



CITY OF MERCER ISLAND

CITY COUNCIL MEETING

Tuesday, December 3, 2019 at 5:30 pm

CITY COUNCIL:

Mayor Debbie Bertlin, Deputy Mayor Salim Nice,
Councilmembers: Lisa Anderl, Bruce Bassett,
Wendy Weiker, David Wisenteiner, and Benson Wong

LOCATION & CONTACT:

Mercer Island City Hall - Council Chambers
9611 SE 36th Street | Mercer Island, WA 98040
Phone: 206.275.7793 | www.mercergov.org

In compliance with the Americans with Disabilities Act, those requiring accommodation for Council meetings should notify the City Clerk's Office at least 24 hours prior to the meeting at 206.275.7793.

CALL TO ORDER & ROLL CALL, 5:30 PM

AGENDA APPROVAL

EXECUTIVE SESSION

Executive Session for planning or adopting the strategy or position to be taken by the City Council during the course of any collective bargaining, professional negotiations, or grievance or mediation proceedings, or reviewing the proposals made in the negotiations or proceedings while in progress pursuant to RCW 42.30.140(4)(b) for approximately 30 minutes. No action is expected.

STUDY SESSION, 6:00 PM

1. AB 5639: Code of Ethics Revisions (2nd Reading)

Recommended Action: Review proposed revisions to the Code of Ethics and Code of Ethics Statement and provide feedback.

SPECIAL BUSINESS

Special Recognition for Fred Jarrett

CITY MANAGER REPORT

APPEARANCES

CONSENT CALENDAR

2. Approve **Accounts Payable Report for the period ending November 21, 2019** in the amount of \$1,944,450.84

3. Approve **Certification of Payroll dated November 22, 2019** in the amount of \$827,636.39

4. AB 5636: CPD Development and Construction Permit Fees Update

Recommended Action: Approve Resolution No. 1567 adopting new development and construction permit fees effective January 1, 2020.

5. AB 5635: Public Institution Code Amendment (2nd Reading & Adoption)

Recommended Action: Adopt Ordinance No. 19C-19 amending MICC 19.05.010 to repeal Subsection B; providing for severability and establishing an effective date.

6. AB 5641: Acceptance of MIYFS Foundation Funds for 2020 Youth and Family Services Staffing

Recommended Action: Accept a donation of \$54,624 from the Mercer Island Youth and Family Services Foundation to fund the half time reduction in the Geriatric Specialist position from January 1, 2020 through December 31, 2020.

REGULAR BUSINESS

7. AB 5637: Public Hearing: Interim Ordinance Design and Concealment Standards for Small Cell Facilities Deployment
Recommended Action: Conduct public hearing and consider public testimony and adopt Ordinance No. 19-22, extending the Interim Design and Concealment Standards for Small Cell Facilities deployment established under Ordinance No. 19C-02.
8. AB 5629: Aubrey Davis Park Master Plan Adoption
Recommended Action: (1) Approve Resolution No 1571 adopting the Aubrey Davis Park Master Plan as revised; and (2) Direct the City Manager and the Parks and Recreation Commission to develop a recommended scope of work for the \$500,000 Washington State Department of Commerce grant to be presented to the City Council for consideration and approval in Q1 2020.
9. AB 5631: 2019 Comprehensive Plan Amendments (2nd Reading & Adoption)
Recommended Action: Adopt Ordinance No. 19-23 amending the Mercer Island Comprehensive Plan Land Use, Capital Facilities, and Transportation Elements
10. AB 5630: 2019 Minor Code Amendments (1st Reading)
Recommended Action: Set Ordinance No. 19C-21 for second reading and adoption on the December 10, 2019 Consent Calendar.
11. AB 5642: Design Commission Vacancy Appointment
Recommended Action: Appoint the Mayor and Deputy Mayor's recommendation of Lara Sanderson to Position No. 2 on the Design Commission.

OTHER BUSINESS

12. Planning Schedule

13. Councilmember Reports

- SCA Legislative Agenda
- SCA Voting Delegate

14. Councilmember Absences

EXECUTIVE SESSION (if needed)

ADJOURNMENT



BUSINESS OF THE CITY COUNCIL CITY OF MERCER ISLAND

AB 5639
December 3, 2019
Study Session

AGENDA BILL INFORMATION

| | | |
|----------------------------|---|---|
| TITLE: | AB 5639: Code of Ethics Revisions (2 nd Reading) | <input checked="" type="checkbox"/> Discussion Only |
| RECOMMENDED ACTION: | Review proposed revisions to the Code of Ethics and provide feedback. | <input type="checkbox"/> Action Needed: <input type="checkbox"/> Motion <input type="checkbox"/> Ordinance <input type="checkbox"/> Resolution |

| | |
|-------------------------------|---|
| DEPARTMENT: | City Manager |
| STAFF: | Ali Spietz, Chief of Administration |
| COUNCIL LIAISON: | n/a |
| EXHIBITS: | 1. Proposed Ordinance No. 19C-20 2. Proposed Resolution No. 1572 |
| CITY COUNCIL PRIORITY: | n/a |

| | |
|-------------------------------|--------|
| AMOUNT OF EXPENDITURE | \$ n/a |
| AMOUNT BUDGETED | \$ n/a |
| APPROPRIATION REQUIRED | \$ n/a |

SUMMARY

At the November 19 City Council Meeting, staff presented revisions to the code of ethics for public officials (AB 5628) to clarify the process for responding to a complaint, removing references to State law, and adding guidance on prohibited conduct. Following Council questions and discussion, staff was directed to further revise the proposed amendments.

REVISIONS

Ordinance No. 19C-20 (Exhibit) has the originally proposed sections regarding the ethics officer, advisory opinions, complaints, investigations, hearings, and enforcement with minor edits as suggested by Councilmembers. It also has new revisions from the first reading detailed below:

- **Definitions (2.60.020):** Added definition of “relative” and removed “compensation,” “contract,” and “contracting party” as they are no longer needed. Also, the City Manager was removed as a “public official” as the position is an employee of the City and follows the employee code of ethics as well as the International City/County Management Association Code of Ethics.
- **Prohibited Conduct (2.60.030):** This section replaces the originally proposed sections of Acceptance of gifts, Interest in contracts prohibited, Incompatible service – confidential information, and Personal

or private interests. The new language is in addition to the code of ethics set forth in chapter 42.23 RCW, which establishes the minimum standards for officials. It is easier to understand and details:

- Conflicts of Interest
 - Misuse of Public Position or Resources
 - Representation of Third Parties
 - Gifts and Favors
 - Confidential Information
- **Advisory Opinions** (2.60.070): Language has been added to direct public officials to request an informal opinion from the City Attorney, who then may refer the public official to the ethics officer for an advisory opinion.
 - **Complaint Process** [2.60.080(A)]: Language regarding investigations by the ethics officer has been removed.
 - **Disposition** [2.60.080(D)(8)]: Language has been added as a possible City Council action to remove the Mayor or Deputy Mayor appointment if they are found to have violated the code of ethics.
 - **No Recovery of Fees or Costs** [2.60.080(G)]: A new section has been added to confirm that costs associated with a code of ethics complaint/violation are not recoverable from the City.

Revisions to the Code of Ethics Statement are included as Exhibit 2 and reflect the language replacement in 2.60.030 Prohibited Conduct.

COUNCIL REVIEW AND NEXT STEPS

At the Study Session on Tuesday night staff will review the new language as well as answer questions about the original revisions. Staff will collect additional revisions and prepare the ordinance and resolution for third reading and adoption on December 10.

RECOMMENDATION

Review proposed revisions to the Code of Ethics and Code of Ethics Statement and provide feedback.

**CITY OF MERCER ISLAND
ORDINANCE NO. 19C-20**

**AN ORDINANCE OF THE CITY OF MERCER ISLAND, WASHINGTON
AMENDING CHAPTER 2.60 OF THE MERCER ISLAND CITY CODE TO
REVISE THE CODE OF ETHICS FOR OFFICIALS; PROVIDING FOR
SEVERABILITY; AND ESTABLISHING AN EFFECTIVE DATE.**

WHEREAS, the residents and businesses of Mercer Island are entitled to have fair, ethical and accountable local government that has earned the public’s full confidence for integrity; and

WHEREAS, a Code of Ethics strengthens the quality of government through ethical principles that shall govern the conduct of the City’s elected and appointed officials; and

WHEREAS, a Code of Ethics for members of the City Council, the City’s boards and commissions, Council-appointed task groups or committees, and the City Manager was adopted in 2018 to promote public confidence in the integrity of local government and its fair operation; and

WHEREAS, the proposed amendments to the Code of Ethics detail prohibited conduct regarding conflicts of interest, appearance of conflict, misuse of public position or resources, representation of third parties, gifts and favors, and confidential information; and

WHEREAS, the proposed amendments also establish an Ethics Officer provide for annual review of the code of ethics, review of training materials provided for education regarding the code of ethics, and advisory opinions concerning the code of ethics; and

WHEREAS, the proposed amendments provide a better process for complaints, investigations, hearings and enforcement related to violations of the Code of Ethics.

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF MERCER ISLAND, WASHINGTON, DO ORDAIN AS FOLLOWS:

Section 1. **Chapter 2.60 MICC, Code of Ethics, Amended.** Chapter 2.60 MICC, Code of Ethics, is hereby amended as follows:

**CHAPTER 2.60
CODE OF ETHICS**

SECTION:

- 2.60.010 Policy
- 2.60.020 ~~Definitions~~ ~~Compliance with Laws~~
- 2.60.030 ~~Prohibited Conduct~~ ~~Code of Ethics Statement~~
- 2.60.040 ~~Code of Ethics Statement~~ ~~Complaints~~
- 2.60.050 Ethics Officer

2.60.060 Advisory Opinions

2.60.070 Complaints, Investigations, Hearings and Enforcement

2.60.080 Limitation Period

2.60.010 Policy

A. Purpose. The purpose of the city of Mercer Island code of ethics is to strengthen the quality of government through ethical principles which shall govern the conduct of members of the city council, the city’s boards and commissions, and council-appointed task groups or committees.

B. Intent. The citizens and businesses of Mercer Island are entitled to have fair, ethical and accountable local government that has earned the public’s full confidence. In keeping with the city of Mercer Island’s commitment to excellence, the effective functioning of democratic government therefore requires that:

1. Officials comply with the laws and policies affecting the operations of government;
2. Officials be independent, impartial and fair in their judgment and actions;
3. Public office be used for the public good, not for personal gain; and
4. Public deliberations and processes be conducted openly, unless legally confidential, in an atmosphere of respect and civility.

C. Liberal Construction. This code of ethics shall be liberally construed to effectuate its purpose and policy and to supplement existing laws that relate to the same subject.

~~C. “Official” as used in this chapter means all members of the city council, the city’s boards and commissions, other council-appointed task groups or committees, and the city manager.~~

2.60.020 Definitions

For the purpose of this Chapter:

OFFICIAL or PUBLIC OFFICIAL means all members of the city council, the city’s boards and commissions, and other council-appointed task groups or committees of the city of Mercer Island.

RELATIVE means spouse or domestic partner, child, stepchild, parent, stepparent, parent-in-law, grandparent, grandchild, sibling, aunt, uncle, niece, nephew, son- or daughter-in-law, brother- or sister-in-law.

~~2.60.020 Compliance with laws.~~

~~Officials shall comply with federal, state and city laws in the performance of their public duties. These laws include but are not limited to: the United States and Washington~~

~~State Constitutions; laws pertaining to conflicts of interest, election campaigns, financial disclosures and open processes of government; and city ordinances and policies, including those listed below, as now enacted or hereafter amended:~~

~~Ch. 9A.72 RCW Perjury and interference with official proceedings~~

~~RCW 35A.13.020 Election of councilmembers — Eligibility — Terms — Vacancies — Forfeiture of office — Council chair~~

~~RCW 35A.13.050 City manager — Qualifications~~

~~Ch. 40.14 RCW Preservation and destruction of public records~~

~~RCW 42.17A.555 Use of public office or agency facilities in campaigns — Prohibition — Exceptions~~

~~RCW 42.17A.565 Solicitation of contributions by public officials or employees~~

~~Ch. 42.23 RCW Code of ethics for municipal officers — Contract interests~~

~~Ch. 42.36 RCW Appearance of fairness doctrine — Limitations~~

~~Ch. 42.56 RCW Public Records Act~~

2.60.030 Prohibited conduct

In addition to the code of ethics set forth in chapter 42.23 RCW, which establishes the minimum standards for officials, officials shall be subject to the following:

A. Conflicts of Interest. In order to ensure their independence and impartiality on behalf of the common good, officials shall not participate in government decisions in which any of the following has a financial interest: (1) the official, (2) a relative, (3) an individual with whom the official resides, or (4) an entity that the official serves as an officer, director, trustee, partner or employee. Officials shall abstain from participating in deliberations and decision-making where conflicts exist. This section shall not apply (1) to decisions regarding taxes or fees, (2) if the financial interest is shared with more than ten percent of the city's population, or (3) if the financial interest exists solely because of ownership of less than one percent of the outstanding shares of a publicly traded corporation.

B. Misuse of Public Position or Resources. Except for infrequent use at little or no cost to the city, officials shall not use public resources that are not available to the public in general, such as city staff time, equipment, supplies or facilities, for other than a city purpose.

C. Representation of Third Parties. Except in the course of official duties, officials shall not appear on behalf of the financial interests of third parties before the bodies on which

the officials serve or in interaction with assigned staff. However, the members of the city council shall not appear on behalf of the financial interest of third parties before the council or any board, commission or proceeding of the city, or in interaction with staff.

D. Gifts and Favors.

1. Officials shall not take any special advantage of services or opportunities for personal gain, by virtue of their public office, which are not available to the public in general. They may not solicit or receive any thing of monetary value from any person or entity where the thing of monetary value has been solicited, or received or given or, to a reasonable person, would appear to have been solicited, received or given with intent to give or obtain special consideration or influence as to any action by the official in their official capacity; provided, that nothing shall prohibit campaign contributions which are solicited or received and reported in accordance with applicable law. They shall not accept or solicit any gifts, favors or promises of future benefits except as allowed by subsection (2).

2. The following items are presumed not to influence the vote, action, or judgment of the official, or be considered as part of a reward for action or inaction, and may be accepted:

- a. Unsolicited flowers, plants, and floral arrangements;
- b. Unsolicited advertising or promotional items of nominal value, such as pens and note pads;
- c. Unsolicited tokens or awards of appreciation in the form of a plaque, trophy, desk item, wall memento, or similar item;
- d. Unsolicited items received by a city official for the purpose of evaluation or review, if the official has no personal beneficial interest in the eventual use or acquisition of the item;
- e. Informational material, publications, or subscriptions related to the recipient's performance of official duties;
- f. Food and beverages consumed at hosted receptions where attendance is related to the city official's official duties;
- g. Admission to, and the cost of food and beverages consumed at, events sponsored by or in conjunction with a civic, charitable, governmental, or community organization;
- h. Unsolicited gifts from dignitaries from another state or a foreign country which are intended to be personal in nature; and
- i. Food and beverages on infrequent occasions in the ordinary course of meals where attendance by the official is related to the performance of official duties.

3. The presumption in subsection (2) of this section is rebuttable and may be overcome based on the circumstances surrounding the giving and acceptance of the item.

E. Confidential Information. Officials shall not disclose or use any confidential information gained by reason of their official position for other than a city purpose. "Confidential information" means (1) specific information, rather than generalized knowledge, that is not available to a person who files a public records request, and (2) information made confidential by law.

2.60.040030 Code of ethics statement.

A. The city council shall adopt a code of ethics statement regarding conduct for officials. The statement shall be set by resolution by the city council, from time to time, and shall be on file with the city clerk.

B. All officials, upon taking office or being appointed, shall sign the code of ethics statement acknowledging having read and understood the contents thereof.

2.60.040 Complaints.

~~A. Complaints regarding:~~

- ~~1. Any official, except for the mayor or the city manager, shall be brought to the mayor, city manager and city attorney.~~
- ~~2. The mayor shall be brought to the deputy mayor, city manager, and city attorney.~~
- ~~3. The city manager shall be brought to the mayor, deputy mayor, and city attorney.~~

~~B. The mayor or deputy mayor, city manager and the city attorney shall cause the complaint to be investigated and shall make a recommendation to the city council as to the appropriate resolution of the complaint based on the findings and conclusions of the investigation.~~

~~C. If a complaint is filed against the mayor, deputy mayor, or city manager, that individual will not direct activities of the investigation.~~

2.60.050 Ethics Officer.

A. The city council creates the position of ethics officer. The city manager will contract with one or more agencies to fill this position. The ethics officer shall be responsible for the prompt and fair enforcement of this code of ethics when necessary.

B. The ethics officer, in addition to other duties, may recommend changes or additions to this code of ethics to the city council. The ethics officer shall provide input into and review the training materials and program developed for this code of ethics if requested by the city manager or city council.

2.60.070 Advisory opinions.

A public official may request an informal opinion from the city attorney concerning the applicability of MICC 2.60.030, including chapter 42.23 RCW, to hypothetical circumstances and/or situations solely related to the official making the request. The city attorney may, instead of providing an informal opinion, refer the public official to the ethics officer for an advisory opinion as follows:

A. The ethics officer shall render written advisory opinions concerning the applicability of MICC 2.60.030, including chapter 42.23 RCW, to hypothetical circumstances and/or situations solely related to the official making the request. The ethics officer will not render opinions on matters that are the purview of other government agencies or officials, e.g., the public disclosure commission, the city’s public records officer, etc.

B. The ethics officer will endeavor to respond to requests for advisory opinions within forty-five days of submission of the request, or more rapidly if the requester expresses urgency in the request.

C. A public official’s conduct based in reasonable reliance on an advisory opinion rendered by the ethics officer to said public official shall not be found to violate this code of ethics to the extent that this code is enforced by the City as a civil matter, as long as all material facts have been fully, completely, accurately presented in a written request for an advisory opinion, the ethics officer issues an advisory opinion that the described conduct would not violate the code of ethics, and the public official’s conduct is consistent with the advisory opinion. The ethics officer reserves the right to reconsider the questions and issues raised in an advisory opinion and, where the public interest requires, rescind, modify, or terminate the opinion, but a modified or terminated advisory opinion will not form the basis of a retroactive enforcement action against the original requestor. Advisory opinions will contain severability clauses indicating that should portions of the opinion be found to be unenforceable or not within the ethics officer’s authority, the remainder of the opinion shall remain intact.

2.60.080 Complaints, investigations, hearings and enforcement.

A. Complaint Process.

1. Complaint Requirements—Service. Any person may submit a written complaint to the ethics officer alleging one or more violations of this code of ethics by a public official, by filing it with the city clerk. The complaint must set forth specific facts with enough precision and detail for the ethics officer to make a determination of sufficiency. It must be signed under penalty of perjury by the person(s) submitting it in a manner consistent with Chapter 9A.72 RCW.

2. Finding of Sufficiency. The ethics officer shall make a determination of sufficiency within thirty days of receipt of the written complaint. A complaint shall be sufficient if the allegations, if established, would violate MICC 2.60.030, including chapter 42.23 RCW. The ethics officer’s determination is final, and no reconsideration or appeal is available through the ethics officer or the City.

3. Dismissal. The complaint shall be dismissed if the ethics officer determines that the violation was not sufficient, or a violation occurred, but appropriate actions have been taken to fully address the allegedly unethical conduct.

4. Notice. Notice of action by the ethics officer shall be provided as follows:

a. Within seven days of the ethics officer rendering a finding of insufficiency or dismissal of a complaint, the city clerk shall send notice to the person who made the complaint and the public official complained against, of the ethics officer's determination. A finding of insufficiency or dismissal of a complaint by the ethics officer is final, and no reconsideration or appeal is available through the ethics officer or the City.

b. Within seven days of the ethics officer rendering a finding of sufficiency, the city clerk shall send notice to the person who made the complaint and the public official complained against, of the ethics officer's determination. A finding of sufficiency of a complaint by the ethics officer is final, and no reconsideration or appeal is available through the ethics officer or the City. Following the initial notice, the city clerk shall schedule and give notice of the hearing which will be held to determine if a violation has occurred. Notice shall be provided at least thirty days prior to the date set for the hearing. The public official complained against shall have the right to file a written answer to the charge and to appear at the hearing with or without legal counsel, submit testimony, be fully heard, and to examine and cross examine witnesses.

5. Stipulations. Prior to, and in-lieu-of the hearing, the ethics officer and the public official complained against may upon agreement jointly submit a recommended stipulation to the city council. The recommended stipulation will include the nature of the complaint, relevant facts, the reasons the ethics officer thinks a stipulation is appropriate, an admission of the violation by the public official complained against, a promise by the public official complained against not to repeat the violation, and if appropriate, a recommended remedy or penalty. The recommended stipulation shall be sent to the person who made the complaint and forwarded to the city council for action.

B. Conduct of Hearings.

1. All hearings on complaints found to be sufficient by the ethics officer shall be conducted by the hearing examiner. The hearing shall be informal, meaning that the hearing examiner shall not be bound by the strict rules of evidence prevailing in courts of law or equity. The hearing examiner may call witnesses on his or her own motion and compel the production of books, records, papers, or other evidence as needed. To that end, the hearing examiner shall issue subpoenas and subpoenas duces tecum. All testimony shall be under oath administered by the hearing examiner. The hearing examiner may adjourn the hearing from time to time to allow for the orderly presentation of evidence. The hearing examiner shall prepare an official record of the hearing, including all testimony, which shall be recorded by mechanical device, and exhibits; provided, that the hearing examiner shall not be required to transcribe such records unless presented with a request accompanied by payment of the cost of transcription.

2. Within thirty days after the conclusion of the hearing, the hearing examiner shall, based upon a preponderance of the evidence, issue a final decision with findings of fact, conclusions of law, and recommended disposition. A copy of the findings, conclusions, and recommended disposition shall be sent to the person who made the complaint and

to the public official complained against. Additional copies of the findings, conclusions, and recommendations shall be forwarded to the ethics officer and city council.

C. City Council Action. Final city council action to decide upon the recommended stipulation from the ethics officer or findings, conclusions, and recommendations from the hearing examiner shall be by majority vote in a public meeting. However, deliberations by the city council may be in executive session pursuant to RCW 42.30.110(1)(f). The member of the city council against whom the complaint was made will not attend or participate in any executive session and shall not vote on any matter involving themselves.

D. Disposition. If the hearing examiner finds that the person against whom the complaint was made has violated the code of ethics, or in response to the recommended stipulation from the ethics officer, the city council may take any of the following actions by a majority vote of the city council as appropriate. The action of the city council shall be final and not subject to further review or appeal except as may be otherwise provided by law or as provided below in subsection (E).

1. Dismissal. Dismissal of the complaint without penalties.

2. Referral. A complaint may be referred to another agency with jurisdiction over the violation, such as the Public Disclosure Commission. Final action on the complaint may be stayed pending resolution of the matter by the agency to which it was referred.

3. Admonition. An admonition shall be an oral non-public statement made by the mayor, or his/her designee, or if the complaint is against the mayor, the deputy mayor or his/her designee, to the public official.

4. Reprimand. A reprimand shall be administered to the public official by a letter of reprimand by the city council. The letter shall be prepared by the city council and shall be signed by the mayor or, if the complaint is against the mayor, the deputy mayor.

5. Censure. A letter of censure shall be a letter read to the public official in public. The letter shall be prepared by the city council and shall be signed by the mayor, or if the complaint is against the mayor, the deputy mayor. The public official shall appear at a city council meeting at a time and place directed by the city council to receive the letter of censure. Notice shall be given at least twenty calendar days before the scheduled appearance at which time a copy of the proposed letter of censure shall be provided to the public official. The letter of censure shall be read publicly, and the public official shall not make any statement in support of, or in opposition thereto, or in mitigation thereof. The letter of censure shall be read at the time it is scheduled whether or not the public official appears as required.

6. Removal—Member of Board or Commission or Other Appointed Task Group or Committee. If the public official against whom the complaint was made is currently a member of a city board or commission or other task group or committee, appointed by

the city council, the city council may, in addition to other possible penalties set forth in this section, and notwithstanding any other provision of the Mercer Island City Code, by a majority vote remove the public official from such board or commission effective immediately.

7. Civil Penalties. In addition to taking any actions above, the city council may also assess a civil penalty of up to one thousand dollars. Any monetary penalty assessed civilly shall be placed in the city's general fund.

8. Other Penalties. In addition to taking any actions above, the city council may also impose restrictions or loss of a committee assignments, or loss of appointment as a representative of the city for any regional or multijurisdictional body, or membership on any board or commission which requires an appointment or confirmation of an appointment by the mayor and deputy mayor or city council. If the public official against whom the complaint was made serves as mayor or deputy mayor, the city council may also remove said appointment.

E. Review of Civil Penalty Amount. If the city council orders an official to pay a civil penalty, the official may seek a reconsideration from the city council of the amount accessed within thirty days of the city council's order.

F. Protection Against Retaliation. Neither the city nor any official may take or threaten to take, directly or indirectly, official or personal action, including but not limited to discharge, discipline, personal attack, harassment, intimidation, or change in job, salary, or responsibilities, against any person because that person files a complaint with the ethics officer.

G. No Recovery of Fees or Costs. No attorney's fees or other costs related to matters covered by this chapter incurred by any public official or complainants shall be recoverable from the City.

2.60.090 Limitation period.

Complaints based on this code of ethics must be submitted within three years from the date of violation.

Section 2. Severability. If any section, sentence, clause or phrase of this ordinance is held to be invalid or unconstitutional by a court of competent jurisdiction, such invalidity or unconstitutionality does not affect the validity of any other section, sentence, clause or phrase of this ordinance.

Section 3. Corrections. The City Clerk and the codifiers of this ordinance are authorized to make necessary clerical corrections to this ordinance including, but not limited to, the correction of scrivener's/clerical errors, references, ordinance numbering, section/subsection numbers and any references thereto.

Section 4. Effective Date. This ordinance shall take effect and be in force 30 days after its passage and publication.

PASSED by the City Council of the City of Mercer Island, Washington at its regular meeting on the 10th day of December 2019 and signed in authentication of its passage.

CITY OF MERCER ISLAND

Debbie Bertlin, Mayor

Approved as to Form:

ATTEST:

Bio Park, Interim City Attorney

Deborah A. Estrada, City Clerk

Date of Publication: _____

**CITY OF MERCER ISLAND
RESOLUTION NO. 1572**

**A RESOLUTION OF THE CITY OF MERCER ISLAND, WASHINGTON TO
REVISE THE CODE OF ETHICS STATEMENT FOR MEMBERS OF THE CITY
COUNCIL, THE CITY’S BOARDS AND COMMISSIONS AND THE CITY
MANAGER**

WHEREAS, the residents and businesses of Mercer Island are entitled to have fair, ethical and accountable local government that has earned the public’s full confidence for integrity; and

WHEREAS, a Code of Ethics for members of the City Council, the City’s boards and commissions, Council-appointed task groups or committees, and the City Manager promotes public confidence in the integrity of local government and fair operation; and

WHEREAS, the Code of Ethics adopted by the City Council is codified in chapter 2.60 MICC; and

WHEREAS, MICC 2.60.030 requires the adoption by resolution of a Code of Ethics Statement, which all officials, as the term is defined in MICC 2.60.010(C), shall comply with and sign to acknowledge having read and understood its contents; and

WHEREAS, the Code of Ethics Statement requires revisions pursuant to amendments to chapter 2.60 MICC as adopted in Ordinance No. 19C-20;

NOW THEREFORE, BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF MERCER ISLAND, WASHINGTON AS FOLLOWS:

The City Council hereby adopts a revised Code of Ethics Statement in substantially the form as the attached Exhibit A, which all officials, as defined in MICC 2.60.010(C), shall comply with and sign to acknowledge having read and understood its contents upon taking office pursuant to MICC 2.60.030.

PASSED BY THE CITY COUNCIL OF THE CITY OF MERCER ISLAND, WASHINGTON, AT ITS REGULAR MEETING ON THE 3RD DAY OF DECEMBER 2019.

CITY OF MERCER ISLAND

Debbie Bertlin, Mayor

ATTEST:

Deborah A. Estrada, City Clerk



Code of Ethics

The purpose of the City of Mercer Island Code of Ethics is to strengthen the quality of government through ethical principles which shall govern the conduct of members of the City Council, the City's boards and commissions, Council-appointed task groups and committees, and the City Manager ("officials") who shall:

Be dedicated to the concepts of effective and democratic local government.

***Democratic Leadership:** Officials shall honor and respect the principles and spirit of representative democracy and set a positive example of good citizenship by scrupulously observing the letter and spirit of laws, rules and regulations.*

Affirm the dignity and worth of the services rendered by government and maintain a deep sense of social responsibility as a trusted public servant.

Be dedicated to the highest ideals of honor and integrity in all public and personal relationships.

***Public Confidence:** Officials shall conduct themselves to maintain public confidence in city government and in the performance of the public trust.*

***Impression of Influence:** Officials shall conduct their official and personal affairs in such a manner as to give the clear impression that they cannot be improperly influenced in the performance of their official duties.*

Recognize that the chief function of local government always is to serve the best interests of all the people.

***Public Interest:** Officials shall treat their position or office as a public trust, only using the power and resources of public office to advance public interests, and not to attain personal benefit or pursue any other private interest incompatible with the public good.*

Keep the community informed on municipal affairs; encourage communication between the citizens and all municipal officers; emphasize respectful and courteous service to the public; and seek to improve the quality and image of public service.

***Accountability:** Officials shall ensure that government is conducted openly, efficiently, equitably and honorably in a manner that permits the citizenry to make informed judgments and hold city Officials accountable.*

Respectability: *Officials shall safeguard public confidence in the integrity of city government by being honest, fair, caring and respectful and by avoiding conduct creating the appearance of impropriety.*

Seek no favor; believe that personal benefit or profit secured by confidential information or by misuse of public time is dishonest.

Conflicts of Interest: *Officials shall abstain from participating in deliberations and decision-making where conflicts exist.*

Misuse of Public Position or Resources: *Officials shall not use public resources that are not available to the public in general, such as city staff time, equipment, supplies or facilities, for other than a city purpose.*

Gifts and Favors: *Officials shall not take any special advantage of services or opportunities for personal gain, by virtue of their public office, which are not available to the public in general.*

Confidential Information: *Officials shall not disclose or use any confidential information gained by reason of their official position for other than a city purpose.*

Business Interests: *Officials shall have no beneficial interest in any contract which may be made by, through or under his or her supervision, or for the benefit of his or her position or office, or accept directly or indirectly, any compensation, gratuity or reward in connection with such contract unless allowed under State law.*

Private Employment: *Officials shall not engage in, solicit, negotiate for, or promise to accept private employment or render services for private interests or conduct a private business when such employment, service or business creates a conflict with or impairs the proper discharge of their official duties.*

Confidential Information: *Officials shall not disclose to others, or use to further their personal interest, confidential information acquired by them during their official duties.*

Gifts: *Officials shall not directly or indirectly solicit, accept, or receive any gift whether it be money, services, loan, travel, entertainment, hospitality, promise, or any other form—under the following circumstances: (a) it could be reasonably inferred or expected that the gift was intended to influence the performance of official duties; or (b) the gift was intended to serve as a reward for any official action on the official's part.*

Investments in Conflict with Official Duties: *Officials shall not invest or hold any investment, directly or indirectly, in any financial business, commercial or other private transaction that creates a conflict with their official duties. Should a conflict arise, Officials shall take proactive steps to disclose all relevant information and refrain from participating in any city business or activity related to the conflict.*

Personal Relationships: *Officials shall disclose personal relationships in any instance where there could be the appearance of a conflict of interest.*

***Business Relationships:** Officials shall not use city time, equipment, or facilities for marketing or soliciting for private business activities.*

Conduct business of the City in a manner which is not only fair in fact, but also in appearance.

Not knowingly violate any Washington statutes, City ordinance, or regulation while performing their duties.

<name>
<title>

Code of Ethics Revisions (2nd Reading)

AB 5639 | December 3, 2019



Code of Ethics Revisions

Changes from 1st Reading to 2nd Reading:

- Removed sections:
 - *Acceptance of gifts*
 - *Interest in contracts prohibited*
 - *Incompatible service – confidential information*
 - *Personal or private interests*
- Added section regarding prohibited conduct for public officials



Code of Ethics Revisions

Prohibited Conduct

- Conflicts of interest
- Misuse of public position or resources
- Representation of third parties
- Gifts and favors
- Confidential information



Code of Ethics Revisions

City Council Action - Disposition

1. Dismissal
2. Refer to another agency
3. Admonition
4. Reprimand
5. Censure
6. Removal (for Board, Commission, Committee Member)
7. Civil Penalties
8. Void Contract
9. Other Penalties
(includes removal as Mayor or Deputy Mayor)



Code of Ethics Revisions

No Recovery of Fees or Costs

- No attorney's fees or other costs related to matters covered by this chapter incurred by any public official or complainants shall be recoverable from the City.

Code of Ethics Statement

- Resolution No. 1572 to adopt revised Code of Ethics Statement to include "Prohibited Conduct"



Matrix of Proposed Changes

Council Questions/Comments/Proposed Changes to Draft Ordinance No. 19C-20

| # | Section | Language in Ordinance | CMember | Question/Comment/Proposal | Staff Response |
|---|--------------|--|---------|---|---|
| 1 | 2.60.010 (B) | Intent. The citizens and businesses of Mercer Island are entitled to have fair, ethical and accountable local government that has earned the public's full confidence. In keeping with the city of Mercer Island's commitment to excellence, the effective functioning of democratic government therefore requires that... | Bassett | Intent. The citizens and businesses of Mercer Island are entitled to have fair, ethical and accountable local government that has earned the public's full confidence. In keeping with the city of Mercer Island's commitment to excellence, the effective functioning of democratic government therefore requires that... | |
| 2 | 2.60.020 | OFFICIAL or PUBLIC OFFICIAL means all members of the city council, the city's boards and commissions, and other council-appointed task groups or committees of the city of Mercer Island. | Wong | I recommend either using "Official" or "Public Official" and not both in this chapter for consistency. | |
| 3 | 2.60.020 | RELATIVE means spouse or domestic partner, child, stepchild, parent, stepparent, parent-in-law, grandparent, grandchild, sibling, aunt, uncle, niece, nephew, son- or daughter-in-law, brother- or sister-in-law. | Bassett | RELATIVE means spouse or domestic partner, child, stepchild, parent, stepparent, parent-in-law, grandparent, grandchild, sibling, aunt, uncle, niece, nephew, son- or daughter-in-law, brother- or sister-in-law. | |
| 4 | 2.60.030(A) | Conflicts of Interest. In order to ensure their independence and impartiality on behalf of the common good, officials shall not participate in government decisions in which any of the following has a financial interest: (1) the official, (2) a relative, (3) an individual with whom the official resides, or (4) an entity that the official serves as an officer, director, trustee, partner or employee. Officials shall abstain from participating in deliberations and decision-making where conflicts exist. This section shall not apply (1) to decisions regarding taxes or fees, (2) if the financial interest is shared with more than ten percent of the city's population, or (3) if the financial interest exists solely because of ownership of less than one percent of the outstanding shares of a publicly traded corporation. | Wong | Do we need to define what is a "financial interest" that creates a conflict of interest? Should there be a <i>de minimis</i> rule? For instance, if a councilmember is only an employee at Microsoft, does that mean the councilmember cannot vote on a city contract that might benefit Microsoft? While there is a carve out if you own less than 1% of shares of a publicly-traded company, what if a councilmember owns a small percent (1% or less) in a privately-held company and is not involved in the decision-making process of that privately-held company and/or in the services to be provided by that privately-held company to the city? | As currently drafted, it means any financial interest, except as exempted. That is correct, the councilmember cannot participate. This is consistent with state law RCW 42.23.040. In this scenario, RCW 42.23.040 would apply to determine whether the public official has a beneficial interest in the contract. If so, the contract would be void. However, even if the interest is deemed to be remote, the public official could not participate or attempt to influence those who do under RCW 42.23.040. |
| 5 | 2.60.030(A) | Conflicts of Interest. In order to ensure their independence and impartiality on behalf of the common good, officials shall not participate in government decisions in which any of the following has a financial interest: (1) the official, (2) an relative, (3) an individual with whom the official resides, or (4) an entity that the official serves as an officer, director, trustee, partner or employee. Officials shall abstain from participating in deliberations and decision-making where conflicts exist. This section shall not apply (1) to decisions regarding taxes or fees, (2) if the financial interest is shared with more than ten percent of the city's population, or (3) if the financial interest exists solely because of ownership of less than one percent of the outstanding shares of a publicly traded corporation. | Bassett | Conflicts of Interest. In order to ensure their independence and impartiality on behalf of the common good, officials shall not participate in government decisions in which any of the following has a financial interest: (1) the official, (2) an <u>immediate</u> relative, (3) an individual with whom the official resides, or (4) an entity that the official serves as an officer, director, trustee, partner or <u>employee owner</u> . <u>For this purpose, an "owner" is an individual who owns more than 1% of the entity.</u> Officials shall abstain from participating in deliberations and decision-making where conflicts <u>of interest</u> exist. <u>Financial interests broadly held by Mercer Island residents shall not be deemed to be conflicts.</u> <u>Financial interest. A financial interest shall be deemed to exist when</u> | |



Next Steps

1. Provide feedback on proposed Ordinance No. 19C-20 and Resolution No. 1572
2. Bring back for adoption on the Consent Calendar at the December 10 Council meeting
3. Staff will contract with
 - Seattle Ethics and Elections Commissions for Ethics Officer
 - Local lawyer for Hearing Examiner

Code of Ethics Revisions

Ethics Officer

- Responsible for prompt and fair enforcement of the code of ethics
- Seattle Ethics and Elections Commission
- After complaint is received, EO makes determination of sufficiency within 30 days
- If the complaint is insufficient or has already been addressed, it will be dismissed



Code of Ethics Revisions

Ethics Officer (continued)

- If the complaint is sufficient, and EO determines material violation of MICC 2.60.030-.060 occurred, a hearing will be held to determine if a violation has occurred
- Notice of hearing at least 30 days prior
- Public official complained against can file written answer to the charge and to appear at the hearing with or without legal counsel, submit testimony, be fully heard, and to examine and cross examine witnesses



Code of Ethics Revisions

Hearings

- Hearing Examiner will conduct hearings on complaints found to be sufficient by the Ethics Officer
- HE will prepare an official record of the hearing, including all testimony, which will also be recorded
- Within 30 days of the hearing, the HE will issue a final decision with findings of fact, conclusions of law, and recommended disposition



Code of Ethics Revisions

City Council Action

- Deliberate the stipulations and recommendations from the Ethics Officer and findings, conclusions, and recommendations from the Hearing Examiner in Executive Session [RCW 42.30.110(1)(f)]
- Public Official complained against cannot participate in Executive Session
- City Council's final action shall be by majority vote in a public meeting



CERTIFICATION OF CLAIMS

I, the undersigned, do hereby certify under penalty of perjury that the materials have been furnished, the services rendered, or the labor performed as described herein, that any advance payment is due and payable pursuant to a contract or is available as an option for full or partial fulfillment of a contractual obligation, and that the claim is a just, due and unpaid obligation against the City of Mercer Island, and that I am authorized to authenticate and certify to said claim.



Finance Director

I, the undersigned, do hereby certify that the City Council has reviewed the documentation supporting claims paid and approved all checks or warrants issued in payment of claims.

Mayor

Date

| <u>Report</u> | <u>Warrants</u> | <u>Date</u> | <u>Amount</u> |
|----------------|-----------------|-------------|---|
| Check Register | 200818 -201197 | 11/21/2019 | \$ 1,944,450.84 \$ 1,944,450.84 |

Accounts Payable Report by GL Key

Item 2.

| PO # | Check # | Vendor: | Transaction Description | Check Amount |
|---|----------|-------------------------------|--------------------------------|--------------|
| <i>Org Key: 001000 - General Fund-Admin Key</i> | | | | |
| P0105700 | 00200939 | WA ST TREASURER'S OFFICE | OCT19 REMIT MI COURT TRANSMITT | 4,498.51 |
| P0105699 | 00200939 | WA ST TREASURER'S OFFICE | OCT19 REMIT NC COURT TRANSMITT | 2,585.53 |
| P0105700 | 00200939 | WA ST TREASURER'S OFFICE | OCT19 REMIT MI COURT TRANSMITT | 2,294.44 |
| P0105700 | 00200939 | WA ST TREASURER'S OFFICE | OCT19 REMIT MI COURT TRANSMITT | 1,939.06 |
| P0105699 | 00200939 | WA ST TREASURER'S OFFICE | OCT19 REMIT NC COURT TRANSMITT | 1,592.18 |
| P0105699 | 00200939 | WA ST TREASURER'S OFFICE | OCT19 REMIT NC COURT TRANSMITT | 1,156.04 |
| P0105589 | 00200900 | NEWPORT ACADEMY | Rental FA-3590 cancelled. Retu | 752.00 |
| P0105700 | 00200939 | WA ST TREASURER'S OFFICE | OCT19 REMIT MI COURT TRANSMITT | 656.18 |
| P0105699 | 00200939 | WA ST TREASURER'S OFFICE | OCT19 REMIT NC COURT TRANSMITT | 501.39 |
| | 00200864 | ICF WASHINGTON STATE | REPLACE WARRANT 198806 | 500.00 |
| P0105839 | 00201093 | MI SCHOOLS BAND BOOSTER | Rental FA-2793 completed. Retu | 500.00 |
| P0105584 | 00200889 | MI FOOTBALL BOOSTER CLUB | Rental FA-1962 completed. Retu | 425.00 |
| P0105700 | 00200939 | WA ST TREASURER'S OFFICE | OCT19 REMIT MI COURT TRANSMITT | 379.99 |
| P0105700 | 00200939 | WA ST TREASURER'S OFFICE | OCT19 REMIT MI COURT TRANSMITT | 379.97 |
| P0105585 | 00200850 | FREE WHEELCHAIR MISSION | Rental FA-2251 completed. Retu | 350.00 |
| P0105699 | 00200939 | WA ST TREASURER'S OFFICE | OCT19 REMIT NC COURT TRANSMITT | 343.02 |
| P0105700 | 00200939 | WA ST TREASURER'S OFFICE | OCT19 REMIT MI COURT TRANSMITT | 327.52 |
| P0105700 | 00200939 | WA ST TREASURER'S OFFICE | OCT19 REMIT MI COURT TRANSMITT | 262.25 |
| P0105699 | 00200939 | WA ST TREASURER'S OFFICE | OCT19 REMIT NC COURT TRANSMITT | 255.23 |
| P0105587 | 00200913 | PSIS | Rental FA-3110 completed. Retu | 250.00 |
| P0105700 | 00200939 | WA ST TREASURER'S OFFICE | OCT19 REMIT MI COURT TRANSMITT | 217.96 |
| P0105699 | 00200939 | WA ST TREASURER'S OFFICE | OCT19 REMIT NC COURT TRANSMITT | 201.61 |
| P0105700 | 00200939 | WA ST TREASURER'S OFFICE | OCT19 REMIT MI COURT TRANSMITT | 124.94 |
| P0105700 | 00200939 | WA ST TREASURER'S OFFICE | OCT19 REMIT MI COURT TRANSMITT | 104.84 |
| P0105699 | 00200939 | WA ST TREASURER'S OFFICE | OCT19 REMIT NC COURT TRANSMITT | 99.99 |
| P0105699 | 00200939 | WA ST TREASURER'S OFFICE | OCT19 REMIT NC COURT TRANSMITT | 99.99 |
| P0105630 | 00200870 | JOHNSON, JASON | Returning deposit. FA-3737 | 75.00 |
| P0105629 | 00200893 | MOHAN SKIING AND BOARDING | Returning deposit. Rental FA-3 | 75.00 |
| P0105840 | 00201147 | SEATTLE CHURCH OF CHRIST, THE | Rental FA-3708 completed. Retu | 75.00 |
| P0105715 | 00201043 | HENDRICKS, YVONNE | returning account credit | 64.00 |
| | 00200864 | ICF WASHINGTON STATE | REPLACE WARRANT 198902 | 50.00 |
| P0105838 | 00201112 | ORANGETHEORY FITNESS WA | \$25 cancellation fee for Nov | 50.00 |
| P0105700 | 00200939 | WA ST TREASURER'S OFFICE | OCT19 REMIT MI COURT TRANSMITT | 46.17 |
| P0105700 | 00200939 | WA ST TREASURER'S OFFICE | OCT19 REMIT MI COURT TRANSMITT | 22.00 |
| P0105588 | 00200948 | YU, BENJAMIN | Returning remaining credit fro | 17.25 |
| P0105699 | 00200939 | WA ST TREASURER'S OFFICE | OCT19 REMIT NC COURT TRANSMITT | 13.77 |
| P0105699 | 00200939 | WA ST TREASURER'S OFFICE | OCT19 REMIT NC COURT TRANSMITT | 13.13 |
| P0105699 | 00200939 | WA ST TREASURER'S OFFICE | OCT19 REMIT NC COURT TRANSMITT | 11.01 |
| P0105699 | 00200939 | WA ST TREASURER'S OFFICE | OCT19 REMIT NC COURT TRANSMITT | 2.31 |
| <i>Org Key: 402000 - Water Fund-Admin Key</i> | | | | |
| P0105698 | 00201024 | FERGUSON ENTERPRISES LLC | INVENTORY PURCHASES | 5,157.11 |
| | 00201124 | PEHA, ROBERT & DONNA | DEPOSIT REFUND | 4,500.00 |
| P0105691 | 00201148 | SEATTLE PUBLIC UTILITIES | OCTOBER 2019 SPU CHARGE FOR RE | 3,744.00 |
| P0105598 | 00200878 | LAKESIDE INDUSTRIES | INVENTORY PURCHASES | 1,745.70 |
| P0105627 | 00200940 | WALTER E NELSON CO | INVENTORY PURCHASES | 1,450.64 |
| P0105881 | 00201183 | WALTER E NELSON CO | INVENTORY PURCHASES | 1,203.42 |
| P0105619 | 00200933 | SUPPLY SOURCE INC,THE | INVENTORY PURCHASES | 1,006.74 |
| | 00201059 | KAUFMAN, RON | OVERPAYMENT REFUND | 885.46 |

Accounts Payable Report by GL Key

Item 2.

| PO # | Check # | Vendor: | Transaction Description | Check Amount |
|--|----------|------------------------------|---------------------------------|--------------|
| P0105403 | 00201192 | WILSON BOHANNAN PADLOCK CO | INVENTORY PURCHASES | 739.07 |
| P0105818 | 00201032 | GRAINGER | INVENTORY PURCHASES | 515.60 |
| P0105681 | 00200991 | CHOW, PHILLIP | REFUND DEPOSIT FOR WATER SERVI | 468.12 |
| P0105775 | 00201126 | PLATT ELECTRIC | INVENTORY PURCHASES | 467.39 |
| P0105845 | 00201094 | MILLAD VI LLC | REFUND DEPOSIT FROM WORK ORDER | 398.61 |
| P0105696 | 00200989 | CESSCO INC | INVENTORY PURCHASES | 357.92 |
| P0105787 | 00201022 | EXCEL SUPPLY COMPANY | INVENTORY PURCHASES | 356.64 |
| | 00201188 | WELLS, ROBERT | OVERPAYMENT REFUND | 323.44 |
| P0105697 | 00201175 | USABlueBook | INVENTORY PURCHASES | 310.49 |
| P0105557 | 00200843 | DUNN LUMBER COMPANY | INVENTORY PURCHASES | 275.66 |
| P0105537 | 00200837 | CROP, EDITH L | 1901-193 Refund deposit for wa | 244.77 |
| P0105684 | 00201032 | GRAINGER | INVENTORY PURCHASES | 254.50 |
| P0105660 | 00200861 | HOME DEPOT CREDIT SERVICE | INVENTORY PURCHASES | 207.17 |
| P0105617 | 00200851 | GRAINGER | INVENTORY PURCHASES | 192.09 |
| | 00201142 | SALERNO, KRISTOPHER | OVERPAYMENT REFUND | 149.03 |
| P0105566 | 00200851 | GRAINGER | INVENTORY PURCHASES | 157.67 |
| P0105546 | 00200819 | ALPINE PRODUCTS INC | INVENTORY PURCHASES | 144.21 |
| P0105628 | 00200834 | COMMERCIAL LANDSC SUPPLY INC | INVENTORY PURCHASES | 146.38 |
| P0105511 | 00200926 | SAFETY FLAG CO OF AMERICA | INVENTORY PURCHASES | 144.00 |
| P0105396 | 00201032 | GRAINGER | INVENTORY PURCHASES | 111.93 |
| P0105657 | 00200846 | EXCEL SUPPLY COMPANY | INVENTORY PURCHASES | 61.12 |
| P0105774 | 00201032 | GRAINGER | INVENTORY PURCHASES | 59.49 |
| P0105642 | 00200851 | GRAINGER | INVENTORY PURCHASES | 26.69 |
| P0105833 | 00201032 | GRAINGER | INVENTORY PURCHASES | 21.79 |
| | 00201047 | HOWISON, DEREK & KELLI | OVERPAYMENT REFUND | 1.00 |
| <i>Org Key: 814006 - Regular Checks Re-Issued</i> | | | | |
| P0105564 | 00200917 | RAUL, ALONSO | RE-ISSUE STALE DATED PAYROLL C | 450.27 |
| <i>Org Key: 814072 - United Way</i> | | | | |
| | 00201172 | UNITED WAY OF KING CO | PAYROLL EARLY WARRANTS | 80.00 |
| <i>Org Key: 814074 - Garnishments</i> | | | | |
| | 00201171 | UNITED STATES TREASURY | PAYROLL EARLY WARRANTS | 780.29 |
| <i>Org Key: 814075 - Mercer Island Emp Association</i> | | | | |
| | 00201085 | MI EMPLOYEES ASSOC | PAYROLL EARLY WARRANTS | 300.00 |
| <i>Org Key: 814076 - City & Counties Local 21M</i> | | | | |
| | 00201194 | WSCCCE AFSCME AFL-CIO | PAYROLL EARLY WARRANTS | 2,612.89 |
| <i>Org Key: 814077 - Police Association</i> | | | | |
| | 00201127 | POLICE ASSOCIATION | PAYROLL EARLY WARRANTS | 2,507.04 |
| <i>Org Key: 814085 - GET Program Deductions</i> | | | | |
| | 00201029 | GET Washington | PAYROLL EARLY WARRANTS | 250.00 |
| <i>Org Key: CA1100 - Administration (CA)</i> | | | | |
| P0105904 | 00201057 | K&L GATES LLP | Professional Services - Invoice | 4,650.08 |
| P0105608 | 00200903 | OGDEN MURPHY WALLACE PLLC | Professional Services - Invoice | 3,079.50 |
| P0105573 | 00200884 | MARTEN LAW | Professional Services - Invoice | 1,076.48 |
| P0105609 | 00200935 | THOMSON REUTERS - WEST | Invoice No. 841120761 - Librar | 870.10 |
| P0105782 | 00200983 | CAPITOL PACIFIC REPORTING | Transcript of Hearing - Owners | 641.25 |

Accounts Payable Report by GL Key

Item 2.

| PO # | Check # | Vendor: | Transaction Description | Check Amount |
|--|----------|----------------------------|---------------------------------|--------------|
| P0105785 | 00201135 | RELX INC DBA LEXISNEXIS | Library Subscriptions - Invoice | 348.70 |
| P0105903 | 00201082 | MARTEN LAW | Professional Services - Invoice | 99.77 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 50.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 4.27 |
| <i>Org Key: CA1200 - Prosecution & Criminal Mngmnt</i> | | | | |
| P0105784 | 00201095 | MOBERLY AND ROBERTS | Professional Services - Invoice | 6,800.00 |
| P0105786 | 00201045 | HONEYWELL, MATTHEW V | Professional Services - Invoice | 1,300.00 |
| P0105786 | 00201045 | HONEYWELL, MATTHEW V | Professional Services - Invoice | 1,300.00 |
| P0105905 | 00201045 | HONEYWELL, MATTHEW V | Professional Services - Invoice | 1,000.00 |
| P0105574 | 00200862 | HONEYWELL, MATTHEW V | Professional Services - Public | 700.00 |
| P0105610 | 00200862 | HONEYWELL, MATTHEW V | Professional Services - Invoice | 600.00 |
| P0105907 | 00201045 | HONEYWELL, MATTHEW V | Professional Services - Invoice | 600.00 |
| P0105574 | 00200862 | HONEYWELL, MATTHEW V | Prof. Services - Invoice No. 1 | 110.00 |
| | 00200911 | PRITCHARD, JOHN K | REPLACE WARRANT #200681 | 36.80 |
| <i>Org Key: CM1100 - Administration (CM)</i> | | | | |
| | 00201174 | US BANK CORP PAYMENT SYS | ICMA Annual Membership: J. Bon | 700.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | ICMA Annual Membership - J. Bo | 700.00 |
| P0105745 | 00200937 | VERIZON WIRELESS | CM Phone & Hotspot | 98.32 |
| P0105816 | 00201179 | VERIZON WIRELESS | P&R Wireless cell phone bill | 69.95 |
| P0105826 | 00201083 | MERCER ISLAND REPORTER | Yearly Subscription | 45.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | Supplies | 38.16 |
| | 00201174 | US BANK CORP PAYMENT SYS | Supplies | 27.45 |
| | 00201000 | COMPLETE OFFICE | OFFICE SUPPLIES OCT2019 | 20.53 |
| <i>Org Key: CM1200 - City Clerk</i> | | | | |
| P0105897 | 00200995 | CODE PUBLISHING CO | City Code Cumulative Print | 900.90 |
| P0105779 | 00201049 | IIMC | IIMC Annual Dues - D. Estrada | 210.00 |
| P0105825 | 00201156 | SOUND PUBLISHING INC | Ntc. Ord. 19C-13 2438577 | 62.48 |
| P0105825 | 00201156 | SOUND PUBLISHING INC | Ntc. Council Mtg Date Change 2 | 49.51 |
| P0105825 | 00201156 | SOUND PUBLISHING INC | Ntc. Special Meeting 2437816 | 45.89 |
| P0105825 | 00201156 | SOUND PUBLISHING INC | Ntc. Joint Council-MISD Mtg 24 | 43.81 |
| <i>Org Key: CM1300 - Sustainability</i> | | | | |
| P0105780 | 00200875 | KING COUNTY-DNRP | 2019 K4C Annual Contribution | 1,200.00 |
| P0105740 | 00200987 | CEDAR GROVE COMPOSTING INC | Organic Waste Service 10/2019 | 57.40 |
| P0105740 | 00200987 | CEDAR GROVE COMPOSTING INC | Organic Waste Service Luther 1 | 28.70 |
| <i>Org Key: CM1400 - Communications</i> | | | | |
| | 00201174 | US BANK CORP PAYMENT SYS | Subscription | 422.40 |
| | 00201174 | US BANK CORP PAYMENT SYS | Sparks 2019 Conference Registr | 175.00 |
| <i>Org Key: CO6100 - City Council</i> | | | | |
| P0105743 | 00200839 | DANIEL, KAMARIA | MITV 10/15 City Council Mtg | 360.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | 10/7 Council Meeting Meal | 358.82 |
| | 00201174 | US BANK CORP PAYMENT SYS | 11/4 Council Meeting Meal | 349.56 |
| P0105743 | 00200839 | DANIEL, KAMARIA | MITV 10/10 MISD Mtg | 300.00 |
| P0105743 | 00200839 | DANIEL, KAMARIA | MITV 10/24 MISD Mtg | 300.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | 10/15 Council Meeting Meal | 255.85 |
| | 00201174 | US BANK CORP PAYMENT SYS | Sister City Reception | 176.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | Council Award Key to the City | 93.25 |

Accounts Payable Report by GL Key

Item 2.

| PO # | Check # | Vendor: | Transaction Description | Check Amount |
|---|----------|--------------------------------|--------------------------------|--------------|
| | 00201174 | US BANK CORP PAYMENT SYS | Council Meeting Supplies/Meals | 63.46 |
| | 00201174 | US BANK CORP PAYMENT SYS | SCA Dinner Registration - W. W | 45.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | SCA Dinner Registration - B. B | 45.00 |
| P0105743 | 00200839 | DANIEL, KAMARIA | Transportation Fee | 40.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | 10/15 Council Meeting Meal | 14.04 |
| | 00201174 | US BANK CORP PAYMENT SYS | 11/4 Council Meeting Meal | 6.04 |
| Org Key: CR1100 - Human Resources | | | | |
| P0105811 | 00201041 | HEALTHFORCE PARTNERS INC | Medicals - Officer Erickson, | 1,665.00 |
| P0105843 | 00201131 | PUBLIC SAFETY SELECTION PC | Pre-hire psychological testing | 400.00 |
| P0105744 | 00201123 | PAYMASTER SALES & SERVICE | Check signature name plate | 312.95 |
| | 00201174 | US BANK CORP PAYMENT SYS | SHRM-SCP Certification Applica | 300.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | Wellness program recognition. | 225.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | Employee Recognition supplies | 144.13 |
| | 00201174 | US BANK CORP PAYMENT SYS | HR staff meeting. | 103.65 |
| | 00201174 | US BANK CORP PAYMENT SYS | WAPELRA Fall Training for L. G | 75.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | Office supplies. | 53.88 |
| | 00201174 | US BANK CORP PAYMENT SYS | Office supplies | 52.01 |
| | 00201174 | US BANK CORP PAYMENT SYS | Office supplies. | 51.37 |
| | 00201174 | US BANK CORP PAYMENT SYS | Employee recognition. | 50.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | Recruitment advertisement. | 45.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | Recruitment advertisement. | 45.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | Charge made in error; reimburs | 41.79 |
| P0105745 | 00200937 | VERIZON WIRELESS | HR Hotspot | 40.01 |
| | 00201174 | US BANK CORP PAYMENT SYS | Office supplies. | 28.59 |
| | 00201174 | US BANK CORP PAYMENT SYS | Certified mail fees. | 8.30 |
| Org Key: CT1100 - Municipal Court | | | | |
| P0105561 | 00200842 | DIRECT MATTERS | Direct Matters invoice #55910 | 472.14 |
| P0105560 | 00200842 | DIRECT MATTERS | Direct Matters invoice #55911 | 398.41 |
| P0105559 | 00200842 | DIRECT MATTERS | Direct Matters invoice #55914 | 309.62 |
| P0105677 | 00201052 | INTERCOM LANGUAGE SERVICES INC | Intercom invoice #19-370 | 276.25 |
| P0105620 | 00200904 | OLSON, JOHN R | Pro Tem Judge 10/22/19 - 5.5 h | 275.00 |
| P0105558 | 00200865 | INTERCOM LANGUAGE SERVICES INC | Intercom invoice 19-358 - 10/1 | 260.00 |
| P0105815 | 00201052 | INTERCOM LANGUAGE SERVICES INC | Intercom invoice #19-328 | 260.00 |
| P0105563 | 00200842 | DIRECT MATTERS | Direct Matters invoice #55913 | 269.37 |
| P0105562 | 00200842 | DIRECT MATTERS | Direct Matters invoice #55912 | 230.12 |
| P0105776 | 00201014 | DMCMA | DMCMA - 2020 membership fee | 150.00 |
| P0105712 | 00201196 | XEROX CORPORATION | Xerox - invoice #098595551 | 152.53 |
| P0105814 | 00201052 | INTERCOM LANGUAGE SERVICES INC | Intercom invoice #19-307 | 130.00 |
| | 00201000 | COMPLETE OFFICE | OFFICE SUPPLIES OCT2019 | 18.15 |
| Org Key: DS0000 - Development Services-Revenue | | | | |
| P0102860 | 00200973 | BELLEVUE, CITY OF | 2019 Quarterly MBP.Com Fees | 9,978.93 |
| | 00201034 | GREENWOOD HEATING & A/C | PERMIT REFUND | 45.60 |
| | 00201034 | GREENWOOD HEATING & A/C | PERMIT REFUND | 2.26 |
| Org Key: DS1100 - Administration (DS) | | | | |
| P0105893 | 00201021 | ESA | Peer review for CAO19-019 | 3,183.40 |
| P0105893 | 00201021 | ESA | Peer review for CAO19-014 | 2,892.90 |
| P0105893 | 00201021 | ESA | Peer review for CAO19-006 | 1,557.56 |
| P0105893 | 00201021 | ESA | Peer review for CAO19-018 | 1,165.19 |

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| PO # | Check # | Vendor: | Transaction Description | Check Amount |
|--|----------|------------------------------|--------------------------------|--------------|
| P0105893 | 00201021 | ESA | Peer review for 1808-172 | 1,133.91 |
| P0105892 | 00201178 | VERIZON WIRELESS | Phone and data charges | 439.74 |
| P0105892 | 00201178 | VERIZON WIRELESS | Mobil hot spots | 160.04 |
| P0105890 | 00201181 | WABO | Advertising for Electrical/ | 100.00 |
| P0105887 | 00201011 | DEPT OF ENTERPRISES SERVICES | Standard Specifications 2020 b | 99.55 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 73.64 |
| | 00201011 | DEPT OF ENTERPRISES SERVICES | BUSINESS CARD PRINTING OCT2019 | 48.08 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 40.46 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 35.18 |
| | 00201174 | US BANK CORP PAYMENT SYS | Tuition & Registrations | 35.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 25.97 |
| Org Key: DS1200 - Bldg Plan Review & Inspection | | | | |
| P0105886 | 00200955 | COOPER ZIETZ ENGINEERS INC | On call electrical inspector | 14,603.42 |
| | 00201011 | DEPT OF ENTERPRISES SERVICES | BUSINESS CARD PRINTING OCT2019 | 48.08 |
| Org Key: DS1300 - Land Use Planning Svc | | | | |
| P0105893 | 00201021 | ESA | CAO Update | 5,506.24 |
| | 00201174 | US BANK CORP PAYMENT SYS | Dues & Subscriptions | 513.00 |
| P0105825 | 00201156 | SOUND PUBLISHING INC | Ntc. Public Hearing 2437559 | 67.66 |
| | 00201073 | LEON, ANDREW | PARKING FEE | 29.42 |
| Org Key: FN1100 - Administration (FN) | | | | |
| | 00201000 | COMPLETE OFFICE | OFFICE SUPPLIES OCT2019 | 352.47 |
| | 00201174 | US BANK CORP PAYMENT SYS | MBP.COM MERCHANT | 54.10 |
| Org Key: FN2100 - Data Processing | | | | |
| P0105727 | 00200932 | SUPERION LLC | ONESOLUTION ANNUAL MAINT | 11,152.75 |
| P0102377 | 00200888 | MI CHAMBER OF COMMERCE | MONTHLY BILLING FOR SERVICES | 1,200.00 |
| P0105622 | 00200932 | SUPERION LLC | ONESOLUTION FINANCE PROJECT | 240.00 |
| P0105680 | 00200887 | METROPRESORT | 2019 3RD QUARTER B&O TAX STATE | 130.27 |
| Org Key: FN4501 - Utility Billing (Water) | | | | |
| P0105831 | 00201039 | HARRIS COMPUTER SYSTEMS | ICIS ANNUAL SUPPORT (INHNAE) | 11,437.26 |
| P0105639 | 00200848 | FERGUSON ENTERPRISES LLC | SENSUS SOFTWARE FLEXNET SUPPOR | 1,449.28 |
| P0103098 | 00200887 | METROPRESORT | SEPTEMBER 2019 PRINTING AND MA | 177.11 |
| P0103098 | 00200887 | METROPRESORT | SEPTEMBER 2019 PRINTING AND MA | 167.62 |
| P0103098 | 00201084 | METROPRESORT | OCTOBER 2019 PRINTING AND MAIL | 149.15 |
| P0103098 | 00201084 | METROPRESORT | OCTOBER 2019 PRINTING AND MAIL | 141.70 |
| P0103098 | 00201084 | METROPRESORT | OCTOBER 2019 PRINTING AND MAIL | 129.90 |
| P0103098 | 00201084 | METROPRESORT | OCTOBER 2019 PRINTING AND MAIL | 123.49 |
| P0103098 | 00200887 | METROPRESORT | SEPTEMBER 2019 PRINTING AND MA | 108.60 |
| P0103098 | 00200887 | METROPRESORT | SEPTEMBER 2019 PRINTING AND MA | 90.53 |
| P0103098 | 00200887 | METROPRESORT | SEPTEMBER 2019 PRINTING AND MA | 85.81 |
| P0103098 | 00200887 | METROPRESORT | SEPTEMBER 2019 PRINTING AND MA | 81.07 |
| P0103098 | 00200887 | METROPRESORT | SEPTEMBER 2019 PRINTING AND MA | 68.54 |
| P0103098 | 00201084 | METROPRESORT | OCTOBER 2019 PRINTING AND MAIL | 67.24 |
| P0103098 | 00200887 | METROPRESORT | SEPTEMBER 2019 PRINTING AND MA | 64.83 |
| P0103098 | 00201084 | METROPRESORT | OCTOBER 2019 PRINTING AND MAIL | 63.68 |
| P0105678 | 00200937 | VERIZON WIRELESS | CITYWORKS IPAD FOR JEEP | 40.01 |

Org Key: FN4502 - Utility Billing (Sewer)

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|--|----------|----------------------------|--------------------------------|--------------|
| P0105831 | 00201039 | HARRIS COMPUTER SYSTEMS | ICIS ANNUAL SUPPORT (INHNAE) | 11,437.26 |
| P0105639 | 00200848 | FERGUSON ENTERPRISES LLC | SENSUS SOFTWARE FLEXNET SUPPOR | 1,449.28 |
| P0105634 | 00200848 | FERGUSON ENTERPRISES LLC | REPLACEMENT BATTERY FOR | 211.97 |
| P0103098 | 00200887 | METROPRESORT | SEPTEMBER 2019 PRINTING AND MA | 177.11 |
| P0103098 | 00200887 | METROPRESORT | SEPTEMBER 2019 PRINTING AND MA | 167.62 |
| P0103098 | 00201084 | METROPRESORT | OCTOBER 2019 PRINTING AND MAIL | 149.15 |
| P0103098 | 00201084 | METROPRESORT | OCTOBER 2019 PRINTING AND MAIL | 141.70 |
| P0103098 | 00201084 | METROPRESORT | OCTOBER 2019 PRINTING AND MAIL | 129.90 |
| P0103098 | 00201084 | METROPRESORT | OCTOBER 2019 PRINTING AND MAIL | 123.49 |
| P0103098 | 00200887 | METROPRESORT | SEPTEMBER 2019 PRINTING AND MA | 108.60 |
| P0103098 | 00200887 | METROPRESORT | SEPTEMBER 2019 PRINTING AND MA | 90.53 |
| P0103098 | 00200887 | METROPRESORT | SEPTEMBER 2019 PRINTING AND MA | 85.81 |
| P0103098 | 00200887 | METROPRESORT | SEPTEMBER 2019 PRINTING AND MA | 81.07 |
| P0103098 | 00200887 | METROPRESORT | SEPTEMBER 2019 PRINTING AND MA | 68.54 |
| P0103098 | 00201084 | METROPRESORT | OCTOBER 2019 PRINTING AND MAIL | 67.24 |
| P0103098 | 00200887 | METROPRESORT | SEPTEMBER 2019 PRINTING AND MA | 64.83 |
| P0103098 | 00201084 | METROPRESORT | OCTOBER 2019 PRINTING AND MAIL | 63.68 |
| <i>Org Key: FN4503 - Utility Billing (Storm)</i> | | | | |
| P0105831 | 00201039 | HARRIS COMPUTER SYSTEMS | ICIS ANNUAL SUPPORT (INHNAE) | 11,437.25 |
| P0103098 | 00200887 | METROPRESORT | SEPTEMBER 2019 PRINTING AND MA | 177.10 |
| P0103098 | 00200887 | METROPRESORT | SEPTEMBER 2019 PRINTING AND MA | 167.62 |
| P0103098 | 00201084 | METROPRESORT | OCTOBER 2019 PRINTING AND MAIL | 149.14 |
| P0103098 | 00201084 | METROPRESORT | OCTOBER 2019 PRINTING AND MAIL | 141.71 |
| P0103098 | 00201084 | METROPRESORT | OCTOBER 2019 PRINTING AND MAIL | 129.91 |
| P0103098 | 00201084 | METROPRESORT | OCTOBER 2019 PRINTING AND MAIL | 123.49 |
| P0103098 | 00200887 | METROPRESORT | SEPTEMBER 2019 PRINTING AND MA | 108.59 |
| P0103098 | 00200887 | METROPRESORT | SEPTEMBER 2019 PRINTING AND MA | 90.53 |
| P0103098 | 00200887 | METROPRESORT | SEPTEMBER 2019 PRINTING AND MA | 85.80 |
| P0103098 | 00200887 | METROPRESORT | SEPTEMBER 2019 PRINTING AND MA | 81.07 |
| P0103098 | 00200887 | METROPRESORT | SEPTEMBER 2019 PRINTING AND MA | 68.55 |
| P0103098 | 00201084 | METROPRESORT | OCTOBER 2019 PRINTING AND MAIL | 67.24 |
| P0103098 | 00200887 | METROPRESORT | SEPTEMBER 2019 PRINTING AND MA | 64.82 |
| P0103098 | 00201084 | METROPRESORT | OCTOBER 2019 PRINTING AND MAIL | 63.69 |
| <i>Org Key: FR1100 - Administration (FR)</i> | | | | |
| P0105769 | 00201161 | SYSTEMS DESIGN WEST LLC | 09/2019 Transport Billing Fees | 1,220.20 |
| P0102380 | 00200999 | COMCAST | FIRE STATION 92 FIBER CIRCUIT | 862.66 |
| | 00201174 | US BANK CORP PAYMENT SYS | Quality Improvement through Ac | 625.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | Quality Improvement Through Ac | 625.00 |
| P0105749 | 00201007 | CULLIGAN SEATTLE WA | Water Service/Fire | 410.23 |
| P0105864 | 00201131 | PUBLIC SAFETY SELECTION PC | Psych Eval/Collins | 400.00 |
| P0105766 | 00201058 | KAISER PERMANENTE | Medical Testing/Collins | 350.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | NW Leadership - Heitman | 350.00 |
| P0105755 | 00200922 | RICOH USA INC (FIRE) | Copier Rental/Fire | 278.52 |
| P0105765 | 00201070 | KROESENS UNIFORM COMPANY | Uniform Pants/Heitman | 259.15 |
| | 00201174 | US BANK CORP PAYMENT SYS | Department Study Advertising | 225.00 |
| P0105768 | 00201062 | KCFCA | 2019 Leadership Seminar | 200.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | Air/Water Hose 75' | 191.35 |
| | 00200831 | CENTURYLINK | PHONE USE NOV 2019 | 179.51 |
| P0105756 | 00200963 | ASPECT SOFTWARE INC | Telestaff Monthly Charges | 165.00 |

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| PO # | Check # | Vendor: | Transaction Description | Check Amount |
|---|----------|-------------------------------|--------------------------------|--------------|
| P0105754 | 00200833 | COMCAST | Internet Charges/Fire | 108.55 |
| | 00201174 | US BANK CORP PAYMENT SYS | Lysol Cleaner/Stock Supply | 101.70 |
| | 00201174 | US BANK CORP PAYMENT SYS | Air/Water Hose - 50' | 92.63 |
| P0105753 | 00200833 | COMCAST | Internet Charges/Fire | 88.60 |
| P0105867 | 00200998 | COMCAST | Internet Charges/Fire | 62.39 |
| | 00201174 | US BANK CORP PAYMENT SYS | Halloween Candy North and Sout | 59.96 |
| | 00201174 | US BANK CORP PAYMENT SYS | Hose Nozzles | 49.45 |
| | 00201174 | US BANK CORP PAYMENT SYS | Uniform Cleaning/Mandella | 40.21 |
| | 00201174 | US BANK CORP PAYMENT SYS | Uniform Cleaning/Mandella | 32.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | Dishwasher Wheel Kit/Station 1 | 29.70 |
| | 00201174 | US BANK CORP PAYMENT SYS | Uniform Cleaning/Mandella | 21.33 |
| | 00201174 | US BANK CORP PAYMENT SYS | Monthly Membership Fee | 14.29 |
| P0105752 | 00200833 | COMCAST | Internet Charges/Fire | 11.49 |
| P0105866 | 00200998 | COMCAST | Internet Charges/Fire | 11.49 |
| | 00201174 | US BANK CORP PAYMENT SYS | Ship Uniform to Kroesen's | 10.94 |
| | 00201174 | US BANK CORP PAYMENT SYS | Uniform Cleaning/Heitman | 8.25 |
| | 00201174 | US BANK CORP PAYMENT SYS | Ship Uniform to Kroesen's | 6.90 |
| | 00201174 | US BANK CORP PAYMENT SYS | Uniform Cleaning/Heitman | 5.50 |
| <i>Org Key: FR2100 - Fire Operations</i> | | | | |
| P0102479 | 00201106 | NORCOM 911 | FIRE DISPATCH 2019 | 42,352.25 |
| P0105767 | 00200918 | REDMOND, CITY OF | 3rd Qtr 2019 App Maint. | 10,639.99 |
| P0105757 | 00201075 | LIFE ASSIST INC | Glucose Strips/BP Cuffs (BLS C | 7,291.49 |
| P0105759 | 00201077 | LN CURTIS & SONS | Medical Bags (BLS Core Funds) | 2,671.90 |
| P0105873 | 00201110 | O'REILLY FIRST CALL | New Engine Map Box Modificatio | 2,029.34 |
| P0105764 | 00201145 | SEA WESTERN INC | Compressor Service | 1,950.00 |
| P0105762 | 00201074 | LES SCHWAB TIRE CENTER | Replace Tires/7607 | 1,817.56 |
| P0105859 | 00201050 | IMS ALLIANCE | Incident Command Worksheets | 1,333.00 |
| P0105751 | 00200938 | VERIZON WIRELESS | MDC Charges/Fire | 1,120.03 |
| P0102405 | 00201020 | EPSCA | MONTHLY RADIO ACCESS FEES 49 R | 1,111.00 |
| P0105763 | 00201139 | ROMAINE ELECTRIC CORP | Battery/7607 | 579.12 |
| P0105761 | 00201163 | TEC EQUIPMENT INC | Parts for 8611 | 452.91 |
| P0105870 | 00200978 | BRAKE AND CLUTCH SUPPLY NORTH | Brake Parts/8611 | 436.80 |
| P0105764 | 00201145 | SEA WESTERN INC | Helmet | 291.50 |
| P0105760 | 00201118 | PACIFIC POWER GROUP LLC | Transducer/8611 | 174.88 |
| P0105872 | 00201109 | O'REILLY AUTOMOTIVE INC | Misc. Apparatus Parts | 169.12 |
| P0105765 | 00201070 | KROESEENS UNIFORM COMPANY | Fleece Jacket/Givens | 118.72 |
| P0105871 | 00201163 | TEC EQUIPMENT INC | Misc. Apparatus Parts - 7608 | 94.85 |
| | 00201174 | US BANK CORP PAYMENT SYS | Reload Good to Go Account | 50.00 |
| P0105869 | 00201179 | VERIZON WIRELESS | Cell Charges/Fire | 14.80 |
| | 00201174 | US BANK CORP PAYMENT SYS | Ship Knox Keypad for Repair | 14.34 |
| | 00201174 | US BANK CORP PAYMENT SYS | Ship Radio to Day Wireless | 12.57 |
| | 00201174 | US BANK CORP PAYMENT SYS | Ship Radio to Day Wireless | 6.90 |
| | 00201174 | US BANK CORP PAYMENT SYS | Ship Knox Key Pad for Repair | 6.90 |
| P0102405 | 00201020 | EPSCA | QTLY ACCESS FEE REBATE FIRE | -194.04 |
| <i>Org Key: FR2400 - Fire Suppression</i> | | | | |
| P0105764 | 00201145 | SEA WESTERN INC | MSA Service/Calibration & | 698.17 |
| P0105860 | 00201077 | LN CURTIS & SONS | Supplies for New Engine | 491.20 |
| P0105759 | 00201077 | LN CURTIS & SONS | Eclipse Rechargeable Batts | 294.71 |

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| PO # | Check # | Vendor: | Transaction Description | Check Amount |
|---|----------|----------------------------|--------------------------------|--------------|
| Org Key: FR2500 - Fire Emergency Medical Svcs | | | | |
| P0105757 | 00201075 | LIFE ASSIST INC | Station/Rig Aid Supplies | 1,301.84 |
| P0105863 | 00201075 | LIFE ASSIST INC | Stock Aid Supplies | 343.03 |
| P0105868 | 00200954 | AIRGAS USA LLC | Oxygen/Fire | 44.71 |
| P0105758 | 00200954 | AIRGAS USA LLC | Oxygen/Fire | 19.71 |
| Org Key: FR4100 - Training | | | | |
| P0105750 | 00200972 | BELLEVUE, CITY OF | January 2019 Academy/Edwards | 9,450.00 |
| P0105390 | 00201041 | HEALTHFORCE PARTNERS INC | 2019 Flu/TB Shots | 1,253.75 |
| P0105861 | 00201068 | KING COUNTY FIRE CHIEFS | Building Construction/Gruger | 250.00 |
| P0105861 | 00201068 | KING COUNTY FIRE CHIEFS | MCI - Austin/Kissel | 200.00 |
| P0105862 | 00201154 | SNYDER, BRIAN WADE | CBT Instructor (Makeups) | 150.00 |
| Org Key: FR5100 - Community Risk Reduction | | | | |
| | 00201174 | US BANK CORP PAYMENT SYS | Firehouse Munch/Tree Lighting | 237.51 |
| Org Key: GGM001 - General Government-Misc | | | | |
| P0105889 | 00201003 | SAHANDY, SHEIDA REBECCA | CPD Continuous Improvement Pro | 5,812.50 |
| P0105889 | 00201003 | SAHANDY, SHEIDA REBECCA | CPD Continuous Improvement Pro | 4,562.50 |
| P0105854 | 00201097 | MORNICK, MATTHEW | CMO/Finance Support 11/2-11/15 | 3,740.00 |
| P0105885 | 00201128 | POT O' GOLD INC | Coffee supplies | 496.13 |
| P0102380 | 00200999 | COMCAST | CITY HALL BACKUP INTERNET | 425.76 |
| | 00201174 | US BANK CORP PAYMENT SYS | Domain Fee: mercerisland.gov | 400.00 |
| P0102379 | 00200996 | COMCAST | CITY HALL HIGH SPEED INTERNET | 121.44 |
| P0105885 | 00201128 | POT O' GOLD INC | Water cooler Oct 2019 | 27.50 |
| P0105885 | 00201128 | POT O' GOLD INC | Water cooler for Nov 2019 | 27.50 |
| P0105885 | 00201128 | POT O' GOLD INC | Tea | 10.62 |
| Org Key: GGM004 - Gen Govt-Office Support | | | | |
| P0105888 | 00201137 | RESERVE ACCOUNT | Postage fund refill | 2,500.00 |
| P0105891 | 00201196 | XEROX CORPORATION | Print & Copy charges for CM co | 605.46 |
| P0105891 | 00201196 | XEROX CORPORATION | Print & copy charges for Mail | 493.61 |
| | 00201000 | COMPLETE OFFICE | OFFICE SUPPLIES OCT2019 | 432.09 |
| P0105804 | 00201001 | CONFIDENTIAL DATA DISPOSAL | City Shredding Services - Invo | 325.00 |
| | 00201000 | COMPLETE OFFICE | OFFICE SUPPLIES OCT2019 | 316.80 |
| | 00201000 | COMPLETE OFFICE | OFFICE SUPPLIES OCT2019 | 190.48 |
| P0105891 | 00201196 | XEROX CORPORATION | Print & Copy charges for CPD C | 172.81 |
| | 00201000 | COMPLETE OFFICE | OFFICE SUPPLIES OCT2019 | 75.22 |
| | 00201000 | COMPLETE OFFICE | OFFICE SUPPLIES OCT2019 | 47.62 |
| Org Key: GGM005 - Genera Govt-L1 Retiree Costs | | | | |
| | 00201044 | HILTNER, PETER | LEOFF1 Medicare Reimb | 556.00 |
| | 00201143 | SCHOENTRUP, WILLIAM | LEOFF1 Medicare Reimb | 483.60 |
| | 00200984 | CARLSON, LARRY | QTRLY LEOFF1 Medicare Reimb | 406.50 |
| P0105631 | 00200869 | JOHNSON, CURTIS | FRLEOFF1 Retiree Medical Expen | 369.49 |
| | 00201153 | SMITH, RICHARD | LEOFF1 Medicare Reimb | 223.10 |
| P0105567 | 00200942 | WEGNER, KEN | LEOFF1 Retiree Medical Expense | 222.01 |
| P0105742 | 00200856 | HAGSTROM, JAMES | FRLEOFF1 Retiree Medical Expen | 217.22 |
| | 00201037 | HAGSTROM, JAMES | LEOFF1 Medicare Reimb | 216.50 |
| | 00200971 | BARNES, WILLIAM | LEOFF1 Medicare Reimb | 211.50 |
| | 00201010 | DEEDS, EDWARD G | LEOFF1 Medicare Reimb | 211.50 |
| | 00201030 | GOODMAN, J C | LEOFF1 Medicare Reimb | 194.20 |

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|---|----------|--------------------------|--------------------------------|--------------|
| | 00201190 | WHEELER, DENNIS | LEOFF1 Medicare Reimb | 181.70 |
| | 00201182 | WALLACE, THOMAS | LEOFF1 Medicare Reimb | 170.10 |
| | 00201019 | ELSOE, RONALD | LEOFF1 Medicare Reimb | 169.50 |
| | 00200953 | ADAMS, RONALD E | LEOFF1 Medicare Reimb | 169.30 |
| | 00200966 | AUGUSTSON, THOR | LEOFF1 Medicare Reimb | 168.00 |
| | 00200977 | BOOTH, GLENDON D | LEOFF1 Medicare Reimb | 168.00 |
| | 00201013 | DEVENY, JAN P | LEOFF1 Medicare Reimb | 168.00 |
| | 00201071 | KUHN, DAVID | LEOFF1 Medicare Reimb | 168.00 |
| P0105636 | 00200879 | LOISEAU, LERI M | LEOFF1 Retiree Medical Expense | 167.68 |
| | 00200982 | CALLAGHAN, MICHAEL | LEOFF1 Medicare Reimb | 164.40 |
| | 00201056 | JOHNSON, CURTIS | LEOFF1 Medicare Reimb | 163.70 |
| | 00201078 | LOISEAU, LERI M | LEOFF1 Medicare Reimb | 162.10 |
| | 00201141 | RUCKER, MANORD J | LEOFF1 Medicare Reimb | 162.10 |
| | 00201015 | DOWD, PAUL | LEOFF1 Medicare Reimb | 159.40 |
| | 00200951 | ABBOTT, RICHARD | LEOFF1 Medicare Reimb | 151.60 |
| | 00201187 | WEGNER, KEN | LEOFF1 Medicare Reimb | 146.60 |
| | 00201101 | MYERS, JAMES S | LEOFF1 Medicare Reimb | 137.40 |
| | 00201133 | RAMSAY, JON | LEOFF1 Medicare Reimb | 136.20 |
| | 00201026 | FORSMAN, LOWELL | LEOFF1 Medicare Reimb | 135.50 |
| | 00201080 | LYONS, STEVEN | LEOFF1 Medicare Reimb | 134.30 |
| | 00201166 | THOMPSON, JAMES | LEOFF1 Medicare Reimb | 123.30 |
| P0105633 | 00200897 | MYERS, JAMES S | LEOFF1 Retiree Medical Expense | 120.35 |
| P0105788 | 00201019 | ELSOE, RONALD | LEOFF1 Retiree Medical Expense | 108.91 |
| P0105842 | 00200953 | ADAMS, RONALD E | LEOFF1 Retiree Medical Expense | 96.90 |
| P0105632 | 00200916 | RAMSAY, JON | FRLEOFF1 Retiree Medical Expen | 60.72 |
| P0105789 | 00200966 | AUGUSTSON, THOR | LEOFF1 Retiree Medical Expense | 57.06 |
| P0105741 | 00200845 | ELSOE, RONALD | LEOFF1 Retiree Medical Expense | 52.19 |
| Org Key: GGM606 - Excess Retirement-Fire | | | | |
| | 00200971 | BARNES, WILLIAM | LEOFF1 Excess Benefit | 6,492.12 |
| | 00201004 | COOPER, ROBERT | LEOFF1 Excess Benefit | 6,415.42 |
| | 00201056 | JOHNSON, CURTIS | LEOFF1 Excess Benefit | 4,406.05 |
| | 00201143 | SCHOENTRUP, WILLIAM | LEOFF1 Excess Benefit | 3,377.07 |
| | 00201133 | RAMSAY, JON | LEOFF1 Excess Benefit | 2,557.20 |
| Org Key: IGMA02 - Alcoholism Program | | | | |
| P0103913 | 00201061 | KC FINANCE | Remit Quarterly Liquor Excise | 1,702.17 |
| Org Key: IGV012 - MW Pool Operation Subsidy | | | | |
| P0102579 | 00200890 | MI SCHOOL DISTRICT #400 | MI Pool Operation Subsidy | 11,611.88 |
| Org Key: IGVO02 - ARCH | | | | |
| P0104059 | 00200972 | BELLEVUE, CITY OF | 2019 Quarterly Admin Fee ARCH | 8,331.75 |
| Org Key: IS1100 - IGS Mapping | | | | |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 28.13 |
| Org Key: IS2100 - IGS Network Administration | | | | |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 1,307.00 |
| P0102965 | 00201066 | KING COUNTY FINANCE | I-NET MONTHLY SERVICES FROM | 1,122.00 |
| | 00200831 | CENTURYLINK | PHONE USE NOV 2019 | 603.65 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 481.44 |

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|----------|----------|--------------------------|-------------------------------|--------------|
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 383.90 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 361.08 |
| P0105672 | 00201048 | HYLAND SOFTWARE | Professional Services | 333.25 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 297.04 |
| P0102376 | 00200881 | MAGNAS LLC | MONTHLY LONG DISTANCE JAN-DEC | 206.00 |
| P0105689 | 00200952 | ACCESS | Final Invoice | 177.66 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 160.20 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 148.51 |
| | 00200882 | MANDEVILLE, TROY | MILEAGE EXPENSE | 138.04 |
| P0105679 | 00200937 | VERIZON WIRELESS | IGS WIFI/IGS LOANER/DEV IGS | 120.03 |
| P0105829 | 00200952 | ACCESS | RETRIEVAL OF BLACK BOX (WE NO | 95.50 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 74.87 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 55.44 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 30.79 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 22.36 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 14.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 13.83 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 11.54 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 6.49 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 4.79 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | -174.24 |

Org Key: MT2100 - Roadway Maintenance

| | | | | |
|----------|----------|-------------------------------|--------------------------------|----------|
| | 00200914 | PUGET SOUND ENERGY | ENERGY USE NOV 2019 | 9,214.85 |
| P0105316 | 00200818 | AG ENTERPRISE SUPPLY INC | DEICE MOUNT | 6,683.60 |
| P0105748 | 00201169 | TRAFFIC SAFETY SUPPLY | A-FRAMES FOR NOTICE SIGNS | 994.95 |
| P0105676 | 00201169 | TRAFFIC SAFETY SUPPLY | STREET SIGNS | 569.78 |
| P0105550 | 00200861 | HOME DEPOT CREDIT SERVICE | 300W HIGH LUMEN BULBS | 362.76 |
| P0105707 | 00201051 | IMS INFRASTRUCTURE MANGT SRVS | MOBILE ASSET DATA COLLECTION F | 283.41 |
| P0105901 | 00201067 | KING COUNTY FINANCE | SIGNALS | 167.99 |
| P0105571 | 00200874 | KING COUNTY FINANCE | SIGNAL SERVICES | 128.42 |
| P0105543 | 00200851 | GRAINGER | POLY 2" COUPLINGS & ADAPTERS | 53.00 |
| P0105613 | 00200863 | IBS INC | MISC. HARDWARE (WAREHOUSE) | 6.20 |

Org Key: MT2150 - Pavement Marking

| | | | | |
|----------|----------|---------------------|-----------------------|----------|
| P0105556 | 00200819 | ALPINE PRODUCTS INC | MARKER PADS & MARKERS | 3,937.29 |
| P0105733 | 00200957 | ALPINE PRODUCTS INC | MARKER PADS & MARKERS | 3,762.00 |

Org Key: MT2200 - Vegetation Maintenance

| | | | | |
|----------|----------|---------|----------------------------|------|
| P0105613 | 00200863 | IBS INC | MISC. HARDWARE (WAREHOUSE) | 5.32 |
|----------|----------|---------|----------------------------|------|

Org Key: MT2255 - Urban Forest Management (ROW)

| | | | | |
|----------|----------|----------------------------|--------------------------------|----------|
| P0105272 | 00201113 | HOLZER, KATHARINE E | 2019 TREE CABLING SYSTEM | 4,416.50 |
| P0103898 | 00200894 | MONARCH LANDSCAPING WA LLC | 2019 ROW PLANT WATERING & | 3,096.73 |
| P0104891 | 00200923 | RON'S STUMP REMOVAL & | 10% Retainage - MI Summer 2019 | 1,752.10 |

Org Key: MT2300 - Planter Bed Maintenance

| | | | | |
|----------|----------|------------------|-------------------------------|--------|
| P0105671 | 00200891 | MI UTILITY BILLS | PAYMENT FOR WATER SERVICE FOR | 488.53 |
| P0105613 | 00200863 | IBS INC | MISC. HARDWARE (WAREHOUSE) | 6.20 |

Org Key: MT2500 - ROW Administration

| | | | | |
|----------|----------|-----------------------|--------------------------------|--------|
| P0105659 | 00200929 | SOUND SAFETY PRODUCTS | SAFETY BOOTS & MISC. WORK CLOT | 403.30 |
|----------|----------|-----------------------|--------------------------------|--------|

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| P0105701 | 00201157 | SOUND SAFETY PRODUCTS | SAFETY BOOTS & MISC. WORK CLOT | 258.05 |
| P0105658 | 00200929 | SOUND SAFETY PRODUCTS | MISC. WORK CLOTHES | 182.57 |
| | 00201038 | HARB, SAM | MILEAGE EXPENSE | 28.30 |
| <i>Org Key: MT3100 - Water Distribution</i> | | | | |
| P0104977 | 00200990 | CHECK RIDE DRIVER TRAINING | CDL TRAINING WATER TEAM | 5,175.00 |
| P0105418 | 00201170 | UNITED RENTALS NORTH AMERICA | BOOM TRUCK RENTAL | 2,132.28 |
| P0105790 | 00201035 | H D FOWLER | PRV PARTS | 1,302.76 |
| P0105690 | 00201108 | NORTON CORROSION LIMITED LLC | NORTH & SOUTH 2019 ANNUAL | 1,088.00 |
| P0105685 | 00201032 | GRAINGER | PERMA PATCH | 577.51 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 307.98 |
| P0105853 | 00201032 | GRAINGER | WRENCH & MAGNETIC HOOKS | 284.08 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 250.00 |
| P0105773 | 00201032 | GRAINGER | MAGNETIC UTILITY HOOK | 105.14 |
| P0105732 | 00201035 | H D FOWLER | 3/8" BRASS PLUG | 84.81 |
| P0105642 | 00200851 | GRAINGER | GREASE GUN | 48.18 |
| P0105832 | 00201032 | GRAINGER | GRIPPER DISPOSABLE GLOVES | 37.50 |
| P0105812 | 00201157 | SOUND SAFETY PRODUCTS | MISC. WORK CLOTHES | 25.00 |
| P0105613 | 00200863 | IBS INC | MISC. HARDWARE (WAREHOUSE) | 6.20 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 5.25 |
| <i>Org Key: MT3150 - Water Quality Event</i> | | | | |
| P0105687 | 00201168 | LINKO TECHNOLOGY INC | SOFTWARE SUPPORT 1 YEAR (12/20 | 790.00 |
| P0105846 | 00201168 | LINKO TECHNOLOGY INC | WEB TEST ANNUAL FEE | 600.00 |
| P0105847 | 00200958 | AM TEST INC | WATER QUALITY | 240.00 |
| P0105510 | 00200855 | HACH COMPANY | REAGENT SET, CHLORINE FREE | 300.17 |
| P0105643 | 00200830 | CASCADE COLUMBIA DISTRIBUTION | SODIUM HYPOCHLORITE | 159.50 |
| P0105792 | 00201168 | LINKO TECHNOLOGY INC | OCTOBER WEB TEST REPORTS | 27.00 |
| P0105614 | 00200924 | ROSEMOUNT INC | SHIPPING CHARGES | 14.86 |
| <i>Org Key: MT3200 - Water Pumps</i> | | | | |
| P0105417 | 00201177 | UTILITY SERVICE CO INC | FIRST HILL BOOSTER | 6,562.60 |
| P0105385 | 00200854 | H D FOWLER | 12" FLANGED CHECK VALVE & RETA | 2,636.29 |
| P0105663 | 00200925 | S&B INC | FIRST HILL PRESSURE STATIONS | 1,184.70 |
| | 00200831 | CENTURYLINK | PHONE USE NOV 2019 | 238.76 |
| P0105603 | 00200851 | GRAINGER | NEOPRENE RUBBER STRIP | 46.93 |
| P0105565 | 00200851 | GRAINGER | NEOPRENE RUBBER STRIP | 31.29 |
| P0105613 | 00200863 | IBS INC | MISC. HARDWARE (WAREHOUSE) | 5.32 |
| <i>Org Key: MT3300 - Water Associated Costs</i> | | | | |
| P0105545 | 00200828 | BRANOM OPERATING COMPANY LLC | MAG METER TESTING | 990.00 |
| P0105812 | 00201157 | SOUND SAFETY PRODUCTS | MISC. WORK CLOTHES | 154.42 |
| P0105777 | 00201193 | WORKWEAR PLACE, THE | MISC. WORK CLOTHES | 100.89 |
| <i>Org Key: MT3400 - Sewer Collection</i> | | | | |
| P0103747 | 00201155 | SOLID WASTE SYSTEMS INC | CCTV TRUCK | 161,529.30 |
| P0105731 | 00201120 | PACIFIC RUBBER | HOSE ASSY. | 168.82 |
| P0105548 | 00200851 | GRAINGER | URINE BAGS (72 PK) | 158.86 |
| P0105710 | 00201091 | MI HARDWARE - UTILITY | MISC. HARDWARE FOR THE MONTH O | 23.72 |
| P0105613 | 00200863 | IBS INC | MISC. HARDWARE (WAREHOUSE) | 6.20 |
| <i>Org Key: MT3500 - Sewer Pumps</i> | | | | |

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| P0103747 | 00201155 | SOLID WASTE SYSTEMS INC | CCTV TRUCK | 29,415.58 |
| | 00200988 | CENTURYLINK BUSINESS SERVICES | PHONE USE NOV 2019 | 3,740.28 |
| P0105824 | 00201033 | GRAYBAR ELECTRIC CO | POWER LINE PROTECTORS 120VAC | 267.49 |
| P0105602 | 00200851 | GRAINGER | DUST PANS & BROOMS | 198.27 |
| P0105850 | 00200965 | AT&T MOBILITY | TELEMETRY NOV | 129.72 |
| P0105850 | 00200965 | AT&T MOBILITY | TELEMETRY - OCT | 113.73 |
| P0105671 | 00200891 | MI UTILITY BILLS | PAYMENT FOR WATER SERVICE FOR | 44.15 |
| P0105613 | 00200863 | IBS INC | MISC. HARDWARE (WAREHOUSE) | 6.20 |
| <i>Org Key: MT3600 - Sewer Associated Costs</i> | | | | |
| P0104977 | 00200990 | CHECK RIDE DRIVER TRAINING | CDL TRAINING SEWER TEAM | 5,175.00 |
| | 00200885 | MARTIN, ERIC | MILEAGE EXPENSE | 185.60 |
| | 00201096 | MOE, JIM | MILEAGE EXPENSE | 82.47 |
| <i>Org Key: MT3800 - Storm Drainage</i> | | | | |
| P0103471 | 00200975 | BEST PARKING LOT CLEANING INC | 2019 ON CALL STORMWATER CCTV | 5,951.99 |
| P0103471 | 00200826 | BEST PARKING LOT CLEANING INC | 2019 ON CALL STORMWATER CCTV | 4,630.30 |
| P0105601 | 00200854 | H D FOWLER | CB RISER W/KO, SOLID COVER, BR | 1,085.04 |
| P0105778 | 00201120 | PACIFIC RUBBER | 2" GREEN HOSE (50') | 204.79 |
| P0105659 | 00200929 | SOUND SAFETY PRODUCTS | SAFETY BOOTS & MISC. WORK CLOT | 190.04 |
| P0105738 | 00200958 | AM TEST INC | STORMWATER SAMPLES | 95.00 |
| P0105612 | 00200854 | H D FOWLER | 18" SOIL-TIGHT SPLIT COUPLING | 65.22 |
| P0105613 | 00200863 | IBS INC | MISC. HARDWARE (WAREHOUSE) | 6.20 |
| <i>Org Key: MT4101 - Support Services - General Fd</i> | | | | |
| | 00201000 | COMPLETE OFFICE | OFFICE SUPPLIES OCT2019 | 310.30 |
| <i>Org Key: MT4102 - Support Services - Water</i> | | | | |
| | 00201000 | COMPLETE OFFICE | OFFICE SUPPLIES OCT2019 | 183.22 |
| <i>Org Key: MT4150 - Support Services - Clearing</i> | | | | |
| P0102911 | 00200993 | CINTAS CORPORATION #460 | 2019 COVERALL SERVICES | 2,822.56 |
| P0102717 | 00200937 | VERIZON WIRELESS | 2019 VERIZON WIRELESS | 2,690.12 |
| P0105902 | 00201180 | WA AUDIOLOGY SERVICES INC | PW ANNUAL HEARING TEST | 1,225.10 |
| P0105606 | 00200941 | WEATHERNET LLC | NOVEMBER WEATHERNET | 490.00 |
| P0102711 | 00201195 | XEROX CORPORATION | 2019 BASE AND METER COPIER SER | 407.38 |
| P0102658 | 00201176 | UTILITIES UNDERGROUND LOCATION | 2019 UTILITY LOCATE SERVICES | 305.73 |
| | 00201000 | COMPLETE OFFICE | OFFICE SUPPLIES OCT2019 | 297.99 |
| P0105606 | 00200941 | WEATHERNET LLC | OCTOBER WEATHERNET | 245.00 |
| P0103275 | 00201006 | CRYSTAL AND SIERRA SPRINGS | PW WATER DELIVERY | 149.40 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 144.05 |
| P0105572 | 00200896 | MULTICARE IMMEDIATE CLINIC | J.FENHAUS DOT EXAM | 100.00 |
| P0105793 | 00201098 | MULTICARE IMMEDIATE CLINIC | J. FELIX CDL PHYSICAL | 100.00 |
| P0105793 | 00201098 | MULTICARE IMMEDIATE CLINIC | K.PAPADEM CDL PHYSICAL | 100.00 |
| P0105572 | 00200896 | MULTICARE IMMEDIATE CLINIC | J.JOHNSON HEP B VACCINE | 99.00 |
| P0102657 | 00200997 | COMCAST | 2019 PW WIFI CONNECTION | 86.39 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 86.33 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 32.99 |
| P0102405 | 00201020 | EPSCA | MONTHLY RADIO ACCESS FEES 1 RA | 25.25 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 17.49 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 11.87 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 9.95 |

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| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 6.25 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 3.25 |
| P0102405 | 00201020 | EPSCA | QTLY ACCESS FEE REBATE PUBLIC | -4.41 |
| <i>Org Key: MT4200 - Building Services</i> | | | | |
| P0105730 | 00201132 | RAINIER BUILDING SERVICES | JANITORIAL CITY BLDGS | 5,126.11 |
| | 00200914 | PUGET SOUND ENERGY | ENERGY USE NOV 2019 | 3,955.45 |
| P0105576 | 00200915 | RAINIER BUILDING SERVICES | FLOOR WAXING CITY BUILDINGS | 1,088.00 |
| P0105671 | 00200891 | MI UTILITY BILLS | PAYMENT FOR WATER SERVICE FOR | 625.99 |
| P0105720 | 00201115 | PACIFIC AIR CONTROL INC | HVAC MAINT PW | 429.28 |
| P0105737 | 00201107 | NORTH COAST ELECTRIC | F592 LIGHTS RELAYS | 420.65 |
| P0105828 | 00201053 | INTERIOR FOLIAGE CO, THE | CITY HALL INTERIOR LANDSCAPING | 282.70 |
| P0105719 | 00201016 | EASTSIDE EXTERMINATORS | CH PEST EXTERMINATOR | 246.33 |
| P0105721 | 00201138 | RODDA PAINT CO | PAINT SUPPLIES CH | 120.01 |
| P0105781 | 00201032 | GRAINGER | LOCKOUT KIT | 116.64 |
| P0105708 | 00201090 | MI HARDWARE - ROW | MISC. HARDWARE FOR THE MONTH O | 53.42 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 47.92 |
| P0105613 | 00200863 | IBS INC | MISC. HARDWARE (WAREHOUSE) | 5.32 |
| <i>Org Key: MT4300 - Fleet Services</i> | | | | |
| P0105810 | 00201103 | NELSON PETROLEUM | UNLEADED DELIVERY | 4,840.35 |
| P0105874 | 00201104 | NELSON TOOL & EQUIPMENT LLC | FIRE STATION UNLEADED/DIESEL | 3,297.48 |
| P0105879 | 00201031 | GOODYEAR TIRE & RUBBER CO, THE | TIRE INVENTORY | 1,961.20 |
| P0105331 | 00200906 | PACIFIC GOLF & TURF | FL-0467 | 1,282.97 |
| P0105665 | 00200901 | NORTH LAKE MARINA- | MARINE FUEL | 1,255.83 |
| P0105668 | 00200902 | NORTON CORROSION LIMITED LLC | CATHODIC POST INSTALLATION | 1,229.00 |
| P0105604 | 00200981 | CALIBER - BELLEVUE | FL-0435 BODY DAMAGE REPAIR | 957.73 |
| P0105667 | 00200823 | AUTONATION INC | FL-0506 & FL-0462 REPAIR PARTS | 939.70 |
| P0105900 | 00201116 | PACIFIC GOLF & TURF | REPAIR PARTS | 635.43 |
| P0102654 | 00200960 | AMERIGAS-1400 | 2019 PROPANE DELIVERY | 485.32 |
| P0105693 | 00201152 | SME SOLUTIONS LLC | 10/19/19 UNLEADED TANK SERVICE | 467.21 |
| P0105664 | 00200866 | INTERSTATE BATTERY SYSTEMS | BATTERY INVENTORY | 396.94 |
| P0105794 | 00200873 | KIA MOTORS FINANCE | PW 2019 KIA NIRO PHEV LEASE | 388.20 |
| P0105693 | 00201152 | SME SOLUTIONS LLC | 10/16 UNLEADED TANK SERVICE | 387.10 |
| P0105857 | 00201197 | ZEE MEDICAL | FIRE EXTINGUISHER ABC 2.5 LB | 357.72 |
| P0105718 | 00200986 | CARQUEST AUTO PARTS STORES | FLEET REPAIR PARTS | 267.79 |
| P0105686 | 00201114 | OWEN EQUIPMENT COMPANY | FL480 | 259.58 |
| P0105392 | 00200978 | BRAKE AND CLUTCH SUPPLY NORTH | FL-0395 REPAIR PARTS | 230.53 |
| P0105848 | 00201150 | SIX ROBBLEES INC | FL-505 REPAIR PARTS | 93.28 |
| P0102659 | 00201129 | PRAXAIR DISTRIBUTION INC | 2019 ACETYLENE AND OXYGEN TANK | 53.85 |
| P0105392 | 00200978 | BRAKE AND CLUTCH SUPPLY NORTH | FREIGHT | 27.17 |
| P0105616 | 00200851 | GRAINGER | POWER CONNECTOR, GRAY, 4 WIRE | 24.59 |
| P0105565 | 00200851 | GRAINGER | PRESSURE GAUGE (0 TO 5000) | 24.40 |
| P0105613 | 00200863 | IBS INC | MISC. HARDWARE (WAREHOUSE) | 5.32 |
| <i>Org Key: MT4450 - Cust Resp - Clearing Acct</i> | | | | |
| P0105596 | 00200945 | WHISTLE WORKWEAR | MISC. WORK CLOTHES | 109.70 |
| <i>Org Key: MT4501 - Water Administration</i> | | | | |
| P0105884 | 00201149 | SEATTLE, CITY OF | OCTOBER 2019 WATER COSTS | 93,196.49 |
| P0105849 | 00200959 | AMERICAN WATER WORKS ASSOC | 2019-2020 DUES | 2,139.00 |

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| <i>Org Key: MT4502 - Sewer Administration</i> | | | | |
| P0102378 | 00201065 | KING COUNTY FINANCE | MONTHLY SEWER JAN-DEC 2019 | 399,175.98 |
| P0105717 | 00201012 | DEPT OF HEALTH | DOH SURVEY FEE | 1,122.00 |
| <i>Org Key: MT4900 - Solid Waste</i> | | | | |
| P0103107 | 00200905 | OLYMPIC ENVIRONMENTAL RES | 2019 ANNUAL RECYCLE EVENTS | 10,811.23 |
| <i>Org Key: PO1100 - Administration (PO)</i> | | | | |
| P0105809 | 00200938 | VERIZON WIRELESS | Cell Data Services - Invoice # | 649.45 |
| | 00201174 | US BANK CORP PAYMENT SYS | WASPC - Magnan | 567.45 |
| | 00201174 | US BANK CORP PAYMENT SYS | Awards for Hyderkhan and ROV | 335.50 |
| | 00201000 | COMPLETE OFFICE | OFFICE SUPPLIES OCT2019 | 118.01 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 59.18 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 15.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 12.28 |
| <i>Org Key: PO1350 - Police Emergency Management</i> | | | | |
| P0102405 | 00201020 | EPSCA | MONTHLY RADIO ACCESS FEES 13 R | 328.25 |
| P0105729 | 00201151 | SKYLINE COMMUNICATIONS INC | EOC INTERNET | 206.55 |
| P0105648 | 00200920 | REMOTE SATELLITE SYSTEMS INT'L | Sat Phone Service - EMAC - Inv | 54.00 |
| P0102405 | 00201020 | EPSCA | QTLY ACCESS FEE REBATE EMAC | -57.33 |
| <i>Org Key: PO1650 - Regional Radio Operations</i> | | | | |
| P0102405 | 00201020 | EPSCA | MONTHLY RADIO ACCESS FEES 59 R | 1,515.00 |
| P0102405 | 00201020 | EPSCA | QTLY ACCESS FEE REFUND POLICE | -264.60 |
| <i>Org Key: PO1700 - Records and Property</i> | | | | |
| P0105835 | 00201196 | XEROX CORPORATION | PD Admin Copier - Invoice #: | 378.13 |
| P0105801 | 00201196 | XEROX CORPORATION | Records Copier - Invoice #: | 207.31 |
| | 00201174 | US BANK CORP PAYMENT SYS | Records Supplies | 188.26 |
| | 00201174 | US BANK CORP PAYMENT SYS | Records Supplies | 81.34 |
| | 00201011 | DEPT OF ENTERPRISES SERVICES | BUSINESS CARD PRINTING OCT2019 | 24.05 |
| | 00201174 | US BANK CORP PAYMENT SYS | Parking for training event | 9.00 |
| <i>Org Key: PO1800 - Contract Dispatch Police</i> | | | | |
| P0102480 | 00201105 | NORCOM 911 | POLICE DISPATCH 2019 | 154,193.75 |
| P0105802 | 00201184 | WASHINGTON STATE PATROL | Background Checks CPL - Invoice | 222.00 |
| <i>Org Key: PO1900 - Jail/Home Monitoring</i> | | | | |
| P0105834 | 00201144 | SCORE | October Jail Housing Bill - In | 2,520.00 |
| P0105649 | 00200927 | SCORE | September Housing - Invoice # | 1,080.00 |
| <i>Org Key: PO2100 - Patrol Division</i> | | | | |
| P0105645 | 00200877 | KROESENS UNIFORM COMPANY | Uniform Supplies - Trudeauux - | 1,378.32 |
| P0105808 | 00201070 | KROESENS UNIFORM COMPANY | Uniform Supplies - Invoice # 5 | 395.18 |
| P0105799 | 00201075 | LIFE ASSIST INC | AED Batteries - Invoice #: 954 | 371.80 |
| P0105645 | 00200877 | KROESENS UNIFORM COMPANY | Uniform Supplies - Invoice # 5 | 296.97 |
| | 00201174 | US BANK CORP PAYMENT SYS | Standard for testing PBT's | 225.77 |
| P0105836 | 00201081 | MAC TOWING | Impound - CS# 2019-7445 - Invo | 207.90 |
| P0105647 | 00200949 | ZEE MEDICAL | First Aid Supplies - Invoice # | 156.22 |
| P0105800 | 00201189 | WESCOM | Maintenance on Radar Unit TA38 | 99.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | LED conversion kits for Stinge | 98.98 |
| P0105803 | 00200994 | CLEANERS PLUS 1 | Uniform Cleaning - Invoice # 7 | 91.79 |

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| | 00201174 | US BANK CORP PAYMENT SYS | Repair & Maintenance Services | 89.95 |
| | 00201174 | US BANK CORP PAYMENT SYS | Waterproof Notebooks | 76.93 |
| | 00201174 | US BANK CORP PAYMENT SYS | Aluminum Clipboard | 33.96 |
| | 00201174 | US BANK CORP PAYMENT SYS | Speakers for Desktop Computer | 29.68 |
| P0105645 | 00200877 | KROESENS UNIFORM COMPANY | Uniform Supplies - MP - Invoice | 29.26 |
| P0105645 | 00200877 | KROESENS UNIFORM COMPANY | Patrol Hat - Robinson - Invoice | 27.50 |
| P0105645 | 00200877 | KROESENS UNIFORM COMPANY | Patrol Hat - Seifert - Invoice | 27.50 |
| P0105796 | 00201072 | LANGUAGE LINE SERVICES | Language Line Services - Telep | 5.39 |
| <i>Org Key: PO2200 - Marine Patrol</i> | | | | |
| P0105855 | 00201130 | PRIORITY MARINE | REBUILD UPPER GEAR BOX DUE TO | 4,224.63 |
| P0105856 | 00201130 | PRIORITY MARINE | PATROL 14 & 11 OIL CHANGES/REP | 2,741.77 |
| P0105653 | 00200899 | NATIONAL CONST RENTALS INC | Seafair Bathroom Rental - Invo | 400.00 |
| P0105807 | 00201089 | MI HARDWARE - POLICE | MP Supplies - Invoice Numbers: | 199.35 |
| P0105650 | 00200943 | WEST MARINE PRO | MP Supplies - Invoice # 7371 | 17.03 |
| <i>Org Key: PO2201 - Dive Team</i> | | | | |
| P0105798 | 00201111 | OCCUPATIONAL HEALTH CTRS OF WA | Dive Physical - Schroeder - In | 482.50 |
| P0105806 | 00201111 | OCCUPATIONAL HEALTH CTRS OF WA | Dive Physical - Herzog - Invoi | 275.00 |
| <i>Org Key: PO3100 - Investigation Division</i> | | | | |
| P0105797 | 00200972 | BELLEVUE, CITY OF | iBase Application - Invoice #: | 1,337.96 |
| P0105652 | 00200935 | THOMSON REUTERS - WEST | Investigative services - Invo | 421.43 |
| P0105795 | 00201167 | THOMSON REUTERS - WEST | West Investigative Services - | 421.43 |
| <i>Org Key: PO3350 - School Resource Officer</i> | | | | |
| | 00201174 | US BANK CORP PAYMENT SYS | SRO purchased air horn for ALI | 14.29 |
| <i>Org Key: PO4100 - Firearms Training</i> | | | | |
| P0105805 | 00200969 | AXON ENTERPRISE INC | Taser Holsters - Invoice # | 343.20 |
| | 00201174 | US BANK CORP PAYMENT SYS | Calipers for sight replacement | 36.26 |
| P0105911 | 00201136 | RENTON FISH & GAME CLUB INC | Renton range fees | 25.00 |
| <i>Org Key: PO4300 - Police Training</i> | | | | |
| P0105582 | 00200827 | BLUE PEAK LOGIC INC | Skills Manager Software and FT | 1,474.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | 2020 Pocket Press law guides | 356.00 |
| <i>Org Key: PR1100 - Administration (PR)</i> | | | | |
| P0102737 | 00200825 | BELLEVUE, CITY OF | 2019 Specialized Recreation Se | 1,187.00 |
| P0105844 | 00201063 | KELLEY IMAGING SYSTEMS | Waterproof paper for the plott | 388.68 |
| P0105683 | 00200950 | 29 ELEVEN INC | Name Tags for staff | 374.55 |
| P0102331 | 00201196 | XEROX CORPORATION | Lease and print/copy charges f | 360.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | Donation Plaque | 203.50 |
| P0105822 | 00200950 | 29 ELEVEN INC | Name badges for staff | 158.40 |
| | 00201174 | US BANK CORP PAYMENT SYS | PR & YFS digital marketing | 68.76 |
| | 00201174 | US BANK CORP PAYMENT SYS | Advertising | 57.18 |
| P0105816 | 00201179 | VERIZON WIRELESS | P&R Wireless cell phone bill | 40.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | Department survey | 38.51 |
| P0105542 | 00200851 | GRAINGER | SEAT COVER DISPENSERS (4) | 34.62 |
| | 00201011 | DEPT OF ENTERPRISES SERVICES | BUSINESS CARD PRINTING OCT2019 | 24.05 |
| <i>Org Key: PR2100 - Recreation Programs</i> | | | | |
| P0105875 | 00201055 | JOHNSON JR, MARV | Instructor Payment for Basketb | 312.00 |

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| PO # | Check # | Vendor: | Transaction Description | Check Amount |
|---|----------|---------------------------|--------------------------------|--------------|
| | 00201174 | US BANK CORP PAYMENT SYS | Risk Management School Trainin | 295.00 |
| P0105623 | 00200876 | KOJIMA, KENTARO | Instructor Payment for Soapsto | 180.00 |
| P0105595 | 00200886 | MERCER ISLAND GUILD OF | Classified | 115.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | Parent's Night Out supplies | 79.17 |
| | 00201174 | US BANK CORP PAYMENT SYS | Supper Club Driver's dinner | 28.07 |
| | 00201174 | US BANK CORP PAYMENT SYS | Parent's Night Out supplies | 16.23 |
| <i>Org Key: PR2101 - Youth and Teen Camps</i> | | | | |
| P0105605 | 00200921 | REPUBLIC SERVICES #172 | 5500 ICW RECYCLE CONTAINER | 1,174.59 |
| P0105600 | 00200849 | FIRST STUDENT INC | Invoice #9308976 Gene Coulon P | 503.13 |
| P0105714 | 00201146 | SEATTLE ADVENTURE SPORTS | August 7 Rock Wall Rental | 405.00 |
| <i>Org Key: PR2103 - Aquatics Programs</i> | | | | |
| P0105837 | 00201065 | KING COUNTY FINANCE | Summer 2019 sample collection | 2,350.00 |
| P0105837 | 00201065 | KING COUNTY FINANCE | Summer 2019 sample collection | 675.00 |
| <i>Org Key: PR2108 - Health and Fitness</i> | | | | |
| P0105716 | 00200860 | HOBBS, TRACY | October Power Flex classes #94 | 1,863.92 |
| P0105876 | 00201140 | ROSENSTEIN, SUSIE | Instructor Payment for October | 465.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | Golf | 30.28 |
| <i>Org Key: PR3500 - Senior Services</i> | | | | |
| P0105626 | 00200892 | MISD FOOD SERVICE | 120 meals for senior social lu | 600.00 |
| P0105877 | 00201005 | CORT PARTY RENTAL | Senior Thanksgiving Lunch | 397.21 |
| P0105713 | 00201005 | CORT PARTY RENTAL | 10/31/19 Halloween Sr Lunch | 308.11 |
| P0105635 | 00200868 | JENSEN, JOHN | Talk for MICEC Luncheon - seni | 200.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 78.92 |
| | 00201174 | US BANK CORP PAYMENT SYS | Food supplies/ snacks for Seni | 74.49 |
| | 00201174 | US BANK CORP PAYMENT SYS | Senior lunch supplies | 63.29 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 55.92 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 55.81 |
| P0105816 | 00201179 | VERIZON WIRELESS | P&R Wireless cell phone bill | 54.41 |
| | 00201174 | US BANK CORP PAYMENT SYS | Senior lunch supplies | 38.59 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 36.88 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 30.17 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 30.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | printer cartridge | 20.89 |
| | 00201174 | US BANK CORP PAYMENT SYS | printer cartridge | 20.89 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 19.97 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 5.29 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 4.14 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 1.99 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 1.98 |
| <i>Org Key: PR4100 - Community Center</i> | | | | |
| | 00200914 | PUGET SOUND ENERGY | ENERGY USE NOV 2019 | 4,172.83 |
| P0105730 | 00201132 | RAINIER BUILDING SERVICES | JANITORIAL CITY BLDGS | 2,496.77 |
| P0105625 | 00200838 | CULTURES CONNECTING LLC | Cultural competence workshop | 1,800.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 1,280.40 |
| | 00201174 | US BANK CORP PAYMENT SYS | Advertising | 966.18 |
| | 00201174 | US BANK CORP PAYMENT SYS | NW Event Show | 700.71 |
| P0105576 | 00200915 | RAINIER BUILDING SERVICES | FLOOR WAXING CITY BUILDINGS | 540.00 |

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|----------|----------|--------------------------|---------------------------------|--------------|
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 503.80 |
| | 00201174 | US BANK CORP PAYMENT SYS | customer service recognition | 394.40 |
| P0105723 | 00201025 | FIRE PROTECTION INC | REPLACE BATTERIES ON A TROUBLE | 355.30 |
| P0102473 | 00201195 | XEROX CORPORATION | Use fee for 9-21-19 to 10-21-1 | 343.42 |
| P0102473 | 00201195 | XEROX CORPORATION | Monthly lease charges for colo | 267.30 |
| P0102461 | 00200833 | COMCAST | MICEC - High Speed Connection | 261.64 |
| | 00201174 | US BANK CORP PAYMENT SYS | mercero room table covers and b | 178.41 |
| | 00201174 | US BANK CORP PAYMENT SYS | ADMP supplies | 169.66 |
| P0105827 | 00201025 | FIRE PROTECTION INC | ADD LINE TO THE SECURITY PANEL | 148.50 |
| P0105722 | 00201025 | FIRE PROTECTION INC | INVESTIGATE TROUBLE ON FIRE AL | 148.50 |
| | 00201174 | US BANK CORP PAYMENT SYS | meeting room media equipment s | 110.21 |
| | 00201174 | US BANK CORP PAYMENT SYS | community coffee supplies | 109.52 |
| | 00201174 | US BANK CORP PAYMENT SYS | Supplies-Employee Recognition | 100.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 96.92 |
| | 00201174 | US BANK CORP PAYMENT SYS | NW Event Show | 75.98 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 71.75 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 70.94 |
| P0105819 | 00201032 | GRAINGER | DISPOSABLE GLOVES (M & L) AND | 69.09 |
| | 00201174 | US BANK CORP PAYMENT SYS | Training manual for CPRP exam. | 60.26 |
| P0105746 | 00201009 | DATAQUEST LLC | Background Checks | 53.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | back office supplies | 48.37 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 47.96 |
| | 00201174 | US BANK CORP PAYMENT SYS | weight room equipment | 40.59 |
| | 00201174 | US BANK CORP PAYMENT SYS | back office supplies | 37.23 |
| | 00201174 | US BANK CORP PAYMENT SYS | First Friday w/ Friends | 36.81 |
| P0105816 | 00201179 | VERIZON WIRELESS | P&R Wireless cell phone bill | 33.50 |
| P0105724 | 00201088 | MI HARDWARE - P&R | Misc P&R Supplies | 32.66 |
| | 00201174 | US BANK CORP PAYMENT SYS | Supplies-Employee Recognition | 25.28 |
| | 00201174 | US BANK CORP PAYMENT SYS | community coffee supplies | 20.08 |
| | 00201174 | US BANK CORP PAYMENT SYS | Staff Training items | 19.75 |
| | 00201174 | US BANK CORP PAYMENT SYS | staff training. | 19.44 |
| | 00201174 | US BANK CORP PAYMENT SYS | community coffee supplies | 17.53 |
| | 00201174 | US BANK CORP PAYMENT SYS | Staff Training Event | 13.98 |
| | 00201174 | US BANK CORP PAYMENT SYS | community coffee supplies | 7.56 |
| | 00201174 | US BANK CORP PAYMENT SYS | RETURN | -31.54 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | -40.43 |
| | 00201174 | US BANK CORP PAYMENT SYS | fitness room equipment return | -72.33 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | -106.95 |

Org Key: PR5300 - Community Arts Support

| | | | | |
|----------|----------|-------------------------|----------------|--------|
| P0105644 | 00200947 | YOUTH THEATER NORTHWEST | Season Sponsor | 800.00 |
|----------|----------|-------------------------|----------------|--------|

Org Key: PR5400 - Gallery Program

| | | | | |
|----------|----------|--------------------------|--------------------------------|--------|
| P0105702 | 00200853 | GREGERSON, ROBERT | Visual Arts Event - MI Arts Co | 150.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | Supplies for MIVAL Holiday Sho | 79.24 |
| | 00201174 | US BANK CORP PAYMENT SYS | Supplies for MIVAL Holiday Sho | 74.34 |

Org Key: PR5600 - Cultural & Performing Arts

| | | | | |
|----------|----------|----------------------|--------------------------------|-------|
| P0105825 | 00201156 | SOUND PUBLISHING INC | Ntc. Arts-Film Ad 10/23 242293 | 72.00 |
| P0105825 | 00201156 | SOUND PUBLISHING INC | Ntc. 10/30 2422942 | 72.00 |

Org Key: PR5700 - Special Programs

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|---|----------|------------------------------|--------------------------------|--------------|
| P0105644 | 00200947 | YOUTH THEATER NORTHWEST | Season Sponsor | 1,000.00 |
| P0105644 | 00200947 | YOUTH THEATER NORTHWEST | Season Sponsor | 700.00 |
| <i>Org Key: PR6100 - Park Maintenance</i> | | | | |
| P0104695 | 00200970 | BARK KING | 2019 Engineered Wood Fiber | 9,281.25 |
| P0105670 | 00200891 | MI UTILITY BILLS | PAYMENT OF UTILITY BILLS FOR W | 2,077.87 |
| P0105820 | 00201032 | GRAINGER | 60 GAL. FLAMMABLE CABINET | 941.64 |
| | 00201174 | US BANK CORP PAYMENT SYS | And wet/dry shop vac for Luthe | 494.97 |
| P0105902 | 00201180 | WA AUDIOLOGY SERVICES INC | PARKS ANNUAL HEARING TESTS | 471.00 |
| P0105821 | 00201122 | PART WORKS INC., THE | REPAIR & DIAPHRAGM KITS | 462.13 |
| P0105695 | 00201087 | MI HARDWARE - MAINT | MISC. HARDWARE FOR THE MONTH O | 379.57 |
| P0105816 | 00201179 | VERIZON WIRELESS | P&R Wireless cell phone bill | 289.84 |
| P0102911 | 00200993 | CINTAS CORPORATION #460 | PARKS COVERALLS | 113.60 |
| P0105813 | 00201032 | GRAINGER | TAMPER RESISTANT HEX KEY SETS | 118.19 |
| | 00201174 | US BANK CORP PAYMENT SYS | Registration at Seattle IPM Re | 106.98 |
| P0105398 | 00200944 | WHISTLE WORKWEAR | MISC. WORK CLOTHES | 101.04 |
| P0105548 | 00200851 | GRAINGER | SHARPS CONTAINERS | 83.85 |
| P0105724 | 00201088 | MI HARDWARE - P&R | Misc P&R Supplies | 75.88 |
| | 00201174 | US BANK CORP PAYMENT SYS | Clips for POW-MIA on City Hall | 21.67 |
| P0105597 | 00200944 | WHISTLE WORKWEAR | MISC. WORK CLOTHES | 19.79 |
| P0105613 | 00200863 | IBS INC | MISC. HARDWARE (WAREHOUSE) | 6.20 |
| | 00201174 | US BANK CORP PAYMENT SYS | Refund for one day registratio | -53.49 |
| <i>Org Key: PR6200 - Athletic Field Maintenance</i> | | | | |
| P0105670 | 00200891 | MI UTILITY BILLS | PAYMENT OF UTILITY BILLS FOR W | 5,657.11 |
| P0105703 | 00201158 | SPORTSLABS USA | Gmax Testing ICP/South Mercer | 750.00 |
| P0105682 | 00201164 | TEMPLINE PRODUCTS INC | FIELD PAINT | 356.40 |
| P0105816 | 00201179 | VERIZON WIRELESS | P&R Wireless cell phone bill | 115.47 |
| P0105673 | 00200928 | SHERWIN-WILLIAMS CO, THE | KIT, PUMP, ST, THREA | 109.21 |
| | 00200831 | CENTURYLINK | PHONE USE NOV 2019 | 86.34 |
| P0105675 | 00200867 | ISSAQUAH HONDA KUBOTA | CARBURETOR & FUEL LINE | 38.23 |
| P0105674 | 00200928 | SHERWIN-WILLIAMS CO, THE | RAC 5 STRIPING 421 PAINT | 24.48 |
| P0105613 | 00200863 | IBS INC | MISC. HARDWARE (WAREHOUSE) | 6.20 |
| <i>Org Key: PR6500 - Luther Burbank Park Maint.</i> | | | | |
| P0105670 | 00200891 | MI UTILITY BILLS | PAYMENT OF UTILITY BILLS FOR W | 2,592.15 |
| P0105730 | 00201132 | RAINIER BUILDING SERVICES | JANITORIAL CITY BLDGS | 2,151.25 |
| P0104695 | 00200970 | BARK KING | 2019 Engineered Wood Fiber | 1,031.25 |
| P0102521 | 00200832 | CINTAS CORPORATION #460 | Weekly floor mat cleaning at L | 295.30 |
| P0105711 | 00201119 | PACIFIC RIM EQUIPMENT RENTAL | 4 LIGHT TOWER RENTAL | 308.28 |
| P0105576 | 00200915 | RAINIER BUILDING SERVICES | FLOOR WAXING CITY BUILDINGS | 272.00 |
| P0104891 | 00200923 | RON'S STUMP REMOVAL & | 10% Retainage - MI Summer 2019 | 254.50 |
| P0105816 | 00201179 | VERIZON WIRELESS | P&R Wireless cell phone bill | 240.96 |
| P0105594 | 00200820 | AM TEST INC | Testing of water at Luther swi | 240.00 |
| P0105656 | 00200907 | PART WORKS INC., THE | CAP SCREWS & BUBBLER CARTRIDGE | 125.94 |
| | 00201174 | US BANK CORP PAYMENT SYS | Composting bags - correct desc | 104.57 |
| | 00201174 | US BANK CORP PAYMENT SYS | And wet/dry shop vac for Luthe | 98.99 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 73.92 |
| P0105695 | 00201087 | MI HARDWARE - MAINT | MISC. HARDWARE FOR THE MONTH O | 48.72 |
| | 00201174 | US BANK CORP PAYMENT SYS | LB Restroom Mirror | 31.87 |
| | 00201174 | US BANK CORP PAYMENT SYS | Disposal of deceased wildlife | 28.01 |

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| P0105709 | 00201086 | MI HARDWARE - BLDG | MISC. HARDWARE FOR THE MONTH O | 19.78 |
| P0105613 | 00200863 | IBS INC | MISC. HARDWARE (WAREHOUSE) | 6.20 |
| <i>Org Key: PR6600 - Park Maint-School Related</i> | | | | |
| P0105703 | 00201158 | SPORTSLABS USA | Gmax Testing ICP/South Mercer | 3,000.00 |
| P0105682 | 00201164 | TEMPLINE PRODUCTS INC | FIELD PAINT | 356.40 |
| P0105793 | 00201098 | MULTICARE IMMEDIATE CLINIC | SAUL CDL PHYSICAL | 100.00 |
| P0105673 | 00200928 | SHERWIN-WILLIAMS CO, THE | KIT, PUMP, ST, THREA | 109.21 |
| P0105675 | 00200867 | ISSAQUAH HONDA KUBOTA | CARBURETOR & FUEL LINE | 38.22 |
| P0105674 | 00200928 | SHERWIN-WILLIAMS CO, THE | RAC 5 STRIPING 421 PAINT | 24.47 |
| P0105816 | 00201179 | VERIZON WIRELESS | P&R Wireless cell phone bill | 15.51 |
| P0105613 | 00200863 | IBS INC | MISC. HARDWARE (WAREHOUSE) | 5.39 |
| <i>Org Key: PR6800 - Trails Maintenance</i> | | | | |
| P0104903 | 00201054 | ISSAQUAH HONDA KUBOTA | Backpack blower, saw chain, | 1,953.05 |
| P0104891 | 00200923 | RON'S STUMP REMOVAL & | 10% Retainage - MI Summer 2019 | 202.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 46.72 |
| P0105895 | 00201186 | WCIA | Intro to Muni Risk Management | 25.00 |
| <i>Org Key: PR6900 - Aubrey Davis Park Maintenance</i> | | | | |
| P0104695 | 00200970 | BARK KING | 2019 Engineered Wood Fiber | 2,062.50 |
| P0105670 | 00200891 | MI UTILITY BILLS | PAYMENT OF UTILITY BILLS FOR W | 1,500.73 |
| P0102825 | 00200936 | UNITED SITE SERVICES | Aubrey Davis & Lid | 456.09 |
| P0102825 | 00200936 | UNITED SITE SERVICES | Lid Park Boat Launch - | 235.14 |
| P0104891 | 00200923 | RON'S STUMP REMOVAL & | 10% Retainage - MI Summer 2019 | 180.00 |
| P0105597 | 00200944 | WHISTLE WORKWEAR | MISC. WORK CLOTHES | 193.19 |
| P0105398 | 00200944 | WHISTLE WORKWEAR | SAFETY BOOTS | 168.44 |
| P0105816 | 00201179 | VERIZON WIRELESS | P&R Wireless cell phone bill | 130.97 |
| P0105793 | 00201098 | MULTICARE IMMEDIATE CLINIC | ALEX CDL PHYSICAL | 100.00 |
| P0102520 | 00201162 | T2 SYSTEMS CANADA INC | Monthly charges for services t | 77.00 |
| P0105613 | 00200863 | IBS INC | MISC. HARDWARE (WAREHOUSE) | 6.20 |
| <i>Org Key: PY4618 - Flex Spending Admin 2018</i> | | | | |
| | 00200857 | HARB, SAM | FLEX SPEND REIMB | 172.93 |
| <i>Org Key: PY4619 - Flex Spending Admin 2019</i> | | | | |
| | 00200909 | PETERSEN, CHRIS | FLEX SPEND REIMB | 4,423.13 |
| | 00200872 | KELLEY, CHRIS M | FLEX SPEND REIMB | 1,194.75 |
| | 00200857 | HARB, SAM | FLEX SPEND REIMB | 396.93 |
| | 00200883 | MANRIQUEZ, CHERYL R | FLEX SPEND REIMB | 129.64 |
| | 00200930 | SPIETZ, ALLISON | FLEX SPEND REIMB | 35.57 |
| <i>Org Key: ST0020 - ST Long Term Parking</i> | | | | |
| P0105908 | 00201082 | MARTEN LAW | Professional Services - Invoic | 14,182.32 |
| P0105898 | 00201042 | HEARTLAND LLC | October 2019 Long-Term Parking | 12,512.50 |
| P0105581 | 00200884 | MARTEN LAW | Professional Services - Invoic | 9,723.43 |
| P0105906 | 00200962 | ASPECT CONSULTING LLC | Invoice No. 35021ST Long Term | 8,648.43 |
| P0105783 | 00201076 | LIGHTHOUSE LAW GROUP PLLC | ST Long Term Commuter Parking/ | 7,050.00 |
| P0105910 | 00201121 | PARKWAY CENTER MANAGEMENT GRP | BP/Arco Site - December Rent | 4,000.00 |
| P0105608 | 00200903 | OGDEN MURPHY WALLACE PLLC | Professional Services - Invoic | 3,510.00 |
| P0105608 | 00200903 | OGDEN MURPHY WALLACE PLLC | Professional Services - Invoic | 300.00 |
| P0105578 | 00200841 | DEPT OF TRANSPORTATION | WSDOT Monitoring Well - Invoic | 36.50 |

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| <i>Org Key: WD533C - Sub Basin 49b</i> | | | | |
| P0104255 | 00201017 | MACRAE, DANIEL J | 2019 WATERCOURSE BANK STABALIZ | 106,833.30 |
| <i>Org Key: WD534C - Sub Basin 51a</i> | | | | |
| P0104255 | 00201017 | MACRAE, DANIEL J | 2019 WATERCOURSE BANK STABALIZ | 3,391.50 |
| <i>Org Key: WD907R - Basin #42.1 and 42.1a Impvt</i> | | | | |
| P0105851 | 00201191 | WHISTLE WORKWEAR | SAFETY BOOTS | 148.62 |
| <i>Org Key: WG101R - City Hall Building Repairs</i> | | | | |
| P0105726 | 00200980 | BUILDERS HARDWARE & SUPPLY CO. | COURT SECURITY LOCKS LABOR | 990.00 |
| <i>Org Key: WG105R - Community Center Bldg Repairs</i> | | | | |
| P0105725 | 00201115 | PACIFIC AIR CONTROL INC | VFD REPLACE FC12 SLATER ROOM | 2,035.39 |
| <i>Org Key: WG107R - Luther Burbank Admin Bldg Rep</i> | | | | |
| P0105883 | 00200956 | ALBA PRO PAINTING LLC | PATCHING & PAINTING LB ADMIN | 10,828.40 |
| <i>Org Key: WG110T - Computer Equip Replacements</i> | | | | |
| P0105163 | 00200964 | ASTRAL COMMUNICATIONS INC | Cradlepoint Netcloud | 4,307.08 |
| P0105052 | 00200840 | DATEC INC. | 5 MDC Mounts for Patrol Cars | 1,373.63 |
| <i>Org Key: WG130E - Equipment Rental Vehicle Repl</i> | | | | |
| P94483 | 00201064 | KIA MOTORS FINANCE | DSG 2016 KIA SOUL LEASE | 211.36 |
| P94483 | 00201064 | KIA MOTORS FINANCE | LATE CHARGE | 10.57 |
| <i>Org Key: WG131E - Fire Equipment</i> | | | | |
| P0105770 | 00201099 | MUNICIPAL EMERGENCY SERVICES | Hazmat Supplies | 10,124.51 |
| P0104415 | 00201077 | LN CURTIS & SONS | 3 Chainsaw Chains | 1,001.55 |
| <i>Org Key: WG550R - Fuel Clean Up</i> | | | | |
| P0099477 | 00201023 | FARALLON CONSULTING LLC | PHASE 3 TECHNICAL SERVICES FOR | 11,839.99 |
| <i>Org Key: WG716T - Thrift Shop Pt of Sale</i> | | | | |
| P0105060 | 00200898 | NATIONAL BUSINESS SYSTEMS | 2 Inch Label Printer | 1,138.50 |
| P0105060 | 00200898 | NATIONAL BUSINESS SYSTEMS | 4 Inch Label Printer | 423.50 |
| P0105688 | 00201102 | NATIONAL BUSINESS SYSTEMS | Dock for POS Tablet | 288.42 |
| <i>Org Key: WG717T - Watercourse GIS Layer</i> | | | | |
| P0104079 | 00200859 | HERRERA ENVIRONMENTAL CONSULT | PROFESSIONAL SERVICES FOR MERC | 5,331.86 |
| <i>Org Key: WPI22P - Open Space - Pioneer/Engstrom</i> | | | | |
| P0102736 | 00200844 | EARTHCORPS INC | 2019-2020 Forest Restoration | 2,590.00 |
| P0105655 | 00200931 | STORM LAKE GROWERS INC | Plants for OSV | 777.59 |
| P0104891 | 00200923 | RON'S STUMP REMOVAL & | 10% Retainage - MI Summer 2019 | 385.30 |
| P0105654 | 00200934 | TADPOLE HAVEN NATIVE PLANTS | Plants for OSV | 309.37 |
| P0102825 | 00200936 | UNITED SITE SERVICES | Pioneer Park - 2019 Portable T | 95.04 |
| <i>Org Key: WPI22R - Vegetation Management</i> | | | | |
| P0105655 | 00200931 | STORM LAKE GROWERS INC | Plants for OSV | 3,128.52 |
| P0102736 | 00200844 | EARTHCORPS INC | 2019-2020 Forest Restoration | 2,717.50 |
| P0103001 | 00201027 | GARDEN CYCLES | MI Open Space Restoration 2019 | 1,649.00 |
| P0103218 | 00201036 | HABITAT RESTORATION SPEC LLC | MI Open Space Restoration 2019 | 1,470.00 |
| P0104698 | 00201028 | GARDEN CYCLES | Clarke Beach Park - Ivy Ring P | 1,320.00 |
| P0105654 | 00200934 | TADPOLE HAVEN NATIVE PLANTS | Plants for OSV | 1,040.88 |

Accounts Payable Report by GL Key

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| PO # | Check # | Vendor: | Transaction Description | Check Amount |
|---|----------|-------------------------------|--------------------------------|--------------|
| P0105272 | 00201113 | HOLZER, KATHARINE E | 2019 TREE CABLING SYSTEM | 962.50 |
| P0104891 | 00200923 | RON'S STUMP REMOVAL & | 10% Retainage - MI Summer 2019 | 753.80 |
| P0105695 | 00201087 | MI HARDWARE - MAINT | MISC. HARDWARE FOR THE MONTH O | 75.29 |
| | 00201174 | US BANK CORP PAYMENT SYS | City of Seattle IPM CE & Recer | 53.49 |
| <i>Org Key: WP506S - Swim Beach Repair at Clarke</i> | | | | |
| P0105315 | 00200822 | APPLIED PROFESSIONAL SERVICES | Clarke Beach Utility locates | 270.00 |
| <i>Org Key: WP700P - Aubrey Davis Multiuse Corridor</i> | | | | |
| P0100545 | 00201046 | HOUGH BECK & BAIRD | Professional Services for Aubr | 4,327.00 |
| <i>Org Key: WP710C - Street End - Lincoln Landing</i> | | | | |
| P0095391 | 00200910 | PND ENGINEERS INC | Lincoln Landing Design Enginee | 2,499.49 |
| <i>Org Key: WR152R - 80th Ave Sidewalk (28th-32nd)</i> | | | | |
| P0104658 | 00201069 | KPG | 80TH AVE SE PEDESTIAN IMPROVEM | 18,300.00 |
| <i>Org Key: WS101U - Backyard Sewer System Impvt</i> | | | | |
| P0105662 | 00200912 | PRO-VAC | 81ST BACKYARD SEWER PUNCHLIST | 7,095.00 |
| <i>Org Key: WS712P - Pump Sta/Lake Line Access Eval</i> | | | | |
| P0099675 | 00201165 | TETRA TECH INC | LAKE LINE & PUMP STATION ACCES | 3,180.40 |
| <i>Org Key: WS713T - SCADA System Upgrade</i> | | | | |
| P0105538 | 00200961 | CHIP GEORGE INC | 3 Pump Station Cellular Deploy | 7,021.31 |
| P0104206 | 00200871 | KBA INC | SCADA SYSTEM CONSTRUCTION | 4,370.45 |
| P0104206 | 00201060 | KBA INC | SCADA SYSTEM CONSTRUCTION | 3,323.72 |
| P0103848 | 00200985 | CAROLLO ENGINEERS INC | CHLORINATION SYSSTEN OPTIMIZAT | 1,006.50 |
| P0105692 | 00200961 | CHIP GEORGE INC | AC Adapters for Pump Routers | 86.91 |
| <i>Org Key: WS901D - Sewer Sys Pump Sta Repairs</i> | | | | |
| P0104206 | 00201060 | KBA INC | UTILITY CONSTRUCTION | 9,105.85 |
| P0104206 | 00200871 | KBA INC | UTILITY CONSTRUCTION | 4,394.93 |
| <i>Org Key: WS901G - Sewer System Generator Repl</i> | | | | |
| P0104206 | 00201060 | KBA INC | UTILTIY CONSTRUCTION | 1,645.57 |
| P0102393 | 00200992 | CHS ENGINEERS LLC | GENERATOR 13/17/24 AND PS 18 | 1,143.10 |
| P0104206 | 00200871 | KBA INC | UTILTIY CONSTRUCTION | 708.55 |
| P0105830 | 00201008 | DAILY JOURNAL OF COMMERCE | PS 11 GENERATOR | 260.40 |
| P0105570 | 00200829 | BUILDERS EXCHANGE OF WA | LIFT STATION 11 BID | 45.00 |
| <i>Org Key: WS906C - Swr PS Flow Monitor Install</i> | | | | |
| P0105728 | 00201018 | EJ USA INC | COMPOSITE MANHOLE RING & COVER | 1,807.53 |
| P0105728 | 00201018 | EJ USA INC | CREDIT- RETURNED MANHOLE | -707.14 |
| <i>Org Key: WW120S - Meter Replacement Residential</i> | | | | |
| P0102980 | 00201040 | HDR ENGINEERING INC | WATER METER REPLACEMENT | 7,666.19 |
| P0104206 | 00201060 | KBA INC | METER REPLACEMENT | 426.82 |
| P0104206 | 00200871 | KBA INC | METER REPLACEMENT | 119.13 |
| <i>Org Key: WW522R - Reservoir Generator</i> | | | | |
| P0105058 | 00201100 | MURRAYSMITH INC | RESERVOIR STANDBY DIESEL GENER | 3,895.78 |
| <i>Org Key: WW523R - EMW 5400-6000 Block Watermain</i> | | | | |
| P0105669 | 00200946 | WILSON CONCRETE CONST INC | GLENHOME PROJECT | 2,145.00 |

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| P0105858 | 00201160 | SULC, JOSEPH M | EMW 5400-6000 WATERMAIN TREE | 660.00 |
| <i>Org Key: WW535D- Booster Chlorination Station</i> | | | | |
| P0104779 | 00201002 | CONFLUENCE ENGINEERING GRP LLC | BOOSTER CHLORINATION OPERATION | 23,234.80 |
| P0105738 | 00200958 | AM TEST INC | WATER QUALITY SAMPLES | 5,000.00 |
| P0104206 | 00201060 | KBA INC | BOOSTER CL2 STATION CONSRUCTIO | 426.82 |
| P0104206 | 00200871 | KBA INC | BOOSTER CL2 STATION CONSRUCTIO | 119.13 |
| <i>Org Key: WW713T - SCADA System Upgrade</i> | | | | |
| P0103284 | 00200979 | BROWN AND CALDWELL CONSULTANTS | PH1 SCADA EQUIPMENT REPLACEMEN | 71,129.45 |
| <i>Org Key: WW718R - Main 87th and 88th Ave SE</i> | | | | |
| P0105310 | 00200968 | AXIS SURVEY & MAPPING | 9600 WATER SYSTEM IMPROVEMENT | 8,000.00 |
| P0105308 | 00200976 | BLUELINE GROUP | 9600 WMW WATER SYSTEM | 5,870.75 |
| P0105310 | 00200824 | AXIS SURVEY & MAPPING | 9600 WATER SYSTEM IMPROVEMENT | 2,000.00 |
| <i>Org Key: WW914R - 96th 97th Ave and SE 34th Main</i> | | | | |
| P0105310 | 00200968 | AXIS SURVEY & MAPPING | 9600 WATER SYSTEM IMPROVEMENT | 12,560.00 |
| P0105308 | 00200976 | BLUELINE GROUP | 9600 WMW WATER S;YSTEM IMPROVE | 9,650.00 |
| P0105310 | 00200824 | AXIS SURVEY & MAPPING | 9600 WATER SYSTEM IMPROVEMENT | 3,140.00 |
| <i>Org Key: XP710R - Luther BB Minor Capital LEVY</i> | | | | |
| P0104854 | 00200821 | ANCHOR QEA LLC | Luther Burbank Irrigation Inta | 1,925.00 |
| <i>Org Key: YF1100 - YFS General Services</i> | | | | |
| P0105736 | 00200974 | BERK CONSULTING | November Invoice for work on | 632.50 |
| P0105791 | 00201009 | DATAQUEST LLC | Background checks | 578.50 |
| P0105844 | 00201063 | KELLEY IMAGING SYSTEMS | Waterproof paper for the plott | 388.68 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 343.30 |
| | 00200858 | HARNISH, CHRISTOPHER JAMES | CREDENTIAL RENEWAL | 342.50 |
| P0105739 | 00201173 | NEVADA SYSTEM OF HIGHER EDU | WA State Prevention Summit | 250.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | Bookshelf for counseling offic | 240.79 |
| P0102331 | 00201196 | XEROX CORPORATION | Lease and print/copy charges f | 235.99 |
| | 00201174 | US BANK CORP PAYMENT SYS | Desk for counseling office | 222.19 |
| P0102593 | 00201195 | XEROX CORPORATION | Lease and billable prints/copi | 183.23 |
| | 00201174 | US BANK CORP PAYMENT SYS | Lamp set for Emergency Assista | 175.99 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 168.72 |
| | 00201174 | US BANK CORP PAYMENT SYS | Desk and office supplies for Y | 147.76 |
| | 00201174 | US BANK CORP PAYMENT SYS | office chair for the YFS fron | 142.99 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 95.64 |
| | 00201174 | US BANK CORP PAYMENT SYS | Locking file cabinet for Clini | 72.96 |
| P0105747 | 00201185 | WASHINGTON STATE PATROL | Background Checks | 44.00 |
| P0102229 | 00200938 | VERIZON WIRELESS | Mobile device service for clie | 40.01 |
| P0102232 | 00201092 | MI HARDWARE - YFS | Operating supplies for Tshop a | 38.96 |
| P0105542 | 00200851 | GRAINGER | SEAT COVER DISPENSERS (4) | 34.63 |
| | 00201011 | DEPT OF ENTERPRISES SERVICES | BUSINESS CARD PRINTING OCT2019 | 24.05 |
| | 00200895 | MONTAGUE, LIANA | PARKING FEES | 20.24 |
| | 00201174 | US BANK CORP PAYMENT SYS | Office supplies for YFS staff | 5.49 |
| | 00201174 | US BANK CORP PAYMENT SYS | 86 one cent stamps from USPS | 0.86 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | -50.28 |
| <i>Org Key: YF1200 - Thrift Shop</i> | | | | |
| P0105730 | 00201132 | RAINIER BUILDING SERVICES | JANITORIAL CITY BLDGS | 2,184.74 |

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| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 875.37 |
| P0102249 | 00201159 | STRANGER, THE | Thrift Shop Advertising for 20 | 800.00 |
| P0102239 | 00200852 | GRAND & BENEDICTS INC | Operating supplies for the Thr | 690.82 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 474.25 |
| P0105823 | 00201079 | LOLLIE FLEUR | Lead design for Thrift Shop Ho | 396.00 |
| P0105771 | 00201117 | PACIFIC MODULAR LLC | CARPET CLEANING | 315.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 313.98 |
| | 00201125 | PHILEN, SUZANNE | LIGHTS | 299.75 |
| | 00200831 | CENTURYLINK | PHONE USE NOV 2019 | 186.17 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 177.99 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 159.68 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 109.07 |
| | 00201125 | PHILEN, SUZANNE | SUPPLIES | 65.73 |
| P0105579 | 00200880 | LOLLIE FLEUR | Thrift shop front entry garden | 59.95 |
| | 00201125 | PHILEN, SUZANNE | SUPPLIES | 55.14 |
| | 00201174 | US BANK CORP PAYMENT SYS | Advertising | 53.91 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 32.60 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 29.64 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 18.22 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 9.99 |
| Org Key: YF2100 - School/City Partnership | | | | |
| P0105739 | 00201173 | NEVADA SYSTEM OF HIGHER EDU | WA State Prevention Summit | 250.00 |
| P0102231 | 00200919 | REGIONAL TOXICOLOGY SERVICES | Lab fees for C.Harnish clients | 61.00 |
| P0102231 | 00201134 | REGIONAL TOXICOLOGY SERVICES | Lab fees for C.Harnish clients | 47.20 |
| Org Key: YF2300 - VOICE Program | | | | |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 240.00 |
| Org Key: YF2600 - Family Assistance | | | | |
| P0105706 | 00200836 | CREST APARTMENT LLC | Partial rent payment for Emerg | 1,000.00 |
| P0105705 | 00200967 | ESSEX PORTFOLIO L.P. | Partial rent payment for Emerg | 1,000.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | Misc-Emergency Assistance | 1,000.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | Misc-Emergency Assistance | 1,000.00 |
| P0102245 | 00200908 | PEBBLE @ MIPC, THE | Miscellaneous-Scholarships | 555.15 |
| P0102990 | 00200835 | CREATIVE LEARNING CENTER | Preschool scholarship tuition | 533.32 |
| | 00201174 | US BANK CORP PAYMENT SYS | Misc-Emergency Assistance | 500.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | Misc-Emergency Assistance | 163.09 |
| | 00201174 | US BANK CORP PAYMENT SYS | Misc-Emergency Assistance | 109.50 |
| | 00201174 | US BANK CORP PAYMENT SYS | Misc-Emergency Assistance | 50.77 |
| | 00201174 | US BANK CORP PAYMENT SYS | Misc-Emergency Assistance | 36.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | Misc-Emergency Assistance | 28.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | Utility bill for an Emergency | 25.26 |
| Org Key: YF2800 - Fed Drug Free Communities Gran | | | | |
| P0105739 | 00201173 | NEVADA SYSTEM OF HIGHER EDU | WA State Prevention Summit | 250.00 |
| | 00201174 | US BANK CORP PAYMENT SYS | Operating Supplies | 158.40 |
| | 00201174 | US BANK CORP PAYMENT SYS | PR & YFS digital marketing | 68.75 |
| Org Key: YF2900 - Trauma Informed Approach Grant | | | | |
| P0105883 | 00200956 | ALBA PRO PAINTING LLC | PATCHING & PAINTING LB ADMIN B | 8,500.00 |
| | 00200847 | FALSGRAF NELSON, LAURA | REPLACE WARRANT 200536 | 4,566.83 |

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| | 00201174 | US BANK CORP PAYMENT SYS | Tuition & Registrations | 439.98 |
| | | | Total | <u>1,944,450.84</u> |

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|----------|------------|---|----------|------------------|--------------|--------------|
| 00200818 | 11/14/2019 | AG ENTERPRISE SUPPLY INC DEICE MOUNT | P0105316 | INV037600 | 10/24/2019 | 6,683.60 |
| 00200819 | 11/14/2019 | ALPINE PRODUCTS INC INVENTORY PURCHASES | P0105556 | TM190299 | 10/11/2019 | 4,081.50 |
| 00200820 | 11/14/2019 | AM TEST INC Testing of water at Luther swi | P0105594 | 112124 | 09/13/2019 | 240.00 |
| 00200821 | 11/14/2019 | ANCHOR QEA LLC Luther Burbank Irrigation Inta | P0104854 | 64542 | 10/09/2019 | 1,925.00 |
| 00200822 | 11/14/2019 | APPLIED PROFESSIONAL SERVICES Clarke Beach Utility locates | P0105315 | 112563 | 10/08/2019 | 270.00 |
| 00200823 | 11/14/2019 | AUTONATION INC FL-0506 & FL-0462 REPAIR PARTS | P0105667 | 140638/141460 | 09/03/2019 | 939.70 |
| 00200824 | 11/14/2019 | AXIS SURVEY & MAPPING 9600 WATER SYSTEM IMPROVEMENT | P0105310 | 13748 | 09/24/2019 | 5,140.00 |
| 00200825 | 11/14/2019 | BELLEVUE, CITY OF 2019 Specialized Recreation Se | P0102737 | 1002019 | 09/16/2019 | 1,187.00 |
| 00200826 | 11/14/2019 | BEST PARKING LOT CLEANING INC 2019 ON CALL STORMWATER CCTV | P0103471 | V183983 | 10/01/2019 | 4,630.30 |
| 00200827 | 11/14/2019 | BLUE PEAK LOGIC INC Skills Manager Software and FT | P0105582 | 1014191 | 10/14/2019 | 1,474.00 |
| 00200828 | 11/14/2019 | BRANOM OPERATING COMPANY LLC MAG METER TESTING | P0105545 | INV2019335355 | 10/10/2019 | 990.00 |
| 00200829 | 11/14/2019 | BUILDERS EXCHANGE OF WA LIFT STATION 11 BID | P0105570 | 1063754 | 10/10/2019 | 45.00 |
| 00200830 | 11/14/2019 | CASCADE COLUMBIA DISTRIBUTION SODIUM HYPOCHLORITE | P0105643 | 763143 | 10/17/2019 | 159.50 |
| 00200831 | 11/14/2019 | CENTURYLINK PHONE USE NOV 2019 | | OH012562 | 11/01/2019 | 1,294.43 |
| 00200832 | 11/14/2019 | CINTAS CORPORATION #460 Weekly floor mat cleaning at L | P0102521 | 4031878990/40324 | 10/07/2019 | 295.30 |
| 00200833 | 11/14/2019 | COMCAST Internet Charges/Fire | P0102461 | OH012561 | 11/07/2019 | 470.28 |
| 00200834 | 11/14/2019 | COMMERCIAL LANDSC SUPPLY INC INVENTORY PURCHASES | P0105628 | 207641 | 10/23/2019 | 146.38 |
| 00200835 | 11/14/2019 | CREATIVE LEARNING CENTER Preschool scholarship tuition | P0102990 | 7904/7924 | 10/22/2019 | 533.32 |
| 00200836 | 11/14/2019 | CREST APARTMENT LLC Partial rent payment for Emerg | P0105706 | OH012591 | 11/05/2019 | 1,000.00 |
| 00200837 | 11/14/2019 | CROP, EDITH L 1901-193 Refund deposit for wa | P0105537 | 1901193 | 10/17/2019 | 244.77 |
| 00200838 | 11/14/2019 | CULTURES CONNECTING LLC Cultural competence workshop | P0105625 | 2915 | 10/11/2019 | 1,800.00 |
| 00200839 | 11/14/2019 | DANIEL, KAMARIA MITV 10/15 City Council Mtg | P0105743 | 31 | 11/05/2019 | 1,000.00 |
| 00200840 | 11/14/2019 | DATEC INC. 5 MDC Mounts for Patrol Cars | P0105052 | 34357 | 10/25/2019 | 1,373.63 |
| 00200841 | 11/14/2019 | DEPT OF TRANSPORTATION WSDOT Monitoring Well - Invoic | P0105578 | RE41JZ0405L004 | 10/14/2019 | 36.50 |
| 00200842 | 11/14/2019 | DIRECT MATTERS Direct Matters invoice #55910 | P0105563 | 55913 | 10/19/2019 | 1,679.66 |
| 00200843 | 11/14/2019 | DUNN LUMBER COMPANY INVENTORY PURCHASES | P0105557 | 6731453 | 10/18/2019 | 275.66 |

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|----------|------------|--|----------|---------------|--------------|--------------|
| 00200844 | 11/14/2019 | EARTHCORPS INC 2019-2020 Forest Restoration | P0102736 | 7615 | 09/30/2019 | 5,307.50 |
| 00200845 | 11/14/2019 | ELSOE, RONALD LEOFF1 Retiree Medical Expense | P0105741 | OH012595 | 11/07/2019 | 52.19 |
| 00200846 | 11/14/2019 | EXCEL SUPPLY COMPANY INVENTORY PURCHASES | P0105657 | 111032 | 10/28/2019 | 61.12 |
| 00200847 | 11/14/2019 | FALSGRAF NELSON, LAURA REPLACE WARRANT 200536 | | OH012563 | 11/12/2019 | 4,566.83 |
| 00200848 | 11/14/2019 | FERGUSON ENTERPRISES LLC SENSUS SOFTWARE FLEXNET SUPPOR | P0105639 | 0819029 | 10/14/2019 | 3,110.53 |
| 00200849 | 11/14/2019 | FIRST STUDENT INC Invoice #9308976 Gene Coulon P | P0105600 | 9308976 | 07/31/2019 | 503.13 |
| 00200850 | 11/14/2019 | FREE WHEELCHAIR MISSION Rental FA-2251 completed. Retu | P0105585 | FA2251 | 10/22/2019 | 350.00 |
| 00200851 | 11/14/2019 | GRAINGER INVENTORY PURCHASES | P0105542 | 9318302628 | 10/09/2019 | 1,115.07 |
| 00200852 | 11/14/2019 | GRAND & BENEDICTS INC Operating supplies for the Thr | P0102239 | 1044746IN | 07/29/2019 | 690.82 |
| 00200853 | 11/14/2019 | GREGERSON, ROBERT Visual Arts Event - MI Arts Co | P0105702 | 231 | 10/31/2019 | 150.00 |
| 00200854 | 11/14/2019 | H D FOWLER 12" FLANGED CHECK VALVE & RETA | P0105612 | I5311994 | 10/18/2019 | 3,786.55 |
| 00200855 | 11/14/2019 | HACH COMPANY REAGENT SET, CHLORINE FREE | P0105510 | 11675395 | 10/10/2019 | 300.17 |
| 00200856 | 11/14/2019 | HAGSTROM, JAMES FRLEOFF1 Retiree Medical Expen | P0105742 | OH012596 | 11/07/2019 | 217.22 |
| 00200857 | 11/14/2019 | HARB, SAM FLEX SPEND REIMB | | 8-NOV-19 | 11/08/2019 | 569.86 |
| 00200858 | 11/14/2019 | HARNISH, CHRISTOPHER JAMES CREDENTIAL RENEWAL | | OH012564 | 11/12/2019 | 342.50 |
| 00200859 | 11/14/2019 | HERRERA ENVIRONMENTAL CONSULT PROFESSIONAL SERVICES FOR MERC | P0104079 | 45313 | 10/22/2019 | 5,331.86 |
| 00200860 | 11/14/2019 | HOBBS, TRACY October Power Flex classes #94 | P0105716 | 9482 | 11/06/2019 | 1,863.92 |
| 00200861 | 11/14/2019 | HOME DEPOT CREDIT SERVICE INVENTORY PURCHASES | P0105550 | 0186112026253 | 10/18/2019 | 569.93 |
| 00200862 | 11/14/2019 | HONEYWELL, MATTHEW V Professional Services - Public | P0105610 | 1132 | 10/22/2019 | 1,410.00 |
| 00200863 | 11/14/2019 | IBS INC MISC. HARDWARE (WAREHOUSE) | P0105613 | 7140441 | 10/18/2019 | 88.67 |
| 00200864 | 11/14/2019 | ICF WASHINGTON STATE REPLACE WARRANT 198806 | | OH012566 | 10/28/2019 | 550.00 |
| 00200865 | 11/14/2019 | INTERCOM LANGUAGE SERVICES INC Intercom invoice 19-358 - 10/1 | P0105558 | 19358 | 10/21/2019 | 260.00 |
| 00200866 | 11/14/2019 | INTERSTATE BATTERY SYSTEMS BATTERY INVENTORY | P0105664 | 647492 | 09/23/2019 | 396.94 |
| 00200867 | 11/14/2019 | ISSAQUAH HONDA KUBOTA CARBURETOR & FUEL LINE | P0105675 | 108121 | 10/31/2019 | 76.45 |
| 00200868 | 11/14/2019 | JENSEN, JOHN Talk for MICEC Luncheon - seni | P0105635 | OH012572 | 10/31/2019 | 200.00 |
| 00200869 | 11/14/2019 | JOHNSON, CURTIS FRLEOFF1 Retiree Medical Expen | P0105631 | OH012571 | 10/28/2019 | 369.49 |

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|----------|------------|---|----------|------------------|--------------|--------------|
| 00200870 | 11/14/2019 | JOHNSON, JASON Returning deposit. FA-3737 | P0105630 | FA3737 | 10/28/2019 | 75.00 |
| 00200871 | 11/14/2019 | KBA INC SCADA SYSTEM CONSTRUCTION | P0104206 | 3004810 | 10/23/2019 | 9,712.19 |
| 00200872 | 11/14/2019 | KELLEY, CHRIS M FLEX SPEND REIMB | | 8-NOV-19 | 11/08/2019 | 1,194.75 |
| 00200873 | 11/14/2019 | KIA MOTORS FINANCE PW 2019 KIA NIRO PHEV LEASE | P0105794 | OH012574 | 11/03/2019 | 388.20 |
| 00200874 | 11/14/2019 | KING COUNTY FINANCE SIGNAL SERVICES | P0105571 | 101163101163 | 09/30/2019 | 128.42 |
| 00200875 | 11/14/2019 | KING COUNTY-DNRP 2019 K4C Annual Contribution | P0105780 | OH012597 | 08/02/2019 | 1,200.00 |
| 00200876 | 11/14/2019 | KOJIMA, KENTARO Instructor Payment for Soapsto | P0105623 | 9618 | 10/28/2019 | 180.00 |
| 00200877 | 11/14/2019 | KROESENS UNIFORM COMPANY Patrol Hat - Robinson - Invoic | P0105645 | 57945/57944/5794 | 10/24/2019 | 1,759.55 |
| 00200878 | 11/14/2019 | LAKESIDE INDUSTRIES INVENTORY PURCHASES | P0105598 | 104978 | 10/18/2019 | 1,745.70 |
| 00200879 | 11/14/2019 | LOISEAU, LERI M LEOFF1 Retiree Medical Expense | P0105636 | OH012575 | 10/29/2019 | 167.68 |
| 00200880 | 11/14/2019 | LOLLIE FLEUR Thrift shop front entry garden | P0105579 | 2019MITS01 | 09/17/2019 | 59.95 |
| 00200881 | 11/14/2019 | MAGNAS LLC MONTHLY LONG DISTANCE JAN-DEC | P0102376 | OH012576 | 10/31/2019 | 206.00 |
| 00200882 | 11/14/2019 | MANDEVILLE, TROY MILEAGE EXPENSE | | OH012547 | 10/28/2019 | 138.04 |
| 00200883 | 11/14/2019 | MANRIQUEZ, CHERYL R FLEX SPEND REIMB | | 8-NOV-19 | 11/08/2019 | 129.64 |
| 00200884 | 11/14/2019 | MARTEN LAW Professional Services - Invoic | P0105581 | 44089496 | 10/15/2019 | 10,799.91 |
| 00200885 | 11/14/2019 | MARTIN, ERIC MILEAGE EXPENSE | | OH012568 | 11/05/2019 | 185.60 |
| 00200886 | 11/14/2019 | MERCER ISLAND GUILD OF Classified | P0105595 | 13419 | 10/13/2019 | 115.00 |
| 00200887 | 11/14/2019 | METROPRESORT 2019 3RD QUARTER B&O TAX STATE | P0103098 | IN615466 | 10/22/2019 | 2,662.57 |
| 00200888 | 11/14/2019 | MI CHAMBER OF COMMERCE MONTHLY BILLING FOR SERVICES | P0102377 | OH012579 | 10/31/2019 | 1,200.00 |
| 00200889 | 11/14/2019 | MI FOOTBALL BOOSTER CLUB Rental FA-1962 completed. Retu | P0105584 | FA1962 | 10/22/2019 | 425.00 |
| 00200890 | 11/14/2019 | MI SCHOOL DISTRICT #400 MI Pool Operation Subsidy | P0102579 | OH012598 | 10/31/2019 | 11,611.88 |
| 00200891 | 11/14/2019 | MI UTILITY BILLS PAYMENT FOR WATER SERVICE FOR | P0105670 | OH012581 | 10/31/2019 | 12,986.53 |
| 00200892 | 11/14/2019 | MISD FOOD SERVICE 120 meals for senior social lu | P0105626 | OH012578 | 09/30/2019 | 600.00 |
| 00200893 | 11/14/2019 | MOHAN SKIING AND BOARDING Returning deposit. Rental FA-3 | P0105629 | FA3702 | 10/28/2019 | 75.00 |
| 00200894 | 11/14/2019 | MONARCH LANDSCAPING WA LLC 2019 ROW PLANT WATERING & | P0103898 | CD50039882 | 08/30/2019 | 3,096.73 |
| 00200895 | 11/14/2019 | MONTAGUE, LIANA PARKING FEES | | OH012567 | 11/12/2019 | 20.24 |

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| 00200896 | 11/14/2019 | MULTICARE IMMEDIATE CLINIC J.JOHNSON HEP B VACCINE | P0105572 | 10358 | 10/01/2019 | 199.00 |
| 00200897 | 11/14/2019 | MYERS, JAMES S LEOFF1 Retiree Medical Expense | P0105633 | OH012577 | 10/28/2019 | 120.35 |
| 00200898 | 11/14/2019 | NATIONAL BUSINESS SYSTEMS 2 Inch Label Printer | P0105060 | 94766 | 09/12/2019 | 1,562.00 |
| 00200899 | 11/14/2019 | NATIONAL CONST RENTALS INC Seafair Bathroom Rental - Invo | P0105653 | 5453638 | 07/31/2019 | 400.00 |
| 00200900 | 11/14/2019 | NEWPORT ACADEMY Rental FA-3590 cancelled. Retu | P0105589 | FA3590 | 10/22/2019 | 752.00 |
| 00200901 | 11/14/2019 | NORTH LAKE MARINA- MARINE FUEL | P0105665 | 32073/32071/3129 | 06/17/2019 | 1,255.83 |
| 00200902 | 11/14/2019 | NORTON CORROSION LIMITED LLC CATHODIC POST INSTALLATION | P0105668 | 255458 | 10/18/2019 | 1,229.00 |
| 00200903 | 11/14/2019 | OGDEN MURPHY WALLACE PLLC Professional Services - Invoic | P0105608 | 830457 | 10/08/2019 | 6,889.50 |
| 00200904 | 11/14/2019 | OLSON, JOHN R Pro Tem Judge 10/22/19 - 5.5 h | P0105620 | OH012582 | 10/22/2019 | 275.00 |
| 00200905 | 11/14/2019 | OLYMPIC ENVIRONMENTAL RES 2019 ANNUAL RECYCLE EVENTS | P0103107 | 2019OER4 | 10/24/2019 | 10,811.23 |
| 00200906 | 11/14/2019 | PACIFIC GOLF & TURF FL-0467 | P0105331 | 0284393 | 10/11/2019 | 1,282.97 |
| 00200907 | 11/14/2019 | PART WORKS INC., THE CAP SCREWS & BUBBLER CARTRIDGE | P0105656 | INV48725 | 10/24/2019 | 125.94 |
| 00200908 | 11/14/2019 | PEBBLE @ MIPC, THE Miscellaneous-Scholarships | P0102245 | OH012583 | 10/22/2019 | 555.15 |
| 00200909 | 11/14/2019 | PETERSEN, CHRIS FLEX SPEND REIMB | | 8-NOV-19 | 11/08/2019 | 4,423.13 |
| 00200910 | 11/14/2019 | PND ENGINEERS INC Lincoln Landing Design Enginee | P0095391 | 1910183/17110217 | 07/17/2019 | 2,499.49 |
| 00200911 | 11/14/2019 | PRITCHARD, JOHN K REPLACE WARRANT #200681 | | OH012569 | 11/12/2019 | 36.80 |
| 00200912 | 11/14/2019 | PRO-VAC 81ST BACKYARD SEWER PUNCHLIST | P0105662 | 52922 | 08/20/2019 | 7,095.00 |
| 00200913 | 11/14/2019 | PSIS Rental FA-3110 completed. Retu | P0105587 | FA3110 | 10/22/2019 | 250.00 |
| 00200914 | 11/14/2019 | PUGET SOUND ENERGY ENERGY USE NOV 2019 | | OH012570 | 11/01/2019 | 17,343.13 |
| 00200915 | 11/14/2019 | RAINIER BUILDING SERVICES FLOOR WAXING CITY BUILDINGS | P0105576 | 18977 | 10/14/2019 | 1,900.00 |
| 00200916 | 11/14/2019 | RAMSAY, JON FRLEOFF1 Retiree Medical Expen | P0105632 | OH012584 | 10/28/2019 | 60.72 |
| 00200917 | 11/14/2019 | RAUL, ALONSO RE-ISSUE STALE DATED PAYROLL C | P0105564 | OH012585 | 10/07/2019 | 450.27 |
| 00200918 | 11/14/2019 | REDMOND, CITY OF 3rd Qtr 2019 App Maint. | P0105767 | 00002931 | 10/10/2019 | 10,639.99 |
| 00200919 | 11/14/2019 | REGIONAL TOXICOLOGY SERVICES Lab fees for C.Hamish clients | P0102231 | TC20290093019 | 09/30/2019 | 61.00 |
| 00200920 | 11/14/2019 | REMOTE SATELLITE SYSTEMS INT'L Sat Phone Service - EMAC - Inv | P0105648 | 00103194 | 10/09/2019 | 54.00 |
| 00200921 | 11/14/2019 | REPUBLIC SERVICES #172 5500 ICW RECYCLE CONTAINER | P0105605 | 0172007992015 | 09/30/2019 | 1,174.59 |

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| 00200922 | 11/14/2019 | RICOH USA INC (FIRE) Copier Rental/Fire | P0105755 | 102862166 | 10/22/2019 | 278.52 |
| 00200923 | 11/14/2019 | RON'S STUMP REMOVAL & 10% Retainage - MI Summer 2019 | P0104891 | OH012586 | 10/29/2019 | 3,527.70 |
| 00200924 | 11/14/2019 | ROSEMOUNT INC SHIPPING CHARGES | P0105614 | 908218 | 10/16/2019 | 14.86 |
| 00200925 | 11/14/2019 | S&B INC FIRST HILL PRESSURE STATIONS | P0105663 | 25347A | 10/25/2019 | 1,184.70 |
| 00200926 | 11/14/2019 | SAFETY FLAG CO OF AMERICA INVENTORY PURCHASES | P0105511 | 74628 | 10/09/2019 | 144.00 |
| 00200927 | 11/14/2019 | SCORE September Housing - Invoice # | P0105649 | 4027 | 10/10/2019 | 1,080.00 |
| 00200928 | 11/14/2019 | SHERWIN-WILLIAMS CO, THE KIT, PUMP, ST, THREA | P0105674 | 40981 | 10/31/2019 | 267.37 |
| 00200929 | 11/14/2019 | SOUND SAFETY PRODUCTS MISC. WORK CLOTHES | P0105659 | 1388033 | 10/19/2019 | 775.91 |
| 00200930 | 11/14/2019 | SPIETZ, ALLISON FLEX SPEND REIMB | | 8-NOV-19 | 11/08/2019 | 35.57 |
| 00200931 | 11/14/2019 | STORM LAKE GROWERS INC Plants for OSV | P0105655 | 19833 | 10/15/2019 | 3,906.11 |
| 00200932 | 11/14/2019 | SUPERION LLC ONESOLUTION ANNUAL MAINT | P0105622 | 254754 | 10/10/2019 | 11,392.75 |
| 00200933 | 11/14/2019 | SUPPLY SOURCE INC,THE INVENTORY PURCHASES | P0105619 | 1904446 | 10/22/2019 | 1,006.74 |
| 00200934 | 11/14/2019 | TADPOLE HAVEN NATIVE PLANTS Plants for OSV | P0105654 | 717 | 10/16/2019 | 1,350.25 |
| 00200935 | 11/14/2019 | THOMSON REUTERS - WEST Invoice No. 841120761 - Librar | P0105652 | 841080534 | 10/01/2019 | 1,291.53 |
| 00200936 | 11/14/2019 | UNITED SITE SERVICES Lid Park Boat Launch - | P0102825 | 1149317073/11493 | 10/18/2019 | 786.27 |
| 00200937 | 11/14/2019 | VERIZON WIRELESS CM Phone & Hotspot | P0102717 | 9840715976 | 10/23/2019 | 2,988.49 |
| 00200938 | 11/14/2019 | VERIZON WIRELESS MDC Charges/Fire | P0102229 | 9840715981 | 10/23/2019 | 1,809.49 |
| 00200939 | 11/14/2019 | WA ST TREASURER'S OFFICE OCT19 REMIT NC COURT TRANSMITT | P0105700 | OH012589 | 10/31/2019 | 18,129.03 |
| 00200940 | 11/14/2019 | WALTER E NELSON CO INVENTORY PURCHASES | P0105627 | 729751 | 10/28/2019 | 1,450.64 |
| 00200941 | 11/14/2019 | WEATHERNET LLC NOVEMBER WEATHERNET | P0105606 | 201915196B | 10/18/2019 | 735.00 |
| 00200942 | 11/14/2019 | WEGNER, KEN LEOFF1 Retiree Medical Expense | P0105567 | OH012587 | 10/21/2019 | 222.01 |
| 00200943 | 11/14/2019 | WEST MARINE PRO MP Supplies - Invoice # 7371 | P0105650 | 7371 | 10/16/2019 | 17.03 |
| 00200944 | 11/14/2019 | WHISTLE WORKWEAR MISC. WORK CLOTHES | P0105597 | 191859 | 10/10/2019 | 482.46 |
| 00200945 | 11/14/2019 | WHISTLE WORKWEAR MISC. WORK CLOTHES | P0105596 | 164387 | 10/05/2019 | 109.70 |
| 00200946 | 11/14/2019 | WILSON CONCRETE CONST INC GLENHOME PROJECT | P0105669 | 9920 | 08/23/2019 | 2,145.00 |
| 00200947 | 11/14/2019 | YOUTH THEATER NORTHWEST Season Sponsor | P0105644 | 30955 | 10/22/2019 | 2,500.00 |

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| 00200948 | 11/14/2019 | YU, BENJAMIN Returning remaining credit fro | P0105588 | OH012590 | 10/22/2019 | 17.25 |
| 00200949 | 11/14/2019 | ZEE MEDICAL First Aid Supplies - Invoice # | P0105647 | 6835352 | 10/01/2019 | 156.22 |
| 00200950 | 11/25/2019 | 29 ELEVEN INC Name Tags for staff | P0105822 | 203993 | 11/04/2019 | 532.95 |
| 00200951 | 11/25/2019 | ABBOTT, RICHARD LEOFF1 Medicare Reimb | | DEC2019B | 11/20/2019 | 151.60 |
| 00200952 | 11/25/2019 | ACCESS Final Invoice | P0105829 | 7769209 | 10/31/2019 | 273.16 |
| 00200953 | 11/25/2019 | ADAMS, RONALD E LEOFF1 Retiree Medical Expense | | DEC2019B | 11/20/2019 | 266.20 |
| 00200954 | 11/25/2019 | AIRGAS USA LLC Oxygen/Fire | P0105868 | 9966222410/90954 | 10/31/2019 | 64.42 |
| 00200955 | 11/25/2019 | COOPER ZIETZ ENGINEERS INC On call electrical inspector | P0105886 | 190320028 | 11/08/2019 | 14,603.42 |
| 00200956 | 11/25/2019 | ALBA PRO PAINTING LLC PATCHING & PAINTING LB ADMIN | P0105883 | #1 | 10/15/2019 | 19,328.40 |
| 00200957 | 11/25/2019 | ALPINE PRODUCTS INC MARKER PADS & MARKERS | P0105733 | TM190793 | 11/01/2019 | 3,762.00 |
| 00200958 | 11/25/2019 | AM TEST INC STORMWATER SAMPLES | P0105847 | 113139 | 11/15/2019 | 5,335.00 |
| 00200959 | 11/25/2019 | AMERICAN WATER WORKS ASSOC 2019-2020 DUES | P0105849 | 7001713549 | 08/22/2019 | 2,139.00 |
| 00200960 | 11/25/2019 | AMERIGAS-1400 2019 PROPANE DELIVERY | P0102654 | 3098766269 | 11/13/2019 | 485.32 |
| 00200961 | 11/25/2019 | CHIP GEORGE INC 3 Pump Station Cellular Deploy | P0105692 | 1033 | 11/04/2019 | 7,108.22 |
| 00200962 | 11/25/2019 | ASPECT CONSULTING LLC Invoice No. 35021ST Long Term | P0105906 | 35021 | 11/13/2019 | 8,648.43 |
| 00200963 | 11/25/2019 | ASPECT SOFTWARE INC Telestaff Monthly Charges | P0105756 | ASI054962 | 11/05/2019 | 165.00 |
| 00200964 | 11/25/2019 | ASTRAL COMMUNICATIONS INC Cradlepoint Netcloud | P0105163 | 175082 | 10/28/2019 | 4,307.08 |
| 00200965 | 11/25/2019 | AT&T MOBILITY TELEMETRY - OCT | P0105850 | 10132019/1113201 | 10/05/2019 | 243.45 |
| 00200966 | 11/25/2019 | AUGUSTSON, THOR LEOFF1 Retiree Medical Expense | | DEC2019B | 11/20/2019 | 225.06 |
| 00200967 | 11/25/2019 | ESSEX PORTFOLIO L.P. Partial rent payment for Emerg | P0105705 | OH012617 | 11/05/2019 | 1,000.00 |
| 00200968 | 11/25/2019 | AXIS SURVEY & MAPPING 9600 WATER SYSTEM IMPROVEMENT | P0105310 | 13881 | 10/29/2019 | 20,560.00 |
| 00200969 | 11/25/2019 | AXON ENTERPRISE INC Taser Holsters - Invoice # | P0105805 | SI1618926 | 10/24/2019 | 343.20 |
| 00200970 | 11/25/2019 | BARK KING 2019 Engineered Wood Fiber | P0104695 | 13320/13290/1326 | 08/12/2019 | 12,375.00 |
| 00200971 | 11/25/2019 | BARNES, WILLIAM LEOFF1 Medicare Reimb | | DEC2019A | 11/20/2019 | 6,703.62 |
| 00200972 | 11/25/2019 | BELLEVUE, CITY OF January 2019 Academy/Edwards | P0105797 | 35165 | 08/07/2019 | 19,119.71 |
| 00200973 | 11/25/2019 | BELLEVUE, CITY OF 2019 Quarterly MBP.Com Fees | P0102860 | 35563 | 10/31/2019 | 9,978.93 |

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| 00200974 | 11/25/2019 | BERK CONSULTING November Invoice for work on | P0105736 | 103571019F | 11/04/2019 | 632.50 |
| 00200975 | 11/25/2019 | BEST PARKING LOT CLEANING INC 2019 ON CALL STORMWATER CCTV | P0103471 | VI84746/V184894 | 10/24/2019 | 5,951.99 |
| 00200976 | 11/25/2019 | BLUELINE GROUP 9600 WMW WATER SYSTEM IMPROVEM | P0105308 | 17817 | 10/31/2019 | 15,520.75 |
| 00200977 | 11/25/2019 | BOOTH, GLENDON D LEOFF1 Medicare Reimb | | DEC2019B | 11/20/2019 | 168.00 |
| 00200978 | 11/25/2019 | BRAKE AND CLUTCH SUPPLY NORTH FL-0395 REPAIR PARTS | P0105870 | 85314 | 10/10/2019 | 694.50 |
| 00200979 | 11/25/2019 | BROWN AND CALDWELL CONSULTANTS PH1 SCADA EQUIPMENT REPLACEMEN | SP0103284 | 14353089 | 09/20/2019 | 71,129.45 |
| 00200980 | 11/25/2019 | BUILDERS HARDWARE & SUPPLY CO. COURT SECURITY LOCKS LABOR | P0105726 | S3723783002 | 10/16/2019 | 990.00 |
| 00200981 | 11/25/2019 | CALIBER - BELLEVUE FL-0435 BODY DAMAGE REPAIR | P0105604 | 2901000693 | 10/23/2019 | 957.73 |
| 00200982 | 11/25/2019 | CALLAGHAN, MICHAEL LEOFF1 Medicare Reimb | | DEC2019B | 11/20/2019 | 164.40 |
| 00200983 | 11/25/2019 | CAPITOL PACIFIC REPORTING Transcript of Hearing - Owners | P0105782 | 19143TJ | 10/31/2019 | 641.25 |
| 00200984 | 11/25/2019 | CARLSON, LARRY QTRLY LEOFF1 Medicare Reimb | | DECTHRUFEB | 11/20/2019 | 406.50 |
| 00200985 | 11/25/2019 | CAROLLO ENGINEERS INC CHLORINATION SYSSTEN OPTIMIZAT | P0103848 | 0181387 | 10/15/2019 | 1,006.50 |
| 00200986 | 11/25/2019 | CARQUEST AUTO PARTS STORES FLEET REPAIR PARTS | P0105718 | OH012607 | 10/31/2019 | 267.79 |
| 00200987 | 11/25/2019 | CEDAR GROVE COMPOSTING INC Organic Waste Service 10/2019 | P0105740 | 0000488678 | 10/31/2019 | 86.10 |
| 00200988 | 11/25/2019 | CENTURYLINK BUSINESS SERVICES PHONE USE NOV 2019 | | 1479783452 | 11/03/2019 | 3,740.28 |
| 00200989 | 11/25/2019 | CESSCO INC INVENTORY PURCHASES | P0105696 | 11934 | 10/25/2019 | 357.92 |
| 00200990 | 11/25/2019 | CHECK RIDE DRIVER TRAINING CDL TRAINING WATER TEAM | P0104977 | OH012606 | 10/28/2019 | 10,350.00 |
| 00200991 | 11/25/2019 | CHOW, PHILLIP REFUND DEPOSIT FOR WATER SERVI | P0105681 | 1710157 | 10/28/2019 | 468.12 |
| 00200992 | 11/25/2019 | CHS ENGINEERS LLC GENERATOR 13/17/24 AND PS 18 | P0102393 | 8017031910 | 10/25/2019 | 1,143.10 |
| 00200993 | 11/25/2019 | CINTAS CORPORATION #460 2019 COVERALL SERVICES | P0102911 | 4031625933/40321 | 10/31/2019 | 2,936.16 |
| 00200994 | 11/25/2019 | CLEANERS PLUS 1 Uniform Cleaning - Invoice # 7 | P0105803 | 76930 | 11/01/2019 | 91.79 |
| 00200995 | 11/25/2019 | CODE PUBLISHING CO City Code Cumulative Print | P0105897 | 65289 | 11/14/2019 | 900.90 |
| 00200996 | 11/25/2019 | COMCAST CITY HALL HIGH SPEED INTERNET | P0102379 | OH012626 | 11/12/2019 | 121.44 |
| 00200997 | 11/25/2019 | COMCAST 2019 PW WIFI CONNECTION | P0102657 | OH012629 | 11/07/2019 | 86.39 |
| 00200998 | 11/25/2019 | COMCAST Internet Charges/Fire | P0105866 | OH012628 | 11/11/2019 | 73.88 |
| 00200999 | 11/25/2019 | COMCAST FIRE STATION 92 FIBER CIRCUIT | P0102380 | 90832029 | 11/01/2019 | 1,288.42 |

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| 00201000 | 11/25/2019 | COMPLETE OFFICE OFFICE SUPPLIES OCT2019 | | OH012605 | 10/31/2019 | 2,362.88 |
| 00201001 | 11/25/2019 | CONFIDENTIAL DATA DISPOSAL City Shredding Services - Invo | P0105804 | 77086713 | 10/31/2019 | 325.00 |
| 00201002 | 11/25/2019 | CONFLUENCE ENGINEERING GRP LLC BOOSTER CHLORINATION OPERATION | P0104779 | 031019MIB COP | 11/01/2019 | 23,234.80 |
| 00201003 | 11/25/2019 | SAHANDY, SHEIDA REBECCA CPD Continuous Improvement Pro | P0105889 | 020/021 | 10/25/2019 | 10,375.00 |
| 00201004 | 11/25/2019 | COOPER, ROBERT LEOFF1 Excess Benefit | | DEC2019A | 11/20/2019 | 6,415.42 |
| 00201005 | 11/25/2019 | CORT PARTY RENTAL 10/31/19 Halloween Sr Lunch | P0105877 | 1114561 | 11/19/2019 | 705.32 |
| 00201006 | 11/25/2019 | CRYSTAL AND SIERRA SPRINGS PW WATER DELIVERY | P0103275 | 14555831110219 | 11/02/2019 | 149.40 |
| 00201007 | 11/25/2019 | CULLIGAN SEATTLE WA Water Service/Fire | P0105749 | 201911672721 | 10/31/2019 | 410.23 |
| 00201008 | 11/25/2019 | DAILY JOURNAL OF COMMERCE PS 11 GENERATOR | P0105830 | 3351701 | 09/18/2019 | 260.40 |
| 00201009 | 11/25/2019 | DATAQUEST LLC Background checks | P0105746 | 10406 | 10/31/2019 | 631.50 |
| 00201010 | 11/25/2019 | DEEDS, EDWARD G LEOFF1 Medicare Reimb | | DEC2019B | 11/20/2019 | 211.50 |
| 00201011 | 11/25/2019 | DEPT OF ENTERPRISES SERVICES Standard Specifications 2020 b | | 73193460 | 11/06/2019 | 267.86 |
| 00201012 | 11/25/2019 | DEPT OF HEALTH DOH SURVEY FEE | P0105717 | N03512 | 11/01/2019 | 1,122.00 |
| 00201013 | 11/25/2019 | DEVENY, JAN P LEOFF1 Medicare Reimb | | DEC2019B | 11/20/2019 | 168.00 |
| 00201014 | 11/25/2019 | DMCMA DMCMA - 2020 membership fee | P0105776 | OH012618 | 11/12/2019 | 150.00 |
| 00201015 | 11/25/2019 | DOWD, PAUL LEOFF1 Medicare Reimb | | DEC2019B | 11/20/2019 | 159.40 |
| 00201016 | 11/25/2019 | EASTSIDE EXTERMINATORS CH PEST EXTERMINATOR | P0105719 | 471799 | 11/01/2019 | 246.33 |
| 00201017 | 11/25/2019 | MACRAE, DANIEL J 2019 WATERCOURSE BANK STABALIZ | P0104255 | #2FINAL | 10/08/2019 | 110,224.80 |
| 00201018 | 11/25/2019 | EJ USA INC COMPOSITE MANHOLE RING & COVER | P0105728 | 110190089232/110 | 11/14/2019 | 1,100.39 |
| 00201019 | 11/25/2019 | ELSOE, RONALD LEOFF1 Retiree Medical Expense | | DEC2019B | 11/20/2019 | 278.41 |
| 00201020 | 11/25/2019 | EPSCA MONTHLY RADIO ACCESS FEES 49 R | P0102405 | 9859 | 11/05/2019 | 2,459.12 |
| 00201021 | 11/25/2019 | ESA Peer review for 1808-172 | P0105893 | 149919/150354/15 | 10/23/2019 | 15,439.20 |
| 00201022 | 11/25/2019 | EXCEL SUPPLY COMPANY INVENTORY PURCHASES | P0105787 | 111663 | 11/06/2019 | 356.64 |
| 00201023 | 11/25/2019 | FARALLON CONSULTING LLC PHASE 3 TECHNICAL SERVICES FOR | P0099477 | 0034243 | 11/06/2019 | 11,839.99 |
| 00201024 | 11/25/2019 | FERGUSON ENTERPRISES LLC INVENTORY PURCHASES | P0105698 | 0824412 | 10/29/2019 | 5,157.11 |
| 00201025 | 11/25/2019 | FIRE PROTECTION INC ADD LINE TO THE SECURITY PANEL | P0105723 | 53178 | 10/30/2019 | 652.30 |

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| 00201026 | 11/25/2019 | FORSMAN, LOWELL LEOFF1 Medicare Reimb | | DEC2019B | 11/20/2019 | 135.50 |
| 00201027 | 11/25/2019 | GARDEN CYCLES MI Open Space Restoration 2019 | P0103001 | P0103001 | 10/30/2019 | 1,649.00 |
| 00201028 | 11/25/2019 | GARDEN CYCLES Clarke Beach Park - Ivy Ring P | P0104698 | 204 | 10/31/2019 | 1,320.00 |
| 00201029 | 11/25/2019 | GET Washington PAYROLL EARLY WARRANTS | | NOV222019 | 11/22/2019 | 250.00 |
| 00201030 | 11/25/2019 | GOODMAN, J C LEOFF1 Medicare Reimb | | DEC2019B | 11/20/2019 | 194.20 |
| 00201031 | 11/25/2019 | GOODYEAR TIRE & RUBBER CO, THE TIRE INVENTORY | P0105879 | 1951152045 | 11/08/2019 | 1,961.20 |
| 00201032 | 11/25/2019 | GRAINGER INVENTORY PURCHASES | P0105685 | 9342922797 | 11/01/2019 | 3,213.10 |
| 00201033 | 11/25/2019 | GRAYBAR ELECTRIC CO POWER LINE PROTECTORS 120VAC | P0105824 | 9313099987 | 11/06/2019 | 267.49 |
| 00201034 | 11/25/2019 | GREENWOOD HEATING & A/C PERMIT REFUND | | 1911053 | 11/14/2019 | 47.86 |
| 00201035 | 11/25/2019 | H D FOWLER 3/8" BRASS PLUG | P0105790 | I5314136 | 10/22/2019 | 1,387.57 |
| 00201036 | 11/25/2019 | HABITAT RESTORATION SPEC LLC MI Open Space Restoration 2019 | P0103218 | 65 | 10/30/2019 | 1,470.00 |
| 00201037 | 11/25/2019 | HAGSTROM, JAMES LEOFF1 Medicare Reimb | | DEC2019B | 11/20/2019 | 216.50 |
| 00201038 | 11/25/2019 | HARB, SAM MILEAGE EXPENSE | | OH012600 | 11/14/2019 | 28.30 |
| 00201039 | 11/25/2019 | HARRIS COMPUTER SYSTEMS ICIS ANNUAL SUPPORT (INHNAE) | P0105831 | MN00124832 | 10/31/2019 | 34,311.77 |
| 00201040 | 11/25/2019 | HDR ENGINEERING INC WATER METER REPLACEMENT PROGRA | P0102980 | 1200227531 | 11/07/2019 | 7,666.19 |
| 00201041 | 11/25/2019 | HEALTHFORCE PARTNERS INC 2019 Flu/TB Shots | P0105811 | 15597 | 09/30/2019 | 2,918.75 |
| 00201042 | 11/25/2019 | HEARTLAND LLC October 2019 Long-Term Parking | P0105898 | 12101008 | 11/19/2019 | 12,512.50 |
| 00201043 | 11/25/2019 | HENDRICKS, YVONNE returning account credit | P0105715 | OH012608 | 11/06/2019 | 64.00 |
| 00201044 | 11/25/2019 | HILTNER, PETER LEOFF1 Medicare Reimb | | DEC2019B | 11/20/2019 | 556.00 |
| 00201045 | 11/25/2019 | HONEYWELL, MATTHEW V Professional Services - Invoic | P0105786 | 1134/1135 | 10/29/2019 | 4,200.00 |
| 00201046 | 11/25/2019 | HOUGH BECK & BAIRD Professional Services for Aubr | P0100545 | OH012630 | 11/01/2019 | 4,327.00 |
| 00201047 | 11/25/2019 | HOWISON, DEREK & KELLI OVERPAYMENT REFUND | | OH012604 | 11/18/2019 | 1.00 |
| 00201048 | 11/25/2019 | HYLAND SOFTWARE Professional Services | P0105672 | LE01123103 | 10/30/2019 | 333.25 |
| 00201049 | 11/25/2019 | IIMC IIMC Annual Dues - D. Estrada | P0105779 | OH012622 | 09/23/2019 | 210.00 |
| 00201050 | 11/25/2019 | IMS ALLIANCE Incident Command Worksheets | P0105859 | 192152 | 11/13/2019 | 1,333.00 |
| 00201051 | 11/25/2019 | IMS INFRASTRUCTURE MANGT SRVS MOBILE ASSET DATA COLLECTION F | P0105707 | 500315 | 10/31/2019 | 283.41 |

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| 00201052 | 11/25/2019 | INTERCOM LANGUAGE SERVICES INC Intercom invoice #19-370 | P0105815 | 19328 | 09/24/2019 | 666.25 |
| 00201053 | 11/25/2019 | INTERIOR FOLIAGE CO, THE CITY HALL INTERIOR LANDSCAPING | P0105828 | 42278 | 11/01/2019 | 282.70 |
| 00201054 | 11/25/2019 | ISSAQUAH HONDA KUBOTA Backpack blower, saw chain, | P0104903 | 110121/110971 | 11/05/2019 | 1,953.05 |
| 00201055 | 11/25/2019 | JOHNSON JR, MARV Instructor Payment for Basketb | P0105875 | 9650 | 11/19/2019 | 312.00 |
| 00201056 | 11/25/2019 | JOHNSON, CURTIS LEOFF1 Medicare Reimb | | DEC2019A | 11/20/2019 | 4,569.75 |
| 00201057 | 11/25/2019 | K&L GATES LLP Professional Services - Invoic | P0105904 | 3719335 | 11/07/2019 | 4,650.08 |
| 00201058 | 11/25/2019 | KAISER PERMANENTE Medical Testing/Collins | P0105766 | 68737509 | 11/01/2019 | 350.00 |
| 00201059 | 11/25/2019 | KAUFMAN, RON OVERPAYMENT REFUND | | OH012603 | 11/18/2019 | 885.46 |
| 00201060 | 11/25/2019 | KBA INC SCADA SYSTEM CONSTRUCTION | P0104206 | 3004844 | 11/13/2019 | 14,928.78 |
| 00201061 | 11/25/2019 | KC FINANCE Remit Quarterly Liquor Excise | P0103913 | 2120368 | 11/08/2019 | 1,702.17 |
| 00201062 | 11/25/2019 | KCFCA 2019 Leadership Seminar | P0105768 | 141 | 07/01/2019 | 200.00 |
| 00201063 | 11/25/2019 | KELLEY IMAGING SYSTEMS Waterproof paper for the plott | P0105844 | IN599114 | 11/07/2019 | 777.36 |
| 00201064 | 11/25/2019 | KIA MOTORS FINANCE DSG 2016 KIA SOUL LEASE | P94483 | OH012623 | 11/16/2019 | 221.93 |
| 00201065 | 11/25/2019 | KING COUNTY FINANCE Summer 2019 sample collection | P0102378 | 30028132 | 11/01/2019 | 402,200.98 |
| 00201066 | 11/25/2019 | KING COUNTY FINANCE I-NET MONTHLY SERVICES FROM | P0102965 | 11008484 | 10/31/2019 | 1,122.00 |
| 00201067 | 11/25/2019 | KING COUNTY FINANCE SIGNALS | P0105901 | 101695 | 10/31/2019 | 167.99 |
| 00201068 | 11/25/2019 | KING COUNTY FIRE CHIEFS Building Construction/Gruger | P0105861 | 7012 | 10/28/2019 | 450.00 |
| 00201069 | 11/25/2019 | KPG 80TH AVE SE PEDESTIAN IMPROVEM | P0104658 | 1016619 | 11/05/2019 | 18,300.00 |
| 00201070 | 11/25/2019 | KROESENS UNIFORM COMPANY Uniform Pants/Heitman | P0105808 | 58027 | 10/30/2019 | 773.05 |
| 00201071 | 11/25/2019 | KUHN, DAVID LEOFF1 Medicare Reimb | | DEC2019B | 11/20/2019 | 168.00 |
| 00201072 | 11/25/2019 | LANGUAGE LINE SERVICES Language Line Services - Telep | P0105796 | 4686668 | 10/03/2019 | 5.39 |
| 00201073 | 11/25/2019 | LEON, ANDREW PARKING FEE | | OH012621 | 11/15/2019 | 29.42 |
| 00201074 | 11/25/2019 | LES SCHWAB TIRE CENTER Replace Tires/7607 | P0105762 | 39400523473 | 11/06/2019 | 1,817.56 |
| 00201075 | 11/25/2019 | LIFE ASSIST INC Stock Aid Supplies | P0105799 | 954413 | 11/04/2019 | 9,308.16 |
| 00201076 | 11/25/2019 | LIGHTHOUSE LAW GROUP PLLC ST Long Term Commuter Parking/ | P0105783 | 20429 | 11/04/2019 | 7,050.00 |
| 00201077 | 11/25/2019 | LN CURTIS & SONS Supplies for New Engine | P0105759 | INV331420 | 10/17/2019 | 4,459.36 |

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| Check No | Check Date | Vendor Name/Description | PO # | Invoice # | Invoice Date | Check Amount |
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| 00201078 | 11/25/2019 | LOISEAU, LERI M LEOFF1 Medicare Reimb | | DEC2019B | 11/20/2019 | 162.10 |
| 00201079 | 11/25/2019 | LOLLIE FLEUR Lead design for Thrift Shop Ho | P0105823 | 2019TSHOP2 | 11/11/2019 | 396.00 |
| 00201080 | 11/25/2019 | LYONS, STEVEN LEOFF1 Medicare Reimb | | DEC2019B | 11/20/2019 | 134.30 |
| 00201081 | 11/25/2019 | MAC TOWING Impound - CS# 2019-7445 - Invo | P0105836 | 413497 | 08/07/2019 | 207.90 |
| 00201082 | 11/25/2019 | MARTEN LAW Professional Services - Invoic | P0105908 | 44089583 | 11/12/2019 | 14,282.09 |
| 00201083 | 11/25/2019 | MERCER ISLAND REPORTER Yearly Subscription | P0105826 | OH012624 | 11/08/2019 | 45.00 |
| 00201084 | 11/25/2019 | METROPRESORT OCTOBER 2019 PRINTING AND MAIL | P0103098 | IN616322/6145 | 11/01/2019 | 2,025.50 |
| 00201085 | 11/25/2019 | MI EMPLOYEES ASSOC PAYROLL EARLY WARRANTS | | NOV222019 | 11/22/2019 | 300.00 |
| 00201086 | 11/25/2019 | MI HARDWARE - BLDG MISC. HARDWARE FOR THE MONTH O | P0105709 | OH012611 | 10/31/2019 | 19.78 |
| 00201087 | 11/25/2019 | MI HARDWARE - MAINT MISC. HARDWARE FOR THE MONTH O | P0105695 | OH012610 | 10/31/2019 | 503.58 |
| 00201088 | 11/25/2019 | MI HARDWARE - P&R Misc P&R Supplies | P0105724 | OH012614 | 10/31/2019 | 108.54 |
| 00201089 | 11/25/2019 | MI HARDWARE - POLICE MP Supplies - Invoice Numbers: | P0105807 | 141330/349/386 | 10/31/2019 | 199.35 |
| 00201090 | 11/25/2019 | MI HARDWARE - ROW MISC. HARDWARE FOR THE MONTH O | P0105708 | OH012613 | 10/31/2019 | 53.42 |
| 00201091 | 11/25/2019 | MI HARDWARE - UTILITY MISC. HARDWARE FOR THE MONTH O | P0105710 | OH012612 | 10/31/2019 | 23.72 |
| 00201092 | 11/25/2019 | MI HARDWARE - YFS Operating supplies for Tshop a | P0102232 | OH012609 | 10/31/2019 | 38.96 |
| 00201093 | 11/25/2019 | MI SCHOOLS BAND BOOSTER Rental FA-2793 completed. Retu | P0105839 | FA2793 | 11/17/2019 | 500.00 |
| 00201094 | 11/25/2019 | MILLAD VI LLC REFUND DEPOSIT FROM WORK ORDER | P0105845 | 11965 | 10/16/2019 | 398.61 |
| 00201095 | 11/25/2019 | MOBERLY AND ROBERTS Professional Services - Invoic | P0105784 | 854 | 11/01/2019 | 6,800.00 |
| 00201096 | 11/25/2019 | MOE, JIM MILEAGE EXPENSE | | OH012599 | 10/25/2019 | 82.47 |
| 00201097 | 11/25/2019 | MORNICK, MATTHEW CMO/Finance Support 11/2-11/15 | P0105854 | #02 | 11/15/2019 | 3,740.00 |
| 00201098 | 11/25/2019 | MULTICARE IMMEDIATE CLINIC J. FELIX CDL PHYSICAL | P0105793 | 10527 | 11/01/2019 | 400.00 |
| 00201099 | 11/25/2019 | MUNICIPAL EMERGENCY SERVICES Hazmat Supplies | P0105770 | IN1385908/IN1386 | 10/10/2019 | 10,124.51 |
| 00201100 | 11/25/2019 | MURRAYSMITH INC RESERVOIR STANDBY DIESEL GENER | P0105058 | 192659001 | 11/13/2019 | 3,895.78 |
| 00201101 | 11/25/2019 | MYERS, JAMES S LEOFF1 Medicare Reimb | | DEC2019B | 11/20/2019 | 137.40 |
| 00201102 | 11/25/2019 | NATIONAL BUSINESS SYSTEMS Dock for POS Tablet | P0105688 | 95818 | 11/04/2019 | 288.42 |
| 00201103 | 11/25/2019 | NELSON PETROLEUM UNLEADED DELIVERY | P0105810 | 0714506IN | 11/04/2019 | 4,840.35 |

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| 00201104 | 11/25/2019 | NELSON TOOL & EQUIPMENT LLC FIRE STATION UNLEADED/DIESEL | P0105874 | 0715331IN | 11/15/2019 | 3,297.48 |
| 00201105 | 11/25/2019 | NORCOM 911 POLICE DISPATCH 2019 | P0102480 | 0000743 | 10/01/2019 | 154,193.75 |
| 00201106 | 11/25/2019 | NORCOM 911 FIRE DISPATCH 2019 | P0102479 | 0000742 | 10/01/2019 | 42,352.25 |
| 00201107 | 11/25/2019 | NORTH COAST ELECTRIC F592 LIGHTS RELAYS | P0105737 | S009463988001 | 08/08/2019 | 420.65 |
| 00201108 | 11/25/2019 | NORTON CORROSION LIMITED LLC NORTH & SOUTH 2019 ANNUAL | P0105690 | 255454 | 10/18/2019 | 1,088.00 |
| 00201109 | 11/25/2019 | O'REILLY AUTOMOTIVE INC Misc. Apparatus Parts | P0105872 | 2519175412/25191 | 10/10/2019 | 169.12 |
| 00201110 | 11/25/2019 | O'REILLY FIRST CALL New Engine Map Box Modificatio | P0105873 | 543635 | 10/21/2019 | 2,029.34 |
| 00201111 | 11/25/2019 | OCCUPATIONAL HEALTH CTRS OF WA Dive Physical - Schroeder - In | P0105806 | 65948393 | 10/30/2019 | 757.50 |
| 00201112 | 11/25/2019 | ORANGETHEORY FITNESS WA \$25 cancellation fee for Nov | P0105838 | OH012632 | 11/17/2019 | 50.00 |
| 00201113 | 11/25/2019 | HOLZER, KATHARINE E 2019 TREE CABLING SYSTEM | P0105272 | 3176/7/8/9/3124 | 10/22/2019 | 5,379.00 |
| 00201114 | 11/25/2019 | OWEN EQUIPMENT COMPANY FL480 | P0105686 | 00095413 | 10/28/2019 | 259.58 |
| 00201115 | 11/25/2019 | PACIFIC AIR CONTROL INC HVAC MAINT PW | P0105725 | 24640 | 10/25/2019 | 2,464.67 |
| 00201116 | 11/25/2019 | PACIFIC GOLF & TURF REPAIR PARTS | P0105900 | 0284745 | 10/18/2019 | 635.43 |
| 00201117 | 11/25/2019 | PACIFIC MODULAR LLC CARPET CLEANING | P0105771 | 8308 | 10/28/2019 | 315.00 |
| 00201118 | 11/25/2019 | PACIFIC POWER GROUP LLC Transducer/8611 | P0105760 | 651414900 | 10/22/2019 | 174.88 |
| 00201119 | 11/25/2019 | PACIFIC RIM EQUIPMENT RENTAL 4 LIGHT TOWER RENTAL | P0105711 | 28834 | 10/30/2019 | 308.28 |
| 00201120 | 11/25/2019 | PACIFIC RUBBER HOSE ASSY. | P0105778 | R031059 | 11/08/2019 | 373.61 |
| 00201121 | 11/25/2019 | PARKWAY CENTER MANAGEMENT GRP BP/Arco Site - December Rent | P0105910 | OH012633 | 11/22/2019 | 4,000.00 |
| 00201122 | 11/25/2019 | PART WORKS INC., THE REPAIR & DIAPHRAGM KITS | P0105821 | INV49674 | 11/14/2019 | 462.13 |
| 00201123 | 11/25/2019 | PAYMASTER SALES & SERVICE Check signature name plate | P0105744 | 1371 | 11/01/2019 | 312.95 |
| 00201124 | 11/25/2019 | PEHA, ROBERT & DONNA DEPOSIT REFUND | | 1807134 | 11/19/2019 | 4,500.00 |
| 00201125 | 11/25/2019 | PHILEN, SUZANNE LIGHTS | | OH012620 | 11/12/2019 | 420.62 |
| 00201126 | 11/25/2019 | PLATT ELECTRIC INVENTORY PURCHASES | P0105775 | X404764 | 10/29/2019 | 467.39 |
| 00201127 | 11/25/2019 | POLICE ASSOCIATION PAYROLL EARLY WARRANTS | | NOV222019 | 11/22/2019 | 2,507.04 |
| 00201128 | 11/25/2019 | POT O' GOLD INC Water cooler for Nov 2019 | P0105885 | 0236132/0234852/ | 10/09/2019 | 561.75 |
| 00201129 | 11/25/2019 | PRAXAIR DISTRIBUTION INC 2019 ACETYLENE AND OXYGEN TANK | P0102659 | 92854092 | 10/31/2019 | 53.85 |

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| 00201130 | 11/25/2019 | PRIORITY MARINE REBUILD UPPER GEAR BOX DUE TO | P0105856 | 1078 | 11/18/2019 | 6,966.40 |
| 00201131 | 11/25/2019 | PUBLIC SAFETY SELECTION PC Pre-hire psychological testing | P0105864 | 4875 | 10/31/2019 | 800.00 |
| 00201132 | 11/25/2019 | RAINIER BUILDING SERVICES JANITORIAL CITY BLDGS | P0105730 | 181004 | 10/31/2019 | 11,958.87 |
| 00201133 | 11/25/2019 | RAMSAY, JON LEOFF1 Medicare Reimb | | DEC2019A | 11/20/2019 | 2,693.40 |
| 00201134 | 11/25/2019 | REGIONAL TOXICOLOGY SERVICES Lab fees for C.Harnish clients | P0102231 | TC20290103119 | 10/31/2019 | 47.20 |
| 00201135 | 11/25/2019 | RELX INC DBA LEXISNEXIS Library Subscriptions - Invoic | P0105785 | 1000RVY57 | 10/31/2019 | 348.70 |
| 00201136 | 11/25/2019 | RENTON FISH & GAME CLUB INC Renton range fees | P0105911 | OH012635 | 11/25/2019 | 25.00 |
| 00201137 | 11/25/2019 | RESERVE ACCOUNT Postage fund refill | P0105888 | OH012634 | 11/20/2019 | 2,500.00 |
| 00201138 | 11/25/2019 | RODDA PAINT CO PAINT SUPPLIES CH | P0105721 | 19914221 | 10/23/2019 | 120.01 |
| 00201139 | 11/25/2019 | ROMAINE ELECTRIC CORP Battery/7607 | P0105763 | 1198815 | 11/04/2019 | 579.12 |
| 00201140 | 11/25/2019 | ROSENSTEIN, SUSIE Instructor Payment for October | P0105876 | OH012636 | 11/15/2019 | 465.00 |
| 00201141 | 11/25/2019 | RUCKER, MANORD J LEOFF1 Medicare Reimb | | DEC2019B | 11/20/2019 | 162.10 |
| 00201142 | 11/25/2019 | SALERNO, KRISTOPHER OVERPAYMENT REFUND | | OH012602 | 11/18/2019 | 149.03 |
| 00201143 | 11/25/2019 | SCHOENTRUP, WILLIAM LEOFF1 Medicare Reimb | | DEC2019A | 11/20/2019 | 3,860.67 |
| 00201144 | 11/25/2019 | SCORE October Jail Housing Bill - In | P0105834 | 4079 | 11/08/2019 | 2,520.00 |
| 00201145 | 11/25/2019 | SEA WESTERN INC Helmet | P0105764 | INV2439/INV2485/ | 11/02/2019 | 2,939.67 |
| 00201146 | 11/25/2019 | SEATTLE ADVENTURE SPORTS August 7 Rock Wall Rental | P0105714 | INV000240 | 07/17/2019 | 405.00 |
| 00201147 | 11/25/2019 | SEATTLE CHURCH OF CHRIST, THE Rental FA-3708 completed. Retu | P0105840 | FA3708 | 11/17/2019 | 75.00 |
| 00201148 | 11/25/2019 | SEATTLE PUBLIC UTILITIES OCTOBER 2019 SPU CHARGE FOR RE | P0105691 | OH012615 | 10/31/2019 | 3,744.00 |
| 00201149 | 11/25/2019 | SEATTLE, CITY OF OCTOBER 2019 WATER COSTS | P0105884 | OH012639 | 11/21/2019 | 93,196.49 |
| 00201150 | 11/25/2019 | SIX ROBBLEES INC FL-505 REPAIR PARTS | P0105848 | 1946787 | 09/25/2019 | 93.28 |
| 00201151 | 11/25/2019 | SKYLINE COMMUNICATIONS INC EOC INTERNET | P0105729 | IN45157 | 11/01/2019 | 206.55 |
| 00201152 | 11/25/2019 | SME SOLUTIONS LLC 10/16 UNLEADED TANK SERVICE | P0105693 | 288051/288260 | 10/22/2019 | 854.31 |
| 00201153 | 11/25/2019 | SMITH, RICHARD LEOFF1 Medicare Reimb | | DEC2019B | 11/20/2019 | 223.10 |
| 00201154 | 11/25/2019 | SNYDER, BRIAN WADE CBT Instructor (Makeups) | P0105862 | OH012637 | 11/19/2019 | 150.00 |
| 00201155 | 11/25/2019 | SOLID WASTE SYSTEMS INC CCTV TRUCK | P0103747 | 0118730IN | 11/08/2019 | 190,944.88 |

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| 00201156 | 11/25/2019 | SOUND PUBLISHING INC Ntc. Council Mtg Date Change 2 | P0105825 | 7951694 | 10/31/2019 | 413.35 |
| 00201157 | 11/25/2019 | SOUND SAFETY PRODUCTS SAFETY BOOTS & MISC. WORK CLOT | P0105812 | 1409433 | 11/11/2019 | 437.47 |
| 00201158 | 11/25/2019 | SPORTSLABS USA Gmax Testing ICP/South Mercer | P0105703 | 4815 | 11/01/2019 | 3,750.00 |
| 00201159 | 11/25/2019 | STRANGER, THE Thrift Shop Advertising for 20 | P0102249 | 10197E5BE/101907 | 10/10/2019 | 800.00 |
| 00201160 | 11/25/2019 | SULC, JOSEPH M EMW 5400-6000 WATERMAIN TREE | P0105858 | OH012638 | 11/06/2019 | 660.00 |
| 00201161 | 11/25/2019 | SYSTEMS DESIGN WEST LLC 09/2019 Transport Billing Fees | P0105769 | 20191400 | 10/24/2019 | 1,220.20 |
| 00201162 | 11/25/2019 | T2 SYSTEMS CANADA INC Monthly charges for services t | P0102520 | IRIS0000062714 | 10/28/2019 | 77.00 |
| 00201163 | 11/25/2019 | TEC EQUIPMENT INC Parts for 8611 | P0105871 | 326291S | 10/28/2019 | 547.76 |
| 00201164 | 11/25/2019 | TEMLINE PRODUCTS INC FIELD PAINT | P0105682 | 10255 | 09/09/2019 | 712.80 |
| 00201165 | 11/25/2019 | TETRA TECH INC LAKE LINE & PUMP STATION ACCES | P0099675 | 51508071 | 10/23/2019 | 3,180.40 |
| 00201166 | 11/25/2019 | THOMPSON, JAMES LEOFF1 Medicare Reimb | | DEC2019B | 11/20/2019 | 123.30 |
| 00201167 | 11/25/2019 | THOMSON REUTERS - WEST West Investigative Services - | P0105795 | 841246808 | 11/01/2019 | 421.43 |
| 00201168 | 11/25/2019 | LINKO TECHNOLOGY INC WEB TEST ANNUAL FEE | P0105792 | 6349 | 10/31/2019 | 1,417.00 |
| 00201169 | 11/25/2019 | TRAFFIC SAFETY SUPPLY STREET SIGNS | P0105748 | INV020308 | 10/31/2019 | 1,564.73 |
| 00201170 | 11/25/2019 | UNITED RENTALS NORTH AMERICA BOOM TRUCK RENTAL | P0105418 | 174794115001 | 10/24/2019 | 2,132.28 |
| 00201171 | 11/25/2019 | UNITED STATES TREASURY PAYROLL EARLY WARRANTS | | NOV222019 | 11/22/2019 | 780.29 |
| 00201172 | 11/25/2019 | UNITED WAY OF KING CO PAYROLL EARLY WARRANTS | | NOV222019 | 11/22/2019 | 80.00 |
| 00201173 | 11/25/2019 | NEVADA SYSTEM OF HIGHER EDU WA State Prevention Summit | P0105739 | 45030953 | 10/07/2019 | 750.00 |
| 00201174 | 11/25/2019 | US BANK CORP PAYMENT SYS Operating Supplies | | 5539NOVEMBER | 11/06/2019 | 30,253.66 |
| 00201175 | 11/25/2019 | USABlueBook INVENTORY PURCHASES | P0105697 | 051004 | 10/28/2019 | 310.49 |
| 00201176 | 11/25/2019 | UTILITIES UNDERGROUND LOCATION 2019 UTILITY LOCATE SERVICES | P0102658 | 9100183 | 10/31/2019 | 305.73 |
| 00201177 | 11/25/2019 | UTILITY SERVICE CO INC FIRST HILL BOOSTER | P0105417 | 2002 | 10/04/2019 | 6,562.60 |
| 00201178 | 11/25/2019 | VERIZON WIRELESS Mobil hot spots | P0105892 | 9840715977 | 10/23/2019 | 599.78 |
| 00201179 | 11/25/2019 | VERIZON WIRELESS P&R Wireless cell phone bill | P0105816 | 9840715974 | 10/23/2019 | 1,005.41 |
| 00201180 | 11/25/2019 | WA AUDIOLOGY SERVICES INC PW ANNUAL HEARING TEST | P0105902 | 56766 | 11/18/2019 | 1,696.10 |
| 00201181 | 11/25/2019 | WABO Advertising for Electrical/ | P0105890 | 38057 | 10/25/2019 | 100.00 |

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| 00201182 | 11/25/2019 | WALLACE, THOMAS LEOFF1 Medicare Reimb | | DEC2019B | 11/20/2019 | 170.10 |
| 00201183 | 11/25/2019 | WALTER E NELSON CO INVENTORY PURCHASES | P0105881 | 733357 | 11/19/2019 | 1,203.42 |
| 00201184 | 11/25/2019 | WASHINGTON STATE PATROL Background Checks CPL - Invoic | P0105802 | I20002812 | 11/06/2019 | 222.00 |
| 00201185 | 11/25/2019 | WASHINGTON STATE PATROL Background Checks | P0105747 | I20002352 | 11/08/2019 | 44.00 |
| 00201186 | 11/25/2019 | WCIA Intro to Muni Risk Management | P0105895 | 14610 | 11/14/2019 | 25.00 |
| 00201187 | 11/25/2019 | WEGNER, KEN LEOFF1 Medicare Reimb | | DEC2019B | 11/20/2019 | 146.60 |
| 00201188 | 11/25/2019 | WELLS, ROBERT OVERPAYMENT REFUND | | OH012601 | 11/19/2019 | 323.44 |
| 00201189 | 11/25/2019 | WESCOM Maintenance on Radar Unit TA38 | P0105800 | 23326 | 10/04/2019 | 99.00 |
| 00201190 | 11/25/2019 | WHEELER, DENNIS LEOFF1 Medicare Reimb | | DEC2019B | 11/20/2019 | 181.70 |
| 00201191 | 11/25/2019 | WHISTLE WORKWEAR SAFETY BOOTS | P0105851 | 193001 | 10/24/2019 | 148.62 |
| 00201192 | 11/25/2019 | WILSON BOHANNAN PADLOCK CO INVENTORY PURCHASES | P0105403 | 0167864IN | 11/12/2019 | 739.07 |
| 00201193 | 11/25/2019 | WORKWEAR PLACE, THE MISC. WORK CLOTHES | P0105777 | 870 | 11/07/2019 | 100.89 |
| 00201194 | 11/25/2019 | WSCCCE AFSCME AFL-CIO PAYROLL EARLY WARRANTS | | NOV222019 | 11/22/2019 | 2,612.89 |
| 00201195 | 11/25/2019 | XEROX CORPORATION 2019 BASE AND METER COPIER SER | P0102593 | 098674074 | 11/06/2019 | 1,201.33 |
| 00201196 | 11/25/2019 | XEROX CORPORATION Xerox - invoice #098595551 | P0105835 | 098674075 | 11/06/2019 | 2,605.84 |
| 00201197 | 11/25/2019 | ZEE MEDICAL FIRE EXTINGUISHER ABC 2.5 LB | P0105857 | 68388322 | 11/19/2019 | 357.72 |
| | | | | | Total | <u>1,944,450.84</u> |

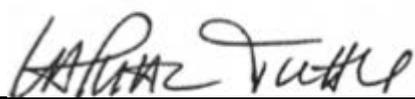


CITY OF MERCER ISLAND CERTIFICATION OF PAYROLL

Item 3.

| PAYROLL PERIOD ENDING | 11.15.2019 |
|---|----------------------|
| PAYROLL DATED | 11.22.2019 |
| Net Cash | \$ 529,591.94 |
| Net Voids/Manuals | \$ 20,968.91 |
| Net Total | \$ 550,560.85 |
| | |
| Federal Tax Deposit - Key Bank | \$ 89,727.92 |
| Social Security and Medicare Taxes | \$ 42,538.39 |
| Medicare Taxes Only (Fire Fighter Employees) | \$ 2,566.99 |
| State Tax (Massachusetts) | \$ 40.71 |
| Public Employees Retirement System 2 (PERS 2) | \$ 30,447.11 |
| Public Employees Retirement System 3 (PERS 3) | \$ 6,211.99 |
| Public Employees Retirement System (PERSJM) | \$ 737.83 |
| Public Safety Employees Retirement System (PSERS) | \$ 209.64 |
| Law Enforc. & Fire fighters System 2 (LEOFF 2) | \$ 27,691.79 |
| Regence & LEOFF Trust - Medical Insurance | \$ 12,821.31 |
| Domestic Partner/Overage Dependant - Insurance | \$ 2,159.74 |
| Group Health Medical Insurance | \$ 1,106.18 |
| Health Care - Flexible Spending Accounts | \$ 2,062.93 |
| Dependent Care - Flexible Spending Accounts | \$ 1,531.09 |
| United Way | \$ 80.00 |
| ICMA Deferred Compensation | \$ 30,357.95 |
| Fire 457 Nationwide | \$ 7,307.13 |
| Roth - ICMA | \$ 510.00 |
| Roth - Nationwide | \$ 879.68 |
| Tax Levy | \$ 780.29 |
| Child Support | \$ 599.99 |
| Mercer Island Employee Association | \$ 300.00 |
| Cities & Towns/AFSCME Union Dues | \$ 2,612.89 |
| Police Union Dues | \$ 2,507.04 |
| Fire Union Dues | \$ 2,111.33 |
| Fire Union - Supplemental Dues | \$ 160.00 |
| Standard - Supplemental Life Insurance | \$ 430.30 |
| Unum - Long Term Care Insurance | \$ 521.90 |
| AFLAC - Supplemental Insurance Plans | \$ 722.21 |
| Coffee Fund | \$ 148.00 |
| Transportation | \$ 62.08 |
| HRA - VEBA | \$ 5,514.47 |
| Nationwide Extra | \$ 1,366.66 |
| GET | \$ 250.00 |
| Tax & Benefit Obligations Total | \$ 277,075.54 |
| | |
| TOTAL GROSS PAYROLL | \$ 827,636.39 |

I, the undersigned, do hereby certify under penalty of perjury that the materials have been furnished, the services rendered or the labor performed as described herein, that any advance payment is due and payable pursuant to a contract or is available as an option for full or partial fulfillment of a contractual obligation, and that the claim is a just, due and unpaid obligation against the City of Mercer Island, and that I am authorized to authenticate and certify to said claim.



 Finance Director

I, the undersigned, do hereby certify that the City Council has reviewed the documentation supporting claims paid and approved all checks or warrants issued in payment of claims.



**BUSINESS OF THE CITY COUNCIL
CITY OF MERCER ISLAND**

**AB 5636
December 3, 2019
Consent Calendar**

AGENDA BILL INFORMATION

| | | |
|----------------------------|--|--|
| TITLE: | AB 5636: CPD Development and Construction Permit Fees Update (Resolution No. 1567) | <input type="checkbox"/> Discussion Only |
| RECOMMENDED ACTION: | Approve Resolution No. 1567 to update Development and Construction Permit Fees. | <input checked="" type="checkbox"/> Action Needed: |
| | | <input type="checkbox"/> Motion |
| | | <input type="checkbox"/> Ordinance |
| | | <input checked="" type="checkbox"/> Resolution |

| | |
|-------------------------------|--|
| DEPARTMENT: | Community Planning and Development |
| STAFF: | Alison Van Gorp, Deputy Director |
| COUNCIL LIAISON: | n/a |
| EXHIBITS: | <ol style="list-style-type: none"> 1. FCS Group Fee Study 2. Resolution No. 1567 including: <ul style="list-style-type: none"> Attachment A: Updated Fee Schedule Attachment B: Building Valuation Table Attachment C: Building Permit Calculation Table |
| CITY COUNCIL PRIORITY: | 1. Implement a Fiscal Sustainability Plan |

| | |
|-------------------------------|--------|
| AMOUNT OF EXPENDITURE | \$ n/a |
| AMOUNT BUDGETED | \$ n/a |
| APPROPRIATION REQUIRED | \$ n/a |

SUMMARY

The City typically raises permit fees annually to adjust for increases in the cost of providing services due to increased personnel costs. This year, while there is a cost of living increase of 1.9% to staff salaries/wages, other expenditure reductions (i.e. elimination of the pay for performance program) are resulting in a net neutral impact on cost of services. Thus, no across-the-board increase is recommended for the development and construction permit fees. However, staff are recommending an increase to the cost recovery targets for some services, as well as the resultant increases in the fees for those services. In addition, there are a few fees that are changing/being added in response to code amendments adopted over the last year. Resolution No. 1567 adopts these new fees, effective January 1, 2020 (Exhibit 2).

COST RECOVERY TARGETS

In May 2016, the City Council increased the target cost recovery levels for Planning and Engineering Services to 80% and directed staff to hire a consultant to conduct a development fee study in 2018, looking at cost recovery levels in 2017 and focusing particularly on the City's planning fees (see AB 5173). No change was made to the target cost recovery level for Building Services, which remains 95%.

In 2018, the City contracted with FCS Group to perform an analysis of cost recovery for Community Planning and Development (Exhibit 1). The analysis included an in-depth look at the cost of service for Planning Services. At the time of the analysis, both Building and Engineering were recovering costs above the targeted level. Planning Services was recovering costs well below the target, and in November 2018 City Council approved a fee adjustment to increase Planning fees to achieve the targeted recovery level of 80% (see AB 5500). Fees for Appeals, Accessory Dwelling Units, and Code Interpretation Requests were not increased as these services all have significant public benefits.

In April 2019, Management Partners, the City’s fiscal sustainability consultant, recommended further increases to development and construction fees to get closer to full cost recovery for fee-based services. Based on this recommendation, staff now recommend further raising the targeted fee recovery levels as follows:

| | |
|----------------------|--------------------|
| Building Services | 95% |
| Engineering Services | 80% 90% |
| Planning Services | 80% 90% |

To achieve the new 90% target level, staff are also recommending another increase to the Planning Services fees (see Exhibit 2, Attachment A). When the FCS Group conducted the fee study last year, Engineering Services was already recovering costs above the 90% level, so an increase in Engineering fees is not recommended at this time.

OTHER UPDATES

In addition to the increases to Planning Services fees to reflect the new cost recovery target, a few fees have been changed or added in response to code amendments over the last year. Included in these fee updates are the following changes (see Exhibit 2a for details):

- SEPA – consolidating Major and minor categories into a single fee for SEPA reviews
- Critical areas determination removed, replaced with critical area review type 1 and critical area review type 2
- Wireless – Small cells – new fee added

Right of way permit fees have also been adjusted to more closely align with the hourly rate and to allow for additional charges for large projects that exceed the base rate. Finally, the fee schedule is being updated to include fire and over the counter review fees. These existing fees have not been consistently included in the adopted fee schedule; adding them will provide more transparency. The fee schedule also includes the Building Valuation Table and Building Permit Calculation Table, which are used to determine most building permit fees (Exhibit 2, Attachments B and C).

NEXT STEPS

Staff recommend conducting another fee study in 2021 (based on data collected in 2020) to check whether costs recovery levels are on target and fees are accurately representing the cost of providing services.

RECOMMENDATION

Approve Resolution No. 1567 adopting new development and construction permit fees effective January 1, 2020.

City of Mercer Island

Development Fee Study

FINAL DRAFT REPORT
November 2018

Washington

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Redmond, WA 98052
425.867.1802

Oregon

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FCS GROUP
Solutions-Oriented Consulting

Item 4.

November 15, 2018

Evan Maxim
Planning Manager
City of Mercer Island
9611 SE 36th Street
Mercer Island, WA 98040

Subject: Development Fee Study

Dear Mr. Maxim:

Attached is our final report on the results of our Development Fee Study. We want to thank you and all the staff from Development Services for their assistance and participation in helping us gather information and in discussing the various issues. If you have any questions, please feel free to contact me at (425) 867-1802 extension 228.

Yours very truly,



Peter Moy
Principal



Matt Hobson
Project Consultant

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CHAPTER I: INTRODUCTION

The City of Mercer Island's Development Services Group (DSG or Department) initiated a cost of service study for its Building Plan Review and Inspection services (Building), Land Use Planning services, and Development Engineering services as well as a fee study for its Land Use Planning services. The Department engaged FCS GROUP to perform the cost of service and fee study. The cost of service and fee study identifies the labor and non-labor resources, establishes the full cost of service for development fee related activities provided by the Department, determines the City's cost recovery rate, and establishes a framework for cost recovery recommendations related to its development permitting functions.

The approach used to conduct the study involved the following:

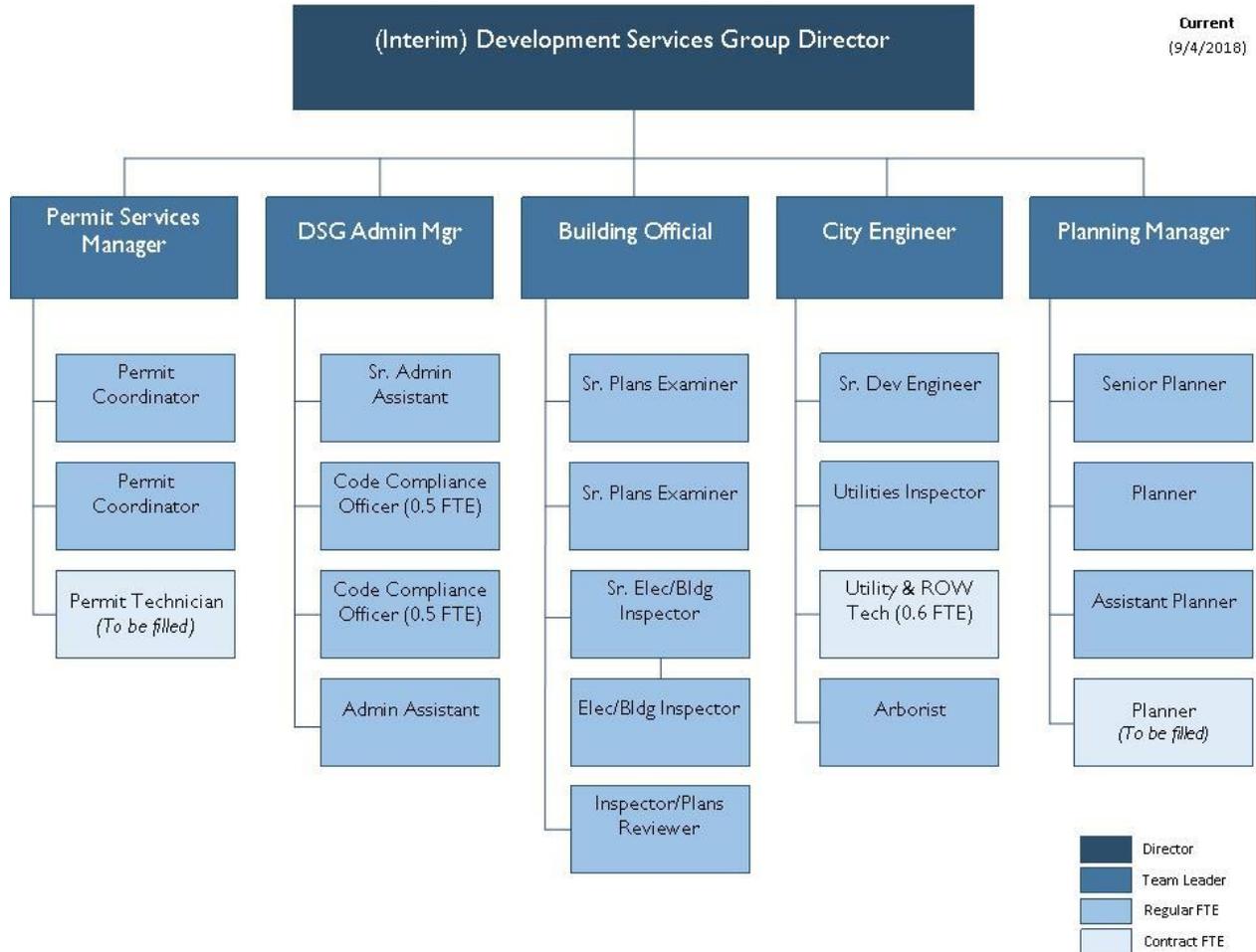
- Working with the DSG management and staff who are involved with fee and non-fee related services for Land Use Planning services,
- Analyzing 2017 financial documentation and data associated with development related services and fees,
- Working with DSG staff to analyze the existing Land Use Planning fees and to estimate the direct labor time needed to provide each fee service,
- Having DSG management and staff review the cost of service and cost recovery for each Land Use Planning service fee,
- Reviewing with DSG management and staff the direct and indirect labor estimates, non-labor and overhead cost allocation results, the cost of service analysis, and the cost recovery results for building, planning, and engineering fee services,
- Presenting the cost of service analysis and cost recovery results to the City Council.

The process used for collecting and analyzing the data required active participation by City staff. We want to take the opportunity to recognize the time, participation, and effort that all DSG staff devoted to the study and for scheduling and organizing the meetings.

DEVELOPMENT SERVICES GROUP BACKGROUND

The Department provides permitting services for building, land use, and private development. In addition to these services, DSG also provides long-term planning, code enforcement, and management of the City's capital improvement projects. According to the 2017-2018 Adopted Budget, the Department includes 20.00 regular FTEs and 3.50 contracted temporary FTEs. Exhibit 1 details the organizational structure of the Department as of September 2018.

Exhibit 1
2018 Development Services Group Organization Chart



The Department’s services are supported by permit fees and other City funds. The Department’s 2017 estimated expenditures and 2018 budgeted expenditures are summarized below in Exhibit 2.

Exhibit 2
Development Services Group Expenditures, 2017 Estimate and 2018 Budget

| Division | 2017 Estimate | 2018 Budget |
|--|--------------------|--------------------|
| Administration (DS 1100) | \$1,194,116 | \$1,329,419 |
| Building Plan Review & Inspection (DS 1200) | 878,340 | 869,845 |
| Land Use Planning Services (DS 1300) | 633,798 | 743,209 |
| Development Engineering (DS 1400) | 404,792 | 387,098 |
| Arborist (DS 1700) | 87,456 | 142,347 |
| Utility Inspections (DS 4501, 4502, 4503, 4504)* | 128,917 | 130,838 |
| Total | \$3,327,419 | \$3,602,756 |

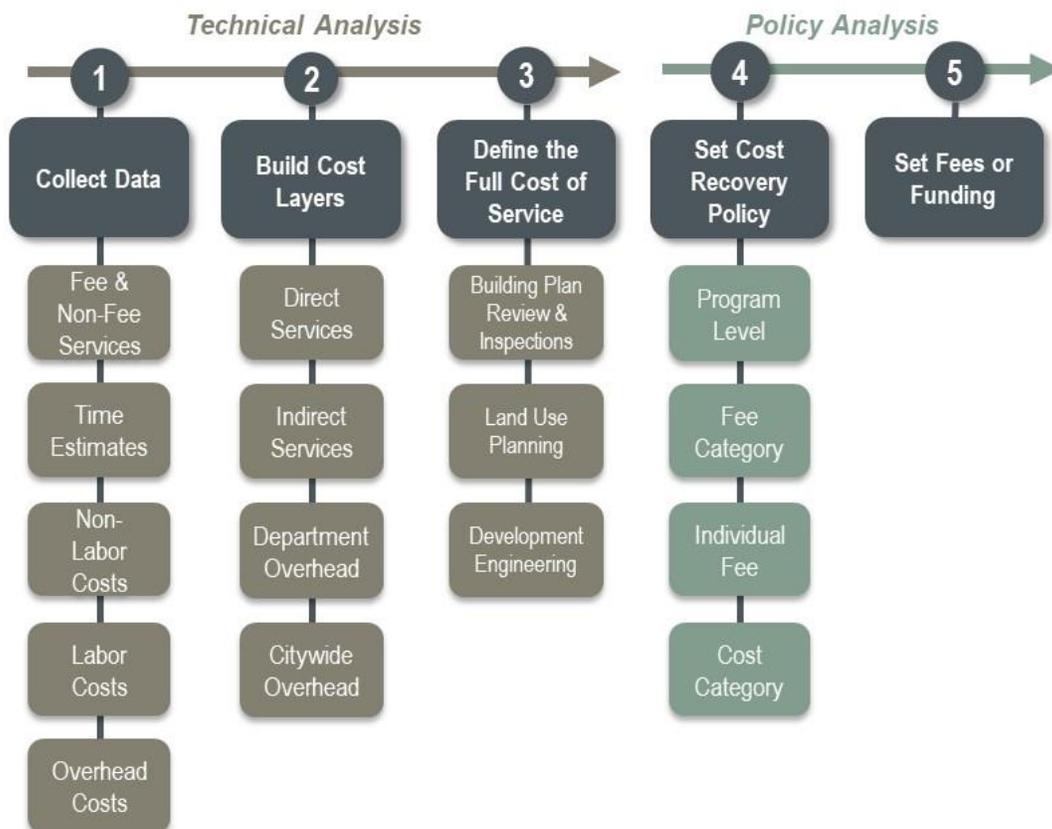
*Utility inspection expenditures are funded through the City’s water, sewer, street, and stormwater funds and are not recovered by permit fees.

CHAPTER II: COST OF SERVICE

METHODOLOGY

To determine the cost of service and the appropriate fees, a defined task plan was followed as outlined below in Exhibit 3. The methodology identifies both the labor and non-labor resources that are required to perform the services and activities and analyzes the cost of service for each of the fee and non-fee services performed by the City’s staff. The analysis provides the City’s elected officials, management, and DSG staff the opportunity to identify the cost basis for its services and fees.

Exhibit 3
Cost of Service and Fee Methodology



Step 1: Collect Data – The data collection phase is the critical step that establishes the parameters of the cost of service and fee analyses. The first part of the data collection process involved reviewing the Planning fee schedule to determine what fees and services should be included in the study.

Individual fees related to the Department's Building and Development Engineering programs were not analyzed in this study.

Based on the fee schedules and discussions with staff, Exhibits 4, 5, and 6 show the list of the individual current and new planning fee services included in the study. In total, DSG staff identified 78 planning fees to review in the study: 63 current fees and 15 new fees.

Exhibit 4
Existing Land Use Planning Fees Included in Study

- Appeals - Land Use
- Appeals - Code Interpretation
- Critical Areas - Determination
- Critical Areas - Reasonable Use Exception
- Design Review - Design Commission Study Session
- Wireless Communication Facilities - Wireless Communication Facilities-6409 Exemption
- Wireless Communication Facilities - New Wireless Communications Facility
- Deviations - Changes to Antenna Requirements
- SEPA Review (Checklist) - Major
- Shoreline Management - Exemption
- Shoreline Management - Permit Revision
- Shoreline Management - Substantial Development Permit
- Subdivision Preliminary Long Plat - Long Plat
- Subdivision Long Plat - Subdivision Alteration to Existing Plat
- Subdivision Long Plat - Final Subdivision Review
- Preliminary Subdivision Short Plat
- Subdivision Short Plat - Three Lots
- Subdivision Short Plat - Four Lots
- Subdivision Short Plat - Short Plat Alteration
- Subdivision Short Plat - Final Short Plat Approval
- Variances (Plus Hearing Examiners Fee)
- Other Land Use - Accessory Dwelling Units (ADU)
- Other Land Use - Code Interpretation Request
- Other Land Use - Comprehensive Plan Amendment (CPA)-Application
- Other Land Use - Conditional Use (CUP)
- Other Land Use - Lot Line Revision
- Other Land Use - Noise Exception - Type 1
- Other Land Use - Noise Exception - Type 2
- Other Land Use - Noise Exception - Type 3
- Other Land Use - Reclassification of Property (Rezoning)
- Other Land Use - Zoning Code Text Amendment
- Pre-Application Meeting
- SEPA Review (Checklist) – Minor
- Deviations - Changes to Open Space
- Deviations - Critical Areas Setback
- Deviations - Shoreline
- Environmental Review (SEPA) - Environmental Impact Statement or Revision
- Subdivision Long Plat - Fire Review
- Subdivision Short Plat - Deviation of Acreage Limitation

- Other Land Use - Code Interpretation Request (after first six hours)

Through the review process, DSG staff also identified a list of current planning fees to consolidate, delete, or retain at the current fee level. A cost of service analysis was not completed for these fee services. Exhibit 5 details current fees that were deleted as a result of the study.

Exhibit 5

Existing Land Use Planning Fees Deleted, Consolidated, or Retained at Current Fee Level

- Appeals – Building (consolidated with other appeals)
- Design Review - Administrative Review Other Than Signs and Colors
- Design Review - Administrative Review of Development Signs
- Design Review - Change to Final Design Approval
- Design Review - Design Review - Major, \$0 to \$5,000
- Design Review - Design Review - Major, \$5,001 - \$25,000
- Design Review - Design Review - Major, \$25,001 - \$50,000
- Design Review - Design Review - Major, \$50,001 - \$100,000
- Design Review - Design Review - Major, \$100,001+
- Design Review - Design Review - Minor, \$0 to \$5,000
- Design Review - Design Review - Minor, \$5,001 - \$25,000
- Design Review - Design Review - Minor, \$25,001 - \$50,000
- Design Review - Design Review - Minor, \$50,001 - \$100,000
- Design Review - Design Review - Minor, \$100,001+
- Deviations – Wet Season Construction Moratorium (retained at current fee level)
- Environmental Review (SEPA) - Environmental Impact Statement Revision
- Shoreline – Semi-Private Recreation Tract – Modification
- Shoreline – Semi-Private Recreation Tract – New
- Subdivision Short Plat - Fire Review (per hour)
- Other Land Use – Addition Fee if added to Final Docket (retained at current fee level)
- Other Land Use – Lot Consolidation
- Other Land Use - Right of Way Encroachment Agreement (transferred to Development Engineering program)
- Other Land Use – Sign Fee (retained at current fee level)

DSG staff also identified new planning fees to include in the study. Exhibit 6 details the list of new fees.

Exhibit 6

New Planning Fees Included in Study

- Pre-Design Meeting
- Requests for Letter
- Hourly Fee for Planning Work Not Covered by a Permit
- Additional Design Review Meeting
- Business License Review
- Appeals - ROW
- Shoreline - Variance
- Shoreline - Conditional Use
- Noise Exception - Type 4
- 3rd+ Correction Request

- Design Review (Code Official)
- Design Review (Design Commission - Exterior Alterations)
- Design Review (Design Commission - New Building)
- Impact Fee Deferral
- Transportation Concurrency

With the fee services identified, the data collection effort focused on collecting budget and time data from all staff involved in the services and activities. The Department provided two sets of timekeeping records for each staff member in 2017. The first data set included work hours, leave hours, and total paid hours for each employee. The second data set provided detailed work hours by activity for each employee. These two data sets were used as the basis for establishing the total number of annual work hours available and the distribution of work hours by activity. Each work activity was then categorized into the following broad categories.

Direct Services – Services provided as the result of a project, permit application, or specific related activity and that are often tied to a specific fee (e.g. plan review). Also, services provided directly for or to the public that are not fee-related (e.g., code enforcement and long-term planning). Appendix A details the distribution of direct service work hours in 2017 for each program by fee and non-fee activities.

Indirect Services – Services provided to support direct services (e.g. customer service, administrative duties) and that cannot be assigned to a specific project, application, activity, or request.

The Department reviewed the work activities in 2017 and the following activities were assigned as indirect services. These services are described as follows:

- Customer Service – Time spent assisting customers and the public with information and questions about fee services.
- Administrative Duties – Time spent on general office tasks, such as division management, supervision, internal meetings/calls/e-mails, filing, and other miscellaneous activities.
- EGov and MBP – Time spent managing electronic permits and permit status (e.g., permit applications, inspection scheduling)

Based on Department feedback, a portion of the indirect administrative activities of Building staff supports the Land Use Planning and Development Engineering programs. As a result, 90 percent of the time spent by Building staff for indirect administrative activities was allocated to the Building Program with the remaining 10 percent of time split evenly between the Land Use Planning and Development Engineering programs. Similarly, indirect administrative time for the Department's customer service team was allocated to the Building (60%), Land Use Planning (25%), and Development Engineering (15%) programs. All of the indirect administrative time for Land Use Planning and Development Engineering staff were directly assigned to their respective programs.

Because the staff perform both fee and non-fee services, hours and costs for the indirect services (e.g. customer service) were allocated between fee and non-fee services based on the proportion of direct fee hours and direct non-fee hours.

Overhead Services – General management and administrative costs primarily related to department or division indirect costs and allocations that support the Department's operations and services.

The combined annual work hours for direct services, indirect services, and overhead services for each employee in 2017 were then compared to and reconciled with the timekeeping data for each

employee's reported work hours for the year. If an employee's direct, indirect, and overhead work hours did not match to total reported work hours, then the direct, indirect, and overhead work hours were adjusted proportionally to reconcile to total work hours.

In addition to each employee's overall time for 2017, staff focus groups were conducted to identify the processing times for each individual Land Use Planning fee service by position class. Planning staff met several times to discuss and identify the time needed for each permit. The total processing time for each Land Use Planning fee service is shown in Appendix B.

Step 2: Build Cost Layers – The next stage in the process was to develop an analytical model for calculating the costs related to each fee category. The design and structure for the analytical model were based on the services and activities identified in Step 1 that were associated with the three service categories: direct, indirect, and overhead services. Cost layers were then developed for each fee category.

To build the cost layers, the staff time allocations for each activity (i.e. direct, indirect, and overhead) were first priced at each individual staff member's loaded hourly rate. The loaded hourly rate for one staff member equals the person's annual salary and benefits divided by the available work hours (i.e. total annual hours minus leave) in 2017. The analysis was done separately for Building, Land Use Planning, and Development Engineering based on staff time records and the services provided (see Appendix A). An additional analysis was completed to develop the cost of service for time spent on direct services by administrative staff including the DSG director and permit center staff.

After the labor costs for each staff member and each service were calculated, the non-labor costs were analyzed. Non-labor costs were allocated among direct fee services, direct non-fee services, and indirect services in proportion to the level of staff time reported for each service. Contract labor services to support Building and Development Engineering services were directly assigned to direct fee services. Contract labor costs that support permit archiving work were distributed across the different fee categories proportionally based on time estimates provided by City staff.

City and department overhead costs were also allocated for fee and non-fee services. City-wide overhead costs were allocated across the divisions (e.g., Administration, Building, Land Use Planning, and Development Engineering) based on the number of 2017-18 budgeted regular FTEs within each division. Department overhead related to the DSG director's indirect labor time was also allocated to the other divisions proportionally based on 2017-18 budgeted regular FTEs. The remaining Department overhead labor costs were allocated to the other divisions proportionally based on 2017 permit volumes. With exception to fleet-related overhead expenses, the Department overhead non-labor costs were allocated to the other divisions proportionally based on 2017 permit volumes. Based on discussions with City staff, 50 percent of fleet overhead costs (maintenance and replacement) are allocated to support code compliance services unrelated to permit services. The remaining 50 percent of fleet overhead costs support direct fee services and are allocated proportionally to the divisions based on 2017 permit volumes.

Step 3: Determine the Full Cost of Service – After establishing the different cost layers, the full cost of service was calculated. The initial steps of the cost of service analysis were focused on taking each fee group's 2017 cost of operations and distributing those costs among the different service categories and components to establish the cost layers that ultimately make up each program's total cost. To determine the full cost of service, the different costs are brought back together. The direct non-labor costs, indirect costs, and overhead costs for a particular fee category were added to each staff member's hourly rate based on each cost layer's cost divided by the total direct hours for the fee

category (e.g. Building, Land Use Planning, and Development Engineering). With these additional components, a fully loaded hourly rate was calculated for each staff member.

For a specific fee category, the cost layers were used to arrive at the full cost of service for each fee category. These cost layers were used to calculate hourly rate components by dividing the number of direct hours into the different cost layers.

For individual planning fees, the cost for each fee service was calculated by applying the fully loaded hourly rates for each position multiplied by the number of hours spent on each individual service. A fee service's total cost equals the sum of the costs of all the employees who provide the fee service. During interviews with Department staff, it was determined that some fee services require time from staff outside of the Department or City (e.g., city attorney, fire marshal, hearing examiner). In these cases, time estimates were provided for their work on specific fee services and the Department provided hourly labor rate estimates for the positions. Because these positions are outside of the Department, the hourly labor rates do not include DSG Department non-labor or overhead costs.

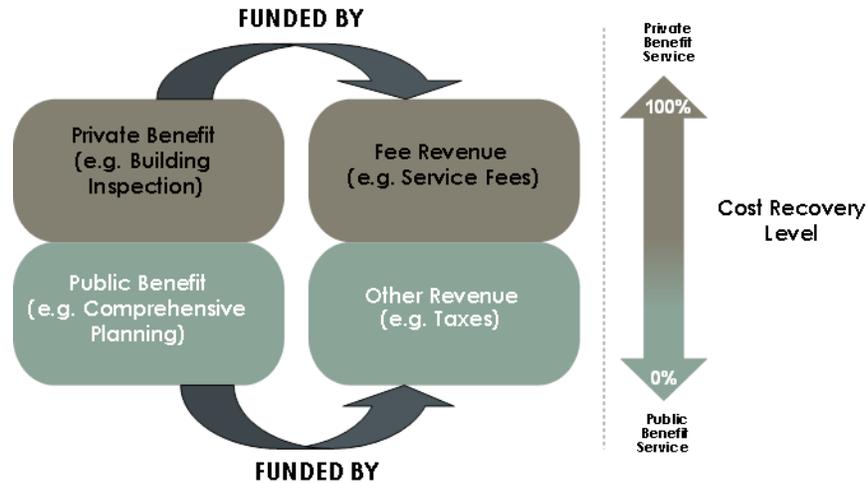
Step 4: Set Cost Recovery Objectives – Once the full cost of service is identified and the hourly rates are established, the next step is to identify the cost recovery levels and to establish cost recovery objectives.

Overall cost recovery levels for Building Plan Review and Inspections, Land Use Planning, and Development Engineering were determined by comparing each fee category's total cost of service to the respective permit fee revenue in 2017. When services cost more than the revenue generated, funding from the General Fund or other funds is needed to cover the gap between costs and revenues. The amount of cost recovery is a policy decision that is generally made by the City Council. Based on discussions with Department staff, the cost recovery goal for Building Plan Review and Inspections is 95 percent. The cost recovery goal for Land Use Planning and Development Engineering is 80 percent.

Cost recovery levels for individual Land Use Planning fees were also determined by comparing the costs of the various services to the individual fees charged (e.g. percentage of full costs compared to revenue generated). For this study, the fees charged for Land Use Planning services exclude the three percent technology fee assessed on all Department fees.

Cost recovery objectives can be based on a variety of factors, including the public versus private benefit provided by the service. If an activity has a public benefit, it might be more appropriately supported by the General Fund. Conversely, if an activity has mostly private benefits, it might be more appropriately supported by fees. Activities that have a mix of public and private benefits might be supported by a combination of fees and the General Fund. Exhibit 7 illustrates the application of public versus private benefit decisions in determining what level of the full cost of service should be recovered from fees. When fee activities combine both public and private benefits, the fees might be less than the cost of service. This concept that should be considered when determining the level of cost recovery.

Exhibit 7 Public Versus Private Benefit and Cost Recovery Decisions



Step 5: Set Fees – The final step of the cost of service and fee analysis was to calculate the fees based on the cost recovery policies. To calculate the fees, the amount of time spent to provide each fee service was established, where applicable. Other considerations in fee setting besides the analytical cost recovery objectives include key questions such as:

- Is it feasible to set fees at the calculated level?
- Will increasing fees result in compliance or public safety problems?
- Can the market bear the fee increases?
- Do adjustments in fees adversely affect other City goals?
- Are there feasible process changes that might bring costs into better balance with revenues?

CHAPTER III: COST OF SERVICE AND RECOVERY ANALYSIS STRATEGIES

Based on the methodology described in the previous chapter, the estimated 2017 full cost of service and cost recovery levels were determined for Building, Land Use Planning, and Development Engineering services as well as for individual Land Use Planning fees. The cost of service analysis shows the costs by type of cost category, overall fee category, and individual fee. Costs were first categorized following the Department’s organizational chart: Building, Land Use Planning, Development Engineering, Administration, and other departments. Because each staff group might provide services to the other divisions, the labor costs were re-grouped based on the staff’s annual time estimates for the different fee categories: Building, Land Use Planning, and Development Engineering. Exhibit 8 shows the results of the estimated direct hours for each staff group by fee category, and Exhibit 9 shows the labor costs attributed to the three fee categories from the different staff groups.

**Exhibit 8
Breakdown of 2017 Direct Hours by Fee Category***

| Fee Category | Division | | | | | Total |
|--|--|----------------------------|-------------------------|----------------|-------------------|---------------|
| | Building Plan Review & Inspection Services | Land Use Planning Services | Development Engineering | Administration | Other Departments | |
| Building Plan Review & Inspection Services | 7,327 | 3,152 | 380 | 672 | - | 11,530 |
| Land Use Planning Services | 17 | 3,299 | 408 | 238 | - | 3,962 |
| Development Engineering | - | 53 | 2,429 | 116 | 10 | 2,608 |
| Total | 7,344 | 6,503 | 3,217 | 1,026 | 10 | 18,100 |

*According to 2017 timesheet data, staff from the City’s Public Works Department provided ten hours of support for Development Engineering fee services.

Exhibit 9
Allocation of 2017 Direct Labor Cost Layer by Fee Category

| Fee Category | Division | | | | | Total |
|--|--|----------------------------|-------------------------|------------------|-------------------|---------------------|
| | Building Plan Review & Inspection Services | Land Use Planning Services | Development Engineering | Administration | Other Departments | |
| Building Plan Review & Inspection Services | \$ 590,449 | \$ 183,286 | \$ 20,123 | \$ 35,077 | \$ - | \$ 828,935 |
| Land Use Planning Services | 1,881 | 197,642 | 37,087 | 25,250 | - | 261,861 |
| Development Engineering | - | 3,955 | 160,802 | 6,588 | 716 | 172,061 |
| Total | \$ 592,330 | \$ 384,883 | \$ 218,012 | \$ 66,915 | \$ 716 | \$ 1,262,856 |

Note: Slight differences may occur due to rounding

Once the cost of service has been established, the level of cost recovery can be analyzed by comparing the cost of service to the revenues or current fee. The analysis evaluates the cost recovery level at an overall fee category level as well as for individual Land Use Planning fees. As previously mentioned, the cost of service is primarily based on the amount of time assigned to each fee category. Time from one fee category might also be related to other fee categories because there might be a certain degree of legal and technical overlap between the different fee categories. Consequently, each fee category's cost of service provides a general cost estimate, and the cost recovery levels might also be affected by these estimates. The following cost of service and cost recovery analyses are based on the fee category and not on the organizational divisions.

BUILDING PLAN REVIEW & INSPECTION

As shown in Exhibit 10, the full cost of Building fee supported services was \$1,705,135. Direct services were about 54% of the full cost of service, while indirect services were 6% of the full cost of service. Department administration and citywide overhead costs represented the remaining cost of service. The Building group's non-labor costs were allocated across building fee and non-fee categories based on staff-reported work hours.

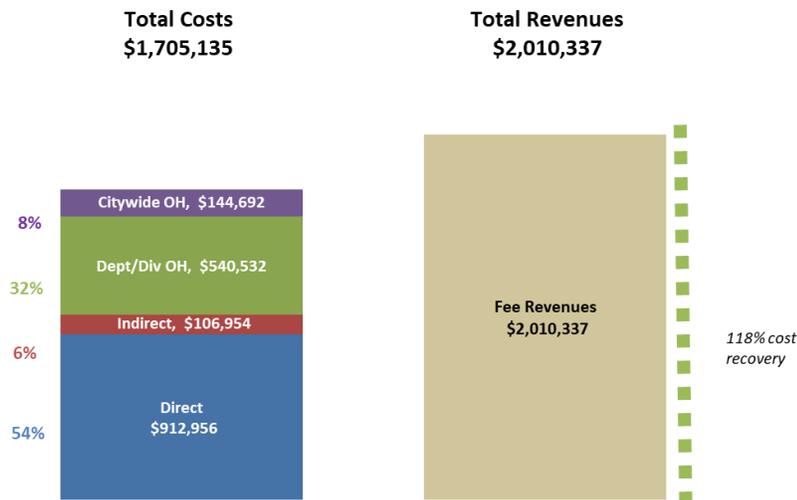
Exhibit 10
2017 Full Cost of Service for Building Plan Review & Inspection

| <i>Building Plan Review & Inspection Services</i> | | Annual Cost Components | | Total Costs | % of Total Costs |
|---|--|------------------------|-------------------|---------------------|------------------|
| | | Labor Costs | Non-Labor Costs | | |
| Direct Costs | Total Direct Services | \$ 828,935 | \$ 26,920 | \$ 855,854 | 50.2% |
| | Contract Services | | \$ 57,102 | \$ 57,102 | 3.3% |
| Subtotal Direct Costs | | \$ 828,935 | \$ 84,021 | \$ 912,956 | 53.5% |
| Indirect Costs | DINADM - Indirect-Administrative Duties | \$ 95,618 | \$ 2,705 | \$ 98,323 | 5.8% |
| | DINCST - Indirect - Customer Service | 2,165 | 46 | 2,211 | 0.1% |
| | DINEGV - Indirect - EGov and MBP | 6,289 | 131 | 6,420 | 0.4% |
| Subtotal Indirect Costs | | 104,072 | 2,882 | \$ 106,954 | 6.3% |
| Overhead Costs | Department Administration OH - Fee Related | \$ 418,694 | \$ 121,838 | \$ 540,532 | 31.7% |
| | Division OH - Fee Related | - | - | - | - |
| | Citywide OH - Fee Related | - | 144,692 | 144,692 | 8.5% |
| Subtotal Overhead Costs | | \$ 418,694 | \$ 266,531 | \$ 685,225 | 40.2% |
| Total Building Plan Review & Inspection Services Costs | | \$ 1,351,700 | \$ 353,435 | \$ 1,705,135 | 100.0% |

Note: Slight differences may occur due to rounding

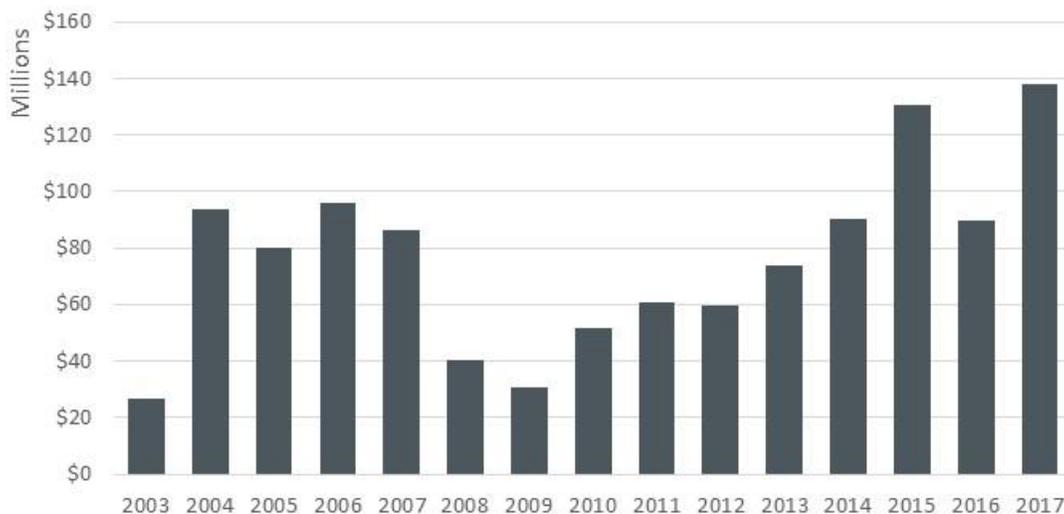
Exhibit 11 compares the Exhibit 10 costs with the 2017 total revenues for Building Plan Review and Inspection services. Permit revenue in this analysis only accounts for recognized or earned revenue in 2017. The Department also received some permit revenue in 2017 for which plan review or inspection work was not completed in 2017. This unearned or carryover revenue is excluded from the cost recovery analysis. The total cost of service for fee supported services (\$1,705,135) was less than earned fee revenue (\$2,010,337) by approximately \$305,000. In other words, the cost recovery rate for Building Plan Review and Inspection services in 2017 was 118 percent. In comparison to previous fee studies completed by FCS GROUP for the Department, the 2017 cost recovery rate is lower than 2015 (125 percent) and higher than 2012 (87 percent).

Exhibit 11
2017 Cost Recovery for Building Plan Review & Inspection Services



Cost recovery levels for building permit services can fluctuate year to year in response to changes in development activity. Exhibit 12 illustrates the change in building valuation over the last fifteen years. Because the majority of building permit revenue is based on building valuation, changes in building valuation have a direct impact on building permit revenue. Higher building valuation generally corresponds to higher cost recovery while lower building valuation generally corresponds to a lower cost recovery. Given the relatively high building valuation in 2017, the cost recovery level for Building Plan Review and Inspections in 2017 exceed the target rate. It is common for jurisdictions to experience higher cost recovery rates during periods of higher permit activity because there is typically a delay in the hiring of additional staff to process the increased workload.

Exhibit 12
Historical Building Valuation, 2003 to 2017



LAND USE PLANNING SERVICES

As shown in Exhibit 13, the full cost of Land Use Planning fee supported services was \$580,870. Direct services were about 48% of the full cost of service, while indirect services were 11% of the full cost of service. The remaining 41% is attributed to Department administration and citywide overhead costs.

Exhibit 13
2017 Full Cost of Service for Land Use Planning Services

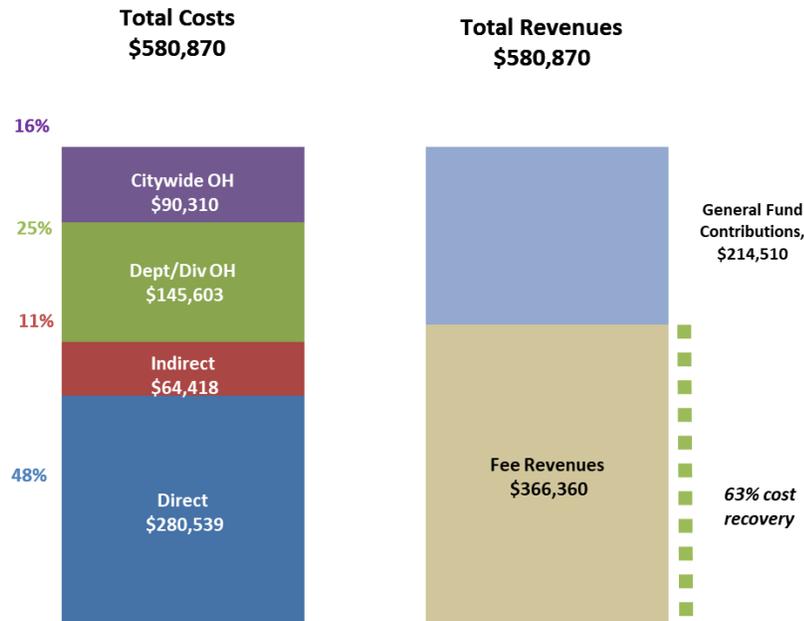
| <i>Land Use Planning Services</i> | | Annual Cost Components | | Total Costs | % of Total Costs |
|---|--|------------------------|-------------------|-------------------|------------------|
| | | Labor Costs | Non-Labor Costs | | |
| Direct Costs | Total Direct Services | \$ 261,861 | \$ 18,678 | \$ 280,539 | 48.3% |
| | Contract Services | | \$ - | \$ - | - |
| Subtotal Direct Costs | | \$ 261,861 | \$ 18,678 | \$ 280,539 | 48.3% |
| Indirect Costs | DINADM - Indirect-Administrative Duties | \$ 46,333 | \$ 1,547 | \$ 47,879 | 8.2% |
| | DINCST - Indirect - Customer Service | 15,962 | 577 | 16,539 | 2.8% |
| | DINEGV - Indirect - EGov and MBP | - | - | - | - |
| Subtotal Indirect Costs | | 62,295 | 2,123 | \$ 64,418 | 11.0% |
| Overhead Costs | Department Administration OH - Fee Related | \$ 123,630 | \$ 21,973 | \$ 145,603 | 25.1% |
| | Division OH - Fee Related | - | - | - | - |
| | Citywide OH - Fee Related | - | 90,310 | 90,310 | 15.5% |
| Subtotal Overhead Costs | | \$ 123,630 | \$ 112,283 | \$ 235,913 | 40.6% |
| Total Land Use Planning Services Costs | | \$ 447,785 | \$ 133,085 | \$ 580,870 | 100.0% |

Note: Slight differences may occur due to rounding

To determine a fully loaded hourly rate for Land Use Planning direct fee services, the total cost of service for the program (\$580,870) was divided by direct fee hours for the program (3,962). The fully loaded average hourly rate for Land Use Planning fees in 2017 is \$146.62.

Exhibit 14 compares the costs found in Exhibit 13 to the 2017 total revenues for Land Use Planning services. This graph illustrates that 63 percent of the division's costs were recovered from permit revenues. The remaining \$215,000 in costs was covered by the General Fund or other revenue sources. In comparison to previous fee studies completed by FCS GROUP for the Department, the 2017 cost recovery rate is lower than in 2015 (83 percent) and higher than in 2012 (39 percent).

Exhibit 14 2017 Cost Recovery for Land Use Planning Services



Individual Land Use Planning Fees

Exhibits 15, 16, and 17 detail the cost of service for all identified planning fees based on the calculated rates, the current fee, and cost recovery percentage. Of the 63 current fees, 23 fees were consolidated, deleted, or retained at current fee levels (See Exhibit 5), so a cost recovery analysis was conducted on the remaining 40 fees (Exhibit 15). Seven of these current fees included in the analysis are proposed as hourly fees and are included in Exhibit 16. In addition, Department staff identified 15 new fees and provided estimated work times for these new fees (Exhibit 17). The current fees listed for all services exclude the three percent technology fee assessed on all Department fee services.

The Department anticipates additional expenses for one of the new planning fee services: the Transportation Concurrency service. To provide this fee service, \$3,200 in non-labor expense was added to the Land Use Planning program’s cost of service in 2017 to account for the eventual replacement of a transportation concurrency model. The additional expense is based on the replacement cost of the model (\$16,000) prorated over a five-year replacement schedule. This additional expense is not included in the Land Use Planning program’s overall cost of service as detailed in Exhibit 13. The additional cost is only used in determining the cost of service for individual Land Use Planning fees. The annualized cost to replace the transportation concurrency model is estimated to increase the loaded hourly rate for Land Use Planning fees by less than \$1 per hour, from \$146.62 to \$147.43 per hour.

The average cost recovery rate for all current and new fees included in the cost recovery analysis was 43 percent. Only one fee had a cost recovery rate greater than 100 percent: New Wireless Communication Facilities (102%). Examples of cost recovery rates for Land Use Planning fees include:

- Accessory Dwelling Unit (8%)
- Conditional Use Permit (94%)
- Design Commission Study Session (29%)
- Subdivision Preliminary Long Plat (70%)

Exhibit 15
2017 Cost of Service by Individual Planning Fee (Current Fees)

| Fee Description | Cost of Service | Current Fee - 2018 | Cost Recovery (%) |
|--|-----------------|--------------------|-------------------|
| Appeals - Land Use | \$ 7,644 | \$ 925 | 12% |
| Appeals - Code Interpretation | 6,824 | 925 | 14% |
| Critical Areas - Determination | 7,863 | 2,859 | 36% |
| Critical Areas - Reasonable Use Exception | 16,238 | 5,722 | 35% |
| Design Review - Design Commission Study Session | 5,387 | 1,583 | 29% |
| Wireless Communication Facilities - Wireless Communication Facilities-6409 Exemption | 4,425 | 1,583 | 36% |
| Wireless Communication Facilities - New Wireless Communications Facility | 5,750 | 6,332 | 110% |
| Deviations - Changes to Antenna Requirements | 3,033 | 1,907 | 63% |
| SEPA Review (Checklist) - Major | 6,110 | 1,907 | 31% |
| Shoreline Management - Exemption | 1,733 | 462 | 27% |
| Shoreline Management - Permit Revision | 1,757 | 764 | 43% |
| Shoreline Management - Substantial Development Permit | 4,533 | 2,860 | 63% |
| Subdivision Preliminary Long Plat - Long Plat | 28,256 | 19,785 | 70% |
| Subdivision Long Plat - Subdivision Alteration to Existing Plat | 13,610 | 4,767 | 35% |
| Subdivision Long Plat - Final Subdivision Review | 11,284 | 4,749 | 42% |
| Preliminary Subdivision Short Plat | 9,864 | 5,539 | 56% |
| Subdivision Short Plat - Three Lots | 14,408 | 6,648 | 46% |
| Subdivision Short Plat - Four Lots | 14,408 | 7,914 | 55% |
| Subdivision Short Plat - Short Plat Alteration | 8,682 | 5,539 | 64% |
| Subdivision Short Plat - Final Short Plat Approval | 4,855 | 954 | 20% |
| Variances (Plus Hearing Examiners Fee) | 9,279 | 3,814 | 41% |
| Other Land Use - Accessory Dwelling Units (ADU) | 2,317 | 191 | 8% |
| Other Land Use - Code Interpretation Request | 2,353 | 950 | 40% |
| Other Land Use - Comprehensive Plan Amendment (CPA)-Application | 22,365 | 4,387 | 20% |
| Other Land Use - Conditional Use (CUP) | 8,105 | 7,628 | 94% |
| Other Land Use - Lot Line Revision | 4,741 | 3,166 | 67% |
| Other Land Use - Noise Exception - Type 1 | 2,750 | 1,267 | 46% |
| Other Land Use - Noise Exception - Type 2 | 1,118 | 632 | 57% |
| Other Land Use - Noise Exception - Type 3 | 846 | 475 | 56% |
| Other Land Use - Reclassification of Property (Rezoning) | 15,379 | 4,767 | 31% |
| Other Land Use - Zoning Code Text Amendment | 21,681 | 4,387 | 20% |
| Pre-Application Meeting | 1,967 | 488 | 25% |
| SEPA Review (Checklist) - Minor | 2,756 | 572 | 21% |

Exhibit 16

2017 Cost of Service by Individual Planning Fee (Current Fees Proposed to be Hourly Fees)*

| Fee Description | Cost of Service | Current Fee - 2018 | Cost Recovery (%) |
|--|-----------------|--------------------|-------------------|
| Deviations - Changes to Open Space - Set to Hourly Fee | \$ 147 | \$ 1,907 | Hourly Fee |
| Deviations - Critical Areas Setback - Set to Hourly Fee | 147 | 2,860 | Hourly Fee |
| Deviations - Shoreline - Set to Hourly Fee | 147 | 3,814 | Hourly Fee |
| Environmental Review (SEPA) - Environmental Impact Statement or Revision - Set to Hourly Fee | 147 | 2,860 | Hourly Fee |
| Subdivision Long Plat - Fire Review - Set to Hourly Fee | 83 | 130 | Hourly Fee |
| Subdivision Short Plat - Deviation of Acreage Limitation - Set to Hourly Fee | 147 | 983 | Hourly Fee |
| Other Land Use - Code Interpretation Request (after first six hours) - Set to Hourly Fee | 147 | 151 | Hourly Fee |

*The hourly fee for Subdivision Long Plat - Fire Review is performed by the Fire Marshal and does not include DSG indirect and overhead costs.

Exhibit 17

2017 Cost of Service by Individual Planning Fee (New Fees)

| Fee Description | Cost of Service | Current Fee - 2018 | Cost Recovery (%) |
|--|-----------------|--------------------|-------------------|
| Pre-Design Meeting | \$ 1,967 | \$ - | 0% |
| Requests for Letter | 682 | - | 0% |
| Hourly Fee for Planning Work Not Covered by a Permit | 147 | - | 0% |
| Additional Design Review Meeting | 4,236 | - | 0% |
| Business License Review | 75 | - | 0% |
| Appeals - ROW | 10,248 | - | 0% |
| Shoreline - Variance | 10,986 | - | 0% |
| Shoreline - Conditional Use | 10,232 | - | 0% |
| Noise Exception - Type 4 | 2,720 | - | 0% |
| 3rd+ Correction Request | 1,773 | - | 0% |
| Design Review (Code Official) | 3,075 | - | 0% |
| Design Review (Design Commission - Exterior Alterations) | 14,071 | - | 0% |
| Design Review (Design Commission - New Building) | 23,122 | - | 0% |
| Impact Fee Deferral | 59 | - | 0% |
| Transportation Concurrency | 134 | - | 0% |

DEVELOPMENT ENGINEERING

As shown in Exhibit 18, the full cost of Development Engineering fee supported services was \$590,305. Direct services were 37% of the full cost of service, while indirect services represented 20%. Department and Citywide overhead costs were the remaining 43%.

**Exhibit 18
2017 Full Cost of Service for Development Engineering**

| <i>Development Engineering</i> | | Annual Cost Components | | Total Costs | % of Total Costs |
|--|--|------------------------|-------------------|-------------------|------------------|
| | | Labor Costs | Non-Labor Costs | | |
| Direct Costs | Total Direct Services | \$ 172,061 | \$ 3,544 | \$ 175,604 | 29.7% |
| | Contract Services | | \$ 42,179 | \$ 42,179 | 7.1% |
| Subtotal Direct Costs | | \$ 172,061 | \$ 45,723 | \$ 217,784 | 36.8% |
| Indirect Costs | DINADM - Indirect-Administrative Duties | \$ 96,275 | \$ 1,268 | \$ 97,543 | 16.5% |
| | DINCST - Indirect - Customer Service | 19,318 | 288 | 19,606 | 3.3% |
| | DINEGV - Indirect - EGov and MBP | - | - | - | - |
| Subtotal Indirect Costs | | 115,594 | 1,555 | \$ 117,149 | 19.8% |
| Overhead Costs | Department Administration OH - Fee Related | \$ 149,452 | \$ 38,849 | \$ 188,301 | 31.9% |
| | Division OH - Fee Related | - | - | - | - |
| | Citywide OH - Fee Related | - | 67,071 | 67,071 | 11.4% |
| Subtotal Overhead Costs | | \$ 149,452 | \$ 105,921 | \$ 255,372 | 43.3% |
| Total Development Engineering Costs | | \$ 437,106 | \$ 153,199 | \$ 590,305 | 100.0% |

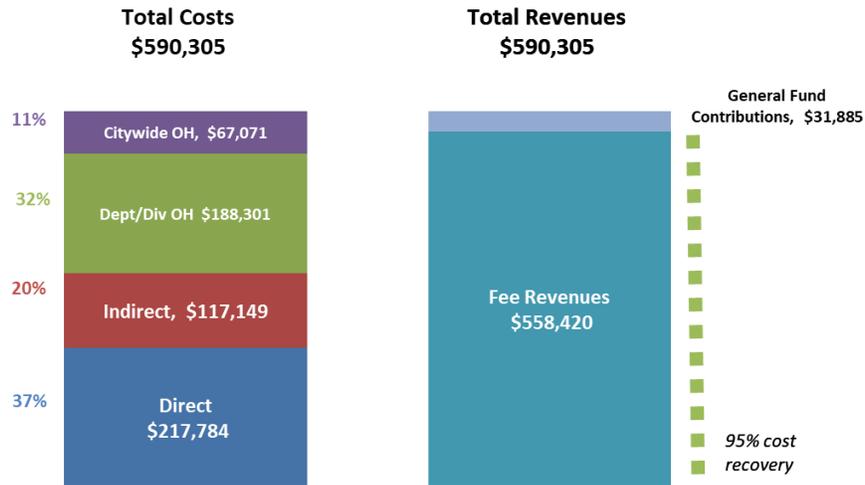
Note: Slight differences may occur due to rounding

In 2017 the overall revenues from Development Engineering fee services were lower than the total costs. Revenue from development engineering permits totaled \$558,420 while calculated costs totaled \$590,305. Based on the analysis, Development Engineering services required approximately \$32,000 in General Fund support in 2017. The overall cost recovery rate is 95%. In comparison to previous fee studies completed by FCS GROUP for the Department, the 2017 cost recovery rate is higher than in 2015 (53 percent) and in 2012 (48 percent). The increase in the cost recovery rate in 2017 is primarily attributed to two changes to the study methodology for Development Engineering.

- Tree permit revenue (\$95,273 in 2017) is allocated to Development Engineering to align with the labor and non-labor costs for the City Arborist, which are allocated to Development Engineering.
- Expenditures related to water, sewer, stormwater, and street utility inspections (\$128,916 in 2017) are recovered by the City's utility funds and, as a result, are not included in the cost recovery analysis for Development Engineering permit fee services.

Exhibit 19 illustrates the cost recovery rate for Development Engineering services in 2017.

Exhibit 19 2017 Cost Recovery for Development Engineering



APPENDIX A – 2017 TIME ESTIMATES

| Building Plan Review & Inspection Services | | | | | | | | |
|---|---|----------------------|-------------------------------|-------------------------------------|---|--|---|---|
| LABOR HOURS | | Total | Cole, Don - Building Official | Gardner, Brent - Building Inspector | Potterf, Mark - Senior Building Inspector | Reece, Gareth - Senior Building Plans Examiner | Skidmore, Paul - Senior Building Plans Examiner | |
| Labor Breakdown | Annual Regular Labor | 10,377 | 2,106 | 2,050 | 2,095 | 2,080 | 2,046 | |
| | Annual Overtime | 0 | | | | | | |
| | Annual Labor | 10,377 | 2,106 | 2,050 | 2,095 | 2,080 | 2,046 | |
| | Annual Leave | 2,068 | 504 | 436 | 444 | 276 | 408 | |
| Total Available Work Hours | | 8,309 | 1,602 | 1,614 | 1,651 | 1,804 | 1,638 | |
| Indirect Activities | DINADM - Indirect-Administrative Duties | 846 | 472 | 58 | 57 | 90 | 170 | |
| | DINCST - Indirect - Customer Service | 20 | 19 | 2 | 0 | 0 | 0 | |
| | DINEGV - Indirect - EGov and MBP | 58 | 58 | 0 | 0 | 0 | 0 | |
| | | 0 | | | | | | |
| Net Annual Labor Related to Direct Services | | 7,384 | 1,053 | 1,554 | 1,594 | 1,714 | 1,468 | |
| Direct Fee Time Categories for Building Plan Review & Inspection Services | Bldg-Com_MF Inspections | 652 | 34 | 275 | 342 | 0 | 0 | |
| | Bldg-Com_MF Plan Review | 1,258 | 301 | 211 | 92 | 0 | 654 | |
| | Bldg-Fire Inspection | 2 | 2 | 0 | 0 | 0 | 0 | |
| | Bldg-Fire Plan Review | 4 | 4 | 0 | 0 | 0 | 0 | |
| | Bldg-OTC Sub Permits | 77 | 9 | 0 | 0 | 51 | 16 | |
| | Bldg-SF Inspection | 2,315 | 150 | 966 | 1,098 | 83 | 18 | |
| | Bldg-SF Plan Review | 2,903 | 420 | 85 | 56 | 1,571 | 772 | |
| | Bldg-Deviation in Wet Season | 12 | 12 | 0 | 0 | 0 | 0 | |
| | Eng-Fire Inspection | 0 | 0 | 0 | 0 | 0 | 0 | |
| | Eng-Fire Plan Review | 0 | 0 | 0 | 0 | 0 | 0 | |
| | Eng-ROW | 0 | 0 | 0 | 0 | 0 | 0 | |
| | Eng-Site Development | 0 | 0 | 0 | 0 | 0 | 0 | |
| | Eng-Storm Review Inspec | 0 | 0 | 0 | 0 | 0 | 0 | |
| | Eng-Tree Review Inspec | 0 | 0 | 0 | 0 | 0 | 0 | |
| | Eng-Utility Inspections | 0 | 0 | 0 | 0 | 0 | 0 | |
| | Pln-Administrative Actions | 0 | 0 | 0 | 0 | 0 | 0 | |
| | Pln-Appeals | 8 | 8 | 0 | 0 | 0 | 0 | |
| | Pln-Business License Review | 0 | 0 | 0 | 0 | 0 | 0 | |
| | Pln-Critical Areas | 0 | 0 | 0 | 0 | 0 | 0 | |
| | Pln-Discretionary Actions | 0 | 0 | 0 | 0 | 0 | 0 | |
| | Pln-Ministerial Actions | 0 | 0 | 0 | 0 | 0 | 0 | |
| | Pln-Other Land Use | 0 | 0 | 0 | 0 | 0 | 0 | |
| | Pln-PreApp Meeting | 9 | 9 | 0 | 0 | 0 | 0 | |
| | Pln-ROW Encroachment | 0 | 0 | 0 | 0 | 0 | 0 | |
| | Pln-Subdivisions | 0 | 0 | 0 | 0 | 0 | 0 | |
| | Pln-Wireless Comm | 0 | 0 | 0 | 0 | 0 | 0 | |
| | Bldg-Zoning Review Perm | 0 | 0 | 0 | 0 | 0 | 0 | |
| | Bldg-Major SF | 0 | 0 | 0 | 0 | 0 | 0 | |
| | Bldg-PreApp Meetings | 30 | 16 | 0 | 6 | 0 | 8 | |
| | Bldg-Sound Transit and East Link | 73 | 69 | 3 | 0 | 1 | 0 | |
| | LastBuilding | 0 | | | | | | |
| | Direct Non-Fee Time Categories for Building Plan Review & Inspection Services | Bldg-Tenant Housing | 3 | 3 | 0 | 0 | 0 | 0 |
| | | Eng-NPDES Compliance | 0 | 0 | 0 | 0 | 0 | 0 |
| Neighborhood Traffic | | 0 | 0 | 0 | 0 | 0 | 0 | |
| TIP Development | | 0 | 0 | 0 | 0 | 0 | 0 | |
| Long Range Planning | | 0 | 0 | 0 | 0 | 0 | 0 | |
| Bldg-Code Enforcement | | 37 | 14 | 14 | 0 | 8 | 0 | |
| Bldg-NonDept Admin | | 0 | 0 | 0 | 0 | 0 | 0 | |
| Bldg-Misc NonRecover | | 1 | 1 | 0 | 0 | 0 | 0 | |
| | | 0 | | | | | | |
| LastBuilding | | 0 | | | | | | |
| Total Direct Hours | | 7,384 | 1,053 | 1,554 | 1,594 | 1,714 | 1,468 | |
| Total Indirect Hours | | 925 | 549 | 60 | 57 | 90 | 170 | |
| Total Direct Hours - Fee Services | | 7,344 | 1,035 | 1,540 | 1,594 | 1,706 | 1,468 | |
| Total Direct Hours - Direct Non-Fee Time Categories | | 41 | 18 | 14 | - | 8 | - | |
| Grand Total | | 8,309 | 1,602 | 1,614 | 1,651 | 1,804 | 1,638 | |

| Land Use Planning Services | | | Anderson, Lauren - Assistant Planner | Gaudette, Nicole - Senior Planner | Leon, Andrew - Planner | Maxim, Evan - Planning Manager | Proebsting, Robin - Senior Planner |
|---|---|--------------|---|---|---------------------------|--------------------------------------|--|
| LABOR HOURS | | Total | | | | | |
| Labor Breakdown | Annual Regular Labor | 9,937 | 2,080 | 2,064 | 1,664 | 2,074 | 2,055 |
| | Annual Overtime | 0 | | | | | |
| | Annual Labor | 9,937 | 2,080 | 2,064 | 1,664 | 2,074 | 2,055 |
| | Annual Leave | 868 | 177 | 206 | 73 | 177 | 236 |
| Total Available Work Hours | | 9,069 | 1,903 | 1,859 | 1,591 | 1,897 | 1,820 |
| Indirect Activities | DINADM - Indirect-Administrative Duties | 907 | 119 | 110 | 126 | 412 | 140 |
| | DINCST - Indirect - Customer Service | 421 | 112 | 133 | 45 | 15 | 116 |
| | DINEGV - Indirect - EGov and MBP | 0 | 0 | 0 | 0 | 0 | 0 |
| Net Annual Labor Related to Direct Services | | 7,741 | 1,672 | 1,616 | 1,420 | 1,470 | 1,564 |
| Direct Fee Time Categories for Land Use Planning Services | Bldg-Com_MF Inspections | 5 | 0 | 0 | 0 | 5 | 0 |
| | Bldg-Com_MF Plan Review | 0 | 0 | 0 | 0 | 0 | 0 |
| | Bldg-Fire Inspection | 0 | 0 | 0 | 0 | 0 | 0 |
| | Bldg-Fire Plan Review | 0 | 0 | 0 | 0 | 0 | 0 |
| | Bldg-OTC Sub Permits | 0 | 0 | 0 | 0 | 0 | 0 |
| | Bldg-SF Inspection | 2 | 0 | 2 | 0 | 0 | 0 |
| | Bldg-SF Plan Review | 27 | 27 | 0 | 0 | 0 | 0 |
| | Blg-Deviation in Wet Season | 19 | 1 | 4 | 14 | 1 | 0 |
| | Eng-Fire Inspection | 0 | 0 | 0 | 0 | 0 | 0 |
| | Eng-Fire Plan Review | 0 | 0 | 0 | 0 | 0 | 0 |
| | Eng-ROW | 46 | 2 | 2 | 0 | 39 | 3 |
| | Eng-Site Development | 7 | 1 | 0 | 0 | 0 | 6 |
| | Eng-Storm Review Inspec | 0 | 0 | 0 | 0 | 0 | 0 |
| | Eng-Tree Review Inspect | 0 | 0 | 0 | 0 | 0 | 0 |
| | Eng-Utility Inspections | 0 | 0 | 0 | 0 | 0 | 0 |
| | Pln-Administrative Actions | 1,361 | 250 | 351 | 432 | 43 | 285 |
| | Pln-Appeals | 151 | 0 | 61 | 0 | 48 | 42 |
| | Pln-Business License Review | 39 | 15 | 5 | 14 | 0 | 5 |
| | Pln-Critical Areas | 349 | 122 | 43 | 75 | 33 | 77 |
| | Pln-Discretionary Actions | 92 | 0 | 38 | 2 | 8 | 44 |
| | Pln-Ministerial Actions | 349 | 87 | 129 | 72 | 13 | 48 |
| | Pln-Other Land Use | 41 | 0 | 27 | 0 | 3 | 11 |
| | Pln-PreApp Meeting | 169 | 19 | 40 | 57 | 12 | 42 |
| | Pln-ROW Encroachment | 33 | 0 | 26 | 0 | 0 | 7 |
| | Pln-Subdivisions | 212 | 66 | 80 | 6 | 5 | 56 |
| | Pln-Wireless Comm | 86 | 3 | 20 | 36 | 1 | 26 |
| | Bldg-Zoning Review Perm | 3,098 | 939 | 621 | 623 | 225 | 690 |
| Pln-Major SF | 156 | 44 | 19 | 39 | 2 | 53 | |
| Pln-PreApp Meetings | 248 | 49 | 39 | 52 | 12 | 96 | |
| Pln-Sound Transit and East Link | 12 | 6 | 6 | 0 | 0 | 0 | |
| LastPlanning | 0 | | | | | | |
| Direct Non-Fee Time Categories for Land Use Planning Services | Bldg-Tenant Housing | 0 | 0 | 0 | 0 | 0 | 0 |
| | Eng-NPDES Compliance | 0 | 0 | 0 | 0 | 0 | 0 |
| | Neighborhood Traffic | 0 | 0 | 0 | 0 | 0 | 0 |
| | TIP Development | 8 | 0 | 8 | 0 | 0 | 0 |
| | Long Range Planning | 1,191 | 28 | 85 | 0 | 1,018 | 61 |
| | Pln-Code Enforcement | 21 | 11 | 1 | 0 | 3 | 6 |
| | Pln-NonDept Admin | 18 | 3 | 9 | 0 | 0 | 6 |
| | Pln-Misc NonRecover | 0 | 0 | 0 | 0 | 0 | 0 |
| | | 0 | | | | | |
| | LastPlanning | 0 | | | | | |
| Total Direct Hours | | 6,503 | 1,672 | 1,616 | 1,420 | 1,470 | 1,564 |
| Total Indirect Hours | | 1,328 | 231 | 243 | 171 | 427 | 256 |
| Total Direct Hours - Fee Services | | 6,503 | 1,630 | 1,513 | 1,420 | 450 | 1,490 |
| Total Direct Hours - Direct Non-Fee Time Categories | | 1,238 | 42 | 102 | - | 1,020 | 73 |
| Grand Total | | 9,069 | 1,903 | 1,859 | 1,591 | 1,897 | 1,820 |

| Development Engineering LABOR HOURS | | Total | Ding, Ruji - Senior Development Engineer | Yamashita, Patrick - City Engineer | Kenney, John - Private Development Arborist |
|---|---|--------------|---|--|--|
| Labor Breakdown | Annual Regular Labor | 5,495 | 2,080 | 1,831 | 1,584 |
| | Annual Overtime | 0 | | | |
| | Annual Labor | 5,495 | 2,080 | 1,831 | 1,584 |
| | Annual Leave | 700 | 309 | 280 | 112 |
| Total Available Work Hours | | 4,795 | 1,771 | 1,552 | 1,472 |
| Indirect Activities | DINADM - Indirect-Administrative Duties | 961 | 85 | 876 | 0 |
| | DINCST - Indirect - Customer Service | 252 | 180 | 72 | 0 |
| | DINEGV - Indirect - EGov and MBP | 0 | 0 | 0 | 0 |
| Net Annual Labor Related to Direct Services | | 3,582 | 1,506 | 604 | 1,472 |
| Direct Fee Time Categories for Development Engineering | Bldg-Com_MF Inspections | 0 | 0 | 0 | 0 |
| | Bldg-Com_MF Plan Review | 0 | 0 | 0 | 0 |
| | Bldg-Fire Inspection | 0 | 0 | 0 | 0 |
| | Bldg-Fire Plan Review | 0 | 0 | 0 | 0 |
| | Bldg-OTC Sub Permits | 0 | 0 | 0 | 0 |
| | Bldg-SF Inspection | 0 | 0 | 0 | 0 |
| | Bldg-SF Plan Review | 0 | 0 | 0 | 0 |
| | Blg-Deviation in Wet Season | 0 | 0 | 0 | 0 |
| | Eng-Fire Inspection | 0 | 0 | 0 | 0 |
| | Eng-Fire Plan Review | 1 | 0 | 1 | 0 |
| | Eng-ROW | 74 | 10 | 64 | 0 |
| | Eng-Site Development | 343 | 322 | 21 | 0 |
| | Eng-Storm Review Inspec | 608 | 595 | 13 | 0 |
| | Eng-Tree Review Inspect | 1,099 | 0 | 1 | 1,098 |
| | Eng-Utility Inspections | 38 | 36 | 2 | 0 |
| | Pln-Administrative Actions | 49 | 9 | 40 | 0 |
| | Pln-Appeals | 14 | 0 | 14 | 0 |
| | Pln-Business License Review | 0 | 0 | 0 | 0 |
| | Pln-Critical Areas | 0 | 0 | 0 | 0 |
| | Pln-Discretionary Actions | 1 | 0 | 1 | 0 |
| | Pln-Ministerial Actions | 35 | 32 | 3 | 0 |
| | Pln-Other Land Use | 31 | 0 | 31 | 0 |
| | Pln-PreApp Meeting | 76 | 64 | 12 | 0 |
| | Pln-ROW Encroachment | 149 | 141 | 8 | 0 |
| | Pln-Subdivisions | 53 | 51 | 2 | 0 |
| | Pln-Wireless Comm | 0 | 0 | 0 | 0 |
| | Bldg-Zoning Review Perm | 380 | 80 | 0 | 300 |
| Eng-Major SF | 0 | 0 | 0 | 0 | |
| Eng-PreApp Meetings | 252 | 164 | 14 | 74 | |
| Eng-Sound Transit and East Link | 14 | 0 | 14 | 0 | |
| | 0 | | | | |
| LastOther1 | 0 | | | | |
| Direct Non-Fee Time Categories for Development Engineering | Bldg-Tenant Housing | 0 | 0 | 0 | 0 |
| | Eng-NPDES Compliance | 146 | 0 | 146 | 0 |
| | Neighborhood Traffic | 14 | 0 | 14 | 0 |
| | TIP Development | 164 | 0 | 164 | 0 |
| | Long Range Planning | 11 | 0 | 11 | 0 |
| | Eng-Code Enforcement | 18 | 1 | 17 | 0 |
| | Eng-NonDept Admin | 1 | 0 | 1 | 0 |
| | Eng-Misc NonRecover | 11 | 1 | 10 | 0 |
| | | 0 | | | |
| | | 0 | | | |
| | LastOther1 | 0 | | | |
| Total Direct Hours | | 3,217 | 1,506 | 604 | 1,472 |
| Total Indirect Hours | | 1,213 | 265 | 948 | 0 |
| Total Direct Hours - Fee Services | | 3,217 | 1,504 | 241 | 1,472 |
| Total Direct Hours - Direct Non-Fee Time Categories | | 365 | 2 | 363 | 0 |
| Grand Total | | 4,795 | 1,771 | 1,552 | 1,472 |

| Administration LABOR HOURS | | Total | Greenberg, Scott - Director | Van Corp, Alison - Administrati ve Services Manager | Henderson, David - Permit Center Supervisor | Mercier, Holly - Permit Process Supervisor | Allerdice, Norine - Permit Technician | Larson, Andrea - Administrati ve Assistant | Serfling, Jimmi L - Code Enforcement Officer | Zlateff, Dana - Code Enforcement Officer |
|---|---|---------------|-----------------------------------|---|---|--|--|---|--|---|
| Labor Breakdown | Annual Regular Labor | 13,497 | 2,000 | 2,072 | 2,160 | 1,784 | 2,080 | 2,080 | 1,221 | 100 |
| | Annual Overtime | 0 | | | | | | | | |
| | Annual Labor | 13,497 | 2,000 | 2,072 | 2,160 | 1,784 | 2,080 | 2,080 | 1,221 | 100 |
| | Annual Leave | 2,061 | 442 | 322 | 339 | 141 | 196 | 357 | 257 | 8 |
| Total Available Work Hours | | 11,436 | 1,559 | 1,751 | 1,821 | 1,643 | 1,884 | 1,723 | 964 | 92 |
| Indirect Activities | DINADM - Indirect-Administrative Duties | 3,802 | 645 | 1,075 | 141 | 270 | 308 | 1,262 | 94 | 7 |
| | DINCST - Indirect - Customer Service | 4,045 | 2 | 122 | 1,435 | 1,302 | 1,182 | 2 | 1 | 0 |
| | DINEGV - Indirect - EGov and MBP | 53 | 0 | 0 | 0 | 53 | 0 | 0 | 0 | 0 |
| | | 0 | | | | | | | | |
| Net Annual Labor Related to Direct Services | | 3,536 | 911 | 554 | 245 | 18 | 395 | 459 | 869 | 85 |
| Direct Fee Time Categories for Administration | Bldg-Com_MF Inspections | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| | Bldg-Com_MF Plan Review | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| | Bldg-Fire Inspection | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| | Bldg-Fire Plan Review | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| | Bldg-OTC Sub Permits | 132 | 0 | 0 | 132 | 0 | 0 | 0 | 0 | 0 |
| | Bldg-SF Inspection | 8 | 0 | 0 | 8 | 0 | 0 | 0 | 0 | 0 |
| | Bldg-SF Plan Review | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| | Blg-Deviation in Wet Season | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| | Eng-Fire Inspection | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| | Eng-Fire Plan Review | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| | Eng-ROW | 116 | 4 | 0 | 0 | 0 | 0 | 112 | 0 | 0 |
| | Eng-Site Development | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| | Eng-Storm Review Inspec | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| | Eng-Tree Review Inspect | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| | Eng-Utility Inspections | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| | Pln-Administrative Actions | 84 | 84 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| | Pln-Appeals | 37 | 20 | 0 | 0 | 0 | 0 | 16 | 0 | 0 |
| | Pln-Business License Review | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| | Pln-Critical Areas | 4 | 2 | 0 | 0 | 0 | 0 | 2 | 0 | 0 |
| | Pln-Discretionary Actions | 25 | 0 | 0 | 0 | 0 | 0 | 25 | 0 | 0 |
| | Pln-Ministerial Actions | 4 | 2 | 0 | 0 | 0 | 0 | 2 | 0 | 0 |
| | Pln-Other Land Use | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| | Pln-PreApp Meeting | 2 | 1 | 1 | 0 | 0 | 0 | 0 | 0 | 0 |
| | Pln-ROW Encroachment | 1 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| | Pln-Subdivisions | 4 | 4 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| | Pln-Wireless Comm | 2 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| Bldg-Zoning Review Perm | 11 | 11 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Admin-Major SF | 522 | 1 | 0 | 54 | 8 | 385 | 73 | 0 | 0 | |
| Adm-PreApp Meetings | 58 | 0 | 0 | 50 | 8 | 0 | 0 | 0 | 0 | |
| Adm-Sound Transit and East Link | 18 | 16 | 0 | 0 | 2 | 0 | 0 | 0 | 0 | |
| | 0 | | | | | | | | | |
| LastOther2 | 0 | | | | | | | | | |
| Direct Non-Fee Time Categories for Administration | Bldg-Tenant Housing | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| | Eng-NPDES Compliance | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| | Neighborhood Traffic | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| | TIP Development | 136 | 134 | 2 | 0 | 0 | 0 | 0 | 0 | 0 |
| | Long Range Planning | 1,167 | 607 | 469 | 0 | 0 | 0 | 91 | 0 | 0 |
| | Adm-Code Enforcement | 1,047 | 11 | 82 | 1 | 0 | 0 | 0 | 869 | 85 |
| | Adm-NonDept Admin | 147 | 0 | 0 | 0 | 0 | 10 | 137 | 0 | 0 |
| | Adm-Misc NonRecover | 13 | 13 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| | | 0 | | | | | | | | |
| | | 0 | | | | | | | | |
| | LastOther2 | 0 | | | | | | | | |
| Total Direct Hours | | 1,026 | 911 | 554 | 245 | 18 | 395 | 459 | 869 | 85 |
| Total Indirect Hours | | 7,900 | 647 | 1,197 | 1,576 | 1,625 | 1,489 | 1,264 | 95 | 7 |
| Total Direct Hours - Fee Services | | 1,026 | 147 | 1 | 244 | 18 | 385 | 230 | 0 | 0 |
| Total Direct Hours - Direct Non-Fee Time Categories | | 2,510 | 764 | 553 | 1 | 0 | 10 | 229 | 869 | 85 |
| Grand Total | | 11,436 | 1,559 | 1,751 | 1,821 | 1,643 | 1,884 | 1,723 | 964 | 92 |

APPENDIX B – PLANNING PERMIT PROCESSING TIMES

Current Land Use Planning Fees*

| Fee Description | Direct Time (in hours) |
|---|---------------------------|
| Appeals - Land Use | 60.50 |
| Appeals - Code Interpretation | 51.50 |
| Critical Areas - Determination | 49.50 |
| Critical Areas - Reasonable Use Exception | 108.00 |
| Design Review - Design Commission Study Session | 40.00 |
| Wireless Communication Facilities - Wireless Communication Facilities-6409 Exemption | 32.50 |
| Wireless Communication Facilities - New Wireless Communications Facility | 39.00 |
| Deviations - Changes to Antenna Requirements | 23.50 |
| SEPA Review (Checklist) - Major | 39.00 |
| Shoreline Management - Exemption | 12.50 |
| Shoreline Management - Permit Revision | 11.00 |
| Shoreline Management - Substantial Development Permit | 34.00 |
| Subdivision Preliminary Long Plat - Long Plat | 177.00 |
| Subdivision Long Plat - Subdivision Alteration to Existing Plat | 93.00 |
| Subdivision Long Plat - Final Subdivision Review | 66.50 |
| Preliminary Subdivision Short Plat | 62.50 |
| Subdivision Short Plat - Three Lots | 80.50 |
| Subdivision Short Plat - Four Lots | 80.50 |
| Subdivision Short Plat - Short Plat Alteration | 54.50 |
| Subdivision Short Plat - Final Short Plat Approval | 31.50 |
| Variances (Plus Hearing Examiners Fee) | 62.00 |
| Other Land Use - Accessory Dwelling Units (ADU) | 18.50 |
| Other Land Use - Code Interpretation Request | 18.00 |
| Other Land Use - Comprehensive Plan Amendment (CPA)-Application | 155.00 |
| Other Land Use - Conditional Use (CUP) | 58.00 |
| Other Land Use - Lot Line Revision | 31.00 |
| Other Land Use - Noise Exception - Type 1 | 10.50 |
| Other Land Use - Noise Exception - Type 2 | 4.50 |
| Other Land Use - Noise Exception - Type 3 | 3.50 |
| Other Land Use - Reclassification of Property (Rezoning) | 109.50 |
| Other Land Use - Zoning Code Text Amendment | 150.00 |
| Pre-Application Meeting | 11.50 |
| SEPA Review (Checklist) - Minor | 21.50 |

*Note: Excludes 7 current fees that are proposed as hourly fees (see Exhibit 16 for list of hourly fees)

New Land Use Planning Fees

| Fee Description | Direct Time (in hours) |
|--|-----------------------------------|
| Pre-Design Meeting | 11.50 |
| Requests for Letter | 4.50 |
| Hourly Fee for Planning Work Not Covered by a Permit | - |
| Additional Design Review Meeting | 32.50 |
| Business License Review | 0.50 |
| Appeals - ROW | 72.00 |
| Shoreline - Variance | 80.00 |
| Shoreline - Conditional Use | 73.00 |
| Noise Exception - Type 4 | 10.00 |
| 3rd+ Correction Request | 11.50 |
| Design Review (Code Official) | 23.50 |
| Design Review (Design Commission - Exterior Alterations) | 99.00 |
| Design Review (Design Commission - New Building) | 160.50 |
| Impact Fee Deferral | 1.00 |
| Transportation Concurrency | 1.50 |

CITY OF MERCER ISLAND
RESOLUTION NO. 1567

A RESOLUTION OF THE CITY OF MERCER ISLAND, WASHINGTON
ADJUSTING DEVELOPMENT AND CONSTRUCTION PERMIT FEES
EFFECTIVE JANUARY 1, 2020 AND THEREAFTER

WHEREAS, fees are charged for services provided by Community Planning and Development for the development of land and construction of structures which includes the review of permit applications, issuance of permits, field inspections, and related actions; and

WHEREAS, the City Council’s Management and Budget Policies indicate that development and construction permit fees should be based on the actual costs of providing services; and

WHEREAS, the cost of providing such services has been tracked since 1999; and

WHEREAS, in November 1999, Resolution No. 1260 set the initial development and construction fees for Mercer Island, which was subsequently ratified and approved by Ordinance No. 00-14; and

WHEREAS, Resolution No. 1491 established that fees shall be adjusted annually, beginning January 1, 2015 and continuing each January 1 thereafter, using an annual escalator that shall be determined by the Finance Director by comparing the estimated salaries and benefits in the coming year to the actual salaries and benefits in the current year for all Development Services Group staff, excluding contract employees; and

WHEREAS, on May 16, 2016, the City Council adopted new target cost recovery levels of 80% for Planning Services and Engineering Services while retaining 95% target cost recovery level for Building Services; and

WHEREAS, the financial sustainability consultant recommended that the fees collected for Engineering and Planning Services be increased to achieve a target recovery level of 90%, a 10% increase in the recovery rate over the target of 80% established by City Council in 2016. The City recommends the adoption of amended fees as set forth in Attachment A to this resolution that will bring the City closer to this new cost recovery target for Engineering and Planning Services;

NOW, THEREFORE, BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF MERCER ISLAND, WASHINGTON, AS FOLLOWS:

Section 1. Repealer. The following Resolutions are hereby repealed:

- A. Resolution No. 1260, enacted November 15, 1999
- B. Resolution No. 1336, enacted June 7, 2004
- C. Resolution No. 1383, enacted December 11, 2006
- D. Resolution No. 1419, enacted November 16, 2009
- E. Resolution No. 1436, enacted November 15, 2010
- F. Resolution No. 1442, enacted October 17, 2011
- G. Resolution No. 1474, enacted November 18, 2013
- H. Resolution No. 1491, enacted November 17, 2014
- I. Resolution No. 1516, enacted June 20, 2016
- J. Resolution No. 1555, enacted November 20, 2018

Section 2. Development and Construction Permit Fee Schedule.

Consistent with the authority set forth in titles 17 and 19 of the Mercer Island City Code for the establishment and collection of development and construction fees, Attachment A to this Resolution amends the Development and Construction Fee Schedule otherwise established by Resolution No. 1555. The Development and Construction Fee Schedule is hereby amended as reflected in Attachment A.

Consistent with the authority set forth in titles 17 and 19 of the Mercer Island City Code for the establishment and collection of development and construction fees, Attachment B to this Resolution amends the Building Valuation Data Table as otherwise established by Resolution No. 1555. The Building Valuation Data Table contained in Resolution No. 1555 is hereby amended as reflected in Attachment B.

Consistent with the authority set forth in titles 17 and 19 of the Mercer Island City Code for the establishment and collection of development and construction fees, Attachment C to this Resolution establishes the Building Permit Calculation table.

Section 3.

The fees set forth in Attachment A shall be adjusted annually, beginning January 1, 2021, based on the estimated annual growth in personnel costs of Community Planning and Development staff as determined by the Finance Director. The annual escalator shall be determined by comparing the estimated salaries and benefits in the coming year to the actual salaries and benefits in the current year for all Community Planning and Development staff, excluding contract employees.

Section 4. Effective Date.

Sections 1, 2 and 3 of this resolution shall take effect and be in force on and after January 1, 2020. Nothing contained herein shall affect the amount of collection of rates, fees, and charges established prior to January 1, 2020.

PASSED BY THE CITY COUNCIL OF THE CITY OF MERCER ISLAND, WASHINGTON, AT ITS REGULAR MEETING ON THE 3RD DAY OF DECEMBER 2019.

CITY OF MERCER ISLAND

Debbie Bertlin, Mayor

ATTEST:

Deborah A. Estrada, City Clerk

ATTACHMENT A

Item 4.

2020 Development and Construction Fee Schedule

| FEE NAME | FEE AMOUNT | |
|---|--------------|--------------|
| PERMIT FEES (all fees include a 3% technology fee) | 2019 | 2020 |
| Appeals (fee refunded if appeal is granted) | | |
| Building, Land Use, Right of Way, Code Compliance, Code Interpretation (plus actual costs of file preparation and verbatim transcripts) | \$ 990.86 | \$ 990.86 |
| Critical Areas | | |
| Reasonable Use Exception | \$ 6,129.53 | \$ 15,654.73 |
| Public Agency Exception | | \$ 15,654.73 |
| Critical Area Review Type 1 (plus actual cost of peer review) | | \$ 708.60 |
| Critical Area Review Type 2 (plus actual cost of peer review) | \$ 3,062.19 | \$ 7,580.56 |
| Design Review | | |
| Pre-Design Meeting | \$ 1,686.11 | \$ 1,896.35 |
| Design Review (Code Official) | \$ 2,634.74 | \$ 2,964.55 |
| Design Commission Study Session | \$ 4,616.46 | \$ 5,193.50 |
| Additional (Optional) Design Commission Meeting | \$ 3,629.72 | \$ 4,083.84 |
| Design Review (Design Commission - Exterior Alteration) | \$ 12,058.21 | \$ 13,565.57 |
| Design Review (Design Commission - New Building) | \$ 19,815.14 | \$ 22,291.46 |
| Wireless Communication Facility | \$ 4,927.52 | \$ 5,543.46 |
| Wireless Communications Facilities -6409 Exemption | \$ 3,792.46 | \$ 4,266.05 |
| Wireless Communication Facility - Small Cells (plus \$100 for each additional facility greater than 5) | | \$ 500.00 |
| Deviations | | |
| Changes to Antenna Requirements | \$ 2,598.69 | \$ 2,924.05 |
| Changes to Open Space (hourly rate) | \$ 125.66 | \$ 141.72 |
| Critical Areas Setback (hourly rate) | \$ 125.66 | \$ 141.72 |
| Shoreline (hourly rate) | \$ 125.66 | \$ 141.72 |
| Wet Season Construction Moratorium | \$ 1,065.02 | \$ 1,065.02 |
| Environmental Review (SEPA) | | |
| SEPA Review | \$ 2,361.79 | \$ 2,657.00 |
| Environmental Impact Statement/Major Development (hourly rate to apply in addition to SEPA Review fee) | \$ 125.66 | \$ 141.72 |
| Shoreline Management | | |
| Exemption | \$ 1,485.26 | \$ 1,670.75 |
| Permit Revision | \$ 1,505.86 | \$ 1,693.89 |
| Shoreline Variance | \$ 9,414.20 | \$ 10,591.38 |
| Shoreline Conditional Use Permit | \$ 8,768.39 | \$ 9,864.47 |
| Substantial Development Permit | \$ 3,884.13 | \$ 4,370.17 |
| Subdivision Long Plat | | |
| Preliminary Long Plat | \$ 24,214.27 | \$ 27,241.04 |
| Long Plat - Plat Alteration | \$ 11,663.72 | \$ 13,121.13 |
| Long Plat - Final Plat | \$ 9,669.64 | \$ 10,878.68 |
| Subdivision Short Plat | | |
| Two Lots | \$ 8,453.21 | \$ 9,509.69 |
| Three Lots | \$ 12,346.61 | \$ 13,890.46 |
| Four Lots | \$ 12,346.61 | \$ 13,890.46 |
| Deviation of Acreage Limitation (hourly rate) | \$ 125.66 | \$ 141.72 |
| Short Plat Alteration | \$ 7,439.69 | \$ 8,370.14 |
| Final Short Plat Approval | \$ 4,160.17 | \$ 4,680.61 |
| Variances (Plus Hearing Examiner Fee) | | |
| Variance | \$ 7,951.60 | \$ 8,945.70 |
| Other Land Use | | |
| Accessory Dwelling Units (ADU) | \$ 204.97 | \$ 204.97 |
| Code Interpretation Request (plus hourly rate over 6 hrs) | \$ 1,017.64 | \$ 2,268.48 |
| Comprehensive Plan Amendment (CPA) - Application | \$ 1,916.83 | \$ 2,156.16 |
| Comprehensive Plan Amendment (CPA) - Additional fee if added to final docket | \$ 17,249.41 | \$ 21,561.65 |
| Conditional Use (CUP) | \$ 6,945.29 | \$ 7,813.87 |
| Development Agreement (hourly) | \$ 125.66 | \$ 141.72 |
| Lot Line Revision | \$ 4,063.35 | \$ 4,570.70 |
| Impact Fee Deferral Review (School, Park, Transportation) | \$ 50.47 | \$ 56.88 |
| Noise Exception - Type 1 | \$ 2,356.64 | \$ 2,651.22 |
| Noise Exception - Type 2 | \$ 957.90 | \$ 1,077.84 |
| Noise Exception - Type 3 | \$ 1,046.48 | \$ 815.61 |

| | | | |
|--|--------------|----|-----------|
| Noise Exception - Type 4 | \$ 2,330.89 | \$ | |
| Planning Services (not otherwise associated with a permit or review) (hourly rate) | \$ 125.66 | \$ | 141.72 |
| Requests for Letter | \$ 584.01 | \$ | 657.50 |
| Reclassification of Property (Rezoning) | \$ 13,178.85 | \$ | 14,826.59 |
| Public Notice Sign Fee (per sign) | \$ 51.50 | \$ | 51.50 |
| 3rd+ Correction Request | \$ 1,519.25 | \$ | 1,709.31 |
| Transportation Concurrency | \$ 114.33 | \$ | 129.19 |
| Zoning Code Text Amendment | \$ 18,580.17 | \$ | 20,902.22 |
| Fire Review Fees Associated with Land Use Applications | | | |
| Fire Review - Reasonable Use Exception (plus hourly rate if review exceeds 1 hour) | | \$ | 139.05 |
| Fire Review - Design Review, Design Commission - Exterior Alteration (plus hourly rate if review exceeds 1 hour) | | \$ | 139.05 |
| Fire Review - Design Review, Design Commission - New Building (plus hourly rate if review exceeds 1 hour) | | \$ | 139.05 |
| Fire Review - Wireless Communications Facilities -6409 Exemption (plus hourly rate if review exceeds 1 hour) | | \$ | 139.05 |
| Fire Review - New Wireless Communications Facility (plus hourly rate if review exceeds 2 hours) | | \$ | 278.10 |
| Fire Review - Shoreline Variance (plus hourly rate if review exceeds 1 hour) | | \$ | 139.05 |
| Fire Review - Shoreline Conditional Use Permit (plus hourly rate if review exceeds 1 hour) | | \$ | 139.05 |
| Fire Review - Preliminary Long Plat (plus hourly rate if review exceeds 2 hours) | | \$ | 278.10 |
| Fire Review - Long Plat - Final Plat (plus hourly rate if review exceeds 1.5 hours) | | \$ | 208.58 |
| Fire Review - Short Plat, Two Lots (plus hourly rate if review exceeds 2 hours) | | \$ | 278.10 |
| Fire Review - Short Plat, Three Lots (plus hourly rate if review exceeds 2 hours) | | \$ | 278.10 |
| Fire Review - Short Plat, Four Lots (plus hourly rate if review exceeds 2 hours) | | \$ | 278.10 |
| Fire Review - Final Short Plat Approval (plus hourly rate if review exceeds 1 hour) | | \$ | 139.05 |
| Fire Review - Variance (plus hourly rate if review exceeds 1 hour) | | \$ | 139.05 |
| Fire Review - Accessory Dwelling Units (ADU) (plus hourly rate if review exceeds 1 hour) | | \$ | 139.05 |
| Fire Review (hourly rate) | \$ 139.05 | \$ | 139.05 |
| Public Right-of-Way (ROW) Use | | | |
| Right-of-Way Encroachment Agreement (also requires separate ROW Use Permit) (plus hourly rate if review exceeds 4.5 hours) | \$ 606.67 | \$ | 625.73 |
| Miscellaneous ROW Use (plus hourly rate if review exceeds 1.5 hours) | \$ 201.88 | \$ | 208.58 |
| Type A - Underground Improvements in Unpaved Area (plus hourly rate is review exceeds 2 hours) | \$ 201.88 | \$ | 278.10 |
| Type B - Surface Improvements (plus hourly rate if review exceeds 3 hours) | \$ 404.79 | \$ | 417.15 |
| Type C - Underground Improvements in Paved Area (plus hourly rate if review exceeds 4.5 hours) | \$ 606.67 | \$ | 625.73 |
| Engineering Review and Inspection | | | |
| Engineerring Review of Construction Permit (hourly) | \$ 223.51 | \$ | 223.51 |
| Storm Drainage Review & Inspection (hourly, 2-hour minimum) | \$ 223.51 | \$ | 223.51 |
| Plat Improvement/Site Development Review and Inspection (hourly, 10 hour deposit required at application) | \$ 223.51 | \$ | 223.51 |
| Water Service | | | |
| Water Service Order (Permit Processing Fee Only) | \$ 646.84 | \$ | 646.84 |
| Side Sewer | | | |
| Side Sewer Revision/Modification/Repair | \$ 278.10 | \$ | 278.10 |
| Side Sewer Disconnect | \$ 278.10 | \$ | 278.10 |
| Side Sewer Connection | \$ 278.10 | \$ | 278.10 |
| Tree Removal | | | |
| Single Family: 1 - 3 trees | \$ 173.04 | \$ | 173.04 |
| Single Family: 4 -10 trees | \$ 434.66 | \$ | 434.66 |
| Single Family: More than 10 trees | \$ 1,215.40 | \$ | 1,215.40 |
| Non Single Family | \$ 1,215.40 | \$ | 1,215.40 |
| Arborist review of Construction Permit | \$ 347.11 | \$ | 347.11 |
| Limited Inspection Permits | | | |
| Demolition - Single Family | \$ 349.17 | \$ | 349.17 |
| Demolition - Non Single Family | \$ 609.76 | \$ | 609.76 |
| Land Clearing | \$ 173.04 | \$ | 173.04 |
| Grading: 50-100 c.y. | \$ 232.78 | \$ | 232.78 |
| Grading: 101-1,000 c.y. | \$ 329.60 | \$ | 329.60 |
| Grading: 1,001-5,000 c.y. | \$ 430.54 | \$ | 430.54 |
| Grading: 5,001-10,000 c.y. | \$ 567.53 | \$ | 567.53 |
| Grading:10,000+ c.y. | \$ 701.43 | \$ | 701.43 |
| State Energy Compliance - Residential New | \$ 420.24 | \$ | 420.24 |
| State Energy Compliance - Residential Alteration | \$ 212.18 | \$ | 212.18 |
| State Energy Compliance - Non-Residential New | \$ 700.40 | \$ | 700.40 |
| State Energy Compliance - Non-Residential Alteration | \$ 212.18 | \$ | 212.18 |
| Inspections Outside Normal Business Hours (hourly, min 2 hour charge) | \$ 139.05 | \$ | 139.05 |
| Re-Inspection (Assessed Under Provisions of Section 305.8) (hourly) | \$ 139.05 | \$ | 139.05 |
| Partial Inspections | \$ 71.07 | \$ | 71.07 |
| Plan Review and Inspections (not otherwise specified) (hourly) | \$ 139.05 | \$ | 139.05 |

| | | | |
|--|----|----------|-------------|
| Review of Special Foundations/Shoring | | | |
| Single Family | \$ | 839.45 | \$ 839.45 |
| Multi-family/Commercial | \$ | 1,260.72 | \$ 1,260.72 |
| Plan Review/Insp. Of Geotech Report | | | |
| Single Family | \$ | 560.32 | \$ 560.32 |
| Multi-family/Commercial | \$ | 1,120.64 | \$ 1,120.64 |
| Temporary Cert. of Occupancy | | | |
| Single Family (plus hourly rate over 4hrs) | \$ | 560.32 | \$ 560.32 |
| Multi-family/Commercial (plus hourly rate over 36hrs) | \$ | 5,038.76 | \$ 5,038.76 |
| Additional Inspections (minimum charge one hour) | \$ | 139.05 | \$ 139.05 |
| Fire Protection Review - Building Permits | | | |
| Residential Remodels & Substantial Alt Review | | | \$ 139.05 |
| New SFR < 10,000 SF | | | \$ 139.05 |
| New SFR > 10,000 SF | | | \$ 695.25 |
| Non Single Family Review < 3,000 SF | | | \$ 139.05 |
| Non Single Family Review < 5,000 SF | | | \$ 278.10 |
| Non Single Family Review 5,000 - 50,000 SF | | | \$ 2,781.00 |
| Non Single Family Review 50,000 SF + | | | \$ 5,562.00 |
| Non Single Family T1 | | | \$ 139.05 |
| Additional Fire Protection Plan Review (hourly) | | | \$ 139.05 |
| Fire Protection Permits | | | |
| Fire Protection Plan Review and Inspection (hourly) | \$ | 139.05 | \$ 139.05 |
| Access Gate (combo permit - includes low voltage electrical inspection fee) | | | \$ 237.57 |
| Backflow Inspection Fee | | | \$ 173.04 |
| Fire Alarm Commercial: NFPA 72 Low Voltage System (combo permit - includes one hour of electrical inspection) | | | \$ 1,418.98 |
| Fire Alarm Communicator - AES Radio Transmitter (combo permit - includes low voltage electrical inspection fee) | | | \$ 237.57 |
| Fire Alarm Residential: NFPA 72 Low Voltage System (combo permit -includes low voltage electrical inspection fee) | | | \$ 376.62 |
| Fire Alarm Tenant Improvement: Low Voltage Fire Alarm System (combo permit - includes low voltage electrical inspection fee) | | | \$ 723.73 |
| Fire Code Alternate | \$ | 429.21 | \$ 429.21 |
| Fire Hydrant | | | \$ 556.20 |
| Fire Pump (combo permit - includes low voltage electrical inspection fee) | | | \$ 723.73 |
| Fire Sprinkler Commercial: NFPA 13 - Per Riser | | | \$ 1,251.45 |
| Fire Sprinkler OTC- Head Change/Relocate/Add/Cap | | | \$ 278.10 |
| Fire Sprinkler Residential (includes backflow inspection fee) | | | \$ 730.27 |
| Fire Sprinkler T1 - Commercial and Residential | | | \$ 556.20 |
| Fuel Tanks | \$ | 173.04 | \$ 173.04 |
| Kitchen Hood - Extinguishing System (combo permit - includes low voltage electrical inspection fee) | | | \$ 445.63 |
| LP Gas | | | \$ 278.10 |
| Operational Permits (hourly) | \$ | 194.67 | \$ 139.05 |
| Special Fire Extinguishing System (combo permit - includes low voltage electrical inspection fee) | | | \$ 723.73 |
| Standpipe Commercial | | | \$ 1,807.65 |
| Underground Fire Main - NFPA 24 Sprinkler Supply Line | | | \$ 695.25 |
| Wood/Pellet Stove | \$ | 419.21 | \$ 419.21 |
| Other Building Related Fees | | | |
| Formal Pre-Application Conference/Second Meeting (2nd meeting is 1/2 cost of 1st meeting) | \$ | 523.24 | \$ 523.24 |
| Intake Screening (2nd screening is 1/2 cost of 1st screening) | \$ | 523.24 | \$ 523.24 |
| Building Plan Revisions (two hour minimum) | \$ | 139.05 | \$ 139.05 |
| Stop Work Fee (Double building permit fee) | | | \$ 139.05 |
| Building Appeal (plus actual cost of file preparation) | \$ | 990.86 | \$ 990.86 |
| OVER THE COUNTER PERMIT FEES (all fees include a 3% technology fee) | | | |
| Over the counter permits will be charged the following minimum fees, unless the issuance, plan review and unit fees exceed the minimum fee. When the minimum fee is exceeded, the permit will be charged based on the sum of the applicable issuance, plan review and unit fees. | | | |
| Residential Minimum Permit Fee | | \$173.04 | \$173.04 |
| Minimum Permit Fee Exceptions: | | | |
| Temporary Power Service | | \$96.82 | \$96.82 |
| Water Heater Exchange (no new gas piping) | | \$96.82 | \$96.82 |
| Water Supply Piping | | \$96.82 | \$96.82 |

| | | |
|---|----------|----------|
| Repair of existing plumbing, water service, gas piping | \$96.82 | |
| Forced Air Exchange - same type | \$96.82 | \$96.82 |
| Building Permits | | |
| Re-Roofing of Single Family Residences (except with same roofing system or masonry tile**) | \$173.04 | \$173.04 |
| Electrical Permit Fees | | |
| Permit Issuance | | |
| Minimum Permit Fee | \$173.04 | \$173.04 |
| For issuing each permit | \$38.11 | \$38.11 |
| For issuing each supplemental permit | \$25.75 | \$25.75 |
| Electrical Plan Review (hourly) | \$139.05 | \$139.05 |
| Electrical Unit Fees - as Listed Below | | |
| New Residential Buildings (Wiring) | | |
| Multifamily | \$0.12 | \$0.12 |
| Single and two-family | \$0.12 | \$0.12 |
| Non-residential | \$0.12 | \$0.12 |
| Private Swimming Pools | | |
| New private, in-ground swimming pools | \$77.39 | \$77.39 |
| Carnivals and Circuses | | |
| For electrical generators and electrically driven motors | \$36.76 | \$36.76 |
| For mechanical driven rides and walk-through attractions | \$11.44 | \$11.44 |
| For a system of area and booth lighting | \$11.44 | \$11.44 |
| Temporary Power Service | | |
| For temporary service pole or pedestal | \$96.82 | \$96.82 |
| For temporary destruction system and temporary lighting | \$19.23 | \$19.23 |
| Receptacle, Switch and Light Outlets | | |
| First 20 fixtures, each | \$1.70 | \$1.70 |
| Additional fixtures, each | \$1.09 | \$1.09 |
| Lighting Fixtures | | |
| First 20 fixtures, each | \$1.70 | \$1.70 |
| Additional fixtures, each | \$1.09 | \$1.09 |
| For pole or platform-mounted lighting fixtures | \$1.70 | \$1.70 |
| For theatrical-type lighting fixtures | \$1.70 | \$1.70 |
| Residential appliances | | |
| For fixed residential appliances | \$7.42 | \$7.42 |
| Non-Residential appliances | | |
| For non-residential appliances and self-contained factory-wired non-residential appliances | \$7.42 | \$7.42 |
| Power Apparatus | | |
| Up to and including 1..... | \$7.42 | \$7.42 |
| Over 1 and not over 10, each | \$19.23 | \$19.23 |
| Over 10 and not over 50, each | \$38.46 | \$38.46 |
| Over 50 and not over 100, each | \$77.39 | \$77.39 |
| Over 100.... | \$116.38 | \$116.38 |
| Busways | | |
| For trolley and plug-in type busways | \$11.44 | \$11.44 |
| Signs, Outline Lighting and Marquees | | |
| Supplied from one branch circuit | \$38.47 | \$38.47 |
| For additional branch circuits, each | \$7.42 | \$7.42 |
| Services | | |
| For services of 600 volts and not over 200 amperes | \$47.60 | \$47.60 |
| For services of 600 volts and over 200 amperes to 1,000 amperes | \$97.14 | \$97.14 |
| For services over 600 volts or over 1,000 amperes in rating | \$194.42 | \$194.42 |
| Upgrade Panel 50 400 amp or less (per panel) | \$173.69 | \$173.69 |
| Low Voltage Electric | \$28.48 | \$28.48 |
| Miscellaneous Apparatus, Conduits and Conductors | | |
| For electrical apparatus, conduits and conductors for which a permit is required but for which no fee is herein set forth | \$28.48 | \$28.48 |
| Mechanical Permit Fees | | |
| Permit Issuance | | |
| Minimum Permit Fee | \$173.04 | \$173.04 |
| For issuing each permit | \$38.11 | \$38.11 |
| For issuing each supplemental permit | \$25.54 | \$25.54 |
| Mechanical Plan Review (hourly) | \$139.05 | \$139.05 |
| Mechanical Unit Fees - as Listed Below | | |
| Furnaces | | |
| For installation or relocation of each forced-air or gravity type furnace or burner, including ducts and vents attached to such appliance up to and including 100,000 Btu/h | \$23.14 | \$23.14 |
| For installation or relocation of each forced-air or gravity type furnace or burner, including ducts and vents attached to such appliance over 100,000 Btu/h | \$28.48 | \$28.48 |
| For installation or location of each floor furnace, including vent | \$23.14 | \$23.14 |
| For installation or relocation of each suspended heater, recessed wall heater or floor-mounted unit heater | \$23.14 | \$23.14 |
| Appliance Vents | | |

| | | |
|---|-----------------|-----------------|
| For installation, relocation or replacement of each appliance vent installed and not included in appliance permit | \$10.67 | \$10.67 |
| Repairs or Additions | | |
| For the repair of, alteration of, or addition to each heating appliance, refrigeration unit, cooling unit, absorption unit, or each heating, cooling, absorption or evaporative cooling system. | \$19.98 | \$19.98 |
| Boilers, Compressors and Absorption Systems | | |
| For the installation or relocation of each boiler or compressor to and including 3 horsepower, or each absorption system to and including 100,000 Btu/h | \$23.14 | \$23.14 |
| For the installation or relocation of each boiler or compressor over 3 horsepower to and including 15 horsepower, or each absorption system over 100,000 Btu/h to and including 500,000 Btu/h | \$42.48 | \$42.48 |
| For the installation or relocation of each boiler or compressor over 15 horsepower to and including 30 horsepower, or each absorption system to over 500,000 Btu/h to and including 1,000,000 Btu/h | \$58.19 | \$58.19 |
| For the installation or relocation of each boiler or compressor over 30 horsepower to and including 50 horsepower, or each absorption system to over 1,000,000 Btu/h to and including 1,750,000 Btu/h | \$86.67 | \$86.67 |
| For the installation or relocation of each boiler or compressor over 15 horsepower to and including 50 horsepower, or each absorption system to over 1,750,000. | \$144.86 | \$144.86 |
| Air Handlers | | |
| For each air-handling unit to and including 10,000 cubic feet per minute (cfm), including ducts attached thereto | \$16.69 | \$16.69 |
| For each air-handling unit over 10,000 cfm | \$28.37 | \$28.37 |
| Evaporation Coolers | | |
| For each evaporative cooler other than portable type | \$16.68 | \$16.68 |
| Ventilation and Exhaust | | |
| For each ventilation fan connected to a single duct | \$11.44 | \$11.44 |
| For each ventilation system which is not a portion of any heating or air-conditioning system authorized by a permit | \$16.68 | \$16.68 |
| For the installation of each hood which is served by mechanical exhaust, including the ducts for such hood | \$16.68 | \$16.68 |
| Miscellaneous | | |
| For each appliance or piece of equipment regulated by the International Mechanical Code but not classed in other appliance categories, or for which no other fee is listed in the table | \$16.68 | \$16.68 |
| Exchange of Existing HVAC System for Single Family Residences (includes thermostat) | \$96.49 | \$96.49 |
| Plumbing Permits | | |
| Permit Issuance | | |
| Minimum Permit Fee | \$173.04 | \$173.04 |
| For issuing each permit | \$38.11 | \$38.11 |
| For issuing each supplemental permit | \$25.75 | \$25.75 |
| Plumbing Plan Review (hourly) | \$139.05 | \$139.05 |
| Plumbing Unit Fees - as Listed Below | | |
| Fixtures & Vents | | |
| Each plumbing fixture or trap or set of fixtures | \$15.34 | \$15.34 |
| For repair or alteration of drainage or vent piping, each fixture | \$7.42 | \$7.42 |
| Sewers, Disposal Systems Interceptors | | |
| For each building sewer | \$35.99 | \$35.99 |
| For each cesspool | \$58.19 | \$58.19 |
| For each private sewage disposal system | \$116.38 | \$116.38 |
| For each industrial waste pretreatment interceptor | \$31.16 | \$31.16 |
| Rainwater systems - per drain | \$15.21 | \$15.21 |
| Water Piping and Water Heaters | | |
| For installation, alteration or repair of water piping or water treating equipment | \$7.42 | \$7.42 |
| For each water heater | \$19.23 | \$19.23 |
| Gas Piping | | |
| For each gas piping system | \$9.63 | \$9.63 |
| For each additional outlet over five | \$1.59 | \$1.59 |
| Lawn Sprinklers, Vacuum Breakers & Backflow | | |
| Backflow Prevention Inspection (Fire and Irrigation) | \$173.04 | \$173.04 |
| For each lawn sprinkler system | \$23.14 | \$23.14 |
| For atmospheric-type vacuum breakers or backflow protection devices 1 - 5 | \$19.23 | \$19.23 |
| Over 5 devices | \$3.53 | \$3.53 |
| For each backflow protection device other than atmospheric type vacuum breakers, 2 inches and smaller | \$19.23 | \$19.23 |
| Over 2 inches | \$37.04 | \$37.04 |
| Swimming Pools | | |
| Public pool, each | \$142.67 | \$142.67 |
| Public spa, each | \$94.95 | \$94.95 |
| Private pool, each | \$94.95 | \$94.95 |
| Private spa, each | \$47.35 | \$47.35 |
| Miscellaneous | | |
| For each appliance or piece of equipment not regulated by the Uniform Plumbing Code not listed in this table | \$15.34 | \$15.34 |
| Supplemental Permits | | |
| Adding to Existing Same Type | \$25.75 | \$25.75 |



Mercer Island Building Valuation Data Table

The unit costs below are intended to provide guidance in determining project valuations as set forth in Mercer Island City Code Chapter 17.14.010, Section 109.4 and shall include architectural, structural, electrical, plumbing and mechanical work, except as specifically listed below. The unit costs also include the contractor's profit, which should not be omitted.

Project valuations provided by the permit applicant will be validated against this table. To use this table to calculate the valuation for residential construction, multiply the cost per square foot for dwellings (Item 8) by the floor area. Include private garages (Item 17) and porches (Item 7). If air conditioning or fire sprinkler systems (Equipment) will be installed, include the valuation for these systems.

Occupancy & Type / Building Valuation per Square Foot

*Add 0.5% to total cost for each story over three. **Deduct 20 % for shell-only buildings. ***Deduct 11% for mini-warehouses

1. APARTMENT HOUSES:

| | |
|------------------------------|----------|
| Type I or II F.R.* | \$134.62 |
| (Good) | \$165.82 |
| Type V—Masonry (or Type III) | \$109.80 |
| (Good) | \$134.62 |
| Type V—Wood Frame | \$96.80 |
| (Good) | \$124.46 |
| Type I—Basement Garage | \$56.85 |

2. AUDITORIUMS:

| | |
|-------------------|----------|
| Type I or II F.R. | \$159.09 |
| Type II—1-Hour | \$115.24 |
| Type II—N | \$108.97 |
| Type III—1-Hour | \$121.03 |
| Type III—N | \$115.00 |
| Type V—1-Hour | \$115.71 |
| Type V—N | \$108.15 |

3. BANKS:

| | |
|--------------------|----------|
| Type I or II F.R.* | \$224.80 |
| Type II—1-Hour | \$165.71 |
| Type II—N | \$160.27 |
| Type III—1-Hour | \$182.84 |
| Type III—N | \$176.22 |
| Type V—1-Hour | \$165.71 |
| Type V—N | \$158.61 |

4. BOWLING ALLEYS:

| | |
|-----------------|---------|
| Type II—1-Hour | \$77.53 |
| Type II—N | \$72.22 |
| Type III—1-Hour | \$84.27 |
| Type III—N | \$78.72 |
| Type V—1-Hour | \$56.85 |

5. CHURCHES:

| | |
|-------------------|----------|
| Type I or II F.R. | \$150.58 |
| Type II—1-Hour | \$113.11 |
| Type II—N | \$107.44 |
| Type III—1-Hour | \$123.04 |
| Type III—N | \$117.48 |
| Type V—1-Hour | \$115.00 |
| Type V—N | \$108.15 |

6. CONVALESCENT HOSPITALS:

| | |
|--------------------|----------|
| Type I or II F.R.* | \$211.33 |
| Type II—1-Hour | \$146.56 |
| Type III—1-Hour | \$150.22 |
| Type V—1-Hour | \$141.71 |

7. DECKS AND PORCHES:

| | |
|-------------------|---------|
| Decks and Porches | \$36.88 |
|-------------------|---------|

8. DWELLINGS:

| | |
|-------------------------|----------|
| Type V—Masonry | \$115.00 |
| (Good) | \$177.76 |
| Type V—Wood Frame | \$102.12 |
| (Good) | \$177.76 |
| Basements—Semi-Finished | \$30.49 |
| (Good) | \$35.22 |
| Unfinished | \$22.10 |
| (Good) | \$26.95 |

9. FIRE STATIONS:

| | |
|-------------------|----------|
| Type I or II F.R. | \$173.62 |
| Type II—1-Hour | \$114.17 |
| Type II—N | \$107.79 |
| Type III—1-Hour | \$125.05 |
| Type III—N | \$119.73 |
| Type V—1-Hour | \$117.36 |
| Type V—N | \$111.22 |

10. HOMES FOR THE ELDERLY:

| | |
|-------------------|----------|
| Type I or II F.R. | \$157.43 |
| Type II—1-Hour | \$127.77 |
| Type II—N | \$122.33 |
| Type III—1-Hour | \$133.08 |
| Type III—N | \$127.65 |
| Type V—1-Hour | \$128.48 |
| Type V—N | \$124.10 |

11. HOSPITALS:

| | |
|--------------------|----------|
| Type I or II F.R.* | \$247.73 |
| Type III—1-Hour | \$204.95 |
| Type V—1-Hour | \$195.61 |

Mercer Island Building Valuation Data Table – CONTINUED

Item 4.

12. HOTELS AND MOTELS:

| | |
|--------------------|----------|
| Type I or II F.R.* | \$153.30 |
| Type III—1-Hour | \$132.73 |
| Type III—N | \$126.58 |
| Type V—1-Hour | \$115.59 |
| Type V—N | \$113.35 |

13. INDUSTRIAL PLANTS:

| | |
|-------------------|---------|
| Type I or II F.R. | \$86.40 |
| Type II—1-Hour | \$60.16 |
| Type II—N | \$55.20 |
| Type III—1-Hour | \$66.19 |
| Type III—N | \$62.41 |
| Tilt-up | \$45.50 |
| Type V—1-Hour | \$62.41 |
| Type V—N | \$57.09 |

14. JAILS:

| | |
|-------------------|----------|
| Type I or II F.R. | \$241.47 |
| Type III—1-Hour | \$220.78 |
| Type V—1-Hour | \$165.71 |

15. LIBRARIES:

| | |
|-------------------|----------|
| Type I or II F.R. | \$176.70 |
| Type II—1-Hour | \$129.30 |
| Type II—N | \$123.04 |
| Type III—1-Hour | \$136.51 |
| Type III—N | \$129.66 |
| Type V—1-Hour | \$128.24 |
| Type V—N | \$123.04 |

16. MEDICAL OFFICES:

| | |
|--------------------|----------|
| Type I or II F.R.* | \$181.43 |
| Type II—1-Hour | \$139.82 |
| Type II—N | \$132.97 |
| Type III—1-Hour | \$151.76 |
| Type III—N | \$141.24 |
| Type V—1-Hour | \$136.87 |
| Type V—N | \$132.14 |

17. OFFICES:**

| | |
|--------------------|----------|
| Type I or II F.R.* | \$162.16 |
| Type II—1-Hour | \$108.50 |
| Type II—N | \$103.30 |
| Type III—1-Hour | \$117.01 |
| Type III—N | \$112.05 |
| Type V—1-Hour | \$109.68 |
| Type V—N | \$103.30 |

18. PRIVATE GARAGES:

| | |
|---------------|---------|
| Wood Frame | \$36.88 |
| Masonry | \$41.49 |
| Open Carports | \$25.17 |

19. PUBLIC BUILDINGS:

| | |
|--------------------|----------|
| Type I or II F.R.* | \$187.33 |
| Type II—1-Hour | \$151.76 |
| Type II—N | \$145.14 |
| Type III—1-Hour | \$157.55 |
| Type III—N | \$152.11 |
| Type V—1-Hour | \$144.08 |
| Type V—N | \$139.11 |

20. PUBLIC GARAGES:

| | |
|----------------------------|---------|
| Type I or II F.R.* | \$74.22 |
| Type I or II Open Parking* | \$55.67 |
| Type II—N | \$42.43 |
| Type III—1-Hour | \$56.02 |
| Type III—N | \$50.00 |
| Type V—1-Hour | \$51.18 |

21. RESTAURANTS:

| | |
|-----------------|----------|
| Type III—1-Hour | \$147.74 |
| Type III—N | \$142.89 |
| Type V—1-Hour | \$135.33 |
| Type V—N | \$130.13 |

22. SCHOOLS:

| | |
|-------------------|----------|
| Type I or II F.R. | \$168.78 |
| Type II—1-Hour | \$115.24 |
| Type III—1-Hour | \$123.27 |
| Type III—N | \$118.55 |
| Type V—1-Hour | \$115.47 |
| Type V—N | \$110.16 |

23. SERVICE STATIONS:

| | |
|-----------------|----------|
| Type II—N | \$102.00 |
| Type III—1-Hour | \$106.37 |
| Type V—1-Hour | \$90.65 |
| Canopies | \$42.43 |

24. STORES:

| | |
|--------------------|----------|
| Type I or II F.R.* | \$125.05 |
| Type II—1-Hour | \$76.47 |
| Type II—N | \$74.93 |
| Type III—1-Hour | \$92.90 |
| Type III—N | \$87.23 |
| Type V—1-Hour | \$78.36 |
| Type V—N | \$72.33 |

25. THEATERS:

| | |
|-------------------|----------|
| Type I or II F.R. | \$166.65 |
| Type III—1-Hour | \$121.38 |
| Type III—N | \$115.59 |
| Type V—1-Hour | \$114.17 |
| Type V—N | \$108.15 |

26. WAREHOUSES*:**

| | |
|---------------------|---------|
| Type I or II F.R. | \$75.05 |
| Type II or V—1-Hour | \$44.44 |
| Type II or V—N | \$41.60 |
| Type III—1-Hour | \$50.35 |
| Type III—N | \$47.99 |

EQUIPMENT

- AIR CONDITIONING:
 - Commercial..... \$6.38
 - Residential \$5.44
- SPRINKLER SYSTEMS.....\$3.90

This fee schedule is intended for informational use only and cannot be used as a substitute for the Mercer Island City Code. To obtain additional information about fees that may apply to your project, please call Community Planning and Development at 206-275-7605 between 8:30 a.m. – 5 p.m. Monday through Friday.

Updated: 01/01/2019

| Valuation Based Construction Permit Fees | |
|--|---|
| The Building Permit Fee is based on the Project Valuation as set forth in the table below. | |
| The Plan Check Fee is 70% of the Building Permit Fee.* | |
| The "Combination Permit" Fee for Mechanical, Electrical and Plumbing Inspection is 31% of the Building Permit Fee. | |
| (Rounded final fee calculations to the nearest cent) | |
| Project Valuation | Building Permit Fee (Final fee calculations rounded to the nearest cent) |
| \$1 - \$500.99 | \$31.51 |
| \$501 - \$2000.99 | \$32.82 for the first \$500 plus \$3.86 for each additional \$100 |
| \$2,001 - \$25,000.99 | \$93.45 for the first \$2,000 plus \$18.27 for each additional \$1,000 |
| \$25,001 - \$50,000.99 | \$528.40 for the first \$25,000 plus \$13.27 for each additional \$1,000 |
| \$50,001 - \$100,000.99 | \$867.83 for the first \$50,000 plus \$9.18 for each additional \$1,000 |
| \$100,001 - \$500,000.99 | \$1340.56 for the first \$100,000 plus \$7.35 for each additional \$1,000 |
| \$500,001 - \$1,000,000.99 | \$4362.05 for the first \$500,000 plus \$6.13 for each additional \$1,000 |
| \$1,000,001 - \$999,999,999.99 | \$7566.17 for the first \$1,000,000 plus \$4.80 for each additional \$1,000 |

*This chart is used only to calculate the Building Permit, Plan Check, and Combination Permit portions of the overall fees required to issue a permit. Additional plan review and permit fees may apply.



**BUSINESS OF THE CITY COUNCIL
CITY OF MERCER ISLAND**

**AB 5635
December 3, 2019
Consent Calendar**

AGENDA BILL INFORMATION

| | | |
|----------------------------|---|---|
| TITLE: | AB 5635: Public Institution Code Amendment (Ordinance No. 19C-19, 2 nd Reading & Adoption) | <input type="checkbox"/> Discussion Only <input checked="" type="checkbox"/> Action Needed: |
| RECOMMENDED ACTION: | Adopt Ordinance No. 19C-19 amending MICC 19.05.010 to repeal Subsection B. | <input type="checkbox"/> Motion <input checked="" type="checkbox"/> Ordinance <input type="checkbox"/> Resolution |

| | |
|-------------------------------|--|
| DEPARTMENT: | Community Planning and Development |
| STAFF: | Evan Maxim, Director |
| COUNCIL LIAISON: | n/a |
| EXHIBITS: | 1. Ordinance No. 19C-19 with Attachment A |
| CITY COUNCIL PRIORITY: | 2. Prepare for Light Rail and Improve Mobility |

| | |
|-------------------------------|--------|
| AMOUNT OF EXPENDITURE | \$ n/a |
| AMOUNT BUDGETED | \$ n/a |
| APPROPRIATION REQUIRED | \$ n/a |

SUMMARY

On November 19, 2019, the City Council reviewed AB 5632 and the attached ordinance. Ordinance No. 19C-19 is intended to comply with the August 5, 2019, the Growth Management Hearings Board (GMHB) decision on an appeal of Ordinance No. 18-13 and 18C-14 (Case No. 19-3-0003c).

One of the appeal “issues” was related to the Comprehensive plan amendment and rezone of property adjacent to the Tully’s Property and known as “Parcel 12 / WSDOT.” In particular, the GMHB decision notes that the language in MICC 19.05.010(B) is not consistent with the amendments to the land use map in the Comprehensive Plan and is not consistent with the amended zoning map. Ordinance No. 19C-19 corrects this inconsistency.

RECOMMENDATION

Adopt Ordinance 19C-19 amending MICC 19.05.010 to repeal Subsection B; providing for severability and establishing an effective date.

**CITY OF MERCER ISLAND
ORDINANCE NO. 19C-19**

AN ORDINANCE OF THE CITY OF MERCER ISLAND AMENDING MERCER ISLAND CITY CODE 19.05.010 TO REPEAL SUBSECTION B; PROVIDING FOR SEVERABILITY AND ESTABLISHING AN EFFECTIVE DATE.

WHEREAS, the Mercer Island City Code (MICC) establishes development regulations that are intended to result in the implementation of the Mercer Island Comprehensive Plan pursuant to RCW 36.70A.040; and,

WHEREAS, the Mercer Island City Council adopted Ordinance No. 18-13 on November 20, 2018 amending the land use map designation of a certain City-owned property from “Linear Park / I-90” to “Town Center; and,

WHEREAS, the Mercer Island City Council adopted Ordinance No. 18C-14 on November 20, 2018 amending the zoning map designation of the same City-owned property from “Public Institution” to “Town Center”; and,

WHEREAS, the Mercer Island City Council expressed the intent with the adoption of Ordinance No. 18C-14 to comply with chapter 36.70A RCW, which requires consistency between the Comprehensive Plan and Mercer Island City Code; and,

WHEREAS, MICC 19.05.010(B) contains a text description of the bounds of the Public Institution zoning designation, which created an inconsistency between the Comprehensive Plan and Mercer Island City Code; and,

WHEREAS, on August 5, 2019, the Growth Management Hearings Board issued a Final Decision and Order (Case No. 19-3-0003c) directing the City to correct the inconsistency between the adopted text contained in MICC 19.05.010(B), the zoning map, and the land use map; and,

WHEREAS, the Mercer Island Planning Commission held a public hearing on October 30, 2019 and considered public comment received prior to the close of the public hearing; and

WHEREAS, the Mercer Island Planning Commission recommended adoption of the proposed code amendment on October 30, 2019; and,

WHEREAS, a SEPA Determination of Non-Significance was issued by the City on October 14, 2019; and,

WHEREAS, the Washington Department of Commerce granted review of the proposed amendments to the development regulations on October 28, 2019;

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF MERCER ISLAND, WASHINGTON, DOES HEREBY ORDAIN AS FOLLOWS:

Section 1: **Adoption of Amendment to Mercer Island City Code 19.05.010.** Subsection B of MICC 19.05.010 is hereby repealed as set forth in Attachment “A” to this ordinance.

Section 2: **Codification and Effective Date of the Regulations.** The City Council authorizes the Community Planning and Development Director and the City Clerk to correct errors in Attachment A, codify the regulatory provisions of the amendments into Title 19 of the Mercer Island City Code, and publish the amended code.

Section 3: **Interpretation.** The City Council authorizes the Community Planning and Development Director to adopt administrative rules, interpret, and administer the amended code as necessary to implement the legislative intent of the City Council.

Section 4: **Severability.** If any section, sentence, clause or phrase of this ordinance or any municipal code section amended hereby should be held to be invalid or unconstitutional by a court of competent jurisdiction, such invalidity or unconstitutionality shall not affect the validity of any other section, sentence, clause or phrase of this ordinance or the amended code section.

Section 5: **Publication and Effective Date.** A summary of this Ordinance consisting of its title shall be published in the official newspaper of the City. This Ordinance shall take effect and be in full force five days after the date of publication.

PASSED by the City Council of the City of Mercer Island, Washington at its regular meeting on December 3, 2019 and signed in authentication of its passage.

CITY OF MERCER ISLAND

Debbie Bertlin, Mayor

Approved as to Form:

ATTEST:

Bio Park, Interim City Attorney

Deborah Estrada, City Clerk

Date of Publication: _____

Attachment A

ZTR19-002: Public Institution Code Amendment

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“Normal Text” is existing code language
“~~Strikethrough Text~~” is existing code language that will be deleted
“Underline Text” is new code language that will be added
“...” indicates that existing code language is omitted and will not be amended

19.05.010 Public institution – PI.

A. *Uses Permitted.*

- 1. Government services.
- 2. Public schools under the administration of Mercer Island School District No. 400 subject to the requirements in subsection F of this section. Subsections B, C and E of this section do not apply to public schools. Uses other than public schools located on land owned by the Mercer Island School District shall comply with applicable provisions of Chapter 19.02 MICC.
- 3. Public park.
- 4. Transit facilities including transit stops and associated parking lots.
- 5. On-site hazardous waste treatment and storage facilities are allowed as accessory uses to a use permitted in this zone. These facilities shall comply with the state siting criteria as set forth in Chapter 173-303 WAC.
- 6. Wireless communications facilities subject to the conditions set out in MICC 19.06.040.

~~B. Mercer Island I-90 Right-of-Way Added to Public Institution Zone. The entire area within the Mercer Island I-90 right-of-way, including, but not limited to, the roadway, street overcrossings, lids, open space, recreation areas, linear greenbelts and the park and ride lot area as approved by the city on November 14, 1983, and incorporated in the right-of-way plan approved by WSDOT on May 1, 1987, shall be part of the public institution zone. All uses within the I-90 right-of-way shall be maintained as set forth in city approved I-90 related documents. (Repealed by Ordinance No. 19C-19.)~~

C. Design Requirements. Any development within the public institution zone shall comply with the applicable sections of Chapter 19.11 MICC, Town Center Development and Design Standards.

...



BUSINESS OF THE CITY COUNCIL CITY OF MERCER ISLAND

AB 5641
December 3, 2019
Consent Calendar

AGENDA BILL INFORMATION

| | | |
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| TITLE: | AB 5641: Acceptance of MIYFS Foundation funds for 2020 Youth and Family Services staffing. | <input type="checkbox"/> Discussion Only |
| RECOMMENDED ACTION: | Accept funds from Mercer Island Youth and Family Services Foundation to fund 0.5 FTE of the Geriatric Specialist for 2020 in the YFS Department. | <input checked="" type="checkbox"/> Action Needed: <input checked="" type="checkbox"/> Motion <input type="checkbox"/> Ordinance <input type="checkbox"/> Resolution |

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| DEPARTMENT: | Youth and Family Services |
| STAFF: | Derek J. Franklin, Senior Programs Manager and Clinical Supervisor |
| COUNCIL LIAISON: | n/a |
| EXHIBITS: | 1. Donation of funds letter from MIYFS Foundation President Jody Kris |
| CITY COUNCIL PRIORITY: | 1. Implement a Fiscal Sustainability Plan |

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| AMOUNT OF EXPENDITURE | \$ 54,624 |
| AMOUNT BUDGETED | \$ 0 |
| APPROPRIATION REQUIRED | \$ 54,624 |

SUMMARY

BACKGROUND

As part of the 2019-2020 biennial budget process, the City Manager proposed service reductions to balance the budget. Several reductions were made in the Youth and Family Services Department, including the reduction of the full time Geriatric Specialist to half time (0.50 FTE).

In late November 2018, the Mercer Island Youth and Family Services Foundation (“Foundation”) sent a letter to the City pledging to donate funds to restore the Geriatric Specialist to a full-time position in 2019. At that time the Foundation also voted to continue funding the position to its original full-time status for 2020, if the City did not find funds to replace this reduction.

FOUNDATION DONATION

The Foundation provides funding to support the Department of Youth and Family Services (YFS) programs and services. This funding is provided in two forms:

1. An annual commitment towards YFS’ operating budget; and
2. One-time or pilot project funds as Foundation reserves allow.

The current proposed donation of \$54,624 to the YFS Department (see Exhibit 1) falls in the latter category (2), as one-time funds to maintain the full time Geriatric Specialist:

- **0.5 FTE Geriatric Specialist (\$54,624), January 1 through December 31, 2020**
Mercer Island has a robust senior population and the need for services remains high. This funding from the Foundation will maintain services at the current level.

RECOMMENDATION

Accept a donation of \$54,624 from the Mercer Island Youth and Family Services Foundation to fund the half time reduction in the Geriatric Specialist position from January 1, 2020 through December 31, 2020.



BUSINESS OF THE CITY COUNCIL CITY OF MERCER ISLAND

AB 5637
December 3, 2019
Public Hearing

AGENDA BILL INFORMATION

| | | |
|----------------------------|--|---|
| TITLE: | AB 5637: Interim Ordinance Design and Concealment Standards for Small Cell Facilities Deployment | <input type="checkbox"/> Discussion Only <input checked="" type="checkbox"/> Action Needed: |
| RECOMMENDED ACTION: | Conduct public hearing and adopt Ordinance No. 19-22 extending the Interim Design and Concealment Standards for Small Cell Facilities deployment established under Ordinance No. 19C-02. | <input type="checkbox"/> Motion <input checked="" type="checkbox"/> Ordinance <input type="checkbox"/> Resolution |

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| DEPARTMENT: | Community Planning and Development |
| STAFF: | Evan Maxim, Director of Community Planning and Development |
| COUNCIL LIAISON: | n/a |
| EXHIBITS: | 1. Ordinance No. 19C-02 2. Ordinance No. 19-22 |
| CITY COUNCIL PRIORITY: | 3. Support the Leadership Team's Work Plan |

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| AMOUNT OF EXPENDITURE | \$ n/a |
| AMOUNT BUDGETED | \$ n/a |
| APPROPRIATION REQUIRED | \$ n/a |

SUMMARY

On January 15, 2019, the City Council unanimously passed Emergency Ordinance No. 19C-02 (see Exhibit 1), establishing Interim Design and Concealment Standards for Small Cell Facilities deployments. The emergency ordinance was adopted in response to the Federal Communications Commission’s (FCC) issuance of a “Declaratory Ruling and Third Report and Order” (“New Rules”) related to the deployment of small cell facilities, which became effective January 14, 2019. The New Rules resulted in significant changes to the approach the City must use to regulate small cell deployment, which were described in AB 5526.

On March 5, 2019, the City Council (see AB 5538) conducted a public hearing in compliance with RCW 35A.63.220 and 36.70A.390. Public testimony included verbal comments from representatives of Crown Castle and Verizon, and written comment from members of the Mercer Island community.

Interim regulations expire 6 months after adoption, unless extended by the City Council following an additional public hearing. On June 18, 2019, the City Council (see AB 5565) held a public hearing and passed Ordinance 19-10, which extended Ordinance No. 19C-02 through January 14, 2020.

Ordinance No. 19C-02 will expire on January 14, 2020, unless extended by the City Council following a public hearing.

On November 20, 2019, the Planning Commission initiated their work on a recommendation for “permanent” standards regulating small cell facilities. The Planning Commission has developed a recommended scope for the proposed update, which staff anticipates will be reviewed by the City Council in January or February of 2020. Once the City Council has confirmed the scope of the proposed update, the Planning Commission will begin their work reviewing draft standards and regulations for adoption by the City Council in mid-2020.

Until permanent design and concealment standards are adopted, staff recommends that the effective period of the Interim Design and Concealment Standards for Deployment of Small Cell Facilities adopted by Ordinance No. 19C-02 be renewed for another six-month period.

RECOMMENDATION

1. Conduct public hearing and consider public testimony.
2. Suspend the City Council Rules of Procedure 6.3, requiring a second reading of an ordinance.
3. Adopt Ordinance No. 19-22, extending the Interim Design and Concealment Standards for Small Cell Facilities deployment established under Ordinance No. 19C-02.

**CITY OF MERCER ISLAND
ORDINANCE NO. 19C-02**

AN ORDINANCE OF THE CITY OF MERCER ISLAND, WASHINGTON, DECLARING AN EMERGENCY; ADOPTING INTERIM DESIGN AND CONCEALMENT STANDARDS FOR SMALL CELL FACILITIES DEPLOYMENT; PROVIDING FOR A PUBLIC HEARING; AND ESTABLISHING AN IMMEDIATE EFFECTIVE DATE.

WHEREAS, the Federal Communications Commission issued a Declaratory Ruling and Third Report and Order (“New Rules”) relating to small cell facilities, which became effective January 14, 2019; and

WHEREAS, the New Rules significantly preempt the City’s ability to regulate the installation of small cell facilities on City-owned public rights-of-way; and

WHEREAS, aesthetic requirements imposed by the City under the New Rules on installation of small cell facilities must be published in advance and must also be reasonable, no more burdensome than those applied to other types of infrastructure deployments, and objective; and

WHEREAS, the City does not have design and concealment standards adopted for deployment of small cell facilities currently; and

WHEREAS, without adopted standards, the City may not impose design and concealment standards on applications for the deployment of small cell facilities under the New Rules; and

WHEREAS, the City Council finds that deployment of small cell facilities with unregulated design and concealment standards may result in uncoordinated installations, visual blight, interference with public facilities and equipment, and traffic dangers that pose harm to public health, safety, property, and welfare; and

WHEREAS, to prevent the potential harm to public health, safety, property, and welfare, the City Council concludes that the City immediately needs interim design and concealment standards for deployment of small cell facilities until permanent standards can be adopted following the process and procedures for adopting development regulations; and

WHEREAS, the City is authorized under RCW 35A.63.220, 36.70A.390 to pass an interim zoning and official control ordinance for up to six months, provided it holds a public hearing on the same within sixty days after passage; and

WHEREAS, consistent with the provisions of RCW 35A.63.220 and RCW 36.70A.390, it is appropriate for the City Council to hold a public hearing and adopt findings of fact supporting and justifying the interim zoning and official control ordinance within at least sixty days of its passage; **NOW, THEREFORE,**

THE CITY COUNCIL OF THE CITY OF MERCER ISLAND, WASHINGTON, DO ORDAIN AS FOLLOWS:

- Section 1. Whereas Clauses Adopted.** The “Whereas Clauses” set forth in the recital of this Ordinance are hereby adopted as the preliminary findings and conclusions of the City Council for passing this Ordinance.

- Section 2. Declaration of Emergency.** As set forth in the “Whereas Clauses” adopted in Section 1 of this Ordinance, the City Council hereby declares that an emergency exists necessitating that this Ordinance take effect immediately upon unanimous passage by the whole membership of the City Council, and that the same is not subject to a referendum (RCW 35A.11.090(2)) and is exempt from SEPA review (WAC 197-11-880 and MICC 19.07.120(D)).

- Section 3. Interim Standards Adopted.** Interim Design and Concealment Standards for Deployment of Small Cell Facilities are hereby adopted as set forth in Exhibit A to this Ordinance.

- Section 4. Public Hearing.** Pursuant to RCW 35A.63.220 and RCW 36.70A.390, a public hearing shall be scheduled for 7:00 p.m. on March 5, 2019, which is within 60 days of this Ordinance passage, at Mercer Island City Hall, 9611 SE 36th Street, during the City Council’s regular meeting, or as soon thereafter as the business of the City Council shall permit, in order to hear and consider the comments and testimony of those wishing to speak at such public hearing regarding the interim standards imposed by this Ordinance and to consider adopting further findings of fact if needed.

- Section 5. Duration of Interim Standards.** The Interim Design and Concealment Standards for Deployment of Small Cell Facilities approved by this Ordinance shall become effective immediately, on the date hereof, and shall continue in effect for an initial period of six months, unless repealed, extended or modified by the City Council after subsequent public hearing(s), entry of appropriate findings of fact, and or development of a work plan for related studies pursuant to RCW 35A.63.220 and RCW 36.70A.390.

- Section 6. Severability.** If any section, sentence, clause, or phrase of this Ordinance should be held to be invalid or unconstitutional by a court of competent jurisdiction, or its application held inapplicable to any person, property, or circumstance, such invalidity or unconstitutionality shall not affect the validity of any other section, sentence, clause, or phrase of this Ordinance or its application to any other person, property or circumstance.

- Section 7. Effective Date.** This Ordinance, as a public emergency ordinance necessary for the protection of the public health, safety, property, and welfare, shall take effect and be in full force and effect immediately upon its unanimous passage by the entire membership of the City Council as required by RCW 35A.11.090(2) and 35A.13.190.

Passed unanimously by the City Council of the City of Mercer Island, Washington, at its regular meeting on the 15th day of January 2019 and signed in authentication of its passage.

CITY OF MERCER ISLAND



Debbie Bertlin, Mayor

Approved as to Form:



Kari L. Sand, City Attorney

ATTEST:



Deborah A. Estrada, City Clerk

Date of Publication: 1/23/2019

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DRAFT
Code Amendments
Small Cell Facilities Design and Concealment Interim Standards

GENERAL REGULATIONS

- 19.06.070 Small Cell Deployment.
- 19.06.075 Small Cell Deployments – Design and Concealment Standards.

DEFINITIONS

- 19.15.030 Land Use Review Types.

DEFINITIONS

- 19.16.010 Definitions.

“Normal Text” is existing code language
 “~~Strikethrough Text~~” is existing code language that will be deleted
 “Underline Text” is new code language that will be added
 “...” represents that existing code language is omitted and will not be amended

1 **19.06.070 ~~Bonding and insurance~~ Small cell facilities deployment.**

2 The following provisions establish standards for small cell facilities deployments; provided, however,
3 that any small cell or small cell network component which is not exempt from SEPA review shall also
4 comply with chapter 19.07 MICC:

5 (1) Small Cell Facility Approval Required. Small cell facilities are permitted in all zoning
6 designations subject to a Type II land use review process pursuant to chapter 19.15 MICC. In
7 addition to the small cell approval, one or more right-of-way use permits may also be required
8 for small cell deployment.

9 (2) Previously Approved Small Cells on Existing or Replacement Utility Poles. Eligible small cell
10 facilities permitted under the provisions of a franchise approval prior to the adoption of these
11 standards shall be considered to have satisfied the design and concealment standards when
12 installed and maintained in accordance with the franchise agreement.

13 (3) Replacement Utility Pole – Street Lighting. With the express permission of the City, a
14 replacement utility pole or a new utility pole may be permitted in the form of a new street light
15 standard. The design of the street light standard shall be in accordance with the City lighting
16 requirements in effect at the time of application. Wherever technologically feasible, all
17 equipment and cabling shall be internal to the replacement street lighting standard, or
18 concealed through the design and implementation of a concealment plan.

19 (4) Undergrounded Utility Areas. A service provider or infrastructure company desiring to locate
20 any aboveground infrastructure in an undergrounded utility area shall provide a separate,
21 standalone pole. Pole design to be approved by the City pursuant to MICC 19.06.075(6)(d).

22
23 **19.06.075 Small Cell Deployments – Design and concealment standards.**

24 Small cell deployments, whether permitted on the right-of-way pursuant to a franchise or in accordance
25 with this chapter, shall conform to the design standards set forth in this section.

26 (1) Small Cell Deployment Design Standards - General Requirements. All small cell deployments
27 shall comply with the following provisions:

28 (a) Ground-mounted equipment in the rights-of-way is prohibited unless such facilities
29 are placed underground or the applicant can demonstrate that pole-mounted or
30 undergrounded equipment is technically infeasible. If ground-mounted equipment is
31 necessary, then the applicant shall submit a concealment plan pursuant to subsection
32 (7). Generators located in the rights-of-way are prohibited.

33 (b) No equipment shall be operated so as to produce noise in violation of chapter 8.24
34 MICC.

35 (c) Small cell facilities are not permitted on traffic signal poles.

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(d) Replacement poles and new poles shall comply with the Americans with Disabilities Act (ADA), City construction and sidewalk clearance standards, and state and federal regulations in order to provide a clear and safe passage within the rights-of-way.

(e) Replacement poles shall be located as near as possible subject to approval by the City Engineer to the existing pole with the requirement to remove the abandoned pole.

(f) No signage, message or identification other than the manufacturer’s identification or identification required by governing law is allowed to be portrayed on any antenna, and any such signage on equipment enclosures shall be of the minimum amount possible to achieve the intended purpose; provided, that signs are permitted as concealment element techniques where appropriate.

(g) Antennas and related equipment shall not be illuminated except for security reasons, required by a federal or state authority, or unless approved as part of a concealment plan.

(h) Side arm mounts for antennas or equipment are prohibited.

(i) The preferred location of a small cell facility on a pole is the location with the least visible impact.

(j) Antennas, equipment enclosures, and ancillary equipment, conduit and cable shall be located within the building or pole to the maximum extent feasible.

(k) Antennas, equipment enclosures and ancillary equipment, conduit and cable shall not adversely affect the aesthetic appearance or visual character of the building or pole upon which they are attached.

(l) The City may consider the cumulative visual effects of small cells mounted on poles, together with existing utility equipment, within the rights-of-way when assessing proposed siting locations so as to not adversely affect the visual character of the City. This provision shall not be applied to limit the number of permits issued when no alternative sites are reasonably available nor to impose a technological requirement on the service provider.

(m) The design criteria as applicable to small cell facilities described herein shall be considered concealment elements and such small cell facilities may only be expanded upon an eligible facilities request described in chapter 19.06 MICC, when the modification does not defeat the concealment elements of the facility.

(2) Small Cell Facilities Attached to Nonwooden Poles. Small cell facilities attached to existing or replacement nonwooden light poles and other nonwooden poles in the right-of-way or poles outside of the right-of-way shall conform to the following design criteria in addition to the General Requirements set forth in subsection (1) above:

(a) Antennas and the associated equipment enclosures shall be sited and installed in a manner which minimizes the visual impact on the streetscape either by either:

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- i. Fully concealing the antennas and associated equipment fully within the pole;
or,
- ii. Through a concealment plan which provides an equivalent or greater impact reduction pursuant to subsection (7), below.

(b) All conduit, cables, wires and fiber must be routed internally in the light pole. Conduit, cables, wires and fiber extending outside the pole to connect with externally mounted antennas or equipment shall be located within shrouds, canisters, or sleeves.

(c) An antenna on top of an existing pole may not extend more than six feet above the height of the existing pole and the diameter may not exceed 16 inches, measured at the top of the pole, unless the applicant can demonstrate that more space is needed. The antennas shall be integrated into the pole design so that they appear as a continuation of the original pole, including colored, powder coated, or other permanent coloration, to match the pole, and shall be shrouded or screened to blend with the pole. All cabling and mounting hardware/brackets from the bottom of the antenna to the top of the pole shall be fully concealed and integrated with the pole.

(d) In addition to the increased antenna height allowed in subsection (c) above, the height of any replacement pole may not extend more than 10 feet above the height of the existing pole or the minimum additional height necessary for adequate clearance from electrical wires, whichever is greater.

(e) Any replacement nonwooden pole shall substantially conform to the design of the pole it is replacing, or the applicable City pole design standards.

(f) The diameter of a replacement pole shall comply with applicable setback and sidewalk clearance requirements, ADA requirements, and if a replacement light standard then with the City's lighting requirements.

(g) The use of the pole for the siting of a small cell facility shall be considered secondary to the primary function of the pole. If the primary function of a pole serving as the host site for a small cell facility becomes unnecessary, the pole shall not be retained for the sole purpose of accommodating the small cell facility and the small cell facility and all associated equipment shall be removed.

(3) Wooden Pole Design Standards. Small cell facilities located on wooden poles shall conform to the following design criteria in addition to the General Requirements set forth in subsection (1) above:

(a) The wooden pole at the proposed location may be replaced with a taller pole for the purpose of accommodating a small cell facility; provided, that the replacement pole shall not exceed a height that is a maximum of 10 feet taller than the existing pole, unless a further height increase is required and confirmed in writing by the pole owner and that such height extension is the minimum extension possible to provide sufficient separation and/or clearance from electrical and wireline facilities.

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(b) A pole extender may be used instead of replacing an existing pole but may not increase the height of the existing pole by more than 10 feet unless a further height increase is required and confirmed in writing by the pole owner and such height increase is the minimum extension possible to provide sufficient separation and/or clearance from electrical and wireline facilities. The pole extender shall be painted to approximately match the color of the pole and shall substantially match the diameter of the pole measured at the top of the pole.

(c) Replacement wooden poles may either match the approximate color and materials of the replaced pole or shall be the standard new wooden pole used by the pole owner in the City.

(d) Antennas, equipment enclosures, and all ancillary equipment, boxes and conduit shall be colored, powder coated, or other permanent coloration, to match the approximate color of the surface of the wooden pole on which they are attached.

(e) Panel antennas shall not be mounted on the side of a pole more than 12 inches from the surface of the wooden pole, measured from the exterior surface of the pole to the furthest extent of the panel antenna.

(f) Antennas should be placed in an effort to minimize visual clutter and obtrusiveness. Multiple antennas are permitted on a wooden pole; provided, that each antenna enclosure shall not be more than three cubic feet in volume, with a cumulative total antenna volume not to exceed 12 cubic feet.

(g) In addition to the increased antenna height allowed in subsection (b) above, a canister antenna may be mounted on top of an existing wooden pole, which may not exceed the height requirements described in subsection (3)(a) of this section. A canister antenna mounted on the top of a wooden pole shall not exceed 16 inches, measured at the top of the pole, and shall be colored or painted to match the pole. The canister antenna must be placed to look as if it is an extension of the pole. In the alternative, the applicant may propose a side-mounted canister antenna, so long as the inside edge of the antenna is no more than 12 inches from the surface of the wooden pole. All cables shall be concealed either within the canister antenna or within a sleeve between the antenna and the wooden pole.

(h) In addition to the increased antenna height allowed in subsection (b) above, an omni-directional antenna may be mounted on the top of an existing wooden pole, provided such antenna is no more than four feet in height and is mounted directly on the top of a pole or attached to a sleeve made to look like the exterior of the pole as close to the top of the pole as technically feasible. All cables shall be concealed within the sleeve between the bottom of the antenna and the mounting bracket.

(i) All related equipment including but not limited to ancillary equipment, radios, cables, associated shrouding, microwaves, and conduit which are mounted on wooden poles shall not be mounted more than six inches from the surface of the pole, unless a further distance is technically required, and is confirmed in writing by the pole owner.

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(j) Equipment for small cell facilities must be attached to the wooden pole, unless otherwise permitted to be ground-mounted pursuant to subsection (1) of this section. The equipment must be placed in the smallest enclosure possible for the intended purpose. The equipment enclosure may not exceed 17 cubic feet. Multiple equipment enclosures may be acceptable if designed to more closely integrate with the pole design and do not cumulatively exceed 17 cubic feet. The applicant is encouraged to place the equipment enclosure behind any banners or road signs that may be on the pole if such banners or road signs are allowed by the pole owner.

(k) The visual effect of the small cell facility on all other aspects of the appearance of the wooden pole shall be minimized to the greatest extent reasonably possible.

(l) The use of the wooden pole for the siting of a small cell facility shall be considered secondary to the primary function of the pole. If the primary function of a pole serving as the host site for a small cell facility becomes unnecessary, the pole shall not be retained for the sole purpose of accommodating the small cell facility and the small cell facility and all associated equipment shall be removed.

(m) All cables and wires shall be routed through conduit along the outside of the pole. The outside conduit shall be colored, powder coated, or other permanent coloration, to match the pole. The number and size of conduits shall be minimized to the number technically necessary to accommodate the small cell.

(4) Small Cell Facilities Attached to Existing Buildings. Small cell facilities attached to existing buildings shall conform to the following design criteria:

(a) Small cell facilities may be mounted to the sides of a building if the antennas do not interrupt the building’s architectural theme.

(b) The interruption of architectural lines or horizontal or vertical reveals is discouraged.

(c) New architectural features such as columns, pilasters, corbels, or other ornamentation that conceal antennas may be used if they complement the architecture of the existing building.

(d) Small cells shall utilize the smallest mounting brackets necessary in order to provide the smallest offset from the building.

(e) Skirts or shrouds shall be utilized on the sides and bottoms of antennas in order to conceal mounting hardware, create a cleaner appearance, and minimize the visual impact of the antennas. Exposed cabling/wiring is prohibited.

(f) Small cell facilities shall be painted and textured to match the adjacent building surfaces.

(5) Small cell facilities mounted on cables strung between utility poles shall conform to the following standards:

(a) Each strand-mounted facility shall not exceed three cubic feet in volume;

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(b) Only one strand-mounted facility is permitted per cable between any two existing poles;

(c) The strand-mounted devices shall be placed as close as possible to the nearest utility pole, in no event more than six feet from the pole unless a greater distance is technically necessary or required by the pole owner for safety clearance;

(d) No strand-mounted device shall be located in or above the portion of the roadway open to vehicular traffic;

(e) Ground-mounted equipment to accommodate such strand-mounted facilities is not permitted, except when placed in preexisting equipment cabinets;

(f) Pole-mounted equipment for strand-mounted facilities shall meet the requirements for pole-mounted small cells; and

(g) Such strand-mounted devices must be installed to cause the least visual impact and with the minimum exterior cabling or wires (other than the original strand) necessary to meet the technological needs of the facility.

(6) New Poles in the Rights-of-Way for Small Cell Facilities.

(a) New poles within the rights-of-way are only permitted if the applicant can establish that:

(i) The proposed small cell facility cannot be located on an existing utility pole or light pole, electrical transmission tower or on a site outside of the public rights-of-way such as a public park, public property, building, transmission tower or in or on a nonresidential use in a Residential Zone whether by roof or panel-mount or separate structure;

(ii) The proposed wireless communications facility receives approval for a concealment plan, as described in subsection (7) of this section;

(iii) The proposed wireless communications facility also complies with the Shoreline Master Program and SEPA, if applicable; and

(iv) No new poles shall be located in a critical area or associated buffer required by the City's critical areas ordinance, except when determined to be exempt pursuant to said ordinance.

(7) The concealment plan shall include the design of the screening, fencing or other concealment technology for a pole or equipment structure, and all related transmission equipment or facilities associated with the proposed wireless communications facility, including but not limited to fiber and power connections.

(a) The concealment plan shall seek to minimize the visual obtrusiveness of wireless communications facility installations. The proposed pole or structure shall have similar designs to existing neighboring poles in the rights-of-way, including to the extent technically feasible similar height. Other concealment methods include, but are not

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limited to, integrating the installation with architectural features or building design components, utilization of coverings or concealment devices of similar material, color and texture – or the appearance thereof – as the surface against which the installation will be seen or on which it will be installed, landscape design, or other camouflage strategies appropriate for the type of installation. Applicants are required to utilize designs in which all conduit and wirelines are installed internally in the structure or otherwise integrated into the design of the structure. Use of a unified enclosure equal to or less than four cubic feet in volume may be permitted in meeting these criteria. This requirement shall be applied in a manner which does not dictate the technology employed by the service provider nor unreasonably impair the technological performance of the equipment chosen by the service provider.

(b) If the code official has already approved a concealment plan either for the applicant or another wireless communications facility along the same public right-of-way or for the same pole type, then the applicant shall utilize a substantially similar concealment plan, unless it can show that such concealment plan is not physically or technologically feasible, or that such deployment would undermine the generally applicable design standards.

(8) These design standards are intended to be used solely for the purpose of concealment and siting. Nothing herein shall be interpreted or applied in a manner which dictates the use of a particular technology. When strict application of these requirements would unreasonably impair the function of the technology chosen by the applicant, alternative forms of concealment or deployment may be permitted which provide similar or greater protections from negative visual impacts to the streetscape.

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19.15.030 Land use review types.

There are four categories of land use review that occur under the provisions of the development code.

A. *Type I.* Type I reviews are based on clear, objective and nondiscretionary standards or standards that require the application of professional expertise on technical issues.

B. *Type II.* Type II reviews are based on clear, objective and nondiscretionary standards or standards that require the application of professional expertise on technical issues. The difference between Type I and Type II review is that public notification shall be issued for Type II decisions.

C. *Type III.* Type III reviews require the exercise of discretion about nontechnical issues.

D. *Type IV.* Type IV reviews require discretion and may be actions of broad public interest. Decisions on Type IV reviews are only taken after an open record hearing.

E. The types of land use approvals are listed in Table A of this section. The required public process for each type of land use approval are listed in Table B of this section.

...

Table A. Land Use Review Type

| Type I | Type II | Type III | Type IV |
|---|---|---|--|
| <ul style="list-style-type: none"> • Home business • Seasonal development limitation waiver • Nonmajor single-family dwelling building permits • Tree removal permit • Right-of-way permit • Special needs group housing safety determination | <ul style="list-style-type: none"> • Modified wireless communication facilities (6409 per <u>47 CFR 1.40001</u>) • Lot line revision • Setback deviations • Final plat^{2, 3} • Code official design review • Accessory dwelling unit | <ul style="list-style-type: none"> • New and modified wireless (non-6409) eligible facility • SEPA threshold determination • Critical areas determination (wetland/watercourse buffer averaging/reduction) • Temporary encampment⁴ | <ul style="list-style-type: none"> • Preliminary long plat approval • Conditional use permit • Variance • Critical areas reasonable use exception • Long plat alteration and vacations • Parking variances (reviewed by design commission) |

| Type I | Type II | Type III | Type IV |
|---|---|---|---|
| <ul style="list-style-type: none"> • Tenant improvement/change of use • Shoreline exemption¹ • Critical areas determination (steep slope alteration) • Final short plat • Temporary commerce on public property • Site development permits • Transportation concurrency certificate | <ul style="list-style-type: none"> • Parking variances (reviewed by city engineer) • <u>Small cell deployment</u> | <ul style="list-style-type: none"> • Short plat alteration and vacations • Preliminary short plat • Development code interpretations • Major single-family dwelling building permit • Shoreline substantial development permit¹ • Shoreline revision (substantial development)¹ | <ul style="list-style-type: none"> • Variance from short plat acreage limitation • Wireless communication facility height variance • Planned unit development • Design commission design review • Permanent commerce on public property • Shoreline conditional use permit (SCUP)⁵ • Shoreline variance⁵ • Shoreline revision (variance and SCUP) |

1

2

...

1 **19.16.010 Definitions.**

2 Words used in the singular include the plural and the plural the singular.

3 ...

4 Pole Extender: An object affixed between a utility pole and pole top mounted equipment (e.g. a small
5 cell antenna) for the purpose of increasing the height of the pole top mounted equipment above the
6 pole.

7 ...

8 Regulated Improvements: Any development of any property within the city, except:

9 1. Property owned or controlled by the city; or

10 2. Single-family dwellings and the buildings, structures and uses accessory thereto; or

11 3. Wireless communications structures, including associated support structures and equipment
12 cabinets; or

13 4. Small cell facilities or small cell networks.

14 ...

15 Small cell deployment: The construction and installation of either small cell facilities, small cell networks,
16 or both small cell facilities and small cell networks, together with the installation of the fiber network
17 supporting the small cell facility and small cell network.

18 ...

19 “Small cell facility” and “small cell network” are defined in accordance with RCW 80.36.375.

20 ...

21 “Small cell” shall mean “small cell facility”.

22 ...

23 Undergrounded Utility Areas: A geographic area where utilities that are commonly located aboveground
24 (e.g. electrical power, cable and telephone lines, etc.) have been placed entirely underground, and
25 associated support structures (e.g. wooden utility poles or guy poles) have been removed.

26 ...

1 Utilities: Facilities providing infrastructure services by a public utility or private utility regulated by the
2 state through fixed wires, pipes, or lines. Such facilities may include water, sewer, storm water facilities
3 (lines, ditches, swales and outfalls) and private utilities such as natural gas lines, telecommunication
4 lines, cable communication lines, electrical lines and other appurtenances associated with these utilities.
5 "Utilities" does not include wireless communication facilities, but do include small cell facilities.

6 ...

7 Wireless Communications:

8 1. Attached Wireless Communications Facility (Attached WCF): An antenna array that is attached
9 to an existing building or structure, including utility poles, with any accompanying attachment
10 structure, transmission cables, and an equipment cabinet which may be located either inside or
11 outside of the attachment building or structure.

12 2. Wireless Communications Antenna Array (Antenna Array): One or more rods, panels, discs or
13 similar devices used for the transmission or reception of radio frequency signals, which may include
14 omni-directional antenna (whip), directional antenna (panel), and parabolic antenna (dish).

15 3. Wireless Communications Facility (WCF): Any unstaffed facility for the transmission and/or
16 reception of radio frequency signals usually consisting of antennas, an equipment cabinet,
17 transmission cables, and a support structure to achieve the necessary elevation.

18 4. Wireless Communications Support Structure (Support Structure): A structure designed and
19 constructed specifically to support an antenna array, and may include a monopole tower, lattice
20 tower, guy-wire support tower or other similar structures. Any structure which is used to attach an
21 attached WCF to an existing building or structure (hereinafter "attachment structure") shall be
22 excluded from the definition of and regulations applicable to support structures.

23 5. Wireless Communications do not include small cells for the purposes of Title 19 MICC.

24

**CITY OF MERCER ISLAND
ORDINANCE NO. 19-22**

AN ORDINANCE OF THE CITY OF MERCER ISLAND, WASHINGTON, RENEWING FOR SIX MONTHS INTERIM DESIGN AND CONCEALMENT STANDARDS FOR DEPLOYMENT OF SMALL CELL FACILITIES ADOPTED IN ORDINANCE 19C-02; PROVIDING FOR SEVERABILITY AND ESTABLISHING AN EFFECTIVE DATE.

WHEREAS, the City Council unanimously passed Emergency Ordinance 19C-02 (“Ord. 19C-02”) on January 15, 2019, and held a public hearing on March 5, 2019, in response to the Federal Communications Commission’s Declaratory Ruling and Third Report and Order (“New Rules”) relating to small cell facilities, which became effective January 15, 2019; and

WHEREAS, the New Rules significantly preempt the City’s ability to regulate the installation of small cell facilities on City-owned public rights-of-way; and

WHEREAS, aesthetic requirements imposed by the City under the New Rules on installation of small cell facilities must be published in advance and must also be reasonable, no more burdensome than those applied to other types of infrastructure deployments, and objective; and

WHEREAS, Ord. 19C-02 adopted interim design and concealment standards for deployment of small cell facilities; and

WHEREAS, the design and concealment standards for small cell facilities in Ord. 19C-02 are effective for an initial period of six months, unless repealed, extended or modified by the City Council after subsequent public hearing(s) and entry of appropriate findings of fact; and

WHEREAS, the City Council held a public hearing, and following the close of the public hearing and considering public testimony, adopted Ord. 19-10 extending the effective period of Ord. 19C-02 for an additional six months; and

WHEREAS, the City has not yet adopted permanent design and concealment standards for deployment of small cell facilities; and

WHEREAS, the interim design and concealment standards adopted under Ord. 19C-02 will expire on or about January 14, 2020; and

WHEREAS, the conditions that existed when Ord. 19C-02 was adopted requiring the need for the City to have interim design and concealment standards for deployment of small cell facilities continue to exist today; and

WHEREAS, the City Council finds that deployment of small cell facilities with unregulated design and concealment standards may result in uncoordinated installations, visual blight, interference with public facilities and equipment, and traffic dangers that pose harm to public health, safety, property, and welfare; and

WHEREAS, to prevent the potential harm to public health, safety, property, and welfare, the City Council concludes that the City needs to extend the interim design and concealment standards

for deployment of small cell facilities until permanent standards can be adopted following the process and procedures for adopting development regulations; and

WHEREAS, the City is authorized under RCW 35A.63.220, 36.70A.390 to renew an interim zoning and official control ordinance for one or more six-month periods, provided it holds a public hearing on the same prior to each renewal; and

WHEREAS, consistent with the provisions of RCW 35A.63.220 and RCW 36.70A.390, the City Council held a public hearing prior to passing this Ordinance;

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF MERCER ISLAND, WASHINGTON, DO ORDAIN AS FOLLOWS:

Section 1. Whereas Clauses Adopted. The “Whereas Clauses” set forth in the recital of this Ordinance are hereby adopted as the findings and conclusions of the City Council for passing this Ordinance. Furthermore, the “Whereas Clauses” set forth in the recital of Ord. 19C-02 are hereby adopted by reference as additional findings and conclusions of the City Council for passing this Ordinance.

Section 2. Interim Standards Renewed. On January 13, 2020 prior to the expiration of the current effective period, the effective period of Ord. 19C-02 and the Interim Design and Concealment Standards for Deployment of Small Cell Facilities, as set forth in Exhibit A of Ord. 19C-02 and adopted thereunder, shall be renewed under RCW 35A.63.220 and RCW 36.70A.390 for another six-months until July 13, 2020, unless repealed, extended or modified by the City Council.

Section 3. Severability. If any section, sentence, clause, or phrase of this Ordinance should be held to be invalid or unconstitutional by a court of competent jurisdiction, or its application held inapplicable to any person, property, or circumstance, such invalidity or unconstitutionality shall not affect the validity of any other section, sentence, clause, or phrase of this Ordinance or its application to any other person, property or circumstance.

Section 4. Publication and Effective Date. A summary of this Ordinance consisting of its title shall be published in the official newspaper of the City. This Ordinance shall take effect and be in full force five days after the date of publication.

Passed by the City Council of the City of Mercer Island, Washington, at its regular meeting on the 3rd day of December 2019 and signed in authentication of its passage.

CITY OF MERCER ISLAND

Debbie Bertlin, Mayor

Approved as to Form:

ATTEST:

Bio F. Park, Interim City Attorney

Deborah A. Estrada, City Clerk

Date of Publication: _____



Interim Small Cell Ordinance: Extension

City Council

December 3, 2019

Tonight

- Overview
- Permanent solution
- Public Hearing



Overview

- January 15, 2019 – City Council adopted an interim small cell ordinance (Ord. 19C-02)
 - March 5, 2019 – Public Hearing
 - July 15, 2019 – “original” expiration of Ord. 19C-02
- June 18, 2019 – City Council extended the interim small cell ordinance
 - Public Hearing
 - January 14, 2020 – “second” expiration of Ord. 19C-02
- Tonight – City Council “third” extension of interim small cell ordinance



Permanent Solution

- November 20, 2019 – Planning Commission Introduction & Scope of Work discussion
- January / February: confirm Scope of Work with City Council
- February – April: Planning Commission review of draft regulations
- May – July: City Council review and adoption of permanent standards



Public Hearing

Recommended Motion

- Suspend the City Council Rules of Procedure 6.3, requiring a second reading of an ordinance
- Adopt Ordinance No. 19-22, extending the Interim Design and Concealment Standards for Small Cell Facilities deployment established under Ordinance No. 19C-02



Item 7.





2019 Comprehensive Plan Amendments

City Council
December 3, 2019

Tonight

- Overview
- Summary of changes between November 19 and tonight
- Recommended Motion



Proposed Amendments (1 of 3)

- Four amendments docketed in November 2018:
 1. Town Center subarea designations
 2. Climate change
 3. Economic development
 4. Multi-modal transportation



Proposed Amendments (2 of 3)

- Amendment 1: TC subarea regulation designations
 - Regulatory figure in Comprehensive Plan (policy document)
 - Figure retained in Mercer Island City Code

- Amendment 2: Climate Change
 - Updated background text
 - Policy language supporting existing programs and supporting an expansion of existing programs



Proposed Amendments (3 of 3)

- Amendment 3: Economic development
 - Support continued economic development, particularly in Town Center
 - Community-based approach to develop an economic development strategy
- Amendment 4: Multimodal Transportation
 - Policy language supporting existing programs and supporting the development of level of service for pedestrian, bike, and transit modes



Summary of Changes

- Exhibit 3 (page 23-30): comparison table
- Amendment 1 (page 4-6): No change
- Amendment 2 (page 7-12):
 - “Up-Level” goal and policy language (page 7-9)
 - Minor edits to contextual language (page 9-12)
- Amendment 3 (page 13):
 - New policy 14.1, amendments to policies 14.5, 14.8, 14.9
- Amendment 4 (page 14-15):
 - Amendment to policy 12.5



Recommended Motion

- Adopt Ordinance No. 19-17, amending the Mercer Island Comprehensive Plan Land Use, Capital Facilities, and Transportation Elements.
 - Once seconded, consider any further amendments



Item 7.



The background of the slide features a large, light blue watermark of the official seal of the City of Mercer Island, Washington. The seal is circular and contains the text "MERCER ISLAND" at the top and "WASHINGTON" at the bottom. In the center, there is a stylized sun with rays and wavy lines representing water below it.

Planning Commission Recommendation: 2019 Minor Code Amendments

City Council

December 3, 2019

Tonight

- Overview of Proposed Amendments
 - Background
- Planning Commission Recommendation

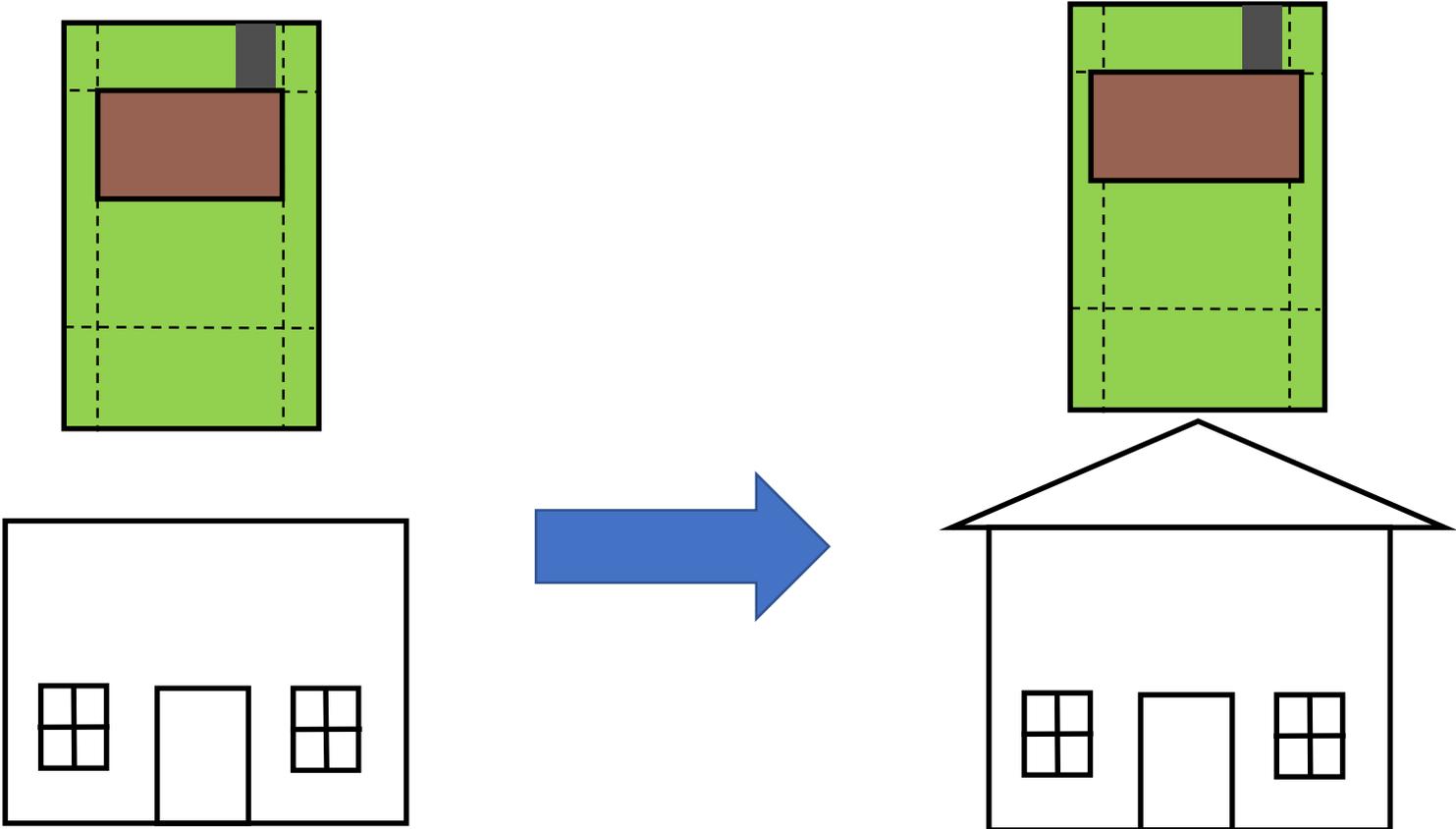


Overview (1 of 4)

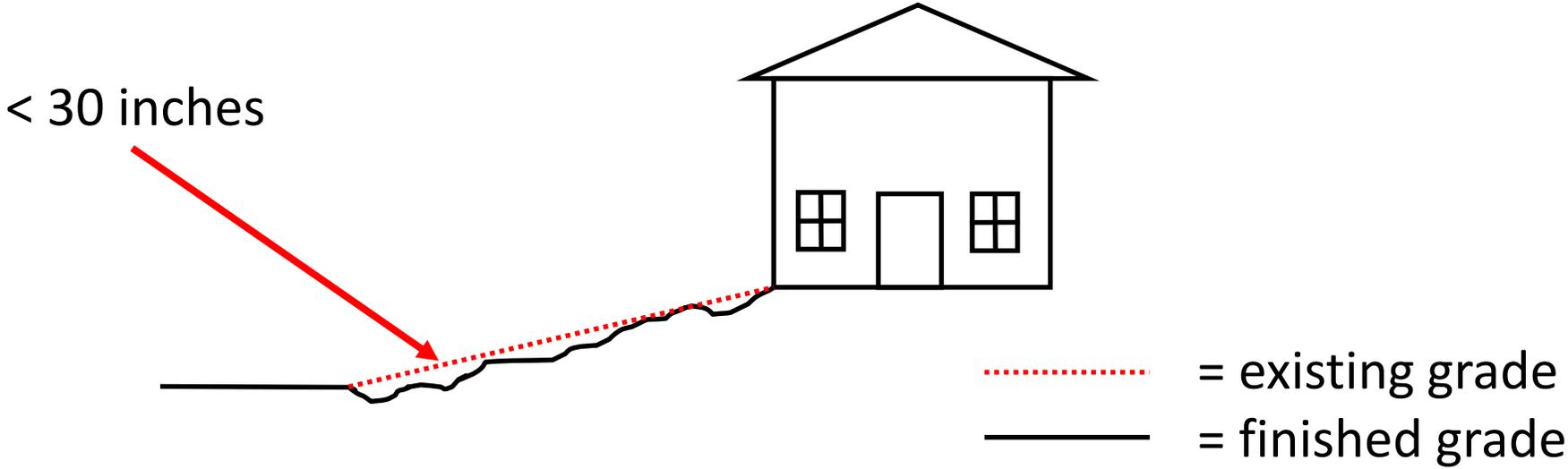
- 19.02.020 (page 5-6, line 33 to 3) – Clarify language around side yard standards
 - Replace “depth” with “width”
 - Eliminate redundant language
- 19.02.020 (page 6, line 7 to 22) – Clarify structures allowed in setbacks
 - Clarify language regarding when eaves may extend into minimum side setbacks
 - Allow grading and retaining walls for driveways providing access to a house



Eaves in Setbacks



Driveway access



Overview (2 of 4)

- 19.03.010, 19.04.010, 19.04.020, 19.04.050 (page 6-7, line 28 to 36) – Building Height in MF zones
 - Establish methodology for calculating height
 - Additional limit on building height for the MF-2L (24 foot façade on downhill)
- 19.09.100 (page 7, line 42) – Clarification that “development” is not avoided
- 19.15.030, Table A (page 8, line 4 to 5) – Revising “Parking Variance” to “Parking Modification”
- 19.15.110 (page 9, line 12 to 20) – Allows city to issue on decision instead of “never-ending” request for correction



Overview (3 of 4)

- 19.15.220 (page 9-10, line 28 to 12) – correcting cross-reference
- 19.15.230, 19.15.250, 19.15.260 (page 10-13, line 19-31) – code amendment docketing procedure
 - Establishes a docketing process whereby any person may initiate a code amendment
 - Retains current “code amendment application”
 - Provides notice in newspaper of general circulation
- **Option:** Eliminate code amendment application process
 - Page 12, delete lines 24-26, revise line 27, delete lines 30-31



Overview (4 of 4)

- 19.16.010 (page 13, line 37 to 43) – definitions
 - Create definition for “Lot, Irregular” – used when identifying rear yard on some irregularly shaped lots
 - Amend definition of lot coverage to include roof eaves and overhangs



Planning Commission Recommendation

- Public Hearing on September 18, 2019
- Planning Commission has recommended that the City Council adopt the minor code amendments
- Recommended motion:
 - Set Ordinance No. 19C-21 for second reading and adoption on the December 10, 2019 Consent Calendar.



Item 7.





BUSINESS OF THE CITY COUNCIL CITY OF MERCER ISLAND

AB 5629
December 3, 2019
Regular Business

AGENDA BILL INFORMATION

| | | |
|----------------------------|---|--|
| TITLE: | AB 5629: Aubrey Davis Park Master Plan Adoption | <input type="checkbox"/> Discussion Only <input checked="" type="checkbox"/> Action Needed: <ul style="list-style-type: none"> <input type="checkbox"/> Motion <input type="checkbox"/> Ordinance <input checked="" type="checkbox"/> Resolution |
| RECOMMENDED ACTION: | Approve Resolution No. 1571 adopting the Aubrey Davis Park Master Plan as revised and direct the City Manager and the Parks and Recreation Commission to develop a scope of work for the \$500,000 Washington State Department of Commerce Grant. | |

| | |
|-------------------------------|--|
| DEPARTMENT: | Parks and Recreation |
| STAFF: | Ryan Daly, Interim Parks and Recreation Director |
| COUNCIL LIAISON: | n/a |
| EXHIBITS: | <ol style="list-style-type: none"> 1. Revised Aubrey Davis Park Master Plan 2. List of Revisions 3. Resolution No. 1571 |
| CITY COUNCIL PRIORITY: | 2. Prepare for Light Rail and Improve Mobility |

| | |
|-------------------------------|--------|
| AMOUNT OF EXPENDITURE | \$ n/a |
| AMOUNT BUDGETED | \$ n/a |
| APPROPRIATION REQUIRED | \$ n/a |

SUMMARY

This agenda bill presents the revised Aubrey Davis Park Master Plan (Plan) for adoption by the City Council and includes details related to recent revisions to the Plan.

BACKGROUND

The draft Plan was developed with significant input from the community over the past year. Planning began in the fall of 2018 with project scoping based on input received from an initial community survey (launched 8/1/19 and completed 10/9/18) and four public forums (9/25/2018, 10/25/2018, 11/8/2018, and 11/29/2018). The planning team (which includes staff from multiple City departments, members from the Washington State Department of Transportation (“WSDOT”), and the project consultant team HBB) developed draft products to characterize how people currently utilize the park and what park amenities or uses could be included in the final Plan.

Products received through initial community engagement opportunities were presented in a second phase of vision and goals at Open House #1 on February 28, 2019 and in a concurrent online survey. City Council

previewed these products at its January 15, 2019 Study Session. The planning team then developed a set of concept alternatives for ideas that had been gathered. Most of these concepts were not supported based on the public feedback received through Open House #2 on April 23, 2019 and in the concurrent online survey. Staff reported this to City Council at its July 16, 2019 Study Session.

The City Council reviewed the Plan at Study Sessions on October 15, 2019 (AB 5613) and November 4, 2019 (AB 5622) and provided additional guidance and feedback.

REVISIONS TO THE DRAFT PLAN

Plan revisions are shown in red text in Exhibit 1 and a list of the substantive revisions is provided in Exhibit 2. Minor revisions for clarity and readability are not listed in the exhibit.

Plan revisions came from four sources. First, the City Council deliberated on seven revisions proposed by staff at the November 4, 2019 Study Session. The changes that received City Council support are as follows:

1. **Planting Palette:** Designate the “Northwest Feel” palette as primary for the park and include the “Ornamental” and “Sensory” palettes to be used when appropriate for specific project designs.
2. **Water Conservation:** Include both strategies in the final Plan. Conduct trials of the strategies on a limited basis with public input and evaluate before considering further implementation.
3. **Trail Width:** Include photographic cross sections. Change the Lid Park cross section trail width label from “current width” to “12 feet”.
4. **Restroom Conflict Zone:** Keep both options in the final Plan. The costs and operational considerations for each option will need to be further studied before a final recommendation is made.
5. **Optional Informal Soft Surface Trails:** Remove from the Plan.
6. **New Restroom (Westside Lid Park):** Show both locations in the final Plan. Develop an engineering analysis as part of an early design phase to assess the cost implications of either location. This is primarily related to the potential utility impacts (sewer and water infrastructure costs) associated with the proposed restroom locations.
7. **Dog Off-leash Area:** Keep in the Plan without specific design details. Development of this area for a future off-lease dog area will require additional analysis and public input.

Second, a few additions were proposed in AB 5622, Exhibit 1: Consolidated List of Questions and Comments:

- Discussion of ADA requirements
- A description of the trail lighting project on the Luther-Shorewood section of trail
- Clarification of how costs will be updated and that inclusion in the Plan does not imply a project will be funded or given special priority.
- Criteria for public engagement on the development of future projects identified in the Plan.
- Addition of safety as part of the criteria for the highest priority projects.

Third, staff also received feedback from Councilmembers that it is unclear whether the trail cross sections apply to just the main Mountains to Sound Trail or to all of the trails in the park. The Plan was revised to clarify that the cross section applies to the Mountains to Sound Trail Only, shown as a red line in the Plan.

Fourth, the Arts Council ad hoc committee on the Aubrey Davis Park Master Plan also asked that clarifying language be added to the Plan to provide a reference to the information in the appendices and that it refer to the City's existing policies on public art to indicate the process that any arts, culture or placemaking project will follow.

With these revisions, the planning team believes that the Plan is complete and ready for adoption. Staff's recommendation is to adopt Resolution No. 1571 (Exhibit 3).

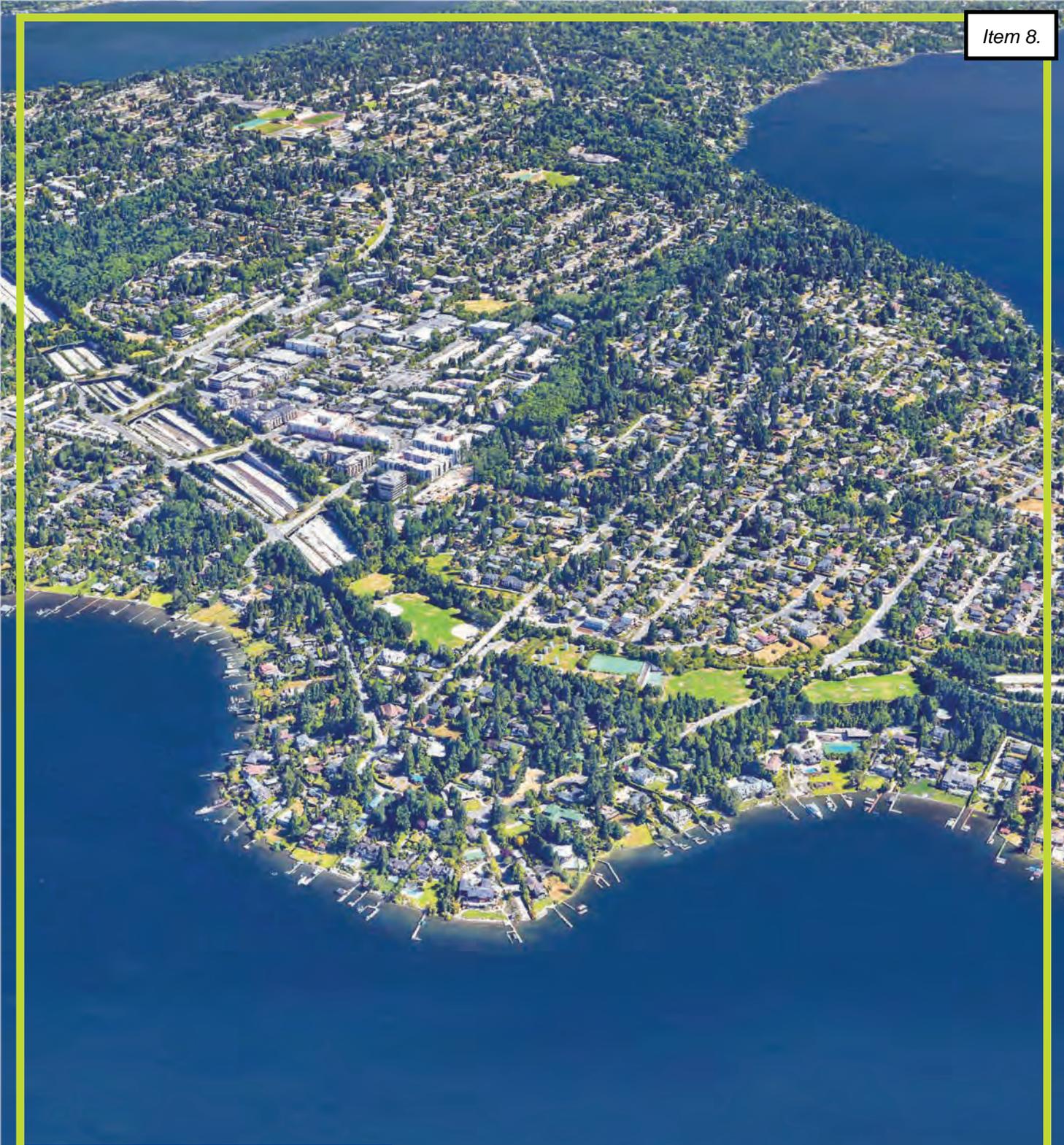
DEPARTMENT OF COMMERCE GRANT

In June 2019, the City of Mercer Island was awarded a Direct Appropriation Grant from the Washington State Department of Commerce for trail safety and accessibility improvements. City staff are preparing the required paperwork to enter into an agreement with the State for this grant award. The grant funding will be available once the grant agreement is executed. The prioritized list of projects in the Plan will be the starting point for consideration of how the grant funds are utilized.

Staff recommends that the City Manager engage the Parks and Recreation Commission in reviewing a scope of work that is consistent with the Plan, provides for adequate public engagement and meets grant requirements. The recommend scope of work for the grant funding will be presented to the City Council for consideration and approval along with a budget adjustment in Q1 2020.

RECOMMENDATION

1. Approve Resolution No 1571 adopting the Aubrey Davis Park Master Plan as revised.
2. Direct the City Manager and the Parks and Recreation Commission to develop a recommended scope of work for the \$500,000 Washington State Department of Commerce grant to be presented to the City Council for consideration and approval in Q1 2020.



MASTER PLAN REPORT

AUBREY DAVIS PARK

Revised Draft



City Council

Debbie Bertlin, Mayor
Salim Nice, Deputy Mayor
Lisa Anderl, Councilmember
Bruce Bassett, Councilmember
Wendy Weiker, Councilmember
David Wisenteiner, Councilmember
Benson Wong, Councilmember

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Anne Tonella-Howe, Assistant City Engineer
Casey Troy, Parks Generalist
Paul West, Capital Projects & Planning Manager

Parks & Recreation Commission

Sara Berkenwald
Don Cohen
Lyn Gualtieri
Jodi McCarthy, Vice Chair
Amy Richter
Kirk Robinson
Rory Westberg, Chair

Mercer Island Arts Council

Anumeha
Amy Barnes, Vice Chair
Erik Gordon
Matt Lome
Damian Schwiethale
Xixi Shakes
Suzanne Skone
Gaylene Vaden
Erin Vivion, Chair



Consultant Team

HBB Landscape Architecture (HBB)

BERK Consulting

Toole Design

David Evans and Associates (DEA)

Rolluda Architects, Inc.

4Culture





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APPENDICES

- Appendix A: Landscape **Typologies** Map
- Appendix B: Landscape Assessment
- Appendix C: Arborist Notes
- Appendix D: Recommended Plant List
- Appendix E: Soil Test Results & Locations
- Appendix F: Bollard Study
- Appendix G: Conflict Area Study
- Appendix H: Facilities Assessment
- Appendix I: Arts & Culture Vision Document
- Appendix J: Arts & Culture Opportunities
- Appendix K: Community Outreach Events

INTRODUCTION

Aubrey Davis Park is a 2.8-mile long park along I-90 that is primarily owned by the Washington State Department of Transportation (WSDOT) and managed by the City of Mercer Island. The Park on the Lid, the Mountains to Sound Trail, the Boat Launch, and the Greta Hackett Outdoor Sculpture Gallery are all part of Aubrey Davis Park.

The purpose of the Master Plan is to establish a long-term vision for the amenities, trails, and open space areas in Aubrey Davis Park. The Master Plan was developed based on feedback received from public engagement events – from online surveys and public forums to open houses and discussions on the City's Let's Talk Mercer Island website.

The Master Plan is divided into four main categories:

- Vegetation Management
- Trail Improvements
- Park Improvements
- Arts, Culture & Placemaking

Recommendations included for each category are conceptual only. Additional planning and design will be needed before any of the recommendations can be implemented. The final Master Plan will also serve as a platform to renegotiate the maintenance agreement with WSDOT.







BACKGROUND AND CONTEXT

Brief History

Built in 1992 as part of I-90, Aubrey Davis Park is a 90+ acre, 2.8-mile-long recreation and transportation facility on Mercer Island. Over time, it has become a treasured community asset.

In 1970, the state highway commission proposed the widening of I-90 along Mercer Island to sixteen lanes. Aubrey Davis, mayor at the time, demanded the highway design take into account the impact on the surrounding community, famously testifying, “We don’t want to see it. We don’t want to hear it. We don’t want to smell it.” Through community input, this resulted in a reduced eight-lane highway and concrete lids over the freeway that are now known as the Lid Park between 63rd Avenue Southeast and 76th Avenue Southeast. Today, it is a beloved park that has improved the quality of life for residents and visitors to Mercer Island for 27 years.

Mercer Island has seen significant growth since the early 1990’s. A light rail station near the Town Center will open in 2023, bringing commuters and new visitors alike near the Park on the Lid. Commuters and residents also cycle east-west along I-90 through the Mountains to Sound Trail, a vital connector between Mercer Island, downtown Seattle, and Bellevue/Redmond to the east.

Given the age of the park, changing park needs, increased use, and the expansion of the light rail, a conversation about the future of this park was needed to plan for its future and continued stewardship.



Photo of an interpretive sign honoring Aubrey Davis near the tennis courts



Segment of the Mountains to Sound Trail near 84th Avenue SE

Other projects adjacent to the park that impact and overlap with Aubrey Davis Park have also served as catalysts for this master planning process. These include the King County Sewer Upgrade project, a new commuter parking project in the Town Center, and the integration of Sound Transit's Link light rail station.

King County's Wastewater Treatment Division (WTD) needs to replace sewer pipes that serve areas in North Mercer Island, the southwest portion of Bellevue near Enatai Beach, and the town of Beaux Arts Village. This pipeline carries wastewater from the North Mercer Island Pump Station to the Swayolocken Pump Station in Bellevue, and the upgrade project directly impacts a portion of Mountains to Sound Trail from Island Crest Way to East Mercer Way. The trail through the project limits will be completely reconstructed to current WSDOT and King County standards as part of this project. Construction is anticipated to begin in 2020.

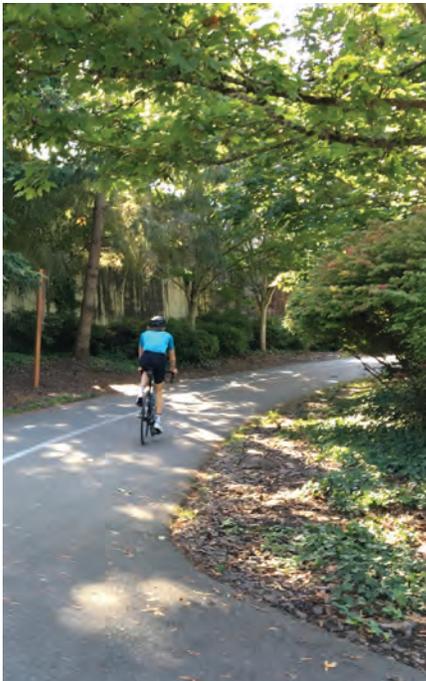
The Mercer Island light rail station is located in the center of I-90 near the Mercer Island Park-and-Ride and the Town Center. Riders enter at either 77th Avenue Southeast or 80th Avenue Southeast. People walking or cycling also access the station from the nearby Mountains to Sound Trail. It is scheduled to begin operation in 2023.

To accommodate this growth, new commuter parking is proposed near Aubrey Davis Park in the Town Center. This will be located at the corner of SE 27th ST and 80th Ave SE and is part of a mixed use project through a public/private partnership with the developer.

The City of Mercer Island also needs to renegotiate the existing landscape maintenance agreement with WSDOT. While most of the park remains within WSDOT right-of-way, the park is maintained by City of Mercer Island maintenance staff based on agreements between the City of Mercer Island and WSDOT from 1987 and 1989. The maintenance agreement is very general, focused on basic upkeep, and does not allow for the required level of soil improvement and periodic capital reinvestment needed for a healthy, growing landscape. Over the years, the required level of maintenance has grown significantly and care under the current maintenance agreement is no longer sufficient.

EXISTING CONDITIONS





Landscape

In interior park areas, the landscape character is predominantly open lawn with deciduous trees interspersed throughout the open space. Soils here are compacted, are sandy, and have low organic matter and nitrogen levels. Many trees exhibit signs of dieback and shallow root systems. This is likely indicative of soil limitations across all the lidded park areas. Off the lid along the trail corridor, the landscape is mostly ornamental shrubs and groundcover with deciduous trees. Some planted areas show signs of chronic water stress. Much of the vegetation has become overgrown, reducing potential sightlines along the corridor. The north boundary of the lid contains stands predominantly of conifer trees. These exhibit crowding, stagnant growth, and attrition.

Opportunities exist for creating a more robust and resilient landscape, improving sightlines, and reducing the cost of long-term maintenance. Areas between the trail, parking, and active areas of the site can be managed to improve sight distances and visibility into the park. [Refer to Appendix B: Landscape Assessment for the full landscape assessment and recommendations.](#)

Trails

The Mountains to Sound Trail (also known as the I-90 regional trail) runs along the edge of the open space within the park. Cyclists, pedestrians, playground users, and athletic field users all share the same space, with little to no separation of uses. This has created significant concerns from the community regarding potential conflicts and the safety of all users in these areas of the park. The topography in this area often results in higher bicycle speeds as well, and some areas have limited sight distances due to the trail alignment and/or vegetation along the edges of the trail. The trail grade does not meet standards outlined in the Americans with Disabilities Act (ADA) between the playground and the tennis court area, or in segments of the trail as it continues down to the softball/baseball fields. There is also no ADA access from the parking lot by the soccer fields to West Mercer Way.

Recreation Programming

This is a heavily used section of the park for sports and active recreation. Heaviest use is concentrated around the area near the sports fields.

The soccer/lacrosse field west of West Mercer Way is used to the point of requiring frequent maintenance and occasional closure of the field to restore the lawn during the growing season. At all fields, there is not enough parking to support the volume of users, particularly during sporting events. Dog owners often use the athletic fields to exercise their dogs off-leash. This can result in dog waste on the fields and occasional holes in the grass surface, including areas of the fields that have a higher intensity of play.

While the fields themselves are accessible from adjacent parking lots, the playground, informal lawn and basketball courts are not ADA-accessible from any of the existing parking areas. The playground, one of two in the park, is also not ADA-accessible and is mainly suitable for younger children. The basketball court area is cracking, not well used, and in general feels like a less desirable place to be in.

In the upper area of the park, the tennis courts are used often, though comments received throughout the public outreach process indicate that all of the courts are seldom used at the same time. Apart from the courts, the open areas around the stacks are used by occasional walkers and off-leash dogs. While there are picnic tables and barbeque areas around the stacks, they don't appear to be used very often. The open space behind the tennis courts is not very visible from the trail and is seldom used. The tennis court surfacing is cracked and in need of repair, and this entire area is not ADA-accessible from the nearby parking due to the topography in this area.

There is opportunity to organize space and programming to improve athletic uses, activate unused areas, and increase the range of activities within the park to appeal to a variety of ages and interests. In particular, the basketball courts, area on the roof of the maintenance facility, and open space around the stacks and tennis courts are opportunities for potential improvements including accessibility, especially as these facilities need to be improved or replaced over time.

Arts and Culture

Public art in this portion of the park consists of *Stepping Forward and Playful Pup*, two sculptures near the playground along West Mercer Way, and *Darwin's Dream*, a mural to the south of where the I-90 off-ramp meets West Mercer Way.

Opportunities for arts and cultural elements could include wayfinding, interpretive elements for the history of the park or I-90 construction, and opportunities to support placemaking in the park.



Landscape

The landscape in this portion of the Lid Park is predominantly lawn and shrub/groundcover areas, with a greater number of large trees. Along the trail, this becomes a forested condition as it continues off the lid to the east. Ivy is common here and some trees along the trail are dead or dying.



As in the previous section, improving maintenance and vegetation management can enhance sightlines and visibility into open spaces, especially along the trail and at the entrances into the open space areas. The greater number of trees and shade provides opportunities to enhance passive experiences. In addition, planted tiers on the easternmost edge of the lidded park can be improved to promote longevity, reduce access, and reduce sound from I-90. The areas along the portal edges here, especially on the east side of the lid, will need significant vegetation management and replacement to maintain a healthy landscape condition into the future. Refer to Appendix B: Landscape Assessments for the full landscape assessment and recommendations.

Trails

This portion of the trail contains one of the biggest conflicts within the entire park corridor, specifically where the trail crosses from the south to the north side between the **Feraglia Fields** and the restroom. The trail as it descends along North Mercer Way is also steep with poor visibility, and a major trail intersection occurs at the bottom of the hill and on a curve with limited sight distances. Park users all share the same space on the trail through this part of the park as well.



Opportunities to adjust the alignment and amount of separation between pedestrian and bicycle users in this part of the park would improve safety, wayfinding, traffic-calming, and access to all areas of the park. There are also opportunities to highlight the trail connection to the Town Center along the south side of the fields.

Recreation Programming

This section of the park contains heavily used ballfields, an open-use lawn area, a playground, and a picnic shelter. The ballfields are generally in good condition, though are sometimes used as an off-leash dog area, leaving dog waste and occasional holes within the fields. Backstops don't always contain foul balls, and these can affect spectators and trail users.

East of the field, the picnic shelter in particular is well used and is the only one in the entire park corridor. Similar to the previous section, the playground is well-used but primarily for younger kids and is not ADA-accessible. The playground, picnic shelter, and restroom are along accessible walkways from the parking area, but it is a significant walk for anyone with impaired mobility and without any rest areas (i.e. seating) in between.

Recreation opportunities here are centered on the open lawn to the east of the playground and improving functionality and maintenance of the existing uses. The open lawn area to the east can offer a variety of passive, unstructured activities, better access through the lidded area from the Town Center, or potentially new uses.

Arts and Culture

There are two art pieces in this area near the picnic shelter: *The Yearling* and *Hope*. Both are sculptural installations in close proximity to each other. Opportunities exist to integrate art into interpretive or educational elements, support placemaking, and improving wayfinding.



Landscape

The landscape here is an open lawn condition with trees in the open space area and adjacent to the trail. Along the I-90 frontage, the landscape is a mix of shrubs, evergreens, and some English ivy. Many of the trees here are dying or nearing the end of their lifespan, especially the evergreen trees.

There is opportunity to use planting to create space or more interactive experiences in both the open space and along the trail as it moves towards the lidded park, acting as a type of gateway into the lidded park area from the Town Center. Improved visibility between the park and the Town Center would also help this space feel more welcoming and improve wayfinding across the corridor.

Refer to Appendix B: Landscape Assessments for the full landscape assessment and recommendations.

Trails

The trail in this location is generally quieter and used more by pedestrians and casual cyclists. Surrounded by trees, it is a more passive experience with less intensity of recreational users overall. All users still use the same trail space, creating occasional conflicts, especially if larger groups try to move through this area.

Opportunities exist to strengthen the connection between the lidded park and Town Center, as well as disperse trail activity and attract new interest in Town Center businesses and events. There are also opportunities to better connect the trail to the future light rail station areas.

Recreation Programming

The open space and trail connections are more passive, with no formal recreation facilities. This space is a great opportunity to create places for gathering or passive recreation due to its location between the lidded park and Town Center.

Arts and Culture

While there are no current art installations, this presents a highly visible area near the Town Center, the Greta Hackett Outdoor Sculpture Gallery, and the future light rail station.





Landscape

The Luther Burbank Lid includes the lidded park area and the open space near Island Crest Way. Within the open space near Island Crest Way, the landscape is an open lawn surrounded by deciduous trees. A mix of shrubs, deciduous, and evergreen trees are planted in wide beds along the edges of the Luther Burbank Lid. The center of the lid itself is an open lawn surrounded by trees, with a steeply graded forest along the southern edge. The condition of the landscape is generally similar to other areas in the park.

This area presents an opportunity for new planting to create a more engaging or interactive space, integrate playful elements into the landscape, or create an enhanced sense of respite from the City, especially with its close proximity to the Town Center. **Refer to Appendix B: Landscape Assessments for the full landscape assessment and recommendations.**

Trails

The Luther Burbank Lid can currently be accessed by the Mountains to Sound Trail to the north via a ramp and from Island Crest Way to the west. There is currently no way to formally enter the open space from SE 28th Street, with steep grades posing a challenge to creating any ADA-accessible entry, or from the Ravine Trail to the east.

Improved pedestrian connections to Town Center can increase usage of this area. A connecting trail to Luther Burbank Park is already part of the Luther Burbank Park Master Plan (2006) and has been approved by WSDOT with future construction anticipated. Exploring connections to SE 28th Street and improvements to the Ravine Trail could connect surrounding neighborhoods to the lid, Luther Burbank Park, and Town Center itself.

Recreation Programming

Despite its proximity to the Town Center, the Luther Burbank Lid is relatively unknown based on feedback received during the public outreach process. Lack of visibility, wayfinding, and a clear reason to visit are the primary reasons that were heard in surveys and open houses. There is also no way to formally enter from the neighborhoods to the south. Opportunities for passive recreation could be considered to enhance its presence within the overall park corridor.

Arts and Culture

This area features a sculptural installation of an eagle titled *Mercy*. Opportunities for artful connections to nature and to Luther Burbank could provide increased wayfinding and more engaging and unique experiences within the lid.



Landscape

The east corridor of Mercer Island along I-90 is a mix of forest, open lawn, and drought tolerant shrubs. Many of these are dying despite species hardiness, either due to age or to the site conditions they were planted in. Tree roots also heave the pavement in many areas along the trail, increasing maintenance and safety concerns.

There are opportunities to increase species diversity and habitat while keeping maintenance low. There are also opportunities to enhance the overall experience along this trail through vegetation management and a more adaptable plant palette. [Refer to Appendix B: Landscape Assessments for the full landscape assessment and recommendations.](#)

Trails

The trail in this location is direct and heavily used by both regional commuters and local residents. In some areas, it is directly against a wall, while other areas are more open and natural on either side of the trail. Nearing the boat launch, there is no continuous pedestrian sidewalk connection to the water. Topography through this area is significant and the trail does not meet ADA accessibility standards in some areas. There are very few opportunities to sit or rest along this segment of the trail. Vegetation has begun to encroach into the trail corridor and is limiting sight distances in some areas.



Improving connections and the overall feel of the trail in this area can enhance safety and the overall user experience. As restoration occurs, managing vegetation where it encroaches on the trail can improve sight-lines and maintain clear zones on either side of the trail, as well as protect the trail from future root incursions.

Recreation Programming

There are few areas for programmed activities in this area of the park. Residents have little to no places to take their kids to play. The Lake Washington shoreline to the east is well-used for its boat launch and for parking during major water-related events but is less-used by pedestrians and families due to lack of gathering spaces and sidewalk connections.

There are open spaces to the north and south of I-90 where it intersects 100th Ave SE that could provide some passive recreation opportunities and enhance wayfinding at the east end of the park.

Arts and Culture

There is a single art installation at the boat launch along the water titled *Gift of Reflection*.

As an entry point for visitors driving west to Mercer Island, this area presents opportunities for public art to create a gateway experience to the park or the adjacent neighborhood. Art or cultural references can also enhance wayfinding and the overall experience along the waterfront.



PUBLIC OUTREACH





PUBLIC OUTREACH

As public input is key to the design process, the City engaged the public throughout the course of the project. The following is a summary of the public outreach. **Item 8.** Refer to Appendix K: Community Outreach Events for full materials and feedback.

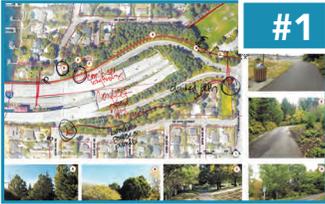
1 POP-UP EVENTS

AUGUST/SEPTEMBER 2018

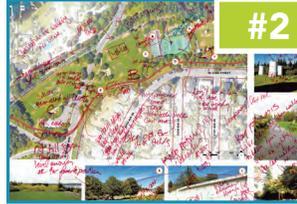


2 PUBLIC FORUMS

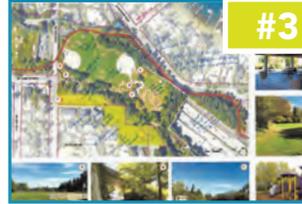
SEPTEMBER 25TH, 2018



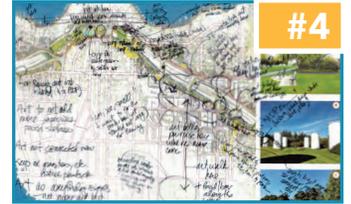
OCTOBER 25TH, 2018



NOVEMBER 8TH, 2018

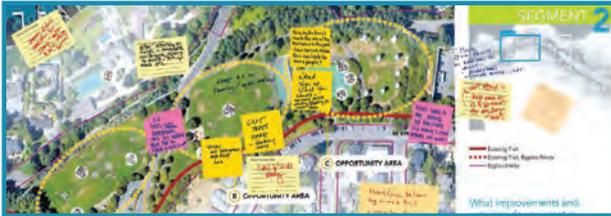


NOVEMBER 29TH, 2018



3 OPEN HOUSE #1

FEBRUARY 28TH, 2019



4 OPEN HOUSE #2

APRIL 23RD, 2019



5 OPEN HOUSE #3

SEPTEMBER 23RD, 2019





Initial public outreach began with pop-up surveys to assess preferred activities, amenities, and guiding principles for the Master Plan. These were conducted by the City and took place in August/September of 2018. Let's Talk Mercer Island, an online community forum, was also used to gather comments and feedback throughout the entire process.



43% PREFER PASSIVE ACTIVITIES

57% PREFER ACTIVE ACTIVITIES

276 PARTICIPANTS 95% LIVE ON THE ISLAND

Top activities that interest participants:

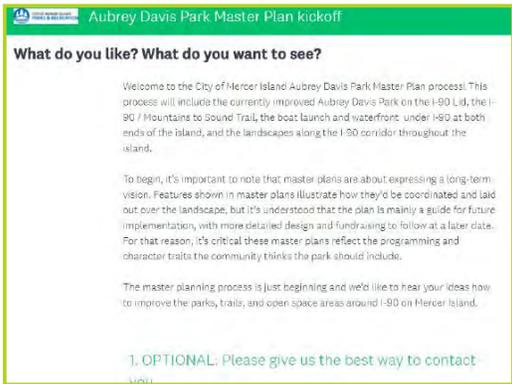
1. Walking or biking on the trail
2. Enjoying the landscape and scenery
3. Walking their dog
4. Playing sports
5. Enjoying public art

Top amenities to be improved or added:

1. Trails/Loop paths
2. Waterfront Access
3. Landscape (plantings, trees, etc.)
4. Picnic Tables & Shelters
5. Open Lawn
6. Playgrounds
7. Public Art
8. Community Gardens
9. Spray Parks
10. Multi-Use Sport Courts

Top guiding principles for the master planning process:

1. Reduce conflicts between trail users and other pedestrians
2. Improve environmental sustainability
3. Improve habitat/ecological systems
4. Improve existing amenities
5. Improve sightlines and visibility along trails and open space

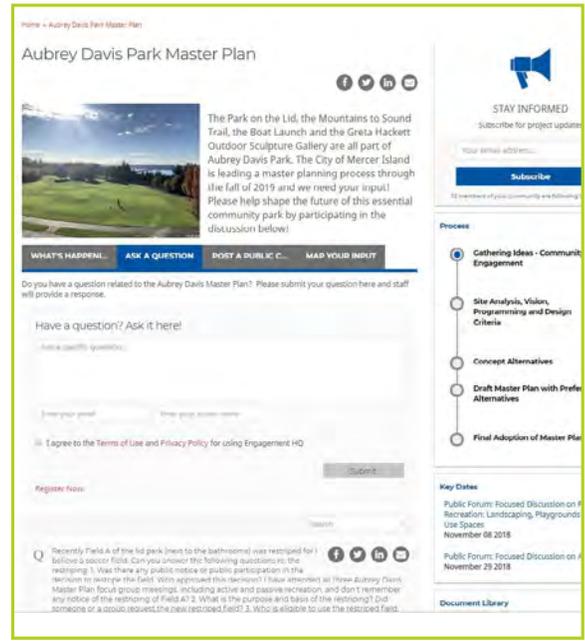


LET'S TALK

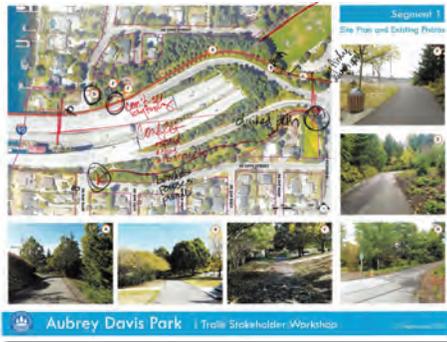
"Let's add more places in the park that encourage people to gather for reasons beyond sports. Being sheltered from the rain while gathering is helpful, though not required."
—Jonathan Shakes

The trail should be widened wherever possible. A 14-foot trail is much more user-friendly than is the current width. It can accommodate all users if it is widened."
—Mclausen

"Many trees have died between Sunset Highway sculpture park and the freeway... Will there be replacement trees to combat noise and air pollution as envisioned by Aubrey Davis?"
—dennyhenkel



Four public forums were conducted in the fall of 2018 to gather targeted community feedback on four specific topics: trails, sports and boating, landscape and open space, and arts and culture. Stakeholders were invited to comment on graphics prepared by the design team to gather general feedback and identify initial opportunity areas for proposed improvements.

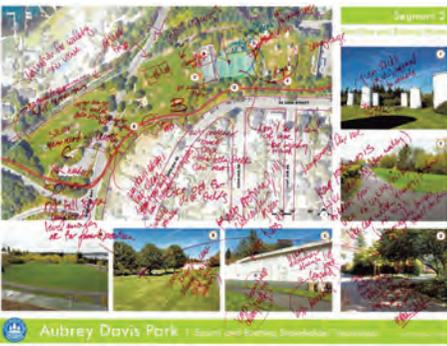


#1

Trails September 25th, 2018

High-Level Comments

- 14' wide trail with 2' wide shoulders preferred
- Separated 6' crushed surface trail where space allows
- Different trail experiences and types for trail users
- Include bypass route with connections
- Improve all trail intersections and conflict zones

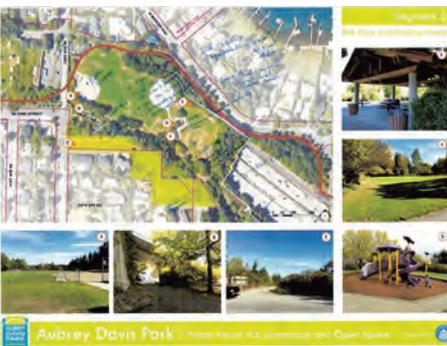


#2

Sports and Boating October 25th, 2018

High-Level Comments

- Improve safety between active & passive users at recreation areas
- Increase parking capacity to support uses (Areas A & B)
- Area C preferred for synthetic turf & lighting improvements
- Improve signage & wayfinding to water access areas
- Create a sense of place for west water access (e.g. dog park, family-oriented activities)

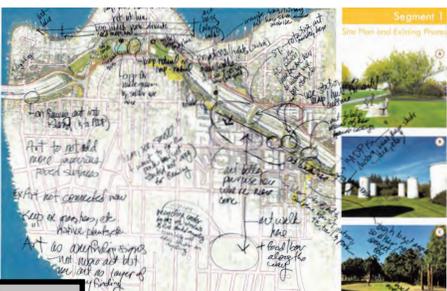


#3

Landscape and Open Space November 8th, 2018

High-Level Comments

- Maintain the overall character of existing landscape
- Consider more native & ecologically appropriate plants
- Remove ivy and other invasives
- Manage vegetation where overgrown or blocking views
- Connect Luther Burbank mini-Lid to adjacent areas
- Connect Town Center towards east of I-90
- Balance of programmed and unprogrammed space



#4

Arts and Culture November 29th, 2018

High-Level Comments

- Greater variety in types of art, especially interactive art
- Greater enjoyment for all ages
- Improved connection between art installations
- Stronger integration with surrounding context
- Respect natural environment for locations of art

Open House #1 focused on visioning and identifying uses and activities the surrounding community would like to improve at the park. 51 participants attended the in-person open house, held at the Mercer Island Community & Event Center on February 28th, 2019.

Opportunity areas identified for future improvements were presented to the public to validate locations and gather more specific feedback. Open house participants were asked to place sticker dots on boards to provide location-specific input on where they would like different types of activities to take place. Participants also had the opportunity to leave comments and suggestions.

A discussion on trail improvements was focused on exploring potential trail cross sections and understanding the diversity of trail users within the community. Five trail cross sections were presented to illustrate possible typical conditions, including a shared-use trail and a more separated approach.



Improvements & Modifications



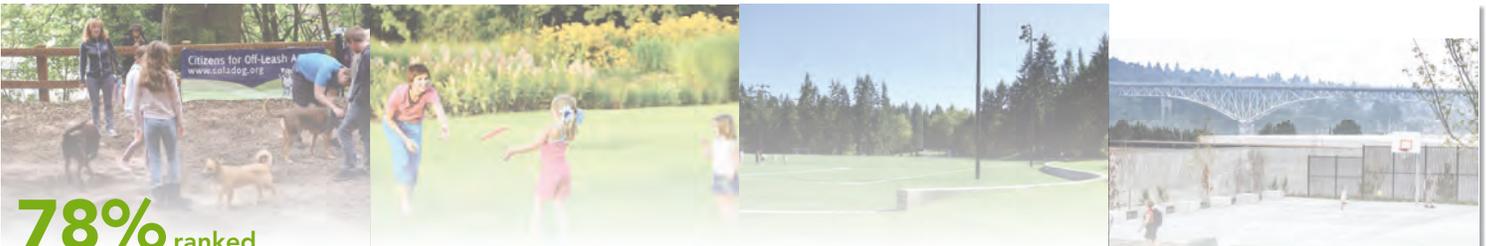
Separating bicyclists and pedestrians was the most commonly desired improvement among respondents in the Lidded Park and Town Center sections of the trail.

Suggested improvements included everything from pavement markings (surface striping) to physical barriers. Some respondents did not indicate which type of separation would be best but noted it was important.

Most Important Design Goal, Ranked

- 53% preferred to retain the natural character of the park
- 30% preferred providing for a variety of uses/activities
- 26% preferred allowing for a variety of trail experiences

Park Programming & Activities



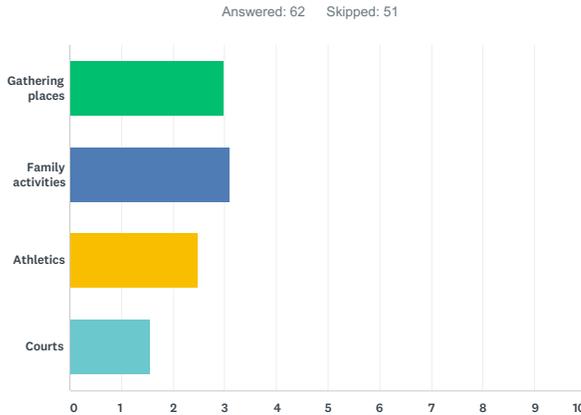
78% ranked

GATHERING AND FAMILY ACTIVITIES as the MOST important activity

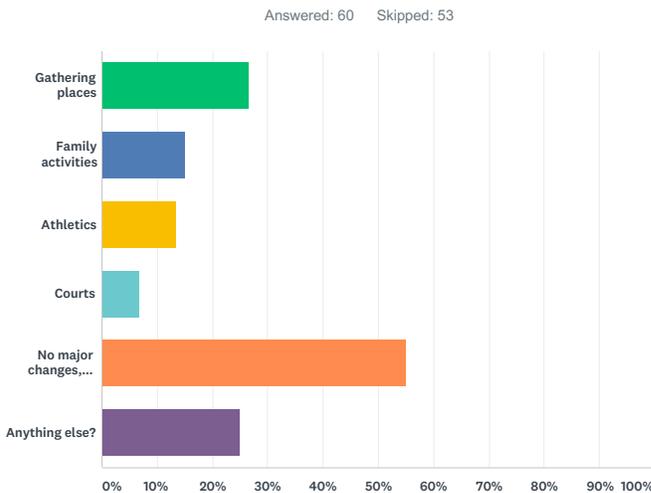
Survey Overview

The first Mercer Island Aubrey Davis Park Survey was designed to complement the first open house and capture feedback on the needed improvements and the long term vision and goals for Aubrey Davis Park. The survey was open from February 28, 2019, to March 15, 2019. See [Appendix K: Community Outreach Events](#) for full survey analysis report.

Q12 Thinking about the images above, please rank the following activities in terms of importance, with 1 being the most important to include and 4 being the least important to include in Aubrey Davis Park.



Q14 What recreation programs or activities would you like to see in this part of the Park (see map above)?



Trails feedback through the Lid Park:

- The most commonly reported improvement in this section of the trail is separating bicyclists and pedestrians.
- The second most commonly noted improvement is increasing the use of traffic calming measures to slow cyclists in this area. Several ways were indicated, including: roundabouts, narrower trails, and speed bumps.
- There was also an indication that signage could be improved on this part of the trail. Proposed improvements included signs that establish user norms, signs indicating "slow" areas, and signs indicating the trail as a cross-island trail.

At Open House #2, two design alternatives were presented for park improvements. 57 participants signed in to the in-person open house, held at the Mercer Island Community & Event Center on April 23rd, 2019.

Both alternatives were developed to address aging infrastructure, lack of ADA accessibility, and declining landscape. Designs were based on feedback received from previous public engagement events, including online surveys, public forums, and Open House #1.

The trail improvements presented were focused on traffic calming and wayfinding and explored trail bypass options with two proposed alternatives: one along the north side of the park and the other along the south side connecting into Town Center.

Participants were asked to place sticker dots on boards to indicate aspects they liked (in green) and didn't like (in orange). The number of dots per participant was not limited. As in the previous open house, participants also had the opportunity to leave comments and suggestions.

The most common theme within the responses was that the park functions well as it currently exists, and no new improvements are needed outside of safety improvements or better signage. Limiting the amount of new pavement and hardscaping was a common response, as was keeping cost in mind.

Keep the parks dirt, grass, trees, blackberries, and shrubs; not concrete, structures, and wide paved areas.

Anything that separates cyclists and pedestrians would be a good thing. It's important to have a wide trail here.

Maximally preserve natural habitat and minimize human intrusiveness.



Survey Overview

The second Mercer Island Aubrey Davis Park Survey was designed to complement the second open house and, similar to the open house, solicit feedback on the preliminary design options presented for Aubrey Davis Park. The survey was open from April 23, 2019, to June 7, 2019. It is important to note that this is not a statistically valid survey.

The survey was presented as a graphic narrative, using the ESRI StoryMap platform with integrated design concepts and survey questions throughout the narrative. It was intended to be a more immersive survey, closer to the experience of attending an open house versus taking an online survey. The general structure showed the respondent a set of design concepts for a particular area of the park, then asked for feedback on those specific design concepts for that area. It was designed to take 15-20 minutes to complete, although that time varied due to the high amount of open-ended questions. See Appendix K: Community Outreach Events#2 for the full survey analysis report.

There were seven sections of the survey:

- | | |
|----------------------------------|--|
| 1. Corridor Improvements | 5. Park on the Lid – West Mercer Way to 72nd Avenue SE |
| 2. North and South Trail Options | 6. Park on the Lid – 72nd Avenue SE to 74th Avenue SE |
| 3. East End Option | 7. Park on the Lid – SE 24th Street Open Space |
| 4. Luther Burbank Lid Option | |

While the survey was implemented to avoid people taking the survey multiple times based on the web settings used, it is possible that an internet-savvy user could have taken the survey multiple times in order to emphasize their opinion in the planning process.

The Corridor Improvements and North and South Trail Options sections had the highest response rate, with the middle and end sections having a lower response rate overall. Additionally, the open-ended questions had far lower participation than questions in which the user gave a number rating or selected from a multiple-choice list.

Overall, the most common theme within the responses was to do nothing and that, outside of some safety improvements or better signage, the park functions well as it currently exists. Another theme was limiting the amount of new pavement and hardscaping.

The contingent of respondents speaking against any investment into the design options shown, and master planning process in general, was quite vocal and descriptive in the ways they felt this process was not a good use of money. Conversely, other users supported the long-term makeover of the park and favorably viewed the design options shown.

The range of opinions expressed in the survey varied greatly, and in instances where specific design options were provided, favorability was often split such that no clear option was preferred, except in the instances where “do nothing” or “neither” was an option.

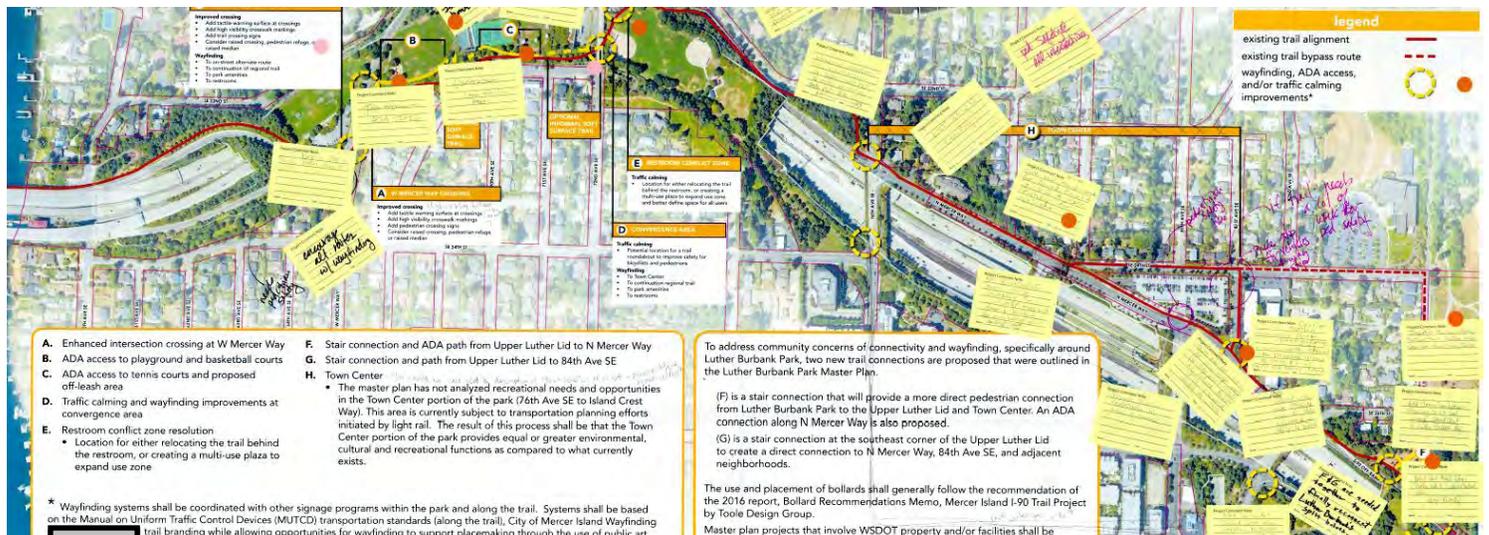
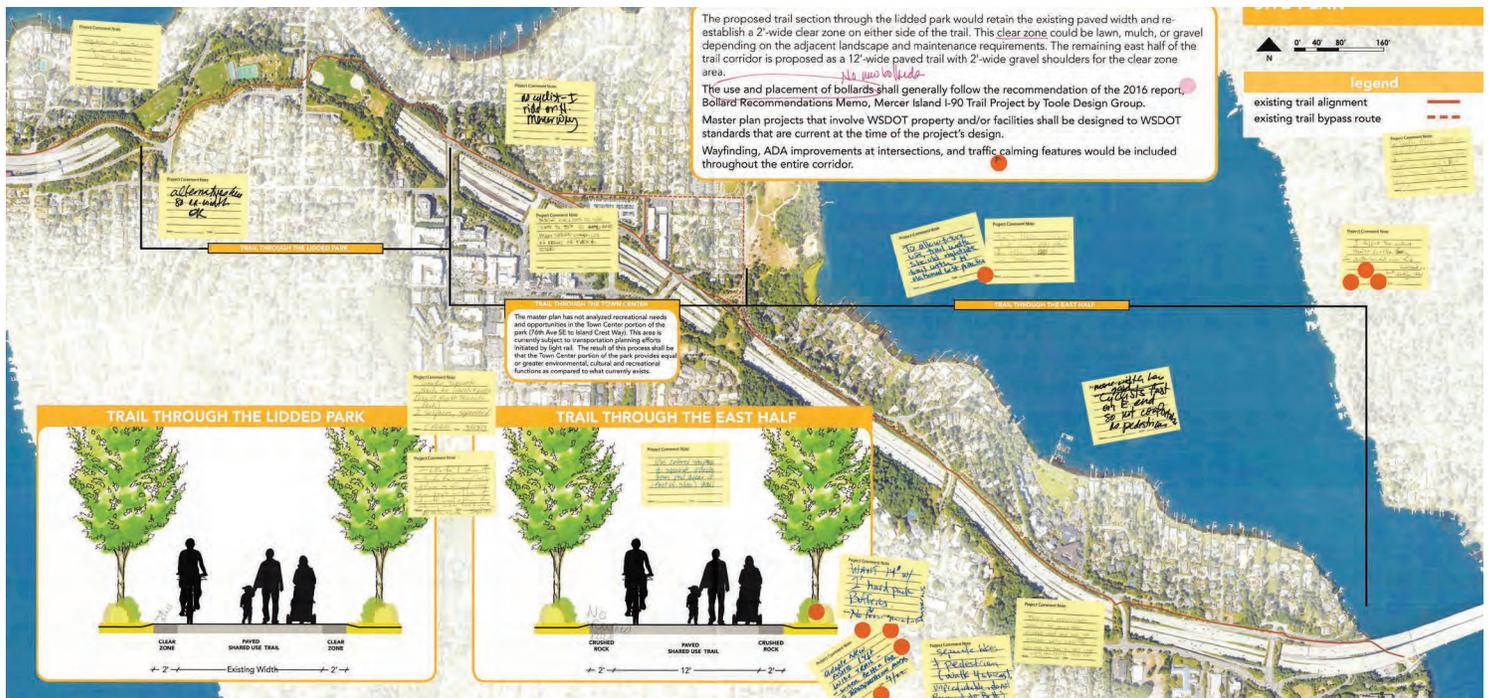
Sample open-ended responses

- Keep the playground where it is but make it more interesting and active. Places to hide and climb. Less pre-fab boring play structures.
- Saving money by keeping the tennis courts in their current spot, and adding an activity zone is a good balance. Adding many picnic tables in this area will not get as much use for the money.
- The tennis courts are very accessible now, but other activities should be added to the area.
- This area is fine as it is. There is plenty of parking and the picnic tables and open grass areas do not need to be changed.
- FISCALLY IRRESPONSIBLE. COURTS ARE PERFECT. PLEASE STOP ADDING PAVEMENT TO OUR PARKS. Increased accessibility to facilities will encourage more off island users.

51 participants attended the third in-person open house, held at the Mercer Island Community & Event Center on September 23rd, 2019. The third open house was focused on presenting conceptual recommendations in four main categories:

- Vegetation Management
- Trail Improvements
- Amenity Improvements
- Arts & Placemaking

Each of these categories had several smaller improvements proposed through the western portion of the park through Town Center and from Luther Burbank Lid to the east end of the park. While the proposed improvements shown were conceptual, input was sought on prioritization and on specific questions regarding vegetation management, the restroom conflict zone, and soft surface trails. Each open house participant received a comment card with three questions and three dots to indicate which proposed improvements they preferred.



Survey Overview

The third survey was designed to complement the third open house and capture feedback on the proposed improvements presented for Aubrey Davis Park. The survey was open from September 23-30, 2019. See Appendix K: Community Outreach Events for the full survey analysis report.

On the three questions asked:

- The Trail Behind the Restroom option was preferred for the **restroom** conflict zone by the existing restroom building;
- Soft Surface Trail option 2 with the pedestrian path along existing trail was preferred;
- The Low Maintenance Meadow option was preferred as the approach for water conservation.

Comment Cards

Responses Received: 41

Question 1: Ballfield Conflict Zone preferred approach

- Option 1 (Multimodal Plaza): 6 votes
- Option 2 (Trail behind restroom): 26 votes
- Neither: 5 votes

Question 2: Soft Surface Trail Alignment

- Option 1 (Pedestrian path to tennis courts): 17 votes
- Option 2 (Pedestrian Path along trail): 25 votes
- Neither: 6 votes

Question 3: Water Conservation preferred approach

- Option 1 (Brown out open areas): 13 votes
- Option 2 (Lower Maintenance Meadows): 21 votes
- Option 3 (Keep existing lawn): 10 votes

Sample Comments:

- Multi-modal plaza "with bike-calming"
- "Separate areas to help with congestion," and "safer to separate users. Walk vs. ride." for trail behind restroom
- "ADA soft surface" for path to tennis courts
- "Switchbacks encourage shortcuts" re: pedestrian path to tennis courts
- Meadow "but not near Island Crest Way"
- Meadow is "attractive with native vegetation and no on-going water needed"
- "Mix [of meadow and lawn] – keep areas green"
- Lawn "at Island Crest Way"



FINAL MASTER PLAN

Overview

The following chapter outlines recommendations for Aubrey Davis Park based on feedback from public outreach efforts. These recommendations are conceptual only, and additional planning, design, and public involvement is needed before any of the recommendations can be implemented. The final Master Plan, once adopted, will also serve as a platform to renegotiate the maintenance agreement with WSDOT.

The recommendations are divided into the following four categories:

- Vegetation Management
- Trail Improvements
- Park Improvements
- Arts & Placemaking

Master Plan projects that involve WSDOT property and/or facilities will be designed to WSDOT standards that are current at the time of the project's design. New or renovated facilities will also be designed using current best practices for Crime Prevention Through Environmental Design (CPTED).

The area through the Town Center was not evaluated as part of this Master Plan for trail improvements or park amenity improvements since this area is currently subject to transportation planning efforts initiated by light rail. The result of this separate process will be that the Town Center portion of the park provides equal or greater environmental, cultural, and recreational functions as compared to what currently exists.

Vegetation Management

The Master Plan identifies the different types of vegetation that exist along the corridor and recommends improvements to maintain the park’s natural character into the future. See Appendix B: Landscape Assessment for the full landscape assessment with recommendations. Given the diverse environment across the corridor, there are three general vegetation management strategies recommended for improving landscape and open space areas across the park. The areas proposed where these techniques will be applied are shown on Page 38.

Vegetation improvements conceptually described in this plan will need to be further developed and adapted to the specific conditions found in the different landscape areas shown. The performance of vegetation improvements will also be evaluated periodically and modified as needed to achieve the objectives of the plan. This approach recognizes that landscapes evolve over time and management techniques may need to be adapted due to changing conditions or new innovations available in the future.

The management strategies proposed include:

- The intensive soil amendment and replanting strategy is proposed for higher visibility areas where existing ivy has taken over much of the landscape and significant vegetation dieback is visible.
- The strategy to infill planting areas with new vegetation in existing soils is proposed for lower visibility areas where the existing vegetation is mostly intact. New vegetation would be selected based on the existing soil profile, microclimate, and lower water use.
- Non-active recreation lawns would be modified to include a reduced maintenance and water use strategy. There are two options proposed for these areas described below. None of these strategies would be applied to the actively used open lawn areas (formal or informal) within the Lid Park.

Pavement installed within the dripline of new or existing trees will be carefully evaluated for root barriers and/or suspended pavement systems to increase the compatibility and longevity of the landscape elements. Locations for radial trenching within tree driplines will also be further explored to increase the long-term health of existing trees.

Reduced Water Use Strategies

Water currently accounts for almost one third of the park’s total operating expenditures. In addition to the management strategies identified above, two alternatives are proposed below for reducing water usage and cost throughout the park over time. This implementation would only occur in lawn areas that aren’t typically used for formal or informal play (frisbee, ballgames, picnicking, etc.). Lawn areas along the trail corridor, adjacent to roadways, or in between vegetated planting areas are examples of where these techniques may be applied.



Current Practice: Keep watering existing lawn areas

- High cost, high water usage
- Higher maintenance
- Low habitat value



Option 2: Brown out open lawn areas (non-athletic) in summer

- Low cost, low water usage
- Low maintenance
- Low habitat value



Option 3: Install lower maintenance meadows

- Medium cost, some initial water usage
- Higher maintenance
- High habitat value

Both alternatives will be implemented on a limited basis as pilot projects, locations to be determined by the City. Smaller areas (approximately a ¼ acre) that are accessible and conducive to public engagement will be selected, while high usage areas, such as places where people run their dogs, would likely not be impacted. To identify these pilot projects to the public, signage informing the intent and potential cost savings of each treatment will be displayed. The public will be able to comment on these alternatives and inform whether these strategies will be more widely deployed. Replacing soils and revising the planting palette for more drought tolerant species is also part of the program and will be used as a priority treatment where applicable.

Landscape Character

The landscape character of new plantings should reflect the existing natural context. Three palettes were shown for public input: Northwest Feel, Ornamental, and Sensory. In all scenarios, participants expressed the need for appropriate maintenance and budgetary considerations to be considered.

The Northwest Feel character was selected as the primary feel for the park, with Ornamental and Sensory palettes included for specific purposes such as accent plantings and art/placemaking opportunities. These would likely be part of an individual project that would be subject to public input, and feedback on the landscaping would be solicited prior to implementation.

NORTHWEST FEEL



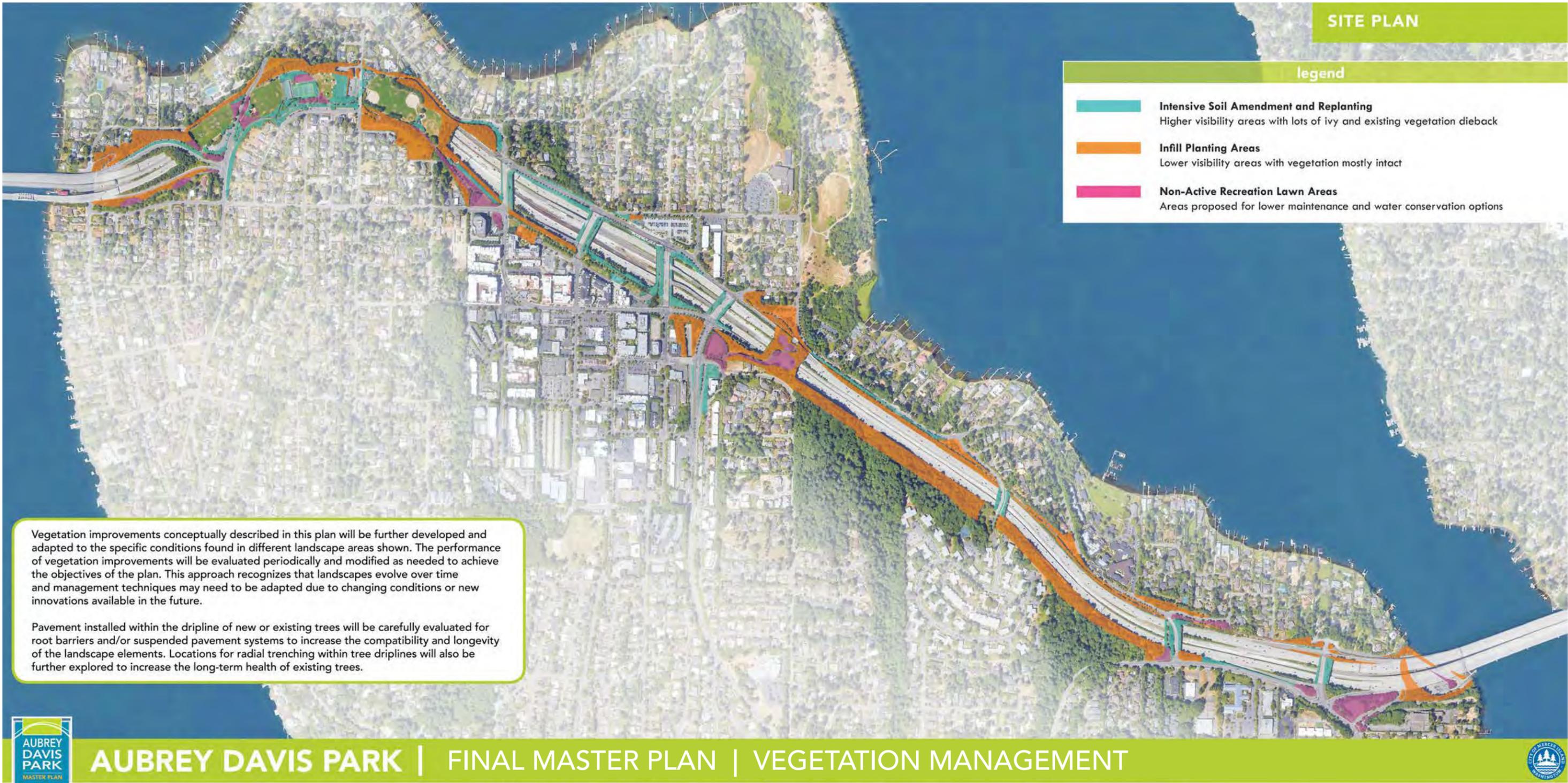
ORNAMENTAL



SENSORY



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Trail Improvements

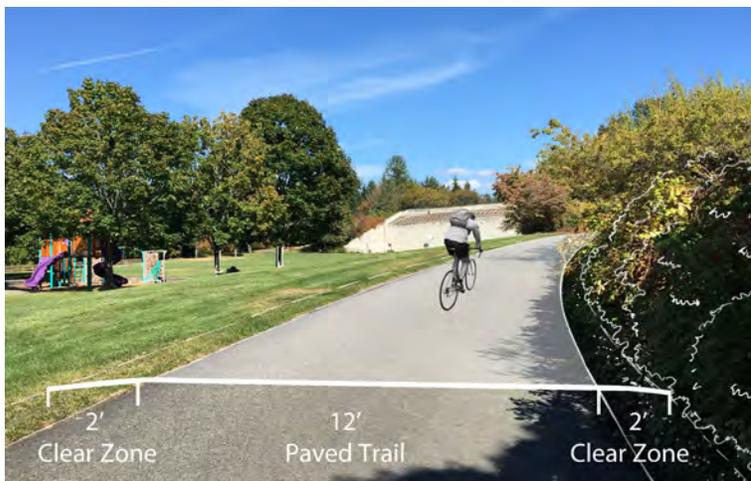
The trail through Aubrey Davis Park is part of the Mountains to Sound Trail and serves as an important multi-modal facility for a wide variety of users. Master Plan recommendations for the trail are primarily limited to re-establishing sight-lines and clear zones along the existing trail, renovating the trail to the east of the Town Center in conjunction with the King County Sewer Upgrade project, improving safety for all trail users, and integrating wayfinding into the corridor. New trails are only proposed where existing amenities do not have ADA access.

Along the East Corridor, from Island Crest Way to the East Channel bridge abutment, the trail is being impacted by the King County Sewer Upgrade project and will be reconstructed through most of this area. Public opinion is split on trail width of the Mountain to Sound trail. Many comments in Open House #3 expressed support for a 14 foot trail width in keeping with new American Association of State Highway and Transportation Office (AASHTO) standards. This balances input received at previous open houses for keeping the trail at its current width. The trail east of Island Crest Way would be widened by 1 foot to a 12 foot width as proposed in the King County Sewer project to incorporate WSDOT's recommended width. Although the intent is to maintain a 12' + 2' standard trail width in this area, there are many sections of the trail where this can't be built due to limited right-of-way width.

The trail through the Lid Park is currently 12-feet wide as confirmed by a Parks survey and would remain this width going forward. In both cross sections, the plan calls for two foot clear zones or shoulders to be constructed or reestablished. These shoulders will be allowed to grow grass, moss, or groundcovers to create a transitional edge on the sides of the trail that is walkable, **does not impact necessary sightlines**, and is free of overhanging vegetation. Beyond the King County Sewer project, no additional reconstruction of the trail is anticipated in the foreseeable future. It is unlikely that these specifications will be applicable to a specific project.

As this trail is a WSDOT facility, if it is ever rebuilt, it will need to adhere to WSDOT standards at the time it is designed. WSDOT retains final approval authority over any trail reconstruction.

The trail cross sections below only apply to the main Mountains to Sound Trail on Mercer Island (shown as a red line on the plan graphics). Secondary trails will remain their existing widths.



TRAIL SECTION THROUGH THE LID PARK



TRAIL SECTION THROUGH THE EAST CORRIDOR

One of the biggest conflict zones between park and trail users is the area immediately adjacent to Feroglia Fields in the eastern Lid Park, near the existing restroom. Two design solutions were explored and presented in Open Houses #2 and #3: one creating a multi-modal plaza for traffic calming and one relocating the regional trail around the back side of the restroom to allow a pedestrian-only trail between the restroom and the field.

In both the online survey and comments received from Open House #3, the option of a new trail to channel cyclists behind the restroom was the overwhelming favorite, with 41 votes, over the multi-modal plaza (14 votes). However, the new trail behind the restroom would displace the Parks maintenance facility there, and the feasibility, cost, and operational impact of relocating that facility have not been evaluated. As loss of this facility may greatly impact operational efficiency, further analysis of both options will be explored in the early design phase of the project.



Option 1: Multi-modal plaza

- Addition of plaza for traffic calming – a ‘mixing zone’ that slows down all users and creates a greater sense of place
- Separate pedestrian path as you enter and leave the ‘mixing zone’
- Bicycle and pedestrian circle at corner for traffic calming and to reduce direct conflicts



Option 2: Trail behind restroom

- Routes trail on east side of structural columns for bridge and through existing city maintenance area
- Relocates city maintenance near WSDOT maintenance area to the west or to another location entirely
- Keeps existing parking in place
- Bicycle and pedestrian circle at corner for traffic calming and to reduce direct conflicts

There are also two places within the Lid Park where ADA access isn't provided between park amenities. One of these locations is from the basketball courts up to the tennis courts, and another a segment of the trail between 71st Avenue SE and the underpass of 72nd Avenue SE.

Two soft-surface, pedestrian-only trails that will be ADA accessible were proposed at Open House #3 (shown in yellow in the images below). These trails would provide an alternate walking route in high-speed sections of the trail, and would increase separation of walkers and cyclists. Public comments indicated a stronger preference for the pedestrian path along the trail, with 39 votes (46%). The pedestrian path to the tennis courts was also supported, with 29 votes (35%). 16 participants chose neither as their preference (19%). Given the responses, both options will be explored further in the early design phase for costs and feasibility.



Pedestrian path to the tennis courts



Pedestrian path along the trail

ADA Accessibility

The Master Plan proposes several new trails that would comply with the Americans with Disabilities Act (ADA) through the Architectural Barriers Act (ABA) standards that have been adopted for outdoor recreation facilities. Neither the City nor WSDOT is required to bring existing facilities up to these standards as they currently exist. Normal maintenance and minor repairs do not prompt ADA compliance. However, work that generates City building permits or WSDOT review will necessitate ADA review. At that time, the extent of the work will determine the requirement for ADA accommodations. For example, a project that keeps the facility for the same use at the same location will likely result in ADA improvements limited to that facility only (e.g. changing restroom fixtures with any upgrades to the existing restroom structure or installing an ADA ramp with ADA accessible playground elements when the playground equipment needs to be replaced). A project that changes the use of the facility or results in a substantially new facility could trigger a more comprehensive set of ADA accommodations, like including accessible routes from designated ADA parking stalls with a redesign of the tennis court or basketball court areas. These types of improvements have been included in the Master Plan, but the Master Plan does not constitute a comprehensive ADA analysis of accessibility needs in the park.

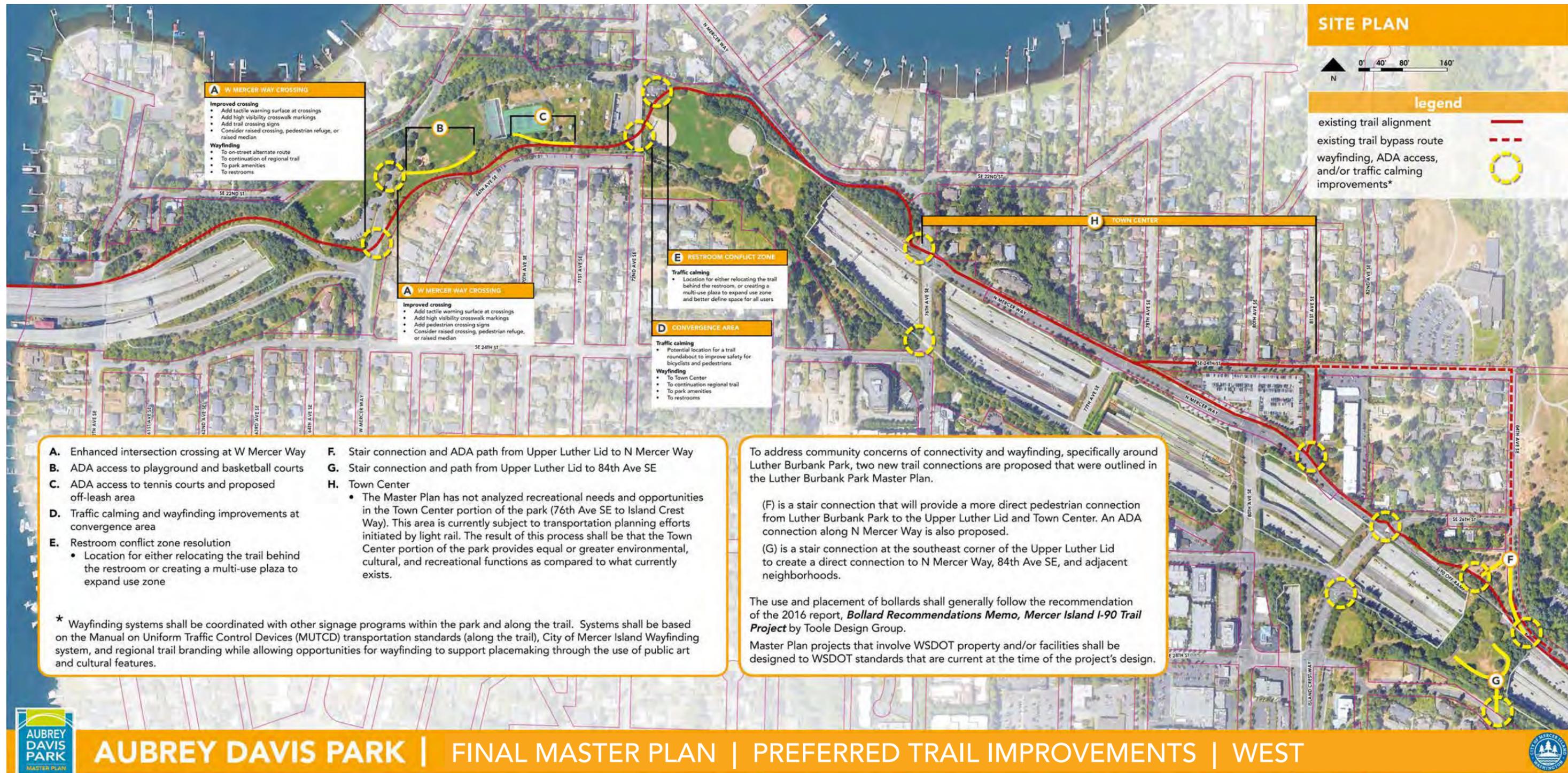
Trail Lighting

The section of the Mountains to Sound Trail between the Luther Lid and Shorewood Drive is shaded by high retaining walls to the south. It is dark in the winter. Pedestrians use this trail as the most direct route between Shorewood and Town Center. The City of Mercer Island has received requests for lighting that section of trail to improve visibility for both pedestrians and cyclists. While there have been no design concepts discussed for this improvement, the most likely solution would be overhead lights on poles. Bollard lighting is an additional option for wayfinding in the dark but not provide adequate illumination for trail users.

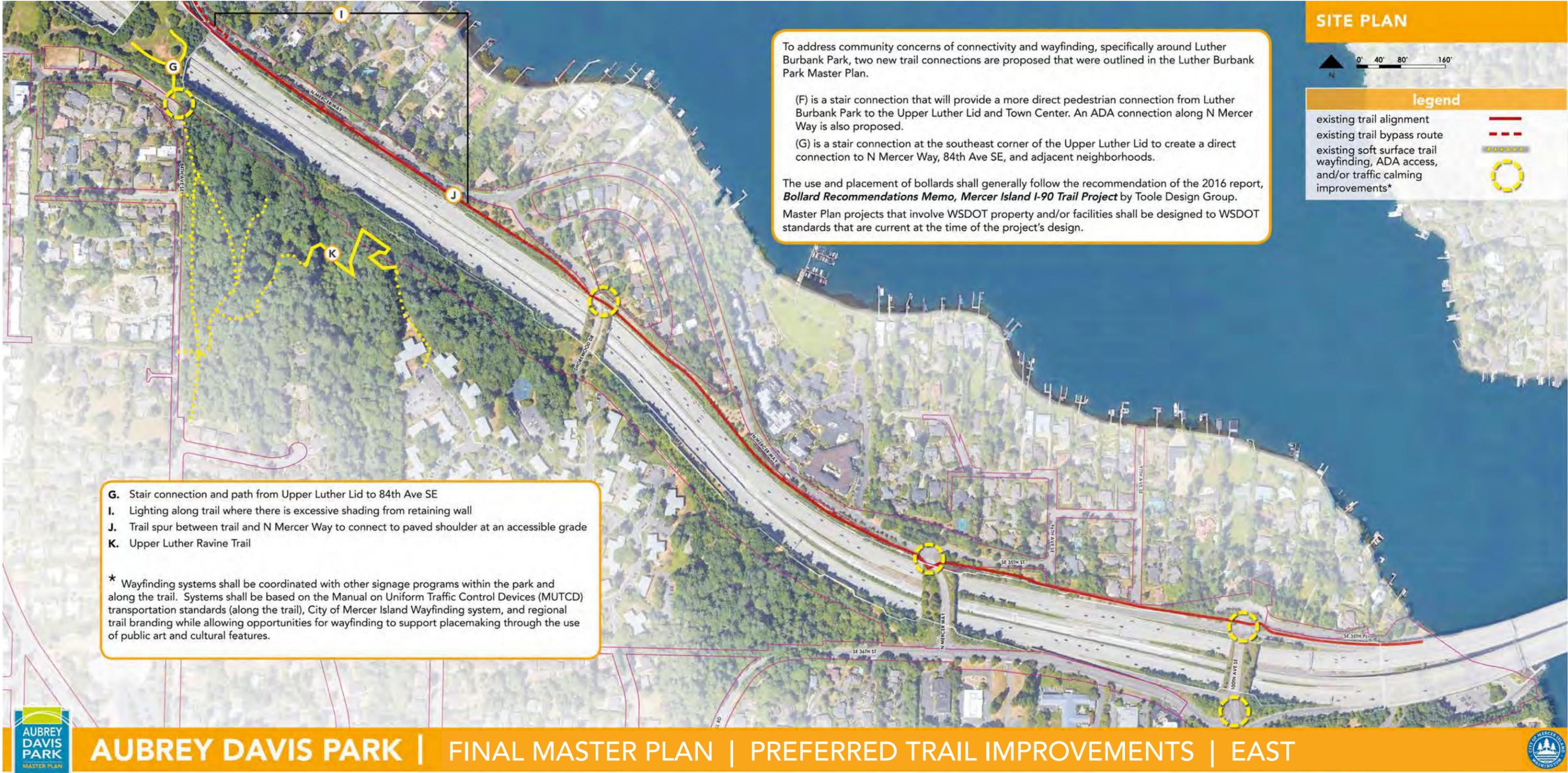
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Park Improvements

Amenities for the park are primarily focused on user safety and ADA access to existing park elements. An off-leash area and a second restroom are the only new uses proposed. The only other improvement to existing uses is proposed at the east end of the corridor to improve shoreline access to Lake Washington.



New Restroom

Two locations are proposed for a new restroom between the soccer fields and the playground along West Mercer Way. Additional analysis will be needed to determine feasibility and cost associated with any restroom in either location. The graphic to the left shows the approximate areas of the two alternative locations considered for the new restroom buildings in orange.



Dog Off-Leash Area

An off-leash area is proposed in the existing open space around the stacks, east of the tennis courts. Currently this area is an informal lawn and picnic area. Integrating an off-leash area or other new programming use would help activate the space, bringing in people to help make this area feel safer with “eyes on the park” and other crime prevention through environmental design (CPTED) principles.

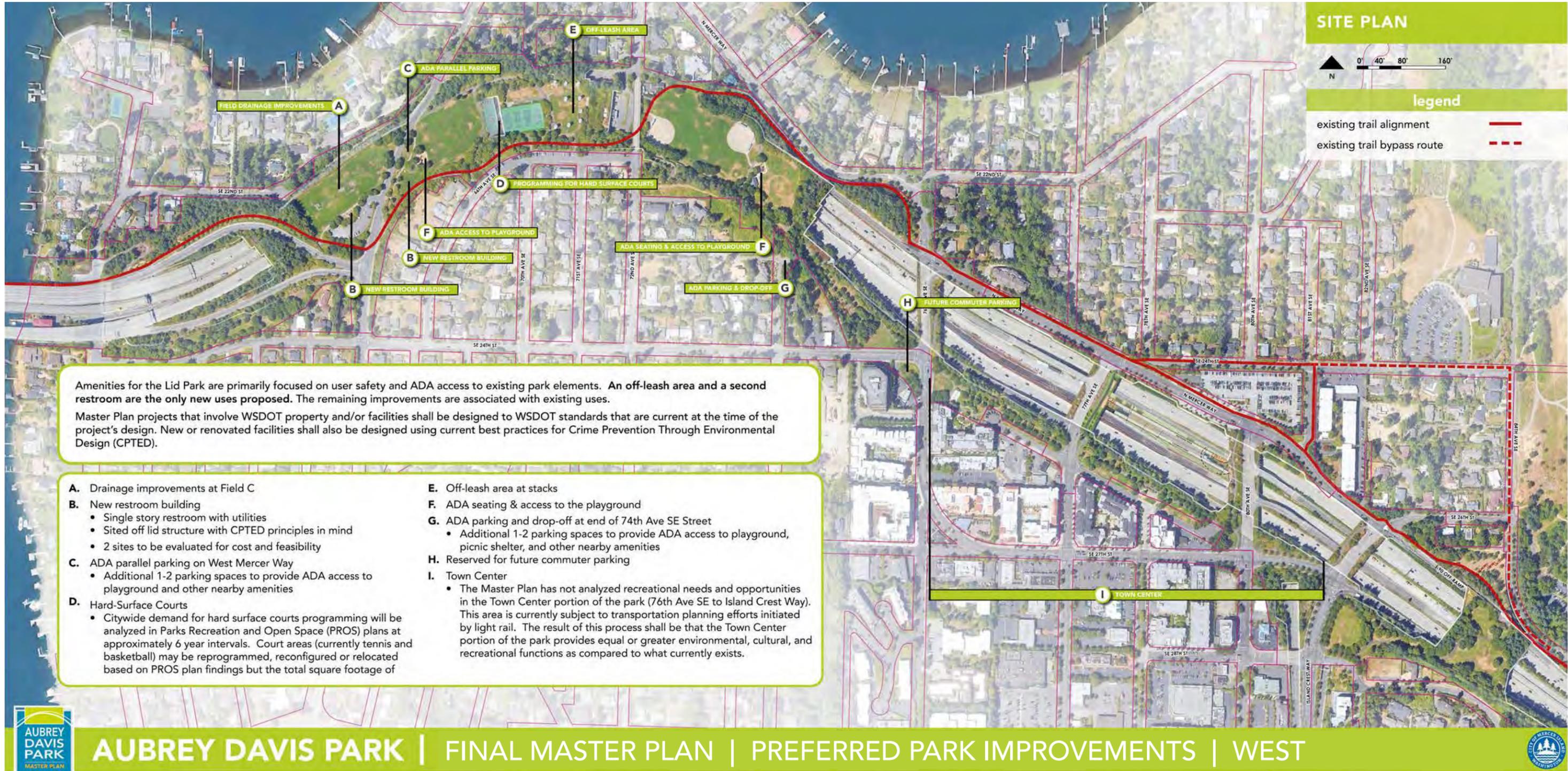
There were several comments critical of an off-leash area here. Some dog owners have previously expressed a desire to have a fenced area for their dogs. As off-leash dogs run at “The Stacks” regularly, this activity in the vicinity of the trail presents a conflict that needs to be addressed. Separation along the trail would help restrict dogs from wandering onto the trail. As such, a dog off-leash area will be further explored in the early design phase with extensive public input.



Shoreline Access Area

An improved shoreline access area on the east end is proposed near the boat launch. This will include shoreline restoration, water access, and potential seating improvements. All existing shoreline uses will remain. There has not been much community feedback for this area, with most comments related to improving pedestrian connections.

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Arts, Culture, & Placemaking

Since its creation, Aubrey Davis Park has been a primary location for community members to engage with arts and culture. Learn more about this history in Appendix I: Arts & Culture Vision Document.

Building on this history, the Mercer Island Arts Council has identified an updated vision for using arts and culture to deepen community connections to the park and further enhance it as a place that community members want to spend their time. This process included its own public outreach process through the summer of 2019. Through site visits, community input, and board working sessions, the Arts Council drafted a vision and core tenets to guide how art could create and maintain a sense of place in Aubrey Davis Park.

Vision:

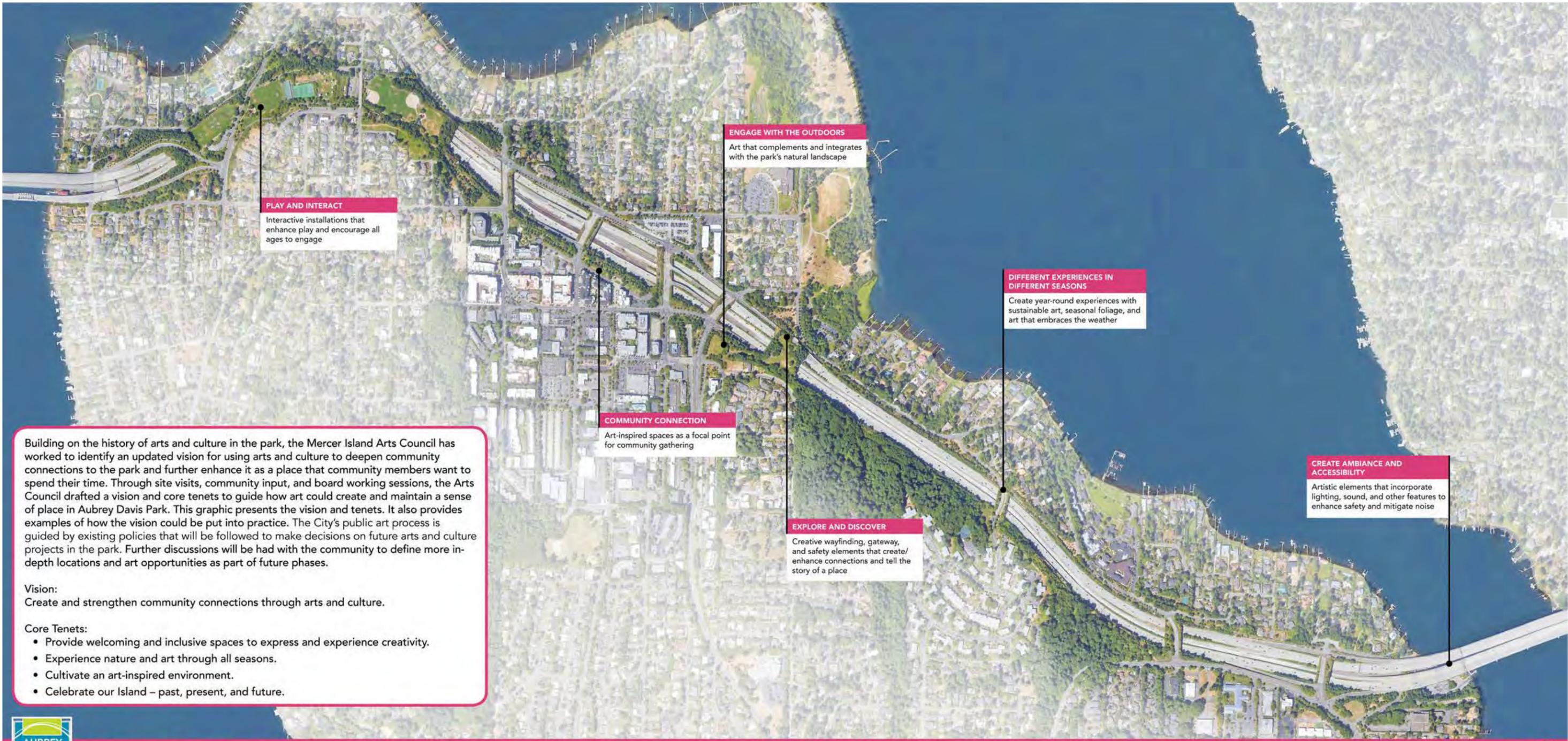
Create and strengthen community connections through arts and culture.

Core Tenets:

- Provide welcoming and inclusive spaces to express and experience creativity.
- Experience nature and art through all seasons.
- Cultivate an art-inspired environment.
- Celebrate our Island – past, present, and future.

The graphic on page 49 provides examples of how the vision can be put into practice. The City's public art process is guided by existing policies that will be followed to make decisions on future arts and culture projects in the park. Further discussions will be had with the community to define more in-depth locations and art opportunities as part of future phases. Refer to Appendices I and J for additional suggestions related to arts, culture, and placemaking.

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Building on the history of arts and culture in the park, the Mercer Island Arts Council has worked to identify an updated vision for using arts and culture to deepen community connections to the park and further enhance it as a place that community members want to spend their time. Through site visits, community input, and board working sessions, the Arts Council drafted a vision and core tenets to guide how art could create and maintain a sense of place in Aubrey Davis Park. This graphic presents the vision and tenets. It also provides examples of how the vision could be put into practice. The City's public art process is guided by existing policies that will be followed to make decisions on future arts and culture projects in the park. Further discussions will be had with the community to define more in-depth locations and art opportunities as part of future phases.

Vision:
Create and strengthen community connections through arts and culture.

Core Tenets:

- Provide welcoming and inclusive spaces to express and experience creativity.
- Experience nature and art through all seasons.
- Cultivate an art-inspired environment.
- Celebrate our Island – past, present, and future.



AUBREY DAVIS PARK | FINAL MASTER PLAN | PLACEMAKING OPPORTUNITIES



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PROJECT IMPLEMENTATION



Prioritization

Criteria for prioritization of the projects included in this Master Plan mirrors the criteria used in the City's Capital Improvement Program (CIP). The prioritization is as follows:

1. Highest Priority – Projects that are critical to be done right away in order to maintain existing infrastructure and projects that address urgent safety issues. Postponing this project would require a higher level of effort and would be at an overall greater cost to the City in future years.
2. Second Level of Priority – If funding is available, it is recommended that this project be done in the biennium. However, it is not critical and could be postponed to a later year if need be.
3. Third Level of Priority – Projects for which there is strong support from the City Council or a group of citizens. However, the project is not critical to the maintenance of the City's infrastructure.

Priorities for the projects included in this plan are listed below:

(1) Highest Priority

Vegetation Management

Trail Improvements

- West Mercer Way Crossing
- Intersection and Crossing Improvements
- Restroom Conflict Zone

(2) Second Level of Priority

Trail Improvements

- ADA Access Improvements to Playground and Basketball Courts
- ADA Access Improvements to Tennis Courts
- Lighting Along Trail
- Connection to Trail

Park Improvements

- Field Drainage Improvements
- ADA Parking (2 spots) & Drop-Off (West Mercer Way)
- ADA Access to Playground (West Mercer Way)
- ADA Access to Playground (Lid Park East End)
- ADA Parking (2 spots) & Drop-Off (74th Ave)

(3) Third Level of Priority

Trail Improvements

- Lower Luther Lid - Connection to North Mercer Way
- Upper Luther Lid - Access to 84th Ave SE
- ~~• Soft Surface Trail - Optional
(Crushed Rock, 6' wide)~~

Park Improvements

- New Restroom Building
- Dog Off-Leash Area
- Improved Shoreline Access

As individual projects included in this plan start moving forward, the following approach will be used to determine the level of project engagement:

1. Projects that generally keep the existing character and function of the park do not need additional public input, and decisions would be made at the director level once Council has approved the budget for them (e.g. landscape renovation, field drainage projects).
2. Projects that maintain or enhance existing functions (e.g. intersection improvements, improved shoreline access, water conservation) would have one round of outreach and input in early design with updates posted on social media and the City's public engagement platform. Decisions would be made at the city manager level after Council approves the budget for them.
3. Projects that involve extensive design or are new facilities (e.g. dog off-leash area, restroom conflict zone trail reconfiguration) would provide the public multiple chances for input with a public engagement webpage and full social media coverage. The City Manager would advise Council of project progress and ask for input at critical stages.

Cost Estimate and Funding

A rough order of magnitude cost estimate was generated for the Aubrey Davis Park Master Plan to get an understanding of high-level costs associated with proposed improvements. This is a one-time snapshot of rough costs, and more detailed cost estimates need to be developed for each individual project during design phase. Including a project in this Master Plan does not imply that the City will fund that project or give it special priority. Capital projects in the Master Plan will be considered for funding as part of the biennial budget process. The exception to this would be opportunities for external funding that would be dedicated specifically to the park or a specific project in the Master Plan. Donations or grants could be received outside of the biennial budget process with City Council approval.

Estimate of Probable Cost of Construction

HBB Landscape Architecture

Date: November 8, 2019

Project Name: Aubrey Davis Park
 Project Number: 2018-26
 Project Phase: Master Plan
 Prepared By: G. Kim / M. Oviir
 Checked By: J. Vong

| | Mobilization & Overhead (10%) | Contingency (30%) | Sales Tax (10%) | Construction Total | Soft Costs (25%) | Total Project Cost |
|---------------------------------------|-------------------------------|-------------------|-----------------|---------------------|------------------|------------------------|
| Vegetation Management - Low Priority | \$2,112,825 | \$6,338,475 | \$2,112,825 | \$31,692,376 | \$7,923,094 | \$39,615,470.25 |
| Vegetation Management - High Priority | \$584,103 | \$1,752,310 | \$584,103 | \$8,761,552 | \$2,190,388 | \$10,951,939.50 |
| Trail Improvements | \$341,100 | \$1,023,300 | \$341,100 | \$5,116,500 | \$1,279,125 | \$6,395,625.00 |
| Park Improvements | \$312,700 | \$938,100 | \$312,700 | \$4,690,500 | \$1,172,625 | \$5,863,125.00 |

Estimate of
 Probable Cost of Construction
 HBB Landscape Architecture

Date: November 8, 2019

Project Name: Aubrey Davis Park
 Project Number: 2018-26
 Project Phase: Master Plan
 Prepared By: G. Kim / M. Oviir
 Checked By: J. Vong

Vegetation Management Total: \$50,567,366.63

Vegetation Management

| Item | Priority | Description | Qty | Unit | Unit Cost | Item Total |
|------|----------|---|-----|------|--------------|------------------------|
| 1.00 | | Intensive Soil Amendment and Planting | | | | |
| | | Remove and Replace Existing Soils | 13 | AC | \$911,711.00 | \$11,852,243.00 |
| | 1 | Higher Priority (20% of area) | 3 | AC | \$911,712.00 | \$2,735,136.00 |
| | 2 | Lower Priority (80% of area) | 10 | AC | \$911,713.00 | \$9,117,130.00 |
| 2.00 | | Infill Planting | 39 | AC | \$380,158.80 | \$14,826,193.20 |
| | 1 | Higher Priority (20% of area) | 8 | AC | \$380,158.80 | \$3,041,270.40 |
| | 2 | Lower Priority (80% of area) | 31 | AC | \$380,158.80 | \$11,784,922.80 |
| 3.00 | | Non-Active Recreation Lawn Areas | | | | |
| | | Lawn to Eco-Lawn | 9 | AC | \$32,314.00 | \$290,826.00 |
| | 1 | Higher Priority (20% of area) | 2 | AC | \$32,314.00 | \$64,628.00 |
| | 2 | Lower Priority (80% of area) | 7 | AC | \$32,314.00 | \$226,198.00 |
| | | <i>Subtotal</i> | | | | \$26,969,262.20 |
| | | <i>Contractor Mobilization & Overhead (10%)</i> | | | | \$2,696,926.22 |
| | | <i>Contingency (30%)</i> | | | | \$8,090,778.66 |
| | | <i>Sales Tax (10%)</i> | | | | \$2,696,926.22 |
| | | Construction Total | | | | \$40,453,893.30 |
| | | Soft Costs (25%) | | | | \$10,113,473.33 |
| | | Total Project Cost | | | | \$50,567,366.63 |

Estimate of Probable Cost of Construction

HBB Landscape Architecture

Date: November 8, 2019

Project Name: Aubrey Davis Park
 Project Number: 2018-26
 Project Phase: Master Plan
 Prepared By: G. Kim / M. Oviir
 Checked By: J. Vong

Trail Improvements Total: \$6,395,625.00

Trail Improvements

| Item | Priority | Description | Qty | Unit | Unit Cost | Item Total |
|------|----------|--|-------|------|------------|-----------------------|
| A | 1 | West Mercer Way Crossing | | | | \$ 50,000 |
| | | Improvements to Existing Crossing | | | | Not Included |
| | | New Pedestrian Crossing | 1 | LS | \$ 50,000 | \$ 50,000 |
| | | ADA Access Improvements - Paved path (Concrete, 6' wide) | | | | \$ 56,000 |
| B | 2 | To Playground and Basketball Courts | 400 | LF | \$ 80 | \$ 32,000 |
| C | 2 | To Tennis Courts | 300 | LF | \$ 80 | \$ 24,000 |
| D | 1 | Intersection and Crossing Improvements | 15 | EA | \$ 75,000 | \$ 1,125,000 |
| E | 1 | Restroom Conflict Zone | | | | |
| | | Option 1: Trail Behind Restroom | | | | \$ 500,000 |
| | | Demolition & site preparation | 1 | LS | \$ 50,000 | \$ 50,000 |
| | | Grading and Fill (including potential wall) | 1 | LS | \$ 250,000 | \$ 250,000 |
| | | Paved Trail | 800 | LF | \$ 250 | \$ 200,000 |
| | | Maintenance Area Relocation | | | | Not Included |
| | | Option 2: Mixing Zone | | | | \$ 375,000 |
| | | Demolition & site preparation | 1 | LS | \$ 25,000 | \$ 25,000 |
| | | Grading and Fill | 1 | LS | \$ 50,000 | \$ 50,000 |
| | | Concrete Paving / Plaza | 5,000 | SF | \$ 60 | \$ 300,000 |
| F | 3 | Lower Luther Lid - Connection to North Mercer Way (I-90 Lid Connector) | | | | \$ 452,500 |
| | | Demolition & site preparation | 1 | LS | \$ 20,000 | \$ 20,000 |
| | | Grading and Fill (including low walls) | 1 | LS | \$ 120,000 | \$ 120,000 |
| | | Concrete Stairs, Railings and Paving | 1,250 | SF | \$ 250 | \$ 312,500 |
| G | 3 | Upper Luther Lid - Access to 84th Ave SE | | | | \$ 648,500 |
| | | Demolition & site preparation | 1 | LS | \$ 50,000 | \$ 50,000 |
| | | Grading and Fill (including walls) | 1 | LS | \$ 250,000 | \$ 250,000 |
| | | Paved Path | 450 | LF | \$ 80 | \$ 36,000 |
| | | Stair Connection | 1,250 | SF | \$ 250 | \$ 312,500 |
| H | | Town Center Improvements | | | | Not Included |
| I | 2 | Lighting Along Trail | | | | \$ 184,000 |
| | | Existing service connection | | | | Not Included |
| | | Pedestrian Light (60-feet on center) | 23 | EA | \$ 8,000 | \$ 184,000 |
| J | 2 | Connection to Trail (Paved Shoulder to Trail at N. Mercer Way) | 1 | LS | \$ 20,000 | \$ 20,000 |
| K | | Upper Luther Ravine Trail | | | | Not Included |
| | | <i>Subtotal</i> | | | | \$3,411,000.00 |
| | | <i>Contractor Mobilization & Overhead (10%)</i> | | | | \$341,100.00 |
| | | <i>Contingency (30%)</i> | | | | \$1,023,300.00 |
| | | <i>Sales Tax (10%)</i> | | | | \$341,100.00 |
| | | Construction Total | | | | \$5,116,500.00 |
| | | Soft Costs (25%) | | | | \$1,279,125.00 |
| | | Total Project Cost | | | | \$6,395,625.00 |

Estimate of Probable Cost of Construction

HBB Landscape Architecture

Date: November 8, 2019

Project Name: Aubrey Davis Park
 Project Number: 2018-26
 Project Phase: Master Plan
 Prepared By: G. Kim / M. Oviir
 Checked By: J. Vong

Park Improvements Total: **\$5,863,125.00**

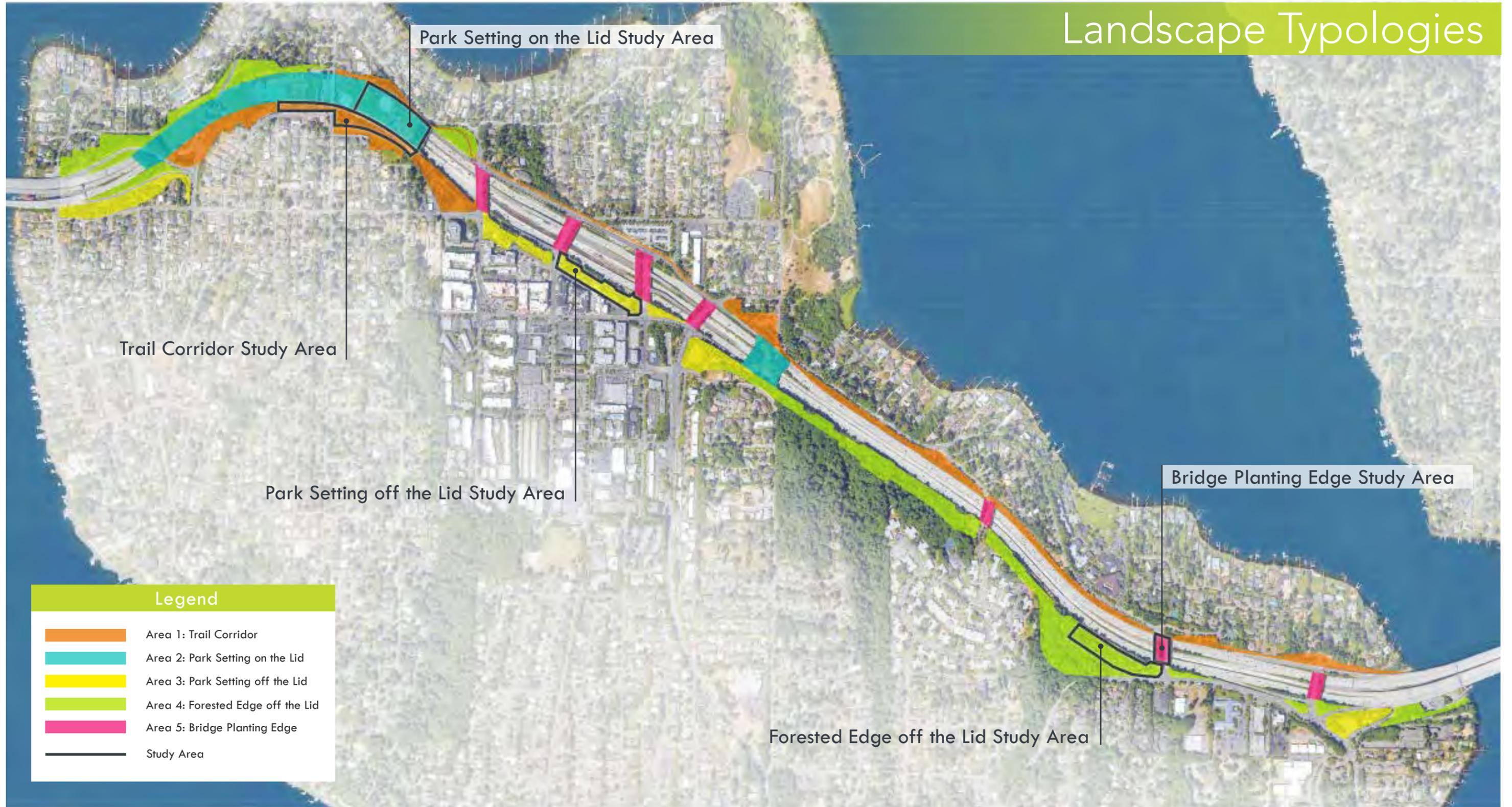
Park Improvements

| Item | Priority | Description | Qty | Unit | Unit Cost | | Item Total |
|------|----------|--|--------|------|------------|---|-----------------------|
| A | 2 | Field Drainage Improvements, Lid C | | | | \$ | 526,000 |
| | | Site preparation & Excavation | 1 | LS | \$ 45,000 | \$ | 45,000 |
| | | Drainage, Grading and Fill | 1 | LS | \$ 65,000 | \$ | 65,000 |
| | | Turf Restoration | 52,000 | SF | \$ 5.00 | \$ | 260,000 |
| | | Irrigation Restoration | 52,000 | SF | \$ 3.00 | \$ | 156,000 |
| B | 5 | New Restroom Building | | | | | \$760,000 |
| | | Demolition & Site preparation | 1 | LS | \$ 20,000 | \$ | 20,000 |
| | | Grading and Fill | 1 | LS | \$ 30,000 | \$ | 30,000 |
| | | Restroom Building | 1 | EA | \$ 400,000 | \$ | 400,000 |
| | | Utility Service Connections | 1 | LS | \$ 300,000 | \$ | 300,000 |
| | | Concrete Paving / Plaza | 500 | SF | \$ 20 | \$ | 10,000 |
| | | Structural Studies & Analysis | | | | | Not Included |
| C | 2 | ADA Parking (2 spots) & Drop-Off (West Mercer Way) | | | | \$ | 600,000 |
| | | Demolition & site preparation | 1 | LS | \$ 75,000 | \$ | 75,000 |
| | | Grading and Fill (including low walls) | 1 | LS | \$ 150,000 | \$ | 150,000 |
| | | Paving & Striping | 1,500 | SF | \$ 250 | \$ | 375,000 |
| D | | Programming For Hard Surface Courts | | | | | Not Included |
| E | 3 | Dog Off Leash Area | 1 | AC | \$ 350,000 | \$ | 350,000 |
| | | Includes fencing and gates, surfacing, furnishings, pick-up station, receptacles | | | | | |
| F | 2 | ADA Access to Playground (West Mercer Way) | | | | \$ | 38,000 |
| | | Demolition & Site preparation | 1 | LS | \$ 10,000 | \$ | 10,000 |
| | | ADA seating | 2 | EA | \$ 2,500 | \$ | 5,000 |
| | | Grading and Fill (including new playground curb edge) | 1 | LS | \$ 15,000 | \$ | 15,000 |
| | | Paved Path | 100 | LF | \$ 80 | \$ | 8,000 |
| F | 2 | ADA Access to Playground (Lid Park East End) | | | | \$ | 38,000 |
| | | Demolition & Site preparation | 1 | LS | \$ 10,000 | \$ | 10,000 |
| | | Grading and Fill (including new playground curb edge) | 1 | LS | \$ 15,000 | \$ | 15,000 |
| | | ADA seating | 2 | EA | \$ 2,500 | \$ | 5,000 |
| | | Paved Path | 100 | LF | \$ 80 | \$ | 8,000 |
| G | 2 | ADA Parking (2 spots) & Drop-Off (74th Ave) | | | | \$ | 115,000 |
| | | Demolition & Site preparation | 1 | LS | \$ 10,000 | \$ | 10,000 |
| | | Grading and Fill | 1 | LS | \$ 15,000 | \$ | 15,000 |
| | | Paving & Striping | 1500 | SF | \$ 60 | \$ | 90,000 |
| H | | Future Commuter Parking | | | | | Not Included |
| I | | Town Center Improvements | | | | | Not Included |
| J | | Existing Marine Patrol Facility | | | | | Not Included |
| K | 3 | Improved Shoreline Access | 1 | LS | \$ 700,000 | \$ | 700,000 |
| | | | | | | <i>Subtotal</i> | \$3,127,000.00 |
| | | | | | | <i>Contractor Mobilization & Overhead (10%)</i> | \$312,700.00 |
| | | | | | | <i>Contingency (30%)</i> | \$938,100.00 |
| | | | | | | <i>Sales Tax (10%)</i> | \$312,700.00 |
| | | | | | | Construction Total | \$4,690,500.00 |
| | | | | | | Soft Costs (25%) | \$1,172,625.00 |
| | | | | | | Total Project Cost | \$5,863,125.00 |

APPENDIX A

LANDSCAPE TYPOLOGIES MAP

Landscape Typologies



APPENDIX B

LANDSCAPE ASSESSMENT

Landscape Assessment

The purpose of the Landscape Assessment is to document the existing conditions of planting and irrigation within the park, analyze plant health, identify visibility and/or safety concerns due to vegetation, and document current landscape maintenance practices. The Landscape Assessment is divided into five typologies - see Appendix A: Landscape Typologies Map for the map. Representative samples of each of the five typologies were reviewed in detail. It is assumed that recommendations for each typology would be applicable across the entire Aubrey Davis Park corridor. In some cases, smaller study areas were identified within each typology. The smaller study areas were evaluated in greater detail with the assessment results and recommendations described below.

The Landscape Assessment includes recommendations for increasing park safety and visibility, as well as improving on-site soil and plant health, plant selection, and park maintenance, including water use reduction. Recommendations are based upon site observations, interviews with City of Mercer Island maintenance staff, soil test results, and review of the Washington State Department of Transportation (WSDOT) as-built documents.

See **Appendix A: Landscape Typologies Map** for the Landscape Typologies map.

See **Appendix C: Arborist Notes** for arborist notes, observations and recommendations.

See **Appendix E: Soil Test Results & Locations** for soil test results and a map with the sample locations.

General Overview

Below are some general observations for Aubrey Davis Park as a whole.

Soils

Soil samples were collected in eight locations within Aubrey Davis Park, corresponding to the five landscape typologies studied as part of the landscape assessment task. These typologies include a park setting on the lid, park setting off the lid, bridge planting edge, forested edge off the lid, and along the trail corridor (east end).

Enclosed in Appendix C: Arborist Notes are:

- An annotated aerial for locations of each sample;
- The individual soil test reports received.

Existing on-site soils display a very low organic content with an average 90% sand, 8% silt and 2% clay soil profile. Due to existing soils being a homogenous uniform sandy material, consolidated organic soil clods and soil veins are missing from the soil profile. This is not unexpected considering the original soil specification for the park was a two-way soil mix consisting of 70% to 80% sand and 20% to 30% ground bark. The ground bark organic matter in the original soil mix has decomposed over time leaving a very high sand content. The soil sample results reveal the remaining sand is poorly graded with up to 80% of sand particles in the medium to fine range. When sand soils do not have a mixture of particle sizes, the soil is more likely to compact without the particle voids needed to hold oxygen and water within the soil. The low organic content and poorly graded sandy soils are causing poor plant growth, overall plant stress, and plant with increased maintenance needed to keep plants in a healthy growing condition. The increased maintenance is directly tied

to low fertility and microbial levels, low oxygen and water holding capacity, increased soil compaction, and low organic content that exists within the on-site soils.



Sandy soils within lawn areas. Healthy soil profile should include variability with organic clods and veins which is not visible in the existing soil.

Notable observations of soil test results include:

- 1) **Soil Classification:** Generally, a Sandy loam soil classification with a 10%-15% organic content is preferred for ornamental plant growth. Existing soils are very sandy with extremely low organic content and low nutrient levels. While low organic content sandy soils may perform well in recreational lawn conditions, higher organic content and nutrient levels are best for tree, shrub and groundcover planting areas. Sandy soils increase maintenance costs by requiring ongoing water and chemical fertilizer applications for healthy plant growth.
- 2) **Organic Matter:** The optimal organic matter content for soils in lawn areas is 5% to 8% by dry weight and 10% to 15% in tree, shrub, and groundcover planting areas. The soil sample tests results reveal that organic content is very low at a 1.5% average. Addition of compost (33% by volume) and organic mulch topdressing will help increase organic content levels within the existing soils and increase water and nutrient holding capability.
- 3) **pH:** The optimal pH range for most plants is between 5.5 and 7.5. The tested pH at an average of 6.0 is acceptable for ornamental plant growth.
- 4) **Nitrogen:** Nitrogen levels are low in all but one of the soil test locations. The grass lawn soil test location displayed a higher nitrogen level likely due to ongoing lawn fertilization. Nitrogen is soluble in water and leaches out of sandy soil quickly when it rains or is irrigated. Addition of compost (33% by volume) to existing soils will increase mineralized nitrogen, active carbon and microorganisms available for plant growth, thereby reducing the need for chemical fertilization.

Character and Health

Generally, the existing plant palette within the park consists of hardy, drought tolerant shrubs and groundcovers suitable for the specified soil mix (sandy, low organic content) that was used at the time of installation. However, some species have performed better than others over time. Many shrubs and groundcovers display an overgrown character adjacent to pedestrian pathways and trail with shrubs reaching their maximum mature sizes. Bare ground is visible in areas where plants have died back and around mature shrubs that have likely shaded out groundcovers. Landscape plantings display a general lack of diversity that does not support the City's long-term goals for sustainability and wildlife

habitat. English Ivy, a noxious weed, is the predominant groundcover found throughout the park. Its aggressive nature has taken over a large percentage of the planting areas, reducing species diversity. Dead trees and shrubs are found throughout the park.



Dead or dying Burning Bush shrubs with Ivy groundcover.



Tree roots visible and bare ground in a planted area.

Throughout the park trees and shrubs display shallow root systems with large roots visible on the ground surface. Plant stress can occur when shallow roots are exposed to extreme soil temperatures and dry surface soil conditions. This phenomenon was not species specific and was observed throughout the park on the lid. Shallow roots often occur when planting and subgrade soils do not provide adequate air, water, moisture and nutrients for deep rooting and plant growth. Shallow roots can also occur when irrigation watering cycles are short and moisture only penetrates the top few inches of soil before running off. Another common reason for shallow roots is water saturated poor draining soils blocking root access to oxygen within the ground.

Current Maintenance Practices

Interviews with City of Mercer Island maintenance staff provided some insight to better understand current vegetation maintenance practices, challenges, and capabilities within Aubrey Davis Park. The park is maintained by City of Mercer Island maintenance staff based on maintenance agreement between the City of Mercer Island and WSDOT. The maintenance agreement is very general, focused on basic upkeep and does not allow for required level of soil improvement and on-going maintenance needed for a healthy, growing landscape. City of Mercer Island has limited capacity for extensive maintenance needed based on the existing conditions today. As a result, prioritization of maintenance efforts is required with higher maintenance in Aubrey Davis Park focused on areas with larger number of users or on areas that are more prominent or visible. Higher maintenance areas generally include the park on the lid and overpasses, with the rest of the area along the trail and forested edges receiving lower maintenance, which is often response oriented and limited to areas where there have been complaints by the general public. Over the years, the required level of maintenance has grown significantly and no one was able to foresee this at the time of park design and establishment of the maintenance agreement.

All landscape areas within the park receive regular irrigation and the automatic spray irrigation system is fully functional. The irrigation system is in place as was originally installed during park construction; however City staff has done regular maintenance which mostly includes fixing broken pipes and replacing spray heads. Minor adjustments in some irrigation zones have occurred over time, some zones (mostly forest areas) have been turned off over the years.

Regular landscape maintenance, as said earlier, is largely based on the current landscape maintenance agreement with WSDOT which only covers basic upkeep (mowing and fertilizing lawn areas, weed removal, irrigation, limited pruning and mulching). Basic upkeep, however, does not adequately cover the park's maintenance needs. Improving the poor-quality sandy soils to provide sufficient nutrient levels for healthy plant growth, replacement of dead plants and trees to keep planting areas full and healthy, and upgrading irrigation system with rotary nozzles or drip emitters and English Ivy removal are examples of additional maintenance practices that the park is in need of but currently not receiving.

Poor drainage due to the poorly graded sandy soils is evident throughout the park with sport fields getting saturated very quickly when it rains. Maintenance staff noted planting areas within N Mercer Way, 77th Ave SE and 80th Ave SE overpasses drain very poorly. Also, certain species don't do well in poor draining soils throughout the park boundaries: Rugosa Rose, Burning Bush, and Otto Luyken Laurel to name a few. Also Maple, Pine, and Cedar trees are declining or dying likely due in part to the poor draining soils.

City maintenance staff performs regular mowing and fertilizing of lawn areas, tree pruning is done every year mostly to prune low hanging branches and dead wood removal for safety. Maintenance within shrub and groundcover areas is limited to an as-needed basis to keep large shrubs off the trail. Mulch is being re-applied every other year in limited planting areas. Trail maintenance is limited to pruning for safety of trail users. The asphalt trail is challenging to maintain, as many tree and shrub roots have encroached under paving causing pavement to heave upward and resulting in a possible tripping hazard to pedestrians.

Safety and Security

According to city staff, the City of Mercer Island has received multiple comments from park users and local citizens about park maintenance. Most fall under the following categories:

- 1) Shrubs/vegetation overgrown
- 2) Lack of lighting in the park
- 3) Vegetation encroaching into neighboring properties
- 4) Dead trees or plants present in the park

The park is generally safe and no police reports have been filed other than some specific accidents on the trail resulting from conflicts between different trail and/or park users. People sleeping or creating camps in the park, according to City staff, is more of a perception rather than reality. However, there have been numerous comments during the master planning process about areas of the park that feel more isolated, hidden, or generally just not comfortable spaces to be in, as well as a number of comments reiterating the conflicts and safety concerns along the trail itself.

The National Institute of Crime Prevention (NICP) has developed Crime Prevention Through Environmental Design (CPTED) principles that should be considered as different areas of the park may be improved over time. These principles are also a good basis for evaluating the existing park features to address safety and security concerns of park users.

Below are some general guidelines for CPTED principles that could apply to Aubrey Davis Park:

- 1) *Natural Surveillance*. Encouraging natural surveillance by opening views and providing visual connection between different parts of the park, including strategic orientation and placement of site furnishings and other physical site elements can greatly reduce undesired activities. Views into the park from the perimeter as well as from adjacent residences into the park help increase natural surveillance. Also important is to provide a 30-foot clear line of site along travel paths. This is a general guiding principle to allow enough time for decision-making in case of potential threat in areas of higher concerns.
- 2) *Territorial Reinforcement*. Clearly established boundaries help define ownership. Low fencing, such as a split rail fence, that can easily fit within the overall character of the park, along the perimeter or use of natural elements

to create separation and clearly defined space is recommended for territorial reinforcement. This helps define ownership and belonging to community, defines a space, and creates a feeling of safety within the park boundary.

- 3) *Natural Access Control.* Provide multiple, clearly defined access points and ensure entrances are easy to locate from inside and outside the park with clear signage and visible focal points. Creating celebrated entryways that are easily identifiable from a distance helps identify escape route in case of a potential threat.
- 4) *Maintenance.* Regular maintenance goes a long way in creating a sense of ownership. Unmaintained areas attract more undesired activities. Picking up trash, regular mowing, replacing dead and dying plants, and maintenance of facilities and park equipment on a regular basis contributes to an overall perception and feeling of safety.

Some strategies to consider:

- *Lighting.* Lighting can greatly contribute to safety and sense of security within the park. Areas more prone to undesirable activities as well as any walkways open to the public at night should be illuminated to at least 0.6 foot candles along the length of the path and 30-feet on both sides of the walkway.
- *Public Art.* Public art can light up the space and when strategically placed, drawing visitors into the park. Public art can also be used for defining entrances, creating gateways and creating major landmarks within the park.
- *Landscape planting.* Applying a simple 6-foot/2-foot rule with trees limbed up to 6-foot height and shrub/groundcover planting kept to about 2-foot height allows better visibility between different parts of the park. This eliminates potential hiding places and, while it should not necessarily be applied everywhere in the park, it should be considered at access points, where park users have concerns about park safety and visibility (real or perceived) and for defining views into different areas of the park.
- *Signage and wayfinding.* Use clear signage throughout the park. Signage combined with today's technology and interactive signage allow adjusting messages that may help users know the quickest route out of the park in case of emergency, where entrances and exits are, indicate how far they are to a known landmark, and to direct park users in the right direction.

Park Setting On the Lid

Planting areas within the Park Setting On the Lid are located on top of the concrete structure over I-90. Soil depths vary between an 8-inch depth in lawn and low groundcover areas to a 6-foot depth in tree planting areas with a subsurface drainage system installed below all planting areas. A majority of the planting in this area is grass lawn with trees, shrubs and groundcovers planted around the perimeter edges. Special accent planting of shrubs and groundcovers occur in select locations, generally around recreation features, like the playgrounds or picnic shelter, with only limited trees planted across the lid itself to provide shade for park users.

Character and Health

Grass lawn is the main vegetation type on the park areas of the lids. Trees, shrubs and groundcovers are generally planted around the perimeter edges of the open space areas at or near the lid edges. Existing plant palette used within the Park On the Lid area is a small list of hardy, drought tolerant shrubs and groundcovers suitable for the specified soil mix (sandy, low organic content) at the time of installation. However, some species certainly have performed better than others over time. Based on observations during site visits, Douglas Fir trees and Strawberry Tree shrubs are doing very well but Rugosa Rose and Burning Bush are struggling. Stretches of Rugosa Rose along the trail by the tennis courts and Burning Bush shrubs have died or are showing signs of declining health. Interestingly though, all of the species named here typically perform well in sandy, low organic soils, but for some reason that is not the case at Aubrey Davis Park. Likely a combination of multiple factors has affected certain species decline in typically suitable growing conditions. It could be that its soil compaction that has occurred over the years combined with nutrient deficiency in the soil, plus heat stress from long dry summers that's causing decline of these species. A contributing factor could also be trimming of the shrubs and the time of year trimming occurred to keep vegetation from overgrowing pedestrian pathways and trail. Bare ground is visible in areas where plants have died back and around mature shrubs that have likely shaded out other groundcovers.



Shrubs and groundcovers overgrowing paving edges.



Dead trees in the park.

The grass lawn fields are heavily used by sports leagues and generally display a healthy grass turf condition. However, some areas of turf do show poor growth from excessive foot traffic and compacted soils. Grass lawn outside of the sports fields is mostly lining the trail or pathway edges with individual or smaller groupings of deciduous trees within the grass area. Grass outside of sports fields is more uneven with yellow patches and areas of bare ground under trees.

Trees within the lawn areas are generally doing better than those located within planting areas with shrubs and groundcovers. It could be that the lawn fertilization and increased watering cycle causes these differences.



Grass lawn with sport fields.



Bare ground in planting areas.

More of a unique area within the Park Setting On the Lid is the rooftop of the WSDOT maintenance building by the stacks for the tunnel below. There is no official access to public into this area, yet the step down onto the rooftop is low enough that it is easy to access. The rooftop looks unmaintained with trash laying around and the walls around the edges are covered with graffiti. Shrubs and smaller sumac trees are present on the rooftop with a denser shrub mass along the back wall and more open areas along the other three edges. Within that large dense shrub mass about half of the shrubs are dead or dying.



WSDOT maintenance building rooftop – walls with graffiti.



WSDOT maintenance building rooftop – shrub mass along back wall.

Current Maintenance Practices

The Park Setting On the Lid landscape areas receive higher level of landscape maintenance due to the larger number of park users and organized, active uses in this area. Lawn within sport fields is aerated twice a year and mowed, watered, and fertilized regularly as needed to maintain high-quality turf. Lawn areas outside sport fields are smaller in size and

don't get the same level of maintenance – maintenance in these areas is limited to regular mowing and watering. Shrubs are selectively pruned on a yearly basis to keep vegetation off of paving edges and to create formal hedges. This pruning is limited to simply keep overgrown vegetation off the trail and pathways, not to address clearing sightlines. Trees are pruned on an as-needed basis to remove dead wood and lower hanging branches for safety and maintaining vertical clearances along the trail and pathways.



Overgrown vegetation blocking views within the park.



Overgrown shrubs that block views of trailhead and park.

Safety and Security

Many shrub beds have large mature shrubs that are overgrowing paving edges and blocking views into the park. This is a safety concern as trail users don't have clear sightlines to see what to expect, or what might be coming at them, from further down the trail. Clearing site lines is important to allow enough time for decision making and appropriate reaction as situations require. Conifer trees have low hanging branches limiting visibility within the park.

The rooftop of the WSDOT maintenance building displays obvious signs of unwanted behavior and does not feel safe nor is inviting for park users but is still easily accessible. With the need for regular maintenance and sense of ownership this particular area in its current state, it is likely to continue to attract undesired activities. Dense vegetation mass is blocking views into the far corners of the rooftop and provides many places to hide and sleep. There is very little surveillance from regular park users in this area which again encourages activities that are less desirable within the park.

There are over a dozen entrances to the Park on the Lid but there are no visual cues defining these entrances except at 3-4 of these entry points. The majority of the smaller park access points are heavily vegetated and views further into the park are blocked by overgrown shrubs. Large shrubs lining pathways create narrow, channel-like conditions, are blocking views into surrounding areas, and provide potential hiding places.

The park has very little pedestrian scale lighting to extend usable hours and contribute to an overall sense of safety and security. Pedestrian lighting may be considered in areas of the park where nighttime use occurs and safety and security are a concern.

Park Setting Off the Lid

The Park Setting Off the Lid planting areas are perimeter plantings located between the I-90 lid plantings and adjacent perimeter roadways. The Park Setting Off the Lid areas include Greta Hackett sculpture gallery in the Town Center and other more open, park-like areas within the Aubrey Davis Park corridor. These areas have a park-like feel and consist of a mixture of informal trees, shrubs, groundcovers and lawn areas along adjacent street edges.

Character and Health

Grass lawn is the predominate vegetation type with trees, shrubs and groundcovers strategically planted to screen parking and I-90 freeway walls. Perimeter trees and shrubs function as a buffer to prevent public access to the freeway edges and portals. The grass lawn is generally located along paths or adjacent to roadways, but isn't large enough for any specific activities (organized or informal).



Trails with lawn, trees, shrubs and groundcovers.



Park at Greta Hackett Outdoor Sculpture Gallery.

Lawn areas display a healthy green turf with some yellow patches visible. Shrubs and groundcovers are overgrown with shrubs reaching their maximum mature size. Areas of bare ground are visible where groundcovers are not completely covering ground surfaces or shrubs have died off. Some walls along the I-90 freeway edges have extensive planting to screen views, while other areas use Boston Ivy planting to help soften the visual appearance of concrete walls.

Trees within Park Setting Off the Lid areas are in a reasonable health and greater species diversity is evident compared to the Park Setting On the Lid areas. Pear trees are large for their species and no signs of branch failure were observed. Cedar trees, however, are in decline likely due to the poor soils on site. Shallow tree and shrub root systems are visible on the ground surface, similar to the Park On the Lid areas, causing plant stress with exposure to extreme soil temperatures and dry surface soil conditions.

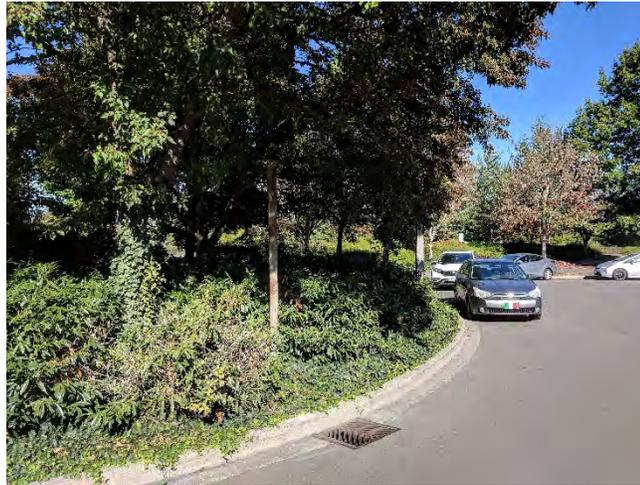
Current Maintenance Practices

The Park Setting Off the Lid landscape areas generally receive a higher level of landscape maintenance due to the high number of users and visibility from adjacent sidewalks and roadways. Lawn is watered, fertilized and mowed regularly to maintain a high-quality turf aesthetic. Aeration is not performed in lawn areas outside the sport fields within the Park On the Lid areas. Shrubs are pruned on a yearly basis to keep vegetation off of paving edges or to create formal hedges, but

are not specifically pruned for sight distances. Tree pruning is performed on as-needed basis to clear dead and low hanging branches.



Gateway sign at the Sculpture Gallery.



Street edge with poor visibility into the park.

Safety and Security

Park Setting Off the Lid areas generally maintain clear views into the landscape areas from sidewalks and adjacent roadways. While planting along the street edges is mostly lawn with trees allowing clear sightlines, larger shrubs block views deeper into the more distant areas of the landscape and potentially provide places to hide. To maximize natural surveillance from adjacent areas and within the park, shrub and groundcover plantings should be kept to a maximum height of two feet within sight lines unless taller plants are needed for perimeter screening purposes. High concentration of art within the Park Setting Off the Lid area helps create identity and sense of ownership. Use of art in public spaces helps to activate the space with wayfinding and, along with more programmed use, is a great tool for reducing tagging and other types of undesired activities. Similar to Park Setting On the Lid, the areas within Park Setting Off the Lid would benefit from pedestrian scale lighting to extend usable hours and contribute to the overall sense of safety and security.

Bridge Planting Edge

The Bridge Planting Edge contains planting areas located on the I-90 bridge decks at 76th Ave. SE, 77th Ave. SE, 80th Ave. SE, Island Crest Way, Shorewood Drive, North Mercer Way and East Mercer Way. The Bridge Edge plantings consist of various sizes of shrub and groundcover plantings with a mix of deciduous and evergreen trees in raised planter boxes.

Character and Health

Shrubs and groundcovers display an overgrown character with many of the shrubs reaching their maximum mature size and some areas of bare ground visible where groundcovers are not completely covering ground surfaces. Trees are planted in individual concrete planter boxes with a variety of tree species: Austrian Pine, Green Ash, Western Red Cedar, Douglas Fir, and Flowering Pear. Some of the species are too large for the size of the planter box and available soil volume. Trees show major health issues on all of the overpasses with a number of trees dead or in decline. Soil volume in the planter boxes has decreased to as little as half of its original volume. Signs of drought stress are present and many trees have outgrown the boxes and developed a lean. Dominant groundcover throughout the Bridge Planting Edge areas is the noxious weed English Ivy, leaving very little to no room for other species. Shrubs and groundcovers have died due to aggressive Ivy growth or are showing signs of declining health.



Trails bordered with lawn, trees, shrubs and groundcovers.



Overgrown shrubs with sparsely vegetated groundcover.

Current Maintenance Practices

The Bridge Edge landscape areas receive a higher level of landscape maintenance due to the high visibility from adjacent sidewalks, trail and roadway. Over the years the planter boxes on the bridge overpasses have received very little fertilizer – according to the maintenance staff perhaps about 4 times over the last 15 years fertilizer has been added to the concrete planter boxes. Maintenance staff noted that poor drainage is evident within the planting areas on overpasses at W Mercer Way, 77th Ave and 80th Ave. Shrubs are pruned on a yearly basis to keep vegetation off of paving edges but not specifically for sight lines. Tree pruning is performed on an as-needed basis.



Overgrown trees in landscape planters.



Staggered planter boxes along the trail edges at bridge crossing block views from adjacent roadway.

Safety and Security

Vegetation is densely planted along the perimeter of the bridge deck edges and railings to minimize public access. This helps reduce the opportunity for objects to be thrown onto the roadway below. Tall shrubs and raised planters between the roadway edge and pedestrian path block line of sight and discourage natural surveillance by passing vehicular traffic. In case of a pedestrian or bicyclist needing help, passing cars would not be able to see them. Keeping shrub and groundcover planting low between the roadway and pedestrian pathway helps minimize places to hide and allows for a longer decision making and reaction time in case of a potential threat.

Forested Edge Off the Lid

The Forested Edge Off the Lid planting areas consist of mostly conifer forest planting areas located between the I-90 lid retaining walls and adjacent perimeter roadways. Planting in these areas consist of mature mixture of trees, shrubs, and groundcovers forming a dense forest environment. These forest areas are not as heavily used due to the dense vegetation and unfavorable terrain conditions, unless a segment of the trail or well used path connection cuts through the area.

Character and Health

Native trees (Douglas Fir, Western Red Cedar, Big Leaf Maple, Western Hemlock) provide a natural forested landscape character and buffer along the edges of I-90. A large percentage of the forest understory is covered with the English Ivy. Over time, English Ivy crowds out desirable shrubs and groundcovers leaving a weakened forest condition without plant diversity. Densely planted forest has very little mid canopy growth due to tight on-center planting of forest trees. Little light is able to penetrate through the existing tree canopy, making it difficult for the undergrowth to establish and contribute to natural renewal and long-term forested condition. Slopes in these areas are often steep and some areas are difficult to pass through due to dead branches. In addition to paved pedestrian path connections and trails cutting through the forested areas, there are also some smaller soft surface trails used by nature lovers and dog walkers. Forested areas off the lid get fewer users compared to the park on the lid.



Trails bordered with lawn, trees, shrubs and groundcovers.



Dead branches in a densely planted forest.

Current maintenance Practices

The Forested Edge Off the Lid landscape receives a lower level of landscape maintenance due to its natural character and lower number of users. Trees are pruned on an as-needed bases and much of the maintenance is response based to keep vegetation off adjacent trails and roadways. No other regular maintenance efforts go into these areas.



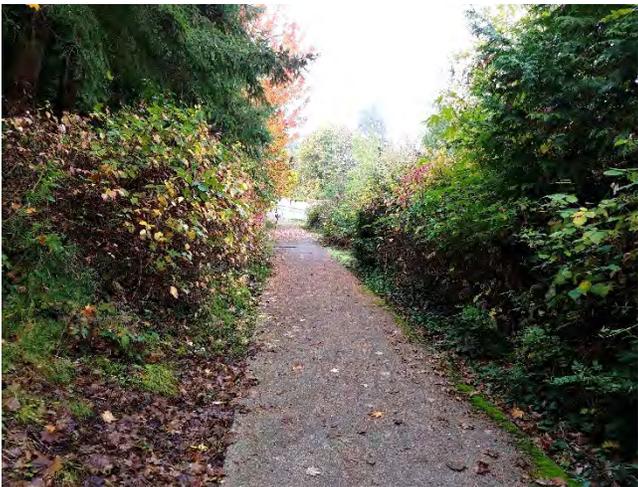
Steep slope with trees and dense ivy groundcover.



Native restoration plantings.

Safety and Security

Densely forested areas have some dead and dying trees which can become a hazard for uses on nearby trails. Conifer trees have low hanging and dead branches extending all the way to the ground, making some areas inaccessible and limiting visibility. Large shrubs are overgrowing pathway edges and block views for desired line of sight which making these forested areas less inviting. Narrow pathways with overgrown vegetation are perceived as less safe, while having dense vegetation pulled back from pathway edges helps to open up line of site and allow time for decision-making and reaction in case of potential threat.



Pedestrian path aligned with dense vegetation.



Conifers with Ivy groundcover.

Trail Corridor

The Trail Corridor planting areas are located adjacent to the Mountains to Sound Trail (I-90 Trail) and framed by bordering roadways. The Trail Corridor plantings consist of a mix of tree, shrub, and groundcover plantings with some stretches of lawn along the trail's edge.

Character & Health

The trail landscape consists of mostly native trees, shrubs, and groundcovers with higher use of ornamental plants within the Town Center segment. Some stretches through the Park On the Lid and trail connections to the Town Center and are lined with lawn. Signs of struggling plant health are present: trees and shrubs show signs of stress likely due to poor quality sandy soils, drought conditions and lack of irrigation. Tree roots are visible on the surface of the ground, bare ground is exposed in planting areas with no mulch topdressing. Stretches of Rugosa Rose within the Park on the Lid by the tennis courts have died along trail's edge, pavement is heaved by roots of adjacent trees and shrubs posing a potential hazard for trail users.



Trail at the east end of the Park On the Lid.



Trail lined with overgrown shrubs on both sides.

Current Maintenance Practices

Different areas of the trail corridor landscape receive different levels of landscape maintenance. Trail segments through the Town Center and within the Park on the Lid that are lined with stretches of lawn receive higher level of regular maintenance. Outside of these boundaries receives a lower level of maintenance due to the less prominent location and more native plant palette. Trees and shrubs are pruned as needed annually to keep vegetation off of adjacent trail and roadways. Encroaching tree roots cause heaving of asphalt paving throughout the park limits creating a maintenance challenge for city staff. Higher level of maintenance is needed for the asphalt trail to keep overgrown vegetation off and accommodate the need for necessary paving repairs.

Safety and Security

According to City staff, multiple accidents have occurred between trail users due to limited visibility along trail. Large shrubs are overhanging trail paving edges and blocking necessary sight lines along the trail as well as limit views into adjacent areas of the park.



Trail with native and ornamental plantings.



Trail with limited visibility.

Trimming back encroaching vegetation along trail edges allows for necessary views up and down the trail to visually spot other trail users and allow time for proper decision making.

General Recommendations

Shrub and Groundcover Planting Areas

To help increase plant diversity, reduce maintenance, and alleviate the poor soil conditions, the following options are recommended for better vegetation management and restoration efforts throughout the park:

1. Infill Planting

This option proposes to infill plants within existing tree and shrub beds with species better suited to grow in soils currently found within the limits of the park - see Appendix D: Recommended Plant List. Infill plants would be selectively planted within existing vegetation with the goal of solid vegetation coverage of all planting areas resulting in no visible bare ground. This is a long-term approach and improvements in the overall character and health would be noticeable over longer period of time. At time of planting, compost should be added to subgrade soils at a ratio of 33% by volume to support healthy plant growth. Regular compost mulch topdressing is also recommended until plants completely grow and cover all bare ground. This option is recommended for areas that are not as visible to the public, outside of the programmed park areas, and generally consists of only partial vegetation decline. If the landscape is still mostly intact, free of English Ivy and able to serve its original function (as a buffer, forested edge, etc.) then this is a good, low-cost option to consider.



Infill plant where plants area showing signs of stress and decline. Consider wood rail fence barrier to deter foot traffic until plants become established.



Trees, shrubs and groundcovers all display signs of declining growth. Remove all vegetation and improve soil with surface restoration method.

2. Surface Restoration

Surface Restoration soil preparation is a method of introducing compost into subgrade soils to improve soil structure, organic content, air and water holding capacity, nutrients, and microbial activity. This is accomplished by placing a compost layer (33% by volume) over existing soil and scooping with a backhoe to a specified depth, recommended at a minimum of 36 inches. The scooped material within the backhoe bucket is raised to a 3 to 4-foot height and dumped back into the same hole that was excavated. Mixed compost and existing soil should have veins and clods within the soil profile, therefore thorough mixing of compost and existing soil is not recommended. If the planting area is covered with English Ivy, this method will uproot the Ivy plants allowing for easier vegetation and

root removal. Dumped soil is then lightly compacted and hand raked to a finish grade prior to planting. During the process of scooping and dumping, the irrigation system may be damaged and require extensive repairs. This option is recommended for planting areas that need to be heavily replanted due to Ivy removal, reduced sight distances, overgrown vegetation, blocked view corridors, and where significant die back or stunted growth has already occurred.

3. Targeted Trenching Soil Restoration

This approach proposes trenching within drip lines of existing trees to improve soil under the tree canopy while also avoiding damage to existing tree roots. Working within critical root zones of existing trees requires care and close collaboration with a certified arborist to minimize impact and avoid damage to roots. Trenches are generally constructed radially from the tree, and are about 12" wide and 24" deep. The process of adding compost to subgrade soils is similar to the surface restoration method described above but the work is limited to the narrow 12-inch wide trench. An air spade pneumatic soil excavator tool is recommended in lieu of a backhoe in areas where tree roots may be easily damaged.



Apply targeted trenching soil restoration method within tree drip lines and maintain mulch rings at base of trees. Remove overgrown vegetation along pathways and trail edges.

4. Remove and Replace

While the soil improvement methods described above provide a few different approaches from less intensive to more intensive, there is also an option to completely remove existing soils and do a full restart. This option is proposing removal of existing soils and vegetation entirely and bringing in new soils to establish new vegetation. This option could be applied in high visibility areas where enhanced ornamental landscape is desired. This approach would provide the greatest soil improvement for long term plant growth.

Remove Overgrown Vegetation

Removal of overgrown vegetation encroaching onto trail and pathways and replacing with smaller more compact forms better suited to edge the pathways and trail reduces amount of required trimming and dead wood removal needed long-term. Overgrown plants also block views into the park and can create unsafe environment by providing hiding places and attract undesirable activities.

Ivy Removal Areas

English ivy is considered a noxious weed because it overtakes the entire planting area creating a monoculture and crowds out other nearby desirable plants. English Ivy also decreases wildlife habitat by reducing diversity. To remove English Ivy:

- Where surface restoration is applied to a landscape area, remove the English Ivy along with other vegetation. Ivy is a very hardy plant and even a small amount of buried root material can survive and start a new plant.
- Where surface restoration soil preparation is not possible under drip lines of existing trees, manual removal methods are recommended in lieu of chemical herbicides. English Ivy's waxy foliage repels herbicides, which run off to damage nearby plants and pollute water systems. The best method for removal is to begin by cutting all vines that have scrambled up trees and shrubs. Remove Ivy from ground by pulling strands and prying roots with a small hand-mattock. Physically remove all vegetation and roots to the greatest extent possible.
- Apply 3-inch depth of arborist wood chip mulch in ornamental planting areas and in forested areas, increase the depth of mulch topdressing to 6-inches to help repress regrowth of the English Ivy prior to planting new plants.
- Monitor planting areas for a one-year period with continued English Ivy removal. During the one-year period continue to remove English Ivy vines where visible.
- After one year plant new shrubs and groundcovers that will tolerate sandy, low organic content subgrade soils. Select a variety of plants that provide a layered landscape of tall, medium, and low growing plants that completely cover the ground surface and crowd out invasive weeds. See Recommended Plant List – Appendix D: Recommended Plant List.
- Continue to monitor and remove English Ivy until completely removed from planting area.



Hand-remove English Ivy inside tree driplines and use surface restoration method outside of driplines.

Active Recreation Lawn Areas

Ongoing maintenance should include fertilization and water audits to reduce water use. Soil laboratory test results provide recommended fertilizer applications - see Appendix E: Soil Test Results & Locations. In addition to regular maintenance, the following is recommended:

- Aerate and de-thatch lawn as needed to increase water and nutrient infiltration.
- Test soils annually to determine what additional nutrients are needed and apply an organic, slow release fertilizer per soil laboratory recommendations.

- Perform yearly irrigation audits to reduce water use.
- Strip and resurface, including any drainage improvements needed, with new subsoil and natural turf approximately every 20-25 years.

Non-Recreation Lawn Areas

Removal of mowed grass and selection of drought tolerant eco-lawn or groundcover planting is preferred. Allowing the lawn to 'brown out' is another option available. To remove the mowed grass, replacement can occur as follows:

1. Eco-lawn Option

Lawn areas not used for active recreation could be turned into a meadow-like eco-lawns that do not require automatic irrigation, fertilizer or regular mowing. Eco-lawn seed mixes provide a mixture of low growing grass and perennial flowering plant species. These lawn areas may be mowed only once or twice a year to an approximate 6-inch height.

2. Drought Tolerant Groundcovers

Replace high water use lawn with low water use groundcovers and shrubs. Select groundcovers that grow low and tolerate occasional foot traffic from nearby active recreation lawn areas, or larger shrubs to help limit disturbance through planting areas and direct circulation towards the desired locations.

Improve Irrigation System

A few strategies are described below to improve the irrigation system and reduce landscape maintenance. The Irrigation system in place is old and could be replaced with more efficient new system using less water and minimizing overspray and runoff. The existing spray and rotor heads apply water more quickly than the compacted, poorly graded sandy soils can absorb, thereby wasting water due to runoff. Changing spray heads to drip or rotary nozzles would reduce water runoff and improve plant health with slower and deeper water application. Lawn areas not used for active recreation could be turned into meadow-like eco-lawn fields that don't require irrigation or changed to drought tolerant groundcover planting areas. This would reduce areas where irrigation is needed and thus reduces irrigation maintenance and water use.

Heaving Sidewalk Paving

Many of the sidewalks and trails display heaving pavement due to shallow tree roots. Shallow tree roots are caused by the poorly graded sandy soils existing on site. To prevent this condition from becoming worse, tree roots should be cut with a soil trencher at pavement edge and root barrier installed to 18-inch depth to prevent future root/pavement conflicts.



Trench along paving edge to stop tree roots from heaving adjacent paving. Install 18" depth root barrier to prevent future root problems.



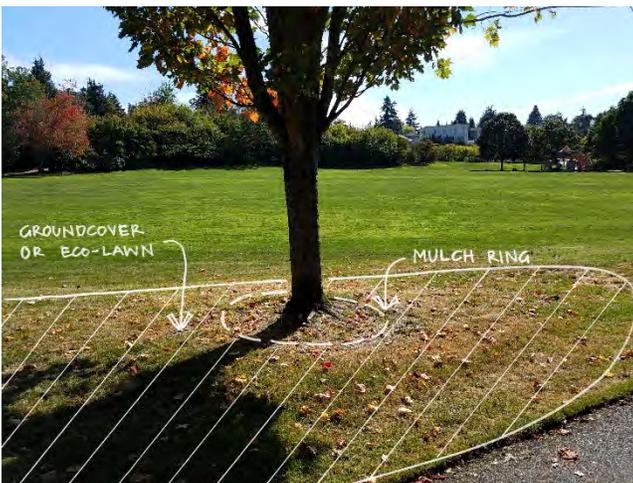
Heaving paving is a potential hazard for trail users.

Blank Walls

To soften the aesthetics of blank walls and reduce the potential for graffiti and tagging, vine plantings may be considered at base of blank walls. Vines may be either self-clinging or supported with green screen lattice systems attached to wall.

Mulching Around Trees

Mulching around trees helps hold moisture, provide necessary nutrients and improves long term health of the trees. A 6-foot to 8-foot diameter, 6-inch deep ring of arborist mulch will help protect tree trunks and roots from mower damage and increase moisture and nutrient holding capabilities within the soil.



Consider groundcover or eco-lawn in areas where lawn is not used for active recreation. Improve soil with targeted trenching and surface restoration methods prior to replanting.



Soften the look of blank walls with green screen vine planting. Also, application of art works effectively to deter tagging.

Site Specific Recommendations

Park Setting On the Lid

- Where existing shrubs have overgrown their planting bed and/or are blocking views into the park, selectively remove and replace shrubs with lower growing shrub varieties. Alternatively, some shrubs, such as the Strawberry Tree can have lower branches selectively removed to open views below the vegetation canopy to create a small tree form.
- Where existing shrub beds display declining health or bare ground, replace entire shrub bed with compost amended soils and new shrubs and groundcovers. Plants should be selected to completely fill in planting areas but not overgrow maximum height and spread requirements. Consider expanding plant selection to provide visual interest and emphasize high visibility areas of the park.
- Remove English Ivy groundcover from all planting areas and replace with alternative groundcovers.
- Consider Eco-lawn or drought tolerant groundcover in lieu of mowed lawn.
- Consider upgrading irrigation system to low water use rotary nozzles or drip emitters.
- Improve WSDOT maintenance building roof planting to reduce potential of undesirable activities.



Remove all overgrown vegetation and Ivy groundcover. Improve soil with targeted trenching and surface restoration methods prior to replanting.



Remove trees that display poor growth with damaged trunks and/or shallow root systems.

Park Setting Off the Lid

- Where existing shrubs have overgrown their planting bed and/or are blocking views into the park, remove and replace shrubs with lower growing shrub varieties.
- Where existing shrub beds display declining health or bare ground, replace entire shrub bed with compost amended soil preparation and new shrubs and groundcovers. Plants should be selected to completely fill in planting areas but not overgrow maximum height and spread requirements. Consider expanding plant selection to increase plant diversity and emphasize high visibility areas of the park.
- Remove English Ivy groundcover from all planting areas and replace with alternative groundcovers.
- Consider upgrading irrigation system to low water use rotary nozzles or drip emitters.

- Consider replacing mowed lawn with a lower maintenance and more environmentally friendly eco-turf or ornamental groundcovers in areas where active or informal play does not occur.
- Consider vine panels and green screens to help screen and soften the look of blank walls.

Bridge Planting Edge

- Maintain vegetation barriers to prevent public access to the bridge deck railings and freeway overlooks.
- Keep vegetation low between roadway and pedestrian path/trail. Taller shrubs may be considered along back edges of bridge planting areas. Where existing shrubs have overgrown their planting bed and/or are blocking views remove all vegetation and replace with lower growing shrub and groundcover varieties.
- Remove English Ivy groundcover from all planting areas and replace with alternative groundcovers.
- Where existing shrub beds display declining health and/or bare ground within groundcover beds, remove all vegetation and replace with compost amended soil preparation and new shrubs and groundcovers.
- Consider low growing ornamental perennials and groundcovers to provide greater color, texture and visual interest for the high visibility intersections and roadway edges.
- Consider painting existing concrete planter boxes to compliment surrounding plantings.
- Consider enhanced architectural planters in lieu of the existing square concrete planter boxes.
- Consider upgrading irrigation system to low water use rotary nozzles or drip emitters.



Remove all existing vegetation and improve soil with surface restoration method.

Forested Edge Off the Lid

- Where existing shrubs have overgrown their planting bed and/or are blocking views into the park, remove and replace shrubs with lower growing shrub varieties.
- Remove English Ivy groundcover from all planting areas and replace with alternative groundcovers.
- In native conifer forest, selectively thin/ remove trees to encourage understory plant growth. Remove English Ivy and replace with compost amended soil preparation and native shrubs and groundcovers to enhance the native forest environment.
- Consider native plants and habitat enhancements within the forest understory planting areas to encourage wildlife and pollinators.

Trail Corridor

- Where existing shrubs have overgrown their planting bed and/or are blocking views into the park, remove and replace shrubs with lower growing shrub varieties.
- Remove English Ivy groundcover from all planting areas and replace with alternative groundcovers.
- Consider native plants and habitat enhancements within understory planting areas to encourage wildlife and pollinators.
- Provide minimum 2-foot wide shoulders along trail edges clear of all lateral obstructions, including shrubs and groundcovers.
- Cut tree roots and install root barrier along edges of paving.
- Keep shrubs and groundcovers low – up to 2-foot maximum height to provide clear sight lines along the trail corridor at curves and roadway crossings.

APPENDIX C
ARBORIST NOTES



ARBORIST NOTES

DATE: December 30, 2018

SUBJECT: Mercer Island I 90 Lid Landscape
Vegetation Condition and Recommendations
Redline Map Notes

FOR: HBB Landscape Architecture
Attn: Merit Oviir
215 Westlake Avenue North
Seattle, Washington 98109-5217

FROM: Urban Forestry Services, Inc.
Anna Marie Heckman
ISA Board Certified Master Arborist® #PN-6153B,
ISA Tree Risk Assessment Qualified

Map 1 Notes



1. Forest Edge Off Lid –

Existing: Douglas fir, *Pseudotsuga menziesii* and Western red cedar, *Thuja plicata* are 10-15 ft apart. English ivy, *Hedera helix* and Blackberry understory, *Rubus armeniacus*.

Recommendation: Crown raise trees in forest. Slowly thin forest over a 5-year span to retain trees at 30' spacing. Remove dying cedar trees. Remove invasive species. Add wood chip mulch for soil protection and restoration.



Forest Setting on Edge of Lid - Soil sample was taken for texture analysis.

Existing: Soils are gravelly sand with large round rocks. Original specifications called for compost mix into subgrade on site.

Recommendation: Replenish soil nutrients in forest with tub grind or arborist wood chips to assist with drought tolerance and weed suppression.



2. Trail Corridor – Landscape trees

Existing: Green ash, *Fraxinus pennsylvanica* and Amur maple, *Acer ginnala*, planted linearly 20 feet apart and less than 10 ft from the wall. English ivy and landscape shrub understory.

Trees in this area are in poor health. The bike path has large angular rock along the edge impacting vegetation growth.

Item 8.

Recommendation: Trees should be replaced when landscape areas are renovated. Investment in tree pruning for structure is not worth the effort for these specimens. Soils should be rejuvenated for deeper root growth when landscape is replaced.



Park Setting on Lid - Soil sampled from landscape area for nutrient analysis.

Existing: Soils were compacted, sandy and had extensive fibrous root mat through the top inch. Soil organic matter and nitrogen levels are low.

Recommendation: Add steer manure and biochar to landscape soils to improve microbial soil conditions.



Photo 1. Trees along the trail corridor within a landscape edge are drought stressed. Some trees require removal. Many require structural pruning and soil enhancement.



3. Park Setting on Lid – Trees in lawn area

Existing: Green ash, *Fraxinus pennsylvanica* and Norway maple, *Acer platanoides*, spaced 20 ft apart. Trees have very small soil protection rings and shallow root systems. Shallow roots are being damaged by mowing operations. In general, trees in the lawn areas are in better health than those in the landscape areas. Watering cycles, plant competition, and soil vegetation cover may explain some of these differences.

Recommendation: Structurally prune and crown raise trees. Increase soil protection ring size to incorporate groups of trees and reduce mowing in circles. Reduce soil compaction with an air spade and add arborists wood chips to tree rings.

2



Photo 2. Trees in lawn areas adjacent to the bike path are larger and healthier than those in the landscape. Soil compaction and shallow roots are evident. Trees should be crown raised and structurally pruned.

3



Photo 3. Trees growing along the edge of the base of lid walls are tall enough to block valuable views from the park. Removing trees or crown raising will impact the screening for neighbors adjacent to the wall. Thin trees and replant to create multiple size canopies along the wall and window prune remaining tall trees for the view over the top of the wall.

Map 2 Notes

★ 4. Park Setting on Lid – Trees in Landscape and Trees in Lawn
 Existing: Norway maple, *Acer platanoides*, red maple, *Acer rubrum*, and Amur maple, *Acer ginnala*. Trees have very small internode growth, some dieback in the crown. Root exposure is being impacted by mowing operations in lawn and are not covered in mulch in landscape.

Recommendation: Structural prune. Increase tree ring size to squares in lawns along road or create a planting strip to reduce mowing in circles. Reduce soil compaction with an air spade and add arborist wood chips.

⊕ Park Setting on Lid – Soil sample from lawn taken for nutrient analysis.
 Existing: Soil was higher in nitrogen and in organic levels than other soil samples on the lid. Levels are still below desired rates.

Recommendation: To improve tree root growth in lawns, water deeper around trees and increase tree ring size in areas where lawns are not growing well. Add arborist wood chip or other wood chip mulch to tree rings to improve soil compaction.

⊕ Park Setting on Lid – Soil sampled from landscape area for structural analysis
 Existing: Soil was taken under ivy cover. Texture is sandy and was unusually dry given the recent wet weather.

Recommendation: Soil structure may not be readily improved other than by adding compost or biochar to decrease density. If sand size is not diverse, compaction is highly possible.

4



Photo 4. Extreme drought conditions are apparent in some irrigated landscape areas indicating a need for increased funding for irrigation maintenance and adjustments through the season.

5



Photo 5. Trees surrounding play areas have some dieback and require structural pruning and crown cleaning.

6



Photo 6. The health of many trees throughout the trail corridor is poor. Landscape soil improvements to improve water retention and reduce temperature fluctuation may improve health. Extensive pruning to thin, improve structure and raise crowns can be conducted to improve visibility in trail junctions.



5. Trail Corridor on lid – Trees in Landscape and Trees in Lawn

Existing: Norway maple, *Acer platanoides*, red maple, *Acer rubrum*, Amur maple, *Acer ginnala*, Katsura, *Cercidiphyllum japonicum* and flowering dogwood, *Cornus florida*.

Trees have very small canopy internode growth. Some areas have greater drought signs with full areas of shrub and tree die back. Dieback seems to be more site specific than species specific along the trail corridors. Many dogwood trees in the sun are dead. Nearly all the Amur maples are overgrown with extensive crown dieback on the interior.

Soil within the tree root zones are bare and compacted. Roots are highly visible on the surface. Some trees have roots growing into and buckling the pavement of the bike trail.

Recommendation: Remove dead trees and renovate soils in large landscape areas prior to replacing. Where roots are lifting the bike trail, grind roots off (sever) at edge of trail and install root barrier when trail is repaved. Many trees are planted closer than 3 feet from trail. Install future plantings further from trail when possible to allow more root growing space. Improve soil conditions for greater root space for trees in landscape by adding surface compost and arborist wood chips.

Conduct an irrigation coverage audit in mid-summer when plants are full grown to provide information relevant for seasonal adjustments in watering due to plant growth. Irrigation zone spray/soak cycles may require assessment to water deeply without surface runoff in compacted soil areas.

7



Photo point 7. This is a low maintenance natural area to the east of the vents. This landscape roof top garden is weedy and in very poor condition. The whole area can be removed and replaced.

★

6. Forest Setting on Edge of Lid

Existing: Douglas fir, *Pseudotsuga menziesii* and Western red cedar, *Thuja plicata* plantation. 8-16 ft apart. English ivy understory. Trees are crown raised for under canopy visibility. Cedar trees are dying and Douglas fir show poor canopy growth. Trees on the inner portion of the forest area are tall and thin with low crown ratios. Diameters range from 8-10 inches. Edge trees are larger in diameter (12-15 inches) and have healthier unevenly distributed crowns. A tree core from a suppressed Douglas fir shows consistent very slow tree ring growth indicating long term poor site conditions.

Recommendation: Remove ivy and add hog fuel or arborist wood chips. Slowly thin the forest over a 5-year span to retain trees at 30' spacing. Remove dying cedar trees. Remove bark beetle infested Douglas fir trees before they fully die. Replant forest areas as openings in the canopy expand. This will maintain a rotation of multi-story conifer forest and provide sound and visual barriers between neighborhoods, parks, freeways, and walls.

8

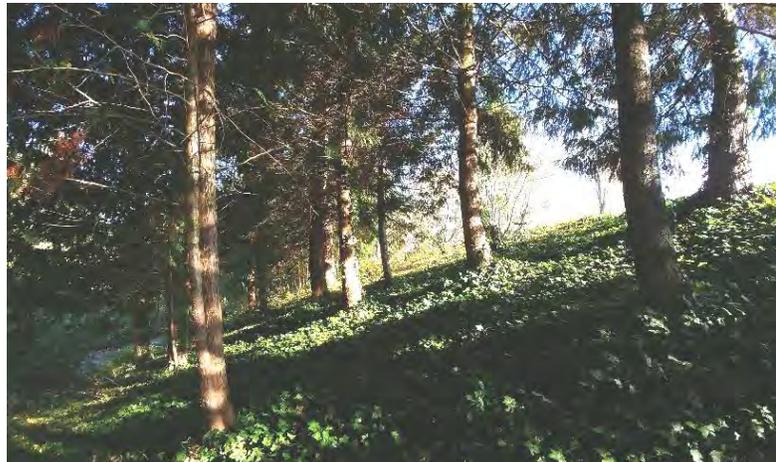


Photo 8. This is a forest area on edge of the lid. Trees in this area have been crown cleaned to allow visibility under the canopy. Trees in the center of the planting have low crown ratios and many are suffering from bark beetle damage.

Map 3 Notes



7. Trail corridor on lid – Trees in Lawn and landscape and forest edge.

Existing: Red oak, *Quercus rubra*, Norway maple, *Acer platanoides*, western red cedar, arborvitae, *Thuja plicata*. Soils are very compacted, in high use area and lawn is not growing well under tree canopies. Tree roots are shallow and impacting trail.

Recommendation: Trees in lawns adjacent to trail may be converted to planter strips to decrease mowing maintenance and protect highly trampled soils. Continual addition of arborist wood chip mulch will prevent further damage. Trees planted between the retaining wall and trail should be small to medium columnar trees to cover the wall and not require pruning for clearance. Crown raise trees along bike trail along forest edges.



Photo 9. Trees in a lawn adjacent to the trail corridor are on the edge of a steep hill side. The soil compaction and poor grass cover make this area difficult to maintain as a lawn.



8. Park maintenance area off lid.

Existing: Blackberry covers much of this non-maintained natural area. This area has potential to add to the cities tree canopy goals and provide greater use for maintenance operations. Currently it is a source of weeds to neighbors and the adjacent park.

Recommendation: Remove blackberry from this area and convert to grass. This landscape could be used as a staging area for wood chips or ivy piles and a holding nursery for replacement plants and volunteer resources for the lid.

10



Photo 10. Where English ivy has been removed, shallow roots are exposed in planted conifer forest area. Invasive removal and the addition of wood chip mulch or hog fuel in these areas will significantly improve forest health conditions.



9. Park Setting on Lid – Low Maintenance Natural Area.

Existing: Black cottonwood, *Populus balsamifera*, western red cedar, *Thuja plicata*, Douglas fir, *Pseudotsuga menziesii*, Norway maple, *Acer platanoides*, big leaf maple, *Acer macrophyllum*; deodoar cedar, *Cedrus deodora*; Pacific madrone, *Arbutus menziesii*; strawberry tree, *Arbutus unedo*; Callery pear, *Pyrus calleryana*. Many species such as the black cottonwood and the random madrone look to be naturally established and some trees look to be random additions into these areas after landscape renovations were complete.

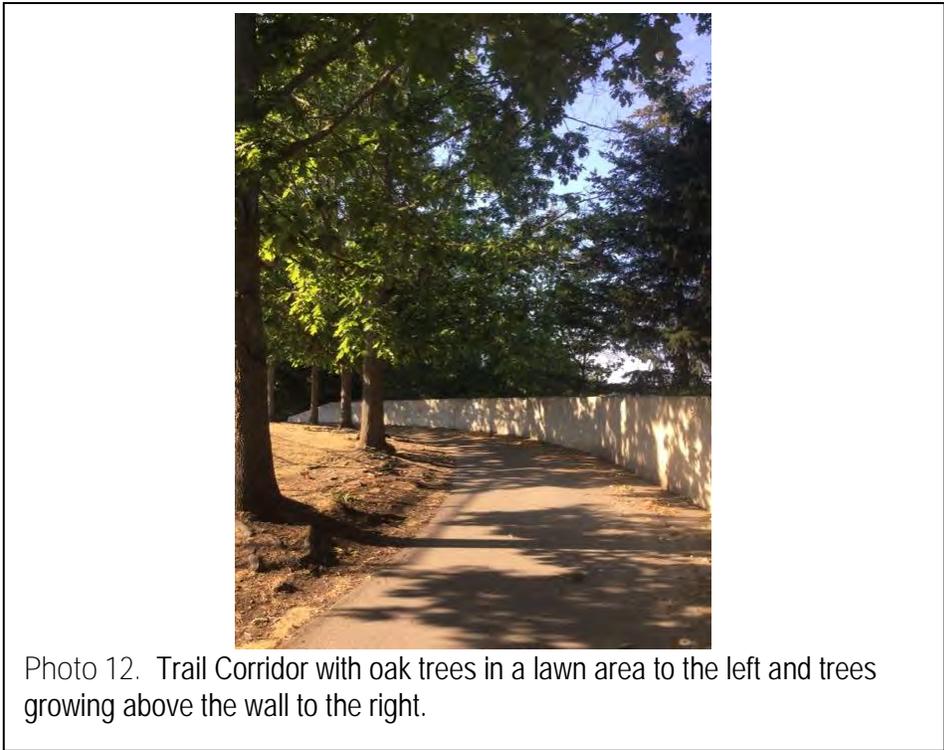
Cedar trees are dead and Black cottonwood are a high risk to have along the freeway. Summer branch drop, and poor structure lend them to breaking in low storm conditions. Trees in this landscape do not look groomed or maintained.

Recommendation: Remove cedars and black cottonwood trees. Replace with another species. Soils may benefit from compost and wood chip enhancements. Trees and shrubs should be pruned for visibility throughout but retain the noise and large-scale visual blocking of the freeway. Pruning techniques should maintain the natural area look

11



12



Map 4 Notes

Item 8.

No photos or site observations were recorded for Map 4

Map 5 Notes



10. Park setting off lid – Trees in landscape, in containers and in lawn.

Existing: Flowering pear, *Pyrus calleryana*, sweet gum, *Liquidambar styraciflua*, Austrian pine, *Pinus nigra*, columnar beech, *Fagus sylvatica* var. Norway maple, red oak, red maple, Douglas fir, western red cedar, giant sequoia, *Sequoiadendron giganteum*. Trees in this area are in reasonable health and have greater species diversity than what is found on the lid. The pears in the landscape bed have grown large for their species and have not shown signs of branch failure. Soil compaction and shallow root issues do not seem dramatically different to those on the lid. Cedars in this area are in decline.

Recommendation: All trees in this area require structural pruning. Tree wells in the lawns can be increased and arborist wood chip mulch can be used to enhance soil. Current wood chip mulch applications should be pulled away from trunks to follow current best management practices for tree health. Trees along the freeway wall can be pruned for small scale visibility into the landscaping while retaining sound and large scale visibility barrier. Dead and dying cedars should be removed from the site.



Soil sample Park setting off lid - Sample was taken from a lawn area for nutrient analysis.

Existing: Soil texture was a similar high sand content consistency to samples taken on the lid. Nutrient analysis was also like those on the lid where nitrogen and organic carbon were slightly higher than samples taken outside the lawn areas.

Recommendation: As with the other soil samples, increasing organic content and nitrogen in the soil is necessary prior to managing other nutrient issues.

13



Photo point 13. Pears in the landscape area are large for their species. Landscape beds and lawn areas have similar issues to those found on the lid. High use, soil compaction, and low soil organics impact planting areas. Cedar trees along forest plantation are dead.

Map 6 Notes

No photos or site observations were recorded on Map 6

Map 7 Notes

14



11. Forest Setting off Lid.

Existing: Big leaf maple, western red cedar, Douglas fir, western hemlock

This site is just outside the area impacted by freeway construction. This forest area has greater species diversity. The trees are older, larger and spaced further apart. A tree core shows tight ring growth similar to that found on the Forested lid edge. Cedar and hemlock trees are suffering in this area. Soils are varied with significant organic layer accumulation where ivy was removed.

Recommendation: Remove English ivy and blackberry. Plant mid story canopy and forest edge species along forest edge near freeway to help retain interior forest health. Monitor trees for continued decline and remove if they become a high risk.

15



Photo 15. Forest site off lid. This mature forest adjacent to the freeway is older than those on the freeway lid and along the wall edges. Similar health and invasive species issues impact this area.

Map 8 Notes



12. Forest Setting off lid.

Existing: Black cottonwood, Red alder, Pacific Madrone, *Arbutus menziesii*, Deodar cedar, *Cedrus deodara*, western red cedar, Douglas fir. This area is adjacent to Site 11, but located within the construction impact area for the freeway. Forest is more open with greater number of edge species and deciduous trees. Soils are compacted near road access area. Many trees were not planted. This forest area is in poor condition with many dead and dying trees.

Recommendation: Invasive species removal and conifer planting is recommended in areas of healthy soil. Some impacted areas along the road edge should have mechanical soil decompaction and be renovated into a landscape edge.



13. Bridge Planting Edge.

Existing: Austrian pine, Green ash, cedar, Douglas fir, flowering pear. On the overpasses, Pines and ash trees are planted in cement boxes. Soil in many boxes has decreased to half the original volume. Some trees show signs of drought stress potentially caused by irrigation issues, soil temperature extremes, or by low soil volume. Some trees have outgrown the boxes and are leaning. In many areas the tree boxes are set in unusual patterns that make maintenance difficult.

Recommendation: Remove all dead and dying trees. Place boxes in easy maintenance pattern (for mowing or ground cover weeding). Connect soils in planter boxes to those surrounding. Refill boxes with soil that will last longer (less compost). Tree replacements should consider a less broad and more even distribution canopy tree for greater stability.

16



Photo 16. English ivy along bridge deck. Tree planter to the left has ample space for maintenance.

17



Photo 17. Trees in planter boxed require pruning. Some with uncorrected leans will require replacement.

Mercer Island Lid Tree and Landscape Assessment Red Line Map

Map 1 of 8

December 30, 2018

Item 8.



LEGEND

-  OBSERVATION POINTS FOR THE 5 MANAGEMENT AREAS
-  CONIFER FOREST - LOCATED ON CUT OR FILL SLOPES ON THE SIDE OF THE LID
-  NATURAL AREAS—MIXED NATIVE AND LANDSCAPE DECIDUOUS AND CONIFER TREES ON EDGES OF LID AND IN DISTURBED AREAS
-  LANDSCAPE AREA TREES—IRRIGATED PLANTING AREAS ADJACENT TO HIGH USE PARK AND BIKE TRAIL AREAS
-  LAWN AREA TREES—IRRIGATED AREAS WHERE TREES ARE PLANTED WITH GRASS.
-  STREET TREE/ OVERPASS TREES—IRRIGATED LANDSCAPE AREAS WHERE TREES ARE PLANTED IN BOXES ABOVE GRADE
-  SOIL SAMPLE POINTS.
-  PHOTO POINTS.

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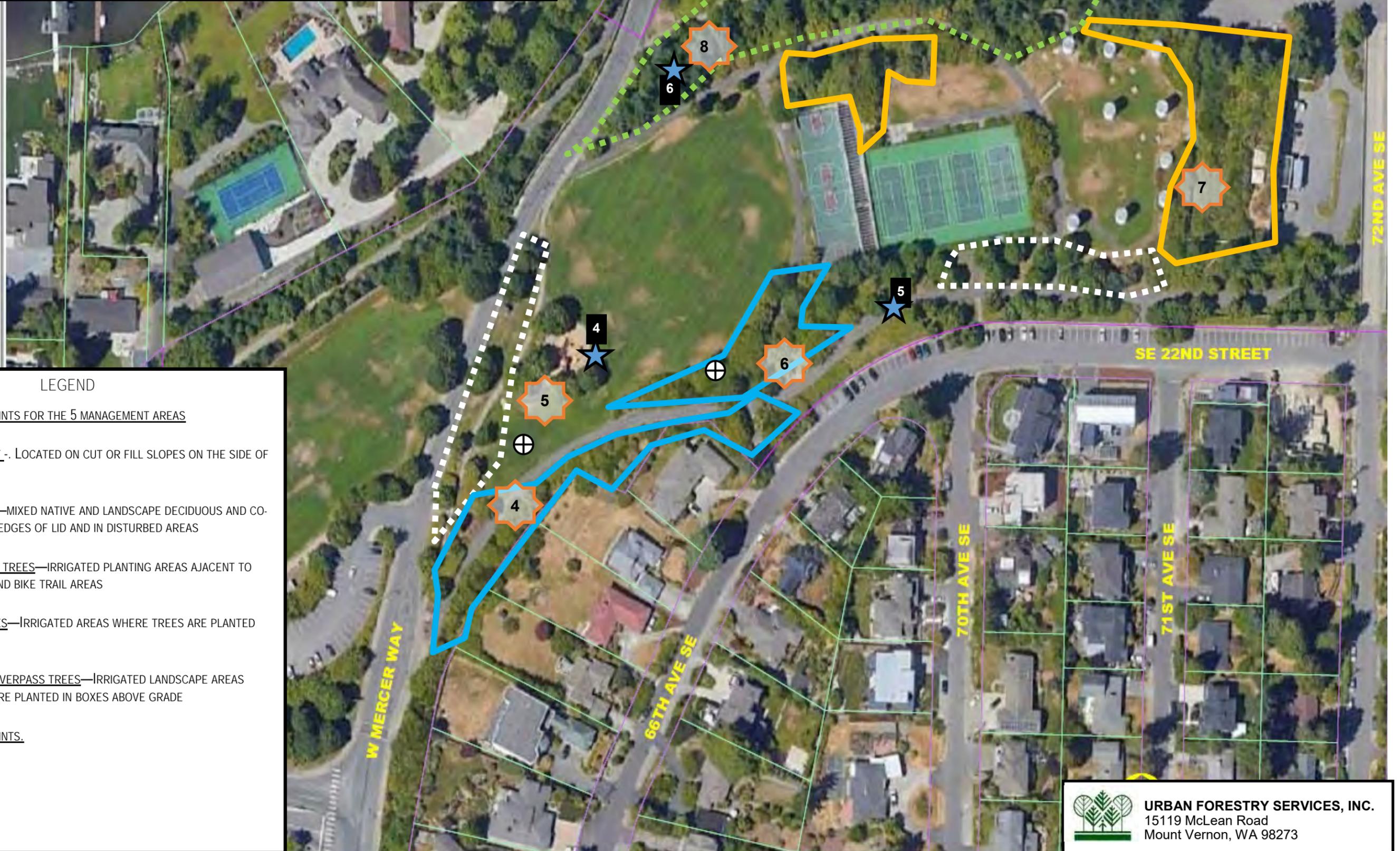
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Mercer Island Lid Tree and Landscape Assessment Red Line Map

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LEGEND

OBSERVATION POINTS FOR THE 5 MANAGEMENT AREAS

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-  **PHOTO POINTS.**

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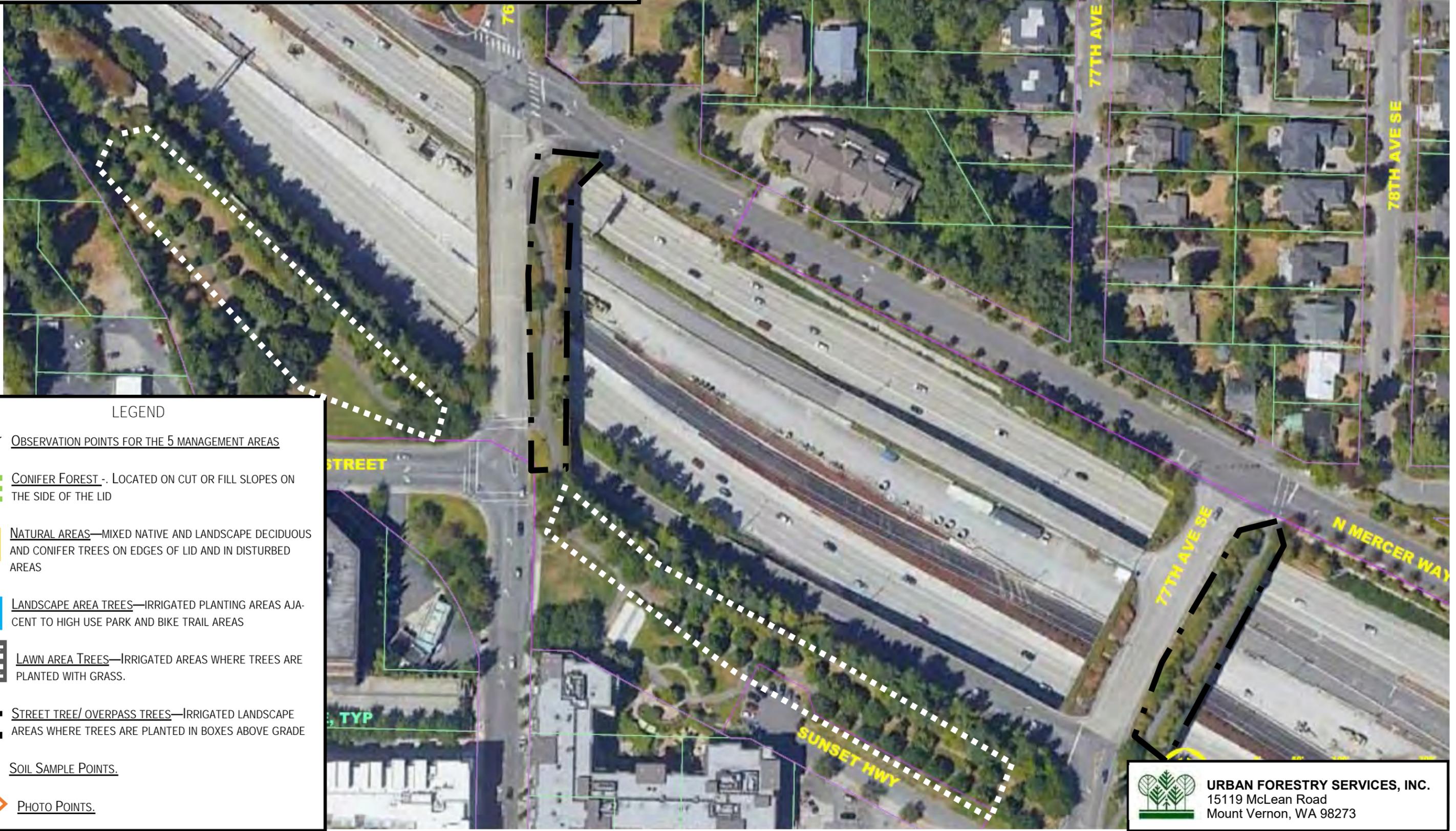
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-  PHOTO POINTS.



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Mercer Island Lid Tree and Landscape Assessment Red Line Map

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LEGEND

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OBSERVATION POINTS FOR THE 5 MANAGEMENT AREAS



CONIFER FOREST - LOCATED ON CUT OR FILL SLOPES ON THE SIDE OF THE LID



NATURAL AREAS—MIXED NATIVE AND LANDSCAPE DECIDUOUS AND CONIFER TREES ON EDGES OF LID AND IN DISTURBED AREAS



LANDSCAPE AREA TREES—IRRIGATED PLANTING AREAS ADJACENT TO HIGH USE PARK AND BIKE TRAIL AREAS



LAWN AREA TREES—IRRIGATED AREAS WHERE TREES ARE PLANTED WITH GRASS.



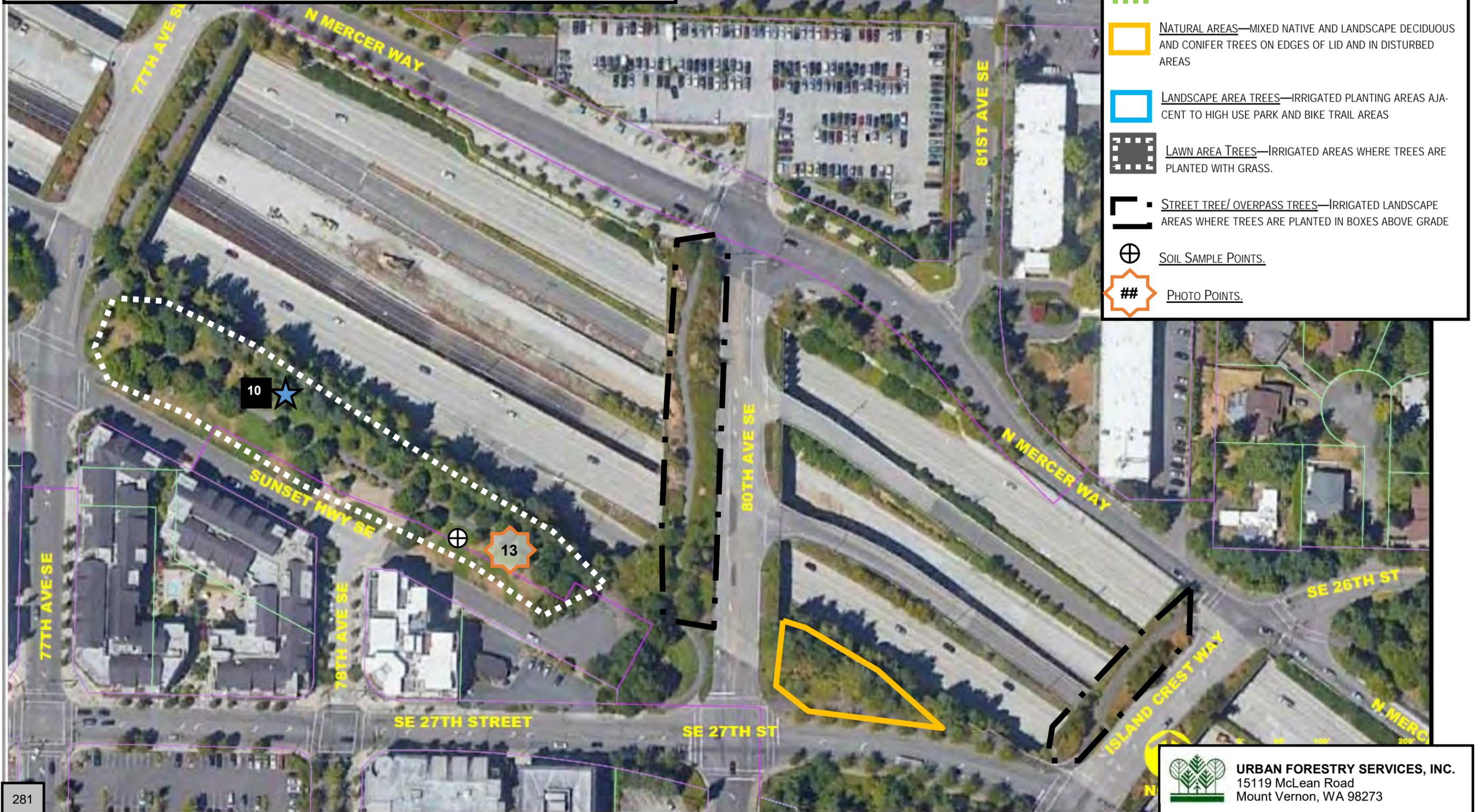
STREET TREE/ OVERPASS TREES—IRRIGATED LANDSCAPE AREAS WHERE TREES ARE PLANTED IN BOXES ABOVE GRADE



SOIL SAMPLE POINTS.



PHOTO POINTS.

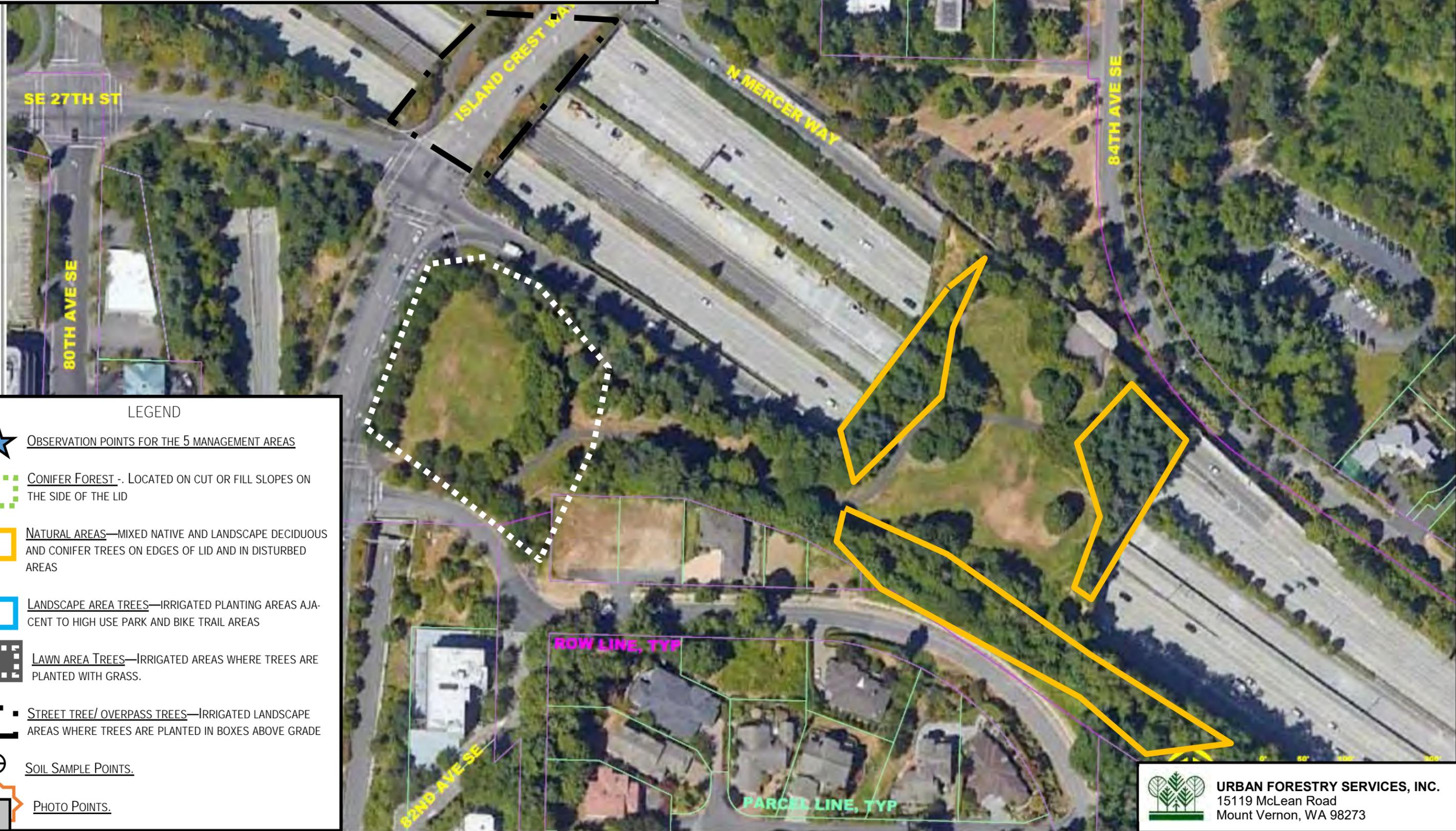


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Mercer Island Lid Tree and Landscape Assessment Red Line Map

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December 30, 2018

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LEGEND

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Mercer Island Lid Tree and Landscape Assessment Red Line Map

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LEGEND

-  OBSERVATION POINTS FOR THE 5 MANAGEMENT AREAS
-  CONIFER FOREST - LOCATED ON CUT OR FILL SLOPES ON THE SIDE OF THE LID
-  NATURAL AREAS—MIXED NATIVE AND LANDSCAPE DECIDUOUS AND CONIFER TREES
-  LANDSCAPE AREA TREES—IRRIGATED PLANTING AREAS ADJACENT TO HIGH USE PARK AND BIKE TRAIL AREAS
-  LAWN AREA TREES—IRRIGATED AREAS WHERE TREES ARE PLANTED WITH GRASS.
-  STREET TREE/ OVERPASS TREES—IRRIGATED LANDSCAPE AREAS WHERE TREES ARE PLANTED IN BOXES ABOVE GRADE
-  SOIL SAMPLE POINTS.
-  PHOTO POINTS.

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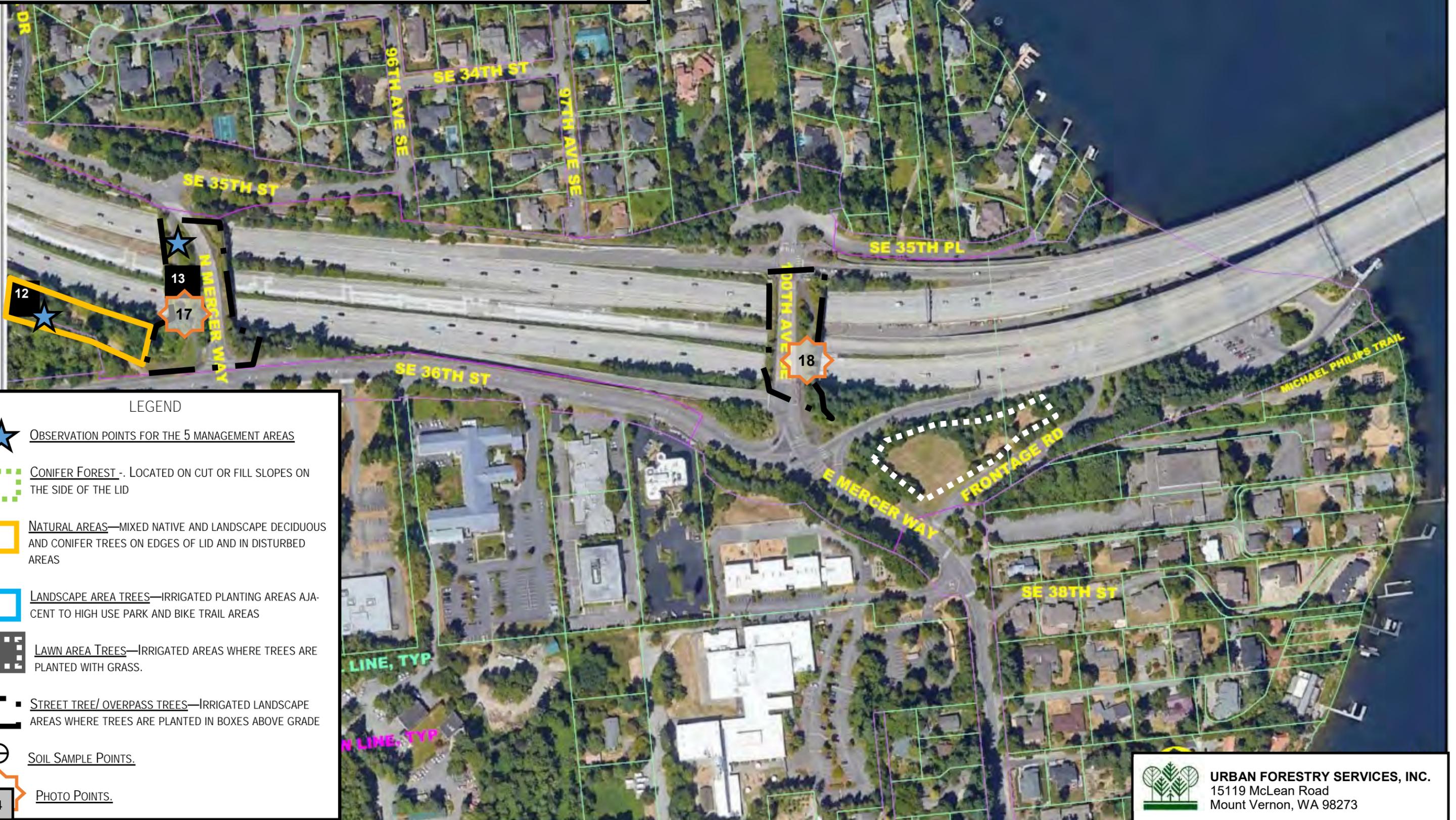
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15119 McLean Road
Mount Vernon, WA 98273

Mercer Island Lid Tree and Landscape Assessment Red Line Map

Map 8 of 8

December 30, 2018

Item 8.
8



LEGEND

-  OBSERVATION POINTS FOR THE 5 MANAGEMENT AREAS
-  CONIFER FOREST - LOCATED ON CUT OR FILL SLOPES ON THE SIDE OF THE LID
-  NATURAL AREAS—MIXED NATIVE AND LANDSCAPE DECIDUOUS AND CONIFER TREES ON EDGES OF LID AND IN DISTURBED AREAS
-  LANDSCAPE AREA TREES—IRRIGATED PLANTING AREAS ADJACENT TO HIGH USE PARK AND BIKE TRAIL AREAS
-  LAWN AREA TREES—IRRIGATED AREAS WHERE TREES ARE PLANTED WITH GRASS.
-  STREET TREE/ OVERPASS TREES—IRRIGATED LANDSCAPE AREAS WHERE TREES ARE PLANTED IN BOXES ABOVE GRADE
-  SOIL SAMPLE POINTS.
-  PHOTO POINTS.

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APPENDIX D

RECOMMENDED PLANT LIST

Appendix D: Recommended Plant List

October 8, 2019

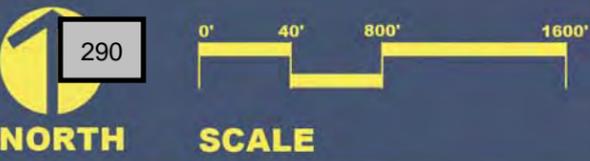
| Type | Species | Common name |
|--------|--|---|
| Trees | <p><i>Acer circinatum</i> <i>Acer griseum</i> <i>Alnus rubra</i> <i>Amelanchier alnifolia</i> <i>Arbutus unedo</i> <i>Cercis spp.</i> <i>Clerodendrum trichotomum</i> <i>Cornus mas</i> <i>Cotinus coggygria</i> <i>Cotinus obovatus</i> <i>Ginkgo biloba</i> <i>Gleditsia triacanthos</i> <i>Gymnocladus dioicus</i> <i>Juniperus spp.</i> <i>Koelreuteria paniculate</i> <i>Laerstroemia x fauriei</i> <i>Laurus nobilis</i> <i>Metasequoia glyptostroboides</i> <i>Parrotia persica</i> <i>Picea engelmannii</i> <i>Pinus contorta</i> <i>Pinus strobus</i> <i>Pseudotsuga menziesii</i> <i>Sorbus spp.</i> <i>Styphnolobium japonicum</i> <i>Quercus spp.</i></p> | <p>Vine maple Paper bark Maple Red alder Saskatoon serviceberry Strawberry tree Redbud Harlequin gloryblower Cornelian cherry Smoketree American smoketree Maidenhair tree Honey locust Kentucky coffee tree Juniper Golden rain tree Crepe myrtle Sweet bay Dawn redwood Persian ironwood Engelmann spruce Shore pine White pine Douglas fir Mountain ash Japanese pagoda tree Oak tree</p> |
| Shrubs | <p><i>Arbutus unedo</i> <i>Abelia spp.</i> <i>Berberis spp.</i> <i>Buxus spp.</i> <i>Calluna vulgaris</i> <i>Caryopteris spp.</i> <i>Ceanothus</i> <i>Choisya ternata</i> <i>Cistus spp.</i></p> | <p>Strawberry tree Abelia Barberry Boxwood Heather Blue mist shrub California lilac Mexican orange Rockrose</p> |

| | | |
|-------------------|---|--|
| | <p> <i>Cotoneaster spp.</i> <i>Elaeagnus spp.</i> <i>Escallonia spp.</i> <i>Euonymus japonicus</i> <i>Fuchsia magellanica</i> <i>Garrya elliptica</i> <i>Gaultheria shallon</i> <i>Hebe spp.</i> <i>Holodiscus discolor</i> <i>Hydrangea quercifolia</i> <i>Juniperus spp.</i> <i>Kerria japonica</i> <i>Mahonia spp.</i> <i>Oemleria cerasiformis</i> <i>Philadelphus spp.</i> <i>Pittosporum spp.</i> <i>Potentilla spp.</i> <i>Rhododendron spp.</i> <i>Rhus spp.</i> <i>Ribes sanguineum</i> <i>Spiraea spp.</i> <i>Symphoricarpos spp.</i> <i>Vaccinium ovatum</i> <i>Viburnum tinus</i> <i>Viburnum opulus var. americanum</i> </p> | <p> Cotoneaster Silverberry Redclaws Japanese spindletree Hardy fuschia Silk tassel bush Salal Hebe Oceanspray Oakleaf hydrangea Juniper Japanese kerria Oregon grape Indian plum Mock orange Cheesewood Cinquefoil Rhododendron Sumac Red flowering currant Meadowsweet Snowberry Evergreen huckleberry Laurustinus American cranberrybush </p> |
| <p>Perennials</p> | <p> <i>Achillea millefolium</i> <i>Armeria maritima</i> <i>Artemisia spp.</i> <i>Aster spp.</i> <i>Bergenia spp.</i> <i>Echinops</i> <i>Euphorbia spp.</i> <i>Gaura spp.</i> <i>Geranium spp.</i> <i>Helianthemum spp.</i> <i>Hemerocallis spp.</i> <i>Lavandula spp.</i> <i>Perovskia atriplicifolia</i> <i>Rudbeckia</i> <i>Salvia spp.</i> <i>Teucrium chamaedrys</i> <i>Kniphofia spp.</i> </p> | <p> Yarrow Sea pink Wormwood Aster Pigsqueak Globe thistle Spurge Beeblossom Cranesbill Sunrose Daylily Lavender Russian sage Coneflower Sage Wall germander Red hot poker </p> |

| | | |
|-------------|---|---|
| Groundcover | <p><i>Arctostaphylos uva-ursi</i> <i>Ceratostigma plumbaginoides</i> <i>Cornus alba</i> <i>Cornus sericea</i> <i>Corylus cornuta</i> <i>Iberis spp.</i> <i>Pachysandra terminalis</i> <i>Polygonum affine</i> <i>Sedum spp.</i> <i>Senecio spp.</i> <i>Zauschneria spp.</i></p> | <p>Kinnikinnick Blue leadwood White dogwood Red osier dogwood Beaked hazelnut Candytuft Japanese spurge Persicaria affinis Stonecrop Ragwort California fuschia</p> |
| Grasses | <p><i>Helictotrichon sempervirens</i> <i>Luzula spp.</i> <i>Miscanthus spp.</i> <i>Nassella tenuissima</i> <i>Phormium spp.</i></p> | <p>Blue oat grass Luzula Silvergrass Mexican feather grass New Zealand flax</p> |
| Vines | <p><i>Clematis montana</i> <i>Wisteria spp.</i></p> | <p>Anemone clematis Wisteria</p> |

APPENDIX E

SOIL TEST RESULTS & LOCATIONS



PARCEL LINE, TYP

SE 22ND STREET

Soil Sample 7

Soil Sample 1

ROW LINE, TYP

I-90

W MERCER WAY

SE 24TH STREET

60TH AVE SE

61ST AVE SE

62ND AVE SE

63RD AVE SE

64TH AVE SE



NORTH



SCALE

PARCEL LINE, TYP

ROW LINE, TYP

Soil Sample 2B

Soil Sample 2

Soil Sample 3

Soil Sample 2A

Soil Sample 1

W MERCER WAY

66TH AVE SE

70TH AVE SE

71ST AVE SE

72ND AVE SE

SE 22ND STREET



Soil Sample 3

Soil Sample 4

72ND AVE SE

N MERCER WAY

76TH AVE SE

SE 22ND STREET

ROW LINE, TYP

71ST AVE SE

76TH AVE SE

PARCEL LINE, TYP

74TH AVE SE



Item 8.

76TH AVE SE

77TH AVE SE

78TH AVE SE

SE 24TH STREET

77TH AVE SE

N MERCER WAY

SUNSET HWY

ROW LINE, TYP

PARCEL LINE, TYP



NORTH



SCALE

Item 8.

SE 24TH STREET

ROW LINE, TYP

PARCEL LINE, TYP

81ST AVE SE

77TH AVE SE

N MERCER WAY

80TH AVE SE

N MERCER WAY

SUNSET HWY SE

77TH AVE SE

78TH AVE SE

Soil Sample 5

SE 26TH ST

SE 27TH STREET

SE 27TH ST

ISLAND CREST WAY

N MERCER



NORTH



SCALE

SE 27TH ST

SE 26TH ST

ISLAND CREST WAY

N MERCER WAY

84TH AVE SE

80TH AVE SE

SE 28TH ST

ISLAND CREST WAY

ROW LINE, TYP

82ND AVE SE

ROW LINE, TYP



NORTH



SCALE



PARCEL LINE, TYP

N MERCER WAY

90TH PL SE

SHOREWOOD DR

ROW LINE, TYP

N MERCER WAY

84TH AVE SE

W CONCORD WAY

SE 33RD PL

SHOREWOOD DR

FORTUNA DR





Soil Sample 6

PARCEL LINE, TYP

ROW LINE, TYP



NORTH



SCALE



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Item 8.

SOILTEST FARM CONSULTANTS - 11

2925 DRIGGS DR

Moses Lake , WA 98837

Laboratory #: S18-18583

Date Received: 9/24/2018

Grower: JIM HOWARD

Sampled By:

Field: 1

Customer Account #:

Customer Sample ID:

Soil Test Results

| | | | |
|-----------------|--------|-----------|------|
| Phosphorus | Bray | mg/kg | 66 |
| Potassium | NH4OAc | mg/kg | 69 |
| Boron | DTPA | mg/kg | 0.18 |
| Zinc | DTPA | mg/kg | 1.3 |
| Manganese | DTPA | mg/kg | 2.3 |
| Copper | DTPA | mg/kg | 0.9 |
| Iron | DTPA | mg/kg | 84 |
| Calcium | NH4OAc | meq/100g | 3.1 |
| Magnesium | NH4OAc | meq/100g | 1.4 |
| Sodium | NH4OAc | meq/100g | 0.06 |
| Lime Req | | Tons/Acre | 0.0 |
| Buffer pH | SMP | | 6.8 |
| Cation Exchange | CEC | meq/100g | 5.8 |
| Total Bases | NH4OAc | meq/100g | 4.7 |
| Base Saturation | NH4OAc | % | 81.2 |
| ESP | ESP | % | 1.0 |

pH 1:1 5.6 CaCl2 pH 4.7

E.C. 1:1 m.mhos/cm 0.04

Est Sat Paste E.C. m.mhos/cm 0.10

Effervescence

Lbs/Acre

Ammonium - N mg/kg 1.1 3

Organic Matter W.B. % 2.0 ENR: 39

| Depth inches | Nitrate-N mg/kg | Sulfate-S mg/kg | Moisture Inches |
|---------------|-----------------|-----------------|-----------------|
| 0 - 12 | 2.5 | 8 | 5 |
| Totals | 2.5 | 8 | 5 |

Sum of Tested N: 50 lbs/acre N

Other Tests:

Texture: 89.0 % Sand, 2.0 % Clay, 9.0 % Silt

Cation % of CEC: Ca = 53.9 Mg = 23.3 Na = 1 K = 3

USDA TEXTURE - SAND

Interpretation Guide

Fertilizer recommendations for

of NATIVE PLANTING after

Low Medium High

| | | | |
|------------|-------------|--|-------------------------|
| Nitrogen | 50 lbs/acre | | 40 lbs/acre of Nitrogen |
| Phosphorus | 66 mg/kg | | 0 lbs/acre of P2O5 |
| Potassium | 69 mg/kg | | 55 lbs/acre of K2O |
| Sulfur | 5 mg/kg | | 20 lbs/acre of Sulfur |
| Boron | 0.18 mg/kg | | 1 lbs/acre of Boron |
| Zinc | 1.3 mg/kg | | 0 lbs/acre of Zinc |
| Manganese | 2.3 mg/kg | | 0 lbs/acre of Mn |

299

We make every effort to provide an accurate analysis of your sample. For reasonable cause we will repeat tests, but because of factors beyond our control including procedures and the inherent variability of soil, our liability is limited to the price of the tests. Recommendations are to be used as general and should be modified for specific field conditions and situations. Note: "u" indicates that the element was analyzed for but not detected

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Reviewed by: KEB

List Cost: \$78.00



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Item 8.

SOILTEST FARM CONSULTANTS - 11

2925 DRIGGS DR

Moses Lake , WA 98837

Laboratory #: S18-18584

Date Received: 9/24/2018

Grower: JIM HOWARD

Sampled By:

Field: 2

Customer Account #:

Customer Sample ID:

Soil Test Results

| | | | |
|-----------------|--------|-----------|------|
| Phosphorus | Bray | mg/kg | 11 |
| Potassium | NH4OAc | mg/kg | 61 |
| Boron | DTPA | mg/kg | 0.33 |
| Zinc | DTPA | mg/kg | 2.1 |
| Manganese | DTPA | mg/kg | 1.5 |
| Copper | DTPA | mg/kg | 1.7 |
| Iron | DTPA | mg/kg | 142 |
| Calcium | NH4OAc | meq/100g | 4.1 |
| Magnesium | NH4OAc | meq/100g | 1.1 |
| Sodium | NH4OAc | meq/100g | 0.07 |
| Lime Req | | Tons/Acre | 0.0 |
| Buffer pH | SMP | | 6.9 |
| Cation Exchange | CEC | meq/100g | 8.4 |
| Total Bases | NH4OAc | meq/100g | 5.5 |
| Base Saturation | NH4OAc | % | 65.0 |
| ESP | ESP | % | 0.8 |

pH 1:1 6.0 CaCl2 pH 5.3

E.C. 1:1 m.mhos/cm 0.11

Est Sat Paste E.C. m.mhos/cm 0.29

Effervescence

Lbs/Acre

Ammonium - N mg/kg 1.9 6

Organic Matter W.B. % 2.6 ENR: 51

| Depth inches | Nitrate-N mg/kg | Sulfate-S mg/kg | Moisture Inches |
|---------------|-----------------|-----------------|-----------------|
| 0 - 12 | 8.0 | 26 | 11 |
| Totals | 8.0 | 26 | 11 |

Sum of Tested N: 83 lbs/acre N

Other Tests:

Texture: 88.0 % Sand, 2.0 % Clay, 10.0 % Silt

Cation % of CEC: Ca = 49 Mg = 13.3 Na = 0.8 K = 1.9

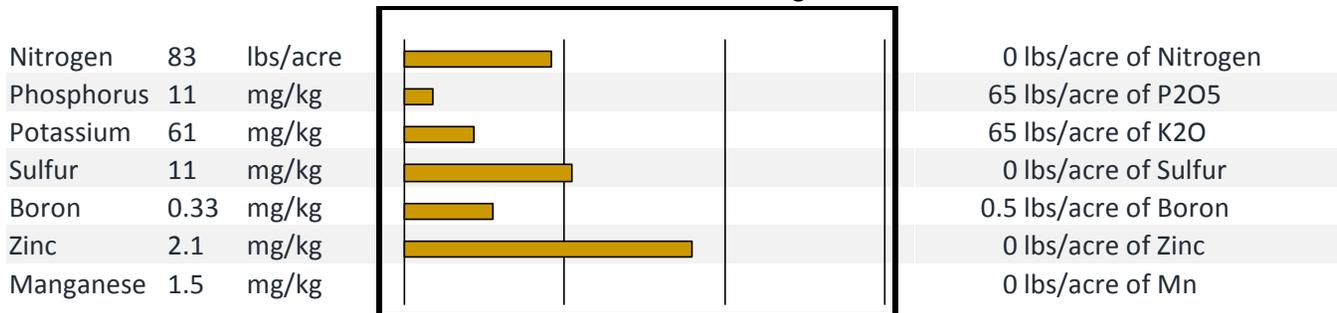
USDA TEXTURE - SAND

Interpretation Guide

Fertilizer recommendations for

of NATIVE PLANTING after

Low Medium High



300

We make every effort to provide an accurate analysis of your sample. For reasonable cause we will repeat tests, but because of factors beyond our control including sampling procedures and the inherent variability of soil, our liability is limited to the price of the tests. Recommendations are to be used as general and should be modified for specific field conditions and situations. Note: "u" indicates that the element was analyzed for but not detected

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Approved by: [Signature]

Reviewed by: KEB

List Cost: \$78.00



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2925 DRIGGS DR

Moses Lake , WA 98837

Laboratory #: S18-18585

Date Received: 9/24/2018

Grower: JIM HOWARD

Sampled By:

Field: 3

Customer Account #:

Customer Sample ID:

Soil Test Results

| | | | |
|-----------------|--------|-----------|------|
| Phosphorus | Bray | mg/kg | 47 |
| Potassium | NH4OAc | mg/kg | 150 |
| Boron | DTPA | mg/kg | 0.17 |
| Zinc | DTPA | mg/kg | 1.7 |
| Manganese | DTPA | mg/kg | 3.3 |
| Copper | DTPA | mg/kg | 1.0 |
| Iron | DTPA | mg/kg | 80 |
| Calcium | NH4OAc | meq/100g | 2.7 |
| Magnesium | NH4OAc | meq/100g | 0.9 |
| Sodium | NH4OAc | meq/100g | 0.05 |
| Lime Req | | Tons/Acre | 0.0 |
| Buffer pH | SMP | | 6.8 |
| Cation Exchange | CEC | meq/100g | 5.5 |
| Total Bases | NH4OAc | meq/100g | 4.0 |
| Base Saturation | NH4OAc | % | 73.3 |
| ESP | ESP | % | 0.9 |

| | | |
|--------------------|-----------|--------------|
| pH 1:1 | 5.3 | CaCl2 pH 4.9 |
| E.C. 1:1 | m.mhos/cm | 0.10 |
| Est Sat Paste E.C. | m.mhos/cm | 0.26 |
| Effervescence | | |

Lbs/Acre

| | | | |
|---------------------|-------|-----|---------|
| Ammonium - N | mg/kg | 1.0 | 3 |
| Organic Matter W.B. | % | 1.3 | ENR: 26 |

| Depth inches | Nitrate-N mg/kg | Sulfate-S mg/kg | Moisture Inches |
|---------------|-----------------|-----------------|-----------------|
| 0 - 12 | 8.4 | 27 | 6 |
| Totals | 8.4 | 27 | 6 |

Sum of Tested N: 56 lbs/acre N

Other Tests:

Texture: 88.0 % Sand, 2.0 % Clay, 10.0 % Silt

Cation % of CEC: Ca = 49.2 Mg = 16.2 Na = 0.9 K = 7

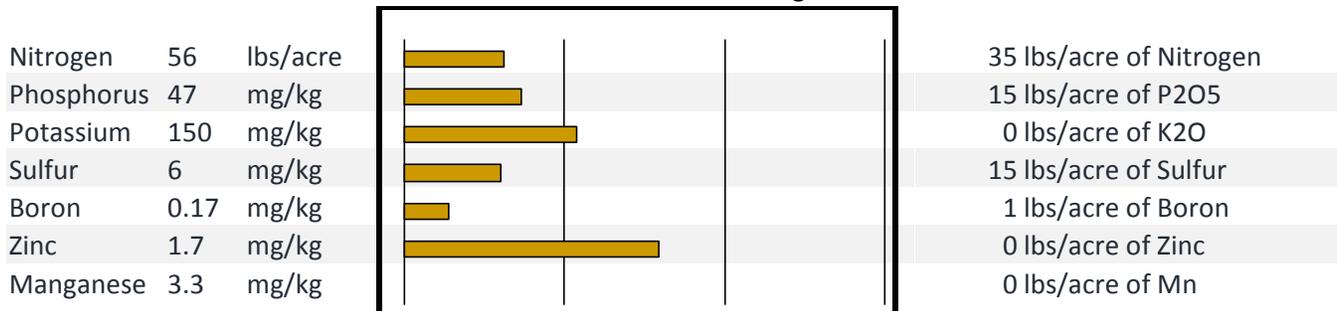
USDA TEXTURE - SAND

Interpretation Guide

Fertilizer recommendations for

of NATIVE PLANTING after

Low Medium High



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This is your Invoice #: S18-18585

Approved by: [Signature] Reviewed by: KEB

List Cost: \$78.00



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2925 DRIGGS DR

Moses Lake , WA 98837

Laboratory #: S18-18586

Date Received: 9/24/2018

Grower: JIM HOWARD

Sampled By:

Field: 4

Customer Account #:

Customer Sample ID:

Soil Test Results

| | | | |
|-----------------|--------|-----------|-------|
| Phosphorus | Bray | mg/kg | 19 |
| Potassium | NH4OAc | mg/kg | 45 |
| Boron | DTPA | mg/kg | 0.08 |
| Zinc | DTPA | mg/kg | 0.3 |
| Manganese | DTPA | mg/kg | 1.2 |
| Copper | DTPA | mg/kg | 0.4 |
| Iron | DTPA | mg/kg | 31 |
| Calcium | NH4OAc | meq/100g | 3.3 |
| Magnesium | NH4OAc | meq/100g | 1.4 |
| Sodium | NH4OAc | meq/100g | 0.06 |
| Lime Req | | Tons/Acre | 0.0 |
| Buffer pH | SMP | | 7.2 |
| Cation Exchange | CEC | meq/100g | 4.8 |
| Total Bases | NH4OAc | meq/100g | 4.8 |
| Base Saturation | NH4OAc | % | 100.7 |
| ESP | ESP | % | 1.2 |

pH 1:1 6.3 CaCl2 pH 5.3

E.C. 1:1 m.mhos/cm 0.13

Est Sat Paste E.C. m.mhos/cm 0.34

Effervescence

Lbs/Acre

Ammonium - N mg/kg 0.7 2

Organic Matter W.B. % 0.8 ENR: 16

| Depth inches | Nitrate-N mg/kg | Sulfate-S mg/kg | Moisture Inches |
|---------------|-----------------|-----------------|-----------------|
| 0 - 12 | 4.1 | 13 | 4 |
| Totals | 4.1 | 13 | 4 |

Sum of Tested N: 31 lbs/acre N

Other Tests:

Texture: 93.0 % Sand, 1.0 % Clay, 6.0 % Silt

Cation % of CEC: Ca = 68.6 Mg = 28.5 Na = 1.2 K = 2.4

USDA TEXTURE - SAND

Interpretation Guide

Fertilizer recommendations for

of NATIVE PLANTING after

Low Medium High

| | | | | |
|------------|------|----------|--|-------------------------|
| Nitrogen | 31 | lbs/acre | | 55 lbs/acre of Nitrogen |
| Phosphorus | 19 | mg/kg | | 60 lbs/acre of P2O5 |
| Potassium | 45 | mg/kg | | 75 lbs/acre of K2O |
| Sulfur | 4 | mg/kg | | 20 lbs/acre of Sulfur |
| Boron | 0.08 | mg/kg | | 1 lbs/acre of Boron |
| Zinc | 0.3 | mg/kg | | 4.5 lbs/acre of Zinc |
| Manganese | 1.2 | mg/kg | | 0 lbs/acre of Mn |

302

We make every effort to provide an accurate analysis of your sample. For reasonable cause we will repeat tests, but because of factors beyond our control including procedures and the inherent variability of soil, our liability is limited to the price of the tests. Recommendations are to be used as general and should be modified for specific field conditions and situations. Note: "u" indicates that the element was analyzed for but not detected

This is your Invoice #: S18-18586

Approved by: [Signature]

Reviewed by: KEB

List Cost: \$78.00



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Item 8.

SOILTEST FARM CONSULTANTS - 11

2925 DRIGGS DR

Moses Lake , WA 98837

Laboratory #: S18-18587

Date Received: 9/24/2018

Grower: JIM HOWARD

Sampled By:

Field: 5

Customer Account #:

Customer Sample ID:

Soil Test Results

| | | | |
|-----------------|--------|-----------|------|
| Phosphorus | Bray | mg/kg | 9 |
| Potassium | NH4OAc | mg/kg | 37 |
| Boron | DTPA | mg/kg | 0.21 |
| Zinc | DTPA | mg/kg | 2.1 |
| Manganese | DTPA | mg/kg | 2.1 |
| Copper | DTPA | mg/kg | 0.4 |
| Iron | DTPA | mg/kg | 101 |
| Calcium | NH4OAc | meq/100g | 1.1 |
| Magnesium | NH4OAc | meq/100g | 0.4 |
| Sodium | NH4OAc | meq/100g | 0.06 |
| Lime Req | | Tons/Acre | 0.0 |
| Buffer pH | SMP | | 7.1 |
| Cation Exchange | CEC | meq/100g | 4.2 |
| Total Bases | NH4OAc | meq/100g | 1.7 |
| Base Saturation | NH4OAc | % | 40.6 |
| ESP | ESP | % | 1.4 |

pH 1:1 5.4 CaCl2 pH 4.8

E.C. 1:1 m.mhos/cm 0.06

Est Sat Paste E.C. m.mhos/cm 0.16

Effervescence

Lbs/Acre

Ammonium - N mg/kg 0.9 3

Organic Matter W.B. % 1.6 ENR: 32

| Depth inches | Nitrate-N mg/kg | Sulfate-S mg/kg | Moisture Inches |
|---------------|-----------------|-----------------|-----------------|
| 0 - 12 | 2.7 | 9 | 5 |
| Totals | 2.7 | 9 | 5 |

Sum of Tested N: 44 lbs/acre N

Other Tests:

Texture: 93.0 % Sand, 2.0 % Clay, 5.0 % Silt

Cation % of CEC: Ca = 26.4 Mg = 10.5 Na = 1.4 K = 2.2

USDA TEXTURE - SAND

Interpretation Guide

Fertilizer recommendations for

of TURF after

Low Medium High

| | | | | |
|------------|------|----------|--|-------------------------|
| Nitrogen | 44 | lbs/acre | | 80 lbs/acre of Nitrogen |
| Phosphorus | 9 | mg/kg | | 100 lbs/acre of P2O5 |
| Potassium | 37 | mg/kg | | 115 lbs/acre of K2O |
| Sulfur | 5 | mg/kg | | 25 lbs/acre of Sulfur |
| Boron | 0.21 | mg/kg | | 1 lbs/acre of Boron |
| Zinc | 2.1 | mg/kg | | 0 lbs/acre of Zinc |
| Manganese | 2.1 | mg/kg | | 0 lbs/acre of Mn |

303

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This is your Invoice #: S18-18587

Accepted by: [Signature]

Reviewed by: KEB

List Cost: \$78.00



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SOILTEST FARM CONSULTANTS - 11

2925 DRIGGS DR

Moses Lake , WA 98837

Laboratory #: S18-18588

Date Received: 9/24/2018

Grower: JIM HOWARD

Sampled By:

Field: 6

Customer Account #:

Customer Sample ID:

Soil Test Results

| | | | |
|-----------------|--------|-----------|------|
| Phosphorus | Bray | mg/kg | 12 |
| Potassium | NH4OAc | mg/kg | 73 |
| Boron | DTPA | mg/kg | 0.11 |
| Zinc | DTPA | mg/kg | 1.0 |
| Manganese | DTPA | mg/kg | 1.5 |
| Copper | DTPA | mg/kg | 0.6 |
| Iron | DTPA | mg/kg | 52 |
| Calcium | NH4OAc | meq/100g | 2.9 |
| Magnesium | NH4OAc | meq/100g | 1.6 |
| Sodium | NH4OAc | meq/100g | 0.07 |
| Lime Req | | Tons/Acre | 0.0 |
| Buffer pH | SMP | | 7.1 |
| Cation Exchange | CEC | meq/100g | 5.7 |
| Total Bases | NH4OAc | meq/100g | 4.7 |
| Base Saturation | NH4OAc | % | 83.4 |
| ESP | ESP | % | 1.2 |

pH 1:1 6.0 CaCl2 pH 5.1

E.C. 1:1 m.mhos/cm 0.04

Est Sat Paste E.C. m.mhos/cm 0.10

Effervescence

Lbs/Acre

Ammonium - N mg/kg 1.6 5

Organic Matter W.B. % 1.6 ENR: 32

| Depth inches | Nitrate-N mg/kg | Sulfate-S mg/kg | Moisture Inches |
|---------------|-----------------|-----------------|-----------------|
| 0 - 12 | 1.2 | 4 | 5 |
| Totals | 1.2 | 4 | 5 |

Sum of Tested N: 41 lbs/acre N

Other Tests:

Texture: 90.0 % Sand, 2.0 % Clay, 8.0 % Silt

Cation % of CEC: Ca = 50.5 Mg = 28.4 Na = 1.2 K = 3.3

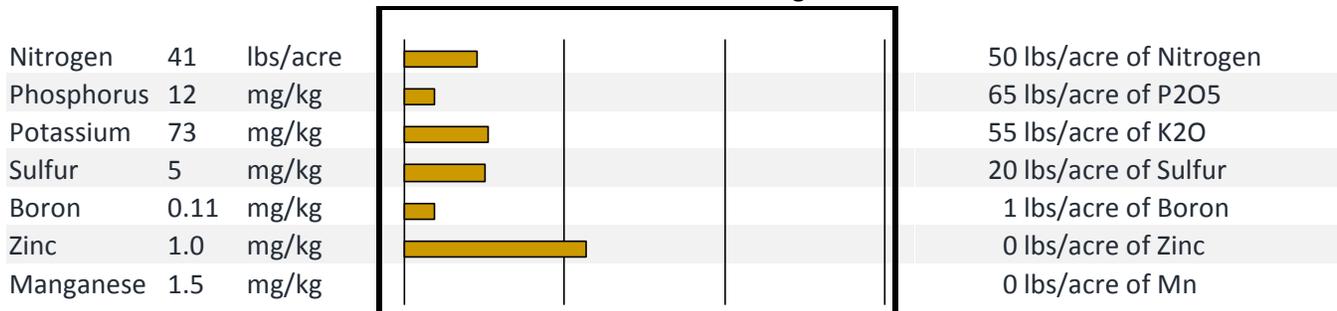
USDA TEXTURE - SAND

Interpretation Guide

Fertilizer recommendations for

of NATIVE PLANTING after

Low Medium High



304

We make every effort to provide an accurate analysis of your sample. For reasonable cause we will repeat tests, but because of factors beyond our control including procedures and the inherent variability of soil, our liability is limited to the price of the tests. Recommendations are to be used as general and should be modified for specific field conditions and situations. Note: "u" indicates that the element was analyzed for but not detected

This is your Invoice #: S18-18588

Approved by: [Signature]

Reviewed by: KEB

List Cost: \$78.00

Anna Heckman
Urban Forestry Services
15119 McLean Rd.
Mt. Vernon, WA 98273

Lab.#9543

10-Jan-19

SUBJECT: SOIL SAMPLE FOR SIEVE ANALYSIS

SAMPLE IDENTIFICATION: Heckman #1 (Pit Run) - submitted 1/04/19

Results of Mechanical Analysis: Particle distribution report.

Sieve Series Analysis: 100% of material passed a 1 inch mesh; this material was then classified by particle size analysis as follows:
Note: This organic composition of this material was less than 1% on a dry wt. basis.

| <u>MESH:</u> | <u>SIEVE SIZE</u> | <u>% PASSING</u> | <u>% Retained</u> | <u>CLASSIFICATION</u> |
|--------------|-------------------|------------------|-------------------|------------------------------|
| | 1/2 INCH | 69.86 | 30.14 | Rock |
| | 1/4 INCH | 47.85 | 22.01 | Gravel |
| #18 | 1.00 mm | 23.71 | 24.14 | Very coarse sand/fine gravel |
| #35 | 500 microns | 12.31 | 11.40 | Coarse sand |
| #60 | 250 microns | 6.31 | 6.00 | Medium sand |
| #140 | 100 microns | 2.47 | 3.84 | Fine sand |
| #270 | 50 microns | 1.30 | 1.17 | Silt |
| #500 | 25 microns | | 1.30 | clays |

Recovery on testing: 99.95% With all calculations being on a dry weight basis.

This particle distribution report was determined using U.S. Standard Sieve Series.

William F. Black, Agronomist

Anna Heckman
Urban Forestry Services
15119 McLean Rd.
Mt. Vernon, WA 98273

Lab.#9543

10-Jan-19

SUBJECT: SOIL SAMPLE FOR SIEVE ANALYSIS

SAMPLE IDENTIFICATION: Heckman # 2A/2B - submitted 1/04/19

Results of Mechanical Analysis: Particle distribution report.

Sieve Series Analysis: 100% of material passed a 1/2 inch mesh; this material was then classified by particle size analysis as follows:

Note: This material was mostly sandy silt with less than 2% organic matter on a dry wt. basis.

| <u>MESH:</u> | <u>SIEVE SIZE</u> | <u>% PASSING</u> | <u>% Retained</u> | <u>CLASSIFICATION</u> |
|--------------|-------------------|------------------|-------------------|------------------------------|
| | 1/4 INCH | 97.61 | 2.39 | Gravel |
| #18 | 1.00 mm | 95.20 | 2.41 | Very coarse sand/fine gravel |
| #35 | 500 microns | 82.33 | 12.87 | Coarse sand |
| #60 | 250 microns | 33.73 | 48.60 | Medium sand |
| #140 | 100 microns | 4.76 | 28.97 | Fine sand |
| #270 | 50 microns | 1.25 | 3.51 | Silt |
| #500 | 25 microns | | 1.25 | clays |

Recovery on testing: 99.92% With all calculations being on a dry weight basis.

This particle distribution report was determined using U.S. Standard Sieve Series.

William F. Black, Agronomist

APPENDIX F

BOLLARD STUDY

BOLLARD RECOMMENDATIONS MEMO

MERCER ISLAND I-90 TRAIL PROJECT

October 31, 2016

PURPOSE

The purpose of this memo is to document a study of the existing bollards currently located within the Mercer Island portion of the I-90 Trail, and provide recommendations for alterations to those bollards as needed. The study presented herein represents an applied analysis based on the bollard guidance concepts provided in the “Decision Guidelines for Bollards” technical memo.

INTRODUCTION

The I-90 Trail is a regional trail that was originally built by the Washington State Department of Transportation (WSDOT) with the support of multiple partners, including the City of Mercer Island. As it crosses the island, the trail generally parallels I-90, with portions of the trail following City streets, and some areas of the trail passing through several parks on the island. The trail is typically built as a smooth paved sidepath¹, with a width to accommodate different users and reduce user conflicts, though there are segments that exist on sidewalks as well.

At most street crossings and other access points to the trail, square wooden bollards were placed across the trail in order to control motorized vehicle access. These bollards are typically placed in groups of three, with one bollard on either edge of the paved trail and one in the middle, arranged perpendicularly across the trail, though some variations on this pattern do exist. Some of the original bollards have had reflective materials attached and/or have been painted white to increase visibility of the bollards.

¹ The terms “sidepath”, “shared use path”, and “sidewalk” are used in this memo to designate different forms that the I-90 Trail takes as it crosses Mercer Island. The term “sidewalk” is used in its commonly-recognizable form, identifying paved walkways immediately adjacent a roadway, while the term “sidepath” refers to a paved pathway whose alignment parallels a roadway but is physically separated from the roadway, either by barrier, lateral offset, or both. The term “shared use path” denotes any pathway that is formally designated for both wheeled and foot traffic, which can take the form of either a sidepath, a sidewalk, or a pathway that is independent of any roadway alignment. The term “trail” typically is used only for its generic meaning in this memo and does not indicate a specific physical form.

Since the time when the original I-90 Trail was constructed, the standard of practice and guidance related to bollard usage on trails has evolved. While bollards were typically installed as a matter of course during that time period, with little to no evaluative analysis involved, current practice is to limit the use of bollards to those locations where they serve an identifiable purpose.

The project team evaluated current published national and state bollard guidance, along with additional pertinent rules and best practice considerations, and developed guidance to govern the application and placement of bollards on the I-90 Trail across Mercer Island. That guidance was provided in a technical memo titled “Decision Guidelines for Bollards”, attached as Appendix A.

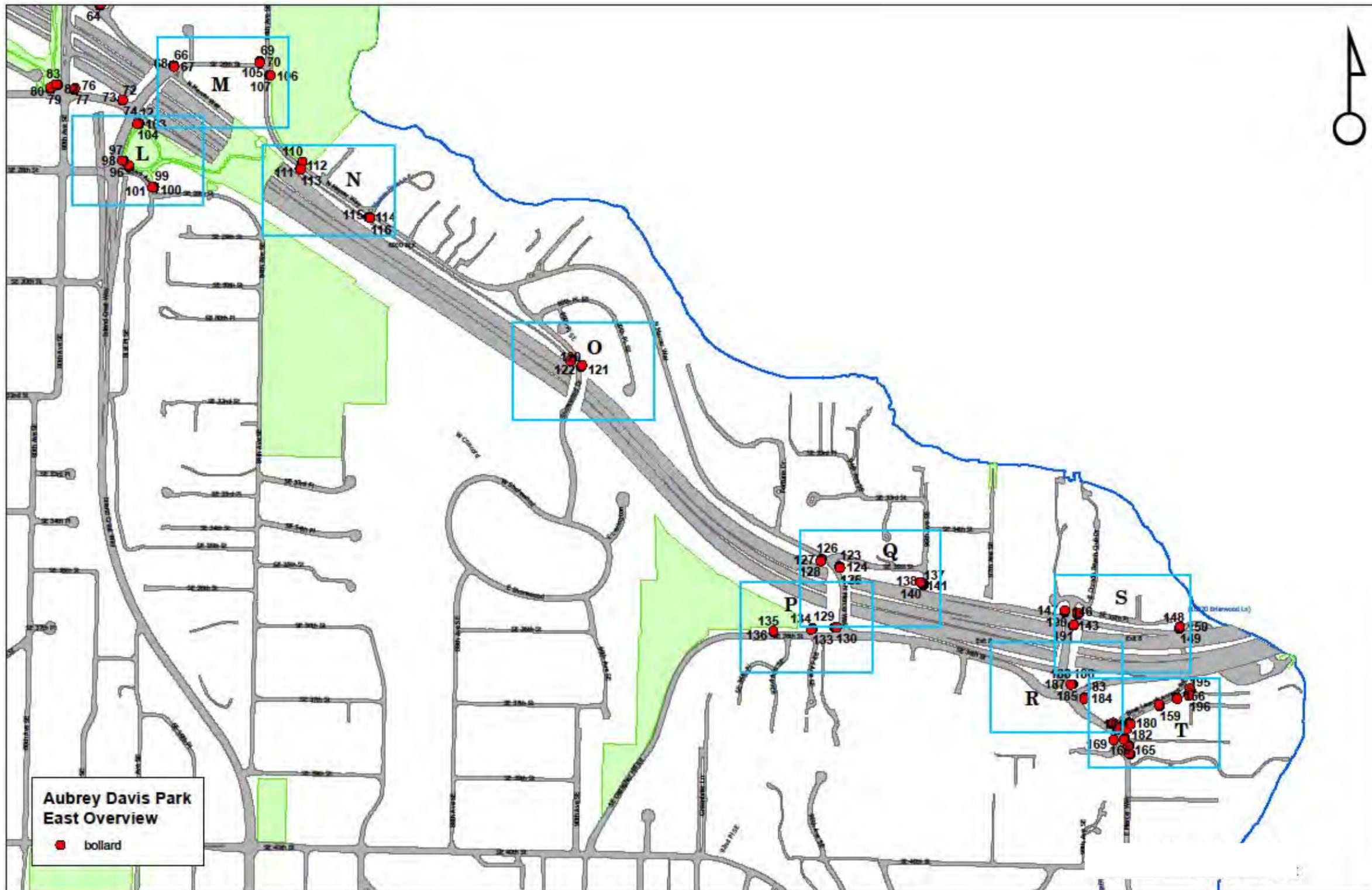
As discussed in the technical memo, the fundamental decision regarding the use of bollards is whether a bollard should or should not be placed at any given location. Based on current guidance and best practices, bollards should only be considered for use at locations where there is a demonstrable need for bollards to be employed. A demonstrable need would include a documented history of unauthorized intrusion at a given access point, or an identifiable characteristic of a particular location that could cause a driver to misinterpret a trail access point as a motor vehicle roadway.

This memo presents an analysis of all bollards currently located along the I-90 corridor across Mercer Island. The analysis includes the results of a field review of each bollard and bollard group, as well as recommendations for treatment of each bollard based on applying the guidance from the “Decision Guidelines for Bollards” technical memo.

REVIEW OF EXISTING BOLLARDS

The project team performed a review of all bollards along the I-90 corridor across Mercer Island. The Mercer Island Parks Department has created a bollard inventory and atlas for all bollards located along the various trails and pathways that follow I-90 across Mercer Island. The City’s atlas is attached as Appendix B; an additional bollard group numbering system has also been added to the City’s atlas to assist in identifying the bollards.

Mercer Island’s bollard atlas was used as a basis for tracking the bollard review and subsequent recommendations in order to maintain consistency with the City’s current bollard tracking system. The bollard inventory map from the City’s atlas is shown on the next two pages (also shown in Appendix B).



BOLLARD USE: PRESENCE VERSUS ABSENCE

As mentioned above and in the “Decision Guidelines for Bollards” tech memo, the primary consideration regarding the application of bollards is whether or not bollards should be employed at any given location. Because bollards by definition represent an obstruction in the pathway, they are not recommended for routine use. Bollards should only be used where there is a demonstrated need to address motor vehicle intrusion.

At many bollard locations across Mercer Island, the presence of bollards does not completely eliminate the possibility that a determined driver could drive onto the trail. At several locations there are opportunities for motor vehicles to easily access the trail either at the location of the bollards themselves or within a few hundred feet of a trail crossing. There is no documented history of repeated intentional vehicular intrusion at any location along the I-90 trail on Mercer Island, so none of the existing bollards meets that warrant to indicate use. As a result of these factors, the evaluation of the existing bollards focused primarily on the potential for accidental vehicular intrusion at each location.

Such evaluation consisted of reviewing the surrounding physical characteristics of each bollard location to reasonably determine the probability that a motor vehicle driver might mistake the trail for a roadway, and whether other features at the location would provide sufficient preventative measures to deter a driver from accidentally driving onto the trail.

For bollard locations where the probability of misinterpreting the trail as a roadway was identified as being low, the recommendation is to remove the bollards in order to eliminate the obstructions to trail users. If these locations also already include physical features that clearly deter drivers from accidentally driving onto the trail, then no further action is recommended. These existing features include such things as the presence of a sidewalk between the end of the trail and the roadway, curbing that clearly demarcates the trail separation from the roadway, or other similar features. If these locations do not include such features, then the recommendation includes suggestions to include design features other than bollards.

EXISTING BOLLARD FEATURES

The team evaluated all 196 bollards identified by the City along the I-90 corridor, which are arranged in 66 bollard groups. The team evaluated each bollard by reviewing photographs and mapping information provided by the City of Mercer Island, and location imagery available via Google Street View. Bollards that required more detailed observation were reviewed by on-site field review.

All of the existing bollards except one are of the square-section wood post type, typically measuring 8 inches square (8 inches by 8 inches) and approximately 32 inches tall. The majority of bollards were placed in groups, with the vast majority of groups consisting of 3 bollards. There are also several groups of bollards that have either 2 or 4 bollards. The typical bollard spacing is generally around 5 feet, measured between the near inside faces of adjacent posts, though this spacing varies. In some locations bollards that previously existed have been removed prior to this study (e.g. previous bollards have been removed in the pathway adjacent both bollard 57 and bollard 58).

In many instances the center bollard in bollard groups is removable in order to provide maintenance vehicle access when needed. The removable bollards are frequently set into a receiving square sleeve inset into the pavement, and are held in place by a simple padlock hasp. In other locations removable bollards are held in place by other methods. In most cases there are no locks used, so many removable bollards are not locked in place. In most locations there are also no caps to cover the open receiving holes when these bollards are removed.

In most locations the center bollards, both removable and permanent, are set into a cast concrete footing approximately 2 feet in diameter if round (see photo), or approximately 2 feet square. The size and shape of these footings varies. Most bollards that are not in the center of a pathway are simply embedded into the soil or asphalt pavement, though some are also embedded in concrete footings. Those that appear to be embedded into the soil or pavement may also have concrete footings below ground.

In most locations there are no pavement markings accompanying the bollard installations. In some locations along the primary trail route there are white pavement stripes that lead into the center bollard of a bollard group. These stripes are single white stripes that lead into and terminate at the bollard.

In some locations along the primary trail route some bollards have had reflective materials added to them, and some bollards have been partially painted white.



Representative Removable Bollard on Mercer Island

Removable bollard (#60) in the center position of a group of bollards.

RECOMMENDATIONS FOR EXISTING BOLLARDS

As discussed above, bollards should only be used where there is a demonstrated need to address motor vehicle intrusion. Furthermore, as discussed in the “Decision Guidelines for Bollards” tech memo, bollards should never be used as a means to divert or slow path traffic. When it is not entirely clear whether to install a bollard at a location in question, it is better to err on the side of caution and not install a bollard.

Additionally, alternate methods of restricting motor vehicle access should be considered prior to any bollard use. Conventional alternate methods in lieu of bollards could include signage, landscaped medians, or targeted enforcement; however, other methods can be equally or more effective, which

might include special mountable curbing that can be easily crossed over by bicycle but provides a demarcation of a street/trail point of intersection, alternate pavement types (varying colors or textures), pavement markings, etc. to identify the beginning of a pathway.

In reviewing the existing bollards, the team found that at many locations the bollards' primary function is not strictly intrusion prevention, but is instead a treatment that indicates locations where the trail meets a roadway. The bollards may provide a visual cue to trail users to anticipate the roadway crossing. In some cases it appears that the bollards serve no specific purpose other than to perhaps maintain a consistent application of the bollard treatment to indicate the location or routing of the trail. This latter treatment appears in locations where the trail is part of the street sidewalk network, and in at least one location where the trail follows a maintenance access road for a short distance. At a number of locations bollards are placed at trail-to-trail intersections, well away from any motor vehicle access points.

For locations where the bollards' function is primarily to either demarcate the presence of a street or trail-to-trail intersection, or to assist trail users in identifying the trail routing, current state of the practice would suggest that the use of bollards is not a good method of performing either of those functions.

After completing the bollard review, the team found that at all locations where bollards are used within the paved trail corridor, there is either a very low possibility of accidental intrusion by cars, or there is an opportunity to use alternative methods of preventing intrusion other than by the use of bollards. The team found no locations within the paved trail corridor where bollards would be considered a necessity. There was one location where the bollards are not on the paved trail corridor and appear to have been placed to control access into an unpaved WSDOT maintenance access road (bollards 132 through 134).

The table on the following pages provides specific recommendations for each bollard and bollard group. As shown in the recommendation table, the team recommends removal of all bollards except for bollards 132, 133, and 134. In some locations the function of the removed bollards would potentially be replaced by alternate treatments such as pavement markings, additional signage, additional curbing, etc. However, in most cases the bollards can be removed with no further revisions needed other than to repair the pavement where the bollards were located.

For the few locations where alternate treatments are suggested, the team intended to address the two separate functions mentioned above, which were previously served by the bollards. The first function is to ensure that the connection point of a pathway would not accidentally be misinterpreted as a roadway connection by drivers passing along the connecting roadway. The second function is to ensure that users of the pathway (primarily cyclists) who are approaching the point of intersection are given warning that there is an intersection ahead. In each location where an alternative treatment was recommended, the intent was to address either one or the other of these two potential issues. All of the alternate treatments are intended to address both issues, but in general the "Alternate 2" treatment presents a stronger visual cue for addressing the first issue, while "Alternate 3" presents sufficient markings for addressing the second issue.

Representational figures of the potential alternate treatments are provided in Appendix C. Note that these figures are intended for discussion purposes only. Each location where an alternate treatment might be applied would require an individual design at a future time.

| BOLL. NO. | BOLL. GROUP | ATLAS PAGE | RECOMMENDATION | REVIEW TYPE | NOTES |
|-----------|-------------|------------|--------------------------------------|---------------------------|---|
| 1 | 1001 | A | Remove/replace with Alt. 2 treatment | Google Earth | Add optional lane edge stripe ("No Motor Vehicles" sign already present) |
| 2 | 1001 | A | Remove/replace with Alt. 2 treatment | Google Earth | Add optional lane edge stripe ("No Motor Vehicles" sign already present) |
| 3 | 1001 | A | Remove/replace with Alt. 2 treatment | Google Earth | Add optional lane edge stripe ("No Motor Vehicles" sign already present) |
| 4 | 1002 | C | Remove | Pictures and data from MI | At crosswalk |
| 5 | 1002 | C | Remove | Pictures and data from MI | At crosswalk |
| 6 | 1002 | C | Remove | Pictures and data from MI | At crosswalk |
| 7 | 1003 | C | Remove | Google Earth | Trail entry behind sidewalk |
| 8 | 1003 | C | Remove | Google Earth | Trail entry behind sidewalk |
| 9 | 1003 | C | Remove | Google Earth | Trail entry behind sidewalk |
| 10 | 1004 | D | Remove | Google Earth | Trail entry behind sidewalk |
| 11 | 1004 | D | Remove | Google Earth | Trail entry behind sidewalk |
| 12 | 1004 | D | Remove | Google Earth | Trail entry behind sidewalk |
| 13 | 1005 | D | Remove/replace with Alt. 3 treatment | Google Earth | No optional components necessary. Trail connects to back of sidewalk. |
| 14 | 1005 | D | Remove/replace with Alt. 3 treatment | Google Earth | No optional components necessary. Trail connects to back of sidewalk. |
| 15 | 1005 | D | Remove/replace with Alt. 3 treatment | Google Earth | No optional components necessary. Trail connects to back of sidewalk. |
| 16 | 1006 | E | Remove | Google Earth | Trail entry behind sidewalk |
| 17 | 1006 | E | Remove | Google Earth | Trail entry behind sidewalk |
| 18 | 1006 | E | Remove | Google Earth | Trail entry behind sidewalk |
| 19 | 1007 | E | Remove/replace with Alt. 2 treatment | Field visit | Add optional lane edge stripe & "No Motor Vehicles" sign |
| 20 | 1007 | E | Remove/replace with Alt. 2 treatment | Field visit | Add optional lane edge stripe & "No Motor Vehicles" sign |
| 21 | 1007 | E | Remove/replace with Alt. 2 treatment | Field visit | Add optional lane edge stripe & "No Motor Vehicles" sign |
| 22 | 1008 | E | Remove | Field visit | Access from parking lot, "No Motor Vehicles" sign already present |
| 23 | 1008 | E | Remove | Field visit | Access from parking lot, "No Motor Vehicles" sign already present |
| 24 | 1008 | E | Remove | Field visit | Access from parking lot, "No Motor Vehicles" sign already present |
| 25 | 1009 | H | Remove | Pictures and data from MI | At crosswalk |
| 26 | 1009 | H | Remove | Pictures and data from MI | At crosswalk |
| 27 | 1009 | H | Remove | Pictures and data from MI | At crosswalk |
| 28 | 1010 | F | Remove/replace with Alt. 3 treatment | Field visit | Add optional lane edge stripe (dead end street) |
| 29 | 1010 | F | Remove/replace with Alt. 3 treatment | Field visit | Add optional lane edge stripe (dead end street) |
| 30 | 1010 | F | Remove/replace with Alt. 3 treatment | Field visit | Add optional lane edge stripe (dead end street) |
| 31 | 1011 | G | Remove/replace with Alt. 3 treatment | Google Earth | No optional components necessary. Trail connects to back of sidewalk. |
| 32 | 1011 | G | Remove/replace with Alt. 3 treatment | Google Earth | No optional components necessary. Trail connects to back of sidewalk. |
| 33 | 1011 | G | Remove/replace with Alt. 3 treatment | Google Earth | No optional components necessary. Trail connects to back of sidewalk. |
| 34 | 1012 | G | Remove | Pictures and data from MI | At crosswalk |
| 35 | 1012 | G | Remove | Pictures and data from MI | At crosswalk |
| 36 | 1012 | G | Remove | Pictures and data from MI | At crosswalk |
| 37 | 1013 | A | Remove/replace with Alt. 3 treatment | Google Earth | Add optional lane edge stripe & "No Motor Vehicles" sign |
| 38 | 1013 | A | Remove/replace with Alt. 3 treatment | Google Earth | Add optional lane edge stripe & "No Motor Vehicles" sign |
| 39 | 1013 | A | Remove/replace with Alt. 3 treatment | Google Earth | Add optional lane edge stripe & "No Motor Vehicles" sign |
| 40 | 1014 | C | Remove | Pictures and data from MI | At crosswalk |
| 41 | 1014 | C | Remove | Pictures and data from MI | At crosswalk |
| 42 | 1014 | C | Remove | Pictures and data from MI | At crosswalk |
| 43 | 1015 | E | Remove/replace with Alt. 2 treatment | Field visit | Install at trail connection past driveway. Add optional lane edge stripe & "No Motor Vehicles" sign |
| 44 | 1015 | E | Remove/replace with Alt. 2 treatment | Field visit | Install at trail connection past driveway. Add optional lane edge stripe & "No Motor Vehicles" sign |
| 45 | 1015 | E | Remove/replace with Alt. 2 treatment | Field visit | Install at trail connection past driveway. Add optional lane edge stripe & "No Motor Vehicles" sign |
| 46 | 1015 | E | Remove/replace with Alt. 2 treatment | Field visit | Install at trail connection past driveway. Add optional lane edge stripe & "No Motor Vehicles" sign |
| 47 | 1016 | E | Remove/replace with Alt. 3 treatment | Field visit | Add optional lane edge stripe ("No Motor Vehicles" sign already present) |
| 48 | 1016 | E | Remove/replace with Alt. 3 treatment | Field visit | Add optional lane edge stripe ("No Motor Vehicles" sign already present) |
| 49 | 1016 | E | Remove/replace with Alt. 3 treatment | Field visit | Add optional lane edge stripe ("No Motor Vehicles" sign already present) |
| 50 | 1017 | E | Remove | Google Earth | Trail-to-trail connection |
| 51 | 1017 | E | Remove | Google Earth | Trail-to-trail connection |

Item 8.

| BOLL. NO. | BOLL. GROUP | ATLAS PAGE | RECOMMENDATION | REVIEW TYPE | NOTES |
|-----------|-------------|------------|--------------------------------------|---------------------------|---|
| 52 | 1017 | E | Remove | Google Earth | Trail-to-trail connection |
| 53 | 1018 | B | Remove/replace with Alt. 2 treatment | Field visit | Add optional lane edge stripe & "No Motor Vehicles" sign |
| 54 | 1018 | B | Remove/replace with Alt. 2 treatment | Field visit | Add optional lane edge stripe & "No Motor Vehicles" sign |
| 55 | 1018 | B | Remove/replace with Alt. 2 treatment | Field visit | Add optional lane edge stripe & "No Motor Vehicles" sign |
| 56 | 1019 | H | Remove | Google Earth | Located along back edge of sidewalk, perform no identifiable function |
| 57 | 1019 | H | Remove | Google Earth | Located along back edge of sidewalk, perform no identifiable function |
| 58 | 1019 | H | Remove | Google Earth | Located along back edge of sidewalk, perform no identifiable function |
| 59 | 1020 | J | Remove/replace with alt. treatment | Field visit | Place standard lane edge stripe along edge of roadway through curve |
| 60 | 1020 | J | Remove/replace with alt. treatment | Field visit | Place standard lane edge stripe along edge of roadway through curve |
| 61 | 1020 | J | Remove/replace with alt. treatment | Field visit | Place standard lane edge stripe along edge of roadway through curve |
| 62 | 1020 | J | Remove/replace with alt. treatment | Field visit | Place standard lane edge stripe along edge of roadway through curve |
| 63 | 1021 | J | Remove | Pictures and data from MI | Bollards located within sidewalk, perform no identifiable function |
| 64 | 1021 | J | Remove | Pictures and data from MI | Bollards located within sidewalk, perform no identifiable function |
| 65 | 1021 | J | Remove | Pictures and data from MI | Bollards located within sidewalk, perform no identifiable function |
| 66 | 1022 | M | Remove | Field visit | Bollards located within sidewalk, perform no identifiable function |
| 67 | 1022 | M | Remove | Field visit | Bollards located within sidewalk, perform no identifiable function |
| 68 | 1022 | M | Remove | Field visit | Bollards located within sidewalk, perform no identifiable function |
| 69 | 1023 | M | Remove | Pictures and data from MI | Bollards located within sidewalk, perform no identifiable function |
| 70 | 1023 | M | Remove | Pictures and data from MI | Bollards located within sidewalk, perform no identifiable function |
| 71 | 1023 | M | Remove | Pictures and data from MI | Bollards located within sidewalk, perform no identifiable function |
| 72 | 1024 | K | Remove | Pictures and data from MI | Bollards located within sidewalk, perform no identifiable function |
| 73 | 1024 | K | Remove | Pictures and data from MI | Bollards located within sidewalk, perform no identifiable function |
| 74 | 1024 | K | Remove | Pictures and data from MI | Bollards located within sidewalk, perform no identifiable function |
| 75 | 1025 | K | Remove | Pictures and data from MI | At crosswalk |
| 76 | 1025 | K | Remove | Pictures and data from MI | At crosswalk |
| 77 | 1025 | K | Remove | Pictures and data from MI | At crosswalk |
| 78 | 1026 | K | Remove | Pictures and data from MI | At crosswalk |
| 79 | 1026 | K | Remove | Pictures and data from MI | At crosswalk |
| 80 | 1026 | K | Remove | Pictures and data from MI | At crosswalk |
| 81 | 1027 | K | Remove | Pictures and data from MI | At crosswalk |
| 82 | 1027 | K | Remove | Pictures and data from MI | At crosswalk |
| 83 | 1027 | K | Remove | Pictures and data from MI | At crosswalk |
| 84 | 1028 | I | Remove | Pictures and data from MI | At crosswalk |
| 85 | 1028 | I | Remove | Pictures and data from MI | At crosswalk |
| 86 | 1028 | I | Remove | Pictures and data from MI | At crosswalk |
| 87 | 1029 | I | Remove | Pictures and data from MI | At crosswalk |
| 88 | 1029 | I | Remove | Pictures and data from MI | At crosswalk |
| 89 | 1029 | I | Remove | Pictures and data from MI | At crosswalk |
| 90 | 1030 | I | Remove | Pictures and data from MI | At crosswalk |
| 91 | 1030 | I | Remove | Pictures and data from MI | At crosswalk |
| 92 | 1030 | I | Remove | Pictures and data from MI | At crosswalk |
| 93 | 1031 | L | Remove | Field visit | At crosswalk |
| 94 | 1031 | L | Remove | Field visit | At crosswalk |
| 95 | 1031 | L | Remove | Field visit | At crosswalk |
| 96 | 1032 | L | Remove | Field visit | At crosswalk |
| 97 | 1032 | L | Remove | Field visit | At crosswalk |
| 98 | 1032 | L | Remove | Field visit | At crosswalk |
| 99 | 1033 | L | Remove | Pictures and data from MI | Bollards located behind sidewalk |
| 100 | 1033 | L | Remove | Pictures and data from MI | Bollards located behind sidewalk |
| 101 | 1033 | L | Remove | Pictures and data from MI | Bollards located behind sidewalk |
| 102 | 1034 | L | Remove | Pictures and data from MI | At crosswalk |

Item 8.

| BOLL. NO. | BOLL. GROUP | ATLAS PAGE | RECOMMENDATION | REVIEW TYPE | NOTES |
|-----------|-------------|------------|---------------------------------------|---------------------------|---|
| 103 | 1034 | L | Remove | Pictures and data from MI | At crosswalk |
| 104 | 1034 | L | Remove | Pictures and data from MI | At crosswalk |
| 105 | 1035 | M | Remove | Pictures and data from MI | At crosswalk |
| 106 | 1035 | M | Remove | Pictures and data from MI | At crosswalk |
| 107 | 1035 | M | Remove | Pictures and data from MI | At crosswalk |
| 108 | 1036 | N | Remove | Pictures and data from MI | At crosswalk |
| 109 | 1036 | N | Remove | Pictures and data from MI | At crosswalk |
| 110 | 1036 | N | Remove | Pictures and data from MI | At crosswalk |
| 111 | 1037 | N | Remove | Pictures and data from MI | At crosswalk |
| 112 | 1037 | N | Remove | Pictures and data from MI | At crosswalk |
| 113 | 1037 | N | Remove | Pictures and data from MI | At crosswalk |
| 114 | 1038 | N | Remove | Field visit | At crosswalk |
| 115 | 1038 | N | Remove | Field visit | At crosswalk |
| 116 | 1038 | N | Remove | Field visit | At crosswalk |
| 117 | 1039 | O | Remove/replace with Alt.: 3 treatment | Field visit | Add optional "No Motor Vehicles" sign |
| 118 | 1039 | O | Remove/replace with Alt.: 3 treatment | Field visit | Add optional "No Motor Vehicles" sign |
| 119 | 1039 | O | Remove/replace with Alt.: 3 treatment | Field visit | Add optional "No Motor Vehicles" sign |
| 120 | 1040 | O | Remove/replace with Alt.: 3 treatment | Field visit | No added optional components necessary ("No Motor Vehicles" sign already present) |
| 121 | 1040 | O | Remove/replace with Alt.: 3 treatment | Field visit | No added optional components necessary ("No Motor Vehicles" sign already present) |
| 122 | 1040 | O | Remove/replace with Alt.: 3 treatment | Field visit | No added optional components necessary ("No Motor Vehicles" sign already present) |
| 123 | 1041 | Q | Remove | Pictures and data from MI | At crosswalk |
| 124 | 1041 | Q | Remove | Pictures and data from MI | At crosswalk |
| 125 | 1041 | Q | Remove | Pictures and data from MI | At crosswalk |
| 126 | 1042 | Q | Remove | Field visit | Bollards located within sidewalk |
| 127 | 1042 | Q | Remove | Field visit | Bollards located within sidewalk |
| 128 | 1042 | Q | Remove | Field visit | Bollards located within sidewalk |
| 129 | 1043 | P | Remove | Field visit | At crosswalk |
| 130 | 1043 | P | Remove | Field visit | At crosswalk |
| 131 | 1043 | P | Remove | Field visit | At crosswalk |
| 132 | 1044 | P | Keep | Field visit | Bollards not located within paved trail, may have other purpose |
| 133 | 1044 | P | Keep | Field visit | Bollards not located within paved trail, may have other purpose |
| 134 | 1044 | P | Keep | Field visit | Bollards not located within paved trail, may have other purpose |
| 135 | 1045 | P | Remove | Field visit | Bollards located within sidewalk |
| 136 | 1045 | P | Remove | Field visit | Bollards located within sidewalk |
| 137 | 1046 | Q | Remove | Field visit | Trail-to-trail connection |
| 138 | 1046 | Q | Remove | Field visit | Trail-to-trail connection |
| 139 | 1047 | Q | Remove | Field visit | Trail connection from stub driveway |
| 140 | 1047 | Q | Remove | Field visit | Trail connection from stub driveway |
| 141 | 1047 | Q | Remove | Field visit | Trail connection from stub driveway |
| 142 | 1048 | S | Remove/replace with Alt.: 2 treatment | Field visit | No optional components necessary |
| 143 | 1048 | S | Remove/replace with Alt.: 2 treatment | Field visit | No optional components necessary |
| 144 | 1048 | S | Remove/replace with Alt.: 2 treatment | Field visit | No optional components necessary |
| 145 | 1049 | S | Remove/replace with Alt.: 2 treatment | Field visit | No optional components necessary |
| 146 | 1049 | S | Remove/replace with Alt.: 2 treatment | Field visit | No optional components necessary |
| 147 | 1049 | S | Remove/replace with Alt.: 2 treatment | Field visit | No optional components necessary |
| 148 | 1050 | S | Remove/replace with Alt.: 3 treatment | Field visit | Add optional "No Motor Vehicles" sign |
| 149 | 1050 | S | Remove/replace with Alt.: 3 treatment | Field visit | Add optional "No Motor Vehicles" sign |
| 150 | 1050 | S | Remove/replace with Alt.: 3 treatment | Field visit | Add optional "No Motor Vehicles" sign |
| 151 | 1051 | T | Remove/replace with alt. treatment | Google Earth | Place three 12" stripes (approx. 40' long) @ trail connection to street (also covers 1052 & 1066) |
| 152 | 1051 | T | Remove/replace with alt. treatment | Google Earth | Place three 12" stripes (approx. 40' long) @ trail connection to street (also covers 1052 & 1066) |
| 153 | 1051 | T | Remove/replace with alt. treatment | Google Earth | Place three 12" stripes (approx. 40' long) @ trail connection to street (also covers 1052 & 1066) |

Item 8.

| BOLL. NO. | BOLL. GROUP | ATLAS PAGE | RECOMMENDATION | REVIEW TYPE | NOTES |
|-----------|-------------|------------|---------------------------------------|---------------------------|---|
| 154 | 1052 | T | Remove/replace with alt. treatment | Google Earth | Place three 12" stripes (approx. 40' long) @ trail connection to street (also covers 1051 & 1066) |
| 155 | 1052 | T | Remove/replace with alt. treatment | Google Earth | Place three 12" stripes (approx. 40' long) @ trail connection to street (also covers 1051 & 1066) |
| 156 | 1052 | T | Remove/replace with alt. treatment | Google Earth | Place three 12" stripes (approx. 40' long) @ trail connection to street (also covers 1051 & 1066) |
| 157 | 1053 | T | Remove | Google Earth | Trail-to-trail connection |
| 158 | 1053 | T | Remove | Google Earth | Trail-to-trail connection |
| 159 | 1053 | T | Remove | Google Earth | Trail-to-trail connection |
| 160 | 1054 | T | Remove | Pictures and data from MI | At crosswalk |
| 161 | 1054 | T | Remove | Pictures and data from MI | At crosswalk |
| 162 | 1054 | T | Remove | Pictures and data from MI | At crosswalk |
| 163 | 1055 | T | Remove | Google Earth | Trail crossing is clear |
| 164 | 1055 | T | Remove | Google Earth | Trail crossing is clear |
| 165 | 1055 | T | Remove | Google Earth | Trail crossing is clear |
| 166 | 1056 | T | Remove | Google Earth | Trail crossing is clear |
| 167 | 1056 | T | Remove | Google Earth | Trail crossing is clear |
| 168 | 1056 | T | Remove | Google Earth | Trail crossing is clear |
| 169 | 1057 | T | Remove | Google Earth | At crosswalk |
| 170 | 1057 | T | Remove | Google Earth | At crosswalk |
| 171 | 1058 | R | Remove | Pictures and data from MI | At crosswalk |
| 172 | 1058 | R | Remove | Pictures and data from MI | At crosswalk |
| 173 | 1058 | R | Remove | Pictures and data from MI | At crosswalk |
| 174 | 1059 | R | Remove | Pictures and data from MI | At crosswalk |
| 175 | 1059 | R | Remove | Pictures and data from MI | At crosswalk |
| 176 | 1059 | R | Remove | Pictures and data from MI | At crosswalk |
| 177 | 1060 | R | Remove/replace with alt. treatment | Google Earth | Add crosswalk (connects to 1061 below) |
| 178 | 1060 | R | Remove/replace with alt. treatment | Google Earth | Add crosswalk (connects to 1061 below) |
| 179 | 1060 | R | Remove/replace with alt. treatment | Google Earth | Add crosswalk (connects to 1061 below) |
| 180 | 1061 | R | Remove/replace with alt. treatment | Google Earth | Add crosswalk (connects to 1060 above) |
| 181 | 1061 | R | Remove/replace with alt. treatment | Google Earth | Add crosswalk (connects to 1060 above) |
| 182 | 1061 | R | Remove/replace with alt. treatment | Google Earth | Add crosswalk (connects to 1060 above) |
| 183 | 1062 | R | Remove | Pictures and data from MI | At crosswalk |
| 184 | 1062 | R | Remove | Pictures and data from MI | At crosswalk |
| 185 | 1062 | R | Remove | Pictures and data from MI | At crosswalk |
| 186 | 1063 | R | Remove | Google Earth | At crosswalk |
| 187 | 1063 | R | Remove | Google Earth | At crosswalk |
| 188 | 1063 | R | Remove | Google Earth | At crosswalk |
| 189 | 1064 | S | Remove/replace with Alt.: 3 treatment | Field visit | No optional components necessary |
| 190 | 1064 | S | Remove/replace with Alt.: 3 treatment | Field visit | No optional components necessary |
| 191 | 1064 | S | Remove/replace with Alt.: 3 treatment | Field visit | No optional components necessary |
| 192 | 1065 | D | Remove/replace with Alt.: 2 treatment | Field visit | No optional components necessary |
| 193 | 1065 | D | Remove/replace with Alt.: 2 treatment | Field visit | No optional components necessary |
| 194 | 1065 | D | Remove/replace with Alt.: 2 treatment | Field visit | No optional components necessary |
| 195 | 1066 | T | Remove/replace with alt. treatment | Google Earth | Place three 12" stripes (approx. 40' long) @ trail connection to street (also covers 1051 & 1052) |
| 196 | 1066 | T | Remove/replace with alt. treatment | Google Earth | Place three 12" stripes (approx. 40' long) @ trail connection to street (also covers 1051 & 1052) |

Where bollards are removed it is important to properly repair the pavement where the bollard was located. If pavement patches are used in lieu of full pavement replacement, the patch surfacing material should be the same as the surrounding surfacing material (i.e. use cement concrete patches in cement concrete pavement, asphalt patches in asphalt pavement).

The team recognizes that there are many bollards evaluated by this memo that may lie within WSDOT right-of-way limits. As such, removal of those bollards would require close coordination with WSDOT.

A planning-level construction cost estimate is provided in Appendix D. This estimate provides for the estimated costs to remove the existing bollards, repair the pavement at those locations, and apply new treatments as recommended in the table above.

Example Bollard Removal Repair



At locations where bollards are removed, the existing bollard and any in-pavement structural support should be removed in their entirety and replaced by a full repair patch. The repair patch should be a continuous surface with the surrounding trail surface.

CONCLUSION

The I-90 Trail across Mercer Island currently uses bollards that were placed during the original construction of the trail in the early 1990s. The team reviewed all bollards to evaluate their need and purpose, in light of more recent bollard guidance and best practice. Current guidance indicates that bollards should only be used where there is a demonstrated need to address motor vehicle intrusion. Using that guidance, and based on a thorough review of all bollards located within the I-90 trail corridor on Mercer Island, the team recommends that most bollards be removed.

APPENDIX A

“Decision Guidelines for Bollards” Technical Memo

TECHNICAL MEMO

MERCER ISLAND I-90 TRAIL PROJECT

DECISION GUIDELINES FOR BOLLARDS

October 18, 2016

PURPOSE

The purpose of this memo is to provide recommended guidance governing the application and placement of bollards within the Mercer Island portion of the I-90 Trail.

INTRODUCTION

Bollards have long been used for a wide variety of purposes and functions. When placed in the public right-of-way, bollards can be used to establish traffic patterns, to delineate the separation of different types of spaces and uses, or be used as physical barriers. When used on public trails and pathways, bollards are typically used as a physical barrier to motorized vehicles, while at the same time allowing passage of foot traffic, bicycles, wheelchairs, baby strollers, etc. This type of bollard usage typically includes a removable bollard so that authorized motorized vehicles may bypass the barrier when needed.

This last type of bollard usage presents a conflict of functions that can create unwanted secondary concerns. While a bollard serves the positive function of preventing unauthorized vehicle access, it also represents an impediment to the intended users of the trail or pathway. Because of this, the positive aspects of preventing unauthorized vehicle access may not always outweigh the safety and usability concerns that might arise as a result of placing a bollard within the usable pathway cross-section. For the I-90 Trail across Mercer Island, the use of bollards along this section of trail will be evaluated against purpose, need, and national and state bollard guidance to determine where bollards should be used. This memo addresses bollards placed as a physical barrier to motor vehicles across a pathway. Bollards are not used as longitudinal separations between different types of spaces along the I-90 Trail and are therefore not addressed in this memo.

The I-90 Trail is one of the most heavily used trails in the King County regional trail system, averaging over 750,000 users per year on Mercer Island. At the time of this writing, the I-90 Trail is the only multi-use trail that crosses Lake Washington, and as such it provides bicycle and pedestrian access between Seattle, Mercer Island, and communities on the east side of the lake.

The I-90 Trail is a regional trail that was originally built with the support of multiple partners. While the portion of the I-90 Trail that crosses Mercer Island lies mostly within Washington State Department of

Transportation (WSDOT) right-of-way, it is maintained by the City of Mercer Island through a maintenance agreement. As it crosses the island, the trail generally parallels I-90, with portions of the trail following City streets across the island. Some areas of the trail pass through several parks on the island, and there are multiple locations of street crossings of the trail. The trail is typically built as a smooth paved sidepath¹, with a width to accommodate different users and reduce user conflicts, though there are segments that exist on sidewalks as well.

At most street crossings and other access points to the trail, square wooden bollards were placed across the trail access point in order to control motorized vehicle access to the paved trail. These bollards are typically placed in groups of three, with one bollard on either edge of the paved trail and one in the middle, arranged perpendicularly across the trail. Some of the original bollards have been reflectorized² and/or painted white, to increase visibility of the bollards.

Since the time that the original I-90 Trail was constructed, the standard of practice and guidance related to bollard usage on trails has evolved. While bollards were typically installed as a matter of course during that time period, with little to no evaluative analysis involved, current practice is to limit the use of bollards to those locations where there is a demonstrated need. The following memo sections will present current national and state bollard guidance, along with additional pertinent rules and best practice considerations, to develop a set of recommendations to inform the placement of bollards on the I-90 Trail across Mercer Island.

¹ The terms “sidepath”, “shared use path”, and “sidewalk” are used in this memo to designate different types of physical forms that the I-90 Trail takes as it crosses Mercer Island. The term “sidewalk” is used in its commonly-recognizable form, identifying paved walkways immediately adjacent a roadway, while the term “sidepath” refers to a paved pathway whose alignment parallels a roadway but is physically separated from the roadway, either by barrier, lateral offset, or both. The term “shared use path” denotes any pathway that is designated for both wheeled and foot traffic, which can take the form of either a sidepath, a sidewalk, or a pathway that is independent of any roadway alignment. The term “trail” typically is used only for its generic meaning in this memo and does not indicate a specific physical form.

² The terms “retroreflective” and “reflective”, and associated word forms, are used interchangeably in this memo, and refer to an object’s optical property in which most of the light that falls on that object is reflected back in the same direction from which it came. Virtually all commercially-produced materials and markers that are marketed as being “reflective” are technically “retroreflective”.

CURRENT NATIONAL AND STATE GUIDANCE

The current applicable guidance comes from the 2012 edition of the AASHTO Guide for the Development of Bicycle Facilities (“AASHTO Bike Guide”) and the current version of the WSDOT Design Manual.³

According to the 2012 AASHTO Bike Guide:

- The unauthorized use of trails by motor vehicles does occur on some trails. However, this unauthorized use should not be anticipated to occur at all trail locations. The AASHTO Bike Guide states that bollards should only be used in the case of a documented history of ‘unauthorized intrusion’. The routine use of bollards to limit access of motor vehicles to a shared-use path is therefore not recommended. Bollards are often ineffective at keeping out motor vehicles, and can create a permanent safety hazard for trail users.
- Bollards can present a safety problem for bicyclists.
- Bollards should not be installed on trails at locations where motor vehicles could easily bypass the bollard to access the trail on adjacent property.
- The Guide suggests considering the following three step strategy to restrict motor vehicle use prior to considering the installation of bollards:
 - Post a R5-3 (from the FHWA “Manual on Uniform Traffic Control Devices”, or “MUTCD”) “NO MOTOR VEHICLES” sign at trail/roadway crossings.
 - Design the path access to differentiate the path from motorized vehicle access. Landscaped medians are noted as a preferred method of restricting access (designating emergency vehicle access to secured access drives). Another strategy for emergency vehicle access is to use removable bollards.
 - Consider targeted enforcement if motor vehicle intrusion is an issue.
- If used, bollards should be marked with a retroreflectorized material on both sides or with appropriate object markers (see section 9B.26 of the 2009 MUTCD), and should provide sufficient clear width for people riding adult tricycles, bicycles towing trailers, and tandem bicycles to pass by the bollard without having to dismount as well as to serve people traveling with disabilities. AASHTO recommends usage of a single bollard placed in the center of the path, and notes that flexible delineator posts may be recommended to reduce the possibility of injuries.
- Bollard placement should ensure adequate sight distance for a person to be able to see an approaching bollard. To improve visibility, it recommends a bollard minimum height of 40 inches and a minimum diameter of 4 inches. Taller bollards can improve visibility. When more than one post (bollard) is used, an odd number spaced at 6 ft. is desirable. Two posts are not

³ *Bollard policies of other agencies in the region were reviewed and no additional guidance specific to bollard usage was available. While most agencies do have standard bollard types that are specified, there is little guidance available that describes any type of analytical bollard use approach. Other agencies in the region that were reviewed include King County, the City of Seattle, the City of Portland, the City of Bellevue, the City of Kirkland, the City of Redmond, the City of Tacoma, and the City of Vancouver, B.C.*

recommended as they direct people towards the center of the path, increasing the risk of collision.

- It also recommends bollards be set back a minimum of 30 feet from a roadway edge to allow bicyclists time to maneuver around the bollard before they must navigate the intersection crossing.
- It recommends the striping of an envelope around the obstruction (see Figure 1 from MUTCD below).

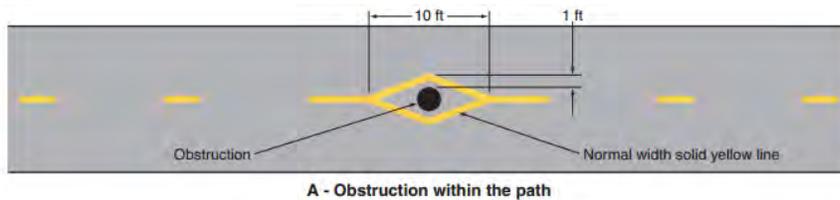


FIGURE 1: MUTCD FIGURE 9C-8

The WSDOT Design Manual provides further guidance on application and installation of bollards:⁴

- Similar to the AASHTO Guide, WSDOT does not recommend widespread use of bollards to restrict unauthorized access. WSDOT also prefers using landscaped islands to restrict motor vehicles by splitting the entryway (using low-growing, hardy plants that can still allow authorized vehicles to pass over).
- WSDOT recommends the use of reflective materials on the post, such as a band at the top and at the base. WSDOT also recommends using a contrasting striping pattern on the post.
- WSDOT recommends designing bollards to be uniform. WSDOT also suggests using removable bollards for emergency vehicle access.
- If bollards are needed, WSDOT also recommends using a single bollard placed in the center of the path to reduce confusion (and locating any needed additional bollards outside the path), and striping an envelope of space around bollards in accordance with the MUTCD guidance shown above. WSDOT states that if multiple bollards are needed, a minimum spacing of 5 feet between the edges of bollards' concrete footings should be used to provide a clear width for vehicles with coaxial wheels such as bicycle-towed trailers, wheelchairs, and adult tricycles.
- For cases where multiple bollards are used longitudinally along the path, locate multiple posts at least 20 feet apart, with the first post in line from each direction having stopping sight distance.
- "Do not use bollards to divert or slow path traffic." WSDOT assumes a 20 mph design speed for flat trail sections and a 30 mph design speed for sustained downhill sections.
- WSDOT recommends that designs make bollards clearly visible to path users, and requires an adequate stopping sight distance. "An ideal location for bollard placement is in a relatively straight area of the path where the post placement has the stopping sight distance given in Exhibit 1515-14A and 14B."

⁴ WSDOT Design Manual, Chapter 1515. For more detail, refer to Standard Plan M-9.60-00

BOLLARD PLACEMENT RECOMMENDATIONS

The primary considerations from national and state guidance related to bollard placement fall into three primary categories: safety, ADA accessibility, and maintenance accessibility. The information provided below organizes and summarizes bollard-specific guidance from the AASHTO Bike Guide, the MUTCD, and the WSDOT Design Manual, and also provides additional guidance that is emerging from current best practice in this field. The universal access requirements of the Americans with Disabilities Act (ADA) also influence certain bollard decisions.

SAFETY

Because bollards are often placed within the usable area of a trail, or on the outer edges of that usable area, they can present safety concerns to users of the trail, particularly for bicycles that travel at higher speeds than walkers or runners. At typical bicycle speeds, it is important to provide sufficient visual cues to alert the cyclist to the presence of the bollard, and to provide enough advance warning to allow the cyclist to adjust their speed or course as needed.

Safety concerns generally result from three aspects of bollard placement: locational properties of the bollard, trail characteristics at the bollard location, and physical characteristics of the bollard itself.

A. Location of Bollards

For any given location being considered for a bollard, the fundamental decision is whether a bollard should or should not be placed at that location. Generally, bollards can be an obstruction to path users and are not recommended for routine use. Bollards should never be used to divert or slow path traffic. When it is not entirely clear whether to install a bollard at a location in question, it is better to err on the side of caution and not install the bollard.

Alternate methods of restricting motor vehicle access should be considered prior to any bollard use. Alternate methods in lieu of bollards could include signage, landscaped medians, or targeted enforcement.

If it is demonstrated that bollards are needed to address motor vehicle intrusion, the placement should carefully consider safety trade-offs that may result. Bollard location should be designed to allow for sufficient visibility, provide clear space for trail users to navigate around them without dismounting their bicycle, and be set back from intersections.

Sight distance provided at each bollard should meet the requirements defined in the WSDOT Design Manual, Exhibits 1515-14A and -14B. The minimum number of bollards that can create the intended purpose at each location should be used. Bollards should be used in groups of odd numbers (e.g. 1, 3, or 5) such that the centerline of the trail leads to a centrally-placed bollard, in order to avoid conflicts with traffic traveling in opposite directions attempting to use the same gap between bollards. Unless there is a critical need, bollards should not be placed on the outside edges of the trail.

Spacing between bollards should ideally be at least 6 feet, with 5 feet considered as the functional minimum. Spacing between bollards is defined as the minimum clear space between adjacent bollards, not the distance between bollard centers. It should be noted that bollards set 8 feet apart or wider will not prevent motorized vehicles from passing; therefore, the effective range of bollard spacing is normally 5 to 7 feet.

Bollards set on trails at street crossings should be a minimum of 30 feet from the edge of roadway so the attention of the cyclist can focus on maneuvering around the bollard as a separate action from crossing the roadway, though the placement of the bollards should always meet sight distance criteria described above.

B. Trail Characteristics

The geometric characteristics of the trail at a potential bollard location may influence whether a bollard should be used. Conversely, if a bollard must be used but the trail characteristics are not favorable at that location, some changes to the trail geometry should be considered in concert with the bollard placement.

Bollards should never be placed in high-speed sections of trail. “High speed sections” in this context means any portion of a trail where bicycle speeds higher than 15 mph can be reasonably expected. Such sections would include locations at the ends of long downhill sections or long uninterrupted straight sections. If bollards are required in a section of trail that is currently a high-speed section, changes to the existing trail geometry should also be considered in order to slow riders ahead of encountering the bollards. Such changes might include adding chicanes to slow bicycle traffic; however, it is understood that any changes to trail geometry should provide for a gradual slowing of bicycle traffic rather than causing an abrupt change.

Artificial nighttime lighting and ambient daytime lighting are concerns at bollard locations. When considering nighttime lighting conditions, ideally bollards should be placed at locations with sufficient artificial lighting to allow trail users to see the bollard even when not using their own lighting. If there is no opportunity to locate bollards near artificial lighting, such as on trails where no artificial lighting is used, additional pavement markings or other warning devices should be considered in advance of the bollard location. When considering daytime lighting conditions, bollards should not be placed in locations where a sudden change in lighting conditions exists, such as at the entrances to tunnels or overpasses. It is important to also consider the changing shadow pattern throughout the day in these circumstances.

Pavement markings should be used on both sides of each bollard in order to provide advance warning to cyclists approaching the bollard. At a minimum, striping should adhere to the envelope striping as described in MUTCD Figure 9C-8. Additional striping may be advisable, such as centerline striping 25 feet on either side of the bollard, or gore striping if the bollard is located ahead of a split in the trail.

Very high trail ridership volumes may also indicate not using bollards. For trails with very high volumes of users, the presence of a number of bicyclists grouped together can shield the

approaching view of bollards for trailing riders. If bollards must be used in locations where groups of riders frequently pass together, it is recommended that additional advance warning be provided, such as additional signing or pavement markings. Taller bollards should also be considered so that trailing riders can see approaching bollards over the group of riders ahead, though special care should be taken in the placement of taller bollards due to the potential for bicycle handlebars to strike taller bollards (further details provided in the height guidance in the following section).

C. Physical Characteristics

Each bollard should have a minimum diameter of 4" (both circular/elliptical and polygonal sections). Bollards with a triangular section are not recommended since they do not present the same face to each direction of traffic. Although the AASHTO Bike Guide recommends a minimum height of 40 inches, it should be considered that bollards taller than about 30" (2.5 feet) will require that more space be provided between bollards since such bollards may not allow all bicycles' handlebars to pass over the top.

Bollards should be of a color and material that provides a strong visual contrast to the surrounding environment. Bollards should be retroreflectorized; the reflective portion of the bollard should cover the full width of the bollard, and should be a minimum of 4" in height on the bollard. Retroreflectivity should be provided around the full circumference of the bollard, even on those sides that do not face the direction of travel on the trail.

Internally-lit bollards should also adhere to all of the visibility requirements of non-lit bollards in order to provide the same level of functionality in case of power failure.

The use of non-rigid devices (i.e. flexible posts or bollards with a spring-hinge base) can be considered as an option to rigid devices, with the understanding that the use of non-rigid devices does not change or eliminate any of the requirements regarding the physical characteristics and location criteria of the bollards.

The physical characteristics of the bollards selected for use should be the same along a given trail corridor, in order to provide a consistent expectation for the trail user.

Removable bollards should be fully removable and not of the hinged "lay down" type, in order to prevent bicycle accidents while the bollard is lowered. No part of the receiver for the removable bollard should extend horizontally more than 2" from the edge of the bollard post at ground level, and should be entirely flush with the trail surface. There should be a permanently-affixed cap that covers the open hole created when the bollard is removed. All portions of the cover and receiver should be flush with the trail surface when the bollard is removed, and should be treated with a non-slip surfacing. When the cover is in place there should be no gaps greater than ¼" in any part of the cover or receiver. Ideally, covers that lock down are preferred, particularly in locations where bollards may be removed for long periods of time. Additional ADA requirements may apply (see below).

ADA CONSIDERATIONS

Due to the fact that the I-90 Trail is open for general public use, it falls under the accessibility requirements of the Americans with Disabilities Act (ADA). The ADA-related requirements are codified in rules promulgated by the Architectural and Transportation Barriers Compliance Board (“Access Board”). The I-90 Trail lies within public right-of-way, parts of which are independent of a street network and parts of which follow a parallel roadway. The I-90 Trail should meet the accessibility guidelines outlined in the Access Board’s current proposed PROWAG requirements and proposed Accessibility Guidelines for Shared Use Paths. Bollard placement should not reduce the ‘clear width’ required by federal accessibility guidance for safe passage of people with disabilities.

Because ADA requirements are evolving, and are codified by federal requirements for all facilities, not just trails, the specific ADA codes will not be enumerated here. Current ADA codes can be found on the United States Access Board’s website at <http://www.access-board.gov>

MAINTENANCE AND EMERGENCY VEHICLES

If the trail must be accessible to maintenance and emergency vehicles, it is recommended that bollards, if used, be flexible or removable. If landscaped medians are used to discourage unauthorized access, use low-growing native species plantings that could be driven over without damage in order to provide maintenance vehicle access.

CONCLUSION

The I-90 Trail across Mercer Island currently uses bollards that were placed during the original construction of the trail in the early 1990s. These bollards were ostensibly placed to prevent motor vehicle access to the trail. In the time since the bollards were placed, the state of the practice has evolved to the extent that the locations and type of bollard that exist on the trail can now be re-evaluated according to more recent bollard best practice. The guidance provided in this memo should be used to evaluate and recommend adjustments to the bollard layout along the trail, as well as the type of bollard to be used.

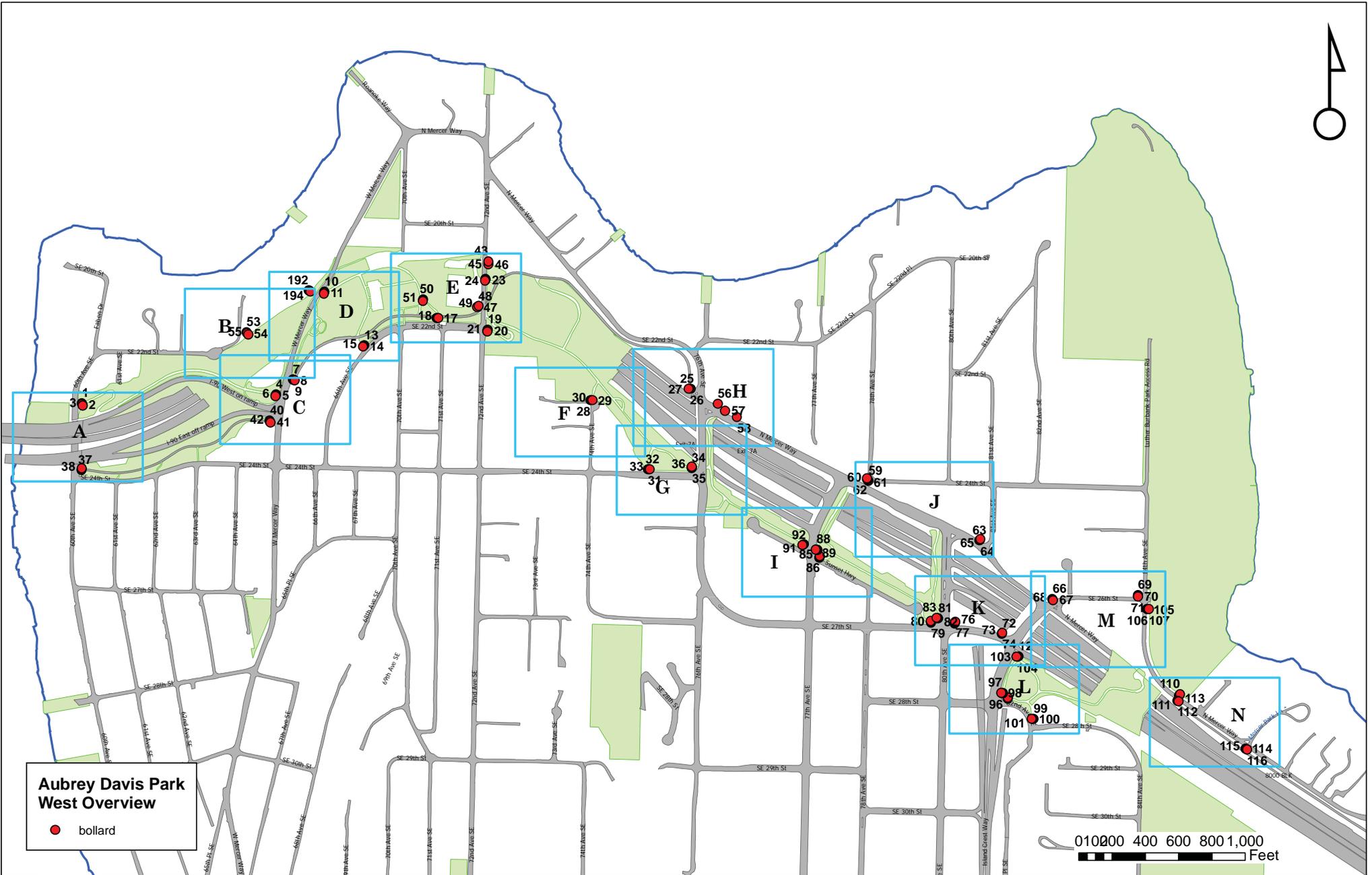
Best Practice



Per guidance in the 2012 AASHTO Bike Guide, Seattle is phasing out bollards and using other designs to enhance safety at roadway/trail crossings (Burke Gilman Trail).

APPENDIX B

City of Mercer Island Bollard Atlas
(with added bollard group numbers)

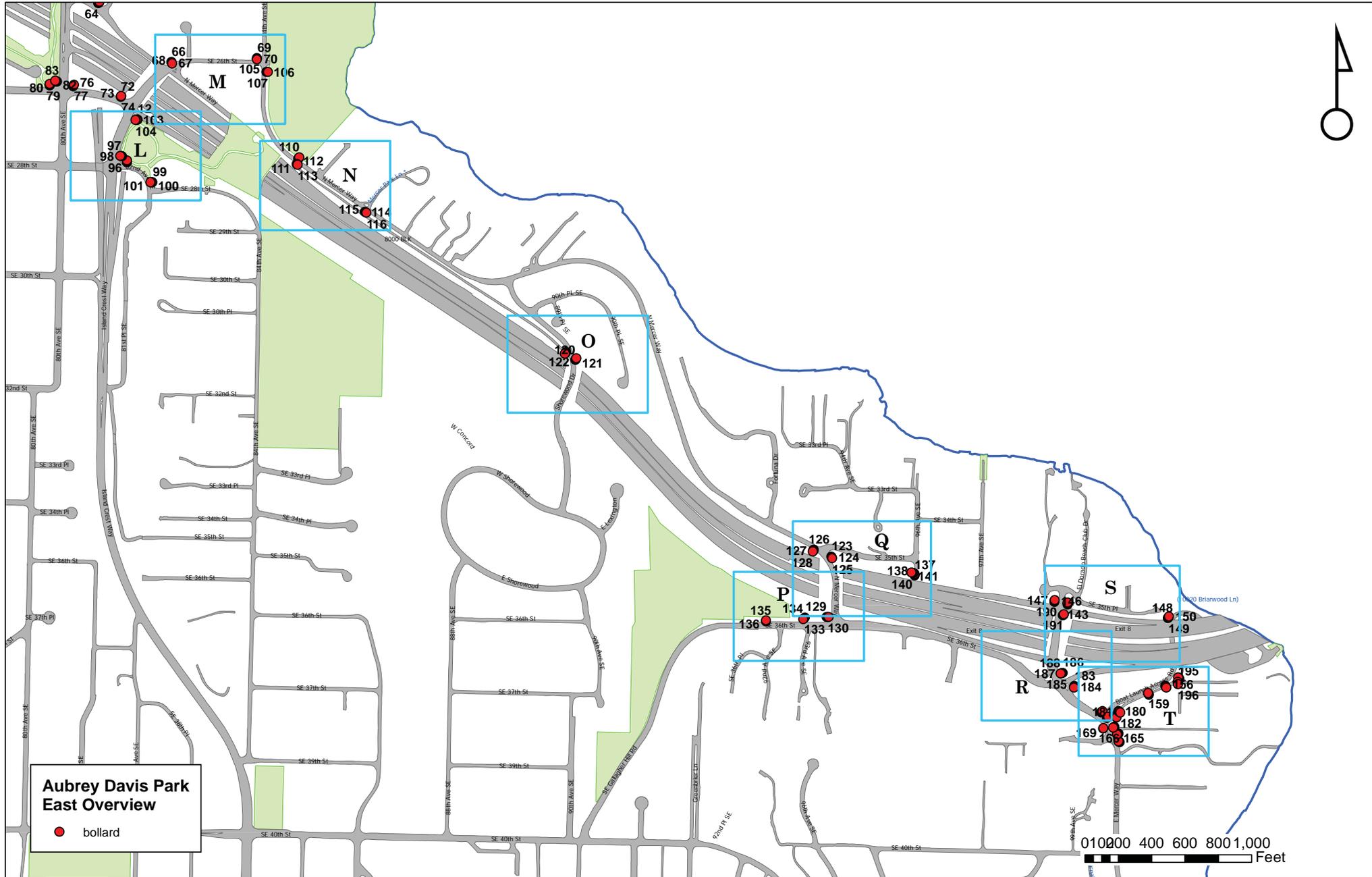


**Aubrey Davis Park
West Overview**

- bollard



331



Aubrey Davis Park East Overview
 ● bollard

332



**Aubrey Davis Park
A - West Portal**

● bollard

Group 1001

1
2
3

39
38
37

Group 1013

0 10 20 40 60 80 100 Feet

333



**Aubrey Davis Park
B - SE 22nd**

● bollard

Group 1018



**Aubrey Davis Park
C - W Mercer**

● bollard

335



**Aubrey Davis Park
D - 66th Ave SE**

● bollard

336



Aubrey Davis Park
E - 72nd Ave SE

● bollard

337



**Aubrey Davis Park
F - 74th Ave SE**

● bollard

338



**Aubrey Davis Park
G - SE 24th St**

● bollard

339



Aubrey Davis Park
H - 76th Ave SE

● bollard

0 10 20 40 60 80 100 Feet

340



Aubrey Davis Park I - 77th Ave SE

● bollard

0 10 20 40 60 80 100 Feet

341



Group 1020

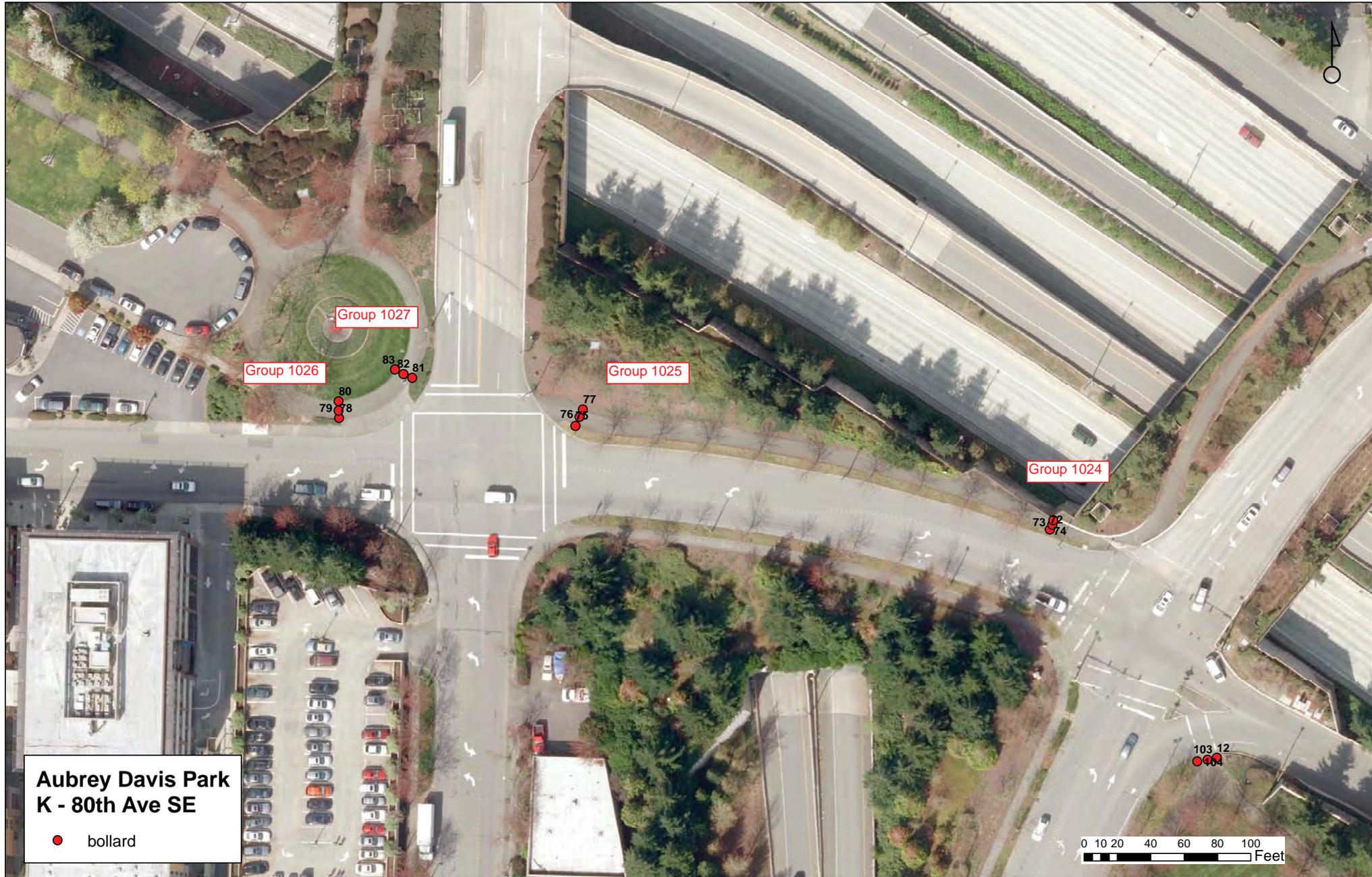
Group 1021

**Aubrey Davis Park
J - Park and Ride**

● bollard

0 10 20 40 60 80 100
Feet

342



**Aubrey Davis Park
K - 80th Ave SE**

● bollard

0 10 20 40 60 80 100
Feet

343





**Aubrey Davis Park
M - SE 26th St**

● bollard

345





**Aubrey Davis Park
O - Shorewood**

● bollard

347





**Aubrey Davis Park
Q - SE 35th St**

● bollard

349



**Aubrey Davis Park
R - E Mercer Way**

● bollard

350



Group 1049

Group 1048

Group 1064

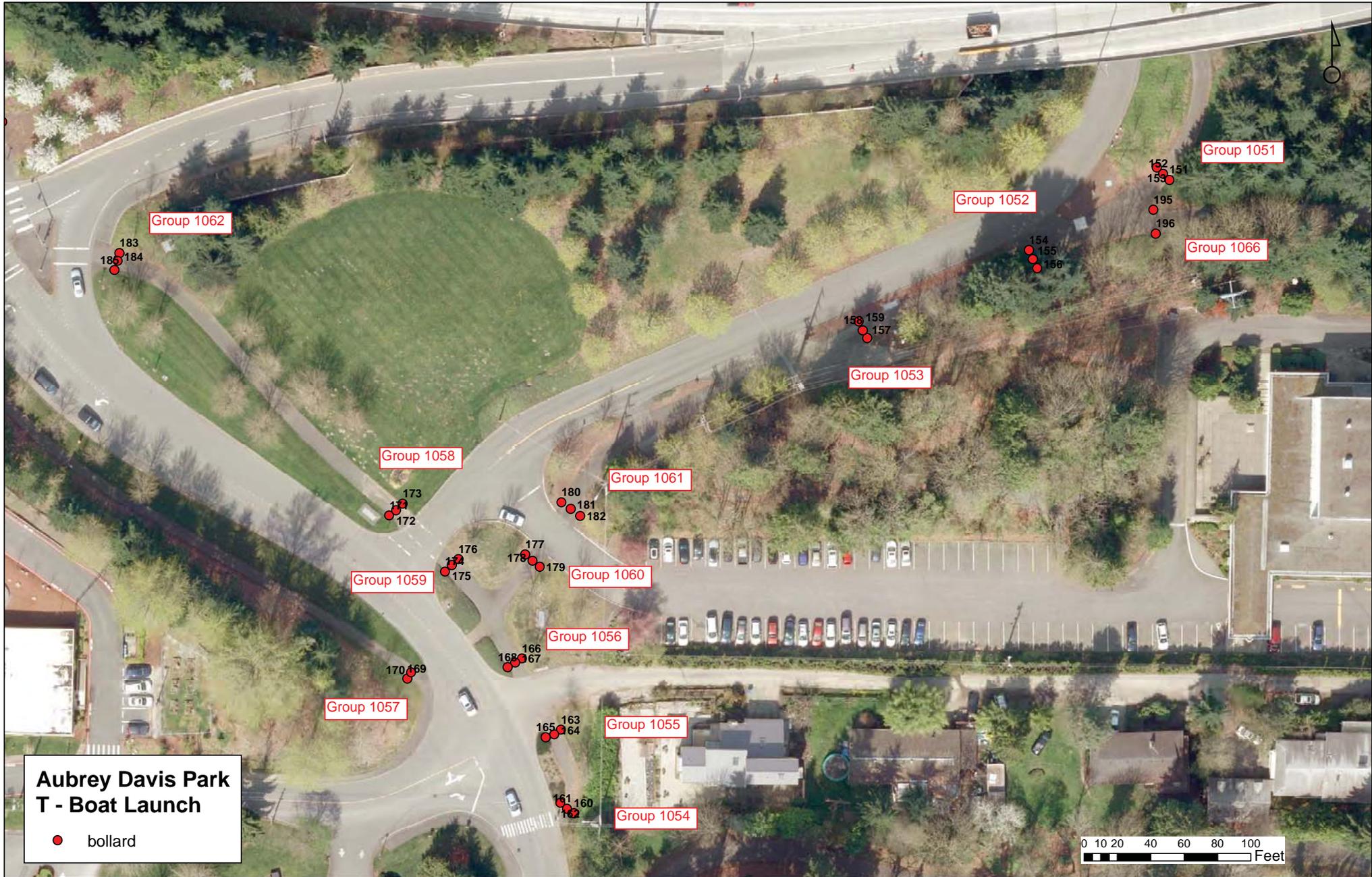
Group 1050

**Aubrey Davis Park
S - East Portal**

● bollard

0 10 20 40 60 80 100
Feet

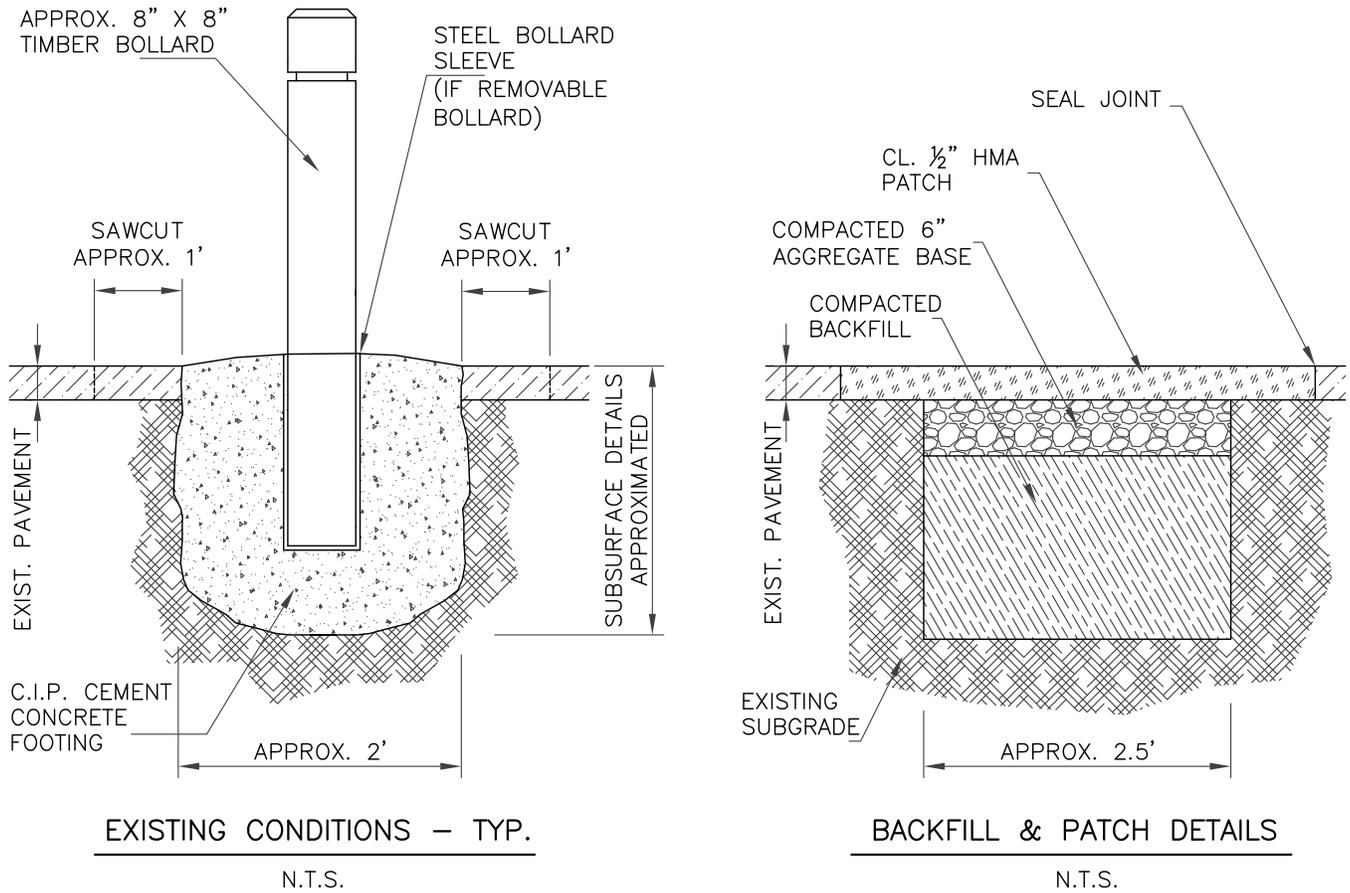
351



APPENDIX C

Representational Figures of Bollard Removal and Potential Alternate Treatments

Bollard Removal - Individual Bollards



NOTES:

- A. ORIGINAL CONSTRUCTION DETAILS ARE UNKNOWN. DIMENSIONS SHOWN ARE APPROXIMATE ONLY.
- B. DETAILS DEPICTED ARE SCHEMATIC ONLY, INTENDED TO REPRESENT A GENERALIZED ORDER OF WORK.
- C. ANTICIPATED REMOVAL AND REPAIR STEPS:
 1. SAWCUT EXISTING PAVEMENT IN SQUARE PATTERN 1' FROM EDGE OF EXISTING CONCRETE FOOTING
 2. BREAK UP AND REMOVE (OR PULL) EXISTING FOOTING
 3. BACKFILL AS SHOWN WITH BORROW AND AGGREGATE BASE, COMPACTING ALL PLACED MATERIAL ACCORDING TO WSDOT OR EQUIVALENT SPECS
 4. PLACE AND COMPACT HMA CL. 1/2" IN 2" LIFTS TO MATCH EXISTING PAVEMENT DEPTH (SEAL PAVEMENT JOINTS WITH ASPHALT SEALER)

**FIGURE DEVELOPED FOR COST ESTIMATING PURPOSES ONLY
-- NOT A DESIGN --
NOT FOR CONSTRUCTION**



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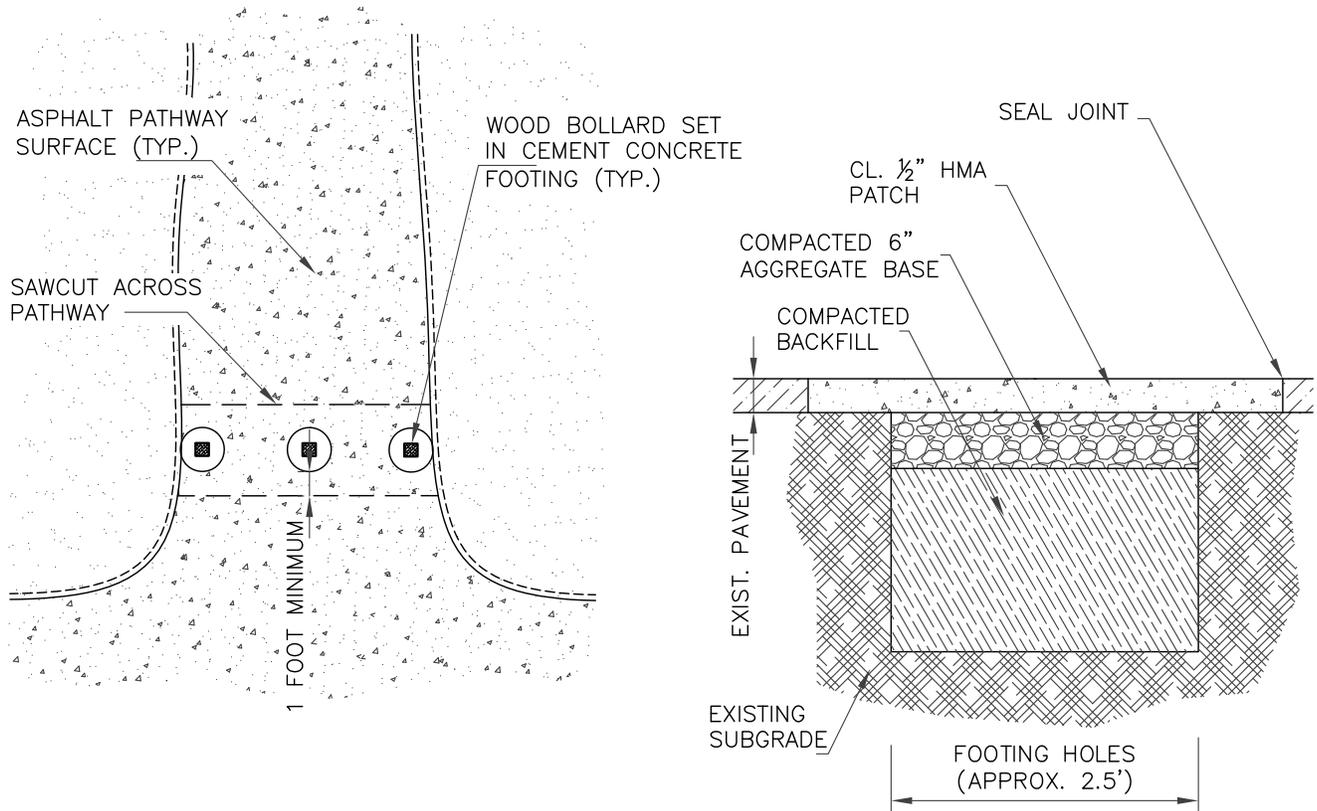
I-90 Trail - Bollards
MERCER ISLAND PARKS
MERCER ISLAND, WA

DETAIL OF PAVEMENT REPAIR FOR TYPICAL BOLLARD EMBEDDED IN CONCRETE WITHIN ASPHALT PAVEMENT

NOT FOR CONSTRUCTION
DATE: OCT 24, 2014

SHEET NAME:
BOLLARD REMOVAL PAVEMENT REPAIR
DRAWING NUMBER
PR.1
SHEET NUMBER
01 OF 06

Bollard Removal - Bollard Groups



SAWCUTTING LOCATION (PLAN)

N.T.S.

BACKFILL & PATCH (SECTION)

N.T.S.

NOTES:

- A. ORIGINAL CONSTRUCTION DETAILS ARE UNKNOWN. DIMENSIONS SHOWN ARE APPROXIMATE ONLY.
- B. DETAILS DEPICTED ARE SCHEMATIC ONLY, INTENDED TO REPRESENT A GENERALIZED ORDER OF WORK.
- C. REMOVAL AND REPAIR STEPS:
 1. SAWCUT EXISTING PAVEMENT ACROSS PATHWAY AS SHOWN (MINIMUM 1' OFFSET FROM FOOTINGS)
 2. REMOVE EXISTING PAVEMENT WITHIN SAWCUT AREA
 3. BREAK UP OR PULL EXISTING CEMENT CONCRETE FOOTINGS WITH BOLLARDS
 4. BACKFILL HOLES LEFT BY FOOTINGS AS SHOWN WITH BORROW AND AGGREGATE BASE, COMPACTING ALL MATERIAL ACCORDING TO WSDOT OR EQUIVALENT SPECS
 5. PLACE AND COMPACT HMA CL. 1/2" IN 2" LIFTS TO MATCH EXISTING PAVEMENT DEPTH (SEAL PAVEMENT JOINTS WITH ASPHALT SEALER)

**FIGURE DEVELOPED FOR COST ESTIMATING PURPOSES ONLY
 -- NOT A DESIGN --
 NOT FOR CONSTRUCTION**



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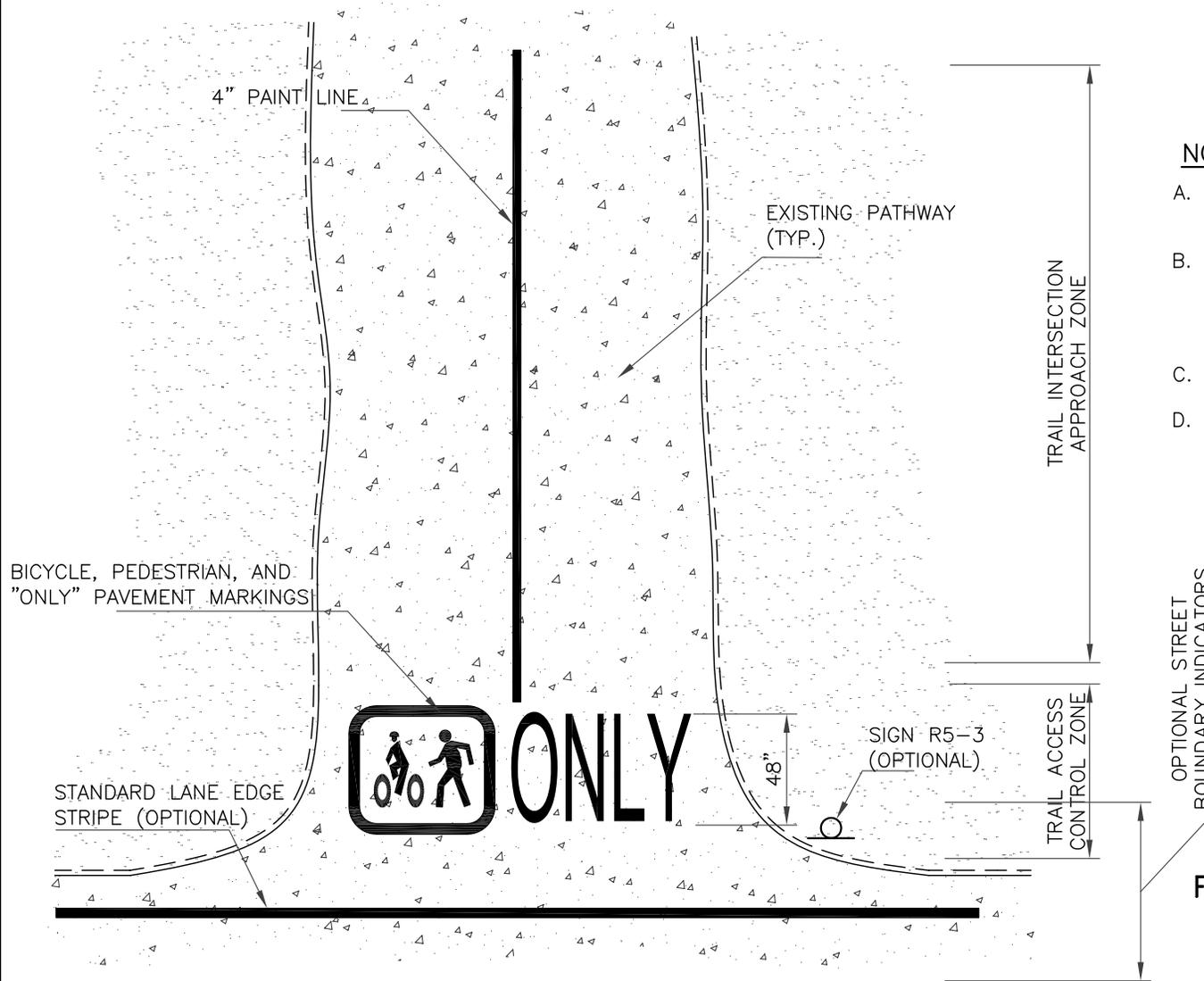
I-90 Trail - Bollards
 MERCER ISLAND PARKS
 MERCER ISLAND, WA

DETAIL OF PAVEMENT REPAIR FOR TYPICAL BOLLARD EMBEDDED IN CONCRETE WITHIN ASPHALT PAVEMENT

NOT FOR CONSTRUCTION
 DATE: OCT 24, 2014

SHEET NAME:
 BOLLARD REMOVAL PAVEMENT REPAIR
 DRAWING NUMBER
PR.2
 SHEET NUMBER
02 OF 06

Alternate Treatment 1 - Painted Bike/Ped Only Access Point



OPTIONAL SIGN
(MUTCD R5-3)



NOTES:

- A. FEATURES SHOWN ARE REPRESENTATIONAL ONLY AND DO NOT DEPICT A SPECIFIC LOCATION OR SPECIFIC DESIGN DETAILS.
- B. DIAGRAM IS SCHEMATIC ONLY, SHOWING APPROXIMATE RELATIONSHIP OF EXISTING FEATURES AND DESIGN ELEMENTS. ACTUAL DESIGN TO BE DETERMINED DURING PROJECT DEVELOPMENT.
- C. DIAGRAM AND FEATURES SHOWN ARE NOT DRAWN TO SCALE.
- D. LANE EDGE STRIPING ALONG ADJACENT ROADWAY IS OPTIONAL. SIGN R5-3 IS ALSO OPTIONAL. USE OF THESE FEATURES TO BE DETERMINED DURING PROJECT DESIGN.

FIGURE DEVELOPED FOR COST ESTIMATING PURPOSES ONLY
-- NOT A DESIGN --
NOT FOR CONSTRUCTION



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I-90 Trail - Bollards
 MERCER ISLAND PARKS
 MERCER ISLAND, WA

ALTERNATIVE ACCESS
 CONTROL FEATURES,
 OPTION 1
 PAVEMENT MARKING 1
 NOT FOR CONSTRUCTION
 DATE: DEC 7, 2016

SHEET NAME:
 BOLLARD REMOVAL
 REPLACEMENT ALTERNATE
 DRAWING NUMBER
RA.1
 SHEET NUMBER
03 OF 06

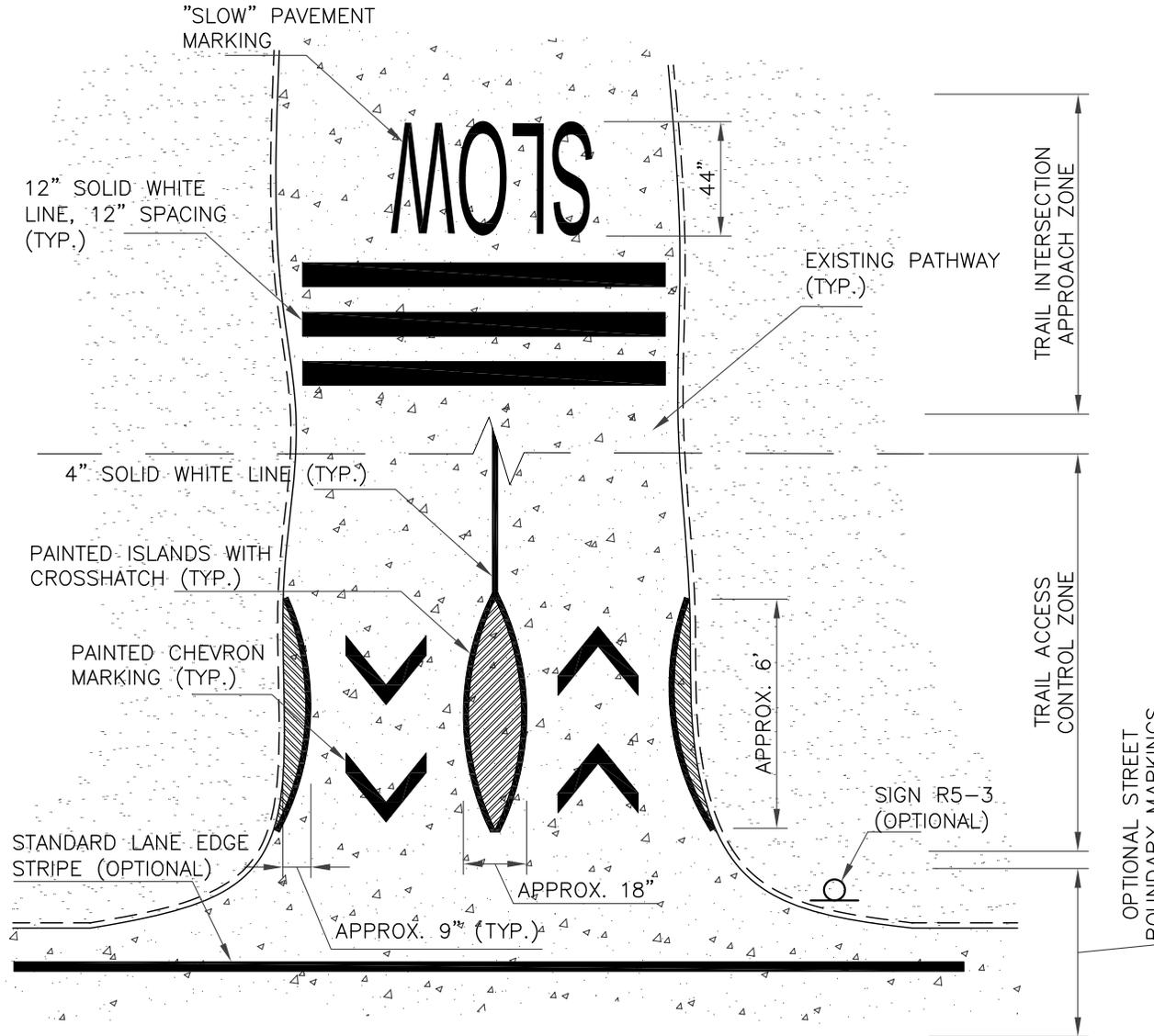
Alternate Treatment 2 - Painted Directional Access Point

OPTIONAL SIGN
(MUTCD R5-3)



NOTES:

- A. FEATURES SHOWN ARE REPRESENTATIONAL ONLY AND DO NOT DEPICT A SPECIFIC LOCATION OR SPECIFIC DESIGN DETAILS.
- B. DIAGRAM IS SCHEMATIC ONLY, SHOWING APPROXIMATE RELATIONSHIP OF EXISTING FEATURES AND DESIGN ELEMENTS. ACTUAL DESIGN TO BE DETERMINED DURING PROJECT DEVELOPMENT.
- C. DIAGRAM AND FEATURES SHOWN ARE NOT DRAWN TO SCALE.
- D. NOTE DIRECTION OF CROSSHATCHING IN PAINTED ISLANDS - SHOULD SWEEP IN DIRECTION OF FLOW OF PATH TRAFFIC.
- D. LANE EDGE STRIPING ALONG ADJACENT ROADWAY IS OPTIONAL. SIGN R5-3 IS ALSO OPTIONAL. USE OF THESE FEATURES TO BE DETERMINED DURING PROJECT DESIGN.
- E. DISTANCE BETWEEN TRAIL ACCESS POINT AND "SLOW" SYMBOL VARIES BY SPECIFIC TRAIL GEOMETRY AT EACH LOCATION, TO BE DETERMINED DURING PROJECT DESIGN.



**FIGURE DEVELOPED FOR COST ESTIMATING PURPOSES ONLY
-- NOT A DESIGN --
NOT FOR CONSTRUCTION**



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I-90 Trail - Bollards
MERCER ISLAND PARKS
MERCER ISLAND, WA

ALTERNATIVE ACCESS CONTROL FEATURES, OPTION 2
PAVEMENT MARKING 2
NOT FOR CONSTRUCTION
DATE: OCT 24, 2016

SHEET NAME:
BOLLARD REMOVAL REPLACEMENT ALTERNATE
DRAWING NUMBER
RA.2
SHEET NUMBER
04 OF 06

Alternate Treatment 3 - Painted Access Point, Basic



OPTIONAL SIGN
(MUTCD R5-3)

NOTES:

- A. FEATURES SHOWN ARE REPRESENTATIONAL ONLY AND DO NOT DEPICT A SPECIFIC LOCATION OR SPECIFIC DESIGN DETAILS.
- B. DIAGRAM IS SCHEMATIC ONLY, SHOWING APPROXIMATE RELATIONSHIP OF EXISTING FEATURES AND DESIGN ELEMENTS. ACTUAL DESIGN TO BE DETERMINED DURING PROJECT DEVELOPMENT.
- C. DIAGRAM AND FEATURES SHOWN ARE NOT DRAWN TO SCALE.
- D. LANE EDGE STRIPING ALONG ADJACENT ROADWAY IS OPTIONAL. SIGN R5-3 IS ALSO OPTIONAL. USE OF THESE FEATURES TO BE DETERMINED DURING PROJECT DESIGN.
- E. USE OF ZIG-ZAG CENTERLINE IS OPTIONAL; USE AT LOCATIONS WHERE LONG STRAIGHT STRETCH OF TRAIL APPROACHES ACCESS POINT.

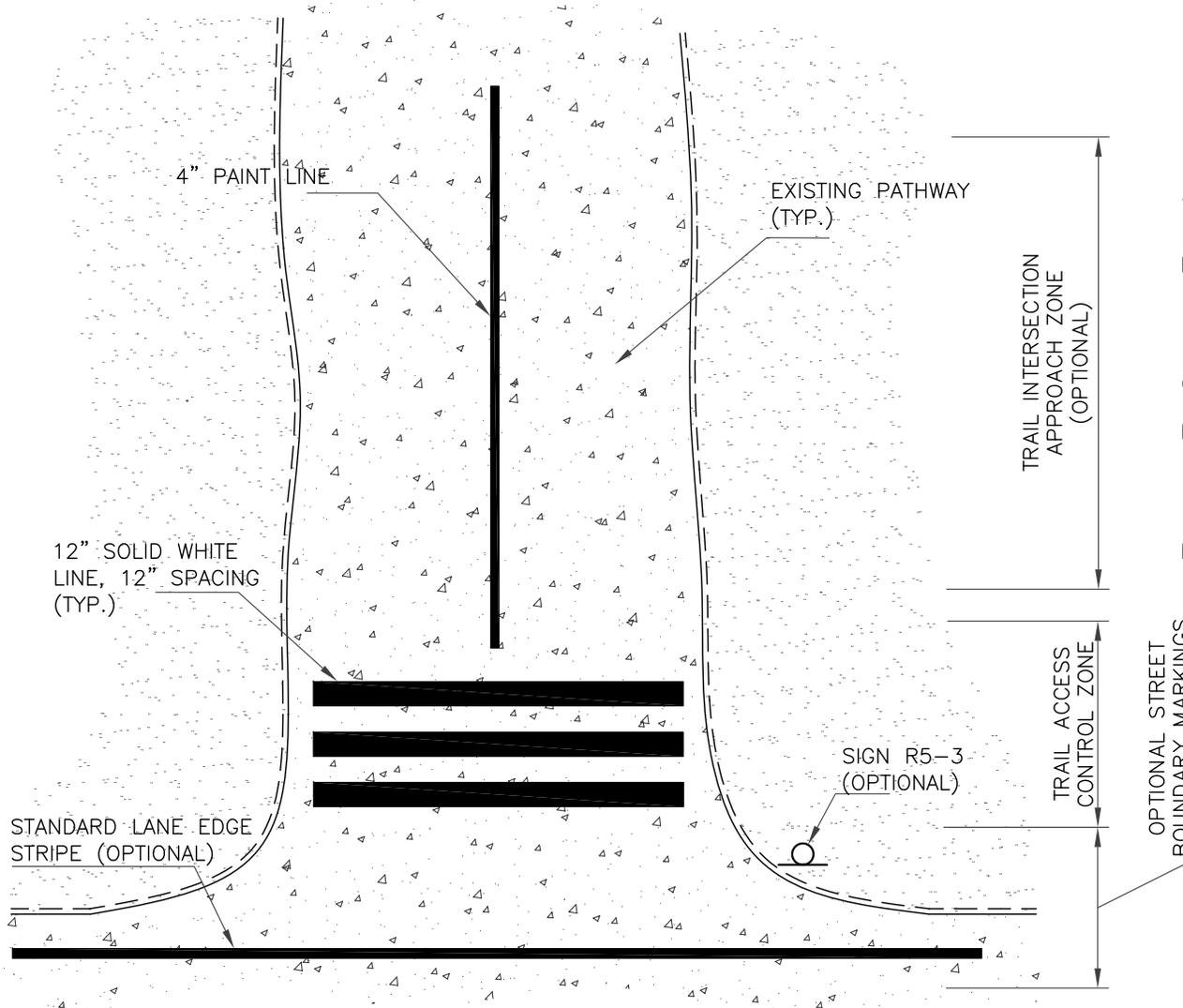


FIGURE DEVELOPED FOR COST ESTIMATING PURPOSES ONLY
-- NOT A DESIGN --
NOT FOR CONSTRUCTION



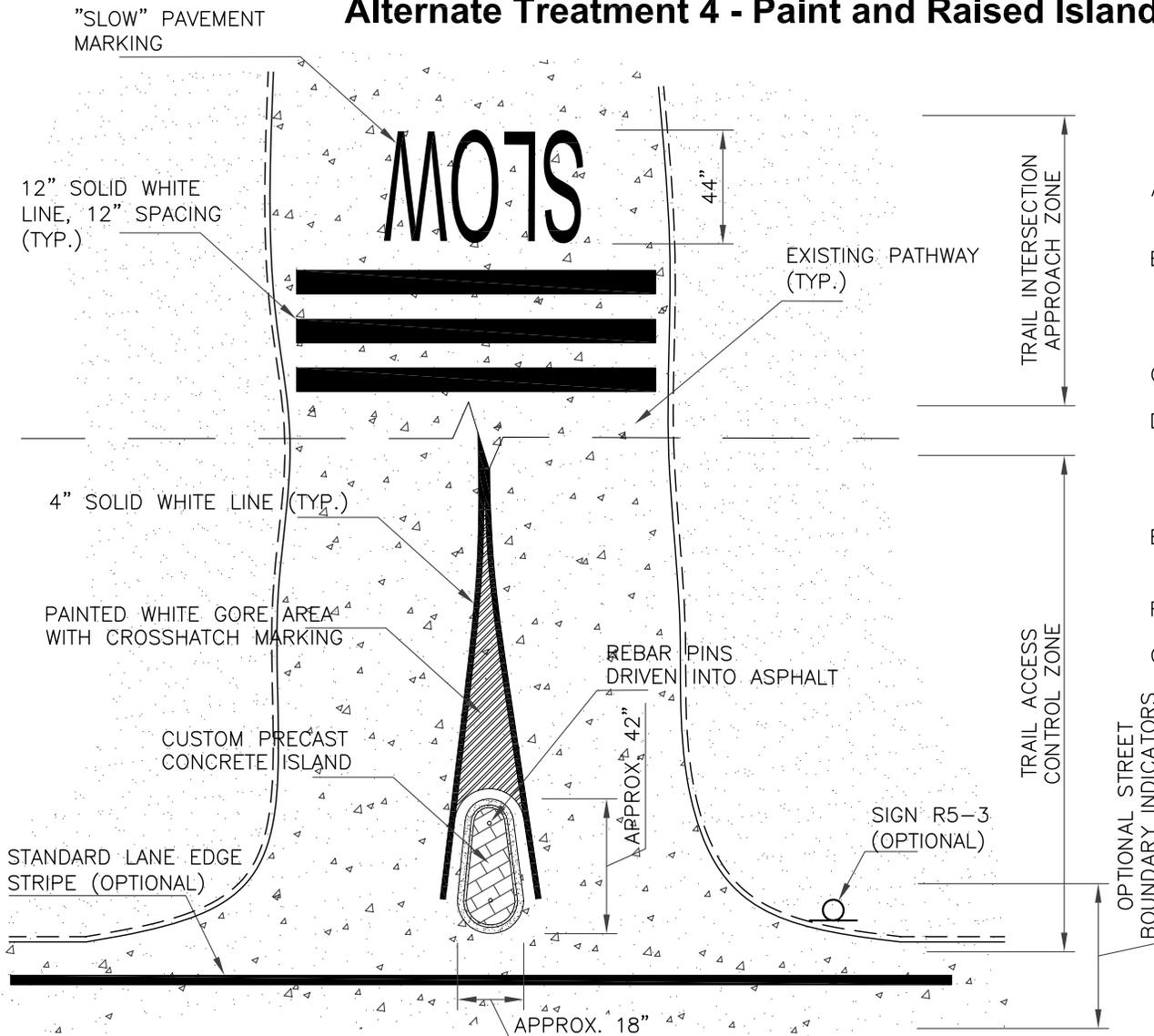
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I-90 Trail - Bollards
 MERCER ISLAND PARKS
 MERCER ISLAND, WA

ALTERNATIVE ACCESS CONTROL FEATURES, OPTION 3
 BASIC MARKING
 NOT FOR CONSTRUCTION
 DATE: DEC 7, 2016

SHEET NAME: BOLLARD REMOVAL REPLACEMENT ALTERNATE
 DRAWING NUMBER
RA.3
 SHEET NUMBER
05 OF 06

Alternate Treatment 4 - Paint and Raised Island Access Point



OPTIONAL SIGN
(MUTCD R5-3)



NOTES:

- FEATURES SHOWN ARE REPRESENTATIONAL ONLY AND DO NOT DEPICT A SPECIFIC LOCATION OR DESIGN DETAILS.
- DIAGRAM IS SCHEMATIC ONLY, SHOWING APPROXIMATE RELATIONSHIP OF EXISTING FEATURES AND DESIGN ELEMENTS. ACTUAL DESIGN TO BE DETERMINED DURING PROJECT DEVELOPMENT.
- DIAGRAM AND FEATURES SHOWN ARE NOT DRAWN TO SCALE.
- LANE EDGE STRIPING ALONG ADJACENT ROADWAY IS OPTIONAL. SIGN R5-3 IS ALSO OPTIONAL. USE OF THESE FEATURES TO BE DETERMINED DURING PROJECT DESIGN.
- DISTANCE BETWEEN TRAIL ACCESS POINT AND "SLOW" SYMBOL VARIES BY SPECIFIC TRAIL GEOMETRY AT EACH LOCATION.
- CUSTOM PRECAST CONCRETE ISLAND TO INCLUDE BEVELED OUTER CURB FACE.
- CUSTOM PRECAST CONCRETE ISLAND HELD IN PLACE BY TWO REBAR PINS DRIVEN INTO EXISTING OR PATCHED ASPHALT. ISLAND DESIGNED TO BE REMOVABLE WITH A SMALL BACKHOE OR FORKLIFT.

FIGURE DEVELOPED FOR COST ESTIMATING PURPOSES ONLY
-- NOT A DESIGN --
NOT FOR CONSTRUCTION



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 MERCER ISLAND PARKS
 MERCER ISLAND, WA

ALTERNATIVE ACCESS CONTROL FEATURES, OPTION 4 RAISED ISLAND
 NOT FOR CONSTRUCTION
 DATE: OCT 24, 2016

SHEET NAME: BOLLARD REMOVAL REPLACEMENT ALTERNATE
 DRAWING NUMBER **RA.4**
 SHEET NUMBER **06 OF 06**

APPENDIX D

Planning-Level Construction Cost Estimate

APPENDIX G

CONFLICT AREA STUDY

Mercer Island I-90 Trail

Bicycle and Pedestrian Conflict Areas Evaluation and Recommendations

July 24, 2017

Purpose

The purpose of this memo is to identify possible treatments for two locations being evaluated to improve conflict areas on the I-90 Trail, at the Mercer Island Park and Ride and at Aubrey Davis Park. TDG met with City staff on site several times to assess conditions and discussed solutions. A description of the locations, treatments considered, and final recommendations follows, along with planning-level cost estimates.

Mercer Island Park and Ride – Analysis and Treatment Recommendations

Concerns:

Pedestrians and people riding bikes share the same space at this sidewalk transit stop, which also serves as a segment of trail, and provides access to the park and ride parking area. Safety, predictability, and comfort for all users is currently compromised.

Looking at all possible options, the team assessed which elements within the space could be moved and which cannot to determine possible solutions to resolve bike/ped conflicts.

Fixed Objects:

- Trees
- Light poles
- Planters
- Wall separating parking lot and sidewalk/trail

Movable Objects:

- Bike lockers
- Bike racks
- Bus shelters
- Bus schedule kiosk
- Garbage cans
- Benches
- Newspaper boxes



Figure 1. Park and Ride, existing conditions

Potential Treatments:

The following treatments were considered and evaluated.

1. Delineation of the bike and pedestrian spaces using:
 - Green thermoplastic/epoxy/paint
 - Detectable separation indicators delineating bicycle and pedestrian space
2. Creating more space for bicyclists by shifting the location of the streetscape elements, including bus shelters, trash receptacle, the information kiosk, etc. closer to the street
3. Creating raised crossings at driveways to increase predictability and visibility at driveways, and enhance awareness of bicyclists by drivers entering/exiting parking area

Example Treatments:



Figure 2. Example showing colored/textured pavement used to demarcate bike and pedestrian travel ways (Brazil)



Figure 3. Example showing grouping of bus shelters and pedestrian amenities into pedestrian space and contrasting paving for bike travel way (Europe)



LEFT: Figure 4. Example showing bike/pedestrian symbols in shared travel way (Portland)

ABOVE: Figure 5. Example of bike and pedestrian space demarcated by detectable separation indicators (Seattle)

Final Recommendations

- Shift the bus shelters, associated signs, kiosk, and trash receptacles toward the curb to provide more operating space for all users. Maintain adequate clearance from the curb for bus access and around the shelter to allow for universal access of the bus loading areas. In relocation, take the opportunity shift the shelters laterally to provide more visual clearance, such as where the southern shelter is located opposite the corner of the garage wall, creating a pinch point.
- Indicate a separate travel way for bikes within the sidewalk, using a green paving, thermoplastic or MMA (see [NATCO Urban Bikeway Design](#) for specifications) along the length of the sidewalk, 6' minimum from the back of sidewalk. The green paving should run from driveway to driveway, and it can be extended to 77th Ave SE and 81st Ave SE
- At driveways, provide a distinctive treatment, along with SLOW messaging for bikes.
- Reinforce the messaging with thermoplastic bicycle and pedestrian symbols placed at regular and strategic intervals along both the pedestrian and bicycle travel ways
- Consider using crosswalks where pedestrians emerge from the parking area, potentially unaware that they may be crossing paths with people riding bikes.
- Install a raised crossing at the primary entry to the park and ride.

Aubrey Davis Park – Analysis and Treatment Recommendations

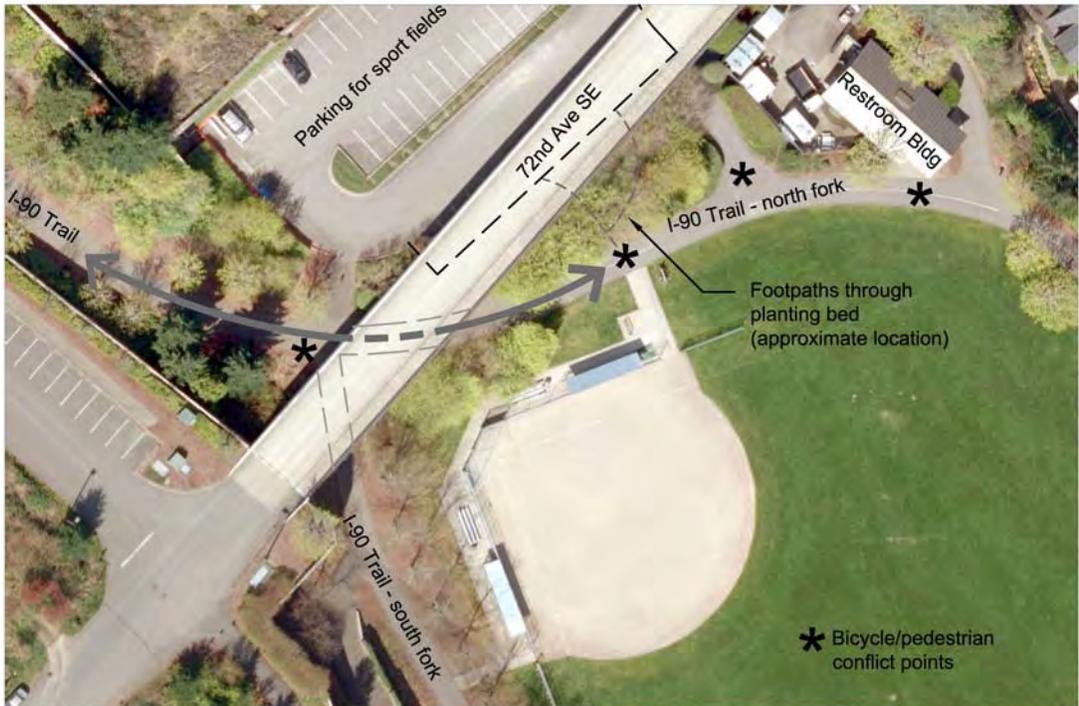


Figure 6. Existing bicycle route and conflict points

Concern #1:

Bicycle/pedestrian conflicts result from eastbound cyclists entering the park after descending a steep grade on a horizontal curve, and then traveling along the pedestrian entrance/gathering area/spectator seating on the west edge of the park’s ball fields, as shown in Figure 6.

Concern #2:

Cyclists stopping to use the park’s restroom and drinking fountain are pausing in the trail, causing conflicts with through traffic. During peak summer usage, there can be many cyclists stopped at the restroom facilities, but there is little bicycle parking available at the site and the facilities are immediately adjacent to the relatively narrow trail.

Concern #3:

Foot traffic to and from the parking lot on the park’s west side has worn informal paths through the planting bed between the park and the parking lot.

Potential Treatments:

The following treatments were considered and evaluated.

- Revising the trail to divert users from the primary conflict area
- Using traffic calming treatments to slow bicyclists approaching the conflict area
- Creating a plaza-like area adjacent to the parking lot to give trail and park users more room to negotiate with one another

- Using a special paving treatment to enhance plaza effect
- Remove/revise plantings between parking area and park to enhance visibility and facilitate more predictable movement through the area

Final Recommendations

1. Re-route bicycle traffic to bypass pedestrian gathering areas (revise trail layout at 72nd Ave SE overpass so that eastbound bicycles are routed along the park's south edge, and must make a soft left turn if they wish to travel along the park's west edge). This re-route is shown in Figure 7.
2. Apply pavement markings for visual and tactile effect to: a) slow bicycle traffic entering the park from the west, and b) delineate pedestrian crossings and areas with high foot traffic (see Figure 6).
3. Wayfinding signs should be added to reinforce use of the new route. Potential locations include the location of proposed trail revision within Aubrey Davis Park (to guide both eastbound and westbound traffic), and the south and north ends of the 76th Ave SE overpass (to route bicycle traffic to/from the I-90 trail alignment on the freeway's south side).
4. This proposed route would require a transition from the 12'-wide trail entering the park's west side to the 10'-wide trail on the park's south side.

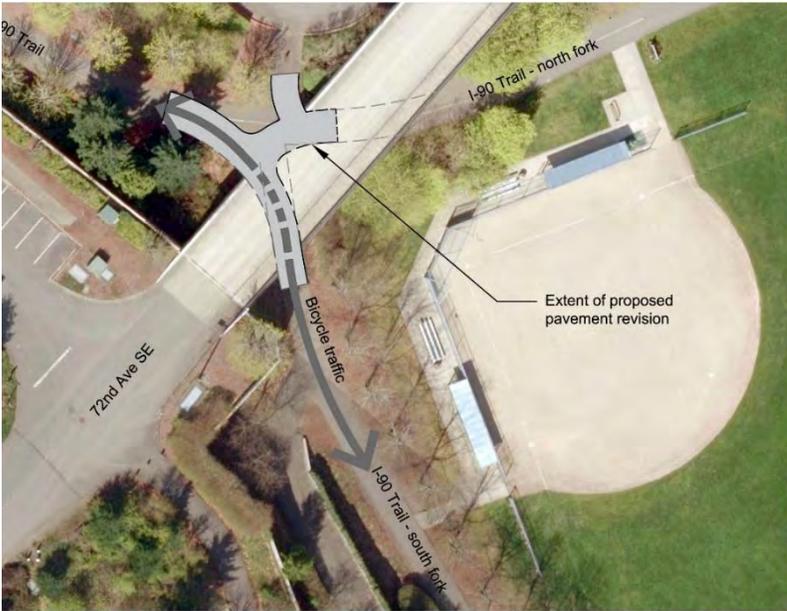


Figure 7. Pavement revised to route bicycle traffic to park's south edge

5. Remove low plantings between the parking areas the park, converting the space to compacted crushed rock (or pavement) to provide a mixing zone.
6. Widen out the pathway and create a larger mixing zone, demarcated with a painted pattern to set the space off visually (see Figure 8).



Figure 8. Example of painted mural along a trail. (Charlotte Rail Trail) SOURCE: <http://www.charlotterailtrail.org/projects/jessie-katey>

Mercer Island Bike-Ped Conflict Study

Planning Level Cost Estimates

7/24/2017

By: Jakob Ward and Kristen Lohse

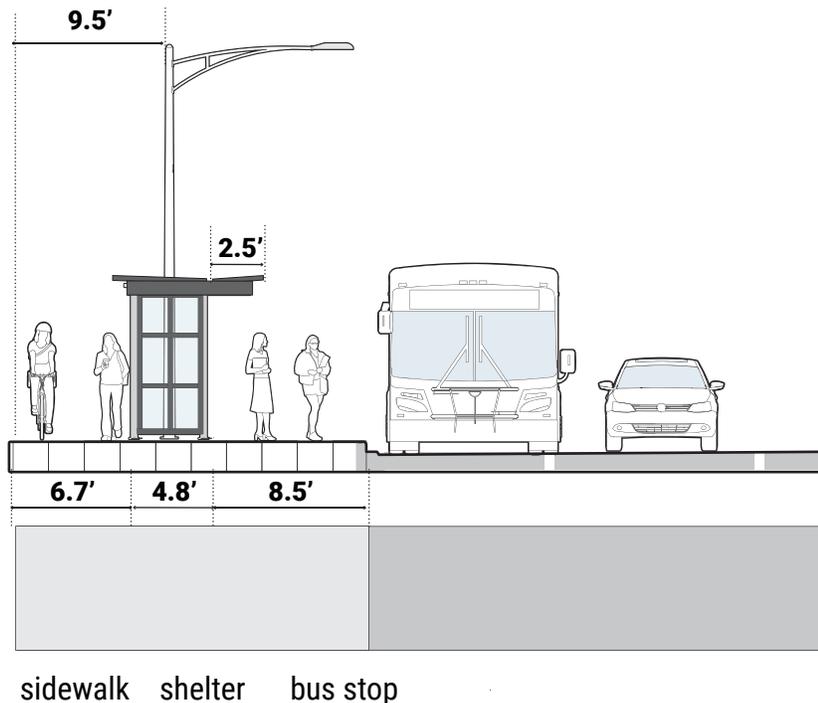
Checked by: Craig Schoenberg, P.E. and Kenneth Loen, P.E.

| Park and Ride | | | | | |
|--|------|-----------|------|-----------|-----------|
| Item | Unit | Unit Cost | QTY | TOTAL | SUM |
| Striping | | | | | \$ 12,140 |
| Thermoplastic Pavement Marking Lines (4") | LF | \$1 | 100 | \$100 | |
| Thermoplastic Pavement Marking Bicyclist Symbol | EA | \$880 | 7 | \$6,160 | |
| Thermoplastic Pavement Marking Pedestrian Symbol | EA | \$880 | 6 | \$5,280 | |
| MMA SLOW Legend | EA | \$150 | 2 | \$300 | |
| MMA LOOK Legend | EA | \$150 | 2 | \$300 | |
| Relocation | | | | | \$ 5,500 |
| Bus Stop Relocation | EA | \$2,000 | 2 | \$4,000 | |
| Street Furniture Relocation | EA | \$500 | 3 | \$1,500 | |
| 81st Ave NE Raised Crossing | SF | | 940 | | \$ 12,960 |
| Site Preparation (Sawcut, Demo, Excavation) | CY | \$ 100 | 70 | \$ 7,000 | |
| Aggregate Base Course | CY | \$ 40 | 35 | \$ 1,400 | |
| Asphalt Base Course | TON | \$ 70 | 36 | \$ 2,520 | |
| Asphalt Surface Course | TON | \$ 85 | 24 | \$ 2,040 | |
| N Mercer Way Crossing | EA | | | | \$ 11,680 |
| Site Preparation (Sawcut, Demo, Excavation) | CY | \$ 100 | 30 | \$ 3,000 | |
| Construct sidewalk | SY | \$ 100 | 15 | \$ 1,500 | |
| Curb ramp | EA | \$ 7,000 | 1 | \$ 7,000 | |
| Crosswalk striping | LF | \$ 1 | 180 | \$ 180 | |
| Signage | | | | | \$ 2,000 |
| Sign Panel (Class I) | EA | \$ 150 | 8 | \$ 1,200 | |
| Steel Sign Post (2x2 Inch Tubing) | EA | \$ 100 | 8 | \$ 800 | |
| | | | | | \$ 44,280 |
| Green Pavement Markings | | | | | |
| Option 1 - Paint | SF | \$ 2 | 2500 | \$ 5,000 | |
| Option 2 - Thermoplastic | SF | \$ 7 | 2500 | \$ 17,500 | |
| Option 3 - MMA or DLPM | SF | \$ 10 | 2500 | \$ 25,000 | |
| | | | | | |
| | | | | | |
| | | | | | |
| TOTALS | | | | | |
| With Option 1 surfacing | | | | | \$ 49,280 |
| With Option 2 surfacing | | | | | \$ 61,780 |
| With Option 3 surfacing | | | | | \$ 69,280 |

| Aubrey Davis Park | | | | | |
|---|-------------|------------------|------------|--------------|-------------------|
| Item | Unit | Unit Cost | QTY | TOTAL | |
| Striping | | | | | \$410 |
| Thermoplastic Pavement Marking Lines | LF | \$1 | 110 | \$110 | |
| MMA SLOW Legend | EA | \$150 | 2 | \$300 | |
| Physical Construction | | | | | |
| Revise paving to reroute bicycle traffic | SF | | 1050 | | \$ 14,548 |
| Site Preparation (Sawcut, Demo, Excavation) | CY | \$ 100 | 77 | \$ 7,700 | |
| Aggregate Base Course | CY | \$ 40 | 39 | \$ 1,560 | |
| Asphalt Base Course | TON | \$ 70 | 39 | \$ 2,730 | |
| Asphalt Surface Course | TON | \$ 85 | 26 | \$ 2,210 | |
| Geotextile Filter Cloth | SY | \$ 3 | 116 | \$ 348 | |
| Remove low-growing shrubs/groundcovers near parking areas and replace with compacted crushed rock or pavement | SF | | 2900 | | \$ 4,300 |
| Remove Shrubs/Groundcover | CY | \$ 20 | 107 | \$ 2,140 | |
| Compacted Crushed Rock | CY | \$ 40 | 54 | \$ 2,160 | |
| Sawcut and create new plaza area, paved in asphalt or concrete, with stamped/painted pattern | SF | | 4800 | | \$ 90,445 |
| Site Preparation (Sawcut, Demo, Excavation) | CY | \$ 75 | 356 | \$ 26,700 | |
| Aggregate Base Course | CY | \$ 40 | 178 | \$ 7,120 | |
| Asphalt Base Course | TON | \$ 70 | 182 | \$ 12,740 | |
| Concrete Surface Course | TON | \$ 85 | 121 | \$ 10,285 | |
| Painted | SF | \$ 7 | 4800 | \$ 33,600 | |
| Signage | | | | | \$ 1,000 |
| Sign Panel (Class I) | EA | 150 | 4 | \$ 600 | |
| Steel Sign Post (2x2 Inch Tubing) | EA | 100 | 4 | \$ 400 | |
| TOTAL | | | | | \$ 110,703 |

EXISTING CONDITION*
looking east

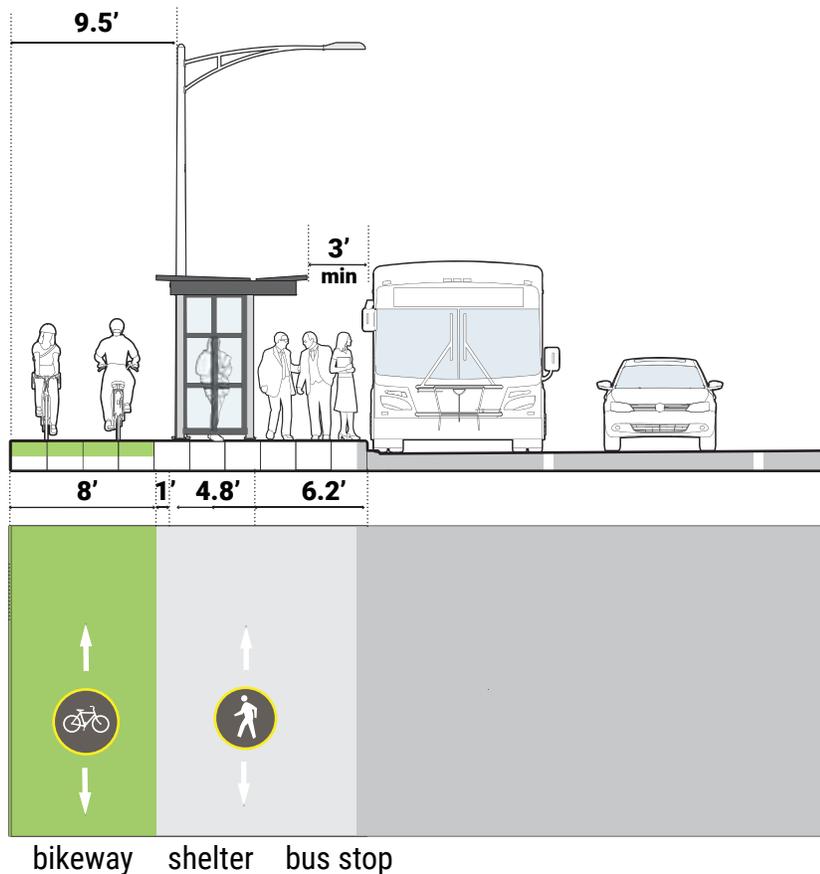
*Dimensions are approximate



PROPOSED CROSS SECTION*
looking east

- Shift transit shelters and associated amenities toward curb
- Demarcate 8' (typ). bike travelway
- 3' min. clearance between bus and shelter roof/overhang is critical dimension. Adjust bikeway dimension as required to maintain 3' clearance

*Dimensions are approximate



Indicate separate travel way for bikes with green paving, thermoplastic or MMA

- 8' wide through plaza area, 6' on adjacent sidewalks
- Reinforce message with bike and ped symbols applied at regular and strategic locations to indicate preferred bike and ped travel ways
- Extend northwest to 77th Ave SE and southeast to 81st Ave SE
- Long term, consider raised crossings of the trail at driveways
- If needed, limb up street trees so branches are well above bicyclists

Use pavement markings to increase awareness

- At driveway crossings, add "SLOW" pavement markings
- At the two ped entries to the parking structure, add crosswalk striping and "Look" message

- Relocate all bus shelters + kiosk + trash receptacles toward curb. Bus shelters to be placed so that overhang on roofs are 3' min. from face of curb. Shelters will be approx. 6' from face of curb.
- Shift southern shelter toward light pole, to avoid pinch point with corner of parking structure (indicated with red arrow) and phone booth. Consider relocating phone booth to an area with more clearance
- Ensure 4' min. clearance between shelter and light poles (and any other vertical element)

Relocate newspaper boxes to plaza area to east

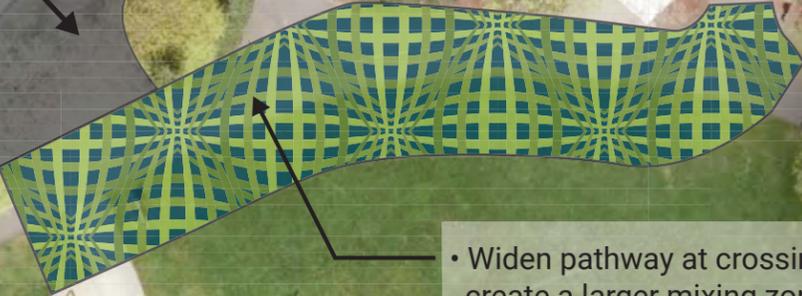
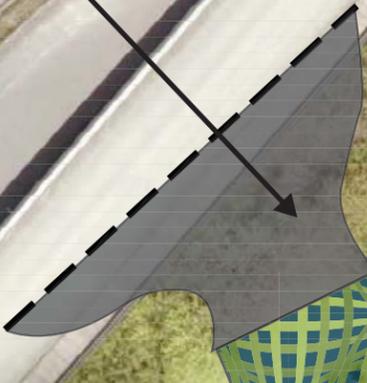
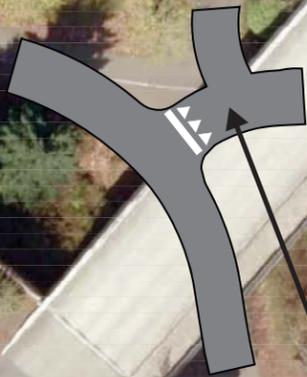
Pinch point

- Relocate kiosk into plaza area, out of bike travelway
- Install a raised crossing across 81st Ave SE and install new directional crosswalk across N. Mercer Way

Pavement Marking Details

- Driveway crossings
- Pedestrian crossings of bike travel way
Crosswalk striping with "Look" message and arrows where pedestrians enter bike travel way
- Bike and ped symbols (not to scale)
White thermoplastic bike and pedestrian symbols over textured thermoplastic circles, 12" diam., dark background with contrasting edge. Thermoplastic arrows optional

Remove low-growing shrubs/ground covers and replace with compacted crushed rock or pavement. Vine maples to remain.



Paint stripes and SLOW markings in advance of each end of mixing zone to promote slower speeds

Widen pathway at crossing to create a larger mixing zone that reads visually more like a plaza and less like a trail. This will allow people walking and people biking more room to maneuver around one another. Paint mixing zone with a distinctive pattern using high durability, non-slippery paint (sample only shown) to increase visibility of mixing zone

Revise trail intersection to route through-traffic to south side of park and slow bike traffic traveling to north edge of park. Reinforce message with yield markings on minor leg and with wayfinding signs, location and design by others

Source: Esri, DigitalGlobe, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AeroGRID, IGN, and the GIS User Community

APPENDIX H

FACILITIES ASSESSMENT

City of Mercer Island
Aubrey Davis Park Master Plan
Facility Condition Assessment

GENERAL

On October 17, 2018 representatives from Rolluda Architects conducted a site visit to Aubrey Davis Park for the purpose of conducting a facility assessment of the existing restroom and picnic shelter structures. They were accompanied by Paul West, Interim Director/Parks Superintendent for the City of Mercer Island, who provided anecdotal history for the performance of the structures.

DESCRIPTION OF BUILDINGS

Restroom

The restroom building construction consists of a concrete slab-on-grade, concrete masonry unit walls (CMU) up to 8 feet high on the exterior walls and up to roof structure on the interior walls, built-up wood roof structure with a batten seam metal roof. The exterior walls above the CMU are framed with 4x4 posts at 4 feet on center with cedar louvered infill panels in between the posts. The built-up roof structure consists of 4x8 rafters at 4 feet on-center with 2x6 sandwiched on either side. 2x4 purlins at 1 foot on-center are layered perpendicular to the rafters and another layer of 1x4 purlins at 1 foot on-center lie perpendicular to the 2x4s. Plywood sheathing and a batten seam metal roof form the final layers of the roof. There is a fiberglass sandwiched skylight over each restroom space. The ceiling in the restroom areas is exposed to roof structure while the storage/shop area and pipe chase have a plywood ceiling at the 8 foot height with attic space above. Programmatically, the building accommodates a men's restroom, women's restroom, pipe chase, and a maintenance storage/shop area.

The interior of the restrooms consists of a sealed concrete floor slab, painted CMU walls, and stained wood structure ceiling. The restrooms, storage/shop area, and the pipe chase have painted hollow metal doors and door frames. The restrooms are equipped with stainless steel plumbing fixtures: 1 sink, 2 urinals, and 1 water closet on the men's side; 1 sink, 3 water closets on the women's side. The urinal screens and the side walls of the toilet partitions are constructed with 6-foot-high CMU, while the fronts of the toilet partitions are floor-mounted, overhead-braces partition walls. There is a wall-hung stainless steel drinking fountain on the exterior wall between the entries to the restrooms

The building is unheated except for a small ceiling-hung unit heater in the corner of the storage/shop area.

Picnic Shelter

The picnic shelter construction consists of a concrete slab-on-grade, 6 concrete columns up to 8 feet high at the perimeter, built-up wood roof structure with a batten seam metal roof. The built-

up roof structure consists of double 2x beams running the length of the structure and double 2x cross beams at each column, 4x8 rafters at 4 feet on-center with 2x6 sandwiched on either side. 2x4 purlins at 1 foot on-center are layered perpendicular to the rafters and another layer of 1x4 purlins at 1 foot on-center lie perpendicular to the 2x4s. Plywood sheathing and a batten seam metal roof form the final layers of the roof.

There is a 2-foot-high concrete wall centered along the column at 2 sides of the shelter. There are 4 fixed picnic tables beneath the roof structure. The tables have a center painted steel structure that supports the table and 4 cantilevered benches. Each table can accommodate up to 8 people.

BUILDING ASSESSMENT

Restroom

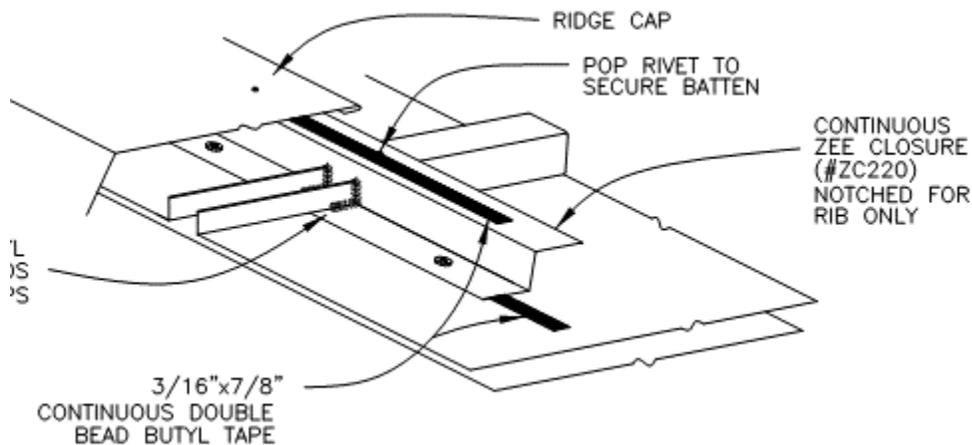
The building appears to have been well maintained but is showing its age, which is estimated to be close to 30 years old. The concrete slab is in good shape with only minor cracking, probably due to the initial shrinkage when the slab was poured.

The CMU wall are also in good condition and well maintained. It appears that if any graffiti appears, the City addresses it relatively quickly. Pressure washing the CMU and resealing it could freshen up the appearance.

The wood structure is in good condition, with only limited areas that show signs of deterioration due to weather exposure. This is likely due to the generous overhangs that protect the wood for the most part. There is some water staining around skylights, but it wasn't clear if it is an ongoing issue or one that has already been addressed. There was one area on a rafter in the men's room that had graffiti. The wood could be cleaned per Master Painters Institute's (MPI) MPI RSP-13, and 2 and refinished with a suitable sealer, and mildew growth should be treated as outlined in MPI RSP-9. There is also a certain beauty to the natural patina of the wood.

The hollow metal doors and door frames are in fair condition. The large double doors to the storage/shop area have been dented along the lower half and appear to sag slightly in the center due to the width of each leaf (wider gap at the bottom of the door at the meeting stiles). If the door is binding, replacement should be considered. Weather stripping along the bottom of the door is torn and should be replaced.

There are several battens on the roof that have slipped down, exposing the roof panel joints. The exposed joints are a point for water entry, so this condition should be addressed immediately. There should be a "Z" closure strip covered by the ridge cap. The batten should be attached to this closure strip, holding them in place. The specific detail should be confirmed with the roof manufacturer.



Mildew growth on the metal roof should be removed according to manufacturer’s instructions to prolong the life of the finish.

The drinking fountain is leaking and the water is staining the CMU beneath the fixture. Due to the leak water doesn’t reach the fountain spout.

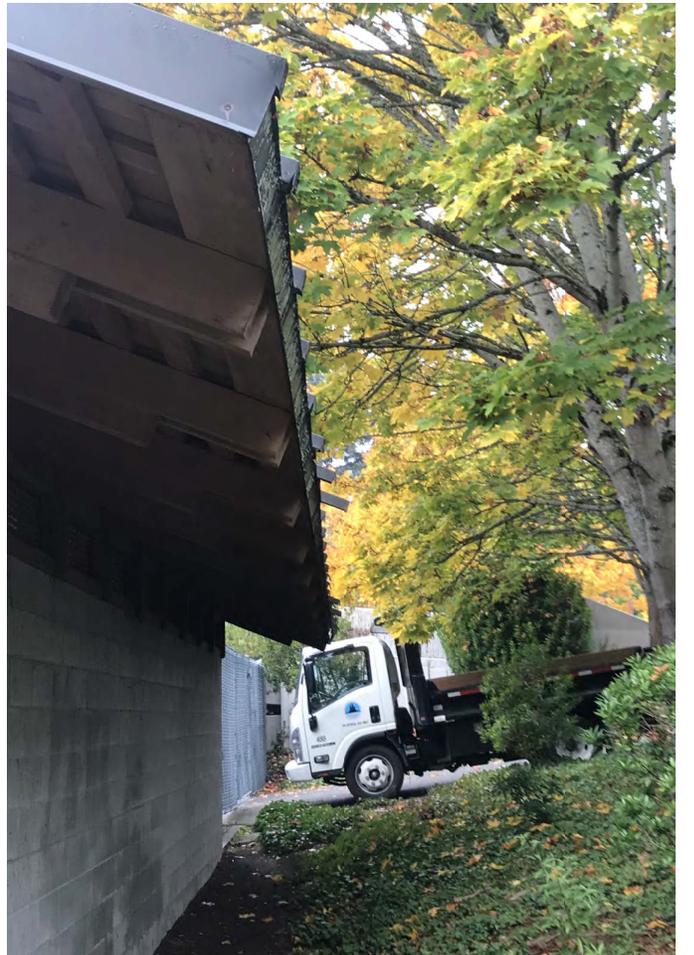
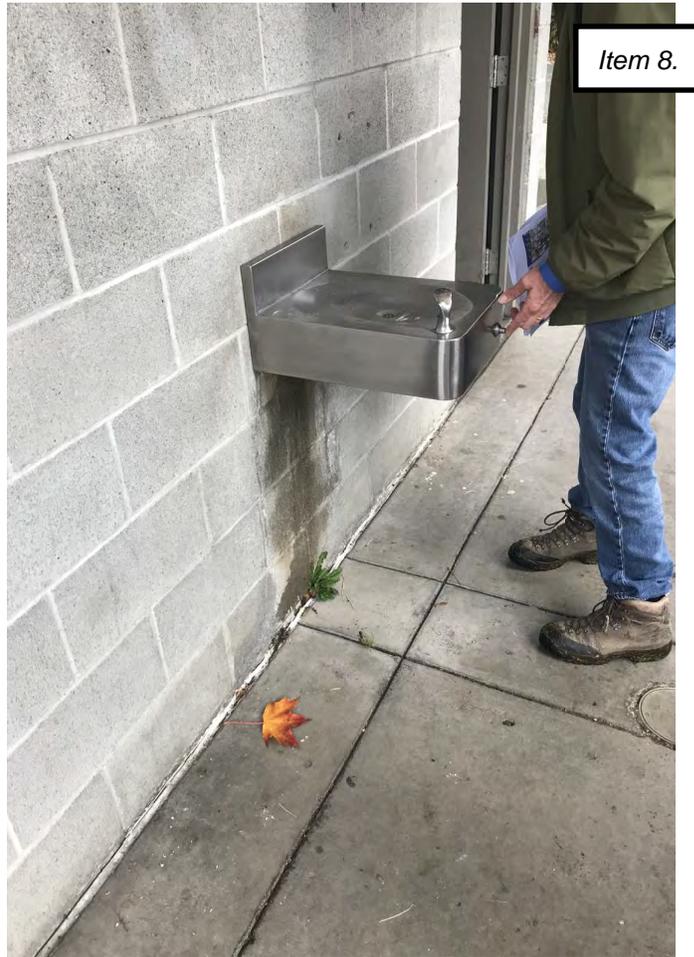
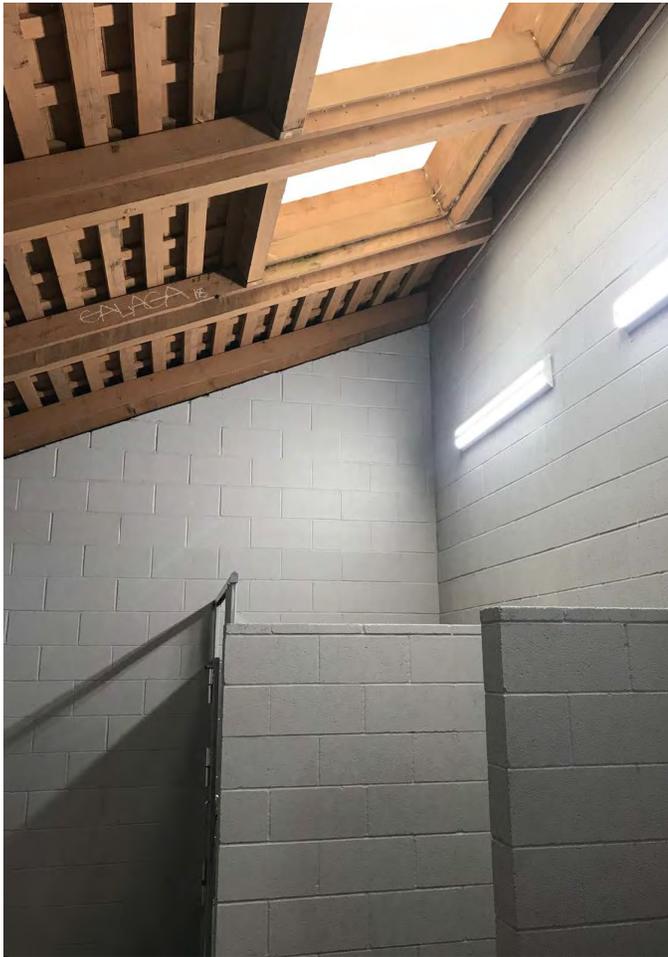
Picnic Shelter

The structure appears to have been well maintained but is showing its age which, like the restroom, is estimated to be close to 30 years old. The concrete slab is in good shape with only minor cracking, probably due to the initial shrinkage when the slab was poured.

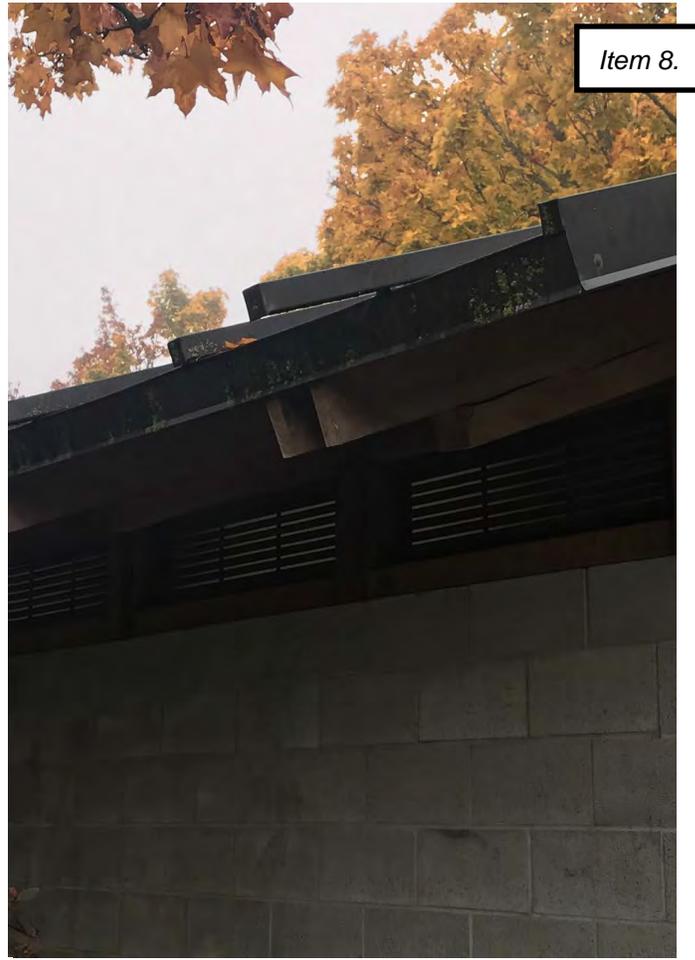
The concrete columns are in good condition.

The wood structure is in good condition, with only limited areas that show signs of deterioration due to weather exposure. There are some water stains around eaves that appear to be caused by a current ongoing issue. The source of the water should be identified immediately and the issue remedied, or further deterioration of the wood structure will occur. The wood could be cleaned per Master Painters Institute’s (MPI) MPI RSP-13, and 2 and refinished with a suitable sealer, and mildew growth should be treated as outlined in MPI RSP-9.

There are several battens on the roof that have slipped down, exposing the roof panel joints. The exposed joints are a point for water entry, so this condition should be addressed immediately. There should be a “Z” closure strip covered by the ridge cap. The batten should be attached to this closure strip, holding them in place. The specific detail should be confirmed with the roof manufacturer.



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APPENDIX I

ARTS & CULTURE VISION DOCUMENT

An Updated Vision for Arts & Culture in Aubrey Davis Park



Primavera II by Roslyn Mazzilli was the first artwork acquired for Aubrey Davis Park. Photographed by Jeff Reid.

Since its creation, the “Park on the Lid”, or Aubrey Davis Park, has been a primary location for Islanders to engage with arts and culture. As the I-90 construction neared completion in 1991, the city formed an I-90 ARTway Task Force to identify and implement public art opportunities throughout the corridor. Today, the park features nearly 25% of the City’s public art collection, stretching from *Playful Pup* by Gary Lee Price at the west end to *Gift of Reflection*, a steel sculpture by David Govedare at the east boat launch.

The first phase of the park’s public art opportunities opened in 1995 as the I-90 Outdoor Sculpture Gallery. The new gallery intended to enhance the Town Center portion of the park with public art and to enrich the Island’s sense of community and earned the city the Pacific Northwest Regional Arts and Humanities Award given by the National Recreation & Park Association in 1997 in recognition of the gallery.

In 1998, the I-90 ARTway Task Force and the Arts Council published a vision statement for the I-90 Sculpture Gallery. This vision emphasized enhancing open spaces, celebrating the unique landscape opportunity, and providing positive art experiences for the broadest audience possible. The city acquired the first keystone sculpture, *Primavera II* by Roslyn Mazzilli, with donations and grants, demonstrating the community’s commitment to public art. The gallery was renamed the Greta Hackett Outdoor Sculpture Gallery in 2017 in honor of the long-time Island resident who was integral in founding the arts space; today, it showcases more than a dozen works of art and remains a focal point of the arts experience in Aubrey Davis Park.

Building on this rich history of providing positive public art opportunities in Aubrey Davis Park and recognizing that the park itself has become a treasured cultural resource in the community, the Mercer Island Arts Council has set forth this updated vision for art in the park:

Create and strengthen community connections through arts and culture.

Be it landscape art or interpretive signage, sound installation, or seasonal foliage, this vision reflects a broader plan to use arts and culture to deepen community connections to the park.

This document elaborates on this vision by outlining four tenets to guide how arts and culture can create and maintain a sense of place in Aubrey Davis Park.

CORE TENETS

Provide welcoming and inclusive spaces to express and experience creativity.

Experience art & nature through all seasons.

Cultivate an art-inspired environment.

Celebrate our Island - past, present, and future.

Provide welcoming and inclusive spaces to express and experience creativity.

Stretching from bridge-to-bridge on the north end of the Island, Aubrey Davis Park offers a prime place for community gathering and connection. Arts and cultural activities can be used to inspire park users to interact, to engage in meaningful dialogue, and to forge bonds with the park itself. Moreover, arts-based solutions for signage and park facilities can also be employed to inspire creativity and interest beyond traditional sculptures and new events.

Objectives

- Community Gathering
- Artmaking & Creative Expression
- Design for Inclusion



Blomster by Louise Hankes is on display for sale in the Greta Hackett Outdoor Sculpture Gallery, an existing space where visitors can engage with arts and culture in the park. Photographed by [redacted] Reid.

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Possible Opportunities

- Art and arts spaces as landmarks and focal points for community gathering
- Art and arts experiences that encourage exploration and discovery
- Community engagement in the public art selection and creation process
- Interactive art installations for everyone
- Accessibility year-round through lighting features and installations, artful shelter structures, and art-inspired safety elements (eg. Designed paths to signal traffic crossings)
- Art-inspired and creative elements incorporated into wayfinding and facilities

Experience nature and art through all seasons.

The park serves as an oasis - a place where Islanders, commuters, neighbors, and others can escape from urban life. Current and future arts and culture features should complement the park's natural landscape and offer ways to interact with nature and experience tranquility. Those experiences should be available in all seasons. Using arts and culture as a tool, park accessibility can also be improved so visitors can discover something new year-round.

Objectives

- Seasonal Experiences & Discoveries
- Sustainable Art Practices
- Exploration & Discovery



The Source by John Hoge is an existing work of public art installed in Luther Burbank Park and represents works that integrate into the natural landscape as well as inspire visitors to interact with it in different ways. Photographed by Jeff Reid.

Possible Opportunities

- Art integrated with natural landscapes
- Noise mitigation through art installations and features (eg. Sound installations and rainwater collection features)
- Art that reacts to and embraces the weather - solar, rainwater, wind
- Landscape and foliage that create a visual art experience
- Interactive art features that engage all senses (eg. Edible plants and climbable or touchable artwork)

Cultivate an art-inspired environment.

Whether exploring one of the park's many works of public art or attending events in the outdoor sculpture gallery, community members have many opportunities to engage with arts and culture in Aubrey Davis Park. Expanding on this artistic atmosphere through intentional curation and community-driven activities helps make the park a place community members want to visit and explore.

Objectives

- Performances & Events
- Public Art
- Artmaking

Possible Opportunities

- Showcase and build off existing artwork to create new experiences
- Provide spaces for arts events and performances
- Explore temporary art projects to inspire public engagement and expression
- Engage community members in the public art selection and creation process
- Provide opportunities for local artists



Hundreds of visitors enjoy Art UnCorked each year, a lively art and wine event hosted by the Mercer Island Chamber of Commerce in the outdoor sculpture gallery. Photographed by city staff.



385 Artist Rachel Holloway completed the mural *Darwin's Dream* at the west side of the park in 2018. Photograph contributed by the artist.

Celebrate our Island - past, present, and future.

From legends shared by the Duwamish who foraged and hunted on the Island to Aubrey Davis's legacy of establishing the "Park on the Lid," the park has many stories to tell. Uncovering and highlighting these stories fosters community understanding of the Island's rich history and offers more opportunities to honor our place. With arts and cultural activities, we can build a bridge between past, present, and future, allowing future Islanders and regional neighbors to embrace the park.

Objectives

- Connect and Support the Island's Diverse Cultures
- Art that Reflects Natural and Cultural Heritage
- Pride - Honor Our Place

Possible Opportunities

- Introduce cultural events in collaboration with the community
- Design wayfinding and interpretive signage that tell the story of the Island and its people
- Strengthen partnerships with Mountains to Sound Greenway Trust and other local community groups
- Create areas for remembrance and reflection



APPENDIX J

ARTS & CULTURE OPPORTUNITIES

Aubrey Davis Park – Art placement within the boundaries of the Park.

A combination of work that offers a diversity of style, permanent to temporary, balance of experiences through the park and contrast in style and approach. Highlight existing work, create new opportunities.

Use art as a connector for the overall park and to draw users through. Consider a variety of users and ages, programming and changed civic experiences.

Enhances design features and infrastructure with artworks that are substantive and timeless.

Consider art as wayfinding at gateways into the park that can be tied to an artist or graphic designer to create a unique unified identity. Themes that respect nature are of interest to the Arts Council and community. Consider multilingual and indigenous language signs for larger use and understanding of site in historical context.

Tie signage to regional links - transportation, city center, bike and pedestrian trails.

Examples such as but not limited to:

Lighting or sound (sustainable methods such as solar when possible)

Consider seasonal changes – when is light at peak, how to harness that. When does the site need more lighting seasonally?

Consider this work in underpasses, under I-90, at boat launch, and park entries.

Artwork that works in concert with nature, trees, view

Appreciation of the parks natural assets and position to unique land and water views.

Existing Art

Consider tours led by creatives to learn and explore what exists at the site.

Engagement or residencies to create temporary work that encourages exploration through the park.

Interactive

Artwork that enhances play and recreational space – all ages.

Can it create ambiance or combat sound pollution? (under I-90, or highlight connection to water)

Infrastructure

I-90, Mountain to Sound Trail.

The Stacks are an amazing, unique community asset that can be projected or painted on or provide an interactive stage for performances at the site that create community gathering and new shared community events at the site.

Individual zones of interaction throughout the park:

Segments 1 & 8 and throughout park:

Functional

Draw off what exists or needs are – unique bike racks, artist designed signal boxes, seating, unique lighting fixtures, shelters, comfort stations, water amenities (fountains for people and pets) – these all create elements that tie to the wayfinding component.

Gateway/Signage

Unique artist or designer created wayfinding that ties the park together from point to point and areas in-between. Gateway artwork or pieces indicate arrival/departure at the park.

Segment 2:

If play area expands utilize this as an opportunity to bring in artist designed, interactive elements or play space that is usable at all ages.

Consider existing wall as site for art – is it graphic? Or does it take on a more natural approach? Consider this site for a green/living wall.

Consider an artist designed viewing platform or stopping point from the tennis courts.

Towers are a great location for ongoing community gathering, temporary art in the form of yearly murals on the towers, projections or lighting during the winter months.

A space for concerts and other cultural creatives to program the space for performative work (music, dance...) in the summer and better weather months.

Segment 3:

Artwork that is on a pedestrian scale that acts as an entry or gateway to the park.

Underpass of trail can utilize lighting as art which will increase visitor perception of safety.

Segment 4:

Create an intervention that provides a place for gathering – a community connection and meet up point. Also clear signage & graphics that provide info about the park but also connect to the town.

Work here that considers pedestrian scale and encourages visitors to explore the park, or conversely the town after exiting the park.

Segment 5:

Artwork that works off the trees and nature in this site.

Signage as pass through.

Segment 6:

- Possible natural play area or small earthwork for community meet up.
- Art that frames and considers natural views and connection to this place in the region.
- Artwork that considers the connection to water and land.

Segment 7:

- Consider seasonal or temporary art that considers natural interventions and work with the trees and landscape.
- Signage that connects to regional trails, other cities, the waterfront.

Segment 8: Lighting or sound interventions to combat overpass and sound from highway. Does the work have an interactive component?

- Possible seasonal temporary art on the water (that respects nature).
- Consider view over I-5 and link to the region and special views.

Color key for art – suggestions by site:

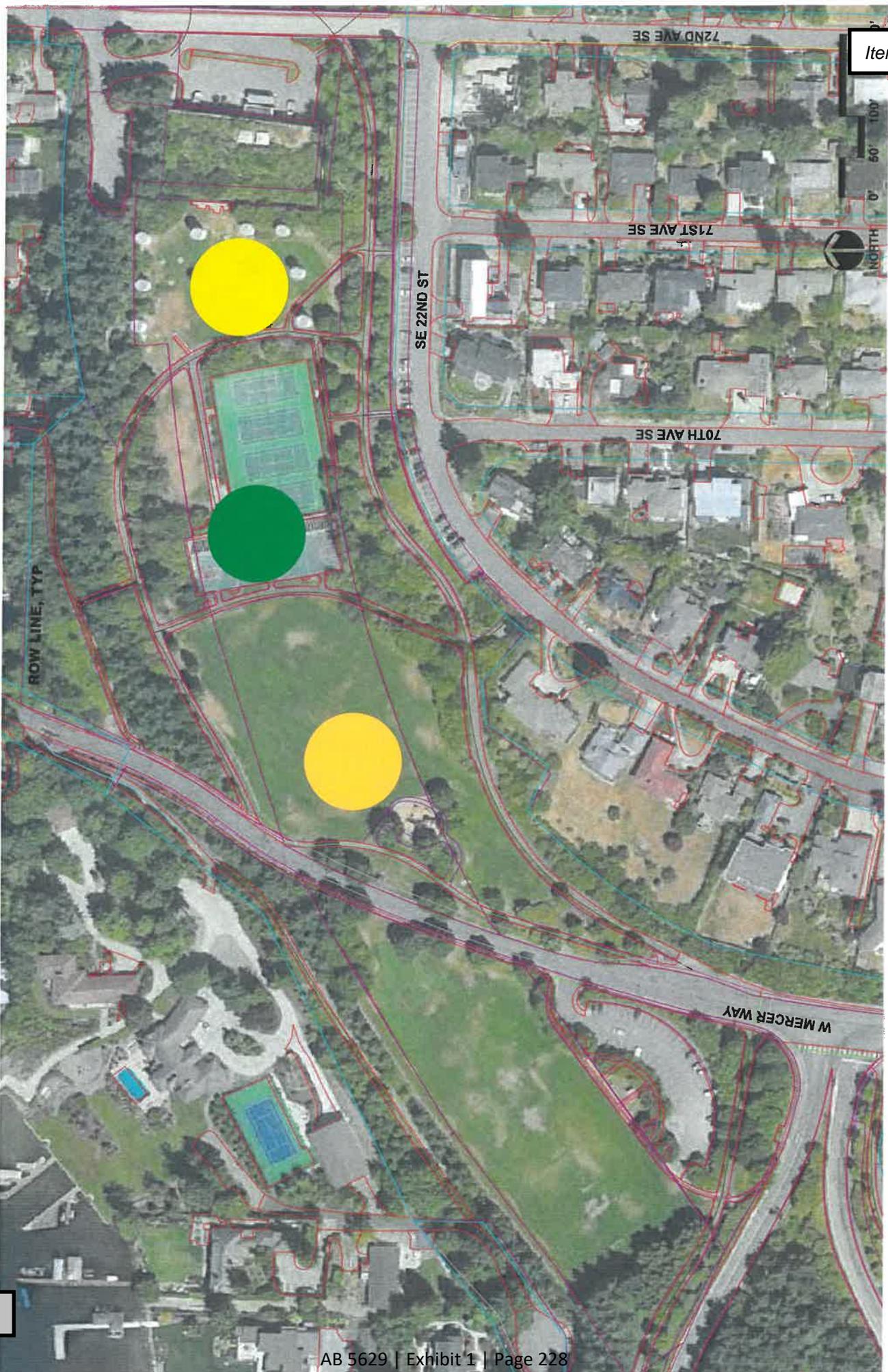
-  Lighting/sound/safety
-  Play/interactive
-  Ties to nature, site context in region, view and “history”
-  Community connections and gathering points
-  Wayfinding/graphic elements that create a connection
-  Seasonal opportunities

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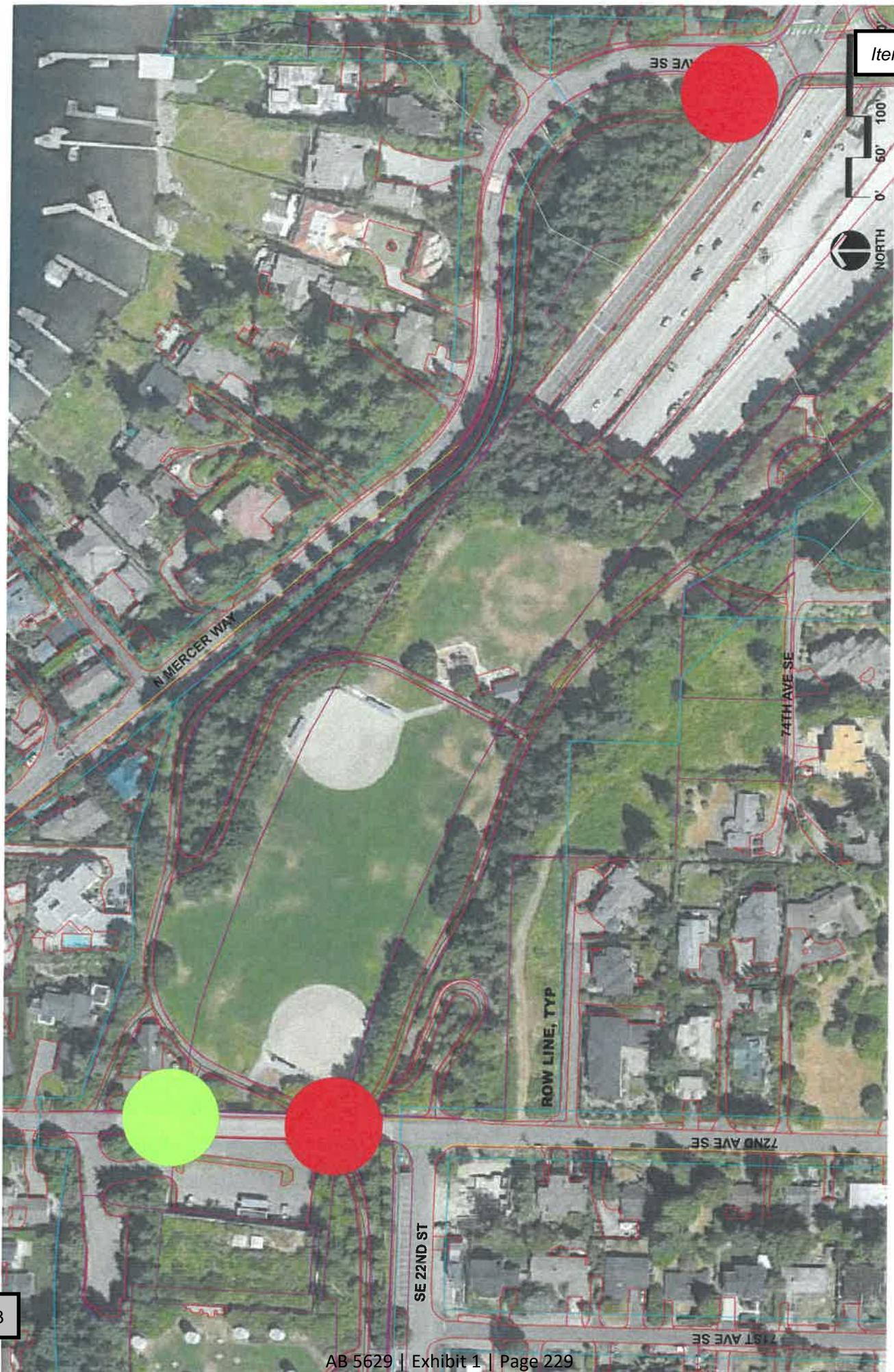


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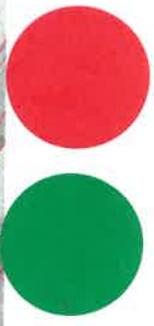
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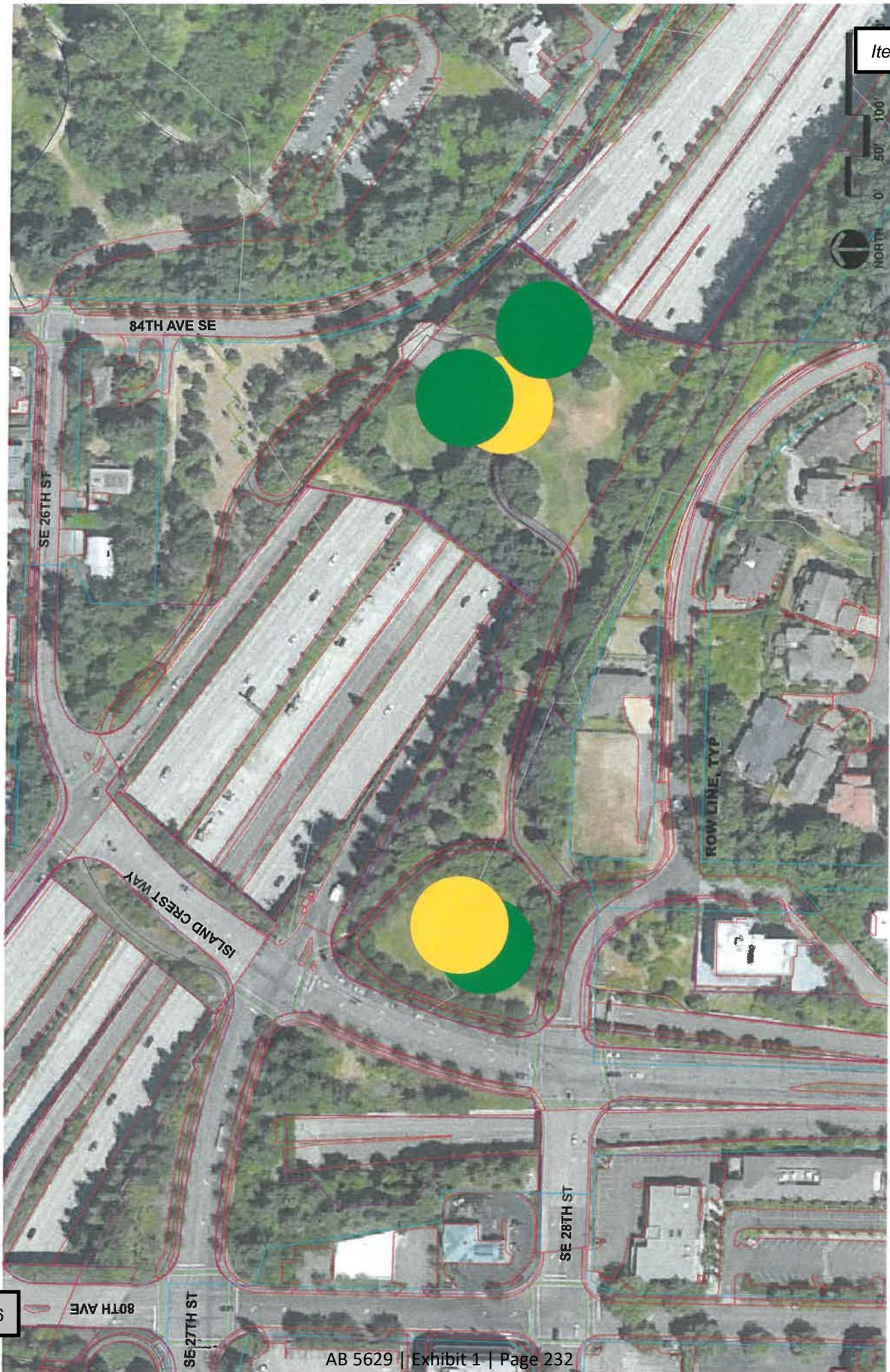


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0' 50' 100'



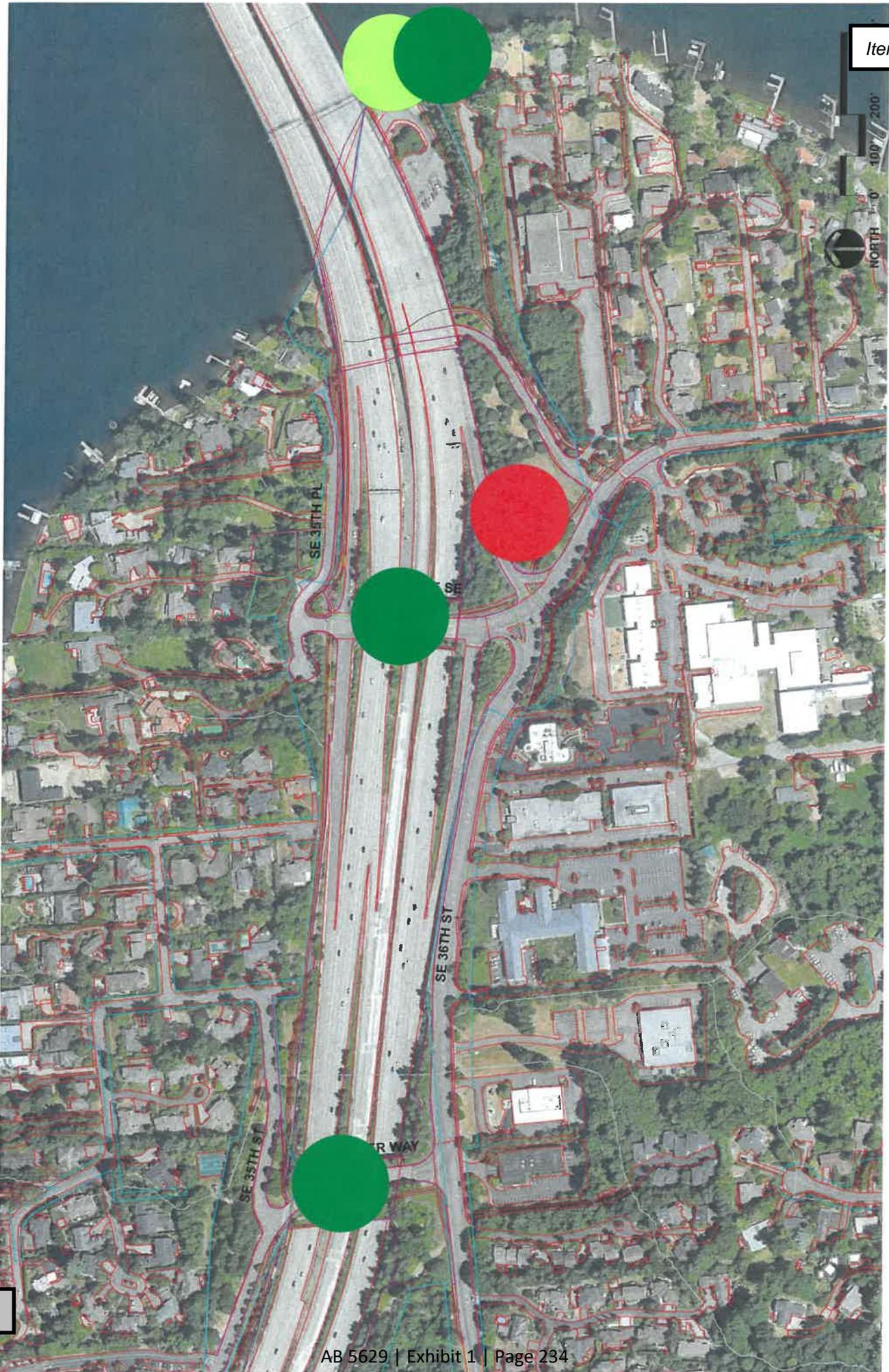
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APPENDIX K

COMMUNITY OUTREACH EVENTS

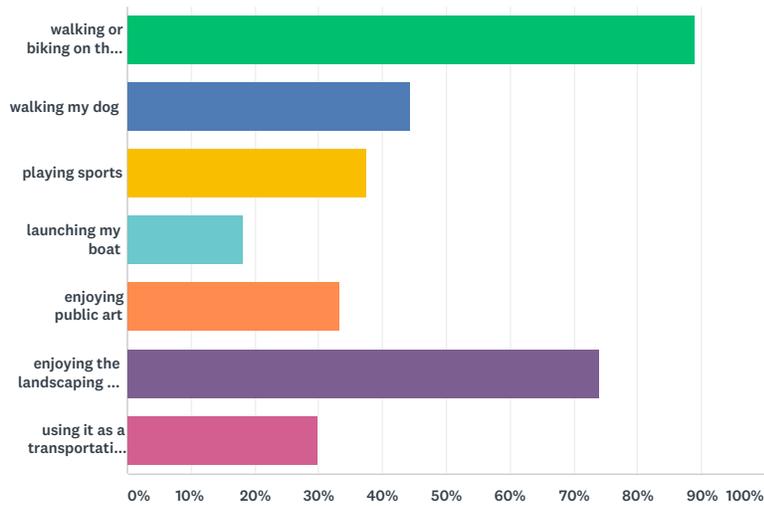
Q1 OPTIONAL: Please give us the best way to contact you

Answered: 146 Skipped: 130

| ANSWER CHOICES | RESPONSES | |
|----------------|-----------|-----|
| Name | 97.26% | 142 |
| email | 98.63% | 144 |
| phone number | 69.86% | 102 |

Q2 What park activities interest you? (select all that apply)

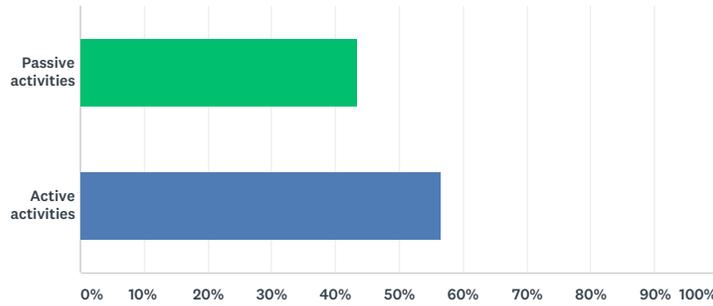
Answered: 270 Skipped: 6



| ANSWER CHOICES | RESPONSES | |
|---|-----------|-----|
| walking or biking on the trail | 88.89% | 240 |
| walking my dog | 44.44% | 120 |
| playing sports | 37.41% | 101 |
| launching my boat | 18.15% | 49 |
| enjoying public art | 33.33% | 90 |
| enjoying the landscaping and scenery | 74.07% | 200 |
| using it as a transportation route (by foot or bike) off-island | 30.00% | 81 |
| Total Respondents: 270 | | |

Q3 When you visit a park, do you typically enjoy passive activities (birdwatching, picnicing etc.) or active activities (sports, playground, etc.)?

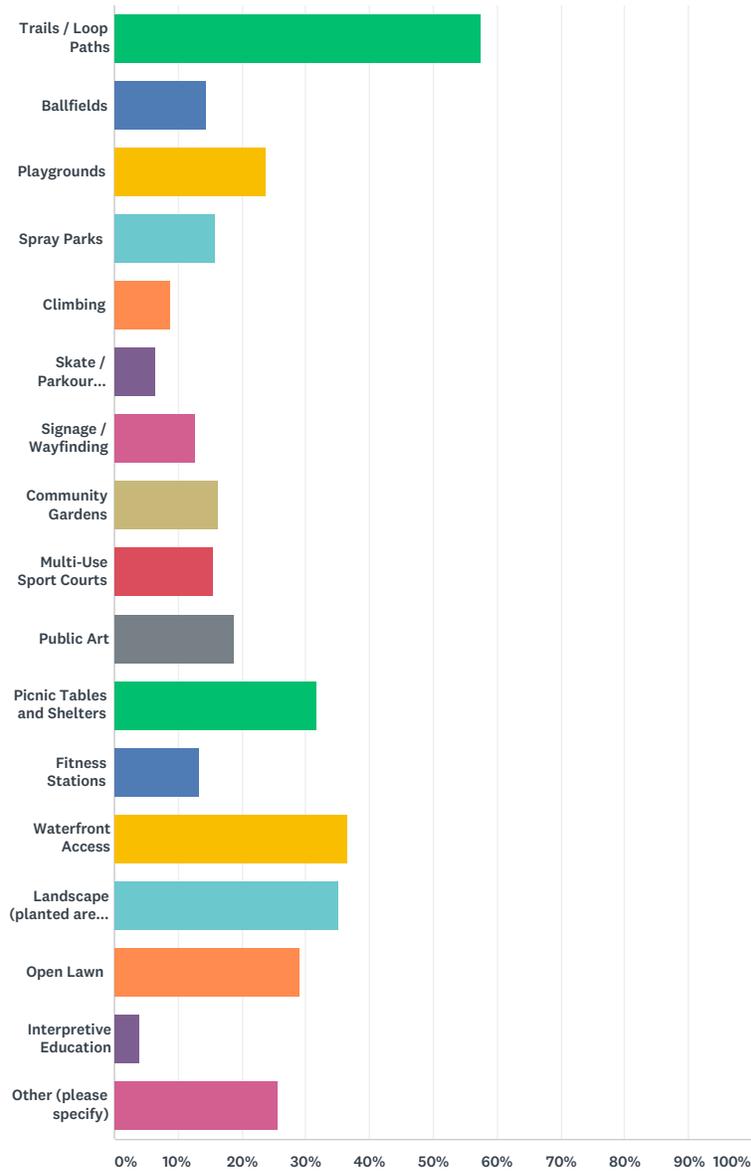
Answered: 269 Skipped: 7



| ANSWER CHOICES | RESPONSES |
|--------------------|------------|
| Passive activities | 43.49% 117 |
| Active activities | 56.51% 152 |
| TOTAL | 269 |

Q4 Of the following amenities, which of the following would you like to see improved or added in Aubrey Davis Park? (select up to 5 total)

Answered: 276 Skipped: 0



| ANSWER CHOICES | RESPONSES |
|----------------------------|------------|
| Trails / Loop Paths | 57.61% 159 |
| Ballfields | 14.49% 40 |
| Playgrounds | 23.91% 66 |
| Spray Parks | 15.94% 44 |
| Climbing | 8.70% 24 |
| Skate / Parkour Elements | 6.52% 18 |
| Signage / Wayfinding | 12.68% 35 |
| Community Gardens | 16.30% 45 |
| Multi-Use Sport Courts | 15.58% 43 |
| Public Art | 18.84% 52 |
| Picnic Tables and Shelters | 31.88% 88 |

Aubrey Davis Park Master Plan kickoff

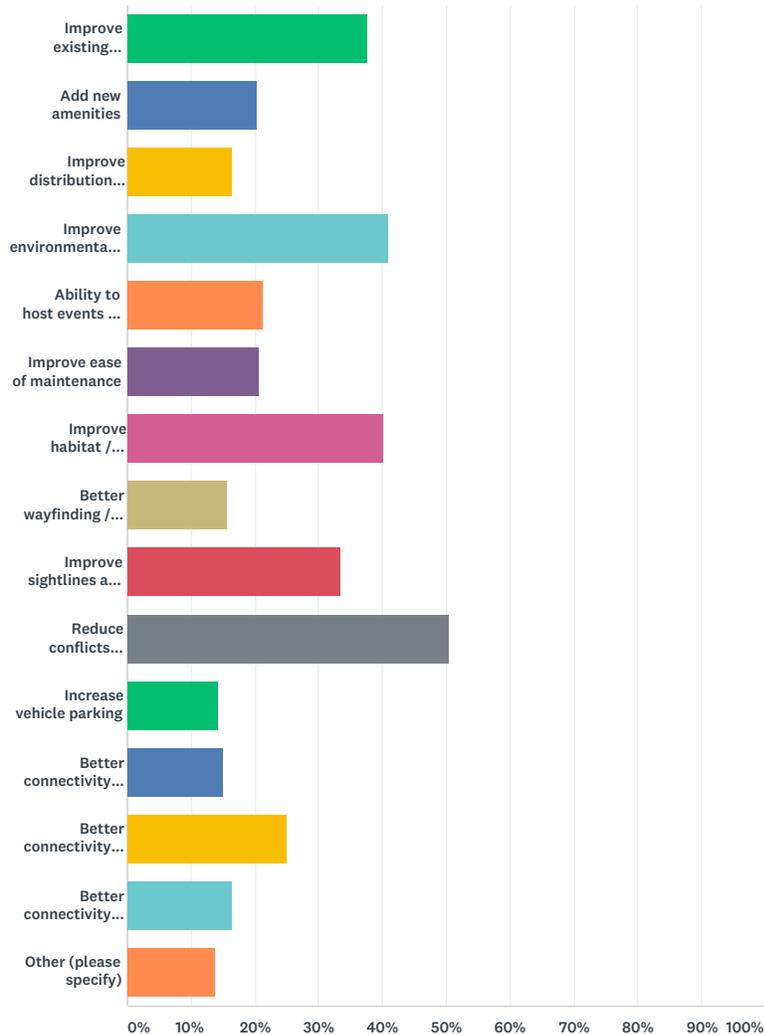
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| | | |
|--|--------|-----|
| Fitness Stations | 13.41% | 37 |
| Waterfront Access | 36.59% | 101 |
| Landscape (planted areas, trees, etc.) | 35.14% | 97 |
| Open Lawn | 28.99% | 80 |
| Interpretive Education | 3.99% | 11 |
| Other (please specify) | 25.72% | 71 |
| Total Respondents: 276 | | |

Q5 Master plans often involve “guiding principles” that help shape and prioritize design ideas. Please select your top 5 guiding principles below.

Answered: 266 Skipped: 10



| ANSWER CHOICES | RESPONSES |
|---|------------|
| Improve existing amenities | 37.59% 100 |
| Add new amenities | 20.30% 54 |
| Improve distribution of amenities | 16.54% 44 |
| Improve environmental sustainability | 40.98% 109 |
| Ability to host events and festivals | 21.43% 57 |
| Improve ease of maintenance | 20.68% 55 |
| Improve habitat / ecological systems | 40.23% 107 |
| Better wayfinding / signage | 15.79% 42 |
| Improve sightlines and visibility along trails and open space | 33.46% 89 |
| Reduce conflicts between trail users and other pedestrians | 50.38% 134 |
| Increase vehicle parking | 14.29% 38 |
| Better connectivity to adjacent neighborhoods | 15.04% 40 |
| Better connectivity to Town Center | 25.19% 67 |
| Better connectivity to transit | 16.54% 44 |

Aubrey Davis Park Master Plan kickoff

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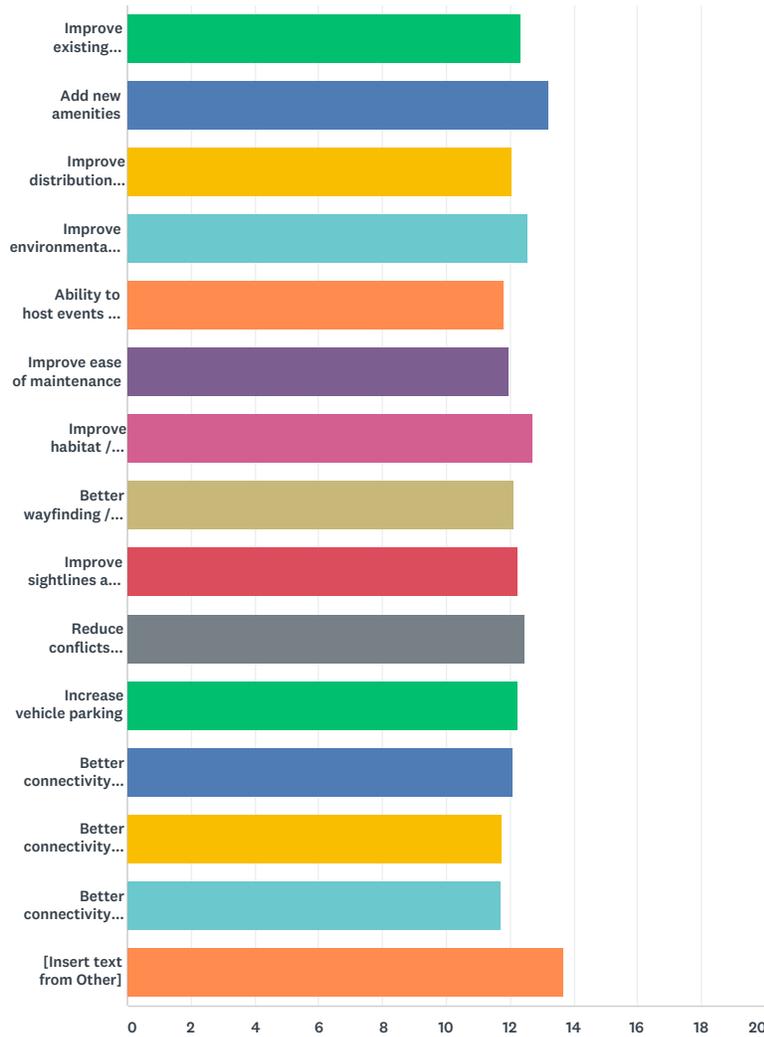
| | | |
|------------------------|--------|----|
| Other (please specify) | 13.91% | 37 |
| Total Respondents: 266 | | |

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Q6 Please rank your answers from the previous question. A rank of 1 means “Most Important” and 5 means “Least Important.”

Answered: 255 Skipped: 21



| | 1 | 2 | 3 | 4 | 5 | 6 | 7 | 8 | 9 | 10 | 11 | 12 | 13 | 14 | N/A |
|--------------------------------------|--------------|--------------|--------------|--------------|--------------|------------|------------|------------|------------|------------|------------|------------|------------|------------|-------|
| Improve existing amenities | 26.09% 24 | 23.91% 22 | 20.65% 19 | 13.04% 12 | 15.22% 14 | 0.00% 0 | 1.09% |
| Add new amenities | 46.00% 23 | 38.00% 19 | 12.00% 6 | 2.00% 1 | 2.00% 1 | 0.00% 0 | 0.00% |
| Improve distribution of amenities | 9.30% 4 | 23.26% 10 | 39.53% 17 | 13.95% 6 | 11.63% 5 | 0.00% 0 | 2.33% |
| Improve environmental sustainability | 25.74% 26 | 30.69% 31 | 19.80% 20 | 19.80% 20 | 3.96% 4 | 0.00% 0 | 0.00% |
| Ability to host events and festivals | 14.29% 8 | 17.86% 10 | 17.86% 10 | 23.21% 13 | 21.43% 12 | 0.00% 0 | 5.36% |
| Improve ease of maintenance | 9.43% 5 | 26.42% 14 | 28.30% 15 | 18.87% 10 | 15.09% 8 | 0.00% 0 | 1.89% |
| Improve habitat / ecological systems | 41.35% 43 | 21.15% 22 | 14.42% 15 | 16.35% 17 | 6.73% 7 | 0.00% 0 | 0.00% |

Q7 Please finish the following sentence: I love Mercer Island because . . .

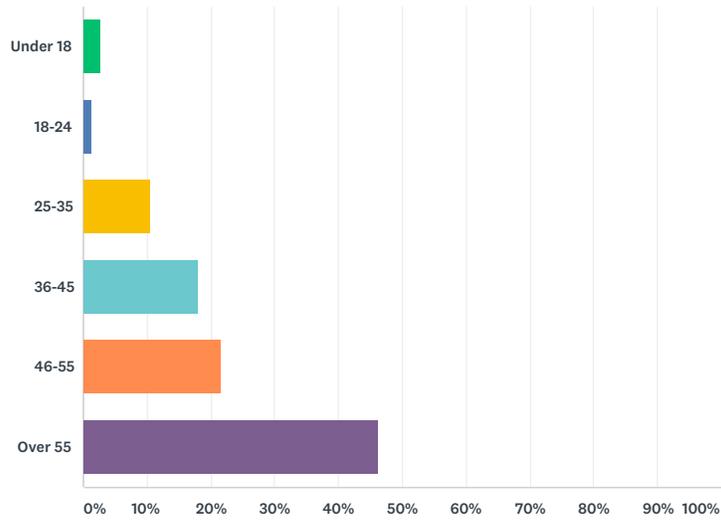
Answered: 230 Skipped: 46

Q8 Is there anything else you would like us to consider as we develop a new master plan for Aubrey Davis Park?

Answered: 157 Skipped: 119

Q9 What is your age?

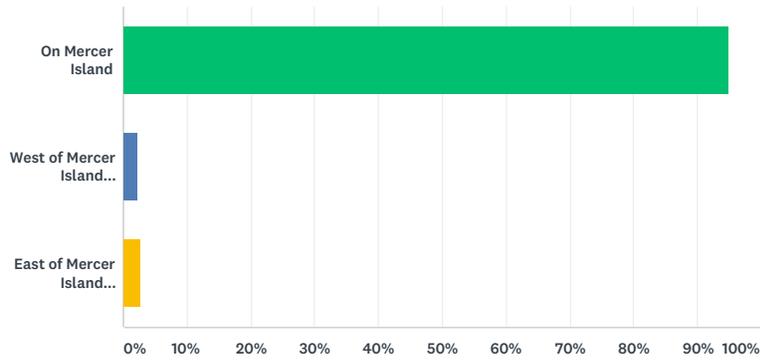
Answered: 251 Skipped: 25



| ANSWER CHOICES | RESPONSES | |
|----------------|-----------|------------|
| Under 18 | 2.79% | 7 |
| 18-24 | 1.20% | 3 |
| 25-35 | 10.36% | 26 |
| 36-45 | 17.93% | 45 |
| 46-55 | 21.51% | 54 |
| Over 55 | 46.22% | 116 |
| TOTAL | | 251 |

Q10 Where do you live?

Answered: 254 Skipped: 22



| ANSWER CHOICES | RESPONSES | |
|--|-----------|-----|
| On Mercer Island | 94.88% | 241 |
| West of Mercer Island (Seattle, Shoreline, Burien, etc.) | 2.36% | 6 |
| East of Mercer Island (Bellevue, Renton, Kirkland, etc.) | 2.76% | 7 |
| TOTAL | | 254 |

Aubrey Davis Park Master Plan

Trails Public Forum | SUMMARY

September 25, 2018

| Topic | Comment Summary |
|--|---|
| Trail section | <ul style="list-style-type: none"> ● 14' wide paved with 2' wide shoulders ● Separated 6' wide crushed surface trail where space allows ● Provide different types of trail types and experiences for different trail users ● Include a bypass route with improved on-road bike facilities along N and W Mercer Way and provide connections between the bypass route and the trail to allow a variety of choices ● Improve all trail intersections and conflict zones for safety and accessibility ● Provide looped trails within the park as well as the regional connections ● Regional trail route preferred to stay on the north side with south side more secondary access ● Extend trail on south side from Island Crest Way to City Hall and the waterfront |
| Trail amenities (north and south) | <ul style="list-style-type: none"> ● Benches ● Picnic areas ● Wayfinding ● Interpretive / educational elements ● Pedestrian lighting ● Activation / programming of open spaces along the trail ● Create destinations along the corridor ● Additional restrooms locations should be considered |
| The trail experience should feel... | <ul style="list-style-type: none"> ● Safe, serene, green, effective, protective, attractive, natural, balance of users, users separated, shared, not a thru park, fun, fast, calm, shaded, secure, predictability, connected, welcoming, accessible |



| | |
|--|---|
| <p>Trail users should include everyone...</p> | <ul style="list-style-type: none"> ● Walkers; walk commuters (to the bus / park-n-ride); skate boarders, scooters, kids on bikes, ride share (uber/lyft) waiting person, travelers (ie, w/ luggage), dogs-walkers, rollerblades, picnic-ers, kids learning to ride bikes, neighbors, high speed cyclists, recreation-ers, seniors, event participants and racers (ie, 5K to marathons), bike share users, visitors to Mercer Island (arriving by trail), group riders, e-bike riders (or e-scooters, e-boarders, etc.) |
| <p>The trail character should be...</p> | <ul style="list-style-type: none"> ● Urban, natural, green, have destinations, horticultural, separate paths for separate users, countryside, central park, benches, signed / wayfinding / rules, surface differences, less asphalt ● Unified but with some highlights of distinction for context of park areas, trail only corridors, and urban centers |
| <p>Trail safety</p> | <ul style="list-style-type: none"> ● Improve sightlines and sight distances, especially around curves and on slopes ● Improve accessibility overall and provide step-outs or rest areas where needed ● Improve accessibility and safety at intersections for all users ● Provide vegetation management to support trail safety and experience ● Provide cyclist-oriented traffic calming to reduce speeds and separate or better define different types of users |
| <p>Trail connections</p> | <ul style="list-style-type: none"> ● Increase connections to adjacent communities and neighbors ● Make sure connections include accessible options and routes ● Create clear connections (with wayfinding) to major destinations in the Town Center, Sound Transit, Luther Burbank Park, Community Center, the waterfront, City Hall, and other locations near and far ● Reduce cyclist speeds and increase safety at all connections ● Highlight entrances into the park and connection locations |

Aubrey Davis Park Master Plan

Trails Public Forum | MEETING NOTES

September 25, 2018

| Plan Area | Notes |
|-----------|--|
| General | <ul style="list-style-type: none"> ● Trail section: proposed is 14' wide paved with 2' wide shoulders and a separated 6' wide crushed surface trail where space allows ● The trail experience should be: safe, serene, green, effective, protective, attractive, natural, balance of users, users separated, shared, not a thru park, fun, fast, calm, shaded, secure, predictability, connected, welcoming ● Right now, the trail is: fractured, segmented, dangerous, noisy, confusing, cyclist / commuter focused ● Trail users include: walkers; walk commuters (to the bus / park-n-ride); skate boarders, scooters, kids on bikes, uber-waiting person, dogs, rollerblades, picnic-ers, kids learning to ride bikes, neighbors, high speed cyclists, recreation-ers, seniors, visitors to Mercer Island (arriving by trail), group riders, e-bike riders (or e-scooters, e-boarders, etc.), travelers w/ luggage ● The trail character should be: urban, natural, green, have destinations, horticultural, separate paths for separate users, countryside, central park, benches, signed / wayfinding / rules, surface differences ● There are a lot of ADA challenges on the trail now; also lot of conflicts and speeding from cyclists ● Like crushed rock path 5' – 6' wide for pedestrians / non-commuters (in addition to the 14' wide paved) ● Some walkers don't like pavement ● Reduce asphalt where possible ● Make N and W Mercer Way a bypass for cyclists – high speed route; bike lanes or other bike-friendly treatments ● Pushing cyclists onto N Mercer Way doesn't feel safe ● Prefer separated path for pedestrians ● Accommodate strollers in destinations ● Include mile markers ● Kiosks and wayfinding needed throughout, including to Luther Burbank Park ● Dog bags and trash cans needed ● Slow cyclists down at connection points ● Add benches, picnic all along the trail (north and south sides) |

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| | <ul style="list-style-type: none"> ● Preserve trail through MKA development ● Add signage along the trail for native plants, wildlife, etc. (interpretive elements) ● Add signage for dog areas (where it is acceptable and where it is not) ● Add educational elements / interpretive signage about wetlands or streams if there are any in the park areas ● Gateways could be located at E Mercer Way (cul-de-sac), park-n-ride, W Mercer Way east side near playground ● Provide more info on park history ● Consider more pedestrian lighting – maybe motion censored; for community and commuters; connection to Sound Transit station ● Don't put fast bikes through park, playgrounds and fields ● Provide a non-paved route from water to water |
| Segment 1 | <ul style="list-style-type: none"> ● Connection to the waterfront (to the west) needs wayfinding, more visibility ● Bicycles speeds excessive heading west; need safety and sight distances improvements, especially at the curve ● Need sight distance improvements around curve ● Can't see around on-ramp ● Add benches, picnic areas along trail on the south side of I90 ● Consider a divided path on trail on the south side of I90 with soft surface for pedestrians ● Trail on south side of I90 is hard to find, many don't know its there so need better visibility, entrances, and wayfinding ● Improve the kayak / canoe launch area at the waterfront |
| Segment 2 | <ul style="list-style-type: none"> ● Intersection at W Mercer Way is confusing, needs safety and accessibility improvements, wayfinding ● Add connection or crosswalk at entrance to parking, more like midblock and more direct to the playground ● Provide an on-street route along W Mercer Way headed north (bypass route) ● Conflicts between people moving to and from the playground and bicycles trying to get through on the path, especially when the bicycle turns down W Mercer and then tries to cut back to the trail to the south ● Need more wayfinding at trail crossing to clear where bicycles should go to stay on trail or go on bypass route ● Add pedestrian lighting or bollards on trail to Town Center ● Add lights at wayfinding or other signage |

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| | <ul style="list-style-type: none"> ● Provide loop path lawn area and connecting courts to the trail ● Provide better connection from the parking area on SE 22nd Street to the courts and open lawn area ● Do something with the stacks so more attractive, maybe a spot for covered picnic ● Create more of a gateway to the lid park at the intersection here ● Add lights along the trail (pedestrian priority but nice for pedestrians and bicycles) – continue through to lid connection to Luther Burbank but priority from W Mercer Way to the park-n-ride ● Stacks could be a focal point and draw for people if more interesting (art, trellis, canopy shelter, etc.) ● Add a bathroom near the corner of the parking and the W Mercer Way (east or west sides) ● Consider separate crossings for different users at intersection of the trail and W Mercer Way – pedestrians, EB cyclist, WB cyclist – and raised table or other approach to make it safer ● Need better sightlines and visibility where trail comes up the hill towards the parking off SE 22nd Street, difficult to see around the curve in trail ● Consider trail separated (bridge) over W Mercer Way, would reduce speed and align with higher elevation by parking lot near courts for greater separation of uses |
| Segment 3 | <ul style="list-style-type: none"> ● Need safety improvements where trail comes down the hill and around a blind corner into the restroom and field area – big conflict zone; add elements to slow down cyclists ● Don't move main regional trail to south side ● Open space used a lot next to playground ● Connect east side of fields to the south side trail ● Consider a trail-sized roundabout at conflict corner ● Need better visibility and sightlines at corner / under the overpass area ● Consider moving trail to the west of the restroom/maintenance building (around the back side) to avoid conflict with park users ● Need safety improvements to reduce speeds coming down the hill along the trail to the north side of the lid ● Need to improve connection on the south side towards the Town Center and add more wayfinding, but main trail should stay to the north side; trail on the south side could be narrower so it doesn't feel as much like the main regional trail route ● Consider programming and other opportunities in open space to the east of the playground ● Improve connection to 74th Avenue SE |

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| | <ul style="list-style-type: none"> ● Consider more of a connection from the south side along the east edge of the lid to the main trail on the north side, this would avoid the conflict area in the middle ● Provide separated trail between bicycles and pedestrians along the south side of the open space area |
| Segment 4 | <ul style="list-style-type: none"> ● Need better crossing at off ramp to 77th Avenue SE; feels unsafe now ● Need safety improvements along trail on the north side of I90 where adjacent to N Mercer Way – wider, better separation, trail surface improvements, visibility, etc. ● Would like to see a more direct crossing of the trail on the south side of I90 (intersection of 76th Avenue SE and SE 24th Street); prioritize the pedestrians / trail users at this intersection ● May need a trail-sized roundabout at intersection of south trail and 76th Avenue SE ● Add benches on south side ● Park-n-ride should be a walk only zone ● Include opportunity for bike-share parking |
| Segment 5 | <ul style="list-style-type: none"> ● Need directional signs / wayfinding at corner of N Mercer Way and SE 24th street for bypass route; and improve the street for a better bypass route ● Improve treatment at the bus stop in front of park-n-ride so no ride zone, priority to pedestrians – need bicycle calming, more clear signage, hierarchy of treatment to pedestrians first, etc. ● Do not encourage bicycles / trail connection along 81st Avenue SE from SE 24th Street to N Mercer Way ● Preserve the trail on the south side through this segment, but north side should be primary trail ● Improve walkways and trail connections along bridges to get from north trail to the Town Center, needs more visibility and nicer walking route as well as intersection crossing improvements so feels safer (enhanced vegetation, lighting, paving, etc.) ● Need crosswalk wider or relocated to align to the trail at intersection of SE 27th Street and 80th Avenue SE; maybe crossing flags or other safety improvements ● Add “bikes too” to stop signs – when bicycles are on the street (bypass route) they forget to obey the street signs ● School buses load at park-n-ride too, so need to consider how they will function with any improvements or changes to the system, right now doesn't feel safe for school kids ● Bike parking needed at Town Center, transit area and activity zones – signage and wayfinding to the bike parking and lockers needed as well ● Need safer intersection crossings for north trail ● Add a restroom at the park-n-ride or at the new Tully's development site |

Segment 6

- Lid area is not visible from roads or other public areas, feels unsafe
- Access to this lid area is not very visible – need signage, improved entry, more sightlines and views into the area, etc.
- Provide stairs from SE 28th Street into the lid area
- This feels like a pass-through but should be a destination – needs activity, reason for people to be here
- Sign the pinch point so warning / added caution
- There is an existing informal trail from overlook down to roadway; steep grade
- Need better access to Covenant Shores area
- This lid feels isolated
- Need safer intersection crossings for north trail
- Opportunity area at corner of Island Crest Way and SE 28th Street, open space could be improved and help get people to use the lid area here better
- Need more formal entrance to the lid – something visible from SE 28th Street
- Uses for lid area – sculpture area, garden / demonstration planting, picnic, other passive uses
- Connect from this lid down to Luther Burbank, make the connection more visible
- Need safe crossing between Luther Burbank and the open space adjacent to I90 (across N Mercer Way)
- Consider using SE 26th Street as a connection from the bypass route back to the trail
- Bypass route on street would continue on N Mercer Way through this area
- Trail on north side should include separated trail for pedestrians (soft surface) where space is available
- Continue a secondary path from the lid area along the south side of I90 through the wooded area (area has some steep slopes)
- Add signs about the history of the area at the top of the hill (base of the overlook)
- Move the bench so you can sit and still see the view at the overlook
- Add picnic area and benches to the lid area
- Add signage about history of Luther Burbank Park to the overlook area; like the signage around the community center as a good example

Segment 7

- Would like to see a trail through the forested area on the south side of I90
- Need more connections to the neighborhood between the north trail and N Mercer Way
- Intersections with the trail crossings need safety improvements
- This might be a good place for the bypass route to connect back into the trail so users have options for on or off trail experience (like a left turn lane for cyclists heading westbound)

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| | <ul style="list-style-type: none"> ● Add a westbound bike lane on N Mercer Way for improved bypass route ● Provide lighting long trail |
| Segment 8 | <ul style="list-style-type: none"> ● Improve connection to City Hall – from north trail, from south secondary trail, from both east and west sides (N Mercer Way and E Mercer Way) ● Need better crosswalks, safe crossings at intersections east and west from City Hall ● Need better wayfinding so can use City Hall parking as trail head parking on weekends ● This section of the trail on the north side feels isolated, need more wayfinding, visibility, clear entrances to the trail system, etc. ● Make the trail feel more permeable to the neighborhood along this segment, more connections ● Consider parking, trailhead type improvements at the existing cul-de-sac off SE 35th Place, connect better to trail (short term trailhead parking) ● Need better wayfinding and letting people know there is waterfront access at east end; potential destination; better path along the water ● Improve connection, provide a clear route from trail (and City Hall) to the waterfront ● Provide lighting along trail ● Create connection to SE 40th Street ● Add a restroom at the boat launch / waterfront area ● Add lights along connection from trail to City Hall |



Aubrey Davis Park Master Plan

Sports & Boating Public Forum |

SUMMARY

October 25, 2018

| Topic | Comment Summary |
|--|--|
| Sports users include... | <ul style="list-style-type: none"> ● Walkers, school sports teams, senior sports teams, day camp users, dog owners, Special Olympics, families |
| Sports Amenities | <ul style="list-style-type: none"> ● Pedestrian lighting ● Field improvements ● Nearby views ● Signage & wayfinding ● Increased parking ● Increased accessibility |
| Sports Field improvements | <ul style="list-style-type: none"> ● Field C is the most ideal for synthetic turf and lighting improvements ● Field B needs a safer pedestrian crossing across W Mercer Way ● Create parking area for Field B ● Increase parking capacity at Field A ● Keep Field A grass; improve maintenance ● Add low buffer at Field A to prevent balls from running into trail ● Improve safety between active and passive users at all recreation areas |
| Water Access users include... | <ul style="list-style-type: none"> ● Boaters, walkers |
| Water Access amenities (east and west) | <ul style="list-style-type: none"> ● Benches ● Picnic areas ● Signage & wayfinding ● Pedestrian lighting ● Additional restrooms ● Increased parking ● Increased accessibility |

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| Water Access Improvements (east) | <ul style="list-style-type: none"> ● East boat launch is well-known and used heavily by boaters. Can increase capacity through improved parking and restrooms ● Currently most utilized by a specific user group (boaters) during specific times of the year (summer). Can increase the type and timing of use by creating a more park-like environment for year-round appeal |
| Water Access Improvements (west) | <ul style="list-style-type: none"> ● West water access is not as well-known or used. Create a sense of place and reasons to come here through signage and programming ● Desire for a family-oriented water access area, with interest in swimming, picnic access, and restrooms ● Interest in creating a dog park here as well |
| Sports and Boating connections | <ul style="list-style-type: none"> ● Highlight entrances into the park and connection locations ● Promote unique identities for recreation areas, especially for the two water access areas ● Increase connections to adjacent communities and neighbors ● Make sure connections include accessible options and routes ● Create clear connections (with wayfinding) to major destinations in the Town Center, Sound Transit, Luther Burbank Park, Community Center, the waterfront, City Hall, and other locations near and far ● Reduce cyclist speeds and increase safety at all connections |



Sports & Boating Public Forum |

MEETING NOTES

October 25, 2018

| Plan Area | Notes |
|-----------|---|
| General | <p>Landscape</p> <ul style="list-style-type: none"> ● Priorities should be to maintain a natural setting, create multi-use areas, and balance between open space and active use ● Central Park in NYC as an inspiration ● Save green space and keep it quiet, with buffers from sports fields ● Consider dog stations along corridors ● Use native planting <p>Recreation</p> <ul style="list-style-type: none"> ● Sports to consider adding: ultimate frisbee, wall ball, frisbee golf, putting green or pitch & putt, pickleball at tennis courts ● Connect uses to trails, Park n Ride, Light Rail Station, and Luther Burbank <p>Trail</p> <ul style="list-style-type: none"> ● Transit corridor use is important; repair cracks and heaves ● Find ways to create variety in a lineal corridor ● Great views from the west end soccer field ● Segregate uses to improve both experiences for bicycles and pedestrians ● Transit corridor to allow for multi-usage and maintain well ● With wide paths, what safety barriers will there be to keep vehicles off the paths? ● Like quietness, don't want fast bikes ● Separate bike lanes similar to Green Lake ● Connections between bike path through city hall |
| Segment 1 | <ul style="list-style-type: none"> ● Boat launch at west end needs a name ● Many not aware there is a water access here ● Water access area is loud, dark, and not inviting ● How could this become a gradual slope? ● Expand parking in the water access area ● Could use a picnic access or restroom here, also a potential area for a dog park ● Potential area for boat launch at west end ● Potential swimming at west end; could have a beach to activate area ● Make west end water access more family-friendly |

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| | <ul style="list-style-type: none"> ● West end a good place to teach kids how to drive ● Uncomfortable with activity from people parking at the west end ● Other water access is better ● Works with any trailer ● Possible 2-lane access |
| Segment 2 | <ul style="list-style-type: none"> ● Like that multiple teams can be scheduled in this area ● Ability to keep kids in one place ● Maintain edges, keep clear, and use herbicide types not harmful to kids or pets ● Like option for different fields ● Keep natural character, with green trees ● More dog bags & cans along trail needed ● Keep all trees ● Bikes need to slow down in park ● Consider adding a trail roundabout ● Keep path as is ● Like variety where walking ● Like no lighting or low-key lighting ● Like that you can walk through, prefer segregated bike usage <p>Field C</p> <ul style="list-style-type: none"> ● Field C as a destination for walking, with nice views ● Use fields for ultimate frisbee ● Too much use of field C – difficult to maintain ● Restroom needed near west parking lot ● Parking tight at field C ● Better drop-off flow at Field C ● Field C is best location for turf and sport lighting ● Boys Lacrosse in Area C year-round ● For Boys Lacrosse use, grass here has divot holes, bad drainage, time of year, no restrooms, and inconsistent field quality. Would prefer to have turf at Field C ● Field C not full sized for soccer, nor level enough. Ok for games & practice ● Field C can connect to “wall ball” at field B ● Improve drainage if needed for Field C <p>Field B</p> <ul style="list-style-type: none"> ● Field B not preferred for soccer; small, sloped, and there are better fields ● Field B needs parking and crossing across street ● Keep calm and quiet near Field B ● Field B slopes too much ● Girls Lacrosse uses basketball courts as a ball wall |

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| | <ul style="list-style-type: none"> ● Boys Lacrosse uses basketball courts as a ball wall ● Improve basketball courts <p>Tennis Courts and Upper Field Area</p> <ul style="list-style-type: none"> ● Parking near vents often full because construction tends to park there ● Don't see a lot of use of tennis fields; too windy and exposed ● Best views from tennis fields ● Repurpose tennis fields to make better use of the views? Perhaps an outdoor theater space or community gathering space ● Like an amphitheater option instead of tennis fields ● Camouflage vents ● Vents as a destination ● Green vents preferred to murals ● Could have street art at vents ● Camouflage wall behind basketball courts ● Use greenery or murals at vents |
| Segment 3 | <ul style="list-style-type: none"> ● Like the convenience, restrooms, drainage, and well-maintained fields at Area A ● Like programmatical variety in this area ● Area A parking lot is small but crucial ● Not enough parking here ● Restroom here is aging ● Conflict zone as trail crosses under bridge ● Consider trail roundabout under bridge ● Bikes too fast through here ● Conflicts on trail due to fast bikes <p>Field A</p> <ul style="list-style-type: none"> ● No sport lighting - path lighting closer to town center and more populated areas ● Lighting at fields a good idea ● Keep field grass ● Field could use low fencing ● Turf fields preferred for field C or homestead, but prefer all turf kept in same area ● Girls Lacrosse in Area A for practices ● Divot holes in grass not ideal ● Rockers (Senior Softball) uses Area A w. field; balls hit across trail and into trees ● Baseball 7 days a week in the spring – like having natural grass options, great location ● Cones as fences work for baseball (fences themselves need more work and pose a storage issue) ● Need fences for All-Stars ● Softballs go over trail sometimes |

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| | <ul style="list-style-type: none"> ● Mercer Island FC uses Area A in the fall. Not full sized, so good for younger us. Level and safe ● Mercer Island Boys & Girls club / Little League use at Area A; day camps for little kids ● Day camp usage on Field A conflicts with walking to restrooms, etc. ● Backstop area at field tight ● Dog owners off leash during softball is dangerous ● Dog owners don't clean up ● Bases sometimes moved, but switched out so no issues ● Special Olympics – bus stop here is crucial <p>Shelter and East Field</p> <ul style="list-style-type: none"> ● Good relationship between athletic field and shelter area ● Use shelter as picnic/potluck for large groups ● Use field by shelter to east for dogs? Not official OLA, but direct dogs here ● Field to the east slanted and not as well-maintained ● Foul balls at east field get lost in bushes; need netting over backstops in general |
| Segment 4 | <ul style="list-style-type: none"> ● [No notes] |
| Segment 5 | <ul style="list-style-type: none"> ● [No notes] |
| Segment 6 | <ul style="list-style-type: none"> ● Well hidden; most did not know it was there ● Many have never been to mini-lid ● Under-used ● Mini-lid a hidden gem – why change it? ● Mini-lid used to watch blue angels ● Nice, private, natural around mini-lid ● Parking – leave as-is, don't have to worry about bikes going by here ● Needs better parking access ● Signage important here ● Signage to connect to entrance; make stronger connection to Burbank from mini-lid ● Light rail connection ● Nice trail from town center ● Take pressure off town center open space here ● Distinguish uses from other town center parks like Meadowdale, sculpture park ● Bikes go way too fast around trail ● Access point at intersection between SE 28th and Island Crest Way ● Possible opportunities for art, lighting, and athletic fields ● Use mini-lid for dog walking and natural areas; however can compete with families and small kids. Make a designated dog walking area |

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| | <ul style="list-style-type: none"> ● Separate dogs from kids ● Designated dog areas |
| Segment 7 | <ul style="list-style-type: none"> ● [No notes] |
| Segment 8 | <ul style="list-style-type: none"> ● Big parking lot ● Fishing off-pier ● Need restroom here ● Boat launch ● Use as a ski bus meeting spot ● Great for boaters; well-designated ● Gets congested when busy |



Landscape & Open Space

Public Forum | SUMMARY

November 8, 2018

| Topic | Comment Summary |
|-------------------------|--|
| Landscape amenities | <ul style="list-style-type: none"> ● Mature trees ● Open space ● Seasonal color ● Noise buffering along I-90 ● Wildlife, ecological functions |
| Landscape improvements | <ul style="list-style-type: none"> ● Keep existing overall character ● Consider more native and ecologically appropriate plants ● Ivy and invasive removal ● Improved maintenance ● Vegetation management where overgrowth is unsafe or blocking views ● Plant identification signage |
| Open space improvements | <ul style="list-style-type: none"> ● Signage for wayfinding and connectivity ● Improved accessibility for those with limited mobility ● Inclusive play integrated into parks ● Separation of users where there are conflicts (i.e. commuter bikes, off-leash dogs) ● Additional trash cans, benches, etc. ● Lighting (pedestrian-scale) for improved safety, but not lights everywhere ● Great views at tennis courts – consider reorganizing space to improve access to viewpoints, more connectivity to open space ● Organize and program spaces for year-round use where feasible ● More views into open space (safety / security) and out towards water or I-90 ● Balance of programmed and unprogrammed space ● Space for passive recreation |
| Connections | <ul style="list-style-type: none"> ● Connect Luther Burbank Lid to adjacent areas ● Connect town center across to the east south of I-90 ● Ensure connection continues through new Tully's site |

Landscape & Open Space

Public Forum | MEETING NOTES

November 8, 2018

| Plan Area | Notes |
|-----------|---|
| Overall | <p>Most important about park and open space:</p> <ul style="list-style-type: none"> ● Green space and open space ● Safety ● Connectedness ● Continuity from one end to another ● Mature trees ● Views (when possible) from park ● Healthy vegetation ● Open forested area (with views through) ● Native plants, ecologically appropriate plants ● Good signage/wayfinding / park maps ● Plant identification signage ● Bordered by large canopy trees ● Remains passive; "no vibrancy" ● Ballfields ● Speed reduction on bike path (ex. Trail roundabouts) ● Friendly for wildlife/birds ● Dogs off-leash / dogs not off-leash (safety concerns for people) ● Concerned about transit parking <p>Most important about existing park</p> <ul style="list-style-type: none"> ● Light, plants, flexibility, serenity, accessible paths, benches, safety, inclusivity, walking (preserve loop), dogs, need more bus stops at east end, safety concern with fast bikes, love variety within park (sports, etc.), diversity of users, plenty of trash cans, views of water, plants/vegetation around trail, peaceful place, need more access points where you can pull up/lock boats |

General Improvements

- Fast moving bikes should go on road along N Mercer Way
- Bikes along N Mercer Way would be less inclusive, less safe than along regional trail
- Food truck areas near fields A and B, near future Sound Transit, Luther Burbank Park, and east end boat launch
- Overall character: bring community together; more native, less maintenance; wild life forage
- Signage throughout
- Preserve unprogrammed open space
- No more picnic tables/benches
- Add more benches
- More trash/recycling cans
- "Friendship" circle

Landscape

- Clear out invasive plants and add natives in neighborhood near Luther Burbank Park
- Trail gets mossy, with roots buckling the trail surface – needs to be improved
- Noise buffering from trees
- Large trees
- Left natural
- More trees that get fall color – big maples
- Evergreens along bike trail to buffer sound
- Get rid of ivy/invasive plants
- Edges of the lid – keep for buffers
- All native plants

Connections

- Playgrounds or access to nearby private schools for public access to their playgrounds
- Maybe playground at open space near Island Crest Way (near Upper Luther Burbank lid)
- Better connection from Luther Burbank Lid to adjacent areas
- Connect town center across to the east south of I-90
- Distance markers on trails
- Disability access signs
- Need trail along south side of I-90 through town center
- Worried about connectivity with new Tully's site
- Formalize goat rail uphill towards Upper Luther Burbank lid
- South side of I-90 from town center to Gallagher Hill: make pedestrian only
- Better crossing near SE 36th near North Mercer Way (near city hall)
- Create shortcut from Snake Hill down to Luther Burbank lid – clear out invasives

- Concern about losing green space around Tully's – and preserve that path. Currently not well maintained, Tully's development may improve preserve/create connection
- Place for food trucks

Waterfronts – East End

- Works well now
- Kayak rentals would be good if they don't do this already
- It's a dead-end trail – create loop
- Better signage
- Uses during winter
- Add spaces for kids with disabilities

Waterfronts – West End

- Feels like it's always under construction
- Better signage
- Crosswalk in front of dog statue/playground
- Lots of litter
- Root barriers
- No root barriers
- Add spaces for kids with disabilities
- Low level lighting – walking at night
- Decrease money to near water and use money to do more thinning
- Planned thinning is needed
- Shrubs by playground and W Mercer Way are overgrown and leaning on walk
- Like wildflowers on Island Crest Way – do this elsewhere
- Water taxi stop – find the stop and include interpretive elements or sign
- Beach
- Boat launch – hand carry on trailers
- Connection to water trails
- Swimming
- Lights
- Status quo – leave along
- Periodic events – recycle, etc.
- Access good
- Better signage/directions to boat launch

Segment 1

- Beach

| | |
|-----------|--|
| | <ul style="list-style-type: none"> ● Small boats ● Like street end ● Only individual/non-motorized boats ● Water trails connections / on water trail maps, etc. ● Swimming ● Lights ● Water taxi – might be better on east end with parking ● Feels like under construction ● Need to clean up ● Not welcoming ● Signage needed here and on street ends ● Lots of litter where I-90 ramps intersect with W Mercer Way ● Crosswalk needed to playground across W Mercer Way |
| Segment 2 | <p>Current Use</p> <ul style="list-style-type: none"> ● Walk dog in Area B, go to Freshy's and back ● Dog run at the stacks and picnic shelter lawn ● Seafair at trail entry east of W Mercer Way ● Picnic at stacks, shelter ● Walking, biking, run, jog ● Views from Lid C ● Family activities ● Seafair ● Tennis courts used every day in summer ● Loop route: tennis courts → WMW → Stacks → Lid A → Trail → Park & Ride → ICW → Overlook lid → Town Center → Return ● Playground ● Picnic shelter – can handle large group; “no dogs” (sign) in that area ● Path for biking ● Basketball courts ● Dog walking – stacks, Lid A ● Homeless hang out by stacks ● Tennis courts get in the way of views ● Mercerdale is better for events ● Overall landscape character is perfect as-is, but lacks maintenance ● It's a nice park – love it ● Should not become more active <p>General Improvements</p> |

- High priority: accessibility and maintenance
- Add trash cans
- Don't over-program – love flexibility of open space
- Walking – north end, not good signs, where work trucks use porta-pottis
- Bike in front of restroom creates conflict. Trail should go behind restroom
- Tennis courts block view – substitute amphitheater
- Bikers could stay on south side to town center
- Get serious bikers off the trail
- Overlooks, view blocked by wall at Lid C
- Would be good parking (SE 22nd Street)
- Nice to park and enjoy view
- Size of path should not change

Open Space

- Inclusive playgrounds
- Dying vegetation addressed
- Trash pickup, more trashcans
- Keep space flexible, open, “not crammed with stuff”
- Accessible to people with limited mobility – go to pops for input
- Exercise stations
- Repurpose tennis court area
- Food truck
- Restroom with single use rooms
- Trail has to be safe, especially with bikes
- Speed bumps
- Don't want high capacity trail
- More maintenance, irrigation
- Keep grass
- Replace trees
- No turf, no lights
- Pickleball
- No turf, no lights
- Keep B and C as park
- Barrier vegetation at field C too high
- Tall hedges at Field C block beautiful sunset
- Need bathrooms at Field C
- Field C feels underutilized – along with Field B, this used to be a large gathering area for fireworks
- Like that Field B is open with not too many trees

- No more picnic tables, benches
- No increase in impervious surface

Landscape

- Ivy – dislike, invasive
- Trees – like amount now, fall color good
- Walk from WMW to backside of stacks, good fall color
- Trees too tall in view corridors on WMW
- Hedges encroach on paths along WMW
- Not too many trees, like light
- Alternative groundcover = less mowing
- Finish concrete with interesting treatment
- P-Patch or flower garden in Area B open space
- Trails need more light, like bollard lights. Solar lights are not enough
- More areas to sit
- Like trees – lower height on the edge of view corridors at Lid A and Lid C
- Keep tree buffer – like tall tree perimeter
- Line of trees along 24th west of 79th
- No bushes for safety
- Small bushes
- Pockets of trees east of parking under overpass
- No bushes in area under overpass for safety – only low-growing shrubs here please
- Love dogwoods north of tennis courts
- Overgrown vegetation at trail entry east of W Mercer Way
- More native
- Use less water
- Wildflowers
- Perennials/no annuals
- More seating
- Community

Basketball Courts

- Basketball courts get used a lot
- On trail north of basketball court, it's hard to know where you're going

Tennis Courts and Stacks

- Great views from tennis court
- Highest point and best view from tennis courts
- Tennis courts get used a lot

- Very windy at tennis courts, maybe not best place for tennis
- Keep tennis courts
- Amphitheater at tennis courts
- Pickleball at tennis courts
- Don't like tennis courts
- No views
- Keep paths between tennis courts and SE 22nd the same width – wider trails will attract more bikers
- Dog walkers at stacks
- Informal dog park at stacks
- Transients and homeless hang out at stacks
- Walk to town center through trail

Segment 3

Current Use

- Views to the northeast from fields
- School organized events at playground – very popular
- Dog walkers in field east of playground
- Lid A underutilized – need bathroom, bikes might stay off center trail
- Picnic shelter great for large gatherings
- Big conflict area between restrooms and field A (peds and bikes)
- E-Bikes are a problem – too fast and less experienced
- Love the number of trees and plants
- Love the light
- Like variety of species of flowering/colors

Improvements

- Make bikes go around back side of restroom
- No more programmed space
- Love the park as is – less is more
- Pervious/impervious surface balance – keep existing balance
- Put amphitheater back of playground
- Like trees on perimeter
- Like some separation along edges – adds to park-like atmosphere
- Connect bikes through area close to lid edge? Take commuters and serious bikers out of the park
- Food trucks in parking lot under overpass?
- Better signage to fields
- More seating at fields
- Need exercise equipment – would be a great addition

| | |
|-----------|--|
| | <ul style="list-style-type: none"> ● Bring food trucks (would help small kids, special needs) ● Change flat greenery to reduce mowing – clover ● Need lighting – without it there's tripping on the trail ● Keep buffer at edge of lid |
| Segment 4 | <ul style="list-style-type: none"> ● Nice area on south side of I-90 directly east of 76th Ave SE |
| Segment 5 | <ul style="list-style-type: none"> ● [none] |
| Segment 6 | <ul style="list-style-type: none"> ● Trail connection from I-90 trail to Luther Burbank Park could be part of the sculpture garden, with signage ● Add signage as Luther Burbank access to lid ● South side of the Luther Burbank Lid - improve signs, vegetation, and have a trail sign ● Views blocked now into Luther Burbank Lid ● Connect Luther Burbank Lid from southeast side ● Enhance pocket park on Snake Hill ● Need connection from Island Crest Way intersection to Luther Burbank Lid ● Island Crest Way open space: great fall color, need signage here to welcome visitors to Luther Burbank, needs focal point (fountains, etc.) ● Fountain or bocce ball at intersection of Island Crest Way and SE 28th Street |
| Segment 7 | <ul style="list-style-type: none"> ● Pedestrians only in forested area east of 84th Ave SE ● Playground near Shorewood Heights neighborhood ● Existing trails in forested area south of I-90 and east of 84th Ave SE – connect these throughout |
| Segment 8 | <ul style="list-style-type: none"> ● Bus access gone south of SE 36th St and N Mercer Way ● Hard to cross from SE 36th St across N Mercer Way ramp, and development in area makes it worse ● Keep kayak rentals ● Need year-round options for boat launch ● Boat launch works good now ● Ski buses ● Recycle fair ● Enough parking ● Signage improved (people get lost) ● Walking is dead-end with sidewalk ● Better signage |

Aubrey Davis Park Master Plan

Arts & Culture Public Forum | SUMMARY

November 29, 2018

| Topic | Comment Summary |
|----------------------------|--|
| Art improvement guidelines | <ul style="list-style-type: none"> ● Use art to help improved wayfinding and access ● Greater variety in types of art, especially interactive art ● Greater enjoyment of art for all ages ● Greater relevance in art subject matter ● Improved connection between art installations ● Stronger integration with the surrounding context ● Respect the natural environment as locations and placements for art are considered |
| Types of Art Desired | <ul style="list-style-type: none"> ● Interactive art – consider age groups, natural processes, functionality, and life cycles of art (long & short-term work) ● Art that ties into public utilities and functional aspects of the site ● Meaningful art – consider cultural resources, nature, connections to history, etc. ● Artful landscape & eco-art ● Iconic work/signature piece/landmark art desired by some; others want to keep park natural and ensure art doesn't overpower the site |
| Preferred locations | <ul style="list-style-type: none"> ● Light Rail station and related utilities ● Connect places where art is installed, especially between Town Center, Mercedale Park, Upper Luther lid, and Luther Burbank Park ● Priority for new art should be underserved areas where beautification is needed ● Keep lidded park natural and uncluttered with art ● Art at boat launches should be calm and meditative |

Aubrey Davis Park Master Plan

Arts & Culture Public Forum | MEETING NOTES

November 29, 2018

Plan Area

Notes

Overall

What do you like about existing art on Mercer Island?

- Adds to your walks in the park
- Place to stop, introduce to friends, comment & reflect
- Multi-use for sculpture gallery (e.g. walk dogs, stop to look)
- Earthwork (source) → moving in it, interactive elements
- Intergenerational works
- "Cyclemates" invites interaction & its connection to the community
- Immersive art experience; moving through the stacks for example
- Spatial element
- "Playful Pup" is a favorite among the community
- "Flock" is also well-known
- "Primavera" and "Gateway Figures" are iconic
- "Primavera" is vibrant, colorful, big – pulls people in
- "Primavera" – first sculpture, a lot of effort to bring it to Mercer Island
- Very visible; drive by "Primavera" a lot
- Bench & chaise are really beautiful sculptures
- Stumble upon works of art – the joy of discovery (like with the Yearling)
- Tree grates in Town Center are beautifying something that's already there
- Works that incorporate natural elements; work with what's there
- Underserved parts of the park – fill in the areas that don't have as much art, but also maintain
- Art one of the first things noticed
- The Source – don't really know it's art, but kids interact with it
- The Yearling and Playful Pup – small scale attracts kids
- The Source is inviting, durable, and playful
- Like interactive elements of works like Cyclemates

What could be improved about existing art on Mercer Island?

Type of Art

- Musical work to engage audiences
- Artwork that responds to or intervenes with how site is used – biking, pedestrian
- Science and technology to create interactive artwork
- Life cycle of art – timelines – long and short-term work
- Shifting of work – reuse or natural resources (fallen trees, other natural materials)
- Create functional work (tree houses, bird habitats)
- Art that creates function and protection (barrier from the rain, responds to the elements)
- Art and experiences that change over time – topiary, natural changes...
- Green/eco art
- Artful landscape is lacking
- No arbors like other communities – walking through the art
- Miss viewpoints in the park; see the sunset
- Would be nice to experience the works at night in the dark
- Experiences for teenagers are lacking
- Not any of the works are interactive
- Kids sitting on Paso Colt a lot
- Kids want something they can climb on – probably why they like “Between Two Worlds” at the library
- Scale allows for different experiences/entry points
- Facilities aren’t conducive for performers/creating art
- No space for temporary experiences/installations
- Place to display kid/class art
- Temporary exhibition space with Sound Transit would be nice
- Hidden sculptures you can interact with your kids
- Art should make you think/feel
- Vision for our current collection to integrate, to make more approachable, to make more accessible, to learn more about it
- Art can be powerful – consider if its too provocative for Mercer Island
- Not a lot of relevant art, or temporary art/space for temporary art
- Smokestack area needs art
- Lacking traditions around existing pieces – how do we create those traditions
- Art is random and not well-displayed – pieces I like and pieces I don’t but area is plain (GHSG); prevents the opportunity to interact
- People aren’t going to the park for art but art you would stop to look at collections; don’t see people on the benches looking at art
- Collection mentality – not enough understanding about the why
- Need unified design

- Create outdoor facilities/space for pop-up performance (e.g. busking)
- What's the theme/vision for art/park
- Stacks view impacts neighbors – art can add value to neighborhood
- Create something iconic on Mercer Island – the Stacks
- Incorporate musical spaces – interactive “ wall”
- I-90 noise – can it be used as part of art experience (i.e. structures that help reflect sound)
- Expand art experience into Light Rail – work with Sound Transit
- Consider maintenance implications
- Vision and character of Aubrey Davis Park
 - Original intent included art bridge to bridge
 - Create art experience for park users and cyclists
 - Gathering place for cycling groups
- Consider how to pay for art
- Performance art/buskers near Luther Burbank Park
- Tully's site – create space for art here
- Use Aubrey Davis Park to revitalize Town Center
- Interactive, educational – teaching gardens
- Find a balance, don't make park too busy
- Use our climate
- Engage younger ages (teenagers)
- More vegetation/landscaping around ballfields areas – create barrier, keep flow of natural park
- Balance of art/natural spaces – respect nature that exists, including art
- Landscape architects involved in art plan
- Create habitat spaces (birds, butterflies)
- Tennis courts – opportunity for more green
- Art is random
- Keep grass and trees
- Don't add more sculptures
- Lid park does not need sculpture
- Art focused on wildlife (birdhouse, butterfly gardens, dead trees that are bird habitats, etc.)
- Habitat for wildlife through the arts

Location & Wayfinding

- People don't know what exists and where it is – need map
- Need better maps/wayfinding for public arts – tangible in addition to high-tech
- Wayfinding opportunities when you get off the light rail is needed – what's planned now
- No logical place for social activities
- Visibility of works of art at intersections, etc.

- Location → bench by "Playful Pup"
- Love "Playful Pup", need to be on cement; can it be integrated better in the landscape? feels isolated; same bench & concrete as everywhere else is boring
- Seating/comforts to linger at art sites; make it a place to go
- Benches – do they really serve the community; what about companion seating
- It could be more discoverable; feel like I'm missing out because I don't know
- Lacking access points
- Park is already beautiful/doesn't need to be beautified; is it the ideal place to experience art
- Beautify existing art
- Should we use park land for art; are some of the existing works better served in different locations
- Poor vantage points with sculptures "in a line" – can't see it when you're on a walk
- Gateway figures – actually see and notice these
- Discoverability
- Create a treasure hunt for art
- Enhance existing structures that are ugly – don't want the natural part cluttered
- Lots of walkers on Mercer Island – nothing drawing them to art along their way
- Wayfinding, treasure hunt
- Utilize existing natural stopping points along trail – make it discoverable
- More accessible/engaging art
- Greta Hackett – don't walk up to art
- Lid area – enhancing existing elements
- Green park not the right space for large-scale art – more appropriate in town center or maybe Mercerdale Park
- Move art to where beautification is needed
- Lid Park not the best place for art – town center and light rail center instead
- Green space already beautiful
- Light rail station entrances – include art, create connection to Aubrey Davis Park
- Along 78th - same look from Aubrey Davis Park to Mercerdale Park; uniformity to town center look; native plants, soften urban look
- Art should go in Town Center – art walk from Mercerdale Park to Town Center; from Tully's to mini-lid
- Mercerdale Park – recycling center – history – building is historic
- Use art as a way of connecting Aubrey Davis Park to Luther Burbank Park (across mini-lid)
- Lead people to mini-lid – create connection
- Wayfinding, pedestrian-friendly
- Uniform wayfinding/signage along AUBREY DAVIS PARK
- Interact with art on trail, passing through

- Create connection, flow of art throughout; make people want to explore
- Highlight expanse of park, experience entire span
- Mark each entrance to trail – arch, sign
- Use sculptures as wayfinding
- Art form for entire park that carries through and connects with wayfinding
- Graphic design for signs and distances to destinations – around trails, at entrances, to town center
- Using water access areas as meditative/quiet spaces
- Art piece at boat launch

Where are opportunities?

- East-west connection with art – create an I-90 art way
- Sound Transit station entrances
- Modes of transportation tied into trail
- Gateway, town center
- Feroglia Fields
- Shorewood entrance
- Sound Transit utility areas
- Continuous experience from light rail to Town Center
- Activate the space across from Freshy's
- Have exhibits rotate around town center
- Entry to mini-lid needs activation
- Take existing restroom and transform it

Cultural resources/heritage – ideas for what you could interpret / highlight

- Two cities that merged
- Rural - "take boat to go on vacation"
- Where is old ferrydock – highlight location / interpretive element nearby or at viewpoint where you can see it
- Don't feel culture connection
- History of logging, maker mentality
- Farming
- Opera house – home in the firs
- Spot for sunken forest information
- Sound Transit station as cultural info center
- When the bridge sunk
- How did native people use this island
- Signs on freeway about park
- Native artists

- Connection to land and wildlife with native American context/culture
- Historical events that are location specific
- “I don’t want to see it, hear it & smell it” – history of how the park came to be
- Keep it natural, improve natural
- Native American history – hunting; thought island sunk at night
- Ferry used to be the way here
- Roanoke landing
- Cows – Dairy barn ruins in Luther Burbank
- Durable board games
- Sleepy hunting cabins – vacation spots used to be why people came to the island
- Anne Frank, Japanese internment – examples of powerful interpretive spaces
- Redlining of N. Seattle
- 1st African American in Mercer Island
- Now did we become more diverse
- Utilization of space – how did density happen
- Mercer Island town & city – mid-60’s
- East Seattle – small lots
- Geology – silt, rock
- Sunken forest
- 1 mile of ice – geologic time
- Climate change – how will it look in 100 years
- Changing levels of Lake Washington
- Families and their stories – how they can interact with or create work at site
- Ben Werner – mini-lid connects Luther Burbank Park with upper Luther
- Sign – Aubrey Davis Park history
- How the park came to be (near stacks)
- Landscaping – more native plants tie in with art
- Sound Transit – integrate AUBREY DAVIS PARK history into station
- 77th – concern about loss of landscaping with Sound Transit; soften urban environment and incorporate native landscape

Examples

- Gasworks Park as “celebrate history”
- Olympic Sculpture Park
- Make natural experience grand
- Dr. Albert Schweitzer Park – told poetry of his thoughts through park
- Storm King – grand scale, sculpture fits
- Grand Coulee – lightshow
- Tennis courts take great viewpoint

- Sunset project on stacks and view of I-90
- Streetscape project – embedded street names
- Horseshoe, milk bottle
- Exhibit of redwood cross-section

From examples shown in presentation, what did you like?

- “Bean” creates landmark, reason to gather
- Gathering place after performances
- Art that encourages you to stay in a functional space
- Art that can be used as cover/rain protection
- Interactive, family oriented – kids can play on, with
- Different approaches in different areas of Aubrey Davis Park versus different spaces
- Incorporate public utilities with new development
- Bring living performance art to existing park/art spaces – temporary installations/performers; more functional; use of resources (not just sculptures)
- Lifecycle of art – transforming existing works
- Determine long term, what can shift
- Pop-ups transform functional to use in a new way
- Liked the house example – transformation and function
- Use art to evoke feeling in regular spaces
- Using stacks – lighting
- Incorporating history – photos, abstract pieces
- Pop-ups can draw new audiences, visitors; new, unusual experiences

What is your vision for art in the park?

- Minimalist, provocative, engaging, grass, temporary, destination, interactive, tranquil, inclusive, views, thought-provoking, discovery, minimalist, educational, natural, green, wildlife, community, fun, light rail, greenscape

Segment
1

General

- Art to not add more impervious/paved surfaces
- Keep north side of AUBREY DAVIS PARK natural area – no art here; art west of Area C and east of lid edge around walk; keep lidded park discrete
- Need maps throughout lidded park – digital opportunity
- Art walk from town center sculpture park to mini-lid
- Blur lines of art and sculpture between sound transit station, town center, trail & park
- Get community involved in curating art
- Temporary art that makes you think
- Lots of art but no one knows where it is

- More QR codes to learn about art pieces – go for more info on-line
- More performing arts/performance pop-ups
- Accessibility → middle of the grass
- Art on a pedestal → views
- Scale, inviting → interaction
- Vibrant, colorful, big
- Also love the small pieces – discovery, joy
- Some areas lack art & experience
- Would like to see bigger scale move through & experience
- Use existing materials as canvas (e.g. chain link fence)
- Display art in collections

West End

- Art on freeway into island
- Sound is loud – art mitigation

Area C and B

- Playful Pup – like having the bench; more of a place to go
- Don't like the bench
- Art on corners is more visible → climb on

Stacks and tennis court area

- Best sunsets from tennis courts – allow more natural around trees
- Opportunity for more green and softer use here
- Keep natural or improve natural around stacks
- Projections/lights on stacks (sunsets)
- Green walls around stacks – can see I-90 from here (interpret)
- "Aubrey Davis Park is..." (history, what these stacks are, etc.)
- Would benefit from additional art
- Use this area for something big
- Provide opportunity for teenagers to engage in art
- Shaft for different experience
- Temporary projection; yearly program

Area A & East of Stacks

- Opportunity for engaging with history near restroom – maybe transitional art here since more mobile
- Corner near restroom is good place for art – would work with the landscape
- Activate corner at SE 24th and 76th

- Kids climb on Yearling here
- Cyclist experiences

Sound Transit Area

- Sound Transit – rotating art mostly here
- Use station as map/hub for art culture
- Art to help visual of these bridge crossings
- Aubrey Davis as the person – interpretive here as history included WDOT notes, etc.
- Provide a map of art (interactive?) with art at light rail station
- Provide opportunity for kids/local community students to display their art
- Sound mitigation at transit station

Town Center, sculpture garden, and connections

- Art better purpose here where near concrete
- Art walk from town center down 77th to Mercerdale Park with food along the way
- History – recycling center by Mercer Island high school students and first started recycling – turn building into educational center on recycling
- Tough to get across Island Crest Way, so either need to connect (more than one sculpture) OR let people back over
- Sculpture garden works
- Don't like art in a row
- Love sculpture garden, provide a space to enjoy a glass of wine
- Gateway Figure most iconic
- Love the wavy branches
- Seems random, disconnected
- Very playful
- Primavera stands out, very popular
- Burbank Lid – art is hidden in the corner, can't see

Segment
2

Luther Burbank mini-lid and connections

- Sculpture at Luther Burbank trail connection hill
- "Mercy" at mini-lid
- Sculpture park connects across mini-lid to Luther Burbank Park
- Sculpture connects along south of I-90 forested area
- Sound Transit utility at Shorewood – make it better visually with art, etc.
- King County Forward Thrust – 1st send-off at Luther Burbank Park
- History/purpose – to green connection from both sides of Luther Burbank Park
- Most expensive section in nation at the time

- Seattle to Boston & we were last piece in cross-country freeway system (held up by Aubrey Davis Park open space)
- Last section of highway
- Use trail as continuation of transportation transition over time
- Mini-lid could be connected to Town Center & down Luther Burbank
- The Source – More interactive, accommodates different ages, like very much, playful
- Don't want park cluttered with art
- Use existing materials in that park as convos for art
- Not everyone is high-tech – would be nice to have info for art for everyone
- Take existing art and transform
- Kids like the Dragon on Island Crest Way

East Boat Launch

- Need sound mitigation at east boat launch – loud
- Create calm water experience

Mercer Island Aubrey Davis Park

April 16, 2019 | Survey Analysis

The Mercer Island Aubrey Davis Park Survey was designed to complement the Open House and provide an additional way for people to provide input to the design team on the vision and goals of the Aubrey Davis Master Plan. Near the beginning of the survey period a response option was added to questions 14 through 21 that allowed respondents to choose an option that would just improve existing conditions. Of the 113 total survey respondents, 20 responses occurred prior to that change and respondents were allowed to retake the survey if they chose. Below is a summary of all responses received, organized by section and question.

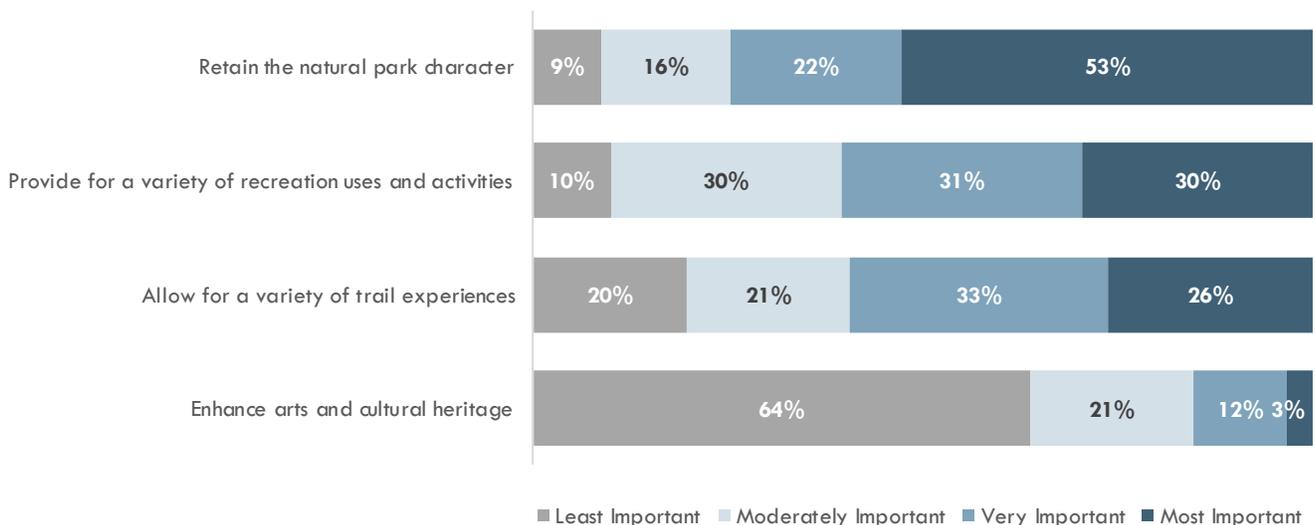
Welcome

1. **Would you like more information about the master plan, timeline, and process or would you rather get started on the survey?**

| | Count | Percent |
|--------------------------|------------|---------|
| More information please! | 35 | 31% |
| Let's get started! | 78 | 69% |
| Answered | 113 | |
| Skipped | 0 | |

Draft Vision, Goals, and Objectives

2. **Please rate the importance of each draft goal from least to most important.**



3. Is there anything missing or that you'd change about the draft vision or goals? Leave us a note here.

| | Count | Percent |
|---|--------------------|---------|
| Better signage | 1 | 2% |
| Don't change anything | 6 | 13% |
| Maintain existing art | 1 | 2% |
| Nothing missing | 2 | 4% |
| Prioritize athletic fields | 2 | 4% |
| Question about survey | 2 | 4% |
| Recognize as transportation corridor/accommodate growing multimodal needs | 7 | 15% |
| Reduce bike use/Slow down bikers | 4 | 8% |
| Rename the park | 1 | 2% |
| Retain natural character | 11 | 23% |
| Safety and security of users | 11 | 23% |
| Separate uses (bike/ped) | 3 | 6% |
| The park should be a place for everyone/accommodate all users | 2 | 4% |
| | Answered 48 | |
| | Skipped 65 | |

Note: Items listed in this summary were compiled from key words or phrases in the open-ended responses.

Sample open-ended responses

- *The corridor needs to be recognized as the high-volume bike/pedestrian space it is and prioritized as such.*
- *Very important-please maintain the "bikeability" of the trail and provide means for safe sharing of trail with pedestrians (signage, etc.)*
- *Need to emphasize that expanding the width of the trail to allow for separation of use and to accommodate the expected increase in users over the coming years is imperative for safety and ease of use*
- *Safety and security of users is the highest importance.*
- *Keeping bicycles off the trail is by far the most important issue that we must solve. Here we should have zero tolerance for any offenders.*
- *Uses are not just recreational; it is also a vital commuting link.*
- *The park is mostly great as is. Need to remediate soil and improve irrigation to preserve trees. Reduce bike speeds to make pedestrians safer. Other than that, if it ain't broke don't fix it!*
- *The trail is both a commuter route and a recreational facility. Ensure that the vision of the trail can accommodate both user groups in a compatible manner.*
- *...all changes should first comply with Aubrey's vision of not wanting to "see it, hear it, or smell it."*
- *Separating conflicting uses is the key to success here.*
- *The vision and goals don't seem to reflect the "place for everyone" ethos.*
- *Please leave the park alone, this is not a necessity at this time and the budget constraints complicate the picture. You will inherit a lot of citizen anger if these plans come to fruition*

Trails

4. How do you or would you typically use the trail?

| | Count | Percent |
|--|-----------------|-----------|
| On wheels - Faster through cyclist | 25 | 28% |
| On wheels - Fitness cyclist/group rider | 21 | 24% |
| On wheels - E-bike cyclist | 12 | 14% |
| On wheels - Casual through cyclist | 23 | 26% |
| On wheels - Casual cyclist | 21 | 24% |
| On wheels - First/last miler (e.g. bikeshare) | 7 | 8% |
| On wheels - Family cyclist | 18 | 20% |
| On wheels - Alternative wheels (scooter, skateboard, etc.) | 3 | 3% |
| On wheels - Assisted mobility | 1 | 1% |
| On foot - Jogger/runner | 26 | 30% |
| On foot - Fitness pedestrian | 32 | 36% |
| On foot - Casual pedestrian | 40 | 45% |
| On foot - Group walking | 12 | 14% |
| On foot - Walking with strollers, dogs, etc. | 20 | 23% |
| On foot - Elderly or senior walker | 15 | 17% |
| On foot - Visually impaired pedestrian | 3 | 3% |
| I don't use the trail and/or don't think I would in the future | 1 | 1% |
| Other (please specify) | 9 | 10% |
| On wheels - Bicycle commuter | 1 | 1% |
| On wheels - Child learning to ride | 1 | 1% |
| On wheels/foot - Family with pedestrians, strollers, and children learning to ride | 1 | 1% |
| On foot - Commuting | 2 | 2% |
| On foot - Walking with bicycle | 1 | 1% |
| Other Comment | 2 | 2% |
| | Answered | 88 |
| | Skipped | 25 |

Note: Respondents could choose multiple responses if they use the trail in different ways. Items listed under "other" were compiled from key words or phrases in the open-ended responses.

- The trails in Aubrey Davis Park are used by a variety of users, both on foot (152 uses) and on wheels (134 uses). Many respondents reported using the trail in a variety of ways.
- The most commonly reported use was on foot as a casual pedestrian (40 respondents), with fitness pedestrians a close second (32 respondents), and joggers/runners the third most common (26 respondents).
- The most commonly reported uses on wheels include: faster thru cyclists (25 responses), casual through cyclists (23 responses), and fitness cyclists (21 responses) or casual cyclists (21 responses).

Sample open-ended responses

- *Bicycle groups and enthusiasts have made a big push through lobbying to have increased access to the roads. Aubrey Davis Park should not turn into an alternative pathway for bikes.*
- *Children beginning to ride bicycles in a safe place with hills and family nearby.*
- *With grandkids, either in stroller, walking or on little bikes*

5. Are there other trail users missing from the list provided?

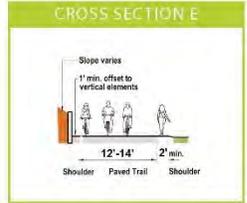
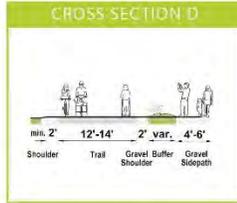
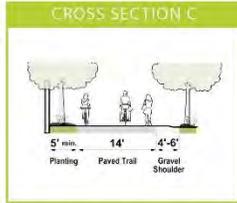
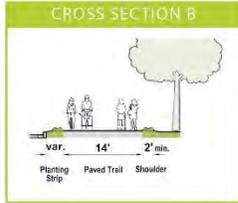
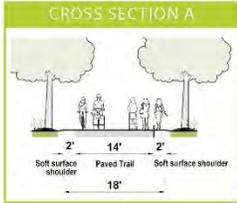
| | Count | Percent |
|-------------------|-----------|---------|
| Bicycle commuters | 3 | 10% |
| Children | 5 | 16% |
| Dog-walkers | 8 | 26% |
| Family and kids | 1 | 3% |
| No users missing | 12 | 39% |
| Scooters | 1 | 3% |
| Skaters | 1 | 3% |
| Taco Truck | 1 | 3% |
| Trail-crossers | 1 | 3% |
| Answered | 31 | |
| Skipped | 82 | |

Note: Items listed in this summary were compiled from key words or phrases in the open-ended responses.

Sample open-ended responses

- *Dogs, even on a leash, are a constant threat to bicyclists and other walkers.*
- *Stand around and talk folks. Crossing trail only folks.*
- *Commuters to P&R. I use trail both on foot and bike to P&R. I guess this falls under "first/last mile", except that you specify e-share bike with that.*
- *Solo kids walking to school, library, friends, park, baseball/soccer fields...*
- *Taco Truck. Please take this in the context of Parks are gathering places for people, family, kids, groups. The trails, both pedestrian and bike allow access. A food truck provides a focal point for group activities.*

LIDDED PARK TRAILS



6. Which trail section do you prefer for this part of the park?

| | Count | Percent |
|--|-----------------|-----------|
| Cross Section A | 21 | 29% |
| Cross Section B | 3 | 4% |
| Cross Section C | 6 | 8% |
| Cross Section D | 22 | 31% |
| Cross Section E | 2 | 3% |
| Less than 14 feet wide | 10 | 14% |
| More than 14 feet wide | 10 | 14% |
| Other (please specify) | 28 | 39% |
| 12-foot trail with 2-3ft pedestrian path on side | 1 | 1% |
| Don't redirect bikes | 1 | 1% |
| Keep existing width | 8 | 11% |
| Keep trees | 1 | 1% |
| Model after Burke Gilman Trail near UW | 1 | 1% |
| Move trail south of ball field | 1 | 1% |
| Narrow the paths | 3 | 4% |
| Paved, gravel shoulder | 1 | 1% |
| Prioritize pedestrians and discourage bikers | 5 | 7% |
| Questions about or unhappy with survey | 3 | 4% |
| Separation of bicyclists and pedestrians | 3 | 4% |
| Widen trail to accommodate all users | 1 | 1% |
| | Answered | 72 |
| | Skipped | 41 |

Note: Items listed under "other" were compiled from key words or phrases in the open-ended responses. Respondents could indicate multiple items in their comment, each is represented in the summary.

- The most common response to this question was a choice other than those presented. Of the other options, 8 respondents wanted to keep the existing width, 3 wanted to narrow the path, and 1 wanted to widen the path.
- Of the proposed options, 22 respondents preferred Cross Section D, and 21 respondents preferred Cross Section A.

Sample open-ended responses

- *Trail should be improved and widened to the maximum extent possible to accommodate a mix of all users which will increase in the future. The wider the trail, the safer it will be for ALL users....*
- *Don't like any. They all look like a road not a path. May as well add cars with those designs.*
- *Keep it the way it is today. Bikes have alternative options.*
- *None. WSDOT requires 10' wide trails if there is heavy traffic which there is not in the park. I think bikes need to be excluded from the park and the trail left at its current width. Otherwise traffic calming like roundabouts will be necessary. There have already been two very serious bicycle injuries on the trail.*

7. What other trail improvements or modifications should be considered in this section of the park?

| | Count | Percent |
|---|-----------|---------|
| Art spaces with seating | 1 | 3% |
| Clear signage | 6 | 15% |
| Do nothing | 4 | 10% |
| Exclude cyclists | 3 | 8% |
| Handicap Parking | 1 | 3% |
| Increased visibility of pedestrians/bicyclists at road inte | 4 | 10% |
| Narrow trails | 2 | 5% |
| Separation of bicyclists and pedestrians | 11 | 28% |
| Preserve natural spaces (maintain existing and don't en | 3 | 8% |
| Remove gravel hazards from trails | 1 | 3% |
| Add traffic calming measures to slow cyclists | 8 | 21% |
| Increase sightlines | 1 | 3% |
| Add water station | 1 | 3% |
| Widen the trail | 1 | 3% |
| Answered | 39 | |
| Skipped | 74 | |

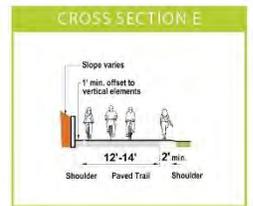
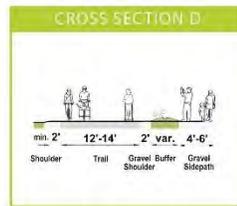
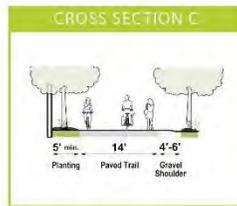
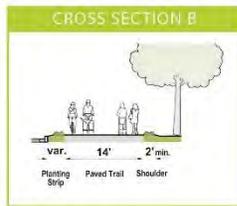
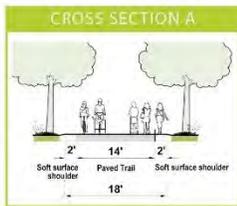
Note: Items listed in this summary were compiled from key words or phrases in the open-ended responses. Respondents could indicate multiple items in their comment, each is represented in the summary.

- The most commonly reported improvement needed in this section of the trail is separating bicyclists and pedestrians. Suggested improvements included everything from pavement markings (surface striping) to physical barriers. Some respondents did not indicate which type of separation would be best, but noted it was important.
- The second most commonly noted improvement is increasing the use of traffic calming measures to slow cyclists in this area. Several ways were noted: roundabouts, narrower trails, and speed bumps.
- There was also an indication that signage could be improved on this part of the trail. Proposed improvements included signs that establish user norms, signs indicating “slow” areas, and signs indicating the trail as a cross-island trail.

Sample open-ended responses

- *Small garden sitting areas, public art, and other slower spaces available adjacent to the trail to act as "eddy zones" for those moving along the trail at a leisurely pace. These spaces shouldn't conflict or detract from the experience of users like commuter cyclists, runners, or recreational riders who value a direct, uninhibited path.*
- *Handicap parking to increase access for mobility impaired park visitors. It needs to be close to provide real accessibility.*
- *The trails are fine. Changing them will diminish them.*
- *Your plans for bicycle freeways in a neighborhood park are inappropriate.*
- *.... It comes back to money. Where are you going to get the money from? I hope you are not planning on putting a levy on the ballot for us to approve any construction for this trail.*

TOWN CENTER TRAILS



8. Which trail section do you prefer for this part of the park?

| | Count | Percent | |
|--|-----------------|-----------|--|
| Cross Section A | 16 | 23% |  |
| Cross Section B | 5 | 7% |  |
| Cross Section C | 3 | 4% |  |
| Cross Section D | 20 | 29% |  |
| Cross Section E | 5 | 7% |  |
| Less than 14 feet wide | 9 | 13% |  |
| More than 14 feet wide | 7 | 10% |  |
| Other (please specify) | 21 | 30% |  |
| 6 ft. wide trail | 1 | 1% |  |
| 24th needs different treatment than trails | 1 | 1% |  |
| Clear signage | 1 | 1% |  |
| Do nothing (keep existing width) | 5 | 7% |  |
| Make space next to trail | 1 | 1% |  |
| Narrow the trail | 2 | 3% |  |
| Traffic calming | 2 | 3% |  |
| Prioritize pedestrians and discourage bikers | 5 | 7% |  |
| Questions about or unhappy with survey | 4 | 6% |  |
| Separate bicyclists and pedestrians (including in commute) | 2 | 3% |  |
| Widen trail to accommodate all users | 1 | 1% |  |
| | Answered | 69 | |
| | Skipped | 44 | |

Note: Items listed under “other” were compiled from key words or phrases in the open-ended responses. Respondents could indicate multiple items in their comment, each is represented in the summary.

- The most common response to this question was “Other (please specify)”. Of the other options, 5 respondents wanted to keep the existing width and 5 wanted to prioritize pedestrians and discourage cyclist use on this part of the trail.
- Of the proposed options, 20 respondents preferred Cross Section D, and 16 respondents preferred Cross Section A.

Sample open-ended responses

- *Speed bumps where necessary. "Slow" signs for bikers. Don't make the trail into a highway.*
- *Why are you starting from the premise that bicycles will migrate from North Mercer to the lid trail?*
- *Clearly indicate transit passenger loading zones and separate bicycle facilities from the waiting transit passengers.*
- *I hate those designs. Why the new road for the park?*

9. What other trail improvements or modifications should be considered in this section of the park?

| | Count | Percent |
|---|-----------|---------|
| Accomodate all users | 1 | 4% |
| Add traffic calming measures to slow cyclists | 1 | 4% |
| Clear signage | 5 | 18% |
| Crosswalk marking | 2 | 7% |
| Do nothing/None needed | 5 | 18% |
| Exclude cyclists | 2 | 7% |
| Increase sightlines | 1 | 4% |
| Narrow trails | 1 | 4% |
| Preserve natural spaces (maintain existing and don't encroach | 3 | 11% |
| Prioritize pedestrians | 2 | 7% |
| Questions about survey | 2 | 7% |
| Separation of bicyclists and pedestrians | 6 | 21% |
| Answered | 28 | |
| Skipped | 85 | |

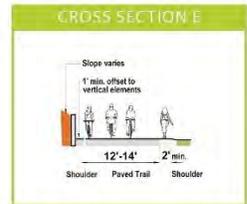
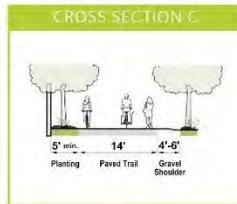
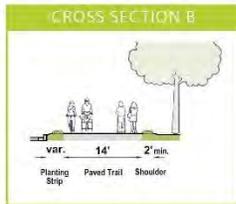
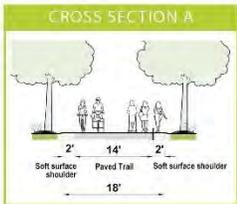
Note: Items listed in this summary were compiled from key words or phrases in the open-ended responses. Respondents could indicate multiple items in their comment, each is represented in the summary.

- The most commonly reported improvement needed in this section of the trail is separating bicyclists and pedestrians. Suggested improvements included pavement striping and solid barriers. There were general comments about the need to increase safety and reduce conflict between pedestrians and bicyclists
- The two second most commonly noted improvements are either 1) to do nothing or 2) to improve signage. Proposed improvements to signage included: using sharrows on the road and obvious signage to divert around the Park and Ride.

Sample open-ended responses

- *76th Ave SE and Island Crest/SE 26th St. need to have their 'slipways' (dedicated right turn lanes with yield) removed. These are highly dangerous intersections for cyclists and pedestrians resulting in severe injuries/death when accidents occur. Narrower crossings and raised cycle/pedestrian crossings are critical.*
- *During commute hours, this section is prone to conflicts between pedestrians trying to access the bus stop and through bicycle commuters. The eastern end of this section is badly lit and dangerous for night time cyclists. I avoided this entire section as a bicycle commuter.*
- *If you intend to make 24th a formal part of the path then it needs major improvements. Right now it is a residential street and the bikes blow stop signs and crosswalks constantly. It isn't safe.*
- *Looks like you want to divert folks around the park & ride? Good luck w/ that. The obvious path is in front of it. You're going to need something even better than signs to encourage that.*
- *Less trees/planting work that requires maintenance (e.g. the opposite of what you did in LBP). The city claims it's out of \$ but is taking our mature trees and adding plantings with a lot of long term maintenance requirements. Why? Let it be until we can afford it.*

EAST SEGMENT TRAILS



10. Which trail section do you prefer for this part of the park?

| | Count | Percent | |
|--|-----------------|-----------|---|
| Cross Section A | 19 | 28% |  |
| Cross Section B | 4 | 6% |  |
| Cross Section C | 5 | 7% |  |
| Cross Section D | 20 | 29% |  |
| Cross Section E | 2 | 3% |  |
| Less than 14 feet wide | 10 | 14% |  |
| More than 14 feet wide | 8 | 12% |  |
| Other (please specify) | 20 | 29% |  |
| Do nothing (keep existing width) | 6 | 9% |  |
| Narrow the trail | 1 | 1% |  |
| Traffic calming | 2 | 3% |  |
| Prioritize pedestrians and discourage bikers | 4 | 6% |  |
| Protect trees | 1 | 1% |  |
| Questions about or unhappy with survey | 1 | 1% |  |
| Separate bicyclists and pedestrians | 2 | 3% |  |
| Street sharrows for cyclists | 1 | 1% |  |
| Widen trail to accommodate all users | 1 | 1% |  |
| | Answered | 69 | |
| | Skipped | 44 | |

Note: Items listed under “other” were compiled from key words or phrases in the open-ended responses. Respondents could indicate multiple items in their comment, each is represented in the summary.

- The most common trail section preferred in this part of the park is Cross Section D (20 respondents).
- The second most common trail section preferred in this part of the park is Cross Section A.
- The same number of respondents that prefer Cross Section D also indicated an “other” preference. Of those indicating a different option, most wanted to do nothing or keep the existing trail width (6 respondents) and four respondents wanted to prioritize pedestrians and discourage bikers.

Sample open-ended responses

- *None of the options. Just leave it as it is.*
- *The trail along WMW does not need to be changed.*
- *Street sharrows in Town Center are the preferred in the cycling community.*

11. What other trail improvements or modifications should be considered in this section of the park?

| | Count | Percent |
|--|-----------------|-----------|
| Better signage | 1 | 5% |
| Do nothing/None | 4 | 21% |
| Exclude dogs from trail | 1 | 5% |
| Handicap parking | 1 | 5% |
| Improve trail connections | 1 | 5% |
| Improve sightlines | 1 | 5% |
| Marked road crossing | 1 | 5% |
| Path striping | 2 | 11% |
| Prioritize pedestrians | 1 | 5% |
| Questions about or unhappy with survey | 2 | 11% |
| Separation of bicyclists and pedestrians | 3 | 16% |
| Traffic calming | 1 | 5% |
| Water fountains | 1 | 5% |
| Widen trail | 1 | 5% |
| | Answered | 19 |
| | Skipped | 94 |

Note: Items listed in this summary were compiled from key words or phrases in the open-ended responses. Respondents could indicate multiple items in their comment, each is represented in the summary.

- 19 respondents provided information about other trail improvements.
- 4 respondents wanted to do nothing in this section of the park.
- 3 respondents wanted pedestrians and bicyclists to be separated either by providing a gravel path for pedestrians, diverting bicyclists to the street, or road markings/textures to indicate uses.

Sample open-ended responses

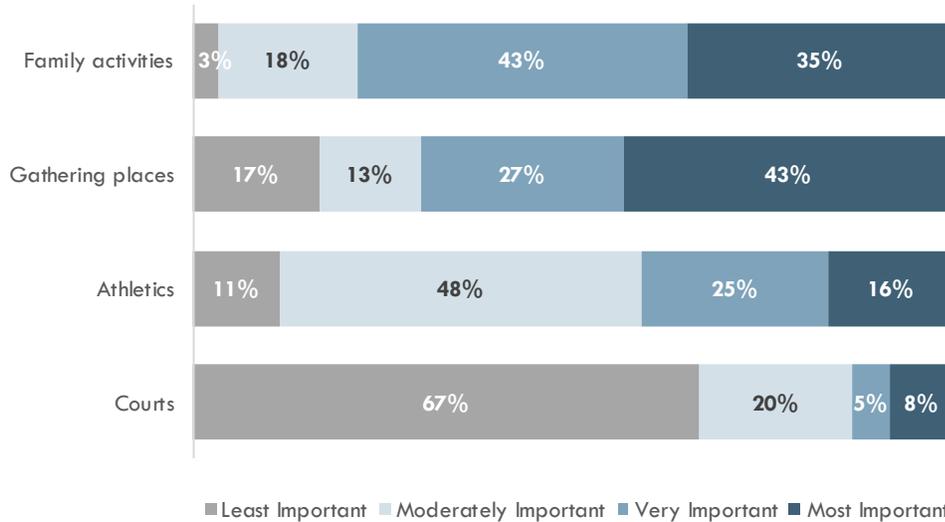
- *More focus needs to be placed on the connection between East Mercer and the Trail. This is a very high demand route that is very dangerous for bicyclists and pedestrians.*
- *You put cyclists at risk when you mix them with pedestrians. They don't want to be there and you should do all to assist them finding good routes on the street.*
- *I think if you can provide a separate, gravel pedestrian path it would be nice. I just don't think there's space for it through most of this area.*
- *Handicap access Parking. This is directly adjacent to senior living facilities and there is no additional parking of any kind.*
- *Please leave the trails as is. What problem are you trying to solve?*
- *You will have to calm speeds if bikes, e-bikes and pedestrians are going to share this trail. Several segments east of the TC don't have 18' in width and so you will end up with concrete from the I-90 retaining wall to NMW which will be around 40' of pure pavement IN A PARK. Plus, there will be several sections that don't have 18' so the trail will narrow creating risks for everyone.*
- *Less is more.*

Park Programming and Activities

ACTIVITIES



12. Thinking about the images above, please rank the following activities in terms of importance, with 1 being the most important to include and 4 being the least important to include in Aubrey Davis Park.



13. Are there any other activities you want to see added or improved?

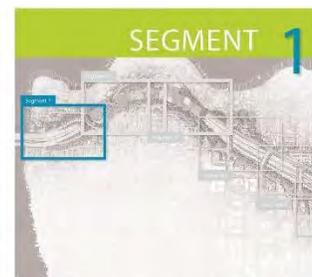
| | Count | Percent |
|--|-----------|---------|
| Better signage | 2 | 7% |
| Bike park/pump track | 1 | 4% |
| Bike repair station | 1 | 4% |
| Do nothing/None | 8 | 30% |
| Maintain athletic fields | 1 | 4% |
| Maintain green space/Prioritize open space | 4 | 15% |
| Off-leash area | 3 | 11% |
| Playgrounds | 1 | 4% |
| Prioritize pedestrians | 1 | 4% |
| Questions about or unhappy with survey | 3 | 11% |
| Restroom | 3 | 11% |
| Seating | 1 | 4% |
| Stages | 1 | 4% |
| Water fountain | 1 | 4% |
| Answered | 27 | |
| Skipped | 86 | |

Note: Items listed in this summary were compiled from key words or phrases in the open-ended responses. Respondents could indicate multiple items in their comment, each is represented in the summary.

Sample open-ended responses

- *Direct cross Island bike traffic along specific trails and lessen that impact on other activities.*
- *Open, green space is the #1 priority.*
- *Plenty of benches for seniors to sit.*
- *Playgrounds... it's a particular need in NE quadrant of island - segment 8 of Aubrey Davis Park.*
- *Less is more. The park is fantastic the way it is.*
- *I would like the park to remain and be enhanced as a recreational area maintaining its natural park like setting. Less concrete, more grass.*
- *The fields need to be better maintained. They are already athletic fields so maintain them better. There is a growing demand for citizens on the island to have more access to field space.*
- *No but please do what you can to minimize more pavement. Make the courts we've got more multi-use so that we don't remove any more grass/trees from the existing park.*

PARK ON THE LID



— Existing Trail
- - - Existing Trail, Bypass Route
— Right-of-Way

14. What recreation programs or activities would you like to see in this part of the Park?

| | Count | Percent |
|---|-----------|---------|
| Gathering places | 16 | 27% |
| Family activities | 9 | 15% |
| Athletics | 8 | 13% |
| Courts | 4 | 7% |
| No major changes, maintain and enhance existing vegetation and infrastructure | 33 | 55% |
| Anything else? | 15 | 25% |
| Better signage | 2 | 3% |
| Do nothing/None | 8 | 13% |
| Maintain green space/Prioritize open space | 4 | 7% |
| Off-leash area | 1 | 2% |
| Questions about or unhappy with survey | 2 | 3% |
| Answered | 60 | |
| Skipped | 53 | |

Note: Items listed under “Anything else?” were compiled from key words or phrases in the open-ended responses. Respondents could indicate multiple items in their comment, each is represented in the summary.

- Most respondents didn’t want any major changes, but were looking for maintenance of existing vegetation and infrastructure

Sample open-ended responses

- Less is more. The park is fantastic the way it is. Get out of the business of providing fun.*
- Leave it as is*
- There are already plenty of activities - no need for more - just better maintenance of what we have*
- Sprinklers so grass and trees don’t die*
- IT IS A PARK. Aubrey Davis and the citizens of MI spent 20 years fighting for this PARK. Keep it a park. No new development or wider trails or impervious surfaces.*
- Welcome sign showing map of the area and safety information for cyclists.*



— Existing Trail
 - - - Existing Trail, Bypass Route
 — Right-of-Way

15. What recreation programs or activities would you like to see in this part of the Park?

| | Count | Percent |
|---|-----------|---------|
| Gathering places | 18 | 29% |
| Family activities | 18 | 29% |
| Athletics | 16 | 26% |
| Courts | 10 | 16% |
| No major changes, maintain and enhance existing vegetation and infrastructure | 24 | 39% |
| Anything else? | 23 | 37% |
| Add gravel walking path | 1 | 2% |
| Better utilize Opportunity Area B | 1 | 2% |
| Bike park | 1 | 2% |
| Do nothing/None | 6 | 10% |
| Expand playground | 1 | 2% |
| Improve athletic fields | 2 | 3% |
| Maintain green space/Prioritize open space | 6 | 10% |
| Off-leash area | 1 | 2% |
| Prioritize pedestrians | 1 | 2% |
| Unobtrusive art (e.g. murals) | 2 | 3% |
| Wider path | 1 | 2% |
| Answered | 62 | |
| Skipped | 51 | |

Note: Items listed under “Anything else?” were compiled from key words or phrases in the open-ended responses. Respondents could indicate multiple items in their comment, each is represented in the summary.

- 39% of respondents would like to see no major changes but instead maintenance to existing vegetation and infrastructure.
- 29% of respondents wanted to see gathering places and family activities in this part of the park.

Sample open-ended responses

- *Add off lease area with separation from trail. Currently used that way now.*
- *This section of the trail is for families and walkers and should be kept that way to encourage its use.*
- *Aesthetics. Ugly industrial features make it unwelcoming and under-utilized.*
- *Less is more. The park is fantastic the way it is. The open fields are fantastic. Don't screw it up.*
- *Refurbish the athletic field. Paint the exhaust towers so that they better blend with the park like/natural setting. Have a local contest for the best community mural design for the wall by the basketball court*
- *The athletic fields are already there and there is increased demand for their use. Maintain them better than they are today and give preferential treatment to local uses. Kids and seniors first, then local island adults (users should provide records of participants).*
- *There are opportunities for unobtrusive art here - murals or the like.*



— Existing Trail
 - - - Existing Trail, Bypass Route
 — Right-of-Way

16. What recreation programs or activities would you like to see in this part of the Park?

| | Count | Percent |
|---|--------------------|---------|
| Gathering places | 16 | 26% |
| Family activities | 17 | 28% |
| Athletics | 19 | 31% |
| Courts | 7 | 11% |
| No major changes, maintain and enhance existing vegetation and infrastructure | 24 | 39% |
| Anything else? | 19 | 31% |
| Better signage | 1 | 2% |
| Bike park | 1 | 2% |
| Do nothing/None | 5 | 8% |
| Improve water fountain | 1 | 2% |
| Keep paths | 1 | 2% |
| Maintain athletic fields | 2 | 3% |
| Maintain green space/Prioritize open space | 4 | 7% |
| Off-leash area | 1 | 2% |
| Playground | 1 | 2% |
| Reduce bike/ped conflict, reroute bike traffic, exclude cyclists | 3 | 5% |
| Restroom | 1 | 2% |
| | Answered 61 | |
| | Skipped 52 | |

Note: Items listed under “Anything else?” were compiled from key words or phrases in the open-ended responses. Respondents could indicate multiple items in their comment, each is represented in the summary.

- 39% of respondents wanted to see no major changes in this part of the park.
- 31% wanted to see athletics, 28% wanted to see family activities, and 26% wanted to see gathering places.

Sample open-ended responses

- *Open green spaces are the priority!*
- *This area has gathering places, family playgrounds, baseball fields and tennis courts are a few hundred yards away. Nothing needs to be added.*
- *The open fields are fantastic. Don't screw it up.*
- *... It is very dangerous for small children on the trail at the bottom of the incline because the bicyclists whizz past at very high speeds and are unable to stop for meandering toddlers.*
- *The athletic fields are already there and there is increased demand for their use. Maintain them better than they are today and give preferential treatment to local uses. Kids and seniors first, then local island adults (users should provide records of participants).*

TOWN CENTER



- Existing Trail
- - - Existing Trail, Bypass Route
- Right-of-Way

17. What recreation programs or activities would you like to see in this part of the Park?

| | Count | Percent |
|---|-----------|---------|
| Gathering places | 15 | 26% |
| Family activities | 15 | 26% |
| Athletics | 5 | 9% |
| Courts | 3 | 5% |
| No major changes, maintain and enhance existing vegetation and infrastructure | 20 | 35% |
| Anything else? | 26 | 46% |
| Add sculpture | 2 | 4% |
| Better signage | 1 | 2% |
| Bike park | 1 | 2% |
| Bike share parking | 1 | 2% |
| Do nothing/None | 5 | 9% |
| Exclude bikes | 1 | 2% |
| Improve trail/Keep trail | 2 | 4% |
| Maintain green space/Prioritize open space | 6 | 11% |
| Off-leash area | 1 | 2% |
| Playground | 1 | 2% |
| Seating | 1 | 2% |
| Questions about or unhappy with survey | 1 | 2% |
| Safer road crossings | 1 | 2% |
| Separate bikes/ped | 3 | 5% |
| Answered | 57 | |
| Skipped | 56 | |

Note: Items listed under “Anything else?” were compiled from key words or phrases in the open-ended responses. Respondents could indicate multiple items in their comment, each is represented in the summary.

Sample open-ended responses

- *Continue focus on safety/separation of bike users from pedestrians along the area in front of the Park n Ride lot, especially given the new configurations for transit traffic once the LINK station opens.*
- *This is a tiny section - why are you even thinking of adding anything here? Right now it offers grass and trees. That's all that's needed.*
- *The grass often browns out and dies there. Needs to be watered better*
- *Keep it low impact - there is a lot of traffic and noise so just trees and other natural improvements to reduce both*
- *Leave as is. Eliminate bicycles for individuals over the age of 12. This is a walking path next to two senior living facilities.*
- *Perhaps add a sculpture.*
- *Do not remove the trail through this area.*



Existing Trail
 Existing Trail, Bypass Route
 Right-of-Way

18. What recreation programs or activities would you like to see in this part of the Park?

| | Count | Percent |
|---|-------|---------|
| Gathering places | 9 | 16% |
| Family activities | 3 | 5% |
| Athletics | 3 | 5% |
| Courts | 0 | 0% |
| No major changes, maintain and enhance existing vegetation and infrastructure | 24 | 43% |
| Anything else? | 31 | 55% |
| Better signage | 1 | 2% |
| Better trail connections | 1 | 2% |
| Bike park | 1 | 2% |
| Bike share parking | 2 | 4% |
| Create a direct bike path with better flow | 3 | 5% |
| Do nothing/None | 4 | 7% |
| Improve trail | 1 | 2% |
| Maintain green space/Prioritize open space | 7 | 13% |
| More parking | 1 | 2% |
| Prioritize pedestrians | 1 | 2% |
| Questions about or unhappy with survey | 3 | 5% |
| Retail/café | 1 | 2% |
| Seating | 2 | 4% |
| Separate bikes/ped | 4 | 7% |
| Traffic calming | 1 | 2% |
| Water fountain | 1 | 2% |

Answered 56
Skipped 57

Note: Items listed under “Anything else?” were compiled from key words or phrases in the open-ended responses. Respondents could indicate multiple items in their comment, each is represented in the summary.

Sample open-ended responses

- *The key in this section is to put pedestrians first and to ensure cyclists (not families) know how to cross the island and avoid the trail.*
- *Better trail connection across driveways*
- *We don't want to see I-90, hear it, or smell it. Enhance the shrubs and trees. New planting, irrigation. Keep the woodsy feel on the bridges.*
- *Bicycle lockers for last mile commuters or those wanting to check out the area on foot.*
- *Leave as is... no reason to spend more money.*
- *Create a direct path for bikes, multimodal through this area. Diverting behind the Park N Ride will be problematic. Remove the yield sign and install stop sign for cars exiting I-90 and turning right. Very dangerous.*
- *If you want to divert walkers and bikers through here, please do more to mitigate traffic noise. Also, as a cyclist, I wouldn't go that way due to the sharp turns - I'm just going to head in front of the P&R. And that's a residential area w/ houses and apartments. I suspect the residents will want some protection of their cars & property from the traffic.*



— Existing Trail
 - - - Existing Trail, Bypass Route
 — Right-of-Way

19. What recreation programs or activities would you like to see in this part of the Park?

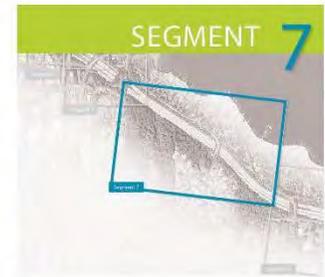
| | Count | Percent |
|---|-----------|---------|
| Gathering places | 17 | 30% |
| Family activities | 13 | 23% |
| Athletics | 4 | 7% |
| Courts | 1 | 2% |
| No major changes, maintain and enhance existing vegetation and infrastructure | 21 | 38% |
| Anything else? | 24 | 43% |
| Better access | 1 | 2% |
| Better signage | 2 | 4% |
| Bike park | 1 | 2% |
| Do nothing/None | 4 | 7% |
| Maintain green space/Prioritize open space | 8 | 14% |
| Off-leash area | 1 | 2% |
| Picnic tables | 1 | 2% |
| Questions about or unhappy with survey | 2 | 4% |
| Restroom | 1 | 2% |
| Safety: improved lighting | 2 | 4% |
| Safety: improved road crossings and slipways | 2 | 4% |
| Shelter with seating | 1 | 2% |
| Water fountain | 1 | 2% |
| Answered | 56 | |
| Skipped | 57 | |

Note: Items listed under “Anything else?” were compiled from key words or phrases in the open-ended responses. Respondents could indicate multiple items in their comment, each is represented in the summary.

Sample open-ended responses

- *The slipway and crosswalks at the I-90 exit need to be addressed. They are very dangerous and place users at risk of severe injury, in addition to increasing the likelihood of all crash types.*
- *Keep the open green space and trees!*
- *A good place for another bathroom/fountain*
- *Need better lighting on this section for night time bicycle commuters*
- *Somewhere with a roof and place to sit down. Grab a snack, get out of rain or sun. Chill for awhile.*
- *I don't see a need to add anything here. It is lovely open space.*
- *Keep it green. Keep it natural. Make it more like Pioneer Park*
- *Replace the yield sign with a stop sign for cars exiting I-90 and turning right. Dangerous situation even with expanded view.*

EAST SEGMENT



- Existing Trail
- - - Existing Trail, Bypass Route
- Right-of-Way

20. What recreation programs or activities would you like to see in this part of the Park?

| | Count | Percent |
|---|-----------|---------|
| Gathering places | 5 | 9% |
| Family activities | 5 | 9% |
| Athletics | 4 | 7% |
| Courts | 1 | 2% |
| No major changes, maintain and enhance existing vegetation and infrastructure | 26 | 46% |
| Anything else? | 30 | 53% |
| Better signage | 2 | 4% |
| Bike park | 1 | 2% |
| Do nothing/None | 5 | 9% |
| Improve/repave/widen trail | 10 | 18% |
| Maintain green space/Prioritize open space | 2 | 4% |
| No parking | 1 | 2% |
| Prioritize pedestrians | 1 | 2% |
| Safety: improved lighting | 4 | 7% |
| Safety: improved road crossings | 1 | 2% |
| Questions about or unhappy with survey | 1 | 2% |
| Seating | 1 | 2% |
| Separate bikes/ped | 2 | 4% |
| Traffic calming | 1 | 2% |
| Answered | 57 | |
| Skipped | 56 | |

Note: Items listed under “Anything else?” were compiled from key words or phrases in the open-ended responses. Respondents could indicate multiple items in their comment, each is represented in the summary.

Sample open-ended responses

- *The path needs to be widened and intersection visibility improved.*
- *Trail widening and improvements. This is the most critical section of the trail. Install or improve lighting in certain sections where trail intersects with roads*
- *"Flattening" of trail that is currently uneven due to tree roots under pavement; clearer delineation between fast cycle-thru traffic and pedestrian usage*
- *Trail is way too narrow in here for all of the multiple users.*
- *It is important in this area to show cyclists how to reach the street and protect pedestrians so they can walk safely without cyclists to the Park and Ride.*
- *Crossing lights at major roads. The kind that can be activated by the walker or bicyclist.*
- *The trail should be more dedicated to pedestrians as most cyclists use North Mercer Way.*
- *Signage with name of park. Distance markers in miles.*
- *This is a dark, narrow, dangerous area used by bikes, scooters, and pedestrians who used for recreation and commuting. The trail needs to be widened and lights installed.*



Existing Trail
 Existing Trail, Bypass Route
 Right-of-Way

21. What recreation programs or activities would you like to see in this part of the Park?

| | Count | Percent |
|---|-----------|---------|
| Gathering places | 15 | 27% |
| Family activities | 14 | 25% |
| Athletics | 8 | 14% |
| Courts | 3 | 5% |
| No major changes, maintain and enhance existing vegetation and infrastructure | 19 | 34% |
| Anything else? | 23 | 41% |
| <hr/> | | |
| Better signage | 2 | 4% |
| Bike park | 1 | 2% |
| Boating support | 1 | 2% |
| Do nothing/None | 5 | 9% |
| Improve access connection | 5 | 9% |
| Improve multimodal capacity/widen trail | 4 | 7% |
| Maintain green space/Prioritize open space | 4 | 7% |
| Don't build multifamily | 1 | 2% |
| Playground | 1 | 2% |
| Separate bikes/ped | 1 | 2% |
| Questions about or unhappy with survey | 1 | 2% |
| Answered | 56 | |
| Skipped | 57 | |

Note: Items listed under “Anything else?” were compiled from key words or phrases in the open-ended responses. Respondents could indicate multiple items in their comment, each is represented in the summary.

Sample open-ended responses

- *More focus needs to be placed on improving the connection from the park South of the 90 on ramp and the trail. The slipway from East Mercer to the on ramp is very dangerous and of limited utility to vehicles in terms of time savings.*
- *Improve access to and surface on East channel bridge, especially for bikes. The current trail is bumpy and access from MI is narrow.*
- *Keep Sculpture park. No density bldgs that would bring crowds.*
- *This is an entrance area for commuting and club cyclists and what is most important here is to address their needs so they know how to cross the island safely without having to risk mixing with pedestrians.*
- *playground; connector trail to Mercerwood neighborhood via city hall property.*
- *A wayfaring sign if this is a local access point. Get from trail to off trail facilities.*
- *This is primarily an open space and should be maintained as such*
- *Keep it green. Keep it natural - make it more like Pioneer Park unless an additional athletic field was added*
- *Leave as is - no reason to spend more money.*
- *Improve capacity to move multimodal transportation through this area.*

Demographics

22. What neighborhood do you live in?

| | Count | Percent |
|----------------------------|-----------|------------|
| South End | 7 | 12% |
| Mercerwood | 5 | 8% |
| West Mercer | 4 | 7% |
| East Seattle | 3 | 5% |
| Ellis Pond | 3 | 5% |
| North End | 3 | 5% |
| Parkwood | 3 | 5% |
| Town Center | 3 | 5% |
| East Mercer | 2 | 3% |
| First Hill | 2 | 3% |
| Island Point | 2 | 3% |
| The Lakes | 2 | 3% |
| Groveland | 1 | 2% |
| Mercer First | 1 | 2% |
| Mercer Island Estates | 1 | 2% |
| Mercerdale | 1 | 2% |
| Other (please specify) | | |
| Seattle | 10 | 17% |
| Mercer Island | 4 | 7% |
| Redmond | 1 | 2% |
| No answer | 1 | 2% |
| Mercer Island Total | 47 | 80% |
| Seattle Total | 10 | 17% |
| Other Total | 2 | 3% |
| Answered | 59 | |
| Skipped | 54 | |

Note: Items listed under "other" were compiled from key words or phrases in the open-ended responses. Any neighborhood listed has been coded to the respective city.

- 80% of respondents are from Mercer Island, while 17% are from different neighborhoods in Seattle including the U District, Capitol Hill, and Beacon Hill.
- Of the respondents on Mercer Island, the most responses came from the South End (12%), followed by Mercerwood (8%), and West Mercer (7%)

23. What is your age?

| | Count | Percent |
|-----------------|-----------|---------|
| Under 18 | 0 | 0% |
| 18-24 | 1 | 2% |
| 25-34 | 9 | 14% |
| 35-44 | 12 | 19% |
| 45-54 | 12 | 19% |
| 55-64 | 10 | 16% |
| 65+ | 19 | 30% |
| Answered | 63 | |
| Skipped | 50 | |

Aubrey Davis Park Master Plan

Open House #1 | MEETING NOTES

February 28, 2019

| Plan Area | Notes |
|----------------|--|
| VISION - Goals | <p>Provide for a variety of uses and activities (10 dots)</p> <ul style="list-style-type: none"> ● I enjoy seeing all the activities happening at this park all year long ● Given the number and variety of uses the whole park should be an "on-leash" area ● Inclusive of all users ● Expand trail & make it safer <p>Retain the natural park character (24 dots)</p> <ul style="list-style-type: none"> ● Yes this is a #1 priority (x2) ● Green open space is a treasure. Retain as much as possible ● Trees are too tall! Cut/trim so as not to block views of Seattle ● Cut the tall overgrown trees by the playground <p>Allow for a variety of trail experiences (10 dots)</p> <ul style="list-style-type: none"> ● Move bikes to the road ● Maybe consider dogs kept ON leashes on the trails ● Separate bikes from walkers, runners, strollers, toddlers ● Separate bikes and other uses with lane markers (+1) ● Moving bikes to the road serves too few riders; the trail is great ● Own bike lanes; walkers afraid to get hit ● Separate bikes from off-leash dogs <p>Enhance arts and cultural heritage (7 dots)</p> <ul style="list-style-type: none"> ● The sculpture park is also a gem; consider expanding areas where art is placed ● I love the happy surprises of art and sculpture in our parks <p>What is missing in the vision? What would you change?</p> <ul style="list-style-type: none"> ● After soccer/lacrosse/volleyball activities there should be park maintenance to immediately repair divets – torn-up areas ● Green spaces provided by ADP is very important to healthy lifestyle options of residents – do not develop, leave it alone ● I support art in the parks and cultural activities |

| | |
|------------------------------------|--|
| | <ul style="list-style-type: none"> ● Balance regional need with local priorities; don't allow big government to stamp out local control ● Aubrey Davis' vision is missing from here – to isolate the freeway from Mercer Island ● Honor man, vision, accomplishments ● Vision statement to have reference to original intent – to not see, hear, or smell [I-90] <p>Other (sticky note comments)</p> <ul style="list-style-type: none"> ● Vegetation replacement priorities: drought tolerant, native, non-invasive! Pollution tolerant, improve soil ● The park is loved but needs TLC ● “We built it and off islanders use it” ● Follow through with promises made – use of \$\$ ● Keep I-90 invisible |
| VISION – What makes a great place? | <ul style="list-style-type: none"> ● Love the ideas in the images with festival lighting, a natural wood balance beam, and red ribbon park |
| PROCESS TO DATE | <p>Under “Sports and Boating”</p> <ul style="list-style-type: none"> ● No more parking is needed; all spaces are only taken for times like Blue Angels |
| KEYMAP | <ul style="list-style-type: none"> ● Crosswalk flashers when people are using at the west end near I-90 entrance ● Permanent restroom at west end would be great ● Remove the painted mural at the I-90 exit at West Mercer, Awful ● I love the new mural! Restful after coming from the city ● Off-leash in lid park is great – keep it that way! Fence off off-leash to keep separate from trail ● Avoid herbicides, compost trimmings, light pathways, use solar panels, and include multi-lingual signage ● Add walking trail to “wooded” section (not a bike path – a “rustic walking path”) ● Walking along natural paths ● Keep I-90 bike trail open for bike commuters ● Keep plant height less than 5' ● Be sustainable ● Updates on process on website, etc. as a YouTube video ● Signs in parks/landscape in multiple languages ● Do a story map for Aubrey Davis Park (like they did for sculpture gallery) ● Survey too many questions |

| | |
|--------------------------|--|
| ACTIVITIES | <p>Gathering (1 dot)</p> <ul style="list-style-type: none"> ● (One dot on mound photo) <p>Family Activities (2 dots)</p> <ul style="list-style-type: none"> ● (One dot on artful sandbox photo) ● (One dot on playground photo) <p>Courts (1 dot)</p> <ul style="list-style-type: none"> ● (One dot on court photo that says "Tennis Courts") ● Tennis courts have many cracks that <u>need</u> repair ● Tennis court nets are broken ● Court is cracked – fix cracks <p>Other/What else?</p> <ul style="list-style-type: none"> ● Off-leash dogs need to be in fenced area please ● I like things just as they are – no wider paths |
| PROGRAMMING Segment 1 | <ul style="list-style-type: none"> ● Passive recreation such as fishing at the water access point |
| PROGRAMMING Segment 2 | <p>Area C</p> <ul style="list-style-type: none"> ● (Sports dot on field) ● (Gathering area dot on field) ● (Family activities dot on field) ● Trees way overgrown. Can't see beautiful views that the island is so lucky to have ● Trees are overgrown onto trail on southeast side of park (x2) ● I lead group bicycle rides and I'd love to start from the Segment 2 parking lot but the time limit is 2 hours; typical bike ride is 4 hours <p>Trail</p> <ul style="list-style-type: none"> ● Trail 6' wide only through park ● No change in width of path – wider and faster bikes are more dangerous to other users ● Bikers zoom so fast they almost hit pedestrians, especially the elderly. Need separate bike lanes ● Trail 10' wide ● Need fence between dog areas and trail; poop bag dispenser and garbage can and signage at voice control |

Area B

- (Sports dot in Area B)
- (Gathering dot south of playground in Area B)
- Keep Area B as a family/open area

Tennis Court Area

- (Sports dot in tennis court area)
- (Family activities dot in tennis court area) (x2)
- Area by the tennis courts has one of the best views in the park – how can we make this available to more people
- Need sign-up sheet for courts – too many people waiting/fighting for court time

Other

- Survey didn't consider "no change" option as if nothing is there now
- After activities on fields – immediate maintenance repairs to divets/torn up areas, etc.
- No field lights and no astroturf please, thanks
- All lighting should point down and not shine into neighborhoods or the eyes of migrating birds
- Stacks could be amazing pieces of artwork
- Basketball court area could be a stunning piece of art
- Keep park as is and no reason for any change and can't afford it
- In WSDOT maintenance area: more sport courts in addition tennis, pickleball, table tennis, etc.

PROGRAMMING
Segment 3

Other

- Ivy is killing the trees; please remove it
- No more rubber matting and no more plastic [in playground]; it should be more natural
- Major trail to town center going east on south side of Area A
- Transient drug use in forested area on the west side of I-90 and N Mercer Way intersection
- First hill does not have sidewalks for the walkers; provide walkers with a trail loop around the perimeter of the Aubrey Davis lid park
- Bikes should use street not park – park is for kids and residents
- Bikes belong on streets NOT in park
- Cars come too fast going north/south on 72nd
- Need crosswalk connecting top of ramp across 72nd

| | |
|--------------------------|--|
| | <ul style="list-style-type: none"> ● Vehicular conflict zone at 72nd and 24th – possible to route cars onto 71st: low visibility due to grades; pedestrian crosswalk not visible at top of hill <p>Ramp to 72nd Ave SE</p> <ul style="list-style-type: none"> ● Opportunity for artwork along ramp wall ● Ramp is first opportunity for parks to get off street – keep it <p>Restroom Conflict Zone</p> <ul style="list-style-type: none"> ● (Gathering area dot on field) ● Loop around field used as trike trail – conflict with faster bikes around restroom ● Conflicts with bicyclists need to watch out for dogs here. Blind spot – they are going too fast ● Re-route bikes behind restroom |
| PROGRAMMING Segment 4 | <p>Opportunity Area F</p> <ul style="list-style-type: none"> ● (Gathering area dot here) ● Less grass; sustainable groundcover and more edible plants <p>76th Ave Overpass</p> <ul style="list-style-type: none"> ● Irrigation leaking/problems in 76th Ave overpass ● Realign trail here for safety <p>Other</p> <ul style="list-style-type: none"> ● (Gathering area dot at cul-de-sac on Sunset Highway) ● Foot, bike trail to 77th Ave SE (From bike path along N Mercer Connector) ● Madrona (cropped) creating sight distance issue at intersection of N Mercer Way and 76th ● Boundary check just north of this tree ● Trees that have been cut because of disease do not have to be replaced. Use money for more trees needing replacement elsewhere ● Maintain trees – all cedars dying along corridor ● Drug use and dumping at NE corner of 77th Ave SE and N Mercer Way ● Terrace trail along contour and open understory (reduce homeless) |
| PROGRAMMING Segment 5 | <p>Other</p> <ul style="list-style-type: none"> ● Love to see more public art ● Bikes in bike only lane ● Keep bikes on road (for bikes going east on SE 24th street) ● Yes, bikes on road |

Sound Transit Parking Area

- Need place for car share and drivers to drop-off/pick up riders
- Different colors between peds and cyclists would be better – can't see symbols from a far distance
- Hate green, walkers go on full width
- Need a bus rider drop-off/pick up area
- Bicycle crossing – in blind spot of left turn vehicle – hazard
- Please add yield sign for cyclists travelling east on N Mercer Way to yield to cars turning left (North) at 81st SE

New Sound Transit Station

- West end: Too much transparency and doesn't meet original MOA & intent
- East end: Keep lots of green and add more trees at entry
- East end: Minimal change – keep just as green and replace landscape

Sculpture Garden

- Mitigate the freeway noise and sight of new 77th Ave light rail station by re-planting evergreen trees in the open space where trees have died behind the bench
- Incorporate seating artistically along path
- I love artwork in the community

PROGRAMMING
Segment 6

Opportunity Area G

- Better walking space between SE 27th Street and entrance to Opportunity Area G
- Entrance to Luther Burbank Park from city
- Entrance to Luther Burbank Park from town center
- (Gathering area dot in NE corner)
- Luther Burbank outreach center

Opportunity Area H

- "The Lookout" – overhang area to the north
- (Gathering area dot in north section)
- Name and sign as "Luther Lid"
- Keep open/natural area

Other

- Keep the bikes out of the park
- No bikes in park
- Keep bikes on the I-90 bike trail

| | |
|----------------------------------|---|
| | <ul style="list-style-type: none"> ● No improvements and activities wanted in this part of the park |
| <p>PROGRAMMING Segment 7</p> | <p>Other</p> <ul style="list-style-type: none"> ● Connect Shorewood and Upper Luther Burbank Park via trail on south side of I-90 ● We like the park as it is! No changes! Use \$ for maintenance not new construction ● Complete east portion of Upper Luther trail up into Shorewood – now only ending in the ravine ● Existing trails to ravine in cul-de-sac in Shorewood neighborhood ● No improvements or activities wanted in this part of the park ● Put some water in the planters along the Shorewood drive overpass <p>Bikes</p> <ul style="list-style-type: none"> ● Bikes here along N Mercer Way ● Put bikes on road (widen) and 6’ trail only ● The city spent a lot of money making roads safe for bikers; keep them on roads |
| <p>PROGRAMMING Segment 8</p> | <p>Boat launch</p> <ul style="list-style-type: none"> ● Parcel by the boat launch should be acquired ● Restroom in area just to the right of the boat launch <p>Opportunity Area I</p> <ul style="list-style-type: none"> ● (Gathering area dot here) ● (Family activities dot here) ● There are no public playgrounds in this quadrant of the island – would be great to see a small playground for kids (x2) ● Access area for gatherings, bike rides/events (gather and park here) <p>Opportunity Area J</p> <ul style="list-style-type: none"> ● (Family activities dot here) (x2) ● Protect bike/pedestrian access to park along E Mercer Way <p>Other</p> <ul style="list-style-type: none"> ● (Family Activities dot in forested area behind Bright Horizons) ● Connector Trail to access park from Mercer Wood neighborhoods ● Pedestrian scale lighting to improve commute and improve safety ● Fix root bumps & cracks in asphalt of bike trail |

| | |
|--------------------------------|---|
| <p>TRAILS Lidded Park</p> | <ul style="list-style-type: none"> ● Add restroom on West side ● Trees along west side of I-90 trail need to be cut – are way overgrown ● Bikers speed down this hill [west side of I-90 trail] ● Loop walking trail along north side of lidded park ● Make it lighted (x2) ● Bikers speed on trail down the hill east of W Mercer Way – not safe for walkers ● Entrance to park needs barricade between 66th Ave parking lot and trail ● This sidewalk is bad – easy to fall down ● Bikes speed around corner approaching 72nd overpass ● Improve this area only at trail and overpass intersection ● Tennis courts: fix cracks, sign-up sheet (always people) ● Fix courts ● Sidewalk access to park on the north side of park at 72nd ● No sidewalks in neighborhoods south of park – need a place to walk! ● Love having sculpture in this park ● Bikes & off-leash dogs – conflicts ● Fencing or designated off-leash area ● Keep trail width as-is (x3) ● Safer roads for hwy bike users – trail for everyone else on other side |
| <p>TRAILS Town Center</p> | <ul style="list-style-type: none"> ● Move cyclists to road northbound (right lane – shared with bus) ● How will bikes get from bike trail to bike parking on 77th at light rail? Consider ped/bike/bridge to facilitate safe crossing ● Needs trees at sculpture garden ● Accommodate bikes through I/S of 81st and the PiR. Explore bike signals. High conflict areas that need fix on N Mercer Way ● Right hook issues at park & ride entry; signs, bike signal ● Need WF signs; bikes at park and ride ● Very tight spot at Island Crest Way apartments – not much room ● Sign alternate routes for bikes to avoid pinch point ● Tully's site - How will we get # cars in and out of here? ● Intersection with all extra cars at Sunset Hwy and 77th ● Lots of congestion on city streets on SE 27th |
| <p>TRAILS East Segment</p> | <ul style="list-style-type: none"> ● The wider you make it the more it'll attract users. ● Concerns with congestion at intersection from current land uses ● Cross Section D – No shoulder, but separate paved and gravel path where possible ● Safety warning (balls) ● Using road more because of bumps (roots) in trail |

| | |
|------------------|---|
| | <ul style="list-style-type: none"> ● Accesses to trail are dangerous ● Move high-speed cyclists to North Mercer Way ● Faster routes to trail are dangerous |
| TRAIL EXPERIENCE | <ul style="list-style-type: none"> ● Trail signage – where am I supposed to be? Bike signal at Park & Ride ● Wayfinding ● Map of the park – mileage marker ● Save the trees – mature ones ● Save the trees ● Preserve the natural feel of trail ● City to provide specific location of conflicts and accident data ● Need to get independent counts for bicyclists on existing trail <p>Trail Design</p> <ul style="list-style-type: none"> ● 14' trail probably a good idea - more leisurely, comfortable, safer. ● Keep to the 10' provide a second separated pedestrian trail ● Narrow trails promote slower speeds & safety! ● Delineated trail; separate trail for cyclists and peds ● Please do not widen trail! ● On 14' bike path, be sure to have a center stripe ● Please don't allow paths to be widened ● Very opposed to widening any portions to 14' (x7) ● Separate bike lanes! Cyclists move way too fast. All weave through pedestrians (esp. elderly). Very unsafe. ● Bikes belong on the roads not on park trails- dangerous for everyone else! ● Lighting on trails ● Separate bikes & peds, more "no dog" signs, voice control is a problem ● Keep to the 10'; provide a second separated ped trail ● 2 paved trails to separate wheels from peds (See Myrtle Edwards Park) – by C. Ridolfi ● Separate paved & gravel trails where possible; through cyclists should be on the road ● Dogs on-leash ● Only widen portions to address conflicts <p>Trail Location</p> <ul style="list-style-type: none"> ● Trail to Upper Luther Burbank needed ● Keep the bikes out of the park ● Bikes use streets not park paths |

| | |
|------------------|--|
| | <ul style="list-style-type: none"> ● The paved I-90 bike trail is important for bike traffic and off-island including bike commuters and recreational riders. <p>Trail Users</p> <ul style="list-style-type: none"> ● Safety for pedestrians getting to & on the trail ● No electric bikes on pathways ● Widen trail, slow down cyclists! ● The 1% experienced/confident bikers do not share the trail. Too fast for conditions! |
| TRAIL EXPERIENCE | <p>Wheels</p> <ul style="list-style-type: none"> ● 3 dots – faster through cyclist ● 5 dots – fitness cyclist/group riders ● 0 dots – e-bike cyclist ● 6 dots – casual through cyclist ● 4 dots – casual cyclist ● 2 dots – first/last milers ● 3 dots – family cyclist ● 1 dot – alternative wheels ● 0 dots – assisted mobility <p>Feet (4 dots in general)</p> <ul style="list-style-type: none"> ● 8 dots – jogger/runner ● 4 dots – fitness pedestrian ● 9 dots – casual pedestrian ● 1 dot – groups walking ● 2 dots – walking with strollers ● 3 dots – elderly and senior walkers ● 1 dot – visually impaired pedestrians <p>Other:</p> <ul style="list-style-type: none"> ● Please consider bicycle commuters (should be added as a category) ● This isn't Mercer Island ● How about the casual pedestrian? ● Scooter share – lime scooters ● Note no bikes belong on the roads! ● Elderly – this is a good one; putting bicyclists on the same path as the elderly; on your left ● Who is supposed to be on the left (x2) |

Mercer Island Aubrey Davis Park

DRAFT July 5, 2019 | Survey Analysis

Survey Overview

The second Mercer Island Aubrey Davis Park Survey was designed to complement the second open house, and similar to the open house, solicit feedback on the preliminary design options presented for Aubrey Davis Park. The survey was open from April 23rd, 2019 to June 7th, 2019. It is important to note that this is not a statistically valid survey.

The survey was presented as a graphic narrative, using the ESRI StoryMap platform with integrated design concepts and survey questions throughout the narrative. It was intended to be a more immersive survey, closer to the experience of attending an open house versus taking an online survey. The general structure showed the respondent a set of design concepts for a particular area of the park, then asked for feedback on those specific design concepts for that area. It was designed to take 15-20 minutes to complete, although that time varied due to the high amount of open-ended questions.

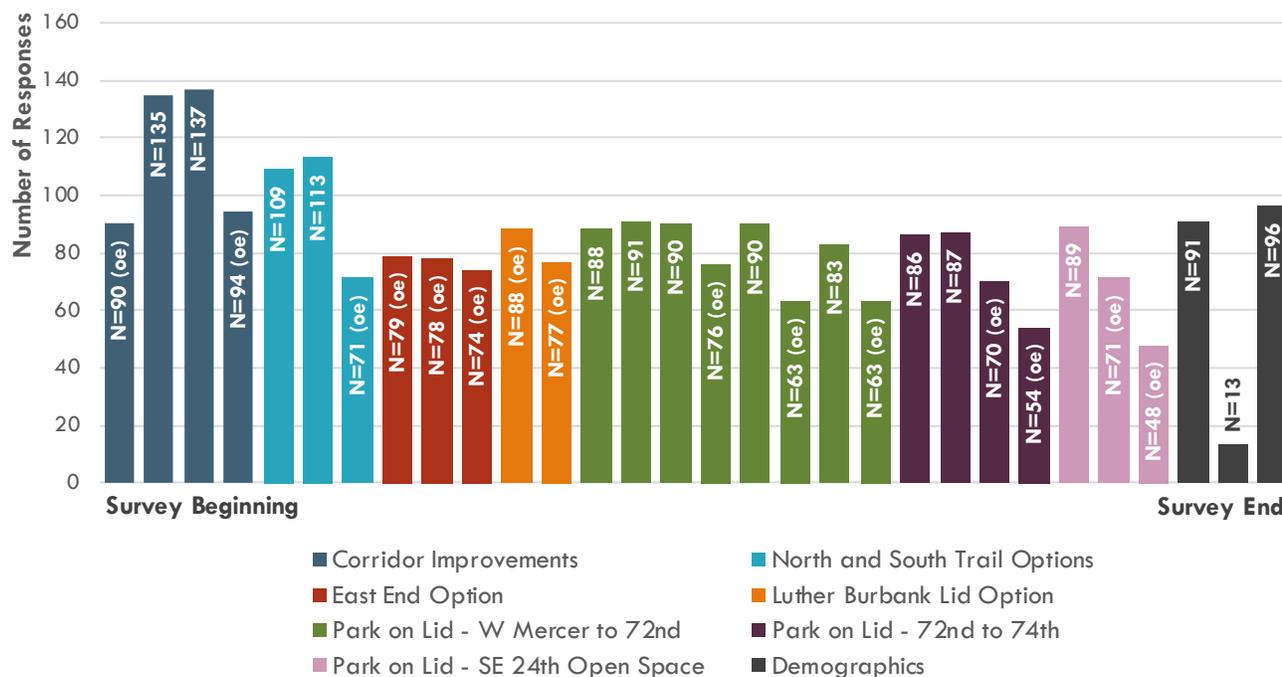
There were eight sections of survey:

1. Corridor Improvements
2. North and South Trail Options
3. East End Option
4. Luther Burbank Lid Option
5. Park on the Lid – West Mercer Way to 72nd Avenue SE
6. Park on the Lid – 72nd Avenue SE to 74th Avenue SE
7. Park on the Lid – SE 24th Street Open Space

While the survey was implemented to avoid people taking the survey multiple times based on the web settings used, it is possible that an internet-savvy user could have taken the survey multiple times in order to emphasize their opinion on the planning process.

The completion of the survey dropped off as users continued through the sections, as shown below in Exhibit 1.

Exhibit 1. Responses per Question



Note: OE in parenthesis (oe) signifies open-ended response question.

The Corridor Improvements and North and South Trail Options sections had the highest response rate, with the middle and end sections having a lower response rate overall. Additionally, the open-ended questions had far lower participation than questions in which the user gave a number rating or selected from a multiple-choice list.

Overall, the most common theme within the responses was to do nothing, and that outside of some safety improvements or better signage, the park functions well as it currently exists. Another theme was limiting the amount of new pavement and hardscaping.

The contingent of respondents speaking against any investment into the design options shown, and master planning process in general, was quite vocal and descriptive in the ways they felt this process was not a good use of money. Conversely, other users supported the long-term makeover of the park, and favorably viewed the design options shown.

The overall opinions expressed in the survey are highly varied, and in instances where specific design options were provided, favorability was often split such that no clear option was preferred, except in the instances where ‘do nothing’ or ‘neither’ was an option.

Corridor Improvements

1. Do you have any comments about the proposed trail improvement to the EAST CORRIDOR portion of the park only?

| | # | % |
|--|-----------|-----|
| Like the view provided by the balcony | 1 | 1% |
| Address rough trail surfaces | 1 | 1% |
| Against due to concerns about costs | 7 | 8% |
| Against due to lost natural area | 8 | 9% |
| Against use by bicycles | 4 | 5% |
| Against widening for bicycles | 1 | 1% |
| Bicycles should be on the street | 4 | 5% |
| Concern about gravel for ADA accessibility | 3 | 3% |
| Desire better pedestrian access to trails | 1 | 1% |
| Do nothing | 14 | 16% |
| General support | 3 | 3% |
| Make the trail narrower | 1 | 1% |
| More lighting | 1 | 1% |
| None | 11 | 13% |
| Not clear on proposal | 2 | 2% |
| Preserve natural features | 1 | 1% |
| Safety issues at crossings | 1 | 1% |
| Signage and speed enforcement needed on trail | 5 | 6% |
| Support trail improvements and shared use of trail | 10 | 11% |
| Use native plants | 1 | 1% |
| Want more differentiation between sections for bicycles and pedestrian | 6 | 7% |
| Want signage | 1 | 1% |
| TOTAL: | 87 | |
| Skipped: | 54 | |

Note: Items listed in this summary were compiled from key word or phrases in the open-ended questions.

Sample open-ended responses

- *Thank you for doing this! I particularly like the idea of a "balcony" on the west edge of the tennis courts to be able to sit and view the Seattle skyline and the Olympics.*
- *No need to change as it is. My family uses it daily and it works great. You want to spend money the city does not have on parks that are working great as they are.*
- *The restoration after the sewer project should be to the existing configuration. Do not pander to cyclists and make unnecessary changes to roads and trails, or paint ridiculous green paths and other wastes of money. I have cycled all my life - and I moved to Mercer Island to escape this kind of stupidity. Do not Seattlize MI.*
- *Anything that separates cyclists and pedestrians would be a good thing.*
- *Looks great - It's important to have a wide trail here.*
- *Prioritize pedestrian mobility and safety over bicyclists.*
- *I think these two options are opposed to what the citizens need or want. With the budget cuts that potentially will effect staffing at schools, the timing of this is not appropriate.*
- *I'm struggling to understand exactly what is being proposed. The concepts are vague. What would be changed, where?*
- *Can't you just leave well enough alone? Most of your "improvements" are not that helpful. The only thing that would really help is setting a speed limit for bicycles. They go way too fast, and treat any pedestrians, strollers, dogs on leash, or any thing on the pats as though they are moving slalom gates. There are some exceptional bikers who are courteous, but they are exceptions.*

2. On a scale of 1-5 (1- not likely to 5- very likely), how likely are you to support the proposed trail improvement to the PARK ON THE LID portion of the park only?

| | # | % | |
|---|----|-----|---|
| 1 | 50 | 37% |  |
| 2 | 6 | 4% |  |
| 3 | 14 | 10% |  |
| 4 | 22 | 16% |  |
| 5 | 43 | 32% |  |

TOTAL: 135

Skipped: 6

- The answers are skewed towards either extreme, with 41% of respondents saying they are unlikely or somewhat unlikely to support the proposed improvements, and 44% they are somewhat likely or likely to support the proposed improvements.

3. On a scale of 1-5 (1- not likely to 5- very likely), how likely are you to support potential shoulder, signage, and wayfinding improvements to NORTH AND WEST MERCER WAY portion of the park only?

| | # | % | |
|---|----|-----|---|
| 1 | 47 | 34% |  |
| 2 | 6 | 4% |  |
| 3 | 17 | 12% |  |
| 4 | 22 | 16% |  |
| 5 | 45 | 33% |  |

TOTAL: 137

Skipped: 4

- The two extremes were represented nearly equally, with 34% of respondents saying they are unlikely to support improvements to North and West Mercer Way portions of the park, while 33% of respondents say they are likely to support those improvements.

4. What would you change about the concepts shown in the Corridor Improvements section above?

| | # | % |
|--|-----------|-----|
| Add an off-leash dog park | 1 | 1% |
| Add improved crosswalk | 1 | 1% |
| Add speed bumps | 2 | 2% |
| Address rough trail surfaces | 3 | 3% |
| Against due to concerns about costs | 9 | 10% |
| Against due to lost natural area | 5 | 5% |
| Against use by bicycles | 8 | 9% |
| Against widening | 1 | 1% |
| Against widening for bicycles | 2 | 2% |
| Block pedestrian trail from bicycle access | 1 | 1% |
| Do not add an on-leash dog park | 1 | 1% |
| Do nothing | 17 | 18% |
| Don't use bright paint | 1 | 1% |
| General support | 3 | 3% |
| Include adult fitness equipment | 1 | 1% |
| Include speed limits and limited to non-motorized bicycles | 1 | 1% |
| More clear signage | 5 | 5% |
| None | 4 | 4% |
| Not clear on proposal | 4 | 4% |
| Prioritize natural areas | 5 | 5% |
| Prioritize pedestrian use | 3 | 3% |
| Prioritize safety | 1 | 1% |
| Pursue private revenue sources to minimize cost to residents | 1 | 1% |
| Remove gravel | 2 | 2% |
| Safety reminders for bicycles | 1 | 1% |
| Separation for pedestrian and bicycle use is unnecessary | 1 | 1% |
| Speed limit and leash law enforcement | 1 | 1% |
| Support widening but concerned about lost natural area | 1 | 1% |
| Want more differentiation between sections for bicycles and pedestrian | 8 | 9% |
| TOTAL: | 94 | |
| Skipped: | 47 | |

Note: Items listed in this summary were compiled from key word or phrases in the open-ended questions.

Sample open-ended responses

- *I like it.*
- *The cost. If the city is has budget constraints that cause it to be unable to fund counselors in our schools, etc... it should not spend \$\$ on the parks unless they are made to be revenue-generating improvements.*
- *Walking dogs is important and the fast paced bikers are a conflict. I would like to see a specified bike path when coming down the hills so that if we are walking we don't have to watch behind us.*
- *Leave as is with the exception of adding signage and/or speed bumps where the bikes go too fast close to the play fields*
- *Glad to see this finally happening!*

N and S Trail Options

In this section, the preference seems to differ from the opinions heard at the in-person open house. At the open house, participants seemed to favor the south trail alignment, however respondents of the online survey below were more strongly against the south trail alignment versus the north trail alignment.

5. On a scale of 1-5, where 1 is no way and 5 is absolutely, should the city pursue further feasibility of the NORTH TRAIL OPTION for a new regional trail bypass route?

| | # | % | |
|---|----|-----|---|
| 1 | 32 | 29% |  |
| 2 | 9 | 8% |  |
| 3 | 16 | 15% |  |
| 4 | 18 | 17% |  |
| 5 | 34 | 31% |  |

TOTAL: 109

Skipped: 6

- Nearly half of respondents (48%) said they think the city should or absolutely should pursue further feasibility of the North Trail Option, while 29% person were strong against this option.
- 15% of respondents were neutral.

6. On a scale of 1-5, where 1 is no way and 5 is absolutely, should the city pursue further feasibility of the SOUTH TRAIL OPTION for a new regional trail bypass route?

| | # | % | |
|---|----|-----|---|
| 1 | 46 | 41% |  |
| 2 | 9 | 8% |  |
| 3 | 21 | 19% |  |
| 4 | 11 | 10% |  |
| 5 | 26 | 23% |  |

TOTAL: 113

Skipped: 2

- Respondents were overall against this proposal, with 41% saying no way, the city should pursue further feasibility of the South Trail Option.
- 19% of respondents were neutral, and just one third of respondents were feeling favorable towards the city pursuing further feasibility of this option.

7. What would you change about the concepts shown in the North and South Trail section above?

| | # | % |
|---|-----------|-----|
| Add off-leash dog park | 1 | 1% |
| Add speed bumps | 3 | 4% |
| Against bright paint | 1 | 1% |
| Against due to concerns about costs | 3 | 4% |
| Against use by bicycles | 7 | 10% |
| Bicycles should use the North Trail option, not the South Trail option | 1 | 1% |
| Bicycles will continue to use N Mercer Way | 1 | 1% |
| Choose plants that don't damage the trail | 1 | 1% |
| Concern about cost | 5 | 7% |
| Do nothing | 11 | 16% |
| Do nothing, but increase signage | 1 | 1% |
| Done | 1 | 1% |
| General support | 4 | 6% |
| Improvements for bicycles on N Mercer Way | 1 | 1% |
| More analysis needed | 1 | 1% |
| Neither trail | 1 | 1% |
| No off-leash dog park | 1 | 1% |
| None | 2 | 3% |
| North Trail better keeps bicycles away from pedestrians | 2 | 3% |
| North Trail path does not make sense | 1 | 1% |
| Prioritize natural areas | 9 | 13% |
| Separate trails for bicycles and pedestrians are not needed | 1 | 1% |
| South Trail is most accessible to Seattle neighborhoods and should remain multipurp | 2 | 3% |
| Support splitting trails for bicycle and pedestrian use | 3 | 4% |
| Support widening for multipurpose use | 3 | 4% |
| Trail should parallel N Mercer | 1 | 1% |
| Unclear on proposal | 2 | 3% |
| TOTAL: | 70 | |
| Skipped: | 45 | |

Note: Items listed in this summary were compiled from key word or phrases in the open-ended questions.

- The largest contingency of respondents wished to do nothing at 17%, 13% of respondents said to prioritize natural areas, and 10% were against the use of bicycles.

Sample open-ended responses

- *I'd not change the trail routing. It's a waste of money. I would support maintenance of the existing trail and greenery.*
- *Maximally preserve natural habitat and minimize human intrusiveness.*
- *Leave as is except ensure bike lanes clearly marked on pavement and with warning signs.*
- *Separate the bicyclists from the pedestrians*

East End Option

8. What do you like about the concepts shown for the SE 35TH PLACE CUL-DE-SAC?

| | # | % |
|---|-----------|-----|
| Boat launch non-pay parking needed | 1 | 1% |
| Concern about the cost | 11 | 14% |
| Concern about transient use of parking | 1 | 1% |
| Connectivity | 1 | 1% |
| Connectivity and wayfinding | 1 | 1% |
| Dislike parking improvements | 1 | 1% |
| General support | 11 | 14% |
| Need more options | 1 | 1% |
| None | 1 | 1% |
| Not clear on the proposal | 5 | 6% |
| Nothing | 23 | 29% |
| Parking improvements | 11 | 14% |
| Parking improvements and seating area | 3 | 4% |
| Play area | 3 | 4% |
| Play area, parking improvements, and wayfinding | 1 | 1% |
| Seating area and wayfinding | 1 | 1% |
| Water access | 1 | 1% |
| Wayfinding | 1 | 1% |
| TOTAL: | 78 | |
| Skipped: | 15 | |

Note: Items listed in this summary were compiled from key word or phrases in the open-ended questions.

- Nearly 30% of respondents said they liked nothing about these concepts, and another 6% were not clear on the proposal.
- 14% voiced general support, 14% said they were concerned about the cost, and another 14% said they liked the parking improvements.

Sample open-ended responses

- *I think this area is underutilized so anything to improve the utilization is good.*
- *I like the idea of improved wayfinding and signage, though I do not think the area needs to be majorly redeveloped.*
- *Nothing. Too much manufactured human intrusiveness.*
- *It provides a great place for transients to park their cars and sleep.*
- *DO NOT LIKE IT. No one would ever use that play area right next to the freeway. Seems like a waste of money.*

9. What do you like about the concepts shown for the BOAT LAUNCH AREA?

| | # | % |
|---------------------------|-----------|-----|
| Access | 6 | 8% |
| Access and play area | 2 | 3% |
| Concern about the cost | 10 | 13% |
| General support | 8 | 10% |
| None | 4 | 5% |
| Not clear on the proposal | 3 | 4% |
| Nothing | 23 | 29% |
| Pedestrian experience | 1 | 1% |
| Play area | 12 | 15% |
| Play area and wayfinding | 1 | 1% |
| Restroom | 6 | 8% |
| Seating area | 1 | 1% |
| Wayfinding | 1 | 1% |
| TOTAL: | 78 | |
| Skipped: | 15 | |

Note: Items listed in this summary were compiled from key word or phrases in the open-ended questions.

- Again, 29% of respondents indicated they did not like the concepts shown, and 13% were concerned about cost.
- 15% liked the play area, 10% voiced general support, and 8% liked the bathroom.

Sample open-ended responses

- *I especially like the play structure for kids, especially near the water.*
- *This is a boat launch not a playground. We have used this launch for many years and as one of the few boat launches on MI it should stay at it is. Addition of restrooms would be fine.*
- *Make a value judgement. Not needed.*
- *The playscape stuff rocks. I've got grandkids!*
- *We need a bar and party boat area to drive rent revenue for city.*
- *Getting in and out quickly is the most important thing, not creating art installations.*
- *Flushing \$20 bills down the toilet would bring more satisfaction to me. This is going to be dark and noisy. No one walks by the boat launch: I used to live by there and no one goes there. Spend money fixing areas and things that people use.*

10. What would you change about either of the concepts shown here?

| | # | % |
|---|-----------|-----|
| Add drinking fountain | 1 | 1% |
| Add path for pedestrians | 2 | 3% |
| Boat launch should not be changed | 4 | 5% |
| Concern about the cost | 6 | 8% |
| Coordinate with other planned local development | 2 | 3% |
| Do not include an off-leash dog park | 1 | 1% |
| Do nothing | 22 | 30% |
| Ensure adequate lighting | 1 | 1% |
| General support | 2 | 3% |
| Improvements should prioritize pedestrians and bicycles | 1 | 1% |
| Include a roundabout | 1 | 1% |
| Include more natural areas | 2 | 3% |
| Include more parking | 1 | 1% |
| Integrate with synagogue | 1 | 1% |
| Keep restroom | 1 | 1% |
| Keep the shore area natural | 1 | 1% |
| No changes | 6 | 8% |
| None | 6 | 8% |
| Not clear on the proposal | 2 | 3% |
| Prioritize natural areas | 1 | 1% |
| Remove parking | 1 | 1% |
| Remove play area | 5 | 7% |
| Remove restrooms | 3 | 4% |
| Use native plants | 1 | 1% |
| TOTAL: | 74 | |
| Skipped: | 19 | |

Note: Items listed in this summary were compiled from key word or phrases in the open-ended questions.

- 30% of respondents suggest doing nothing, while 8% say they're concerned about the cost.

Sample open-ended responses

- Be sure to make the restrooms make it into the plan.
- Cul-de -sac should have basic maintenance. Boat launch restrooms would be great but it should stay a boat launch.
- Forget the playscape if Mercer Island is going to pay for it. We have bigger problems on Mercer Island that need fixing, not something new to maintain. If we were in better financial shape, then maybe.
- Looks like a good improvement.
- I question public restrooms - their draw for homeless people, public funds for upkeep, security, vandalism.

Luther Burbank Lid Option

What do you like about the LUTHER BURBANK LID concepts shown here?

| | # | % |
|--|-----------|-----|
| Concern about the cost | 7 | 8% |
| General support | 12 | 14% |
| More information needed | 1 | 1% |
| Natural focus | 4 | 5% |
| None | 2 | 2% |
| Not clear on the proposal | 1 | 1% |
| Nothing | 23 | 26% |
| Nothing - prioritize natural areas | 9 | 10% |
| Pedestrian trail on the south side of 1-90 | 1 | 1% |
| Picnic area | 1 | 1% |
| Picnic area and staircase | 1 | 1% |
| Play areas | 12 | 14% |
| Playful design | 4 | 5% |
| Prioritize natural areas | 2 | 2% |
| Proximity to dense housing | 1 | 1% |
| Scenic outlook and staircase | 1 | 1% |
| Stair connection from 84th Ave and community areas | 1 | 1% |
| Staircase | 4 | 5% |
| Too fragile | 1 | 1% |
| TOTAL: | 88 | |
| Skipped: | 8 | |

Note: Items listed in this summary were compiled from key word or phrases in the open-ended questions.

- 36% of respondents said do nothing, or do nothing and prioritize natural areas.
- 14% voiced general support, and 14% supported the play areas

Sample open-ended responses

- *Very clever! I appreciate the idea of natural looking playstructures.*
- *I like the idea of adding a play area and trees to the park.*
- *I think they are beautiful. Luther Burbank provides an incredible opportunity for children to experience the natural world close to the urban center of Mercer Island, so natural play structures seem appropriate and more in line with their surroundings. The structures also may prompt conversations and learning opportunities for children (for example playing the nest structures may lead to learning about our native bird species), and less plastic is always welcome!*
- *This is ridiculous. Instead of building a playground for children, let's fund YFS counselors to improve their mental health and keep our children safe.*
- *This space has always been rarely used and any improvements would be a waste of money.*

11. What would you change about the LUTHER BURBANK LID concepts shown here?

| | # | % |
|---|-----------|-----|
| Add shelter | 1 | 1% |
| Add wayfinding | 1 | 1% |
| Concern about safety on multiuse path | 3 | 4% |
| Concern about the cost | 8 | 10% |
| Concern about vandalism | 1 | 1% |
| Connect to commercial district | 1 | 1% |
| Do not remove the basketball courts or include a dog park | 1 | 1% |
| Do nothing | 11 | 14% |
| Do nothing - prioritize natural areas | 3 | 4% |
| General support | 8 | 10% |
| Improve bike park at Snake Hill | 1 | 1% |
| Include a grassy field for use as a dog park | 1 | 1% |
| Include a unique feature like a climbing wall or skate park | 1 | 1% |
| Include parking | 1 | 1% |
| Include solar panels | 1 | 1% |
| Include trails for pedestrians and bicycles | 1 | 1% |
| Include volleyball court | 1 | 1% |
| Increase ADA accessibility | 3 | 4% |
| Increase connection to other parks | 1 | 1% |
| Keep design simple | 1 | 1% |
| More information needed | 2 | 3% |
| None | 3 | 4% |
| Not clear on the proposal | 1 | 1% |
| Play areas should be suitable for a range of ages | 2 | 3% |
| Playful natural elements do not make sense | 4 | 5% |
| Prioritize natural areas | 6 | 8% |
| Prioritize walking trails | 1 | 1% |
| Remove play areas | 3 | 4% |
| Remove restrooms | 1 | 1% |
| Replace improvements with unpaved walking path | 1 | 1% |
| Use funds for park maintenance instead of improvements | 2 | 3% |
| Use native plants | 1 | 1% |
| TOTAL: | 77 | |
| Skipped: | 19 | |

Note: Items listed in this summary were compiled from key word or phrases in the open-ended questions.

Sample open-ended responses

- *Eliminate all the manufactured items. They become a target for vandalism and another area of city maintenance burden.*
- *They represent the desires of out-of-control bureaucrats at the city, county and state levels ALL desiring to burn public resources for the most trivial and frivolous purposes imaginable.*
- *Make sure there are good walking/ biking trails with wayfinding throughout this lid. My sense is it's not heavily used or even known about.*
- *Just create a walking path...preferably unpaved*

Park on Lid – W Mercer to 72nd

12. On a scale of 1-5, where 1 is no way and 5 is absolutely, should the city consider synthetic surfacing for the soccer field on the lid?

| | # | % | |
|---|----|-----|---|
| 1 | 39 | 44% |  |
| 2 | 8 | 9% |  |
| 3 | 14 | 16% |  |
| 4 | 11 | 13% |  |
| 5 | 16 | 18% |  |

TOTAL: 88

Skipped: 8

- Overwhelmingly, 53% of respondents believed the city should not consider synthetic surfacing for the soccer field on the lid, whereas 31% felt the city should consider this option. 16% were neutral.

13. On a scale of 1-5, where 1 is no way and 5 is absolutely, should West Mercer Way be improved for drop-off and ADA parking?

| | # | % | |
|---|----|-----|---|
| 1 | 37 | 41% |  |
| 2 | 7 | 8% |  |
| 3 | 20 | 22% |  |
| 4 | 10 | 11% |  |
| 5 | 17 | 19% |  |

TOTAL: 91

Skipped: 5

- Over 50% of respondents felt the city should not improve drop-off and ADA parking, whereas 30% felt this option should be considered. 22% were neutral.

EXPANDING GATHERING/FAMILY ACTIVITIES

14. Which concept do you prefer for expanding gathering / family activities in the open space near West Mercer Way and existing basketball courts and why?

| | # | % | |
|----------|----|-----|---|
| Option A | 10 | 11% |  |
| Option B | 28 | 31% |  |
| Neither | 52 | 58% |  |

TOTAL: 90

Skipped: 6

- Most people (58%) did not feel that either option was suitable. 31% supported Option B, and 11% supported Option A.

Responses if 'Option A' was selected:

| | # | % | |
|--|----------|-----|--|
| Add the off leash dog area | 1 | 13% | |
| Better use of space currently used for basketball courts | 1 | 13% | |
| Do not remove the tennis courts | 1 | 13% | |
| Natural play elements | 1 | 13% | |
| Prioritize basketball courts | 1 | 13% | |
| Rain shelter | 1 | 13% | |
| Suggest restricting parking | 1 | 13% | |
| Turfed field | 1 | 13% | |
| TOTAL: | 8 | | |
| Skipped: | 2 | | |

Note: Items listed in this summary were compiled from key word or phrases in the open-ended questions.

Responses if 'Option B' was selected:

Why do you prefer Option B?

| | # | % | |
|---|-----------|-----|--|
| Option A does not maintain the park's natural look and feel | 1 | 4% | |
| General support | 1 | 4% | |
| Like the bouldering/fitness areas | 5 | 22% | |
| More activities for kids | 1 | 4% | |
| More green space | 5 | 22% | |
| Natural play elements | 2 | 9% | |
| Off leash dog area | 2 | 9% | |
| Preserves basketball courts | 2 | 9% | |
| Prioritize basketball courts | 3 | 13% | |
| Remove off leash dog area | 1 | 4% | |
| TOTAL: | 23 | | |
| Skipped: | 5 | | |

Note: Items listed in this summary were compiled from key word or phrases in the open-ended questions.

Sample open-ended responses

- *A is ugly, b is ok but don't love it. I REALLY want an open area for dogs. LB was ruined with a silly sand pit and hope that you don't take away our ability to walk when the rain/winds prevent us from using pioneer park.*
- *Opt B seems is preferred as it seems to leave more of the greenspace intact, which is the point of a park.*
- *We need more green...trees, plants. This whole plan seems to be catering to kids, what about the adults and dog walkers.*
- *I like the more natural option as it will fit the location better*

Responses if 'Neither' was selected:

| Why do you prefer neither option? | # | % |
|---|-----------|-----|
| Do not like off leash dog area | 1 | 2% |
| Basketball and tennis courts should both be preserved | 1 | 2% |
| Bicycles and pedestrians should not share a path | 1 | 2% |
| Both options remove green space | 8 | 18% |
| Concern about the cost | 6 | 14% |
| Concern about use by the homeless | 1 | 2% |
| Confused about survey | 2 | 5% |
| Do not remove the tennis courts | 2 | 5% |
| More parking is unnecessary | 3 | 7% |
| Natural play elements | 1 | 2% |
| No changes are needed | 14 | 32% |
| None | 1 | 2% |
| Options cater too much to kids and bicycles | 1 | 2% |
| Prioritize basketball courts | 2 | 5% |
| TOTAL: | 44 | |
| Skipped: | 8 | |

Note: Items listed in this summary were compiled from key word or phrases in the open-ended questions.

- Most people selected this option because they believe no changes are needed, and don't like that both Option A and Option B remove green space.
- As is a theme throughout the survey, respondents are concerned about the cost of all proposals.

Sample open-ended responses

- It's perfect as is. Use the money for other pressing issues around the City.*
- Looks like a lot of open space is being taken away with either concept. To me the park is a place to enjoy greenery, not structures or enormous wide pathways. Keep the parks dirt, grass, trees, blackberries, & shrubs, not concrete, structures & wide paved areas.*
- Sufficient space exists elsewhere on Mercer Island for Mercer Island citizens. The proposed expansions would encourage regional infringement.*
- The park as it is great. It needs some TLC but not a re purposing.*
- Picnic areas are ok but I would prefer to keep the area open and natural with trees and grass.*
- I like them just the way they are. If you start introducing climbing walls, should someone have a bad fall and injure themselves, are you, the City, responsible? And you keep forgetting, you don't have money. Why are you looking to come up with expensive projects when you don't have money? It doesn't make sense.*

TENNIS COURT ACCESSIBILITY AND MORE GATHERING/FAMILY ACTIVITIES IN THE AREA

15. Which concept do you prefer for making the tennis court area more accessible and programming more gathering / family activities in this area and why? (or neither)

| | # | % |
|---------------|-----------|-----|
| Option A | 9 | 10% |
| Option B | 23 | 26% |
| Neither | 58 | 64% |
| TOTAL: | 90 | |
| Skipped: | 6 | |

Response if 'Option A' was selected:

Why do you prefer Option A?

| | # | % |
|------------------------------|----------|-----|
| More functional | 1 | 13% |
| More aesthetically pleasing | 1 | 13% |
| Offers more seating | 1 | 13% |
| Prefer off leash area | 1 | 13% |
| Prefer open lawn | 1 | 13% |
| Tennis courts are not needed | 1 | 13% |
| TOTAL: | 6 | |
| Skipped: | 3 | |

Note: Items listed in this summary were compiled from key word or phrases in the open-ended questions.

Response if 'Option B' was selected:

Why do you prefer Option B?

| | # | % |
|-------------------------------|-----------|-----|
| General support | 1 | 13% |
| Includes activities for teens | 1 | 13% |
| More family friendly | 1 | 13% |
| Paths are more direct | 1 | 13% |
| Picnic areas | 1 | 13% |
| Prefer off leash area | 1 | 13% |
| Preserves tennis courts | 1 | 13% |
| Prioritize green space | 2 | 25% |
| Prioritize play areas | 1 | 13% |
| TOTAL: | 10 | |
| Skipped: | 13 | |

Note: Items listed in this summary were compiled from key word or phrases in the open-ended questions.

Response if 'Neither' was selected:

Why do you prefer neither option?

| | # | % |
|--|-----------|-----|
| Confused about survey | 1 | 2% |
| Both options remove green space | 1 | 2% |
| Cars not needed | 1 | 2% |
| Concern about the cost | 1 | 2% |
| Confused by the survey | 1 | 2% |
| Congestion | 1 | 2% |
| Do not include the off leash area | 1 | 2% |
| Encourages use by non-residents | 3 | 7% |
| Lawn by tennis courts should be repurposed | 2 | 4% |
| More activities for teens | 1 | 2% |
| No changes are needed | 20 | 44% |
| No opinion on tennis courts | 1 | 2% |
| Not ADA accessible | 1 | 2% |
| Other activities are needed | 1 | 2% |
| Park improvements will not be used | 1 | 2% |
| Parking is currently adequate | 1 | 2% |
| Preserve the tennis courts | 4 | 9% |
| Tennis courts are not needed | 1 | 2% |
| Too elaborate | 1 | 2% |
| Viewing area instead of tennis courts | 1 | 2% |
| TOTAL: | 45 | |
| Skipped: | 13 | |

Note: Items listed in this summary were compiled from key word or phrases in the open-ended questions.

- 44% of respondents suggest no changes are needed, 9% wish to preserve the tennis courts, and 7% are concerned the options encourage use by non-residents.

Sample open-ended responses

- Keep the playground where it is but make it more interesting and active. Places to hide and climb. Less pre-fab boring play structures.*
- Saving money by keeping the tennis courts in their current spot, and adding an activity zone is a good balance. Adding many picnic tables in this area will not get as much use for the money.*
- The tennis courts are very accessible now, but other activities should be added to the area.*
- This area is fine as it is. There is plenty of parking and the picnic tables and open grass areas do not need to be changed.*
- FISCALLY IRRESPONSIBLE. COURTS ARE PERFECT. PLEASE STOP ADDING PAVEMENT TO OUR PARKS.**
- Increased accessibility to facilities will encourage more off island users.*

IMPROVING AREA OF STACKS FOR ACCESSIBILITY AND SECURITY

16. Which concept do you prefer for improving the use of the area around the existing stacks to improve accessibility and increase eyes on the park for safety and security in this area and why? (or neither)

| | # | % |
|---------------|-----------|-----|
| Option A | 6 | 7% |
| Option B | 21 | 25% |
| Neither | 56 | 67% |
| TOTAL: | 83 | |
| Skipped: | 13 | |

- Two thirds of respondents do not like either option.

Response if 'Option A' was selected:

Why do you prefer Option A?

| | # | % |
|----------------------------------|----------|-----|
| Do not include an off leash area | 1 | 13% |
| General support | 1 | 13% |
| Open spaces are not cozy | 1 | 13% |
| TOTAL: | 3 | |
| Skipped: | 3 | |

Note: Items listed in this summary were compiled from key word or phrases in the open-ended questions.

Response if 'Option B' was selected:

Why do you prefer Option B?

| | # | % |
|-----------------------------|-----------|-----|
| Bouldering area | 1 | 8% |
| General support | 1 | 8% |
| More aesthically pleasing | 1 | 8% |
| Offers more activities | 1 | 8% |
| Prioritize green space | 1 | 8% |
| Security is not an issue | 1 | 8% |
| Support the off leash area | 6 | 46% |
| Support the shade structure | 1 | 8% |
| TOTAL: | 13 | |
| Skipped: | 8 | |

Note: Items listed in this summary were compiled from key word or phrases in the open-ended questions.

Response if 'Neither' was selected:

Why do you prefer neither?

| | # | % |
|--|-----------|-----|
| Area is already a de facto off leash area | 1 | 2% |
| Concern about the cost | 2 | 5% |
| Confused about survey | 5 | 12% |
| Do not include an off leash area | 2 | 5% |
| Dog walkers concerned about being restricting to | 1 | 2% |
| Encourages use by non-residents | 1 | 2% |
| Ensure fence is tall | 1 | 2% |
| Install cameras | 1 | 2% |
| More information needed | 1 | 2% |
| No changes are needed | 15 | 36% |
| None | 1 | 2% |
| Off leash area should be fenced | 1 | 2% |
| Play areas are too safe - safety is a parent's job | 1 | 2% |
| Prefer only the look-out area | 1 | 2% |
| Preserve basketball courts | 1 | 2% |
| Prioritize green space | 3 | 7% |
| Remove bushes | 1 | 2% |
| Security is not an issue | 3 | 7% |
| TOTAL: | 42 | |
| Skipped: | 14 | |

Note: Items listed in this summary were compiled from key word or phrases in the open-ended questions.

- There were varied reasons why people chose 'neither', however 36% of those who chose this option suggest no changes are needed.

Sample open-ended responses

- I would say just ensure that the fence around is very tall and to make sure that there is very dense vegetation around the fence to prevent anyone from climbing it.*
- It's perfect as is. Use the money for other pressing issues around the City.*
- People will allow their dogs off leash in that area whether you say they can or not. With no enforcement of the leash rules in that area they have already de facto made it an off leash area and it will be very difficult to change that behavior.*
- MI can't afford any part of this "improvement"*
- I think we need to take out all the bushes. As someone who lives near the LID - this is where all the trouble happens. This is where teens do things like drink, smoke, etc. We don't need added accessibility.*
- If the City is concerned about security, install cameras. Allow the community to paint the stacks - perhaps a place where graffiti is allowed - maybe people would be less likely to do it elsewhere.*

Park on Lid – 72nd to 74th

17. On a scale of 1-5, where 1 is no way and 5 is absolutely, would you support a concept that included additional ADA parking and a drop-off area near the cul-de-sac at 74th Avenue SE?

| | # | % | |
|---|----|-----|---|
| 1 | 36 | 42% |  |
| 2 | 8 | 9% |  |
| 3 | 11 | 13% |  |
| 4 | 19 | 22% |  |
| 5 | 12 | 14% |  |

TOTAL: 86

Skipped: 7

- 42% of respondents are strongly unfavorable to ADA parking and drop-off areas in this portion of the Park on the Lid.

INCREASE GATHERING/FAMILY ACTIVITIES EAST OF BALLFIELDS

18. Which concept do you prefer to increase gathering / family activities in the open space area east of the existing ballfields and why? (or neither)

| | # | % | |
|----------|----|-----|---|
| Option A | 8 | 9% |  |
| Option B | 32 | 37% |  |
| Neither | 47 | 54% |  |

TOTAL: 87

Skipped: 6

Response if 'Option A' was selected:

| | # | % | |
|----------------------------|---|-----|---|
| Concern about parking | 1 | 20% |  |
| Keep tennis courts | 1 | 20% |  |
| More green space | 2 | 40% |  |
| Support off leash dog park | 1 | 20% |  |

TOTAL: 5

Skipped: 3

Note: Items listed in this summary were compiled from key word or phrases in the open-ended questions.

Response if 'Option B' was selected:

| | # | % |
|----------------------------|-----------|-----|
| Expand playground | 1 | 5% |
| Improve playground | 1 | 5% |
| Increase gathering spaces | 1 | 5% |
| Keep tennis courts | 1 | 5% |
| Like overlook | 4 | 18% |
| More green space | 1 | 5% |
| More play structures | 2 | 9% |
| No change | 4 | 18% |
| No off leash | 3 | 14% |
| Support off leash dog park | 3 | 14% |
| Support trails | 1 | 5% |
| TOTAL: | 22 | |
| Skipped: | 10 | |

Note: Items listed in this summary were compiled from key word or phrases in the open-ended questions.

Response if 'Neither' was selected:

| | # | % |
|----------------------------|-----------|-----|
| Add ballfields | 1 | 3% |
| Concern about bikes | 2 | 5% |
| Concern about costs | 6 | 15% |
| Concern about homeless | 1 | 3% |
| Confused about the options | 1 | 3% |
| Increase ADA parking | 1 | 3% |
| Keep green space | 1 | 3% |
| Keep tennis courts | 2 | 5% |
| More green space | 3 | 8% |
| Neither | 4 | 10% |
| No change | 12 | 30% |
| No off leash | 6 | 15% |
| TOTAL: | 40 | |
| Skipped: | 7 | |

Note: Items listed in this summary were compiled from key word or phrases in the open-ended questions.

Sample open-ended responses

- *Don't love hardscapes. Not sure what you think it will be used for. Prefer as much greenery and open as possible (no fences).*
- *I use dog parks, they also increase year round use in the park, whereas younger families use the parks less in the winter than dog families*
- *Playgrounds near sporting fields is very helpful for siblings of athletes. I do love the observation area concept.*
- *Again I vehemently oppose off leash areas near highly trafficked play areas*
- *Increased shelters will encourage homeless gatherings like just across the floating bridge in Seattle.*
- *There are parts of both plans that I like but I don't love either one.*

19. What would you change in any of the concepts?

| | # | % |
|----------------------------|-----------|-----|
| Add bouldering area | 1 | 2% |
| Add parking | 1 | 2% |
| Add volleyball court | 1 | 2% |
| Amenities for young people | 3 | 6% |
| Concern about bikes | 4 | 8% |
| Concern about costs | 8 | 16% |
| Concerns about traffic | 1 | 2% |
| Increase ADA Parking | 1 | 2% |
| Increase open space | 2 | 4% |
| Keep off leash | 4 | 8% |
| Keep tennis courts | 4 | 8% |
| More open space | 1 | 2% |
| No change | 13 | 25% |
| No off leash | 5 | 10% |
| Reduce concrete | 2 | 4% |
| TOTAL: | 51 | |
| Skipped: | 42 | |

Note: Items listed in this summary were compiled from key word or phrases in the open-ended questions.

Sample open-ended responses

- *It's perfect as is. Use the money for other pressing issues around the City.*
- *We need to retain green space. We do not have to pave over areas of the park. Keep it simple and think about how much this will cost. This is not a bicycle throughway. People need to be able to walk and enjoy each other when they meet without worrying about being run over by a fast moving bike.*
- *I'm guessing that these changes will also include an attempt to limit dogs and their owners from the park. I do not support this so any proposal will also have to state that no new areas will be made out of limits for dogs and their owners.*
- *Eliminate them...a waste of \$*
- *Just please leave everything alone. If there is a way to make some traffic calming measures without being gaudy, then fine, but please show us. And also show us where the fast bicyclists will be riding.*
- *Support Option A if dog park will be in final design. Don't see need for fence around ball fields.*
- *Not too sold on the off the leash area.*
- *I support ADA parking & accessibility as well as improved drop off areas, but I don't want to expand parking otherwise. I would like the priority to be to keep our green spaces green and see the city/state prioritize alternative modes of transportation (like bikes/bike shares/walking/bus service) to get to and from the park.*
- *This all sounds like a significant increase in our property taxes down the road. Enough is enough.*

Park on Lid – SE 24th Open Space

20. Which concept do you prefer to increase gathering / family activities in the SE 24th Street Open Space and why? (or neither)

| | # | % |
|---------------|-----------|-----|
| Option A | 19 | 21% |
| Option B | 18 | 20% |
| Neither | 52 | 58% |
| TOTAL: | 89 | |
| Skipped: | 3 | |

- As is a theme throughout this survey, the majority of respondents (58%) did not prefer either option.

Response if 'Option A' was selected:

| | # | % |
|-----------------------------------|-----------|-----|
| Concerns about safety | 1 | 7% |
| Like wayfinding | 1 | 7% |
| More amenities for young people | 1 | 7% |
| More general use | 1 | 7% |
| More green space | 2 | 14% |
| No major changes | 1 | 7% |
| No more parking | 2 | 14% |
| No preference | 1 | 7% |
| Support additional picnic options | 1 | 7% |
| Visually appealing | 3 | 21% |
| TOTAL: | 14 | |
| Skipped: | 5 | |

Note: Items listed in this summary were compiled from key word or phrases in the open-ended questions.

Response if 'Option B' was selected:

| | # | % |
|------------------------|-----------|-----|
| More active uses | 1 | 8% |
| More interactive space | 1 | 8% |
| No major changes | 1 | 8% |
| No preference | 1 | 8% |
| Visually appealing | 8 | 67% |
| TOTAL: | 12 | |
| Skipped: | 6 | |

Note: Items listed in this summary were compiled from key word or phrases in the open-ended questions.

Response if 'Neither' was selected:

| | # | % |
|----------------------------|-----------|-----|
| Concerns about costs | 6 | 14% |
| Concerns about maintenance | 1 | 2% |
| Dislike changes | 1 | 2% |
| More green space | 8 | 19% |
| More natural materials | 6 | 14% |
| No changes | 10 | 23% |
| No major changes | 4 | 9% |
| No more concrete | 3 | 7% |
| No preference | 2 | 5% |
| Preserve green space | 1 | 2% |
| Too much change | 1 | 2% |
| TOTAL: | 43 | |
| Skipped: | 9 | |

Note: Items listed in this summary were compiled from key word or phrases in the open-ended questions.

Sample open-ended responses

- *Both of these plans destroy the natural, peaceful part of the park. Most of the people that go there to enjoy nature. This current plans eliminate the glassy areas in favor of pavement.*
- *It's perfect as is. Use the money for other pressing issues around the City.*
- *I probably sound like a broken record, but this project is to enhance a green space that crosses Mercer Island. We have plenty of urban space where families can experience modern art, medal structures, and plastic playgrounds. This space is green, should stay green, and should teach our children how to live more sustainably and in harmony with our natural world.*
- *I don't want to add car access to the park. I think we should keep green areas green. Option B with a parking focus would make this area more unpleasant for cyclists, pedestrians and families.*
- *I like this playful look with opportunities for kids to engage.*
- *This area could be used as is for walking up to the more open area where the fields are and dog area is.*
- **FISCALLY IRRESPONSIBLE. CITIZENS WANT TREES AND NATURAL NATIVE FLORA. PLEASE FOCUS MONEY ON PLATING NATIVE CONIFIER TREES WHERE POSSIBLE**
- *Let's try and keep it natural !!*
- *Too much money and citizen time has already been wasted. Citizens do not want wider paths or more paved surfaces in our parks. Retain green space.*
- *Option B with its artistic use of materials that can also be played with/on is a more visually appealing and functional use of the space.*

21. What would you change in any of the concepts?

| | # | % |
|----------------------------|-----------|-----|
| Add bouldering | 1 | 2% |
| Add off leash | 1 | 2% |
| Concerns about bikes | 1 | 2% |
| Concerns about costs | 3 | 7% |
| Concerns about maintenance | 1 | 2% |
| Confused | 1 | 2% |
| Dislike changes | 4 | 9% |
| Like play structure | 2 | 5% |
| More ballfields | 1 | 2% |
| More green space | 9 | 20% |
| More natural materials | 2 | 5% |
| More seating | 3 | 7% |
| No changes | 12 | 27% |
| No preference | 1 | 2% |
| Not visually appealing | 1 | 2% |
| Too much change | 1 | 2% |
| TOTAL: | 44 | |
| Skipped: | 48 | |

Note: Items listed in this summary were compiled from key word or phrases in the open-ended questions.

Sample open-ended responses

- *Don't put a bunch of structures and concrete in the park!*
- *Burn them, or visit them on some other community gullible enough to accept your "choices".*
- *Definitely NO MORE art. The "art" that you have is hideous.*
- *Leave it relatively as-is.*
- *Not necessary, not environmentally friendly, not inviting, not safe.*
- *Get rid of all the bright lights for a start, and the metal bars, and the cement. It is too artificial. We are a bedroom community which you keep forgetting.*
- *Make a combination of uses. Wayfinding and seating should be included but perhaps expanded seating including tables/shaded areas given the lack of adequate town center gathering places.*
- *I would leave it natural as is*
- *Please, just think natural. This looks like they are going to pave over the parks, we don't want that. Where are you going to get the money from for these projects?*
- *Redirect high-speed bikers to North Mercer Way.*
- *Include picnic space, add trees. A play structure invites families.*

Demographics

22. What neighborhood do you live in?

| | # | % |
|-----------------------|-----------|-------|
| East Mercer | 8 | 9% |
| East Seattle | 7 | 7.3% |
| Ellis Pond | 2 | 2.1% |
| First Hill | 19 | 19.8% |
| Forest Avenue | 1 | 1.0% |
| Fruitland | 1 | 1.0% |
| Island Point | 3 | 3.1% |
| Mercer Island Estates | 2 | 2.1% |
| Mercerdale | 4 | 4.2% |
| Mercerwood | 7 | 7.3% |
| North End | 14 | 14.6% |
| South End | 8 | 8.3% |
| The Lakes | 3 | 3.1% |
| Town Center | 6 | 6.3% |
| West Mercer | 6 | 6.3% |
| TOTAL: | 91 | |
| Skipped: | 9 | |

- Roughly 20% of respondents are from the First Hill neighborhood, with about 15% from the North End.

23. What age group are you in?

| | # | % |
|---------------|-----------|-------|
| <18 | 0 | 0.0% |
| 18-24 | 2 | 2.1% |
| 25-34 | 6 | 6.3% |
| 35-44 | 15 | 15.6% |
| 45-54 | 21 | 21.9% |
| 55-64 | 22 | 22.9% |
| 65+ | 30 | 31.3% |
| TOTAL: | 96 | |
| Skipped: | 4 | |

- Over half of the respondents are over the age of 55, and nearly 75% are 45 years old or older.
- Very few younger residents responded to the survey.

Aubrey Davis Park Master Plan

Open House #2 | MEETING NOTES

February 28, 2019

| Plan Area | Notes |
|---------------------------------------|---|
| Corridor Trail Improvements (Sheet 1) | <p>Overall (9 orange dots)</p> <ul style="list-style-type: none"> ● Need more options for local residents. No more parking. What about options for kids 8-22? ● Don't widen the park trails ● Focus on resident needs first ● This is too wide for pedestrian crossing ● Raise up crosswalk to enhance visibility of pedestrians and bikes to cars, slow cars down ● More parking would be good with access to picnicking ● Too many bike/ped accidents here (2 crossings at 72nd Ave SE). Youth do not expect bike when heading to bathroom – and vice versa ● Try to get WSDOT to replant, water and care for trees ● Where is the bike path to the town center? ● Sculpture park between 77th and 78th. Remove ivy from trees. Replant trees that have died. Prevent freeway pollution. ● Please take the bikes off this path altogether (around Mercer Way, SE 24th ST and 81st Ave SE) and have them take the bikes along 84th Ave and use the bike paths ● On-street bikes – How does city pay for bike lanes? ● On-street improvements won't push bike users onto the street – particularly commuters – will use the most direct route. <p>Park on the lid trail section (6 green dots, 7 orange dots)</p> <ul style="list-style-type: none"> ● Put speedbumps on trails to slow down the bikes <p>East corridor trail section (3 green dots, 8 orange dots)</p> <ul style="list-style-type: none"> ● Put speedbumps on trails to slow down the bikes ● I understand wanting trails narrow but safety matters. 2' is not wide enough to provide safety for a parent with a child |

| | |
|---------------------------------------|--|
| | <p>Traffic Calming examples (Left to right: A - 3 orange dots; B - 4 green dots, 2 orange dots; C - 2 green dots, 1 orange dot; D - 2 green dots, 1 orange dot; E - 2 green dots, 3 orange dots)</p> <ul style="list-style-type: none"> ● I like this colorful "calming" ● Yes bike lanes on the road |
| Corridor Trail Improvements (Sheet 2) | <p>Overall ()</p> <ul style="list-style-type: none"> ● Need lights in winter – pedestrians worried about being hit by bikes in the winter ● No ped route here. Keep natural (2 green dots) ● May want to limit seating for maintenance. Focus at slopes (top of hills) ● Please add a crosswalk across SE 36th Street so that pedestrians can easily cross the trail ● Please create a trail through this city owned property to connect the Mercerwood Neighborhood to the park/trail //// |
| PROCESS TO DATE | <p>Under "Sports and Boating"</p> <ul style="list-style-type: none"> ● No more parking is needed; all spaces are only taken for times like Blue Angels |
| KEYMAP | <ul style="list-style-type: none"> ● Crosswalk flashers when people are using at the west end near I-90 entrance ● Permanent restroom at west end would be great ● Remove the painted mural at the I-90 exit at West Mercer, Awful ● I love the new mural! Restful after coming from the city ● Off-leash in lid park is great – keep it that way! Fence off off-leash to keep separate from trail ● Avoid herbicides, compost trimmings, light pathways, use solar panels, and include multi-lingual signage ● Add walking trail to "wooded" section (not a bike path – a "rustic walking path") ● Walking along natural paths ● Keep I-90 bike trail open for bike commuters ● Keep plant height less than 5' ● Be sustainable ● Updates on process on website, etc. as a YouTube video ● Signs in parks/landscape in multiple languages ● Do a story map for Aubrey Davis Park (like they did for sculpture gallery) ● Survey too many questions |
| ACTIVITIES | <p>Gathering (1 dot)</p> <ul style="list-style-type: none"> ● (One dot on mound photo) |

| | |
|--------------------------|---|
| | <p>Family Activities (2 dots)</p> <ul style="list-style-type: none"> ● (One dot on artful sandbox photo) ● (One dot on playground photo) <p>Courts (1 dot)</p> <ul style="list-style-type: none"> ● (One dot on court photo that says "Tennis Courts") ● Tennis courts have many cracks that <u>need</u> repair ● Tennis court nets are broken ● Court is cracked – fix cracks <p>Other/What else?</p> <ul style="list-style-type: none"> ● Off-leash dogs need to be in fenced area please ● I like things just as they are – no wider paths |
| PROGRAMMING Segment 1 | <ul style="list-style-type: none"> ● Passive recreation such as fishing at the water access point |
| PROGRAMMING Segment 2 | <p>Area C</p> <ul style="list-style-type: none"> ● (Sports dot on field) ● (Gathering area dot on field) ● (Family activities dot on field) ● Trees way overgrown. Can't see beautiful views that the island is so lucky to have ● Trees are overgrown onto trail on southeast side of park (x2) ● I lead group bicycle rides and I'd love to start from the Segment 2 parking lot but the time limit is 2 hours; typical bike ride is 4 hours <p>Trail</p> <ul style="list-style-type: none"> ● Trail 6' wide only through park ● No change in width of path – wider and faster bikes are more dangerous to other users ● Bikers zoom so fast they almost hit pedestrians, especially the elderly. Need separate bike lanes ● Trail 10' wide ● Need fence between dog areas and trail; poop bag dispenser and garbage can and signage at voice control <p>Area B</p> <ul style="list-style-type: none"> ● (Sports dot in Area B) |

- (Gathering dot south of playground in Area B)
- Keep Area B as a family/open area

Tennis Court Area

- (Sports dot in tennis court area)
- (Family activities dot in tennis court area) (x2)
- Area by the tennis courts has one of the best views in the park – how can we make this available to more people
- Need sign-up sheet for courts – too many people waiting/fighting for court time

Other

- Survey didn't consider "no change" option as if nothing is there now
- After activities on fields – immediate maintenance repairs to divets/torn up areas, etc.
- No field lights and no astroturf please, thanks
- All lighting should point down and not shine into neighborhoods or the eyes of migrating birds
- Stacks could be amazing pieces of artwork
- Basketball court area could be a stunning piece of art
- Keep park as is and no reason for any change and can't afford it
- In WSDOT maintenance area: more sport courts in addition tennis, pickleball, table tennis, etc.

PROGRAMMING Segment 3

Other

- Ivy is killing the trees; please remove it
- No more rubber matting and no more plastic [in playground]; it should be more natural
- Major trail to town center going east on south side of Area A
- Transient drug use in forested area on the west side of I-90 and N Mercer Way intersection
- First hill does not have sidewalks for the walkers; provide walkers with a trail loop around the perimeter of the Aubrey Davis lid park
- Bikes should use street not park – park is for kids and residents
- Bikes belong on streets NOT in park
- Cars come too fast going north/south on 72nd
- Need crosswalk connecting top of ramp across 72nd
- Vehicular conflict zone at 72nd and 24th – possible to route cars onto 71st: low visibility due to grades; pedestrian crosswalk not visible at top of hill

| | |
|--------------------------|--|
| | <p>Ramp to 72nd Ave SE</p> <ul style="list-style-type: none"> ● Opportunity for artwork along ramp wall ● Ramp is first opportunity for parks to get off street – keep it <p>Restroom Conflict Zone</p> <ul style="list-style-type: none"> ● (Gathering area dot on field) ● Loop around field used as trike trail – conflict with faster bikes around restroom ● Conflicts with bicyclists need to watch out for dogs here. Blind spot – they are going too fast ● Re-route bikes behind restroom |
| PROGRAMMING Segment 4 | <p>Opportunity Area F</p> <ul style="list-style-type: none"> ● (Gathering area dot here) ● Less grass; sustainable groundcover and more edible plants <p>76th Ave Overpass</p> <ul style="list-style-type: none"> ● Irrigation leaking/problems in 76th Ave overpass ● Realign trail here for safety <p>Other</p> <ul style="list-style-type: none"> ● (Gathering area dot at cul-de-sac on Sunset Highway) ● Foot, bike trail to 77th Ave SE (From bike path along N Mercer Connector) ● Madrona (cropped) creating sight distance issue at intersection of N Mercer Way and 76th ● Boundary check just north of this tree ● Trees that have been cut because of disease do not have to be replaced. Use money for more trees needing replacement elsewhere ● Maintain trees – all cedars dying along corridor ● Drug use and dumping at NE corner of 77th Ave SE and N Mercer Way ● Terrace trail along contour and open understory (reduce homeless) |
| PROGRAMMING Segment 5 | <p>Other</p> <ul style="list-style-type: none"> ● Love to see more public art ● Bikes in bike only lane ● Keep bikes on road (for bikes going east on SE 24th street) ● Yes, bikes on road <p>Sound Transit Parking Area</p> <ul style="list-style-type: none"> ● Need place for car share and drivers to drop-off/pick up riders |

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| | <ul style="list-style-type: none"> ● Different colors between peds and cyclists would be better – can't see symbols from a far distance ● Hate green, walkers go on full width ● Need a bus rider drop-off/pick up area ● Bicycle crossing – in blind spot of left turn vehicle – hazard ● Please add yield sign for cyclists travelling east on N Mercer Way to yield to cars turning left (North) at 81st SE <p>New Sound Transit Station</p> <ul style="list-style-type: none"> ● West end: Too much transparency and doesn't meet original MOA & intent ● East end: Keep lots of green and add more trees at entry ● East end: Minimal change – keep just as green and replace landscape <p>Sculpture Garden</p> <ul style="list-style-type: none"> ● Mitigate the freeway noise and sight of new 77th Ave light rail station by re-planting evergreen trees in the open space where trees have died behind the bench ● Incorporate seating artistically along path ● I love artwork in the community |
| PROGRAMMING Segment 6 | <p>Opportunity Area G</p> <ul style="list-style-type: none"> ● Better walking space between SE 27th Street and entrance to Opportunity Area G ● Entrance to Luther Burbank Park from city ● Entrance to Luther Burbank Park from town center ● (Gathering area dot in NE corner) ● Luther Burbank outreach center <p>Opportunity Area H</p> <ul style="list-style-type: none"> ● "The Lookout" – overhang area to the north ● (Gathering area dot in north section) ● Name and sign as "Luther Lid" ● Keep open/natural area <p>Other</p> <ul style="list-style-type: none"> ● Keep the bikes out of the park ● No bikes in park ● Keep bikes on the I-90 bike trail ● No improvements and activities wanted in this part of the park |

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| <p>PROGRAMMING Segment 7</p> | <p>Other</p> <ul style="list-style-type: none"> ● Connect Shorewood and Upper Luther Burbank Park via trail on south side of I-90 ● We like the park as it is! No changes! Use \$ for maintenance not new construction ● Complete east portion of Upper Luther trail up into Shorewood – now only ending in the ravine ● Existing trails to ravine in cul-de-sac in Shorewood neighborhood ● No improvements or activities wanted in this part of the park ● Put some water in the planters along the Shorewood drive overpass <p>Bikes</p> <ul style="list-style-type: none"> ● Bikes here along N Mercer Way ● Put bikes on road (widen) and 6' trail only ● The city spent a lot of money making roads safe for bikers; keep them on roads |
| <p>PROGRAMMING Segment 8</p> | <p>Boat launch</p> <ul style="list-style-type: none"> ● Parcel by the boat launch should be acquired ● Restroom in area just to the right of the boat launch <p>Opportunity Area I</p> <ul style="list-style-type: none"> ● (Gathering area dot here) ● (Family activities dot here) ● There are no public playgrounds in this quadrant of the island – would be great to see a small playground for kids (x2) ● Access area for gatherings, bike rides/events (gather and park here) <p>Opportunity Area J</p> <ul style="list-style-type: none"> ● (Family activities dot here) (x2) ● Protect bike/pedestrian access to park along E Mercer Way <p>Other</p> <ul style="list-style-type: none"> ● (Family Activities dot in forested area behind Bright Horizons) ● Connector Trail to access park from Mercer Wood neighborhoods ● Pedestrian scale lighting to improve commute and improve safety ● Fix root bumps & cracks in asphalt of bike trail |
| <p>TRAILS Lidded Park</p> | <ul style="list-style-type: none"> ● Add restroom on West side ● Trees along west side of I-90 trail need to be cut – are way overgrown |

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| | <ul style="list-style-type: none"> ● Bikers speed down this hill [west side of I-90 trail] ● Loop walking trail along north side of lidded park ● Make it lighted (x2) ● Bikers speed on trail down the hill east of W Mercer Way – not safe for walkers ● Entrance to park needs barricade between 66th Ave parking lot and trail ● This sidewalk is bad – easy to fall down ● Bikes speed around corner approaching 72nd overpass ● Improve this area only at trail and overpass intersection ● Tennis courts: fix cracks, sign-up sheet (always people) ● Fix courts ● Sidewalk access to park on the north side of park at 72nd ● No sidewalks in neighborhoods south of park – need a place to walk! ● Love having sculpture in this park ● Bikes & off-leash dogs – conflicts ● Fencing or designated off-leash area ● Keep trail width as-is (x3) ● Safer roads for hwy bike users – trail for everyone else on other side |
| <p>TRAILS Town Center</p> | <ul style="list-style-type: none"> ● Move cyclists to road northbound (right lane – shared with bus) ● How will bikes get from bike trail to bike parking on 77th at light rail? Consider ped/bike/bridge to facilitate safe crossing ● Needs trees at sculpture garden ● Accommodate bikes through I/S of 81st and the PiR. Explore bike signals. High conflict areas that need fix on N Mercer Way ● Right hook issues at park & ride entry; signs, bike signal ● Need WF signs; bikes at park and ride ● Very tight spot at Island Crest Way apartments – not much room ● Sign alternate routes for bikes to avoid pinch point ● Tully's site - How will we get # cars in and out of here? ● Intersection with all extra cars at Sunset Hwy and 77th ● Lots of congestion on city streets on SE 27th |
| <p>TRAILS East Segment</p> | <ul style="list-style-type: none"> ● The wider you make it the more it'll attract users. ● Concerns with congestion at intersection from current land uses ● Cross Section D – No shoulder, but separate paved and gravel path where possible ● Safety warning (balls) ● Using road more because of bumps (roots) in trail ● Accesses to trail are dangerous ● Move high-speed cyclists to North Mercer Way |

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| | <ul style="list-style-type: none"> ● Faster routes to trail are dangerous |
| TRAIL EXPERIENCE | <ul style="list-style-type: none"> ● Trail signage – where am I supposed to be? Bike signal at Park & Ride ● Wayfinding ● Map of the park – mileage marker ● Save the trees – mature ones ● Save the trees ● Preserve the natural feel of trail ● City to provide specific location of conflicts and accident data ● Need to get independent counts for bicyclists on existing trail <p>Trail Design</p> <ul style="list-style-type: none"> ● 14' trail probably a good idea - more leisurely, comfortable, safer. ● Keep to the 10' provide a second separated pedestrian trail ● Narrow trails promote slower speeds & safety! ● Delineated trail; separate trail for cyclists and peds ● Please do not widen trail! ● On 14' bike path, be sure to have a center stripe ● Please don't allow paths to be widened ● Very opposed to widening any portions to 14' (x7) ● Separate bike lanes! Cyclists move way too fast. All weave through pedestrians (esp. elderly). Very unsafe. ● Bikes belong on the roads not on park trails- dangerous for everyone else! ● Lighting on trails ● Separate bikes & peds, more "no dog" signs, voice control is a problem ● Keep to the 10'; provide a second separated ped trail ● 2 paved trails to separate wheels from peds (See Myrtle Edwards Park) – by C. Ridolfi ● Separate paved & gravel trails where possible; through cyclists should be on the road ● Dogs on-leash ● Only widen portions to address conflicts <p>Trail Location</p> <ul style="list-style-type: none"> ● Trail to Upper Luther Burbank needed ● Keep the bikes out of the park ● Bikes use streets not park paths ● The paved I-90 bike trail is important for bike traffic and off-island including bike commuters and recreational riders. |

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| | <p>Trail Users</p> <ul style="list-style-type: none"> ● Safety for pedestrians getting to & on the trail ● No electric bikes on pathways ● Widen trail, slow down cyclists! ● The 1% experienced/confident bikers do not share the trail. Too fast for conditions! |
| TRAIL EXPERIENCE | <p>Wheels</p> <ul style="list-style-type: none"> ● 3 dots – faster through cyclist ● 5 dots – fitness cyclist/group riders ● 0 dots – e-bike cyclist ● 6 dots – casual through cyclist ● 4 dots – casual cyclist ● 2 dots – first/last milers ● 3 dots – family cyclist ● 1 dot – alternative wheels ● 0 dots – assisted mobility <p>Feet (4 dots in general)</p> <ul style="list-style-type: none"> ● 8 dots – jogger/runner ● 4 dots – fitness pedestrian ● 9 dots – casual pedestrian ● 1 dot – groups walking ● 2 dots – walking with strollers ● 3 dots – elderly and senior walkers ● 1 dot – visually impaired pedestrians <p>Other:</p> <ul style="list-style-type: none"> ● Please consider bicycle commuters (should be added as a category) ● This isn't Mercer Island ● How about the casual pedestrian? ● Scooter share – lime scooters ● Note no bikes belong on the roads! ● Elderly – this is a good one; putting bicyclists on the same path as the elderly; on your left ● Who is supposed to be on the left (x2) |

Mercer Island Aubrey Davis Park

DRAFT October 4, 2019 | Survey #3 - Analysis

Survey Overview

The third Mercer Island Aubrey Davis Park Survey was designed to complement the third open house on September 23rd, 2019. The survey asked for targeted feedback on three design and implementation options for the park. It was passed out on comment cards at the in-person open house, and presented as an online survey. The survey was open from September 23rd to 30th, 2019. It was not a statistically valid survey.

The survey used the ESRI StoryMap platform which presents a graphic narrative with a link to survey questions at the end. The narrative contained four major sections:

- Vegetation Management
- Trail Improvements
- Amenity Improvements
- Arts and Placemaking

After learning about these parts of the Aubrey Davis Park Master Plan, open house attendees and online survey respondents were presented with three proposed actions and asked three questions to give targeted feedback on those actions. The questions were multiple choice or multiple answer to simplify and focus feedback. The three options focused on the following subject areas:

- The Ballfield Conflict Zone
- Soft-Surface Trail Alignment
- Water Conservation

All exhibits below present the combined results of the comment cards filled out at the in-person open house on September 23rd, as well as all online survey responses. There were 33 unique respondents for the online survey, and 41 comment cards received at the in-person open house. Some questions were skipped by respondents, and some questions were multiple answer, allowing respondents to choose more than one response.

BALLFIELD CONFLICT ZONE

The area immediately adjacent to Feroglia Fields in the Central Lid Park is one of the most congested areas of Aubrey Davis Park. Two design solutions are proposed to improve safety long term.

What is your preferred approach? (pick one)

1. Option 1: Multi-Modal Plaza



2. Option 2: Trail Behind Restroom



3. Option 3: Neither

Combined Results from Open House and Online Survey:

| | # | % |
|--|-----------|-----|
| Option 1: Multi-Modal Plaza | 14 | 21% |
| Option 2: Trail Behind Restroom | 41 | 61% |
| Option 3: Neither | 12 | 18% |
| TOTAL: | 67 | |

- Over 60% of respondents preferred Option 2: Trail Behind Restroom
- 21% chose the Multi-Modal Plaza option, and 18% preferred neither option.

SOFT-SURFACE TRAIL ALIGNMENT

Two soft-surface pedestrian trails are proposed to improve access and provide separation between pedestrians and cyclists.

Which proposed pedestrian paths do you like? (pick all that apply)

1. Option 1: Pedestrian path to the tennis courts



2. Option 2: Pedestrian path along trail



3. Option 3: Neither

Combined Results from Open House and Online Survey:

| | # | % |
|---|-----------|-----|
| Option 1: Pedestrian Path to Tennis Courts | 29 | 35% |
| Option 2: Pedestrian Path Along Trail | 39 | 46% |
| Option 3: Neither | 16 | 19% |
| TOTAL: | 84 | |

- Nearly half (46%) of respondents chose Option 2: Pedestrian Path Along Trail. However, there was fairly strong support for Option 1: Pedestrian Path to the Tennis Courts, with 35% of respondents choosing that option.
- 19% responded that they did not prefer either option.
- This question was multiple answer question, allowing respondents to pick more than one answer.

WATER CONSERVATION

Water use currently accounts for almost one third of the park's total operating expenditures. Below are three alternatives to consider for reducing annual water usage and cost in selected areas.

What is your preferred water conservation approach? (pick one)

1. Option 1: Brown out open lawn areas (non-athletic) in summer

- Low cost, low water usage
- Low maintenance
- Low habitat value



2. Option 2: Install lower maintenance meadows

- Medium cost, some initial water usage
- Moderate maintenance
- High habitat value



3. Option 3: Keep watering existing lawn areas (current practice)

- High cost, high water usage
- Higher maintenance, higher performance
- Low habitat value



Combined Results from Open House and Online Survey:

| | # | % |
|--|----|-----|
| Option 1: Brown out open lawn areas in Summer | 22 | 29% |
| Option 2: Install lower maintenance meadows | 36 | 47% |
| Option 3: Keep watering existing lawn areas | 19 | 25% |

TOTAL: 77

- Nearly half (47%) of respondents preferred Option 2: Install lower maintenance meadows.
- 29% of respondents preferred Option 1: Brown out open lawn areas (non-athletic) in summer, while one quarter (25%) preferred the current practice of watering existing lawn areas.

Aubrey Davis Park Master Plan

Open House #3 | MEETING NOTES

September 23, 2019

Each participant at the Open House received a survey card and three sticky dots when they signed in. They were encouraged to put the dots on their top three favorite projects. They were also encouraged to write any comments they had on sticky notes and place them on the 11 boards that encircled the room.

| Plan Area | Notes |
|--------------------------|---|
| Process to Date Board | <p>Pop-Up Survey</p> <ul style="list-style-type: none"> ● 6 dots indicate the top activities include walking or biking on the trail and enjoying the landscape and scenery ● 2 dots indicate the guiding principles for the master planning process are to reduce conflicts between trail users and other pedestrians and to improve existing amenities <p>Open House #1 Survey</p> <ul style="list-style-type: none"> ● 12 dots on retaining the natural park character ● 3 dots on allowing for a variety of trail experiences |
| Planting Character Board | <p>Planting Character – Northwest Feel</p> <ul style="list-style-type: none"> ● 2 dots on Northwest feel (ferns) ● “No ivy, plus you can see, there is a lot of weeding involved. You don’t have the money! And no more ivy” ● “Like northwest feel, but low on water usage. Like browning out open lawns (non-athletic areas)” <p>Planting Character - Ornamental</p> <ul style="list-style-type: none"> ● 5 dots on ornamental feel (2 on purple flowers, 3 on groomed lawn/garden) ● On ornamental: “this is just too much foo foo, less is better.” <p>Planting Character - Sensory</p> <ul style="list-style-type: none"> ● 3 dots on sensory (wind-chime picture) ● “Who has the time to maintain these gardens? Give details.” <p>Water Conservation Options</p> <p>Option 1: Brown out open lawn areas in summer</p> <ul style="list-style-type: none"> ● One dot ● “No brown in high visual areas.” |

Option 2: Lower maintenance meadows

- 4 dots
- "I have no problem with plain old grass."

Trail Improvements

Ballfield Conflict Zone

Option 1: Multi-Modal Plaza

- 3 dots on traffic calming measures
- 3 dots on ghost sketch
- "Every time you want to widen the path and add gravel, trees need to be removed. Please leave natural."
- "Bad – as walkers walk across trail, great danger to all users"
- "Like traffic calming element"

Option 2: Trail Behind Restroom

- 12 dots on Option 2
- Drawing: signage indicating bus vs walk paths
- "Separate pedestrian trail is great! Soft shoulder is important for runners knees, not just peds."
- "Traffic circle is great! But here (arrow pointing at path behind bathroom) bikes will take the shortest path rendering the bike detour useless. Slow their speeds down. Widen the trail to 14' standard though."

Soft Surface Trails

- 3 dots total: 2 on connection to tennis courts, 1 on pedestrian path along trail
- "Make new pedestrian path paved!"
- "No disabled person has complained about the paths."
- "Neither. Don't like either. This has been talked about a lot. It is like they are not listening to us."

General Comments

- "Please put your efforts into removing all the ivy first. Then come in with making the area look nice."
- "Not removing the ivy. A large section fell down, that could have killed someone!"
- "Remember you have no money! You need to look at the maintenance cost instead of coming up with projects"
- "Regional best practices and soon-to-be adopted national trail standards underscore that 14' trails give all users room to safely navigate. That's 14 paved feet, plus soft shoulders!"
- "Please just leave as is, just remove the ivy please. Adding plants just leads to more



work for staff. They have enough work to do as is."

Preferred Trail Improvements: West Half

(A) W Mercer Way Crossing

- One dot (existing crossing improvements)
- Pointing at intersection near ramp and existing parking - "needs pedestrian safety" and "Encourage alternate routes with wayfinding."
- "High visibility bollards needed throughout the city"

(B) ADA Access to Playground and Basketball Courts

- One dot
- "All islanders pay for the park. All islanders should have access." (one dot)
- "Too expensive"

(C) ADA Access to tennis Courts and Proposed Off-Leash Area

- One dot
- "Unnecessary. Use trail to east."

Optional – Soft Surface Trail Connection

- One dot
- "Separating bikes from walkers is a great idea" (one dot)

(E) Restroom Conflict Zone

- One dot
- "Should relocate trail behind restroom – Separation is a great tool to mitigate and prevent user conflicts."
- "Make sure Wash DOT can provide transit for bikes and peds re-order priorities. Ped – Bike – Car."
- "Relocate bike trail behind and west of bathrooms, limit bike-walker interaction. That section is 'fast' downhill for bikes - move them off current trail."

(H) Town Center

- "Ped safety at all intersections."
- "Make sure new round-about for sound transit is safe and user friendly for cyclists." (one dot)
- "NO ROUNDABOUT, buses will be blocking the crosswalk going W to East and blocking the roundabout going East to West"
- At 24th and 81st intersection – "Prefer stop for vehicles. 4-way stop" and "if trail, needs a lot of work for ped safety."
- At N Mercer Way and parking lot intersection "Pedestrian safety here."
- "Cyclist concern: corner of 81st and north Mercer Way, crosswalk should be painted green so cars know cyclists cross there"
- At 81st and N Mercer Way "there does not need to be any traffic calming. The bike trail should go behind to 24th St. The sidewalk cannot be widened."
- "Cannot widen here. T-Mobile have all their equipment in the side panel."

- "Need to widen Island Crest Way between N Mercer Way and 28th St. How about a Lid?"

(F) Stair Connection & ADA Path from Luther Lid to N Mercer Way

(G) Stair Connection & Path from Upper Luther Lid to 84th Ave SE

- "F&G are needed together to finally reconnect Luther Burbank's split halves."
- "Important to have safety features that SLOW bikes down on shared use trails" (one dot)
- "Add connection between MICA and crossing Island Crest to Luther Burbank Lid."
- "Need this trail from Luther Lid to Luther Burbank Park. High Priority!" (one dot)
- "Island Crest Way and North Mercer Way intersection is very dangerous. Need stop sign vs yield sign." (one dot)

Preferred Trail Improvements: East Half

(G) Stair Connection & Path from Upper Luther Lid to 84th Ave SE

- 6 dots
 - "Very excited about the stairs from 84th to Upper Luther Lid."
- (K) Upper Luther Ravine Trail
- 5 dots
 - "Love the Upper Luther trail – allows a trail from town center to City Hall"
 - "The look of grass on either side of the trail is nice. And please think of the trees before you look at each project."
 - "Please remove the gravel. It is a mess of adding to the danger. Plus it causes a lot of work for staff to maintain."
 - "K would connect the existing trail from 36th to Shorewood to 84th."

General

- "Where the trail crossed Shorewood Dr. east bound – the sight line for cyclists is poor and cars are descending Shorewood Drive. Not good."
- "Existing Ped trail show potential for connectivity"
- "East segment – Pedestrians and slow cyclists on path. Faster bikes on NMW. Please encourage this."
- "G. I like the connection from 84th to the lid. K. Also the connection to Shorewood"
- "Please build the trail to national standards! Not thinner"
- "East end of trail is too narrow. Electric bikes popularity will overwhelm this sections capacity in a few years. It's tight now with bikes in opposite directions."
- One dot on wayfinding, ADA access, and/or traffic calming for 100th Avenue intersection

Preferred Trail Improvements: General

General

- "Vegetation on water side of wall off W Mercer Way has never been maintained"
- "Force cyclists to use 24th to 81st to avoid high speed conflicts in front of park and ride"
- "As cyclist – I ride on N Mercer Way."
- One dot for traffic calming measures
- One dot for bollards
- "No new bollards."
- "Alternatives help so ex. width OK."
- "To allow future use, trail width should right size trail width 14' national best practice." (One dot)
- "Yup! Heavy congestion and tight path here! Please adhere to the 14' Standard!"
- Along I-90 "More width here GOOD cyclists fast on E end so not comfortable as pedestrian"
- "Ideally, make space for pedestrians! Leave space for trail expansion one day! Don't plant trees too close to trail, and leave right of way."
- "I support the widest trails possible and consistent with best design standards for multi-mode use." (3 dots)
- "Consider separate trails for wheels and peds (like and Myrtle Edwards Park) 2 surfaces, separated."
- "If bike trail doesn't work for faster commuters and slower recreational users, fewer people will use it and multi-modal option for reducing carbon won't work."

Trail Through East Half Section

- "Use colored stripes to separate wheels from ped areas (fast vs slow) trail"
- "Want 14' with 2' hard pack buffer. No loose gravel – dangerous."
- "Adopt new ASHTO 14 ft wide trail – wider better for all transportation modes" (4 dots)
- "Separate bikes and pedestrian (unpredictable, do not respond to bells)"
- "This is a heavily used trail! 14' paved trail is safer now and into the future. Let's plan now for growth so that in 2023 when light rail opens we're not hurting ☺"

Preferred Park Improvements: West Half

General

- At western end of Lid Park "Need restroom in this area" and "Put restroom across the street by parking lot"
- (B) New Restroom Building
 - 4 dots
- (E) Dog Off-Leash Area
 - One dot
 - "Dog parks also kills grass."



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| | <ul style="list-style-type: none"> ● " Dog owners are very good at self-management of their dogs in the park wonderful to have open access to the park makes it so inviting." ● " Don't fence this in." (one dot) ● " Dogs off leash can be scary if in park areas and mobility impaired for others." ● " Bad area for a dog park." ● " It will be disused if you fence off. It would change the feel." ● " How do you enforce off-leash vs on leash areas?" <p>(F) ADA Seating and Access</p> <ul style="list-style-type: none"> ● 2 dots for ADA seating and access to playground – one dot at each proposed location <p>(H) Future Commuter Parking</p> <ul style="list-style-type: none"> ● One dot ● " No parking here!" ● " This is not even a part of the Aubrey Davis Trail." ● " Bad idea to pave this." ● " I agree, very bad idea to pave this." ● " Please clean up the contamination and leave the grass. Why did you put this in here?" |
| <p>Preferred Park Improvements: East Half</p> | <p>General</p> <ul style="list-style-type: none"> ● On shoreline restoration " Please don't start interfering. The next minute, you will want to develop it. Thanks." (one dot) ● " Is it possible to create a pedestrian path on the south-side of I-90 from NMW to the TC?" ● " Please consider adding a crosswalk (ideally with flashing lights" to help pedestrians cross SE 36th at N. Mercer Way." <p>(K) Improved Shoreline Access</p> <ul style="list-style-type: none"> ● One dot |
| <p>Place-making Opportunities</p> | <p>General</p> <ul style="list-style-type: none"> ● One dot on the draft vision ● One dot on cultivate an art-inspired environment of the core tenets. ● In town center area " Make a notable gateway space with iconic artwork!" ● " Art for pedestrian safety" ● In town center " Would like this section under a lid" ● " Make art experiences congruent to specific differences in location of park" <ul style="list-style-type: none"> ● Interactive art installations - 6 dots |

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| | <ul style="list-style-type: none"> ● Community connection through art inspired spaces – 3 dots ● Engage with the outdoors - 2 dots ● Explore and discover - 2 dots ● Different experiences and different seasons - 2 dots ● Create ambience an accessibility - 1 dot |
| <p>Vegetation Management</p> | <ul style="list-style-type: none"> ● Intensive soil amendment and replanting – 1 (one) dot ● Infill planting areas - 3 dots <p>General</p> <ul style="list-style-type: none"> ● “ Meadows on the lid” towards Luther Burbank Park ● “ If it looks like the meadow along 86th Ave. in Luther Burbank then no meadow. Think of the maintenance.” ● “ Not infill at IC Way” ● “ Keep green” (agree) at IC Way ● “ Like blue areas to also block views to freeway” ● “ Need to reduce bunny habitat” ● “ Like benches and seating along trail” ● “ Like all the conservation measures” ● “ Trim and lower trees to improve better views west toward Olympic Mountains and East to Cascade snow-capped mountains” ● “ Like having hillside green to sit on” (agree) on west side of West Mercer Way ● “ Green at picnic areas” (agree) on east side of West Mercer Way ● “ Green-space important at this intersection” near town-center ● “ Keep green” (agree) at town-center intersection ● One dot on lawn at East Mercer St and I-90 ● “ Ok with alternative here” at East Mercer St and I-90 |
| <p>Comment Cards</p> | <p>Responses Received: 41</p> <p>Question 1: Ballfield Conflict Zone preferred approach</p> <ul style="list-style-type: none"> ● Option 1 (Multimodal Plaza): 6 votes ● Option 2 (Trail behind restroom): 26 votes ● Neither: 5 votes <p>Question 2: Soft Surface Trail Alignment</p> <ul style="list-style-type: none"> ● Option 1 (Pedestrian path to tennis courts): 17 votes ● Option 2 (Pedestrian Path along trail): 25 votes ● Neither: 6 votes |

Question 3: Water Conservation preferred approach

- Option 1 (Brown out open areas): 13 votes
- Option 2 (Lower Maintenance Meadows): 21 votes
- Option 3 (Keep existing lawn): 10

Additional Comments:

- Multi-modal plaza "with bike-calming"
- "Separate areas to help with congestion," and "safer to separate users. Walk vs. ride." for trail behind restroom
- "ADA soft surface" for path to tennis courts
- "Switchbacks encourage shortcuts" re: pedestrian path to tennis courts
- Meadow "but not near Island Crest Way"
- Meadow is "attractive with native vegetation and no on-going water needed"
- "Mix [of meadow and lawn] – keep areas green"
- Lawn "at Island Crest Way"

Revised Aubrey Davis Park Master Plan

List of Revisions

Updated: November 12, 2019

N.B. – this list details revisions that represent substantive changes. Changes to text are reflected in the graphic portion of the plan as well. Other revisions for clarity and readability are show in red in the plan but are not listed here.

| # | Section | Revision process or purpose | Revision |
|---|--|---|---|
| | Vegetation Management, Reduced Water Use Strategies, Page 37 | Recommended by staff, supported by Council at 11/4 Study Session, with additional clarification based on councilmember questions. | Added: “Both of these options will be implemented on a limited basis as pilot projects, locations to be determined by the city. Smaller areas (approximately a ¼ acre) that are accessible and conducive to public engagement will be selected, while high usage areas, such as places where people run their dogs, would likely not be impacted. To identify these pilot projects to the public, signage informing the intent and potential cost savings of each treatment will be displayed. The public will be able to comment on these alternatives and inform whether these strategies will be more widely deployed. Replacing soils and revising the planting palette for more drought tolerant species is also part of the program and will be used as a priority treatment where applicable.” |
| | Vegetation Management, Landscape Character, Page 37 | Recommended by staff, supported by Council at 11/4 Study Session | Added: “The Northwest Feel character was selected as the primary feel for the park, with Ornamental and Sensory palettes included for specific purposes such as accent plantings and art/placemaking opportunities. These would likely be part of an individual project that would be subject to public input, and feedback on the landscaping would be solicited prior to implementation.” |
| | Trail Improvements, Page 39 | Added for clarity by staff based on councilmember questions and comments. | Added: “Trail cross sections only apply to the main Mountains to Sound trail on Mercer Island (shown as a red line on the plan graphics). Secondary trails will remain their existing widths.” |
| | Trail Improvements, Page 39 | Recommended by staff, supported by Council at 11/4 Study Session | Trail cross section diagram titled Trail Section Through the Lid Park: width changed from “current width” (as shown in pre-plan documents) to “12’ Paved Trail” |
| | Trail Improvements, Page 41 | Revision supported by Council at 11/4 Study Session | Section on Optional Informal Soft Surface Trails and all subsequent references removed. |
| | ADA Accessibility, Page 41 | New section added at councilmember request. Similar text was previously included in AB 5622 Exhibit 1. | The ADMP proposes several new trails that would comply with the Americans with Disabilities Act (ADA) through the Architectural Barriers Act (ABA) standards that have been adopted for outdoor recreation facilities. Neither the City nor WSDOT is required to bring existing facilities up to these standards as they currently exist. |

| | | | |
|--|--|--|---|
| | | | Normal maintenance and minor repairs do not prompt ADA compliance. However, work that generates City building permits or WSDOT review will necessitate ADA review. At that time, the extent of the work will determine the requirement for ADA accommodations. For example, a project that continues to keep the facility for the same use at the same location will likely result in ADA improvements limited to that facility only (e.g. changing restroom fixtures with any upgrades to the existing restroom structure, or installing an ADA ramp with ADA accessible playground elements when the playground equipment needs to be replaced). A project that changes the use of the facility or results in a substantially new facility could trigger a more comprehensive set of ADA accommodations, like including accessible routes from designated ADA parking stalls with a redesign of the tennis court or basketball court areas. These types of improvements have been included in the ADMP, but the ADMP does not constitute a comprehensive ADA analysis of accessibility needs in the park. |
| | Trail Lighting, Page 41 | New section added by staff based on councilmember question. Similar text was previously included in AB 5622 Exhibit 1. | Added: "The section of the Mountains to Sound Trail between the Luther Lid and Shorewood Drive is shaded by high retaining walls to the south. It is dark in the winter. Pedestrians use this trail as the most direct route between Shorewood and Town Center. Staff have had requests for lighting that section of trail to improve visibility for both pedestrians and cyclists. While there have been no design concepts discussed for this improvement, the most likely solution would be overhead lights on poles. Bollard lighting is an additional option for wayfinding in the dark, but does not provide adequate illumination for trail users." |
| | New Restroom, Page 45 | Recommended by staff, supported by Council at 11/4 Study Session | Both potential restroom locations are shown. Added: "The graphic to the left shows the approximate areas proposed for the new restrooms in orange." |
| | Dog Off Leash Area, Page 45 | Clarification added by staff based on the recommendation by staff that was supported by Council at 11/4 Study Session | Added: "As such, a dog off-leash area will be further explored in the early design phase with extensive public input." |
| | Arts, Culture and Placemaking, Page 48 | Additional clarification by staff based on questions from councilmembers and input from the Arts Council ad hoc committee on Aubrey Davis Park | Added: "Since its creation, Aubrey Davis Park has been a primary location for community members to engage with arts and culture. (Learn more about this history in Appendix I.) Building on this history, the Mercer Island Arts Council has identified an updated vision for..." |
| | Arts, Culture and | Additional clarification by staff based on questions from councilmembers and | Added: "The city's public art process is guided by existing policies that will be followed to make decisions on future arts and culture projects in the park." |

| | | | |
|--|---|---|--|
| | Placemaking, Page 48 | input from the Arts Council ad hoc committee on Aubrey Davis Park | |
| | Project Implementation, Prioritization, Page 52 | Recommended by staff, supported by Council at 11/4 Study Session | <p>Added safety criteria to highest priority, as follows: “Criteria for prioritization of the projects included in this master plan mirrors the criteria used in the City’s Capital Improvement Program (CIP). The prioritization is as follows:</p> <ol style="list-style-type: none"> 1. Highest Priority – Projects that are critical to be done right away in order to maintain existing infrastructure and projects that address urgent safety issues. Postponing this project would require a higher level of effort and would be at an overall greater cost to the City in future years. 2. Second Level of Priority – If funding is available, it is recommended that this project be done in the biennium. However, it is not critical and could be postponed to a later year if need be. 3. Third Level of Priority – Projects for which there is strong support from the City Council or a group of citizens. However, the project is not critical to the maintenance of the City’s infrastructure. |
| | Project Implementation, Prioritization, Page 52 | Recommended by staff, supported by Council at 11/4 Study Session | <p>Added: “As individual projects included in this plan start moving forward, the following approach will be used to determine the level of project engagement:</p> <ol style="list-style-type: none"> 1. Projects that generally keep the existing character and function of the park do not need additional public input and decisions would be made at the director level once Council has approved the budget for them (e.g. landscape renovation, field drainage projects). 2. Projects that maintain or enhance existing functions (e.g intersection improvements, improved shoreline access, water conservation) would have one round of outreach and input in early design with updates posted on social media and Let’s Talk. Decisions would be made at the City Manager level after Council approves the budget for them. 3. Projects that involve extensive design or are new facilities (e.g. dog off-leash area, restroom conflict zone trail reconfiguration) would provide the public multiple chances for input with a Let’s Talk page and full social media coverage. City Manager would advise Council of project progress and ask for input at critical stages. |
| | Project Implementation, | Clarification added by staff based on questions from councilmembers | A rough order of magnitude cost estimate was generated for Aubrey Davis Park master plan to get an understanding of high level costs associated with proposed |

| | | | |
|--|--------------------------------------|--|--|
| | Cost Estimating and Funding, Page 53 | | improvements. This is a one-time snapshot of rough costs and more detailed cost estimates need to be developed for each individual project during design phase. Including a project in this master plan does not imply that the City will fund that project or give it special priority. Capital projects in the plan will be considered for funding as part of the biennial budget process. The exception to this would be opportunities for external funding that would be dedicated specifically to the park or a specific project in the master plan. Donations or grants could be received outside of the biennial budget process with City Council approval. |
|--|--------------------------------------|--|--|

**CITY OF MERCER ISLAND
RESOLUTION NO. 1571**

**A RESOLUTION OF THE CITY OF MERCER ISLAND, WASHINGTON
ADOPTING THE AUBREY DAVIS PARK MASTER PLAN**

WHEREAS, Aubrey Davis Park was created in the early 1990's as a result of the efforts of many Mercer Island citizens who demanded mitigation for the expansion of Interstate 90; and

WHEREAS, the park extends over 92 acres and contains the Park on the Lid (also known as the Lid Park), the Mountains to Sound Regional Multi-use Trail, the Gretta Hackett Outdoor Sculpture Gallery, and the Mercer Island Boat Launch, as well as all the overpass and lid structures spanning the highway canyon; and

WHEREAS, Aubrey Davis Park is primarily located on state highway right-of-way owned by the Washington State Department of Transportation (WSDOT); and

WHEREAS, the City of Mercer Island (City) maintains the landscaping in Aubrey Davis Park through agreements with WSDOT that reimburse the City for maintaining WSDOT's landscape; and

WHEREAS, the declining landscape and failing infrastructure that are now evident throughout the park, represent capital reinvestment needs that were not anticipated in the maintenance agreements; and

WHEREAS, the City and WSDOT have both expressed the desire to update the agreements to reflect current and future needs for the landscape; and

WHEREAS, in 2018, WSDOT and the City recognized the need to make future planning decisions about the park jointly by executing a Memorandum of Understanding to create a master plan for Aubrey Davis Park; and

WHEREAS, the combined results of Sound Transit's East Link light rail, commuter parking, and transit integration projects are expected to significantly impact Aubrey Davis Park; and

WHEREAS, King County Wastewater Treatment Division's North Mercer Enatai sewer project will remove and replace major sections of the Mountains to Sound Trail; and

WHEREAS, the landscaping, trails, recreational facilities and arts/cultural elements of the Aubrey Davis Park are essential to the character of the Park and are inextricably interconnected; and

WHEREAS, the Mercer Island City Council recognized the need for a master plan to guide the operational and capital needs for the park and appropriated a budget of \$300,000 to develop a master plan for Aubrey Davis Park; and

WHEREAS, WSDOT funded \$100,000 of the master plan costs through a Local Programs Grant and also funded staff time to provide guidance and review throughout the planning process; and

WHEREAS, the City of Mercer Island conducted an extensive public engagement process from August 2018 to November 2019, including four public forums, three public open houses, four public online surveys, two all-island mailings, five City Council study sessions, and maintained the Let's Talk public engagement platform throughout the planning process; and

WHEREAS, the Mercer Island Arts Council developed a vision and four tenets for future arts, culture, and placemaking opportunities in the Aubrey Davis Park; and

WHEREAS, the resulting plan provides conceptual guidance for the operational and capital needs for Aubrey Davis Parks' landscaping, trails, recreational facilities and arts/culture/placemaking elements; and

WHEREAS, the City of Mercer Island Community Planning and Development Department issued a Determination of Non-significance (DNS) after conducting State Environmental Policy Act (SEPA) review of the draft plan; and

WHEREAS, it is in the best interest of the Mercer Island community to have a formal master plan for the stewardship of Aubrey Davis Park going forward;

NOW, THEREFORE, BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF MERCER ISLAND AS FOLLOWS:

Section 1. The City Council adopts the Aubrey Davis Park Master Plan as presented and revised at the November 19, 2019 City Council Regular Meeting and attached as Exhibit A.

Section 2. This resolution shall take effect immediately upon passage.

PASSED BY THE CITY COUNCIL OF THE CITY OF MERCER ISLAND, WASHINGTON AT ITS REGULAR MEETING ON THE 3RD DAY OF DECEMBER 2019.

CITY OF MERCER ISLAND

Debbie Bertlin, Mayor

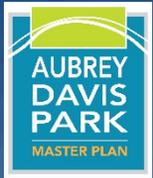
ATTEST:

Deborah A. Estrada, City Clerk



Revised Draft
AUBREY DAVIS PARK
MASTER PLAN

City Council Presentation



1. PUBLIC ENGAGEMENT



Item 8.

1 POP-UP EVENTS
AUGUST/SEPTEMBER 2018

2 PUBLIC FORUMS

| | | | |
|----------------------|--------------------|--------------------|---------------------|
| SEPTEMBER 25TH, 2018 | OCTOBER 25TH, 2018 | NOVEMBER 8TH, 2018 | NOVEMBER 29TH, 2018 |
| #1 | #2 | #3 | #4 |

3 OPEN HOUSE #1
FEBRUARY 28TH, 2019

4 OPEN HOUSE #2
APRIL 23RD, 2019

5 OPEN HOUSE #3
SEPTEMBER 23RD, 2019

2. CITY COUNCIL MEETINGS

Item 8.

1. **November 21, 2017 – Aubrey Davis Park Master Plan (study session)**
2. **October 23, 2018 – King County Sewer Project (study session on trail standards)**
3. **January 15, 2019 – Scoping and Goals (study session)**
4. **July 16, 2019 – Preferred Alternatives (study session)**
5. **October 15, 2019 – Draft ADMP (study session)**
6. **November 4, 2019 – Draft ADMP continued (study session)**
7. **December 3, 2019 – Revised Draft ADMP and adoption resolution (regular business)**

3.

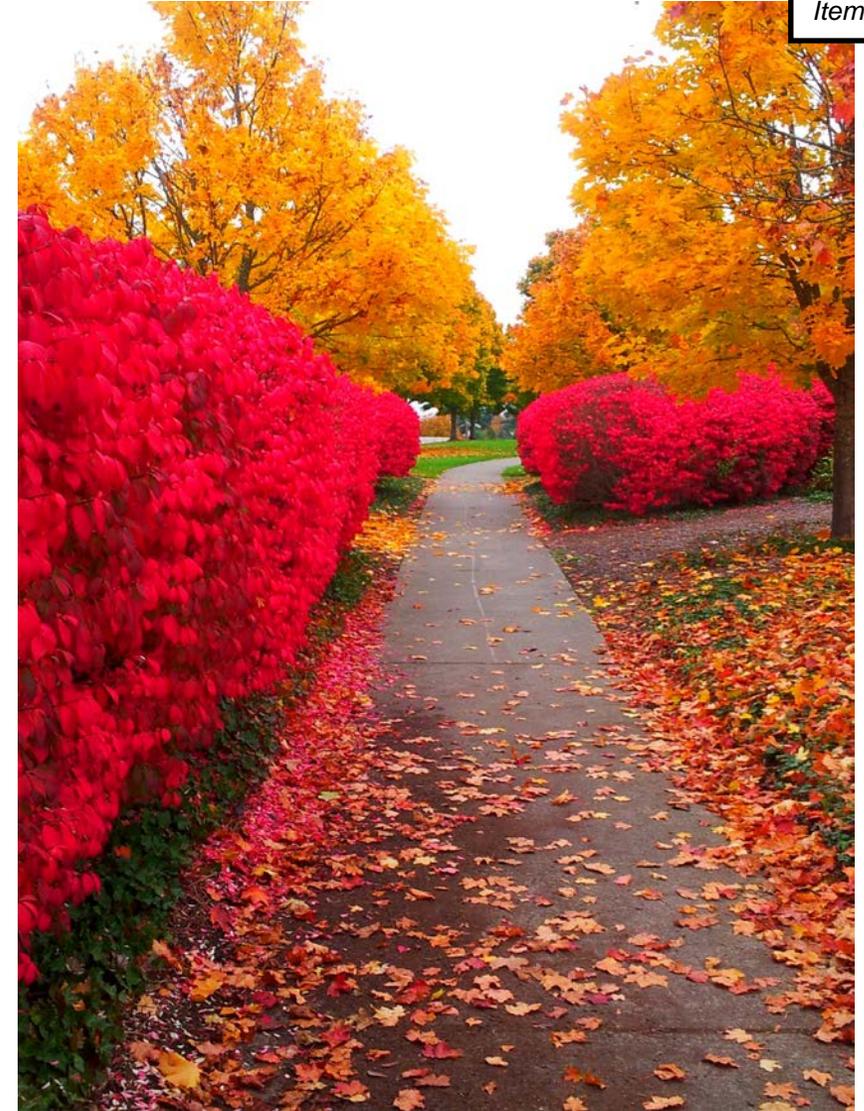
REVISIONS TO THE DRAFT ADMP

Item 8.



Revisions: Vegetation

- **Planting Palette:** Designated the “Northwest Feel” palette as primary. Ornamental and Sensory palettes can be used in specific projects as designed.
- **Water Conservation:** Included the two strategies (meadow and “brown out”) as pilot projects, keep existing practice.



Revisions: Trails

- **Trail Width:** west section width revised from “current” to “12’” for consistency. These sections only apply to the main Mountains to Sound Trail.
- **Optional Soft Surface Trails:** deleted
- **Restroom Conflict Zone:** both options retained. Both will be studied in early design.
- **ADA Requirements:** added a discussion of when they might apply
- **Trail Lighting Project:** added a description of the project

Item 8.



Revisions: Improvements

Item 8.

- **New Restroom:** revised to include both locations. Both will be studied in early design.
- **Dog Off-leash Area:** details are not specified. Scoping with public input will be part of early design.



Revisions: Arts, Culture and Placemaking

Item 8.

- **Historical Context:** added a reference to historical information in the appendix.
- **Existing Policies on Public Art:** added a reference to the City’s existing policies on public art.



Revisions: Project Implementation

- **Cost Updates:** revised to clarify how costs will be updated and that inclusion in the ADMP does not imply funding for a project.
- **Public Engagement:** added criteria for public engagement during the development of future projects.
- **Safety as a Priority:** added safety as a criteria for the highest priority projects.



The ADMP preserves the natural character of the park and anticipates future needs. It provides the City with the documentation needed to pursue the needed renovation of the landscape with WSDOT and other partners.

New development was kept to a minimum. The additions of new trails are identified primarily to meet ADA standards or create linkages with Luther Burbank Park.

The 12' trail profiles for the Mountains to Sound Trail meet standards while providing a visually softer edge in the Lid Park.

The ADMP provides a road map for the future of the park. It maintains the legacy that Mercer Island citizens fought for.

5.

LOOKING AHEAD – DEPARTMENT OF COMMERCE GRANT

Item 8.



558

Looking Ahead – Following Adoption

Item 8.

- **The City Manager and the Parks and Recreation Commission will recommend a scope of work for the Dept. of Commerce Grant (\$500k)**
- **Staff will return to City Council with a scope of work and proposed budget adjustment for the state DOC grant.**



Item 8.

Questions?



BUSINESS OF THE CITY COUNCIL CITY OF MERCER ISLAND

AB 5631
December 3, 2019
Regular Business

AGENDA BILL INFORMATION

| | | |
|----------------------------|--|---|
| TITLE: | AB 5631: 2019 Comprehensive Plan Amendments (2 nd Reading & Adoption) | <input type="checkbox"/> Discussion Only |
| RECOMMENDED ACTION: | Adopt Ordinance No. 19-23 amending the Mercer Island Comprehensive Plan. | <input checked="" type="checkbox"/> Action Needed: <input type="checkbox"/> Motion <input checked="" type="checkbox"/> Ordinance <input type="checkbox"/> Resolution |

| | |
|-------------------------------|---|
| DEPARTMENT OF: | Community Planning and Development Evan Maxim, Director |
| COUNCIL LIAISON: | n/a |
| EXHIBITS: | 1. Draft Ordinance No. 19-23 with Attachment "A" 2. Original text of Planning Commission Amendment No. 2: Climate Change |
| CITY COUNCIL PRIORITY: | 3. Support the Leadership Team's Work Plan |

| | |
|-------------------------------|--------|
| AMOUNT OF EXPENDITURE | \$ n/a |
| AMOUNT BUDGETED | \$ n/a |
| APPROPRIATION REQUIRED | \$ n/a |

SUMMARY

On October 2, 2019, the Planning Commission recommended that the City Council adopt the 2019 Comprehensive Plan amendments. At the first reading on October 15, 2019 (AB 5617), an overview of the docketing process, the community engagement, and the Planning Commission’s recommendation was provided to the City Council. The Council reviewed the proposed amendments and provided direction to Community Planning and Development staff regarding substantive and clarifying changes to the proposed amendments.

Following the review of proposed amendment 2, related to climate change, the City Council directed staff, in consultation with the Planning Commission chair and vice chair, to “up level” the goal and policy language. CPD staff partnered with the city’s Sustainability Manager to prepare revised language for the City Council’s review; the revised language was reviewed by the Planning Commission chair and vice chair on November 4, 2019. The City Council’s direction from October 2, 2019 is reflected in Exhibit 1. The Planning Commission’s recommended Comprehensive Plan amendment related to climate change is included in Exhibit 2, for reference.

RECOMMENDATION

Adopt Ordinance No. 19-23 amending the Mercer Island Comprehensive Plan Land Use, Capital Facilities, and Transportation Elements.

**CITY OF MERCER ISLAND
ORDINANCE NO. 19-23**

**AN ORDINANCE OF THE CITY OF MERCER ISLAND AMENDING THE
MERCER ISLAND COMPREHENSIVE PLAN LAND USE ELEMENT, CAPITAL
FACILITIES ELEMENT, AND TRANSPORTATION ELEMENT; PROVIDING
FOR SEVERABILITY; AND ESTABLISHING AN EFFECTIVE DATE.**

WHEREAS, in compliance with the Washington State Growth Management Act, chapter 36.70A RCW, the City of Mercer Island adopted a Comprehensive Plan in 1994 and has amended the plan on several occasions since that time; and

WHEREAS, in accordance with RCW 36.70A.130, an adopted Comprehensive Plan shall be subject to continuing evaluation and review; and

WHEREAS, the City sought community participation in the 2019 Citizen Comprehensive Plan Amendments by publishing notice through multiple channels, publicizing the opportunity for residents, business owners and interested parties to submit amendments to the Mercer Island Comprehensive Plan; and

WHEREAS, on November 20, 2018, the City Council passed Resolution No. 1554, which established a docket of four proposed Comprehensive Plan amendments for review in 2019; and

WHEREAS, pursuant to RCW 36.70A.130(2)(b), all proposals that were submitted were considered concurrently so the cumulative effect of the various proposals could be ascertained; and

WHEREAS, the City of Mercer Island has met all applicable public notice requirements for said Comprehensive Plan amendments consistent with chapter 19.15 MICC in effect at the time notice was given; and

WHEREAS, the Department of Commerce received notice of Mercer Island's proposed Comprehensive Plan amendments on August 20, 2019; and

WHEREAS, the City of Mercer Island issued SEPA Threshold Determinations (DNS) for the respective amendments on August 30, 2019; and

WHEREAS, the Planning Commission held the required public hearing on October 2, 2019 and recommended approval of the Comprehensive Plan Amendments; and

WHEREAS, the proposed amendments have been reviewed for internal consistency with the comprehensive plan and for consistency with the applicable review criteria in chapter 19.15 MICC; and

WHEREAS, the City Council considered the proposed Comprehensive Plan Amendments on October 15, 2019 (first reading) and December 3, 2019 (second reading);

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF MERCER ISLAND, WASHINGTON, DO ORDAIN AS FOLLOWS:

Section 1. Adoption of Amendments.

The amendments to the Land Use, Capital Facilities, and Transportation Elements as set forth in Attachment "A" to this Ordinance are hereby adopted.

Section 2. Severability.

If any section, sentence, clause, or phrase of this Ordinance or any municipal code section amended hereby should be held to be invalid or unconstitutional by a court of competent jurisdiction, such invalidity or unconstitutionality shall not affect the validity of any other section, sentence, clause or phrase of this Ordinance or the amended code section.

Section 3. Effective Date.

This ordinance shall take effect five days after passage and publication in the official newspaper of the City.

ADOPTED BY THE CITY COUNCIL OF MERCER ISLAND, WASHINGTON AT ITS REGULAR MEETING ON THE 3RD DAY OF DECEMBER 2019.

CITY OF MERCER ISLAND

Debbie Bertlin, Mayor

ATTEST:

APPROVED AS TO FORM:

Deborah A. Estrada, City Clerk

Bio Park, Interim City Attorney

Date of Publication: _____

2019 Comprehensive Plan Amendments

Amendment 1: Remove Specific Town Center subarea designations from the Land Use Element

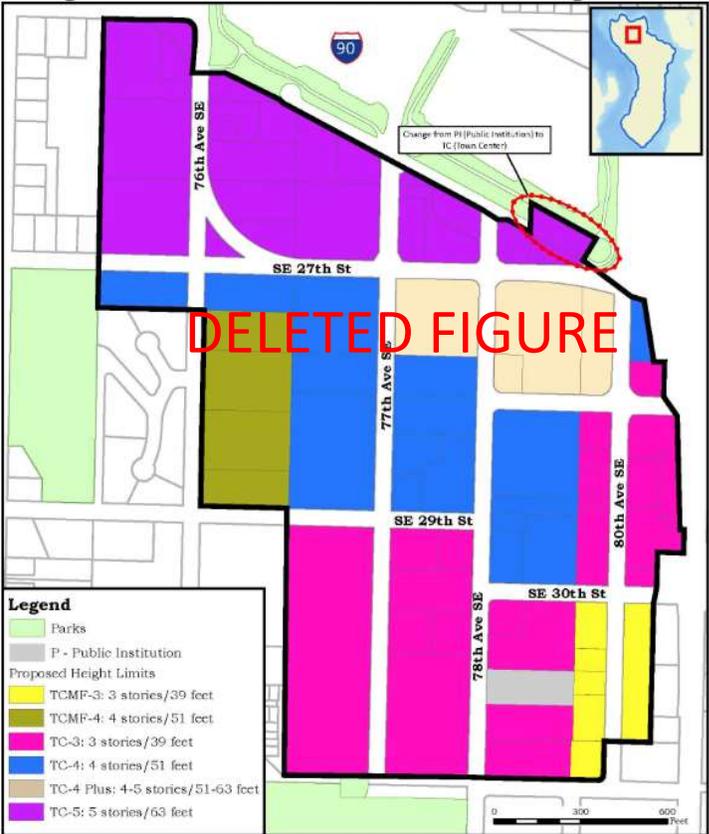
Land Use Element

Land Use Goal 3: Have a mixture of building types, styles and ages that reflects the evolution of the Town Center over time, with human-scaled buildings, varied height, set-backs and step-backs and attractive facades.

3.1 Buildings taller than two stories may be permitted if appropriate public amenities and enhanced design features are provided.

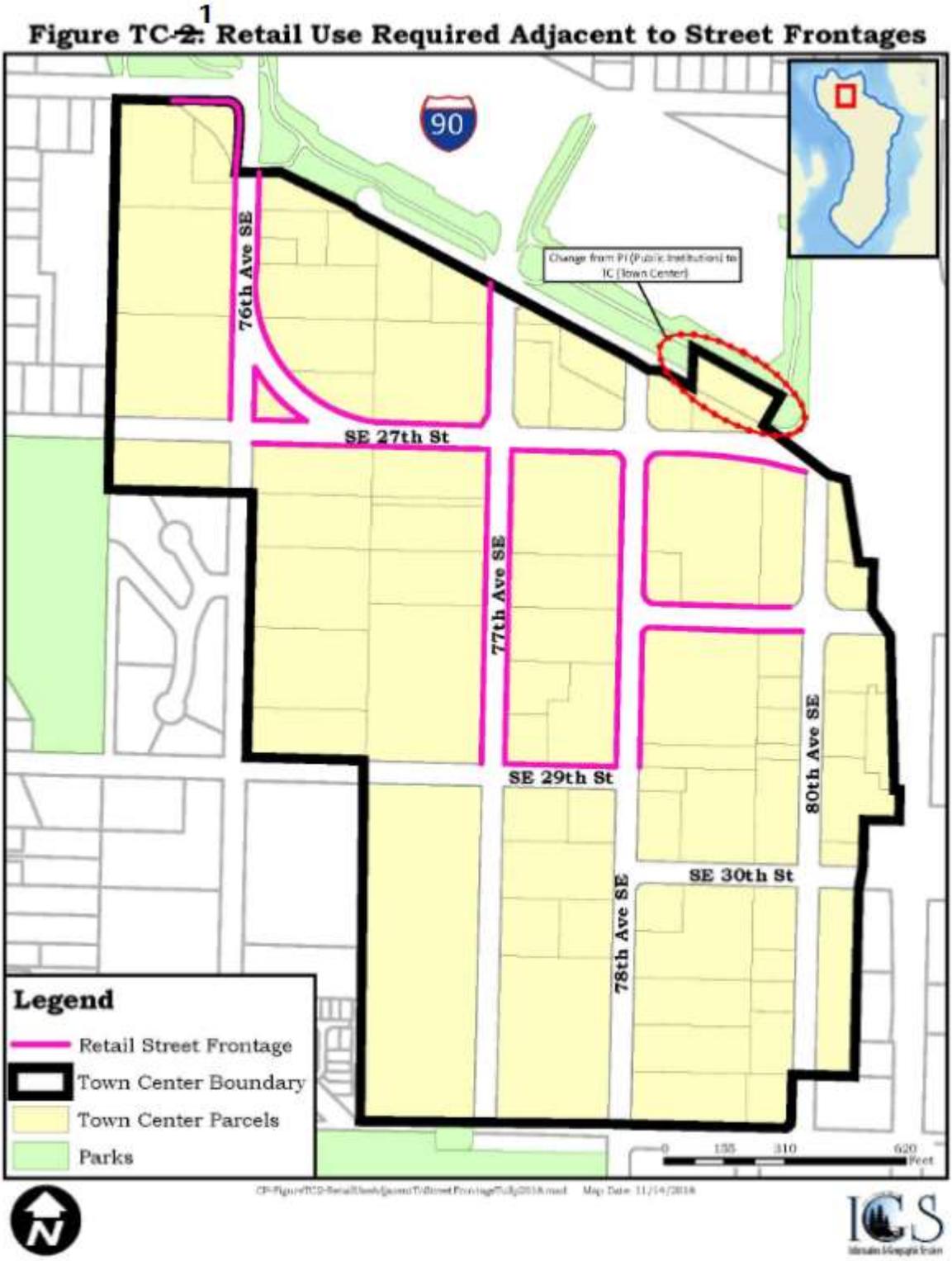
3.2 Locate taller buildings on the north end of the Town Center and step down building height through the center to lower heights on the south end, bordering Mercedale Park. **See Figure TC-1.**

Figure TC-1: Town Center Subareas & Maximum Height Limit



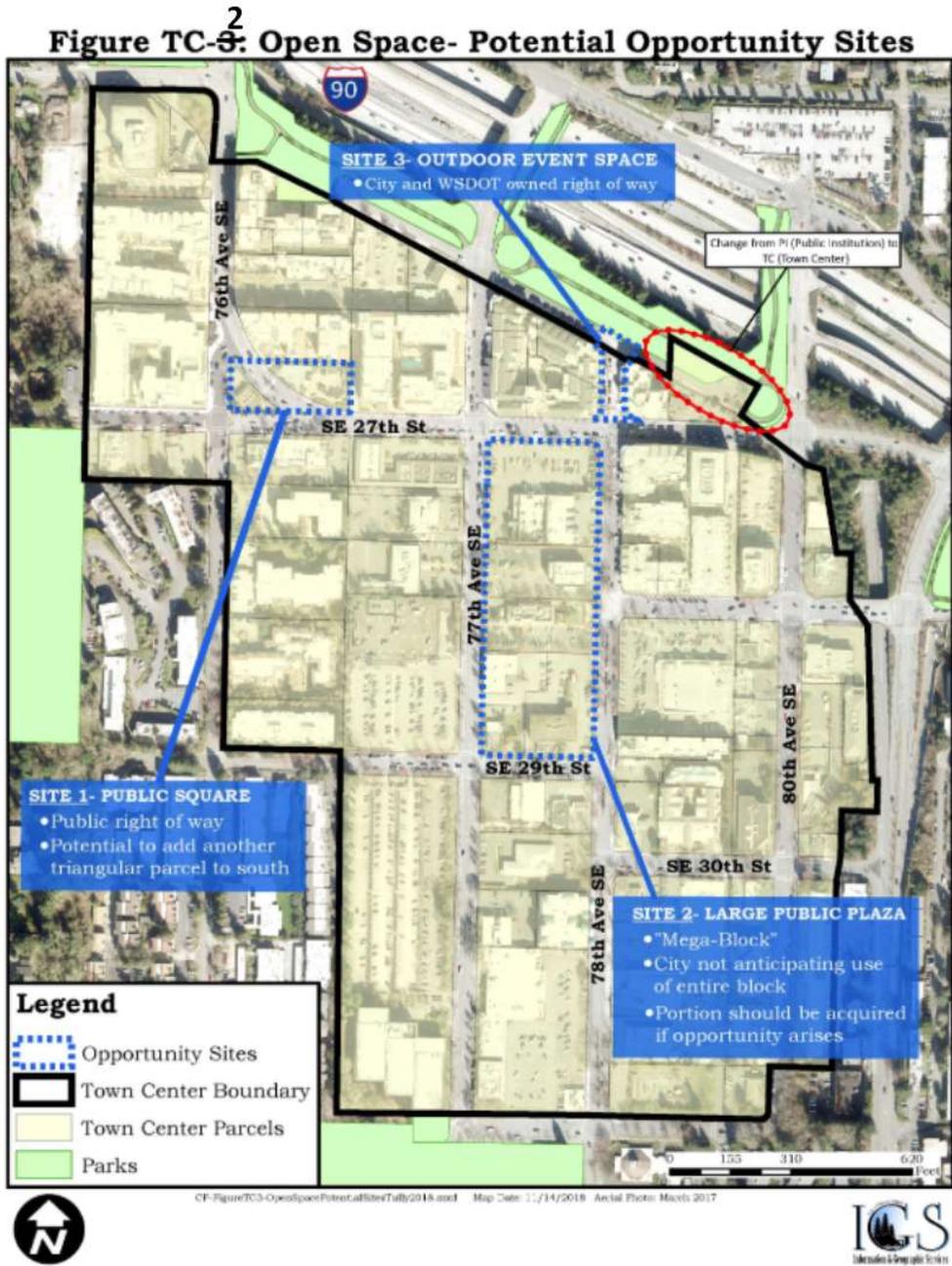
15

- 1
- 2 4.2 Retail street frontages (Figure TC-21) should be the area where the majority of retail activity is
- 3 focused. Retail shops and restaurants should be the dominant use, with personal services also
- 4 encouraged to a more limited extent. [...]



5

- 1 12.3 Investigate potential locations and funding sources for the development (and acquisition if
- 2 needed) of one or more significant public open space(s) that can function as an anchor for the
- 3 Town Center’s character and redevelopment. Identified “opportunity sites” are shown in Figure
- 4 TC-3-2 and described below. These opportunity sites should not preclude the identification of
- 5 other sites, should new opportunities or circumstances arise.



6
7

1 Amendment 2: Establish goals and policies to prevent and/or mitigate the impacts of
2 climate change

3 Land Use Element

4 I. Introduction [...]

5 The community strongly values environmental protection. As a result, local development regulations
6 have sought to safeguard land, water and the natural environment, balanced with private property
7 rights. To reflect community priorities, development regulations also attempt to balance views and tree
8 conservation.

9 Town Center

10 For many years, Mercer Island citizens have been concerned about the future of the community’s
11 downtown. Past business district revitalization initiatives (e.g. Project Renaissance in 1990) strove to
12 overcome the effects of “under-capitalization” in the Town Center. These efforts sought to support and
13 revitalize downtown commercial/retail businesses and devised a number of recommendations for future
14 Town Center redevelopment. Growing out of previous planning efforts, a renewed interest in Town
15 Center revitalization emerged in 1992 -- one looking to turn the 33-year-old downtown into the vital
16 economic and social center of the community.[...]

17 Sustainability

18 In 2006, a grassroots effort of Island citizens led the City to modify the vision statement in its
19 Comprehensive Plan to include language embracing general sustainability, and in May 2007 the City
20 Council committed to a sustainability work program as well as a specific climate goal of reducing
21 greenhouse gas (GHG) emissions by 80% from 2007 levels by 2050, which was consistent with King
22 County and Washington State targets. Later in 2007, the City Council set an interim emissions reduction
23 goal (often called a “milepost”) for City operations of 5% by 2012.

24 Mercer Island has a proud tradition of accomplishment toward sustainability. One of the earliest efforts
25 was the formation of the Committee to Save the Earth by high school students in the early 1970s.
26 Through the students’ fundraising, the construction and opening of the Mercer Island Recycling Center
27 (Center) was realized in 1975. The self-supported Center was well-patronized by Islanders and, during its
28 many years of operation, it prevented millions of pounds of recyclable materials from ending up in the
29 landfill while contributing to the development of a sustainability ethic on Mercer Island.

30 Numerous community groups have contributed to sustainability accomplishments in the ensuing years,
31 and many are still active. Sustainable Mercer Island (SMI), has helped organize and publicize solarize
32 campaigns, among other contributions. SMI participants are also independently involved in youth
33 environmental education, public outreach, advocating for bicycle and pedestrian facilities, and many
34 other activities. Some are doing research, and many are volunteering with national and local
35 organizations working to solve the climate crisis. One volunteer leads the very successful Green Schools
36 program for the Mercer Island School District, supported by King County Department of Natural
37 Resources and Parks. SMI fosters waste reduction, recycling, and conservation by students and schools.

1 IslandVision, a non-profit organization, encourages and supports sustainable practices on Mercer Island.
2 It provided to the City, in 2018, a technical analysis of GHG sources on Mercer Island and recommended
3 strategies to reduce GHG emissions.

4 In 2012, the City convened a Sustainability Policy Taskforce, a City/community partnership, to
5 recommend sustainability policies to the City. The City Council adopted its recommendations including
6 dedicated staffing, incorporation of recommendations into City planning documents, development of a
7 Sustainability Plan, and legislative actions to foster sustainability. The City's Sustainability Manager was
8 hired in 2013.

9 From 2010 to 2014⁹, with the entire community's sustainability in mind, the City has implemented a
10 wide range of outreach programs, efficiency campaigns, alternative energy initiatives, land-use
11 guidelines, and other natural resource management measures designed to minimize the overall impacts
12 generated by Island residents, for the benefit of future generations. Due to the 20-year horizon
13 envisioned by this Comprehensive plan, it is especially appropriate to include measures that address
14 the long-term actions needed to reduce greenhouse gas emissions, ideally in collaboration with other
15 local governments. Actions that the City will take in the management of its own facilities and operations
16 are addressed in the Capital Facilities Element of this plan. In 2018, the City continued to promote and
17 support sustainable development, through the development of green building goals and policies for all
18 residential development.

19 Climate Change

20 Climate change has far-reaching and fundamental consequences for our economy, environment, public
21 health, and safety. Cities have a vital role in mitigating and adapting to climate change both individually
22 and by working collaboratively with other local governments. Current science indicates that to avoid the
23 worst impacts of global warming we need to reduce global GHG emissions sharply.

24 In 2008, the City created a Climate Action Task Force which was charged with developing a climate
25 action plan for the City and community. The resulting plan called for tracking emissions and the
26 formation of a City/community partnership which was called the Green Ribbon Commission. It was
27 tasked with identifying strategies to reduce GHG emissions. Notable outcomes were the successful
28 promotion of Puget Sound Energy's Green Power Program, which generated funds to cover the cost of
29 the solar array the City installed at the Mercer Island Community & Events Center, and the 22 Ways
30 emissions reduction campaign.

31 Leap for Green Sustainability Fair spearheaded by IslandVision and co-developed with the City is a vital
32 instrument to educate and encourage engagement in sustainability. In addition to food and
33 entertainment, the fair offers activities for kids and adults, demonstrations and displays of
34 environmentally friendly ways of living, sustainability vendors, and more. The fair was not held in 2019
35 due to budget constraints.

36 The City has been very active in addressing climate change and has received national recognition for its
37 efforts. In 2013, the City was recognized by the EPA as a Green Power Community of the Year for its very

1 successful Green Power sign up campaign for residents and for its commitment to local solar power
2 generation. It was awarded Sol Smart Gold Designation from the Department of Energy in January 2018
3 for meeting stringent and objective criteria targeting removal of obstacles to solar development
4 including streamlined permitting. As of January 2018, there were 184 known solar installations in the
5 City, higher per capita than any other Eastside City. The City offers same day permitting for most solar
6 installations and most require only an electrical permit. The City has also installed electric vehicle
7 charging stations, banned plastic bags, successfully piloted bike share and ride hailing services, and
8 contracted with PSE for energy from a new windfarm to power 100% of City facilities, among many
9 other actions.

10 The Capital Facilities Element includes a summary of the City’s actions to reduce its own carbon
11 footprint.

12 King County and cities formed the innovative King County-Cities Climate Collaboration (K4C) in 2014 to
13 coordinate and enhance local government climate efforts. Mercer Island was a founding member. The
14 K4C has charted opportunities for joint action to reduce GHG emissions and accelerate progress toward
15 a clean and sustainable future. Mercer Island, through K4C, seeks opportunities to partner on outreach
16 to decision-makers and the public, adopt consistent standards and strategies, share solutions,
17 implement pilot projects, and cooperate on seeking funding resources. In 2016, Mercer Island, along
18 with King County and other partners in K4C, was recognized with a national Climate Leadership Award
19 from EPA.

20 Community GHG emissions have been inventoried and reported to K4C and the public when possible,
21 though 2016 through 2019 data have yet to be entered. The major sources of GHG on Mercer Island
22 have been found to be passenger car travel (estimated at 40% of total) and building energy consumption
23 (48% residential plus commercial).

24 With many good efforts completed and underway, it is necessary to take further action in order to meet
25 GHG reduction targets, both in our households and in our community.

26
27 Land Use Element

28 Goal 28: Reduce community-wide greenhouse gas emissions

29 28.1 Partner with the King County-Cities Climate Collaboration (K4C) to mitigate climate change.

30 28.2 Establish, and support annual reporting on, Mercer Island GHG emission reduction targets
31 consistent with K4C’s.

32 28.3 Provide public information and support to individual and community efforts to mitigate climate
33 change.

34 28.4 Evaluate and prioritize K4C-recommended actions to reduce GHG emissions.

1 28.5 Encourage the reduction of and emissions from passenger vehicles through the development of
2 zero- or low-greenhouse gas emitting transportation options and by reducing single-occupancy
3 vehicle trips.

4 28.6 Promote an energy-efficient built environment by:

- 5 1. Focusing development where utility and transportation investments have been made;
- 6 2. Promoting the use of renewable and zero- and low-GHG emitting energy sources; and
- 7 3. Encouraging the use of carbon-efficient building materials and building design.
- 8 4. Mitigate urban heat island effects by expanding tree canopy and vegetation cover.

9 28.7 Promote renewable power generation in the community.

10

11 Goal 29: Develop and implement a Climate Action Plan.

12

13 Goal 30: Adapt to and mitigate local climate change impacts.

14 30.1 Prioritize the prevention of climate change.

15 30.2 Develop an adaptive response to expected climate change impacts on the community.

16 30.3 Increase carbon sequestration through expanding tree canopy and vegetation cover.

17 [...]

18 Utility Element

19 Solid Waste Policies

20 5.1 All new construction, with the exception of single-family homes, shall be required to provide
21 adequate space for on-site storage and collection of recyclables pursuant to Ordinance A-99.

22 5.2 The City shall actively promote and support recycling, composting and waste reduction
23 techniques among the single-family, multi-family and commercial sectors with the aim of
24 meeting or exceeding King County diversion goals.

25 5.3 The City shall, whenever practical, provide convenient opportunities for residents to recycle
26 appliances, tires, bulky yard debris and other hard-to-recycle materials.

27 5.4 The City shall actively promote and support the proper handling and disposal of hazardous
28 waste produced by households and businesses. The use of alternate products that are less
29 hazardous or produce less waste shall be encouraged.

1 5.5 City departments and facilities shall actively participate in waste reduction and recycling
2 programs.

3 5.6 All hazardous waste generated by City departments and facilities shall be handled and disposed
4 of in accordance with applicable county, state, regional and federal regulations.

5 5.7 The City shall actively enforce the Solid Waste Code and other ordinances and regulations that
6 prohibit the illegal dumping of yard debris and other types of waste.

7 5.8 The City shall play an active role in regional solid waste planning, with the goal of promoting
8 uniform regional approaches to solid waste management.

9 5.9 The City shall actively promote and support the recycling, re-use or composting of construction,
10 demolition and land-clearing debris wherever feasible.

11 [...]

12 Capital Facilities Element

13 I. Introduction [...]

14 In 2012, activities were expanded further with the hiring of the City’s first dedicated Sustainability
15 Manager, who designs, implements, and then oversees much of the internal sustainability project work.
16 In addition, the Mayor and City Council have increasingly addressed or supported specific regional and
17 state-level climate commitments or legislation.

18 In 2017, the City confirmed a major commitment to clean power by announcing its contract with Puget
19 Sound Energy for 2019 through 2039, in which it will buy 20 years of clean wind power to replace its
20 current mix of electricity, covering its annual municipal usage of three million kilowatt hours.

21 Due to the 20-year horizon envisioned by this Comprehensive pPlan, it is especially appropriate to
22 include internal measures that address the long-term actions needed to reduce greenhouse gas
23 emissions, ideally in collaboration with other local governments. Actions that the City will implement
24 with the entire community’s sustainability in mind are addressed in the Land Use Element of this plan.
25 Various City Departments, such as Parks and Recreation and Maintenance, prepare functional plans that
26 directly implement some sustainability programs.

27 ~~These Capital Facilities measures, and others under consideration, are identified in more detail in a~~
28 ~~rolling 6-year Sustainability Plan, to be adopted in 2016, which will guide the City’s internal and external~~
29 ~~actions while taking into account the interrelated issues of climate change, population change, land use,~~
30 ~~public infrastructure, natural resources management, quality of life, public health, and economic~~
31 ~~development.~~

32 V. Capital Facilities Goals and Policies [...]

33 1.20 City operations should be optimized to minimize carbon footprint impacts, especially with
34 respect to energy consumption and waste reduction. New Capital Facilities should incorporate
35 and encourage the sustainable stewardship of the natural environment, ~~and~~ consider the
36 benefit of creating cutting-edge, demonstration projects, and favor options that have the lowest

1 feasible carbon footprint and greatest carbon sequestration potential. The adoption of GHG
2 emission reduction targets recommended by K4C should be considered.

3 1.21 City procurement should include consideration of total lifecycle costs, recycled content, and
4 other common measures of product sustainability.

5 1.22 Current City facilities are operated in an energy-efficient manner, and opportunities for
6 improvement are implemented when feasible. New City facilities should explore meeting public
7 and private-sector sustainable building certification standards, such as the 'BuiltGreen' system
8 and the Leadership in Energy and Environmental Design (LEED) system.

9 1.23 Parks & Open Space Capital Facilities – Identify measures to reduce carbon footprint and GHG
10 emissions when planning projects, choosing options with the lowest feasible carbon footprint
11 and greatest carbon sequestration potential. Implement sustainability measures identified
12 within the City's Parks and Recreation Management Plan, including special attention to direct
13 sustainability measures, such as tree retention, preference for native vegetation and habitat
14 creation, minimized use of chemicals, and reductions in energy and fuel use.

1 Amendment 3: Placeholder for the development of goals and policies supporting
2 economic development on Mercer Island

3
4 Land Use Element

5 Goal 14: ~~Support the further Continue to encourage vitality through the support of~~ economic
6 development ~~of Mercer Island, particularly activities~~ in the Town Center.

7 14.1 Develop an Economic Development Plan.

8 14.2 Establish the Town Center as an active and attractive commercial node, including the use of
9 gateways, wayfinding and signage, and links to transit.

10 ~~14.32~~ Maintain a diversity of downtown land uses.

11 ~~14.43~~ Support economic growth that accommodates Mercer Island’s share of the regional
12 employment growth target of 1,228 new jobs from 2006-2035, by maintaining adequate zoning
13 capacity, infrastructure, and supportive economic development policies.

14 14.54 Investigate formation of a business improvement area (BIA), or other mechanism authorized by
15 state law, to help promote Island businesses, to support Town Center activities, and to finance
16 improvements and amenities.

17 14.6 Identify a staff person who will help coordinate economic development activities.

18 ~~14.75~~ Support public and private investment in existing properties, infrastructure, and marketing to
19 help maintain longstanding businesses and attract new ones.

20 ~~14.86~~ Create a healthy and safe economic environment where Town Center businesses can serve the
21 needs of Mercer Island residents as well as draw upon broader retail and commercial market
22 areas.

23 14.97 Proactively and persistently engage residents, community organizations, and businesses in a
24 collaborative effort to establish a strategy for Mercer Island economic development.

25

1 Amendment 4: Goals and policies supporting the review and possible establishment of
2 multi-modal transportation level of service

3
4 Transportation Element

5 Goal 7: Provide a safe, convenient and reliable transportation system for Mercer Island.

6 7.1 Include in the City’s roadway design standards, requirements for facilities to safely
7 accommodate travel by all travel modes.

8 7.2 Provide a safe transportation system through maintenance and upkeep of transportation
9 facilities.

10 7.3 Monitor the condition and performance of the transportation system to compare growth
11 projections with actual conditions, assess the adequacy of transportation facilities and services,
12 and to identify locations where improvements may become necessary.

13 7.4 Monitor traffic collisions, citizen input/complaints, traffic violations, and traffic volumes to
14 identify and prioritize locations for safety improvements.

15 7.5 Where a need is demonstrated, consider signage, traffic controls, or other strategies to improve
16 the safety of pedestrian crossings.

17 7.6 Verify the policies, criteria and a process to determine when, and under what conditions, private
18 roads and privately maintained roads in the public right of way should be accepted for public
19 maintenance and improvement.

20 7.7 Coordinate with local and regional emergency services to develop priority transportation
21 corridors and develop coordinated strategies to protect and recover from disaster.

22 7.8 Strive to create a complete, connected active transportation system allowing direct and safe
23 access for active transportation modes.

24 7.9 New or remodeled public institution, commercial mixed use and multifamily facilities should
25 have sufficient storage for bicycles and other active transportation modes.

26
27 [...]

28 Goal 10: Maintain acceptable levels of service for transportation facilities and services on Mercer Island.

29 10.1 The City of Mercer Island Level of Service (LOS) at arterial street intersections shall be a
30 minimum of “C” within and adjacent to the Town Center and “D” for all other intersections.

31 10.2 Use the level of service standard to evaluate the performance of the transportation system and
32 guide future system improvements and funding. Emphasize projects and programs that focus on
33 the movement of people and provide alternatives to driving alone.

1 10.3 Implement the following strategy when vehicle capacity or funding is insufficient to maintain the
2 LOS standard: (1) seek additional funding for capacity improvements, (2) explore alternative,
3 lower-cost methods to meet level-of-service standards (e.g., transportation demand
4 management program, bicycle corridor development or other strategies), (3) reduce the types
5 or size of development, (4) restrict development approval, and (5) reevaluate the level of service
6 standard to determine how it might be adjusted to meet land use objectives.

7 10.4 Ensure that the City’s level of service policies are linked to the land use vision and comply with
8 concurrency requirements.

9 10.5 Revise the Transportation Element if the Land Use and/or Capital Facilities Element of the
10 ~~C~~omprehensive ~~P~~lan are changed to maintain a balanced and consistent plan.

11 10.6 Levels of service for pedestrian, bicycle, and transit transportation modes should be established.

12

13 [...]

14 Goal 12: Promote bicycle and pedestrian networks that safely access and link commercial areas,
15 residential areas, schools, and parks within the City.

16 12.1 Maximize the safety and functionality of the bicycle system by enhancing road shoulders, which
17 are to be distinguished from designated bicycle lanes.

18 12.2 Implement the Pedestrian and Bicycle Facilities Plan to meet existing and anticipated needs for
19 non-motorized transportation. This Plan should be coordinated with other transportation
20 planning efforts and periodically updated.

21 12.3 Study opportunities for use of innovative methods for pedestrians crossing streets, including use
22 of colored and textured pavements within the City.

23 12.4 Strive to build community through the in-person interactions facilitated by active transportation
24 at community connection points (schools, library, community centers, bikeshare hubs, etc.)

25 12.5 Prioritize areas near schools and commercial areas for a higher level of service for pedestrians,
26 bicycles, and transit.

27

1 Planning Commission Recommended Comprehensive Plan Amendment
2 Amendment No. 2: Climate Change
3 Dated: October 2, 2019
4

5 Establish goals and policies to prevent and/or mitigate the impacts of climate change
6 Land Use Element
7 I. Introduction [...]

8 The community strongly values environmental protection. As a result, local development regulations
9 have sought to safeguard land, water and the natural environment, balanced with private property
10 rights. To reflect community priorities, development regulations also attempt to balance views and tree
11 conservation.

12 Town Center

13 For many years, Mercer Island citizens have been concerned about the future of the community’s
14 downtown. Past business district revitalization initiatives (e.g. Project Renaissance in 1990) strove to
15 overcome the effects of “under-capitalization” in the Town Center. These efforts sought to support and
16 revitalize downtown commercial/retail businesses and devised a number of recommendations for future
17 Town Center redevelopment. Growing out of previous planning efforts, a renewed interest in Town
18 Center revitalization emerged in 1992 -- one looking to turn the 33-year-old downtown into the vital
19 economic and social center of the community.[...]

20 Sustainability

21 In 2006, a grassroots effort of Island citizens led the City to modify the vision statement in its
22 eComprehensive pPlan to include language embracing general sustainability, and in May 2007 the City
23 Council committed to a sustainability work program as well as a specific climate goal of reducing
24 greenhouse gasGHG emissions by 80% from 2007 levels by 2050, which was consistent with King County
25 and Washington State targets. Later in 2007, the City Council set an interim emissions reduction goal
26 (often called a “milepost”) for City operations of 5% by 2012.

27 Mercer Island has a proud tradition of accomplishment toward sustainability. One of the earliest efforts
28 was the formation of the Committee to Save the Earth by high school students in the early 1970s.

29 Through the students’ fundraising, the construction and opening of the Mercer Island Recycling Center
30 (Center) was realized in 1975. The self-supported Center was well-patronized by Islanders and, during its
31 many years of operation, it prevented millions of pounds of recyclable materials from ending up in the
32 landfill while contributing to the development of a sustainability ethic on Mercer Island.

33 Numerous community groups have contributed to sustainability accomplishments in the ensuing years,
34 and many are still active. Sustainable Mercer Island (SMI), has helped organize and publicize solarize
35 campaigns, among other contributions. SMI participants are also independently involved in youth
36 environmental education, public outreach, advocating for bicycle and pedestrian facilities, and many

1 other activities. Some are doing research and many are volunteering with national and local
2 organizations working to solve the climate crisis. One volunteer leads the very successful Green Schools
3 program for the Mercer Island School District, supported by King County Department of Natural
4 Resources and Parks. SMI fosters waste reduction, recycling, and conservation by students and schools.
5 IslandVision, a non-profit organization, encourages and supports sustainable practices on Mercer Island.
6 It provided to the City, in 2018, a technical analysis of greenhouse gas (GHG) sources on Mercer Island
7 and recommended strategies to reduce GHG emissions.

8 In 2012, the City convened a Sustainability Policy Taskforce, a City/community partnership, to
9 recommend sustainability policies to the City. The City Council adopted its recommendations including
10 dedicated staffing, incorporation of recommendations into City planning documents, development of a
11 Sustainability Plan, and legislative actions to foster sustainability. The City’s Sustainability Manager was
12 hired in 2013.

13 From 2010 to 2019, 4, with the entire community’s sustainability in mind, the City has implemented a
14 wide range of outreach programs, efficiency campaigns, alternative energy initiatives, land-use
15 guidelines, and other natural resource management measures designed to minimize the overall impacts
16 generated by Island residents, for the benefit of future generations. Due to the 20-year horizon
17 envisioned by this Comprehensive plan, it is especially appropriate to include measures that address
18 the long-term actions needed to reduce greenhouse gas emissions, ideally in collaboration with other
19 local governments. Actions that the City will take in the management of its own facilities and operations
20 are addressed in the Capital Facilities Element of this plan. In 2018, the City continued to promote and
21 support sustainable development, through the development of green building goals and policies for all
22 residential development.

23 ~~Beginning in 2018, the City assessed the City’s strengths and weaknesses in supporting sustainability~~
24 ~~using the STAR Communities framework. Information from this assessment, along with the measures~~
25 ~~discussed above, and others under consideration, will be identified in more detail in a rolling 6-year~~
26 ~~Sustainability Plan, to be adopted in 2019, which will guide the City’s internal and external actions while~~
27 ~~taking into account the interrelated issues of climate change, population change, land use, public~~
28 ~~infrastructure, transportation choices, natural resources management, equitable services and~~
29 ~~accessibility, arts and community, public health and safety, human services, and economic development.~~

30 Climate Change

31 Climate change has far-reaching and fundamental consequences for our economy, environment, public
32 health, and safety. Cities have a vital role in mitigating and adapting to climate change both individually
33 and by working collaboratively with other local governments. Current science indicates that to avoid the
34 worst impacts of global warming we need to reduce global GHG emissions sharply.

35 In 2008, the City created a Climate Action Task Force which was charged with developing a climate
36 action plan for the City and community. The resulting plan called for tracking emissions and the
37 formation of a City/community partnership which was called the Green Ribbon Commission. It was
38 tasked with identifying strategies to reduce GHG emissions. Notable outcomes were the successful

1 promotion of Puget Sound Energy’s Green Power Program, which generated funds to cover the cost of
2 the solar array the City installed at the Mercer Island Community & Events Center, and the 22 Ways
3 emissions reduction campaign.

4 Leap for Green Sustainability Fair spearheaded by IslandVision and co-developed with the City is a vital
5 instrument to educate and encourage engagement in sustainability. In addition to food and
6 entertainment, the fair offers activities for kids and adults, demonstrations and displays of
7 environmentally friendly ways of living, sustainability vendors, and more. The fair was not held in 2019
8 due to budget constraints.

9 In 2013, the City was recognized by the EPA as a Green Power Community of the Year for its Green
10 Power sign up campaign and for its commitment to local solar power generation. It was awarded Sol
11 Smart Gold Designation from the Department of Energy in January 2018 for removing obstacles to solar
12 development. The City offers same-day permitting for most solar installations.

13 The Capital Facilities Element includes a summary of the City’s actions to reduce its own carbon
14 footprint.

15 King County and cities formed the innovative King County-Cities Climate Collaboration (K4C) in 2014 to
16 coordinate and enhance local government climate efforts. The City was a founding member of K4C. K4C
17 has charted opportunities for joint action to reduce GHG emissions and accelerate progress toward a
18 clean and sustainable future. Mercer Island, through K4C, seeks opportunities to partner on outreach to
19 decision-makers and the public, adopt consistent standards and strategies, share solutions, and
20 cooperate on seeking funding resources. In 2016, Mercer Island, along with King County and other
21 partners in K4C was recognized with a national Climate Leadership Award from EPA.

22 The City has been very active in addressing climate change and has received national recognition for its
23 efforts. In 2013, the City was recognized by the EPA as a Green Power Community of the Year for its very
24 successful Green Power sign up campaign for residents and for its commitment to local solar power
25 generation. It was awarded Sol Smart Gold Designation from the Department of Energy in January 2018
26 for meeting stringent and objective criteria targeting removal of obstacles to solar development
27 including streamlined permitting. As of January 2018 there were 184 known solar installations in the
28 City, higher per capita than any other Eastside City. The City offers same day permitting for most solar
29 installations and most require only an electrical permit. The City has installed electric vehicle charging
30 stations, banned plastic bags, and successfully piloted bike share and ride hailing services among many
31 other actions.

32 Community GHG emissions have been inventoried and reported to K4C and the public when possible.
33 From 2016 through 2019, staff was not able to complete this task. The major sources of GHG on Mercer
34 Island have been found to be passenger car travel (estimated at 40% of total) and building energy
35 consumption (48% residential plus commercial).

36 With many good efforts completed and underway, it is necessary to take further action in order to meet
37 GHG reduction targets, both in our households and in our community.

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Goal 28: Reduce community-wide greenhouse gas emissions

28.1 Collaborate with King County and cities as a member of the King County-Cities Climate Collaboration (K4C) to increase the efficiency of efforts to reduce GHG emissions.

28.2 Dedicate staff to represent the City in K4C and to coordinate City programs and actions to mitigate climate change.

28.3 Update and adopt Mercer Island GHG reduction targets consistent with K4C, as amended.

28.4 Prioritize for implementation those K4C-recommended strategies that are relevant and feasible for Mercer Island.

28.5 Support annual reporting of Mercer Island GHG emissions to K4C and the public.

28.6 Engage individuals, community organizations, and businesses in a collaborative effort to mitigate climate change.

28.7 Provide public outreach and information to citizens about how they can mitigate and respond to climate changes.

28.8 GHG reduction strategies should be evaluated and used to set a priority for actions. Community acceptance and legal requirements should be considered.

28.9 Consider for early action the reduction of emissions from passenger vehicles.

a) Work with the community to develop zero- or low-greenhouse gas emitting transportation options for traveling intra-Island to or from community connection points. These options should be popular enough in use to substantially reduce aggregate GHG emissions from passenger vehicles. The program should be in place by 2023 when light rail arrives.

b) Promote electric vehicles.

28.10 Consider for early action the reduction of energy use in buildings.

a) Determine the best methods to promote a transition from natural gas to electricity for the energy needs of new buildings and retrofit of existing buildings as the regional source of power moves entirely away from fossil fuels.

b) Encourage and provide incentives for energy-saving retrofits of existing homes and buildings, in partnership with Puget Sound Energy and other organizations.

c) Determine the best methods to promote the use of construction materials that embody the least carbon feasible in manufacture and use. Consider building code changes, as necessary, to accommodate this transition.

d) Consider adopting a local building energy-benchmarking and disclosure ordinance.

e) Support green power community challenges and other programs to reduce building energy use.

- 1 28.11 Promote renewable power generation in the community.
- 2 a) Support campaigns to install solar energy and other power generation methods.
- 3 b) Continue to offer streamlined renewable energy installation permitting, when possible,
- 4 incentives, and other means to encourage power generation.
- 5 c) Consider building code changes, as necessary, to accommodate community renewable
- 6 power generation.
- 7 28.12 Focus future land development where utility and transportation investments have been made and
- 8 encourage land use patterns to be carbon efficient.
- 9 28.13 Strive to increase carbon sequestration and resilience to urban heat island effects by expanding
- 10 tree canopy and vegetation cover. Consider the use of cool roofs and pavements (reflective, for
- 11 example), reduced pavement widths, green roofs, and other potentially useful strategies.
- 12 28.14 Consider development of an Urban Forest Management Plan to assess canopy cover, set goals, and
- 13 establish implementation strategies.
- 14 28.15 Encourage residents using fossil fuels, such as home heating oil, to convert to clean and renewable
- 15 energy sources.

16 **Goal 29: Develop a Climate Action Plan.**

17 Consider development of a Climate Action Plan including the following components:

- 18 • A summary of City actions to date;
- 19 • A broad examination of actions to reduce greenhouse gas emissions;
- 20 • Re-evaluation of the recommendations from the 2012 Sustainability Policy Recommendations
- 21 Report;
- 22 • Provisions to monitor progress of implementation; and
- 23 • Provisions to update the plan in response to changing conditions and new opportunities.

25 **Goal 30: Adapt to climate change.**

26 Place the highest priority on mitigating climate change but respond to indications of impacts in the

27 community that may require an adaptive response.

28 [...]

29 Utility Element
30 Solid Waste Policies

31 5.1 All new construction, with the exception of single-family homes, shall be required to provide
32 adequate space for on-site storage and collection of recyclables pursuant to Ordinance A-99.

1 5.2 The City shall actively promote and support recycling, composting and waste reduction techniques
2 among the single-family, multi-family and commercial sectors with the aim of meeting or exceeding
3 King County diversion goals.

4 5.3 The City shall, whenever practical, provide convenient opportunities for residents to recycle
5 appliances, tires, bulky yard debris and other hard-to-recycle materials.

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7 produced by households and businesses. The use of alternate products that are less hazardous or
8 produce less waste shall be encouraged.

9 5.5 City departments and facilities shall actively participate in waste reduction and recycling programs.

10 5.6 All hazardous waste generated by City departments and facilities shall be handled and disposed of
11 in accordance with applicable county, state, regional and federal regulations.

12 5.7 The City shall actively enforce the Solid Waste Code and other ordinances and regulations that
13 prohibit the illegal dumping of yard debris and other types of waste.

14 5.8 The City shall play an active role in regional solid waste planning, with the goal of promoting
15 uniform regional approaches to solid waste management.

16 5.9 The City shall actively promote and support the recycling, re-use or composting of construction,
17 demolition and land-clearing debris wherever feasible.

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22 Manager, who designs, implements, and then oversees much of the internal sustainability project work.
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26 Sound Energy for 2019 through 2039, in which it will buy 20 years of clean wind power to replace its
27 current mix of electricity, covering its annual municipal usage of three million kilowatt hours.

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11 1.21 City procurement should include consideration of total lifecycle costs, recycled content, and other
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14 improvement are implemented when feasible. New City facilities should explore meeting public
15 and private-sector sustainable building certification standards, such as the ‘BuiltGreen’ system and
16 the Leadership in Energy and Environmental Design (LEED) system.

17 1.23 Parks & Open Space Capital Facilities – Identify measures to reduce carbon footprint and GHG
18 emissions when planning projects, choosing options with the lowest feasible carbon footprint and
19 greatest carbon sequestration potential. Implement sustainability measures identified within the
20 City’s Parks and Recreation Management Plan, including special attention to direct sustainability
21 measures, such as tree retention, preference for native vegetation and habitat creation, minimized
22 use of chemicals, and reductions in energy and fuel use.

Comparison Matrix of draft 2019 Comprehensive Plan Amendments

October 15, 2019 (First Reading) to December 3, 2019 (Second Reading)

| Policy / Goal # | October 15, 2019 (1st Reading) | December 3, 2019 (2nd Reading) | Note |
|---|--|---|---|
| Amendment No. 1: Remove Specific Town Center Subarea designations from the Land Use Element | | | |
| | Delete Figure TC-1 and re-number remaining figures in Land Use Element | Delete Figure TC-1 and re-number remaining figures in Land Use Element | No change |
| Amendment No. 2: Establish goals and policies to prevent and/or mitigate the impacts of climate change | | | |
| Background / introduction text | | | Background / introduction paragraphs were re-ordered and edited for clarity; no policy change was included. For brevity, this background material is omitted from the matrix. |
| Land Use Goal 28 | <u>Goal 28: Reduce community-wide greenhouse gas emissions</u> | <u>Goal 28: Reduce community-wide greenhouse gas emissions</u> | No change |
| Land Use Policy 28.1 | <u>28.1 Collaborate with King County and cities as a member of the King County-Cities Climate Collaboration (K4C) to increase the efficiency of efforts to reduce GHG emissions.</u> | <u>28.1 Partner with the King County-Cities Climate Collaboration (K4C) to mitigate climate change.</u> | Planning Commission leadership / Staff revision |
| Land Use Policy 28.2 | <u>28.2 Dedicate staff to represent the City in K4C and to coordinate City programs and actions to mitigate climate change.</u> | <u>28.2 Establish, and support annual reporting on, Mercer Island GHG emission reduction targets consistent with K4C's.</u> | Planning Commission leadership / Staff revision (consolidates October 15 policies 28.3, 28.5) |
| Land Use Policy 28.3 | <u>28.3 Update and adopt Mercer Island GHG reduction targets consistent with K4C, as amended.</u> | <u>28.3 Provide public information and support to individual and community efforts to mitigate climate change.</u> | Planning Commission leadership / Staff revision (consolidates October 15 policies 28.6, 28.7, 28.15) |
| Land Use Policy 28.4 | <u>28.4 Prioritize for implementation those K4C-recommended strategies that are relevant and feasible for Mercer Island.</u> | <u>28.4 Evaluate and prioritize K4C-recommended actions to reduce GHG emissions.</u> | Planning Commission leadership / Staff revision (consolidates October 15 policies 28.4 and 28.8) |
| Land Use Policy 28.5 | <u>28.5 Support annual reporting of Mercer Island GHG emissions to K4C and the public.</u> | <u>28.5 Encourage the reduction of emissions from passenger vehicles through the</u> | Planning Commission leadership / Staff revision (consolidates October 15 policy 28.9) |

| Policy / Goal # | October 15, 2019 (1st Reading) | December 3, 2019 (2nd Reading) | Note |
|----------------------|---|---|--|
| | | <u>development of zero- or low-greenhouse gas emitting transportation options and by reducing single-occupancy vehicle trips.</u> | |
| Land Use Policy 28.6 | <u>28.6 Engage individuals, community organizations, and businesses in a collaborative effort to mitigate climate change.</u> | <u>28.6 Promote an energy-efficient built environment by:</u> <u>1. Focusing development where utility and transportation investments have been made;</u> <u>2. Promoting the use of renewable and zero- and low-GHG emitting energy sources; and</u> <u>1.3. Encouraging the use of carbon-efficient building materials and building design.</u> <u>2.4. Mitigate urban heat island effects by expanding tree canopy and vegetation cover.</u> | Planning Commission leadership / Staff revision (consolidates October 15 policies 28.10, 28.11, 28.12, 28.13, 28.14) |
| Land Use Policy 28.7 | <u>28.7 Provide public outreach and information to citizens about how they can mitigate and respond to climate changes.</u> | <u>28.7 Promote renewable power generation in the community.</u> | Planning Commission leadership / Staff revision (consolidates October 15 policies 28.10, 28.11) |
| Land Use Policy 28.8 | <u>28.8 GHG reduction strategies should be evaluated and used to set a priority for actions. Community acceptance and legal requirements should be considered.</u> | | See above notes regarding consolidation |
| Land Use Policy 28.9 | <u>28.9 Consider for early action the reduction of emissions from passenger vehicles.</u> a) <u>Work with the community to develop zero-greenhouse gas emitting transportation option for traveling intra-</u> | | See above notes regarding consolidation |

| Policy / Goal # | October 15, 2019 (1st Reading) | December 3, 2019 (2nd Reading) | Note |
|------------------------------|--|--------------------------------|--|
| | <p><u>Island to or from community connection points. These options should be popular enough in use to substantially reduce aggregate GHG emissions from passenger vehicles. The program should be in place by 2023 when light rail arrives.</u></p> <p>b) <u>Promote electric vehicles.</u></p> | | |
| <p>Land Use Policy 28.10</p> | <p><u>28.10 Consider for early action the reduction of energy use in in buildings.</u></p> <p>a) <u>Determine the best methods to promote a transition from natural gas to electricity for the energy needs of new buildings and retrofit of existing buildings as the regional source of power moves entirely away from fossil fuels.</u></p> <p>b) <u>Encourage and provide incentives for energy-saving retrofits of existing homes and buildings, in partnership with Puget Sound Energy and other organizations.</u></p> <p>c) <u>Determine the best methods to promote the use of construction materials that embody the least carbon feasible in manufacture and use. Consider building code changes, as necessary, to accommodate this transition.</u></p> <p>d) <u>Consider adopting a local building energy-benchmarking and disclosure ordinance.</u></p> | | <p>See above notes regarding consolidation</p> |

| Policy / Goal # | October 15, 2019 (1st Reading) | December 3, 2019 (2nd Reading) | Note |
|-----------------------|--|--------------------------------|---|
| | e) <u>Support green power community challenges and other programs to reduce building energy use.</u> | | |
| Land Use Policy 28.11 | <p><u>28.11 Promote renewable power generation in the community.</u></p> <p>a) <u>Support campaigns to install solar energy and other power generation methods.</u></p> <p>b) <u>Continue to offer streamlined renewable energy installation permitting, when possible, incentives, and other means to encourage power generation.</u></p> <p>c) <u>Consider building code changes, as necessary, to accommodate community renewable power generation.</u></p> | | See above notes regarding consolidation |
| Land Use Policy 28.12 | <u>28.12 Focus future land development where utility and transportation investments have been made and encourage land use patterns to be carbon efficient.</u> | | See above notes regarding consolidation |
| Land Use Policy 28.13 | <u>28.13 Strive to increase carbon sequestration and resilience to urban heat island effects by expanding tree canopy and vegetation cover. Consider the use of cool roofs and pavements (reflective, for example), reduced pavement widths, green roofs, and other potentially useful strategies.</u> | | See above notes regarding consolidation |
| Land Use Policy 28.14 | <u>28.14 Consider development of an Urban Forest Management Plan to assess canopy cover, set goals, and establish implementation strategies.</u> | | See above notes regarding consolidation |

| Policy / Goal # | October 15, 2019 (1st Reading) | December 3, 2019 (2nd Reading) | Note |
|-----------------------|--|---|--|
| Land Use Policy 28.15 | <u>28.15 Encourage residents using fossil fuels, such as home heating oil, to convert to clean and renewable energy sources.</u> | | See above notes regarding consolidation |
| Land Use Goal 29 | <u>Goal 29: Develop a Climate Action Plan. Consider development of a Climate Action Plan including the following components:</u> <ul style="list-style-type: none"> • <u>A summary of City actions to date;</u> • <u>A broad examination of actions to reduce greenhouse gas emissions;</u> • <u>Re-evaluation of the recommendations from the 2012 Sustainability Policy Recommendations Report;</u> • <u>Provisions to monitor progress of implementation; and</u> • <u>Provisions to update the plan in response to changing conditions and new opportunities.</u> | <u>Goal 29: Develop and implement a Climate Action Plan.</u> | Planning Commission leadership / Staff revision |
| Land Use Goal 30 | <u>Goal 30: Adapt to climate change. Place the highest priority on mitigating climate change but respond to indications of impacts in the community that may require an adaptive response.</u> | <u>Goal 30: Adapt to and mitigate local climate change impacts.</u> | Planning Commission leadership / Staff revision |
| Land Use Policy 30.1 | | <u>30.1 Prioritize the prevention of climate change.</u> | Planning Commission leadership / Staff revision (incorporates October 15 policy 28.8) |
| Land Use Policy 30.2 | | <u>30.2 Develop an adaptive response to expected climate change impacts on the community.</u> | Planning Commission leadership / Staff revision (incorporates “adaptive” aspect of October 15 goal 30) |
| Land Use Policy 30.3 | | <u>30.3 Increase carbon sequestration through expanding tree canopy and vegetation cover.</u> | Planning Commission leadership / Staff revision (incorporates October 15 policy 28.13) |

| Policy / Goal # | October 15, 2019 (1st Reading) | December 3, 2019 (2nd Reading) | Note |
|--------------------------------|--|--|------------|
| Utility Policy 5.2 | 5.2 The City shall actively promote and support recycling, composting and waste reduction techniques among the single-family, multi-family and commercial sectors <u>with the aim of meeting or exceeding King County diversion goals.</u> | 5.2 The City shall actively promote and support recycling, composting and waste reduction techniques among the single-family, multi-family and commercial sectors <u>with the aim of meeting or exceeding King County diversion goals.</u> | No change |
| Capital Facilities Policy 1.20 | 1.20 City operations should be optimized to minimize carbon footprint impacts, especially with respect to energy consumption and waste reduction. New Capital Facilities should incorporate and encourage the sustainable stewardship of the natural environment, and consider the benefit of creating cutting-edge, demonstration projects, <u>and favor options that have the lowest feasible carbon footprint and greatest carbon sequestration potential. The adoption of greenhouse gas emission reduction targets recommended by King County-Cities Climate Collaboration should be considered.</u> | 1.20 City operations should be optimized to minimize carbon footprint impacts, especially with respect to energy consumption and waste reduction. New Capital Facilities should incorporate and encourage the sustainable stewardship of the natural environment, and consider the benefit of creating cutting-edge, demonstration projects, <u>and favor options that have the lowest feasible carbon footprint and greatest carbon sequestration potential. The adoption of GHG emission reduction targets recommended by K4C should be considered.</u> | Minor edit |
| Capital Facilities Policy 1.23 | 1.23 Parks & Open Space <u>Capital Facilities – Identify measures to reduce carbon footprint and GHG emissions when planning projects, choosing options with the lowest feasible carbon footprint and greatest carbon sequestration potential.</u> Implement sustainability measures identified within the City’s Parks and Recreation Management Plan, including special attention to direct sustainability measures, such as tree retention, preference for native vegetation and habitat creation, minimized use of | 1.23 Parks & Open Space <u>Capital Facilities – Identify measures to reduce carbon footprint and GHG emissions when planning projects, choosing options with the lowest feasible carbon footprint and greatest carbon sequestration potential.</u> Implement sustainability measures identified within the City’s Parks and Recreation Management Plan, including special attention to direct sustainability measures, such as tree retention, preference for native vegetation and habitat creation, minimized use of | No change |

| Policy / Goal # | October 15, 2019 (1st Reading) | December 3, 2019 (2nd Reading) | Note |
|--|---|--|---|
| | chemicals, and reductions in energy and fuel use. | chemicals, and reductions in energy and fuel use. | |
| Amendment No. 3: Placeholder for the development of goals and policies supporting economic development on Mercer Island | | | |
| Land Use Goal 14 | GOAL 14: Support the continued <u>Continue to encourage vitality through the support of economic development of Mercer Island, particularly activities in the Town Center.</u> | Goal 14: Support the further <u>Continue to encourage vitality through the support of economic development of Mercer Island, particularly activities in the Town Center.</u> | No change |
| New Land Use Policy 14.1 | | 14.1 <u>Develop an Economic Development Plan.</u> | New policy direction following City Council meeting on October 15, 2019. Resulted in re-numbering policies under Goal 14. |
| Land Use Policy 14.4 & new Land Use Policy 14.5 | 14.4 Investigate formation of a business improvement area (BIA), or other mechanism authorized by state law, to help promote Island businesses, to support Town Center activities, and to finance improvements and amenities. Identify a staff person who will help coordinate economic development activities. | 14.54 Investigate formation of a business improvement area (BIA), or other mechanism authorized by state law, to help promote Island businesses, to support Town Center activities, and to finance improvements and amenities. 14.6 Identify a staff person who will help coordinate economic development activities. | City Council direction on October 15, 2019. |
| Land Use Policy 14.6 / 14.8 (renumbered) | 14.6 Create a healthy economic environment where Town Center businesses can serve the needs of Mercer Island residents as well as draw upon broader retail and commercial market areas. | 14.86 Create a healthy <u>and safe</u> economic environment where Town Center businesses can serve the needs of Mercer Island residents as well as draw upon broader retail and commercial market areas. | City Council direction on October 15, 2019. |
| Land Use Policy 14.7 / 14.9 (renumbered) | 14.7 <u>Engage residents, community organizations, and businesses in a collaborative effort to establish a strategy for Mercer Island economic development.</u> | 14.9 <u>Proactively and persistently engage residents, community organizations, and businesses in a collaborative effort to establish a strategy for Mercer Island economic development.</u> | City Council direction on October 15, 2019. |
| Amendment 4: Goals and policies supporting the review and possible establishment of multi-modal transportation level of service | | | |
| Transportation Policy 7.8 | 7.8 <u>Strive to create a complete, connected active transportation system allowing direct</u> | 7.8 <u>Strive to create a complete, connected active transportation system allowing direct</u> | No change |

| Policy / Goal # | October 15, 2019 (1st Reading) | December 3, 2019 (2nd Reading) | Note |
|----------------------------|--|--|--|
| | <u>and safe access for active transportation modes.</u> | <u>and safe access for active transportation modes.</u> | |
| Transportation Policy 7.9 | <u>7.9 New or remodeled public institution, commercial mixed use and multifamily facilities should have sufficient storage for bicycles and other active transportation modes.</u> | <u>7.9 New or remodeled public institution, commercial mixed use and multifamily facilities should have sufficient storage for bicycles and other active transportation modes.</u> | No change |
| Transportation Policy 10.6 | <u>10.6 Levels of service for pedestrian, bicycle, and transit transportation modes should be established.</u> | <u>10.6 Levels of service for pedestrian, bicycle, and transit transportation modes should be established.</u> | No change |
| Transportation Policy 12.4 | <u>12.4 Strive to build community through the in-person interactions facilitated by active transportation at community connection points (schools, library, community centers, bikeshare hubs, etc.)</u> | <u>12.4 Strive to build community through the in-person interactions facilitated by active transportation at community connection points (schools, library, community centers, bikeshare hubs, etc.)</u> | No change |
| Transportation Policy 12.5 | <u>12.5 Areas near schools and commercial areas should have higher levels of service for pedestrians, bicycles, and transit.</u> | <u>12.5 Prioritize areas near schools and commercial areas for a higher level of service for pedestrians, bicycles, and transit.</u> | City Council direction on October 15, 2019 |

Comparison Matrix of draft 2019 Comprehensive Plan Amendments

October 15, 2019 (First Reading) to December 3, 2019 (Second Reading)

| Policy / Goal # | October 15, 2019 (1st Reading) | December 3, 2019 (2nd Reading) | Note |
|---|--|---|---|
| Amendment No. 1: Remove Specific Town Center Subarea designations from the Land Use Element | | | |
| | Delete Figure TC-1 and re-number remaining figures in Land Use Element | Delete Figure TC-1 and re-number remaining figures in Land Use Element | No change |
| Amendment No. 2: Establish goals and policies to prevent and/or mitigate the impacts of climate change | | | |
| Background / introduction text | | | Background / introduction paragraphs were re-ordered and edited for clarity; no policy change was included. For brevity, this background material is omitted from the matrix. |
| Land Use Goal 28 | <u>Goal 28: Reduce community-wide greenhouse gas emissions</u> | <u>Goal 28: Reduce community-wide greenhouse gas emissions</u> | No change |
| Land Use Policy 28.1 | <u>28.1 Collaborate with King County and cities as a member of the King County-Cities Climate Collaboration (K4C) to increase the efficiency of efforts to reduce GHG emissions.</u> | <u>28.1 Partner with the King County-Cities Climate Collaboration (K4C) to mitigate climate change.</u> | Planning Commission leadership / Staff revision |
| Land Use Policy 28.2 | <u>28.2 Dedicate staff to represent the City in K4C and to coordinate City programs and actions to mitigate climate change.</u> | <u>28.2 Establish, and support annual reporting on, Mercer Island GHG emission reduction targets consistent with K4C's.</u> | Planning Commission leadership / Staff revision (consolidates October 15 policies 28.3, 28.5) |
| Land Use Policy 28.3 | <u>28.3 Update and adopt Mercer Island GHG reduction targets consistent with K4C, as amended.</u> | <u>28.3 Provide public information and support to individual and community efforts to mitigate climate change.</u> | Planning Commission leadership / Staff revision (consolidates October 15 policies 28.6, 28.7, 28.15) |
| Land Use Policy 28.4 | <u>28.4 Prioritize for implementation those K4C-recommended strategies that are relevant and feasible for Mercer Island.</u> | <u>28.4 Evaluate and prioritize K4C-recommended actions to reduce GHG emissions.</u> | Planning Commission leadership / Staff revision (consolidates October 15 policies 28.4 and 28.8) |
| Land Use Policy 28.5 | <u>28.5 Support annual reporting of Mercer Island GHG emissions to K4C and the public.</u> | <u>28.5 Encourage the reduction of emissions from passenger vehicles through the</u> | Planning Commission leadership / Staff revision (consolidates October 15 policy 28.9) |

| Policy / Goal # | October 15, 2019 (1st Reading) | December 3, 2019 (2nd Reading) | Note |
|----------------------|---|---|--|
| | | <u>development of zero- or low-greenhouse gas emitting transportation options and by reducing single-occupancy vehicle trips.</u> | |
| Land Use Policy 28.6 | <u>28.6 Engage individuals, community organizations, and businesses in a collaborative effort to mitigate climate change.</u> | <u>28.6 Promote an energy-efficient built environment by:</u> <u>1. Focusing development where utility and transportation investments have been made;</u> <u>2. Promoting the use of renewable and zero- and low-GHG emitting energy sources; and</u> <u>1.3. Encouraging the use of carbon-efficient building materials and building design.</u> <u>2.4. Mitigate urban heat island effects by expanding tree canopy and vegetation cover.</u> | Planning Commission leadership / Staff revision (consolidates October 15 policies 28.10, 28.11, 28.12, 28.13, 28.14) |
| Land Use Policy 28.7 | <u>28.7 Provide public outreach and information to citizens about how they can mitigate and respond to climate changes.</u> | <u>28.7 Promote renewable power generation in the community.</u> | Planning Commission leadership / Staff revision (consolidates October 15 policies 28.10, 28.11) |
| Land Use Policy 28.8 | <u>28.8 GHG reduction strategies should be evaluated and used to set a priority for actions. Community acceptance and legal requirements should be considered.</u> | | See above notes regarding consolidation |
| Land Use Policy 28.9 | <u>28.9 Consider for early action the reduction of emissions from passenger vehicles.</u> a) <u>Work with the community to develop zero-greenhouse gas emitting transportation option for traveling intra-</u> | | See above notes regarding consolidation |

| Policy / Goal # | October 15, 2019 (1st Reading) | December 3, 2019 (2nd Reading) | Note |
|-----------------------|--|--------------------------------|---|
| | <p><u>Island to or from community connection points. These options should be popular enough in use to substantially reduce aggregate GHG emissions from passenger vehicles. The program should be in place by 2023 when light rail arrives.</u></p> <p>b) <u>Promote electric vehicles.</u></p> | | |
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| Land Use Policy 28.13 | <u>28.13 Strive to increase carbon sequestration and resilience to urban heat island effects by expanding tree canopy and vegetation cover. Consider the use of cool roofs and pavements (reflective, for example), reduced pavement widths, green roofs, and other potentially useful strategies.</u> | | See above notes regarding consolidation |
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| Land Use Policy 30.1 | | <u>30.1 Prioritize the prevention of climate change.</u> | Planning Commission leadership / Staff revision (incorporates October 15 policy 28.8) |
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| Capital Facilities Policy 1.20 | 1.20 City operations should be optimized to minimize carbon footprint impacts, especially with respect to energy consumption and waste reduction. New Capital Facilities should incorporate and encourage the sustainable stewardship of the natural environment, and consider the benefit of creating cutting-edge, demonstration projects, <u>and favor options that have the lowest feasible carbon footprint and greatest carbon sequestration potential. The adoption of greenhouse gas emission reduction targets recommended by King County-Cities Climate Collaboration should be considered.</u> | 1.20 City operations should be optimized to minimize carbon footprint impacts, especially with respect to energy consumption and waste reduction. New Capital Facilities should incorporate and encourage the sustainable stewardship of the natural environment, and consider the benefit of creating cutting-edge, demonstration projects, <u>and favor options that have the lowest feasible carbon footprint and greatest carbon sequestration potential. The adoption of GHG emission reduction targets recommended by K4C should be considered.</u> | Minor edit |
| Capital Facilities Policy 1.23 | 1.23 Parks & Open Space <u>Capital Facilities – Identify measures to reduce carbon footprint and GHG emissions when planning projects, choosing options with the lowest feasible carbon footprint and greatest carbon sequestration potential.</u> Implement sustainability measures identified within the City’s Parks and Recreation Management Plan, including special attention to direct sustainability measures, such as tree retention, preference for native vegetation and habitat creation, minimized use of | 1.23 Parks & Open Space <u>Capital Facilities – Identify measures to reduce carbon footprint and GHG emissions when planning projects, choosing options with the lowest feasible carbon footprint and greatest carbon sequestration potential.</u> Implement sustainability measures identified within the City’s Parks and Recreation Management Plan, including special attention to direct sustainability measures, such as tree retention, preference for native vegetation and habitat creation, minimized use of | No change |

| Policy / Goal # | October 15, 2019 (1st Reading) | December 3, 2019 (2nd Reading) | Note |
|--|---|--|---|
| | chemicals, and reductions in energy and fuel use. | chemicals, and reductions in energy and fuel use. | |
| Amendment No. 3: Placeholder for the development of goals and policies supporting economic development on Mercer Island | | | |
| Land Use Goal 14 | GOAL 14: Support the continued <u>Continue to encourage vitality through the support of economic development of Mercer Island, particularly activities in the Town Center.</u> | Goal 14: Support the further <u>Continue to encourage vitality through the support of economic development of Mercer Island, particularly activities in the Town Center.</u> | No change |
| New Land Use Policy 14.1 | | 14.1 <u>Develop an Economic Development Plan.</u> | New policy direction following City Council meeting on October 15, 2019. Resulted in re-numbering policies under Goal 14. |
| Land Use Policy 14.4 & new Land Use Policy 14.5 | 14.4 Investigate formation of a business improvement area (BIA), or other mechanism authorized by state law, to help promote Island businesses, to support Town Center activities, and to finance improvements and amenities. Identify a staff person who will help coordinate economic development activities. | 14.54 Investigate formation of a business improvement area (BIA), or other mechanism authorized by state law, to help promote Island businesses, to support Town Center activities, and to finance improvements and amenities. 14.6 Identify a staff person who will help coordinate economic development activities. | City Council direction on October 15, 2019. |
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| Amendment 4: Goals and policies supporting the review and possible establishment of multi-modal transportation level of service | | | |
| Transportation Policy 7.8 | 7.8 <u>Strive to create a complete, connected active transportation system allowing direct</u> | 7.8 <u>Strive to create a complete, connected active transportation system allowing direct</u> | No change |

| Policy / Goal # | October 15, 2019 (1st Reading) | December 3, 2019 (2nd Reading) | Note |
|----------------------------|--|--|--|
| | <u>and safe access for active transportation modes.</u> | <u>and safe access for active transportation modes.</u> | |
| Transportation Policy 7.9 | <u>7.9 New or remodeled public institution, commercial mixed use and multifamily facilities should have sufficient storage for bicycles and other active transportation modes.</u> | <u>7.9 New or remodeled public institution, commercial mixed use and multifamily facilities should have sufficient storage for bicycles and other active transportation modes.</u> | No change |
| Transportation Policy 10.6 | <u>10.6 Levels of service for pedestrian, bicycle, and transit transportation modes should be established.</u> | <u>10.6 Levels of service for pedestrian, bicycle, and transit transportation modes should be established.</u> | No change |
| Transportation Policy 12.4 | <u>12.4 Strive to build community through the in-person interactions facilitated by active transportation at community connection points (schools, library, community centers, bikeshare hubs, etc.)</u> | <u>12.4 Strive to build community through the in-person interactions facilitated by active transportation at community connection points (schools, library, community centers, bikeshare hubs, etc.)</u> | No change |
| Transportation Policy 12.5 | <u>12.5 Areas near schools and commercial areas should have higher levels of service for pedestrians, bicycles, and transit.</u> | <u>12.5 Prioritize areas near schools and commercial areas for a higher level of service for pedestrians, bicycles, and transit.</u> | City Council direction on October 15, 2019 |



BUSINESS OF THE CITY COUNCIL CITY OF MERCER ISLAND

AB 5630
December 3, 2019
Regular Business

AGENDA BILL INFORMATION

| | | |
|----------------------------|---|---|
| TITLE: | AB 5630: 2019 Minor Code Amendments (1st | <input type="checkbox"/> Discussion Only |
| RECOMMENDED ACTION: | Set Ordinance No. 19C-21 for second reading and adoption on the December 10, 2019 Consent Calendar. | <input checked="" type="checkbox"/> Action Needed: <input checked="" type="checkbox"/> Motion <input type="checkbox"/> Ordinance <input type="checkbox"/> Resolution |

| | |
|-------------------------------|--|
| DEPARTMENT: | Community Planning and Development |
| STAFF: | Evan Maxim, Director |
| COUNCIL LIAISON: | n/a |
| EXHIBITS: | 1. Draft Ordinance No. 19C-21 |
| CITY COUNCIL PRIORITY: | 3. Support the Leadership Team's Work Plan |

| | |
|-------------------------------|--------|
| AMOUNT OF EXPENDITURE | \$ n/a |
| AMOUNT BUDGETED | \$ n/a |
| APPROPRIATION REQUIRED | \$ n/a |

SUMMARY

BACKGROUND

Following the adoption of Ordinance No. 17C-015 (Residential Development Standards code update), the City Council requested that Community Planning and Development (CPD) staff to periodically review and update the zoning code to promote clarity and internal consistency. CPD staff maintain a running list of code amendment items, which is generated through application of the code during project review.

In sum, the 2019 "Minor" Code Amendments (ZTR19-001) contain code amendments related to the following subjects:

1. Clarification of side yard terminology;
2. Clarification of roof pitch when allowing eaves into non-conforming setbacks;
3. An allowance for a driveway that exceeds 30 inches in height in a yard where necessary to provide vehicle access to the house;
4. Establishing a height limit in the MF-2L zone and a methodology for calculating the height limit in the MF-2, MF-3, PBZ, and CO zones;
5. Correcting a grammatical error that indicated that all development should be avoided;

6. Revising the term used to describe the City’s determination of the amount of required parking from “variance” to “modification”;
7. Allowing the City to issue a decision on a project or permit review when requests for a correction are repeatedly not addressed;
8. Correcting a cross reference in design review;
9. Creating a definition of irregular lot;
10. Amending the definition of lot coverage to include eaves and roof overhangs; and,
11. Creating a process whereby any person may propose the docketing of a code amendment for review by the City Council.

MINOR AMENDMENTS

Nos. 1 through 10 (Pages 1 through 6 and page 9): Amendment items 1 through 10 represent minor changes to the regulations initiated as part of the CPD staff’s periodic review. The proposed code amendments are not intended to represent substantive changes to policy.

No. 11 (Pages 6 through 8): On July 16, 2019 the City received a “Petition for Review” before the Growth Management Hearings Board (GMHB), which was assigned Case No. 19-3-0014. The petition highlighted that the City’s code language regarding the application for code amendments, and who can initiate them, is not in alignment with state law. RCW 36.70A.470 requires cities to include code provisions that provide:

“a procedure for any interested person, including applicants, citizens, hearing examiners, and staff of other agencies, to suggest plan or development regulation amendments. The suggested amendments shall be docketed and considered on at least an annual basis.”

In practice, the City currently allows any interested person to apply for a code amendment. The application for a code amendment involves the payment of a fee, and preparation of an application form and proposed amendment. The fee is intended to cover 80% of the City’s administrative costs for processing the amendment. The City does not currently have a process for any interested person to “suggest” an amendment without paying a fee.

Following consultation with the City Attorney’s office, the City has drafted a process that will allow any person to suggest a code amendment through the same type of docketing process used for Comprehensive Plan amendments. Suggested amendments will require sufficient description by the person suggesting the amendment for the City Council to determine whether further action is appropriate. A person suggesting the code amendment will not be required to pay a fee. The proposed code amendment docketing process is in addition to the City’s current code amendment “application” process.

Following a public hearing on September 18, 2019, the Planning Commission has recommended that the City Council adopt the minor code amendments.

RECOMMENDATION

Set Ordinance No. 19C-21 for second reading and adoption on the December 10, 2019 Consent Calendar.

**ORDINANCE NO. 19C-21
CITY OF MERCER ISLAND**

AN ORDINANCE OF THE CITY OF MERCER ISLAND, WASHINGTON, AMENDING TITLE 19 MERCER ISLAND CITY CODE TO CLARIFY DEVELOPMENT AND ADMINISTRATIVE STANDARDS AND TO CREATE A PROCEDURE TO DOCKET AND CONSIDER SUGGESTED AMENDMENTS TO DEVELOPMENT REGULATIONS, PROVIDING FOR SEVERABILITY, AND ESTABLISHING AN EFFECTIVE DATE.

WHEREAS, the Mercer Island City Code (MICC) establishes development regulations that are intended to result in the implementation of the Mercer Island Comprehensive Plan pursuant to RCW 36.70A.040; and,

WHEREAS, the Mercer Island City Council determined that amendments to the development regulations were necessary to ensure that residential development was occurring consistently with the provisions of the Mercer Island Comprehensive Plan; and,

WHEREAS, the Mercer Island City Council directed the Planning Commission to periodically review Title 19 MICC and recommend amendments to clarify the regulations to the City Council; and,

WHEREAS, the Growth Management Act requires the City to adopt procedures to docket and consider suggested development regulation amendments; and

WHEREAS, the Mercer Island Planning Commission held a public hearing on September 18, 2019, and considered public comment received prior to the close of the public hearing; and

WHEREAS, the Mercer Island Comprehensive Plan Land Use Element establish numerous goals and policies that are implemented through the adoption of revised development standards; and,

WHEREAS, a SEPA Determination of Non-Significance was issued by the City on August 12, 2019; and,

WHEREAS, the Washington Department of Commerce granted expedited review of the proposed amendments to the development regulations on September 26, 2019;

NOW, THEREFORE, THE CITY COUNCIL OF THE CITY OF MERCER ISLAND, WASHINGTON, DO ORDAIN AS FOLLOWS:

Section 1: **Adoption of amendments to Title 19 of the Mercer Island City Code.** The amendments to Title 19 of the Mercer Island City Code as set forth in Attachment “A” to this ordinance are hereby adopted.

Section2: **Codification of the regulations.** The City Council authorizes the Community Planning and Development Director and the City Clerk to correct errors in Attachment A, codify the regulatory provisions of the amendment into Title 19 of the Mercer Island City Code, and publish the amended code.

Section 3. Interpretation. The City Council authorizes the Community Planning and Development Director to adopt administrative rules, interpret, and administer the amended code as necessary to implement the legislative intent of the City Council.

Section 4: Severability. If any section, sentence, clause, or phrase of this ordinance or any city code section amended hereby should be held to be invalid or unconstitutional by a court of competent jurisdiction, such invalidity or unconstitutionality shall not affect the validity of any other section, sentence, clause, or phrase of this ordinance or the amended code section.

Section 6: Publication and Effective Date. A summary of this Ordinance consisting of its title shall be published in the official newspaper of the City. This Ordinance shall take effect and be in full force five days after the date of publication.

PASSED by the City Council of the City of Mercer Island, Washington at a special meeting on the 10th day of December 2019 and signed in authentication of its passage.

CITY OF MERCER ISLAND

Debbie Bertlin, Mayor

APPROVED AS TO FORM:

ATTEST:

Bio Park, Interim City Attorney

Deborah A. Estrada, City Clerk

Date of Publication: _____

ZTR19-001 Minor Code Amendments

Planning Commission Recommendation

- 1 RESIDENTIAL
- 2 19.02.020 Development standards
- 3
- 4 MULTIPLE-FAMILY
- 5 19.03.010 Multiple-family
- 6
- 7 COMMERCIAL
- 8 19.04.010 Planned business zone - PBZ
- 9 19.04.020 Commercial offices
- 10 19.04.050 Business - B
- 11
- 12 PROPERTY DEVELOPMENT
- 13 19.09.100 Preferred practices
- 14
- 15 ADMINISTRATION
- 16 19.15.030 Land use review types
- 17 19.15.110 Response to comments and extensions
- 18 19.15.220 Design review and the design commission
- 19 19.15.230 Comprehensive plan amendments
- 20 19.15.250 Code amendment
- 21 19.15.260 Review procedures for comprehensive plan amendments, reclassification of property,
- 22 and code amendments
- 23 DEFINITIONS
- 24 19.16.010(L) "L" definitions
- 25
- 26 "Normal Text" is existing code language
- 27 "~~Strikethrough Text~~" is existing code language that will be deleted
- 28 "Underline Text" is new code language that will be added
- 29 "... " indicates that existing code language is omitted and will not be amended
- 30
- 31 **MICC 19.02.020 Development standards**
- 32 ...
- 33 C. Yard Requirements.
- 34 1. Minimum. Except as otherwise provided in this section, each lot shall have front, rear, and side
- 35 yards not less than the depths or widths following:
- 36 a. Front yard depth: 20 feet or more.
- 37 b. Rear yard depth: 25 feet or more.
- 38 c. Side yards shall be provided as follows:
- 39 i. *Total ~~Depth~~Width.*
- 40 (a) For lots with a lot width of 90 feet or less, the sum of the side yards' ~~depth~~width
- 41 shall be at least 15 feet.
- 42 (b) For lots with a lot width of more than 90 feet, the sum of the side yards' ~~depth~~
- 43 width shall be a width that is equal to at least 17 percent of the lot width.
- 44 ii. *Minimum Side Yard ~~Depth~~Width.*

ZTR19-001 Minor Code Amendments

Planning Commission Recommendation

1 ~~(a)~~The minimum side yard depth ~~abutting an interior lot line~~ is five feet or 33
 2 percent of the aggregate side yard total ~~depth~~width, whichever is greater.
 3 ~~(a) The minimum side yard depth abutting a street is five feet.~~

3. Intrusions into Required Yards.

a. Minor Building Elements.

- i. Except as provided in subsection (C)(3)(a)(ii) of this section, porches, chimney(s) and fireplace extensions, window wells, and unroofed, unenclosed outside stairways and decks shall not project more than three feet into any required yard. Eaves shall not protrude more than 18 inches into any required yard.
- ii. No penetration shall be allowed into the minimum side yard setback abutting an interior lot line except where an existing flat-roofed house has been built to within 18 inches of the interior side yard setback line and the roof is changed to a pitched roof with a ~~minimum~~ pitch of ~~- 2:12 or steeper~~4:12, the eaves up to 18 inches may penetrate ~~up to 18 inches~~ into the side yard setback.

b. Hardscape and Driveways.

- i. Hardscape and driveways not more than 30 inches above existing grade or finished grade, whichever is lower, may be located in any required yard, provided that driveways may exceed the 30-inch limit when a permit applicant demonstrates the proposed height is the minimum feasible to meet the standards in MICC 19.09.040.

MICC 19.03.010

E. Building Height Limit.

1. ~~MF-2L: No building shall exceed 24 feet or two stories in height (excluding daylight basements), whichever is less, except appurtenances may extend to a maximum of five feet above the height allowed for the main structure.~~

~~2.1.~~ MF-2, MF-3: No building shall exceed 36 feet or three stories in height, whichever is less, except appurtenances may extend to a maximum of five feet above the height allowed for the main structure.

2. Building height for buildings within the MF-2 and MF-3 zone shall be calculated using the method described in MICC 19.11.030(A)(3).

3. Buildings within the MF-2L shall meet the following standards:

- a. No building shall exceed 24 feet in height above the average building elevation to the highest point of the roof; and
- b. The maximum building facade height on the downhill side of a sloping lot shall not exceed 24 feet in height. The building facade height shall be measured from the existing grade or finished grade, whichever is lower, at the furthest downhill extent of the proposed building, to the top of the exterior wall facade supporting the roof framing, rafters, trusses, etc.

ZTR19-001 Minor Code Amendments

Planning Commission Recommendation

MICC 19.04.010

E. *Development Standards – Nonresidential.*

1. *Building Height Limit.* No nonresidential structure shall exceed 36 feet in height, [calculated using the method described in MICC 19.11.030\(A\)\(3\).](#)

2. *Minimum Parcel Area Requirements.* There shall be no minimum parcel size for nonresidential uses.

3. *Parking Requirements.* All nonresidential uses permitted in this zone shall comply with the parking requirements set out in MICC 19.04.040.

F. *Development Standards – Residential.*

1. *Criteria for Residential Units.* The intent for residential development in the PBZ is for a variety of housing units smaller in size than the surrounding neighborhood, developed in a planned and coordinated manner. Except as otherwise provided in this section, no residential units shall be located under or over another unit or within a commercial structure.

2. *Building Height Limit.* No residential building shall exceed 30 feet in height, [calculated using the method described in MICC 19.11.030\(A\)\(3\).](#) Antennas, lightning rods, plumbing stacks, flagpoles, electrical service leads, chimneys and fireplaces and other similar appurtenances may extend to a maximum of five feet above the height allowed for the main structure.

MICC 19.04.020

C. *Building Height Limit.*

1. Structures shall not exceed 36 feet in height, [calculated using the method described in MICC 19.11.030\(A\)\(3\).](#)

MICC 19.04.050

D. *Building Height Limit.* Maximum allowable building height shall be [the lesser of 1\) three stories or 2\) 36 feet, -calculated using the method described in MICC 19.11.030\(A\)\(3\).](#) ~~whichever is less.~~

MICC 19.09.100

B. Development, including roads, walkways and parking areas, in critical areas, should be avoided, or if not avoided, adverse impacts to critical areas will be mitigated to the greatest extent reasonably feasible.

ZTR19-001 Minor Code Amendments

Planning Commission Recommendation

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MICC 19.15.030 Table A

| Type I | Type II | Type III | Type IV |
|--|--|--|---|
| Table A. Land Use Review Type | | | |
| <ul style="list-style-type: none"> • Home business • Seasonal development limitation waiver • Nonmajor single-family dwelling building permits • Tree removal permit • Right-of-way permit • Special needs group housing safety determination • Tenant improvement/change of use • Shoreline exemption¹ • Critical areas determination (steep slope alteration) • Final short plat • Temporary commerce on public property • Site development permits • Transportation concurrency certificate | <ul style="list-style-type: none"> • Modified wireless communication facilities (6409 per 47CFR 1.40001) • Lot line revision • Setback deviations • Final plat^{2,3} • Code official design review • Accessory dwelling unit • Parking variances modification (reviewed by city engineer) | <ul style="list-style-type: none"> • New and modified wireless (non-6409) eligible facility • SEPA threshold determination • Critical areas determination (wetland/watercourse buffer averaging/reduction) • Temporary encampment⁴ • Short plat alteration and vacations • Preliminary short plat • Development code interpretations • Major single-family dwelling building permit⁵ • Shoreline substantial development permit¹ • Shoreline revision (substantial development)¹ | <ul style="list-style-type: none"> • Preliminary long plat approval • Conditional use permit • Variance • Critical areas reasonable use exception • Long plat alteration and vacations • Parking variances modifications (reviewed by design commission) • Variance from short plat acreage limitation • Wireless communication facility height variance • Planned unit development • Design commission design review • Permanent commerce on public property • Shoreline conditional use permit (SCUP)⁶ • Shoreline variance⁶ • Shoreline revision (variance and SCUP) |

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ZTR19-001 Minor Code Amendments

Planning Commission Recommendation

MICC 19.15.110

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5 C. *Deadline Extension.* Applicants may request an extension to provide requested materials. Extension
6 requests shall be in writing, shall include a basis for the extension and shall be submitted in writing
7 prior to expiration of the time limit. The code official is authorized to extend the time limit in
8 writing. There is no limit to the number of extensions an applicant may be granted, however, the
9 total time limit for a response shall not exceed 180 days unless there is an extenuating
10 circumstance. An extenuating circumstance must be unexpected and beyond the control of the
11 applicant.

12 ~~C.D.~~ *Limit on Number of Review Cycles.* The code official may issue a decision when multiple
13 requests for the same information have remained unaddressed by materials submitted by the
14 applicant. The official or entity shall provide written notification to the applicant, informing them
15 that a decision will be issued and providing the opportunity for one set of information to be
16 submitted before the decision is issued. The intent of this provision is to allow the code official to
17 issue a decision when the content of submittal materials demonstrates an inability or unwillingness
18 to meet applicable code requirements after repeated requests by the City. It is not the intent of this
19 section to limit good faith efforts to meet code requirements by submitting new information in
20 pursuit of approval.

MICC 19.15.220

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28 C. *Design Review Procedure.*

29 1. *General.*

30 a. *Intent.* The intent of the design review process is to ensure that regulated development in
31 all land use zones complies with design objectives and standards established in Chapters
32 19.11 and 19.12 MICC.

33 b. *Scope.* No building permit or other required permit shall be issued by the city for
34 development of any regulated improvement without prior approval of the design
35 commission or code official as authorized pursuant to this chapter. Deviations from a plan
36 approved by the design commission or code official shall be permitted only upon the filing
37 and approval of an amended plan. In no instance shall the design commission's or code
38 official's action conflict with the city's development code or other applicable city ordinances
39 or with state or federal requirements. Certain development and activities that do not
40 require a permit are subject to design review as provided in subsection (C)(1)(c) of this
41 section.

42 c. *Review Authority.*

43 i. The following development proposals shall require design commission review:

44 (a) New buildings;

45 (b) Any additions of gross floor area to an existing building(s);

ZTR19-001 Minor Code Amendments
Planning Commission Recommendation

- (c) Any alterations to an existing building that will result in a change of 50 percent, or more, of the exterior surface area;
- (d) Any alterations to a site, where the alteration will result in a change to the site design that affects more than 50 percent of the development proposal site; and
- (e) Any alterations to existing facades, where the building is identified by the city as an historic structure.

ii. All other development proposals requiring design review and not requiring design commission review under [MICC 19.15.220\(C\)\(1\)\(c\)\(i\)subsection A](#) of this [subsection](#) shall be reviewed by the code official. The code official shall have the authority to determine that an application normally reviewed by code official shall require design commission review and approval, based on factors such as the scope, location, context, and visibility of the proposed change or modification; and

19.15.230 **Comprehensive plan amendments [and docketing procedures](#).**

D. Docketing of Proposed Amendments. For purpose of this section, docketing refers to compiling and maintaining a list of suggested changes to the comprehensive plan [or code](#) -in a manner that will ensure such suggested changes will be considered by the city and will be available for review [on the City's website](#) by the public. The following process will be used to create the docket:

1. Preliminary Docket Review. By September 1, the city will issue notice of the annual comprehensive plan [and code](#) amendment cycle for the following calendar year. The amendment request deadline is October 1. Proposed amendment requests received after October 1 will not be considered for the following year's comprehensive plan [and code](#) amendment process but will be held for the next eligible comprehensive plan [and code](#) amendment process.
 - a. The code official shall compile and ~~maintain~~ [post](#) for public review a list of suggested amendments and identified deficiencies as received throughout the year.
 - b. The code official shall review all complete and timely filed applications [and suggestions](#) proposing amendments to the comprehensive plan [or code](#) -and place these applications on the preliminary docket along with other city-initiated amendments to the comprehensive plan.
 - c. The planning commission shall review the preliminary docket at a public meeting and make a recommendation on the preliminary docket to the city council each year.
 - d. The city council shall review the preliminary docket at a public meeting. By December 31, the city council shall establish the final docket based on the criteria in subsection E of this section. Once approved, the final docket defines the work plan and resource needs for the following year's comprehensive plan amendments.
2. Final Docket Review.
 - a. Placement on the final docket does not mean a proposed amendment will be approved. The purpose of the final docket is to allow for further analysis and consideration by the city.

ZTR19-001 Minor Code Amendments

Planning Commission Recommendation

b. All items on the final docket shall be considered concurrently so that the cumulative effect of the various proposals can be ascertained. Proposed amendments may be considered at separate meetings or hearings, so long as the final action taken considers the cumulative effect of all proposed amendments to the comprehensive plan.

c. The code official shall review and assess the items placed on the final docket and prepare recommendations for each proposed amendment. The code official shall be responsible for developing an environmental review of the combined impacts of all proposed amendments on the final docket, except that applicants seeking a site-specific amendment shall be responsible for submittal of a SEPA environmental checklist and supporting information. The applicant will need to submit SEPA and any other accompanying legislative actions such as a rezone or a code amendment at this time. The code official may require an applicant to pay for peer review and/or additional resources needed to review the proposal. The code official shall set a date for consideration of the final docket by the planning commission and timely transmit the staff recommendation prior to the scheduled date.

d. The planning commission shall review the proposed amendments contained in the final docket based on the criteria set forth in subsection (FE)(1) of this section. The planning commission shall hold at least one public hearing on the proposed amendments. The planning commission shall make a recommendation on the proposed amendments and transmit the recommendation to the city council.

e. After issuance of the planning commission's recommendation, the code official shall set a date for consideration of the final docket by the city council. The city council shall review the proposed amendments taking into consideration the recommendations of the planning commission and code official. The city council may deny, approve, or modify the planning commission's recommendations consistent with the criteria set forth in subsection (DF)(1) of this section. The city council's establishment of a final docket of proposed amendments is not appealable.

f. The planning commission and the city council may hold additional public hearings, meetings, or workshops as warranted by the proposed amendments.

E. Docketing Criteria. The following criteria shall be used to determine whether a proposed amendment is added to the final docket in subsection D of this section:

1. The request has been filed in a timely manner, and either:
 - a. State law requires, or a decision of a court or administrative agency has directed, such a change; or
 - b. All of the following criteria are met:
 - i. The proposed amendment presents a matter appropriately addressed through the comprehensive plan or the code;
 - ii. The city can provide the resources, including staff and budget, necessary to review the proposal, or resources can be provided by an applicant for an amendment;
 - iii. The proposal does not raise policy or land use issues that are more appropriately addressed by an ongoing work program item approved by the city council;

ZTR19-001 Minor Code Amendments

Planning Commission Recommendation

- iv. The proposal will serve the public interest by implementing specifically identified goals of the comprehensive plan or a new approach supporting the city's vision; and
- v. The essential elements of the proposal and proposed outcome have not been considered by the city council in the last three years. This time limit may be waived by the city council if the proponent establishes that there exists a change in circumstances that justifies the need for the amendment.

19.15.250 Code amendment.

A. Purpose. The purpose of this section is to establish the process and criteria for amendment of this code.

B. Process. ~~Zoning c~~Code amendments shall be considered as provided in MICC 19.15.260.

C. Initiation of ~~Zoning~~Code Amendment Request.

1. ~~A zoning~~ code amendment request may be initiated at any time by the city council, planning commission, or code official.

2. A code amendment request may be initiated by any interested person as follows:

a) Suggestion. A code amendment may be suggested by any interested person. The suggested amendments shall be docketed pursuant to MICC 19.15.230(D) and considered on at least an annual basis.

b) Application. An application for a code amendment may be submitted by any interested person. An application for a code amendment shall be accompanied by the filing fee established by resolution.

c) All proposed code amendments, whether initiated by suggestion or application, shall be accompanied by an application form and shall require a detailed description of the proposed amendment in nontechnical terms.

3. Applications for a code amendment shall not be subject to the docketing procedures of MICC 19.15.230(D).

4. Multiple code amendments may be consolidated for review and action at the city's discretion.

D. Criteria. The city may approve or approve with modifications a proposal to amend ~~the text of~~ this code only if:

- 1. The amendment is consistent with the comprehensive plan; and
- 2. The amendment bears a substantial relation to the public health, safety, or welfare; and
- 3. The amendment is in the best interest of the community as a whole.

E. Codification. Following approval of an amendment, the city shall amend this code to reflect the amendment.

ZTR19-001 Minor Code Amendments
Planning Commission Recommendation

1 **19.15.260 Review procedures for comprehensive plan amendments, reclassification of property,**
2 **and code amendments.**
3

4 A. The city shall issue a notice for comprehensive plan amendments, reclassifications of property, and
5 zoning code text amendments as described in MICC 19.15.230, 19.15.240, and 19.15.250. Notice
6 shall be provided in the weekly CPD bulletin, [a newspaper of general circulation](#), made available to
7 the general public upon request, and, if the proposed amendment will affect a specific property or
8 defined area of the city, mailed to all property owners within 300 feet of the affected property or
9 defined area, and posted on the site in a location that is visible to the public right-of-way.

- 10 1. The notice shall include the following information:
 - 11 a. The name of the party proposing the proposed amendment or change;
 - 12 b. The location and description of the project, if applicable;
 - 13 c. The requested actions and/or required studies;
 - 14 d. The date, time, and place of the open record hearing;
 - 15 e. Identification of environmental documents, if any;
 - 16 f. A statement of the public comment period which shall not be less than 30 days. The city
17 shall accept public comments at any time prior to the closing of the record of an open
18 record predecision hearing; and a statement of the rights of individuals to comment on the
19 application, receive notice and participate in any hearings, request a copy of the decision
20 once made and any appeal rights;
 - 21 g. The city staff contact and contact information;
 - 22 h. The identification of other reviews or permits that are associated with the review of the
23 proposed comprehensive plan, zoning text, or zoning map amendment, to the extent known
24 by the city;
 - 25 i. A description of those development regulations used in determining consistency of the
26 review with the city's comprehensive plan;
 - 27 j. A link to a website where additional information about the project can be found; and
 - 28 k. Any other information that the city determines appropriate.
- 29 2. Timing of Notice. The city shall provide the notice at least 30 days prior to the hearing.
- 30 3. The city shall accept public comments at any time prior to the closing of the record of an open
31 record public hearing.

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33 **MICC 19.16.010(L)**

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37 [Lot, Irregular: A legally established lot that 1\) is not rectangular and 2\) does not meet minimum width,](#)
38 [depth, and area standards required by the zone in which the lot is located](#)

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42 Lot Coverage Maximum: The maximum area of a residentially zoned lot that may be covered by a
43 combination of buildings, [including eaves and roof overhangs](#), and vehicular driving surfaces.
44



**BUSINESS OF THE CITY COUNCIL
CITY OF MERCER ISLAND**

**AB 5642
December 3, 2019
Regular Business**

AGENDA BILL INFORMATION

| | | |
|----------------------------|--|---|
| TITLE: | AB 5642: Design Commission Vacancy Appointment | <input type="checkbox"/> Discussion Only |
| RECOMMENDED ACTION: | Appoint the Mayor and Deputy Mayors recommendation for appointment to the Design Commission. | <input checked="" type="checkbox"/> Action Needed: <input checked="" type="checkbox"/> Motion <input type="checkbox"/> Ordinance <input type="checkbox"/> Resolution |

| | | | |
|-------------------------------|--|-----------------|-----------------|
| DEPARTMENT: | City Council & City Manager | | |
| STAFF: | Mayor Debbie Bertlin, Deputy Mayor Salim Nice, & Deb Estrada, City Clerk | | |
| COUNCIL LIAISON: | n/a | Choose an item. | Choose an item. |
| EXHIBITS: | n/a | | |
| CITY COUNCIL PRIORITY: | n/a | | |

| | |
|-------------------------------|--------|
| AMOUNT OF EXPENDITURE | \$ n/a |
| AMOUNT BUDGETED | \$ n/a |
| APPROPRIATION REQUIRED | \$ n/a |

SUMMARY

BACKGROUND

Each March the City begins its annual recruitment process for filling positions whose terms are expiring on the City’s advisory boards and commissions. Position vacancies were advertised in the Mercer Island Reporter, the MI Weekly, and on the City website. The application deadline was also announced at four meetings of the City Council and emails were sent to current board and commission members encouraging recruitment. In response to outreach efforts, there was one request for reappointment to the Design Commission and no additional applications were received, leaving one vacancy.

In consultation with the Mayor and Deputy Mayor, staff recommended contacting past Design Commission members with landscape architecture experience that wished to serve again. At which point, June 24 was set as the new application deadline. Two applications were subsequently received and neither applicant had landscape experience; however, it was noted that Lara Sanderson had previous experience as a Design Commissioner.

Staff met with the Mayor and Deputy Mayor and discussed the Design Commission’s current membership (3 Lay Members, 3 architects, and 1 vacancy). Pursuant to Mercer Island City Code 3.34.030, the Design Commission membership should achieve a broad-based representation by appointing members that specialize in “architecture, landscape architecture, urban planning, and civil engineering.” To achieve the

desired representation, it was agreed that the application deadline should be extended again. The new application deadline was therefore extended to mid-September.

NEXT STEPS

Four applications were received by the September 18 deadline and forwarded to City Council on September 23 and again on November 26 for review. All applications are on file with the City Clerk.

The Mayor and Deputy Mayor, in consultation with other Councilmembers, put forth a recommendation to appoint Lara Sanderson to Position No. 2 on the Design Commission, which expires May 31, 2023.

RECOMMENDATION

Appoint the Mayor and Deputy Mayor's recommendation of Lara Sanderson to Position No. 2 on the Design Commission.



PLANNING SCHEDULE

Item 12.

Please email the City Manager & City Clerk when an agenda item is added, moved, or removed.

Special Meetings and Study Sessions begin at 6:00 pm. Regular Meetings begin at 7:00 pm. Items are not listed in any particular order. Agenda items & meeting dates are subject to change.

| DECEMBER 3 | | DUE TO: | 11/22 D/P | 11/25 FN | 11/25 CA | 11/26 Clerk |
|---|--|---------|-----------|-----------------------|----------|-------------|
| ABSENCES: | | | | | | |
| ITEM TYPE TIME TOPIC | | | | STAFF | | |
| EXECUTIVE SESSION (5:30-6:00 pm) | | | | | | |
| 30 | Executive Session for planning or adopting the strategy or position to be taken by the City Council during the course of any collective bargaining, professional negotiations, or grievance or mediation proceedings, or reviewing the proposals made in the negotiations or proceedings while in progress pursuant to RCW 42.30.140(4)(b) for approximately 30 minutes. | | | | | |
| STUDY SESSION (6:00-7:00 pm) | | | | | | |
| 60 | AB 5639: Code of Ethics Revisions (2nd Reading) | | | Ali Spietz | | |
| SPECIAL BUSINESS (7:00 pm) | | | | | | |
| 15 | Special Recognition | | | Mayor Bertlin | | |
| CONSENT CALENDAR | | | | | | |
| -- | AB 5636: CPD Development and Construction Permit Fees Update (Resolution No. 1567) | | | Alison VanGorp | | |
| -- | AB 5635: Public Institution Code Amendment (2nd Reading & Adoption) | | | Evan Maxim | | |
| -- | AB 5641: Accept MIYFS Foundation Funds for Geriatric Specialist | | | Derek Franklin | | |
| REGULAR BUSINESS | | | | | | |
| 30 | PUBLIC HEARING (Legal Notice: 10/30) AB 5637: Public Hearing: Interim Design and Concealment Standards for Small Cell Facilities Deployment Ordinance (Extension and Adoption) | | | Evan Maxim | | |
| 30 | AB 5629: Aubrey Davis Park Master Plan Adoption | | | Ryan Daly & Paul West | | |
| 30 | AB 5631: 2019 Comprehensive Plan Amendments (2 nd Reading & Adoption) | | | Evan Maxim | | |
| 30 | AB 5630: Minor Code Amendments (1 st Reading) | | | Evan Maxim | | |
| 10 | AB 5642: Design Commission Vacancy Appointment | | | Deb Estrada | | |
| COUNCIL REPORTS/REGIONAL COMMITTEE REPORTS | | | | | | |
| 10 | SCA Legislative Report | | | Councilmember Wong | | |
| EXECUTIVE SESSION | | | | | | |
| 30 | Executive Session to discuss with legal counsel pending or potential litigation for approximately 30 minutes pursuant to RCW 42.30.110(1)(i) | | | | | |

| DECEMBER 10 – 7:00 PM | | | | | | |
|-----------------------------------|---|--|--|------------|-------------------|--|
| Legal Notice: published 12/4 | | | | | | |
| SPECIAL BUSINESS (7:00 pm) | | | | | | |
| 30 | Resolutions of Appreciation for Mayor Debbie Bertlin, Councilmember Bruce Basset and Councilmember Dave Wisenteiner | | | | Deputy Mayor Nice | |
| CONSENT CALENDAR | | | | | | |
| -- | AB 5643: Code of Ethics Revisions (3 rd Reading & Adoption) | | | Ali Spietz | | |
| -- | AB 5638: Minor Code Amendments (2 nd Reading & Adoption) | | | Evan Maxim | | |
| REGULAR BUSINESS | | | | | | |

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|--------------------------|---|------------|-----------------|
| 30 | AB 5624: 2020 Comprehensive Plan Amendment Docket | Evan Maxim | <i>Item 12.</i> |
| EXECUTIVE SESSION | | | |
| | | | |

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|------------------------------|--|--|--|--|--|
| DECEMBER 17 | | | | | |
| Legal Notice: published 12/4 | | | | | |
| CANCELED | | | | | |

OTHER ITEMS TO BE SCHEDULED:

- Open Space Vegetation Management Report – A. Sommargren
- Comprehensive Mobility Plan (ST Settlement) – K. Taylor
- Utility Projects Update – J. Kintner

2020 Agenda Items:

- Process to Appoint Permanent City Manager – K. Segle
- Pavement 101 (Q1)
- Stormwater Dissolved Metals Testing Report (Q2)
- Joint Meeting with Parks & Recreation Commission (Mar)
- Code Compliance and Short-Term Rentals Discussion – A. Van Gorp
- Zayo Franchise – B. Park
- Puget Sound Emergency Radio Network (PSERN) Operator Interlocal Agreement – D. Jokinen

MISD BOARD JOINT MEETING DATES:

- Thursday, April 23, 2020, 4:45-6:00 pm

2020

| JANUARY 7 | | DUE TO: | 12/27 D/P | 12/30 FN | 12/30 CA | 12/31 Clerk |
|-----------------------------------|---|----------------|------------------|-----------------|-----------------|--------------------|
| ABSENCES: | | | | | | |
| ITEM TYPE TIME TOPIC | | | | STAFF | | |
| SPECIAL BUSINESS (7:00 pm) | | | | | | |
| 30 | AB xxxx: Councilmember Oaths of Office and Mayor and Deputy Mayor Elections | | | | Deb Estrada | |
| 15 | Flash Family Inspiration Award | | | | Cindy Goodwin | |
| CONSENT CALENDAR | | | | | | |
| -- | AB xxxx: Roadside Shoulder Improvements, WMW Phase 2, Contract Award | | | | Clint Morris | |
| REGULAR BUSINESS | | | | | | |
| 90 | AB xxxx: CIP Projects Update and Project Management Report | | | | LaJuan Tuttle | |
| 30 | AB xxxx: Rooftop Railings Code Amendment (1 st Reading) | | | | Evan Maxim | |
| EXECUTIVE SESSION | | | | | | |
| 45 | To discuss pending or potential litigation with legal counsel pursuant to RCW 42.30.110(1)(i) for approximately 45 minutes. | | | | | |

| JANUARY 21 | | DUE TO: | 1/10 D/P | 1/13 FN | 1/13 CA | 1/14 Clerk |
|-------------------------------------|--|----------------|-----------------|----------------|----------------|-------------------|
| ABSENCES: | | | | | | |
| ITEM TYPE TIME TOPIC | | | | STAFF | | |
| STUDY SESSION (5:00-7:00 pm) | | | | | | |
| 60 | AB xxxx: Community Needs Assessment & Human Services Funding | | | | Cindy Goodwin | |
| 615 | AB xxxx: MIFYS Foundation Strategic Plan Report | | | | Cindy Goodwin | |

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|-----------------------------------|---|-------------------------|
| SPECIAL BUSINESS (7:00 pm) | | Item 12. |
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| CONSENT CALENDAR | | |
| -- | AB xxxx: Rooftop Railings Code Amendment (2 nd Reading & Adoption) | Evan Maxim |
| REGULAR BUSINESS | | |
| | AB xxxx: Community Facility – Planning Commission Report | Evan Maxim |
| | AB xxxx: Discussion on Long-Term Financial Forecast | LaJuan Tuttle/Jessi Bon |
| EXECUTIVE SESSION | | |
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| JANUARY 24-25 (FRIDAY-SATURDAY) | |
| LEGAL NOTICE | |
| 2020 City Council Planning Session | |

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|-------------------------------------|---|-------------|-------------|--------------|-------------|
| FEBRUARY 4 | DUE | 1/24 | 1/27 | 1/27 | 1/28 |
| ABSENCES: | TO: | D/P | FN | CA | Clerk |
| ITEM TYPE TIME TOPIC | | | | STAFF | |
| STUDY SESSION (5:30-7:00 pm) | | | | | |
| 90 | Regional Growth Update - Joint Study Session with Planning Commission | | | Evan Maxim | |
| SPECIAL BUSINESS (7:00 pm) | | | | | |
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| CONSENT CALENDAR | | | | | |
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| EXECUTIVE SESSION | | | | | |
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| FEBRUARY 18 | DUE | 2/7 | 2/10 | 2/10 | 2/11 |
| ABSENCES: | TO: | D/P | FN | CA | Clerk |
| ITEM TYPE TIME TOPIC | | | | STAFF | |
| STUDY SESSION (6:00-7:00 pm) | | | | | |
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| SPECIAL BUSINESS (7:00 pm) | | | | | |
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| CONSENT CALENDAR | | | | | |
| -- | AB xxxx: Rooftop Railings Code Amendment (2 nd Reading & Adoption) | | | Evan Maxim | |
| REGULAR BUSINESS | | | | | |
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| EXECUTIVE SESSION | | | | | |
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616

EXECUTIVE SESSION

| MARCH 3 | | DUE | 2/21 | 2/24 | 2/24 | 2/25 |
|-------------------------------------|---|------------|-------------|-------------|--------------|--------------|
| ABSENCES: | | TO: | D/P | FN | CA | Clerk |
| ITEM TYPE TIME TOPIC | | | | | STAFF | |
| STUDY SESSION (5:30-7:00 pm) | | | | | | |
| 90 | PROS Plan Kick-Off Meeting – Joint Study Session with Parks and Recreation Commission | | | | Ryan Daly | |
| SPECIAL BUSINESS (7:00 pm) | | | | | | |
| CONSENT CALENDAR | | | | | | |
| REGULAR BUSINESS | | | | | | |
| EXECUTIVE SESSION | | | | | | |

| MARCH 17 | | DUE | 3/6 | 3/2 | 3/2 | 3/3 |
|-------------------------------------|--|------------|------------|------------|--------------|--------------|
| ABSENCES: | | TO: | D/P | FN | CA | Clerk |
| ITEM TYPE TIME TOPIC | | | | | STAFF | |
| STUDY SESSION (6:00-7:00 pm) | | | | | | |
| SPECIAL BUSINESS (7:00 pm) | | | | | | |
| CONSENT CALENDAR | | | | | | |
| REGULAR BUSINESS | | | | | | |
| EXECUTIVE SESSION | | | | | | |

| APRIL 7 | | DUE | 3/27 | 3/30 | 3/30 | 3/31 |
|-------------------------------------|--|------------|-------------|-------------|--------------|--------------|
| ABSENCES: | | TO: | D/P | FN | CA | Clerk |
| ITEM TYPE TIME TOPIC | | | | | STAFF | |
| STUDY SESSION (6:00-7:00 pm) | | | | | | |
| SPECIAL BUSINESS (7:00 pm) | | | | | | |
| CONSENT CALENDAR | | | | | | |

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| REGULAR BUSINESS | | | | | Item 12. |
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| EXECUTIVE SESSION | | | | | |
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| APRIL 21 | | | | | DUE TO: | 4/10 D/P | 4/13 FN | 4/13 CA | 4/14 Clerk |
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| ABSENCES: | | | | | | | | | |
| ITEM TYPE TIME TOPIC | | | | | | | STAFF | | |
| STUDY SESSION (6:00-7:00 pm) | | | | | | | | | |
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| SPECIAL BUSINESS (7:00 pm) | | | | | | | | | |
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| REGULAR BUSINESS | | | | | | | | | |
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| EXECUTIVE SESSION | | | | | | | | | |
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| APRIL 23 (THURSDAY, SPECIAL MEETING) | | | | | | | | | | |
|--------------------------------------|--|--|--|--|--|--|--|-------|--|--|
| Legal Notice: | | | | | | | | | | |
| ABSENCES: | | | | | | | | | | |
| 60 | Joint Meeting with MISD Board (4:45 – 6:00 pm) | | | | | | | Jessi | | |

| MAY 5 | | | | | DUE TO: | D/P | FN | CA | Clerk |
|------------------------------|--|--|--|--|---------|-----|-------|----|-------|
| ABSENCES: | | | | | | | | | |
| ITEM TYPE TIME TOPIC | | | | | | | STAFF | | |
| STUDY SESSION (6:00-7:00 pm) | | | | | | | | | |
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| SPECIAL BUSINESS (7:00 pm) | | | | | | | | | |
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| MAY 19 | | | | | DUE TO: | D/P | FN | CA | Clerk |
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| ABSENCES: | | | | | | | | | |
| ITEM TYPE TIME TOPIC | | | | | | | STAFF | | |
| 618 | | | | | | | | | |

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| STUDY SESSION (6:00-7:00 pm) | <i>Item 12.</i> |
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| SPECIAL BUSINESS (7:00 pm) | |
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| CONSENT CALENDAR | |
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| REGULAR BUSINESS | |
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| JUNE 2 ABSENCES: | DUE TO: | D/P | FN | CA | Clerk |
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| ITEM TYPE TIME TOPIC | STAFF | | | | |
| STUDY SESSION (6:00-7:00 pm) | | | | | |
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| SPECIAL BUSINESS (7:00 pm) | | | | | |
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| REGULAR BUSINESS | | | | | |
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| EXECUTIVE SESSION | | | | | |
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| JUNE 16 ABSENCES: | DUE TO: | D/P | FN | CA | Clerk |
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| ITEM TYPE TIME TOPIC | STAFF | | | | |
| STUDY SESSION (6:00-7:00 pm) | | | | | |
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| SPECIAL BUSINESS (7:00 pm) | | | | | |
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| JULY 7 ABSENCES: | | | | DUE TO: | D/P | FN | CA | Item 12. |
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| ITEM TYPE TIME TOPIC | | | | | | STAFF | | |
| STUDY SESSION (6:00-7:00 pm) | | | | | | | | |
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| SPECIAL BUSINESS (7:00 pm) | | | | | | | | |
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| REGULAR BUSINESS | | | | | | | | |
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| JULY 21 ABSENCES: | | | | DUE TO: | D/P | FN | CA | Clerk |
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| ITEM TYPE TIME TOPIC | | | | | | STAFF | | |
| STUDY SESSION (6:00-7:00 pm) | | | | | | | | |
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| SPECIAL BUSINESS (7:00 pm) | | | | | | | | |
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| REGULAR BUSINESS | | | | | | | | |
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| AUGUST 4 | | | | | | | | |
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| POTENTIALLY CANCELED | | | | | | | | |

| AUGUST 18 | | | | | | | | |
|----------------------|--|--|--|--|--|--|--|--|
| POTENTIALLY CANCELED | | | | | | | | |

| SEPTEMBER 1 ABSENCES: | | | | DUE TO: | D/P | FN | CA | Clerk |
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| ITEM TYPE TIME TOPIC | | | | | | STAFF | | |
| STUDY SESSION (6:00-7:00 pm) | | | | | | | | |
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| 620 L BUSINESS (7:00 pm) | | | | | | | | |

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| CONSENT CALENDAR | | | | | |
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| REGULAR BUSINESS | | | | | |
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| EXECUTIVE SESSION | | | | | |
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| SEPTEMBER 15 | | | | DUE TO: | D/P | FN | CA | Clerk |
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| ABSENCES: | | | | | | | | |
| ITEM TYPE TIME TOPIC | | | | | | STAFF | | |
| STUDY SESSION (6:00-7:00 pm) | | | | | | | | |
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| SPECIAL BUSINESS (7:00 pm) | | | | | | | | |
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| EXECUTIVE SESSION | | | | | | | | |
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| OCTOBER 6 | | | | DUE TO: | D/P | FN | CA | Clerk |
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| ABSENCES: | | | | | | | | |
| ITEM TYPE TIME TOPIC | | | | | | STAFF | | |
| STUDY SESSION (6:00-7:00 pm) | | | | | | | | |
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| SPECIAL BUSINESS (7:00 pm) | | | | | | | | |
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| EXECUTIVE SESSION | | | | | | | | |
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| OCTOBER 20 | | | | DUE TO: | D/P | FN | CA | Clerk |
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| ABSENCES: | | | | | | | | |
| ITEM TYPE TIME TOPIC | | | | | | STAFF | | |
| 621 SESSION (6:00-7:00 pm) | | | | | | | | |

Item 12.

SPECIAL BUSINESS (7:00 pm)

CONSENT CALENDAR

REGULAR BUSINESS

EXECUTIVE SESSION

NOVEMBER 3

ABSENCES:

DUE TO:

D/P

FN

CA

Clerk

ITEM TYPE | TIME | TOPIC

STAFF

STUDY SESSION (6:00-7:00 pm)

SPECIAL BUSINESS (7:00 pm)

CONSENT CALENDAR

REGULAR BUSINESS

EXECUTIVE SESSION

NOVEMBER 17

ABSENCES:

DUE TO:

D/P

FN

CA

Clerk

ITEM TYPE | TIME | TOPIC

STAFF

STUDY SESSION (6:00-7:00 pm)

SPECIAL BUSINESS (7:00 pm)

CONSENT CALENDAR

REGULAR BUSINESS

EXECUTIVE SESSION

DECEMBER 1

622 CES:

DUE TO:

D/P

FN

CA

Clerk

| ITEM TYPE TIME TOPIC | STAFF |
|------------------------------|----------|
| STUDY SESSION (6:00-7:00 pm) | Item 12. |
| | |
| SPECIAL BUSINESS (7:00 pm) | |
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| CONSENT CALENDAR | |
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| REGULAR BUSINESS | |
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| EXECUTIVE SESSION | |
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| DECEMBER 15 ABSENCES: | DUE TO: | D/P | FN | CA | Clerk |
|------------------------------|---------|-----|----|----|-------|
| ITEM TYPE TIME TOPIC | STAFF | | | | |
| STUDY SESSION (6:00-7:00 pm) | | | | | |
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| SPECIAL BUSINESS (7:00 pm) | | | | | |
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| CONSENT CALENDAR | | | | | |
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| REGULAR BUSINESS | | | | | |
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| EXECUTIVE SESSION | | | | | |
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ANNUAL (ROUTINE) ITEMS:**Council/City Manager:**

- Legislative Agenda (Q3 & Q4)
- City Council Annual Planning Session (Q1)
- Adoption of City Council Priorities (Q2)
- City Council Mid-Year Planning Session (Q2)
- Sustainability Update (Q1 & Q3)
- Boards & Commissions Annual Appointments (Q2)

Community Planning and Development:

- ARCH Budget and Work Program (Q1)
- ARCH Trust Fund Recommendations (Q1)
- Code Amendment to Update School Impact Fees (Q3)
- Comprehensive Plan Amendments (Q4)
- Comprehensive Plan Docket (Q4)

Finance/Budget:

- Every Year:
 - General Fund & REET Surplus Disposition (Q2)
 - 4th Quarter Financial Status Report & Budget Adjustments (Q2)
 - 1st Quarter Financial Status Report & Budget Adjustments (Q2)
 - 2nd Quarter Financial Status Report & Budget Adjustments (Q3)
 - 3rd Quarter Financial Status Report & Budget Adjustments (Q4)
- Odd Years:
 - Mid-Biennial Budget Review (3rd Quarter Financial Status Report & Budget Adjustments, Utility Rates, and Property Tax Levy) (Nov Mtg)
- Even Years:
 - Capital Improvement Program (CIP) Budget Kick-Off (2nd Mar Mtg)
 - Operating Budget Kick-Off (Mid-Year PS)
 - Proposed Budget: Presentation & Distribution (1st Oct Mtg)
 - Proposed Budget: Operating Budget Review (2nd Oct Mtg)
 - Proposed Budget: Capital Improvement Program Review (1st Nov Mtg)
 - Proposed Budget: Finalize Changes & Adopt Tax Ordinances and Fee Resolutions (2nd Nov Mtg)
 - Final Budget Adoption (1st Dec Mtg)

Fire Department:**Human Resources:**

- Police & Police Support Collective Bargaining Agreements
- Fire Collective Bargaining Agreement
- AFSCME Collective Bargaining Agreement

Parks & Recreation:

- Open Space Conservancy Trust Board Annual Report and Work Plan (Q2)
- Open Space Vegetation Management Report (Q2, every other year)

Public Works:

- Bid Awards & Project Close-Outs
- Public Hearing: Preview of 6-Year Transportation Improvement Program (Q2)
- Adoption of 6-Year Transportation Improvement Program (Q2)

Youth & Family Services:

- Interlocal Agreement with MISD for School Mental Health Counselors (Q3)

Proclamations:

- Sexual Assault Awareness Month (1st Apr)
- Safe Boating and Paddling Week (2nd May)
- Parks and Recreation Month (1st Jul)
- National Recovery Month (1st Sep)
- National Preparedness Month (1st Sep)
- Mayor’s Day of Concern for the Hungry (1st Sep)
- Peace Day on Mercer Island (September 18)
- Domestic Violence Action Month (1st Oct)



COUNCILMEMBER BENSON WONG
CITY OF MERCER ISLAND, WASHINGTON

9611 SE 36th Street • Mercer Island, WA 98040-3732
(206) 275-7995 • www.mercergov.org

DATE: December 3, 2019

TO: Honorable Mayor Bertlin and Councilmembers

FROM: City Clerk Deborah Estrada on behalf of Councilmember Benson Wong

SUBJECT: Draft SCA 2020 Legislative Agenda (Exhibit 1)

At the November 13 PIC meeting, the attached Draft SCA 2020 Legislative Agenda was discussed. The PIC is scheduled to meet again on December 11 to review and approve the SCA 2020 Legislative Agenda, in advance of the 2020 legislative session scheduled to start in Olympia this January.

Councilmember Wong noted in his November 14 email (see Exhibit 2) to Council that the SCA 2020 Legislative Agenda aligns with the Mercer Island Legislative Agenda that was previously adopted.

DRAFT SCA 2020 Legislative Agenda

Address the Affordable Housing and Homelessness Crisis

The state and cities must partner to preserve and increase the supply of affordable housing as, well as address behavioral health needs and other root causes of homelessness. Sound Cities Association urges the Legislature to:

- *Allow cities to create and preserve affordable housing through optional local tools, including extending the timeline to approve a “qualifying local tax” provided under HB 1406 adopted in the 2019 legislative session*
- *Continue to expand investment in the Housing Trust Fund*
- *Address other underlying causes of homelessness by providing support to cities to implement innovative local solutions and increasing investments in our state’s behavioral health system*

Invest in Transportation Infrastructure and Mobility

The economic vitality of our state demands that we invest in our existing transportation infrastructure and prioritize new investments that improve the movement of people and goods. Cities have increased investments in local transportation systems, but still face an annual funding gap for maintenance and operation of those systems of \$1 billion statewide. Sound Cities Association urges the Legislature to:

- *Partner with cities to develop a comprehensive transportation bill that provides new resources and options for local government to address transportation and mobility needs*

Fully Fund the Public Works Trust Fund and Provide Options for Local Infrastructure Needs

Today’s cities are building the infrastructure necessary to accommodate a growing population and economy. Investments in infrastructure keep communities vibrant, protect the environment, and attract economic development. Cities need tools to lower the cost of providing local infrastructure and to avoid further exacerbating the housing affordability crisis. Sound Cities Association urges the Legislature to:

- *Fully fund the Public Works Trust Fund*
- *Support economic development tools that help maintain and expand local infrastructure, such as Tax Increment Financing and similar tools*

Preserve Local Decision-Making Authority

Cities possess strong local knowledge and authority to keep communities safe, healthy, and improve quality of life. It is critical cities maintain the authority to provide the necessary services and retain local control over land use planning that will help communities thrive.

Provide the Tools for Cities to Address Local Priorities

City revenue streams are limited and are not structured to sustainably keep-up with rising costs. Cities need flexible local funding tools, fewer unfunded mandates, and continued support from the state for shared responsibilities. Sound Cities Association urges the Legislature to:

- *Replace the arbitrary 1% cap on annual property tax increases with a limit tied to inflation plus population growth*
- *Meet the state’s commitment to revenues intended to be shared with cities to serve our shared constituents*
- *Continue streamlined sales tax mitigation to affected cities*

From: [Benson Wong](#)
To: [Council](#)
Cc: [Jessi Bon](#); [Ali Spietz](#); [Deborah Estrada](#)
Subject: Draft SCA 2020 Legislative Agenda
Date: Thursday, November 14, 2019 4:46:43 PM
Attachments: [SCA 2020 Leg Agenda \(11.13.2019\).pdf](#)

At last night's PIC meeting, there was a brief discussion about the Draft SCA 2020 Legislative Agenda. The PIC will have one more meeting in December. The hope is that the PIC members will give their approval of the proposed SCA 2020 Legislative Agenda (or that any changes will be agreed to by a majority of the PIC members in attendance). Ideally, SCA would like to get its 2020 Legislative Agenda approved before the start of the session in Olympia in January.

I will ask for a show of support for the SCA 2020 Legislative Agenda on Tuesday (11/19) as part of my councilmember report. Hopefully each of you will have had time to review it before the meeting. As you will see, many of the items on the SCA 2020 Legislative Agenda align with the MI Legislative Agenda that was adopted earlier.

Benson

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