. Comprehensive automobile liability insurance coverage with a minimum of \$50,000 per person and \$300,000 per accident for bodily injury or death and \$100,000 for property damage.



5. Professional Liability (errors and omissions) Insurance Certificate providing a minimum of \$100,000 policy value.

The CONSULTANT shall, upon written request, furnish the OWNER certificates of insurance which shall include a provision that such insurance shall not be canceled without at least thirty days' written notice to the OWNER. The OWNER shall require all project contractors to include the OWNER, the CONSULTANT, and its parent company, affiliated and subsidiary entities, directors, officers and employees, as additional insureds on their General and Automobile Liability insurance policies, and to indemnify both the OWNER and the CONSULTANT, each to the same extent.

#### **ARTICLE 10 - LIMITATIONS OF RESPONSIBILITY**

The CONSULTANT shall not be responsible for: (a) construction means, methods, techniques, sequences, procedures, or safety precautions and programs in connection with the PROJECT that are outside of CONSULTANT's reasonable control and/or the **Scope of Services** set forth in **Attachment A**; (b) the failure of any contractor, subcontractor, vendor, or other PROJECT participant, not under contract to or otherwise under the control of the CONSULTANT, to fulfill contractual responsibilities to the OWNER or to comply with federal, state, or local laws, regulations, and codes; or (c) procuring permits, certificates, and licenses required for any construction unless such procurement responsibilities are specifically assigned to the CONSULTANT in **Attachment A, Scope of Services**. In the event the OWNER requests the CONSULTANT to execute any certificates or other documents, the proposed language of such certificates or documents shall be submitted to the CONSULTANT for review at least 15 days prior to the requested date of execution. The CONSULTANT shall not be required to execute any certificates or documents that in any way would, in CONSULTANT's sole judgment; (a) increase the CONSULTANT's legal or contractual obligations or risks beyond the terms of this AGREEMENT; (b) require knowledge, services or responsibilities beyond the scope of this AGREEMENT; or (c) result in the CONSULTANT having to certify, guarantee or warrant the existence of conditions which existence the CONSULTANT cannot reasonably ascertain.

#### **ARTICLE 11 - OPINIONS OF COST AND SCHEDULE**

Because the CONSULTANT has no control over the cost of labor, materials, or equipment furnished by others, or over the resources provided by others to meet project schedules, the CONSULTANT's opinion of probable costs and of project schedules shall be made on the basis of experience and qualifications as a practitioner of its profession. The CONSULTANT does not guarantee that proposals, bids, or actual project costs will not vary from the CONSULTANT's cost estimates or that actual schedules will not vary from the CONSULTANT's projected schedules.

#### **ARTICLE 12 - REUSE OF DOCUMENTS**

All documents, including, but not limited to, plans, drawings, and specifications prepared by the CONSULTANT as deliverables pursuant to the **Attachment A, Scope of Services** are instruments of service in respect to the PROJECT. They are not intended or represented to be suitable for reuse by the OWNER or others on modifications or extensions of the PROJECT or on any other project. Any reuse without prior written verification or adaptation by the CONSULTANT for the specific purpose intended will be at the OWNER's sole risk and without liability or legal exposure to the CONSULTANT. The OWNER shall indemnify and hold harmless the CONSULTANT and its

sub consultants against all judgments, losses, damages, injuries, and expenses, including reasonable attorneys' fees, arising out of or resulting from such reuse. Any verification or adaptation of documents will entitle the CONSULTANT to additional compensation at rates to be agreed upon by the OWNER and the CONSULTANT.

#### **ARTICLE 13 - OWNERSHIP OF DOCUMENTS AND INTELLECTUAL PROPERTY**

Except as otherwise provided herein, documents, drawings, and specifications prepared by the CONSULTANT and furnished to the OWNER as part of the SERVICES shall become the property of the OWNER; provided, however, that the CONSULTANT shall have the unrestricted right to their use. The CONSULTANT shall retain its copyright and ownership rights in its design, drawing details, specifications, data bases, computer software, and other proprietary property. Intellectual property developed, utilized, or modified in the performance of the SERVICES shall remain the property of the CONSULTANT.

#### **ARTICLE 14 - TERMINATION AND SUSPENSION**

This AGREEMENT may be terminated by either party upon written notice in the event of substantial failure by the other party to perform in accordance with the terms of this AGREEMENT; provided, however, the nonperforming party shall have 14 calendar days from the receipt of the termination notice to cure or to submit a plan for cure acceptable to the other party. The OWNER may terminate or suspend performance of this AGREEMENT for the OWNER's convenience upon written notice to the CONSULTANT. The CONSULTANT shall terminate or suspend performance of the SERVICES on a schedule acceptable to the OWNER, and the OWNER shall pay the CONSULTANT for all the SERVICES performed. Upon restart of suspended SERVICES, an equitable adjustment shall be made to the CONSULTANT's compensation and the project schedule.

#### **ARTICLE 15 - DELAY IN PERFORMANCE**

Neither the OWNER nor the CONSULTANT shall be considered in default of this AGREEMENT for delays in performance caused by circumstances beyond the reasonable control of the nonperforming party. For purposes of this AGREEMENT, such circumstances include, but are not limited to, abnormal weather conditions; floods; earthquakes; fire; epidemics; war, riots, and other civil disturbances; strikes, lockouts, work slowdowns, and other labor disturbances; sabotage; judicial restraint; and delay in or inability to procure permits, licenses, or authorizations from any local, state, or federal agency for any of the supplies, materials, accesses, or services required to be provided by either the OWNER or the CONSULTANT under this AGREEMENT. The CONSULTANT shall be granted a reasonable extension of time for any delay in its performance caused by any such circumstances. Should such circumstances occur, the nonperforming party shall, within a reasonable time of being prevented from performing, give written notice to the other party describing the circumstances preventing continued performance and the efforts being made to resume performance of this AGREEMENT.

#### **ARTICLE 16 - NOTICES**

Any notice required by this AGREEMENT shall be made in writing to the address specified below:



**OWNER:**

Joseph Hill  
Streets Program Manager  
City of Norman  
P.O. Box 370  
Norman, OK 73070  
(405) 366-7274 Phone

**CONSULTANT:**

H.W. Lochner, Inc.  
Evan Ludwig, Vice President  
701 Cedar Lake Blvd, Suite 230  
Oklahoma City, Oklahoma 73114  
1 (405) 748-6651

Nothing contained in this ARTICLE shall be construed to restrict the transmission of routine communications between representatives of the OWNER and the CONSULTANT.

**ARTICLE 17 - DISPUTES**

In the event of a dispute between the OWNER and the CONSULTANT arising out of or related to this AGREEMENT, the aggrieved party shall notify the other party of the dispute within a reasonable time after such dispute arises. If the parties cannot thereafter resolve the dispute, each party shall nominate a senior officer of its management to meet to resolve the dispute by direct negotiation or mediation. Should such negotiation or mediation fail to resolve the dispute, either party may pursue available legal remedies.

**ARTICLE 18 - EQUAL EMPLOYMENT OPPORTUNITY**

The CONSULTANT hereby affirms its support of affirmative action and that it abides by the provisions of the "Equal Opportunity Clause" of Section 202 of Executive Order 11246 and other applicable laws and regulations, including the City of Norman's policies and provisions. The CONSULTANT affirms its policy to recruit and hire employees without regard to race, age, color, religion, sex, sexual preference/orientation, marital status, citizen status, national origin or ancestry, presence of a disability or status as a Veteran of the Vietnam era or any other legally protected status. It is the CONSULTANT's policy to treat employees equally with respect to compensation, advancement, promotions, transfers and all other terms and conditions of employment. The CONSULTANT further affirms completion of applicable governmental employer information reports including the EEO-1 and VETS-1 00 reports, and maintenance of a current Affirmative Action Plan as required by Federal regulations.

**ARTICLE 19 - WAIVER**

A waiver by either the OWNER or the CONSULTANT of any breach of this AGREEMENT shall be in writing. Such a waiver shall not affect the waiving party's rights with respect to any other or further breach.

**ARTICLE 20 - SEVERABILITY**

The invalidity, illegality, or unenforceability of any provision of this AGREEMENT or the occurrence of any event rendering any portion or provision of this AGREEMENT void shall in no

way affect the validity or enforceability of any other portion or provision of this AGREEMENT. Any void provision shall be deemed severed from this AGREEMENT, and the balance of this AGREEMENT shall be construed and enforced as if it did not contain the particular portion or provision held to be void. The parties further agree to amend this AGREEMENT to replace any stricken provision with a valid provision that comes as close as possible to the intent of the stricken provision. The provisions of this ARTICLE shall not prevent this entire AGREEMENT from being void should a provision which is of the essence of this AGREEMENT be determined void.

#### **ARTICLE 21 - INTEGRATION**

This AGREEMENT, including **Attachments A, B, and C** incorporated by this reference, represents the entire and integrated AGREEMENT between the OWNER and the CONSULTANT. It supersedes all prior and contemporaneous communications, representations, and agreements, whether oral or written, relating to the subject matter of this AGREEMENT.

#### **ARTICLE 22 - SUCCESSORS AND ASSIGNS**

The OWNER and the CONSULTANT each binds itself and its successors, executors, administrators, permitted assigns, legal representatives and, in the case of a partnership, its partners, to the other party to this AGREEMENT and to the successors, executors, administrators, permitted assigns, legal representatives, and partners of such other party in respect to all provisions of this AGREEMENT.

#### **ARTICLE 23 - ASSIGNMENT**

Neither the OWNER nor the CONSULTANT shall assign any rights or duties under this AGREEMENT without the prior written consent of the other party, which consent shall not be unreasonably withheld; provided, however, the CONSULTANT may assign its rights to payment without the OWNER's consent. Unless otherwise stated in the written consent to an assignment, no assignment will release or discharge the assignor from any obligation under this AGREEMENT. Nothing contained in this ARTICLE shall prevent the CONSULTANT from engaging independent consultants, associates and subcontractors to assist in the performance of the SERVICES.

#### **ARTICLE 24 - NO THIRD PARTY RIGHTS**

The SERVICES provided for in this AGREEMENT are for the sole use and benefit of the OWNER and the CONSULTANT. Nothing in this AGREEMENT shall be construed to give any rights or benefits to anyone other than the OWNER and the CONSULTANT.

IN WITNESS WHEREOF, OWNER and H.W. Lochner, Inc. have executed this AGREEMENT.

DATED this 19th day of January, 2022.

The City of Norman  
(OWNER)

Signature \_\_\_\_\_

Name \_\_\_\_\_

Title \_\_\_\_\_

Date \_\_\_\_\_

Attest:

\_\_\_\_\_  
City Clerk

H.W. Lochner, Inc.  
(CONSULTANT)

Signature Evan Ludwig

Name Evan Ludwig

Title Vice President

Date 1-19-2022

Attest:

Donelle Mez  
Secretary

Approved as to form and legality this \_\_\_\_\_ day of \_\_\_\_\_ 2022.

\_\_\_\_\_  
City Attorney



January 12, 2022

Brandon Brooks, EIT  
Staff Engineer  
City of Norman  
201 West Gray, Bldg. A  
Norman, OK 73069

H.W. Lochner, Inc.  
701 Cedar Lake Blvd.  
Suite 230  
Oklahoma City, OK 73114  
T 405.748.6651  
hwlochner.com

RE: Bridge Bond Discovery Proposal

Mr. Brooks,

We are writing to provide a proposal for the development of detailed cost estimates for the proposed bridge bond program. The City of Norman has identified 20 bridges that will be replaced or rehabilitated through the bond program. The bridge locations are as follows:

- NBI 05274 – North Porter Avenue 0.6 Miles South of Franklin Road (Replace)
- NBI 06106 – 72<sup>nd</sup> Avenue NE 0.1 Miles South of Tecumseh Road (Replace)
- NBI 18958 – Imhoff Road 0.2 Miles East of Berry Road (Replace)
- NBI 09189 – 60<sup>th</sup> Avenue NE 0.5 Miles North of Rock Creek Road (Replace)
- NBI 05850 – East Robinson Street 0.6 Miles East of 72<sup>nd</sup> Avenue NE (Replace)
- NBI 09991 – Franklin Road 0.1 Miles West of 36<sup>th</sup> Avenue NE (Replace)
- NBI 18802 – 24<sup>th</sup> Avenue NE 0.9 Miles North of Robinson Street (Replace)
- NBI 05634 – 36<sup>th</sup> Avenue NE 0.2 Miles South of Robinson Street (Replace)
- NBI 07545 – 36<sup>th</sup> Avenue NE 0.5 Miles North of Franklin Road (Replace)
- NBI 26914 – East Post Oak Road 0.2 Miles East of 96<sup>th</sup> Avenue SE (Replace)
- NBI 07923 – Lindsey Street 0.1 Miles East of 48<sup>th</sup> Avenue SE (Replace)
- NBI 19451 – West Robinson Street 0.2 Miles East of 36<sup>th</sup> Avenue NW (Rehabilitate)
- NBI 19418 – Indian Hills Road 0.4 Miles East of Porter Avenue (Rehabilitate)
- NBI 12549 – Lindsey Street 100' West of Classen Boulevard (Rehabilitate)
- NBI 12330 – Boyd Street 500' East of Oklahoma Avenue (Rehabilitate)
- NBI 08335 – 24<sup>th</sup> Avenue SW 0.4 Miles south of Main Street (Rehabilitate)
- NBI 09865 – Robinson Street 0.5 Miles East of I-35 (Rehabilitate)
- NBI 10801 – Iowa Street 150' East of Merkle Drive (Resurface)
- NBI 19348 – Franklin Road 0.2 Miles West of 72<sup>nd</sup> Avenue NE (Rehabilitate)
- NBI 18911 – Main Street 0.3 Miles East of 24<sup>th</sup> Avenue SW (Rehab & Resurface)

Frontier Land Surveying, LLC will perform all survey services required. Refer to the attached exhibit from Frontier Land Surveying for the survey scope of work and services.

Olsson will perform all hydraulic services for the following structures:

- NBI 05274 – North Porter Avenue 0.6 Miles South of Franklin Road
- NBI 06106 – 72<sup>nd</sup> Avenue NE 0.1 Miles South of Tecumseh Road
- NBI 18958 – Imhoff Road 0.2 Miles East of Berry Road

Olsson will perform hydrology calculations and develop an HY8 model for each proposed bridge replacement for existing conditions and modify that model to estimate the proposed bridge size. A memo style report will be provided and will include the following in the memo:

- Contributing drainage area
- Computed flow rate used to size the proposed structure
- Size of the existing structure
- Size of the proposed structure

H.W. Lochner, Inc. will perform all detailed cost estimates for the proposed bridges. A line-item opinion of probable costs will be provided for each bridge with estimates for right-of-way, utilities, and engineering and inspection costs.

Our proposed fee schedule for these services is attached with this proposal. The total project fee, \$61,400.00, is a not-to-exceed value. If there should be a change in scope that leads to additional work or additional services are requested, we will negotiate a fee to provide the changes or additional services. Final documents will be delivered 90 days from receipt of notice to proceed. To expedite contract work prior to City Council approval, we request the City of Norman approve an initial amount of \$30,000.00 to begin the critical path items in schedule and allow work to begin immediately.

Thank you for the opportunity to submit this proposal. If you have any questions or need additional information, please contact me at (405) 418-5882.

Respectfully,



Evan Ludwig, PE  
Vice President  
**H.W. LOCHNER, INC.**

January 7, 2022



Survey Proposal  
For  
**LOCHNER**

City of Norman  
Hydraulics Study of Structures  
Multiple Locations (TBD)  
Design Survey Proposal

For:  
Evan Ludwig, P.E.  
Project Manager  
LOCHNER  
701 Cedar Lake Blvd, Suite 230  
Oklahoma City, OK 73114

### Understanding of Project

Pursuant to your request, Frontier Land Surveying is pleased to present this proposal for the City of Norman Hydraulics study. This following describes our understanding of the scope of work and services your project requires.

### Structures to be surveyed

- NBI 06106 – 72<sup>nd</sup> Ave NE over Rock Creek
- NBI 18958 – Imhoff Rd over Imhoff Creek
- NBI 05274 – N. Porter Ave over Little River

### SURVEY SERVICES

Frontier will perform topographic services at drain structure locations to provide items requested for a hydraulics study. Following is a list of items included but not limited to:

- Utilize VRS Network or existing control if provided to establish temporary control at each location for data collection.
- Collect a minimum of one channel cross section downstream of the existing structure at a location as close as possible to the existing bridge that is not hydraulically impacted by the existing bridge.
- Collect one cross section upstream within a maximum of 100 feet of existing bridge.
- Flowline elevations on upstream and downstream faces of the existing bridge
- Roadway elevations at the center of the bridge and each end of the bridge (at the centerline of the road)
- Existing bridge size and dimensions
  - If a bridge: the number of piers, the dimensions and orientation of piers and the elevation of the abutments
  - If an RCB: the dimensions of each barrel, the top elevation of the structure as well as the flowline
- The location and the elevation of the roadway low point closest to the bridge (fuse plug)

### DELIVERABLE

- Following is a list of deliverables for the design topographic services:
  - Microstation DGN:
    - Topographic/planimetric features
    - Cross sections

Lump Sum Survey Fee (per structure)

\$11,400 (\$3,800)

### Items not included in this estimate of services, but available upon request

- Private/commercial parcels/boundaries adjacent to structures.
- Easements/right-of-way
- Utility locations
- Elevation certificate
- Construction staking services

### NOTES

- Project Schedule
  - Following "Notice to Proceed", it is expected that Frontier can begin work on the project or will communicate a reasonable schedule of work
- Frontier will NOT be responsible for filing any needed documents with the appropriate governing bodies NOR any fees resulting from the filing.
- Frontier makes no guarantee as to the entirety of title documents when asked to perform title research on private or commercial land without the benefit of a title commitment.
- Changes in scope
 

If there should be a change in project scope, limits, or additional phases that leads to additional work or additional services requested by CLIENT, we will negotiate a fee to provide the changes or additional services prior to commencing work.



- If at any point the request of services is terminated, after an executed "Notice to Proceed" by the client, Frontier shall be compensated for services rendered up to that point.
- This survey will be placed on the NGS Oklahoma State Plane Coordinate System, Lambert Projection, South Zone – unless otherwise directed by client.

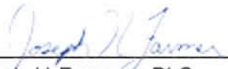
#### INDEMNITY

Frontier Land Surveying, LLC and LOCHNER each agree to indemnify and hold the other harmless, and their respective officers, employees, agents and representatives, from and against liability for all claims, losses, damages and expenses, including reasonable attorneys' fees, to the extent such claims, losses, damages, or expenses are caused by the indemnifying party's negligent acts, errors or omissions. In the event claims, losses, damages or expenses are caused by the joint or concurrent negligence of Frontier and LOCHNER, they shall be borne by each party in proportion to its negligence. The amount of the liability shall not exceed the total compensation received by Frontier under this Agreement.

Thank you for your consideration of this proposal for services. If this proposal meets with your approval, please indicate your approval by signing below. Email to [joe@fls-survey.com](mailto:joe@fls-survey.com). If you have any questions or need additional information, please call me at 405.285.0433.

Submitted for approval by:

Accepted by:

  
 \_\_\_\_\_  
 Joseph H Farmer, PLS  
 Vice President

\_\_\_\_\_

Print: \_\_\_\_\_

Date: January 7, 2022

Date: \_\_\_\_\_

Title: \_\_\_\_\_

## DESIGN SERVICES

TOTAL HOURS =  
HOURLY RATES =  
SUBTOTAL =

TOTAL HOURS =	350
TOTAL DIRECT LABOR =	\$13,573.00

### OUT-OF-POCKET EXPENSES

**TOTAL FEE**

## ATTACHMENT C OWNER'S RESPONSIBILITIES

ARTICLE 6 of the AGREEMENT is amended and supplemented to include the following agreement of the parties.

### OWNER RESPONSIBILITIES

1. The OWNER shall furnish to the CONSULTANT all available information pertinent to the PROJECT including previous reports, construction plans and any other data relative to the PROJECT.
2. The OWNER shall be responsible for all permit fees.
3. The OWNER shall examine all studies, reports, sketches, estimates specifications, plan drawings, proposals and any other documents presented by the CONSULTANT and render in writing decisions pertaining thereto within a reasonable time so as not to delay the SERVICES of the CONSULTANT.
4. The OWNER shall designate in writing a person to act as its representative in respect to the work to be performed under this AGREEMENT, and such person shall have complete authority to transmit instructions, receive information, interpret, and define the OWNER's policies and decisions with respect to materials, equipment, elements and systems pertinent to the services covered by this AGREEMENT.
5. The OWNER shall provide for meeting facilities (or arrange for meeting facilities) for all PROJECT meetings with OWNER or Public Meetings in connection with the PROJECT.



# CERTIFICATE OF LIABILITY INSURANCE

 DATE(MM/DD/YYYY)  
01/18/2022

THIS CERTIFICATE IS ISSUED AS A MATTER OF INFORMATION ONLY AND CONFERS NO RIGHTS UPON THE CERTIFICATE HOLDER. THIS CERTIFICATE DOES NOT AFFIRMATIVELY OR NEGATIVELY AMEND, EXTEND OR ALTER THE COVERAGE AFFORDED BY THE POLICIES BELOW. THIS CERTIFICATE OF INSURANCE DOES NOT CONSTITUTE A CONTRACT BETWEEN THE ISSUING INSURER(S), AUTHORIZED REPRESENTATIVE OR PRODUCER, AND THE CERTIFICATE HOLDER.

**IMPORTANT:** If the certificate holder is an ADDITIONAL INSURED, the policy(ies) must have ADDITIONAL INSURED provisions or be endorsed. If SUBROGATION IS WAIVED, subject to the terms and conditions of the policy, certain policies may require an endorsement. A statement on this certificate does not confer rights to the certificate holder in lieu of such endorsement(s).

<b>PRODUCER</b> Aon Risk Services Northeast, Inc. New York NY Office One Liberty Plaza 165 Broadway, Suite 3201 New York NY 10006 USA		<b>CONTACT NAME:</b> PHONE (A/C No. Ext): (866) 283-7122 FAX (A/C No.): (800) 363-0105 <b>E-MAIL ADDRESS:</b>	
<b>INSURED</b> H.W. Lochner, Inc. 225 W. Washington Street, 12th Floor Chicago IL 60606 USA		<b>INSURER(S) AFFORDING COVERAGE</b>	<b>NAIC #</b>
		INSURER A: The Charter Oak Fire Insurance Company	25615
		INSURER B: Beazley Insurance Company, Inc.	37540
		INSURER C: Travelers Property Casualty Ins Co	36161
		INSURER D: Travelers Property Cas Co of America	25674
		INSURER E:	
		INSURER F:	

## COVERAGES

CERTIFICATE NUMBER: 570091331076

REVISION NUMBER:

THIS IS TO CERTIFY THAT THE POLICIES OF INSURANCE LISTED BELOW HAVE BEEN ISSUED TO THE INSURED NAMED ABOVE FOR THE POLICY PERIOD INDICATED. NOTWITHSTANDING ANY REQUIREMENT, TERM OR CONDITION OF ANY CONTRACT OR OTHER DOCUMENT WITH RESPECT TO WHICH THIS CERTIFICATE MAY BE ISSUED OR MAY PERTAIN, THE INSURANCE AFFORDED BY THE POLICIES DESCRIBED HEREIN IS SUBJECT TO ALL THE TERMS, EXCLUSIONS AND CONDITIONS OF SUCH POLICIES. LIMITS SHOWN MAY HAVE BEEN REDUCED BY PAID CLAIMS.

Limits shown are as requested

INSR LTR	TYPE OF INSURANCE	ADOL SUBR INSD WVD	POLICY NUMBER	POLICY EFF (MM/DD/YYYY)	POLICY EXP (MM/DD/YYYY)	LIMITS
A	<input checked="" type="checkbox"/> COMMERCIAL GENERAL LIABILITY <input type="checkbox"/> CLAIMS-MADE <input checked="" type="checkbox"/> OCCUR  GEN'L AGGREGATE LIMIT APPLIES PER <input type="checkbox"/> POLICY <input checked="" type="checkbox"/> PRO-JECT <input type="checkbox"/> LOC <input type="checkbox"/> OTHER		P6308451B877C0F21	05/01/2021	05/01/2022	EACH OCCURRENCE \$1,000,000 DAMAGE TO RENTED PREMISES (Ea occurrence) \$500,000 MED EXP (Any one person) \$5,000 PERSONAL & ADV INJURY \$1,000,000 GENERAL AGGREGATE \$2,000,000 PRODUCTS - COMP/OP AGG \$2,000,000
A	AUTOMOBILE LIABILITY <input checked="" type="checkbox"/> ANY AUTO <input type="checkbox"/> OWNED AUTOS ONLY <input type="checkbox"/> HIRED AUTOS ONLY <input type="checkbox"/> SCHEDULED AUTOS <input type="checkbox"/> NON-OWNED AUTOS ONLY		810-9M511530-21-43-G	05/01/2021	05/01/2022	COMBINED SINGLE LIMIT (Ea accident) \$1,000,000 BODILY INJURY (Per person) BODILY INJURY (Per accident) PROPERTY DAMAGE (Per accident)
C	<input checked="" type="checkbox"/> UMBRELLA LIAB <input checked="" type="checkbox"/> OCCUR <input type="checkbox"/> EXCESS LIAB <input type="checkbox"/> CLAIMS-MADE <input type="checkbox"/> DED <input checked="" type="checkbox"/> RETENTION \$10,000		ZUP10P6385421NF	05/01/2021	05/01/2022	EACH OCCURRENCE \$15,000,000 AGGREGATE \$15,000,000
D	WORKERS COMPENSATION AND EMPLOYERS' LIABILITY ANY PROPRIETOR / PARTNER / EXECUTIVE OFFICER/MEMBER EXCLUDED? (Mandatory in NH) If yes, describe under DESCRIPTION OF OPERATIONS below	Y/N <input checked="" type="checkbox"/> Y <input type="checkbox"/> N/A	UB4K2046172143G	05/01/2021	05/01/2022	<input checked="" type="checkbox"/> PER STATUTE <input type="checkbox"/> OTHER E L EACH ACCIDENT \$1,000,000 E L DISEASE-EA EMPLOYEE \$1,000,000 E L DISEASE-POLICY LIMIT \$1,000,000
B	Env CPL/Prof		V2AEA4210201	05/01/2021	05/01/2022	Professional Liab. \$10,000,000 SIR \$750,000 Pollution Liab. Lim \$10,000,000

DESCRIPTION OF OPERATIONS / LOCATIONS / VEHICLES (ACORD 101, Additional Remarks Schedule, may be attached if more space is required)

Re: Job Project No. PN 19832 City of Norman Bridge Bond Program  
 Certificate holder, The Owner, The Consultant and it's parent company, affiliated and subsidiary entities, directors, officers, and employees are included as Additional Insureds in accordance with the provisions of the General Liability, Automobile Liability policies.

## CERTIFICATE HOLDER

## CANCELLATION

City of Norman PO Box 370 Norman OK 73070 USA	SHOULD ANY OF THE ABOVE DESCRIBED POLICIES BE CANCELLED BEFORE THE EXPIRATION DATE THEREOF, NOTICE WILL BE DELIVERED IN ACCORDANCE WITH THE POLICY PROVISIONS.  AUTHORIZED REPRESENTATIVE  <i>Aon Risk Services Northeast Inc.</i>
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## Page \_ of \_

AGENCY Aon Risk Services Northeast, Inc.		NAMED INSURED H.W. Lochner, Inc.
POLICY NUMBER See Certificate Number: 570091331076		
CARRIER See Certificate Number: 570091331076	NAIC CODE	EFFECTIVE DATE

**FORM NUMBER:** ACORD 25 **FORM TITLE:** Certificate of Liability Insurance

INSURER(S) AFFORDING COVERAGE	NAIC #
INSURER	
INSURER	
INSURER	
INSURER	

If a policy below does not include limit information, refer to the corresponding policy on the ACORD certificate form for policy limits.

[illegible]



# CERTIFICATE OF LIABILITY INSURANCE

Page 1

Item 9.

DATE (MM/DD/YYYY)  
01/18/2022

THIS CERTIFICATE IS ISSUED AS A MATTER OF INFORMATION ONLY AND CONFERS NO RIGHTS UPON THE CERTIFICATE HOLDER. THIS CERTIFICATE DOES NOT AFFIRMATIVELY OR NEGATIVELY AMEND, EXTEND OR ALTER THE COVERAGE AFFORDED BY THE POLICIES BELOW. THIS CERTIFICATE OF INSURANCE DOES NOT CONSTITUTE A CONTRACT BETWEEN THE ISSUING INSURER(S), AUTHORIZED REPRESENTATIVE OR PRODUCER, AND THE CERTIFICATE HOLDER.

IMPORTANT: If the certificate holder is an ADDITIONAL INSURED, the policy(ies) must have ADDITIONAL INSURED provisions or be endorsed. If SUBROGATION IS WAIVED, subject to the terms and conditions of the policy, certain policies may require an endorsement. A statement on this certificate does not confer rights to the certificate holder in lieu of such endorsement(s).

<b>PRODUCER</b> Willis Towers Watson Midwest, Inc. c/o 26 Century Blvd P.O. Box 305191 Nashville, TN 372305191 USA		<b>CONTACT NAME:</b> Willis Towers Watson Certificate Center <b>PHONE (A/C, No, Ext):</b> 1-877-945-7378 <b>FAX (A/C, No):</b> 1-888-467-2378 <b>E-MAIL ADDRESS:</b> certificates@willis.com	
<b>INSURED</b> H.W. Lochner, Inc. 225 West Washington, Suite 1200 Chicago, IL 60606		<b>INSURER(S) AFFORDING COVERAGE</b> <b>INSURER A:</b> Beazley Insurance Company Inc <b>INSURER B:</b> <b>INSURER C:</b> <b>INSURER D:</b> <b>INSURER E:</b> <b>INSURER F:</b>	
		<b>NAIC #</b> 37540	

**COVERAGES****CERTIFICATE NUMBER:** W23797085**REVISION NUMBER:**

THIS IS TO CERTIFY THAT THE POLICIES OF INSURANCE LISTED BELOW HAVE BEEN ISSUED TO THE INSURED NAMED ABOVE FOR THE POLICY PERIOD INDICATED. NOTWITHSTANDING ANY REQUIREMENT, TERM OR CONDITION OF ANY CONTRACT OR OTHER DOCUMENT WITH RESPECT TO WHICH THIS CERTIFICATE MAY BE ISSUED OR MAY PERTAIN, THE INSURANCE AFFORDED BY THE POLICIES DESCRIBED HEREIN IS SUBJECT TO ALL THE TERMS, EXCLUSIONS AND CONDITIONS OF SUCH POLICIES. LIMITS SHOWN MAY HAVE BEEN REDUCED BY PAID CLAIMS.

INSR LTR	TYPE OF INSURANCE	ADDL SUBR INSD WVD	POLICY NUMBER	POLICY EFF (MM/DD/YYYY)	POLICY EXP (MM/DD/YYYY)	LIMITS
	<b>COMMERCIAL GENERAL LIABILITY</b> <input type="checkbox"/> CLAIMS-MADE <input type="checkbox"/> OCCUR  GEN'L AGGREGATE LIMIT APPLIES PER: <input type="checkbox"/> POLICY <input type="checkbox"/> PRO-JECT <input type="checkbox"/> LOC <input type="checkbox"/> OTHER					EACH OCCURRENCE \$ DAMAGE TO RENTED PREMISES (Ea occurrence) \$ MED EXP (Any one person) \$ PERSONAL & ADV INJURY \$ GENERAL AGGREGATE \$ PRODUCTS - COMP/OP AGG \$ \$
	<b>AUTOMOBILE LIABILITY</b> <input type="checkbox"/> ANY AUTO <input type="checkbox"/> OWNED AUTOS ONLY <input type="checkbox"/> SCHEDULED AUTOS <input type="checkbox"/> HIRED AUTOS ONLY <input type="checkbox"/> NON-OWNED AUTOS ONLY					COMBINED SINGLE LIMIT (Ea accident) \$ BODILY INJURY (Per person) \$ BODILY INJURY (Per accident) \$ PROPERTY DAMAGE (Per accident) \$ \$
	<b>UMBRELLA LIAB</b> <input type="checkbox"/> OCCUR <b>EXCESS LIAB</b> <input type="checkbox"/> CLAIMS-MADE <input type="checkbox"/> DED <input type="checkbox"/> RETENTION \$					EACH OCCURRENCE \$ AGGREGATE \$ \$
	<b>WORKERS COMPENSATION AND EMPLOYERS' LIABILITY</b> ANY PROPRIETOR/PARTNER/EXECUTIVE OFFICER/MEMBER EXCLUDED? <input type="checkbox"/> Y <input checked="" type="checkbox"/> N (Mandatory in NH) If yes, describe under DESCRIPTION OF OPERATIONS below					PER STATUTE <input type="checkbox"/> OTH-ER <input type="checkbox"/> E.L. EACH ACCIDENT \$ E.L. DISEASE - EA EMPLOYEE \$ E.L. DISEASE - POLICY LIMIT \$
A	Professional Liability		V2AEA4210201	05/01/2021	05/01/2022	Per Claim: \$1,000,000

DESCRIPTION OF OPERATIONS / LOCATIONS / VEHICLES (ACORD 101, Additional Remarks Schedule, may be attached if more space is required)

Job: City of Norman Bridge Bond Program.

Job No./Contract No./Lease No.: PN #19832

**CERTIFICATE HOLDER****CANCELLATION**

SHOULD ANY OF THE ABOVE DESCRIBED POLICIES BE CANCELLED BEFORE THE EXPIRATION DATE THEREOF, NOTICE WILL BE DELIVERED IN ACCORDANCE WITH THE POLICY PROVISIONS.

AUTHORIZED REPRESENTATIVE

City of Norman  
PO Box 370  
Norman, OK 73070

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ACORD 25 (2016/03)

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SR ID: 22112840

BATCH: 2377491

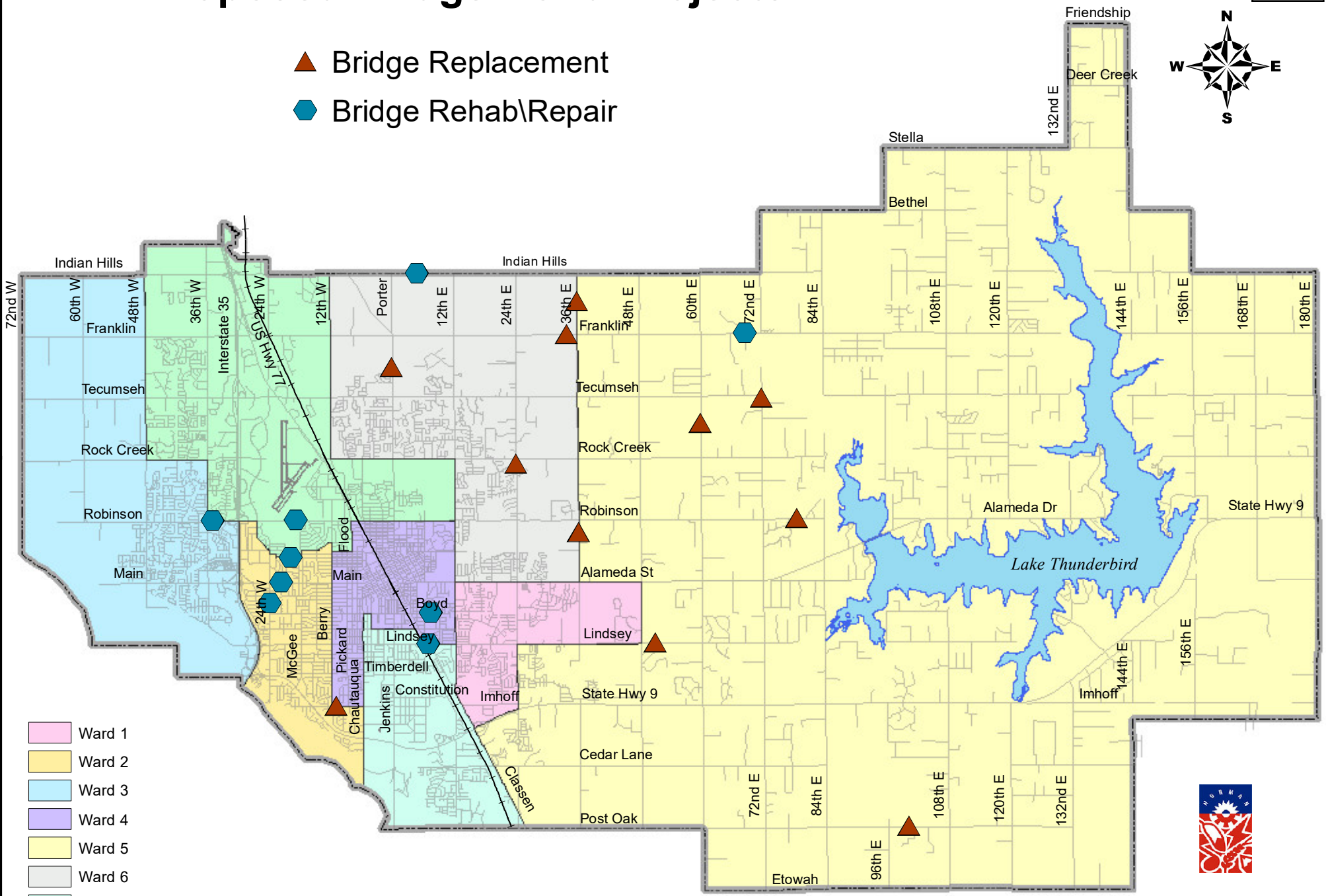
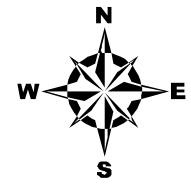
227

1-19-22

# Proposed Bridge Bond Projects

Item 9.

- ▲ Bridge Replacement
- ◆ Bridge Rehab\Repair



- Ward 1
- Ward 2
- Ward 3
- Ward 4
- Ward 5
- Ward 6
- Ward 7
- Ward 8

January 11, 2022

0 2 Miles



Map Produced by the City of Norman  
Geographic Information System  
The City of Norman assumes  
responsibility for errors or omissions  
in the information presented

**File Attachments for Item:**

10. CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT AND/OR POSTPONEMENT OF RESOLUTION R-2122-82: A RESOLUTION OF THE COUNCIL OF THE CITY OF NORMAN APPROPRIATING \$83,778 FROM THE CAPITAL FUND BALANCE TO ADJUST THE LOCAL SHARE DEPOSIT PAYMENT TO THE OKLAHOMA DEPARTMENT OF TRANSPORTATION FOR THE CONSTITUTION STREET MULTIMODAL PATH PROJECT BETWEEN JENKINS AVENUE AND CLASSEN BOULEVARD.





## CITY OF NORMAN, OK STAFF REPORT

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**MEETING DATE:** 01/25/2022

**REQUESTER:** Katherine Coffin

**PRESENTER:** Michael Rayburn, Capitol Projects Engineer

**ITEM TITLE:** CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT AND/OR POSTPONEMENT OF RESOLUTION R-2122-82: A RESOLUTION OF THE COUNCIL OF THE CITY OF NORMAN APPROPRIATING \$83,778 FROM THE CAPITAL FUND BALANCE TO ADJUST THE LOCAL SHARE DEPOSIT PAYMENT TO THE OKLAHOMA DEPARTMENT OF TRANSPORTATION FOR THE CONSTITUTION STREET MULTIMODAL PATH PROJECT BETWEEN JENKINS AVENUE AND CLASSEN BOULEVARD.

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### BACKGROUND:

The design of the Constitution Street Multimodal Path, between Jenkins Avenue and Classen Boulevard, began on December 11, 2018, when design contract K-1718-75 Amendment 1 was approved by City Council with the engineering firm Freese and Nichols. The project will provide a multimodal 10'-wide concrete sidewalk on the north side of Constitution Street, and will include modification of two prestressed concrete bridges on this corridor to accommodate it. The project design required coordination with the BNSF Railway, and "buy-in" in terms of right-of-way and tree removal/replacement from the OU Golf Course which is adjacent to the proposed path.

For this project, a federal Transportation Alternative Project (TAP) grant of \$600,000 and local funding for the remaining portion will pay for the construction costs and the railroad work required. On November 30, 2021, the City Council approved Maintenance, Funding, and Right-of-Way Agreement K-2122-70 and Resolution R-2122-62 with the Oklahoma Department of Transportation (ODOT), as well as payment authorization of \$545,459 to ODOT, to be paid when invoiced, in advance of the bid opening. The City was never officially invoiced for this amount.

All of the pre-construction activity on this project has been completed by the City of Norman. A bid opening is tentatively scheduled for April 14, 2022. Construction is anticipated to begin in July 2022, with completion around March 2023.

### DISCUSSION:

The local share of the funding was based on a total 90% project estimate of \$1,083,617. On January 7, 2022, the final plans were completed and submitted with an adjusted final project estimate of \$1,167,395. The \$83,778 difference in the estimates will have to be paid with local

funding since the federal TAP grant is capped at \$600,000. The total local funding, then, will change from \$545,459 (comprised of \$483,617 remainder + \$61,842 railroad work) to \$629,237 (comprised of \$567,395 remainder + \$61,842 railroad work). ODOT has now officially invoiced the City in two parts to keep separate the local share of construction funding (\$567,395) and the local share railroad funding (\$61,842) for a total of \$629,237. Copies of these invoices are attached.

To summarize the funding sources of the local share, most of the funds (\$395,459) will come from the Norman Forward Project New Trail Development Throughout Town, Construction (Account 51790097-46101; Project NFP107); a portion (\$150,000) already encumbered to ODOT in the FYE 2022 Capital Improvement Plan, Other Traffic Special Projects, Construction (Account 50596688-46101; Project TR-0111); and the remaining \$83,778 to be appropriated into Other Traffic Special Projects, Construction (50596688-46101; Project TR-0111) from the FYE 2022 Capital Fund balance (50-29000).

**RECOMMENDATION:**

Staff recommends approval of Resolution R-2122-82 for an appropriation of an additional payment authorization to ODOT of \$83,778, from the Capital Fund Balance (Account 50-29000) to the FYE 2022 Capital Improvement Plan, Other Traffic Special Projects, Construction (Account 50596688-46101; Project TR-0111) for the increase in the construction cost estimate for the Constitution Street Multimodal Path project.

# Resolution

R-2122-82

A RESOLUTION OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, APPROPRIATING \$83,778 FROM THE CAPITAL FUND BALANCE TO INCREASE THE LOCAL SHARE PAYMENT TO THE OKLAHOMA DEPARTMENT OF TRANSPORTATION (ODOT) FROM \$545,459 TO \$629,237 FOR THE CONSTITUTION STREET MULTIMODAL PATH PROJECT BETWEEN JENKINS AVENUE AND CLASSEN BOULEVARD.

- § 1. WHEREAS, on November 30, 2021, City Council approved Contract K-2122-70 with ODOT for the Constitution Street Multimodal Path Project between Jenkins Avenue and Classen Boulevard, Federal-Aid Project TAP-214E (102) AG, State Job 33271(04), and
- § 2. WHEREAS, the funding for the project includes a Transportation Alternative Project (TAP) Grant of \$600,000 and local funding to pay for the construction costs and railroad work; and
- § 3. WHEREAS, the local share of the funding was originally \$545,459 and after the final plans were completed the local share construction cost estimate increased to \$629,237, a difference of \$83,778; and

NOW, THEREFORE, BE IT RESOLVED BY THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA:

- § 4. That the following appropriation be made for the reason stated above:

<u>Gaining Account Name</u>	<u>Losing Account Name</u>	<u>Amount</u>
Project TR0111, Constitution Multi Modal Path Ext, Construction (5059688-46101)	Capital Fund Balance (50-29000)	\$83,778

PASSED AND ADOPTED this 25th day of January, 2022.

\_\_\_\_\_  
Mayor

ATTEST:

\_\_\_\_\_  
City Clerk





## OKLAHOMA DEPARTMENT OF TRANSPORTATION

## INVOICE

Make check PAYABLE, and MAIL TO:

Oklahoma Department of Transportation  
 Comptroller Division  
 200 N.E. 21<sup>st</sup> Street  
 Oklahoma City, Oklahoma 73105-3204

To: City of Norman

Department Invoice No 33271(04)-01  
 1/11/2022

Project Type	Div	County	JP No	Project No.	Work Type	Description
ENHANCEMENT	03	Cleveland	33271(04)	TAP-214E(102)AG	PEDESTRIAN / BIKE IMPROVEMENTS	NORMAN: CONSTITUTION ST MULTIMODAL PATH

## Construction JP# 33271(04)

Description – Explanation of Charge	Total	Federal	Sponsor
Due Date: Upon Receipt			
Total Estimate for Project Construction with 6% inspection fee included	\$1,167,395	\$600,000	\$567,395
Total	\$1,167,395		
Accounting Use Only		Invoice Total	\$567,395

Distribution:

City  
 Remit with Payment  
 Division Project File  
 Comptroller Division

## OKLAHOMA DEPARTMENT OF TRANSPORTATION

## INVOICE

Make check PAYABLE, and MAIL TO:

Oklahoma Department of Transportation  
 Comptroller Division  
 200 N.E. 21<sup>st</sup> Street  
 Oklahoma City, Oklahoma 73105-3204

To: City of Norman

Department Invoice No 33271(05)-01  
 1/11/2022

Project Type	Div	County	JP No	Project No.	Work Type	Description
ENHANCEMENT	03	Cleveland	33271(04)(05)	TAP-214E(102)AG	PEDESTRIAN / BIKE IMPROVEMENTS	NORMAN: CONSTITUTION ST MULTIMODAL PATH

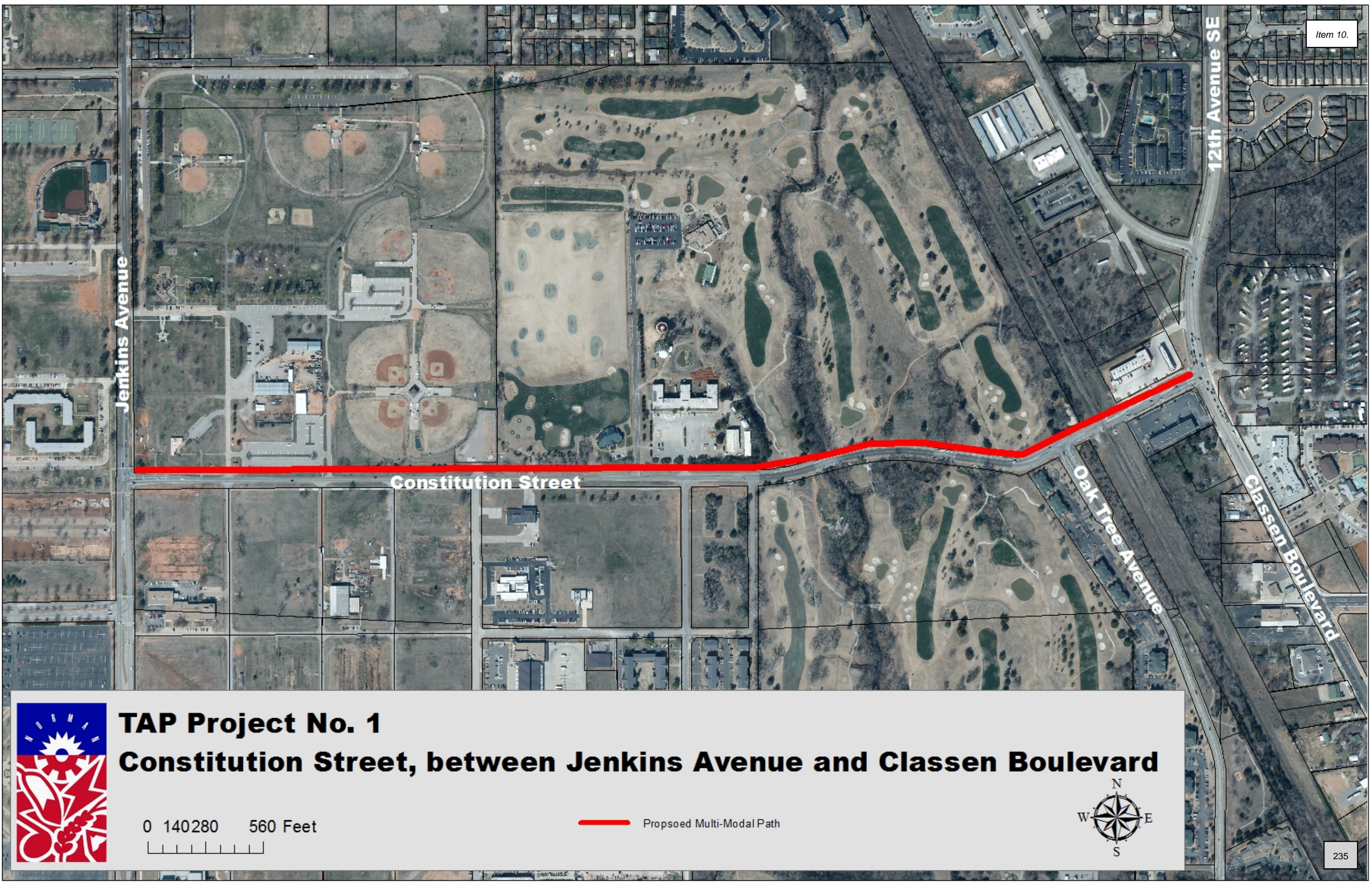
## Construction JP# 33271(04)

Description – Explanation of Charge	Total	Federal	Sponsor
Due Date: Upon Receipt  (05) Railroad Construction	\$61,842	\$0	\$61,842
Accounting Use Only		Invoice Total	\$61,842.00

Distribution:

City  
 Remit with Payment  
 Division Project File  
 Comptroller Division





# **TAP Project No. 1**

## **Constitution Street, between Jenkins Avenue and Classen Boulevard**

0 140 280 560 Feet

Proposed Multi-Modal Path





**File Attachments for Item:**

11. CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF RESOLUTION R-2122-57: A RESOLUTION OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, AMENDING THE NORMAN 2025 LAND USE AND TRANSPORTATION PLAN SO AS TO REMOVE PART OF THE NORTHWEST QUARTER (NW/4) OF SECTION TWENTY-NINE (29), TOWNSHIP NINE (9) NORTH, RANGE TWO (2) WEST OF THE INDIAN MERIDIAN (I.M.), CLEVELAND COUNTY, OKLAHOMA, FROM THE INSTITUTIONAL DESIGNATION AND PLACE THE SAME IN THE MIXED USE DESIGNATION. (NRHS Porter Campus)



## CITY OF NORMAN, OK STAFF REPORT

**MEETING DATE:** 01/25/2022

**REQUESTER:** Norman Regional Health System and City of Norman

**PRESENTER:** Jane Hudson, Director, Planning & Community Development

**ITEM TITLE:** CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF RESOLUTION R-2122-57: A RESOLUTION OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, AMENDING THE NORMAN 2025 LAND USE AND TRANSPORTATION PLAN SO AS TO REMOVE PART OF THE NORTHWEST QUARTER (NW/4) OF SECTION TWENTY-NINE (29), TOWNSHIP NINE (9) NORTH, RANGE TWO (2) WEST OF THE INDIAN MERIDIAN (I.M.), CLEVELAND COUNTY, OKLAHOMA, FROM THE INSTITUTIONAL DESIGNATION AND PLACE THE SAME IN THE MIXED USE DESIGNATION. (NRHS Porter Campus)

### SUMMARY OF REQUEST:

The applicants are proposing redevelopment of the Porter Campus of the Norman Regional Health System. The property is approximately 29.3 acres. This development proposal requires rezoning from R-1, Single Family Dwelling District, C-3, Intensive Commercial District, and O-1, Office-Institutional District, to PUD, Planned Unit Development, and a NORMAN 2025 Land Use and Transportation Plan amendment from Institutional to Mixed Use. Proposed uses include a Behavioral Health Facility, a Variety Care facility, the City's Senior Wellness Center, an office building, a multi-use facility, and existing medical facilities.

### STAFF ANALYSIS:

For changes in classification under the NORMAN 2025 Land Use and Transportation Plan, the following information is forwarded for consideration.

The role of the NORMAN 2025 Plan in the City's ongoing and diverse planning activities states the document must be flexible, and that it is updated and amended periodically. The Plan defines the desired land use patterns for use and development of all private sector properties. This Plan will serve as a policy guide for zoning and planning requests as they are presented to the Planning Commission and City Council.

1. ***Has there been a change in circumstances resulting from development of the properties in the general vicinity which suggest that the proposed change will not be contrary to the public interest?*** Norman Regional Hospital is an anchor of this area of Norman. The Norman Regional Hospital System has proposed the redevelopment

of the Porter Campus as part of the Inspire Health program. On April 2, 2019, the “Porter Avenue Streetscape” project was approved by voters as part of the 2019 Transportation Bond issue. The project has redesigned the streetscape of Porter Avenue from Robinson Street to Alameda Drive to provide a more pedestrian friendly, walkable environment. This will help spur redevelopment along Porter Avenue in this area.

The community continues to reinvest in this area. Cleveland County recently redeveloped the County Fairgrounds on the north side of Robinson Street. A new strip mall was built on the corner of Porter Avenue and E. Mosier Street. The Sonic just south of the Porter Campus completely redeveloped and rebuilt a new restaurant. Dimensions School redeveloped the building that was previously medical offices at the corner of Findlay Avenue and Oliver Street. A new restaurant, HTeaO, is building a new location at the corner of Porter Avenue and E. Himes Street. And finally, a new senior apartment complex/living facility was constructed on the north side of Robinson Street, directly north of this proposed campus.

**2. *Is there a determination that the proposed change would not result in adverse land use or adverse traffic impacts to surrounding properties or the vicinity?***

The City Traffic Engineer states the following in the review form for this development:

“While no negative traffic impacts are anticipated, however based on the traffic impact analysis there was an increase in delay at the signalized intersection of Robinson Street and Porter Avenue. This would cause the intersection to operate at an unacceptable level of service, therefore a modification to the eastbound Robinson Street approach to Porter Avenue, along with signal timing adjustments, is recommended with staff concurrence. The modification includes adding a right turn lane for the eastbound approach to turn south onto Porter Avenue.”

With this information, no further adverse land use or traffic impacts are anticipated with this development. The uses will be similar to existing uses on the site.

**CONCLUSION:**

Staff forwards this request, Resolution R-2122-57, for City Council’s consideration.

At their December 9, 2021 meeting, Planning Commission unanimously recommended adoption of Resolution R-2122-57, by a vote of 7-0.

R-2122-57

A RESOLUTION OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, AMENDING THE NORMAN 2025 LAND USE AND TRANSPORTATION PLAN SO AS TO REMOVE PART OF THE NORTHWEST QUARTER (NW/4) OF SECTION TWENTY-NINE (29), TOWNSHIP NINE (9) NORTH, RANGE TWO (2) WEST OF THE INDIAN MERIDIAN (I.M.), CLEVELAND COUNTY, OKLAHOMA, FROM THE INSTITUTIONAL DESIGNATION AND PLACE THE SAME IN THE MIXED USE DESIGNATION.  
(NRHS Porter Campus)

- § 1. WHEREAS, the Council of the City of Norman recognizes citizens' concerns about the future development of Norman; and
- § 2. WHEREAS, the City Council at its meeting of November 16, 2004, reviewed and adopted the NORMAN 2025 Land Use and Transportation Plan, with an effective date of December 16, 2004; and
- § 3. WHEREAS, Norman Regional Health System and the City of Norman have requested that the following described property be moved from the Institutional Designation and placed in the Mixed Use Designation for the hereinafter described property, to wit:

A tract of land being all of Blocks 1 and 2, WHITWELL ADDITION recorded in Book 1 of Plats, Page 26; AND all of Blocks 1 and 2, GRIFFIN HEIGHTS ADDITION recorded in Book 2 of Plats, Page 62; AND all of Block 1, NORTHEAST ADDITION recorded in Book 1 of Plats, Page 92; AND all of Blocks 1 and 2, Lots 1 through 13, Block 3 and Lots 1 through 12, Block 4, HIGH SCHOOL ADDITION recorded in Book 1 of Plats, Page 32, together with the platted streets and alleys lying within the aforescribed plats AND those unplatted parts of the Northwest Quarter (NW/4) of Section 29, Township 9 North, Range 2 West of the Indian Meridian, Norman, Cleveland County, Oklahoma all lying within the following described tract of land;

COMMENCING at the northwest corner of said Northwest Quarter; THENCE South 00°09'01" East, along the west line of said Northwest Quarter, a distance of 553.00 feet to the POINT OF BEGINNING; THENCE North 89°50'59" East a distance of 420.40 feet; THENCE North 00°09'01" West a distance of 237.84 feet to a point on the south line of Lot1, Block 1, DOCTOR'S PARK NO. 1 recorded in Book 8 of Plats, Page 77 extended; THENCE North 89°44'54" East, along the south line of said Lot 1 extended, the south line of said Lot 1 and the south line of Lot 1, Block 1, DOCTOR'S PARK NO. 2 recorded in Book 10 of Plats, Page 39, a distance of 615.57 feet to the southeast corner of said Lot 1, also being a point on the west right of way line of Findlay Avenue; THENCE South 00°09'01" East, along said west right of way line and the east line of Block 1 of said GRIFFIN HEIGHTS ADDITION extended and the east line of said Block 1, a distance of 859.73 feet; THENCE South 89°26'27" West a distance of 8.00 feet to a point of intersection with the east line of Block 1, NORTHEAST ADDITION extended; THENCE South 00°09'01" East, along the east line of said Block 1 extended, the east line of said Block 1 and said west right of way line, a distance of 335.00 feet; THENCE North 89°26'27" East a distance of 8.00 feet;



THENCE South 00°09'01" East a distance of 165.00 feet to a point of intersection with the south line of Block 4, HIGH SCHOOL ADDITION extended; THENCE South 89°26'27" West, along said south line extended, the south line of said Block 4 and the south line of Block 3 in said Addition, a distance of 1,003.00 feet to the southwest corner of said Block 3, also being a point on the east right of way line of Porter Avenue; THENCE North 00°09'01" West, along the west line of said Block 3, the west line of Block 2 of said HIGH SCHOOL ADDITION, the west line of Block 1, WHITWELL ADDITION and said east right of way line, a distance of 952.35 feet to the northwest corner of said Block 1, WHITWELL ADDITION; THENCE South 89°26'27" West a distance of 33.00 feet to a point on the west line of said Northwest Quarter; THENCE North 00°09'01" West, along said west line, a distance of 175.84 feet to the POINT OF BEGINNING.

Said described tract of land contains an area of 1,277,585 square feet or 29.3293 acres, more or less.

NOW, THEREFORE BE IT RESOLVED BY THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA:

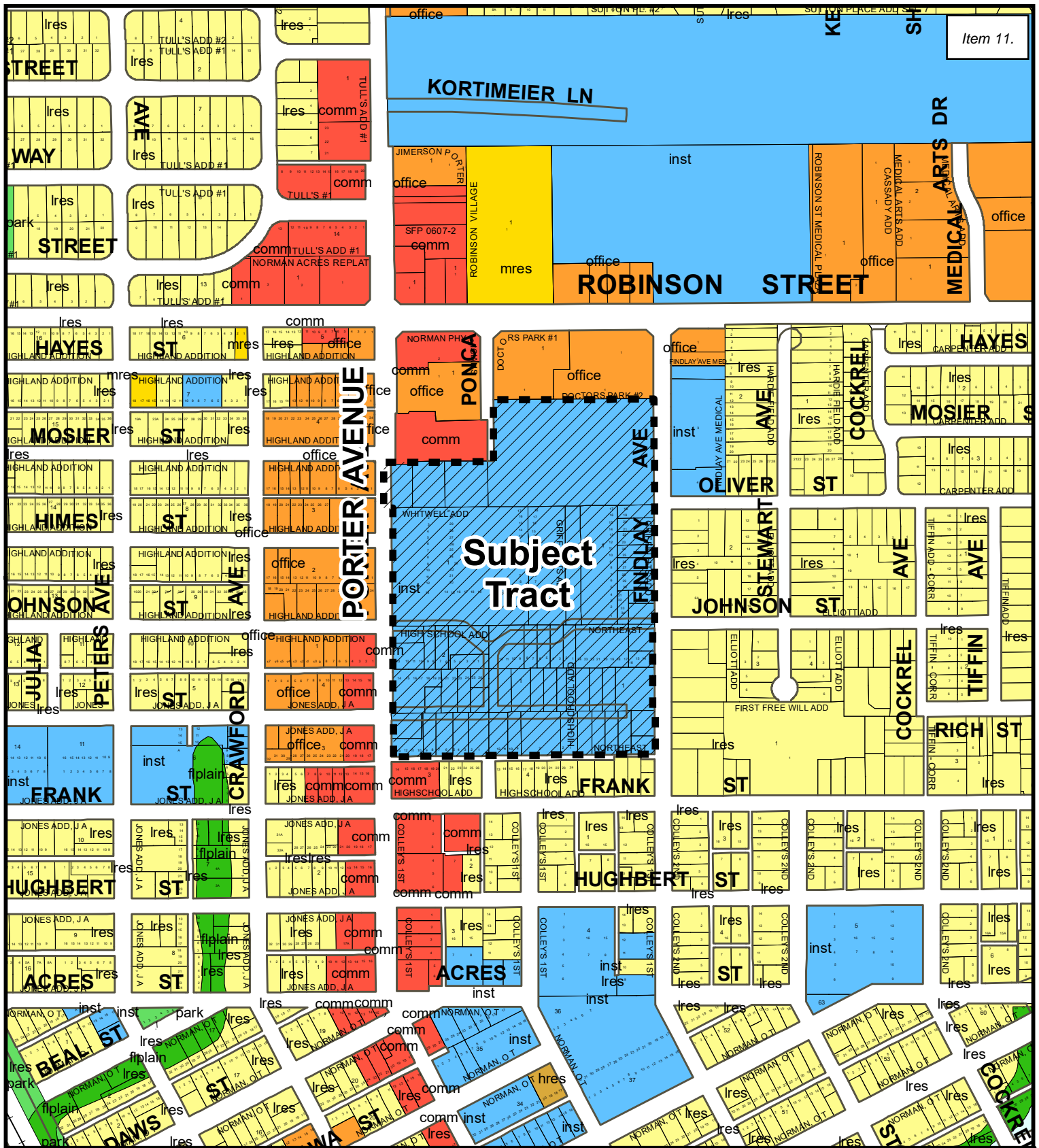
- § 4. That the Council of the City of Norman recognizes the need to control the future growth of the City of Norman; and, that after due consideration has determined that the requested amendment to the NORMAN 2025 Land Use and Transportation Plan should be adopted, and does hereby approve the requested designation.

PASSED AND ADOPTED this \_\_\_\_\_ day of \_\_\_\_\_, 2022.

\_\_\_\_\_  
(Mayor)

ATTEST:

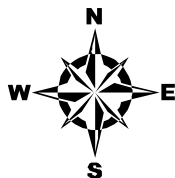
\_\_\_\_\_  
(City Clerk)



# Norman 2025 Land Use Plan



Map Produced by the City of Norman  
Geographic Information System.  
The City of Norman assumes no  
responsibility for errors or omissions  
in the information presented.



November 4, 2021

0 250 500 Ft.



Subject Tract



Zoning

RESOLUTION NO. R-2122-57

ITEM NO. 10

**STAFF REPORT**

**ITEM:** Norman Regional Health System and the City of Norman request amendment of the NORMAN 2025 Land Use and Transportation Plan from Institutional Designation to Mixed Use Designation for approximately 29.3 acres of property generally located south of E. Robinson Street, west of N. Findlay Avenue, north of E. Frank Street, and east of N. Porter Avenue.

**SUMMARY OF REQUEST:** The applicants are proposing redevelopment of the Porter Campus of the Norman Regional Health System. The property is approximately 29.3 acres. This development proposal requires rezoning from R-1, Single Family Dwelling District, C-3, Intensive Commercial District, and O-1, Office-Institutional District, to PUD, Planned Unit Development, and a NORMAN 2025 Land Use and Transportation Plan amendment from Institutional to Mixed Use.

**STAFF ANALYSIS:** For changes in classification under the NORMAN 2025 Land Use and Transportation Plan, the following information is forwarded for consideration.

The role of the NORMAN 2025 Plan in the City's ongoing and diverse planning activities states the document must be flexible, and that it is updated and amended periodically. The Plan defines the desired land use patterns for use and development of all private sector properties. This Plan will serve as a policy guide for zoning and planning requests as they are presented to the Planning Commission and City Council.

1. ***Has there been a change in circumstances resulting from development of the properties in the general vicinity which suggest that the proposed change will not be contrary to the public interest?*** Norman Regional Hospital is an anchor of this area of Norman. The Norman Regional Hospital System has proposed the redevelopment of the Porter Campus as part of the Inspire Health program. On April 2, 2019, the "Porter Avenue Streetscape" project was approved by voters as part of the 2019 Transportation Bond issue. The project has redesigned the streetscape of Porter Avenue from Robinson Street to Alameda Drive to provide a more pedestrian friendly, walkable environment. This will help spur redevelopment along Porter Avenue in this area.

The community continues to reinvest in this area. Cleveland County recently redeveloped the County Fairgrounds on the north side of Robinson Street. A new strip mall was built on the corner of Porter Avenue and E. Mosier Street. The Sonic just south of the Porter Campus completely redeveloped and rebuilt a new restaurant. Dimensions School redeveloped the building that was previously medical offices at the corner of Findlay Avenue and Oliver Street. A new restaurant, HTeaO, is building a new location at the corner of Porter Avenue and E. Himes Street. And finally, a new senior apartment complex/living facility was constructed on the north side of Robinson Street, directly north of this proposed campus.

2. ***Is there a determination that the proposed change would not result in adverse land use or adverse traffic impacts to surrounding properties or the vicinity?*** The City Traffic Engineer states the following in the review form for this development:

"While no negative traffic impacts are anticipated, however based on the traffic impact analysis there was an increase in delay at the signalized intersection of Robinson Street and Porter Avenue. This would cause the intersection to operate at an unacceptable level of service, therefore a modification to the eastbound Robinson Street approach to Porter Avenue, along with signal timing adjustments, is recommended with staff concurrence. The modification includes adding a right turn lane for the eastbound approach to turn south onto Porter Avenue."

With this information, no further adverse land use or traffic impacts are anticipated with this development. The uses will be similar to existing uses on the site.

**CONCLUSION:** Staff forwards this request, Resolution No. R-2122-57, for Planning Commission's consideration.

**Applicant:** Norman Regional Hospital/City of Norman

**Project Location:** NRH Porter Campus Site

**Case Number:** PD21-39

**Time:** 6:30 p.m.

**Applicant/Representative**

Shawn Rieger  
Troy Glover  
John Manfred  
Richie Splitt  
Paula Price

**Attendees**

Danny Millsap  
Randall Foster  
Jerry Cos  
Judy Hatfield  
Alex Snoddy  
Mark Cox

**City Staff**

Jane Hudson, Planning Director  
Kathryn Walker, City Attorney  
Jason Olsen, Parks Director

**Application Summary**

The applicant is requesting to rezone the NRH Porter Campus Site to a PUD, Planned Unit Development to allow for the development of a Mixed Use Development.

**Neighbor's Comments/Concerns/Responses**

What is the overall timeline?

Senior Center first, 12 – 13-month build

What is the plan for the hospital growth?

Rebuilding – future growth, there are no plans to vacate the site, we will be keeping the anchor educational facility and building around it.

Will there be a Phasing Plan/timeline?

Senior Center, Behavioral Medicine and simultaneously maybe Variety Care – done by 2023 – 2024.

When the services that are going to move to the West Campus are complete, the demolition of specific areas will begin, we are not demoing the educational facility. Once demo is complete, we will start the rebuild of the campus. The “S” curve street will not be constructed until approximately 75% of the campus site is complete.

The plan is to mimic the Porter Corridor design concept for the reconstruction of the campus.

**NORMAN PLANNING COMMISSION  
REGULAR SESSION MINUTES**

**DECEMBER 9, 2021**

The Planning Commission of the City of Norman, Cleveland County, State of Oklahoma, met in Regular Session in the Council Chambers of the Norman Municipal Building, 201 West Gray Street, on the 9<sup>th</sup> day of December, 2021.

Notice and agenda of the meeting was posted at the Norman Municipal Building and online at <https://norman-ok.municodemeetings.com> at least twenty-four hours prior to the beginning of the meeting.

Chair Erica Bird called the meeting to order at 6:30 p.m.

\* \* \*

**ROLL CALL**

**MEMBERS PRESENT**

Erin Williford (arrived at 6:32 p.m.)  
Kevan Parker  
Steven McDaniel  
Erica Bird  
Dave Boeck  
Sandy Bahan  
Michael Jablonski

**MEMBERS ABSENT**

Lark Zink

A quorum was present.

**STAFF MEMBERS PRESENT**

Jane Hudson, Director, Planning &  
Community Development  
Lora Hoggatt, Planning Services Manager  
Logan Hubble, Planner I  
Anais Starr, Planner II  
Colton Wayman, Planner I  
Roné Tromble, Recording Secretary  
Ken Danner, Subdivision Development  
Manager  
Kathryn Walker, City Attorney  
Heather Poole, Asst. City Attorney  
Jeanne Snider, Asst. City Attorney  
Jami Short, Traffic Engineer  
Bryce Holland, Multimedia Specialist

\* \* \*

Item No. 10, being:

**CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF RESOLUTION NO. R-2122-57 FOR NORMAN REGIONAL HEALTH SYSTEM AND THE CITY OF NORMAN REQUEST AMENDMENT OF THE NORMAN 2025 LAND USE AND TRANSPORTATION PLAN FROM INSTITUTIONAL DESIGNATION TO MIXED USE DESIGNATION FOR APPROXIMATELY 29.3 ACRES OF PROPERTY GENERALLY LOCATED SOUTH OF E. ROBINSON STREET, WEST OF N. FINDLAY AVENUE, NORTH OF E. FRANK STREET, AND EAST OF N. PORTER AVENUE.**

**ITEMS SUBMITTED FOR THE RECORD:**

1. NORMAN 2025 Map
2. Staff Report
3. Pre-Development Summary

Item No. 11, being:

**CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF ORDINANCE NO. O-2122-29 FOR NORMAN REGIONAL HEALTH SYSTEM AND THE CITY OF NORMAN REQUEST REZONING FROM R-1, SINGLE FAMILY DWELLING DISTRICT, C-3, INTENSIVE COMMERCIAL DISTRICT, AND O-1, OFFICE-INSTITUTIONAL DISTRICT, TO PUD, PLANNED UNIT DEVELOPMENT, FOR APPROXIMATELY 29.3 ACRES OF PROPERTY GENERALLY LOCATED SOUTH OF E. ROBINSON STREET, WEST OF N. FINDLAY AVENUE, NORTH OF E. FRANK STREET, AND EAST OF N. PORTER AVENUE.**

**ITEMS SUBMITTED FOR THE RECORD:**

1. Location Map
2. Staff Report
3. PUD Narrative with Exhibits A-D
4. Pre-Development Summary

Item No. 12, being:

**CONSIDERATION OF APPROVAL, ACCEPTANCE, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF PP-2122-9, A PRELIMINARY PLAT SUBMITTED BY NORMAN REGIONAL HEALTH SYSTEM AND THE CITY OF NORMAN (SMC CONSULTING ENGINEERS, P.C.) FOR NORMAN REGIONAL HEALTH SYSTEM PORTER CAMPUS, A PLANNED UNIT DEVELOPMENT FOR APPROXIMATELY 29.3 ACRES OF PROPERTY GENERALLY LOCATED SOUTH OF E. ROBINSON STREET, WEST OF N. FINDLAY AVENUE, NORTH OF E. FRANK STREET, AND EAST OF N. PORTER AVENUE.**

**ITEMS SUBMITTED FOR THE RECORD:**

1. Location Map
2. Preliminary Plat
3. Staff Report
4. Transportation Impacts
5. Preliminary Site Development Plan
6. Master Site & Utility Development Plan
7. Pre-Development Summary

Item No. 13, being:

**CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF ORDINANCE NO. O-2122-30 FOR NORMAN REGIONAL HEALTH SYSTEM AND THE CITY OF NORMAN REQUEST VACATION AND CLOSURE OF CERTAIN PUBLIC INTERESTS IN AN ALLEY, A PORTION OF PONCA AVENUE, AND A PORTION OF GRIFFIN AVENUE.**

**ITEMS SUBMITTED FOR THE RECORD:**

1. Location Map
2. Staff Report
3. Request to Vacate/Close Public Easement

**PRESENTATION BY STAFF:**

1. Ms. Lora Hoggatt presented the staff report. The Parks Board met earlier in the day and recommended fee in lieu to be assigned to the Frances Cate Park, instead of an on-site park.



Commissioner Jablonski asked a question about the traffic study.  
Ms. Jami Short, Traffic Engineer, responded.

**PRESENTATION BY THE APPLICANT:**

Mr. Sean Rieger, representing the applicant, made comments.  
Mr. Richie Splitt, CEO, talked about the Inspire Health program.  
Mr. Sean Rieger explained the mechanics of the site, the zoning and platting and the rights-of-way and easements to be closed and vacated.  
Ms. Kathryn Walker, City Attorney, discussed the Adult Wellness Center.  
Commissioner Boeck made comments.  
Mr. Todd Butler, Traffic Engineering Consultants, discussed traffic issues at Porter and Robinson.  
Commissioner Parker thanked the team for their work.  
Commissioner Bird asked about emergency services at the site.  
Mr. Richie Splitt responded.  
Commissioner Bird asked about the proposed nine lots.  
Mr. Sean Rieger and Mr. Richie Splitt responded.

**AUDIENCE PARTICIPATION:**

Rachelle HardIn-Montz, 2601 132<sup>nd</sup> Avenue S.E., made comments regarding the behavioral health facility.

**DISCUSSION AND ACTION BY THE PLANNING COMMISSION:**

Commissioner Boeck made comments.  
Commissioner Bird made comments.

*Dave Boeck moved to recommend adoption of Resolution No. R-2122-57, Ordinance No. O-2122-29, PP-2122-9, and Ordinance No. O-2122-30 to City Council. Kevan Parker seconded the motion.*

There being no further discussion, a vote on the motion was taken with the following result:

YEAS	Erin Williford, Kevan Parker, Steven McDaniel, Erica Bird, Dave Boeck, Sandy Bahan, Michael Jablonski
NAYES	None
MEMBERS ABSENT	Lark Zink

The motion, to recommend adoption of Resolution No. R-2122-57, Ordinance No. O-2122-29, PP-2122-9, and Ordinance No. O-2122-30 to City Council, passed by a vote of 7-0.

\*\*\*

**File Attachments for Item:**

12. CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF ORDINANCE O-2122-29 UPON SECOND AND FINAL READING: AN ORDINANCE OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, AMENDING SECTION 460 OF CHAPTER 22 OF THE CODE OF THE CITY OF NORMAN SO AS TO REMOVE PART OF THE NORTHWEST QUARTER (NW/4) OF SECTION TWENTY-NINE (29), TOWNSHIP NINE (9) NORTH, RANGE TWO (2) WEST OF THE INDIAN MERIDIAN, TO NORMAN, CLEVELAND COUNTY, OKLAHOMA, FROM THE R-1, SINGLE FAMILY DWELLING DISTRICT, THE C-3, INTENSIVE COMMERCIAL DISTRICT, AND THE O-1, OFFICE-INSTITUTIONAL DISTRICT, AND PLACE SAME IN THE PUD, PLANNED UNIT DEVELOPMENT DISTRICT; AND PROVIDING FOR THE SEVERABILITY THEREOF. (NORMAN REGIONAL HEALTH SYSTEM PORTER CAMPUS)



## CITY OF NORMAN, OK STAFF REPORT

**MEETING DATE:** 01/25/2022

**REQUESTER:** Norman Regional Health System and City of Norman

**PRESENTER:** Jane Hudson, Director, Planning & Community Development

**ITEM TITLE:** CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF ORDINANCE O-2122-29 UPON SECOND AND FINAL READING: AN ORDINANCE OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, AMENDING SECTION 460 OF CHAPTER 22 OF THE CODE OF THE CITY OF NORMAN SO AS TO REMOVE PART OF THE NORTHWEST QUARTER (NW/4) OF SECTION TWENTY-NINE (29), TOWNSHIP NINE (9) NORTH, RANGE TWO (2) WEST OF THE INDIAN MERIDIAN, TO NORMAN, CLEVELAND COUNTY, OKLAHOMA, FROM THE R-1, SINGLE FAMILY DWELLING DISTRICT, THE C-3, INTENSIVE COMMERCIAL DISTRICT, AND THE O-1, OFFICE-INSTITUTIONAL DISTRICT, AND PLACE SAME IN THE PUD, PLANNED UNIT DEVELOPMENT DISTRICT; AND PROVIDING FOR THE SEVERABILITY THEREOF. (NORMAN REGIONAL HEALTH SYSTEM PORTER CAMPUS)

**SYNOPSIS:** The applicants, Norman Regional Health System and the City of Norman, are requesting to rezone the subject property with approximately 29.3 acres to PUD, Planned Unit Development. The subject property is currently the site of the Porter Campus of Norman Regional Hospital. The applicant is requesting to redevelop as a mixed use site, in addition to the site planned for the Senior Wellness Center.

**HISTORY:** On July 13, 1954, City Council adopted Ordinance No. 884, which placed the R-1, Single-Family Dwelling District, and C-3, Intensive Commercial District, portions of the subject property into these zoning districts. The portions of the subject property that are currently zoned O-1, Office-Institutional District, were rezoned to O-1 with Ordinance Nos. O-9900-9 (adopted October 12, 1999), O-9900-52 (adopted June 13, 2000), and O-0203-18 (adopted December 17, 2002). Over the years, Norman Regional Health System has developed the subject property into the Porter Campus for the hospital. Numerous doctors' offices have also been developed.

### **ZONING ORDINANCE CITATION:**

#### **SEC. 420 – PLANNED UNIT DEVELOPMENT**

Statement of Purpose. It is the intent of this section to encourage developments with a superior built environment brought about through unified development and to provide for the application of design ingenuity in such developments while protecting existing and future surrounding areas

in achieving the goals of the comprehensive plan of record. The "PUD" Planned Unit Development district herein established is intended to provide for greater flexibility in the design of buildings, yards, courts, circulation, and open space than would otherwise be possible through the strict application of other district regulations. In this way, applicants may be awarded certain premiums in return for assurances of overall planning and design quality, or which will be of exceptional community benefit and which are not now required by other regulations. By permitting and encouraging the use of such procedures, the Planning Commission and City Council will be able to make more informed land use decisions and thereby guide development more effectively in the best interest of the health, safety, and welfare of the City.

Specifically, the purposes of this section are to encourage:

- (a) A maximum choice in the types of environment and living units available to the public.
- (b) Provision of more usable and suitably located open space, recreation areas, or other common facilities than would otherwise be required under conventional land development regulations.
- (c) Maximum enhancement and minimal disruption of existing natural features and amenities.
- (d) Comprehensive and innovative planning and design of diversified developments which are consistent with the City's long range plan and remain compatible with surrounding developments.
- (e) More efficient and economic use of land resulting in smaller networks of utilities and streets, thereby lowering costs.
- (f) Preparation of more complete and useful information which will enable the Planning Commission and City Council to make more informed decisions on land use.

The PUD (Planned Unit Development) Regulations are designed to provide for small and large scale developments incorporating a single type or a variety of residential, commercial, industrial and related uses which are planned and developed as a unit. Such development may consist of individual lots, or it may have common building sites. Private or public common land and open space must be an essential, major element of the development which is related to, and affects, the long term value of the homes and other development. A Planned Unit Development shall be a separate entity with a distinct character that respects and harmonizes with surrounding development.

**EXISTING ZONING:** A majority of the subject property is zoned R-1 which allows for single-family residential and accessory uses. A hospital is not a use permitted by right or by Special Use application in R-1. Medical offices are allowed by right in O-1 and C-3 zoning districts. The subject property is within the Porter Corridor Commercial Development area.

**ANALYSIS:** The particulars of this PUD include:

USE: The applicant listed what uses will be allowed in the PUD Narrative, Exhibit E, "Allowable Uses."

**OPEN SPACE:** The PUD Narrative states the development will have a minimum of 10% space open. The proposed open space areas are shown on the Open Space Exhibit D.

**PARKING:** Parking for the development will meet or exceed the requirements of Section 431.5, Off-Street Parking Requirements. Parking lots abutting residential properties will be designed in accordance with the Porter Corridor Zoning Overlay District regulations, except that no buffer walls shall be required. Buffer landscaping and parking lot lighting regulations will still apply.

**PHASES:** The Porter Campus will be developed in multiple phases. The initial phase is anticipated to include the City's Senior Wellness Center and behavioral health care.

**SITE PLAN/ACCESS:** The proposed site development plan shows six access points off Findlay Avenue and two access points off Porter Avenue. A new interior street will be constructed. The existing EMSTAT building on the north of the subject property will remain. Two mixed use buildings and an office building are proposed on the east side of the property. NRHS will have three buildings on the northwest side of the property off Porter Avenue; the NRHS North building and the NRHS Education building will remain. A new NRHS building is proposed between the two existing buildings. The City's Senior Wellness Center is proposed for the southeast corner of the property. A variety care site and a BMS site are proposed for the southwest corner of the property. There are parking lots and walking trails proposed throughout the development.

**AREA REGULATIONS:** The PUD Narrative states that the lots within the development have minimal setbacks "in order to create a vibrant, walkable, and high-quality development." There are no minimum setbacks for buildings except for a 20' setback along the south border of the Porter Campus property and a 20' setback from Findlay Avenue. The maximum height for buildings within 100' from a residentially zoned property will be three stories. Otherwise, buildings have no maximum height in the development.

**LANDSCAPING:** Landscaping for the development will comply with Section 431.8, Landscaping Requirements for Off-Street Parking Facilities.

**SIGNAGE:** The PUD Narrative states all signs will comply with the Commercial Zone Sign Standards in Section 18-504 with the following exceptions. Grounds signs on the Porter Campus shall be allowed at zero setback so long as all ground signs do not block the applicable sight triangle. Development entrance signage and tenant identification signage shall be allowed at the entrances of the Porter Campus. Any off-premises entrance or identification signage along Findlay shall not exceed six feet in height. Any off-premises entrance or identification signage along Porter shall not exceed fifteen feet in height. Internal directional and identification signage shall be allowed within the Porter Campus in order to ensure adequate wayfinding, provided that no single internal directional or identification sign may exceed five feet in height.

**LIGHTING:** All exterior lighting for the development will comply with Section 431.6, Commercial Outdoor Lighting Standards.

FENCING: The PUD Narrative states a masonry wall currently exists along the southern boundary of the property which will remain.

**OTHER AGENCY COMMENTS:**

PARK BOARD: The applicant is on the agenda for a December 9, 2021 Park Board meeting. Staff will update on the floor at the Planning Commission Meeting.

PUBLIC WORKS: Findlay Avenue is existing; the proposed interior street will be constructed to city standards. Porter Avenue is part of a city streetscape project. Utilities, sanitary sewer and water, including fire hydrants, will be installed per city and DEQ standards. Sidewalks will be installed adjacent to all public streets. Storm water will be conveyed to existing and proposed detention facilities. This proposal contains less impervious cover than the existing site.

PREDEVELOPMENT: PD21- November 17, 2021

Neighbor's Comments/Concerns/Responses

What is the overall timeline?

Senior Center first, 12 – 13-month build

What is the plan for the hospital growth?

Rebuilding – future growth, there are no plans to vacate the site, we will be keeping the anchor educational facility and building around it.

Will there be a Phasing Plan/timeline?

Senior Center, Behavioral Medicine and simultaneously maybe Variety Care – done by 2023 – 2024.

When the services that are going to move to the West Campus are complete, the demolition of specific areas will begin, we are not demoing the educational facility. Once demo is complete, we will start the rebuild of the campus. The “S” curve street will not be constructed until approximately 75% of the campus site is complete.

The plan is to mimic the Porter Corridor design concept for the reconstruction of the campus.

GREENBELT COMMISSION: GBC21-33, November 15, 2021

Greenbelt forwards this item with no additional comments.

**CONCLUSION:** Staff forwards this request and Ordinance O-2122-29 for City Council's consideration.

Planning Commission, at their December 9, 2021 meeting, unanimously recommended adoption of Ordinance O-2122-29 by a vote of 7-0.

O-2122-29

AN ORDINANCE OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, AMENDING SECTION 460 OF CHAPTER 22 OF THE CODE OF THE CITY OF NORMAN SO AS TO REMOVE PART OF THE NORTHWEST QUARTER (NW/4) OF SECTION TWENTY-NINE (29), TOWNSHIP NINE (9) NORTH, RANGE TWO (2) WEST OF THE INDIAN MERIDIAN, TO NORMAN, CLEVELAND COUNTY, OKLAHOMA, FROM THE R-1, SINGLE FAMILY DWELLING DISTRICT, THE C-3, INTENSIVE SOMMERCIAL DISTRICT, AND THE O-1, OFFICE-INSTITUTIONAL DISTRICT, AND PLACE SAME IN THE PUD, PLANNED UNIT DEVELOPMENT DISTRICT; AND PROVIDING FOR THE SEVERABILITY THEREOF. (Norman Regional Health System Porter Campus)

- § 1. WHEREAS, Norman Regional Health System and the City of Norman, the owners of the hereinafter described property, have made application to have the subject property removed from the R-1, Single Family Dwelling District, the C-3, Intensive Commercial District, and the O-1, Office-Institutional District and placed in the PUD, Planned Unit Development District; and
- § 2. WHEREAS, said application has been referred to the Planning Commission of said City and said body has, after conducting a public hearing as required by law, considered the same and recommended that the same should be granted and an ordinance adopted to effect and accomplish such rezoning; and
- § 3. WHEREAS, the City Council of the City of Norman, Oklahoma, has thereafter considered said application and has determined that said application should be granted and an ordinance adopted to effect and accomplish such rezoning.

NOW, THEREFORE, BE IT ORDAINED BY THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA:

- § 4. That Section 460 of Chapter 22 of the Code of the City of Norman, Oklahoma, is hereby amended so as to remove the following described property from the R-1, Single Family Dwelling District, the C-3, Intensive Commercial District, and the O-1, Office-Institutional District and place the same in the PUD, Planned Unit Development District, to wit:

A tract of land being all of Blocks 1 and 2, WHITWELL ADDITION recorded in Book 1 of Plats, Page 26; AND all of Blocks 1 and 2, GRIFFIN HEIGHTS ADDITION recorded in Book 2 of Plats, Page 62; AND all of Block 1, NORTHEAST ADDITION recorded in Book 1 of Plats, Page 92; AND all of



Blocks 1 and 2, Lots 1 through 13, Block 3 and Lots 1 through 12, Block 4, HIGH SCHOOL ADDITION recorded in Book 1 of Plats, Page 32, together with the platted streets and alleys lying within the aforescribed plats AND those unplatted parts of the Northwest Quarter (NW/4) of Section 29, Township 9 North, Range 2 West of the Indian Meridian, Norman, Cleveland County, Oklahoma all lying within the following described tract of land;

COMMENCING at the northwest corner of said Northwest Quarter; THENCE South 00°09'01" East, along the west line of said Northwest Quarter, a distance of 553.00 feet to the POINT OF BEGINNING; THENCE North 89°50'59" East a distance of 420.40 feet; THENCE North 00°09'01" West a distance of 237.84 feet to a point on the south line of Lot1, Block 1, DOCTOR'S PARK NO. 1 recorded in Book 8 of Plats, Page 77 extended; THENCE North 89°44'54" East, along the south line of said Lot 1 extended, the south line of said Lot 1 and the south line of Lot 1, Block 1, DOCTOR'S PARK NO. 2 recorded in Book 10 of Plats, Page 39, a distance of 615.57 feet to the southeast corner of said Lot 1, also being a point on the west right of way line of Findlay Avenue; THENCE South 00°09'01" East, along said west right of way line and the east line of Block 1 of said GRIFFIN HEIGHTS ADDITION extended and the east line of said Block 1, a distance of 859.73 feet; THENCE South 89°26'27" West a distance of 8.00 feet to a point of intersection with the east line of Block 1, NORTHEAST ADDITION extended; THENCE South 00°09'01" East, along the east line of said Block 1 extended, the east line of said Block 1 and said west right of way line, a distance of 335.00 feet; THENCE North 89°26'27" East a distance of 8.00 feet; THENCE South 00°09'01" East a distance of 165.00 feet to a point of intersection with the south line of Block 4, HIGH SCHOOL ADDITION extended; THENCE South 89°26'27" West, along said south line extended, the south line of said Block 4 and the south line of Block 3 in said Addition, a distance of 1,003.00 feet to the southwest corner of said Block 3, also being a point on the east right of way line of Porter Avenue; THENCE North 00°09'01" West, along the west line of said Block 3, the west line of Block 2 of said HIGH SCHOOL ADDITION, the west line of Block 1, WHITWELL ADDITION and said east right of way line, a distance of 952.35 feet to the northwest corner of said Block 1, WHITWELL ADDITION; THENCE South 89°26'27" West a distance of 33.00 feet to a point on the west line of said Northwest Quarter; THENCE North 00°09'01" West, along said west line, a distance of 175.84 feet to the POINT OF BEGINNING.

Said described tract of land contains an area of 1,277,585 square feet or 29.3293 acres, more or less.

§ 5. Further, pursuant to the provisions of Section 22:420 of the Code of the City of Norman, as amended, the following condition is hereby attached to the zoning of the tract:

- a. The site shall be developed in accordance with the PUD Narrative and Site Development Plan, approved by the Planning Commission on December 9, 2021, and supporting documentation submitted by the applicant and approved by the Planning Commission, and made a part hereof.

§ 6. Severability. If any section, subsection, sentence, clause, phrase, or portion of this ordinance is, for any reason, held invalid or unconstitutional by any court of competent jurisdiction, such portion shall be deemed a separate, distinct, and independent provision, and such holding shall not affect the validity of the remaining portions of this ordinance.

ADOPTED this \_\_\_\_\_ day of \_\_\_\_\_, 2022.

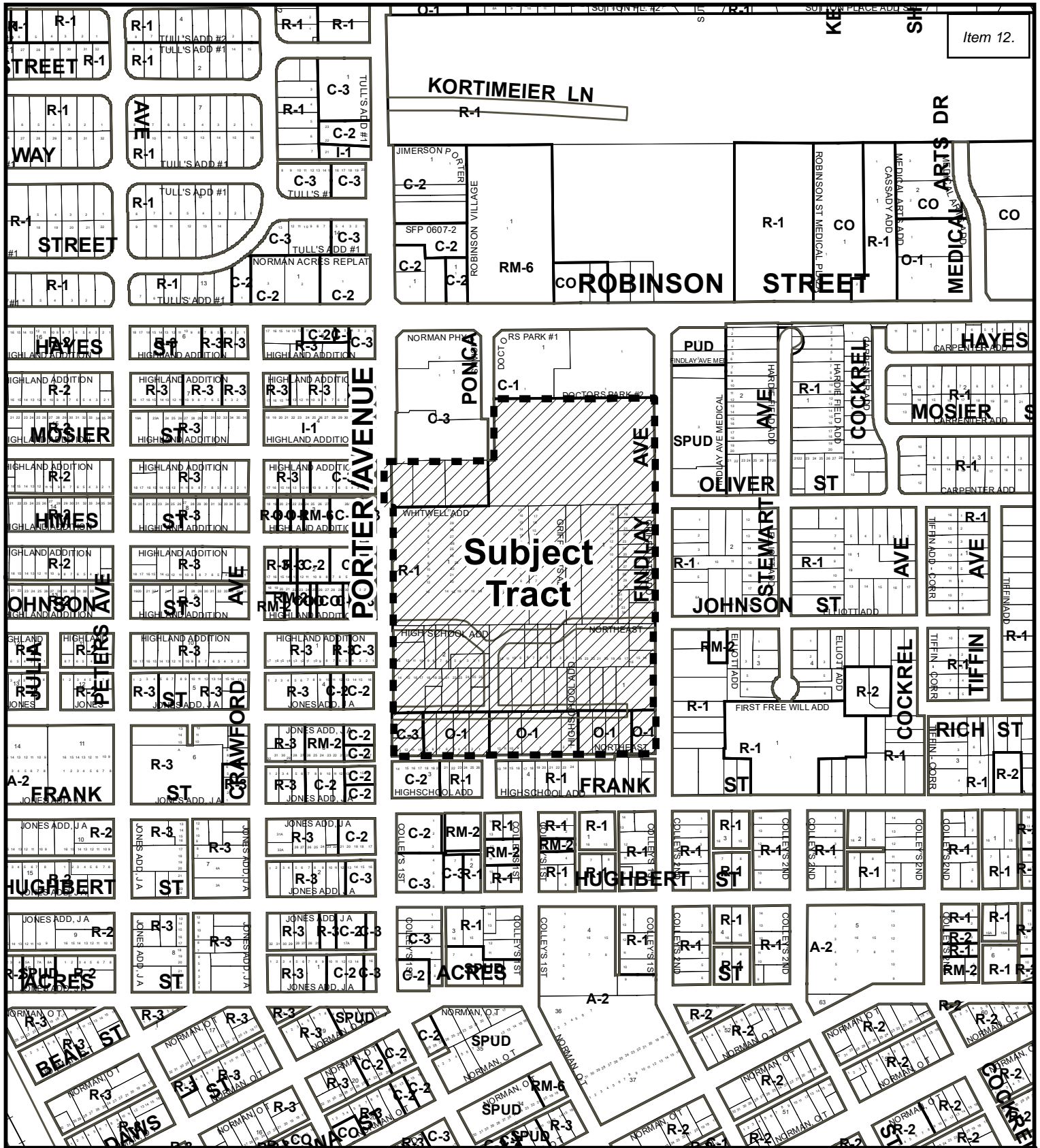
NOT ADOPTED this \_\_\_\_\_ day of \_\_\_\_\_, 2022.

\_\_\_\_\_  
(Mayor)

\_\_\_\_\_  
(Mayor)

ATTEST:

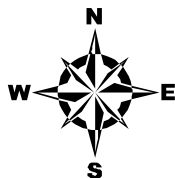
\_\_\_\_\_  
(City Clerk)



# Location Map



Map Produced by the City of Norman  
Geographic Information System.  
The City of Norman assumes no  
responsibility for errors or omissions  
in the information presented.



November 4, 2021

0 250 500 Ft.



Subject Tract



Zoning

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ORDINANCE NO. O-2122-29

ITEM NO. 11

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**STAFF REPORT**

**GENERAL INFORMATION**

APPLICANT	Norman Regional Health System and the City of Norman
REQUESTED ACTION	Rezoning to PUD, Planned Unit Development District
EXISTING ZONING	R-1, Single-Family Dwelling District C-3, Intensive Commercial District O-1, Office-Institutional District
SURROUNDING ZONING	North: C-1, Local Commercial District and C-3, Intensive Commercial District East: SPUD, Simple Planned Unit Development O-1920-33, and R-1, Single-Family Dwelling District South: C-2, General Commercial District, and R-1, Single-Family Dwelling District, West: C-2, General Commercial District, and C-3, Intensive Commercial District
LOCATION	South of E. Robinson Street, west of N. Findlay Avenue, north of E. Frank Street, and east of N. Porter Avenue
SIZE	29.3 acres, more or less
PURPOSE	Mixed use development
EXISTING LAND USE	Norman Regional Hospital and medical offices
SURROUNDING LAND USE	North: Pharmacy and medical offices East: Dimensions School and single-family residential

South: Sonic and single  
West: residential  
Commercial

Item 12.

**SYNOPSIS:** The applicants, Norman Regional Health System and the City of Norman, are requesting to rezone the subject property with approximately 29.3 acres to PUD, Planned Unit Development. The subject property is currently the site of the Porter Campus of Norman Regional Hospital. The applicant is requesting to redevelop as a mixed use site, in addition to the site planned for the Senior Wellness Center.

**HISTORY:** On July 13, 1954, City Council adopted Ordinance No. 884, which placed the R-1, Single-Family Dwelling District, and C-3, Intensive Commercial District, portions of the subject property into these zoning districts. The portions of the subject property that are currently zoned O-1, Office-Institutional District, were rezoned to O-1 with Ordinance Nos. O-9900-9 (adopted October 12, 1999), O-9900-52 (adopted June 13, 2000), and O-0203-18 (adopted December 17, 2002). Over the years, Norman Regional Health System has developed the subject property into the Porter Campus for the hospital. Numerous doctors' offices have also been developed.

**ZONING ORDINANCE CITATION:** SEC. 420 – PLANNED UNIT DEVELOPMENT

Statement of Purpose. It is the intent of this section to encourage developments with a superior built environment brought about through unified development and to provide for the application of design ingenuity in such developments while protecting existing and future surrounding areas in achieving the goals of the comprehensive plan of record. The "PUD" Planned Unit Development district herein established is intended to provide for greater flexibility in the design of buildings, yards, courts, circulation, and open space than would otherwise be possible through the strict application of other district regulations. In this way, applicants may be awarded certain premiums in return for assurances of overall planning and design quality, or which will be of exceptional community benefit and which are not now required by other regulations. By permitting and encouraging the use of such procedures, the Planning Commission and City Council will be able to make more informed land use decisions and thereby guide development more effectively in the best interest of the health, safety, and welfare of the City.

Specifically, the purposes of this section are to encourage:

- (a) A maximum choice in the types of environment and living units available to the public.
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**ANALYSIS:** The particulars of this PUD include:

**USE:** The applicant listed what uses will be allowed in the PUD Narrative, Exhibit E, "Allowable Uses."

**OPEN SPACE:** The PUD Narrative states the development will have a minimum of 10% space open. The proposed open space areas are shown on the Open Space Exhibit D.

**PARKING:** Parking for the development will meet or exceed the requirements of Section 431.5, Off-Street Parking Requirements. Parking lots abutting residential properties will be designed in accordance with the Porter Corridor Zoning Overlay District regulations, except that no buffer walls shall be required. Buffer landscaping and parking lot lighting regulations will still apply.

**PHASES:** The Porter Campus will be developed in multiple phases. The initial phase is anticipated to include the City's Senior Wellness Center and behavioral health care.

**SITE PLAN/ACCESS:** The proposed site development plan shows six access points off Findlay Avenue and two access points off Porter Avenue. A new interior street will be constructed. The existing EMSTAT building on the north of the subject property will remain. Two mixed use buildings and an office building are proposed on the east side of the property. NRHS will have three buildings on the northwest side of the property off Porter Avenue; the NRHS North building and the NRHS Education building will remain. A new NRHS building is proposed between the two existing buildings. The City's Senior Wellness Center is proposed for the southeast corner of the property. A variety care site and a BMS site are proposed for the southwest corner of the property. There are parking lots and walking trails proposed throughout the development.

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**LANDSCAPING:** Landscaping for the development will comply with Section 431.8, Landscaping Requirements for Off-Street Parking Facilities.

**SIGNAGE:** The PUD Narrative states all signs will comply with the Commercial Zoning Standards in Section 18-504 with the following exceptions. Grounds signs on the Porter Campus shall be allowed at zero setback so long as all ground signs do not block the applicable sight triangle. Development entrance signage and tenant identification signage shall be allowed at the entrances of the Porter Campus. Any off-premises entrance or identification signage along Findlay shall not exceed six feet in height. Any off-premises entrance or identification signage along Porter shall not exceed fifteen feet in height. Internal directional and identification signage shall be allowed within the Porter Campus in order to ensure adequate wayfinding, provided that no single internal directional or identification sign may exceed five feet in height.

**LIGHTING:** All exterior lighting for the development will comply with Section 431.6, Commercial Outdoor Lighting Standards.

**FENCING:** The PUD Narrative states a masonry wall currently exists along the southern boundary of the property. The applicant requests not to comply with the Porter Corridor Zoning Overlay District's requirement for a buffer wall along the Commercial Development Line, which is Findlay Avenue in this case.

### **OTHER AGENCY COMMENTS:**

**PARK BOARD:** The applicant is on the agenda for a December 9, 2021 Park Board meeting. Staff will update on the floor at the Planning Commission Meeting.

**PUBLIC WORKS:** Findlay Avenue is existing; the proposed interior street will be constructed to city standards. Porter Avenue is part of a city streetscape project. Utilities, sanitary sewer and water, including fire hydrants, will be installed per city and DEQ standards. Sidewalks will be installed adjacent to all public streets. Storm water will be conveyed to existing and proposed detention facilities. This proposal contains less impervious cover than the existing site.

**PREDEVELOPMENT:** PD21- November 17, 2021

Neighbor's Comments/Concerns/Responses

What is the overall timeline?

Senior Center first, 12 – 13-month build

What is the plan for the hospital growth?

Rebuilding – future growth, there are no plans to vacate the site, we will be keeping the anchor educational facility and building around it.

Will there be a Phasing Plan/timeline?

Senior Center, Behavioral Medicine and simultaneously maybe Variety Care – done by 2023 – 2024.

When the services that are going to move to the West Campus are complete, the demolition of specific areas will begin, we are not demoing the educational facility. Once demo is complete, we will start the rebuild of the campus. The "S" curve street will not be constructed until approximately 75% of the campus site is complete.

The plan is to mimic the Porter Corridor design concept for the reconstruction of the campus.

**GREENBELT COMMISSION:** GBC21-33, November 15, 2021

Greenbelt forwards this item with no additional comments.



**CONCLUSION:** Staff forwards this request and Ordinance No. O-2122-29 for Planning Commission's consideration.

Item 12.

**Applicant:** Norman Regional Hospital/City of Norman

**Project Location:** NRH Porter Campus Site

**Case Number:** PD21-39

**Time:** 6:30 p.m.

**Applicant/Representative**

Shawn Rieger  
Troy Glover  
John Manfred  
Richie Splitt  
Paula Price

**Attendees**

Danny Millsap  
Randall Foster  
Jerry Cos  
Judy Hatfield  
Alex Snoddy  
Mark Cox

**City Staff**

Jane Hudson, Planning Director  
Kathryn Walker, City Attorney  
Jason Olsen, Parks Director

**Application Summary**

The applicant is requesting to rezone the NRH Porter Campus Site to a PUD, Planned Unit Development to allow for the development of a Mixed Use Development.

**Neighbor's Comments/Concerns/Responses**

What is the overall timeline?

Senior Center first, 12 – 13-month build

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**PORTER CAMPUS  
A PLANNED UNIT DEVELOPMENT**

**NORMAN, OKLAHOMA**

APPLICANTS:



***NORMAN REGIONAL HOSPITAL AUTHORITY,  
AN OKLAHOMA PUBLIC TRUST D/B/A NORMAN REGIONAL HEALTH SYSTEM***

**&**



***THE CITY OF NORMAN, OKLAHOMA,  
AN OKLAHOMA MUNICIPAL CORPORATION***

APPLICATION FOR:

PLANNED UNIT DEVELOPMENT  
2025 AMENDMENT  
PRELIMINARY PLAT

Submitted November 1, 2021  
Revised December 3, 2021

PREPARED BY:

RIEGER LAW GROUP PLLC  
136 Thompson Drive  
Norman, Oklahoma 73069

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  - E. Utility Services
  - F. Fire Protection Services
  - G. Traffic Circulation and Access
- III. DEVELOPMENT PLAN AND DESIGN CONCEPT
  - A. Uses Permitted
  - B. Area Regulations
  - C. Parking
  - D. Miscellaneous Development Criteria

## EXHIBITS

- A. Legal Description of the Porter Campus
- B. Preliminary Plat
- C. Preliminary Site Development Plan
- D. Allowable Uses

## I. **INTRODUCTION**

Norman Regional Hospital Authority, an Oklahoma Public Trust d/b/a Norman Regional Health System (“NRHS”), and the City of Norman, Oklahoma, an Oklahoma municipal corporation (“City”), seek to preliminarily plat and rezone multiple tracts of property making up approximately 29.3 acres located in Ward 4 of the City of Norman. The subject property is more particularly described on the attached **Exhibit A** and shall be referred to herein as the “Porter Campus”. NRHS and the City may be collectively referred to herein as the “Applicants”.

The Applicants intend to put forth the parameters for which the development of the Porter Campus may be phased over time. Through the use of this Planned Unit Development (“PUD”) and the Preliminary Plat, attached as **Exhibit B**, the Porter Campus may be developed in substantial conformance with the Preliminary Site Development Plan, attached as **Exhibit C**, featuring the City’s Senior Wellness Center, other medical uses, such as, but not limited to, variety care, behavioral health care, administrative and educational buildings, as well as numerous other compatible uses as further enumerated in this PUD. It is anticipated that the Porter Campus will be developed in multiple phases, with the Senior Wellness Center expected to be part of the initial development phase.

## II. **PROPERTY DESCRIPTION/GENERAL SITE CONDITIONS**

### A. **Location**

The Porter Campus is generally located South of E. Robinson Street, West of N. Findlay Ave, North of E. Frank Street, and East of N. Porter Ave.

### B. **Existing Land Use and Zoning**

The Porter Campus currently contains multiple zoning classification, including C-3, Intensive Commercial, R-1, Single Family Dwelling, and O-1, Office Institutional. It is completely contained within the Porter Corridor Zoning Overlay District, the purpose of which is to provide regulations that create a buffer between commercial and residential areas. The existing NORMAN 2025 designation is Institutional.

Across N. Porter Ave., West of the Porter Campus are properties generally zoned C-3, Intensive Commercial, with NORMAN 2025 designations of Office and Commercial. The properties immediately South of the Porter Campus are zoned R-1, Single Family Dwelling, with one parcel zoned C-2, General Commercial, at the NE corner of N. Porter Ave. and E. Frank Street. Those same properties have corresponding NORMAN 2025 designations of Low Density Residential and Commercial, respectively. The properties to the East of the Porter Campus are generally zoned R-1, Single Family Dwelling, with NORMAN 2025 designations of Low Density Residential. The properties located at the SE corner of N. Findlay

Ave. and E. Robinson Street are zoned PUD and SPUD. The property zoned SPUD has a NORMAN 2025 designation of Institutional and the property zoned PUD has a NORMAN 2025 designation of Office. The property located at the SW corner of N. Findlay Ave. and E. Robinson Street is zoned C-1, Local Commercial, and has a NORMAN 2025 designation of Office. The properties located at the SE corner of N. Porter Ave. and E. Robinson Street are zoned C-3, Intensive Commercial. The Northernmost parcel at said intersection has a NORMAN 2025 designation of Commercial the abutting parcel to the South has a NORMAN 2025 designation of Office.

### **C. Elevation and Topography**

The Porter Campus is currently developed with Norman Regional Hospital and other improvements. The Porter Campus generally slopes from the North to the South.

### **D. Drainage**

A drainage report has been provided by the Applicants to City Staff as part of the Preliminary Plat application. The Applicants shall comply with all applicable City ordinances and regulations regarding drainage for the Porter Campus.

### **E. Utility Services**

The necessary utility services for this project are already located in close proximity to the Porter Campus. The Applicants shall extend such utility services to the Porter Campus, as necessary, to facilitate their intended development.

### **F. Fire Protection Services**

Fire Protection services will be provided by the City of Norman Fire Department and by the Applicants as such are required by adopted City codes.

### **G. Traffic Circulation and Access**

Access to the Porter Campus shall be permitted in the manner depicted on the attached Preliminary Site Development Plan.

## **III. DEVELOPMENT PLAN AND DESIGN CONCEPT**

The Porter Campus is planned to accommodate a mixed-use development over the approximately 29.3 acres of land, including, but not limited to, the City's Senior Wellness Center, other medical uses, such as, but not limited to, variety care, behavioral health care, administrative and educational buildings, as well as numerous other compatible uses as further enumerated in this PUD. The Porter Campus shall be developed in substantial compliance with the Preliminary Site Development Plan,

attached as **Exhibit C**. The Exhibits attached hereto, and as submitted on behalf on the Applicants, are incorporated herein by reference and further depict the development criteria for the Porter Campus.

#### **A. Uses Permitted:**

The Porter Campus will feature the City's Senior Wellness Center, as well as other medical uses, such as, but not limited to, Variety Care, behavioral health care, administrative and educational buildings, and other compatible uses. Lot 1, Block 1 of the Porter Campus (the "**Senior Wellness Tract**") shall be developed as the City's Senior Wellness Center. A complete list of the allowable uses for the Porter Campus is attached hereto as **Exhibit D**.

#### **B. Area Regulations:**

The lots within the preliminary plat of the Porter Campus shall be allowed to be developed with minimal setbacks in order to create a vibrant, walkable, and high-quality development. Therefore, there shall be no minimum setbacks for each individual lot, except that no building shall be constructed within (i) twenty (20') feet from the South boundary of the Porter Campus or (ii) twenty (20') feet from the Findlay Avenue right-of-way. No buildings shall be permitted to encroach upon any public easement or right-of-way. The maximum height for buildings located within one hundred (100) feet from where the Porter Campus abuts residentially zoned property shall be three (3) stories. Otherwise, there shall be no maximum height within the Porter Campus.

#### **C. Parking:**

Parking will meet or exceed the requirements of Section 431.5 of the City of Norman's Zoning Ordinance for Off-Street Parking, as amended from time to time.

#### **D. Dumpster and Trash Enclosures**

Trash may be handled through on-site dumpsters. A trash compactor(s) and its enclosure(s) may also be located on site to facilitate trash removal. Any dumpster or trash facilities shall be screened within enclosures that are built of materials to be compatible with the building exteriors of the main building on said lot and in compliance with the City's standards for Solid Waste Container Enclosures.

#### **E. Miscellaneous Development Criteria**

##### **1. Site Plan**

The Preliminary Site Development plan for the Porter Campus is concurrently submitted with this PUD and shall be incorporated herein as an integral part of the PUD and the development of the Porter Campus shall



be generally constructed as presented thereon, subject to final design development and the changes allowed by Section 22.420(7) of the City of Norman's PUD Ordinance as amended from time to time.

## **2. Open Space**

Open space and green space areas are located throughout the Porter Campus. The Porter Campus shall contain a minimum of 10% of open space throughout the development. Impervious area for the Porter Campus shall not exceed 90% as applied to the entirety of the development.

## **3. Signage**

Each lot within the Porter Campus shall comply with the City of Norman's applicable Commercial Zone Sign Standards, contained in Section 18-504 of the City of Norman's Municipal Code, as amended from time to time. Grounds signs on the Porter Campus shall be allowed at zero setback so long as all ground signs do not block the applicable sight triangle. Development entrance signage and tenant identification signage shall be allowed at the entrances of the Porter Campus. Any off-premises entrance or identification signage along Findlay shall not exceed six (6) feet in height. Any off-premises entrance or identification signage along Porter shall not exceed fifteen (15) feet in height. Internal directional and identification signage shall be allowed within the Porter Campus in order to ensure adequate wayfinding, provided that no single internal directional or identification sign may exceed five (5) feet in height.

## **4. Traffic access/circulation/parking and sidewalks**

Access to the Porter Campus shall be permitted in the manner depicted on the attached Preliminary Site Development Plan. The Porter Campus shall comply with the City of Norman standards to allow for emergency access and fire access as necessary, as such standards may be amended from time to time. Sidewalks will be provided in the locations shown on the attached Preliminary Site Development Plan and Preliminary Plat.

## **5. Lighting**

All exterior lighting shall be installed in conformance with the City of Norman's Commercial Outdoor Lighting Standards, contained in Section 431.6 of the City of Norman's Zoning Ordinance, as amended from time to time.

## **6. Landscaping**

Landscaping shall be provided in conformity to Sections 429.6 and 431.8 of the City of Norman's Zoning Ordinance, as amended from time to time.

**7. Fencing**

The Porter Campus currently features a masonry wall along the Southern boundary, which will remain in its current location..

**8. Phasing**

It is anticipated that the Porter Campus will be developed in multiple phases. The initial phase is anticipated to include the City's Senior Wellness Center and behavioral health care. The timing and number of future phases will be determined by market demand and absorption rates.

**9. Exterior Appearance**

The exterior materials of the building to be constructed within the Porter Campus may be brick, glass, stone, synthetic stone, stucco, EIFS, masonry, metal accents, composition shingles, and any combination and percentage thereof. This section shall govern and supersede the terms of Section 431.4 of the City's Zoning Ordinance within the Porter Campus.

**EXHIBIT A**

## Legal Description of the Porter Campus

A tract of land being all of Blocks 1 and 2, WHITWELL ADDITION recorded in Book 1 of Plats, Page 26; AND all of Blocks 1 and 2, GRIFFIN HEIGHTS ADDITION recorded in Book 2 of Plats, Page 62; AND all of Block 1, NORTHEAST ADDITION recorded in Book 1 of Plats, Page 92; AND all of Blocks 1 and 2, Lots 1 through 13, Block 3 and Lots 1 through 12, Block 4, HIGH SCHOOL ADDITION recorded in Book 1 of Plats, Page 32, together with the platted streets and alleys lying within the aforescribed plats AND those unplatted parts of the Northwest Quarter (NW/4) of Section 29, Township 9 North, Range 2 West of the Indian Meridian, Norman, Cleveland County, Oklahoma all lying within the following described tract of land;

COMMENCING at the northwest corner of said Northwest Quarter;

THENCE South 00°09'01" East, along the west line of said Northwest Quarter, a distance of 553.00 feet to the POINT OF BEGINNING;

THENCE North 89°50'59" East a distance of 420.40 feet;

THENCE North 00°09'01" West a distance of 237.84 feet to a point on the south line of Lot1, Block 1, DOCTOR'S PARK NO. 1 recorded in Book 8 of Plats, Page 77 extended;

THENCE North 89°44'54" East, along the south line of said Lot 1 extended, the south line of said Lot 1 and the south line of Lot 1, Block 1, DOCTOR'S PARK NO. 2 recorded in Book 10 of Plats, Page 39, a distance of 615.57 feet to the southeast corner of said Lot 1, also being a point on the west right of way line of Findlay Avenue;

THENCE South 00°09'01" East, along said west right of way line and the east line of Block 1 of said GRIFFIN HEIGHTS ADDITION extended and the east line of said Block 1, a distance of 859.73 feet;

THENCE South 89°26'27" West a distance of 8.00 feet to a point of intersection with the east line of Block 1, NORTHEAST ADDITION extended;

THENCE South 00°09'01" East, along the east line of said Block 1 extended, the east line of said Block 1 and said west right of way line, a distance of 335.00 feet;

THENCE North 89°26'27" East a distance of 8.00 feet;

THENCE South 00°09'01" East a distance of 165.00 feet to a point of intersection with the south line of Block 4, HIGH SCHOOL ADDITION extended;

THENCE South 89°26'27" West, along said south line extended, the south line of said Block 4 and the south line of Block 3 in said Addition, a distance of 1,003.00 feet to the southwest corner of said Block 3, also being a point on the east right of way line of

Porter Avenue;

THENCE North 00°09'01" West, along the west line of said Block 3, the west line of Block 2 of said HIGH SCHOOL ADDITION, the west line of Block 1, WHITWELL ADDITION and said east right of way line, a distance of 952.35 feet to the northwest corner of said Block 1, WHITWELL ADDITION;

THENCE South 89°26'27" West a distance of 33.00 feet to a point on the west line of said Northwest Quarter;

THENCE North 00°09'01" West, along said west line, a distance of 175.84 feet to the POINT OF BEGINNING.

Said described tract of land contains an area of 1,277,585 square feet or 29.3293 acres, more or less.

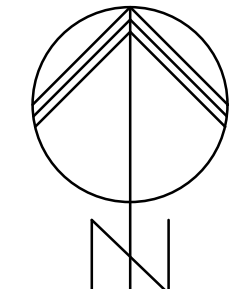
The basis of bearings for this legal description was the Oklahoma State Plane Coordinate System (NAD83-South Zone) using a bearing of South 00°09'01" East on the west line of the Northwest Quarter of Section 29, Township 9 North, Range 2 West of the Indian Meridian.

Prepared by:     Randall A. Mansfield, Professional Land Surveyor No. 1613  
                          Dodson-Thompson-Mansfield PLLC  
                          20 N.E. 38th Street - OKC, OK 73105  
                          October 25, 2021

**EXHIBIT B**  
Preliminary Plat  
*Full Size Documents Submitted to City Staff*

[Attached hereto]





# PRELIMINARY PLAT

## NORMAN REGIONAL HEALTH SYSTEM

### PORTER CAMPUS

#### A PLANNED UNIT DEVELOPMENT

A PART OF THE N.E./4, SEC. 29, T9N, R2W, I.M.  
NORMAN, CLEVELAND COUNTY, OKLAHOMA

PUL  
0340

#### LEGAL DESCRIPTION:

A TRACT OF LAND BEING ALL OF BLOCKS 1 AND 2, WHITWELL ADDITION RECORDED IN BOOK 1 OF PLATS, PAGE 26; AND ALL OF BLOCKS 1 AND 2, GRIFFIN HEIGHTS ADDITION RECORDED IN BOOK 2 OF PLATS, PAGE 62; AND ALL OF BLOCK 1, NORTHEAST ADDITION RECORDED IN BOOK 1 OF PLATS, PAGE 92; AND ALL OF BLOCKS 1 AND 2, LOTS 1 THROUGH 13, BLOCK 3 AND LOTS 1 THROUGH 12, BLOCK 4, HIGH SCHOOL ADDITION RECORDED IN BOOK 1 OF PLATS, PAGE 32; TOGETHER WITH THE PLATTED STREETS AND THOSE UNPLATTED PARTS OF THE NORTHWEST QUARTER (NW/4) OF SECTION 29, TOWNSHIP 9 NORTH, RANGE 2 WEST OF THE INDIAN MERIDIAN, NORMAN, CLEVELAND COUNTY, OKLAHOMA ALL LYING WITHIN THE FOLLOWING DESCRIBED TRACT OF LAND;

COMMENCING AT THE NORTHWEST CORNER OF SAID NORTHWEST QUARTER;

THENCE SOUTH 00°09'01" EAST, ALONG THE WEST LINE OF SAID NORTHWEST QUARTER, A DISTANCE OF 553.00 FEET TO THE POINT OF BEGINNING;

THENCE NORTH 89°50'59" EAST A DISTANCE OF 420.40 FEET;

THENCE NORTH 00°09'01" WEST A DISTANCE OF 237.84 FEET TO A POINT ON THE SOUTH LINE OF LOT 1, BLOCK 1, DOCTOR'S PARK NO. 1 RECORDED IN BOOK 8 OF PLATS, PAGE 77 EXTENDED;

THENCE NORTH 89°44'54" EAST, ALONG THE SOUTH LINE OF SAID LOT 1 EXTENDED, THE SOUTH LINE OF SAID LOT 1 AND THE SOUTH LINE OF LOT 1, BLOCK 1, DOCTOR'S PARK NO. 2 RECORDED IN BOOK 10 OF PLATS, PAGE 39, A DISTANCE OF 615.57 FEET TO THE SOUTHEAST CORNER OF SAID LOT 1, ALSO BEING A POINT ON THE WEST RIGHT OF WAY LINE OF FINDLAY AVENUE;

THENCE SOUTH 00°09'01" EAST, ALONG SAID WEST RIGHT OF WAY LINE AND THE EAST LINE OF BLOCK 1 OF SAID GRIFFIN HEIGHTS ADDITION EXTENDED AND THE EAST LINE OF SAID BLOCK 1, A DISTANCE OF 859.73 FEET;

THENCE SOUTH 89°26'27" WEST A DISTANCE OF 8.00 FEET TO A POINT OF INTERSECTION WITH THE EAST LINE OF BLOCK 1, NORTHEAST ADDITION EXTENDED;

THENCE SOUTH 00°09'01" EAST, ALONG THE EAST LINE OF SAID BLOCK 1 EXTENDED, THE EAST LINE OF SAID BLOCK 1 AND SAID WEST RIGHT OF WAY LINE, A DISTANCE OF 335.00 FEET;

THENCE NORTH 89°26'27" EAST A DISTANCE OF 8.00 FEET;

THENCE SOUTH 00°09'01" EAST A DISTANCE OF 165.00 FEET TO A POINT OF INTERSECTION WITH THE SOUTH LINE OF BLOCK 4, HIGH SCHOOL ADDITION EXTENDED;

THENCE SOUTH 89°26'27" WEST, ALONG SAID SOUTH LINE EXTENDED, THE SOUTH LINE OF SAID BLOCK 4 AND THE SOUTH LINE OF BLOCK 3 IN SAID ADDITION, A DISTANCE OF 1,033.00 FEET TO THE SOUTHWEST CORNER OF SAID BLOCK 3, ALSO BEING A POINT ON THE EAST RIGHT OF WAY LINE OF PORTER AVENUE;

THENCE NORTH 00°09'01" WEST, ALONG THE WEST LINE OF SAID BLOCK 3, THE WEST LINE OF BLOCK 2 OF SAID HIGH SCHOOL ADDITION, THE WEST LINE OF BLOCK 1, WHITWELL ADDITION AND SAID EAST RIGHT OF WAY LINE, A DISTANCE OF 952.35 FEET TO THE NORTHWEST CORNER OF SAID BLOCK 1, WHITWELL ADDITION;

THENCE SOUTH 89°26'27" WEST A DISTANCE OF 33.00 FEET TO A POINT ON THE WEST LINE OF SAID NORTHWEST QUARTER;

THENCE NORTH 00°09'01" WEST, ALONG SAID WEST LINE, A DISTANCE OF 175.84 FEET TO THE POINT OF BEGINNING.

SAID DESCRIBED TRACT OF LAND CONTAINS AN AREA OF 1,277,585 SQUARE FEET OR 29.3293 ACRES, MORE OR LESS.

THE BASIS OF BEARINGS FOR THIS LEGAL DESCRIPTION WAS THE OKLAHOMA STATE PLANE COORDINATE SYSTEM (NAD83-SOUTH ZONE) USING A BEARING OF SOUTH 00°09'01" EAST ON THE WEST LINE OF THE NORTHWEST QUARTER OF SECTION 29, TOWNSHIP 9 NORTH, RANGE 2 WEST OF THE INDIAN MERIDIAN.

#### NOTES:

- FIRE HYDRANTS WILL BE LOCATED AND INSTALLED IN ACCORDANCE WITH THE FINAL PLANS AND THE CITY OF NORMAN STANDARDS AND SPECIFICATIONS.
- ALL SIDEWALKS WILL BE CONSTRUCTED IN ACCORDANCE WITH THE FINAL PLANS AND THE CITY OF NORMAN STANDARDS AND SPECIFICATIONS.
- REFER TO DRAINAGE REPORT FOR SPECIFIC DATA.
- ALL PROPOSED SANITARY LINES ARE 8-INCH EXCEPT AS NOTED.
- ALL PROPOSED WATERLINES ARE 8-INCH EXCEPT AS NOTED.
- ALL ISLANDS AND/OR MEDIAN WITHIN RIGHTS-OF-WAY AND ALL COMMON AREAS WITHIN THIS DEVELOPMENT WILL BE MAINTAINED BY THE PROPERTY OWNERS' ASSOCIATION.
- EXISTING ZONING IS MIXED WITH INTENSIVE COMMERCIAL, SINGLE FAMILY DWELLING, AND OFFICE INSTITUTIONAL; WITH A PROPOSED CHANGE TO PUD.
- THE EXISTING ENTRYWAY SIGNAGE AT THE NORTHEAST CORNER OF THE PORTER AVENUE AND TBA STREET INTERSECTION SHALL BE ALLOWED TO REMAIN IN ITS CURRENT LOCATION AS INDICATED ON THIS PLAN.
- PAVING SHALL BE ALLOWED OVER DRAINAGE AND UTILITY EASEMENTS.

#### STORM DRAINAGE DETENTION FACILITY EASEMENT

DRAINAGE DETENTION FACILITY EASEMENTS ARE HEREBY ESTABLISHED AS SHOWN TO PROVIDE FOR DETENTION OF STORM SURFACE WATER AND CONSTRUCTED AS APPROVED BY THE CITY ENGINEER. ALL MAINTENANCE WITHIN THE DRAINAGE DETENTION FACILITY EASEMENT SHALL BE THE RIGHT, DUTY AND RESPONSIBILITY OF THE PROPERTY OWNERS ASSOCIATION IN THE PLAT OF NORMAN REGIONAL HEALTH SYSTEM PORTER CAMPUS ADDITION. HOWEVER, IF MAINTENANCE IS NEGLECTED OR SUBJECT TO OTHER UNUSUAL CIRCUMSTANCES AND IS DETERMINED TO BE A HAZARD OR THREAT TO PUBLIC SAFETY BY THE CITY ENGINEER, CORRECTIVE MAINTENANCE MAY BE PERFORMED BY THE GOVERNING JURISDICTION WITH COSTS ASSESSED TO AND BORN UPON SAID PROPERTY OWNER(S). OFFICIALS REPRESENTING THE PUBLIC WORKS DEPARTMENT, SHALL HAVE THE RIGHT TO ENTER UPON THE EASEMENT FOR PURPOSES OF PERIODIC INSPECTION AND/OR CORRECTIVE MAINTENANCE OF THE FACILITY. UPON RECEIVING WRITTEN APPROVAL FROM THE PUBLIC WORKS DEPARTMENT, PROPERTY OWNER(S) MAY CONSTRUCT IMPROVEMENTS WITHIN THE EASEMENT, PROVIDED THE IMPROVEMENT DOES NOT INTERFERE WITH THE FUNCTION OF THE DETENTION FACILITY.

LOT SUMMARY TABLE				
LOT	BLOCK	AREA (SQ. FT.)	AREA (ACRES)	
1	1	174,621	4.01	
2	1	180,257	3.68	
3	1	109,988	2.53	
4	1	94,047	2.16	
5	1	84,585	1.94	
6	1	75,303	1.73	
1	2	283,862	6.52	
2	2	79,436	1.82	
3	2	113,800	2.61	
ROW	PORTER AVE.	10,972	0.25	
ROW	TBA STREET	90,722	2.08	
TOTAL		1,277,585	29.33	

TBA STREET CURVE TABLE						
CURVE #	RADIUS	LENGTH	DELTA	TANGENT	CHORD BEARING	CHORD LENGTH
C1	582.00'	152.12'	14°58'31"	76.49'	S 83°14'38" E	151.68'
C2	215.00'	392.20'	104°31'05"	277.77'	N 51°59'05" E	340.04'
C3	272.00'	425.90'	89°42'54"	270.65'	N 44°35'00" E	383.71'

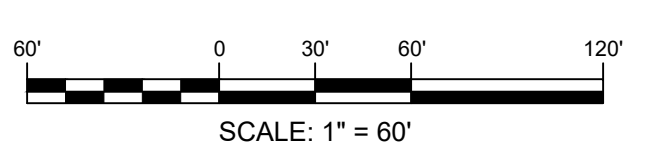
OWNER / DEVELOPER

NORMAN REGIONAL HOSPITAL AUTHORITY  
an Oklahoma public trust d/b/a  
NORMAN REGIONAL HEALTH SYSTEM ("NRHS")  
NORMAN, OKLAHOMA

PREPARED BY

SMC CONSULTING ENGINEERS, P.C.  
815 W. MAIN ST.  
OKLAHOMA CITY, OKLAHOMA 73106  
(405) 232-7715

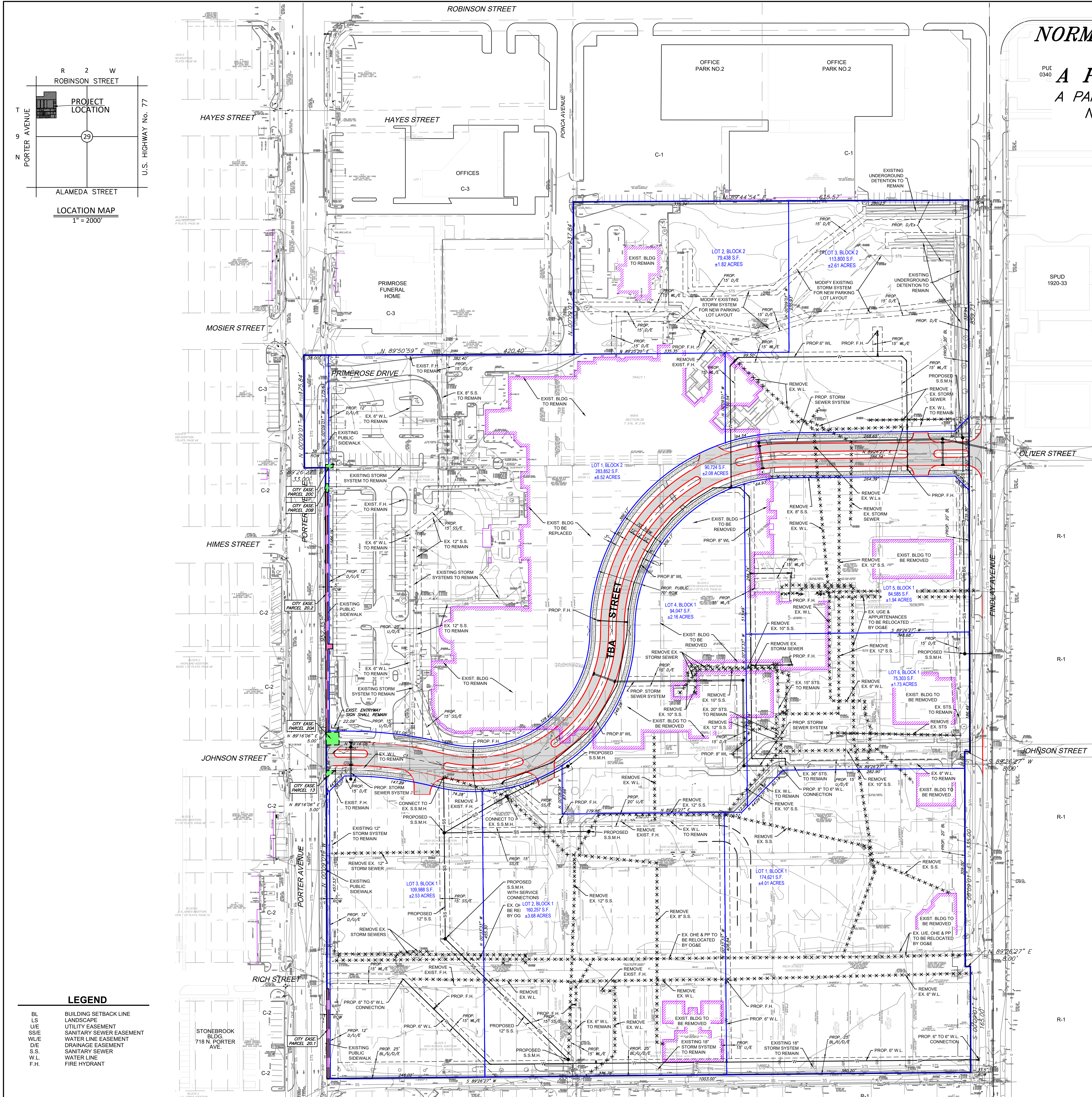
THE CITY OF NORMAN, OKLAHOMA  
a municipal corporation



NORMAN REGIONAL HEALTH SYSTEM -  
PORTER CAMPUS  
PORTER AVENUE & ROBINSON STREET  
NORMAN, OKLAHOMA

SMC Consulting Engineers, P.C.  
1405-232-7715 Fax: 405-232-7889  
Website: www.smc-engineers.com  
PROJECT NO.: 6437.00  
DATE: DECEMBER 3, 2021  
SCALE: 1" = 60'  
DRAWN BY: PWC  
ENGINEER: TERENCE L. HAYNES  
P.E. NUMBER: 16820

PRELIMINARY PLAT  
SHEET NO.  
PP1.00



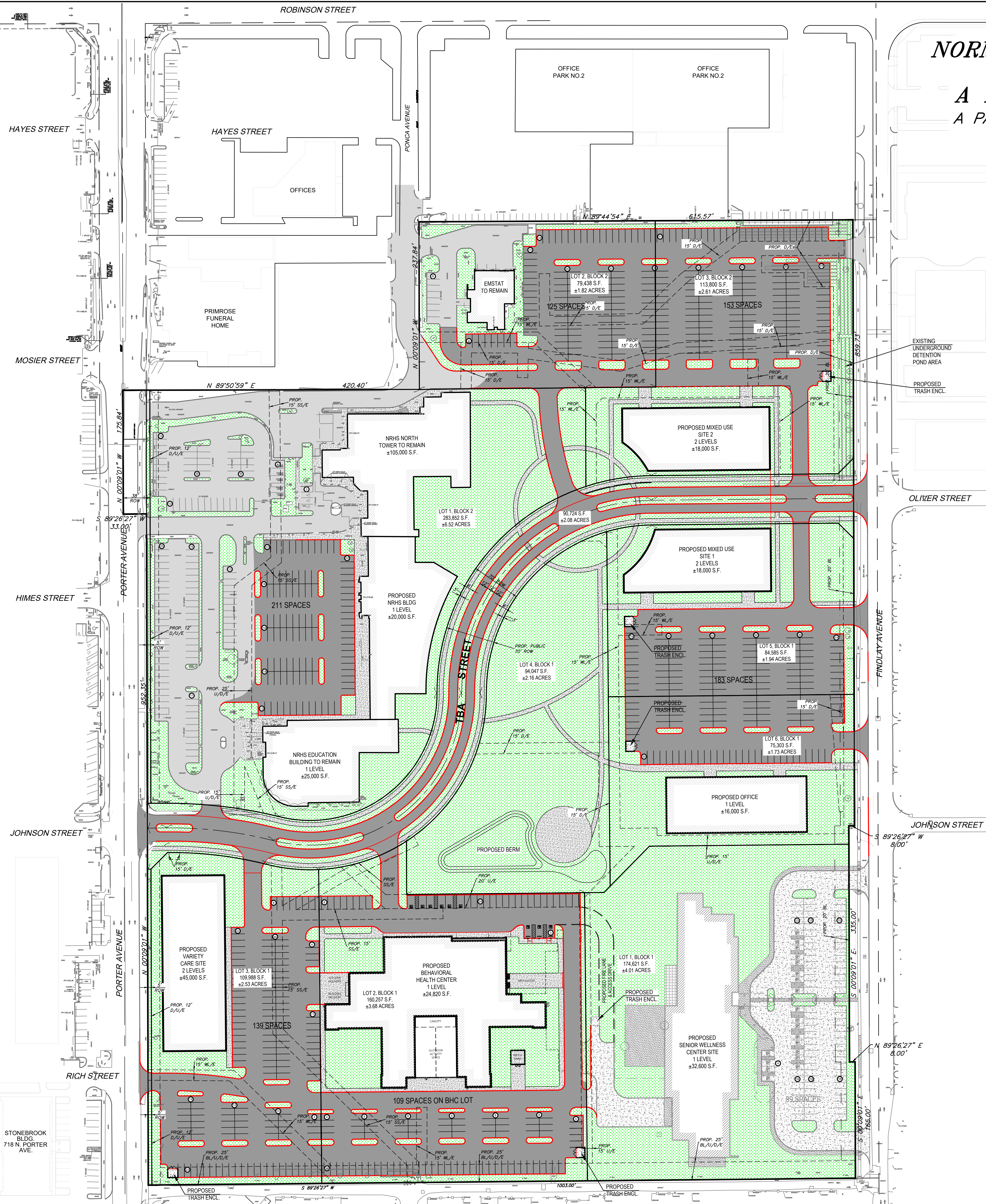


**EXHIBIT C**

Preliminary Site Development Plan  
*Full Size Documents Submitted to City Staff*

[Attached hereto]





LOT SUMMARY TABLE			
LOT	BLOCK	AREA (SQ. FT.)	AREA (ACRES)
1	1	174,621	4.01
2	1	160,257	3.68
3	1	109,908	2.53
4	1	94,047	2.16
5	1	84,585	1.94
6	1	75,303	1.73
	2	283,862	6.52
2	2	79,438	1.82
3	2	113,800	2.61
ROW	PORTER AVE.	10,972	0.25
ROW	TBA STREET	90,722	2.08
TOTAL		1,277,585	29.33

THE CITY OF NORMAN, OKLAHOMA  
a municipal corporation

SMC CONSULTING ENGINEERS, P.C.  
815 W. MAIN ST.  
OKLAHOMA CITY, OKLAHOMA 73106  
(405) 232-7715



N:\DWGS\6437 - NRG PORTER CAMPUS\PRELIMINARY FLAT\6437 CO MASTER SITE DEVELOPMENT PLAN 01.DWG  
 Created by: Tracy Thomas, P.E. 10/03/2004 10:53:04



**EXHIBIT D**  
**Allowable Uses**

- Senior wellness center.
- Hospital.
- Behavioral health care
- High Impact Institutional Use.
- Office.
- Administrative or educational buildings.
- General medical uses, such as, but not limited to, variety care, temporary patient observation and short stay uses, laboratory, diagnostic image services, physician offices, EMSTAT, pharmacy, and other similar uses.
- Amusement enterprises.
- Bakery.
- Bus terminal.
- Carpenter and cabinet shop.
- Cleaning and dyeing works.
- Electric transmission station.
- Frozen food locker.
- Glass shop.
- Heating, ventilating or plumbing supplies, sales and service.
- Ice plant or storage house for ice and food housing not more than ten (10) tons capacity.
- Laundry.
- Music, radio or television shop.
- Outdoor advertising signs.
- Small animal hospital.
- Storage warehouse.
- Hotel.
- Art Gallery.
- Assembly Halls of non-profit corporations.
- Laboratories for research and testing where all work is housed in buildings.
- Libraries.
- Museums.
- Music Conservatories.
- Public and private schools and college with or without students in residence and dormitories associated therewith.
- Trade schools and schools for vocational training.
- Churches.
- Child Care Center.
- Antique shop.
- Appliance Store.
- Artist materials supply, or studio.
- Automobile parking lots.
- Baby shop.

- Bank.
- Barber shop, or beauty parlor.
- Book or stationery store.
- Camera shop.
- Candy store.
- Catering establishment.
- Clothing or apparel store.
- Dairy products or ice cream store.
- Delicatessen store.
- Dress shop.
- Drug store or fountain.
- Dry Cleaning and Laundry Plant and/or Laundry Pick-up Station
- Dry goods store.
- Fabric or notion store.
- Florist.
- Furniture Store
- Gift Shop.
- Grocery or supermarket.
- Interior decorating store.
- Jewelry shop.
- Leathergoods shop.
- Messenger or telegraph service.
- Office business.
- Outdoor or indoor sport or recreation areas, including, but not limited to, courts for handball, racquet ball, tennis, basketball, or sports of a similar nature (lighted outdoor courts shall not to be operated later in the evening than 10:00 p.m. and lighting must be arranged to direct light away from any adjoining property in a residential district).
- Painting and decorating shop.
- Pet shop.
- Pharmacy.
- Photographer's studio.
- Radio and television sales and service.
- Restaurant. A restaurant may include live entertainment and/or a dance floor, (all such activity fully within an enclosed building) provided the kitchen remains open with full food service whenever live entertainment is offered.
- Sewing machine sales.
- Sporting goods sales.
- Shoe store or repair shop.
- Tailor shop.
- Theater (excluding drive-in theaters), including one that sells alcoholic beverages in compliance with state law.
- Toy store
- Emergency Medical Transportation Services.
- Municipal use, public buildings, and public utility.

- Senior living facilities, including but not limited to senior independent living, assisted living, and memory care facilities.
- Mixed use buildings with a combination of retail and residential units. The retail uses shall be on the ground floor and will feature general neighborhood-oriented retail uses, including but not limited to the following:
  - (1) Antique store
  - (2) Art gallery
  - (3) Bank (drive-through must be at side or rear of structure)
  - (4) Book store, music store
  - (5) Camera shop
  - (6) Candy, ice cream, or confection shop
  - (7) Catering business
  - (8) Clothing and apparel store
  - (9) Daycare facility (child or adult)
  - (10) Design Offices, including but not limited to Interior Decorator (including sales)
  - (11) Florist
  - (12) Food or drug stores (bakery, delicatessen, grocery, pharmacy)
  - (13) Health club or spa
  - (14) Indoor arcade, including electronic amusement
  - (15) Jewelry store
  - (16) Key shop
  - (17) Office use (including medical offices)
  - (18) Personal services
  - (19) Pet store
  - (20) Repair shop
  - (21) Shoe shop (including repair)
  - (22) Small electronic equipment (including sales and repair)
  - (23) Toy store
  - (24) Restaurants
  - (25) Studios and shops of artists and artisans (including sales)

**NORMAN PLANNING COMMISSION  
REGULAR SESSION MINUTES**

**DECEMBER 9, 2021**

The Planning Commission of the City of Norman, Cleveland County, State of Oklahoma, met in Regular Session in the Council Chambers of the Norman Municipal Building, 201 West Gray Street, on the 9<sup>th</sup> day of December, 2021.

Notice and agenda of the meeting was posted at the Norman Municipal Building and online at <https://norman-ok.municodemeetings.com> at least twenty-four hours prior to the beginning of the meeting.

Chair Erica Bird called the meeting to order at 6:30 p.m.

\* \* \*

**ROLL CALL**

**MEMBERS PRESENT**

Erin Williford (arrived at 6:32 p.m.)  
Kevan Parker  
Steven McDaniel  
Erica Bird  
Dave Boeck  
Sandy Bahan  
Michael Jablonski

**MEMBERS ABSENT**

Lark Zink

A quorum was present.

**STAFF MEMBERS PRESENT**

Jane Hudson, Director, Planning &  
Community Development  
Lora Hoggatt, Planning Services Manager  
Logan Hubble, Planner I  
Anais Starr, Planner II  
Colton Wayman, Planner I  
Roné Tromble, Recording Secretary  
Ken Danner, Subdivision Development  
Manager  
Kathryn Walker, City Attorney  
Heather Poole, Asst. City Attorney  
Jeanne Snider, Asst. City Attorney  
Jami Short, Traffic Engineer  
Bryce Holland, Multimedia Specialist

\* \* \*

Item No. 10, being:

**CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF RESOLUTION NO. R-2122-57 FOR NORMAN REGIONAL HEALTH SYSTEM AND THE CITY OF NORMAN REQUEST AMENDMENT OF THE NORMAN 2025 LAND USE AND TRANSPORTATION PLAN FROM INSTITUTIONAL DESIGNATION TO MIXED USE DESIGNATION FOR APPROXIMATELY 29.3 ACRES OF PROPERTY GENERALLY LOCATED SOUTH OF E. ROBINSON STREET, WEST OF N. FINDLAY AVENUE, NORTH OF E. FRANK STREET, AND EAST OF N. PORTER AVENUE.**

**ITEMS SUBMITTED FOR THE RECORD:**

1. NORMAN 2025 Map
2. Staff Report
3. Pre-Development Summary

Item No. 11, being:

**CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF ORDINANCE NO. O-2122-29 FOR NORMAN REGIONAL HEALTH SYSTEM AND THE CITY OF NORMAN REQUEST REZONING FROM R-1, SINGLE FAMILY DWELLING DISTRICT, C-3, INTENSIVE COMMERCIAL DISTRICT, AND O-1, OFFICE-INSTITUTIONAL DISTRICT, TO PUD, PLANNED UNIT DEVELOPMENT, FOR APPROXIMATELY 29.3 ACRES OF PROPERTY GENERALLY LOCATED SOUTH OF E. ROBINSON STREET, WEST OF N. FINDLAY AVENUE, NORTH OF E. FRANK STREET, AND EAST OF N. PORTER AVENUE.**

**ITEMS SUBMITTED FOR THE RECORD:**

1. Location Map
2. Staff Report
3. PUD Narrative with Exhibits A-D
4. Pre-Development Summary

Item No. 12, being:

**CONSIDERATION OF APPROVAL, ACCEPTANCE, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF PP-2122-9, A PRELIMINARY PLAT SUBMITTED BY NORMAN REGIONAL HEALTH SYSTEM AND THE CITY OF NORMAN (SMC CONSULTING ENGINEERS, P.C.) FOR NORMAN REGIONAL HEALTH SYSTEM PORTER CAMPUS, A PLANNED UNIT DEVELOPMENT FOR APPROXIMATELY 29.3 ACRES OF PROPERTY GENERALLY LOCATED SOUTH OF E. ROBINSON STREET, WEST OF N. FINDLAY AVENUE, NORTH OF E. FRANK STREET, AND EAST OF N. PORTER AVENUE.**

**ITEMS SUBMITTED FOR THE RECORD:**

1. Location Map
2. Preliminary Plat
3. Staff Report
4. Transportation Impacts
5. Preliminary Site Development Plan
6. Master Site & Utility Development Plan
7. Pre-Development Summary

Item No. 13, being:

**CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF ORDINANCE NO. O-2122-30 FOR NORMAN REGIONAL HEALTH SYSTEM AND THE CITY OF NORMAN REQUEST VACATION AND CLOSURE OF CERTAIN PUBLIC INTERESTS IN AN ALLEY, A PORTION OF PONCA AVENUE, AND A PORTION OF GRIFFIN AVENUE.**

**ITEMS SUBMITTED FOR THE RECORD:**

1. Location Map
2. Staff Report
3. Request to Vacate/Close Public Easement

**PRESENTATION BY STAFF:**

1. Ms. Lora Hoggatt presented the staff report. The Parks Board met earlier in the day and recommended fee in lieu to be assigned to the Frances Cate Park, instead of an on-site park.

Commissioner Jablonski asked a question about the traffic study.  
Ms. Jami Short, Traffic Engineer, responded.

**PRESENTATION BY THE APPLICANT:**

Mr. Sean Rieger, representing the applicant, made comments.  
Mr. Richie Splitt, CEO, talked about the Inspire Health program.  
Mr. Sean Rieger explained the mechanics of the site, the zoning and platting and the rights-of-way and easements to be closed and vacated.  
Ms. Kathryn Walker, City Attorney, discussed the Adult Wellness Center.  
Commissioner Boeck made comments.  
Mr. Todd Butler, Traffic Engineering Consultants, discussed traffic issues at Porter and Robinson.  
Commissioner Parker thanked the team for their work.  
Commissioner Bird asked about emergency services at the site.  
Mr. Richie Splitt responded.  
Commissioner Bird asked about the proposed nine lots.  
Mr. Sean Rieger and Mr. Richie Splitt responded.

**AUDIENCE PARTICIPATION:**

Rachelle HardIn-Montz, 2601 132<sup>nd</sup> Avenue S.E., made comments regarding the behavioral health facility.

**DISCUSSION AND ACTION BY THE PLANNING COMMISSION:**

Commissioner Boeck made comments.  
Commissioner Bird made comments.

*Dave Boeck moved to recommend adoption of Resolution No. R-2122-57, Ordinance No. O-2122-29, PP-2122-9, and Ordinance No. O-2122-30 to City Council. Kevan Parker seconded the motion.*

There being no further discussion, a vote on the motion was taken with the following result:

YEAS	Erin Williford, Kevan Parker, Steven McDaniel, Erica Bird, Dave Boeck, Sandy Bahan, Michael Jablonski
NAYES	None
MEMBERS ABSENT	Lark Zink

The motion, to recommend adoption of Resolution No. R-2122-57, Ordinance No. O-2122-29, PP-2122-9, and Ordinance No. O-2122-30 to City Council, passed by a vote of 7-0.

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**File Attachments for Item:**

13. CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF ORDINANCE O-2122-30 UPON SECOND AND FINAL READING: AN ORDINANCE OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, CLOSING CERTAIN PUBLIC INTERESTS IN AN ALLEY AND RIGHTS-OF-WAY LOCATED AT THE NORMAN REGIONAL HEALTH SYSTEM PORTER CAMPUS SITE; AND PROVIDING FOR THE SEVERABILITY THEREOF.



## CITY OF NORMAN, OK STAFF REPORT

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**MEETING DATE:** January 25, 2022

**REQUESTER:** Ken Danner, Subdivision Development Manager

**PRESENTER:** Shawn O'Leary, Director of Public Works

**TITLE:** CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF ORDINANCE O-2122-30 UPON SECOND AND FINAL READING: AN ORDINANCE OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, CLOSING CERTAIN PUBLIC INTERESTS IN AN ALLEY AND RIGHTS-OF-WAY LOCATED AT THE NORMAN REGIONAL HEALTH SYSTEM PORTER CAMPUS SITE; AND PROVIDING FOR THE SEVERABILITY THEREOF.

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### **BACKGROUND:**

This is a request for closing certain platted rights-of-way (typically used for alley or street purposes and utilities). Various easements and rights-of-way were dedicated over the years with the filing of final plats for High School Addition, Whitwell Addition, Northeast Addition and Griffin Heights Addition. Also, over the years, several rights-of-way and easements have been vacated by City Council.

### **DISCUSSION:**

The applicant's representatives have made a request to close certain rights-of-way for this proposed development. Franchised utilities were not notified to assess the impact on their facilities if the rights-of-way are closed because the Norman Regional Health System and City of Norman will provide utility easements to cover existing or proposed utilities with future final platting.

At this time, the request is for closure of the rights-of-way used for alley and streets that will be in conflict with the overall proposal.

Planning Commission, at its meeting of December 9, 2021, recommended adoption by a vote of 7-0.

### **RECOMMENDATION:**

With the overall design of the new proposal, staff supports the closure as described in the applicant's request.

O-2122-30

AN ORDINANCE OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, CLOSING CERTAIN PUBLIC INTERESTS IN AN ALLEY AND RIGHTS-OF-WAY THE NORMAN REGIONAL HEALTH SYSTEM PORTER CAMPUS SITE; AND PROVIDING FOR THE SEVERABILITY THEREOF.

BE IT ORDAINED BY THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA:

- § 1. That, pursuant to Resolution Number R-8182-66, Norman Regional Health System and the City of Norman, the owners of the subject property, has petitioned the City to have certain public interests in an alley and rights-of-way closed; and,
- § 2. That, also pursuant to Resolution Number R-8182-66, the proper notice has been given, and the maps, memorandums and other items required by said Resolution have been presented to this Council; and
- § 3. That, also pursuant to Resolution Number R-8182-66, a public hearing has been held regarding said closing; and
- § 4. That, the portion of the public rights-of-way described as follows are hereby closed:

A tract of land being all of Blocks 1 and 2, WHITWELL ADDITION recorded in Book 1 of Plats, Page 26; AND all of Blocks 1 and 2, GRIFFIN HEIGHTS ADDITION recorded in Book 2 of Plats, Page 62; AND all of Block 1, NORTHEAST ADDITION recorded in Book 1 of Plats, Page 92; AND all of Blocks 1 and 2, Lots 1 through 13, Block 3 and Lots 1 through 12, Block 4, HIGH SCHOOL ADDITION recorded in Book 1 of Plats, Page 32, together with the platted streets and alleys lying within the aforescribed plats AND those unplatted parts of the Northwest Quarter (NW/4) of Section 29, Township 9 North, Range 2 West of the Indian Meridian, Norman, Cleveland County, Oklahoma all lying within the following described tract of land;

COMMENCING at the northwest corner of said Northwest Quarter;  
 THENCE South 00°09'01" East, along the west line of said Northwest Quarter, a distance of 553.00 feet to the POINT OF BEGINNING;  
 THENCE North 89°50'59" East a distance of 420.40 feet;  
 THENCE North 00°09'01" West a distance of 237.84 feet to a point on the south line of Lot1, Block 1, DOCTOR'S PARK NO. 1 recorded in Book 8 of Plats, Page 77 extended;

THENCE North 89°44'54" East, along the south line of said Lot 1 extended, the south line of said Lot 1 and the south line of Lot 1, Block 1, DOCTOR'S PARK NO. 2 recorded in Book 10 of Plats, Page 39, a distance of 615.57 feet to the southeast corner of said Lot 1, also being a point on the west right of way line of Findlay Avenue;

THENCE South 00°09'01" East, along said west right of way line and the east line of Block 1 of said GRIFFIN HEIGHTS ADDITION extended and the east line of said Block 1, a distance of 859.73 feet;

THENCE South 89°26'27" West a distance of 8.00 feet to a point of intersection with the east line of Block 1, NORTHEAST ADDITION extended;

THENCE South 00°09'01" East, along the east line of said Block 1 extended, the east line of said Block 1 and said west right of way line, a distance of 335.00 feet;

THENCE North 89°26'27" East a distance of 8.00 feet;

THENCE South 00°09'01" East a distance of 165.00 feet to a point of intersection with the south line of Block 4, HIGH SCHOOL ADDITION extended;

THENCE South 89°26'27" West, along said south line extended, the south line of said Block 4 and the south line of Block 3 in said Addition, a distance of 1,003.00 feet to the southwest corner of said Block 3, also being a point on the east right of way line of Porter Avenue;

THENCE North 00°09'01" West, along the west line of said Block 3, the west line of Block 2 of said HIGH SCHOOL ADDITION, the west line of Block 1, WHITWELL ADDITION and said east right of way line, a distance of 952.35 feet to the northwest corner of said Block 1, WHITWELL ADDITION;

THENCE South 89°26'27" West a distance of 33.00 feet to a point on the west line of said Northwest Quarter;

THENCE North 00°09'01" West, along said west line, a distance of 175.84 feet to the POINT OF BEGINNING.

Said described tract of land contains an area of 1,277,585 square feet or 29.3293 acres, more or less.

§ 5. Severability. If any section, subsection, sentence, clause, phrase, or portion of this ordinance is, for any reason, held invalid or unconstitutional by any court of competent jurisdiction, such portion shall be deemed a separate, distinct, and independent provision, and such holding shall not affect the validity of the remaining portions of this ordinance.

ADOPTED this \_\_\_\_\_ day of \_\_\_\_\_, 2022.

NOT ADOPTED this \_\_\_\_\_ day of \_\_\_\_\_, 2022.

\_\_\_\_\_  
(Mayor)

\_\_\_\_\_  
(Mayor)

ATTEST:

\_\_\_\_\_  
(City Clerk)



DATE: November 1, 2021

TO: Kathryn Walker, City Attorney  
Chris Mattingly, Director of Utilities  
Ken Danner, Subdivision Manager  
Rone Tromble, Administrative Technician IV  
Jane Hudson, Director of Planning and Community Development

FROM: Brenda Hall, City Clerk

SUBJECT: Request to Vacate/Close Public Easement

I am in receipt of a request to vacate and close an easement for property located in a portion of Ponca Avenue and Griffin Avenue for Norman Regional Health System Porter Campus.

In accordance with Resolution No. R-8182-66, I am forwarding the request, legal description, and certified ownership list to your office and requesting that your office send notice to the furnished list of property owners and have the necessary ordinance prepared. If further action is needed from my office, please notify me.

BH:smr  
attachments

office memorandum



November 1, 2021

Ms. Brenda Hall  
City Clerk  
City of Norman  
201 West Gray  
Norman, OK 73069

**RE: Vacation & Closure of Public Interest**

Dear Ms. Hall,

We submit this request to vacate and close certain public interests in an alley, a portion of Ponca Avenue, and a portion of Griffin Avenue, as more particularly described in the attached application. The Norman Regional Hospital Authority, an Oklahoma Public Trust d/b/a Norman Regional Health System ("NRHS") and the City of Norman, as co-applicants, seek to redevelop the existing NRHS Porter Campus. The applicants are requesting to rezone and preliminary plat the property and the existing alleyway and rights-of-way are incompatible with the applicants' proposed design. Therefore, pursuant to 11 O.S. § 42-101 et seq., we submit this application on behalf of the applicants.

In addition to this request, we hereby submit the filing fee and a certified ownership list reflecting the property owners within 350 feet of the Porter Campus. Please let us know if you need any additional information from us in order to place this item on the agenda for Planning Commission and City Council consideration. Thank you very much for your assistance and cooperation.

Respectfully Submitted,  
**RIEGER LAW GROUP PLLC**

FILED IN THE OFFICE  
OF THE CITY CLERK  
ON 11-1-21

**SEAN PAUL RIEGER**  
Attorney ■ Architect ■ Broker

Sean Paul Rieger  
Attorney ■ Architect ■ Broker

Daniel L. Sadler  
Attorney at Law

Gunner B. Joyce  
Attorney at Law

Keith A. Barrett  
Attorney at Law

Kendra D. Streeter  
Attorney at Law





Planning Commission Agenda  
December 9, 2021

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ORDINANCE NO. O-2122-30

ITEM NO. 13

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**STAFF REPORT**

**GENERAL INFORMATION**

Norman Regional Health System & City of Norman

**REQUESTED ACTION**

Closure of rights-of-way within the property owned by the Norman Regional Health System and City of Norman.

**BACKGROUND:** Various easements and rights-of-way were dedicated over the years with the filing of final plats for High School Addition, Whitwell Addition, Northeast Addition and Griffin Heights Addition. Over the years, several rights-of-way and easements have been vacated by City Council.

**DISCUSSION:** The applicants representatives have made a request to close certain rights-of-way for this proposed development. Franchised utilities were not notified to assess the impact on their facilities if the right-of-way is closed because the Norman Regional Health System and City of Norman will provide utility easements to cover existing or proposed utilities with future final platting.

At this time, the request is for closure of the rights-of-way used for alley and streets that will be in conflict with the overall proposal.

**RECOMMENDATION:** Staff recommends approval of the request to close the twenty-foot (20') right-of-way used for alley purposes subject to the above comments.

**ACTION NEEDED:** Recommend approval or disapproval of the request to close the specific rights-of-way as described in Ordinance No. O-2122-30 to City Council.

**ACTION TAKEN:** \_\_\_\_\_

**NORMAN PLANNING COMMISSION  
REGULAR SESSION MINUTES**

**DECEMBER 9, 2021**

The Planning Commission of the City of Norman, Cleveland County, State of Oklahoma, met in Regular Session in the Council Chambers of the Norman Municipal Building, 201 West Gray Street, on the 9<sup>th</sup> day of December, 2021.

Notice and agenda of the meeting was posted at the Norman Municipal Building and online at <https://norman-ok.municodemeetings.com> at least twenty-four hours prior to the beginning of the meeting.

Chair Erica Bird called the meeting to order at 6:30 p.m.

\* \* \*

**ROLL CALL**

**MEMBERS PRESENT**

Erin Williford (arrived at 6:32 p.m.)  
Kevan Parker  
Steven McDaniel  
Erica Bird  
Dave Boeck  
Sandy Bahan  
Michael Jablonski

**MEMBERS ABSENT**

Lark Zink

A quorum was present.

**STAFF MEMBERS PRESENT**

Jane Hudson, Director, Planning &  
Community Development  
Lora Hoggatt, Planning Services Manager  
Logan Hubble, Planner I  
Anais Starr, Planner II  
Colton Wayman, Planner I  
Roné Tromble, Recording Secretary  
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Manager  
Kathryn Walker, City Attorney  
Heather Poole, Asst. City Attorney  
Jeanne Snider, Asst. City Attorney  
Jami Short, Traffic Engineer  
Bryce Holland, Multimedia Specialist

\* \* \*

Item No. 10, being:

**CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF RESOLUTION NO. R-2122-57 FOR NORMAN REGIONAL HEALTH SYSTEM AND THE CITY OF NORMAN REQUEST AMENDMENT OF THE NORMAN 2025 LAND USE AND TRANSPORTATION PLAN FROM INSTITUTIONAL DESIGNATION TO MIXED USE DESIGNATION FOR APPROXIMATELY 29.3 ACRES OF PROPERTY GENERALLY LOCATED SOUTH OF E. ROBINSON STREET, WEST OF N. FINDLAY AVENUE, NORTH OF E. FRANK STREET, AND EAST OF N. PORTER AVENUE.**

**ITEMS SUBMITTED FOR THE RECORD:**

1. NORMAN 2025 Map
2. Staff Report
3. Pre-Development Summary

Item No. 11, being:

**CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF ORDINANCE NO. O-2122-29 FOR NORMAN REGIONAL HEALTH SYSTEM AND THE CITY OF NORMAN REQUEST REZONING FROM R-1, SINGLE FAMILY DWELLING DISTRICT, C-3, INTENSIVE COMMERCIAL DISTRICT, AND O-1, OFFICE-INSTITUTIONAL DISTRICT, TO PUD, PLANNED UNIT DEVELOPMENT, FOR APPROXIMATELY 29.3 ACRES OF PROPERTY GENERALLY LOCATED SOUTH OF E. ROBINSON STREET, WEST OF N. FINDLAY AVENUE, NORTH OF E. FRANK STREET, AND EAST OF N. PORTER AVENUE.**

**ITEMS SUBMITTED FOR THE RECORD:**

1. Location Map
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3. PUD Narrative with Exhibits A-D
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Item No. 12, being:

**CONSIDERATION OF APPROVAL, ACCEPTANCE, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF PP-2122-9, A PRELIMINARY PLAT SUBMITTED BY NORMAN REGIONAL HEALTH SYSTEM AND THE CITY OF NORMAN (SMC CONSULTING ENGINEERS, P.C.) FOR NORMAN REGIONAL HEALTH SYSTEM PORTER CAMPUS, A PLANNED UNIT DEVELOPMENT FOR APPROXIMATELY 29.3 ACRES OF PROPERTY GENERALLY LOCATED SOUTH OF E. ROBINSON STREET, WEST OF N. FINDLAY AVENUE, NORTH OF E. FRANK STREET, AND EAST OF N. PORTER AVENUE.**

**ITEMS SUBMITTED FOR THE RECORD:**

1. Location Map
2. Preliminary Plat
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5. Preliminary Site Development Plan
6. Master Site & Utility Development Plan
7. Pre-Development Summary

Item No. 13, being:

**CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF ORDINANCE NO. O-2122-30 FOR NORMAN REGIONAL HEALTH SYSTEM AND THE CITY OF NORMAN REQUEST VACATION AND CLOSURE OF CERTAIN PUBLIC INTERESTS IN AN ALLEY, A PORTION OF PONCA AVENUE, AND A PORTION OF GRIFFIN AVENUE.**

**ITEMS SUBMITTED FOR THE RECORD:**

1. Location Map
2. Staff Report
3. Request to Vacate/Close Public Easement

**PRESENTATION BY STAFF:**

1. Ms. Lora Hoggatt presented the staff report. The Parks Board met earlier in the day and recommended fee in lieu to be assigned to the Frances Cate Park, instead of an on-site park.

Commissioner Jablonski asked a question about the traffic study.  
Ms. Jami Short, Traffic Engineer, responded.

**PRESENTATION BY THE APPLICANT:**

Mr. Sean Rieger, representing the applicant, made comments.  
Mr. Richie Splitt, CEO, talked about the Inspire Health program.  
Mr. Sean Rieger explained the mechanics of the site, the zoning and platting and the rights-of-way and easements to be closed and vacated.  
Ms. Kathryn Walker, City Attorney, discussed the Adult Wellness Center.  
Commissioner Boeck made comments.  
Mr. Todd Butler, Traffic Engineering Consultants, discussed traffic issues at Porter and Robinson.  
Commissioner Parker thanked the team for their work.  
Commissioner Bird asked about emergency services at the site.  
Mr. Richie Splitt responded.  
Commissioner Bird asked about the proposed nine lots.  
Mr. Sean Rieger and Mr. Richie Splitt responded.

**AUDIENCE PARTICIPATION:**

Rachelle HardIn-Montz, 2601 132<sup>nd</sup> Avenue S.E., made comments regarding the behavioral health facility.

**DISCUSSION AND ACTION BY THE PLANNING COMMISSION:**

Commissioner Boeck made comments.  
Commissioner Bird made comments.

*Dave Boeck moved to recommend adoption of Resolution No. R-2122-57, Ordinance No. O-2122-29, PP-2122-9, and Ordinance No. O-2122-30 to City Council. Kevan Parker seconded the motion.*

There being no further discussion, a vote on the motion was taken with the following result:

YEAS	Erin Williford, Kevan Parker, Steven McDaniel, Erica Bird, Dave Boeck, Sandy Bahan, Michael Jablonski
NAYES	None
MEMBERS ABSENT	Lark Zink

The motion, to recommend adoption of Resolution No. R-2122-57, Ordinance No. O-2122-29, PP-2122-9, and Ordinance No. O-2122-30 to City Council, passed by a vote of 7-0.

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**File Attachments for Item:**

14. CONSIDERATION OF APPROVAL, ACCEPTANCE, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF A PRELIMINARY PLAT FOR NORMAN REGIONAL HEALTH SYSTEM PORTER CAMPUS ADDITION, A PLANNED UNIT DEVELOPMENT. (GENERALLY LOCATED SOUTH OF ROBINSON STREET BETWEEN PORTER AVENUE AND FINDLAY AVENUE).



## CITY OF NORMAN, OK STAFF REPORT

**MEETING DATE:** 01/25/2022

**REQUESTER:** Ken Danner, Subdivision Development Manager

**PRESENTER:** Shawn O'Leary, Director of Public Works

**TITLE:** CONSIDERATION OF APPROVAL, ACCEPTANCE, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF A PRELIMINARY PLAT FOR NORMAN REGIONAL HEALTH SYSTEM PORTER CAMPUS ADDITION, A PLANNED UNIT DEVELOPMENT. (GENERALLY LOCATED SOUTH OF ROBINSON STREET BETWEEN PORTER AVENUE AND FINDLAY AVENUE).

### **BACKGROUND:**

This item is a preliminary plat for Norman Regional Health System Porter Campus Addition, a Planned Unit Development, generally located 260' south of Robinson Street between Porter Avenue and Findlay Avenue. The preliminary plat consists of 29+ acres and nine (9) lots. Lot 1, Block 1, is the City of Norman Senior Wellness Center; Lot 2, Block 1, is a proposed Behavioral Health Center facility; Lot 3, Block 1, is a proposed Variety Care facility; Lot 4, Block 1, is a proposed Open Space; Lot 5, Block 1, is a proposed mixed-use facility; and Lot 6, Block 1, is a proposed office building. Lot 1, Block 2, contains existing medical facilities; Lot 2, Block 2, is the existing EMSTAT facility; and Lot 3, Block 2, is a proposed mixed-use facility.

The Norman Board of Parks Commissioners, at its meeting of December 9, 2021, (special meeting), recommended a fee in lieu of park land requirements for Norman Regional Health System Porter Campus Addition, a Planned Unit Development, for any potential residential component. Planning Commission, at its meeting of December 9, 2021, by a vote of 7-0, recommended to City Council to amend the NORMAN 2025 Land Use and Transportation Plan from Office Designation to Mixed Use Designation. Also, Planning Commission recommended to City Council that this property be placed in the PUD, Planned Unit Development, and removed from R-1, Single-Family Dwelling District; C-3, Intensive Commercial District; and O-1, Office-Institutional District. In addition, Planning Commission recommended approval of the preliminary plat for Norman Regional Health System Porter Campus Addition, a Planned Unit Development.

### **DISCUSSION:**

The proposed redevelopment of the existing Norman Regional Health Systems (NRHS) which includes medical care facilities, educational facilities, and other commercial land uses is expected to generate approximately 5,014 trips per day, 518 AM peak hour trips, and 502 PM peak hour trips. Traffic capacities on Porter Avenue, Robinson Street, and Findlay Avenue



exceed the demand for existing and proposed trips as a result of this development. No negative traffic impacts are anticipated. However, there was a considerable delay expected at the signalized intersection of Robinson Street and Porter Avenue which results in an unacceptable level of service for the intersection. Traffic impact fees to bring the intersection back to an acceptable level of service have been calculated.

STREET	NO. OF LANES	EXISTING TRAFFIC (Veh/day)	PROJECTED TRAFFIC (Veh/day)	TOTAL PROJECTED TRAFFIC (Veh/day)	ROADWAY CAPACITY L.O.S. "E"	% CAPACITY USED (EXISTING)	% CAPACITY USED (PROJECTED)
Porter Avenue	4	16,301	1,905	18,215	34,200	47.69	54.26
Robinson Street	4	24,159	2,306	26,465	34,200	70.64	77.38
Findlay Avenue	2	2,605	602	3,207	17,100	15.23	18.75

Being above the threshold for when a traffic impact study is required, Traffic Engineering Consultants, Inc., submitted a traffic impact study in November 2021. The development will feature ten total access points. Four of these are located along Porter Avenue, five will be along Findlay Avenue and one will be along Robinson Street. All connections to public roadways will afford full access.

While no negative traffic impacts are anticipated, due to the increase in delay anticipated at the signalized intersection of Robinson Street and Porter Avenue, a modification to the eastbound Robinson Street approach to Porter Avenue along with signal timing adjustments is recommended with staff concurrence. The modification includes adding a right turn lane for the eastbound approach to turn south onto Porter Avenue. The estimated cost of construction for this exclusive right turn lane is \$250,000.

Traffic impact fees were calculated based on the amount of trips for the redevelopment and the cost estimate mentioned previously. The traffic impact fee was calculated to be \$64.50 per trip for a total of \$21,420. Traffic impact fees broken down separately for the NRHS portion of the development will be \$19,030 and the City of Norman Senior Wellness Center will be \$2,390. The traffic impact fees should be collected with the filing of the Final Plat.

#### **PUBLIC IMPROVEMENTS:**

1. **Fire Hydrants.** Fire hydrants will be installed in accordance with approved plans. Their locations have been approved by the Fire Department.
2. **Permanent Markers.** Permanent markers will be installed prior to the final plat being filed of record.
3. **Sanitary Sewers.** A sanitary sewer main will be installed in accordance with approved plans and City and State Department of Environmental Quality standards.
4. **Sidewalks.** Sidewalks will be constructed adjacent to all public streets.
5. **Storm Sewers.** Storm sewers and appurtenant drainage structures will be installed in accordance with approved plans and City drainage standards. Storm water runoff will be

conveyed to privately maintained detention facilities. With the overall proposal, there is less impervious cover than what is existing.

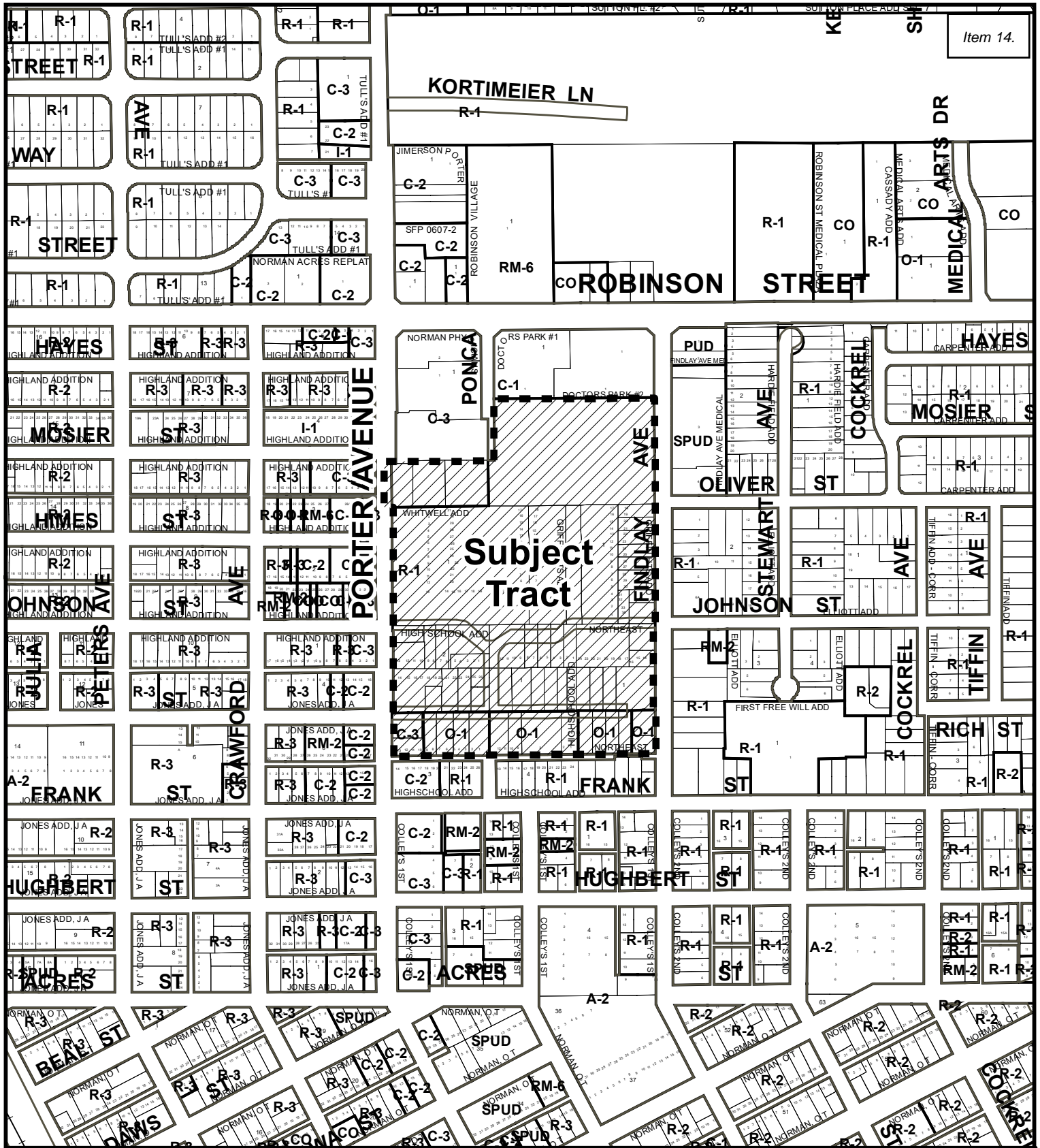
6. **Streets**. Porter Avenue is part of a City of Norman Streetscape Project. Findlay Avenue is existing. The new interior street will be constructed to City standards with final platting.
7. **Water Main**. Water mains will be installed in accordance with approved plans and City and Department of Environmental Quality standards. There is an existing 12-inch water main adjacent to Porter Avenue. There is an existing 6-inch water main adjacent to Findlay Avenue. There is a proposed 8-inch water line adjacent to a new interior street.

#### **PUBLIC DEDICATIONS.**

1. **Rights-of Way and Easements**. All rights-of-way and easements will be dedicated to the City with final platting.
2. **Park Land**. The Norman Board of Parks Commissioners recommended fee in lieu of park land requirements for any proposed residential area.

#### **RECOMMENDATION:**

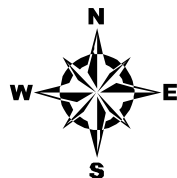
Staff recommends approval of the preliminary plat for Norman Regional Health System Porter Campus Addition, a Planned Unit Development.



# Location Map



Map Produced by the City of Norman  
Geographic Information System.  
The City of Norman assumes no  
responsibility for errors or omissions  
in the information presented.



November 4, 2021

0 250 500 Ft.



Subject Tract



Zoning

PRELIMINARY PLAT  
PP-2122-9

ITEM NO. 12

**STAFF REPORT****ITEM:** Consideration of a Preliminary Plat for **NORMAN REGIONAL HEALTH SYSTEM PORTER CAMPUS, A PLANNED UNIT DEVELOPMENT.****LOCATION:** Generally located 260' south of Robinson Street between Porter Avenue and Findlay Avenue.**INFORMATION:**

1. Owner. Norman Regional Health System and City of Norman.
2. Developer. Norman Regional Health System and City of Norman.
3. Engineer. SMC Consulting Engineers, P.C.

**HISTORY:**

1. July 1, 1909. The final plat for High School Addition was filed of record with the Cleveland County Clerk
2. May 20, 1913. City Council adopted Ordinance No. 209 annexing a portion of this property into the Norman Corporate City limits without zoning.
3. December 10, 1921. The final plat for Whitwell Addition was filed of record with the Cleveland County Clerk.
4. September 8, 1925. The final plat for Northeast Addition was filed of record with the Cleveland County Clerk.
5. March 28, 1939. The final plat for Griffin Heights Addition was filed of record with the Cleveland County Clerk.
6. October 13, 1942. City Council adopted Ordinance No. 627 annexing a portion of this property into the Norman Corporate City limits without zoning.
7. February 9, 1943. City Council adopted Ordinance No. 626 annexing a portion of this property into the Norman Corporate City limits without zoning.

## HISTORY (CONT.)

8. July 13, 1954. City Council adopted Ordinance No. 884 placing this property in R-1, Single-Family Dwelling District and C-3, Intensive Commercial District.
9. February 24, 1959. City Council adopted Ordinance No. 1130 vacating certain easements in Griffin Heights Addition.
10. November 26, 1963. City Council adopted Ordinance No. 1594 vacating a portion of Ponca Avenue north of Johnson Street.
11. October 14, 1969. City Council adopted Ordinance No. 2232 closing and vacating Griffin Avenue north of Johnson Street.
12. January 12, 1988. City Council adopted Ordinance No. O-8788-25 closing Johnson Street between Porter Avenue and Findlay Avenue; Ponca Avenue between Johnson Street and Rich Street and the 20' right-of-way (alley) south of Johnson Street between Porter Avenue and Findlay Avenue.
13. August 12, 1999. Planning Commission, on a vote of 8-0, recommended to City Council that a portion of this property be placed in the O-1, Office-Institutional District and removed from R-1, Single-Family Dwelling District.
14. October 12, 1999. City Council adopted Ordinance No. O-9900-9 placing a portion of this property in the O-1, Office-Institutional District and removing it from R-1, Single-Family Dwelling District.
15. May 11, 2000. Planning Commission, on a vote of 7-0, recommended to City Council that a portion of this property be placed in the O-1, Office-Institutional District and removed from R-1, Single-Family Dwelling District.
16. June 13, 2000. City Council adopted Ordinance No. O-9900-52 placing a portion of this property in the O-1, Office-Institutional District and removing it from R-1, Single-Family Dwelling District.
17. June 13, 2000. City Council adopted Ordinance No. O-9900-53 closing a portion of Rich Street from Porter Avenue to 90-feet west of Findlay Avenue and a portion of Ponca Avenue from the south line of Rich Street south to the 20-foot right-of-way (alley), for a distance of 140-feet.
18. July 10, 2001. City Council adopted Ordinance No. O-0001-70 closing part of Ponca Avenue from a point beginning 432.35 feet north of Johnson Street thence running north for a distance of 175-feet.

## HISTORY (CONT.)

19. November 14, 2002. Planning Commission, on a vote of 8-0, recommended to City Council that a portion of this property be placed in the O-1, Office-Institutional District and removed from R-1, Single-Family Dwelling District.
20. December 17, 2002. City Council adopted Ordinance No. O-0203-18 placing a portion of this property in the O-1, Office-Institutional District and removing it from R-1, Single-Family Dwelling District.
21. December 17, 2002. City Council adopted Ordinance No. O-0203-17 closing a portion of Rich Street from Findlay Avenue west for a distance of 90-feet.
22. December 9, 2021. The Norman Board of Parks Commissioners is scheduled to consider the preliminary plat for Norman Regional Health System Porter Campus, a Planned Unit Development. Results of that review will be presented separately.
23. December 9, 2021. The applicants have requested amending the NORMAN 2025 Land Use and Transportation Plan from Office Designation to Mixed Use Designation.
24. December 9, 2021. The applicants have requested placing this property in the PUD, Planned Unit Development and removing it from R-1, Single-Family Dwelling District, C-3, Intensive Commercial District and O-1, Office-Institutional District.
25. December 9, 2021. The applicants have requested closing various easements and rights-of-way within the property.

## IMPROVEMENT PROGRAM:

1. Fire Hydrants. Fire hydrants will be installed in accordance with approved plans.
2. Permanent Markers. Permanent markers will be installed prior to filing of the final plats.
3. Sanitary Sewers. Sanitary sewer mains will be installed in accordance with approved plans and City and State Department of Environmental Quality standards.
4. Sidewalks. Sidewalks will be constructed adjacent to all public streets.

5. Storm Sewers. Storm sewers and appurtenant drainage structures will be installed in accordance with approved plans and City drainage standards. Storm water runoff will be conveyed to privately maintained detention facilities. With the overall proposal, there is less impervious cover than what is existing.
6. Streets. Porter Avenue is part of a City of Norman Streetscape Project. Findlay Avenue is existing. The new interior street will be constructed to City standards with final platting.
7. Water Mains. Water mains will be installed in accordance with approved plans and City and Department of Environmental Quality standards. There is an existing 12-inch water main adjacent to Porter Avenue. There is an existing 6-inch water main adjacent to Findlay Avenue. There is a proposed 8-inch water line adjacent to a new interior street.

**PUBLIC DEDICATIONS:**

1. Easements. All required easements will be dedicated to the City on the final plat.
2. Rights-of-Way. All street rights-of-way will be dedicated to the City on the final plat.

**SUPPLEMENTAL MATERIAL:** Copies of a location map, preliminary site development plan and preliminary plat are included in the Agenda Book.

**STAFF COMMENTS AND RECOMMENDATION:** The preliminary plat consists of 29+ acres and nine (9) lots. Lot 1, Block 1 is City of Norman Senior Wellness Center, Lot 2, Block 1 is a proposed BMS facility, Lot 3, Block 1 is a proposed Variety Care facility, Lot 4, Block 1 is a proposed Open Space, Lot 5, Block 1 is a proposed mixed-use facility and Lot 6, Block 1 is a proposed office building. Lot 1, Block 2, are existing medical facilities. Lot 2, Block 2 is the existing EMSTAT facility and Lot 3, Block 2 is a proposed mixed-use facility. Staff recommends approval of the preliminary plat for Norman Regional Health System Porter Campus, a Planned Unit Development.

**ACTION NEEDED:** Recommend approval or disapproval of the preliminary plat for Norman Regional Health System Porter Campus, a Planned Unit Development to City Council subject to the approval of Resolution No. R-2122-57 and Ordinance No. O-2122-29.

**ACTION TAKEN:** \_\_\_\_\_





# CITY OF NORMAN

## Development Review Form

### Transportation Impacts

**DATE:** December 2, 2021

**STAFF REVIEW BY:** Jami L. Short, P.E.  
City Traffic Engineer

**PROJECT NAME:** NRHS Porter Campus PP      **PROJECT TYPE:** Mixed Use PUD  
**Owner:** Norman Regional Health Systems  
**Developer's Engineer:** SMC  
**Developer's Traffic Engineer:** TEC

#### **SURROUNDING ENVIRONMENT (Streets, Developments)**

Commercial development surrounds the development area with some low density residential further east and west and a medium density housing in the form of a senior living community among commercial development to the north.

#### **ALLOWABLE ACCESS:**

The site proposes ten total access points. Four access points are located along Porter Avenue, five will be along Findlay Avenue and one will be along Robinson Street. Locations of all access points meet the applicable requirements in the City's Engineering Design Criteria.

#### **EXISTING STREET CHARACTERISTICS (Lanes, Speed Limits, Sight Distance, Medians)**

Porter Avenue: 4 lanes (existing and future). Speed Limit - 30 mph. No sight distance problems. A striped median exists.

Robinson Street: 4 lanes (existing and future). Speed Limit - 35 mph. No sight distance problems. A two way left turn lane (TWLTL) exists.

Findlay Avenue: 2 lanes (existing and future). Speed Limit - 25 mph. No sight distance problems. No median.

**ACCESS MANAGEMENT CODE COMPLIANCE:**                      YES ☒                      NO ☐

Proposed number of access points for the development is in compliance with what is allowed in the subdivision regulations.

#### **TRIP GENERATION**

Time Period	Total	In	Out
Weekday	5,014	2,507	2,507
A.M. Peak Hour	518	417	101
P.M. Peak Hour	502	108	394

**TRANSPORTATION IMPACT STUDY REQUIRED?**                      YES ☒                      NO ☐

Being above the threshold for when a traffic impact study is required, Traffic Engineering Consultants, Inc., submitted a traffic impact study in November 2021. The development will feature ten total access points. Four access points are located along Porter Avenue, five will be along Findlay Avenue and one will be along Robinson Street. All connections to public roadways will afford full access.

While no negative traffic impacts are anticipated, however based on the traffic impact analysis there was an increase in delay at the signalized intersection of Robinson Street and Porter Avenue. This would cause the intersection to operate at an unacceptable level of service, therefore a modification to the eastbound Robinson Street approach to Porter Avenue, along with signal timing adjustments, is recommended with staff concurrence. The modification includes adding a right turn lane for the eastbound approach to turn south onto Porter Avenue. Traffic impact fees were calculated based on the cost estimate to construct the right turn lane and the number of trips the redevelopment will generate. The traffic impact fee was calculated to be \$64.50 per trip for a total of \$21,420. Traffic impact fees broken down separately for the NRHS portion of the development will be \$19,030 and the City of Norman Senior Wellness Center will be \$2,390. Traffic impact fees are due upon the filing of the Final Plat.

**RECOMMENDATION:**    APPROVAL ☒    DENIAL ☐ N/A ☐ STIPULATIONS ☐

*Recommendations for Approval refer only to the transportation impact and do not constitute an endorsement from City Staff.*

The proposed redevelopment of the existing NRHS which includes medical care facilities, educational facilities, and other commercial land uses are expected to generate approximately 5,014 trips per day, 518 AM peak hour trips, and 502 PM peak hour trips. Traffic capacities on Porter Avenue, Robinson Street, and Findlay Avenue, with improvements identified above, exceed the demand for existing and proposed trips as a result of this development.

### NRHS PORTER CAMPUS (PRELIMINARY PLAT)

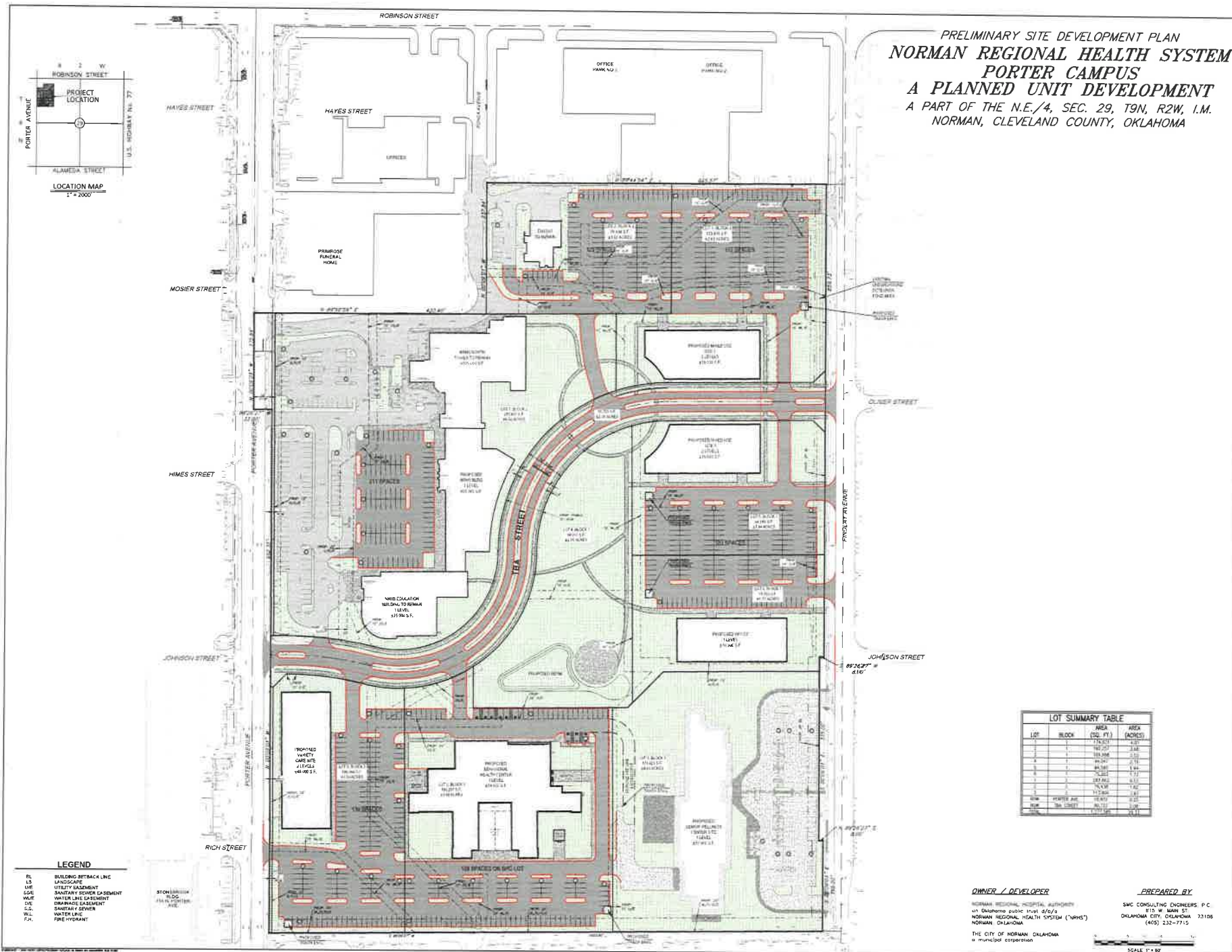
The proposed redevelopment of the existing Norman Regional Health Systems (NRHS) which includes medical care facilities, educational facilities, and other commercial land uses are expected to generate approximately 5,014 trips per day, 518 AM peak hour trips, and 502 PM peak hour trips. Traffic capacities on Porter Avenue, Robinson Street, and Findlay Avenue exceed the demand for existing and proposed trips as a result of this development. No negative traffic impacts are anticipated. However, there was considerable delay expected at the signalized intersection of Robinson Street and Porter Avenue which results in an unacceptable level of service for the intersection. Traffic impact fees to bring the intersection back to an acceptable level of service have been calculated.

STREET	NO. OF LANES	EXISTING TRAFFIC (Veh/day)	PROJECTED TRAFFIC (Veh/day)	TOTAL PROJECTED TRAFFIC (Veh/day)	ROADWAY CAPACITY L.O.S. "E"	% CAPACITY USED (EXISTING)	% CAPACITY USED (PROJECTED)
Porter Avenue	4	16,310	1905	18,215	34,200	47.69	53.26
Robinson Street	4	24,159	2306	26,465	34,200	70.64	77.38
Findlay Avenue	2	2,605	602	3,207	13,680	19.04	23.44

Being above the threshold for when a traffic impact study is required, Traffic Engineering Consultants, Inc., submitted a traffic impact study in November 2021. The development will feature ten total access points. Four of these are located along Porter Avenue, five will be along Findlay Avenue and one will be along Robinson Street. All connections to public roadways will afford full access.

While no negative traffic impacts are anticipated, due to the increase in delay anticipated at the signalized intersection of Robinson Street and Porter Avenue, a modification to the eastbound Robinson Street approach to Porter Avenue, along with signal timing adjustments, is recommended with staff concurrence. The modification includes adding a right turn lane for the eastbound approach to turn south onto Porter Avenue. The estimated cost of construction for this exclusive right turn lane to be \$250,000.

Traffic impact fees were calculated based on the amount of trips for the redevelopment and the cost estimate mentioned previously. The traffic impact fee amount was calculated to be \$64.50 per trip for a total of \$21,420. Traffic impact fees broken down separately for the NRHS portion of the development will be \$19,030 and the City of Norman Senior Wellness Center will be \$2,390. The traffic impact fees should be collected with the filing of the Final Plat.



**PRELIMINARY  
NOT FOR  
CONSTRUCTION**

**NORMAN REGIONAL HEALTH SYSTEM -  
PORTER CAMPUS**  
**PORTER AVENUE & ROBINSON STREET  
NORMAN, OKLAHOMA**

**SMC**  
S.M. CONSULTING ENGINEERS, P.C.  
1111 NORTH MAIN STREET  
NORMAN, OKLAHOMA 73106  
(405) 232-7715

**PRELIMINARY SITE  
DEVELOPMENT PLAN**



LEGAL DESCRIPTION:

A TRACT OF LAND BEING ALL OF BLOCKS 1 AND 2, WHITEHALL ACQUISITION RECORDED IN BOOK 1 OF PLATS, PAGE 38, AND ALL OF BLOCKS 1 AND 2, UPRICH FISHING ACQUISITION RECORDED IN BOOK 1 OF PLATS, PAGE 52, AND ALL OF BLOCK 1, NORTHPORT ACQUISITION RECORDED IN BOOK 1 OF PLATS, PAGE 56, AND ALL OF BLOCKS 1 AND 2, LOTS 1 THROUGH 12, BLOCK 3 AND LOTS 1 THROUGH 12, BLOCK 4, HIGH SCHOOL ACQUISITION RECORDED IN BOOK 1 OF PLATS, PAGE 11, TOGETHER WITH THE PLATTED STREETS AND ALLEYS JOINING THE ABOVE DESCRIBED PLATS AND THESE UNPLATTED PORTS OF THE MENTIONED QUARTER (PART) OF SECTION 29, TOWNSHIP 9 NORTH, RANGE 3 WEST OF THE 10TH MERIDIAN, ANCHORAGE, CHUGACH AND COASTAL, ALASKA, ALL OF WHICH ARE THE FOLLOWING DESCRIBED:

COMPOUNDING OF THE HORNWELL GROUP OF TWO HORNWELL QUARTZES

THENCE SOUTH 02°09'01" EAST, ALONG THE WEST LINE OF SAID NORTHWEST QUARTER A DISTANCE OF 553.00 FEET TO THE POINT OF BEGINNING.

THENCE NORTH 89°30'59" EAST A DISTANCE OF 420.40 FEET

TENNESSEE DEPARTMENT OF REVENUE  
TAXPAYER'S NAME: [REDACTED]  
ADDRESS: [REDACTED]  
CITY: [REDACTED] STATE: [REDACTED] ZIP: [REDACTED]  
FEDERAL ID NUMBER: [REDACTED]  
TAXPAYER'S SIGNATURE: [REDACTED]  
DATE: [REDACTED]

THIS DOCUMENT IS THE PROPERTY OF THE TENNESSEE DEPARTMENT OF REVENUE. IT IS TO BE RETURNED TO THE DEPARTMENT OF REVENUE WHEN REQUESTED.

IF YOU HAVE ANY QUESTIONS OR NEED ASSISTANCE, PLEASE CALL THE TENSURE SERVICE CENTER AT 800-762-2869.

FOR MORE INFORMATION, VISIT OUR WEBSITE AT [WWW.TN.GOV/REVENUE](http://www.tn.gov/revenue).

THANK YOU FOR YOUR COOPERATION AND COMPLIANCE WITH TAX LAWS.

Sincerely,  
[Signature]  
Director, Tennessee Department of Revenue

ENCLOSURE

THENCE NORTH 89°14" EAST, ALONG THE SOUTH LINE OF SAID LOT 1 EXTENDED, THE SOUTH LINE OF SAID LOT 1 AND THE SOUTH LINE OF LOT 1, SUBDIVISION 1, DISTRICT PARK NO. 2 RECORDED IN BOOK 10 OF PLATS, PAGE 35, A DISTANCE OF 88.2 FEET TO THE SOUTHWEST CORNER OF SAID LOT 1, ALSO BEING A POINT ON THE WEST RIGHT OF WAY LINE OF FOREVER WATERS.

THENCE SOUTH 00°09'01" EAST, ALONG SAID WEST RIGHT OF WAY LINE AND THE EAST LINE OF BLOCK 1 OF SAID GRIFFIN HEIGHTS ADDITION EXTENDED AND THE EAST LINE OF SAID BLOCK 1, A DISTANCE OF 204.73 FEET;

THENCE SOUTH 00°02'01" EAST, ALONG THE EAST LINE OF SAID BLOCK 1 EXTENDED, THE EAST LINE OF SAID BLOCK 1 AND SAID WEST RIGHT OF WAY LINE, A DISTANCE OF 335.00 FEET.

THENCE NORTH 89°26'27" EAST A DISTANCE OF 8.00 FEET.

THENCE SOUTH 00°29'01" EAST A DISTANCE OF 165.00 FEET TO A POINT OF INTERSECTION WITH THE SOUTH LINE OF BLOCK 4, HIGH SCHOOL ADDITION EXTENDED;

THENCE SOUTH 89°37'23" WEST, ALONG SAID SECTION LINE, 400.00 FEET, TO THE 1/4 SECTION LINE OF BLOCK 2, T.40S. R.10E. S.10E. A DISTANCE OF 1,000.00 FEET TO THE SOUTHWEST CORNER OF SAID BLOCK 2, ALSO BEING A POINT ON THE EAST-HAND OF WAY LINE OF PORTER HIGHWAY.

THENCE NORTH 00°00'00" WEST, ALONG THE WEST LINE OF SAID BLOCK 1, THE WEST LINE OF BLOCK 2 OF SAID HIGH SCHOOL, ALONG THE WEST LINE OF BLOCK 1, MERRILL ADDITION AND SAID EAST RIGHT OF WAY LINE, A DISTANCE OF 432.25 FEET TO THE NORTHWEST CORNER OF SAID BLOCK 1, MERRILL ADDITION.

THENCE SOUTH 89°28'27" WEST A DISTANCE OF 33.00 FEET TO A POINT ON THE WEST LINE OF SAID NORTHWEST QUARTER.

SAID DESCRIBED TRACT OF LAND CONTAINS AN AREA OF 1,277,585 SQUARE FEET OR 29.3293 ACRES MORE OR LESS

THE BASIS OF BEARINGS FOR THIS LEGAL DESCRIPTION WAS THE OKLAHOMA STATE PLANE COORDINATE SYSTEM (NAD83-SOUTH ZONE) USING A BEARING OF SOUTH 00°00'01" EAST ON THE WEST LINE OF THE NORTHWEST QUARTER OF SECTION 20, TOWNSHIP 8 NORTH, RANGE 2 WEST OF THE INDIAN MERIDIAN.

## NOTES:

1. THE STANDARDS WILL BE LOCATED AND INSTALLED IN ACCORDANCE WITH THE FINAL PLANS AND THE CITY OF NORMAN STANDARDS AND SPECIFICATIONS.
2. ALL STANDARDS WILL BE CONSTRUCTED IN ACCORDANCE WITH THE FINAL PLANS AND THE CITY OF NORMAN STANDARDS AND SPECIFICATIONS.
3. REFER TO DRAWING PERMIT FOR SPECIFIC DATA.
4. ALL PROPOSED SANITARY LINES ARE 8-INCH DEPTH AS NOTED.
5. ALL PROPOSED INTERFERENCES ARE 8-INCH DEPTH AS NOTED.
6. ALL EXISTING AROUND WORKS WITHIN 10'-0" OF THE LINE AND ALL COMMON AREAS WITHIN THE DEVELOPMENT WILL BE MAINTAINED BY THE PROPOSER.
7. EXISTING DRAINAGE TO BE USED WITH INTERIOR COMMERCIAL, SINGLE FAMILY DWELLING, AND OFFICE BUILDINGS WITH PROPOSED COVERING TO BE MAINTAINED.
8. EXISTING SANITARY SEWAGE AT THE NORTHEAST CORNER OF THE PORTER INTERIOR AND THE STREET INTERSECTION SHALL BE ALLOWED TO REMAIN IN ITS CURRENT LOCATION AS SHOWN ON THIS PLAN.
9. THE EXISTING SANITARY SEWAGE AT THE SOUTHWEST CORNER OF THE PORTER INTERIOR AND THE STREET INTERSECTION SHALL BE ALLOWED TO REMAIN THROUGHOUT AND BEHIND DRIVEWAYS.

[illegible]

LOT	BLOCK	AREA (SQ. FT.)	AREA (ACRES)
1	2	176,827	4.027
2	1	180,251	4.06
3	1	156,986	3.57
4	1	30,247	0.70
5	4	84,785	1.94
6	1	75,345	1.73
7	2	285,866	6.52
8	2	79,438	1.82
9	2	113,869	2.61
10	1	10,332	0.24
11	1	361,721	8.28
TOTAL		1,257,586	28.94

TBA STREET CURVE TABLE						
CURVE #	RADIUS	LENGTH	DELTA	TANGENT	CHORD BEARING	CHORD LENGTH
C1	560.00'	253.23'	109° 31"	74.40	S. 87° 14' 30" E.	253.56
C2	245.00'	262.20'	106° 34' 20"	51.77	S. 84° 00' 00" E.	262.00
C3	473.00'	273.80'	88° 00' 30"	270.80	S. 84° 00' 00" E.	269.70

CHAMBER / DECEMBER 2005

NORMAN REGIONAL HOSPITAL AUTHORITY  
an Oklahoma public trust d/b/a  
NORMAN REGIONAL HEALTH SYSTEM ("NRHS")  
NORMAN, OKLAHOMA

THE CITY OF NORMAN, OKLAHOMA  
is a municipal corporation.

00504850 OK

SMC CONSULTING ENGINEERS P.C.  
815 W. MAIN ST.  
OKLAHOMA CITY OKLAHOMA 73106  
(405) 232-7215

PRELIMINARY PLAT



<p>C. S. MCGOUGH ENGINEERING, P.C. REGISTERED PROFESSIONAL ENGINEER NO. 10,123 FOR THE STATE OF OKLAHOMA PROJECT: PORTER CAMPUS DEVELOPMENT SHEET NO. 1 OF 1</p>	
<p></p>	
<p><b>PRELIMINARY NOT FOR CONSTRUCTION</b></p>	
<p>NORMAN REGIONAL HEALTH SYSTEM - PORTER CAMPUS</p>	<p>PORTER AVENUE &amp; ROBINSON STREET NORMAN, OKLAHOMA</p>
<p><b>SMC</b> S. MCGOUGH ENGINEERING, P.C. REGISTERED PROFESSIONAL ENGINEER NO. 10,123 FOR THE STATE OF OKLAHOMA PROJECT: PORTER CAMPUS DEVELOPMENT SHEET NO. 1 OF 1</p>	
<p>MASTER SITE &amp; UTILITY DEVELOPMENT PLAN</p>	
<p>SHEET NO. 1</p>	

**Applicant:** Norman Regional Hospital/City of Norman

**Project Location:** NRH Porter Campus Site

**Case Number:** PD21-39

**Time:** 6:30 p.m.

**Applicant/Representative**

Shawn Rieger  
Troy Glover  
John Manfred  
Richie Splitt  
Paula Price

**Attendees**

Danny Millsap  
Randall Foster  
Jerry Cos  
Judy Hatfield  
Alex Snoddy  
Mark Cox

**City Staff**

Jane Hudson, Planning Director  
Kathryn Walker, City Attorney  
Jason Olsen, Parks Director

**Application Summary**

The applicant is requesting to rezone the NRH Porter Campus Site to a PUD, Planned Unit Development to allow for the development of a Mixed Use Development.

**Neighbor's Comments/Concerns/Responses**

What is the overall timeline?

Senior Center first, 12 – 13-month build

What is the plan for the hospital growth?

Rebuilding – future growth, there are no plans to vacate the site, we will be keeping the anchor educational facility and building around it.

Will there be a Phasing Plan/timeline?

Senior Center, Behavioral Medicine and simultaneously maybe Variety Care – done by 2023 – 2024.

When the services that are going to move to the West Campus are complete, the demolition of specific areas will begin, we are not demoing the educational facility. Once demo is complete, we will start the rebuild of the campus. The “S” curve street will not be constructed until approximately 75% of the campus site is complete.

The plan is to mimic the Porter Corridor design concept for the reconstruction of the campus.

**NORMAN PLANNING COMMISSION  
REGULAR SESSION MINUTES**

**DECEMBER 9, 2021**

The Planning Commission of the City of Norman, Cleveland County, State of Oklahoma, met in Regular Session in the Council Chambers of the Norman Municipal Building, 201 West Gray Street, on the 9<sup>th</sup> day of December, 2021.

Notice and agenda of the meeting was posted at the Norman Municipal Building and online at <https://norman-ok.municodemeetings.com> at least twenty-four hours prior to the beginning of the meeting.

Chair Erica Bird called the meeting to order at 6:30 p.m.

\* \* \*

**ROLL CALL**

**MEMBERS PRESENT**

Erin Williford (arrived at 6:32 p.m.)  
Kevan Parker  
Steven McDaniel  
Erica Bird  
Dave Boeck  
Sandy Bahan  
Michael Jablonski

**MEMBERS ABSENT**

Lark Zink

A quorum was present.

**STAFF MEMBERS PRESENT**

Jane Hudson, Director, Planning &  
Community Development  
Lora Hoggatt, Planning Services Manager  
Logan Hubble, Planner I  
Anais Starr, Planner II  
Colton Wayman, Planner I  
Roné Tromble, Recording Secretary  
Ken Danner, Subdivision Development  
Manager  
Kathryn Walker, City Attorney  
Heather Poole, Asst. City Attorney  
Jeanne Snider, Asst. City Attorney  
Jami Short, Traffic Engineer  
Bryce Holland, Multimedia Specialist

\* \* \*



Item No. 10, being:

**CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF RESOLUTION NO. R-2122-57 FOR NORMAN REGIONAL HEALTH SYSTEM AND THE CITY OF NORMAN REQUEST AMENDMENT OF THE NORMAN 2025 LAND USE AND TRANSPORTATION PLAN FROM INSTITUTIONAL DESIGNATION TO MIXED USE DESIGNATION FOR APPROXIMATELY 29.3 ACRES OF PROPERTY GENERALLY LOCATED SOUTH OF E. ROBINSON STREET, WEST OF N. FINDLAY AVENUE, NORTH OF E. FRANK STREET, AND EAST OF N. PORTER AVENUE.**

**ITEMS SUBMITTED FOR THE RECORD:**

1. NORMAN 2025 Map
2. Staff Report
3. Pre-Development Summary

Item No. 11, being:

**CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF ORDINANCE NO. O-2122-29 FOR NORMAN REGIONAL HEALTH SYSTEM AND THE CITY OF NORMAN REQUEST REZONING FROM R-1, SINGLE FAMILY DWELLING DISTRICT, C-3, INTENSIVE COMMERCIAL DISTRICT, AND O-1, OFFICE-INSTITUTIONAL DISTRICT, TO PUD, PLANNED UNIT DEVELOPMENT, FOR APPROXIMATELY 29.3 ACRES OF PROPERTY GENERALLY LOCATED SOUTH OF E. ROBINSON STREET, WEST OF N. FINDLAY AVENUE, NORTH OF E. FRANK STREET, AND EAST OF N. PORTER AVENUE.**

**ITEMS SUBMITTED FOR THE RECORD:**

1. Location Map
2. Staff Report
3. PUD Narrative with Exhibits A-D
4. Pre-Development Summary

Item No. 12, being:

**CONSIDERATION OF APPROVAL, ACCEPTANCE, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF PP-2122-9, A PRELIMINARY PLAT SUBMITTED BY NORMAN REGIONAL HEALTH SYSTEM AND THE CITY OF NORMAN (SMC CONSULTING ENGINEERS, P.C.) FOR NORMAN REGIONAL HEALTH SYSTEM PORTER CAMPUS, A PLANNED UNIT DEVELOPMENT FOR APPROXIMATELY 29.3 ACRES OF PROPERTY GENERALLY LOCATED SOUTH OF E. ROBINSON STREET, WEST OF N. FINDLAY AVENUE, NORTH OF E. FRANK STREET, AND EAST OF N. PORTER AVENUE.**

**ITEMS SUBMITTED FOR THE RECORD:**

1. Location Map
2. Preliminary Plat
3. Staff Report
4. Transportation Impacts
5. Preliminary Site Development Plan
6. Master Site & Utility Development Plan
7. Pre-Development Summary

Item No. 13, being:

**CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF ORDINANCE NO. O-2122-30 FOR NORMAN REGIONAL HEALTH SYSTEM AND THE CITY OF NORMAN REQUEST VACATION AND CLOSURE OF CERTAIN PUBLIC INTERESTS IN AN ALLEY, A PORTION OF PONCA AVENUE, AND A PORTION OF GRIFFIN AVENUE.**

**ITEMS SUBMITTED FOR THE RECORD:**

1. Location Map
2. Staff Report
3. Request to Vacate/Close Public Easement

**PRESENTATION BY STAFF:**

1. Ms. Lora Hoggatt presented the staff report. The Parks Board met earlier in the day and recommended fee in lieu to be assigned to the Frances Cate Park, instead of an on-site park.

Commissioner Jablonski asked a question about the traffic study.  
Ms. Jami Short, Traffic Engineer, responded.

**PRESENTATION BY THE APPLICANT:**

Mr. Sean Rieger, representing the applicant, made comments.  
Mr. Richie Splitt, CEO, talked about the Inspire Health program.  
Mr. Sean Rieger explained the mechanics of the site, the zoning and platting and the rights-of-way and easements to be closed and vacated.  
Ms. Kathryn Walker, City Attorney, discussed the Adult Wellness Center.  
Commissioner Boeck made comments.  
Mr. Todd Butler, Traffic Engineering Consultants, discussed traffic issues at Porter and Robinson.  
Commissioner Parker thanked the team for their work.  
Commissioner Bird asked about emergency services at the site.  
Mr. Richie Splitt responded.  
Commissioner Bird asked about the proposed nine lots.  
Mr. Sean Rieger and Mr. Richie Splitt responded.

**AUDIENCE PARTICIPATION:**

Rachelle HardIn-Montz, 2601 132<sup>nd</sup> Avenue S.E., made comments regarding the behavioral health facility.

**DISCUSSION AND ACTION BY THE PLANNING COMMISSION:**

Commissioner Boeck made comments.  
Commissioner Bird made comments.

*Dave Boeck moved to recommend adoption of Resolution No. R-2122-57, Ordinance No. O-2122-29, PP-2122-9, and Ordinance No. O-2122-30 to City Council. Kevan Parker seconded the motion.*

There being no further discussion, a vote on the motion was taken with the following result:

YEAS	Erin Williford, Kevan Parker, Steven McDaniel, Erica Bird, Dave Boeck, Sandy Bahan, Michael Jablonski
NAYES	None
MEMBERS ABSENT	Lark Zink

The motion, to recommend adoption of Resolution No. R-2122-57, Ordinance No. O-2122-29, PP-2122-9, and Ordinance No. O-2122-30 to City Council, passed by a vote of 7-0.

\*\*\*

**File Attachments for Item:**

15. CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF RESOLUTION R-2122-31: A RESOLUTION OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, AMENDING THE NORMAN 2025 LAND USE AND TRANSPORTATION PLAN SO AS TO REMOVE THE NORTH ONE HUNDRED FIFTY (150) FEET OF LOT TWO (2), BLOCK TEN (10), OF PICKARD ACRES ADDITION, TO NORMAN, CLEVELAND COUNTY, OKLAHOMA, FROM THE LOW DENSITY RESIDENTIAL DESIGNATION, AND THE SOUTH ONE HUNDRED FIFTY-TWO AND ONE-HALF (152.5) FEET OF LOT TWO (2), BLOCK TEN (10), OF PICKARD ACRES ADDITION, TO NORMAN, CLEVELAND COUNTY, OKLAHOMA, FROM THE OFFICE DESIGNATION AND PLACE THE SAME IN THE COMMERCIAL DESIGNATION. (1027 AND 1035 SOUTH BERRY ROAD)



## CITY OF NORMAN, OK STAFF REPORT

**MEETING DATE:** 01/25/2021

**REQUESTER:** Sooner Traditions, L.L.C.

**PRESENTER:** Jane Hudson, Director of Planning & Community Development

**ITEM TITLE:** CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF RESOLUTION R-2122-31: A RESOLUTION OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, AMENDING THE NORMAN 2025 LAND USE AND TRANSPORTATION PLAN SO AS TO REMOVE THE NORTH ONE HUNDRED FIFTY (150) FEET OF LOT TWO (2), BLOCK TEN (10), OF PICKARD ACRES ADDITION, TO NORMAN, CLEVELAND COUNTY, OKLAHOMA, FROM THE LOW DENSITY RESIDENTIAL DESIGNATION, AND THE SOUTH ONE HUNDRED FIFTY-TWO AND ONE-HALF (152.5) FEET OF LOT TWO (2), BLOCK TEN (10), OF PICKARD ACRES ADDITION, TO NORMAN, CLEVELAND COUNTY, OKLAHOMA, FROM THE OFFICE DESIGNATION AND PLACE THE SAME IN THE COMMERCIAL DESIGNATION. (1027 AND 1035 SOUTH BERRY ROAD)

### SUMMARY OF REQUEST:

The applicant is proposing development of a commercial shopping center on a 1.33-acre parcel. This development proposal requires rezoning from R-1, Single Family Dwelling District, and CO, Suburban Office Commercial District, because of requested uses and a NORMAN 2025 Land Use and Transportation Plan amendment from Low Density Residential Designation and Office Designation to Commercial Designation.

**STAFF ANALYSIS:** For changes in classification under the NORMAN 2025 Land Use and Transportation Plan, the following information is forwarded for consideration.

The role of the NORMAN 2025 Plan in the City's ongoing and diverse planning activities states the document must be flexible, and that it is updated and amended periodically. The Plan defines the desired land use patterns for use and development of all private sector properties. This Plan will serve as a policy guide for zoning and planning requests as they are presented to the Planning Commission and City Council.

***Has there been a change in circumstances resulting from development of the properties in the general vicinity which suggest that the proposed change will not be contrary to the public interest?***

Public Works worked with Gateway Consulting and SAIC to develop a redesign of the streetscape on West Lindsey Street from 24<sup>th</sup> Ave S.W. to just east of South Berry Road. Construction of the project was completed in 2017. The project was created to help spark redevelopment of West Lindsey Street in this general area.

The lot to the south of the subject property, across West Lindsey Street, 1150 West Lindsey Street, was redeveloped as a restaurant and a lot south of the restaurant was developed as an additional parking area. The lot for the parking area was rezoned from R-1, Single-Family Dwelling District, to CO, Suburban Office Commercial District, with Special Use for a parking lot with Ordinance O-1314-3. The Land Use designation was changed from Low Density Residential to Commercial on the NORMAN 2025 Land Use and Transportation Plan with Resolution R-1314-7.

With the exception of the subject property, the corners of the intersection of West Lindsey Street and South Berry Road are zoned and designated as commercial.

***Is there a determination that the proposed change would not result in adverse land use or adverse traffic impacts to surrounding properties or the vicinity?***

The property is already platted so a Traffic Impact Analysis (TIA) is not required. However, a Traffic Impact Analysis was submitted by the applicant to the City Traffic Engineer for review. The study showed that there will be minimal impact on the traffic in the area. There are two access points proposed for this development, one located on South Berry Rd. and one located on West Lindsey Street.

## **CONCLUSION:**

Staff forwards this request, Resolution R-2122-31, for City Council's consideration.

At their meeting of October 14, 2021, Planning Commission recommended adoption of Resolution R-2122-31 by a vote of 5-1.

# Resolution

R-2122-31

A RESOLUTION OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, AMENDING THE NORMAN 2025 LAND USE AND TRANSPORTATION PLAN SO AS TO REMOVE THE NORTH ONE HUNDRED FIFTY (150) FEET OF LOT TWO (2), BLOCK TEN (10), OF PICKARD ACRES ADDITION, TO NORMAN, CLEVELAND COUNTY, OKLAHOMA, FROM THE LOW DENSITY RESIDENTIAL DESIGNATION, AND THE SOUTH ONE HUNDRED FIFTY-TWO AND ONE-HALF (152.5) FEET OF LOT TWO (2), BLOCK TEN (10), OF PICKARD ACRES ADDITION, TO NORMAN, CLEVELAND COUNTY, OKLAHOMA, FROM THE OFFICE DESIGNATION AND PLACE THE SAME IN THE COMMERCIAL DESIGNATION. (1027 AND 1035 SOUTH BERRY ROAD)

- § 1. WHEREAS, the Council of the City of Norman recognizes citizens' concerns about the future development of Norman; and
- § 2. WHEREAS, the City Council at its meeting of November 16, 2004, reviewed and adopted the NORMAN 2025 Land Use and Transportation Plan, with an effective date of December 16, 2004; and
- § 3. WHEREAS, Sooner Traditions, L.L.C. and Hunter Miller Family, L.L.C. have requested that the following described property be moved from the Low Density Residential Designation and placed in the Commercial Designation for the hereinafter described property, to wit:

The North 150 feet of Lot Two (2), in Block Ten (10), of Pickard Acres Addition, to the City of Norman, Cleveland County, Oklahoma, according to the recorded plat thereof.

- § 4. WHEREAS, Sooner Traditions, L.L.C. and Hunter Miller Family, L.L.C. have requested that the following described property be moved from the Office Designation and placed in the Commercial Designation for the hereinafter described property, to wit:

The South 152.5 feet of Lot Two (2), in Block Ten (10), of Pickard Acres Addition, to the City of Norman, Cleveland County, Oklahoma, according to the recorded plat thereof.



Resolution R-2122-31

Page 2

NOW, THEREFORE BE IT RESOLVED BY THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA:

- § 5. That the Council of the City of Norman recognizes the need to control the future growth of the City of Norman; and, that after due consideration has determined that the requested amendment to the NORMAN 2025 Land Use and Transportation Plan should be adopted, and does hereby approve the requested designation.

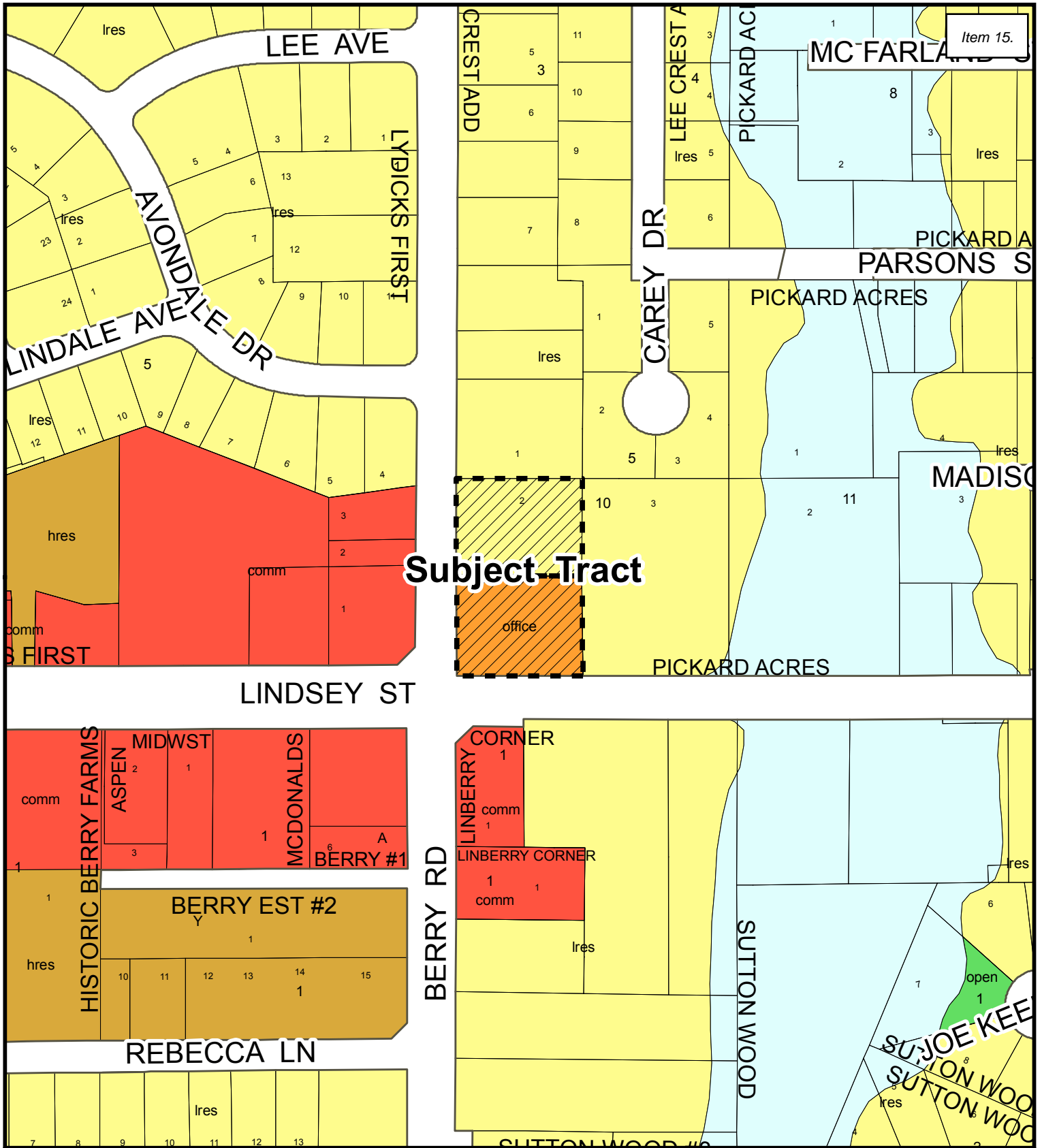
PASSED AND ADOPTED this \_\_\_\_\_ day of \_\_\_\_\_, 2021.

\_\_\_\_\_  
(Mayor)

ATTEST:

\_\_\_\_\_  
(City Clerk)

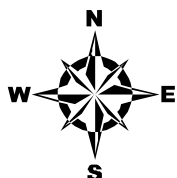




# Norman 2025 Land Use Plan



Map Produced by the City of Norman  
Geographic Information System.  
The City of Norman assumes no  
responsibility for errors or omissions  
in the information presented.



May 5, 2020

0 100 200 Ft.



Subject Tract



Zoning

317

RESOLUTION NO. R-2122-31

ITEM NO. 2

**STAFF REPORT**

**ITEM:** Sooner Traditions, L.L.C. & Hunter Miller Family, L.L.C. request amendment of the NORMAN 2025 Land Use & Transportation Plan from Office Designation and Low Density Residential Designation to Commercial Designation for 1.33 acres of property located at the northeast corner of S. Berry Road and W. Lindsey Street (1027 & 1035 S. Berry Road).

**SUMMARY OF REQUEST:** The applicant is proposing development of a commercial shopping center on a 1.33-acre parcel. This development proposal requires rezoning from R-1, Single Family Dwelling District, and CO, Suburban Office Commercial District, because of requested uses and a NORMAN 2025 Land Use and Transportation Plan amendment from Low Density Residential Designation and Office Designation to Commercial Designation.

**STAFF ANALYSIS:** For changes in classification under the NORMAN 2025 Land Use and Transportation Plan, the following information is forwarded for consideration.

The role of the NORMAN 2025 Plan in the City's ongoing and diverse planning activities states the document must be flexible, and that it is updated and amended periodically. The Plan defines the desired land use patterns for use and development of all private sector properties. This Plan will serve as a policy guide for zoning and planning requests as they are presented to the Planning Commission and City Council.

***Has there been a change in circumstances resulting from development of the properties in the general vicinity which suggest that the proposed change will not be contrary to the public interest?***

Public Works worked with Gateway Consulting and SAIC to develop a redesign of the streetscape on W. Lindsey Street from 24<sup>th</sup> Ave S.W. to just east of S. Berry Rd. Construction of the project was completed in 2017. The project was created to help spark redevelopment of W. Lindsey Street in this general area.

The lot to the south of the subject property, across W. Lindsey Street, 1150 W. Lindsey St., was redeveloped as a restaurant and a lot south of the restaurant was developed as an additional parking area. The lot for the parking area was rezoned from R-1, Single-Family Dwelling District, to CO, Suburban Office Commercial District, with Special Use for a parking lot with Ordinance O-1314-3. The Land Use designation was changed from Low Density Residential to Commercial on the NORMAN 2025 Land Use and Transportation Plan with Resolution R-1314-7.

With the exception of the subject property, the corners of the intersection of W. Lindsey St. and S. Berry Rd. are zoned and designated as commercial.

***Is there a determination that the proposed change would not result in adverse land adverse traffic impacts to surrounding properties or the vicinity?*** The property is already platted so a Traffic Impact Analysis (TIA) is not required. However, a Traffic Impact Analysis was submitted by the applicant to the City Traffic Engineer for review. The study showed that there will be minimal impact on the traffic in the area. There are two access points proposed for this development, one located on S. Berry Rd. and one located on W. Lindsey St.

**CONCLUSION:** Staff forwards this request, Resolution No. R-2122-31, for Planning Commission's consideration.

**Applicant:** Sooner Traditions, LLC

**Project Location:** 1027 & 1035 S. Berry Road

**Case Number:** PD21-23

**Time:** 5:30 p.m.

**Applicant/Representative**

Gunner Joyce, Rieger Law, PLLC

Sean Rieger, Rieger Law, PLLC

**Attendees**

Robert Castleberry

John Cornwell

Michelle Nehrenz

Councilmember Hall

Dennis Yarbrow

Susan Meyer

Councilmember Nash

Stephen Maple

Dana Drury

James Akey

Elizabeth Gohl

Mindy Wood, Kevin Potts

**City Staff**

Brevin Ghoram, Planner I

Jane Hudson, Director, Planning & Community Development

Beth Muckala, Assistant City Attorney

Heather Poole, Assistant City Attorney

Ken Danner, Subdivision Development Manager

**Application Summary**

The applicant seeks to rezone the properties to facilitate the operation of a commercial development on the subject properties. The applicant seeks to rezone to a SPUD, Simple Planned Unit Development. The updated site plan is submitted with this application. The building is brought forward of the lot, 25' setback, increased north setback, created a buffer, lighting ordinance in place, photometric plan, access points away from corner and a single-story structure.

**Neighbor's Comments/Concerns/Responses**

- City denied access to the property on the west side of Berry, why?
  - (City staff responded – we will have to research as we are not familiar with that request.)
- This is the third time the application has been submitted - 2015, 2020 and 2021, why is this allowed to be submitted again?

- (City staff responded - an applicant has the right to submit an application to rezone/develop property. Council ultimately votes on the application.)
- Traffic backs up on Berry Road – this will create more traffic.
- Will there be a traffic study?
  - (Applicant's representative responded - a traffic consultant has been hired.)
- Concern with access on Lindsey or people turning into the site from Lindsey – this will create traffic congestion.
- Berry is a residential street.
- If the zoning is C-1, SPUD, then the uses can be anything.
- Neighbors fear commercial creep into neighborhood.
- Abundance of commercial property on Lindsey so go there – don't buy residential and convert to commercial.
- Will this SPUD be the same uses as previously submitted?
  - (Applicant's representative responded - possibly, not finalized at this point.)

**NORMAN PLANNING COMMISSION  
REGULAR SESSION MINUTES**

**OCTOBER 14, 2021**

The Planning Commission of the City of Norman, Cleveland County, State of Oklahoma, met in Regular Session in the Council Chambers of the Norman Municipal Building, 201 West Gray Street, on the 14<sup>th</sup> day of October, 2021.

Notice and agenda of the meeting was posted at the Norman Municipal Building and online at <https://norman-ok.municodemeetings.com> at least twenty-four hours prior to the beginning of the meeting.

Chair Erica Bird called the meeting to order at 6:30 p.m.

\* \* \*

**ROLL CALL**

**MEMBERS PRESENT**

Erin Williford  
Steven McDaniel  
Erica Bird  
Dave Boeck  
Sandy Bahan  
Michael Jablonski

**MEMBERS ABSENT**

Nouman Jan  
Lark Zink

A quorum was present.

**STAFF MEMBERS PRESENT**

Jane Hudson, Director, Planning &  
Community Development  
Lora Hoggatt, Planning Services Manager  
Roné Tromble, Recording Secretary  
Todd McLellan, Development Engineer  
Heather Poole, Asst. City Attorney  
Jami Short, Traffic Engineer

\* \* \*

Item No. 2, being:

**R-2122-31 – SOONER TRADITIONS, L.L.C. AND HUNTER MILLER FAMILY, L.L.C. REQUEST AMENDMENT OF THE NORMAN 2025 LAND USE AND TRANSPORTATION PLAN FROM OFFICE DESIGNATION AND LOW DENSITY RESIDENTIAL DESIGNATION TO COMMERCIAL DESIGNATION FOR 1.33 ACRES OF PROPERTY LOCATED AT THE NORTHEAST CORNER OF S. BERRY ROAD AND W. LINDSEY STREET (1027 & 1035 S. BERRY ROAD).**

**ITEMS SUBMITTED FOR THE RECORD:**

1. NORMAN 2025 Map
2. Staff Report
3. Pre-Development Summary

and

Item No. 3, being:

**O-2122-15 – SOONER TRADITIONS, L.L.C. AND HUNTER MILLER FAMILY, L.L.C. REQUEST REZONING FROM R-1, SINGLE FAMILY DWELLING DISTRICT, AND CO, SUBURBAN OFFICE COMMERCIAL DISTRICT, TO SPUD, SIMPLE PLANNED UNIT DEVELOPMENT, FOR APPROXIMATELY 1.33 ACRES OF PROPERTY LOCATED AT THE NORTHEAST CORNER OF S. BERRY ROAD AND W. LINDSEY STREET (1027 & 1035 S. BERRY ROAD).**

**ITEMS SUBMITTED FOR THE RECORD:**

1. Location Map
2. Staff Report
3. SPUD Narrative with Exhibits A-D
4. Transportation Impacts
5. Revised Traffic Impact Analysis
6. Pre-Development Summary
7. Protest & Support Map – October 6, 2021
8. Protests & Supports received by October 6, 2021

**PRESENTATION BY STAFF:**

1. Ms. Lora Hoggatt presented the staff report. Protest letters were received on this item, which represented 12.5% of the notification area. Support letters were received which represented 17.8% of the notification area.

**PRESENTATION BY THE APPLICANT:**

1. Mr. Sean Rieger, Rieger Law Group, representing the applicant

**AUDIENCE PARTICIPATION:**

1. Mr. Jim Akey, 819 Carey Drive, spoke in opposition
2. Mr. David Nehrenz, 902 Carey Drive, spoke in opposition
3. Ms. Roxana Meyer, 1202 W. Brooks Street, spoke in opposition
4. Ms. Michelle Nehrenz, 906 Carey Drive, spoke in opposition
5. Ms. Gabrielle Mandeville, 905 Carey Drive, spoke in opposition
6. Mr. Dan Mains, 1128 W. Brooks Street, spoke in opposition
7. Ms. Lara Souza, 823 Carey Drive, spoke in opposition
8. Mr. Dennis Yarbro, 803 S. Berry Road, spoke in opposition
9. Ms. Susan Meyer, 808 Carey Drive, spoke in opposition
10. Mr. Steve Maple, 951 S. Berry Road, spoke in opposition
11. Mr. Kevin Connywerdy, 1132 W. Brooks Street, spoke in opposition
12. Ms. Nolita Morgan, 1027 Madison Street, spoke in opposition

**DISCUSSION AND ACTION BY THE PLANNING COMMISSION:**

1. Chair Erica Bird made comments
2. Commissioner Michael Jablonski made comments
3. Mr. Sean Rieger responded to a question
4. Commissioner Erin Williford made comments



5. Commissioner Dave Boeck made comments
6. Chair Erica Bird made comments

*Michael Jablonski moved to recommend adoption of Resolution No. R-2122-31 and Ordinance No. O-2122-15 to City Council. Erin Williford seconded the motion.*

There being no further discussion, a vote on the motion was taken with the following result:

YEAS	Erin Williford, Steven McDaniel, Erica Bird, Dave Boeck, Michael Jablonski
NAYES	Sandy Bahan
MEMBERS ABSENT	Nouman Jan, Lark Zink

The motion, to recommend adoption of Ordinance No. O-2122-14 and PP-2122-5 to City Council, passed by a vote of 5-1.

\* \* \*

**File Attachments for Item:**

16. CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF ORDINANCE O-2122-15 UPON SECOND AND FINAL READING: AN ORDINANCE OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, AMENDING SECTION 460 OF CHAPTER 22 OF THE CODE OF THE CITY OF NORMAN SO AS TO REMOVE THE NORTH ONE HUNDRED FIFTY (150) FEET OF LOT TWO (2), BLOCK TEN (10), OF PICKARD ACRES ADDITION, TO NORMAN, CLEVELAND COUNTY, OKLAHOMA, FROM THE R-1, SINGLE FAMILY DWELLING DISTRICT, AND PLACE SAME IN THE SPUD, SIMPLE PLANNED UNIT DEVELOPMENT DISTRICT, AND TO REMOVE THE SOUTH ONE HUNDRED FIFTY-TWO AND ONE-HALF (152.5) FEET OF LOT TWO (2), BLOCK TEN (10), OF PICKARD ACRES ADDITION, TO NORMAN, CLEVELAND COUNTY, OKLAHOMA, FROM THE CO, SUBURBAN OFFICE COMMERCIAL DISTRICT, AND PLACE SAME IN THE SPUD, SIMPLE PLANNED UNIT DEVELOPMENT DISTRICT; AND PROVIDING FOR THE SEVERABILITY THEREOF. (1027 AND 1035 SOUTH BERRY ROAD)



## CITY OF NORMAN, OK STAFF REPORT

**MEETING DATE:** 01/25/2022

**REQUESTER:** Sooner Traditions, L.L.C.

**PRESENTER:** Jane Hudson, Director of Planning & Community Development

**ITEM TITLE:** CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF ORDINANCE O-2122-15 UPON SECOND AND FINAL READING: AN ORDINANCE OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, AMENDING SECTION 460 OF CHAPTER 22 OF THE CODE OF THE CITY OF NORMAN SO AS TO REMOVE THE NORTH ONE HUNDRED FIFTY (150) FEET OF LOT TWO (2), BLOCK TEN (10), OF PICKARD ACRES ADDITION, TO NORMAN, CLEVELAND COUNTY, OKLAHOMA, FROM THE R-1, SINGLE FAMILY DWELLING DISTRICT, AND PLACE SAME IN THE SPUD, SIMPLE PLANNED UNIT DEVELOPMENT DISTRICT, AND TO REMOVE THE SOUTH ONE HUNDRED FIFTY-TWO AND ONE-HALF (152.5) FEET OF LOT TWO (2), BLOCK TEN (10), OF PICKARD ACRES ADDITION, TO NORMAN, CLEVELAND COUNTY, OKLAHOMA, FROM THE CO, SUBURBAN OFFICE COMMERCIAL DISTRICT, AND PLACE SAME IN THE SPUD, SIMPLE PLANNED UNIT DEVELOPMENT DISTRICT; AND PROVIDING FOR THE SEVERABILITY THEREOF. (1027 AND 1035 SOUTH BERRY ROAD)

**SYNOPSIS:** The applicant, Sooner Traditions, L.L.C., is requesting to rezone the subject tract from R-1, Single Family Dwelling District (1027 S. Berry Rd.), and CO, Suburban Office Commercial District (1035 S. Berry Rd.), to SPUD, Simple Planned Unit Development District to allow for a commercial shopping center. The site consists of one lot on 1.33 acres. The proposed development will follow a site plan and a SPUD Narrative; see attached. The applicant is requesting a SPUD; the SPUD will establish limitations on building area and height and requirements for tree replacement. Through the SPUD Narrative the applicant is limiting the building height to no more than one story, maximum impervious area for the lot is 65%, and there are replacement guidelines for removal of mature trees.

**HISTORY:** This site was subdivided by warranty deed many years ago; legally this property is one lot with two zonings. The north portion, 1027 S. Berry Rd., has been zoned R-1, Single-Family Dwelling District, since February 22, 1955. The south portion, 1035 S. Berry Rd. was rezoned to CO, Suburban Office Commercial, on July 7, 1981. The City has received applications to rezone/redevelopment this property previously, however, nothing has been approved to date.

## **ZONING ORDINANCE CITATION:**

### SEC 420.05 – SIMPLE PLANNED UNIT DEVELOPMENTS

**General Description.** The Simple Planned Unit Development referred to as SPUD, is a special zoning district that provides an alternate approach to the conventional land use controls and to maximize the unique physical features of a particular site and produce unique, creative, progressive, or quality land developments.

The SPUD may be used for particular tracts or parcels of land that are to be developed, according to a SPUD Narrative and a Development Plan Map and contains less than five (5) acres.

The SPUD is subject to review procedures by Planning Commission and adoption by City Council.

**Statement of Purpose.** It is the intent of this section to encourage developments with a superior built environment brought about through unified development and to provide for the application of design ingenuity in such developments while protecting existing and future surrounding areas in achieving the goals of comprehensive plan of record. In addition, the SPUD provides for the following:

Encourage efficient, innovative use of land in the placement and/or clustering of buildings in a development and protect the health, safety and welfare of the community.

Contribute to the revitalization and/or redevelopment of areas where decline of any type has occurred. Promote infill development that is compatible and harmonious with adjacent uses and would otherwise not be an area that could physically be redeveloped under conventional zoning.

Maintain consistency with the City's Zoning Ordinance, and other applicable plans, policies, standards and regulations on record.

Approval of a zone change to a SPUD adopts the Master Plan prepared by the applicant and reviewed as a part of the application. The SPUD establishes new and specific requirements for the amount and type of land use, residential densities, if appropriate, development regulations and location of specific elements of the development, such as open space and screening.

**EXISTING ZONING:** The subject property currently has two zoning designations. The north portion, 1027 S. Berry Rd. is zoned R-1, Single Family Dwelling District. This district allows mainly for single family dwelling units with some accessory uses. The south portion, 1035 S. Berry Rd., is zoned CO, Suburban Office Commercial District. This district allows for institutional and commercial activities that require separate buildings surrounded by landscaped yards and open area near residential neighborhoods. Neither of these zoning districts allow for retail sales or restaurant uses.

**ANALYSIS:** The particulars of this SPUD include:

**USE:** The property may be utilized for any of the uses listed in Exhibit B. The applicant intends to build a commercial retail center on the property.

**OPEN SPACE/LANDSCAPING:** The SPUD Narrative states the impervious area for the property will not exceed 65% of the lot, which is the maximum allowed for residential developments. The City's commercial zoning districts do not have maximum coverage allowance. The applicant is proposing a minimum 25' landscape buffer along the south and west property lines, a minimum 20' landscape buffer along the north property line and a minimum 16' landscape buffer along the east property line. The applicant is also proposing to use low impact development techniques (LIDs) and best management practices (BMPs) to develop the site. These are shown on the Stormwater Enhancement Diagram, Exhibit D. The SPUD Narrative discusses tree preservation and states the applicant will plant two trees, minimum two-inch caliper, should any one mature tree need to be removed.

**PARKING:** The submitted site plan shows 55 parking spaces. With the recently proposed change in the parking requirements from "Required" to "Recommended," there may be no parking requirements for this site. If the proposed change is adopted by City Council, the site should still provide adequate parking for the proposed facility. The site plan submitted shows the location of the bike racks; they are planned to be installed over impervious pavers.

**SITE PLAN/ACCESS:** The applicant is proposing a building to be situated on the southwest corner of the property. The building is proposed at a 25' setback from the west and south property lines and a 90' setback from the north property line. The east side of the building will follow a 45' setback. As shown on the proposed site plan, the building is located in excess of 90' from the east property line. The parking for the development is along the north and east sides of the property. The dumpster is located on the southeast side of the property, just west of the proposed access point off W. Lindsey St. There are two proposed access points – one off W. Lindsey St. on the southeast side of the property and one off S. Berry Rd. on the northwest side of the property. The W. Lindsey St. access point will be a right-in, right-out, as shown on the site plan. A 5' sidewalk is required along S. Berry Rd. There is an existing sidewalk along W. Lindsey St.

**SIGNAGE:** The SPUD Narrative states that all signage will be in conformance with the City's Sign Code as is applicable for district regulations of commercial zoning.

**FENCING:** The SPUD Narrative states there will be a 6' wood privacy fence along the northern and eastern boundaries of the property.

**HEIGHT:** The SPUD Narrative states the height of the proposed building is restricted to one story and will have a pitched roof to conceal all mechanical equipment.

**LIGHTING:** All exterior lighting will conform to the City of Norman Commercial Outdoor Lighting Standards, which require full cut-off fixtures and any light poles installed within 25' of a residential

property line or public right-of-way may not exceed 20' in height. The Commercial Outdoor Lighting Standards state, all applications for building permits subject to this section which exceed 7,500 square feet of new construction in an Office category, or 5,000 square feet of new construction for all other categories of use within this section, and involve the installation of new outdoor lighting shall include a photometric plan with estimated footcandles at ten feet beyond all property boundaries that are adjacent or across the street from single or two-family residential zones or uses.

**OTHER AGENCY COMMENTS:**

**PREDEVELOPMENT: PD21-23, June 24, 2021**

Neighbors fear this development will create commercial creep into the existing neighborhoods. They are also concerned about increased traffic. Please see the attached Predevelopment summary for all concerns and comments.

**PUBLIC WORKS:** The property is platted and all rights-of-way and easements have been dedicated. All public improvements are installed and accepted. Stormwater will continue to sheet flow to the south and west across the site as it has historically. Low Impact Development Techniques such as bio-retention swales, rain gardens, flow through planters, and tree wells will be utilized to increase storm water infiltration and water quality.

**TRAFFIC:** Please see the Traffic Engineer's analysis of this proposal in the agenda packet.

**CONCLUSION:** Staff forwards this request and Ordinance O-2122-15 for the City Council's consideration.

At their meeting of October 14, 2021, Planning Commission recommended adoption of Ordinance O-2122-15 by a vote of 5-1.

O-2122-15

AN ORDINANCE OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, AMENDING SECTION 460 OF CHAPTER 22 OF THE CODE OF THE CITY OF NORMAN SO AS TO REMOVE THE NORTH ONE HUNDRED FIFTY (150) FEET OF LOT TWO (2), BLOCK TEN (10), OF PICKARD ACRES ADDITION, TO NORMAN, CLEVELAND COUNTY, OKLAHOMA, FROM THE R-1, SINGLE FAMILY DWELLING DISTRICT, AND PLACE SAME IN THE SPUD, SIMPLE PLANNED UNIT DEVELOPMENT DISTRICT, AND TO REMOVE THE SOUTH ONE HUNDRED FIFTY-TWO AND ONE-HALF (152.5) FEET OF LOT TWO (2), BLOCK TEN (10), OF PICKARD ACRES ADDITION, TO NORMAN, CLEVELAND COUNTY, OKLAHOMA, FROM THE CO, SUBURBAN OFFICE COMMERCIAL DISTRICT, AND PLACE SAME IN THE SPUD, SIMPLE PLANNED UNIT DEVELOPMENT DISTRICT; AND PROVIDING FOR THE SEVERABILITY THEREOF. (1027 and 1035 South Berry Road)

- § 1. WHEREAS, Sooner Traditions, L.L.C. and Hunter Miller Family, L.L.C., the owners of the hereinafter described property, have made application to have the subject property removed from the R-1, Single Family Dwelling District and the CO, Suburban Office Commercial District, and placed in the SPUD, Simple Planned Unit Development District; and
- § 2. WHEREAS, said application has been referred to the Planning Commission of said City and said body has, after conducting a public hearing as required by law, considered the same and recommended that the same should be granted and an ordinance adopted to effect and accomplish such rezoning; and
- § 3. WHEREAS, the City Council of the City of Norman, Oklahoma, has thereafter considered said application and has determined that said application should be granted and an ordinance adopted to effect and accomplish such rezoning.

NOW, THEREFORE, BE IT ORDAINED BY THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA:

- § 4. That Section 460 of Chapter 22 of the Code of the City of Norman, Oklahoma, is hereby amended so as to remove the following described property from the R-1, Single Family Dwelling District and place the same in the SPUD, Simple Planned Unit Development District, to wit:



Ordinance No. O-2122-15

Page 2

The North 150 feet of Lot Two (2), in Block Ten (10), of Pickard Acres Addition, to the City of Norman, Cleveland County, Oklahoma, according to the recorded plat thereof.

- § 5. That Section 460 of Chapter 22 of the Code of the City of Norman, Oklahoma, is hereby amended so as to remove the following described property from the CO, Suburban Office Commercial District and place the same in the SPUD, Simple Planned Unit Development District, to wit:

The South 152.5 feet of Lot Two (2), in Block Ten (10), of Pickard Acres Addition, to the City of Norman, Cleveland County, Oklahoma, according to the recorded plat thereof.

- § 6. Further, pursuant to the provisions of Section 22:420.05 of the Code of the City of Norman, as amended, the following condition is hereby attached to the zoning of the tract:

- a. The site shall be developed in accordance with the SPUD Narrative and the Site Development Plan, approved by the Planning Commission on October 14, 2021, and supporting documentation submitted by the applicant and approved by the Planning Commission, and made a part hereof.

- § 7. Severability. If any section, subsection, sentence, clause, phrase, or portion of this ordinance is, for any reason, held invalid or unconstitutional by any court of competent jurisdiction, such portion shall be deemed a separate, distinct, and independent provision, and such holding shall not affect the validity of the remaining portions of this ordinance.

ADOPTED this \_\_\_\_\_ day of \_\_\_\_\_, 2021.

NOT ADOPTED this \_\_\_\_\_ day of \_\_\_\_\_, 2021.

\_\_\_\_\_  
(Mayor)

\_\_\_\_\_  
(Mayor)

ATTEST:

\_\_\_\_\_  
(City Clerk)

**1027 AND 1035 S. BERRY ROAD  
NORMAN OK**

**SIMPLE PLANNED UNIT DEVELOPMENT**

APPLICANT:

***SOONER TRADITIONS L.L.C.***

APPLICATION FOR:

SIMPLE PLANNED UNIT DEVELOPMENT

Submitted August 2, 2021  
Revised November 2, 2021

PREPARED BY:

**RIEGER LAW GROUP PLLC**  
136 Thompson Drive  
Norman, Oklahoma 73069

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## I. **INTRODUCTION**

This Simple Planned Unit Development (the “**SPUD**”) is being submitted for the property located at the Northeast corner of the South Berry Road and West Lindsey Street intersection, which is commonly known as 1027 and 1035 S. Berry Road, Norman, OK (the “**Property**”). The Property consists of one platted lot that was subdivided through different zoning requests over time. This request seeks to return the zoning classification and use of the Property back to a one single platted lot. Currently, the south half of the Property is zoned CO, Suburban Office Commercial District, and the north half is zoned R-1, Single-Family Dwelling District. However, the properties on all three other corners of the Lindsey Street and Berry Road intersection are all NORMAN 2025 Planned Commercial, and zoned C-1 or C-2 Commercial districts. The Applicant seeks to develop a small-scale commercial center that incorporates C-1 Local Commercial District allowable uses as more particularly detailed on **Exhibit B**. In keeping with the intent and spirit of the City of Norman’s C-1 Zoning Ordinance, this request is intended to provide zoning for the conduct of uses that meet the regular needs and for the convenience of the people of adjacent residential areas.

## II. **PROPERTY DESCRIPTIONS; EXISTING CONDITIONS**

### A. **Location**

The Property is located at the Northeast corner of the South Berry Road and West Lindsey Street intersection, which is commonly known as 1027 and 1035 S. Berry Road, Norman, Oklahoma. The Berry Road and Lindsey Street intersection is a section line intersection featuring a combined daily traffic count (per ACOG) of almost 27,000 vehicles per day.

### B. **Existing Land Use and Zoning**

The south half of the Property is currently zoned CO, Suburban Office Commercial District and the north half of the Property is zoned R-1, Single-Family Dwelling District. The south half of the Property is currently designated on NORMAN 2025 as Office and the north half of the Property is designated Low Density Residential. As noted above, all other corners of the Lindsey Street and Berry Road intersection are NORMAN 2025 Planned Commercial, and zoned C-1, Local Commercial or C-2, General Commercial.

### C. **Elevation and Topography; Drainage**

The Property is developed with two older residential structures and gently slopes southwest. No portion of the Property is in the FEMA 100-year flood plain or the WQPZ. A drainage report has been conducted for the Property and provided to City Staff for review.

### D. **Utility Services**

The necessary utility services for this project are already located on or near the Property as this is an already developed location.

**E. Fire Protection Services**

Fire protection services are as provided by the City of Norman Fire Department and per the City of Norman regulations for such. If required, fire hydrants will be installed on or near the Property in compliance with the applicable provisions of Norman's fire and building codes.

**F. Traffic Circulation and Access**

There are currently three access points on South Berry Road for the Property.

**G. Fencing**

The Property currently has no boundary fencing.

**III. DEVELOPMENT PLAN AND DESIGN CONCEPT**

The Property shall be developed as depicted on the Site Development Plan, attached hereto as **Exhibit A**. The exhibits attached hereto are incorporated herein by reference. The Property shall be developed in conformance with the Site Development Plan, subject to final design development and the changes allowed by Section 22.420.05(11) of the City of Norman's SPUD Ordinance, as may be amended from time to time.

**A. Uses Permitted**

It is the intent of this SPUD to allow the Property to incorporate in, and provide for, the allowable uses provided under the City of Norman's C-1, Local Commercial District. An exhaustive list of the allowable uses for the Property is attached hereto as **EXHIBIT B**.

**B. Area Regulations**

North Setback: There shall be a ninety (90') foot building setback from the North property line.

South Setback: There shall be a twenty-five (25') foot building setback from the South property line.

East Setback: There shall be a forty-five (45') foot building setback from the East property line.

West Setback. There shall be a twenty-five (25') foot building setback from the West property line.

The building setbacks enumerated herein shall be applicable to vertical habitable or occupiable structures.

**C. Open space and green space**

The development of the Property will feature open space and green space areas, as illustrated on the Open Space Diagram, attached hereto as **Exhibit C**. The impervious area for the Property shall not exceed 65%. There shall be a minimum twenty-five (25') foot landscape buffer along the South and West property lines, a minimum twenty (20') foot landscape buffer along the North property line, and a minimum sixteen (16') foot landscape buffer along the East property line. Notwithstanding the foregoing, minor improvements, such as, but not limited to, the patio area, pedestrian bike access path, architectural benches and features, and/or a paved walkway around the building, may encroach within the landscape buffers to the South and West of the building. Additionally, the Applicant will utilize low impact development techniques ("LIDs") and best management practices ("BMPs") in the development of the Property. Examples of potential LIDs and BMPs that may be utilized on the Property are indicated on the Stormwater Enhancement Diagram, attached hereto as **Exhibit D**. The locations and types of LIDs and BMPs are subject to modification during final site development.

**D. Traffic access/circulation/sidewalks**

There are currently three access points on South Berry Road for the Property, which as noted above is currently being used as if it were two separate lots. The Applicant proposes removing two of those access points, relocating one of them farther north, and providing a new access point on Lindsey Street. New sidewalks would be installed along Berry Road frontage. The access point from Lindsey Street shall be restricted access of right in, right out only. The Applicant has provided a Traffic Impact Analysis discussing the proposed development to City Staff.

**E. Landscaping/Tree Preservation**

Trees shall be preserved by the Applicant, when possible, to fit the proposed site plan. If the Applicant needs to remove a mature tree in order to facilitate the development of the Property per the site plan, the Applicant shall plant two (2) trees, minimum two-inch caliper, on the Property, in a location to be determined by the Applicant. Additionally, no trees located within the public right of ways, will be removed by the Applicant unless one or more of the following circumstances apply: (i) a tree needs to be removed or altered in order to comply with Norman's City Ordinances, as amended from time to time; (ii) a tree(s) needs to be removed to allow for installation of the new sidewalk and entry drive approaches, utility installation, bus stop changes, or other infrastructure modifications; or (iii) a tree is

a dangerous tree or diseased or infected tree as defined in Article X of Chapter 10 of the City of Norman's Code of Ordinances, as amended from time to time.

**F. Signage**

Signage will comply with the City of Norman's commercial signage ordinances applicable to Norman's C-1, Local Commercial District, as may be amended from time to time.

**G. Lighting**

The Applicant shall meet the City of Norman's Commercial Outdoor Lighting Standards, as amended from time to time.

**H. Fencing**

The Property currently has no boundary fencing. The Property will feature a six (6') foot wood privacy boundary fence along the North and East boundaries of the Property, provided that the fence may commence at the South setback line on the East boundary of the Property.

**I. Height**

Height shall be restricted to no more than one (1) story for all building structures.

**J. Parking**

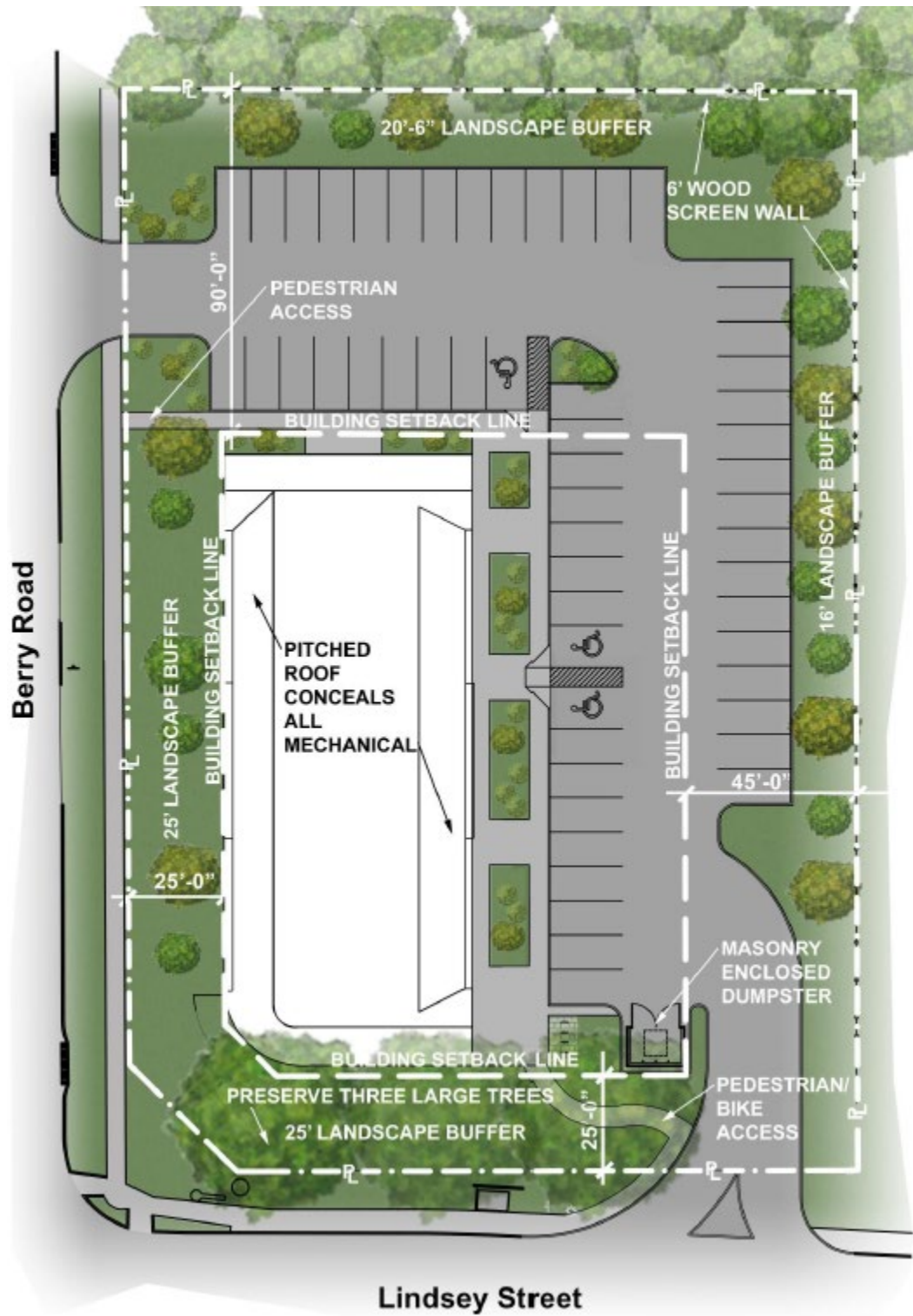
The Property shall comply with Norman's applicable parking ordinances, as amended from time to time. Additionally, bike racks will be provided on the Property to encourage and support multi-modal transportation to and from the development.

**K. Exterior Materials**

The exterior materials of the building to be constructed on the Property may be brick, glass, stone, synthetic stone, stucco, EIFS, masonry, metal accents, composition shingles, and any combination thereof.



**EXHIBIT A**  
Site Development Plan



## **EXHIBIT B**

### Allowable Uses

#### **Commercial Uses:**

*(for purposes of familiarity, this list is based on the City of Norman's C-1, Local Commercial District. In the event of any ambiguity or conflict between the City of Norman's C-1, Local Commercial District Ordinance and this list, this list shall control)*

- Apartment Hotel.
- Antique shop.
- Appliance Store.
- Art Gallery.
- Artist materials supply or studio.
- Assembly Halls of non-profit corporations.
- Automobile parking lots.
- Automobile supply store.
- Baby store.
- Bakery goods store.
- Bank.
- Barber shop, or beauty parlor.
- Book or stationery store.
- Camera shop.
- Candy store.
- Catering establishment.
- Child care establishment.
- Churches.
- Clothing or apparel store.
- Dairy products or ice cream store.
- Delicatessen store.
- Dress shop.
- Drug store or fountain.
- Dry Cleaning and/or Laundry Plant with no more than three (3) dry cleaning machines and/or Laundry Pick-up Station.
- Dry goods store.
- Fabric or notion store.
- Florist.
- Furniture Store.
- Gift Shop.
- Grocery or supermarket.
- Hardware store.
- Hotel or motel.
- Interior decorating store.
- Jewelry shop.

- Key shop.
- Leathergoods shop.
- Libraries.
- Medical Marijuana Dispensary, as allowed by state law.
- Museums.
- Music Conservatories.
- Messenger or telegraph service.
- Office business.
- Painting and decorating shop.
- Pet shop.
- Pharmacy.
- Photographer's studio.
- Radio and television sales and service.
- Restaurant. A restaurant may include live entertainment and/or a dance floor, (all such activity fully within an enclosed building) provided the kitchen remains open with full food service whenever live entertainment is offered.
- Retail spirits store.
- Self service laundry.
- Sewing machine sales.
- Sporting goods sales.
- Shoe store or repair shop.
- Tailor Shop.
- Trade schools and schools for vocational training.
- Theaters (excluding drive-in theaters), including one that sells alcoholic beverages in compliance with state law.
- Tier I Medical Marijuana Processor, as allowed by state law.
- Tier II Medical Marijuana Processor, as allowed by state law.
- Toy store.

**EXHIBIT C**

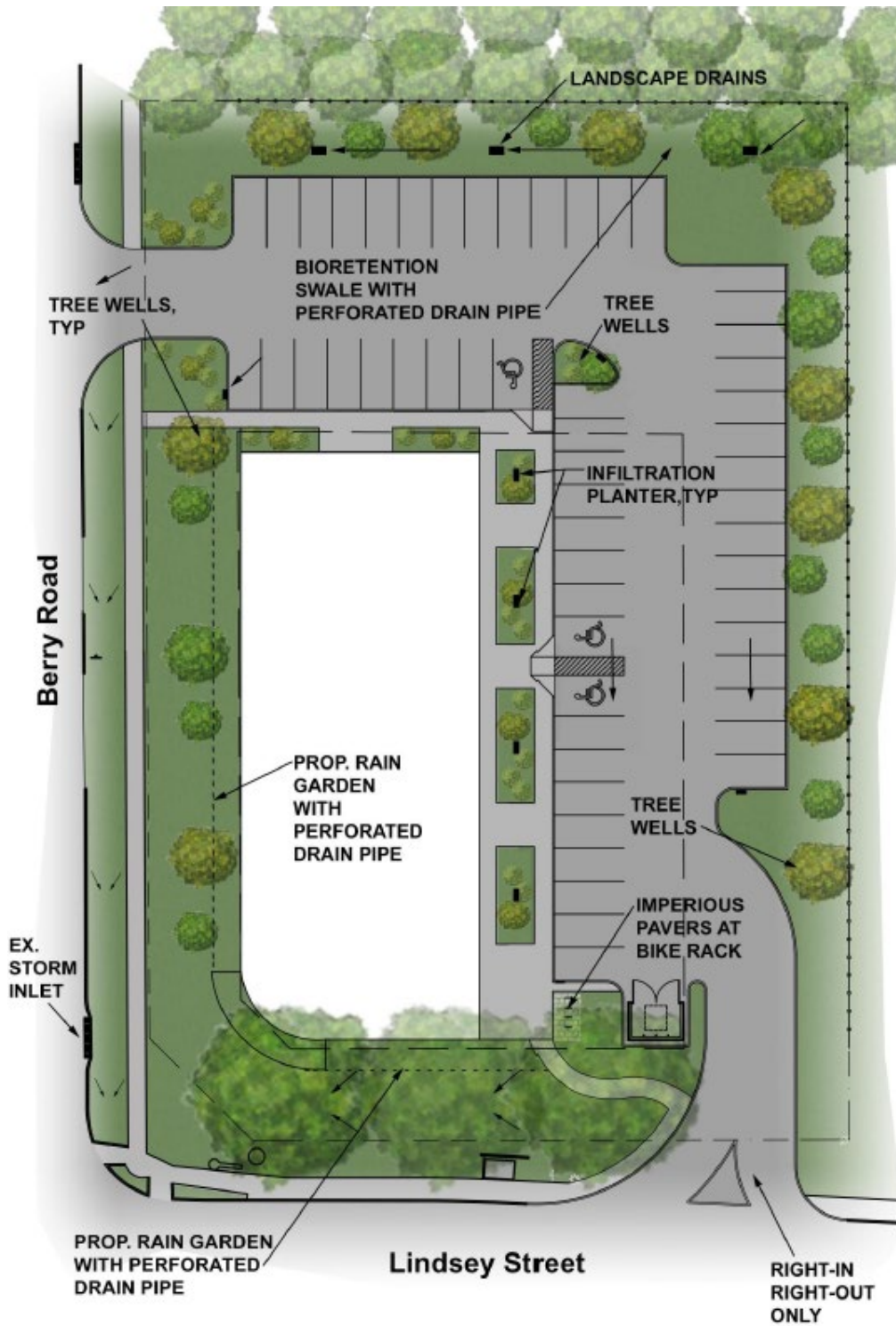
**OPEN SPACE DIAGRAM**



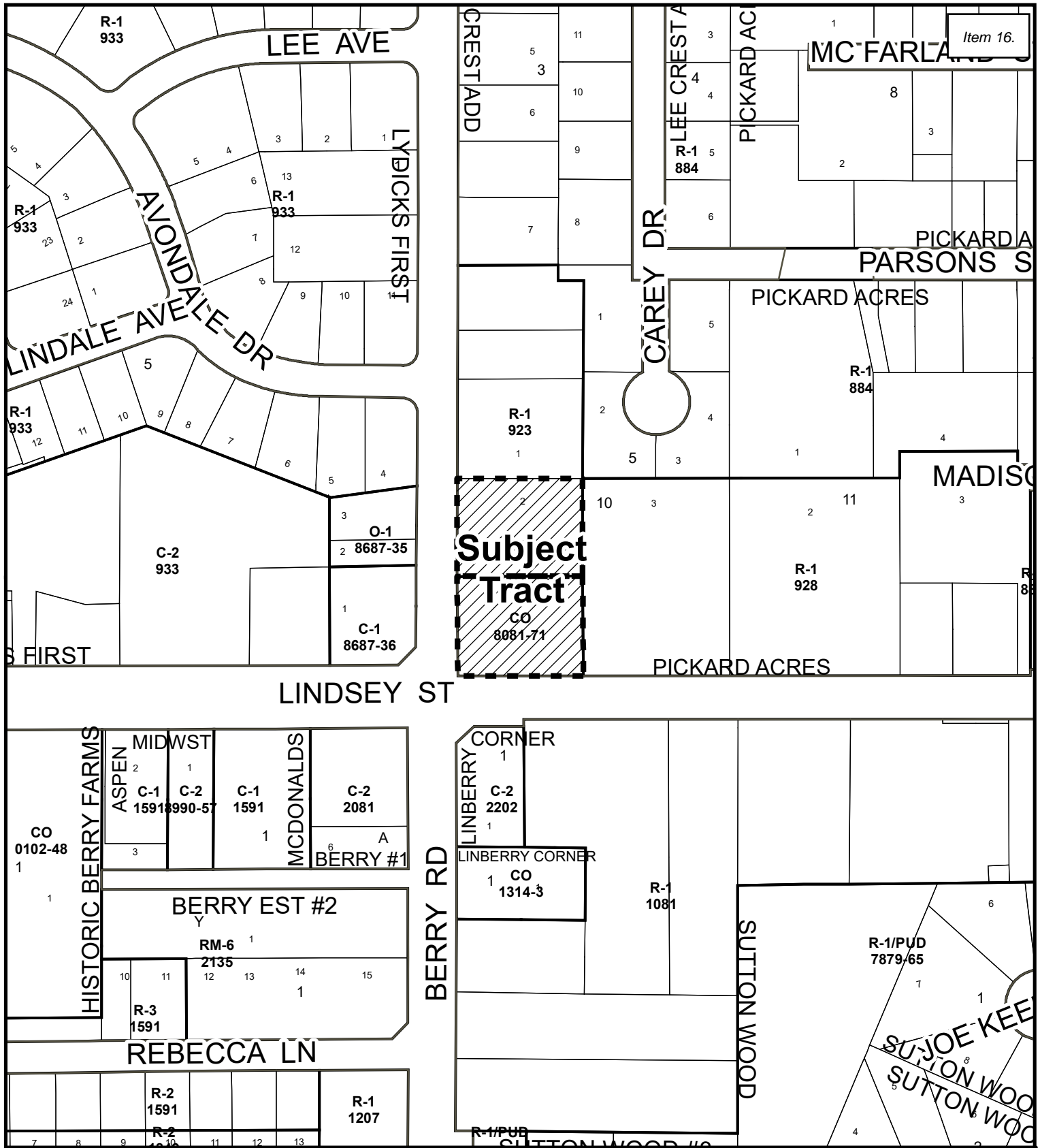
**Open Space Diagram**

## EXHIBIT D

### STORMWATER ENHANCEMENT DIAGRAM



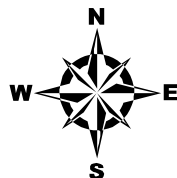
**Stormwater Enhancements**



# Location Map



Map Produced by the City of Norman  
Geographic Information System.  
The City of Norman assumes no  
responsibility for errors or omissions  
in the information presented.



June 8, 2021

0 100 200 Ft.



Subject Tract



Zoning

343



ORDINANCE NO. O-2122-15

ITEM NO. 3

**STAFF REPORT****GENERAL INFORMATION**

APPLICANT	Sooner Traditions, L.L.C. and Hunter Miller Family, L.L.C.
REQUESTED ACTION	Rezoning to SPUD, Simple Planned Unit Development
EXISTING ZONING	R-1, Single Family Dwelling District, and CO, Suburban Office Commercial District
SURROUNDING ZONING	North: R-1, Single Family Dwelling District East: R-1, Single Family Dwelling District South: C-2, General Commercial District and R-1, Single Family Dwelling District West: O-1, Office Institutional District and C-1, Local Commercial District
LOCATION	1027 & 1035 South Berry Road
SIZE	1.33 acres, more or less
PURPOSE	Commercial Retail Center
EXISTING LAND USE	Vacant Residential and Office
SURROUNDING LAND USE	North: Residential East: Vacant South: Commercial and Residential West: Commercial

**SYNOPSIS:** The applicant, Sooner Traditions, L.L.C. is requesting to rezone the subject tract from R-1, Single Family Dwelling District (1027 S. Berry Rd.), and CO, Suburban Office Commercial District (1035 S. Berry Rd.), to SPUD, Simple Planned Unit Development District to allow for a commercial shopping center. The site consists of one lot on 1.33 acres. The proposed development will follow a site plan and a SPUD Narrative; see attached. The applicant is requesting a SPUD; the SPUD will establish limitations on building height and



requirements for tree replacement. The applicant is limiting the building height to no more than one story and establishing replacement guidelines for removal of mature trees.

**HISTORY:** This site was subdivided by warranty deed many years ago; legally this area is one lot with two zonings. The north portion, 1027 S. Berry Rd., has been zoned R-1, Single-Family Dwelling District, since February 22, 1955. The south portion, 1035 S. Berry Rd. was rezoned to CO, Suburban Office Commercial, on July 7, 1981. The City has received applications to rezone/redevelopment this property previously, however, nothing has been approved to date.

**ZONING ORDINANCE CITATION:** SEC 420.05 – SIMPLE PLANNED UNIT DEVELOPMENTS

**General Description.** The Simple Planned Unit Development referred to as SPUD, is a special zoning district that provides an alternate approach to the conventional land use controls and to a PUD, Planned Unit Development to maximize the unique physical features of a particular site and produce unique, creative, progressive, or quality land developments.

The SPUD may be used for particular tracts or parcels of land that are to be developed, according to a SPUD Narrative and a Development Plan Map and contains less than five (5) acres.

The SPUD is subject to review procedures by Planning Commission and adoption by City Council.

**Statement of Purpose.** It is the intent of this section to encourage developments with a superior built environment brought about through unified development and to provide for the application of design ingenuity in such developments while protecting existing and future surrounding areas in achieving the goals of comprehensive plan of record. In addition the SPUD provides for the following:

Encourage efficient, innovative use of land in the placement and/or clustering of buildings in a development and protect the health, safety and welfare of the community.

Contribute to the revitalization and/or redevelopment of areas where decline of any type has occurred. Promote infill development that is compatible and harmonious with adjacent uses and would otherwise not be an area that could physically be redeveloped under conventional zoning.

Maintain consistency with the City's Zoning Ordinance, and other applicable plans, policies, standards and regulations on record.

Approval of a zone change to a SPUD adopts the Master Plan prepared by the applicant and reviewed as a part of the application. The SPUD establishes new and specific requirements for the amount and type of land use, residential densities, if appropriate, development regulations and location of specific elements of the development, such as open space and screening.

**EXISTING ZONING:** The subject property currently has two zoning designations. The north portion, 1027 S. Berry Rd. is zoned R-1, Single Family Dwelling District. This district allows mainly for single family dwelling units with some accessory uses. The south portion, 1035 S. Berry Rd., is zoned CO, Suburban Office Commercial District. This district allows for institutional and commercial activities that require separate buildings surrounded by landscaped yards and

open area near residential neighborhoods. Neither of these zoning districts allow for retail or restaurant uses.

**ANALYSIS:** The particulars of this SPUD include:

**USE:** The property may be utilized for any of the uses listed in Exhibit B. The applicant intends to build a commercial retail center on the property.

**OPEN SPACE/LANDSCAPING:** The SPUD Narrative states the impervious area for the property will not exceed 65% of the lot, which is the maximum allowed for residential developments. The City's commercial zoning districts do not have maximum coverage allowance. The applicant is proposing a 10' landscape buffer along the south, east, and west property lines and a 20' landscape buffer along the north property line. The applicant is also proposing to use low impact development techniques (LIDs) and best management practices (BMPs) to develop the site. These are shown on the Stormwater Enhancement Diagram, Exhibit D. The SPUD Narrative discusses tree preservation and states the applicant will plant two trees, minimum two-inch caliper, should any one mature tree need to be removed.

**PARKING:** The submitted site plan shows 55 parking spaces. With the recently proposed change in the parking requirements from "Required" to "Recommended," there may be no parking requirements for this site. If the proposed change is adopted by City Council, the site should provide adequate parking for the proposed facility.

**SITE PLAN/ACCESS:** The applicant is proposing a building to be situated on the southwest corner of the property. The building is proposed at a 25' setback from the west and south property lines and a 90' setback from the north property line. The east side of the building will follow a 45' setback. The parking for the development is along the north and east sides of the property. The dumpster is located on the southeast side of the property, just west of the proposed access point off W. Lindsey St. There are two proposed access points – one off W. Lindsey St. on the southeast side of the property and one off S. Berry Rd. on the northwest side of the property. The W. Lindsey St. access point will be a right-in, right-out, as shown on the site plan. A 5' sidewalk is required along S. Berry Rd. There is an existing sidewalk along W. Lindsey St.

**SIGNAGE:** The SPUD Narrative states that all signage will be in conformance with the City's Sign Code as is applicable for district regulations of commercial zoning.

**FENCING:** The SPUD Narrative states there will be a 6' wood privacy fence along the northern and eastern boundaries of the property.

**HEIGHT:** The SPUD Narrative states the height of the proposed building is restricted to one story.

**LIGHTING:** All exterior lighting will conform to the City of Norman Commercial Outdoor Lighting Standards, which require full cut-off fixtures and any light poles installed within 25' of a residential property line or public right-of-way may not exceed 20' in height. The Commercial Outdoor Lighting Standards state, all applications for building permits subject to this section which exceed 7,500 square feet of new construction in an Office category, or 5,000 square feet of new construction for all other categories of use within this section, and involve the installation of new outdoor lighting shall include a photometric plan with estimated footcandles at ten feet beyond all property boundaries that are adjacent or across the street from single or two-family residential zones or uses.

**OTHER AGENCY COMMENTS:****PREDEVELOPMENT: PD21-23, June 24, 2021**

Neighbors fear this development will create commercial creep into the existing neighborhoods. They are also concerned about increased traffic. Please see the attached Predevelopment summary for all concerns and comments.

**PUBLIC WORKS:** The property is platted and all rights-of-way and easements have been dedicated. All public improvements are installed and accepted. Stormwater will continue to sheet flow to the south and west across the site as it has historically. Low Impact Development Techniques such as bioretention swales, rain gardens, flow through planters, and tree wells will be utilized to increase storm water infiltration and water quality.

**TRAFFIC:** Please see the Traffic Engineer's analysis of this proposal in the agenda packet.

**CONCLUSION:** Staff forwards this request and Ordinance No. O-2122-15 for the Planning Commission's consideration.

**Applicant:** Sooner Traditions, LLC

**Project Location:** 1027 & 1035 S. Berry Road

**Case Number:** PD21-23

**Time:** 5:30 p.m.

**Applicant/Representative**

Gunner Joyce, Rieger Law, PLLC

Sean Rieger, Rieger Law, PLLC

**Attendees**

Robert Castleberry

John Cornwell

Michelle Nehrenz

Councilmember Hall

Dennis Yarbrow

Susan Meyer

Councilmember Nash

Stephen Maple

Dana Drury

James Akey

Elizabeth Gohl

Mindy Wood, Kevin Potts

**City Staff**

Brevin Ghoram, Planner I

Jane Hudson, Director, Planning & Community Development

Beth Muckala, Assistant City Attorney

Heather Poole, Assistant City Attorney

Ken Danner, Subdivision Development Manager

**Application Summary**

The applicant seeks to rezone the properties to facilitate the operation of a commercial development on the subject properties. The applicant seeks to rezone to a SPUD, Simple Planned Unit Development. The updated site plan is submitted with this application. The building is brought forward of the lot, 25' setback, increased north setback, created a buffer, lighting ordinance in place, photometric plan, access points away from corner and a single-story structure.

**Neighbor's Comments/Concerns/Responses**

- City denied access to the property on the west side of Berry, why?
  - (City staff responded – we will have to research as we are not familiar with that request.)
- This is the third time the application has been submitted - 2015, 2020 and 2021, why is this allowed to be submitted again?

- (City staff responded - an applicant has the right to submit an application to rezone/develop property. Council ultimately votes on the application.)
- Traffic backs up on Berry Road – this will create more traffic.
- Will there be a traffic study?
  - (Applicant's representative responded - a traffic consultant has been hired.)
- Concern with access on Lindsey or people turning into the site from Lindsey – this will create traffic congestion.
- Berry is a residential street.
- If the zoning is C-1, SPUD, then the uses can be anything.
- Neighbors fear commercial creep into neighborhood.
- Abundance of commercial property on Lindsey so go there – don't buy residential and convert to commercial.
- Will this SPUD be the same uses as previously submitted?
  - (Applicant's representative responded - possibly, not finalized at this point.)



# CITY OF NORMAN

## Development Review Form

### Transportation Impacts

**DATE:** October 5, 2021

**CONDUCTED BY:** Jami L. Short, P.E.  
City Traffic Engineer

**PROJECT NAME:** Sooner Traditions SPUD

**PROJECT TYPE:** Commercial SPUD

Owner: Sooner Traditions LLC & Hunter Miller Family, LLC

Developer's Representative: Rieger Law Group PLLC

Developer's Traffic Engineer: Traffic Engineering Consultants, Inc. (TEC)

#### **SURROUNDING ENVIRONMENT (Streets, Developments)**

The areas surrounding this site are generally commercial to the west and south and low density residential to the north and floodplain to the east. The development will connect to Berry Road to the west and Lindsey Street to the south.

#### **ALLOWABLE ACCESS:**

The access will be in accordance with Section 4018 of the City's Engineering Design Criteria.

#### **EXISTING STREET CHARACTERISTICS (Lanes, Speed Limits, Sight Distance, Medians)**

Berry Road: 2 lanes (existing and future). Speed Limit—30 mph. No sight distance problems. No median.

Lindsey Street: Transitions from 2 lanes to 4 lanes with 2 bike lanes (existing and future). Speed Limit—30 mph. No sight distance problems. No median, but median exists further west of Berry Road.

#### **ACCESS MANAGEMENT CODE COMPLIANCE:**

YES ☒ NO ☐

Proposed access for the development will comply with what is allowed in the subdivision regulations.

#### **TRIP GENERATION**

	<b>Total</b>	<b>In</b>	<b>Out</b>
Weekday	404	202	202
A.M. Peak Hour	10	6	4
P.M. Peak Hour	41	20	21

#### **TRANSPORTATION IMPACT STUDY REQUIRED?**

YES ☐ NO ☒

The development is proposed for location at the northeast corner of the intersection of Lindsey Street and Berry Road with an access drive to Berry Road to the west and an access drive to Lindsey Street to the south. Even though being below the threshold for when a traffic impact study is required (>100 peak hour trips is the threshold), the developer submitted a traffic impact analysis documenting the trip generation information for this SPUD due to the concerns expressed by nearby residents that this development will have on traffic volumes on Berry Road and Lindsey Street. The traffic data was obtained in September of 2021 when schools were in session. The impact from the development to the delay at the signalized intersection of Berry Road and Lindsey Street is anticipated to be 4 to 6 seconds. Queuing at the signal during the PM peak hour is expected to extend to the development's proposed access driveways on Berry Road north of the signal and on Lindsey Street east of the signal. However queuing from the development will not impact the through movements on either Berry Road or Lindsey Street, as the queuing will be contained within the development. No traffic operational issues are anticipated during the AM peak hour or throughout the remainder of the day.

**RECOMMENDATION:** APPROVAL ☒ DENIAL ☐ N/A ☐ **STIPULATIONS** ☐

*Recommendations for Approval refer only to the transportation impact and do not constitute an endorsement from City Staff.*

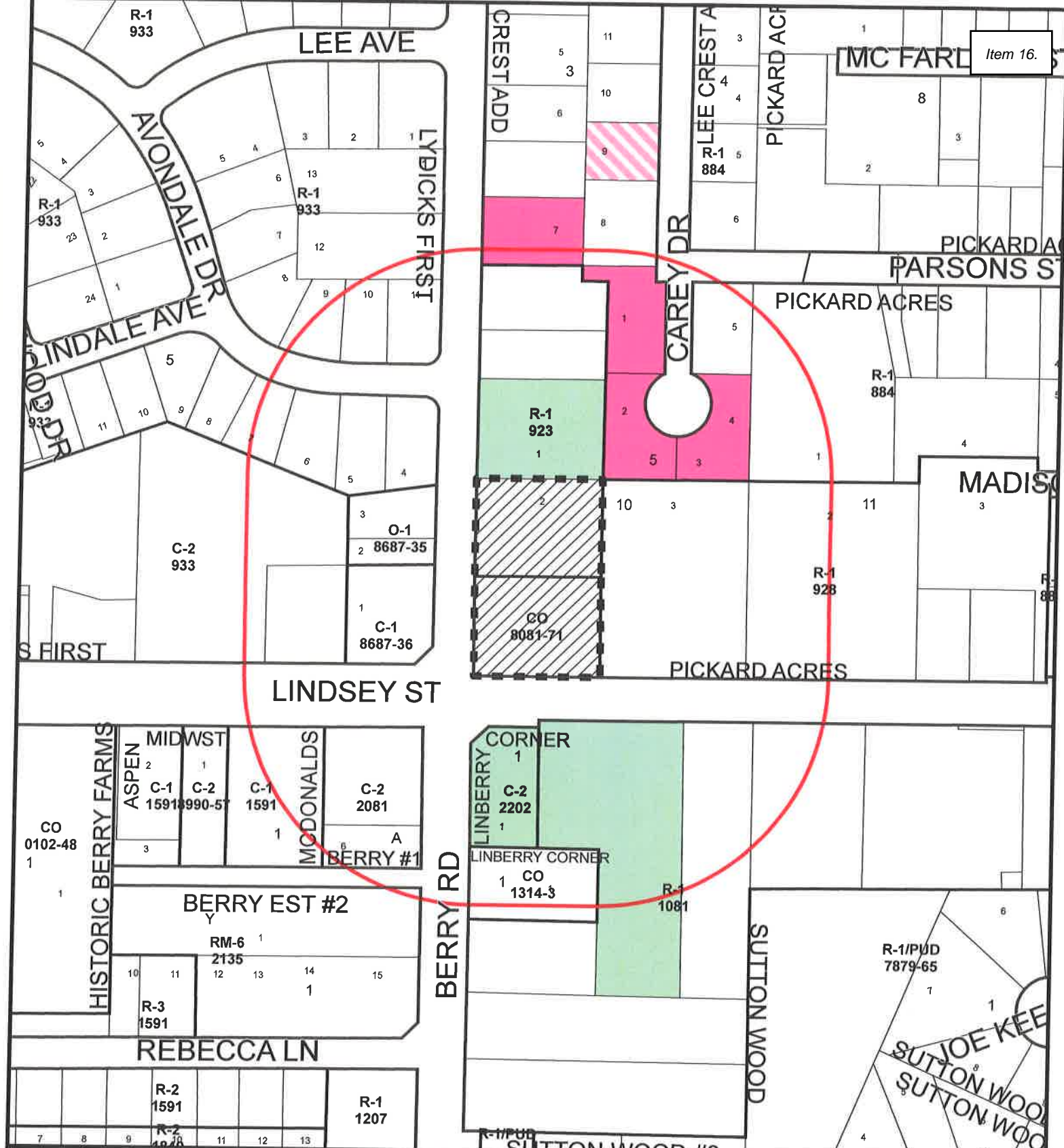
The proposed development will access Berry Road from the east by the proposed access drive located approximately 255 feet north of Lindsey Street and will access Lindsey Street from the north by the proposed access driveway located approximately 180 feet east of Berry Road. The proposed driveway on Lindsey Street will be designed for right turns in and right turns out only. Such a design will impact the current bus stop on Lindsey Street just east of Berry Road. The developer is willing to relocate this existing bus stop to just west of its current location. Capacity exceeds demand in this area. As such, no additional off-site improvements are anticipated.

PROTESTS & SUPPORTS  
Map & Letters

Sooner Traditions, L.L.C. & Hunter Miller Family, L.L.C.  
2025 Amendment & Rezoning from R-1 and CO to SPUD  
1027 & 1035 S. Berry Road

We will update the map with any additional letters received prior to the meeting.





Item 16.

# Protest & Support Map

8.3% Protest Within Notification Area  
17.8% Support Within Notification Area



Map Produced by the City of Norman  
Geographic Information System.  
The City of Norman assumes no  
responsibility for errors or omissions  
in the information presented.



0 100 200 Feet

- Subject Tract
- Notification Area
- Protest
- Support

October 6, 2021

STATEMENT OF SUPPORT

TO: Norman City Council  
TO: City of Norman Planning Commission  
TO: City of Norman Planning Department

I am familiar with the proposed rezoning request by Sooner Traditions, LLC, for the property situated to the northeast of the intersection at W Lindsey Street and Berry Road. I understand that the proposal is to change the zoning of that property to a Planned Unit Development that will mostly allow uses similar to C-1, Local Commercial. I support the development of a high-quality commercial building that will feature landscaping buffers and a privacy fence to nearby residential such as my house to the north. This will be better than the existing vacant residential and office that are in need of updating and replacement. As an owner of nearby property that is within the notification radius, I hereby sign below in full support of the proposed project, and respectfully request of you to adopt the proposal. Thank you very much for your consideration.

Signature: Printed Name: Dan GrahamDate: 8/18, 2021

Address of all the Property(s) we own near the proposed development:

1150 W. Lindsey

FILED IN THE OFFICE  
OF THE CITY CLERK  
ON 10/6/21-LW

STATEMENT OF SUPPORT

TO: Norman City Council  
TO: City of Norman Planning Commission  
TO: City of Norman Planning Department

I am familiar with the proposed rezoning request by Sooner Traditions, LLC, for the property situated to the northeast of the intersection at W Lindsey Street and Berry Road. I understand that the proposal is to change the zoning of that property to a Planned Unit Development that will mostly allow uses similar to C-1, Local Commercial. I support the development of a high-quality commercial building that will feature landscaping buffers and a privacy fence to nearby residential such as my house to the north. This will be better than the existing vacant residential and office that are in need of updating and replacement. As an owner of nearby property that is within the notification radius, I hereby sign below in full support of the proposed project, and respectfully request of you to adopt the proposal. Thank you very much for your consideration.

Signature: \_\_\_\_\_

Printed Name: \_\_\_\_\_

Date: \_\_\_\_\_, 2021

Address of all the Property(s) we own near the proposed development:

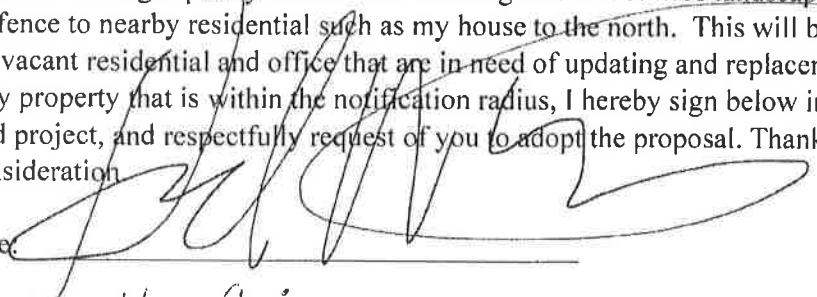
1104 W. LINDSEY STREET

FILED IN THE OFFICE  
OF THE CITY CLERK  
ON 10/6/21-LEA

STATEMENT OF SUPPORT

TO: Norman City Council  
TO: City of Norman Planning Commission  
TO: City of Norman Planning Department

I am familiar with the proposed rezoning request by Sooner Traditions, LLC, for the property situated to the northeast of the intersection at W Lindsey Street and Berry Road. I understand that the proposal is to change the zoning of that property to a Planned Unit Development that will mostly allow uses similar to C-1, Local Commercial. I support the development of a high-quality commercial building that will feature landscaping buffers and a privacy fence to nearby residential such as my house to the north. This will be better than the existing vacant residential and office that are in need of updating and replacement. As an owner of nearby property that is within the notification radius, I hereby sign below in full support of the proposed project, and respectfully request of you to adopt the proposal. Thank you very much for your consideration.

Signature: Printed Name: HAL SMITHDate: 08-18-2021, 2021

Address of all the Property(s) we own near the proposed development:

1424 W. LINDSEY STREET

FILED IN THE OFFICE  
OF THE CITY CLERK  
ON 10/6/21 - PW

**STATEMENT OF SUPPORT**

TO: Norman City Council  
TO: City of Norman Planning Commission  
TO: City of Norman Planning Department

I am familiar with the proposed rezoning request by Sooner Traditions, LLC, for the property situated to the northeast of the intersection at W Lindsey Street and Berry Road. I understand that the proposal is to change the zoning of that property to a Planned Unit Development that will mostly allow uses similar to C-1, Local Commercial. I support the development of a high-quality commercial building that will feature landscaping buffers and a privacy fence to nearby residential such as my house to the north. This will be better than the existing vacant residential and office that are in need of updating and replacement. As an owner of nearby property that is within the notification radius, I hereby sign below in full support of the proposed project, and respectfully request of you to adopt the proposal. Thank you very much for your consideration.

Signature: Carol West

Printed Name: CAROL WEST

Date: JUNE 28<sup>th</sup>, 2021

Address of all the Property(s) we own near the proposed development:

1017 S. BERRY ROAD

FILED IN THE OFFICE  
OF THE CITY CLERK  
ON 10/6/21-LW

The City of Norman  
 Planning and Community Development  
 201 West Gray, Bldg. A  
 Norman, OK 73069

Re: PD21-23

Dear Planning Commission:

I have read the Notice for Norman 2025 Land Use and Transportation Plan Amendment and Rezoning notice dated 18 August, 2021 and have developed the following opinion in regard to changing the zoning from CO (Suburban Office Commercial) to SPUD for the property located at 1035 South Berry Road and the change from R-1 (Single Family Dwelling) to SPUD for the property located at 1027 South Berry Road.

I, AUSE OSIS & DANIEL MAINS (printed name)

Own or reside at the property located

1128 W. BROOKS ST.  
NORMAN, OK 73069

Am (check the one that applies)

       Approve of the rezoning

X Oppose the rezoning

       Do not have an opinion about the rezoning

FILED IN THE OFFICE  
 OF THE CITY CLERK  
 ON 9/9/21-LW

Sincerely,

  (signature)

9/2/21 (date)

### Reasons for Opposition to Rezoning

1. The proposed rezoning will be a major nuisance to current residents who purchased their property with the understanding that they would be in a residential neighborhood. A commercial building will bring increased light and noise, disrupting the sleep and mental health of current residents. Additional vehicle exhaust will also undermine the health of current residents.
2. There is no need for commercial development on these lots. There is plenty of unused space in Norman that is already designated for commercial development. The city should first fill in this unused space before tearing down existing homes to make space for more businesses. At last year's planning commission meeting the attorney for the developers suggested that this new development would attract new businesses to Lindsey Street. There is no evidence that this would occur in Norman. Attorneys should be discouraged from making such statements without evidence that is specific to Norman. The city needs to invest in revitalizing and building up existing commercial districts rather than rezoning residential neighborhoods.
3. This new development will generate large volumes of additional storm water in Imhoff Creek. Although city regulations require new developments to not increase rates of flow, volume of flow will increase. Imhoff Creek is already extremely stressed due to development in the watershed, as evidenced by the recent closure of the Imhoff Road bridge – a problem that will cost close to \$1 million to repair. A sixty car parking lot will place great stress on the city's stormwater system, leading to more expenses that must be covered by Norman residents.



August 26, 2021

Protest Letter

Item 16.

TO: City of Norman Department of Planning and Community Development

FORMER CASE NUMBER: PD21-23

APPLICANT: Sooner Traditions, L.L.C. and Hunter Miller Family, L.L.C.

Location: 1027 and 1035 S. Berry Rd.

Ward: 4

FROM:

David and Kathy Nehrenz

902 Carey Dr.

Norman, OK. 73069

We received the letter about this re-zoning request. Our property in Ward 4 is within the 350-foot boundary and we have lived here since 1988.

We are opposed to the applicant's request that this property be re-zoned from CO/R-1 to SPUD and Commercial Designation, so that an office and commercial business complex can be built right next to our residential neighborhood.

Our house at 902 Carey Dr. and the house next door, in which our daughter lives at 906 Carey Dr., are the properties whose backyards will back up to those properties at 1027 and 1035 S. Berry Rd.

The reasons for our opposition are as follows:

1. The proposed plan to put a commercial building with 60 parking spaces right up against our residential neighborhood should be rejected now. It is an affront to all the people who live in the houses around these properties. The planning commission should not allow the applicant to have the SPUD zoning. We do not need another commercial property at that intersection. This has always been and should remain a residential area. There are vacant commercial properties all over Norman. We do not need more of them.
2. We request that a traffic study and a light-pollution study be ordered and paid for by the applicant, since a commercial building with 60 spaces would drastically impact the neighborhood. The increased traffic at the Berry and Lindsey intersection and the commercial lighting would ruin the residential atmosphere of all the houses around it. It is disconcerting that this request has been made again and is actually being reconsidered by the city. The traffic backup is already bad and will become even worse with a commercial building there.
3. Your decision about this request does not have to be based upon hypotheticals. Every one of you on the planning commission needs to go over to those two houses on Berry Rd. Walk up to the sides and backyards of those properties to see that over 11 large mature trees were clear cut and removed. Do we really want a strip mall with 6 businesses and 60 parking spots on that corner? The applicant is assuming that it will be approved for this re-zoning. Why else would all the large trees already have been cut down? It is presently being prepared and groomed to be developed, before the case has even been considered by you.

FILED IN THE OFFICE  
OF THE CITY CLERK  
ON 8/30/21 - 2W

4. We do not want a parking lot with 60 cars, the automobile exhaust, with noise and light pollution backyards. We already have problems with transients coming off of Berry Rd., going through the properties without permission, and cutting through our back yards to come over to Carey Dr. This will increase if that becomes a commercial development with a parking lot and dumpsters by our properties.

5. The decision is actually a simple one: Does the planning commission want to see those two houses removed so that it becomes a further extension east of the Lindsey St. commercial zone? Or do you want those two properties to remain an extension of the residential properties extending down Berry Rd. to Lindsey St.? Placing another commercial development near our houses will decrease their resale values and adversely affect all the houses located on Berry Rd.

6. On a positive note, the applicant should withdraw the application for this commercial development on Berry Rd. by our neighborhood. The two houses could be improved and increase the value of our adjacent homes. Then the applicant would be showing us that they want to be happy and good neighbors to all of us. This would be the best outcome for everyone.

Please hear the concerns of neighbors like us, **reject this re-zoning application.**

Sincerely,

David and Kathy Nehrenz

[davenehrenz@aol.com](mailto:davenehrenz@aol.com)

405-306-8464

*David Nehrenz*  
*Kathy Nehrenz*

P.S. Here are photos of the back-sides of the Carey Dr. and Berry Rd properties.



June 21, 2021

**TO: City of Norman Department of Planning and Community Development**

**CASE NUMBER: PD21-23**

APPLICANT: Sooner Traditions, L.L.C.

Location: 1027 and 1035 S. Berry Rd.

Ward: 4

FILED IN THE OFFICE  
OF THE CITY CLERK  
ON 6-22-21

**FROM INTERESTED NEIGHBORS:**

David and Kathy Nehrenz

902 Carey Dr.

Norman, OK. 73069

We received the letter about this re-zoning request. Our property in Ward 4 is within the 350-foot boundary and we have lived here since 1988.

We are opposed to the applicant's request that this property be re-zoned from R-1 to SPUD, so that an office and commercial business complex can be built right next to our residential neighborhood.

Our house at 902 Carey Dr. and the house next door, in which our daughter lives at 906 Carey Dr., are the properties whose backyards will back up to those properties at 1027 and 1035 S. Berry Rd.

The reasons for our opposition are as follows:

1. The proposed plan to put a commercial building with 60 parking spaces right up against our residential neighborhood should be rejected now. It is an affront to all the people who live in the houses around these properties. The planning commission should not allow the applicant to have the SPUD zoning. We do not need another commercial property at that intersection. This has always been and should remain a residential area. There are vacant commercial properties all over Norman. We do not need more of them.
2. We request that a traffic study and a light-pollution study be ordered and paid for by the applicant, since a commercial building with 60 spaces would drastically impact the neighborhood. The increased traffic at the Berry and Lindsey intersection and the commercial lighting would ruin the residential atmosphere of all the houses around it. It is disconcerting that this request has been made and is actually being considered by the city.
3. Your decision about this request does not have to be based upon hypotheticals. Every one of you on the planning commission needs to go over to those two houses on Berry Rd. Walk up to the sides and backyards of those properties to see that over 11 large mature trees were clear cut and removed. Do we really want a strip mall with 6 businesses and 60 parking spots on that corner? The applicant is assuming that it will be approved for this re-zoning. Why else would all the large trees already have been cut down? It is presently being prepared and groomed to be developed, before the case has even been considered by you!
4. We do not want a parking lot with 60 cars, the automobile exhaust, with noise and light pollution in our backyards. We already have problems with transients coming off of Berry Rd., going through the properties without permission, and cutting through our back yards to come over to Carey Dr. This will increase if that becomes a commercial development with a parking lot and dumpsters by our properties.



5. The decision is actually a simple one: Does the planning commission want to see those two houses removed so that it becomes a further extension east of the Lindsey St. commercial zone? Or do you want those two properties to remain an extension of the residential properties extending down Berry Rd. to Lindsey St.?

6. On a positive note, the applicant should withdraw the application for this commercial development on Berry Rd. by our neighborhood. The two houses could be improved and increase the value of our adjacent homes. Then the applicant would be showing us that they want to be happy and good neighbors to all of us. This would be the best outcome for everyone.

Please hear the concerns of neighbors like us, reject this re-zoning application.

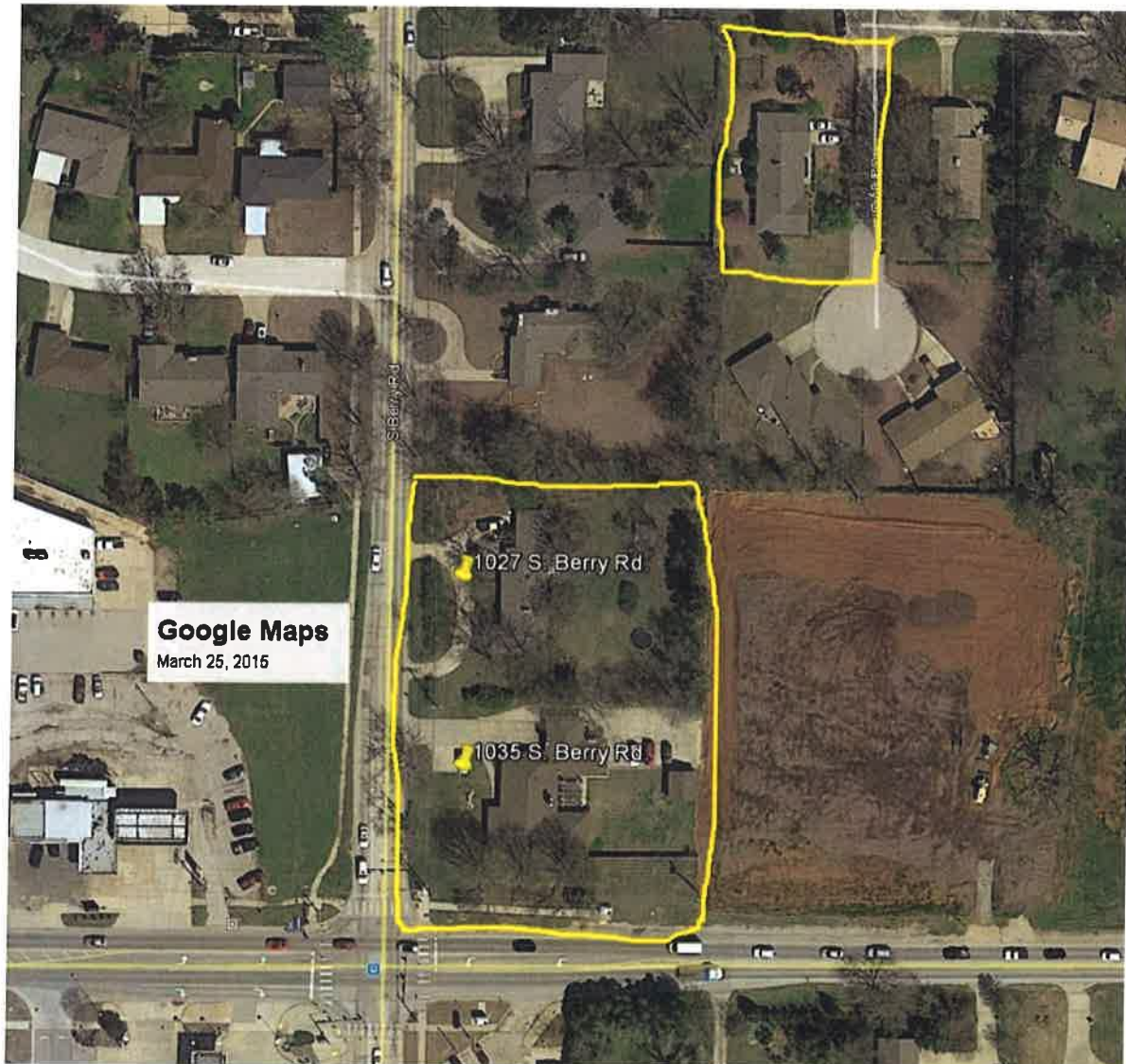
Sincerely,

David and Kathy Nehrenz

[davenehrenz@aol.com](mailto:davenehrenz@aol.com)

405-306-8464

P.S. Here are photos of the back-sides of the Carey Dr. and Berry Rd properties.







**TO:**

City of Norman Department of Planning and Community Development

**FORMER CASE NUMBER: PD21-23**

APPLICANT: Sooner Traditions, L.L.C. and Hunter Miller Family, L.L.C.

Location: 1027 and 1035 South Berry Road

Ward: 4

**FROM:**

Michelle Nehrenz

906 Carey Drive

Norman, OK 73069

I am writing in regards to the letter I received about the rezoning request for the properties at 1027 and 1035 South Berry Road. The property I reside in is in Ward 4 within the 350-foot boundary of those locations. I am adamantly **opposed** to the applicant's request that these properties should be rezoned from CO/R-1 to SPUD and Commercial Designation.

My house is at 906 Carey Drive, which comes right to the corner of the 1035 South Berry Road property. The house next door to me, 902 Carey Drive, is where my parents reside and the house where I grew up. Carey Drive is a wonderful, quiet, clean street that any family in Norman would be proud to live on. I know how fortunate I am to live in a neighborhood where we take care of each other and genuinely enjoy life. The request to rezone the properties that back up to this spectacular street will ruin that atmosphere. The proposed plan for a commercial development with parking spaces that back up to my house and encroach on our residential neighborhood should be rejected.

**I am opposed to this rezoning request because of the following:**

1. The traffic at Lindsey and Berry Road is already a nightmare. On a daily basis the stoplights are packed and with college students back in full force the cars back up for blocks. This intersection is completely unique. There is no other intersection in Norman which leads into the university taking the road from 4 lanes down to 2 with turn lanes and a bike lane in the mix. A traffic study **of this specific intersection** needs to be conducted as adding a commercial space with ingress and egress on both Lindsey and Berry will drastically impact the already congested traffic. The increased traffic at this intersection will ruin the residential feel of the surrounding neighborhoods and make it difficult for us to get in and out of our own driveways and streets. I'm very concerned that this request to rezone has been made without due diligence and consideration for how it will affect our streets.
2. The commercial lighting which would be installed will completely ruin the atmosphere of the houses around it. My property line backs up to the property which would contain the lighting and parking for the proposed plan. I do not want my entire back yard being illuminated by bright lights spilling over from the parking lot. A light-pollution and environmental study also needs to be conducted for this area to determine how much spillover the proposed lighting solution would have.

FILED IN THE OFFICE  
OF THE CITY CLERK  
ON 9/3/21 - *RW*

3. We do not need more commercial real estate space in Norman. There are countless vacant buildings in this town where businesses could reside. Adding more commercial space that will end up sitting empty and eventually deteriorate is irresponsible, pointless, and not warranted at this intersection. This has always been a residential area and that is how it should remain.
4. Allowing this proposed project near our homes will decrease their resale values and negatively affect all surrounding properties. No one wants to live next to a commercial building with the smell of dumpsters in the parking lot, car exhaust from people coming and going, and trash blowing into their yards.

The representatives of the applicant have stated in previous city meetings that they want to better the Norman community, but trying to place a commercial development at this intersection does exactly the opposite. It does nothing to support our neighborhood and causes more problems that we, the actual residents of the area, will have to deal with on a daily basis.

Please listen to our concerns. This directly affects our lives and I ask that you **reject the rezoning application.**

Sincerely,

Michelle Nehrenz  
[michellenehrenz@gmail.com](mailto:michellenehrenz@gmail.com)  
405-570-7188





**Gabrielle Mandeville**  
**905 Carey Drive**  
**Norman, OK 73069**

The City of Norman  
Planning and Community Development  
City Clerk  
201 W. Gray, Bldg. A  
Norman, OK 73069

August 30, 2021

Re: Case PD20-10 / O-1920-58 & R-1920-115  
Re-zoning from R-1 to SPUD

It is incredible that we have to object to this request once again. This neighborhood's unified protest last year appears to have been ignored by the City and an apparently influential developer is able to yet make another attempt to encroach our purely residential neighborhood, requesting re-zoning to establish a strip-mall – practically in my and my neighbors backyard. This is simply outrageous.

I strongly protest this request, as it is based on greed and certainly not need. There are many other properties available. That particular part of Berry Road/Lindsey has a traffic congestion problem already, because the City of Norman failed to establish a right-turn lane from Berry unto Lindsey Street when that street was “improved”. That oversight not only created a serious traffic jam at peak hours, but it is virtually impossible for an emergency vehicle to turn from Berry unto Lindsey Street (heading South from Berry) now. A strip-mall would make any emergency approach impossible in that area.

There are serious security, sanitary and noise issues to be addressed as well. My house is within 200 feet of those properties requested for re-zoning. I hope the City of Norman shows that they care about their citizens in this neighborhood and understands our concerns and uses common sense in their decision.



FILED IN THE OFFICE  
OF THE CITY CLERK  
ON 8-31-21

The City of Norman  
 Planning and Community Development  
 201 West Gray, Bldg. A  
 Norman, OK 73069

Re: PD21-23

Dear Planning Commission:

I have read the Notice for Norman 2025 Land Use and Transportation Plan Amendment and Rezoning notice dated 18 August, 2021 and have developed the following opinion in regard to changing the zoning from CO (Suburban Office Commercial) to SPUD for the property located at 1035 South Berry Road and the change from R-1 (Single Family Dwelling) to SPUD for the property located at 1027 South Berry Road.

I, Cynthia Brnx Mills (printed name)

Own or reside at the property located

1119 W. Brooks

FILED IN THE OFFICE  
 OF THE CITY CLERK  
 ON 9/2/21-RW

Am (check the one that applies)

☐ Approve of the rezoning

☒ Oppose the rezoning -Please do not! My house

☐ Do not have an opinion about the rezoning

Sincerely,

Cynthia Brnx Mills (signature)

8/30/21 (date)

is at the end of Carey Dr. and I can see the cars go by on Lindsay  
Please no more!!!

The City of Norman  
 Planning and Community Development  
 201 West Gray, Bldg. A  
 Norman, OK 73069

Re: PD21-23

Dear Planning Commission:

I have read the Notice for Norman 2025 Land Use and Transportation Plan Amendment and Rezoning notice dated 18 August, 2021 and have developed the following opinion in regard to changing the zoning from CO (Suburban Office Commercial) to SPUD for the property located at 1035 South Berry Road and the change from R-1 (Single Family Dwelling) to SPUD for the property located at 1027 South Berry Road.

I, Delbert L. Mills (printed name)

Own or reside at the property located

1119 W Brook St  
Norman, OK  
73069

Am (check the one that applies)

☐ Approve of the rezoning

☒ Oppose the rezoning

☐ Do not have an opinion about the rezoning

FILED IN THE OFFICE  
 OF THE CITY CLERK  
 ON 9/2/21-RW

Sincerely,

Delbert L. Mills (signature)

8-30-2021 (date)

The City of Norman  
 Planning and Community Development  
 201 West Gray, Bldg. A  
 Norman, OK 73069

Re: PD21-23

Dear Planning Commission:

I have read the Notice for Norman 2025 Land Use and Transportation Plan Amendment and Rezoning notice dated 18 August, 2021 and have developed the following opinion in regard to changing the zoning from CO (Suburban Office Commercial) to SPUD for the property located at 1035 South Berry Road and the change from R-1 (Single Family Dwelling) to SPUD for the property located at 1027 South Berry Road.

I, GARY KRUK (printed name)

Own or reside at the property located

816 CAREY Dr.  
NORMAN, OK. 73069

Am (check the one that applies)

☐ Approve of the rezoning

☒ Oppose the rezoning STRONGLY OPPOSE! Please don't allow this to happen to our neighborhood.

☐ Do not have an opinion about the rezoning

Sincerely,

Gary Kruk (signature)

August 30, 2021 (date)

FILED IN THE OFFICE  
 OF THE CITY CLERK  
 ON 9/2/21-LW

## MICAH ALCORN, CCIM

906 Carey Drive  
Norman, OK 73069  
mail@micahalcorn.com  
405.818.2257

August 28, 2021

City of Norman  
City Clerk  
P.O. Box 370  
201 West Gray  
Norman, OK 73070

Re: Cases No. O-1920-58 & R-1920-115

To Whom It May Concern:

Please accept this letter as my protest in the matter of the Norman 2025 Land Use and Transportation Plan Amendment and Rezoning request by Sooner Traditions, L.L.C. and Hunter Miller Family, L.L.C. in regard to the properties commonly known as 1027 S. Berry and 1035 S. Berry as described in the letter from the Norman Planning Commission dated August 18, 2021. The subject property touches my property at the subject's northeast corner and I do not support an encroachment of commercial uses further into an established residential neighborhood. I would encourage members of the Planning Commission and City Council to vote to deny the request.

Sincerely,



Micah Alcorn

FILED IN THE OFFICE  
OF THE CITY CLERK  
ON 9/2/21

Dana Drury and Stephen Maple  
951 South Berry Road  
Norman, OK 73069

Norman Planning Commission  
201-A West Gray Street (PO Box 370)  
Norman, OK 73069

FILED IN THE OFFICE  
OF THE CITY CLERK  
ON 9/3/21 - EW

**RE: 1027 and 1035 South Berry Road**

Dear Commissioners,

We are once again protesting the Sooner Traditions proposal to change the zoning from R-1/C0 to commercial for the subject parcels at the northeast corner of Berry Road and Lindsay.

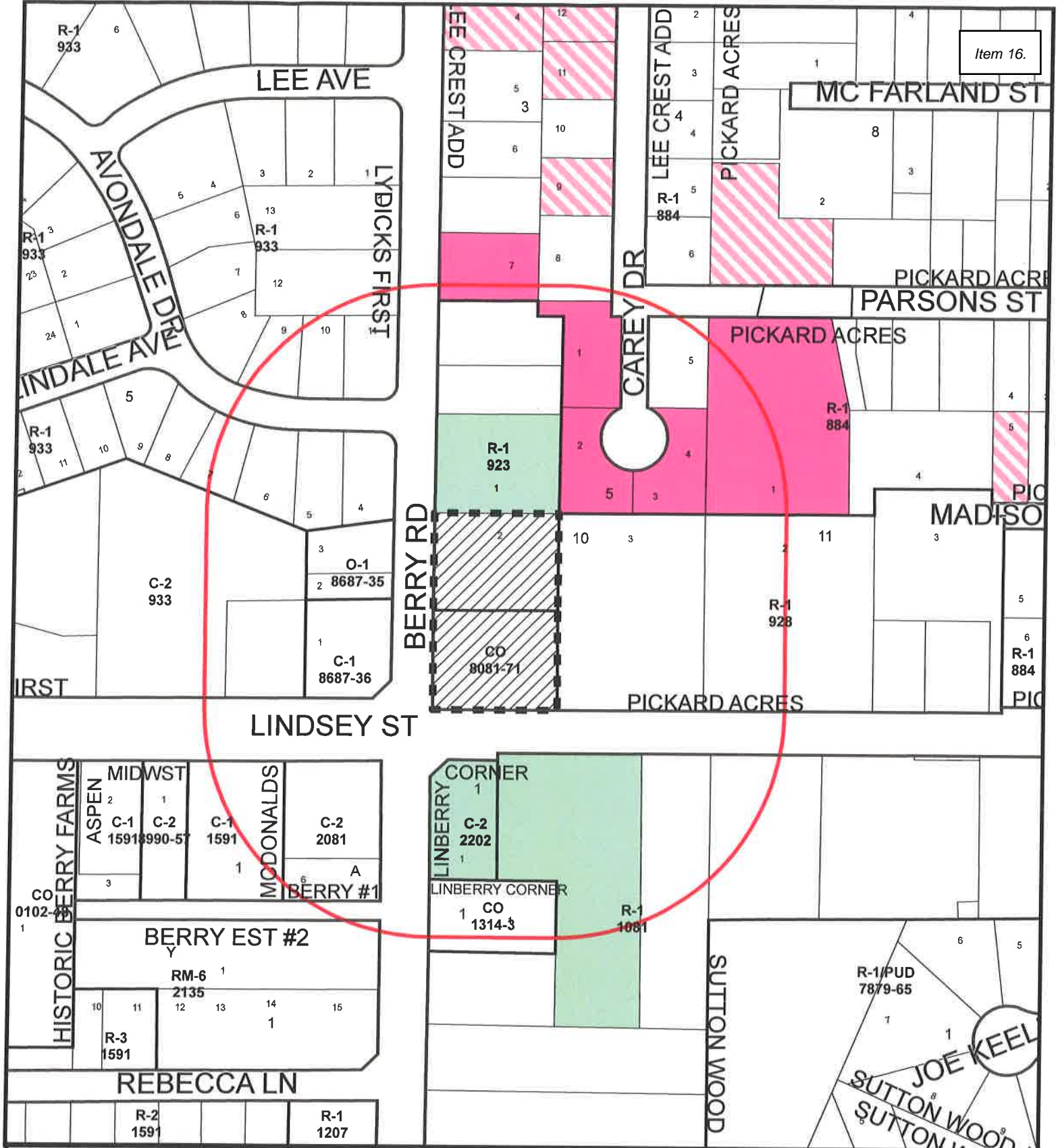
- Berry Road north of Lindsay is essentially a residential street: narrow, two-lane, single family houses, nice trees, with deteriorating sidewalk on only the west side of Berry. At night the area is poorly lit.
- Proximity to the university and west Lindsay commerce results in frequent bicycle, skateboard, scooter and pedestrian traffic on Berry at all hours of the day and night.
- Southbound traffic on Berry already backs up to Lee daily during lunch and evening commutes. In the 20 years we have lived here, we have seen it back up to Brooks, especially when students are in town. Adding a commercial center requiring left turn from southbound Berry will likely increase traffic backups throughout the day, beyond just the current high traffic times.
- To accommodate the additional traffic entering and exiting a commercial property, the Lindsay/Berry intersection could be widened. Who will pay for that? And is that even the answer? Seems like a “build it and they will come” ... a nice wide, modern intersection that eases traffic flow would of course increase traffic ... which will only make things worse farther north on Berry, which — like the area just north of Lindsay — is residential, narrow two lane, lots of pedestrian/bicycle/scooter traffic, with a deteriorating sidewalk on only the west side until north of the Berry/Brooks intersection.
- There is plenty of unoccupied and empty commercial space in the area. Assuming there is an actual legitimate business





UPDATED PROTESTS & SUPPORTS  
As of October 14, 2021  
Map & Letters

Sooner Traditions, L.L.C. & Hunter Miller Family, L.L.C.  
2025 Amendment & Rezoning from R-1 and CO to SPUD  
1027 & 1035 S. Berry Road



# Protest & Support Map

12.5% Protest Within Notification Area  
 17.8% Support Within Notification Area



Map Produced by the City of Norman  
 Geographic Information System.  
 The City of Norman assumes no  
 responsibility for errors or omissions  
 in the information presented.



0 100 200 Feet

- Subject Tract
- Notification Area
- Protest
- Support

October 14, 2021

To: The City of Norman  
Planning and Community Development  
201 West Gray, Bldg A  
Norman, OK, 73069

From: Nathalie Rocher and Solon Clinton  
903 Carey Drive,  
Norman, OK 73069  
October 12<sup>th</sup>, 2021

Dear Planning Commission,

My husband and I own a home at 903 Carey Drive, which is located within the 350 feet radius of the two properties that are being considered for re-zoning at the corner of Berry Road and Lindsey Street.

We **oppose the proposed re-zoning of these two properties** because we believe that the proposed changes would impact our neighborhood and Norman in the following negative ways:

- Norman is already full of half-empty strip malls that do not cater to the local residents. This project misses an opportunity for a locally owned business to create a carefully and thoughtfully designed plan that nearby residents would cherish.
- An increase in light and noise pollution due to a busy commercial property adjacent to multiple residential lots.
- An impact from the water run-off as the property would become mostly paved.
- A large increase in the vehicular traffic coming and going from this property will cause an already chaotic intersection to become far less safe for cars, bikes, and pedestrians. This intersection regularly backs up in all directions and the small size of the parcel will not allow the entrances to the property to be far enough from the Berry/Lindsey corner to avoid causing major issues.
- A decrease in pedestrian safety along both Lindsey St and Berry Rd due to the number of vehicles entering a congested traffic zone.

While the list of negative impacts is significant, the proposed rezoning project does not seem to bring any benefits to the community as a whole, and especially not to the neighborhood. The City of Norman website states that "the Department of Planning & Community Development at the City of Norman nurtures the growth of the City and aims to improve the health, safety, and welfare of the citizens." In our judgement, the proposed zoning changes would in no way improve the health, safety or welfare of the citizens of Norman, and would have serious negative impacts on the neighborhood.

We hope that you will take our concerns into consideration, and we thank you for your time.

Kind regards,  
Nathalie Rocher, PhD and Solon Clinton



FILED IN THE OFFICE  
OF THE CITY CLERK  
ON 10/13/21 - RW

October 2021

City of Norman  
Planning and Community Development  
P.O. Box 370  
Norman, OK 73070

10/5/2021

Dear Planning Commission,

I am opposed to the rezoning which would allow for the building of a new strip mall on the North East Corner of Berry Road and Lindsey Street.

There are already too many vacant commercial structures in this area, and I want to support Norman's existing business owners in their efforts to rebuild Lindsey Street after the City of Norman installed restrictive traffic medians.

This development will add almost an acre of impervious surface that will increase the intensity of stormwater runoff directly into Imhoff Creek.

Berry and Lindsey are EMS Safety Corridors and should not be restricted.

My Name is: Ron LaSpisa

I live at: 2811 Walnut Rd

Norman 73072

Signed: RJ LaSpisa

FILED IN THE OFFICE  
OF THE CITY CLERK  
ON 10/7/21-LW

Sept 30, 2021

Dear Planning Commissioners,

I am AGAINST the rezoning of the property located a 1027 South Berry Road. I live at 808 Carey Drive (Carey Drive is a cul-de-sac within the impacted area.) Please realize the developer bought property in this area with full knowledge at time of purchase it could only be used for R1 and CO.

Zoning is a contractual agreement between a city and its residents. We homeowners, bought and maintain our houses in this stable high-end R-1 neighborhood, next to other R-1 families. I am asking the Planning Commission to continue to honor their part in Norman's R-1 zoning agreement.

You are being asked to vote for? What business? I'm sure you will be shown some pretty architectural drawings; however, I would like you to picture those pretty plans on the vacant property catty corner across the street - property which is already zoned commercial. Lindsey Street has many vacant commercially zoned properties within this market area; and if the developer has development money then why isn't it being used in an already commercially zoned area? Your vote comes down to simply being asked to add another vacant commercial building because this is a speculative developer. This development will only add to the commercial vacancy problem in Norman.

**There are several other important reasons to vote No.**

**Emergency Services:** Consider the direct impact it would have to emergency services. Berry Road in this area is only a two-lane road. This traffic safety corridor serves a very large residential area in Norman. This development would slow and constrict emergency services along the Berry Road safety corridor; and restrict traffic along Lindsey while cars are waiting to turn left.

**Support of Existing Businesses:** Lindsey Street, is an area which has been clearly defined as commercial with traffic controlled turning medians. The developer wants to circumvent these traffic controls that the City placed on existing businesses. Lindsey Street needs all the help it can get. Lindsey Street is one of four OU "first impression" streets from I-35, and Norman should put it's best foot forward with a vibrant first view when people exit on Lindsey Street.

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OF THE CITY CLERK  
ON 10/7/21

**Stormwater:** Please learn from Penny Hill with the Imhoff bridge washout and the adjoining home auctioned off. This development will add almost an acre of impervious surface, increasing the intensity of stormwater runoff directly into Imhoff Creek. (By the way, Imhoff bridge is being repaired using Covid relief funds because the City of Norman lacked local repair funding)

Back to my plea to please keep this R-1 Neighborhood stable. People do not remodel their homes nor plant gardens near commercial dumpsters.

I'm trusting you as a Planning Commissioner to carefully consider the harmful consequences of this speculative vacant structure for some unknown commercial business in my healthy R-1 neighborhood.

Susan Meyer



808 Carey Drive  
Norman, OK 73069

TO: City of Norman Planning Commission

RE: Proposed rezoning of lots on NE Corner of Lindsey and Berry

FROM: Dennis Yarbrow  
803 S Berry Rd  
Norman, OK 73069

Planning Commission Members:

I am emphatically AGAINST the re-zoning of property for merely greedy purposes. Not just here, but anywhere. Re-zoning of well established neighborhood properties should be reserved for the best interests of the whole community. Not for speculation and profiteering. Not to the detriment of the existing residents and the livability of this area.

When I retired in 2016, my wife and I were discussing where to move and she wanted to return her birth city of Norman, OK to be near her aging family members and her "home town".

We looked around and invested our retirement dollars in property in a well established, stable neighborhood, near her families' homes. We invested more of our retirement savings to make the property more suitable for our needs. We could have bought in a new neighborhood on the fringes of Norman, but chose to rehabilitate a structure in an older neighborhood, thinking we would be insulated from radical changes in land use. We followed Norman's processes, obtained permits, used local contractors, got inspections, etc. We did not ask for special favors or try to circumvent the established zoning and stability of any neighborhood. We now ask that Norman honor its long term commitment to the neighborhood into which we placed our life savings.

This developer chose to purchase a property which had been changed from R-1 to CO for a residential style office. CO use has a low amount of traffic, noise or light pollution, not much different from a normal residence. That change from R-1 to CO was most likely approved because it presented a use fundamentally very little different from just a normal residence. Re-zoning to SPUD radically changes that use: higher traffic; more noise; more intense lighting; extended operational hours. All adjacent to the backyards of long term residents who want a quiet, peaceful neighborhood in which to raise their families and enjoy retirement and quiet evenings.

The developer also purchased an adjacent property zoned R-1, knowing full well how both these were zoned and intended to be used. They speculated that if they hired the right attorney, the properties could be shoved through, re-zoned and they could make a huge profit on their speculation and greed. There is no telling which of the numerous possible uses of a SPUD will go into these spaces, now or in the future, as business ventures come and go and indeed, traditional businesses are shuttered and new previously unimagined of uses emerge in a neighborhood where they previously were not allowed.

The developer did not want to re-invest in the numerous already commercially zoned, vacant properties which line both sides of Lindsey, west of Berry. They want to impose

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ON 10/27/21



their greed-based development into a long established residential neighborhood and disregard both the neighborhood residents and previous Planning Commission votes. They continue to bring this back time and time again with the hopes that the members of the Planning Commission will one day change and then support the re-zoning.

The Planning Commission serves as part of a check and balance system for the benefit of all residents of the City of Norman. Building standards, permitting, licensing, other rules are all part of that same system. It presents a method for a planned, consistent and thoughtful process for bringing Norman and its residents toward a bright future which is inviting for people and businesses alike. Un-checked development is not the way to make people want to move into and stay in Norman. That is a well known fact in all modern cities.

In contrast, actions like this proposed re-zoning, and the accompanying uncertainty it presents, make people want to move out of the core city, which increases traffic and increases the amount of vacant and run-down properties to a point which is detrimental to the citizens and to established businesses. Or, move even farther away into some of the bedroom communities which are near, but not too near. We need to encourage re-investment in established, properly zoned areas, in a planned and consistent method, not shoehorn commercial zones into neighborhoods.

This proposed development will increase traffic in an already congested intersection. The proposal ADDS a left turn entrance off East Lindsey in very close proximity to the traffic control signal which at times will back up traffic into the Lindsey/Berry intersection: Result - Gridlock. It also increases the likelihood of impeding traffic along Berry, making it even more difficult for residents to exit and enter their homes.

Further traffic congestion slows Emergency Response and adds precious minutes to life and property saving efforts on the staff of the Fire Department, the Police Department and Emergency Services.

I implore the Commission to continue to hold to the core values which have long guided Norman as a people-friendly, neighborhood focused community, not give in to Wild West developers and speculators, which can only drive residents away from Norman.

Re-zoning of these properties is WRONG.

It is WRONG for the established neighborhood.

It is WRONG minded to rezone when there are so many vacant commercial properties.

It is WRONG for Norman and the future of this city.

The RIGHT thing would be to re-establish the R-1 designation for the CO property, to protect residents investments and to provide a neighborhood where people want to live, they know their neighbors, raise their families by building attractive, well-maintained, enticing homes which encourage generations to stay and help Norman grow.

~~June 8, 2020~~

Oct 10, 2021

(R)

Planning Commission  
Planning and Community Development  
201 West Gray Street - Building A  
Norman, OK 73069

Dear Planning Commission,

I am opposed to the PD20-10 zoning request for the property located at 1035 and 1027 South Berry Road.

I live near the corner of Berry and Brooks Street and often travel through that congested intersection.

I also own commercial rental property on Main Street and the City should not destabilize R1 residential area and Suburban Office Commercial. This SPUD request hinders the filling of the vacant commercial properties directly across the street and all along Lindsey where there are appropriate traffic controls.

Please deny the PD20-10 request on the corner of Berry and Lindsey.

Thank you,



Roxana Leonard  
1202 West Brooks Street  
Norman, OK 73069

FILED IN THE OFFICE  
OF THE CITY CLERK  
ON 10/7/21

TO: City of Norman Planning Commission

RE: Proposed rezoning of lots on NE Corner of Lindsay and Berry

FROM: Konstantinos Karathanasis and  
Ekaterini Akarepi  
804 Carey Dr.  
Norman, OK, 73069

10-08-2021

Dear Planning Commission Members:

We are strongly opposed to the rezoning of the said property from CO/R1 to SPUD.

The quiet, family-oriented character of the little cul-de-sac neighborhood at Carey Drive was the primary reason that attracted us to buy our property on the street. Colleagues and friends at the University of Oklahoma were commenting that this neighborhood is one of the most well kept and charming streets in Central Norman. We are one of six families in Carey with elementary aged children who meet and play in the street with no worries about traffic and strangers coming and going.

We are deeply concerned that the proposed rezoning will drastically alter the residential character of our neighborhood and have a dramatically negative impact on the quality of our lives. Should the proposed rezoning is approved, the value of our property will get a direct hit. In matter of fact, the applicant has already caused harm to the neighborhood by cutting large trees that filtered traffic noise from Lindsey.

Why would we want another strip mall in Norman, in such a close proximity to historic and long-established family neighborhoods, while there are vacant commercial lots on Lindsey and Main? There are no other reasons apart from profiteering. We firmly believe that the Norman Planning Commission should protect the residents of Carey Drive and their quality of life instead of rewarding the blind corporate greed of the applicant.

FILED IN THE OFFICE  
OF THE CITY CLERK  
ON 10/8/21

Thank you for your consideration and support,

Konstantinos Karathanasis, Ph.D.



Professor of Music Technology & Composition  
School of Music, The University of Oklahoma

Ekaterini Akarepi, Ph.D.



Lecturer of World Music  
The University of Oklahoma

816 Carey Drive  
Norman, OK 73069  
October 8, 2021

The City of Norman  
Planning and Community Development  
201 W. Gray, Bldg. A  
Norman, OK 73069

Re: O-2122-15 and R-2122-31  
Rezoning from R-1 and CO to SPUD

To whom it may concern:

Six years ago, our neighborhood, just northeast of South Berry Road and Lindsey Street, thwarted an attempt to rezone some residential lots for a strip mall. Before it was all over, most of the big trees on the lots along Lindsey Street were cut down. These trees blocked the view of traffic on Lindsey Street from our neighborhood. Two of the four lots were recently sold to Sooner Traditions who has again applied to rezone these two lots for a strip mall. Our neighborhood is working to stop this rezoning. On Monday, September 14th, 2020 ten large Oak, Pecan and Sweetgum trees on one of the lots subject to the rezoning application were cut down. A few months ago, all the trees on the lot just north of those subject to the rezoning application were removed.

So, I guess this is a form of punishment that is taken out on trees! I think those involved in the rezoning will continue the effort to build a strip mall in a residential area as it seems that money can do whatever it wants. We need to protect our neighborhoods and our TREES!

I have lived on Carey Drive for 33 years. When I moved here I was a young adult with three children and now I am one of the seniors on the block. It is a great place to live because we all know and care about each other. We have block parties in our quiet street and talk to each other outside frequently. I can't imagine how much this would change our street to have a strip mall that close to us. The strip mall would be within 300 feet of our block party site!

The closeness of the commercial development and the increased traffic are major concerns to me. Therefore, I oppose the rezoning.

Sincerely,



Leslie Cornwell

FILED IN THE OFFICE  
OF THE CITY CLERK  
ON 10/8/21-LW

818 Carey Drive  
Norman, OK 73069  
October 8, 2021

City of Norman  
Planning Department  
201 West Gray, Building A  
Norman, OK 73069

Re: R-2122-31 and O-2122-15 (Rezoning of properties located at 1027 and 1035 South Berry Road)

Dear Planning Commission:

I am writing this letter of protest to highlight several issues for your consideration during the planning commission meeting of October 14, 2021. Unfortunately, I will be out of town during the meeting and unable to attend.

1. The "traffic study" that was presented in the agenda meeting packet is
  - a. not a true traffic study since it did not address the issues that will arise if the strip mall is in place, and
  - b. is in violation of current City of Norman rules/guidelines for the distance between a commercial driveway and an intersection.

There are other issues with the traffic study as presented. The most notable is that there is not another intersection in Norman designed or operated as the one at the intersection of South Berry Road and West Lindsey Street. The generic traffic counts are meaningless.

2. The letters of "support" for the proposed rezoning are indeed self-serving. Mr. Miller has an interest (direct as in owning the commercial property or indirect through employment of persons owning property) in the support letters. As there are only three in the packet and Mr. Miller has an interest in all three, they deserve careful scrutiny.
3. I moved to Norman in 1981. The neighborhood of which Carey Drive is a part has always been and was initially designated as residential. The subdivision of the lot in question from 100 % R-1 to a 50/50 mix of R-1 and CO was done in 1981 according to the agenda packet. A CO designation is a far cry from a C-1 designation. The driveways at 1027 and 1035 South Berry Road are appropriately spaced by City of Norman standards since they were developed as residential properties.

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OF THE CITY CLERK  
ON 10/8/21-LW

4. As you already know, if the two subject properties are rezoned to C-1 the owner can sell them instantly with the C-1 zoning intact. Only the SPUD restrictions will need to be followed. A strip mall by any other name is still a strip mall.
5. The current SPUD is identical to the one presented last year with the exception that the building is now turned around. There is really nothing new. In fact, the "new" layout could be considered worse than the one presented last year (voted down by a 6-1 vote of the planning commission). A summary of a few reasons are as follows.
  - a. The back of the building now faces South Berry Road. The aesthetics of this are obvious. The back of the building will now be located beside the sidewalk and just on the other side of the sidewalk is South Berry Road. How would the Hollywood Shopping Center look if it were turned around and moved up to West Lindsey Street?
  - b. The lighting will now more easily infiltrate the residential area.
  - c. The noise will now more easily infiltrate the residential area.
  - d. The odors will infiltrate the residential area.
  - e. The parking will now be in the front (East) of the building and the noise, odor, lights, and pollution will now be closer to the residential area.
6. The six feet of wooden fence on the North and East side of the proposed strip mall will not make any difference when the issues above are considered.

There are a host of other issues (traffic, pedestrian safety, corporate creep into a neighborhood, etc.) that can be raised and may be raised by others.

In summary, I urge you to vote not to allow the proposed development of this strip mall in a long-standing residential neighborhood.

Thank you for reading.

Sincerely,



John B. Cornwell



October 2021

City of Norman  
Planning and Community Development  
P.O. Box 370  
Norman, OK 73070

FILED IN THE OFFICE  
OF THE CITY CLERK  
ON 10/8/21-LW

~~FILED IN THE OFFICE  
OF THE CITY CLERK  
ON~~

Dear Planning Commission,

I am opposed to the rezoning which would allow for the building of a new strip mall on the North East Corner of Berry Road and Lindsey Street.

There are already too many vacant commercial structures in this area, and I want to support Norman's existing business owners in their efforts to rebuild Lindsey Street after the City of Norman installed restrictive traffic medians.

This development will add almost an acre of impervious surface that will increase the intensity of stormwater runoff directly into Imhoff Creek.

Berry and Lindsey are EMS Safety Corridors and should not be restricted.

My Name is:

JoAnn Oliver

I live at:

1618 Homeland Av

Signed:

JoAnn Oliver

*We do not need another  
empty strip mall near  
or on Lindsey St!*

October 2021

City of Norman  
Planning and Community Development  
P.O. Box 370  
Norman, OK 73070

Dear Planning Commission,

I am opposed to the rezoning which would allow for the building of a new strip mall on the North East Corner of Berry Road and Lindsey Street.

There are already too many vacant commercial structures in this area, and I want to support Norman's existing business owners in their efforts to rebuild Lindsey Street after the City of Norman installed restrictive traffic medians.

This development will add almost an acre of impervious surface that will increase the intensity of stormwater runoff directly into Imhoff Creek.

Berry and Lindsey are EMS Safety Corridors and should not be restricted.

My Name is: Linda Lockett

I live at: 910 Elm

Norman, OK 73070

Signed: [Signature]

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OF THE CITY CLERK  
ON 10/12/21-LW

The City of Norman  
Planning and Community Development  
City Clerk  
201 W. Gray, Bldg. A  
Norman, OK 73069

August 30, 2021

Re: Case PD20-10 / O-1920-58 & R-1920-115  
Re-zoning from R-1 to SPUD

I protest the re-zoning and development proceedings for the proposed building and parking lot at the Berry and Lindsey intersection.

I am advocating for my neighborhood to remain peaceful and safe for families to flourish. The proposed development is not a good fit for our neighborhood and the last thing Lindsey Street needs is another strip mall.

Thank you for your dedication to the City of Norman, it's a great place.



Name and address:

WILLIAM FARRELL ALAKA SOUZA  
873 Carey drive  
NORMAN OK  
73069

FILED IN THE OFFICE  
OF THE CITY CLERK  
ON 10/12/21

October 2021

City of Norman  
Planning and Community Development  
P.O. Box 370  
Norman, OK 73070

Dear Planning Commission,

I am opposed to the rezoning which would allow for the building of a new strip mall on the North East Corner of Berry Road and Lindsey Street.

There are already too many vacant commercial structures in this area, and I want to support Norman's existing business owners in their efforts to rebuild Lindsey Street after the City of Norman installed restrictive traffic medians.

This development will add almost an acre of impervious surface that will increase the intensity of stormwater runoff directly into Imhoff Creek.

Berry and Lindsey are EMS Safety Corridors and should not be restricted.

My Name is: William Lockett

I live at: 1720 Cherry Stone

Norman 73072

Signed: Gillian Lockett

FILED IN THE OFFICE  
OF THE CITY CLERK  
ON 10/12/21-RW

October 2021

City of Norman  
Planning and Community Development  
P.O. Box 370  
Norman, OK 73070

Dear Planning Commission,

I am opposed to the rezoning which would allow for the building of a new strip mall on the North East Corner of Berry Road and Lindsey Street.

There are already too many vacant commercial structures in this area, and I want to support Norman's existing business owners in their efforts to rebuild Lindsey Street after the City of Norman installed restrictive traffic medians.

This development will add almost an acre of impervious surface that will increase the intensity of stormwater runoff directly into Imhoff Creek.

Berry and Lindsey are EMS Safety Corridors and should not be restricted.

My Name is: Kevin & Kricket Condymerley

I live at: 1132 W. Brooks, Norman OK 73069

Signed: 

FILED IN THE OFFICE  
OF THE CITY CLERK  
ON 10/12/21

October 2021

City of Norman  
Planning and Community Development  
P.O. Box 370  
Norman, OK 73070

Dear Planning Commission,

I am opposed to the rezoning which would allow for the building of a new strip mall on the North East Corner of Berry Road and Lindsey Street.

There are already too many vacant commercial structures in this area, and I want to support Norman's existing business owners in their efforts to rebuild Lindsey Street after the City of Norman installed restrictive traffic medians.

This development will add almost an acre of impervious surface that will increase the intensity of stormwater runoff directly into Imhoff Creek.

Berry and Lindsey are EMS Safety Corridors and should not be restricted.

My Name is: Carolyn and Gary Kauley

I live at: 1628 Rosemont Drive  
Norman OK 73072

Signed: Carolyn Kauley Gary Kauley

FILED IN THE OFFICE  
OF THE CITY CLERK  
ON 10/12/21

FILED IN THE OFFICE  
OF THE CITY CLERK  
ON 10/12/21

City of Norman  
Planning Department  
201 West Gray, Building A  
Norman, OK 73069

Re: R-2122-31 and O-2122-15 (Rezoning of properties located at 1027 and 1035 South Berry Road)

Dear Planning Commission,

I am writing this letter in protest to the rezoning of the properties at 1027 and 1035 South Berry Road. I have lived in Norman since 1979. My husband and I moved into our home in Pickard Acres on Madison Street 16 years ago so that our daughter would be in a diverse school setting and neighborhood. Since that time, we have adopted 2 more children who are enjoying the diversity of our neighborhood and school and the wonderful things that living close to the university has to offer.



Our property is on the East side of Imhoff Creek, just to the Northeast of the property at Lindsey and Berry.





My concerns with the residential property becoming a strip mall are rooted in safety and environmental issues and are as follows:

- 1) Many of the properties in our neighborhood are along Imhoff Creek. My back drive, which runs along Imhoff Creek from Madison Street to Parsons, along with my detached garage are in a flood plain which was created by the concreting of Imhoff Creek. The concrete ditch is shallow on the North side of the Lindsey Street bridge and deeper and box shaped on the South side of Lindsey causing major flooding at times of torrential downpours in Central Norman. This is already an issue that residents along the creek face with our properties flooding (including basements and garages). Adding more concrete along a poorly developed drainage system for Central Norman will only add to the flood issues that current residents experience.
- 2) Another concern is increased traffic in the creek, especially if the strip mall is East facing and visible to the creek. With increased traffic in the creek comes increased littering and other crimes such as graffiti.
- 3) Increased traffic at the Berry and Lindsey intersection is another big concern for our family and neighbors. Traffic is already congested in the mornings before school and after school at this intersection. My children use the bike path on Lindsey street and my son has almost been hit twice since the beginning of this school year by drivers who are not paying attention when he has the right of way.
- 4) There are public bus stops on both the NE and SE corners of Lindsey and Berry adding to the congestion in this area with buses picking up passengers every 30 minutes. Adding another driveway/entrance to this busy corner will likely cause more issues with auto/pedestrian/biker accidents.



I urge you to vote against the proposed development of this strip mall in our wonderful neighborhood.

Thank you,

Nolita C. Morgan

# SWIFT CO

Dear Mayor & City Council,

As a resident of Ward 4 and a developer of projects throughout Norman's Core, I am in support of the proposed rezoning and development for the corner of Berry Road and Lindsey Street. This intersection is the gateway to our city to those visiting The University of Oklahoma. This project will bring new life to this corner that is desperately needed. I also believe the proposed site plan allows for a gentle transition from the busy commercial nature west of the site with the calmer residential aspect to the east of the site.

Sincerely,



Brent Swift

FILED IN THE OFFICE  
OF THE CITY CLERK  
ON 10/14/21 - EW

# TEC

## REVISED TRAFFIC IMPACT ANALYSIS

---

**Sooner Traditions Development  
Lindsey Street and Berry Road  
Norman, Oklahoma**

---

**Prepared for:  
Shops at Berry, LLC.  
October 2021**

**Prepared by:  
Traffic Engineering Consultants, Inc.**



A handwritten signature in blue ink, appearing to read "B.J. Hawkins", written over a horizontal line.

**B.J. Hawkins, P.E., PTOE  
Oklahoma P.E. # 25164  
CA # 1160**

**10 / 4 / 2021**

**Date**

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\* - Figure/Table is following page number indicated

## 1.0 INTRODUCTION

Traffic Engineering Consultants, Inc. (TEC) was retained by Shops at Berry, LLC. to conduct a traffic impact analysis (TIA) for a proposed commercial development to be constructed in Norman, Oklahoma. The study was requested to determine the effects the proposed development would have on the adjacent street system, to review the available access to the development, and to provide recommendations for improvements that may be necessary to accommodate the traffic expected to be generated by the development.

## 2.0 BACKGROUND

### 2.1 PROPOSED DEVELOPMENT

The site of the proposed development is located north of Lindsey Street and east of Berry Road as shown in **Figure 1**. The proposed development would be an approximate 10,700 square foot retail strip center. Access to the new development, as shown in **Figure 2**, is proposed via one full-access driveway on Berry Road and one right-in/right-out driveway on Lindsey Street. To properly design the proposed right-in/right-out driveway on Lindsey Street, the developer is willing to relocate the existing bus stop just to the west of its current location. The developer will coordinate with the City of Norman to determine the proper relocation of the bus stop.

### 2.2 EXISTING ROADWAY NETWORK

Lindsey Street is a four-lane divided east/west principal urban arterial west of Berry Road and a two-lane minor urban arterial east of Berry Road. It has a posted speed limit of 30 mph and carries an approximate average daily traffic (ADT) of 22,300 vehicles per day (vpd) west of Berry Road and a posted speed limit of 25 mph and carried an approximate ADT of 20,300 vpd east of Berry Road. Berry Road is a two-lane north/south minor urban arterial. It has a posted speed limit of 30 mph and carries an approximate ADT of 8,400 vpd north of Lindsey Street and 5,200 vpd south of Lindsey Street.

The intersection of Lindsey Street and Berry Road is a signalized intersection with protected/permissive "flashing yellow arrow" left turn movements on each approach and an eastbound right turn lane with a protected overlap.





FIGURE 1. Project Location Map  
Lindsey Street and Berry Road  
Norman, Oklahoma



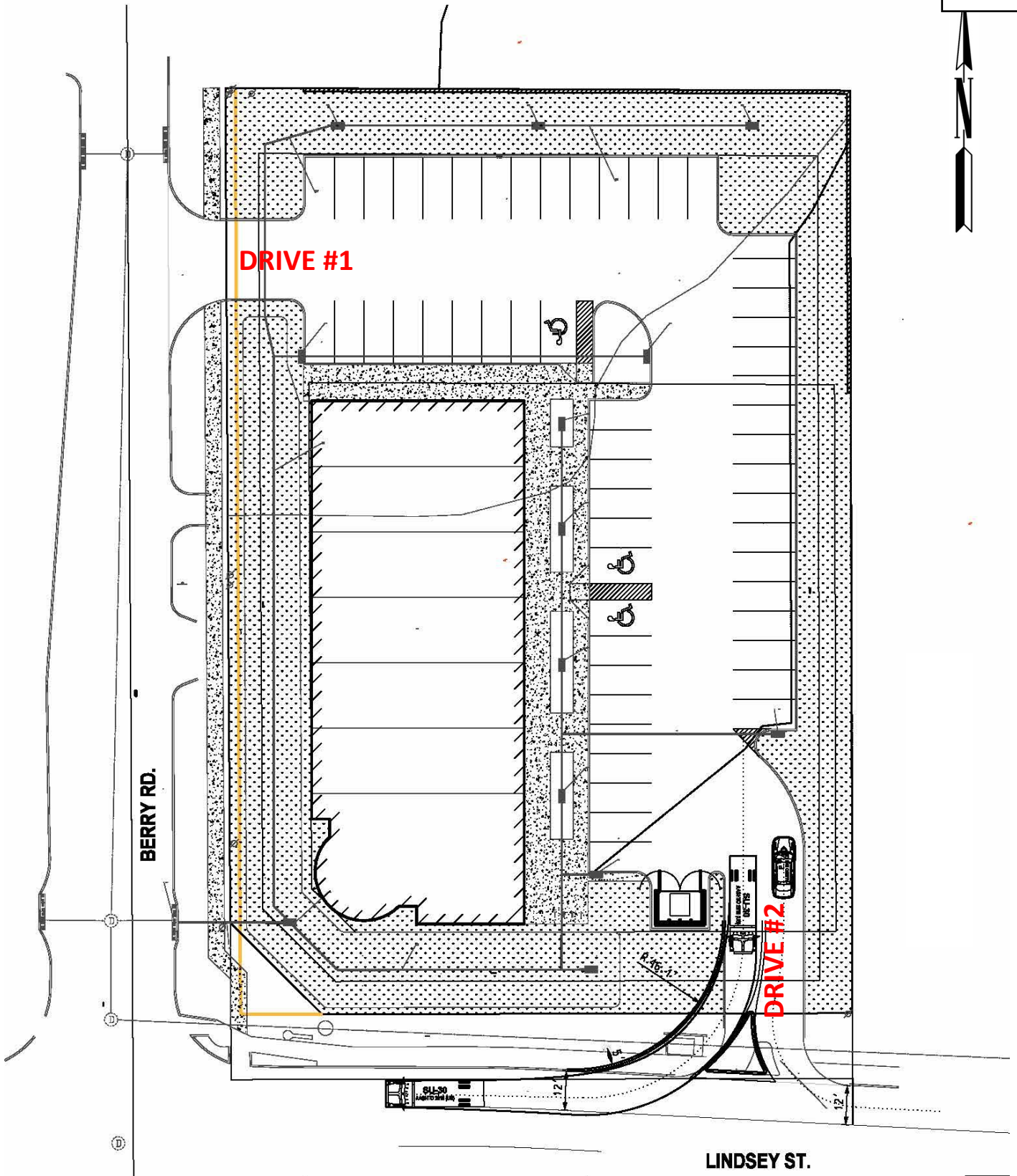


FIGURE 2. Proposed Site Plan

### 3.0 TRAFFIC DATA

#### 3.1 EXISTING TRAFFIC

Existing traffic volume data was collected adjacent to the proposed development in September of 2021 when school was in session. Twenty-four hour turning movement volumes were collected at the intersection of Lindsey Street and Berry Road which was utilized to determine the peak hour turning movement volumes as well as daily bi-directional traffic volumes on each approach of the intersection. Given the traffic characteristics in the area and the anticipated trip generation for the proposed development, the weekday peak periods would represent a “worst-case scenario” with regards to traffic impact on the surrounding roadway network. If traffic operations are acceptable during these weekday peak hours, it can be reasoned that conditions would be acceptable throughout the remainder of the day and week. The 2021 existing traffic is summarized in **Figure 3** and detailed printouts of all the traffic count data are included in the appendix.

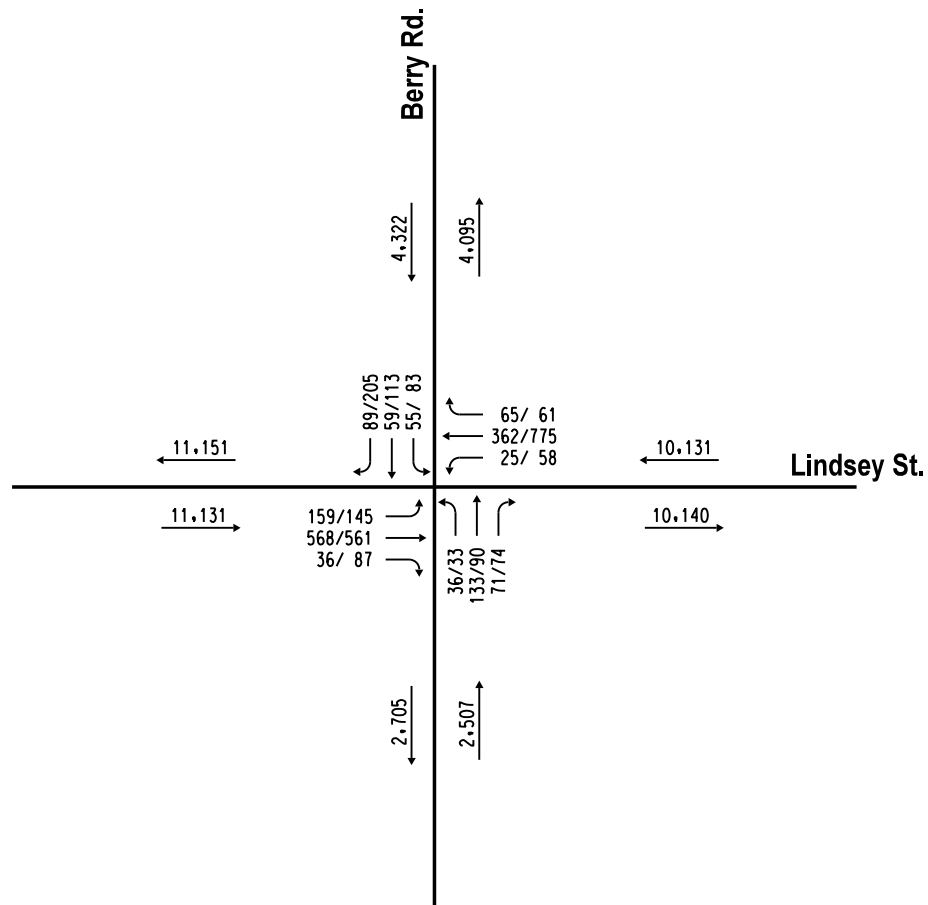
#### 3.2 FUTURE BACKGROUND TRAFFIC

The 2021 existing traffic volumes were utilized to determine the background traffic for 2023. The 2023 year was selected as a future design year the development is estimated to be completed. The background traffic was determined for the 2023 future design year by applying an average annual growth rate of 2.5% to the 2021 existing traffic volumes. The annual growth rate was provided by the City of Norman staff and represents the assumed traffic growth in addition to the projected development traffic. The 2023 future background traffic is summarized in **Figure 4**.

### 4.0 DEVELOPMENT TRAFFIC

#### 4.1 PROPOSED DEVELOPMENT TRAFFIC

To determine the effects a new development will have on an existing street system, the new or additional traffic must be projected. The latest edition of the *Trip Generation Manual*, published by the Institute of Transportation Engineers, was used to determine the amount of traffic the development is expected to generate. The report is a nationally accepted reference which provides trip rates for determining the traffic expected to be generated by different land use types.



LEGEND	
xxx/xxx	P.M. PEAK HOUR
xxx	A.M. PEAK HOUR
xxxx	= 24 HOUR VOL

FIGURE 3. 2021 Existing Traffic

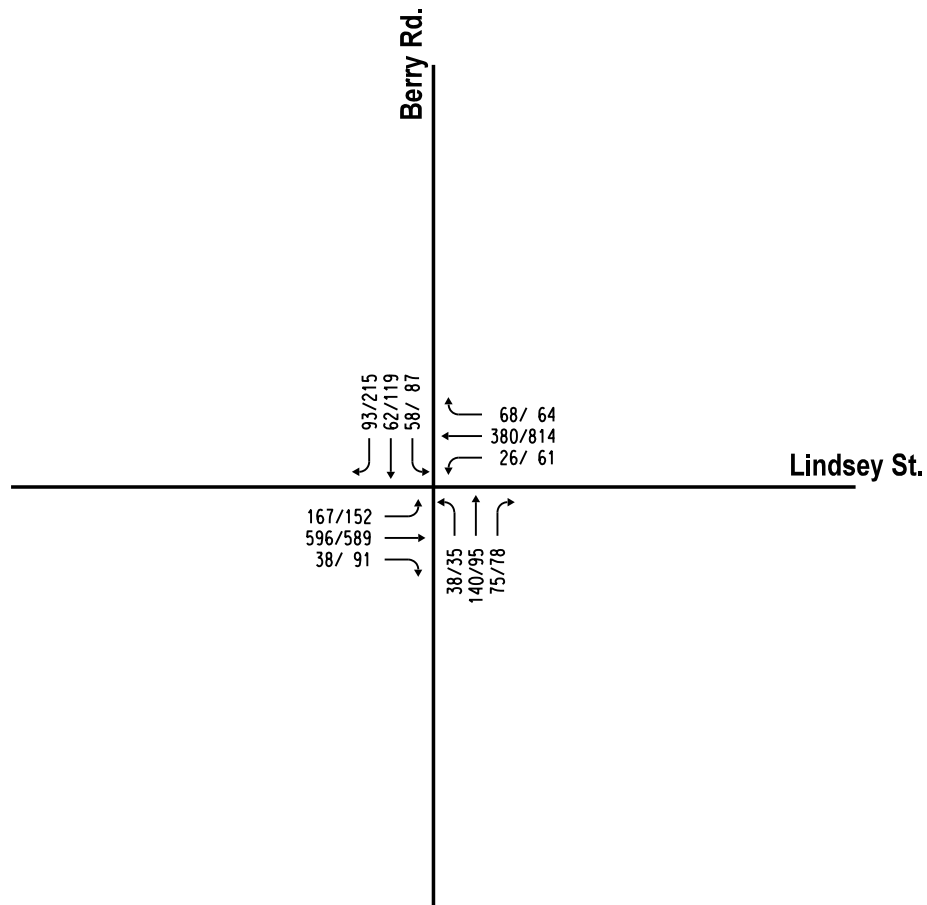


FIGURE 4. 2023 Future Background Traffic

LEGEND	
xxx/xxx	P.M. PEAK HOUR
—	A.M. PEAK HOUR



Available information was utilized regarding the anticipated land use to determine the site generated traffic. The *Shopping Center* land use category was selected to determine the trip generation for the proposed development. The exact tenant(s) for the retail strip center is currently unknown. For the purpose of this analysis, the *Shopping Center* land use category was selected to represent the retail property and encompasses a wide variety of land uses including retail, restaurants, office, and more and will appropriately represent any possible retail businesses which may develop on that property. The resulting traffic volumes projected to be generated by the proposed development once fully constructed and occupied are indicated in **Table 1**.

**TABLE 1**  
PROJECTED SITE GENERATED TRAFFIC VOLUMES

Building Type (Land Use)	ITE Land Use Code	Approximate Gross Floor Area or Other	Average Weekday Vehicle Trip Ends			Average AM Peak Hour Directional Distribution		Average AM Peak Hour Directional Volume (vph)		Average PM Peak Hour Directional Distribution		Average PM Peak Hour Directional Volume (vph)	
			Per Day	Per Peak Hour of Adjacent Street Traffic									
				One Hour Between 7am & 9am	One Hour Between 4pm & 6pm								
			(vpd)	(vph)	(vph)	IN	OUT	IN	OUT	IN	OUT	IN	OUT
Trip Rate* Shopping Center	820	(sf)  10,700	37.75  404	0.94  10	3.81  41	0.62	0.38	6	4	0.48	0.52	20	21

\* Trip Rates from "TRIP GENERATION MANUAL", 10th Ed., Volume 2: Data, Institute of Transportation Engineers.

The proposed development would be expected to generate 404 vehicle trips per day with 6 entering and 4 exiting vehicles during the a.m. peak hour and 20 entering and 21 exiting vehicles during the p.m. peak hour.

#### 4.2 DISTRIBUTION OF PROPOSED DEVELOPMENT TRAFFIC

The traffic expected to be generated by the proposed development was then distributed among the point of access and surrounding roadway network for the a.m. and p.m. peak hours. The distribution of the proposed development traffic was based on anticipated usage of the site and traffic patterns in the area which were obtained from the traffic data that was collected for this study. The directional distribution of the site generated traffic for the adjacent future development is expected to be:

- 39% to/from Lindsey Street west of the development
- 35% to/from Lindsey Street east of the development
- 16% to/from Berry Road north of the development
- 11% to/from Berry Road south of the development

The traffic expected to be generated from the proposed development is summarized in **Figure 5**.

#### 4.3 PROJECTED COMBINED TRAFFIC

The proposed development traffic was then added to the future background traffic for the 2023 design year. The 2023 projected combined traffic (2023 future background traffic + proposed development traffic) for each access point to the proposed development as well as the surrounding roadway network are summarized in **Figure 6**.

### 5.0 CAPACITY ANALYSIS

#### 5.1 METHODOLOGY

The capacity analyses were conducted using *Synchro 11*, which is a software package for modeling and optimizing traffic signal timings at signalized intersections and analyzing unsignalized intersections in accordance with the methodology of the latest edition of the *Highway Capacity Manual*. The *Highway Capacity Manual* is published by the Transportation Research Board of the National Research Council, Washington, D.C. The information has been widely accepted throughout the U.S. as a guide for defining and solving transportation challenges. The information is approved and distributed by the U.S. Department of Transportation, Federal Highway Administration.

The capacity analysis provides a measure of the amount of traffic that a given facility can accommodate. Traffic facilities generally operate poorly at or near capacity. The analysis is intended to estimate the maximum amount of traffic that can be accommodated by a facility while maintaining prescribed operational qualities. The definition of operational criteria is accomplished using levels-of-service. The concept of levels-of-service is defined as a qualitative measure and describes operational conditions in terms of such factors as speed and travel time, freedom to maneuver, traffic interruptions, comfort and convenience, and safety. Six levels-of-service are defined for each type of facility for which analysis procedures are available. They are given letter designations, from “A” to “F”, with level-of-service “A” representing the best operating conditions and level-of-service “F” the worst.

The average control delay for signalized intersections is estimated for each lane group and aggregated for each approach and for the intersection as a whole. The level-of-service for this type of traffic control is

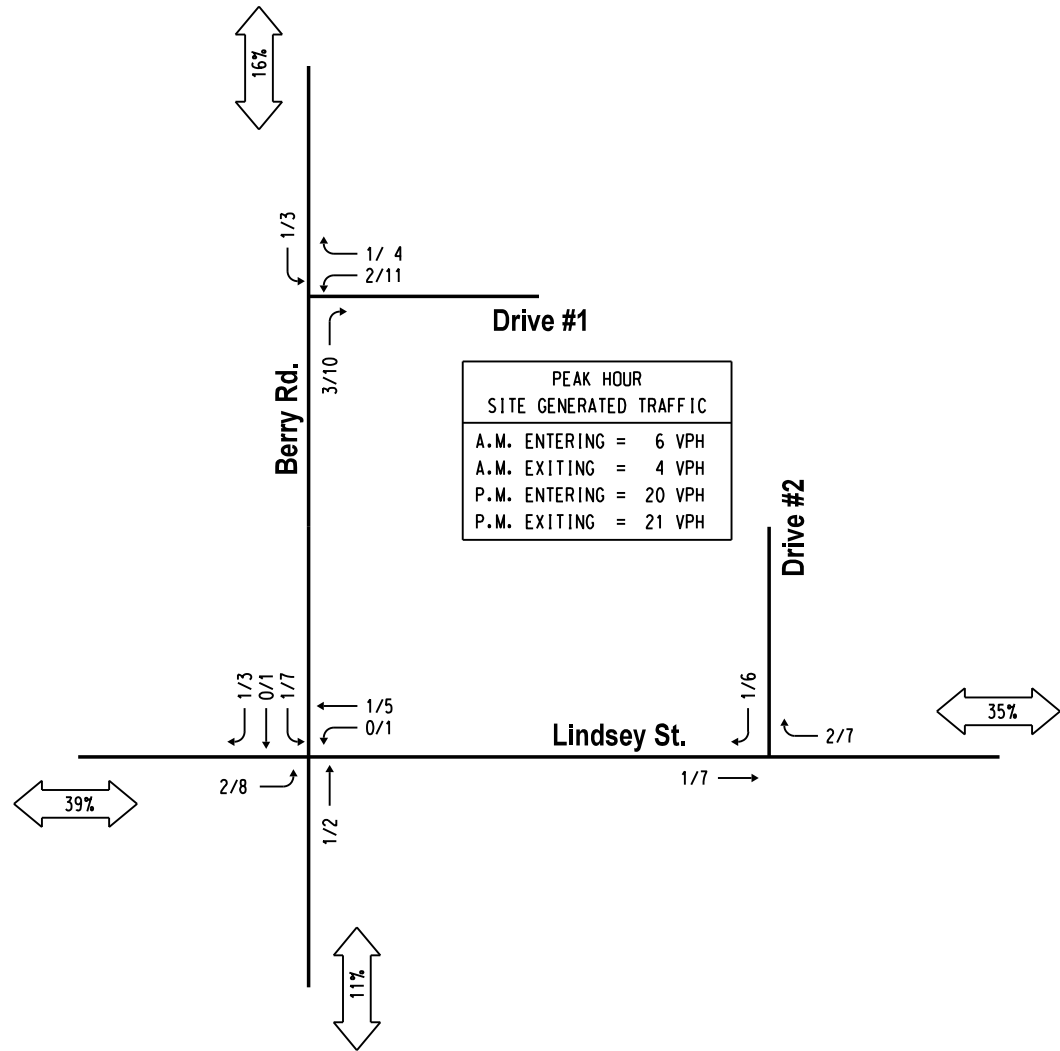
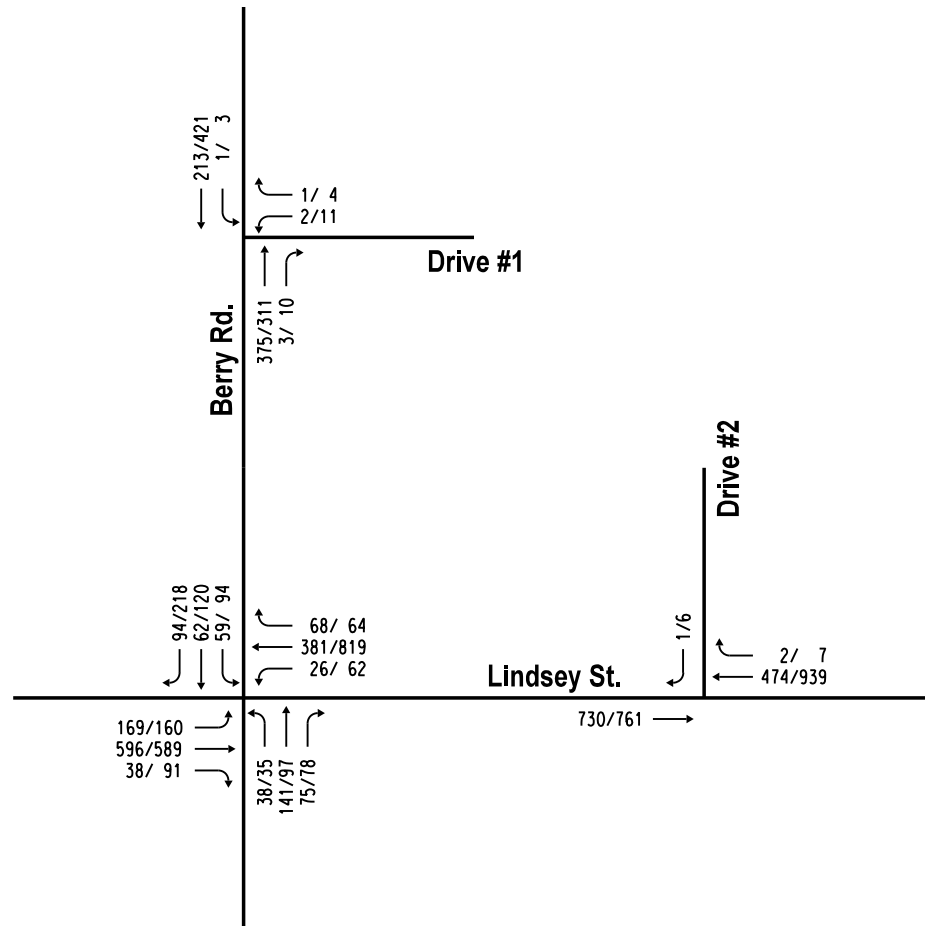


FIGURE 5. Proposed Development Traffic

LEGEND	
XXX/XXX	P.M. PEAK HOUR
---	A.M. PEAK HOUR





directly related to the control delay value. The criteria for stop controlled or unsignalized intersections have different threshold values than do those for signalized intersections. A higher level of control delay has been determined to be acceptable at a signalized intersection for the same level-of-service. The level-of-service criteria are summarized in **Table 2**. For purposes of this report an overall intersection level-of-service “D” or better and a critical approach (approach with the lowest level-of-service) level-of-service “E” or better was considered an acceptable level-of-service.

**TABLE 2**  
Level-of-Service Criteria

Level of Service	Average Delay (seconds/vehicle)		Traffic Condition
	Unsignalized	Signalized	
A	≤10	≤10	Free Flow
B	> 10 - 15	> 10 - 20	Stable Flow (slight delays)
C	> 15 - 25	> 20 - 35	Stable Flow (acceptable delays)
D	> 25 - 35	> 35 - 55	Approaching Unstable Flow (tolerable delay, occasionally wait through more than one signal cycle before proceeding)
E	> 35 - 50	> 55 - 80	Unstable Flow (intolerable delay)
F	> 50	> 80	Forced Flow (congested and queues fail to clear)

## 5.2 SCENARIOS

Capacity analyses were conducted for the a.m. and p.m. peak hours at each access point to the proposed development as well as the study intersection of Lindsey Street and Berry Road. The intersections were analyzed and reviewed under the 2021 existing traffic, 2023 future background traffic, and 2023 projected combined traffic. The existing traffic signal timing parameters were obtained from the City of Norman and utilized in the analyses to accurately model existing conditions. The results of the capacity analyses conducted are summarized in **Table 3** and the raw data sheets have been included in the appendix.

**TABLE 3**  
Intersection Capacity Analysis Results

Intersection	Type of Traffic Control	AM Peak Hour					PM Peak Hour				
		Critical Approach			Intersection		Critical Approach			Intersection	
		Approach	Delay (sec/veh)	LOS	Delay (sec/veh)	LOS	Approach	Delay (sec/veh)	LOS	Delay (sec/veh)	LOS
2021 Existing Traffic											
Lindsey Street and Berry Road	Signalized	NB	54.4	D	43.8	D	SB	67.9	E	42.4	D
2023 Future Background Traffic											
Lindsey Street and Berry Road	Signalized	EB	60.8	E	47.7	D	SB	72.9	E	45.8	D
2023 Projected Combined Traffic											
Lindsey Street and Berry Road	Signalized	EB	60.8	E	47.7	D	SB	74.7	E	46.9	D
Berry Road and Drive #1	Unsignalized/WB Stop	WB	12.4	B	0.1	A	WB	14.3	B	0.3	A
Lindsey Street and Drive #2	Unsignalized/SB Stop	SB	11.5	B	0.0	A	SB	17.9	C	0.1	A

### 5.2.1 2021 EXISTING TRAFFIC

The analyses conducted under the 2021 existing traffic indicated that the critical approach at the intersection of Lindsey Street and Berry Road currently operates at level-of-service “D” during the a.m. peak hour and level-of-service “E” during the p.m. peak hour. The intersection currently operates at an overall level-of-service “D” during the peak hours.

### 5.2.2 2023 FUTURE BACKGROUND TRAFFIC

Under the 2023 future background traffic, the intersection would be expected to continue operating at acceptable levels-of-service during the peak hours.

### 5.2.3 2023 PROJECTED COMBINED TRAFFIC

Once the proposed development traffic was added to the 2023 future background traffic, the intersection and each development drive would be expected to operate at acceptable levels-of-service during the peak hours and throughout the remainder of the day and week.

## 6.0 DRIVEWAY SPACING

In accordance with *“City of Norman Engineering Criteria for Streets, Storm Drainage, Waterlines and Sanitary Sewers”*, July 11, 2006 the following types of driveway criteria were evaluated:

- 1) Minimum spacing requirements for driveways along arterial roadways.
- 2) Corner clearance for driveways next to public road intersections

### 6.1 MINIMUM SPACING

According to the above-mentioned publication, the minimum spacing requirements for a driveway along an arterial roadway is based on the amount of traffic the development is expected to generate and the posted speed limit on the adjacent roadway which the driveways intersect. The proposed development is considered a small generator (0 to 100 peak hour trips) and Lindsey Street and Berry Road both have

posted speed limits less than 40 mph. Based on these criteria, the minimum spacing between driveways is 220 feet centerline to centerline.

Drive #1 on Berry Road has three private residential driveways within 220 feet. One driveway is the secondary access to the rear of a single-family residence and the other two driveways form a circular drive for one single-family residence. Drive #2 on Lindsey Street has two driveways spaced within 220 feet. One of the driveways is an exit-only driveway for Penny Hill Deli and the other driveway accesses a single-family residence. Due to the specific types of the existing driveways, traffic operational issues would not be anticipated as a result of the close driveway spacing.

## 6.2 CORNER CLEARANCE

According to the above-mentioned publication, the corner clearance for a driveway next to a public road intersection is based on the posted speed limit of the adjacent street which the driveway intersects and the traffic control at the intersection. The intersection of Lindsey Street and Berry Road is signalized and the posted speed limit on Lindsey Street and Berry Road is less than 40 miles per hour. Based on these criteria, the minimum required corner clearance from the edge of pavement of the intersecting street to the centerline of driveway 175 feet on Lindsey Street and Berry Road.

The centerline of Drive #1 on Berry Road is proposed to be constructed approximately 255 feet north of the edge of road of Lindsey Street. The centerline of Drive #2 on Lindsey Street is proposed to be constructed approximately 180 feet east of the edge of road of Berry Road. Therefore, both development driveways satisfy the City's minimum corner clearance requirement.

## 7.0 QUEUING ANALYSIS

Development Drive #1 is proposed to be located approximately 240 feet north of the southbound stop bar and development Drive #2 is proposed to be located approximately 160 feet east of the westbound stop bar at the intersection of Lindsey Street and Berry Road. The southbound and westbound queue lengths at the intersection of Lindsey Street and Berry Road were evaluated to determine the 95<sup>th</sup> percentile queue length to determine if the through traffic on Lindsey Street and Berry Road would queue beyond the development driveways. The 95<sup>th</sup> percentile queue is defined as the queue length of vehicles

which has only a five percent probability of being exceeded during the analysis periods and is commonly used to determine the appropriate storage length for turn lanes. The queuing analyses were conducted in Synchro SimTraffic and the results were based on the average of five sixty-minute traffic models. The results of the queuing analyses have been included in the appendix.

The queuing analysis of the southbound approach indicated the 95<sup>th</sup> percentile queue length would be 223 feet during the a.m. peak hour and 274 feet during the p.m. peak hour under the 2023 projected combined traffic scenario. Based on the results of the analyses, the southbound movement on Berry Road would not be expected to queue to or beyond Drive #1 except for a brief period during the p.m. peak hour. Drive #1 would not impact traffic operations of the through traffic on Berry Road, but the southbound queuing may increase vehicular delay of left turning vehicles exiting Drive #1 during the p.m. peak hour. The additional vehicular queuing would be contained within the development. There would be no traffic operational issues during the a.m. peak hour or throughout the remainder of the day and week.

The queuing analysis of the westbound approach indicated the 95<sup>th</sup> percentile queue length would be 156 feet during the a.m. peak hour and 178 feet during the p.m. peak hour. Based on the results of the analyses, the westbound movement on Lindsey Street would not be expected to queue to or beyond Drive #2 except for a brief period during the p.m. peak hour. Drive #2 would not impact traffic operations of the through traffic on Lindsey Street, but the westbound queuing may increase vehicular delay of right turning vehicles exiting Drive #2 during the p.m. peak hour. The additional vehicular queuing would be contained within the development. There would be no traffic operational issues during the a.m. peak hour or throughout the remainder of the day and week.

## **8.0 CONCLUSIONS**

### **8.1 SUMMARY**

TEC was requested to conduct a traffic impact analysis on a proposed commercial development in Norman, Oklahoma. Existing traffic volume data was collected adjacent to the proposed development. The existing traffic was utilized to determine the background traffic for 2023 by applying an average annual growth rate of 2.5% to the 2021 existing traffic volumes. The 2023 design period was selected as

the year the development is projected to be completed. The proposed development traffic was then determined and added to the 2023 future background traffic for conducting the reviews and analyses.

The analyses conducted under the 2021 existing traffic and 2023 future background traffic indicated that the intersection of Lindsey Street and Berry Road currently operates and would be expected to continue operating at acceptable levels-of-service during the a.m. and p.m. peak hours. Once the proposed development traffic was added to the 2023 future background traffic, each study intersection and the development driveway would be expected to continue operating at an acceptable level-of-service during the peak hours and throughout the remainder of the day and week.

## 8.2 RECOMMENDATIONS

The small amount of traffic projected to be generated by the development would have minimal effects on the surrounding roadway network. The analyses indicate the additional development traffic would not increase vehicle delay during the a.m. peak hour, when retail shops are typically closed, and would only increase vehicle delay by less than two seconds during the p.m. peak hour. Based on the results of the analyses conducted, no traffic control or geometric roadway improvements are necessary as a result of the proposed development for traffic to operate at an acceptable level-of-service through 2023 when the proposed development is estimated to be completed.





## ***TRAFFIC DATA***

# Lindsey St. & Berry Rd. - TMC

Wed Sep 1, 2021

Full Length (12 AM-12 AM (+1))

All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks)

All Movements

ID: 868249, Location: 35.203926, -97.459005



Provided by: Traffic Engineering Consultants, Inc.  
6000 S. Western Ave, Suite 300, Oklahoma City, OK, 73139, US

Leg Direction	North Southbound					East Westbound					South Northbound					West Eastbound					
Time	L	T	R	U	App	L	T	R	U	App	L	T	R	U	App	L	T	R	U	App	Int
2021-09-01 12:00AM	2	5	3	0	10	0	13	2	0	15	1	4	3	0	8	5	24	0	0	29	62
12:15AM	2	1	0	0	3	3	21	3	0	27	3	0	2	0	5	2	27	2	0	31	66
12:30AM	2	3	0	0	5	0	17	0	0	17	1	2	0	0	3	2	28	0	0	30	55
12:45AM	0	1	3	0	4	0	12	2	0	14	0	0	1	0	1	5	20	1	0	26	45
Hourly Total	6	10	6	0	22	3	63	7	0	73	5	6	6	0	17	14	99	3	0	116	228
1:00AM	1	2	1	0	4	0	8	0	0	8	0	1	1	0	2	2	10	0	0	12	26
1:15AM	1	0	2	0	3	0	13	1	0	14	0	0	0	0	0	1	13	0	0	14	31
1:30AM	0	0	1	0	1	1	8	0	0	9	0	0	2	0	2	1	12	0	0	13	25
1:45AM	1	3	0	0	4	0	10	0	0	10	1	0	0	0	1	0	5	1	0	6	21
Hourly Total	3	5	4	0	12	1	39	1	0	41	1	1	3	0	5	4	40	1	0	45	103
2:00AM	0	2	3	0	5	0	4	0	0	4	0	1	0	0	1	4	9	0	0	13	23
2:15AM	0	0	2	0	2	0	6	3	0	9	0	0	0	0	0	1	9	1	0	11	22
2:30AM	0	1	3	0	4	0	4	0	0	4	0	0	0	0	0	1	7	0	0	8	16
2:45AM	0	0	2	0	2	0	3	2	0	5	0	0	0	0	0	0	4	2	0	6	13
Hourly Total	0	3	10	0	13	0	17	5	0	22	0	1	0	0	1	6	29	3	0	38	74
3:00AM	0	1	1	0	2	0	7	0	0	7	0	0	0	0	0	1	5	0	0	6	15
3:15AM	1	0	0	0	1	0	9	0	0	9	0	0	1	0	1	0	2	0	0	2	13
3:30AM	0	1	1	0	2	0	4	0	0	4	1	1	0	0	2	0	3	0	0	3	11
3:45AM	0	0	2	0	2	0	3	1	0	4	0	0	0	0	0	1	6	0	0	7	13
Hourly Total	1	2	4	0	7	0	23	1	0	24	1	1	1	0	3	2	16	0	0	18	52
4:00AM	1	0	0	0	1	0	7	0	0	7	0	0	1	0	1	0	5	0	0	5	14
4:15AM	1	0	2	0	3	1	12	0	0	13	0	1	1	0	2	0	10	0	0	10	28
4:30AM	4	0	1	0	5	0	10	0	0	10	0	0	5	0	5	0	9	0	0	9	29
4:45AM	1	1	3	0	5	0	16	2	0	18	0	0	2	0	2	0	7	0	0	7	32
Hourly Total	7	1	6	0	14	1	45	2	0	48	0	1	9	0	10	0	31	0	0	31	103
5:00AM	1	0	1	0	2	0	15	0	0	15	0	0	2	0	2	2	11	1	0	14	33
5:15AM	4	1	0	0	5	0	13	2	0	15	0	2	2	0	4	2	25	0	0	27	51
5:30AM	3	0	3	0	6	0	24	2	0	26	3	3	1	0	7	4	25	0	0	29	68
5:45AM	5	2	5	0	12	0	38	4	0	42	3	2	0	0	5	4	26	1	0	31	90
Hourly Total	13	3	9	0	25	0	90	8	0	98	6	7	5	0	18	12	87	2	0	101	242
6:00AM	3	2	5	0	10	0	32	3	0	35	2	5	4	0	11	6	30	2	0	38	94
6:15AM	3	2	3	0	8	0	44	1	0	45	1	6	5	0	12	2	36	2	0	40	105
6:30AM	5	3	4	0	12	3	67	3	0	73	3	10	0	0	13	5	31	5	0	41	139
6:45AM	9	4	14	0	27	2	75	5	0	82	5	16	2	0	23	16	67	3	0	86	218
Hourly Total	20	11	26	0	57	5	218	12	0	235	11	37	11	0	59	29	164	12	0	205	556
7:00AM	9	3	11	0	23	2	85	13	0	100	11	20	6	0	37	18	65	6	0	89	249
7:15AM	5	14	13	0	32	1	86	10	0	97	6	24	8	0	38	31	73	3	1	108	275
7:30AM	22	11	35	1	69	5	118	18	0	141	12	23	5	0	40	40	115	9	0	164	416

Leg Direction	North Southbound					East Westbound					South Northbound					West Eastbound					Item 16.
Time	L	T	R	U	App	L	T	R	U	App	L	T	R	U	App	L	T	R	U	App	Int
7:45AM	21	14	24	2	61	6	87	16	0	109	9	26	17	0	52	52	152	6	0	210	432
Hourly Total	57	42	83	3	185	14	376	57	0	447	38	93	36	0	167	141	405	24	1	571	1370
8:00AM	10	11	19	1	41	5	119	18	0	142	9	29	21	0	59	32	124	11	0	167	409
8:15AM	11	13	22	1	47	6	74	12	0	92	9	34	11	0	54	33	145	10	0	188	381
8:30AM	13	21	24	0	58	8	82	19	0	109	9	44	22	0	75	42	147	9	0	198	440
8:45AM	29	30	33	0	92	5	91	12	0	108	4	41	13	0	58	32	117	11	0	160	418
Hourly Total	63	75	98	2	238	24	366	61	0	451	31	148	67	0	246	139	533	41	0	713	1648
9:00AM	16	10	30	0	56	3	69	10	0	82	9	25	17	0	51	31	148	7	0	186	375
9:15AM	11	10	26	0	47	3	105	19	0	127	7	19	16	0	42	28	94	4	0	126	342
9:30AM	7	9	27	0	43	6	128	14	0	148	4	12	9	0	25	30	105	7	0	142	358
9:45AM	13	21	30	0	64	6	93	11	0	110	4	18	8	0	30	31	114	10	0	155	359
Hourly Total	47	50	113	0	210	18	395	54	0	467	24	74	50	0	148	120	461	28	0	609	1434
10:00AM	11	9	20	0	40	7	93	18	0	118	5	13	12	0	30	44	116	4	0	164	352
10:15AM	13	9	30	0	52	12	91	14	0	117	8	21	10	0	39	28	106	11	0	145	353
10:30AM	16	23	24	0	63	10	133	19	0	162	5	16	6	0	27	29	97	12	0	138	390
10:45AM	12	17	23	1	53	4	115	14	1	134	6	19	12	0	37	29	96	7	0	132	356
Hourly Total	52	58	97	1	208	33	432	65	1	531	24	69	40	0	133	130	415	34	0	579	1451
11:00AM	22	19	24	1	66	3	101	11	0	115	5	17	10	0	32	24	114	10	0	148	361
11:15AM	10	11	30	0	51	8	106	12	0	126	7	22	12	0	41	39	102	15	0	156	374
11:30AM	19	32	27	0	78	9	148	13	0	170	20	25	7	0	52	35	105	16	0	156	456
11:45AM	19	19	41	1	80	8	145	21	0	174	11	25	10	0	46	41	102	17	0	160	460
Hourly Total	70	81	122	2	275	28	500	57	0	585	43	89	39	0	171	139	423	58	0	620	1651
12:00PM	15	26	54	0	95	10	142	16	0	168	10	21	18	0	49	39	134	19	0	192	504
12:15PM	16	18	37	0	71	9	125	9	0	143	11	18	16	0	45	43	145	21	0	209	468
12:30PM	17	25	40	0	82	14	178	16	0	208	9	21	19	0	49	53	124	24	0	201	540
12:45PM	23	22	40	0	85	11	123	17	0	151	8	20	19	0	47	45	140	18	0	203	486
Hourly Total	71	91	171	0	333	44	568	58	0	670	38	80	72	0	190	180	543	82	0	805	1998
1:00PM	29	22	44	0	95	0	101	13	1	115	7	17	7	0	31	44	140	7	0	191	432
1:15PM	21	17	45	0	83	11	121	23	0	155	8	15	17	0	40	31	109	13	0	153	431
1:30PM	6	27	38	0	71	12	177	19	1	209	13	11	12	0	36	26	110	13	0	149	465
1:45PM	17	17	32	0	66	7	175	19	0	201	7	14	10	0	31	38	128	12	0	178	476
Hourly Total	73	83	159	0	315	30	574	74	2	680	35	57	46	0	138	139	487	45	0	671	1804
2:00PM	14	18	31	1	64	7	113	12	1	133	10	12	8	0	30	26	115	14	0	155	382
2:15PM	18	27	29	0	74	7	126	16	1	150	8	15	11	0	34	44	125	16	0	185	443
2:30PM	18	24	44	0	86	11	150	14	1	176	9	17	13	0	39	43	129	13	0	185	486
2:45PM	19	25	46	0	90	8	179	16	0	203	10	22	16	0	48	30	118	16	0	164	505
Hourly Total	69	94	150	1	314	33	568	58	3	662	37	66	48	0	151	143	487	59	0	689	1816
3:00PM	26	30	49	0	105	15	202	17	1	235	4	13	8	0	25	30	108	9	0	147	512
3:15PM	15	27	36	0	78	13	164	15	0	192	11	19	4	0	34	27	138	12	0	177	481
3:30PM	12	21	44	1	78	14	165	17	0	196	16	26	15	0	57	35	124	16	0	175	506
3:45PM	31	18	30	1	80	11	157	22	0	190	13	30	14	0	57	40	139	16	0	195	522
Hourly Total	84	96	159	2	341	53	688	71	1	813	44	88	41	0	173	132	509	53	0	694	2021
4:00PM	31	37	31	0	99	7	148	18	0	173	5	24	18	0	47	32	132	20	0	184	503
4:15PM	23	37	57	1	118	18	142	8	1	169	4	15	16	0	35	34	130	14	0	178	508
4:30PM	22	26	53	0	101	12	196	18	0	226	11	23	12	0	46	33	140	26	0	199	508

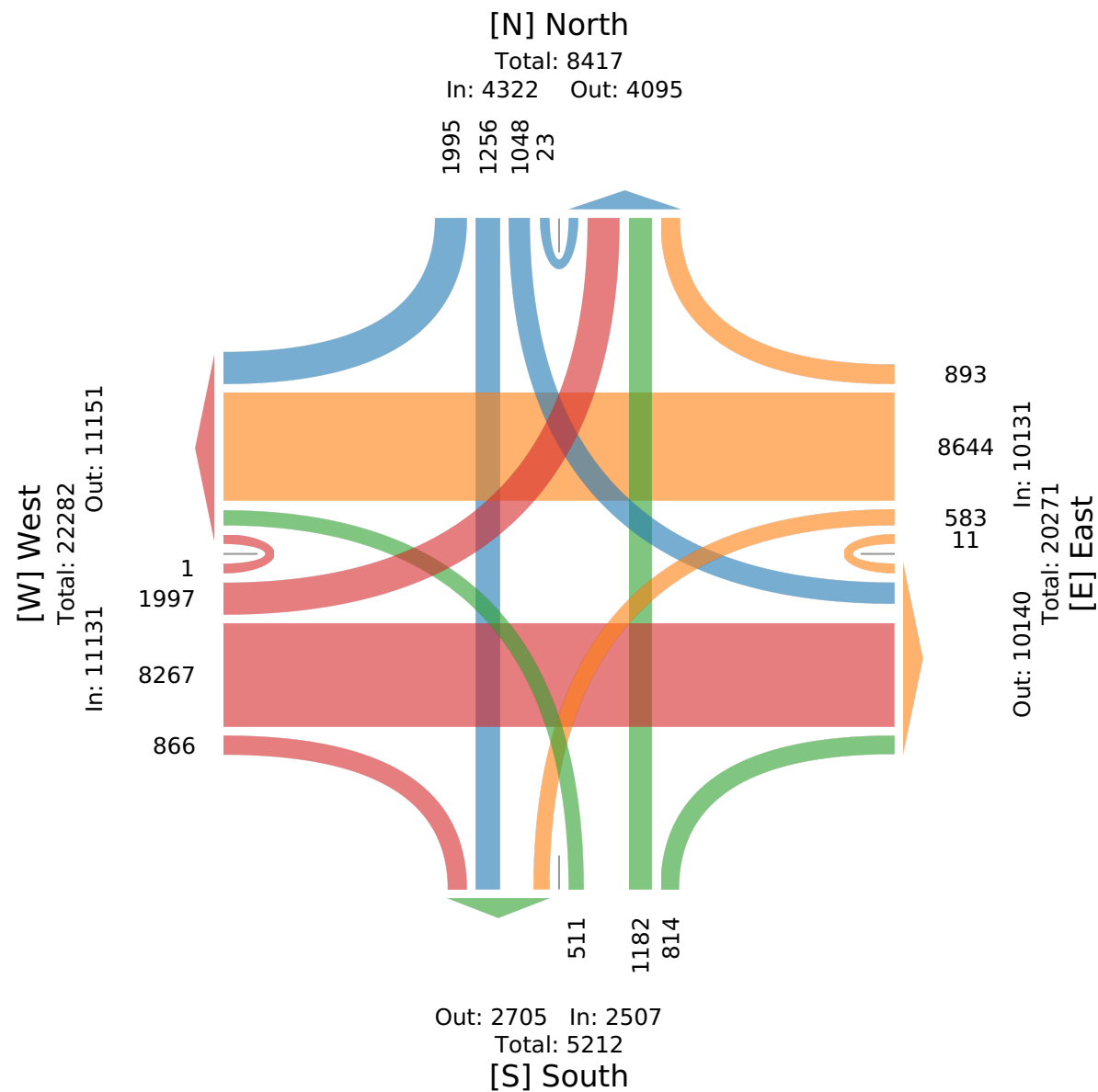
Leg Direction	North Southbound					East Westbound					South Northbound					West Eastbound					Item 16.	
Time	L	T	R	U	App	L	T	R	U	App	L	T	R	U	App	L	T	R	U	App	Int	
4:45PM	20	28	60	0	108	23	188	13	1	225	9	28	22	0	59	37	136	23	0	196	588	
Hourly Total	96	128	201	1	426	60	674	57	2	793	29	90	68	0	187	136	538	83	0	757	2163	
5:00PM	23	33	40	3	99	11	200	20	0	231	6	19	24	0	49	43	134	17	0	194	573	
5:15PM	18	26	52	0	96	12	191	10	0	213	7	20	16	0	43	32	151	21	0	204	556	
5:30PM	27	31	32	1	91	16	191	7	0	214	10	16	15	0	41	31	147	27	0	205	551	
5:45PM	9	36	40	0	85	13	166	11	1	191	4	24	20	0	48	38	147	21	0	206	530	
Hourly Total	77	126	164	4	371	52	748	48	1	849	27	79	75	0	181	144	579	86	0	809	2210	
6:00PM	11	17	40	2	70	23	185	20	0	228	10	16	21	0	47	35	133	27	0	195	540	
6:15PM	15	32	34	1	82	10	143	12	0	165	14	17	19	0	50	21	152	23	0	196	493	
6:30PM	10	27	25	1	63	10	137	18	0	165	5	9	8	0	22	34	137	20	0	191	441	
6:45PM	14	15	34	0	63	7	131	9	0	147	10	20	13	0	43	35	150	18	0	203	456	
Hourly Total	50	91	133	4	278	50	596	59	0	705	39	62	61	0	162	125	572	88	0	785	1930	
7:00PM	10	21	19	0	50	11	153	8	0	172	6	12	12	0	30	21	136	17	0	174	426	
7:15PM	8	14	21	0	43	7	129	4	0	140	7	11	18	0	36	20	149	9	0	178	397	
7:30PM	15	21	26	1	63	11	112	5	0	128	7	11	13	0	31	21	118	21	0	160	382	
7:45PM	14	20	29	1	64	10	112	8	0	130	10	18	10	0	38	24	105	13	0	142	374	
Hourly Total	47	76	95	2	220	39	506	25	0	570	30	52	53	0	135	86	508	60	0	654	1579	
8:00PM	13	18	25	0	56	12	116	14	0	142	6	6	13	0	25	30	122	8	0	160	383	
8:15PM	12	12	20	0	44	9	122	15	0	146	9	20	13	0	42	16	136	15	0	167	399	
8:30PM	13	18	18	0	49	8	105	10	1	124	4	10	4	0	18	24	113	15	0	152	343	
8:45PM	8	19	14	0	41	12	107	15	0	134	7	10	7	0	24	19	138	13	0	170	369	
Hourly Total	46	67	77	0	190	41	450	54	1	546	26	46	37	0	109	89	509	51	0	649	1494	
9:00PM	15	10	16	0	41	10	105	12	0	127	6	10	7	0	23	14	105	10	0	129	320	
9:15PM	15	14	19	1	49	11	98	7	0	116	6	4	8	0	18	11	103	7	0	121	304	
9:30PM	9	10	12	0	31	5	96	8	0	109	2	4	3	0	9	10	97	10	0	117	266	
9:45PM	10	7	15	0	32	2	66	4	0	72	0	3	8	0	11	8	85	6	0	99	214	
Hourly Total	49	41	62	1	153	28	365	31	0	424	14	21	26	0	61	43	390	33	0	466	1104	
10:00PM	7	7	9	0	23	6	68	5	0	79	0	3	2	0	5	6	72	5	0	83	190	
10:15PM	9	3	9	0	21	3	35	3	0	41	2	3	2	0	7	9	81	4	0	94	163	
10:30PM	3	1	4	0	8	4	49	5	0	58	2	2	3	0	7	5	70	3	0	78	151	
10:45PM	6	1	11	0	18	3	56	7	0	66	0	3	5	0	8	9	55	0	0	64	156	
Hourly Total	25	12	33	0	70	16	208	20	0	244	4	11	12	0	27	29	278	12	0	319	660	
11:00PM	6	3	2	0	11	5	51	2	0	58	0	1	2	0	3	4	43	0	0	47	119	
11:15PM	8	3	4	0	15	1	26	2	0	29	2	1	2	0	5	6	55	0	0	61	110	
11:30PM	5	3	3	0	11	3	39	2	0	44	2	0	2	0	4	3	38	5	0	46	105	
11:45PM	3	1	4	0	8	1	19	2	0	22	0	1	2	0	3	2	28	3	0	33	66	
Hourly Total	22	10	13	0	45	10	135	8	0	153	4	3	8	0	15	15	164	8	0	187	400	
Total	1048	1256	1995	23	4322	583	8644	893	11	10131	511	1182	814	0	2507	1997	8267	866	1	11131	28091	
% Approach	24.2%	29.1%	46.2%	0.5%	-	5.8%	85.3%	8.8%	0.1%	-	20.4%	47.1%	32.5%	0%	-	17.9%	74.3%	7.8%	0%	-	-	
% Total	3.7%	4.5%	7.1%	0.1%	15.4%	2.1%	30.8%	3.2%	0%	36.1%	1.8%	4.2%	2.9%	0%	8.9%	7.1%	29.4%	3.1%	0%	39.6%	-	
Lights	1041	1248	1971	23	4283	577	8534	889	11	10011	503	1165	803	0	2471	1969	8152	857	1	10979	27744	
% Lights	99.3%	99.4%	98.8%	100%	99.1%	99.0%	98.7%	99.6%	100%	98.8%	98.4%	98.6%	98.6%	0%	98.6%	98.6%	98.6%	99.0%	100%	98.6%	98.8%	
Articulated Trucks	0	0	0	0	0	2	18	1	0	21	0	0	2	0	2	2	25	0	0	27	50	
% Articulated Trucks	0%	0%	0%	0%	0%	0.3%	0.2%	0.1%	0%	0.2%	0%	0%	0.2%	0%	0.1%	0.1%	0.3%	0%	0%	0.2%		
Buses and Single-Unit Trucks	7	8	24	0	39	4	92	3	0	99	8	17	9	0	34	26	90	9	0	125	418	

Leg Direction	North Southbound					East Westbound					South Northbound					West Eastbound					Item 16.	
Time	L	T	R	U	App	L	T	R	U	App	L	T	R	U	App	L	T	R	U	App	Int	
% Buses and Single-Unit Trucks		0.7%	0.6%	1.2%	0%	0.9%	0.7%	1.1%	0.3%	0%	1.0%	1.6%	1.4%	1.1%	0%	1.4%	1.3%	1.1%	1.0%	0%	1.1%	1.1%

\*L: Left, R: Right, T: Thru, U: U-Turn

Lindsey St. & Berry Rd. - TMC  
Wed Sep 1, 2021  
Full Length (12 AM-12 AM (+1))  
All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks)  
All Movements  
ID: 868249, Location: 35.203926, -97.459005

Provided by: Traffic Engineering Consultants, Inc.  
6000 S. Western Ave, Suite 300, Oklahoma City, OK, 73139, US



# Lindsey St. & Berry Rd. - TMC

Wed Sep 1, 2021

AM Peak (7:45 AM - 8:45 AM)

All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks)

All Movements

ID: 868249, Location: 35.203926, -97.459005



Provided by: Traffic Engineering Consultants, Inc.  
6000 S. Western Ave, Suite 300, Oklahoma City, OK, 73139, US

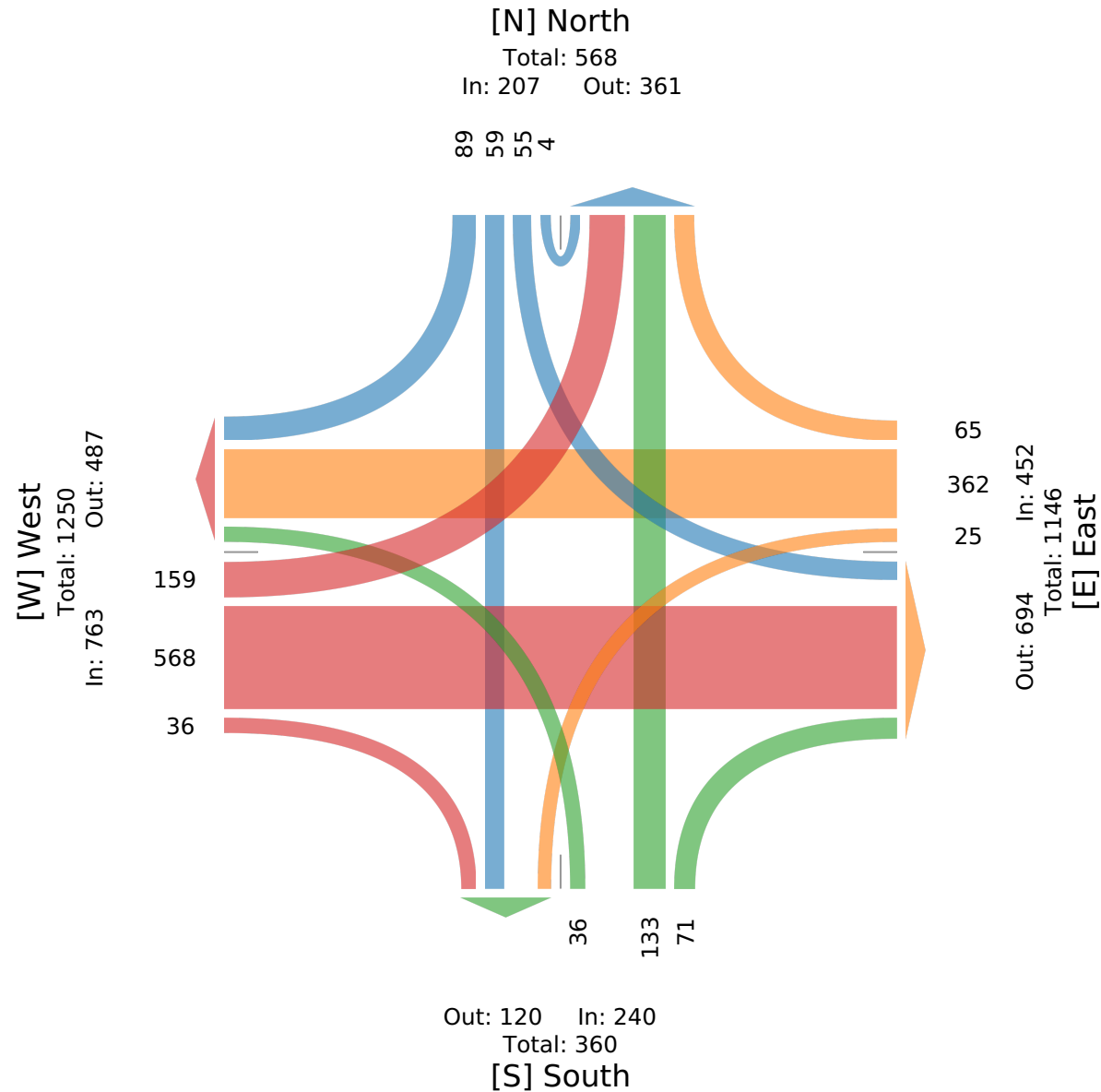
Leg Direction	North Southbound					East Westbound					South Northbound					West Eastbound					
Time	L	T	R	U	App	L	T	R	U	App	L	T	R	U	App	L	T	R	U	App	Int
2021-09-01 7:45AM	21	14	24	2	61	6	87	16	0	109	9	26	17	0	52	52	152	6	0	210	432
8:00AM	10	11	19	1	41	5	119	18	0	142	9	29	21	0	59	32	124	11	0	167	409
8:15AM	11	13	22	1	47	6	74	12	0	92	9	34	11	0	54	33	145	10	0	188	381
8:30AM	13	21	24	0	58	8	82	19	0	109	9	44	22	0	75	42	147	9	0	198	440
<b>Total</b>	55	59	89	4	207	25	362	65	0	452	36	133	71	0	240	159	568	36	0	763	1662
<b>% Approach</b>	26.6%	28.5%	43.0%	1.9%	-	5.5%	80.1%	14.4%	0%	-	15.0%	55.4%	29.6%	0%	-	20.8%	74.4%	4.7%	0%	-	-
<b>% Total</b>	3.3%	3.5%	5.4%	0.2%	12.5%	1.5%	21.8%	3.9%	0%	27.2%	2.2%	8.0%	4.3%	0%	14.4%	9.6%	34.2%	2.2%	0%	45.9%	-
<b>PHF</b>	0.655	0.702	0.927	0.500	0.848	0.781	0.761	0.855	-	0.796	1.000	0.756	0.807	-	0.800	0.764	0.934	0.818	-	0.908	0.944
<b>Lights</b>	55	59	89	4	207	24	355	64	0	443	36	133	71	0	240	157	553	36	0	746	1636
<b>% Lights</b>	100%	100%	100%	100%	100%	96.0%	98.1%	98.5%	0%	98.0%	100%	100%	100%	0%	100%	98.7%	97.4%	100%	0%	97.8%	98.4%
<b>Articulated Trucks</b>	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	3	0	0	3	4
<b>% Articulated Trucks</b>	0%	0%	0%	0%	0%	0%	0.3%	0%	0%	0.2%	0%	0%	0%	0%	0%	0%	0.5%	0%	0%	0.4%	0.2%
<b>Buses and Single-Unit Trucks</b>	0	0	0	0	0	1	6	1	0	8	0	0	0	0	0	2	12	0	0	14	22
<b>% Buses and Single-Unit Trucks</b>	0%	0%	0%	0%	0%	4.0%	1.7%	1.5%	0%	1.8%	0%	0%	0%	0%	0%	1.3%	2.1%	0%	0%	1.8%	1.3%

\* L: Left, R: Right, T: Thru, U: U-Turn



Lindsey St. & Berry Rd. - TMC  
Wed Sep 1, 2021  
AM Peak (7:45 AM - 8:45 AM)  
All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks)  
All Movements  
ID: 868249, Location: 35.203926, -97.459005

Provided by: Traffic Engineering Consultants, Inc.  
6000 S. Western Ave, Suite 300, Oklahoma City, OK, 73139, US



# Lindsey St. & Berry Rd. - TMC

Wed Sep 1, 2021

Midday Peak (12 PM - 1 PM)

All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks)

All Movements

ID: 868249, Location: 35.203926, -97.459005



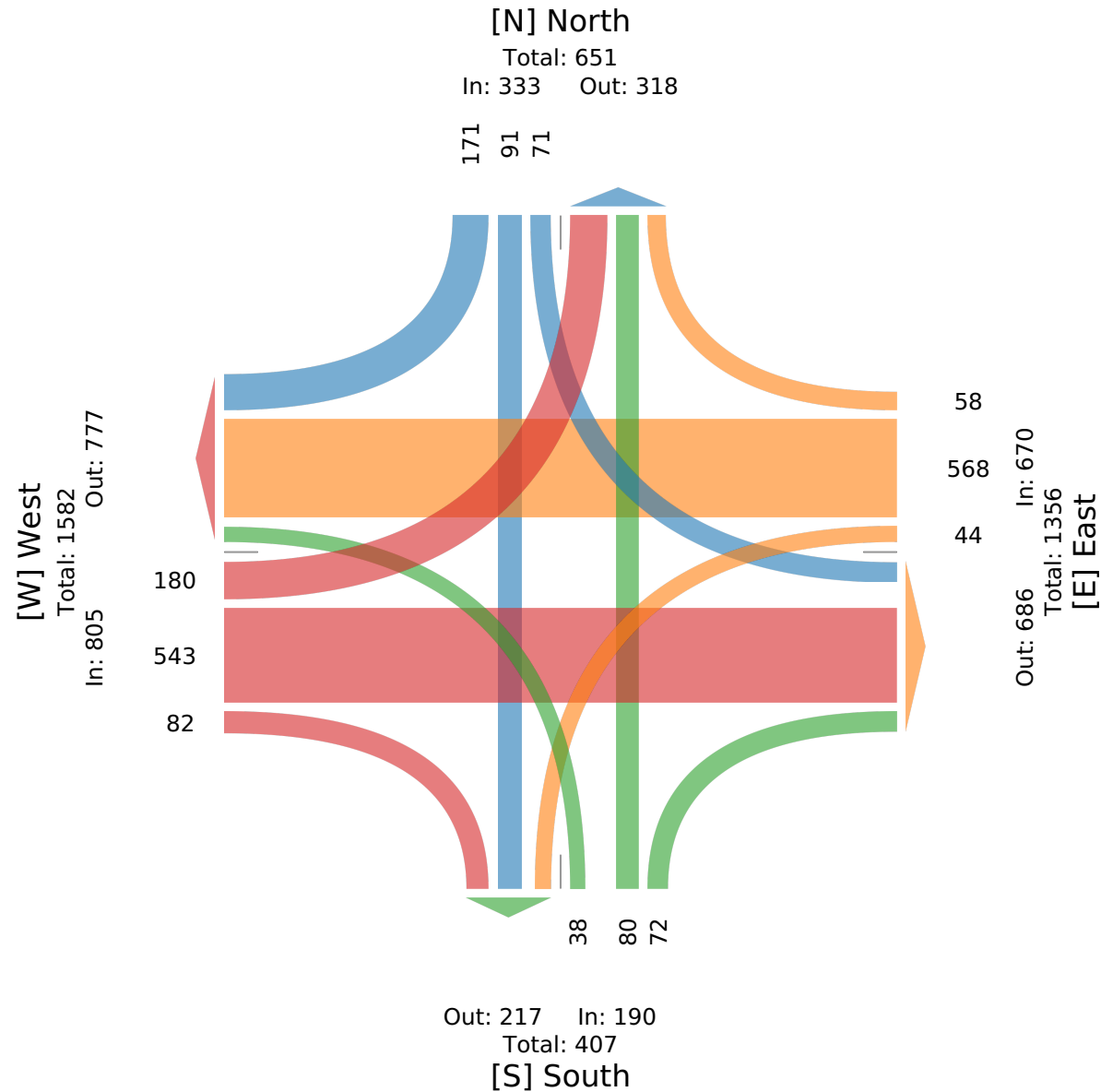
Provided by: Traffic Engineering Consultants, Inc.  
6000 S. Western Ave, Suite 300, Oklahoma City, OK, 73139, US

Leg Direction	North Southbound					East Westbound					South Northbound					West Eastbound					
Time	L	T	R	U	App	L	T	R	U	App	L	T	R	U	App	L	T	R	U	App	Int
2021-09-01 12:00PM	15	26	54	0	95	10	142	16	0	168	10	21	18	0	49	39	134	19	0	192	504
12:15PM	16	18	37	0	71	9	125	9	0	143	11	18	16	0	45	43	145	21	0	209	468
12:30PM	17	25	40	0	82	14	178	16	0	208	9	21	19	0	49	53	124	24	0	201	540
12:45PM	23	22	40	0	85	11	123	17	0	151	8	20	19	0	47	45	140	18	0	203	486
<b>Total</b>	71	91	171	0	333	44	568	58	0	670	38	80	72	0	190	180	543	82	0	805	1998
<b>% Approach</b>	21.3%	27.3%	51.4%	0%	-	6.6%	84.8%	8.7%	0%	-	20.0%	42.1%	37.9%	0%	-	22.4%	67.5%	10.2%	0%	-	-
<b>% Total</b>	3.6%	4.6%	8.6%	0%	16.7%	2.2%	28.4%	2.9%	0%	33.5%	1.9%	4.0%	3.6%	0%	9.5%	9.0%	27.2%	4.1%	0%	40.3%	-
<b>PHF</b>	0.772	0.875	0.792	-	0.876	0.786	0.798	0.853	-	0.805	0.864	0.952	0.947	-	0.969	0.849	0.936	0.854	-	0.963	0.925
<b>Lights</b>	71	90	168	0	329	43	559	58	0	660	37	79	70	0	186	178	536	80	0	794	1969
<b>% Lights</b>	100%	98.9%	98.2%	0%	98.8%	97.7%	98.4%	100%	0%	98.5%	97.4%	98.8%	97.2%	0%	97.9%	98.9%	98.7%	97.6%	0%	98.6%	98.5%
<b>Articulated Trucks</b>	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	2
<b>% Articulated Trucks</b>	0%	0%	0%	0%	0%	0%	0.2%	0%	0%	0.1%	0%	0%	0%	0%	0%	0%	0.2%	0%	0%	0.1%	0.1%
<b>Buses and Single-Unit Trucks</b>	0	1	3	0	4	1	8	0	0	9	1	1	2	0	4	2	6	2	0	10	27
<b>% Buses and Single-Unit Trucks</b>	0%	1.1%	1.8%	0%	1.2%	2.3%	1.4%	0%	0%	1.3%	2.6%	1.3%	2.8%	0%	2.1%	1.1%	1.1%	2.4%	0%	1.2%	1.4%

\* L: Left, R: Right, T: Thru, U: U-Turn

Lindsey St. & Berry Rd. - TMC  
Wed Sep 1, 2021  
Midday Peak (12 PM - 1 PM)  
All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks)  
All Movements  
ID: 868249, Location: 35.203926, -97.459005

Provided by: Traffic Engineering Consultants, Inc.  
6000 S. Western Ave, Suite 300, Oklahoma City, OK, 73139, US



# Lindsey St. & Berry Rd. - TMC

Wed Sep 1, 2021

PM Peak (4:30 PM - 5:30 PM) - Overall Peak Hour

All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks)

All Movements

ID: 868249, Location: 35.203926, -97.459005



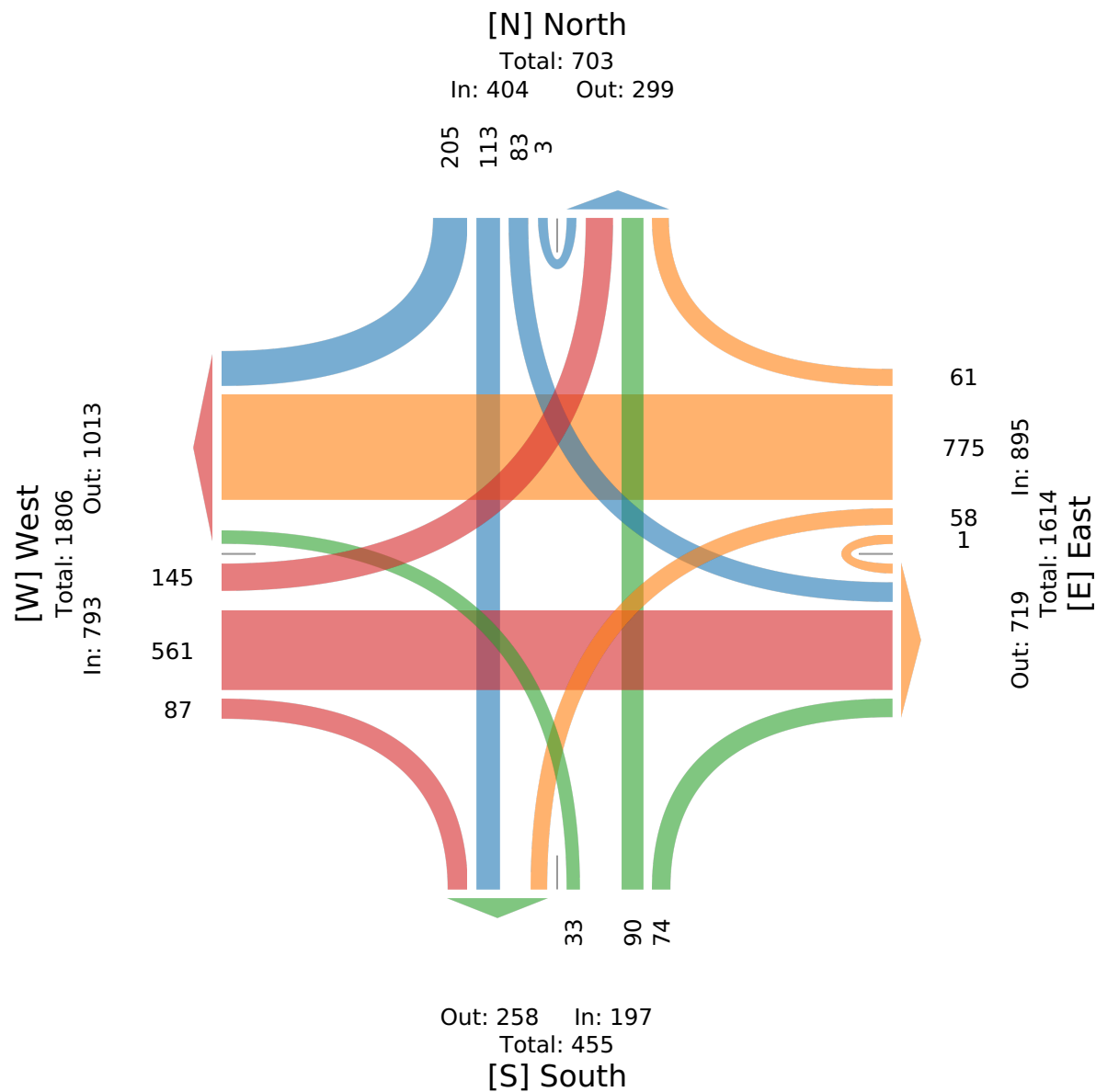
Provided by: Traffic Engineering Consultants, Inc.  
6000 S. Western Ave, Suite 300, Oklahoma City, OK, 73139, US

Leg Direction	North Southbound					East Westbound					South Northbound					West Eastbound					
Time	L	T	R	U	App	L	T	R	U	App	L	T	R	U	App	L	T	R	U	App	Int
2021-09-01 4:30PM	22	26	53	0	101	12	196	18	0	226	11	23	12	0	46	33	140	26	0	199	572
4:45PM	20	28	60	0	108	23	188	13	1	225	9	28	22	0	59	37	136	23	0	196	588
5:00PM	23	33	40	3	99	11	200	20	0	231	6	19	24	0	49	43	134	17	0	194	573
5:15PM	18	26	52	0	96	12	191	10	0	213	7	20	16	0	43	32	151	21	0	204	556
<b>Total</b>	83	113	205	3	404	58	775	61	1	895	33	90	74	0	197	145	561	87	0	793	2289
<b>% Approach</b>	20.5%	28.0%	50.7%	0.7%	-	6.5%	86.6%	6.8%	0.1%	-	16.8%	45.7%	37.6%	0%	-	18.3%	70.7%	11.0%	0%	-	-
<b>% Total</b>	3.6%	4.9%	9.0%	0.1%	17.6%	2.5%	33.9%	2.7%	0%	39.1%	1.4%	3.9%	3.2%	0%	8.6%	6.3%	24.5%	3.8%	0%	34.6%	-
<b>PHF</b>	0.902	0.856	0.854	0.250	0.935	0.630	0.969	0.763	0.250	0.969	0.750	0.804	0.771	-	0.835	0.843	0.929	0.837	-	0.972	0.973
<b>Lights</b>	82	113	204	3	402	58	769	61	1	889	33	89	73	0	195	143	555	87	0	785	2271
<b>% Lights</b>	98.8%	100%	99.5%	100%	99.5%	100%	99.2%	100%	100%	99.3%	100%	98.9%	98.6%	0%	99.0%	98.6%	98.9%	100%	0%	99.0%	99.2%
<b>Articulated Trucks</b>	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
<b>% Articulated Trucks</b>	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%	0%
<b>Buses and Single-Unit Trucks</b>	1	0	1	0	2	0	6	0	0	6	0	1	1	0	2	2	6	0	0	8	18
<b>% Buses and Single-Unit Trucks</b>	1.2%	0%	0.5%	0%	0.5%	0%	0.8%	0%	0%	0.7%	0%	1.1%	1.4%	0%	1.0%	1.4%	1.1%	0%	0%	1.0%	0.8%

\* L: Left, R: Right, T: Thru, U: U-Turn

Lindsey St. & Berry Rd. - TMC  
Wed Sep 1, 2021  
PM Peak (4:30 PM - 5:30 PM) - Overall Peak Hour  
All Classes (Lights, Articulated Trucks, Buses and Single-Unit Trucks)  
All Movements  
ID: 868249, Location: 35.203926, -97.459005

Provided by: Traffic Engineering Consultants, Inc.  
6000 S. Western Ave, Suite 300, Oklahoma City, OK, 73139, US



## ***CAPACITY ANALYSIS***

Volumes

09/16/2021



2021 Existing Traffic  
A.M. Peak Hour























Traffic Engineering Consultants, Inc.



# HCM 6th Signalized Intersection Summary

## 3: Berry Rd & Lindsey St

09/16/2021

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	159	568	36	25	362	65	36	133	71	55	59	89
Future Volume (veh/h)	159	568	36	25	362	65	36	133	71	55	59	89
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	173	617	39	27	393	71	39	145	77	60	64	97
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	334	655	603	452	1425	255	195	169	90	157	104	158
Arrive On Green	0.09	0.35	0.35	0.21	0.47	0.47	0.03	0.15	0.15	0.04	0.16	0.16
Sat Flow, veh/h	1781	1870	1585	1781	3011	539	1781	1150	611	1781	671	1017
Grp Volume(v), veh/h	173	617	39	27	231	233	39	0	222	60	0	161
Grp Sat Flow(s),veh/h/ln	1781	1870	1585	1781	1777	1773	1781	0	1760	1781	0	1687
Q Serve(g_s), s	8.9	38.4	0.7	0.0	9.4	9.6	2.2	0.0	14.8	3.4	0.0	10.7
Cycle Q Clear(g_c), s	8.9	38.4	0.7	0.0	9.4	9.6	2.2	0.0	14.8	3.4	0.0	10.7
Prop In Lane	1.00		1.00	1.00		0.30	1.00		0.35	1.00		0.60
Lane Grp Cap(c), veh/h	334	655	603	452	841	840	195	0	258	157	0	262
V/C Ratio(X)	0.52	0.94	0.06	0.06	0.27	0.28	0.20	0.00	0.86	0.38	0.00	0.61
Avail Cap(c_a), veh/h	336	655	603	452	841	840	245	0	440	191	0	422
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	32.5	37.8	7.6	36.7	19.1	19.2	41.9	0.0	50.0	42.1	0.0	47.3
Incr Delay (d2), s/veh	1.1	23.6	0.2	0.0	0.8	0.8	0.4	0.0	6.5	1.1	0.0	1.7
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	4.0	21.6	0.4	0.6	4.1	4.2	1.0	0.0	7.0	1.6	0.0	4.6
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	33.5	61.4	7.8	36.8	19.9	20.0	42.3	0.0	56.5	43.3	0.0	49.0
LnGrp LOS	C	E	A	D	B	B	D	A	E	D	A	D
Approach Vol, veh/h		829			491			261			221	
Approach Delay, s/veh		53.1			20.9			54.4			47.5	
Approach LOS		D			C			D			D	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	17.9	63.8	12.7	25.6	32.7	49.0	11.6	26.7				
Change Period (Y+Rc), s	7.0	7.0	8.0	8.0	7.0	7.0	8.0	8.0				
Max Green Setting (Gmax), s	11.0	42.0	7.0	30.0	11.0	42.0	7.0	30.0				
Max Q Clear Time (g_c+I1), s	10.9	11.6	5.4	16.8	2.0	40.4	4.2	12.7				
Green Ext Time (p_c), s	0.0	2.6	0.0	0.8	0.0	0.6	0.0	0.7				
<b>Intersection Summary</b>												
HCM 6th Ctrl Delay			43.8									
HCM 6th LOS			D									

Volumes

09/16/2021

























2021 Existing Traffic  
P.M. Peak Hour

Traffic Engineering Consultants, Inc.

# HCM 6th Signalized Intersection Summary

## 3: Berry Rd & Lindsey St

09/16/2021

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	145	561	87	58	775	61	33	90	74	83	113	205
Future Volume (veh/h)	145	561	87	58	775	61	33	90	74	83	113	205
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	158	610	95	63	842	66	36	98	80	90	123	223
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	215	701	641	325	1448	113	130	187	152	281	131	238
Arrive On Green	0.07	0.38	0.38	0.13	0.43	0.43	0.03	0.20	0.20	0.05	0.22	0.22
Sat Flow, veh/h	1781	1870	1585	1781	3338	262	1781	953	778	1781	596	1080
Grp Volume(v), veh/h	158	610	95	63	448	460	36	0	178	90	0	346
Grp Sat Flow(s),veh/h/ln	1781	1870	1585	1781	1777	1823	1781	0	1730	1781	0	1676
Q Serve(g_s), s	7.8	36.3	2.2	0.0	22.9	22.9	1.9	0.0	11.1	4.8	0.0	24.3
Cycle Q Clear(g_c), s	7.8	36.3	2.2	0.0	22.9	22.9	1.9	0.0	11.1	4.8	0.0	24.3
Prop In Lane	1.00		1.00	1.00		0.14	1.00		0.45	1.00		0.64
Lane Grp Cap(c), veh/h	215	701	641	325	771	791	130	0	339	281	0	370
V/C Ratio(X)	0.73	0.87	0.15	0.19	0.58	0.58	0.28	0.00	0.53	0.32	0.00	0.94
Avail Cap(c_a), veh/h	215	701	641	325	771	791	182	0	389	289	0	377
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	32.7	34.8	7.4	43.7	25.7	25.7	38.8	0.0	43.2	36.1	0.0	45.9
Incr Delay (d2), s/veh	11.7	13.9	0.5	0.2	3.2	3.1	0.8	0.0	0.9	0.5	0.0	30.0
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	4.0	18.9	1.2	1.7	10.4	10.6	0.9	0.0	4.8	2.1	0.0	13.2
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	44.4	48.6	7.9	43.9	28.9	28.8	39.6	0.0	44.2	36.6	0.0	76.0
LnGrp LOS	D	D	A	D	C	C	D	A	D	D	A	E
Approach Vol, veh/h		863			971			214				436
Approach Delay, s/veh		43.4			29.9			43.4				67.9
Approach LOS		D			C			D				E
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	15.0	59.0	14.5	31.5	22.0	52.0	11.5	34.5				
Change Period (Y+Rc), s	7.0	7.0	8.0	8.0	7.0	7.0	8.0	8.0				
Max Green Setting (Gmax), s	8.0	48.0	7.0	27.0	11.0	45.0	7.0	27.0				
Max Q Clear Time (g_c+I1), s	9.8	24.9	6.8	13.1	2.0	38.3	3.9	26.3				
Green Ext Time (p_c), s	0.0	5.4	0.0	0.7	0.1	2.1	0.0	0.1				
<b>Intersection Summary</b>												
HCM 6th Ctrl Delay			42.4									
HCM 6th LOS			D									



Volumes

09/16/2021
























2023 Future Background Traffic  
A.M. Peak Hour

Traffic Engineering Consultants, Inc.

# HCM 6th Signalized Intersection Summary

## 3: Berry Rd & Lindsey St

09/16/2021

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	167	596	38	26	380	68	38	140	75	58	62	93
Future Volume (veh/h)	167	596	38	26	380	68	38	140	75	58	62	93
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	182	648	41	28	413	74	41	152	82	63	67	101
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	327	655	604	426	1399	249	201	176	95	160	110	166
Arrive On Green	0.09	0.35	0.35	0.21	0.46	0.46	0.03	0.15	0.15	0.04	0.16	0.16
Sat Flow, veh/h	1781	1870	1585	1781	3015	536	1781	1143	617	1781	673	1015
Grp Volume(v), veh/h	182	648	41	28	242	245	41	0	234	63	0	168
Grp Sat Flow(s),veh/h/ln	1781	1870	1585	1781	1777	1774	1781	0	1759	1781	0	1688
Q Serve(g_s), s	9.4	41.3	0.7	0.0	10.1	10.3	2.3	0.0	15.6	3.5	0.0	11.1
Cycle Q Clear(g_c), s	9.4	41.3	0.7	0.0	10.1	10.3	2.3	0.0	15.6	3.5	0.0	11.1
Prop In Lane	1.00		1.00	1.00		0.30	1.00		0.35	1.00		0.60
Lane Grp Cap(c), veh/h	327	655	604	426	825	823	201	0	270	160	0	275
V/C Ratio(X)	0.56	0.99	0.07	0.07	0.29	0.30	0.20	0.00	0.87	0.39	0.00	0.61
Avail Cap(c_a), veh/h	327	655	604	426	825	823	250	0	440	191	0	422
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	32.8	38.8	7.6	37.9	20.0	20.0	41.2	0.0	49.6	41.4	0.0	46.7
Incr Delay (d2), s/veh	1.7	32.8	0.2	0.0	0.9	0.9	0.4	0.0	8.1	1.2	0.0	1.6
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	4.2	24.6	0.4	0.7	4.5	4.5	1.0	0.0	7.4	1.6	0.0	4.8
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	34.5	71.6	7.8	37.9	20.9	20.9	41.6	0.0	57.7	42.6	0.0	48.3
LnGrp LOS	C	E	A	D	C	C	D	A	E	D	A	D
Approach Vol, veh/h		871			515			275			231	
Approach Delay, s/veh		60.8			21.8			55.3			46.7	
Approach LOS		E			C			E			D	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	18.0	62.7	12.9	26.4	31.7	49.0	11.7	27.6				
Change Period (Y+Rc), s	7.0	7.0	8.0	8.0	7.0	7.0	8.0	8.0				
Max Green Setting (Gmax), s	11.0	42.0	7.0	30.0	11.0	42.0	7.0	30.0				
Max Q Clear Time (g_c+I1), s	11.4	12.3	5.5	17.6	2.0	43.3	4.3	13.1				
Green Ext Time (p_c), s	0.0	2.7	0.0	0.9	0.0	0.0	0.0	0.7				
<b>Intersection Summary</b>												
HCM 6th Ctrl Delay			47.7									
HCM 6th LOS			D									



Volumes

09/16/2021
























2023 Future Background Traffic  
P.M. Peak Hour

Traffic Engineering Consultants, Inc.

# HCM 6th Signalized Intersection Summary

## 3: Berry Rd & Lindsey St

09/16/2021

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	152	589	91	61	814	64	35	95	78	87	119	215
Future Volume (veh/h)	152	589	91	61	814	64	35	95	78	87	119	215
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	165	640	99	66	885	70	38	103	85	95	129	234
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	204	701	642	298	1429	113	124	188	155	281	134	243
Arrive On Green	0.07	0.38	0.38	0.12	0.43	0.43	0.03	0.20	0.20	0.06	0.22	0.22
Sat Flow, veh/h	1781	1870	1585	1781	3336	264	1781	948	782	1781	596	1080
Grp Volume(v), veh/h	165	640	99	66	471	484	38	0	188	95	0	363
Grp Sat Flow(s),veh/h/ln	1781	1870	1585	1781	1777	1823	1781	0	1730	1781	0	1676
Q Serve(g_s), s	8.0	39.0	2.3	0.0	24.8	24.8	2.0	0.0	11.7	5.0	0.0	25.7
Cycle Q Clear(g_c), s	8.0	39.0	2.3	0.0	24.8	24.8	2.0	0.0	11.7	5.0	0.0	25.7
Prop In Lane	1.00		1.00	1.00		0.14	1.00		0.45	1.00		0.64
Lane Grp Cap(c), veh/h	204	701	642	298	761	781	124	0	344	281	0	377
V/C Ratio(X)	0.81	0.91	0.15	0.22	0.62	0.62	0.31	0.00	0.55	0.34	0.00	0.96
Avail Cap(c_a), veh/h	204	701	642	298	761	781	175	0	389	285	0	377
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	33.7	35.6	7.5	45.7	26.7	26.7	38.7	0.0	43.2	35.8	0.0	46.0
Incr Delay (d2), s/veh	20.2	18.3	0.5	0.3	3.8	3.7	1.0	0.0	1.0	0.5	0.0	36.4
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	4.7	21.0	1.2	1.8	11.3	11.6	0.9	0.0	5.1	2.2	0.0	14.4
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	53.9	53.9	8.0	46.0	30.4	30.4	39.7	0.0	44.2	36.3	0.0	82.4
LnGrp LOS	D	D	A	D	C	C	D	A	D	D	A	F
Approach Vol, veh/h		904			1021			226			458	
Approach Delay, s/veh		48.9			31.4			43.5			72.9	
Approach LOS		D			C			D			E	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	15.0	58.4	14.7	31.9	21.4	52.0	11.6	35.0				
Change Period (Y+Rc), s	7.0	7.0	8.0	8.0	7.0	7.0	8.0	8.0				
Max Green Setting (Gmax), s	8.0	48.0	7.0	27.0	11.0	45.0	7.0	27.0				
Max Q Clear Time (g_c+I1), s	10.0	26.8	7.0	13.7	2.0	41.0	4.0	27.7				
Green Ext Time (p_c), s	0.0	5.6	0.0	0.7	0.1	1.5	0.0	0.0				
<b>Intersection Summary</b>												
HCM 6th Ctrl Delay			45.8									
HCM 6th LOS			D									



Volumes

09/16/2021
























2023 Projected Combined Traffic  
A.M. Peak Hour

Traffic Engineering Consultants, Inc.

## HCM 6th Signalized Intersection Summary

## 3: Berry Rd &amp; Lindsey St

09/16/2021

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	169	596	38	26	381	68	38	141	75	59	62	94
Future Volume (veh/h)	169	596	38	26	381	68	38	141	75	59	62	94
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	184	648	41	28	414	74	41	153	82	64	67	102
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	327	655	604	424	1396	248	202	177	95	161	110	167
Arrive On Green	0.09	0.35	0.35	0.20	0.46	0.46	0.03	0.15	0.15	0.04	0.16	0.16
Sat Flow, veh/h	1781	1870	1585	1781	3016	535	1781	1146	614	1781	669	1018
Grp Volume(v), veh/h	184	648	41	28	243	245	41	0	235	64	0	169
Grp Sat Flow(s),veh/h/ln	1781	1870	1585	1781	1777	1774	1781	0	1760	1781	0	1687
Q Serve(g_s), s	9.6	41.3	0.7	0.0	10.2	10.3	2.3	0.0	15.6	3.6	0.0	11.2
Cycle Q Clear(g_c), s	9.6	41.3	0.7	0.0	10.2	10.3	2.3	0.0	15.6	3.6	0.0	11.2
Prop In Lane	1.00		1.00	1.00		0.30	1.00		0.35	1.00		0.60
Lane Grp Cap(c), veh/h	327	655	604	424	823	821	202	0	271	161	0	277
V/C Ratio(X)	0.56	0.99	0.07	0.07	0.29	0.30	0.20	0.00	0.87	0.40	0.00	0.61
Avail Cap(c_a), veh/h	327	655	604	424	823	821	251	0	440	191	0	422
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	32.8	38.8	7.5	38.0	20.0	20.1	41.2	0.0	49.5	41.4	0.0	46.6
Incr Delay (d2), s/veh	1.9	32.8	0.2	0.0	0.9	0.9	0.4	0.0	8.2	1.2	0.0	1.6
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	4.3	24.6	0.4	0.7	4.5	4.6	1.0	0.0	7.5	1.6	0.0	4.8
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	34.7	71.6	7.8	38.0	21.0	21.0	41.6	0.0	57.8	42.6	0.0	48.2
LnGrp LOS	C	E	A	D	C	C	D	A	E	D	A	D
Approach Vol, veh/h		873			516			276			233	
Approach Delay, s/veh		60.8			21.9			55.3			46.6	
Approach LOS		E			C			E			D	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	18.0	62.6	12.9	26.5	31.6	49.0	11.7	27.7				
Change Period (Y+Rc), s	7.0	7.0	8.0	8.0	7.0	7.0	8.0	8.0				
Max Green Setting (Gmax), s	11.0	42.0	7.0	30.0	11.0	42.0	7.0	30.0				
Max Q Clear Time (g_c+I1), s	11.6	12.3	5.6	17.6	2.0	43.3	4.3	13.2				
Green Ext Time (p_c), s	0.0	2.7	0.0	0.9	0.0	0.0	0.0	0.7				
<b>Intersection Summary</b>												
HCM 6th Ctrl Delay			47.7									
HCM 6th LOS			D									

HCM 6th TWSC  
6: Lindsey St & Drive #2

Item 16.

09/16/2021

Intersection

Int Delay, s/veh 0

Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↑	↑			↑
Traffic Vol, veh/h	0	730	474	2	0	1
Future Vol, veh/h	0	730	474	2	0	1
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	793	515	2	0	1

Major/Minor	Major1	Major2	Minor2
Conflicting Flow All	-	0	- 0 - 516
Stage 1	-	-	- - -
Stage 2	-	-	- - -
Critical Hdwy	-	-	- - 6.22
Critical Hdwy Stg 1	-	-	- - -
Critical Hdwy Stg 2	-	-	- - -
Follow-up Hdwy	-	-	- - 3.318
Pot Cap-1 Maneuver	0	-	- 0 559
Stage 1	0	-	- 0 -
Stage 2	0	-	- 0 -
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	-	-	- - 559
Mov Cap-2 Maneuver	-	-	- - -
Stage 1	-	-	- - -
Stage 2	-	-	- - -




Approach	EB	WB	SB
HCM Control Delay, s	0	0	11.5
HCM LOS			B

Minor Lane/Major Mvmt	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	-	-	-	559
HCM Lane V/C Ratio	-	-	-	0.002
HCM Control Delay (s)	-	-	-	11.5
HCM Lane LOS	-	-	-	B
HCM 95th %tile Q(veh)	-	-	-	0

# HCM 6th TWSC

## 7: Berry Rd & Drive #1

09/16/2021

Intersection						
Int Delay, s/veh	0.1					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	2	1	375	3	1	213
Future Vol, veh/h	2	1	375	3	1	213
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	2	1	408	3	1	232
Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	644	410	0	0	411	0
Stage 1	410	-	-	-	-	-
Stage 2	234	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218	-
Pot Cap-1 Maneuver	437	642	-	-	1148	-
Stage 1	670	-	-	-	-	-
Stage 2	805	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	437	642	-	-	1148	-
Mov Cap-2 Maneuver	437	-	-	-	-	-
Stage 1	670	-	-	-	-	-
Stage 2	804	-	-	-	-	-
Approach	WB	NB	SB			
HCM Control Delay, s	12.4	0	0			
HCM LOS	B					
Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT		
Capacity (veh/h)	-	-	489	1148	-	
HCM Lane V/C Ratio	-	-	0.007	0.001	-	
HCM Control Delay (s)	-	-	12.4	8.1	0	
HCM Lane LOS	-	-	B	A	A	
HCM 95th %tile Q(veh)	-	-	0	0	-	



Volumes

09/16/2021
























2023 Future Background Traffic  
P.M. Peak Hour

Traffic Engineering Consultants, Inc.

# HCM 6th Signalized Intersection Summary

## 3: Berry Rd & Lindsey St

09/16/2021

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	160	589	91	62	819	64	35	97	78	94	120	218
Future Volume (veh/h)	160	589	91	62	819	64	35	97	78	94	120	218
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	174	640	99	67	890	70	38	105	85	102	130	237
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	203	701	642	298	1430	112	121	188	152	280	134	243
Arrive On Green	0.07	0.38	0.38	0.12	0.43	0.43	0.03	0.20	0.20	0.06	0.22	0.22
Sat Flow, veh/h	1781	1870	1585	1781	3337	262	1781	957	774	1781	594	1082
Grp Volume(v), veh/h	174	640	99	67	474	486	38	0	190	102	0	367
Grp Sat Flow(s),veh/h/ln	1781	1870	1585	1781	1777	1823	1781	0	1731	1781	0	1676
Q Serve(g_s), s	8.0	39.0	2.3	0.0	24.9	24.9	2.0	0.0	11.9	5.4	0.0	26.1
Cycle Q Clear(g_c), s	8.0	39.0	2.3	0.0	24.9	24.9	2.0	0.0	11.9	5.4	0.0	26.1
Prop In Lane	1.00		1.00	1.00		0.14	1.00		0.45	1.00		0.65
Lane Grp Cap(c), veh/h	203	701	642	298	761	781	121	0	340	280	0	377
V/C Ratio(X)	0.86	0.91	0.15	0.22	0.62	0.62	0.31	0.00	0.56	0.36	0.00	0.97
Avail Cap(c_a), veh/h	203	701	642	298	761	781	172	0	389	280	0	377
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	35.1	35.6	7.5	45.7	26.7	26.7	38.9	0.0	43.5	36.0	0.0	46.1
Incr Delay (d2), s/veh	27.8	18.3	0.5	0.3	3.8	3.7	1.1	0.0	1.1	0.6	0.0	39.1
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	5.3	21.0	1.2	1.8	11.4	11.7	0.9	0.0	5.2	2.4	0.0	14.9
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	62.9	53.9	8.0	46.0	30.5	30.5	40.0	0.0	44.6	36.6	0.0	85.3
LnGrp LOS	E	D	A	D	C	C	D	A	D	D	A	F
Approach Vol, veh/h		913			1027			228			469	
Approach Delay, s/veh		50.6			31.5			43.8			74.7	
Approach LOS		D			C			D			E	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	15.0	58.4	15.0	31.6	21.4	52.0	11.6	35.0				
Change Period (Y+Rc), s	7.0	7.0	8.0	8.0	7.0	7.0	8.0	8.0				
Max Green Setting (Gmax), s	8.0	48.0	7.0	27.0	11.0	45.0	7.0	27.0				
Max Q Clear Time (g_c+I1), s	10.0	26.9	7.4	13.9	2.0	41.0	4.0	28.1				
Green Ext Time (p_c), s	0.0	5.6	0.0	0.7	0.1	1.5	0.0	0.0				
<b>Intersection Summary</b>												
HCM 6th Ctrl Delay			46.9									
HCM 6th LOS			D									

HCM 6th TWSC  
6: Lindsey St & Drive #2

Item 16.

09/16/2021

Intersection

Int Delay, s/veh 0.1

Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↑	↑			↑
Traffic Vol, veh/h	0	761	939	7	0	6
Future Vol, veh/h	0	761	939	7	0	6
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	827	1021	8	0	7

Major/Minor	Major1	Major2	Minor2
Conflicting Flow All	-	0	- 1025
Stage 1	-	-	- -
Stage 2	-	-	- -
Critical Hdwy	-	-	- 6.22
Critical Hdwy Stg 1	-	-	- -
Critical Hdwy Stg 2	-	-	- -
Follow-up Hdwy	-	-	- 3.318
Pot Cap-1 Maneuver	0	-	- 0 285
Stage 1	0	-	- 0 -
Stage 2	0	-	- 0 -
Platoon blocked, %	-	-	-
Mov Cap-1 Maneuver	-	-	- 285
Mov Cap-2 Maneuver	-	-	- -
Stage 1	-	-	- -
Stage 2	-	-	- -

Approach	EB	WB	SB
HCM Control Delay, s	0	0	17.9
HCM LOS			C




Minor Lane/Major Mvmt	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	-	-	-	285
HCM Lane V/C Ratio	-	-	-	0.023
HCM Control Delay (s)	-	-	-	17.9
HCM Lane LOS	-	-	-	C
HCM 95th %tile Q(veh)	-	-	-	0.1



# HCM 6th TWSC

## 7: Berry Rd & Drive #1

09/16/2021

Intersection						
Int Delay, s/veh	0.3					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	11	4	311	10	3	421
Future Vol, veh/h	11	4	311	10	3	421
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	12	4	338	11	3	458
Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	808	344	0	0	349	0
Stage 1	344	-	-	-	-	-
Stage 2	464	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218	-
Pot Cap-1 Maneuver	350	699	-	-	1210	-
Stage 1	718	-	-	-	-	-
Stage 2	633	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	349	699	-	-	1210	-
Mov Cap-2 Maneuver	349	-	-	-	-	-
Stage 1	718	-	-	-	-	-
Stage 2	631	-	-	-	-	-
Approach	WB	NB	SB			
HCM Control Delay, s	14.3	0	0.1			
HCM LOS	B					
Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT		
Capacity (veh/h)	-	-	403	1210	-	
HCM Lane V/C Ratio	-	-	0.04	0.003	-	
HCM Control Delay (s)	-	-	14.3	8	0	
HCM Lane LOS	-	-	B	A	A	
HCM 95th %tile Q(veh)	-	-	0.1	0	-	

# ***QUEUING ANALYSIS***

# Queuing and Blocking Report

## A.M. Peak Hour

Item 16.

10/04/2021

### Intersection: 3: Berry Rd & Lindsey St

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	274	409	108	89	163	153	124	300	124	200
Average Queue (ft)	84	234	13	28	99	98	38	135	49	88
95th Queue (ft)	201	396	70	72	156	151	108	238	105	167
Link Distance (ft)		402	402		147	147		336		223
Upstream Blk Time (%)		1	0		2	1		0		0
Queuing Penalty (veh)		0	0		4	3		0		0
Storage Bay Dist (ft)	250			65			100		100	
Storage Blk Time (%)		5		0	18		0	23	1	13
Queuing Penalty (veh)		9		1	5		0	9	2	7

### Intersection: 6: Lindsey St & Drive #2

Movement	WB	SB
Directions Served	TR	R
Maximum Queue (ft)	69	22
Average Queue (ft)	7	1
95th Queue (ft)	45	11
Link Distance (ft)	150	57
Upstream Blk Time (%)	0	
Queuing Penalty (veh)	0	
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

### Intersection: 7: Berry Rd & Drive #1

Movement	WB	SB
Directions Served	LR	LT
Maximum Queue (ft)	30	12
Average Queue (ft)	2	1
95th Queue (ft)	16	7
Link Distance (ft)	123	140
Upstream Blk Time (%)		
Queuing Penalty (veh)		
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

### Network Summary

Network wide Queuing Penalty: 40

# Queuing and Blocking Report

## P.M. Peak Hour

Item 16.

10/04/2021

### Intersection: 3: Berry Rd & Lindsey St

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	R	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	275	428	412	89	185	179	124	224	125	233
Average Queue (ft)	137	288	59	47	159	156	30	93	81	182
95th Queue (ft)	279	454	234	92	175	178	82	179	149	274
Link Distance (ft)		402	402		147	147		336		223
Upstream Blk Time (%)		8	1		30	24				12
Queuing Penalty (veh)		0	0		140	116				51
Storage Bay Dist (ft)	250			65			100		100	
Storage Blk Time (%)		13		4	46			11	5	41
Queuing Penalty (veh)		22		15	28			4	16	38

### Intersection: 6: Lindsey St & Drive #2

Movement	WB	SB
Directions Served	TR	R
Maximum Queue (ft)	184	31
Average Queue (ft)	151	7
95th Queue (ft)	220	25
Link Distance (ft)	150	57
Upstream Blk Time (%)	26	0
Queuing Penalty (veh)	0	0
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

### Intersection: 7: Berry Rd & Drive #1

Movement	WB	SB
Directions Served	LR	LT
Maximum Queue (ft)	43	159
Average Queue (ft)	11	44
95th Queue (ft)	35	142
Link Distance (ft)	123	140
Upstream Blk Time (%)		5
Queuing Penalty (veh)		0
Storage Bay Dist (ft)		
Storage Blk Time (%)		
Queuing Penalty (veh)		

### Network Summary

Network wide Queuing Penalty: 429

**NORMAN PLANNING COMMISSION  
REGULAR SESSION MINUTES**

**OCTOBER 14, 2021**

The Planning Commission of the City of Norman, Cleveland County, State of Oklahoma, met in Regular Session in the Council Chambers of the Norman Municipal Building, 201 West Gray Street, on the 14<sup>th</sup> day of October, 2021.

Notice and agenda of the meeting was posted at the Norman Municipal Building and online at <https://norman-ok.municodem meetings.com> at least twenty-four hours prior to the beginning of the meeting.

Chair Erica Bird called the meeting to order at 6:30 p.m.

\* \* \*

**ROLL CALL**

**MEMBERS PRESENT**

Erin Williford  
Steven McDaniel  
Erica Bird  
Dave Boeck  
Sandy Bahan  
Michael Jablonski

**MEMBERS ABSENT**

Nouman Jan  
Lark Zink

A quorum was present.

**STAFF MEMBERS PRESENT**

Jane Hudson, Director, Planning &  
Community Development  
Lora Hoggatt, Planning Services Manager  
Roné Tromble, Recording Secretary  
Todd McLellan, Development Engineer  
Heather Poole, Asst. City Attorney  
Jami Short, Traffic Engineer

\* \* \*

Item No. 2, being:

**R-2122-31 – SOONER TRADITIONS, L.L.C. AND HUNTER MILLER FAMILY, L.L.C. REQUEST AMENDMENT OF THE NORMAN 2025 LAND USE AND TRANSPORTATION PLAN FROM OFFICE DESIGNATION AND LOW DENSITY RESIDENTIAL DESIGNATION TO COMMERCIAL DESIGNATION FOR 1.33 ACRES OF PROPERTY LOCATED AT THE NORTHEAST CORNER OF S. BERRY ROAD AND W. LINDSEY STREET (1027 & 1035 S. BERRY ROAD).**

**ITEMS SUBMITTED FOR THE RECORD:**

1. NORMAN 2025 Map
2. Staff Report
3. Pre-Development Summary

and

Item No. 3, being:

**O-2122-15 – SOONER TRADITIONS, L.L.C. AND HUNTER MILLER FAMILY, L.L.C. REQUEST REZONING FROM R-1, SINGLE FAMILY DWELLING DISTRICT, AND CO, SUBURBAN OFFICE COMMERCIAL DISTRICT, TO SPUD, SIMPLE PLANNED UNIT DEVELOPMENT, FOR APPROXIMATELY 1.33 ACRES OF PROPERTY LOCATED AT THE NORTHEAST CORNER OF S. BERRY ROAD AND W. LINDSEY STREET (1027 & 1035 S. BERRY ROAD).**

**ITEMS SUBMITTED FOR THE RECORD:**

1. Location Map
2. Staff Report
3. SPUD Narrative with Exhibits A-D
4. Transportation Impacts
5. Revised Traffic Impact Analysis
6. Pre-Development Summary
7. Protest & Support Map – October 6, 2021
8. Protests & Supports received by October 6, 2021

**PRESENTATION BY STAFF:**

1. Ms. Lora Hoggatt presented the staff report. Protest letters were received on this item, which represented 12.5% of the notification area. Support letters were received which represented 17.8% of the notification area.

**PRESENTATION BY THE APPLICANT:**

1. Mr. Sean Rieger, Rieger Law Group, representing the applicant

**AUDIENCE PARTICIPATION:**

1. Mr. Jim Akey, 819 Carey Drive, spoke in opposition
2. Mr. David Nehrenz, 902 Carey Drive, spoke in opposition
3. Ms. Roxana Meyer, 1202 W. Brooks Street, spoke in opposition
4. Ms. Michelle Nehrenz, 906 Carey Drive, spoke in opposition
5. Ms. Gabrielle Mandeville, 905 Carey Drive, spoke in opposition
6. Mr. Dan Mains, 1128 W. Brooks Street, spoke in opposition
7. Ms. Lara Souza, 823 Carey Drive, spoke in opposition
8. Mr. Dennis Yarbrow, 803 S. Berry Road, spoke in opposition
9. Ms. Susan Meyer, 808 Carey Drive, spoke in opposition
10. Mr. Steve Maple, 951 S. Berry Road, spoke in opposition
11. Mr. Kevin Connywerdy, 1132 W. Brooks Street, spoke in opposition
12. Ms. Nolita Morgan, 1027 Madison Street, spoke in opposition

**DISCUSSION AND ACTION BY THE PLANNING COMMISSION:**

1. Chair Erica Bird made comments
2. Commissioner Michael Jablonski made comments
3. Mr. Sean Rieger responded to a question
4. Commissioner Erin Williford made comments

5. Commissioner Dave Boeck made comments
6. Chair Erica Bird made comments

*Michael Jablonski moved to recommend adoption of Resolution No. R-2122-31 and Ordinance No. O-2122-15 to City Council. Erin Williford seconded the motion.*

There being no further discussion, a vote on the motion was taken with the following result:

YEAS	Erin Williford, Steven McDaniel, Erica Bird, Dave Boeck, Michael Jablonski
NAYES	Sandy Bahan
MEMBERS ABSENT	Nouman Jan, Lark Zink

The motion, to recommend adoption of Ordinance No. O-2122-14 and PP-2122-5 to City Council, passed by a vote of 5-1.

\*\*\*



**1027 AND 1035 S. BERRY ROAD  
NORMAN OK**

**SIMPLE PLANNED UNIT DEVELOPMENT**

APPLICANT:

***SOONER TRADITIONS L.L.C.***

APPLICATION FOR:

SIMPLE PLANNED UNIT DEVELOPMENT

Submitted August 2, 2021  
Revised November 29, 2021

PREPARED BY:

**RIEGER LAW GROUP PLLC**  
136 Thompson Drive  
Norman, Oklahoma 73069

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- F. Traffic Circulation and Access
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### III. DEVELOPMENT PLAN AND DESIGN CONCEPT

- A. Permitted Uses
- B. Area Regulations
- C. Open Space
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- E. Landscaping/Tree Preservation
- F. Signage
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- H. Fencing
- I. Height
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- K. Exterior Materials

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- A. Site Development Plan
- B. Allowable Uses
- C. Open Space Diagram
- D. Stormwater Enhancement Diagram

## **I. INTRODUCTION**

This Simple Planned Unit Development (the “**SPUD**”) is being submitted for the property located at the Northeast corner of the South Berry Road and West Lindsey Street intersection, which is commonly known as 1027 and 1035 S. Berry Road, Norman, OK (the “**Property**”). The Property consists of one platted lot that was subdivided through different zoning requests over time. This request seeks to return the zoning classification and use of the Property back to a one single platted lot. Currently, the south half of the Property is zoned CO, Suburban Office Commercial District, and the north half is zoned R-1, Single-Family Dwelling District. However, the properties on all three other corners of the Lindsey Street and Berry Road intersection are all NORMAN 2025 Planned Commercial, and zoned C-1 or C-2 Commercial districts. The Applicant seeks to develop a small-scale commercial center that incorporates C-1 Local Commercial District allowable uses as more particularly detailed on **Exhibit B**. In keeping with the intent and spirit of the City of Norman’s C-1 Zoning Ordinance, this request is intended to provide zoning for the conduct of uses that meet the regular needs and for the convenience of the people of adjacent residential areas.

## **II. PROPERTY DESCRIPTIONS; EXISTING CONDITIONS**

### **A. Location**

The Property is located at the Northeast corner of the South Berry Road and West Lindsey Street intersection, which is commonly known as 1027 and 1035 S. Berry Road, Norman, Oklahoma. The Berry Road and Lindsey Street intersection is a section line intersection featuring a combined daily traffic count (per ACOG) of almost 27,000 vehicles per day.

### **B. Existing Land Use and Zoning**

The south half of the Property is currently zoned CO, Suburban Office Commercial District and the north half of the Property is zoned R-1, Single-Family Dwelling District. The south half of the Property is currently designated on NORMAN 2025 as Office and the north half of the Property is designated Low Density Residential. As noted above, all other corners of the Lindsey Street and Berry Road intersection are NORMAN 2025 Planned Commercial, and zoned C-1, Local Commercial or C-2, General Commercial.

### **C. Elevation and Topography; Drainage**

The Property is developed with two older residential structures and gently slopes southwest. No portion of the Property is in the FEMA 100-year flood plain or the WQPZ. A drainage report has been conducted for the Property and provided to City Staff for review.

### **D. Utility Services**

The necessary utility services for this project are already located on or near the Property as this is an already developed location.

**E. Fire Protection Services**

Fire protection services are as provided by the City of Norman Fire Department and per the City of Norman regulations for such. If required, fire hydrants will be installed on or near the Property in compliance with the applicable provisions of Norman's fire and building codes.

**F. Traffic Circulation and Access**

There are currently three access points on South Berry Road for the Property.

**G. Fencing**

The Property currently has no boundary fencing.

**III. DEVELOPMENT PLAN AND DESIGN CONCEPT**

The Property shall be developed as depicted on the Site Development Plan, attached hereto as **Exhibit A**. The exhibits attached hereto are incorporated herein by reference. The Property shall be developed in conformance with the Site Development Plan, subject to final design development and the changes allowed by Section 22.420.05(11) of the City of Norman's SPUD Ordinance, as may be amended from time to time.

**A. Uses Permitted**

It is the intent of this SPUD to allow the Property to incorporate in, and provide for, the allowable uses provided under the City of Norman's C-1, Local Commercial District. An exhaustive list of the allowable uses for the Property is attached hereto as **EXHIBIT B**.

**B. Area Regulations**

North Setback: There shall be a ninety (90') foot building setback from the North property line.

South Setback: There shall be a twenty-five (25') foot building setback from the South property line.

East Setback: There shall be a forty-five (45') foot building setback from the East property line.

West Setback. There shall be a twenty-five (25') foot building setback from the West property line.

The building setbacks enumerated herein shall be applicable to vertical habitable or occupiable structures.

**C. Open space and green space**

The development of the Property will feature open space and green space areas, as illustrated on the Open Space Diagram, attached hereto as **Exhibit C**. The impervious area for the Property shall not exceed 65%. There shall be a minimum twenty-five (25') foot landscape buffer along the South and West property lines, a minimum twenty (20') foot landscape buffer along the North property line, and a minimum sixteen (16') foot landscape buffer along the East property line. Notwithstanding the foregoing, minor improvements, such as, but not limited to, the patio area, pedestrian bike access path, architectural benches and features, and/or a paved walkway around the building, may encroach within the landscape buffers to the South and West of the building. Additionally, the Applicant will utilize low impact development techniques ("LIDs") and best management practices ("BMPs") in the development of the Property. Examples of potential LIDs and BMPs that may be utilized on the Property are indicated on the Stormwater Enhancement Diagram, attached hereto as **Exhibit D**. The locations and types of LIDs and BMPs are subject to modification during final site development.

**D. Traffic access/circulation/sidewalks**

There are currently three access points on South Berry Road for the Property, which as noted above is currently being used as if it were two separate lots. The Applicant proposes removing two of those access points, relocating one of them farther north, and providing a new access point on Lindsey Street. New sidewalks would be installed along Berry Road frontage. The access point from Lindsey Street shall be restricted access of right in, right out only. The Applicant has provided a Traffic Impact Analysis discussing the proposed development to City Staff.

**E. Landscaping/Tree Preservation**

Trees shall be preserved by the Applicant, when possible, to fit the proposed site plan. If the Applicant needs to remove a mature tree in order to facilitate the development of the Property per the site plan, the Applicant shall plant two (2) trees, minimum two-inch caliper, on the Property, in a location to be determined by the Applicant. Additionally, no trees located within the public right of ways, will be removed by the Applicant unless one or more of the following circumstances apply: (i) a tree needs to be removed or altered in order to comply with Norman's City Ordinances, as amended from time to time; (ii) a tree(s) needs to be removed to allow for installation of the new sidewalk and entry drive approaches, utility installation, bus stop changes, or other infrastructure modifications; or (iii) a tree is

a dangerous tree or diseased or infected tree as defined in Article X of Chapter 10 of the City of Norman's Code of Ordinances, as amended from time to time.

**F. Signage**

Signage will comply with the City of Norman's commercial signage ordinances applicable to Norman's C-1, Local Commercial District, as may be amended from time to time.

**G. Lighting**

The Applicant shall meet the City of Norman's Commercial Outdoor Lighting Standards, as amended from time to time.

**H. Fencing**

The Property currently has no boundary fencing. The Property will feature a six (6') foot high brick masonry privacy boundary wall along the North and East boundaries of the Property, provided that the wall may commence at the South setback line on the East boundary of the Property. The wall may be of panelized construction.

**I. Height**

Height shall be restricted to no more than one (1) story for all building structures.

**J. Parking**

The Property shall comply with Norman's applicable parking ordinances, as amended from time to time. Additionally, bike racks will be provided on the Property to encourage and support multi-modal transportation to and from the development.

**K. Exterior Materials**

The exterior materials of the building to be constructed on the Property may be brick, glass, stone, synthetic stone, stucco, EIFS, masonry, metal accents, composition shingles, and any combination thereof.

**EXHIBIT A****Site Development Plan**

The note of 6' screen wall below is changed to 6' brick wall





## **EXHIBIT B**

### Allowable Uses

#### **Commercial Uses:**

*(for purposes of familiarity, this list is based on the City of Norman's C-1, Local Commercial District. In the event of any ambiguity or conflict between the City of Norman's C-1, Local Commercial District Ordinance and this list, this list shall control)*

- Antique shop.
- Appliance Store.
- Art Gallery.
- Artist materials supply or studio.
- Assembly Halls of non-profit corporations.
- Automobile parking lots.
- Baby store.
- Bakery goods store.
- Bank.
- Barber shop, or beauty parlor.
- Book or stationery store.
- Camera shop.
- Candy store.
- Catering establishment.
- Child care establishment.
- Churches.
- Clothing or apparel store.
- Dairy products or ice cream store.
- Delicatessen store.
- Dress shop.
- Drug store or fountain.
- Dry Cleaning Pick-up Station.
- Dry goods store.
- Fabric or notion store.
- Florist.
- Furniture Store.
- Gift Shop.
- Grocery or supermarket.
- Hardware store.
- Interior decorating store.
- Jewelry shop.
- Key shop.
- Leathergoods shop.
- Libraries.
- Medical Marijuana Dispensary, as allowed by state law.

- Museums.
- Music Conservatories.
- Messenger or telegraph service.
- Office business.
- Painting and decorating shop.
- Pet shop.
- Pharmacy.
- Photographer's studio.
- Radio and television, mobile phones or electronics sales and service.
- Restaurant. A restaurant may include live entertainment and/or a dance floor, (all such activity fully within an enclosed building) provided the kitchen remains open with full food service whenever live entertainment is offered.
- Retail spirits store.
- Sewing machine sales.
- Sporting goods sales.
- Shoe store or repair shop.
- Tailor Shop.
- Trade schools and schools for vocational training; or educational services such as tutoring.
- Tier I Medical Marijuana Processor, as allowed by state law.
- Tier II Medical Marijuana Processor, as allowed by state law.
- Toy store.

**EXHIBIT C**

**OPEN SPACE DIAGRAM**



**Open Space Diagram**

## EXHIBIT D

### STORMWATER ENHANCEMENT DIAGRAM



**Stormwater Enhancements**

**File Attachments for Item:**

17. CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF RESOLUTION R-2122-64: A RESOLUTION OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, AMENDING THE NORMAN 2025 LAND USE AND TRANSPORTATION PLAN SO AS TO REMOVE PART OF THE WEST HALF (W/2) OF THE SOUTHWEST QUARTER (SW/4) OF SECTION TWENTY-NINE (29), TOWNSHIP NINE (9) NORTH, RANGE ONE (1) WEST, OF THE INDIAN MERIDIAN (I.M.), CLEVELAND COUNTY, OKLAHOMA, FROM THE COUNTRY RESIDENTIAL DESIGNATION AND PLACE THE SAME IN THE COMMERCIAL DESIGNATION. (WEST OF OLIPHANT AVENUE BETWEEN ALAMEDA DRIVE AND ALAMEDA STREET)



## CITY OF NORMAN, OK STAFF REPORT

**MEETING DATE:** 01/25/2022

**REQUESTER:** Saffron Fletcher, Sanctuary Gardens and Wellness, L.L.C.

**PRESENTER:** Jane Hudson, Director, Planning & Community Development

**ITEM TITLE:** CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF RESOLUTION R-2122-64: A RESOLUTION OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, AMENDING THE NORMAN 2025 LAND USE AND TRANSPORTATION PLAN SO AS TO REMOVE PART OF THE WEST HALF (W/2) OF THE SOUTHWEST QUARTER (SW/4) OF SECTION TWENTY-NINE (29), TOWNSHIP NINE (9) NORTH, RANGE ONE (1) WEST, OF THE INDIAN MERIDIAN (I.M.), CLEVELAND COUNTY, OKLAHOMA, FROM THE COUNTRY RESIDENTIAL DESIGNATION AND PLACE THE SAME IN THE COMMERCIAL DESIGNATION. (WEST OF OLIPHANT AVENUE BETWEEN ALAMEDA DRIVE AND ALAMEDA STREET)

### **SUMMARY OF REQUEST:**

The applicant is proposing development of a plant nursery and monarch butterfly waystation on a 5.99-acre parcel. This development proposal requires rezoning from RE, Residential Estates District, to CR, Rural Commercial District, and a NORMAN 2025 Land Use and Transportation Plan amendment from Country Residential to Commercial.

### **STAFF ANALYSIS:**

For changes in classification under the NORMAN 2025 Land Use and Transportation Plan, the following information is forwarded for consideration.

The role of the NORMAN 2025 Plan in the City's ongoing and diverse planning activities states the document must be flexible, and that it is updated and amended periodically. The Plan defines the desired land use patterns for use and development of all private sector properties. This Plan will serve as a policy guide for zoning and planning requests as they are presented to the Planning Commission and City Council.

1. ***Has there been a change in circumstances resulting from development of the properties in the general vicinity which suggest that the proposed change will not be contrary to the public interest?*** A lot to the north of the subject property was rezoned to CR and changed NORMAN 2025 designation to Commercial for the development of a Dollar General in 2012. A lot to the east across Oliphant Avenue is where Alameda Market is located. This has been a successful convenience store with fuel pumps for many years.
2. ***Is there a determination that the proposed change would not result in adverse land use or adverse traffic impacts to surrounding properties or the vicinity?*** Alameda Drive was previously designated as State Highway No. 9. Alameda Drive is one of the main access points to Lake Thunderbird, therefore Alameda Drive carries a moderate amount of traffic. The use of a plant nursery does not require a Traffic Impact Analysis as it does not generate enough trips, however, with this use it is not anticipated that traffic will increase significantly. The applicant will have normal business hours with limited special events. A visual barrier will be maintained with trees around the perimeter of the property.

### **CONCLUSION:**

Staff forwards this request, Resolution R-2122-64, for City Council's consideration. At their meeting of December 9, 2021, Planning Commission recommended adoption of Resolution R-2122-64 by a vote 6-1.



R-2122-64

A RESOLUTION OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, AMENDING THE NORMAN 2025 LAND USE AND TRANSPORTATION PLAN SO AS TO REMOVE PART OF THE WEST HALF (W/2) OF THE SOUTHWEST QUARTER (SW/4) OF SECTION TWENTY-NINE (29), TOWNSHIP NINE (9) NORTH, RANGE ONE (1) WEST OF THE INDIAN MERIDIAN (I.M.), CLEVELAND COUNTY, OKLAHOMA, FROM THE COUNTRY RESIDENTIAL DESIGNATION AND PLACE THE SAME IN THE COMMERCIAL DESIGNATION.

(West of Oliphant Avenue between Alameda Drive and Alameda Street)

- § 1. WHEREAS, the Council of the City of Norman recognizes citizens' concerns about the future development of Norman; and
- § 2. WHEREAS, the City Council at its meeting of November 16, 2004, reviewed and adopted the NORMAN 2025 Land Use and Transportation Plan, with an effective date of December 16, 2004; and
- § 3. WHEREAS, Sanctuary Gardens and Wellness, L.L.C. has requested that the following described property be moved from the Country Residential Designation and placed in the Commercial Designation for the hereinafter described property, to wit:

All of the West Half (W/2) of the Southwest Quarter (SW/4) of Section Twenty-nine (29), Township Nine (9) North, Range One (1) West, of the Indian Meridian, Cleveland County, Oklahoma, Less and Except Tracts One and Two (1 & 2) of ALAMEDA 80 CERTIFICATE OF SURVEY. Said Tract being more particularly described as follows:

A tract of land being a part of the West Half (W/2) of the Southwest Quarter (SW/4) of Section Twenty-nine (29), Township Nine (9) North, Range One (1) West, of the Indian Meridian, Cleveland County, Oklahoma, more particularly described as follows:

BEGINNING at the Southwest Corner of said Southwest Quarter (SW/4) of Section 29; thence N00°44'11"W along the West line of said SW/4 a distance of 81.50 feet to a point on the North Right of Way line of Alameda Drive and the Southwest Corner of Tract 2 of the ALAMEDA 80 CERTIFICATE OF SURVEY; thence along said Right of Way and South line of Tract 2, on a curve to the left with an arc length of 1350.44, with a radius of 5679.70 feet, a chord bearing of N77°29'40"E, and a chord length of 1347.26 to a point on the East line of the West Half (W/2) of the Southwest Quarter (SW/4); thence S00°47'32"E along said East line a distance of 368.99 feet to the Southeast Corner of the W/2 SW/4; thence S89°49'57"W along the South line of said SW/4 a distance of 1318.97 feet to the point of beginning.

Said Tract contains  $\pm$ 261042.56 square feet, or  $\pm$ 5.99 acres.

NOW, THEREFORE BE IT RESOLVED BY THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA:

- § 4. That the Council of the City of Norman recognizes the need to control the future growth of the City of Norman; and, that after due consideration has determined that the requested amendment to the NORMAN 2025 Land Use and Transportation Plan should be adopted, and does hereby approve the requested designation.

PASSED AND ADOPTED this \_\_\_\_\_ day of \_\_\_\_\_, 2022.

\_\_\_\_\_  
(Mayor)

ATTEST:

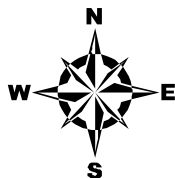
\_\_\_\_\_  
(City Clerk)



# Norman 2025 Land Use Plan



Map Produced by the City of Norman  
Geographic Information System.  
The City of Norman assumes no  
responsibility for errors or omissions  
in the information presented.



November 16, 2021

0 250 500 Ft.

Subject Tract

Zoning

RESOLUTION NO. R-2122-64

ITEM NO. 5

**STAFF REPORT**

**ITEM:** Sanctuary Gardens and Wellness, L.L.C. requests amendment of the NORMAN 2025 Land Use and Transportation Plan from Country Residential Designation to Commercial Designation for approximately 5.99 acres of property located west of Oliphant Avenue between Alameda Drive and Alameda Street.

**SUMMARY OF REQUEST:** The applicant is proposing development of a plant nursery and monarch butterfly waystation on a 5.99-acre parcel. This development proposal requires rezoning from RE, Residential Estates District, to CR, Rural Commercial District, and a NORMAN 2025 Land Use and Transportation Plan amendment from Country Residential to Commercial.

**STAFF ANALYSIS:** For changes in classification under the NORMAN 2025 Land Use and Transportation Plan, the following information is forwarded for consideration.

The role of the NORMAN 2025 Plan in the City's ongoing and diverse planning activities states the document must be flexible, and that it is updated and amended periodically. The Plan defines the desired land use patterns for use and development of all private sector properties. This Plan will serve as a policy guide for zoning and planning requests as they are presented to the Planning Commission and City Council.

1. ***Has there been a change in circumstances resulting from development of the properties in the general vicinity which suggest that the proposed change will not be contrary to the public interest?*** A lot to the north of the subject property was rezoned to CR and changed NORMAN 2025 designation to Commercial for the development of a Dollar General in 2012. A lot to the east across Oliphant Avenue is where Alameda Market is located. This has been a successful convenience store with fuel pumps for many years.
2. ***Is there a determination that the proposed change would not result in adverse land use or adverse traffic impacts to surrounding properties or the vicinity?*** Alameda Drive was previously designated as State Highway No. 9. Alameda Drive is one of the main access points to Lake Thunderbird, therefore Alameda Drive carries a moderate amount of traffic. The use of a plant nursery does not require a Traffic Impact Analysis as it does not generate enough trips, however, with this use it is not anticipated that traffic will increase significantly. The applicant will have normal business hours with limited special events. A visual barrier will be maintained with trees around the perimeter of the property.

**CONCLUSION:** Staff forwards this request, Resolution No. R-2122-64, for Planning Commission's consideration.

**Applicant:** Saffron Fletcher, Sanctuary Gardens & Wellness, LLC

**Project Location:** West of Oliphant Avenue north of Alameda Street

**Case Number:** PD21-27

**Time:** 6:00 p.m.

**Applicant/Representative**

Saffron Fletcher  
Jason Fletcher  
Cedric LeBlanc  
Celeste LeBlanc  
Dylan West

**Attendees**

Tim Sherban  
Kim Sherban  
Brandi Rice  
Mark Cox

**City Staff**

Lora Hoggatt, Planning Services Manager  
Beth Muckala, Assistant City Attorney  
Ken Danner, Subdivision Development Manager

**Application Summary**

The applicant is requesting to preliminary plat and rezone from RE, Residential Estates District, to CR, Rural Commercial District.

**Neighbor's Comments/Concerns/Responses**

Neighbors asked about operating hours and traffic. The applicant explained they will have normal business hours with some special events in the evenings or on weekends. They plan to be open 6 days a week. March and April will be peak months but at the height of the growing season they only expect 10 people at once as the maximum. Traffic should be similar to other plant nursery traffic. There will be no medical marijuana on site. The applicant intends to keep as many trees on the property as possible to maintain a visual barrier around the perimeter. There will be a monarch butterfly weigh station and pollinator friendly plants. The applicant would like to use this business and property to give back to the community.

**NORMAN PLANNING COMMISSION  
REGULAR SESSION MINUTES**

**DECEMBER 9, 2021**

The Planning Commission of the City of Norman, Cleveland County, State of Oklahoma, met in Regular Session in the Council Chambers of the Norman Municipal Building, 201 West Gray Street, on the 9<sup>th</sup> day of December, 2021.

Notice and agenda of the meeting was posted at the Norman Municipal Building and online at <https://norman-ok.municodemeetings.com> at least twenty-four hours prior to the beginning of the meeting.

Chair Erica Bird called the meeting to order at 6:30 p.m.

\* \* \*

**ROLL CALL**

**MEMBERS PRESENT**

Erin Williford (arrived at 6:32 p.m.)  
Kevan Parker  
Steven McDaniel  
Erica Bird  
Dave Boeck  
Sandy Bahan  
Michael Jablonski

**MEMBERS ABSENT**

Lark Zink

A quorum was present.

**STAFF MEMBERS PRESENT**

Jane Hudson, Director, Planning &  
Community Development  
Lora Hoggatt, Planning Services Manager  
Logan Hubble, Planner I  
Anais Starr, Planner II  
Colton Wayman, Planner I  
Roné Tromble, Recording Secretary  
Ken Danner, Subdivision Development  
Manager  
Kathryn Walker, City Attorney  
Heather Poole, Asst. City Attorney  
Jeanne Snider, Asst. City Attorney  
Jami Short, Traffic Engineer  
Bryce Holland, Multimedia Specialist

\* \* \*

Item No. 5, being:

**CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF RESOLUTION NO. R-2122-64: SANCTUARY GARDENS AND WELLNESS, L.L.C. REQUESTS AMENDMENT OF THE NORMAN 2025 LAND USE AND TRANSPORTATION PLAN FROM COUNTRY RESIDENTIAL DESIGNATION TO COMMERCIAL DESIGNATION FOR APPROXIMATELY 5.99 ACRES OF PROPERTY LOCATED WEST OF OLIPHANT AVENUE BETWEEN ALAMEDA DRIVE AND ALAMEDA STREET.**

**ITEMS SUBMITTED FOR THE RECORD:**

1. NORMAN 2025 Map
2. Staff Report
3. Pre-Development Summary

Item No. 6, being:

**CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF ORDINANCE NO. O-2122-22: SANCTUARY GARDENS AND WELLNESS, L.L.C. REQUESTS REZONING FROM RE, RESIDENTIAL ESTATES DWELLING DISTRICT, TO CR, RURAL COMMERCIAL DISTRICT, FOR APPROXIMATELY 5.99 ACRES OF PROPERTY LOCATED WEST OF OLIPHANT AVENUE BETWEEN ALAMEDA DRIVE AND ALAMEDA STREET.**

**ITEMS SUBMITTED FOR THE RECORD:**

1. Location Map
2. Staff Report
3. Site Plan
4. Pre-Development Summary

Item No. 7, being:

**CONSIDERATION OF APPROVAL, ACCEPTANCE, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF PP-2122-6, A PRELIMINARY PLAT SUBMITTED BY SAFFRON FLETCHER/SANCTUARY GARDENS AND WELLNESS, L.L.C. (NSE ENGINEERING CONSULTANTS) FOR ALAMEDA GARDEN CENTER FOR APPROXIMATELY 5.99 ACRES OF PROPERTY LOCATED WEST OF OLIPHANT AVENUE BETWEEN ALAMEDA DRIVE AND ALAMEDA STREET.**

**ITEMS SUBMITTED FOR THE RECORD:**

1. Location Map
2. Preliminary Plat
3. Staff Report
4. Transportation Impacts
5. Site Plan
6. Pre-Development Summary
7. Greenbelt Commission Action

**PRESENTATION BY STAFF:**

1. Ms. Lora Hoggatt presented the staff report. One protest letter was received which represented 7.6% of the notification area.

**PRESENTATION BY THE APPLICANT:**

The applicant's representative was in attendance, but did not make a presentation.

Commissioner Parker asked a question.

Ms. Saffron Fletcher, the applicant, responded to the question.

Commissioner McDaniel asked a follow-up question.

Ms. Jami Short, Traffic Engineer, responded.

**AUDIENCE PARTICIPATION:**

None



**DISCUSSION AND ACTION BY THE PLANNING COMMISSION:**

Commissioner Boeck made comments.  
Commissioner Parker made comments.  
Commissioner Jablonski made comments.  
Commissioner Williford made comments.  
Ms. Jami Short, Traffic Engineer, responded.

*Michael Jablonski moved to recommend adoption of Resolution No. R-2122-64, Ordinance No. O-2122-22, and PP-2122-6, the Preliminary Plat for ALAMEDA GARDEN CENTER, to City Council. Steven McDaniel seconded the motion.*

There being no further discussion, a vote on the motion was taken with the following result:

YEAS	Erin Williford, Steven McDaniel, Erica Bird, Dave Boeck, Sandy Bahan, Michael Jablonski
NAYES	Kevan Parker
MEMBERS ABSENT	Lark Zink

The motion, to recommend adoption of Resolution No. R-2122-64, Ordinance No. O-2122-22, and PP-2122-6 to City Council, passed by a vote of 6-1.

\* \* \*

PROTESTS – Items 5-7  
Map & Letters

R-2122-64  
O-2122-22  
and PP-2122-6

Sanctuary Gardens & Wellness, L.L.C.

ALAMEDA GARDEN CENTER Preliminary Plat

COPPER TRAIL COS  
ALAMEDA 80 COS

ROCKIN' ROSE FARM COS

RE  
7273-91

A-2  
1322

Tract 1

Tract 2

DOLLAR GEN. ALAMEDA

1  
CR  
1213-12

ALAMEDA DR

TC  
7172-78

ALAMEDA 80 COS

TC  
1862

A-2  
1322

EDA ST

**Subject Tract**

BDL ADD.

ALAMEDA ST

REDBUD ESTATES

REDBUD ESTATES

FLOYD AVE

OLIPHANT AVE

BUMGARDNER AVE

RE  
7172-112

# Protest Map

7.6% Protest Within Notification Area



Map Produced by the City of Norman  
Geographic Information System.  
The City of Norman assumes no  
responsibility for errors or omissions  
in the information presented.



0 150 300 Feet

December 7, 2021



Subject Tract



Notification Area



Protest

Dear Norman Planning Commission,

My name is Maeghan Hennessey and live at 103 Floyd Ave., Norman, OK 73026. I wanted to bring to you concerns my husband, John, and I share regarding the potential for a property next to ours to be rezoned to CR (Rural Commercial District) from RE (Residential Estate Dwelling District), with additional amendment of the NORMAN 2025 Land Use and Transportation Plan. Specifically, the property is located east of 72<sup>nd</sup> Ave., south of Alameda Dr., and north of Alameda St. We PROTEST the possibility of rezoning this property. Sanctuary Gardens & Wellness, L.L.C. and its owners are potential buyers of this property, seeking for the property to be rezoned so they can establish a plant nursery. Our family received a notice of the potential rezoning and ask you to consider this written protest in your decision.

Our property lies directly on the south side of Alameda St. from the property described above. As such, we are direct neighbors of the property up for rezoning consideration, sharing the property's largest inhabited border. We feel the impacts to our property, neighborhood, and family are quite large and oppose the potential for the aforementioned property to be rezoned. Below I highlight our concerns:

1. We are concerned that the location of this business, given the limitations on the size and configuration of the property, will create adverse traffic patterns for our neighborhood (Red Bud Estates). I believe with the exception of Alameda St., our entire neighborhood lives on private maintenance roads. If rezoning is approved and the new business is established in the location, all roads in our neighborhood will experience increased traffic volume, most likely coming from parts of Norman south of the property and including Highway 9. Because homeowners pay for the maintenance of private roads, rezoning of this property to accommodate a plant nursery means we will essentially be subject to a new business at our own cost. The property under question does not fall along any of the private maintenance roads, so the potential buyers will not incur these costs - it will all be on our backs. Moreover, my understanding from previous communication is that the potential buyers of this property plan to place the driveway into the business on Alameda St., greatly increasing the potential for a growing number of vehicles on the private maintenance roadways. The other business in the area does not have an easily accessible driveway on Alameda St., so it will be this new business alone creating different traffic patterns in our area.
2. Increased traffic comes with other risks, to our family in particular. We have five children, some of whom are quite young. I can only imagine the increased traffic posing a risk to our children. We will no longer be able to allow them to walk to our mailbox unsupervised for fear a car will be coming around the corner. We will no longer be able to allow them walk our dog or to ride their bikes down our own road with their siblings because there will be more traffic on it - traffic that does not know there are no less than five families with small children living on our road. The wear and tear on the road will also pose risks to our children while they are riding bikes and may cost us more in car maintenance. If we want to continue these normal activities, which are part of the reasons we moved to a rural residential area, we would have to make changes to our own property (e.g., cutting down trees to increase line of sight), and we shouldn't have to do that.
3. Should the parcel be rezoned and the business opens on it, we will need to make changes to the way we use our own property. Specifically, we will not be able to allow our children to access all parts of their own property because there will be many more people on property directly across the street. As it now stands, the north side of our property abuts Alameda St., directly across from the property under question. We allow our children to walk into that part of our property on their own, most often to collect wild blackberries, but we will no longer feel safe enough to do this with

FILED IN THE OFFICE  
OF THE CITY CLERK  
ON 12-6-21 ss

the increased number of people in close proximity. This will be made further dangerous by the entrance to the rezoned property being almost directly across from our property.

4. Further considering the increased number of people in close proximity to our property: we live on a rose rock strain. The rocks jutting out of the ground on our property next to Alameda St. make the presence of a rose rock strain evident. Because this part of the property is easily accessible from Alameda St. (we do not wish to place fences on our property because much wildlife move through the area), once in a while we will find trespassers looking for and collecting rose rocks on that side of our land. They seem to not know the land is not public, and some do not care (based on communications we've had). The increased number of people visiting the proposed nursery (who I assume will enjoy "natural" things more than the average person) coupled with the obvious presence of rose rocks on our property will likely increase the number of trespassers we experience. This makes us think even harder about the safety of our family should rezoning occur.

5. Finally, and most importantly, we are concerned that "once rezoned, always rezoned." Though a nursery may seem like a benign business for this area, particularly given the rural nature of Ward 5, we feel it is very likely Sanctuary Gardens & Wellness, L.L.C. will outgrow the property under question - it's in the shape of a triangle with Alameda Dr. running through it, utility easements, and a large grade on part of the property, so much of it may not be usable for expansion in a current location. For instance, the potential buyers have stated they wish to cultivate blackberries (wild blackberries are already present on the property) and strawberries for picking by the general public. The property is too small for this venture to be successful, leading us to believe the potential for the business moving locations is high. As another example, the potential buyers wish the plant butterfly gardens, but they places they wish for these to exist are located in areas Cleveland County mows - along Alameda St. and Alameda Dr. Simply put, the county will mow down their butterfly gardens multiple times per year during warmer months. We are concerned with what happens with the "next use" of the property, particularly when the nursery grows larger and moves locations. Many of the businesses on the list of "acceptable" businesses in property zoned CR (Rural Commercial District) are not the kinds of businesses we want across the street from our home. We moved to a rural area to get away from those types of businesses, and we are very concerned about the possibility they will be encroaching on us.

Considering all these issues, we feel this rezoning proposal is in direct conflict with the quiet, rural lifestyle we and many others in Ward 5 wish to live. We also ask you to consider that the business asking for rezoning of this plot does not own the plot, and that the owner has received other offers to buy the property that do not come with rezoning requests. We ask the Norman Planning Commission consider favorably our PROTEST to rezone this particular plot of land. Though it may seem like a small issue in the larger role of the Planning Commission, I assure you it is not a small matter to us.

Maeghan Hennessey and John Hennessey




103 Floyd Ave.  
Norman, OK 73026  
405-366-8977 (h)  
405-537-4975 (c)

**File Attachments for Item:**

18. CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF ORDINANCE O-2122-22 UPON SECOND AND FINAL READING: AN ORDINANCE OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, AMENDING SECTION 460 OF CHAPTER 22 OF THE CODE OF THE CITY OF NORMAN SO AS TO REMOVE PART OF THE WEST HALF (W/2) OF THE SOUTHWEST QUARTER (SW/4) OF SECTION TWENTY-NINE (29), TOWNSHIP NINE (9) NORTH, RANGE ONE (1) WEST OF THE INDIAN MERIDIAN, TO NORMAN, CLEVELAND COUNTY, OKLAHOMA, FROM THE RE, RESIDENTIAL ESTATES DWELLING DISTRICT AND PLACE THE SAME IN THE CR, RURAL COMMERCIAL DISTRICT, OF SAID CITY; AND PROVIDING FOR THE SEVERABILITY THEREOF. (WEST OF OLIPHANT AVENUE BETWEEN ALAMEDA DRIVE AND ALAMEDA STREET)



## CITY OF NORMAN, OK STAFF REPORT

**MEETING DATE:** 01/25/2022

**REQUESTER:** Saffron Fletcher, Sanctuary Gardens & Wellness, L.L.C.

**PRESENTER:** Jane Hudson, Director, Planning & Community Development

**ITEM TITLE:** CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF ORDINANCE O-2122-22 UPON SECOND AND FINAL READING: AN ORDINANCE OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, AMENDING SECTION 460 OF CHAPTER 22 OF THE CODE OF THE CITY OF NORMAN SO AS TO REMOVE PART OF THE WEST HALF (W/2) OF THE SOUTHWEST QUARTER (SW/4) OF SECTION TWENTY-NINE (29), TOWNSHIP NINE (9) NORTH, RANGE ONE (1) WEST OF THE INDIAN MERIDIAN, TO NORMAN, CLEVELAND COUNTY, OKLAHOMA, FROM THE RE, RESIDENTIAL ESTATES DWELLING DISTRICT AND PLACE THE SAME IN THE CR, RURAL COMMERCIAL DISTRICT, OF SAID CITY; AND PROVIDING FOR THE SEVERABILITY THEREOF. (WEST OF OLIPHANT AVENUE BETWEEN ALAMEDA DRIVE AND ALAMEDA STREET)

**SYNOPSIS:** The applicant, Sanctuary Gardens and Wellness, L.L.C., is requesting to rezone from RE, Residential Estates District, to CR, Rural Commercial District, to allow for a plant nursery, pollinator plants, and monarch butterfly waystation. The subject property is approximately 5.99 acres and a preliminary plat is also part of this application.

**HISTORY:** The subject property was rezoned from A-2, Rural Agricultural District, to RE, Residential Estates District, as part of a large rezoning of 240 acres approved by City Council on July 24, 1973. This area of Alameda Dr. has commercial development. The Dollar General across Alameda Dr. to the north was rezoned to CR, Rural Commercial District, with Ordinance O-1213-12 approved by City Council on October 23, 2012. The parcel directly adjacent to the subject parcel on the west side of N. Oliphant Ave. and the parcel on the east side of N. Oliphant Ave. were rezoned to TC, Tourist Commercial, with Ordinance No. 1862 approved by City Council on February 22, 1966.

### **ZONING ORDINANCE CITATION:**

SEC 424.3 – CR, RURAL COMMERCIAL DISTRICT

General Description. This commercial district is intended for the conduct of retail trade and to provide personal services to meet the regular needs and convenience of rural residents. It is



anticipated that this district will be the predominately used commercial district in rural Norman. It is intended that this zoning district be located at the intersection of improved section line roads.

**EXISTING ZONING:** The subject project is currently zoned RE, Residential Estates District. This zoning district allows only for residential and accessory uses. A commercial plant nursery would not be permitted in RE.

**ANALYSIS:** As stated above, the CR zoning district is meant to be the predominant commercial district in rural Norman. CR is also meant to be located at the intersection of improved section line roads; the subject property is located at the northeast corner of Alameda Street and 72nd Ave. N.E.

**SITE PLAN:** The proposed site plan shows three buildings: an office, a guttered greenhouse, and a single prothouse greenhouse. Adequate parking will be supplied for employees and customers. There are two proposed access points, one off Alameda Dr. and one off Alameda St. The site plan shows a dumpster enclosure on the north side of the parking lot. There is a detention basin in the southeast corner of the property. Because this application includes a preliminary plat and site development plan, the owner of the property will be required to follow the submitted site plan when obtaining building permits.

**OPEN SPACE:** Open space is not required in CR zoning, however, the applicant proposes to keep most of the property open to allow for plants and the monarch butterfly waystation. The development will follow all setback and landscaping requirements for this zoning district and use. The site plan shows a 10,000 square foot space to be used as a bearing orchard.

**USE:** The applicant proposes to use the property as a plant nursery and monarch butterfly waystation. There are a number of other commercial uses allowed by right, including:

- Artist material supply, studio, or hobby shop.
- Automobile service station.
- Bank.
- Barber shop, or beauty parlor.
- Child care center.
- Clothing and dry goods store.
- Farm Feed store.
- Firewood sales.
- Florist.
- Grocery or supermarket.
- Hardware store.
- Key shop.
- Medical Marijuana Dispensary, as allowed by state law.
- Office building and offices for such professional services as accountant, architect, attorney, business or management consultant, court reporter, dentist or dental surgeon, engineer, geologist or geophysicist, linguist, landscape architect, optometrist, optician, osteopathic physician, planning consultant, psychologist, physician or surgeon, or registered nurse. Funeral homes and mortuaries shall not be considered professional services permitted in this district.

- Pharmacy.
- Plant nursery.
- News stand and tobacco store.
- Restaurant.
- Retail spirits store.
- Shoe store or repair shop.
- Tier I Medical Marijuana Processor, as allowed by state law.
- Tier II Medical Marijuana Processor, as allowed by state law.

No individual use shall exceed a Gross Floor Area of 35,000 square feet.

**OTHER AGENCY COMMENTS:**

**PARK BOARD:** Because this is a commercial development, the preliminary plat was not required to go before the Park Board.

**PUBLIC WORKS:** Alameda Drive and Alameda Street are existing. A 20' trail easement will be adjacent to Alameda Drive. There are no public utilities (water and sanitary sewer) to serve the property. Private systems will be utilized with City and ODEQ approvals. Stormwater runoff will be controlled by a proposed privately-maintained detention facility.

**PREDEVELOPMENT:** PD-27, July 22, 2021

Neighbors asked about operating hours and traffic. The applicant explained they will have normal business hours with some special events in the evenings or on weekends. They plan to be open 6 days a week. March and April will be peak months but at the height of the growing season they only expect 10 people at once as the maximum. Traffic should be similar to other plant nursery facilities. There will be no medical marijuana on site. The applicant intends to keep as many trees on the property as possible to maintain a visual barrier around the perimeter. There will be a monarch butterfly waystation and pollinator friendly plants. The applicant would like to use this business and property to give back to the community.

**GREENBELT COMMISSION:** November 15, 2021

Greenbelt forwards this item with no additional comments.

**CONCLUSION:** Staff forwards this request for rezoning to Rural Commercial District and Ordinance O-2122-22 to City Council for your consideration.

At their meeting of December 9, 2021, Planning Commission recommended adoption of Ordinance O-2122-22 by a vote of 6-1.

O-2122-22

AN ORDINANCE OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, AMENDING SECTION 460 OF CHAPTER 22 OF THE CODE OF THE CITY OF NORMAN SO AS TO REMOVE PART OF THE WEST HALF (W/2) OF THE SOUTHWEST QUARTER (SW/4) OF SECTION TWENTY-NINE (29), TOWNSHIP NINE (9) NORTH, RANGE ONE (1) WEST OF THE INDIAN MERIDIAN, TO NORMAN, CLEVELAND COUNTY, OKLAHOMA, FROM THE RE, RESIDENTIAL ESTATES DWELLING DISTRICT AND PLACE THE SAME IN THE CR, RURAL COMMERCIAL DISTRICT, OF SAID CITY; AND PROVIDING FOR THE SEVERABILITY THEREOF. (West of Oliphant Avenue between Alameda Drive and Alameda Street)

- § 1. WHEREAS, Sanctuary Gardens and Wellness, L.L.C., with the permission of the owners: Mark Cox, Charles Cotton, and Joe Alexander, has made application to have the property described below removed from the RE, Residential Estates Dwelling District, and to have the same placed in the CR, Rural Commercial District; and
- § 2. WHEREAS, said application has been referred to the Planning Commission of said City and said body has, after conducting a public hearing as required by law, considered the same and recommended that the same should be granted and an ordinance adopted to effect and accomplish such rezoning; and
- § 3. WHEREAS, the City Council of the City of Norman, Oklahoma, has thereafter considered said application and has determined that said application should be granted and an ordinance adopted to effect and accomplish such rezoning.

NOW, THEREFORE, BE IT ORDAINED BY THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA:

- § 4. That Section 460 of Chapter 22 of the Code of the City of Norman, Oklahoma, is hereby amended so as to remove the following described property from the RE, Residential Estates Dwelling District, and to place the same in the CR, Rural Commercial District, to wit:

All of the West Half (W/2) of the Southwest Quarter (SW/4) of Section Twenty-nine (29), Township Nine (9) North, Range One (1) West, of the Indian Meridian, Cleveland County, Oklahoma, Less and Except Tracts One

and Two (1 & 2) of ALAMEDA 80 CERTIFICATE OF SURVEY. Said Tract being more particularly described as follows:

A tract of land being a part of the West Half (W/2) of the Southwest Quarter (SW/4) of Section Twenty-nine (29), Township Nine (9) North, Range One (1) West, of the Indian Meridian, Cleveland County, Oklahoma, more particularly described as follows:

BEGINNING at the Southwest Corner of said Southwest Quarter (SW/4) of Section 29; thence N00°44'11"W along the West line of said SW/4 a distance of 81.50 feet to a point on the North Right of Way line of Alameda Drive and the Southwest Corner of Tract 2 of the ALAMEDA 80 CERTIFICATE OF SURVEY; thence along said Right of Way and South line of Tract 2, on a curve to the left with an arc length of 1350.44, with a radius of 5679.70 feet, a chord bearing of N77°29'40"E, and a chord length of 1347.26 to a point on the East line of the West Half (W/2) of the Southwest Quarter (SW/4); thence S00°47'32"E along said East line a distance of 368.99 feet to the Southeast Corner of the W/2 SW/4; thence S89°49'57"W along the South line of said SW/4 a distance of 1318.97 feet to the point of beginning.

Said Tract contains  $\pm 261042.56$  square feet, or  $\pm 5.99$  acres.

- § 5. Severability. If any section, subsection, sentence, clause, phrase, or portion of this ordinance is, for any reason, held invalid or unconstitutional by any court of competent jurisdiction, such portion shall be deemed a separate, distinct, and independent provision, and such holding shall not affect the validity of the remaining portions of this ordinance.

ADOPTED this \_\_\_\_\_ day of

NOT ADOPTED this \_\_\_\_\_ day of

\_\_\_\_\_, 2022.

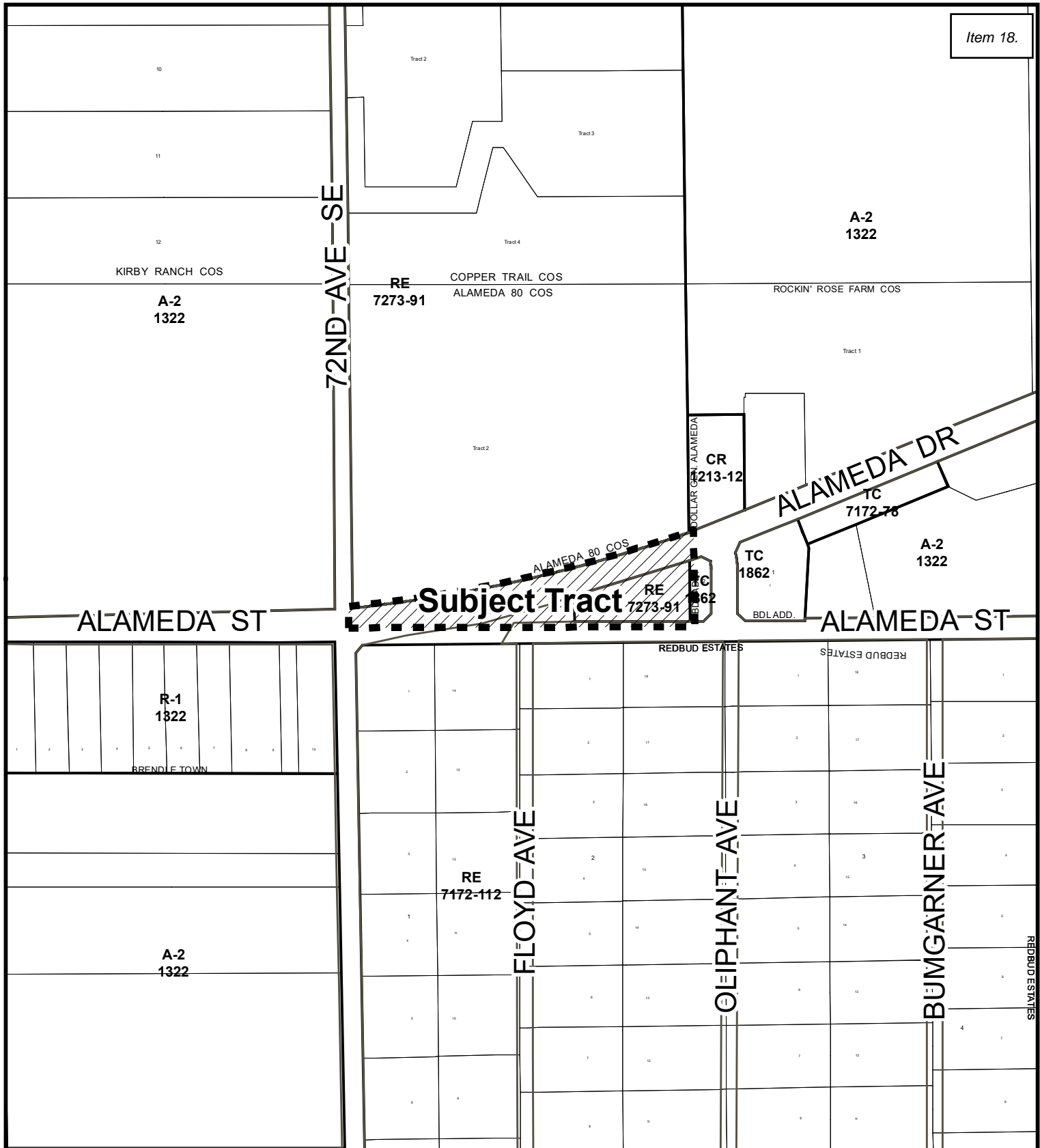
\_\_\_\_\_, 2022.

\_\_\_\_\_  
(Mayor)

\_\_\_\_\_  
(Mayor)

ATTEST:

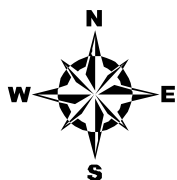
\_\_\_\_\_  
(City Clerk)



# Location Map




Map Produced by the City of Norman  
Geographic Information System.  
The City of Norman assumes no  
responsibility for errors or omissions  
in the information presented.



November 16, 2021

0 250 500 Ft.

 Subject Tract

 Zoning

**Applicant:** Saffron Fletcher, Sanctuary Gardens & Wellness, LLC

**Project Location:** West of Oliphant Avenue north of Alameda Street

**Case Number:** PD21-27

**Time:** 6:00 p.m.

**Applicant/Representative**

Saffron Fletcher  
Jason Fletcher  
Cedric LeBlanc  
Celeste LeBlanc  
Dylan West

**Attendees**

Tim Sherban  
Kim Sherban  
Brandi Rice  
Mark Cox

**City Staff**

Lora Hoggatt, Planning Services Manager  
Beth Muckala, Assistant City Attorney  
Ken Danner, Subdivision Development Manager

**Application Summary**

The applicant is requesting to preliminary plat and rezone from RE, Residential Estates District, to CR, Rural Commercial District.

**Neighbor's Comments/Concerns/Responses**

Neighbors asked about operating hours and traffic. The applicant explained they will have normal business hours with some special events in the evenings or on weekends. They plan to be open 6 days a week. March and April will be peak months but at the height of the growing season they only expect 10 people at once as the maximum. Traffic should be similar to other plant nursery traffic. There will be no medical marijuana on site. The applicant intends to keep as many trees on the property as possible to maintain a visual barrier around the perimeter. There will be a monarch butterfly weigh station and pollinator friendly plants. The applicant would like to use this business and property to give back to the community.

ORDINANCE NO. O-2122-22

ITEM NO. 6

**STAFF REPORT****GENERAL INFORMATION**

APPLICANT	Sanctuary Gardens and Wellness, L.L.C.
REQUESTED ACTION	Rezoning to CR, Rural Commercial District
EXISTING ZONING	RE, Residential Estates Dwelling District
SURROUNDING ZONING	North: RE, Residential Estates District East: TC, Tourist Commercial District South: RE, Residential Estates District West: RE, Residential Estates District
LOCATION	West of Oliphant Avenue between Alameda Drive and Alameda Street
SIZE	5.99 acres, more or less
PURPOSE	Plant nursery
EXISTING LAND USE	Vacant
SURROUNDING LAND USE	North: Vacant, Dollar General East: Vacant, Alameda Market South: Single-family residential West: Single-family residential
CURRENT LAND USE PLAN DESIGNATION	Country Residential
PROPOSED LAND USE PLAN DESIGNATION	Commercial

**SYNOPSIS:** The applicant, Sanctuary Gardens and Wellness, L.L.C., is requesting to rezone from RE, Residential Estates District, to CR, Rural Commercial District, to allow for a plant nursery, pollinator plants, and monarch butterfly waystation. The subject property is approximately 5.99 acres and a preliminary plat is also part of this application.

**HISTORY:** The subject property was rezoned from A-2, Rural Agricultural District, to RE, Residential Estates District, as part of a large rezoning of 240 acres approved by City Council on July 24, 1973. This area of Alameda Dr. has commercial development. The Dollar General across Alameda Dr. to the north was rezoned to CR, Rural Commercial District, with Ordinance



No. O-1213-12 approved by City Council on October 23, 2012. The parcel directly adjacent to the subject parcel on the west side of N. Oliphant Ave. and the parcel on the east side of N. Oliphant Ave. were rezoned to TC, Tourist Commercial, with Ordinance No. 1862 approved by City Council on February 22, 1966.

**ZONING ORDINANCE CITATION:** SEC 424.3 – CR, RURAL COMMERCIAL DISTRICT

**General Description.** This commercial district is intended for the conduct of retail trade and to provide personal services to meet the regular needs and convenience of rural residents. It is anticipated that this district will be the predominately used commercial district in rural Norman. It is intended that this zoning district be located at the intersection of improved section line roads.

**EXISTING ZONING:** The subject project is currently zoned RE, Residential Estates District. This zoning district allows only for residential and accessory uses. A commercial plant nursery would not be permitted in RE.

**ANALYSIS:** As stated above, the CR zoning district is meant to be the predominant commercial district in rural Norman. CR is also meant to be located at the intersection of improved section line roads; the subject property is located at the northeast corner of Alameda Street and 72<sup>nd</sup> Ave. N.E.

**SITE PLAN:** The proposed site plan shows three buildings: an office, a guttered greenhouse, and a single propthouse greenhouse. Adequate parking will be supplied for employees and customers. There are two proposed access points, one off Alameda Dr. and one off Alameda St. The site plan shows a dumpster enclosure on the north side of the parking lot. There is a detention basin in the southeast corner of the property. Because this application includes a preliminary plat and site development plan, the owner of the property will be required to follow the submitted site plan when obtaining building permits.

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**USE:** The applicant proposes to use the property as a plant nursery and monarch butterfly waystation. There are a number of other commercial uses allowed by right, including:

- Artist material supply, studio, or hobby shop.
- Automobile service station.
- Bank.
- Barber shop, or beauty parlor.
- Child care center.
- Clothing and dry goods store.
- Farm Feed store.
- Firewood sales.
- Florist.
- Grocery or supermarket.
- Hardware store.
- Key shop.
- Medical Marijuana Dispensary, as allowed by state law.

- Office building and offices for such professional services as accountant, architect, attorney, business or management consultant, court reporter, dentist or dental surgeon, engineer, geologist or geophysicist, linguist, landscape architect, optometrist, optician, osteopathic physician, planning consultant, psychologist, physician or surgeon, or registered nurse. Funeral homes and mortuaries shall not be considered professional services permitted in this district.
- Pharmacy.
- Plant nursery.
- News stand and tobacco store.
- Restaurant.
- Retail spirits store.
- Shoe store or repair shop.
- Tier I Medical Marijuana Processor, as allowed by state law.
- Tier II Medical Marijuana Processor, as allowed by state law.

No individual use shall exceed a Gross Floor Area of 35,000 square feet.

**OTHER AGENCY COMMENTS:**

PARK BOARD: Because this is a commercial development, the preliminary plat was not required to go before the Park Board.

PUBLIC WORKS: Alameda Drive and Alameda Street are existing. A 20' trail easement will be adjacent to Alameda Drive. There are no public utilities (water and sanitary sewer) to serve the property. Private systems will be utilized with City and ODEQ approvals. Stormwater runoff will be controlled by a proposed privately-maintained detention facility.

PREDEVELOPMENT: PD-27, July 22, 2021

Neighbors asked about operating hours and traffic. The applicant explained they will have normal business hours with some special events in the evenings or on weekends. They plan to be open 6 days a week. March and April will be peak months but at the height of the growing season they only expect 10 people at once as the maximum. Traffic should be similar to other plant nursery facilities. There will be no medical marijuana on site. The applicant intends to keep as many trees on the property as possible to maintain a visual barrier around the perimeter. There will be a monarch butterfly waystation and pollinator friendly plants. The applicant would like to use this business and property to give back to the community.

GREENBELT COMMISSION: November 15, 2021

Greenbelt forwards this item with no additional comments.

**CONCLUSION:** Staff forwards this request for rezoning to Rural Commercial District and Ordinance O-2122-22 to Planning Commission for your consideration.

## LOT IMPERVIOUS CALCULATIONS

Item	Existing	Proposed
Lot Area	5.99 Ac	5.99 Ac
Lot Area outside Public ROW	2.15 Ac	2.15 Ac
Paved Public Roads	39175 SF	39175 SF
Building	90 SF	890 SF
Pavement Impervious	1360 SF	2160 SF
Pavement Previous		10562 SF
Accessory Buildings (Green Houses)		5706 SF

# SITE PLAN

## ALAMEDA GARDEN CENTER

### A PART OF THE SW/4 OF SECTION 29, T9N,R1W,IM



#### NSE ENGINEERING CONSULTANTS

2800 WEST LINDEY STREET  
SUITE 111  
NORMAN, OK 73072  
TEL 405-244-2900  
FAX 405-244-4739  
www.nseengineering.com

OLYMPIA COUNTY OF PROFESSIONAL  
ENGINEERS AND LAND SURVEYORS  
LICENSE NO. 10000  
EXPIRES JAN 15, 2022

TAHIR M. NASIR, P.E.  
LICENSED PROFESSIONAL ENGINEER  
STATE OF OKLAHOMA LICENSE NO. 10000  
EXPIRES JAN 15, 2022



#### ALAMEDA GARDEN CENTER

7450 ALAMEDA DRIVE, NORMAN,  
CLEVELAND COUNTY, OKLAHOMA

SHEET TITLE: OVERALL SITE PLAN

Revision:  
Issue Date:  
11.30.2021

Project No:  
2123

Sheet No:  
C02

Scale: 1" = 50'  
Scale in Feet

PRINT SIZE: 24"x36"

**REFERENCE:**  
1. SURVEY INFORMATION SHOWN ON THIS PLAN IS BASED ON THE SURVEY DATA FILED WITH THE OKLAHOMA STATE DEPARTMENT OF REVENUE, LAND RECORDS, IN 2011.  
2. THE DATA AND INFORMATION SHOWN ON THIS PLAN IS BASED ON A SURVEY BY PROFESSIONAL CONSULTANT/ENGINEER/ARCHITECT, MR. T. M. NASIR, P.E., IN 2021.  
3. THE DATA AND INFORMATION SHOWN ON THIS PLAN IS BASED ON A SURVEY BY PROFESSIONAL CONSULTANT/ENGINEER/ARCHITECT, MR. T. M. NASIR, P.E., IN 2021.  
4. THE DATA AND INFORMATION SHOWN ON THIS PLAN IS BASED ON A SURVEY BY PROFESSIONAL CONSULTANT/ENGINEER/ARCHITECT, MR. T. M. NASIR, P.E., IN 2021.

COPPER TRAIL COS  
ALAMEDA 80 COS

ROCKIN' ROSE FARM COS

A-2  
1322

Tract 1

RE  
7273-91

Tract 2

DOLLAR GEN. ALAMEDA

1  
CR  
1213-12

ALAMEDA DR

TC  
7172-78

ALAMEDA 80 COS

TC  
1862

A-2  
1322

BDL ADD.

ALAMEDA ST

EDA ST

**Subject Tract**

REDBUD ESTATES

REDBUD ESTATES

FLOYD AVE

OLIPHANT AVE

BUMGARDNER AVE

RE  
7172-112

# Protest Map

7.6% Protest Within Notification Area



Map Produced by the City of Norman  
Geographic Information System.  
The City of Norman assumes no  
responsibility for errors or omissions  
in the information presented.



0 150 300 Feet

December 7, 2021



Subject Tract



Notification Area



Protest

Dear Norman Planning Commission,

My name is Maeghan Hennessey and live at 103 Floyd Ave., Norman, OK 73026. I wanted to bring to you concerns my husband, John, and I share regarding the potential for a property next to ours to be rezoned to CR (Rural Commercial District) from RE (Residential Estate Dwelling District), with additional amendment of the NORMAN 2025 Land Use and Transportation Plan. Specifically, the property is located east of 72<sup>nd</sup> Ave., south of Alameda Dr., and north of Alameda St. We PROTEST the possibility of rezoning this property. Sanctuary Gardens & Wellness, L.L.C. and its owners are potential buyers of this property, seeking for the property to be rezoned so they can establish a plant nursery. Our family received a notice of the potential rezoning and ask you to consider this written protest in your decision.

Our property lies directly on the south side of Alameda St. from the property described above. As such, we are direct neighbors of the property up for rezoning consideration, sharing the property's largest inhabited border. We feel the impacts to our property, neighborhood, and family are quite large and oppose the potential for the aforementioned property to be rezoned. Below I highlight our concerns:

1. We are concerned that the location of this business, given the limitations on the size and configuration of the property, will create adverse traffic patterns for our neighborhood (Red Bud Estates). I believe with the exception of Alameda St., our entire neighborhood lives on private maintenance roads. If rezoning is approved and the new business is established in the location, all roads in our neighborhood will experience increased traffic volume, most likely coming from parts of Norman south of the property and including Highway 9. Because homeowners pay for the maintenance of private roads, rezoning of this property to accommodate a plant nursery means we will essentially be subject to a new business at our own cost. The property under question does not fall along any of the private maintenance roads, so the potential buyers will not incur these costs - it will all be on our backs. Moreover, my understanding from previous communication is that the potential buyers of this property plan to place the driveway into the business on Alameda St., greatly increasing the potential for a growing number of vehicles on the private maintenance roadways. The other business in the area does not have an easily accessible driveway on Alameda St., so it will be this new business alone creating different traffic patterns in our area.
2. Increased traffic comes with other risks, to our family in particular. We have five children, some of whom are quite young. I can only imagine the increased traffic posing a risk to our children. We will no longer be able to allow them to walk to our mailbox unsupervised for fear a car will be coming around the corner. We will no longer be able to allow them walk our dog or to ride their bikes down our own road with their siblings because there will be more traffic on it - traffic that does not know there are no less than five families with small children living on our road. The wear and tear on the road will also pose risks to our children while they are riding bikes and may cost us more in car maintenance. If we want to continue these normal activities, which are part of the reasons we moved to a rural residential area, we would have to make changes to our own property (e.g., cutting down trees to increase line of sight), and we shouldn't have to do that.
3. Should the parcel be rezoned and the business opens on it, we will need to make changes to the way we use our own property. Specifically, we will not be able to allow our children to access all parts of their own property because there will be many more people on property directly across the street. As it now stands, the north side of our property abuts Alameda St., directly across from the property under question. We allow our children to walk into that part of our property on their own, most often to collect wild blackberries, but we will no longer feel safe enough to do this with

FILED IN THE OFFICE  
OF THE CITY CLERK  
ON 12-6-21 ss

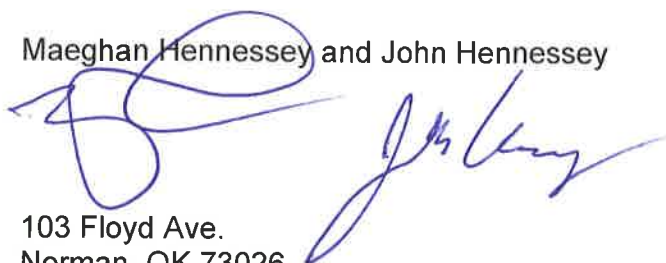
the increased number of people in close proximity. This will be made further dangerous by the entrance to the rezoned property being almost directly across from our property.

4. Further considering the increased number of people in close proximity to our property: we live on a rose rock strain. The rocks jutting out of the ground on our property next to Alameda St. make the presence of a rose rock strain evident. Because this part of the property is easily accessible from Alameda St. (we do not wish to place fences on our property because much wildlife move through the area), once in a while we will find trespassers looking for and collecting rose rocks on that side of our land. They seem to not know the land is not public, and some do not care (based on communications we've had). The increased number of people visiting the proposed nursery (who I assume will enjoy "natural" things more than the average person) coupled with the obvious presence of rose rocks on our property will likely increase the number of trespassers we experience. This makes us think even harder about the safety of our family should rezoning occur.

5. Finally, and most importantly, we are concerned that "once rezoned, always rezoned." Though a nursery may seem like a benign business for this area, particularly given the rural nature of Ward 5, we feel it is very likely Sanctuary Gardens & Wellness, L.L.C. will outgrow the property under question - it's in the shape of a triangle with Alameda Dr. running through it, utility easements, and a large grade on part of the property, so much of it may not be usable for expansion in a current location. For instance, the potential buyers have stated they wish to cultivate blackberries (wild blackberries are already present on the property) and strawberries for picking by the general public. The property is too small for this venture to be successful, leading us to believe the potential for the business moving locations is high. As another example, the potential buyers wish the plant butterfly gardens, but they places they wish for these to exist are located in areas Cleveland County mows - along Alameda St. and Alameda Dr. Simply put, the county will mow down their butterfly gardens multiple times per year during warmer months. We are concerned with what happens with the "next use" of the property, particularly when the nursery grows larger and moves locations. Many of the businesses on the list of "acceptable" businesses in property zoned CR (Rural Commercial District) are not the kinds of businesses we want across the street from our home. We moved to a rural area to get away from those types of businesses, and we are very concerned about the possibility they will be encroaching on us.

Considering all these issues, we feel this rezoning proposal is in direct conflict with the quiet, rural lifestyle we and many others in Ward 5 wish to live. We also ask you to consider that the business asking for rezoning of this plot does not own the plot, and that the owner has received other offers to buy the property that do not come with rezoning requests. We ask the Norman Planning Commission consider favorably our PROTEST to rezone this particular plot of land. Though it may seem like a small issue in the larger role of the Planning Commission, I assure you it is not a small matter to us.

Maeghan Hennessey and John Hennessey



103 Floyd Ave.  
Norman, OK 73026  
405-366-8977 (h)  
405-537-4975 (c)

**NORMAN PLANNING COMMISSION  
REGULAR SESSION MINUTES**

**DECEMBER 9, 2021**

The Planning Commission of the City of Norman, Cleveland County, State of Oklahoma, met in Regular Session in the Council Chambers of the Norman Municipal Building, 201 West Gray Street, on the 9<sup>th</sup> day of December, 2021.

Notice and agenda of the meeting was posted at the Norman Municipal Building and online at <https://norman-ok.municodemeetings.com> at least twenty-four hours prior to the beginning of the meeting.

Chair Erica Bird called the meeting to order at 6:30 p.m.

\* \* \*

**ROLL CALL**

**MEMBERS PRESENT**

Erin Williford (arrived at 6:32 p.m.)  
Kevan Parker  
Steven McDaniel  
Erica Bird  
Dave Boeck  
Sandy Bahan  
Michael Jablonski

**MEMBERS ABSENT**

Lark Zink

A quorum was present.

**STAFF MEMBERS PRESENT**

Jane Hudson, Director, Planning &  
Community Development  
Lora Hoggatt, Planning Services Manager  
Logan Hubble, Planner I  
Anais Starr, Planner II  
Colton Wayman, Planner I  
Roné Tromble, Recording Secretary  
Ken Danner, Subdivision Development  
Manager  
Kathryn Walker, City Attorney  
Heather Poole, Asst. City Attorney  
Jeanne Snider, Asst. City Attorney  
Jami Short, Traffic Engineer  
Bryce Holland, Multimedia Specialist

\* \* \*



Item No. 5, being:

**CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF RESOLUTION NO. R-2122-64: SANCTUARY GARDENS AND WELLNESS, L.L.C. REQUESTS AMENDMENT OF THE NORMAN 2025 LAND USE AND TRANSPORTATION PLAN FROM COUNTRY RESIDENTIAL DESIGNATION TO COMMERCIAL DESIGNATION FOR APPROXIMATELY 5.99 ACRES OF PROPERTY LOCATED WEST OF OLIPHANT AVENUE BETWEEN ALAMEDA DRIVE AND ALAMEDA STREET.**

**ITEMS SUBMITTED FOR THE RECORD:**

1. NORMAN 2025 Map
2. Staff Report
3. Pre-Development Summary

Item No. 6, being:

**CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF ORDINANCE NO. O-2122-22: SANCTUARY GARDENS AND WELLNESS, L.L.C. REQUESTS REZONING FROM RE, RESIDENTIAL ESTATES DWELLING DISTRICT, TO CR, RURAL COMMERCIAL DISTRICT, FOR APPROXIMATELY 5.99 ACRES OF PROPERTY LOCATED WEST OF OLIPHANT AVENUE BETWEEN ALAMEDA DRIVE AND ALAMEDA STREET.**

**ITEMS SUBMITTED FOR THE RECORD:**

1. Location Map
2. Staff Report
3. Site Plan
4. Pre-Development Summary

Item No. 7, being:

**CONSIDERATION OF APPROVAL, ACCEPTANCE, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF PP-2122-6, A PRELIMINARY PLAT SUBMITTED BY SAFFRON FLETCHER/SANCTUARY GARDENS AND WELLNESS, L.L.C. (NSE ENGINEERING CONSULTANTS) FOR ALAMEDA GARDEN CENTER FOR APPROXIMATELY 5.99 ACRES OF PROPERTY LOCATED WEST OF OLIPHANT AVENUE BETWEEN ALAMEDA DRIVE AND ALAMEDA STREET.**

**ITEMS SUBMITTED FOR THE RECORD:**

1. Location Map
2. Preliminary Plat
3. Staff Report
4. Transportation Impacts
5. Site Plan
6. Pre-Development Summary
7. Greenbelt Commission Action

**PRESENTATION BY STAFF:**

1. Ms. Lora Hoggatt presented the staff report. One protest letter was received which represented 7.6% of the notification area.

**PRESENTATION BY THE APPLICANT:**

The applicant's representative was in attendance, but did not make a presentation.

Commissioner Parker asked a question.

Ms. Saffron Fletcher, the applicant, responded to the question.

Commissioner McDaniel asked a follow-up question.

Ms. Jami Short, Traffic Engineer, responded.

**AUDIENCE PARTICIPATION:**

None

**DISCUSSION AND ACTION BY THE PLANNING COMMISSION:**

Commissioner Boeck made comments.  
Commissioner Parker made comments.  
Commissioner Jablonski made comments.  
Commissioner Williford made comments.  
Ms. Jami Short, Traffic Engineer, responded.

*Michael Jablonski moved to recommend adoption of Resolution No. R-2122-64, Ordinance No. O-2122-22, and PP-2122-6, the Preliminary Plat for ALAMEDA GARDEN CENTER, to City Council. Steven McDaniel seconded the motion.*

There being no further discussion, a vote on the motion was taken with the following result:

YEAS	Erin Williford, Steven McDaniel, Erica Bird, Dave Boeck, Sandy Bahan, Michael Jablonski
NAYES	Kevan Parker
MEMBERS ABSENT	Lark Zink

The motion, to recommend adoption of Resolution No. R-2122-64, Ordinance No. O-2122-22, and PP-2122-6 to City Council, passed by a vote of 6-1.

\* \* \*

**File Attachments for Item:**

19. CONSIDERATION OF APPROVAL, ACCEPTANCE, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF A PRELIMINARY PLAT FOR ALAMEDA GARDEN CENTER AND A WAIVER FOR DRIVEWAY SPACING. (GENERALLY LOCATED WEST OF OLIPHANT AVENUE BETWEEN ALAMEDA DRIVE AND ALAMEDA STREET).



## CITY OF NORMAN, OK STAFF REPORT

**MEETING DATE:** 01/25/2022

**REQUESTER:** Ken Danner, Subdivision Development Manager

**PRESENTER:** Shawn O'Leary, Director of Public Works

**TITLE:** CONSIDERATION OF APPROVAL, ACCEPTANCE, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF A PRELIMINARY PLAT FOR ALAMEDA GARDEN CENTER AND A WAIVER FOR DRIVEWAY SPACING. (GENERALLY LOCATED WEST OF OLIPHANT AVENUE BETWEEN ALAMEDA DRIVE AND ALAMEDA STREET).

### BACKGROUND:

This item is a preliminary plat for Alameda Garden Center generally located west of Oliphant Avenue between Alameda Drive and Alameda Street.

Planning Commission at its meeting of December 9, 2021, recommended to City Council amending the NORMAN 2025 Land Use and Transportation Plan for this property from Country Residential Designation to Commercial Designation. Also, Planning Commission recommended that this property be placed in the CR, Rural Commercial District, from RE, Residential Estates District. In addition, Planning Commission, on December 9, 2021, recommended to City Council the approval of the preliminary plat for Alameda Garden Center.

### DISCUSSION:

The proposed 800 square foot garden center in this Preliminary Plat (PP) is expected to generate approximately 55 trips per day, 2 AM peak hour trips, and 6 PM peak hour trips. Obviously being below the threshold for when a traffic impact study is required (>100 peak hour trips is the threshold), the developer submitted a traffic impact memorandum documenting the trip generation information for this addition. The development is proposed for location on the south side of Alameda Drive approximately 300 feet west of Oliphant Avenue.

STREET	NO. OF LANES	EXISTING TRAFFIC (Veh/day)	PROJECTED TRAFFIC (Veh/day)	TOTAL PROJECTED TRAFFIC (Veh/day)	ROADWAY CAPACITY L.O.S. "E"	% CAPACITY USED (EXISTING)	% CAPACITY USE (PROJECTED)
Alameda Drive	2	2,500	55	2,555	17,100	14.62	14.94

The proposed addition will access Alameda Drive to the north of the development and Alameda Street to the south. The access to Alameda Street does not meet the requirements for driveway

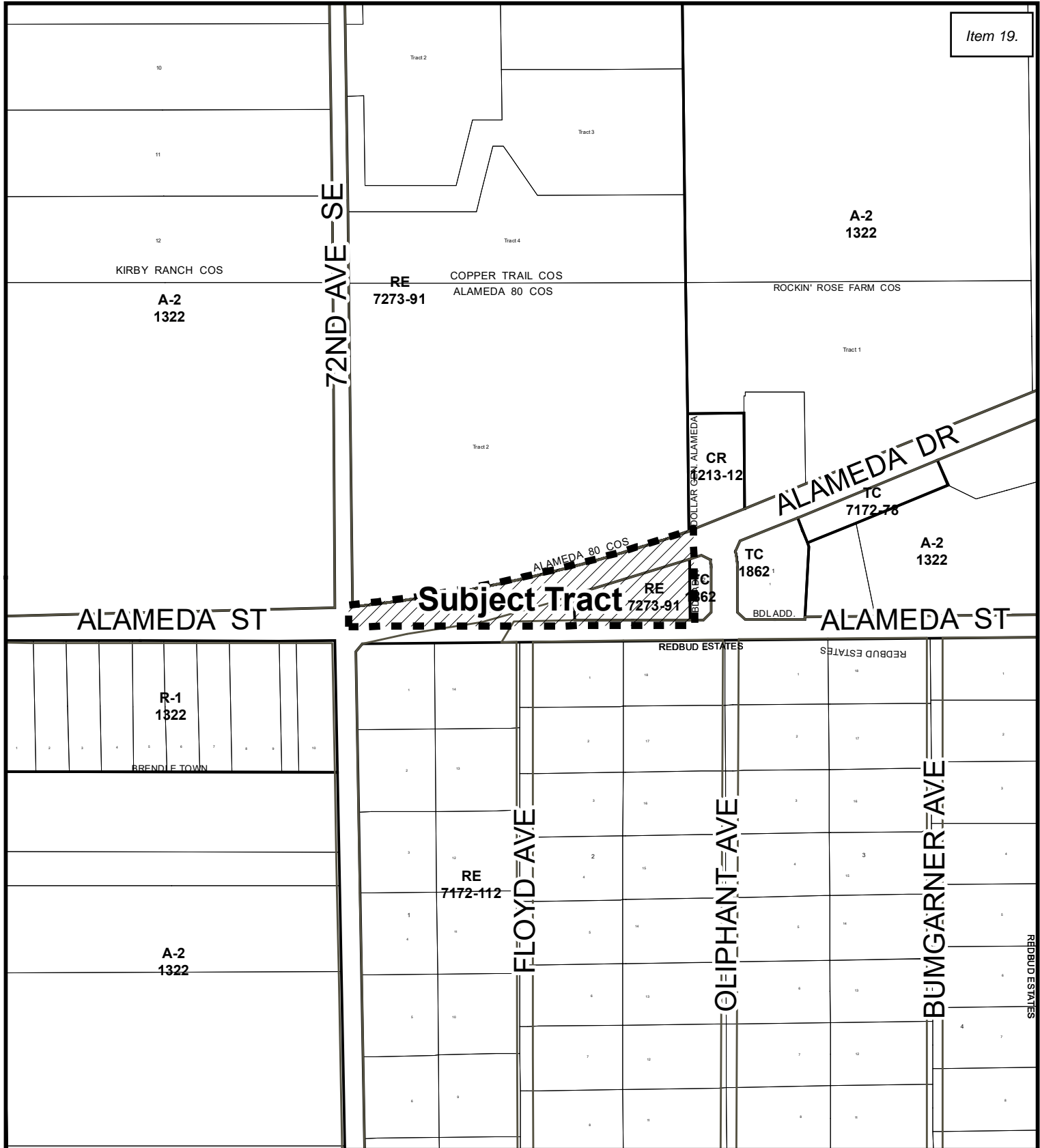
spacing in the City's Engineering Design Criteria. The developer has submitted a letter requesting a variance to the Director of Public Works. Staff can support this request for variance. Capacity exceeds demand in this area. As such, no off-site improvements are anticipated.

**PLATTING INFORMATION:**

1. Fire Protection. The Norman Fire Department will provide fire protection.
2. Sanitary Sewer. Any private system will require the Oklahoma Department of Environmental Quality approval.
3. Streets. Alameda Drive will include a paved shoulder toward the western end of the property. There is an existing paved shoulder serving the majority of the property. Alameda Street is existing.
4. Water. Private water system for this property will require City and Oklahoma Department of Environmental Quality approvals.
5. Trail. A 20' width trail easement will be required adjacent to Alameda Drive.

This property consists of 5.89 acres and one (1) lot. The proposal is a garden center serving the area.

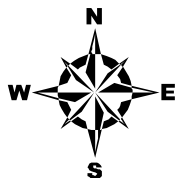
**RECOMMENDATION:** Based upon the above information, Staff recommends approval of the preliminary plat for Alameda Garden Center.



# Location Map



Map Produced by the City of Norman  
Geographic Information System.  
The City of Norman assumes no  
responsibility for errors or omissions  
in the information presented.



November 16, 2021

0 250 500 Ft.

Subject Tract

Zoning

Planning Commission Agenda  
December 9, 2021

PRELIMINARY PLAT  
PP-2122-6

ITEM NO. 7

### STAFF REPORT

ITEM: CONSIDERATION OF A PRELIMINARY PLAT FOR ALAMEDA GARDEN CENTER.

LOCATION: Generally located west of Oliphant Avenue between Alameda Drive and Alameda Street.

#### INFORMATION:

1. Owner. Saffron Fletcher/Sanctuary Gardens and Wellness, LLC.
2. Developer. Saffron Fletcher/Sanctuary Gardens and Wellness, LLC.
3. Engineer. NSE Engineering Consultants.

#### HISTORY:

1. October 18, 1961. City Council adopted Ordinance No. 1312 annexing this property into the Norman Corporate City limits without zoning.
2. October 30, 1961. Planning Commission recommended to City Council that this property be placed in A-2 zoning classification.
3. December 12, 1961. City Council adopted Ordinance No. 1332 placing this property in the A-2 zoning classification.
4. May 10, 1973. Planning Commission, on a vote 5-3, recommended to City Council that this property should not be placed in the RE, Residential Estates District and removed from A-2, Rural Agricultural District.
5. July 24, 1973. City Council adopted Ordinance No. O-7273-91 placing this property in RE, Residential Estates District and removing it from A-2, Rural Agricultural District.
6. December 9, 2021. The applicant has made a request to amend the NORMAN 2025 Land Use and Transportation Plan from Country Residential Designation to Commercial Designation.
7. December 9, 2021. The applicant has made a request to place this property in the CR, Rural Commercial District and remove it from RE, Residential Estates District.



IMPROVEMENT PROGRAM:

1. Permanent Markers. Permanent markers will be installed prior to filing of the final plat.
2. Sanitary Sewer. Any private system will require the Oklahoma Department of Environmental Quality approval.
3. Fire Protection. The City of Norman Fire Department located near this proposal will provide fire protection.
4. Streets. Alameda Drive will include a paved shoulder toward the western end of the property. There is an existing paved shoulder serving the majority of the property. Alameda Street is existing.
5. Trail. A 20-foot trail easement is proposed adjacent to Alameda Drive.

PUBLIC DEDICATIONS:

1. Easements. All required easements will be dedicated to the City on the final plat.
2. Rights-of-Way. All required street rights-of-way will be dedicated to the City on the final plat.

SUPPLEMENTAL MATERIAL: Copies of a location map, site plan and preliminary plat are included in the Agenda Book.

STAFF RECOMMENDATION: This property consist of 5.89 acres and one lot. The proposal is a garden center. Staff recommends approval of the preliminary plat for Alameda Garden Center.

ACTION NEEDED: Recommend approval or disapproval of the preliminary plat for Alameda Garden Center to City Council subject to approval of Resolution No. R-2122-64 Ordinance No. O-2122-22.

ACTION TAKEN: \_\_\_\_\_

**Applicant:** Saffron Fletcher, Sanctuary Gardens & Wellness, LLC

**Project Location:** West of Oliphant Avenue north of Alameda Street

**Case Number:** PD21-27

**Time:** 6:00 p.m.

**Applicant/Representative**

Saffron Fletcher  
Jason Fletcher  
Cedric LeBlanc  
Celeste LeBlanc  
Dylan West

**Attendees**

Tim Sherban  
Kim Sherban  
Brandi Rice  
Mark Cox

**City Staff**

Lora Hoggatt, Planning Services Manager  
Beth Muckala, Assistant City Attorney  
Ken Danner, Subdivision Development Manager

**Application Summary**

The applicant is requesting to preliminary plat and rezone from RE, Residential Estates District, to CR, Rural Commercial District.

**Neighbor's Comments/Concerns/Responses**

Neighbors asked about operating hours and traffic. The applicant explained they will have normal business hours with some special events in the evenings or on weekends. They plan to be open 6 days a week. March and April will be peak months but at the height of the growing season they only expect 10 people at once as the maximum. Traffic should be similar to other plant nursery traffic. There will be no medical marijuana on site. The applicant intends to keep as many trees on the property as possible to maintain a visual barrier around the perimeter. There will be a monarch butterfly weigh station and pollinator friendly plants. The applicant would like to use this business and property to give back to the community.

GBC 21-31

APPLICANT	Sanctuary Gardens
LOCATION	Alameda Drive and Oliphant Avenue
PROPOSAL	Preliminary Plat – Alameda Garden Center
NORMAN 2025 LAND USE	Current: Country Residential Proposed: Commercial
LAND USE	Current: Vacant Proposed: Plant nursery and monarch weigh station

Greenbelt Commission Final Comments - GBC 21-31

Greenbelt forwards this item with no additional comments.

### ALAMEDA GARDEN CENTER PRELIMINARY

The proposed 800 square foot garden center in this Preliminary Plat (PP) is expected to generate approximately 55 trips per day, 2 AM peak hour trips, and 6 PM peak hour trips. Obviously being below the threshold for when a traffic impact study is required (>100 peak hour trips is the threshold), the developer submitted a traffic impact memorandum documenting the trip generation information for this addition. The development is proposed for location on the south side of Alameda Drive approximately 300 feet west of Oliphant Avenue.

STREET	NO. OF LANES	BACK-GROUND TRAFFIC (Veh/day)	PROJECTED TRAFFIC (Veh/day)	TOTAL PROJECTED TRAFFIC (Veh/day)	ROADWAY CAPACITY L.O.S. "E"	% CAPACITY USED (BACKGROUND)	% CAPACITY USED (PROJECTED)
Alameda Drive	2	2,500	55	2,555	17,100	14.62	14.94

The proposed addition will access Alameda Drive to the north of the development and Alameda Street to the south. The access to Alameda Street does not meet the requirements for driveway spacing in the City's Engineering Design Criteria. The developer has submitted a letter requesting a variance to the Director of Public Works. Staff can support this request for variance. Capacity exceeds demand in this area. As such, no off-site improvements are anticipated.



# CITY OF NORMAN

## Development Review Form

### Transportation Impacts

**DATE:** December 1, 2021

**CONDUCTED BY:** Jami L. Short, P.E.  
City Traffic Engineer

**PROJECT NAME:** Alameda Garden Center PP

**PROJECT TYPE:** Commercial

Owner: Saffron Fletcher Sanctuary Gardens & Wellness  
Developer's Engineer: Tahir Nasir, NSE Engineering Consultants  
Developer's Traffic Engineer: Tahir Nasir, NSE Engineering Consultants

#### **SURROUNDING ENVIRONMENT (Streets, Developments)**

The areas surrounding this site are generally commercial to the east, parkland to the north with low density residential to the south and west. Alameda Drive connects to 72nd Avenue SE to the west and Oliphant Avenue to the east. Alameda Street is to the south of the site and connects to Oliphant Avenue to the east and Floyd Avenue to the west and ends in a cul-a-sac further west.

#### **ALLOWABLE ACCESS:**

The access will be in accordance with Section 4018 of the City's Engineering Design Criteria.

#### **EXISTING STREET CHARACTERISTICS (Lanes, Speed Limits, Sight Distance, Medians)**

Alameda Drive: 2 lanes (existing and future). Speed Limit—50 mph. No sight distance problems. No median.

Alameda Street: 2 lanes (existing and future). Speed Limit—25 mph. No sight distance problems. No median.

**ACCESS MANAGEMENT CODE COMPLIANCE:** YES ☒ NO ☐

The access to Alameda Street does not meet the requirements for driveway spacing in the City's Engineering Design Criteria. The developer has submitted a letter requesting a variance to the Director of Public Works. Staff supports this request for variance.

#### **TRIP GENERATION**

	<b>Total</b>	<b>In</b>	<b>Out</b>
Weekday	55	27	28
A.M. Peak Hour	2	1	1
P.M. Peak Hour	6	3	3

**TRANSPORTATION IMPACT STUDY REQUIRED?** YES ☐ NO ☒

Obviously being below the threshold for when a traffic impact study is required (>100 peak hour trips is the threshold), the developer submitted a traffic impact memorandum documenting the trip generation information for this development. The development is proposed for location on the south side of Alameda Drive approximately 300 feet west of Oliphant Avenue.

**RECOMMENDATION:** APPROVAL ☒ DENIAL ☐ N/A ☐ **STIPULATIONS** ☐

*Recommendations for Approval refer only to the transportation impact and do not constitute an endorsement from City Staff.*

The proposed addition will access Alameda Drive to the north of the development and Alameda Street to the south. Capacity exceeds demand in this area. As such, no off-site improvements are anticipated.









**NSE ENGINEERING CONSULTANTS**  
(Civil & Structural Engineers)

Item 19.

**TAHIR M. NASIR, P.E.**

November 29, 2021

Shawn O'Leary  
Director Public Works  
City of Norman  
201A West Gray Street  
Norman, OK 73069

**Re: Request for Variance/Waiver  
Alameda Garden Center,  
7450 Alameda Drive,  
Norman, OK 73026.**

Dear Mr. O'Leary:

I am writing this letter on behalf of our client to request a variance/waiver for the proposed project driveway on Alameda Drive. The Alameda Drive is a rural two-lane highway with a posted speed of 50 mph. The City of Norman Design Criteria (CNDC) Section 4018.F requires that driveways on arterial roads with posted speeds greater than 40 mph and trips less than 100 in peak hour shall have a minimum separation of 330 feet from the intersection. The proposed project driveway is 316 feet from the Alameda Drive/Oliphant Ave intersection and will generate four (4) trips in peak hours. This distance is shorter than the minimum recommended by the CNDC. The proposed driveway is necessary for business aspects of the project and vehicular circulation through the site. We are requesting a variance/waiver from this requirement to construct this driveway.

Your support in this matter is greatly appreciated. If you need additional information, please contact me at the address below.

Very truly yours,

NSE Engineering Consultants, PLLC



Tahir Nasir, P.E.  
Principle

**NORMAN PLANNING COMMISSION  
REGULAR SESSION MINUTES**

**DECEMBER 9, 2021**

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Notice and agenda of the meeting was posted at the Norman Municipal Building and online at <https://norman-ok.municodemeetings.com> at least twenty-four hours prior to the beginning of the meeting.

Chair Erica Bird called the meeting to order at 6:30 p.m.

\* \* \*

**ROLL CALL**

**MEMBERS PRESENT**

Erin Williford (arrived at 6:32 p.m.)  
Kevan Parker  
Steven McDaniel  
Erica Bird  
Dave Boeck  
Sandy Bahan  
Michael Jablonski

**MEMBERS ABSENT**

Lark Zink

A quorum was present.

**STAFF MEMBERS PRESENT**

Jane Hudson, Director, Planning &  
Community Development  
Lora Hoggatt, Planning Services Manager  
Logan Hubble, Planner I  
Anais Starr, Planner II  
Colton Wayman, Planner I  
Roné Tromble, Recording Secretary  
Ken Danner, Subdivision Development  
Manager  
Kathryn Walker, City Attorney  
Heather Poole, Asst. City Attorney  
Jeanne Snider, Asst. City Attorney  
Jami Short, Traffic Engineer  
Bryce Holland, Multimedia Specialist

\* \* \*

Item No. 5, being:

**CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF RESOLUTION NO. R-2122-64: SANCTUARY GARDENS AND WELLNESS, L.L.C. REQUESTS AMENDMENT OF THE NORMAN 2025 LAND USE AND TRANSPORTATION PLAN FROM COUNTRY RESIDENTIAL DESIGNATION TO COMMERCIAL DESIGNATION FOR APPROXIMATELY 5.99 ACRES OF PROPERTY LOCATED WEST OF OLIPHANT AVENUE BETWEEN ALAMEDA DRIVE AND ALAMEDA STREET.**

**ITEMS SUBMITTED FOR THE RECORD:**

1. NORMAN 2025 Map
2. Staff Report
3. Pre-Development Summary

Item No. 6, being:

**CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF ORDINANCE NO. O-2122-22: SANCTUARY GARDENS AND WELLNESS, L.L.C. REQUESTS REZONING FROM RE, RESIDENTIAL ESTATES DWELLING DISTRICT, TO CR, RURAL COMMERCIAL DISTRICT, FOR APPROXIMATELY 5.99 ACRES OF PROPERTY LOCATED WEST OF OLIPHANT AVENUE BETWEEN ALAMEDA DRIVE AND ALAMEDA STREET.**

**ITEMS SUBMITTED FOR THE RECORD:**

1. Location Map
2. Staff Report
3. Site Plan
4. Pre-Development Summary

Item No. 7, being:

**CONSIDERATION OF APPROVAL, ACCEPTANCE, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF PP-2122-6, A PRELIMINARY PLAT SUBMITTED BY SAFFRON FLETCHER/SANCTUARY GARDENS AND WELLNESS, L.L.C. (NSE ENGINEERING CONSULTANTS) FOR ALAMEDA GARDEN CENTER FOR APPROXIMATELY 5.99 ACRES OF PROPERTY LOCATED WEST OF OLIPHANT AVENUE BETWEEN ALAMEDA DRIVE AND ALAMEDA STREET.**

**ITEMS SUBMITTED FOR THE RECORD:**

1. Location Map
2. Preliminary Plat
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7. Greenbelt Commission Action

**PRESENTATION BY STAFF:**

1. Ms. Lora Hoggatt presented the staff report. One protest letter was received which represented 7.6% of the notification area.

**PRESENTATION BY THE APPLICANT:**

The applicant's representative was in attendance, but did not make a presentation.

Commissioner Parker asked a question.

Ms. Saffron Fletcher, the applicant, responded to the question.

Commissioner McDaniel asked a follow-up question.

Ms. Jami Short, Traffic Engineer, responded.

**AUDIENCE PARTICIPATION:**

None

**DISCUSSION AND ACTION BY THE PLANNING COMMISSION:**

Commissioner Boeck made comments.  
Commissioner Parker made comments.  
Commissioner Jablonski made comments.  
Commissioner Williford made comments.  
Ms. Jami Short, Traffic Engineer, responded.

*Michael Jablonski moved to recommend adoption of Resolution No. R-2122-64, Ordinance No. O-2122-22, and PP-2122-6, the Preliminary Plat for ALAMEDA GARDEN CENTER, to City Council. Steven McDaniel seconded the motion.*

There being no further discussion, a vote on the motion was taken with the following result:

YEAS	Erin Williford, Steven McDaniel, Erica Bird, Dave Boeck, Sandy Bahan, Michael Jablonski
NAYES	Kevan Parker
MEMBERS ABSENT	Lark Zink

The motion, to recommend adoption of Resolution No. R-2122-64, Ordinance No. O-2122-22, and PP-2122-6 to City Council, passed by a vote of 6-1.

\* \* \*

**File Attachments for Item:**

20. CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF ORDINANCE O-2122-27 UPON SECOND AND FINAL READING: AN ORDINANCE OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, AMENDING SECTION 460 OF CHAPTER 22 OF THE CODE OF THE CITY OF NORMAN SO AS TO GRANT SPECIAL USE FOR MEDICAL MARIJUANA PROCESSING (TIER III) IN THE C-2, GENERAL COMMERCIAL DISTRICT FOR LOT 3C IN BLOCK TWO (2) OF EAST LINDSEY PLAZA SECTION 2, TO NORMAN, CLEVELAND COUNTY, OKLAHOMA; AND PROVIDING FOR THE SEVERABILITY THEREOF. (1228 LINDSEY PLAZA DRIVE)



## CITY OF NORMAN, OK STAFF REPORT

**MEETING DATE:** 01/25/2022

**REQUESTER:** Binh Vu To and Hong Loan Thi Danh

**PRESENTER:** Jane Hudson, Director, Planning & Community Development

**ITEM TITLE:** CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF ORDINANCE O-2122-27 UPON SECOND AND FINAL READING: AN ORDINANCE OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, AMENDING SECTION 460 OF CHAPTER 22 OF THE CODE OF THE CITY OF NORMAN SO AS TO GRANT SPECIAL USE FOR MEDICAL MARIJUANA PROCESSING (TIER III) IN THE C-2, GENERAL COMMERCIAL DISTRICT FOR LOT 3C IN BLOCK TWO (2) OF EAST LINDSEY PLAZA SECTION 2, TO NORMAN, CLEVELAND COUNTY, OKLAHOMA; AND PROVIDING FOR THE SEVERABILITY THEREOF. (1228 LINDSEY PLAZA DRIVE)

**SYNOPSIS:** The applicants, Binh Vu To and Hong Loan Thi Danh, are requesting Special Use for a Tier III Medical Marijuana Processor at 1228 Lindsey Plaza Drive. The property is zoned C-2, General Commercial District.

**HISTORY:** The property was rezoned from A-2, Rural Agricultural District, to C-2, General Commercial District, in April 1972. The subject and surrounding properties were intended at the time of rezoning to be used for the construction of a small shopping center.

**ZONING ORDINANCE CITATION:** A Special Use request shall be reviewed and evaluated on the following criteria according to the Zoning Ordinance 22:434.1, Special Uses:

1. Conformance with applicable regulations and standards established by the Zoning Regulations.
2. Compatibility with existing or permitted uses on abutting sites, in terms of building height, bulk and scale, setbacks and open spaces, landscaping and site development, and access and circulation features.
3. Potentially unfavorable effects or impacts on other existing or permitted uses on abutting sites, to the extent such impacts exceed those which reasonably may result from use of the site by a permitted use. (NOTE: Throughout this Section, "Permitted Use" means any use authorized as a matter of right under the applicable zoning district.)

4. Modifications to the site plan which would result in increased compatibility, or would mitigate potentially unfavorable impacts, or would be necessary to conform to applicable regulations and standards and to protect the public health, safety, morals, and general welfare.
5. Safety and convenience of vehicular and pedestrian circulation in the vicinity, including traffic reasonably expected to be generated by the proposed "Special Use" and other uses authorized and anticipated in the area, considering existing zoning and land uses in the area.
6. That any conditions applicable to approval are the minimum necessary to minimize potentially unfavorable impacts on nearby uses and to ensure compatibility of the proposed "Special Use" with existing or permitted uses in the surrounding area.

**EXISTING ZONING:** The property is currently zoned C-2, General Commercial District. This district is intended for the conduct of personal and business services and the general retail business of the community. The C-2 Zoning District requires Special Use approval for Tier III Medical Marijuana Processing.

### **ANALYSIS:**

**SITE PLAN:** This property has two access points; one off E Lindsey Plaza Drive and one off an alley. The applicant will not be changing the existing exterior building or site. There is existing adequate on-site parking.

The applicant will be performing light processing, including the creation of pre-rolls, cannabis cigars, cartridges, and distillates, which are in Tier III of Medical Marijuana Processing as defined by the State of Oklahoma. A Tier III Medical Marijuana Processor is "a facility defined and regulated by Oklahoma state law as a Medical Marijuana Processor, and which engages in any type(s) of Medical Marijuana Processing, including all allowed extraction processes, except that on-site sales are not permitted."

**IMPACTS:** The applicant will not grow medical marijuana or operate a dispensary from this site. The applicant is required to follow all City of Norman codes, and acquire all City of Norman permits, licenses and obtain an Annual Medical Marijuana Processor License to continue the Special Use permit.

### **OTHER AGENCY COMMENTS:**

**PUBLIC WORKS:** This site is platted and all public utilities are installed.

### **PREDEVELOPMENT: PD21-37, November 17, 2021**

Neighbors are concerned that families live nearby. Applicant described rigorous process and explained that children could not get access to marijuana. Neighbors were also concerned about addiction and crime. Applicant said that medical marijuana is not an addiction. Neighbor asked how much processing is in Norman. Planning staff responded that they did not know. Neighbor



encouraged applicant to move to an industrial area. Applicant stated that they have a two-year lease.

**CONCLUSION:** Staff forwards this request for Special Use and Ordinance O-2122-27 for City Council's consideration.

Planning Commission, at their December 9, 2021 meeting, recommended adoption of Ordinance O-2122-27 by a vote of 4-3.

O-2122-27

AN ORDINANCE OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, AMENDING SECTION 460 OF CHAPTER 22 OF THE CODE OF THE CITY OF NORMAN SO AS TO GRANT SPECIAL USE FOR MEDICAL MARIJUANA PROCESSING (TIER III) IN THE C-2, GENERAL COMMERCIAL DISTRICT FOR LOT 3C IN BLOCK TWO (2) OF EAST LINDSEY PLAZA SECTION 2, TO NORMAN, CLEVELAND COUNTY, OKLAHOMA; AND PROVIDING FOR THE SEVERABILITY THEREOF. (1228 Lindsey Plaza Drive)

- § 1. WHEREAS, Binh Vu To and Hong Loan Thi Danh have made application to have Special Use for Medical Marijuana Processing (Tier III) on the property described below in the C-2, General Commercial District; and
- § 2. WHEREAS, said application has been referred to the Planning Commission of said City and said body has, after conducting a public hearing as required by law, considered the same and recommended that the same should be granted and an ordinance adopted to effect and accomplish such rezoning; and
- § 3. WHEREAS, the City Council of the City of Norman, Oklahoma, has thereafter considered said application and has determined that said application should be granted and an ordinance adopted to effect and accomplish such rezoning.

NOW, THEREFORE, BE IT ORDAINED BY THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA:

- § 4. That Section 460 of Chapter 22 of the Code of the City of Norman, Oklahoma, is hereby amended so as to grant Special Use for Medical Marijuana Processing (Tier III) in the C-2, General Commercial District, for the following described property, to wit:

Lot 3C, in Block 2 of EAST LINDSEY PLAZA SECTION 2, to Norman, Cleveland County, Oklahoma.

Ordinance No. O-2122-27

Page 2

§ 5. Further, pursuant to the provisions of Section 22:434.1 of the Code of the City of Norman, as amended, the following conditions are hereby attached to the zoning of the tract:

- a. The site shall be developed in accordance with the Site Plan (aerial photo), and supporting documentation submitted by the applicant and approved by the Planning Commission on December 9, 2021.

§ 6. Severability. If any section, subsection, sentence, clause, phrase, or portion of this ordinance is, for any reason, held invalid or unconstitutional by any court of competent jurisdiction, such portion shall be deemed a separate, distinct, and independent provision, and such holding shall not affect the validity of the remaining portions of this ordinance.

ADOPTED this \_\_\_\_\_ day of

NOT ADOPTED this \_\_\_\_\_ day of

\_\_\_\_\_, 2022.

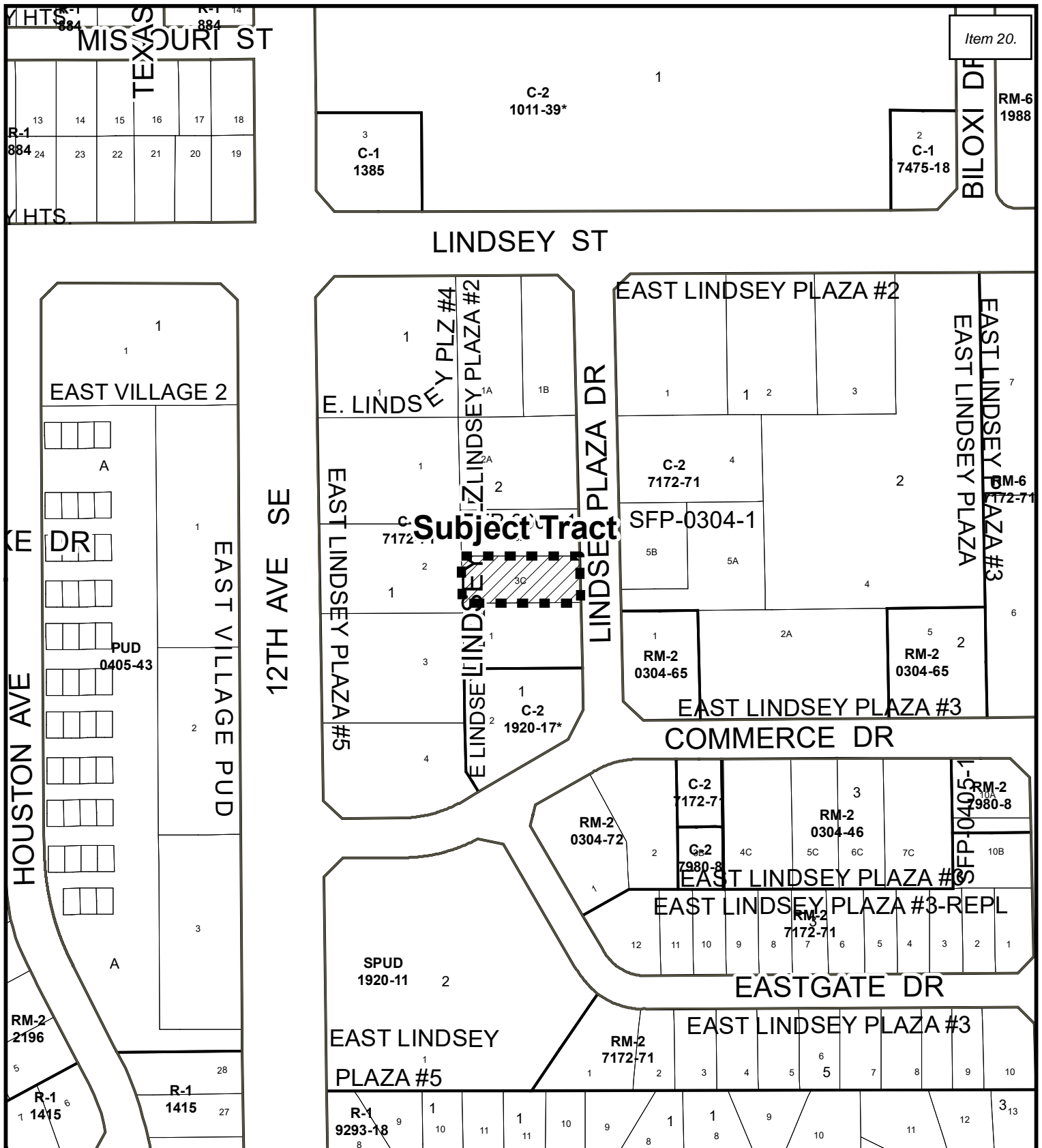
\_\_\_\_\_, 2022.

\_\_\_\_\_  
(Mayor)

\_\_\_\_\_  
(Mayor)

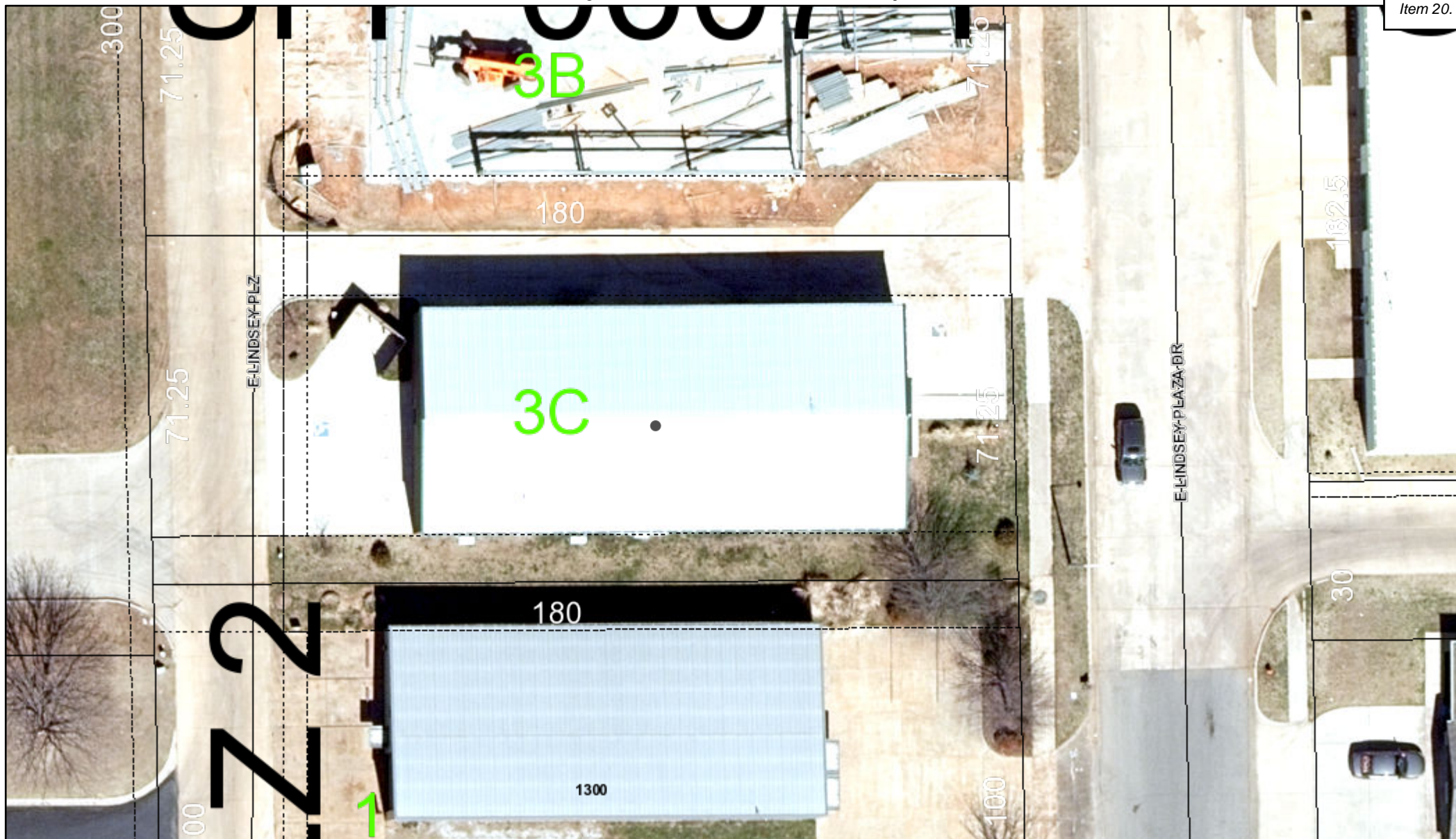
ATTEST:

\_\_\_\_\_  
(City Clerk)



# City of Norman WebMap

Item 20.



12/1/2021, 1:34:57 PM



Core Area



Easement



Conservation Easements



Lot Line



Parcel

Address Number



OU



Park



Lake Thunderbird



Railroad



Streets

Centerline Labels (10,000+)

AerialPhoto2019



Red: Band\_1

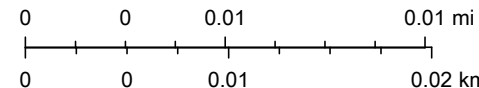


Green: Band\_2



Blue: Band\_3

1:335



The Sanborn Map Company LLC, City of Norman, GIS Services Division

517

City of Norman, Interactive Map

The City of Norman assumes no responsibility of errors or omissions in the information presented.

**Applicant:** Greg Iman, Hong Loan Thi Danh

**Project Location:** 1228 Lindsey Plaza Drive

**Case Number:** PD21-37

**Time:** 5:30 p.m.

**Applicant/Representative**

Hong Loan Danh  
Binh Vu To

**Attendees**

Kenyan Hill  
Gina Hill

**City Staff**

Logan Hubble, Planner I  
Beth Muckala, Assistant City Attorney

**Application Summary**

The applicant is requesting Special Use for medical marijuana Tier III processing.

**Neighbor's Comments/Concerns/Responses**

Neighbors are concerned that families live nearby. Applicant described rigorous process and explained that children could not get access to marijuana. Neighbors were also concerned about addiction and crime. Applicant said that medical marijuana is not an addiction. Neighbor asked how much processing is in Norman. Planning staff responded that they did not know. Neighbor encouraged applicant to move to an industrial area. Applicant stated that they have a two year lease.

ORDINANCE NO. O-2122-27

ITEM NO. 8

**STAFF REPORT****GENERAL INFORMATION**

APPLICANT	Binh Vu To and Hong Loan Thi Danh
REQUESTED ACTION	Special Use for a Tier III Medical Marijuana Processor
EXISTING ZONING	C-2, General Commercial District
SURROUNDING ZONING	North: C-2, General Commercial District East: C-2, General Commercial District and RM-2, Low Density Apartment District South: C-2, General Commercial District and RM-2, Low Density Apartment District West: C-2, General Commercial District and PUD, Planned Unit Development
LOCATION	1228 Lindsey Plaza Drive
SIZE	0.3 acres, more or less
PURPOSE	Medical Marijuana Processing
EXISTING LAND USE	Warehouse
SURROUNDING LAND USE	North: Commercial East: Commercial South: Commercial West: Commercial
LAND USE PLAN DESIGNATION	Commercial Designation

**SYNOPSIS:** The applicants, Binh Vu To and Hong Loan Thi Danh, are requesting Special Use for a Tier III Medical Marijuana Processor at 1228 Lindsey Plaza Drive. The property is zoned C-2, General Commercial District.



**HISTORY:** The property was rezoned from A-2, Rural Agricultural District, to C-2, General Commercial District, in April 1972. The subject and surrounding properties were intended at the time of rezoning to be used for the construction of a small shopping center.

**ZONING ORDINANCE CITATION:** A Special Use request shall be reviewed and evaluated on the following criteria according to the Zoning Ordinance 22:434.1, Special Uses:

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3. Potentially unfavorable effects or impacts on other existing or permitted uses on abutting sites, to the extent such impacts exceed those which reasonably may result from use of the site by a permitted use. (NOTE: Throughout this Section, "Permitted Use" means any use authorized as a matter of right under the applicable zoning district.)
4. Modifications to the site plan which would result in increased compatibility, or would mitigate potentially unfavorable impacts, or would be necessary to conform to applicable regulations and standards and to protect the public health, safety, morals, and general welfare.
5. Safety and convenience of vehicular and pedestrian circulation in the vicinity, including traffic reasonably expected to be generated by the proposed "Special Use" and other uses authorized and anticipated in the area, considering existing zoning and land uses in the area.
6. That any conditions applicable to approval are the minimum necessary to minimize potentially unfavorable impacts on nearby uses and to ensure compatibility of the proposed "Special Use" with existing or permitted uses in the surrounding area.

**EXISTING ZONING:** The property is currently zoned C-2, General Commercial District. This district is intended for the conduct of personal and business services and the general retail business of the community. The C-2 Zoning District requires Special Use approval for Tier III Medical Marijuana Processing.

**ANALYSIS:**

**SITE PLAN:** This property has two access points; one off E Lindsey Plaza Drive and one off an alley. The applicant will not be changing the existing exterior building or site. There is existing adequate on-site parking.

The applicant will be performing light processing, including the creation of pre-rolls, cannabis cigars, cartridges, and distillates, which are in Tier III of Medical Marijuana Processing as defined by the State of Oklahoma. A Tier III Medical Marijuana Processor is "a facility defined and regulated by Oklahoma state law as a Medical Marijuana Processor, and which engages in any type(s) of Medical Marijuana Processing, including all allowed extraction processes, except that on-site sales are not permitted."

**IMPACTS:** The applicant will not grow medical marijuana or operate a dispensary from this site. The applicant is required to follow all City of Norman codes, and acquire all City of Norman permits, licenses and obtain an Annual Medical Marijuana Processor License to continue the Special Use permit.

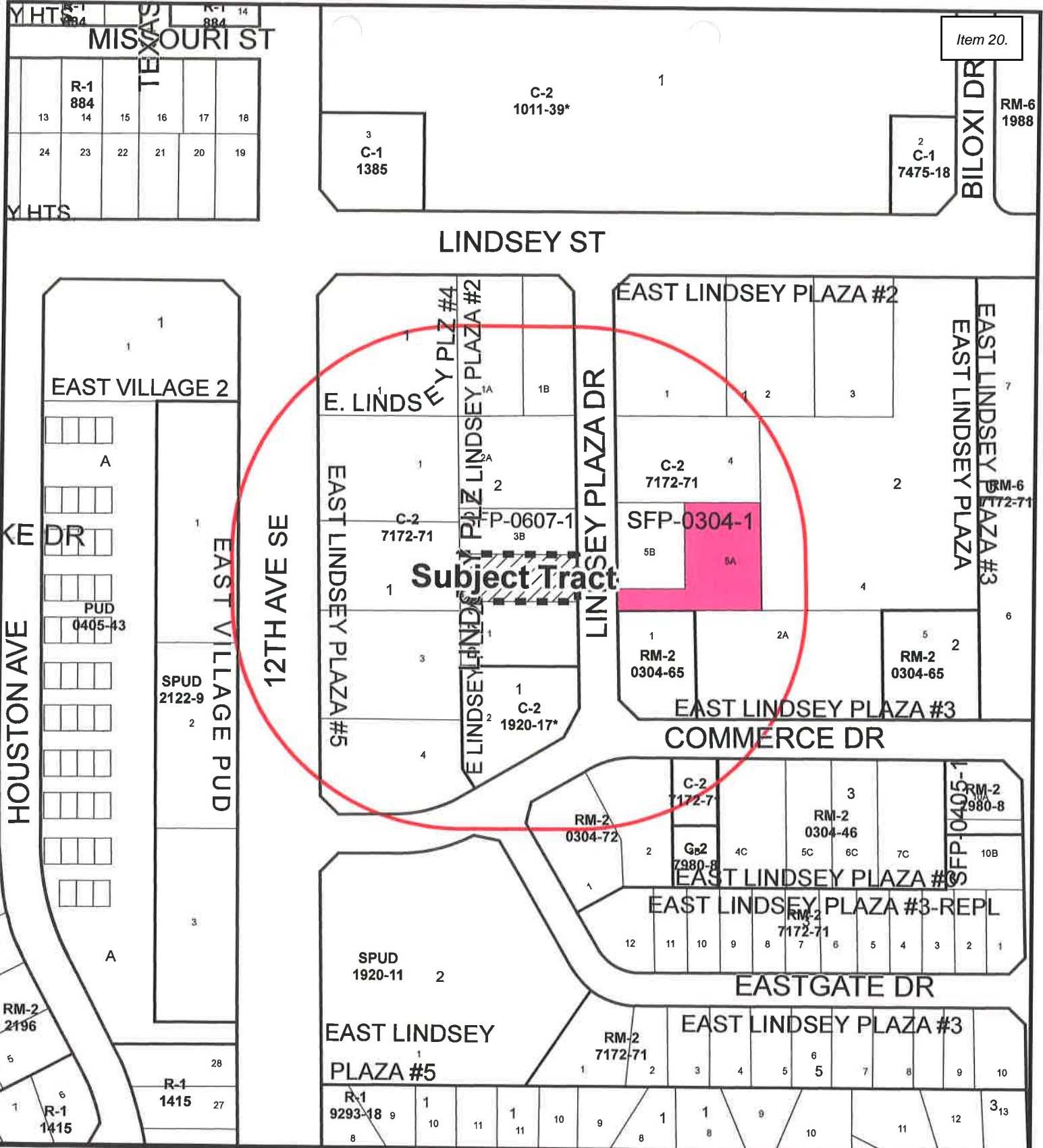
**OTHER AGENCY COMMENTS:**

**PUBLIC WORKS:** This site is platted and all public utilities are installed.

**PREDEVELOPMENT: PD21-37, November 17, 2021**

Neighbors are concerned that families live nearby. Applicant described rigorous process and explained that children could not get access to marijuana. Neighbors were also concerned about addiction and crime. Applicant said that medical marijuana is not an addiction. Neighbor asked how much processing is in Norman. Planning staff responded that they did not know. Neighbor encouraged applicant to move to an industrial area. Applicant stated that they have a two year lease.

**CONCLUSION:** Staff forwards this request for Special Use and Ordinance No. O-2122-27 for Planning Commission's consideration.



# Protest Map

5.3% Protest Within Notification Area



Map Produced by the City of Norman  
Geographic Information System.  
The City of Norman assumes no  
responsibility for errors or omissions  
in the information presented.



0 100 200 Feet



Subject Tract



Notification Area



Protest

December 7, 2021

The City of Norman  
201 W. Gray A  
Norman, OK 73070

RE: Notice of Request for Special Use

December 6, 2021

Dear Planning Commission and City Council;

The purpose of this letter is to protest the request for Special Use for Tier III Medical Marijuana Processing at 1228 Lindsey Plaza Dr., Norman, OK.

This processing would create additional traffic from deliveries of product, large amounts of trash from manufacturing, fire danger from highly flammable chemicals used in extraction process, increased risk of crime for addictive products, foul odor and sanitation of building can create issues in affected areas.

This will cause increased danger to children living in nearby homes. School buses travel to this area as well.

This will have a negative affect on all property in this area.

Norman does not need another marijuana business!

Sincerely,



Gina Waller  
Owner  
Emcee Investments, LLC

Authorized to sign on behalf of Emcee Investments

**NORMAN PLANNING COMMISSION  
REGULAR SESSION MINUTES**

**DECEMBER 9, 2021**

The Planning Commission of the City of Norman, Cleveland County, State of Oklahoma, met in Regular Session in the Council Chambers of the Norman Municipal Building, 201 West Gray Street, on the 9<sup>th</sup> day of December, 2021.

Notice and agenda of the meeting was posted at the Norman Municipal Building and online at <https://norman-ok.municodemeetings.com> at least twenty-four hours prior to the beginning of the meeting.

Chair Erica Bird called the meeting to order at 6:30 p.m.

\* \* \*

**ROLL CALL**

**MEMBERS PRESENT**

Erin Williford (arrived at 6:32 p.m.)  
Kevan Parker  
Steven McDaniel  
Erica Bird  
Dave Boeck  
Sandy Bahan  
Michael Jablonski

**MEMBERS ABSENT**

Lark Zink

A quorum was present.

**STAFF MEMBERS PRESENT**

Jane Hudson, Director, Planning &  
Community Development  
Lora Hoggatt, Planning Services Manager  
Logan Hubble, Planner I  
Anais Starr, Planner II  
Colton Wayman, Planner I  
Roné Tromble, Recording Secretary  
Ken Danner, Subdivision Development  
Manager  
Kathryn Walker, City Attorney  
Heather Poole, Asst. City Attorney  
Jeanne Snider, Asst. City Attorney  
Jami Short, Traffic Engineer  
Bryce Holland, Multimedia Specialist

\* \* \*

Item No. 8, being:

**CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF ORDINANCE NO. O-2122-27 FOR BINH VU TO AND HONG LOAN THI DANH REQUEST SPECIAL USE FOR MEDICAL MARIJUANA PROCESSING (TIER III) FOR PROPERTY CURRENTLY ZONED C-2, GENERAL COMMERCIAL DISTRICT, AND LOCATED AT 1228 LINDSEY PLAZA DRIVE.**

**ITEMS SUBMITTED FOR THE RECORD:**

1. Location Map
2. Staff Report
3. Aerial Photo
4. Pre-Development Summary

**PRESENTATION BY STAFF:**

1. Mr. Logan Hubble presented the staff report. One protest letter was received which represented 5.3% of the notification area.

**PRESENTATION BY THE APPLICANT:**

The applicant's representative was in attendance, but did not make a presentation.

Commissioner Jablonski asked a question

Ms. Hong Loan Thi Danh, the applicant, 12404 Rockwood Avenue, Oklahoma City, responded to questions.

**AUDIENCE PARTICIPATION:**

None

**DISCUSSION AND ACTION BY THE PLANNING COMMISSION:**

*Dave Boeck moved to recommend adoption of Ordinance No. O-2122-27 to City Council. Erin Williford seconded the motion.*

There being no further discussion, a vote on the motion was taken with the following result:

YEAS

NAYES

MEMBERS ABSENT

Erin Williford, Erica Bird, Dave Boeck, Michael Jablonski

Kevan Parker, Steven McDaniel, Sandy Bahan

Lark Zink

The motion, to recommend adoption of Ordinance No. O-2122-27 to City Council, passed by a vote of 4-3.

\*\*\*

**File Attachments for Item:**

21. CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF RESOLUTION R-2122-58: A RESOLUTION OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, ADOPTING THE HISTORIC PRESERVATION GUIDELINES TO BE USED BY THE NORMAN HISTORIC DISTRICT COMMISSION IN REVIEWING PROPOSED ADDITIONS, ALTERATIONS, AND DEMOLITIONS TO STRUCTURES LOCATED IN THE HISTORIC DISTRICTS.





## CITY OF NORMAN, OK STAFF REPORT

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**MEETING DATE:** 01/25/2022

**REQUESTER:** Anais Starr, Historic Preservation Officer

**PRESENTER:** Jane Hudson, Director of Planning & Community Development

**ITEM TITLE:** CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF RESOLUTION R-2122-58: A RESOLUTION OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, ADOPTING THE HISTORIC PRESERVATION GUIDELINES TO BE USED BY THE NORMAN HISTORIC DISTRICT COMMISSION IN REVIEWING PROPOSED ADDITIONS, ALTERATIONS, AND DEMOLITIONS TO STRUCTURES LOCATED IN THE HISTORIC DISTRICTS.

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**BACKGROUND:**

After more than 10 years of use, the Historic District Commission found both the 2008 Preservation Guidelines and 2009 Historic Preservation Handbook were in need of revisions. The following areas were identified as needing to be addressed: corrections of errors and omissions; development of new layout for the handbook resulting in a user-friendly document for citizens, Commission and staff; incorporation of the Southridge District; expansion of proposed work approvable by Administrative Bypass review and the creation of standards for those Bypass items; clarification of routine maintenance and repair items; creation of guidelines addressing energy efficiency requests (windows and solar); and revisions to the guidelines for parking, windows, doors and additions.

The City Council on August 12, 2008 adopted by resolution a set of Historic Preservation Guidelines. The newly adopted Preservation Guidelines were then incorporated by staff into a Historic Preservation Handbook in March of 2009. The Handbook was developed to assist Historic District property owners with best practices in maintaining and renovating their historic properties along with providing the required Historic Preservation Guidelines that must be followed. Copies of the Handbook were distributed to every property owner in the Chautauqua and Miller Historic Districts. Since that time, staff has been tracking potential revisions needed to the 2008 Historic Preservation Guidelines and the 2009 Historic Preservation Handbook.

In July of 2018, the Historic District Commission initiated the update of the 2008 Historic Preservation Guidelines and 2009 Preservation Handbook by forming the Historic Preservation Guideline Update Subcommittee. A copy of identified revisions tracked for both documents over the years by staff was used as a beginning point for discussions. The Subcommittee = continued reviewing the Handbook until December of 2018.

The Subcommittee's review process resulted in a list of corrections to be used as a guide to a future revision process by a consultant. Items identified by the Subcommittee included: the clarification of design guidelines for garages, particularly the size, height and materials; the addition of a door replacement process; window replacement clarification; coordination of demolition guidelines with criteria listed in the Historic District Ordinance; the need for more information on alternative materials and solar panels; as well as minor corrections such as typos. See Exhibit A.

On July 9, 2019, the Council accepted Certified Local Government grant funds through a contract with the State Historic Preservation Office. As part of that grant acceptance, the City appropriated matching funds for the professional services of a consultant to update the City of Norman Historic Preservation Guidelines and Handbook. A Request for Proposals for professional services was issued September 24, 2019.

A contract between the City of Norman and Mainstreet Architects, Inc., out of San Antonio, TX, was signed on November 19, 2019 for the update of the Historic Preservation Guidelines and Handbook. A kick-off meeting was held on December 9, 2019 with the consultant to provide an opportunity for the Historic District Commission to discuss desired revisions. The Commission highlighted particular issues of concern, including: materials, garages, parking, additions, windows, energy efficiency aspects, the lack of historical information regarding Southridge Historic District, graphics of appropriate architectural elements and concepts, were all identified as elements that needed to be revised.

The consultant used both the Subcommittee's list of identified corrections and the Commission input as a starting point for revisions to the Historic Preservation Guidelines and Handbook. The consultant met again with the Historic District Commission on January 27, 2020 to review an outline of proposed changes. In February of that year the Coronavirus Pandemic struck and the revision process slowed. The contract with the consultant was extended to provide additional time to complete the revisions.

The consultant presented a proposed Draft Historic Preservation Guideline and Handbook at the October 5, 2020 Historic District Commission Workshop. The Draft Guidelines and Handbook included revisions to the following sections: materials, additions, garages, energy efficiency, and background information on Southridge. The consultant also added graphic illustrations of appropriate architectural elements for the various house styles. The Commission gave feedback at this meeting and the consultant incorporated those revisions, delivering a final copy of the Historic Preservation Guidelines and Handbook in November of 2020. See Exhibit B.

In January of 2021, staff prepared the Draft Historic Preservation Guidelines for the Historic District Commission's consideration and recommendation for adoption to the City Council. The Historic Preservation Handbook document is not part of this adoption process, as the contents are not regulatory. Once the Draft Historic Preservation Guidelines are adopted they will be incorporated into a revised Historic Preservation Handbook at a later date.

The Draft Historic Preservation Guidelines prepared by staff, were discussed by the Historic District Commission at three public Historic District Commission Workshops held on March 2, April 5, and May 3, 2021. Further refinements were made at these Workshops regarding the

garage, addition, accessory structures, and windows sections by the Commission with staff's assistance, resulting in the 2021 Draft Historic Preservation Guidelines.

The 2021 Draft Historic Preservation Guidelines were posted to the City of Norman website in late June. At the same time, postcards were mailed to every property owner in the three designated Historic Districts inviting them to make comments at two Historic Preservation Guidelines Public Input Meetings to be held in July.

The Historic Preservation Guidelines Public Input Meetings were held on July 19 and July 26, 2021 in the City Council Chambers. Staff presented a summary of revisions to the Historic Preservation Guidelines at both meetings and citizens provided feedback and comments. Twelve citizens attended the July 19 meeting with three of them providing comments. At the July 26, meeting, there were six citizens in attendance with two of them providing comments. In addition, staff received emailed comments or in-person comments from three other citizens. A summary of the comments are attached as Exhibit C.

At the August 2, 2021 Historic District Commission Meeting, public comments were reviewed and/or incorporated into the Guidelines. A recommendation for adoption of the 2021 Draft Historic Preservation Guidelines as presented in Exhibit D was made at this meeting. An annotated version of the 2021 Historic Preservation Guidelines is also provided in Exhibit E.

With the completion of the 2021 Draft Historic District Guidelines, staff revised the Historic District section of the Zoning Ordinance to incorporate proposed Historic Preservation Guidelines. The 2021 Draft Historic District Ordinance will be presented in a separate agenda item.

### **DISCUSSION:**

The Historic District Commission over the course of the last year and half beginning with the consultant kick-off meeting in December of 2019 and ending with recommendation for adoption of the Draft Historic Preservation Guidelines Historic District Commission Meeting in August, has worked in a public process to revise the Historic Preservation Guidelines.

Additionally, to ensure adequate public feedback, as previously described, the Commission sponsored two public input meetings to allow residents of the Chautauqua, Miller and Southridge Historic Districts, an opportunity to provide feedback on the 2021 Draft Historic Preservation Guidelines. With the completion of the public revision process the Historic District Commission forwards the Draft Historic Preservation Guidelines for consideration and adoption.

Below staff provides a summation of the proposed revisions found in the 2021 Draft Historic Preservation Guidelines.

## SUMMARY OF REVISIONS TO THE HISTORIC PRESERVATION GUIDELINES

1. The following work items were made eligible for approval for a Certificate of Appropriateness through the Administrative Bypass process by city staff:
  - a. Garden and accessory structures less than 400 sq. ft. in rear yard
  - b. Storm shelters less than 120 sq. ft. in rear yard
  - c. Parking/concrete areas less than 400 sq. ft. in rear yard
  - d. Walk ways in rear yard
  - e. Swimming pools in rear yard
  - f. Garage door replacement either with wood or wood composite door
  - g. Front & side yard fences less than 4' – clarified the criteria to be met
  - h. Rear yard fences less than 6' - clarified the criteria to be met
  - i. National Register of Historic Places plaque – limited to 2 sq ft.
  - j. Removal of non-historic siding to reveal historic siding
  - k. Solar panels and solar tubes on the rear of house
  - l. Solar racks less than 120 sq. ft.
  - m. Window replacement – clarified criteria to be met
  - n. Storm windows & doors and screens
  - o. Fabric awnings
  - p. Front doors
  - q. Screening of rear porches
  - r. Rear balconies or porches less than 120 sq. ft.
  - s. Replacement of steps in-kind while allowing for reconfiguration of steps to meet building codes
  - t. Replacement of concrete porch flooring in-kind
  - u. Accessibility ramps and handrails
  - v. Decks less than 300 sq. ft. that are located on the rear of the structure
  - w. Demolition or relocation of accessory structure less than 120 sq. ft.
  
2. A majority of the revisions were made in the following sections of the Historic Preservation Guidelines:
  - a. Garages:
    - i. Garages capped at 575 sq. ft. or 50% of the principal structure, whichever is smaller;
    - ii. Cumulative sq. ft. of garage(s) can be no larger than foot print of house
    - iii. Maximum of 2 garages per property;
    - iv. Allows the use of cement fiberboard when visibility is limited.
  - b. Accessory structures:
    - i. Accessory structures were divided into three separate sections with separate design guidelines for each:
      1. Accessory Structures 120 – 400 sq. ft.;
      2. Secondary Structures 400 sq. ft. or larger, this includes such structures as art studios & garage apartments;
      3. Garages.

- c. Windows and Doors:
  - i. Window and doors were separated into two sections, each with their own design guidelines;
  - ii. Clarified when deteriorated windows are allowed to be replaced – must be more than 50% deteriorated;
  - iii. Clarified appropriate replacement materials.
- d. Fences:
  - i. Removed fence palette as it was not being used;
  - ii. Allowed for chain link in rear yard.
- e. Energy efficiency:
  - i. Guidelines added for such items as solar panels and solar racks;
- f. Material Sections:
  - i. Additional sections were added for stucco, masonry, metal and cement fiberboard.

### **CONCLUSION:**

Staff presents Resolution R-2122-58 to City Council for discussion and consideration.

At their December 9, 2021 meeting, Planning Commission unanimously recommended adoption of Resolution R-2122-58, by a vote of 7-0.

### **Exhibits:**

Exhibit A - 2018 Historic Preservation Update Subcommittee List of Revisions

Exhibit B – 2020 Draft Historic Preservation Handbook & Guidelines

Exhibit C - 2021 Summary of Public Comments on Revised HP Guidelines

Exhibit D - 2021 Historic Preservation Guidelines Clean

Exhibit E - 2021 Annotated Historic Preservation Guidelines

Exhibit F – August 4, 2021 Historic District Commission Minutes

# Resolution

R-2122-58

A RESOLUTION OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, ADOPTING THE HISTORIC PRESERVATION GUIDELINES TO BE USED BY THE NORMAN HISTORIC DISTRICT COMMISSION IN REVIEWING PROPOSED ADDITIONS, ALTERATIONS, AND DEMOLITIONS TO STRUCTURES LOCATED IN THE HISTORIC DISTRICTS.

- §1. WHEREAS, the Miller, Chautauqua and Southridge Historic Districts, more specifically delineated on the maps, attached hereto as Exhibit "A", have unique historical characteristics and issues; and
- §2. WHEREAS, the Historic District Commission and the Council of the City of Norman recognize the concerns of the citizens of these three districts regarding the fair and consistent regulation of changes to historic structures and the impact that changes have on the historic integrity of the areas; and
- §3. WHEREAS, the Historic District Commission has conducted two public meetings to gather community input on the content of the guidelines, attached hereto as Exhibit "B"; and
- §4. WHEREAS, the Historic District Commission has undertaken the task of clarifying and updating the current *Preservation Guidelines for the Miller, Chautauqua and Southridge Historic Districts* which are used to determine the appropriateness of changes to structures within the three districts and the future policy of preservation activity; and
- §5. WHEREAS, exterior changes to structures in historic districts have great potential to alter the historic characteristics of these structures, and therefore, require guidelines that are clear, consistent, and address modern expectations for property use; and
- §6. WHEREAS, the revised guidelines will assist both property owners and the Historic District Commission in evaluating the appropriateness of proposed changes to structures in historic districts; and
- §7. WHEREAS, Section 12 of Section 22:429 of the Zoning Ordinance of the City of Norman requires City Council approval of any revisions to the Preservation Guidelines prior to adoption of the revisions by the Historic District Commission.



R-2122-58

NOW, THEREFORE, BE IT RESOLVED BY THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA,

- §8. That the Historic Preservation Guidelines, as revised, are hereby adopted.
- §9. That said guidelines shall remain in effect until the Council of the City of Norman adopts any further amendments on a future date.
- §10. That the Historic District Commission shall be authorized to correct any scrivener's errors that may be discovered without a review by the Council of the City of Norman.
- §11. That the Historic District Commission of the City of Norman is directed to use these new adopted Historic Preservation Guidelines in reviewing all applications for Certificates of Appropriateness.

PASSED AND ADOPTED this \_\_\_\_ day of \_\_\_\_\_, 2022.

\_\_\_\_\_  
Mayor

ATTEST:

\_\_\_\_\_  
City Clerk



RESOLUTION NO. R-2122-58

ITEM NO. 14

**STAFF REPORT**

**ITEM:** A RESOLUTION OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, ADOPTING THE HISTORIC PRESERVATION GUIDELINES TO BE USED BY THE NORMAN HISTORIC DISTRICT COMMISSION IN REVIEWING PROPOSED ADDITIONS, ALTERATIONS, AND DEMOLITIONS TO STRUCTURES LOCATED IN THE HISTORIC DISTRICTS.

**BACKGROUND:**

After more than 10 years of use, the Historic District Commission found both the 2008 Preservation Guidelines and 2009 Historic Preservation Handbook were in need of revisions. The following areas were identified as needing to be addressed: corrections of errors and omissions; development of new layout for the handbook resulting in a user-friendly document for citizens, Commission and staff; incorporation of the Southridge District; expansion of proposed work approvable by Administrative Bypass review and the creation of standards for those Bypass items; clarification of routine maintenance and repair items; creation of guidelines addressing energy efficiency requests (windows and solar); and revisions to the guidelines for parking, windows, doors and additions.

The City Council on August 12, 2008 adopted by resolution a set of Historic Preservation Guidelines. The newly adopted Preservation Guidelines were then incorporated by staff into a Historic Preservation Handbook in March of 2009. The Handbook was developed to assist Historic District property owners with best practices in maintaining and renovating their historic properties along with providing the required Historic Preservation Guidelines that must be followed. Copies of the Handbook were distributed to every property owner in the Chautauqua and Miller Historic Districts. Since that time, staff has been tracking potential revisions needed to the 2008 Historic Preservation Guidelines and the 2009 Historic Preservation Handbook.

In July of 2018, the Historic District Commission initiated the update of the 2008 Historic Preservation Guidelines and 2009 Preservation Handbook by forming the Historic Preservation Guideline Update Subcommittee. A copy of identified revisions tracked for both documents over the years by staff was used as a beginning point for discussions. The Subcommittee met twice in-person and then continued reviewing the Handbook virtually until December of 2018.

The Subcommittee's review process resulted in a list of corrections to be used as a guide to a future revision process by a consultant. Items identified by the Subcommittee included: the clarification of design guidelines for garages, particularly the size, height and materials; the addition of a door replacement process; window replacement clarification; coordination of demolition guidelines with criteria listed in the Historic District Ordinance; the need for more information on alternative materials and solar panels; as well as minor corrections such as typos. See Exhibit A.

On July 9, 2019, the Council accepted Certified Local Government grant funds through a contract with the State Historic Preservation Office. As part of that grant acceptance, the City appropriated matching funds for the professional services of a consultant to update the City of Norman Historic Preservation Guidelines and Handbook. A Request for Proposals for professional services was issued September 24, 2019.

A contract between the City of Norman and Mainstreet Architects, Inc., out of San Antonio, TX, was signed on November 19, 2019 for the update of the Historic Preservation Guidelines and Handbook. A kick-off meeting was held on December 9, 2019 with the consultant to provide an opportunity for the Historic District Commission to discuss desired revisions. The Commission highlighted particular issues of concern, including: materials, garages, parking, additions, windows, energy efficiency aspects, the lack of historical information regarding Southridge Historic District, graphics of appropriate architectural elements and concepts, were all identified as elements that needed to be revised.

The consultant used both the Subcommittee's list of identified corrections and the Commission input as a starting point for revisions to the Historic Preservation Guidelines and Handbook. The consultant met again with the Historic District Commission on January 27, 2020 to review an outline of proposed changes. In February of that year the Coronavirus Pandemic struck and the revision process slowed. The contract with the consultant was extended to provide additional time to complete the revisions.

The consultant presented a proposed Draft Historic Preservation Guideline and Handbook at the October 5, 2020 Historic District Commission Workshop. The Draft Guidelines and Handbook included revisions to the following sections: materials, additions, garages, energy efficiency, and background information on Southridge. The consultant also added graphic illustrations of appropriate architectural elements for the various house styles. The Commission gave feedback at this meeting and the consultant incorporated those revisions, delivering a final copy of the Historic Preservation Guidelines and Handbook in November of 2020. See Exhibit B.

In January of 2021, staff prepared the Draft Historic Preservation Guidelines for the Historic District Commission's consideration and recommendation for adoption to the City Council. The Historic Preservation Handbook document is not part of this adoption process, as the contents are not regulatory. Once the Draft Historic Preservation Guidelines are adopted they will be incorporated into a revised Historic Preservation Handbook at a later date.

The Draft Historic Preservation Guidelines prepared by staff, were discussed by the Historic District Commission at three public Historic District Commission Workshops held on March 2, April 5, and May 3, 2021. Further refinements were made at these Workshops regarding the garage, addition, accessory structures, and windows sections by the Commission with staff's assistance, resulting in the 2021 Draft Historic Preservation Guidelines.

The 2021 Draft Historic Preservation Guidelines were posted to the City of Norman website in late June. At the same time, postcards were mailed to every property owner in the three designated Historic Districts inviting them to make comments at two Historic Preservation Guidelines Public Input Meetings to be held in July.

The Historic Preservation Guidelines Public Input Meetings were held on July 19 and July 26, 2021 in the City Council Chambers. Staff presented a summary of revisions to the Historic Preservation Guidelines at both meetings and citizens provided feedback and comments. Twelve citizens attended the July 19 meeting with three of them providing comments. At the

July 26, meeting, there were six citizens in attendance with two of them providing comments. In addition, staff received emailed comments or in-person comments from three other citizens. A summary of the comments are attached as Exhibit C.

At the August 2, 2021 Historic District Commission Meeting, public comments were reviewed and/or incorporated into the Guidelines. A recommendation for adoption of the 2021 Draft Historic Preservation Guidelines as presented in Exhibit D was made at this meeting. An annotated version of the 2021 Historic Preservation Guidelines is also provided in Exhibit E.

With the completion of the 2021 Draft Historic District Guidelines, staff revised the Historic District section of the Zoning Ordinance to incorporate proposed Historic Preservation Guidelines. The 2021 Draft Historic District Ordinance will be presented in a separate agenda item.

### **DISCUSSION:**

The Historic District Commission over the course of the last year and half beginning with the consultant kick-off meeting in December of 2019 and ending with recommendation for adoption of the Draft Historic Preservation Guidelines Historic District Commission Meeting in August, has worked in a public process to revise the Historic Preservation Guidelines.

Additionally, to ensure adequate public feedback, as previously described, the Commission sponsored two public input meetings to allow residents of the Chautauqua, Miller and Southridge Historic Districts, an opportunity to provide feedback on the 2021 Draft Historic Preservation Guidelines. With the completion of the public revision process the Historic District Commission forwards the Draft Historic Preservation Guidelines for consideration and adoption.

Below staff provides a summation of the proposed revisions found in the 2021 Draft Historic Preservation Guidelines.

### **SUMMARY OF REVISIONS TO THE HISTORIC PRESERVATION GUIDELINES**

1. The following work items were made eligible for approval for a Certificate of Appropriateness through the Administrative Bypass process by city staff:
  - a. Garden and accessory structures less than 400 sq. ft. in rear yard
  - b. Storm shelters less than 120 sq. ft. in rear yard
  - c. Parking/concrete areas less than 400 sq. ft. in rear yard
  - d. Walk ways in rear yard
  - e. Swimming pools in rear yard
  - f. Garage door replacement either with wood or wood composite door
  - g. Front & side yard fences less than 4' – clarified the criteria to be met
  - h. Rear yard fences less than 6' - clarified the criteria to be met
  - i. National Register of Historic Places plaque – limited to 2 sq ft.
  - j. Removal of non-historic siding to reveal historic siding
  - k. Solar panels and solar tubes on the rear of house
  - l. Solar racks less than 120 sq. ft.
  - m. Window replacement – clarified criteria to be met
  - n. Storm windows & doors and screens
  - o. Fabric awnings
  - p. Front doors
  - q. Screening of rear porches
  - r. Rear balconies or porches less than 120 sq. ft.

- s. Replacement of steps in-kind while allowing for reconfiguration of steps to building codes
  - t. Replacement of concrete porch flooring in-kind
  - u. Accessibility ramps and handrails
  - v. Decks less than 300 sq. ft. that are located on the rear of the structure
  - w. Demolition or relocation of accessory structure less than 120 sq. ft.
2. A majority of the revisions were made in the following sections of the Historic Preservation Guidelines:
- a. Garages:
    - i. Garages capped at 575 sq. ft. or 50% of the principal structure, whichever is smaller;
    - ii. Cumulative sq. ft. of garage(s) can be no larger than foot print of house
    - iii. Maximum of 2 garages per property;
    - iv. Allows the use of cement fiberboard when visibility is limited.
  - b. Accessory structures:
    - i. Accessory structures was divided into three separate sections with separate design guidelines for each:
      - 1. Accessory Structures 120 – 400 sq. ft.;
      - 2. Secondary Structures 400 sq. ft. or larger, this includes such structures as art studios & garage apartments;
      - 3. Garages.
  - c. Windows and Doors:
    - i. Window and doors were separated into two sections, each with their own design guidelines;
    - ii. Clarified when deteriorated windows are allowed to be replaced – must be more than 50% deteriorated;
    - iii. Clarified appropriate replacement materials.
  - d. Fences:
    - i. Removed fence palette as it was not being used;
    - ii. Allowed for chain link in rear yard.
  - e. Energy efficiency:
    - i. Guidelines added for such items as solar panels and solar racks;
  - f. Material Sections:
    - i. 4050 Additional sections were added for stucco, masonry, metal and cement fiberboard.

### **CONCLUSION:**

Staff presents Resolution No. R-2122-58 to Planning Commission for discussion and consideration.

### **Exhibits:**

Exhibit A - 2018 Historic Preservation Update Subcommittee List of Revisions  
 Exhibit B – 2020 Draft Historic Preservation Handbook & Guidelines  
 Exhibit C - 2021 Summary of Public Comments on Revised HP Guidelines  
 Exhibit D - 2021 Historic Preservation Guidelines Clean  
 Exhibit E - 2021 Annotated Historic Preservation Guidelines  
 Exhibit F – August 4, 2021 Historic District Commission Minutes

Section	Page	Guideline	Note/Change	Committee Member
2.1	23	2.1.1	Edit: Needs more clarity	Cameron Brewer
2.1	23		Add: Matching language for potential tree ordinance/historic trees (caliper minimum)	Cameron Brewer
2.3	26	2.3.5	Add: Address alternate materials (including required setback to be eligible for use)	Cameron Brewer
2.3	26	2.3.5	Add: Maximum Garage Footprint (Idea: Proportional to home footprint?)	Cameron Brewer
2.3	26	All	Edit: Needs more clarity re: new build vs rehab	Cameron Brewer
2.3	26	All	Add: Graphical palette of accepted garage door styles (similar to fences on p. 32)	Cameron Brewer
2.3	26	2.3.5	Edit: Remove "traditional" in front of "height" or add more definitive language	Cameron Brewer
2.3	26	2.3.2	Edit: Remove "according to pertinent guidelines" - redundant	Cameron Brewer
2.3	26	2.3.3	Edit: "consider compatible substitute materials" - needs more clarity	Cameron Brewer
2.4	28		Add: Off-street parking/rear yard/side yard parking	Cameron Brewer
2.4	29	2.4.5	Add: "Single" in front of "Driveway Approaches"	Cameron Brewer
2.4	29	2.4.6	Edit: "unless demonstrated as historically present" to "unless there is historic documentation"	Cameron Brewer
2.4	29		Add: Guideline regarding second drive (e.g. existing driveway in front, but add'l paving in back)	Cameron Brewer
3.1	39		Edit: First two bullet points are duplicated	Emily Wilkins
3.1	39		Edit: Remove comma after "on" in third bullet	Emily Wilkins
3.1	39	3.1.7	Add: other materials?	Emily Wilkins
3.2	40		Edit: In first paragraph load-bearing should have a hyphen	Emily Wilkins
3.2	41		Edit: In the lead section the referenced URL no longer exists	Emily Wilkins
3.2	42	3.2.4	Add: or not original to this historic structure	Emily Wilkins
3.3	43/44		Edit: Repointing is duplicated on pages 43 & 44	Emily Wilkins
3.3	44		Edit: 2 periods at the end of final paragraph	Emily Wilkins
3.3	45	3.3.6	Edit: color not controlled	Emily Wilkins
3.3	45	3.3.7	etc.	Emily Wilkins
3.4	46		Edit: Under maintenance "repairing" should be "repair"	Emily Wilkins
3.5	56	3.5.9	Edit: add phrase "deteriorated beyond repair" at end	Glen R
3.5	65	3.5.10	Edit: May be approved by admin. bypass	Glen R
3.5	53	3.5.11	Edit Remove "50%"	Glen R
3.5	53	3.5.11	Omission: address issue of windows and doors	Glen R
3.5	53	3.5.12	Add: "where not visible from front row	Glen R
3.5	53	3.5.13	Add: "conform to historic material, style, shape, and location"	Glen R
3.5	52	3.5.1	Edit: comma after WINDOWS	Glen R
3.5	52	3.5.3	Omission: applies to front doors only	Glen R
3.5	52	3.5.4	Omission: does this include the rear of the house? Second floor	Glen R
3.5	52	3.5.5	Omission: does this apply to all doors?"	Glen R
3.5	52	3.5.7	Edit: include door in heading	Glen R
3.5	52	3.5.7	Omission: specify wood doors and windows	Glen R
3.5	52	3.5.7	Omission: oor details only	Glen R
3.5	52	3.5.7	Add: storm windows and doors approved by Admin. bypass	Glen R
3.6	56	3.6.8	Add: Specify this applies to front facade	Glen R
3.6	56		Add: new guideline on hand rails	Glen R
3.6	56		Add: new guideline on floor decking	Glen R
3.6	56		Add: guideline on floor decking on porches	Glen R
3.6	56		Add: new guideline on wood decking added to concrete	Glen R
3.6	53	3.6.11	Omission: allow approval admin. bypass	Glen R
3.8	59		Edit: Use of word "should" v. "may" throughout section	Lee Hall
3.8	59		Edit: Satellite Dishes - Clarify when COA or Administrative Bypass is needed	Lee Hall
3.8	59		Edit: Mechanical - Is a COA or Administrative Bypass needed?	Lee Hall
3.9	60		Edit: Storm Windows - Move section on storm windows to Windows section	Lee Hall
3.9	60		Edit: Storm Windows - Consider rewriting storm window section as standards	Lee Hall
3.9	61		Edit: Window Awnings - In conflict with 3.5.13	Lee Hall
3.9	61		Edit: Window Awnings - Move to 3.5.13 and incorporate	Lee Hall
3.9	61		Add: Solar Panels - develop guidelines and standards	Lee Hall
3.9	61		Add: Solar Panels - Rear facing	Lee Hall
3.9	61		Add: Solar Panels - Do not permanently alter roof and/or structure	Lee Hall
3.9	61		Add: Consider other energy efficient issues that are not currently included	Lee Hall
3.10	63	3.10.1	Edit: Security bars - Move to Windows section	Lee Hall
3.10	63	3.10.2	Edit: Ramps - Develop standards to be met by Administrative Bypass	Lee Hall
3.10	63	3.10.2	Add: Ramps - Include rear or side location in standards for Administrative Bypass	Lee Hall
3.10	63	3.10.2	Edit: Ramps - Remove "May Be" in first sentence and second sentence	Lee Hall
3.10	63	3.10.2	Edit: Ramps - Consider adding other approved materials in addition to wood.	Lee Hall
3.10	63	3.10.3	Edit: Lifts - Remove "a Certificate of Appropriateness"	Lee Hall
3.10	63	3.10.3	Add: Lifts - Insert "review by Historic District Commission" at end of sentence	Lee Hall
3.10	63	3.10.4	Add: Safety Aids - Approve by Administrative Bypass	Lee Hall
3.10	63	3.10.5	Edit: Modify doorways - COA for front door only?	Lee Hall
3.10	63	3.10.5	Edit: Modify doorways - Administrative bypass for rear door or entry?	Lee Hall
4.1	66		Add: From side caption. What alternative deck materials?	Taber H.
4.1	66		Edit: Remove "undesirable" when referencing removal architectural elements & mature trees	Taber H.
4.1	67		Edit: Make side caption into guidelines/standards	Taber H.
4.2	69		Edit: More direct wording entire section	Taber H.
4.2	69	4.2.2	Edit: Clarity, ie additions limited to rear 50% of home	Taber H.
4.2	69	4.2.2	Edit: Remove unclear wording, "Generally considered inappropriate"	Taber H.
4.2	69	4.2.4	Edit: Mature trees retained with reference to additions	Taber H.
4.2	69	4.2.5	Edit: Remove "inappropriate", make direct/clear what can/cant be done and penalties	Taber H.
4.2	69	4.2.6	Edit: Move to new structures or garage section	Taber H.
4.2	69	Caption	Edit: make into guideline or as new paragraph to 4.2.3	Taber H.
4.2	69	Caption	Edit: Possible restriction to rear 50% of home for roofline alterations	Taber H.
4.3	72		Edit: Add "secondary" to headline	Taber H.
4.3	72	4.3.4	Edit: Size restrictions to secondary structures	Taber H.
4.3	72	4.3.4	Add: Historic or traditional side/rear setbacks for new primary or secondary infill	Taber H.
4.3	72	4.3.4	Add: Diagram consistent setbacks & Same porportion of lot filled by new structure as others	Taber H.
4.3	72	4.3.4	Add: New structure to be of sam time period. avoid false historic appearance	Taber H.
5.2	76		Edit: Where is 90 day postponement for demolition mentioned in guidelines	Jim G
5.2	76		Add: Does demolition regulation include non-conforming structures?	Jim G
5.2	76		Add: demolition definition should be included in definition section	Jim G
5.2	77		Add: discussion of demolition of accessory structures, such as garages	Jim G
5.2	77		Add: discuss sties that will remain vacant	Jim G

Committee Member	Section Assignment
Cameron	2.1-2.4
Anne	2.5-2.8
Emily	3.1-3.4
Glen	3.5-3.7
Lee	3.8-3.10
Taber	4.1-4.3
Jim	5.1-5.2

**Comment**

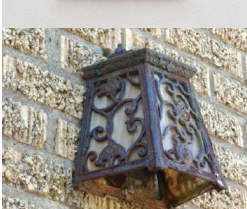
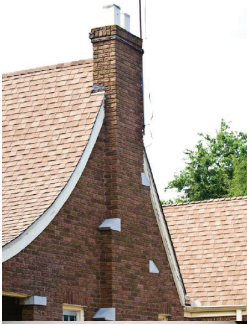
p. 60, 62, 63 - Use of text boxes??? Format in a different way  
p. 60 - Instead of use of text box, create bibliography of relevant articles.  
p.62 - Instead of use of text box, create resource guide.

**Committee Member**

Lee Hall  
Lee Hall  
Lee Hall



# HISTORIC PRESERVATION STANDARDS AND GUIDELINES NORMAN HISTORIC DISTRICTS



CITY OF NORMAN, OKLAHOMA  
2020



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# Historic Preservation Standards and Guidelines Norman, Oklahoma

## A Publication of the Norman Historic District Commission

This printed document represents the City of Norman Historic District Standards and Guidelines (also known as “the Standards and Guidelines”) as adopted by the City of Norman City Council on \_\_\_\_\_ with an effective date of \_\_\_\_\_. The Standards and Guidelines may be revised from time to time. The most current version of the Standards and Guidelines is available from The City of Norman through the City Clerk’s Office.

### Norman City Council

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Ward 1, Kate Bierman

Ward 2, Joe Carter, DVM

Ward 3, Alison Petrone

Ward 4, Lee Hall

Ward 5, Michael Nash

Ward 6, Elizabeth Foreman

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Ward 8, Matthew Peacock

### Norman Historic District Commission

Chair, Russell Kaplan

Mitch Baroff

Greg Heiser

Joan Koos

Brent Swift

Emily Wilkins

Barrett Williamson

Michael Zorba

Taber Halford

### Staff

Anais Starr, Historic Preservation Officer

**Acknowledgement of Support:** The development and printing of this report has been funded with federal funds from the National Park Service, U.S. Department of the Interior. However, the contents and opinions do not necessarily reflect the views or policies of the Department of the Interior, nor does the mention of trade names or commercial products, if any, constitute endorsement or recommendation by the Department of the Interior.

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# SECTION

# 1

## INTRODUCTION

## 1.1 Purpose of the Design Guidelines

---

*By authority of the Norman Code of Ordinances, sec. 429.3, Historic District Commission approval via a Certificate of Appropriateness is required for all new construction, structural alterations to the exterior of an existing structure, and demolition within a historic district.*

### 1. Preserve and Maintain the Character

These Standards and Guidelines are intended to preserve and maintain the character of the historic buildings in Norman. They reinforce and protect the important features of the historic districts and define those visual elements which are common to each district as well as the qualities unique to this community.

### 2. Preserve Integrity and Enhance Value

This document will help preserve the integrity of historic buildings and enhance the value of the historic district for the private investor, residents and owners, and the community as a whole. Changes to an individual building should not be considered in isolation. Modifications affect the block as a whole and must have the broad interest of the community in mind.

### 3. Limited to Exterior Site

The Standards and Guidelines do not address the use of the building or its interior. Only the exterior portions, which includes new construction, additions, and rehabilitation of the building, must comply with the guidelines set forth.

### 4. Look at the Building's Original Use

These Standards and Guidelines must be applied to a building based on its original use and construction. For example, although a former residence may currently be used as an office, it is still subject to the standards and guidelines appropriate to a residential building.



These Standards and Guidelines are designed to assist everyone with a stake in preserving Norman's Historic Districts. They are an essential tool in helping the Historic District Commission fulfill its mission to preserve, protect, and educate the public through the application of consistent standards and guidelines.

**Who Is This Document For?**

This handbook is intended to assist property owners in planning projects which will alter the exterior of their property and therefore impact the overall character and integrity of the historic districts. For property owners, residents, and contractors, the Standards and Guidelines provide clear guidance in planning projects that are sympathetic to the special character of Norman's designated Historic Districts.

For Historic District Commissioners and city staff, the Standards and Guidelines offer guidelines by which to evaluate proposed changes to historic structures.

**Why Historic Preservation Matters to Norman**

Historic preservation is vitally important to the Norman community — now more than ever. Historic buildings embody a distinctive form of our city's architecture that will never again be duplicated, and these buildings and their surroundings add an irreplaceable component to the character and personality of Norman. The architecture of our neighborhoods shapes our sense of place and our feelings about where we live. This is what makes the neighborhoods worthy of protection.

**The Mission of Norman's Historic District Commission**

The Norman Historic District Commission serves as the City Council's official historic preservation body to identify, protect, and educate the public about Norman's historic resources.

## 1.2 How to Use This Document

Whether the proposed work to the building is a small repair or a major renovation or addition, it is important to consult pertinent Standards and Guidelines for guidance on your project.

These Standards and Guidelines will be used by the City of Norman to provide an objective basis for the decisions of the Historic District Commission and staff.

This document is laid out in five general characteristics of a historic property. Each characteristic is then divided into architectural features of that characteristic.

Each section contains the following items:

- **History and Development** addresses about the origin and evolution of the discussed feature.
- The **Policy** statement is the guiding principle by which the Standards and Guidelines have been established.
- **Things to Consider** addresses particular conditions that may affect your approach to a new project.
- **Maintenance and Recommendations** help guide you in preserving character defining features in your property.
- The **Standards for Administrative Bypass** are used by the Historic Preservation Officer to help determine if a Certificate of Appropriateness (COA) can be granted without Historic District Commission review.
- **Guidelines** are the specific rules used by the Commission to determine if a project is eligible to receive a Certificate of Appropriateness (COA). The use of guidelines enables the Commission to make consistent, policy-based decisions that will protect the city’s historic resources for years to come.
- For the purpose of clarification, all **Guidelines** and **Standards** in this handbook are italicized.

The Standards and Guidelines specifically look at the following design elements:

Height	Rhythm of entrance and/or porch projection
Proportion of building’s front façade	Relationship of materials and texture
Proportion of openings within the facility	Roof shapes
Rhythm of solids to voids in front façades	Walls of continuity
Rhythm of spacing of buildings on streets	Scale of building
	Site and Setting

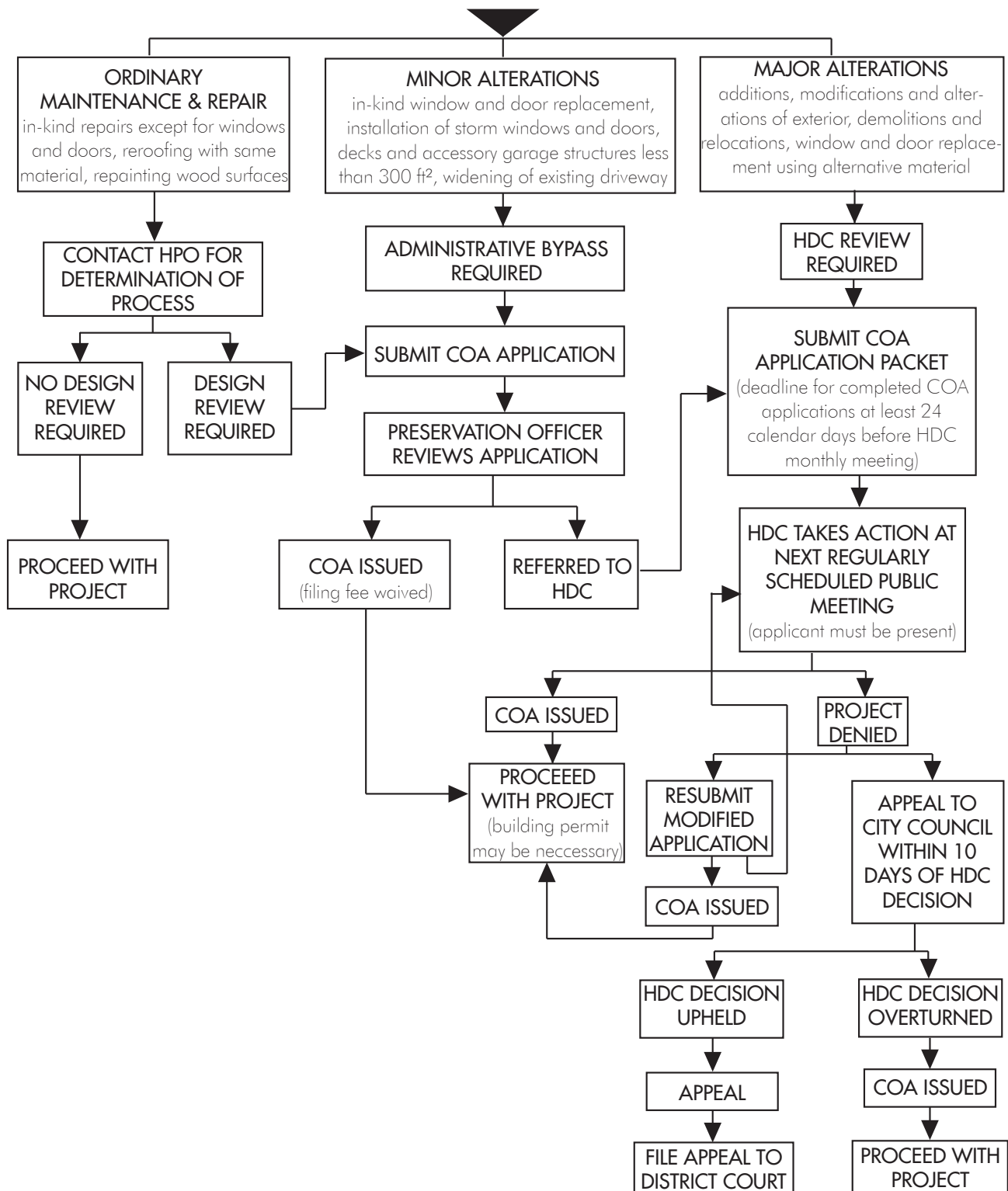
## 1.3 Frequently Used Terms

*Throughout this handbook a number of terms are frequently used to reflect the design principles that the Historic District Commission will consider when making decisions.*

- **Appropriate:** Rehabilitation and new construction actions especially suitable or compatible with the design standards and guidelines.
- **Character:** Attributes, qualities and features that make up and distinguish a particular place or development and give such place a sense of definition, purpose and uniqueness.
- **COA:** A Certificate of Appropriateness is required before undertaking any construction, structural alteration, or demolition within a Historic District.
- **Compatible/Compatibility:** The characteristics of different uses, activities, materials, and design that permit them to be located near each other in a visual harmony and without conflict.
- **Contributing Resource:** A historic building or site that retains the essential architectural integrity of its original design or condition.
- **Guidelines:** Criteria that must be used by the Historic District Commission in reviewing proposed construction, structural alteration, or demolition and ensuring development appropriate to the historic house and neighborhood.
- **Like-for-Like:** Use of the same or similar materials to the original or existing materials. Also called “in-kind.”
- **Mass:** The overall bulk, size, volume, or magnitude of a structure.
- **Preservation:** The adaptive use, conservation, protection, reconstruction, restoration, rehabilitation or stabilization of sites, buildings, districts, structures, or objects significant to the heritage of the people of Norman.
- **Proportion:** The relative physical sizes within and between buildings and building components.
- **Recommended:** Suggested but not mandatory actions outlined in the design guidelines.
- **Rehabilitation:** The act or process of making possible a compatible use for a property through repair, alterations, and additions, while preserving those portions or features which convey its historic, cultural, or architectural values.
- **Scale:** The harmonious proportion of parts of a building, structure, or monument to one another and to the human figure.
- **Significant (Characteristics of Historical or Architectural Resources):** Those characteristics that are important to, or expressive of, the historical, architectural or cultural quality and integrity of the resource and its setting; and includes, but is not limited to, building material, detail, height, mass, proportion, rhythm, scale, setback, setting, shape, street accessories and workmanship.
- **Standards:** Criteria that must be met to have work approved by administrative bypass

## 1.4 Does My Project Require a Certificate of Appropriateness?

### DOES MY PROJECT REQUIRE A CERTIFICATE OF APPROPRIATENESS?



## 1.5 Administrative Bypass

Certain specific project requests for alterations to the exterior of a property or site may be issued a Certificate of Appropriateness approvable through a process known as Administrative Bypass.

Each section of the *Historic Preservation Standards and Guidelines* contains a set of Standards for projects approvable through the Administrative Bypass process.

### **Applying for Certificate of Appropriateness by Administrative Bypass:**

In order to obtain a Certificate of Appropriateness by Administrative Bypass, an application form and support documentation that sufficiently describes the proposed work must be submitted to staff prior to commencement of work.

Support documents that may be required by staff to allow for a complete review include the following:

- Sketches
- Photographs
- Floor plans
- Site plans
- Elevation drawings
- Trees preservation plan
- Material lists
- Material samples
- And/or other means of adequately describing the work proposed.

Staff will make a determination of the support documents required for a complete review. There is not an application fee for a Certificate of Appropriateness by Administrative Bypass. There is not a deadline; however, it takes 5-7 days to process a request. Therefore, applicants should submit in a timely manner to ensure issuance prior to the desired installation date of the proposed work.

If Administrative Bypass is denied by the Historic Preservation Officer, or authorized designee, the applicant shall have the right to appear before the Historic District Commission at its next regularly scheduled meeting time for formal action regarding approval or denial of the Certificate of Appropriateness.

Any person aggrieved by the decision of the Historic District Commission regarding a Certificate of Appropriateness may seek relief through the appeal process listed in Section 429.3(10) Appeals of the Zoning Ordinance.

Any person, firm or corporation who violates the provisions listed in the Historic Preservation Standards and Guidelines Book, will be prosecuted per Section 429.3(11) Penalty of the Zoning Ordinance.

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## SECTION

# 2

## PRESERVATION BASICS



## 2.1 Introduction to the Standards

1. The Secretary of the Interior is responsible for establishing standards for all programs under departmental authority and for advising federal agencies on the preservation of historic properties listed in or eligible for listing in the National Register of Historic Places. In partial fulfillment of this responsibility the Secretary of the Interior's Standards for the Treatment of Historic Properties have been developed to guide work undertaken on historic properties; there are separate standards for preservation, rehabilitation, restoration, and reconstruction. The Standards for Rehabilitation (codified in 36 CFR 67) comprise that section of the overall treatment standards and address the most prevalent treatment. "Rehabilitation" is defined as the act or process of making possible a compatible use for a property through repair, alterations, and additions while preserving those portions or features which convey its historical, cultural, or architectural values.

For more information about the *Secretary of the Interior's Standards for Rehabilitation* visit the National Park Service's Technical Preservation Service website found at [www.nps.gov/tps](http://www.nps.gov/tps)

The primary goal of Technical Preservation Services is to publish state-of-the-art information that conveys to the public responsible methods of caring for historic buildings.

2. Initially developed by the Secretary of the Interior to determine the appropriateness of proposed project work on registered properties supported by the Historic Preservation Fund grant-in-aid program, the Standards have been widely used over the years—particularly to determine if a rehabilitation project qualifies as a Certified Rehabilitation for Federal Historic Preservation Tax Incentives. In addition, the Standards have guided federal agencies in carrying out their responsibilities for properties in federal ownership or control and state and local officials in reviewing both federal and non-federal rehabilitation proposals. They have also been adopted by historic district and planning commissions across the country.

3. The intent of the Standards is to assist in the long-term preservation of historic materials and features. The Standards pertain to historic buildings of all materials, construction types, sizes and occupancy and include the exterior and the interior of the buildings. They also encompass the building's site and environment, including landscape features, as well as attached, adjacent or related new construction. To be certified for federal tax purposes, a rehabilitation project must be determined by the Secretary of the Interior to be consistent with the historic character of the structure(s) and, where applicable, the district in which it is located.

4. As stated in the definition, the treatment "rehabilitation" assumes that at least some repair or alteration of the historic building will be needed in order to provide for an efficient contemporary use; however, these repairs and alterations must not damage or destroy materials, features or finishes that are important in defining the building's historic character. For example, certain treatments—if improperly applied—may cause or accelerate physical deterioration of the historic building. This can include using improper repointing or exterior masonry cleaning techniques or introducing insulation that may damage historic fabric. Any of these treatments will likely result in a project that does not meet the Standards. Similarly, exterior additions that duplicate the form, material and detailing of the historic structure to the extent that they compromise its historic character will also fail to meet the Standards.

## 2.2 Secretary of the Interior Standards for Rehabilitation

Both the Historic District Ordinance and the guidelines portion of the *Norman Historic Preservation Handbook* include *The Secretary of the Interior's Standards for Rehabilitation and Guidelines for Rehabilitating Historic Buildings* (US Department of the Interior/National Park Service, Heritage Preservation Services, Revised 1990).

Projects must meet Secretary of the Interior's Standards in addition to other sections.

1. **Make Minimal Changes.** *A property shall be used for its historic purpose or be placed in a new use that requires minimal change to the defining characteristics of the building and its site and environment.*
2. **Retain Historic Character.** *The historic character of a property shall be retained and preserved. The removal of historical materials or alterations of features and spaces that characterize a property shall be avoided.*
3. **Avoid False Historical Impressions.** *Each property shall be recognized as a physical record of its time, place, and use. Changes that create a false sense of historical development, such as adding conjectural features or architectural elements from other buildings, shall not be undertaken.*
4. **Acknowledge Changes Over Time.** *Most properties change over time; those changes that have acquired historic significance in their own right shall be retained and preserved.*
5. **Preserve Distinctive Features.** *Distinctive features, finishes, and construction techniques or examples of craftsmanship that characterize a historic property shall be preserved.*
6. **Repair Rather Than Replace.** *Deteriorated historic features shall be repaired rather than replaced. Where the severity of deterioration requires replacement of a distinctive feature, the new feature shall match the old in design, color, texture, and other visual qualities and, where possible, materials. Replacement of missing features shall be substantiated by documentary, physical, or pictorial evidence.*
7. **Avoid Harsh Treatments.** *Chemical or physical treatments, such as sand-blasting, that cause damage to historic materials shall not be used. The surface cleaning of structures, if appropriate, shall be undertaken using the gentlest means possible.*
8. **Protect Archaeological Resources.** *Significant archaeological resources affected by a project shall be protected and preserved. If such resources must be disturbed, mitigation measures shall be undertaken.*
9. **Make Compatible Additions.** *New additions, exterior alterations, or related new construction shall not destroy historic materials that characterize the property. The new work shall be differentiated from the old and shall be compatible with the massing, size, scale, and architectural features to protect the historic integrity of the property and its environment.*
10. **Preserve Original Integrity.** *New additions and adjacent or related new construction shall be undertaken in such a manner that, if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired.*

## **2.3 Priority Planning for Historic Buildings**

### **Know what you have**

1. Identify the building type and style, its components, and parts associated with the style. Respect the type and style.
2. Identify the characteristics associated with that style and with the building.

### **Review the work you want or need to do**

1. Will the proposed work impact the appearance of the building?
2. Is the proposed work compatible with the style and character of the building?
3. Will the proposed work take away characteristics that are important to the building?
4. Does the proposed work impact the surrounding buildings?

### **Before You Start, Where To Start?**

1. Address life safety issues first. Then think bottom (foundation), top (roof), then middle (body of building).
2. Evaluate the overall condition of all aspects of the building to determine appropriate priorities for maintenance and other desired work to the building.
3. Prioritize those activities that will extend the life of the building such as repairs to the roof, foundation, window repairs, and repairs to exterior siding. For example, a new coat of paint for the front of the building will not do much to extend the building's life if the roof is leaking badly.

### **Getting To Work**

1. Retain and repair as much of the original building material and detailing as possible.
2. If a historic feature is beyond repair, replace it to match the original in materials and dimensions.
3. Determine the overall quantity of material to be repaired or replaced and plan to repair only that material. If one window is beyond repair, there is no need to replace all windows in the building.
4. If compromises must be made with regard to budget and existing conditions, focus on what will extend the life of the building, look at what is most visible from the street and what has the most impact on the overall streetscape.
5. Contact the city's Historic Preservation Office for help at (405) 366-5392.

### **Look at the glossary for terms that may not be familiar.**

## 2.4 Recommendations for Maintenance of Historic Buildings

All buildings require maintenance. It is generally more cost effective to maintain a historic building and repair limited areas of damage as they occur than it is to defer maintenance and have to wholly replace damaged materials and features.

The following are recommendations for maintaining historic buildings:

1. **Inspect regularly.** Inspect features and surfaces regularly for signs of moisture damage, air infiltration, rust, paint failure, vegetation, structural damage or settlement, corrosion, and fungal or insect infestation.
2. **Cleaning.** Historic buildings should be cleaned using the gentlest means possible, which typically includes water and soft bristle brushes.
3. **Do not pressure wash.** Sandblasting and high-pressure washing can cause irreparable damage to historic building materials and are not advisable.
4. **Chemical cleaners.** Chemical cleaners must be tested in small areas of limited visibility to ensure compatibility and effectiveness on the historic materials.
5. **Drainage.** Regularly clean roof drains, gutters and downspouts of trash and leaves, and inspect for good drainage. Install splash blocks or extenders where necessary for proper drainage away from the building.
6. **Roofs.** Regularly inspect the roof for leaks and patch them immediately. Leaks commonly occur where the roof and wall meet and where roof penetrations are present.
7. **Windows and doors.** Regularly inspect windows and doors and conduct cyclical maintenance. Historic wood windows were constructed so the damaged wood elements could be repaired without requiring that the entire window be replaced. Damaged wood components should be repaired or replaced as appropriate.
8. **Glass.** Any damaged or missing glazing putty should be replaced, and the window should be painted to ensure long term preservation. Wash windows and replace broken or missing glass.
9. **Shutters, canopies, and awnings.** Regularly inspect shutter, canopy and awning attachments and anchors, and replace worn or damaged materials when necessary.
10. **Repainting.** Repaint wood and metal building components to protect them from deterioration.
11. **Signs.** Keep signs freshly painted and securely anchored on commercial buildings.



Maintain historic buildings by repairing limited areas of damage rather than the whole building.



Wood windows should be repaired and not replaced with aluminum frame windows.



Broken glass should be replaced, and features such as canopies should be replaced where they existed originally.



Historic materials should be maintained or replaced in-kind.



## 2.5 Restoring Previously Modified Buildings

A building usually has a time period when it is considered most important, or its “period of significance.” Period of significance is a time when a property is associated with important events, activities, or persons, or other characteristics which qualify it for National Register listing. Period of significance usually begins with the date when important activities or events began giving the property its historic significance; this is often a date of construction. (Source: National Park Service)

Buildings tend to be modified and modernized over time as a way of “keeping up with the times” and through maintaining a building by replacing deteriorated materials. Replacement materials may or may not have been compatible with the original design and, if not, may have negatively impacted the historic appearance of the building. However, some additions and modifications may be historically significant or part of the “period of significance” for a building.

Consider restoring a building to its original appearance when appropriate. This will enhance the building and the surrounding district. Refer to historic photographs to determine the historic appearance of the building. If clear evidence of previous details exists, use these clues to return the building or detail to its original appearance.

Restoration measures should not be undertaken if the historic appearance of the building cannot be determined. Do not create a false history.

### Recommendations

The following restoration measures are recommended for buildings for which appropriate historical documentation exists:

1. Porches are one of the most modified elements of a house. Restore a porch to its original design.
2. Consider raising the porch to its original height if previously covered. Replace the columns if missing or modified. Reconstruct a previously removed porch and restore an enclosed porch.
3. Remove non-historic, synthetic siding that has been applied over the original siding. Siding changes the character of the house and can cause deterioration of any wood siding retained behind the new material. Non-original siding frequently covers original detail.
4. Depending on the condition of the underlying historic material, removal of any non-historic siding may require in-kind replacement of the historic siding.
5. When windows have been removed and replaced with windows of a different material and proportion, consider replacing them with windows to match the original in material, proportion, configuration, and operation.
6. Retain or restore original roof pitch.



Restoration of a commercial building to its original appearance.



Walch-Kirk home in 1912, built in 1903, located at 606 Chautauqua Ave.



Walch-Kirk home today.

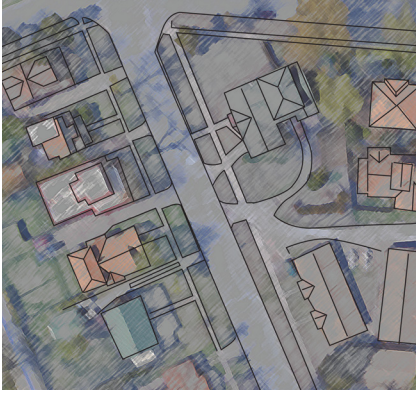
## 2.6 Differences Between the Historic Districts

### Chautauqua Historic District:

- Built between 1903-1940.
- Tree lined neighborhood with stately residences that reflect the status of the university deans and faculty and other prominent individuals who helped shape early development of the city.
- Its development was tied closely to the development of the city.
- Architecturally, Chautauqua is very eclectic. Bungalows are prominently represented, but Tudor Revival and Minimal Traditional are also quite prevalent.
- The district also includes fine examples of Prairie, Colonial Revival, Spanish Eclectic, Neoclassical Revival, and even one example of Queen Anne.
- More than 70% of the houses have paved driveways to the left or right of the house that lead to an outbuilding in the rear of the property.
- Many houses in this district have a shared driveway.
- Very few houses have an attached garage or carport to the side of the house.
- Houses do not have a consistent setback from the street.
- All streets in this district have parkways and sidewalks on both sides of streets, and paved walkways that lead from the sidewalk to the front door.



**Aerial view of a section of Chautauqua Historic District on South Lahoma Avenue and West Boyd Street.**



**Aerial view of a section of Miller Historic District on Miller Avenue and Castro Street.**

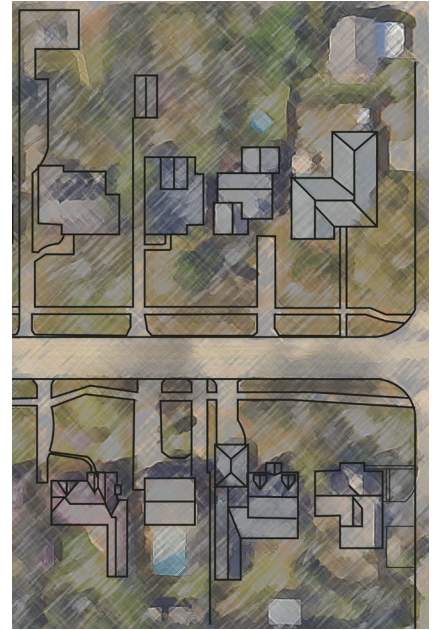
### **Miller Historic District:**

- Built between 1910-1938.
- This district does not have as many trees lining the streets as Chautauqua and Southridge.
- It began to fully develop after WWI as an exclusive neighborhood for university faculty and Norman business leaders.
- Nearly half the structures are classified as Bungalows, but the neighborhood also includes Minimal Traditional, Colonial Revival, National Folk, and Tudor Revival.
- The westernmost blocks of the district parallel the railroad tracks; the remaining blocks follow the cardinal points of the compass.
- About 50% of the houses have paved driveways to the left or right of the house that lead to an outbuilding in the rear of the property.
- Around 20% of the houses have garages attached to the side of the house.
- Only a few houses have carports attached to the side of the house.
- All houses have a consistent setback from the street.
- All streets in this district have sidewalks, parkways on both sides of the streets, and paved walkways that lead from the sidewalk to the front door.



## **Southridge Historic District:**

- Built between 1920-1950.
- Tree lined streets with front yard gardens, located eleven blocks south of downtown district and three blocks east of the university.
- Largest decade of growth was between 1931-1940 with the construction of approximately sixty-seven buildings. The advent of World War II escalated the demand for housing in Norman as military students, frequently with their families, came in droves to attend the Naval Training School and subsequently the Naval Air Station.
- Architecturally, the dominant styles are Tudor Revival, Colonial Revival, and Minimal Traditional.
- About 50% of the houses have paved driveways to the left or right of the house that lead to an outbuilding in the back.
- Around 30% of the houses have an attached garage to the side of the house and few have carports.
- Many houses have semi-circular driveways.
- All houses have a consistent setback from the street.
- The majority of streets have sidewalks and parkways on both sides of the streets.
- All houses have paved walkways that lead from either the sidewalk or the driveway to the front door.



**Aerial view of a section of Southridge  
Historic District on East Boyd street and  
Oklahoma Avenue.**

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## SECTION

# 3

### EXTERIOR FEATURES OF HISTORIC HOUSING

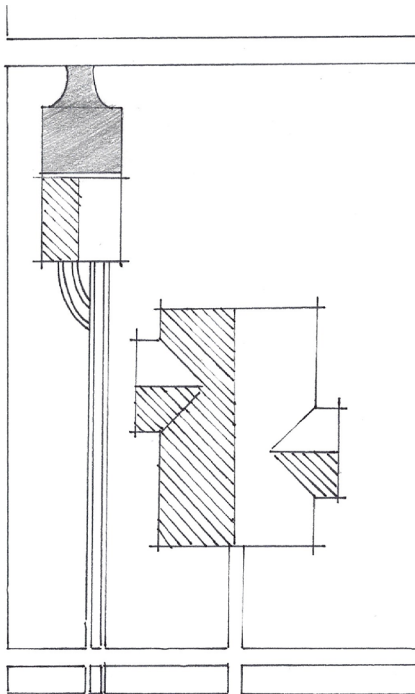
### 3.1 Neighborhood Characteristics & Distinctions

#### Policy

When altering existing site features or proposing new ones, property owners should consider the character, pattern, and rhythm of existing features as well as the dominant pattern within the historic district. Selecting wisely from the existing vocabulary of distinctive site features to define circulation, create site spaces, or otherwise articulate and develop sites within a district is central to preserving the district's overall character. It is also important to consider whether proposed changes will affect neighbors' views or usage of their property.

#### On the Street Where You Live

- The character of Norman's Historic Districts is defined not only by individual buildings and their settings, but also by the network of streets, sidewalks, tree canopies, landscaping, lighting, and alleyways that connect those buildings and sites. The sum of these elements creates the background for the historic residences that line the streets.
- Historic districts are a network of spatial and social relationships: individual buildings relate to their sites, buildings relate to their neighbors, and both relate to the street. In this way, city blocks are linked to each other by a continuous rhythm.
- The setbacks of the houses throughout the neighborhood are consistent for the most part, but they can vary depending on the area of development.
- As changes are proposed to a site or home, review the lines of continuity and rhythm established in the specific neighborhood. Look at the scale, form, and proportion of proposed changes and ensure that the proposed project will retain these characteristics.



Typical for Chautauqua and Miller is a narrow (8') drive along the property line, leading to a small detached garage as shown in the diagram above.

#### Building Form

Building form is primarily dictated by the style of the building. For example, Queen Anne and Victorian styles are recognizable by their composition of multiple shapes which include bays, dramatic roof lines, dormers, and porches, while the Craftsman style is derived from a simplified rectangular plan. The Neoclassical building also derived its form from a rectangular plan but has a dominant central entry porch with columns which extend the full height of the building.

#### Scale

The scale of a building is measured as the relationship of building size to something else, such as a human. Windows, entrances, porches, bays and the dimensions of building materials contribute to the overall scale of the building.

#### Rhythm and Visual Continuity

The rhythm of a street is created by the spacing between houses, the location and spacing of sidewalks from the curb as well as walkways to the entrances of the houses, and the location and spacing of the driveway entrances to each property.

Early 20th century paving patterns in Norman are simple and uniform. Many garages are located very near or on the property line. Fences are generally used to enclose rear yards.

### Proportion

Proportion is the relationship of the dimensions of an object to itself, such as height to width. Proportion is inherent in all aspects of a building form, components, and material. As an example, older homes with high ceilings have windows that are taller than they are wide. Houses after 1960s usually have lower ceiling heights so their windows are shorter and wider.

### Relationship of Materials and Texture

The materials and texture of each home are representative of the style and period of construction. The inherent properties and dimensions of construction materials like brick and wood boards help in understanding the home's size, scale, and proportion. Because stucco has no dimension, it is difficult to measure its relationship to the scale of a building.

### Walls of Continuity or Setbacks

The front of each building, its walls, its porch alignment and fences help to define a wall that establishes a visual pattern along the streetscape. The neighborhood's visual continuity starts at the street, which is basically a straight line of uniform width. Then the front yard is established and sometimes includes a stone wall or a fence. Each of these elements works to organize a neighborhood. These organizational elements, along with orientation and placement of houses on the lot, establish the visual continuity of a neighborhood.

Sidewalks separate a continuous grassy strip from individual front yards. A walkway typically divides the front yard and connects the public sidewalk to the building entrance. A narrow, concrete driveway is usually located near the property line on the side of the residence and it stretches to the rear, typically ending at a one or sometimes two-car garage.



A hedge serves both a landscape and a fencing function, here accentuated by a garden gate of complementary color and design.



This carved wood gate at the Jacobson House complements the house's stucco walls and Italian Renaissance style.





This Craftsman structure has an unusual two-story, overhanging porch.



An arched, recessed entryway is characteristic of Tudor Revival structures.



This Tudor Revival doorway has a patterned, arched, brick entry under a gable roof. Note the matching arched top wooden door.

## 3.2 Entrances, Porches, and Balconies

### History and Development

Entrances themselves draw attention to a front doorway with such features as sidelights, transoms, pilasters, architraves, and pediments. One-story front porches that extend across the full façade supported on masonry piers are common on Norman's early residences. Some front porches wrap around side façades as well. The prominent, character-defining role of front entrances, porches, and balconies for most historic structures makes their preservation of primary importance.

In Norman, most porches are constructed and detailed in wood and include a variety of functional yet decorative features such as columns, pilasters, rails, latticework, balustrades, soffits, steps, brackets, beaded board ceilings, and tongue-and-groove flooring.

### Policy

Original historic porches are character defining features that should be preserved.

### Things to Consider As You Plan

- Entrances, porches, and balconies often weather rapidly from constant exposure to the elements. They require regular inspection for signs of deterioration due to moisture damage, fungal or insect infestation, or structural settlement. Keeping gutters and downspouts maintained and ensuring that all flooring slopes away from the building for proper drainage will help protect entrances and porches from moisture damage.
- Routine maintenance of wooden features includes caulking joints to prevent water or air penetration and repainting as necessary to maintain a sound, protective paint film. The repair of traditional entrance and porch materials, such as wood, masonry, and architectural metals, is addressed in the pertinent guidelines.
- Entrances and front porches often distinguish the street façades of historic buildings and provide highly visible opportunities for stylistic embellishments. Sleeping porches, balconies, side porches, mud-rooms, back porches, and rear entries offer additional outdoor access and living space.

**Match Original Details.** When entrance, porch, or balcony features and details are deteriorated and require replacement, it is important to match the original features and details in design, dimension, detail, texture, material, and color. Similarly, should an entire entrance or porch be deteriorated or damaged beyond repair, the property owner should match the original entrance or porch. The design of a new entrance, porch, or balcony for one that is lost should be an accurate reproduction of the original or a design that is compatible with the historic character of the building and its site. Compatibility of a new design should be reviewed in terms of proportion, height, roof shape, material, scale, texture, detail, and color.

The introduction of a new entrance, porch, or balcony on a secondary façade may be appropriate if it does not diminish the building's architectural character and the design is compatible with the building and the site. Occasionally, the enclosure of a side or rear porch may be considered to accommodate a change in use or a need for space. Given the prominence of the front façade, the enclosure of a front entrance, porch, or balcony is not considered appropriate. However, the sensitively designed enclosure of a side or rear porch may be appropriate if the building's architectural integrity is not compromised and the character of the porch is retained.

### Ordinary Maintenance

The following are suggestions for maintaining historic porches and entrances:

- Protect and maintain original wood, masonry, and metal elements of entrances, porches, and balconies through appropriate surface treatments.
- Inspect regularly for signs of moisture damage, rust, structural damage or settlement, and fungal or insect infestation.
- Provide adequate drainage to prevent water from standing on flat, horizontal surfaces and collecting on decorative elements or along foundations.
- Clean soiled surfaces using the gentlest means possible.
- Recaulk wooden joints properly to prevent moisture penetration and air infiltration.
- Retain protective surface coatings, such as paint or stain, to prevent damage from ultraviolet light or moisture.
- Reapply protective coatings, such as paint or stain, when they are damaged or deteriorated.

### 3.2.1 Standards for Administrative Bypass:

*The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, then the application will be forwarded to the Historic District Commission for a full review.*

- *Front or rear porch screening that is temporary and easily reversible, and can be designed to preserve the historic character of the porch and the building.*
- *Decks and porches that are built on rear and not visible from front.*
- *Handrails required by code may be approvable by administrative bypass but must meet City Standards.*



On this Prairie style structure, a partial front porch extends into a porte cochere, meaning literally “a carriage door.”



This Prairie-style structure has a full-width front porch.



This Craftsman bungalow has a partial front porch with massive brick piers topped by short, elephantine columns.



This Colonial Revival structure has a wrap-around porch with turned balustrades.



### 3.2.2 Guidelines for Entrances, Porches, and Balconies

*A full review by the Historic District Commission will take the following criteria into consideration to be issued a Certificate of Appropriateness (COA):*



A side porch is a less prominent feature but still contributes to overall building design.



This Italian Renaissance structure has a partially enclosed porch with massive masonry columns.

- .1 Preserve Original Entrances, Porches and Balconies.** *Retain and preserve entrances, porches, and balconies that contribute to the overall historic character of a building, including columns, pilasters, piers, entablatures, balustrades, sidelights, fanlights, transoms, steps, railings, floors, and ceilings.*
- .2 Replace Only Deteriorated Elements.** *If replacement of a deteriorated detail or element of an entrance, porch, or balcony feature is necessary, replace only the deteriorated detail or element in kind rather than the entire feature. Match the original in design, dimension, and material. Consider compatible substitute materials only if using the original material is not technically feasible.*
- .3 Match Originals.** *If full replacement of an entrance, porch, or balcony is necessary, replace it in kind, matching the original in design, dimension, detail, texture, and material. Consider compatible substitute materials only if using the original material is not technically feasible.*
- .4 Replace Missing Features.** *Replace missing entrance, porch, or balcony features with a new feature based on accurate documentation of the missing original or a new design compatible with the historic character of the building and the district.*
- .5 Avoid Enclosures.** *It is not appropriate to enclose a front porch or a front balcony to provide more living space as this dramatically alters the appearance of the house. Rear porches may be screened with a COA through administrative bypass.*
- .6 Avoid Removing Details.** *It is not appropriate to remove any detail material associated with entrances and porches, such as graining, beveled glass, or beaded board, unless an accurate restoration requires it.*
- .7 Avoid Changes to Primary Façades.** *It is not appropriate to remove an original entrance or porch or to add a new entrance or porch on a primary façade. Alterations to secondary façades may be considered where not highly visible from the street and when no character-defining features are destroyed for its creation.*
- .8 Avoid False Historical Appearances.** *Features or details that are introduced to a house should reflect its style, period, and design. Features should not create a false historical appearance by reflecting other time periods, styles, or geographic regions of the country.*
- .9 Porch Elevation.** *At no time should the porch elevation be lowered to grade and steps redesigned.*
- .10 Wood Elements.** *Wood porch floors and columns may require an eventual replacement due to moisture penetration; wood floors and columns should only be replaced with wood of the same profile and dimension.*
- .11 Original Porch Floors.** *Do not cover original porch floors with paint, stain, or other permanently affixed materials.*

- .12 New Balconies.** *Balconies, as with other alterations must be compatible with the style of the house and must not be visible from the street.*
- .13 Tile.** *Original design, construction, and materials should be respected on primary façades. Installation of non-original materials, such as decorative tile, is not appropriate.*
- .14 Wood Decking Over Concrete.** *If concrete or brick has been installed, a wood porch may be installed over the concrete if it was the material of the original construction. Wood can be installed over sleepers which would allow it to be elevated and breathe.*



Because of their visibility, decks on corner properties should be designed sensitively to least detract from the original structure.



Alternative building materials are possible when decks are installed in an inconspicuous location such as the rear yard of an interior lot.



Covered decks are essentially house additions and will be reviewed for their overall impact on the original structure.

### 3.3 Decks

#### Policy

A deck should be compatible with but differentiated from the building. It should be constructed to be structurally independent so that it could be removed in the future without damage to the building. A deck should never be so large that it overpowers the building or the site and should have limited visibility from front.

#### Things to Consider As You Plan

- The outdoor deck is a temporary, exterior feature frequently introduced into residential historic districts.
- To maintain a building's historic character, deck additions are generally located unobtrusively on the rear elevation.
- Decks are usually built on posts to align at or below the first-floor level of a residence.

**Deck Locations.** In locating a deck, property owners should always consider the proposed location's impact on the historic structure, the site, and the district. Locations that are visible from the street, with corner properties as an exception, or locations that would damage or diminish significant architectural elements or significant site features such as mature trees are generally not recommended.

Wood is the most common decking material, however new alternative materials may be utilized. Alternative composite materials must be similar in dimensions and details as wood counterparts.

**Protective Treatments.** Because decks are exposed to the elements, decay-resistant woods or pressure-treated lumber should be used. Staining or painting are strongly recommended to protect decks from water and sunlight and to make them more compatible with the colors of the historic structure. Some pressure-treated lumber may require six to twelve months of weathering before primer and paint will bond well to it. Opaque stains are a good option for exposed decks since they do not peel; stains are not an applied film like paint, but rather are a protective treatment that is absorbed into the wood surface. Use appropriate nails and fasteners in deck construction to avoid rust stains or chemical reactions. Some decks may require railings to comply with local building codes. City staff can assist with compliance.

**Screening.** To relate a deck visually to a historic building, the structural framing should be screened with traditional materials such as skirtboards, lattice, or dense evergreen plantings. Because a deck is a contemporary feature, detailing it to duplicate the architectural detailing of the historic building is discouraged. Deck elements that reflect the materials and the proportions of the building and the district are most appropriate.

### 3.3.1 Standards for Administrative Bypass:

*The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, then the application will be forwarded to the Historic District Commission for a full review.*

- Deck is less than 300 square feet in total area.
- Deck is not visible from the street.
- Deck makes no permanent changes to the historic structure.
- Deck meets the city's coverage restrictions.
- Alternative material acceptable.

*Decks that do not meet all of these criteria must be reviewed by the Historic District Commission.*

### 3.3.2 Guidelines for Decks

*A full review by the Historic District Commission will take the following criteria into consideration to be issued a Certificate of Appropriateness (COA):*

- .1 Protect Historic Fabric of Structure.** *Locate and construct decks so that the historic fabric of the primary structure and its character-defining features and details are not damaged or obscured. Install decks so that they are structurally self-supporting and may be removed in the future without damage to the historic structure.*
- .2 Choose Inconspicuous Locations.** *Decks were not used prior to 1950 on Norman's older homes and as such are prohibited additions on front façades. Introduce decks in inconspicuous locations, usually on the building's rear or side elevations and inset from its rear corners, where the deck will not be visible from the street. A deck proposed for the side of a building must not detract from the design of the house, and must be completely reversible. Decks on corner properties will be reviewed on a case-by-case basis.*
- .3 Deck Design Should Reflect Building Design.** *Design decks and their associated railings and steps to reflect the materials, scale, and proportions of the building.*
- .4 Design Visible Decks Carefully.** *Where it is appropriate to site a deck in a location visible from the street (i.e. the side of a building), treat the deck in a more formal architectural way.*
- .5 Align Deck with First-Floor Level.** *Decks shall generally be no higher than the building's first-floor level. Visually tie the deck to the building by screening with compatible foundation materials such as skirtboards, lattice, or dense evergreen foundation plantings.*
- .6 Preserve Significant Building Elements.** *It is not appropriate to introduce a deck if doing so will require removal of a significant building element or site feature.*

- .7 **Do Not Detract from Overall Character.** *It is not appropriate to introduce a deck if the deck will detract from the overall historic character of the building or the site.*
- .8 **Inset Decks.** *Insetting a deck at least six inches from a building corner is required to help diminish its impact and differentiate it from the existing building.*



## 3.4 Exterior Walls

### History and Development

Within Norman's Historic Districts, exterior walls clad in horizontal, lapped wooden siding are most typical, although walls surfaced with wooden shingles, brick, stone, or stucco are found as well. Combinations of materials, including brick with stone details or lapped siding with wooden shingles are also found.

The foundations of early Norman buildings are differentiated from the rest of the wall by a change in material, plane, and/or color. Brick foundations are the most common, but foundations of stone or masonry with stucco are not unusual. Some masonry pier foundations with infill panels of recessed brick or lattice are also found in the districts.

### Policy

Through their shape, features, materials, details, and finishes, exterior walls contribute to the form and the character of historic buildings. They also provide opportunities for stylistic detailing and ornamentation. Features such as projecting bays, dormers, sun porches, and chimneys boldly manipulate the shapes of exterior walls. In addition, columns, braces and brackets, and window openings all embellish the connections between wall planes or link exterior walls to other building elements. Variations in exterior wall materials all contribute to the pattern, texture, scale, color, and finish of the building exterior and are major character defining features of historic housing that should be preserved.

### Things to Consider As You Plan

- Routine inspection, maintenance, and repair of exterior walls should follow the guidelines for the specific wall materials. Each exterior wall surface material requires different maintenance which can be referenced in the building materials section of this document.

**Preserve Original Details and Materials.** Replacement of deteriorated exterior wall materials and details requires careful attention to the scale, texture, pattern, and detail of the original material. The three-dimensionality of wood moldings and trim, the distinctive texture of weatherboards, and the bonding pattern of masonry walls are all important to duplicate when replacement is necessary. Generally, replacement or concealment of exterior wall materials with substitute materials is not appropriate. For example, the application of synthetic sidings or contemporary stucco-like materials in place of the original materials results in a loss of original fabric, texture, and detail. In addition, such surfaces may conceal moisture damage or other causes of structural deterioration from view.

The loss of a distinctive exterior wall feature such as a projecting chimney or window bay would compromise the character of a historic building. Similarly, the introduction of a new feature, such as a window or door opening, can also compromise the integrity of the original wall. Alterations such as these require a clear understanding of the significant characteristics of the original wall and also the wall's role in creating the



Stucco walls and porch piers give this Mission Revival style Bungalow its distinctive character and should be preserved.



Stucco walls and a flat roof define this Spanish Revival style house in the Chautauqua District.



Masonry details such as bond pattern and copings articulate style and design.



Craftsman-style structures often use multiple materials for exterior walls. This house has a combination of stucco and wood shingles.



Wood siding, also known as weatherboard, is very typical on historic structures in Norman.

building's significance. Using that knowledge, a compatible change that will not diminish the building's architectural character may be developed.

### Ordinary Maintenance

- While houses with existing synthetic siding installed are not required to remove the siding and restore the exterior, removal of synthetic siding and repairing of original siding and trim are encouraged.
- Protect and maintain the original material surfaces, details, and features of exterior walls through appropriate methods.
- Inspect regularly for signs of moisture damage, vegetation, fungal or insect infestation, corrosion, and structural damage or settlement.
- Provide adequate drainage to prevent water from standing on horizontal surfaces and collecting on decorative elements or along foundations.
- Clean exterior walls as necessary to remove heavy soiling or to prepare for repainting. Use the gentlest methods possible.
- Retain protective surface coatings, such as paint or stain, to prevent deterioration.
- Reapply protective surface coatings, such as paint or stain, when they are damaged or deteriorated.
- Use recognized preservation methods. Repair exterior wall surfaces, details, and features using recognized preservation repair methods for the surface material or coating.
- Painting of wood is considered maintenance and allowed without review.
- No painting of brick/masonry.

### 3.4.1 Standards for Administrative Bypass

*The following item can receive a Certificate of Appropriateness (COA) through the Administrative Bypass:*

- *Removal of existing synthetic materials to reveal existing historic materials.*

### 3.4.2 Guidelines for Exterior Walls

*A full review by the Historic District Commission will take the following criteria into consideration to be issued a Certificate of Appropriateness (COA):*

- .1 Preserve Original Walls.** *Retain and preserve exterior walls that contribute to the overall historic form and character of a building, including functional and decorative features and details.*
- .2 Retain Original Building Materials.** *Retain and preserve exterior wall materials that contribute to the overall historic character of a building. It is important to retain the original siding and trim and its dimension, profile, and shadow lines.*



- .3 Replace Only Deteriorated Portions.** *If replacement of a deteriorated wall or feature is necessary, replace only the deteriorated portion in kind rather than the entire feature. Match the original in material, design, dimension, detail, texture, and pattern. Consider compatible substitute materials only if using the original material is not technically feasible. If the building was constructed of wood siding and needs repairs or board replacement, most siding types are still manufactured and available from suppliers or can be milled for a nominal fee.*
- .4 Avoid Covering Original Materials.** *Building materials and decorative elements are important character-defining components of historic buildings. It is not appropriate to remove or cover any wall material or detail with coatings or contemporary substitute materials. Vinyl and aluminum siding is not appropriate for use on historic structures. Hardieboard might be appropriate for accessory structures or garages.*
- .5 Replace Missing Features.** *When replacing an exterior wall or feature, replace it with a new wall or feature based on accurate documentation of the original or a new design that is compatible with the historic character of the building and the district. Consider compatible substitute materials only if using the original material is not technically feasible.*
- .6 Avoid False Historical Appearances.** *Features or details that are introduced to a house should reflect its style, period, and design. Features should not create a false historical appearance by reflecting other time periods, styles, or geographic regions of the country.*
- .7 Substitute Materials.** *Cement fiberboard (e.g. Hardieplank siding) will be considered on a case-by-case basis. It is considered appropriate for new construction but not for replacement on historic structures unless original material is not feasible. Exterior insulating and finish systems (EIFS) will not be considered for use in historic structures.*
- .8 Other Materials.** *Do not remove original siding and replace with T-111 plywood/OSB or other synthetic materials. T-111 is a plywood siding re-tailed in the form of sheets, with grooves or channels cut into it.*



This Colonial Revival structure has arched wood windows that echo the arched brick entryway.



Window pairings on this Colonial Revival structure give rhythm to the overall house design.



A mixture of hung and casement windows characterize this unusual Italian Renaissance structure in the Chautauqua District.



Original window placement often maximizes passive solar as in this south-facing elevation with three pairs of windows.

## 3.5 Windows and Doors

### History and Development

Although many window types are found in early Norman houses, the vast majority are wooden, double-hung windows with rope-and-pulley systems and set into wall framing. Depending on the style and the age of the house, each sash may be divided by muntins that hold individual panes of glass in place. “One-over-one” window configurations are common, as is a pattern of four vertical panes over one single pane. Other common window configurations are “six-over-six” or “nine-over-nine,” though many other configurations are seen.

More contemporary housing styles, such as Mid-Century Modern, were built with steel-framed casement and picture windows. These character-defining features should be maintained and retained.

Doors with a variety of glazing configurations, as well as a combination of solid panels and glazing with sidelights or transoms are found throughout Norman’s Historic Districts.

### Policy

Windows and doors are among the most character-defining features of historic buildings; therefore, their preservation is one of the highest priorities in historic rehabilitations. The various arrangements of windows and doors — their proportion, shape, positioning, pattern, size and the decorative elements associated with them — are used to achieve specific architectural effects on buildings. The retention and repair of original wood doors and windows is strongly encouraged.

### Things to Consider As You Plan

- Improper or insensitive treatment of the windows and doors of a historic building can seriously detract from the architectural character. Original windows are nearly always constructed from higher quality lumber — in most cases, old growth timber — than any replacement window available today. In most cases, repairing original windows and doors in an older building is more appropriate and cost-effective in the long-term than replacing them with new units. Peeling paint, high air infiltration, sticking sash, or broken panes are all very repairable conditions and do not necessitate replacement.
- Replacement window and door units should fill the original opening. They may need to be custom-made. Today’s open-stock windows and doors may or may not match the dimensions of the existing opening. Custom-made wooden window sash that match many original windows are available at some lumber yards and some manufacturers. (See the City of Norman Historic Preservation Officer for a list of suppliers.)
- Changing existing window and door openings, closing existing openings, or adding new openings on an early Norman house should be very carefully considered and undertaken only for compelling reasons. Changes to original openings in a character-defining façade should

never be considered. For less significant façades the pattern of proposed openings should be characteristic of and complementary to the historic building and the historic district context. Generally, rear elevations or elevations not seen from the street allow more flexibility and change.

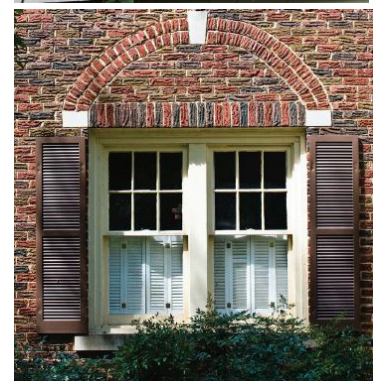
**Storm Windows and Doors.** Choose storm doors constructed of wood or metal that do not obscure or damage the existing door and frame. Storm windows and doors with painted, stained, or baked-enamel finish color are highly recommended.

**Details Are Important.** Windows in early Norman houses are often set into relatively deep openings or have surrounding casings and substantial sash components that cast shadows which help define the architectural style. Consequently, preserving original window glazing — including the preservation of original glass — is always desirable. If the details of a window or a door, such as casing or muntins, are deteriorated, they may be replaced in-kind and match the existing.

**Doors.** If replacement is necessary, the design of replacement doors must reflect the style and period of the building. Wood doors are required unless there is documentation that other materials were historically used on a particular structure. Wood doors that are repaired and properly maintained will have greatly extended service lives while also contributing to the historic character of the house.

#### Routine Maintenance of windows and doors

- Protect and maintain the wood and metal elements of historic windows and doors through appropriate methods.
- For steel-framed windows it is important to remove rust and repaint, replace missing screws or fasteners, and clean and lubricate hinges.
- Inspect regularly for deterioration, moisture damage, air infiltration, paint failure, and corrosion.
- Clean the surface using the gentlest means possible.
- Limit paint removal and reapply protective coatings, as necessary.
- Reglaze sash as necessary to prevent moisture infiltration.
- Recaulk and weatherstrip windows and doors to reduce air infiltration and increase energy efficiency.
- Repair historic windows and doors and their distinctive features through recognized preservation methods for rebuilding, patching, consolidating, splicing, and reinforcing.
- If an original window or door opening has been blocked, consider re-opening it and installing a historically compatible window or door.
- If original screen doors and windows are removed to allow the installation of storm doors and windows, it is strongly encouraged that these be retained for possible future use.
- Before bare aluminum storm sash is painted, it should always be primed with a zinc chromate primer to ensure that the finish paint will bond.
- Reduce airflow at the bottom of the door by installing a door sweep to fit snugly against the threshold. Install weather stripping for energy efficiency.



Different window pairings and with a variety of muntin patterns found in Norman's Historic Districts.





A six-panel door with sidelights and a fan light with tracery typifies Colonial Revival style.

#### Are Old Windows The Problem?

Infiltration of outside air — rather than heat lost through the glass — is the principal culprit affecting energy, accounting for up to 50 percent of the total heat loss in a building. Sash pockets, pulleys, and meeting rails areas are also prone to air infiltration in double-hung units. The energy efficiency of restored windows incorporating retrofit components (weatherstripping and weatherseals combining pile, brush, bulb, or spring seals) can meet and even exceed the efficiency of replacement units. In addition to evaluating windows for energy efficiency, property owners should strongly consider adding insulation R-Value to walls and ceilings. Storm windows can also be very helpful in reducing air infiltration. Source: "What Replacement Windows Can't Replace: The Real Cost of Removing Historic Windows" by Walter Sedovic and Jill H. Gotthelf, APT Bulletin: Journal of Preservation Technology / 36:4, 2005.

### 3.5.1 Standards for Administrative Bypass for Windows and Doors:

*The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, then the application will be forwarded to the Historic District Commission for a full review.*

- **Window Replacement by Administrative Bypass.** *A deteriorated window that is not repairable may be replaced if it meets the following criteria:*
  - \* *Like-for-like, meaning a wood window which matches the original identically.*
  - \* *Muntin width and profile are same as the original in width and profile.*
  - \* *Light pattern is the same as the original.*
  - \* *True divided lights (panes) are the same as the original glass thickness.*
  - \* *Size and dimension of all window components are the same as the original.*
- **Door Replacement by Administrative Bypass.** *A deteriorated door that is not repairable may be replaced with a like-for-like, meaning a door that matches the original in materials and design. A non-original door may be replaced with a historic door.*
- **Screen Door Replacement.** *Screen doors should be retained and repaired when necessary. Any replacement screen door should match the historic screen door and should be built to mirror the panels and sash divisions of the door that it covers.*
- **Awnings.** *Fabric window awnings that conform to historic material, style, shape, and location may be approved by Administrative Bypass. Install fabric awnings over windows, doors, storefronts, or porch openings with care to ensure that historic features are not damaged or obscured.*
- **Storm Doors and Screens.** *Wood framed, full-light storms and screens can be approved through Administrative Bypass. Choose storm doors constructed of wood or metal that do not obscure or damage the existing door and frame. Storm doors with painted, stained, or baked-enamel finish color compatible with the color of the existing door are highly recommended. If storm and screen doors are installed where none existed originally, select a "full vision panel" design to allow the original door to be seen. (Additional information on storm windows and doors is provided in Section 4.9, Utilities and Energy Retrofit).*
- **Storm Windows and Screens.** *Wood framed, full-light storms and screens can be approved through Administrative Bypass. The use of interior storm windows is encouraged. Relatively unobtrusive, narrow-profile, exterior storm windows that do not obscure the window itself, that are carefully installed to prevent damage to the sill or the frame, and that are finished in a painted or a baked-enamel color compatible with the sash color are fairly common in the historic districts. Select storm units that align with the meeting rails of the window.*

### 3.5.2 General Guidelines

*A full review by the Historic District Commission will take the following criteria into consideration to be issued a Certificate of Appropriateness (COA):*

- .1 Retain Original Windows.** Retain and preserve original windows, including glass, frames, sash, muntins, sills, heads, moldings, surrounds, and hardware.
- .2 Retain Original Doors.** Retain and preserve original doors and door surrounds including frames, glazing, panels, sidelights, fanlights, surrounds, thresholds, and hardware on front doors and side doors visible from the street.
- .3 Preserve Original Openings.** Do not create new openings in the front or side façades of historic structures. Do not enlarge or diminish existing openings to fit stock window and door sizes. If new openings are necessary to meet code requirements, they shall be compatible with historic windows for that structure in proportion, shape, location, pattern, size, materials, and details.
- .4 Aluminum.** For original wood windows, mill finished aluminum should be avoided even in the installation of windows' screens and storm windows. Avoid the use of bright aluminum screen fabric or dark solar screen fabric. Woven copper screen wire or charcoal aluminum fabric is appropriate.

### 3.5.3 Guidelines for Windows

*A full review by the Historic District Commission will take the following criteria into consideration to be issued a Certificate of Appropriateness (COA):*

- .1 Retain Historic Glass.** Retain original glass in historic windows if at all possible. Leaded glass windows shall be preserved. Bubbles and waves give old glass its distinctive look and add to the historic character of the house.
- .2 Replace Only Deteriorated Features.** If replacement of a deteriorated window feature or detail is necessary, replace only the deteriorated feature in kind rather than the entire unit. Match the original in design, dimension, placement, and material.
- .3 Window Replacement.** Replacement sash, often referred to as sash replacement kits, are acceptable for use in historic structures. However, replacement window sash shall be unclad wood, with single-pane thickness, true divided light patterns that match the historic muntin pattern and profile of the house.
- .4 Replacement.** A deteriorated wood window that is not repairable may be replaced if it meets the following:
  - \* Shall have a wood exterior, unless replacing a metal casement window
  - \* Aluminum or vinyl cladding is not appropriate
  - \* Light patterns same as the original
  - \* Size and dimension the same as the original



Pairs of windows are common decorative features of many early 20th Century houses.



Triple windows are also common especially on front and southern elevations.



Four-over-one vertical pane configurations are characteristic of Bungalow architecture.

**Save Those Old Windows**

Few changes can have a greater impact on a historic structure than replacing its doors and windows. In most cases, old windows are absolutely repairable! Common complaints such as broken panes and sash cords, rotten muntins, and windows that are painted shut do not mean the entire window unit must be replaced! Hold onto your historic windows if at all possible — they are what make your house unique.

**Don't Grow 'em Like That Anymore!**

Did you know that the wood used in historic windows is a far better insulator than even the most expensive replacement units made today? This is because most historic windows were constructed using old growth timber. At 25 growth rings or more per inch, the tight grain of this wood is far superior to the 3-4 growth rings found in modern lumber.

*\* Double-pane simulated divided lights with wood muntins on the exterior and interior and a shadow bar between the panes may be allowed for windows on the side or rear that are not visible from the street.*

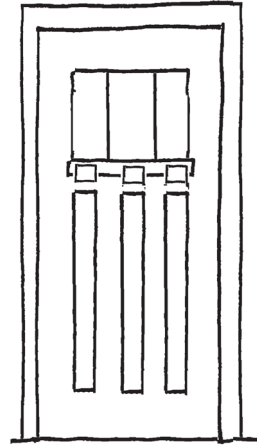
- .5 Retain Original Metal Windows.** *Replace original metal casement windows only when deteriorated beyond repair. Replace with either metal, aluminum clad or fiberglass.*
- .6 Locate Privacy Glass in Rear.** *Privacy glass may be installed if deemed necessary (such as in a bathroom) and can only be located in the rear of the structure, where not visible from the front. Smoked or tinted glass is not appropriate for use in historic structures.*
- .7 Beveled Glass.** *Could be acceptable on doors and windows as long as it is compatible with style of the historic building and the original configuration remains.*
- .8 Windows in New Construction.** *Windows in new construction should be similar to windows in adjacent historic structures in terms of size, profile and material. If the windows in the historic building are wood, then new windows must be wood, or aluminum-clad wood, or fiberglass may be used.*
- .9 Colored Glass.** *Colored glass may be used in transoms and sidelights if supported by historical documentation or compatible with the architectural style.*
- .10 Windows in Primary Structures and Additions.** *For construction of new primary structures, choose windows that complement window types in surrounding structures in material, placement, size, shape, and design. While single-pane, true divided light, wood frame windows are the most desirable choice for new construction in historic districts, double-pane glass wood windows with interior and exterior applied muntins and shadow bars between the panes are permitted. Aluminum cladding of wooden windows is permissible for use in construction of new primary structures and additions. Vinyl cladding of wood windows is not appropriate.*
- .11 Security Bars.** *A Certificate of Appropriateness is required for the installation of burglar bars within historic districts and are generally discouraged. If deemed necessary security bars should be designed to complement the style and design characteristics of the structure to which they are being attached or should be located inside the window if possible.*
- .12 Shutters.** *Shutters may be installed if they are in keeping with the style of the house and period of construction. Shutters need to be correctly proportioned to the width and height of the window and be installed with hinges rather than fixed to the wall.*



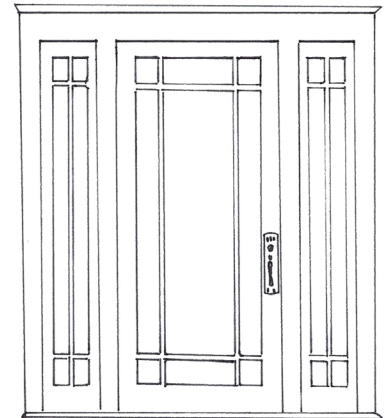
### 3.6 Guidelines for Doors

*A full review by the Historic District Commission will take the following criteria into consideration to be issued a Certificate of Appropriateness (COA):*

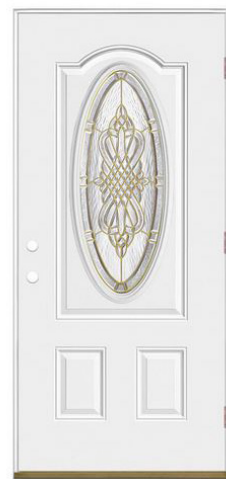
- .1 Replace Only Deteriorated Features.** *If replacement of a deteriorated door feature or details is necessary, replace only the deteriorated feature in kind rather than the entire unit.*
- .2 Repair Damaged Transoms and Sidelights.** *Avoid altering transoms and sidelights as it distorts the strong vertical proportions of the windows and doors and changes the character of the residence.*
- .3 Wood Doors.** *Wood doors are required unless there is documentation that other materials were historically used on a particular structure. Keep wood doors appropriately stained or painted to protect from weather.*
- .4 Replacement Doors.** *Replacement doors and door surrounds shall be appropriate to the style of the structure. Doors shall be relocated, enlarged, or introduced only when the alteration is appropriate to the style of the building.*
- .5 New Doors.** *New doors should be in keeping with the style of the house. Installation of metal doors is not appropriate.*



A three-panel, three-light door with dentil molding is found in many Craftsman houses.



This Craftsman door is flanked by sidelights which, together with decorative muntins, add both light and visual interest to the doorway.



Pressed metal doors are not an appropriate addition to any historic structure.





Chimneys are most commonly located on a side elevation of a structure.



Chimneys are most commonly located on a side elevation of a structure.



A typical bungalow chimney with a chimney cap.

## 3.7 Roofs

### History and Development

Roof form and pitch are among the major distinguishing characteristics of historic buildings. Roofs can be flat, pitched, hipped, curved, or arranged in various combinations of these forms.

Architectural styles are clearly distinguished by roof types, e.g. Craftsman Bungalows usually have deeply overhanging eaves with a generously pitched roof. Tudor Revival structures often have steeply pitched roofs, almost like a capital “A”. Roofing materials also contribute to the character of historic buildings. Depending on the age and the style of the building, the original roofing may have been any of a variety of materials, including wood or metal shingles, slate, clay tiles, and slate-like composite roofing materials. Asphalt and asbestos shingles became popular roofing materials in the 20th century both for new construction and for re-roofing of earlier buildings. Historic roofing materials were usually dark in color.

Architectural metals are often used for roofing and guttering applications including flashings, gutters, downspouts, finials, cornices, copings, crestings, and sometimes for the primary roofing material (i.e. a metal roof).

### Policy

Maintain original roof patterns. It is particularly important to retain and preserve historic roofs that create distinctive effects by shapes or color; to alter or remove them would result in the loss of a significant architectural feature.

### Things to Consider As You Plan

- If a roofing material must be replaced and is not readily available, a property owner should identify a substitute material that closely resembles the original.
- When a roofing material is clearly distinctive to a building’s architectural style, retaining or replacing it in kind is important. For example, a Mission-style building that features a clay tile roof should not be reroofed with fiberglass shingles. This principle applies to shingle patterns as well. Changes in shingle patterns would compromise the building’s architectural character.
- Because contemporary roof features such as skylights and solar collectors often compromise the character of a building and damage historic roof features and materials, they are generally discouraged. If they are proposed, it is important to ensure that they will not damage or diminish the historic character of the building or the district.
- Dormers are common and are found in a variety of shapes and sizes, some have windows while others have vents. Dormers and other historic roof details such as weathervanes add to the character of the house and the neighborhood.

### Ordinary Maintenance

Routine care and maintenance of a roof are critical. A leaky roof allows water damage to the structure and detail elements of a building. It is wise to keep a roof free of leaves and other debris and to inspect it regularly for leaks, loose or damaged shingles, slates, or tiles and repair them immediately. Slate and clay tiles are extremely durable but brittle. They can last more than a century, but their fasteners, flashing, and sheathing may not. However, if they are carefully reset, they may last another lifetime. It is not appropriate to cover shingles, tiles, or valleys with roofing tar in an attempt to stop roof leaks. Gutters, scuppers, and downspouts must be cleaned out often and kept in good repair if they are successfully to carry water off the roof.

Distinctive built-in gutters incorporated into the roof and concealed from view within a boxed cornice are important to retain. However, they must be kept properly functioning to avoid undetected damage to the structure. The distinctive shape of half-round gutters is typical for exposed gutters and preserves cornice crown molding.

### The following are suggestions for roof maintenance:

- Inspect regularly for signs of deterioration and moisture penetration.
- Clean gutters and downspouts to ensure proper drainage.
- Replace deteriorated flashing, as necessary.
- Reapply appropriate protective coatings to metal roofs, as necessary.
- Maintain adequate ventilation of roof sheathing to prevent moisture damage.
- Ensure that roofing materials are adequately anchored to resist wind and water.
- Re-fasten loose (or replace damaged) shingles, slates, or tiles.
- Composition shingles should not be installed on a low-slope pitch roof because they will leak.
- Roofing or re-roofing of any structure with materials that are similar in appearance, regardless of color, provided the building is not structurally altered during the roofing or re-roofing process.



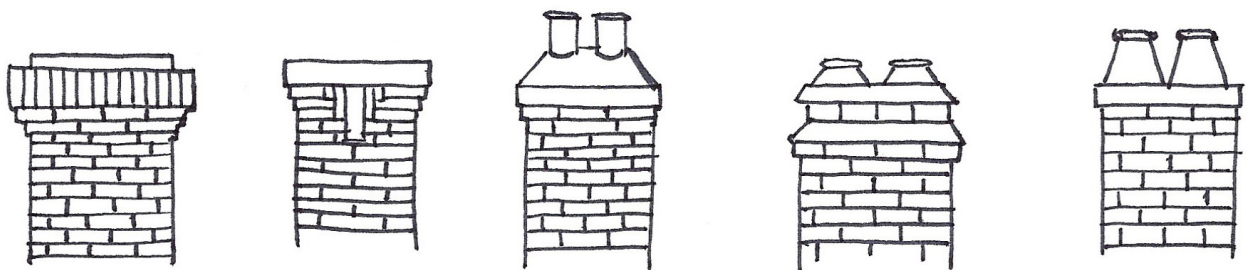
This Mission Revival structure in the Miller District includes a front elevation chimney, a detail borrowed from Tudor Revival style.



Tudor Revival structures often have steeply pitched roofs and front elevation chimneys.



This Tudor Revival structure takes advantage of its corner location to show off a steeply pitched cross gabled roof to full effect.



There are many chimney top forms found in Norman Historic Districts; some are brick, some have pots, some have cast concrete tops.





A symmetrical, hipped roof is common on Prairie style structures.



The rear of this Craftsman shows how many roof planes can be used in a single structure.



Multiple, low-slung roof planes are very characteristic of Craftsman structures.



One of the oldest houses in the Chautauqua District, this Dutch Colonial Revival structure with a gambrel roof was built around 1903.

### 3.7.1 Standards For Administrative Bypass

*The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, then the application will be forwarded to the Historic District Commission for a full review.*

- Reroofing with in-kind materials with no change to the shape, pitch, or structure of the roof.
- Maintenance and installation of gutters.
- New roof features such as dormers, skylights, and solar tubes, and equipment such as power ventilators, solar collectors, photovoltaics, and antennae are approved by administrative bypass when located on rear.

### 3.7.2 Guidelines for Roofs

*A full review by the Historic District Commission will take the following criteria into consideration to be issued a Certificate of Appropriateness (COA):*

- 1 Preserve Original Features.** Retain and preserve roofs and roof features that contribute to the overall historic character of a building, such as cresting, dormers, cupolas, and cornices. Tile and slate roofs rarely need to be discarded.
- 2 Replace Only Deteriorated Portions of Roof Features.** If replacement of a deteriorated roof feature is necessary, replace only the deteriorated portion in kind to match the original feature in design, dimension, detail, and material. Consider compatible substitute materials only if using the original material is not technically feasible.
- 3 Replacements Match Original.** If full replacement of historic roofing material or feature is necessary, replace it in kind, matching the original in scale, detail, pattern, design, and material. If using the original material is not technically feasible, substitute materials can be considered by Historic District Commission review.
- 4 Replace Missing Features.** Replace missing roof features based on accurate documentation of the missing original or a new design compatible in scale, size, and material with the style, period, and design of the historic building and the district as a whole.
- 5 Built-In Gutters.** It is not appropriate to replace concealed, built-in gutter systems with exposed gutters.
- 6 Locate New Features and Mechanical Equipment Carefully.** Adding new features or equipment on a roof requires a Certificate Of Appropriateness. New roof features such as dormers, skylights, and solar tubes, and equipment such as power ventilators, solar collectors, photovoltaics, and antennae, shall be introduced carefully so as not to compromise the historic roof design, or damage character-defining roof materials, or the overall character of the historic district.
- 7 Retain Chimneys and Chimney Tops.** Chimneys are an important architectural feature and the removal or alteration of existing chimneys alters the historical integrity of the house and is not recommended.

- .8 Retain the Original Roof Form and Details.** *If attic space is converted into living space and dormers are added, retain the original roof pitch to avoid a “pop-up” appearance, especially on the front façade. Avoid adding details that did not exist originally.*
- .9 Existing Dormers.** *Original dormers should be preserved and only elements beyond repair may be replaced. If a replacement is needed, original size and shape should be maintained.*
- .10 New Dormers.** *New dormers must be functional, to allow light in or to add more living space, they should not be merely decorative and should be in keeping with the style of the historic house. They should be located on the rear and inset from first-floor side wall below it. Set new dormers back from eave and do not extend above the ridge of roof.*
- .11 Metal Roofs.** *Avoid installing an inappropriately scaled metal roofing material on a house that did not have a metal roof originally. Many of the current metal roofs have an industrial appearance and should be avoided. Only a true crimped or seam standing seam metal roof will be considered.*

### 3.8 Garages & Accessory Structures

#### History and Development

Most early garages were detached and sited in the rear yard, accessed either by a linear driveway leading from the street or from the rear property line via an alley. Corner lots sometimes oriented garages toward the side street. Most, though not all, garages were single bay; sometimes garages were shared by adjoining property owners. Smaller storage buildings and sheds were also typically located unobtrusively in the rear yard.

Many original garages and even a few “carriage houses” remain in use in Norman’s Historic Districts. Like other early site features, these accessory structures contribute to the historic character of individual sites and the district as a whole. In some cases, the accessory building echoes the architectural style, materials, and details of the principal structure on the site. Many are humble gabled structures with the gable end facing the street. Many houses in Southridge Historic District have garages attached to the main structure and facing the street.

Contemporary style houses have incorporated their garage or carports into their house plan, but typically they do not project beyond the established front wall of the house. While the construction of new garages and carports is necessary, their placement and approach should respect the original “front line” of the house. This would place them behind the existing setback. Locating them to the rear of the property is preferable.

#### Policy

Original historic structures should be preserved if possible. However, since many early garage structures were of poor construction quality or sited inconveniently, or of a size not accommodating of modern vehicles their demolition or removal may be considered.

In historic districts, the compatibility of a proposed new garage or accessory building should be reviewed in terms of location, orientation, form, scale, size, materials, finish, and details. It is also important to consider the impact of the proposed construction on the existing site and site features, as well as neighboring structures in close proximity. Proposed changes to garages will also be reviewed in terms of their role in site circulation.

#### Things to Consider As You Plan

- Many of Norman’s early accessory buildings are very simple structures with little in the way of internal framing. Consequently, routine maintenance and repair of early garages and accessory structures is essential to their preservation.
- Additional information on the appropriate rehabilitation of roofs, walls, windows, doors, and materials of garages and accessory structures can be found in Chapter 3, Exterior Features of Historic Housing.



This Mission Revival-style, two-car garage is associated with Patricio Gimeno House in the Chautauqua District.



This two-car garage was built around 1944, apparently using leftover building materials.



This two-car garage plus workshop space was sensitively located in the rear and oriented inwards to minimize its visual presence.



### 3.8.1 Standards for Administrative Bypass

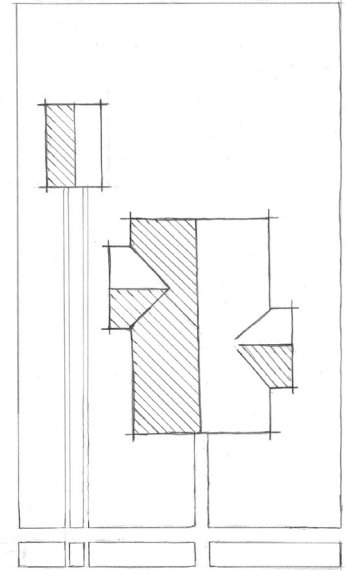
*The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, then the application will be forwarded to the Historic District Commission for a full review.*

- **Small Buildings Allowable.** An Administrative Bypass may be issued if the accessory building's footprint is no greater than 108 square feet and is not constructed on or attached to a concrete slab, foundation, or permanent base and has no electric, plumbing, or gas service connection. This does not require a building permit. It is recommended that the design of these buildings be compatible with the primary structure and the other surrounding or nearby structures or screened with fencing or landscaping. Accessory buildings must be located in the rear yard and must not be visible from the street.

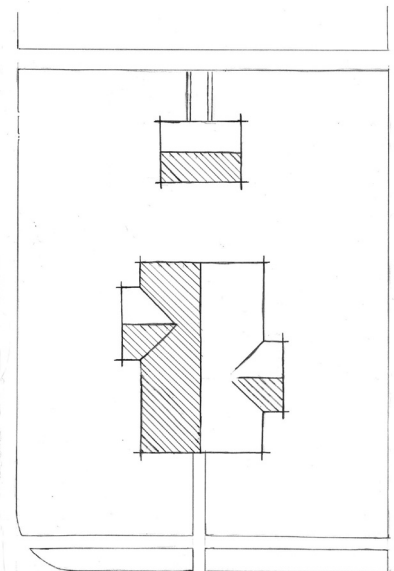
### 3.8.2 Guidelines for Garages & Accessory Structures

*A full review by the Historic District Commission will take the following criteria into consideration to be issued a Certificate of Appropriateness (COA):*

- 1 Preserve Accessory Structures.** When possible, retain and preserve garages and accessory structures in their original locations and configurations. Even if the function changes, the exterior appearance should remain the same.
  - 2 Preserve Original Materials.** When possible, retain and preserve character-defining materials, features, and details of historic garages and accessory buildings, including foundations, siding, masonry, windows, garage doors, and architectural trim. When necessary, repair character-defining materials, features, and details of historic garages and accessory buildings according to pertinent guidelines.
  - 3 Replace Only Deteriorated Portions.** If replacement of a deteriorated element or detail of a historic garage or accessory building is necessary, replace only the deteriorated portion in kind rather than replacing the entire feature. Match the original in design, dimension, texture, and material. Consider compatible substitute materials only if using the original materials is not technically feasible.
  - 4 New Garage Construction.** A new garage shall be compatible in form, scale, size, materials, features, and finish with the principal structure. New garage structures shall be the traditional height and proportion of accessory buildings in the district.
- The following criteria will be considered for a new garage constructed where there is currently no historic structure:
    - \* The new structure will utilize alley access though alley access may not currently exist.
    - \* The new footprint will be 500 square feet or 5% of the property.
    - \* The proposed construction will preserve existing trees.



The traditional location for a garage is the rear of the house at the end of single driveway that runs along the property line.



New garage location is at the rear, off alley, and not visible from the front. Not replacing a historic garage.



Design details such as the jerkin head gable of this garage are often used as complements to the primary structure.



A pyramidal roof was common on single car garages built in the 1920s.



Roof pitch is also used as a design detail. This garage accompanies a Tudor Revival house with the characteristic steeply pitched roof.

- .5 Maximum Garage Footprint of New Garage Construction.** *New garages may expand beyond the original footprint of one- or two-car garages up to a maximum of 500 square feet or 5% (in total, not each structure) of the property, whichever is greater.*
- .6 New Garage Height.** *New garages in blocks that contain only one-story garages should be one-story. Two-story and one and a half story garages may be built if located on a block where two-story or one and a half story garages are dominant or in an adjacent property. One and a half story garages may be built if their massing and height are similar to that of the original garage or adjacent one-story garages. Wall height should be no greater than the principal structure.*
- .7 Location.** *Site garages and accessory structures away from primary view and set them behind the front wall of the house.*
- .8 New Garage Doors.** *Install single doors instead of double width doors. Stamped metal and vinyl doors are inappropriate. Recessed panel doors are appropriate. Raised panel products should be avoided.*
- .9 Prohibited Materials.** *The use of vinyl, masonite, aluminum or other metal sidings is prohibited and are inappropriate to the local historic districts.*
- .10 Alternative Materials.** *As long as they are consistent with the size, pattern, shape, dimensions and texture of the historic wood siding, fiber cement products may be appropriate in new construction of garages for rear or side elevations that are not easily visible from the public right-of-way. It should be noted that wood siding does not have "wood grain." Smooth cement board is allowed.*
- .11 Reconstruction.** *The reconstruction of outbuildings should be based on historic evidence, such as photographs, Sanborn maps or other documentation. If no such evidence exists, the design should be derived from the architectural style of the primary building and historic patterns and characteristics of the historic district.*
- .12 Carports.** *Carports shall be unattached to the primary structure, located in the rear yard, be constructed of wood or masonry, and have limited visibility from the street.*
- .13 Request for Garage Demolitions.** *The following criteria will be considered when a garage structure demolition and/or replacement is proposed:*
  - \* If the existing structure is of extraordinary architectural or historical significance, it should be retained if repairs are reasonably possible.*
  - \* If the existing structure is dilapidated, leaning, lacking a solid foundation, or of substandard construction, it may be eligible for demolition.*
  - \* If the existing structure is 240 sq. ft. or less, it may be eligible for demolition.*
  - \* If the existing structure was built after the period of significance, it may be eligible for demolition.*
  - \* The demolition of existing historic structure will enable access to the rear yard where no access currently exists, it may be eligible for demolition.*



- .14 Storage Buildings Over 108 sq. ft.** *Storage buildings should be located in the rear yard, and not visible from front right-of-way. They should be made of wood or material compatible with the historic structure.*
- .15 Additions.** *Additions to existing garages may be more appropriate than demolition or reconstruction.*

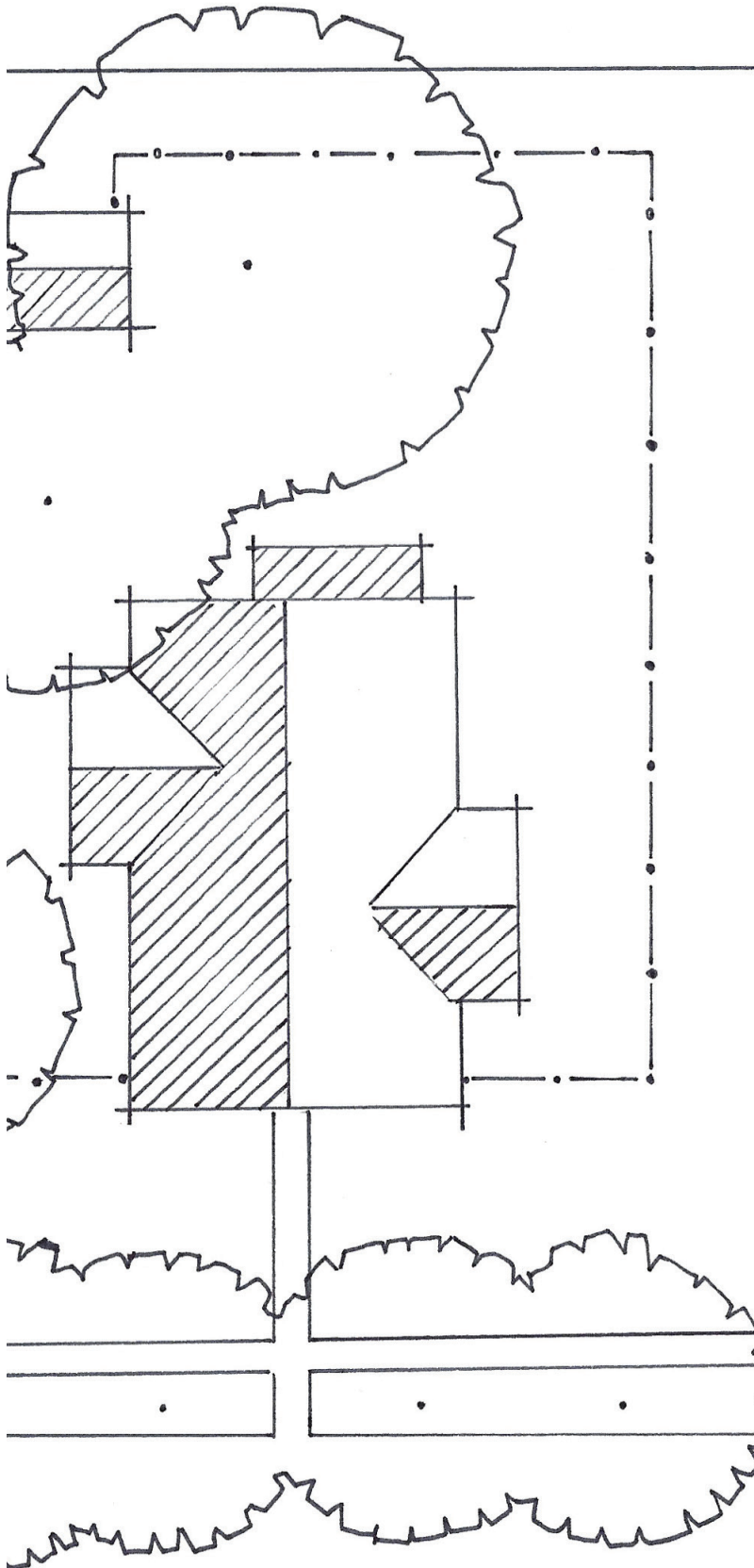


Recessed panel doors are appropriate.



Raised panel products should be avoided.

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# SECTION

# 4

SITE AND SETTING  
OF HISTORIC  
NEIGHBORHOODS

## 4.1 Site Features and Landscape



Landscape design that complements building patterns and style is encouraged.



A mature tree canopy is a vital part of a historic neighborhood's streetscape.



Period-style lamp posts help define streetscape character in historic districts.

The introduction of intrusive, contemporary site features or equipment such as large parking areas, swimming pools, tennis courts, freestanding metal buildings, or mechanical equipment must be carefully reviewed to determine if it will compromise the historic character of the site and the district. Although the impact of intrusive contemporary features can often be diminished through careful siting and screening, in some cases it may be so detrimental to the character of the site or the streetscape that the alteration cannot be accommodated. Such might be the case if the bulk of a residential front yard were paved for parking or if an addition required the removal of several healthy, mature shade trees.

Shade trees are a precious resource in Oklahoma and play a major role in defining the historic character of Norman's residential districts. Historically, well-located shade trees were an important means of providing summer cooling. Today they still contribute shade as well as imparting a distinctive atmosphere to the historic districts.

Distinctive site features also contribute significantly to the overall character of the districts and to individual settings. These site-defining elements include things such as hedges, foundation plantings, lawns, gardens, and tree canopies; features that define circulation such as walkways, streets, alleys, driveways, and parking areas; and features that articulate or develop a site such as accessory buildings, fences, walls, lighting, terraces, waterways, swales, fountains, patios, sculptures, arbors, pergolas, pools and planters.

### Things to Consider As You Plan

- Most early Norman neighborhoods are shaded by a heavy deciduous tree canopy that adds enormous aesthetic appeal and historically performed a vital cooling function during the hot summer.
- Removal of mature, healthy trees should be considered only for absolutely compelling reasons, such as safety or life of property.
- Whenever a tree is removed, whether it is diseased, storm damaged, or healthy, the district setting is diminished.
- The planting of a similar replacement tree in its place or nearby is strongly encouraged to perpetuate the tree canopy that is so important to the landscape as well as the individual building sites.
- The City of Norman's Urban Forester has a list of appropriate tree species and local sources.

### Landscape Considerations

**Protect Mature Trees During Construction.** Protect large trees and other significant site features from immediate damage during construction and from delayed damage due to construction activities, such as loss of root area or compaction of the soil by equipment. The critical root zone of a threatened tree must be surrounded by temporary fencing to prevent any construction activity or equipment from endangering it. It is especially critical to avoid soil compaction within the drip line of trees.

**Preserve Tree Canopy.** Prune and trim trees in front yards and public rights-of-way in a manner that preserves tree canopies. In consultation with the City Forester, introduce new and replacement plantings to ensure that existing tree canopies will be preserved.

**Replace Aging Trees.** Replace a seriously diseased or severely damaged tree (see Section 8.2 Preservation Glossary for definition) or hedge with a new tree or hedge of an appropriate species. It is not appropriate to remove healthy, mature trees.

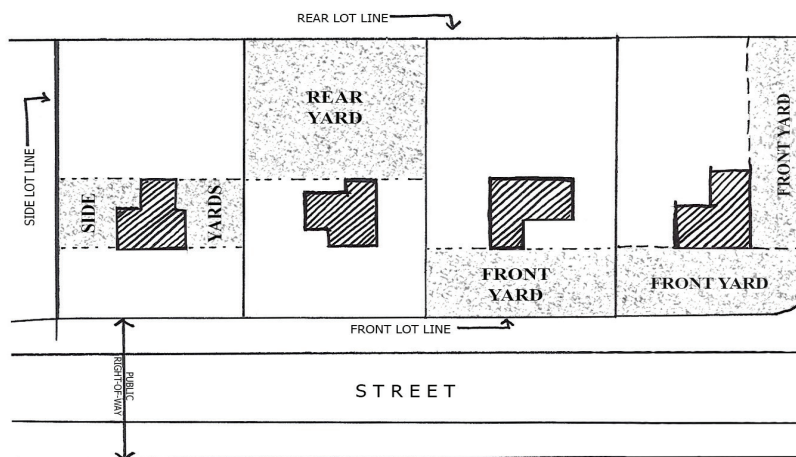
**Install Root Barriers.** When sidewalks are replaced, they should have root barriers installed to protect the concrete from future breakage by tree roots.

These are key steps to maintain the vitally important tree canopy in historic districts:

- Protect mature trees during construction.
- Protect tree canopies.
- Replace aging or diseased trees.
- Install root barriers under new sidewalks.

### Recommendations

- Avoid landscaping that requires continual moisture, i.e. shrubs, trees, and plants, within ten feet of a historic building.
- Remove climbing vines and ivy from historic buildings and walls as they damage the building fabric.
- All plants and vegetation growing in wall and foundation crevices needs to be removed without damage to the historic fabric.
- All landscaping and planters must not block or obstruct the normal flow of pedestrian or vehicular traffic.
- Parking structures shall be compatible in design and materials with surrounding historic buildings and districts.
- New construction is encouraged to provide parking behind the building.
- At no time shall a building be demolished to provide surface parking.
- Ensure that the design of any new parking structure follows the standards of New Secondary Structures.



Corner lots have two front yards.

### **4.1.1 Standards for Administrative Bypass:**

*The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, then the application will be forwarded to the Historic District Commission for a full review.*

- **Garden Structures.** Garden structures such as a pergola or freestanding trellis, if located behind the home in an inconspicuous location, may be constructed if it is less than 108 sq. ft.
- **Surface Parking.** All design and construction of parking areas within historic districts may be approved through administrative bypass if placed behind primary structures at the rear of the property, and also set back as far as possible from side streets on corner lots.
- **Storm Shelters.** Storm shelters are a necessary safety concern in Norman. Structures, which are not visible from the street may be approved through administrative bypass. They must meet state requirements for wind load design.
- **Swimming Pools.** If located in rear yard and not visible from front right-of-way.

### **4.1.2 Guidelines for Site Features and Landscape**

*A full review by the Historic District Commission will take the following criteria into consideration to be issued a Certificate of Appropriateness (COA):*

- .1 Pergolas and Trellis.** Do not add a new pergola or freestanding trellis on a front or side elevation unless there is historical evidence one existed. Not appropriate if larger than 108 sq. ft.



## 4.2 Sidewalks, Driveways, and Off-Street Parking

### History and Development

In Norman's early neighborhoods, front walks usually led directly to the front door of a house from the sidewalk. Depending on the topography, the walkways often incorporated steps and, sometimes if the front yard was fenced, a decorative gateway. Traditional paving materials were concrete and brick or stone pavers. Plantings often lined the walkways.

Driveways typically located along the property line, led directly to the back yard, sometimes to a garage or carriage house. Public alleys sometimes provided the automobile access to rear yards and garages. Occasionally, a porte cochere provided a covered parking space attached to the main structure. Driveways were usually made of gravel or compacted soil, changing over time to concrete. Often a grass median separated two gravel or aggregate concrete runners. Occasionally, more decorative brick or stone pavers were used.

Historically, off-street parking areas for multiple cars were rare in residential neighborhoods. Initially, on-street parking met the demand for parking spaces, even in commercial districts. Over time as cars have grown both larger and more numerous, a greater demand has been created to park more cars.

### Policy

Preserving original paving patterns is important to maintaining the character of individual building sites and the district as a whole. The location of driveways and other paved areas is very important to both the preservation of neighborhood character, and to the preservation of historic integrity of individual properties.

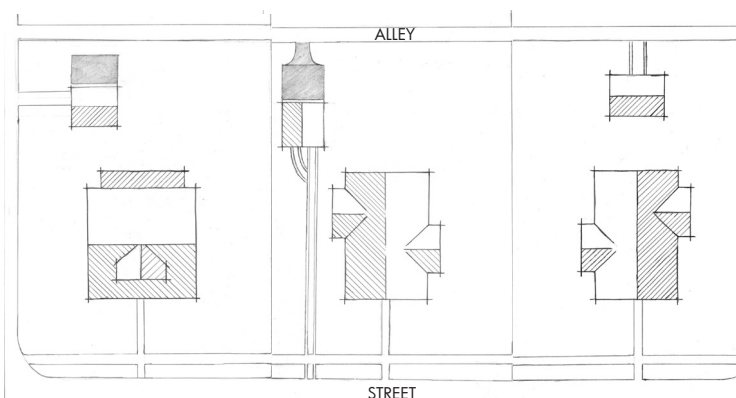
The consistency and repetition of sidewalk and driveway spacing, placement, dimension and materials creates a rhythm to the street in Chautauqua and Miller Historic Districts.



Shared driveways were quite common in the early 20th century. A few still exist in Norman.



Ribbons or runners are very traditional and generate less runoff than impervious surfaces.



Typical configuration of Historic Districts except in Southridge, which has a variety of drive and sidewalk configurations.





Traditional ribbon drives are sometimes modified to create a solid surface.

### 4.2.1 Standards for Administrative Bypass:

*The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, then the application will be forwarded to the Historic District Commission for a full review.*

- **Widening Driveways.** Widening a driveway can be approved by administrative bypass if to a maximum width of 10 feet. Must be either concrete or city approved pavers.
- **Parking pads.** Allowed if located off alley as long as they are less than 400 sq. ft.
- **Walkways.** Private sidewalks, walkways are allowed as long as they meet typical configuration.
- **Paving.** Maintain existing concrete ribbons or runners and widen by lining with another material such as brick.
- **Rear Parking.** Rear access and rear yard parking may be approved by administrative bypass.
- **Driveways and Sidewalks.** Reconstructing driveways and sidewalks in their original location with materials that match the original.
- **Gravel Driveway to Concrete.** Converting a gravel driveway to a concrete driveway as long as it is ten feet 10' wide. (Will need a paving permit.)

### 4.2.2 Guidelines for Sidewalks, Driveways, and Off-Street Parking

*A full review by the Historic District Commission will take the following criteria into consideration to be issued a Certificate of Appropriateness (COA):*

- .1 **Front Yard Driveways.** In historic districts, residential driveways shall be perpendicular to the street, except in individual cases where there is historical documentation of an alternate configuration. Unless there is historic documentation otherwise, driveways shall be located near the property line on one side of the house.
- .2 **Driveway Width.** Driveways shall be one car width, not to exceed 10 feet wide, unless there is historic documentation of an alternate configuration. Driveway width may vary as it approaches a garage in order to correspond to the width of the door opening.
- .3 **New Driveway Composition.** Driveways shall be constructed from material allowed by the City Codes.
- .4 **Ribbon Driveways.** Ribbon driveways may be newly installed in historic districts. The minimum width of ribbon paving is 18 inches.
- .5 **Circular Drives.** Circular drives are not appropriate in front yards or corner side yards unless there is historic documentation on the specific property in question.

- .6 Shared Driveways.** *Historic driveways shared by two adjacent properties may be retained and preserved.*
- .7 Driveway Location.** *Driveway locations should not be altered if it affects the rhythm of the street.*
- .8 Sidewalk Location.** *Sidewalks on private property shall be maintained in their traditional location.*
- .9 Sidewalks and Curbs.** *Public sidewalks and approaches shall meet City Codes. Sidewalks and curbs on private property may be constructed of finished concrete, brick, or stone.*
- .10 New Paved Areas.** *New paved areas should never directly abut the principal site structure, significantly alter the site topography, or overwhelm in area the residential, landscaped character of a rear or side yard. Care must be taken that paved areas do not injure nearby trees by intruding onto their root areas. They should be designed to be compatible in location, patterns, spacing, configurations, dimensions, and materials with existing walkways and driveways.*
- .11 Rear Yard Area.** *New parking areas are encouraged to be off alleyway. Rear yard parking must meet city requirements.*
- .12 Side Yard Parking Area.** *Not appropriate.*
- .13 Off Street Parking Area.** *Not appropriate at the front yard of the property except within an existing driveway.*



A front yard fence under 36 inches tall creates definition and separation but still allows the beauty of the house to shine.



There are few stone walls in Norman's historic districts so those that exist have special significance.

## 4.3 Fences and Masonry Walls

### Policy

Original historic fences and walls are important character-defining features and should be preserved and maintained. Front yards create a context for houses and establish a rhythm for the street. These elements are important to the preservation of a district's historic character and to strengthening the cohesiveness of a residential historic district.

### Things to Consider As You Plan

Preservation of existing fences and walls requires routine maintenance and repair. Keeping the bottom edge of wooden fence lines raised slightly above ground and protected by a sound paint film, opaque stain, or wood preservative will significantly extend their life span. When deteriorated pickets or boards must be replaced, decay-resistant or pressure-treated wood should be considered.

A need for security or privacy or the desire to enhance a site may lead to a decision to introduce a new fence or wall. Within the historic districts any proposed new fence is reviewed with regard to the compatibility of location, materials, design, pattern, scale, spacing, and color with the character of the principal building on the site and the historic district.

### 4.3.1 Standards for Administrative Bypass

*The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, then the application will be forwarded to the Historic District Commission for a full review.*

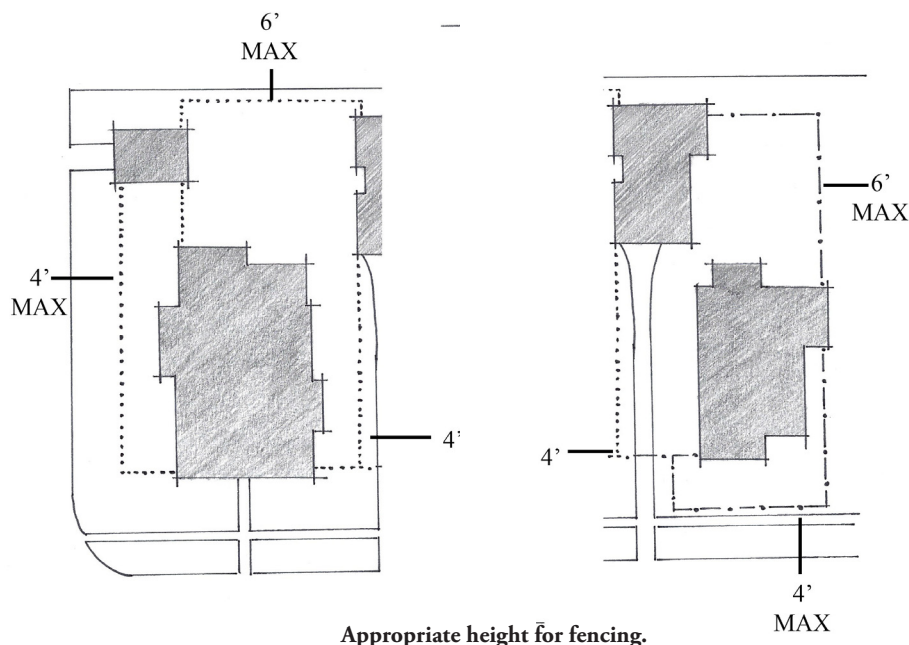
- **Replacing Fences.** *If an existing fence or wall is being replaced with one that is the same in material, height, location, and design, a Certificate of Appropriateness is not required.*
- **Front Yard Fences.** *Front yard fences of up to 4 feet in height may be approved by Administrative Bypass.*
- **Side Yard Fences.** *Side yard fences of up to 4 feet in height may be approved by Administrative Bypass.*
- **Rear Yard Fences.** *Rear yard fences of up to 6 feet in height may be approved by Administrative Bypass.*

### 4.3.2 Guidelines for Fences and Masonry Walls

*A full review by the Historic District Commission will take the following criteria into consideration to be issued a Certificate of Appropriateness (COA):*

- 1 **Preserve Original Materials.** *Retain and preserve historic fences and walls that contribute to the overall historic character of a building.*

- .2 **Front Yard Fences.** *Front yard fences taller than 4 feet are prohibited by the Norman Zoning Ordinance. Corner lots have two front yards; therefore, fences on either "front" that are taller than 4 feet are prohibited. Chainlink fences are not allowed on front yards.*
- .3 **Side Yard Fences.** *Side yard fences taller than 4 feet require a Certificate Of Appropriateness. Side yard fences taller than 6 feet are prohibited.*
- .4 **Rear Yard Fences.** *Rear yard fences taller than 6 feet require a Certificate Of Appropriateness. Rear yard fences taller than 8 feet are prohibited by the Norman Zoning Ordinance.*
- .5 **Fences on Corner Properties Adjacent to Alleys.** *Fences on corner properties with alley access shall be located very carefully to maximize sight lines and minimize conflicts between alley traffic, pedestrians, and on-street traffic.*
- .6 **Fence and Wall Materials.** *Fences or walls shall be constructed of wood, brick, stone, iron, cast or forged metal, chain-link, stucco, or a combination of these materials. Stone or brick used in walls shall be compatible in size, scale, and style to that used elsewhere in the historic district. No vinyl, cinder block, concrete block, or corrugated metal, may be used for fences or walls in historic districts.*
- .7 **Other Materials.** *Although compatible contemporary fence and wall designs constructed in traditional materials are appropriate in the districts, new fencing or wall systems constructed of incompatible contemporary materials such as vinyl and imitation stone or stucco are not appropriate for use in historic districts.*
- .8 **Finished Side Out.** *Fences or walls facing the street shall be constructed with the finished side out.*
- .9 **Fence Styles and Design.** *Opaque fences that are less than 75% transparent should not block view of significant architectural features of the primary property.*







Original fixtures on a Spanish Revival structure



Understated overdoor light fixture and entry on a Classical Revival structure



Ornate original iron fixture on Tudor Revival structure

## 4.4 Lighting

### History and Development

Early Norman streetlights ranged from elaborate designs, such as translucent globes mounted on cast-iron poles capped with decorative finials, to simple, bracketed globes mounted on utility poles. Manufacturers of the day described the light cast by these early fixtures as a “soft, yellow-toned glow.” This is a marked contrast to the harsher bluish-tone light cast by today’s mercury vapor streetlights. In response to increasing public demand for dark skies, lighting manufacturers have begun to offer high-pressure sodium vapor fixtures that produce a softer glow.

### Policy

Installing new lighting fixtures on historic properties does not require a Certificate of Appropriateness (COA); however, appropriate lighting is an important consideration in maintaining the character of Norman’s Historic Districts.

### Things to Consider As You Plan

- Balancing issues of light pollution with needs for safety and security requires careful forethought and coordination regarding the quantity and location of exterior lighting. Considerations in lighting fixtures should include location, design, material, size, color, scale, and brightness.

**Retain Original Fixtures or Appropriate Motifs.** Retaining and maintaining original light fixtures is always preferable; however, if fixtures are missing or damaged, alternatives may be considered. Antique or reproduction lighting fixtures of a similar design and scale may be installed, or reproduction fixtures that reflect the design of the building may be selected. For example, it would be appropriate to select Mission motif lighting for a Craftsman bungalow. Selecting a fixture style in contrast to the building style is not recommended.

**Choose simple, discreet styles.** Inconspicuous contemporary fixtures that complement the style and the character of the building are recommended for historic buildings. Simple, discreet styles and materials are usually successful. If more illumination is desired than original fixtures provide, unobtrusively located recessed lights may solve lighting needs without competing with original design.

**Choose appropriate locations for security lighting.** Due to concerns for security and safety, additional lighting may be desirable on a particular site. Property owners should give careful consideration to where supplemental lighting is needed and in what quantity. Adequate lighting can often be introduced through fixtures on residential-scale posts, recessed lighting, footlights, or directional lighting mounted in unobtrusive locations. Such solutions are far more in keeping with the historic character of the districts than harsh floodlights and standard security lights mounted on tall utility poles. Sometimes even compatible fixtures may compromise a building or a site if they are improperly spaced or located.

## 4.5 Signage

### Policy

In addition to a review by the Historic District Commission, signs will be subject to the regulations and permitting requirements established in Chapter 18 of the Code of Norman, Oklahoma, also referred to as the Sign Ordinance. Applicants shall coordinate the design and placement of any sign in a historic district with the Sign Ordinance as well as these guidelines.

A Certificate of Appropriateness is required for any new sign of a permanent nature which is to be attached to, or erected on the site of, any structure located within a historic district. This includes, but is not limited to, signs and lettering painted onto elements of the structure or applied to awnings. A Certificate of Appropriateness will also be required for any existing sign which is to be moved, demolished, reconstructed, restored, or altered, except when such work satisfies all the requirements for “ordinary maintenance and repair.”

### Things to Consider As You Plan

- Applicants are encouraged to seek out photographs and illustrations of historic sign examples for guidance.
- In addition to reviewing sign design, the Historic District Commission will also evaluate the dimensions, materials, legibility, color, letter styles, overall effect, placement, and lighting of proposed signs.

Signage must comply with the requirements established in Chapter 18 of the Code of Ordinances, also referred to as the Sign Ordinance

### 4.5.1 Standards for Administrative Bypass

*The following item can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process.*

- *National Register commemorative plaques, if less than 2 sq. ft. and bronze.*



Projecting sign on a commercial building in Miller Historic District.

### 4.5.2 Guidelines for Signage

*A full review by the Historic District Commission will take the following criteria into consideration to be issued a Certificate of Appropriateness (COA):*

- 1 City Ordinance.** *Signage shall meet City Sign Ordinance for size, location, and materials.*
- 2 Signs Must Be Compatible.** *Size, design, and placement of a sign shall relate to the architectural elements of the structure. Signs shall be compatible with historic character of principal structure and surrounding character of neighborhood.*



This small sign at the Jacobson House conveys information but is very inobtrusive.



This 1960's Ranch-style house probably replaced an older structure in the Miller District.



This 1950's structure has been enhanced with traditional design details that make it more compatible with surrounding structures.



This Mid-Century Modern structure was built after the Chautauqua District's period of significance.

## 4.6 Non-Contributing Resources

### History and Development

In many cases buildings are classified as non-contributing because they were built after the district's period of significance. For example, the Miller Historic District's period of significance is 1903-1949. That means that structures built after 1949 are too new to be contributing resources to the district. These structures may be fine examples of their own time, but they do not contribute to the defining character of the historic district.

Basic guidelines should be applied to architecturally important buildings that are outside of the period of significance.

Newer structures often have greater accommodations for cars, a different approach and orientation to the street which can begin to overwhelm or alter the pedestrian character of a historic district.

### Policy

It is important that non-contributing structures not detract from the integrity and historic character of the district. Because non-contributing resources do occur, the preservation goal is to support a harmonious blend of the old and the new. Therefore, the rules and regulations of the historic district apply to all properties, both contributing and non-contributing.

Non-contributing resources should be controlled only to the extent necessary to make them compatible with the general atmosphere of the district with regard to exterior alterations, additions, site work, and signage.

### Things to Consider As You Plan

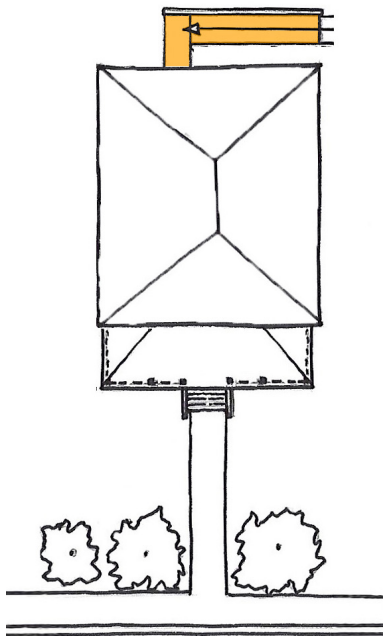
- The term "contributing resource" refers to a historic building or site that retains its original architectural integrity or design. In the case of a structure, it refers to a building whose architectural style is typical of or integral to an historic district.
- A resource is described as "non-contributing" when it adds no historical significance to an individual property or district. This typically occurs when the building is outside the determined period of significance, or has been physically altered past the point where it retains any of its original integrity, e.g. major structural changes have occurred that diminish the building's historic appearance (porches removed or enclosed), windows have been replaced with inappropriate materials or styles, or vinyl or metal siding has been installed.
- Allow for and encourage reconstruction and restoration of missing/ altered features which can change the designation of "non-contributing" to contributing resource.
- A building described as "non-contributing" due to previous alterations, may be re-evaluated if restored to the original design and detail.



### **4.6.1 Guidelines for Non-Contributing Resources**

*A full review by the Historic District Commission will take the following criteria into consideration to be issued a Certificate of Appropriateness (COA):*

- .1 Preservation Guidelines.** *The Historic Preservation Guidelines apply to all structures in Norman's Historic Districts, both contributing and non-contributing.*
- .2 Support Harmony Between Old and New.** *Non-contributing structures shall be controlled only to the degree necessary to make them compatible with the general atmosphere of the district with regard to alterations, additions and changes to the site, and paving. As with all requests for Certificates of Appropriateness in historic districts, each project will be evaluated on its own merits for overall impact on the district as a whole.*
- .3 Non-Contributing Resources.** *Newer buildings are products of their own time. Property owners should avoid making changes that attempt to create a false historical appearance.*



Ramps should be located to minimize the loss of historic features and overall preserve the historic character of the property.

## 4.7 Accessibility, Health, and Safety Considerations

A need for public access to, a change in use of, or a substantial rehabilitation of a historic building may necessitate compliance with current standards for life safety and accessibility. Both the *2006 International Building Code* (adopted by Norman in 2007) and the *Federal Americans with Disabilities Act of 1990* include some flexibility in compliance when a historic building is involved.

The *Americans with Disabilities Act of 1990* does not apply to private residences. Most ramps installed at private residences are generally not permanent structures. Most are constructed from wood and are meant to provide easier access for persons with disability for a finite period of time.

### Policy

Temporary accessibility features are encouraged on the rear of the historic property.

### Things to Consider As You Plan

- Weigh the historic integrity of the house and neighborhood with the value of the improvement and the quality of life.
- **Safety and Accessibility Aids.** Because of the characteristic raised foundation of many early Norman buildings, accessibility needs often require the introduction of a ramp or a lift to the first-floor level or the introduction of railings, handrails, or other safety features.
- Installing accessibility aides in ways that are sensitive to the historic character of the building sometimes requires creative design solutions. Whether the modifications are large or small, with respect to the long-term preservation of the historic building, temporary or reversible alternatives are preferable to permanent or irreversible ones. Consult the Historic Preservation Officer for guidance on how both needs can be served.
- Also see Section 3.2 Entrances and Porches for more information on the addition of handrails.

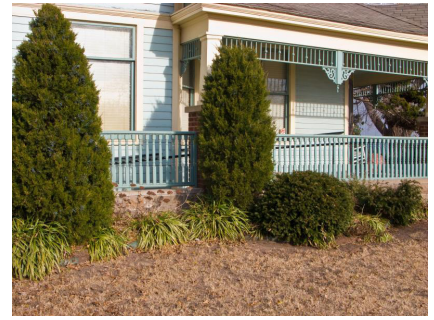
**Modern Security Devices.** Modern security devices such as motion detection systems tend to be more effective in deterring crime, more reliable and less expensive than the burglar bars. Motion detection systems are also far less visually obtrusive in historic structures.

### 4.7.1 Standards for Administrative Bypass:

*The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, then the application will be forwarded to the Historic District Commission for a full review.*

- **Ramps Eligible for Administrative Bypass.** *Wood and wood-like materials used for accessibility ramps may be approved by Administrative Bypass. Ramps shall be designed to have minimal structural and visual impact on the historic resource.*

- **Standards for bypass:**
  - *If temporary/emergency and not permanently attached to building on any side.*
  - *If portable.*
  - *Rear of the structure, not visible from the front right-of-way.*
- **Add Safety Aids Carefully.** *Elements such as handrails, grab bars, or other safety aids shall be added in a way that preserves character-defining features and finishes of the structure and allows them to be removed when no longer needed.*
- **Doorways.** *In an emergency situation, a rear entryway modification application is approved by bypass.*
- **Home Security Devices.** *Electronic detection systems may be installed.*
- **Mailboxes.** *Mailboxes and mail slots should be simple and as unobtrusive as possible. Mailboxes designed with the time period of the house are allowed.*



Ramps are essential for universal access. They can be installed sensitively as this one at the Mary Abbott House, 231 E. Symmes.

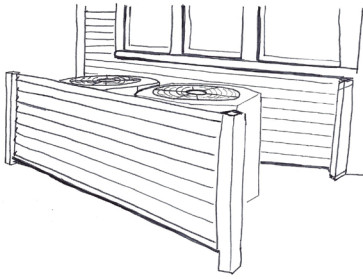


This ramp is successful because it incorporates design elements from the house and is well screened with landscaping.

#### **4.7.2 Guidelines for Accessibility, Health, and Safety Considerations**

*A full review by the Historic District Commission will take the following criteria into consideration to be issued a Certificate of Appropriateness (COA):*

- .1 **Access Ramp.** *If an access ramp needs to be installed due to a change in mobility, it should be located on the rear to minimize the loss of historic features and should preserve the overall historic character of the property. Accessibility aids should be temporary structures that are removable. Permanent ramps and aids will be considered when no other options exist and will be evaluated on a case by case review.*
- .2 **Lifts Require Approval.** *Accessibility aids such as ramps or lifts that require concrete, brick or other more permanent foundations require review by the Historic District Commission.*
- .3 **Modify Doorways Carefully.** *A front doorway is a critical design element in a historic structure and is not appropriate to modify. Rear doorways can be modified when there is no other option. The installation of offset hinges can provide additional width to an opening without physical change to the opening by allowing the door to swing out.*



HVAC units with fence screen



HVAC unit with planted screen



Avoid installing satellite dishes and large antennas on locations visible from the street.

## 4.8 Mechanical, Electrical, and Communication Equipment

Energy conservation, replacement or upgrading of inadequate utility service and the introduction or upgrading of mechanical systems are typical concerns of property owners today. In historic districts, it is important to ensure that these very real concerns are addressed in ways that do not damage or diminish the historic character of the building, site, or district.

**Satellite Dishes.** Satellite dishes, if anchored to the ground by means of a pole, base, or slab are considered structures and may require a Certificate of Appropriateness.

**New Mechanical or Communication Systems.** Systems that include outside units or equipment such as condensers, ventilators, solar collectors, satellite dishes, and large antennas, should be located and installed so that they do not damage or diminish the historic character of the building, site, or district. An inconspicuously located outdoor unit can often be further screened by plantings or fences.

**Utility Lines.** Although utility lines and poles have long been present in the districts, attention should also be given to consolidating old and new utility and communication lines where possible to avoid overpowering the landscape with additional overhead wires. If a new or upgraded power supply will necessitate an additional pole and overhead wires, the use of underground cables may be preferable to prevent visual intrusion.

### Ordinary Maintenance

- Installation of mechanical or electronic equipment such as HVAC systems provided that the location does not obstruct or otherwise detract from the view of the front façade. In the instance of corner lots, both street-facing façades shall be considered as front façades.

### Recommendations

- **Line of Sight.** Mechanical equipment shall not be within line of sight.
- **Rooftop Equipment.** Place rooftop mechanical equipment out of pedestrian sight lines.
- **Ground Mounted Equipment.** Place ground mounted mechanical equipment behind the line of the front façade and screen with planted material or fence screen.
- **New Window A/C Units.** New window air-conditioning units may be used but should not be placed in front or side façades.
- **Existing Window A/C Units.** Existing window units should be replaced in-kind and may remain in its original location. Avoid the installation of air conditioning and electrical equipment on the prominent face of the house; only install equipment in such a way that it does not damage the historic building fabric.



## 4.9 Utilities and Energy Retrofit

### History and Development

In Norman Historic Districts, many energy-conserving site and building features illustrate the sensibility of an earlier era to issues of climate and energy efficiency. Thoughtfully located shade trees buffer residences and sidewalks from the hot summer sun. Projecting porches provide shaded outdoor space and lessen the impact of harsh sunlight on the building's interior. Operable windows and awnings allow occupants to control the introduction of sunlight and breezes within the building. An understanding of how such historic features enhance energy efficiency is critical to maximizing the energy efficiency of historic buildings.

### Policy

Energy conservation, replacement or upgrading of inadequate utility service, and the upgrading of mechanical systems are ever-growing concerns to owners of historic properties. In historic districts, it is important to ensure that these very real concerns are addressed in ways that do not damage or diminish the historic character of the building, the site, or the district.

### Things to Consider As You Plan

In considering energy retrofit options, property owners should be sure that the inherent energy-conserving features of the building are being used and maintained. Consider replacing lost shade trees and introducing additional strategically located shade trees. Besides trees, typical retrofit measures include installation of storm windows, storm doors, additional weather-stripping, insulation, and installation of more energy-efficient mechanical systems. All retrofit measures must be reviewed with their impact on the historic character of the building and the district in mind.

**Storm Windows.** Following any necessary repair of windows to ensure their weathertightness, storm windows can provide additional efficiency. If a property owner chooses interior storm windows, they should be tension-mounted with airtight gaskets. On both exterior and interior storm windows, the ventilating holes must be kept open to prevent condensation from damaging the window or the sill. For more information on selecting new screen and storm doors see Sections 3.5 and 3.6 for Windows and Doors.

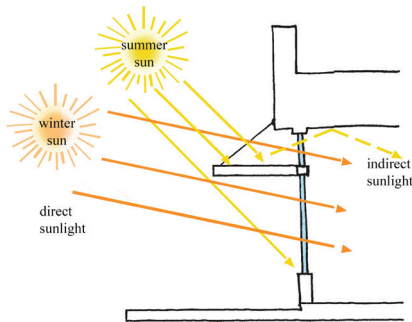
**Window Awnings.** Historically, fabric window awnings were conservation features that also provided opportunities to introduce color.

**Alternative Energy.** Alternative energy, such as solar and wind, can be a benefit to the energy use of a building but only if the energy efficiency maintenance of the building has been addressed appropriately.

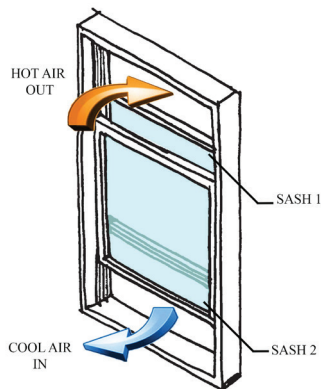


Window awnings may serve as an energy saver by regulating the amount of sunlight entering the structure.

*A Well-Maintained Building Is A Sustainable Building*



**Canopy works as a light shelf allowing indirect light through the transom.**



**Double-hung windows allow for natural ventilation and better control of airflow.**

**Weatherization and Insulation.** Having a comfortable living environment and saving energy are important components to sustainability. The goal with weatherization is to keep the outside out and the inside in. The Secretary of the Interior has guidelines specifically for addressing sustainability in historic buildings.

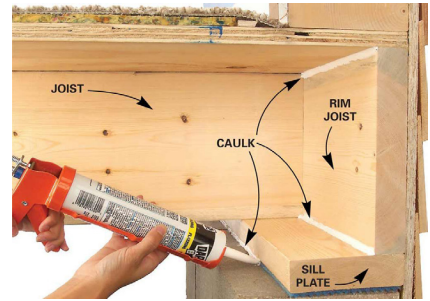
### Recommendations

1. The first step in weatherization is to address air infiltration. Begin with the least invasive and most cost-effective weatherization measures, such as caulking around openings and weather stripping of doors and windows.
2. The second step, if necessary, is to install a breathable insulation in the exterior cavities such as attic, underfloor, and exterior wall if accessible. At no time should historic siding be removed to install insulation.
3. Appropriate insulation materials might be fiberglass batt, rockwool or mineral wool. Loose fill blown-in insulation such as cellulose or fiberglass are acceptable. Installation of insulation in under floor and attic space should always be adequately ventilated.
4. The Secretary of the Interior's Guidelines for Sustainability do not recommend "Using wet-spray or other spray-in insulation that is not reversible or may damage historic materials." Removing spray foam in the future is difficult and can cause damage to the building.
5. Inappropriate insulation that does not allow the original building to breathe can trap moisture and mask water leaks. This can cause wood to rot and the building frame to deteriorate.
6. Radiant barrier, in the form of "paint" or film can be installed on the underside of the roof in unfinished attic space. The radiant barrier can reduce the heat buildup in the attic which makes the living space below more comfortable and reduces energy bills.
7. Underfloor insulation can be installed between floor joists if the building is elevated off the ground and will improve physical comfort without trapping moisture. Underfloor crawl space should always maintain cross ventilation.

### Ordinary Maintenance for Energy Efficiency

- Identify ways to reduce energy consumption and enhance comfort without destroying original features. Start with small steps that can make a big difference. Caulk and weather stripping can enhance the performance of a well-maintained historic window. A replacement window often must be replaced again in a few years because the window fails and fogs.
- A well-maintained wood window can be more energy efficient than an inexpensive replacement. Aluminum is a conductor of heat and cold while wood is an insulator.

- Installing replacement windows that reduce the size of the original opening changes the character of the building, reduces the natural light and the potential ventilation.
- Light colored Low-E energy efficient film can be applied to the interior of windows to reduce solar heat gain without dramatically changing the appearance of the window.
- Interior or exterior storm windows can be installed to improve energy efficiency. Care should be taken to choose a compatible storm window that matches the original design.



Caulking around openings is the least invasive and most cost-effective weatherization measure.

### 4.9.1 Standards for Administrative Bypass

*The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, then the application will be forwarded to the Historic District Commission for a full review.*

- **Storm Windows.** *The installation of storm windows does not require a Certificate of Appropriateness; however, if metal storm windows are installed over wood windows, avoid unfinished or clear anodized aluminum finishes. Exterior storm windows should be painted to blend with surrounding elements (typically the window frame and sashes) and match existing trim color and window styles.*
- **Solar Panels.** *Solar panels can be installed where they do not detract from the building such as on the “back” side of the house, or on the roof where they are not visible from the street or public view.*
- **Free-standing Solar Racks.** *Solar racks can be installed at the rear of the property to create a shade structure or can be installed on an outbuilding, such as a garage roof.*
- **Skylights.** *If flat in profile and positioned away from public view, skylights can be installed in older houses.*



Fiberglass batt is an appropriate insulation material.



Mineral wool is also an appropriate insulation material.



Spray insulation is not recommended since it can trap moisture and mask water leaks, causing wood to rot.





Not recommended - solar roof panels have been installed at the rear, but because the house is situated on a corner, they are highly visible and negatively impact the character of the historic property. (Source: NPS)



Solar panels, which also serve as awnings, were installed in secondary locations on the side and rear of this historic post office and cannot be seen from the front of the building. (Source: NPS)



Free-standing solar panels have been installed where they are visible but appropriately located at the rear of the property and compatible with the character of the building. (Source: NPS)



Solar panels were installed appropriately on the rear portion of the roof on this historic house that are not visible from the primary elevation. (Source: NPS)

## 4.9.2 Guidelines for Utilities and Energy Retrofit

*A full review by the Historic District Commission will take the following criteria into consideration to be issued a Certificate of Appropriateness (COA):*

1. **Retain Inherent Energy-Conserving Features.** *Retain and preserve the inherent energy-conserving features of historic buildings and their sites, including shade trees, porches, awnings, as well as operable windows, transoms, shutters, and blinds.*
2. **Use Traditional Energy-Saving Practices.** *Increase the thermal efficiency of historic buildings by observing appropriate traditional practices, such as weatherstripping and caulking, and by introducing energy-efficient features such as awnings, operable shutters, and storm windows and doors, where appropriate.*
3. **Skylights.** *Skylights can add light to interior spaces and make attics spaces more useable. Bubble-dome skylights are not appropriate for buildings within historic districts.*
4. **Solar Panels.** *Avoid installing solar panels on the street side of the house or permanently altering roof with the installation of solar panels. Panels should be installed flat and not alter the slope of the roof. They should be positioned behind existing architectural features such as parapets, dormers, and chimneys to limit their visibility.*
5. **Compatibility.** *Use solar panels and mounting systems that are compatible in color to the property's roof materials.*
6. **Free-Standing Solar Racks.** *Free-standing solar racks can be installed at the rear of the property to create a shade structure or can be installed on an outbuilding, such as a garage roof.*
7. **Low Pitch Roofs.** *Low pitch roofs and low-profile panels may be installed on non-street-facing roof planes. Avoid roof racks that elevate the panels or are at a different pitch than the roof. Solar shingles may be installed on sloped roof-surfaces and are less intrusive than panels. However, removal of historic material should be avoided.*
8. **Flat Roofs.** *Flat roof structures should have solar panel installations set back from the roof edge to minimize visibility. Pitch and elevation should be adjusted to reduce visibility from public right-of-way.*

## 4.10 Recommendations for Color

*A Certificate of Appropriateness (COA) is not required for painting properties in Norman historic districts. However, color is an important element of neighborhood appearance.*

### History and Development

During the first quarter of the century, when the Chautauqua and Miller neighborhoods were being constructed, many bungalows and Colonial Revival style residences were painted white. Popular colors for other styles included muted earth tones or grays, with black sometimes used as a trim color.

A well-executed exterior color scheme can dramatically alter the appearance of a building. Likewise, the application of garish colors on a building can overpower its architectural character and compromise its integrity. Although an exterior paint job is not an irreversible change to a building, it is a highly visible and relatively expensive one, so a careful study of the style of the building, the surrounding streetscape, and the region's climatic conditions makes sense.

### Things to Consider As You Plan

The following suggestions should provide guidance in painting historic structures:

- Consider building style. When selecting paint colors for historic properties, consider the style of the residence. Note mortar color in any masonry such as foundations or porch piers and select a color scheme that is compatible with the mortar color.
- Avoid painting unpainted masonry. Unpainted brick and masonry should not be painted. If masonry is already painted, keep paint in good repair to protect masonry underneath.
- Match new masonry with existing. If new brick or stone is used on an addition or for repair, it should be identical or similar in color, style, shape, and texture to the original material.
- Match mortar color. When mortar is applied to new additions or used for repair or repointing, match the old mortar in color, composition, and texture.

**Maintain Your Investment.** Routine cleaning of painted surfaces is an important maintenance step. Often, washing of a previously painted exterior with a garden hose will reveal that the paint film is intact under the surface dirt or mildew. However, power washing can damage intact paint layers and force water into the wall itself.

The success and longevity of any paint job depends primarily on the quality of the surface preparation and the paint. Proper preparation includes removing all loose or peeling paint down to the first sound paint layer.

Stripping intact layers of paint is unnecessary and undesirable from both a historical and a practical standpoint. Often, only hand-scraping and hand-sanding are necessary for removing loose paint.



Natural colors work in harmony with the color of the house.



Brick has an inherent natural color and should not be painted or sealed.



Compatible colors create harmony in the façade.





Color schemes should tie a building together and create harmony in the façade.



Color can be used to highlight details.



Consider the architectural style before choosing a paint color. What is appropriate for this house, might not be appropriate for a Tudor style.

**Avoid Destructive Methods.** Destructive paint-removal methods such as sandblasting, waterblasting, or using propane or butane torches, can be very destructive to historic buildings because they irreversibly damage historic woodwork, soft metals, and masonry, and they are potential fire hazards. However, if paint is severely deteriorated and gentler methods are not successful, thermal devices such as electric hot-air guns may be used with care on decorative wooden features, and electric heat plates may be used with care on flat wooden surfaces. Similarly, chemical paint strippers may be used to augment gentler methods, but the surface must then be neutralized to allow the new paint film to bond.

Mildew can ruin a new paint job. Eradicate it before repainting by using either a commercial preparation containing 5 percent calcium hypochlorite or a homemade solution consisting of 3 quarts of warm water, 1 quart of chlorine bleach, 2/3 cup of borax, and 1/2 cup of household detergent. Either solution should be applied with care using a soft scrub brush, and thoroughly rinsed off. Keep the solution off your skin.

Once wooden surfaces have been cleaned, scraped, and sanded, any exposed surfaces should be primed with a high-quality exterior primer, and all open joints should be recaulked (not including the horizontal lap seam of clapboard siding) before repainting with a compatible paint. Although the color is more uniform and less translucent than the early, less homogeneous oil paints, today's high-quality latex and acrylic semi-gloss paints provide a similar appearance. Preparation for painting stucco and previously painted brick or stone is similar to that for painting wooden surfaces.

**Accent Details With Color.** Generally, the body of the building is a natural material or is subdued to serve as the base or background for lighter, brighter trim colors which can highlight the details. Color can be used to accent the details of buildings or highlight the entry by painting the doors a different color.

**Original Color Scheme.** To find the original color scheme of a building, gently scrape away layers of paint to reveal the paint history. When matching paint samples, it should be noted that the original probably faded in the sun, so research areas that might have been protected to find a color truer to the original.

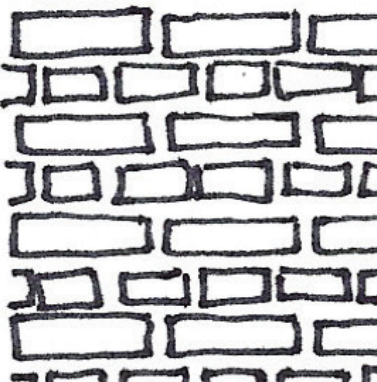
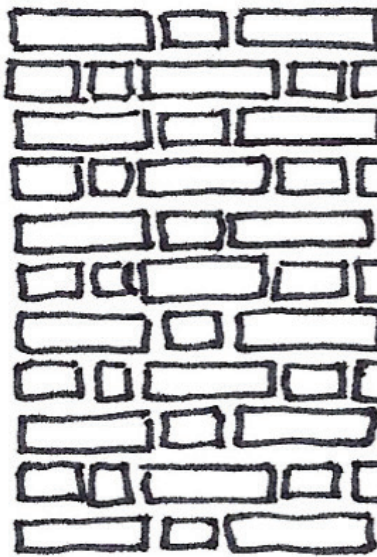
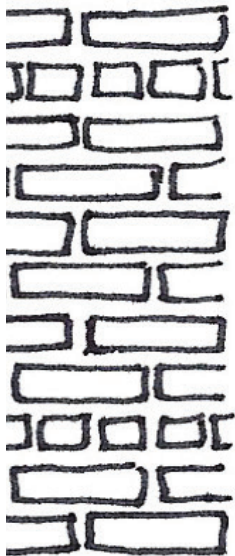
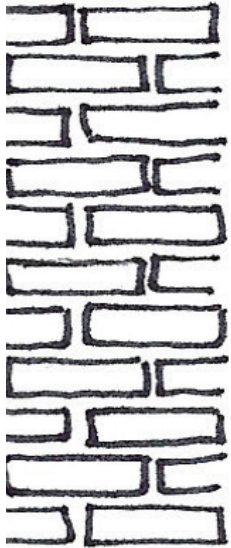
For a compatible color scheme, research the colors that were available at the time your building was built. Most paint manufacturers can provide that information.

Paint colors vary according to the style and period of the building. Stylebooks are available from most paint manufacturers and offer color schemes commonly used and appropriate for the building. For example, color schemes for a Folk Victorian are not appropriate for a Craftsman style house.

Color schemes should tie a building together and create harmony in the façade.

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# SECTION

# 5

## BUILDING MATERIALS





Wood brackets, sometimes known as knee braces, are common decorative support elements on bungalows.



Brackets are sometimes used to support bay windows on bungalow structures.



A shed roof supported by brackets provides shade, decorative detail, and support on a Craftsman structure.



Gable returns highlighted by a multi-color paint scheme are very attractive details on a front gable, gambrel roof structure.

## 5.1 Wood Features

### History and Development

Wood was the most commonly used building material in early Norman neighborhoods. The structural system of most homes is a wood frame-work referred to as balloon framing; this was a Victorian-era building innovation that set up all exterior load-bearing walls and partitions with single vertical studs and nailed the floor joists to those studs. Clapboard, flush siding, board and batten, or textured siding (consisting of patterned wooden shingles) was then applied to the exterior.

Depending on the styles of the era and the taste and financial resources of the owner, decorative details were added. For example, decorative wooden moldings, brackets, pediments, balustrades, and columns embellished early Norman buildings. Porches, fences, and storefronts often were constructed of wood as well.

### Policy

Character defining wooden features and surfaces on a building should be preserved and repaired in a manner that enhances their inherent qualities and maintains as much as possible of their original character.

### Things to Consider as You Plan

- A regular inspection and maintenance program involving caulking and sealing, carpentry, cleaning, and painting will help to keep problems with wooden features and surfaces manageable.
- Flexible sealants and caulking protect wooden joinery from moisture penetration as the wood shrinks and swells. A sound paint film protects wooden surfaces from deterioration due to ultraviolet light and moisture.
- If a wooden feature or surface remains damp for extended periods of time, the possibility of mildew, fungal rot, or insect infestation increases dramatically.

### Maintenance

The following are suggestions for maintaining historic wood surfaces and features:

- Inspect regularly for signs of moisture damage, mildew, and fungal or insect infestation.
- Provide adequate drainage to prevent water from standing on flat, horizontal surfaces and collecting on decorative elements.
- Keep wooden joints properly sealed or caulked to prevent moisture infiltration.
- Treat traditionally unpainted, exposed wooden features with chemical preservatives to prevent or slow their decay and deterioration.
- Retain protective surface coatings, such as paint, to prevent damage from ultraviolet light and moisture.
- Clean painted surfaces regularly by the gentlest means possible and repaint them only when the paint film is damaged or deteriorated.



- Repair historic wooden features using recognized preservation methods for patching, consolidating, splicing, and reinforcing.

**Use Gentle Cleaning Methods.** Clean wooden features and surfaces using gentle methods such as low-pressure washing with detergents and natural bristle brushes. Destructive methods such as sandblasting, power washing, or propane or butane torches are very damaging to old wood, and torches are potential fire hazards. Thermal devices such as electric hot-air guns may be used with care on decorative wooden features, and electric heat plates on flat wooden surfaces. Similarly, chemical paint strippers may be used to augment gentler methods, but the surface must then be neutralized to allow the new paint film to bond.

**Use Preservatives.** The application of wood preservatives or the use of pressure-treated lumber (wood chemically treated with preservatives during manufacture) can also extend the life of wooden elements and surfaces. However, some pressure-treated wood must be allowed to weather for six to twelve months before it is primed and painted.

**Avoid Stripping.** On exterior surfaces do not strip historically painted surfaces down to bare wood and apply clear stains or finishes to create a natural wood appearance.

**A Word of Caution.** Beware that historic structures are likely to have lead paint on painted surfaces both inside and out. For your own protection and the protection of the environment, consult the following website for safe removal practices.

- [https://www.hud.gov/program\\_offices/healthy\\_homes/lbp/hudguidelines](https://www.hud.gov/program_offices/healthy_homes/lbp/hudguidelines)

**Selective Replacement.** Repair or replacement of deteriorated wooden elements or surfaces may involve selective replacement of portions in-kind through splicing or piecing, or it may involve the application of an epoxy wood consolidant to stabilize the deteriorated portion in place. Specifying decay-resistant wood species for replacement of deteriorated wooden elements and surfaces may prevent future deterioration.

**Avoid the Synthetic Siding Trap.** Resurfacing a wooden building with synthetic siding materials, such as aluminum, vinyl, asbestos, and asphalt, is usually a short-sighted solution to a maintenance problem. In fact, these synthetic materials may hide signs of damage or deterioration, preventing early detection and repair. At their best, synthetic sidings conceal the historic fabric of a building. At their worst, synthetic sidings remove or destroy with nail holes the materials and the craftsmanship that reflect Norman's cultural heritage. Synthetic sidings also allow for new rot to go undetected. Because the application of synthetic sidings does grave damage to the character of most historic buildings, it is not appropriate in the historic districts.



Attic ventilation is accomplished through the use of decorative wooden louvers in a roof gable.



The corner of this Prairie-style porch has a wealth of wood details: molded columns, exposed rafter tails, and massive cross beams.



Knee braces support this shed roof over several pairs of windows.

### 5.1.1 Guidelines for Wood

*A full review by the Historic District Commission will take the following criteria into consideration to be issued a Certificate of Appropriateness (COA):*



Wood dormers and full-height porch columns highlight this Neoclassical-style structure, originally a sorority house.



A gablet supported by wood braces provides a decorative covered entrance on this National-style structure in the Miller District.



Decorative collar beams spanning a porch gable are characteristic of high Craftsman style.

- .1 Preserve Original Features.** *Retain and preserve wood features that contribute to the overall historic character of a building, including siding, shingles, cornices, brackets, pediments, columns, balustrades, corner boards and architectural trim.*
- .2 Replace Only Deteriorated Elements.** *Replace only the deteriorated detail or element in-kind rather than the entire feature if replacement of a deteriorated detail or element of a wooden feature is necessary. Match the original detail or element in design, dimension, texture, and material. Consider compatible substitute materials only if using the original material is not technically feasible.*
- .3 Replace Missing Features.** *Replace missing wooden features based on accurate documentation of the missing original or a new design compatible in scale, size, material, and texture, with the style, period, and design of the historic building and the district as a whole. Consider compatible substitute materials only if using the original material is not technically feasible.*
- .4 Avoid False Historical Appearances.** *Features or details should reflect its style, period, and design. Features should not create a false historical appearance by reflecting other time periods, styles, or geographic regions of the country or not original to this historic structure.*
- .5 Rotten Wood.** *Replace rotted wood that is in contact with the ground with a chemically treated wood to prolong the life of the feature. This can be done on skirting and steps. Treated wood can be used to rebuild lattice skirting by cutting strips from standard treated 2 x 4 material.*
- .6 Rough Sawn Wood.** *Avoid using rough sawn wood as is not appropriate for installation in historic buildings.*
- .7 Skirts.** *All solid skirt materials should have vents installed to allow air to pass under the house and eliminate moisture from the wood foundation.*
- .8 Treated Wood.** *All treated wood should be thoroughly dried prior to installation*
- .9 Cleaning.** *Do not use excessive water pressure or sandblasting on wood surfaces as it pits the wood.*
- .10 Defining Features.** *Corner boards and window trim are character defining features on houses with wood siding; they are frequently removed when alternative siding is installed.*



## 5.2 Masonry Features

### History and Development

Brick foundations are quite common in the districts; stone foundations are far less typical. Clay tile roofs and a number of slate roofs distinguish a few early Norman buildings. Although clapboard siding is most typical in residential districts, some brick and stone are also found there.

A variety of historic masonry materials such as brick, terra-cotta, limestone, stucco, slate, concrete, cement block, and clay tile are employed for a range of distinct features including sidewalks, driveways, steps, walls, roofs, foundations, parapets, and cornices.

### Policy

Site features as well as building elements, surfaces, and details executed in masonry materials contribute a great deal of texture to Norman's Historic Districts and should be preserved.

### Things to Consider As You Plan

- Masonry surfaces require minimal maintenance and are known for their durability. They develop a patina over time and should be cleaned only when heavy soiling or stains occur. Gentle cleaning using a low-pressure water wash with detergent and the scrubbing action of a natural bristle brush will usually accomplish the task.

### Maintenance

The following are suggestions for maintaining historic masonry:

- Inspect surfaces and features regularly for signs of moisture damage, vegetation, structural cracks or settlement, deteriorated mortar, and loose or missing masonry units.
- Provide adequate drainage to prevent water from standing on flat, horizontal surfaces, collecting on decorative elements or along foundations and piers, and rising through capillary action.
- Use appropriate repair methods. Repair historic masonry surfaces and features using recognized preservation methods for piecing-in, consolidating, or patching damaged or deteriorated masonry. It is not appropriate to apply a waterproof coating to exposed masonry rather than repair it. The use of clear silicone coatings on masonry surfaces may be appropriate when dealing with water infiltration issues.
- Use only gentle cleaning methods. Clean masonry only when necessary to remove heavy soiling or prevent deterioration. Use the gentlest means possible. Repaint painted masonry surfaces when needed. Test any cleaning technique, including chemical solutions, on an inconspicuous sample area well in advance of the proposed cleaning to evaluate its effects. Sandblasting, high-pressure waterblasting, and power washing are very destructive to historic masonry surfaces and should be avoided.



This Colonial Revival-style masonry structure has a wood porch with a roof line balustrade.



This porch has brick columns with a decoratively curved masonry return wall.



These brick columns on a Craftsman structure also include cast concrete capitals and details.



Stone porch columns are fairly unusual in Norman's Craftsman structures.



This bungalow has a full-width porch with stone cladding and an unusual elliptical opening.



Stucco and half-timbering convey a cottage like atmosphere on this Tudor Revival structure.



This Colonial Revival structure includes a pedimented front porch and rhythmic dormers.



Details such as stucco-clad half columns and planters adorn the Jacobson House.

**Do Not Paint Masonry Surfaces.** The painting of unpainted masonry surfaces is not considered appropriate because it conceals the inherent color and texture and initiates a continuing cycle of paint maintenance. However, the repainting of previously painted masonry is encouraged over attempts to remove the paint films chemically or abrasively.

**Repointing.** Choose mortar for repointing very carefully — Portland cement is not mortar! In a proper repointing, the new mortar will match the visual and physical properties of the original mortar, including its strength. Mortar high in Portland cement content exceeds the strength of historic brickwork and will deteriorate it. The new mortar joint should match the original in width and profile. Moisture damage may also cause a stucco coating to separate from its masonry backing. To repair it, remove any loose or deteriorated stucco and patch the area with new stucco to match the original in composition, texture, color, and strength. Moisture penetration, with subsequent damage to a masonry wall, is often the result of open or deteriorated mortar joints. The wall can be repaired through skillful repointing of the joints with new mortar. Before repointing, any loose or deteriorated mortar must be removed with hand tools, taking care not to chip or damage the surrounding masonry.

**Selective Replacement.** If masonry units themselves are damaged or missing, replacement units should match the original as closely as possible in design, material, dimension, color, texture, and detail. Beyond the individual units, any bond pattern or detailing of the original feature should be duplicated. Given the selection of brick and stone units available today, replacement in kind is generally not an issue. Consequently, substitution of materials or masonry systems such as concrete units for brick or EIFS for traditional stucco is not appropriate for use in historic structures.

### 5.2.1 Standards for Administrative Bypass

*The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, then the application will be forwarded to the Historic District Commission for a full review.*

- **Chimneys.** Primary chimneys are a character-defining masonry feature of historic structures and should be preserved. A non-functional, secondary chimney visible only at the roof may be considered for removal on a case by case basis per Administrative Bypass.

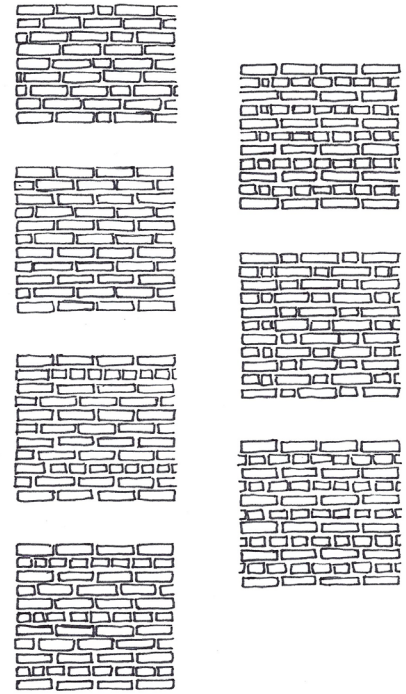


### 5.2.3 Guidelines for Masonry Features

*A full review by the Historic District Commission will take the following criteria into consideration to be issued a Certificate of Appropriateness (COA):*

- .1 Preserve Original Features.** *Retain and preserve masonry features that contribute to the overall historic character of a building, including foundations, chimneys, cornices, steps, piers, columns, lintels, arches, and sills.*
- .2 Preserve Original Materials and Details.** *Retain and preserve historic masonry materials, such as brick, terra-cotta, limestone, granite, stucco, slate, concrete, cement block, and clay tile, and their distinctive construction features.*
- .3 Replace Only Deteriorated Elements.** *If replacement of a deteriorated detail or elements of masonry feature is necessary, replace only the deteriorated in kind rather than replacing the entire feature. Consider compatible substitute materials only if using the original material is not technically feasible.*
- .4 Replace Surfaces Only as Necessary.** *Replace large masonry surfaces in-kind only as necessary, matching the original in design, detail, dimension, color, pattern, texture, and material.*
- .5 Replace Missing Features.** *Replace missing masonry features based on accurate documentation of the missing original or a new design compatible in size, scale, material, and texture with the style, period, and design of the historic building and the district as a whole. Consider compatible substitute materials only if using the original material is not technically feasible.*
- .6 Preserve Unpainted Surfaces.** *Painting unpainted masonry surfaces is prohibited.*
- .7 Chimneys.** *If a chimney, often used as a flue rather than fireplace, is to be removed from the interior of the house, retain the portion above the roofline. A platform will need to be constructed in the attic to carry the weight of the brick.*
- .8 Demolition of Chimneys.** *Chimneys are a character defining feature and should be retained and maintained. If the foundation of the chimney has failed or the chimney is badly deteriorated, the chimney can be carefully dismantled and reconstructed using original materials or materials matching the original. Mortar should match the original in composition and joint profile.*
- .9 Portland Cement.** *At no time shall portland cement be used as the mortar for historic masonry.*

#### COMMON MASONRY BOND PATTERNS:



Bond pattern refers to the design formed by masonry units and mortar joints, and is a character defining feature to be retained.



This Colonial Revival house is decorated by a brick arch and keystone over the front entry.



Colonial Revival style houses are typically constructed of brick.



The Norman Public Library has a brick façade with carved stone ornaments.



When cleaning brick, use the gentlest means possible.



Brick siding is also common in Tudor Revival style construction.

## 5.3 Brick

### Policy

Character defining brick features and surfaces on a building should be preserved and repaired in a manner that enhances their inherent qualities and maintains as much as possible of their original character.

### Things to Consider as you Plan

- Brick walls are constructed by stacking single pieces together to create a pattern. Most wall patterns have a defined horizontal line.
- Several more contemporary houses have a brick veneer siding material over them.
- Brick is also used to create decorative features that should be preserved. These features are usually found around openings on a building, at the top of buildings to create a cornice, or as a detail to add to the horizontal organizations of the building block.
- Brick is typically used for chimney construction and, occasionally, for the construction of foundations.
- Chimney tops are usually constructed with decorative brick detailing or corbel. The mortar in this portion of the chimney is frequently loose or missing due to weather.
- Rough-faced concrete block, which resembles the look of stone, is used as a residential building material for skirt and wall construction.

### 5.3.1 Guidelines for Brick

*A full review by the Historic District Commission will take the following criteria into consideration to be issued a Certificate of Appropriateness (COA):*

- 1 Retain Original Material.** *Retain and maintain the original brick or block material. Installing brick or block where these materials were not originally used is prohibited. Installing brick on the walls of a house that originally had wood siding is prohibited as it changes the character of the house and can destroy the wood beneath.*
- 2 Mortar.** *Replace loose or missing mortar with one of the same composition as the original. Mortar is important to the integrity of the brick wall. If the mortar is missing, its replacement should match the historic mortar in composition, color, and joint width. Use a sand-lime recipe for mortar, which is compatible with the old brick.*
- 3 Detailing.** *It is important to preserve brick detailing because it adds to the character of the building.*
- 4 Chimneys.** *Avoid removing chimneys, rather repair and maintain them.*
- 5 Flashing.** *Repair or replace flashing as needed to ensure a watertight connection between the chimney and roof.*



- .6 Cleaning.** *Historic buildings should be cleaned in the gentlest means possible which typically includes water and soft bristle brushes. Sandblasting and high-pressure washing can cause irreparable damage to brick and are not permissible.*
- .7 Chemicals.** *Any chemical cleaner must be tested in small areas of limited visibility to ensure compatibility and effectiveness on the brick.*
- .8 Cement.** *Modern masonry mortar has cement as a main ingredient, which is too hard for historic brick. A high cement content will trap moisture in the brick and cause it to deteriorate.*
- .9 Paint.** *Brick is a clay material that “breathes”; it does not require paint like its metal or wood counterparts. Some coatings can trap moisture in historic brick causing damage to mortar and interior finishes. Changing the appearance and scale of a brick building by painting it is prohibited.*



It is important to preserve brick detailing since it adds to the character of the building.



The repainting of previously painted masonry is encouraged over attempts to remove the paint films chemically or abrasively.



Avoid removing chimneys, rather repair and maintain them.



## 5.4 Stone

### Policy

Character defining stone features and surfaces on a building should be preserved and repaired in a manner that enhances their inherent qualities and maintains as much as possible of their original character.

### Things to Consider as you Plan

- Stone is used in the construction of commercial buildings, residential houses, foundations, retaining walls / fences, and details.
- Field stone or stone rubble refers to stone that varies in size and has an undefined shape. The uneven face of stone rubble and uneven size of the pieces provide a unique visual appearance.
- Cut stone is a precisely shaped stone, usually with a smooth face. It is frequently used as a decorative element on buildings or as a way to accent an opening. Cut stone can also have a great amount of detail, such as on columns and capitals.
- The stone walls are put together with soft lime mortar in the same way brick walls are. The mortar should not be harder than the stone.
- Stone can be cleaned with a mild solution of soap and water. Sand-blasting and high-pressure washing can cause irreparable damage to stone and are not permissible.
- Another use for stone in Norman can be found in walkways and planter beds.



Front façade with stone rubble.



Cut stone siding.



Contemporary house with stone skirting.



Cut stone piers and columns.



Cut stone siding on Ranch Style house.

### 5.4.1 Guidelines for Stone

*A full review by the Historic District Commission will take the following criteria into consideration to be issued a Certificate of Appropriateness (COA):*

- 1 Replacing Deteriorated Elements.** Replace deteriorated stone with stone that matches the original in color and texture.
- 2 Mortar.** Replace deteriorated or missing mortar with mortar of the same composition as the original in composition and color.
- 3 Portland Cement.** Portland cement, or masons mortar, is too hard and will cause the stone to deteriorate and crumble.
- 4 Foundation.** It is not recommended that stone be added to the foundation or face of a house.
- 5 Drainage.** Retain stone walls and drainage beds.
- 6 Site Design.** Use stone as a site design material for features such as walks, walls, and planter beds.
- 7 Chemicals.** Any chemical cleaner must be tested in small areas of limited visibility to ensure compatibility and effectiveness on the stone. Some chemicals may burn the face of stone.

## 5.5 CMU

### Policy

Character defining CMU features and surfaces on a building should be preserved and repaired in a manner that enhances their inherent qualities and maintains as much as possible of their original character.

### Things to Consider as you Plan

- Concrete masonry units (CMU), “concrete block” or “cinder block,” are both a historic building material and a modern one. It is a masonry material such as brick and stone but of a larger size and material content. The standard size is 8x8x16. It is assembled with the use of mortar.
- Historic concrete block has a rusticated face and was made to imitate stone. It appears as the primary building material on several houses in the historic district and on foundation walls.
- “Smooth” faced concrete block is a common material for commercial buildings as well as modern residential buildings. Modern concrete block is a porous material and is often painted or plastered with a smooth surface.
- Concrete block is often used in landscape construction for walls and columns.



Painted concrete block should remain painted.

### 5.5.1 Guidelines for CMU

*A full review by the Historic District Commission will take the following criteria into consideration to be issued a Certificate of Appropriateness (COA):*

- .1 Retain Original Materials.** *Recognize concrete block as a building material and maintain it.*
- .2 Mortar.** *Replace deteriorated or missing mortar with mortar of the same composition and joint profile.*
- .3 Paint.** *Painted concrete block should remain painted.*
- .4 Landscape.** *Retain and maintain concrete block in landscape features. This may include repairing or reconstructing foundations.*



Stucco siding on a Spanish Revival house with Craftsman style influence.



Asbestos shingle siding.



Stucco siding is commonly used on Tudor Revival style houses.



Stucco siding on Tudor Revival house.



Asbestos shingle siding.

## 5.6 Synthetic Materials / Stucco

### Policy

Character defining features and surfaces of stucco or synthetic materials on a building should be preserved and repaired in a manner that enhances their inherent qualities and maintains as much as possible of their original character.

### Things to Consider as you Plan

- Stucco is not commonly used on houses in historic neighborhoods in Norman. Only a few examples appear to be part of the original style, although stucco has been applied on a few wood-frame and wood-sided houses.
- Stucco should not be used to cover historic building materials due to the damage its application causes to the underlying building material; however, it may be used in new construction.
- As is true in most American cities, synthetic siding materials have been installed over original building materials such as wood siding.
- Asbestos siding, in the shape of shingles, is the oldest synthetic siding material used in residential construction.
- Asbestos shingles are not detrimental to the siding underneath because they breathe and do not trap moisture.
- Aluminum or steel siding followed asbestos as a modern material. Vinyl siding is a common material sold today to cover older wood homes and it can trap moisture when installed over existing wood siding.
- Vinyl and cement fiberboard (Hardieplank) sidings are commonly used in new construction where the substrate is designed differently than traditional construction. Both can trap moisture and cause deterioration.

### 5.6.1 Guidelines for Synthetic Materials / Stucco

*A full review by the Historic District Commission will take the following criteria into consideration to be issued a Certificate of Appropriateness (COA):*

- 1 Retain Original Materials.** *Retain and repair the original building material. Installing any synthetic building material or stucco on top of existing wood is prohibited. Many of these materials can trap moisture in the wall, which will cause the wood beneath to deteriorate. It can also trap moisture in the insulation, which reduces the value of the insulation.*
- 2 Replace Deteriorated Materials.** *Replace only that material which is beyond repair with visually compatible new material. Match the original in profile as closely as possible.*



- .3 Retain Character Defining Features.** *Installing synthetic siding on top of an existing siding as a way of “modernizing” the house or attempting to make the house more energy efficient is prohibited. This changes the character of the original design and frequently destroys the character-defining features of the house and neighborhood.*
- .4 Stucco.** *Stucco is usually a cementitious material that may develop hairline cracks over time. It should be gently washed with low pressure and allowed to dry thoroughly. The application of an elastomeric paint will cover most hairline cracks and provide some flexibility at those locations.*
- .5 Details.** *Such details as corner boards, windows and door surrounds, gable vents and rafter ends are often changed or eliminated when the installation of synthetic materials occur.*
- .6 Cement Fiberboard.** *Cement fiberboard (Hardieplank) and synthetic wood materials are prohibited except for new construction. These are not comparable substitutes for wood siding except in certain applications. A good use of cement board siding is where it is in contact with the ground, such as the skirt of a pier-and-beam house. Be sure to retain ventilation of the crawl space. If using cement board, use smooth only. Wood used in historic houses was smoothly sanded with no obvious grain.*

## 5.7 Metal

### Policy

Character defining metal features and surfaces on a building should be preserved and repaired in a manner that enhances their inherent qualities and maintains as much as possible of their original character.

### Things to Consider as you Plan

- Pressed metal is often thought of as an interior ceiling material but was used for cornices and other details on some of the buildings of Norman. Pressed-metal cornices are constructed over a wooden framework. Deteriorated wood should be replaced to provide adequate support for metal cornices. Damage and deteriorated pressed-metal panels can be fabricated and replaced if necessary.
- Aluminum is more contemporary and was used on buildings dating from the 1930s.
- Miscellaneous steel components can also be found on porch columns and porch structures, railings, turnbuckle supports at canopies, downspouts, etc.
- Metal roofs are commonly installed on odd shapes or projections from the wall of the main house. This is the most common application of standing seam metal.
- Corrugated metal roofing is commonly found on outbuildings such as garages and barns. Other sheet metal roofing materials found are “V” crimp and pre-finished metal with a deep profile.
- Ornamental iron columns have been installed to replace wooden columns on some houses and was a “fashion trend” throughout the United States.



Metal roof replacement on Bungalow.



Metal roof on front porch.



Ornamental iron columns

### 5.7.1 Guidelines for Metal

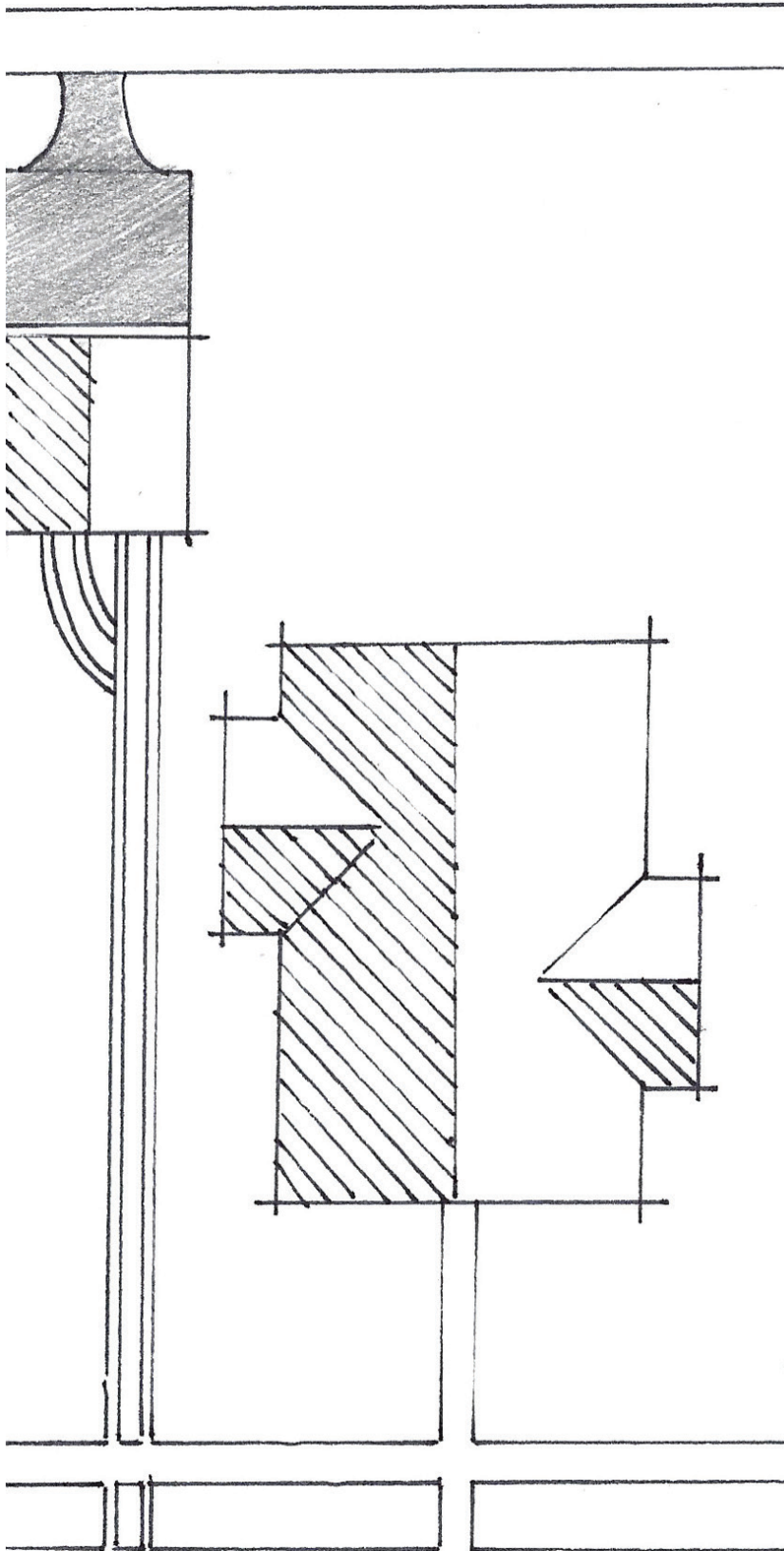
*A full review by the Historic District Commission will take the following criteria into consideration to be issued a Certificate of Appropriateness (COA):*

- 1 Replacing Deteriorated Material.** *Replace deteriorated metal with new primed metal of the same or compatible material. Metal materials should not be used to replace wood or other historic non-metal materials.*
- 2 Aluminum.** *Aluminum should not replace wood as a building material but is used for cornices and other details on many buildings. This is especially true of doors and windows and their frames. If aluminum appears to be the only option as a replacement material for deteriorated wood, the aluminum should be of similar profile and should have a factory painted finish. Mill finish or “shiny” aluminum should not be used on a historic building to replace a previously painted material.*
- 3 Paint.** *It is important to keep pressed metal, cast iron and steel well painted to avoid rust and deterioration.*
- 4 Decorative Details.** *Retain decorative roof details when replacing the primary roofing material.*

- .5 **Roofing.** *Installing an inappropriately scaled metal roofing material on a house that did not have a metal roof originally is not appropriate. Many of the current metal roofs have an industrial appearance and should be avoided.*
- .6 **Decorative Iron.** *Avoid installing decorative iron work over windows that did not include them in the original design.*
- .7 **Pressed Metal.** *Avoid installing a pressed metal skirt where one did not previously exist.*

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# SECTION

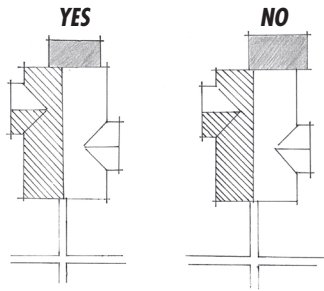
# 6

ADDITIONS  
& NEW  
CONSTRUCTION

## 6.1 Additions to Historic Buildings

Additions shall be defined as construction which increases any exterior dimension of an original structure by building outside of the existing walls and/or roof. Additions can be either horizontal or vertical.

Over the life of a house, its form may evolve as additional space is needed or new family needs are accommodated. Many houses in Norman's historic districts reflect their history through the series of alterations and additions that they exhibit. Such changes become significant to the history of the building and the district.



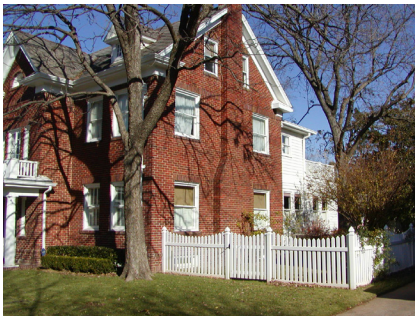
Additions made to the rear of historic structures generally have the least impact on the overall appearance of the house.

### Policy

Additions within the historic districts are appropriate as long as they do not destroy historic features, materials, and spatial relationships that are significant to the original building and site. Further, new additions should be differentiated from the original structure and constructed so that they could conceivably be removed in the future without damage to the original structure.

### Things to Consider As You Plan

- When undertaking historic rehabilitation of houses that include non-contributing additions, owners should consider making the addition more compatible with the historic portion of the house. While modern additions should always remain distinct — in other words, complement, don't copy — owners should consider redesigning additions to complement the historic character of the building rather than detract from it.
- Additions should never compromise the integrity of the original structure or site either directly through destruction of historic features and materials or indirectly through their location, size, height, or scale. Negative impacts of an addition to the original building can be significantly diminished by locating the addition on the least character-defining elevation — typically the rear — and by keeping it smaller than the original structure. Additions should never overpower the original building through height, width, or depth. The overall size, scale, form, design, relationship of openings, and selection of materials, details, colors, and features of proposed new additions will be reviewed in view of compatibility with the original building.
- Although designed to be compatible with the original building, an addition should be discernible from it. For example, it can be differentiated from the original building through a break in roofline, cornice height, wall plane, materials, siding profile, or window type.
- The impact of an addition on the building site must be considered as well. The addition should be designed and located so that significant site features, including mature trees, are not lost.



Additions that provide needed living space can usually be added unobtrusively to the rear of historic structures.

## 6.1.1 Guidelines for Additions to Historic Buildings

*A full review by the Historic District Commission will take the following criteria into consideration to be issued a Certificate of Appropriateness (COA):*

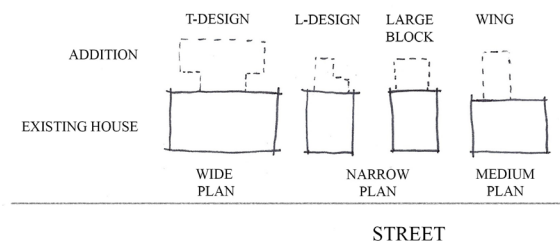
- .1 Make Additions Compatible.** *Additions shall be compatible with the historic building in size, scale, mass, materials, and the pattern of windows and doors to solid walls.*
- .2 Locate Addition Inconspicuously.** *Locate a new addition on an inconspicuous façade of the historic building, usually the rear or side. Additions that alter the front façade are inappropriate for a historic structure.*
- .3 Limit Size and Scale.** *The footprint of the addition shall not exceed 50% of the footprint of the existing structure or 750 square feet, whichever is greater. Exterior dimensions of the addition shall not exceed the exterior dimensions of the existing structure, including height, width, and depth. An addition which does not increase the footprint of the existing structure may be allowed to increase roof height and will be reviewed on a case-by-case basis.*
- .4 Preserve the Site.** *Design new additions so that the overall character of the site, character-defining site features, and trees, are retained.*
- .5 Avoid Detracting from Principal Building.** *Avoid construction of an addition if it will detract from the overall historic character of the principal building and the site, or if it will require the removal of a significant building element or site feature. Construct new additions so that character-defining features of the historic buildings are not destroyed, damaged, or obscured.*
- .6 Differentiate.** *New additions should be easily differentiated from the original historic structure. This can be accomplished by using different but compatible materials or indenting the façade of the addition back from the original structure.*
- .7 Second Floor Additions.** *Depending on design, site orientation, and visibility, creating a second floor in a historic structure can provide much-needed living space that enables long-term habitation. While second story modifications must be fully evaluated by the Commission for their impact on the primary structure and neighboring structures, the addition of a second story that does not change the footprint of the original structure is not considered an addition per se. It is considered a modification and as such may be allowed to violate height restrictions and may be allowed on the back 1/3 of existing house. Applications for such are reviewed on a case-by-case basis.*



A sensitive addition to this Tudor Revival structure maximized living space but remained true to the house's original design.



A common modification to bungalows is the addition of a small second story or "pop up."



Examples of appropriate locations and shapes for additions to historic properties.

- .8 Height.** *Additions should not overpower the original structure. Limit any height increases to the rear half of the building.*
- .9 Dormers.** *Dormers may be necessary to meet egress requirements from second story living spaces and should be compatible in design to original structures. At no time are butterfly dormers appropriate on the front roofline.*



## 6.2 New Primary and Secondary Structures

Infill construction is defined as the erection of a new structure on a vacant lot or the relocation of an existing structure to a vacant lot from another location.

### Policy

Infill construction within a historic district can enhance the existing district character if the proposed design and its siting reflect an understanding of, and a compatibility with, the distinctive character of the district setting and buildings. In fact, the introduction of a compatible contemporary building can add depth and interest to the district. New structures should be compatible with the district.

### Things to Consider As You Plan

**Review Overall Compatibility.** The compatibility of new site development with the district setting depends on its compatibility with characteristic district features as well as the retention of the specific site's topography and character-defining site features. The descriptions and guidelines included in Chapter 4, Site and Setting, should be useful in determining the compatibility of proposed site development within a historic district.

The guidelines for various site features, including driveways, fences, lighting, garages, and plantings, apply to both existing site features and proposed development. Because buildings within the historic districts generally display a clear consistency in setback, orientation, spacing, and distance between adjacent buildings, the compatibility of proposed new construction siting should be reviewed in those terms as well.

**Let Overall District Character Guide You.** The success of new construction within a historic district does not depend on direct duplication of existing building forms, features, materials, and details. Rather, it relies on understanding the distinctive architectural character of the district. Infill buildings must be compatible with that character. Contemporary design generated from such understanding can enrich the architectural continuity of a historic district.

**Look Around for Clues.** In considering the overall compatibility of a proposed structure, its size, scale, height, form, massing, proportion, and roof shape should first be reviewed. A careful analysis of structures surrounding the building site is essential in determining how consistent and significant each of these criteria is. The overall massing and proportion of the building's front elevation is vital to consider because the front façade will have the most impact on the streetscape. For example, if the street façades of neighboring buildings are vertical in proportion, i.e., taller than they are wide, then maintaining the vertical orientation of the building façade will result in a more compatible design.

A similar study of materials, building features, and details typical of existing buildings along the streetscape, block, or square will provide a vocabulary to draw on in designing a compatible building. Beyond the obvious study of prominent building elements such as porches and storefronts,



This new house takes its design cues from Tudor Revival style though is clearly a product of its own time.



This new bungalow complements neighboring structures in materials, form, size and scale.



This new two-car garage is located in the rear and oriented inwards to minimize its visual impact on the primary structure.

particular attention should be given to the spacing, placement, scale, orientation, and size of window and door openings as well as the design of the doors and the windows themselves.

**Doors and Windows are the Eyes of a House.** The appropriate choice of doors and windows is a very important aspect of the architectural character of a house and is important to ensuring a comfortable blend of old and new structures in an historic neighborhood. Doors and windows give the first impression of a structure.

The proportion, shape, location, pattern, size, and material composition of doors and windows contribute significantly to the character of a building and are particularly important in helping identify the style and period of the building. Most early Norman homes were built with true divided light, wood windows, though metal windows were original to a few structures. Therefore, the use of a real wood window is an important detail to consider in making an infill project compatible with its neighbors.

**Choose Compatible Materials.** Compatibility at the building skin level is also critical. The selection of appropriate exterior materials and finishes depends on the compatibility of proposed materials and finishes in composition, scale, module, pattern, texture, color, and sheen. Chapter 5, Building Materials, also provides pertinent information on traditional materials, features, and details found in the historic districts.

**Relocating an Old Building to a New Site in a Historic District.** Moving historic structures is usually undertaken to save them from demolition. Often a significant building that is threatened with demolition or surrounded by an incompatible environment without realistic prospects for adaptive reuse can be relocated into a compatible environment. Relocation can result in multiple benefits: saving the building, enhancing the new environment, and increasing the real estate value of the building.

**Traditional setbacks for primary structures as per City of Norman Zoning Ordinance are:**

- **Front yard:** minimum twenty-five (25) feet. When lot has double frontage, requirements apply for both streets.
- **Side yard:** not less than five (5) feet on each side.
- **Rear yard:** not less than twenty (20) feet or 20% of the depth of the lot, whichever is smaller.

**Traditional setbacks for secondary structures such as garage apartments as per City of Norman Zoning Ordinance are:**

- **Side yard:** minimum five (5) feet.
- **Rear yard:** ten (10) feet from the rear lot line.

## 6.2.2 Guidelines for New Primary and Secondary Structures

*A full review by the Historic District Commission will take the following criteria into consideration to be issued a Certificate of Appropriateness (COA):*

- .1 Consider Historic Context.** *Design new structures to be compatible with historic buildings in the district in terms of size, scale, height, form, massing, proportion, finished floor elevation, size of door and window openings, and roof shape. Proposals for new construction shall include streetscape elevation drawings that depict proposed structure as well as elevations of properties on either side to provide a comparison of massing, scale, and design.*
- .2 Select Doors & Windows Carefully.** *Select doors and windows for new buildings that are compatible in material, proportion, pattern, and detail with the doors and windows of historic buildings in the district. See Sections 3.5 and 3.6 Windows and Door.*
- .3 Select Compatible Finishes.** *Select materials and finishes for proposed new buildings that are compatible with historic materials and finishes found in historic buildings in the district in terms of composition, scale, pattern, detail, texture, and finish.*
- .4 Evaluate Potential for Archaeological Resources.** *Evaluate in advance and limit any disturbance to the site's terrain during construction to minimize the possibility of destroying unknown archaeological resources.*
- .5 Avoid False Historical Appearance.** *New structures should be of their own time period and easily distinguishable from the historic structure.*
- .6 Location of Secondary Structures.** *The appropriate location for a secondary structure should be the rear yard with limited visibility from the street right-of-way. It should be compatible with other accessory buildings on the property, adjacent properties, or the historic district in terms of size, height, scale, and setback patterns.*
- .7 Primary Structures.** *New primary structures should align with the typical setback on the block.*
- .8 Secondary Structures.** *In secondary structures such as cabanas or studios, spacing and size of window and door openings, as well as window to wall proportions should be similar to other historic structures within the block or the historic district.*
- .9 Small Structures.** *108 sq. ft. accessory structures including pergolas, trellises and plastic Rubbermaid storage buildings do not require review, if they are in the rear yard and not visible from the street.*
- .10 Medium Structures.** *109-399 sq ft structures need administrative bypass review, if they are behind the house and not visible from the front.*
- .11 Large Structures.** *Secondary structures over 400 sq. ft. such as garage apartments or large studios for hobbies, craft or art need to be reviewed by the Historic District Commission.*



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# SECTION

# 7

## RELOCATION & DEMOLITION

## 7.1 Relocation of Structures

Relocation is defined as the movement or repositioning of a primary or accessory structure on its original site. Repositioning a building on its original site can provide benefits such as improved site access but it can also result in a loss of integrity of setting and environment, thus compromising the significance of the historic structure itself. Therefore, the decision to relocate a structure must be weighed carefully.

**Relocating an Old Building to a New Site in a Historic District.** Moving historic structures is usually undertaken to save them from demolition. Often a significant building that is threatened with demolition or surrounded by an incompatible environment without realistic prospects for adaptive reuse can be relocated into a compatible environment. Relocation can result in multiple benefits: saving the building, enhancing the new environment, and increasing the real estate value of the building.

### Things to Consider As You Plan

- Because moving structures is complicated, time-consuming, and expensive, it should not be undertaken until every aspect of the project has been considered and evaluated. Both property owners and the Historic District Commission must give full consideration to the architectural and environmental aspects of the situation before addressing the practical problems of moving a structure.

### The following questions are useful for evaluating the architectural and environmental context for such a decision:

- Is the structure threatened with demolition?
- Is relocation the only alternative to demolition?
- Is the structure significant enough architecturally or historically to warrant moving it?
- Is the building structurally sound enough to survive a move and be adapted to its new site?
- If the structure is currently sited in a historic district, what is proposed for the site once the structure is removed?
- Will the move adversely affect the overall character of the historic district or of remaining historic structures?
- Will the move damage significant district site features, such as a tree canopy, either en route or on the site?
- If the proposed site for a relocated structure is in a historic district, does the structure fit into the era of the district; is its style, architectural quality, size, and scale compatible with the district?
- If the proposed site for a relocated structure is not in a historic district, what covenants, if any, will be established to preserve the distinctive character of the relocated structure?
- Is there an appropriate and practical new use for the structure on its new site?

The Historic District Commission must issue a Certificate of Appropriateness for the move before any other necessary permits can be obtained. City staff and the Commission will make every effort to assist the property owner through the process.

### **7.1.1 Standards for Administrative Bypass:**

*The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, then the application will be forwarded to the Historic District Commission for a full review.*

- *Only an accessory structure may be relocated and receive a Certificate Of Appropriateness through administrative bypass.*

### **7.1.2 Guidelines for Relocation of Structures**

*A full review by the Historic District Commission will take the following criteria into consideration to be issued a Certificate of Appropriateness (COA):*

- .1 Document Original Context.** *Before moving a historic structure, applicants and City staff shall document its original setting and context using photographs, site plans, or other graphic or written statements to record the existing site conditions.*
- .2 Protect Existing Structures.** *Ensure that the relocation of a structure will not diminish or damage existing buildings or the overall character of the historic district. Pay particular attention to protection of the tree canopy along the route of the move.*
- .3 Furnish Relocation Site Plans Within District.** *Applicants shall provide the Historic District Commission with detailed site plans for proposed site features and plantings of the new setting, including information on accessory buildings, driveways, site lighting, and parking areas.*
- .4 Protect Significant Features.** *Protect significant site features of the original site, the new site, and the route of the move during the relocation.*

## **7.2 Demolition of Structures**

Demolition of significant structures, sites, objects, or mature trees within Norman's historic districts is strongly discouraged. Given the irreversible nature of demolition, full deliberation of all alternatives before action is essential. The criteria that the Historic District Commission will use for the review of demolitions is included in Section 7(a), 7(b), and 7(c) of the Historic District section of the *City of Norman Zoning Ordinance*.

### **Things to Consider As You Plan**

- In considering a request for a Certificate of Appropriateness to demolish a structure within a historic district, the commission will weigh the impact of the proposed demolition on the overall character of the historic district as well as adjacent historic buildings. This includes contributing structures as well as non-contributing. In addition, the commission will consider whether any specific use for the site has been proposed to mediate the loss of the historic structure.
- In Norman, demolition shall be defined as the removal of any structure from its original site. This includes moving a building from one site to another. If demolition of a historic structure occurs without a Certificate of Appropriateness (COA), property owners will be required to obtain a COA for demolition retroactively before a COA for new construction or any City of Norman building permits will be issued.

### **Recommendations**

- Prior to demolition, consider salvageable architectural features and materials for reuse through deconstruction standards.
- Following demolition, clear site of safety hazards and debris.
- Do not remove mature trees from site.

### **7.2.1 Standards for Administrative Bypass:**

The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass:

- *Demolition of all secondary structures require a Certificate Of Appropriateness.*

### **7.2.2 Guidelines for Demolition of Structures**

A full review by the Historic District Commission will take the following criteria into consideration to be issued a Certificate of Appropriateness (COA):

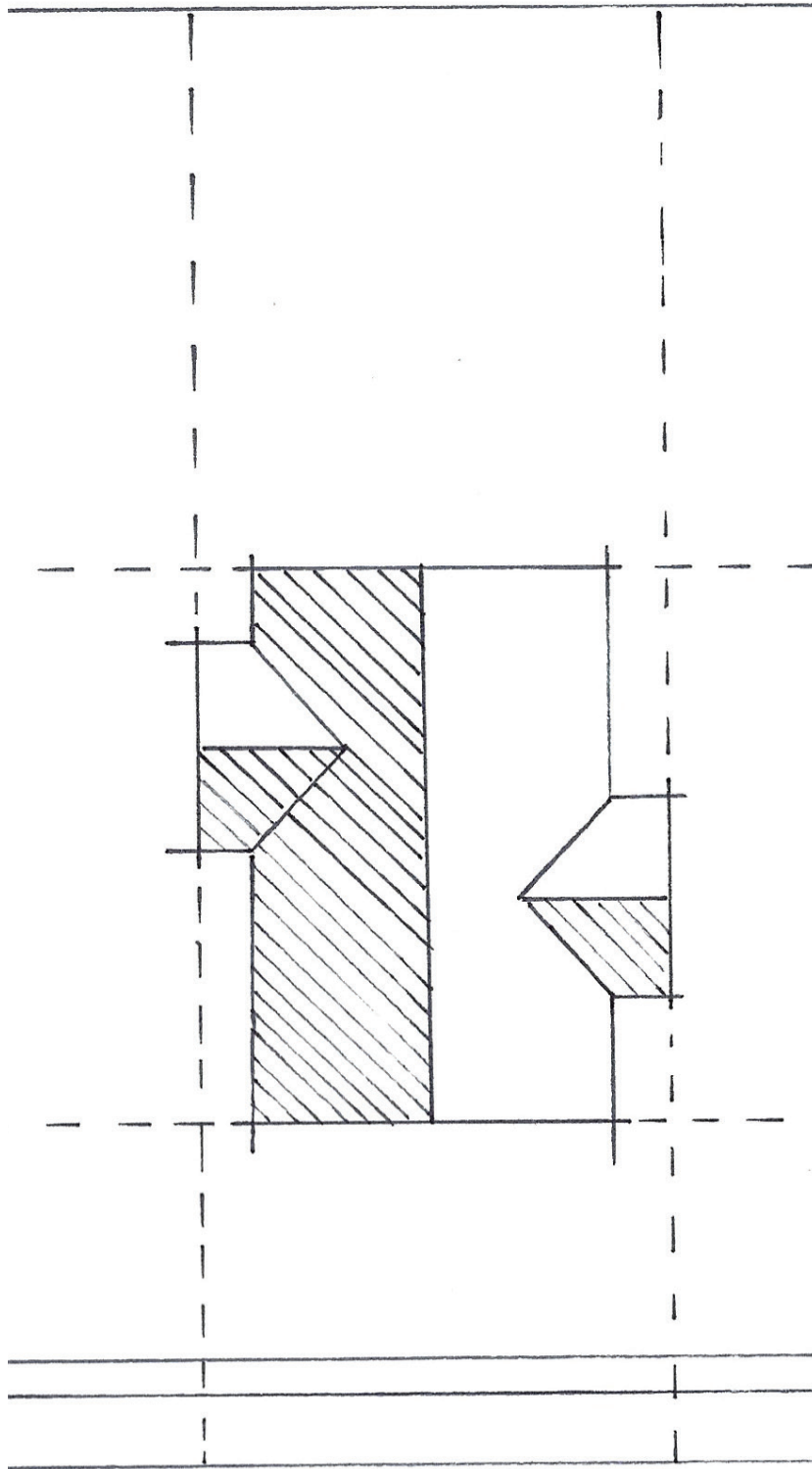
- .1 Submit Site Plan.** *Before demolition occurs, a site plan illustrating any proposed development or introduction of plantings following demolition should be developed and submitted to the commission at the time the request for a Certificate of Appropriateness is made. The documents shall be kept in the commission's files.*
- .2 Document Structure Thoroughly.** *Before demolition, record significant structures through photographs and/or measured drawings as specified by the Historic District Commission and City Staff.*



- .3 Sites To Remain Vacant.** *Sites that will remain vacant after demolition must be properly maintained and free of overgrown vegetation and debris.*
- .4 Engineer Report.** *An applicant must show that unreasonable economic hardship will result from keeping the building, or that the structure has suffered such structural failure that it is determined to be unsafe by building officials or structural engineering report.*
- .5 90 Day Postponement.** *The Historic District Commission may postpone a decision on demolition for up to 90 days in order to allow adequate time for the commission and property owners to explore every alternative to the destruction of the historic resource. After 90 days, the commission may also recommend that City Council enact additional postponement.*
- .6 Additional Postponement.** *If the Historic District Commission recommends additional postponement to the City Council, the City Council shall hold a public hearing to consider additional postponement of demolition. After this hearing, the City Council may approve the demolition or may postpone demolition for an additional period not to exceed 60 days from the date of such order. At the conclusion of this final postponement period, the City Council shall hold another public hearing and may either approve the requested demolition or may disapprove the demolition. In the event demolition is not approved, no demolition shall occur. For purposes of this ordinance, the word "demolition" shall include "removal."*
- .7 Saving Threatened Structures.** *Because the commission and the City Council take the loss of resources in the historic districts and potential historic districts very seriously, use of the delay time is extremely important in reviewing all possibilities for saving a threatened structure.*
- .8 Demolition by Neglect.** *A property owner's failure to properly maintain a historic property can result in its eventual demolition due to the loss of its structural integrity. Such irresponsible treatment of historic structures conflicts directly with the goals of the City in establishing the historic districts.*
- .9 Alternatives to Demolition:**
  - *The owner shall enter into a binding contract for the sale of the property,*
  - *Approved arrangements shall be made for the structure to be moved to an approved new location, or*
  - *The City of Norman shall determine to condemn the property and take it by the power of eminent domain for rehabilitation or reuse by the City or other disposition with appropriate preservation restrictions in order to promote the historic preservation purposes to maintain the structure and protect it from demolition.*
- .10 Replacement Plans.** *Replacement plans shall include project concept, preliminary elevations and master development plans, and completed working drawings for at least the foundation plan which will enable the applicant to receive a permit for foundation construction.*
- .11 Recording Procedures.** *Applicants shall document buildings, objects, sites, and structures, and prepare for the historic preservation officer a salvage strategy for reuse of building materials deemed valuable by the historic preservation officer for other preservation and restoration activities.*



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## **Technical Resources**

### **Local Resources**

#### City of Norman

Planning and Community Development

201 A West Gray Street

<http://www.normanok.gov/your-government/departments/planning-and-community-development>

For information on Norman Historic Districts, certificates of appropriateness, and technical assistance, contact the Historic Preservation Officer at (405)366-5322

### **State Resources**

#### State of Oklahoma Historic Preservation Office

#### Oklahoma Historical Society

800 Nazih Zuhdi Drive

Oklahoma City, OK 73105

<http://www.okhistory.org/shpo>

For Information on historic structures throughout Oklahoma, the National Register of Historic Places, preservation tax credits, and technical restoration assistance, call (405)521-6249.

#### Oklahoma Archaeological Survey

111 E. Chesapeake

Norman, OK 73019

<http://www.ou.edu/archsurvey>

For information on archaeological sites, resource protection, and volunteer opportunities, contact the Survey at (405)325-7211.

### **National Resources**

#### US Department of the Interior

1849 C Street NW

Washington, DC 20240

Office of the Director (202)208-4621

Office of Communications (202)208-6843

Cultural Resource Stewardship and Partnership (202)208-7625

#### Technical Preservation Services

<https://www.nps.gov/tps/>

#### Intermountain Regional Office of the National Park Service

12795 Alameda Parkway

Denver, CO 80225

(303)969-2500

For information on all national park properties and NPS activities in AZ, CO, MT, NM, OK, TX, UT, and WY

## Preservation Glossary

**Architectural Resources** — districts, structures, buildings, monuments, sites, or landscaping which possess local interest or artistic merit, or which are particularly representative of their class or period, or represent achievements in architecture, engineering, or design.

**Addition** — construction that increases any exterior dimension of an original structure by building outside of the existing walls and/or roof. Additions can be either horizontal or vertical.

**Aluminum Siding** — sheets of exterior architectural covering, usually with a colored finish, fabricated of aluminum to approximate the appearance of wooden siding. Aluminum siding was developed in the early 1940s and became increasingly common in the 1950s and the 1960s.

**Alteration** — an act that changes one or more of the exterior architectural features of a structure or its appurtenances, including but not limited to the erection, construction, reconstruction, or removal of any structure or appurtenance.

**Appropriate** — typical of the historic architectural style, compatible with the character of the historic district, and consistent with the *Norman Historic Preservation Handbook*.

**Arcade** — a line of counterthrusting arches supported by columns or piers; a covered walk with a line of arches along one or both sides.

**Arch** — a curved opening in a wall, usually constructed of stone or brick, as in top of a window opening.

**Asbestos Shingle** — a dense, rigid roofing shingle containing a high percentage of asbestos fiber (a non-combustible, flexible fiber able to withstand high temperatures) bonded with Portland Cement known for distinctive patterns.

**Asbestos Siding** — dense, rigid board containing a high proportion of asbestos fibers bonded with Portland cement; resistant to fire, flame, or weathering and having a low resistance to heat flow. It is usually applied as large overlapping shingles. Asbestos siding was applied to many buildings in the 1950s.

**Ashlar Masonry** — masonry composed of rectangular units of stone, generally larger in size than brick and having sawn, dressed, or squared sides laid in mortar.

**Asphalt Siding** — siding manufactured from saturated construction felts (rag, asbestos, or fiberglass) with asphalt and finished with mineral granules on the side exposed to weather. It sometimes displays designs seeking to imitate brick or stone. Asphalt siding was applied to many buildings in the 1950s.

**Attached Structure** — a building that is structurally connected to the primary building on the site.



Historic house with addition



Arched entryway



Asbestos siding



Attic ventilator



Awning



Balustrade along edge of porch



Base of a column



Bay window



Board-and-Batten siding



Box columns

**Attic Ventilator** — in houses, an attic ventilator is a screened or louvered opening, sometimes in decorative shapes, located on gables or soffits.

**Awning** — a rooflike covering of canvas, often adjustable, over a window, a door, etc., to provide protection against sun, rain, and wind. Aluminum awnings were developed in the 1950s.

**Awning Window** — type of window consisting of top-hinged horizontal sash with the bottom edges swinging outward.

**Baluster** — one of a number of short vertical members, often circular in section used to support a stair handrail or a coping, forming a balustrade.

**Balustrade** — a low barrier formed of balusters, or uprights, supporting a railing.

**Band, Band Course, Bandmold, Belt** — flat trim running horizontally in the wall to denote a division in the wall plane or a change in level.

**Bargeboard / Vergeboard** — a board which hangs from the projecting end of a roof, covering the gables, often elaborately carved and ornamented.

**Base** — lower part of a column or pier, wider than the shaft, and resting on a plinth, pedestal or podium.

**Base Course** — a foundation or footing course, as the lowest course in a masonry wall.

**Batten** — a long, flat strip of squared wood or metal used to hold something in place or as a fastening against a wall.

**Bay** — within a structure a regularly repeated spatial element usually defined in plan by beams and their supports, or in elevation by repetition of windows and doors in the building façade.

**Bay Window** — a window forming a recess in a room and projecting outwards from the wall.

**Beaded Board** — a 4" or 6" wide tongue-and-groove wood finish with a milled bead along the centerline and along the edge adjoining the tongues.

**Bearing Wall** — a wall capable of supporting more than its own weight, such as a roof or floor.

**Belvedere** — a pavilion on the roof from where you can enjoy a view.

**Beveled Glass** — glass panes whose edges are ground and polished at a slight angle to create a visual pattern.

**Blank Window** — a window that has been sealed but is still visible; a temporary solution to make a damaged opening airtight.

**Board-And-Batten** — closely applied vertical boards, the joints of which are covered by vertical narrow wooden strips; usually found on Gothic Revival-style buildings.

**Bond** — the laying of bricks or stones regularly in a wall according to a recognized pattern for strength. Masonry bond is essential to brickwork when wire reinforcement is not used.



**Bow Window** — a rounded bay window that projects from the wall.

**Box Column** — a hollow, built-up column constructed of wood, which is rectangular in shape.

**Boxed Eave or Box Cornice** — a hollow cornice, built up of boards, moldings, shingles, etc.

**Bracket** — projecting support members found under eaves or overhangs; may be plain or decorated

**Brick Course / Pattern** — the way in which brick is laid in a building.

**Building** — a more or less enclosed and permanent structure.

**Built-Up Roof** — a roofing system covering a relatively flat roof, consisting of several layers of saturated felt where each layer is mopped with hot tar or asphalt finished with a mineral or rock covering.

**Bulkhead** — base panels just below display windows on storefronts, also referred to as kick plates.

**Caliper** — refers to the diameter of a tree's trunk which is measured with a device that goes by the same name. The caliper is a utensil that looks like the letter "E" with measuring increments on the long arm of the tool.

**Canopy** — a covered area which extends from the wall of a building, protecting an entrance.

**Cantilever** — a support member used to transport the cornice or the extended eaves of a building; a beam or other structural member that protrudes beyond its support wall or column.

**Casement Window** — a window that swings open along its entire length, usually on hinges fixed to the sides of the opening into which it is fitted.

**Casing** — the exposed trim molding, framing, or lining around a door or a window; may be either flat or molded.

**Carved Stone** — rough natural stone shaped by the controlled removal of stone pieces with tools to create decorative detailing.

**Cast Stone** — a mixture of stone chips or fragments, usually embedded in mortar, cement, or plaster, treated to simulate stone; also known as "artificial stone."

**Caulking** — a resilient compound of silicone, bituminous, or rubber base, used to seal cracks and fill joints.

**Cement Siding** — A semi-rigid material made of portland cement, sand, water, and cellulose fibers. Used for exterior siding.

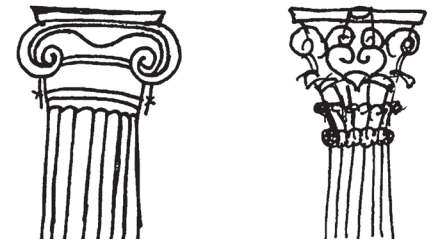
**Certificate Of Appropriateness (COA)** — the official document issued by the Historic District Commission approving any application affecting the exterior of any structure designated by the authority of this Historic District Ordinance for permission to construct, erect, demolish, remove, relocate, reconstruct, restore, or alter said structure.



Brackets



Brick course



Column capitals



Casement windows



Carved stone





Box columns



Cladding



Clipped gable



Columns



Composition shingle roofing

**Certified Local Government** — a program established through the 1980 amendment to the National Historic Preservation Act of 1966 that encourages local government participation in the identification, evaluation, registration and conservation of historic properties within its jurisdiction and promotes the integration of interests and concerns for local conservation to local planning processes and decision making. The CLG program is an association between local governments, the State Historic Preservation Office (SHPO) and the National Park Service.

**Chamfer** — a beveled edge, usually at a 45-degree angle on the edge of a board or masonry surface.

**Cladding** — a finish that covers the exterior wall of a building.

**Clapboard** — horizontal wooden boards, tapered at the upper end and laid so as to cover a portion of a similar board underneath and to be covered by a similar one above. The exposed face of clapboard is usually less than 6 inches wide. This was a common outer face of nineteenth and early twentieth century buildings.

**Classical Order** — a particular style of column with its entablature having standardized details; Greek order includes the Doric, Ionic, and Corinthian and the Roman order includes the Tuscan and Composite.

**Clerestory Window** — an upper window that admits light to the center of a lofty room.

**Clipped Gable** — end of a roof when it is formed into a sharp intermediate between a gable and a hip; also called Jerkin head roof.

**Coffering** — ceiling with deeply recessed panels, often highly ornamented.

**Column** — a vertical shaft or pillar that supports or appears to support a load.

**Capital** — the top or head of a column, usually decorative.

**Combination Hip Roof** — a composition of more than one hipped element at the roof or a combination of hipped and gable roof form.

**Commission** — the Historic District Commission of the City of Norman.

**Compatible** — a design or use that does not conflict with the historical appearance of a building or district and does not require irreversible alteration.

**Composition Board** — a building board, usually intended to resemble clapboard, fabricated from wood or paper fabric under pressure and at an elevated temperature, usually with a binder.

**Composition Shingles** — shingles made from a mixture of binder materials with fibers, also called asphalt shingles.

**Conservation** — the sustained use and appearance of a resource essentially in its existing state.

**Console** — a decorative bracket in the form of a vertical scroll, projecting from a wall to support a cornice, a door, or window head, etc.

**Construction** — all the on-site work done in building or altering structures, from land clearance through completion, including excavation, erection, and the assembly and installation of components and equipment.

**Contemporary** — happening, existing, living, or coming into being during the same period of time. Contemporary denotes characteristics that illustrate that a building, structure, or detail was constructed in the present, rather than being imitative or reflective of a historic design.

**Context** — the setting in which something exists or occurs.

**Contributing Resource** — a historic building or site that retains the essential architectural integrity of its original design or condition.

**Coping** — the cap or the top course of a masonry wall.

**Corbel** — in masonry, a projection, or one of a series of projections, each stepped progressively farther forward with height anchored in a wall, story, column, or chimney.

**Corbelled Chimney Cap** — a brick or stone capping at the top of a chimney that has a series of projections, each stepping out farther than the one below it.

**Corinthian Order** — the most ornate of the classical orders, characterized by a bell-shaped capital with scrolls and acanthus leaves.

**Corner Block** — a block placed at a corner of the casing around a wooden door or window frame, usually treated ornamentally.

**Corner Board** — one of the narrow vertical boards at the corner of a traditional wooden frame building, into which the clapboards abut.

**Cornerstone** — a stone which is located near the base of a corner in a building and displays information recording the dedicatory ceremonies: a foundation stone.

**Cornice** — the top part of an entablature, usually molded and projecting; originally intended to carry the eaves of a roof beyond the outer surface.

**Cresting** — a decorative element located at the top of a parapet or roof ridge.

**Cross Gable** — a gable that is set parallel to the ridge of the roof.

**Cupola** — a small vault on top of a roof; sometimes spherical in shape, sometimes square with a mansard or conical roof.

**Cut Stone** — finished stone block which has been shaped by cutting.

**Damaged or Diseased Tree** — A tree that is damaged in such a way as to create a hazard (e.g. has a large wound) or has been pruned in a way which permanently alters its natural attributes (e.g. topped). A seriously diseased tree is one with obvious signs of internal decay (e.g. cavity with fruiting bodies present), is infested with a disease for which there is no remedy (e.g. Pine Wilt, Dutch Elm Disease), or suffers from a decline disorder.



Corbelled chimney cap



Corner board



Cornice

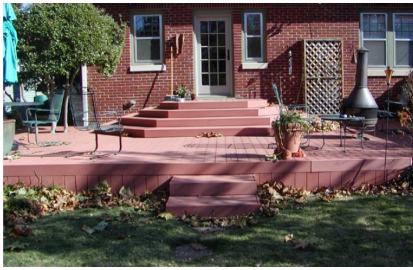


Cross gables



Cut stone





Rear deck



Dentil



Divided light sash



Dormers



Double-hung window

**Deck** — an uncovered porch, usually at the rear of a building; popular in modern residential design.

**Demolition** — the intentional destruction of all or part of a building or structure, may include removal of structural elements, partitions, mechanical equipment, and electrical wiring and fixtures.

**Demolition by Neglect** — the destruction of a structure caused by failure to perform maintenance over a long time period.

**Dentil** — a repetitive cubical element at the base of a classical cornice. Dentils resemble teeth.

**Detached Structure** — a building that is not structurally connected to the primary building on the site.

**Development Pattern** — the configuration of residential lots, the location and orientation of structures on the lots, and the relationship of lots and buildings to the street.

**District** — an area designated by the City of Norman for possessing a significant concentration, linkage, or continuity of sites, buildings, structures, or objects united historically or aesthetically by plan or physical development.

**Divided Light Sash** — a window with glass divided into small pieces.

**Doric Order** — the simplest of the classical orders, sturdy in proportion, with a simple cushion capital.

**Dormer** — a structure containing a window (or windows) that projects through a pitched roof.

**Double-Hung Window** — a window with two sashes that open and close by sliding up and down in a cased frame.

**Double Glazed Window** — a window with an inner and outer pane of glass with an airspace in between.

**Downspout** — a vertical pipe, often of sheet metal, used to conduct water from a roof drain or gutter to the ground or a cistern.

**Drainage Beds** — stone lined ditch used to transport water runoff.

**Drop Siding** — a type of wood cladding characterized by overlapping boards with varying profiles.

**Dropped Ceiling** — a nonstructural ceiling suspended below the overhead structural slab or from the structural elements of a building and not bearing on walls.

**Eave** — the part of a sloping roof that projects beyond a wall.

**Elevation** — a drawing showing the vertical elements of a building, either exterior or interior, as a direct projection to a vertical plane.

**Engaged Column** — a column partially built into the wall, not free-standing.

**Entablature** — in classical architecture, the elaborate beam member carried by the columns.

**Escutcheon** — a protective or ornamental cover plate, attached to a wall with a hook or eye to hold a canopy support or anchor a tie rod.

**Floor Area Ratio (FAR)** — the ratio of the total area under roof to the total contiguous land area of the lot(s) upon which the structures are located. For example, if the total land area is 10,000 square feet and the total area under roof (all structures with a roof without regard to use) is 2,500 square feet. The FAR is  $2,500/10,000 = 0.25$ .

**Fabricated Metal** — any kind of building component manufactured of metal, often decorative in nature and frequently used as columns and railings.

**Façade** — the exterior face of a building.

**Fanlight** — an arched overdoor light whose form and tracery suggest an open fan.

**Fascia** — a flat board with a vertical face that forms the trim along the edge of a flat roof, or along the horizontal, or eave side of a pitched roof. The rain gutter is often mounted on it.

**Feature** — a structural or decorative element that contributes to the overall character of that building, e.g. walls, foundations, roofs, chimneys, steps, piers, columns, lintels, and sills.

**Fenestration** — the windows and doors and the pattern of their openings in a building.

**Finial** — a formal ornament at the top of a canopy, gable, pinnacle, street-light, etc.

**Fixed Lights** — a window or an area of a window which does not open.

**Flashing** — a thin impervious material placed in construction to prevent water penetration, to provide water drainage, or both, especially between a roof and a wall.

**Flat Arch** — an arch that is horizontal or nearly horizontal; also called a jack arch.

**Fluting** — shallow concave grooves running vertically on the shaft of a column.

**Footing** — the portion of the foundation which transfers loads directly to the soil; a widened part of a wall or column at or below the ground to spread the load directly to the soil.

**Foundation** — the supporting portion of a structure below the first-floor construction, or below grade, including footings.

**French Doors** — a pair of doors having top rails, bottom rails, and stiles, with glass panes throughout the entire length.



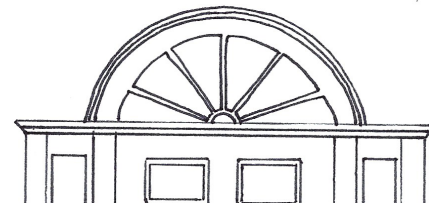
Eaves



Escutcheon plates



Fabricated metal column



Fanlight



Finial





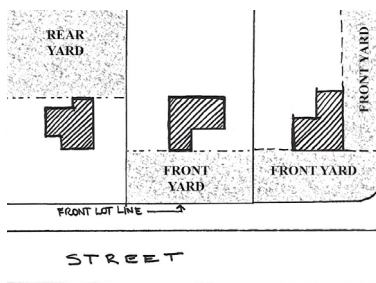
Fixed lights



Fretwork



Front facing gable



Front yard



Gambrel roof

**French Window** — a long window reaching to floor level and opening in two leaves like a pair of doors. Missing a central post/mullion creating one large opening.

**Fretwork** — ornamental wood which is usually carved or turned and installed over doorways and other openings.

**Front Facing Gable** — the end wall of a building with a gable roof that faces the street.

**Front Façade** — the principal face of a building that looks onto a street or open space. In the case of corner lots, both street-facing façades are considered front façades.

**Front Yard** — an open space extending the full width of the lot, the depth of which is the minimum horizontal distance between the front lot line and the nearest line of the main building.

**Gable** — the vertical triangular piece of a wall at the end of a ridged roof, from the level of the eaves to the summit.

**Gable Roof** — a roof that slopes on two sides from the ridge.

**Gambrel Roof** — a gable roof more or less symmetrical, having four inclined surfaces, the pair meeting at the ridge having a shallower pitch.

**Garden Loop Fence** — a woven wire fencing which is distinguished by the loop at the top and mid height.

**Glass Block** — a hollow block of glass, usually translucent and often with textured faces, used for decorative purposes in non load-bearing walls and in sidewalks to permit light transfer to basement floors.

**Glazing** — setting glass in an opening.

**Grade** — the height of the surface of the ground in relationship to a structure (building).

**Guidelines** — An important part of the *Norman Historic Preservation Handbook*. The guidelines are a set of rules administered by the Norman Historic District Commission intended to assist owners of historic buildings in Norman's historic districts maintain, preserve, protect, and enhance the architectural quality of their property.

**Gutter** — a shallow channel of metal or wood set immediately below or built in along the eaves of a building to catch and carry off rainwater.

**Hardscape** — any material which is impervious to water and not covered by roof.

**Header** — a brick laid across the thickness of a wall to bond together different wythes of a wall; the exposed end of a brick.

**Hipped Roof** — a roof without gables, each of whose sides, generally four, lies in a single plane and joins the others at an apex or ridge.

**Historic District** — a geographically definable area with a concentration or linkage of significant sites, buildings, structures, or monuments; (or, an

individual structure, building, site or monument which contributes to the cultural, social, political, or architectural heritage of the City of Norman).

**Historic Preservation Officer** — the chief staff person responsible for historic preservation in the City of Norman's Planning and Community Development Department.

**Historic Property** — any individual structure, building, site or monument which contributes to the historic, architectural, archeological and/or cultural heritage of the City of Norman, Oklahoma as determined by the Historic District Commission.

**Historic Rehabilitation** — the process of returning a historical or architectural resource to a state of efficiency or soundness by repair or alteration designed to encourage its continued use but without noticeably changing the historic exterior appearance of the resource.

**Historic Resources** — sites, districts, structures, buildings, or objects that represent facets of history in the locality, state or nation; places where significant historical or unusual events occurred; places associated with a personality or group important to the past.

**Hood Mold** — a projecting molding over a door or a window.

**Hopper Window** — a window which opens inward and is hinged at the bottom.

**Infill Construction** — the erection of a new structure on a vacant lot or the relocation of an existing structure to a vacant lot from another location.

**In Kind** — the replacement of existing materials or features with materials of identical appearance and/or composition. (See also: matching)

**Ionic** — the classical order of architecture characterized by its capital with large scrolls, less heavy than the Doric and less elaborate than the Corinthian.

**Jamb** — the vertical sides of an opening, usually for a door or a window.

**Jerkin Head Roof** — a roof whose end has been formed into a shape midway between a gable and a hip, resulting in a truncated or clipped "A" appearance; sometimes called clipped gable.

**Joint** — the gap between brick or stone filled by mortar.

**Jalousie Window** — a window consisting of a series of overlapping horizontal glass louvers which pivot simultaneously.

**Keystone** — in masonry, the center piece of an arch, often in contrasting material.

**Landmark** — any building, structure, or place which has a special character or special historical or aesthetic interest or value as part of the development, heritage, or cultural characteristics of a city, state, or nation.



Glass block



Hipped roof



Hood mold



Ionic columns



Keystone





Lattice skirting



Lintel



Louvered vent



Lunette



Marker

**Landscape** — the whole of the exterior environment of a site, district, or region, including landforms, trees and plants, rivers and lakes, and the built environment.

**Lath And Plaster** — a metal mesh or wood strips of metal or wood, used as screening or ornamental construction.

**Lattice** — a network, often diagonal, of interlocking lath or other thin strips used as screening, typically located in the base of a porch.

**Light** — A pane of glass.

**Lintel** — A horizontal member spanning an opening and supporting construction above; a beam.

**Load Bearing Wall** — a wall capable of supporting an imposed load in addition to its own weight. these walls frequently run the full height of a building from foundation to roof.

**Loggia** — an arcaded or colonnaded structure, open on one or more sides.

**Louver** — an assembly of sloping, overlapping blades or slats, fixed or adjustable, designed to admit air and/or light in varying degrees and to exclude rain and snow.

**Lunette** — A semicircular opening.

**Mansard Roof** — a roof with a double slope on all four sides, with the lower slope being much steeper.

**Marker** — a plaque located on or near a historic site, building, structure, or object; usually put in place by a government agency or a private organization.

**Marquee** — a projecting exterior structure placed over the entrance of a building, common for theaters and hotels, that displays the name of the building and/or relative information typically in a large font and surrounded by lights.

**Masonry** — stone, brick, concrete blocks, etc. used to form walls and other parts of a building.

**Materials** — the substance of which something is composed or constructed.

**Mass** — the overall bulk, size, volume, or magnitude of a structure.

**Matching** — in historic rehabilitations, the use of replacement materials that are identical to the original in composition, size, shape, and profile. (See also: in kind).

**Meeting Rail** — either the bottom rail of the top sash or the top rail of the bottom sash; closes the joint completely when the window is shut.

**Molding** — a decorative band having a constant profile or having a pattern in low relief, generally used in cornices or as trim around openings.

**Mortar** — a mixture of Portland cement, lime, putty, and sand in various proportions, used for laying bricks or stones. Until the use of hard

Portland cement became a standard building material, softer lime-clay or lime-sand mortars and masonry cement were common.

**Mosaic** — a pattern formed by inlaying small pieces of stone, glass, tile, or enamel into a cement, mortar, or plaster mix.

**Mullion** — a vertical member dividing a window area and forming part of the window frame.

**Muntin** — a molding forming part of the frame of a window sash and holding one side of a pane.

**National Register of Historic Places** — the list of national districts, sites, buildings, structures, and objects significant in American history, architecture, archeology, engineering and culture, maintained by the Secretary of the Interior under authority of Section 101(a)(1)(A) of the National Historic Preservation Act, as amended.

**New Construction** — see definition for infill construction.

**Niche** — a recessed space in a wall typically semicircular in plan and commonly used for the placement of statuary.

**Non-Contributing** — properties, structures, features or other resources that happen to be located within the recommended historic district boundaries, but which have no relevance to the area's identified significance, significant physical features, or identifying characteristics.

**Oculus** — a round or oval panel or aperture. The aperture may be glazed, open, or louvered.

**One-Over-One Configuration** — a window with a single sheet of glass in the top sash and a single sheet in the bottom sash.

**Orientation** — the relationship of structure to compass points or a site feature such as a street or the direction a façade faces.

**Out Building** — a building detached from the main house or structure but located on the same lot.

**Ordinary Maintenance and Repair** — work meant to remedy damage or deterioration of a structure or its appurtenances, and which will involve no change in materials, dimensions, design, configuration, color, texture or visual appearance to the exterior of an historic structure. Ordinary maintenance and repair shall include painting and reroofing.

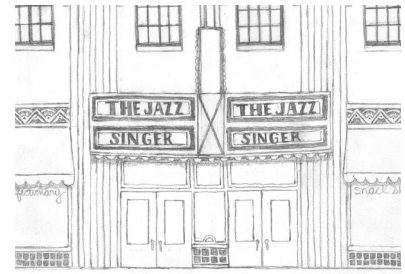
**Palladian Window** — a Classical Revival style window with a center window, often with an arched top and flanked by two rectangular windows.

**Paneled Door** — a wood door comprised of flat and raised panels or pieces.

**Parapet** — an exterior wall which projects above the roof structure.

**Parkways** — the space between the curb and sidewalk, usually green space.

**Parting Strip** — any thin element used to separate two adjoining members.



Marquee



Oculus



One-over-one configuration



Out building



Paneled door





Parapet



Parkways



Pediment



Pilaster



Front porch

**Partition Wall** — dividing wall within a building which may be load bearing or non-load bearing.

**Patio** — an open, outdoor living space adjacent to a building, usually surfaced with stone, tiles, or concrete and at ground level.

**Pediment** — a triangular roof form of a building or as an ornament or hood mold over a door or window.

**Pergola** — an arbor or a passageway of columns supporting a roof of trelliswork on which climbing plants may be trained to grow.

**Pier and Beam** — a foundation system consisting of rows of posts spaced at an appropriate intervals and supporting beams which form a base on which a building is built.

**Pilaster** — a flat or half-round decorative member applied at a wall suggesting a column; sometimes called engaged column.

**Pillars** — a simple, massive, vertical structural support such as a column or post.

**Pinnacle** — a turret or part of a building elevated above the main building.

**Pitch** — the slope of a roof that is not flat or horizontal.

**Pivoted Window** — a window having a sash which rotates about fixed vertical or horizontal pivots, or points, located at or toward the center, in contrast to one hung on hinges along an edge.

**Plaque** — a decorative or commemorative flat plate attached to a wall or surface.

**Plaster** — a paste-like substance of sand, water, and lime installed over another material to provide a finished surface.

**Plinth Block** — a small, slightly projecting block at the bottom of the door trim, extending to the finished floor.

**Porch** — a structure attached to a building to shelter an entrance or to serve as a semi-enclosed space; usually roofed and generally open-sided. It may also be called a veranda.

**Porte Cochere** — a roofed passageway large enough for wheeled vehicles to pass through. Literal definition: a carriage door.

**Portico** — a small entrance porch or covered walk consisting of a roof supported by open columns.

**Portland Cement** — A type of hydraulic cement (one that hardens under water) made by heating a slurry of clay and limestone in a kiln.

**Preservation** — the adaptive use, conservation, protection, reconstruction, rehabilitation, or stabilization of buildings, districts, monuments, sites, or structures significant to the heritage of the people of Norman. The following terms further define types of preservation activities:

- **Adaptive Use** shall mean the restrained alteration of a historical or architectural resource to accommodate uses for which the resource was not originally constructed, but in such a way so as to maintain the general historical and architectural character.
- **Conservation** shall mean the sustained use and appearance of a resource essentially in its existing state.
- **Protection** shall mean the security of a resource as it exists through the establishment of the mechanisms of this section.
- **Reconstruction** shall mean the act or process of duplicating the original structure, building form and materials by means of new construction based on documentation of the historic condition.
- **Rehabilitation** shall mean the act or process of making a compatible use for a property through repair, alterations, and additions while preserving those portions or features which convey its historic, cultural or architectural values.
- **Stabilization** the process of applying methods designated to halt deterioration and to establish the structural stability of an unsafe or deteriorated resource while maintaining the essential form as it presently exists without noticeably changing the exterior appearance of the resource.



Portico



Stabilization

**Pressed Metal** — metal that has been pressed into a decorative shape or pattern.

**Pressed Metal Shingle Roofing** — a roofing unit or shingle which is pressed from sheet metal and frequently has a decorative pattern.

**Prevailing Height** — the most commonly occurring height on a block face on which a project is proposed.

**Prevailing Lot Coverage** — the most commonly occurring lot coverage on the block and across the street.

**Profile** — the outline of a building or an element of that building that is usually shown as a cross section.

**Proportion** — the relationship of the size, shape, and location of one building element to all the other elements, each architectural style typically has its own rules of proportion.

**Purlin** — a piece of timber, board, or metal laid horizontally on the principal rafters of a roof to provide support for the common rafters on which the roof covering is laid.

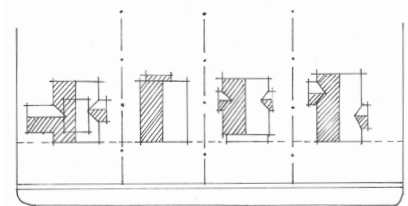
**Quoins** — a large stone or block of brick used to reinforce an external corner or edge of a wall that is often distinguished decoratively from adjacent masonry.

**Rabbet** — a groove cut into one piece of wood to receive the projection or tongue of another

**Rear Yard** — an open space extending the full width of the lot the depth of which is the minimum horizontal distance between the rear lot line and the nearest line of the main building.



Pressed metal shingle roofing

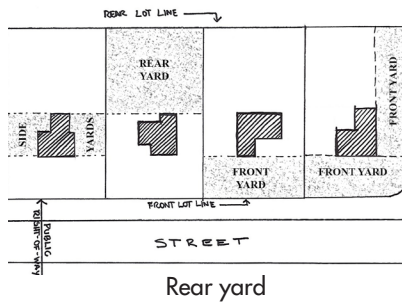


Prevailing lot coverage



Quoins





**Rehabilitation** — the act or the process of making possible a compatible use for a property through repair, alterations, and additions while preserving the portions or the features that convey the property's historical, cultural, or architectural values.

**Relocation** — the movement or repositioning of a primary or accessory structure from its original site.

**Repointing** — raking out deteriorated mortar joints and filling them with a surface mortar to repair the joint.

**Restoration** — the act or the process of accurately depicting the form, features, and character of a property as it appeared at a particular period of time by removing features or changes from other periods in its history and reconstructing missing features from the restoration period.

**Retaining Wall** — a wall, freestanding or laterally braced, that bears against an earth or other fill surface and resists lateral and other forces from the material in contact with the side of the wall.

**Retractable Awning** — a roof-like covering of canvas or rigid material over a window or door that is moveable and can be opened and closed.

**Ribbon Driveway** — a drive providing access between the street and onsite parking that consists of two parallel strips of paving with grass between.

**Ribbon Window** — one of a horizontal series of windows, separated only by mullions, which form a horizontal band across the façade of a building.

**Ridge** — the highest point of a pitched roof.

**Ridgecap** — any covering (such as metal, wood, shingle, etc.) used to cover the ridge of a roof.

**R-Panel Metal Roofing** — a galvanized or painted metal roofing material with ribbed profile used primarily in commercial applications.

**Riser** — the vertical portion of a stair, connecting two steps.

**Roofing Tile** — a tile for roofing, usually of burnt clay; available in many configurations and types including plain, single-lap, and interlocking.

**Rubble** — rough irregular stone which may vary in size, used in wall construction.

**Sash** — the moving part of a window.

**Scale** — the proportion of parts of a building, structure, or monument to one another, to surrounding structures, and to the human figure.

**Score** — the cut of a channel or groove in a material with a hand tool or circular saw to decorate a surface.

**Scupper** — an opening in a wall or parapet that directs water to drain from a roof.

**Secretary of the Interior Standards for Rehabilitation of Historic Buildings** — a set of standards intended to assist the long-term preservation of



Ribbon driveway



Ridgecap



R-panel metal roofing



Roofing tile

a historic property through the preservation of historic building materials and features. The Standards pertain to historic buildings of all materials, construction types, sizes, and occupancy and encompass the exterior and interior of the buildings. “Rehabilitation” is defined as “the process of returning a property to a state of utility, through repair or alteration, which makes possible an efficient contemporary use while still preserving those portions and features of the property which are significant to its historic, architectural, and cultural values.

**Sheet Metal** — a flat, rolled-metal product, rectangular in cross-section and form; when used as roofing material, usually terne- or zinc-plated.

**Shed Roof** — a roof shape sloping in only one plane or direction.

**Shingle** — a roofing unit of wood, asphalt, slate, tile, or other material cut to stock lengths, widths, and thicknesses; used as an exterior covering on roofs and applied in an overlapping fashion.

**Shiplap** — horizontal wood sheathing which butts together. when used on the interior walls it was frequently covered with cheesecloth and wallpaper.

**Sidelight** — a narrow window area beside an outside door, generally seen in Colonial Revival style.

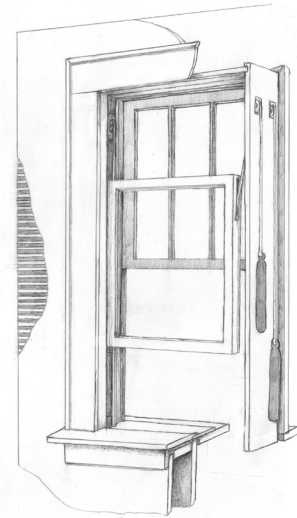
**Siding** — the finish covering of an exterior wall on a frame building.

**Sign / Signage** — a permanent or fixed graphic or display that provides information. It may be freestanding or integrated into the building.

**Significant Trees** — trees which measure twenty-four caliper inches four feet above the ground, or those which are identified with historic personages or important events in local, state, or national history and protected by local ordinance.

**Significant Characteristics** — those characteristics which are important to or expressive of the historic or architectural quality and integrity of the resources and its setting and which include, but are not limited to building material, detail, height, proportion, rhythm, scale, setback, setting, shape, street accessories, and workmanship.

- **Building Mass** — describes the relationship of a building’s height to its width and depth.
- **Building Materials** — the physical characteristics which create the aesthetic and structural appearance of the resource, including but not limited to a consideration of the texture and style of the components and their combinations, such as brick, stone, shingle, wood, concrete, or stucco.
- **Detail** — architectural aspects which, due to particular treatment, draw attention to certain parts or features of a structure.
- **Height** — the vertical dimension of a given structure, building or monument.
- **Proportion** — the relative physical sizes within and between buildings and building components.



Sash



Scale

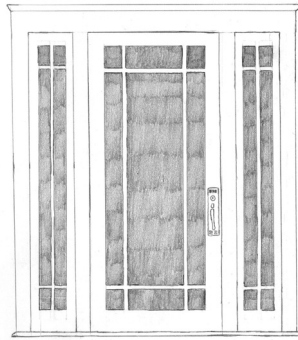


Shingle siding



Shed roof

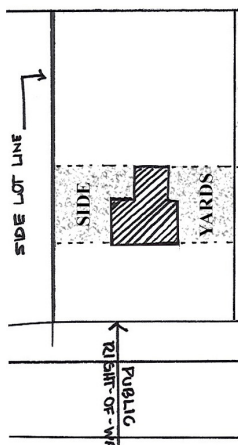




Sidelights



Horizontal wood siding



Side yard



Skirt

- **Rhythm** — a discernible pattern of shapes including, but not limited to, windows, doors, projections, and heights, within a building, structure or monument, or a group of same.
- **Scale** — the proportion of parts of a building, structure, or monument to one another and to the human figure.
- **Setting** — the surrounding structures, monuments, and landscaping which establish the visual, aesthetic, or auditory qualities of the historic or architectural resources.
- **Shape** — the physical configuration of structures or landscaping and their component parts.

**Sill** — the lowest horizontal member in a wall opening.

**Single Hung Window** — a window having a single movable sash.

**Side Yard** — an open space between a main building and the side lot line, extending from the front yard to the rear yard, the width of which is the horizontal distance from the nearest point of the side lot line to the nearest point of the main building.

**Site** — the land on which a building is located. For historic purposes, the location of a significant event, a prehistoric or historic occupation or activity, or a building or structure, whether standing, ruined or vanished, where the location itself maintains a historical or architectural value regardless of the value of any existing structure.

**Skirt** — an element used to cover a foundation or the space between the main house and ground level.

**Slate** — a hard, brittle metamorphic rock that is split into thin sheets for flooring and roofing panels and chalkboards.

**Sliding Windows** — a window which moves horizontally in grooves or between runners.

**Slope** — the amount of degree of incline.

**Soffit** — the exposed undersurface of any overhead component of a building, such as an arch, balcony, beam, cornice, lintel, or vault.

**Sound** — materials and structures that may show wear but retain their original form and function, e.g. sound wood is not rotted.

**Spindles** — one of a series of thin, vertical, round elements of railing often part of a balustrade.

**Spire** — a steep pointed roof form common on church towers.

**Splash Block** — a small masonry block laid on the ground below a downspout to prevent soil erosion.

**Standards** — refers to the *Secretary of the Interior Standards for Rehabilitation*.

**State Historic Preservation Office (SHPO)** — the office within the State of Oklahoma that has been designated by the Governor to administer the historic preservation program in the state.

**State Register of Historic Places** — the State of Oklahoma list of districts, sites, buildings, structures and objects significant in state history, architecture, archeology, engineering and culture, maintained by the State Historic Preservation Officer, under the authority of 53 O.S., 1984 Supplement, Sections 351-355.

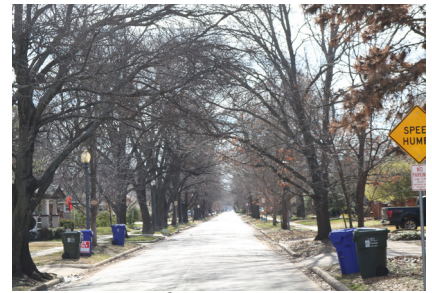
**Standing Seam Metal Roof** — a sheet metal roofing with vertical folded seams running parallel along the slope.



Standing seam metal roof

**Stile and Rail Door** — components of a door; the stiles are the upright structural members and the rails are the horizontal framing members at top, middle, and bottom of the door.

**Street Accessories** — those sidewalk or street fixtures which include, but are not limited to, trash receptacles, benches, signs, lights, hydrants, and landscaping.



Streetscape

**Streetscape** — the view along a street from the perspective of a driver or pedestrian. The streetscape includes street trees, lawns, buildings, landscape buffers, signs, streetlights, above-ground utilities, drainage structures, sidewalks, bus stop shelters and street furniture.

**Stretcher** — a brick or a stone laid with its length parallel to the length of the wall.

**Structure** — anything constructed or erected, the use of which requires permanent location on the ground, or which is attached to something having a permanent location on the ground. These include, but are not limited to, buildings, fences, walls, driveways, sidewalks, and parking areas.



Stucco

**Stucco** — an exterior finish, usually textured, composed of Portland cement, lime, and sand mixed with water. Older-type stucco may be mixed from softer masonry cement rather than Portland cement.

**Style** — a type of architecture distinguished by special characteristics of structure and ornament and often related in time.

**Sympathetic Design** — new work that has an appropriate relationship to the existing historic architecture and character of the surrounding area, based on rhythm, proportion, and scale.

**Surround** — the molded trim around a door or window opening.

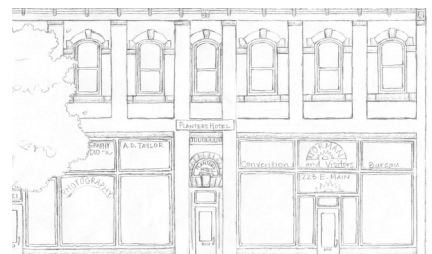


Tapered box column

**Tapered Box Column** — a hollow, built-up column, constructed of wood, which is frequently seen in Craftsman style houses.

**Terra-Cotta** — hard unglazed fired clay, used for ornamental work and roof and floor tile; also fabricated with a decorative glaze and used as a surface finish for buildings in the Art Deco style.

**Terrazo** — a floor finish of stone chips laid in a mortar bed, ground and polished smooth, often with brass dividers, used as a floor surface.



Transom

**Tongue and Groove Lumber** — a joinery system in which boards are milled with a tongue on one side and a groove on the other so that they can be tightly joined with a flush surface alignment.



Turned wood posts



Valley



Veranda

**Tooling** — compressing and shaping the face of a mortar joint.

**Tower** — a portion of a building characterized by its relatively great height in relation to the rest of the structure.

**Transom, or Overdoor Light** — a glazed panel above a door or a storefront, sometimes hinged to be opened for ventilation at ceiling level.

**Trim** — the finish material on a building, such as moldings applied around openings or at the floors and the ceilings of rooms.

**Triple Hung Window** — a window with three vertically sliding sashes that allow the window to open to two-thirds of its height often used for access to porches or balconies.

**Turnbuckle** — a device for connecting and tightening a rod as for a canopy support.

**Turned Wood Baluster** — a decorative picket used to support a handrail, part of a balustrade.

**Turned Wood Post** — a round, wooden support with a decorative profile that has been turned on a lathe.

**Turned Wood Railing** — a railing whose architectural components are turned on a lathe to create a spindle.

**Turret** — a small tower, usually corbelled from a corner.

**Tuscan Order** — A classical order similar to Roman Doric but having columns with an unfluted shaft and a simplified base, capital, and entablature.

**Valley** — the trough or gutter formed by the intersection of two inclined planes of a roof.

**V-Crimp Roofing** — sheet metal roofing which is folded to create a “V” in profile and laps at a “V” joint.

**Veneer** — a thin layer of material applied over a structural backing such as brick, stone, etc.

**Veranda** — a covered porch or balcony, extending along the outside of a building.

**Vernacular** — a building whose form reflects the local influences, materials, and tradition.

**Vestibule** — a small enclosed space between outer and inner doors.

**Vinyl Siding** — sheets of thermal plastic compound made from chloride or vinyl acetates, as well as some plastics made from styrene and other chemicals, usually fabricated to resemble clapboard, sometimes used to cover wood building exteriors.

**Wainscot** — a decorative paneling applied to the lower portion of an inner wall.

**Water Table** — a horizontal exterior band or ledge or projecting molding on a wall, often sloped to prevent water from running down the face of the lower portion.

**Waterblasting** — a cleaning method similar to sandblasting except that water is used as the abrasive. As in sandblasting, high-pressure water jets can damage wood and masonry surfaces. Waterblasting is also known as power washing.

**Welded Wire Fencing** — a welded wire fencing comprised of square or rectangular openings also known locally as “hog wire” or “goat wire.” An acceptable alternative for chainlink fencing in historic neighborhoods.

**Wood Sash Window** — a window where the framework is constructed of wood, may be movable or fixed.

**Wythe** — a vertical section of bricks or other masonry that is one unit thick.



## Brief History of Norman



**April 23, 1889, tents and railway platform.**



**Norman Depot**



**Andrew Kingkade, Norman's S.F. first railroad agent**



**1890-1891, Livery Stable General Store**

The origin of Norman took place in 1872, when the United States Land Office Survey established the boundaries of the future townsite. The town name honors Abner E. Norman, who led the team appointed to survey the Unassigned Lands between 1870 and 1873. His group camped where the town is now situated and the words "Norman's Camp" were burned into a tree.

When the Sooners (those who headed west before the official Land Run date) and other settlers arrived in the heart of Oklahoma, they kept the name "Norman."

More than a decade after, in 1886, the Atchison, Topeka, and Santa Fe Railway Company selected this site for one of its stations. Norman Station was thereby created, and two railroad employees, J.L. Hefley and Andrew Kingkade, subsequently became the area's first legal residents. The following year, the company platted a townsite and filed the plat with the United States Department of the Interior.

The first train rolled through Norman on June 13, 1887, laying the foundation for Norman to flourish into a prominent city. After Norman was thrown open for settlement in the first Oklahoma Land Run, on April 22, 1889, the railroad continued to play a key role in the town's economic development. Norman became a transportation center for cotton, agricultural products and livestock, the region's principal commodities, and served as a shipping center for building supplies in the developing region.

In July 1889 Ed Ingle established the *Norman Transcript*, which continued to report the news at the beginning of the twenty-first century. By 1890, the population stood at 787, and the burgeoning town held doctors, lawyers, hotels, and all the amenities and retail outlets of a community that size, including a cotton gin.

After the passage of the Organic Act in 1890, Cleveland County was organized as county 3 and Norman became the county seat. That same year, High Gate College opened, offering grammar, high school, and college classes. And in December, the Territorial Legislature passed an act to locate The University of Oklahoma (OU) at Norman. Its establishment was pivotal in the urban development of Norman. In 1892 OU held its first classes in rented downtown buildings and that year the first university building was erected.

In 1894 High Gate closed, and its college students transferred to OU. A private sanitarium company purchased the college building, and it evolved into the Oklahoma State Asylum in 1915, later Griffin Memorial Hospital.

By 1900 Norman's population had climbed from 150 to 2,225 and the business community boomed. Within the next two years, the Downtown District contained two banks, two hotels, and a flour mill, among other businesses.

In 1913 the Oklahoma Railway Company extended its interurban service, which ran from Oklahoma City to Moore, south to Norman. After more than 30 years on September 27, 1947, the interurban service came to an end. The demise of the interurban was presaged by the growing popularity of the automobile since the 1920s. In 1923 the Cemetery Road (also called East Road) was paved and became Norman's first intercity highway; it was known after 1925 as the Van Fleet Highway, and in 1955 designated State Highway 77-H.

By the 1920s the OU campus spread over 267 acres and had added several new structures, including Memorial Stadium. The population continued to rise, reaching 9,603 in 1930 and 11,429 in 1940. The sanitarium and university helped the community weather the Great Depression. In 1939 the Tankersley Company built the Cleveland County Courthouse, which was a mixture of Classical Revival and Art Deco elements, and replaced a 1906 Solomon Layton-designed government building.

World War II brought more changes to the city. In 1941, OU, with help from Norman officials, established Max Westheimer Field, a university airstrip, and the next year offered to lease it to the U.S. Navy as a training facility. During the war the airfield became the Naval Flight Training Center, known as north base, and the navy established the Naval Air Technical Training Center (NATTC), known as south base, south of the OU campus. A naval hospital was also established. The north base trained nearly nine thousand men, with the south base training thousands more. In 1946 the navy donated the bases to the university, but in 1952, with the advent of the Korean War, the military utilized the bases in a smaller capacity until 1959. The addition of the government buildings and land helped OU handle the large enrollment increase of the post-World War II era. This also allowed the city to develop, and the 1950 population stood at 27,006.

Norman's proximity and easy access to Oklahoma City contributed to it being a "bedroom" community for employees who worked outside Norman proper. The population increased from 33,412 in 1960 to 52,117 in 1970. In the 1960s the city, through annexations, expanded to 174 square miles, incorporating a large land area in the Lake Thunderbird vicinity. In 1984 the community supported sixty-three manufacturing establishments, which employed 2,562. The population stood at 68,020 in 1980 and climbed to 80,071 in 1990.

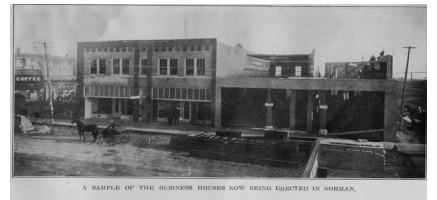
At the beginning of the twenty-first century Norman had 4,270 business establishments engaging a total of 47,665 workers. OU (with more than eight thousand on staff) and Norman Regional Hospital (with more than two thousand) were the two largest employers. In 1944 Norman residents passed bonds to fund the hospital. Several other institutions had extensive work forces, including York International (opened in 1981, after it purchased the defunct Westinghouse air conditioner plant), a U.S. Postal Training Center (1969), Moore-Norman Technology Center (1972), National Oceanic and Atmospheric Administration (NOAA, which dedicated a new laboratory in 1972), Oklahoma Veterans Center (occupied



**Santa Fe Locomotive**



**1898-1902, Agnes Hotel on 110 West Main**



**1908, Businesses being built in Norman**



**1908, Norman Milling and Grain Elevator**

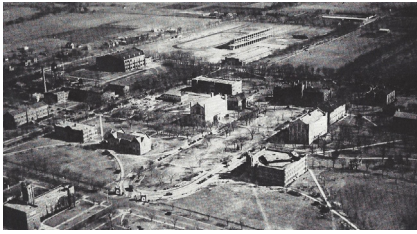


**1908 Residence of John Hardie on Peters Ave.**



**High Gate College**

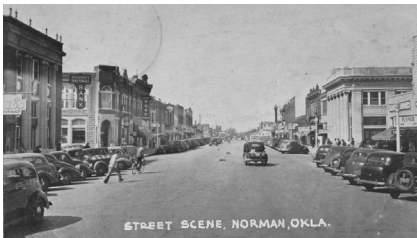




**The University of Oklahoma (OU) campus**



**Naval Air Station designed by Leonard H. Bailey**



**Street scene in the 40s**



**Aerial View of Norman in the 1950s**



**Main street in 1950.**



**Main street today.**

a new building in 1996), Sysco Food Services (1991), Hitachi Computer Products (1987), Saxon Publishers (1981), Yamanouchi Pharma Technologies (2001), and Shaklee Corporation (1978).

By 2000 the population stood at 95,694. The Norman School District enrolled 12,596 students, and several other school districts (Little Axe, Robin Hill, and Cleveland County) came within the city's borders. The city offered several attractions, including the Fred Jones Jr. Museum of Art, the Sam Noble Oklahoma Museum of Natural History, the Jacobson House Native Art Center, the Firehouse Art Center, and other theaters and museums.

Seventeen properties were listed in the National Register of Historic Places. These included the Cleveland County Courthouse, the DeBarr Historic District, the Oscar Jacobson House, the Norman Historic District, the Norman Public Library, the Santa Fe Depot, the United States Post Office, and the Moore-Lindsay House, which also served as the Norman and Cleveland County Museum. The University of Oklahoma's Bizzell Library is a National Historic Landmark. Several festivals, including the Medieval Fair, Jazz in June, and 89er's Day Festival, are annually held in Norman. The city of Norman had experienced a large population growth after 2000, registering 110,925 residents in the 2010 census.

Today, OU and the City of Norman are still making history. The Norman campus has an enrollment of approximately 22,000. And Norman was recently recognized as one of the most progressive cities in the state and the Norman Public School System was acknowledged as the top school system in Oklahoma. Currently, Norman is involved in a downtown revitalization project as well as a project that will guide Norman and its citizens into the 21st century. The Norman 2020 plan was designed to address future population growth and infrastructure problems and offer solutions to solve these problems before they occur.

#### Sources:

1. Larry O'Dell, "Norman," The Encyclopedia of Oklahoma History and Culture, [www.okhistory.org/publications/enc/entry.php?entry-NO006](http://www.okhistory.org/publications/enc/entry.php?entry-NO006)
2. (1987-1988), Architectural/Historic Survey of Norman, Oklahoma. University of Oklahoma, College of Architecture Design/Research.
3. "About the City," City of Norman, Building an Inclusive Community. <https://www.normanok.gov/content/about-city>.

## History of Norman's Historic Districts

### 8.5.1 Chautauqua Historic District

#### **Location**

Norman's Chautauqua Historic District is located one block west of The University of Oklahoma campus in central Norman. Chautauqua is a tree-lined, residential neighborhood built primarily between the years 1903 and 1940. The district includes properties facing Chautauqua and Lahoma Avenues between Symmes Street on the north and Brooks Street on the south.

#### **Early History and Prominence**

Chautauqua District's architecture and environment represent a unique time period in Norman's history. Stately residences lining the streets reflect the status of the university deans and faculty and other prominent individuals who helped shape early development of the city. The mature trees lining Lahoma and Chautauqua Avenues reveal early settlers' commitment to turn a town on the prairie into a leafy burg.

By the end of World War I, Norman was firmly established and The University of Oklahoma was growing apace. Acceleration in Chautauqua's development was tied closely to the growth of the university, which grew nearly eight-fold between 1911 and 1931. During the 1920s, farmland on the west side of campus began being platted and Chautauqua became the neighborhood of choice for faculty. At one time, the 500 block of Chautauqua Avenue was known as "Dean's Row," with five college deans living practically side by side.

#### **Design**

Architecturally, Chautauqua is very eclectic. This eight-block district includes almost every architectural style prevalent during the first quarter of the 20th century. Bungalows are most prominently represented; however, Tudor Revival and Minimal Traditional are also quite prevalent. The district also includes fine examples of Prairie, Colonial Revival, Spanish Eclectic, Neoclassical Revival, and even one example of Queen Anne style.

#### **Historical Significance and Designation**

In 1988, the Chautauqua neighborhood was one of six Norman neighborhoods surveyed by The University of Oklahoma for historical significance. The original survey included nearly thirty blocks that were determined eligible for listing in the National Register of Historic Places.

After numerous public discussions over a two-year period, the original 30-block district was drawn ever smaller until 80% of the property owners in the area agreed to the district designation. Today, the Chautauqua Historic District includes eight of those thirty blocks and represents the heart of the neighborhood. The Chautauqua District is considered significant for its architectural merits and includes around 370 structures.



*The Chautauqua Historic District is an eight block area that includes around 370 structures.*



**The Miller Historic District includes 14 blocks and approximately 235 structures.**



**Miller Historic District is identified by a distinctive gateway: "the Miller Rock" at the confluence of Classen Blvd. and Miller Avenue.**

### **National Register Listing in Chautauqua**

Chautauqua District includes an individual house listing in the National Register of Historic Places. The Oscar B. Jacobson House (NR 1986), located at 609 Chautauqua, was constructed in 1921. A simplified yet elegant example of Italian Renaissance Revival style, its one-story configuration is unusual. Its features include a flat roof, a stuccoed exterior, a recessed entry, widely overhanging eaves, and the use of clay roof tiles. The structure is now home to the Jacobson House Native Art Center.

## **Miller Historic District**

### **Location and Platting**

Bounded by Symmes, Classen Boulevard, Miller Lane, and a line just south of Emelyn Street, the fourteen-block Miller Historic District was dedicated as Norman's second local Historic District in 1997. The Miller Historic District has an unusual form for cities of the Great Plains: the westernmost blocks of the district parallel the railroad tracks; the remaining blocks follow the cardinal points of the compass, a pattern that came to dominate the later development of Norman. These juxtaposed orientations create an intriguing collection of lot shapes and sizes. Overall, the Miller District forms a distinct triangle in the heart of Norman.

### **Early History and Prominence**

On February 26, 1903, the *Norman Transcript* declared "there is no room for argument on the proposition that the Classen-Miller addition to Norman, which will be placed on the market next week, offers some of the finest residential lots in the city." For several weeks, *The Transcript* ran full-page ads expounding the virtues of the Classen-Miller area. It was noted for its proximity to the city's business district, its convenient access to the railroad, and its closeness to The University of Oklahoma. The area was well drained, the streets were graded, and trees had been planted. "An ideal place for a home," the *Norman Transcript* proclaimed. Lot prices ranged from \$30 to \$75.

Though construction began immediately after the Classen-Miller addition opened, it was not until after World War I that the neighborhood began to be fully developed. During the 1920s, Classen-Miller began developing as an exclusive neighborhood for university faculty and Norman business leaders.

### **Design**

Nearly half the structures in the Miller District are classified as Bungalow/Craftsman, the comfortable, down-to-earth American style that flourished from coast to coast for the first four decades of the 20th Century. The neighborhood also includes a fine collection of Minimal Traditional, Colonial Revival, National Folk, and Tudor Revival style structures. The Miller District includes approximately 235 structures.

**Historical Significance and Designation**

The historical significance of the Miller Historic District is two-fold. The neighborhood played a significant role in the urban development of the city, and it is architecturally significant for its eclectic collection of residential architecture built between 1910 and 1938. An estimated 95 percent of neighborhood structures built between 1910 and 1938 remain standing, and approximately 90 percent of these retain their architectural integrity. The Miller District's period of significance is 1903-1949. In 2003, the Miller District was determined to be eligible for the National Register of Historic Places.

The Classen-Miller neighborhood was one of six Norman neighborhoods surveyed in 1988 by The University of Oklahoma, though it did not become a historic district until 1997. Concerned about encroachment from neighboring industrial and commercial uses, Miller residents organized themselves and quickly gained support from a clear majority of property owners to become a local historic district.

The area has really experienced few significant changes since 1938, so initial survey boundaries were similar, though not identical to the original plat of the Classen-Miller Addition. Like the Chautauqua District before it, the final boundaries of the Miller Historic District encompass what is considered the heart of the neighborhood.



## Southridge Historic District

### Location

Southridge Historic District is located directly south of the Classen-Miller District and encompasses an area roughly bounded by Macy Street on the north, Shawnee Street on the south, Classen Boulevard on the west and Oklahoma Avenue on the east.

### Early History and Prominence

Taking advantage of Norman's 1920's population boom, the Miller family opened a new residential addition, the Southridge Addition, in October 1922.

The Southridge Historic District is comprised of 156 properties and Earl Sneed Park. The Southridge District was platted in 1922 with the majority of development occurring between the 1920s and 1950. This tree lined neighborhood is located eleven blocks south of downtown district and three blocks east of the university.

Convenient to The University of Oklahoma and downtown business district, the Southridge District attracted many notable citizens of Norman and The University of Oklahoma. Today Southridge continues to be a vibrant residential neighborhood with charming historic character.

Its largest decade of growth was between 1931-1940 with the construction of approximately sixty-seven buildings. The advent of World War II escalated the demand for housing in Norman as military students, frequently with their families, came in droves to attend the Naval Training School and subsequently the Naval Air Station.

### Design

The dominant architectural styles in Southridge District are Tudor Revival and Colonial Revival, which were popular in the 1920s and 1930s across Oklahoma. There are nine blocks in this district, covering an area of approximately 39 acres.

### Historical Significance and Designation

The Southridge Historic District was established on October 11, 2016, and expanded on June 26, 2018.

## Maps of Norman's Historic Districts

### 8.6.1 Chautauqua Historic District



## Chautauqua Historic District

Adopted October 24, 1995  
Ordinance #O - 9596 - 9

Expanded August 14, 2018  
Ordinance #O - 1819-1

- 330 Structures Contributing
- 40 Structures Non-Contributing

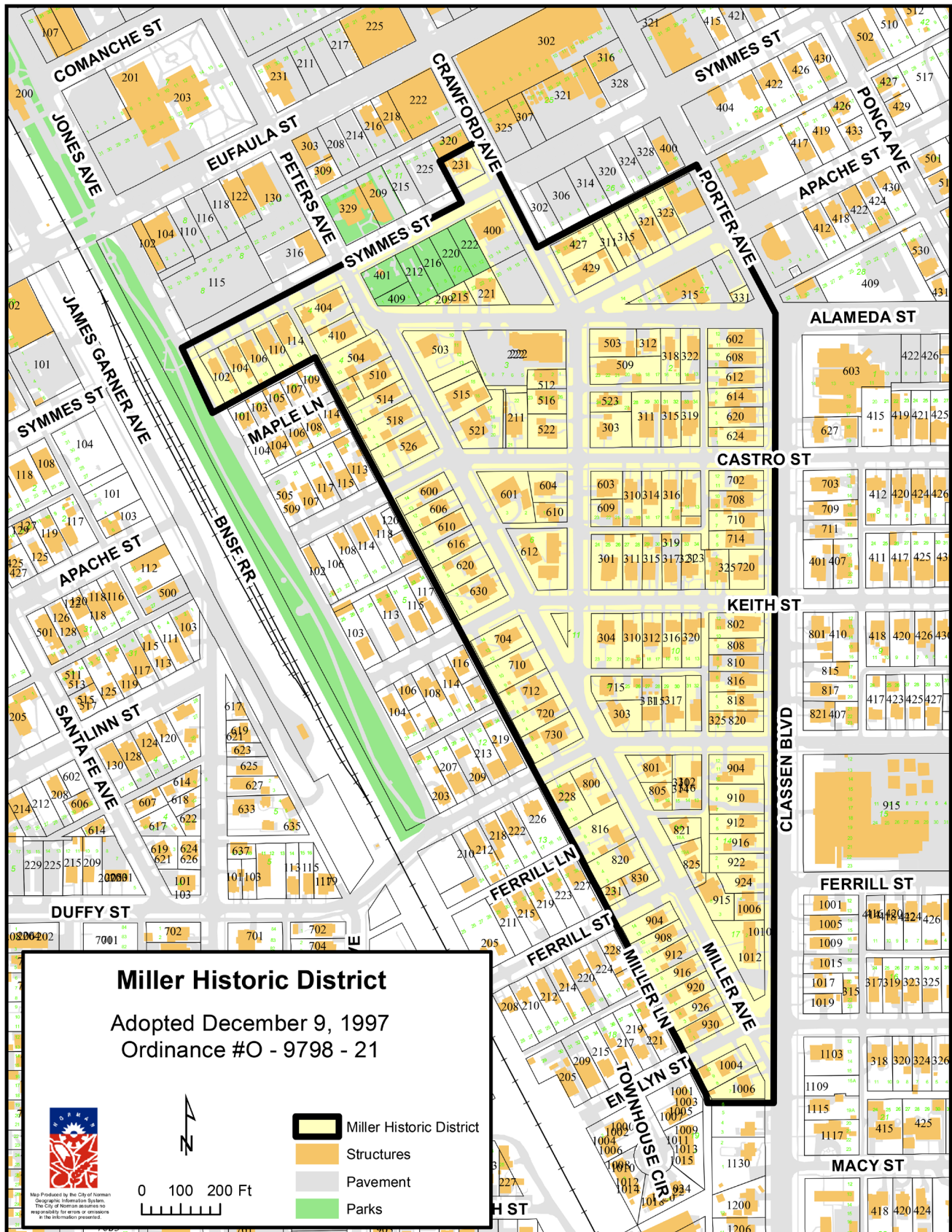
- Structures
- Pavement
- Parks
- Schools
- 100yr. Floodplain
- Floodway



Map Produced by the City of Norman  
Geographic Information System.  
The City of Norman assumes no  
responsibility for errors or omissions  
in the information presented.



# Miller Historic District



# Southridge Historic District



## Southridge Historic District

Adopted October 11, 2016  
Ordinance #O - 1617 - 8

Expanded June 26, 2018  
Ordinance #O - 1718 - 48

- 134 Structures Contributing
- 90 Structures Non-Contributing
- 1 Structure Warrants Further Study

- Southridge Historic District
- Structures
- Pavement
- Parks
- Schools
- 100yr. Floodplain
- Floodway



0 50 100 Feet



Map Produced by the City of Norman  
Geographic Information System  
The City of Norman assumes no  
responsibility for errors or omissions  
in the information presented.



## Prominent Architectural Styles in Norman's Historic Districts



***Craftsman Bungalow***

The Miller, Chautauqua, and Southridge Historic Districts each boast a fine array of residential architecture from the first half of the 20th century. With buildings that date from around 1903 through 1945, these districts illustrate the evolution of vernacular residential architecture in Oklahoma from the dawn of the 20th century through the end of World War II.

Most, though not all, structures in the Miller, Chautauqua and Southridge Districts fit well into well-known architectural categories. On the following pages are brief descriptions of the most prevalent styles found throughout Norman's designated Historic Districts.



***Tudor Revival***



***Colonial Revival***

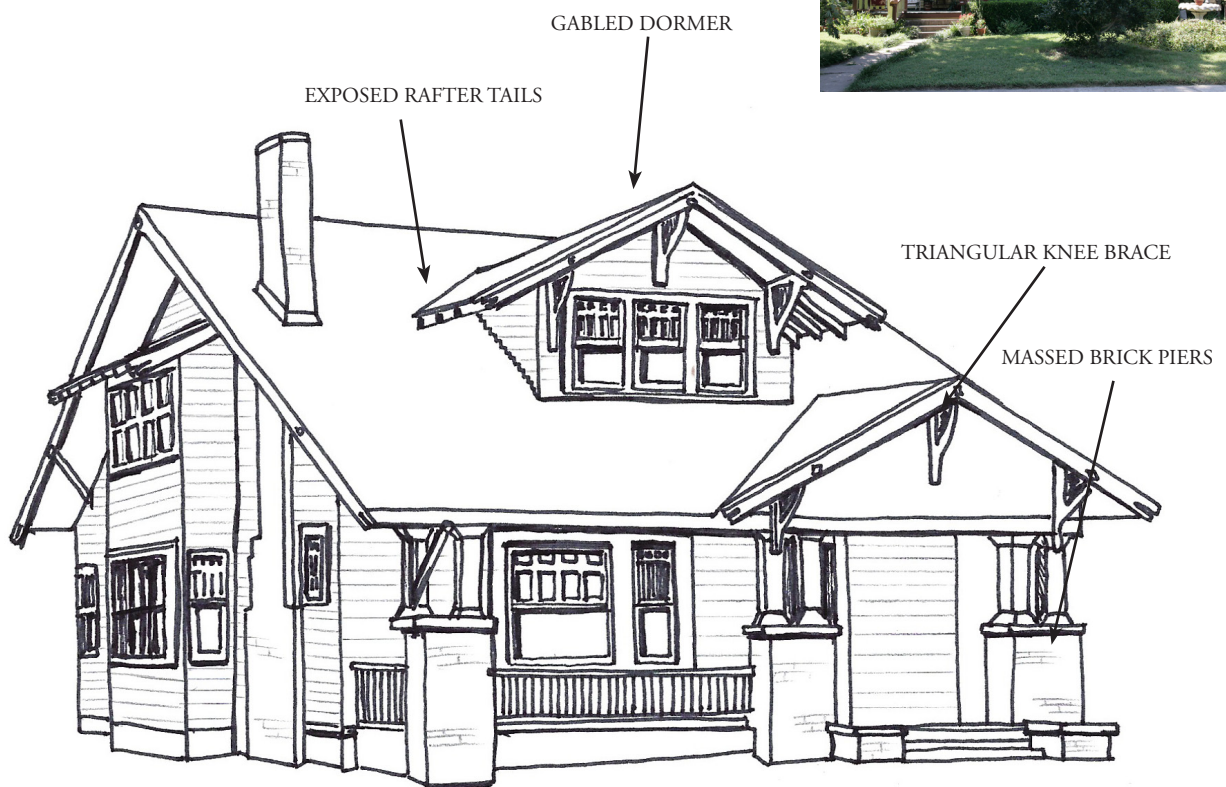
## Craftsman Style

Craftsman style originated in Southern California and spread like wildfire across America through magazines and catalogues. A complete departure from the formal Victorian styles of the previous era, Craftsman houses offered an open floor plan which drew its inspiration from the English Arts and Crafts movement.

The Craftsman style differs slightly from the Bungalow, though both styles share characteristics.

### Characteristics:

- Exposed rafter tails;
- Triangular knee braces under the eaves;
- Massed brick, stone, or stuccoed piers;
- Wooden porch columns;
- Often have two stories;
- Wide, wooden cornice boards;
- Wooden belt courses dividing the upper floors from the lower;
- Large, gabled dormers, and intersecting gabled roofs;
- Natural or local materials such as stone, or heavily applied stucco.





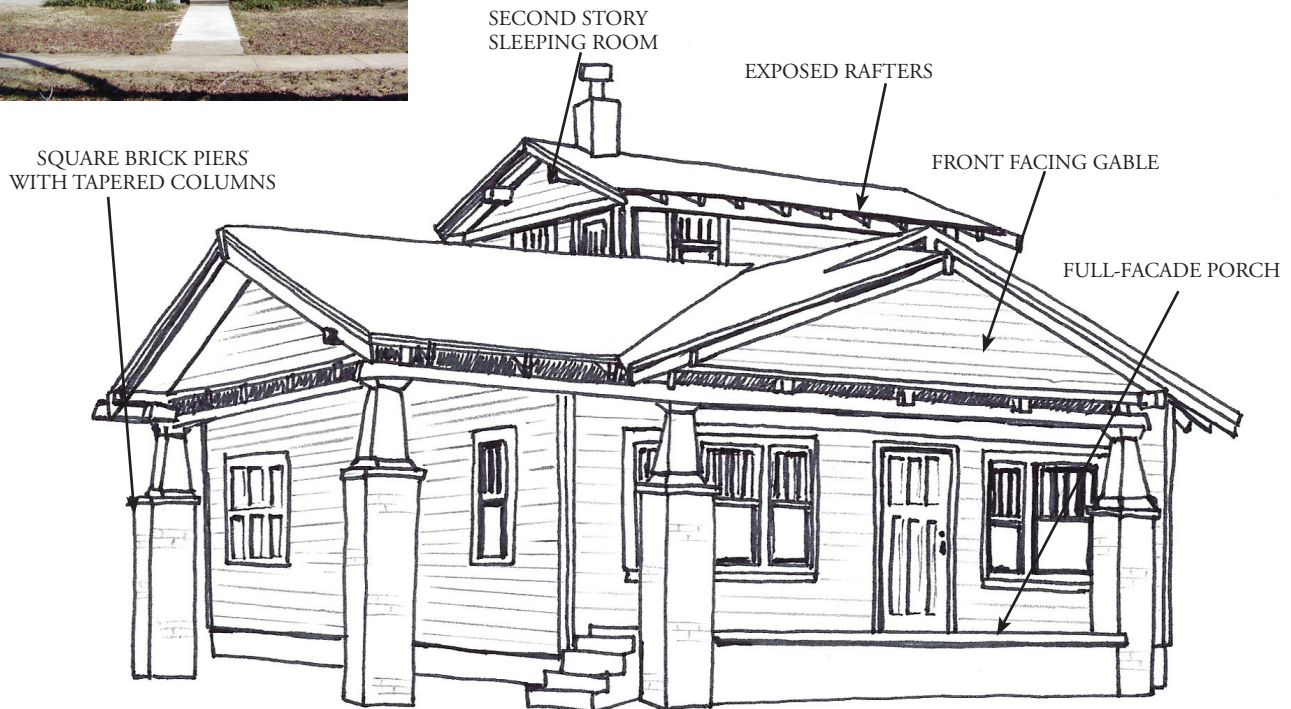
## Bungalow Style



One-story Craftsman-style houses are often referred to as Bungalows. Throughout the country, these structures were ubiquitous between 1900 through the 1940. They were economical to build, easy to live in, and could be easily expanded as family size grew. The presence of Bungalows provides a strong sense of design continuity throughout both Miller and Chautauqua Districts.

### Characteristics:

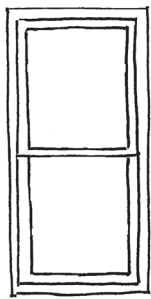
- One-story with a front-facing gable roof;
- Full-façade porch;
- Exposed rafter tails;
- Triangular knee braces;
- Square brick supporting piers capped with concrete and surmounted by tapered wooden columns;
- An important subtype is the Airplane Bungalow. These are constructed with a centrally placed second-story sleeping room;
- Earlier Bungalows tend to have ornamental concrete block foundations while later Bungalows usually have brick or poured concrete foundations.



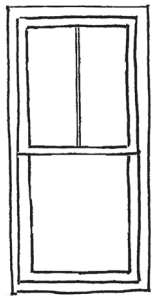
*An example of an Airplane Bungalow*

## Appropriate Elements for the Architectural Style:

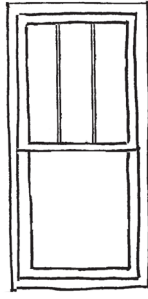
### Windows



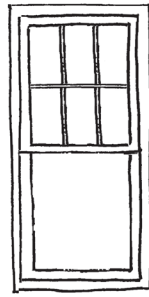
1 OVER 1  
DOUBLE HUNG



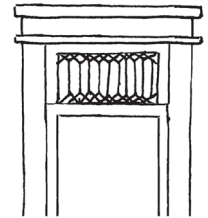
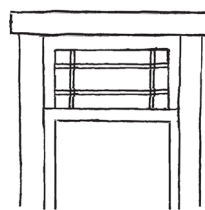
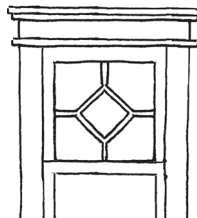
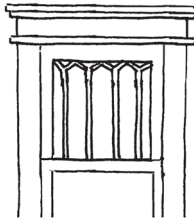
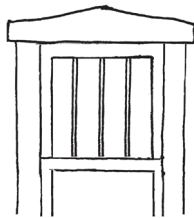
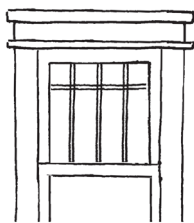
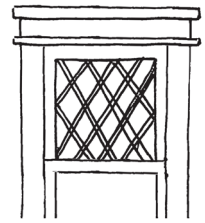
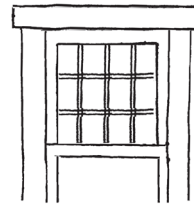
2 OVER 1  
DOUBLE HUNG



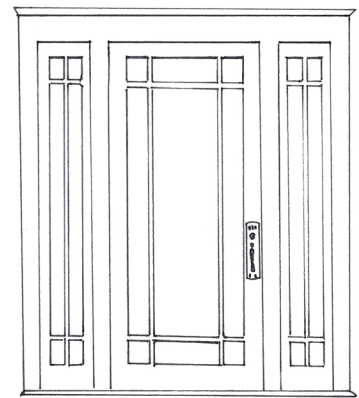
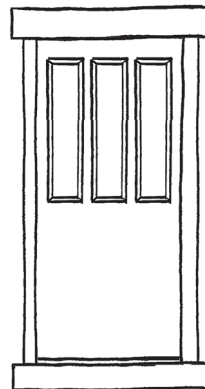
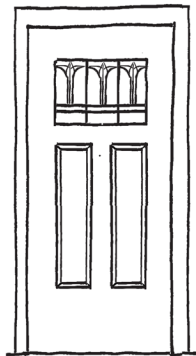
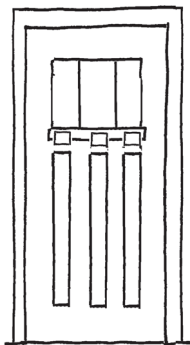
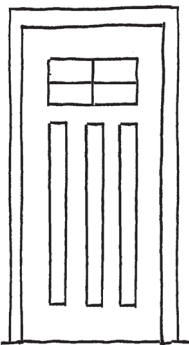
3 OVER 1  
DOUBLE HUNG



6 OVER 1  
DOUBLE HUNG



### Doors And Entrances



### Light Fixtures



\*ALL ELEMENTS FOUND IN BOTH CRAFTSMAN AND BUNGALOW STYLES



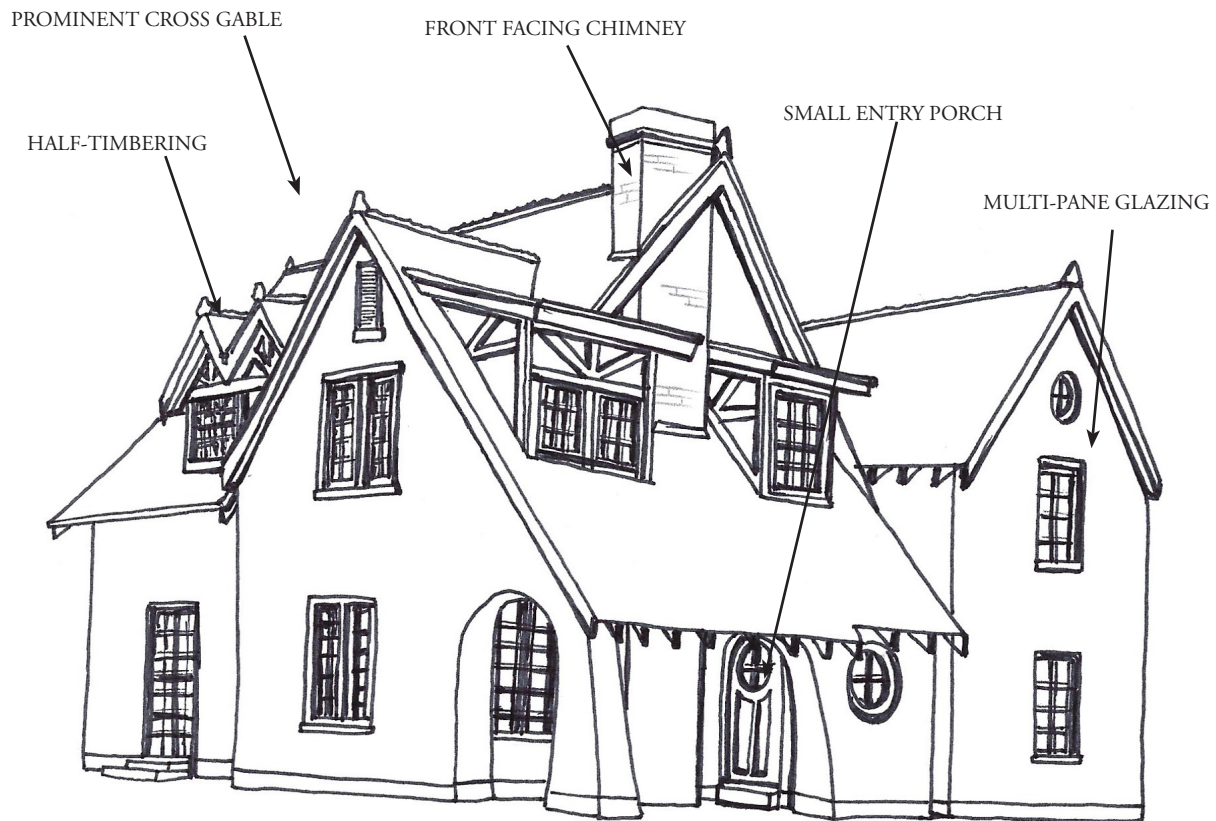
## Tudor Revival Style



Tudor Revival style is prevalent in both Miller and Chautauqua Historic Districts. After World War I this style became enormously popular as new construction technologies allowed brick and stone veneer to be applied to frame buildings.

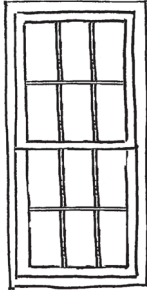
### Characteristics:

- Steeply pitched roofs;
- Usually side-gabled with one or more prominent cross gables;
- Windows usually appear very tall and narrow in multiple groups and multi-pane glazing;
- Walls typically clad with stucco, brick, or wood and feature false half-timbering;
- Front façade porches are generally either small entry porches or absent;
- Side porches are common;
- Front facing chimneys with chimney pots;
- Round arched doorways with heavy doors are common;
- Windows commonly located on or below the dominant front gable(s).

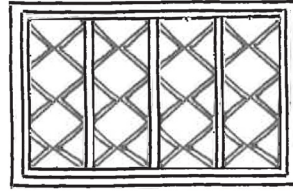
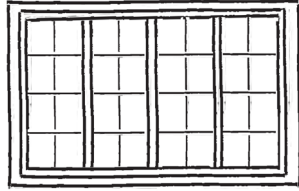


## Appropriate Elements for the Architectural Style:

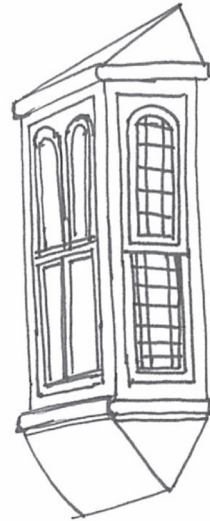
### Windows



6 OVER 6  
DOUBLE HUNG

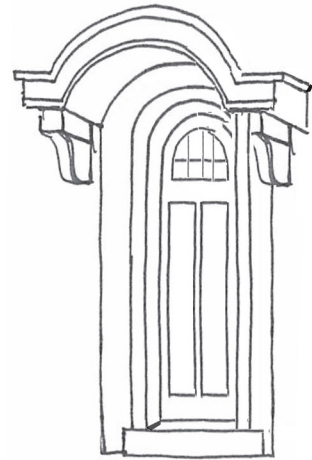
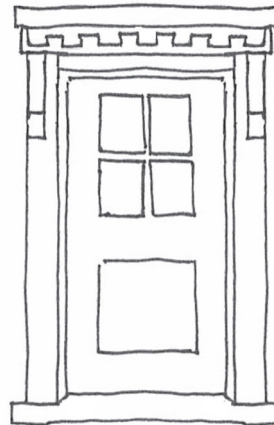
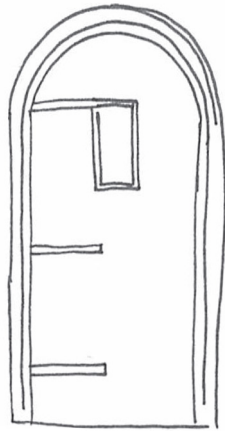
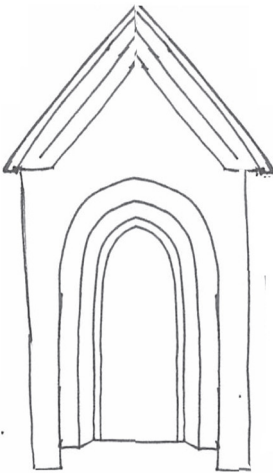


CASEMENT



ORIEL

### Doors And Entrances



### Light Fixtures



## National Style

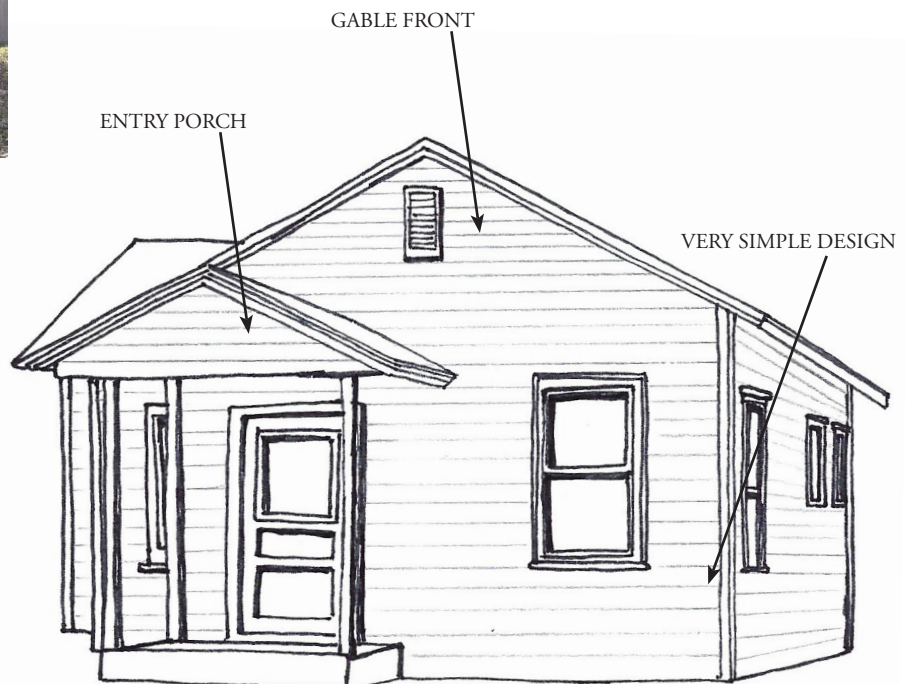


Popular between 1890 and 1950, National style is perhaps the oldest architectural style in Norman's historic districts. Not connected to any particular classical style of architecture, National style responded to the constraints of locally available materials and the need for economical buildings.

Between 1890 and 1910, many one-and two-story side-gable houses were constructed throughout Norman. Front-gable and wing houses were also very popular. Many of Norman's alley houses are also classified as National style. Originally built to rent to students and faculty from the university, this simple one-and-two-story style was later used during the 1940s for war housing.

### Characteristics:

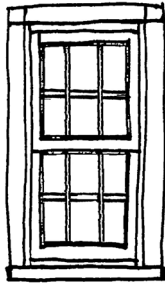
- Very simple design;
- No ornamentation;
- The porch is often the most decorative element;
- The house shape is the National style's first distinguishing feature and includes forms such as gable-front, gable-front-and-wing, hall-and-parlor, side-gabled houses, pyramidal houses, and I-shaped plans;
- The modest shotgun house is an example of National style.



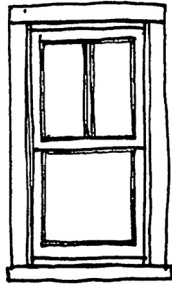


## Appropriate Elements for the Architectural Style:

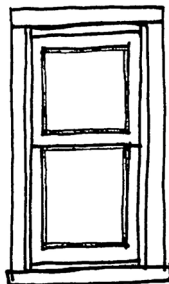
### Windows



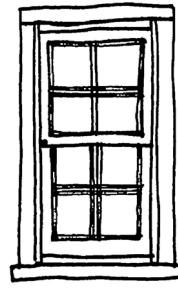
6 OVER 6  
DOUBLE HUNG



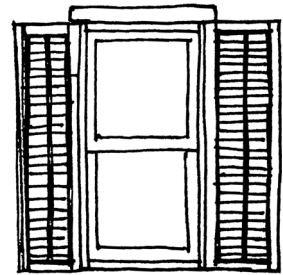
2 OVER 1  
DOUBLE HUNG



1 OVER 1  
DOUBLE HUNG

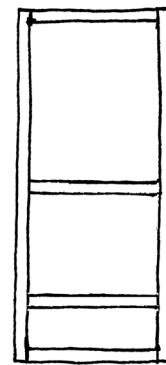
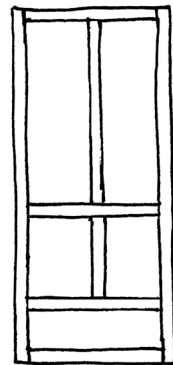
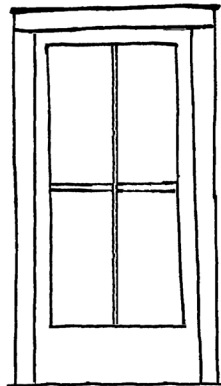
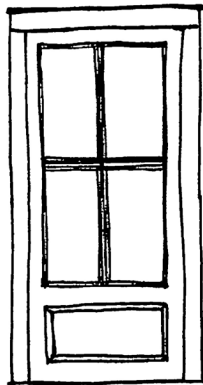


4 OVER 4  
DOUBLE HUNG



1 OVER 1  
DOUBLE HUNG  
WITH SHUTTERS

### Doors And Entrances



### Screen Doors

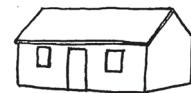
### House Shapes



GABLE-FRONT



GABLE-FRONT &  
WING (L SHAPED)



HALL & PARLOR



I-HOUSE



MASSED-PLAN  
SIDE-GABLED



PYRAMIDAL

## Colonial Revival Style

Colonial Revival style structures, common between 1889 and 1955, are scattered throughout Miller and Chautauqua Historic Districts. Most examples built after 1910 have side-gabled roofs.

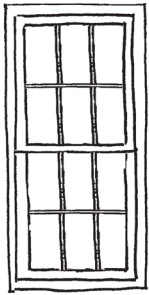
### Characteristics:

- They are distinguished by their symmetrical, rectangular shape;
- Generally two stories;
- Accentuated front door;
- Decorative crown or pediment supported by pilasters;
- Entry porch with classical columns;
- Fanlights and sidelights;
- Were constructed of both brick and wood;
- Often decorated with shutters.

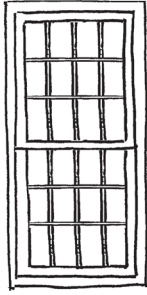


## Appropriate Elements for the Architectural Style:

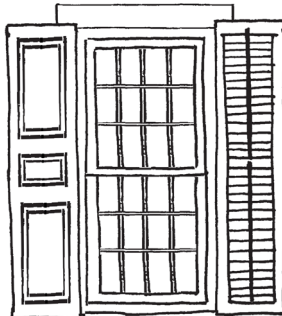
### Windows



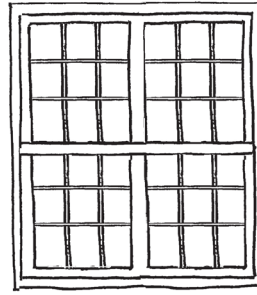
6 OVER 6  
DOUBLE HUNG



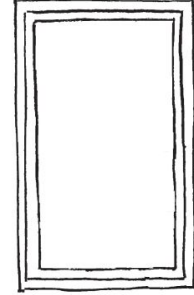
12 OVER 12  
DOUBLE HUNG



12 OVER 12  
DOUBLE HUNG WITH  
MOST COMMON  
SHUTTERS

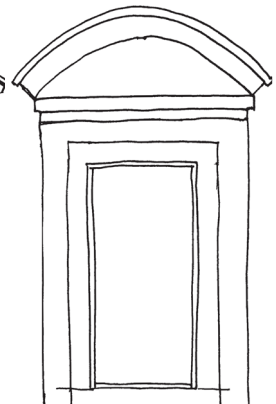
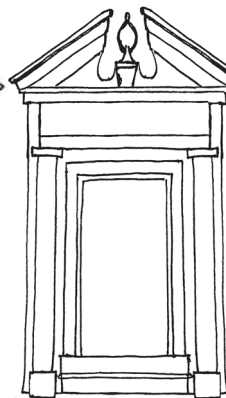
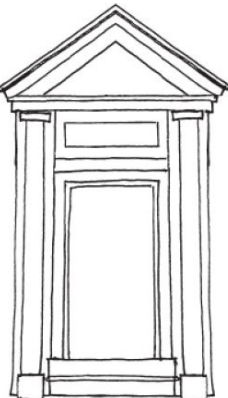
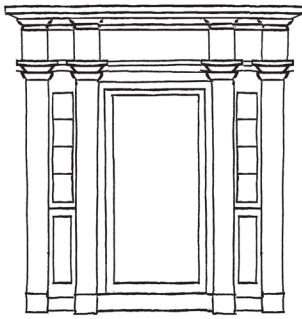
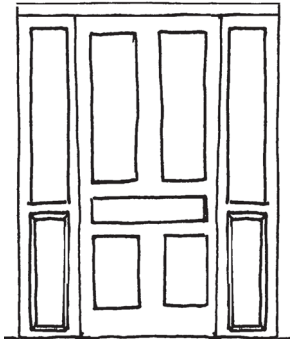
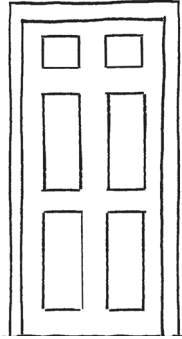
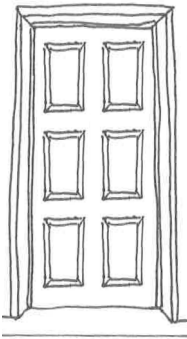


PAIRED WINDOWS

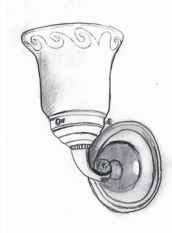


CASEMENT  
(OPENS LIKE A DOOR)

### Doors And Entrances



### Light Fixtures





## Prairie Style



Developed in Chicago by Frank Lloyd Wright and Louis Sullivan, Prairie style is regarded as one of the few truly American styles of architecture and became very popular between 1900 and 1920.

A simplified version of Prairie style, known as the American Foursquare, was perhaps the most popular subtype, particularly in the Midwest. Although common in urban settings, it was one of the preferred styles of farm families on the Plains.

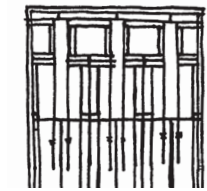
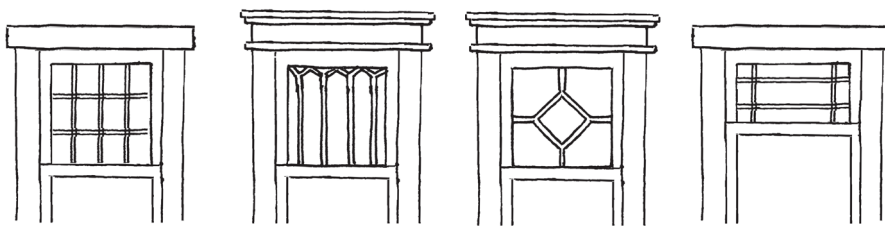
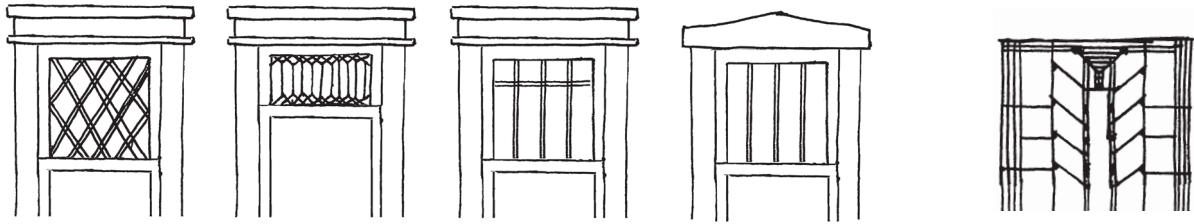
### Characteristics:

- Distinctive horizontal lines;
- Hipped roofs;
- Wide eaves;
- Massive square porch supports;
- Contrasting caps on porch and balcony railings;
- Contrasting wood trim between stories;
- Horizontal siding;
- Use of contrasting colors.



## Appropriate Elements for the Architectural Style:

### Windows



WRIGHTIAN  
ANGULAR GEOMETRIC

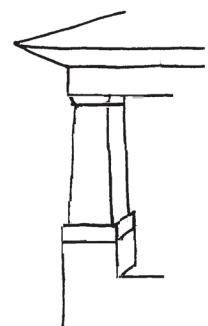
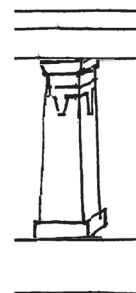
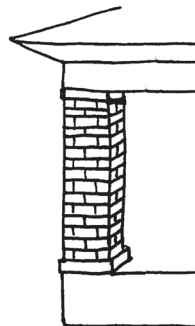
### Doors And Entrances



### Light Fixtures



### Common Porch Supports



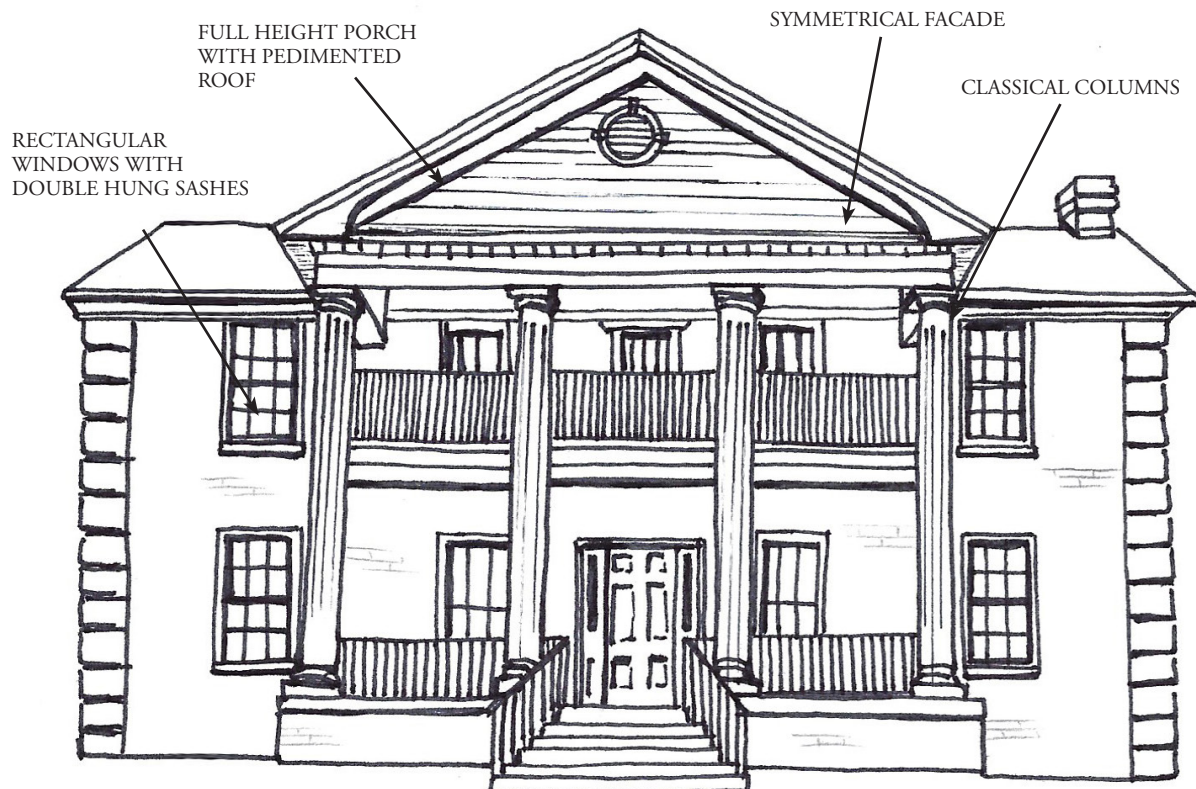
## Neoclassical Style



The Neoclassical style dominated domestic architecture throughout the country during the first half of the 20th century.

### Characteristics:

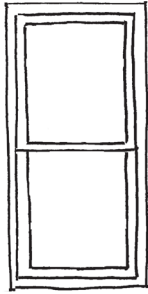
- Full height porch with roof supported by classical columns with Ionic or Corinthian capitals;
- Symmetrical façade, centered door and balanced windows;
- Subtypes have central entry porch extending full height but not full width;
- Porch with a gabled roof or pedimented roof;
- Curved semi-circular entry porches with flat roofs;
- Front gable roofs with a full-façade colonnaded porch like miniature Greek temple;
- One story cottages usually have hipped roofs with prominent central dormers with colonnaded porch;
- Elaborate decorative surrounds on doors;
- Rectangular windows with double hung sashes;
- Boxed eave with moderate overhang with the dentils beneath at cornices.



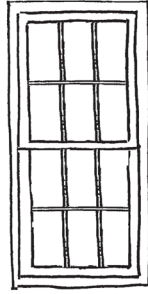


## Appropriate Elements for the Architectural Style:

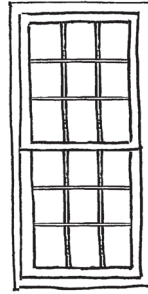
### Windows



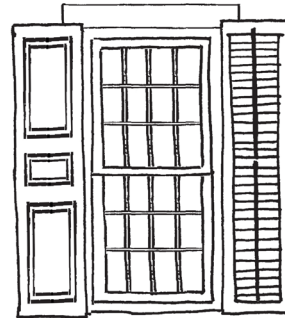
1 OVER 1  
DOUBLE HUNG



6 OVER 6  
DOUBLE HUNG

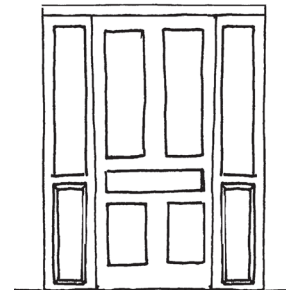
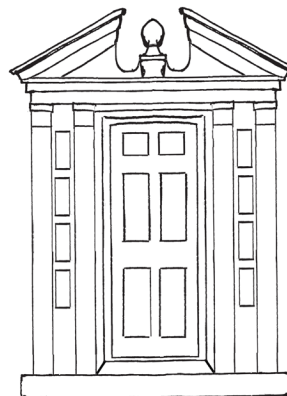
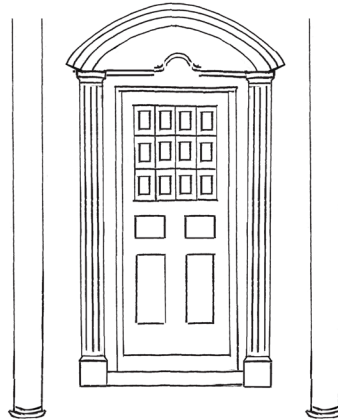
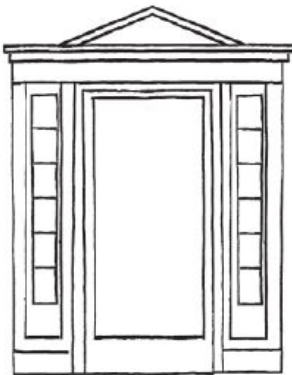


9 OVER 9  
DOUBLE HUNG



12 OVER 12  
DOUBLE HUNG WITH  
MOST COMMON  
SHUTTERS

### Doors And Entrances



### Light Fixtures

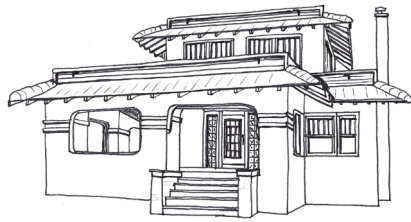


## Spanish Revival Style

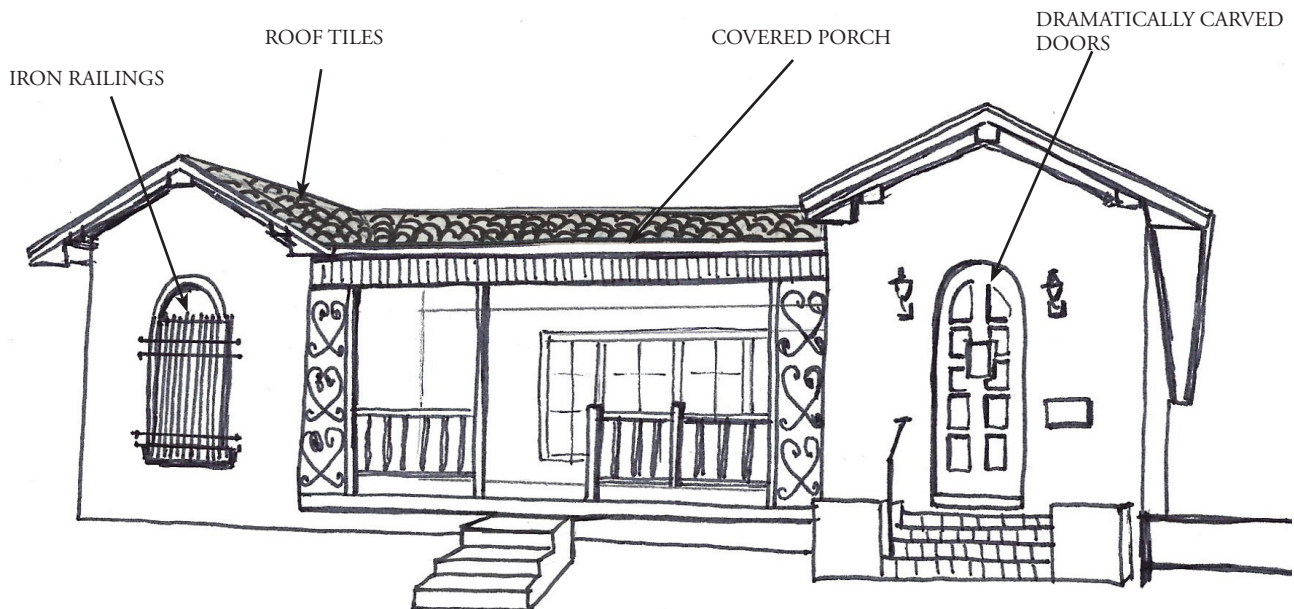
The style uses decorative details borrowed from the entire history of Spanish architecture. It is most common in the southwestern states, particularly California, Arizona, Texas, and Florida. Before about 1920, houses of Hispanic precedent were based on simple early Spanish missions.

### Characteristics:

- Decorative details borrowed from Spanish architecture;
- Roof tiles of two varieties: Mission tiles (half-cylinders) and Spanish tiles (S-curve);
- Dramatically carved doors in high style are more common;
- Less elaborate entrance doors of heavy wood panel are also common;
- Multi-level roofs;
- One or two-story covered porches;
- Canopies clad with terracotta tiles;
- Decorative iron door hardware;
- Balconettes with iron railings the full width of the windows.

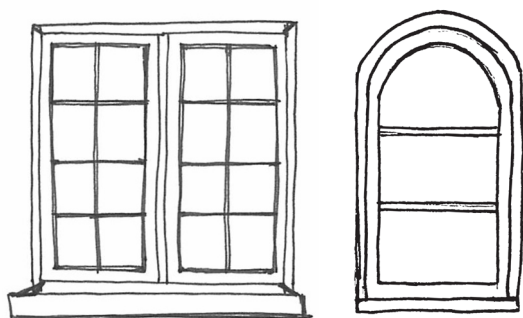


***This stucco Craftsman house has strong Spanish Colonial Revival influences.***

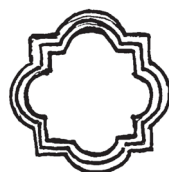


## Appropriate Elements for the Architectural Style:

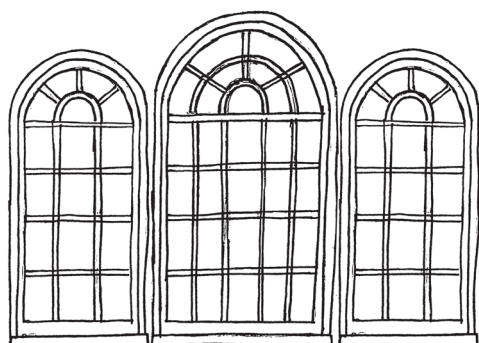
### Windows



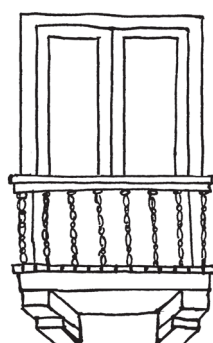
CASEMENT WINDOWS



QUATREFOIL WINDOW

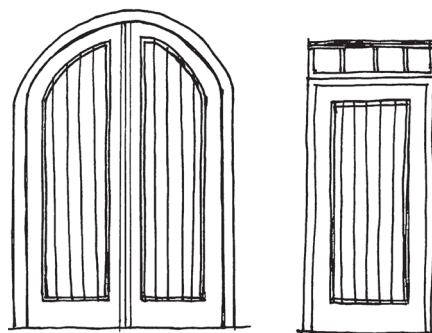


FOCAL WINDOWS



BALCONETTE

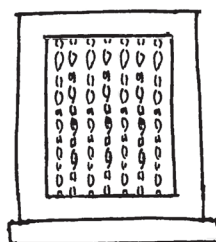
### Typical Doors



### Window Grilles



METAL

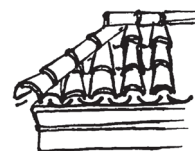


WOOD

### Tile Roof Patterns:



SPANISH TILE



TAPERED MISSION TILE

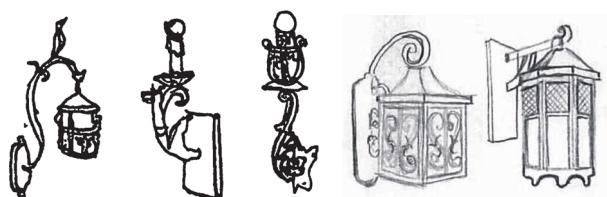


AMERICAN  
SPANISH TILE



STRAIGHT BARREL  
MISSION TILE

### Light Fixtures





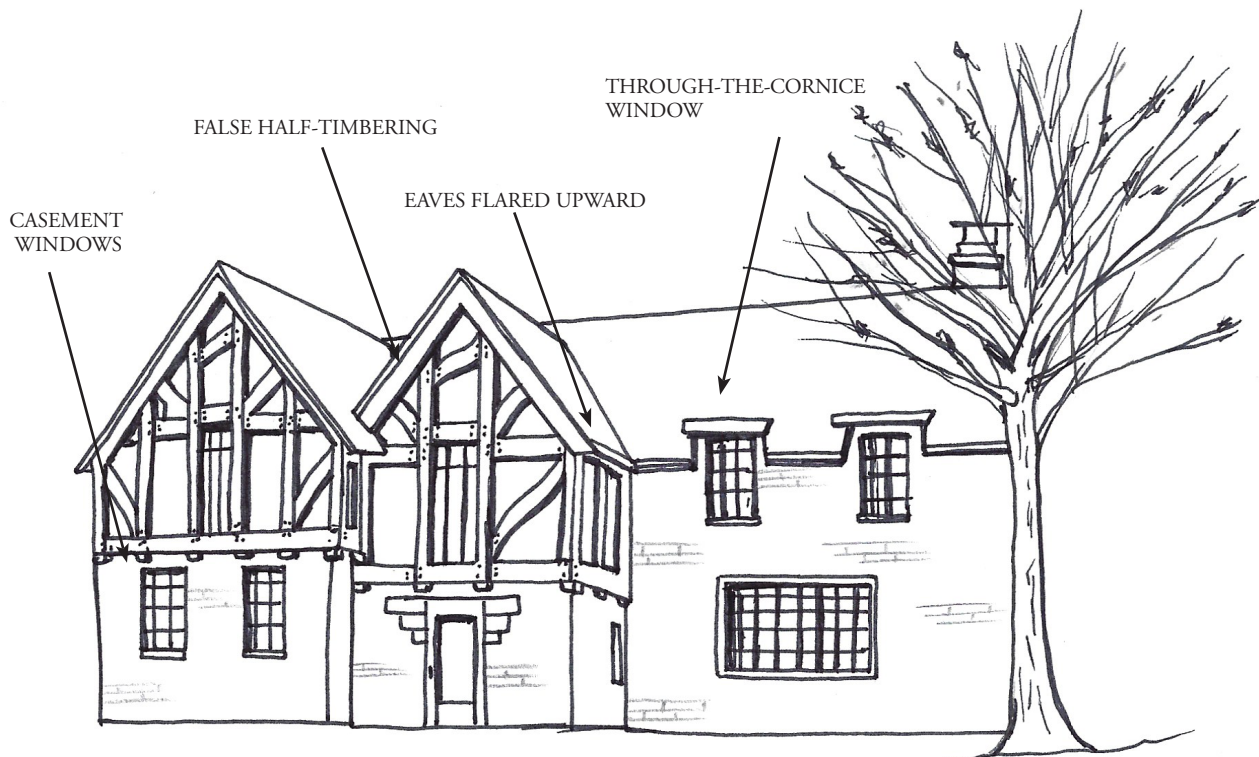


## French Eclectic Style

This style began to be somewhat fashionable in the early 1920s, and in 1925 about five percent of the new homes built were French, according to a study of houses published in architectural journals that year. The use of half-timbering with a variety of different wall materials, as well as roofs of flat tile, slate, stone, or thatch, are common to both as a result. French Eclectic houses often resemble the contemporaneous Tudor style based on related English precedent.

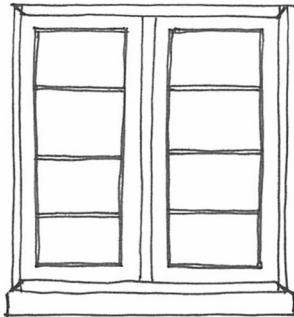
### Characteristics:

- Steeply pitched roof;
- Eaves commonly flared upward at roof-wall junction;
- Brick, stone, or stucco wall cladding;
- False half-timbering;
- Overhanging upper stories;
- Through-the-cornice window (breaks roof line);
- Casement windows.

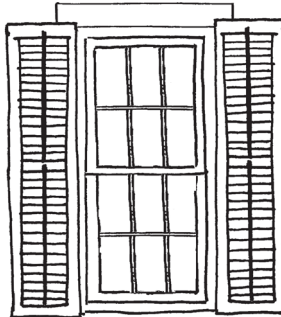


## Appropriate Elements for the Architectural Style:

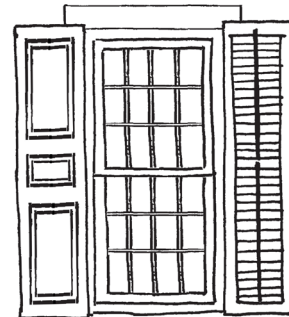
### Windows



CASEMENT WINDOWS

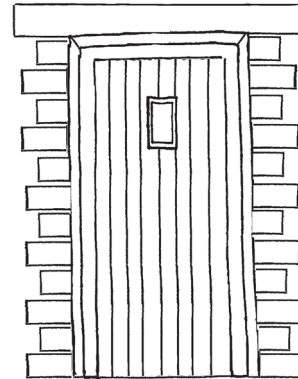
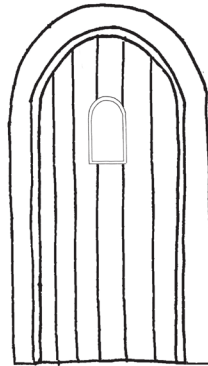
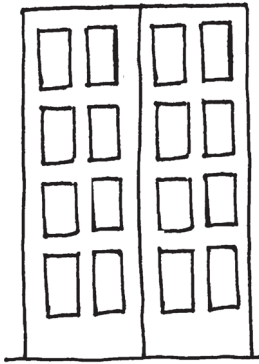


6 OVER 6  
DOUBLE HUNG WITH  
SHUTTERS



12 OVER 12  
DOUBLE HUNG WITH  
MOST COMMON  
SHUTTERS

### Doors And Entrances



### Light Fixtures



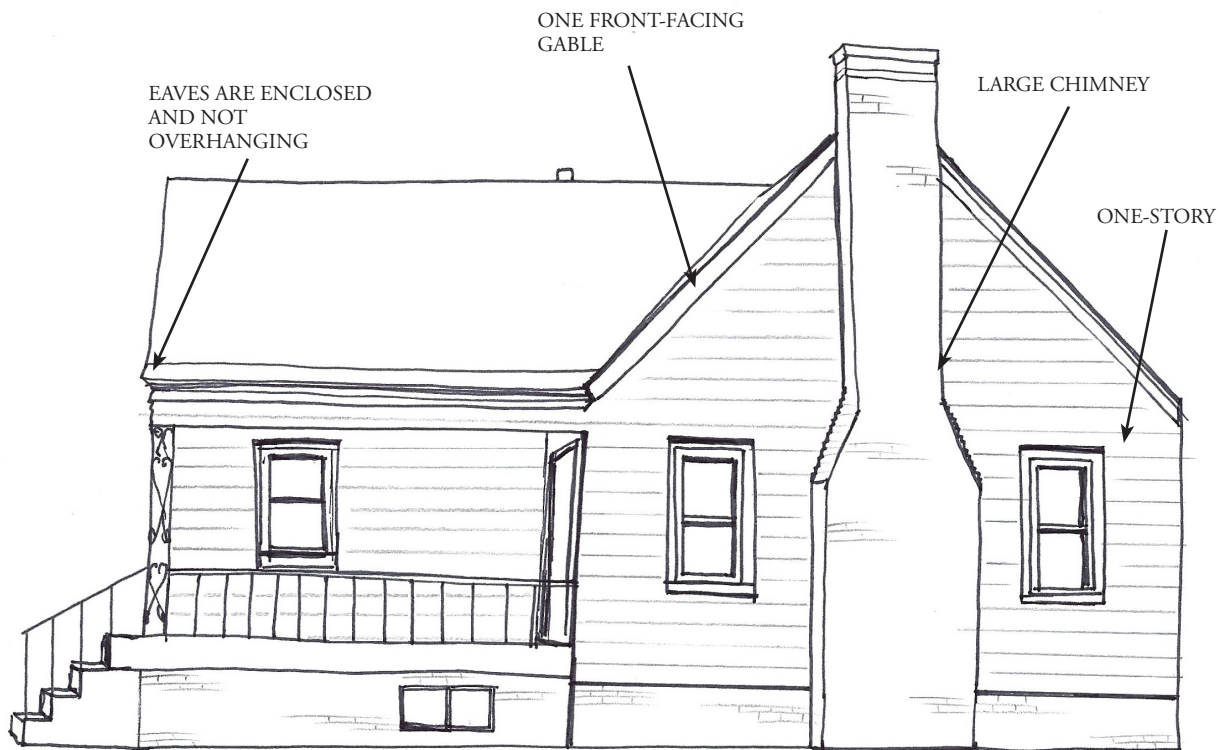
## Minimal Traditional



During the early 1940s, concentrations were rapidly built where new sites for World War II production plants created an urgent local need for worker housing. These late 1940's developments were necessary to begin to fulfill the wartime GI Bill promise that every returning serviceman would be able to purchase a home.

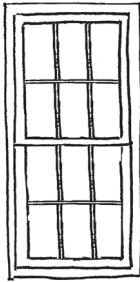
### Characteristics:

- Reflects the form of traditional style houses but lacks their decorative detailing;
- Roof pitches are low to intermediate;
- Eaves and rakes are close rather than overhanging;
- Eaves are enclosed;
- Usually but not always there is a large chimney, and at least one front-facing gable;
- Many reflect Tudor cottages with the roofline lowered and detailing removed;
- Most are one-story houses; occasionally two-story examples are seen;
- Most commonly two-story examples have extra detailing.

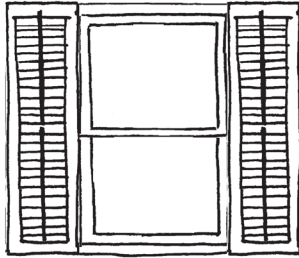


## Appropriate Elements for the Architectural Style:

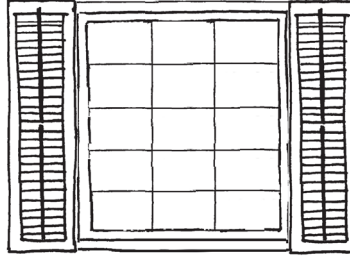
### Windows



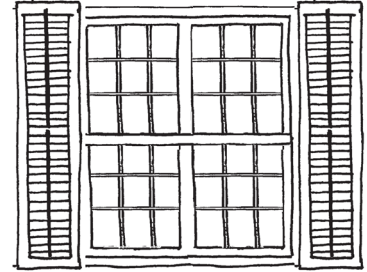
6 OVER 6  
DOUBLE HUNG



DOUBLE HUNG  
1 OVER 1 WITH SHUTTERS

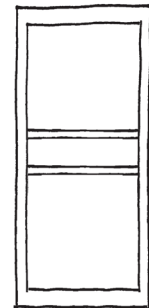
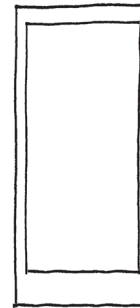
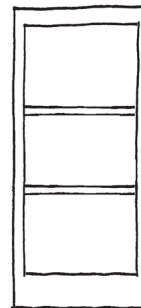
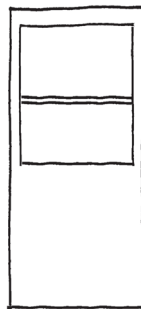
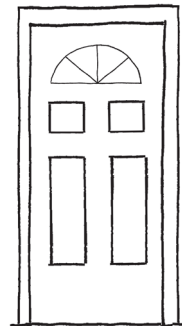
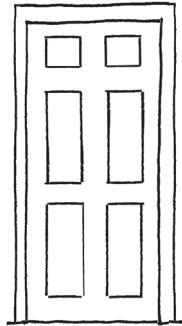
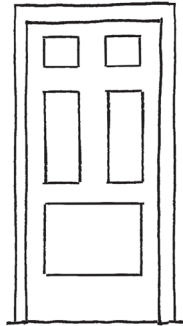
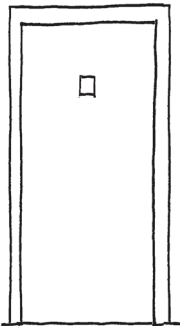


CASEMENT WINDOW  
WITH SHUTTERS

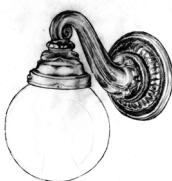


PAIRED WINDOWS  
WITH SHUTTERS

### Doors And Screen Doors



### Light Fixtures





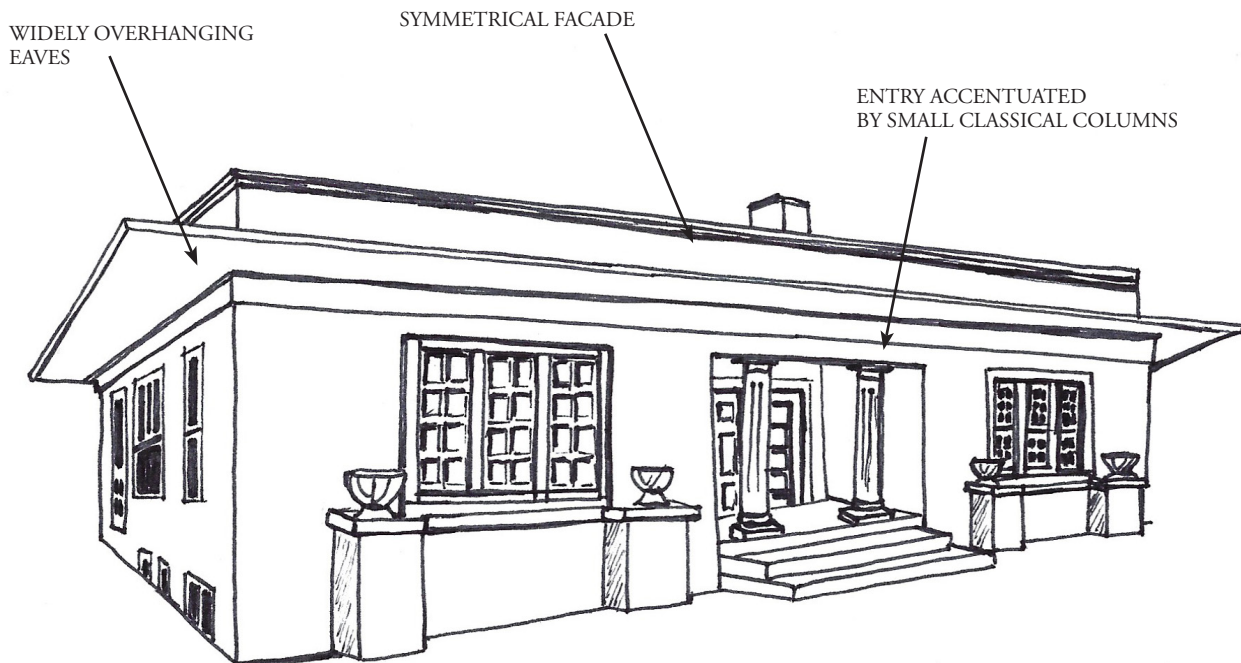
## Italian Renaissance



The Italian Renaissance style is found in early 20th-century houses throughout the country but is considerably less common than the contemporaneous Craftsman, Tudor, or Colonial Revival styles. Primarily a style for architect-designed landmarks in major metropolitan areas prior to World War I, vernacular interpretations spread widely with the perfection of masonry veneering techniques; most of these date from the 1920s.

### Characteristics

- Characterized by brick or stucco veneer over wood framing;
- Entry area accentuated by small classical columns or pilasters;
- Façade most commonly symmetrical;
- Widely overhanging eaves;
- Subtypes include: Simple hipped roof, hipped roof with projecting wing, asymmetrical or flat roof.

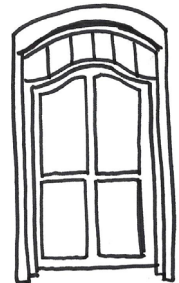
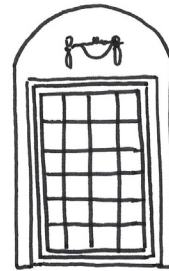
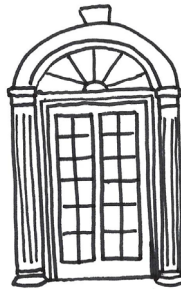
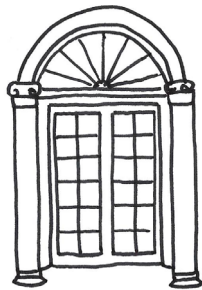


## Appropriate Elements for the Architectural Style:

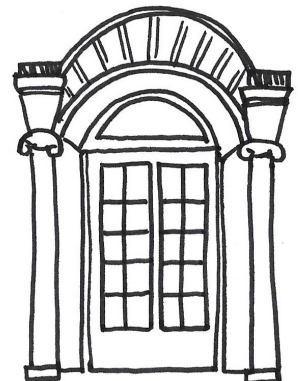
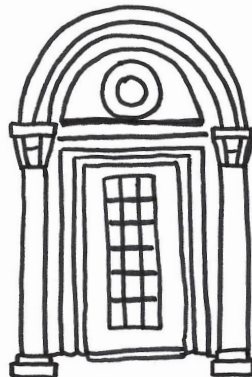
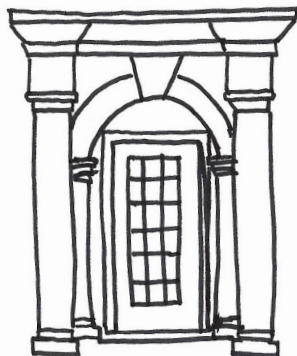
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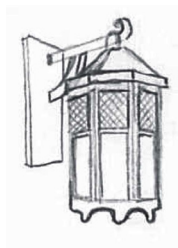
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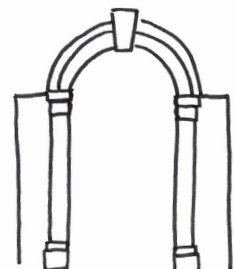
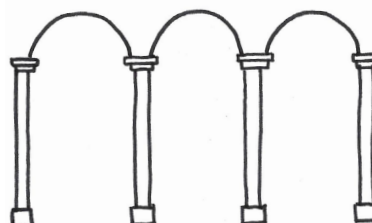
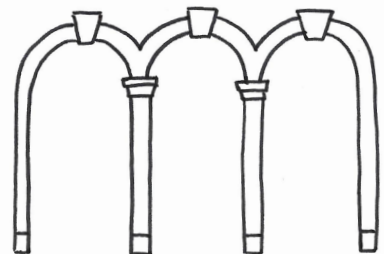
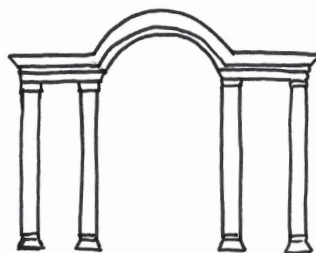
### Doors And Entrances



### Light Fixtures



### Porches





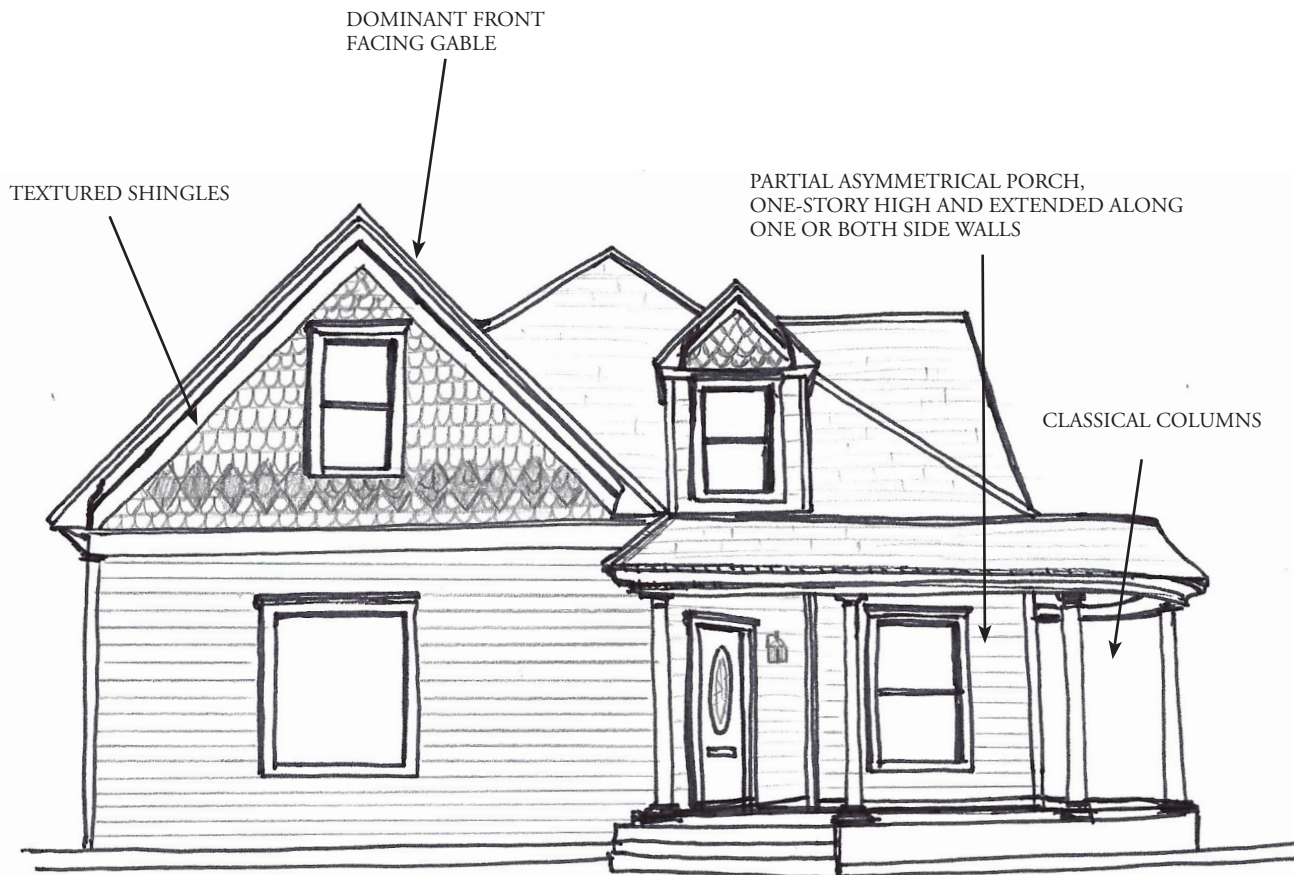
## Queen Anne



Queen Anne was the dominant style of domestic building during the period from about 1880 until 1900; it persisted with decreasing popularity through the first decade of the century. In the heavily populated north-eastern states the style is somewhat less common than elsewhere. There, except for resort areas, it is usually more restrained in decorative detailing and is more often executed in masonry.

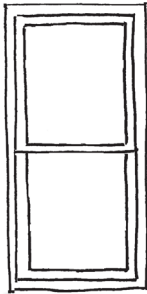
### Characteristics

- Hipped roof with lower cross gables;
- About 50 percent of houses have spindlework;
- Classical columns;
- Partial or full-width symmetrical porch, usually one-story high and extended along one or both side walls;
- Differing wall textures like patterned wood shingles shaped into varying designs.

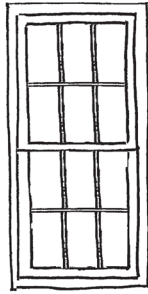


## Appropriate Elements for the Architectural Style:

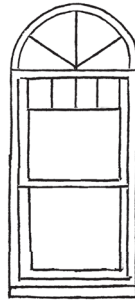
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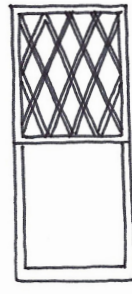
1 OVER 1  
DOUBLE HUNG



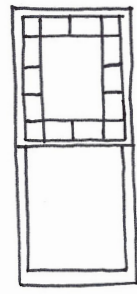
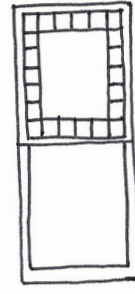
6 OVER 6  
DOUBLE HUNG



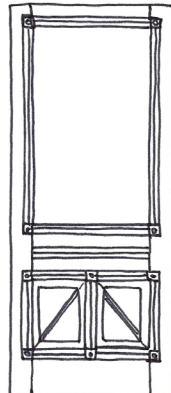
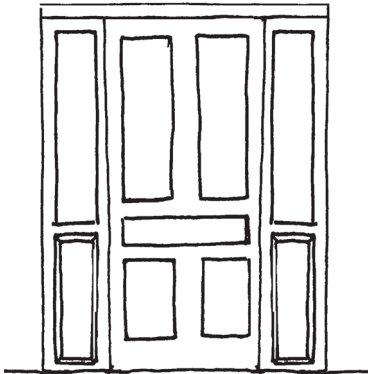
DOUBLE HUNG  
WITH HALF  
ROUND TRANSOM



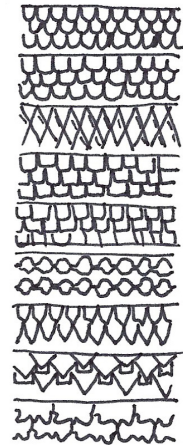
DOUBLE HUNG WITH  
GRILLE ON TOP SASH



### Doors And Entrances



### Wood Shingles



### Light Fixtures



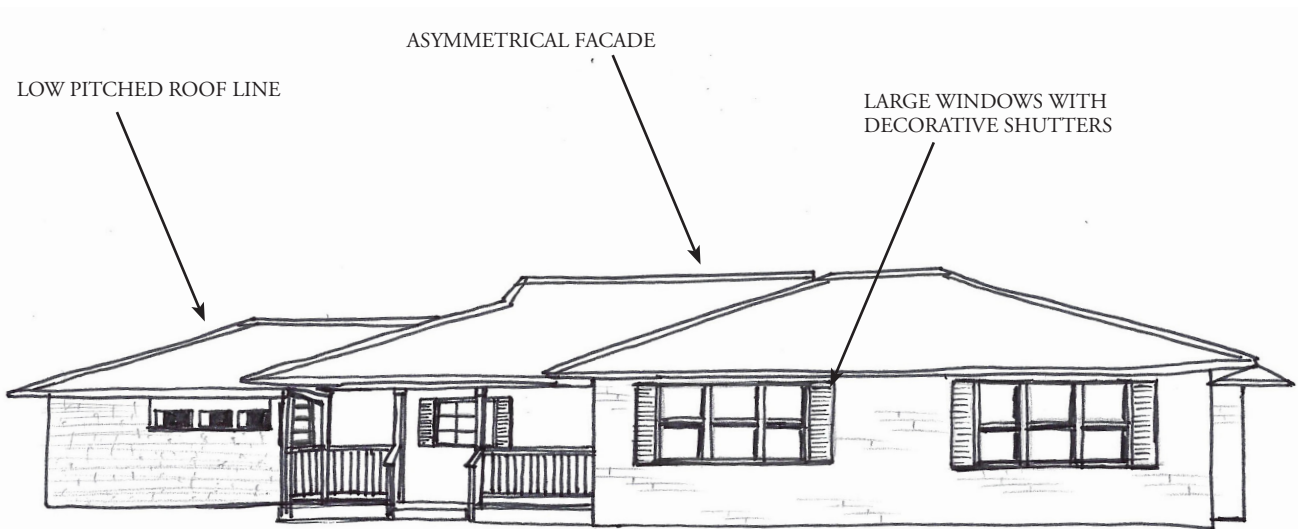
## Ranch



The Ranch style is a uniquely American domestic architectural style. It began in the 1930s and is loosely based on Spanish Colonial, Craftsman, and Prairie precedents.

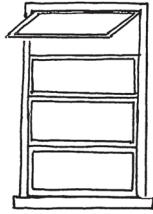
### Characteristics

- Single-story with asymmetrical façade;
- Three common roof forms, hipped-roof dominates, followed by cross-gabled, finally side-gabled;
- Large picture windows with decorative shutters;
- Low-pitched roof with long, low roofline;
- Wide to moderate with eave overhang, boxed or open;
- Porch roof supports in decorative iron;
- Brick or wood cladding.

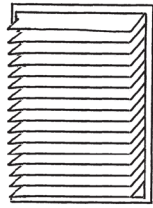


## Appropriate Elements for the Architectural Style:

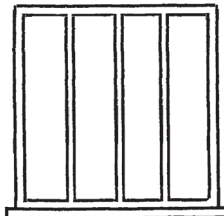
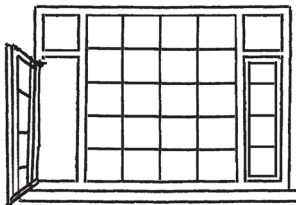
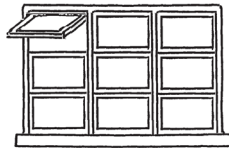
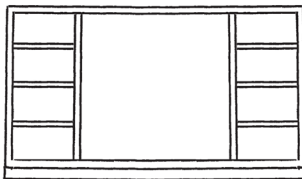
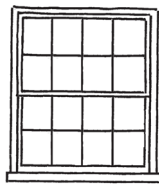
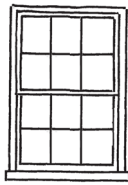
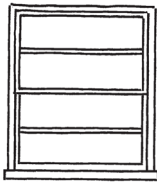
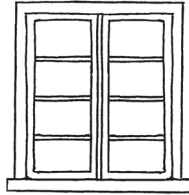
### Windows



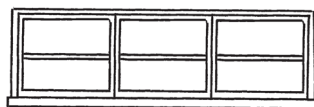
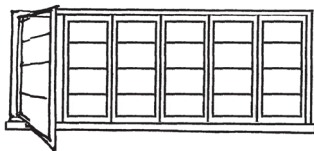
AWNING



CASEMENT

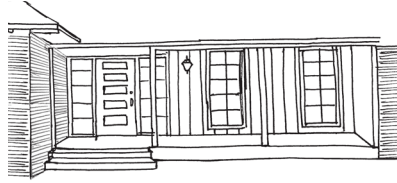


PICTURE WINDOWS

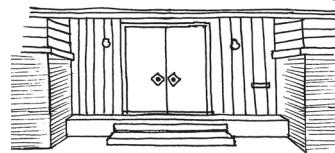
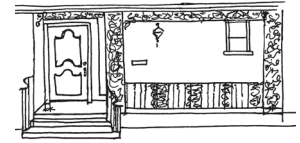


SHORT WINDOWS

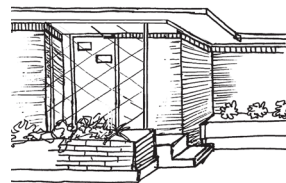
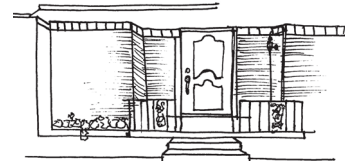
### Doors And Entrances



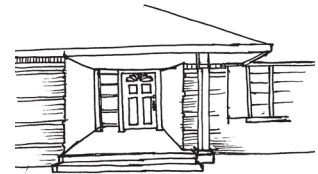
BROAD ENTRY PORCH



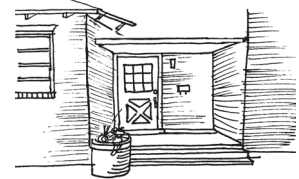
ENTRY ON FLAT FACADE



ENTRY PORCH ON CROSS GABLE OR CROSS HIP



ENTRY SET INTO L



### Light Fixtures





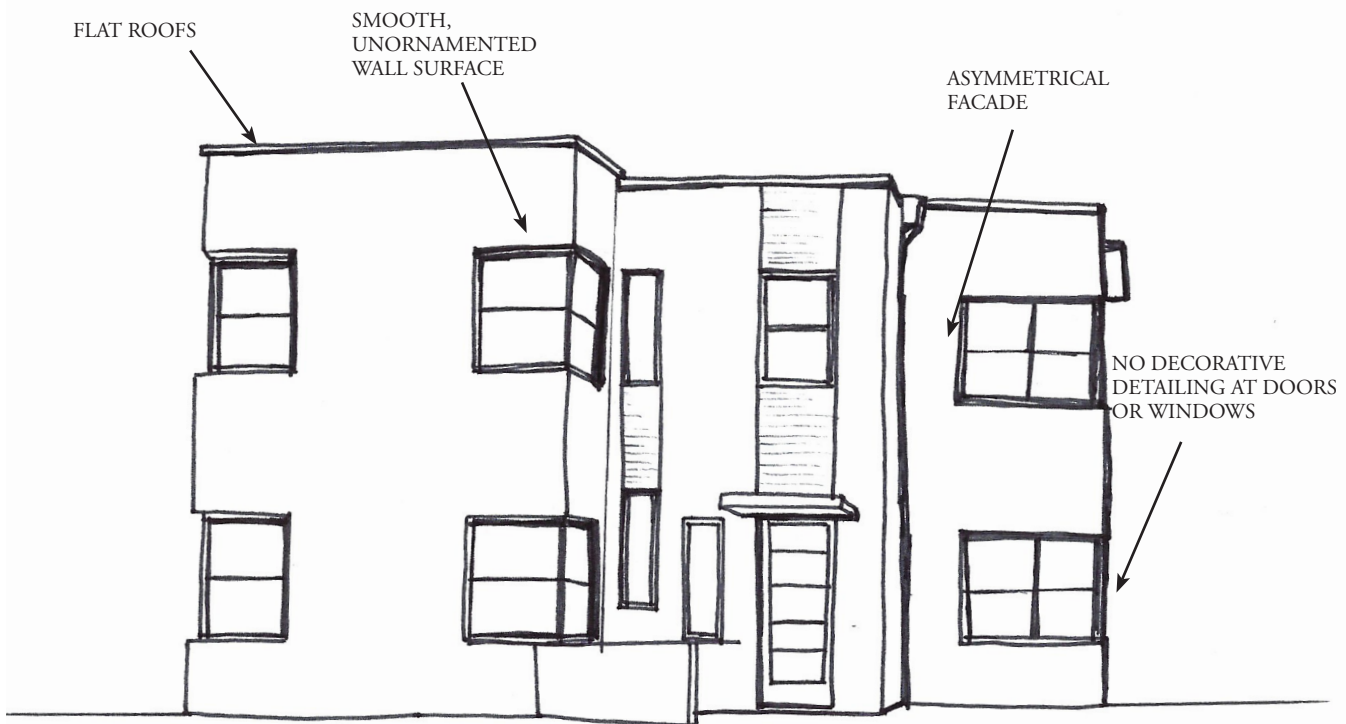
## International



In the decades separating WWI and WWII - while Americans were building neighborhoods of period houses, European architects were busy creating dramatic new modern homes and buildings. Le Corbusier in France, Oud and Rietveld in Holland, and Walter Gropius and Mies Van Der Rohe in Germany were all working without historic precedent, trying to exploit the materials and technology of the day. These pioneers wished to create an International architecture “independent of specific materials, sites or cultural tradition” and specifically chose white stucco as a uniting material to achieve these ideals.

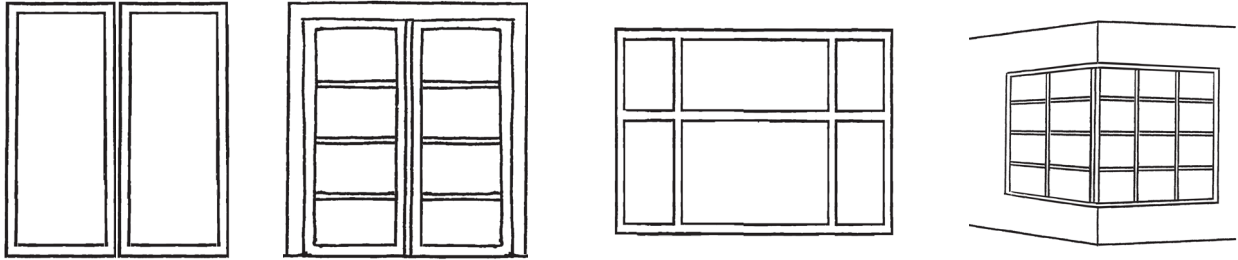
### Characteristics

- Flat roof, usually without ledge (coping) at roof line;
- Windows set flush with outer walls;
- Smooth unornamented surfaces with no decorative detailing at doors or windows;
- Large window groupings, often linear, and expanses of windowless wall surface;
- Unified wall cladding, generally white stucco;
- Commonly asymmetrical.

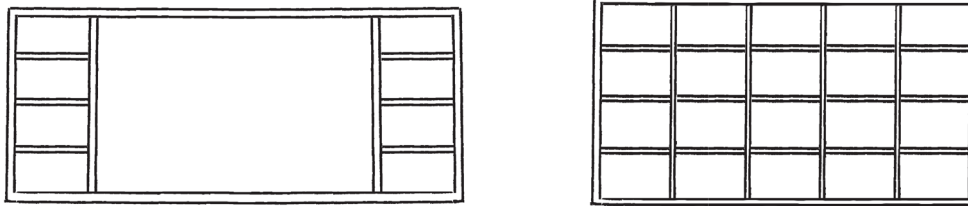


## Appropriate Elements for the Architectural Style:

### Windows

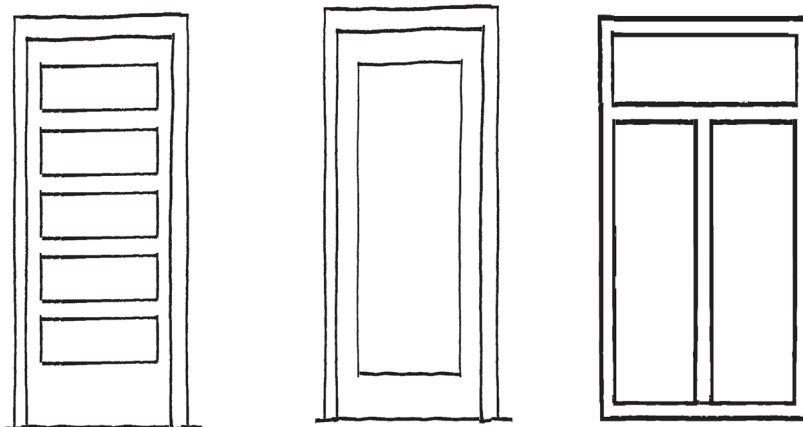


CASEMENT WINDOWS



PICTURE WINDOWS

### Doors And Entrances





## **Historic District Ordinance**



# DRAFT CITY OF NORMAN HISTORIC PRESERVATION GUIDELINES

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## Introduction

### 1.1 Purpose of Design Guidelines

*By authority of the Norman Code of Ordinances, sec. 429.3, Historic District Commission approval via a Certificate of Appropriateness is required for all new construction, structural alterations to the exterior of an existing structure, and demolition within a historic district.*

**.1 Preserve and Maintain the Character.** These Standards and Guidelines are intended to preserve and maintain the character of the historic buildings in Norman. They reinforce and protect the important features of the historic districts and define those visual elements which are common to each district as well as the qualities unique to this community.

**.2 Preserve Integrity and Enhance Value.** This document will help preserve the integrity of historic buildings and enhance the value of the historic district for the private investor, residents and owners, and the community as a whole. Changes to an individual building should not be considered in isolation. Modifications affect the block as a whole and must have the broad interest of the community in mind.

**.3 Limited to Exterior Site.** The Standards and Guidelines do not address the use of the building or its interior. Only the exterior portions, which includes new construction, additions, alterations to the site and rehabilitation of the structures, must comply with the guidelines set forth.

**.4 Look at the Building's Original Use.** These Standards and Guidelines must be applied to a building based on its original use and construction.

- a. For example, although a former residence may currently be used as an office, it is still subject to the standards and guidelines appropriate to a residential building.
- b. These Standards and Guidelines are designed to assist everyone with a stake in preserving Norman's Historic Districts. They are an essential tool in helping the Historic District Commission fulfill its mission to preserve, protect, and educate the public through the application of consistent standards and guidelines.

**.5 Who Is This Document For?** This handbook is intended to assist property owners in planning projects which will alter the exterior of their property and, therefore, impact the overall character and integrity of the historic districts. For property owners, residents, and contractors, the Standards and Guidelines provide clear guidance in planning projects that are sympathetic to the special character of Norman's designated Historic Districts. For Historic District Commissioners and city staff, the Standards and Guidelines offer guidelines by which to evaluate proposed changes to historic structures.

**.6 Why Historic Preservation Matters to Norman?** Historic preservation is vitally important to the Norman community — now more than ever. Historic buildings embody a distinctive form of our city's architecture that will never again be duplicated, and these buildings and their surroundings add an irreplaceable component to the character and personality of

Norman. The architecture of our historic neighborhoods shapes our sense of place and our feelings about where we live. This is what makes the historic neighborhoods worthy of protection.

**.7 The Mission of Norman's Historic District Commission.** The Norman Historic District Commission serves as the City Council's official historic preservation body to identify, protect, and educate the public about Norman's historic resources.

## 1.2 How to Use This Document

.1 Whether the proposed work to the building is a small repair or a major renovation or addition, it is important to consult pertinent Standards and Guidelines for guidance on your project. These Standards and Guidelines will be used by the City of Norman to provide an objective basis for the decisions of the Historic District Commission and staff. This document is laid out in five general characteristics of a historic property. Each characteristic is then divided into architectural features of that characteristic.

.2 The Standards and Guidelines specifically look at the following design elements:

- Height
- Proportion of building's front façade
- Proportion of openings within the building
- Rhythm of solids to voids in front façades
- Rhythm of spacing of buildings on streets
- Rhythm of entrance and/or porch projection
- Relationship of materials and texture
- Roof shapes
- Walls of continuity
- Scale of building
- Site and Setting

## 1.3 Certificate of Appropriateness or Administrative Bypass

### .1 Certificate of Appropriateness

- a. Requests for alterations to the exterior of a property or site require a Certificate of Appropriateness (COA), issued after thorough review of the project by the Historic District Commission. In addition to completing the COA application form, the property owner, agent, or resident must attach a detailed description of the project as specified on the application. As applicable, the following information is also required:

- Sketches
- Photographs
- Floor plans
- Site plans

Elevation drawings  
 Trees preservation plan  
 Material lists  
 Material samples

And/or other means of adequately describing the work proposed.

## .2 Administrative Bypass

- a. Certain specific project requests for alterations to the exterior of a property or site may be issued a Certificate of Appropriateness approvable through a process known as Administrative Bypass. Each section of the Historic Preservation Standards and Guidelines contains a set of Standards for projects approvable through the Administrative Bypass process.
- b. Applying for Certificate of Appropriateness by Administrative Bypass. In order to obtain a Certificate of Appropriateness by Administrative Bypass, an application form and support documentation that sufficiently describes the proposed work must be submitted to staff prior to commencement of work.
- c. Support documents that may be required by staff to allow for a complete review include the following:

Sketches  
 Photographs  
 Floor plans  
 Site plans  
 Elevation drawings  
 Trees preservation plan  
 Material lists  
 Material samples

And/or other means to adequately describe or illustrate proposed alteration(s).

- d. There is not an application fee for a Certificate of Appropriateness by Administrative Bypass. There is not a deadline; however, it can take 5-7 days to process a request. Requests may require the application and approval of building permit in addition to the issuance of a Certificate of Appropriateness. Therefore, applicants should submit requests in a timely manner to ensure issuance of a Certificate of Appropriateness and building permit prior to the desired installation date of the proposed work.
- e. If a Certificate of Appropriateness by Administrative Bypass is denied by the Historic Preservation Officer, or authorized designee, the applicant shall have the right to appear before the Historic District Commission at its next regularly scheduled meeting time for formal action regarding the Certificate of Appropriateness.

## 1.4 Introduction to the Secretary of the Interior Standards

.1 The Secretary of the Interior is responsible for establishing standards for all programs under departmental authority and for advising federal agencies on the preservation of historic properties listed in or eligible for listing in the National Register of Historic Places. In partial fulfillment of this responsibility the Secretary of the Interior's Standards for the Treatment of Historic Properties have been developed to guide work undertaken on historic properties; there are separate standards for preservation, rehabilitation, restoration, and reconstruction. The Standards for Rehabilitation (codified in 36 CFR 67) comprise that section of the overall treatment standards and address the most prevalent treatment. "Rehabilitation" is defined as the act or process of making possible a compatible use for a property through repair, alterations, and additions while preserving those portions or features which convey its historical, cultural, or architectural values.

.2 Initially developed by the Secretary of the Interior to determine the appropriateness of proposed project work on registered properties supported by the Historic Preservation Fund grant-in-aid program, the Standards have been widely used over the years—particularly to determine if a rehabilitation project qualifies as a Certified Rehabilitation for Federal Historic Preservation Tax Incentives. In addition, the Standards have guided federal agencies in carrying out their responsibilities for properties in federal ownership or control and state and local officials in reviewing both federal and non-federal rehabilitation proposals. They have also been adopted by historic district and planning commissions across the country.

.3 The intent of the Standards is to assist in the long-term preservation of historic materials and features. The Standards pertain to historic buildings of all materials, construction types, sizes and occupancy and include the exterior and the interior of the buildings. They also encompass the building's site and environment, including landscape features, as well as attached, adjacent or related new construction. To be certified for federal tax purposes, a rehabilitation project must be determined by the Secretary of the Interior to be consistent with the historic character of the structure(s) and, where applicable, the district in which it is located.

.4 As stated in the definition, the treatment "rehabilitation" assumes that at least some repair or alteration of the historic building will be needed in order to provide for an efficient contemporary use; however, these repairs and alterations must not damage or destroy materials, features or finishes that are important in defining the building's historic character. For example, certain treatments—if improperly applied—may cause or accelerate physical deterioration of the historic building. This can include using improper repointing or exterior masonry cleaning techniques or introducing insulation that may damage historic fabric. Any of these treatments will likely result in a project that does not meet the Standards. Similarly, exterior additions that duplicate the form, material and detailing of the historic structure to the extent that they compromise its historic character will also fail to meet the Standards.

**1.5 Secretary of the Interior Standards for Rehabilitation.** Both the Historic District Ordinance and the guidelines portion of the Norman Historic Preservation Handbook include The Secretary of the Interior's Standards for Rehabilitation and Guidelines for Rehabilitating Historic

Buildings (US Department of the Interior/National Park Service, Heritage Preservation Services, Revised 1990).

- .1 **Make Minimal Changes.** A property shall be used for its historic purpose or be placed in a new use that requires minimal change to the defining characteristics of the building and its site and environment.
- .2 **Retain Historic Character.** The historic character of a property shall be retained and preserved. The removal of historical materials or alterations of features and spaces that characterize a property shall be avoided.
- .3 **Avoid False Historical Impressions.** Each property shall be recognized as a physical record of its time, place, and use. Changes that create a false sense of historical development, such as adding conjectural features or architectural elements from other buildings, shall not be undertaken.
- .4 **Acknowledge Changes Over Time.** Most properties change over time; those changes that have acquired historic significance in their own right shall be retained and preserved.
- .5 **Preserve Distinctive Features.** Distinctive features, finishes, and construction techniques or examples of craftsmanship that characterize a historic property shall be preserved.
- .6 **Repair Rather Than Replace.** Deteriorated historic features shall be repaired rather than replaced. Where the severity of deterioration requires replacement of a distinctive feature, the new feature shall match the old in design, color, texture, and other visual qualities and, where possible, materials. Replacement of missing features shall be substantiated by documentary, physical, or pictorial evidence.
- .7 **Avoid Harsh Treatments.** Chemical or physical treatments, such as sandblasting, that cause damage to historic materials shall not be used. The surface cleaning of structures, if appropriate, shall be undertaken using the gentlest means possible.
- .8 **Protect Archaeological Resources.** Significant archaeological resources affected by a project shall be protected and preserved. If such resources must be disturbed, mitigation measures shall be undertaken.
- .9 **Make Compatible Additions.** New additions, exterior alterations, or related new construction shall not destroy historic materials that characterize the property. The new work shall be differentiated from the old and shall be compatible with the massing, size, scale, and architectural features to protect the historic integrity of the property and its environment.
- .10 **Preserve Original Integrity.** New additions and adjacent or related new construction shall be undertaken in such a manner that, if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired.

## 1.6 Norman's Historic Districts

### .1 Chautauqua Historic District

- a. Built between 1903-1940.
- b. Tree lined neighborhood with stately residences that reflect the status of the university deans and faculty and other prominent individuals who helped shape early development of the city.
- c. Its development was tied closely to the development of the city.
- d. Architecturally, Chautauqua is very eclectic. Bungalows are prominently represented, but Tudor Revival and Minimal Traditional are also quite prevalent.
- e. The district also includes fine examples of Prairie, Colonial Revival, Spanish Eclectic, Neoclassical Revival, and even one example of Queen Anne.
- f. More than 70% of the houses have paved driveways to the left or right of the house that lead to an outbuilding in the rear of the property.
- g. Many houses in this district have a shared driveway.
- h. Very few houses have an attached garage or carport to the side of the house.
- i. Houses do not have a consistent setback from the street.
- j. All streets in this district have parkways and sidewalks on both sides of streets, and paved walkways that lead from the sidewalk to the front door.

### .2 Miller Historic District

- a. Built between 1910-1938.
- b. This district does not have as many trees lining the streets as Chautauqua and Southridge.
- c. It began to fully develop after WWI as an exclusive neighborhood for university faculty and Norman business leaders.
- d. Nearly half the structures are classified as Bungalows, but the neighborhood also includes Minimal Traditional, Colonial Revival, National Folk, and Tudor Revival.
- e. The westernmost blocks of the district align parallel to the railroad tracks; the remaining blocks follow the cardinal points of the compass.
- f. About 50% of the houses have paved driveways to the left or right of the house that lead to an outbuilding in the rear of the property.
- g. Around 20% of the houses have garages attached to the side of the house.
- h. Only a few houses have carports attached to the side of the house.
- i. All houses have a consistent setback from the street.
- j. A majority of the streets in this district have sidewalks, parkways on both sides of the streets, and paved walkways that lead from the sidewalk to the front door.



### .3 Southridge Historic District

- a. Built between 1920–1950.
- b. Tree lined streets with front yard gardens, located eleven blocks south of downtown district and three blocks east of the university.
- c. Largest decade of growth occurred from 1931–1940 with the construction of approximately sixty-seven buildings. The advent of World War II escalated the demand for housing in Norman as military students, frequently with their families, came in droves to attend the Naval Training School and subsequently the Naval Air Station.
- d. Architecturally, the dominant styles are Tudor Revival, Colonial Revival, and Minimal Traditional.
- e. About 50% of the houses have paved driveways to the left or right of the house that lead to an outbuilding in the back.
- f. Around 30% of the houses have an attached garage to the side of the house and few have carports.
- g. Many houses have semi-circular driveways.
- h. All houses have a consistent setback from the street.
- i. The majority of streets have sidewalks and parkways on both sides of the streets.
- j. All houses have paved walkways that lead from either the sidewalk or the driveway to the front door.

## Site and Setting

### Site Features

#### 2.1 Standards for Administrative Bypass

*The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, the application will be forwarded to the Historic District Commission for a full review.*

**.1 Garden Structures.** Garden structures such as a pergola or freestanding trellis, 120 square feet or less, located behind the principal structure with limited or no visibility from the front right-of-way. Wood, metal, wood composite or combination of these materials are acceptable. Vinyl structures are prohibited. Building structures greater than 108 square feet require a building permit.

**.2 Surface Parking.** Parking areas 400 square feet or less, located off the alleyway and not visible from the front right-of-way. Corner lots are considered to have two front elevations.

**.3 Storm Shelters.** Above ground storm shelters 120 square feet or less that are not visible from the front right-of-way. Underground storm shelters of any size located in the rear yard and not visible from the front right-of-way. Corner lots are considered to have two front elevations.

**.4 Swimming Pools.** Located behind the principal structure in the rear yard and not visible from front right-of-way. Corner lots are considered to have two front elevations.

## 2.2 Guidelines

*A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):*

**.1 Garden Structures.** Garden structures, such as pergolas and trellis, larger than 120 square feet, are to be located behind the principal structure with very limited or no visibility from the front right-of-way. Front or side yard installation can be considered if documentation shows one existed historically. Structures abutting or attached to the principal structure will be reviewed as a building addition. Structures that have a roof and/or sides will be reviewed as accessory structures.

**.2 Materials.** Structures are to be comprised of wood. Metal, composite wood or cement fiberboard will be considered on a case-by-case basis. Vinyl is prohibited.

**.3 Height.** Structure shall be no taller than the height of the principal structure.

**.4 Swimming Pools.** Swimming pools are to be located behind the principal structure with no visibility from the front right-of-way. Side yard installations will be considered on a case-by-case basis. A front yard installation is prohibited. Corner lots are considered to have two front elevations

**.5 Storm Shelters.** Above ground storm shelters greater than 120 square feet are to be located behind the principal structure with no visibility from the front right-of-way. Side yard installations of below ground storm shelters will be considered on a case-by-case basis. A front yard installation of above ground or below ground storm shelters are prohibited.

## Garages

### 2.3 Standards for Administrative Bypass

*The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, the application will be forwarded to the Historic District Commission for a full review.*

#### **.1 Garage Door Replacement.**

For non-historic garages that face the alleyway or that are not visible from the right-of-way, the following is allowed:

- a. Wood, wood composite or a raised metal panel garage door.

- b. The original size, height and width of doors must be maintained.
- c. Designs must match the style of the original garage door and/or garage.

## 2.4 Guidelines

*A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):*

- .1 **Preserve Historic Garage Structures.** Retain and preserve garages in their original locations and configurations. Even if the function changes, the exterior appearance shall remain the same.
- .2 **Preserve Original Materials.** Retain and preserve character-defining materials, features, and details of historic garages, including foundations, siding, masonry, windows, garage doors, and architectural trim. When necessary, repair character-defining materials, features, and details of historic garages in-kind according to pertinent guidelines.
- .3 **Replace Only Deteriorated Portions.** If replacement of a deteriorated element or detail of a historic garage is necessary, replace only the deteriorated portion in-kind rather than replacing the entire feature. Match the original in design, dimension, texture, and material. Consider compatible substitute materials only if the original materials are no longer available.
- .4 **Request for Garage Demolitions.** A request to demolish a historic garage will utilize the following in determining the eligibility for demolition:
  - a. An existing structure of architectural or historical significance shall be retained if repairs are reasonably possible.
  - b. An existing structure is dilapidated, leaning, lacking a solid foundation, or of substandard construction, it may be eligible for demolition.
  - c. An existing structure is 240 square feet or less, it may be eligible for demolition.
  - d. An existing structure was built after the period of significance; it may be eligible for demolition.
  - e. The removal of existing historic structure will enable access to the rear yard where no access currently exists; it may be eligible for demolition.
- .5 **New Garage Construction.** A new garage shall be compatible in form, scale, size, materials, features, and finish with the principal structure. The following criteria will be considered for a new garage constructed where there is currently no historic structure:
  - a. The new structure will utilize alley access if available.
  - b. The new footprint will be 575 square feet or 50% of the footprint of the principal structure, whichever is smaller.
  - c. The cumulative of square footages for all garage structures on the lot, shall be no greater than the footprint of the principal structure.
  - d. New garage are to be subservient to the principal structure and in no case will the

garage structure be taller, wider or deeper than the principal structure.

- e. The proposed construction will preserve existing trees.
- f. Maximum of two garages are allowed per site.

**.6 New Garage Height.** New garage structures shall be the traditional height and proportion of garages in the district. New garages in blocks that contain only one-story garages shall be one-story. One and a half story and two-story garages may be built if located on a block where one and a half story and two-story garages are dominant or if adjacent properties contain similar height garages. The wall height and height of roof ridge are to be no greater than the principal structure.

**.7 New Garage Location.** New garage structures that are not replacing a historic garage are to be located behind the principal structure in the rear yard with limited or no visibility from the front right-of-way. Garages replacing historic garages shall maintain the location and configuration of a historic garage, typically at the end of a front driveway. Such garages shall be located behind the back elevation of the principal structure.

**.8 New Garage Materials.** The following may be considered on a case-by-case basis for new garages:

- a. Acceptable materials include wood, brick and stone masonry, and stucco. Fiber cement products for new garage construction located off an alleyway or if setback behind the rear of the house will be considered on a case-by-case basis. It should be noted that wood siding does not have “wood grain.” Only smooth cement board is permitted. The use of vinyl, Masonite, aluminum or other metal sidings is prohibited.
- b. Aluminum clad doors and windows are allowed for garages located off an alleyway or behind the rear elevation of the house, with no or limited visibility from the front right-of-way.
- c. Wood, wood composite or metal overhead garage doors with wood/wood composite trim are allowed.
- d. Garage doors shall be a single width. Double width garage doors will be considered on a case-by-case basis.

**.9 Additions to Garage Structures.** Additions to existing garages may be appropriate if not visible from the front right-of-way. Additions shall not be greater than the footprint of the existing garage. Additions must match the materials and design of existing garage structure.

**.10 Reconstruction of Historic Garage.** The reconstruction of out buildings shall be based on historic evidence, such as photographs, Sanborn maps or other documentation. If no such evidence exists, the design should be derived from the architectural style of the principal building and historic patterns and characteristics of the historic district. Wood, brick and stucco are appropriate materials for reconstruction of a historic garage. Overhead garage doors with the appearance of double doors will be considered on a case-by-case basis. Historic garages shall be located at the end of a driveway along the side property line and face the front street right-of-way.

**.11 Replacement Garage Doors.** Retain and preserve wood overhead garage doors on historic

garages. Retain double doors if possible. Replacement overhead garage doors with the appearance of double doors will be considered on a case-by-case basis. For historic garages, and garages that face the front or are visible from the right-of-way the following replacement door is allowed:

- a. Wood is preferred. However, wood composite or metal with composite trim can be considered on a case-by-case basis. Vinyl is prohibited.
- b. The original size, height and width of doors must be maintained.
- c. Designs must match the style of the original historic garage door.

**.12 Carports.** Carports shall be unattached to the primary structure and meet the following:

- a. Located in the rear yard behind the principal structure, with no visibility from the front right-of-way(s). Corner lots are considered to have two front elevations.
- b. Constructed of wood or masonry. Cement fiberboard to be considered on a case-by-case basis.
- c. Maximum footprint size of 400 square feet with an eave height no greater than 10 feet.
- d. In no case shall the carport be taller, wider or deeper than the historic principal structure of the lot.

## Accessory Structures less than 400 square feet

### 2.5 Standards for Administrative Bypass

*The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, the application will be forwarded to the Historic District Commission for a full review.*

**.1 Small Accessory Structures 120 square feet or less.** Must meet the following:

- a. No greater than 120 square feet footprint. Owner/applicant must meet the building code requirement for a building permit.
- b. The design of accessory buildings are compatible with the primary structure and surrounding district.
- c. Located in the rear yard with no visibility from the front right-of-way.
- d. Metal and vinyl exterior materials are prohibited.

### 2.6 Guidelines

*A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):*

**.1 Preserve Accessory Structures.** When possible, retain and preserve historic accessory structures in their original locations and configurations. Even if the function changes, the exterior appearance shall remain the same.

**.2 Preserve Original Materials.** When possible, retain and preserve character-defining materials, features, and details of historic accessory structures, including foundations, siding, masonry, windows, doors, and architectural trim. When necessary, repair character-defining materials, features, and details of historic accessory structures in accordance with pertinent guidelines.

**.3 Replace Only Deteriorated Portions.** If replacement of a deteriorated element or detail of an historic accessory building is necessary, replace only the deteriorated portion in-kind rather than replacing the entire feature. Match the original in design, dimension, texture, and material. Compatible substitute materials can be considered if in-kind replacement material are not available or feasible.

**.4 Request for Accessory Structure Demolitions.** A request to demolish a historic accessory structure will utilize the following in determining the eligibility for demolition:

- a. An existing structure of architectural or historical significance shall be retained if repairs are reasonably possible.
- b. An existing structure is dilapidated, leaning, lacking a solid foundation, or of substandard construction, it may be eligible for demolition.
- c. An existing structure is 240 square feet or less, it may be eligible for demolition.
- d. An existing structure was built after the period of significance; it may be eligible for demolition.
- e. The removal of existing historic structure will enable access to the rear yard where no access currently exists; it may be eligible for demolition.

**.5 Make New Construction Compatible.** Accessory structures greater than 120 square feet but less than 400 square feet shall be compatible in form, scale, size, materials, features, and finish with the principal structure. New construction must meet the following:

- a. Located in the rear yard, and not visible from front right-of-way.
- b. Compatible in design, style, material to the principal historic structure and the surrounding historic neighborhood.
- c. Select materials and finishes for proposed new accessory buildings that found in historic structures in the district in terms of composition, scale, pattern, detail, texture, and finish. Acceptable materials include brick and stone masonry, stucco and wood. Cement fiberboard will be considered on a case-by-case basis when there is limited visibility from the front right-of-way. Structures with no visibility from the front may utilize cement fiberboard. No metal or vinyl structures allowed.
- d. New accessory structures shall be one-story in height and less than 10 feet in wall height.

Structures with a footprint of 400 square feet and greater and/or taller than one-story will be reviewed utilizing the either the Guidelines for Secondary Structures or the Guidelines for Garages.



## Secondary Structures

### 2.7 Guidelines.

*A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):*

- .1 **Secondary structures.** Secondary structures are accessory structures with a footprint of 400 square feet or greater and/or taller than one-story, examples of a secondary structures are garage apartments, studios, workshops and cabanas.
- .2 **Preserve Secondary Structures.** When possible, retain and preserve historic secondary structures in their original locations and configurations. Even if the function changes, the exterior appearance shall remain the same.
- .3 **Preserve Original Materials.** When possible, retain and preserve character-defining materials, features, and details of historic secondary structures, including foundations, siding, masonry, windows, doors, and architectural trim. When necessary, repair character-defining materials, features, and details of secondary structures in accordance with pertinent guidelines.
- .4 **Replace Only Deteriorated Portions.** If replacement of a deteriorated element or detail of an historic secondary structure is necessary, replace only the deteriorated portion in-kind rather than replacing the entire feature. Match the original in design, dimension, texture, and material. Consider compatible substitute materials only if using the original material is not technically feasible.
- .5 **Request for Secondary Structure Demolitions.** The following will be utilized to assess a demolition request for a secondary structure:
  - a. An existing structure of architectural or historical significance shall be retained if repairs are reasonably possible.
  - b. An existing structure is dilapidated, leaning, lacking a solid foundation, or of substandard construction, it may be eligible for demolition.
  - c. An existing structure is 240 square feet or less, it may be eligible for demolition.
  - d. An existing structure was built after the period of significance; it may be eligible for demolition.
  - e. The removal of existing historic structure will enable access to the rear yard where no access currently exists; it may be eligible for demolition.
- .6 **Make New Construction Compatible.** Secondary accessory structures are to be compatible with the principal structure and surrounding district and in no case overwhelm the principal structure. Construction of secondary accessory structures will utilize the following criteria for new construction:
  - a. Match in design, style, and material to the principal historic structure and the surrounding historic neighborhood.

- b. Compatible with the principal historic structure and/or the district in regards to materials, size, scale, height, form, massing, proportions, spacing and size of window and door openings, window to wall proportions and traditional setbacks seen in the neighborhood.

**.7 Size of New Secondary Structures.** New secondary accessory structures are to be subservient to the principal structure in no case will the secondary structure be taller, wider or deeper than the principal structure. The size of a secondary structure is limited to 575 square feet or 50% of the principal structure footprint. The cumulative of square footages for all accessory structures and garages on the lot, shall be no greater than the footprint of the principal structure.

**.8 Location and Setbacks of Secondary Structures.** New secondary structures are to maintain traditional locations and setbacks seen in the neighborhood. Locations are to be in the rear yard, with limited or no visibility from front right-of-way, unless there historical indications of a different location. Corner lots are considered to have two front elevations.

**.9 Windows and Doors for Secondary Accessory Structures.** Select doors and windows for new secondary accessory buildings that are compatible in material, proportion, pattern, and detail with the doors and windows of historic buildings in the district. See Windows and Door Guidelines.

**.10 Materials.** Select materials and finishes for proposed new buildings that found in historic buildings in the district in terms of composition, scale, pattern, detail, texture, and finish. Acceptable materials include brick and stone masonry, stucco and wood. Cement fiberboard will be considered on a case-by-case basis for those structures located behind the back elevation of the principal structure but with limited visibility from the front right-of-way. Metal and vinyl exterior materials are prohibited.

**.11 Avoid False Historical Appearance.** New secondary accessory structures are to be compatible with the style, age and character of the principal structure and district without creating a false historical appearance. New structures are to be of their own time and differentiated from the historic structure while maintaining compatibility with the principal structure and the character of the neighborhood.

## Sidewalks, Driveways, and Off-Street Parking

### 2.8 Standards for Administrative Bypass

*The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, the application will be forwarded to the Historic District Commission for a full review.*

- .1 Driveways.** Widening of an existing driveway or the installation of a new driveway to a maximum width of 10 feet. Driveways are to be constructed from materials allowed by city codes.

Approaches can be widened to a maximum of 16 feet.

.2 **Concrete Areas.** Concrete patios/areas 300 square feet or less and not visible from the front right-of-way (s). Corner lots are considered to have two front elevations.

.3 **Parking pads.** Parking pads 400 square feet or less are allowed if located off alley and vehicles parked on the parking pad not visible from the front right-of-way (s). Corner lots are considered to have two front elevations.

.4 **Walkways.** Private sidewalks and walkways in the rear yard.

## 2.9 Guidelines

*A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):*

.1 **Front Driveway Location.** Preserve and retain historic front driveways locations. New or expanded front driveways shall be perpendicular to the street, except in individual cases where there is historical documentation of an alternate configuration. Unless there is historic documentation otherwise, driveways shall be located along the property line on one side of the house.

.2 **Driveway Width.** Driveways shall be one car width, not to exceed 10 feet wide, unless there is historic documentation of an alternate configuration. Driveway width may vary as it approaches a garage in order to correspond to the width of the door opening.

.3 **New Driveway Composition.** Driveways shall be constructed from material allowed by the City Code. Existing gravel driveways may remain in place subject to other provisions in the City Code.

.4 **Ribbon Driveways.** Ribbon driveways are permitted to remain or may be newly installed in historic districts. The minimum width of ribbon paving is 18 inches.

.5 **Driveway Approaches.** Maintain the rhythm of existing approaches when introducing new driveways. Driveway approaches may be a maximum of 16 feet wide at the curb, narrowing to 10 feet at the sidewalk or property line.

.6 **Circular Drives.** Drives connecting to the street by two or more curb cut openings are not permitted in front yards or corner side yards unless demonstrated as historically present on the specific property in question.

.7 **Shared Driveways.** Historic driveways shared by two adjacent properties may be retained and preserved.

.8 **Sidewalk Location.** Sidewalks on private property shall be maintained in their traditional location, usually perpendicular to the street, unless there is historical documentation of another

location.

**.9 Sidewalks and Curbs.** Public sidewalks and curbs on the street shall be constructed of finished concrete. Sidewalks and curbs on private property may be constructed of finished concrete, brick, or stone.

**.10 New Paved Areas.** New paved areas should not directly abut the principal site structure, significantly alter the site topography, or overwhelm in area the residential, landscaped character of a rear or side yard. Care must be taken that paved areas do not injure nearby trees by intruding onto their root areas. They shall be designed to be compatible in location, patterns, spacing, configurations, dimensions, and materials with existing walkways and driveways. Paved areas shall not overwhelm the principal structure.

**.11 Rear Yard Area.** New parking areas are permitted off alleyway with no visibility or limited visibility from the front right-of-way(s). Corner lots are considered to have two front elevations. Rear yard parking must meet Norman City Codes.

**.12 Side Yard Parking Area.** The establishment of parking areas adjacent to the side of historic structures is not allowed.

**.13 Front Yard Parking Area.** Parking areas in the front yard of the property are prohibited except within an existing driveway.

## Fences and Masonry Walls

### 2.10 Standards for Administrative Bypass.

*The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, the application will be forwarded to the Historic District Commission for a full review.*

**.1 Repair of Fences.** If an existing fence or wall is replaced with a fence that is the same in material, height, location, and design; it will be considered ordinary maintenance and repair and will not require a Certificate of Appropriateness.

**.2 Installation of Fences.** Front and side yard fences of up to 4 feet in height and rear yard fences of up to 6 feet in height, may be approved by Administrative Bypass if they meet the following criteria:

- a. Composed of the following materials: wood, cast iron, iron, twisted wire, painted aluminum that mimics the appearance of cast iron or iron fences or a combination of these materials. Chain link, stone, brick, or stucco walls will be forwarded to the Historic District Commission for review. Vinyl fences are prohibited.
- b. Of traditional or historic design, contemporary designs/horizontal designs will be forwarded to the Commission for review.
- c. No footing required. Walls or fences that require a footing shall be forwarded to the

Commission for review.

## 2.11 Guidelines

*A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):*

- .1 Replacing Conforming Fences.** If an existing, conforming type of fence or wall is being replaced with one that is the same in material, height, placement, and style, a Certificate of Appropriateness is not required.
- .2 Materials.** Retain and preserve historic wall and fence materials that contribute to the overall historic character of a building. Acceptable materials for new fences and walls are wood, brick, stone, cast iron, iron, twisted wire, painted aluminum that mimics the appearance of cast iron or iron fences. Vinyl is prohibited. A 4 foot chain link in the side or rear yards will be considered on a case-by-case basis.
- .3 Front Yard Fences.** Front yard fences taller than 4 feet are prohibited by the *Norman Zoning Ordinance*.
- .4 Side Yard Fences.** Side yard fences of up to 4 feet in height may be approved by Administrative Bypass. Side yard fences taller than 4 feet require a COA. Side yard fences taller than 6 feet are prohibited.
- .5 Rear Yard Fences.** Rear yard fences of a contemporary design or of non-traditional materials or of height greater than 8 feet will be considered on a case-by-case basis. Such fences will be review for their impact to the historic structure and the District as a whole. The Norman Zoning Ordinance prohibits rear yard fences taller than 8 feet.
- .6 Fences on Corner Properties Adjacent to Alleys.** Fences on corner properties with alley access shall be located very carefully to maximize sight lines and minimize conflicts between alley traffic, pedestrians, and on-street traffic.
- .7 Fence and Wall Materials.** Fences or walls shall be constructed of wood, brick, stone, iron or cast or forged metal, stucco, or a combination of these materials. Stone or brick used in walls shall be compatible in size, scale, and style to that used elsewhere in the historic district, or typical of residential structures of this type, age, and location. No vinyl, cinder block, concrete block, or corrugated metal, may be used for fences or walls in historic districts. Chain link in the rear yard will be considered on a case-by-case basis.
- .8 Colors and Finishes.** Although paint color is not regulated by the Commission, it is strongly recommended that wood fences be stained or painted in colors and finishes appropriate to the style and period of the property and the district or left unfinished. No decorative murals shall be applied to fence or wall surfaces visible from the street.
- .9 Finished Side Out.** Fences or walls facing the street shall be constructed with the finished side out.

**.10 Setback and Adjacent Property Tie-In.** A fence 4 feet or less in height shall be set back a minimum of 1 foot from the inner edge of a public sidewalk. Where no sidewalk exists, fences shall be set back a minimum of 6 feet from the back of curb or edge of pavement. If a fence exists on an adjacent property, the corner side yard fence shall tie into the existing fence.

## Signage

### 2.12 Standards for Administrative Bypass

*The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, the application will be forwarded to the Historic District Commission for a full review.*

**.1 National Register of Historic Places Plaques.** A National Register of Historic Place commemorative plaques, if less than 2 square feet, bronze, mounted so that will not permanently damage the exterior façade material or impact the architectural features of the structure of the historic structure.

### 2.13 Guidelines

*A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):*

**.1 Sign Ordinance Also Applies.** In addition to a review by the Historic District Commission, signs will be subject to the regulations and permitting requirements established in Chapter 18 of the Code of Norman, Oklahoma, also referred to as the Sign Ordinance. Applicants shall coordinate the design and placement of any sign in a historic district with the Sign Ordinance as well as these guidelines.

**.2 Signs Must Be Compatible.** Size, design, and placement of a sign shall relate to the architectural elements of the structure. Signs shall be compatible with other signs and other structures along the street.

**.3 Non-Contributing Resources.** Signs associated with non-contributing structures will be controlled only to the degree necessary to make them compatible with the general atmosphere of the district.

## Non-Contributing Resources

### 2.14 Guidelines

*A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):*



**.1 Preservation Guidelines Apply.** The Historic Preservation Guidelines apply to all structures in Norman's Historic Districts, both contributing and non-contributing.

**.2 Support Harmony Between Old and New.** Non-contributing structures shall be controlled only to the degree necessary to make them compatible with the general atmosphere of the district with regard to alterations, additions, changes to the site, and the like. As with all requests for Certificates of Appropriateness in historic districts, each project will be evaluated on its own merits for overall impact on the district as a whole.

## **Building Exteriors**

### **Exterior Walls**

#### **3.1 Standards for Administrative Bypass**

*The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, the application will be forwarded to the Historic District Commission for a full review.*

**.1 Removal of wall materials.** Removal of non-original or contemporary synthetic materials to reveal existing historic materials is permitted. If existing historic siding material underneath the non-original or contemporary synthetic materials has been removed, the reinstallation of appropriate/compatible material requires review by the Historic District Commission.

#### **3.2 Guidelines**

*A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):*

**.1 Preserve Original Walls.** Retain and preserve exterior walls that contribute to the overall historic form and character of a building, including functional and decorative features and details.

**.2 Retain Original Building Materials.** Retain and preserve exterior wall materials that contribute to the overall historic character of a building.

**.3 Replace Only Deteriorated Portions.** If replacement of a deteriorated wall or feature is necessary, replace only the deteriorated portion in-kind rather than the entire feature. Match the original in material, design, dimension, detail, texture, and pattern. Compatible substitute materials can be considered if in-kind replacement material are not available or feasible.

**.4 Avoid Covering Original Materials.** Building materials and decorative elements are important character-defining components of historic buildings. It is not appropriate to remove or cover any wall material or detail with coatings or contemporary substitute materials. Vinyl

and aluminum siding is not appropriate for use in historic districts.

**.5 Replace Missing Features.** When replacing an exterior wall or feature, replace it with a new wall or feature based on accurate documentation of the original or a new design that is compatible with the historic character of the building and the district. Compatible substitute materials can be considered if in-kind replacement material are not available or feasible.

**.6 Avoid False Historical Appearances.** Features or details of walls and fences that are introduced to a property shall reflect its style, period, and design. Fences and walls features shall not create a false historical appearance by reflecting other time periods, styles, or geographic regions of the country.

**.7 Substitute Materials.** Cement fiberboard (e.g. Hardiplank® siding) will be considered on a case-by-case basis. Exterior insulating and finish systems (EIFS) will not be considered for use in historic structures.

## Wood Features

### 3.3 Guidelines

*A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):*

**.1 Preserve Original Features.** Retain and preserve wood features that contribute to the overall historic character of a building, including siding, shingles, cornices, brackets, pediments, columns, balustrades, and architectural trim.

**.2 Replace Only Deteriorated Elements.** If replacement of a deteriorated details or element of a wood feature is necessary, replace only the deteriorated detail or element in-kind rather than the entire feature. Match the original in design, dimension, texture, and material. Compatible substitute materials can be considered if in-kind replacement material are not available or feasible.

**.3 Replace Missing Features.** Replace missing wooden features based on accurate documentation of the missing original or a new design compatible in scale, size, material, and texture, with the style, period, and design of the historic building and the district as a whole. Compatible substitute materials can be considered if in-kind replacement material are not available or feasible.

**.4 Avoid False Historical Appearances.** Features or details that are introduced to a house shall reflect its style, period, and design. Features shall not create a false historical appearance by reflecting other time periods, styles, or geographic regions of the country.

**.5 Rough Sawn Wood.** Avoid using rough sawn wood as is not appropriate for installation in historic buildings.

**.6 Skirts.** All solid skirt materials shall have vents installed to allow air to pass under the house and eliminate moisture from the wood foundation.

- .7 **Treated Wood.** All treated wood shall be thoroughly dried prior to installation.
- .8 **Cleaning.** Do not use excessive water pressure or sandblasting on wood surfaces as it pits the wood.
- .9 **Defining Features.** Retain corner boards and window trim as they are character-defining features on houses with wood siding or replaced with historic accuracy.

## Masonry and Brick Features

### 3.4 Guidelines

*A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):*

- .1 **Preserve Original Features.** Retain and preserve masonry features that contribute to the overall historic character of a building, including foundations, chimneys, cornices, steps, piers, columns, lintels, arches, and sills. Installing brick or block where these materials were not originally used is prohibited. Installing brick on the walls of a house that originally had wood siding is prohibited as it changes the character of the house and can destroy the wood beneath.
- .2 **Preserve Original Materials.** Retain and preserve historic masonry materials, such as brick, terra-cotta, limestone, granite, stucco, slate, concrete, cement block, and clay tile, and their distinctive construction features.
- .3 **Replace Only Deteriorated Elements.** If replacement of a deteriorated detail or elements of masonry feature is necessary, replace only the deteriorated in-kind rather than replacing the entire feature. Compatible substitute materials can be considered if in-kind replacement material are not available or feasible.
- .4 **Replace Surfaces Only As Necessary.** Replace large masonry surfaces in-kind only as necessary, matching the original in design, detail, dimension, color, pattern, texture, and material. Consider substitute materials only if using the original material is no longer available.
- .5 **Replace Missing Features.** Replace missing masonry and brick features based on accurate documentation of the missing original or a new design compatible in size, scale, material, and texture with the style, period, and design of the historic building and the district as a whole. Compatible substitute materials can be considered if in-kind replacement material are not available or feasible.
- .6 **Preserve Unpainted Surfaces.** It is not appropriate to paint unpainted masonry and brick surfaces that were not painted historically. Repaint previously painted masonry surfaces in colors appropriate to the historic building material, the building, and the district.
- .7 **Chimneys.** Retain and preserve primary chimneys. If a primary chimney, often used as a flue rather than fireplace, is to be removed from the interior of the house, retain the portion above

the roofline. A platform will need to be constructed in the attic to carry the weight of the chimney. A secondary non-functional chimney visible from the front right-of-way will be reviewed for removal on a case-by-case basis.

**.8 Demolition of Chimneys.** Chimneys are a character-defining feature and shall be retained and maintained. If the foundation of the chimney has failed or the chimney is badly deteriorated, the chimney can be carefully dismantled and reconstructed using original materials or materials matching the original. Mortar shall match the original in composition and joint profile.

**.9 Materials.** Replace loose or missing mortar with one of the same composition as the original. Mortar is important to the integrity of the brick wall. If the mortar is missing, its replacement shall match the historic mortar in composition, color, and joint width. Use a sand-lime recipe for mortar, which is compatible with the old brick. Modern masonry mortar has cement as a main ingredient, which is too hard for historic brick. A high Portland cement content will trap moisture in the brick and cause it to deteriorate.

**.10 Flashing.** Repair or replace flashing as needed to ensure a watertight connection between the chimney and roof.

**.11 Cleaning.** Historic buildings shall be cleaned in the gentlest means possible which typically includes water and soft bristle brushes. Sandblasting and high-pressure washing can cause irreparable damage to brick and are not permissible. Any chemical cleaner must be tested in small areas of limited visibility to ensure compatibility and effectiveness on the brick.

## Stone

### 3.5 Guidelines

*A full review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):*

**.1 Replacing Deteriorated Elements.** Replace deteriorated stone with stone that matches the original in color and texture.

**.2 Mortar.** Replace deteriorated or missing mortar with mortar of the same composition as the original in composition and color.

**.3 Portland Cement.** Do not use Portland Cement on historic stone structures. Portland cement, or masons mortar, is too hard and will cause the stone to deteriorate and crumble.

**.4 Foundation.** The addition of stone to the foundation or exterior of a house is prohibited.

**.5 Walls.** Retain and preserve historic stonewalls.

**.6 Chemicals.** Any chemical cleaner must be tested in small areas of limited visibility to ensure compatibility and effectiveness on the stone. Some chemicals may burn the face of stone.

## Historic Block and CMU (Concrete Masonry Unit)

### 3.6 Guidelines

*A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):*

- .1 **Retain Original Materials.** Retain historic concrete block as a building material and maintain it.
- .2 **Mortar.** Replace deteriorated or missing mortar with mortar of the same composition and joint profile.
- .3 **Paint.** Painted concrete block shall remain painted.
- .4 **Landscape.** Retain and maintain historic concrete block. This may include repairing or reconstructing foundations.
- .5 **Contemporary Concrete Masonry Units.** Contemporary CMU is not appropriate for use on a historic structure.

## Synthetic Materials / Stucco

### 3.7 Guidelines

*A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):*

- .1 **Retain Original Materials.** Retain and repair the original building material. Installing any synthetic building material or stucco on top of existing wood is prohibited. Many of these materials can trap moisture in the wall, which will cause the wood beneath to deteriorate. It can also trap moisture in the insulation, which reduces the value of the insulation.
- .2 **Replace Deteriorated Materials.** Replace only that material which is beyond repair with visually compatible new material. Match the original in profile as closely as possible.
- .3 **Retain Character Defining Features.** Installing synthetic siding on top of an existing siding as a way of “modernizing” the house or attempting to make the house more energy efficient is prohibited. This changes the character of the original design and frequently destroys the character-defining features of the house and neighborhood.
- .4 **Stucco.** Stucco is a material that may develop hairline cracks over time. It shall be gently washed with low pressure and allowed to dry thoroughly. The application of an elastomeric paint will cover most hairline cracks and provide some flexibility at those locations.
- .5 **Details.** Retain details as corner boards, windows and door surrounds, gable vents and rafter ends.

**.6 Cement Fiberboard.** Cement fiberboard (Hardieplank®) and synthetic wood materials are prohibited except for new construction. These are not comparable substitutes for wood siding except in certain applications. A good use of cement board siding is where it is in contact with the ground, such as the skirt of a pier-and-beam house. Be sure to retain ventilation of the crawl space. If using cement board, use smooth only. Wood used in historic houses was sanded smooth with no obvious grain.

## Metal

### 3.8 Guidelines

*A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):*

- .1 Replacing Deteriorated Material.** Replace deteriorated metal with new primed metal of the same or compatible material. Metal materials shall not be used to replace wood or other historic non-metal materials.
- .2 Aluminum.** Aluminum shall not replace wood as a building material but is used for cornices and other details on many buildings. This is especially true of doors and windows and their frames. If aluminum appears to be the only option as a replacement material for deteriorated wood, the aluminum shall be of similar profile and shall have a factory painted finish. Mill finish or “shiny” aluminum shall not be used on a historic building to replace a previously painted material.
- .3 Paint.** It is important to keep pressed metal, cast iron and steel well painted to avoid rust and deterioration.
- .4 Decorative Details.** Retain metal decorative roof details when replacing the primary roofing material.
- .5 Decorative Iron.** Do not create a false history by installing decorative iron work over windows that did not include them in the original design.
- .6 Pressed Metal.** Do not create a false history by installing a pressed metal skirt where one did not previously exist.

## Roofs

### 3.9 Standards for Administrative Bypass

*The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, the application will be forwarded to the Historic District Commission for a full review.*

- .1 Re-Roofing.** Reroofing with in-kind materials with no change to the shape, pitch, or structure of the roof. Replacement in-kind of existing, non-historic composition roofing material



with any type of contemporary asphalt, laminated or composition shingles is not subject to review and does not require a Certificate of Appropriateness.

**.2 Gutters.** Replacement or and installation of non-historic gutters and downspouts in-kind is not subject to review and does not require a Certificate of Appropriateness.

**.3 New Features.** New roof features such as skylights, solar tubes, and equipment such as power ventilators, solar collectors, photovoltaics, and antennae that are:

- a. Located on rear of the structure, and not visible from the front right of way right-of-way. Corner lots are considered to have two front elevations.

**.4 Removal of Secondary Chimneys.** The removal of a non-functional, secondary chimney is allowed by Administrative Bypass if not visible from the front right-of-way.

### 3.10 Guidelines

*A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):*

**.1 Preserve Original Features.** Retain and preserve historic wood, tile and slate roofs as well as roof features that contribute to the overall historic character of a building, such as cresting, dormers, cupolas, and cornices.

**.2 Replace Only Deteriorated Portions of Roof Features.** If replacement of a deteriorated roof feature is necessary, replace only the deteriorated portion in-kind to match the original feature in design, dimension, detail, and material. Compatible substitute materials can be considered if in-kind replacement material are not available or feasible.

**.3 Replacements Match Original.** If full replacement of historic roofing material or feature is necessary, replace it in-kind, matching the original in scale, detail, pattern, design, and material. Compatible substitute materials can be considered if in-kind replacement material are not available or feasible.

**.4 Replace Missing Features.** Replace missing roof features based on accurate documentation of the missing original or a new design compatible in scale, size, and material with the style, period, and design of the historic building and the district as a whole.

**.5 Built-In Gutters.** Retain and preserve built-in gutter systems.

**.6 Locate New Features and Mechanical Equipment Carefully.** New roof features such as dormers, skylights, and solar tubes, and equipment such as power ventilators, solar collectors, photovoltaics, and antennae, shall be introduced carefully so as not to compromise the historic roof design, or damage character-defining roof materials, or the overall character of the historic district.

**.7 Retain the Original Roof Form and Details.** If attic space is converted into living space and dormers are added, retain the original roof pitch to avoid a “pop-up” appearance, especially on the front façade. Avoid adding details that did not exist originally.

.8 **Existing Dormers.** Original dormers shall be preserved and only elements beyond repair may be replaced. If a replacement is needed, original size and shape shall be maintained.

.9 **New Dormers.** New dormers must be functional, to allow light in or to add more living space, they should not be merely decorative and should be in keeping with the style of the historic house. They shall be located on the rear and inset from first-floor side wall below it. Set new dormers back from eave and do not extend above the ridge of roof.

.10 **Alternative Materials for Roofs.** Metal simulated clay, slate or other designs as well as other materials will be reviewed on a case-by-case basis to see if appropriate to the historic structure and compatible with the surrounding historic district.

## Windows

### 3.11 Standards for Administrative Bypass

*The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed below. If they do not meet the criteria, the application will be forwarded to the Historic District Commission for a full review.*

.1 **Window Replacement.** An historic window that is deteriorated more than 50% and is not repairable may be replaced in-kind if it meets the following:

- a. Replace original windows in-kind, meaning match the original in material and finish.
- b. Muntin width and profile are same as the original in width and profile.
- c. Light pattern is the same as the original.
- d. True divided lights (panes) are the same as the original glass thickness.
- e. Size and dimension of all window components are the same as the original.
- f. Replacement of less than 50% of the windows on a given elevation.

.2 **Storm Windows and Screens.** The use of interior storm windows is encouraged. Installation of exterior storm windows are allowable if they meet the following criteria:

- a. Wood framed, full-light storms and screens that are low profile and align with meeting rails of the window.
- b. Relatively unobtrusive, narrow-profile, metal exterior storm windows that do not obscure the window itself, that are carefully installed to prevent damage to the sill or the frame, and that are finished in a painted or a baked-enamel color compatible with the sash color are allowed. Storm window rails are to align with meeting rails of the window.
- c. The use of ¼ inch thick clear laminated glass for the purposes of weatherization and noise reduction maybe used in storm windows.

.3 **Awnings.** Window awnings that conform to following criteria:

- a. Material is fabric.
- b. Of traditional style and shape.
- c. Located on the rear of the structure.
- d. Installed over windows, doors, storefronts, or porch openings with care to ensure that historic features are not damaged or obscured.

### 3.12 Guidelines

*A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):*

- .1 **Retain Original Windows.** Retain and preserve original windows, including glass, frames, sash, muntins, sills, heads, moldings, surrounds, and hardware.
- .2 **Retain Historic Glass.** Retain original glass in historic windows if at all possible. Leaded glass windows shall be preserved. Bubbles and waves give old glass its distinctive look and add to the historic character of the house.
- .3 **Glass Replacement.** Individual panes of historic glass that have been broken or cracked, may be replaced with modern-day clear glass. Salvaged historic glass or reproduction historic “wavy” glass is also acceptable replacement historic glass was present.
- .4 **Glass Variations.**
  - a. Privacy glass may only be located in the rear or on the side of the structure, where not visible from the front. Smoked or tinted glass is not appropriate for use in historic structures.
  - b. Beveled glass in doors and windows is allowed as long as it is compatible with style of the historic building and the original configuration of window panes remains.
  - c. Colored glass may be used in transoms and sidelights if supported by historical documentation or compatible with the architectural style.
- .5 **Replace Only Deteriorated Features.** If replacement of a deteriorated window or door feature or details is necessary, replace only the deteriorated feature in-kind rather than the entire unit. Broken sash cords, for example, can be repaired and do not necessitate replacing an entire window. Match the original in design, dimension, placement, and material.
- .6 **Sash Replacement.** Replacement sash, often referred to as sash replacement kits, are acceptable for use in historic structures. However, replacement window sash shall be unclad wood, with single-pane thickness, true divided light patterns that match the historic muntin pattern and profile of the house.
- .7 **Window Replacement.** An original window that is deteriorated more than 50% and is not repairable may be replaced in-kind if it meets the following:
  - a. Shall have a wood exterior, unless replacing a metal casement window.
  - b. Light patterns same as the original.

- c. Size and dimension the same as the original.
- d. Double-pane simulated divided lights with wood muntins on the exterior and interior and a shadow bar between the panes may be allowed for windows on the side or rear that are not visible from the street.

**.8 Retain Original Metal Windows.** Replace original metal casement windows only as a last resort after weatherization measures have proven unsuccessful.

**.9 Preserve Original Openings.** Do not create new openings in the front or side façades of historic structures. Do not enlarge or diminish existing openings to fit stock window sizes. If new openings are necessary to meet code requirements, they shall be compatible with historic windows for that structure in proportion, shape, location, pattern, size, materials, and details.

**.10 Materials.** Wood is allowable for in-kind replacement of windows. Aluminum-clad and metal windows can be considered for the replacement of metal casement windows that are deteriorated on a case-by-case basis. Fiberglass and aluminum-clad windows can be considered on non-contributing resources and on rear elevations not visible from the front right-of-way. Vinyl-clad windows are prohibited for both contributing and non-contributing structures in the historic districts.

**.11 New Primary and Secondary Accessory Structures.** Windows in new construction are to be compatible with in adjacent historic structures in terms of size, profile, design, proportions, and material. Wood and aluminum clad windows are acceptable for use in new construction.

**.12 Additions.** For construction of additions, choose windows that match the original structure. While single-pane, true divided light, wood frame windows are the most desirable choice for new construction in historic districts, double-pane glass wood windows with interior and exterior applied muntins and shadow bars between the panes are permitted. Aluminum cladding of wooden windows is permissible for use in additions. Vinyl or vinyl-clad windows are prohibited.

**.13 Install Awnings Carefully.** Install fabric awnings over window, doors, storefronts, or porch openings with care to ensure that historic features are not damaged or obscured. Awnings composed of wood or metal are not permitted unless there is historic documentation of their use.

## Doors

### 3.13 Standards for Administrative Bypass

*The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed below. If they do not meet the criteria, the application will be forwarded to the Historic District Commission for a full review.*

**.1 Door Replacement.** A deteriorated door that is not repairable may be replaced in-kind, meaning a door that matches the original in materials and design. A non-original door may be

replaced with a wood door that is appropriate design for the house and the historic district.

**.2 Screen Door Replacement.** Screen doors shall be retained and repaired when necessary. Any replacement screen door shall match the historic screen door and shall be built to mirror the panels and sash divisions of the door that it covers.

**.3 Storm Doors and Screens.** Storm doors constructed of wood or metal that do not obscure or damage the existing door and frame. Storm doors required to be painted, stained, or have a baked-enamel finish color compatible with the color of the existing door. If storm and screen doors are installed where none existed originally, select a “full vision panel” design to allow the original door to be seen. (Additional information on storm windows and doors is provided in Section 3.17, Utilities and Energy Retrofit).

### 3.14 Guidelines

*A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):*

**.1 Retain and Preserve Original Doors.** Retain and preserve original doors and door surrounds including frames, glazing, panels, sidelights, fanlights, surrounds, thresholds, and hardware on front doors and side doors visible from the street.

**.2 Replace Only Deteriorated Features.** If replacement of a deteriorated door feature or details is necessary, replace only the deteriorated feature in-kind rather than the entire unit.

**.3 Retain and Preserve Transoms and Sidelights.** Transoms and sidelights should be retained and preserved. Avoid altering transoms and sidelights as it distorts the strong vertical proportions of the windows and doors and changes the character of the residence.

**.4 Retain Historic Glass.** Retain original glass in historic doors. Bubbles and waves give old glass its distinctive look and add to the historic character of the house.

#### **.5 Glass Variations**

- a. Privacy glass may only be located in the rear or on the side of the structure, where not visible from the front. Smoked or tinted glass is not appropriate for use in historic structures.
- b. Beveled glass in doors is allowed as long as it is compatible with style of the historic building and the original configuration of window panes remains.
- c. Colored glass may be used in transoms and sidelights if supported by historical documentation or compatible with the architectural style.

**.6 Wood Doors.** Wood doors are required unless there is documentation that other materials were historically used on a particular structure. Keep wood doors appropriately stained or painted to protect from weather.

**.7 Replacement Doors.** Replacement doors on a historic structure are to be wood and in

appropriate design, size and details in keeping with the style of the house. Installation of steel doors on the front of a historic structure is prohibited. Aluminum clad doors are permissible on rear of the structure upon review on a case-by-case basis.

**.8 Preserve Original Openings.** Do not create new openings in the front or side façades of historic structures. Do not enlarge or diminish existing openings to fit stock door sizes. If new openings are necessary to meet code requirements, they shall be compatible with historic doors for that structure in proportion, shape, location, pattern, size, materials, and details.

**.9 Materials.** Wood is allowable for in-kind replacement of doors. Fiberglass and aluminum-clad doors can be considered on non-contributing resources and on rear elevations of historic structures when not visible from the front right-of-way. Vinyl is prohibited for historic and non-contributing structures.

**.10 New Primary and Secondary Accessory Structures.** Doors in new construction shall be similar to those in adjacent historic structures in terms of size, profile, design, proportions, and material. Aluminum clad and fiberglass doors with limited or no visibility from the front façade can be considered on a case-by-case basis.

**.11 Additions.** For construction of additions, choose doors that match the original structure. Aluminum-clad wood doors are permissible for use in additions that are not visible from the front right-of-way. Fiberglass doors can be considered on a case-by-case basis.

## Entrances, Porches, and Balconies

### 3.15 Standards for Administrative Bypass

*The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, the application will be forwarded to the Historic District Commission for review.*

**.1 Screening of a rear porch.** Screening of a rear porch that is temporary, easily reversible, and is designed to preserve the historic character of the porch and the building. Screening must be with compatible materials.

**.2 Balconies and Porches.** Balconies and porches that are less than 120 square feet, built on the rear and not visible from the front right-of-way and compatible with the structure in material, scale, and size.

**.3 Handrails.** Installation of handrails required by building code may be approvable by Administrative Bypass. Handrails must meet adopted City building codes and be of a simple design that is compatible with the house in material and scale. Wood or metal are acceptable materials for handrails on historic structures.



**.4 Concrete Steps and Porch floorings.** Replacement of existing concrete steps and porch flooring in-kind, with the same materials and design. Steps are to match the original steps in size, form and detail. The number of steps shall be retained if possible, unless building codes require a different configuration.

### 3.16 Guidelines

*The Historic District Commission will use following criteria for review of a Certificate of Appropriateness (COA):*

**.1 Preserve Original Entrances, Porches, and Balconies.** Retain and preserve entrances, porches, and balconies that contribute to the overall historic character of a building, including columns, pilasters, piers, entablatures, balustrades, sidelights, fanlights, transoms, steps, railings, floors, and ceilings.

**.2 Replace Only Deteriorated Elements.** If replacement of a deteriorated detail or element of an entrance, porch, or balcony feature is necessary, replace only the deteriorated detail or element in-kind rather than the entire feature. Match the original in design, dimension, and material. Compatible substitute materials can be considered only if using the original material is not available.

**.3 Match Original.** If full replacement of an entrance, porch, or balcony is necessary, replace it in-kind, matching the original in design, dimension, detail, texture, and material. Compatible substitute materials can be considered only if original material is no longer available.

**.4 Replace Missing Features.** Replace missing entrance, porch, or balcony features with a new feature based on accurate documentation of the missing original or a new design compatible with the historic character of the building and the district.

**.5 Screen Porches Carefully.** Consider the screening of a historic porch only if the alteration is reversible and can be designed to preserve the historic character of the porch and the building.

**.6 Avoid Enclosures.** It is not appropriate to enclose a front porch or a front balcony.

**.7 Avoid Removing Details.** It is not appropriate to remove any detail material associated with entrances and porches, such as graining, beveled glass, or bead board, unless an accurate restoration requires it.

**.8 Avoid Changes to Primary Façades.** It is not appropriate to remove an original entrance or porch or to add a new entrance or porch on a primary façade.

**.9 Avoid False Historical Appearances.** Features or details that are introduced to a house shall reflect its style, period, and design. Features shall not create a false historical appearance by reflecting other time periods, styles, or geographic regions of the country.

**.10 Maintain Porch Elevation.** At no time shall the porch elevation be lowered to grade and

steps redesigned.

**.11 Maintain Wood Elements.** Wood porch floors and columns may require an eventual replacement due to moisture penetration; wood floors and columns shall only be replaced with wood of the same profile and dimension.

**12. New Balconies and Porches.** Balconies and porches built on the rear and not visible from the front right-of-way are to be constructed to be compatible with the principal structure in material, scale, and size. New balconies or porches on the front or side of a historic structure will only be considered if there is historic evidence that one existed. The design and materials are to be based on historic evidence of the design or be a design seen in similar structures in the historic neighborhood.

**13. Respect Design.** Original design, construction, and materials shall be respected on primary façades. Installation of non-original materials, such as decorative tile, is not appropriate.

## Utilities and Energy Retrofit

### 3.17 Standards for Administrative Bypass

*The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, the application will be forwarded to the Historic District Commission for a full review.*

**.1 Storm Windows and Doors.** Interior storm windows are encouraged and do not require a COA. Exterior storm windows are allowable with a COA by administrative bypass if they meet the following criteria:

- a. Metal storm windows and windows with painted, stained, or baked-enamel finish color compatible with the color of the existing window or door. Unfinished or clear anodized aluminum finishes are not permitted.
- b. Storm windows and doors that do not obscure or damage the existing window/door and/or frame.

**.2 Solar Panels.** Solar panels installed on the “back” side of the house, or on the roof where they are not visible from the front right-of-way or public view.

**.3 Freestanding Solar Racks.** Solar racks can be installed at the rear of the property to create a shade structure or can be installed on an outbuilding, such as a garage roof, as long as they meet the following:

- a. Located in the rear yard and not visible from the front right-of-way. Not taller than the principal structure. Less than 120 square feet.

**.4 Solar Tubes and Skylights.** If flat in profile and on the rear or back side of the house, and not visible from the front right-of-way.

### 3.18 Guidelines

*A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):*

1. **Retain Inherent Energy-Conserving Features.** Retain and preserve the inherent energy-conserving features of historic buildings and their sites, including shade trees, porches, awnings, as well as operable windows, transoms, shutters, and blinds.
2. **Use Traditional Energy-Saving Practices.** Increase the thermal efficiency of historic buildings by observing appropriate traditional practices, such as weather stripping and caulking, and by introducing energy-efficient features such as awnings, operable shutters, and storm windows and doors, where appropriate.
3. **Solar Tubes and Skylights.** Solar Tubes and Skylights can add light to interior spaces and make attics spaces more useable. Bubble-dome skylights are not appropriate for buildings within historic districts.
4. **Solar Panels.** Avoid installing solar panels on the street side of the house or permanently altering roof with the installation of solar panels. Panels shall be installed flat and not alter the slope of the roof. They shall be positioned behind existing architectural features such as parapets, dormers, and chimneys to limit their visibility.
5. **Compatibility.** Use solar panels and mounting systems that are compatible in color to the property's roof materials.
6. **Free-Standing Solar Racks.** Free-standing solar racks larger than 120 square feet will be considered on a case-by-case basis. Solar racks installed at the rear of the property with no or limited visibility and create a shade structure or installed on an outbuilding, such as on a garage roof.
7. **Low Pitch Roofs for Solar Panels.** Low pitch roofs may utilize low-profile panels on non-street-facing roof planes. Avoid roof racks that elevate the panels or are at a different pitch than the roof.
8. **Solar Shingles.** Solar shingles may be installed on sloped roof-surfaces and are less intrusive than panels. However, removal of historic materials must be avoided.
9. **Flat Roofs.** On structures with flat roofs, solar panel installations are to set back from the roof edge to minimize visibility. Pitch and elevation shall be adjusted to reduce visibility from public right-of-way.

## Accessibility, Health & Safety Considerations

### 3.19 Standards for Administrative Bypass

*The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, the application will be forwarded to the Historic District Commission for a full review.*

**.1 Access Ramp.** Access ramps can be approved by Administrative Bypass if they meet the following standards:

- a. Wood, wood-like materials, such as smooth cement fiberboard, and temporary metal ramps can be used.
- b. Vinyl material is prohibited.
- c. Temporary and removable, and do not permanently alter the historic structure.
- d. Located on the rear of the structure, not visible from the front right-of-way.
- e. Side and front ramps require review by the Historic District Commission.

**.2 Safety Aid.** Elements such as handrails, grab bars, or other safety aids shall be added in a way that preserves character-defining features and finishes of the structure and allows them to be removed when no longer needed.

**.3 Doorways.** The widening of entryways can be approved by administrative bypass if located on the rear of the structure and not visible from the front right-of-way.

### 3.20 Guidelines

*A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):*

**.1 Security Bars.** A Certificate of Appropriateness is required for the installation of security bars within historic districts. Security bars shall be designed to complement the style and design characteristics of the structure to which they are being attached.

**.2 Accessibility Ramps.** The Commission will use the following when considering accessibility ramps on the front façade or side of structure:

- a. Locate ramp with the least amount of visibility from the front right-of-way.
- b. Ramps must be temporary and composed of wood, cement fiberboard, or metal. Concrete ramps on the rear of the structure will be considered on a case-by-case basis.
- c. Cannot permanently alter the historic structure or be permanently attached to the structure.
- d. Must be easily removable and reversible.

**.3 Lifts Require Approval.** Accessibility lifts that require concrete, brick or other more

permanent foundations are permissible on the rear of the structure with no visibility from the front right-of-way.

- .4 **Add Safety Aids Carefully.** Elements such as handrails, grab bars, or other safety aids shall be added in a way that preserves character-defining features and finishes of the structure and allows them to be removed when no longer needed.
- .5 **Modify Doorways Carefully.** The enlargement of a door opening on the rear of the structure is allowable upon review on a case-by-case basis.

## Additions and New Construction

### Decks

#### 4.1 Standards for Administrative Bypass:

*The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, the application will be forwarded to the Historic District Commission for a full review.*

##### 1 Decks under 300 Square Feet

- a. Less than 300 square feet in total area. Located behind the structure and not visible from the front right-of-way. Corner lots have two front right-of-ways.
- b. Constructed in a way that makes no permanent changes to the historic structure. Built of compatible wood, wood composite or smooth cement board with functional elements made of metal elements. Synthetic, materials such as plastic and vinyl are prohibited.
- c. Decks with roofs or walls will be forwarded as a porch or balcony request for a full review by the Historic District Commission.

#### 4.2 Guidelines

*A full review by the Historic District Commission will take the following criteria into consideration before issuing a Certificate of Appropriateness (COA):*

- .1 **Protect Historic Structure.** Locate and construct decks so that the historic fabric of the primary structure and its character-defining features and details are not damaged or obscured. Install decks so that they are structurally self-supporting and may be removed in the future without damage to the historic structure.
- .2 **Deck Locations.** Front decks are prohibited. Decks on the rear shall be inset from the rear corners to eliminate visibility from the front right-of-way. Decks on corner properties will

be reviewed on a case-by-case basis.

- .3 **Deck Design Shall Reflect Building Design.** Design decks and their associated railings and steps to reflect the materials, scale, and proportions of the building.
- .4 **Align Deck with First Floor Level.** Decks shall be no higher than the building's first-floor level. Visually tie the deck to the building by screening with compatible foundation materials such as skirt boards, lattice, or dense evergreen foundation plantings.
- .5 **Preserve Significant Building Elements.** Preserve significant building and site elements and new deck installations are not to obscure or remove significant building or site elements.
- .6 **Decks May Not Detract from Overall Character.** It is not appropriate to introduce a deck if it will detract from the overall historic character of the building or the site.

## Additions to Historic Buildings

### 4.4 Guidelines

*A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):*

- .1 **Make Additions Compatible.** Additions shall be compatible with the historic building in size, scale, mass, materials, proportions and the pattern of windows and doors to solid walls.
- .2 **Locate Addition Inconspicuously.** Locate a new addition on an inconspicuous façade of the historic building, usually the rear one. Additions that alter the front façade are generally considered inappropriate for a historic structure.
- .3 **Limit Size and Scale.** The footprint of the addition shall not exceed 50% of the footprint of the existing structure or 750 square feet, whichever is greater. Exterior dimensions of the addition shall not exceed the exterior dimensions of the existing structure, including height, width, and depth. An addition which does not increase the footprint of the existing structure may be allowed to increase roof height and will be reviewed on a case-by-case basis.
- .4 **Preserve the Site.** Design new additions so that the overall character of the site, character-defining site features, and trees, are retained.
- .5 **Avoid Detracting From Principal Building.** It is not appropriate to construct an addition if it will detract from the overall historic character of the principal building and the site, or if it will require the removal of a significant building element or site feature. Construct new additions so that character-defining features of the historic buildings are not destroyed, damaged, or obscured.



## New Primary Structures

### 4.5 Guidelines

*A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):*

- .1 **Consider Historic Context.** Design new structures to be compatible with historic buildings in the district in terms of size, scale, height, form, massing, proportions, finished floor elevation, size of door and window openings, roof shape, and setbacks. Proposals for new construction shall include streetscape elevation drawings that depict proposed structure as well as elevations of properties on either side to provide a comparison of massing, scale, floor elevations, proportions, setback and design.
- .2 **Select Windows and Doors Carefully.** Select windows and doors for new buildings that are compatible in material, proportion, pattern, and detail with the windows and doors of historic buildings in the district. See Chapters 3.11 through 3.14.
- .3 **Select Compatible Finishes.** Select materials and finishes for proposed new buildings that are compatible with historic materials and finishes found in historic buildings in the district in terms of composition, scale, pattern, detail, texture, and finish.
- .4 **Design.** Design new primary structures to be compatible with historic buildings in the district in terms of size, scale, height, form, massing, proportion, finished floor elevation, size of door and window openings, and roof shape. Proposals for new primary structures shall include streetscape elevation drawings that depict proposed structure as well as elevations of properties on either side to provide a comparison of massing, scale, and design.
- .5 **Location.** New primary structures shall align with the typical front and side setback on the block.
- .6 **Evaluate Potential for Archaeological Resources.** Evaluate in advance and limit any disturbance to the site's terrain during construction to minimize the possibility of destroying unknown archaeological resources.
- .7 **Avoid False Historical Appearance.** New structures shall be of their own time period and easily distinguishable from the historic structure.

## Relocation and Demolition

### Relocation of Structures

#### 5.1 Standards for Administrative Bypass

*The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, the application will be forwarded to the Historic District Commission for a full review.*

**.1 Relocation of Structures Less Than 120 Square Feet.** Non-historic accessory structure less than 120 square feet may be relocated to another location in the rear yard not visible from the front right-of-way. Relocation outside the district is allowed as well.

#### 5.2 Guidelines

*A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):*

**.1 Document Original Context.** Before moving a historic structure, applicants and City staff shall document its original setting and context using photographs, site plans, or other graphic or written statements to record the existing site conditions.

**.2 Protect Existing Structures.** Ensure that the relocation of a structure will not diminish or damage existing buildings or the overall character of the historic district. Pay particular attention to protection of the tree canopy along the route of the move.

**.3 Furnish Relocation Site Plans.** Applicants shall provide the Historic District Commission with detailed site plans for proposed site features and plantings of the new setting, including information on accessory buildings, driveways, site lighting, and parking areas.

**.4 Protect Significant Features.** Protect significant site features of the original site, the new site, and the route of the move during the relocation.

## Demolition of Structures

### 5.3 Standards for Administrative Bypass

*The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, the application will be forwarded to the Historic District Commission for a full review.*

- .1 **Demolition of Structures Less Than 120 Square Feet.** Non-historic accessory structure less than 120 square feet are eligible for demolition.

### 5.4 Guidelines

*The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, then the application will be forwarded to the Historic District Commission for a full review.*

- .1 **A Certificate of Appropriateness.** A Certificate of Appropriateness is required to be issued prior to demolition.
- .2 **Criteria for Demolition.** Demolition requests must meet Zoning Ordinance Section 429.3.9(c), Criteria for Demolition.
- .3 **Procedures and Process for Demolitions.** Demolitions must meet the Zoning Ordinance Section 429.3.9(b), Procedure and Postponement Orders.
- .4 **Site Plan Required.** Applicants shall provide the Historic District Commission with detailed site plans for proposed site features of the new parcel, including information any structures, driveways, site lighting, and parking areas.
- .5 **Document Thoroughly.** Document original context of the historic structure prior to demolition.

# Appendices

## 6.1 Technical Resources

### Local Resources

City of Norman  
Planning and Community Development  
201 A West Gray Street  
Norman, OK 73069

<https://www.normanok.gov/your-government/departments/planning-and-community-development/planning-and-zoning/historic>

For information on Norman Historic Districts, Certificates of Appropriateness and technical assistance, contact the Historic Preservation Officer at (405) 366-5322.

### State Resources

State of Oklahoma Historic Preservation Office  
Oklahoma Historical Society  
2401 N. Laird Avenue  
Oklahoma City, OK 73105

<https://www.okhistory.org/shpo/index>

For information on historic structures throughout Oklahoma, the National Register of Historic Places, preservation tax credit credits, and technical restoration assistance, call (405) 521-6249.

Oklahoma Archaeological Survey  
111 E. Chesapeake  
Norman, OK 73019

<https://www.ou.edu/archsurvey>

For information on archaeological sites, resource protection, and Volunteer opportunities, contact the Survey at (405) 325-7211.

### National Resources

US Department of the Interior  
National Park Service  
1849 C Street NW  
Washington, DC 20240

Office of the Director (202) 208-4621  
Office Communications (202) 208-6843  
Cultural Resource Stewardship and Partnerships (20) 208-7625  
Heritage Preservation Services

Intermountain Regional Office of the National Park Service  
<https://www.nps.gov/subjects/nationalhistoriclandmarks/contact-us-intermountain-region.htm>  
 12795 Alameda Parkway  
 Denver, CO 80225  
 (303) 987-6690

For information on all national park properties and NPS activities in AX, CO, MT, NM, OK, TX, UT and WY

## 6.2 Definitions

**Addition** - construction that increases any exterior dimension of an original structure by building outside of the existing walls and/or roof. Additions can be either horizontal or vertical.

**Alteration** - an act that changes one or more of the exterior architectural features of a structure or its appurtenances, including but not limited to the erection, construction, reconstruction, or removal of any structure or appurtenance.

**Appropriate** - typical of the historic architectural style, compatible with the character of the historic district, and consistent with the Norman Historic Preservation Handbook.

**Architectural resources** - districts, structures, buildings, monuments, sites, or landscaping which possess local interest or artistic merit or which are particularly representative of their class or period, or represent achievements in architecture, engineering, or design.

**Certificate of Appropriateness (COA)** - the official document issued by the Historic District Commission approving any application affecting the exterior of any structure designated by the authority of the Historic District Ordinance for permission to construct, erect, demolish, remove, relocate, reconstruct, restore, or alter said structure.

**Commission** - the Historic District Commission of the City of Norman.

**Compatible** - a design or use that does not conflict with the historical appearance of a building or district and does not require irreversible alteration.

**Contributing resource** - a historic building or site that retains the essential architectural integrity of its original design or condition and whose architectural style is typical of or integral to a historic district.

**Damaged or diseased tree** - a tree that is damaged in such a way as to create a hazard (e.g. has a large wound) or has been pruned in a way which permanently alters its natural attributes (e.g. topped). A seriously diseased tree is one with obvious signs of internal decay (e.g. cavity with fruiting bodies present), is infested with a disease for which there is no remedy (e.g. Pine Wilt, Dutch Elm Disease), or suffers from a decline disorder.

**Demolition** - the removal of any historic structure from its original site. This includes moving a building from one site to another.

**Elevation** - a drawing showing the vertical elements of a building, either exterior or interior, as a direct projection to a vertical plane.

**Façade** - the exterior face of a building.

**False historical appearance** - architectural features or details introduced to a structure that do not reflect its period, style, or design.

**Feature** - a structural or decorative element that contributes to the overall character of that building, e.g. walls, foundations, roofs, chimneys, steps, piers, columns, lintels, and sills.

**Guidelines** - Guidelines are utilized by the Norman Historic District Commission to determine if a proposed work is compatible with the principal historic structure on the site as well as compatible with the adjacent or surrounding historic district.

**Historic district** - a geographically definable area with a concentration or linkage of significant sites, buildings, structures, or monuments; or, an individual structure, building, site or monument which contributes to the cultural, social, political, or architectural heritage of the City of Norman.

**Historic District Ordinance** - the portion of Norman Zoning Ordinance (Chapter 22:429.3HD) establishing an overlay zoning district for the purpose of protecting and preserving the architectural, cultural, and historic resources included in that designated district.

**Historic property** - any individual structure, building, site or monument which contributes to the historic, architectural, archeological and/or cultural heritage of the City of Norman, Oklahoma as determined by the Historic District Commission.

**Historic resources** - sites, districts, structures, buildings, or monuments that represent facets of history in the locality, state or nation; places where significant historical or unusual events occurred; places associated with a personality or group important to the past.

**Infill construction** - the erection of a new structure between or adjacent to existing buildings or the relocation of an existing structure to a vacant lot from another location.

**In-kind** - the replacement of existing materials or features with materials of identical appearance and/or composition. (See also: matching)

**Like with like** - repair or replacement of deteriorated exterior features or site elements with identical materials.

**Matching** - in historic rehabilitations, the use of replacement materials that are identical to the original in composition, size, shape, and profile. (See also: in-kind).

**National Register of Historic Places** - the national list of districts, sites, buildings, structures, and objects significant in American history, architecture, archeology, engineering and culture, maintained by the Secretary of the Interior under authority of Section 101(a)(1)(A) of the National Historic Preservation Act, as amended.

**New construction** - see: infill construction.

**Non-contributing resource** - a resource that adds no historical significance to an individual property, site, or district, and detracts from the visual integrity or interpretability of an historic district.

**Ordinary maintenance and repair** - work meant to remedy damage or deterioration of a structure or its appurtenances, and which will involve no change in materials, dimensions, design, configuration, texture or visual appearance to the exterior of an historic structure. Ordinary maintenance and repair shall include painting and



reroofing with similar materials.

**Original** - buildings, building materials or features that were present during the period of significance for the historic district.

**Period of significance** - the span of time during which a group of properties attained the significance that makes them eligible for designation as a historic district.

**Preservation** - the adaptive use, conservation, protection, reconstruction, rehabilitation, or stabilization of buildings, districts, monuments, sites, or structures significant to the heritage of the people of Norman. The following terms further define types of preservation activities:

*Adaptive Use* – the restrained alteration of a historical or architectural resource to accommodate uses for which the resource was not originally constructed, but in such a way so as to maintain the general historical and architectural character.

*Conservation* – the sustained use and appearance of a resource essentially in its existing state.

*Protection* – the security of a resource as it exists through the establishment of the mechanisms of this section.

*Reconstruction* – the act or process of duplicating the original structure, building form and materials by means of new construction based on documentation of the historic condition.

*Rehabilitation* – the act or process of making a compatible use for a property through repair, alterations, and additions while preserving those portions or features which convey its historic, cultural or architectural values.

*Restoration* — the act or the process of accurately depicting the form, features, and character of a property as it appeared at a particular period of time by removing features or changes from other periods in its history and reconstructing missing features from the restoration period.

*Stabilization* – the process of applying methods designated to halt deterioration and to establish the structural stability of an unsafe or deteriorated resource while maintaining the essential form as it presently exists without noticeably changing the exterior appearance of the resource.

**Relocation** - the movement or repositioning of a primary or accessory structure on its original site, or from one location to another.

**Secretary of the Interior Standards for Rehabilitation of Historic Buildings** - a set of standards intended to assist the long-term preservation of a historic property through the preservation of historic building materials and features. The Standards for Rehabilitation pertain to historic buildings of all materials, construction types, sizes, and occupancy and encompass the exterior and interior of the buildings. The Secretary of the Interior describes rehabilitation as “the process of returning a property to a state of utility, through repair or alteration, which makes possible an efficient contemporary use while still preserving those portions and features of the property which are significant to its historic, architectural, and cultural values.”

**Significant characteristics** - those characteristics which are important to or expressive of the historic or architectural quality and integrity of the resources and its setting and which include, but are not limited to building material, detail, height, proportion, rhythm, scale, setback, setting, shape, street accessories, and workmanship. Examples include:

*Building mass* - describes the relationship of a building's height to its width and depth.

*Building materials* - the physical characteristics which create the aesthetic and structural appearance of the resource, including but not limited to a consideration of the texture and style of the components and their combinations, such as brick, stone, shingle, wood, concrete, or stucco.

*Detail* - architectural aspects which, due to particular treatment, draw attention to certain parts or features of a structure.

*Height* - the vertical dimension of a given structure, building or monument.

*Proportion* - the relative physical sizes within and between buildings and building components.

*Rhythm* - a discernible pattern of shapes including, but not limited to, windows, doors, projections, and heights, within a building, structure or monument, or a group of same.

*Scale* - the proportion of parts of a building, structure, or monument to one another and to the human figure.

*Setting* - the surrounding structures, monuments, and landscaping which establish the visual, aesthetic, or auditory qualities of the historic or architectural resources.

*Shape* - the physical configuration of structures or landscaping and their component parts.

**Streetscape** - the view along a street from the perspective of a driver or pedestrian. The streetscape includes street trees, lawns, buildings, landscape buffers, signs, street lights, above-ground utilities, drainage structures, sidewalks, bus stop shelters and street furniture.

**Structure** - anything constructed or erected, the use of which requires permanent location on the ground or which is attached to something having a permanent location on the ground. These include, but are not limited to, buildings, fences, walls, driveways, sidewalks and parking areas.

**Stucco** - an exterior finish, usually textured, composed of Portland cement, lime, and sand mixed with water. Older types of stucco may be mixed from softer masonry cement rather than Portland cement.

## 6.3 Glossary

**Aluminum siding** - sheets of exterior architectural covering, usually with a colored finish, fabricated of aluminum to approximate the appearance of wooden siding. Aluminum siding was developed in the early 1940s and became increasingly common in the 1950s and the 1960s.

**Asbestos siding** - dense, rigid board containing a high proportion of asbestos fibers bonded with Portland cement; resistant to fire, flame, or weathering and having a low resistance to heat flow. It is usually applied as large overlapping shingles. Asbestos siding was applied to many buildings in the 1950s.

**Asphalt siding** - siding manufactured from saturated construction felts (rag, asbestos, or fiberglass) with asphalt and finished with mineral granules on the side exposed to weather. It sometimes displays designs seeking to imitate brick or stone. Asphalt siding was applied to many buildings in the 1950s.

**Attached structure** - a building that is structurally connected to the primary building on the site.

**Attic ventilator** - in houses, an attic ventilator is a screened or louvered opening, sometimes in decorative shapes,

located on gables or soffits.

**Awning** - a roof-like covering of canvas, often adjustable, over a window, a door, etc., to provide protection against sun, rain, and wind. Aluminum awnings were developed in the 1950s.

**Balustrade** - a low barrier formed of balusters, or uprights, supporting a railing.

**Band, band course, band mold, belt** - flat trim running horizontally in the wall to denote a division in the wall plane or a change in level.

**Bay** - within a structure a regularly repeated spatial element usually defined in plan by beams and their supports, or in elevation by repetition of windows and doors in the building façade.

**Beveled glass** - glass panes whose edges are ground and polished at a slight angle to create a visual pattern.

**Board-and-batten** - closely applied vertical boards, the joints of which are covered by vertical narrow wooden strips; usually found on Gothic Revival-style buildings.

**Bond** - the laying of bricks or stones regularly in a wall according to a recognized pattern for strength. Masonry bond is essential to brickwork when wire reinforcement is not used.

**Bracket** - projecting support members found under eaves or overhangs; may be plain or decorated

**Capital** - the top or head of a column. In classical architecture there exist orders of columns: Doric, Ionic, Corinthian, Tuscan, and Composite.

**Casement window** - a window that swings open along its entire length, usually on hinges fixed to the sides of the opening into which it is fitted.

**Casing** - the exposed trim molding, framing, or lining around a door or a window; may be either flat or molded.

**Clapboard** - horizontal wooden boards, tapered at the upper end and laid so as to cover a portion of a similar board underneath and to be covered by a similar one above. The exposed face of clapboard is usually less than 6 inches wide. This was a common outer face of nineteenth and early twentieth century buildings.

**Column** - a vertical shaft or pillar that supports or appears to support a load.

**Composition board** - a building board, usually intended to resemble clapboard, fabricated from wood or paper fabric under pressure and at an elevated temperature, usually with a binder.

**Coping** - the cap or the top course of a masonry wall.

**Comer block** - a block placed at a corner of the casing around a wooden door or window frame, usually treated ornamentally.

**Comer board** - one of the narrow vertical boards at the corner of a traditional wooden frame building, into which the clapboards butt.

**Cornice** - the top part of an entablature, usually molded and projecting; originally intended to carry the eaves of a roof beyond the outer surface.

**Cupola** - a small vault on top of a roof; sometimes spherical in shape, sometimes square with a mansard or conical roof.

**Damaged or diseased tree** - a tree that is damaged in such a way as to create a hazard (e.g. has a large wound) or has been pruned in a way which permanently alters its natural attributes (e.g. topped). A seriously diseased tree is one with obvious signs of internal decay (e.g. cavity with fruiting bodies present), is infested with a disease for which there is no remedy (e.g. Pine Wilt, Dutch Elm Disease), or suffers from a decline disorder.

**Deck** - an uncovered porch, usually at the rear of a building; popular in modern residential design.

**Demolition** - the destruction or removal of any historic structure from its original site.

**Dentil** - a repetitive cubical element at the base of a classical cornice. Dentils resemble teeth.

**Detached structure** - a building that is not structurally connected to the primary building on the site.

**Development pattern** - the configuration of residential lots, the location and orientation of structures on the lots, and the relationship of lots and buildings to the street.

**Dormer** - a structure containing a window (or windows) that projects through a pitched roof.

**Double hung window** - a window with two sashes that open and close by sliding up and down in a cased frame.

**Downspout** - a vertical pipe, often of sheet metal, used to conduct water from a roof drain or gutter to the ground or a cistern.

**Eave** - the part of a sloping roof that projects beyond a wall.

**Fanlight** - an arched over door light whose form and tracery suggest an open fan.

**Fascia** - a flat board with a vertical face that forms the trim along the edge of a flat roof, or along the horizontal, or eave side of a pitched roof. The rain gutter is often mounted on it.

**Feature** - a structural or decorative element that contributes to the overall character of that building, e.g., walls, foundations, roofs, chimneys, steps, piers, columns, lintels, and sills.

**Fenestration** - the windows and doors and the pattern of their openings in a building.

**Finial** - a formal ornament at the top of a canopy, gable, pinnacle, street- light, etc.

**Flashing** - a thin impervious material placed in construction to prevent water penetration, to provide water drainage, or both, especially between a roof and a wall.

**Foundation** - the supporting portion of a structure below the first-floor construction, or below grade, including

*footings.*

**French window** - a long window reaching to floor level and opening in two leaves like a pair of doors.

**Gable** - the vertical triangular piece of a wall at the end of a ridged roof, from the level of the eaves to the summit.

**Gambrel roof** - a gable roof more or less symmetrical, having four inclined surfaces, the pair meeting at the ridge having a shallower pitch.

**Guidelines** - a set of rules administered by the Norman Historic District Commission intended to assist owners of historic buildings in Norman's historic districts maintain, preserve, protect, and enhance the architectural quality of their property.

**Gutter** - a shallow channel of metal or wood set immediately below or built in along the eaves of a building to catch and carry off rainwater.

**Hardscape** - any material which is impervious to water and not covered by roof.

**Header** - a brick laid across the thickness of a wall to bond together different widths of a wall; the exposed end of a brick.

**Hipped roof** - a roof without gables, each of whose sides, generally four, lies in a single plane and joins the others at an apex or ridge.

**Historic rehabilitation** - the process of returning a historical or architectural resource to a state of efficiency or soundness by repair or alteration designed to encourage its continued use but without noticeably changing the historic exterior appearance of the resource.

**Jamb** - the vertical sides of an opening, usually for a door or a window.

**Jerkin head roof** - a roof whose end has been formed into a shape midway between a gable and a hip, resulting in a truncated or clipped "A" appearance; sometimes called clipped gable.

**Lattice** - a network, often diagonal, of interlocking lath or other thin strips used as screening, especially in the base of a porch.

**Light** - a pane of glass.

**Lintel** - a horizontal member spanning an opening and supporting construction above; a beam.

**Like with like** - repair or replacement of deteriorated exterior features or site elements with identical materials

**Lunette** - a semicircular opening.

**Mass** - the overall bulk, size, volume, or magnitude of a structure.

**Molding** - a decorative band having a constant profile or having a pattern in low relief, generally used in cornices

or as trim around openings.

**Mortar** - a mixture of Portland cement, lime, putty, and sand in various proportions, used for laying bricks or stones. Until the use of hard Portland cement became a standard building material, softer lime-day or lime-sand mortars and masonry cement were common.

**Mullion** - a vertical member dividing a window area and forming part of the window frame.

**Muntin** - a molding forming part of the frame of a window sash and holding one side of a pane.

**New construction** - see definition for infill construction.

**Non-contributing structure** - a structure that adds no historical significance to an individual property or district, and detracts from the visual integrity or interpretability of an historic district.

**Patio** - an open, outdoor living space adjacent to a building, usually surfaced with stone, tiles, or concrete and at ground level.

**Pergola** - an arbor or a passageway of columns supporting a roof of trelliswork on which climbing plants may be trained to grow.

**Pilaster** - a flat or half-round member applied at a wall suggesting a column; sometimes called engaged column. Pilasters can also be structural members, as in a partially exposed column within a wall.

**Porte cochere** - a roofed passageway large enough for wheeled vehicles to pass through. Literal definition: a carriage door.

**Portico** - a small entrance porch or covered walk consisting of a roof supported by open columns.

**Portland cement** - a type of hydraulic cement (one that hardens under water) made by heating a slurry of clay and limestone in a kiln.

**Preservation Guidelines** - see definition for Guidelines.

**Prevailing height** - the most commonly occurring height on a block face on which a project is proposed.

**Prevailing lot coverage** - the most commonly occurring lot coverage on the block and across the street.

**Rehabilitation** - the act or the process of making possible a compatible use for a property through repair, alterations, and additions while preserving the portions or the features that convey the property's historical, cultural, or architectural values.

**Repointing** - raking out deteriorated mortar joints and filling into them with a surface mortar to repair the joint.

**Restoration** - the act or the process of accurately depicting the form, features, and character of a property as it appeared at a particular period of time by removing features or changes from other periods in its history and



*reconstructing missing features from the restoration period.*

**Riser** - the vertical portion of a stair, connecting two steps.

**Roofing Tile** - a tile for roofing, usually of burnt clay; available in many configurations and types including plain, single-lap, and interlocking.

**Sash** - the moving part of a window.

**Scale** - the proportion of parts of a building, structure, or monument to one another, to surrounding structures, and to the human figure.

**Shingle** - a roofing unit of wood, asphalt, slate, tile, or other material cut to stock lengths, widths, and thicknesses; used as an exterior covering on roofs and applied in an overlapping fashion.

**Sidelight** - a narrow window area beside an outside door, generally seen in Greek Revival style.

**Sheet metal** - a flat, rolled-metal product, rectangular in cross-section and form; when used as roofing material, usually terne or zinc-plated.

**Sill** - the lowest horizontal member in a wall opening.

**Soffit** - the exposed undersurface of any overhead component of a building, such as an arch, balcony, beam, cornice, lintel, or vault.

**Sound** - materials and structures that may show wear but retain their original form and function, e.g. sound wood is not rotted.

**Standards** - refers to the Secretary of the Interior Standards for Rehabilitation of Historic Buildings.

**Stretcher** - a brick or a stone laid with its length parallel to the length of the wall.

**Stucco** - an exterior finish, usually textured, composed of Portland cement, lime, and sand mixed with water. Older-type stucco may be mixed from softer masonry cement rather than Portland cement.

**Surround** - the molded trim around a door or window opening.

**Terra-cotta** - hard unglazed fired clay, used for ornamental work and roof and floor tile; also fabricated with a decorative glaze and used as a surface finish for buildings in the Art Deco style.

**Tongue and groove lumber** - a joinery system in which boards are milled with a tongue on one side and a groove on the other so that they can be tightly joined with a flush surface alignment.

**Transom, or over door light** - a glazed panel above a door or a store-front, sometimes hinged to be opened for ventilation at ceiling level.

**Trim** - the finish material on a building, such as moldings applied around openings or at the floors and the ceilings

of rooms.

**Turret** - a small tower, usually corbelled from a corner.

**Vinyl siding** - sheets of thermal plastic compound made from chloride or vinyl acetates, as well as some plastics made from styrene and other chemicals, usually fabricated to resemble clapboard, sometimes used to cover wood building exteriors.

**Water blasting** - a cleaning method similar to sandblasting except that water is used as the abrasive. As in sandblasting, high-pressure water jets can damage wood and masonry surfaces. Water blasting is also known as power washing.

DRAFT

Public Comments- Revised Historic Preservation Guidelines						
Name	Source of Comment	Address	Historic District	Comment	Question	Misc Info
Kathleen Wallis	Comment left via feedback		Resides/Owns in Miller HD		Will high impact shingles be allowed?	Sent email back 7-26-2 & 7-26-21
Benny Ellis	Phone call to staff		Owens property in Chautauqua HD	Would like the Commission to consider allow property owners to replace all windows for energy efficiency reasons. Old properties loose heat and cold through single panes.		Spoke to him on the phone and told him I would relay his concerns. Sent him hard copies of PP presentation from the July 19th public meeting. Also hard copy of tracked changes.
Cheryl Clayton	July 19 Public Comment Meeting	503 Tulsa Street	Resides/Owns in Southridge HD	Administrative Bypass is limited to installation of items in rear yard that are not visible, this does not work for a corner lots. Does not think corner lots should have "two fronts" as laid out in Guidelines.		Staff explained that the Guidelines that since at least the 2008 version of the Guidelines have stated that corner lots have 2 fronts.
				Cement fiberboard or hardieboard should be allowed out right. Other cities Historic Districts, such as Baltimore, allow cement board outright.		
				Accessory structures should not have to match the principal structure. If you look in the neighborhood, the historic accessory structures were made out of wood most of the time. Metal sheds are not historic. Metal sheds are not historic. Are Tuff sheds outlawed? Those should be allowed.		
				The proposal to allow parking pads 400 square feet or less in the rear yard with no visibility is too small. It should be at a minimum 650 sq ft. House should be user friendly, residents should be able to step out of their vehicles onto concrete not 6' tall fences on the sides of the house should be approvable by administrative bypass.		
				Increase the size of National Register Plaque approvable by Admin Bypass to 15" x 15" instead of 2 square feet.		
				The proposed revised Guidelines require chimneys to be repaired and supported internally in the attic. Why can't they just be removed if they are deteriorated since they are no longer used? (she is talking about secondary chimneys that no longer		
				The proposed revised Guidelines does not allow metal roofs. Metal shingles is what she has on her house and they should be allowed, just like asphalt shingles.		
				All access ramps should be allowed, even if on the front of the structure and made out of concrete. Applicants should not have to go HDC. should be approvable by		
Lary Lessman	July 19 Public Comment Meeting	600 Miller Ave	Owens/resides in the Miller HD	Thinks the definition of two fronts for a corner lot should be clarified in the Guidelines		Staff explained that corner lots do receive more scrutiny than interior lots, in order to protect the historic character of the neighborhood as a whole. Encourage alterations to historic structures in the rear with no or less visibility.
David John	July 19 Public Comment Meeting	410 S Peters	Owens/resides in the Miller HD	Historic houses in Norman do have metal roofs. The revised Guidelines should allow metal roofs by Commission review.		
				Asked: Isn't the property in the three Historic Districts zoned R-1 and therefore garage apartments are not allowed?		Staff explained that there were areas in both Miller and Chautauqua Historic Districts that are zoned R-2 an R-3 that would allow for garage apartment. Regardless, the HD Commission does not have purvey over zoning. Accessory structure requests can allow be reviewed by the Commission as to whether they meet the design Guidelines or not.
Marsha McDaris	July 26 Public Comment Meeting	448 College Ave	Resides in Chautauqua. Also owns properties in Chaut. HD	Thought garages should be limited to 1 2-car garage. 3-car garage not appropriate		
				Should allow cement fiberboard on historic structures		
				Can Guidelines prohibit small cell tower sites in Historic District		
Karen Thurston	July 26 Public Comment Meeting	712 Cruce St	Resides/Owns in Chautauqua HD	Wanted to let us know that narrow porch flooring is no longer available at Forest Lumber		
				Make clear what Guidelies are for landscaping.		
Lee Hall	July 26 Public Comment Meeting	648 S Lahoma	Resides/Owns in Chautauqua HD	Might add for information to the Historic Preservtion Handbook that that the Tree Ordinance provides information on how to have a tree designated as historic and have an restriction place placed upon the deed for it.		
Joyce Green	Comment left via feedback button on webpage		Resides/Owns in Miller HD	I feel very strongly that accessory buildings and parking areas should be secondary to the main structure. The allowances in the guidelines should not be increased. The could allow for a granny flat(should zoning be changed) or reasonable sized garage without negatively affecting neighbors or drainage.		

**HISTORIC DISTRICT COMMISSION  
MINUTES OF  
August 2, 2021**

The Historic District Commission of the City of Norman, Cleveland County, State of Oklahoma, met for the Regular Meeting on August 2, 2021, at 5:30 p.m. Notice and Agenda of the meeting were posted at 201 West Gray Building-A, the Norman Municipal Building and at [www.Normanok.gov](http://www.Normanok.gov) twenty-four hours prior to the beginning of the meeting.

Commissioner Emily Wilkins called the meeting to order at 5:34p.m.

**Item No. 1, being: Roll Call.**

MEMBERS PRESENT:	Mitch Baroff Aaron Brooks Shavonne Evans Tabor Halford Joan Koos Brent Swift Emily Wilkins Barrett Williamson
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MEMBERS ABSENT:	Michael Zorba
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A quorum was present.

STAFF MEMBERS PRESENT:	Anaïs Starr, Planner II Tara Reynolds, Admin Tech III Jeanne Snider, Assistant City Attorney
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GUESTS:

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**Item No. 2, being: Approval of the Minutes from the April 5, 2021 regular meeting.**

Joan Koos would like clarification on the motion for Item #3 that approval for the solar panels on the East side of the structure was in a single row of 3 panels.

**Motion** by Barrett Williamson for approval of the amended minutes from the April 5, 2021 Regular Meeting: **Second** by Joan Koos.

*The motion was passed unanimously with Brent Swift abstaining.*

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**Item No. 3, being: Staff report on active Certificates of Appropriateness and Administrative Bypass issued since April 5, 2021 and consideration of six-month extension requests for expiring COAs.**

- 904 Miller – Staff will pursue violation notice.
- 510 Shawnee - COA was issued 7/1/19 and work has not started - No update given at this meeting.
- 720 W Boyd – Installation almost complete. No update given at this meeting.
- 518 Chautauqua – COA issued 6/1/20, building permit issued August 2020, work has begun.
- 536 Chautauqua – COA approved 8/3/20, and work on garage and driveway is complete.
- 1320 Classen – Interior work continues, windows were replaced.
- 620 Miller – Work has not started.
- 605 Okmulgee – COS issued 4/5/21, and work has not started on siding or windows.
- 428 Chautauqua – COA issued 3/17/21 and work is complete on solar panel installation.

6 month extension requests – None.

**Administrative bypass:**

802 Classen Blvd – Installation of wood storm windows  
808 Classen Blvd – Installation of storage shed less than 108 sq ft.  
421 College – Installation of storage shed less than 108 sq ft.  
509 S Crawford – installation of 6' fence in the rear yard  
712 Cruce – Installation of 4' side and rear yard fence  
432 Chautauqua – Installation of solar panels on rear of house and rear garage

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**Item No. 4, being: Discussion of progress report regarding the FY 2021-2022 CLG Projects.**

**2021-2022 Certified Local Government Fund**

\$150 National Alliance of Preservation Conference (NAPC) Dues  
\$7,000 Commission Assistance and Mentoring Program (C.A.M.P)  
Training for Commissioners  
\$2,500 Planning Conference attendance for staff  
\$600 Education Mailing  
\$10,750 CLG Total allocation for 2021-2022

Anais will send out possible dates of the CAMP and a 2-hour SHPO training to the Commission to choose which works best.

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**Item No. 5, being: Discussion and recommendation to City Council of the revised Historic Preservation Guidelines.**

The public comments gathered from the July 19 and July 26 meetings were discussed, including:

- **Are high impact shingles allowed?** Asphalt based shingles is permissible, but simulated/synthetic/composite materials will go through the commission for a case by case basis for review.
- **Guidelines should allow double pane window replacements for energy efficiency reasons.** Storm windows are permissible.
- **Corner lots should not have two fronts.** There will be no change regarding corner lots having two fronts.
- **Cement fiberboard/Hardieboard should be allowed in all cases.** There will be no change in allowing these materials. It is a case by case review basis.
- **Accessory Structures should not have to match principal structure if brick/stucco – allow wood accessory structures.** The guidelines will allow wood if appropriate and brick/stone masonry accessory structures.
- **Are plastic Tuff Sheds allowed or are they banned now?** They are not banned, as long as they are below 120 square feet.
- **Allow 650 sq ft. as a minimum for parking pad in rear yard.** No changes to this guideline.
- **6' tall fences on side yard should be allowed by Admin Bypass.** No changes.
- **Increase the size allowed for NHRP plaques.** No changes.
- **Allow the removal of secondary chimneys.** This is already allowed.
- **Allow metal shingles.** This was previously discussed.
- **Allow all access ramps on front of the house even if concrete.** No changes.
- **Clarify two fronts in Guidelines.** This will be clarified in the guidelines with illustrations.
- **Metal roofs should be allowed, since asphalt shingles are not historic either.** This was previously discussed.
- **Are garage apts. allowed in Historic Districts?** Some are, it depends on the zoning ordinance for that specific location.
- **Garages should be limited to one two-car garage, at the very least 2-car garage max.** Garages are limited to 575 square feet, or half the footprint of the primary structure, whichever is smaller. There is not a guideline limiting how many car bays a garage can have.
- **Should allow cement fiberboard on historic houses.** No changes.
- **Can guidelines prohibit small cell tower sites?** Legally the guidelines cannot prohibit them. Staff will check for more information about the small cell tower ordinance.
- **Make clear the Guidelines for Landscaping.** There are no guidelines that cover landscaping.
- **Add info to Handbook regarding the new Tree Ordinance which does provide a means to designate a tree historic.** This will be added to the handbook.
- **Feels strongly that accessory structures should be secondary to the main structure. The allowance in the Guidelines should not be increased.** The guidelines are restricting the size of accessory structures, not increasing it.



Commission asked that the Guidelines be revised as follows

- 1) Roofs: Metal roofs allowed upon Commission review. Any type of asphalt shingles are allowed to replace existing composition/asphalt shingles.
- 2) In regards to garage size, on page 13, it should be corrected to say: "The new footprint will be 575 square feet or 50% of the footprint of the principal structure, whichever is **smaller**." Currently it says greater.
- 3) In regards to materials for accessory structures states as: Brick and stone masonry, wood and stucco".

With a recommendation from the Commission the Preservation Guidelines would move forward to the Planning Commission meeting, and then on to City Council for approval, possibly in November. The revisions discussed tonight, and necessary editing will be completed before sending it forward to the Commission.

**Motion** by Brent Swift to recommend the revised guidelines as amended tonight to be forwarded to City Council. **Second** by Barrett Williamson.

The vote went as follows:

Mitch Baroff:	No
Aaron Brooks:	Yes
Shavonne Evans:	Yes
Taber Halford:	Yes
Joan Koos:	Yes
Brent Swift:	Yes
Emily Wilkins:	Yes
Barrett Williamson:	Yes
Michael Zorba:	Absent

**Motion passes 7-1.**

**Item No. 6, being: Miscellaneous comments of the Historic District Commission and city staff.**

Commissioner Barrett Williamson said well-done city staff.

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**Item No. 7, being: Adjournment.**

The meeting adjourned at 7:00p.m.

Passed and approved this \_\_\_\_\_ day of \_\_\_\_\_, 2021.

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Emily Wilkins, Chair  
Historic District Commission

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## Site and Setting

### Site Features and Landscape

#### 2.1 Standards for Administrative Bypass Guidelines for Site Features and Landscape

The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, then the application will be forwarded to the Historic District Commission for a full review.

.1 Garden Structures. Garden structures such as a pergola or freestanding trellis 120 square feet or less, located behind the principal structure with limited or no visibility from the front right-of-way. Wood, metal, wood composite or combination of these materials are acceptable. Vinyl structures are prohibited.

.2 Surface Parking. Parking areas 400 square feet or less, located off the alleyway and not visible from the front right-of-way. Corner lots have two fronts.

.3 Storm Shelters. Above ground storm shelters 120 square feet or less that are not visible from the front right-of-way. Underground storm shelters of any size located in the rear yard and not visible from the front right-of-way. Corner lots have two fronts.

.4 Swimming Pools. Located behind the principal structure in the rear yard and not visible from front right-of-way. Corner lots have two fronts.

#### 2.2 Guidelines for Site Features and Landscape

A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):

.1 Pergolas and Trellis. Garden structures, such as pergolas and trellis, larger than 120 square feet, are to be located behind the principal structure with very limited or no visibility from the front right-of-way. Front or side yard installation can be considered if documentation shows one existed historically. Structures abutting or attached to the principal structure will be reviewed as a building addition. Structures that have a roof and/or sides will be reviewed as accessory structures.

.2 Materials. Structures are to be comprised of wood. Metal, composite wood or cement fiberboard will be considered on a case-by-case basis. Vinyl is prohibited.

.3 Height. Structure shall be no taller than the height of the principal structure.

.1 Swimming Pools. Locate swimming pools in unobtrusive locations.

.4 Swimming Pools. Swimming Pools are to be located behind the principal structure with no visibility from the front right-of-way. Side yard installations will be considered on a case-by-case basis. A front yard installation is prohibited. Corner lots have two fronts.

.5 Storm Shelters. Above ground storm shelters greater than 120 square feet are to be located behind the principal structure with no visibility from the front right-of-way. Side yard installations of below ground storm shelters will be considered on a case-by-case basis. A front

yard installation of above ground or below ground storm shelters are prohibited.

## 2.2 — ~~Archaeology (Advisory Only)~~

## 2.3 — ~~Guidelines for Garages & Accessory Structures~~

### 2.34 ~~Standards for Administrative Bypass for Garage~~

#### .1 ~~Garage Door Replacement.~~

For non-historic garages that face the alleyway or that are not visible from the right-of-way, the following is allowed:

- a. Wood, wood composite or a raised metal panel garage door is an allowed
- b. The original size, height and width of doors must be maintained.
- c. Designs must match the style of the original garage door and/or garage.

### 2.45 ~~Guidelines for Garages~~

A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):

.1 ~~Preserve Accessory Historic Garage Structures.~~ When possible, retain and preserve garages and accessory structures in their original locations and configurations. Even if the function changes, the exterior appearance should remain the same.

.2 ~~Preserve Original Materials.~~ When possible, retain and preserve character-defining materials, features, and details of historic garages and accessory buildings, including foundations, siding, masonry, windows, garage doors, and architectural trim. When necessary, repair character-defining materials, features, and details of historic garages and accessory buildings in-kind according to pertinent guidelines.

.3 ~~Replace Only Deteriorated Portions.~~ If replacement of a deteriorated element or detail of a historic garage or accessory building is necessary, replace only the deteriorated portion in-kind rather than replacing the entire feature. Match the original in design, dimension, texture, and material. Consider a compatible substitute materials only if using the original materials materials are is not technically feasible no longer available.

.4 ~~Request for Garage Demolitions.~~ The HDC will consider the following criteria when a garage structure demolition and/or replacement is proposed:

- ~~Is the existing structure of extraordinary architectural or historical significance?~~
- ~~Is existing structure dilapidated, leaning, lacking a solid foundation, or of substandard construction?~~
- ~~Is existing structure 240 square feet or less?~~
- ~~Was existing structure built after the period of significance?~~
- ~~Will demolition enable access to the rear yard where none currently exists?~~
- ~~Will new structure be limited to one car?~~
- ~~Will new structure have similar street visibility as existing structure?~~
- ~~Will new structure utilize alley access where none currently exists?~~
- ~~Will new footprint be 500 square feet or less?~~



• Will proposed construction preserve existing trees?

A request to demolish a historic garage will utilize the following in determining the eligibility for demolition:

- a. An existing structure of architectural or historical significance should be retained if repairs are reasonably possible.
- b. An existing structure is dilapidated, leaning, lacking a solid foundation, or of substandard construction, it may be eligible for demolition
- c. An existing structure is 240 square feet or less, it may be eligible for demolition.
- d. An existing structure was built after the period of significance; it may be eligible for demolition.
- e. The removal of existing historic structure will enable access to the rear yard where no access currently exists; it may be eligible for demolition.

.5 **Make New Construction Compatible.** If a new garage is the approved alternative, it shall be compatible in form, scale, size, materials, features, and finish with the principal structure. New accessory structures shall maintain the traditional height and proportion of accessory buildings in the district.

.5 **New Garage Construction.** A new garage shall be compatible in form, scale, size, materials, features, and finish with the principal structure.

The following criteria will be considered for a new garage constructed where there is currently no historic structure:

- a. The new structure will utilize alley access if available.
- b. The new footprint will be 575 square feet or 50% of the footprint of the principal structure, whichever is smaller.
- c. The cumulative of square footages for all garage structures on the lot, should be no greater than the footprint of the principal structure. New garage are to be subservient to the principal structure in no case will the garage structure be taller, wider or deeper than the principal structure.
- d. Garages shall be not be any taller, wider, or deeper than the principal structure.
- e. The proposed construction will preserve existing trees.
- f. Maximum of two garages are allowed per site.

.6 **New Garage Height.** New garage structures shall be the traditional height and proportion of garages in the district. New garages in blocks that contain only one-story garages should be one-story. One and a half story and two-story garages may be built if located on a block where one and a half story and two-story garages are dominant or if an adjacent properties contain similar height garages. One and a half story garages may be built if their massing and height are similar to that of the original garage or adjacent one-story garages. The wall height and height of roof ridge are to be no greater than the principal structure.

.7 **New Garage Location.** New garages structures that are not replacing a historic garage are to be located behind the principal structure in the rear yard with limited or no visibility from the front right-of-way. Garages replacing historic garages should maintain the location and configuration of a historic garage, typically at the end of a front driveway. Such garages should be located behind the back elevation of the principal structure.

.8 New Garage Materials. The following may be considered on a case-by-case basis for new garages:

- a. Acceptable materials include wood, brick and stone masonry, and stucco. Fiber cement products for new garage construction located off an alleyway or if setback behind the rear of the house will be considered on a case-by-case basis. It should be noted that wood siding does not have "wood grain." Only smooth cement board is permitted. The use of vinyl, Masonite, aluminum or other metal sidings is prohibited.
- b. Aluminum clad doors and windows are allowed for garages located of an alleyway or behind the rear elevation of the house, with no or limited visibility from the from the front right-of-way.
- c. Wood, wood composite or metal overhead garage doors with wood/wood composite trim are allowed.
- d. Garage doors should be a single width. Double width garage doors will be considered on a case-by-case basis.

.9 Additions to Garage Structures. Additions to existing garages may be appropriate if not visible from the front right-of-way. Addition shall not be greater than the footprint of the existing garage. Must match the materials and design of exiting garage structure.

.10 Reconstruction of Historic Garage. The reconstruction of out buildings should be based on historic evidence, such as photographs, Sanborn maps or other documentation. If no such evidence exists, the design should be derived from the architectural style of the principal building and historic patterns and characteristics of the historic district. Wood, brick and stucco are appropriate materials for reconstruction of a historic garage. Overhead garage doors with the appearance of double doors will be considered on a case-by-case basis. Historic garages should be located at the end of a driveway along the side property line and face the front street right-of way.

.6 Setback Variance. If a new garage violates the City's setback requirements, applicants must apply to the Board of Adjustment for a variance. If a COA is granted, the HDC will provide a letter of recommendation to the Board of Adjustment to accompany the application for variance.

.7 Design Carports Carefully. Carports require a COA. They shall be unattached to the primary structure, located in the rear yard, be constructed of wood or masonry, and have limited visibility from the street.

.118 Replacement Garage Doors. Retain and preserve wood overhead garage doors on historic garages. Retain double doors if possible. Replacement overhead garage doors with the appearance of double doors will be considered on a case-by-case basis. For historic garages, and garages that face the front or are visible from the right-of-way the following replacement door is allowed:

- a. Wood is preferred. However, wood composite or metal with composite trim can be considered on a case-by-case basis. Vinyl is prohibited.
- b. The original size, height and width of doors must be maintained.
- c. Designs must match the style of the original historic garage door.

- .124 Carports. Carports shall be unattached to the primary structure and meet the following:
- Located in the rear yard behind the principal structure, with no visibility from the front right-of-way(s). Corner lots have two fronts
  - Constructed of wood or masonry. Cement fiberboard to be considered on a case-by-case basis
  - Maximum footprint size of 400 square feet with an eave height no greater than 10 feet. In no case shall the carport be taller, wider or deeper than the historic principal structure of the lot.

~~.8 Small Buildings Allowable by Administrative Bypass.~~ Accessory buildings which have a footprint no greater than 108 square feet and are not constructed on or attached to a concrete slab, foundation, or permanent base and have no electric, plumbing, or gas service connection do not require a building permit. However, an Administrative Bypass is required, subject to the conditions set forth in Chapter 1.32. It is recommended that the design of these buildings be compatible with the primary structure and the other surrounding or nearby structures or screened with fencing or landscaping.

### Accessory Structures less than 400 square feet

#### Guidelines for Accessory Structures less than 400 square feet

#### Standards for Administrative Bypass for Accessory Structures less than a 400 square feet

The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, then the application will be forwarded to the Historic District Commission for a full review.

#### 2.5 Administrative Bypass

#### Small Accessory Structures 120 square feet or less.

Must meet the following:

- No greater than 120 square feet footprint. Owner/applicant must meet the building codes requirement for a building permit.
- The design of accessory buildings are compatible with the primary structure and surrounding district.
- Located in the rear yard with no visibility from the front right-of-way.
- Metal and vinyl exterior materials are prohibited.

#### 2.65 Guidelines for Accessory Structures with less than 400 square feet footprint

.1 Preserve Accessory Structures. When possible, retain and preserve accessory structures in their original locations and configurations. Even if the function changes, the exterior appearance should remain the same.

.2 Preserve Original Materials. When possible, retain and preserve character-defining materials, features, and details of historic accessory structures, including foundations, siding, masonry, windows, doors, and architectural trim. When necessary, repair character-defining materials, features, and details of accessory structures in accordance with pertinent guidelines.

.3 Replace Only Deteriorated Portions. If replacement of a deteriorated element or detail of an historic accessory building is necessary, replace only the deteriorated portion in-kind rather than replacing the entire feature. Match the original in design, dimension, texture, and material. Consider compatible substitute materials only if using the original material is not technically feasible.

.4 Request for Accessory Structure Demolitions. A request to demolish a historic accessory structure will utilize the following in determining the eligibility for demolition:

- a. An existing structure of architectural or historical significance should be retained if repairs are reasonably possible.
- b. An existing structure is dilapidated, leaning, lacking a solid foundation, or of substandard construction, it may be eligible for demolition
- c. An existing structure is 240 square feet or less, it may be eligible for demolition.
- d. An existing structure was built after the period of significance; it may be eligible for demolition.
- e. The removal of existing historic structure will enable access to the rear yard where no access currently exists; it may be eligible for demolition.

.5 Make New Construction Compatible. Accessory structures greater than 120 square feet but less than 400 square feet shall be compatible in form, scale, size, materials, features, and finish with the principal structure. New construction must meet the following:

- a. Located in the rear yard, and not visible from front right-of-way.
- b. Compatible in design, style, material to the principal historic structure and the surrounding historic neighborhood.
- c. Select materials and finishes for proposed new accessory buildings that found in historic structures in the district in terms of composition, scale, pattern, detail, texture, and finish. Acceptable materials include brick and stone masonry, stucco and wood. Cement fiberboard will be considered on a case-by-case basis with limited visibility from the front right-of-way. Structures with no visibility from the front may utilize cement fiberboard. No metal or vinyl structures allowed.
- d. New accessory structures shall be one-story in height and less than 10 feet in wall height.

Structures with a footprint of 400 square feet and greater and/or taller than one-story will be reviewed utilizing the either the Guidelines for Secondary Structures or the Guidelines for Garages.

## Secondary Structures

2.76 Guidelines for Secondary Structures. Secondary structures are accessory structures with a footprint of 400 square feet or greater and/or taller than one-story, examples of a secondary structures are garage apartments, studios, workshops and cabanas.

.1 Preserve Secondary Structures. When possible, retain and preserve historic secondary structures in their original locations and configurations. Even if the function changes, the exterior appearance should remain the same.

.2 Preserve Original Materials. When possible, retain and preserve character-defining materials, features, and details of historic secondary structures, including foundations, siding, masonry, windows, doors, and architectural trim. When necessary, repair character-defining materials, features, and details of secondary structures in accordance with pertinent guidelines.

.3 Replace Only Deteriorated Portions. If replacement of a deteriorated element or detail of an historic secondary structure is necessary, replace only the deteriorated portion in-kind rather than replacing the entire feature. Match the original in design, dimension, texture, and material. Consider compatible substitute materials only if using the original material is not technically feasible.

.4 Request for Secondary Structure Demolitions. The following will be utilized to assess a demolition request for a secondary structure:

- a. An existing structure of architectural or historical significance should be retained if repairs are reasonably possible.
- b. An existing structure is dilapidated, leaning, lacking a solid foundation, or of substandard construction, it may be eligible for demolition
- c. An existing structure is 240 square feet or less, it may be eligible for demolition.
- d. An existing structure was built after the period of significance; it may be eligible for demolition.
- e. The removal of existing historic structure will enable access to the rear yard where no access currently exists; it may be eligible for demolition.

.5 Make New Construction Compatible. Secondary accessory structures are to be compatible with the principal structure and surrounding district and in no case should overwhelm the principal structure. Construction of secondary accessory structures will utilize the following criteria for new construction:

- a. Match in design, style, and material to the principal historic structure and the surrounding historic neighborhood.
- b. Compatible with the principal historic structure and/or the district in regards to materials, size, scale, height, form, massing, proportions, spacing and size of window and door openings, window to wall proportions and traditional setbacks seen in the neighborhood.

.6 Size of New Secondary Structures. A new secondary structure should be subservient to the principal structure. It should be no wider, deeper, or taller than principal structure. The size of a secondary structure is limited to 575 square feet or 50% of the principal structure footprint. The cumulative of square footages for all accessory structures and garages on the lot, should be no greater than the footprint of the principal structure. New secondary accessory structures are to be subservient to the principal structure in no case will the secondary structure be taller, wider or deeper than the principal structure.

.7 Location and Setbacks of Secondary Structures. New secondary structures are to maintain traditional locations and setbacks seen in the neighborhood. Locations are to be in the rear yard, with limited or no visibility from front right-of-way, unless there historical indications of a different location. Corner lots have two fronts.

.8 Windows and Doors for Secondary Accessory Structures. Select doors and windows for new secondary accessory buildings that are compatible in material, proportion, pattern, and detail with the doors and windows of historic buildings in the district. See Windows and Door Guidelines.

.9 Materials. Select materials and finishes for proposed new buildings that found in historic buildings in the district in terms of composition, scale, pattern, detail, texture, and finish. **Acceptable materials include brick and stone masonry, stucco and wood.** Cement fiberboard will be considered on a case-by-case basis for those structures located behind the back elevation of the principal structure but with limited visibility from the front right-of-way. Metal and vinyl exterior materials are prohibited.

.10 Avoid False Historical Appearance. New secondary accessory structures are to be compatible with the style, age and character of the principal structure and district without creating a false historical appearance. New structures are to be of their own time and differentiated from the historic structure while maintaining compatibility with the principal structure and the character of the neighborhood.

## **2.4—Sidewalks, Driveways, and Off-Street Parking**

### **2.784.1 Standards for Administrative Bypass for Sidewalks, Driveways, and Off-Street Parking**

The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, then the application will be forwarded to the Historic District Commission for a full review.

.1 Driveways. Widening of an existing driveway or the installation of a new driveway to a maximum width of 10 feet. Driveways are to be constructed from materials allowed by city codes. Approaches can be widen to a maximum of 16 feet.

.2 Concrete Areas. Concrete patios/areas 300 square feet or less and not visible from the front right-of-way (s). Corner lots have two fronts.

.3 Parking pads. Parking pads 400 square feet or less are allowed if located off alley and vehicles parked on the parking pad not visible from the front right-of-way (s). Corner lots have two fronts.

.4 Walkways. Private sidewalks and walkways in the rear yard as long as they meet typical configuration.

### **2.894.2 Guidelines for Sidewalks, Driveways, and Off-Street Parking**

A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):

.1 Front Driveway Location. ~~In historic districts, residential~~ Preserve and retain historic front driveways locations. New or expanded front driveways shall be perpendicular to the



street, except in individual cases where there is historical documentation of an alternate configuration. Unless there is historic documentation otherwise, driveways shall be located ~~near~~ along the property line on one side of the house.

.2 **Driveway Width.** Driveways shall be one car width, not to exceed 10 feet wide, unless there is historic documentation of an alternate configuration. Driveway width may vary as it approaches a garage in order to correspond to the width of the door opening.

.3 **New Driveway Composition.** Driveways shall be constructed from material allowed by the ~~Norman Zoning Ordinance~~ City Code. Existing gravel driveways may remain in place subject to other provisions in the City Code.

.4 **Ribbon Driveways.** Ribbon driveways are permitted to remain or may be newly installed in historic districts. The minimum width of ribbon paving is 18 inches.

.5 **Driveway Approaches.** Maintain the rhythm of existing approaches when introducing new driveways. Driveway approaches may be a maximum of 16 feet wide at the curb, narrowing to 10 feet at the sidewalk or property line.

.6 **Circular Drives.** Drives connecting to the street by two or more curb cut openings are not permitted in front yards or corner side yards unless demonstrated as historically present on the specific property in question.

.7 **Shared Driveways.** Historic driveways shared by two adjacent properties may be retained and preserved.

.8 **Sidewalk Location.** Sidewalks on private property shall be maintained in their traditional location, usually perpendicular to the street, unless there is historical documentation of another location.

.9 **Sidewalks and Curbs.** Public sidewalks and curbs on the street shall be constructed of finished concrete. Sidewalks and curbs on private property may be constructed of finished concrete, brick, or stone.

.10 **New Paved Areas.** New paved areas should not directly abut the principal site structure, significantly alter the site topography, or overwhelm in area the residential, landscaped character of a rear or side yard. Care must be taken that paved areas do not injure nearby trees by intruding onto their root areas. They should be designed to be compatible in location, patterns, spacing, configurations, dimensions, and materials with existing walkways and driveways. Paved areas should not overwhelm the principal structure.

.11 **Rear Yard Area.** New parking areas are permitted off alleyway with no visibility or limited visibility from the front right-of-way(s). Corner lots have two fronts. Rear yard parking must meet Norman City Codes.

.12 **Side Yard Parking Area.** The establishment of parking areas adjacent to the side of historic structures is not allowed.

.13 Front Yard Parking Area. Parking areas in the front yard of the property are prohibited except within an existing driveway.

## 2.5

### Fences and Masonry Walls

#### 2.910 Standards for Administrative Bypass for Fences and Masonry Walls.

The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, then the application will be forwarded to the Historic District Commission for a full review.

- .1 If an existing fence or wall is being replaced with a fence one that is the same in material, height, location, and design, it will be considered ordinary maintenance and repair and will not require a Certificate of Appropriateness.
- .2 Front and side yard fences of up to 4 feet in height and rear yard fences of up to 6 feet in height, may be approved by Administrative Bypass if they meet the following criteria:
  - a. Composed of the following materials: wood, cast iron, iron, twisted wire, painted aluminum that mimics the appearance of cast iron or iron fences or a combination of these materials. Chain link, stone, brick, or stucco walls will be forwarded to the Historic District Commission for review. Vinyl fences are prohibited.
  - b. Of traditional or historic design, contemporary designs/horizontal designs will be forwarded to the Commission for review.
  - c. No footing required. Walls or fences that require a footing shall be forwarded to the Commission for review.

#### 2.10 Guidelines Standards for Fences and Masonry Walls

A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):

- .1 Replacing Conforming Fences. If an existing, conforming type of fence or wall is being replaced with one that is the same in material, height, placement, and style, a Certificate of Appropriateness is not required.
- .2 Preserve Original Materials. Retain and preserve exterior historic wall and fence materials that contribute to the overall historic character of a building. Acceptable materials for new fences and walls are wood, brick, stone, cast iron, iron, twisted wire, painted aluminum that mimics the appearance of cast iron or iron fences, Vinyl is prohibited. 4-foot tall chain link in the side or rear yards will be considered on a case-by-case basis.

.3 Replacing Non-Conforming Fences. Existing fences that are non-conforming as to height, material, style and placement shall not be replaced in kind. Replacement fences shall be

~~conforming as to height, materials, and placement.~~

~~.34 Front Yard Fences. Front yard fences of up to 4 feet in height may be approved by Administrative Bypass. Front yard fences taller than 4 feet are prohibited by the Norman Zoning Ordinance. See diagram #\_\_ for definition.~~

~~.45 Side Yard Fences. Side yard fences of up to 4 feet in height may be approved by Administrative Bypass. Side yard fences taller than 4 feet require a COA. Side yard fences taller than 6 feet are prohibited. See diagram #\_\_ for definition.~~

~~.56 Rear Yard Fences. Rear yard fences of up to 6 feet in height may be approved by Administrative Bypass. Rear yard fences taller than 6 feet require a COA. Rear yard fences of a contemporary design or of non-traditional materials or of height greater than 8' will be considered on a case-by-case basis. Such fences will be review for their impact to the historic structure and the District as a whole. Rear yard fences taller than 8 feet are prohibited by the Norman Zoning Ordinance. The Norman Zoning Ordinance prohibits rear yard fences taller than 8 feet. See diagram #\_\_ for definition.~~

~~.67 Fences on Corner Properties Adjacent to Alleys. Fences on corner properties with alley access shall be located very carefully to maximize sight lines and minimize conflicts between alley traffic, pedestrians, and on-street traffic.~~

~~.78 Fence and Wall Materials. Fences or walls shall be constructed of wood, brick, stone, iron or cast or forged metal, stucco, or a combination of these materials, which are consistent with period styles in Norman's historic districts. Stone materials. Stone or brick used in walls shall be compatible in size, scale, and style to that used elsewhere in the historic district, or typical of residential structures of this type, age, and location. No vinyl, cinder block, concrete block, or corrugated metal, may be used for fences or walls in historic districts. Chain link in the rear yard will be considered on a case-by-case basis.~~

~~.89 Colors and Finishes. Although paint color is not regulated by the Commission, it is strongly recommended that wood fences be stained or painted in colors and finishes appropriate to the style and period of the property and the district or left unfinished. No decorative murals shall be applied to fence or wall surfaces visible from the street.~~

~~.910 Finished Side Out. Fences or walls facing the street shall be constructed with the finished side out.~~

~~.1014 Setback and Adjacent Property Tie-In. A fence 4 feet or less in height shall be set back a minimum of 1 foot from the inner edge of a public sidewalk. A fence over 4 feet in height shall be set back a minimum of 2 feet. Where no sidewalk exists, fences shall be set back a minimum of 6 feet from the back of curb or edge of pavement. If a fence exists on an adjacent property, the corner side yard fence should tie into the existing fence. In no case shall a fence extend beyond the property line.~~

~~Note: This section shall be accompanied by Fence Palette detailing approvable fence styles and configurations.~~

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## Signage

### 2.6 — Guidelines for Signage

#### 2.121 6.1 Standards for Administrative Bypass for Signage

The following item can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process:

.1 National Register of Historic Places Plaques. A National Register of Historic Place commemorative plaques, if less than 2 square feet, bronze, mounted so that will not permanently damage the exterior façade material or impact the architectural features of the structure of the historic structure.

### 2.13 12-6.2 Guidelines for Signage

A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):

- .1 Sign Ordinance Also Applies. In addition to a review by the Historic District Commission, signs will be subject to the regulations and permitting requirements established in Chapter 18 of the Code of Norman, Oklahoma, also referred to as the Sign Ordinance. Applicants shall coordinate the design and placement of any sign in a historic district with the Sign Ordinance as well as these guidelines.
- .2 Signs Must Be Compatible. Size, design, and placement of a sign shall relate to the architectural elements of the structure. Signs shall be compatible with other signs and other structures along the street.
- .3 Non-Contributing Resources. Signs associated with non-contributing structures will be controlled only to the degree necessary to make them compatible with the general atmosphere of the district.

## 2.7 Non-Contributing Resources

### 2.7.1 Standards for Administrative Bypass for Non-Contributing Resources

There are no items eligible for Administrative Bypass.

### 2.14 37.2 Guidelines for Non-Contributing Resources

A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):

- .1 Preservation Guidelines Apply. The Historic Preservation Guidelines apply to all structures in Norman's Historic Districts, both contributing and non-contributing.
- .2 Support Harmony Between Old and New. Non-contributing structures shall be controlled only to the degree necessary to make them compatible with the general atmosphere of the district with regard to alterations, additions, changes to the site, and the like. As with all requests for Certificates of Appropriateness in historic districts, each project will be evaluated

on its own merits for overall impact on the district as a whole.

## Building Exteriors

### ~~3.1 Guidelines for Exterior Walls~~

#### ~~3.1.1 Standards for Administrative Bypass for Exterior Walls~~

The following item can receive a Certificate of Appropriateness (COA) through the Administrative Bypass:

.1 Removal of wall materials. Removal of non-original or contemporary synthetic materials siding to reveal existing historic siding and trim materials is permitted. If existing historic siding material underneath the non-original or contemporary synthetic materials has been removed, the reinstallation of appropriate/compatible material requires review by the Historic District Commission.

#### ~~3.2.2 Guidelines for Exterior Walls~~

A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):

- .1 Preserve Original Walls. Retain and preserve exterior walls that contribute to the overall historic form and character of a building, including functional and decorative features and details.
- .2 Retain Original Building Materials. Retain and preserve exterior wall materials that contribute to the overall historic character of a building.
- .3 Replace Only Deteriorated Portions. If replacement of a deteriorated wall or feature is necessary, replace only the deteriorated portion ~~in-kind~~ in-kind rather than the entire feature. Match the original in material, design, dimension, detail, texture, and pattern. Consider compatible substitute materials only if using the original material is ~~not technically feasible~~ no longer available.
- .4 Avoid Covering Original Materials. Building materials and decorative elements are important character-defining components of historic buildings. It is not appropriate to remove or cover any wall material or detail with coatings or contemporary substitute materials. Vinyl and aluminum siding is not appropriate for use in historic districts.
- .5 Replace Missing Features. When replacing an exterior wall or feature, replace it with a new wall or feature based on accurate documentation of the original or a new design that is compatible with the historic character of the building and the district. Consider compatible substitute materials only if using the original material is ~~not technically feasible~~ no longer available.
- .6 Avoid False Historical Appearances. Features or details of walls and fences that are introduced to a ~~house~~ property should reflect its style, period, and design. Fences and walls



Features should not create a false historical appearance by reflecting other time periods, styles, or geographic regions of the country.

**.7 Substitute Materials.** Cement fiberboard (e.g. Hardiplank siding) will be considered on a case-by-case basis. Exterior insulating and finish systems (EIFS) will not be considered for use in historic structures.

## Wood Features

### 3.2.1 Standards for Administrative Bypass for Wood Features

See pertinent sections for items eligible for Administrative Bypass.

### 3.32.2 Guidelines for Wood Features

A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):

- .1 Preserve Original Features.** Retain and preserve wood features that contribute to the overall historic character of a building, including siding, shingles, cornices, brackets, pediments, columns, balustrades, and architectural trim.
- .2 Replace Only Deteriorated Elements.** If replacement of a deteriorated details or element of a wood feature is necessary, replace only the deteriorated detail or element in kind rather than the entire feature. Match the original in design, dimension, texture, and material. Consider compatible substitute materials only if using the original material is not technically feasible no longer available.
- .3 Replace Missing Features.** Replace missing wooden features based on accurate documentation of the missing original or a new design compatible in scale, size, material, and texture, with the style, period, and design of the historic building and the district as a whole. Consider compatible substitute materials only if using the original material is not technically feasible no longer available.
- .4 Avoid False Historical Appearances.** Features or details that are introduced to a house should reflect its style, period, and design. Features should not create a false historical appearance by reflecting other time periods, styles, or geographic regions of the country.
- .5 Rough Sawn Wood.** Avoid using rough sawn wood as is not appropriate for installation in historic buildings.
- .6 Skirts.** All solid skirt materials should have vents installed to allow air to pass under the house and eliminate moisture from the wood foundation.
- .7 Treated Wood.** All treated wood should be thoroughly dried prior to installation.
- .8 Cleaning.** Do not use excessive water pressure or sandblasting on wood surfaces as it pits the wood.

.9 Defining Features. Retain corner boards and window trim as they are character-defining features on houses with wood siding or replaced with historic accuracy.

### **3.3—Guidelines for Masonry and Brick Features**

#### **3.3.1 Standards for Administrative Bypass for Masonry and Brick Features**

The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, then the application will be forwarded to the Historic District Commission for a full review.

.1 Chimneys. Primary chimneys are a character-defining masonry feature of historic structures and should be preserved. A non-functional, secondary chimney visible only at the roof and located on the back half of the structure, may be approved through the Administrative Bypass.

#### **3.43.2 Guidelines for Administrative Bypass for Masonry Features**

A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):

.1 Preserve Original Features. Retain and preserve masonry features that contribute to the overall historic character of a building, including foundations, chimneys, cornices, steps, piers, columns, lintels, arches, and sills. Installing brick or block where these materials were not originally used is prohibited. Installing brick on the walls of a house that originally had wood siding is prohibited as it changes the character of the house and can destroy the wood beneath.

.2 Preserve Original Materials. Retain and preserve historic masonry materials, such as brick, terra-cotta, limestone, granite, stucco, slate, concrete, cement block, and clay tile, and their distinctive construction features.

.3 Replace Only Deteriorated Elements. If replacement of a deteriorated detail or elements of masonry feature is necessary, replace only the deteriorated in-kind rather than replacing the entire feature. Consider compatible substitute materials only if using the original material is not technically feasible no longer available.

.4 Replace Surfaces Only As Necessary. Replace large masonry surfaces in-kind in-kind only as necessary, matching the original in design, detail, dimension, color, pattern, texture, and material. Consider substitute materials only if using the original material is not technically feasible no longer available.

.5 Replace Missing Features. Replace missing masonry and brick features based on accurate documentation of the missing original or a new design compatible in size, scale, material, and texture with the style, period, and design of the historic building and the district as a whole. Consider compatible substitute materials only if using the original material is not technically feasible no longer available.

.6 Preserve Unpainted Surfaces. It is not appropriate to paint unpainted masonry and

brick surfaces that were not painted historically. Repaint previously painted masonry surfaces in colors appropriate to the historic building material, the building, and the district.

~~.7 **Chimneys.** Chimneys are often a character-defining masonry feature of historic structures. A non-functional, secondary chimney visible only at the roof may be considered for removal on a case-by-case basis per Administrative Bypass.~~

.7 **Chimneys.** Retain and preserve primary chimneys. If a chimney, often used as a flue rather than fireplace, is to be removed from the interior of the house, retain the portion above the roofline. A platform will need to be constructed in the attic to carry the weight of the chimney. If a secondary non-functional chimney that is visible from the front right-of-way will be reviewed for removal on a case-by-case basis.

.8 **Demolition of Chimneys.** Chimneys are a character-defining feature and should be retained and maintained. If the foundation of the chimney has failed or the chimney is badly deteriorated, the chimney can be carefully dismantled and reconstructed using original materials or materials matching the original. Mortar should match the original in composition and joint profile.

.9 **Materials.** Replace loose or missing mortar with one of the same composition as the original. Mortar is important to the integrity of the brick wall. If the mortar is missing, its replacement should match the historic mortar in composition, color, and joint width. Use a sand-lime recipe for mortar, which is compatible with the old brick. Modern masonry mortar has cement as a main ingredient, which is too hard for historic brick. A high Portland cement content will trap moisture in the brick and cause it to deteriorate.

.10 **Flashing.** Repair or replace flashing as needed to ensure a watertight connection between the chimney and roof.

.11 **Cleaning.** Historic buildings should be cleaned in the gentlest means possible which typically includes water and soft bristle brushes. Sandblasting and high-pressure washing can cause irreparable damage to brick and are not permissible. Any chemical cleaner must be tested in small areas of limited visibility to ensure compatibility and effectiveness on the brick.

## **Stone**

### **3.53 Guidelines for Stone**

A full review by the Historic District Commission will take the following criteria into consideration to be issued a Certificate of Appropriateness (COA):

.1 **Replacing Deteriorated Elements.** Replace deteriorated stone with stone that matches the original in color and texture.

.2 **Mortar.** Replace deteriorated or missing mortar with mortar of the same composition as the original in composition and color.

.3 **Portland Cement.** Do not use Portland Cement on historic stone structures. Portland cement, or masons mortar, is too hard and will cause the stone to deteriorate and crumble.

.4 Foundation. The addition of stone to the foundation or exterior of a house is prohibited.

.5 Walls. Retain and preserve historic stonewalls.

.6 Chemicals. Any chemical cleaner must be tested in small areas of limited visibility to ensure compatibility and effectiveness on the stone. Some chemicals may burn the face of stone.

### **5.3—Historic Block and CMU (Concrete Masonry Unit)**

#### **5.3.1—Standards for Administrative Bypass for CMU**

There are no Administrative Bypass eligible items.

#### **3.65.3.2 Guidelines for CMU**

A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):

.1 Retain Original Materials. Retain historic concrete block as a building material and maintain it.

.2 Mortar. Replace deteriorated or missing mortar with mortar of the same composition and joint profile.

.3 Paint. Painted concrete block should remain painted.

.4 Landscape. Retain and maintain historic concrete block. This may include repairing or reconstructing foundations.

.5 Contemporary Concrete Masonry Units. Contemporary CMU is not appropriate for use on a historic structure.

### **5.4—Synthetic Materials / Stucco**

#### **5.4.1—Standards for Administrative Bypass for Synthetic Materials/Stucco**

#### **3.75.4. Guidelines**

A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):

.1 Retain Original Materials. Retain and repair the original building material. Installing any synthetic building material or stucco on top of existing wood is prohibited. Many of these materials can trap moisture in the wall, which will cause the wood beneath to deteriorate. It can also trap moisture in the insulation, which reduces the value of the insulation.

.2 Replace Deteriorated Materials. Replace only that material which is beyond repair with visually compatible new material. Match the original in profile as closely as possible.

.3 Retain Character Defining Features. Installing synthetic siding on top of an existing siding as a way of “modernizing” the house or attempting to make the house more energy efficient is prohibited. This changes the character of the original design and frequently destroys the character-defining features of the house and neighborhood.

.4 Stucco. Stucco is a material that may develop hairline cracks over time. It should be gently washed with low pressure and allowed to dry thoroughly. The application of an elastomeric paint will cover most hairline cracks and provide some flexibility at those locations.

.5 Details. Retain details as corner boards, windows and door surrounds, gable vents and rafter ends.

.6 Cement Fiberboard. Cement fiberboard (Hardieplank) and synthetic wood materials are prohibited except for new construction. These are not comparable substitutes for wood siding except in certain applications. A good use of cement board siding is where it is in contact with the ground, such as the skirt of a pier-and-beam house. Be sure to retain ventilation of the crawl space. If using cement board, use smooth only. Wood used in historic houses was sanded smooth with no obvious grain.

## 5.5—Metal

### 3.85.5.1 Guidelines for Metal

A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):

.1 Replacing Deteriorated Material. Replace deteriorated metal with new primed metal of the same or compatible material. Metal materials should not be used to replace wood or other historic non-metal materials.

.2 Aluminum. Aluminum should not replace wood as a building material but is used for cornices and other details on many buildings. This is especially true of doors and windows and their frames. If aluminum appears to be the only option as a replacement material for deteriorated wood, the aluminum should be of similar profile and should have a factory painted finish. Mill finish or “shiny” aluminum should not be used on a historic building to replace a previously painted material.

.3 Paint. It is important to keep pressed metal, cast iron and steel well painted to avoid rust and deterioration.

.4 Decorative Details. Retain metal decorative roof details when replacing the primary roofing material.

.5 Decorative Iron. Do not create a false history by installing decorative iron work over windows that did not include them in the original design.

.6 Pressed Metal. Do not create a false history by installing a pressed metal skirt where one did not previously exist

### ~~3.4 Guidelines for Roofs~~

#### ~~3.94.1 Standards for Administrative Bypass for Roofs~~

The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, then the application will be forwarded to the Historic District Commission for a full review.

.1 **Re-Roofing.** Reroofing with in-kind materials with no change to the shape, pitch, or structure of the roof. Replacement in-kind of existing, non-historic composition roofing material with any type of contemporary asphalt, laminated or composition shingles is not subject to review and does not require a Certificate of Appropriateness.

.2 **Gutters.** Replacement or and installation of non-historic gutters and downspouts in-kind is not subject to review and does not require a Certificate of Appropriateness.

.3 **New Features.** New roof features such as skylights, solar tubes, and equipment such as power ventilators, solar collectors, photovoltaics, and antennae that are:

- a. **Located on rear of the structure, and not visible from the front right of way right-of-way.** Corner lots have two fronts.

#### ~~3.104.2 Guidelines for Roofs~~

A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):

.1 **Preserve Original Features.** Retain and preserve historic wood, tile and slate roofs as well as ~~and~~ roof features that contribute to the overall historic character of a building, such as cresting, dormers, cupolas, and cornices. ~~Tile and slate roofs rarely need to be discarded.~~

.2 **Replace Only Deteriorated Portions of Roof Features.** If replacement of a deteriorated roof feature is necessary, replace only the deteriorated portion ~~in-kind in-kind~~ to match the original feature in design, dimension, detail, and material. Consider compatible substitute materials only if using the original material is ~~not technically feasible~~ no longer available.

.3 **Replacements Match Original.** If full replacement of historic roofing material or feature is necessary, replace it ~~in-kind in-kind~~, matching the original in scale, detail, pattern, design, and material. Consider compatible substitute materials only if using the original material is ~~not technically feasible~~ no longer available.

.4 **Replace Missing Features.** Replace ~~it~~ missing roof features based on accurate documentation of the missing original or a new design compatible in scale, size, and material with the style, period, and design of the historic building and the district as a whole.

.5 **~~Avoid Replacing Built-In Gutters.~~** ~~Retain and preserve~~ It is not appropriate to replace concealed, built-in gutter systems with exposed gutters.

.6 **Locate New Features and Mechanical Equipment Carefully.** ~~Adding new features or equipment on a roof requires a COA.~~ New roof features such as dormers, skylights, and solar tubes, and equipment such as power ventilators, solar collectors, photovoltaics, and antennae,



shall be introduced carefully so as not to compromise the historic roof design, or damage character-defining roof materials, or the overall character of the historic district.

.7 Retain the Original Roof Form and Details. If attic space is converted into living space and dormers are added, retain the original roof pitch to avoid a “pop-up” appearance, especially on the front façade. Avoid adding details that did not exist originally.

.8 Existing Dormers. Original dormers should be preserved and only elements beyond repair may be replaced. If a replacement is needed, original size and shape should be maintained.

.9 New Dormers. New dormers must be functional, to allow light in or to add more living space, they should not be merely decorative and should be in keeping with the style of the historic house. They should be located on the rear and inset from first-floor side wall below it. Set new dormers back from eave and do not extend above the ridge of roof.

.10 Alternative Materials for Roofs. Metal simulated clay, slate or other designs as well as other materials will be reviewed on a case-by-case basis to see if appropriate to the historic structure and compatible with the surrounding historic district.

### **3.5 Guidelines for Windows and Doors**

#### **3.1154 Standards for Administrative Bypass for Windows:**

The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed below. If they do not meet the criteria, then the application will be forwarded to the Historic District Commission for a full review.

.1 Window Replacement by Administrative Bypass. An historic window that is deteriorated more than 50% and is not repairable may be replaced in-kind if it meets the following:

- a. Replace original windows in-kind, meaning match the original in material and finish.
- b. Muntin width and profile are same as the original in width and profile.
- c. Light pattern is the same as the original.
- d. True divided lights (panes) are the same as the original glass thickness.
- e. Size and dimension of all window components are the same as the original.
- f. Replacement of less than 50% of the windows on a given elevation.

.2 Storm Windows and Screens. The use of interior storm windows is encouraged. Installation of storm windows if they meet the following criteria:

- a. Wood framed, full-light storms and screens that are low profile and align with meeting rails of the window.
- b. Relatively unobtrusive, narrow-profile, metal exterior storm windows that do not obscure the window itself, that are carefully installed to prevent damage to the sill or the frame, and that are finished in a painted or a baked-enamel color compatible with the sash color are allowed. Storm window rails to align with meeting rails of the window.

c. The use of ¼ inch thick clear laminated glass for the purposes of weatherization and noise reduction maybe used in storm windows.

.3 Awnings. Window awnings that conform to following criteria:

- a. Material is fabric
- b. Of traditional style and shape
- c. Located on the rear of the structure.
- d. Installed over windows, doors, storefronts, or porch openings with care to ensure that historic features are not damaged or obscured.

3.125.2 Guidelines for Windows

A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):

.1 ~~Preserve~~ Retain Original Windows. Retain and preserve original windows, including glass, frames, sash, muntins, sills, heads, moldings, surrounds, and hardware.

.2 Retain Historic Glass. Retain original glass in historic windows if at all possible. Leaded glass windows shall be preserved. Bubbles and waves give old glass its distinctive look and add to the historic character of the house.

.3 Glass. Retain original glass in historic windows. Bubbles and waves give old glass its distinctive look and add to the historic character of the house.

.4 Glass Variations.

- a. Privacy glass may only be located in the rear or on the side of the structure, where not visible from the front. Smoked or tinted glass is not appropriate for use in historic structures.
- b. Beveled glass in doors and windows is allowed as long as it is compatible with style of the historic building and the original configuration of window panes remains.
- c. Colored Glass. Colored glass may be used in transoms and sidelights if supported by historical documentation or compatible with the architectural style.

.3 ~~Preserve Original Doors.~~ Retain and preserve original doors and door surrounds including frames, glazing, panels, sidelights, fanlights, surrounds, thresholds, and hardware.

.54 Replace Only Deteriorated Features. If replacement of a deteriorated window or door feature or details is necessary, replace only the deteriorated feature in-kind rather than the entire unit. Broken sash cords, for example, can be repaired and do not necessitate replacing an entire window. Match the original in design, dimension, placement, and material.

.5 ~~Replacement Doors.~~ Replacement doors and door surrounds shall be appropriate to the style of the structure. Doors shall be relocated, enlarged, or introduced only when the alteration is appropriate to the style of the building.

.6 ~~Storm/Screen Doors.~~ Wood framed screen doors and full light storm doors do not

require a COA or Administrative Bypass.

~~.7 Window Replacement by Administrative Bypass.~~ A deteriorated window may be replaced "like with like," based on the following criteria:

- ~~• Typically all wood construction~~
- ~~• Muntin width and profile are very similar to the original in width and profile~~
- ~~• Light pattern is the same as the original~~
- ~~• True divided lights (panes) are the same as the original~~
- ~~• Size and dimension of all window components are the same as the original.~~

.68 Sash Replacement. Replacement sash, often referred to as sash re- placement kits, are acceptable for use in historic structures. However, re- placement window sash shall be unclad wood, with single-pane thickness, true divided light patterns that match the historic muntin pattern and profile of the house.

.798 Window Replacement by COA. A deteriorated window replacement, other than "like with like," as defined above requires a COA and shall conform to the following: An original window that is deteriorated more than 50% and is not repairable may be placed in-kind if it meets the following:

- Shall have a wood exterior, unless replacing a metal casement window
- ~~• Aluminum or vinyl cladding is not appropriate~~
- Light patterns same as the original
- Size and dimension the same as the original
- Double-pane simulated divided lights with wood muntins on the exterior and interior and a shadow bar between the panes may be allowed for windows on the side or rear that are not visible from the street.

.8109 Retain Original Metal Windows. Replace original metal casement windows only as a last resort after weatherization measures have proven unsuccessful.

.9110 Preserve Original Openings. Do not create new openings in the front or side facades of historic structures. Do not enlarge or diminish existing openings to fit stock window and door sizes. If new openings are necessary to meet code requirements, they shall be compatible with historic windows for that structure in proportion, shape, location, pattern, size, materials, and details.

~~.11 Locate Privacy Glass in Rear.~~ Privacy glass may be installed where required in divided light windows (such as in a bathroom) but only located in the rear 50% of the structure. ~~Smoked or tinted glass is not appropriate for use in historic structures.~~

~~.12 Use Wood Windows in Primary Structures and Additions.~~ For construction of new primary structures, choose windows that complement window types in surrounding structures in material, placement, size, shape, and design. While single-pane, true divided light, wood frame windows are the most desirable choice for new construction in historic districts, double-pane glass wood windows with interior and exterior applied muntins and shadow bars between

the panes are permitted. Aluminum cladding of wooden windows is permissible for use in construction of new primary structures and additions. Vinyl cladding of wood windows is not appropriate.

.104 Materials. Wood is allowable for in-kind replacement of windows. Aluminum-clad and metal windows can be considered for the replacement of metal casement windows that are deteriorated on a case-by-case basis. Fiberglass and aluminum-clad windows can be considered on non-contributing resources and on rear elevations not visible from the front right-of-way. Vinyl-clad windows are prohibited for both contributing and non-contributing structures in the historic districts.

.115 New Primary & Accessory Structures. Construction. Windows in new construction are to be compatible with in adjacent historic structures in terms of size, profile, design, proportions, and material. Wood and aluminum clad windows are acceptable for use in new construction.

.126 Additions. For construction of additions, choose windows that match the original structure. While single-pane, true divided light, wood frame windows are the most desirable choice for new construction in historic districts, double-pane glass wood windows with interior and exterior applied muntins and shadow bars between the panes are permitted. Aluminum cladding of wooden windows is permissible for use in additions. Vinyl or vinyl-clad windows are prohibited.

.1373 Install Awnings Carefully. Fabric window awnings that conform to material, style, shape, and location may be approved by Administrative Bypass. Install fabric awnings over window, doors, storefronts, or porch openings with care to ensure that historic features are not damaged or obscured. Awnings composed of wood or metal are not permitted unless there is historic documentation of their use.

### 3.6—Doors

#### ~~3.136.1~~ Standards for Administrative Bypass for Doors:

The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed below. If they do not meet the criteria, then the application will be forwarded to the Historic District Commission for a full review.

**.1 Door Replacement.** A deteriorated door that is not repairable may be replaced in-kind, meaning a door that matches the original in materials and design. A non-original steel door may be replaced with a wood door that is appropriate design for the house and the historic district.

**.2 Screen Door Replacement.** Screen doors should be retained and re-paired when necessary. Any replacement screen door should match the historic screen door and should be built to mirror the panels and sash divisions of the door that it covers.

**.3 Storm Doors and Screens.** Storm doors ~~are to be~~ constructed of wood or metal that do not obscure or damage the existing door and frame. Storm doors required to ~~be painted~~ be painted, stained, or have a baked-enamel finish color compatible with the color of the existing door. If storm and screen doors are installed where none existed originally, select a “full vision panel” design to allow the original door to be seen. (Additional information on storm windows and doors is provided in Section 3.174-9, Utilities and Energy Retrofit).

### **3.146.2 Guidelines for Doors**

A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):

**.1 Retain and Preserve Original Doors.** Retain and preserve original doors and door surrounds including frames, glazing, panels, sidelights, fanlights, sur-rounds, thresholds, and hardware on front doors and side doors visible from the street.

**.2 Replace Only Deteriorated Features.** If replacement of a deteriorated door feature or details is necessary, replace only the deteriorated feature in-kind rather than the entire unit.

**.3 Retain and Preserve Transoms and Sidelights.** Transoms and sidelights should be retained and preserved. Avoid altering transoms and sidelights as it distorts the strong vertical proportions of the windows and doors and changes the character of the residence.

**.4 Retain Historic Glass.** Retain original glass in historic doors. Bubbles and waves give old glass its distinctive look and add to the historic character of the house.

#### **.5 Glass Variations**

**\*a.** Privacy glass may only be located in the rear or on the side of the structure, where not visible from the front. Smoked or tinted glass is not appropriate for use in historic structures.

**\*b.** Beveled glass in doors is allowed as long as it is compatible with style of the historic building and the original configuration of window panes remains.

**\*c.** Colored Glass. Colored glass may be used in transoms and sidelights if supported by historical documentation or compatible with the architectural style.

**.64 Wood Doors.** Wood doors are required unless there is documentation that other materials were historically used on a particular structure. Keep wood doors appropriately stained or painted to protect from weather.

**.75 Replacement Doors.** Replacement doors on a historic structure are to be wood and in appropriate design, size and details in keeping with the style of the house. Installation of steel doors on the front of a historic structure is prohibited. Aluminum clad doors are permissible on rear of the structure on a limited case by case case by case basis.

**.87 Preserve Original Openings.** Do not create new openings in the front or side facades of historic structures. Do not enlarge or diminish existing openings to fit stock door sizes. If new openings are necessary to meet code requirements, they shall be compatible with historic windows for that structure in proportion, shape, location, pattern, size, materials, and details.

**.98 Materials.** Wood is allowable for in-kind replacement of doors. Fiberglass and aluminum-clad doors can be considered on non-contributing resources and on rear elevations of historic structures when not visible from the front right-of-way. Vinyl is prohibited for historic and non-contributing structures.

**.109 New Primary & Secondary Accessory Structures.** ~~Construction.~~ Doors in new construction should be similar to ~~windows~~ those in adjacent historic structures in terms of size, profile, design, proportions, and material. Aluminum clad and fiberglass doors with limited or no visibility from the front façade can be considered on a case-by-case basis.

**.110 Additions.** For construction of additions, choose doors that match the original structure. Aluminum-clad wood doors are permissible for use in additions that are not visible from the front right-of-way. Fiberglass doors can be considered on a case-by-case basis.

### ~~3.76~~ Guidelines for Entrances, Porches, and Balconies

#### ~~3.157.1~~ Administrative Bypass Standards for Entrances, Porches and Balconies

The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, the application will be forwarded to the Historic District Commission for review.

**.1 Screening of a rear porch.** Screening of a rear porch that is temporary, easily reversible, and is designed to preserve the historic character of the porch and the building. Screening must be with compatible materials.

**.2 Balconies and Porches.** Balconies and porches that are less than 120 square feet, built on the rear and not visible from the front right-of-way and built with compatible with the structure in material, scale, and size.

**.3 Handrails.** Installation of handrails required by building code may be approvable by Administrative Bypass. Handrails must meet adopted City building codes and be of a simple design that is compatible with the house in material and scale. Wood or metal are acceptable materials for handrails on historic structures.

**.4 Concrete Steps and Porch floorings.** Replacement of existing concrete steps porch flooring in-kind, with the same materials and design. Steps are to match the original steps in size, form and detail. The number of steps should be retained if possible, unless building codes require a different configuration.

#### ~~3.167.2~~ Guidelines for Entrances, Porches, and Balconies

The Historic District Commission will use following criteria for review of a Certificate of Appropriateness (COA):



- .1 Preserve Original Entrances, Porches, and Balconies.** Retain and preserve entrances, porches, and balconies that contribute to the overall historic character of a building, including columns, pilasters, piers, entablatures, balustrades, sidelights, fanlights, transoms, steps, railings, floors, and ceilings.
- .2 Replace Only Deteriorated Elements.** If replacement of a deteriorated detail or element of an entrance, porch, or balcony feature is necessary, replace only the deteriorated detail or element ~~in-kind~~ rather than the entire feature. Match the original in design, dimension, and material. ~~Consider~~ Compatible substitute materials can be considered only if using the original material is not available, technically feasible.
- .3 Replacements Match Original.** If full replacement of an entrance, porch, or balcony is necessary, replace it ~~in-kind~~, matching the original in design, dimension, detail, texture, and material. Compatible substitute materials can be considered only if original material is no longer available. ~~Consider compatible substitute materials only if using the original material is not technically feasible.~~
- .4 Replace Missing Features.** Replace missing entrance, porch, or balcony features with a new feature based on accurate documentation of the missing original or a new design compatible with the historic character of the building and the district.
- .5 Screen Porches Carefully.** Consider the screening of a historic porch only if the alteration is reversible and can be designed to preserve the historic character of the porch and the building.
- .6 Avoid Enclosures.** It is not appropriate to enclose a front porch or a front balcony.
- .7 Avoid Removing Details.** It is not appropriate to remove any detail material associated with entrances and porches, such as graining, beveled glass, or beaded board, unless an accurate restoration requires it.
- .8 Avoid Changes to Primary Facades.** It is not appropriate to remove an original entrance or porch or to add a new entrance or porch on a primary facade.
- .9 Avoid False Historical Appearances.** Features or details that are introduced to a house should reflect its style, period, and design. Features should not create a false historical appearance by reflecting other time periods, styles, or geographic regions of the country.
- .10 Maintain Porch Elevation.** At no time should the porch elevation be lowered to grade and steps redesigned.
- .11 Maintain Wood Elements.** Wood porch floors and columns may require an eventual replacement due to moisture penetration; wood floors and columns should only be replaced with wood of the same profile and dimension.
- 12. New Balconies and Porches.** Balconies and porches built on the rear and not visible

from the front right-of-way are to be constructed to be compatible with the principal structure in material, scale, and size. New balconies or porches on the front or side will only be considered if there is historic evidence that one existed. The design and materials is to be based on historic evidence.

13. **Respect Design.** Original design, construction, and materials should be respected on primary façades. Installation of non-original materials, such as decorative tile, is not appropriate.

### 3.7 — Recommendations for Color (Advisory Only)

### 3.8 — Mechanical, Electrical, and Communication Equipment (Advisory Only)

### 3.109 Utilities and Energy Retrofit (Advisory Only)

#### 3.1740.1 Standards for Administrative Bypass for Utilities and Energy Retrofit

The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, then the application will be forwarded to the Historic District Commission for a full review.

.1 **Storm Windows & Doors.** Interior storm windows are encouraged and do not require a COA. Exterior storm windows are allowable with a COA by administrative bypass if they meet the following criteria:

- a. Metal storm windows and windows with painted, stained, or baked-enamel finish color compatible with the color of the existing window or door. Unfinished or clear anodized aluminum finishes are not permitted.
- b. Storm windows and doors that do not obscure or damage the existing window/door and/or frame.

.2 **Solar Panels.** Solar panels installed on the “back” side of the house, or on the roof where they are not visible from the front right-of-way or public view.

.3 **Free-standing Solar Racks.** Solar racks can be installed at the rear of the property to create a shade structure or can be installed on an outbuilding, such as a garage roof, as long as they meet the following:

- a. Located in the rear yard and not visible from the front right-of-way. Not taller than the principal structure. Less than 120 square feet.

.4 **Solar Tubes and Skylights.** If flat in profile and on the rear or back side of the house, and not visible from the front right-of-way.

#### 3.1810.2 Guidelines for Utilities and Energy Retrofit

A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):

1. **Retain Inherent Energy-Conserving Features.** Retain and preserve the inherent

energy-conserving features of historic buildings and their sites, including shade trees, porches, awnings, as well as operable windows, transoms, shutters, and blinds.

2. Use Traditional Energy-Saving Practices. Increase the thermal efficiency of historic buildings by observing appropriate traditional practices, such as weather stripping and caulking, and by introducing energy-efficient features such as awnings, operable shutters, and storm windows and doors, where appropriate.

3. Solar Tubes and Skylights. Solar Tubes and Skylights can add light to interior spaces and make attics spaces more useable. Bubble-dome skylights are not appropriate for buildings within historic districts.

4. Solar Panels. Avoid installing solar panels on the street side of the house or permanently altering roof with the installation of solar panels. Panels should be installed flat and not alter the slope of the roof. They should be positioned behind existing architectural features such as parapets, dormers, and chimneys to limit their visibility.

5. Compatibility. Use solar panels and mounting systems that are compatible in color to the property's roof materials.

6. Free-Standing Solar Racks. Free-standing solar racks larger than 120 sq. ft. will be considered on a case-by-case basis. Solar racks installed at the rear of the property with no or limited visibility and create a shade structure or installed on an outbuilding, such as on a garage roof.

7. Low Pitch Roofs for Solar Panels . Low pitch roofs may utilize low-profile panels on non-street-facing roof planes. Avoid roof racks that elevate the panels or are at a different pitch than the roof.

8. Solar Shingles. Solar shingles may be installed on sloped roof-surfaces and are less intrusive than panels. However, removal of historic materials must be avoided.

8. Flat Roofs. On structures with flat roofs, solar panel installations are to set back from the roof edge to minimize visibility. Pitch and elevation should be adjusted to reduce visibility from public right-of-way.

### ~~13.10 Guidelines for Accessibility, and Health & Safety Considerations~~

#### ~~3.19.1 Standards for Administrative Bypass for Accessibility, Health & Safety~~

The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, then the application will be forwarded to the Historic District Commission for a full review.

.1 Access Ramp. Access ramps can be approved by Administrative Bypass if they meet the following standards:

- a. Wood, Wood-like materials, such as smooth cement fiberboard, and temporary metal

- ramps can be used.
- b. Vinyl material is prohibited.
- c. Temporary and removable, and do not permanently alter the historic structure
- d. Located on the rear of the structure, not visible from the front right-of-way.
- e. Side and front ramps require review by the Historic District Commission.

.2 Safety Aid. Elements such as handrails, grab bars, or other safety aids shall be added in a way that preserves character-defining features and finishes of the structure and allows them to be removed when no longer needed.

.3 Doorways. The widening of entryways can be approved by administrative bypass if located on the rear of the structure and not visible from the front right-of-way.

### 3.20 Guidelines for Accessibility, and Health and Safety Considerations

A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):

- .1 Security Bars Require Approval. A Certificate of Appropriateness is required for the installation of ~~burglar security~~ bars within historic districts. Security bars shall be designed to complement the style and design characteristics of the structure to which they are being attached.
- .2 Accessibility Ramps. May Be Eligible for Administrative Bypass. ~~Wooden~~ The Commission will use the following when ~~accessibility~~ considering accessibility ramps ~~may be approved on the front façade or side of structure; ed by Administrative Bypass.~~ Ramps shall be designed to have minimal structural and visual impact on the historic resource. See Chapter 1.32 for more information on Administrative Bypass.
  - a. Locate ramp with the least amount of visibility from the front right-of-way.
  - b. Ramps must be temporary and composed of wood, cement fiberboard, or metal.  
Concrete ramps on the rear of the structure will be considered on a case-by-case bases.
  - c. Cannot permanently alter the historic structure or be permanently attached to the structure.
  - d. Must be easily removable and reversible.
- .3 Lifts Require Approval. ~~Accessibility aids such as ramps or lifts that require concrete, brick or other more permanent foundations are allowed on the rear of the structure with no visibility from the front right of way. require a Certificate of Appropriateness.~~
- .4 Add Safety Aids Carefully. Elements such as handrails, grab bars, or other safety aids shall be added in a way that preserves character-defining features and finishes of the structure and allows them to be removed when no longer needed.
- .5 Modify Doorways Carefully. ~~A doorway is a critical design element in a historic structure, so a Certificate of Appropriateness is required to alter an entryway. In an emergency situation, an entryway modification application and hearing may be expedited. The enlargement of a door opening on the rear of the structure is allowable on a case-by-case basis.~~

## Additions and New Construction

### 4.14.1 Guidelines for Decks

#### 4.23 Standards for Administrative Bypass for Decks:

The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, then the application will be forwarded to the Historic District Commission for a full review.

#### .1 Decks under 300 square feet:

- a. Less than 300 square feet in total area. Located behind the structure and not visible from the front right-of-way. Corner lots have two front right-of-ways.
- b. Constructed in a way that makes no permanent changes to the historic structure. Built of compatible wood, wood composite or smooth cement board with functional elements made of metal elements. Synthetic materials such as plastic and vinyl are prohibited.
- c. Decks that with roofs or walls will be forwarded as a porch or balcony request for a full review by the Historic District Commission.

### 4.34.2 Guidelines for Decks

A full review by the Historic District Commission will take the following criteria into consideration before issuing a Certificate of Appropriateness (COA):

.1 Protect Historic Fabric of Structure. Locate and construct decks so that the historic fabric of the primary structure and its character-defining features and details are not damaged or obscured. Install decks so that they are structurally self-supporting and may be removed in the future without damage to the historic structure.

.2 Choose Inconspicuous Locations Deck Locations. ~~Front decks are prohibited. Decks on the rear should be inset from the rear corners to eliminate visibility from the front right-of-way. Introduce decks in inconspicuous locations, usually on the building's rear elevation and inset from its rear corners, where the deck will not be visible from the street.~~ Decks on corner properties will be reviewed on a case-by-case basis.

.3 Deck Design Should Reflect Building Design. Design decks and their associated railings and steps to reflect the materials, scale, and proportions of the building.

.4 Design Visible Decks Carefully. ~~Where it is appropriate to site a deck in a location visible from the street (i.e. the side of a building), treat the deck in a more formal architectural way.~~

.45 Align Deck with First Floor Level. Decks shall ~~generally~~ be no higher than the building's first-floor level. Visually tie the deck to the building by screening with compatible foundation materials such as skirt boards, lattice, or dense evergreen foundation plantings.

.56 Preserve Significant Building Elements. ~~It is not appropriate to introduce a deck~~

Preserve significant building and site elements and new deck installations are not to obscure or remove significant building or site elements. if doing so will require removal of a significant building element or site feature.

~~.67~~ **Decks May Not Detract from Overall Character.** It is not appropriate to introduce a deck if ~~the deck~~ it will detract from the overall historic character of the building or the site.

~~.8~~ **Administrative Bypass.** Deck construction may be approved by Administrative Bypass if the proposed deck footprint is less than 300 square feet, is not visible from the street, does not make changes to the historic structure itself, and meets City lot coverage restrictions.

## Additions to Historic Buildings

### 4.45 3.14.2 Guidelines for Additions to Historic Buildings

A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):

- .1 **Make Additions Compatible.** Additions shall be compatible with the historic building in size, scale, mass, materials, proportions and the pattern of windows and doors to solid walls.
- .2 **Locate Addition Inconspicuously.** Locate a new addition on an inconspicuous facade of the historic building, usually the rear one. Additions that alter the front facade are generally considered inappropriate for a historic structure.
- .3 **Limit Size and Scale.** The footprint of the addition shall not exceed 50% of the footprint of the existing structure or 750 square feet, whichever is greater. Exterior dimensions of the addition shall not exceed the exterior dimensions of the existing structure, including height, width, and depth. An addition which does not increase the footprint of the existing structure may be allowed to increase roof height and will be reviewed on a case-by-case basis.
- .4 **Preserve the Site.** Design new additions so that the overall character of the site, character-defining site features, and trees, are retained.
- .5 **Avoid Detracting From Principal Building.** It is not appropriate to construct an addition if it will detract from the overall historic character of the principal building and the site, or if it will require the removal of a significant building element or site feature. Construct new additions so that character-defining features of the historic buildings are not destroyed, damaged, or obscured.
- ~~.6~~ **~~Small Buildings Allowable by Administrative Bypass.~~** ~~Accessory buildings which have a footprint no greater than 108 square feet and are not constructed on or attached to a concrete slab, foundation, or permanent base and have no electric, plumbing, or gas service connection do not require a building permit. However, an Administrative Bypass is required, subject to the conditions set forth in Chapter 1.32. It is recommended that the design of these buildings be compatible with the primary structure and the other surrounding or nearby structures or screened with fencing or landscaping.~~



## New Primary Structures

### 4.56 4.14.3 Guidelines for New Primary Structures

A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):

- .1 Consider Historic Context. Design new structures to be compatible with historic buildings in the district in terms of size, scale, height, form, massing, proportions, finished floor elevation, size of door and window openings, ~~and roof shape, and setbacks~~. Proposals for new construction shall include streetscape elevation drawings that depict proposed structure as well as elevations of properties on either side to provide a comparison of massing, scale, floor elevations, proportions, setback and design.
- .2 Select ~~Doors & Windows~~ and Doors Carefully. Select ~~doors and windows and doors~~ for new buildings that are compatible in material, proportion, pattern, and detail with the windows and doors and windows of historic buildings in the district. See Chapters ~~3.5 Doors and Windows~~. 3.11 through 3.14.
- .3 Select Compatible Finishes. Select materials and finishes for proposed new buildings that are compatible with historic materials and finishes found in historic buildings in the district in terms of composition, scale, pattern, detail, texture, and finish.
- .4 ~~Evaluate Potential for Archaeological Resources.~~ Evaluate in advance and limit any disturbance to the site's terrain during construction to minimize the possibility of destroying unknown archaeological resources. See Chapter 2.2 Archaeology.
- .4 ~~Design of New Primary Structures.~~ Design new primary structures to be compatible with historic buildings in the district in terms of size, scale, height, form, massing, proportion, finished floor elevation, size of door and window openings, and roof shape. Proposals for new primary structures shall include streetscape elevation drawings that depict proposed structure as well as elevations of properties on either side to provide a comparison of massing, scale, and design.
- .5 ~~Location of Primary Structures.~~ New primary structures should align with the typical front and side setback on the block.
- .9 ~~Select Doors & Windows Carefully.~~ Select doors and windows for new buildings that are compatible in material, proportion, pattern, and detail with the doors and windows of historic buildings in the district. See Sections 3.5 and 3.6 Windows and Door.
- .10 ~~Select Compatible Finishes.~~ Select materials and finishes for proposed new buildings that are compatible with historic materials and finishes found in historic buildings in the district in terms of composition, scale, pattern, detail, texture, and finish.
- .6H Evaluate Potential for Archaeological Resources. Evaluate in advance and limit any

disturbance to the site's terrain during construction to minimize the possibility of destroying unknown archaeological resources.

.712 Avoid False Historical Appearance. New structures should be of their own time period and easily distinguishable from the historic structure.

## Relocation of Structures

### 4.5.15.1 Guidelines for Relocation of Structures

#### 4.75.1 Standards for Administrative Bypass

The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, then the application will be forwarded to the Historic District Commission for a full review.

.1 Relocation of Structures less than 120 square feet. Non-historic accessory structure less than 120 square feet may be relocated to another location in the rear yard not visible from the front right of way. Relocation outside the district is allowed as well.

### 5.2.8 Guidelines for Relocation of Structures.

A review by the Historic District Commission will use the following criteria for the issuance of a Certificate of Appropriateness (COA):

.1 **Document Original Context.** Before moving a historic structure, applicants and City staff shall document its original setting and context using photographs, site plans, or other graphic or written statements to record the existing site conditions.

.2 **Protect Existing Structures.** Ensure that the relocation of a structure will not diminish or damage existing buildings or the overall character of the historic district. Pay particular attention to protection of the tree canopy along the route of the move.

.3 **Furnish Relocation Site Plans.** Applicants shall provide the Historic District Commission with detailed site plans for proposed site features and plantings of the new setting, including information on accessory buildings, driveways, site lighting, and parking areas.

.4 **Protect Significant Features.** Protect significant site features of the original site, the new site, and the route of the move during the relocation.

## Demolition of Structures

### 5.39.15.2 Standards for Administrative Bypass for the Demolition of Structures

.1 Demolition of structures less than 120 square feet. Non-historic accessory structure less than 120 square feet may be demolished.

### 5.410.2 Guidelines for Demolition of Structures

~~.1 — A Certificate of Appropriateness (COA) is Required for Demolition and Construction of New Primary Structures. Applicants must obtain a Certificate of Appropriateness for construction of new primary structures on a demolition site prior to the demolition taking place.~~

~~.2 — Submit Site Plan. Before demolition occurs, submit a site plan to the Historic District Commission illustrating proposed site development to follow demolition.~~

~~.3 — Document Structure Thoroughly. Before demolition, record significant structures through photographs and/or measured drawings as specified by the Historic District Commission and City Staff.~~

*The following items can receive a Certificate of Appropriateness (COA) through the Administrative Bypass process if they meet the criteria listed. If they do not meet the criteria, then the application will be forwarded to the Historic District Commission for a full review.*

.1 — A Certificate of Appropriateness. A Certificate of Appropriateness is required to be issued prior to demolition.

.2 — Criteria for Demolition. Demolition requests must meet Zoning Ordinance Section 429.3.9(c), Criteria for Demolition.

.3 — Procedures and Process for Demolitions. Demolitions must meet the Zoning Ordinance Section 429.3.9(b), Procedure and Postponement Orders.

.4 — Site Plan Required. Applicants shall provide the Historic District Commission with detailed site plans for proposed site features of the new parcel, including information any structures, driveways, site lighting, and parking areas.

.5 — Document Thoroughly. Document original context of the historic structure prior to demolition.

## Appendices

6.1 Technical Resources [section underdevelopment....repository for bibliography, recommended readings, preservation resource guides, glossary etc.]

### 6.2 Definitions

**Addition** — construction that increases any exterior dimension of an original structure by building outside of the existing walls and/or roof. Additions can be either horizontal or vertical.

**Alteration** — an act that changes one or more of the exterior architectural features of a structure or its appurtenances, including but not limited to the erection, construction, reconstruction, or removal of any structure or appurtenance.

**Appropriate** — typical of the historic architectural style, compatible with the character of the historic district, and consistent with the *Norman Historic Preservation Handbook*.

**Architectural resources** — districts, structures, buildings, monuments, sites, or landscaping which possess local interest or artistic merit or which are particularly representative of their class or period, or represent achievements in architecture, engineering, or design.

**Certificate of Appropriateness (COA)** — the official document issued by the Historic District Commission approving any application affecting the exterior of any structure designated by the authority of the Historic District Ordinance for permission to construct, erect, demolish, remove, relocate, reconstruct, restore, or alter said structure.

**Commission** — the Historic District Commission of the City of Norman.

**Compatible** — a design or use that does not conflict with the historical appearance of a building or district and does not require irreversible alteration.

**Contributing resource** — a historic building or site that retains the essential architectural integrity of its original design or condition and whose architectural style is typical of or integral to a historic district.

**Damaged or diseased tree** — A tree that is damaged in such a way as to create a hazard (e.g. has a large wound) or has been pruned in a way which permanently alters its natural attributes (e.g. topped). A seriously diseased tree is one with obvious signs of internal decay (e.g. cavity with fruiting bodies present), is infested with a disease for which there is no remedy (e.g. Pine Wilt, Dutch Elm Disease), or suffers from a decline disorder.

**Demolition** — the removal of any historic structure from its original site. This includes moving a building from one site to another.

**Elevation** — a drawing showing the vertical elements of a building, either exterior or interior, as a direct projection to a vertical plane.

**Facade** — the exterior face of a building.

**False historical appearance** — architectural features or details introduced to a structure that do not reflect its period, style, or design.

**Feature** — a structural or decorative element that contributes to the overall character of that building, e.g. walls, foundations, roofs, chimneys, steps, piers, columns, lintels, and sills.

**Guidelines** — ~~An important part of the *Norman Historic Preservation Handbook*. The guidelines are a set of rules administered by the Norman Historic District Commission intended to assist owners of historic buildings in Norman's historic districts maintain, preserve, protect, and enhance the architectural quality of their property.~~ Guidelines are utilized by the Norman Historic District Commission to determine if a proposed work is compatible with the principal historic structure on the site as well as compatible with the adjacent or surrounding historic district.

**Historic district** — a geographically definable area with a concentration or linkage of significant sites, buildings, structures, or monuments; or, an individual structure, building, site or monument which contributes to the cultural, social, political, or architectural heritage of the City of Norman.

**Historic District Ordinance** – the portion of *Norman Zoning Ordinance* (Chapter 22:429.3HD) establishing an overlay zoning district for the purpose of protecting and preserving the architectural, cultural, and historic resources included in that designated district.

**Historic property** — any individual structure, building, site or monument which contributes to the historic, architectural, archeological and/or cultural heritage of the City of Norman, Oklahoma as determined by the Historic District Commission.

**Historic resources** — sites, districts, structures, buildings, or monuments that represent facets of history in the locality, state or nation; places where significant historical or unusual events occurred; places associated with a personality or group important to the past.

**Infill construction** — the erection of a new structure between or adjacent to existing buildings or the relocation of an existing structure to a vacant lot from another location.

~~In-kind~~ **In-kind** — the replacement of existing materials or features with materials of identical appearance and/or composition. (See also: matching)

**Like with like** — repair or replacement of deteriorated exterior features or site elements with identical materials.

**Matching** — in historic rehabilitations, the use of replacement materials that are identical to the original in composition, size, shape, and profile. (See also: ~~in-kind~~ **in-kind**).

**National Register of Historic Places** — the national list of districts, sites, buildings, structures, and objects significant in American history, architecture, archeology, engineering and culture, maintained by the Secretary of the Interior under authority of Section 101(a)(1)(A) of the National Historic Preservation Act, as amended.

**New construction** — see: infill construction.

**Non-contributing resource** — a resource that adds no historical significance to an individual property, site, or district, and detracts from the visual integrity or interpretability of an historic district.

**Ordinary maintenance and repair** — work meant to remedy damage or deterioration of a structure or its appurtenances, and which will involve no change in materials, dimensions, design, configuration, texture or visual appearance to the exterior of an historic structure. Ordinary maintenance and repair shall include painting and reroofing with similar materials.

**Original** — buildings, building materials or features that were present during the period of significance for the historic district.

**Period of significance** — the span of time during which a group of properties attained the significance that makes them eligible for designation as a historic district.

**Preservation** — the adaptive use, conservation, protection, reconstruction, rehabilitation, or stabilization of buildings, districts, monuments, sites, or structures significant to the heritage of the people of Norman. The following terms further define types of preservation activities:

*Adaptive Use* – the restrained alteration of a historical or architectural resource to accommodate uses for which the resource was not originally constructed, but in such a way so as to maintain the general historical and architectural character.

*Conservation* – the sustained use and appearance of a resource essentially in its existing state.

*Protection* – the security of a resource as it exists through the establishment of the mechanisms of this section.

*Reconstruction* – the act or process of duplicating the original structure, building form and materials by means of new construction based on documentation of the historic condition.

*Rehabilitation* – the act or process of making a compatible use for a property through repair, alterations, and additions while preserving those portions or features which convey its historic, cultural or architectural values.

*Restoration* — the act or the process of accurately depicting the form, features, and character of a property as it appeared at a particular period of time by removing features or changes from other periods in its history and reconstructing missing features from the restoration period.

*Stabilization* – the process of applying methods designated to halt deterioration and to establish the structural stability of an unsafe or deteriorated resource while maintaining the essential form as it presently exists without noticeably changing the exterior appearance of the resource.

**Relocation** — the movement or repositioning of a primary or accessory structure on its original site, or from one location to another.

***Secretary of the Interior Standards for Rehabilitation of Historic Buildings*** — a set of standards intended to assist the long-term preservation of a historic property through the preservation of historic building materials and features. The Standards pertain to historic buildings of all materials, construction types, sizes, and occupancy and encompass the exterior and interior of the buildings. “Rehabilitation” is defined as “the process of returning a property to a state of utility, through repair or alteration, which makes possible an efficient contemporary use while still preserving those portions and features of the property which are significant to its historic, architectural, and cultural values.”



**Significant characteristics** — those characteristics which are important to or expressive of the historic or architectural quality and integrity of the resources and its setting and which include, but are not limited to building material, detail, height, proportion, rhythm, scale, setback, setting, shape, street accessories, and workmanship. Examples include:

*Building mass* — describes the relationship of a building's height to its width and depth.

*Building materials* — the physical characteristics which create the aesthetic and structural appearance of the resource, including but not limited to a consideration of the texture and style of the components and their combinations, such as brick, stone, shingle, wood, concrete, or stucco.

*Detail* — architectural aspects which, due to particular treatment, draw attention to certain parts or features of a structure.

*Height* — the vertical dimension of a given structure, building or monument.

*Proportion* — the relative physical sizes within and between buildings and building components.

*Rhythm* — a discernible pattern of shapes including, but not limited to, windows, doors, projections, and heights, within a building, structure or monument, or a group of same.

*Scale* — the proportion of parts of a building, structure, or monument to one another and to the human figure.

*Setting* — the surrounding structures, monuments, and landscaping which establish the visual, aesthetic, or auditory qualities of the historic or architectural resources.

*Shape* — the physical configuration of structures or landscaping and their component parts.

**Streetscape** — the view along a street from the perspective of a driver or pedestrian. The streetscape includes street trees, lawns, buildings, landscape buffers, signs, street lights, above-ground utilities, drainage structures, sidewalks, bus stop shelters and street furniture.

**Structure** — anything constructed or erected, the use of which requires permanent location on the ground or which is attached to something having a permanent location on the ground. These include, but are not limited to, buildings, fences, walls, driveways, sidewalks and parking areas.

**Stucco** — an exterior finish, usually textured, composed of Portland cement, lime, and sand mixed with water. Older types of stucco may be mixed from softer masonry cement rather than Portland cement.

**NORMAN PLANNING COMMISSION  
REGULAR SESSION MINUTES**

**DECEMBER 9, 2021**

The Planning Commission of the City of Norman, Cleveland County, State of Oklahoma, met in Regular Session in the Council Chambers of the Norman Municipal Building, 201 West Gray Street, on the 9<sup>th</sup> day of December, 2021.

Notice and agenda of the meeting was posted at the Norman Municipal Building and online at <https://norman-ok.municodemeeetings.com> at least twenty-four hours prior to the beginning of the meeting.

Chair Erica Bird called the meeting to order at 6:30 p.m.

\* \* \*

**ROLL CALL**

**MEMBERS PRESENT**

Erin Williford (arrived at 6:32 p.m.)  
Kevan Parker  
Steven McDaniel  
Erica Bird  
Dave Boeck  
Sandy Bahan  
Michael Jablonski

**MEMBERS ABSENT**

Lark Zink

A quorum was present.

**STAFF MEMBERS PRESENT**

Jane Hudson, Director, Planning &  
Community Development  
Lora Hoggatt, Planning Services Manager  
Logan Hubble, Planner I  
Anais Starr, Planner II  
Colton Wayman, Planner I  
Roné Tromble, Recording Secretary  
Ken Danner, Subdivision Development  
Manager  
Kathryn Walker, City Attorney  
Heather Poole, Asst. City Attorney  
Jeanne Snider, Asst. City Attorney  
Jami Short, Traffic Engineer  
Bryce Holland, Multimedia Specialist

\* \* \*

Item No. 14, being:

**CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF RESOLUTION NO. R-2122-58 -- A RESOLUTION OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, ADOPTING THE HISTORIC PRESERVATION GUIDELINES TO BE USED BY THE NORMAN HISTORIC DISTRICT COMMISSION IN REVIEWING PROPOSED ADDITIONS, ALTERATIONS, AND DEMOLITIONS TO STRUCTURES LOCATED IN THE HISTORIC DISTRICTS.**

**ITEMS SUBMITTED FOR THE RECORD:**

1. Staff Report
2. Exhibit A – 2018 Historic Preservation Update Subcommittee List of Revisions
3. Exhibit B – 2020 Draft Historic Preservation Handbook & Guidelines
4. Exhibit C – 2021 Summary of Public Comments on Revised HP Guidelines
5. Exhibit D – 2021 Historic Preservation Guidelines Clean
6. Exhibit E – 2021 Annotated Historic Preservation Guidelines
7. Exhibit F – August 4, 2021 Historic District Commission Minutes

**PRESENTATION BY STAFF:**

1. Ms. Anais Starr presented the staff report.

Commissioner Bird asked a question about garage apartments.  
Ms. Starr responded.

**AUDIENCE PARTICIPATION:**

None

**DISCUSSION AND ACTION BY THE PLANNING COMMISSION:**

*Steven McDaniel moved to recommend adoption of Resolution No. R-2122-58 to City Council.  
Kevan Parker seconded the motion.*

There being no further discussion, a vote on the motion was taken with the following result:

YEAS	Erin Williford, Kevan Parker, Steven McDaniel, Erica Bird, Dave Boeck, Sandy Bahan, Michael Jablonski
NAYES	None
MEMBERS ABSENT	Lark Zink

The motion, to recommend adoption of Resolution No. R-2122-58 to City Council, passed by a vote of 7-0.

\* \* \*

**File Attachments for Item:**

22. CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF ORDINANCE O-2122-31 UPON SECOND AND FINAL READING: AN ORDINANCE OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA AMENDING CHAPTER 22 (ZONING ORDINANCE), ARTICLE XI, SPECIFIC DISTRICT REGULATIONS, SECTION 429.3 IN DEFINITIONS DELETING CONSERVATION AND ADDING ORIGINAL, PERIOD OF SIGNIFICANCE, RELOCATION, AND SECRETARY OF THE INTERIOR STANDARDS OF HISTORIC BUILDINGS; EDITS FOR CONSISTENCY OR CORRECTION PURPOSES; ADDING WRITTEN DENIAL REQUIREMENT; ADDING SEVEN DAY NOTICE; EXTENDING TIME LIMIT OF CERTIFICATE OF APPROPRIATENESS (COA) FROM SIX MONTHS TO TWELVE MONTHS; AND RESTRUCTURING EXCEPTIONS TO ADMINISTRATIVE BYPASS; AND PROVIDING FOR THE SEVERABILITY THEREOF.



## CITY OF NORMAN, OK STAFF REPORT

**MEETING DATE:** 01/25/2022

**REQUESTER:** Anais Starr, Planner II

**PRESENTER:** Jane Hudson, Director, Planning & Community Development

**ITEM TITLE:** CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF ORDINANCE O-2122-31 UPON SECOND AND FINAL READING: AN ORDINANCE OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA AMENDING CHAPTER 22 (ZONING ORDINANCE), ARTICLE XI, SPECIFIC DISTRICT REGULATIONS, SECTION 429.3 IN DEFINITIONS DELETING CONSERVATION AND ADDING ORIGINAL, PERIOD OF SIGNIFICANCE, RELOCATION, AND SECRETARY OF THE INTERIOR STANDARDS OF HISTORIC BUILDINGS; EDITS FOR CONSISTENCY OR CORRECTION PURPOSES; ADDING WRITTEN DENIAL REQUIREMENT; ADDING SEVEN DAY NOTICE; EXTENDING TIME LIMIT OF CERTIFICATE OF APPROPRIATENESS (COA) FROM SIX MONTHS TO TWELVE MONTHS; AND RESTRUCTURING EXCEPTIONS TO ADMINISTRATIVE BYPASS; AND PROVIDING FOR THE SEVERABILITY THEREOF.

### **BACKGROUND:**

Council enacted Ordinance O-9293-30 on August 10, 1993, establishing the Historic District Ordinance for the City of Norman. The ordinance establishes the Historic District Commission and associated provisions to regulate designated Historic Districts including the development and revision of Historic Preservation Guidelines.

With the adoption of revised Historic Preservation Guidelines in the previous agenda item, it is necessary to update the Historic District Ordinance to reflect those revisions. This also provides an opportunity to correct any errors or issues identified with the Ordinance.

### **DISCUSSION:**

After the Draft Historic Preservation Guidelines were recommended for approval on August 2, 2021 by the Historic District Commission, staff revised the Historic District Ordinance to incorporate necessary revisions to allow for enforcement of the revised Guidelines. Additionally, other revisions were identified by staff and presented to the Commission for consideration. The Commission discussed the proposed revisions to the Historic District Ordinance at their

October 4, 2021 meeting and recommended the adoption of the attached Historic District Ordinance. The following provides a summation of the revisions proposed.

### **SUMMARY OF HISTORIC DISTRICT ORDINANCE REVISIONS**

1. Removal of a repetitive definition for the word *conservation* which is defined elsewhere in the Ordinance.
2. Provided definitions of original, period of significance, relocation, Secretary of the Interior Standards.
3. Reorganized sections of the ordinance to provide clarity. For example, two sections discussed the appeals process. The two sections were combined into one section to provide clarity.
4. Requires adjacent property owner letters be sent out at least 7 calendar days prior to the Historic District Meeting. Presently, there is not a specific notice date requirement for adjacent property owner notification letters.
5. Extended the expiration of Certificate of Appropriateness from six months to 12 months. This provides a more reasonable amount of time for applicants to begin projects.
6. Restructured ordinance to allow additional items approvable by the Administrative Bypass process as identified in the Historic Preservation Guidelines.
7. The correction of typos found during the revision process.

### **CONCLUSION:**

Staff presents Ordinance O-2122-31 to the City Council for discussion and consideration.

At their December 9, 2021 meeting, Planning Commission unanimously recommended adoption of Ordinance O-2122-31, by a vote of 7-0.



AN ORDINANCE OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA AMENDING CHAPTER 22 (ZONING ORDINANCE), ARTICLE XI, SPECIFIC DISTRICT REGULATIONS, SECTION 429.3 IN DEFINITIONS DELETING CONSERVATION AND ADDING ORIGINAL, PERIOD OF SIGNIFICANCE, RELOCATION, SECRETARY OF THE INTERIOR STANDARDS OF HISTORIC BUILDINGS; EDITS FOR CONSISTENCY OR CORRECTIVE PURPOSES; ADDING WRITTEN DENIAL REQUIREMENT; ADDING SEVEN DAY NOTICE; EXTENDING TIME LIMIT OF COA FROM SIX MONTHS TO TWELVE MONTHS; RESTRUCTURING EXCEPTIONS TO ADMINISTRATIVE BYPASS; AND PROVIDING FOR THE SEVERABILITY THEREOF.

NOW THEREFORE, BE IT ORDAINED BY THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA:

\* \* \* \* \*

§ 1. That, Section 429.3 of Chapter 22 of the Zoning Ordinance of the City of Norman, Oklahoma be amended as follows:

\* \* \* \* \*

#### SEC 429.3 HD, HISTORIC DISTRICT

1. Description and Purpose. The Historic District Ordinance, hereinafter referred to as the "HDO", and its regulations may be applied to property located in any zoning district in accordance with the provisions of this Ordinance. The HDO is intended to be an overlay zoning district and the regulations imposed by such district shall be in addition to the regulations of the underlying zoning district applicable to the subject parcel.

The City of Norman hereby declares that the historical, architectural, cultural, and aesthetic features of the City represent some of the finest and most valuable resources of the City, and such resources are the embodiment of the heritage of the people of the City of Norman. Therefore, it is hereby declared that the purposes of this Ordinance, to be known as the Historic District Ordinance, shall be as follows:

\* \* \*

(g) To safeguard the heritage of the City by preserving and regulating historic district structures in such a way that maintains or restores their historic integrity while allowing modern day uses and conveniences for their residents.

2. Definitions. As used in this chapter, unless the context otherwise requires, the following words or phrases have the meaning listed:

- (a) Addition – construction that increases the size of the original structure by building outside of the existing structure. Additions can be either horizontal or vertical.
- (b) Alteration – an act that changes one or more of the exterior architectural features of a structure or its appurtenances, including but not limited to the erection, construction, reconstruction, or removal of any structure or appurtenance.
- (c) Appropriate – typical of the historic architectural style, compatible with the character of the historic district, and consistent with the Preservation Guidelines of the City of Norman.
- (d) Architectural Resources - districts, structures, buildings, monuments, sites, or landscaping which possess local interest or artistic merit or which are particularly representative of their class or period, or represent achievements in architecture, engineering, or design.
- (e) Certificate of Appropriateness (COA) - the official document issued by the Historic District Commission approving any application affecting the exterior of any structure designated by the authority of this Ordinance for permission to construct, erect, demolish, remove, relocate, reconstruct, restore, or alter said structure.
- (f) Commission – The Historic District Commission of the City of Norman.
- (g) Compatible – means a design or use that does not conflict with the historical appearance of a building or district and does not require irreversible alteration.
- (h) Contributing Resource – means a resource -- a building, site, or district -- that retains its essential architectural integrity in design and whose architectural style is typical of or integral to a historic district.
- (i) Elevation – an exterior wall of a structure.
  - 1. *Front elevation* – the façade or face of a structure which is visible and prominent from a public right-of-way and which often has distinguishing architectural features. Structures on corner lots shall be considered to have two front elevations. No structure shall be considered to have more than two front elevations.
  - 2. *Side elevation* – a wall adjacent to the front elevation that is usually visible from a public right-of-way.
  - 3. *Rear elevation* – an elevation parallel to the front façade; the rear elevation usually includes the back door of the structure.
  - 4. *Primary elevation* – the front or side elevation of a structure.

5. *Secondary elevation* – the rear elevation of a structure.

- (j) *Façade* – the front wall or face of a building.
- (k) *Historic District* - a geographically definable area with a concentration or linkage of significant sites, buildings, structures, or monuments; or, an individual structure, building, site or monument which contributes to the cultural, social, political, or architectural heritage of the City of Norman.
- (l) *Historic Preservation Officer* – the chief staff person responsible for historic preservation in the City of Norman’s Planning and Community Development Department.
- (m) *Historic Property* -- any individual structure, building, site or monument which contributes to the historic, architectural, archeological and/or cultural heritage of the City of Norman, Oklahoma as determined by the Historic District Commission.
- (n) *Historic Resources* - sites, districts, structures, buildings, monuments, major landscape features that represent facets of history in the locality, state or nation; places where significant historical or unusual events occurred; places associated with a personality or group important to the past.
- (o) *Infill construction* – construction on property between or adjacent to existing buildings.
- (p) *In kind* – to replace existing materials or features with materials of identical design, size, texture and composition. (see also: matching)
- (q) *Landmark* – an individual structure, building, site, or monument that contributes to the historical, architectural, or archaeological heritage of the city.
- (r) *Matching* – In historic rehabilitations, the use of replacement materials that are identical to the original in composition, size, shape, and profile. (see also: in kind)
- (s) *National Register of Historic Places* - the national list of districts, sites, buildings, structures, and objects significant in American history, architecture, archeology, engineering and culture, maintained by the Secretary of the Interior under authority of Section 101(a)(1)(A) of the National Historic Preservation Act, as amended.
- (t) *Non-Contributing Resource* – A building, structure, or site that does not add to the historic significance of a property or district, and which detracts from the visual integrity or interpretability of an historic district.
- (u) *Ordinary Maintenance and Repair* – Work meant to remedy damage or deterioration of a structure or its appurtenances, and which will involve no change in materials, dimensions, design, configuration, texture or visual appearance to the exterior of an

- historic structure. Ordinary maintenance and repair shall include, but is not limited to, painting and reroofing.
- (v) Original – buildings, building materials or features that were present during the period of significance for the historic district.
  - (w) Period of Significance – the span of time during which a group of properties attained the significance that makes them eligible for designation as a historic district.
  - (x) Preservation – the adaptive use, conservation, protection, reconstruction, rehabilitation, or stabilization of buildings, districts, monuments, sites, or structures significant to the heritage of the people of Norman. The following terms further define types of preservation activities:
    - 1. *Adaptive Use* – the restrained alteration of a historical or architectural resource to accommodate uses for which the resource was not originally constructed, but in such a way so as to maintain the historical and architectural character of the resource.
    - 2. *Conservation* – the sustained use and appearance of a resource essentially in its existing state.
    - 3. *Historic Rehabilitation* – the act or process of making a compatible use for a property through repair, alterations, and additions while preserving those portions or features which convey its historic, cultural or architectural values.
    - 4. *Historic Reconstruction* – the act or process of duplicating the original structure, building form and materials by means of new construction based on documentation of the historic condition.
    - 5. *Protection* – the security of a resource as it exists through the establishment of the mechanisms of this section.
    - 6. *Restoration* – the process of accurately recovering all or a part of the form and details of a resource and its setting as it appeared at a particular period by means of the removal of later work and the replacement of missing earlier work.
    - 7. *Stabilization* – the process of applying measures designated to halt deterioration and to establish the structural stability of an unsafe or deteriorated resource while maintaining the essential form as it presently exists without changing the exterior appearance of the resource.
  - (y) Relocation – the movement or repositioning of a primary or accessory structure on its original site, or from one location to another.

- (z) Secretary of the Interior Standards of Historic Buildings - A set of principles established in 1977 and amended periodically thereafter, by the Secretary of the Interior, who is responsible for all national preservation programs under Department of the Interior authority and for advising federal agencies on the preservation of historic properties listed or eligible for listing in the National Register of Historic Places. The Norman Historic District Commission utilizes the Secretary of Interior Standards as basis for developing the City of Norman Preservation Guidelines as well as for design review.
- (aa) Significant Characteristics -those characteristics that are important to or expressive of the historic or architectural quality and integrity of the resources and its setting and which include, but are not limited to building mass, building material, detail, height, proportion, rhythm, scale, setback, setting, shape, street accessories, and workmanship.
1. *Building Mass* – describes the relationship of a building’s height to its width and depth.
  2. *Building Materials* - the physical characteristics which create the aesthetic and structural appearance of the resource, including but not limited to a consideration of the texture and style of the components and their combinations, such as brick, stone, shingle, wood, concrete, or stucco.
  3. *Detail* - architectural aspects which, due to particular treatment, draw attention to certain parts or features of a structure.
  4. *Height* - the vertical dimension of a given structure, building or monument.
  5. *Proportion* - the relative physical sizes within and between buildings and building components.
  6. *Rhythm* - a discernible pattern of shapes including, but not limited to, windows, doors, projections, and heights, within a building, structure or monument, or a group of same.
  7. *Setback* - the distance that a structure sets from the property line, typically the front or side property line.
  8. *Scale* - the proportion of parts of a building, structure, or monument to one another and to the human figure.
  9. *Setting* - the surrounding structures, monuments, and landscaping which establish the visual, aesthetic, or auditory qualities of the historic or architectural resources.
  10. *Shape* - the physical configuration of structures or landscaping and their component parts.

11. *Street Accessories* - those sidewalk or street fixtures which include, but are not limited to, trash receptacles, benches, signs, lights, hydrants, and landscaping.
- (bb) State Historic Preservation Officer (SHPO) - the official within the State of Oklahoma who has been delegated and appointed by the Governor to administer the Historic Preservation Program in the State.
- (cc) State Register of Historic Places - the State of Oklahoma list of districts, sites, buildings, structures and objects significant in state history, architecture, archeology, engineering and culture, maintained by the State Historic Preservation Officer, under the authority of 53 O.S., 1984 Supplement, Sections 351-355.
- (dd) Streetscape - the view along a street from the perspective of a driver or pedestrian, of the natural and man-made elements in or near the street right of way, including buildings and their relationship to street trees, lawns, landscape buffers, signs, street lights, above-ground utilities, drainage structures, sidewalks, bus stop shelters and street furniture.
- (ee) Structure - anything constructed or erected, the use of which requires permanent location on the ground or which is attached to something having a permanent location on the ground. These include, but are not limited to, buildings, fences, walls, driveways, sidewalks and parking areas.
3. District Regulations. The following regulations shall be applicable to the HD, Historic District, and shall control the use of all properties within such district:
- (a) Any person responsible for a structure, building, landmark, or monument within a Historic District shall keep all of the exterior portions of such resources in good repair.
- (b) The erection, moving, demolition, removal, rehabilitation, reconstruction, restoration, or alteration of the exterior of any structure is prohibited unless a Certificate of Appropriateness (COA) is granted by the Historic Commission of the City of Norman, unless such Certificate is not required by Subsection 8.
- (c) Changes to rear elevations do require a COA; however the rear elevation of a historic structure is considered a secondary elevation and is therefore regulated to a lower standard to allow flexibility for additions or other modern day appurtenances.
4. Permitted Uses. Property located within the HD, Historic District, may be used for only those purposes permitted within the zoning district in which such property is located, subject to compliance with all regulations imposed by such zoning district and subject to compliance with all provisions of the Article.



5. Historic District Commission

- (a) Creation. There is hereby created an Historic District Commission of the City of Norman, Oklahoma. The Commission shall be composed of nine members in accordance with the following requirements:
1. Five of the members shall be owners of property in existing historic districts. At least three of these five members shall also reside in historic districts.
  2. Two of the members shall be persons with specialized technical expertise in structural engineering, law, real estate, building construction, or similar fields.
  3. Two of the members shall be persons with specific professional backgrounds in areas such as history, architecture, planning, landscape architecture, archaeology, or related fields.
  4. Provided that all of the above criteria for membership composition are met, remaining Commission appointments may be filled by at-large Norman residents who have some demonstrated knowledge, experience, expertise or interest in historic preservation.
- (b) Duties of Historic District Commission. Unless otherwise specified in this article, the duties of the Historic District Commission shall be as follows:

\* \* \*

- (d) Meetings and Rules of Commission. The Commission shall be empowered to adopt rules for the conduct of its business. The Commission shall elect a Chairman who shall serve for one year or until his/her successor takes office, and who shall be eligible for reelection. All meetings of the Commission shall be open to the public. Any person, or his duly appointed representative, shall be entitled to appear and be heard on any matter before the Commission. The Commission shall keep a record of its proceedings, a copy of which shall be filed for public view in the office of the City Clerk.
- (e) Quorum. A quorum shall consist of five members.
- (f) Historic Preservation Officer. The Historic Preservation Officer is the City's representative to the Historic District Commission. He/she shall act in an advisory capacity only and may participate in the Commission's discussions but may not have a vote in any Commission decisions.

6. Historic District Designation.

- (a) Procedure for Designation of Historic District. Historic District designation is an overlay to the Norman Zoning Ordinance. Either the Norman City Council or individual property owners or their authorized agents may recommend tracts and sites for inclusion within an ~~HD~~, Historic District, in the same manner prescribed for the designation of other zoning districts by this Code and subject to compliance with this

section. Rezoning application fees in the case of Historic District designation shall be waived, though applicants for Historic District status are still responsible for all other associated costs of district designation.

\* \* \*

- (e) Notice of Consideration. Notice of consideration of a historic district designation by the Historic District Commission shall be the same as is required for consideration of the adoption or amendment of zoning district boundaries by the City Council. As a part of such notice, the Historic Preservation Officer shall notify the owners of record of affected properties by mail of the proposed designation and include a letter outlining the basis for the designation.

\* \* \*

7. Certificates of Appropriateness.

- (a) Certificate of Appropriateness (COA) Required. A Certificate of Appropriateness shall be required in the following instances before the commencement of work upon any structure or site located within a HD, Historic District:
  - 1. Whenever such work includes alteration to the exterior of any building, structure or site, including erection, moving, demolition, reconstruction, or restoration except when such work satisfies all the requirements for "ordinary maintenance and repair" as defined in section 2(u) of this Ordinance.
  - 2. Whenever such work requires a building permit issued by the City.
  - 3. Whenever such work includes the construction or enlargement of a driveway or parking area.
- (b) General Provisions and Procedures for Certificates of Appropriateness: No building permit shall be issued by the City of Norman for any structure or site located within the HD, Historic District, until the application for such permit has been reviewed by the Historic District Commission and a Certificate of Appropriateness approved by the Historic District Commission.
- (c) Submitting COA Application Materials. When applying for a Certificate of Appropriateness, the applicant shall furnish copies of all detailed site and building plans, elevations, perspectives, material samples, and specifications, with sufficient detail to clearly illustrate the applicant's intent. Applicants may meet with the Historic Preservation Officer before submitting an application and may also request a meeting with the Historic District Commission before submitting an application in order to get feedback from the Commission on a forthcoming application. Applicants may also consult with the Historic Preservation Officer as needed during the review of the COA)

- application. Incomplete applications will not be forwarded to the Commission for review.
- (d) **Historic District Commission Review.** Upon receipt of the application for a Certificate of Appropriateness, the Historic District Commission shall determine whether the proposed work is of a nature which will adversely affect any historical or architectural resource and whether such work is appropriate and consistent with the spirit and intent of this Ordinance and the Preservation Guidelines. The Historic District Commission shall apply the criteria established by this Ordinance and the Preservation Guidelines and based thereon shall approve or disapprove requests for Certificates of Appropriateness. If the Historic District Commission denies a Certificate of Appropriateness, no permit shall be issued and the applicant shall not proceed with the proposed work. Article 10 establishes the process for appealing decisions of the Historic District Commission.
  - (e) **Development of Preservation Guidelines.** The Historic District Commission shall develop such guidelines as it may find necessary to supplement the provisions of this Ordinance and to inform owners, residents, and the general public of those techniques which are considered most appropriate for undertaking work relating to historical and architectural resources. The Historic District Commission shall have the opportunity to advise the City Council concerning provisions in the building, electrical, plumbing, heat and air and housing codes and other codes which affect preservation work.
  - (f) **Infill Construction.** In the case of new or infill construction in Historic Districts, it is not the intent of this Ordinance to limit new construction to any one period or architectural style, but to preserve the overall integrity of Historic Districts and architectural resources and to ensure that new construction is compatible with existing historic and architectural resources.
  - (g) **In the case of denial of plans by the Historic District Commission,** the Commission shall state in writing the reasons for such denial and may include suggestions of the Commission in regard to actions the applicant might take to secure the approval of the Commission.
  - (h) **Archaeological Resources.** With regard to the development of a property containing a designated archeological resource, a Certificate of Appropriateness shall be required prior to the issuance of the permit for which the applicant has applied; and further, the following requirements shall be satisfied:

\* \* \*
  - (i) **Compliance with COA.** The Historic District Commission may approve Certificates of Appropriateness subject to certain conditions to be stated in writing. Work performed pursuant to the issuance of a Certificate of Appropriateness shall conform to the conditions of such certificate, if any. It shall be the duty of the Historic Preservation Officer of the City of Norman to inspect from time to time any work performed

pursuant to a Certificate of Appropriateness to assure such compliance. In the event that such work is not in compliance, the Historic Preservation Officer shall issue a stop work order. The Historic District Commission may request by resolution that the Historic Preservation Officer inspect work at a particular location and, if found to be non-compliant, issue a stop work order.

- (j) COA Application Requirements and Procedures, property owners, developers or agents applying for a Certificate of Appropriateness shall be required to submit the following as applicable:

\* \* \*

2. Required Procedures for Certificate of Appropriateness:

\* \* \*

- [b] Notification of Affected Property Owners: All recorded property owners immediately adjacent to or directly across the street or alley in any direction from the subject property shall be notified of an application for a Certificate of Appropriateness. This notice, as provided by the Historic Preservation Officer, shall contain adequate information to notify adjacent property owners of the specific request of the applicant for a Certificate of Appropriateness, as well as the time, date, and place of the meeting of the Historic District Commission at least seven (7) days before the hearing. In addition, the Historic Preservation Officer shall post a sign in the yard of the subject property at least seven (7) days before the hearing that shall include the time, date, and place of the meeting of the Historic District Commission. The sign shall be removed ten (10) days after the application is reviewed.

\* \* \*

- [h] Resubmitting of an Application: If the Historic District Commission determines that a Certificate of Appropriateness should not be issued, a new application may be submitted on the proposed construction, rehabilitation, reconstruction, alteration, restoration, or moving, only if substantive change is made to the original plans for the proposed work. In such a case, applicants will be required to submit a new application with all supporting documentation, including the payment of another application fee and a certified list of adjacent property owners. Reapplication fees may be waived when the Commission denies a request for COA due to incomplete application information.

\* \* \*

- [j] Time Limits of Certificate of Appropriateness: A Certificate of Appropriateness issued by the Historic District Commission shall become null and void if

construction, reconstruction, alteration, restoration, moving or demolition is not commenced within twelve (12) months of the date of issuance. An extension of time for the Certificate of Appropriateness, not to exceed six (6) months, may be granted by the Historic District Commission upon review, provided application for such extension is submitted in writing prior to expiration of the Certificate of Appropriateness.

3. Review Criteria. The Historic District Commission shall have responsibility for reviewing requests for Building and Demolition permits for designated historic structures within any area designated as a historic district, and for issuing or denying Certificates of Appropriateness for such requests. The purpose of this Section to specify for Historic District Commission members policies and criteria that they shall follow in reaching decisions on matters relative to such changes.

Review criteria, procedural policies and consequences of decisions will extend beyond the tenure of any Historic District Commission members. It is essential that policies be based on consistency and basic preservation guidelines. Highest priority should go to the preservation and restoration of historically and architecturally significant structures and sites that express the unique characteristics of the particular periods in which they were built.

- [a] Preservation Guidelines. In addition to the above-mentioned criteria, the Historic District Commission shall use specific Preservation Guidelines which shall be prepared by the Commission to outline and describe the evaluation criteria used in assessing the appropriateness of proposed project work within the designated Historic Districts. These Preservation Guidelines shall be prepared and periodically amended by Historic District Commission action as part of the regular duties of the Commission and shall be adopted by the Commission prior to their application in the review process.
- [b] Secretary of the Interior Standards. The Historic District Commission shall utilize those criteria in "Secretary of the Interior's Standards for Rehabilitation and Guidelines for Rehabilitating Historic Buildings (Revised 2017)." The Standards are as follows:

\* \* \*

## 8. Exceptions to Certificate of Appropriateness

A Certificate of Appropriateness shall not be required under the following conditions:

- (a) No exterior changes. No COA is required for any work affecting the exterior of a structure that does not alter the character of the exterior appearance of the resource; or for any work for which a building permit or any other City permit or certificate is not required for any work where the purpose of such work is stabilization and/or ordinary

maintenance and repair. The painting or repainting of any structure shall be considered ordinary maintenance and repair, regardless of color.

- (b) Meets All Requirements for Ordinary Maintenance and Repair. Any work not satisfying all of the requirements for ordinary maintenance and repair as defined in Section 2(u), shall not be considered ordinary maintenance and repair. The construction or enlargement of a driveway or parking area shall not be considered ordinary maintenance and repair.
- (c) Interior Changes Only. Any work affecting the interior of a structure for which a building permit or any other City permit or certificate is required but which does not alter the exterior appearance of the structure does not require a Certificate of Appropriateness.
- (d) Administrative Bypass for the Certificate of Appropriateness. A Certificate of Appropriateness may be granted by the Historic Preservation Officer or authorized designee as listed in the current edition of the Historic District Guidelines. The Historic Preservation Officer shall inform the Historic District Commission of Administrative Bypass actions at its next regular meeting. If a request for Administrative Bypass is denied by the Historic Preservation Officer, the applicant shall have the right to submit an application for a Certificate of Appropriateness to the Historic District Commission to be reviewed at its next regularly scheduled meeting time in order to request formal action regarding approval or denial of the Certificate of Appropriateness. All application fees and requirements shall apply.

9. Demolitions.

- (a) General Provisions. No structure or resource within any Historic District shall be demolished and/or removed unless such demolition has been reviewed by the Historic District Commission and a Certificate of Appropriateness for such demolition and/or removal has been granted.
- (b) Procedure and Postponement Orders
  - 1. The Historic District Commission shall hold a public hearing for the purpose of considering Certificates of Appropriateness for demolition or removal. After such hearing, the Historic District Commission may approve the Certificate of Appropriateness authorizing the demolition or may enter an order postponing demolition for up to ninety (90) days.
  - 2. At the conclusion of such period of postponement as specified in the Historic District Commission's order, the Commission shall within forty-five (45) days thereafter hold a second public hearing to consider whether or not to recommend to the City Council that additional postponement of demolition be ordered.



3. In the event that the Historic District Commission recommends additional postponement to the City Council, the City Council shall hold a public hearing for the purpose of considering additional postponement of demolition.
  4. After such public hearing, the City Council may enter an order approving the demolition or may enter an order postponing demolition for an additional period not to exceed sixty (60) days from the date of such order. At the conclusion of this final postponement period, the City Council shall hold a public hearing and may either approve the requested demolition or may disapprove such requested demolition. In the event demolition is not approved, no demolition shall occur. For purposes of this Ordinance, the word "demolition" shall include "removal."
- (c) Criteria for Review of Demolitions. The Historic District Commission and City Council shall be guided by the following criteria in considering Certificates of Appropriateness and authorizations for demolition or removal of structures or sites within the Historic District:
1. The purposes and intent of this Ordinance.
  2. The degree to which the proposed removal of the historical resource would damage or destroy the integrity and continuity of the Historic District of which it is a part.
  3. The nature of the resource as a representative type of style of architecture, a socioeconomic development, a historical association, or other element of the original designation criteria applicable to such structure or site.
  4. The condition of the resource from the standpoint of structural integrity and the extent of work necessary to stabilize the structure.
  5. The alternatives available to the demolition applicant, including:
    - [a] Donation of the subject structure or site to a public or benevolent agency.
    - [b] Donation of a part of the value of the subject structure or site to a public or benevolent agency, including the conveyance of historical easements.
    - [c] The possibility of sale of the structure or site, or any part thereof, to a prospective purchaser capable of preserving such structure or site.
    - [d] The potential of such structure or site for renovation and its potential for continuing same.
    - [e] The potential of the subject structure or site for rezoning in an effort to render such property more compatible with the physical potential of the structure.

6. The ability of the subject structure or site to produce a reasonable economic return on investment to its owner; provided however, that it is specifically intended that this factor shall not have exclusive control and effect, but shall be considered along with all other criteria contained in this Section.
10. Appeals.
  - (a) Any person aggrieved by a decision of the Historic District Commission, excluding postponements as defined in Section 8(b) (1-4), shall have such right of appeal to the Board of Adjustment within ten (10) days from the decision of the Historic District Commission.
  - (b) Aggrieved persons must exhaust all administrative processes before any appeal is valid.
11. Penalty.
  - (a) Any person, firm or corporation who violates any provision of this Ordinance shall, upon conviction, be punished by a fine as provided for in Section 440.3(a) of this chapter. A violation exists whenever there is a performance of an act which is prohibited by the provisions of this Ordinance, or a failure to perform an act which is required by this Ordinance. Each day this Ordinance is violated shall be considered a separate offense.
  - (b) In case any building or structure is erected, constructed, externally reconstructed, externally altered, added to or demolished in violation of this Ordinance, the City or any person may institute an appropriate action or proceeding in a court with competent jurisdiction to prevent such unlawful erection, construction, reconstruction, exterior alteration, addition or demolition, and the violating party shall pay all court costs and expenses, including reasonable attorney's fee, if the court should find in favor of the City or persons suing on behalf of the City to enforce this Ordinance.
12. City Council Approval of Revisions to Preservation Guidelines.
  - (a) Upon receiving or drafting a proposed revision of the Preservation Guidelines, the Historic District Commission shall submit said revisions to the City Council along with a recommendation for approval or disapproval. Said report shall outline efforts made to gather community input from residents of the Historic District as well as summarize such input.
  - (b) Upon City Council receiving said revisions, City Council shall have the duty to review proposed revisions and vote to either approve or disapprove the inclusion of the revisions in the Preservation Guidelines during the next available City Council meeting. The effective date of any approved revisions shall be thirty (30) days from the date the City Council vote on the proposed revisions is recorded.

\* \* \* \* \*

§ 2. SEVERABILITY. If any section, subsection, sentence, clause, phrase, or portion of this Ordinance is, for any reason, held invalid or unconstitutional, such portion shall be deemed a separate, distinct and independent provision and such holding shall not affect the validity of the remaining portions of this Ordinance.

ADOPTED this \_\_\_\_\_ day  
of \_\_\_\_\_, 2022.

NOT ADOPTED this \_\_\_\_\_ day  
of \_\_\_\_\_, 2022.

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Mayor

ATTEST:

\_\_\_\_\_  
City Clerk

## HISTORIC DISTRICT COMMISSION MINUTES OF October 4, 2021

The Historic District Commission of the City of Norman, Cleveland County, State of Oklahoma, met for the Regular Meeting on October 4, 2021, at 5:30 p.m. Notice and Agenda of the meeting were posted at 201 West Gray Building-A, the Norman Municipal Building and at [www.Normanok.gov](http://www.Normanok.gov) twenty-four hours prior to the beginning of the meeting.

Commissioner Emily Wilkins called the meeting to order at 5:32p.m.

### **Item No. 1, being: Roll Call.**

MEMBERS PRESENT:	Mitch Baroff Aaron Brooks Shavonne Evans *Left 6:30pm Tabor Halford Joan Koos Emily Wilkins Barrett Williamson *Left 6:30pm
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MEMBERS ABSENT:	Brent Swift Michael Zorba
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A quorum was present.

STAFF MEMBERS PRESENT:	Anaïs Starr, Planner II Tara Reynolds, Admin Tech III Jeanne Snider, Assistant City Attorney
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GUESTS:	Dave Boeck Fred Buxton
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### **Item No. 2, being: Approval of the Minutes from the September 7, 2021 regular meeting.**

**Motion** by Barrett Williamson for approval of the minutes from the September 7, 2021 regular meeting; **Second** by Joan Koos.

*The motion was passed unanimously.*

\*

**Motion** by Shavonne Evans to amend the order of the agenda to review item #5 first; **Second** by Joan Koos.

*The motion was passed unanimously with Commissioner Barrett Williamson abstaining.*

**Item No. 5, being: HD (21-15) Commission review and feedback regarding the proposed design for an addition, garage, pool, paving, fencing, and rear deck for the property located at 506 S. Lahoma.**

Commissioner Barrett Williamson recused himself to present this proposal. He presented the proposed project and asked the Commission for their feedback

Commission comments and discussion consisted of:

- Will there be any demolition? No, the previous addition will remain, but a part of the rear non-original addition will be removed.
- The addition with a bedroom and bathroom suite will be 683 square feet; there will be a travertine pool to connect to a swimming pool,
- A 572 square foot garage is proposed to be placed at the southwest corner of the rear yard adjacent to the pool. The garage will have porch that will act as a cabana for the pool.
- Applicants would like to extend the 8 ft. privacy fence from the side property line to the side of the house.
- Commissioner Mitch Baroff suggested flipping the addition to behind the house. This would prevent the addition from extending past the wall of the original structure. This would mean less pavement, less visible garage, less impervious surface.
- Commissioner Shavonne Evans noted the suite is large, and could be scaled down to prevent the addition from protruding past the original structure.
- Commissioner Joan Koos agrees with the previous statements. She would prefer to not see the garage, and the addition portion past the edge of the house is a concern. She would like to see the current driveway utilized. It is a good design, and well done, though she recognized that is not in accordance with Historic District guidelines. However, the driveway was installed prior to the establishment of the Chautauqua Historic District.
- Commissioner Aaron Brooks thought it was a thorough presentation and addressed issues well.
- Commissioner Tabor Halford asked staff about feedback from the past approval of a 535 square foot garage, which was never built. Staff indicated that the neighborhood did not want a garage on the south side of the property, and did not want to see it from the front. Commission wanted the garage placed at the end of the driveway on north side.
- The addition will extend 10 feet from an existing “bump out” addition on the side of the house, and 15 feet from the original wall of the structure.

- The addition would increase the footprint of the house by approximately 25%.
- Chair Emily Wilkins agrees with the previous comments by Commissioners, and adds that this is a significantly sized addition. The main concern being how much the addition protrudes past the sides of the primary structure, and would like to see the addition behind the house if possible. Historic Guideline 4.2.2 states to “locate additions inconspicuously” and 4.2.3 states to “limit size and scale.” The width shouldn’t exceed the width of the house.

**Item No. 3, being: HD (21-09) Consideration of a Certificate of Appropriateness request for the re-installation of a pair of wood windows on the south side of the structure for property located at 549 S. Lahoma Avenue.**

**Motion** by Aaron Brooks to approve item as submitted #3; **Second** by Tabor Halford.

Anais Starr presented the staff report. In 2011 a COA was denied for replacement of three historic windows with inappropriate non-wood windows. The applicant appealed this decision to the City Council and then through a civil court appeal process. The ligation is now concluded and the applicant wishes to re-install a pair of one-over-one wood windows where there is currently one picture window. Ms. Starr presented a staff report regarding the request and pointed out this COA request was only concerned with the south picture window.

The applicant’s representative, Fred Buxton, discussed the reasons for the project:

- The applicant can now find historic windows to replace the windows and would like to have all historical windows. The picture window would be a pair of double hung one-over-one wood windows, which meet the Guidelines.

No public comments were made.

Commission comments and discussion consisted of:

- The Commission was in agreement that this request meets Historic District Guidelines.

***There being no further discussion, a vote on the motion was taken with the following result:***

**YEAS** Mitch Baroff  
Aaron Brooks  
Tabor Halford  
Joan Koos  
Emily Wilkins

**NAYS** None

Ms. Starr noted that there is a 10-day waiting period until the COA will be issued.



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**Item No. 4, being: HD (21-17) Consideration of a Certificate of Appropriateness request for a garage with associated driveway, and for the replacement of rear porch with a deck for property located at 1320 Classen Boulevard.**

**Motion** by Aaron Brooks to approve request as submitted. **Second** by Joan Koos.

Anais Starr presented the staff report.

The applicant's representative, Dave Boeck, discussed the reasons for the project:

- The site plan reviewed in March had a front building line drawn and listed incorrectly at 25' instead of the actual 40' building line that exist. This error meant that the Commission must re-review the COA request for the garage, driveway, and deck with a corrected site plan.

No public comments were made.

Commission comments and discussion consisted of:

- The Commission had approved the same 718 square-foot garage and associated driveway at March meeting earlier this year.
- The proposed deck is 20 feet wide by 19 feet deep, which is too large for an administrative bypass.
- The footprint of the garage is 75% of the primary structure's footprint, and does not meet guidelines.
- The size of the garage at 704 square feet is too large; should be 500-600 square feet, and the garage is now closer to the deck because the site plan has been corrected. There will be 11 feet between the deck and the garage.

**Motion** by Joan Koos to amend the original motion to allow voting of items separately for item #4; **Second** by Aaron Brooks.

*The motion was approved unanimously.*

**Motion** by Aaron Brooks to approve the replacement of rear porch with a deck; **Second** by Tabor Halford.

The motion was approved unanimously with the following vote:

**YEAS** Mitch Baroff  
Aaron Brooks  
Tabor Halford  
Joan Koos  
Emily Wilkins

**NAYS** None

**Motion** by Aaron Brooks to approve the garage as submitted; **Second** by Mitch Baroff. The voting went as follows:

Mitch Baroff: No  
Aaron Brooks: Yes  
Tabor Halford: No  
Joan Koos: No  
Emily Wilkins: No

The motion for approval of the garage as submitted failed 4-1.

**Motion** by Aaron Brooks to rescind original motion for the garage as submitted; **Second** by Mitch Baroff.

**YEAS** Mitch Baroff  
Aaron Brooks  
Tabor Halford  
Joan Koos  
Emily Wilkins

**NAYS** None

**Motion** by Aaron Brooks to approve the amended request for a garage of 575 square feet; **Second** by Joan Koos.

*The motion was approved unanimously with the following vote:*

**YEAS** Mitch Baroff  
Aaron Brooks  
Tabor Halford  
Joan Koos  
Emily Wilkins

**NAYS** None

**Motion** by Aaron Brooks to approve the associated driveway as submitted; **Second** by Joan Koos.

*The motion was approved unanimously with the following vote:*

**YEAS** Mitch Baroff  
Aaron Brooks  
Tabor Halford  
Joan Koos

Emily Wilkins

**NAYS** None

**Motion** by Aaron Brooks to approve the deck as submitted; Second by Joan Koos.

**The motion was approved unanimously with the following vote:**

**YEAS** *Mitch Baroff*  
*Aaron Brooks*  
*Tabor Halford*  
*Joan Koos*  
*Emily Wilkins*

**NAYS** None

Ms. Starr noted that there is a 10-day waiting period until the COA will be issued.

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**Item No. 6, being: Staff report on active Certificates of Appropriateness and Administrative Bypass issued since September 7, 2021 and consideration of six-month extension requests for expiring COAs.**

- 904 Miller – Violation notice sent to property owner, who contacted staff and is weighing her options.
- 518 Chautauqua – COA issued 6/1/20, building permit issued August 2020, work has begun as of August 2021 and continues.
- 536 Chautauqua – COA issued 8/3/20. Work on garage and driveway is complete, and the fence is waiting to start.
- 1320 Classen – COA issued 3/1/21. Windows replaced correctly. Unapproved work on soffit was stopped by HPO, that work is in the middle of being replaced with original design. Rear porch was not built to the approved COA. Driveway and garage laid out incorrectly. Submitted site plan was incorrect. Stop work order issued after columns started to be painted. (COA to amend request submitted) Windows replaced correctly. Unapproved work on soffit was stopped by HPO, that work is in the middle of being replaced with original design. Rear porch was not built to the approved COA. Driveway and garage laid out incorrectly. Submitted site plan incorrect. Stop work order issued after columns started to be painted. COA reheard at tonight's meeting.
- 620 Miller – COA issued 3/1/21. Work has not started on the shutters.
- 605 Okmulgee – COA issued 4/5/21. Construction almost complete.
- 519 S Lahoma – Construction underway.

6 month extension requests – None.

Administrative bypass requests – 549 S Lahoma: Replacement of 10 deteriorated historic and non-historic windows with wood one-over-one wood sashes.

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**Item No. 7, being: Discussion of progress report regarding the FY 2021-2022 CLG Projects.**

2021-2022 Certified Local Government Fund

\$ 150 National Alliance of Preservation Conference (NAPC) Dues

\$7,000 Commission Assistance and Mentoring Program (C.A.M.P)

Training for Commissioners

\$2,500 Planning Conference attendance for staff

\$ 600 Education Mailing

\$10,750 CLG Total allocation for 2021-2022

The Commission will have to watch the NAPC C.A.M.P. training together in person, and they chose the dates of November 9 and November 18 from 8:30am – 12:30pm.

Special meeting November 1 at 4pm for a SHPO training over the Secretary of the Interior Standards. \*

**Item No. 7, being: Consideration and recommendation to the City Council of the Draft Historic District Ordinance.**

The ordinance will go before Planning Commission for approval in November, and City Council in December.

Discussion about clarification of addition guidelines. The guidelines might be brought back at the next meeting for discussion.

**Motion** by Aaron Brooks to recommend the draft Historic District Ordinance to City Council for approval: **Second** by Joan Koos.

***There being no further discussion, a vote on the motion was taken with the following result:***

**YEAS** Mitch Baroff  
Aaron Brooks  
Tabor Halford  
Joan Koos  
Emily Wilkins

**NAYS** None

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**Item No. 9, being: Historic District Commission Meeting Calendar for 2022.**

The Commission would like to relocate the regularly scheduled meetings in Conference room D of Building A in 2022.

**Item No. 10, being: Miscellaneous comments of the Historic District Commission and city staff.**

None.

**Item No. 11, being: Adjournment.**

The meeting adjourned at 7:47 p.m.

Passed and approved this \_\_\_\_\_ day of \_\_\_\_\_, 2021.

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Emily Wilkins, Chair  
Historic District Commission

AN ORDINANCE OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA AMENDING CHAPTER 22 (ZONING ORDINANCE), ARTICLE XI, SPECIFIC DISTRICT REGULATIONS, SECTION 429.3 IN DEFINITIONS DELETING CONSERVATION AND ADDING ORIGINAL, PERIOD OF SIGNIFICANCE, RELOCATION, SECRETARY OF THE INTERIOR STANDARDS OF HISTORIC BUILDINGS; EDITS FOR CONSISTENCY OR CORRECTIVE PURPOSES; ADDING WRITTEN DENIAL REQUIREMENT; ADDING SEVEN DAY NOTICE; EXTENDED TIME LIMIT OF COA FROM SIX MONTHS TO TWELVE MONTHS; RESTRUCTURING EXCEPTIONS TO ADMINISTRATIVE BYPASS; AND PROVIDING FOR THE SEVERABILITY THEREOF.

NOW THEREFORE, BE IT ORDAINED BY THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA:

\* \* \* \* \*

§ 1. That, Section 429.3 of Chapter 22 of the Zoning Ordinance of the City of Norman, Oklahoma be amended as follows:

\* \* \* \* \*

#### SEC. 429.3 - HD, HISTORIC DISTRICT

1. Description and Purpose. The Historic District Ordinance, hereinafter referred to as the "HDO", and its regulations may be applied to property located in any zoning district in accordance with the provisions of this Ordinance. The HDO is intended to be an overlay zoning district and the regulations imposed by such district shall be in addition to the regulations of the underlying zoning district applicable to the subject parcel.

The City of Norman hereby declares that the historical, architectural, cultural, and aesthetic features of the City represent some of the finest and most valuable resources of the City, and such resources are the embodiment of the heritage of the people of the City of Norman. Therefore, it is hereby declared that the purposes of this Ordinance, to be known as the Historic District Ordinance, shall be as follows:

\* \* \*

(g) To safeguard the heritage of the City by preserving and regulating historic district structures in such a way that maintains or restores their historic integrity while allowing modern day uses and conveniences for their residents.

2. Definitions. As used in this chapter, unless the context otherwise requires, the following words or phrases have the meaning listed:

(a) Addition – construction that increases the size of the original structure by building outside of the existing structure. Additions can be either horizontal or vertical.

- (b) Alteration – an act that changes one or more of the exterior architectural features of a structure or its appurtenances, including but not limited to the erection, construction, reconstruction, or removal of any structure or appurtenance.
- (c) Appropriate – typical of the historic architectural style, compatible with the character of the historic district, and consistent with the Preservation Guidelines of the City of Norman.
- (d) Architectural Resources - districts, structures, buildings, monuments, sites, or landscaping which possess local interest or artistic merit or which are particularly representative of their class or period, or represent achievements in architecture, engineering, or design.
- (e) Certificate of Appropriateness (COA)\_- the official document issued by the Historic District Commission approving any application affecting the exterior of any structure designated by the authority of this Ordinance for permission to construct, erect, demolish, remove, relocate, reconstruct, restore, or alter said structure.
- (f) Commission – The Historic District Commission of the City of Norman.
- (g) Compatible – means a design or use that does not conflict with the historical appearance of a building or district and does not require irreversible alteration.
- (h) ~~Conservation—the sustained use and appearance of a resource essentially in its existing state.~~
- (hi) Contributing Resource – means a resource—a building, site, or district—that retains its essential architectural integrity in design and whose architectural style is typical of or integral to a historic district.
- (ij) Elevation – an exterior wall of a structure.)
  1. *Front elevation* – the façade or face of a structure which is visible and prominent from a public right-of-way and which often has distinguishing architectural features. Structures on corner lots shall be considered to have two front elevations. No structure shall be considered to have more than two front elevations.
  2. *Side elevation* – a wall adjacent to the front elevation that is usually visible from a public right-of-way.
  3. *Rear elevation* – an elevation parallel to the front façade; the rear elevation usually includes the back door of the structure.
  4. *Primary elevation* – the front or side elevation of a structure.



5. *Secondary elevation* – the rear elevation of a structure.

- (j~~k~~) Façade – the front wall or face of a building.
- (k~~l~~) Historic District - a geographically definable area with a concentration or linkage of significant sites, buildings, structures, or monuments; or, an individual structure, building, site or monument which contributes to the cultural, social, political, or architectural heritage of the City of Norman.
- (l~~m~~) Historic Preservation Officer – the chief staff person responsible for historic preservation in the City of Norman’s Planning and Community Development Department.
- (m~~n~~) Historic Property - ~~the term shall mean~~ any individual structure, building, site or monument which contributes to the historic, architectural, archeological and/or cultural heritage of the City of Norman, Oklahoma as determined by the Historic District Commission.
- (n~~o~~) Historic Resources - sites, districts, structures, buildings, monuments, major landscape features that represent facets of history in the locality, state or nation; places where significant historical or unusual events occurred; places associated with a personality or group important to the past.
- (o~~p~~) Infill construction – construction on property between or adjacent to existing buildings.
- (p~~q~~) In kind – to replace existing materials or features with materials of identical ~~appearance~~ design, size, texture and/or composition. (see also: matching)
- (q~~r~~) Landmark – an individual structure, building, site, or monument that contributes to the historical, architectural, or archaeological heritage of the city.
- (r~~s~~) Matching – In historic rehabilitations, the use of replacement materials that are identical to the original in composition, size, shape, and profile. (see also: in kind)
- (s~~t~~) National Register of Historic Places - ~~the term shall mean~~ the national list of districts, sites, buildings, structures, and objects significant in American history, architecture, archeology, engineering and culture, maintained by the Secretary of the Interior under authority of Section 101(a)(1)(A) of the National Historic Preservation Act, as amended.
- (t~~u~~) Non-Contributing Resource – A building, structure, or site that does not add to the historic significance of a property or district, and which detracts from the visual integrity or interpretability of an historic district.

(u~~v~~) Ordinary Maintenance and Repair – Work meant to remedy damage or deterioration of a structure or its appurtenances, and which will involve no change in materials, dimensions, design, configuration, ~~color~~, texture or visual appearance to the exterior of an historic structure. Ordinary maintenance and repair shall include, but is not limited to, painting and reroofing.

(v) Original – buildings, building materials or features that were present during the period of significance for the historic district.

(w) Period of Significance – the span of time during which a group of properties attained the significance that makes them eligible for designation as a historic district.

(x~~w~~) Preservation – ~~shall mean~~ the adaptive use, conservation, protection, reconstruction, rehabilitation, or stabilization of buildings, districts, monuments, sites, or structures significant to the heritage of the people of Norman. The following terms further define types of preservation activities:

1. *Adaptive Use* – the restrained alteration of a historical or architectural resource to accommodate uses for which the resource was not originally constructed, but in such a way so as to maintain the historical and architectural character of the resource.
2. *Conservation* – the sustained use and appearance of a resource essentially in its existing state.
3. *Historic Rehabilitation* – the act or process of making a compatible use for a property through repair, alterations, and additions while preserving those portions or features which convey its historic, cultural or architectural values.
4. *Historic Reconstruction* – the act or process of duplicating the original structure, building form and materials by means of new construction based on documentation of the historic condition.
5. *Protection* – the security of a resource as it exists through the establishment of the mechanisms of this section.
6. *Restoration* – the process of accurately recovering all or a part of the form and details of a resource and its setting as it appeared at a particular period by means of the removal of later work and the replacement of missing earlier work.
7. *Stabilization* – the process of applying measures designated to halt deterioration and to establish the structural stability of an unsafe or deteriorated resource while maintaining the essential form as it presently exists without changing the exterior appearance of the resource.

(y) Relocation – the movement or repositioning of a primary or accessory structure on its original site, or from one location to another.

(z) Secretary of the Interior Standards of Historic Buildings - A set of principles established in 1977 and amended periodically thereafter, by the Secretary of the Interior, who is responsible for all national preservation programs under Department of the Interior authority and for advising federal agencies on the preservation of historic properties listed or eligible for listing in the National Register of Historic Places. The Norman Historic District Commission utilizes the Secretary of Interior Standards as basis for developing the City of Norman Preservation Guidelines as well as for design review.

(aa~~x~~) Significant Characteristics -those characteristics that are important to or expressive of the historic or architectural quality and integrity of the resources and its setting and which include, but are not limited to building mass, building material, detail, height, proportion, rhythm, scale, setback, setting, shape, street accessories, and workmanship.

1. *Building Mass* – describes the relationship of a building’s height to its width and depth.
2. *Building Materials* - the physical characteristics which create the aesthetic and structural appearance of the resource, including but not limited to a consideration of the texture and style of the components and their combinations, such as brick, stone, shingle, wood, concrete, or stucco.
3. *Detail* - architectural aspects which, due to particular treatment, draw attention to certain parts or features of a structure.
4. *Height* - the vertical dimension of a given structure, building or monument.
5. *Proportion* - the relative physical sizes within and between buildings and building components.
6. *Rhythm* - a discernible pattern of shapes including, but not limited to, windows, doors, projections, and heights, within a building, structure or monument, or a group of same.
7. *Setback* - the distance that a structure sets from the property line, typically the front or side property line.
8. *Scale* - the proportion of parts of a building, structure, or monument to one another and to the human figure.
9. *Setting* - the surrounding structures, monuments, and landscaping which establish the visual, aesthetic, or auditory qualities of the historic or architectural resources.

10. *Shape* - the physical configuration of structures or landscaping and their component parts.
11. *Street Accessories* - those sidewalk or street fixtures which include, but are not limited to, trash receptacles, benches, signs, lights, hydrants, and landscaping.
- (~~bb~~y) State Historic Preservation Officer (SHPO) - ~~the term shall mean~~ the official within the State of Oklahoma who has been delegated and appointed by the Governor to administer the Historic Preservation Program in the State.
- (~~cc~~z) State Register of Historic Places - ~~the term shall mean~~ the State of Oklahoma list of districts, sites, buildings, structures and objects significant in state history, architecture, archeology, engineering and culture, maintained by the State Historic Preservation Officer, under the authority of 53 O.S., 1984 Supplement, Sections 351-355.
- (~~dd~~aa) Streetscape - the view along a street from the perspective of a driver or pedestrian, of the natural and man-made elements in or near the street right of way, including buildings and their relationship to street trees, lawns, landscape buffers, signs, street lights, above-ground utilities, drainage structures, sidewalks, bus stop shelters and street furniture.
- (~~ee~~bb) Structure - anything constructed or erected, the use of which requires permanent location on the ground or which is attached to something having a permanent location on the ground. These include, but are not limited to, buildings, fences, walls, driveways, sidewalks and parking areas.
3. District Regulations. The following regulations shall be applicable to the HD, Historic District, and shall control the use of all properties within such district:
- (a) Any person responsible for a structure, building, landmark, or monument within a Historic District shall keep all of the exterior portions of such resources in good repair.
  - (b) The erection, moving, demolition, removal, rehabilitation, reconstruction, restoration, or alteration of the exterior of any structure is prohibited unless a Certificate of Appropriateness (COA) is granted by the Historic Commission of the City of Norman, unless such Certificate is not required by Subsection 8.
  - (c) Changes to rear elevations do require a COA; however the rear elevation of a historic structure is considered a secondary elevation and is therefore regulated to a lower standard to allow flexibility for additions or other modern day appurtenances.
4. Permitted Uses. Property located within the HD, Historic District, may be used for only those purposes permitted within the zoning district in which such property is located, subject to compliance with all regulations imposed by such zoning district and subject to compliance with all provisions of the Article.

## 5. Historic District Commission

- (a) Creation. There is hereby created an Historic District Commission of the City of Norman, Oklahoma. The Commission shall be composed of nine members in accordance with the following requirements:
1. Five of the members shall be owners of property in existing historic districts. At least three of these five members shall also reside in historic districts.
  2. Two of the members shall be persons with specialized technical expertise in structural engineering, law, real estate, building construction, or similar fields.
  3. Two of the members shall be persons with specific professional backgrounds in areas such as history, architecture, planning, landscape architecture, archaeology, or related fields.
  4. Provided that all of the above criteria for membership composition are met, remaining Commission appointments may be filled by at-large Norman residents who have some demonstrated knowledge, experience, expertise or interest in historic preservation.
- (b) Duties of Historic District Commission. Unless otherwise specified in this article, the duties of the Historic District Commission shall be as follows:

\* \* \*

- (d) Meetings and Rules of Commission. The Commission shall be empowered to adopt rules for the conduct of its business. The Commission shall elect a Chairman who shall serve for one year or until his/her success takes office, and who shall be eligible for reelection. All meetings of the Commission shall be open to the public. Any person, or his duly appointed representative, shall be entitled to appear and be heard on any matter before the Commission. The Commission shall keep a record of its proceedings, a copy of which shall be filed for public view in the office of the City Clerk.
- (e) Quorum. A quorum shall consist of five members.
- (f) Historic Preservation Officer. The Historic Preservation Officer is the City's representative to the Historic District Commission. He/she shall act in an advisory capacity only and may participate in the Commission's discussions but may not have a vote in any Commission decisions.

## 6. Historic District Designation.

- (a) Procedure for Designation of Historic District. Historic District designation is an overlay to the Norman Zoning Ordinance. Either the Norman City Council or individual

property owners or their authorized agents may recommend tracts and sites for inclusion within an ~~HD~~, Historic District, in the same manner prescribed for the designation of other zoning districts by this Code and subject to compliance with this section. Rezoning application fees in the case of Historic District designation shall be waived, though applicants for Historic District status are still responsible for all other associated costs of district designation.

\* \* \*

- (e) Notice of Consideration. Notice of consideration of a historic district designation by the Historic District Commission shall be the same as is required for consideration of the adoption or amendment of zoning district boundaries by the City Council. As a part of such notice, the Historic Preservation Officer shall notify the owners of record of affected properties by mail of the proposed designation and include a letter outlining the basis for the designation.

\* \* \*

## 7. Certificates of Appropriateness.

- (a) Certificate of Appropriateness (COA) Required. A Certificate of Appropriateness shall be required in the following instances before the commencement of work upon any structure or site located within a ~~HD~~, Historic District:
1. Whenever such work includes alteration to the exterior of any building, structure or site, including erection, moving, demolition, reconstruction, or restoration except when such work satisfies all the requirements for "ordinary maintenance and repair" as defined in section 2(u) ~~(t)~~ of this Ordinance.
  2. Whenever such work requires a building permit issued by the City.
  3. Whenever such work includes the construction or enlargement of a driveway or parking area.
- (b) General Provisions and Procedures for Certificates of Appropriateness: No building permit shall be issued by the City of Norman for any structure or site located within the HD, Historic District, until the application for such permit has been reviewed by the Historic District Commission and a Certificate of Appropriateness approved by the Historic District Commission.
- (c) Submitting COA Application Materials. When applying for a Certificate of Appropriateness, the applicant shall furnish copies of all detailed site and building plans, elevations, perspectives, material samples, and specifications, with sufficient detail to clearly illustrate the applicant's intent. Applicants may ~~are encouraged to~~ meet with the Historic Preservation Officer before submitting an application and may also request a meeting with the Historic District Commission before submitting an

application in order to get feedback from the Commission on a forthcoming application. Applicants may also consult with the Historic Preservation Officer as needed during the review of the ~~Certificate of Appropriateness (COA)~~ application. Incomplete applications will not be forwarded to the Commission for review.

- (d) Historic District Commission Review. Upon receipt of the application for a Certificate of Appropriateness, the Historic District Commission shall determine whether the proposed work is of a nature which will adversely affect any historical or architectural resource and whether such work is appropriate and consistent with the spirit and intent of this Ordinance and the Preservation Guidelines. The Historic District Commission shall apply the criteria established by this Ordinance and the Preservation Guidelines and based thereon shall approve or disapprove requests for Certificates of Appropriateness. If the Historic District Commission denies a Certificate of Appropriateness, no permit shall be issued and the applicant shall not proceed with the proposed work. Article 10 establishes the process for appealing decisions of the Historic District Commission.
- (e) Development of Preservation Guidelines. The Historic District Commission shall develop such guidelines as it may find necessary to supplement the provisions of this Ordinance and to inform owners, residents, and the general public of those techniques which are considered most appropriate for undertaking work relating to historical and architectural resources. The Historic District Commission shall have the opportunity to advise the City Council concerning provisions in the building, electrical, plumbing, heat and air and housing codes and other codes which affect preservation work.
- (f) Infill Construction. In the case of new or infill construction in Historic Districts, it is not the intent of this Ordinance to limit new construction to any one period or architectural style, but to preserve the overall integrity of Historic Districts and architectural resources and to ensure that new construction is compatible with existing historic and architectural resources.
- (g) In the case of denial of plans by the Historic District Commission, the Commission shall state in writing the reasons for such denial and may include suggestions of the Commission in regard to actions the applicant might take to secure the approval of the Commission.
- (~~h~~g) Archaeological Resources. With regard to the development of a property containing a designated archeological resource, a Certificate of Appropriateness shall be required prior to the issuance of the permit for which the applicant has applied; and further, the following requirements shall be satisfied:
 

\* \* \*
- (~~ih~~) Compliance with COA. The Historic District Commission may approve Certificates of Appropriateness subject to certain conditions to be stated in writing. Work performed pursuant to the issuance of a Certificate of Appropriateness shall conform to the



conditions of such certificate, if any. It shall be the duty of the Historic Preservation Officer of the City of Norman to inspect from time to time any work performed pursuant to a Certificate of Appropriateness to assure such compliance. In the event that such work is not in compliance, the Historic Preservation Officer shall issue a stop work order. The Historic District Commission may request by resolution that the Historic Preservation Officer inspect work at a particular location and, if found to be non-compliant, issue a stop work order.

- (j) COA Application Requirements and Procedures, property owners, developers or agents applying for a Certificate of Appropriateness shall be required to submit the following as applicable:

\* \* \*

## 2. Required Procedures for Certificate of Appropriateness:

\* \* \*

- [b] Notification of Affected Property Owners: All recorded property owners immediately adjacent to or directly across the street or alley in any direction from the subject property shall be notified of an application for a Certificate of Appropriateness. This notice, as provided by the Historic Preservation Officer, shall contain adequate information to notify adjacent property owners of the specific request of the applicant for a Certificate of Appropriateness, as well as the time, date, and place of the meeting of the Historic District Commission at least seven (7) days before the hearing. In addition, the Historic Preservation Officer shall post a sign in the yard of the subject property at least seven (7) days before the hearing that shall include ~~the specific request of the applicant for a Certificate of Appropriateness, as well as~~ the time, date, and place of the meeting of the Historic District Commission. The sign shall be removed ten (10) days after the application is reviewed.

\* \* \*

- [h] Resubmitting of an Application: If the Historic District Commission determines that a Certificate of Appropriateness should not be issued, a new application may be submitted on the proposed construction, rehabilitation, reconstruction, alteration, restoration, or moving, only if substantive change is made to the original plans for the proposed work. In such a case, applicants will be required to submit a new application with all supporting documentation, including the payment of another \$75-application fee and a certified list of adjacent property owners. Reapplication fees may be waived when the Commission denies a request for COA due to incomplete application information.

\* \* \*

[j] Time Limits of Certificate of Appropriateness: A Certificate of Appropriateness issued by the Historic District Commission shall become null and void if construction, reconstruction, alteration, restoration, moving or demolition is not commenced within twelve (12) ~~six (6)~~ months of the date of issuance. An extension of time for the Certificate of Appropriateness, not to exceed six (6) months, may be granted by the Historic District Commission upon review, provided application for such extension is submitted in writing prior to expiration of the Certificate of Appropriateness.

3. Review Criteria. The Historic District Commission shall have responsibility for reviewing requests for Building and Demolition permits for designated historic structures within any area designated as a historic district, and for issuing or denying Certificates of Appropriateness for such requests. The purpose of this Section to specify for Historic District Commission members policies and criteria that they shall follow in reaching decisions on matters relative to such changes.

Review criteria, procedural policies and consequences of decisions will extend beyond the tenure of any Historic District Commission members. It is essential that policies be based on consistency and basic preservation guidelines. Highest priority should go to the preservation and restoration of historically and architecturally significant structures and sites that express the unique characteristics of the particular periods in which they were built.

[a] Preservation Guidelines. In addition to the above-mentioned criteria, the Historic District Commission shall use specific Preservation Guidelines which shall be prepared by the Commission to outline and describe the evaluation criteria used in assessing the appropriateness of proposed project work within the designated Historic Districts. These Preservation Guidelines shall be prepared and periodically amended by Historic District Commission action as part of the regular duties of the Commission and shall be adopted by the Commission prior to their application in the review process.

[b] Secretary of the Interior Standards. The Historic District Commission shall utilize those criteria in "Secretary of the Interior's Standards for Rehabilitation and Guidelines for Rehabilitating Historic Buildings (Revised 2017 ~~1983~~)."  
The Standards are as follows:

\* \* \*

## 89. Exceptions to Certificate of Appropriateness

A Certificate of Appropriateness shall not be required under the following conditions:

## Annotated

- (a) No exterior changes. No COA is required for any work affecting the exterior of a structure that does not alter the character of the exterior appearance of the resource; or for any work for which a building permit or any other City permit or certificate is not required for any work where the purpose of such work is stabilization and/or ordinary maintenance and repair. The painting or repainting of any structure shall be considered ordinary maintenance and repair, regardless of color.
- (b) Meets All Requirements for Ordinary Maintenance and Repair. ~~See Section 2t for definition of ordinary maintenance and repair.~~ Any work not satisfying all of the requirements for ordinary maintenance and repair as defined in Section 2(ut). shall not be considered ordinary maintenance and repair. The construction or enlargement of a driveway or parking area shall not be considered ordinary maintenance and repair.
- (c) Interior Changes Only. Any work affecting the interior of a structure for which a building permit or any other City permit or certificate is required but which does not alter the exterior appearance of the structure does not require a Certificate of Appropriateness.
- (d) Administrative Bypass for the Certificate of Appropriateness. A Certificate of Appropriateness may be granted by the Historic Preservation Officer or authorized designee ~~for the following: as listed in the current edition of the Historic District Guidelines.~~ The Historic Preservation Officer shall inform the Historic District Commission of Administrative Bypass actions at its next regular meeting. If a request for Administrative Bypass is denied by the Historic Preservation Officer, the applicant shall have the right to submit an application for a Certificate of Appropriateness to the Historic District Commission to be reviewed at its next regularly scheduled meeting time in order to request formal action regarding approval or denial of the Certificate of Appropriateness. All application fees and requirements shall apply.

~~1. Installation of storm windows or storm doors~~

~~2. Roofing or reroofing of any structure with materials that are very similar in appearance and composition, regardless of color, provided the building is not structurally altered during the roofing or reroofing process.~~

~~3. The Historic Preservation Officer shall inform the Historic District Commission of Administrative Bypass actions at its next regular meeting. If a request for Administrative Bypass is denied by the Historic Preservation Officer, the applicant shall have the right to submit an application for a Certificate of Appropriateness to the Historic District Commission to be reviewed at its next regularly scheduled meeting time in order to request formal action regarding approval or denial of the Certificate of Appropriateness. All application fees and requirements shall apply.~~

89. Demolitions.

- (a) General Provisions. No structure or resource within any Historic District shall be demolished and/or removed unless such demolition has been reviewed by the Historic District Commission and a Certificate of Appropriateness for such demolition and/or removal has been granted.
- (b) Procedure and Postponement Orders
1. The Historic District Commission shall hold a public hearing for the purpose of considering Certificates of Appropriateness for demolition or removal. After such hearing, the Historic District Commission may approve the Certificate of Appropriateness authorizing the demolition or may enter an order postponing demolition for up to ninety (90) days.
  2. At the conclusion of such period of postponement as specified in the Historic District Commission's order, the Commission shall within forty-five (45) days thereafter hold a second public hearing to consider whether or not to recommend to the City Council that additional postponement of demolition be ordered.
  3. In the event that the Historic District Commission recommends additional postponement to the City Council, the City Council shall hold a public hearing for the purpose of considering additional postponement of demolition.
  4. After such public hearing, the City Council may enter an order approving the demolition or may enter an order postponing demolition for an additional period not to exceed sixty (60) days from the date of such order. At the conclusion of this final postponement period, the City Council shall hold a public hearing and may either approve the requested demolition or may disapprove such requested demolition. In the event demolition is not approved, no demolition shall occur. For purposes of this Ordinance, the word "demolition" shall include "removal."
- (c) Criteria for Review of Demolitions. The Historic District Commission and City Council shall be guided by the following criteria in considering Certificates of Appropriateness and authorizations for demolition or removal of structures or sites within the Historic District:
1. The purposes and intent of this Ordinance.
  2. The degree to which the proposed removal of the historical resource would damage or destroy the integrity and continuity of the Historic District of which it is a part.
  3. The nature of the resource as a representative type of style of architecture, a socioeconomic development, a historical association, or other element of the original designation criteria applicable to such structure or site.
  4. The condition of the resource from the standpoint of structural integrity and the extent of work necessary to stabilize the structure.

5. The alternatives available to the demolition applicant, including:
    - [a] Donation of the subject structure or site to a public or benevolent agency.
    - [b] Donation of a part of the value of the subject structure or site to a public or benevolent agency, including the conveyance of historical easements.
    - [c] The possibility of sale of the structure or site, or any part thereof, to a prospective purchaser capable of preserving such structure or site.
    - [d] The potential of such structure or site for renovation and its potential for continuing same.
    - [e] The potential of the subject structure or site for rezoning in an effort to render such property more compatible with the physical potential of the structure.
  6. The ability of the subject structure or site to produce a reasonable economic return on investment to its owner; provided however, that it is specifically intended that this factor shall not have exclusive control and effect, but shall be considered along with all other criteria contained in this Section
10. Appeals.
- (a) Any person aggrieved by a decision of the Historic District Commission, excluding postponements as defined in Section 8(b) (1-4), shall have such right of appeal to the Board of Adjustment within ten (10) days from the decision of the Historic District Commission.
  - (b) Aggrieved persons must exhaust all administrative processes before any appeal is valid.
11. Penalty.
- (a) Any person, firm or corporation who violates any provision of this Ordinance shall, upon conviction, be punished by a fine as provided for in Section 440.3(a) of this chapter. A violation exists whenever there is a performance of an act which is prohibited by the provisions of this Ordinance, or a failure to perform an act which is required by this Ordinance. Each day this Ordinance is violated shall be considered a separate offense.
  - (b) In case any building or structure is erected, constructed, externally reconstructed, externally altered, added to or demolished in violation of this Ordinance, the City or any person may institute an appropriate action or proceeding in a court with competent jurisdiction to prevent such unlawful erection, construction, reconstruction, exterior alteration, addition or demolition, and the violating party shall pay all court costs and

expenses, including reasonable attorney's fee, if the court should find in favor of the City or persons suing on behalf of the City to enforce this Ordinance.

12. City Council Approval of Revisions to Preservation Guidelines.)

- (a) Upon receiving or drafting a proposed revision of the Preservation Guidelines, the Historic District Commission shall submit said revisions to the City Council along with a recommendation for approval or disapproval. Said report shall outline efforts made to gather community input from residents of the Historic District as well as summarize such input.
- (b) Upon City Council receiving said revisions, City Council shall have the duty to review proposed revisions and vote to either approve or disapprove the inclusion of the revisions in the Preservation Guidelines during the next available City Council meeting. The effective date of any approved revisions shall be thirty (30) days from the date the City Council vote on the proposed revisions is recorded.

\* \* \* \* \*

§ 2. SEVERABILITY. If any section, subsection, sentence, clause, phrase, or portion of this Ordinance is, for any reason, held invalid or unconstitutional, such portion shall be deemed a separate, distinct and independent provision and such holding shall not affect the validity of the remaining portions of this Ordinance.

ADOPTED this\_\_\_\_day  
of\_\_\_\_\_, 2022.

NOT ADOPTED this\_\_\_\_day  
of\_\_\_\_\_, 2022.

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Mayor

ATTEST:

\_\_\_\_\_  
City Clerk

ORDINANCE NO. O-2122-31

ITEM NO. 15

**STAFF REPORT**

**ITEM:** AN ORDINANCE OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA AMENDING CHAPTER 22 (ZONING ORDINANCE), ARTICLE XI, SPECIFIC DISTRICT REGULATIONS, SECTION 429.3 IN DEFINITIONS DELETING CONSERVATION AND ADDING ORIGINAL, PERIOD OF SIGNIFICANCE, RELOCATION, SECRETARY OF THE INTERIOR STANDARDS OF HISTORIC BUILDINGS; EDITS FOR CONSISTENCY OR CORRECTIVE PURPOSES; ADDED WRITTEN DENIAL REQUIREMENT; ADDED SEVEN DAY NOTICE; EXTENDED TIME LIMIT OF COA FROM SIX MONTHS TO TWELVE MONTHS; RESTRUCTURED EXCEPTIONS TO ADMINISTRATIVE BYPASS; AND PROVIDING FOR THE SEVERABILITY THEREOF.

**BACKGROUND:**

Council enacted Ordinance No. O-9293-30 on August 10, 1993, establishing the Historic District Ordinance for the City of Norman. The ordinance establishes the Historic District Commission and associated provisions to regulate designated Historic Districts including the development and revision of Historic Preservation Guidelines.

With the adoption of revised Historic Preservation Guidelines in the previous agenda item, it is necessary to update the Historic District Ordinance to reflect those revisions. This also provides an opportunity to correct any errors or issues identified with the Ordinance.

**DISCUSSION:**

After the Draft Historic Preservation Guidelines were recommended for approval on August 2, 2021 by the Historic District Commission, staff revised the Historic District Ordinance to incorporate necessary revisions to allow for enforcement of the revised Guidelines. Additionally, other revisions were identified by staff and presented to the Commission for consideration. The Commission discussed the proposed revisions to the Historic District Ordinance at their October 4, 2021 meeting and recommended the adoption of the attached Historic District Ordinance. The following provides a summation of the revisions proposed.

**SUMMARY OF HISTORIC DISTRICT ORDINANCE REVISIONS**

1. Removal of a repetitive definition for the word *conservation* which is defined elsewhere in the Ordinance.
2. Provided definitions of original, period of significance, relocation, Secretary of the Interior Standards.
3. Reorganized sections of the ordinance to provide clarity. For example, two sections discussed the appeals process. The two sections were combined into one section to provide clarity.



4. Requires adjacent property owner letters be sent out at least 7 calendar days prior to the Historic District Meeting. Presently, there is not a specific notice date requirement for adjacent property owner notification letters.
5. Extended the expiration of Certificate of Appropriateness from six months to 12 months. This provides a more reasonable amount of time for applicants to begin projects.
6. Restructured ordinance to allow additional items approvable by the Administrative Bypass process as identified in the Historic Preservation Guidelines.
7. The correction of typos found during the revision process.

**CONCLUSION:**

Staff presents Ordinance No. O-2122-31 to the Planning Commission for discussion and consideration.

**NORMAN PLANNING COMMISSION  
REGULAR SESSION MINUTES**

**DECEMBER 9, 2021**

The Planning Commission of the City of Norman, Cleveland County, State of Oklahoma, met in Regular Session in the Council Chambers of the Norman Municipal Building, 201 West Gray Street, on the 9<sup>th</sup> day of December, 2021.

Notice and agenda of the meeting was posted at the Norman Municipal Building and online at <https://norman-ok.municodemeetings.com> at least twenty-four hours prior to the beginning of the meeting.

Chair Erica Bird called the meeting to order at 6:30 p.m.

\* \* \*

**ROLL CALL**

**MEMBERS PRESENT**

Erin Williford (arrived at 6:32 p.m.)  
Kevan Parker  
Steven McDaniel  
Erica Bird  
Dave Boeck  
Sandy Bahan  
Michael Jablonski

**MEMBERS ABSENT**

Lark Zink

A quorum was present.

**STAFF MEMBERS PRESENT**

Jane Hudson, Director, Planning &  
Community Development  
Lora Hoggatt, Planning Services Manager  
Logan Hubble, Planner I  
Anais Starr, Planner II  
Colton Wayman, Planner I  
Roné Tromble, Recording Secretary  
Ken Danner, Subdivision Development  
Manager  
Kathryn Walker, City Attorney  
Heather Poole, Asst. City Attorney  
Jeanne Snider, Asst. City Attorney  
Jami Short, Traffic Engineer  
Bryce Holland, Multimedia Specialist

\* \* \*

Item No. 15, being:

**CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT, AND/OR POSTPONEMENT OF ORDINANCE NO. O-2122-31 - AN ORDINANCE OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, AMENDING CHAPTER 22 (ZONING ORDINANCE), ARTICLE XI, SPECIFIC DISTRICT REGULATIONS, SECTION 429.3 IN DEFINITIONS DELETING CONSERVATION AND ADDING ORIGINAL, PERIOD OF SIGNIFICANCE, RELOCATION, SECRETARY OF THE INTERIOR STANDARDS OF HISTORIC BUILDINGS; EDITS FOR CONSISTENCY OR CORRECTIVE PURPOSES; ADDED WRITTEN DENIAL REQUIREMENT; ADDED SEVEN DAY NOTICE; EXTENDED TIME LIMIT OF COA FROM SIX MONTHS TO TWELVE MONTHS; RESTRUCTURED EXCEPTIONS TO ADMINISTRATIVE BYPASS; AND PROVIDING FOR THE SEVERABILITY THEREOF.**

**ITEMS SUBMITTED FOR THE RECORD:**

1. Staff Report
2. October 4, 2021 Historic District Commission Minutes
3. Annotated Ordinance No. O-2122-31

**PRESENTATION BY STAFF:**

1. Ms. Anais Starr presented the staff report.

Commissioner Bird asked a question about windows.  
Ms. Anais Starr responded.

**AUDIENCE PARTICIPATION:**

None

**DISCUSSION AND ACTION BY THE PLANNING COMMISSION:**

*Dave Boeck moved to recommend adoption of Ordinance No. O-2122-31 to City Council. Erin Williford seconded the motion.*

There being no further discussion, a vote on the motion was taken with the following result:

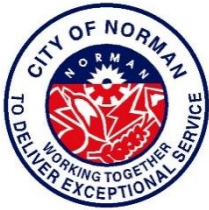
YEAS	Erin Williford, Kevan Parker, Steven McDaniel, Erica Bird, Dave Boeck, Sandy Bahan, Michael Jablonski
NAYES	None
MEMBERS ABSENT	Lark Zink

The motion, to recommend adoption of Ordinance No. O-2122-31 to City Council, passed by a vote of 7-0.

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**File Attachments for Item:**

23. CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT AND/OR POSTPONEMENT OF ORDINANCE O-2122-34 UPON SECOND AND FINAL READING: AN ORDINANCE OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA AMENDING SECTION 21-113 OF CHAPTER 21 OF THE CODE OF ORDINANCES OF THE CITY OF NORMAN, OKLAHOMA, CONTINGENT ON VOTER APPROVAL OF ORDINANCE NO. O-2122-35, INCREASING THE MONTHLY BASE FEE FOR RESIDENTIAL METERED USERS FROM SIX DOLLARS (\$6.00) TO SEVEN DOLLARS AND EIGHTY CENTS (\$7.80); INCREASING THE MONTHLY WATER RATES FOR RESIDENTIAL METERED USERS FROM THREE DOLLARS AND THIRTY-FIVE CENTS (\$3.35) TO FOUR DOLLARS AND TWENTY CENTS (\$4.20) PER THOUSAND GALLONS FOR THE FIRST 5,000 GALLONS OF WATER, FROM FOUR DOLLARS AND TEN CENTS (\$4.10) TO FIVE DOLLARS AND FIFTEEN CENTS (\$5.15) PER THOUSAND GALLONS FOR WATER USAGE BETWEEN 5,001 AND 15,000 GALLONS, FROM FIVE DOLLARS AND TWENTY CENTS (\$5.20) TO SIX DOLLARS AND FIFTY CENTS (\$6.50) PER THOUSAND GALLONS FOR WATER USAGE BETWEEN 15,001 AND 20,000 GALLONS, AND FROM SIX DOLLARS AND EIGHTY CENTS (\$6.80) TO EIGHT DOLLARS AND FIFTY CENTS (\$8.50) PER THOUSAND GALLONS FOR WATER USAGE OVER 20,000 GALLONS; INCREASING THE MONTHLY BASE FEE FOR NON-RESIDENTIAL WATER SERVICE FROM SIX DOLLARS (\$6.00) TO SEVEN DOLLARS AND EIGHTY CENTS (\$7.80); INCREASING THE WATER RATES FOR NON-RESIDENTIAL METERED USERS FROM THREE DOLLARS AND EIGHTY CENTS (\$3.80) TO FOUR DOLLARS AND THIRTY-FIVE CENTS (\$4.35) PER THOUSAND GALLONS FOR WATER USAGE UP TO SUCH CUSTOMER'S AVERAGE WINTER CONSUMPTION AS DEFINED HEREIN AND FROM FOUR DOLLARS AND TWENTY CENTS (\$4.20) TO FOUR DOLLARS AND EIGHTY CENTS (\$4.80) PER THOUSAND GALLONS FOR WATER USAGE EXCEEDING SUCH NON-RESIDENTIAL METERED USER'S AVERAGE WINTER CONSUMPTION AS DEFINED HEREIN; PROVIDING AN EFFECTIVE DATE FOR SAID INCREASE SUBJECT TO VOTER APPROVAL; AND PROVIDING FOR THE SEVERABILITY THEREOF.



## CITY OF NORMAN, OK STAFF REPORT

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**MEETING DATE:** 01/25/2022

**REQUESTER:** Chris Mattingly

**PRESENTER:** Chris Mattingly, Director of Utilities

**ITEM TITLE:** CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT AND/OR POSTPONEMENT OF ORDINANCE O-2122-34 UPON SECOND AND FINAL READING: AN ORDINANCE OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA AMENDING SECTION 21-113 OF CHAPTER 21 OF THE CODE OF ORDINANCES OF THE CITY OF NORMAN, OKLAHOMA, CONTINGENT ON VOTER APPROVAL OF ORDINANCE NO. O-2122-35, INCREASING THE MONTHLY BASE FEE FOR RESIDENTIAL METERED USERS FROM SIX DOLLARS (\$6.00) TO SEVEN DOLLARS AND EIGHTY CENTS (\$7.80); INCREASING THE MONTHLY WATER RATES FOR RESIDENTIAL METERED USERS FROM THREE DOLLARS AND THIRTY-FIVE CENTS (\$3.35) TO FOUR DOLLARS AND TWENTY CENTS (\$4.20) PER THOUSAND GALLONS FOR THE FIRST 5,000 GALLONS OF WATER, FROM FOUR DOLLARS AND TEN CENTS (\$4.10) TO FIVE DOLLARS AND FIFTEEN CENTS (\$5.15) PER THOUSAND GALLONS FOR WATER USAGE BETWEEN 5,001 AND 15,000 GALLONS, FROM FIVE DOLLARS AND TWENTY CENTS (\$5.20) TO SIX DOLLARS AND FIFTY CENTS (\$6.50) PER THOUSAND GALLONS FOR WATER USAGE BETWEEN 15,001 AND 20,000 GALLONS, AND FROM SIX DOLLARS AND EIGHTY CENTS (\$6.80) TO EIGHT DOLLARS AND FIFTY CENTS (\$8.50) PER THOUSAND GALLONS FOR WATER USAGE OVER 20,000 GALLONS; INCREASING THE MONTHLY BASE FEE FOR NON-RESIDENTIAL WATER SERVICE FROM SIX DOLLARS (\$6.00) TO SEVEN DOLLARS AND EIGHTY CENTS (\$7.80); INCREASING THE WATER RATES FOR NON-RESIDENTIAL METERED USERS FROM THREE DOLLARS AND EIGHTY CENTS (\$3.80) TO FOUR DOLLARS AND THIRTY-FIVE CENTS (\$4.35) PER THOUSAND GALLONS FOR WATER USAGE UP TO SUCH CUSTOMER'S AVERAGE WINTER CONSUMPTION AS DEFINED HEREIN AND FROM FOUR DOLLARS AND TWENTY CENTS (\$4.20) TO FOUR DOLLARS AND EIGHTY CENTS (\$4.80) PER THOUSAND GALLONS FOR WATER USAGE EXCEEDING SUCH NON-RESIDENTIAL METERED USER'S AVERAGE WINTER CONSUMPTION AS DEFINED HEREIN; PROVIDING AN EFFECTIVE DATE FOR SAID INCREASE SUBJECT TO VOTER APPROVAL; AND PROVIDING FOR THE SEVERABILITY THEREOF.

## BACKGROUND:

The City of Norman receives revenues from water customers through monthly billing based on approved water rates. In accordance with the City Charter, water rates are only modified through a public vote. The last rate increase was approved in January 2015 and provided funding for the following projects:

PROJECT	PROP. COST (\$, in millions)	ACTUAL COST (\$, in millions)	STATUS
Water Treatment Plant Phase II Improvements	31.0	36.2	Complete
Groundwater Blending	2.0	0	Insufficient Funds
Additional Groundwater Supply of 2 Million Gallons per Day	9.0	11.0	Complete
Land Purchase	2.0	0	Land Unavailable
Water Line Replacements	3.0	15.8	Complete
<b>TOTAL</b>	<b>47.0</b>	<b>63.0</b>	

The funded projects from the 2015 rate increase were completed except for two projects. The groundwater blending project was not fully funded within the last rate increase and is still a proposed capital project and is discussed further in this memo. A site has been identified for the purchase of land for the groundwater treatment plant and negotiations are underway. Since the completion of these past projects, additional needs have arisen and are discussed further below.

### 1. GROUNDWATER SYSTEM DISINFECTION

The Norman water system is supplied by water from three sources: the water treatment plant which receives water from Lake Thunderbird, the groundwater well system, and the Oklahoma City wholesale connection. Water from the water treatment plant and the Oklahoma City wholesale connection are from surface water sources and therefore are required to have a disinfectant residual. Water from the groundwater system has not historically and does not currently have disinfection or a disinfectant residual since groundwater systems are not required to have a disinfectant residual. However, since the system is a combined surface water and groundwater system, the Oklahoma Department of Environmental Quality is requiring that Norman begin adding a disinfectant to the groundwater system so that a chlorine residual of 1.0 parts per million (ppm) meeting Oklahoma Administrative Code 252:631-3-3 (d) (5) is maintained throughout the entire distribution system. To meet this mandate, a capital project is proposed to build a centralized facility to collect groundwater for disinfection and future treatment should regulations become more stringent. This project will include the construction of the facility containing buildings, chemical systems, tanks, pump station, and required piping to get the water to the facility.

### 2. ADVANCED WATER METERING

The Advanced Water Metering project will replace approximately 41,000 meters and install new meters with electronic transmitters that will regularly transmit wirelessly recorded water usage. The majority of existing water meters have reached the end of their recommended life warranting replacement and the conversion to this advanced metering has become an industry standard

with newer technology due to the numerous benefits that result from the system. Benefits anticipated from this project include:

- Improved customer satisfaction through accurate water bills;
- Better water use management for customers;
- Leak notifications;
- Water conservation advances with the additional data;
- More efficient use of City staff; and
- Reduced energy from vehicles use and water pumping.

### 3. *WATER LINE REPLACEMENT*

The water system is comprised of over 630 miles of water lines with over half of these pipes made of cast or ductile iron. Due to soil conditions in Norman, these metal lines have shown an increased rate of failure which impacts service to our customers. Repairs for failures in an emergency manner are also very cost-inefficient compared to replacement projects in advance of failures. Additional funds for this work will improve service reliability and reduce operating costs in the long-term financial outlook.

### 4. *INCREASED OPERATIONAL COSTS*

Operating costs of power, gas, chemicals, and numerous other costs necessary for treating and pumping water and operating the water system continue to increase. Additional funds are required to operate the system in light of increasing costs.

The NUA hired Raftelis to complete an analysis of the water fund and its revenue sources and anticipated expenditures including the above items. This analysis included an evaluation of water connection fees and water rates. A water connection fee increased based on future growth projects was determined to be warranted. However, the groundwater system disinfection project, the advanced water metering project, additional water line replacements, and increased operating costs are not growth related items so connection fees cannot be used for these items.

Regarding proposed water rate increases, Raftelis presented three rate increase alternatives to Council during a study session on December 7, 2021 to generate an additional \$4.7 million annually to provide the required funding. The three alternatives presented were:

1. Increase of base fee and volume rates evenly;
2. Staff-calculated base fee and volume rates; and
3. Larger increase of base fee and smaller increase of volume rates.

Council/NUA discussed the advantages/benefits of the different alternatives but ultimately determined that Alternative 1 was the best option to move forward.

**DISCUSSION:** Ordinance No. O-2122-34 amends the water rates contingent on voter approval. Ordinance No. O- 2122-35 calls for an election to be held on April 5, 2022, for voters to approve or reject O-2122-34. The proposed rate increases for residential customers as outlined within Ordinance No. O- 2122-34 are briefly shown in the following table.



Description	Existing Rates	Proposed Rates
<b>Monthly Water Service Charge, \$ per bill*</b>		
All Customers	\$7.50	\$9.30
<b>Volume Rates, \$ per 1,000 gallons</b>		
Block 1 (0 - 5,000 gallons)	\$3.35	\$4.20
Block 2 (5,001 – 15,000)	\$4.10	\$5.15
Block 3 (15,001 – 20,000)	\$5.20	\$6.50
Block 4 (20,001 and more)	\$6.80	\$8.50
<i>*Includes \$1.50 monthly capital improvement charge (CIC) which is NOT modified by the proposed rate increase</i>		

The proposed rate increases for non-residential customers as outlined within Ordinance O-2122-34 are briefly shown in the following table.

Description	Existing Rates	Proposed Rates
<b>Monthly Water Service Charge, \$ per bill*</b>		
All Customers	\$6.00	\$7.80
<b>Volume Rates, \$ per 1,000 gallons</b>		
Average Winter Consumption (AWC)	\$3.80	\$4.35
Above AWC	\$4.20	\$4.80
<i>*Does not include the monthly capital improvement charge (CIC) which is calculated at 60 percent of the billed sewer revenue and is NOT modified by the proposed rate increase</i>		

The next available election date is April 5, 2022. If the rates are adopted by the citizens they will go into effect June 1, 2022.

**RECOMMENDATION:** The proposed rates in Ordinance No. O-2122-34 are the result of NUA/Council's discussion and represent rates designed to cover necessary capital and operation expenses associated with planned improvements. Ordinance O-2122-35 calls a Special Election for a public vote on these rates on April 5, 2022. Staff recommends approval of Ordinances Nos. O-2122-34 and O-2122-35 upon Second and Final Reading.

O-2122-34  
Clean

AN ORDINANCE OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA AMENDING SECTION 21-113 OF CHAPTER 21 OF THE CODE OF ORDINANCES OF THE CITY OF NORMAN, OKLAHOMA, CONTINGENT ON VOTER APPROVAL OF ORDINANCE NO. O-2122-35, INCREASING THE MONTHLY BASE FEE FOR RESIDENTIAL METERED USERS FROM SIX DOLLARS (\$6.00) TO SEVEN DOLLARS AND EIGHTY CENTS (\$7.80); INCREASING THE MONTHLY WATER RATES FOR RESIDENTIAL METERED USERS FROM THREE DOLLARS AND THIRTY-FIVE CENTS (\$3.35) TO FOUR DOLLARS AND TWENTY CENTS (\$4.20) PER THOUSAND GALLONS FOR THE FIRST 5,000 GALLONS OF WATER, FROM FOUR DOLLARS AND TEN CENTS (\$4.10) TO FIVE DOLLARS AND FIFTEEN CENTS (\$5.15) PER THOUSAND GALLONS FOR WATER USAGE BETWEEN 5,001 AND 15,000 GALLONS, FROM FIVE DOLLARS AND TWENTY CENTS (\$5.20) TO SIX DOLLARS AND FIFTY CENTS (\$6.50) PER THOUSAND GALLONS FOR WATER USAGE BETWEEN 15,001 AND 20,000 GALLONS, AND FROM SIX DOLLARS AND EIGHTY CENTS (\$6.80) TO EIGHT DOLLARS AND FIFTY CENTS (\$8.50) PER THOUSAND GALLONS FOR WATER USAGE OVER 20,000 GALLONS; INCREASING THE MONTHLY BASE FEE FOR NON-RESIDENTIAL WATER SERVICE FROM SIX DOLLARS (\$6.00) TO SEVEN DOLLARS AND EIGHTY CENTS (\$7.80); INCREASING THE WATER RATES FOR NON-RESIDENTIAL METERED USERS FROM THREE DOLLARS AND EIGHTY CENTS (\$3.80) TO FOUR DOLLARS AND THIRTY-FIVE CENTS (\$4.35) PER THOUSAND GALLONS FOR WATER USAGE UP TO SUCH CUSTOMER'S AVERAGE WINTER CONSUMPTION AS DEFINED HEREIN AND FROM FOUR DOLLARS AND TWENTY CENTS (\$4.20) TO FOUR DOLLARS AND EIGHTY CENTS (\$4.80) PER THOUSAND GALLONS FOR WATER USAGE EXCEEDING SUCH NON-RESIDENTIAL METERED USER'S AVERAGE WINTER CONSUMPTION AS DEFINED HEREIN; PROVIDING AN EFFECTIVE DATE FOR SAID INCREASE SUBJECT TO VOTER APPROVAL; AND PROVIDING FOR THE SEVERABILITY THEREOF.

- § 1. WHEREAS, the rates for water service provided in the City of Norman and currently in effect in the City were last increased on January 13, 2015; and
- § 2. WHEREAS, the Oklahoma Department of Environmental Quality has mandated that the City of Norman begin disinfection of groundwater in order to maintain chlorine residual throughout the distribution system thereby necessitating the construction of a centralized facility with chemical systems, piping, pumps and tanks to meet this requirement, which will also allow for the incorporation of future treatment processes should more stringent future regulations require additional treatment of groundwater; and

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Clean

- § 3. WHEREAS, the City desires to purchase and install new advanced meters that will timely and accurately measure water usage for customers to allow for accurate water billing, which will improve customer satisfaction and allow our customers to better manage their water usage, while also allowing the City to improve water conservation efforts, more efficiently use staff, and measure water service in a more environmentally friendly manner; and
- §4. WHEREAS, the City proposes to increase funding for the replacement of water mains, since the City's water system is comprised of approximately 630 miles of transmission and distribution mains, and over half of these mains are cast or ductile iron which experiences increased failures compared to other pipe materials due to soil corrosion, and their emergency repair is extremely inefficient and costly compared to scheduled replacement projects; and
- §5. WHEREAS, since the last rate increase in 2015, operational costs have continued to increase; and
- §6. WHEREAS, the rates for water service must be increased in order for the necessary improvements to be accomplished and continued operation of the City of Norman's water system; and

NOW, THEREFORE, BE IT ORDAINED BY THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA:

- §7. That should the voters approve the rate increase in water service set out in Ordinance No. O-2122-35, then Section 21-113 of Chapter 21 of the Code of Ordinances of the City of Norman, Oklahoma, shall be and is hereby amended to read as follows:

Sec. 21-113. Rates for water service.

- (a) The water rates and charges hereinafter enumerated shall be collected for the purposes of providing funds for the continued operation, improvement, servicing and maintenance of the City water system and for other municipal governmental purposes related to providing utility services.
- (b) The following monthly rates shall apply to each residential metered user (including residence, mobile home, or other single-family dwelling unit, apartment, townhouse or dwelling):
- (1) \$7.80 base fee;

O-2122-34  
Clean

- (2) \$4.20 per one thousand (1,000) gallons for 0 to 5,000 gallons;
  - (3) \$5.15 per one thousand (1,000) gallons for 5,001 to 15,000 gallons;
  - (4) \$6.50 per one thousand (1,000) gallons for 15,001 to 20,000 gallons;
  - (5) \$8.50 per one thousand (1,000) gallons for over 20,000 gallons.
- (c) The base rate and the first five thousand (5,000) gallons of usage for residential metered users under subsection (b) above, who are low income as defined by Section 8 of the Housing Act of 1937, amended by the Housing and Community Development Act of 1974, will be calculated at a reduction of twenty-five (25) percent.
- (d) In addition to the monthly rates set forth herein, all households (defined as a residential unit consistent with the purposes statement of R-1 and RI-A zoning districts) shall be assessed a high usage surcharge of thirty-five cents (\$0.35) per thousand gallons of usage over twenty thousand (20,000) gallons used during the peak water demand months of July and August (billed in August and September, respectively).
- (e) The following monthly rates shall apply to each non-residential metered user (including business, industrial, or commercial establishments):
- (1) Average winter consumption: the average of a non-residential metered user's water consumption for the monthly billings generated by the City of Norman in December, January, and February of the most recent year.
  - (2) All non-residential metered users shall pay a base fee of six dollars (\$7.80) for each water meter.
  - (3) The water rate for consumption up to a non-residential metered user's average winter consumption shall be three dollars and eighty cents (\$4.35) per one thousand (1,000) gallons of water.
  - (4) The water rate for consumption exceeding a non-residential metered user's average winter consumption shall be four dollars and twenty cents (\$4.80) per one thousand (1,000) gallons of water.
- § 8. Effective date. The rates described above shall be effective for all billings issued on or after the 1st day of June, 2022, and thereafter conditioned upon said rate increase being approved

O-2122-34  
Clean

by a majority of the registered voters voting in an election called for the purpose of approving or rejecting said rates; Said election to be held on the 5th day of April, 2022.

- § 9. Severability. If any section, subsection, sentence, clause, phrase or portion of this ordinance is for any reason, held invalid or unconstitutional by any court of competent jurisdiction, such portion shall be deemed a separate, distinct and independent provision and such holding shall not affect the validity of the remaining portions of this ordinance, except, that the effective date provision shall not be severable from the operative provisions of the ordinance.

ADOPTED this \_\_\_\_\_ day  
of \_\_\_\_\_, 2022.

NOT ADOPTED this \_\_\_\_\_ day  
of \_\_\_\_\_, 2022.

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Mayor

ATTEST:

\_\_\_\_\_  
City Clerk

AN ORDINANCE OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA AMENDING SECTION 21-113 OF CHAPTER 21 OF THE CODE OF ORDINANCES OF THE CITY OF NORMAN, OKLAHOMA, CONTINGENT ON VOTER APPROVAL OF ORDINANCE NO. O-2122-35, INCREASING THE MONTHLY BASE FEE FOR RESIDENTIAL METERED USERS FROM SIX DOLLARS (\$6.00) TO SEVEN DOLLARS AND EIGHTY CENTS (\$7.80); INCREASING THE MONTHLY WATER RATES FOR RESIDENTIAL METERED USERS FROM THREE DOLLARS AND THIRTY-FIVE CENTS (\$3.35) TO FOUR DOLLARS AND TWENTY CENTS (\$4.20) PER THOUSAND GALLONS FOR THE FIRST 5,000 GALLONS OF WATER, FROM FOUR DOLLARS AND TEN CENTS (\$4.10) TO FIVE DOLLARS AND FIFTEEN CENTS (\$5.15) PER THOUSAND GALLONS FOR WATER USAGE BETWEEN 5,001 AND 15,000 GALLONS, FROM FIVE DOLLARS AND TWENTY CENTS (\$5.20) TO SIX DOLLARS AND FIFTY CENTS (\$6.50) PER THOUSAND GALLONS FOR WATER USAGE BETWEEN 15,001 AND 20,000 GALLONS, AND FROM SIX DOLLARS AND EIGHTY CENTS (\$6.80) TO EIGHT DOLLARS AND FIFTY CENTS (\$8.50) PER THOUSAND GALLONS FOR WATER USAGE OVER 20,000 GALLONS; INCREASING THE MONTHLY BASE FEE FOR NON-RESIDENTIAL WATER SERVICE FROM SIX DOLLARS (\$6.00) TO SEVEN DOLLARS AND EIGHTY CENTS (\$7.80); INCREASING THE WATER RATES FOR NON-RESIDENTIAL METERED USERS FROM THREE DOLLARS AND EIGHTY CENTS (\$3.80) TO FOUR DOLLARS AND THIRTY-FIVE CENTS (\$4.35) PER THOUSAND GALLONS FOR WATER USAGE UP TO SUCH CUSTOMER'S AVERAGE WINTER CONSUMPTION AS DEFINED HEREIN AND FROM FOUR DOLLARS AND TWENTY CENTS (\$4.20) TO FOUR DOLLARS AND EIGHTY CENTS (\$4.80) PER THOUSAND GALLONS FOR WATER USAGE EXCEEDING SUCH NON-RESIDENTIAL METERED USER'S AVERAGE WINTER CONSUMPTION AS DEFINED HEREIN; PROVIDING AN EFFECTIVE DATE FOR SAID INCREASE SUBJECT TO VOTER APPROVAL; AND PROVIDING FOR THE SEVERABILITY THEREOF.

- § 1. WHEREAS, the rates for water service provided in the City of Norman and currently in effect in the City were last increased on January 13, 2015; and
- § 2. WHEREAS, the Oklahoma Department of Environmental Quality has mandated that the City of Norman begin disinfection of groundwater in order to maintain chlorine residual throughout the distribution system thereby necessitating the construction of a centralized facility with chemical systems, piping, pumps and tanks to meet this requirement, which will also allow for the incorporation of future treatment processes should more stringent future regulations require additional treatment of groundwater; and



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Annotated

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- (2) ~~\$3.35~~ \$4.20 per one thousand (1,000) gallons for 0 to 5,000 gallons;
  - (3) ~~\$4.10~~ \$5.15 per one thousand (1,000) gallons for 5,001 to 15,000 gallons;
  - (4) ~~\$5.20~~ \$6.50 per one thousand (1,000) gallons for 15,001 to 20,000 gallons;
  - (5) ~~\$6.80~~ \$8.50 per one thousand (1,000) gallons for over 20,000 gallons.
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- (d) In addition to the monthly rates set forth herein, all households (defined as a residential unit consistent with the purposes statement of R-1 and RI-A zoning districts) shall be assessed a high usage surcharge of thirty-five cents (\$0.35) per thousand gallons of usage over twenty thousand (20,000) gallons used during the peak water demand months of July and August (billed in August and September, respectively).
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- § 8. Effective date. The rates described above shall be effective for all billings issued on or after the 1st day of June, 2022, and thereafter conditioned upon said rate increase being approved

O-2122-34  
Annotated

by a majority of the registered voters voting in an election called for the purpose of approving or rejecting said rates; Said election to be held on the 5th day of April, 2022.

- § 9. Severability. If any section, subsection, sentence, clause, phrase or portion of this ordinance is for any reason, held invalid or unconstitutional by any court of competent jurisdiction, such portion shall be deemed a separate, distinct and independent provision and such holding shall not affect the validity of the remaining portions of this ordinance, except, that the effective date provision shall not be severable from the operative provisions of the ordinance.

ADOPTED this \_\_\_\_\_ day  
of \_\_\_\_\_, 2022.

NOT ADOPTED this \_\_\_\_\_ day  
of \_\_\_\_\_, 2022.

\_\_\_\_\_  
Mayor

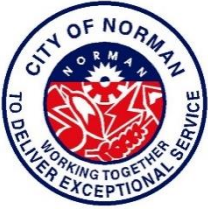
\_\_\_\_\_  
Mayor

ATTEST:

\_\_\_\_\_  
City Clerk

**File Attachments for Item:**

24. CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT AND/OR POSTPONEMENT OF ORDINANCE O-2122-35 UPON SECOND AND FINAL READING: AN ORDINANCE OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, AUTHORIZING THE CALLING AND HOLDING OF A SPECIAL ELECTION IN THE CITY OF NORMAN, COUNTY OF CLEVELAND, STATE OF OKLAHOMA, ON THE 5TH DAY OF APRIL, 2022, FOR THE PURPOSE OF SUBMITTING TO THE REGISTERED VOTERS OF THE CITY OF NORMAN THE QUESTION OF APPROVING OR REJECTING ORDINANCE O-2122-34, WHICH ORDINANCE AMENDING SECTION 21-113 OF CHAPTER 21 OF THE CODE OF ORDINANCES OF THE CITY OF NORMAN, OKLAHOMA, CONTINGENT ON VOTER APPROVAL OF ORDINANCE O-2122-35, INCREASING THE MONTHLY BASE FEE FOR RESIDENTIAL METERED USERS FROM SIX DOLLARS (\$6.00) TO SEVEN DOLLARS AND EIGHTY CENTS (\$7.80); INCREASING THE MONTHLY WATER RATES FOR RESIDENTIAL METERED USERS FROM THREE DOLLARS AND THIRTY-FIVE CENTS (\$3.35) TO FOUR DOLLARS AND TWENTY CENTS (\$4.20) PER THOUSAND GALLONS FOR THE FIRST 5,000 GALLONS OF WATER, FROM FOUR DOLLARS AND TEN CENTS (\$4.10) TO FIVE DOLLARS AND FIFTEEN CENTS (\$5.15) PER THOUSAND GALLONS FOR WATER USAGE BETWEEN 5,001 AND 15,000 GALLONS, FROM FIVE DOLLARS AND TWENTY CENTS (\$5.20) TO SIX DOLLARS AND FIFTY CENTS (\$6.50) PER THOUSAND GALLONS FOR WATER USAGE BETWEEN 15,001 AND 20,000 GALLONS, AND FROM SIX DOLLARS AND EIGHTY CENTS (\$6.80) TO EIGHT DOLLARS AND FIFTY CENTS (\$8.50) PER THOUSAND GALLONS FOR WATER USAGE OVER 20,000 GALLONS; INCREASING THE MONTHLY BASE FEE FOR NON-RESIDENTIAL WATER SERVICE FROM SIX DOLLARS (\$6.00) TO SEVEN DOLLARS AND EIGHTY CENTS (\$7.80); INCREASING THE WATER RATES FOR NON-RESIDENTIAL METERED USERS FROM THREE DOLLARS AND EIGHTY CENTS (\$3.80) TO FOUR DOLLARS AND THIRTY-FIVE CENTS (\$4.35) PER THOUSAND GALLONS FOR WATER USAGE UP TO SUCH CUSTOMER'S AVERAGE WINTER CONSUMPTION AS DEFINED HEREIN AND FROM FOUR DOLLARS AND TWENTY CENTS (\$4.20) TO FOUR DOLLARS AND EIGHTY CENTS (\$4.80) PER THOUSAND GALLONS FOR WATER USAGE EXCEEDING SUCH NON-RESIDENTIAL METERED USER'S AVERAGE WINTER CONSUMPTION AS DEFINED HEREIN; PROVIDING AN EFFECTIVE DATE FOR SAID INCREASE SUBJECT TO VOTER APPROVAL; AND PROVIDING FOR THE SEVERABILITY THEREOF.



## CITY OF NORMAN, OK STAFF REPORT

**MEETING DATE:** 01/25/2022

**REQUESTER:** Chris Mattingly

**PRESENTER:** Chris Mattingly, Director of Utilities

**ITEM TITLE:** CONSIDERATION OF ADOPTION, REJECTION, AMENDMENT AND/OR POSTPONEMENT OF ORDINANCE O-2122-35 UPON SECOND AND FINAL READING: AN ORDINANCE OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, AUTHORIZING THE CALLING AND HOLDING OF A SPECIAL ELECTION IN THE CITY OF NORMAN, COUNTY OF CLEVELAND, STATE OF OKLAHOMA, ON THE 5TH DAY OF APRIL, 2022, FOR THE PURPOSE OF SUBMITTING TO THE REGISTERED VOTERS OF THE CITY OF NORMAN THE QUESTION OF APPROVING OR REJECTING ORDINANCE O-2122-34, WHICH ORDINANCE AMENDING SECTION 21-113 OF CHAPTER 21 OF THE CODE OF ORDINANCES OF THE CITY OF NORMAN, OKLAHOMA, CONTINGENT ON VOTER APPROVAL OF ORDINANCE O-2122-35, INCREASING THE MONTHLY BASE FEE FOR RESIDENTIAL METERED USERS FROM SIX DOLLARS (\$6.00) TO SEVEN DOLLARS AND EIGHTY CENTS (\$7.80); INCREASING THE MONTHLY WATER RATES FOR RESIDENTIAL METERED USERS FROM THREE DOLLARS AND THIRTY-FIVE CENTS (\$3.35) TO FOUR DOLLARS AND TWENTY CENTS (\$4.20) PER THOUSAND GALLONS FOR THE FIRST 5,000 GALLONS OF WATER, FROM FOUR DOLLARS AND TEN CENTS (\$4.10) TO FIVE DOLLARS AND FIFTEEN CENTS (\$5.15) PER THOUSAND GALLONS FOR WATER USAGE BETWEEN 5,001 AND 15,000 GALLONS, FROM FIVE DOLLARS AND TWENTY CENTS (\$5.20) TO SIX DOLLARS AND FIFTY CENTS (\$6.50) PER THOUSAND GALLONS FOR WATER USAGE BETWEEN 15,001 AND 20,000 GALLONS, AND FROM SIX DOLLARS AND EIGHTY CENTS (\$6.80) TO EIGHT DOLLARS AND FIFTY CENTS (\$8.50) PER THOUSAND GALLONS FOR WATER USAGE OVER 20,000 GALLONS; INCREASING THE MONTHLY BASE FEE FOR NON-RESIDENTIAL WATER SERVICE FROM SIX DOLLARS (\$6.00) TO SEVEN DOLLARS AND EIGHTY CENTS (\$7.80); INCREASING THE WATER RATES FOR NON-RESIDENTIAL METERED USERS FROM THREE DOLLARS AND EIGHTY CENTS (\$3.80) TO FOUR DOLLARS AND THIRTY-FIVE CENTS (\$4.35) PER THOUSAND GALLONS FOR WATER USAGE UP TO SUCH CUSTOMER'S AVERAGE WINTER CONSUMPTION AS DEFINED HEREIN AND FROM FOUR DOLLARS AND TWENTY CENTS (\$4.20) TO FOUR DOLLARS AND EIGHTY CENTS (\$4.80) PER THOUSAND GALLONS FOR WATER USAGE EXCEEDING SUCH NON-

RESIDENTIAL METERED USER'S AVERAGE WINTER CONSUMPTION AS DEFINED HEREIN; PROVIDING AN EFFECTIVE DATE FOR SAID INCREASE SUBJECT TO VOTER APPROVAL; AND PROVIDING FOR THE SEVERABILITY THEREOF.

## BACKGROUND:

The City of Norman receives revenues from water customers through monthly billing based on approved water rates. In accordance with the City Charter, water rates are only modified through a public vote. The last rate increase was approved in January 2015 and provided funding for the following projects:

PROJECT	PROP. COST (\$, in millions)	ACTUAL COST (\$, in millions)	STATUS
Water Treatment Plant Phase II Improvements	31.0	36.2	Complete
Groundwater Blending	2.0	0	Insufficient Funds
Additional Groundwater Supply of 2 Million Gallons per Day	9.0	11.0	Complete
Land Purchase	2.0	0	Land Unavailable
Water Line Replacements	3.0	15.8	Complete
<b>TOTAL</b>	<b>47.0</b>	<b>63.0</b>	

The funded projects from the 2015 rate increase were completed except for two projects. The groundwater blending project was not fully funded within the last rate increase and is still a proposed capital project and is discussed further in this memo. A site has been identified for the purchase of land for the groundwater treatment plant and negotiations for acquisition are underway. Since the completion of these past projects, additional needs have arisen and are discussed further below.

### 1. GROUNDWATER SYSTEM DISINFECTION

The Norman water system is supplied by water from three sources: the water treatment plant which receives water from Lake Thunderbird, the groundwater well system, and the Oklahoma City wholesale connection. Water from the water treatment plant and the Oklahoma City wholesale connection are from surface water sources and therefore are required to have a disinfectant residual. Water from the groundwater system has not historically and does not currently have disinfection or a disinfectant residual since groundwater systems are not required to have a disinfectant residual. However, since the system is a combined surface water and groundwater system, the Oklahoma Department of Environmental Quality is requiring that Norman begin adding a disinfectant to the groundwater system so that a chlorine residual of 1.0 parts per million (ppm) meeting Oklahoma Administrative Code 252:631-3-3 (d) (5) is maintained throughout the entire distribution system. To meet this mandate, a capital project is proposed to build a centralized facility to collect groundwater for disinfection and future treatment should regulations become more stringent. This project will include the construction of the facility

containing buildings, chemical systems, tanks, pump station, and required piping to get the water to the facility.

## 2. *ADVANCED WATER METERING*

The Advanced Water Metering project will replace approximately 41,000 meters and install new meters with electronic transmitters that will regularly transmit wirelessly recorded water usage. The majority of existing water meters have reached the end of their recommended life warranting replacement and the conversion to this advanced metering has become an industry standard with newer technology due to the numerous benefits that result from the system.

Benefits anticipated from this project include:

- Improved customer satisfaction through accurate water bills;
- Better water use management for customers;
- Leak notifications;
- Water conservation advances with the additional data;
- More efficient use of City staff; and
- Reduced energy from vehicles use and water pumping.

## 3. *WATER LINE REPLACEMENT*

The water system is comprised of over 630 miles of water lines with over half of these pipes made of cast or ductile iron. Due to soil conditions in Norman, these metal lines have shown an increased rate of failure which impacts service to our customers. Repairs for failures in an emergency manner are also very cost-inefficient compared to replacement projects in advance of failures. Additional funds for this work will improve service reliability and reduce operating costs in the long-term financial outlook.

## 4. *INCREASED OPERATIONAL COSTS*

Operating costs of power, gas, chemicals, and numerous other costs necessary for treating and pumping water and operating the water system continue to increase. Additional funds are required to operate the system in light of increasing costs.

The NUA hired Raftelis to complete an analysis of the water fund and its revenue sources and anticipated expenditures including the above items. This analysis included an evaluation of water connection fees and water rates. A water connection fee increased based on future growth projects was determined to be warranted. However, the groundwater system disinfection project, the advanced water metering project, additional water line replacements, and increased operating costs are not growth related items so connection fees cannot be used for these items.

Regarding proposed water rate increases, Raftelis presented three rate increase alternatives to Council during a study session on December 7, 2021 to generate an additional \$4.7 million annually to provide the required funding. The three alternatives presented were:

1. Increase of base fee and volume rates evenly;
2. Staff-calculated base fee and volume rates; and
3. Larger increase of base fee and smaller increase of volume rates.

Council/NUA discussed the advantages/benefits of the different alternatives but ultimately



determined that Alternative 1 was the best option to move forward.

**DISCUSSION:** Ordinance No. O-2122-34 amends the water rate structure contingent on voter approval. Ordinance No. O- 2122-35 calls for an election to be held on April 5, 2022, for voters to approve or reject O-2122-34. The proposed rate increases for residential customers as outlined within Ordinance No. O- 2122-34 are briefly shown in the following table.

Description	Existing Rates	Proposed Rates
<b>Monthly Water Service Charge, \$ per bill*</b>		
All Customers	\$7.50	\$9.30
<b>Volume Rates, \$ per 1,000 gallons</b>		
Block 1 (0 - 5,000 gallons)	\$3.35	\$4.20
Block 2 (5,001 – 15,000)	\$4.10	\$5.15
Block 3 (15,001 – 20,000)	\$5.20	\$6.50
Block 4 (20,001 and more)	\$6.80	\$8.50
<i>*Includes \$1.50 monthly capital improvement charge (CIC) which is NOT modified by the proposed rate increase</i>		

The proposed rate increases for non-residential customers as outlined within Ordinance No. O-2122-34 are briefly shown in the following table.

Description	Existing Rates	Proposed Rates
<b>Monthly Water Service Charge, \$ per bill*</b>		
All Customers	\$6.00	\$7.80
<b>Volume Rates, \$ per 1,000 gallons</b>		
Average Winter Consumption (AWC)	\$3.80	\$4.35
Above AWC	\$4.20	\$4.80
<i>*Does not include the monthly capital improvement charge (CIC) which is calculated at 60 percent of the billed sewer revenue and is NOT modified by the proposed rate increase</i>		

The next available election date is April 5, 2022. If the rates are adopted by the citizens they will go into effect June 1, 2022.

**RECOMMENDATION:** The proposed rates in Ordinance O-2122-34 are the result of NUA/Council's discussion and represent rates designed to cover necessary capital and operation expenses associated with planned improvements. Ordinance O-2122-35 calls a Special Election for a public vote on these rates on April 5, 2022. Staff recommends approval of Ordinances O-2122-34 and O-2122-35 upon Second and Final Reading.

O-2122-35

AN ORDINANCE OF THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA, AUTHORIZING THE CALLING AND HOLDING OF A SPECIAL ELECTION IN THE CITY OF NORMAN, COUNTY OF CLEVELAND, STATE OF OKLAHOMA, ON THE 13<sup>TH</sup> DAY OF APRIL, 2022, FOR THE PURPOSE OF SUBMITTING TO THE REGISTERED VOTERS OF THE CITY OF NORMAN THE QUESTION OF APPROVING OR REJECTING ORDINANCE NO. O-2122-34, WHICH ORDINANCE AMENDS SECTION 21-113 OF THE CITY OF NORMAN, OKLAHOMA, ORDINANCES CONTINGENT ON VOTER APPROVAL OF ORDINANCE NO. O-2122-34, INCREASING THE MONTHLY BASE FEE FOR RESIDENTIAL METERED USERS FROM \$6.00 TO \$7.80; INCREASING THE MONTHLY WATER RATES FOR RESIDENTIAL METERED USERS FROM \$3.35 TO \$4.20 PER THOUSAND GALLONS FOR THE FIRST 5,000 GALLONS OF WATER, FROM \$4.10 TO \$5.15 PER THOUSAND GALLONS FOR WATER USAGE BETWEEN 5,001 AND 15,000 GALLONS, FROM \$5.20 TO \$6.50 PER THOUSAND GALLONS FOR WATER USAGE BETWEEN 15,001 AND 20,000 GALLONS, AND FROM \$6.80 TO \$8.50 PER THOUSAND GALLONS FOR WATER USAGE OVER 20,000 GALLONS; INCREASING THE MONTHLY BASE FEE FOR NON-RESIDENTIAL WATER SERVICE FROM \$6.00 TO \$7.80; INCREASING THE WATER RATES FOR NON-RESIDENTIAL METERED USERS FROM \$3.80 TO \$4.35 PER THOUSAND GALLONS FOR WATER USAGE UP TO SUCH CUSTOMER'S AVERAGE WINTER CONSUMPTION AND FROM \$4.20 TO \$4.80 PER THOUSAND GALLONS FOR WATER USAGE EXCEEDING SUCH NON-RESIDENTIAL METERED USER'S AVERAGE WINTER CONSUMPTION; PROVIDING AN EFFECTIVE DATE FOR SAID INCREASE SUBJECT TO VOTER APPROVAL; AND PROVIDING FOR THE SEVERABILITY THEREOF.

NOW, THEREFORE, BE IT ORDAINED BY THE COUNCIL OF THE CITY OF NORMAN, OKLAHOMA:

- § 1. Proposition. That the Mayor of the City of Norman, Oklahoma, or in his or her absence or incapacity, the duly qualified Mayor Pro Tem, be and hereby is authorized and directed to call a special election to be held in the City of Norman, Oklahoma, on the 5<sup>th</sup> day of April, 2022, for the purpose of submitting to the registered, qualified voters of said City of Norman, Oklahoma, for their approval or rejection the following proposition:

Shall Ordinance No. O-2122-34 amending Section 21-113 of the City of Norman, Oklahoma Ordinances, increasing the monthly base fee for residential metered users from \$6.00 to \$7.80; increasing the monthly water rates for residential metered users from \$3.35 to \$4.20 per thousand gallons for the first 5,000 gallons of water, from

O-2122-35

\$4.10 to \$5.15 per thousand gallons for water usage between 5,001 and 15,000 gallons, from \$5.20 to \$6.50 per thousand gallons for water usage between 15,001 and 20,000 gallons, and from \$6.80 to \$8.50 per thousand gallons for water usage over 20,000 gallons; increasing the monthly base fee for non-residential water service from \$6.00 to \$7.80; increasing the water rates for non-residential metered users from \$3.80 to \$4.35 per thousand gallons for water usage up to such customer's average winter consumption and from \$4.20 to \$4.80 per thousand gallons for water usage exceeding such non-residential metered user's average winter consumption; be approved?

- § 2. That such call for said election shall be by Special Election Proclamation and Notice, signed by the Mayor or Mayor Pro Tem and attested to by the City Clerk, setting forth the proposition to be voted on; that the ballots set forth in the proposition be voted upon substantially as set out in Section 1 hereof; and that the returns of said election shall be made to and canvassed by the Cleveland County Election Board.
- § 3. That the number and location of the polling places and the persons who conduct the elections shall be the same as the regular polling places and persons prescribed and selected by the Cleveland County Election Board for elections in the City of Norman, Oklahoma.
- §4. That the Special Election Proclamation and Notice of event date, a copy of which is on file with the City Clerk and which is incorporated herein by reference, calling such a special election, is hereby approved in all respects, and that the Mayor or Mayor Pro Tem is hereby authorized to execute said special election proclamation on behalf of the City, and the City Clerk is hereby authorized to attest and affix the seal of said City to said Special Election Proclamation and Notice and cause a copy of said Special Election Proclamation and Notice to be published as required by law, and a copy thereof delivered to the Cleveland County Election Board.
- §5. That the City Clerk shall serve or cause to be served, a copy of this Ordinance and a copy of the Special Election Proclamation and Notice of Election upon the office of the Cleveland County Election Board, not less than sixty (60) days prior to the date of the election.

ADOPTED this \_\_\_\_\_ day  
of \_\_\_\_\_, 2022.

NOT ADOPTED this \_\_\_\_\_ day  
of \_\_\_\_\_, 2022.

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Mayor

ATTEST:

\_\_\_\_\_  
City Clerk

## SPECIAL ELECTION PROCLAMATION AND NOTICE OF ELECTION

Under and by virtue of the Statutes of the State of Oklahoma and acts complimentary, supplementary and enacted pursuant thereto, and Ordinance No. O-2122-35 dated January 25, 2022, authorizing the calling of an election on the Propositions hereinafter set forth, I, the undersigned Mayor of the City of Norman, Oklahoma, hereby call a special election and give notice thereof to be held in the City of Norman, Oklahoma, on the 5th day of April, 2022 for the purpose of submitting to the registered qualified voters in said City the following Proposition:

### PROPOSITION I

Shall Ordinance No.O-2122-34 amending Section 21-113 of the City of Norman, Oklahoma Ordinances, increasing the monthly base fee for residential metered users from \$6.00 to \$7.80; increasing the monthly water rates for residential metered users from \$3.35 to \$4.20 per thousand gallons for the first 5,000 gallons of water, from \$4.10 to \$5.15 per thousand gallons for water usage between 5,001 and 15,000 gallons, from \$5.20 to \$6.50 per thousand gallons for water usage between 15,001 and 20,000 gallons, and from \$6.80 to \$8.50 per thousand gallons for water usage over 20,000 gallons; increasing the monthly base fee for non-residential water service from \$6.00 to \$7.80; increasing the water rates for non-residential metered users from \$3.80 to \$4.35 per thousand gallons for water usage up to such customer's average winter consumption and from \$4.20 to \$4.80 per thousand gallons for water usage exceeding such non-residential metered user's average winter consumption; be approved?

The ballot used at said election shall set out the Proposition as above set forth and shall also contain the words:

1 <sup>st</sup>	<input type="checkbox"/>	YES	- FOR THE ABOVE PROPOSITION
2 <sup>nd</sup>	<input type="checkbox"/>	NO	- AGAINST THE ABOVE PROPOSITION

(If the voter desires to vote for the above Proposition, he shall mark the ballot accordingly; if he desires to vote against the above Proposition, he shall mark the ballot accordingly.)

That only the registered qualified voters of the City of Norman, Oklahoma, may vote upon the Propositions as above set forth.

The polls shall be opened at 7:00 o'clock a.m. and shall remain open continuously until and be closed at 7:00 p.m.

The special election shall be held at the same places and in the same manner prescribed by law for conducting county and state elections and the numbers and locations of the polling places and the persons who shall conduct said election shall be the same as for county and state elections, all as respectively designated and prescribed by the County Election Board of Cleveland County, Oklahoma.

WITNESS my hand as Mayor of the City of Norman, Oklahoma, and the Seal of said City affixed hereto on the \_\_\_\_ day of \_\_\_\_\_, 2022.

\_\_\_\_\_  
Mayor

(SEAL)

ATTEST:

\_\_\_\_\_  
City Clerk