



Planning & Zoning Meeting

AGENDA

Tuesday, June 18, 2019

5:30 PM

215 N Broad Street, Monroe, GA 30655

I. **CALL TO ORDER**

II. **ROLL CALL**

III. **MINUTES OF PREVIOUS MEETING**

[1.](#) Minutes of Previous Meeting - May 21, 2019

IV. **REPORT FROM CODE ENFORCEMENT OFFICER**

V. **PUBLIC HEARINGS**

[1.](#) Request for Rezone from B3 to PCD - 0 Charlotte Rowell Blvd

[2.](#) Request for Variance - 350 Davis Street

[3.](#) Request for COA - 1190 West Spring Street

VI. **RECOMMENDATIONS ON REQUESTS**

VII. **OLD BUSINESS**

VIII. **NEW BUSINESS**

IX. **ADJOURNMENT**

**MONROE PLANNING COMMISSION
MINUTES
May 21, 2019**

Present: Mike Eckles, Rosalind Parks, Randy Camp, Kyle Harrison, David Butler

Absent: None

Staff: Debbie Adkinson – Code Dept Assistant
Patrick Kelley – Director of Code and Planning
Darrell Stone – Director of Planning and Development
Logan Propes – City Administrator

Visitors: John Hampton, Kyle Ward, Lee Rowell, Mie Birnbrey, Roger Rowell, George Baker III, Dennis Harkins, Steve Lee, Blake LeGate, Mark Meadows

CALL TO ORDER by CHAIRMAN MIKE ECKLES at 5:30 pm

Chairman Eckles asked for any changes, corrections or additions to the April 16, 2019 minutes. Hearing none he entertained a motion. Camp made a motion to approve. Parks seconded. Motion carried. Minutes approved.

Code Officer Report: None

Public Hearing open 5:31 pm

The first item of business: is for petition # VAR-1-2019 for a variance at 1415 E Church Street. The applicant, Terrastone Development, request a variance of Sect. 700.1 Table 11 of the City of Monroe Zoning Ordinance. This variance is for lot size reduction from 14,000 to 10,000 sq. ft.

Chairman Eckles asked for a representative to speak for the request.

Steve Lee a representative for Terrastone Development spoke to the request. They would like to reduce the lot size to preserve some green spaces and stream buffers yielding approximately 25 to 35 lots to facilitate this while maintaining the width standard for the R1 zoning. The plans have been changed to accommodate a second entrance/exit in the neighborhood.

Chairman Eckles asked if he knew what size the houses would be.

Lee – 1800 to 2200 sq. ft. up to 2400 for 2 story.

Harrison- Are you keeping the road frontage width within the R1 standards?

Lee – yes 100 ft wide.

Lee – The average house prices will be low end \$235,000 and topping out maybe \$280,000.

Chairman Eckles then asked if there were anyone opposed to the request?

There were none.

Public Hearing closed at 5:34 pm

Chairman Eckles entertained a motion to approve or deny the request.

Motion to approve made by Harrison
Seconded by Butler
Motion carried. Recommend approval.

Public Hearing open at 5:35 pm

The seconded item of business: is for petition # LDU-3-2019 for a Conditional Use to allow a 216-unit apartment development. The location for this Conditional Use is at 1031 Charlotte Rowell Boulevard. The applicant is Hillpointe Group.

Chairman Eckles asked for a Code Office recommendation.

Kelley – the recommendation is for denial. The comprehensive plan shows this to be Commercial/Retail corridor currently completely undeveloped.

Chairman Eckles asked if there is a representative to speak to this request.

John Hampton and Kyle Ward, Representatives of Hillpointe Group spoke to the request. Hampton - The project will be a class A, market rate non subsidized project. This project is a three story, stick frame, 2 bed, 2 bath units that consist of 9 buildings with 24 units per building. High end smart home finishes with quartz countertops, resort style pool, clubhouse, fitness center, dog park and more. Their market study and analysis looking at Monroe and areas around here is there is a substantial lack of affordable, quality, residential for rent housing in Monroe. There is a need in the market. The topo is not overly conducive to a commercial use. It goes up and then down in the back so they feel a multifamily use makes sense on this location. They presented a copy of a letter from MAB of Charlotte NC, developer of the Monroe Pavilion, stating that they support the project for the Oak Club Apartment as a synergistic component to the Monroe Pavilion development.

Kelley mentioned that most of the components for that density and building size increase are met on this plan but there may be some additional zoning requirements that you would have to comply with in order get the density increases and building size increases.

Harrison: What backs up to the 16 acres?

Ward: There is a creek at the back of the property with a dense natural buffer and behind that is a single-family residential neighborhood.

Kelley: That neighborhood is currently in the county.

Harrison: Patrick you stated this is all commercial/retail use at this time.

Kelley: yes

After more discussion of the need for this type of development in this area of Monroe Chairman Eckles asked for more questions or concerns. There was none.

Chairman Eckles asked if there is anyone to speak opposed to the request?

There was none.

Public Hearing closed at 5:46 pm

Chairman Eckles entertained a motion.

Motion to deny made by Harrison
Seconded by Parks
Motion carried with 3 yea 1 nay
Recommendation to deny

Public Hearing opened at 5:50 pm

The third item of business: is petition # VAR-5-2019 for a variance of Sect 643A.3(2) Parking and Sect 643A.4(2) Landscape Buffer. The location for the variance is at 2130 W Spring Street. The applicant is Southbound Monroe LLC. The property consists of .92 ac and is zoned B3.

Chairman Eckles asked if there is a representative to speak to this request.

Mike Bimbrey a representative for Southbound Monroe LLC spoke to the request. They are requesting the 10 ft landscape buffer on the western and eastern sides of the property to be reduced to 5 ft. Also requesting that the requirement of the Corridor Design Overlay District for one row of parking between the building and the road be increased to two rows. This is so the building can be lined up with other properties in the complex area. They have gotten concerns from Arby's that if the building is set up closer to the road it will restrict the view of Arby's from potential customs traveling West on Business 10/West Spring Street.

Harrison asked if the building had to be moved, would the front of the it sit about where the first row of parking is.

Kelley: yes. Also, they have 45 spaces here and with the information we have been given the calculations would not allow for that number of spaces. This is based on the parking requirements that we have in our ordinance now. We figured on the high twenties with the 20% we can do administratively would be no more that 34 permitted parking spaces with the intended uses and square footage we have gotten so far.

There was more discussion concerning the parking and moving the building closer to the road.

Chairman Eckles asked if there were any questions.
There was none.

Chairman Eckles asked if there was any opposition to the request.
There was none.

Public Hearing Closed at 6:09 pm.

Chairman Eckles entertained a motion.

Motion to deny made by Camp
Seconded by Harrison
Motion Carried
Recommendation to deny

Public Hearing opened at 6:09 pm

The fourth item of business is petition # PCOM-6-2019 for a Certificate of Appropriateness for a new Commercial complex. The request is for 2130 West Spring Street.

There was no one available to speak for or against this request.

Chairman Eckles entertained a motion

Motion to deny made by Parks
Seconded by Butler
Motion Carried
COA Denied

Public Hearing closed at 6:10 pm

Old Business: none
New Business: none

Chairman Eckles entertained a motion to adjourn.

Motion to adjourn by Camp Seconded by Butler
Motion Carried – Meeting adjourned at 6:12 pm



To: Planning and Zoning / City Council
From: Patrick Kelley
Department: Planning, Zoning, Code and Development
Date: 05-20-2019
Description: Request for re-zoning from B-3 to PCD

Budget Account/Project Name: NA

Funding Source: 2019 NA

Budget Allocation: NA

Budget Available: NA

Requested Expense: \$NA **Company of Purchase:** NA

Recommendation: *Approval with recommended amendments determined by staff, the Planning and Zoning Commission and the City Council. Suggested Staff edits are:*

- *Require completed side walk network*
- *Do not accept the definition change of 570.2 (1)Expansion project (on page 3.1A)*
- *Apply ornamentation and fenestration standards for all street facing facades including those facing Hwy 78*
- *Require alternatives to internally illuminated signs on building faces but, allow on monument and directional signs.*

Background: This is a request for a planned commercial district which would allow greater flexibility in the development pattern of this large project which would otherwise be prohibited or restricted by The City of Monroe Zoning Ordinance.

Attachment(s):

Monroe Pavilion Pattern design book



City of Monroe
215 N. Broad Street
Monroe, GA 30655
(770)207-4674

Plan Report

Plan NO.: RZ-000008-2019

Plan Type: Re-Zoning Request All Types

Work Classification: Request for Rezone

Plan Status: Issued

Apply Date: 05/06/2019

Expiration:

Location Address

0 CHARLOTTE ROWELL BLVD, MONROE, GA 30656

Contacts

MAB AMERICAN Applicant
525 N TRYON S STE 1600, CHARLOTTE, NC 28202
(334)462-8851 JOHN@MABAMERICAN.COM

Description: REQUEST FOR REZONE FROM B3 TO PCD-P&Z MTG 6/18/19 @ 5:30 PM -
COUNCIL MTG 7/2/19 @ 6:00 PM 215 N BROAD ST

Valuation: \$0.00
Total Sq Feet: 0.00

Fees	Amount
Commercial Rezone or Variance Fee	\$200.00
Total:	\$200.00

Payments	Amt Paid
Total Fees	\$200.00
Credit Card	\$200.00
Amount Due:	\$0.00

<u>Condition Name</u>	<u>Description</u>	<u>Comments</u>
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Issued By:

Plan_Signature_1

May 06, 2019

Date

Date

Plan_Signature_2

Date

REZONE APPLICATION FORM

PERMIT NUMBER _____

I. LOCATION US Highway 78 AT CHARLOTTE ROWELL RD

COUNCIL DISTRICT 3RD OF WALTON COUNTY

LAND LOTS

MAP NUMBER 40 AND 63, 3RD DISTRICT

PARCEL NUMBER M0050045

II. PRESENT ZONING B3 REQUESTED ZONING PCD

III. ACREAGE 95.48 PROPOSED USE COMMERCIAL

Contract
(Purposes)

IV. OWNER OF RECORD MAB AMERICAN MANAGEMENT, LLC

ADDRESS 525 N. TRACON ST. SUITE 1600

PHONE NUMBER 334.462.8851

The following information must be supplied by the applicant. (attach additional pages if needed)

V. ANALYSIS:

1. A description of all existing uses and zoning of nearby property

B3 AND B-2

2. Description of the extent to which the property value of the subject property is diminished by the existing zoning district classification

WE CANNOT DEVELOP THIS PROPERTY WITH OUR MAJOR TENANTS UNDER B-3. REFER TO OUR SUBMITTED PATTERN BOOK

3. The existing value of the property contained in the petition for rezoning under the existing zoning classification

MARKET VALUE FOR TYPE OF PROJECT

4. The value of the property contained in the application for rezoning under the proposed zoning Classification

MARKET VALUE FOR TYPE OF PROJECT

5. A description of the suitability of the subject property under the existing zoning classification

WITH THIS PROPERTY ZONED B-3 WE CAN NOT BUILD OUR PROJECT THE BIG BOX TENANTS REQUIREMENTS DO NOT FIT WITHIN B-3

6. A description of the suitability of the subject property under the proposed zoning classification of the property

WE USE REZONING TO PCD WE CAN ADOPT MOST OF B3 REQUIREMENT BUT CAN ALTER SOME OF THE REQUIREMENTS TO WORK W/ OUR MAJOR TENANTS. OUR SUBMITTED PATTERN BOOK GOES INTO DETAIL ABOUT THESE REQUIREMENTS.

Rezoning Application
Page Three (3)

Wherefore, applicant prays that the procedures incident to the presentation of this petition be taken, and the property be rezoned accordingly.

5/6/19
OWNER'S PUBLIC AGENT
Owner of property (signature) _____
Address M43 American Parkway the 505 N Tryon St. Suite 1000
Phone Number 704-331-6587 Charlotte, NC 28202
Attorney/Agent (signature) [Signature]
Address _____
Phone Number _____

Personally appeared before me the above applicant named Rick Maxian who on oath says that he/she is the Agent for the foregoing, and that all the above statements are true to the best of his/her knowledge.

Jean Norton (Notary Public) 5/6/2019 (Date)
My Commission Expires 4/19/2021



7. A description of any existing use of property including a description of all structures presently occupying the property UNDEVELOPED FOR MOST OF SITE. SMALL EXISTING BUILDING ON SO HWY 11 THAT WILL BE REMOVED.
8. The length of time the property has been vacant or unused as currently zoned N/A - UNDEVELOPED PROPERTY
9. A detailed description of all efforts taken by the property owner(s) to use the property or sell the property under the existing zoning classification WE ARE IN PROCESS OF PURCHASING PROPERTY. THE OWNER WENT BACK TO AN TAX MOTEL TENANTS AND JUNIOR ANCHORS TO TRY TO MAKE THEM WORK w/ THE B-3 REQUIREMENTS. MOTEL/JUNIOR ANCHORS WOULD NOT ACCEPT THOSE REQUIREMENTS

Applications found to be incomplete or incorrect will be rejected. See the attached calendar for deadline dates. It is the responsibility of the applicant and not the staff to ensure that a complete and accurate application is submitted.

LEGAL DESCRIPTION OF PROPERTY

EXHIBIT A ATTACHED

Rezoning Application
Page Four (4)

What method of sewage disposal is planned for the subject property?

Sanitary Sewer Septic Tank

The following information must be included in the application material requesting an annexation or zoning change from B3 to PCD located at US Highway 78, containing 95.48 acre(s), property owner being MAB AMERSON MANAGEMENT, LLC filed on _____

(Purchasing Property)

CHECK LIST - APPLICATION MATERIAL

- Application Fee (\$100.00 Application Fee Single Family Rezoning)
(\$300.00 Application Fee Multi Family Rezoning)
(\$200.00 Application Fee Commercial Rezoning)
(Application fee For Annexation is the same as a Rezone)
- The completed application form (one original with original signatures)
- Special Conditions made part of the rezoning/annexation request
- Legal Description
- Survey plat of property showing bearings and distances and:
 - abutting property owners
 - the zoning of abutting property
 - the current zoning of the subject property
- Development Plan (two full size and one 11x17)
- Site plan of the property at an appropriate scale
 - the proposed use
 - internal circulation and parking (proposed number of parking spaces)
 - landscaping minimum square footage of landscaped area
 - grading
 - lighting
 - drainage (storm water retention structures)
 - amenities (location of amenities)
 - buildings (maximum gross square footage and height of structures)
 - buffers
 - Additional information that may be required by the Code Enforcement Officer:

____ Monroe Utilities Network Availability Letter

Application Material-Section 1421.4 of the Zoning Ordinance outlines the specific items to be included on the site plan:

EXHIBIT A

MONROE PAVILION LAND DESCRIPTION

ALL THAT TRACT OR PARCEL OF LAND LYING AND BEING IN LAND LOTS 40 AND 63 OF THE 3RD DISTRICT OF WALTON COUNTY GEORGIA AND MORE PARTICULARLY DESCRIBED AS FOLLOWS:

COMMENCE AT A R/W MONUMENT AT THE SOUTHWESTERN END OF THE MITER OF THE R/W OF GEORGIA HIGHWAY 11 AND THE NORTHERLY R/W OF US HIGHWAY 78 THENCE ALONG US HIGHWAY 78 (R/W S73°01'41"W A DISTANCE OF 203.67' TO A POINT; THENCE ALONG A CURVE TO THE LEFT 317.68' HAVING A RADIUS OF 939.08' AND SUBTEND BY A CHORD OF S63°33'37"W AND DISTANCE OF 316.17' TO A 1" GALVANISED OPEN TOP PIPE FOUND **THE TRUE POINT OF BEGINNING**; ALONG THE R/W OF US 78 THE FOLLOWING COURSES S50°29'12"W A DISTANCE OF 129.13' TO A POINT; THENCE S47°16'54"W A DISTANCE OF 105.01' TO A R/W MONUMENT FOUND; THENCE S55°17'08"W A DISTANCE OF 251.83' TO A R/W MONUMENT FOUND; THENCE S62°19'32"W A DISTANCE OF 168.85' TO A R/W MONUMENT FOUND; THENCE S60°45'46"W A DISTANCE OF 133.57' TO A 1" OPEN TOP PIPE (DISTURBED) FOUND; THENCE S25°48'10"E A DISTANCE OF 9.36' TO A R/W MONUMENT (DISTURBED) FOUND; THENCE S60°59'06"W A DISTANCE OF 638.93' TO A R/W MONUMENT FOUND; THENCE N28°58'31"W A DISTANCE OF 9.00' TO A POINT NEAR A R/W MONUMENT FOUND; THENCE ALONG A CURVE TO THE RIGHT 355.90' HAVING A RADIUS OF 1382.39' AND SUBTENDED BY A CHORD OF S68°24'00"W A DISTANCE OF 354.92' TO A POINT NEAR A R/W MONUMENT FOUND; THENCE N14°13'28"W A DISTANCE OF 10.00' TO A POINT NEAR A R/W MONUMENT FOUND; THENCE ALONG A CURVE TO THE RIGHT 401.19' HAVING A RADIUS OF 1372.39' AND SUBTENDED BY A CHORD OF S84°09'00"W AND DISTANCE OF 399.76' TO A R/W MONUMENT FOUND; THENCE N87°28'31"W A DISTANCE OF 681.27' TO A POINT NEAR A R/W MONUMENT FOUND; THENCE S02°31'29"W A DISTANCE OF 10.00' TO A POINT NEAR A R/W MONUMENT FOUND; THENCE N74°40'22"W A DISTANCE OF 121.85' TO A R/W MONUMENT FOUND; THENCE N71°01'09"W A DISTANCE OF 197.61' TO A R/W MONUMENT FOUND; THENCE N59°56'09"W A DISTANCE OF 111.80' TO A R/W MONUMENT FOUND AT THE MITER OF THE R/W OF US HIGHWAY 78 AND THE R/W OF GEORGIA HIGHWAY 138; THENCE N15°22'59"W A DISTANCE OF 30.40' TO A R/W MONUMENT FOUND ON THE R/W OF GEORGIA HIGHWAY 138; THENCE ALONG THE R/W OF GEORGIA HIGHWAY 138 THE FOLLOWING COURSES; N15°06'52"E A DISTANCE OF 57.11' TO A R/W MONUMENT FOUND; THENCE N10°50'21"E A DISTANCE OF 148.01' TO A R/W MONUMENT FOUND; THENCE N02°50'25"E A DISTANCE OF 105.67' TO A R/W MONUMENT FOUND; THENCE N02°27'12"E A DISTANCE OF 140.44' TO A R/W MONUMENT FOUND; THENCE ALONG A CURVE TO THE LEFT 135.22' HAVING A RADIUS OF 1375.00' AND SUBTENDED BY A CHORD OF N05°19'56"W A DISTANCE OF 135.16' TO A R/W MONUMENT FOUND; THENCE N08°08'55"W A DISTANCE OF 334.16' TO A POINT; THENCE N81°51'05"E A DISTANCE OF 51.00' TO A R/W MONUMENT FOUND; THENCE N08°08'55"W A DISTANCE OF 150.00' TO A R/W MONUMENT FOUND; THENCE S81°51'05"W A DISTANCE OF 51.00' TO A POINT; THENCE N05°54'28"W A DISTANCE OF 230.18' TO A POINT NEAR A R/W MONUMENT FOUND; THENCE LEAVING GEORGIA HIGHWAY 138 R/W N63°58'20"E A DISTANCE OF 1850.41' TO A 1" OPEN TOP PIPE FOUND; THENCE S38°30'16"E A DISTANCE OF 101.94' TO AN AXLE FOUND; THENCE S31°31'35"E A DISTANCE OF 192.91' TO A ½" OPEN TOP PIPE FOUND; THENCE S30°46'42"E A DISTANCE OF 517.35' TO A IPS, THENCE N66°20'44"E A DISTANCE OF 371.58' TO A 1" STEEL OPEN TOP PIPE FOUND; THENCE N57°19'54"E A DISTANCE OF 250.77' TO A ½" OPEN TOP PIPE FOUND; THENCE N48°14'44"E A DISTANCE OF 104.12' TO A ½" OPEN TOP PIPE FOUND; THENCE N31°14'10"W A DISTANCE OF 50.08' TO A ½" CRIMP TOP PIPE FOUND; THENCE N62°39'33"E A DISTANCE OF 379.10' TO A POINT ON THE R/W OF GEORGIA HIGHWAY 11; THENCE ALONG GEORGIA HIGHWAY 11 S29°37'20"E A

EXHIBIT A CONT.

DISTANCE OF 151.18' TO A 1" OPEN TOP PIPE (GALVINIZED) FOUND; THENCE LEAVING GEORGIA HIGHWAY 11 S52°35'58"W A DISTANCE OF 609.73' TO A ½" REBAR FOUND; THENCE S29°05'23"E A DISTANCE OF 451.11' TO A ½" OPEN TOP PIPE FOUND; THENCE N62°01'22"E A DISTANCE OF 318.75' TO A 5/8" ROD FOUND; THENCE N62°01'22"E A DISTANCE OF 292.13' TO AN IPS ON THE R/W OF GEORGIA HIGHWAY 11; THENCE ALONG GEORGIA HIGHWAY 11 S29°34'37"E A DISTANCE OF 208.52' TO A ½" OPEN TOP PIPE FOUND; THENCE LEAVING GEORGIA HIGHWAY 11 S61°13'49"W A DISTANCE OF 624.23' TO A SCAPE BLADE FOUND; THENCE S38°23'22"E A DISTANCE OF 115.03' TO A ½" REBAR W/CAP FOUND; THENCE S38°23'22"E A DISTANCE OF 116.11' TO A 1" OPEN TOP PIPE (GALVINIZED) **AT THE TRUE POINT OF BEGINNING.**

For any application for P, B-1, B-2, B-3 or M-1 districts the site plan shall identify: (circle the appropriate district applied for):

- the maximum gross square footage of building area
- the maximum lot coverage of building area
- the minimum square footage of landscaped area
- the maximum height of any structure
- the minimum square footage of parking and drive areas
- the proposed number of parking spaces

For any application for the R-1, R-1A, R-2 or MH districts the site plan shall additionally identify: (circle the appropriate district applied for)

- the maximum number of residential dwelling units
- the minimum square footage of heated floor area for any residential dwelling unit
- the maximum height of any structure
- the minimum square footage of landscaped area
- the maximum lot coverage of building area
- the proposed number of parking spaces
- on all rezoning applications a revised site plan to be approved at a later date by the Mayor and City Council may be required
- yes no Applicant site plan indicates a variance requested
- for any application for multi-family residential uses, the site plan shall also identify the maximum height of any structure, location of amenities, and buffer areas: and,
- any other information as may be reasonably required by the Code Enforcement Officer.

Any applicant requesting consideration of a variance to any provision of the zoning ordinance as shown on the required site plan shall identify the variance(s) and identify for each variance shown the following information which shall confirm that the following condition(s) exist:

1. Any information which identifies that there are extraordinary and exceptional conditions pertaining to the particular piece of property in question because of its size, shape or topography that are not applicable to other lands or structures in the same district.
2. Any information whereby a literal interpretation of the provisions of this Ordinance would deprive the applicant of rights commonly enjoyed by other properties of the district in which the property is located.
3. Any information supporting that granting the variance requested will not confer upon the property of the applicant any special privileges that are denied to other properties of the district in which the applicant's property is located.
4. Information clearly showing that the requested variance will be in harmony with the purpose and intent of this Ordinance and will not be injurious to the neighborhood or to the general welfare.
5. Information that the special circumstances are not the result of the actions of the applicant.
6. A description of how the variance requested is the minimum variance that will make possible the legal use of the land, building, or structure in the use district proposed.
7. Information indicating the variance is not a request to permit a use of land, buildings, or structures, which are not permitted by right in the district involved.

Rezoning Application

**NOTICE TO THE PUBLIC
CITY OF MONROE**

**A petition has been filed with the
City of Monroe requesting the
property at 0 Charlotte Rowell Blvd to
be rezoned from B3 to PCD**

**A public hearing will be held before
the Monroe Planning and Zoning
Commission at City Hall Auditorium at
215 N. Broad Street on June 18, 2019
at 5:30 P.M. All those having an
interest should be present to voice
their interest.**

**A petition has been filed with the
City of Monroe requesting the
property at 0 Charlotte Rowell Blvd to
be rezoned from B3 to PCD**

**A public hearing will be held before
The Mayor and City Council
at the City Hall Auditorium at
215 N. Broad Street on July 2, 2019
at 6:00 P.M. All those having an
interest should be present to voice
their interest.**

**PLEASE RUN ON THE
FOLLOWING DATE:**

June 2, 2019

TRAFFIC ENGINEERING STUDY

Prepared For

MAB American Management, LLC

MONROE PAVILION

MONROE, GA

May 22, 2018



Report Submitted: June 4, 2019

Prepared For:

Mr. John Argo
Principal Developer
MAB American Management, LLC
525 N. Tyron Street, Suite 1600
Charlotte, NC 28202
john@mabamerican.com

Prepared By:

Speedy Boutwell, PE, PTOE
Wilburn Engineering
55 Millard Farmer Industrial Boulevard
Newnan, GA 30263
678.423.0050
speedy@wilburnengineering.com

Additional investigation by:
Mallory Maldino

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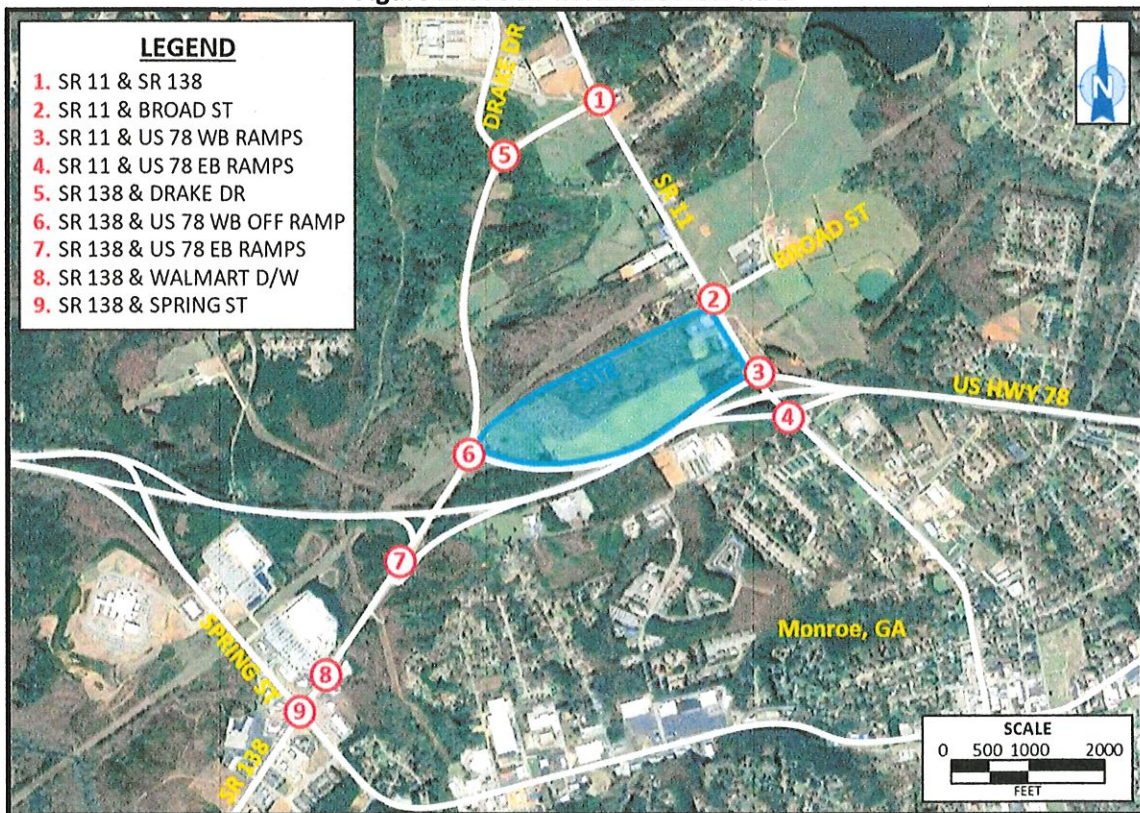
INTRODUCTION

This study includes an analysis of the impact expected from a proposed multi-use development in Monroe, Georgia. The project location is shown in Figure 1. The intersections included in the study are shown in Figure 2.

Figure 1: PROJECT LOCATION MAP



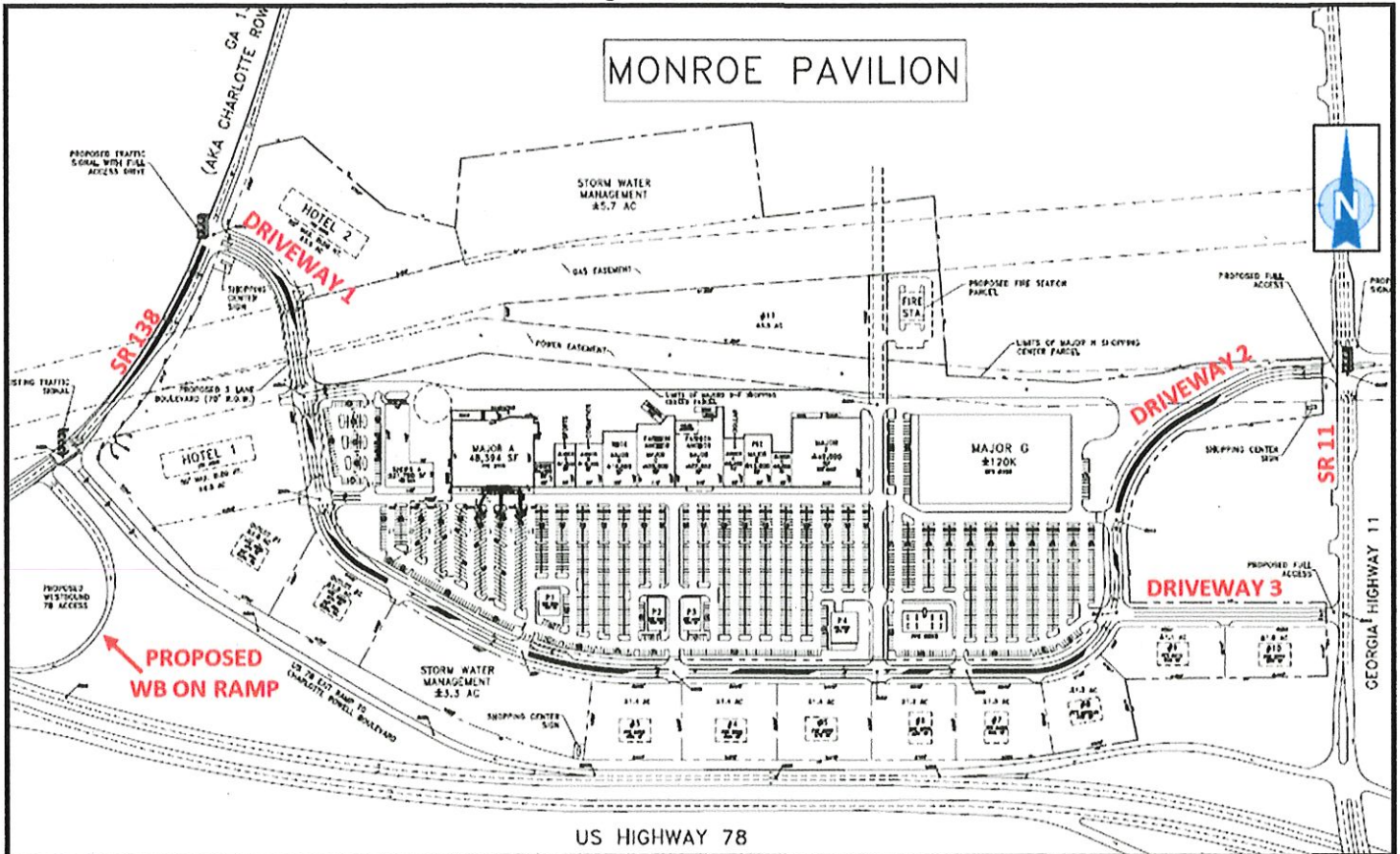
Figure 2: STUDY INTERSECTION MAP



PLANNED DEVELOPMENT

The proposed development is a multi-use shopping and commerce center, including hotels, large retail generators, and various other uses. The proposed site plan is shown in Figure 3. A larger version of the site plan is provided in Appendix A.

Figure 3: SITE PLAN



Also proposed in the project are modifications to the interchange of US 78 and SR 138. A westbound on ramp is proposed to be constructed, as shown in the site plan above. Modifications are also proposed at the US Highway 78 eastbound ramp junctions to accommodate vehicles originating in the west on US Highway 78 and wishing to travel north on SR 138. Various design concepts to accommodate this movement are analyzed later in the report.

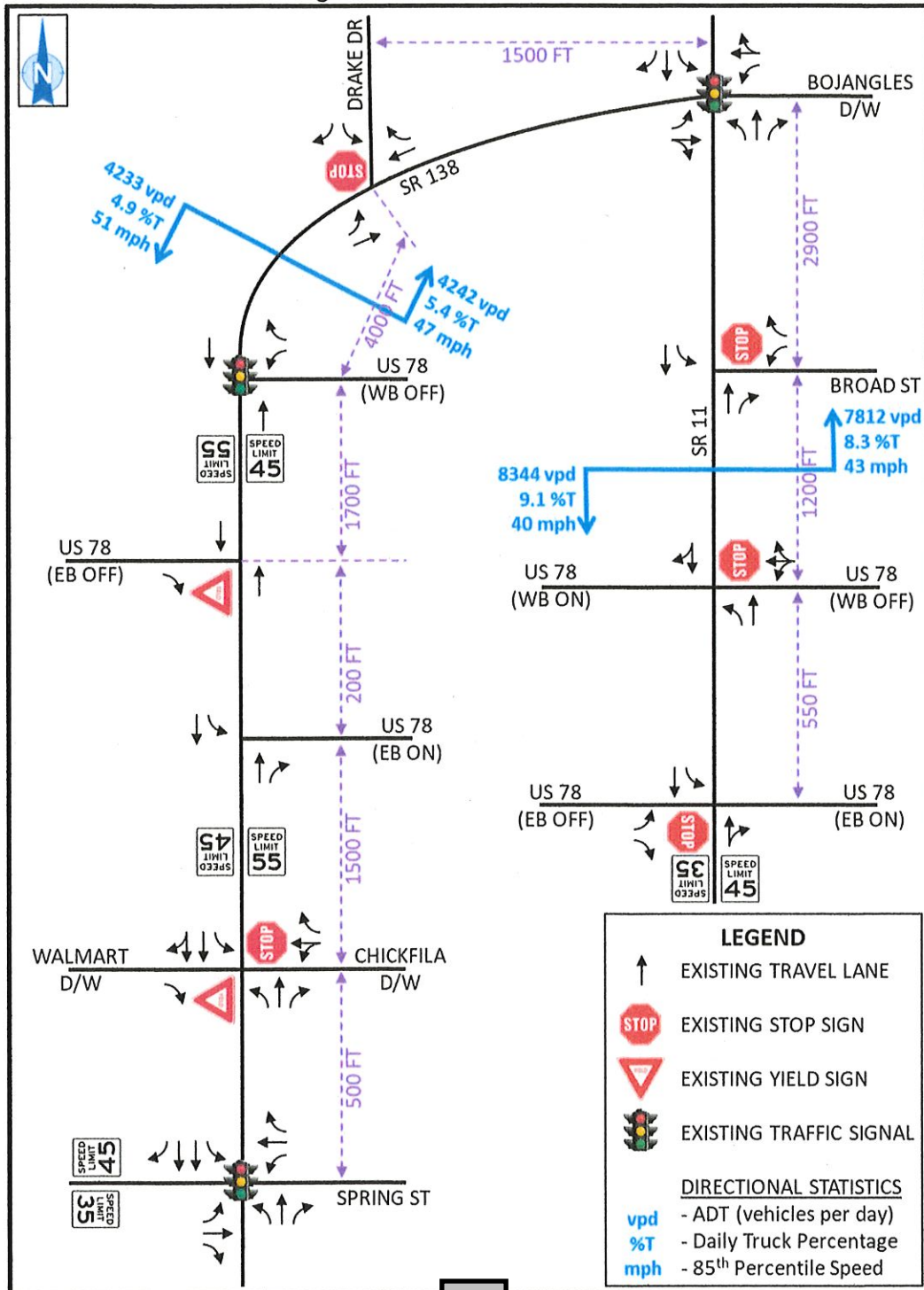
EXISTING CONDITIONS

An inventory was conducted of the current conditions in the study area, including roadway geometry, traffic control, and traffic volumes.

INVENTORY OF EXISTING GEOMETRY AND TRAFFIC CONTROL

The existing roadway geometry and traffic control in the study area is shown in Figure 4.

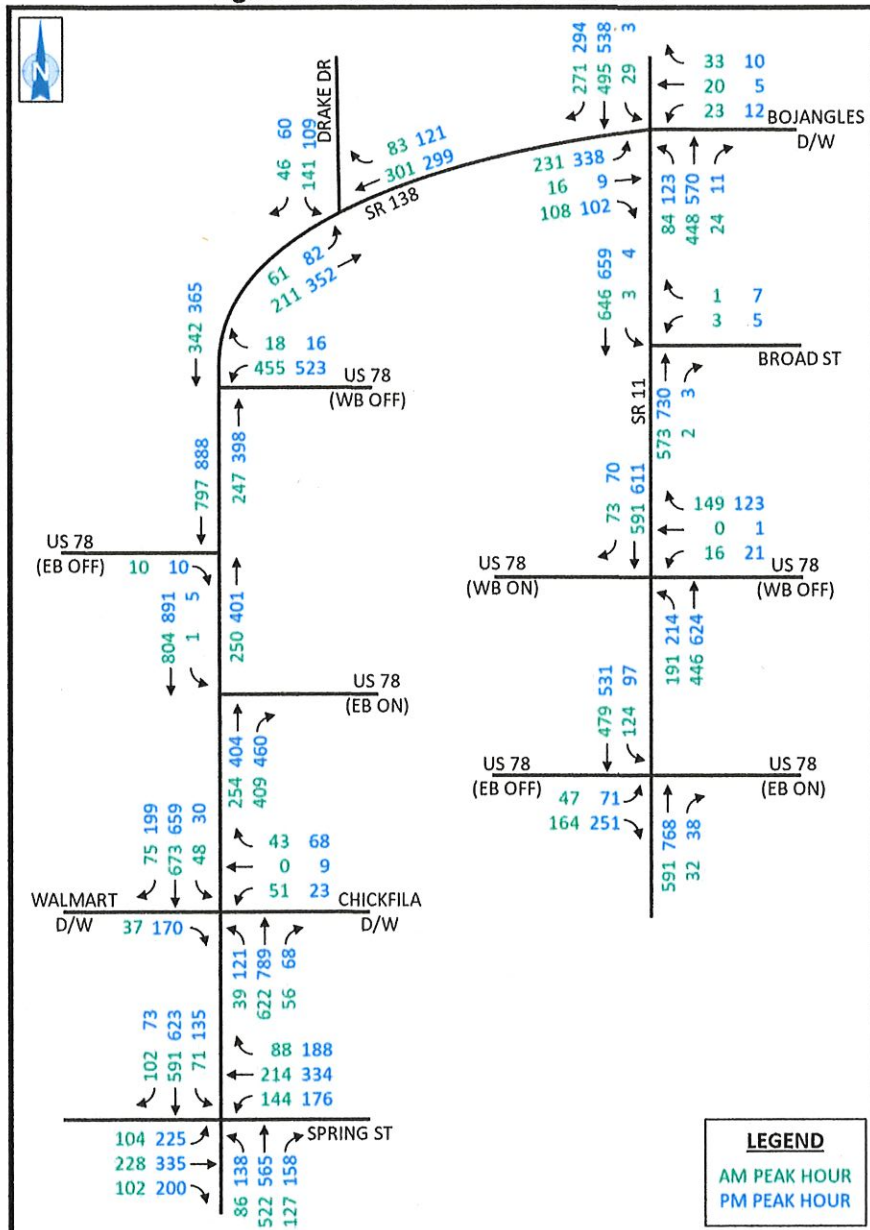
Figure 4: EXISTING CONDITIONS



TURNING MOVEMENT VOLUMES

Turning Movement Counts (TMC's) were conducted at the study intersections on Tuesday, February 13, 2018 from 7:00 to 9:00 AM and 2:00 to 6:00 PM. The AM and PM Peak Hours within the study area were determined to be 7:15 to 8:15 AM and 4:45 to 5:45 PM, respectively. Existing Peak Hour turning movement volumes are shown in Figure 5. Turning movement data is provided in Appendix B.

Figure 5: EXISTING TRAFFIC VOLUMES



DAILY VOLUMES

Automatic Traffic Recordings (ATR's) were taken at 4 locations throughout the study area for 24 hours, starting at 12:00 AM on Tuesday, February 13, 2018. Class and speed data were collected during the 24-hour ATR's. ATR data is provided in Appendix C.

PROJECTED CONDITIONS

Projected conditions include the new development, all geometric modifications included in the development site plan, and the traffic generated by the development superimposed onto the existing traffic.

TRIP GENERATION

The trips generated by the development were estimated using trip generation rates found in ITE's publication *Trip Generation*, 9th Edition, and *TripGen 10* software from Trafficware. The land uses included in the development, and the trips generated by each, are listed in Table 1. The specific land uses included in the development were unknown at the time of the study, therefore a wide variety of common land uses was assumed. Trip generation reports are provided in Appendix D.

Estimated pass-by trips expected to be drawn from SR 11 and SR 138 were limited to 15% of the adjacent street volume. Diverted link trips expected to be drawn from US Highway 78 were limited to 10% of the volume on US Highway 78. The existing volume on US Highway 78 was obtained from the most recent data available for the nearest GDOT Geocount stations.

Table 1: MONROE PAVILION TRIP GENERATION

ITE CODE	LAND USE	SIZE	DAILY TRIPS	AM PEAK HOUR			PM PEAK HOUR		
				ENTER	EXIT	TOTAL	ENTER	EXIT	TOTAL
310	Hotel	176 Rooms	1438	55	38	93	54	52	106
820	Shopping Center	215 KSF	11169	154	95	249	480	521	1001
850	Supermarket	48 KSF	4908	101	62	163	232	223	455
862	Home Improvement Superstore	120 KSF	3689	102	77	179	137	143	280
879	Arts & Crafts Store	40 KSF	2262	0	0	0	114	134	248
SUBTOTAL NEW TRIPS			23466	412	272	684	1017	1073	2090
LESS INTERNAL CAPTURE			-372	-5	-5	-10	-17	-17	-34
LESS PASS-BY TRIPS			-2944	0	0	0	-174	-174	-348
LESS DIVERTED LINKS			-2267	0	0	0	-134	-134	-268
GRAND TOTAL NEW TRIPS			17883	407	267	674	692	748	1440

The ITE 820 code for "Shopping Center" was used for approximately one-third of the site. The site will largely operate as a unit in which large joint parking areas will allow vehicles to access multiple land uses on the same trip. Square footage allotted for land uses that have yet to be determined was included in this ITE code.

TRIP DISTRIBUTION

In order to assign generated trips to the roadway network, it was necessary to determine how new trips to and from the site should be distributed. A distribution pattern was developed to define the origin and destination of new trips.

The majority of trip-making associated with commercial land use is home-based on one end. The distribution of the population within a reasonable driving distance of the site and the locations of commercial centers in surrounding cities were used to develop a market area by which to assign newly generated trips. Figure 6 shows the market area superimposed onto the census tracts. Census data is provided in Appendix E.

The market area was defined by the distance that can be travelled to and from the site within approximately ten minutes. The market area was divided into five sectors (A, B, C, D, and E) that correspond to the travel routes to and from the site. The distribution of the market area within the five sectors establishes the basis for distributing the generated traffic. The market distribution is summarized in Table 2 on the following page.

Figure 6: MARKET AREA

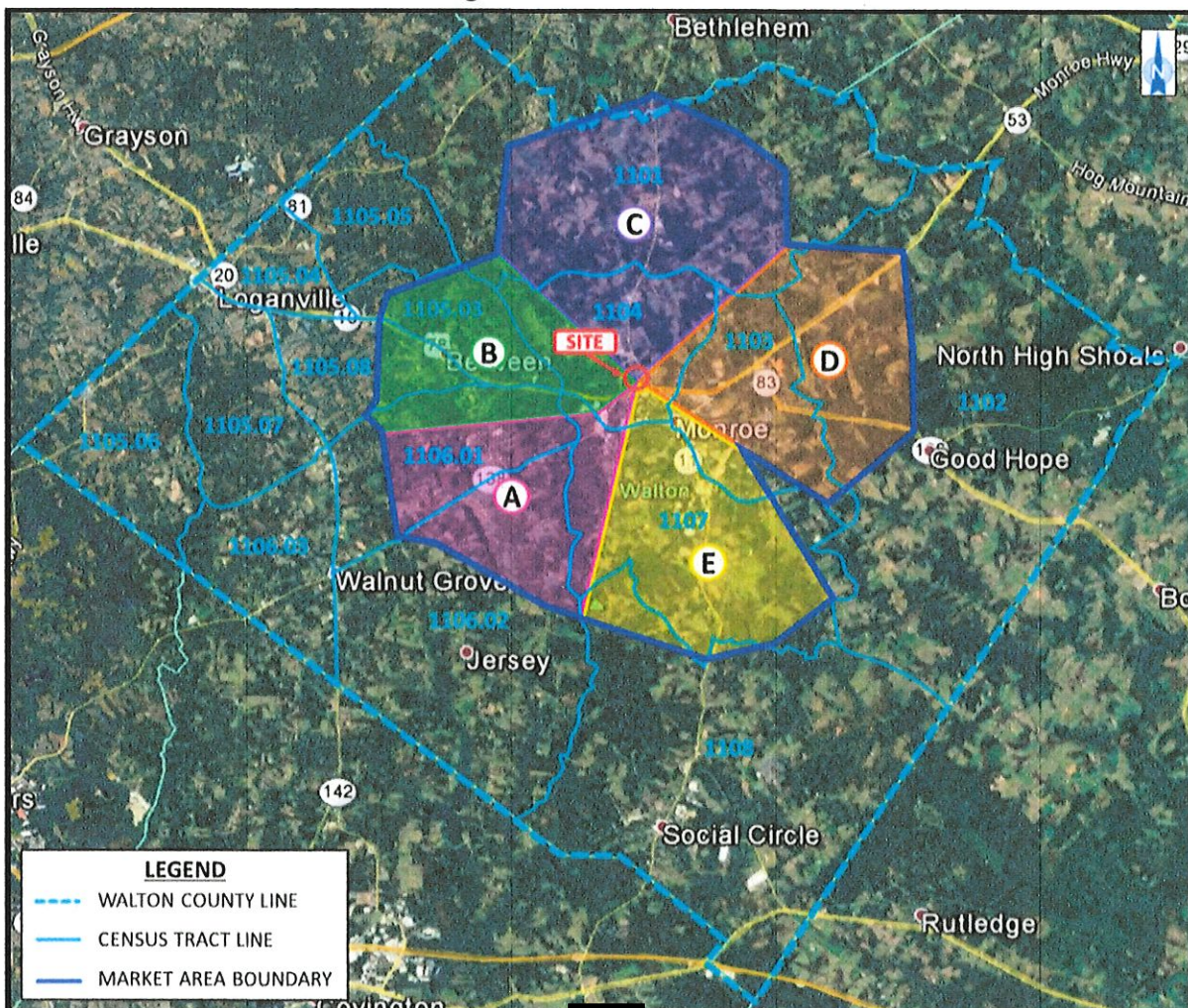
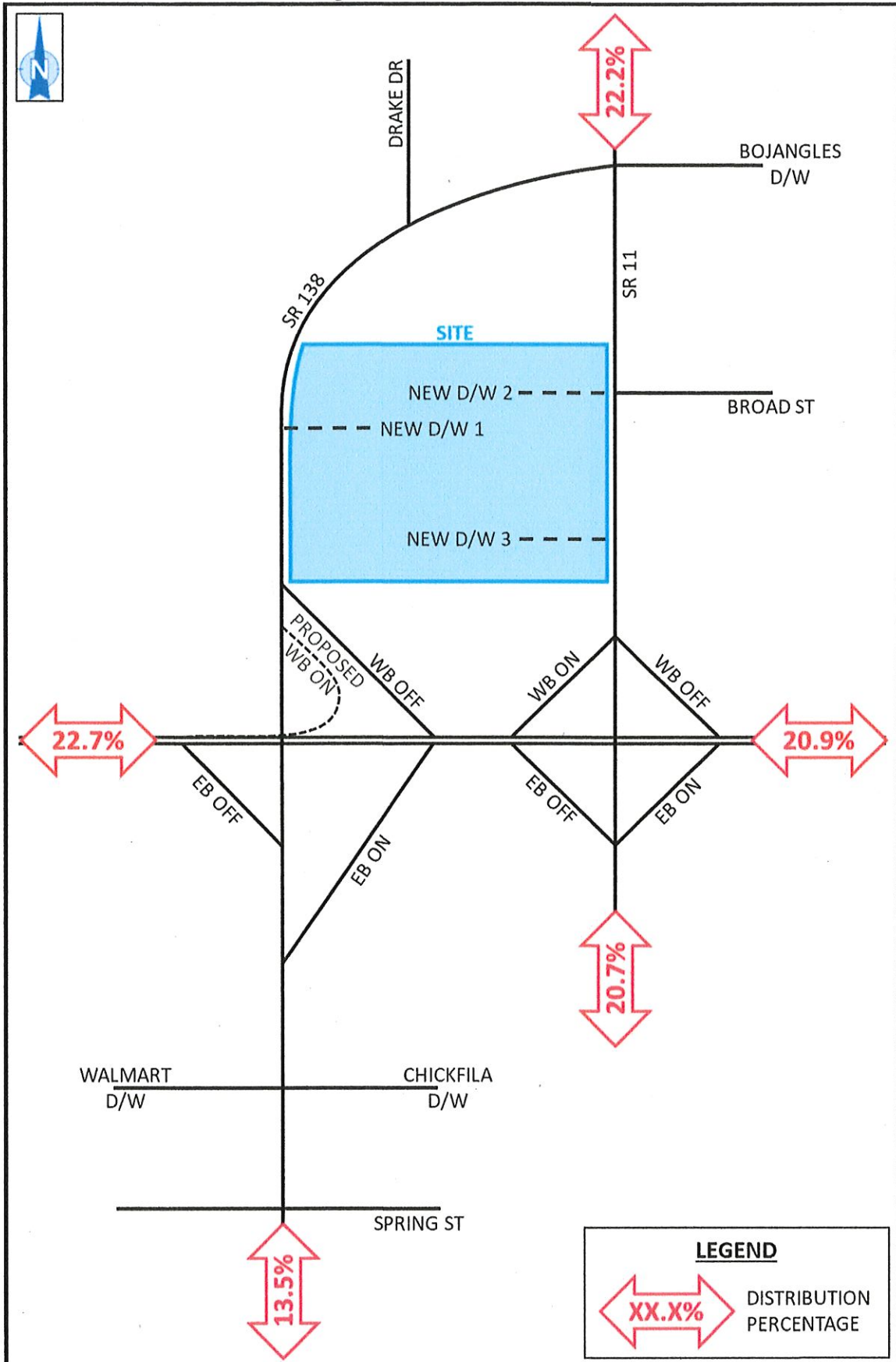


Table 2: MARKET AREA DISTRIBUTION

SECTOR	CENSUS TRACT	TOTAL POP. OF TRACT	% OF TRACT IN SECTOR	POP. OF TRACT IN SECTOR	TOTAL SECTOR POP.	% OF MARKET AREA
A	1104	6,351	5%	318	4,588	13.5
	1106.01	4,764	40%	1,906		
	1106.02	5,076	25%	1,269		
	1107	7,305	15%	1,096		
B	1101	8,454	3%	254	7,718	22.7
	1104	6,351	30%	1,905		
	1105.03	3,355	80%	2,684		
	1105.08	4,661	33%	1,538		
	1106.01	4,764	25%	1,191		
1107	7,305	2%	146			
C	1101	8,454	50%	4,227	7,540	22.2
	1103	6,875	2%	138		
	1104	6,351	50%	3,176		
D	1101	8,454	5%	423	7,105	20.9
	1102	4,457	20%	891		
	1103	6,875	75%	5,156		
	1104	6,351	10%	635		
E	1103	6,875	15%	1,031	7,030	20.7
	1104	6,351	5%	318		
	1107	7,305	75%	5,479		
	1108	6,739	3%	202		
TOTAL					33,981	

Based on the market area described in Figure 6 and Table 2, the distribution shown in Figure 7 on the following page was developed for assigning new commercial trips and pass-by trips to the roadway.

Figure 7: TRIP DISTRIBUTION



TRIP ASSIGNMENT

Once it was determined what portion of new trips to the site would originate from which market sectors, it was necessary to make assumptions as to what route vehicles might take within the study area in order to get to and from the site.

The proposed westbound on ramp and modified intersection of the eastbound off ramp at the interchange of US 78 and SR 138 were included in the assignment of generated new trips.

The assumptions made in order to assign new trips within the study area are as follows:

- New trips originating from or bound for the southwest via SR 138 (Market Area A) will enter or exit via Driveway 1.
- Of new trips originating from or bound for the north via SR 11 (Market Area C), 50% will enter or exit via Driveway 1, 25% will enter or exit via Driveway 2, and 25% will enter or exit via Driveway 3.
- Of new trips originating from or bound for the west via US 78 (Market Area B), 50% will enter or exit via Driveway 1, 25% will enter or exit via Driveway 2, and 25% will enter or exit via Driveway 3.
- Of new trips originating from or bound for the south via SR 11 (Market Area E), 50% will enter and exit via Driveway 2 and 50% will enter and exit via Driveway 3.
- Of new trips originating from or bound for the east via US 78 (Market Area D), 50% will enter or exit via Driveway 1, 25% will enter or exit via Driveway 2, and 25% will enter or exit via Driveway 3.

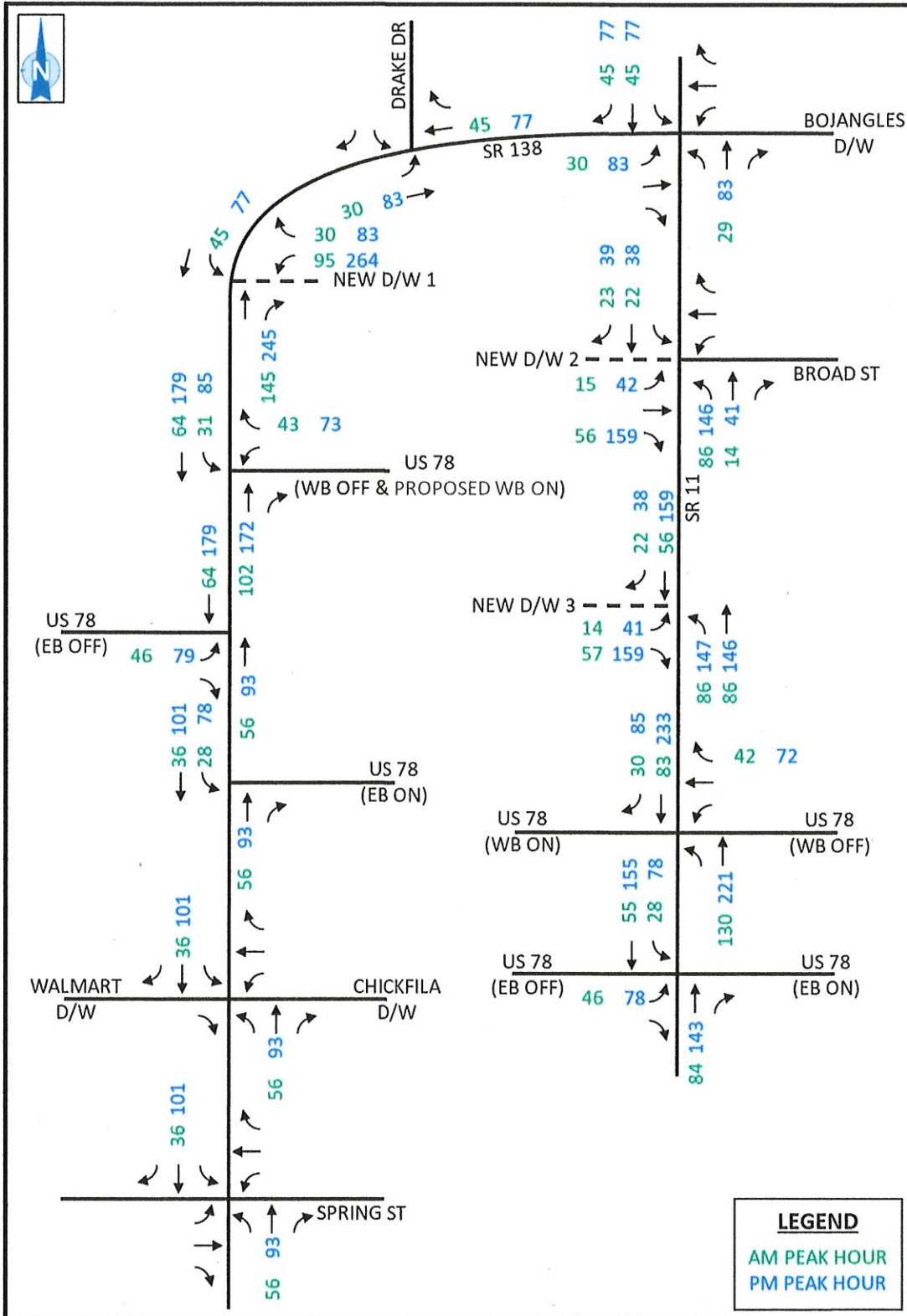
The assumptions made in order to assign pass-by trips and diverted link trips to the study area are as follows:

- Of pass-by trips originating in the north on SR 11 (Market Area C), 50% will enter or exit using Driveway 2 and 50% will enter or exit using Driveway 3, all of which will continue traveling south on SR 11.
- Of pass-by trips originating in the southwest using SR 138 (Market Area A), 100% will enter using Driveway 1. Of pass-by trips originating in the southwest on SR 138 (Market Area A), 50% will exit using Driveway 1 and continue traveling north on SR 138, 25% will exit using Driveway 1 and travel west on US Highway 78, and 25% will exit using Driveway 1 and travel east on US Highway 78.
- Of pass-by trips originating in the south on SR 11 (Market Area E), 50% will enter using Driveway 2 and 50% will enter using Driveway 3. 100% of pass-by trips originating in the south on SR 11 (Market Area E) will exit using Driveway 2 and continue traveling north on SR 11.
- Of diverted link trips originating in the east on US Highway 78 (Market Area D), 50% will enter or exit using Driveway 1, 25% will enter or exit using Driveway 2, and 25% will enter or exit using Driveway 3, all of which will continue traveling west.
- Of diverted link trips originating in the west on US Highway 78 (Market Area B), 50% will enter or exit using Driveway 1, 25% will enter or exit using Driveway 2, and 25% will enter or exit using Driveway 3, all of which will continue traveling east.

New Trips

The generated new trips are shown in Figure 8. These trips were assigned in accordance with the distribution and assumptions listed on the previous pages.

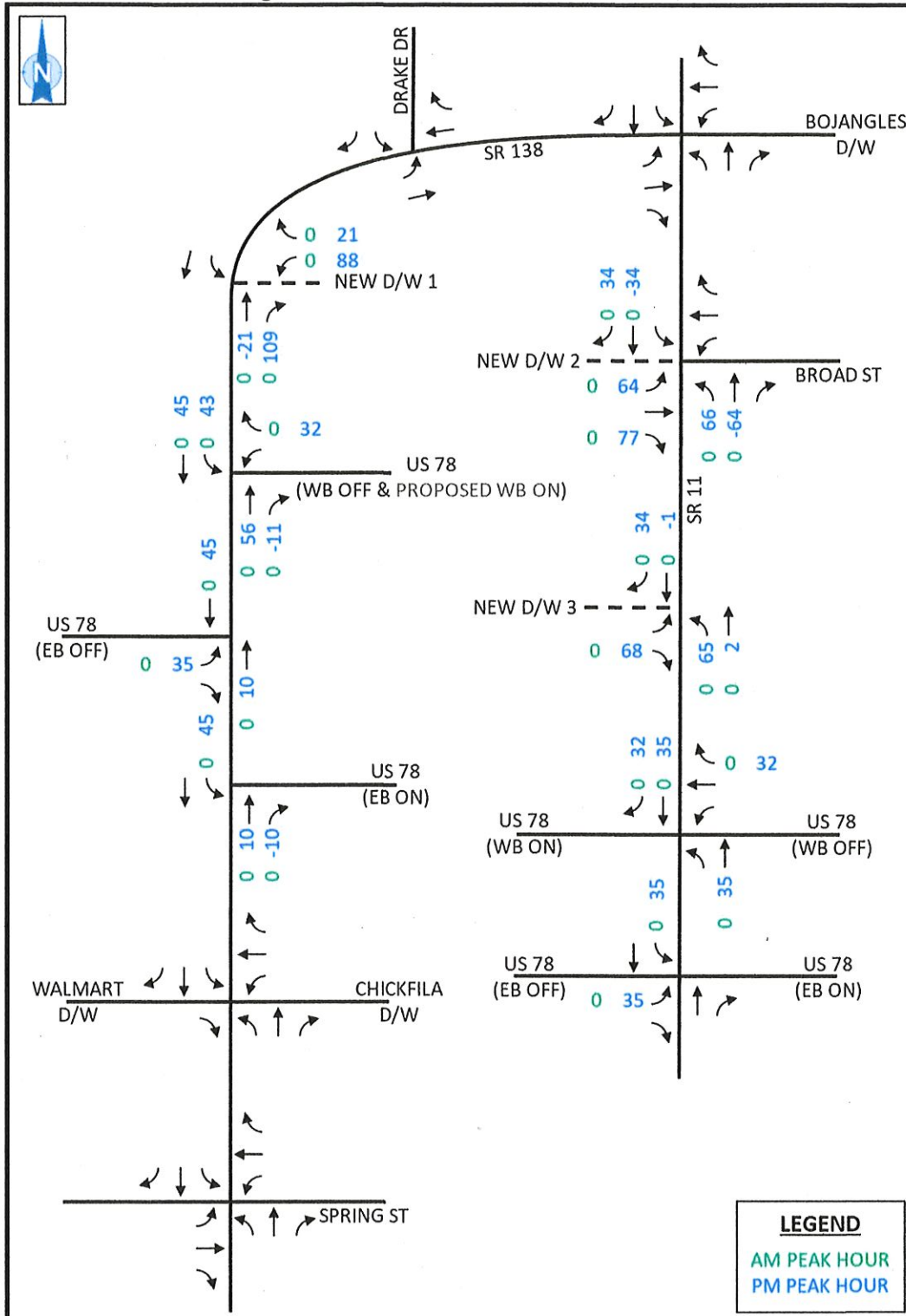
Figure 8: NEW TRIPS



Pass-By & Diverted Link Trips

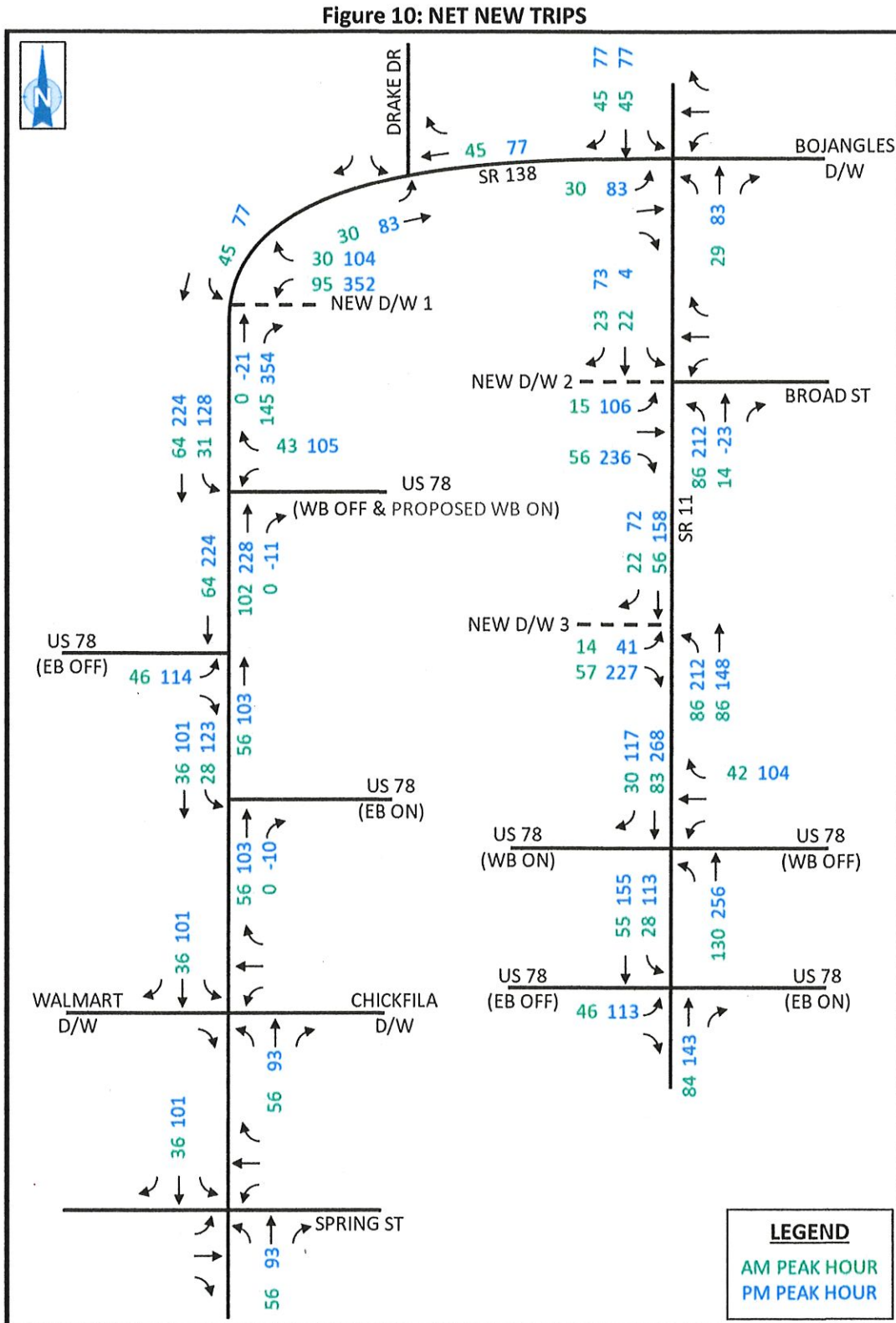
The generated pass-by and diverted link trips are shown in Figure 9. Pass-by trips are drawn from the existing traffic stream of the adjacent street and are not added as new trips to the roadway network. Diverted link trips are drawn from the existing traffic stream of a nearby street, but are rerouted so as to create new trips on streets adjacent to the development.

Figure 9: PASS-BY & DIVERTED LINK TRIPS



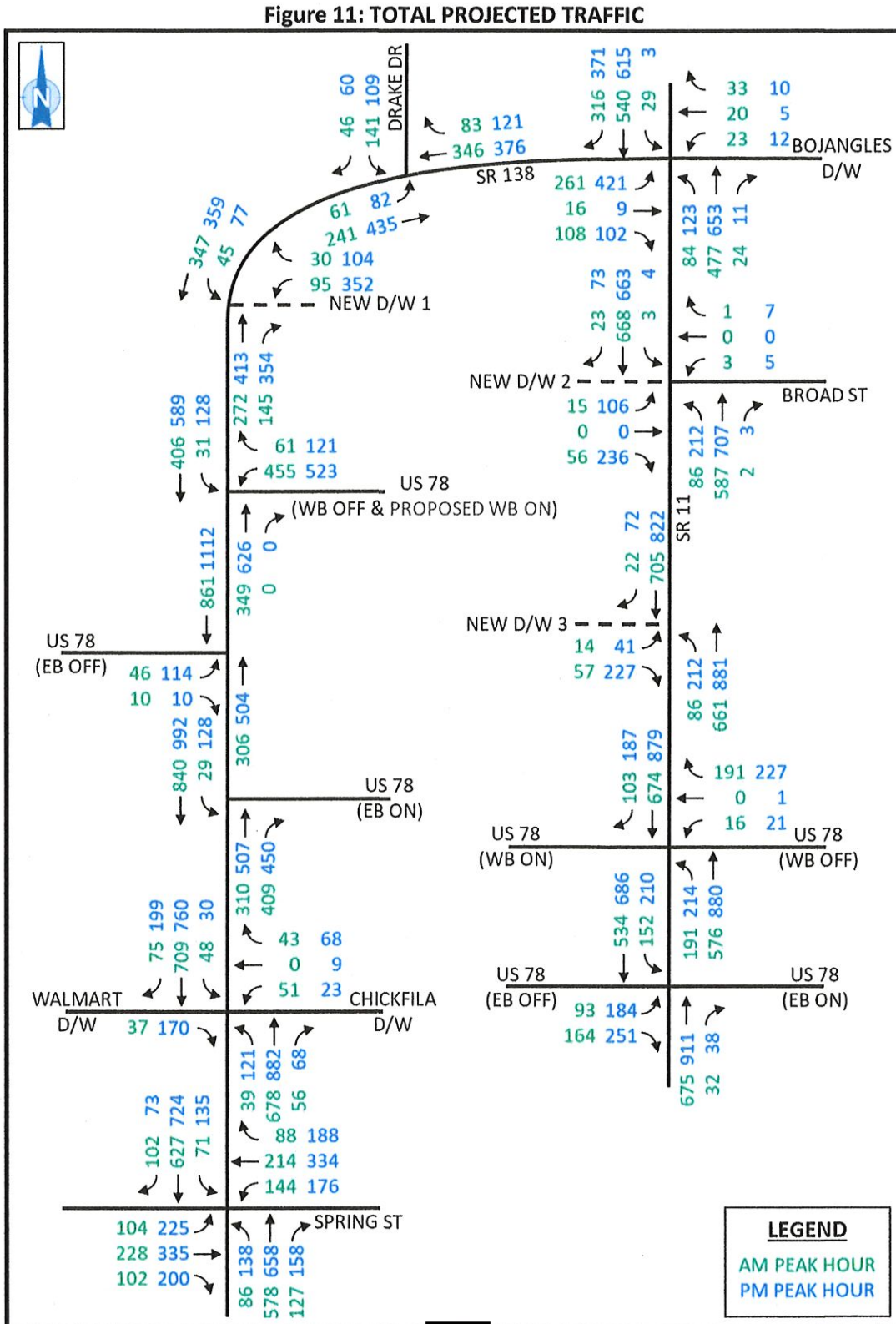
Net New Trips

The resulting trip assignment for the total traffic being added to the street network is shown in Figure 10. This assignment is a result of combining the new trip assignment in Figure 8 with the pass-by and diverted link trip assignment in Figure 9.



Total Projected Traffic

The resulting trip assignment for the total projected traffic within the study area is shown in Figure 11. Volumes shown are a result of combining the net new trip assignment in Figure 10 with the existing volumes in Figure 5.



CAPACITY ANALYSIS

Existing and projected conditions were evaluated using capacity analysis techniques described in the *Highway Capacity Manual, Special Report 209*, published by the Transportation Research Board, 2010, and with the help of *Synchro 10* from Trafficware. HCM Level of Service (LOS) definitions are shown in Table 3.

Table 3: LEVEL OF SERVICE CRITERIA

LEVEL OF SERVICE	DELAY PER VEHICLE (SECONDS)	
	SIGNALIZED INTERSECTIONS	STOP CONTROLLED INTERSECTIONS
A	≤10.0	≤10.0
B	10.1 to 20.0	10.1 to 15.0
C	20.1 to 35.0	15.1 to 25.0
D	35.1 to 55.0	25.1 to 35.0
E	55.1 to 80.0	35.1 to 50.0
F	>80.0	>50.0

Source: Highway Capacity Manual, Special Report 209, Transportation Research Board, 2010

EXISTING & PROJECTED CONDITIONS

The study intersections were analyzed under existing and projected conditions. Projected conditions include the new development, all geometric modifications included in the development site plan, and the traffic generated by the development superimposed onto the existing traffic. The results of the capacity analysis for existing signalized intersections under each set of conditions are summarized in Table 4, and for stop-controlled intersections, in Table 5.

Intersections serving as driveways to the new development, including the intersection at SR 11 at Broad Street/Driveway 2 (Study Intersection #2), SR 11 at Driveway 3, and SR 138 at Driveway 1 are analyzed separately in Table 6. Each driveway is analyzed under stop-control and signal-control conditions, as these intersections are shown under signal control on the site plan provided by Columbia Engineering.

The eastbound off ramp at the interchange of US Highway 78 and SR 138 was evaluated under proposed conditions under which vehicles originating in the west on US Highway 78 wishing to travel north on SR 138 will merge onto SR 138 southbound as exiting vehicles are doing under existing conditions, then turn left onto the eastbound on ramp and utilize a proposed merge ramp from the eastbound on ramp onto SR 138 northbound.

For each condition, the Level of Service is shown, followed parenthetically by the average delay per vehicle, in seconds. Capacity analysis reports for existing and projected conditions are included in Appendix F.

Table 4: CAPACITY ANALYSIS – EXISTING AND PROJECTED – SIGNAL CONTROL

INTERSECTION	APPROACH - MOVEMENT	AM PEAK HOUR		PM PEAK HOUR		
		EXISTING	PROJECTED	EXISTING	PROJECTED	
1	SR 11 @ SR 138	EB-L	C (34.7)	D (40.3)	D (36.9)	D (49.9)
		EB-T/R	A (8.9)	A (9.2)	A (6.4)	A (6.3)
		WB-L	C (22.3)	C (24.3)	C (20.6)	C (22.32)
		WB-T/R	B (13.8)	B (14.9)	B (12.2)	B (12.7)
		NB-L	B (11.2)	B (13.1)	B (15.8)	C (27.5)
		NB-T	B (11.6)	B (13.2)	B (17.9)	C (24.6)
		NB-R	A (3.2)	A (3.4)	A (6.0)	A (7.0)
		SB-L	B (15.4)	B (16.0)	B (18.3)	B (20.0)
		SB-T	C (28.5)	C (31.0)	C (35.0)	D (43.2)
		SB-R	A (3.6)	A (3.5)	A (4.0)	A (5.1)
6	SR 138 @ US HWY 78 WB OFF RAMP	WB-L	B (14.0)	B (15.7)	B (17.1)	D (40.2)
		WB-R	A (3.9)	A (3.2)	A (3.8)	C (25.6)
		NB-T	B (15.2)	B (18.5)	C (22.8)	C (26.0)
		NB-R ¹	-	A (0.0)	-	A (0.0)
		SB-L ¹	-	B (13.8)	-	D (52.7)
		SB-T	B (19.4)	C (22.4)	B (18.9)	C (21.3)
9	SR 138 @ SPRING ST	EB-L	D (35.5)	D (35.5)	D (51.7)	D (51.7)
		EB-T	E (74.7)	E (74.7)	D (49.5)	D (49.5)
		EB-R	A (8.7)	A (8.7)	A (6.5)	A (6.5)
		WB-L	D (54.9)	D (54.9)	D (42.9)	D (42.9)
		WB-T	D (52.2)	D (52.2)	E (77.2)	E (77.2)
		WB-R	A (8.5)	A (8.5)	A (8.1)	A (8.1)
		NB-L	E (65.4)	E (65.4)	E (63.7)	E (63.7)
		NB-T	C (29.8)	C (33.7)	D (41.6)	E (56.1)
		NB-R	A (3.5)	A (4.4)	A (5.8)	A (7.9)
		SB-L	E (68.9)	E (68.9)	E (73.5)	E (73.5)
SB-T/R	C (22.1)	C (22.7)	C (30.7)	C (32.7)		

¹ Movement or dedicated lane only exists in projected conditions.

Capacity analyses show Study Intersections 1 and 6 operating acceptably on all movements under existing and projected conditions. Study Intersection 9 is experiencing failing operation on some movements under existing and projected conditions, however these movements are not worsened from existing to projected conditions.

Table 5: CAPACITY ANALYSIS – EXISTING AND PROJECTED – STOP CONTROL

INTERSECTION		APPROACH-MOVEMENT	AM PEAK HOUR		PM PEAK HOUR	
			EXISTING	PROJECTED PLAN	EXISTING	PROJECTED PLAN
3	SR 11 @ US HWY 78 WB RAMPS	WB-L/T/R	D (25.3)	F (57.8)	E (44.4)	F (686.1)
		NB-L	B (10.6)	B (11.6)	B (10.7)	B (14.6)
		NB-T	A (0.0)	A (0.0)	A (0.0)	A (0.0)
		SB-T/R	A (0.0)	A (0.0)	A (0.0)	A (0.0)
4	SR 11 @ US HWY 78 EB RAMPS	EB-L/R	D (32.4)	F (556.1)	F (167.3)	F (*)
		NB-T/R	A (0.0)	A (0.0)	A (0.0)	A (0.0)
		SB-L	B (10.1)	B (11.1)	B (10.8)	B (14.8)
		SB-T	A (0.0)	A (0.0)	A (0.0)	A (0.0)
5	SR 138 @ DRAKE DR	EB-L	F (129.8)	F (207.0)	E (41.5)	F (84.2)
		EB-R	A (0.0)	A (0.0)	A (0.0)	A (0.0)
		NB-L	A (9.0)	A (9.2)	A (8.7)	A (9.0)
		NB-T	A (0.0)	A (0.0)	A (0.0)	A (0.0)
		SB-T	A (0.0)	A (0.0)	A (0.0)	A (0.0)
		SB-R	A (0.0)	A (0.0)	A (0.0)	A (0.0)
7.1	SR 138 @ US HWY 78 EB OFF RAMP	EB-R	C (15.5)	C (18.8)	C (16.6)	E (39.3)
		NB-T	A (0.0)	A (0.0)	A (0.0)	A (0.0)
		SB-T	A (0.0)	A (0.0)	A (0.0)	A (0.0)
7.2	SR 138 @ US HWY 78 EB ON RAMP	NB-T	A (0.0)	A (0.0)	A (0.0)	A (0.0)
		NB-R	A (0.0)	A (0.0)	A (0.0)	A (0.0)
		SB-L	A (7.8)	A (8.7)	A (8.5)	F (56.3)
		SB-T	A (0.0)	A (0.0)	A (0.0)	A (0.0)
	SR 138 @ US HWY 78 EB OFF RAMP U-TURN MERGE	-	-	B (10.5)	-	B (13.7)
8	SR 138 @ WALMART D/W	EB-R	B (11.6)	B (11.8)	C (15.1)	C (16.4)
		WB-L/T/R	F (99.0)	F (171.8)	F (826.9)	F (*)
		NB-L	A (9.3)	A (9.4)	A (9.6)	B (10.2)
		NB-T	A (0.0)	A (0.0)	A (0.0)	A (0.0)
		NB-R	A (0.0)	A (0.0)	A (0.0)	A (0.0)
		SB-L	A (9.5)	A (9.9)	B (10.7)	B (11.8)
		SB-T/R	A (0.0)	A (0.0)	A (0.0)	A (0.0)

*Delay is beyond calculable range.

¹ Movement or dedicated lane only exists in projected conditions.

Capacity analyses show the side-street approaches at Study Intersections 3, 4, 5, and 8 are failing under existing conditions, with the exception of the US Highway 78 eastbound and westbound ramps on SR 11 in the AM Peak Hour. Under projected conditions, all side-street approaches at all stop-controlled intersections are expected to experience poor operation in one or both Peak Hours, with the exception of the US Highway 78 eastbound off ramp U-turn merge onto SR 138. Intersections that are failing under existing conditions and are expected to fail under projected conditions are also expected to see significant increases in delay from existing to projected conditions.

Table 6: CAPACITY ANALYSIS – EXISTING AND PROJECTED – DRIVEWAYS

INTERSECTION	APPROACH- MOVEMENT	AM PEAK HOUR			PM PEAK HOUR		
		EXISTING	PROJECTED (STOP)	PROJECTED (SIGNAL)	EXISTING	PROJECTED (STOP)	PROJECTED (SIGNAL)
SR 138 @ DRIVEWAY 1 ¹	WB-L	-	C (19.4)	D (44.6)	-	F (327.0)	D (39.5)
	WB-R	-	B (10.1)	B (13.6)	-	B (12.2)	A (5.1)
	NB-T	-	A (0.0)	A (2.8)	-	A (0.0)	B (10.6)
	NB-R	-	A (0.0)	A (0.7)	-	A (0.0)	A (2.0)
	SB-L	-	A (8.4)	A (2.7)	-	B (10.0)	A (9.8)
	SB-T	-	A (0.0)	A (3.0)	-	A (0.0)	B (10.0)
2 SR 11 @ BROAD ST /DRIVEWAY 2	EB-L ¹	-	C (21.0)	D (55.0)	-	F (96.1)	F (271.1)
	EB-T/R ¹	-	B (14.7)	A (0.5)	-	D (25.8)	A (2.0)
	WB-L/T	B (14.5)	C (22.5)	E (55.7)	C (15.4)	F (427.5)	D (40.6)
	WB-R	A (0.0)	A (0.0)	A (0.0)	A (0.0)	A (0.0)	A (0.6)
	NB-L ¹	-	A (9.6)	A (5.2)	-	B (11.1)	C (22.4)
	NB-T	A (0.0)	A (0.0)	A (6.1)	A (0.0)	A (0.0)	B (10.8)
	NB-R	A (0.0)	A (0.0)	A (0.0)	A (0.0)	A (0.0)	A (0.0)
	SB-L	A (9.8)	A (9.8)	A (5.3)	A (9.3)	A (9.2)	A (7.5)
	SB-T	A (0.0)	A (0.0)	A (7.1)	A (0.0)	A (0.0)	B (12.8)
SB-R ¹	-	A (0.0)	A (0.7)	-	A (0.0)	A (2.6)	
SR 11 @ DRIVEWAY 3 ¹	EB-L	-	C (18.4)	D (56.0)	-	E (43.4)	D (39.0)
	EB-R	-	C (15.8)	C (21.7)	-	E (44.6)	B (17.7)
	NB-L	-	A (9.9)	A (1.8)	-	B (12.5)	B (14.7)
	NB-T	-	A (0.0)	A (2.3)	-	A (0.0)	A (6.4)
	SB-T/R	-	A (0.0)	A (1.5)	-	A (0.0)	A (3.7)

*Delay is beyond calculable range.

¹ Movement or dedicated lane only exists in projected conditions.

Capacity analysis shows Study Intersection 2 operating acceptably under existing conditions. Under projected conditions and with side-street stop control, all driveways into the development are expected to experience failing operation on the side-street approaches during the Peak Hours. Under projected conditions and with signal control, all driveways are expected to experience acceptable operation, with the exception of the side-street approaches at Study Intersection 2.

PROPOSED IMPROVEMENTS

In this section, various geometric and traffic control improvements, additional to those specified on the site plan, were evaluated to determine the optimal configuration at new intersections and improve existing intersections adversely impacted by the development. Intersections evaluated in this section include:

- SR 11 & Driveway 2
- SR 11 & Driveway 3
- SR 11 & US Highway 78 Eastbound Ramps
- SR 11 & US Highway 78 Westbound Ramps
- SR 138 & Driveway 1
- SR 138 & Drake Drive
- SR 138 & US Highway 78 Eastbound Ramps

Though the westbound approach at the intersection of SR 138 and Walmart Driveway is worsened under projected conditions, this is a relatively low volume commercial driveway, therefore this intersection will not be included in improvements.

The GDOT Intersection Control Evaluation (ICE) Tool was used to preliminarily evaluate various methods of traffic control at the intersections of the new driveways to the development. Traffic control methods utilized in the improvements, described further in this section, are methods that passed the first stage of analysis using the ICE tool.

The process for determining the most effective combinations of improvements at each study intersection listed above was an iterative process, in which a variety of improvements were analyzed at each intersection. The improvements at each intersection or group of intersections included in the following sections are those that yield the most desirable levels of service, rank positively according to ICE analysis, and were deemed most feasible based on previous discussion between Wilburn Engineering and the client.

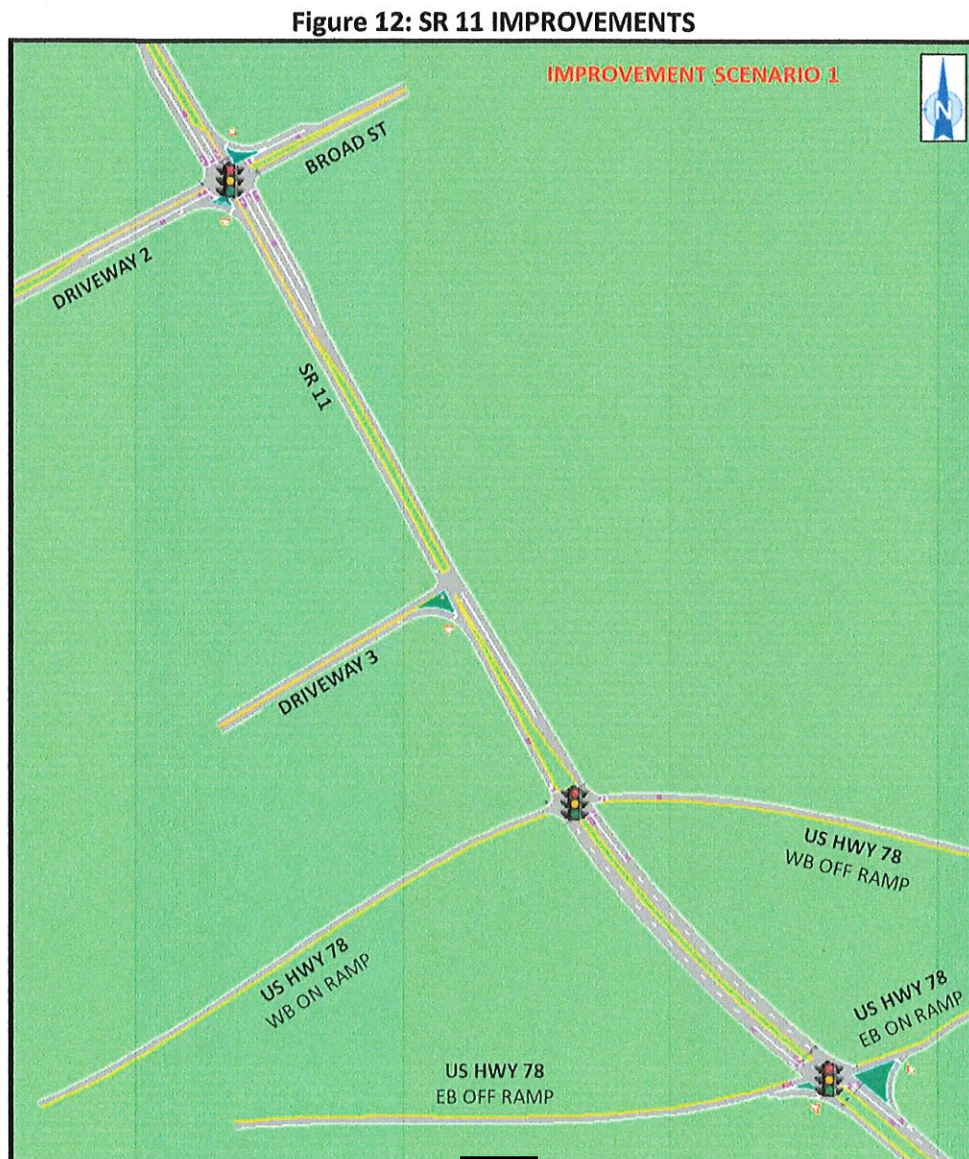
Capacity analysis reports for all improvements are provided in Appendix G.

SR 11 IMPROVEMENTS

Geometric and traffic control modifications at the intersections on SR 11 that yield acceptable results are as follows:

- Installation of a traffic signal, with protected/permitted phasing on the southbound left turn movement, at the intersection of SR 11 and US Highway 78 Eastbound Ramps
- Installation of a traffic signal at the intersection of SR 11 and US Highway 78 Westbound Ramp
- Implementation of unsignalized RCUT control at the intersection of SR 11 and Driveway 3 (**ICE top ranked alternative**), relocating left turning vehicles to the intersection at Driveway 2/Broad Street
- Installation of a traffic signal at the intersection of SR 11 and Driveway 2/Broad Street (**ICE top ranked alternative**)

These improvements are shown in Figure 12.



Capacity analysis results for improvements included on the previous page are shown in Table 7.

Table 7: CAPACITY ANALYSIS – SR 11 IMPROVEMENTS

INTERSECTION	APPROACH-MOVEMENT	AM PEAK HOUR		PM PEAK HOUR	
		EXISTING	IMPROVED	EXISTING	IMPROVED
2 SR 11 @ BROAD ST/DRIVEWAY 2	EB-L ¹	-	D (44.8)	-	D (54.2)
	EB-T/R ¹	-	A (0.4)	-	A (3.9)
	WB-L/T	B (14.5)	D (40.3)	C (15.4)	C (34.8)
	WB-R	A (0.0)	A (0.0)	A (0.0)	A (0.1)
	NB-L ¹	-	A (5.3)	-	A (7.2)
	NB-T	A (0.0)	A (5.7)	A (0.0)	A (6.2)
	NB-R	A (0.0)	A (0.0)	A (0.0)	A (2.7)
	SB-L	A (9.8)	A (5.3)	A (9.3)	A (8.5)
	SB-T	A (0.0)	A (7.4)	A (0.0)	B (16.4)
	SB-R ¹	-	A (0.3)	-	A (2.4)
SR 11 @ DRIVEWAY 3 ¹	EB-R	-	C (15.6)	-	E (48.9)
	NB-L	-	B (10.3)	-	B (14.6)
	NB-T	-	A (0.0)	-	A (0.0)
	SB-T/R	-	A (0.0)	-	A (0.0)
3 SR 11 @ US HWY 78 WB	WB-L/T	D (25.3)	B (18.0)	E (44.4)	C (27.1)
	NB-L	B (10.6)	A (7.2)	B (10.7)	D (52.8)
	NB-T	A (0.0)	A (2.8)	A (0.0)	A (9.0)
	SB-T/R	A (0.0)	A (3.8)	A (0.0)	D (51.8)
4 SR 11 @ US HWY 78 EB	EB-L	D (32.4)	D (44.8)	F (167.3)	D (48.4)
	EB-R	A (0.0)	B (17.8)	A (0.0)	C (26.6)
	NB-T/R	A (0.0)	A (9.3)	A (0.0)	B (17.7)
	SB-L	B (10.1)	A (5.9)	B (10.8)	B (19.7)
	SB-T	A (0.0)	A (6.0)	A (0.0)	B (10.8)

¹ Intersection, movement, or dedicated lane only exists in projected conditions.

Capacity analyses show all intersections operating acceptably under these improvements, with the exception of the eastbound right turn at the intersection of SR 11 and Driveway 3 in the PM Peak Hour, which is expected to experience a level of service ‘E’. When drivers become acclimated to the new configurations at these intersections, right turn movements from Driveway 2 and Driveway 3 will likely reach a capacity-based equilibrium, leading to acceptable operation for both movements.

While these results show the mainline of SR 11 working acceptably under existing and projected conditions, current daily volumes are approaching typical thresholds for necessitating widening of a roadway from a 2-lane typical section to a 4-lane typical section. It is recommended that consideration is given to widening SR 11, unassociated with the proposed development.

SR 138 & DRIVEWAY 1

Capacity analysis indicated that, under projected conditions, acceptable levels of service cannot be maintained with minor stop-control at the intersection of SR 138 and Driveway 1. The intersection meets signal warrants, as mentioned in the following section of the report, and is shown as a signalized intersection on the site plan. Methods of traffic control included in the analysis for the intersection of SR 138 and Driveway 1 include a signal and a single-lane roundabout.

Signal control at the intersection of SR 138 and Driveway 1 (**ICE top ranked alternative**) would be in accordance with the site plan provided by Columbia Engineering, therefore would not require any modifications additional to those shown on the site plan.

Under single-lane roundabout control, changes to the intersection include the following geometric and traffic control modifications:

- Installation of a roundabout at the intersection of SR 138 and Driveway 1
- Modification of all approaches to provide a single approach lane

The results of the capacity analysis are summarized in Table 8.

Table 8: CAPACITY ANALYSIS – SR 138 @ DRIVEWAY 1 IMPROVEMENTS

INTERSECTION	APPROACH-MOVEMENT	AM PEAK HOUR				PM PEAK HOUR			
		EXISTING	SIGNAL	ROUNDAABOUT		EXISTING	SIGNAL	ROUNDAABOUT	
				SIDRA	GDOT			SIDRA	GDOT
5 SR 138 @ DRIVEWAY 1 ¹	WB-L	-	D (44.6)	A (5.9)	A (5.0)	-	D (39.5)	C (23.9)	B (14.0)
	WB-R	-	B (13.6)			-	A (5.1)		
	NB-T	-	A (2.8)	A (8.1)	A (7.0)	-	B (10.6)	C (21.9)	B (14.0)
	NB-R	-	A (0.7)			-	A (2.0)		
	SB-L	-	A (2.7)	A (8.7)	A (7.0)	-	A (9.8)	C (15.5)	B (10.0)
	SB-T	-	A (3.0)			-	B (10.0)		

Capacity analysis results indicate the intersection of SR 138 and Driveway 1 would operate acceptably on all approaches under signal or roundabout control.

While these results show the mainline of SR 138 working acceptably under projected conditions, projected daily volumes at this location are near typical thresholds for necessitating widening of a roadway from a 2-lane typical section to a 4-lane typical section. It is recommended that consideration is given to widening SR 138, unassociated with the proposed development.

SR 138 & DRAKE DRIVE

Capacity analysis indicated the intersection of SR 138 and Drake Drive is failing under existing conditions and, without any mitigating action, will worsen under projected conditions. The intersection does not meet signal warrants, as addressed in the following section of the report, therefore improvements at this intersection were limited to unsignalized forms of traffic control. Due to the existing geometric characteristics of the intersection and the surrounding area, it was determined that a roundabout is the only form of unsignalized control that could feasibly be incorporated at the intersection without significant geometric changes to SR 138, therefore this is the only form of traffic control included in this section.

Under single-lane roundabout control, changes to the intersection include the following geometric and traffic control modifications:

- Installation of a roundabout at the intersection of SR 138 and Drake Drive
- Modification of all approaches to provide a single approach lane

The results of the capacity analysis are summarized in Table 9.

Table 9: CAPACITY ANALYSIS – SR 138 @ DRAKE DRIVE IMPROVEMENT

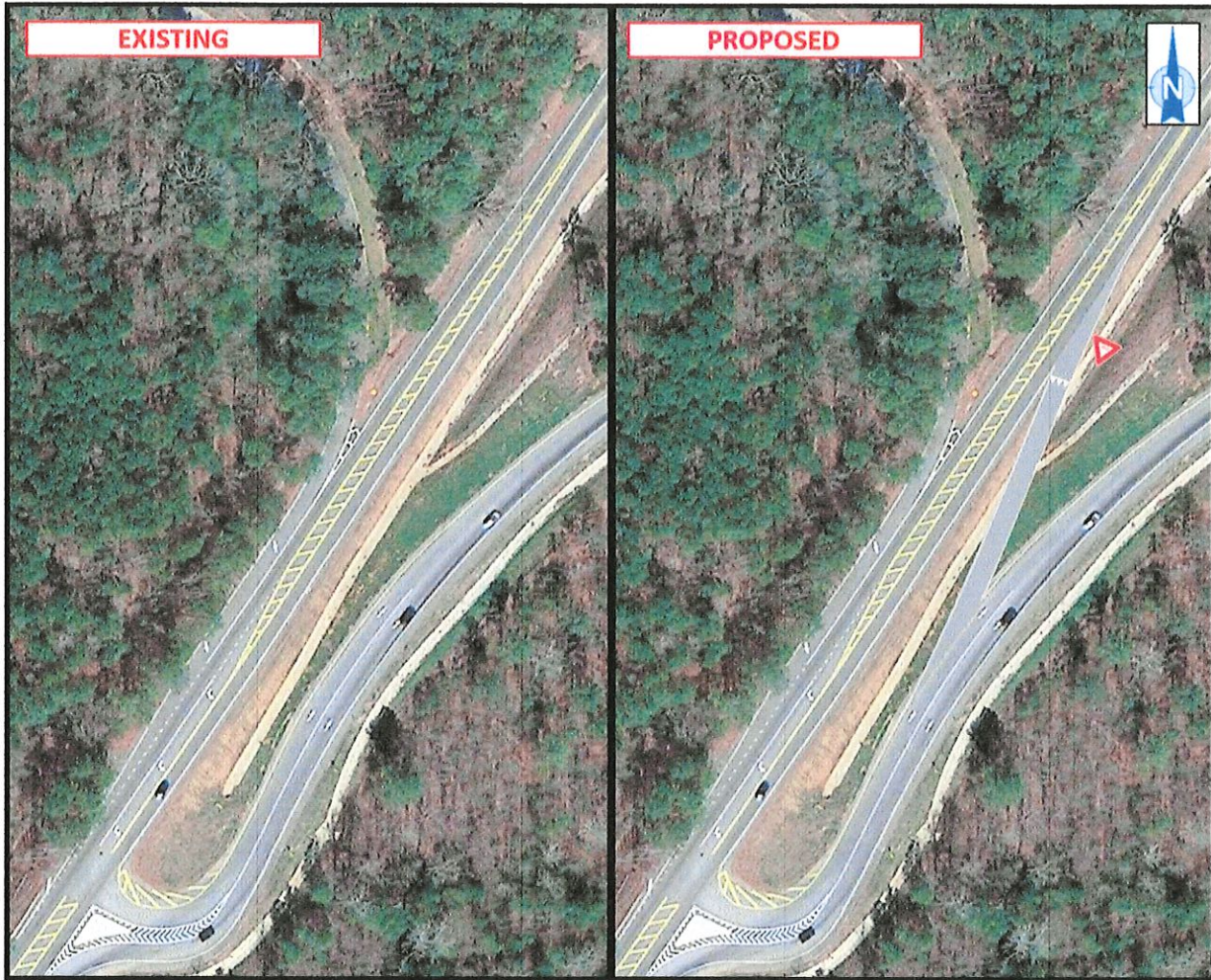
INTERSECTION	APPROACH-MOVEMENT	AM PEAK HOUR			PM PEAK HOUR		
		EXISTING	ROUNDBABOUT		EXISTING	ROUNDBABOUT	
			SIDRA	GDOT		SIDRA	GDOT
5 SR 138 @ DRAKE DR	EB-L	F (129.8)	B (10.1)	B (10.0)	E (41.5)	A (7.3)	A (8.0)
	EB-R	A (0.0)			A (0.0)		
	NB-L	A (9.0)	B (13.8)	A (9.0)	A (8.7)	B (14.0)	A (9.0)
	NB-T	A (0.0)			A (0.0)		
	SB-T	A (0.0)			A (0.0)		
	SB-R	A (0.0)	B (12.1)	A (7.0)	A (0.0)	A (10.0)	A (8.0)

Capacity analysis results indicate the intersection of SR 138 and Drake Drive would operate acceptably on all approaches under roundabout control.

SR 138 & US HIGHWAY 78 EASTBOUND RAMP

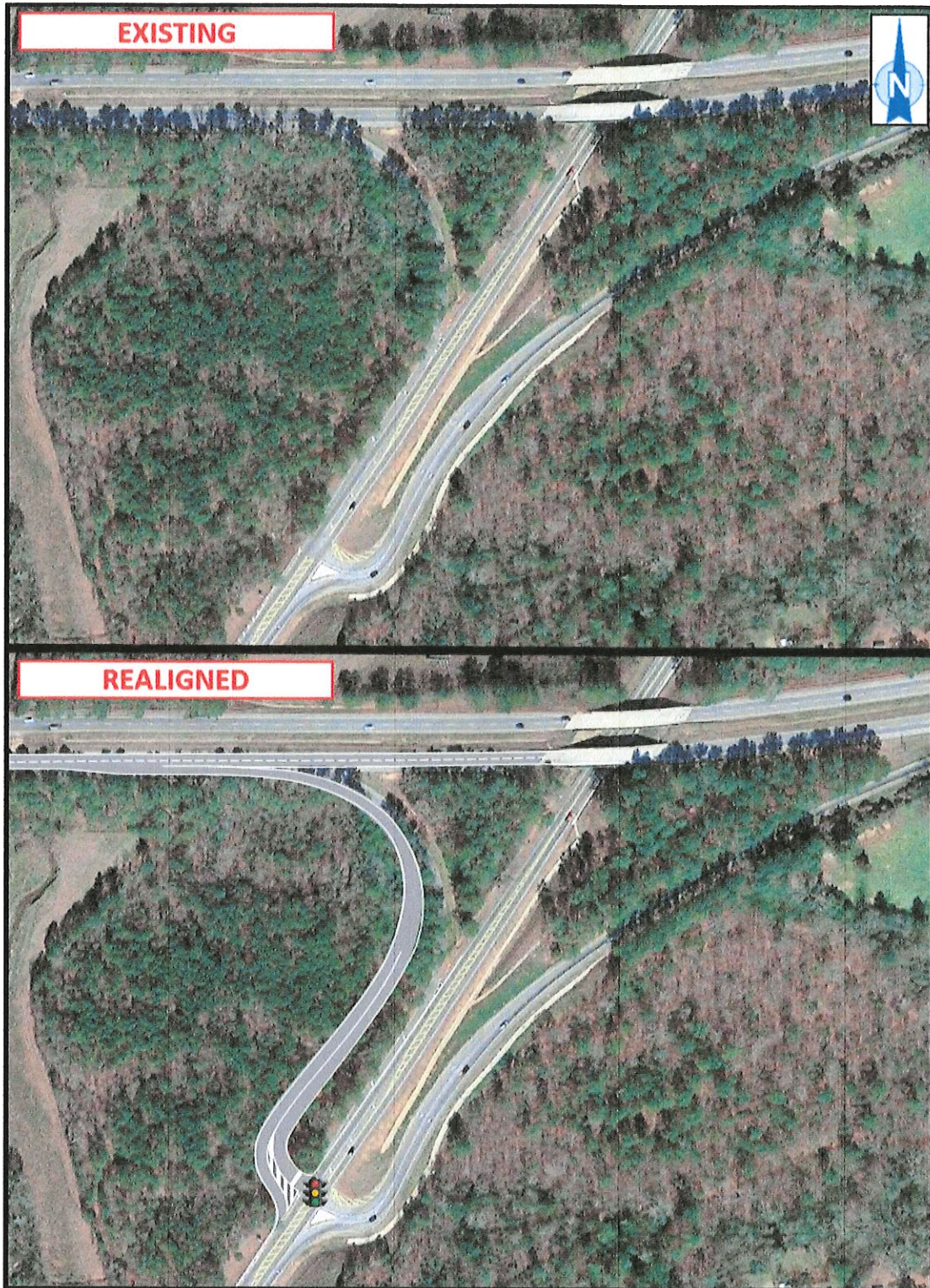
The eastbound off ramp at the interchange of US Highway 78 and SR 138 was evaluated under proposed conditions under which vehicles originating in the west on US Highway 78 wishing to travel north on SR 138 will merge onto SR 138 southbound as the ramp currently operates, then turn left onto the eastbound on ramp and utilize a proposed merge ramp from the eastbound on ramp onto SR 138 northbound. This configuration is shown in Figure 13.

Figure 13: PROPOSED CONFIGURATION 1 AT SR 138 AND US HWY 78 EASTBOUND RAMP



Capacity analysis indicated the intersections on SR 138 at the eastbound ramp junctions, particularly the merge onto SR 138 from the eastbound off ramp and the left onto the eastbound on ramp from SR 138, are expected to fail under this configuration. In this section, an alternative configuration is evaluated in which the ramps are realigned to form one intersection. A concept is shown in Figure 14 on the following page.

Figure 14: PROPOSED CONFIGURATION 2 AT SR 138 AND US HWY 78 EASTBOUND RAMP



Under this configuration, the intersection meets signal warrants and, therefore, was evaluated under signal control. Roundabout control was also considered. However, preliminary analysis revealed the intersection does not meet the GDOT 90/10 roundabout screening criteria, therefore this method of control was not included in further analysis. Results of capacity analysis under this configuration are shown in Table 10 on the following page.

Table 10: CAPACITY ANALYSIS – SR 138 @ US HWY 78 EASTBOUND REALIGNED IMPROVEMENT

INTERSECTION	APPROACH-MOVEMENT	AM PEAK HOUR		PM PEAK HOUR	
		EXISTING	REALIGNED (Signalized)	EXISTING	REALIGNED (Signalized)
5 SR 138 @ EASTBOUND RAMPS	EB-L ¹	-	D (43.1)	-	D (45.2)
	EB-R	C (15.5)	A (0.5)	C (16.6)	A (0.3)
	NB-T	A (0.0)	A (4.4)	A (0.0)	B (10.2)
	NB-R	A (0.0)	A (1.2)	A (0.0)	A (1.8)
	SB-L	A (7.8)	A (1.8)	A (8.5)	A (4.0)
	SB-T	A (0.0)	A (4.3)	A (0.0)	A (9.6)

¹ Movement or dedicated lane only exists in projected conditions.

Capacity analysis results indicate the intersection of SR 138 and US Highway 78 Eastbound would operate acceptably on all approaches under signal control.

While these results show the mainline of SR 138 working acceptably under projected conditions, projected daily volumes at this location are near typical thresholds for necessitating widening of a roadway from a 2-lane typical section to a 4-lane typical section. It is recommended that consideration is given to widening SR 138, unassociated with the proposed development.

SIGNAL WARRANT ANALYSIS

The capacity analysis results identified that stop control operation is insufficient at many of the intersections within the study area under projected conditions. Traffic signal warrant analysis was conducted for the following intersections under existing and projected conditions:

- SR 11 & US Highway 78 Eastbound Ramps
- SR 11 & US Highway 78 Westbound Ramps
- SR 11 & Driveway 3
- SR 11 & Driveway 2/Broad Street
- SR 138 & Drake Drive
- SR 138 & Driveway 1
- SR 138 & US Highway 78 Eastbound Ramps (Realigned)

The warrant analysis was conducted in accordance with the requirements of the *Manual on Uniform Traffic Control Devices*, 2009 (MUTCD) published by the Federal Highway Administration.

According to the MUTCD, the investigation of the need for traffic control signal shall include an analysis of the applicable factors contained in the following traffic signal warrants and other factors related to existing operation and safety at the study intersection:

- Warrant 1 – Eight-Hour Vehicular Volume
- Warrant 2 – Four-Hour Vehicular Volume
- Warrant 3 – Peak Hour
- Warrant 4 – Pedestrian Volume
- Warrant 5 – School Crossing
- Warrant 6 – Coordinated Signal System
- Warrant 7 – Crash Experience
- Warrant 8 – Roadway Network

The warrant analysis was conducted using the conventional and alternate methods for Warrants 1 and 2. Based on GDOT guidelines, 100% volume thresholds will be used to determine satisfaction of signal warrants. Right turn volumes included in the analysis were reduced based on guidance found in NCHRP Report 457.

Since Warrants 1 and 2 require hourly volumes to meet thresholds for a minimum of eight and four hours, respectively, it was necessary to estimate the 8 highest hours. A peak hour K-factor was estimated based on ATR count data and daily trips generated by the development. By interpolating between the estimated peak hour K-factor and the accepted 8th highest hour of 5.6%, the remaining 7 highest hours were estimated.

The derived 8th and 4th highest hour volumes were compared to the warrant requirements contained in the *Manual on Uniform Traffic Control Devices, 2009* (MUTCD) published by the Federal Highway Administration.

The results of signal warrant analysis are summarized in Table 11. Signal warrant analysis reports are provided in Appendix H.

Table 11: SIGNAL WARRANT ANALYSIS SUMMARY

INTERSECTION	WARRANT 1		WARRANT 2	
	EXISTING	PROJECTED	EXISTING	PROJECTED
2 SR 11 @ DRIVEWAY 2/BROAD ST	N	Y	N	Y
SR 11 @ DRIVEWAY 3 ¹	-	Y	-	Y
3 SR 11 @ US HWY 78 WESTBOUND	N	Y	N	Y
4 SR 11 @ US HWY 78 EASTBOUND	N	Y	N	Y
5 SR 138 @ DRAKE DR	N	N	N	N
SR 138 @ DRIVEWAY 1 ¹	-	Y	-	Y
7 SR 138 @ US HWY 78 EASTBOUND (Realigned)	-	Y	-	Y

¹Intersection only exists under projected conditions.

Signal warrant analysis results indicate no signal warrants are satisfied at any intersection under existing conditions. Under projected conditions, signal warrant analysis results indicate that warrants 1 and 2 are satisfied at all intersections mentioned above except the intersection at SR 138 and Drake Drive.

LEFT TURN PHASING

It was determined through capacity analysis that signals under projected conditions at many locations within the study area would operate more efficiently with protected/permissive left turn phasing on certain approaches, rather than strictly permissive phasing.

The GDOT maintains guidelines regarding left turn phasing at a signalized intersection. To ensure that protected/permissive left turn phasing at this intersection would be in accordance with these guidelines, the GDOT Policy 6785-2 was consulted.

Section A of the policy states that protected/permissive phasing can be used for left turn phases if any one of multiple criteria are met. The first of these requirements will be examined here.

According to the first requirement, protected/permissive phasing can be used for left turn phases if the cross-product (see formula below) is greater than 50,000 for a leading left turn phase, or greater than 30,000 for a lagging left turn phase.

$$\text{Cross Product} = \text{Left Turn Volume} \left(\frac{\text{Opposing Thru Volume}}{\# \text{ of Opposing Thru Lanes}} \right)$$

The cross-product calculations for all left turns in question under projected conditions are shown in Table 12.

Table 12: LEFT TURN PHASING WARRANT CALCULATIONS

INTERSECTION	APPROACH	LEFT TURN VOLUME	OPPOSING THRU VOLUME	OPPOSING THRU LANES	CROSS PRODUCT	>50,000?
2 SR 11 @ DRIVEWAY 2/BROAD ST	NB	299	716	1	140,556	Y
SR 11 @ DRIVEWAY 3	NB	298	976	1	174,264	Y
3 SR 11 @ US HWY 78 WB	NB	214	946	1	188,106	Y
4 SR 11 @ US HWY 78 EB	SB	258	931	2	95,655	Y
6 SR 138 @ US HWY 78 WB	SB	128	626	1	80,128	Y
7.2 SR 138 @ US HWY 78 EB ON	SB	128	507	1	64,896	Y

Based on the cross-product criterion, installing protected/permissive left turn phasing for all left turns evaluated would be in agreement with the GDOT Policy 6785-2 for left turn phasing.

SUMMARY OF CONCLUSIONS

The findings of this study include the following:

- Under existing conditions, the following intersections within the study area are experiencing failing operation on one or more movements:
 - SR 11 & US Highway 78 Westbound
 - SR 11 & US Highway 78 Eastbound
 - SR 138 & Drake Drive
 - SR 138 & Walmart Driveway
 - SR 138 & Spring Street

- **Many assumptions regarding unknown variables involved in the analysis of the study area and the new development were based on engineering judgment. Depending on the land uses that end up going into the development, and the final configuration of the eastbound US Highway 78 ramps at SR 138, the volume and distribution of trips throughout the study area could be significantly different than what was projected. This is particularly critical to operation on SR 11 and SR 138, which are already near typical daily volume thresholds that necessitate the widening of a road to a four-lane facility.**

- Under projected conditions (conditions including development traffic superimposed onto existing traffic with roadway geometry matching the site plan provided by Columbia Engineering), the following intersections are expected to fail on one or more movements, or were failing under existing conditions but are expected to experience a significant increase in delay on one or more movements, due to traffic associated with the development:
 - SR 11 & Driveway 2
 - SR 11 & Driveway 3
 - SR 11 & US Highway 78 Eastbound Ramps
 - SR 11 & US Highway 78 Westbound Ramps
 - SR 138 & Driveway 1
 - SR 138 & Drake Drive
 - SR 138 & US Highway 78 Eastbound Off Ramp
 - SR 138 & US Highway 78 Eastbound On Ramp

- Signal warrants are met for the following intersections under projected conditions:
 - SR 11 & US Highway 78 Westbound Ramps
 - SR 11 & US Highway 78 Eastbound Ramps
 - SR 11 & Driveway 2/Broad Street
 - SR 11 & Driveway 3
 - SR 138 & Driveway 1
 - SR 138 & US Highway 78 Eastbound Ramps (Realigned)

- Regarding improvements at the intersections on SR 11 at Driveway 2/Broad Street, at Driveway 3, at US Highway 78 Westbound, and at US Highway 78 Eastbound:

Improvements that yield acceptable operation under projected conditions during both Peak Hours and are most economical include a traffic signal at both US Highway 78 ramp junctions on SR 11, a traffic signal at Driveway 2/Broad Street, and unsignalized RCUT geometry at Driveway 3, which will not necessitate the construction of a median because vehicles wishing to go left onto SR 11 out of the development will relocate to the signalized intersection at Driveway 2/Broad Street.

Current daily volumes on SR 11 are approaching thresholds which typically necessitate widening to a four-lane facility. Consideration should be given to the widening of SR 11 to a four-lane divided typical section, unrelated to the development.

- Regarding improvements at the intersection of SR 138 and Drake Drive:

A single-lane roundabout is the most feasible form of traffic control that yields acceptable operation at the intersection of SR 138 and Drake Drive.

- Regarding improvements at the intersection of SR 138 and Driveway 1:

A traffic signal is the most feasible form of traffic control that yields acceptable operation at the intersection of SR 138 and Driveway 1.

Projected daily volumes on SR 11 are near thresholds which typically necessitate widening to a four-lane facility. Consideration should be given to the widening of SR 138 to a four-lane divided typical section, unrelated to the development.

RECOMMENDATIONS

Based on the findings of this study, the recommendations are as follows:

- Unrelated to the development, it is recommended that consideration is given to widening SR 11 and SR 138 to a four-lane divided typical section.
- Install a traffic signal, with permitted/protected left-turn phasing on the northbound approach, at the intersection of SR 11 and Driveway 2/Broad Street (**ICE top ranked alternative**), with a northbound left turn lane of minimum length 300 feet.
- Install a traffic signal, with permitted/protected left-turn phasing on the northbound approach, at the intersection of SR 11 and US Highway 78 Westbound Ramps.
- Install a traffic signal, with permitted/protected left-turn phasing on the southbound approach, at the intersection of SR 11 and US Highway 78 Eastbound Ramps.
- Implement unsignalized RCUT geometry at the intersection of SR 11 and Driveway 3 (**ICE top ranked alternative**), with a northbound left turn lane of minimum length 300 feet.
- Install a single-lane roundabout at the intersection of SR 138 and Drake Drive.
- Install a traffic signal at the intersection of SR 138 and Driveway 1 (**ICE top ranked alternative**).
- Modify the existing traffic signal at the intersection of SR 138 and US Highway 78 Westbound Ramps to provide permitted/protected left-turn phasing on the southbound approach.
- Realign the eastbound on and off ramps at the interchange of SR 138 and US Highway 78 and install a traffic signal, with permitted/protected left-turn phasing on the southbound approach.

Monroe Pavilion

Located in the City of Monroe, Georgia

Submitted: May 6, 2019

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Monroe Pavilion

City of Monroe

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Introduction

On behalf of MAB American, LLC, the purpose of this Pattern Book is to provide a development outline and illustrations for the 95.48-acre Monroe Pavilion Planned Community Development (PCD), which will have more than 365,000 square feet of building area at final build-out. The 95.48-acre site is bordered on the south by US Highway 78, on the east by Georgia Highway 11, also known as North Broad Street, and on the west by Charlotte Rowell Boulevard. The subject site is currently zoned B-3, Highway Commercial District, and is within the Corridor Design Overlay District.

In addition to outlining the details and objectives of the Monroe Pavilion development and how it will meet the City of Monroe Zoning Ordinance requirements, this Pattern Book is intended to accompany MAB's request for rezoning to PCD, Planned Commercial Development, and a Certificate of Appropriateness related to the Corridor Design Overlay District for the Development.

Planning Objective

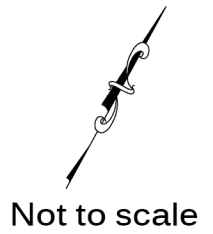
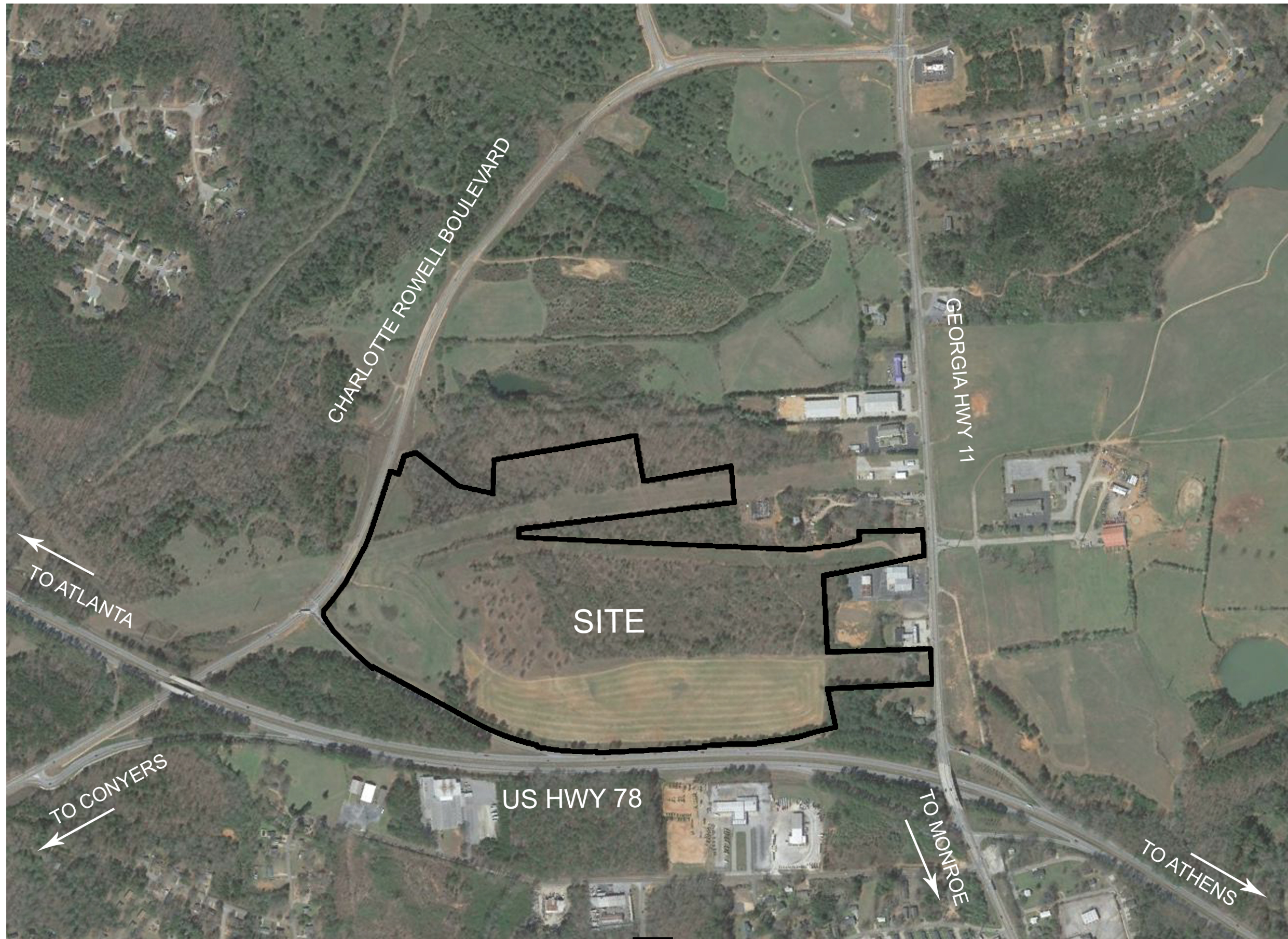
Monroe Pavilion is a Planned Community Development that provides a diverse commercial center with retail, restaurants, hotels, services and entertainment to serve the citizens of Monroe and the surrounding communities in Walton County with shopping opportunities to meet their needs. Situated between the interchanges with US Highway 78 and Charlotte Rowell Boulevard and Georgia Highway 11, Monroe Pavilion is ideally located to take advantage of the major thoroughfares and regional traffic patterns.

The proposed architecture, signage, streetscape, landscape and other design elements within Monroe Pavilion will be coordinated to reflect a cohesive character throughout the Development. Connectivity for both vehicular and pedestrian access binds the development components together to create a desirable gateway hub for Monroe. These design elements achieve a strong sense of arrival that is reflective of the City's character.

VICINITY MAP

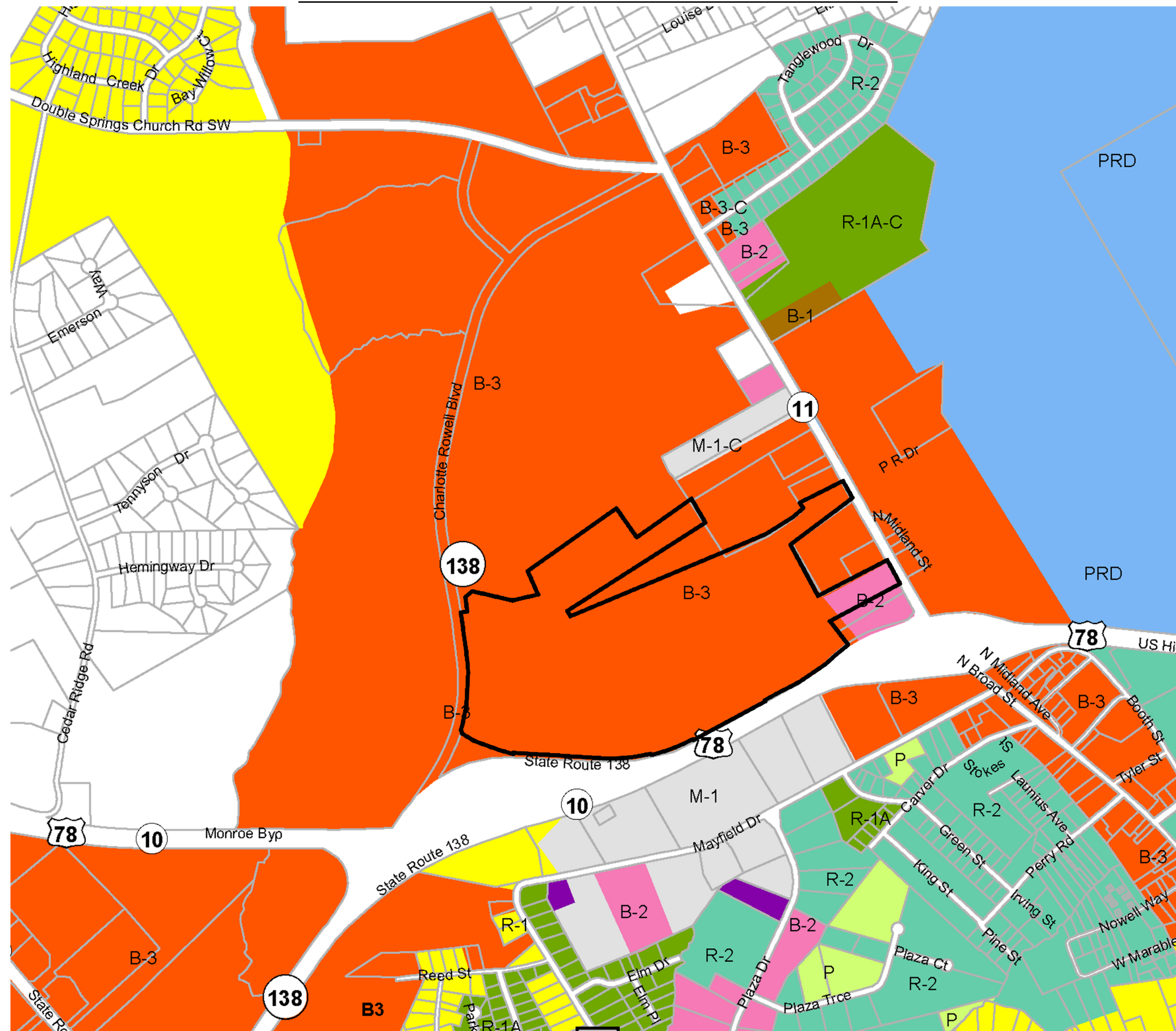


LOCATION MAP



CITY OF MONROE ZONING MAP

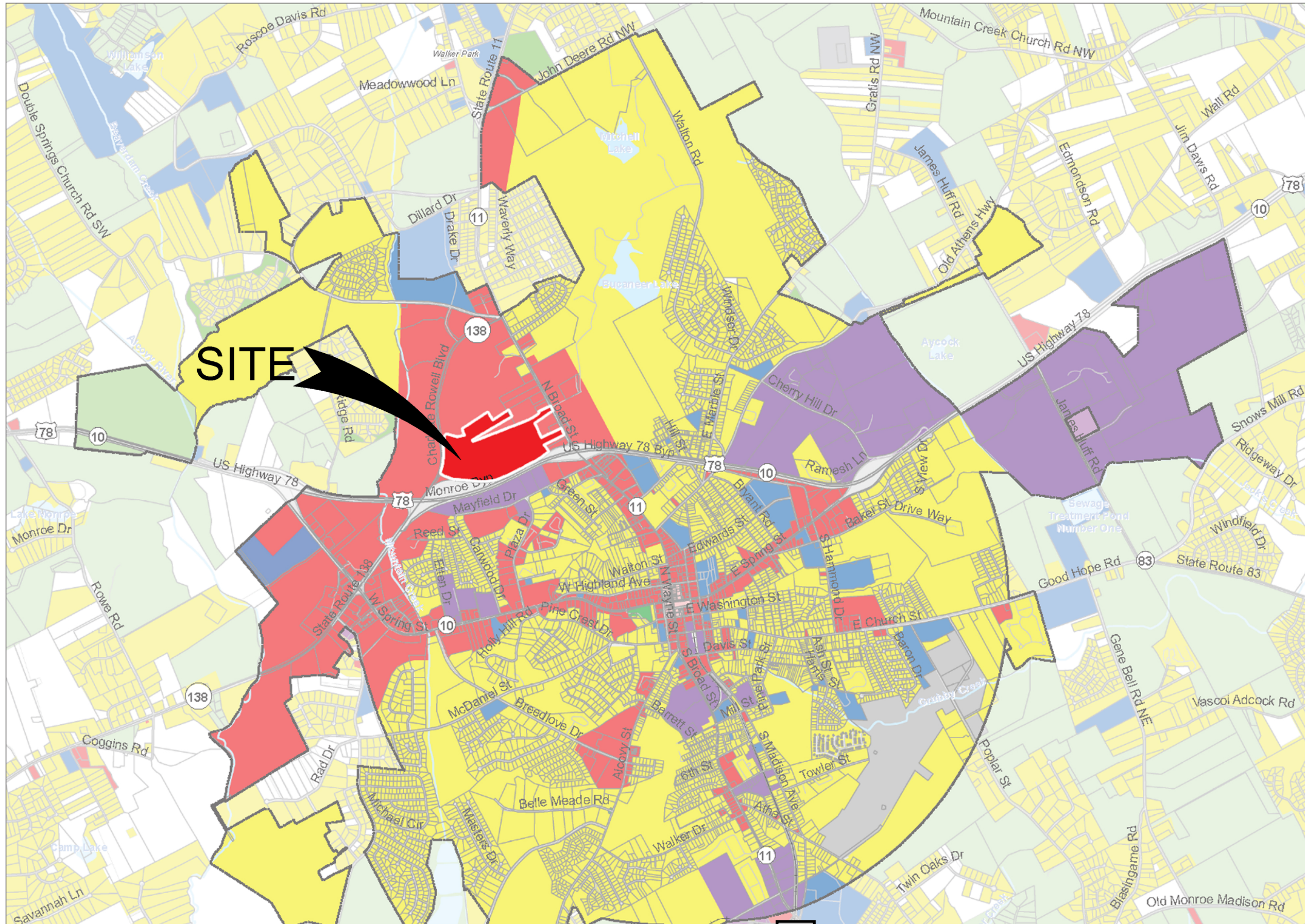
Introduction



This site lies within the B-3 Highway Commercial District of the City of Monroe Zoning Ordinance.



CITY OF MONROE FUTURE LAND USE MAP

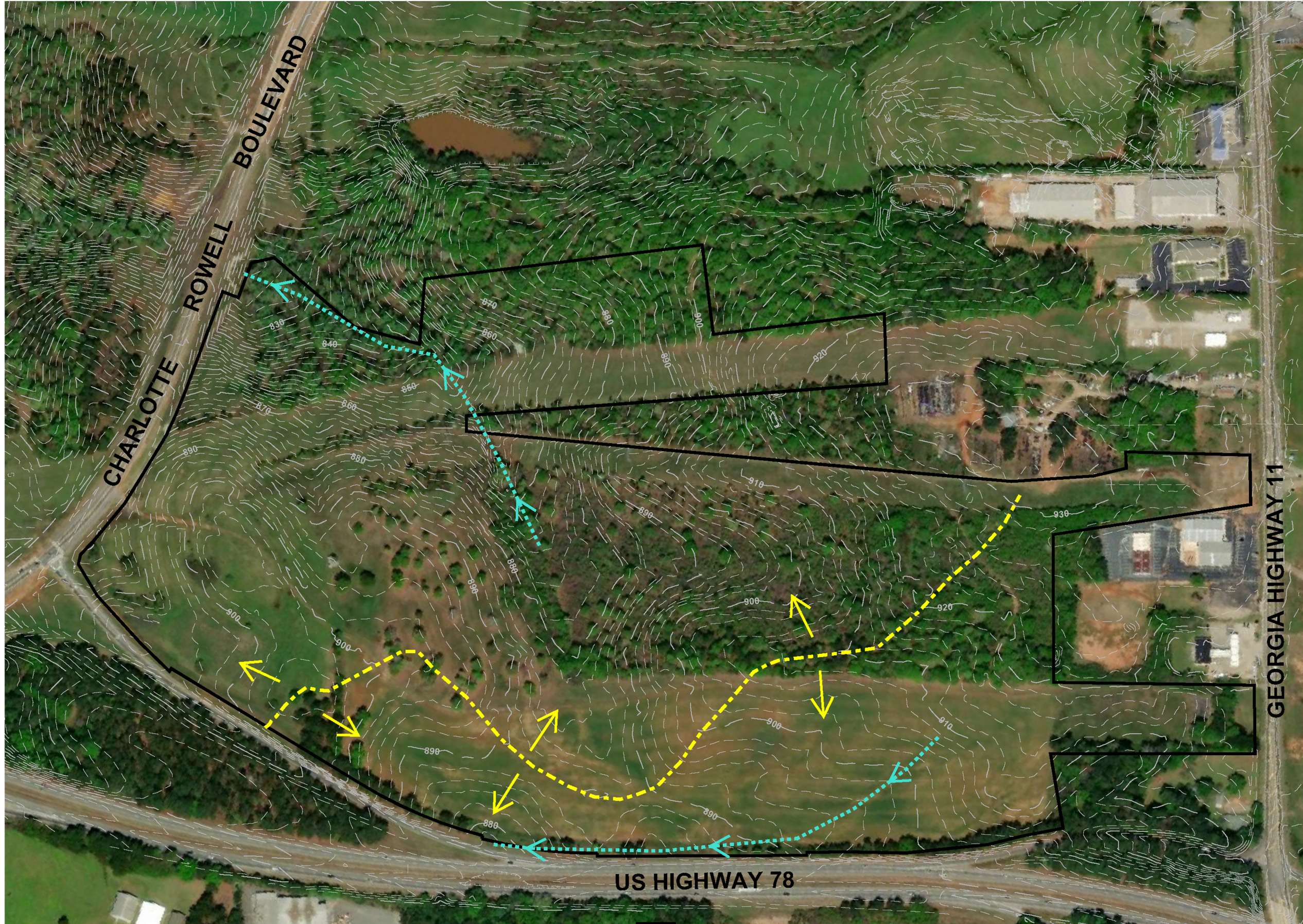


- Future Land Use**
- Residential
 - Commercial
 - Industrial
 - Public/Institutional
 - T/C/U
 - Park/Rec/Cons
 - Agriculture/Forestry
 - Mixed Use

- Other**
- State Route
 - Local Road
 - Railroad
 - City Limits
 - Rivers and Streams
 - Lakes
 - Parcels

This site lies within the Future Land Use category of "Commercial"

EXISTING CONDITIONS TOPOGRAPHIC MAP



LEGEND

DRAINAGE FLOW PATH:

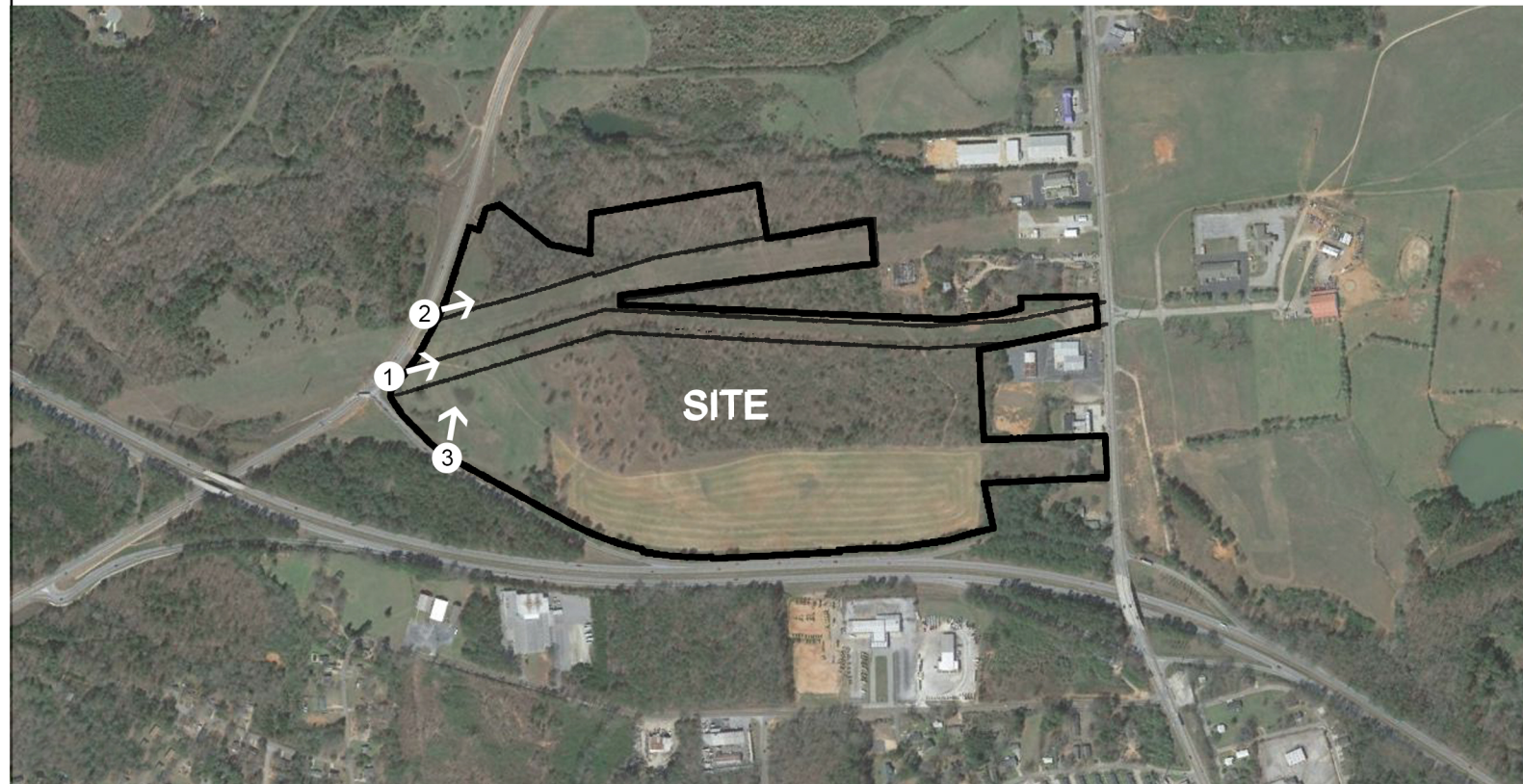
RIDGELINE:

DIRECTION OF FLOW:

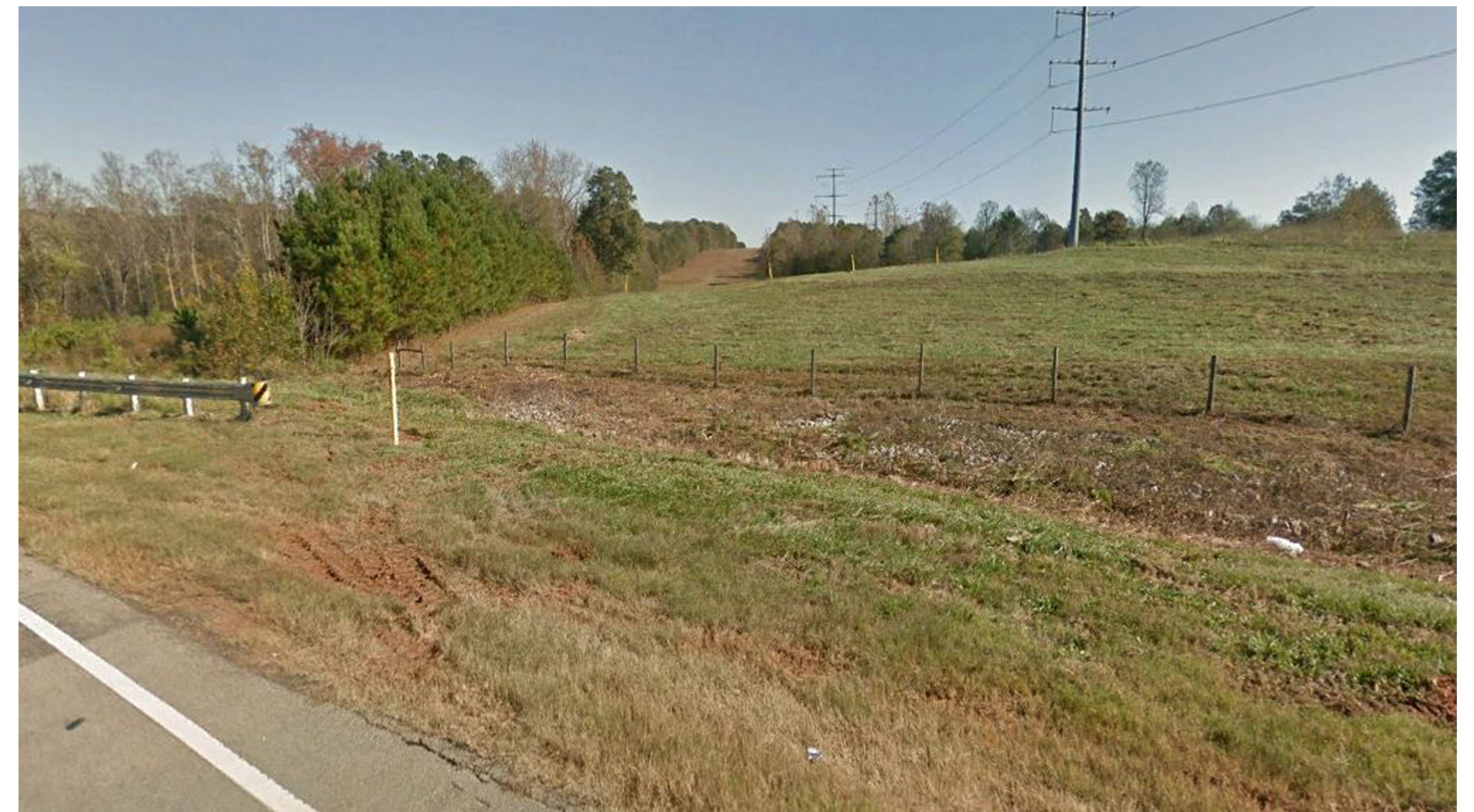
SITE BOUNDARY:

EXISTING CONDITIONS PHOTOS

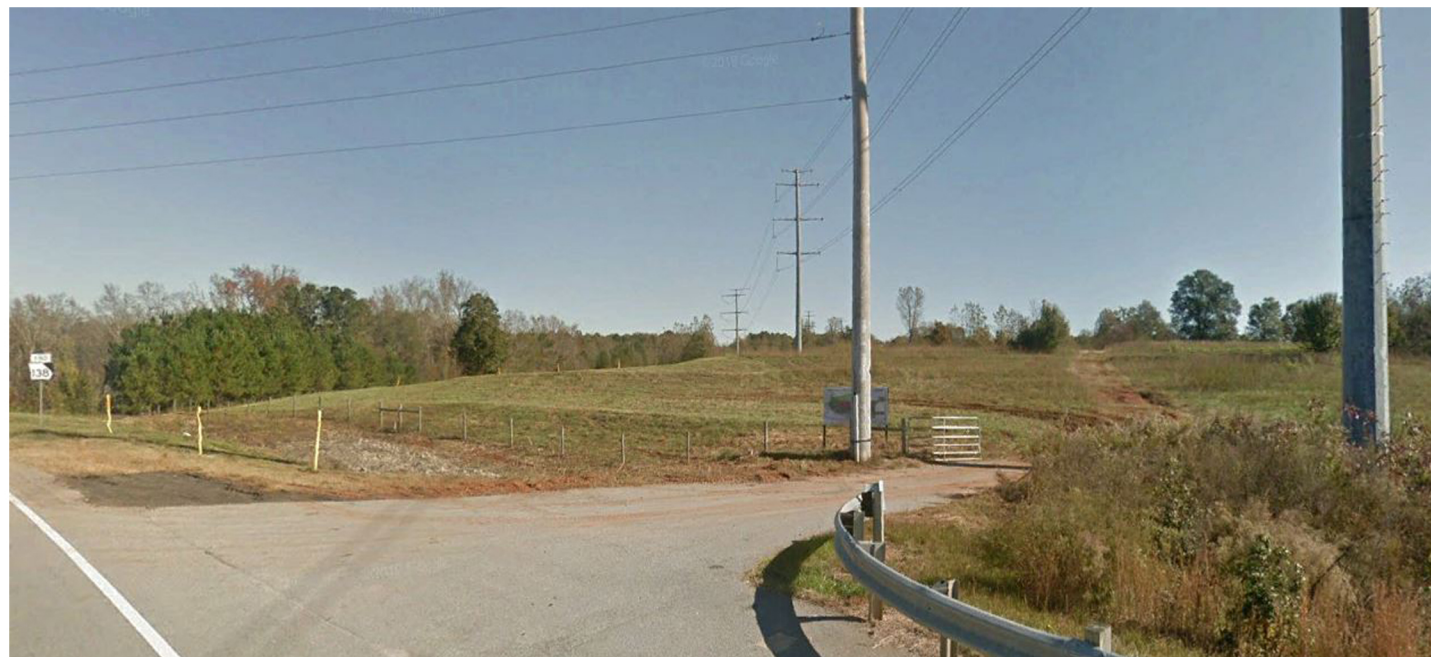
Key Map



Picture 2



Picture 1

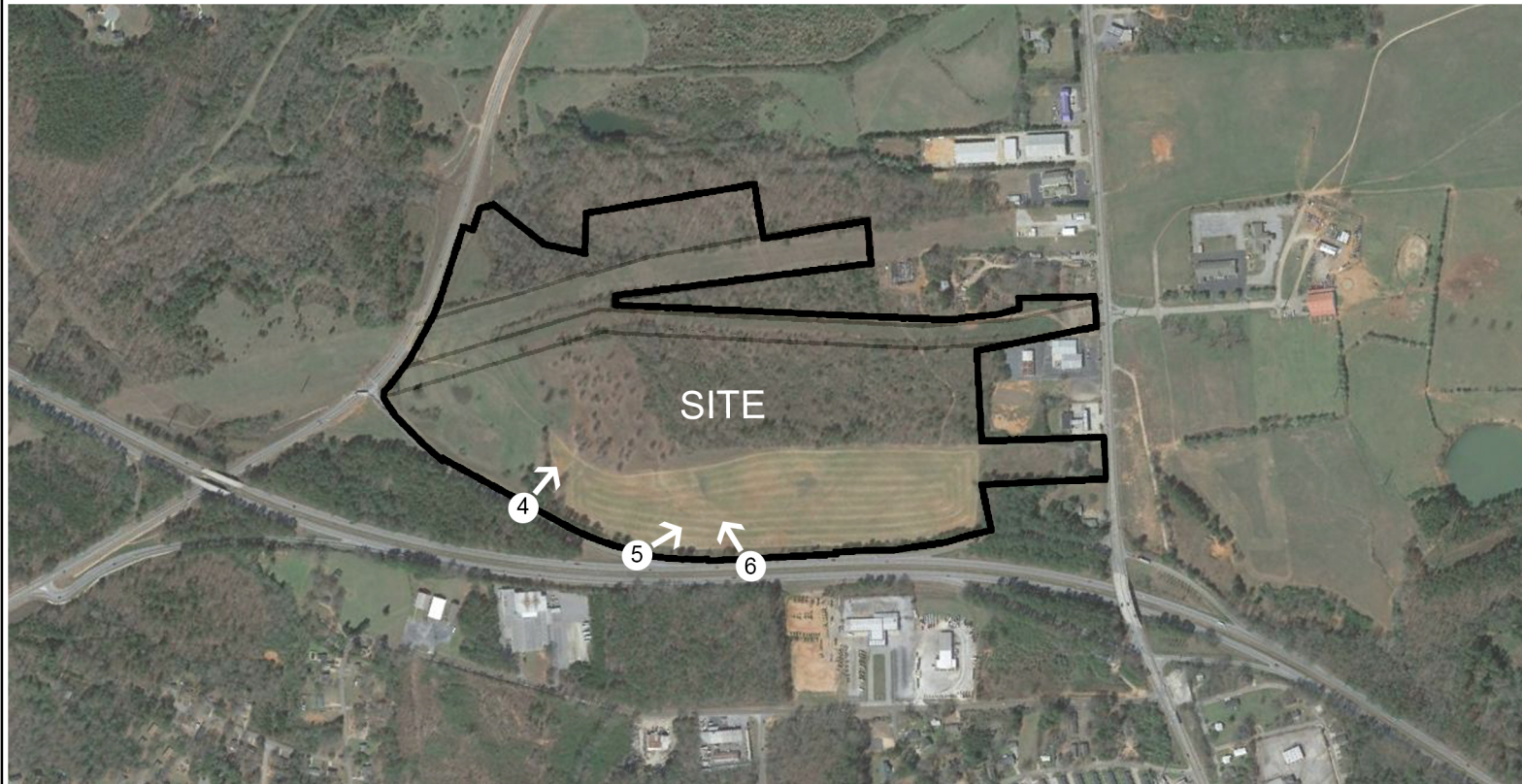


Picture 3



EXISTING CONDITIONS PHOTOS

Key Map



Picture 5



Picture 4

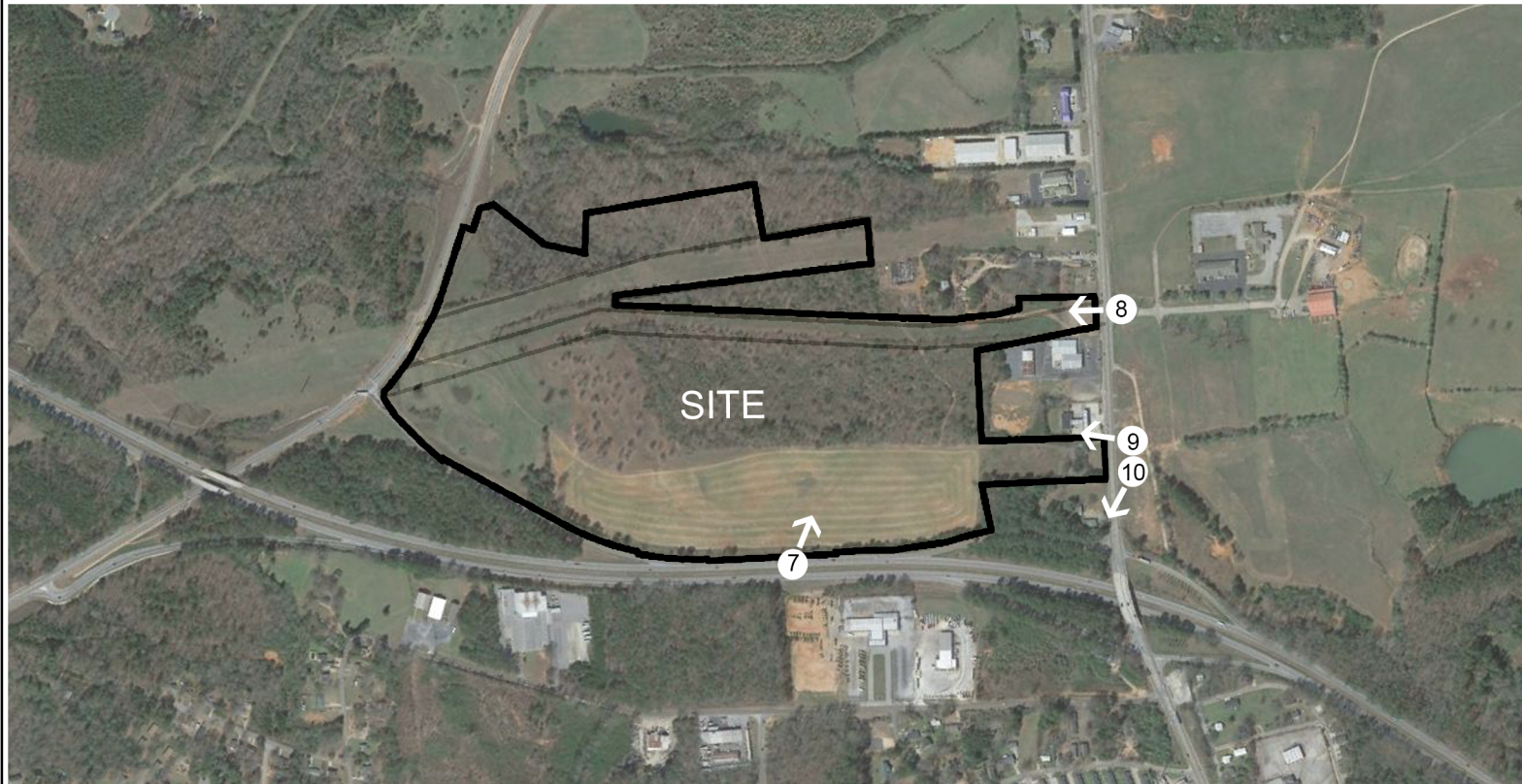


Picture 6



EXISTING CONDITIONS PHOTOS

Key Map



Picture 8



Picture 9



Picture 10

Picture 7



PROJECT OVERVIEW

Overview

Monroe Pavilion is a Planned Community Development that provides a diverse commercial center with retail, restaurants, hotels, services and entertainment to serve the citizens of Monroe and the surrounding communities in Walton County. The Master Plan, as shown on Page 2.3, reflects the development of 215,157 square feet of new retail and restaurant buildings within the 48.93-acre Shopping Center Tract, twelve (12) outlots, of which one (1) is planned to be a multi-story hotel and one (1) is planned to be a multi-story hotel or office, and the potential for future development of 150,431 square feet in building expansions and new buildings within the Shopping Center Tract and the Future Major G Parcel.

The Development also includes a new collector street, currently called “Retail Boulevard”, which will provide an east-west connection between Georgia Highway 11 and Charlotte Rowell Boulevard. The new “Retail Boulevard” will be more than 5,000 linear feet in length, will have a 70-foot right-of-way width, will be lined with street trees and sidewalks and will be dedicated to the City of Monroe. Design of the “Retail Boulevard” will include traffic calming measures such as a roundabout at the three-way intersection west of Georgia Highway 11 and two (2) raised intersections at main entry points to the Development from the “Retail Boulevard”. The outlots within the Development shall have shared access points along the “Retail Boulevard” as depicted in the Master Plan and shall be required by the Developer to have cross access connectivity between adjoining outlots.

Pedestrian connectivity is provided throughout the Development with five-foot wide public sidewalks along the new “Retail Boulevard” and four-foot wide internal sidewalks connecting the proposed buildings to the “Retail Boulevard”. These sidewalks along the “Retail Boulevard” will also provide pedestrian connectivity to the proposed outlots within the Development and each outlot, as it is developed, shall provide their own four-foot wide sidewalk connection to the public sidewalk at the “Retail Boulevard”.

The Major G Parcel will be developed in a future phase of the Development. The layout for this parcel as shown in the Master Plan is conceptual in nature and is subject to changes once a tenant has been identified. The conceptual layout currently reflects a large retail building with a pad building fronting the “Retail Boulevard”. The proposed buildings will have parking that is shared between the different uses.

Monroe Pavilion promotes the City’s goal of open space conservation through the proposed landscaping within the Development, streetscape along the new “Retail Boulevard” and preservation of the green space on the north side of the Development due to the existing Williams Transcontinental Gas Pipeline easement and the MEAG Transmission Powerline Right-of-Way that both traverse from the east to the west across the Development. Also a part of the open space at Monroe Pavilion will be master stormwater detention facilities that will serve the entire Development, including the Shopping Center Tract, new “Retail Boulevard”, Future Major G Parcel and all outlots. At final buildout, the 95.48-acre Monroe Pavilion development will have more than the 40% open space required by the City’s Zoning Ordinance.

OVERALL SITE DATA

TOTAL SITE AREA	95.48 Acres
EXISTING ZONING	B-3 (Highway Commercial District)
OVERLAY DISTRICT	Corridor Design Overlay District
SHOPPING CENTER TRACT	48.93 Acres
FUTURE MAJOR G PARCEL	11.58 Acres
DEDICATED COLLECTOR R.O.W.	10.07 Acres
OUTLOTS	24.79 Acres
PROPOSED BUILDING AREA	215,157 SF
FUTURE BUILDING AREA	150,431 SF

OUTLOTS

HOTEL/OFFICE 1	6.54 Acres
HOTEL 2	3.82 Acres
OUTLOT 1	2.06 Acres
OUTLOT 2	1.64 Acres
OUTLOT 3	1.51 Acres
OUTLOT 4	1.57 Acres
OUTLOT 5	1.54 Acres
OUTLOT 6	1.32 Acres
OUTLOT 7	1.38 Acres
OUTLOT 8	1.28 Acres
OUTLOT 9	1.07 Acres
OUTLOT 10	1.06 Acres

Note: Outlot areas are subject to change.

BUILDING AREAS

MAJOR A	48,387 SF
MAJOR B	18,000 SF
MAJOR C	21,000 SF
MAJOR D	6,400 SF
MAJOR E	20,000 SF
MAJOR F	11,700 SF
SHOPS A	21,700 SF
SHOPS B	4,200 SF
JUNIOR A	10,000 SF
JUNIOR B	8,470 SF
JUNIOR C	6,000 SF
JUNIOR D	5,000 SF
JUNIOR E	5,000 SF
JUNIOR F	5,000 SF
PAD 1	5,000 SF
PAD 2	5,000 SF
PAD 3	5,000 SF
PAD 4	8,000 SF
FUTURE MAJOR G	120,000 SF
FUTURE PAD 5	4,800 SF
FUTURE EXPANSIONS	25,631 SF

Note: Building areas and tenant square footage is subject to change based on executed lease agreements obtained by the Developer.

VICINITY MAP RENDERING



MASTER PLAN

Site



ZONING ORDINANCE & OVERLAY DISTRICT ADHERENCE

The Master Plan for the Monroe Pavilion Planned Community Development (PCD), as illustrated on Page 2.3, is governed by the City of Monroe Zoning Ordinance adopted June 10, 2014 and last amended May 8, 2018. The subject 95.48-acre site is currently zoned B-3, Highway Commercial District, and is within the Corridor Design Overlay District (CDO). The Master Plan has been developed to meet the zoning ordinance and overlay requirements, and, where certain requirements cannot be met, the spirit of the ordinance has been applied. The following is a discussion of how the Monroe Pavilion Master Plan complies with requirements while allowing compromises to meet current developer and tenant requirements and needs. Future parcels and outlots shall meet the same requirements as shown and stated within this Pattern Book.

Section 430 – One Principle Structure on a Lot

This section allows only one (1) principle structure and permitted customary accessory buildings on any lot. The Shopping Center Tract and Future Major G Parcel shall be allowed to have multiple buildings within their respective lots as reflected on the Master Plan. Each of the twelve (12) outlots shall comply with this section by having one (1) principle structure.

Section 520 – Off-Street Automobile Parking

520.2(1) Minimum Size of Off-Street Parking Space

This section requires a minimum parking space size of nine (9) feet wide by twenty (20) feet long with a minimum area of 180 SF. The Master Plan shows all 60-degree parking spaces sized at ten (10) feet wide by eighteen (18) feet long and 90-degree parking spaces sized at nine (9) feet wide by eighteen (18) feet long.

520.3 – Parking Space Requirements for All Districts

Per Table 3 of the Zoning Ordinance, the following are minimum required parking spaces based on use:

- Retail Establishments: 1 space/employee + 1 space/500 SF
- Restaurants: 1 space/5 seats + 1 space/600 SF

This section also requires that the maximum number of parking spaces shall not exceed 120% of the minimum required spaces. The Master Plan currently shows 1406 parking spaces within the Shopping Center Tract that will be shared between the multiple proposed buildings and uses. Per the Parking Analysis on this page, the Shopping Center Tract complies with this section of the ordinance. However, due to standard tenant requirements anticipated at each standalone outlot, the maximum number of parking spaces allowed within the outlots shall be 135% of the minimum required spaces.

520.4(6) – Site Requirements: Landscaping

This section requires that for all parking areas with ten (10) or more parking spaces shall have a minimum of 15% of the interior parking area permanently landscaped. The CDO requirement of 12% interior landscaped area, as stated in Section 643A.4(1)(c)(iv)(c), shall be the requirement for the Monroe Pavilion Development. For the Shopping Center Tract, the Master Plan currently shows approximately 13% of the interior parking area to be permanently landscaped area.

520.4(10) – Site Requirements: Sidewalk

This section requires all parking facilities to have five (5) foot wide sidewalks connecting the facility to all public right of way sidewalks and building entrances. For the Monroe Pavilion Development, all public sidewalks shall be five (5) feet wide and all private internal sidewalks shall be four (4) feet wide.

Section 550 – Screening, Buffering, Landscaping

550.4(3) – Required Landscaping: Thoroughfare Landscaping

This section requires that a 25-foot landscape area about the right-of-way of all roads within the City limits. The CDO requirement of a frontage landscape area, as stated in Section 643A.4(2), is required to be 20 feet deep. As depicted in the Master Plan and in the Vignettes within this Pattern Book, the Shopping Center Tract, Major G Parcel and each outlot shall provide a minimum 20-foot deep frontage landscape area as measured from the edge of the public sidewalk to the back of curb at the internal parking along all road rights-of-way.

SHOPPING CENTER TRACT PARKING ANALYSIS

RETAIL PARKING CALCULATIONS

BUILDING	AREA	EST. EMPLOYEES
MAJOR A	48,387 SF	145
MAJOR B	18,000 SF	27
MAJOR C	21,000 SF	43
MAJOR D	6,400 SF	58
MAJOR E	20,000 SF	18
MAJOR F	11,700 SF	43
SHOPS A-Retail	13,700 SF	45
SHOPS B	4,200 SF	24
JUNIOR A	10,000 SF	15
JUNIOR B	8,470 SF	23
JUNIOR C	6,000 SF	20
JUNIOR D	5,000 SF	15
JUNIOR E	5,000 SF	15
JUNIOR F	5,000 SF	15
Total	184,157 SF	506

RESTAURANT PARKING CALCULATIONS

BUILDING	AREA
SHOPS A-Restaurant	8,000 SF
PAD 1	5,000 SF
PAD 2	5,000 SF
PAD 3	5,000 SF
PAD 4	8,000 SF
Total	31,000 SF

Estimated Seats 1,343

RETAIL PARKING REQUIRED:	875
RESTAURNAT PARKING REQUIRED:	321
TOTAL PARKING REQUIRED:	1196
MAXIMUM PARKING ALLOWED:	1435

Note: Building areas and tenant square footage is subject to change based on executed lease agreements obtained by the Developer.

ZONING ORDINANCE & OVERLAY DISTRICT ADHERENCE

Section 570 – Lot Coverage

This section defines that the maximum impervious lot coverage in all zoning districts shall be 60% with the exception of the CBD Downtown District. For this 93.48-acre Development, there are only three (3) tracts that can independently meet this requirement. Therefore, this 93.48-acre Development shall be considered as a whole when evaluating lot coverage and each tract shall have a lot coverage as defined in the Lot Coverage table found on this page. With the lot coverages provided, the overall lot coverage for the Development will be less than the allowed maximum of 60%.

LOT COVERAGE

SHOPPING CENTER TRACT	51%
DEDICATED COLLECTOR R.O.W.	50%
FUTURE PARCEL G	85%
HOTEL/OFFICE 1	40%
HOTEL 2	65%
OUTLOT 1	80%
OUTLOT 2	80%
OUTLOT 3	80%
OUTLOT 4	80%
OUTLOT 5	80%
OUTLOT 6	80%
OUTLOT 7	80%
OUTLOT 8	80%
OUTLOT 9	90%
OUTLOT 10	90%

OVERALL PROJECT: <60%

Section 643 – Corridor Design Overlay District (CDO)

Section 643A – Corridor Design Standards and Guidelines

643A.1(2) – Site Planning: Area Specific Criteria

This section requires that the Corridor shall have an integrated streetscape of a scale and character consistent with City’s traditional development pattern with walkways connecting buildings with the public right-of-way sidewalk network. The Master Plan reflects extensive streetscape and sidewalks along the new “Retail Boulevard” and internal sidewalk networks connecting each proposed building to the “Retail Boulevard” within the Shopping Center Tract. As the Major G Parcel and all outlots are developed, they will expand the Development’s sidewalk network along the “Retail Boulevard” right-of-way they have frontage on and internally on their respective lots. As defined above, all public sidewalks shall be five (5) feet wide and all private internal sidewalks shall be four (4) feet wide.

643A.2(2) – Architecture: Area Specific Criteria

This section requires that properties greater than three (3) acres should reserve two (2) or more outparcels for future development to insert buildings closer to the roadway. The Shopping Center Tract, Major G Parcel, Hotel 1 and Hotel 2 are the only tracts that are larger than 3 acres. As depicted on the Master Plan, the Shopping Center Tract has four (4) pad buildings that, although they will not be subdivided as outlots, they act as outlots as they are close to the “Retail Boulevard” roadway. The Major G Parcel has one (1) pad building that acts as an outlot in keeping with this zoning requirement. Hotel 1 and Hotel 2 will not be required to comply with this section.

643A.3(1)(d) – Pavement: Sidewalk Networks

This section requires that all developments within the CDO construct six (6) foot wide public sidewalks on both sides of the public roadway. As discussed above, the Master Plan reflects an extensive sidewalk network along the new “Retail Boulevard”. At the time of construction of the Shopping Center Tract and the “Retail Boulevard” a five (5) foot public sidewalk will be constructed on the north side of the “Retail Boulevard”. As the Major G Parcel, Hotel/Office 1, Hotel 2, and Outlots 1-8 are developed, they will expand the Development’s sidewalk network on the south side of the “Retail Boulevard” right-of-way that they have frontage on by constructing a five (5) foot wide sidewalk. The exception to this will be Outlots 9 and 10 which due to limits of lot size will not be required to construct a sidewalk along their frontage. All public sidewalks are depicted on the Master Plan.

643A.3(2) – Pavement: Area Specific Criteria

This section allows additional frontage parking for planned centers like the Monroe Pavilion Planned Community Development if the center has a front building setback of 200 feet, if all landscape criteria is met, if there are 2 or more frontage outparcels and if a sidewalk network is provided. As discussed above for each of these items the Monroe Pavilion Development adheres to these requirements. Therefore, frontage parking is shown on the Master Plan for the Shopping Center Tract and Major G Parcel.

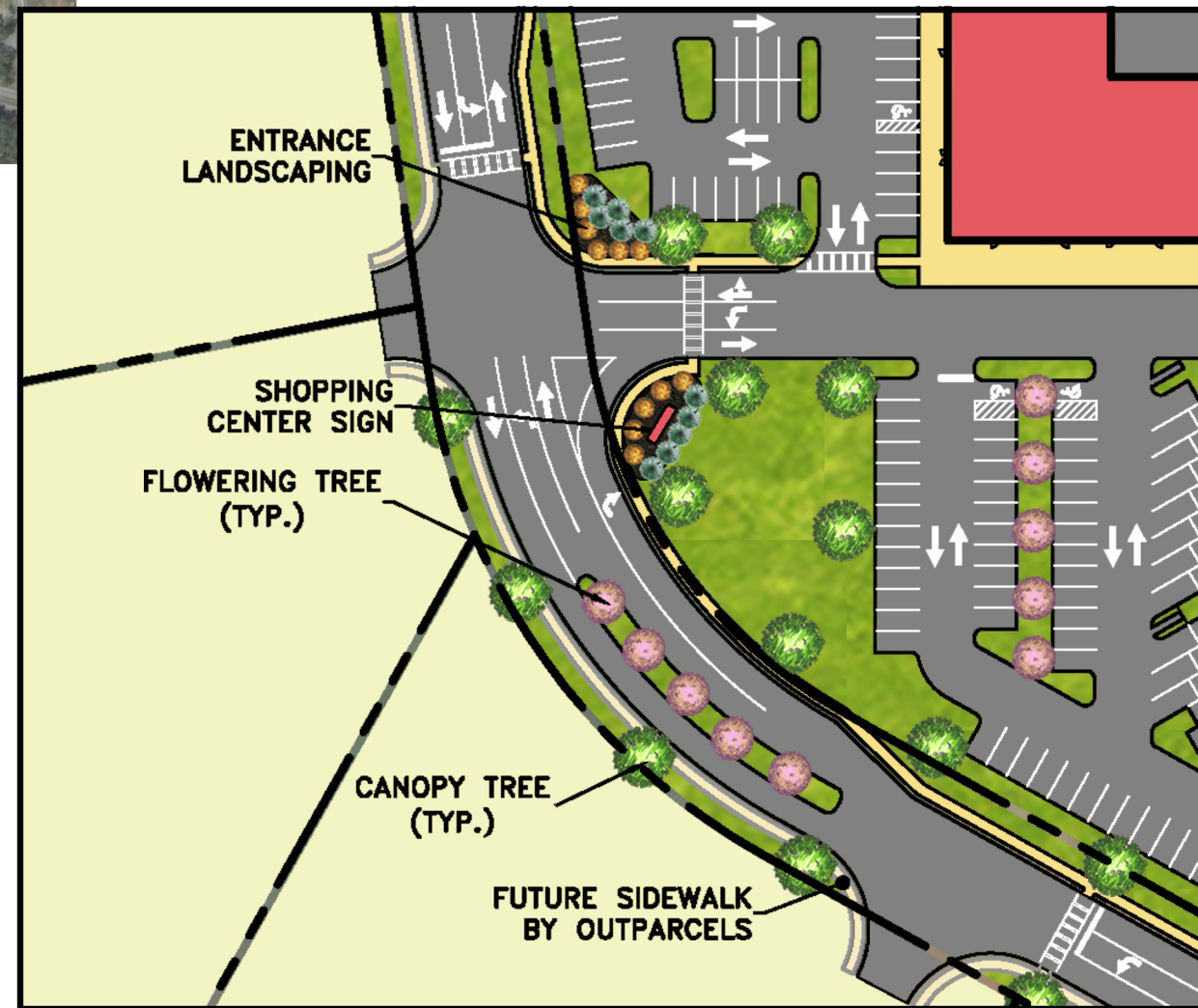
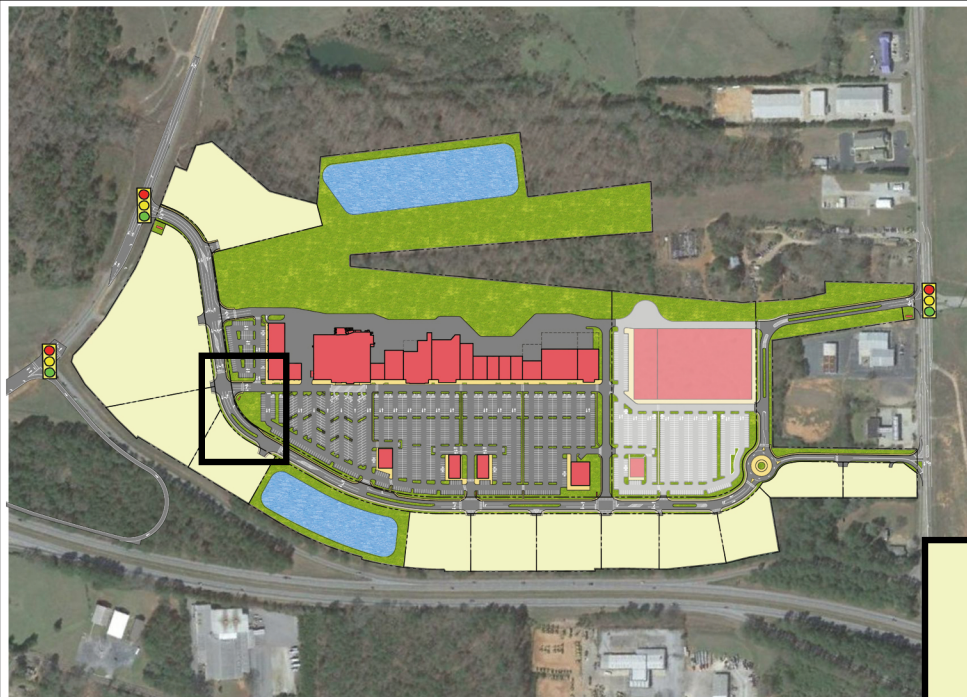
643A.4(1)(c)(iv)(c) – Landscaping: Interior

This section requires that for all parking lots with 15 or more parking spaces a minimum of 12% interior landscaped area be provided within the interior parking. For the Shopping Center Tract, the Master Plan currently shows approximately 13% of the interior parking area to be permanently landscaped area. All other tracts within the Development shall adhere to this requirement. This section also states that one (1) in four (4) parking rows should have 15-foot wide landscape area dividing the parking rows. The Shopping Center Tract shall provide the required 15-foot wide landscape areas at an interval and as is depicted on the Master Plan.

643A.4(2) – Landscaping: Specific Criteria

This section requires that frontage landscape areas be a minimum of 20 feet in depth. As depicted in the Master Plan and in the Vignettes within this Pattern Book, the Shopping Center Tract, Major G Parcel and each outlot shall provide a minimum 20-foot deep frontage landscape area as measured from the edge of the public sidewalk to the back of curb at the internal parking along all road rights-of-way.

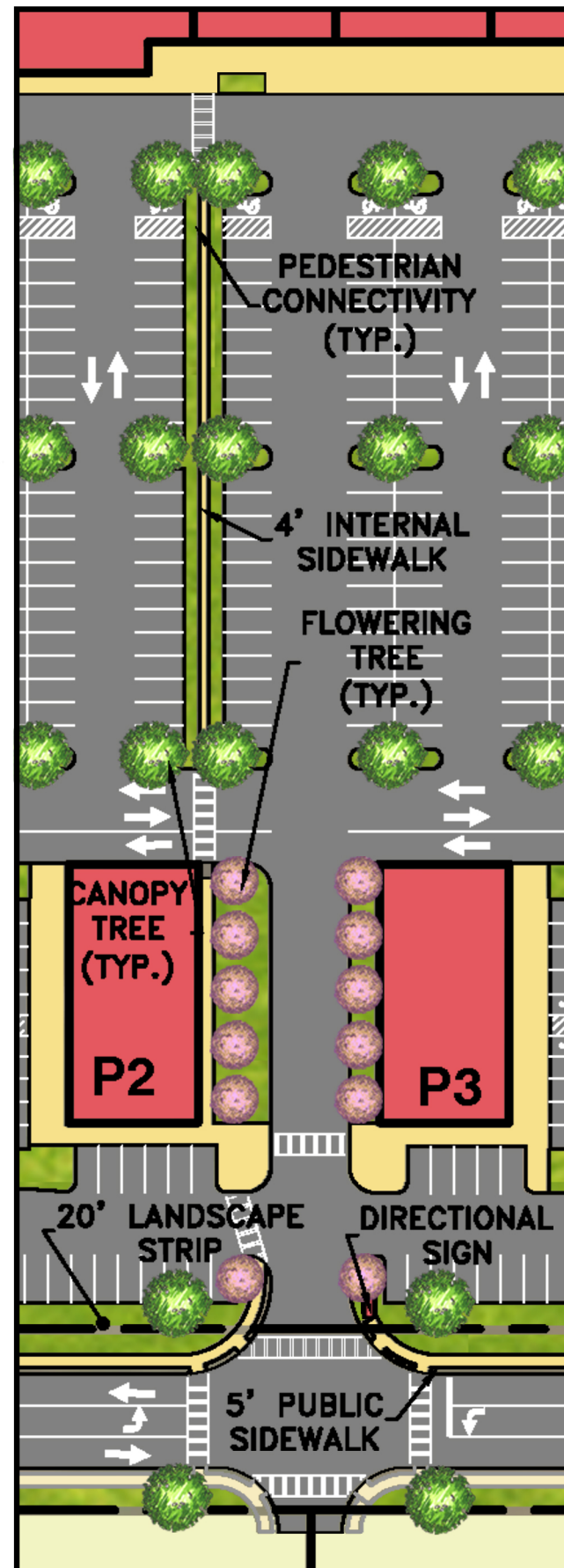
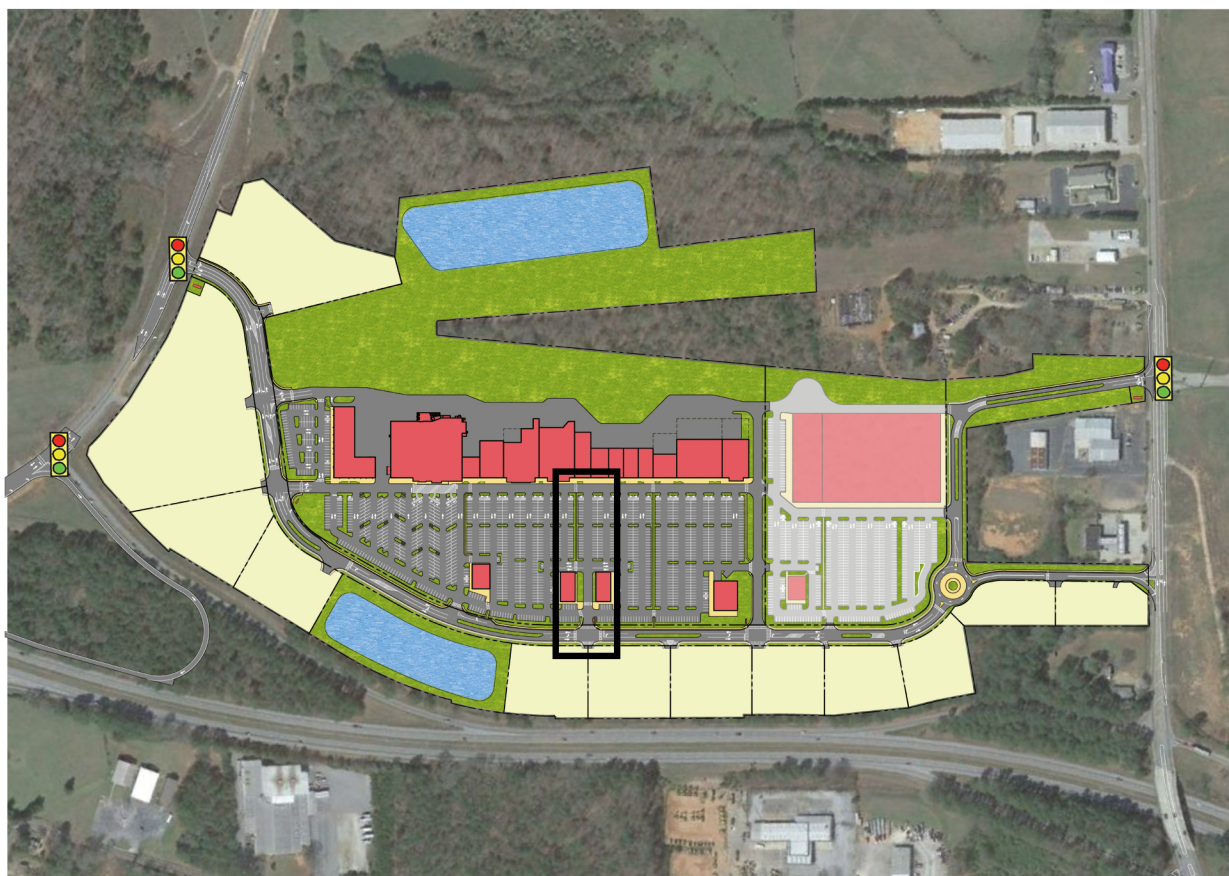
VIGNETTE 1



Not to Scale

The master planned development shall include substantial landscape areas which enhance the experience to motorists and pedestrians. An emphasis on detailed landscape plantings at major intersections and entry points shall contribute to the feeling of a destination center. The canopy street trees will be planted at an interval of 75' on center and will be installed on either side of the boulevard to provide continuity as the outparcels are developed.

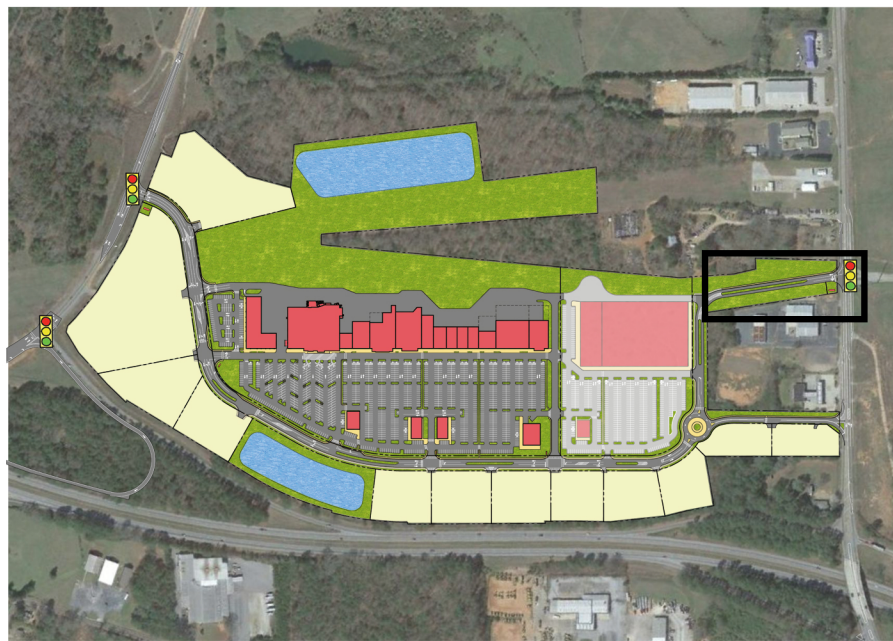
VIGNETTE 2



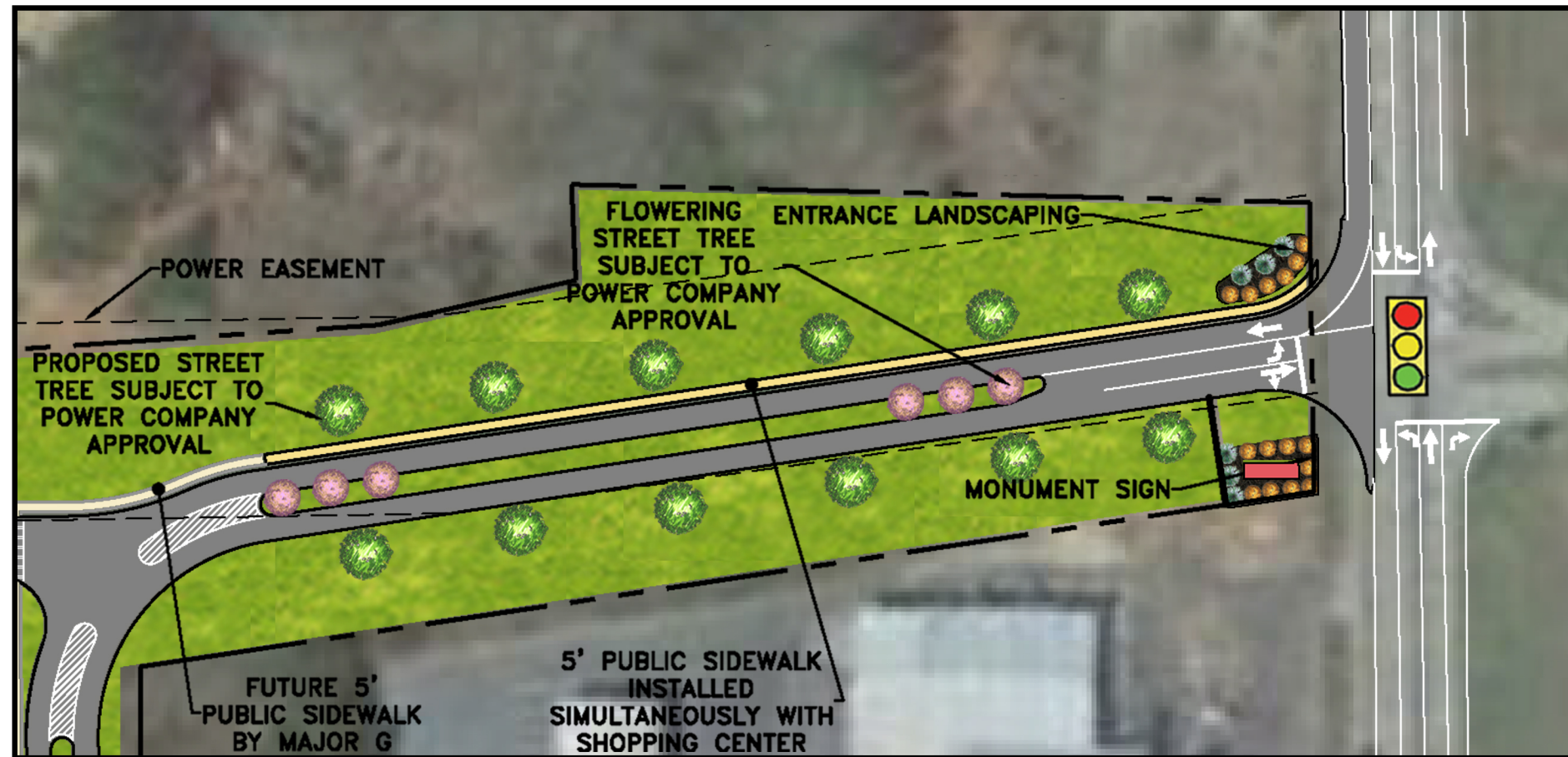
Pedestrian connectivity is at the forefront of design at Monroe Pavilion. Pedestrian access extends from the Retail Blvd sidewalk to the main retail building by running walkways adjacent to buildings and inside of landscaped medians. This route promotes safety and synergy among street front developments and to the primary commercial spaces behind them.

Not to Scale

VIGNETTE 3

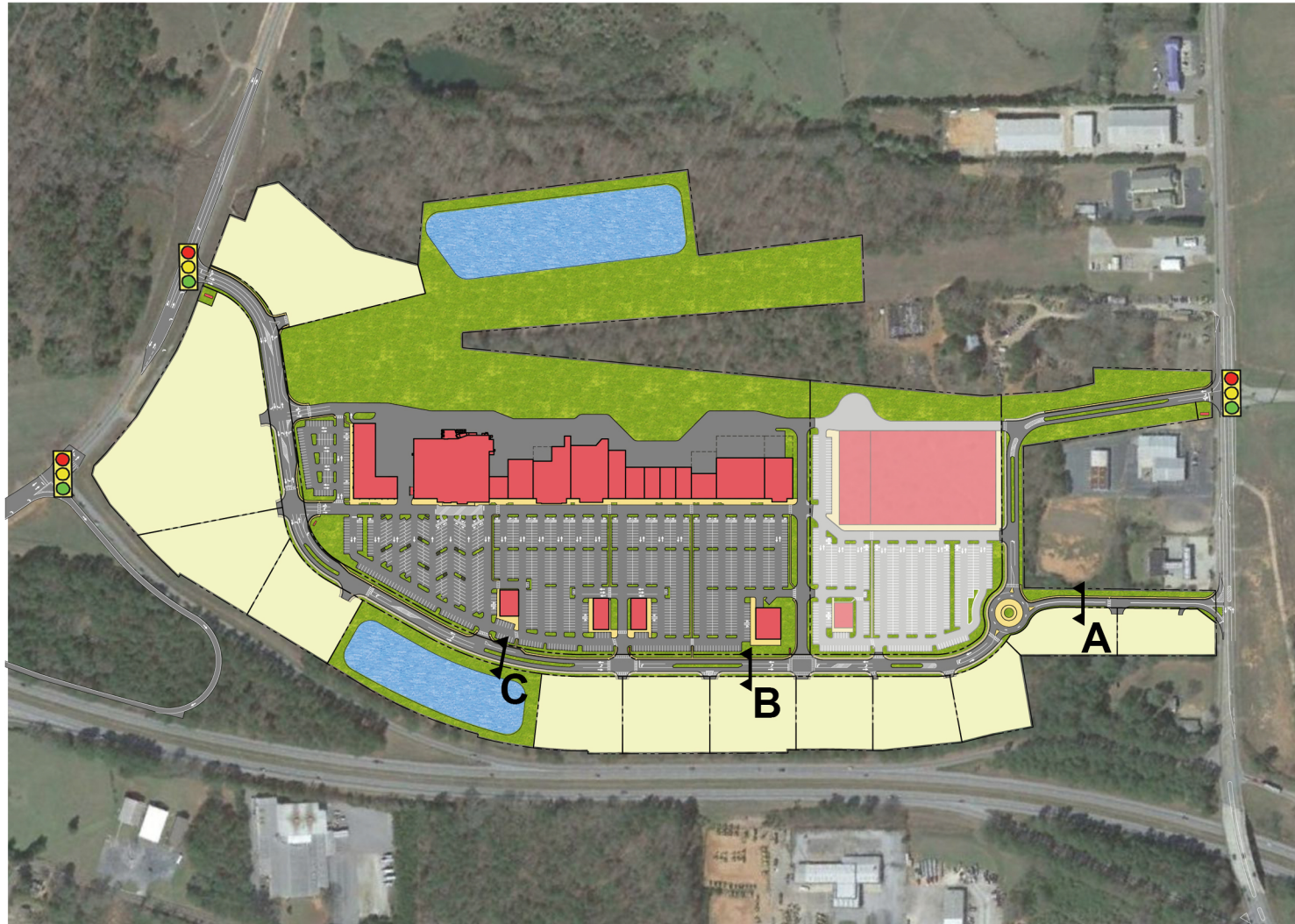


Not to Scale

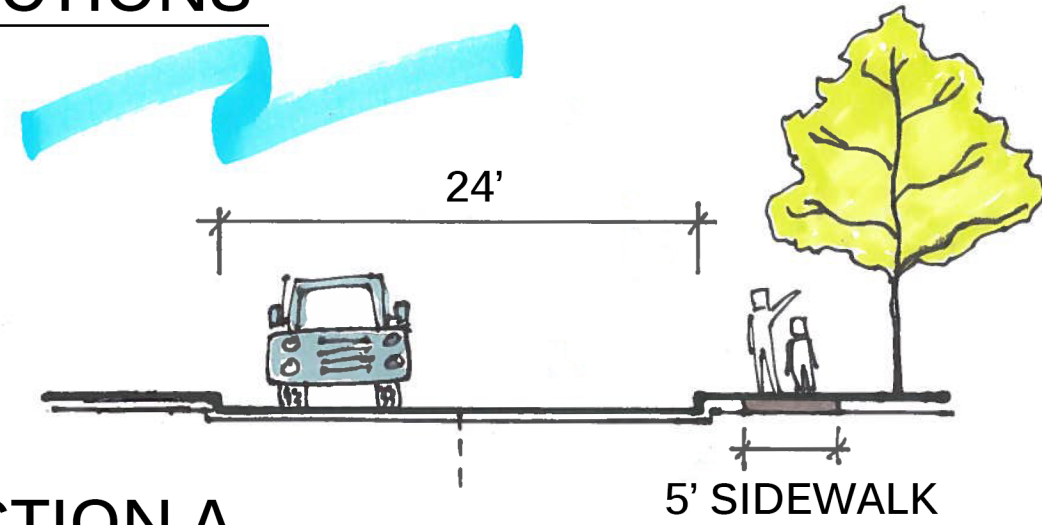


A tree lined boulevard will connect Georgia Highway 11 to Charlotte Rowell Boulevard. Canopy trees at regular intervals of 75' on center spacing will create rhythm and shade for motorists and pedestrians. Planted medians will provide multi-season interest.

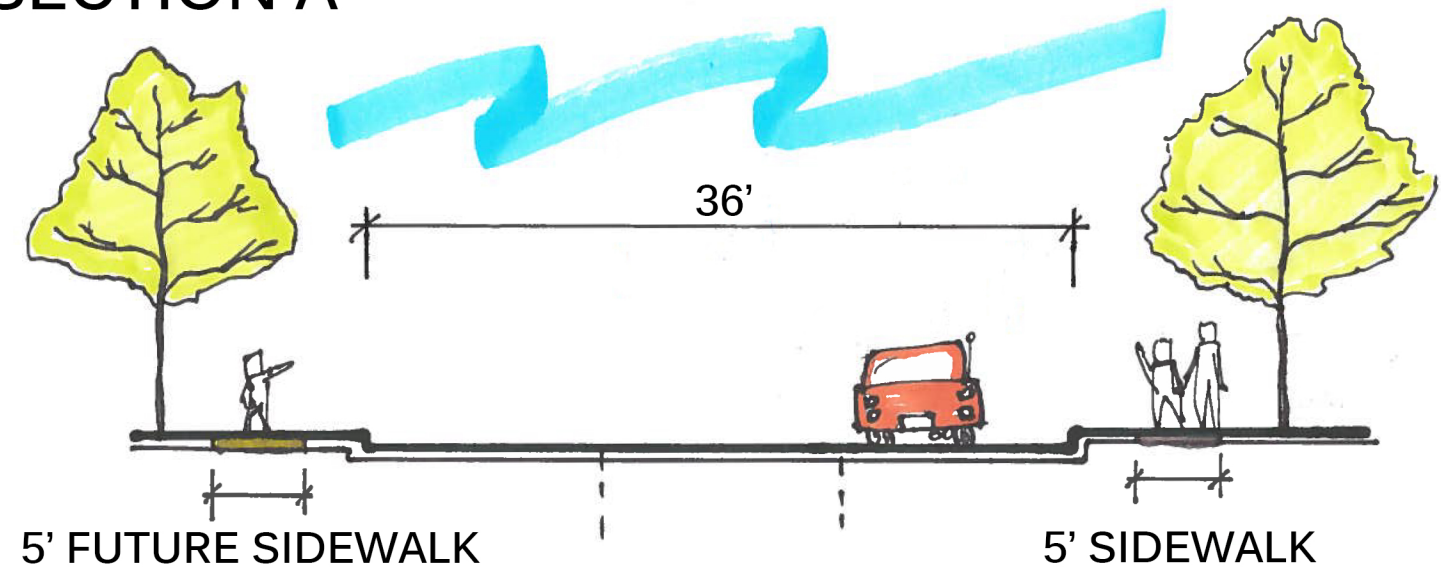
BOULEVARD SECTIONS



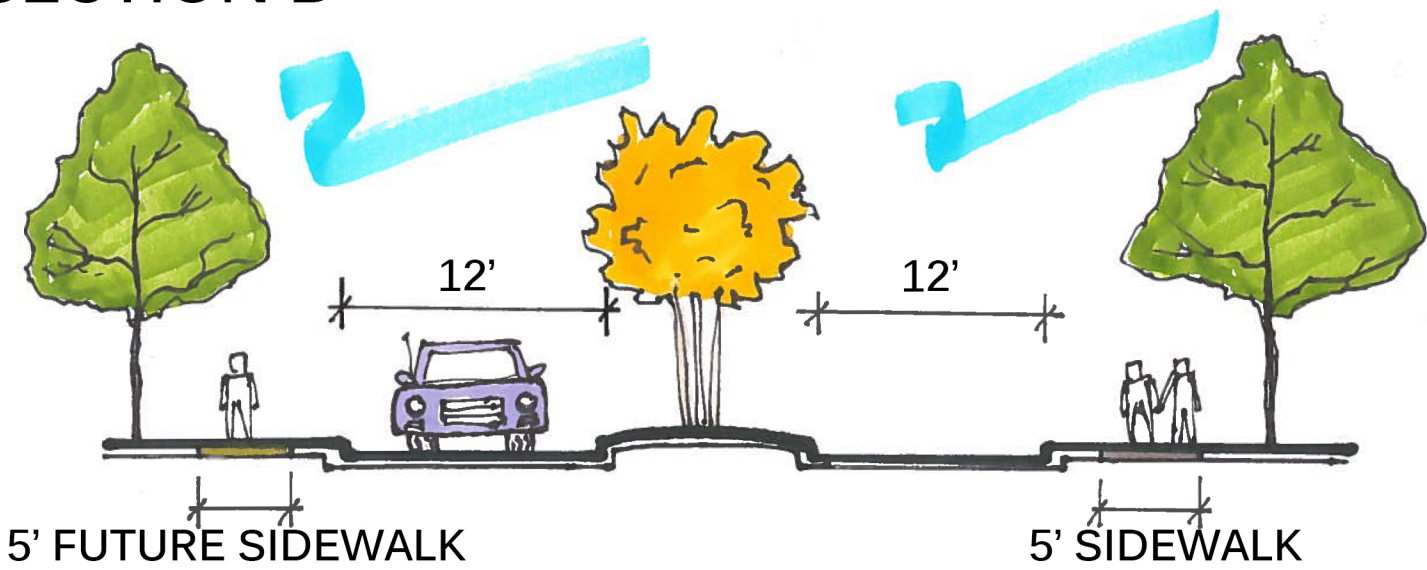
SECTION A



SECTION B



SECTION C



3.1
INTRODUCTION:

ARCHITECTURAL INTRODUCTION

Introduction:

This Pattern Book demonstrates a Planned Community Development which is a compromise to the current City of Monroe Zoning Ordinance and Corridor Design Overlay (CDO) within the Highway Commercial District (B-3). Throughout this book, this Planned Community Development shall also be referred to as PCD. This Pattern Book will describe and revise some sections of the current City of Monroe Zoning Ordinance, Corridor Design Overlay (CDO), and Highway Commercial District (B-3), to allow for a commercial shopping center development for future tenants and development plans to cohesively and conform as closely as possible to the City of Monroe Zoning Ordinance, Corridor Design Overlay (CDO), and Highway Commercial District (B-3), while allowing compromises to meet current developer and tenant(s) requirements and needs. Future Parcels & Outlots will meet the same architectural building requirements, materials, fenestrations, ornamentation, and signage requirements as shown and stated within this Pattern Book. This Pattern Book addresses REVISED items/wording within the current City of Monroe Zoning Ordinance and does not address items/wording that are of mutual agreement.

MATERIALS

Article V: GENERAL STANDARDS FOR ALL DISTRICTS

SECTION 570 - LOT COVERAGE

570.2 EXPANSION PROJECTS (page 26):

(1) Definition: Future building expansion not counted as expansion as long as illustrated on sheet 2.3.

ARTICLE VI: USE OF PROVISIONS BY DISTRICT

SECTION 643A - CORRIDOR DESIGN STANDARDS AND GUIDELINES

643A.2 ARCHITECTURE (page 49):

1) General Criteria for Conceptual Planning:

a) The general architecture of this new Planned Community Development with retail, grocery, restaurants, and services, will be designed with harmonious elements that create a character that is unique upon itself and is reflective of a gateway into the City of Monroe while using some materials that are common with the traditional and/or historic built environment, Fenestration and Ornamentation.

c) In areas where new structures are adjacent to and visible from roadways, architecture shall adhere to the standards set forth below:

(vii) Materials: Brick will be used throughout on every building in the entire project consistent with meeting the corporate identity of developments. Stucco (DFS) is a common product used in the tenant's prototype designs and shall be used in accordance to bring Major Tenants into this center affordably. Aluminum siding and vinyl siding will not be used anywhere. Wood siding and/or fiber cement products may be considered as architectural features on some buildings. E.I.F.S. will be used at cornice locations and on all sign bands for Anchor tenants.

PRIMARY & ACCESSORY MATERIAL	
PRIMARY	ACCESSORY
BRICK	STONE
STUCCO	FIBER CEMENT
DFS	FABRIC (AWNINGS)
	STANDING SEAM METAL (ROOFING & CANOPIES)
	EIFS
	STOREFRONT, WINDOWS, GLASS

FENESTRATIONS, & ORNAMENTATION

Article V: GENERAL STANDARDS FOR ALL DISTRICTS

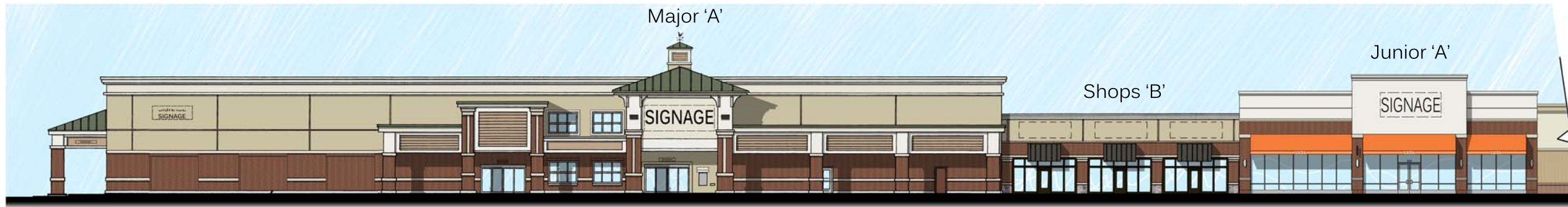
SECTION 570 - LOT COVERAGE

570.2 EXPANSION PROJECTS (page 26):

(viii) Fenestration: The spacing and placement of architectural bays and openings, and the solid-to-void ratio (wall-to-window), shall conform to keep façades from expanding longer than 100 feet in linear length by interrupting this linear length with fenestration. No uninterrupted length of façade shall exceed 100 feet. The façade of a main building facing a public street shall feature at least one public entrance and be defined as the façade that will meet Fenestration requirements. The other 3 sides of the building is not required to meet the fenestration requirements. Where a main building faces more than one public street, an entrance for each side may be proposed and then, each public entrance side will meet Fenestration requirements. To avoid box-like buildings, windows and other features are important to obtain the solid to void ratio. Facades that face public streets shall have arcades / canopies, display windows, exterior building façade accent elements, entry areas, awnings, recessed accent panels or other similar features along no less than 60% of the entire length of the building along the length of walkway span, not per individual tenant suite. Where additional establishments will be located within or nearly adjacent to the principal building, the solid to void criteria set forth above apply to each building façade along one length of walkway span. These requirements will also apply to each individual free-standing building front façade in a future Parcel or Outlot. Windows shall be recessed and shall include visually prominent framing features. Building façades on outlots facing U.S. Highway 78 are required to meet fenestration standards except no main entrance is required. The tenant's corporate identity needs to be protected per their design.

(ix) Ornamentation: Detailing found upon local buildings should serve as a model for new design elements, including but not limited to decorative masonry work, accent masonry, accent stucco elements, brackets, knee brackets, etc. Limited use of architectural masonry blocks may be used or considered for the creation of building details. Glass block will not be used anywhere. Building public entrance facades will include an expression of architectural or structural bay through a change in plane, such as an offset, stucco reveal, projecting rib, pilaster, or column. At least one of these elements shall repeat horizontally per tenant at an interval of not more than 30 feet. The façade of a main building facing a public street shall feature at least one public entrance and be defined as the façade that will meet Ornamentation requirements. The other 3 sides of the building is not required to meet the ornamentation requirements. Where a main building faces more than one public street, an entrance for each side may be proposed and then, each public entrance side will meet Ornamentation requirements. Projected panels for wall mounted signage is not required but preferred. Building façades on outlots facing U.S. Highway 78 are required to meet ornamentation standards except no main entrance is required. The tenant's corporate identity needs to be protected per their design.

RETAIL BUILDING OVERVIEW



Architecture

- This Planned Community Development book is a compromise to the current City of Monroe Zoning Ordinance and Corridor Design Overlay (CDO) within the Highway Commercial District (B-3).
- In compliance with the PCD, we will use some of the City's traditional materials and elements that include brick, columns, pilasters, defined framing around windows and doors, fenestration elements, and ornamentation to give interesting features to the buildings and to break up the facades with pleasing aspects. Parapets will be used throughout to eliminate direct visibility of flat roofs. For additional information, refer to each individual tenant sheet in this PCD submittal.
- If elements and features are not addressed in this PCD document then refer to the City of Monroe Zoning Ordinance and Corridor Design Standards and Guidelines.

Summary

In compliance with the PCD, we will use some of the City's traditional materials and elements that include brick, columns, pilasters, defined framing around windows and doors, fenestration elements, and ornamentation to give interesting features to the buildings and to break up the facades with pleasing aspects. Parapets will be used throughout to eliminate direct visibility of flat roofs. For additional information, refer to each individual tenant sheet in this PCD submittal.

TENANT MAJOR 'A'

Architecture



FRONT ELEVATION

FENESTRATIONS:

241'-0" TOTAL LINEAR FEET (LF)
166'-7" FENESTRATION LF

$166\text{'-}7\text{'}/241\text{'-}0\text{'}$ = 69% FENESTRATIONS (MEETS THE 60% MIN. FENESTRATIONS REQUIRED)

15'-0" = MAXIMUM LF WITHOUT FENESTRATIONS (MEETS THE 100' MAX. LF REQUIREMENT)

ORNAMENTATION:

VERTICAL REVEALS, BRICK ACCENT AREAS, FIBER CEMENT PANELS, AND COLUMNS ARE LESS THAN 30'-0" IN SPAN ACROSS ENTIRE LENGTH (MEETS THE 30'-0" MAXIMUM SPAN WITHOUT ORNAMENTATION REQUIREMENT)

EXTERIOR FINISH SCHEDULE

MARK	CATEGORY	MANUFACTURER	DESCRIPTION / FINISH
B-1	MODULAR BRICK	ACME BRICK	COLOR: CHERRYWOOD WITH CEMEX IVORY MORTAR, TOOLED JOINTS
B-2	MODULAR BRICK	TAYLOR	COLOR: 301w WHITE WITH CEMEX IVORY MORTAR, TOOLED JOINTS
DF-4	STUCCO	DRYVIT	FINISH SANDBLAST, COLOR TO MATCH: SW 6156 "RAMIE"
EF-1	EIFS	DRYVIT	FINISH SANDBLAST, COLOR TO MATCH: SW 7005 "PURE WHITE"
EF-3	EIFS	DRYVIT	FINISH SANDBLAST, COLOR TO MATCH: SW 6154 "NACRE"
SC-2	STUCCO	DRYVIT	FINISH SANDBLAST, COLOR TO MATCH: SW 6094 "SENSATIONAL SAND"
SC-3	STUCCO	DRYVIT	FINISH SANDBLAST, COLOR TO MATCH: SW 6154 "NACRE"
SC-4	STUCCO	DRYVIT	FINISH SANDBLAST, COLOR TO MATCH: SW 6156 "RAMIE"
SC-5	STUCCO	DRYVIT	FINISH SANDBLAST, COLOR TO MATCH: SW 6163 "GRASSLAND"
M-1	METAL	PAC-CLAD	STANDING SEAM ROOF, COLOR: "AGED COPPER"
M-2	METAL	PAC-CLAD	METAL COPING, COLOR: "SAND STONE"
M-3	METAL	PAC-CLAD	METAL COPING, COLOR: "BURNISHED SLATE"
P-1	PAINT	SHERWIN WILLIAMS	PAINTED FIBER CEMENT BOARDS, COLOR TO MATCH: SW 7005 "PURE WHITE"
P-2	PAINT	SHERWIN WILLIAMS	PAINTED FIBER CEMENT BOARDS, COLOR TO MATCH: SW 6094 "SENSATIONAL SAND"
SF-1	STOREFRONT	YKK	STOREFRONT SYSTEM, COLOR: "ANODIZED ALUMINUM"
SF-2	STOREFRONT	YKK	STOREFRONT SYSTEM, COLOR: "DARK BRONZE"
ST-1	STONE	ECHOLON	WATERFORD STONE, COLOR: "PEWTER"

- This Planned Community Development book is a compromise to the current City of Monroe Zoning Ordinance and Corridor Design Overlay (CDO) within the Highway Commercial District (B-3).

- In compliance with the PCD, we will use some of the City's traditional materials and elements that include brick, columns, pilasters, defined framing around windows and doors, fenestration elements, and ornamentation to give interesting features to the buildings and to break up the facades with pleasing aspects. Parapets will be used throughout to eliminate direct visibility of flat roofs. For additional information, refer to each individual tenant sheet in this PCD submittal.

- Grocery Anchor,

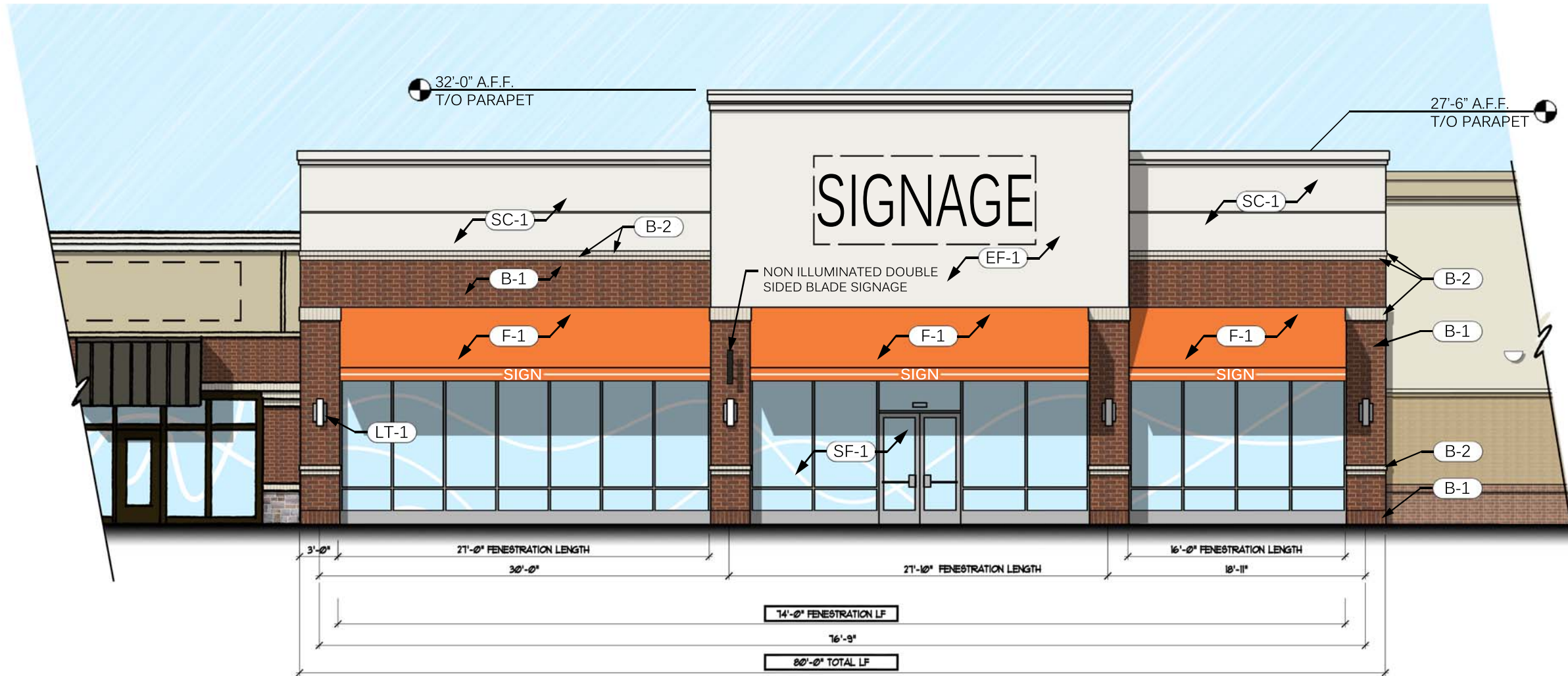
- Prototype shell.

- Signage per tenant prototype requirements.

- Interior design layout dictates glazing location.

- Material selections and building form coordination with tenant.

TENANT JUNIOR 'A'



FRONT ELEVATION

FENESTRATIONS:

80'-0" TOTAL LENGTH (LF)
74'-0" FENESTRATION LF

74'-0"/80'-0" = 93% FENESTRATIONS (MEETS THE 60% MIN. FENESTRATIONS REQUIRED)

3'-0" = MAXIMUM LENGTH WITHOUT FENESTRATIONS (MEETS THE 100' MAX. LENGTH REQUIREMENT)

ORNAMENTATION:

BRICK COLUMNS, FABRIC AWNINGS, & STOREFRONT (WINDOWS) ARE 30'-0" OR LESS IN SPAN ACROSS ENTIRE LENGTH (MEETS THE 30'-0" MAXIMUM SPAN WITHOUT ORNAMENTATION REQUIREMENT)

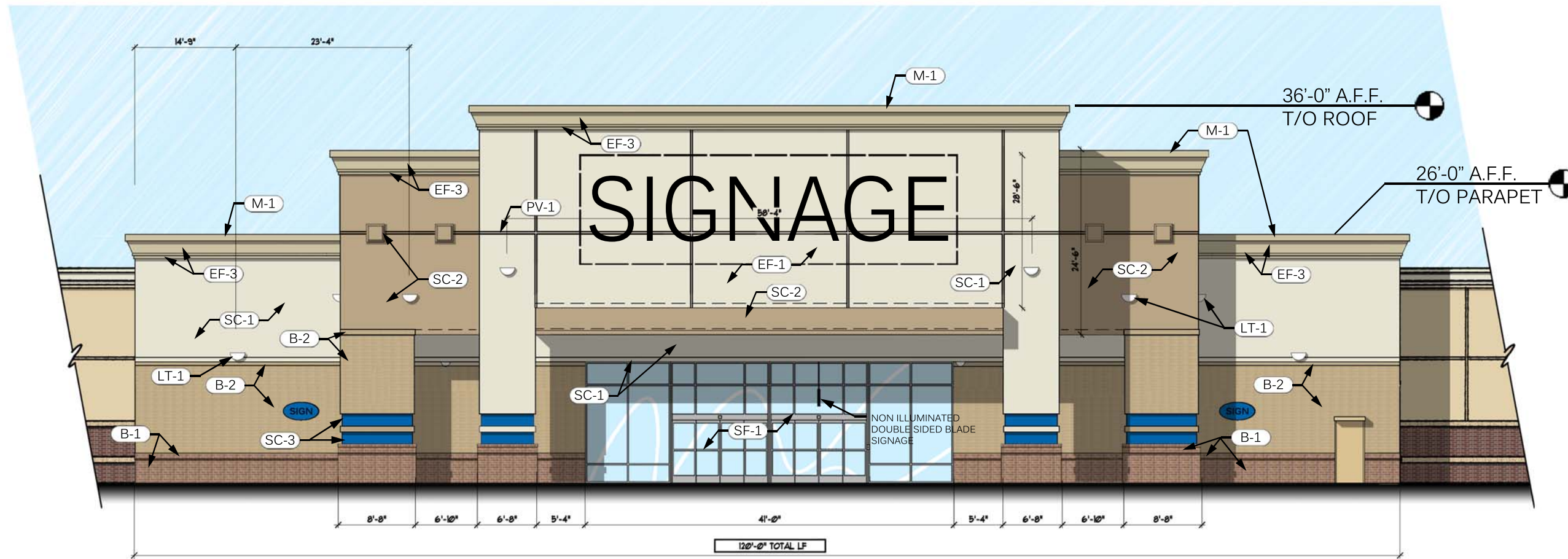
EXTERIOR FINISH SCHEDULE

MARK	CATEGORY	MANUFACTURER	DESCRIPTION / FINISH
B-1	MODULAR BRICK	ACME BRICK	COLOR: CHERRYWOOD WITH CEMEX IVORY MORTAR, TOOLED JOINTS
B-2	MODULAR BRICK	TAYLOR	COLOR: 301w WHITE WITH CEMEX IVORY MORTAR, TOOLED JOINTS
EF-1	EIFS	DRYVIT	FINISH SANDBLAST, COLOR TO MATCH: SW 7005 "PURE WHITE"
F-1	FABRIC	TBD	PATIO 500 ULTRA/EMBOSS, COLOR: "ORANGE POP" w/ WHITE LETTERS AND STRIPE
LT-1	LIGHTING	TBD	ARCHITECTURAL LIGHT FIXTURE
SC-1	STUCCO	DRYVIT	FINISH SANDBLAST, COLOR TO MATCH: SW 7005 "PURE WHITE"
SF-1	STOREFRONT	YKK	STOREFRONT SYSTEM, COLOR: "ANODIZED ALUMINUM"

Architecture

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- Beauty supply anchor.
- Dictated prototype.
- Material selection with enhanced brick features on prototype façade.
- Tenant corporate colors.
- Tenant signage.

TENANT MAJOR 'B'



FRONT ELEVATION

FENESTRATIONS:

120'-0" TOTAL LENGTH (LF)
96'-0" FENESTRATION LF

96'-0"/120'-0" = 80% FENESTRATIONS (MEETS THE 60% MIN. FENESTRATIONS REQUIRED)

29'-6" = MAXIMUM LENGTH WITHOUT FENESTRATIONS (MEETS THE 100' MAX. LENGTH REQUIREMENT)

ORNAMENTATION:

COLUMNS, COLOR CHANGES, AND VERTICAL REVEALS ARE LESS THAN 30'-0" MAXIMUM SPAN ACROSS ENTIRE LENGTH (MEETS THE 30'-0" MAXIMUM SPAN WITHOUT ORNAMENTATION REQUIREMENT)

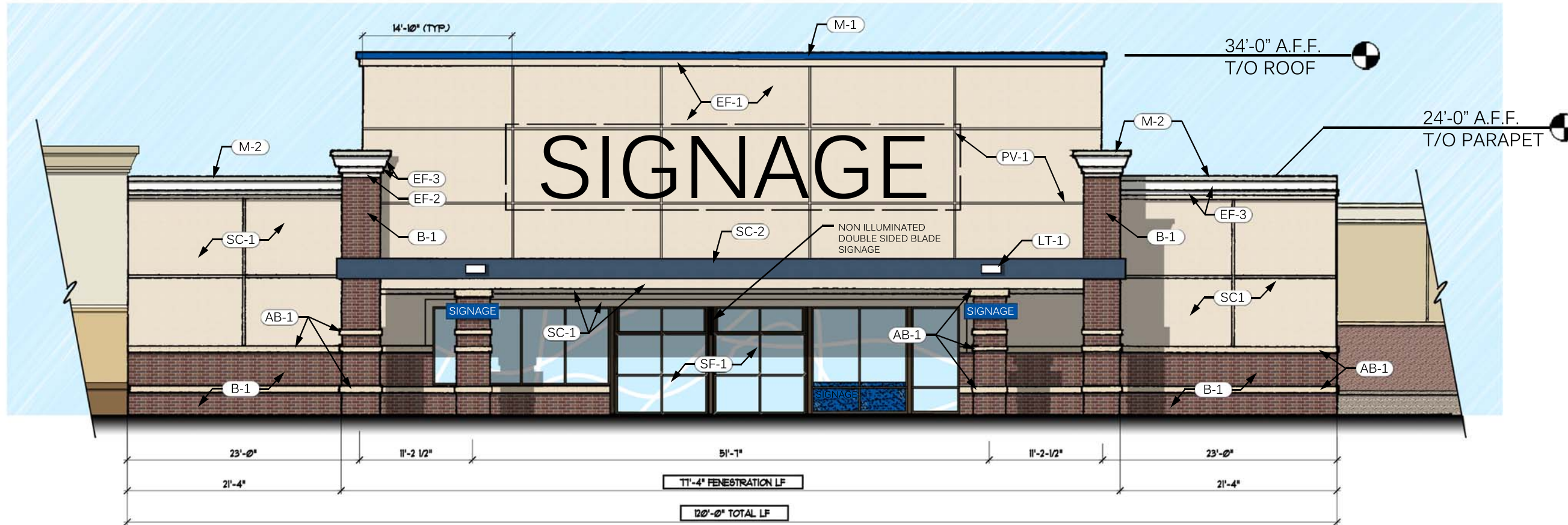
EXTERIOR FINISH SCHEDULE

Code	Material	Manufacturer	Color/Finish
B-1	MODULAR BRICK	BELDEN	COLOR: ST SIMON BLEND W/ ARGOS LIMESTONE MORTAR, TOOLED JOINTS
B-2	MODULAR BRICK	TAYLOR	COLOR: 319 GRAY W/ ARGOS SAHARA MORTAR, TOOLED JOINTS
EF-1	EIFS	DRYVIT	FINISH SANDBLAST, COLOR TO MATCH: SW 6154 NACRE
EF-3	EIFS	DRYVIT	FINISH SANDBLAST, COLOR TO MATCH: SW 6156 RAMIE
LT-1	LIGHTING	T.B.D.	ARCHITECTURAL LIGHT FIXTURE
M-1	COPING	BERRIDGE	METAL COPING, COLOR: SIERRA TAN
PV-1	METAL	DRYVIT	PAINTED METAL REVEAL, COLOR TO MATCH: SW 6154 NACRE
SC-1	STUCCO	DRYVIT	FINISH SANDBLAST, COLOR TO MATCH: SW 6154 NACRE
SC-2	STUCCO	DRYVIT	FINISH SANDBLAST, COLOR TO MATCH: SW 6108 LATTE
SC-3	STUCCO	DRYVIT	FINISH SANDBLAST, COLOR: PMS 300 (N23824)
SF-1	STOREFRONT	YKK	STOREFRONT SYSTEM, COLOR: ANODIZED ALUMINUM

Architecture

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- Fashion anchor.
- Dictated prototype.
- Generalized description of construction and metals.
- Colors by corporate company as they relate to owners overall development.
- EIFS at signage band.

TENANT MAJOR 'C'



FRONT ELEVATION

FENESTRATIONS:

120'-0" TOTAL LENGTH (LF)
77'-4" FENESTRATION LF

$77'-4"/120'-0" = 64\%$ FENESTRATIONS (MEETS THE 60% MIN. FENESTRATIONS REQUIRED)

23'-0" = MAXIMUM LENGTH WITHOUT FENESTRATIONS (MEETS THE 100' MAX. LENGTH REQUIREMENT)

ORNAMENTATION:

BRICK/STUCCO COLUMNS, STOREFRONT (WINDOWS), & STUCCO VERTICAL REVEALS ARE 14'-10" OR LESS IN SPAN ACROSS ENTIRE LENGTH (MEETS THE 30'-0" MAXIMUM SPAN WITHOUT ORNAMENTATION REQUIREMENT)

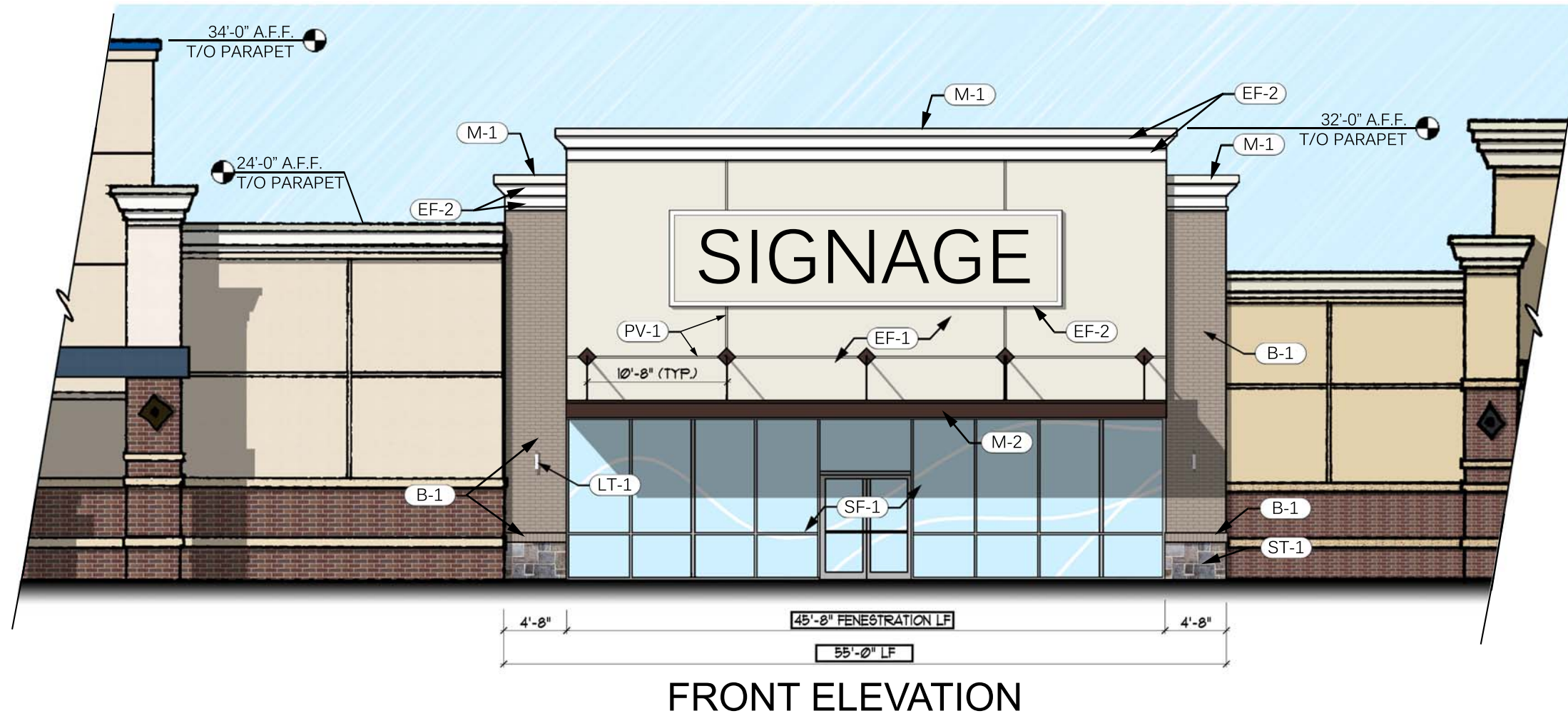
EXTERIOR FINISH SCHEDULE

Code	Material	Brand	Color/Finish
AB-1	ARCHITECTURAL BLOCK	TRENWYTH	COLOR: CAMBRIDGE W/ MORTAR, TOOLED JOINTS
B-1	MODULAR BRICK	BELDEN	COLOR: AMHERST BLEND W/ ARGOS RED MORTAR, TOOLED JOINTS
EF-1	EIFS	DRYVIT	FINISH SANDBLAST, COLOR TO MATCH: DRYVIT - 449 BUCKSKIN
EF-2	EIFS	DRYVIT	FINISH SANDBLAST, COLOR TO MATCH: DRYVIT - 385A SANDPIPER
EF-3	EIFS	DRYVIT	FINISH SANDBLAST, COLOR TO MATCH: DRYVIT - 310 CHINA WHITE
LT-1	LIGHTING	COOPER	WALL SCENCE, ENVUE # ENT-C02-LED-EI-BL3-WH
M-1	COPING	BERRIDGE	METAL COPING, COLOR TO MATCH: BM #2016-20 CHAMPION COBALT BLUE
M-2	COPING	BERRIDGE	METAL CANOPY, COLOR: PARCHMENT
PV-1	METAL	DRYVIT	PAINTED METAL REVEAL, COLOR TO MATCH: DRYVIT - 449 BUCKSKIN
SC-1	STUCCO	DRYVIT	FINISH SANDBLAST, COLOR TO MATCH: DRYVIT - 449 BUCKSKIN
SC-2	STUCCO	DRYVIT	FINISH SANDBLAST, COLOR TO MATCH: DRYVIT - 578 MIDNIGHT BLUE
SF-1	STOREFRONT	YKK	STOREFRONT SYSTEM, COLOR: MEDIUM BRONZE

Architecture

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- Fashion anchor.
- Dictated prototype.
- Signage dictated by tenant with EIFS signage area.
- Enhanced brick on façade compared to prototype design.

TENANT JUNIOR 'B'



FENESTRATIONS:

60'-0" TOTAL LENGTH (LF)
45'-8" FENESTRATION LF

45'-8"/60'-0" = 76% FENESTRATIONS (EXCEEDS THE 60% MIN. FENESTRATIONS REQUIRED)

4'-8" = MAXIMUM LENGTH WITHOUT FENESTRATIONS (MEETS THE 100' MAX. LENGTH REQUIREMENT)

ORNAMENTATION:

BRICK/STONE PILASTERS, VERTICAL REVEALS, & METAL CANOPY BRACKETS ARE 10'-8" OR LESS IN SPAN ACROSS ENTIRE LENGTH (MEETS THE 30'-0" MAXIMUM SPAN WITHOUT ORNAMENTATION REQUIREMENT)

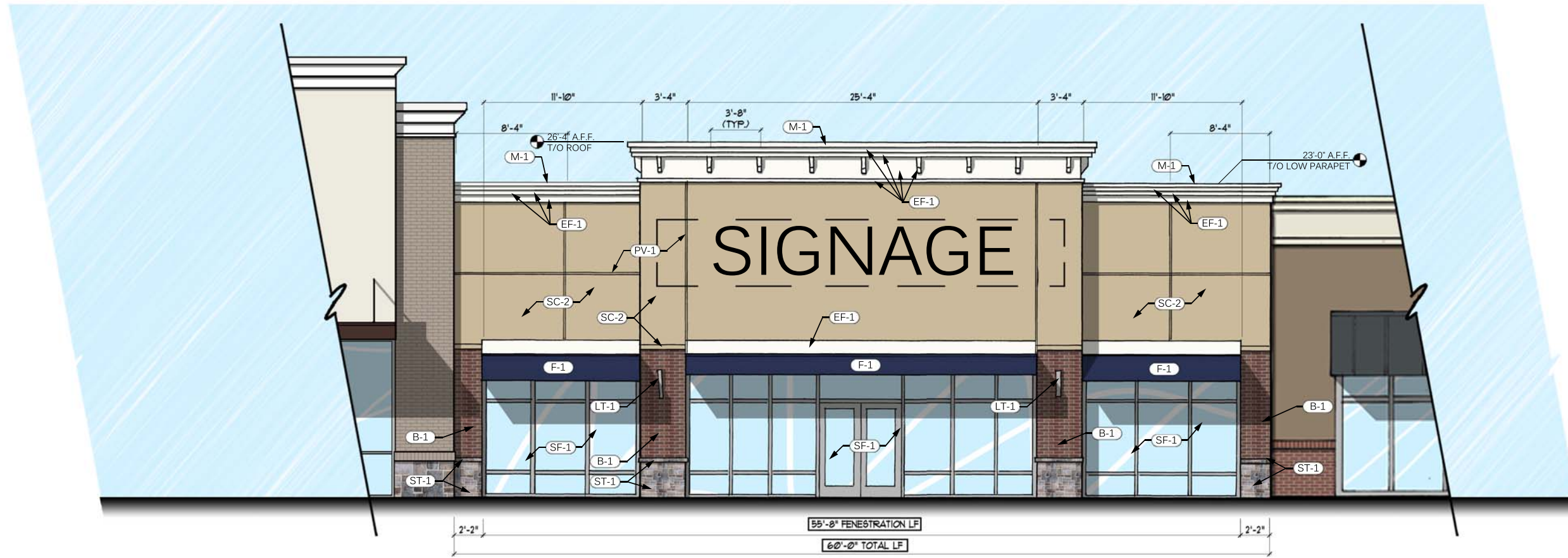
EXTERIOR FINISH SCHEDULE

MARK	CATEGORY	MANUFACTURER	DESCRIPTION / FINISH
B-1	MODULAR BRICK	TAYLOR	EXTERIOR BRICK, "3118 CREAM" W/ ARGOS LITE BEIGE MORTAR, TOOLED JOINTS OR APPROVED EQUAL
EF-1	STUCCO	DRYVIT	EIFS; COLOR TO MATCH DRYVIT 456 "OYSTER SHELL"
EF-2	EIFS	DRYVIT	EIFS; COLOR TO MATCH DRYVIT 310 "CHINA WHITE"
LT-1	LIGHTING	BROWNLEE	WALL SCONCE, 7176-18-GM-H08-40K-BAC
M-1	METAL	PAC-CLAD	METAL COPING, COLOR: "BONE WHITE"
M-2	METAL	MAPES	METAL AWNING, COLOR: "MAPES BRONZE BAKED ENAMEL"
PV-1	METAL	DRYVIT	PAINTED METAL REVEAL, COLOR TO MATCH: DRYVIT 456 "OYSTER SHELL"
SF-1	STOREFRONT	YKK	STOREFRONT SYSTEM, COLOR: "CLEAR ANODIZED ALUMINUM"
ST-1	STONE	WATERFORD	STONE, COLOR: "PEWTER"

Architecture

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- General merchandise anchor.
- Design dictated by corporate prototype.
- Colors and material dictated by corporate prototype.
- Tenant signage.

TENANT JUNIOR 'C'



FRONT ELEVATION

FENESTRATIONS:

60'-0" TOTAL LENGTH (LF)
56'-8" FENESTRATION LF

56'-8"/60'-0" = 95% FENESTRATIONS (EXCEEDS THE 60% MIN. FENESTRATIONS REQUIRED)

3'-4" = MAXIMUM LENGTH WITHOUT FENESTRATIONS (MEETS THE 100' MAX. LENGTH REQUIREMENT)

ORNAMENTATION:

BRICK/STUCCO PILASTERS, VERTICAL REVEALS, & CORNICE BRACKETS ARE 8'-4" OR LESS IN SPAN ACROSS ENTIRE LENGTH (MEETS THE 30'-0" MAXIMUM SPAN WITHOUT ORNAMENTATION REQUIREMENT)

EXTERIOR FINISH SCHEDULE

MARK	CATEGORY	MANUFACTURER	DESCRIPTION / FINISH
B-1	MODULAR BRICK	BELDEN	COLOR: AMHERST BLEND ARGOS CANYON BROWN MORTAR, TOOLED JOINTS
EF-1	EIFS	DRYVIT	COLOR: DRYVIT "310 CHINA WHITE"
F-1	FABRIC	SUNBRELLA	COLOR: SUNBRELLA "MARINE BLUE"
LT-1	LIGHTING	T.B.D.	ARCHITECTURAL LIGHT FIXTURE
M-1	METAL	PAC-CLAD	COPING, COLOR: "BONE WHITE"
PV-1	METAL	DRYVIT	PAINTED METAL REVEAL, COLOR TO MATCH: SW 6142 "MACADAMIA"
SC-1	STUCCO	DRYVIT	COLOR: DRYVIT "310 CHINA WHITE"
SC-2	STUCCO	DRYVIT	FINISH SANDBLAST, COLOR TO MATCH: SW 6142 "MACADAMIA"
SF-1	STOREFRONT	YKK	STOREFRONT SYSTEM, COLOR: "ANODIZED ALUMINUM"

Architecture

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- Shoe / accessories anchor.
- Dictated prototype.
- Material selection with enhanced brick features on prototype façade.
- Tenant corporate colors.
- Tenant signage.

SHOPS A & B ELEVATIONS

Architecture



SHOPS 'A' ELEVATION

FENESTRATIONS:

140'-0" TOTAL LENGTH (LF)
140'-0" FENESTRATION LF

140'-0"/140'-0" = 100% FENESTRATIONS (EXCEEDS THE 60% MIN. FENESTRATIONS REQUIRED)

ORNAMENTATION:

BRICK/STONE PILASTERS, VERTICAL REVEALS, & METAL CANOPIES ARE 16'-10" OR LESS IN SPAN ACROSS ENTIRE LENGTH (MEETS THE 30'-0" MAXIMUM SPAN WITHOUT ORNAMENTATION REQUIREMENT)

EXTERIOR FINISH SCHEDULE

MARK	CATEGORY	MANUFACTURER	DESCRIPTION / FINISH
B-1	MODULAR BRICK	ACME BRICK	COLOR: CHERRYWOOD WITH CEMEX IVORY MORTAR, TOOLED JOINTS
B-2	MODULAR BRICK	TAYLOR	COLOR: 301w WHITE WITH CEMEX IVORY MORTAR, TOOLED JOINTS
DF-4	STUCCO	DRYVIT	FINISH SANDBLAST, COLOR TO MATCH: SW 6156 "RAMIE"
EF-1	EIFS	DRYVIT	FINISH SANDBLAST, COLOR TO MATCH: SW 7005 "PURE WHITE"
EF-3	EIFS	DRYVIT	FINISH SANDBLAST, COLOR TO MATCH: SW 6154 "NACRE"
SC-2	STUCCO	DRYVIT	FINISH SANDBLAST, COLOR TO MATCH: SW 6094 "SENSATIONAL SAND"
SC-3	STUCCO	DRYVIT	FINISH SANDBLAST, COLOR TO MATCH: SW 6154 "NACRE"
SC-4	STUCCO	DRYVIT	FINISH SANDBLAST, COLOR TO MATCH: SW 6156 "RAMIE"
SC-5	STUCCO	DRYVIT	FINISH SANDBLAST, COLOR TO MATCH: SW 6163 "GRASSLAND"
LT-1	LIGHTING	BROWNLEE	WALL SCONCE, 7176-18-GM-H08-40K-BAC
M-1	METAL	PAC-CLAD	STANDING SEAM ROOF, COLOR: "AGED COPPER"
M-2	METAL	PAC-CLAD	METAL COPING, COLOR: "SAND STONE"
M-3	METAL	PAC-CLAD	METAL COPING, COLOR: "BURNISHED SLATE"
P-1	PAINT	SHERWIN WILLIAMS	PAINTED FIBER CEMENT BOARDS, COLOR TO MATCH: SW 7005 "PURE WHITE"
P-2	PAINT	SHERWIN WILLIAMS	PAINTED FIBER CEMENT BOARDS, COLOR TO MATCH: SW 6094 "SENSATIONAL SAND"
SF-1	STOREFRONT	YKK	STOREFRONT SYSTEM, COLOR: "ANODIZED ALUMINUM"
SF-2	STOREFRONT	YKK	STOREFRONT SYSTEM, COLOR: "DARK BRONZE"
ST-1	STONE	ECHELON	WATERFORD STONE, COLOR: "PEWTER"



SHOPS 'B' ELEVATION

FENESTRATIONS:

60'-0" TOTAL LENGTH (LF)
60'-0" FENESTRATION LF

60'-0"/60'-0" = 100% FENESTRATIONS (EXCEEDS THE 60% MIN. FENESTRATIONS REQUIRED)

ORNAMENTATION:

BRICK/STONE PILASTERS, VERTICAL REVEALS, & METAL CANOPIES ARE 16'-10" OR LESS IN SPAN ACROSS ENTIRE LENGTH (MEETS THE 30'-0" MAXIMUM SPAN WITHOUT ORNAMENTATION REQUIREMENT)

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• General retail / Restaurant / Services tenants

4.1 SIGNAGE

SIGNAGE INTRODUCTION

643A.5 SIGNS (page 52):

(1) General Criteria: Signage shall be compatible with the new development's style and mass and visual character while maintaining tenant requirements and needs per the following criteria:

- a. Placement: Attached signs shall be in scale with and proportional to the host building, not to cover details, and be aligned substantially with adjacent tenant suites signage within the center and/or consistent with the design location of the specific store's façade of each tenant suite. Ground/Monument signs shall be oriented perpendicular to traffic flow. Corner lots may choose which traffic flow to address.
- b. Materials: Signs shall be wood or metal, and other materials per tenant signage design requirements consistent with this development and per sheet 4.10.
- c. Design: Sign design may be simple or in character with the development.
- d. Dimensions: Ground/Monument signs are permitted and allowed to be in proportion to the center that it represents in size, material, and scale.
- e. Lighting: Major and Junior anchors signs, tenants 5,000 square feet or greater, future independent buildings at Parcels and/or Outlots, Hotel signs, Office signs, and Ground/Monument signs, shall be internally illuminated. All other signs shall have halo lighting features or indirectly lit with goose-neck or similar light fixtures that do not block or interfere with the visual reading of the sign.

ARTICLE VII: DIMENSIONAL REQUIREMENTS AND EXCEPTIONS:

SECTION 700 – CHARTS OF DIMENSIONAL REQUIREMENTS

700.2 P, CBD, B-1, B-2, & B-3 DIMENSIONAL REQUIREMENTS (page 61):

SECTION 700.2: TABLE 12 – COMMERCIAL ZONING DISTRICT DIMENSIONAL REQUIREMENTS (pages 61-62):

The PCD Maximum Building Parapet Height or roof peak will not be higher than 37'-0" above finish floor, not including future hotel(s). The PCD Maximum Building Parapet Height for future hotel(s) will not be higher than 54'-0" above finish floor.

SECTION 710 - EXCEPTIONS AND MODIFICATIONS

SECTION 710.2: HEIGHT LIMITS (page 63):

The height limits listed in this PCD shall not apply to structures not intended for human occupancy such as church spires, flagpoles, chimneys, cupolas, monuments, radio or television towers or aerials, water towers, or similar architectural vertical projections. However, the top of signage allowable height will be no greater than 35'-0" above finish floor, not including future hotel(s). The top of signage allowable height for future hotel(s) will be no greater than 52'-0" above finish floor. The heights of telecommunication towers and antennas are governed by Article XI.

ARTICLE XII – SIGNS

SECTION 1210 - DEFINITIONS

1210.1 GENERAL DEFINITIONS (page 78)

AREA OF SIGN: Only one face of a double-faced sign, as defined herein, bearing identical copy on each side shall be used in computing the area, otherwise both sides shall be used in computing area.

(2) WALL SIGN: The area of a wall sign shall mean and shall be computed using the smallest contiguous square, circle, rectangle, triangle, or combination thereof, that would encompass the external limits of the writing, representation, emblem, or other display. Any material or color forming a background of the display or used to differentiate the sign from the backdrop or structure against which it is placed will not be included in the sign area. When a wall sign is formed by placing individual letters, numbers, or figures on the wall, without a distinguishing background, the area shall be determined by a contiguous perimeter drawn around all letters, numbers figures, trademark, or other symbols, enclosing the limits of writing. Any letters, numbers, figures, trademarks, or graphics separated by 12 inches or more shall be considered two separate signs.

SIGNAGE INTRODUCTION

1210.2 SPECIFIC DEFINITIONS (pages 78-81):

(1) AWNING AND CANOPY SIGN means a sign imposed or painted upon any roof-like structure that provides either permanent or temporary shelter for adjacent walkways or entrances to a building or property. Awning and canopy signs are allowed up to 20% of total area of independent awning or canopy. Canopy signage area is considered separate than wall signage and is to be computed separate than wall signage.

(15) GROUND SIGN OR MONUMENT SIGN means a permanently affixed sign, which is wholly independent of a building for support.

(16) HEIGHT OF A GROUND SIGN OR MONUMENT SIGN means the vertical distance from the base of the sign at normal grade to the top of the highest component of the sign. Normal grade shall be the predominant grade after construction, exclusive of any filling, berming, mounding or excavating for the purpose of locating or elevating the sign. Base shall be where the sign support meets, or should meet, the normal grade. Ground Signs or Monument Signs shall be no greater than 28'-0" above normal grade. This particular development is allowed two (2) monument signs.

(18) ILLUMINATED SIGN, INTERNAL means a sign illuminated by an internal light source. Internally illuminated signs are allowed for Ground Signs or Monument Signs. Internally illuminated signs are allowed for wall signs at Major Anchors and Junior Anchors which are 5,000 square feet or greater, and future independent buildings at Parcels and/or Outlots. Internally illuminated signs are discouraged for all other wall mounted signs.

(50) WINDOW SIGN means any type of sign located entirely within the interior of a building or structure, and placed near or on a window or door, the letters, numbers, pictorial or sculptured matter of which is visible from public right of ways at the exterior of the premises. Window signs shall not be internally illuminated except for signs referring to 'open / closed' and 'hours of operations' signs.

1250.2 NON-RESIDENTIAL ALLOWABLE SIGNS (pages 84-85):

(1) GROUND SIGNS:

(a) Platted Outlots, and/or a single building, or tenant suite shall be permitted a maximum of one (1) sixty-four (64) foot double sided Ground Signage area per each public street frontage.

(c) No Ground Sign for Outlots, and/or a single building, or tenant suite shall have a height greater than nine feet above normal grade.

(2) WALL SIGNS:

(a) Platted Outlots, and/or a single building, or tenant suite shall be permitted one (1) or more wall sign(s) not to exceed two and one-half (2.5) square feet per linear foot of building frontage along the length of one sidewalk span per each public street frontage/entrance. The façade of a main building facing a public street shall feature at least one public entrance and be defined as the façade that will meet Wall Signage requirements. Where a main building faces more than one public streets, an entrance for each side may be proposed and then, each public entrance side will meet Wall Signage requirements. Building façade signage facing U.S. Highway 78 will follow city signage ordinance.

(3) PLANNED CENTERS AND MULTI-TENANT COMPLEXES:

(a) GROUND SIGNS:

(i) Planned Center Developments and Multi-Tenant Complexes shall be permitted a maximum of one (1) four hundred (400) square foot ground sign for each street adjacent to project site.

(ii) No ground sign shall have a height greater than 28'-0" above normal grade.

(vi) Planned Center Developments and Multi-Tenant Complexes shall also be permitted an entrance sign. Ground/Monument signs may be considered as an entrance sign. Such signs shall be either one double -sided Ground/Monument sign located at each entrance of the development, each side of which shall not exceed 230 SF signage area or two one sided signs, each sign side is not to exceed 230 square feet signage area. The only permitted text on entrance signs shall be the name and street number of the planned center and the individual Tenant names.

(b) WALL SIGNS: Refer to Article XII -SIGNS in this PCD book

(d) TENANT DIRECTIONAL SIGNS Each planned community development shall be allowed additional directional signage as follows:

(i) Maximum sign size is twenty-four (24) square feet, not including sign base or decorative sign perimeter. Maximum height from base is 6'-0".

(v) Three signs are allowed per internal intersection of private drives or public streets within the project and shall not be allowed on state routes or arterial roads.

(e) Refer to Section 1250.2 NON-RESIDENTIAL ALLOWABLE SIGNS in this PCD book

SECTION 1255 CONSTRUCTION AND MAINTENANCE REQUIREMENTS (pages 88-89):

(10) The illumination of internally illuminated signs shall not exceed twenty (25) foot candles of LED and/or incandescent light measured at a distance of ten feet from such structure.

SECTION 1265 PROHIBITED SIGNS AND ADVERTISING DEVICES (page 89):

(2) Awning and canopy signs are allowed – refer to Section 1210.2 SPECIFIC DEFINITIONS in this PCD book.

TENANT MAJOR 'A'

Signage

In compliance with the PCD, we will use signage that compliments Monroe Pavilion in size, scale, and design that is usually used by some Major and Junior Anchors that are considering complimenting this community in this center. Signage is an integral and important aspect for this Planned Community Development and future tenants of Parcels and Outlots and will certainly represent this center with pleasing visual character. For additional information, refer to PCD Section 4.1.



FRONT ELEVATION

MAJOR 'A' - WALL SIGNAGE AREA

BUILDING FRONT FAÇADE AREA = 7,721 SF

MAIN WALL SIGN AT ENTRY TOWER = 7'-3" HIGH x 20'-10" WIDE = 151 SF
 SIGN AT LEFT SIDE OF BUILDING = 4'-3" HIGH x 12'-11-1/4" WIDE = 55 SF

TOTAL ESTIMATED WALL SIGNAGE SF = 206 SF

WALL SIGNAGE AREA IS NOT TO EXCEED 2.5 SF OF LINEAR FOOT OF BUILDING FRONTAGE:

BUILDING FRONTAGE = 241'-0"

CALCULATION = 241'-0" x 2.5 = 602.5 SF (THIS FAÇADE MEETS SIGNAGE AREA REQUIREMENT WITH 206 SF OF WALL SIGNAGE)

TENANT JUNIOR 'A'



Signage

In compliance with the PCD, we will use signage that compliments Monroe Pavilion in size, scale, and design that is usually used by some Major and Junior Anchors that are considering complimenting this community in this center. Signage is an integral and important aspect for this Planned Community Development and future tenants of Parcels and Outlots and will certainly represent this center with pleasing visual character. For additional information, refer to PCD Section 4.1

JUNIOR 'A' - WALL SIGNAGE AREA

BUILDING FRONT FAÇADE AREA: 824 SF + 986.3 SF + 518 SF = 2,328.3 SF

TOTAL ESTIMATED WALL SIGNAGE SF = 9'-0" HIGH x 20'-0" WIDE = 180 SF

AWNING 1 = 150 SF
AWNING 1 SIGNAGE AREA = 3 SF

AWNING 2 = 137 SF
AWNING 2 SIGNAGE AREA = 3 SF

AWNING 3 = 88 SF
AWNING 3 SIGNAGE AREA = 3 SF

WALL SIGNAGE AREA IS NOT TO EXCEED 2.5 SF OF LINEAR FOOT OF BUILDING FRONTAGE:
BUILDING FRONTAGE = 80'-0"
CALCULATION = 80'-0" x 2.5 = 200 SF (THIS FAÇADE MEETS SIGNAGE AREA REQUIREMENTS WITH 180 SF OF WALL SIGNAGE)

AWNING SIGNAGE AREA IS NOT TO EXCEED 20% OF TOTAL AREA OF INDEPENDENT AWNING AND IS TO BE COMPUTED SEPARATE THAN WALL SIGNAGE:
AWNING 1: 3 SF/150 SF = 2% SIGNAGE AREA OF AWNING (THIS AWNING SIGN DOES NOT EXCEED THE 20% MAXIMUM AREA)
AWNING 2: 3 SF/137 SF = 2% SIGNAGE AREA OF AWNING (THIS AWNING SIGN DOES NOT EXCEED THE 20% MAXIMUM AREA)
AWNING 3: 3 SF/88 SF = 2% SIGNAGE AREA OF AWNING (THIS AWNING SIGN DOES NOT EXCEED THE 20% MAXIMUM AREA)

TENANT MAJOR 'B'



Signage

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MAJOR 'B' - WALL SIGNAGE AREA

BUILDING FRONT FAÇADE AREA = 3,881 SF

WALL MAIN SIGN 1 = 6'-0" HIGH x 24'-0" WIDE = 144 SF

WALL MAIN SIGN 2 = 3'-6" HIGH x 42'-0" WIDE = 147 SF

TWO PLAQUE SIGNS = 8 SF x 2 = 16 SF (THESE ACCENT SIGNS ARE NOT INCLUDED IN TOTAL SIGNAGE AREA)

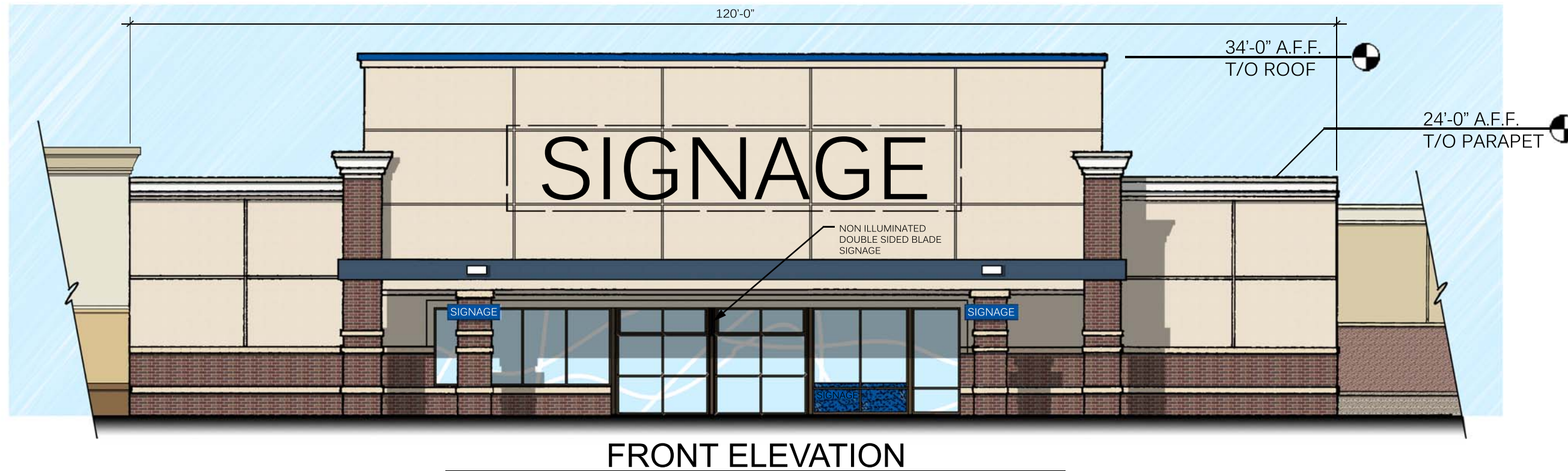
TOTAL ESTIMATED WALL SIGNAGE SF = 291 SF

MAX WALL SIGNAGE NOT TO EXCEED 2.5 SF OF LINEAR FOOT OF BUILDING FRONTAGE:

BUILDING FRONTAGE = 120'-0"

CALCULATION = 120'-0" x 2.5 = 300 SF (THIS FAÇADE MEETS SIGNAGE AREA REQUIREMENTS WITH 291 SF OF WALL SIGNAGE)

TENANT MAJOR 'C'



Signage

In compliance with the PCD, we will use signage that compliments Monroe Pavilion in size, scale, and design that is usually used by some Major and Junior Anchors that are considering complimenting this community in this center. Signage is an integral and important aspect for this Planned Community Development and future tenants of Parcels and Outlots and will certainly represent this center with pleasing visual character. For additional information, refer to PCD Section 4.1

MAJOR 'C' - WALL SIGNAGE AREA:

BUILDING FRONT FAÇADE AREA: 553.5 SF + 2644.3 SF + 556.2 SF = 3754 SF

MAIN SIGNAGE = 6'-0" HIGH x 31'-1" WIDE = 185.5 SF

COLUMN-WRAP SIGNAGE = 1'-2" HIGH x 4'-0" = 4.67 SF x 2 = 9.33 SF

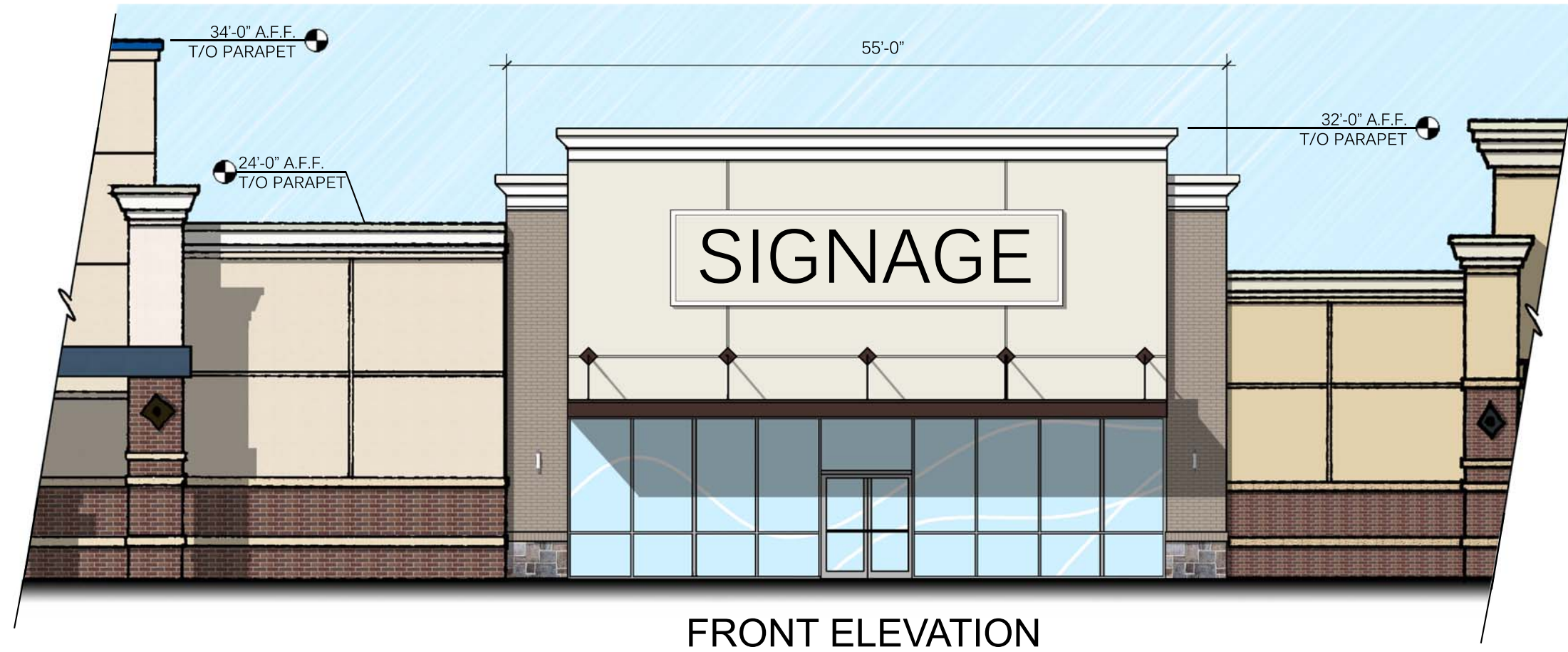
TOTAL ESTIMATED WALL SIGNAGE SF = 195 SF

MAX WALL SIGNAGE NOT TO EXCEED 2.5 SF OF LINEAR FOOT OF BUILDING FRONTAGE:

BUILDING FRONTAGE = 120'-0"

CALCULATION = 120'-0" x 2.5 = 300 SF (THIS FAÇADE MEETS SIGNAGE AREA REQUIREMENTS WITH 195 SF OF WALL SIGNAGE)

TENANT JUNIOR 'B'



JUNIOR 'B' - WALL SIGNAGE AREA:

BUILDING FRONT FAÇADE AREA: 148.2 SF + 1574.4 SF = 148.2 SF = 1,870.8 SF

TOTAL ESTIMATED WALL SIGNAGE SF = 3'-3" HIGH x 23'-0" WIDE = 75 SF

MAX WALL SIGNAGE NOT TO EXCEED 2.5 SF OF LINEAR FOOT OF BUILDING FRONTAGE:

BUILDING FRONTAGE = 55'-0"

CALCULATION = 55'-0" x 2.5 = 137.5 SF (THIS FAÇADE MEETS SIGNAGE AREA REQUIREMENTS WITH 75 SF OF WALL SIGNAGE)

Signage

In compliance with the PCD, we will use signage that compliments Monroe Pavilion in size, scale, and design that is usually used by some Major and Junior Anchors that are considering complimenting this community in this center. Signage is an integral and important aspect for this Planned Community Development and future tenants of Parcels and Outlots and will certainly represent this center with pleasing visual character. For additional information, refer to PCD Section 4.1

TENANT JUNIOR 'C'



FRONT ELEVATION

JUNIOR 'C' - WALL SIGNAGE AREA:

BUILDING FRONT FAÇADE AREA: 326 SF + 865 SF + 326 SF = 1,517 SF

TOTAL ESTIMATED WALL SIGNAGE SF = 5'-0" x 30' = 1500 SF

MAX WALL SIGNAGE NOT TO EXCEED 2.5 SF OF LINEAR FOOT OF BUILDING FRONTAGE:

BUILDING FRONTAGE = 60'-0"

CALCULATION = 60'-0" x 2.5 = 150 SF (THIS FAÇADE WILL MEET SIGNAGE AREA REQUIREMENTS WITH 150 SF OR LESS OF WALL SIGNAGE)

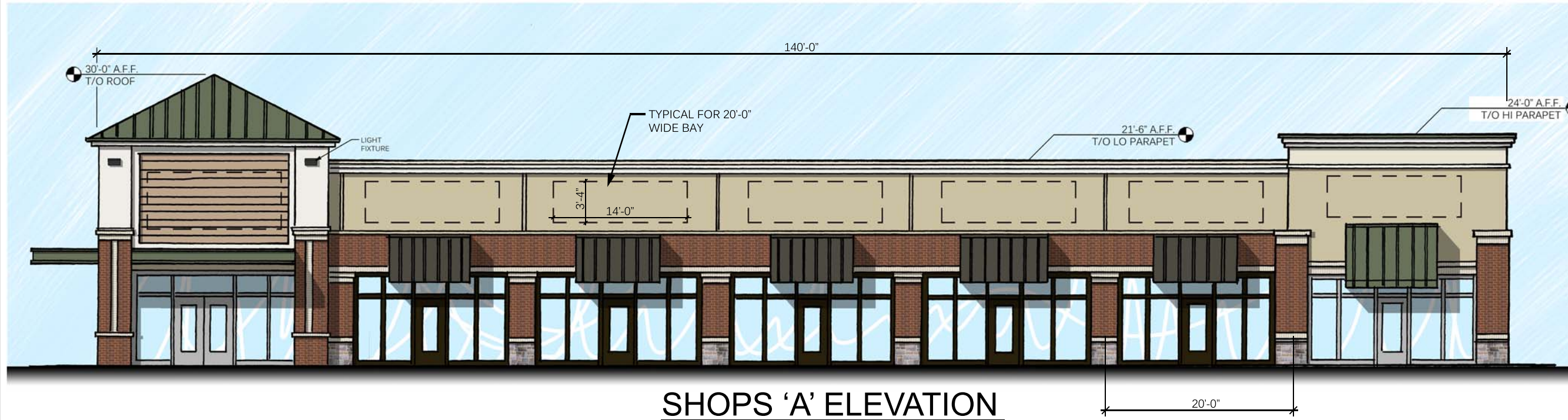
Signage

In compliance with the PCD, we will use signage that compliments Monroe Pavilion in size, scale, and design that is usually used by some Major and Junior Anchors that are considering complimenting this community in this center. Signage is an integral and important aspect for this Planned Community Development and future tenants of Parcels and Outlots and will certainly represent this center with pleasing visual character. For additional information, refer to PCD Section 4.1

SHOPS A & B ELEVATIONS

Signage

In compliance with the PCD, we will use signage that compliments Monroe Pavilion in size, scale, and design that is usually used by some Major and Junior Anchors that are considering complimenting this community in this center. Signage is an integral and important aspect for this Planned Community Development and future tenants of Parcels and Outlots and will certainly represent this center with pleasing visual character. For additional information, refer to PCD Section 4.1



SHOPS 'A' ELEVATION



SHOPS 'B' ELEVATION

SHOPS 'A' - WALL SIGNAGE AREA:

BUILDING FRONT FAÇADE AREA = 3,521 SF

WALL SIGNAGE AREA FOR ENTIRE LENGTH OF SIDEWALK SPAN:
SEVEN (7) SIGNS = 3'-6" HIGH x 14'-0" WIDE = 49 SF x 7 = 343 SF

MAX WALL SIGNAGE NOT TO EXCEED 2.5 SF OF LINEAR FOOT OF BUILDING FRONTAGE:
BUILDING FRONTAGE = 140'-0"
CALCULATION = 140'-0" x 2.5 = 350 SF (THIS FAÇADE MEETS THE SIGNAGE AREA REQUIREMENTS WITH 343 SF OF WALL SIGNAGE)

SHOPS 'B' - WALL SIGNAGE AREA:

BUILDING FRONT FAÇADE AREA = 1,290 SF

WALL SIGNAGE AREA FOR ENTIRE LENGTH OF SIDEWALK SPAN:
THREE (3) SIGNS = 3'-6" HIGH x 14'-0" WIDE = 49 x 3 = 147 SF

MAX WALL SIGNAGE NOT TO EXCEED 2.5 SF OF LINEAR FOOT OF BUILDING FRONTAGE:
BUILDING FRONTAGE = 60'-0"
CALCULATION = 60'-0" x 2.5 = 150 SF (THIS FAÇADE MEETS THE SIGNAGE AREA REQUIREMENTS WITH 147 SF OF WALL SIGNAGE)

SIGNAGE LIGHTING

Signage

Indirect signage lighting examples



- This page demonstrates non-internally illuminated examples of wall signage.
- Internally illuminated signs are allowed for wall signs at Major Anchors, Junior Anchors, and future independent buildings at Parcels and/or Outlots, which are 5,000 square feet or greater.

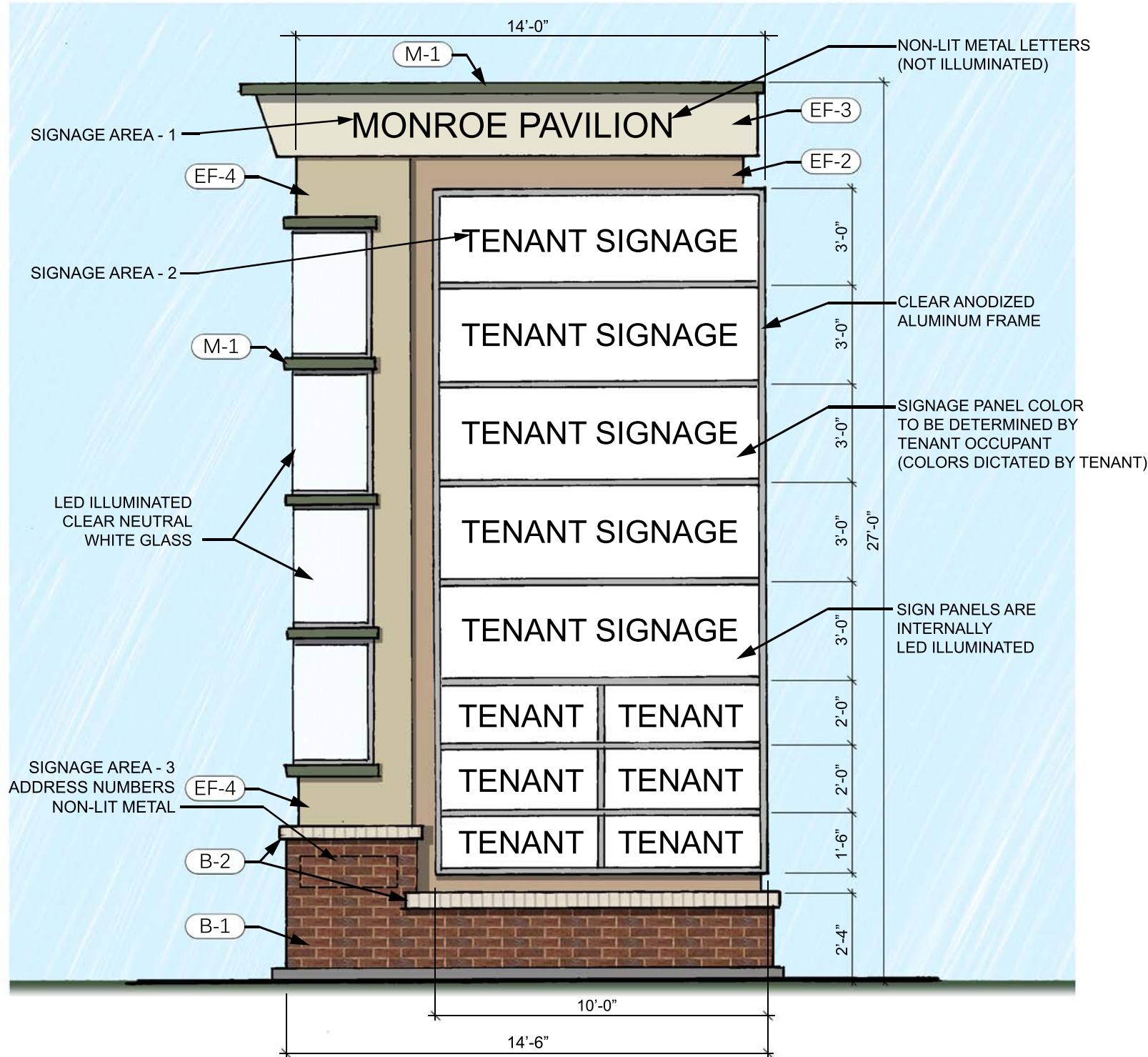
Halo signage lighting examples



Internally illuminated signage examples



FREE STANDING SIGNS

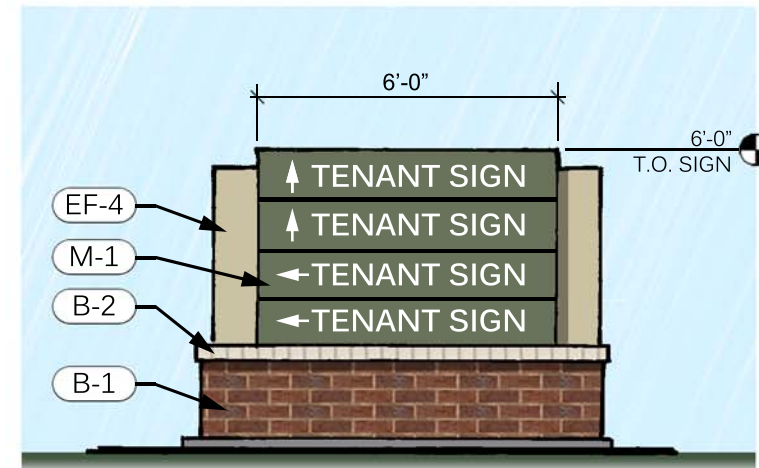


MONUMENT SIGN ELEVATION

N.T.S.

MONUMENT AREA CALCULATIONS FOR ONE SIDE:
TOTAL MONUMENT AREA SF = 380 SF

SIGNAGE AREA CALCULATIONS FOR ONE SIDE:
AREA 1: 1'-0" x 10'-0" = 10 SF
AREA 2: 20'-6" x 10'-0" = 205 SF
AREA 3: 1'-0" x 3'-0" = 3 SF
TOTAL SIGNAGE AREA SF = 218 SF



DIRECTIONAL SIGN ELEVATION

N.T.S.

SIGNAGE AREA CALCULATIONS FOR ONE SIDE:
AREA: (4) 1'-0" x 6'-0" PANELS = 6 SF
TOTAL SIGNAGE AREA SF = 24 SF

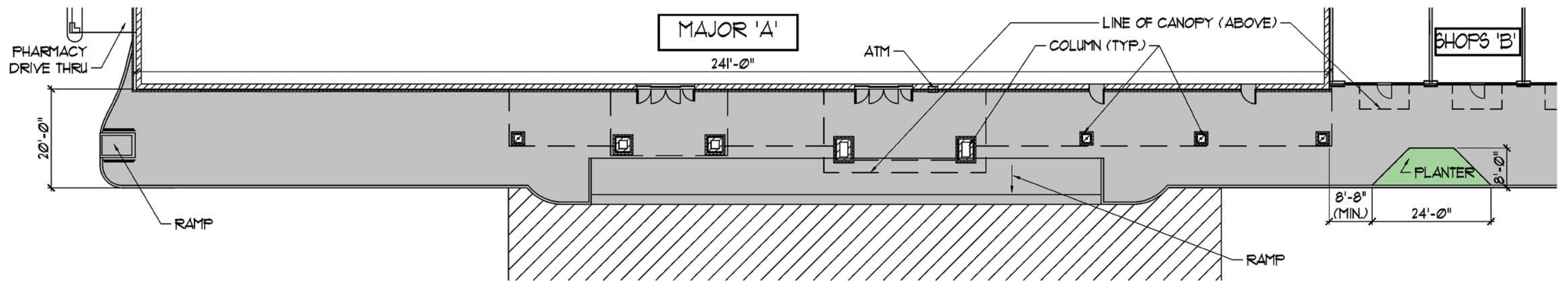
NOTE:
FOR SIGN LOCATIONS
REFER TO PAGE 2.3

EXTERIOR FINISH SCHEDULE

MARK	CATEGORY	MANUFACTURER	DESCRIPTION / FINISH
B-1	MODULAR BRICK	ACME BRICK	COLOR: CHERRYWOOD WITH CEMEX IVORY MORTAR, TOOLED JOINTS
B-2	MODULAR BRICK	TAYLOR	COLOR: 301w WHITE WITH CEMEX IVORY MORTAR, TOOLED JOINTS
EF-2	STUCCO	DRYVIT	FINISH SANDBLAST, COLOR TO MATCH: SW 6094 "SENSATIONAL SAND"
EF-3	STUCCO	DRYVIT	FINISH SANDBLAST, COLOR TO MATCH: SW 6154 "NACRE"
EF-4	STUCCO	DRYVIT	FINISH SANDBLAST, COLOR TO MATCH: SW 6156 "RAMIE"
M-1	METAL	PAC-CLAD	STANDING SEAM ROOF, COLOR: "AGED COPPER"

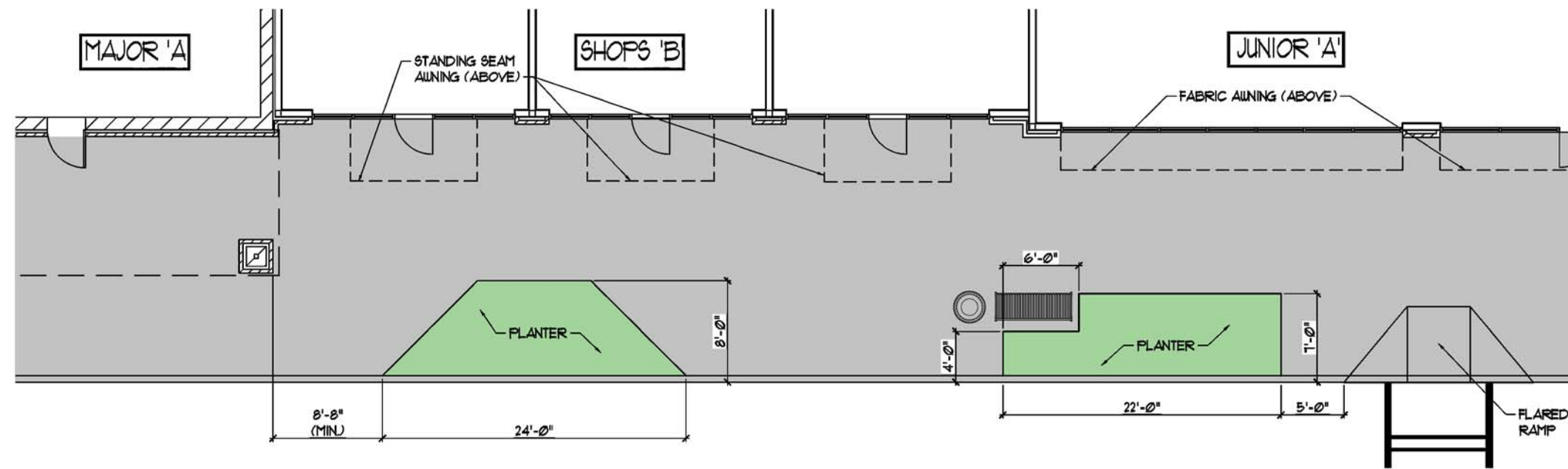
TENANT MAJOR 'A'

Planters



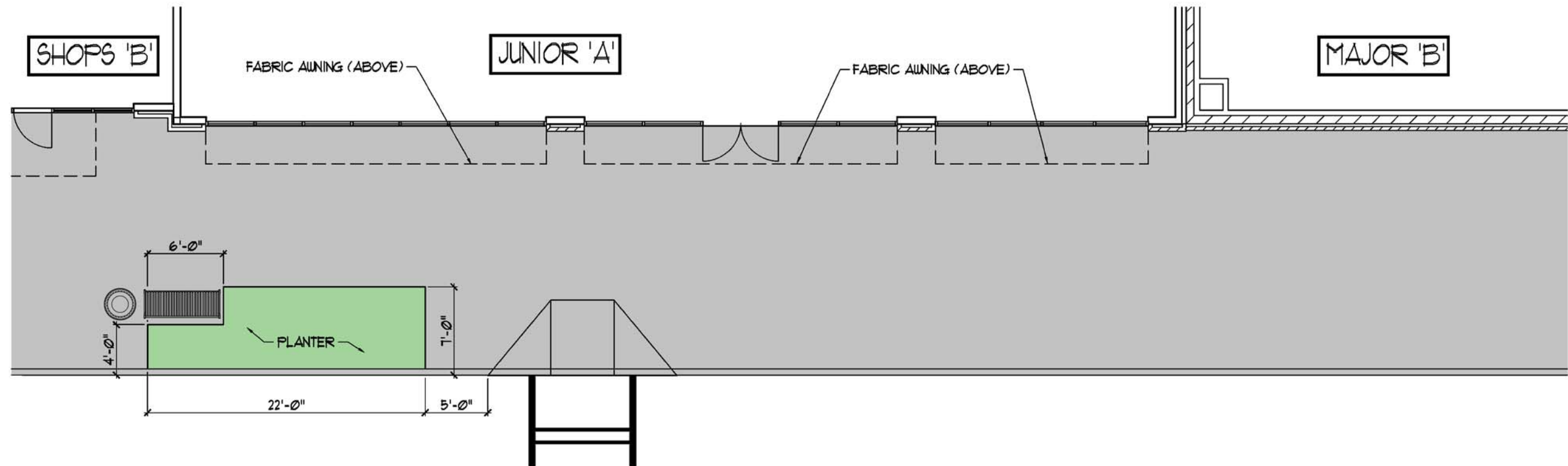
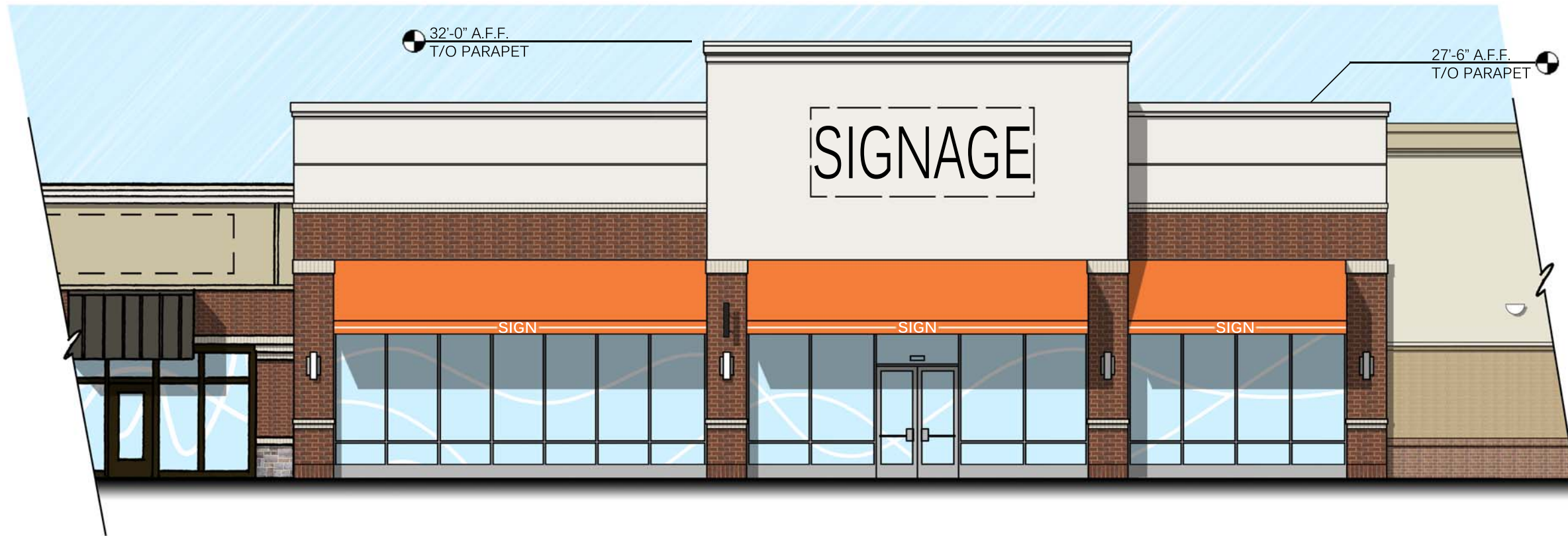
MAJOR 'A' & PARTIAL SHOPS 'B' - PLANTER INFORMATION

SHOPS B ELEVATION

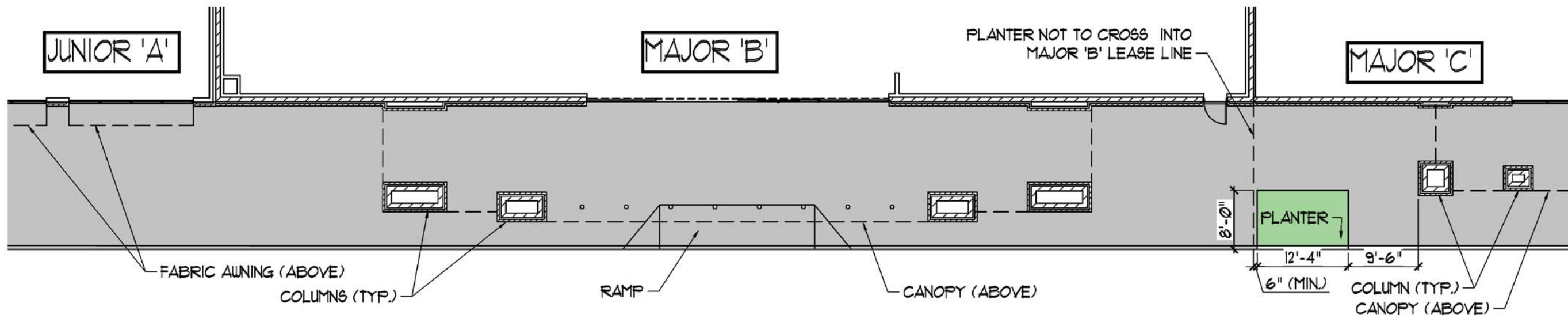
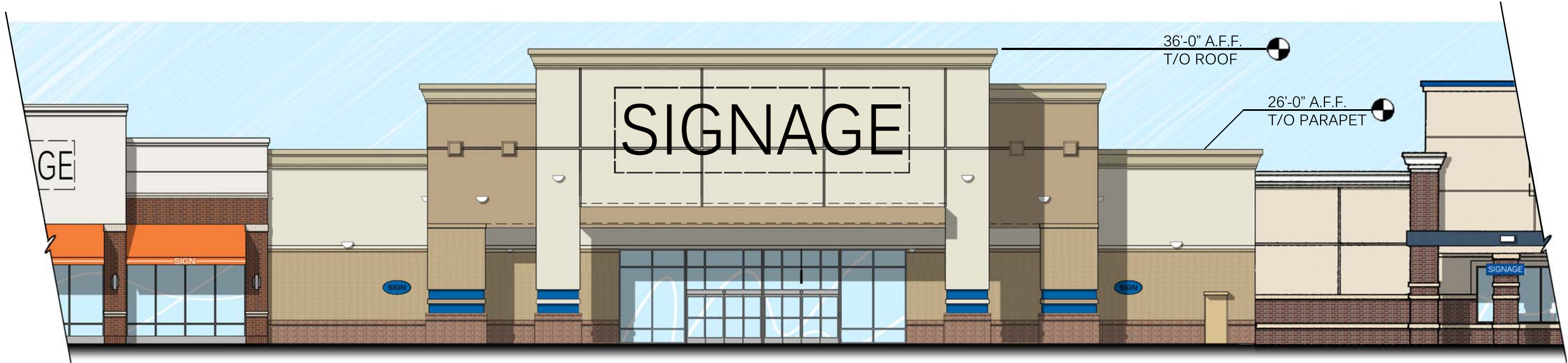


SHOPS 'B' & PARTIAL JUNIOR 'A' - PLANTER INFORMATION

TENANT JUNIOR 'A'

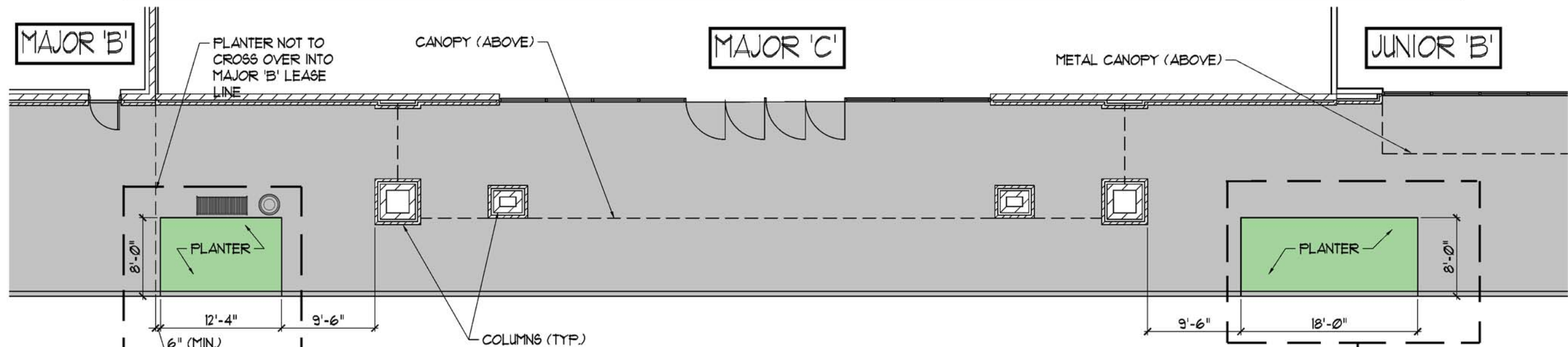


JUNIOR 'A' - PLANTER INFORMATION

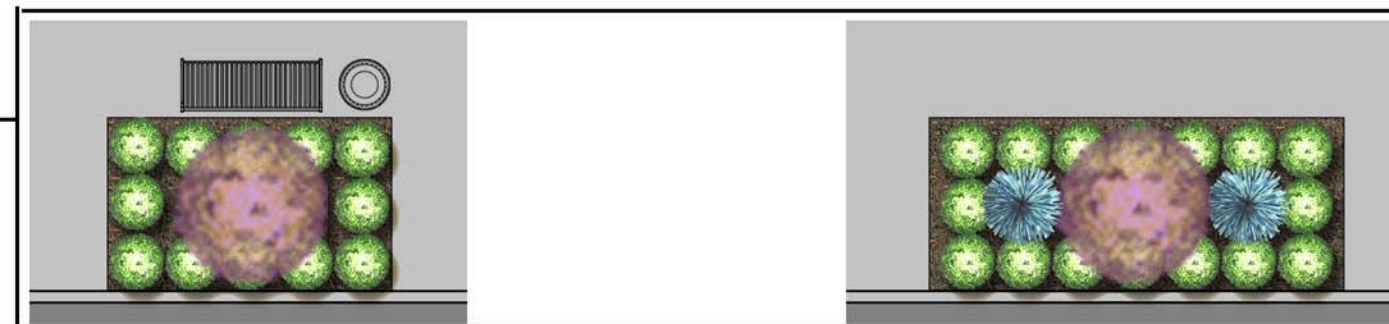


MAJOR 'B' & PARTIAL MAJOR 'C' - PLANTER INFORMATION

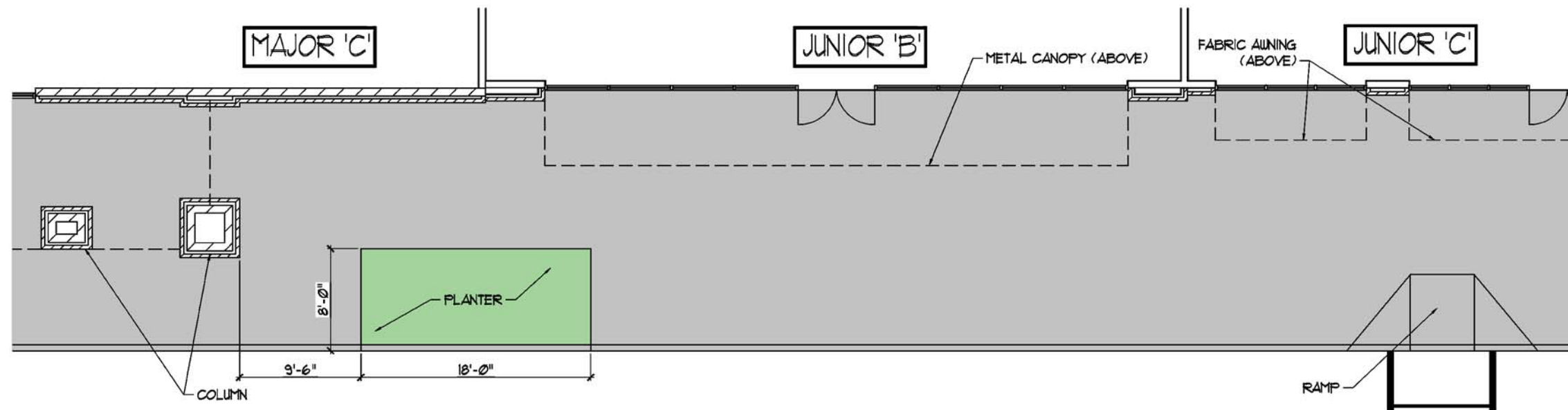
TENANT MAJOR 'C'



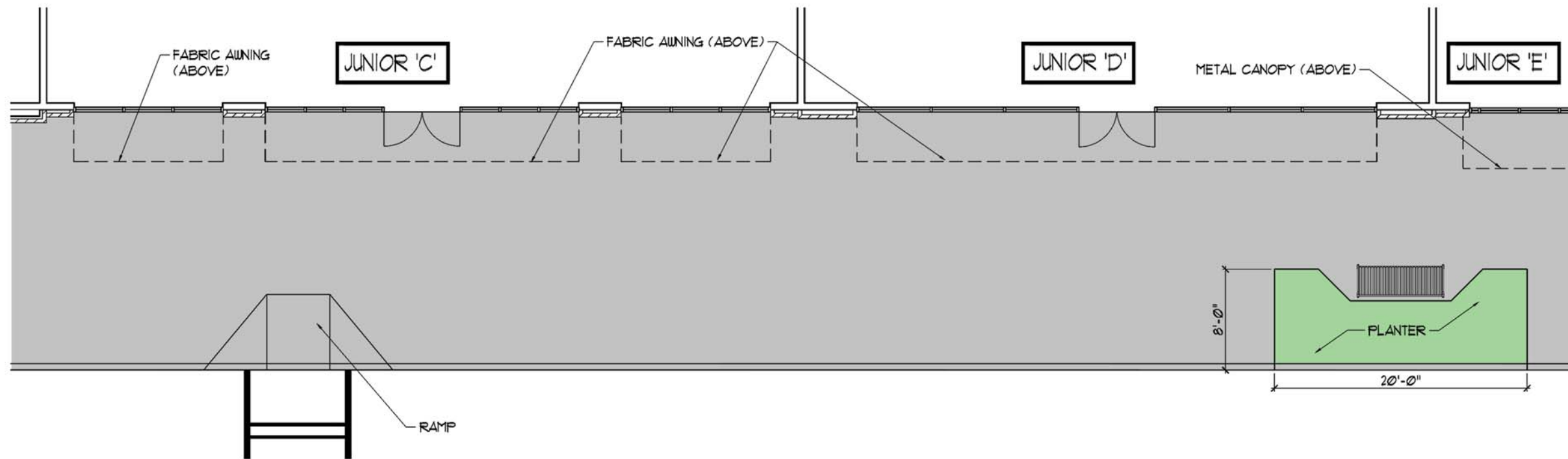
MAJOR 'C' & PARTIAL JUNIOR 'B' - PLANTER INFORMATION



TENANT JUNIOR 'B'

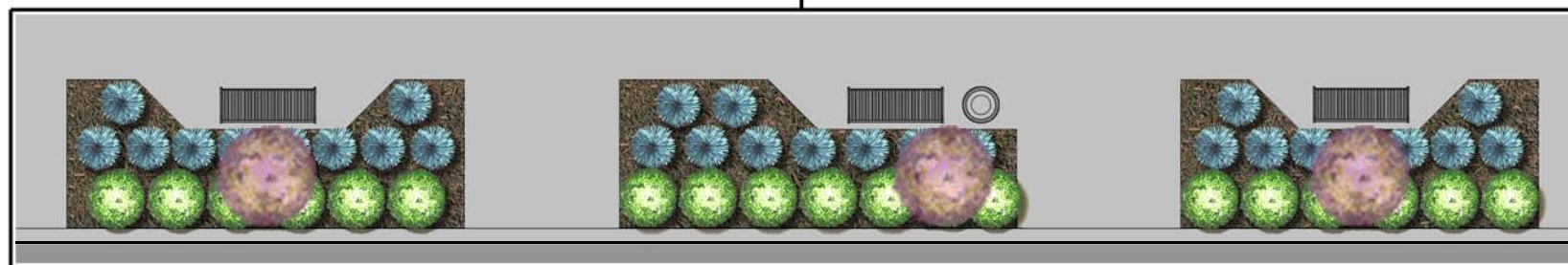
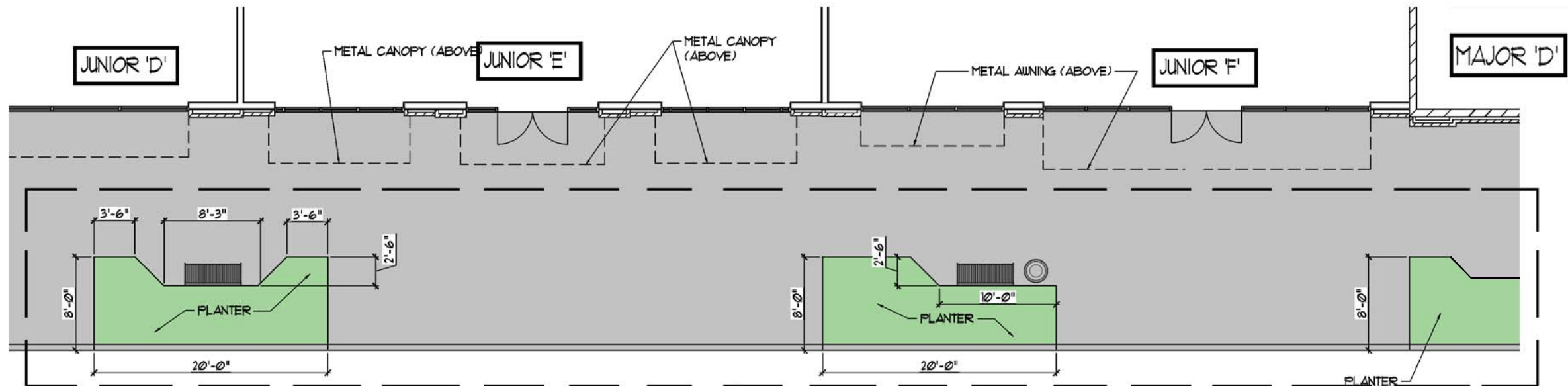


JUNIOR 'B' & PARTIAL MAJOR 'C' & PARTIAL JUNIOR 'C' - PLANTER INFORMATION

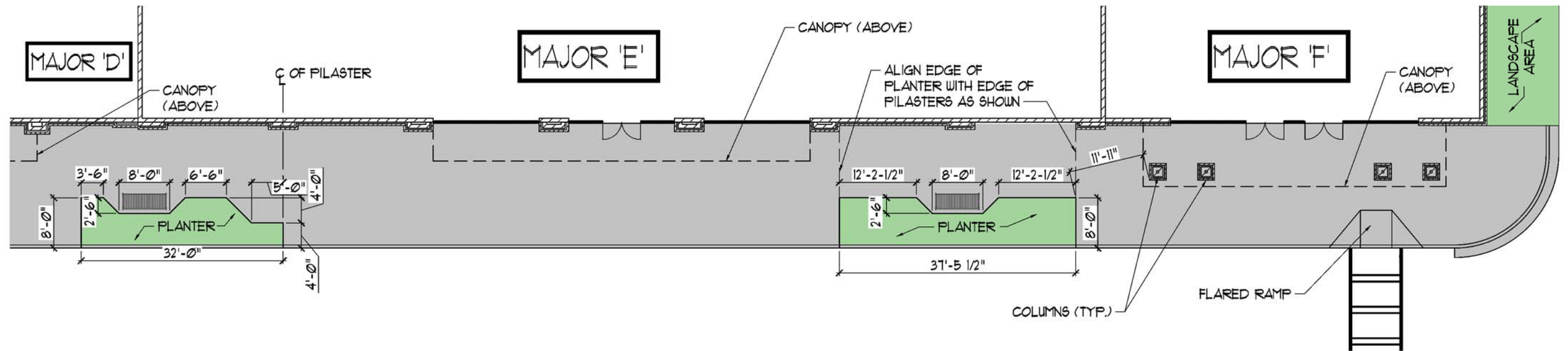


JUNIOR 'C' & JUNIOR 'D' & PARTIAL JUNIOR 'E' - PLANTER INFORMATION

TENANT JUNIOR 'E' & JUNIOR 'F'

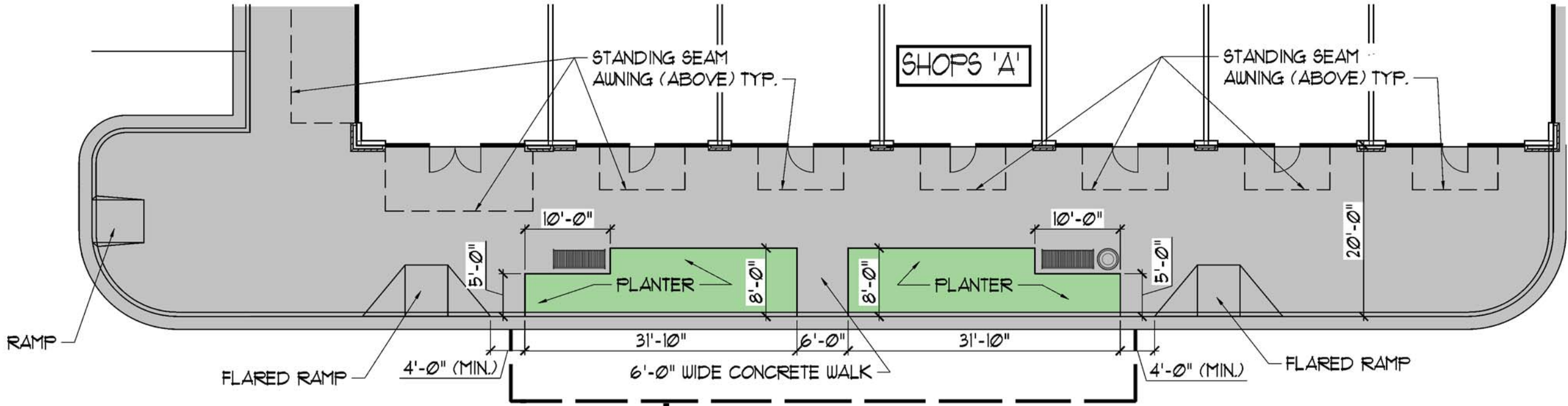


JUNIOR 'E' & JUNIOR 'F' & PARTIAL JUNIOR 'D' - PLANTER INFORMATION

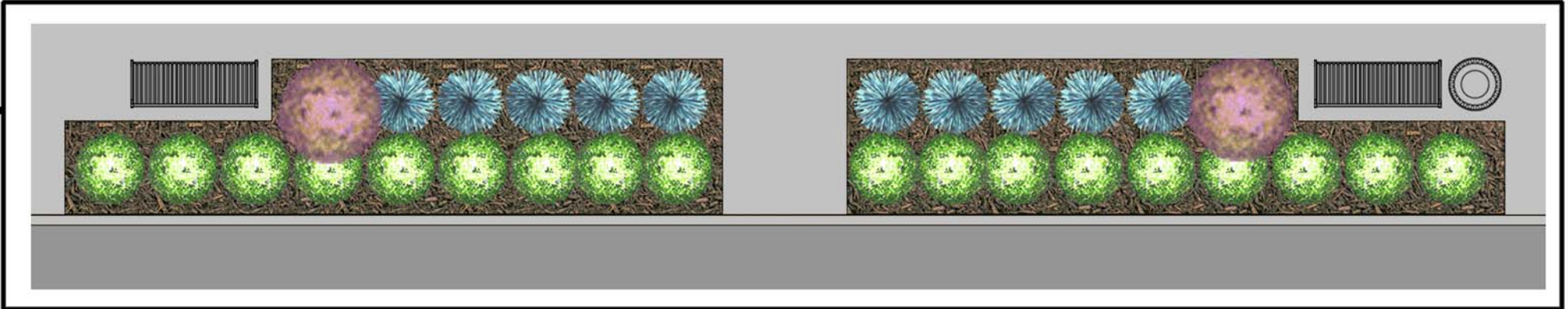


MAJOR 'D' & MAJOR 'E' & MAJOR 'F' & PARTIAL MAJOR 'D' - PLANTER INFORMATION

SHOPS A ELEVATIONS



SHOPS 'A' - PLANTER INFORMATION





Variance/Conditional Use Application

Application must be submitted to the Code Department 45 days prior to the Planning & Zoning

Meeting of: _____

Your representative must be present at the meeting

Street address _____ Council District _____ / _____ Map and Parcel # _____
Zoning _____ Acreage _____ Proposed Use _____ Road Frontage _____ ft. / on
_____ (street or streets)

Applicant
Name _____
Address _____
Phone # _____

Owner
Name _____
Address _____
Phone # _____

Request Type: (check one) Variance Conditional Use

Nature of proposed use, including without limitation the type of activity proposed, manner of operation, number of occupants and/or employees, hours of operation, number of vehicle trips, water and sewer use, and similar matters:

Proposed use is a planned residential development

State relationship of structure and/or use to existing structures and uses on adjacent lots;

adjacent lots are residential as well

State reason for request and how it complies with the Zoning Ordinance section 1425.5(1)-(10) & 1430.6(1)-(8):

We are requesting a variance on the setback from the stream on the western side of the lot. We are asking for it to be reduced from 75' to 50'. This change will allow for the construction of a driveway to serve two of the homes.

State area, dimensions and details of the proposed structure(s) or use(s), including without limitation, existing and proposed parking, landscaped areas, height and setbacks of any proposed buildings, and location and number of proposed parking/loading spaces and access ways:

(see attached)

State the particular hardship that would result from strict application of this Ordinance:

The current setback doesn't allow for the best layout of the parking and driveways within the development.

Check all that apply: Public Water: Well: Public Sewer: Septic: Electrical: Gas:

For any application for an overlay district, a Certificate of Appropriateness or a letter of support from the Historic Preservation Commission or the Corridor Design Commission for the district is required.

Documents to be submitted with request:

- Recorded deed
- Survey plat
- Site plan to scale
- Proof of current tax status

- Application Fees:
- \$100 Single Family
 - \$300 Multi Family
 - \$200 Commercial

Each applicant has the duty of filing a disclosure report with the City if a contribution or gift totaling two hundred and fifty dollars (\$250.00) or more has been given to an official of the City of Monroe within the last two (2) years.

The above statements and accompanying materials are complete and accurate. Applicant hereby authorizes Code department personnel to enter upon and inspect the property for all purposes allowed and required by the zoning ordinance and the development regulations.

Signature Sadie Krawczyk Date: 5/6/2019

**PUBLIC NOTICE WILL BE PLACED AND REMOVED BY THE CODE DEPARTMENT
SIGN WILL NOT BE REMOVED UNTIL AFTER THE COUNCIL MEETING.**

***Property owners signature if not the applicant**

Signature _____ Date: _____

_____ Date: _____

Notary Public

Commission Expires: _____

I hereby withdraw the above application: Signature _____ Date _____

Return Recorded Document to:
PRESTON & MALCOM, P.C.
Attorneys at Law
Post Office Box 984
110-112 Court Square
Monroe, Georgia 30655
File #: 13-23459

[Space above this line for recording data]

QUITCLAIM DEED

STATE OF GEORGIA
COUNTY OF WALTON

THIS INDENTURE, Made the 23rd day of August, 2013, between

RHONDA CONNELLY f/k/a RHONDA LEE MATHEWS,

of the County of Walton, and the State of Georgia, as party or parties of the first part, hereinafter called Grantor, and

OPEN WELLS, LLC

of the County of Walton, and the State of Georgia, as party or parties of the second part, hereinafter called Grantee (the words "Grantor" and "Grantee" to include their respective heirs, successors and assigns where the context requires or permits).

WITNESSETH that: Grantor, for and in consideration of the sum of one dollar (\$1.00) and other valuable considerations in hand paid at and before the sealing and delivery of these presents, the receipt whereof is hereby acknowledged, by these presents does hereby remise, convey and forever **QUITCLAIM** unto the said Grantee,

All that tract or parcel of land, together with all improvements thereon, situate, lying and being in the State of Georgia, County of Walton, City of Monroe, located in Land Lot 65 & 66, 3rd District, containing 1.65 Acres, as shown by a plat of survey entitled "Survey for: DKN Properties, LLC", prepared by C.S.A. Confederate Surveying Associates, Inc., certified by D. Evan Castle, Georgia Registered Land Surveyor No. 2657, dated July 6, 2009, recorded in Plat Book 105, Page 113, Clerk's Office, Walton Superior Court. Reference to said plat of survey and the record thereof being hereby made for a more complete description.

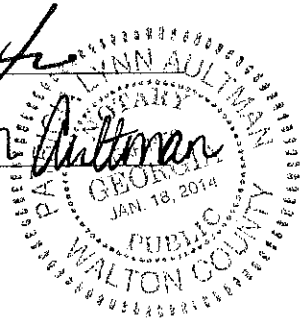
TO HAVE AND TO HOLD the said described premises to grantee, so that neither grantor nor any person or persons claiming under grantor shall at any time, by any means or ways, have, claim or demand any right to title to said premises or appurtenances, or any rights thereof.

IN WITNESS WHEREOF, the Grantor has signed and sealed this deed, the day and year first above written.

Signed, sealed and delivered in the presence of:

Ann Smith
(Unofficial witness)

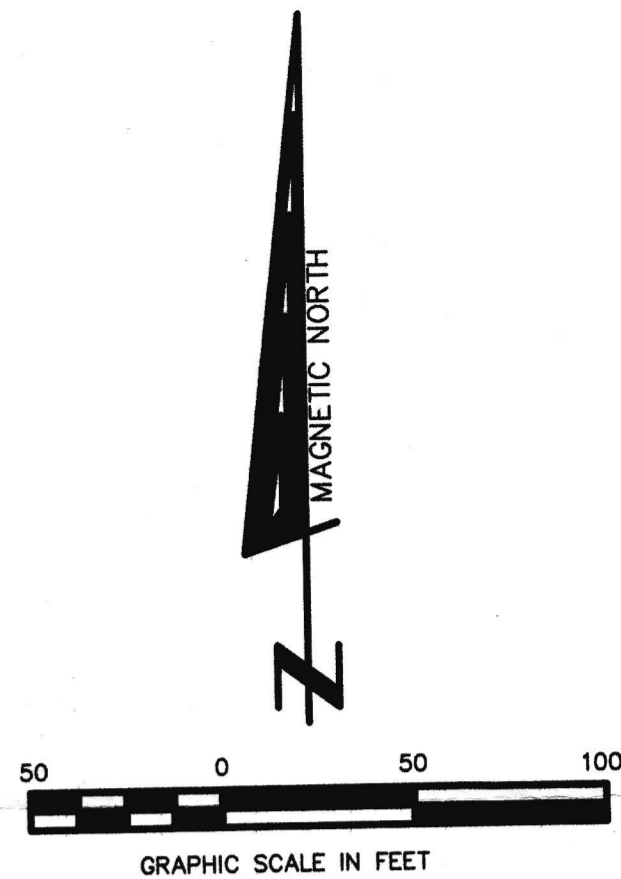
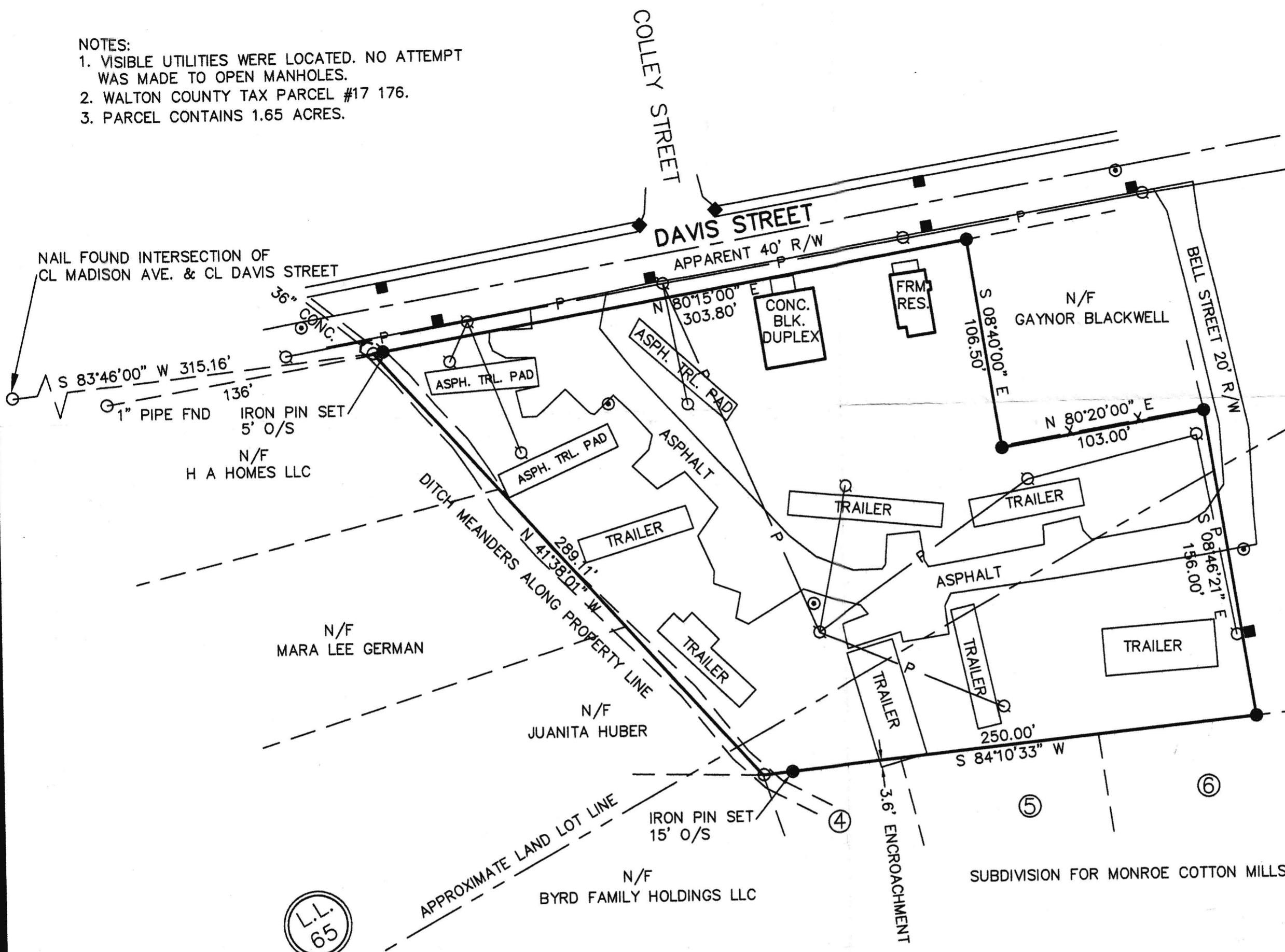
Debra Lynn Aultman
(Notary Public)



Rhonda Connelly (Seal)
RHONDA CONNELLY f/k/a RHONDA LEE MATHEWS

NOTES:

1. VISIBLE UTILITIES WERE LOCATED. NO ATTEMPT WAS MADE TO OPEN MANHOLES.
2. WALTON COUNTY TAX PARCEL #17 176.
3. PARCEL CONTAINS 1.65 ACRES.



FILED AND RECORDED *7/6/09*
 AT *5:33* A.M. *P.M.*
 PLAT BOOK *105* PAGE *113*
 KATHY K. TROST
 CLERK SUPERIOR COURT
 WALTON COUNTY, GEORGIA

APPROVED FOR RECORDING
[Signature]
 7-6-09

- REFERENCES:**
1. PLAT FOR MRS. ROSA M. WALKER, DATED 2-10-54, BY H.L. DUNAHOO, RECORDED IN PLAT BOOK 7 PAGE 86.
 2. SURVEY FOR PAULA Q. BYRD & J. HAROLD BYRD, DATED 4-21-95, BY SIMS SURVEYING CO, RECORDED IN PLAT BOOK 66 PAGE 58.
 3. SURVEY FOR W.B. NEWTON, MD, DATED 8-14-80, BY GREGG & ASSOC., RECORDED IN PLAT BOOK 27 PAGE 29.
 4. SUBDIVISION PLAT FOR MONROE COTTON MILLS, DATED 8-21-63, BY PICKERELL & PICKERELL, RECORDED IN PLAT BOOK 10 PAGE 215.
 5. WARRANTY DEED TO H A HOUSES LLC, DATED 1-02-02, RECORDED IN DEED BOOK 1390 PAGE 62.

The field data upon which this survey is based has a relative closure precision of one foot in 26.912 feet and a relative angular error of 01 per angle point and was adjusted using Least Squares.

This plat has been calculated for closure and is found to be accurate within one foot in 100,000+ feet.

Equipment used for measurements:
 Electronic total station
 Direct Reading:
 Angular: 1 second
 Linear: 0.001 feet

In my opinion, this plat is a correct representation of the land platted and has been prepared in conformity with the minimum standards and requirements of law.

NOTE:
 THIS PROPERTY IS NOT LOCATED IN A FLOOD HAZARD AREA PER WALTON COUNTY F.I.R.M. FLOOD MAP PANEL NO. 13297C0112B, DATED 2/16/90.

LEGEND

- ☒ GAS VALVE
- COMPUTED POINT
- ⊕ POWER POLE
- ⊕ WATER VALVE MARKER
- ⊕ WATER VALVE
- IRON PIN
- ⊕ SANITARY SEWER MANHOLE
- ⊕ FIRE HYDRANT
- ⊕ DROP INLET
- ⊕ SINGLE WING CATCH BASIN
- ⊕ DOUBLE WING CATCH BASIN
- ⊕ HEADWALL
- ⊕ CONCRETE MONUMENT FOUND
- ⊕ WATER METER

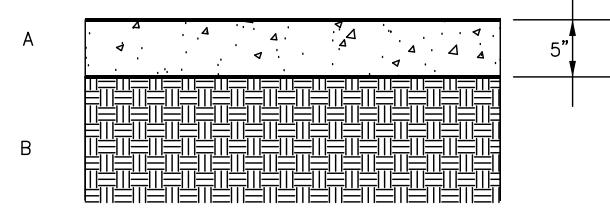
	DATE	REVISION	SURVEY FOR: DKN PROPERTIES, LLC LAND LOT 65 & 66, 3RD DISTRICT, CITY OF MONROE, WALTON COUNTY, GEORGIA C.S.A. Confederate Surveying Associates, Inc. 1470 WATERFORD LANE BETWEEN, GEORGIA 30656 770-207-1695
SURVEYED: DEC DR.	DESIGNED:	CHECKED: DEC	PRO. NO. 0903 SCALE: 1"=50' DATE: 7-6-09 SHEET NO. OF

SITE PLAN NOTES

1. SITE CONTAINS 1.35 ACRES TOTAL.
2. PROPERTY IS ZONED MDPRD.
3. TOPOGRAPHIC INFORMATION TAKEN FROM A SURVEY BY ALCOVY SURVEYING AND ENGINEERING, INC. DATED 6/19/15.
4. NO PART OF THIS SITE IS IN A FLOOD HAZARD ZONE ACCORDING TO F.I.R.M. COMMUNITY PANEL NO. 113297C0137D, DATED 5/18/09.
5. THERE ARE NO MAXIMUM ALLOWABLE BUILDING HEIGHT LIMIT.
6. ALL CURB RADI ARE 5' MEASURED TO FOC UNLESS OTHERWISE NOTED.
7. ALL CURB DIMENSIONS ARE TO THE BACK OF CURB UNLESS OTHERWISE NOTED.
8. ALL CONSTRUCTION MATERIALS AND PROCEDURES SHALL CONFORM TO CITY OF MONROE AND GEORGIA DEPARTMENT OF TRANSPORTATION STANDARDS.
9. SEE ARCHITECTURAL PLANS FOR BUILDING FLOOR PLAN DIMENSIONS, DOOR LOCATIONS, AND OTHER ARCHITECTURAL DETAILS.
10. HANDICAP PARKING SPACES SHALL BE DESIGNATED AS RESERVED FOR DISABLED BY A SIGN SHOWING THE SYMBOL OF ACCESSIBILITY. SUCH SIGNS SHALL NOT BE OBTAINED BY VEHICLES PARKING IN THE SPACES.
11. CONTRACTOR TO NOTIFY INSPECTOR AT LEAST 24 HOURS BEFORE BEGINNING EACH PHASE OF CONSTRUCTION.
12. SIGNS, LOCATION, NUMBER AND SIZE ARE NOT APPROVED UNDER THIS DEVELOPMENT PERMIT.
13. NO CERTIFICATE OF OCCUPANCY WILL BE ISSUED UNTIL ALL SITE IMPROVEMENTS HAVE BEEN COMPLETED.
14. HIGH INTENSITY LIGHTING FACILITIES SHALL BE SO ARRANGED THAT THE SOURCE OF ANY LIGHT IS CONCEALED FROM PUBLIC VIEW AND FROM ADJACENT RESIDENTIAL PROPERTY AND DOES NOT INTERFERE WITH TRAFFIC.
15. THE LOCATIONS OF ALL BUILDINGS FROM PROPERTY LINES AND OTHER BUILDINGS SHALL COMPLY WITH THE HEIGHT AND AREA LIMITATIONS OF TABLE 500 AND THE FIRE RESISTIVE REQUIREMENTS OF TABLE 600 OF THE 1995 EDITION OF THE STANDARD BUILDING CODE BASED ON OCCUPANCY GROUP CLASSIFICATIONS AND TYPES OF CONSTRUCTION.
16. NO DRIVE UP WINDOWS ARE PROPOSED.
17. BUILDING NOT TO BE SPRINKLERED.
18. NO SIGNAGE IS PROPOSED AT THIS TIME AND THE APPROPRIATE SIGN PERMITS TO BE OBTAINED AT A LATER DATE.
19. THERE ARE NO EXISTING OR PROPOSED INERT WASTE BURY PITS ON THIS SITE.
20. OWNER IS RESPONSIBLE FOR MAINTENANCE OF STORM DRAIN SYSTEM OUTSIDE OF RIGHT OF WAY.
21. OWNER IS RESPONSIBLE FOR COMPLIANCE WITH N.P.D.E.S. GENERAL PERMIT REQUIREMENTS.
22. THERE ARE NO WETLANDS ON THIS SITE.
23. SEWER SERVICE FOR THIS SITE WILL BE PROVIDED BY CITY OF MONROE.
24. WATER SERVICE FOR THIS SITE WILL BE PROVIDED BY CITY OF MONROE.
25. THERE ARE NO CEMETERIES OR OTHER SIGNIFICANT OR HISTORICAL AREAS ON THE SITE. NECESSARY BARRICADES, SUFFICIENT LIGHTS, SIGNS AND OTHER TRAFFIC CONTROL METHODS AS MAY BE NECESSARY FOR THE PROTECTION AND SAFETY OF THE PUBLIC SHALL BE PROVIDED AND MAINTAINED THROUGHOUT THE WIDENING OF AND CONSTRUCTION ON ROADS IN CITY OF MONROE.

A. 5.0 INCH CONCRETE W/ FIBER MESH

B. STABILIZED SUBGRADE
RAW SUBGRADE SOIL (IN SITU OR COMPACTED FILL)
COMPACTED TO A MINIMUM OF 100% STD. PROCTOR



CONC. ALLEYWAY
NO SCALE

TOTAL SITE AREA = 1.35 ACRES
TOTAL DISTURBED AREA = 0.99 ACRES
THERE ARE STATE WATERS ON THE SITE.
THERE ARE NO NWI WETLAND ON SITE. UPON A SITE VISIT, THERE WERE NO VEGETATION WHICH INDICATES WETLANDS ON SITE.

SITE PLAN KEYED NOTES

- (A) EX. POWER POLE TO BE RELOCATED
- (B) ACCESSIBLE PARKING SPACE TYPICAL. SEE DETAIL SHEET.
- (C) ACCESSIBLE RAMP. SEE DETAIL SHEET D-2.
- (Dg) 24" CONCRETE CURB AND GUTTER (TYPE A) TYPICAL. SEE DETAIL SHEET.
- (Db) 24" CONCRETE CURB AND GUTTER (TYPE B) TYPICAL. SEE DETAIL SHEET.
- (E) 20X20' PARKING PAD
- (F) 20X50' PARKING PAD
- (G) 4' CONCRETE SIDEWALK.



VICINITY MAP
N.T.S.

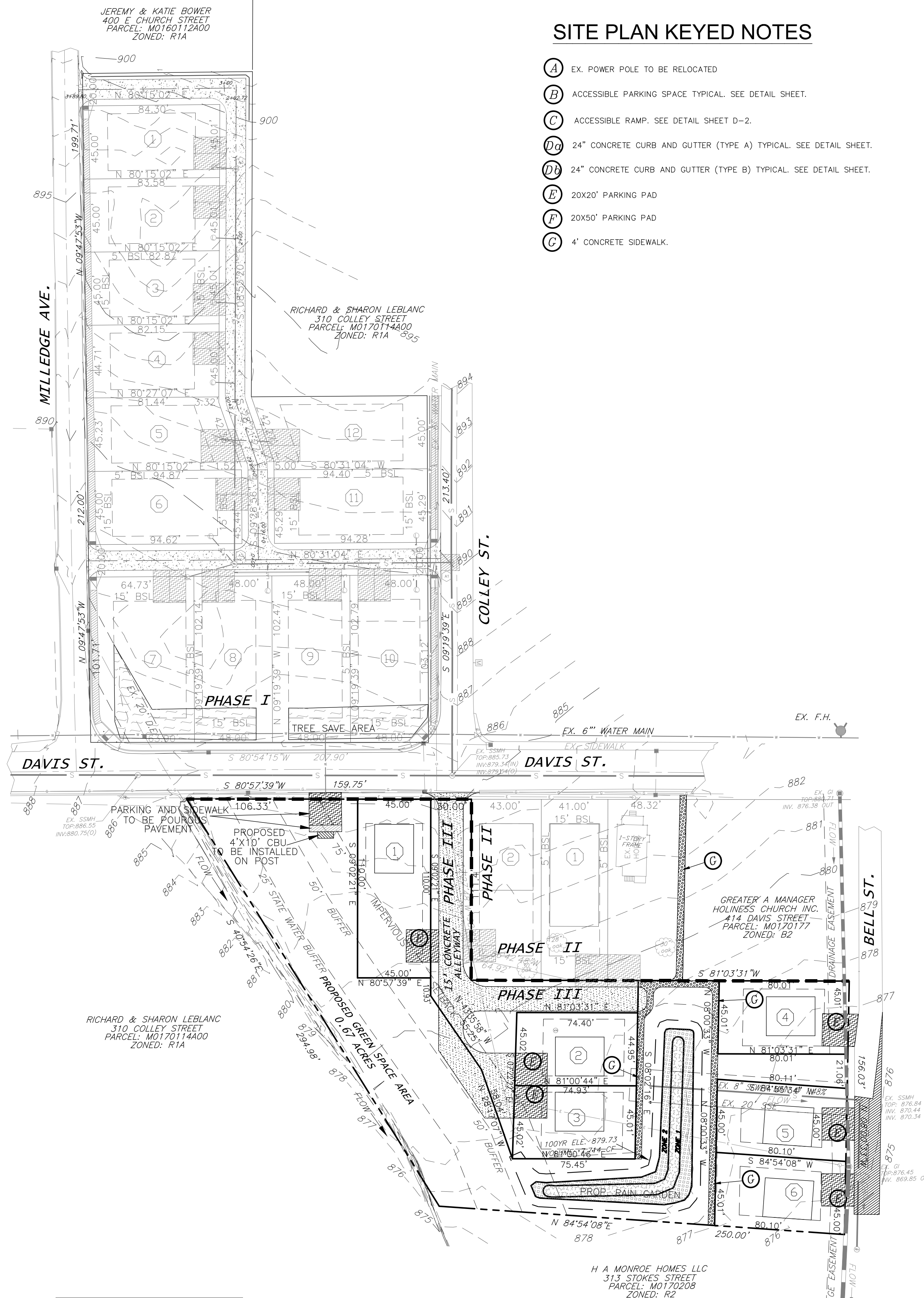
TOTAL AREA = 2.83 AC
PROPOSED 20 RESIDENTIAL UNITS
PROPOSED DENSITY = 6.36 UNITS/AC

Density; Open Space; Set Back; Parking Requirements

- (1) The permitted usage for a MDPRD development is single-family detached or attached dwellings.
- (2) The maximum permitted density in the MDPRD shall be ten dwelling units per gross acre.
- (3) A minimum separation of [10] feet shall be provided between all principal buildings and structures.
- (4) All MDPRD developments shall have set backs consistent with the form and style of set backs of similar properties in the MDPRD. Setbacks generally should be at least 15 feet from the public right of way unless surrounding forms suggest otherwise.
- (5) The MDPRD shall provide one and one half (1.5) parking spaces per dwelling unit. In order to promote shared functional open space and access to and from the site, all parking shall be located in clusters, which provide not more than five adjoining spaces each. Parking on the MDPRD is prohibited in the front yard set back areas. Off street parking is defined as clustered parking which is adjacent to a city maintained street and which is approved as part of the MDPRD site plan. All on site parking other than off street parking which is adjacent to structures shall be permitted only when it is located toward the rear of the principal structure and is served by an alley or private drive specifically designated on the MDPRD development site plan. A MDPRD development may also provide on street parking for some of the units if the on street parking area is contiguous to the MDPRD development and the MDPRD development is located on a city maintained street not defined as an arterial or collector street. Any such on street parking shall be specifically designated in the MDPRD development site plan, and may not encroach upon the paved width of the street, and must be specifically approved by the Code Enforcement Officer. All MDPRD developments shall be permitted to utilize materials other than concrete and asphalt for driveways and parking areas for on site parking and ingress and egress that are approved by the Code Enforcement Officer.
- (6) Each building shall be located on a lot or parcel that provides lot frontage onto a public street or public green space.
- (7) Rear alley drives shall be a minimum of 12 feet wide.

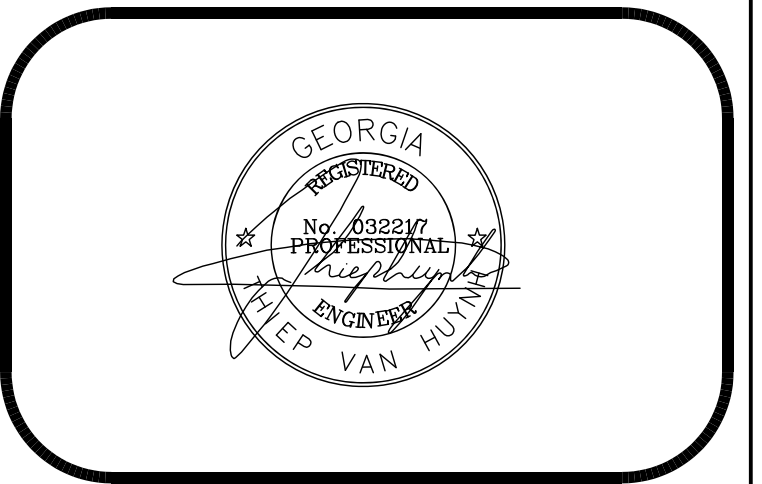
Dwelling Unit Size; Dwelling Unit Design; Private Open Space Requirements

- (1) All single story dwelling units shall have a minimum of 800 square feet and a maximum of 2,600 square feet. All dwelling units which are one and one-half or two story shall have a minimum of 650 square feet on the first floor and a minimum combined 1200 square feet for both floors. All two story dwelling units shall contain a maximum of 2,600 square feet for both floors and a maximum of 1,800 square feet for the first floor.
- (2) Areas of the dwelling unit that do not count toward the total floor calculation shall include unheated storage space, unheated porches or patio areas, architectural projections (such as bay windows), attached roof porches, detached garages or utility buildings and so forth.
- (3) All dwelling units shall have pitched roofs with a minimum 6/12 pitch. All dwelling units shall have roofing of an architectural type asphalt shingle, metal or other similar material type roofing approved by the Code Enforcement Officer. Standard non-dimensional three tab asphalt shingles are prohibited. The maximum building height permitted for dwelling units shall be eighteen (18) feet excluding the pitched roof. The highest point of any pitched roof may not exceed twenty-five (25) feet.
- (4) All dwelling units shall have a covered entry porch with a floor area measuring at least 60 square feet in size and the floor having minimum dimensions of not less than six feet in length or width.
- (5) Each dwelling unit shall provide an area of private open space in both the front and the rear yards. The private open space shall separate the main entrance to the dwelling unit from the common open space so as to create a private yard area. The private open space shall be separate from the common open space with a small hedge, picket fence or other visual separation material approved by the Code Enforcement Officer. Each dwelling unit shall be provided with a minimum of 200 square feet of usable private open space and no dimension of the private open space shall be less than eight feet.
- (6) All dwelling units in a MDPRD development shall be constructed on crawl space or raised contained slab, and all foundation walls shall be brick veneered.



LOT #	AREA
1	0.11 ACRES 4,950.00 SQ. FT.
2	0.08 ACRES 3,357.56 SQ. FT.
3	0.08 ACRES 3,383.29 SQ. FT.
4	0.08 ACRES 3,600.48 SQ. FT.
5	0.08 ACRES 3,598.70 SQ. FT.
6	0.08 ACRES 3,600.00 SQ. FT.

ALCOVY
SURVEYING & ENGINEERING, INC.
P.O.C. TIP HUYNH, P.E.
2205 Highway 81 South
Loganville, Georgia 30052
Phone: 770-466-4002
Fax: 770-466-4296
tip@alcovyse.com
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VARIANCE REQUEST

PROPOSED
MADISON DAVIS
PLANNED RESIDENTIAL
DEVELOPMENT
(MDPRD)
PHASE III

M0170176

PARCELS: M0160112, M0170113

GMD DISTRICT:
DAVIS ST.

CITY OF MONROE, GA

9/4/2018

1"=40'

OWNER/DEVELOPER

BRIAN KRAWCZYK
411 E. MADISON AVE.
MONROE, GA 30655

24 HOUR - EMERGENCY CONTACT
BRIAN KRAWCZYK
404-944-7992

REVISIONS

NO.	DATE	DESCRIPTION
1	1/4/19	UTILITY COMMENTS

JOB No. 13-089

V-1

2018 Property Tax Statement

Tax Commissioner
303 South Hammond Drive STE 100
Walton County Government Building
Monroe, Georgia 30655

Ph: 770-266-1736, Fax: 770-267-1416

OPEN WELLS LLC
411 S MADISON AVENUE

MONROE, GA 30655

Bill No.	Due Date	Current Due	Prior Payment	Back Taxes	*Total Due*
2018-26891	11/15/2018	\$0.00	\$1615.25	\$0.00	Paid 12/04/2018

Map: M0170-00000-176-000

Location: 350 DAVIS STREET

Account No: 537820 010

The Tax Commissioner is the tax collector and is not responsible for values nor for rates. If you feel the assessed fair market value of your property is incorrect, please contact the Tax Assessors office at 770-267-1352.

RETURN THIS PORTION WITH PAYMENT

(Interest will be added per month if not paid by due date)

Payments made after the due date are subject to interest and penalties governed by Georgia Code. State law requires all tax bills to be mailed to owner of record on January 1st. If property has been sold, please contact our office.

Tax Commissioner
303 South Hammond Drive STE 100
Walton County Government Building
Monroe, Georgia 30655

Ph: 770-266-1736, Fax: 770-267-1416



Tax Payer: OPEN WELLS LLC
Map Code: M0170-00000-176-000
Description: MH-PK(10SP) 1.31AC
Location: 350 DAVIS STREET
Bill No: 2018-26891

Building Value	Land Value	Acres	Fair Market Value	Due Date	Billing Date	Payment Good through	Exemptions
0.00	0.00	0.0000	\$101,900.00	11/15/2018	08/08/2018		

Entity	Adjusted FMV	Net Assessment	Exemptions	Taxable Value	Millage Rate	Gross Tax	Credit	Net Tax
CITY BOND	\$0.00	\$40,760.00	\$0.00	\$40,760.00	0.002	\$80.66	\$0.00	\$80.66
CITY TAX	\$0.00	\$40,760.00	\$0.00	\$40,760.00	0.005	\$382.05	-\$166.10	\$215.95
COUNTY	\$0.00	\$40,760.00	\$0.00	\$40,760.00	0.011	\$542.31	-\$97.82	\$444.49
SCH BOND	\$0.00	\$40,760.00	\$0.00	\$40,760.00	0.003	\$105.98	\$0.00	\$105.98
SCHOOL	\$0.00	\$40,760.00	\$0.00	\$40,760.00	0.019	\$758.14	\$0.00	\$758.14
TOTALS					0.039	\$1,869.14	-\$263.92	\$1,605.22

State law requires all tax bills to be mailed to the owner of record on January 1st. If property has been sold, please contact our office.

This bill is not sent to your mortgage company. If you have an escrow account, please forward a copy of this bill to your mortgage company. We encourage you to pay by mail or on our website at www.waltoncountypay.com

Certain persons are eligible for certain homestead exemptions from ad valorem taxation. In addition, certain elderly persons are entitled to additional homestead exemptions. Applications must be filed by April 1st.

For eligibility requirements regarding exemptions or questions about your value, contact the Tax Assessors office at 770-267-1352.

Current Due	\$1,605.22
Discount	\$0.00
Penalty	\$0.00
Interest	\$0.00
Other Fees	\$10.03
Previous Payments	\$1,615.25
Back Taxes	\$0.00
Total Due	\$0.00
Paid Date	12/04/2018



City of Monroe
 215 N. Broad Street
 Monroe, GA 30655
 (770)207-4674

Plan Report

Plan NO.: PCOM-000015-2019

Plan Type: Planning Commission

Work Classification: Certificate of Appropriateness

Plan Status: In Review

Apply Date: 05/17/2019

Expiration:


Location Address	Parcel Number
1190 W SPRING ST, MONROE, GA 30655	M0060095

Contacts	
DMD PARTNERS ENTERPRISES LLC 289 NUNNALLY FARM RD, MONROE , GA 30655 (404)405-5179	Owner MHODGES289@GMAIL.COM
DAN & MARSHA HODGES 289 Nunnally Farm RD, Monroe, GA 30655	Applicant

Description: REQUEST FOR COA FOR NEW BUILDING - P&Z MTG 6/18/19 @ 5:30 PM - 215 N BROAD ST	Valuation: <u> \$0.00 </u>
	Total Sq Feet: <u> 0.00 </u>

Fees	Amount
Planning Commission Regular Meeting (COA)	\$50.00
Total:	\$50.00

Payments	Amt Paid
Total Fees	\$50.00
Check # 1030	\$50.00
Amount Due:	\$0.00

<u>Condition Name</u>	<u>Description</u>	<u>Comments</u>
	 Issued By: Debbie Adkinson	May 17, 2019 _____ Date
	_____ Plan_Signature_1	_____ Date
	_____ Plan_Signature_2	_____ Date

REQUEST FOR A CERTIFICATE OF APPROPRIATENESS

Under Monroe's Zoning ordinance, you are required to obtain a Certificate of Appropriateness (COA) from the Planning Commission for any exterior material change you wish to make to your property if your property is located within a Corridor Design Overlay District. Please see pages 44 through 58 of the Zoning Ordinance.

The following steps must be taken in order to have your proposed change considered by the Planning Commission:

1. Stop by the Code Office at least 45 working days in advance of the next regular meeting of the Planning Commission (the Third Tuesday of each month at 5:30 pm), read the Zoning Ordinance, and request that your proposal be added to the agenda.
2. Fill out the application for a COA and turn it in to the Code Office.
3. Pay the \$50.00 fee. If you request a "special meeting" at some date or time (other than the 3rd Tuesday of every month), the fee is \$100.00.
4. Provide all documentation which will assist the Planning Commission in deciding if your proposed change(s) are in keeping with the Corridor Design Overlay District standards and guidelines. The more complete your presentation, the more efficiently your request can be handled. Please see the COA application for additional information.

Suggested documentation consists of all architectural drawings, sketches, pictures, diagrams, or actual examples. All documentation should be turned in to the Code Office at the time of your application, as it gives Planning Commission members time to consider your request prior to the meeting. Physical samples, if applicable, should be brought with you to the meeting.

5. Appear at the meeting on the scheduled day and time to present your proposal.

Thank you, the Planning Commission looks forward to considering your request.

APPLICATION FOR CERTIFICATE OF APPROPRIATENESS
MONROE PLANNING COMMISSION

Please read the Zoning Ordinance (Corridor Sections 643-1 through 643A.6). In addition, please consult with the Code Office to determine if any other Monroe ordinances apply, and if applicable, read those ordinances.

DATE: 5-17-19

APPLICANT: Dan and Marsha Hodges

APPLICANT'S ADDRESS: 289 Nunnally Farm Rd
Monroe Ga 30655

TELEPHONE NUMBER: 404-405-5179

PROPERTY OWNER: DMD Partners Enterprises LLC

OWNER'S ADDRESS: 289 Nunnally Farm Rd
Monroe, Ga 30655

TELEPHONE NUMBER: 404-405-517

PROJECT ADDRESS: 1190 West Spring Street
Monroe, Ga 30655

Brief description of project: Dental / Medical
Office

(Continue on separate sheet, if necessary.)

Attach photograph(s) of existing condition of property necessary to show all areas affected.

Attach plans, sketches, drawings, and diagrams of the project and detail the materials that will be used.

Provide such other information that is necessary for the Planning Commission to adequately consider the application in light of the following criteria.

In reviewing applications for COAs, the Planning Commission duties include taking into account all aspects of the exterior of the structure.

In its review, the Planning Commission shall also take into account the following elements to ensure that the exterior form and appearance of the structure is consistent with the historical and visual character of the District:

1. The height of the building in relation to the average height of the nearest adjacent and opposite buildings;
2. The setback and placement of the building on the lot in relation to the average setback and placement of the nearest adjacent and opposite building;
3. Exterior construction materials, including textures and patterns.
4. Architectural detailing, such as lintels, cornices, brick bond, and foundation materials;
5. Roof shapes, forms, and materials;
6. Proportions, shapes, positionings and locations, patterns and sizes of any elements of fenestration.

The **Code Office** will post a notice which will notify all affected property owners of the material change being requested not less than fifteen (15) days prior to the meeting at which the request for a COA is to be considered. The applicant and the affected property owners will be given an opportunity to be heard at the meeting in which the application is presented.

DEFINITIONS:

A “material change in appearance” means a change that will affect either the exterior architectural or environmental features of a corridor property or any structure, site, or work of art within a corridor design overlay district, and may include any one or more of the following:

1. A reconstruction or alteration of the size, shape, or façade, including any of the architectural elements or details;
2. Demolition;
3. Commencement of excavation for construction purposes;
4. A change in the location of advertising visible from the public right-of-way; and

5. The erection, alteration, restoration, or removal of any building or other structure within a corridor district, including walls, fences, steps and pavements, or other appurtenant features.

[Sec 643.4(4) (a)]

“Exterior architectural features” means the architectural style, general design, and general arrangement of the exterior of a building or other structure, including but not limited to, the kind or texture of the building material and the type and style of all windows, doors, signs, and other appurtenant architectural fixtures, features, details, or elements relative to the foregoing [Sec. 210(65)].

“Landscaping/landscape area” means a portion of lot or land area, either natural or modified for aesthetic or functional purposes through grading and planting, consisting of vegetation including but not limited to trees, shrubs, ground covers, grass, flowers, decorative rock, bark, mulch and other similar materials. [Sec. 210(86)].

Ordinary maintenance or repair of any exterior architectural feature in or on a corridor property that does not involve a material change in design, material, or outer appearance is excluded from review. [Sec. 643.4(3) (b)]

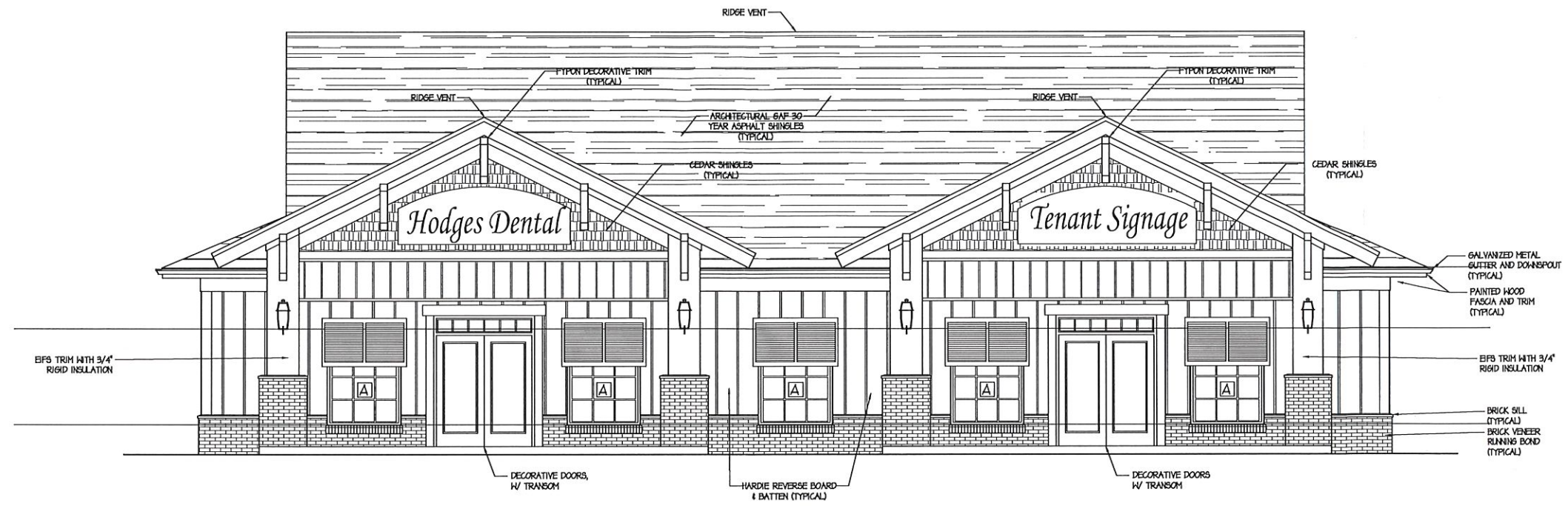


Applicant

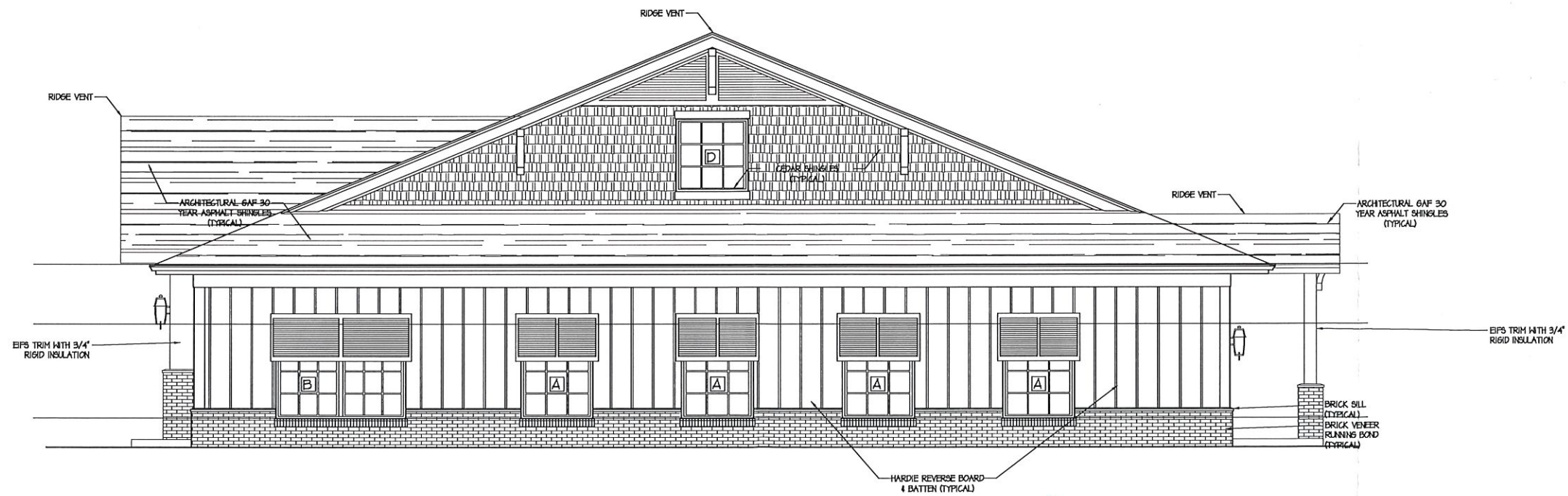
Date: 5-15-19

Effective July 1, 2014

DATE	REVISIONS



FRONT ELEVATION:
SCALE: 1/4" = 1'-0"



RIGHT SIDE ELEVATION:
SCALE: 1/4" = 1'-0"

DANIEL A. HODGES, D.M.D.
1190 WEST SPRING STREET
MONROE, GEORGIA 30655

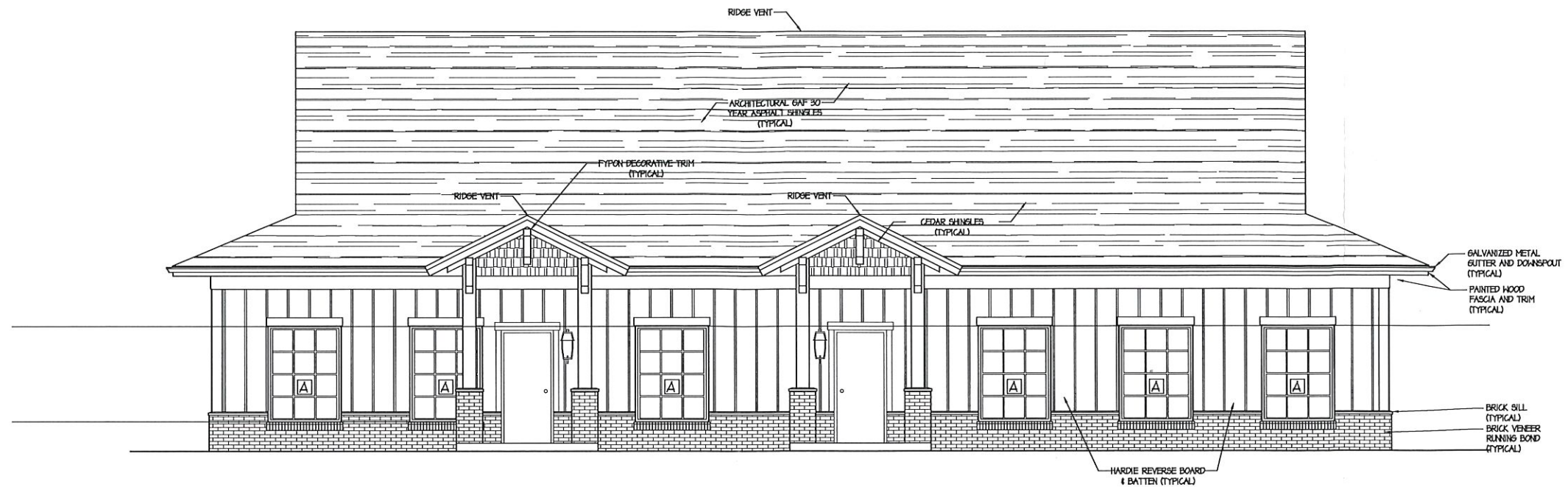
WILLIAM D. STILL, ARCHITECT
STILL ARCHITECTURE, LLC
5755 Chisholm Trail
Lilburn, GA 30047
(404) 395-5278 stillarch@comcast.net

REVIEW PLANS ONLY!
NOT FOR CONSTRUCTION

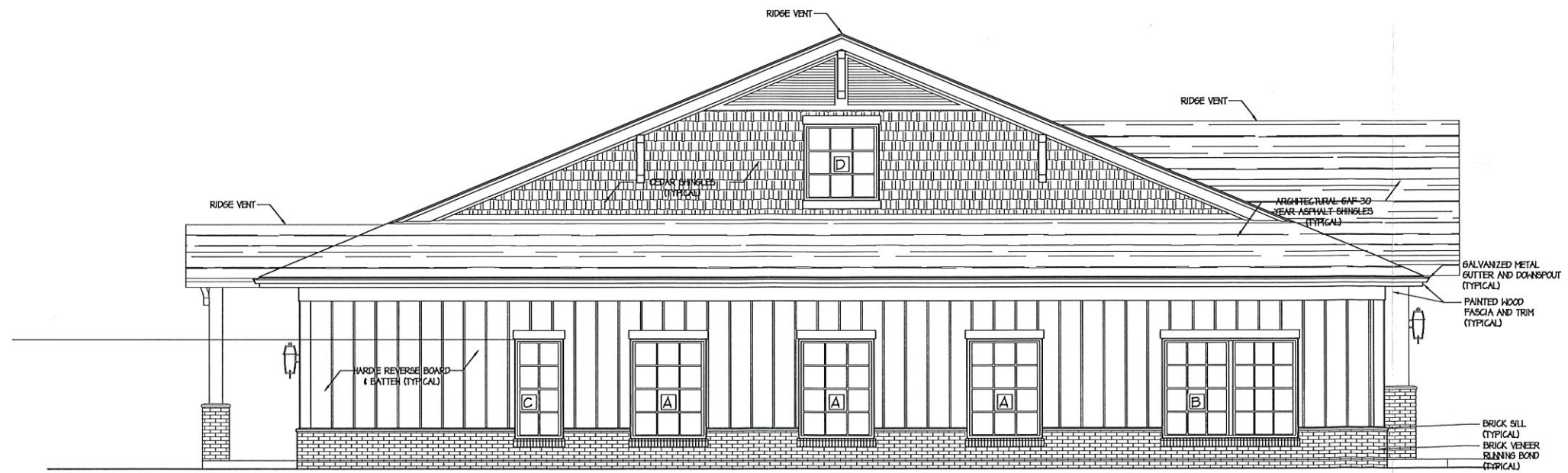
JOB NO.: SAHODGES
FILE: 201
DATE: 04-17-19
DRAWN BY: CADDS

SHEET FRONT AND RIGHT SIDE ELEVATIONS
AS-201

DATE	REVISIONS



REAR ELEVATION:
SCALE: 1/4" = 1'-0"



LEFT SIDE ELEVATION:
SCALE: 1/4" = 1'-0"

DANIEL A. HODGES, D.M.D.
1190 WEST SPRING STREET
MONROE, GEORGIA 30655

WILLIAM D. STILL, ARCHITECT
STILL ARCHITECTURE, LLC
5759 Chisholm Trail
Lilburn, GA 30047
(404) 395-5278 stillarch@comcast.net

REVIEW PLANS ONLY!
NOT FOR CONSTRUCTION

JOB NO.: SAHODGES
FILE: 202
DATE: 04-17-19
DRAWN BY: CHD05
DRAWN BY:

SHEET
REAR AND LEFT SIDE ELEVATIONS
AS-202

**NOTICE TO THE PUBLIC
CITY OF MONROE**

The City of Monroe has received a request for a Certificate of Appropriateness. A public hearing will be held on June 18, 2019 before the Planning & Zoning Commission, at 5:30 P. M. for 1190 West Spring Street.

The meeting will be held in City Hall Meeting Room, 215 North Broad Street. All those having an interest should be present.

Please run on the following date:

June 2, 2019