

**CONTINUATION OF SPECIAL CALLED
PLANNING AND ZONING BOARD HEARING
FROM 01-10-2024
CITY OF LAKE CITY
January 17, 2024 at 5:30 PM
Venue: City Hall**

AGENDA

The meeting will be held in the City Council Chambers on the second floor of City Hall located at 205 North Marion Avenue, Lake City, FL 32055. Members of the public may also view the meeting on our YouTube channel. YouTube channel information is located at the end of this agenda.

INVOCATION

ROLL CALL

MINUTES

OLD BUSINESS

NEW BUSINESS

- I. SPR22-15**, a petition submitted by Jarod Stubbs, as agent for Daniel Hotte of GWC Development Partners, LLC, owner, for a site plan review application for a property located in a commercial highway interchange zoning district, parcels 35-3S-16-02524-001 and 35-3S-16-02524-102.

WORKSHOP

ADJOURNMENT

YouTube Channel Information

Members of the public may also view the meeting on our YouTube channel at:
<https://youtube.com/c/CityofLakeCity>

Pursuant to 286.0105, Florida Statutes, the City hereby advises the public if a person decides to appeal any decision made by the City Council with respect to any matter considered at its meeting or hearings, he or she will need a record of the proceedings,

and that, for such purpose, he or she may need to ensure that a verbatim record of the proceedings is made, which record includes the testimony and evidence upon which the appeal is to be based.

Pursuant to 286.26, Florida Statutes, persons needing special accommodations to participate in this meeting should contact the City Manager's Office at (386) 719-5768.

File Attachments for Item:

i. SPR22-15, a petition submitted by Jarod Stubbs, as agent for Daniel Hotte of GWC Development Partners, LLC, owner, for a site plan review application for a property located in a commercial highway interchange zoning district, parcels 35-3S-16-02524-001 and 35-3S-16-02524-102.



GROWTH MANAGEMENT

205 North Marion Ave.
Lake City, FL 32055
Telephone: (386)719-5750
E-Mail:
growthmanagement@lcfla.com

FOR PLANNING USE ONLY

Application # SPR22-19
Application Fee: \$200.00
Receipt No. _____
Filing Date 4/1/22
Completeness Date _____

Site Plan Application

A. PROJECT INFORMATION

1. Project Name: CIRCLE K - US 90 & I-75
2. Address of Subject Property: 143 NW Centurion Ct, Lake City, FL 32055
3. Parcel ID Number(s): 35-3S-16-02524-001, 35-3S-16-02524-102, 35-3S-16-02524-111
4. Future Land Use Map Designation: Commercial
5. Zoning Designation: CHI - Commercial Highway Interchange
6. Acreage: ±3.46
7. Existing Use of Property: Existing Circle K gas station and convenience store
8. Proposed use of Property: Circle K gas station and high speed diesel station
9. Type of Development (Check All That Apply):
 - Increase of floor area to an existing structure: Total increase of square footage ±652 SF
 - New construction: Total square footage ±54,470 SF
 - Relocation of an existing structure: Total square footage _____

B. APPLICANT INFORMATION

1. Applicant Status Owner (title holder) **Agent**
2. Name of Applicant(s): Jarod Stubbs P.E. Title: Civil Engineer
 Company name (if applicable): Kimley-Horn
 Mailing Address: 189 S. Orange Ave, Suite 1000
 City: Orlando State: FL Zip: 32801
 Telephone: (407) 409-7002 Fax: () Email: jarod.stubbs@kimley-horn.com

PLEASE NOTE: Florida has a very broad public records law. Most written communications to or from government officials regarding government business is subject to public records requests. Your e-mail address and communications may be subject to public disclosure.

3. If the applicant is agent for the property owner*.
 Property Owner Name (title holder): Daniel Hotte of GWC Development Partners, LLC
 Mailing Address: 2682 W Noegel Rd
 City: Lake City State: FL Zip: 32055
 Telephone: (407) 580-5173 Fax: () Email: dberry@shafferconst.com

PLEASE NOTE: Florida has a very broad public records law. Most written communications to or from government officials regarding government business is subject to public records requests. Your e-mail address and communications may be subject to public disclosure.

***Must provide an executed Property Owner Affidavit Form authorizing the agent to act on behalf of the property owner.**

C. ADDITIONAL INFORMATION

1. Is there any additional contract for the sale of, or options to purchase, the subject property?
If yes, list the names of all parties involved: _____
If yes, is the contract/option contingent or absolute: Contingent Absolute
2. Has a previous application been made on all or part of the subject property? Yes No
Future Land Use Map Amendment: Yes _____ No _____
Future Land Use Map Amendment Application No. _____
Site Specific Amendment to the Official Zoning Atlas (Rezoning): Yes _____ No
Site Specific Amendment to the Official Zoning Atlas (Rezoning) Application No. _____
Variance: Yes _____ No
Variance Application No. _____
Special Exception: Yes _____ No
Special Exception Application No. _____

D. ATTACHMENT/SUBMITTAL REQUIREMENTS

1. Vicinity Map – Indicating general location of the site, abutting streets, existing utilities, complete legal description of the property in question, and adjacent land use.
2. Site Plan – Including, but not limited to the following:
 - a. Name, location, owner, and designer of the proposed development.
 - b. Present zoning for subject site.
 - c. Location of the site in relation to surrounding properties, including the means of ingress and egress to such properties and any screening or buffers on such properties.
 - d. Date, north arrow, and graphic scale not less than one inch equal to 50 feet.
 - e. Area and dimensions of site (Survey).
 - f. Location of all property lines, existing right-of-way approaches, sidewalks, curbs, and gutters.
 - g. Access to utilities and points of utility hook-up.
 - h. Location and dimensions of all existing and proposed parking areas and loading areas.
 - i. Location, size, and design of proposed landscaped areas (including existing trees and required landscaped buffer areas).
 - j. Location and size of any lakes, ponds, canals, or other waters and waterways.
 - k. Structures and major features fully dimensioned including setbacks, distances between structures, floor area, width of driveways, parking spaces, property or lot lines, and percent of property covered by structures.
 - l. Location of trash receptacles.
 - m. For multiple-family, hotel, motel, and mobile home park site plans:
 - i. Tabulation of gross acreage.
 - ii. Tabulation of density.
 - iii. Number of dwelling units proposed.
 - iv. Location and percent of total open space and recreation areas.
 - v. Percent of lot covered by buildings.

- vi. Floor area of dwelling units.
 - vii. Number of proposed parking spaces.
 - viii. Street layout.
 - ix. Layout of mobile home stands (for mobile home parks only).
5. Stormwater Management Plan—Including the following:
 - a. Existing contours at one foot intervals based on U.S. Coast and Geodetic Datum.
 - b. Proposed finished elevation of each building site and first floor level.
 - c. Existing and proposed stormwater management facilities with size and grades.
 - d. Proposed orderly disposal of surface water runoff.
 - e. Centerline elevations along adjacent streets.
 - f. Water management district surface water management permit.
 6. Fire Department Access and Water Supply Plan: The Fire Department Access and Water Supply Plan must demonstrate compliance with Chapter 18 of the Florida Fire Prevention Code, be located on a separate signed and sealed plan sheet, and must be prepared by a professional fire engineer licensed in the State of Florida. The Fire Department Access and Water Supply Plan must contain fire flow calculations in accordance with the Guide for Determination of Required Fire Flow, latest edition, as published by the Insurance Service Office ("ISO") and/or Chapter 18, Section 18.4 of the Florida Fire Prevention Code, whichever is greater.
 7. Concurrency Impact Analysis: Concurrency Impact Analysis of impacts to public facilities. For commercial and industrial developments, an analysis of the impacts to Transportation, Potable Water, Sanitary Sewer, and Solid Waste impacts are required.
 8. Comprehensive Plan Consistency Analysis: An analysis of the application's consistency with the Comprehensive Plan (analysis must identify specific Goals, Objectives, and Policies of the Comprehensive Plan and detail how the application complies with said Goals, Objectives, and Policies).
 9. Legal Description with Tax Parcel Number (In Word Format).
 10. Proof of Ownership (i.e. deed).
 11. Agent Authorization Form (signed and notarized).
 12. Proof of Payment of Taxes (can be obtained online via the Columbia County Tax Collector's Office).
 13. Fee. The application fee for a Site and Development Plan Application is \$200.00. No application shall be accepted or processed until the required application fee has been paid.

NOTICE TO APPLICANT

All eleven (11) attachments are required for a complete application. Once an application is submitted and paid for, a completeness review will be done to ensure all the requirements for a complete application have been met. If there are any deficiencies, the applicant will be notified in writing. If an application is deemed to be incomplete, it may cause a delay in the scheduling of the application before the Planning & Zoning Board.

A total of ten (10) copies of proposed site plan application and all support materials must be submitted along with a PDF copy on a CD. See City of Lake City submittal guidelines for additional submittal requirements.

THE APPLICANT ACKNOWLEDGES THAT THE APPLICANT OR AGENT MUST BE PRESENT AT THE PUBLIC HEARING BEFORE THE PLANNING AND ZONING BOARD, AS ADOPTED IN THE BOARD RULES AND PROCEDURES, OTHERWISE THE REQUEST MAY BE CONTINUED TO A FUTURE HEARING DATE.

I hereby certify that all of the above statements and statements contained in any documents or plans submitted herewith are true and accurate to the best of my knowledge and belief.

Jared Stubbs

Applicant/Agent Name (Type or Print)

[Signature]

Applicant/Agent Signature

6/8/2022

Date

Applicant/Agent Name (Type or Print)

Applicant/Agent Signature

Date

STATE OF FLORIDA
COUNTY OF Orange

The foregoing instrument was acknowledged before me this 8th day of June, 2022, by (name of person acknowledging) Jared Stubbs.

(NOTARY SEAL or



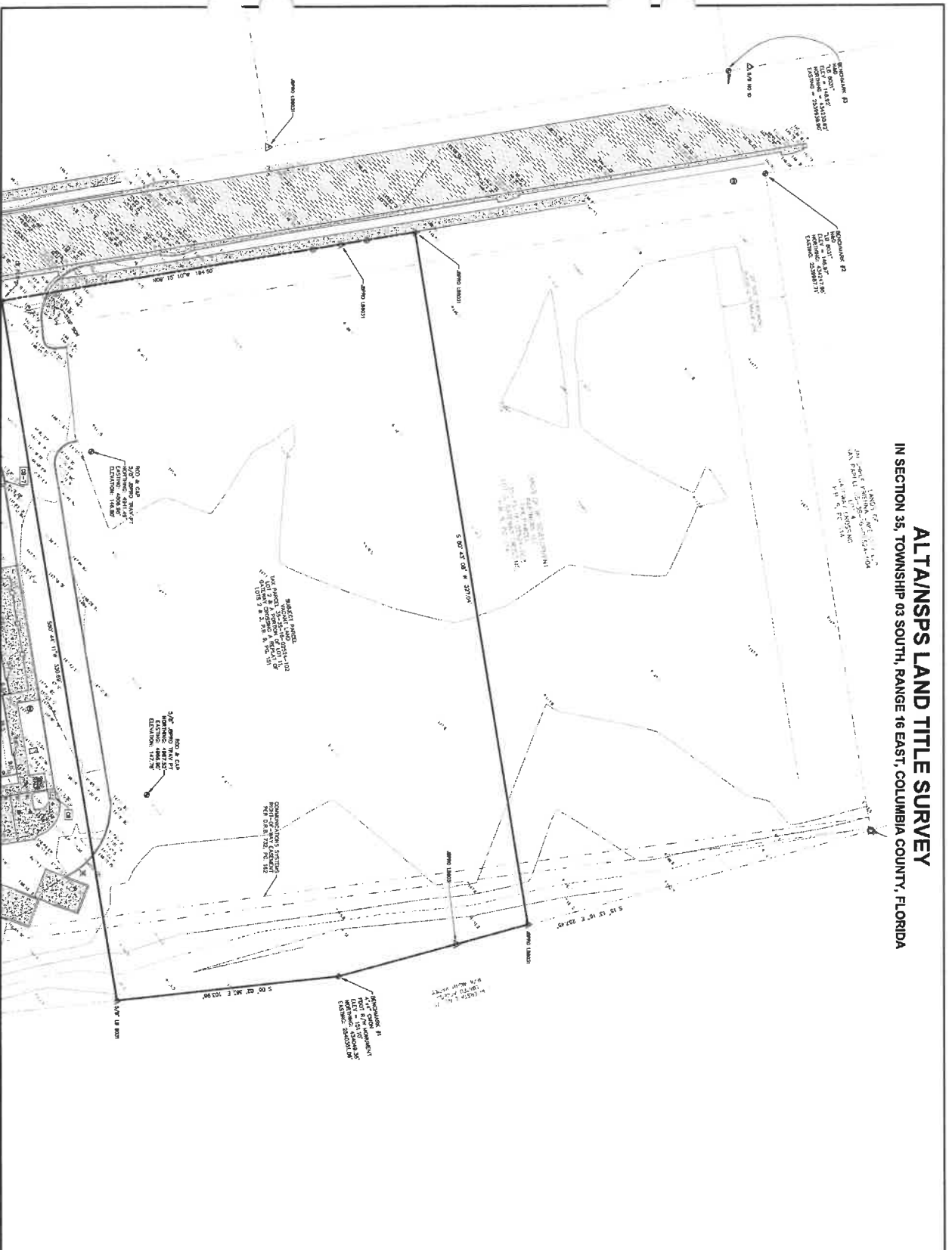
[Signature]
Signature of Notary

Printed Name of Notary

Personally Known OR Produced Identification _____
Type of Identification Produced

ALT/ANSPS LAND TITLE SURVEY

IN SECTION 35, TOWNSHIP 03 SOUTH, RANGE 16 EAST, COLUMBIA COUNTY, FLORIDA



SYMBOL LEGEND

- BOUNDARY LINE
- RIGHT-OF-WAY CENTRELINE
- RIGHT-OF-WAY BOUNDARY LINE
- FENCE
- OVERHEAD WIRE
- WIRE
- STORM SEWER MAIN
- CONCRETE MANHOLE
- MANHOLE
- STORM SEWER MANHOLE
- WATER MAIN
- ELECTRICAL SERVICE BOX
- FIRE HYDRANT
- SIGN
- POST
- NAIL
- CORNER
- ELEVATION
- DISTANCE
- BEARING
- CURVE DATA
- ADJACENT SURVEYS
- EXISTING STRUCTURES

ABBREVIATIONS

- PR - PROFESSIONAL RECORD
- PA - PLAT BOOK
- IN - INSTRUMENT
- NO - NOTATION
- IC - IDENTIFICATION
- APRO - APPROXIMATE PROFESSIONAL GROUP
- LS - LEGAL SURVEYING BUSINESS
- LSA - LEGAL SURVEYING BUSINESS
- PA - PLAT BOOK
- O.R.B. - ORIGINAL RECORDS BOOK
- PR - PROFESSIONAL RECORD
- PR - PROFESSIONAL RECORD
- PR - PROFESSIONAL RECORD
- PR - PROFESSIONAL RECORD

TOPOGRAPHIC SURVEY SHEET KEY

ALT/ANSPS LAND TITLE SURVEY
143 NW CENTURION CT LANE CITY

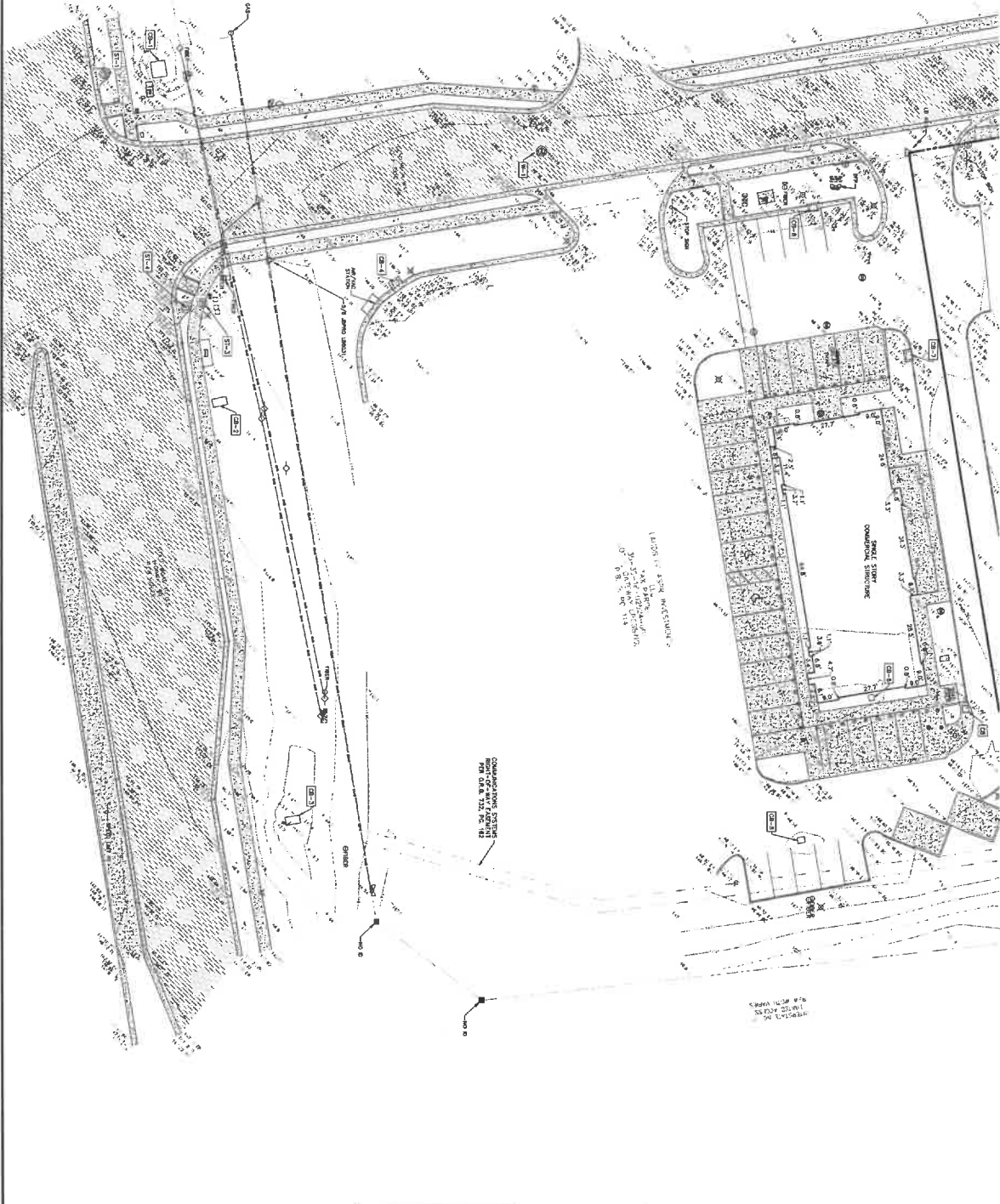
JBR Pro
Professional Surveyors
143 NW CENTURION CT LANE CITY
FLORIDA
2025

SCALE 1" = 20'
 20 10 0 20 40 80

Symbol Legend
Abbreviations
Topographic Survey Sheet Key

ALTANSPS LAND TITLE SURVEY

IN SECTION 36, TOWNSHIP 03 SOUTH, RANGE 16 EAST, COLUMBIA COUNTY, FLORIDA

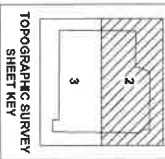


SYMBOL LEGEND

- BOUNDARY LINE
- RIGHT-OF-WAY CENTERLINE
- RIGHT-OF-WAY EDGE LINE
- RAILROAD CENTERLINE
- OVERHEAD WIRE
- STONE SERVICE LINE
- BRICK SERVICE LINE
- CONCRETE MONUMENT
- WOODEN SERVICE MONUMENT
- SIGNAL POLE
- WIRE VALVE
- WIRE METER
- FIRE HYDRANT
- WATER METER
- METER FOR UNDERGROUND UTILITY
- ELECTRICAL SERVICE BOX
- TRANSFORMER
- LIGHT POLE
- SIGN POLE SIGN
- WIRE PULL BOX - 50FT SERVICE
- SPOT ELEVATION - HARD SURFACE
- CENTERLINE
- ADJACENT SURFACE
- CONCRETE SERVICE

ABBREVIATIONS

- PT - PLATTED
- NY - MEASURED
- NY - KNOWN
- ID - IDENTIFICATION
- JBP - JEROME PROFESSIONAL GROUP
- LS - LICENSED SURVEYING BUSINESS
- FS - FIELD SKETCH
- FB - FIELD BOOK
- O.S.B. - ORIGINAL RECORDS BOOK
- PM - PROFESSIONAL MEASUREMENT
- PL - PROFESSIONAL LAND SURVEYOR



ALTANSPS LAND TITLE SURVEY

143 NW CENTURION CT LAKE CITY

JBP Pro

Surveyed by: J. B. P.	Date: 11/15/2011
Project: 143 NW CENTURION CT LAKE CITY	Sheet: 3 of 3

AGENT AUTHORIZATION FORM

FOR THE CIRCLE K – US 90 & I-75 PROJECT LOCATED IN LAKE CITY, FLORIDA

I, SAMMY OR PRITI VIRANI OF ASPRI INVESTMENTS, LLC AS THE OWNER OF THE REAL PROPERTY DESCRIBED AS FOLLOWS, COLUMBIA COUNTY PARCEL NO. 35-3S-16-02524-001, DO HEREBY AUTHORIZE TO ACT AS MY/OUR AGENT(S) EDWARD GIUNTA, CIRCLE K STORES, INC., AND JAROD STUBBS, KIMLEY-HORN AND ASSOCIATES, INC., TO EXECUTE ANY PETITIONS OR OTHER DOCUMENTS NECESSARY TO AFFECT THE APPLICATION APPROVAL REQUESTED AND MORE SPECIFICALLY DESCRIBED AS FOLLOWS, CITY OF LAKE CITY PERMIT(S), SRWMD PERMIT(S), FDEP PERMIT(S), FDOT PERMIT(S), AND TO APPEAR ON MY/OUR BEHALF BEFORE ANY ADMINISTRATIVE OR LEGISLATIVE BODY IN THE COUNTY OR CITY CONSIDERING THIS APPLICATION AND TO ACT IN ALL RESPECTS AS OUR AGENT IN MATTERS PERTAINING TO THE APPLICATION.

Date: March 31st, 2022

Signature of Property Owner

Sammy Virani

Print Name Property Owner

STATE OF Texas ~~FLORIDA~~
COUNTY OF Galveston :

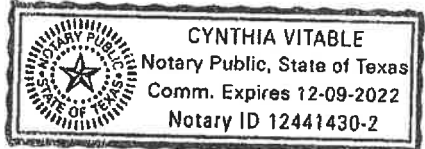
I certify that the foregoing instrument was acknowledged before me this 31st day of MARCH 2022 by SAMMY VIRANI. He/she is personally know to me or has produced as identification and did / did not take an oath.

Witness my hand and official seal in the county and state stated above on the 31st day of MARCH, in the year 2022.

Signature of Notary Public
Notary Public for the State of Florida Texas

(Notary Seal)

My Commission Expires: 12-09-2022



AGENT AUTHORIZATION FORM

FOR THE CIRCLE K - US 90 & I-75 PROJECT LOCATED IN LAKE CITY, FLORIDA

I, DANIEL HOTTE OF GWC DEVELOPMENT PARTNERS, LLC AS THE OWNER OF THE REAL PROPERTY DESCRIBED AS FOLLOWS, COLUMBIA COUNTY PARCEL NO.'S 35-3s-16-02524-111 & 35-3s-16-02524-102, DO HEREBY AUTHORIZE TO ACT AS MY/OUR AGENT(S) EDWARD GIUNTA, CIRCLE K STORES, INC., AND JAROD STUBBS, KIMLEY-HORN AND ASSOCIATES, INC., TO EXECUTE ANY PETITIONS OR OTHER DOCUMENTS NECESSARY TO AFFECT THE APPLICATION APPROVAL REQUESTED AND MORE SPECIFICALLY DESCRIBED AS FOLLOWS, CITY OF LAKE CITY PERMIT(S), SRWMD PERMIT(S), FDEP PERMIT(S), FDOT PERMIT(S), AND TO APPEAR ON MY/OUR BEHALF BEFORE ANY ADMINISTRATIVE OR LEGISLATIVE BODY IN THE COUNTY CONSIDERING THIS APPLICATION AND TO ACT IN ALL RESPECTS AS OUR AGENT IN MATTERS PERTAINING TO THE APPLICATION.

Signature of Property Owner

Date: 3/29/2022

DANIEL HOTTE

Print Name Property Owner

STATE OF FLORIDA :
COUNTY OF Broward :

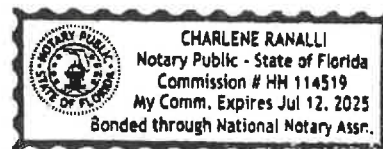
I certify that the foregoing instrument was acknowledged before me this 29th day of March 2022 by Daniel Hotte. He/she is personally know to me or has produced as identification and did / did not take an oath.

Witness my hand and official seal in the county and state stated above on the 29th day of March, in the year 2022.

(Notary Seal)

Signature of Notary Public
Notary Public for the State of Florida

My Commission Expires: 7/12/2025



Project Summary

Project Name: Circle K- US 90 & I75

Project Number: SPR22-15

Parcel Number: 02524-001

Project Notes

- Project type: Site Plan Review
- Future land use is: Commercial
- Zoning designation is: Commercial Highway Interchange
- Proposed use of the property: Expand existing building and add high flow diesel pumps.
- Land is conducive for use: Yes, per the LDR section 4.15.2.1
- See staff review for notes from directors and city staff for their comments.
- Parcel was replated in March 2022.

Project Summary

Project SPR22-15 is for a site plan review and has been reviewed by city staff. Application is sufficient for review. In March of 2022 the parcel was replated and was approved by city council. After review of the petition the city staff has determined that the petition is consistent with the land development regulations and the comprehensive plan. At this time the City has no concerns.



DEPARTMENT OF GROWTH MANAGEMENT
205 North Marion Avenue
Lake City, Florida 32055
Telephone: (386) 719-5750
growthmanagement@lcfla.com

REVIEW REPORT TO PLANNING AND ZONING, BOARD OF ADJUSTMENT AND
HISTORICAL COMMITTEES' BY STAFF
FOR SITE PLAN REVIEW, SPECIAL EXCEPTIONS, VARIANCES, COMPREHENSIVE
PLAN AMENDMENTS/ ZONING AND CERTIFICATE OF APPROPRIATENESS

Date: 06/15/2022

Request Type: Site Plan Review (SPR) Special Exception (SE) Variances (V)

Comprehensive Plan Amendment/Zoning (CPA/Z) Certificate of Appropriateness (COA)

Project Number: SPR22-15

Project Name: Circle K-US 90 and I75 (Gateway Crossings)

Project Address: 143 NW Centurion CT, Lake City FL

Project Parcel Number: 35-3S-16-02524-001,102, and 111

Owner Name: Daniel Hotte of GWC Development Partners, LLC

Owner: Address: 2682 W Noegel RD

Owner Contact Information: telephone number 407-580-5173 e-mail dberry@shafferconst.com

Owner Agent Name: Jarod Stubbs P.E.

Owner Agent Address: 180 S. Orange Ave, Suite 1000 Orlando FL 32801

Owner Agent Contact Information: telephone 407-409-7002 e-mail jarod.stubbs@kimley-horne.com

The City of Lake City staff has reviewed the application and documents provided for the above request and have determined the following:

Growth Management – Building Department, Planning and Zoning, Code Enforcement, Permitting

Building Department: Approved Disapproved Reviewed by: _____

Comments: N/A

Planning and Zoning: Approve Disapprove Reviewed by: Robert Angelo

Comments: No Concerns at this time

No Concerns at this time

Business License: Approve Disapprove Reviewed by: Marshall Sova

Comments: No Concerns at this time

Code Enforcement: Approve Disapprove Reviewed by: Marshall Sova

Comments: No Concerns at this time

Permitting: Approve Disapprove Reviewed by: Ann Jones

Comments: No Concerns at this time

No Concerns at this time

Utilities – Water, Sewer, Gas, Water Distribution/Collections, Customer Service

Water Department: Approved Disapproved Reviewed by: _____

Comments: N/A

Sewer Department: Approved Disapproved Reviewed by: _____

Comments: N/A

Gas Department: Approved Disapproved Reviewed by: Steve Brown

Comments: No Concerns at this time

Water Distribution/Collection: Approved Disapproved Reviewed by: Brian Scott

Comments: _____
If they do not use the taps in place they will be required to make new ones and
cut and cap sewer and dig to water main and shut off before construction.

Customer Service: Approved Disapproved Reviewed by: Shasta Pelham

Comments: Utility Plan 6.0 dated 05/04/22 references a 1" water meter and an existing 6" sewer tap. A tap application would be required to access city utilities.
The tap fees, impact fees and utility deposits will be calculated upon approval of the tap application. A floor plan with detailed fixture units of the restroom addition
is required. City utilities border the property; locates must be obtained to ensure that the utility infrastructure is not damaged or obstructed.

Public Safety – Public Works, Fire Department, Police Department

Public Works: Approved Disapproved Reviewed by: Steve Brown

Comments: No Concerns at this time

Fire Department: Approve Disapprove Reviewed by: Assistant Chief Boozer

Comments: No Concerns at this time

Police Department: Approve Disapprove Reviewed by: Assistant Chief Andy

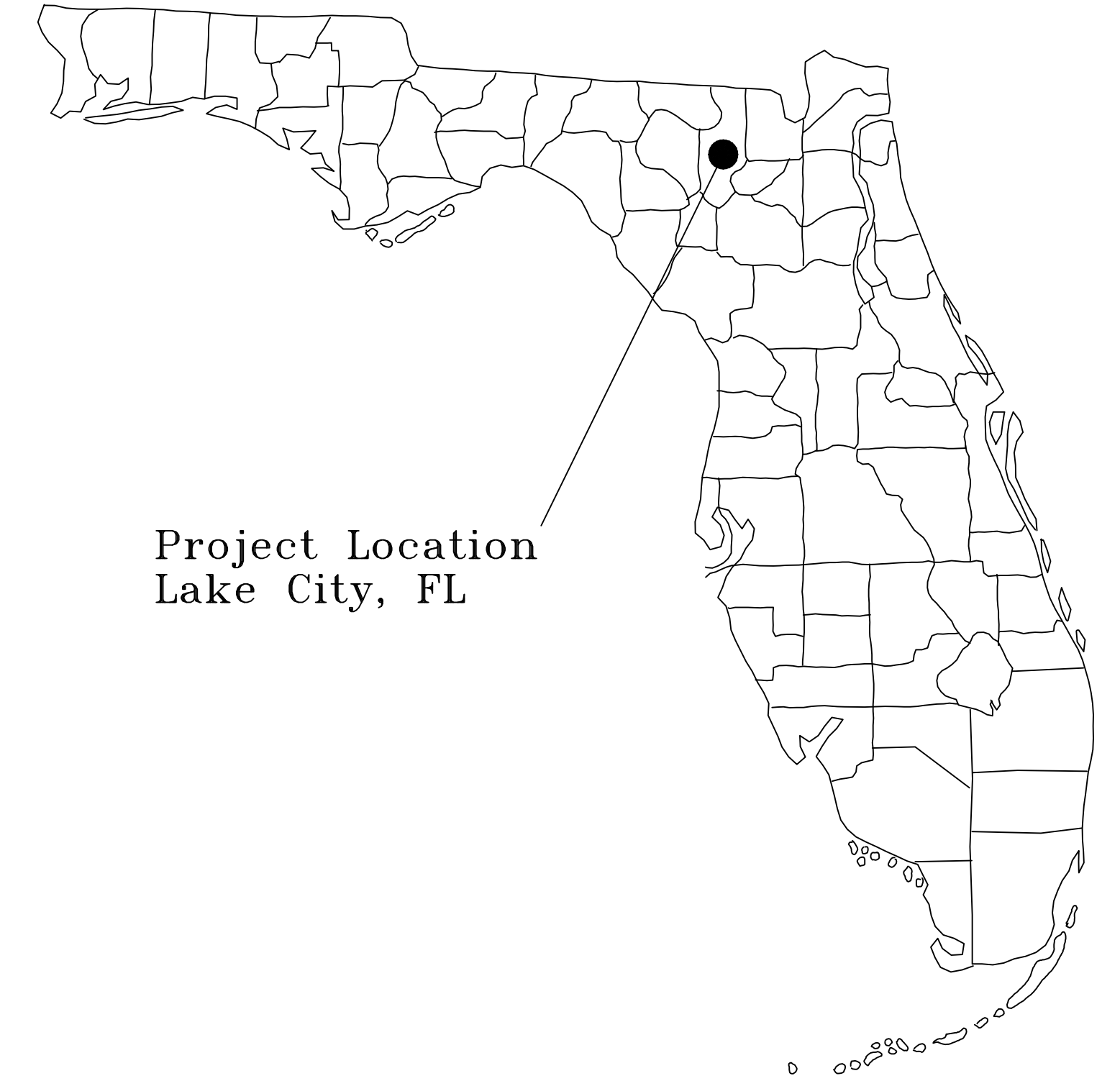
Comments: No Concerns at this time

Please provide separate pages for comments that will not fit in provided spaces and please label the pages for your department and for the project.

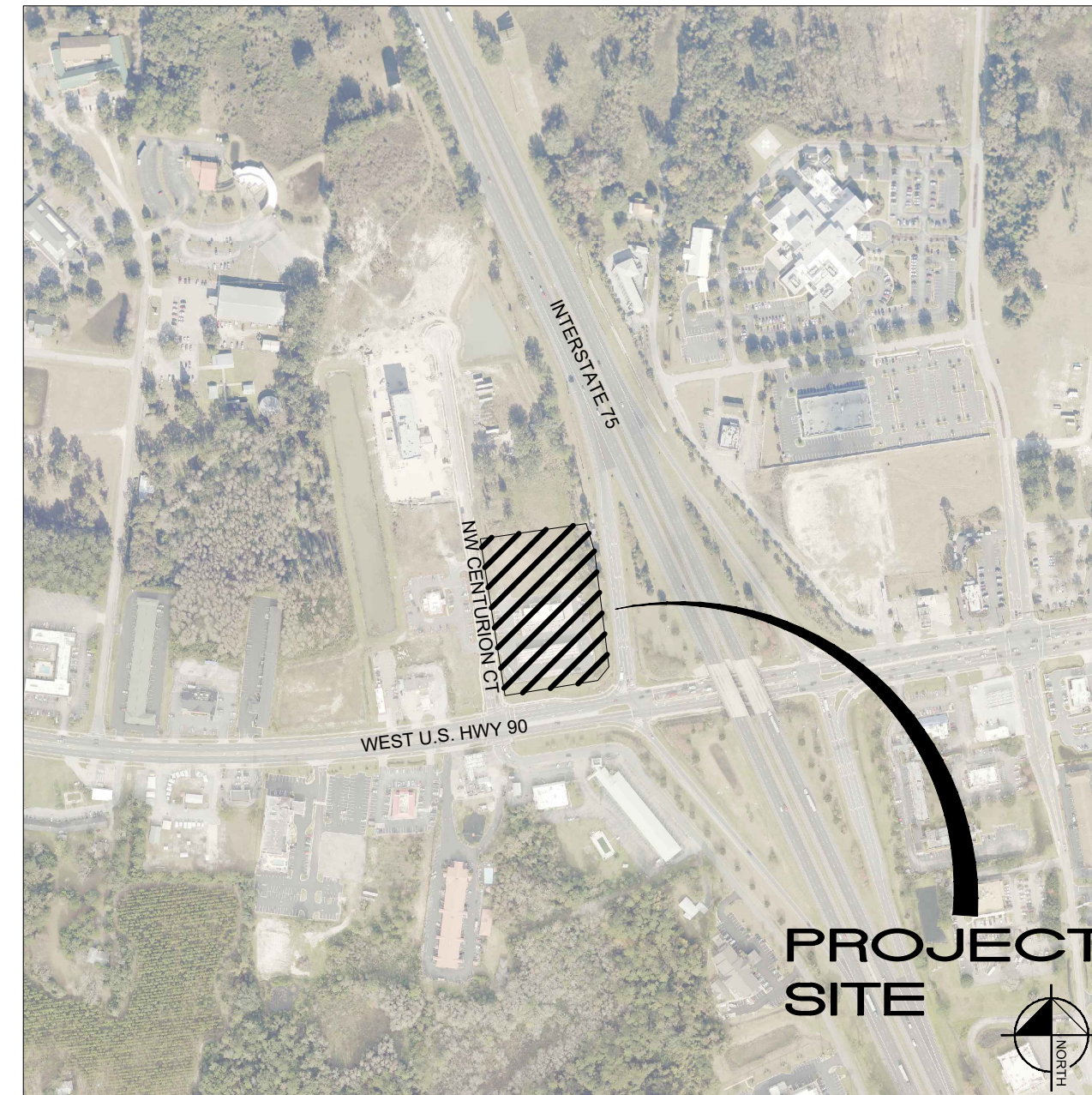
CONSTRUCTION PLANS FOR CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION

143 NW CENTURION COURT
LAKE CITY, FLORIDA 32055
MAY 4, 2022

PARCEL IDs: 35-3S-16-02524-001,
35-3S-16-02524-102 AND 35-3S-16-02524-111



Project Location
Lake City, FL



VICINITY MAP

1" = 500'

PROJECT TEAM

OWNER:
GWC DEVELOPMENT PARTNERS LLC
2682 W NOEGEL ROAD
LAKE CITY, FL 32055
CONTACT: DIANE BERRY
PHONE: (407) 580-5173
EMAIL: DBERRY@SCHAFERCONST.COM

CIVIL ENGINEER:
KIMLEY-HORN AND ASSOCIATES, INC.
189 SOUTH ORANGE AVENUE, SUITE 1000
ORLANDO, FL 32801
CONTACT: JAROD C. STUBBS, P.E.
PHONE: (407) 409-7002
EMAIL: JAROD.STUBBS@KIMLEY-HORN.COM

SURVEYOR:
JBPRO
3530 NW 43RD STREET
GAINESVILLE, FL 32606
CONTACT: TROY V. WRIGHT
PHONE: (352) 375-8999

LANDSCAPE ARCHITECT:
KIMLEY-HORN AND ASSOCIATES, INC.
189 SOUTH ORANGE AVENUE, SUITE 1000
ORLANDO, FL 32801
CONTACT: MATTHEW FRANKO
PHONE: (407) 427-1629
EMAIL: MATT.FRANKO@KIMLEY-HORN.COM

DEVELOPER:
CIRCLE K STORES, INC
3802 CORPOREX PARK DRIVE, SUITE 413
TAMPA, FL 33619
CONTACT: EDWARD GIUNTA
PHONE: (407) 580-5173

ARCHITECT:
RDC COLLABORATIVE
11921 FREEDOM DRIVE, SUITE #1110
RESTON, VA 20190
CONTACT: MEGAN LARGENT
PHONE: (703) 668-0086
FAX: (703) 668-0085

PREPARED BY

Kimley»Horn

© 2022 KIMLEY-HORN AND ASSOCIATES, INC.
189 S. ORANGE AVE, SUITE 1000, ORLANDO, FL 32801
PHONE: 407-898-1511
WWW.KIMLEY-HORN.COM REGISTRY No. 35106

LEGAL DESCRIPTION

LOT 2 AND THE NORTH 34.55 FEET OF LOT 11 OF GATEWAY CROSSING A REPLAT OF LOTS 2 & 3, ACCORDING TO PLAT THEREOF RECORDED IN PLAT BOOK 9, PAGE 151, PUBLIC RECORDS OF COLUMBIA COUNTY, FLORIDA.

UTILITY PROVIDERS

WATER/SEWER:
CITY OF LAKE CITY UTILITIES
692 SW SAINT MARGARETS ST
LAKE CITY, FL 32025
CONTACT:
PHONE:

CABLE:
COMCAST CABLE
5934 RICHARD STREET
JACKSONVILLE, FL 32216
CONTACT: ANDREW SWEENEY
PHONE: (904) 738-6898

ELECTRIC :
FLORIDA POWER & LIGHT
2618 NE BASCOM NORRIS DRIVE
LAKE CITY, FL 32055
CONTACT: SHANE EUBANK
PHONE: (386) 754-2020

TELEPHONE:
AT&T
6628 LAKESIDE ROAD
WEST PALM BEACH, FL 33411
CONTACT: DINO FARRUGGIO
EMAIL: G27896@ATT.COM
PHONE: (561) 683-2729

FIBER OPTIC :
HARGRAY OF FLORIDA, INC.
8324 BAYMEADOWS WAY, STE. 102
JACKSONVILLE, FL 32256
CONTACT: EDWARD HARDING
PHONE: (904) 652-9934

GAS :
CITY OF LAKE CITY GAS/PUBLIC WORKS
180 NE GUM SWAMP ROAD
LAKE CITY, FL 32055
CONTACT: THOMAS HENRY
EMAIL: HENRYT@CFLA.COM
PHONE: (386) 758-5425

SHEET INDEX

C0.0	COVER SHEET
C1.0-C1.1	GENERAL NOTES
C2.0	STORMWATER POLLUTION PREVENTION PLAN
C3.0-C3.1	EXISTING CONDITIONS & DEMOLITION PLAN
C4.0	OVERALL SITE PLAN
C4.1	SITE PLAN
C4.2	INTERSECTION MODIFICATION PLAN
C4.3-C4.5	TRUCK TURNING MOVEMENTS
C5.0	PAVING, GRADING AND DRAINAGE PLAN
C6.0	UTILITY PLAN
C7.0-C7.1	GENERAL CONSTRUCTION DETAILS
L1.00	LANDSCAPE PLAN
L1.50	LANDSCAPE DETAILS
L1.51	LANDSCAPE SPECIFICATIONS
L2.00	SCHEMATIC IRRIGATION PLAN
L2.50	IRRIGATION DETAILS
L2.51	IRRIGATION NOTES

Drawing name: K:\ORL\Civil\149680040-Circle K US90 & I75\CADD\CONST\PlanSheets\C0.0-COVER.dwg C0.0 COVER SHEET Oct. 03, 2022 3:45pm by: Theodoros.Martelli
This document, together with the concepts and designs presented herein, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.

Digitally signed by Jarod Stubbs
Date: 2022.10.04 16:43:44 -0400
P.E. # 89587

Professional Engineer, License No. 89587
Jarod C. Stubbs, P.E., State of Florida

NO. 89507
LICEA
No. 89507
P.E. # 89587

REVISED SHEETS
DATE
18

Plotted By: Morrell, Theodore Sheet Set: CIRCLE K - US HWY 90 & I-75 High Speed Diesel Fuel Expansion Layout: C1.1 GENERAL NOTES October 03, 2022 03:46:06pm K:\ORL_Civil\149880040-Circle K US90 & I-75 CADD\CONSTRUCTION\PlanSheets\C1.0-NOTES.dwg
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SANITARY SYSTEM

1. ALL PVC PIPE SHALL BE SOLID WALL POLYVINYL CHLORIDE PIPE AND COMPLY WITH ASTM D 3034 AND ALL APPLICABLE ASTM DOCUMENTS AS COVERED IN SECTION NO. 2 OF ASTM D 3034. MAIN LINES SHALL BE A MINIMUM OF 8" DIAMETER, AND LATERALS SHALL BE A MINIMUM 6" DIAMETER.
2. ALL GRAVITY SEWERS MUST BE SDR 26 PVC. ELASTOMERIC GASKET JOINTS SHALL BE UTILIZED FOR PVC PIPE, AND SHALL COMPLY WITH ASTM F477, ASTM D3034 & ASTM F679. JOINTS SHALL COMPLY WITH ASTM D3212.
3. ALL SLOPES FOR GRAVITY SEWER MAINS AND SERVICE CONNECTIONS SHALL COMPLY WITH THE FOLLOWING MINIMUM GRADES: 4" @ 2.00%; 6" @ 1.00% ; AND 8" @ 0.40%.
4. ALL SANITARY SEWER WORK SHALL CONFORM WITH APPLICABLE CITY OF LAKE CITY WATER UTILITIES DEPARTMENT STANDARDS AND SPECIFICATIONS.
5. PRIOR TO COMMENCING WORK WHICH REQUIRES CONNECTING PROPOSED FACILITIES TO EXISTING LINES OR APPURTENANCES, THE CONTRACTOR SHALL VERIFY THE LOCATION AND ELEVATION(S) OF EXISTING CONNECTION POINT(S) AND NOTIFY THE OWNER'S ENGINEER OF ANY CONFLICTS OR DISCREPANCIES.

SANITARY TESTING AND INSPECTION

1. ALL GRAVITY SEWER PIPING SHALL BE SUBJECT TO A VISUAL INSPECTION BY THE OWNER'S ENGINEER AND APPLICABLE MUNICIPALITY/AGENCY. THE CONTRACTOR SHALL NOTIFY THE ENGINEER 48 HOURS IN ADVANCE TO SCHEDULE INSPECTION(S). THE CONTRACTOR SHALL BE RESPONSIBLE FOR COSTS ASSOCIATED WITH A LAMPING INSPECTION OF THE PROPOSED GRAVITY SEWER LINE CONSTRUCTION. THE CONTRACTOR SHALL PROVIDE COPIES OF THE LAMPING INSPECTION TO THE ENGINEER, THE OWNER AND THE APPLICABLE MUNICIPALITY/AGENCY.
2. THE CONTRACTOR SHALL PERFORM AN INFILTRATION/EXFILTRATION TEST ON ALL GRAVITY SEWERS IN ACCORDANCE WITH THE REGULATORY AGENCY HAVING JURISDICTION. SAID TESTS ARE TO BE CERTIFIED BY THE ENGINEER OF RECORD AND SUBMITTED TO THE REGULATORY AGENCY FOR APPROVAL. THE SCHEDULING, COORDINATION AND NOTIFICATION OF ALL PARTIES IS THE CONTRACTOR'S RESPONSIBILITY.
3. LEAKAGE TESTS ARE SPECIFIED REQUIRING THAT:
 - A. THE LEAKAGE EXFILTRATION OR INFILTRATION DOES NOT EXCEED 200 GALLONS PER INCH OF PIPE DIAMETER PER MILE PER DAY FOR ANY SECTION OF THE SYSTEM.
 - B. EXFILTRATION OR INFILTRATION TESTS BE PERFORMED WITH A MINIMUM POSITIVE HEAD OF 2 FEET
 - C. AIR TESTS, AS A MINIMUM, CONFORM TO THE TEST PROCEDURE DESCRIBED IN ASTM C-828 FOR CLAY PIPE, ASTM C 924 FOR CONCRETE PIPE, ASTM F-1417 FOR PLASTIC PIPE, AND FOR OTHER MATERIALS APPROPRIATE TEST PROCEDURES.
4. CONTRACTOR TO PERFORM APPROPRIATE DEFLECTION TESTS FOR ALL FLEXIBLE PIPE. TESTING IS REQUIRED AFTER THE FINAL BACKFILL HAS BEEN IN PLACE AT LEAST 30 DAYS TO PERMIT STABILIZATION OF THE SOIL-PIPE SYSTEM. TESTING REQUIREMENTS SPECIFY:
 - A. NO PIPE SHALL EXCEED A DEFLECTION OF 5%.
 - B. USING A RIGID BALL OR MANDREL FOR THE DEFLECTION TEST WITH A DIAMETER NOT LESS THAN 95% OF THE BASE INSIDE DIAMETER OR AVERAGE INSIDE DIAMETER OF THE PIPE, DEPENDING ON WHICH IS SPECIFIED IN THE ASTM SPECIFICATION, INCLUDING THE APPENDIX, TO WHICH THE PIPE IS MANUFACTURED.
 - C. PERFORMING THE TEST WITHOUT MECHANICAL PULLING DEVICES.
5. CONTRACTOR TO INSPECT & TEST MANHOLE FOR WATERTIGHTNESS OR DAMAGE PRIOR TO PLACING INTO SERVICE. AIR TESTING, IF SPECIFIED FOR CONCRETE SEWER MANHOLES, SHALL CONFORM TO THE TEST PROCEDURES DESCRIBED IN ASTM C-1244.

POTABLE WATER SYSTEM

1. ALL DIP PIPE SHALL BE CLASS 50 OR HIGHER. REFER TO NOTE #4 BELOW FOR ADDITIONAL DIP SPECIFICATIONS. ADEQUATE MEASURES (PER AWWA, FDEP, AND POLK COUNTY CRITERIA) AGAINST CORROSION SHALL BE UTILIZED.
2. ALL WATER MAIN PIPE FITTINGS AND APPURTENANCES SHALL BE INSTALLED TO COMPLY WITH POLK COUNTY STANDARDS AND SPECIFICATIONS.
3. ALL WATER SERVICE LINES, VALVES AND METERS SHALL BE INSTALLED TO COMPLY WITH APPLICABLE MUNICIPALITY/AGENCY DEPARTMENT STANDARDS AND SPECIFICATIONS.
4. ALL DUCTILE IRON PIPE, 4" TO 24", SHALL BE MANUFACTURED IN ACCORDANCE WITH THE LATEST EDITION OF AWWA C151/A21.51. PIPE SHALL BE FURNISHED IN 18 OR 20 FOOT SECTIONS, PIPE THICKNESS SHALL BE CLASS 50, UNLESS OTHERWISE SPECIFIED.
5. ALL WATER SYSTEM CONSTRUCTION, FROM THE POINT OF CONNECTION IN THE RIGHT OF WAY UP TO AND INCLUDING POINT OF METERING AND BACK FLOW PREVENTION (IF REQUIRED), SHALL BE BUILT ACCORDING TO POLK COUNTY STANDARDS AND SPECIFICATIONS.
6. CONTRACTOR TO INSTALL TEMPORARY BLOWOFFS, AT THE END(S) OF PROPOSED WATER MAINS AND SERVICE LATERALS TO BUILDING(S), TO ASSURE ADEQUATE (PER AWWA, FDEP, AND POLK COUNTY CRITERIA) FLUSHING AND DISINFECTION/CHLORINATION.
7. ALL WATER MAINS SHALL BE STERILIZED IN ACCORDANCE WITH THE APPLICABLE SECTION OF THE LATEST AWWA SPECIFICATION C651 AND CITY OF Land City WATER DEPARTMENT SPECIFICATIONS.
8. ALL PVC WATER MAIN, 6" TO 12" DIAMETER PIPING, SHALL BE AWWA C-900 DR-18. JOINTS SHALL BE RUBBER GASKETED PUSH-ON CONFORMING TO ASTM D1869.
9. POTABLE WATER MAINS WILL BE PVC SDR 21 (200 PSI) FOR PIPES LESS THEN 4". SCHEDULE 40 AND SCHEDULE 80 PIPING MATERIAL ARE ALSO ACCEPTABLE FOR PIPES SIZES LESS THAN 4". THE ABOVE TYPE INSTALLATIONS MUST BEAR THE "NFS" STAMP FOR COMPATIBILITY WITH POTABLE WATER USE.
10. ALL POLYVINYL CHLORIDE PIPE SHALL BE LAID WITH AN INSULATED 10 GAUGE A.W.G. SOLID STRAND COPPER WIRE ON TOP OF THE PIPE. THIS WIRE IS TO BE CONTINUOUS WITH SPLICES MADE ONLY BY METHODS APPROVED BY THE ENGINEER. THIS WIRE IS TO BE SECURED TO ALL VALVES, TEES AND ELBOWS.
11. ALL POTABLE WATER WORK SHALL CONFORM WITH APPLICABLE POLK COUNTY UTILITIES DEPARTMENT STANDARDS AND SPECIFICATIONS.
12. PVC PIPE BURIED BENEATH ROADWAYS, PARKING LOTS OR PARKING LOT ENTRANCES SHALL MEET AWWA SPECIFICATION C900 OR C905, LATEST REVISION. ALL 6" TO 12" PIPE IN SUCH LOCATIONS SHALL BE A MINIMUM OF CLASS 200, DR-14, AND ALL 14" TO 36" PIPE SHALL BE A MINIMUM OF CLASS 235, DR-18.

POTABLE WATER TESTING AND INSPECTION

1. ALL COMPONENTS OF THE WATER SYSTEM, INCLUDING FITTINGS, HYDRANTS, CONNECTIONS, AND VALVES SHALL BE PROPERLY PRESSURE TESTED AND ACCEPTED BY THE OWNER'S ENGINEER. PRESSURE TESTS TO BE IN ACCORDANCE WITH POLK COUNTY UTILITIES DEPARTMENT SPECIFICATIONS. CONTRACTOR TO NOTIFY THE OWNER'S ENGINEER AND APPLICABLE AGENCY INSPECTORS 2 FULL BUSINESS DAYS IN ADVANCE OF PERFORMING TESTS.
2. CONTRACTOR TO PERFORM CHLORINATION AND BACTERIOLOGICAL SAMPLING, AND OBTAIN CLEARANCE OF DOMESTIC AND FIRE LINE WATER SYSTEM(S). COPIES OF ALL BACTERIOLOGICAL TEST RESULTS ARE TO BE SUBMITTED TO THE OWNER'S ENGINEER FOR CERTIFICATION PURPOSES.
3. ALL WATER MAINS SHALL BE PRESSURE TESTED IN ACCORDANCE WITH AWWA MANUAL M23, CONCERNING HYDROSTATIC TESTING OF PVC PIPING. OFF-SITE UTILITIES HYDROSTATIC TESTING TO BE WITNESSED BY THE CITY OF Land City WATER DEPARTMENT INSPECTOR.

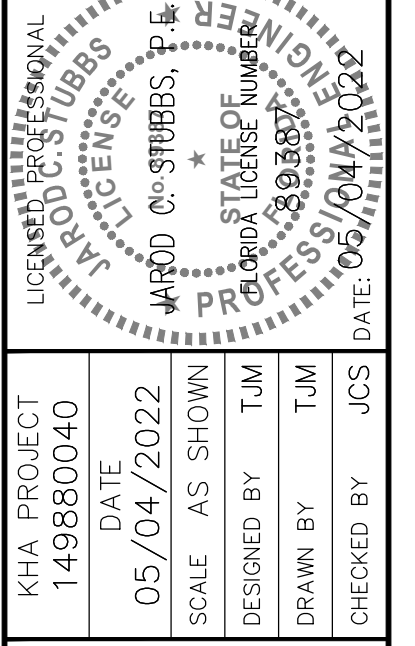
FDOT GENERAL NOTES

1. MAINTENANCE OF TRAFFIC TO BE SUPERVISED BY A CERTIFIED PERSON.
2. THE CONTRACTOR SHALL NOTIFY THE DEPARTMENT A MINIMUM OF TWO BUSINESS DAYS PRIOR TO ANY LANE CLOSURES OR BEGINNING ANY CONSTRUCTION WITHIN THE FDOT RIGHT-OF-WAY.
3. ALL WORK PERFORMED WITHIN THE FDOT RIGHT OF WAY SHALL BE IN ACCORDANCE WITH THE FY2021-22 OR CURRENT EDITION OF FDOT STANDARD PLANS.
4. IF THE DEPARTMENT DETERMINES THAT AS-BUILT CONDITIONS VARY SIGNIFICANTLY FROM THE APPROVED PLANS, THE PERMITTEE SHALL PROVIDE AS-BUILT PLANS, ALONG WITH A RECORD DRAWINGS REPORT BY PERMITTEE'S PROFESSIONAL ENGINEER, FORM 850-040-19, WITHIN 30 DAYS.
5. IT WILL BE THE RESPONSIBILITY OF THE PERMITTEE TO REPAIR ANY DAMAGE TO FDOT FACILITIES CAUSED BY CONSTRUCTION OF THE PROJECT.
6. TEST RESULTS OF ANY TESTS TAKEN FOR OR DURING CONSTRUCTION OF THE PERMITTED WORK SHALL BE PROVIDED TO THE FDOT UPON REQUEST.
7. ALL CONCRETE TO BE REMOVED SHALL BE SAW CUT AT THE NEAREST JOINT IN GOOD CONDITION, SO AS TO PRODUCE A CONNECTION WITH NEW CONCRETE THAT IS FREE OF CRACKS, DEFORMITY IN SHAPE, NOTICEABLE VOIDS, SURFACE IRREGULARITIES, AND OTHER DEFECTS.
8. ALL CONCRETE SHALL BE AN APPROVED FDOT MIX DESIGN OF 3,000 PSI MINIMUM.
9. ALL MATERIALS INSTALLED WITHIN FDOT RIGHT-OF-WAY SHALL BE LIMITED TO THOSE ON THE FDOT'S QUALIFIED PRODUCTS LIST OR APPROVED PRODUCT LIST OF TRAFFIC CONTROL SIGNALS AND DEVICES.
10. THE PERMITTEE SHALL CONTACT THE CITY OF LAKE CITY TRAFFIC DEPT. (386) 758-5400.
11. ALL CONSTRUCTION IN THE FDOT ROW SHALL CONFIRM TO THE LATEST EDITIONS OF THE FDOT DESIGN STANDARDS, THE FDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, AND THE FDOT UTILITY ACCOMMODATION MANUAL.
12. ALL DISTURBED AREAS IN FDOT ROW SHALL BE SODDED.
13. ALL WORK PERFORMED WITHIN THE FDOT RIGHT-OF-WAY SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE FDOT DESIGN STANDARDS, THE LATEST EDITION OF THE SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, AND THE 2017 UTILITY ACCOMMODATION MANUAL.
14. PLEASE NOTIFY JACKSONVILLE OPERATIONS TWO BUSINESS DAYS BEFORE BEGINNING WORK @ (904) 306-7500.

No.	REVISIONS	DATE	BY



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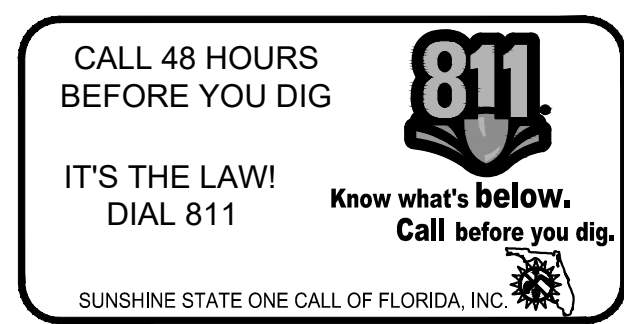


KHA PROJECT: 149880040
 DATE: 05/04/2022
 SCALE: AS SHOWN
 DESIGNED BY: TJM
 DRAWN BY: TJM
 CHECKED BY: JCS DATE: 05/04/2022

GENERAL NOTES

**CIRCLE K - US HWY
 90 & I-75 FUEL
 EXPANSION**
 CITY OF LAKE CITY FLORIDA

SHEET NUMBER
C1.1



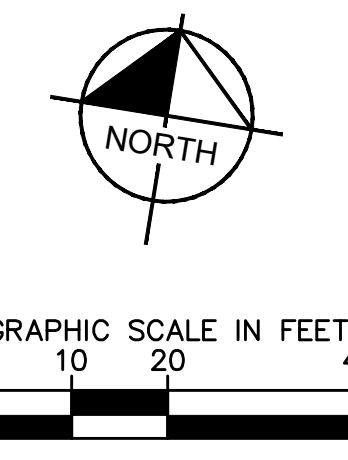
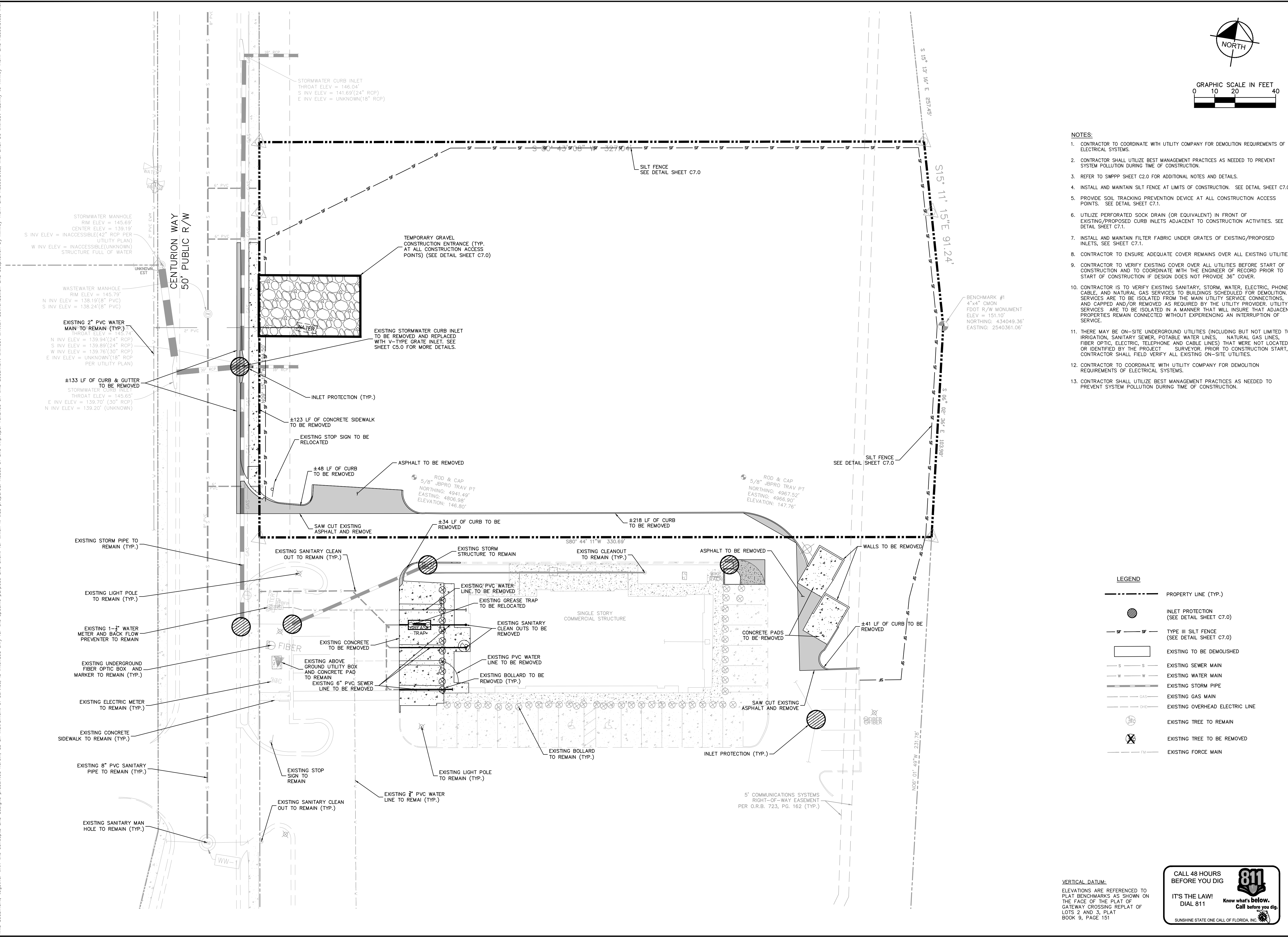
Plotted By: Martell, Theodore - Street - SRT: CIRCLE K - US HWY 90 & I-75 Fuel Expansion - LAYOUT: C2.0 STORMWATER POLLUTION PREVENTION PLAN - October 03, 2022 03:46:18pm - K:\ORL\Civil\49880040-Circle K - US HWY 90 & I-75 CAD\CONST\Plan\Sheet\C2.0-SWPFFP.dwg
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STORMWATER POLLUTION PREVENTION PLAN

<p>SITE DESCRIPTION</p> <p>PROJECT NAME AND LOCATION CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION TAX PARCEL: 24-29-11-281016-000020 CITY OF LAKE CITY, FLORIDA</p> <p>*SEE COVER SHEET FOR LOCATION MAP</p> <p>DEVELOPER NAME AND ADDRESS SCHAFFER CONSTRUCTION, LLC 2601 NETWORK BLVD., SUITE 413 FRISCO, TX 75034 CONTACT: DIANE BERRY PHONE: (407) 580-5173 EMAIL: DBERRY@SCHAFFERCONST.COM</p> <p>PROJECT DESCRIPTION THE PROJECT WILL CONSIST OF CONSTRUCTING A CIRCLE K CONVENIENCE STORE BUILDING EXPANSION WITH HIGH SPEED DIESEL FUELING STATIONS AND SEMI-TRUCK PARKING ON A PREVIOUSLY MASS GRADED SITE. THE PROJECT IS 3.46 ± ACRES LOCATED ON THE NORTHEAST CORNER OF US HIGHWAY 90 AND CENTURION COURT IN LAKE CITY, FLORIDA.</p> <p>PROJECT AREA: 3.46 ACRES CONTRIBUTING DRAINAGE AREA: 3.46 ACRES LONGITUDE: W 82° 41' 26.2" LATITUDE: N 30° 10' 51.1"</p> <p>ACTIVITIES THAT REQUIRE EROSION CONTROL PROVIDING A STABILIZED CONSTRUCTION ENTRANCE, PERIMETER, AND OTHER EROSION AND SEDIMENT CONTROLS; DEMOLITION; SITE GRADING; INSTALLATION OF STORM WATER; CURB, DRIVEWAYS, AND ROADWAY FACILITIES.</p> <p>*SEE PLANS FOR THE LOCATION OF TEMPORARY SEDIMENT BARRIERS AND OTHER EROSION CONTROL METHODS.</p> <p>SOIL PARAMETERS</p> <p>SOIL TYPES:</p> <table border="1" style="width: 100%; margin-left: 20px;"> <tr> <th style="width: 70%;">SERIES NAME</th> <th style="width: 30%;">HYDROLOGIC GROUP</th> </tr> <tr> <td>BLANTON FINE SAND, 0-5% SLOPES</td> <td>A</td> </tr> </table> <p>SEQUENCE OF MAJOR ACTIVITIES</p> <p>THE ORDER OF CONSTRUCTION IS AS FOLLOWS:</p> <ol style="list-style-type: none"> 1. PROVIDE STABILIZED CONSTRUCTION ENTRANCE 2. INSTALL SILT FENCES AND OTHER EROSION CONTROL METHODS 3. DEMOLITION 4. CLEAR AND GRUB FOR SEDIMENT BASIN AND EARTH DIKE 5. CONSTRUCT EARTH DIKE AND SEDIMENT BASIN 6. FINISH CLEARING AND GRUBBING 7. REMOVE AND STORE TOPSOIL 8. PROVIDE INITIAL GRADING AS REQUIRED 9. STABILIZE ALL DISTURBED AREAS AS SOON AS POSSIBLE 10. INSTALL UTILITIES, STORM SEWER, CURB AND GUTTER 11. INSTALL BASE TO ROAD AND DRIVEWAY AREA 12. FINISH GRADING ENTIRE SITE 13. CONSTRUCT FINAL PAVING 14. REMOVE ACCUMULATED SEDIMENT 15. REMOVE ANY ITEMS THAT ARE NOT REQUIRED <p>TIMING OF CONTROL MEASURES</p> <p>THE INSTALLATION OF SILT FENCE (AND OTHER EROSION CONTROL MEASURES), A STABILIZED ENTRANCE AND SEDIMENT BASIN SHALL OCCUR PRIOR TO CLEARING AND GRUBBING ACTIVITY. AFTER CONSTRUCTION IS COMPLETE, THE ACCUMULATED SEDIMENT SHALL BE REMOVED AND THE AREAS SHALL BE REGRADED AND PERMANENTLY STABILIZED AS SHOWN ON THE PLANS.</p>	SERIES NAME	HYDROLOGIC GROUP	BLANTON FINE SAND, 0-5% SLOPES	A	<p>EROSION AND SEDIMENT CONTROLS</p> <p>BEST MANAGEMENT PRACTICES SHALL BE USED FOR THIS PROJECT TO CONTROL EROSION AND TURBIDITY CAUSED BY STORM WATER RUN-OFF. THE LOCATION AND DETAILS OF EROSION CONTROL METHODS ARE SHOWN ON THE PLANS. THE CONTRACTOR IS RESPONSIBLE FOR PLACING AND MAINTAINING THESE CONTROL METHODS AS SHOWN ON THE PLANS OR AS REQUIRED. HE/SHE SHALL ALSO PROVIDE THE REQUIRED EROSION PROTECTION AS REQUIRED BY LOCAL, STATE AND FEDERAL LAW.</p> <p>STORM WATER MANAGEMENT</p> <p>STORMWATER COLLECTION SHALL BE PROVIDED BY DRAINAGE INLETS WITHIN THE PROPOSED DRIVE AISLES. THE PROPOSED DRAINAGE INLETS WILL CONNECT INTO THE EXISTING OFFSITE STORM DRAINAGE COLLECTION SYSTEM, WHICH DRAINS TO AN OFFSITE MASTER STORMWATER POND THAT PROVIDES ATTENUATION FOR THIS SITE. THE POND IS DESIGNED IN ACCORDANCE WITH SRWMD AND LAKE CITY CODE.</p> <p>STABILIZATION PRACTICES:</p> <p>TEMPORARY STABILIZATION - TOPSOIL STOCK PILES AND DISTURBED PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITY TEMPORARILY OR PERMANENTLY CEASE, SHALL BE STABILIZED WITH TEMPORARY SEED AND MULCH WITHIN 7 DAYS OF THE LAST CONSTRUCTION ACTIVITY IN THAT AREA. THE TEMPORARY SEED REQUIRED CAN BE FOUND IN TABLE 1.65 A OF THE FLORIDA DEVELOPMENT MANUAL. PRIOR TO SEEDING, WHERE SOILS ARE ACIDIC 2 TONS OF PULVERIZED AGRICULTURAL LIMESTONE SHOULD BE ADDED PER ACRE AND 450 POUNDS OF 10-20-20 FERTILIZER SHALL BE APPLIED TO EACH ACRE. AFTER SEEDING, EACH AREA SHALL BE IMMEDIATELY MULCHED WITH STRAW OR EQUIVALENT EQUAL. AREAS OF THE SITE WHICH ARE TO BE PAVED SHALL BE TEMPORARILY STABILIZED BY APPLYING GEOTEXTILE AND STONE SUB-BASE UNTIL BITUMINOUS PAVEMENT CAN BE APPLIED.</p> <p>PERMANENT STABILIZATION - DISTURBED PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES PERMANENTLY CEASE SHALL BE STABILIZED WITH PERMANENT SEED NO LATER THAN 7 DAYS AFTER THE LAST CONSTRUCTION ACTIVITY. THE APPROPRIATE PERMANENT SEED MIX CAN BE FOUND IN TABLES 1.66A, 1.66B AND 1.66C OF THE FLORIDA DEVELOPMENT MANUAL. PRIOR TO SEEDING, 2 TONS/ACRE OF FINELY GROUND AGRICULTURAL LIMESTONE AND THE PROPER FERTILIZER BASED ON THE TYPE OF SEEDING SHALL BE APPLIED TO EACH ACRE TO PROVIDE PLANT NUTRIENTS. AFTER SEEDING, EACH AREA SHALL BE MULCHED IMMEDIATELY.</p> <p>STRUCTURAL PRACTICES:</p> <p>EARTH DIKE - IF REQUIRED, AN EARTH DIKE SHALL BE CONSTRUCTED ALONG THE SITE PERIMETER. A PORTION OF THE DIKE SHALL DIVERT RUN-ON AROUND THE CONSTRUCTION SITE. THE REMAINING PORTION OF THE DIKE SHALL COLLECT RUNOFF FROM THE DISTURBED AREA AND DIRECT THE RUNOFF TO THE SEDIMENT BASIN.</p> <p>SEDIMENT BASIN - A SEDIMENT BASIN SHALL BE CONSTRUCTED IN THE COMMON DRAINAGE AREA FOR THE SITE. ALL SEDIMENT COLLECTED IN THE BASIN MUST BE REMOVED FROM THE BASIN UPON COMPLETION OF CONSTRUCTION. SEDIMENT FROM THE BASIN MAY BE USED AS FILL ON THE SITE IF IT IS SUITABLE SOIL.</p> <p>WASTE DISPOSAL</p> <p>WASTE MATERIALS - ALL WASTE MATERIALS SHALL BE COLLECTED AND STORED IN A METAL DUMPSTER WITH A SECURE LID IN ACCORDANCE WITH ALL LOCAL AND STATE LAWS. ALL TRASH AND CONSTRUCTION DEBRIS FROM THE SITE SHALL BE DEPOSITED IN THE DUMPSTER. THE SUPERINTENDENT SHALL COORDINATE WITH THE LOCAL UTILITIES TO HAVE THE DUMPSTER EMPTIED AT LEAST TWICE A WEEK AND THE WASTE TAKEN TO AN APPROPRIATE LANDFILL. NO CONSTRUCTION WASTE MATERIALS SHALL BE BURIED ON SITE. THE SUPERINTENDENT SHALL ORGANIZE TRAINING FOR THE EMPLOYEES IN THE PROPER PRACTICES WHEN DEALING WITH WASTE MATERIALS. THE SUPERINTENDENT SHALL BE RESPONSIBLE FOR POSTING AND ENFORCING WASTE MATERIAL PROCEDURES.</p> <p>HAZARDOUS WASTE - HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN ACCORDANCE WITH ALL LOCAL AND STATE LAWS OR AS DIRECTED BY THE MANUFACTURER. THE SUPERINTENDENT SHALL ORGANIZE THE PROPER TRAINING FOR EMPLOYEES IN THE PROPER PRACTICES WHEN DEALING WITH HAZARDOUS WASTE MATERIALS. THESE PROCEDURES SHALL BE POSTED ON THE SITE. THE PERSON WHO MANAGES THE SITE SHALL BE RESPONSIBLE FOR ENFORCING THE PROCEDURES.</p> <p>SANITARY WASTE - SANITARY WASTE SHALL BE COLLECTED AND DISPOSED OF IN ACCORDANCE WITH ALL LOCAL AND STATE LAWS. THE SUPERINTENDENT SHALL COORDINATE WITH THE LOCAL UTILITY FOR COLLECTION OF THE SANITARY WASTE AT LEAST THREE TIMES A WEEK TO PREVENT SPILLAGE ONTO THE SITE.</p> <p>OFF-SITE TRACKING</p> <p>A STABILIZED CONSTRUCTION ENTRANCE SHALL BE PROVIDED TO REDUCE SEDIMENT TRACKING OFFSITE. THE MAJOR ROAD CONNECTED TO THE PROJECT SHALL BE CLEANED ONCE A DAY TO REMOVE ANY EXCESS MUD, DIRT OR ROCK RESULTING FROM CONSTRUCTION TRAFFIC. ALL TRUCKS HAULING MATERIALS OFFSITE SHALL BE COVERED WITH A TARP/AULIN.</p>	<p>ITEMS REQUIRING POLLUTION PREVENTION</p> <p>THE FOLLOWING ITEMS ARE EXPECTED TO BE PRESENT ON THE PROJECT SITE:</p> <ul style="list-style-type: none"> -ASPHALT -CONCRETE -FERTILIZERS -METAL PIECES -PETROLEUM BASED PRODUCTS -TAR -CLEANING SUPPLIES -DETERGENTS -MASONARY BLOCK/BRICKS -PAINT -WOOD <p>THE FOLLOWING ARE NON-STORM WATER SOURCES THAT WILL BE ENCOUNTERED AT THE SITE AND SHOULD BE DIRECTED TO THE SEDIMENT BASIN PRIOR TO DISCHARGE:</p> <ul style="list-style-type: none"> -UNCONTAMINATED GROUNDWATER EXPOSED DURING EXCAVATION -WATER FROM WATER LINE FLUSHING -PAVEMENT WASH WATERS (WHERE NO SPILLS OR LEAKS OF TOXIC OR HAZARDOUS MATERIALS HAVE OCCURRED). <p>SPILL PREVENTION AND CONTROL</p> <p>THE FOLLOWING ARE THE MATERIAL MANAGEMENT PRACTICES THAT WILL BE USED TO REDUCE THE RISK OF SPILLS OR OTHER ACCIDENTAL EXPOSURE OF MATERIALS AND SUBSTANCES TO STORM WATER RUNOFF.</p> <p>GOOD HOUSEKEEPING</p> <p>SUPERINTENDENT SHALL INSPECT PROJECT AREA DAILY FOR PROPER STORAGE, USE, AND DISPOSAL OF CONSTRUCTION MATERIALS.</p> <p>STORE ONLY ENOUGH MATERIAL ON SITE FOR PROJECT COMPLETION.</p> <p>ALL SUBSTANCES SHOULD BE USED BEFORE DISPOSAL OF CONTAINER.</p> <p>ALL CONSTRUCTION MATERIALS STORED SHALL BE ORGANIZED AND IN THE PROPER CONTAINER AND IF POSSIBLE, STORED UNDER A ROOF OR PROTECTIVE COVER.</p> <p>PRODUCTS SHALL NOT BE MIXED UNLESS DIRECTED BY THE MANUFACTURER.</p> <p>ALL PRODUCTS SHALL BE USED AND DISPOSED OF ACCORDING TO THE MANUFACTURER'S RECOMMENDATIONS.</p> <p>HAZARDOUS PRODUCTS</p> <p>MATERIALS SHOULD BE KEPT IN ORIGINAL CONTAINER WITH LABELS UNLESS THE ORIGINAL CONTAINERS CANNOT BE RESEALED. IF ORIGINAL CONTAINERS CANNOT BE USED, LABELS AND PRODUCT INFORMATION SHALL BE SAVED.</p> <p>PROPER DISPOSAL PRACTICES SHALL ALWAYS BE FOLLOWED IN ACCORDANCE WITH MANUFACTURER AND LOCAL/STATE REGULATIONS.</p> <p>PRODUCT SPECIFIC PRACTICES</p> <p>PETROLEUM PRODUCTS MUST BE STORED IN PROPER CONTAINERS AND CLEARLY LABELED. VEHICLES CONTAINING PETROLEUM PRODUCTS SHALL BE PERIODICALLY INSPECTED FOR LEAKS. PRECAUTIONS SHALL BE TAKEN TO AVOID LEAKAGE OF PETROLEUM PRODUCTS ON SITE.</p> <p>THE MINIMUM AMOUNT OF FERTILIZER SHALL BE USED AND MIXED INTO THE SOIL IN ORDER TO LIMIT EXPOSURE TO STORM WATER. FERTILIZERS SHALL BE STORED IN A COVERED SHED. THE CONTENTS OF ANY PARTIALLY USED BAGS OF FERTILIZER SHALL BE TRANSFERRED TO A SEALABLE PLASTIC BIN TO AVOID SPILLS.</p> <p>PAINT CONTAINERS SHALL BE SEALED AND STORED WHEN NOT IN USE. EXCESS PAINT MUST BE DISPOSED OF IN AN APPROVED MANNER.</p> <p>CONCRETE TRUCKS SHALL NOT BE ALLOWED TO WASH OUT OR DISCHARGE SURPLUS CONCRETE OR DRUM WASH WATER ON THE SITE.</p> <p>SPILL CONTROL PRACTICES</p> <p>IN ADDITION TO THE GOOD HOUSEKEEPING AND MATERIAL MANAGEMENT PRACTICES DISCUSSED IN THE PREVIOUS SECTIONS OF THIS PLAN, THE FOLLOWING PRACTICES SHALL BE FOLLOWED FOR SPILL PREVENTION AND CLEANUP:</p> <ul style="list-style-type: none"> -SPILL CLEANUP INFORMATION SHALL BE POSTED ON SITE TO INFORM EMPLOYEES ABOUT CLEANUP PROCEDURES AND RESOURCES. -THE FOLLOWING CLEAN-UP EQUIPMENT MUST BE KEPT ON-SITE NEAR THE MATERIAL STORAGE AREA: GLOVES, MOPS, RAGS, BROOMS, DUST PANS, SAND, SAWDUST, LIQUID ABSORBER, GOGGLES, AND TRASH CONTAINERS. -ALL SPILLS SHALL BE CLEANED UP AS SOON AS POSSIBLE. -WHEN CLEANING A SPILL, THE AREA SHOULD BE WELL VENTILATED AND THE EMPLOYEE SHALL WEAR PROPER PROTECTIVE COVERING TO PREVENT INJURY. -TOXIC SPILLS MUST BE REPORTED TO THE PROPER AUTHORITY REGARDLESS OF THE SIZE OF THE SPILL. -AFTER A SPILL, THE PREVENTION PLAN SHALL BE REVIEWED AND CHANGED TO PREVENT FURTHER SIMILAR SPILLS FROM OCCURRING. THE CAUSE OF THE SPILL, MEASURES TO PREVENT IT, AND HOW TO CLEAN THE SPILL UP SHALL BE RECORDED. -THE SUPERINTENDENT SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR AND IS RESPONSIBLE FOR THE DAY TO DAY SITE OPERATIONS. THE SUPERINTENDENT ALSO OVERSEES THE SPILL PREVENTION PLAN AND SHALL BE RESPONSIBLE FOR EDUCATING THE EMPLOYEES ABOUT SPILL PREVENTION AND CLEANUP PROCEDURES. 	<p>MAINTENANCE AND INSPECTION PRACTICES</p> <p>THE FOLLOWING ARE MAINTENANCE AND INSPECTION PRACTICES THAT SHALL BE COMPLETED BY THE CONTRACTOR:</p> <ul style="list-style-type: none"> -ALL SEDIMENT AND EROSION CONTROL METHODS SHALL BE INSPECTED AT LEAST ONCE EVERY SEVEN (7) CALENDAR DAYS AND WITHIN 24 HOURS OF THE END OF A STORM THAT IS 0.50 INCHES OR GREATER BY A QUALIFIED INSPECTOR. -ALL SEDIMENT AND EROSION CONTROL METHODS SHALL BE KEPT IN GOOD CONDITION. REPAIRS MUST BE MADE WITHIN 7 CALENDAR DAYS OF INSPECTION. -THE SILT FENCE SHALL BE INSPECTED PERIODICALLY FOR HEIGHT OF SEDIMENT AND CONDITION OF FENCE. -THE SILT FENCE SHALL BE CLEARED OF SEDIMENT WHEN SEDIMENT MEASURES ONE-THIRD THE HEIGHT OF THE FENCE. -THE SEDIMENT BASINS/DITCHES SHALL BE CHECKED PERIODICALLY FOR DEPTH OF SEDIMENT. THEY SHALL BE CLEANED WHEN SEDIMENT REACHES 10% OF TOTAL CAPACITY AND AFTER CONSTRUCTION IS COMPLETE. -ALL SEEDING SHALL BE CHECKED FOR PROPER GROWTH AND UNIFORMITY. UNSTABILIZED AREAS SHALL BE RE-SODDED. -A MAINTENANCE REPORT SHALL BE COMPLETED DAILY AFTER EACH INSPECTION OF THE SEDIMENT AND EROSION CONTROL METHODS. THE REPORTS SHALL BE FILED IN AN ORGANIZED MANNER AND RETAINED ON-SITE DURING CONSTRUCTION. AFTER CONSTRUCTION IS COMPLETED, THE REPORTS SHALL BE SAVED FOR AT LEAST THREE YEARS. THE REPORTS SHALL BE AVAILABLE FOR ANY AGENCY THAT HAS JURISDICTION OVER EROSION CONTROL. -THE SUPERINTENDENT SHALL ORGANIZE THE TRAINING FOR INSPECTION PROCEDURES AND PROPER EROSION CONTROL METHODS FOR EMPLOYEES THAT COMPLETE INSPECTIONS AND REPORTS. <p>POLLUTION PREVENTION PLAN CERTIFICATION</p> <p>I CERTIFY UNDER PENALTY OF LAW THAT THIS DOCUMENT AND ALL ATTACHMENTS WERE PREPARED UNDER MY DIRECTION OR SUPERVISION IN ACCORDANCE WITH A SYSTEM DESIGNED TO ASSURE THAT QUALIFIED PERSONNEL PROPERLY GATHERED AND EVALUATED THE INFORMATION SUBMITTED. BASED ON MY INQUIRY OF THE PERSON OR PERSONS WHO MANAGE THE SYSTEM, OR THOSE PERSONS DIRECTLY RESPONSIBLE FOR GATHERING THE INFORMATION, THE INFORMATION SUBMITTED IS, TO THE BEST OF MY KNOWLEDGE AND BELIEF, TRUE, ACCURATE, AND COMPLETE. I AM AWARE THAT THERE ARE SIGNIFICANT PENALTIES FOR SUBMITTING FALSE INFORMATION, INCLUDING THE POSSIBILITY OF FINE AND IMPRISONMENT FOR KNOWING VIOLATIONS.</p> <p>SIGNED: _____ DATE: _____</p> <p style="text-align: right;">JAROD C. STUBBS, P.E. FLORIDA REGISTRATION NUMBER: 89387 PROFESSIONAL ENGINEER</p> <p>CONTRACTOR'S CERTIFICATION</p> <p>I CERTIFY UNDER PENALTY OF LAW THAT I UNDERSTAND, SHALL COMPLY WITH, THE TERMS AND CONDITIONS OF THE STATE OF FLORIDA GENERIC PERMIT FOR STORMWATER DISCHARGE FORM LARGE AND SMALL CONSTRUCTION ACTIVITIES AND THIS STORMWATER POLLUTION PREVENTION PLAN PREPARED THEREUNDER.</p> <table border="1" style="width: 100%; margin-top: 10px;"> <thead> <tr> <th style="width: 25%;">SIGNATURE AND DATE</th> <th style="width: 40%;">NAME AND TITLE, COMPANY / ADDRESS AND TELEPHONE NUMBER</th> <th style="width: 35%;">RESPONSIBILITY</th> </tr> </thead> <tbody> <tr><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td></tr> </tbody> </table>	SIGNATURE AND DATE	NAME AND TITLE, COMPANY / ADDRESS AND TELEPHONE NUMBER	RESPONSIBILITY															
SERIES NAME	HYDROLOGIC GROUP																								
BLANTON FINE SAND, 0-5% SLOPES	A																								
SIGNATURE AND DATE	NAME AND TITLE, COMPANY / ADDRESS AND TELEPHONE NUMBER	RESPONSIBILITY																							

LICENSED PROFESSIONAL ENGINEER STATE OF FLORIDA NO. 89387									
KHA PROJECT 149880040	DATE 05/04/2022	SCALE AS SHOWN	DRAWN BY E.J.F.	CHECKED BY J.C.S.	DATE:	STORMWATER POLLUTION PREVENTION PLAN CITY OF LAKE CITY, FLORIDA			
CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION									
SHEET NUMBER C2.0									
REVISIONS No. _____ DATE _____									

Plotted By: Mariell, Theodore - Sheet: SET-CIRCLE K - US HWY 90 & I-75 High Speed Diesel Fuel Expansion - Layout: C3.0 EXISTING CONDITIONS & DEMOLITION PLAN - October 03, 2022 03:46:43pm - K:\VRL\CIVIL\149880040-Circle K USRD & I75 HADD CONSTRUCTION\PlanSheets\C3.0 - EXISTING CONDITIONS & DEMO PLAN.dwg
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- NOTES:**
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 - INSTALL AND MAINTAIN SILT FENCE AT LIMITS OF CONSTRUCTION. SEE DETAIL SHEET C7.0.
 - PROVIDE SOIL TRACKING PREVENTION DEVICE AT ALL CONSTRUCTION ACCESS POINTS. SEE DETAIL SHEET C7.1.
 - UTILIZE PERFORATED SOCK DRAIN (OR EQUIVALENT) IN FRONT OF EXISTING/PROPOSED CURB INLETS ADJACENT TO CONSTRUCTION ACTIVITIES. SEE DETAIL SHEET C7.1.
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LEGEND

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	TYPE III SILT FENCE (SEE DETAIL SHEET C7.0)
	EXISTING TO BE DEMOLISHED
	EXISTING SEWER MAIN
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	EXISTING STORM PIPE
	EXISTING GAS MAIN
	EXISTING OVERHEAD ELECTRIC LINE
	EXISTING TREE TO REMAIN
	EXISTING TREE TO BE REMOVED
	EXISTING FORCE MAIN

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LICENSED PROFESSIONAL ENGINEER
 KHA PROJECT 149880040
 DATE 05/04/2022
 SCALE AS SHOWN
 DESIGNED BY E/JF
 DRAWN BY E/JF
 CHECKED BY JCS
 DATE: 05/04/2022

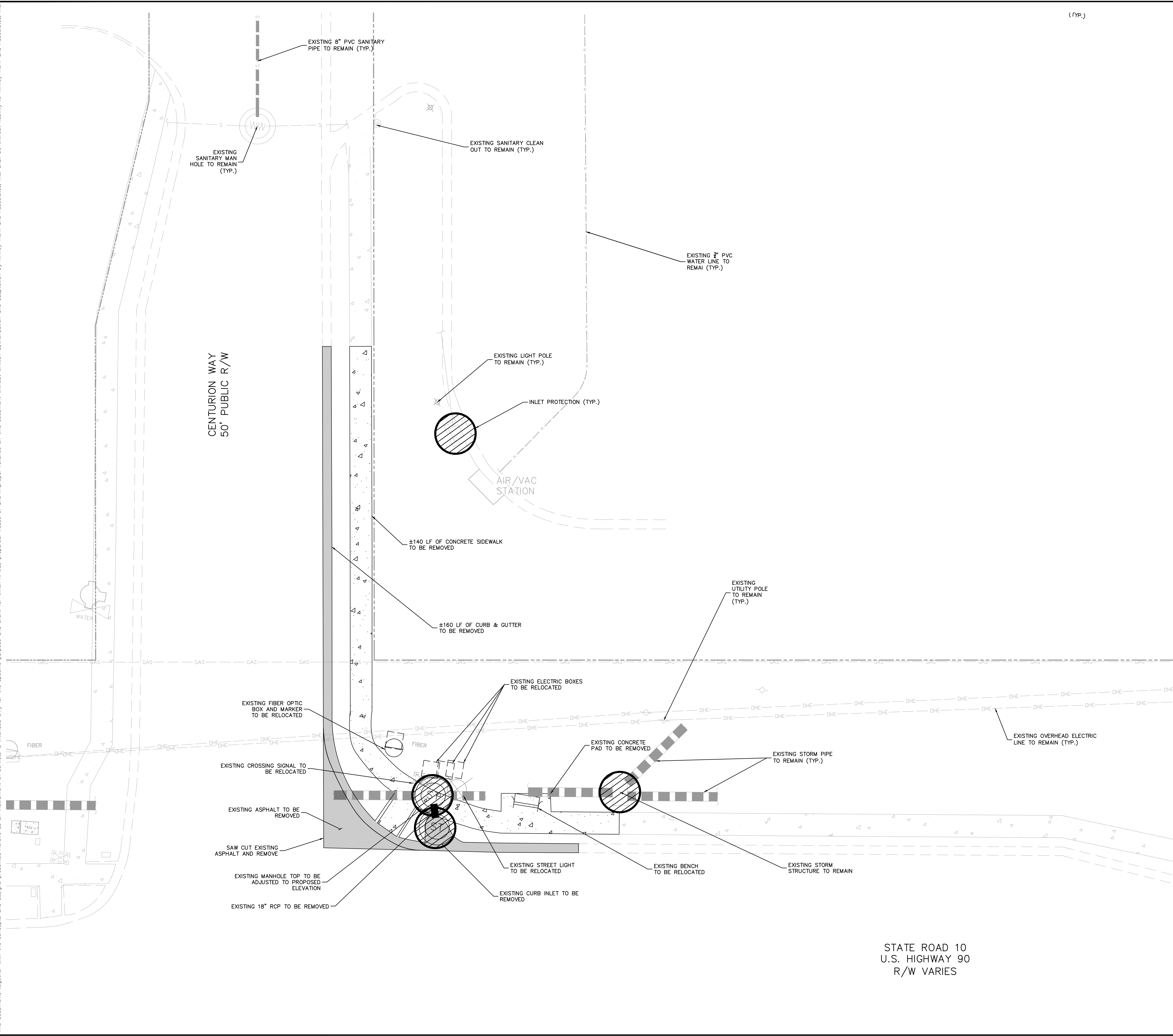
EXISTING CONDITIONS & DEMOLITION PLAN

CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION
 CITY OF LAKE CITY FLORIDA
 SHEET NUMBER **C3.0**

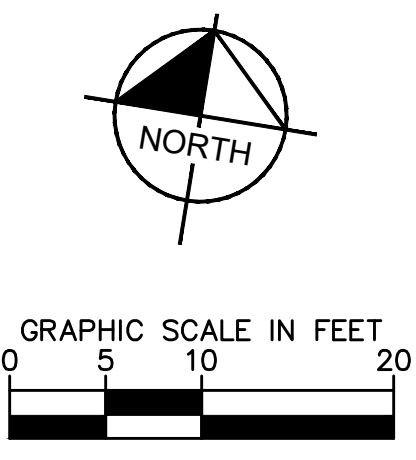
VERTICAL DATUM:
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(NYP.)



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- PROPERTY LINE (TYP.)
- INLET PROTECTION (SEE DETAIL SHEET C7.0)
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LICENSED PROFESSIONAL ENGINEER
 STATE OF FLORIDA
 PROFESSIONAL REGISTRATION NO. 12088
 DATE: 05/04/2022

EXISTING CONDITIONS & DEMOLITION PLAN

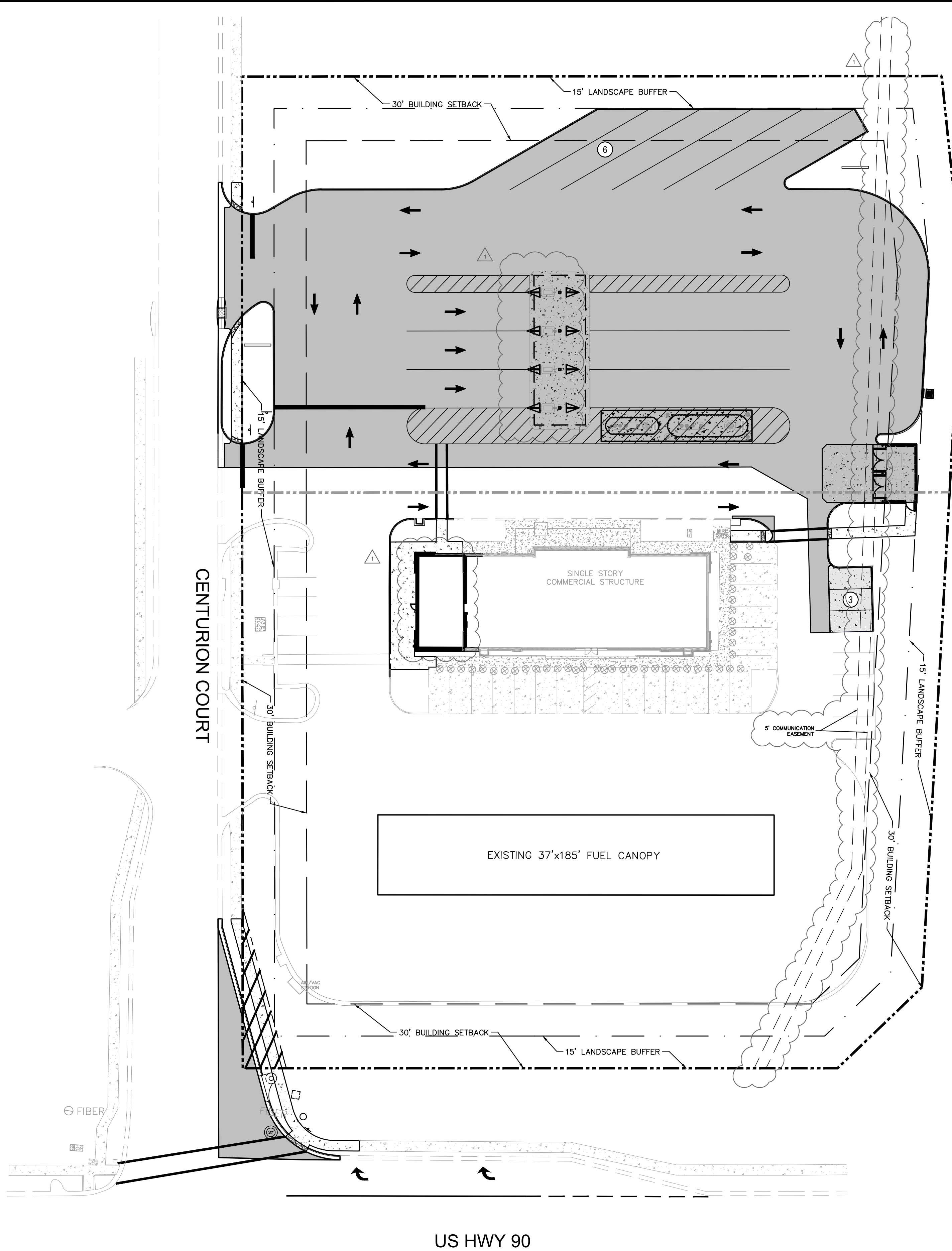
CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION
 CITY OF LAKE CITY FLORIDA

SHEET NUMBER **C3.1**

STATE ROAD 10
U.S. HIGHWAY 90
R/W VARIES

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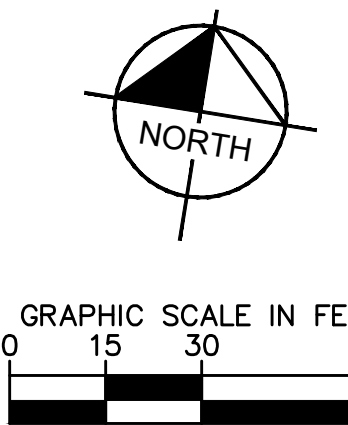


INTERSTATE 75 OFF-RAMP

US HWY 90

Circle K - U.S. 90 & I-75 Impervious Area Calculations

Lot #	Total Lot Area (AC)	Existing Pervious Area (SF)	Existing Impervious Area (SF)	Proposed Pervious Area (SF)	Proposed Impervious Area (SF)	Allowed Impervious Area (SF)
1	1.979	22952.000	63254.000	22483.000	63723.000	64904.4
2	1.223	49218.910	4049.09	6900.31	46367.69	40075.2
3	0.263	11464.200	0	8711.37	2752.830	8581.32
Total (SF)	150938.200	83635.110	67303.09	38094.68	112843.520	113560.920
Total (AC)	3.465	1.920	1.545	0.875	2.591	2.607
%	100.000%	55.410%	44.590%	25.239%	74.761%	75.237%



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 - REFER TO ARCHITECTURAL AND STRUCTURAL PLANS TO VERIFY ALL BUILDING DIMENSIONS.
 - REFER TO SIGNAGE PLANS FOR MONUMENT SIGN DETAILS.
 - SEE MEP PLANS FOR ELECTRICAL DRAWINGS.
 - ALL PROPOSED ON-SITE STRIPING AND PAVEMENT MARKING WILL BE PAINTED UNLESS OTHERWISE NOTED AND IN ACCORDANCE WITH FDOT INDEX 711-001.
 - REFER TO ARCHITECTURAL PLANS FOR PROPOSED TRASH CAN LOCATIONS AND DESIGN.
 - BOLLARDS IN SIDEWALK ADJACENT TO BUILDING SHALL BE COVERED WITH RED PLASTIC COVERS TO BE SUPPLIED BY CONTRACTOR.
 - BOLLARDS UNDER CANOPY SHALL BE COVERED WITH GRAY PLASTIC COVERS TO BE SUPPLIED BY CONTRACTOR (SEE FUEL PUMP DESIGNER PLANS FOR MORE DETAIL).
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 - ALL SIGNAGE AND PAVEMENT MARKINGS SHALL MEET MUTCD AND FDOT STANDARDS.
 - ALL SIGNAGE SHALL MEET THE REQUIREMENTS OF CITY OF LAKE CITY LAND DEVELOPMENT CODE, CHAPTER 7, SEC. 760

SITE DATA:

PROJECT AREA:	3.465± ACRES (150,938 SF)
FUTURE LAND USE:	COMMERCIAL
EXISTING ZONING:	CHI - COMMERCIAL, HIGHWAY INTERCHANGE
EXISTING USE:	UNDEVELOPED
PROPOSED USE:	COMMERCIAL
BUILDING HEIGHT:	1 STORY / <35 FT
FAR:	0.0388
EXISTING PERVIOUS AREA:	83,635 SF (1.92 AC) (55.41%)
PROPOSED PERVIOUS AREA:	38,094 SF (0.875 AC) (25.239%)
PROPOSED IMPERVIOUS AREA:	5,863 SF (0.135 AC) (3.884%)
(EXISTING BLDG+EXPANSION)	
ASPHALT/CONCRETE AREA:	±106,980.52 SF (2.456 AC) (70.877%)
(EXISTING+HSD EXPANSION)	
TOTAL IMPERVIOUS AREA:	±112,843.52 SF (2.591 AC) (74.761%)

PARKING REQUIRED

CONVENIENCE STORE (WITH GAS STATION)	
1 SPACES / 150 SF NON-STORAGE AREA (5,043 SF)	34
TOTAL REQUIRED PARKING	34

PARKING PROVIDED

PROPOSED HANDICAP SPACES:	2
PROPOSED REGULAR SPACES:	31
PROPOSED SEMI TRUCK SPACES:	6
PROPOSED ON-SITE SPACES:	39

BICYCLE PARKING

REQUIRED SPACES:	0
PROVIDED SPACES:	4

BUILDING SETBACKS

	REQUIRED	PROVIDED
SIDE (WEST):	30 FT	83 FT
REAR (NORTH):	30 FT	220 FT
FRONT (SOUTH):	30 FT	196 FT
SIDE (EAST):	30 FT	107 FT

LANDSCAPE SETBACKS

	REQUIRED	PROVIDED
SIDE (WEST):	15 FT	15 FT
REAR (NORTH):	15 FT	15 FT
FRONT (SOUTH):	N/A	0 FT
SIDE (EAST):	15 FT	15 FT

LEGEND

- PROPERTY LINE (TYP.)
- PROPOSED ASPHALT PAVEMENT (SEE DETAIL SHEET C7.0)
- PROPOSED CONCRETE SIDEWALK (SEE DETAIL SHEET C7.0)
- PROPOSED MEDIUM DUTY CONCRETE (SEE DETAIL SHEET C7.0)
- PROPOSED HEAVY DUTY CONCRETE (SEE DETAIL SHEET C7.0)

VERTICAL DATUM:

ELEVATIONS ARE REFERENCED TO PLAT BENCHMARKS AS SHOWN ON THE FACE OF THE PLAT OF GATEWAY CROSSING REPLAT OF LOTS 2 AND 3, PLAT BOOK 9, PAGE 151

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LICENSED PROFESSIONAL ENGINEER
 STATE OF FLORIDA
 PROFESSIONAL REGISTRATION NUMBER: 12083
 EXPIRES: 08/31/2025

KHA PROJECT: 149880040
 DATE: 05/04/2022
 SCALE: AS SHOWN
 DESIGNED BY: E/JF
 DRAWN BY: E/JF
 CHECKED BY: JCS

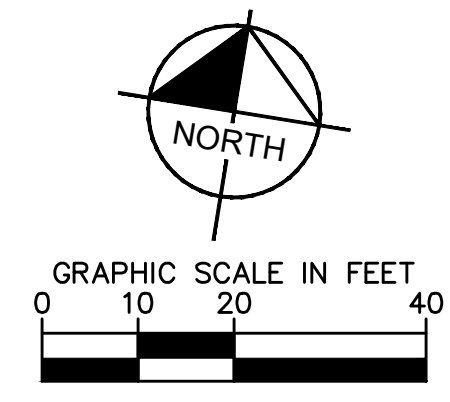
OVERALL SITE PLAN

CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION

CITY OF LAKE CITY FLORIDA

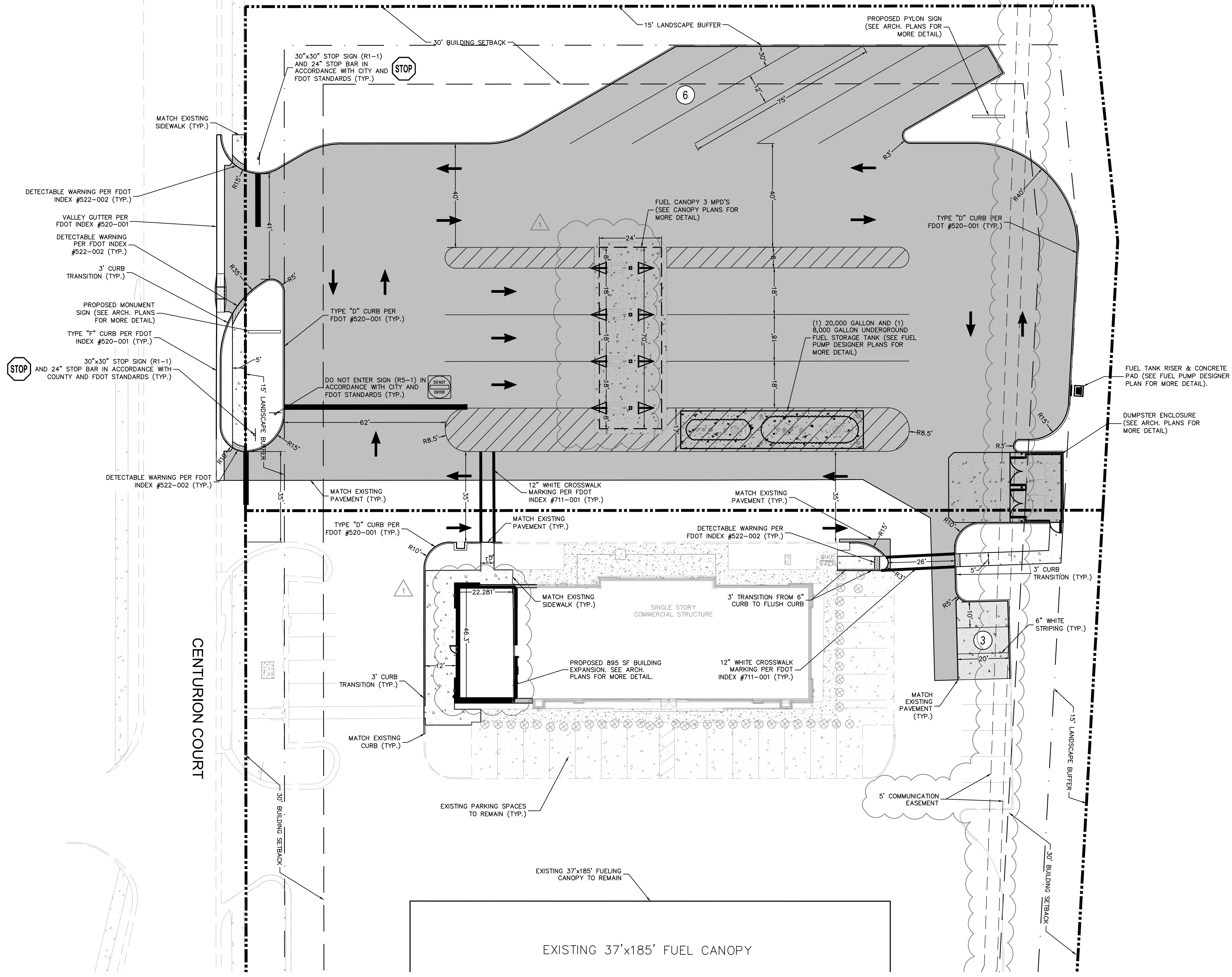
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INTERSTATE 75 OFF-RAMP

LEGEND

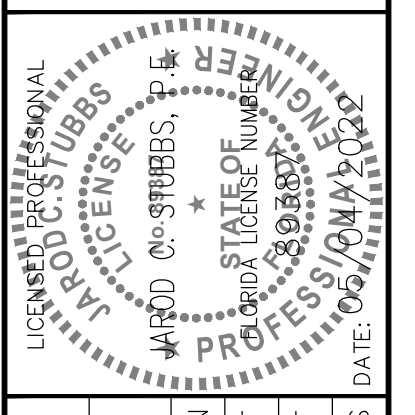
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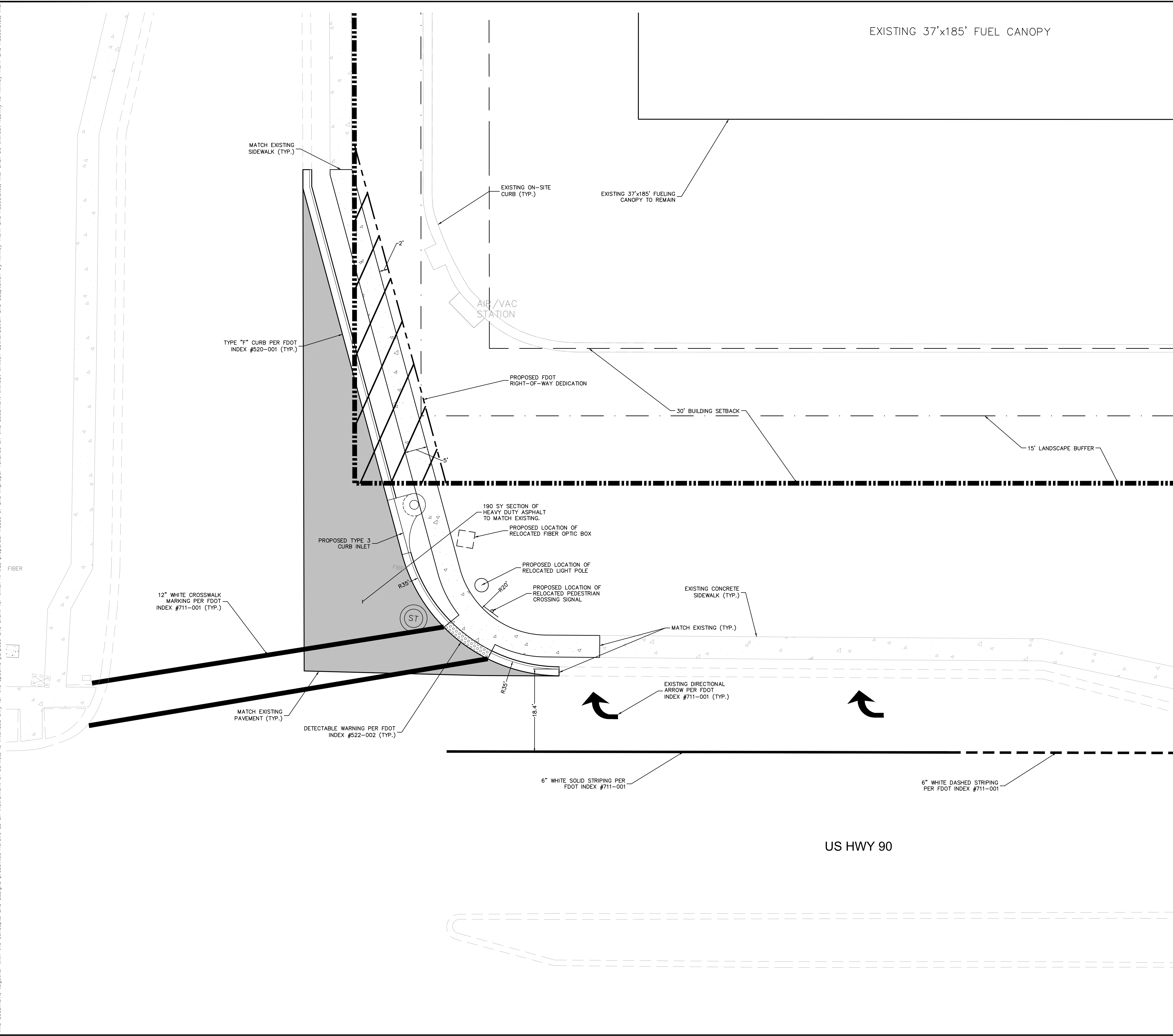
SITE PLAN

CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION

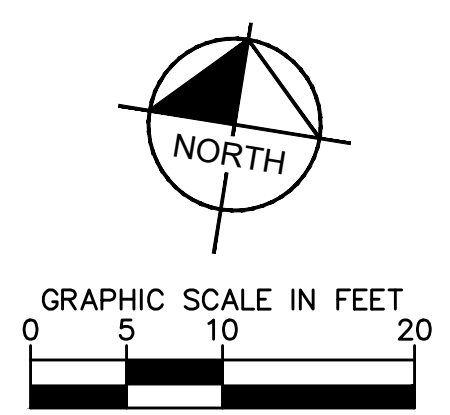
CITY OF LAKE CITY FLORIDA

SHEET NUMBER **C4.1**

Plotted By: Mariell, Theodore - Sheet Set: CIRCLE K - US HWY 90 & I-75 High Speed Diesel Fuel Expansion - Layout: C4.2 INTERCHANGE MODIFICATIONS PLAN - October 03, 2022 - 03:47:10pm - K:\ORL_Civil\149880040-Circle K US90 & I75 CAD\CADD\CONSTR\PlanSheets\C4.0 - SITE PLAN.dwg
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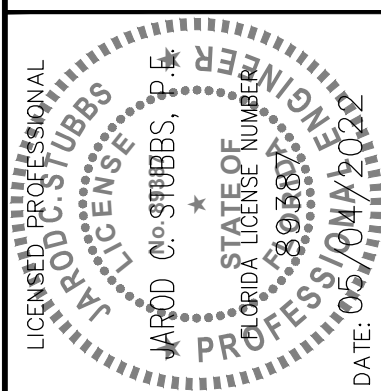
EXISTING 37'x185' FUEL CANOPY



- NOTES:**
1. ALL CURB DIMENSIONS ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
 2. BUILDING DIMENSIONS ARE TO THE OUTSIDE FACE OF BUILDING UNLESS OTHERWISE NOTED.
 3. ALL PROPOSED ON-SITE STRIPING AND PAVEMENT MARKING WILL BE PAINTED UNLESS OTHERWISE NOTED AND IN ACCORDANCE WITH FDOT INDEX 711-001.
 4. ALL SIGNAGE AND PAVEMENT MARKINGS SHALL MEET MUTCD AND FDOT STANDARDS.

NO.	REVISIONS	DATE	BY
1	ARCHITECTURE REVISION	08/29/22	AG

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KHA PROJECT	149880040
DATE	05/04/2022
SCALE	AS SHOWN
DESIGNED BY	EJF
DRAWN BY	EJF
CHECKED BY	JCS

**INTERSECTION
 MODIFICATION PLAN**

**CIRCLE K - US HWY
 90 & I-75 FUEL
 EXPANSION**
 CITY OF LAKE CITY FLORIDA
 SHEET NUMBER
C4.2

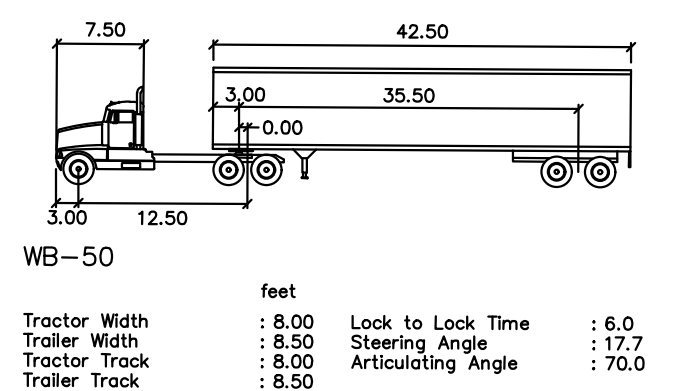
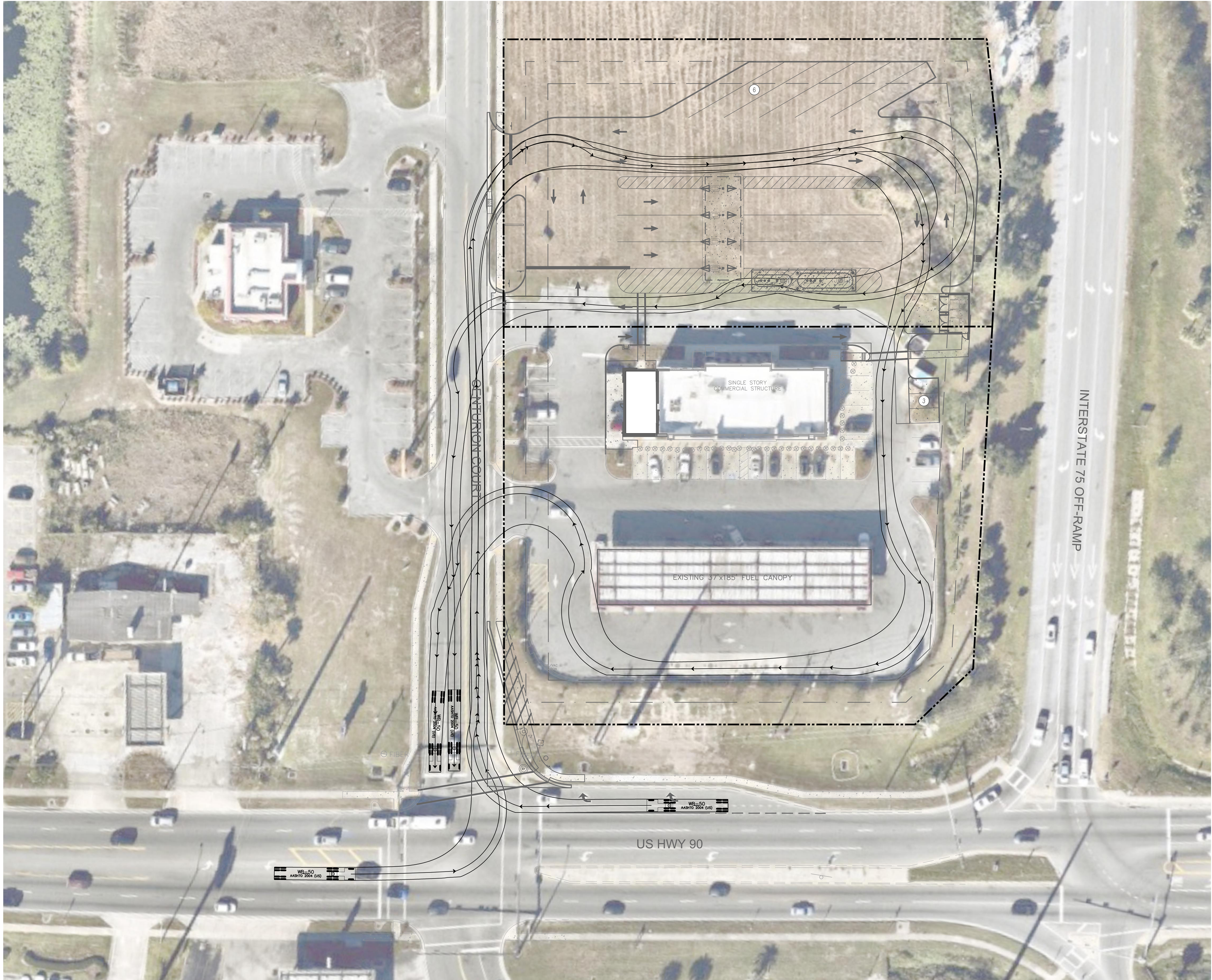
LEGEND

- PROPERTY LINE (TYP.)
- PROPOSED ASPHALT PAVEMENT (SEE DETAIL SHEET C7.0)
- PROPOSED CONCRETE SIDEWALK (SEE DETAIL SHEET C7.0)
- PROPOSED MEDIUM DUTY CONCRETE (SEE DETAIL SHEET C7.0)
- PROPOSED HEAVY DUTY CONCRETE (SEE DETAIL SHEET C7.0)

VERTICAL DATUM:
 ELEVATIONS ARE REFERENCED TO PLAT BENCHMARKS AS SHOWN ON THE FACE OF THE PLAT OF GATEWAY CROSSING REPLAT OF LOTS 2 AND 3, PLAT BOOK 9, PAGE 151

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Plotted By: Mariella, Theodore. Sheet Set: CIRCLE K - US HWY 90 & I-75 High Speed Diesel Fuel Expansion. Layout: C4.3 FUEL TRUCK MOVEMENTS. October 03, 2022. 03:47:19pm. K:\ORL\CIVIL\49880040-Circle K US90 & I75\CADD\CONSTR\PlanSheets\C4.0 - SITE PLAN.dwg
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Tractor Width	Tractor Track	Trailer Width	Trailer Track	Lock to Lock Time	Steering Angle	Articulating Angle
: 8.00	: 8.50	: 8.00	: 8.50	: 6.0	: 17.7	: 70.0

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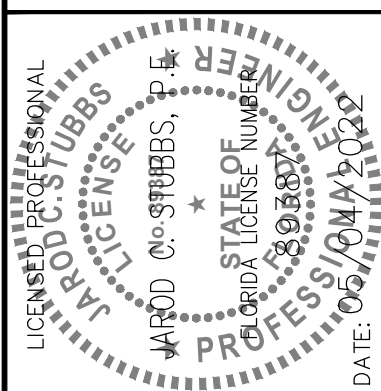
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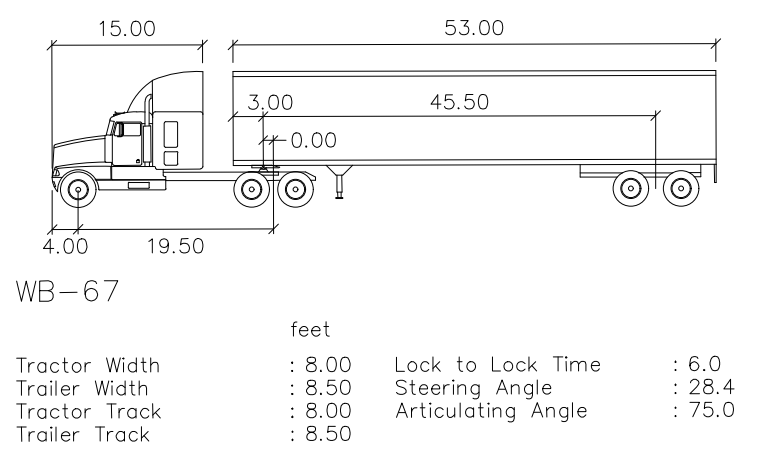


KHA PROJECT	DATE	DESIGNED BY	DRAWN BY	CHECKED BY
149880040	05/04/2022	EJF	EJF	JCS

TRUCK TURNING MOVEMENTS

CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION

Plotted By: Martell, Theodore - Sheet Set: CIRCLE K - US HWY 90 & I-75 High Speed Diesel Fuel Expansion - Layout: C4.4 FUELING TRUCK MOVEMENTS - October 03, 2022 03:47:24pm - K:\VRL_Civil\149880040-Circle K USRD & I-75\CADD\CONSTR\PlanSheets\C4.0 - SITE PLAN.dwg
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LICENSED PROFESSIONAL ENGINEER
 STATE OF FLORIDA
 PROFESSIONAL REGISTRATION NUMBER: 120883
 DATE: 05/04/2022

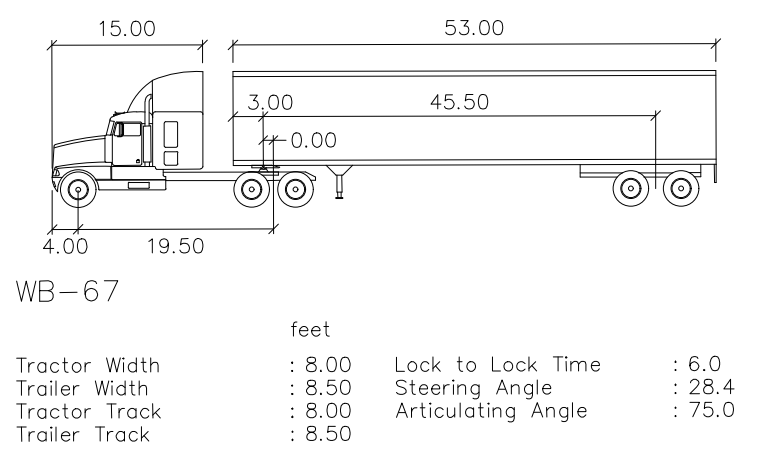
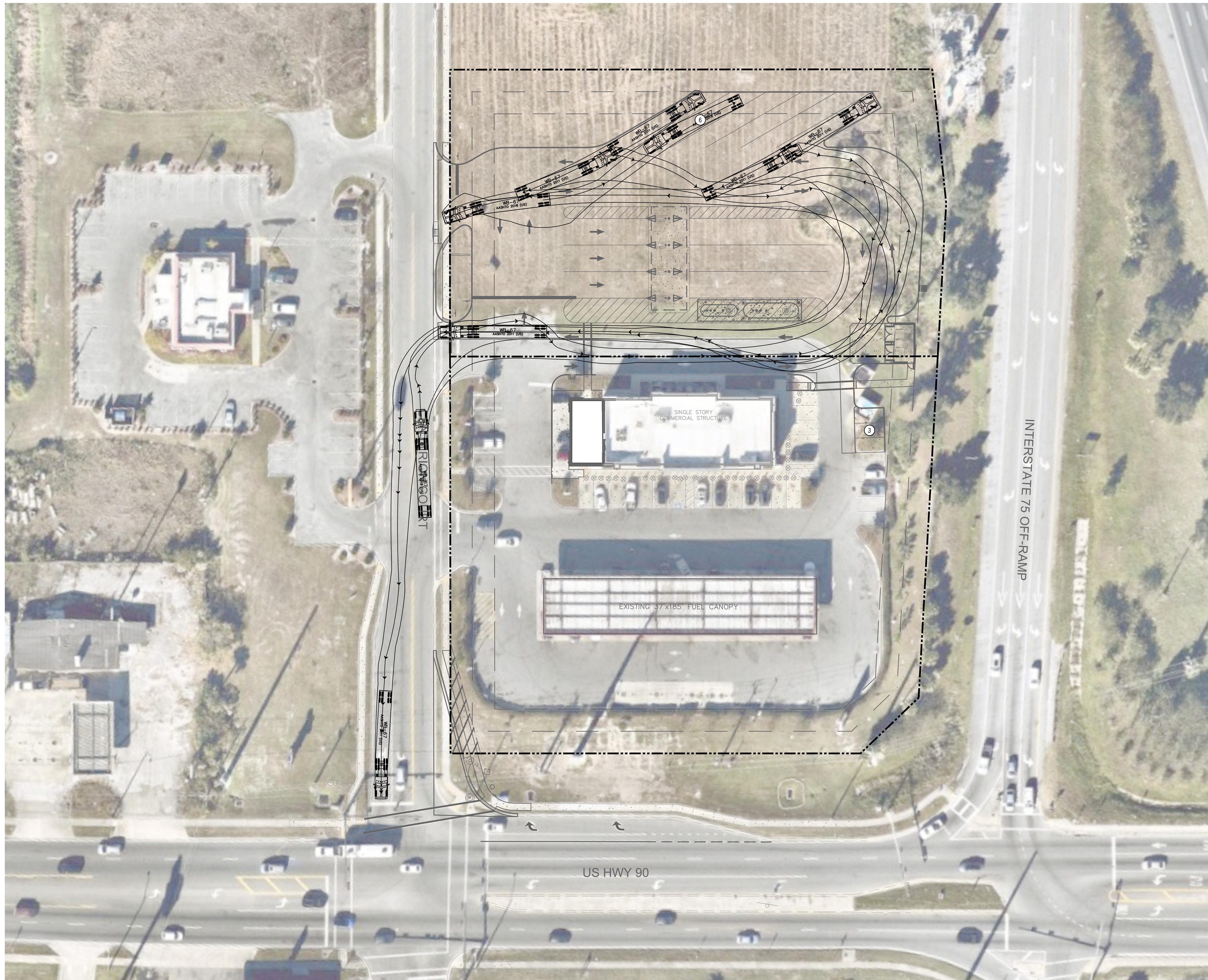
KHA PROJECT	149880040
DATE	05/04/2022
SCALE	AS SHOWN
DESIGNED BY	EJF
DRAWN BY	EJF
CHECKED BY	JCS

TRUCK TURNING MOVEMENTS

CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION

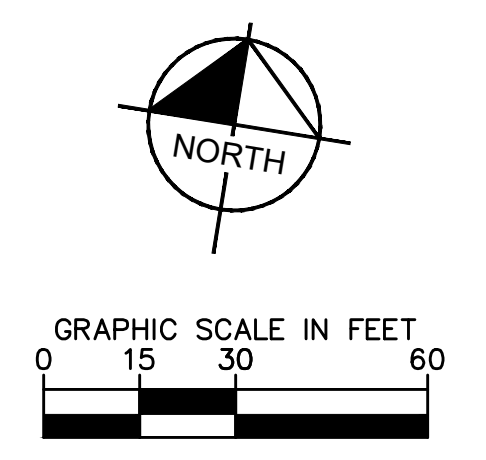
SHEET NUMBER
C4.4

Plotted By: Martell, Theodore - Sheet Set: CIRCLE K - US HWY 90 & I-75 High Speed Diesel Fuel Expansion - Layout: C4.5 PARKING TRUCK MOVEMENTS - October 03, 2022 03:47:31pm - K:\VRL\CIVIL\149880040-Circle K US90 & I75\ADD.CONST\PlanSheets\C4.0 - SITE PLAN.dwg
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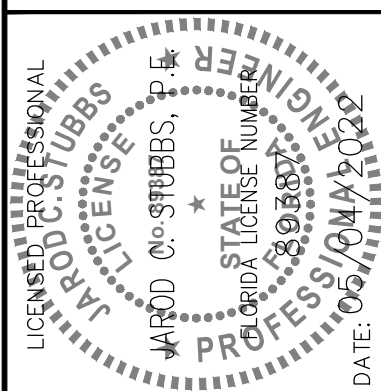
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Trailer Width	: 8.50	Steering Angle	: 28.4
Tractor Track	: 8.00	Articulating Angle	: 75.0
Trailer Track	: 8.50		

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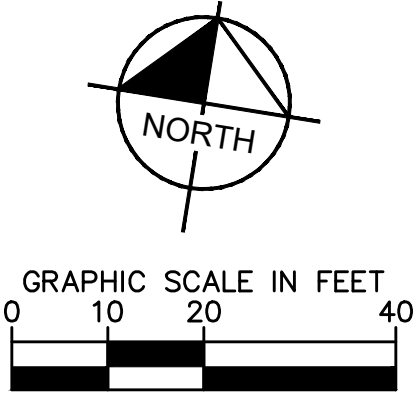
KHA PROJECT	149880040
DATE	05/04/2022
SCALE	AS SHOWN
DESIGNED BY	EJF
DRAWN BY	EJF
CHECKED BY	JCS

C4.5 - TRUCK TURNING MOVEMENTS

CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION

SHEET NUMBER
C4.5

Plotted By: Mariell, Theodore. Sheet: S-CIRCLE K - US HWY 90 & I-75 High Speed Diesel Fuel Expansion. Layout: C5.0 PAVING, GRADING AND DRAINAGE PLAN. October 03, 2022. 03:47:55pm. K:\DR\Civil\149880040-Circle K US90 & I75 CAD\CD.CONST\PlanSheets\C5.0 - PGD.dwg
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STORMWATER MANHOLE
 RIM ELEV = 145.69'
 CENTER ELEV = 139.19'
 S INV ELEV = INACCESSIBLE (42" RCP PER UTILITY PLAN)
 W INV ELEV = INACCESSIBLE (UNKNOWN)
 STRUCTURE FULL OF WATER

REPLACE EXISTING TYPE 6
 STORM STRUCTURE TOP
 WITH TYPE W GUTTER INLET
 PER FDOT INDEX #425-041

STORMWATER CURB INLET
 THROAT ELEV = 145.74'
 N INV ELEV = 139.94 (24" RCP)
 S INV ELEV = 139.89 (24" RCP)
 W INV ELEV = 139.76 (30" RCP)
 E INV ELEV = UNKNOWN (18" RCP PER UTILITY PLAN)

STORMWATER CURB INLET
 THROAT ELEV = 145.65'
 N INV ELEV = 139.70 (30" RCP)
 S INV ELEV = 139.20 (UNKNOWN)

CONNECT TO EXISTING 18" RCP
 STUB OUT AT ELEV. 140.01
 (CONTRACTOR TO VERIFY ELEVATION
 AND NOTIFY ENGINEER IF ANY
 DISCREPANCIES ARE FOUND)

SINGLE STORY
 COMMERCIAL STRUCTURE
 EXISTING BUILDING
 5265 SQ FL
 BUILDING EXPANSION
 1027 SQ FL
 F.F.E = 147.37

CONNECT TO EXISTING
 MANHOLE CORE BORE
 INV. = 140.57

ADJUST MANHOLE TOP TO
 MATCH PROPOSED GRADE

EXISTING STORM STRUCTURE
 W INV. ELEV. = 140.57
 E INV. ELEV. = 140.86

EXISTING STORM STRUCTURE
 W INV. ELEV. = 140.21
 NE INV. ELEV. = 140.24
 E INV. ELEV. = 140.21

- NOTES:**
- ALL STORM PIPES LABELED "HDPE" SHALL BE ADS HIGH PERFORMANCE (HP) POLYPROPYLENE STORM SEWER PIPE.
 - CURB RAMPS ALONG PUBLIC STREETS AND IN THE PUBLIC RIGHT-OF-WAY SHALL BE CONSTRUCTED IN ACCORDANCE WITH FDOT STANDARDS AND SPECIFICATIONS.
 - PRIVATE CURB RAMPS ON THE SITE (I.E. OUTSIDE PUBLIC STREET RIGHT-OF-WAY) SHALL CONFORM TO ADA STANDARDS AND SHALL HAVE A DETECTABLE WARNING SURFACE THAT IS FULL WIDTH AND FULL DEPTH OF THE CURB RAMP, NOT INCLUDING FLARES.
 - ALL ACCESSIBLE ROUTES, GENERAL SITE AND BUILDING ELEMENTS, RAMPS, CURB RAMPS, STRIPING, AND PAVEMENT MARKINGS SHALL CONFORM TO ADA STANDARDS FOR ACCESSIBLE DESIGN, LATEST EDITION.
 - BEFORE PLACING PAVEMENT, CONTRACTOR SHALL VERIFY THAT SUITABLE ACCESSIBLE PEDESTRIAN ROUTES (PER ADA AND FHA) EXIST TO AND FROM EVERY DOOR AND ALONG SIDEWALKS, ACCESSIBLE PARKING SPACES, ACCESSIBLE AISLES, AND ACCESSIBLE ROUTES. IN NO CASE SHALL AN ACCESSIBLE RAMP SLOPE EXCEED 1 VERTICAL TO 12 HORIZONTAL. IN NO CASE SHALL SIDEWALK CROSS SLOPE EXCEED 2.0 PERCENT. IN NO CASE SHALL LONGITUDINAL SIDEWALK SLOPE EXCEED 5.0 PERCENT. ACCESSIBLE PARKING SPACES AND ACCESS AISLES SHALL NOT EXCEED 2.0 PERCENT SLOPE IN ANY DIRECTION.
 - CONTRACTOR SHALL TAKE FIELD SLOPE MEASUREMENTS ON FINISHED SUBGRADE AND FORM BOARDS PRIOR TO PLACING PAVEMENT TO VERIFY THAT ADA SLOPE REQUIREMENTS ARE PROVIDED. CONTRACTOR SHALL CONTACT ENGINEER PRIOR TO PAVING IF ANY EXCESSIVE SLOPES ARE ENCOUNTERED. NO CONTRACTOR CHANGE ORDERS WILL BE ACCEPTED FOR ADA SLOPE COMPLIANCE ISSUES.
 - ALL PEDESTRIAN SIDEWALKS, PATHWAYS, AND CROSSWALKS SHALL BE CONSTRUCTED NOT TO EXCEED MAX. 2.0% CROSS SLOPE, MAX. 5.0% RUNNING SLOPE.
 - ALL HANDICAP ACCESSIBLE PARKING SPACES SHALL BE CONSTRUCTED NOT TO EXCEED MAX. 2.0% CROSS SLOPE IN ALL DIRECTIONS.
 - PROPOSED GRADES TO MATCH EXISTING ELEVATIONS AT PROPERTY LINE
 - CONTRACTOR TO FIELD VERIFY ELEVATIONS AT ALL EXISTING SIDEWALK AND ROAD CONNECTION POINTS WITH ENGINEER OF RECORD BEFORE CONSTRUCTION OF ANY IMPROVEMENTS.
 - FOR EROSION CONTROL NOTES REFER TO SHEET C2.0.
 - ALL DRAINAGE PIPES SHALL BE FILTER FABRIC WRAPPED PER FDOT STANDARD PLAN #430-001.
 - CONTRACTOR TO ENSURE ADEQUATE COVER REMAINS OVER ALL EXISTING UTILITIES.
 - CONTRACTOR TO VERIFY EXISTING COVER OVER ALL UTILITIES BEFORE START OF CONSTRUCTION AND TO COORDINATE WITH THE ENGINEER OF RECORD PRIOR TO START OF CONSTRUCTION IF DESIGN DOES NOT PROVIDE 36" COVER.
 - ALL EXISTING VALVES, BOXES, MANHOLE LIDS, COVERS, AND SIMILAR APPURTENANCES MUST BE ADJUSTED ACCORDINGLY TO MATCH FINISHED GRADE.
 - ALL PAVEMENT MUST BE SOURCED FROM AN FDOT APPROVED PLANT.

STORM STRUCTURE TABLE	
STRUCTURE NAME:	DETAILS:
D-1	TYPE "D" INLET PER FDOT INDEX #425-052 TOP: 146.45 N. INV. OUT: 141.77 (18" HDPE)
D-2	TYPE "D" INLET PER FDOT INDEX #425-052 TOP: 145.97 S. INV. IN: 141.29 (18" HDPE) W. INV. OUT: 141.29 (18" HDPE)
D-3	TYPE "D" INLET PER FDOT INDEX #425-052 TOP: 146.82 E. INV. IN: 140.24 (18" HDPE) SW. INV. OUT: 140.24 (18" HDPE)
D-4	TYPE "3" INLET PER FDOT INDEX #425-020 TOP: 144.65 S. INV. OUT: 140.71 (18" RCP)

LEGEND

---	PROPERTY LINE
---	PROPOSED STORM PIPE
⊙	PROPOSED STORM MANHOLE
⊠	PROPOSED STORM INLET
XX.XX	PROPOSED ELEV. TOP OF CURB
XX.XX	PROPOSED ELEV. BOT. OF CURB
XX.XX	PROPOSED SPOT ELEVATION
---	EXISTING STORM PIPE
⊙	EXISTING STORM MANHOLE
⊠	EXISTING STORM INLET

VERTICAL DATUM:
 ELEVATIONS ARE REFERENCED TO
 PLAT BENCHMARKS AS SHOWN ON
 THE FACE OF THE PLAT OF
 GATEWAY CROSSING REPLAT OF
 LOTS 2 AND 3, PLAT
 BOOK 9, PAGE 151

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KHA PROJECT
 149880040

DATE
 05/04/2022

SCALE AS SHOWN

DESIGNED BY E/JF
 DRAWN BY E/JF
 CHECKED BY JCS

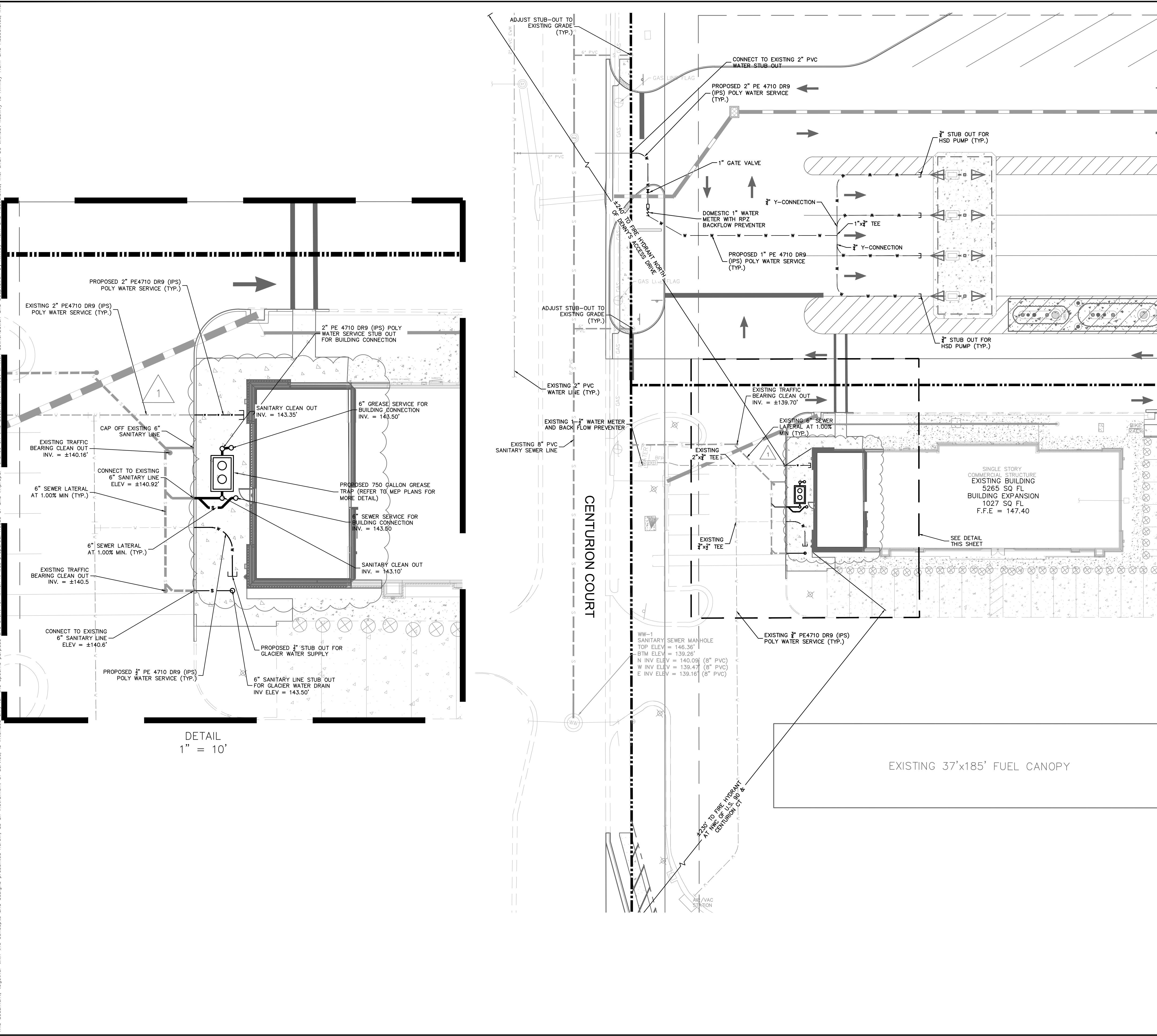
**PAVING, GRADING
 AND DRAINAGE PLAN
 EXPANSION**

CIRCLE K - US HWY 90
 & I-75 FUEL
 EXPANSION

FLORIDA
 CITY OF LAKE CITY

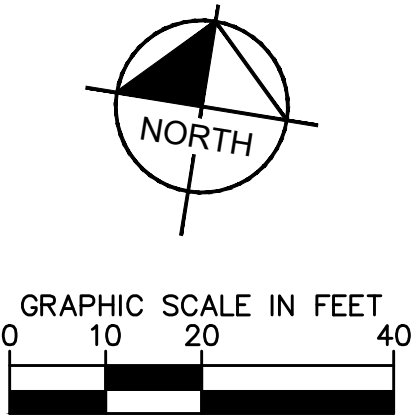
SHEET NUMBER
C5.0

Plotted By: Mariell, Theodore - Sheet: SET-CIRCLE K - US HWY 90 & I-75 High Speed Diesel Fuel Expansion - Layout: C6.0 UTILITY PLAN - October 03, 2022 - 03:48:23pm - K:\A\01\149880040 - Circle K - USRD - 175 VACAD.CONST.PlanSheetA06.0 - UTILITIES PLAN.dwg
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NOTES:

- ALL ON-SITE UTILITIES SHALL BE PRIVATELY OWNED AND MAINTAINED.
- MAINTAIN A MINIMUM OF 3'-FT OF COVER OVER ALL PROPOSED WATER LINES AND 4'-FT MINIMUM OF COVER FOR WASTEWATER LINES.
- REFER TO ADDITIONAL UTILITY NOTES AND DETAILS ON SHEETS C1.0 AND C7.0.
- ALL WATER MAINS, AND DOMESTIC LINES SHALL BE CONSTRUCTED IN ACCORDANCE WITH CITY OF LAKE CITY UTILITY STANDARDS.
- BEDDING AND BACKFILL SHALL BE INSTALLED PER CITY OF LAKE CITY. GRANULAR BACKFILL IS REQUIRED UNDER PAVEMENT AND WITHIN 5 FEET OF PAVEMENT.
- ALL SANITARY SEWER LINES SHALL BE GREEN C-900 PVC MEETING, ASTM D-3034 SDR 26.
- CONTRACTOR SHALL COORDINATE ANY DISRUPTIONS TO EXISTING UTILITY SERVICES WITH ADJACENT PROPERTY OWNERS.
- ALL ELECTRIC AND TELEPHONE EXTENSIONS INCLUDING SERVICE LINES SHALL BE CONSTRUCTED TO THE APPROPRIATE UTILITY COMPANY SPECIFICATIONS. ALL UTILITY DISCONNECTIONS SHALL BE COORDINATED WITH THE DESIGNATED UTILITY COMPANIES.
- CONTRACTOR TO CALL "SUNSHINE STATE ONE CALL OF FLORIDA" (1-800-432-4770) TO COORDINATE FIELD LOCATIONS OF EXISTING UNDERGROUND UTILITIES BEFORE ORDERING MATERIALS OR COMMENCING CONSTRUCTION. NOTIFY ENGINEER OF ANY DISCREPANCIES IMMEDIATELY.
- PRIOR TO THE CONSTRUCTION OF OR CONNECTION TO ANY STORM DRAIN, SANITARY SEWER, WATER MAIN OR ANY OTHER UTILITIES, THE CONTRACTOR SHALL EXCAVATE, VERIFY AND CALCULATE ALL POINTS OF CONNECTION AND ALL UTILITY CROSSINGS AND INFORM THE ENGINEER AND THE OWNER/ DEVELOPER OF ANY CONFLICT OR REQUIRED DEVIATIONS FROM THE PLAN. NOTIFICATION SHALL BE MADE A MINIMUM OF 72 HOURS PRIOR TO CONSTRUCTION. THE ENGINEER AND ITS CLIENTS SHALL BE HELD HARMLESS IN THE EVENT THAT THE CONTRACTOR FAILS TO MAKE SUCH NOTIFICATION. CITY OF LAKE CITY SHALL BE NOTIFIED OF ANY AND ALL CHANGES TO THE DESIGN PLANS.
- CONTRACTOR SHALL COMPLY COMPLETELY WITH THE LATEST STANDARDS OF OSHA DIRECTIVES OR ANY OTHER AGENCY HAVING JURISDICTION FOR EXCAVATION AND TRENCHING PROCEDURES. THE CONTRACTOR SHALL USE SUPPORT SYSTEMS, SLOPING, BENCHING AND OTHER MEANS OF PROTECTION. THIS IS TO INCLUDE, BUT NOT LIMITED TO ACCESS AND EGRESS FROM ALL EXCAVATION AND TRENCHING. CONTRACTOR IS RESPONSIBLE FOR COMPLYING WITH PERFORMANCE CRITERIA AS REQUIRED BY OSHA.
- CONTRACTOR TO AVOID DISRUPTION OF ANY ADJACENT TENANT'S TRAFFIC OPERATIONS, TO THE MAXIMUM EXTENT, DURING INSTALLATION OF UTILITIES. IF TRAFFIC IS TO BE OBSTRUCTED CONTRACTOR SHALL COORDINATE TRAFFIC CONTROL PLAN WITH THE LOCAL MUNICIPALITY.
- ALL DIMENSIONS ARE TO CENTERLINE OF PIPE OR CENTER OF MANHOLE UNLESS NOTED OTHERWISE.
- SEE PLUMBING PLANS FOR EXACT UTILITY CONNECTION LOCATIONS AT BUILDING.
- LIGHT POLES SHOWN FOR COORDINATION PURPOSES ONLY AND DO NOT REPRESENT ACTUAL SIZE. SEE SITE LIGHTING PLANS BY OTHERS FOR MORE INFORMATION.
- ELECTRIC, TELEPHONE, AND OTHER DRY UTILITIES SHALL BE PLACED WITHIN CONDUIT, MEETING PRIVATE UTILITY STANDARDS, WHEN ROUTE CROSSES PAVED DRIVES AND PARKING AREAS.
- ALL UNDERGROUND WATER MAINS AND HYDRANTS SHALL BE INSTALLED, COMPLETED AND IN SERVICE PRIOR TO ANY COMBUSTIBLES BEING BROUGHT ON-SITE.
- CONTRACTOR TO ENSURE ADEQUATE COVER REMAINS OVER ALL EXISTING UTILITIES.
- CONTRACTOR TO VERIFY EXISTING COVER OVER ALL UTILITIES BEFORE START OF CONSTRUCTION AND TO COORDINATE WITH THE ENGINEER OF RECORD PRIOR TO START OF CONSTRUCTION IF DESIGN DOES NOT PROVIDE 36" COVER.
- ALL ELECTRIC, CABLE, AND TELECOMMUNICATION UTILITIES FOR BUILDING SERVICE TO BE INSTALLED UNDERGROUND.



DETAIL
1" = 10'

LEGEND

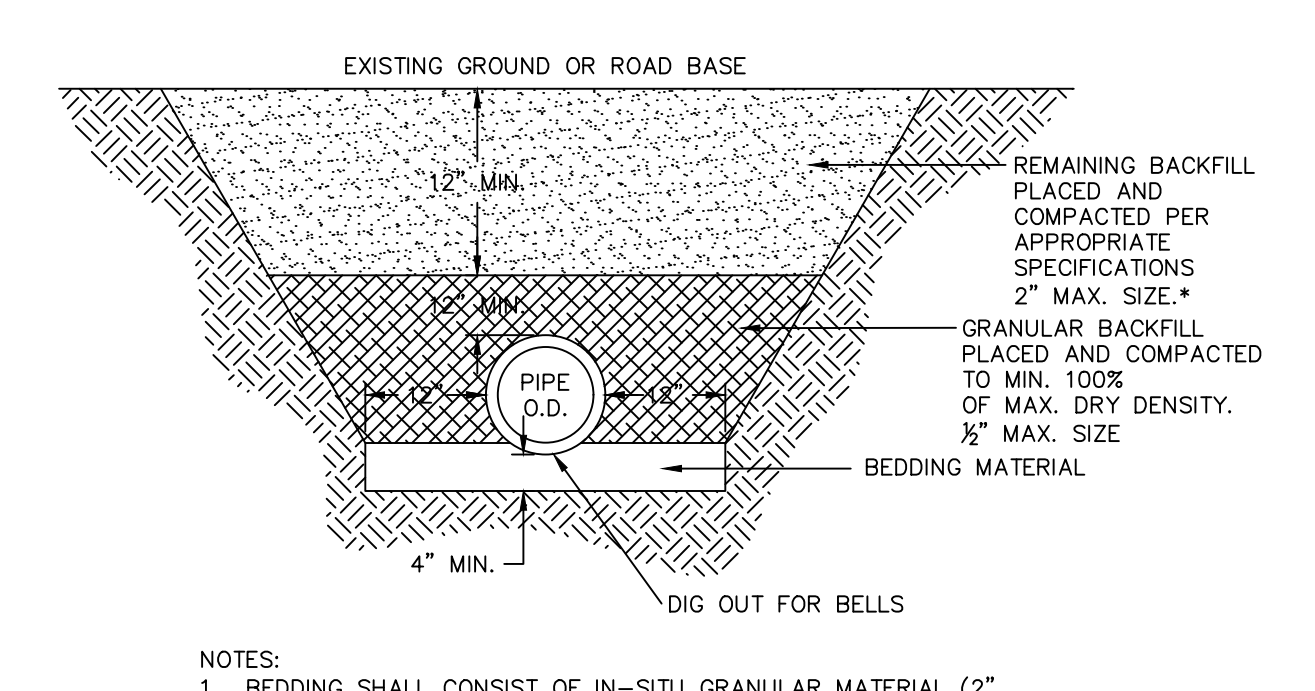
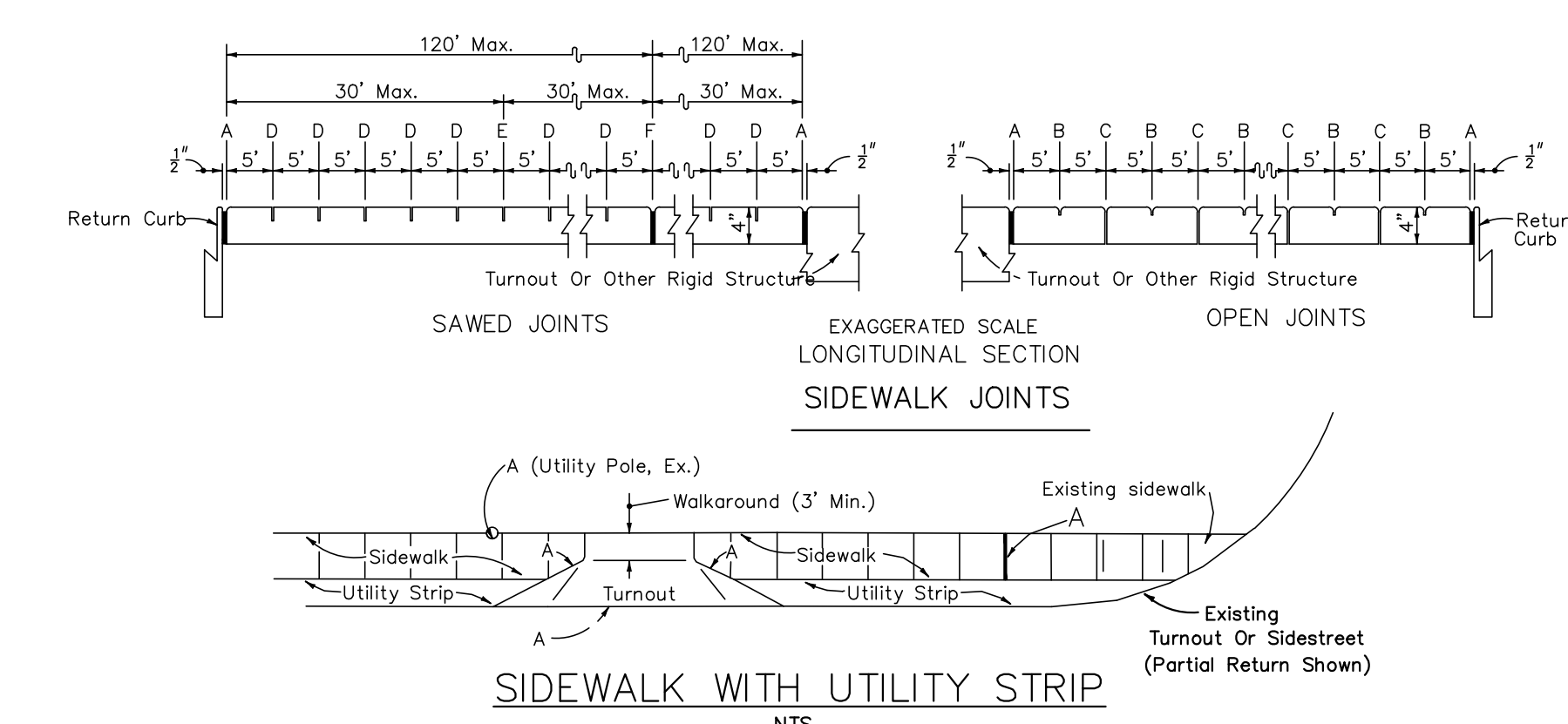
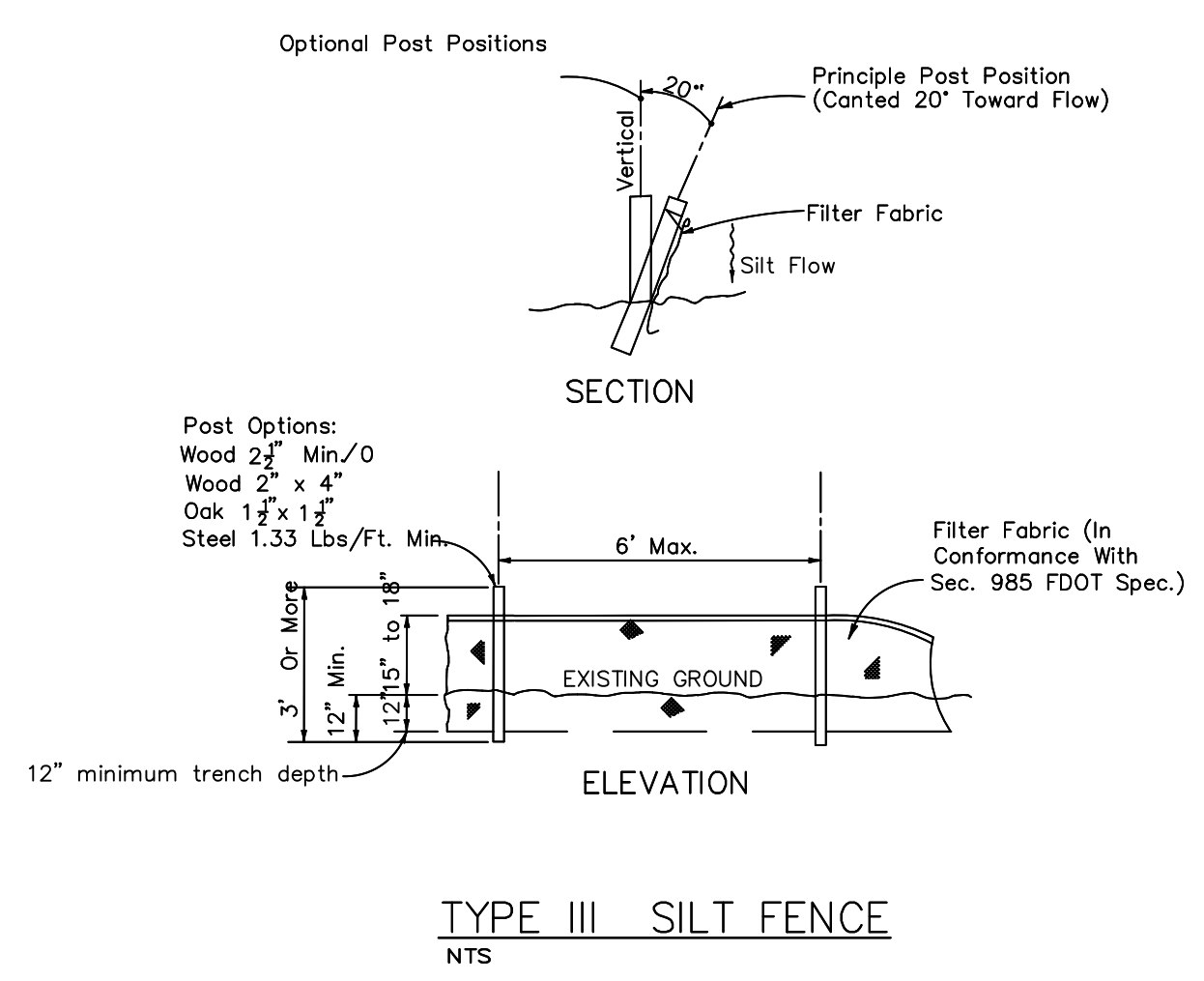
- PROPERTY BOUNDARY
- PROPOSED STORM PIPE
- PROPOSED SANITARY SEWER LINE
- EXISTING WATER LINE
- EXISTING SANITARY SEWER LINE
- EXISTING WATER LINE
- EXISTING STORM PIPE
- EXISTING GAS LINE
- EXISTING OVERHEAD ELECTRIC LINE
- EXISTING FORCE MAIN
- EXISTING STORM MANHOLE
- EXISTING SANITARY SEWER MANHOLE
- ⊗ PROPOSED FIRE HYDRANT

VERTICAL DATUM:
ELEVATIONS ARE REFERENCED TO PLAT BENCHMARKS AS SHOWN ON THE FACE OF THE PLAT OF GATEWAY CROSSING REPLAT OF LOTS 2 AND 3, PLAT BOOK 9, PAGE 151

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	<p>REVISIONS DATE BY</p>
<p>KHA PROJECT 149880040 DATE 05/04/2022 SCALE AS SHOWN DESIGNED BY E/JF DRAWN BY E/JF CHECKED BY JCS</p>	<p>DATE: 05/04/2022</p>
<p>UTILITY PLAN</p>	<p>FLORIDA</p>
<p>CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION</p>	<p>CITY OF LAKE CITY</p>
<p>SHEET NUMBER C6.0</p>	<p>31</p>

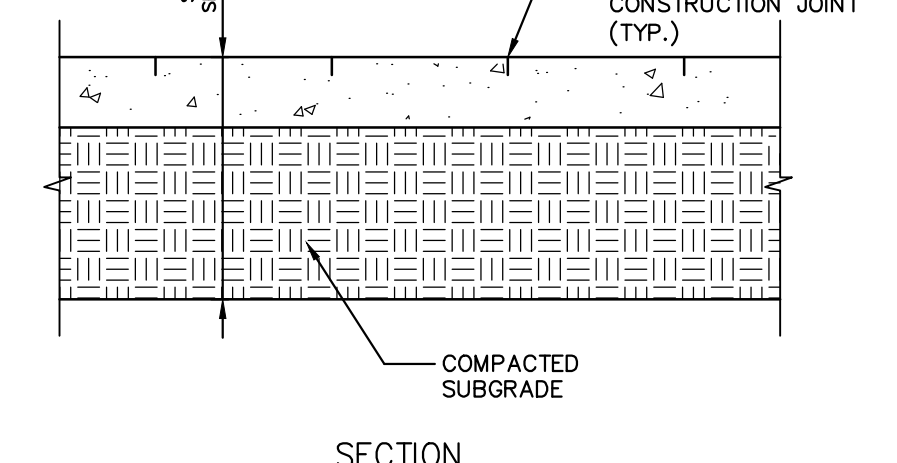
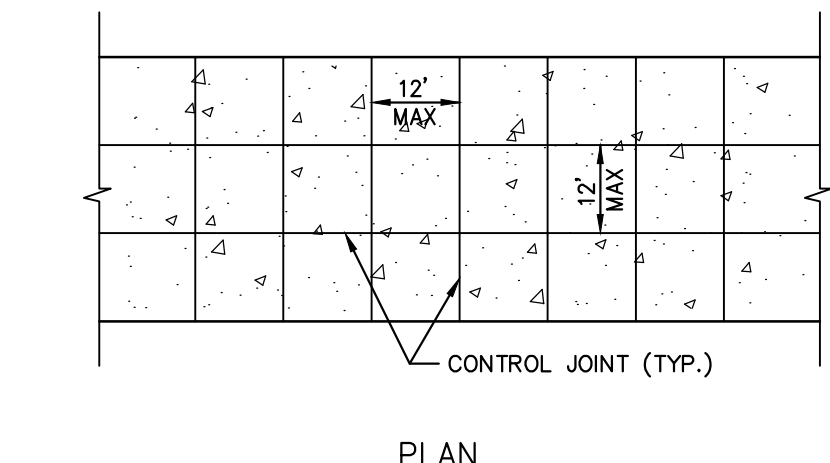
Plotted By: Martell, Theodore. Sheet: S-CIRCLE K - US HWY 90 & I-75 High Speed Diesel Fuel Expansion. Layout: C7.D. GENERAL CONSTRUCTION DETAILS - October 03, 2022. 03:48:59pm. K:\ORL\Civil\49880040-Circle K US90 & I-75\CADD\CONSTR\PlanSheets\C7.D - DETAILS.dwg
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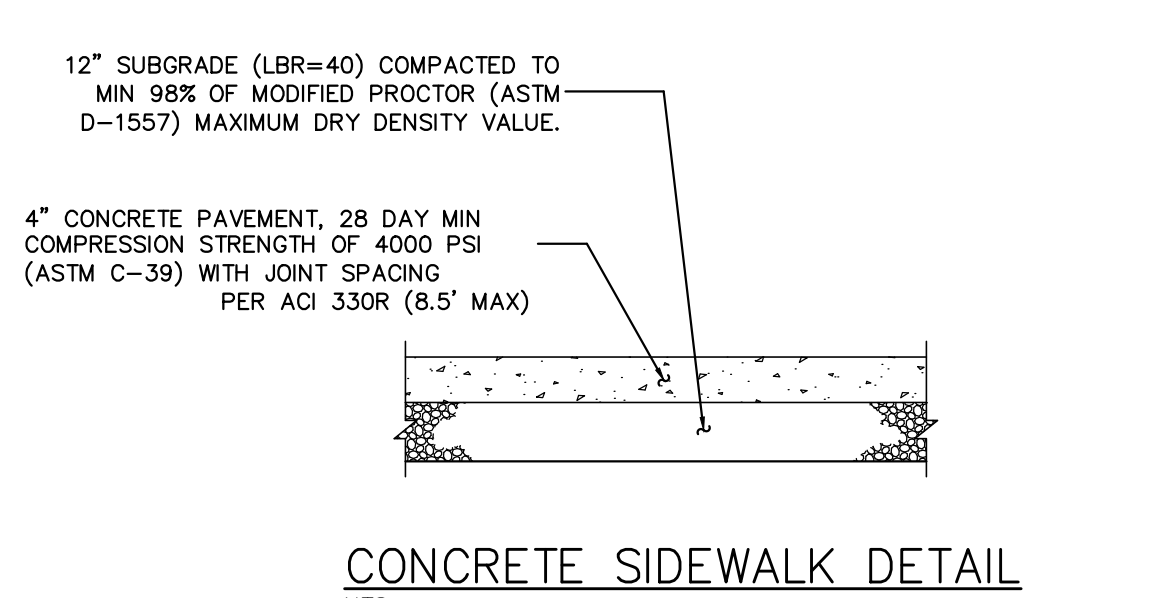
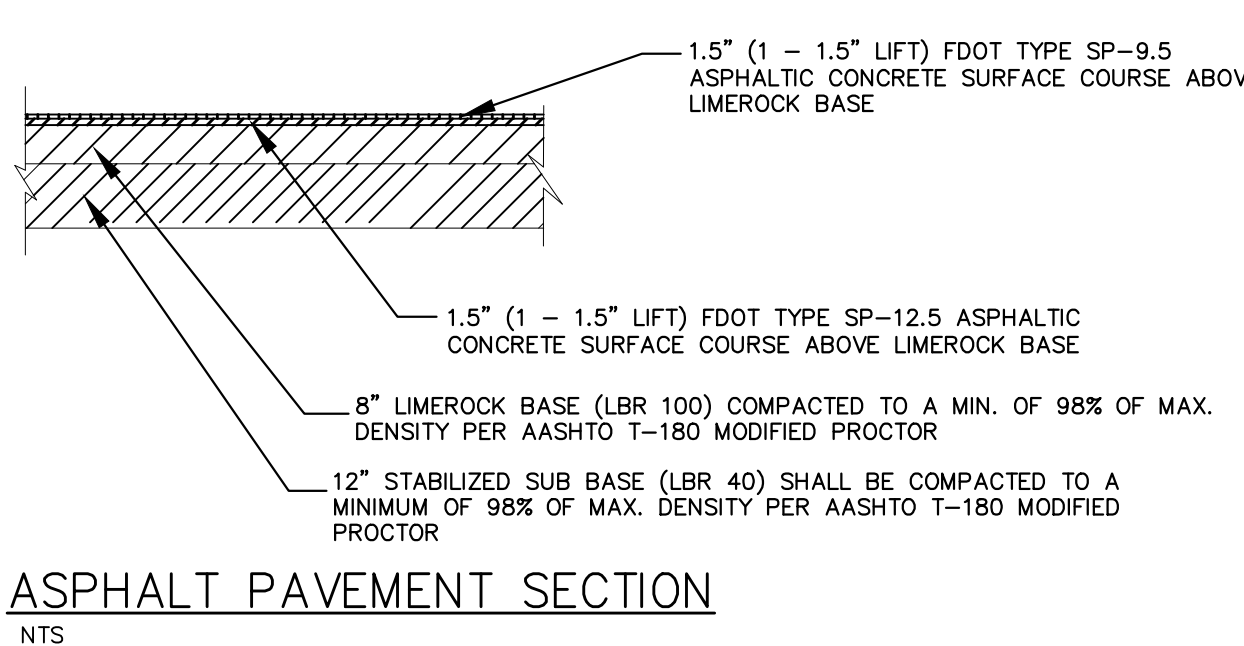
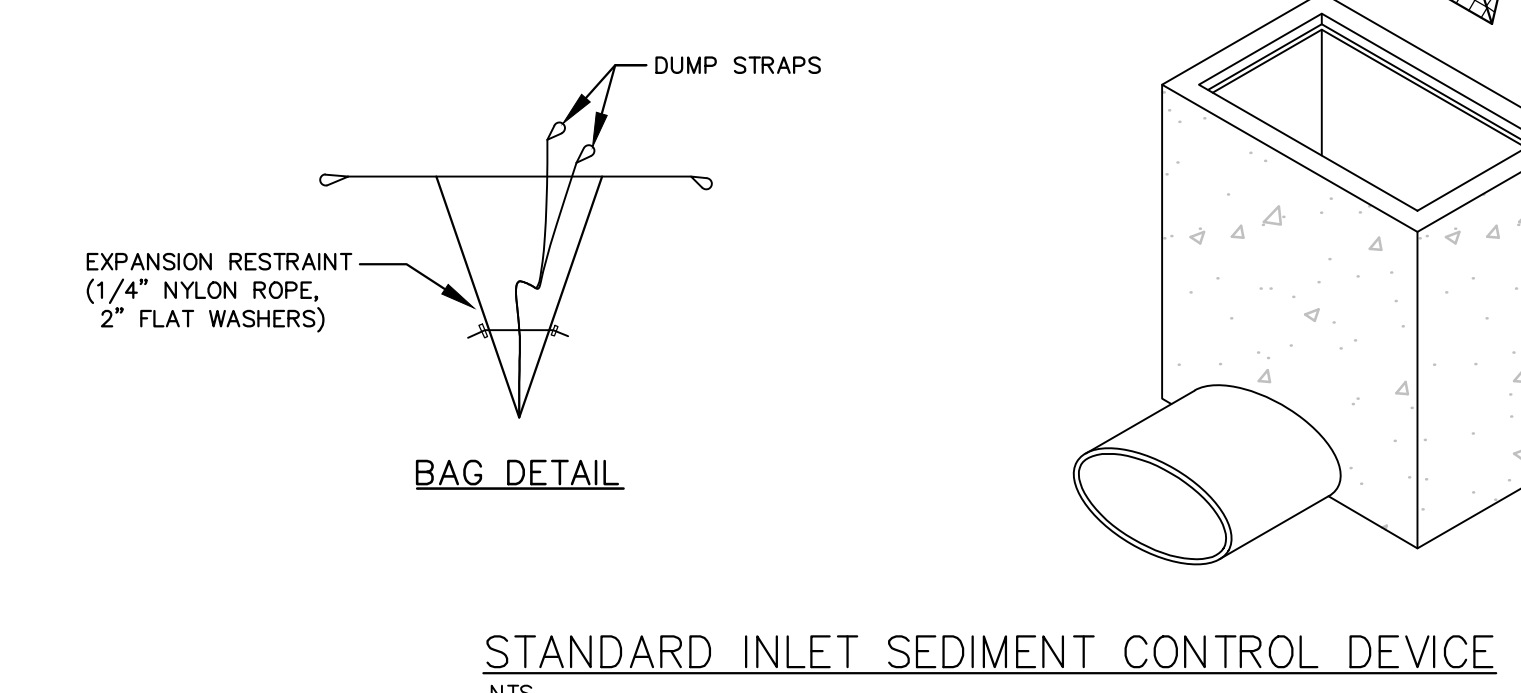
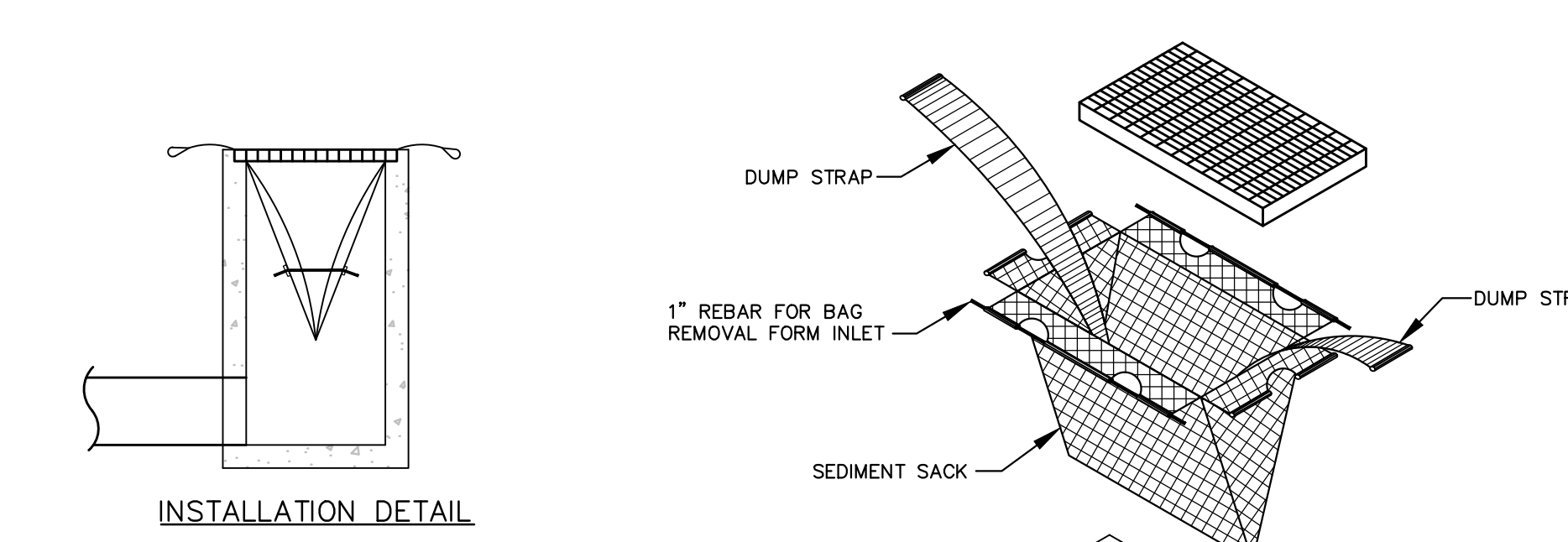
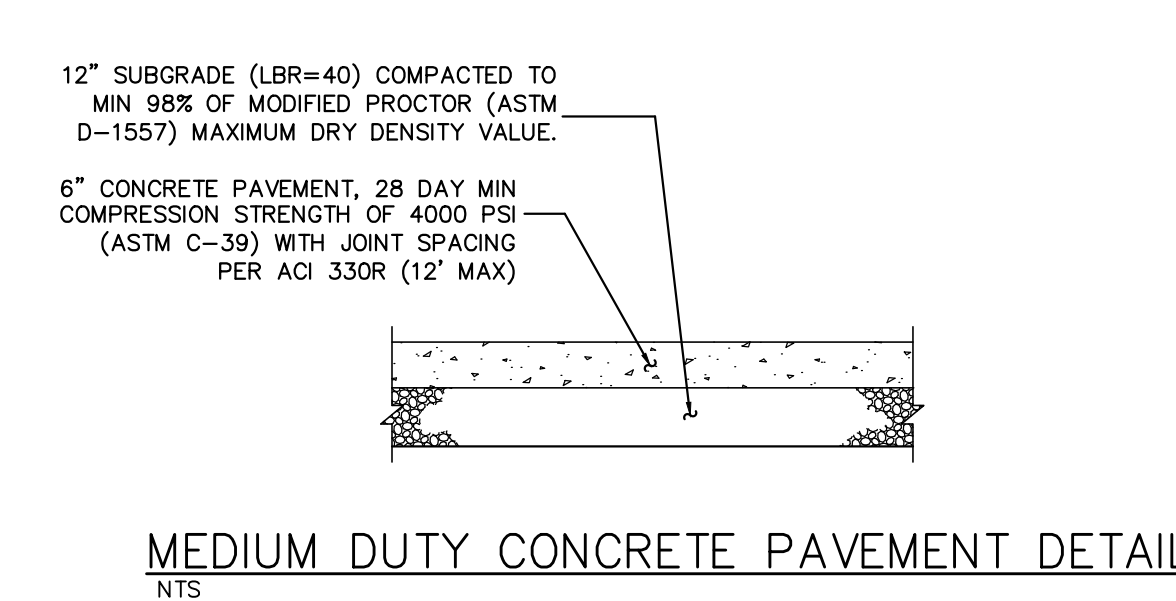
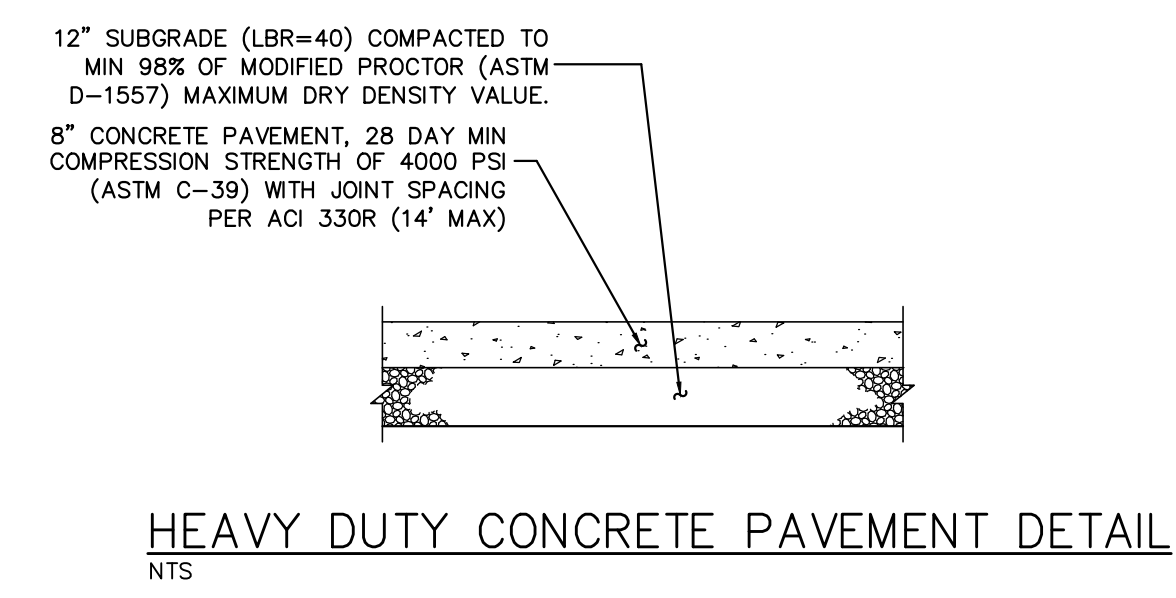
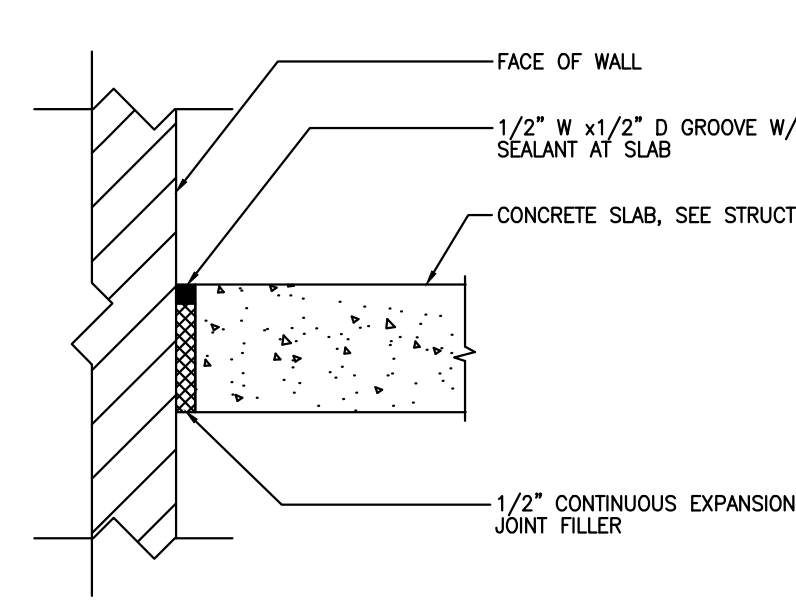
GENERAL NOTES

SIDEWALKS SHALL BE CONSTRUCTED IN ACCORDANCE WITH FDOT STANDARD SPECIFICATION NO. 522 EXCEPT FOR CURB CUT RAMP RUNS WHICH SHALL BE FINISHED IN ACCORDANCE WITH FDOT INDEX 522-002.
 BOND BREAKER MATERIAL CAN BE ANY IMPERMEABLE COATED OR SHEET MEMBRANE OR PREFORMED MATERIAL HAVING A THICKNESS OF NOT LESS THAN 6 MILS NOR MORE THAN 1/2".

CONCRETE SIDEWALK DETAIL
NTS

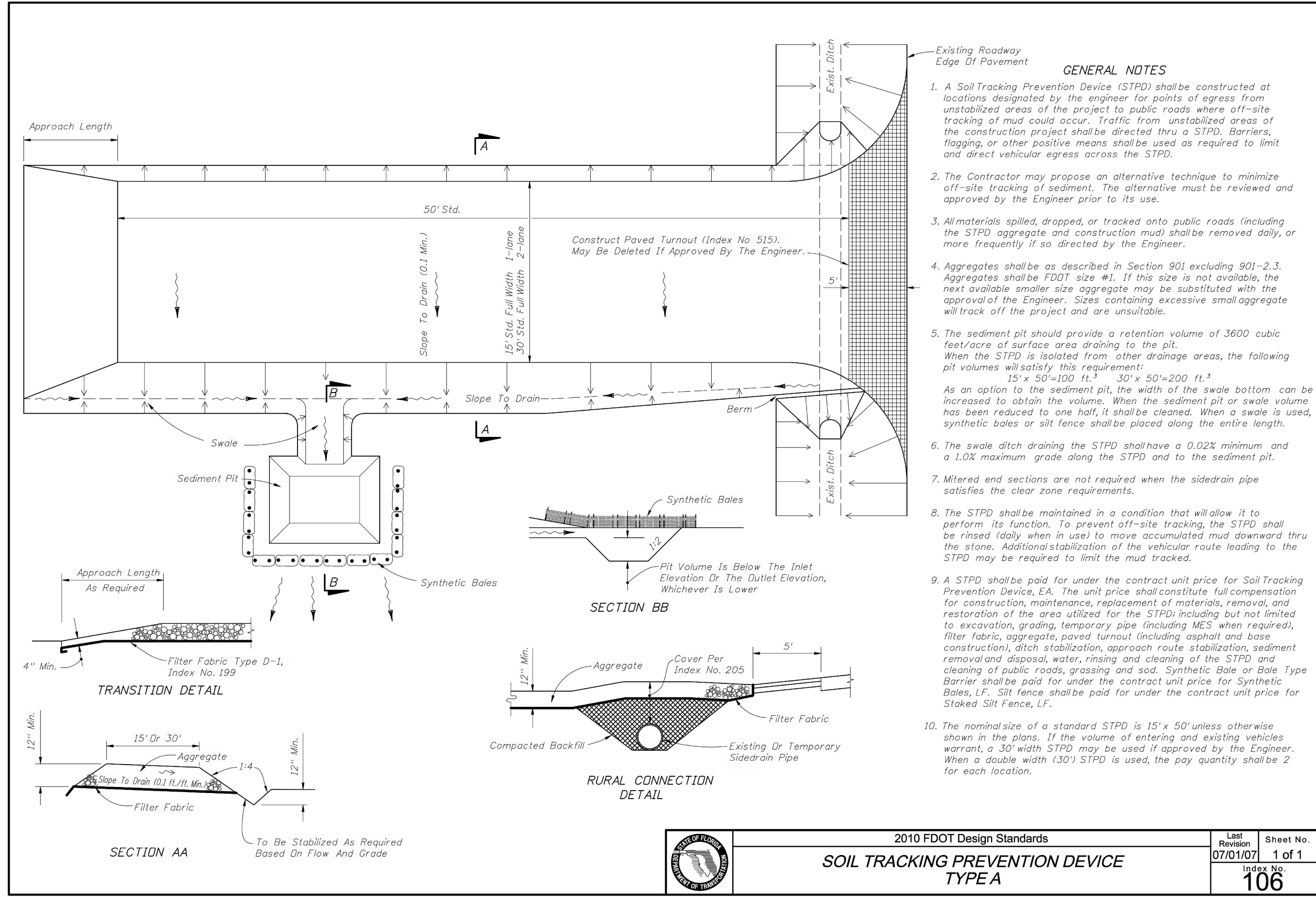


- NOTES:**
- LONGITUDINAL AND TRANSVERSE JOINTS SHALL BE SAWED AS INDICATED.
 - THE JOINT SEAL MANUFACTURER'S SPECIFICATIONS SHALL BE COMPATIBLE WITH THE JOINT CONFIGURATION. PROVIDE TYPICAL DETAILS FOR CONSTRUCTION AND EXPANSION JOINTS.
 - FOR ALL JOINTS THE BACKER ROD MATERIAL SHALL BE COMPATIBLE WITH THE COLD POURED SEALANT AND SLIGHTLY OVERSIZED TO PREVENT MOVEMENT DURING THE JOINT SEALANT OPERATION.
 - THE WIDTH OF ALL JOINTS SHALL BE CORRECTED FOR 68 DEGREE (F).
 - EXPANSION JOINTS SHALL BE USED AT ALL LOCATIONS WHERE PAVEMENT ABUTS A PERMANENT STRUCTURE.
 - DOWELS AND THE BARS FOR CONSTRUCTION JOINTS SHALL BE CAST IN PLACE.
- CONCRETE JOINTING AND REINFORCING DETAIL**
NOT TO SCALE

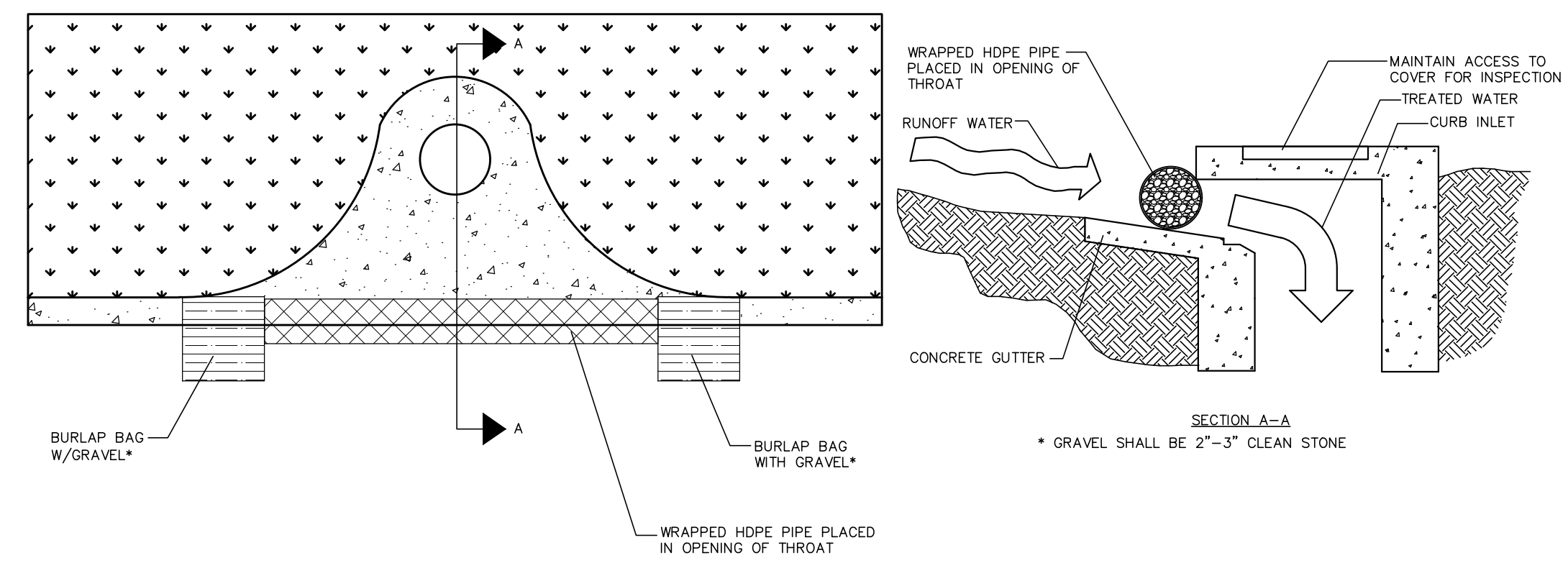


KIMLEY-HORN & ASSOCIATES, INC.		DATE	
189 S. ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801		NO.	
WWW.KIMLEY-HORN.COM		REVISIONS	
REGISTRY NO. 35106		BY	
© 2022 KIMLEY-HORN AND ASSOCIATES, INC.		DATE	
PHONE: 407-898-1511		NO.	
KIMLEY-HORN		REVISIONS	
LICENSED PROFESSIONAL ENGINEER		DATE	
STATE OF FLORIDA		NO.	
KHA PROJECT 149880040		DATE 05/04/2022	
SCALE AS SHOWN		DESIGNED BY E/JF	
DRAWN BY E/JF		CHECKED BY JCS	
GENERAL CONSTRUCTION DETAILS		FLORIDA	
CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION		CITY OF LAKE CITY	
SHEET NUMBER C7.0		32	

Plotted By: Martell, Theodore - Sheet: CIRCLE K - US HWY 90 & I-75 High Speed Diesel Fuel Expansion - Layout: C7.1 GENERAL CONSTRUCTION DETAILS - October 03, 2022 - 03:49:03pm - K:\ORL\CWA\149880040-Circle K US90 & I75\CADD\CONSTR\PlanSheets\C7.0 - DETAILS.dwg
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- GENERAL NOTES**
1. A Soil Tracking Prevention Device (STPD) shall be constructed at locations designated by the engineer for points of egress from unstabilized areas of the project to public roads where off-site tracking of mud could occur. Traffic from unstabilized areas of the construction project shall be directed thru a STPD. Barriers, flagging, or other positive means shall be used as required to limit and direct vehicular egress across the STPD.
 2. The Contractor may propose an alternative technique to minimize off-site tracking of sediment. The alternative must be reviewed and approved by the Engineer prior to its use.
 3. All materials spilled, dropped, or tracked onto public roads (including the STPD aggregate and construction mud) shall be removed daily, or more frequently if so directed by the Engineer.
 4. Aggregates shall be as described in Section 901 excluding 901-2.3. Aggregates shall be FDOT size #1. If this size is not available, the next available smaller size aggregate may be substituted with the approval of the Engineer. Sizes containing excessive small aggregate will track off the project and are unsuitable.
 5. The sediment pit should provide a retention volume of 3600 cubic feet/cure of surface area draining to the pit. When the STPD is isolated from other drainage areas, the following pit volumes will satisfy this requirement:
 15' x 50' = 100 ft³ 30' x 50' = 200 ft³
 As an option to the sediment pit, the width of the swale bottom can be increased to obtain the volume. When the sediment pit or swale volume has been reduced to one half, it shall be cleaned. When a swale is used, synthetic bales or silt fence shall be placed along the entire length.
 6. The swale ditch draining the STPD shall have a 0.02% minimum and a 1.0% maximum grade along the STPD and to the sediment pit.
 7. Mitered end sections are not required when the side drain pipe satisfies the clear zone requirements.
 8. The STPD shall be maintained in a condition that will allow it to perform its function. To prevent off-site tracking, the STPD shall be rinsed (daily when in use) to move accumulated mud downward thru the stone. Additional stabilization of the vehicular route leading to the STPD may be required to limit the mud tracked.
 9. A STPD shall be paid for under the contract unit price for Soil Tracking Prevention Device, EA. The unit price shall constitute full compensation for construction, maintenance, replacement of materials, removal, and restoration of the area utilized for the STPD, including but not limited to excavation, grading, temporary pipe (including MES when required), filter fabric, aggregate, paved turnout (including asphalt and base construction), ditch stabilization, approach route stabilization, sediment removal and disposal, water, rinsing and cleaning of the STPD, and cleaning of public roads, grassing and sod. Synthetic Bale or Bale Type Barrier shall be paid for under the contract unit price for Synthetic Bales, LF. Silt fence shall be paid for under the contract unit price for Staked Silt Fence, LF.
 10. The nominal size of a standard STPD is 15' x 50' unless otherwise shown in the plans. If the volume of entering and existing vehicles warrant, a 30' width STPD may be used if approved by the Engineer. When a double width (30') STPD is used, the pay quantity shall be 2 for each location.



SOCK DRAIN INLET SEDIMENT FILTER

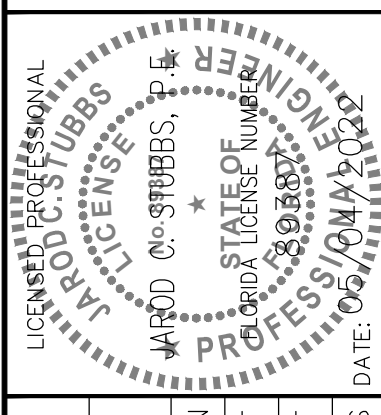
NOTE: THE PERFORATED PIPE MUST EXTEND AT LEAST 1' BEYOND THE CURB OPENING ON EACH SIDE AND BE ANCHORED WITH GRAVEL BAGS, OR SIMILAR, ON EACH END. A SPACER MUST BE PROVIDED FOR BETWEEN THE INLET OPENING AND THE PIPE TO ALLOW FOR OVERFLOW, PREVENT FLOODING AND TO PREVENT THE PIPE FROM FALLING INTO THE INLET.

2010 FDOT Design Standards		Sheet No.
SOIL TRACKING PREVENTION DEVICE TYPE A		1 of 1
Index No.		106

NO.	REVISIONS	DATE	BY

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KHA PROJECT	149880040
DATE	05/04/2022
SCALE	AS SHOWN
DESIGNED BY	EJF
DRAWN BY	EJF
CHECKED BY	JCS

GENERAL CONSTRUCTION DETAILS

CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION

CITY OF LAKE CITY FLORIDA

SHEET NUMBER
C7.1

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SHEET NUMBER
C8.0

**CIRCLE K - US HWY
90 & I-75 FUEL
EXPANSION**
CITY OF LAKE CITY FLORIDA

**CITY OF LAKE CITY
STANDARD DETAILS**

KHA PROJECT
149880040
DATE
05/04/2022
SCALE AS SHOWN
DESIGNED BY EJF
DRAWN BY EJP
CHECKED BY JCS DATE:

LICENSED PROFESSIONAL

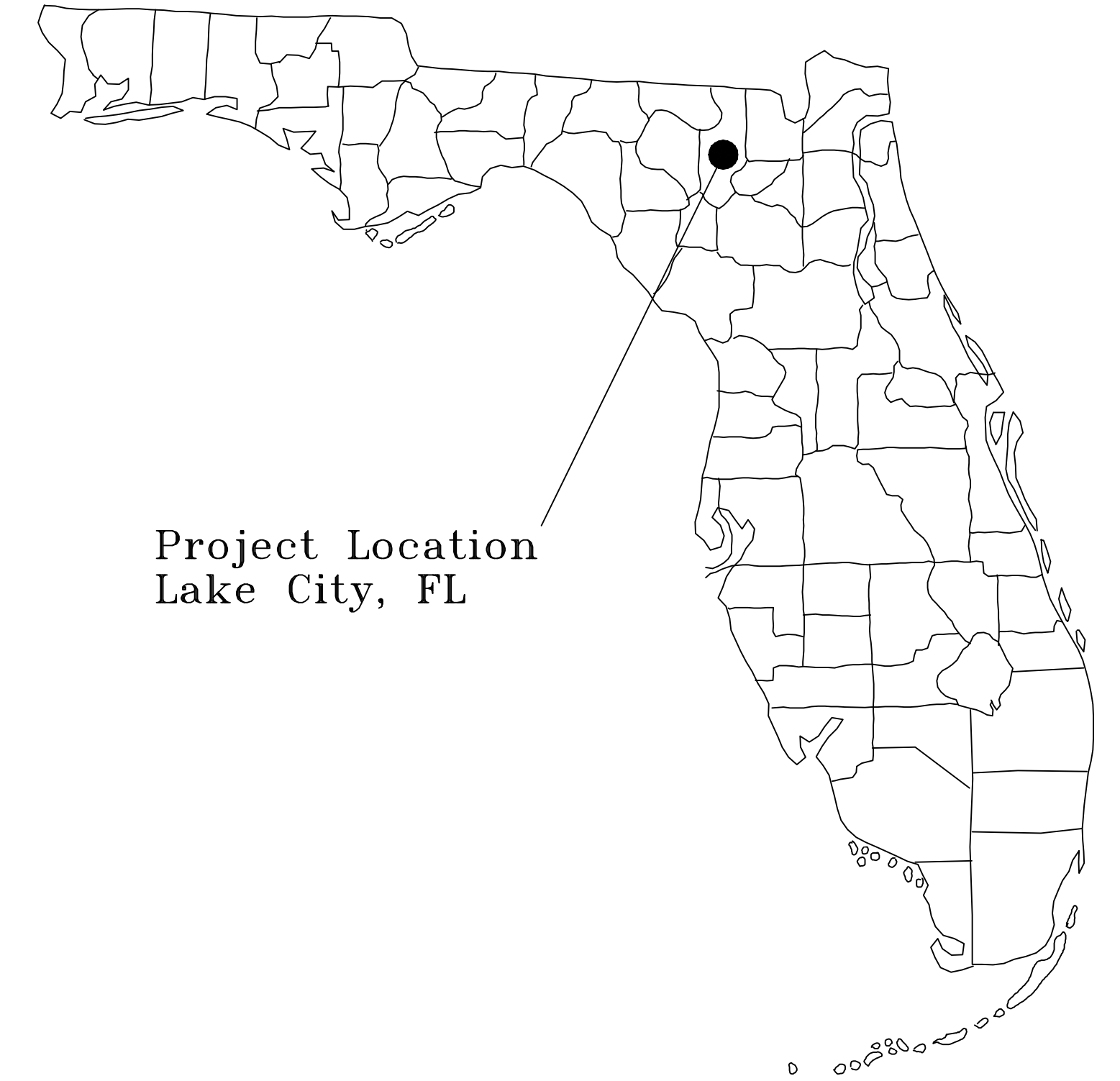


No. REVISIONS DATE BY

CONSTRUCTION PLANS FOR CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION

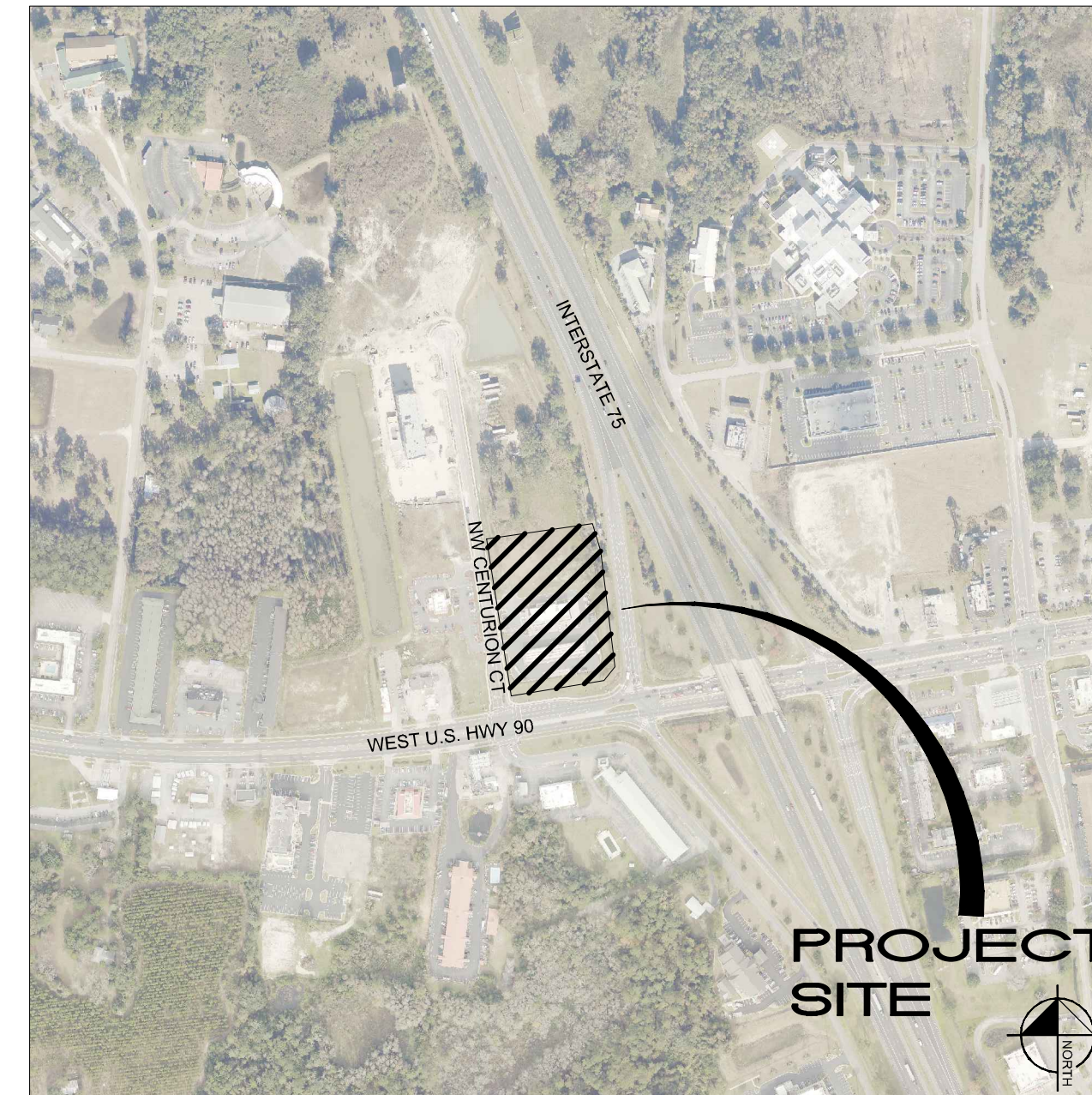
143 NW CENTURION COURT
LAKE CITY, FLORIDA 32055
MAY 4, 2022

PARCEL IDs: 35-3S-16-02524-001,
35-3S-16-02524-102 AND 35-3S-16-02524-111



LEGAL DESCRIPTION

LOT 2 AND THE NORTH 34.55 FEET OF LOT 11 OF GATEWAY CROSSING A REPLAT OF LOTS 2 & 3, ACCORDING TO PLAT THEREOF RECORDED IN PLAT BOOK 9, PAGE 151, PUBLIC RECORDS OF COLUMBIA COUNTY, FLORIDA.



VICINITY MAP

PROJECT TEAM

SHEET INDEX

C0.0	COVER SHEET
C1.0-C1.1	GENERAL NOTES
C2.0	STORMWATER POLLUTION PREVENTION PLAN
C3.0-C3.1	EXISTING CONDITIONS & DEMOLITION PLAN
C4.0	OVERALL SITE PLAN
C4.1	SITE PLAN
C4.2	INTERSECTION MODIFICATION PLAN
C4.3-C4.5	TRUCK TURNING MOVEMENTS
C5.0	PAVING, GRADING AND DRAINAGE PLAN
C6.0	UTILITY PLAN
C7.0-C7.1	GENERAL CONSTRUCTION DETAILS
L1.00	LANDSCAPE PLAN
L1.50	LANDSCAPE DETAILS
L1.51	LANDSCAPE SPECIFICATIONS
L2.00	SCHEMATIC IRRIGATION PLAN
L2.50	IRRIGATION DETAILS
L2.51	IRRIGATION NOTES

UTILITY PROVIDERS

WATER/SEWER:

CITY OF LAKE CITY UTILITIES
892 SW SAINT MARGARETS ST
LAKE CITY, FL 32025
CONTACT:
PHONE:

ELECTRIC :

FLORIDA POWER & LIGHT
2618 NE BASCOM NORRIS DRIVE
LAKE CITY, FL 32055
CONTACT: SHANE EUBANK
PHONE: (386) 754-2020

FIBER OPTIC :

HARGRAY OF FLORIDA, INC.
8324 BAYMEADOWS WAY, STE. 102
JACKSONVILLE, FL 32256
CONTACT: EDWARD HARDING
PHONE: (904) 652-9934

CABLE:

COMCAST CABLE
5934 RICHARD STREET
JACKSONVILLE, FL 32216
CONTACT: ANDREW SWEENEY
PHONE: (904) 738-6898

TELEPHONE:

AT&T
6628 LAKESIDE ROAD
WEST PALM BEACH, FL 33411
CONTACT: DINO FARRUGGIO
EMAIL: G27896@ATT.COM
PHONE: (561) 683-2729

GAS :

CITY OF LAKE CITY GAS/PUBLIC WORKS
180 NE GUM SWAMP ROAD
LAKE CITY, FL 32055
CONTACT: THOMAS HENRY
EMAIL: HENRYT@CFLA.COM
PHONE: (386) 758-5425

OWNER:

GWC DEVELOPMENT PARTNERS LLC
2682 W NOEGEL ROAD
LAKE CITY, FL 32055
CONTACT: DIANE BERRY
PHONE: (407) 580-5173
EMAIL: DBERRY@SCHAFERCONST.COM

DEVELOPER:

CIRCLE K STORES, INC
3802 CORPOREX PARK DRIVE, SUITE 413
TAMPA, FL 33619
CONTACT: EDWARD GIUNTA
PHONE: (407) 580-5173

CIVIL ENGINEER:

KIMLEY-HORN AND ASSOCIATES, INC.
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ORLANDO, FL 32801
CONTACT: JAROD C. STUBBS, P.E.
PHONE: (407) 409-7002
EMAIL: JAROD.STUBBS@KIMLEY-HORN.COM

ARCHITECT:

RDC COLLABORATIVE
11921 FREEDOM DRIVE, SUITE #1110
RESTON, VA 20190
CONTACT: MEGAN LARGENT
PHONE: (703) 668-0086
FAX: (703) 668-0085

SURVEYOR:

JBPRO
3530 NW 43RD STREET
GAINESVILLE, FL 32606
CONTACT: TROY V. WRIGHT
PHONE: (352) 375-8999

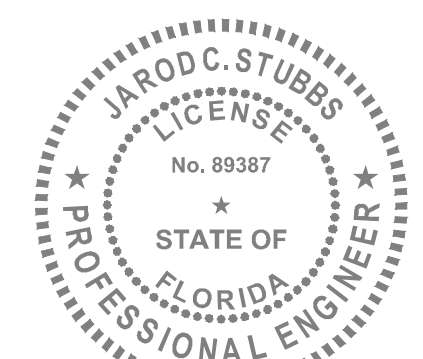
LANDSCAPE ARCHITECT:

KIMLEY-HORN AND ASSOCIATES, INC.
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PREPARED BY

Kimley»Horn

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JAROD C. STUBBS, P.E.
FL P.E. # 89387
05/04/2022

CIRCLE K - US HWY 90 & I-75 HIGH
SPEED DIESEL FUEL EXPANSION
MAY 4, 2022
ENGINEER'S PROJECT No. 149880040
REVISIONS:
DATE

SANITARY SYSTEM

- ALL PVC PIPE SHALL BE SOLID WALL POLYVINYL CHLORIDE PIPE AND COMPLY WITH ASTM D 3034 AND ALL APPLICABLE ASTM DOCUMENTS AS COVERED IN SECTION NO. 2 OF ASTM D 3034. MAIN LINES SHALL BE A MINIMUM OF 8" DIAMETER, AND LATERALS SHALL BE A MINIMUM 6" DIAMETER.
- ALL GRAVITY SEWERS MUST BE SDR 26 PVC. ELASTOMERIC GASKET JOINTS SHALL BE UTILIZED FOR PVC PIPE, AND SHALL COMPLY WITH ASTM F477, ASTM D3034 & ASTM F679. JOINTS SHALL COMPLY WITH ASTM D3212.
- ALL SLOPES FOR GRAVITY SEWER MAINS AND SERVICE CONNECTIONS SHALL COMPLY WITH THE FOLLOWING MINIMUM GRADES: 4" @ 2.00%; 6" @ 1.00% ; AND 8" @ 0.40%.
- ALL SANITARY SEWER WORK SHALL CONFORM WITH APPLICABLE CITY OF LAKE CITY WATER UTILITIES DEPARTMENT STANDARDS AND SPECIFICATIONS.
- PRIOR TO COMMENCING WORK WHICH REQUIRES CONNECTING PROPOSED FACILITIES TO EXISTING LINES OR APPURTENANCES, THE CONTRACTOR SHALL VERIFY THE LOCATION AND ELEVATION(S) OF EXISTING CONNECTION POINT(S) AND NOTIFY THE OWNER'S ENGINEER OF ANY CONFLICTS OR DISCREPANCIES.

SANITARY TESTING AND INSPECTION

- ALL GRAVITY SEWER PIPING SHALL BE SUBJECT TO A VISUAL INSPECTION BY THE OWNER'S ENGINEER AND APPLICABLE MUNICIPALITY/AGENCY. THE CONTRACTOR SHALL NOTIFY THE ENGINEER 48 HOURS IN ADVANCE TO SCHEDULE INSPECTION(S). THE CONTRACTOR SHALL BE RESPONSIBLE FOR COSTS ASSOCIATED WITH A LAMPING INSPECTION OF THE PROPOSED GRAVITY SEWER LINE CONSTRUCTION. THE CONTRACTOR SHALL PROVIDE COPIES OF THE LAMPING INSPECTION TO THE ENGINEER, THE OWNER AND THE APPLICABLE MUNICIPALITY/AGENCY.
- THE CONTRACTOR SHALL PERFORM AN INFILTRATION/EXFILTRATION TEST ON ALL GRAVITY SEWERS IN ACCORDANCE WITH THE REGULATORY AGENCY HAVING JURISDICTION. SAID TESTS ARE TO BE CERTIFIED BY THE ENGINEER OF RECORD AND SUBMITTED TO THE REGULATORY AGENCY FOR APPROVAL. THE SCHEDULING, COORDINATION AND NOTIFICATION OF ALL PARTIES IS THE CONTRACTOR'S RESPONSIBILITY.
- LEAKAGE TESTS ARE SPECIFIED REQUIRING THAT:
 - THE LEAKAGE EXFILTRATION OR INFILTRATION DOES NOT EXCEED 200 GALLONS PER INCH OF PIPE DIAMETER PER MILE PER DAY FOR ANY SECTION OF THE SYSTEM.
 - EXFILTRATION OR INFILTRATION TESTS BE PERFORMED WITH A MINIMUM POSITIVE HEAD OF 2 FEET
 - AIR TESTS, AS A MINIMUM, CONFORM TO THE TEST PROCEDURE DESCRIBED IN ASTM C-828 FOR CLAY PIPE, ASTM C 924 FOR CONCRETE PIPE, ASTM F-1417 FOR PLASTIC PIPE, AND FOR OTHER MATERIALS APPROPRIATE TEST PROCEDURES.
- CONTRACTOR TO PERFORM APPROPRIATE DEFLECTION TESTS FOR ALL FLEXIBLE PIPE. TESTING IS REQUIRED AFTER THE FINAL BACKFILL HAS BEEN IN PLACE AT LEAST 30 DAYS TO PERMIT STABILIZATION OF THE SOIL-PIPE SYSTEM. TESTING REQUIREMENTS SPECIFY:
 - NO PIPE SHALL EXCEED A DEFLECTION OF 5%.
 - USING A RIGID BALL OR MANDREL FOR THE DEFLECTION TEST WITH A DIAMETER NOT LESS THAN 95% OF THE BASE INSIDE DIAMETER OR AVERAGE INSIDE DIAMETER OF THE PIPE, DEPENDING ON WHICH IS SPECIFIED IN THE ASTM SPECIFICATION, INCLUDING THE APPENDIX, TO WHICH THE PIPE IS MANUFACTURED.
 - PERFORMING THE TEST WITHOUT MECHANICAL PULLING DEVICES.
- CONTRACTOR TO INSPECT & TEST MANHOLE FOR WATERTIGHTNESS OR DAMAGE PRIOR TO PLACING INTO SERVICE. AIR TESTING, IF SPECIFIED FOR CONCRETE SEWER MANHOLES, SHALL CONFORM TO THE TEST PROCEDURES DESCRIBED IN ASTM C-1244.

POTABLE WATER SYSTEM

- ALL DIP PIPE SHALL BE CLASS 50 OR HIGHER, REFER TO NOTE #4 BELOW FOR ADDITIONAL DIP SPECIFICATIONS. ADEQUATE MEASURES (PER AWWA, FDEP, AND POLK COUNTY CRITERIA) AGAINST CORROSION SHALL BE UTILIZED.
- ALL WATER MAIN PIPE FITTINGS AND APPURTENANCES SHALL BE INSTALLED TO COMPLY WITH POLK COUNTY STANDARDS AND SPECIFICATIONS.
- ALL WATER SERVICE LINES, VALVES AND METERS SHALL BE INSTALLED TO COMPLY WITH APPLICABLE MUNICIPALITY/AGENCY DEPARTMENT STANDARDS AND SPECIFICATIONS.
- ALL DUCTILE IRON PIPE, 4" TO 24", SHALL BE MANUFACTURED IN ACCORDANCE WITH THE LATEST EDITION OF AWWA C151/A21.51. PIPE SHALL BE FURNISHED IN 18 OR 20 FOOT SECTIONS, PIPE THICKNESS SHALL BE CLASS 50, UNLESS OTHERWISE SPECIFIED.
- ALL WATER SYSTEM CONSTRUCTION, FROM THE POINT OF CONNECTION IN THE RIGHT OF WAY UP TO AND INCLUDING POINT OF METERING AND BACK FLOW PREVENTION (IF REQUIRED), SHALL BE BUILT ACCORDING TO POLK COUNTY STANDARDS AND SPECIFICATIONS.
- CONTRACTOR TO INSTALL TEMPORARY BLOWOFFS, AT THE END(S) OF PROPOSED WATER MAINS AND SERVICE LATERALS TO BUILDING(S), TO ASSURE ADEQUATE (PER AWWA, FDEP, AND POLK COUNTY CRITERIA) FLUSHING AND DISINFECTION/CHLORINATION.
- ALL WATER MAINS SHALL BE STERILIZED IN ACCORDANCE WITH THE APPLICABLE SECTION OF THE LATEST AWWA SPECIFICATION C651 AND CITY OF Land City WATER DEPARTMENT SPECIFICATIONS.
- ALL PVC WATER MAIN, 6" TO 12" DIAMETER PIPING, SHALL BE AWWA C-900 DR-18. JOINTS SHALL BE RUBBER GASKETED PUSH-ON CONFORMING TO ASTM D1869.
- POTABLE WATER MAINS WILL BE PVC SDR 21 (200 PSI) FOR PIPES LESS THEN 4". SCHEDULE 40 AND SCHEDULE 80 PIPING MATERIAL ARE ALSO ACCEPTABLE FOR PIPES SIZES LESS THAN 4". THE ABOVE TYPE INSTALLATIONS MUST BEAR THE "NFS" STAMP FOR COMPATIBILITY WITH POTABLE WATER USE.
- ALL POLYVINYL CHLORIDE PIPE SHALL BE LAID WITH AN INSULATED 10 GAUGE A.W.G. SOLID STRAND COPPER WIRE ON TOP OF THE PIPE. THIS WIRE IS TO BE CONTINUOUS WITH SPLICES MADE ONLY BY METHODS APPROVED BY THE ENGINEER. THIS WIRE IS TO BE SECURED TO ALL VALVES, TEES AND ELBOWS.
- ALL POTABLE WATER WORK SHALL CONFORM WITH APPLICABLE POLK COUNTY UTILITIES DEPARTMENT STANDARDS AND SPECIFICATIONS.
- PVC PIPE BURIED BENEATH ROADWAYS, PARKING LOTS OR PARKING LOT ENTRANCES SHALL MEET AWWA SPECIFICATION C900 OR C905, LATEST REVISION. ALL 6" TO 12" PIPE IN SUCH LOCATIONS SHALL BE A MINIMUM OF CLASS 200, DR-14, AND ALL 14" TO 36" PIPE SHALL BE A MINIMUM OF CLASS 235, DR-18.

POTABLE WATER TESTING AND INSPECTION

- ALL COMPONENTS OF THE WATER SYSTEM, INCLUDING FITTINGS, HYDRANTS, CONNECTIONS, AND VALVES SHALL BE PROPERLY PRESSURE TESTED AND ACCEPTED BY THE OWNER'S ENGINEER. PRESSURE TESTS TO BE IN ACCORDANCE WITH POLK COUNTY UTILITIES DEPARTMENT SPECIFICATIONS. CONTRACTOR TO NOTIFY THE OWNER'S ENGINEER AND APPLICABLE AGENCY INSPECTORS 2 FULL BUSINESS DAYS IN ADVANCE OF PERFORMING TESTS.
- CONTRACTOR TO PERFORM CHLORINATION AND BACTERIOLOGICAL SAMPLING, AND OBTAIN CLEARANCE OF DOMESTIC AND FIRE LINE WATER SYSTEM(S). COPIES OF ALL BACTERIOLOGICAL TEST RESULTS ARE TO BE SUBMITTED TO THE OWNER'S ENGINEER FOR CERTIFICATION PURPOSES.
- ALL WATER MAINS SHALL BE PRESSURE TESTED IN ACCORDANCE WITH AWWA MANUAL M23, CONCERNING HYDROSTATIC TESTING OF PVC PIPING. OFF-SITE UTILITIES HYDROSTATIC TESTING TO BE WITNESSED BY THE CITY OF Land City WATER DEPARTMENT INSPECTOR.

FDOT GENERAL NOTES

- MAINTENANCE OF TRAFFIC TO BE SUPERVISED BY A CERTIFIED PERSON.
- THE CONTRACTOR SHALL NOTIFY THE DEPARTMENT A MINIMUM OF TWO BUSINESS DAYS PRIOR TO ANY LANE CLOSURES OR BEGINNING ANY CONSTRUCTION WITHIN THE FDOT RIGHT-OF-WAY.
- ALL WORK PERFORMED WITHIN THE FDOT RIGHT OF WAY SHALL BE IN ACCORDANCE WITH THE FY2021-22 OR CURRENT EDITION OF FDOT STANDARD PLANS.
- IF THE DEPARTMENT DETERMINES THAT AS-BUILT CONDITIONS VARY SIGNIFICANTLY FROM THE APPROVED PLANS, THE PERMITTEE SHALL PROVIDE AS-BUILT PLANS, ALONG WITH A RECORD DRAWINGS REPORT BY PERMITTEE'S PROFESSIONAL ENGINEER, FORM 850-040-19, WITHIN 30 DAYS.
- IT WILL BE THE RESPONSIBILITY OF THE PERMITTEE TO REPAIR ANY DAMAGE TO FDOT FACILITIES CAUSED BY CONSTRUCTION OF THE PROJECT.
- TEST RESULTS OF ANY TESTS TAKEN FOR OR DURING CONSTRUCTION OF THE PERMITTED WORK SHALL BE PROVIDED TO THE FDOT UPON REQUEST.
- ALL CONCRETE TO BE REMOVED SHALL BE SAW CUT AT THE NEAREST JOINT IN GOOD CONDITION, SO AS TO PRODUCE A CONNECTION WITH NEW CONCRETE THAT IS FREE OF CRACKS, DEFORMITY IN SHAPE, NOTICEABLE VOIDS, SURFACE IRREGULARITIES, AND OTHER DEFECTS.
- ALL CONCRETE SHALL BE AN APPROVED FDOT MIX DESIGN OF 3,000 PSI MINIMUM.
- ALL MATERIALS INSTALLED WITHIN FDOT RIGHT-OF-WAY SHALL BE LIMITED TO THOSE ON THE FDOT'S QUALIFIED PRODUCTS LIST OR APPROVED PRODUCT LIST OF TRAFFIC CONTROL SIGNALS AND DEVICES.
- THE PERMITTEE SHALL CONTACT THE CITY OF LAKE CITY TRAFFIC DEPT. (386) 758-5400.
- ALL CONSTRUCTION IN THE FDOT ROW SHALL CONFIRM TO THE LATEST EDITIONS OF THE FDOT DESIGN STANDARDS, THE FDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, AND THE FDOT UTILITY ACCOMMODATION MANUAL.
- ALL DISTURBED AREAS IN FDOT ROW SHALL BE SODDED.
- ALL WORK PERFORMED WITHIN THE FDOT RIGHT-OF-WAY SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE FDOT DESIGN STANDARDS, THE LATEST EDITION OF THE SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, AND THE 2017 UTILITY ACCOMMODATION MANUAL.
- PLEASE NOTIFY JACKSONVILLE OPERATIONS TWO BUSINESS DAYS BEFORE BEGINNING WORK @ (904) 306-7500.

CALL 48 HOURS BEFORE YOU DIG

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SUNSHINE STATE ONE CALL OF FLORIDA, INC.

CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION

CITY OF LAKE CITY FLORIDA

GENERAL NOTES

KHA PROJECT 149880040	DATE 05/04/2022	SCALE AS SHOWN	DESIGNED BY TJM	DRAWN BY TJM	CHECKED BY JCS
LICENSED PROFESSIONAL SURVEYOR STATE OF FLORIDA No. 49983 EXPIRES 06/30/2026					

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PHONE: 407-898-1511
WWW.KIMLEY-HORN.COM REGISTRY No. 35106

No.	REVISIONS	DATE	BY

SHEET NUMBER
C1.1

STORMWATER POLLUTION PREVENTION PLAN

SITE DESCRIPTION

PROJECT NAME AND LOCATION

CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION
TAX PARCEL: 24-29-11-281016-000020
CITY OF LAKE CITY, FLORIDA

*SEE COVER SHEET FOR LOCATION MAP

DEVELOPER NAME AND ADDRESS

SCHAFFER CONSTRUCTION, LLC
2601 NETWORK BLVD., SUITE 413
FRISCO, TX 75034
CONTACT: DIANE BERRY
PHONE: (407) 580-5173
EMAIL: DBERRY@SCHAFERCONST.COM

PROJECT DESCRIPTION

THE PROJECT WILL CONSIST OF CONSTRUCTING A CIRCLE K CONVENIENCE STORE BUILDING EXPANSION WITH HIGH SPEED DIESEL FUELING STATIONS AND SEMI-TRUCK PARKING ON A PREVIOUSLY MASS GRADED SITE. THE PROJECT IS 3.46 ± ACRES LOCATED ON THE NORTHEAST CORNER OF US HIGHWAY 90 AND CENTURION COURT IN LAKE CITY, FLORIDA.

PROJECT AREA: 3.46 ACRES
CONTRIBUTING DRAINAGE AREA: 3.46 ACRES
LONGITUDE: W 82° 41' 26.2" LATITUDE: N 30° 10' 51.1"

ACTIVITIES THAT REQUIRE EROSION CONTROL

PROVIDING A STABILIZED CONSTRUCTION ENTRANCE, PERIMETER, AND OTHER EROSION AND SEDIMENT CONTROLS; DEMOLITION; SITE GRADING; INSTALLATION OF STORM WATER; CURB, DRIVEWAYS, AND ROADWAY FACILITIES.

*SEE PLANS FOR THE LOCATION OF TEMPORARY SEDIMENT BARRIERS AND OTHER EROSION CONTROL METHODS.

SOIL PARAMETERS

SOIL TYPES:

SERIES NAME	HYDROLOGIC GROUP
BLANTON FINE SAND, 0-5% SLOPES	A

SEQUENCE OF MAJOR ACTIVITIES

THE ORDER OF CONSTRUCTION IS AS FOLLOWS:

1. PROVIDE STABILIZED CONSTRUCTION ENTRANCE
2. INSTALL SILT FENCES AND OTHER EROSION CONTROL METHODS
3. DEMOLITION
4. CLEAR AND GRUB FOR SEDIMENT BASIN AND EARTH DIKE
5. CONSTRUCT EARTH DIKE AND SEDIMENT BASIN
6. FINISH CLEARING AND GRUBBING
7. REMOVE AND STORE TOPSOIL
8. PROVIDE INITIAL GRADING AS REQUIRED
9. STABILIZE ALL DISTURBED AREAS AS SOON AS POSSIBLE
10. INSTALL UTILITIES, STORM SEWER, CURB AND GUTTER
11. INSTALL BASE TO ROAD AND DRIVEWAY AREA
12. FINISH GRADING ENTIRE SITE
13. CONSTRUCT FINAL PAVING
14. REMOVE ACCUMULATED SEDIMENT
15. REMOVE ANY ITEMS THAT ARE NOT REQUIRED

TIMING OF CONTROL MEASURES

THE INSTALLATION OF SILT FENCE (AND OTHER EROSION CONTROL MEASURES), A STABILIZED ENTRANCE AND SEDIMENT BASIN SHALL OCCUR PRIOR TO CLEARING AND GRUBBING ACTIVITY. AFTER CONSTRUCTION IS COMPLETE, THE ACCUMULATED SEDIMENT SHALL BE REMOVED AND THE AREAS SHALL BE REGRADED AND PERMANENTLY STABILIZED AS SHOWN ON THE PLANS.

EROSION AND SEDIMENT CONTROLS

BEST MANAGEMENT PRACTICES SHALL BE USED FOR THIS PROJECT TO CONTROL EROSION AND TURBIDITY CAUSED BY STORM WATER RUN-OFF. THE LOCATION AND DETAILS OF EROSION CONTROL METHODS ARE SHOWN ON THE PLANS. THE CONTRACTOR IS RESPONSIBLE FOR PLACING AND MAINTAINING THESE CONTROL METHODS AS SHOWN ON THE PLANS OR AS REQUIRED. HE/SHE SHALL ALSO PROVIDE THE REQUIRED EROSION PROTECTION AS REQUIRED BY LOCAL, STATE AND FEDERAL LAW.

STORM WATER MANAGEMENT

STORMWATER COLLECTION SHALL BE PROVIDED BY DRAINAGE INLETS WITHIN THE PROPOSED DRIVE AISLES. THE PROPOSED DRAINAGE INLETS WILL CONNECT INTO THE EXISTING OFFSITE STORM DRAINAGE COLLECTION SYSTEM, WHICH DRAINS TO AN OFFSITE MASTER STORMWATER POND THAT PROVIDES ATTENUATION FOR THIS SITE. THE POND IS DESIGNED IN ACCORDANCE WITH SRWMD AND LAKE CITY CODE.

STABILIZATION PRACTICES:

TEMPORARY STABILIZATION - TOPSOIL STOCK PILES AND DISTURBED PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITY TEMPORARILY OR PERMANENTLY CEASE, SHALL BE STABILIZED WITH TEMPORARY SEED AND MULCH WITHIN 7 DAYS OF THE LAST CONSTRUCTION ACTIVITY IN THAT AREA. THE TEMPORARY SEED REQUIRED CAN BE FOUND IN TABLE 1.65 A OF THE FLORIDA DEVELOPMENT MANUAL. PRIOR TO SEEDING, WHERE SOILS ARE ACIDIC 2 TONS OF PULVERIZED AGRICULTURAL LIMESTONE SHOULD BE ADDED PER ACRE AND 450 POUNDS OF 10-20-20 FERTILIZER SHALL BE APPLIED TO EACH ACRE. AFTER SEEDING, EACH AREA SHALL BE IMMEDIATELY MULCHED WITH STRAW OR EQUIVALENT EQUAL. AREAS OF THE SITE WHICH ARE TO BE PAVED SHALL BE TEMPORARILY STABILIZED BY APPLYING GEOTEXTILE AND STONE SUB-BASE UNTIL BITUMINOUS PAVEMENT CAN BE APPLIED.

PERMANENT STABILIZATION - DISTURBED PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES PERMANENTLY CEASE SHALL BE STABILIZED WITH PERMANENT SEED NO LATER THAN 7 DAYS AFTER THE LAST CONSTRUCTION ACTIVITY. THE APPROPRIATE PERMANENT SEED MIX CAN BE FOUND IN TABLES 1.66A, 1.66B AND 1.66C OF THE FLORIDA DEVELOPMENT MANUAL. PRIOR TO SEEDING, 2 TONS/ACRE OF FINELY GROUND AGRICULTURAL LIMESTONE AND THE PROPER FERTILIZER BASED ON THE TYPE OF SEEDING SHALL BE APPLIED TO EACH ACRE TO PROVIDE PLANT NUTRIENTS. AFTER SEEDING, EACH AREA SHALL BE MULCHED IMMEDIATELY.

STRUCTURAL PRACTICES:

EARTH DIKE - IF REQUIRED, AN EARTH DIKE SHALL BE CONSTRUCTED ALONG THE SITE PERIMETER. A PORTION OF THE DIKE SHALL DIVERT RUN-ON AROUND THE CONSTRUCTION SITE. THE REMAINING PORTION OF THE DIKE SHALL COLLECT RUNOFF FROM THE DISTURBED AREA AND DIRECT THE RUNOFF TO THE SEDIMENT BASIN.

SEDIMENT BASIN - A SEDIMENT BASIN SHALL BE CONSTRUCTED IN THE COMMON DRAINAGE AREA FOR THE SITE. ALL SEDIMENT COLLECTED IN THE BASIN MUST BE REMOVED FROM THE BASIN UPON COMPLETION OF CONSTRUCTION. SEDIMENT FROM THE BASIN MAY BE USED AS FILL ON THE SITE IF IT IS SUITABLE SOIL.

WASTE DISPOSAL

WASTE MATERIALS - ALL WASTE MATERIALS SHALL BE COLLECTED AND STORED IN A METAL DUMPSTER WITH A SECURE LID IN ACCORDANCE WITH ALL LOCAL AND STATE LAWS. ALL TRASH AND CONSTRUCTION DEBRIS FROM THE SITE SHALL BE DEPOSITED IN THE DUMPSTER. THE SUPERINTENDENT SHALL COORDINATE WITH THE LOCAL UTILITIES TO HAVE THE DUMPSTER EMPTIED AT LEAST TWICE A WEEK AND THE WASTE TAKEN TO AN APPROPRIATE LANDFILL. NO CONSTRUCTION WASTE MATERIALS SHALL BE BURIED ON SITE. THE SUPERINTENDENT SHALL ORGANIZE TRAINING FOR THE EMPLOYEES IN THE PROPER PRACTICES WHEN DEALING WITH WASTE MATERIALS. THE SUPERINTENDENT SHALL BE RESPONSIBLE FOR POSTING AND ENFORCING WASTE MATERIAL PROCEDURES.

HAZARDOUS WASTE - HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN ACCORDANCE WITH ALL LOCAL AND STATE LAWS OR AS DIRECTED BY THE MANUFACTURER. THE SUPERINTENDENT SHALL ORGANIZE THE PROPER TRAINING FOR EMPLOYEES IN THE PROPER PRACTICES WHEN DEALING WITH HAZARDOUS WASTE MATERIALS. THESE PROCEDURES SHALL BE POSTED ON THE SITE. THE PERSON WHO MANAGES THE SITE SHALL BE RESPONSIBLE FOR ENFORCING THE PROCEDURES.

SANITARY WASTE - SANITARY WASTE SHALL BE COLLECTED AND DISPOSED OF IN ACCORDANCE WITH ALL LOCAL AND STATE LAWS. THE SUPERINTENDENT SHALL COORDINATE WITH THE LOCAL UTILITY FOR COLLECTION OF THE SANITARY WASTE AT LEAST THREE TIMES A WEEK TO PREVENT SPILLAGE ONTO THE SITE.

OFF-SITE TRACKING

A STABILIZED CONSTRUCTION ENTRANCE SHALL BE PROVIDED TO REDUCE SEDIMENT TRACKING OFFSITE. THE MAJOR ROAD CONNECTED TO THE PROJECT SHALL BE CLEANED ONCE A DAY TO REMOVE ANY EXCESS MUD, DIRT OR ROCK RESULTING FROM CONSTRUCTION TRAFFIC. ALL TRUCKS HAULING MATERIALS OFFSITE SHALL BE COVERED WITH A TARPAULIN.

ITEMS REQUIRING POLLUTION PREVENTION

THE FOLLOWING ITEMS ARE EXPECTED TO BE PRESENT ON THE PROJECT SITE:

-ASPHALT
-CONCRETE
-FERTILIZERS
-METAL PIECES
-PETROLEUM BASED PRODUCTS
-TAR
-CLEANING SUPPLIES
-DETERGENTS
-MASONARY BLOCK/BRICKS
-PAINT
-WOOD

THE FOLLOWING ARE NON-STORM WATER SOURCES THAT WILL BE ENCOUNTERED AT THE SITE AND SHOULD BE DIRECTED TO THE SEDIMENT BASIN PRIOR TO DISCHARGE:

-UNCONTAMINATED GROUNDWATER EXPOSED DURING EXCAVATION
-WATER FROM WATER LINE FLUSHING
-PAVEMENT WASH WATERS (WHERE NO SPILLS OR LEAKS OF TOXIC OR HAZARDOUS MATERIALS HAVE OCCURRED).

SPILL PREVENTION AND CONTROL

THE FOLLOWING ARE THE MATERIAL MANAGEMENT PRACTICES THAT WILL BE USED TO REDUCE THE RISK OF SPILLS OR OTHER ACCIDENTAL EXPOSURE OF MATERIALS AND SUBSTANCES TO STORM WATER RUNOFF.

GOOD HOUSEKEEPING

SUPERINTENDENT SHALL INSPECT PROJECT AREA DAILY FOR PROPER STORAGE, USE, AND DISPOSAL OF CONSTRUCTION MATERIALS.

STORE ONLY ENOUGH MATERIAL ON SITE FOR PROJECT COMPLETION.

ALL SUBSTANCES SHOULD BE USED BEFORE DISPOSAL OF CONTAINER.

ALL CONSTRUCTION MATERIALS STORED SHALL BE ORGANIZED AND IN THE PROPER CONTAINER AND IF POSSIBLE, STORED UNDER A ROOF OR PROTECTIVE COVER.

PRODUCTS SHALL NOT BE MIXED UNLESS DIRECTED BY THE MANUFACTURER.

ALL PRODUCTS SHALL BE USED AND DISPOSED OF ACCORDING TO THE MANUFACTURER'S RECOMMENDATIONS.

HAZARDOUS PRODUCTS

MATERIALS SHOULD BE KEPT IN ORIGINAL CONTAINER WITH LABELS UNLESS THE ORIGINAL CONTAINERS CANNOT BE RESEALED. IF ORIGINAL CONTAINERS CANNOT BE USED, LABELS AND PRODUCT INFORMATION SHALL BE SAVED.

PROPER DISPOSAL PRACTICES SHALL ALWAYS BE FOLLOWED IN ACCORDANCE WITH MANUFACTURER AND LOCAL/STATE REGULATIONS.

PRODUCT SPECIFIC PRACTICES

PETROLEUM PRODUCTS MUST BE STORED IN PROPER CONTAINERS AND CLEARLY LABELED. VEHICLES CONTAINING PETROLEUM PRODUCTS SHALL BE PERIODICALLY INSPECTED FOR LEAKS. PRECAUTIONS SHALL BE TAKEN TO AVOID LEAKAGE OF PETROLEUM PRODUCTS ON SITE.

THE MINIMUM AMOUNT OF FERTILIZER SHALL BE USED AND MIXED INTO THE SOIL IN ORDER TO LIMIT EXPOSURE TO STORM WATER. FERTILIZERS SHALL BE STORED IN A COVERED SHED. THE CONTENTS OF ANY PARTIALLY USED BAGS OF FERTILIZER SHALL BE TRANSFERRED TO A SEALABLE PLASTIC BIN TO AVOID SPILLS.

PAINT CONTAINERS SHALL BE SEALED AND STORED WHEN NOT IN USE. EXCESS PAINT MUST BE DISPOSED OF IN AN APPROVED MANNER.

CONCRETE TRUCKS SHALL NOT BE ALLOWED TO WASH OUT OR DISCHARGE SURPLUS CONCRETE OR DRUM WASH WATER ON THE SITE.

SPILL CONTROL PRACTICES

IN ADDITION TO THE GOOD HOUSEKEEPING AND MATERIAL MANAGEMENT PRACTICES DISCUSSED IN THE PREVIOUS SECTIONS OF THIS PLAN, THE FOLLOWING PRACTICES SHALL BE FOLLOWED FOR SPILL PREVENTION AND CLEANUP:

-SPILL CLEANUP INFORMATION SHALL BE POSTED ON SITE TO INFORM EMPLOYEES ABOUT CLEANUP PROCEDURES AND RESOURCES.

-THE FOLLOWING CLEAN-UP EQUIPMENT MUST BE KEPT ON-SITE NEAR THE MATERIAL STORAGE AREA: GLOVES, MOPS, RAGS, BROOMS, DUST PANS, SAND, SAWDUST, LIQUID ABSORBER, GOGGLES, AND TRASH CONTAINERS.

-ALL SPILLS SHALL BE CLEANED UP AS SOON AS POSSIBLE.

-WHEN CLEANING A SPILL, THE AREA SHOULD BE WELL VENTILATED AND THE EMPLOYEE SHALL WEAR PROPER PROTECTIVE COVERING TO PREVENT INJURY.

-TOXIC SPILLS MUST BE REPORTED TO THE PROPER AUTHORITY REGARDLESS OF THE SIZE OF THE SPILL.

-AFTER A SPILL, THE PREVENTION PLAN SHALL BE REVIEWED AND CHANGED TO PREVENT FURTHER SIMILAR SPILLS FROM OCCURRING. THE CAUSE OF THE SPILL, MEASURES TO PREVENT IT, AND HOW TO CLEAN THE SPILL UP SHALL BE RECORDED.

-THE SUPERINTENDENT SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR AND IS RESPONSIBLE FOR THE DAY TO DAY SITE OPERATIONS. THE SUPERINTENDENT ALSO OVERSEES THE SPILL PREVENTION PLAN AND SHALL BE RESPONSIBLE FOR EDUCATING THE EMPLOYEES ABOUT SPILL PREVENTION AND CLEANUP PROCEDURES.

MAINTENANCE AND INSPECTION PRACTICES

THE FOLLOWING ARE MAINTENANCE AND INSPECTION PRACTICES THAT SHALL BE COMPLETED BY THE CONTRACTOR:

-ALL SEDIMENT AND EROSION CONTROL METHODS SHALL BE INSPECTED AT LEAST ONCE EVERY SEVEN (7) CALENDAR DAYS AND WITHIN 24 HOURS OF THE END OF A STORM THAT IS 0.50 INCHES OR GREATER BY A QUALIFIED INSPECTOR.

-ALL SEDIMENT AND EROSION CONTROL METHODS SHALL BE KEPT IN GOOD CONDITION. REPAIRS MUST BE MADE WITHIN 7 CALENDAR DAYS OF INSPECTION.

-THE SILT FENCE SHALL BE INSPECTED PERIODICALLY FOR HEIGHT OF SEDIMENT AND CONDITION OF FENCE.

-THE SILT FENCE SHALL BE CLEARED OF SEDIMENT WHEN SEDIMENT MEASURES ONE-THIRD THE HEIGHT OF THE FENCE.

-THE SEDIMENT BASINS/DITCHES SHALL BE CHECKED PERIODICALLY FOR DEPTH OF SEDIMENT. THEY SHALL BE CLEANED WHEN SEDIMENT REACHES 10% OF TOTAL CAPACITY AND AFTER CONSTRUCTION IS COMPLETE.

-ALL SEEDING SHALL BE CHECKED FOR PROPER GROWTH AND UNIFORMITY. UNSTABILIZED AREAS SHALL BE RE-SODDED.

-A MAINTENANCE REPORT SHALL BE COMPLETED DAILY AFTER EACH INSPECTION OF THE SEDIMENT AND EROSION CONTROL METHODS. THE REPORTS SHALL BE FILED IN AN ORGANIZED MANNER AND RETAINED ON-SITE DURING CONSTRUCTION. AFTER CONSTRUCTION IS COMPLETED, THE REPORTS SHALL BE SAVED FOR AT LEAST THREE YEARS. THE REPORTS SHALL BE AVAILABLE FOR ANY AGENCY THAT HAS JURISDICTION OVER EROSION CONTROL.

-THE SUPERINTENDENT SHALL ORGANIZE THE TRAINING FOR INSPECTION PROCEDURES AND PROPER EROSION CONTROL METHODS FOR EMPLOYEES THAT COMPLETE INSPECTIONS AND REPORTS.

POLLUTION PREVENTION PLAN CERTIFICATION

I CERTIFY UNDER PENALTY OF LAW THAT THIS DOCUMENT AND ALL ATTACHMENTS WERE PREPARED UNDER MY DIRECTION OR SUPERVISION IN ACCORDANCE WITH A SYSTEM DESIGNED TO ASSURE THAT QUALIFIED PERSONNEL PROPERLY GATHERED AND EVALUATED THE INFORMATION SUBMITTED. BASED ON MY INQUIRY OF THE PERSON OR PERSONS WHO MANAGE THE SYSTEM, OR THOSE PERSONS DIRECTLY RESPONSIBLE FOR GATHERING THE INFORMATION, THE INFORMATION SUBMITTED IS, TO THE BEST OF MY KNOWLEDGE AND BELIEF, TRUE, ACCURATE, AND COMPLETE. I AM AWARE THAT THERE ARE SIGNIFICANT PENALTIES FOR SUBMITTING FALSE INFORMATION, INCLUDING THE POSSIBILITY OF FINE AND IMPRISONMENT FOR KNOWING VIOLATIONS.

SIGNED: _____ DATE: _____

JAROD C. STUBBS, P.E.
FLORIDA REGISTRATION NUMBER: 89387
PROFESSIONAL ENGINEER

CONTRACTOR'S CERTIFICATION

I CERTIFY UNDER PENALTY OF LAW THAT I UNDERSTAND, SHALL COMPLY WITH, THE TERMS AND CONDITIONS OF THE STATE OF FLORIDA GENERIC PERMIT FOR STORMWATER DISCHARGE FORM LARGE AND SMALL CONSTRUCTION ACTIVITIES AND THIS STORMWATER POLLUTION PREVENTION PLAN PREPARED THEREUNDER.

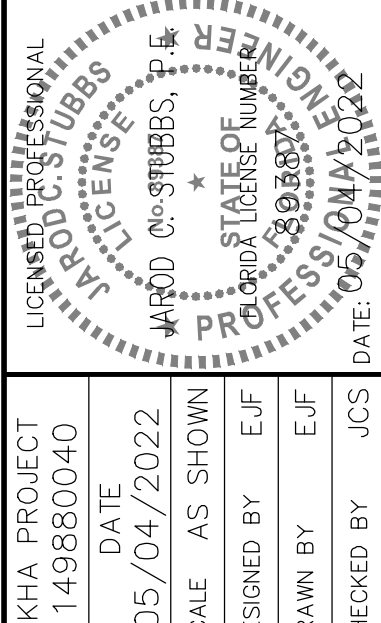
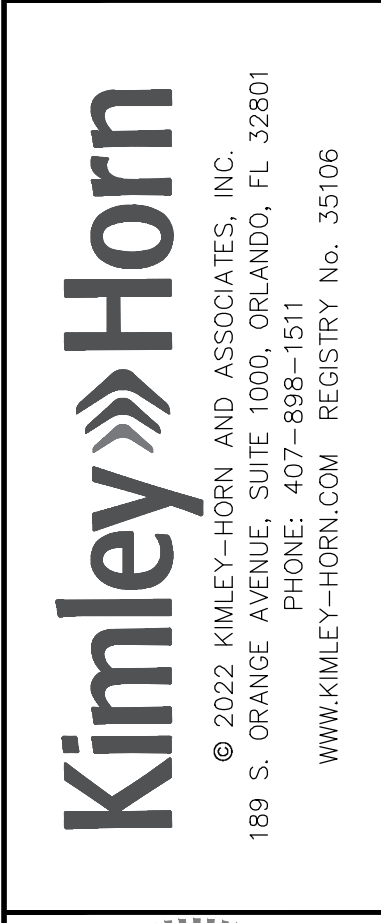
SIGNATURE AND DATE	NAME AND TITLE, COMPANY / ADDRESS AND TELEPHONE NUMBER	RESPONSIBILITY

CIRCLE K - US
HWY 90 & I-75
FUEL EXPANSION
FLORIDA
CITY OF LAKE CITY

STORMWATER
POLLUTION
PREVENTION PLAN

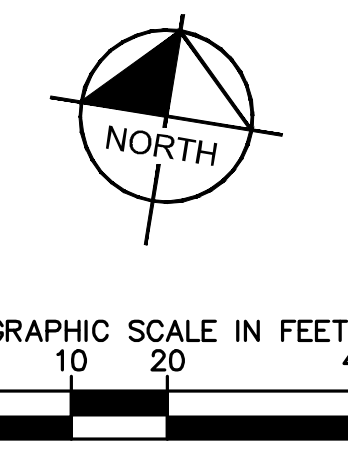
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No.	REVISIONS	DATE



KHA PROJECT	DATE	SCALE	DESIGNED BY	DRAWN BY	CHECKED BY	JCS
149880040	05/04/2022	AS SHOWN	EJF	EJF		

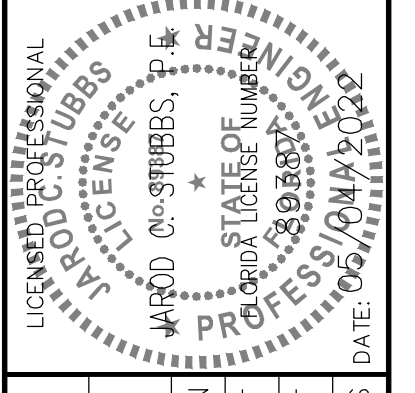
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- NOTES:**
- CONTRACTOR TO COORDINATE WITH UTILITY COMPANY FOR DEMOLITION REQUIREMENTS OF ELECTRICAL SYSTEMS.
 - CONTRACTOR SHALL UTILIZE BEST MANAGEMENT PRACTICES AS NEEDED TO PREVENT SYSTEM POLLUTION DURING TIME OF CONSTRUCTION.
 - REFER TO SWPPP SHEET C2.0 FOR ADDITIONAL NOTES AND DETAILS.
 - INSTALL AND MAINTAIN SILT FENCE AT LIMITS OF CONSTRUCTION. SEE DETAIL SHEET C7.0.
 - PROVIDE SOIL TRACKING PREVENTION DEVICE AT ALL CONSTRUCTION ACCESS POINTS. SEE DETAIL SHEET C7.1.
 - UTILIZE PERFORATED SOCK DRAIN (OR EQUIVALENT) IN FRONT OF EXISTING/PROPOSED CURB INLETS ADJACENT TO CONSTRUCTION ACTIVITIES. SEE DETAIL SHEET C7.1.
 - INSTALL AND MAINTAIN FILTER FABRIC UNDER GRATES OF EXISTING/PROPOSED INLETS. SEE SHEET C7.1.
 - CONTRACTOR TO ENSURE ADEQUATE COVER REMAINS OVER ALL EXISTING UTILITIES.
 - CONTRACTOR TO VERIFY EXISTING COVER OVER ALL UTILITIES BEFORE START OF CONSTRUCTION AND TO COORDINATE WITH THE ENGINEER OF RECORD PRIOR TO START OF CONSTRUCTION IF DESIGN DOES NOT PROVIDE 36" COVER.
 - CONTRACTOR IS TO VERIFY EXISTING SANITARY, STORM, WATER, ELECTRIC, PHONE, CABLE, AND NATURAL GAS SERVICES TO BUILDINGS SCHEDULED FOR DEMOLITION. SERVICES ARE TO BE ISOLATED FROM THE MAIN UTILITY SERVICE CONNECTIONS, AND CAPPED AND/OR REMOVED AS REQUIRED BY THE UTILITY PROVIDER. UTILITY SERVICES ARE TO BE ISOLATED IN A MANNER THAT WILL INSURE THAT ADJACENT PROPERTIES REMAIN CONNECTED WITHOUT EXPERIENCING AN INTERRUPTION OF SERVICE.
 - THERE MAY BE ON-SITE UNDERGROUND UTILITIES (INCLUDING BUT NOT LIMITED TO IRRIGATION, SANITARY SEWER, POTABLE WATER LINES, NATURAL GAS LINES, FIBER OPTIC, ELECTRIC, TELEPHONE AND CABLE LINES) THAT WERE NOT LOCATED OR IDENTIFIED BY THE PROJECT SURVEYOR. PRIOR TO CONSTRUCTION START, CONTRACTOR SHALL FIELD VERIFY ALL EXISTING ON-SITE UTILITIES.
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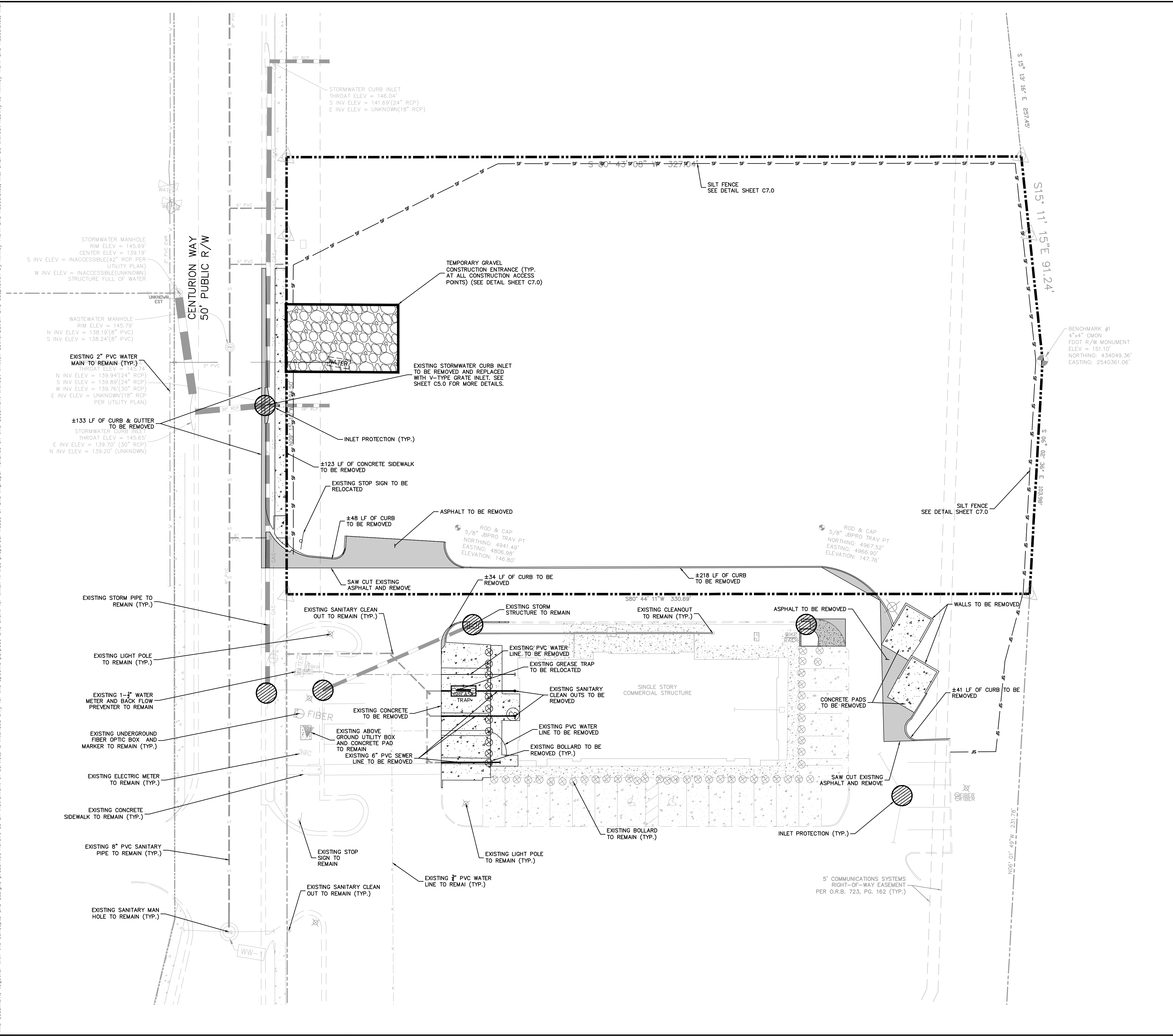


KHA PROJECT	149880040
DATE	05/04/2022
SCALE	AS SHOWN
DESIGNED BY	EJF
DRAWN BY	EJF
CHECKED BY	JCS

EXISTING CONDITIONS & DEMOLITION PLAN

CIRCLE K - US HWY 30 & I-75 FUEL EXPANSION
 CITY OF LAKE CITY, FLORIDA

SHEET NUMBER
C3.0



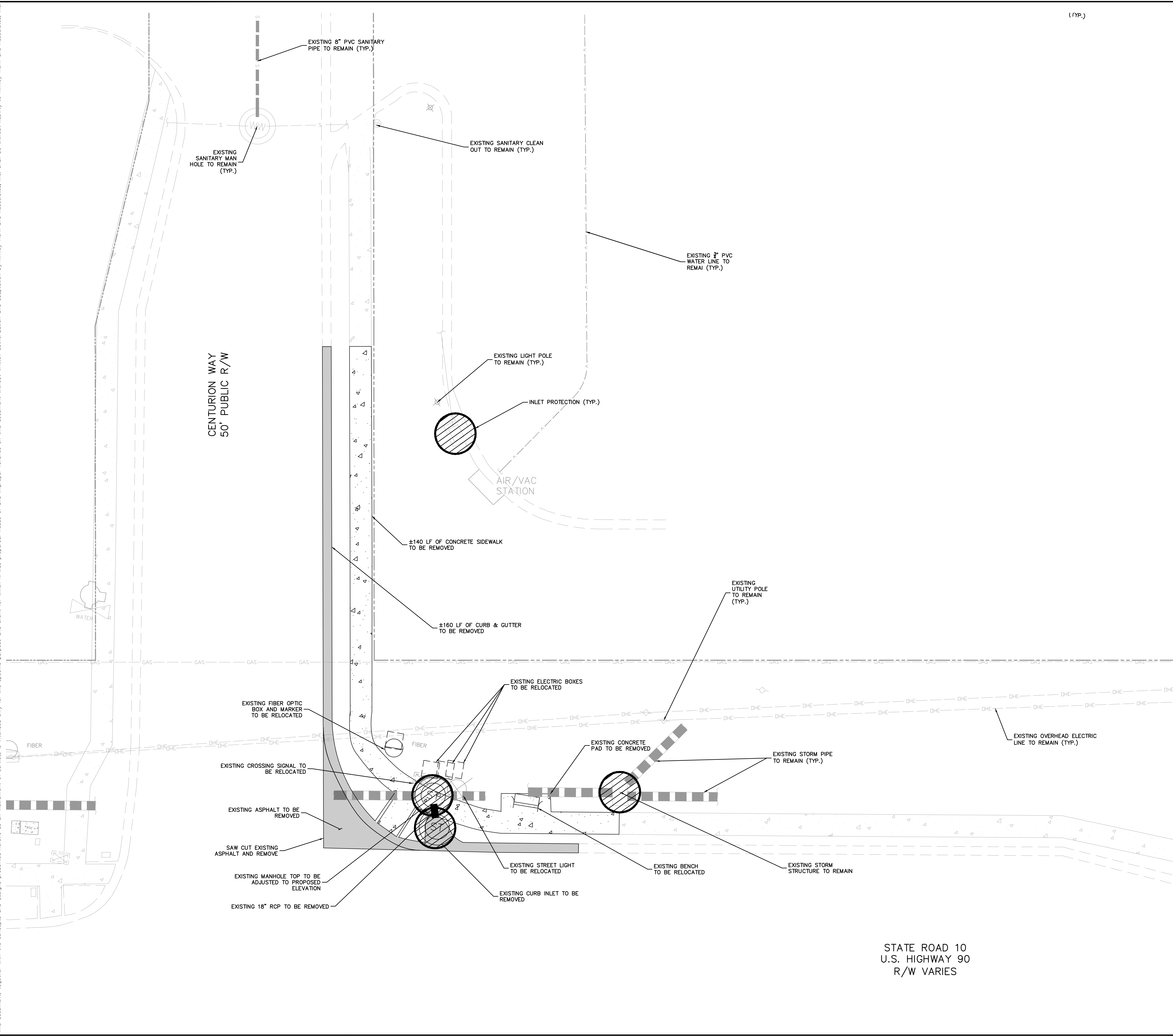
LEGEND

	PROPERTY LINE (TYP.)
	INLET PROTECTION (SEE DETAIL SHEET C7.0)
	TYPE III SILT FENCE (SEE DETAIL SHEET C7.0)
	EXISTING TO BE DEMOLISHED
	EXISTING SEWER MAIN
	EXISTING WATER MAIN
	EXISTING STORM PIPE
	EXISTING GAS MAIN
	EXISTING OVERHEAD ELECTRIC LINE
	EXISTING TREE TO REMAIN
	EXISTING TREE TO BE REMOVED
	EXISTING FORCE MAIN

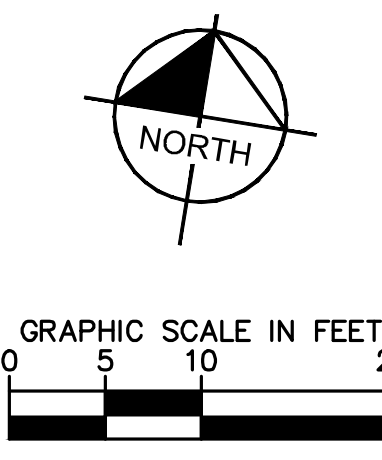
VERTICAL DATUM:
 ELEVATIONS ARE REFERENCED TO PLAT BENCHMARKS AS SHOWN ON THE FACE OF THE PLAT OF GATEWAY CROSSING REPLAT OF LOTS 2 AND 3, PLAT BOOK 9, PAGE 151

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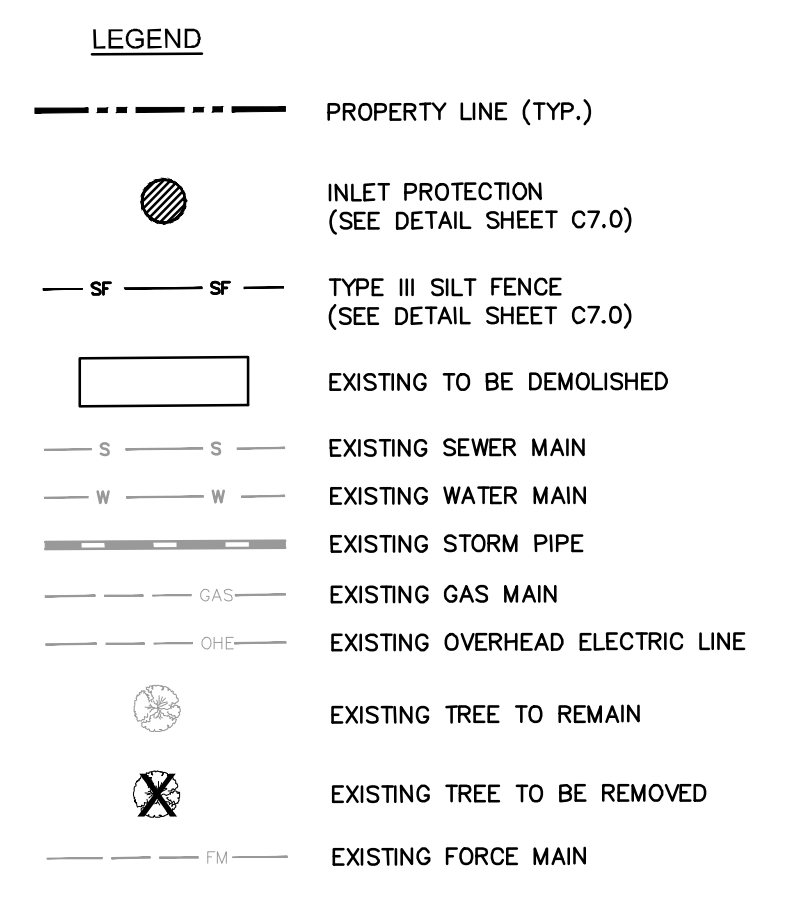
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(TYP.)



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LICENSED PROFESSIONAL ENGINEER
 STATE OF FLORIDA
 PROFESSIONAL REGISTERED NUMBER: 120883
 DATE: 06/11/2011

KHA PROJECT: 149880040
 DATE: 05/04/2022
 SCALE: AS SHOWN
 DESIGNED BY: E/JF
 DRAWN BY: E/JF
 CHECKED BY: JCS

EXISTING CONDITIONS & DEMOLITION PLAN

CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION

CITY OF LAKE CITY FLORIDA

SHEET NUMBER **C3.1**

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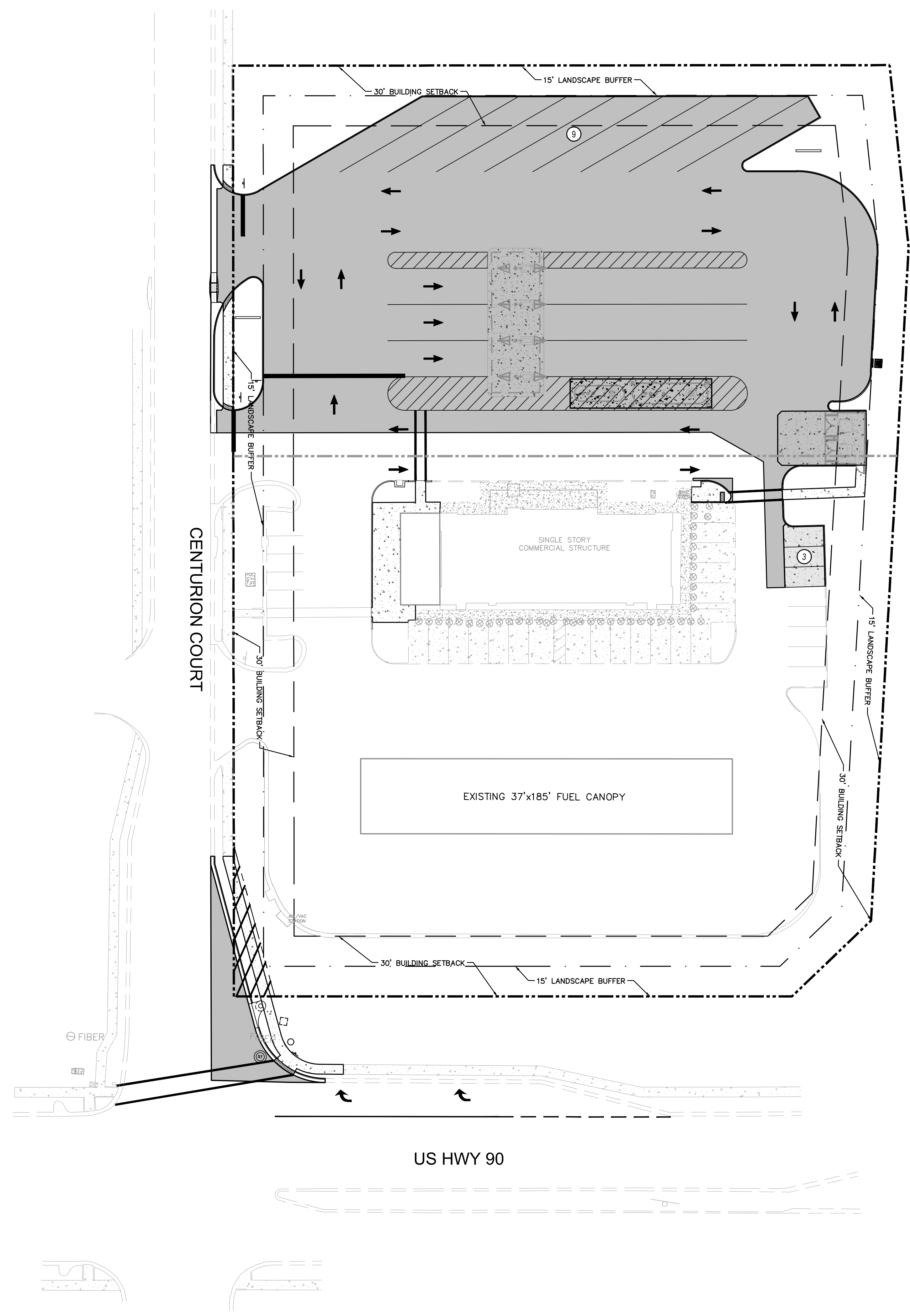
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STATE ROAD 10
 U.S. HIGHWAY 90
 R/W VARIES

Plotted By: Parks, Etham - Sheet Set: CIRCLE K - US HWY 30 & I-75 High Speed Diesel Fuel Expansion - Layout: C4.0 OVERALL SITE PLAN - June 08, 2022 - 04:29:42pm - K:\ORL\Civil\149880040-Circle K USBD & I75 CADD\CONSIST\PlanSheets\C4.0 - SITE PLAN.dwg
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- NOTES:**
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 2. BUILDING DIMENSIONS ARE TO THE OUTSIDE FACE OF BUILDING UNLESS OTHERWISE NOTED.
 3. REFER TO ARCHITECTURAL AND STRUCTURAL PLANS TO VERIFY ALL BUILDING DIMENSIONS.
 4. REFER TO SIGNAGE PLANS FOR MONUMENT SIGN DETAILS.
 5. SEE MEP PLANS FOR ELECTRICAL DRAWINGS.
 6. ALL PROPOSED ON-SITE STRIPING AND PAVEMENT MARKING WILL BE PAINTED UNLESS OTHERWISE NOTED AND IN ACCORDANCE WITH FDOT INDEX 711-001.
 7. REFER TO ARCHITECTURAL PLANS FOR PROPOSED TRASH CAN LOCATIONS AND DESIGN.
 8. BOLLARDS IN SIDEWALK ADJACENT TO BUILDING SHALL BE COVERED WITH RED PLASTIC COVERS TO BE SUPPLIED BY CONTRACTOR.
 9. BOLLARDS UNDER CANOPY SHALL BE COVERED WITH GRAY PLASTIC COVERS TO BE SUPPLIED BY CONTRACTOR (SEE FUEL PUMP DESIGNER PLANS FOR MORE DETAIL).
 10. REFER TO ARCHITECTURAL PLANS FOR SITE LIGHTING AND ELECTRICAL PLANS.
 11. ALL SIGNAGE AND PAVEMENT MARKINGS SHALL MEET MUTCD AND FDOT STANDARDS.
 12. ALL SIGNAGE SHALL MEET THE REQUIREMENTS OF CITY OF LAKE CITY LAND DEVELOPMENT CODE, CHAPTER 7, SEC. 760

SITE DATA:

PROJECT AREA:	3.46± ACRES (150,953 SF)
FUTURE LAND USE:	COMMERCIAL
EXISTING ZONING:	CHI - COMMERCIAL, HIGHWAY INTERCHANGE
EXISTING USE:	UNDEVELOPED
PROPOSED USE:	COMMERCIAL
BUILDING HEIGHT:	
PROPOSED:	1 STORY / <35 FT
FAR:	0.0388
EXISTING PAVEMENT AREA:	99,714.2 SF (2.29 AC) (66%)
PROPOSED PAVEMENT AREA:	34,981 SF (0.803 AC) (23%)
PROPOSED IMPERVIOUS AREA:	
BUILDING AREA :	5,863 SF (0.135 AC) (3.88%)
(EXISTING BLDG+EXPANSION)	
ASPHALT/CONCRETE AREA:	106,401.5 SF (2.44 AC) (70.5%)
(EXISTING+HSD EXPANSION)	
TOTAL IMPERVIOUS AREA:	112,264.5 SF (2.58 AC) (65%)

PARKING REQUIRED

CONVENIENCE STORE (WITH GAS STATION)	
1 SPACES / 150 SF NON-STORAGE AREA (5,043 SF)	34
TOTAL REQUIRED PARKING	34

PARKING PROVIDED

PROPOSED HANDICAP SPACES:	2
PROPOSED REGULAR SPACES:	31
PROPOSED SEMI TRUCK SPACES:	9
PROPOSED ON-SITE SPACES:	42

BICYCLE PARKING

REQUIRED SPACES:	0
PROVIDED SPACES:	4

BUILDING SETBACKS

	REQUIRED	PROVIDED
SIDE (WEST):	30 FT	83 FT
REAR (NORTH):	30 FT	220 FT
FRONT (SOUTH):	30 FT	196 FT
SIDE (EAST):	30 FT	107 FT

LANDSCAPE SETBACKS

	REQUIRED	PROVIDED
SIDE (WEST):	15 FT	15 FT
REAR (NORTH):	15 FT	15 FT
FRONT (SOUTH):	N/A	0 FT
SIDE (EAST):	15 FT	15 FT

LEGEND

	PROPERTY LINE (TYP.)
	PROPOSED ASPHALT PAVEMENT (SEE DETAIL SHEET C7.0)
	PROPOSED CONCRETE SIDEWALK (SEE DETAIL SHEET C7.0)
	PROPOSED MEDIUM DUTY CONCRETE (SEE DETAIL SHEET C7.0)
	PROPOSED HEAVY DUTY CONCRETE (SEE DETAIL SHEET C7.0)

VERTICAL DATUM:
 ELEVATIONS ARE REFERENCED TO PLAT BENCHMARKS AS SHOWN ON THE FACE OF THE PLAT OF GATEWAY CROSSING REPLAT OF LOTS 2 AND 3, PLAT BOOK 9, PAGE 151

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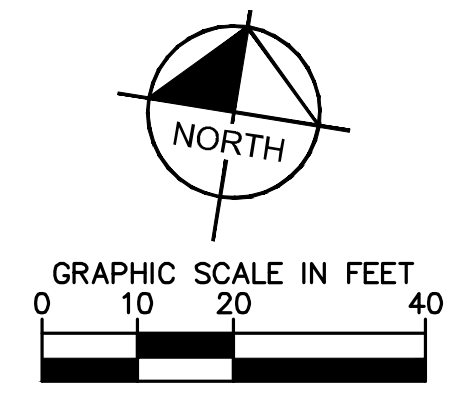
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LICENSED PROFESSIONAL ENGINEER
 STATE OF FLORIDA
 PROFESSIONAL REGISTRY NO. 10614
 DATE: 05/04/2022
 SCALE: AS SHOWN
 DESIGNED BY: EJJF
 DRAWN BY: EJJF
 CHECKED BY: JCS

OVERALL SITE PLAN

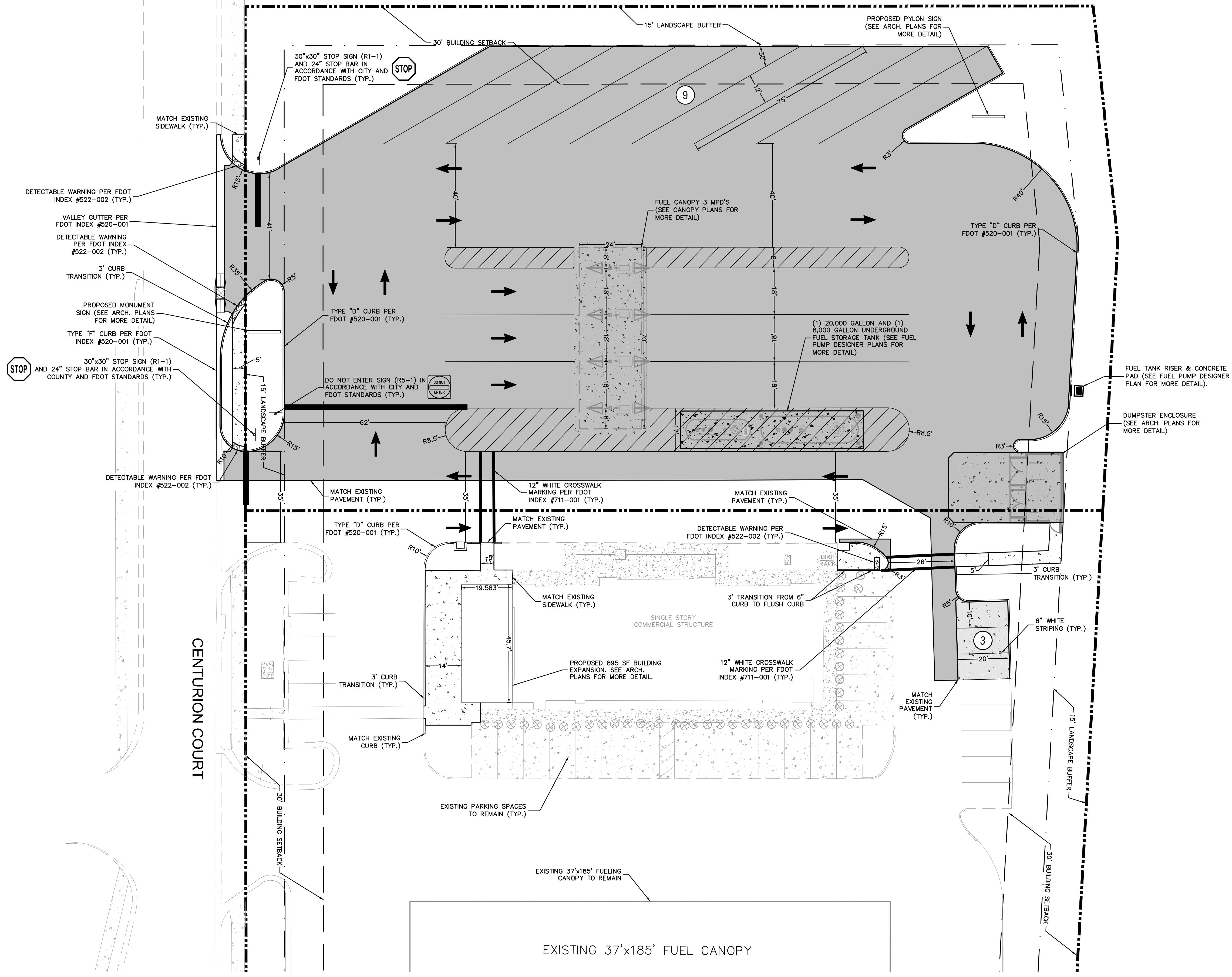
CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION
 CITY OF LAKE CITY FLORIDA
 SHEET NUMBER
C4.0

Plotted By: Parks, Etham - Sheet Set: CIRCLE K - US HWY 30 & I-75 High Speed Diesel Fuel Expansion - Layout: CA.1 - SITE PLAN - June 08, 2022 - 04:29:35pm - K:\ORL\CIVIL\49880040-Circle K US90 & I75 CARDACONST\PlanSheets\C4.0 - SITE PLAN.dwg
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NOTES:

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INTERSTATE 75 OFF-RAMP

LEGEND

- PROPERTY LINE (TYP.)
- PROPOSED ASPHALT PAVEMENT (SEE DETAIL SHEET C7.0)
- PROPOSED CONCRETE SIDEWALK (SEE DETAIL SHEET C7.0)
- PROPOSED MEDIUM DUTY CONCRETE (SEE DETAIL SHEET C7.0)
- PROPOSED HEAVY DUTY CONCRETE (SEE DETAIL SHEET C7.0)

VERTICAL DATUM:
 ELEVATIONS ARE REFERENCED TO PLAT BENCHMARKS AS SHOWN ON THE FACE OF THE PLAT OF GATEWAY CROSSING REPLAT OF LOTS 2 AND 3, PLAT BOOK 9, PAGE 151

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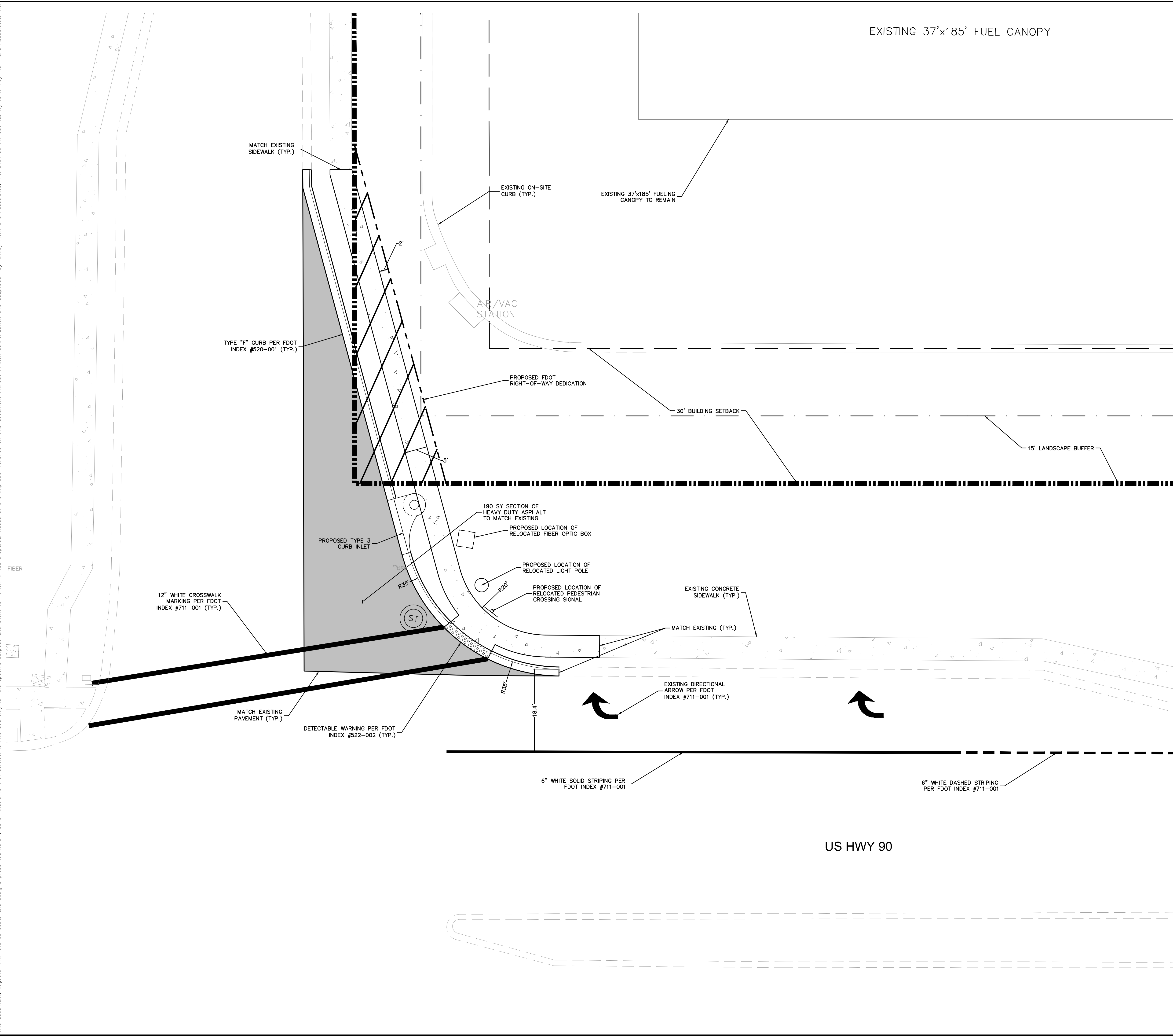
KHA PROJECT 149880040
 DATE 05/04/2022
 SCALE AS SHOWN
 DESIGNED BY E.J.F.
 DRAWN BY E.J.F.
 CHECKED BY JCS

SITE PLAN

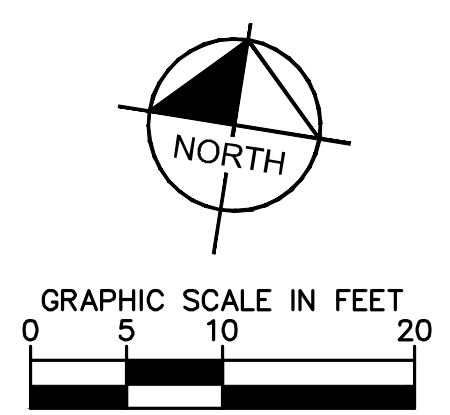
CIRCLE K - US HWY 30 & I-75 FUEL EXPANSION
 CITY OF LAKE CITY FLORIDA

SHEET NUMBER **C4.1**

Plotted By: Parks, Ethan - Sheet Set: CIRCLE K - US HWY 30 & I-75 High-Speed Diesel Fuel Expansion - Layout: CA.2 INTERCHANGE MODIFICATIONS PLAN - June 08, 2022 - 04:28:37pm - K:\ORL_CIVIL\49880040-Circle K US90 & I75\CADD\CONSTR\PlanSheets\C4.0 - SITE PLAN.dwg
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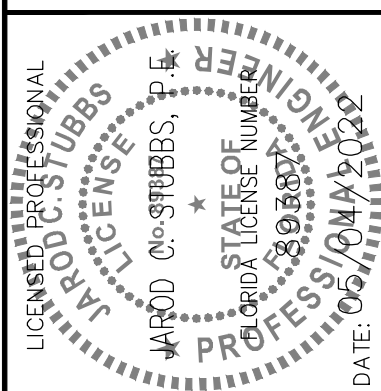
EXISTING 37'x185' FUEL CANOPY



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 4. ALL SIGNAGE AND PAVEMENT MARKINGS SHALL MEET MUTCD AND FDOT STANDARDS.

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DATE	05/04/2022
SCALE	AS SHOWN
DESIGNED BY	EJF
DRAWN BY	EJF
CHECKED BY	JCS

**INTERSECTION
 MODIFICATION PLAN**

**CIRCLE K - US HWY
 90 & I-75 FUEL
 EXPANSION**
 CITY OF LAKE CITY FLORIDA
 SHEET NUMBER
C4.2

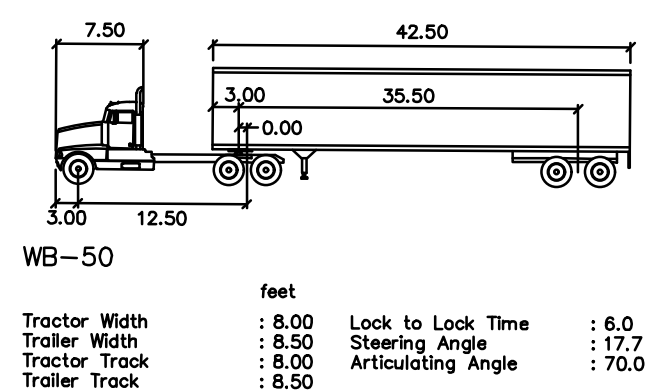
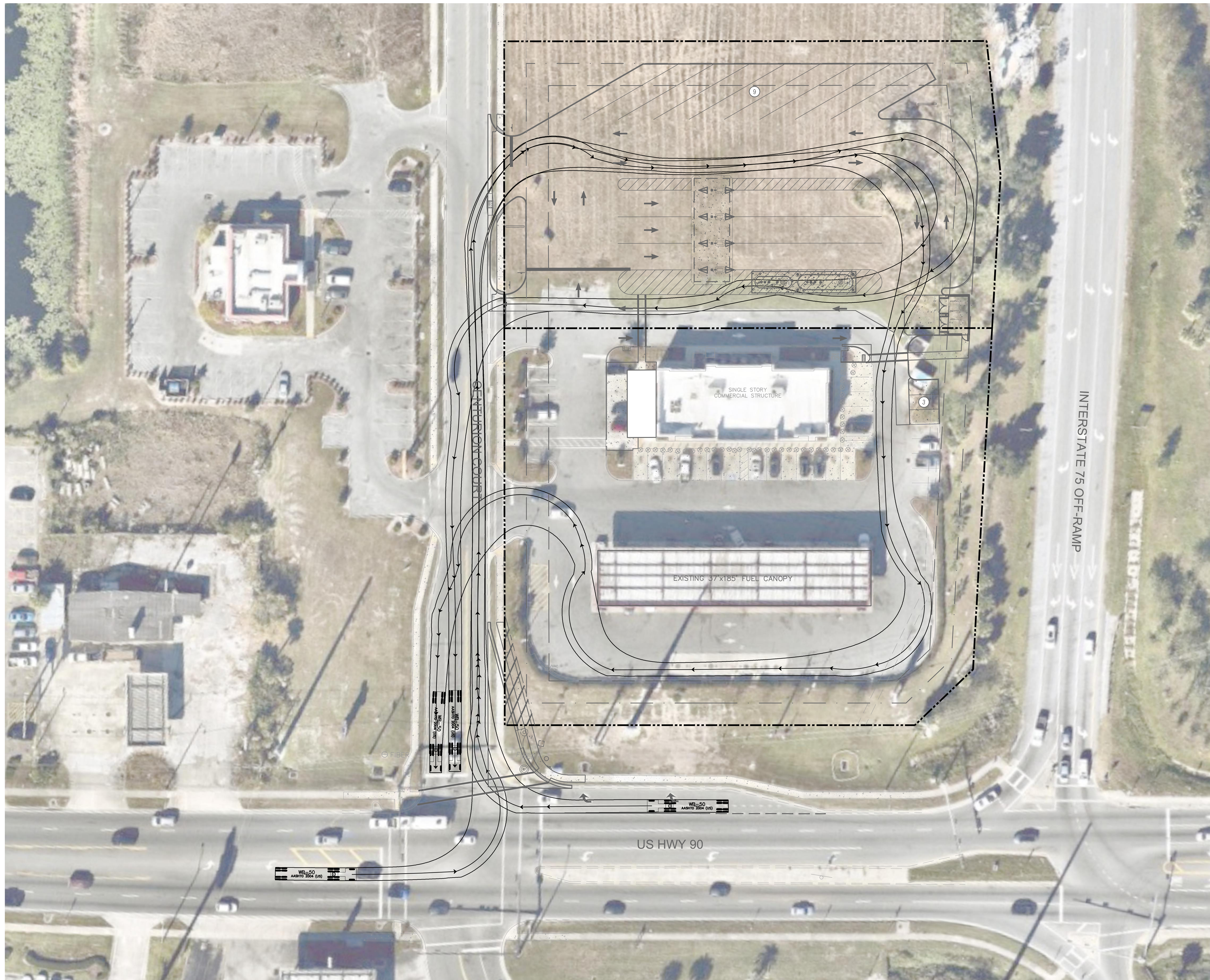
LEGEND

	PROPERTY LINE (TYP.)
	PROPOSED ASPHALT PAVEMENT (SEE DETAIL SHEET C7.0)
	PROPOSED CONCRETE SIDEWALK (SEE DETAIL SHEET C7.0)
	PROPOSED MEDIUM DUTY CONCRETE (SEE DETAIL SHEET C7.0)
	PROPOSED HEAVY DUTY CONCRETE (SEE DETAIL SHEET C7.0)

VERTICAL DATUM:
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Plotted By: Parks, Ethan - Sheet Set: CIRCLE K - US HWY 90 & I-75 High Speed Diesel Fuel Expansion - Layout: C4.3 FUEL TRUCK MOVEMENTS - June 08, 2022 04:29:40pm - K:\VDR\Civil\149880040-Circle K US90 & I75\GADD\CONST\PlanSheets\C4.0 - SITE PLAN.dwg
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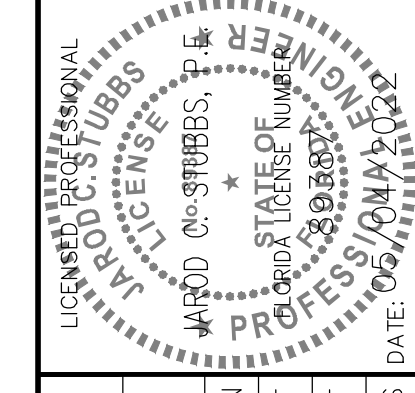
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DESIGNED BY	EJF
DRAWN BY	EJF
CHECKED BY	JCS
JCS DATE	05/04/2022

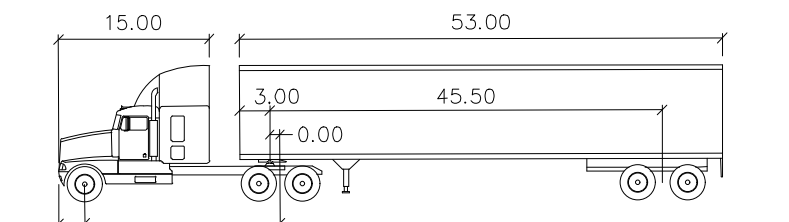
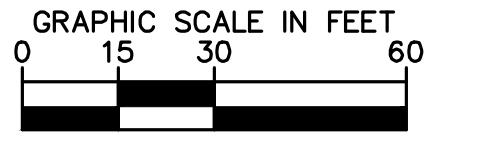
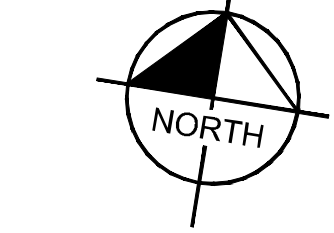
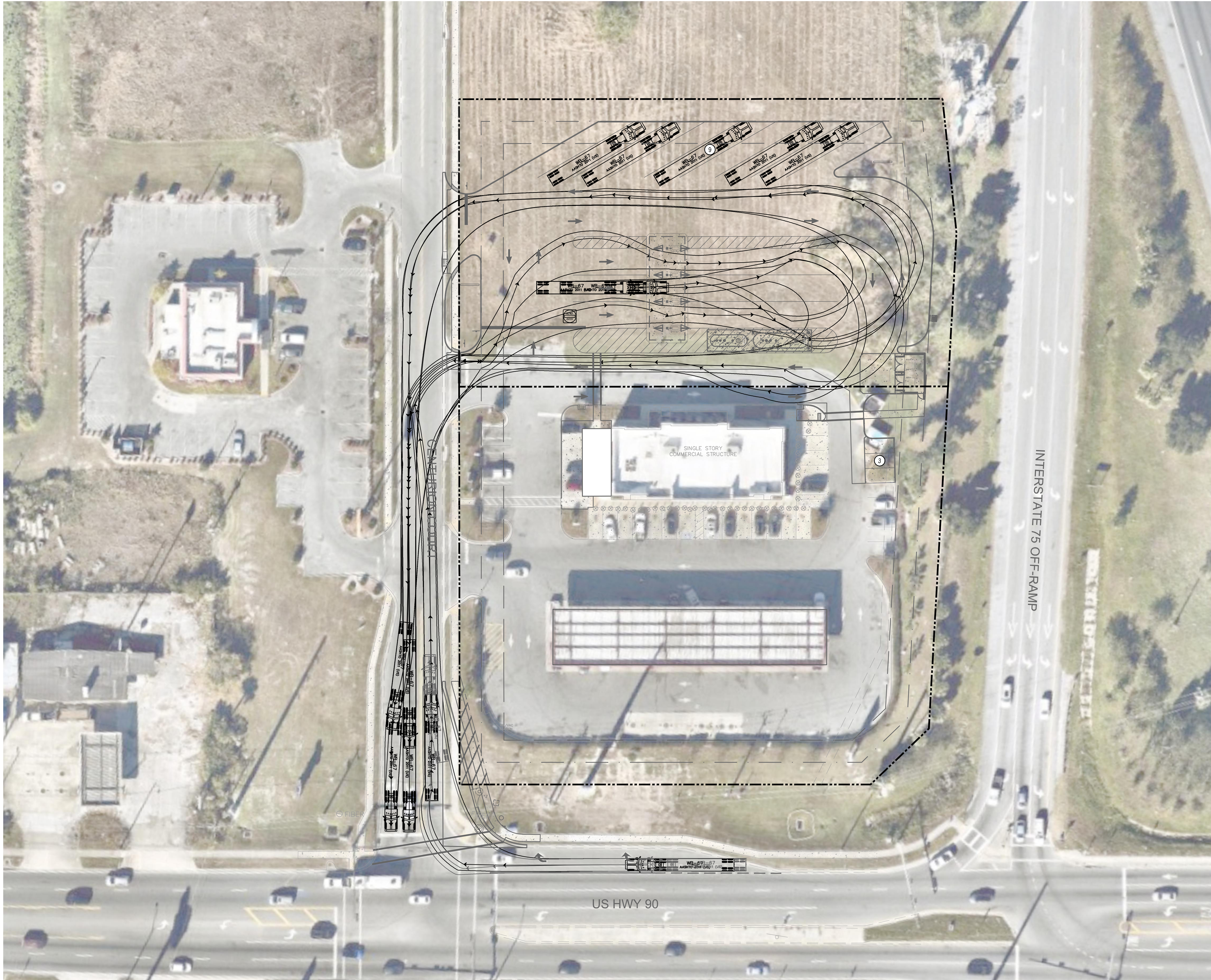
TRUCK TURNING MOVEMENTS

CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION

CITY OF LAKE CITY FLORIDA

SHEET NUMBER
C4.3

Plotted By: Parks, Ethan - Sheet Set: CIRCLE K - US HWY 30 & I-75 High Speed Diesel Fuel Expansion - Layout: C4.4 FUELING TRUCK MOVEMENTS - June 08, 2022 04:29:45pm - K:\DR\Civil\149880040-Circle K US90 & I75\CADD\CONST\PlanSheets\C4.0 - SITE PLAN.dwg
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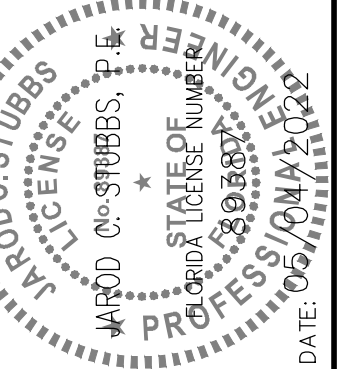


Tractor Width	: 4.00	Lock to Lock Time	: 6.0
Tractor Track	: 19.50	Steering Angle	: 28.4
Tractor Length	: 15.00	Articulating Angle	: 75.0
Trailer Length	: 53.00		
Trailer Width	: 45.50		

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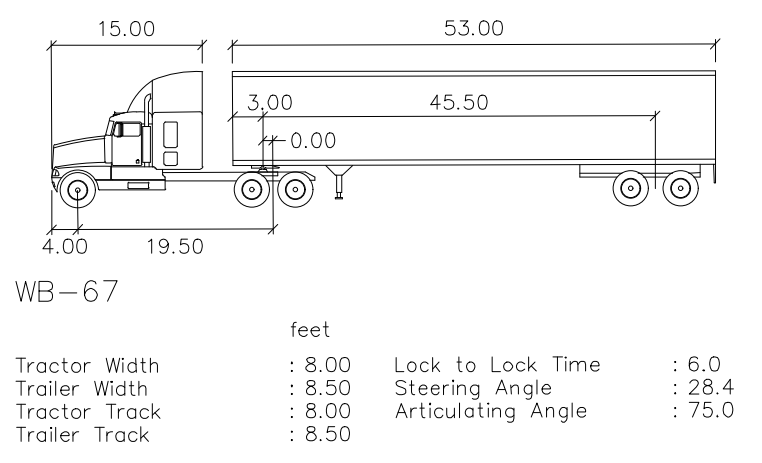
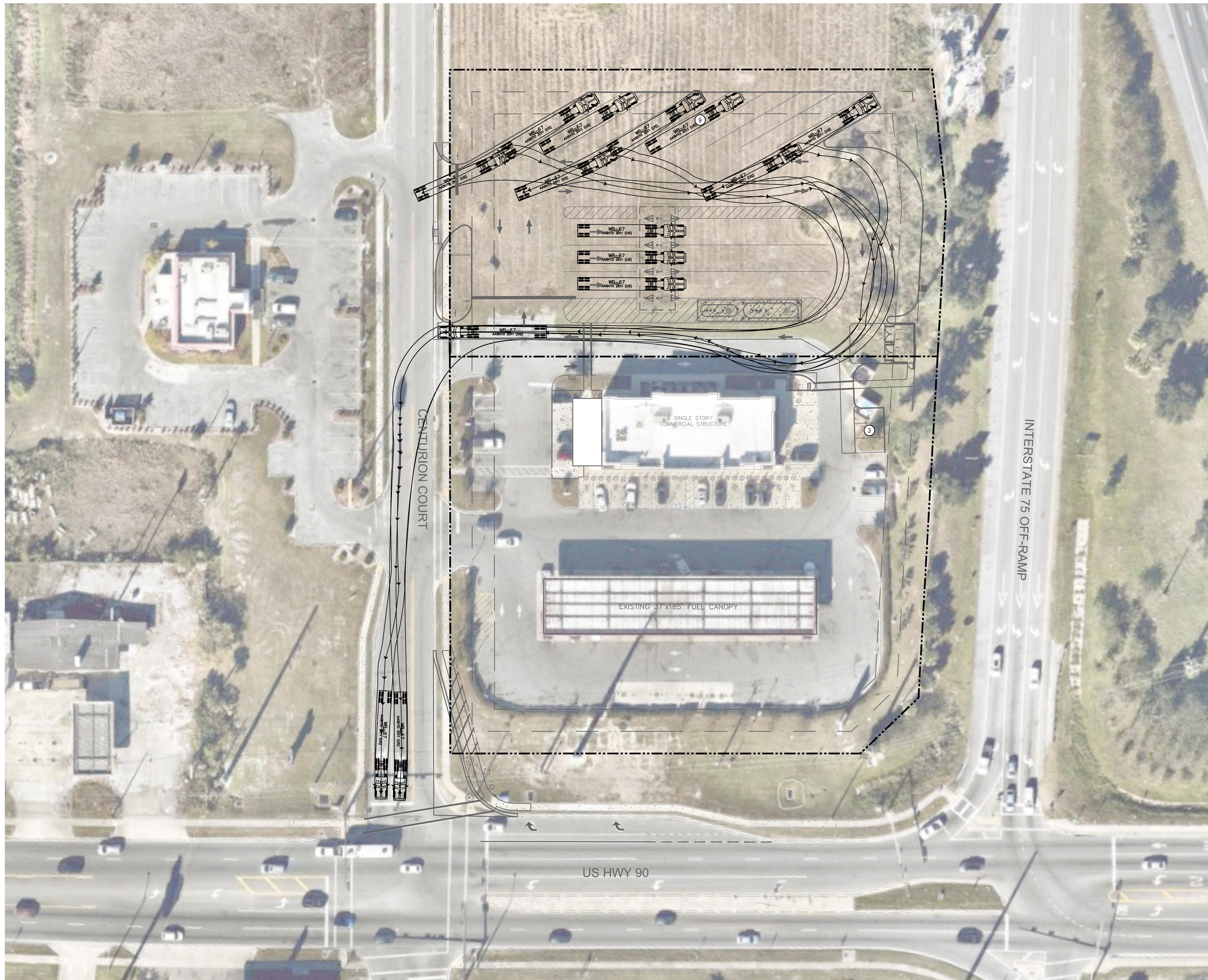
KHA PROJECT	149880040
DATE	05/04/2022
SCALE	AS SHOWN
DESIGNED BY	EJF
DRAWN BY	EJF
CHECKED BY	JCS
JCS DATE:	06/08/2022

TRUCK TURNING MOVEMENTS

CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION

SHEET NUMBER
C4.4

Plotted By: Parks, Ethan - Sheet Set: CIRCLE K - US HWY 90 & I-75 High Speed Diesel Fuel Expansion - Layout: C4.5 - PARKING TRUCK MOVEMENTS - June 05, 2022 - 04:29:47pm - K:\ORL_Civil\149880040 - Circle K - US90 & I75 CAD\CONSTR\PlanSheets\C4.0 - SITE PLAN.dwg
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Tractor Width	: 8.00	Lock to Lock Time	: 6.0
Trailer Width	: 8.50	Steering Angle	: 28.4
Tractor Track	: 8.00	Articulating Angle	: 75.0
Trailer Track	: 8.50		

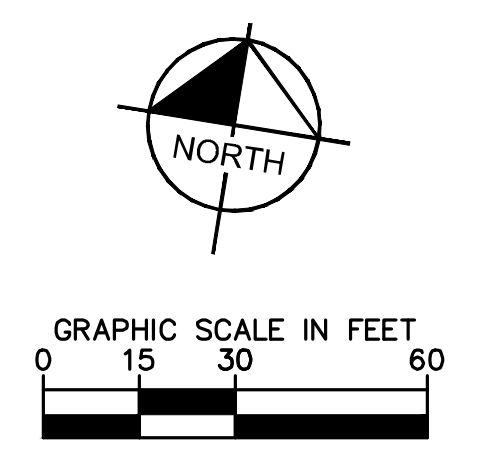
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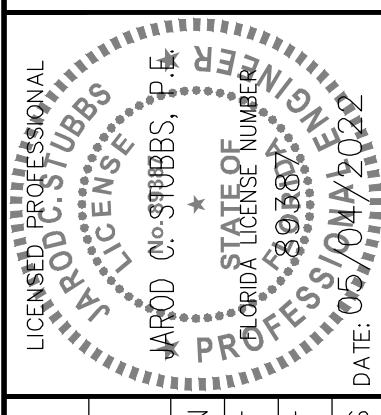
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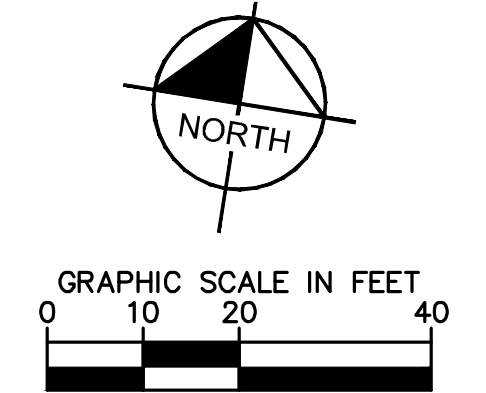
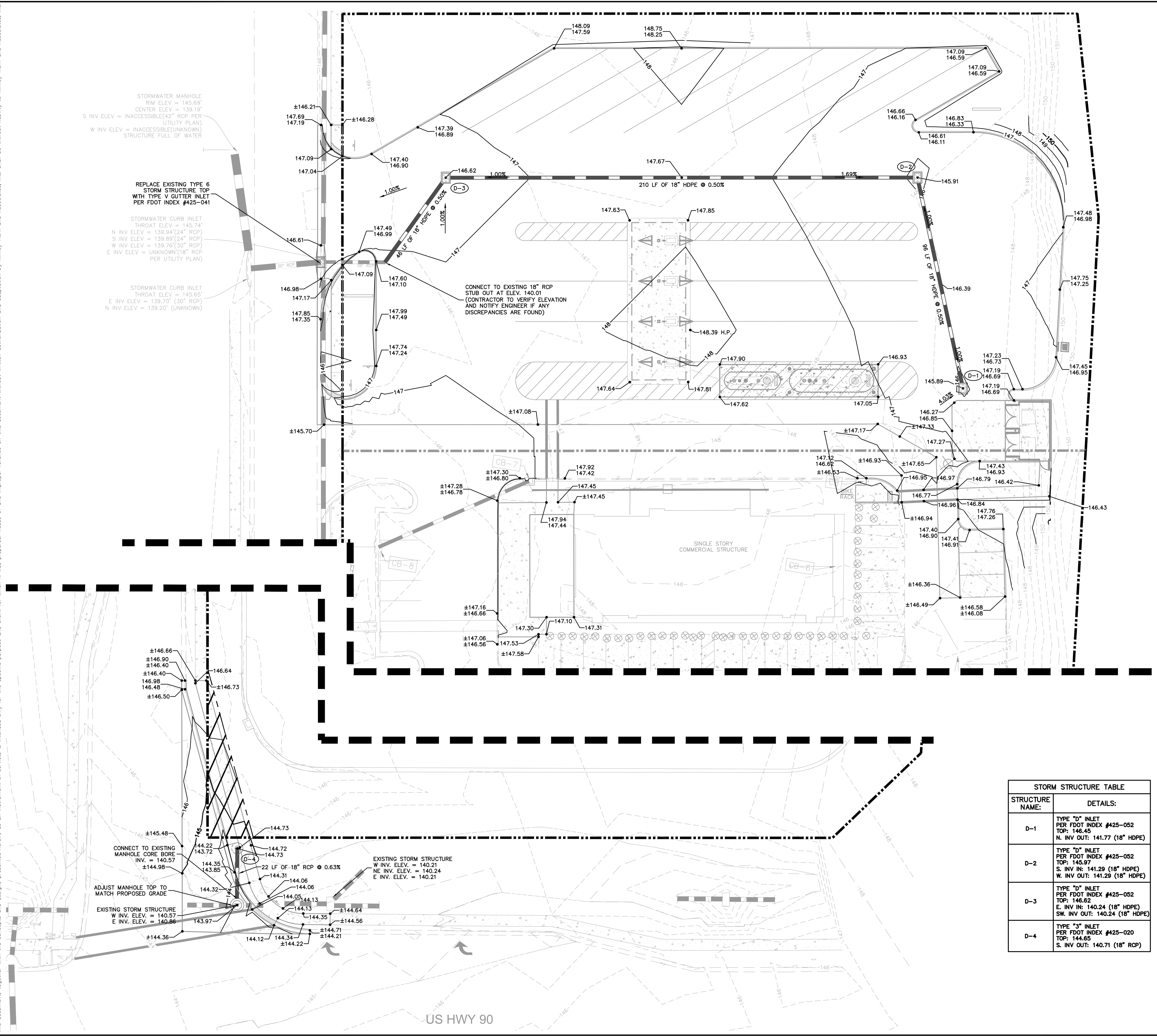


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DATE	05/04/2022
SCALE	AS SHOWN
DESIGNED BY	EJF
DRAWN BY	EJF
CHECKED BY	JCS
JCS DATE:	05/05/2022

C4.5 - TRUCK TURNING MOVEMENTS

CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION

Plotted By: Parks, Etham - Sheet Set: CIRCLE K - US HWY 90 & I-75 High-Speed Diesel Fuel Expansion - Layout: C5.0 PAVING, GRADING AND DRAINAGE PLAN - June 08, 2022, 04:30:40pm - K:\ORL\Civil\49880040-Circle K US90 & I75 CADD\CONSTR\PlanSheet\C5.0 - PCD.dwg
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- NOTES:**
- ALL STORM PIPES LABELED "HDPE" SHALL BE ADS HIGH PERFORMANCE (HP) POLYPROPYLENE STORM SEWER PIPE.
 - CURB RAMPS ALONG PUBLIC STREETS AND IN THE PUBLIC RIGHT-OF-WAY SHALL BE CONSTRUCTED IN ACCORDANCE WITH FDOT STANDARDS AND SPECIFICATIONS.
 - PRIVATE CURB RAMPS ON THE SITE (I.E. OUTSIDE PUBLIC STREET RIGHT-OF-WAY) SHALL CONFORM TO ADA STANDARDS AND SHALL HAVE A DETECTABLE WARNING SURFACE THAT IS FULL WIDTH AND FULL DEPTH OF THE CURB RAMP, NOT INCLUDING FLARES.
 - ALL ACCESSIBLE ROUTES, GENERAL SITE AND BUILDING ELEMENTS, RAMPS, CURB RAMPS, STRIPING, AND PAVEMENT MARKINGS SHALL CONFORM TO ADA STANDARDS FOR ACCESSIBLE DESIGN, LATEST EDITION.
 - BEFORE PLACING PAVEMENT, CONTRACTOR SHALL VERIFY THAT SUITABLE ACCESSIBLE PEDESTRIAN ROUTES (PER ADA AND FHA) EXIST TO AND FROM EVERY DOOR AND ALONG SIDEWALKS, ACCESSIBLE PARKING SPACES, ACCESSIBLE AISLES, AND ACCESSIBLE ROUTES. IN NO CASE SHALL AN ACCESSIBLE RAMP SLOPE EXCEED 1 VERTICAL TO 12 HORIZONTAL. IN NO CASE SHALL LONGITUDINAL CROSS SLOPE EXCEED 2.0 PERCENT. IN NO CASE SHALL LONGITUDINAL SIDEWALK SLOPE EXCEED 5.0 PERCENT. ACCESSIBLE PARKING SPACES AND ACCESS AISLES SHALL NOT EXCEED 2.0 PERCENT SLOPE IN ANY DIRECTION.
 - CONTRACTOR SHALL TAKE FIELD SLOPE MEASUREMENTS ON FINISHED SUBGRADE AND FORM BOARDS PRIOR TO PLACING PAVEMENT TO VERIFY THAT ADA SLOPE REQUIREMENTS ARE PROVIDED. CONTRACTOR SHALL CONTACT ENGINEER PRIOR TO PAVING IF ANY EXCESSIVE SLOPES ARE ENCOUNTERED. NO CONTRACTOR CHANGE ORDERS WILL BE ACCEPTED FOR ADA SLOPE COMPLIANCE ISSUES.
 - ALL PEDESTRIAN SIDEWALKS, PATHWAYS, AND CROSSWALKS SHALL BE CONSTRUCTED NOT TO EXCEED MAX. 2.0% CROSS SLOPE, MAX. 5.0% RUNNING SLOPE.
 - ALL HANDICAP ACCESSIBLE PARKING SPACES SHALL BE CONSTRUCTED NOT TO EXCEED MAX. 2.0% CROSS SLOPE IN ALL DIRECTIONS.
 - PROPOSED GRADES TO MATCH EXISTING ELEVATIONS AT PROPERTY LINE
 - CONTRACTOR TO FIELD VERIFY ELEVATIONS AT ALL EXISTING SIDEWALK AND ROAD CONNECTION POINTS WITH ENGINEER OF RECORD BEFORE CONSTRUCTION OF ANY IMPROVEMENTS.
 - FOR EROSION CONTROL NOTES REFER TO SHEET C2.0.
 - ALL DRAINAGE PIPES SHALL BE FILTER FABRIC WRAPPED PER FDOT STANDARD PLAN #430-001.
 - CONTRACTOR TO ENSURE ADEQUATE COVER REMAINS OVER ALL EXISTING UTILITIES.
 - CONTRACTOR TO VERIFY EXISTING COVER OVER ALL UTILITIES BEFORE START OF CONSTRUCTION AND TO COORDINATE WITH THE ENGINEER OF RECORD PRIOR TO START OF CONSTRUCTION IF DESIGN DOES NOT PROVIDE 36" COVER.
 - ALL EXISTING VALVES, BOXES, MANHOLE LIDS, COVERS, AND SIMILAR APPURTENANCES MUST BE ADJUSTED ACCORDINGLY TO MATCH FINISHED GRADE.
 - ALL PAVEMENT MUST BE SOURCED FROM AN FDOT APPROVED PLANT.

STORM STRUCTURE TABLE	
STRUCTURE NAME:	DETAILS:
D-1	TYPE "D" INLET PER FDOT INDEX #425-052 TOP: 146.45 N. INV. OUT: 141.77 (18" HDPE)
D-2	TYPE "D" INLET PER FDOT INDEX #425-052 TOP: 145.97 S. INV. IN: 141.29 (18" HDPE) W. INV. OUT: 141.29 (18" HDPE)
D-3	TYPE "D" INLET PER FDOT INDEX #425-052 TOP: 146.62 E. INV. IN: 140.24 (18" HDPE) SW. INV. OUT: 140.24 (18" HDPE)
D-4	TYPE "3" INLET PER FDOT INDEX #425-020 TOP: 144.65 S. INV. OUT: 140.71 (18" RCP)

LEGEND

- PROPERTY LINE
- PROPOSED STORM PIPE
- ⊙ PROPOSED STORM MANHOLE
- ⊠ PROPOSED STORM INLET
- XX.XX PROPOSED ELEV. TOP OF CURB
- XX.XX PROPOSED ELEV. BOT. OF CURB
- XX.XX PROPOSED SPOT ELEVATION
- EXISTING STORM PIPE
- ⊙ EXISTING STORM MANHOLE
- ⊠ EXISTING STORM INLET

VERTICAL DATUM:
ELEVATIONS ARE REFERENCED TO PLAT BENCHMARKS AS SHOWN ON THE FACE OF THE PLAT OF GATEWAY CROSSING REPLAT OF LOTS 2 AND 3, PLAT BOOK 9, PAGE 151

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Know what's below. Call before you dig.

SUNSHINE STATE ONE CALL OF FLORIDA, INC.

CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION

CITY OF LAKE CITY FLORIDA

SHEET NUMBER C5.0

149880040

05/04/2022

SCALE AS SHOWN

DESIGNED BY E.J.F.

DRAWN BY E.J.F.

CHECKED BY J.C.S.

Kimley»Horn

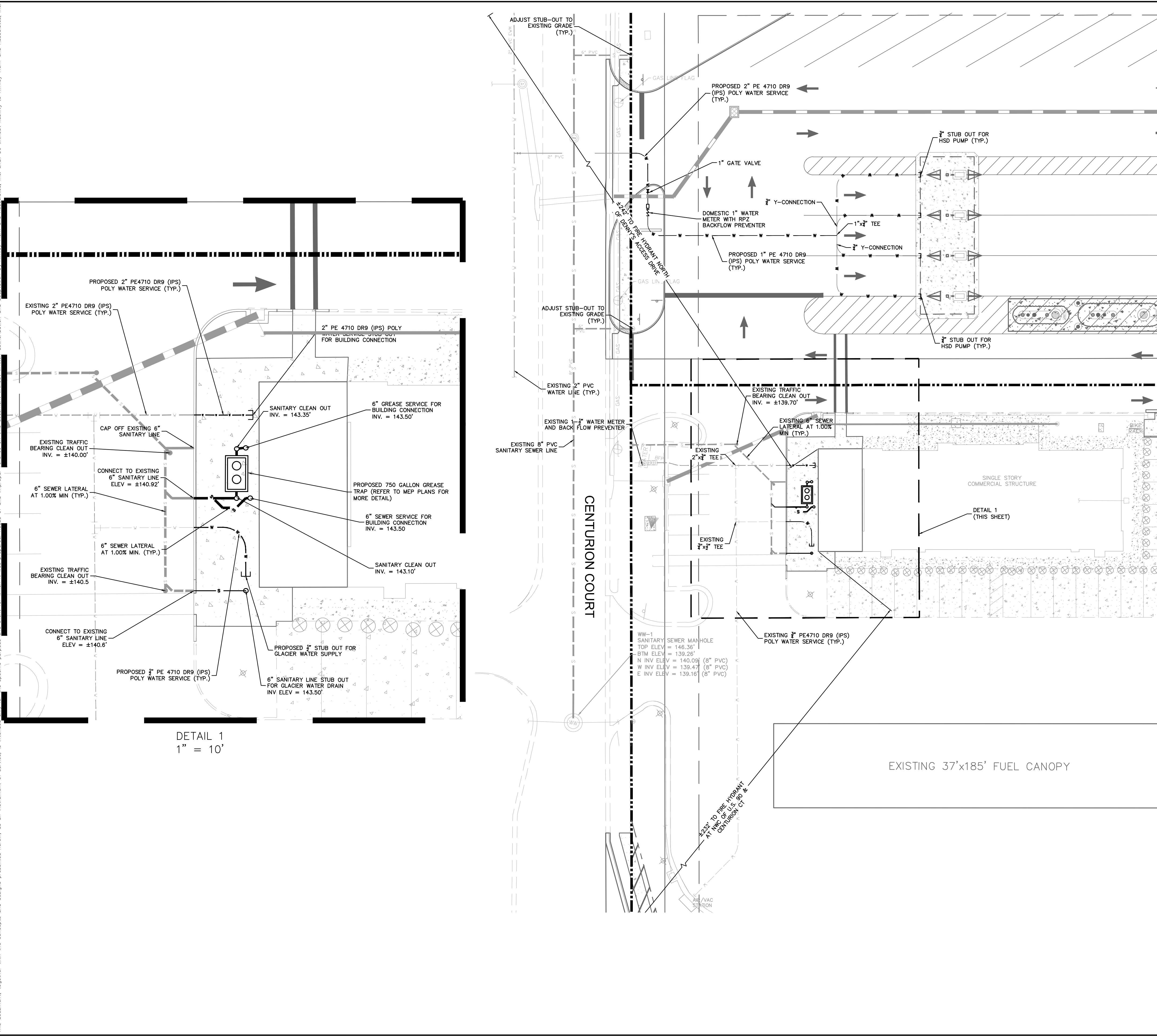
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189 S. ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801
PHONE: 407-898-1511
WWW.KIMLEY-HORN.COM REGISTRY NO. 35106

NO. _____

REVISIONS _____

DATE _____

Plotted By: Parks, Etham - Sheet Set: CIRCLE K - US HWY 30 & I-75 High-Speed Diesel Fuel Expansion - Layout: CEG UTILITY PLAN - June 08, 2022 - 04:30:24pm - K:\ORL_CAD\149880040-Circle K USRD & I-75 CAD\CONSTR\PlanSheets\C6.0 - UTILITIES PLAN.dwg
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- NOTES:**
- ALL ONSITE UTILITIES SHALL BE PRIVATELY OWNED AND MAINTAINED.
 - MAINTAIN A MINIMUM OF 3-FT OF COVER OVER ALL PROPOSED WATER LINES AND 4-FT MINIMUM OF COVER FOR WASTEWATER LINES.
 - REFER TO ADDITIONAL UTILITY NOTES AND DETAILS ON SHEETS C1.0 AND C7.0.
 - ALL WATER MAINS, AND DOMESTIC LINES SHALL BE CONSTRUCTED IN ACCORDANCE WITH CITY OF LAKE CITY UTILITY STANDARDS.
 - BEDDING AND BACKFILL SHALL BE INSTALLED PER CITY OF LAKE CITY. GRANULAR BACKFILL IS REQUIRED UNDER PAVEMENT AND WITHIN 5 FEET OF PAVEMENT.
 - ALL SANITARY SEWER LINES SHALL BE GREEN C-900 PVC MEETING, ASTM D-3034 SDR 26.
 - CONTRACTOR SHALL COORDINATE ANY DISRUPTIONS TO EXISTING UTILITY SERVICES WITH ADJACENT PROPERTY OWNERS.
 - ALL ELECTRIC AND TELEPHONE EXTENSIONS INCLUDING SERVICE LINES SHALL BE CONSTRUCTED TO THE APPROPRIATE UTILITY COMPANY SPECIFICATIONS. ALL UTILITY DISCONNECTIONS SHALL BE COORDINATED WITH THE DESIGNATED UTILITY COMPANIES.
 - CONTRACTOR TO CALL "SUNSHINE STATE ONE CALL OF FLORIDA" (1-800-432-4770) TO COORDINATE FIELD LOCATIONS OF EXISTING UNDERGROUND UTILITIES BEFORE ORDERING MATERIALS OR COMMENCING CONSTRUCTION. NOTIFY ENGINEER OF ANY DISCREPANCIES IMMEDIATELY.
 - PRIOR TO THE CONSTRUCTION OF OR CONNECTION TO ANY STORM DRAIN, SANITARY SEWER, WATER MAIN OR ANY OTHER UTILITIES, THE CONTRACTOR SHALL EXCAVATE, VERIFY AND CALCULATE ALL POINTS OF CONNECTION AND ALL UTILITY CROSSINGS AND INFORM THE ENGINEER AND THE OWNER/ DEVELOPER OF ANY CONFLICT OR REQUIRED DEVIATIONS FROM THE PLAN. NOTIFICATION SHALL BE MADE A MINIMUM OF 72 HOURS PRIOR TO CONSTRUCTION. THE ENGINEER AND ITS CLIENTS SHALL BE HELD HARMLESS IN THE EVENT THAT THE CONTRACTOR FAILS TO MAKE SUCH NOTIFICATION. CITY OF LAKE CITY SHALL BE NOTIFIED OF ANY AND ALL CHANGES TO THE DESIGN PLANS.
 - CONTRACTOR SHALL COMPLY COMPLETELY WITH THE LATEST STANDARDS OF OSHA DIRECTIVES OR ANY OTHER AGENCY HAVING JURISDICTION FOR EXCAVATION AND TRENCHING PROCEDURES. THE CONTRACTOR SHALL USE SUPPORT SYSTEMS, SLOPING, BENCHING AND OTHER MEANS OF PROTECTION. THIS IS TO INCLUDE, BUT NOT LIMITED FOR ACCESS AND EGRESS FROM ALL EXCAVATION AND TRENCHING. CONTRACTOR IS RESPONSIBLE FOR COMPLYING WITH PERFORMANCE CRITERIA AS REQUIRED BY OSHA.
 - CONTRACTOR TO AVOID DISRUPTION OF ANY ADJACENT TENANT'S TRAFFIC OPERATIONS, TO THE MAXIMUM EXTENT, DURING INSTALLATION OF UTILITIES. IF TRAFFIC IS TO BE OBSTRUCTED CONTRACTOR SHALL COORDINATE TRAFFIC CONTROL PLAN WITH THE LOCAL MUNICIPALITY.
 - ALL DIMENSIONS ARE TO CENTERLINE OF PIPE OR CENTER OF MANHOLE UNLESS NOTED OTHERWISE.
 - SEE PLUMBING PLANS FOR EXACT UTILITY CONNECTION LOCATIONS AT BUILDING.
 - LIGHT POLES SHOWN FOR COORDINATION PURPOSES ONLY AND DO NOT REPRESENT ACTUAL SIZE. SEE SITE LIGHTING PLANS BY OTHERS FOR MORE INFORMATION.
 - ELECTRIC, TELEPHONE, AND OTHER DRY UTILITIES SHALL BE PLACED WITHIN CONDUIT, MEETING PRIVATE UTILITY STANDARDS, WHEN ROUTE CROSSES PAVED DRIVES AND PARKING AREAS.
 - ALL UNDERGROUND WATER MAINS AND HYDRANTS SHALL BE INSTALLED, COMPLETED AND IN SERVICE PRIOR TO ANY COMBUSTIBLES BEING BROUGHT ONSITE.
 - CONTRACTOR TO ENSURE ADEQUATE COVER REMAINS OVER ALL EXISTING UTILITIES.
 - CONTRACTOR TO VERIFY EXISTING COVER OVER ALL UTILITIES BEFORE START OF CONSTRUCTION AND TO COORDINATE WITH THE ENGINEER OF RECORD PRIOR TO START OF CONSTRUCTION IF DESIGN DOES NOT PROVIDE 36" COVER.
 - ALL ELECTRIC, CABLE, AND TELECOMMUNICATION UTILITIES FOR BUILDING SERVICE TO BE INSTALLED UNDERGROUND.

LEGEND

	PROPERTY BOUNDARY
	PROPOSED STORM PIPE
	PROPOSED SANITARY SEWER LINE
	EXISTING WATER LINE
	EXISTING SANITARY SEWER LINE
	EXISTING WATER LINE
	EXISTING STORM PIPE
	EXISTING GAS LINE
	EXISTING OVERHEAD ELECTRIC LINE
	EXISTING FORCE MAIN
	EXISTING STORM MANHOLE
	EXISTING SANITARY SEWER MANHOLE
	PROPOSED FIRE HYDRANT

VERTICAL DATUM:
 ELEVATIONS ARE REFERENCED TO PLAT BENCHMARKS AS SHOWN ON THE FACE OF THE PLAT OF GATEWAY CROSSING REPLAT OF LOTS 2 AND 3, PLAT BOOK 9, PAGE 151

CALL 48 HOURS BEFORE YOU DIG

811

IT'S THE LAW!
 DIAL 811

Know what's below.
 Call before you dig.

SUNSHINE STATE ONE CALL OF FLORIDA, INC.

UTILITY PLAN

CIRCLE K - US HWY 30 & I-75 FUEL EXPANSION

CITY OF LAKE CITY FLORIDA

SHEET NUMBER
C6.0

DATE
05/04/2022

DESIGNED BY
EJF

DRAWN BY
EJF

CHECKED BY
JCS

REVISIONS

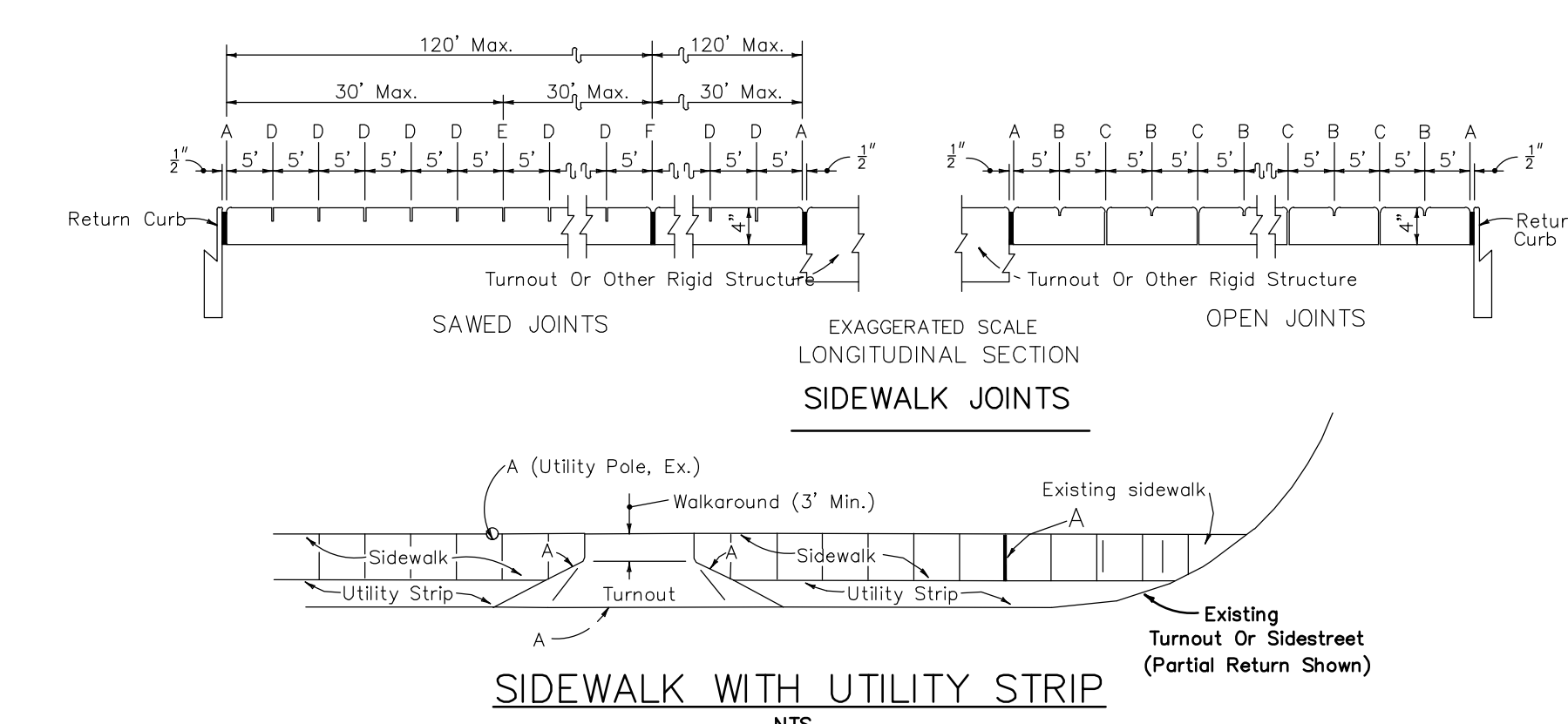
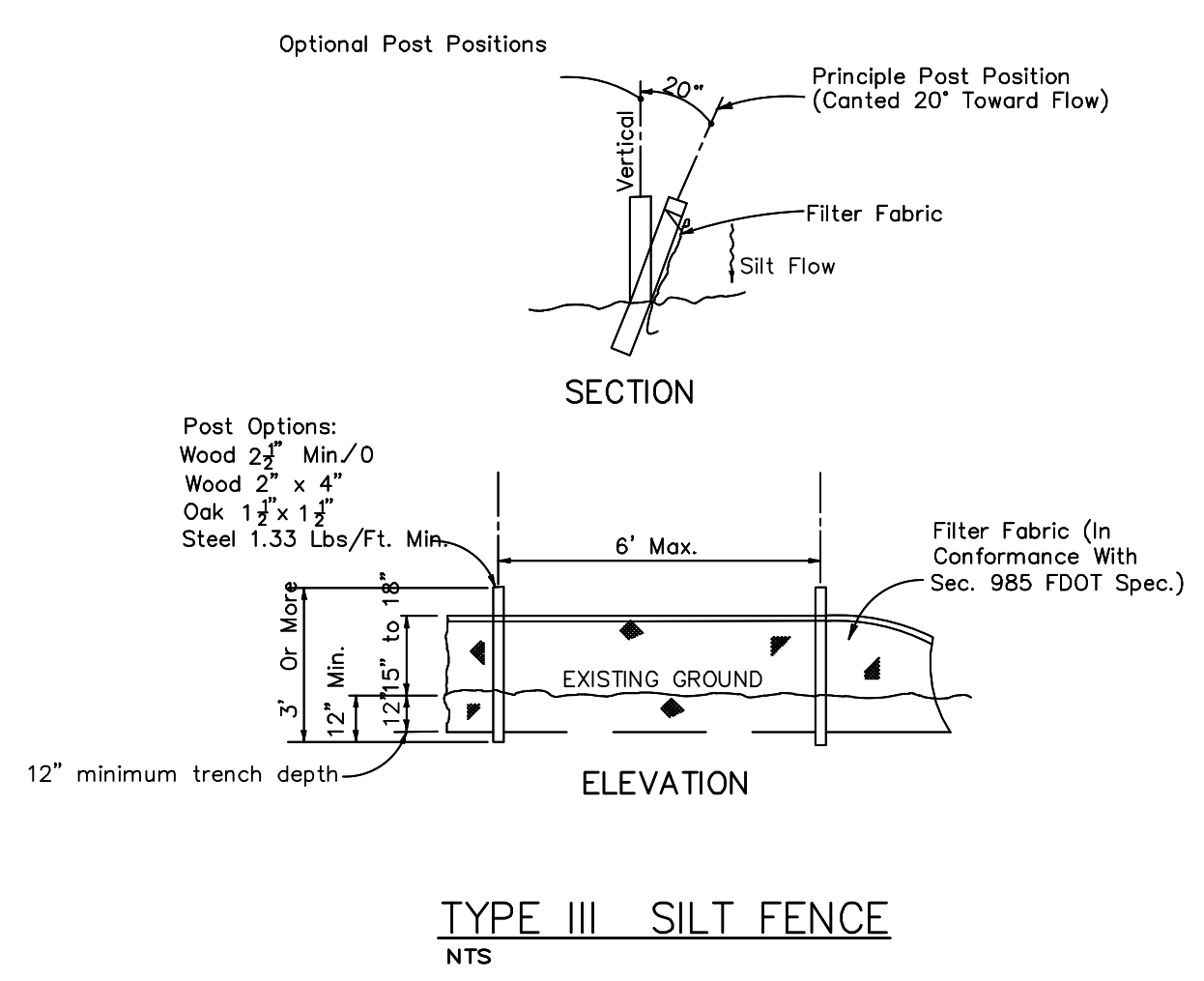
DATE

BY

Kimley-Horn

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JOINT LEGEND

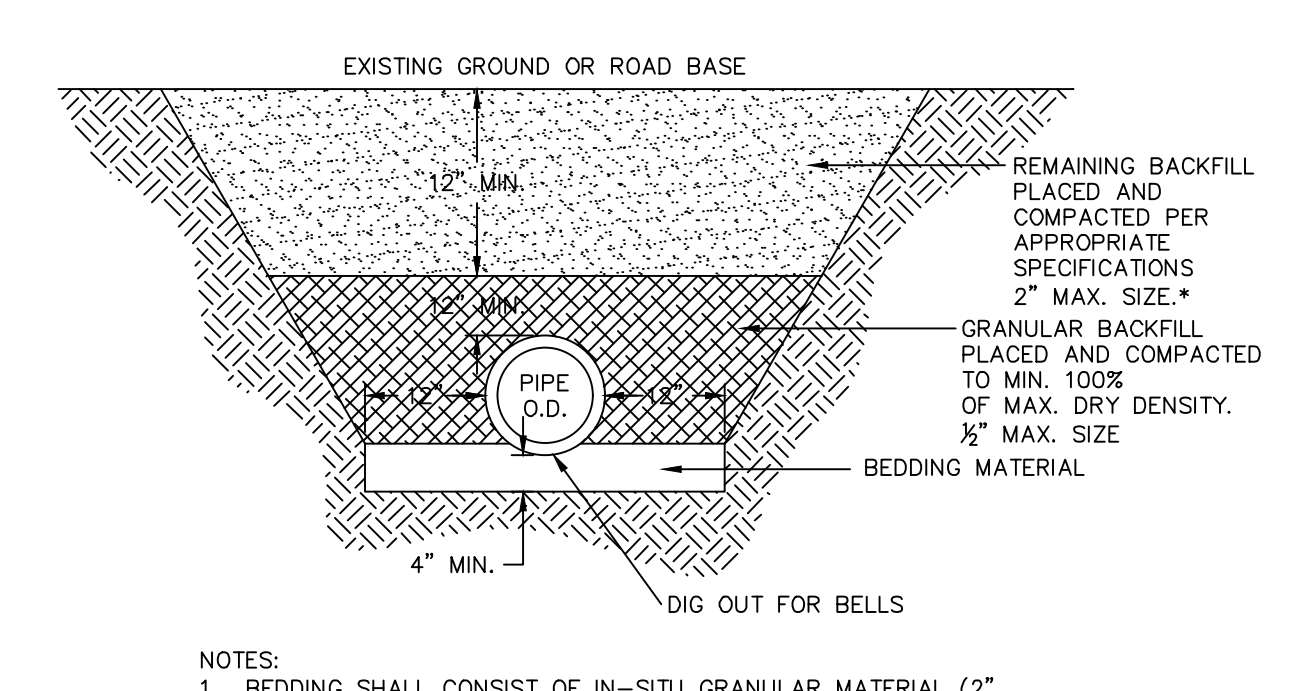
A - 1/2" EXPANSION JOINTS (PERFORMED JOINT FILLER)
 B - 1/2" DUMMY JOINTS, TOOLED
 C - 1/2" FORMED OPEN JOINTS
 D - 1/2" SAW CUT JOINTS, 1/4" DEEP (96 HOUR) MAX. 5' CENTERS
 E - 1/2" SAW CUT JOINTS, 1/4" DEEP (12 HOUR) MAX. 30' CENTERS
 F - 1/2" EXPANSION JOINT WHEN RUN OF SIDEWALK EXCEEDS 120'
 G - COLD JOINT WITH BOND BREAKER, TOOLED

FOOTNOTES:

ALL 1/2" JOINTS SHALL BE CONSTRUCTED WITH PREFORMED JOINT FILLER.
 * 1/8" OPEN JOINTS PLACED AT EQUAL (20' MAX.) INTERVALS FOR DRIVEWAYS OVER 20' WIDE. JOINTS IN CURB AND GUTTER TO MATCH JOINTS IN DRIVEWAYS.
 Ø DRIVEWAYS (6" CONCRETE) SHALL BE OF A UNIFORM WIDTH (W) AS SHOWN ON SHT. P-1.

GENERAL NOTES

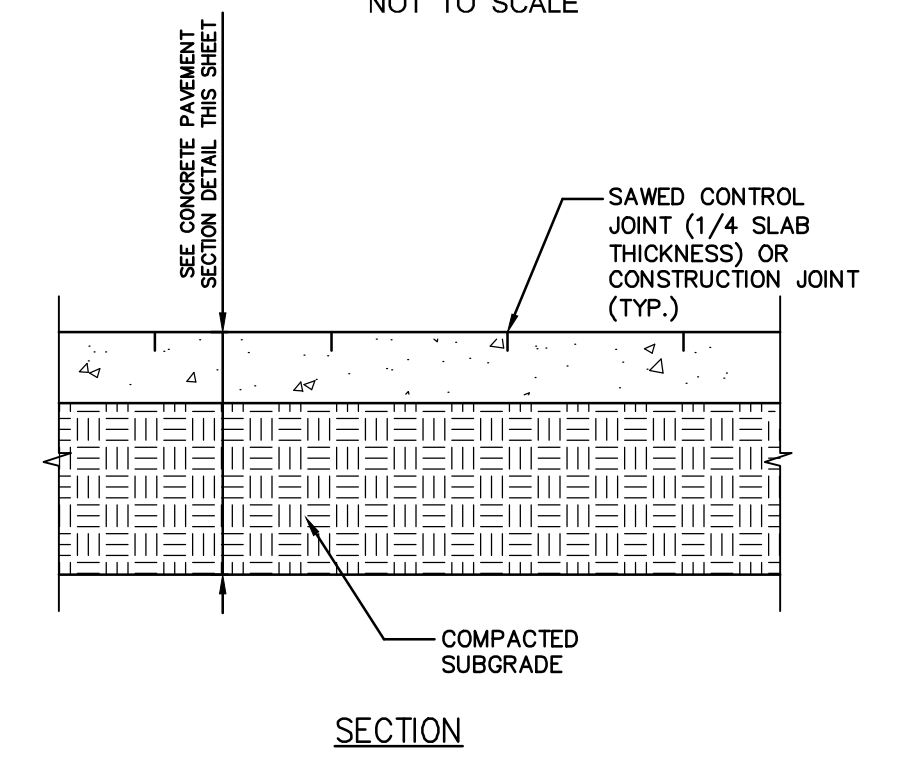
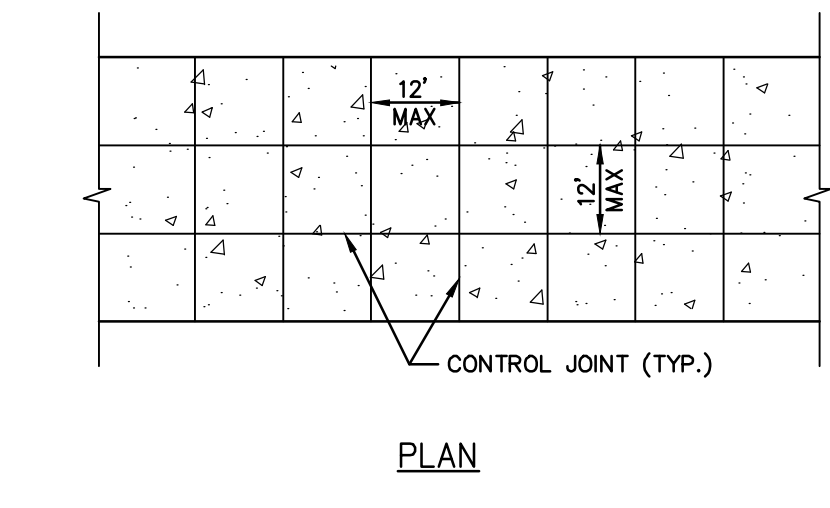
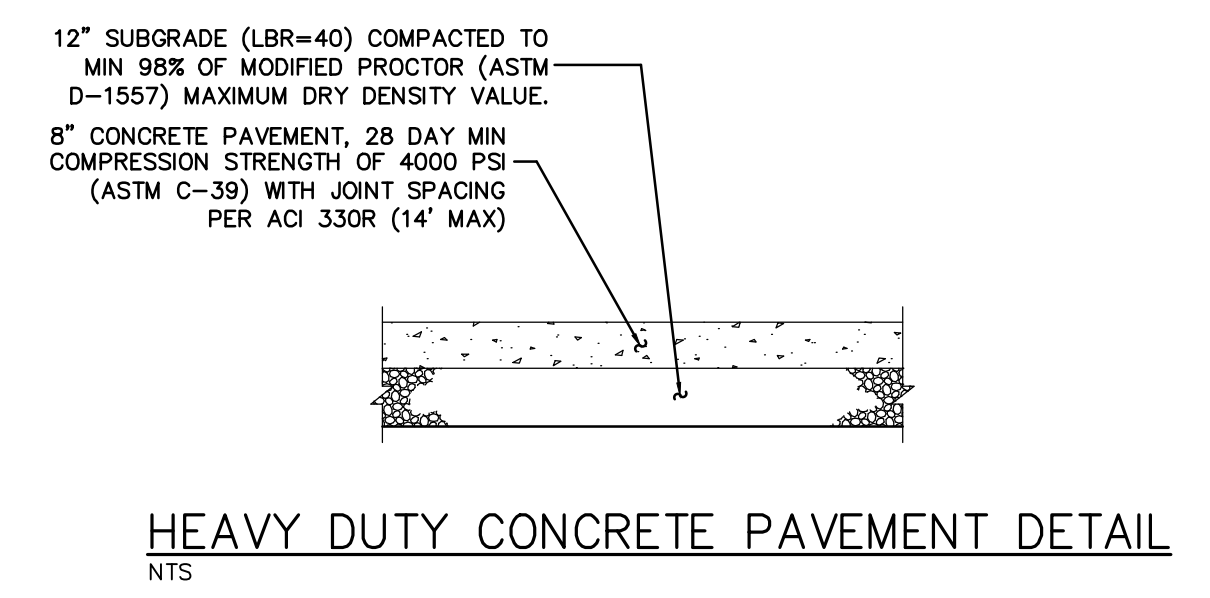
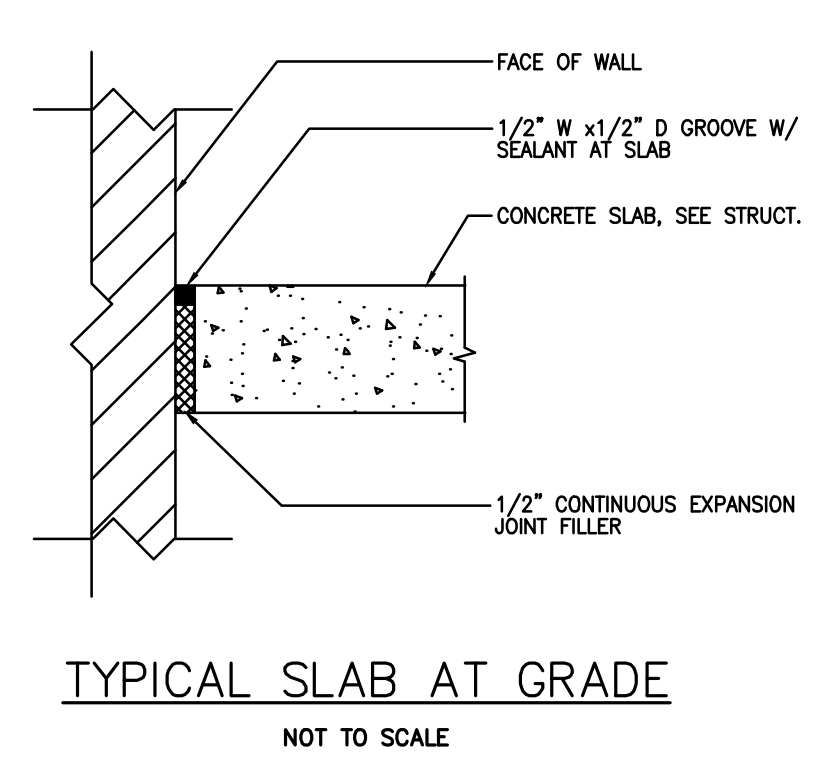
SIDEWALKS SHALL BE CONSTRUCTED IN ACCORDANCE WITH FDOT STANDARD SPECIFICATION NO. 522 EXCEPT FOR CURB CUT RAMP RUNS WHICH SHALL BE FINISHED IN ACCORDANCE WITH FDOT INDEX 522-002.
 BOND BREAKER MATERIAL CAN BE ANY IMPERMEABLE COATED OR SHEET MEMBRANE OR PREFORMED MATERIAL HAVING A THICKNESS OF NOT LESS THAN 6 MILS NOR MORE THAN 1/2".



NOTES:

- BEDDING SHALL CONSIST OF IN-SITU GRANULAR MATERIAL (2" MAX. SIZE). UNSUITABLE IN-SITU MATERIALS SUCH AS MUCK, DEBRIS AND LARGER ROCK SHALL BE REMOVED.
- THE PIPE SHALL BE FULLY SUPPORTED FOR ITS ENTIRE LENGTH WITH APPROPRIATE COMPACTION UNDER THE PIPE HAUNCHES.
- THE PIPE SHALL BE PLACED IN A DRY TRENCH.
- BACKFILL SHALL BE FREE OF UNSUITABLE MATERIAL SUCH AS LARGER ROCK, MUCK AND DEBRIS.
- ALL WORK SHALL CONFORM TO STATE OF FLORIDA TRENCH SAFETY ACT.

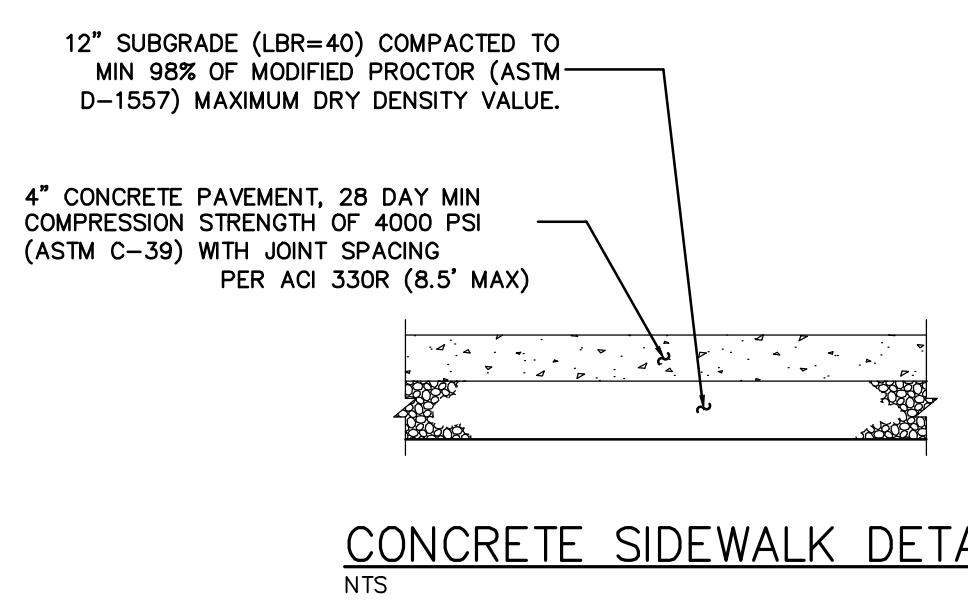
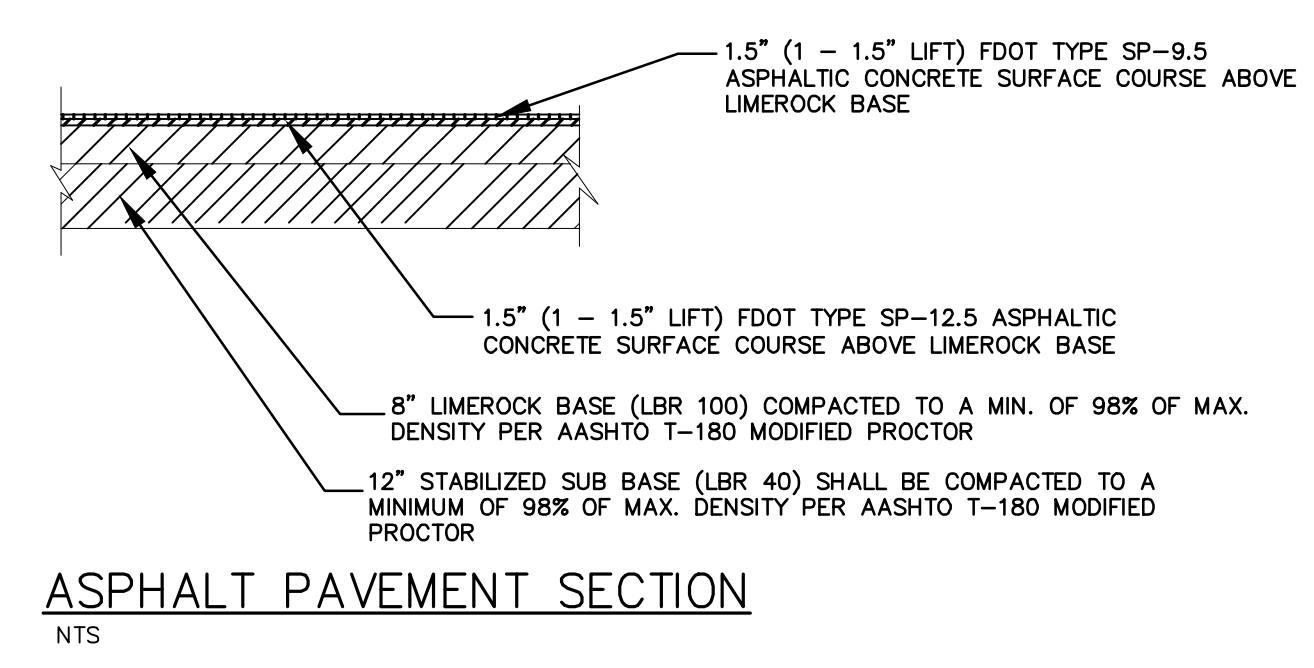
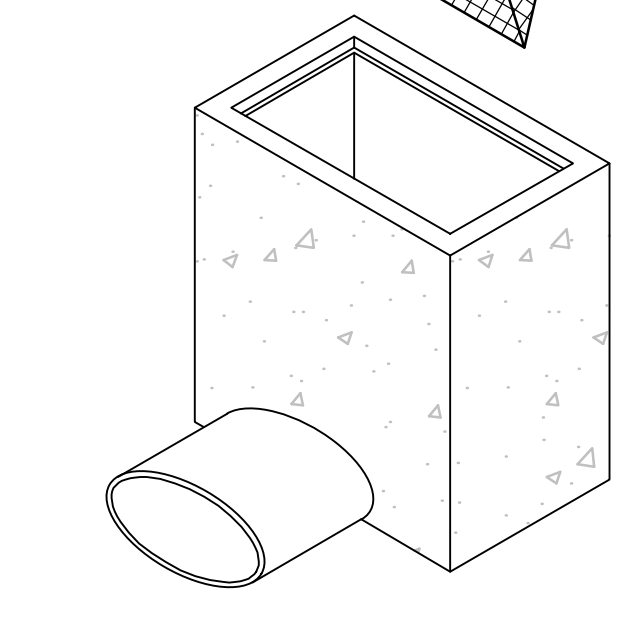
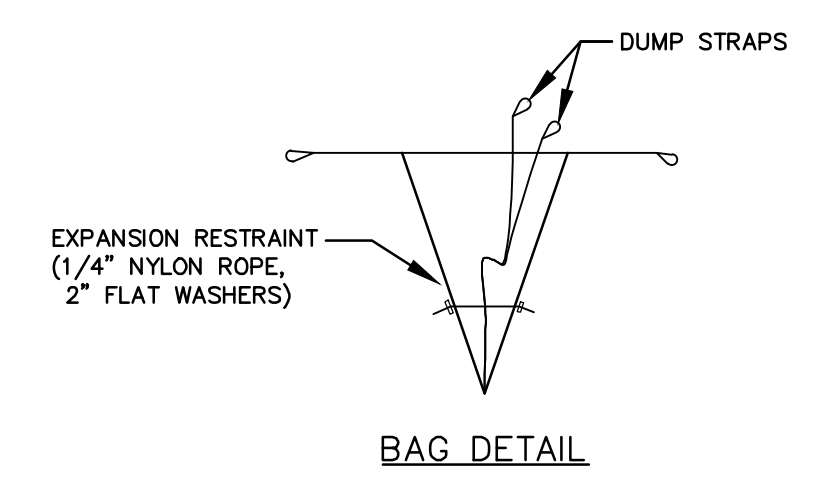
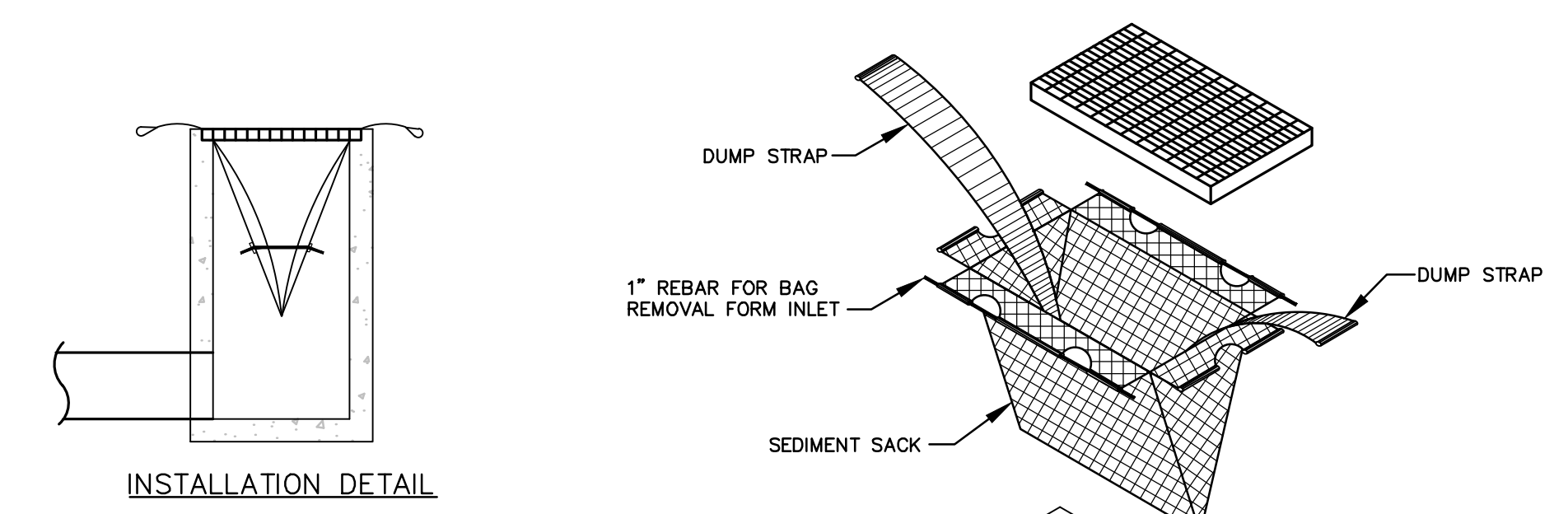
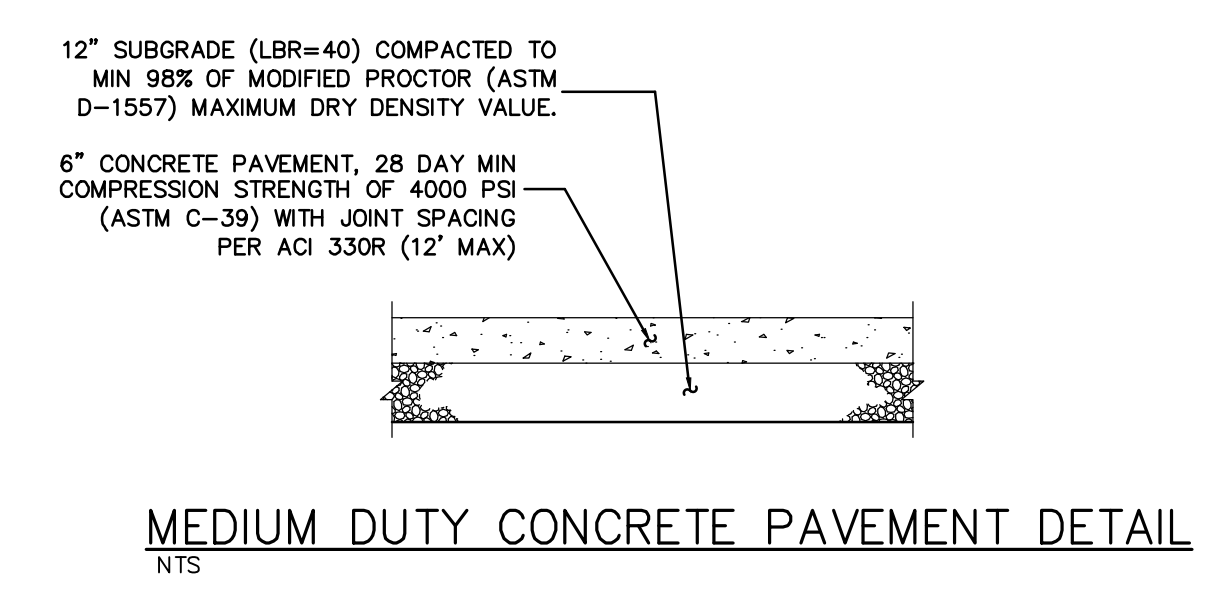
* COMPACT TO 98% OF AASHTO T-180 MAXIMUM DRY DENSITY UNDER EXISTING AND PROPOSED PAVEMENT AREAS. COMPACT TO 95% ON ALL OTHER AREAS UNLESS OTHERWISE NOTED..



NOTES:

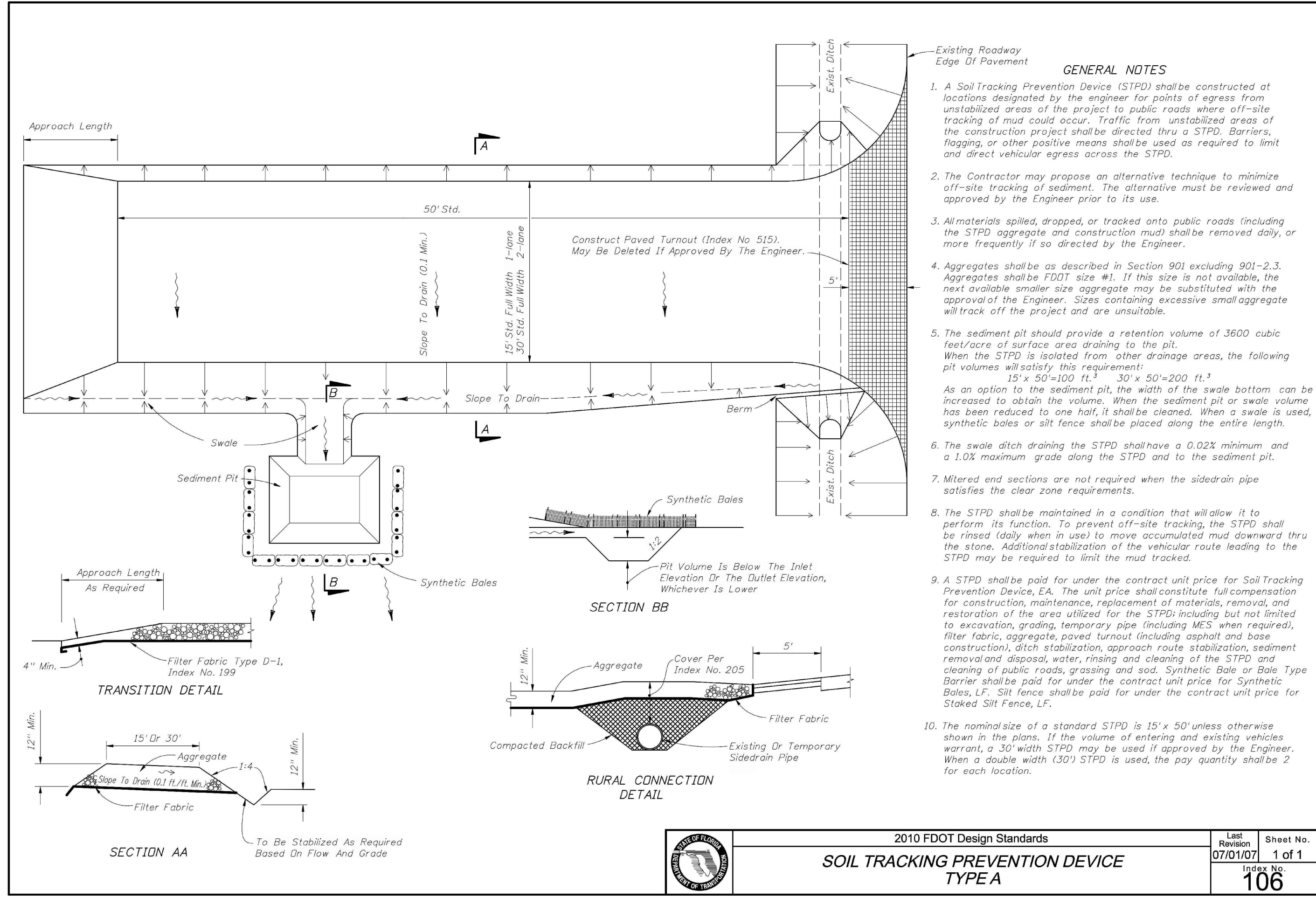
- LONGITUDINAL AND TRANSVERSE JOINTS SHALL BE SAWED AS INDICATED.
- THE JOINT SEAL MANUFACTURER'S SPECIFICATIONS SHALL BE COMPATIBLE WITH THE JOINT CONFIGURATION. PROVIDE TYPICAL DETAILS FOR CONSTRUCTION AND EXPANSION JOINTS.
- FOR ALL JOINTS THE BACKER ROD MATERIAL SHALL BE COMPATIBLE WITH THE COLD POURED SEALANT AND SLIGHTLY OVERSIZED TO PREVENT MOVEMENT DURING THE JOINT SEALANT OPERATION.
- THE WIDTH OF ALL JOINTS SHALL BE CORRECTED FOR 68 DEGREE (F).
- EXPANSION JOINTS SHALL BE USED AT ALL LOCATIONS WHERE PAVEMENT ABUTS A PERMANENT STRUCTURE.
- DOWELS AND TIE BARS FOR CONSTRUCTION JOINTS SHALL BE CAST IN PLACE.

CONCRETE JOINTING AND REINFORCING DETAIL
NOT TO SCALE

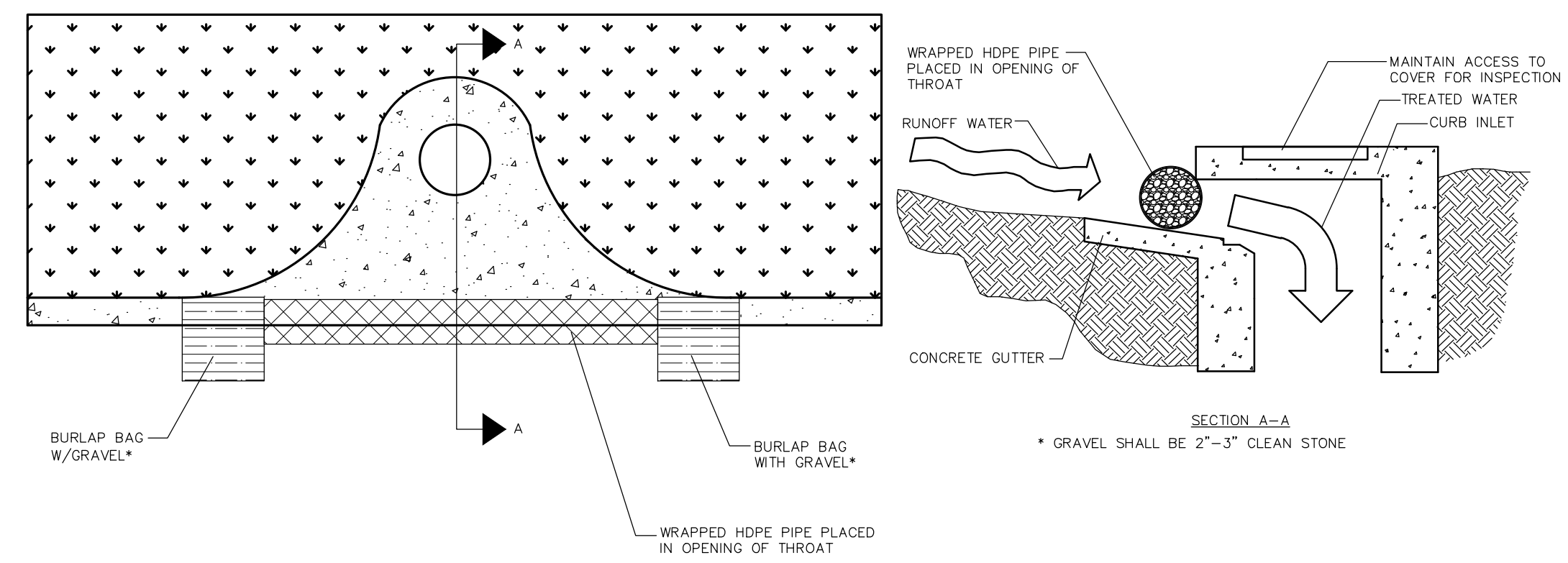


KHA PROJECT 148880040		DATE 05/04/2022		SCALE AS SHOWN		DRAWN BY EJF		CHECKED BY JCS	
KIMLEY-HORN AND ASSOCIATES, INC. 189 S. ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801 PHONE: 407-898-1511 WWW.KIMLEY-HORN.COM		DESIGNED BY EJF		DRAWN BY EJF		CHECKED BY JCS		DATE	
GENERAL CONSTRUCTION DETAILS									
CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION									
CITY OF LAKE CITY, FLORIDA									
SHEET NUMBER C7.0									

Plotted By: Parks, Ethan - Sheet Set: CIRCLE K - US HWY 30 & I-75 High Speed Diesel Fuel Expansion - Layout: C7.1 GENERAL CONSTRUCTION DETAILS - June 08, 2022 04:30:44pm - K:\VRL\Civil\149880040-Circle K USRD & I-75\GADD\CONST\PlanSheets\C7.0 - DETAILS.dwg
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- ### GENERAL NOTES
- A Soil Tracking Prevention Device (STPD) shall be constructed at locations designated by the engineer for points of egress from unstabilized areas of the project to public roads where off-site tracking of mud could occur. Traffic from unstabilized areas of the construction project shall be directed thru a STPD. Barriers, flagging, or other positive means shall be used as required to limit and direct vehicular egress across the STPD.
 - The Contractor may propose an alternative technique to minimize off-site tracking of sediment. The alternative must be reviewed and approved by the Engineer prior to its use.
 - All materials spilled, dropped, or tracked onto public roads (including the STPD aggregate and construction mud) shall be removed daily, or more frequently if so directed by the Engineer.
 - Aggregates shall be as described in Section 901 excluding 901-2.3. Aggregates shall be FDOT size #1. If this size is not available, the next available smaller size aggregate may be substituted with the approval of the Engineer. Sizes containing excessive small aggregate will track off the project and are unsuitable.
 - The sediment pit should provide a retention volume of 3600 cubic feet/cure of surface area draining to the pit.
 When the STPD is isolated from other drainage areas, the following pit volumes will satisfy this requirement:
 15' x 50' = 100 ft.³ 30' x 50' = 200 ft.³
 As an option to the sediment pit, the width of the swale bottom can be increased to obtain the volume. When the sediment pit or swale volume has been reduced to one half, it shall be cleaned. When a swale is used, synthetic bales or silt fence shall be placed along the entire length.
 - The swale ditch draining the STPD shall have a 0.02% minimum and a 1.0% maximum grade along the STPD and to the sediment pit.
 - Mitered end sections are not required when the side drain pipe satisfies the clear zone requirements.
 - The STPD shall be maintained in a condition that will allow it to perform its function. To prevent off-site tracking, the STPD shall be rinsed (daily when in use) to move accumulated mud downward thru the stone. Additional stabilization of the vehicular route leading to the STPD may be required to limit the mud tracked.
 - A STPD shall be paid for under the contract unit price for Soil Tracking Prevention Device, EA. The unit price shall constitute full compensation for construction, maintenance, replacement of materials, removal, and restoration of the area utilized for the STPD including but not limited to excavation, grading, temporary pipe (including MES when required), filter fabric, aggregate, paved turnout (including asphalt and base construction), ditch stabilization, approach route stabilization, sediment removal and disposal, water, rinsing and cleaning of the STPD, and cleaning of public roads, grassing and sod. Synthetic Bale or Bale Type Barrier shall be paid for under the contract unit price for Synthetic Bales, LF. Silt fence shall be paid for under the contract unit price for Staked Silt Fence, LF.
 - The nominal size of a standard STPD is 15' x 50' unless otherwise shown in the plans. If the volume of entering and existing vehicles warrant, a 30' width STPD may be used if approved by the Engineer. When a double width (30') STPD is used, the pay quantity shall be 2 for each location.



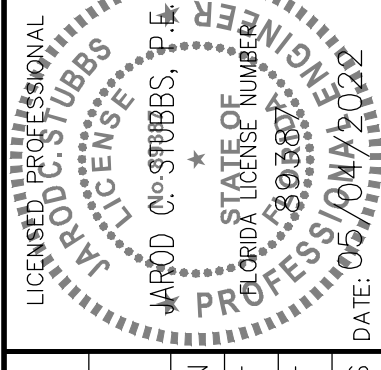
SOCK DRAIN INLET SEDIMENT FILTER

NTS
 NOTE: THE PERFORATED PIPE MUST EXTEND AT LEAST 1' BEYOND THE CURB OPENING ON EACH SIDE AND BE ANCHORED WITH GRAVEL BAGS, OR SIMILAR, ON EACH END. A SPACER MUST BE PROVIDED FOR BETWEEN THE INLET OPENING AND THE PIPE TO ALLOW FOR OVERFLOW, PREVENT FLOODING AND TO PREVENT THE PIPE FROM FALLING INTO THE INLET.

2010 FDOT Design Standards		Sheet No.
SOIL TRACKING PREVENTION DEVICE TYPE A		1 of 1
Index No.		106

NO.		REVISIONS	DATE

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 PHONE: 407-898-1511
 WWW.KIMLEY-HORN.COM REGISTRY No. 35106



KHA PROJECT	149880040
DATE	05/04/2022
SCALE	AS SHOWN
DESIGNED BY	EJF
DRAWN BY	EJF
CHECKED BY	JCS

GENERAL CONSTRUCTION DETAILS

CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION
 CITY OF LAKE CITY FLORIDA

SHEET NUMBER	C7.1
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SHEET NUMBER
C8.0

**CIRCLE K - US HWY
90 & I-75 FUEL
EXPANSION**
CITY OF LAKE CITY FLORIDA

**CITY OF LAKE CITY
STANDARD DETAILS**

KHA PROJECT	149880040
DATE	05/04/2022
SCALE	AS SHOWN
DESIGNED BY	EJF
DRAWN BY	EJF
CHECKED BY	JCS
DATE:	

LICENSED PROFESSIONAL

Kimley»»Horn
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PHONE: 407-898-1511
WWW.KIMLEY-HORN.COM REGISTRY No. 35106



CITY OF LAKE CITY

**APPLICATION FOR
NEW DEVELOPMENT AND
SITE REVIEW COMMITTEE MEETING**

FOR OFFICIAL USE ONLY

MEETING DATE: _____

MEETING TIME: _____

APPLICANT INFORMATION

Date: 09/01/2021

Name: Jarod C. Stubbs, P.E.
Address: 189 S Orange Ave., Ste 1000, Orlando, FL
Phone: (407) 409-7002
Email: jarod.stubbs@kimley-horn.com

Business Name:

Kimley-Horn and Associates

Business Address:

189 South Orange Ave., Suite 1000
Orlando, FL 32801

SUBJECT PROPERTY INFORMATION

Address: NE corner of US Hwy 90 and NW Centurion Ct (behind the Circle K)
Parcel ID#: 35-3s-16-02524-102; and 35-3s-16-02524-111
Existing Use: Vacant Commercial
Zoning District: CHI Commercial Highway Intensive

Property Owner :

GWC Development Partners, LLC

Owner Address:

2682 NW Noegel Rd
Lake City, FL 32055

DESCRIPTION OF REQUEST (may be attached, separately)

PLEASE PROVIDE AS MUCH DETAIL AS POSSIBLE SO THAT STAFF CAN BE PREPARED TO ADDRESS YOUR QUESTIONS.

Please include information regarding:

- Proposed use
- Proposed improvements to building and/or site

The proposed project is to be a high speed diesel expansion to the existing Circle K with related parking, underground fuel storage tanks, and other necessary improvements. The project is anticipated to take up space on both parcels listed in this application. Expected new impervious area for the project is +/- 49,850 square feet. The existing Circle K will also have improvements including a building expansion for additional restrooms and an adjustment to the parking spaces to allow space for said expansion. See attached site plan for more detail.

SUBMIT WITH THIS FORM

- Copy of survey or sketch of location/building
- Sketch of any proposed improvements
- Any other information that will help in review of the proposal

SUBMIT COMPLETED FORM AND DOCUMENTS TO:

Mail: Lake City Growth Management Department, 205 N Marion Ave, Lake City, FL 32055

Email: growthmanagement@lcfla.com

Fax: 386-758-5426

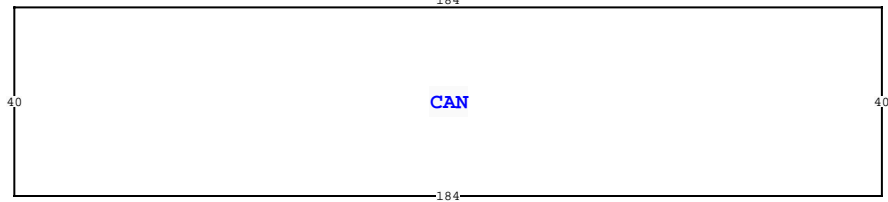
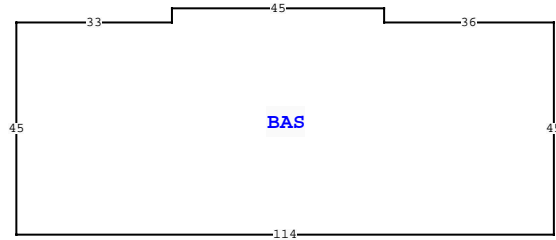
If you have any further questions, please contact Growth Management, 386-719-5750

WD 1339-654,

BUILDING CHARACTERISTICS		
ELEMENT	CD	CONSTRUCTION
Exterior Wall	17	MSNRY STUC 90
Exterior Wall	21	STONE 10
Roof Structure	09	RIDGE FRME 100
Roof Cover	04	BUILT-UP 100
Interior Wall	08	DECORATIVE 100
Interior Floor	15	HARDTILE 100
Ceiling	01	FIN.SUSPD 100
Air Condition	06	ENG CENTRL 100
Heating Type	09	ENG F AIR 100
Fixtures		12 100
Frame	03	MASONRY 100
Story Height		12 100
RMS		0 100
Stories	1.	1. 100
Units		0 100
Condition Adj	04	04 100
Quality	08	08

MARKET ADJUSTMENTS												
TYPE	MDL	EFF. AREA	TOT ADJ PTS	EFF. BASE RATE	REPL. COST NEW	AYB	EYB	ECON	FNCT	NORM	% COND	
4300	04	7,473	161.1610	97.50	728,618	2017	2017	0	0	0	3.00	97.00

COLUMBIA COUNTY PROPERTY			
VALUATION SUMMARY			PAGE 1 of 1
VALUATION BY	STANDARD		
Tax Group: 1	Tax Dist:		
BUILDING MARKET VALUE	706,759		
TOTAL MARKET OB/XF VALUE	103,200		
TOTAL LAND VALUE - MARKET	1,239,211		
TOTAL MARKET VALUE	2,049,170		
SOH/AGL Deduction	0		
ASSESSED VALUE	2,049,170		
TOTAL EXEMPTION VALUE	0		
BASE TAXABLE VALUE	2,049,170		
TOTAL JUST VALUE	2,049,170		
INCOME VALUE			
PREVIOUS YEAR MKT VALUE	2,056,457		



DOR CODE	CONV STORE/GAS			
1126				
MAP NUM	MKT AREA			
	06			
NEIGHBORHOOD	35316.040 1.00			
AREA TYPE	TOTAL GROSS AREA	PCT OF BASE	TOT ADJ AREA	SUBAREA MARKET VALUE
BAS	5,265	100	5,265	497,938
CAN	7,360	30	2,208	208,822
TOTALS	12,625		7,473	706,759

PERMIT NUM	DESCRIPTION	AMT	ISSUED

SALES DATA						
OFF RECORD Number	DATE	TYPE INST	Q / U	V / I	RSN CD	SALE PRICE
1339/0654	6/20/2017	WD Q	Q	I	01	2,487,200
GRANTOR: GWC DEVELOPMENT PARTN						
GRANTEE: ASPRI INVESTMENTS L						

EXTRA FEATURES																
L N	OB/XF CODE	DESCRIPTION	BLD CAP	L	W	UNITS	UT	Adj R	ADJ UNIT PRICE	ORIG COND	YEAR ON	YEAR ACTUAL	Q	% COND	OB/XF MKT VALUE	NOTES
1	0260	PAVEMENT-A	0	0	0	36,400.00	UT	1.60	1.60	100	2017	2017	3	100	58,240	
2	0166	CONC, PAVMT	0	0	0	11,300.00	UT	2.25	2.25	100	2017	2017	3	100	25,425	
3	0253	LIGHTING	0	0	0	10.00	UT	1,500.00	1,500.00	100	2017	2017	3	100	15,000	
4	0164	CONC BIN	0	0	0	308.00	UT	11.00	11.00	100	2017	2017	3	100	3,388	
5	0169	FENCE/WOOD	0	0	0	74.00	UT	15.50	15.50	100	2017	2017	3	100	1,147	

TOTAL OB/XF													103,200										
L N	USE CODE	LAND USE DESCRIPTION	CAP	R D	LOC ZONE	FRONT	DEPTH	TOT LND UTS	UNIT TYPE	D T	DPTH FACT	% COND	TOT ADJ	UNIT PRICE	ADJ UNIT PRICE	LAND VALUE	OTHER ADJUSTMENTS AND NOTES	YEAR	DENSITY	DECL	FRZ	YR	CONSRV
1	1410	CONV STORE	0			0.00	0.00	86,206.00	SF		1.00	1.00	1.25	11.50	14.38	1,239,211							

BUILDING NOTES			

BUILDING DIMENSIONS			
BAS= W36 N3 W45 S3 W33 S45 E114 PTR=S50 E35 CAN= W184 S40 E184 N40\$ W35 N50\$ N45\$.			



BUILDING CHARACTERISTICS

MARKET ADJUSTMENTS

COLUMBIA COUNTY PROPERTY PAGE 1 of 1 1

ELEMENT	CD	CONSTRUCTION

TYPE	MDL	EFF. AREA	TOT ADJ PTS	EFF. BASE RATE	REPL. COST NEW	AYB	EYB	ECON	FNCT	NORM	% COND

VALUATION BY		STANDARD
Valuation by:		
Building Market Value		0
Total Market OB/XF Value		0
Total Land Value - Market		515,968
Total Market Value		515,968
SOH/AGL Deduction		0
Assessed Value		515,968
Total Exemption Value		0
Base Taxable Value		515,968
Total Just Value		515,968
Income Value		
Previous Year MKT Value		515,968

DOR CODE	1000	VACANT COMMERCIAL		
MAP NUM		MKT AREA 06		
NEIGHBORHOOD	35316.060	1.00		
AREA TYPE	TOTAL GROSS AREA	PCT OF BASE	TOT ADJ AREA	SUBAREA MARKET VALUE

PERMIT NUM	DESCRIPTION	AMT	ISSUED

SALES DATA							
OFF RECORD Number	DATE	TYPE INST	Q / U	V / I	RSN CD	SALE PRICE	

TOTALS

BLD DATE		LGL DATE	
XF DATE		LAND DATE	
INC DATE		AG DATE	

BUILDING NOTES

L N	OB/XF CODE	DESCRIPTION	BLD	CAP	L	W	UNITS	UT	Adj R	ADJ UNIT PRICE	ORIG COND	YEAR ON	YEAR ACTUAL	Q	% COND	OB/XF MKT VALUE	NOTES

BUILDING DIMENSIONS

LAND DESCRIPTION													TOTAL OB/XF					OTHER ADJUSTMENTS AND NOTES						
L N	USE CODE	LAND USE DESCRIPTION	CAP	R D	LOC ZONE	FRONT	DEPTH	TOT LND UTS	UNIT TYPE	D T	DPTH FACT	% COND	TOT ADJ	UNIT PRICE	ADJ UNIT PRICE	LAND VALUE	YEAR	DENSITY	DECL	FRZ	YR	CONSRV		
1	1000	VACANT COMME	0			0.00	0.00	44,866.80	SF		1.00	1.00	1.00	11.50	11.50	515,968								

Columbia County Property Appraiser

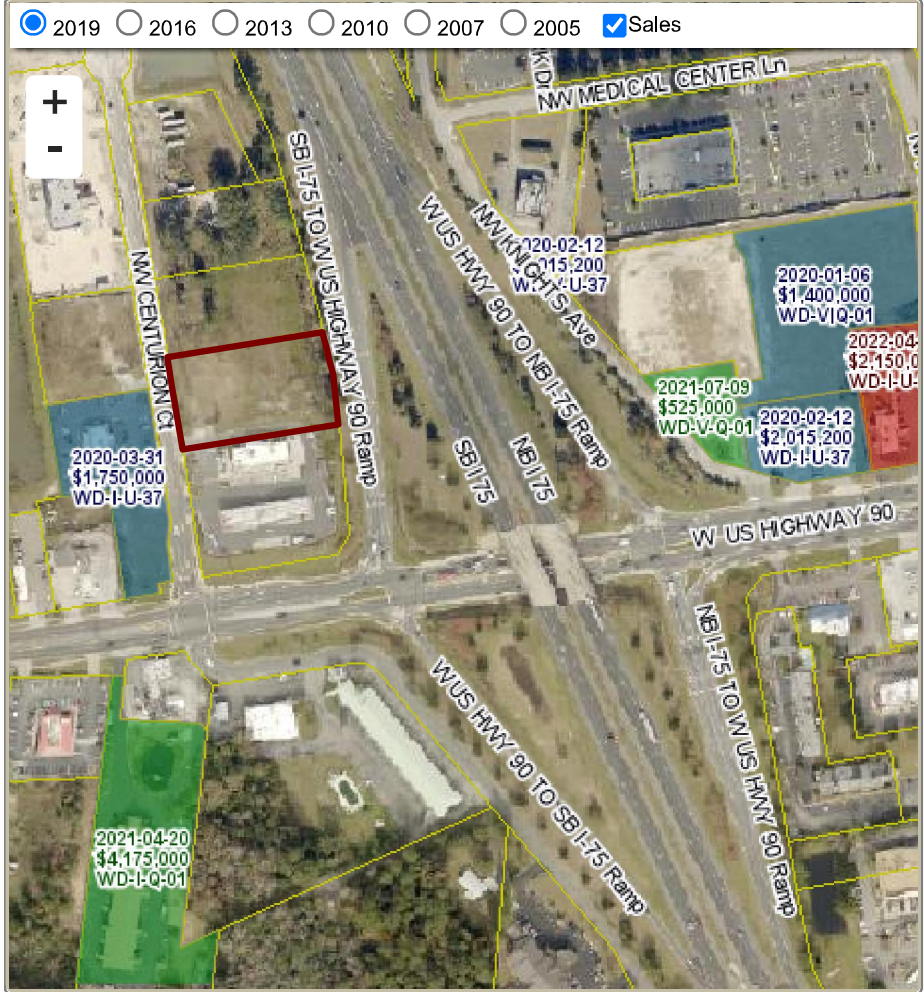
Jeff Hampton

2022 Working Values

updated: 6/9/2022

Parcel: << **35-3S-16-02524-102 (10494)** >>

Aerial Viewer Pictometry Google Maps



Owner & Property Info

Owner	GWC DEVELOPMENT PARTNERS LLC 2682 NW NOEGEL RD LAKE CITY, FL 32055		
Site			
Description*	LOT 2 GATEWAY CROSSING S/D A REPLAT OF LOTS 2 & 3. NKA LOT 2 A REPLAT OF LOTS 2, 3 & 11 OF GATEWAY CROSSING IN PLAT BK 9 PGS 176 & 177.		
Area	1.49 AC	S/T/R	35-3S-16
Use Code**	VACANT COMMERCIAL (1000)	Tax District	1

*The Description above is not to be used as the Legal Description for this parcel in any legal transaction.
 **The Use Code is a FL Dept. of Revenue (DOR) code and is not maintained by the Property Appraiser's office. Please contact your city or county Planning & Zoning office for specific zoning information.

Property & Assessment Values

2021 Certified Values		2022 Working Values	
Mkt Land	\$515,968	Mkt Land	\$746,396
Ag Land	\$0	Ag Land	\$0
Building	\$0	Building	\$0
XFOB	\$0	XFOB	\$0
Just	\$515,968	Just	\$746,396
Class	\$0	Class	\$0
Appraised	\$515,968	Appraised	\$746,396
SOH Cap [?]	\$0	SOH Cap [?]	\$178,831
Assessed	\$515,968	Assessed	\$746,396
Exempt	\$0	Exempt	\$0
Total Taxable	county:\$515,968 city:\$515,968 other:\$0 school:\$515,968	Total Taxable	county:\$567,565 city:\$567,565 other:\$0 school:\$746,396

Sales History

Sale Date	Sale Price	Book/Page	Deed	V/I	Qualification (Codes)	RCode
NONE						

Building Characteristics

Bldg Sketch	Description*	Year Blt	Base SF	Actual SF	Bldg Value
NONE					

▼ **Extra Features & Out Buildings** [\(Codes\)](#)

Code	Desc	Year Blt	Value	Units	Dims
N O N E					

▼ **Land Breakdown**

Code	Desc	Units	Adjustments	Eff Rate	Land Value
1000	VACANT COMMERCIAL (MKT)	64,904.000 SF (1.490 AC)	1.0000/1.0000 1.0000/ /	\$12 /SF	\$746,396



April 1, 2022

Suwanee River Water Management District
9225 CR 49
Live Oak, FL 32060

Subject: Circle K – US 90 & I-75 De-Minimis Exemption Letter
Project Name: Circle K – Circle K – US 90 & I-75
County: Columbia
Sec/Twp/Rge: S35 T3S R16E

To Whom it May Concern:

The proposed 3.47-acre Circle K – US 90 & I-75 project lies within the previously issued ERP No. 023-226410 and connects into the master project “Gateway Crossing” stormwater system. The project site is located at the northeast corner of the intersection of US Highway 90 and Centurion Court in the City of Lake City, Columbia County, Florida. We understand that this development is under SRWMD ERP No. 023-226410, and is shown as a portion of Basin DA-1 of the Gateway Crossing project. The proposed development will contain the addition of a 2,064 square foot Circle K high speed diesel canopy, with fueling stations, and associated infrastructure to the existing Circle K project constructed in 2016.

The proposed 3.47-acre Circle K – US 90 & I-75 lies within the previously issued ERP No. 023-226410 as stated above. We understand that this site is permitted up to 75% impervious area per ERP No. 023-226410.

As seen in the attached construction plans the Circle K – US 90 & I-75 project is proposing 2.63 acres (+/-114,580 SF) of impervious surface area to discharge into the system permitted under SRWMD ERP No. 023-226410. As the Circle K – US 90 & I-75 project is proposing impervious area less than or equal to the maximum allowed impervious surface area, and is connected to the master stormwater system that was approved in the Gateway Crossing project, ERP No. 023-226410, the project meets the requirements for a De-Minimis Exemption.

If you have any questions, or if you require additional information, please do not hesitate to contact our office at (407) 409-7002.

Very truly yours,

KIMLEY-HORN AND ASSOCIATES, INC.

Jarod C. Stubbs, P.E.
Project Engineer



Circle K – Lake City, FL

Traffic Impact Analysis

March 2022

Kimley»Horn

TRAFFIC IMPACT ANALYSIS

Circle K – US 90 & Centurion Court
Lake City, FL

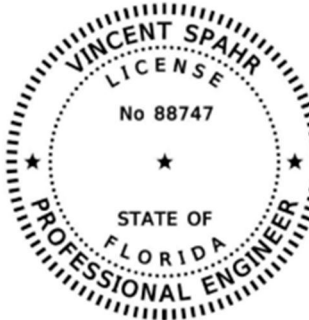
Prepared for:

Circle K

Prepared by:

Kimley-Horn and Associates, Inc.

March 2022



This document has been digitally signed and sealed by Vincent Spahr, P.E. on the date adjacent to the seal.

Vincent E Spahr
2022.03.18 09:03:21 - 04'00'

Printed copies of this document are not considered signed and sealed and the signature must be verified on any electronic copies.

Vincent Spahr, P.E.
Florida Registration Number 88747
Kimley-Horn and Associates, Inc.
800 SW 2nd Avenue, Suite 100
Gainesville, Florida 32601
Registry 35106

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Appendix A: Conceptual Site Plan

Appendix B: Traffic Data

Appendix C: Intersection Volume Development Worksheets

Appendix D: Synchro Output Reports

Appendix E: Trip Generation Calculations

Appendix F: FDOT *Trend* Worksheet

1.0 INTRODUCTION

Kimley-Horn has been retained by Circle K to analyze and document the traffic impacts associated with the expansion of a gas station and Circle K convenience market on the northeast quadrant of the intersection of US Highway 90 (US 90) and Centurion Court/SW Florida Gateway Drive in Lake City, Florida.

There is an existing 4,968 square-foot convenience market with 24 vehicle fueling positions (VFP) on the site. The project location is shown in Figure 1.

The applicant is proposing to add a 900-square foot expansion to the convenience market and 3 vehicle fueling positions designed for diesel trucks. The conceptual site plan is provided in Appendix A.

The study area for this traffic impact analysis includes the project driveways and the signalized intersection of US 90 and Centurion Court/SW Florida Gateway Drive, as shown in Figure 1.



Figure 1: Project Location Map

March 2022
 Project No.: 149880040

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2.0 EXISTING CONDITIONS ANALYSIS

2.1 EXISTING TRAFFIC DATA

Turning movement counts (TMCs) were collected at the study intersection on Thursday, September 2, 2021 during the AM (7:00AM – 9:00 AM) and PM (4:00PM – 6:00PM) peak periods. Raw turning movement counts are provided in Appendix B.

Turning movement volumes were adjusted using the peak season conversion factor (PSCF) from the Florida Department of Transportation (FDOT) Florida Traffic Online (FTO). Seasonal factor data is included in Appendix B. Existing signal timings were provided by Lake City staff for use in the analysis, signal timing worksheets are included in Appendix B.

Figure 2 illustrates turning movement volumes for existing peak season conditions at the study intersection. The intersection volume development worksheet can be found in Appendix C.

2.2 EXISTING INTERSECTION CONDITIONS

Intersection capacity analyses were performed for existing (2021) conditions using the operational analysis procedures outlined in the latest *Highway Capacity Manual, 6th Edition* (HCM 6). Specifically, *Synchro* (v11) software was used to evaluate existing operational conditions at study area intersections by reporting delay, level of service (LOS), volume-to-capacity (v/c) ratios, and the 95th percentile queue for each movement. Table 1 summarizes the operational analyses for the existing AM and PM peak hour conditions at the study intersection. Synchro outputs are provided in Appendix D.

Table 1: Existing Intersection Conditions

		AM Peak Hour				PM Peak Hour			
		Delay (sec/veh)	LOS	v/c Ratio	95th percentile queue (veh)	Delay (sec/veh)	LOS	v/c Ratio	95th percentile queue (veh)
US 90 & Centurion Court	Overall Intersection	13.2	B	-	-	13.2	B	-	-
	Eastbound	11.5	B	-	-	9.6	A	-	-
	EBL	5.5	A	0.10	0.5	6.9	A	0.08	0.3
	EBT	11.7	B	0.58	14.6	9.7	A	0.48	12.5
	EBT/R	11.7	B	0.58	15.2	9.6	A	0.48	12.9
	Westbound	7.7	A	-	-	8.7	A	-	-
	WBL	7.9	A	0.16	0.5	6.7	A	0.24	1.1
	WBT	7.9	A	0.38	8.0	9.0	A	0.54	13.7
	WBR	5.8	A	0.06	0.9	5.4	A	0.09	1.5
	Northbound	55.6	E	-	-	65.1	E	-	-
	NBL	54.9	D	0.06	0.6	66.7	E	0.28	2.9
	NBT/R	55.7	E	0.42	4.1	64.0	E	0.38	4.2
	Southbound	58.8	E	-	-	68.1	E	-	-
	SBL	61.9	E	0.45	3.8	71.6	E	0.51	5.0
	SBT/R	53.6	D	0.23	2.1	62.7	E	0.27	3.0

The intersection of US 90 and Centurion Court operates with LOS B during existing (2021) AM peak hour and PM peak hour conditions. All movements operate with v/c ratios less than 1.00 under existing (2021) AM and PM peak hour conditions. The northbound and southbound approaches operate with LOS E during the AM and PM peak hour due to the prioritization of green time for the mainline US 90 movements.

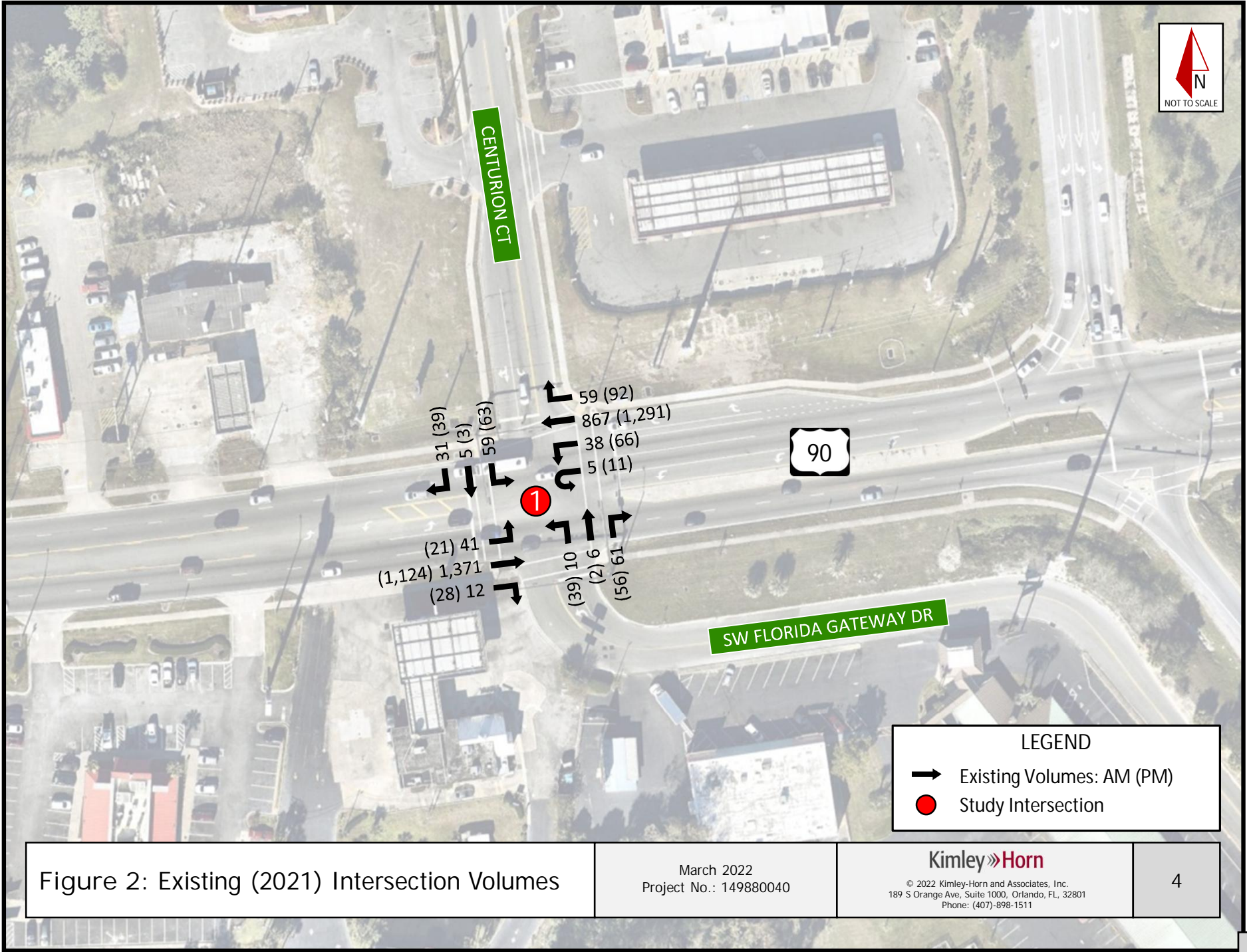


Figure 2: Existing (2021) Intersection Volumes

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3.0 PROJECT DEVELOPMENT

The existing site currently has 24 VFPs and a 4,968-square foot Circle K convenience store. The proposed expansion will add approximately 900-square feet to the existing convenience market and 3 VFPs north of the existing site. The latest industry standards were referenced to evaluate the amount of new external trips to be generated by the site at buildout.

3.1 SITE ACCESS

Access to the site is proposed via two existing driveways and one new driveway along Centurion Court, as shown in the site plan provided in Appendix A.

3.2 TRIP GENERATION

Trip generation and pass-by rates for the proposed development were calculated using the 11th Edition of the Institute of Transportation Engineers' (ITE) *Trip Generation Manual*. Land Use Code (LUC) 945 (Gasoline Station with Convenience Market) was used to calculate the trip generation potential for the existing and proposed development.

The trip generation potential of the existing Circle K convenience store and gas station was compared to observed traffic volumes on Centurion Court north of US 90. Table 2 summarizes the comparison of the calculated trip generation potential of the existing development and the observed peak hour volumes on Centurion Court.

Table 2: Existing Site Trip Generation Comparison

	AM Peak Hour			PM Peak Hour		
	Total	In (NB)	Out (SB)	Total	In (NB)	Out (SB)
ITE <i>Trip Generation Manual</i>	649	325	324	546	273	273
Observed Peak Season Traffic	201	106	95	220	115	105

Since the existing AM and PM peak hour traffic volumes were significantly less than the trip generation potential of the existing development, the trip generation calculations for the proposed expansion to the convenience store and gas station were adjusted proportionately to reflect actual conditions anticipated at the site under buildout conditions.

Table 3 provides the AM peak hour, and PM peak hour trip generation calculations for the proposed expansion and the adjustment applied based on the existing trip generation comparison. A factor of 0.31 (201/649) was applied to the AM peak hour trip generation calculations and a factor of 0.40 (220/546) was applied to the PM peak hour trip generation calculations in accordance with the comparison illustrated in Table 2.

As summarized in Table 3, the proposed expansion is anticipated to generate 16 net new AM peak hour trips (8 inbound and 8 outbound), and 18 net new PM peak hour trips (9 inbound and 9 outbound) to the external roadway network at buildout. In addition, the proposed expansion is anticipated to generate 48 AM peak hour pass-by trips (24 inbound and 24 outbound), and 54 PM peak hour pass-by trips (27 inbound

and 27 outbound). A detailed table including all trip generation calculations and adjustments is provided in Appendix E.

Table 3: Trip Generation Summary

	AM Peak Hour			PM Peak Hour		
	Total	In (NB)	Out (SB)	Total	In (NB)	Out (SB)
ITE <i>Trip Generation Manual</i> (Net New)	50	25	25	46	23	23
ITE <i>Trip Generation Manual</i> (Pass-by)	204	102	102	180	90	90
Adjustment Factor	0.31			0.40		
Adjusted Net New Trips	16	8	8	18	9	9
Adjusted Pass-by Trips	48	24	24	54	27	27

3.3 TRIP DISTRIBUTION

The project's trip distribution was developed based on observed traffic patterns within the study area roadway network and engineering judgement. Figure 3 displays the anticipated trip distribution for the proposed Circle K gas station expansion at buildout.

3.4 TRIP ASSIGNMENT

Site distribution percentages were used to assign anticipated project trips to the study area intersection and driveways. Figure 4 shows the anticipated AM and PM peak hour project movements at the study area intersection and project driveways.

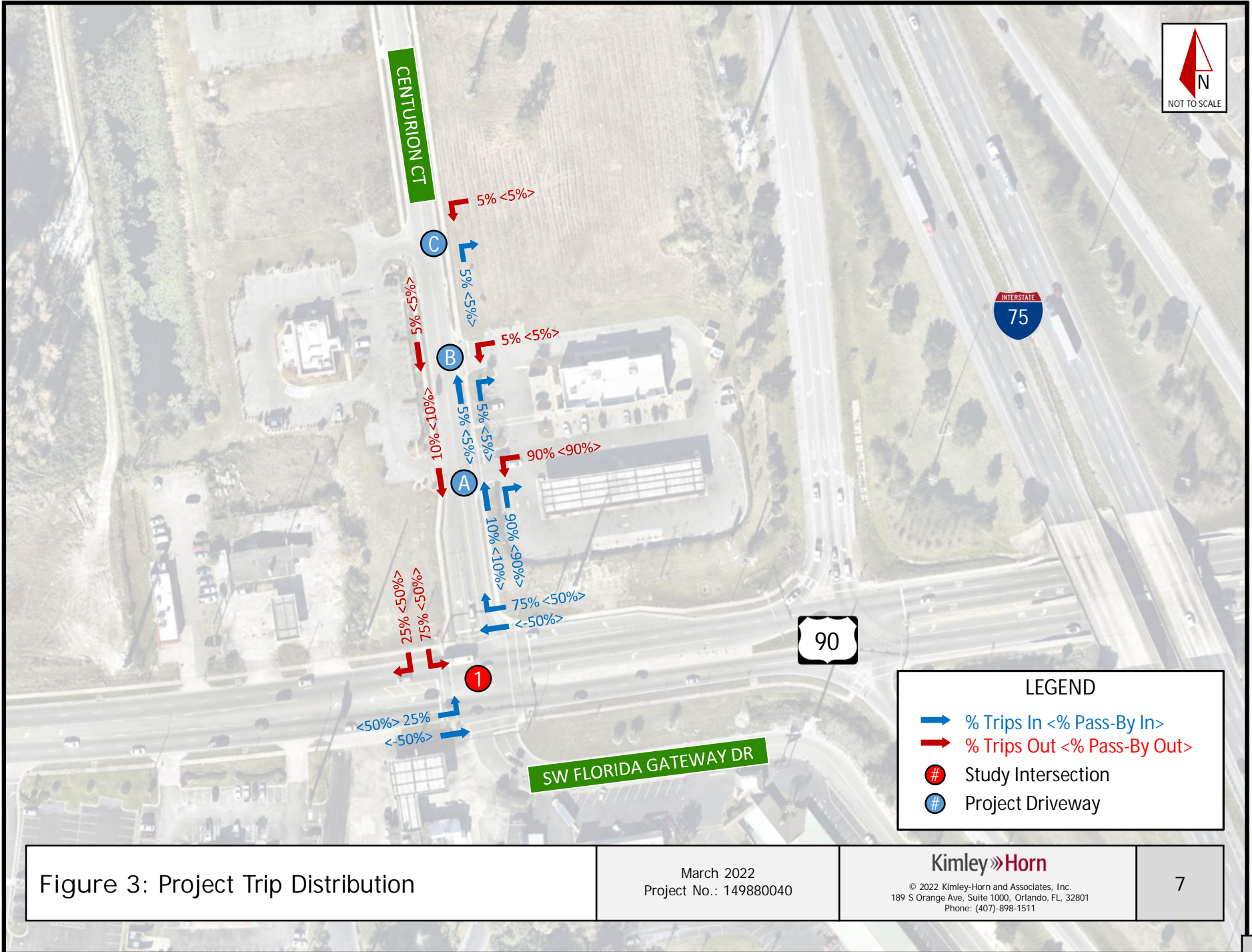
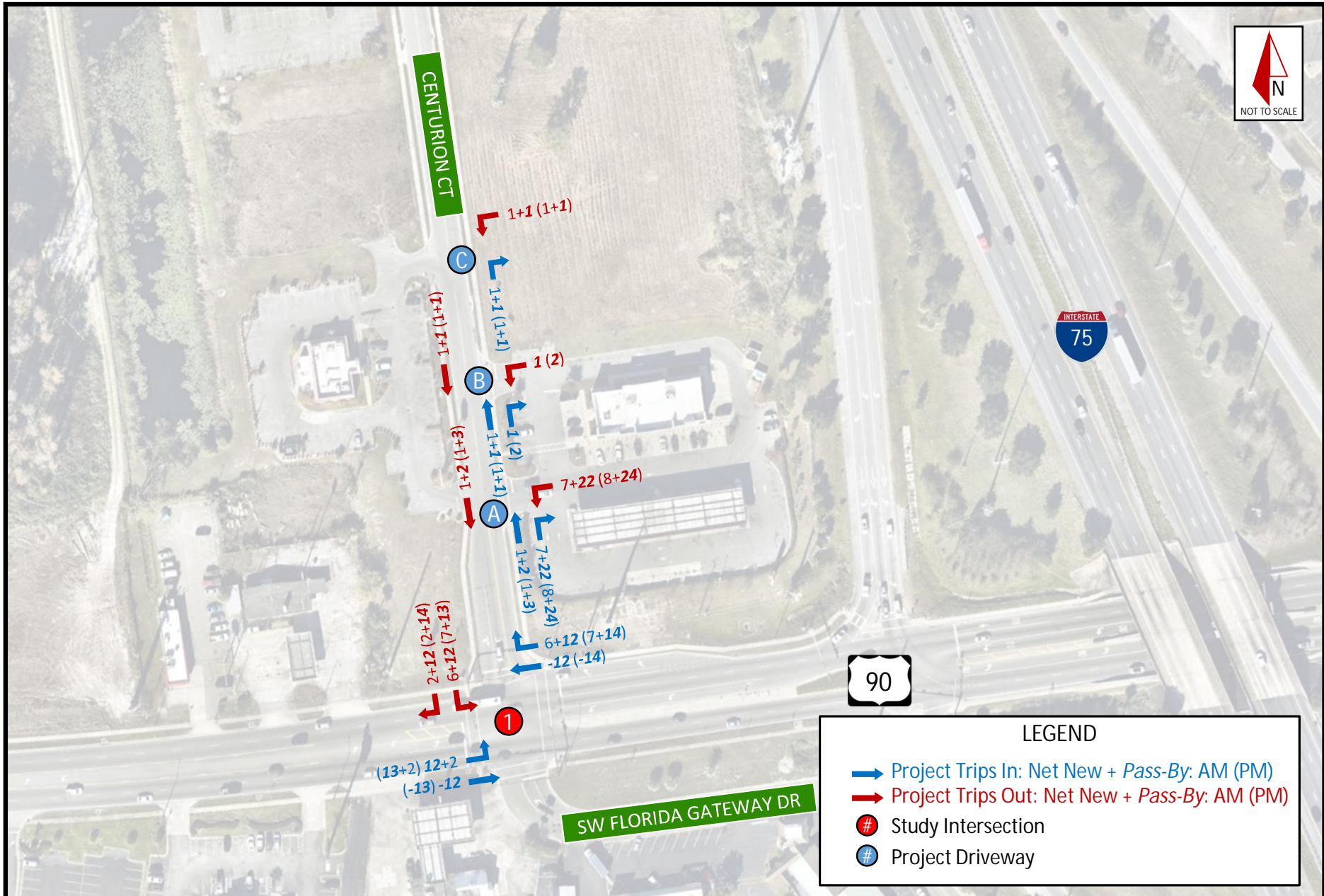


Figure 3: Project Trip Distribution

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LEGEND

- Project Trips In: Net New + Pass-By: AM (PM)
- Project Trips Out: Net New + Pass-By: AM (PM)
- 1 Study Intersection
- C Project Driveway

Figure 4: Project Trip Assignment

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4.0 BACKGROUND CONDITIONS ANALYSIS – YEAR 2023

4.1 HISTORICAL TRAFFIC GROWTH

A historical traffic growth rate was calculated based upon the nearest historical Annual Average Daily Traffic (AADT) data available from FTO. A 2.11% annual historical growth rate was calculated based on the average traffic growth exhibited over the past five (5) years from an FDOT count station located east of the project site on US 90. The growth trend worksheet can be found in Appendix F.

4.2 BACKGROUND TRAFFIC

Traffic conditions were evaluated for year 2023 background conditions prior to the addition of project traffic. Background volumes at study area intersections were derived by applying 2.11% annual growth to existing (2021) traffic counts. Figure 5 illustrates AM peak hour and PM peak hour turning movement volumes for background conditions at the study intersection. The intersection volume development worksheet can be found in Appendix C.

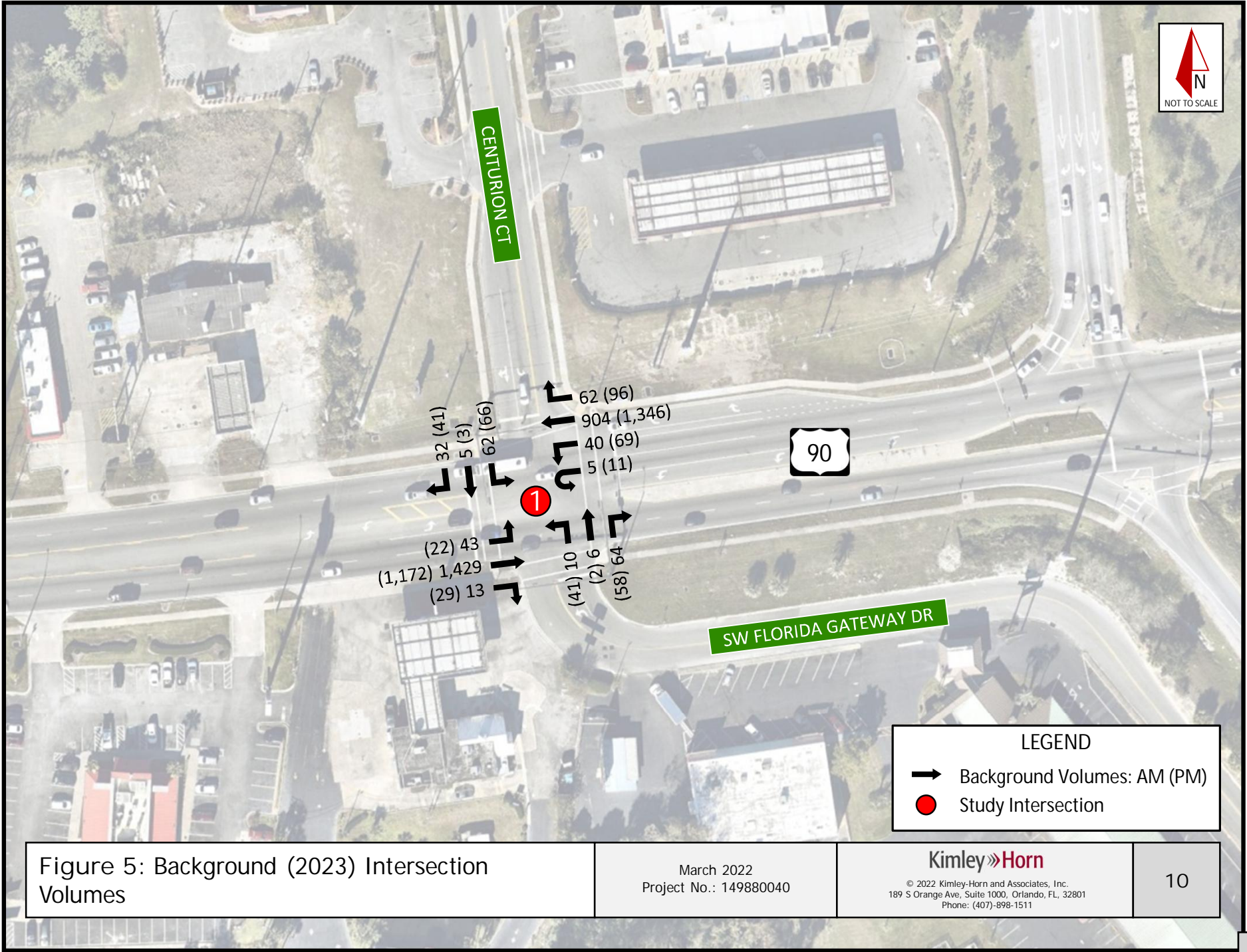
4.3 BACKGROUND INTERSECTION ANALYSIS

Intersection operational analyses were performed for 2023 background conditions in the AM and PM peak hours using procedures outlined in the *Highway Capacity Manual 6* with *Synchro* (v11) software. Table 4 summarizes the operational analyses for the 2023 background AM and PM peak hour conditions at the study intersection. Synchro outputs are provided in Appendix D.

Table 4: Background Intersection Conditions

		AM Peak Hour				PM Peak Hour			
		Delay (sec/veh)	LOS	v/c Ratio	95th percentile queue (veh)	Delay (sec/veh)	LOS	v/c Ratio	95th percentile queue (veh)
US 90 & Centurion Court	Overall Intersection	13.8	B	-	-	13.7	B	-	-
	Eastbound	12.4	B	-	-	10.2	B	-	-
	EBL	5.8	A	0.11	0.5	7.5	A	0.09	0.3
	EBT	12.6	B	0.61	15.8	10.2	B	0.50	13.4
	EBT/R	12.5	B	0.61	16.4	10.2	B	0.51	13.8
	Westbound	8.2	A	-	-	9.3	A	-	-
	WBL	8.8	A	0.17	0.6	7.3	A	0.26	1.1
	WBT	8.3	A	0.39	8.6	9.7	A	0.57	14.9
	WBR	6.0	A	0.06	0.9	5.6	A	0.09	1.6
	Northbound	55.1	E	-	-	64.8	E	-	-
	NBL	54.4	D	0.06	0.6	66.6	E	0.29	3.1
	NBT/R	55.2	E	0.42	4.3	63.5	E	0.38	4.4
	Southbound	58.6	E	-	-	67.8	E	-	-
	SBL	61.8	E	0.47	4.1	71.5	E	0.52	5.2
SBT/R	53.1	D	0.22	2.2	62.3	E	0.28	3.2	

The intersection of US 90 and Centurion Court is expected to operate with LOS B during background (2023) AM peak hour and PM peak hour conditions. All movements are expected to operate with v/c ratios less than 1.00 under background (2023) AM and PM peak hour conditions. The northbound and southbound approaches are expected to continue to operate with LOS E during the AM and PM peak hour due to the prioritization of green time for the mainline US 90 movements.



LEGEND

- ➔ Background Volumes: AM (PM)
- Study Intersection

Figure 5: Background (2023) Intersection Volumes

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5.0 BUILDOUT CONDITIONS ANALYSIS – YEAR 2023

5.1 BUILDOUT TRAFFIC

Future traffic conditions for the proposed development were evaluated for year 2023 conditions with the inclusion of project traffic. Buildout volumes were developed by adding anticipated project trips to background (2023) volumes. Figure 6 illustrates the projected turning movement volumes under buildout AM and PM peak hour conditions at the study intersection and the proposed driveways. The intersection volume development worksheet can be found in Appendix C.

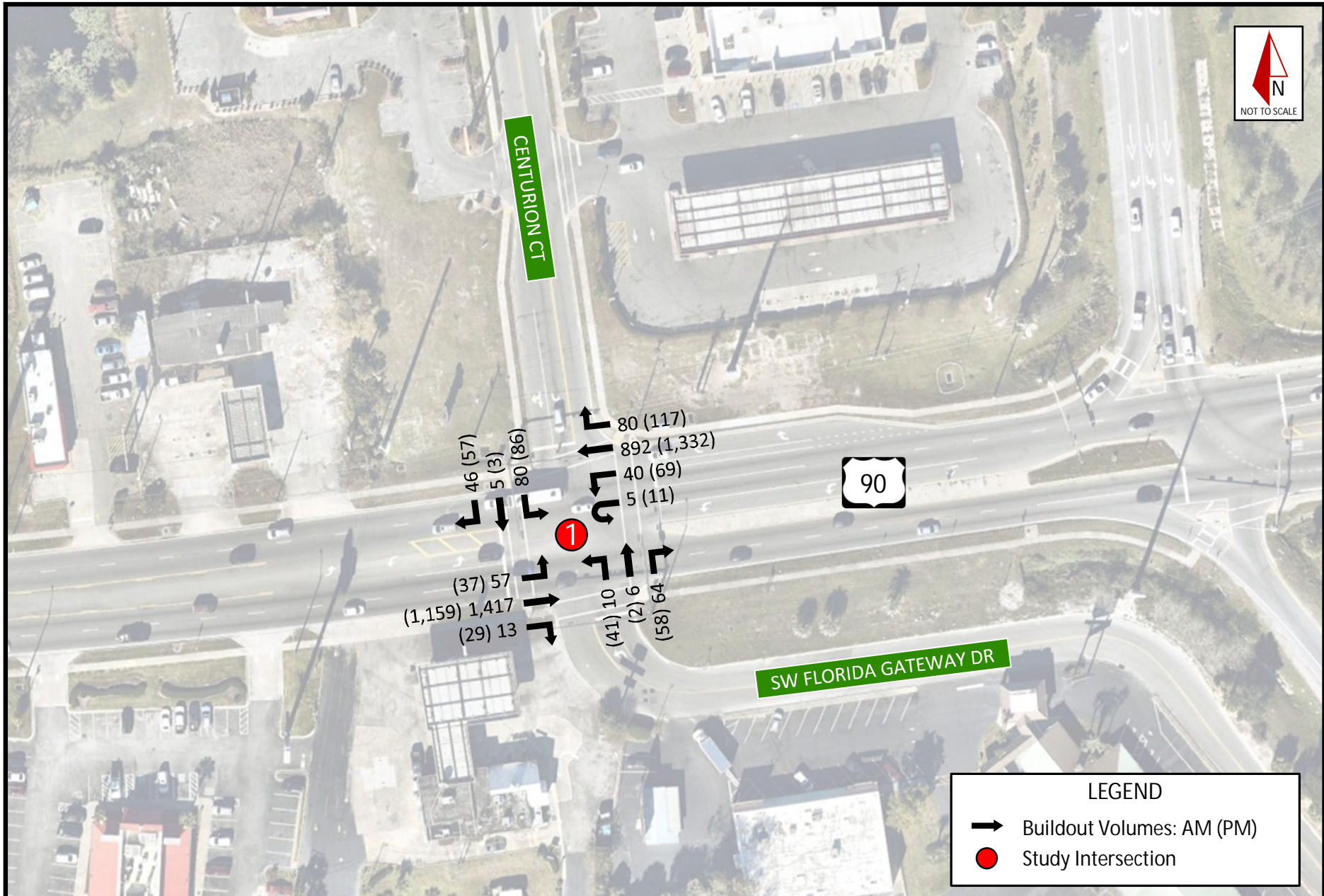
5.2 BUILDOUT INTERSECTION ANALYSIS

Intersection operational analyses were performed for 2023 buildout conditions in the AM and PM peak hour conditions using procedures outlined in the *Highway Capacity Manual 6* with *Synchro* (v11) software. Table 5 summarizes the operational analyses for the 2023 buildout AM and PM peak hour conditions at the study intersection. Synchro outputs are provided in Appendix D.

Table 5: Buildout Intersection Conditions

		AM Peak Hour				PM Peak Hour			
		Delay (sec/veh)	LOS	v/c Ratio	95th percentile queue (veh)	Delay (sec/veh)	LOS	v/c Ratio	95th percentile queue (veh)
US 90 & Centurion Court	Overall Intersection	15.0	B	-	-	15.5	B	-	-
	Eastbound	13.3	B	-	-	11.3	B	-	-
	EBL	6.4	A	0.14	0.8	8.9	A	0.16	0.6
	EBT	13.6	B	0.62	16.5	11.4	B	0.51	14.2
	EBT/R	13.5	B	0.62	17.1	11.4	B	0.51	14.7
	Westbound	9.0	A	-	-	10.8	B	-	-
	WBL	9.5	A	0.18	0.6	8.3	A	0.27	1.3
	WBT	9.2	A	0.4	9.0	11.3	B	0.58	16.2
	WBR	6.8	A	0.08	1.3	6.8	A	0.12	2.2
	Northbound	53.2	D	-	-	62.7	E	-	-
	NBL	53.8	D	0.06	0.6	65.5	E	0.28	3.1
	NBT/R	53.1	D	0.37	4.2	60.7	E	0.33	4.3
	Southbound	57.8	E	-	-	66.4	E	-	-
	SBL	61.3	E	0.54	5.2	70.4	E	0.58	6.8
SBT/R	52.1	D	0.28	3.0	60.7	E	0.33	4.2	

The intersection of US 90 and Centurion Court is expected to operate with LOS B during buildout (2023) AM peak hour and PM peak hour conditions. All movements are expected to operate with v/c ratios less than 1.00 under buildout (2023) AM and PM peak hour conditions. The northbound and southbound approaches are expected to continue to operate with LOS E during the AM and PM peak hour due to the prioritization of green time for the mainline US 90 movements.



LEGEND

- ➔ Buildout Volumes: AM (PM)
- Study Intersection

Figure 6: Buildout (2023) Intersection Volumes

March 2022
Project No.: 149880040

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6.0 CONCLUSION

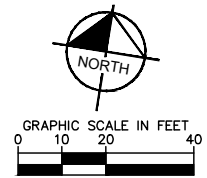
This traffic impact analysis was performed to assess the transportation impacts of the proposed expansion of a gas station and Circle K convenience market located in the northwest quadrant of the intersection of US Highway 90 (SR 10) and Centurion Court/SW Florida Gateway Drive. The expansion, proposed for buildout in year 2023, will include the addition of 3 vehicle fueling positions designed for diesel trucks and a 900-square foot expansion to the existing Circle K convenience market. Access to the site will be provided via two existing driveways and one new driveway to the north on Centurion Court.

Accounting for the observed trip generation of the existing site, the proposed expansion is anticipated to generate 16 net new AM peak hour trips and 18 net new PM peak hour trips at buildout. An additional 48 new AM peak hour pass-by trips and 54 new PM peak hour pass-by trips are expected at the site as well.

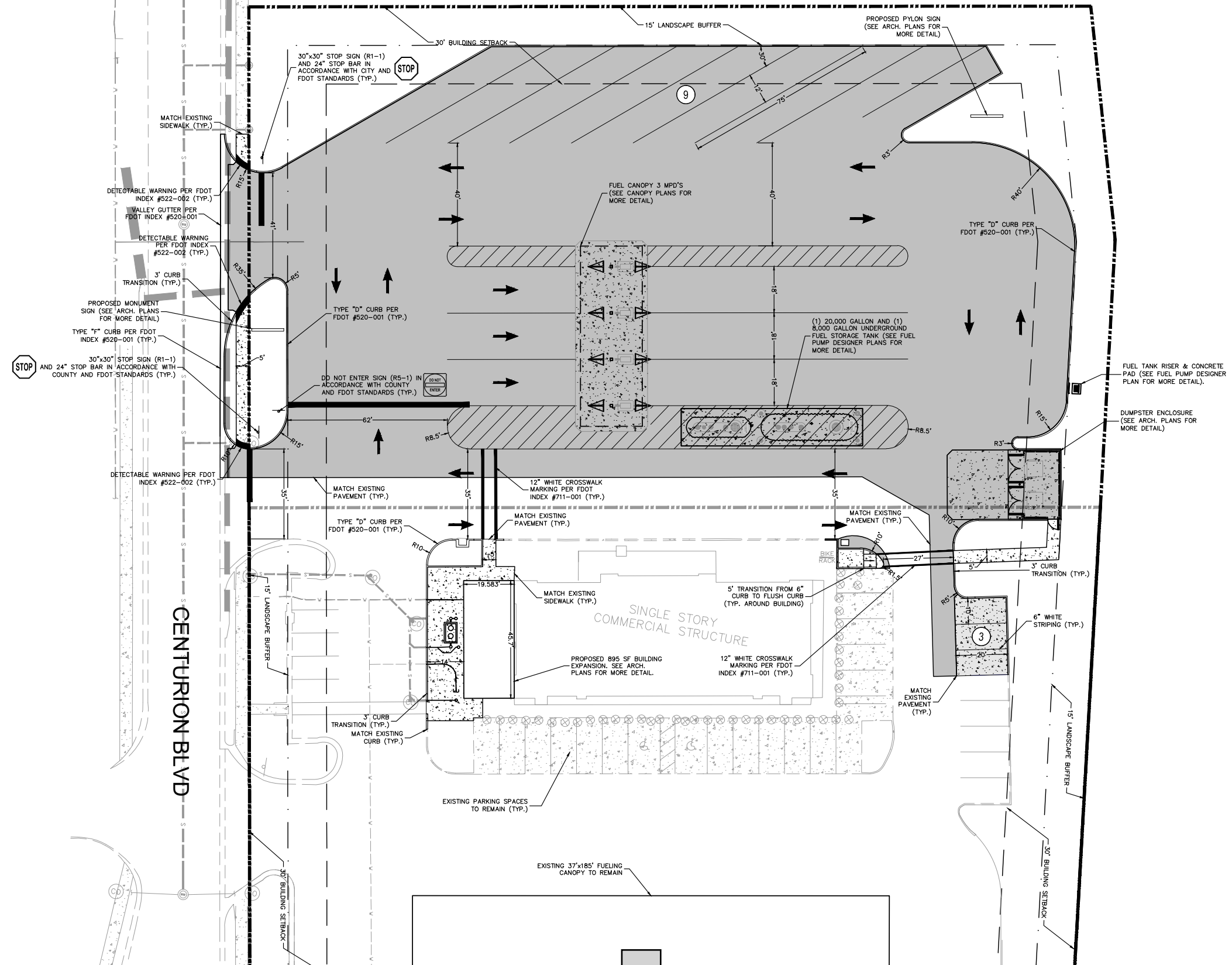
Operational analyses were performed utilizing *Synchro* software for the existing (2021), background (2023), and buildout (2023) conditions at the study intersection of US 90 and Centurion Court/SW Florida Gateway Drive during the AM peak hour and the PM peak hour. Results indicated that the study intersection is expected to operate at LOS B through the buildout year. No operational deficiencies are expected at the study intersection with the inclusion of project traffic under buildout (2023) conditions.

APPENDIX A
Conceptual Site Plan

Plotted By: Fitch, Elliot - Sheet Set: CIRCLE K - US HWY 90 & I-75 High Speed Diesel Fuel Expansion - Layout: C4.1 SITE PLAN - March 14, 2022 - 07:56:28am - K:\ORL\Civil\149880040-Circle K US90 & I75\CADD\CONSTR\PlanSheets\C4.0 - SITE PLAN.dwg
 This document, together with the concepts and designs presented herein, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.



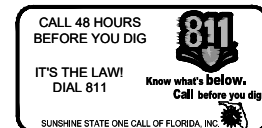
- NOTES:**
1. ALL CURB DIMENSIONS ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
 2. BUILDING DIMENSIONS ARE TO THE OUTSIDE FACE OF BUILDING UNLESS OTHERWISE NOTED.
 3. REFER TO ARCHITECTURAL AND STRUCTURAL PLANS TO VERIFY ALL BUILDING DIMENSIONS.
 4. REFER TO SIGNAGE PLANS FOR MONUMENT SIGN DETAILS.
 5. SEE MEP PLANS FOR ELECTRICAL DRAWINGS.
 6. ALL PROPOSED ON-SITE STRIPING AND PAVEMENT MARKING WILL BE PAINTED UNLESS OTHERWISE NOTED AND IN ACCORDANCE WITH FDOT INDEX 711-001.
 7. REFER TO ARCHITECTURAL PLANS FOR PROPOSED TRASH CAN LOCATIONS AND DESIGN.
 8. BOLLARDS IN SIDEWALK ADJACENT TO BUILDING SHALL BE COVERED WITH RED PLASTIC COVERS TO BE SUPPLIED BY CONTRACTOR.
 9. BOLLARDS UNDER CANOPY SHALL BE COVERED WITH GRAY PLASTIC COVERS TO BE SUPPLIED BY CONTRACTOR (SEE FUEL PUMP DESIGNER PLANS FOR MORE DETAIL).
 10. REFER TO ARCHITECTURAL PLANS FOR SITE LIGHTING AND ELECTRICAL PLANS.
 11. ALL SIGNAGE AND PAVEMENT MARKINGS SHALL MEET MUTCD AND FDOT STANDARDS.
 12. ALL SIGNAGE SHALL MEET THE REQUIREMENTS OF POLK COUNTY LAND DEVELOPMENT CODE, CHAPTER 7, SEC. 760



LEGEND

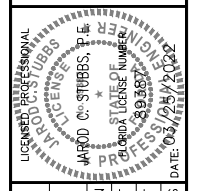
	PROPERTY LINE (TYP.)
	PROPOSED ASPHALT PAVEMENT (SEE DETAIL SHEET C7.0)
	PROPOSED CONCRETE SIDEWALK (SEE DETAIL SHEET C7.0)
	PROPOSED MEDIUM DUTY CONCRETE (SEE DETAIL SHEET C7.0)
	PROPOSED HEAVY DUTY CONCRETE (SEE DETAIL SHEET C7.0)

VERTICAL DATUM:
 ELEVATIONS ARE BASED ON BENCHMARK DESIGNATION BM 32 BEING: 117.497 FEET, (NGVD 29), PUBLISHED BY FLORIDA DEPARTMENT OF TRANSPORTATION. ORTHOMETRIC HEIGHT CONVERSION PROVIDED BY VERTCON; DATUM SHIFT (NAVD-NGVD) = -0.883 FEET



No.	REVISIONS	DATE	BY

Kimley»Horn
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 189 S. ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801
 WWW.KIMLEY-HORN.COM REGISTRY No. 35106



KHA PROJECT	149880040
DATE	03/25/2022
SCALE	AS SHOWN
DESIGNED BY	EJF
DRAWN BY	EJF
CHECKED BY	JCS
DATE	03/25/2022

SITE PLAN

CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION
 FLORIDA
 CITY OF LAKE CITY
 SHEET NUMBER
C4.1

APPENDIX B

Traffic Data

APPENDIX 8 : TRAFFIC COUNT

Florida

LEASE - NTI

US 90 and Florida Gateway Drive
Lake City FL



TRAFFIC COUNT AT THE SITE PROPOSED

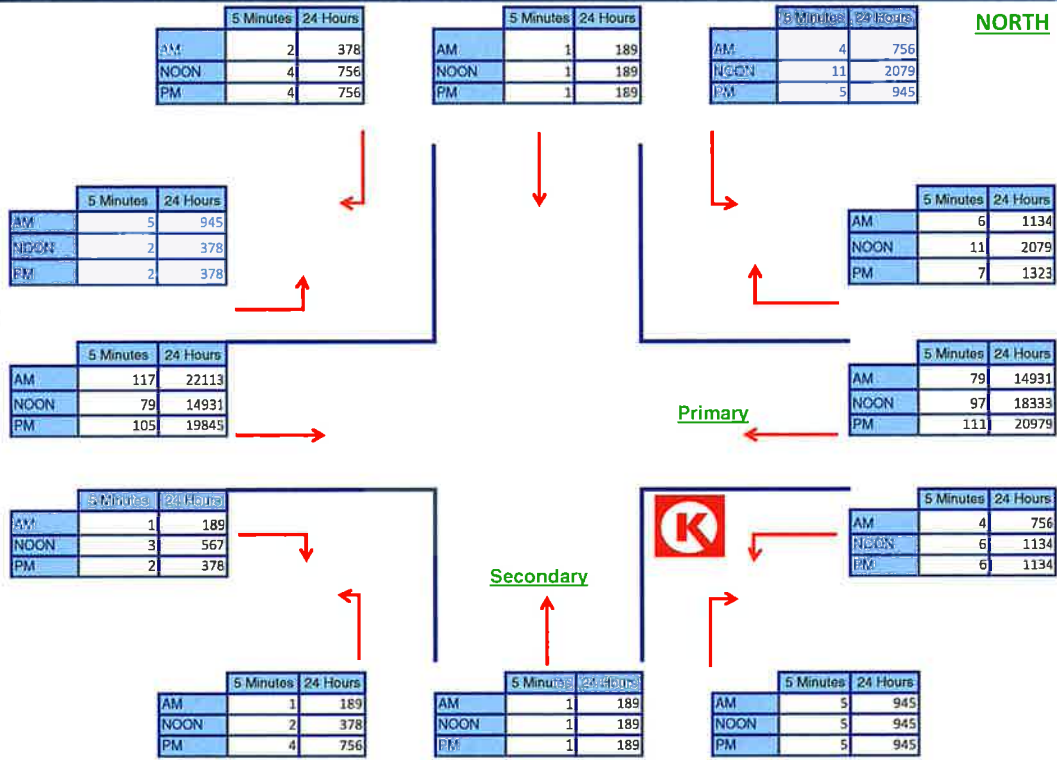
MANUAL

Traffic counts taken during COVID-19

Total AM:	42,714
Total NOON:	41,958
Total PM:	47,817

NOON COUNT

Primary	37,422
Secondary	4,536
TOTAL:	41,958 cars/day



D.O.T.

	PRIMARY	SECONDARY	TOTAL	YEAR
DAILY TRAFFIC TOTALS	27,000		27,000	2020

DIRECT TRAFFIC

IS ACCESS TO THIS SITE AFFECTED BY MEDIAN **NO**

	PRIMARY	SECONDARY	TOTAL
DIRECT TRAFFIC TOTALS			0

TRUCK TRAFFIC COUNT

	PRIMARY	SECONDARY	TOTAL	SOURCE
TRUCK TRAFFIC TOTALS			0	



National Data & Surveying Services

Site Code: 21-120370-001

Date: 09/02/2021

Weather: Sunny

City: Lake City

County: Columbia

Count Times: 07:00 - 09:00

12:00 - 14:00

16:00 - 18:00

Control: Signalized

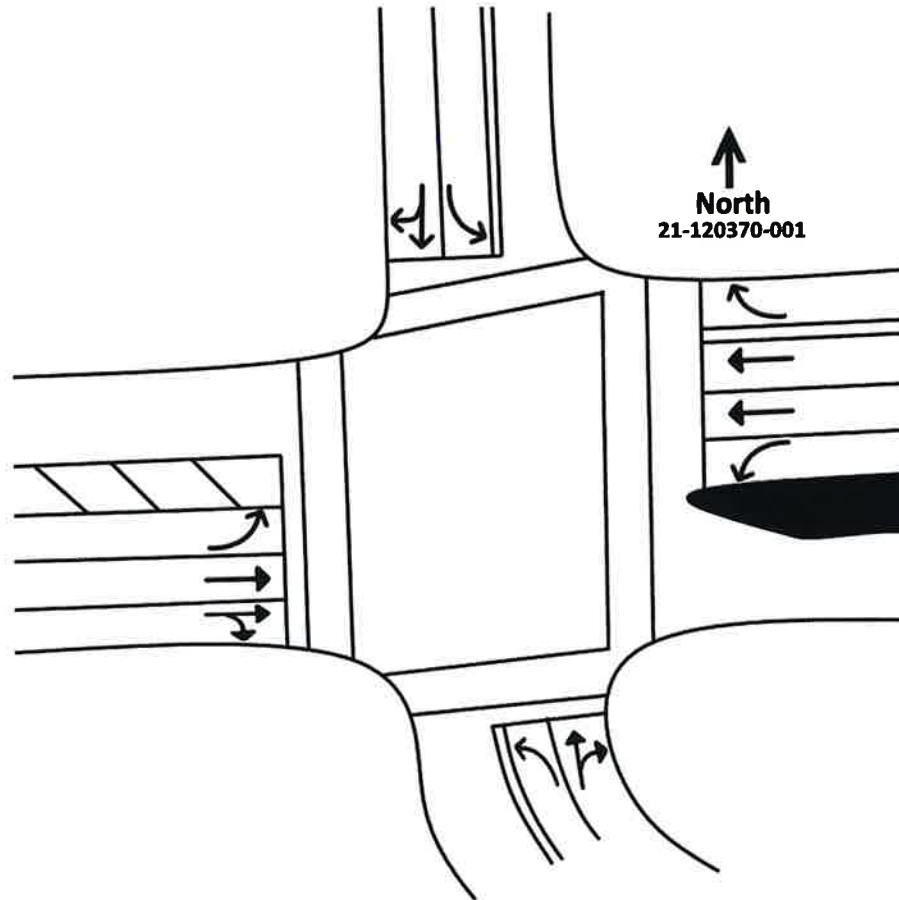
SIGNAL TIMING

PHASES	1	2	3
NT/ST	00:25	00:33	00:20
EL/WL	00:15	-	-
WL/WT	-	00:13	-
ET/WT	01:42	01:34	01:59



N/S Street: Florida Gateway Dr

Speed: N/A



E/W Street: US Hwy 90

Speed: 45 MPH

National Data & Surveying Services Intersection Turning Movement Count

Location: Florida Gateway Dr & US Hwy 90
City: Lake City
Control: Signalized

Project ID: 21-120370-001
Date: 9/2/2021

Data - Total

NS/EW Streets:	Florida Gateway Dr				Florida Gateway Dr				US Hwy 90				US Hwy 90				TOTAL
	NORTHBOUND				SOUTHBOUND				EASTBOUND				WESTBOUND				
AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	TOTAL
	NL	NT	NR	NU	SL	ST	SR	SU	EL	ET	ER	EU	WL	WT	WR	WU	
7:00 AM	4	1	7	0	16	0	7	0	6	215	1	0	2	149	8	1	417
7:15 AM	1	0	16	0	7	2	8	0	6	348	2	0	4	182	11	1	588
7:30 AM	2	2	14	0	14	2	4	0	8	378	1	0	10	215	11	0	661
7:45 AM	1	1	15	0	13	1	6	0	15	350	4	0	11	236	18	2	673
8:00 AM	6	3	14	0	23	0	12	0	11	255	5	0	12	209	17	2	569
8:15 AM	2	1	16	0	22	0	10	0	10	214	5	0	16	196	16	0	508
8:30 AM	4	0	12	0	19	3	11	0	7	239	6	0	16	211	21	1	550
8:45 AM	6	2	12	0	17	4	9	0	7	211	6	0	7	203	11	1	496
TOTAL VOLUMES :	26	10	106	0	131	12	67	0	70	2210	30	0	78	1601	113	8	4462
APPROACH %'s :	18.31%	7.04%	74.65%	0.00%	62.38%	5.71%	31.90%	0.00%	3.03%	95.67%	1.30%	0.00%	4.33%	88.94%	6.28%	0.44%	
PEAK HR :	07:15 AM - 08:15 AM																TOTAL
PEAK HR VOL :	10	6	59	0	57	5	30	0	40	1331	12	0	37	842	57	5	2491
PEAK HR FACTOR :	0.417	0.500	0.922	0.000	0.620	0.625	0.625	0.000	0.667	0.880	0.600	0.000	0.771	0.892	0.792	0.625	0.925
	0.815				0.657				0.893				0.881				
NOON	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	TOTAL
	NL	NT	NR	NU	SL	ST	SR	SU	EL	ET	ER	EU	WL	WT	WR	WU	
12:00 PM	5	0	10	0	23	0	8	0	6	318	4	0	16	230	26	3	649
12:15 PM	7	0	18	0	27	0	8	0	7	237	4	0	19	261	25	4	617
12:30 PM	4	2	13	0	21	0	13	0	6	290	3	1	15	252	21	0	641
12:45 PM	7	1	12	0	18	1	13	0	8	234	4	0	15	300	34	2	649
1:00 PM	7	1	16	0	34	1	13	0	5	236	9	0	16	291	34	2	665
1:15 PM	8	1	14	0	28	1	5	0	4	252	7	0	17	291	22	2	652
1:30 PM	3	0	12	0	30	0	5	0	5	243	9	0	21	273	26	4	631
1:45 PM	2	1	18	0	25	1	4	0	5	254	1	0	11	290	20	0	632
TOTAL VOLUMES :	43	6	113	0	206	4	69	0	46	2064	41	1	130	2188	208	17	5136
APPROACH %'s :	26.54%	3.70%	69.75%	0.00%	73.84%	1.43%	24.73%	0.00%	2.14%	95.91%	1.91%	0.05%	5.11%	86.04%	8.18%	0.67%	
PEAK HR :	12:30 PM - 01:30 PM																TOTAL
PEAK HR VOL :	26	5	55	0	101	3	44	0	23	1012	23	1	63	1134	111	6	2607
PEAK HR FACTOR :	0.813	0.625	0.859	0.000	0.743	0.750	0.846	0.000	0.719	0.872	0.639	0.250	0.926	0.945	0.816	0.750	0.980
	0.896				0.771				0.883				0.936				
PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	TOTAL
	NL	NT	NR	NU	SL	ST	SR	SU	EL	ET	ER	EU	WL	WT	WR	WU	
4:00 PM	4	0	14	0	16	0	7	0	3	273	3	0	11	349	30	3	713
4:15 PM	4	0	16	0	18	0	12	0	5	237	3	0	15	314	12	1	637
4:30 PM	4	0	13	0	21	2	7	0	7	239	7	0	11	295	21	0	627
4:45 PM	5	1	14	0	15	0	6	0	5	287	6	0	17	310	19	3	688
5:00 PM	13	1	15	0	16	2	13	0	6	316	6	0	14	334	21	4	761
5:15 PM	7	0	12	0	13	1	9	0	3	290	7	0	15	265	24	4	650
5:30 PM	13	0	13	0	17	0	10	0	6	198	8	0	18	344	25	0	652
5:45 PM	5	0	27	0	18	1	7	0	4	223	8	0	22	265	28	4	612
TOTAL VOLUMES :	55	2	124	0	134	6	71	0	39	2063	48	0	123	2476	180	19	5340
APPROACH %'s :	30.39%	1.10%	68.51%	0.00%	63.51%	2.84%	33.65%	0.00%	1.81%	95.95%	2.23%	0.00%	4.40%	88.49%	6.43%	0.68%	
PEAK HR :	04:45 PM - 05:45 PM																TOTAL
PEAK HR VOL :	38	2	54	0	61	3	38	0	20	1091	27	0	64	1253	89	11	2751
PEAK HR FACTOR :	0.731	0.500	0.900	0.000	0.897	0.375	0.731	0.000	0.833	0.863	0.844	0.000	0.889	0.911	0.890	0.688	0.904
	0.810				0.823				0.867				0.915				

National Data & Surveying Services Intersection Turning Movement Count

Location: Florida Gateway Dr & US Hwy 90
 City: Lake City
 Control: Signalized

Project ID: 21-120370-001
 Date: 9/2/2021

Data - Cars

NS/EW Streets:	Florida Gateway Dr				Florida Gateway Dr				US Hwy 90				US Hwy 90				TOTAL
	NORTHBOUND				SOUTHBOUND				EASTBOUND				WESTBOUND				
AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	TOTAL
	NL	NT	NR	NU	SL	ST	SR	SU	EL	ET	ER	EU	WL	WT	WR	WU	
7:00 AM	4	1	6	0	16	0	7	0	6	207	1	0	2	146	8	1	405
7:15 AM	1	0	16	0	6	2	8	0	6	343	2	0	4	175	9	1	573
7:30 AM	2	2	14	0	14	2	4	0	7	371	1	0	10	206	10	0	643
7:45 AM	1	1	15	0	11	1	5	0	12	342	4	0	10	229	17	2	650
8:00 AM	6	3	14	0	20	0	12	0	11	249	5	0	12	202	17	2	553
8:15 AM	2	1	16	0	22	0	9	0	9	205	5	0	16	186	16	0	487
8:30 AM	4	0	12	0	19	3	10	0	7	234	6	0	14	200	20	1	530
8:45 AM	6	2	12	0	17	3	7	0	6	203	6	0	7	191	10	1	471
TOTAL VOLUMES :	26	10	105	0	125	11	62	0	64	2154	30	0	75	1535	107	8	4312
APPROACH %'s :	18.44%	7.09%	74.47%	0.00%	63.13%	5.56%	31.31%	0.00%	2.85%	95.82%	1.33%	0.00%	4.35%	88.99%	6.20%	0.46%	
PEAK HR :	07:15 AM - 08:15 AM																TOTAL
PEAK HR VOL :	10	6	59	0	51	5	29	0	36	1305	12	0	36	812	53	5	2419
PEAK HR FACTOR :	0.417	0.500	0.922	0.000	0.638	0.625	0.604	0.000	0.750	0.879	0.600	0.000	0.750	0.886	0.779	0.625	0.930
	0.815				0.664				0.892				0.878				
NOON	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	TOTAL
	NL	NT	NR	NU	SL	ST	SR	SU	EL	ET	ER	EU	WL	WT	WR	WU	
12:00 PM	5	0	9	0	19	0	8	0	6	307	4	0	15	225	25	3	626
12:15 PM	7	0	17	0	27	0	7	0	7	231	4	0	17	258	22	4	601
12:30 PM	4	1	13	0	19	0	13	0	6	281	2	1	15	242	21	0	618
12:45 PM	7	0	9	0	18	1	12	0	7	226	4	0	15	293	32	2	626
1:00 PM	5	1	14	0	33	1	12	0	5	232	8	0	16	279	34	2	642
1:15 PM	8	1	13	0	27	1	5	0	4	246	5	0	17	281	21	2	631
1:30 PM	3	0	12	0	30	0	5	0	5	233	8	0	20	267	25	4	612
1:45 PM	2	1	17	0	23	1	4	0	3	247	1	0	10	279	20	0	608
TOTAL VOLUMES :	41	4	104	0	196	4	66	0	43	2003	36	1	125	2124	200	17	4964
APPROACH %'s :	27.52%	2.68%	69.80%	0.00%	73.68%	1.50%	24.81%	0.00%	2.06%	96.16%	1.73%	0.05%	5.07%	86.13%	8.11%	0.69%	
PEAK HR :	12:30 PM - 01:30 PM																TOTAL
PEAK HR VOL :	24	3	49	0	97	3	42	0	22	985	19	1	63	1095	108	6	2517
PEAK HR FACTOR :	0.750	0.750	0.875	0.000	0.735	0.750	0.808	0.000	0.786	0.876	0.594	0.250	0.926	0.934	0.794	0.750	0.980
	0.864				0.772				0.885				0.930				
PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	TOTAL
	NL	NT	NR	NU	SL	ST	SR	SU	EL	ET	ER	EU	WL	WT	WR	WU	
4:00 PM	4	0	14	0	16	0	7	0	3	262	3	0	11	336	29	3	688
4:15 PM	4	0	16	0	17	0	12	0	5	231	3	0	14	303	11	1	617
4:30 PM	4	0	12	0	21	2	6	0	7	233	7	0	10	284	21	0	607
4:45 PM	5	1	13	0	15	0	6	0	5	282	6	0	16	302	19	3	673
5:00 PM	11	1	15	0	15	2	13	0	6	312	6	0	13	330	19	4	747
5:15 PM	7	0	12	0	13	1	9	0	3	282	7	0	14	257	24	4	633
5:30 PM	13	0	13	0	17	0	10	0	6	196	8	0	18	340	25	0	646
5:45 PM	5	0	27	0	18	1	7	0	4	218	8	0	22	258	24	4	596
TOTAL VOLUMES :	53	2	122	0	132	6	70	0	39	2016	48	0	118	2410	172	19	5207
APPROACH %'s :	29.94%	1.13%	68.93%	0.00%	63.46%	2.88%	33.65%	0.00%	1.85%	95.86%	2.28%	0.00%	4.34%	88.64%	6.33%	0.70%	
PEAK HR :	04:45 PM - 05:45 PM																TOTAL
PEAK HR VOL :	36	2	53	0	60	3	38	0	20	1072	27	0	61	1229	87	11	2699
PEAK HR FACTOR :	0.692	0.500	0.883	0.000	0.882	0.375	0.731	0.000	0.833	0.859	0.844	0.000	0.847	0.904	0.870	0.688	0.903
	0.843				0.842				0.863				0.906				

National Data & Surveying Services Intersection Turning Movement Count

Location: Florida Gateway Dr & US Hwy 90
City: Lake City
Control: Signalized

Project ID: 21-120370-001
Date: 9/2/2021

Data - HT

NS/EW Streets:	Florida Gateway Dr				Florida Gateway Dr				US Hwy 90				US Hwy 90				
AM	NORTHBOUND				SOUTHBOUND				EASTBOUND				WESTBOUND				TOTAL
	NL	NT	NR	NU	SL	ST	SR	SU	EL	ET	ER	EU	WL	WT	WR	WU	
7:00 AM	0	0	1	0	0	0	0	0	0	8	0	0	0	3	0	0	12
7:15 AM	0	0	0	0	1	0	0	0	0	5	0	0	0	7	2	0	15
7:30 AM	0	0	0	0	0	0	0	0	1	7	0	0	0	9	1	0	18
7:45 AM	0	0	0	0	2	0	1	0	3	8	0	0	1	7	1	0	23
8:00 AM	0	0	0	0	3	0	0	0	0	6	0	0	0	7	0	0	16
8:15 AM	0	0	0	0	0	0	1	0	1	9	0	0	0	10	0	0	21
8:30 AM	0	0	0	0	0	0	1	0	0	5	0	0	2	11	1	0	20
8:45 AM	0	0	0	0	0	1	2	0	1	8	0	0	0	12	1	0	25
TOTAL VOLUMES :	0	0	1	0	6	1	5	0	6	56	0	0	3	66	6	0	150
APPROACH %'s :	0.00%	0.00%	100.00%	0.00%	50.00%	8.33%	41.67%	0.00%	9.68%	90.32%	0.00%	0.00%	4.00%	88.00%	8.00%	0.00%	
PEAK HR :	07:15 AM - 08:15 AM																TOTAL
PEAK HR VOL :	0	0	0	0	6	0	1	0	4	26	0	0	1	30	4	0	72
PEAK HR FACTOR :	0.000	0.000	0.000	0.000	0.500	0.000	0.250	0.000	0.333	0.813	0.000	0.000	0.250	0.833	0.500	0.000	0.783
					0.583				0.682				0.875				
NOON	NORTHBOUND				SOUTHBOUND				EASTBOUND				WESTBOUND				TOTAL
	NL	NT	NR	NU	SL	ST	SR	SU	EL	ET	ER	EU	WL	WT	WR	WU	
12:00 PM	0	0	1	0	4	0	0	0	0	11	0	0	1	5	1	0	23
12:15 PM	0	0	1	0	0	0	1	0	0	6	0	0	2	3	3	0	16
12:30 PM	0	1	0	0	2	0	0	0	0	9	1	0	0	10	0	0	23
12:45 PM	0	1	3	0	0	0	1	0	1	8	0	0	0	7	2	0	23
1:00 PM	2	0	2	0	1	0	1	0	0	4	1	0	0	12	0	0	23
1:15 PM	0	0	1	0	1	0	0	0	0	6	2	0	0	10	1	0	21
1:30 PM	0	0	0	0	0	0	0	0	0	10	1	0	1	6	1	0	19
1:45 PM	0	0	1	0	2	0	0	0	2	7	0	0	1	11	0	0	24
TOTAL VOLUMES :	2	2	9	0	10	0	3	0	3	61	5	0	5	64	8	0	172
APPROACH %'s :	15.38%	15.38%	69.23%	0.00%	76.92%	0.00%	23.08%	0.00%	4.35%	88.41%	7.25%	0.00%	6.49%	83.12%	10.39%	0.00%	
PEAK HR :	12:30 PM - 01:30 PM																TOTAL
PEAK HR VOL :	2	2	6	0	4	0	2	0	1	27	4	0	0	39	3	0	90
PEAK HR FACTOR :	0.250	0.500	0.500	0.000	0.500	0.000	0.500	0.000	0.250	0.750	0.500	0.000	0.000	0.813	0.375	0.000	0.978
	0.625				0.750				0.800				0.875				
PM	NORTHBOUND				SOUTHBOUND				EASTBOUND				WESTBOUND				TOTAL
	NL	NT	NR	NU	SL	ST	SR	SU	EL	ET	ER	EU	WL	WT	WR	WU	
4:00 PM	0	0	0	0	0	0	0	0	0	11	0	0	0	13	1	0	25
4:15 PM	0	0	0	0	1	0	0	0	0	6	0	0	1	11	1	0	20
4:30 PM	0	0	1	0	0	0	1	0	0	6	0	0	1	11	0	0	20
4:45 PM	0	0	1	0	0	0	0	0	0	5	0	0	1	8	0	0	15
5:00 PM	2	0	0	0	1	0	0	0	0	4	0	0	1	4	2	0	14
5:15 PM	0	0	0	0	0	0	0	0	0	8	0	0	1	8	0	0	17
5:30 PM	0	0	0	0	0	0	0	0	0	2	0	0	0	4	0	0	6
5:45 PM	0	0	0	0	0	0	0	0	0	5	0	0	0	7	4	0	16
TOTAL VOLUMES :	2	0	2	0	2	0	1	0	0	47	0	0	5	66	8	0	133
APPROACH %'s :	50.00%	0.00%	50.00%	0.00%	66.67%	0.00%	33.33%	0.00%	0.00%	100.00%	0.00%	0.00%	6.33%	83.54%	10.13%	0.00%	
PEAK HR :	04:45 PM - 05:45 PM																TOTAL
PEAK HR VOL :	2	0	1	0	1	0	0	0	0	19	0	0	3	24	2	0	52
PEAK HR FACTOR :	0.250	0.000	0.250	0.000	0.250	0.000	0.000	0.000	0.000	0.594	0.000	0.000	0.750	0.750	0.250	0.000	0.765
	0.375				0.250				0.594				0.806				

National Data & Surveying Services Intersection Turning Movement Count

Location: Florida Gateway Dr & US Hwy 90
 City: Lake City
 Control: Signalized

Project ID: 21-120370-001
 Date: 9/2/2021

Data - Bikes

NS/EW Streets:	Florida Gateway Dr				Florida Gateway Dr				US Hwy 90				US Hwy 90					
AM	NORTHBOUND				SOUTHBOUND				EASTBOUND				WESTBOUND				TOTAL	
	NL	NT	NR	NU	SL	ST	SR	SU	EL	ET	ER	EU	WL	WT	WR	WU		
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:15 AM	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	1
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1
7:45 AM	0	1	0	0	0	0	1	0	0	0	0	0	1	0	0	0	0	3
8:00 AM	0	0	0	0	1	0	0	0	0	1	0	0	0	0	0	0	0	2
8:15 AM	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	1
8:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
TOTAL VOLUMES :	0	1	0	0	1	0	1	0	0	3	0	0	1	1	0	0	0	8
APPROACH %'s :	0.00%	100.00%	0.00%	0.00%	50.00%	0.00%	50.00%	0.00%	0.00%	100.00%	0.00%	0.00%	50.00%	50.00%	0.00%	0.00%		
PEAK HR :	07:15 AM - 08:15 AM																TOTAL	
PEAK HR VOL :	0	1	0	0	1	0	1	0	0	2	0	0	1	1	0	0		7
PEAK HR FACTOR :	0.000	0.250	0.000	0.000	0.250	0.000	0.250	0.000	0.000	0.500	0.000	0.000	0.250	0.250	0.000	0.000		0.583
				0.250			0.500			0.500					0.500			
NOON	NORTHBOUND				SOUTHBOUND				EASTBOUND				WESTBOUND				TOTAL	
	NL	NT	NR	NU	SL	ST	SR	SU	EL	ET	ER	EU	WL	WT	WR	WU		
12:00 PM	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	1
12:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
12:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1
12:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
TOTAL VOLUMES :	0	0	0	0	0	0	0	0	0	1	0	0	0	1	0	0	0	2
APPROACH %'s :	0	0	0	0	0	0	0	0	0.00%	100.00%	0.00%	0.00%	0.00%	100.00%	0.00%	0.00%		
PEAK HR :	12:30 PM - 01:30 PM																TOTAL	
PEAK HR VOL :	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0		1
PEAK HR FACTOR :	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.250	0.000	0.000		0.250
														0.250				
PM	NORTHBOUND				SOUTHBOUND				EASTBOUND				WESTBOUND				TOTAL	
	NL	NT	NR	NU	SL	ST	SR	SU	EL	ET	ER	EU	WL	WT	WR	WU		
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1
4:15 PM	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	1
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1
5:30 PM	0	0	0	0	0	0	1	0	0	0	0	0	0	1	0	0	0	2
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
TOTAL VOLUMES :	0	0	0	0	0	0	2	0	0	1	0	0	0	3	0	0	0	6
APPROACH %'s :	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	100.00%	0.00%	0.00%	100.00%	0.00%	0.00%	0.00%	100.00%	0.00%	0.00%		
PEAK HR :	04:45 PM - 05:45 PM																TOTAL	
PEAK HR VOL :	0	0	0	0	0	0	2	0	0	0	0	0	0	2	0	0		4
PEAK HR FACTOR :	0.000	0.000	0.000	0.000	0.000	0.000	0.500	0.000	0.000	0.000	0.000	0.000	0.000	0.500	0.000	0.000		0.500
							0.500							0.500				

National Data & Surveying Services Intersection Turning Movement Count

Location: Florida Gateway Dr & US Hwy 90
City: Lake City

Project ID: 21-120370-001
Date: 9/2/2021

Data - Pedestrians (Crosswalks)

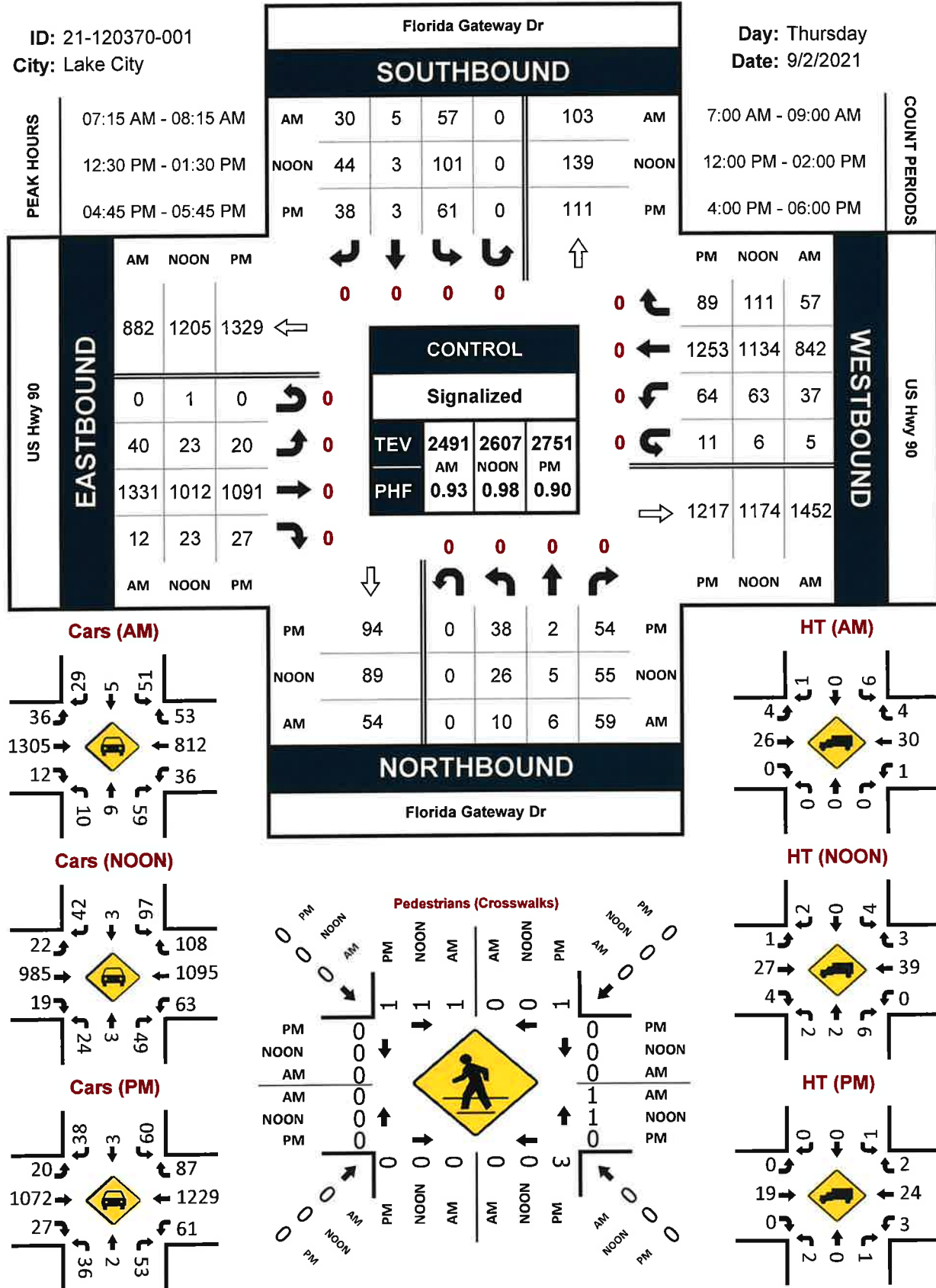
NS/EW Streets:	Florida Gateway Dr	Florida Gateway Dr	US Hwy 90	US Hwy 90					
AM	NORTH LEG		SOUTH LEG		EAST LEG	WEST LEG	TOTAL		
	EB	WB	EB	WB	NB	SB		NB	SB
7:00 AM	0	0	1	0	0	0	0	0	1
7:15 AM	0	0	0	0	0	0	0	0	0
7:30 AM	0	0	0	0	0	0	0	0	0
7:45 AM	1	0	0	0	0	0	0	0	1
8:00 AM	0	0	0	0	1	0	0	0	1
8:15 AM	0	0	0	0	0	0	0	0	0
8:30 AM	1	0	0	0	0	0	0	0	1
8:45 AM	0	0	0	0	0	0	0	0	0
TOTAL VOLUMES :	EB	WB	EB	WB	NB	SB	NB	SB	TOTAL
APPROACH %'s :	2	0	1	0	1	0	0	0	4
	100.00%	0.00%	100.00%	0.00%	100.00%	0.00%			
PEAK HR :	07:15 AM - 08:15 AM								TOTAL
PEAK HR VOL :	1	0	0	0	1	0	0	0	2
PEAK HR FACTOR :	0.250				0.250				0.500
	0.250				0.250				
NOON	NORTH LEG		SOUTH LEG		EAST LEG	WEST LEG	TOTAL		
	EB	WB	EB	WB	NB	SB		NB	SB
12:00 PM	0	1	0	0	0	0	0	0	1
12:15 PM	0	0	0	0	1	0	0	1	2
12:30 PM	0	0	0	0	0	0	0	0	0
12:45 PM	0	0	0	0	1	0	0	0	1
1:00 PM	0	0	0	0	0	0	0	0	0
1:15 PM	1	0	0	0	0	0	0	0	1
1:30 PM	0	0	0	0	0	0	0	0	0
1:45 PM	0	0	0	0	0	0	0	0	0
TOTAL VOLUMES :	EB	WB	EB	WB	NB	SB	NB	SB	TOTAL
APPROACH %'s :	1	1	0	0	2	0	0	1	5
	50.00%	50.00%			100.00%	0.00%	0.00%	100.00%	
PEAK HR :	12:30 PM - 01:30 PM								TOTAL
PEAK HR VOL :	1	0	0	0	1	0	0	0	2
PEAK HR FACTOR :	0.250				0.250				0.500
	0.250				0.250				
PM	NORTH LEG		SOUTH LEG		EAST LEG	WEST LEG	TOTAL		
	EB	WB	EB	WB	NB	SB		NB	SB
4:00 PM	0	0	0	1	0	0	0	0	1
4:15 PM	1	1	0	0	0	0	0	0	2
4:30 PM	1	0	2	1	0	0	0	0	4
4:45 PM	0	1	0	1	0	0	0	0	2
5:00 PM	0	0	0	2	0	0	0	0	2
5:15 PM	0	0	0	0	0	0	0	0	0
5:30 PM	1	0	0	0	0	0	0	0	1
5:45 PM	0	0	0	0	0	0	0	0	0
TOTAL VOLUMES :	EB	WB	EB	WB	NB	SB	NB	SB	TOTAL
APPROACH %'s :	3	2	2	5	0	0	0	0	12
	60.00%	40.00%	28.57%	71.43%					
PEAK HR :	04:45 PM - 05:45 PM								TOTAL
PEAK HR VOL :	1	1	0	3	0	0	0	0	5
PEAK HR FACTOR :	0.250	0.250		0.375					0.625
	0.500		0.375						

Florida Gateway Dr & US Hwy 90

Peak Hour Turning Movement Count

ID: 21-120370-001
City: Lake City

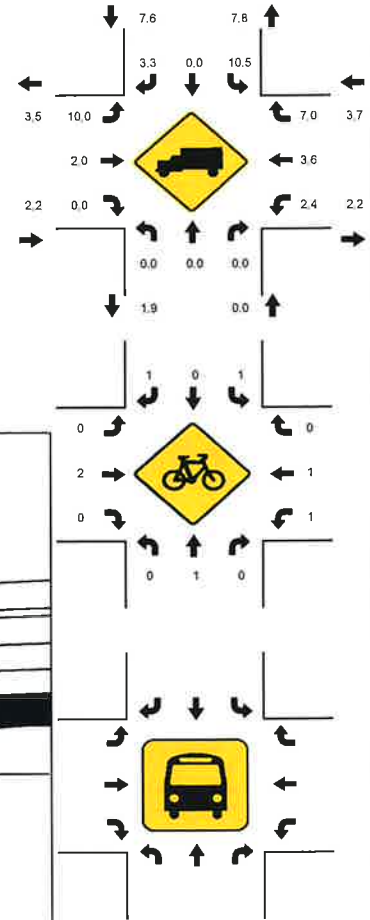
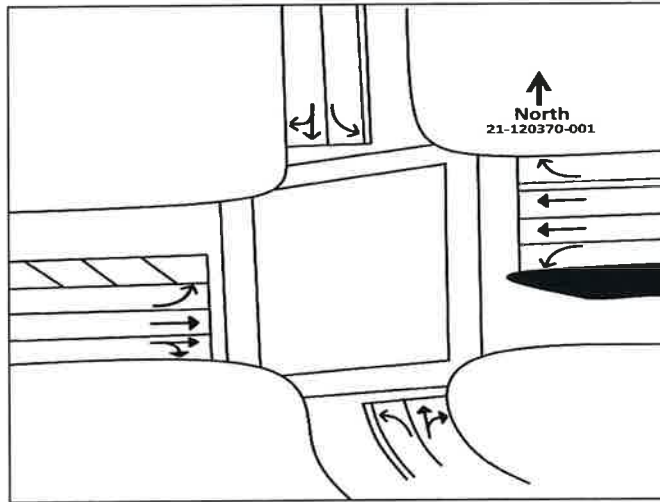
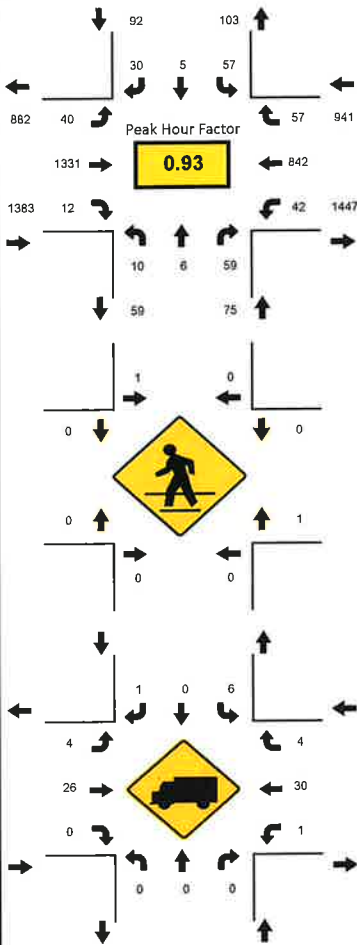
Day: Thursday
Date: 9/2/2021



Peak-Hour: 07:15 AM - 08:15 AM
 Peak 15-Minute: 07:45 AM - 08:00 AM

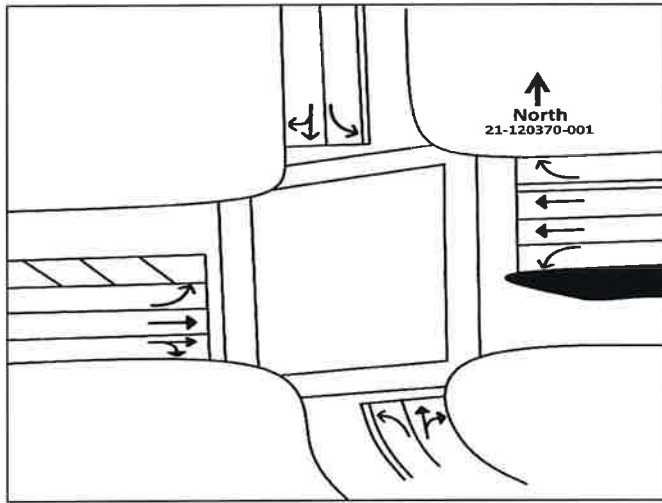
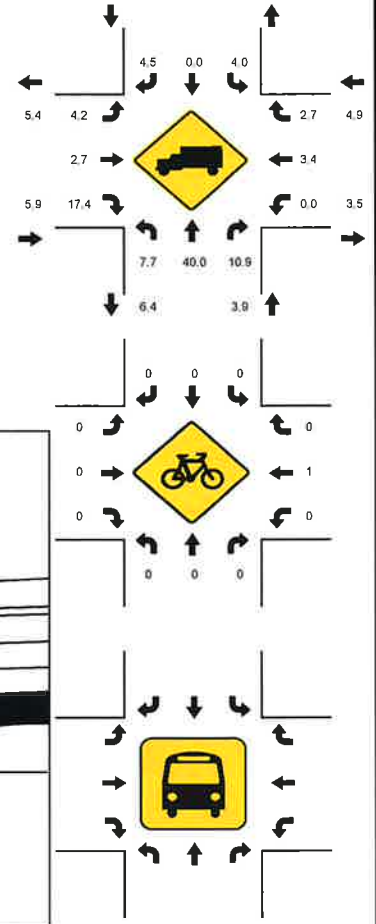
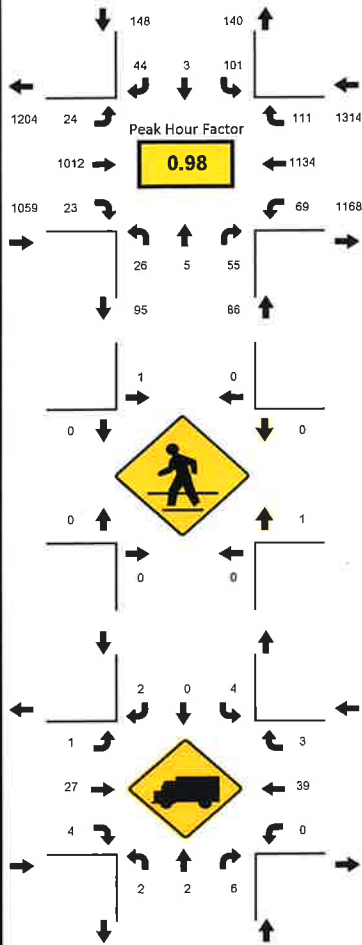


National Data & Surveying Services



15-Min Count Period Beginning At	Florida Gateway Dr Northbound					Florida Gateway Dr Southbound					US Hwy 90 Eastbound					US Hwy 90 Westbound					Total	Hourly Total
	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*		
07:00 AM	4	1	7	0		16	0	7	0		6	215	1	0		2	149	8	1		417	2339
07:15 AM	1	0	16	0		7	2	8	0		6	348	2	0		4	182	11	1		588	2491
07:30 AM	2	2	14	0		14	2	4	0		8	378	1	0		10	215	11	0		661	2411
07:45 AM	1	1	15	0		13	1	6	0		15	350	4	0		11	236	18	2		673	2300
08:00 AM	6	3	14	0		23	0	12	0		11	255	5	0		12	209	17	2		569	2123
08:15 AM	2	1	16	0		22	0	10	0		10	214	5	0		16	196	16	0		508	1554
08:30 AM	4	0	12	0		19	3	11	0		7	239	6	0		16	211	21	1		550	1046
08:45 AM	6	2	12	0		17	4	9	0		7	211	6	0		7	203	11	1		496	496
Peak 15-Min Flowrates	Northbound					Southbound					Eastbound					Westbound					Total	
All Vehicles	24	12	64	0		92	8	48	0		60	1512	20	0		48	944	72	8		2912	
Heavy Trucks	0	0	0	0		12	0	4	0		12	32	0	0		4	36	8	0		108	
Pedestrians			0					4				0					4				8	
Bicycles	0	4	0	0		4	0	4	0		0	4	0	0		4	4	0	0		24	
Buses																						
Stopped Buses																						

Peak-Hour: 12:30 PM - 01:30 PM
 Peak 15-Minute: 01:00 PM - 01:15 PM

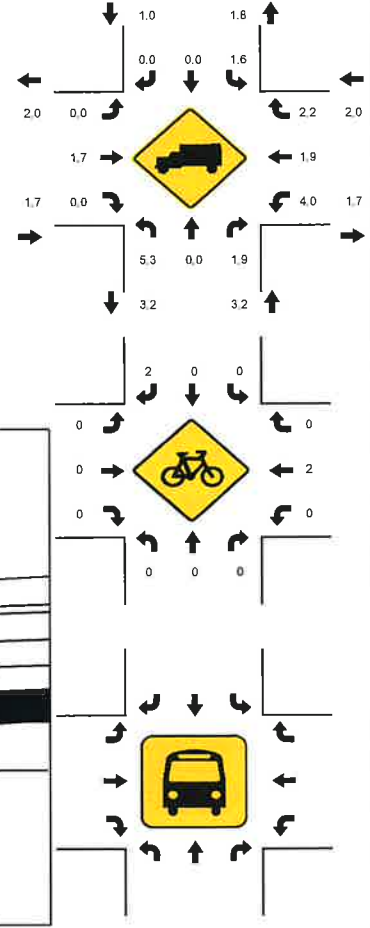
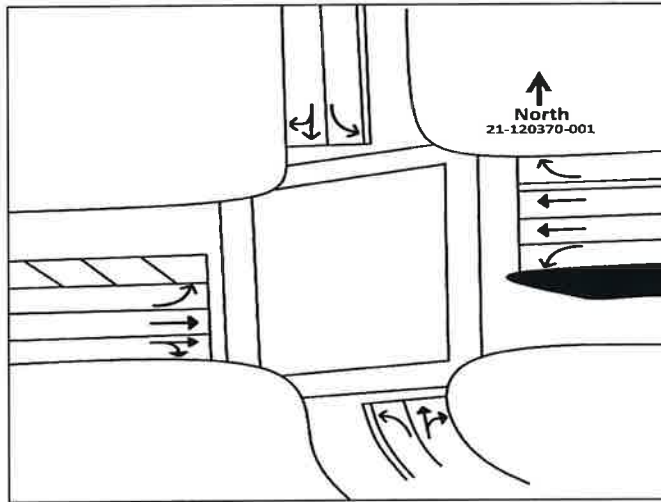
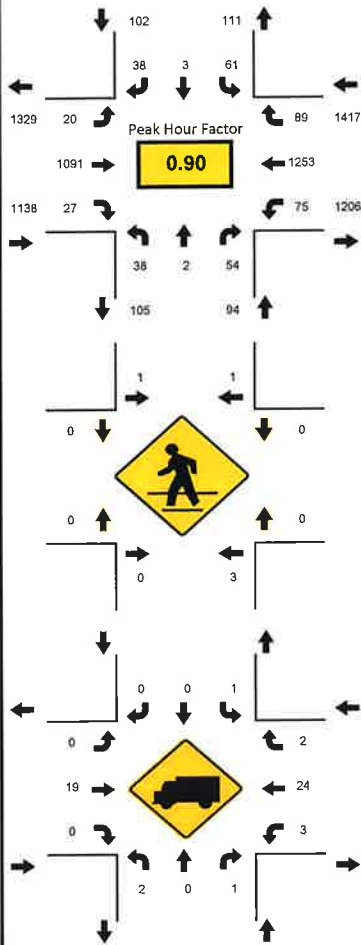


15-Min Count Period Beginning At	Florida Gateway Dr Northbound					Florida Gateway Dr Southbound					US Hwy 90 Eastbound					US Hwy 90 Westbound					Total	Hourly Total
	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*		
12:00 PM	5	0	10	0		23	0	8	0		6	318	4	0		16	230	26	3		649	2556
12:15 PM	7	0	18	0		27	0	8	0		7	237	4	0		19	261	25	4		617	2572
12:30 PM	4	2	13	0		21	0	13	0		6	290	3	1		15	252	21	0		641	2607
12:45 PM	7	1	12	0		18	1	13	0		8	234	4	0		15	300	34	2		649	2597
01:00 PM	7	1	16	0		34	1	13	0		5	236	9	0		16	291	34	2		665	2580
01:15 PM	8	1	14	0		28	1	5	0		4	252	7	0		17	291	22	2		652	1915
01:30 PM	3	0	12	0		30	0	5	0		5	243	9	0		21	273	26	4		631	1263
01:45 PM	2	1	18	0		25	1	4	0		5	254	1	0		11	290	20	0		632	632
Peak 15-Min Flowrates	Northbound					Southbound					Eastbound					Westbound					Total	
All Vehicles	32	8	64	0		136	4	52	0		32	1160	36	4		68	1200	136	8		2940	
Heavy Trucks	8	4	12	0		8	0	4	0		4	36	8	0		0	48	8	0		140	
Pedestrians		0					4					0					4				8	
Bicycles	0	0	0	0		0	0	0	0		0	0	0	0		0	4	0	0		4	
Buses																						
Stopped Buses																						

Peak-Hour: 04:45 PM - 05:45 PM
 Peak 15-Minute: 05:00 PM - 05:15 PM



National Data & Surveying Services



15-Min Count Period Beginning At	Florida Gateway Dr Northbound					Florida Gateway Dr Southbound					US Hwy 90 Eastbound					US Hwy 90 Westbound					Total	Hourly Total
	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*		
04:00 PM	4	0	14	0		16	0	7	0		3	273	3	0		11	349	30	3		713	2665
04:15 PM	4	0	16	0		18	0	12	0		5	237	3	0		15	314	12	1		637	2713
04:30 PM	4	0	13	0		21	2	7	0		7	239	7	0		11	295	21	0		627	2726
04:45 PM	5	1	14	0		15	0	6	0		5	287	6	0		17	310	19	3		688	2751
05:00 PM	13	1	15	0		16	2	13	0		6	316	6	0		14	334	21	4		761	2675
05:15 PM	7	0	12	0		13	1	9	0		3	290	7	0		15	265	24	4		650	1914
05:30 PM	13	0	13	0		17	0	10	0		6	198	8	0		18	344	25	0		652	1264
05:45 PM	5	0	27	0		18	1	7	0		4	223	8	0		22	265	28	4		612	612
Peak 15-Min Flowrates	Northbound					Southbound					Eastbound					Westbound					Total	
All Vehicles	52	4	60	0		68	8	52	0		24	1264	32	0		72	1376	100	16		3128	
Heavy Trucks	8	0	4	0		4	0	0	0		0	32	0	0		4	32	8	0		92	
Pedestrians	8					4					0					0					12	
Bicycles	0	0	0	0		0	0	4	0		0	0	0	0		0	4	0	0		8	
Buses																						
Stopped Buses																						

2019 PEAK SEASON FACTOR CATEGORY REPORT - REPORT TYPE: ALL
 CATEGORY: 2900 COLUMBIA COUNTYWIDE

WEEK	DATES	SF	MOCF: 0.97 PSCF
1	01/01/2019 - 01/05/2019	1.02	1.05
2	01/06/2019 - 01/12/2019	1.05	1.08
3	01/13/2019 - 01/19/2019	1.08	1.11
4	01/20/2019 - 01/26/2019	1.06	1.09
5	01/27/2019 - 02/02/2019	1.04	1.07
6	02/03/2019 - 02/09/2019	1.03	1.06
7	02/10/2019 - 02/16/2019	1.01	1.04
8	02/17/2019 - 02/23/2019	1.00	1.03
9	02/24/2019 - 03/02/2019	0.99	1.02
10	03/03/2019 - 03/09/2019	0.98	1.01
*11	03/10/2019 - 03/16/2019	0.97	1.00
*12	03/17/2019 - 03/23/2019	0.97	1.00
*13	03/24/2019 - 03/30/2019	0.97	1.00
*14	03/31/2019 - 04/06/2019	0.97	1.00
*15	04/07/2019 - 04/13/2019	0.97	1.00
*16	04/14/2019 - 04/20/2019	0.97	1.00
*17	04/21/2019 - 04/27/2019	0.97	1.00
*18	04/28/2019 - 05/04/2019	0.97	1.00
*19	05/05/2019 - 05/11/2019	0.97	1.00
*20	05/12/2019 - 05/18/2019	0.97	1.00
*21	05/19/2019 - 05/25/2019	0.97	1.00
*22	05/26/2019 - 06/01/2019	0.97	1.00
*23	06/02/2019 - 06/08/2019	0.98	1.01
24	06/09/2019 - 06/15/2019	0.98	1.01
25	06/16/2019 - 06/22/2019	0.99	1.02
26	06/23/2019 - 06/29/2019	1.00	1.03
27	06/30/2019 - 07/06/2019	1.00	1.03
28	07/07/2019 - 07/13/2019	1.01	1.04
29	07/14/2019 - 07/20/2019	1.02	1.05
30	07/21/2019 - 07/27/2019	1.02	1.05
31	07/28/2019 - 08/03/2019	1.01	1.04
32	08/04/2019 - 08/10/2019	1.01	1.04
33	08/11/2019 - 08/17/2019	1.01	1.04
34	08/18/2019 - 08/24/2019	1.01	1.04
35	08/25/2019 - 08/31/2019	1.01	1.04
36	09/01/2019 - 09/07/2019	1.00	1.03
37	09/08/2019 - 09/14/2019	1.00	1.03
38	09/15/2019 - 09/21/2019	1.00	1.03
39	09/22/2019 - 09/28/2019	1.00	1.03
40	09/29/2019 - 10/05/2019	1.00	1.03
41	10/06/2019 - 10/12/2019	1.00	1.03
42	10/13/2019 - 10/19/2019	1.00	1.03
43	10/20/2019 - 10/26/2019	1.01	1.04
44	10/27/2019 - 11/02/2019	1.01	1.04
45	11/03/2019 - 11/09/2019	1.02	1.05
46	11/10/2019 - 11/16/2019	1.02	1.05
47	11/17/2019 - 11/23/2019	1.02	1.05
48	11/24/2019 - 11/30/2019	1.02	1.05
49	12/01/2019 - 12/07/2019	1.02	1.05
50	12/08/2019 - 12/14/2019	1.02	1.05
51	12/15/2019 - 12/21/2019	1.02	1.05
52	12/22/2019 - 12/28/2019	1.05	1.08
53	12/29/2019 - 12/31/2019	1.08	1.11

* PEAK SEASON

14-FEB-2020 15:39:21

830UPD

2_2900_PKSEASON.TXT

Location Details			
Signal ID:	1002	Date:	November 20, 2021
Major Street:	US 90	Orientation:	E-W
Minor Street:	FL Gateway Dr	Orientation:	N-S

Controller Timings (seconds)

Movement # (Controller Phase Ø)	Ø1	Ø2	Ø3	Ø4	Ø5	Ø6	Ø7	Ø8	Ø9	Ø10	Ø11	Ø12	Ø13	Ø14	Ø15	Ø16	Notes
Direction	EBLT	WB		NB	WBLT	EB		SB									
Turn Type	Prot Perm				Prot Perm												
Min Green	5	15		7	5	15		7									
Ext	3.0	4.0		3.0	3.0	4.0		3.0									
Yellow	4.8	4.9		3.8	4.9	4.9		3.8									
All Red	2.0	2.0		2.0	2.0	2.0		2.0									
Max I	15	75		20	15	75		20									
Max II																	
Walk		7		7		7		7									
Flashing Don't Walk		18		29		18		22									
Detector Memory																	
Det. Switching to:	Ø6				Ø2												
Recall		MIN				MIN											
CNA																	

Coordination Timings (seconds)

Pattern	C-S-O	Cycle Length	Splits																Offset	Seq	Coord Ø
			Ø1	Ø2	Ø3	Ø4	Ø5	Ø6	Ø7	Ø8	Ø9	Ø10	Ø11	Ø12	Ø13	Ø14	Ø15	Ø16			
1		130	15	91 MAX		24	16	90 MAX		24								24	1	2	
2		130	15	70 MAX		45	20	65 MAX		45								15	1	2	
3		150	15	88 MAX		47	25	78 MAX		47								20	1	2	
4		110	16	64 MAX		30	23	57 MAX		30								18	1	2	
5		100	15	59 MAX		26	17	57 MAX		26								22	1	2	
6		140	15	75 MAX		50	23	67 MAX		50								7	1	2	
7		110	17	58 MAX		35	18	57 MAX		35								63	1	2	
8		100	15	59 MAX		26	17	57 MAX		26								22	1	2	
9		140	15	75 MAX		50	23	67 MAX		50								7	1	2	
10		110	17	58 MAX		35	18	57 MAX		35								63	1	2	

Offset Reference Point	Phase Mode
End of Green of first through movement	STD8

- Notes:
- 1) Use 'Max I' during FREE Operation.
 - 2) Program phase restriction to omit Ø1 during Ø2 green and omit Ø5 during Ø6 green.

SEQ 1		
Ring - 1	1	2
Ring - 2	5	6

Signal ID:	1002
Major Street:	US 90
Minor Street:	FL Gateway Dr

Day Plans

Monday-Thursday Day Plan 1			
Hr	Min	Patt	Cycl
00	00	254	Free
6	30	1	130
10	00	2	130
15	00	3	150
18	30	4	110
21	00	254	Free

Saturday Day Plan 2			
Hr	Min	Patt	Cycl
00	00	254	Free
8	00	5	100
10	00	6	140
17	00	7	110
22	00	254	Free

Sunday Day Plan 3			
Hr	Min	Patt	Cycl
00	00	254	Free
9	30	8	100
11	00	9	140
16	30	10	110
21	00	254	Free

Friday Day Plan 4			
Hr	Min	Patt	Cycl
00	00	254	Free
6	30	1	130
10	00	2	130
11	30	3	150
19	00	4	110
22	00	254	Free

Day Plan 5			
Hr	Min	Patt	Cycl

Day Plan 6			
Hr	Min	Patt	Cycl

Day Plan 7			
Hr	Min	Patt	Cycl

Day Plan 8			
Hr	Min	Patt	Cycl

Patt	Force Mode	Alt Opt Table	Alt Time Table	Coord Max Plan	Alt Time Table Max Values (Seconds)															
					Ø1	Ø2	Ø3	Ø4	Ø5	Ø6	Ø7	Ø8	Ø9	Ø10	Ø11	Ø12	Ø13	Ø14	Ø15	Ø16
1	FIXED	None	None	Max Inh																
2	FIXED	None	None	Max Inh																
3	FIXED	None	None	Max Inh																
4	FIXED	None	None	Max Inh																
5	FIXED	None	None	Max Inh																
6	FIXED	None	None	Max Inh																
7	FIXED	None	None	Max Inh																
8	FIXED	None	None	Max Inh																
9	FIXED	None	None	Max Inh																
10	FIXED	None	None	Max Inh																

APPENDIX C
Intersection Volume Development Worksheets

TRAFFIC VOLUMES AT STUDY INTERSECTIONS

INTERSECTION: US 90 & Centurion Ct/Florida Gateway Dr
COUNT DATE: September 2, 2021
AM PEAK HOUR FACTOR: 0.93
PM PEAK HOUR FACTOR: 0.9

"AM EXISTING TRAFFIC"	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
AM Raw Turning Movements		40	1,331	12	5	37	842	57		10	6	59		57	5	30
Peak Season Conversion Factor	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03

AM EXISTING CONDITIONS		41	1,371	12	5	38	867	59		10	6	61		59	5	31
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"PM EXISTING TRAFFIC"	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
PM Raw Turning Movements		20	1,091	27	11	64	1,253	89		38	2	54		61	3	38
Peak Season Conversion Factor	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03

PM EXISTING CONDITIONS		21	1,124	28	11	66	1,291	92		39	2	56		63	3	39
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"AM BACKGROUND TRAFFIC"	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Years To Buildout	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2
Yearly Growth Rate	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%
AM BACKGROUND TRAFFIC GROWTH		2	58	1	0	2	37	3		0	0	3		3	0	1

AM NON-PROJECT TRAFFIC		43	1,429	13	5	40	904	62		10	6	64		62	5	32
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"PM BACKGROUND TRAFFIC"	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Years To Buildout	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2
Yearly Growth Rate	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%
PM BACKGROUND TRAFFIC GROWTH		1	48	1	0	3	55	4		2	0	2		3	0	2

PM NON-PROJECT TRAFFIC		22	1,172	29	11	69	1,346	96		41	2	58		66	3	41
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"AM PROJECT DISTRIBUTION"		EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
LAND USE	TYPE																
Pass-By Distribution	Entering		50.0%	-50.0%				-50.0%	50.0%								
	Exiting														50.0%		50.0%
Net New Distribution	Entering		25.0%					75.0%									
	Exiting													75.0%			25.0%

"PM PROJECT DISTRIBUTION"		EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
LAND USE	TYPE																
Pass-By Distribution	Entering		50.0%	-50.0%				-50.0%	50.0%								
	Exiting														50.0%		50.0%
Net New Distribution	Entering		25.0%					75.0%									
	Exiting													75.0%			25.0%

"AM PROJECT TRAFFIC"		EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
LAND USE	TYPE																
Project Trips	Pass - By		12	-12				-12	12						12		12
	Net New		2					6							6		2
AM TOTAL PROJECT TRAFFIC			14	-12	0	0	0	-12	18		0	0	0		18	0	14

AM TOTAL TRAFFIC		57	1,417	13	5	40	892	80		10	6	64		80	5	46
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"PM PROJECT TRAFFIC"		EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
LAND USE	TYPE																
Project Trips	Pass - By		13	-13				-14	14						13		14
	Net New		2					7							7		2
PM TOTAL PROJECT TRAFFIC			15	-13	0	0	0	-14	21		0	0	0		20	0	16

PM TOTAL TRAFFIC		37	1,159	29	11	69	1,332	117		41	2	58		86	3	57
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APPENDIX D
Synchro Output Reports

Lanes, Volumes, Timings
 1: SW Florida Gateway Dr/Centurion Ct & US 90

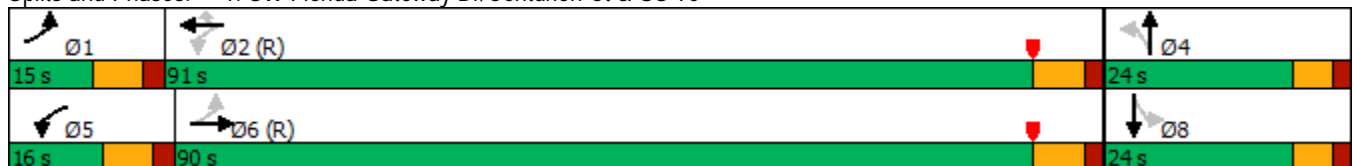
Circle K - I-75 & US 90
 Existing (2021) Conditions, AM Peak Hour

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	41	1371	12	43	867	59	10	6	61	59	5	31
Future Volume (vph)	41	1371	12	43	867	59	10	6	61	59	5	31
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	150		0	250		125	50		0	0		110
Storage Lanes	1		0	1		1	1		0	1		1
Taper Length (ft)	25			50			25			25		
Right Turn on Red			Yes			Yes			Yes			Yes
Link Speed (mph)		45			45			30			30	
Link Distance (ft)		398			433			442			282	
Travel Time (s)		6.0			6.6			10.0			6.4	
Confl. Peds. (#/hr)	1						1			1	1	
Confl. Bikes (#/hr)			2				1			1		
Peak Hour Factor	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93
Heavy Vehicles (%)	2%	2%	2%	4%	4%	4%	2%	2%	2%	8%	8%	8%
Shared Lane Traffic (%)												
Turn Type	pm+pt	NA		pm+pt	NA	Perm	Perm	NA		Perm	NA	
Protected Phases	1	6		5	2			4				8
Permitted Phases	6			2		2	4			8		
Detector Phase	1	6		5	2	2	4	4		8		8
Switch Phase												
Minimum Initial (s)	5.0	15.0		5.0	15.0	15.0	7.0	7.0		7.0	7.0	
Minimum Split (s)	11.8	31.9		11.9	31.9	31.9	41.8	41.8		34.8	34.8	
Total Split (s)	15.0	90.0		16.0	91.0	91.0	24.0	24.0		24.0	24.0	
Total Split (%)	11.5%	69.2%		12.3%	70.0%	70.0%	18.5%	18.5%		18.5%	18.5%	
Yellow Time (s)	4.8	4.9		4.9	4.9	4.9	3.8	3.8		3.8	3.8	
All-Red Time (s)	2.0	2.0		2.0	2.0	2.0	2.0	2.0		2.0	2.0	
Lost Time Adjust (s)	0.0	0.0		0.0	0.0	0.0	0.0	0.0		0.0	0.0	
Total Lost Time (s)	6.8	6.9		6.9	6.9	6.9	5.8	5.8		5.8	5.8	
Lead/Lag	Lead	Lag		Lead	Lag	Lag						
Lead-Lag Optimize?	Yes	Yes		Yes	Yes	Yes						
Recall Mode	None	C-Min		None	C-Min	C-Min	None	None		None	None	

Intersection Summary

Area Type: Other
 Cycle Length: 130
 Actuated Cycle Length: 130
 Offset: 24 (18%), Referenced to phase 2:WBTL and 6:EBTL, Start of Yellow
 Natural Cycle: 100
 Control Type: Actuated-Coordinated

Splits and Phases: 1: SW Florida Gateway Dr/Centurion Ct & US 90



HCM 6th Signalized Intersection Summary
 1: SW Florida Gateway Dr/Centurion Ct & US 90

Circle K - I-75 & US 90
 Existing (2021) Conditions, AM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗	↗	↖	↗		↖	↗	
Traffic Volume (veh/h)	41	1371	12	43	867	59	10	6	61	59	5	31
Future Volume (veh/h)	41	1371	12	43	867	59	10	6	61	59	5	31
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		0.98	1.00		0.98	1.00		0.98	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1841	1841	1841	1870	1870	1870	1781	1781	1781
Adj Flow Rate, veh/h	44	1474	13	46	932	63	11	6	66	63	5	33
Peak Hour Factor	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93
Percent Heavy Veh, %	2	2	2	4	4	4	2	2	2	8	8	8
Cap, veh/h	453	2533	22	296	2485	1085	173	14	158	139	22	145
Arrive On Green	0.03	0.70	0.70	0.04	0.71	0.71	0.11	0.11	0.11	0.11	0.11	0.11
Sat Flow, veh/h	1781	3609	32	1753	3497	1527	1366	132	1449	1263	202	1335
Grp Volume(v), veh/h	44	725	762	46	932	63	11	0	72	63	0	38
Grp Sat Flow(s),veh/h/ln	1781	1777	1864	1753	1749	1527	1366	0	1580	1263	0	1537
Q Serve(g_s), s	0.9	26.7	26.8	0.9	13.7	1.6	1.0	0.0	5.5	6.4	0.0	2.9
Cycle Q Clear(g_c), s	0.9	26.7	26.8	0.9	13.7	1.6	3.9	0.0	5.5	11.9	0.0	2.9
Prop In Lane	1.00		0.02	1.00		1.00	1.00		0.92	1.00		0.87
Lane Grp Cap(c), veh/h	453	1247	1308	296	2485	1085	173	0	172	139	0	167
V/C Ratio(X)	0.10	0.58	0.58	0.16	0.38	0.06	0.06	0.00	0.42	0.45	0.00	0.23
Avail Cap(c_a), veh/h	510	1247	1308	351	2485	1085	216	0	221	178	0	215
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(l)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	5.4	9.8	9.8	7.7	7.4	5.7	54.7	0.0	54.1	59.6	0.0	52.9
Incr Delay (d2), s/veh	0.1	2.0	1.9	0.2	0.4	0.1	0.2	0.0	1.6	2.3	0.0	0.7
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	0.5	14.6	15.2	0.5	8.0	0.9	0.6	0.0	4.1	3.8	0.0	2.1
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	5.5	11.7	11.7	7.9	7.9	5.8	54.9	0.0	55.7	61.9	0.0	53.6
LnGrp LOS	A	B	B	A	A	A	D	A	E	E	A	D
Approach Vol, veh/h		1531			1041			83			101	
Approach Delay, s/veh		11.5			7.7			55.6			58.8	
Approach LOS		B			A			E			E	
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	10.8	99.3		19.9	11.9	98.2		19.9				
Change Period (Y+Rc), s	6.8	6.9		* 5.8	6.9	6.9		* 5.8				
Max Green Setting (Gmax), s	8.2	84.1		* 18	9.1	83.1		* 18				
Max Q Clear Time (g_c+I1), s	2.9	15.7		7.5	2.9	28.8		13.9				
Green Ext Time (p_c), s	0.0	7.7		0.2	0.0	13.9		0.1				

Intersection Summary

HCM 6th Ctrl Delay	13.2
HCM 6th LOS	B

Notes

- User approved pedestrian interval to be less than phase max green.
- * HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Lanes, Volumes, Timings
 1: SW Florida Gateway Dr/Centurion Ct & US 90

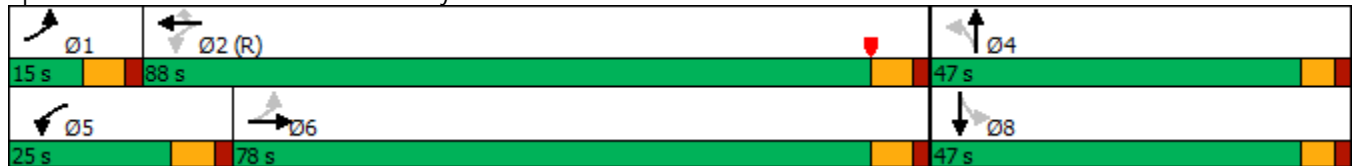
Circle K - I-75 & US 90
 Existing (2021) Conditions, PM Peak Hour

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	21	1124	28	77	1291	92	39	2	56	63	3	39
Future Volume (vph)	21	1124	28	77	1291	92	39	2	56	63	3	39
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	150		0	250		125	50		0	0		110
Storage Lanes	1		0	1		1	1		0	1		1
Taper Length (ft)	25			50			25			25		
Right Turn on Red			Yes			Yes			Yes			Yes
Link Speed (mph)		45			45			30				30
Link Distance (ft)		398			433			442				282
Travel Time (s)		6.0			6.6			10.0				6.4
Confl. Peds. (#/hr)	2		3	3			2					
Confl. Bikes (#/hr)							2					
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90
Heavy Vehicles (%)	2%	2%	2%	2%	2%	2%	3%	3%	3%	2%	2%	2%
Shared Lane Traffic (%)												
Turn Type	pm+pt	NA		pm+pt	NA	Perm	Perm	NA		Perm	NA	
Protected Phases	1	6		5	2			4				8
Permitted Phases	6			2		2	4			8		
Detector Phase	1	6		5	2	2	4	4		8		8
Switch Phase												
Minimum Initial (s)	5.0	15.0		5.0	15.0	15.0	7.0	7.0		7.0		7.0
Minimum Split (s)	11.8	31.9		11.9	31.9	31.9	41.8	41.8		34.8		34.8
Total Split (s)	15.0	78.0		25.0	88.0	88.0	47.0	47.0		47.0		47.0
Total Split (%)	10.0%	52.0%		16.7%	58.7%	58.7%	31.3%	31.3%		31.3%		31.3%
Yellow Time (s)	4.8	4.9		4.9	4.9	4.9	3.8	3.8		3.8		3.8
All-Red Time (s)	2.0	2.0		2.0	2.0	2.0	2.0	2.0		2.0		2.0
Lost Time Adjust (s)	0.0	0.0		0.0	0.0	0.0	0.0	0.0		0.0		0.0
Total Lost Time (s)	6.8	6.9		6.9	6.9	6.9	5.8	5.8		5.8		5.8
Lead/Lag	Lead	Lag		Lead	Lag	Lag						
Lead-Lag Optimize?	Yes	Yes		Yes	Yes	Yes						
Recall Mode	None	Max		None	C-Max	C-Max	None	None		None		None

Intersection Summary

Area Type: Other
 Cycle Length: 150
 Actuated Cycle Length: 150
 Offset: 20 (13%), Referenced to phase 2:WBTL, Start of Yellow
 Natural Cycle: 100
 Control Type: Actuated-Coordinated

Splits and Phases: 1: SW Florida Gateway Dr/Centurion Ct & US 90



HCM 6th Signalized Intersection Summary
 1: SW Florida Gateway Dr/Centurion Ct & US 90

Circle K - I-75 & US 90
 Existing (2021) Conditions, PM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗	↗	↖	↗		↖	↗	
Traffic Volume (veh/h)	21	1124	28	77	1291	92	39	2	56	63	3	39
Future Volume (veh/h)	21	1124	28	77	1291	92	39	2	56	63	3	39
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		0.98	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1856	1856	1856	1870	1870	1870
Adj Flow Rate, veh/h	23	1249	31	86	1434	102	43	2	62	70	3	43
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90
Percent Heavy Veh, %	2	2	2	2	2	2	3	3	3	2	2	2
Cap, veh/h	278	2589	64	360	2644	1152	154	5	161	138	11	158
Arrive On Green	0.02	0.73	0.73	0.03	0.74	0.74	0.11	0.11	0.11	0.11	0.11	0.11
Sat Flow, veh/h	1781	3543	88	1781	3554	1549	1349	49	1531	1338	104	1497
Grp Volume(v), veh/h	23	626	654	86	1434	102	43	0	64	70	0	46
Grp Sat Flow(s),veh/h/ln	1781	1777	1854	1781	1777	1549	1349	0	1580	1338	0	1601
Q Serve(g_s), s	0.5	22.0	22.0	1.8	26.0	2.7	4.5	0.0	5.7	7.7	0.0	4.0
Cycle Q Clear(g_c), s	0.5	22.0	22.0	1.8	26.0	2.7	8.5	0.0	5.7	13.4	0.0	4.0
Prop In Lane	1.00		0.05	1.00		1.00	1.00		0.97	1.00		0.93
Lane Grp Cap(c), veh/h	278	1298	1355	360	2644	1152	154	0	167	138	0	169
V/C Ratio(X)	0.08	0.48	0.48	0.24	0.54	0.09	0.28	0.00	0.38	0.51	0.00	0.27
Avail Cap(c_a), veh/h	338	1298	1355	515	2644	1152	383	0	434	365	0	440
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	6.8	8.4	8.4	6.4	8.2	5.3	65.7	0.0	62.6	68.8	0.0	61.8
Incr Delay (d2), s/veh	0.1	1.3	1.2	0.3	0.8	0.2	1.0	0.0	1.4	2.8	0.0	0.9
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	0.3	12.5	12.9	1.1	13.7	1.5	2.9	0.0	4.2	5.0	0.0	3.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	6.9	9.7	9.6	6.7	9.0	5.4	66.7	0.0	64.0	71.6	0.0	62.7
LnGrp LOS	A	A	A	A	A	A	E	A	E	E	A	E
Approach Vol, veh/h		1303			1622			107				116
Approach Delay, s/veh		9.6			8.7			65.1				68.1
Approach LOS		A			A			E				E
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	9.9	118.5		21.6	11.9	116.5		21.6				
Change Period (Y+Rc), s	6.8	6.9		* 5.8	6.9	6.9		* 5.8				
Max Green Setting (Gmax), s	8.2	81.1		* 41	18.1	71.1		* 41				
Max Q Clear Time (g_c+I1), s	2.5	28.0		10.5	3.8	24.0		15.4				
Green Ext Time (p_c), s	0.0	15.2		0.5	0.1	10.3		0.4				

Intersection Summary

HCM 6th Ctrl Delay	13.2
HCM 6th LOS	B

Notes

- User approved pedestrian interval to be less than phase max green.
- * HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Lanes, Volumes, Timings
 1: SW Florida Gateway Dr/Centurion Ct & US 90

Circle K - I-75 & US 90
 Background (2023) Conditions, AM Peak Hour



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↕		↖	↕	↗	↖	↗		↖	↗	
Traffic Volume (vph)	43	1429	13	45	904	62	10	6	64	62	5	32
Future Volume (vph)	43	1429	13	45	904	62	10	6	64	62	5	32
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	150		0	250		125	50		0	0		110
Storage Lanes	1		0	1		1	1		0	1		1
Taper Length (ft)	25			50			25			25		
Right Turn on Red			Yes			Yes			Yes			Yes
Link Speed (mph)		45			45			30				30
Link Distance (ft)		398			433			442				282
Travel Time (s)		6.0			6.6			10.0				6.4
Confl. Peds. (#/hr)	1						1			1	1	
Confl. Bikes (#/hr)			2						1			
Peak Hour Factor	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93
Heavy Vehicles (%)	2%	2%	2%	4%	4%	4%	2%	2%	2%	8%	8%	8%
Shared Lane Traffic (%)												
Turn Type	pm+pt	NA		pm+pt	NA	Perm	Perm	NA		Perm	NA	
Protected Phases	1	6		5	2			4				8
Permitted Phases	6			2		2	4			8		
Detector Phase	1	6		5	2	2	4	4		8		8
Switch Phase												
Minimum Initial (s)	5.0	15.0		5.0	15.0	15.0	7.0	7.0		7.0		7.0
Minimum Split (s)	11.8	31.9		11.9	31.9	31.9	41.8	41.8		34.8		34.8
Total Split (s)	15.0	90.0		16.0	91.0	91.0	24.0	24.0		24.0		24.0
Total Split (%)	11.5%	69.2%		12.3%	70.0%	70.0%	18.5%	18.5%		18.5%		18.5%
Yellow Time (s)	4.8	4.9		4.9	4.9	4.9	3.8	3.8		3.8		3.8
All-Red Time (s)	2.0	2.0		2.0	2.0	2.0	2.0	2.0		2.0		2.0
Lost Time Adjust (s)	0.0	0.0		0.0	0.0	0.0	0.0	0.0		0.0		0.0
Total Lost Time (s)	6.8	6.9		6.9	6.9	6.9	5.8	5.8		5.8		5.8
Lead/Lag	Lead	Lag		Lead	Lag	Lag						
Lead-Lag Optimize?	Yes	Yes		Yes	Yes	Yes						
Recall Mode	None	C-Min		None	C-Min	C-Min	None	None		None		None

Intersection Summary

Area Type: Other
 Cycle Length: 130
 Actuated Cycle Length: 130
 Offset: 24 (18%), Referenced to phase 2:WBTL and 6:EBTL, Start of Yellow
 Natural Cycle: 100
 Control Type: Actuated-Coordinated

Splits and Phases: 1: SW Florida Gateway Dr/Centurion Ct & US 90



HCM 6th Signalized Intersection Summary
 1: SW Florida Gateway Dr/Centurion Ct & US 90

Circle K - I-75 & US 90
 Background (2023) Conditions, AM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗	↗	↖	↗		↖	↗	
Traffic Volume (veh/h)	43	1429	13	45	904	62	10	6	64	62	5	32
Future Volume (veh/h)	43	1429	13	45	904	62	10	6	64	62	5	32
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		0.98	1.00		1.00	1.00		0.98	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1841	1841	1841	1870	1870	1870	1781	1781	1781
Adj Flow Rate, veh/h	46	1537	14	48	972	67	11	6	69	67	5	34
Peak Hour Factor	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93
Percent Heavy Veh, %	2	2	2	4	4	4	2	2	2	8	8	8
Cap, veh/h	432	2515	23	278	2467	1099	179	14	165	143	22	152
Arrive On Green	0.03	0.70	0.70	0.04	0.71	0.71	0.11	0.11	0.11	0.11	0.11	0.11
Sat Flow, veh/h	1781	3608	33	1753	3497	1559	1365	126	1453	1259	197	1339
Grp Volume(v), veh/h	46	757	794	48	972	67	11	0	75	67	0	39
Grp Sat Flow(s),veh/h/ln	1781	1777	1864	1753	1749	1559	1365	0	1580	1259	0	1536
Q Serve(g_s), s	0.9	29.2	29.2	1.0	14.7	1.7	1.0	0.0	5.7	6.8	0.0	3.0
Cycle Q Clear(g_c), s	0.9	29.2	29.2	1.0	14.7	1.7	4.0	0.0	5.7	12.5	0.0	3.0
Prop In Lane	1.00		0.02	1.00		1.00	1.00		0.92	1.00		0.87
Lane Grp Cap(c), veh/h	432	1239	1299	278	2467	1099	179	0	179	143	0	174
V/C Ratio(X)	0.11	0.61	0.61	0.17	0.39	0.06	0.06	0.00	0.42	0.47	0.00	0.22
Avail Cap(c_a), veh/h	489	1239	1299	333	2467	1099	215	0	221	176	0	215
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	5.7	10.4	10.4	8.5	7.8	5.9	54.2	0.0	53.6	59.5	0.0	52.4
Incr Delay (d2), s/veh	0.1	2.2	2.2	0.3	0.5	0.1	0.1	0.0	1.5	2.4	0.0	0.6
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	0.5	15.8	16.4	0.6	8.6	0.9	0.6	0.0	4.3	4.1	0.0	2.2
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	5.8	12.6	12.5	8.8	8.3	6.0	54.4	0.0	55.2	61.8	0.0	53.1
LnGrp LOS	A	B	B	A	A	A	D	A	E	E	A	D
Approach Vol, veh/h		1597			1087			86			106	
Approach Delay, s/veh		12.4			8.2			55.1			58.6	
Approach LOS		B			A			E			E	
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	10.9	98.6		20.6	11.9	97.5		20.6				
Change Period (Y+Rc), s	6.8	6.9		* 5.8	6.9	6.9		* 5.8				
Max Green Setting (Gmax), s	8.2	84.1		* 18	9.1	83.1		* 18				
Max Q Clear Time (g_c+I1), s	2.9	16.7		7.7	3.0	31.2		14.5				
Green Ext Time (p_c), s	0.0	8.2		0.2	0.0	15.0		0.1				

Intersection Summary

HCM 6th Ctrl Delay	13.8
HCM 6th LOS	B

Notes

- User approved pedestrian interval to be less than phase max green.
- * HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Lanes, Volumes, Timings
 1: SW Florida Gateway Dr/Centurion Ct & US 90

Circle K - I-75 & US 90
 Background (2023) Conditions, PM Peak Hour

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	22	1172	29	80	1346	96	41	2	58	66	3	41
Future Volume (vph)	22	1172	29	80	1346	96	41	2	58	66	3	41
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	150		0	250		125	50		0	0		110
Storage Lanes	1		0	1		1	1		0	1		1
Taper Length (ft)	25			50			25			25		
Right Turn on Red			Yes			Yes			Yes			Yes
Link Speed (mph)		45			45			30				30
Link Distance (ft)		398			433			442				282
Travel Time (s)		6.0			6.6			10.0				6.4
Confl. Peds. (#/hr)	2		3	3		2						
Confl. Bikes (#/hr)						2						
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90
Heavy Vehicles (%)	2%	2%	2%	2%	2%	2%	3%	3%	3%	2%	2%	2%
Shared Lane Traffic (%)												
Turn Type	pm+pt	NA		pm+pt	NA	Perm	Perm	NA		Perm	NA	
Protected Phases	1	6		5	2			4				8
Permitted Phases	6			2		2	4			8		
Detector Phase	1	6		5	2	2	4	4		8		8
Switch Phase												
Minimum Initial (s)	5.0	15.0		5.0	15.0	15.0	7.0	7.0		7.0		7.0
Minimum Split (s)	11.8	31.9		11.9	31.9	31.9	41.8	41.8		34.8		34.8
Total Split (s)	15.0	78.0		25.0	88.0	88.0	47.0	47.0		47.0		47.0
Total Split (%)	10.0%	52.0%		16.7%	58.7%	58.7%	31.3%	31.3%		31.3%		31.3%
Yellow Time (s)	4.8	4.9		4.9	4.9	4.9	3.8	3.8		3.8		3.8
All-Red Time (s)	2.0	2.0		2.0	2.0	2.0	2.0	2.0		2.0		2.0
Lost Time Adjust (s)	0.0	0.0		0.0	0.0	0.0	0.0	0.0		0.0		0.0
Total Lost Time (s)	6.8	6.9		6.9	6.9	6.9	5.8	5.8		5.8		5.8
Lead/Lag	Lead	Lag		Lead	Lag	Lag						
Lead-Lag Optimize?	Yes	Yes		Yes	Yes	Yes						
Recall Mode	None	Max		None	C-Max	C-Max	None	None		None		None

Intersection Summary

Area Type: Other
 Cycle Length: 150
 Actuated Cycle Length: 150
 Offset: 20 (13%), Referenced to phase 2:WBTL, Start of Yellow
 Natural Cycle: 100
 Control Type: Actuated-Coordinated

Splits and Phases: 1: SW Florida Gateway Dr/Centurion Ct & US 90



HCM 6th Signalized Intersection Summary
 1: SW Florida Gateway Dr/Centurion Ct & US 90

Circle K - I-75 & US 90
 Background (2023) Conditions, PM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗	↗	↖	↗		↖	↗	
Traffic Volume (veh/h)	22	1172	29	80	1346	96	41	2	58	66	3	41
Future Volume (veh/h)	22	1172	29	80	1346	96	41	2	58	66	3	41
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		0.98	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1856	1856	1856	1870	1870	1870
Adj Flow Rate, veh/h	24	1302	32	89	1496	107	46	2	64	73	3	46
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90
Percent Heavy Veh, %	2	2	2	2	2	2	3	3	3	2	2	2
Cap, veh/h	260	2577	63	341	2630	1146	157	5	167	142	11	164
Arrive On Green	0.02	0.73	0.73	0.03	0.74	0.74	0.11	0.11	0.11	0.11	0.11	0.11
Sat Flow, veh/h	1781	3544	87	1781	3554	1549	1345	48	1532	1335	98	1502
Grp Volume(v), veh/h	24	652	682	89	1496	107	46	0	66	73	0	49
Grp Sat Flow(s),veh/h/ln	1781	1777	1854	1781	1777	1549	1345	0	1580	1335	0	1600
Q Serve(g_s), s	0.5	23.8	23.8	1.9	28.4	2.9	4.9	0.0	5.8	8.1	0.0	4.2
Cycle Q Clear(g_c), s	0.5	23.8	23.8	1.9	28.4	2.9	9.1	0.0	5.8	13.9	0.0	4.2
Prop In Lane	1.00		0.05	1.00		1.00	1.00		0.97	1.00		0.94
Lane Grp Cap(c), veh/h	260	1292	1348	341	2630	1146	157	0	172	142	0	174
V/C Ratio(X)	0.09	0.50	0.51	0.26	0.57	0.09	0.29	0.00	0.38	0.52	0.00	0.28
Avail Cap(c_a), veh/h	320	1292	1348	496	2630	1146	380	0	434	363	0	439
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	7.3	8.8	8.8	6.9	8.8	5.4	65.6	0.0	62.1	68.6	0.0	61.4
Incr Delay (d2), s/veh	0.2	1.4	1.4	0.4	0.9	0.2	1.0	0.0	1.4	2.9	0.0	0.9
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	0.3	13.4	13.8	1.1	14.9	1.6	3.1	0.0	4.4	5.2	0.0	3.2
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	7.5	10.2	10.2	7.3	9.7	5.6	66.6	0.0	63.5	71.5	0.0	62.3
LnGrp LOS	A	B	B	A	A	A	E	A	E	E	A	E
Approach Vol, veh/h		1358			1692			112			122	
Approach Delay, s/veh		10.2			9.3			64.8			67.8	
Approach LOS		B			A			E			E	
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	10.0	117.9		22.1	11.9	116.0		22.1				
Change Period (Y+Rc), s	6.8	6.9		* 5.8	6.9	6.9		* 5.8				
Max Green Setting (Gmax), s	8.2	81.1		* 41	18.1	71.1		* 41				
Max Q Clear Time (g_c+I1), s	2.5	30.4		11.1	3.9	25.8		15.9				
Green Ext Time (p_c), s	0.0	16.3		0.5	0.1	11.0		0.5				

Intersection Summary

HCM 6th Ctrl Delay	13.7
HCM 6th LOS	B

Notes

- User approved pedestrian interval to be less than phase max green.
- * HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Lanes, Volumes, Timings
1: SW Florida Gateway Dr/Centurion Ct & US 90

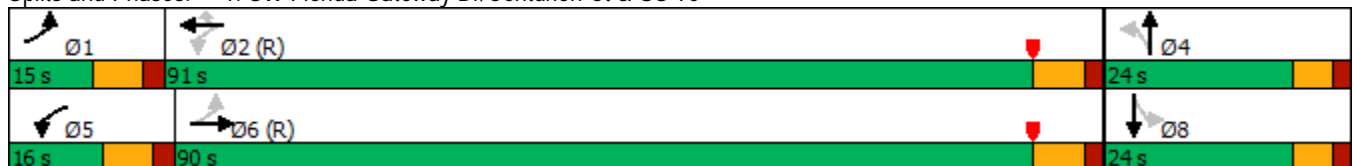
Circle K - I-75 & US 90
Buildout (2023) Conditions, AM Peak Hour

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	57	1417	13	45	892	80	10	6	64	80	5	46
Future Volume (vph)	57	1417	13	45	892	80	10	6	64	80	5	46
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	150		0	250		125	50		0	0		110
Storage Lanes	1		0	1		1	1		0	1		1
Taper Length (ft)	25			50			25			25		
Right Turn on Red			Yes			Yes			Yes			Yes
Link Speed (mph)		45			45			30				30
Link Distance (ft)		398			433			442				282
Travel Time (s)		6.0			6.6			10.0				6.4
Confl. Peds. (#/hr)	1						1			1	1	
Confl. Bikes (#/hr)			2						1			
Peak Hour Factor	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93
Heavy Vehicles (%)	2%	2%	2%	4%	4%	4%	2%	2%	2%	8%	8%	8%
Shared Lane Traffic (%)												
Turn Type	pm+pt	NA		pm+pt	NA	Perm	Perm	NA		Perm	NA	
Protected Phases	1	6		5	2			4				8
Permitted Phases	6			2		2	4			8		
Detector Phase	1	6		5	2	2	4	4		8		8
Switch Phase												
Minimum Initial (s)	5.0	15.0		5.0	15.0	15.0	7.0	7.0		7.0		7.0
Minimum Split (s)	11.8	31.9		11.9	31.9	31.9	41.8	41.8		34.8		34.8
Total Split (s)	15.0	90.0		16.0	91.0	91.0	24.0	24.0		24.0		24.0
Total Split (%)	11.5%	69.2%		12.3%	70.0%	70.0%	18.5%	18.5%		18.5%		18.5%
Yellow Time (s)	4.8	4.9		4.9	4.9	4.9	3.8	3.8		3.8		3.8
All-Red Time (s)	2.0	2.0		2.0	2.0	2.0	2.0	2.0		2.0		2.0
Lost Time Adjust (s)	0.0	0.0		0.0	0.0	0.0	0.0	0.0		0.0		0.0
Total Lost Time (s)	6.8	6.9		6.9	6.9	6.9	5.8	5.8		5.8		5.8
Lead/Lag	Lead	Lag		Lead	Lag	Lag						
Lead-Lag Optimize?	Yes	Yes		Yes	Yes	Yes						
Recall Mode	None	C-Min		None	C-Min	C-Min	None	None		None		None

Intersection Summary

Area Type: Other
 Cycle Length: 130
 Actuated Cycle Length: 130
 Offset: 24 (18%), Referenced to phase 2:WBTL and 6:EBTL, Start of Yellow
 Natural Cycle: 100
 Control Type: Actuated-Coordinated

Splits and Phases: 1: SW Florida Gateway Dr/Centurion Ct & US 90



HCM 6th Signalized Intersection Summary
 1: SW Florida Gateway Dr/Centurion Ct & US 90

Circle K - I-75 & US 90
 Buildout (2023) Conditions, AM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗	↗	↖	↗		↖	↗	
Traffic Volume (veh/h)	57	1417	13	45	892	80	10	6	64	80	5	46
Future Volume (veh/h)	57	1417	13	45	892	80	10	6	64	80	5	46
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		0.98	1.00		1.00	1.00		0.98	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1841	1841	1841	1870	1870	1870	1781	1781	1781
Adj Flow Rate, veh/h	61	1524	14	48	959	86	11	6	69	86	5	49
Peak Hour Factor	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93
Percent Heavy Veh, %	2	2	2	4	4	4	2	2	2	8	8	8
Cap, veh/h	425	2466	23	273	2409	1074	184	16	185	161	18	176
Arrive On Green	0.03	0.68	0.68	0.04	0.69	0.69	0.13	0.13	0.13	0.13	0.13	0.13
Sat Flow, veh/h	1781	3607	33	1753	3497	1559	1347	126	1454	1259	141	1387
Grp Volume(v), veh/h	61	750	788	48	959	86	11	0	75	86	0	54
Grp Sat Flow(s),veh/h/ln	1781	1777	1864	1753	1749	1559	1347	0	1581	1259	0	1528
Q Serve(g_s), s	1.3	30.1	30.1	1.0	15.3	2.4	1.0	0.0	5.7	8.7	0.0	4.2
Cycle Q Clear(g_c), s	1.3	30.1	30.1	1.0	15.3	2.4	5.1	0.0	5.7	14.4	0.0	4.2
Prop In Lane	1.00		0.02	1.00		1.00	1.00		0.92	1.00		0.91
Lane Grp Cap(c), veh/h	425	1215	1274	273	2409	1074	184	0	201	161	0	194
V/C Ratio(X)	0.14	0.62	0.62	0.18	0.40	0.08	0.06	0.00	0.37	0.54	0.00	0.28
Avail Cap(c_a), veh/h	476	1215	1274	328	2409	1074	201	0	221	177	0	214
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	6.3	11.3	11.3	9.2	8.7	6.7	53.7	0.0	52.0	58.6	0.0	51.3
Incr Delay (d2), s/veh	0.2	2.4	2.3	0.3	0.5	0.1	0.1	0.0	1.1	2.7	0.0	0.8
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	0.8	16.5	17.1	0.6	9.0	1.3	0.6	0.0	4.2	5.2	0.0	3.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	6.4	13.6	13.5	9.5	9.2	6.8	53.8	0.0	53.1	61.3	0.0	52.1
LnGrp LOS	A	B	B	A	A	A	D	A	D	E	A	D
Approach Vol, veh/h		1599			1093			86			140	
Approach Delay, s/veh		13.3			9.0			53.2			57.8	
Approach LOS		B			A			D			E	
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	11.2	96.4		22.3	11.9	95.8		22.3				
Change Period (Y+Rc), s	6.8	6.9		* 5.8	6.9	6.9		* 5.8				
Max Green Setting (Gmax), s	8.2	84.1		* 18	9.1	83.1		* 18				
Max Q Clear Time (g_c+I1), s	3.3	17.3		7.7	3.0	32.1		16.4				
Green Ext Time (p_c), s	0.0	8.1		0.2	0.0	14.7		0.1				

Intersection Summary

HCM 6th Ctrl Delay	15.0
HCM 6th LOS	B

Notes

- User approved pedestrian interval to be less than phase max green.
- * HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Lanes, Volumes, Timings
 1: SW Florida Gateway Dr/Centurion Ct & US 90

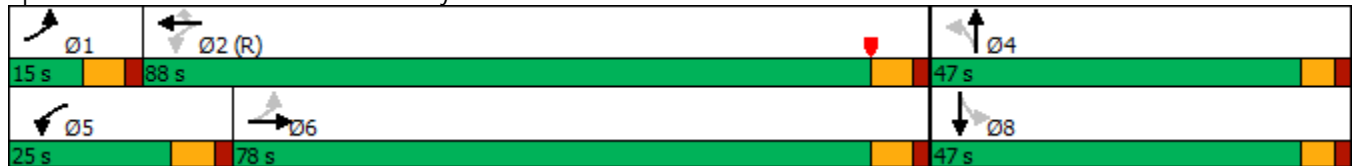
Circle K - I-75 & US 90
 Buildout (2023) Conditions, PM Peak Hour

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	37	1159	29	80	1332	117	41	2	58	86	3	57
Future Volume (vph)	37	1159	29	80	1332	117	41	2	58	86	3	57
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	150		0	250		125	50		0	0		110
Storage Lanes	1		0	1		1	1		0	1		1
Taper Length (ft)	25			50			25			25		
Right Turn on Red			Yes			Yes			Yes			Yes
Link Speed (mph)		45			45			30				30
Link Distance (ft)		398			433			442				282
Travel Time (s)		6.0			6.6			10.0				6.4
Confl. Peds. (#/hr)	2		3	3		2						
Confl. Bikes (#/hr)						2						
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90
Heavy Vehicles (%)	2%	2%	2%	2%	2%	2%	3%	3%	3%	2%	2%	2%
Shared Lane Traffic (%)												
Turn Type	pm+pt	NA		pm+pt	NA	Perm	Perm	NA		Perm	NA	
Protected Phases	1	6		5	2			4				8
Permitted Phases	6			2		2	4			8		
Detector Phase	1	6		5	2	2	4	4		8		8
Switch Phase												
Minimum Initial (s)	5.0	15.0		5.0	15.0	15.0	7.0	7.0		7.0		7.0
Minimum Split (s)	11.8	31.9		11.9	31.9	31.9	41.8	41.8		34.8		34.8
Total Split (s)	15.0	78.0		25.0	88.0	88.0	47.0	47.0		47.0		47.0
Total Split (%)	10.0%	52.0%		16.7%	58.7%	58.7%	31.3%	31.3%		31.3%		31.3%
Yellow Time (s)	4.8	4.9		4.9	4.9	4.9	3.8	3.8		3.8		3.8
All-Red Time (s)	2.0	2.0		2.0	2.0	2.0	2.0	2.0		2.0		2.0
Lost Time Adjust (s)	0.0	0.0		0.0	0.0	0.0	0.0	0.0		0.0		0.0
Total Lost Time (s)	6.8	6.9		6.9	6.9	6.9	5.8	5.8		5.8		5.8
Lead/Lag	Lead	Lag		Lead	Lag	Lag						
Lead-Lag Optimize?	Yes	Yes		Yes	Yes	Yes						
Recall Mode	None	Max		None	C-Max	C-Max	None	None		None		None

Intersection Summary

Area Type: Other
 Cycle Length: 150
 Actuated Cycle Length: 150
 Offset: 20 (13%), Referenced to phase 2:WBTL, Start of Yellow
 Natural Cycle: 100
 Control Type: Actuated-Coordinated

Splits and Phases: 1: SW Florida Gateway Dr/Centurion Ct & US 90



HCM 6th Signalized Intersection Summary
 1: SW Florida Gateway Dr/Centurion Ct & US 90

Circle K - I-75 & US 90
 Buildout (2023) Conditions, PM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗	↗	↖	↗		↖	↗	
Traffic Volume (veh/h)	37	1159	29	80	1332	117	41	2	58	86	3	57
Future Volume (veh/h)	37	1159	29	80	1332	117	41	2	58	86	3	57
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		0.98	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1856	1856	1856	1870	1870	1870
Adj Flow Rate, veh/h	41	1288	32	89	1480	130	46	2	64	96	3	63
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90
Percent Heavy Veh, %	2	2	2	2	2	2	3	3	3	2	2	2
Cap, veh/h	258	2515	62	334	2546	1110	165	6	193	166	9	192
Arrive On Green	0.03	0.71	0.71	0.03	0.72	0.72	0.13	0.13	0.13	0.13	0.13	0.13
Sat Flow, veh/h	1781	3543	88	1781	3554	1548	1325	48	1532	1335	73	1524
Grp Volume(v), veh/h	41	646	674	89	1480	130	46	0	66	96	0	66
Grp Sat Flow(s),veh/h/ln	1781	1777	1854	1781	1777	1548	1325	0	1580	1335	0	1596
Q Serve(g_s), s	0.9	24.8	24.9	2.0	30.3	3.9	4.9	0.0	5.7	10.6	0.0	5.7
Cycle Q Clear(g_c), s	0.9	24.8	24.9	2.0	30.3	3.9	10.6	0.0	5.7	16.3	0.0	5.7
Prop In Lane	1.00		0.05	1.00		1.00	1.00		0.97	1.00		0.95
Lane Grp Cap(c), veh/h	258	1261	1316	334	2546	1110	165	0	199	166	0	201
V/C Ratio(X)	0.16	0.51	0.51	0.27	0.58	0.12	0.28	0.00	0.33	0.58	0.00	0.33
Avail Cap(c_a), veh/h	307	1261	1316	489	2546	1110	362	0	434	364	0	438
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	8.6	9.9	9.9	7.8	10.3	6.6	64.6	0.0	59.8	67.2	0.0	59.7
Incr Delay (d2), s/veh	0.3	1.5	1.4	0.4	1.0	0.2	0.9	0.0	1.0	3.2	0.0	0.9
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	0.6	14.2	14.6	1.3	16.2	2.2	3.1	0.0	4.3	6.8	0.0	4.2
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	8.9	11.4	11.4	8.3	11.3	6.8	65.5	0.0	60.7	70.4	0.0	60.7
LnGrp LOS	A	B	B	A	B	A	E	A	E	E	A	E
Approach Vol, veh/h		1361			1699			112				162
Approach Delay, s/veh		11.3			10.8			62.7				66.4
Approach LOS		B			B			E				E
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	10.9	114.4		24.7	11.9	113.4		24.7				
Change Period (Y+Rc), s	6.8	6.9		* 5.8	6.9	6.9		* 5.8				
Max Green Setting (Gmax), s	8.2	81.1		* 41	18.1	71.1		* 41				
Max Q Clear Time (g_c+I1), s	2.9	32.3		12.6	4.0	26.9		18.3				
Green Ext Time (p_c), s	0.0	16.0		0.5	0.1	10.8		0.6				

Intersection Summary

HCM 6th Ctrl Delay	15.5
HCM 6th LOS	B

Notes

- User approved pedestrian interval to be less than phase max green.
- * HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

APPENDIX E
Trip Generation Calculations

Table 1: Trip Generation

Land Use	Intensity	AM Peak Hour of Adjacent Street			PM Peak Hour of Adjacent Street		
		Total	In	Out	Total	In	Out
Existing Development Convenience Store/Gas Station (4-5.5k)	24 VFP	649	325	324	546	273	273
Existing Development Pass-By Convenience Store/Gas Station (4-5.5k)	<u>Daily</u> <u>AM</u> <u>PM</u> 75% 76% 75%	494	247	247	410	205	205
EXISTING SITE - POTENTIAL TOTAL DRIVEWAY VOLUMES		649	325	324	546	273	273
EXISTING SITE - POTENTIAL PASS-BY TRIPS		494	247	247	410	205	205
EXISTING SITE - POTENTIAL NEW EXTERNAL TRIPS		155	78	77	136	68	68
OBSERVED DRIVEWAY VOLUMES		201	106	95	220	115	105
ACTUAL/POTENTIAL DRIVEWAY VOLUMES ADJUSTMENT FACTOR		0.31			0.40		
Proposed Development Convenience Store/Gas Station (5.5-10k)	27 VFP	853	427	426	726	363	363
Proposed Development Pass-By Convenience Store/Gas Station (5.5-10k)	<u>Daily</u> <u>AM</u> <u>PM</u> 75% 76% 75%	648	324	324	544	272	272
PROPOSED SITE - POTENTIAL TOTAL DRIVEWAY VOLUMES		853	427	426	726	363	363
PROPOSED SITE - POTENTIAL TOTAL PASS-BY TRIPS		648	324	324	544	272	272
PROPOSED SITE - POTENTIAL TOTAL NEW EXTERNAL TRIPS		205	103	102	182	91	91
POTENTIAL NET NEW TOTAL DRIVEWAY VOLUMES (PROPOSED - EXISTING)		204	102	102	180	90	90
POTENTIAL NET NEW PASS-BY TRIPS (PROPOSED - EXISTING)		154	77	77	134	67	67
POTENTIAL NET NEW EXTERNAL TRIPS (PROPOSED - EXISTING)		50	25	25	46	23	23
ADJUSTED NET NEW TOTAL DRIVEWAY VOLUMES		64	32	32	72	36	36
ADJUSTED NET NEW PASS-BY TRIPS		48	24	24	54	27	27
ADJUSTED NET NEW EXTERNAL TRIPS		16	8	8	18	9	9

Trip generation and pass-by reductions were calculated using the following data from ITE's Trip Generation Manual, 11th Edition.

Convenience Store/ Gas Station (4-5.5k) [ITE 945]

Daily: $T = 257.13*(X)$; X is vehicle fueling positions
 AM Peak Hour of Adjacent Street: $T = 27.04*(X)$; X is vehicle fueling positions; (50% in, 50% out)
 PM Peak Hour of Adjacent Street: $T = 22.76*(X)$; X is vehicle fueling positions; (50% in, 50% out)

Convenience Store/ Gas Station (5.5-10k) [ITE 945]

Daily: $T = 345.75*(X)$; X is vehicle fueling positions
 AM Peak Hour of Adjacent Street: $T = 31.60*(X)$; X is vehicle fueling positions; (50% in, 50% out)
 PM Peak Hour of Adjacent Street: $T = 26.90*(X)$; X is vehicle fueling positions; (50% in, 50% out)

APPENDIX F
FDOT *Trend* Worksheet

FLORIDA DEPARTMENT OF TRANSPORTATION
 TRANSPORTATION STATISTICS OFFICE
 2020 HISTORICAL AADT REPORT

COUNTY: 29 - COLUMBIA

SITE: 0278 - SR 10 400' W. OF I-75

YEAR	AADT		DIRECTION 1		DIRECTION 2	*K FACTOR	D FACTOR	T FACTOR
2020	27000	C	E 13500		W 13500	9.00	54.80	6.80
2019	30000	C	E 15000		W 15000	9.00	54.80	6.20
2018	28000	C	E 14000		W 14000	9.00	54.70	6.20
2017	27500	C	E 14000		W 13500	9.00	55.50	5.80
2016	27000	C	E 13500		W 13500	9.00	53.90	5.40
2015	27500	C	E 14000		W 13500	9.00	54.50	5.70
2014	27000	C	E 13500		W 13500	9.00	54.40	5.90
2013	25000	C	E 12500		W 12500	9.00	55.30	6.40
2012	26000	C	E 13000		W 13000	9.00	54.70	5.50
2011	26000	C	E 13000		W 13000	9.00	53.70	5.30
2010	25500	C	E 12500		W 13000	9.94	54.40	4.90
2009	25000	C	E 12500		W 12500	9.78	54.18	5.30
2008	27000	C	E 13500		W 13500	9.82	54.63	6.20
2007	27500	C	E 13500		W 14000	9.99	54.46	6.40
2006	27000	C	E 13500		W 13500	10.01	55.64	7.00
2005	31500	C	E 15500		W 16000	9.90	56.60	9.80

AADT FLAGS: C = COMPUTED; E = MANUAL ESTIMATE; F = FIRST YEAR ESTIMATE
 S = SECOND YEAR ESTIMATE; T = THIRD YEAR ESTIMATE; R = FOURTH YEAR ESTIMATE
 V = FIFTH YEAR ESTIMATE; 6 = SIXTH YEAR ESTIMATE; X = UNKNOWN

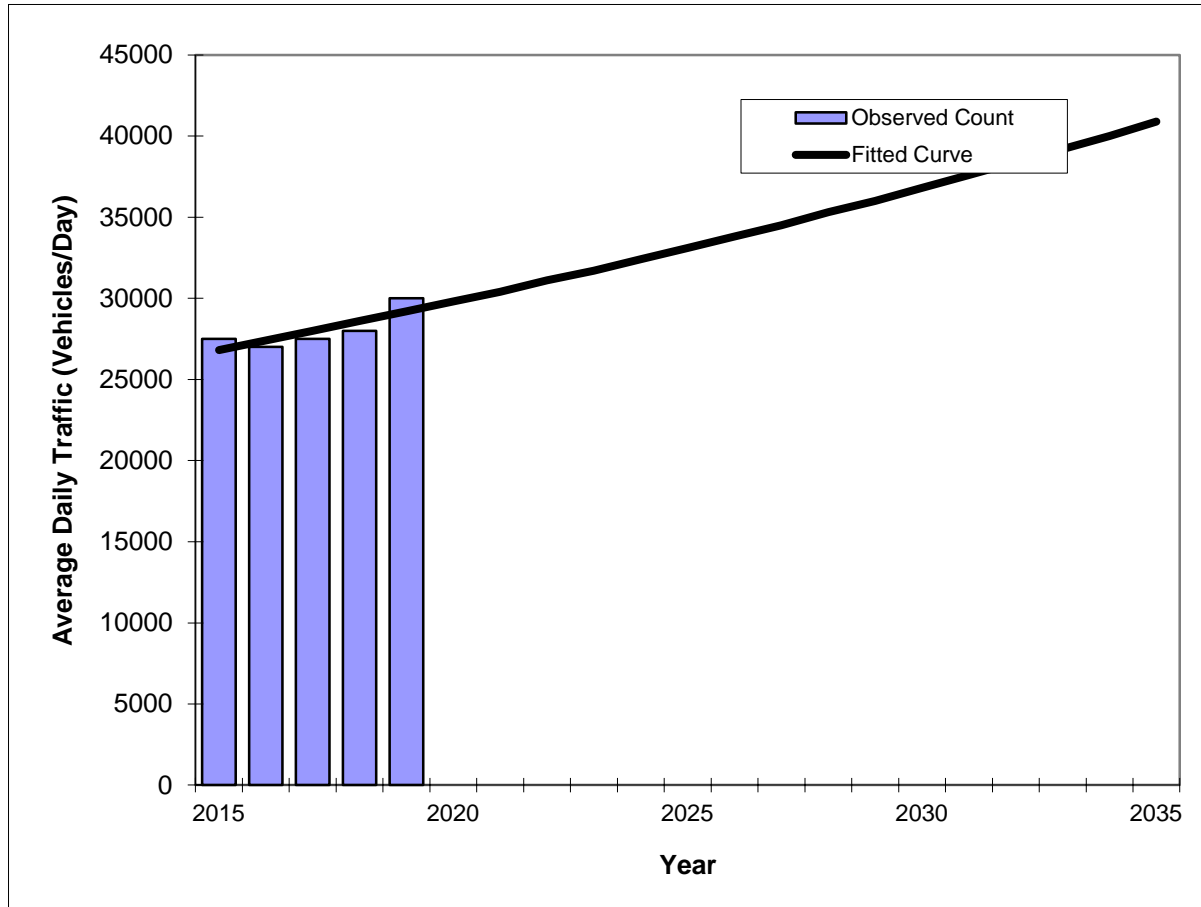
*K FACTOR: STARTING WITH YEAR 2011 IS STANDARDK, PRIOR YEARS ARE K30 VALUES

Traffic Trends - V03.a

SR 10 -- 400' W OF I-75

FIN#	429193-1
Location	1

County:	Columbia (29)
Station #:	0278
Highway:	SR 10



Year	Traffic (ADT/AADT)	
	Count*	Trend**
2015	27500	26800
2016	27000	27400
2017	27500	28000
2018	28000	28600
2019	30000	29200
2023 Opening Year Trend		
2023	N/A	31700
2024 Mid-Year Trend		
2024	N/A	32400
2025 Design Year Trend		
2025	N/A	33100
TRANPLAN Forecasts/Trends		

Trend R-squared:	65.77%
Compounded Annual Historic Growth Rate:	2.17%
Compounded Growth Rate (2019 to Design Year):	2.11%
Printed:	3-Mar-22
Exponential Growth Option	

*Axle-Adjusted

DESCRIPTION:

PARCEL 1: 35-3S-16-02524-001 (Existing Circle K):

LOT 1 GATEWAY CROSSING S/D. WD 1339-654,

PARCEL 2: 35-3S-16-02524-102:

LOT 2 GATEWAY CROSSING S/D A REPLAT OF LOTS 2 & 3.


PARCEL 3: 35-3S-16-02524-111

LOT 11 GATEWAY CROSSING S/D A REPLAT OF LOTS 2 & 3.

This Instrument Was Prepared By,
Record and Return to:

John Hotte, Esquire
Krinzman, Huss & Lubetsky
110 SE 6th Street, 20th Floor
Fort Lauderdale, FL 33301
Telephone: (954) 761-3454

Property Appraiser
Identification No.:
Consideration:\$ _____

 Inst:201612000647 Date:1/14/2016 Time:10:37 AM
Doc: Stamp-Deed:19775.00
DC,P.DeWitt Cason,Columbia County Page 1 of 4 B:1307 P:1888

SPECIAL WARRANTY DEED

THIS SPECIAL WARRANTY DEED made this 12th day of January, 2016, by **Inn of Lake City, Inc.**, a Florida corporation ("Grantor"), whose mailing address is 1000 Red Fern Place, Flowood, MS 39232 in favor of **GWC Development Partners, LLC**, a Florida limited liability company ("Grantee"), whose mailing address is 2682 West Noegel Road, Lake City, FL 32055.

WITNESSETH:

That Grantor, for and in consideration of the sum of TEN DOLLARS (\$10.00) and other good and valuable consideration paid by Grantee, the receipt and sufficiency whereof are hereby acknowledged, does hereby grant, bargain, sell, alien, remise, release, convey and confirm unto Grantee the real property (the "Property") located in Columbia County, Florida, and more particularly described in Exhibit "A" attached hereto and made a part hereof.

SUBJECT ONLY TO the matters set forth in Exhibit "B" attached hereto and made a part hereof.

TOGETHER with all the tenements, hereditaments and appurtenances belonging or in any way appertaining to the Property, including, without limitation, all of Grantor's right, title and interest, if any, in and to all of the easements, rights, and privileges belonging or in any way appertaining to the Property and/or improvements located thereon.

TO HAVE AND TO HOLD the same in fee simple forever.

AND GRANTOR hereby covenants with Grantee that Grantor is lawfully seized of the Property in fee simple; that Grantor has good right and lawful authority to sell and convey the Property; that, subject to the matters described on Exhibit "B" attached hereto, Grantor does hereby warrant specially the title to the Property; and that Grantor and its successors and assigns will forever warrant and defend the same against the lawful claims of all persons claiming by, through or under Grantor, but against none other.

IN WITNESS WHEREOF, Grantor has caused this Special Warranty Deed to be executed by its duly authorized representative on the day and year first above written.

Two Witnesses:

Inn of Lake City, Inc., a Florida corporation

Debra D. Hardwick
Printed Name: Debra D. Hardwick

By: *Michael J. Hart*
Michael J. Hart, Vice President, Treasurer
and Assistant Secretary

John M. Stephens IV
Printed Name: John M. Stephens IV

STATE OF Mississippi
COUNTY OF Hinds

The foregoing instrument was acknowledged before me this 12 day of January, 2016, by Michael J. Hart, as Vice President, Treasurer and Assistant Secretary of Inn of Lake City, Inc., a Florida corporation, on behalf of the corporation. He is personally known to me.

Suzanna Baker
Notary Public, State of Mississippi
Print Name: Suzanna Baker
Commission No.: 83877
My Commission Expires: January 21, 2019
[Affix Notary Seal]



Signature Page of Special Warranty Deed

EXHIBIT "A"

Real Property Description

COMMENCE at the Northwest corner of Section 35, Township 3 South, Range 16 East, Columbia County, Florida as established by B.G. Moore, PLS No. 439 and run thence S 06°22'00" W, along the West line of said Section 35, 1894.50 feet to the West Limited Access Right of Way of Interstate No. 75, thence run Southerly and Westerly along said West Limited Access Right of Way the following courses. S 24°54'32" E, 472.32 feet to the POINT OF BEGINNING, S 24°54'32" E, 940.25 feet; S 15°12'50" E, 512.06 feet; S 06°01'43" E, 335.81 feet; S 36°55'36" W, 54.60 feet to the Northerly Right of Way of West U.S. Highway 90 and the end of said courses; thence S 80°47'35" W, along said Northerly Right of Way, 371.77 feet; thence S 08°51'10" E, along said Northerly Right of Way, 22.18 feet; thence S 80°47'36" W, along said Northerly Right of Way, 73.15 feet; thence N 08°55'17" W, 150.09 feet; thence S 80°42'55" W, 150.25 feet; thence N 08°52'22" W, 60.12 feet; thence S 80°53'59" W, 79.99 feet; thence S 08°59'18" E, 210.15 feet to the aforesaid Northerly Right of Way; thence S 80°47'36" W, along said Northerly Right of Way, 26.39 feet to a point of a curve; thence run Westerly along the arc of said curve concave to the North having a radius of 3224.04 feet, a central angle of 05°24'20", a chord bearing and distance of S 83°26'26" W 304.06 feet, an arc distance of 304.18 feet to the aforesaid West line of Section 35; thence N 06°22'00" E, along said West line, 1784.01 feet; thence N 65°09'42" E, 286.69 feet to the POINT OF BEGINNING.

LESS AND EXCEPT the parcel described in O.R. Book 1284, Page 229, of the Official Records of Columbia County, Florida

EXHIBIT "B"

Exceptions

1. Taxes and assessments for the year 2016 and subsequent years, which are not yet due and payable.
2. Any land use, zoning and building laws and ordinances.
3. Any declaration of covenants, conditions and restrictions, or other recorded restrictions.
4. Any right, title, interest, claim, violation, variation, encumbrance, encroachment, fact, matters or other adverse circumstance affecting title revealed, or that should have been revealed, by that certain ALTA/ACSM survey of the Property by JBrown Professional Group Inc. dated October 16, 2015, as revised (Proj. No. 366-15-01).
5. Any obligations, rights and other matters related to, and any agreements with and requirements of the State of Florida or other governmental agency regarding, the remediation of certain environmental issues on the Property by or on behalf of the State of Florida or a political subdivision thereof under a state-funded cleanup program(s).
6. Rights-of-way, utility easements, other easements, restrictions and other restrictive and/or use covenants filed of record and other matters which are revealed by a title search or title commitments, including the following:
 - a. Easement(s) in favor of Mississippi Management, Inc. set forth in instrument(s) recorded in Official Records Book 634, Page 338.
 - b. Easement(s) in favor of Shell Oil Company set forth in instrument(s) recorded in Official Records Book 674, Page 104.
 - c. Easement(s) in favor of American Telephone and Telegraph Company set forth in instrument(s) recorded in Official Records Book 723, Page 162.
 - d. Easement(s) in favor of The City of Lake City, Florida set forth in instrument(s) recorded in Official Records Book 776, Page 1724.
 - e. Easement contained in Deed recorded in Official Records Book 685, Page 38.
 - f. Easement recorded in Official Records Book 960, Page 1492.
 - g. Easement recorded in Official Records 104, Page 118, and in Official Records Book 361, Page 499.
 - h. Easement for ingress and egress recorded in Official Records Book 370, Page 337.
 - i. Easement(s) in favor of Florida Power and Light Company set forth in instrument(s) recorded in Official Records Book 361, Page 499.
 - j. Easement(s) in favor of The City of Lake City, Florida set forth in instrument(s) recorded in Official Records Book 559, Page 229.

Columbia County Tax Collector

generated on 6/9/2022 1:59:44 PM EDT

Tax Record

Last Update: 6/9/2022 1:58:23 PM EDT



Ad Valorem Taxes and Non-Ad Valorem Assessments

The information contained herein does not constitute a title search and should not be relied on as such.

Account Number	Tax Type	Tax Year			
R02524-102	REAL ESTATE	2021			
Mailing Address GWC DEVELOPMENT PARTNERS LLC 2682 NW NOEGEL RD LAKE CITY FL 32055		Property Address GEO Number 353S16-02524-102			
Exempt Amount	Taxable Value				
See Below	See Below				
Exemption Detail	Millage Code	Escrow Code			
NO EXEMPTIONS	001				
<u>Legal Description (click for full description)</u>					
35-3S-16 1000/10001.03 Acres LOT 2 GATEWAY CROSSING S/D A REPLAT OF LOTS 2 & 3.					
Ad Valorem Taxes					
Taxing Authority	Rate	Assessed Value	Exemption Amount	Taxable Value	Taxes Levied
BOARD OF COUNTY COMMISSIONERS	7.8150	515,968	0	\$515,968	\$4,032.29
CITY OF LAKE CITY	4.9000	515,968	0	\$515,968	\$2,528.24
COLUMBIA COUNTY SCHOOL BOARD					
DISCRETIONARY	0.7480	515,968	0	\$515,968	\$385.95
LOCAL	3.6430	515,968	0	\$515,968	\$1,879.67
CAPITAL OUTLAY	1.5000	515,968	0	\$515,968	\$773.95
SUWANNEE RIVER WATER MGT DIST	0.3615	515,968	0	\$515,968	\$186.52
LAKE SHORE HOSPITAL AUTHORITY	0.0000	515,968	0	\$515,968	\$0.00
Total Millage	18.9675	Total Taxes		\$9,786.62	
Non-Ad Valorem Assessments					

Code	Levying Authority	Amount
XLCF	CITY FIRE ASSESSMENT	\$50.40
Total Assessments		\$50.40
Taxes & Assessments		\$9,837.02
If Paid By		Amount Due
		\$0.00

Date Paid	Transaction	Receipt	Item	Amount Paid
12/29/2021	PAYMENT	1200971.0006	2021	\$9,541.91

Prior Years Payment History

Prior Year Taxes Due
NO DELINQUENT TAXES



UNIVERSAL ENGINEERING SCIENCES

Consultants In: Geotechnical Engineering • Environmental Sciences
Geophysical Services • Construction Materials Testing • Threshold Inspection
Building Inspection • Plan Review • Building Code Administration

LOCATIONS:

- Atlanta, GA
- Chantilly, VA
- Daytona Beach, FL
- Fort Myers, FL
- Fort Pierce, FL
- Gainesville, FL
- Hagerstown, MD
- Jacksonville, FL
- Miami, FL
- Ocala, FL
- Orlando, FL (Headquarters)
- Palm Coast, FL
- Panama City, FL
- Pensacola, FL
- Rockledge, FL
- Sarasota, FL
- St. Petersburg, FL
- Tampa, FL
- Tifton, GA
- West Palm Beach, FL

October 8, 2021

Circle K Florida
3802 Corporex Park Drive, Suite 200
Tampa, Florida 33619

Attention: Mr. Chris Roick
croick@circlek.com

Reference: **Geotechnical Exploration**
Circle K Store – Lake City
US Highway 90 & I-75
Lake City, Columbia County, Florida
UES Project No. 0730.2100190.0000
UES Docs Report No. 1905351

Dear Mr. Roick:

Universal Engineering Sciences (UES) has completed a geotechnical exploration at the above referenced site in Columbia County, Florida. The scope of our exploration was planned in conjunction with Schaffer Construction and authorized by you. This exploration was performed in general accordance with UES Proposal No. 1880491 dated June 29, 2021 and generally accepted soil and foundation engineering practices. No other warranty, express or implied, is made.

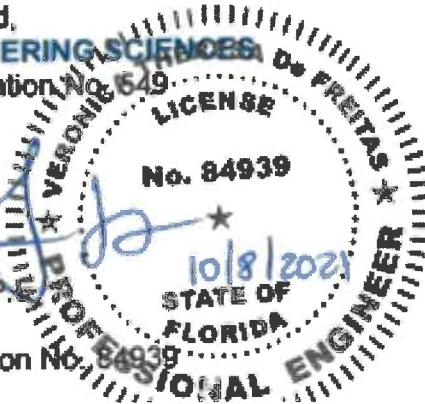
The following report presents the results of our field exploration with a geotechnical engineering interpretation of those results with respect to the project characteristics as provided to us. We have included our estimates of the seasonal high groundwater level at the boring locations and geotechnical recommendations for foundation design, pavement design, and site preparation. *The site was found to be generally suitable for the proposed development construction following typical site preparation procedures presented in this report.*

We appreciate the opportunity to have worked with you on this project and look forward to a continued association. Please do not hesitate to contact us if you should have any questions, or if we may further assist you as your plans proceed.

Respectfully Submitted,

UNIVERSAL ENGINEERING SCIENCES
Certificate of Authorization No. 649

Veronica De Freitas, P.E.
Department Manager
Florida P.R. Registration No. 84939



Mark Hardy, P.E.
Regional Manager



**UNIVERSAL ENGINEERING
SCIENCES**

GEOTECHNICAL EXPLORATION

CIRCLE K STORE – LAKE CITY
US HIGHWAY 90 & I-75
LAKE CITY, COLUMBIA COUNTY, FLORIDA

UES PROJECT No. 0730.2100190.0000
UES DOCS REPORT No. 1905351

PREPARED FOR:

Circle K Florida
3802 Corporex Park Drive, Suite 200
Tampa, Florida 33619

PREPARED BY:

Universal Engineering Sciences
9802 Palm River Road
Tampa, Florida 33619
(813)-470-5606

October 8, 2021

Consultants in: Geotechnical Engineering • Environmental Sciences • Construction Materials Testing • Threshold Inspection
Offices in: Orlando (Headquarters) • Atlanta, GA • Chantilly, VA • Daytona Beach, FL • Fort Myers, FL • Fort Pierce, FL • Gainesville, FL • Hagerstown, MD • Jacksonville, FL • Miami, FL • Ocala, FL • Palm Coast, FL • Panama City, FL • Pensacola, FL • Rockledge, FL • Sarasota, FL • St. Petersburg, FL • Tampa, FL • Tifton, GA • West Palm Beach, FL

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1.0 PROJECT DESCRIPTION

UES understands that the proposed project will include the construction of a new fueling station addition to an existing Circle K gas station and convenience store in Lake City, Florida. The new fueling station will be located north of the existing Circle K gas station, and will include a gas pump canopy, underground storage tanks (UST), and paved driveways. A boring location plan was prepared by UES and approved by Circle K's project manager prior to initiating this geotechnical exploration program.

Structural loading information was provided in Circle K's Geotechnical Investigation Work Scope document dated Revised February 24, 2020. We understand that structural loads will be carried by exterior load bearing walls having a maximum loading of 3.5 kips per linear foot (klf) and isolated interior columns with maximum loads of 60 kips. Floor loads are anticipated to be 175 psf.

No grading information was available at the time of this report. The geotechnical exploration and corresponding boring termination depths were based on the assumption that final site grades and finish floor elevations will be within ± 2 feet of current grades. If grading information becomes available, please contact us so that we may revise the report accordingly.

Should any of the above information or assumptions made by UES be inconsistent with the planned development and construction, we request that you contact us immediately to allow us the opportunity to review the new information in conjunction with our report and revise or modify our engineering recommendations accordingly, as needed.

No site or project facilities/improvements, other than those described herein, should be designed using the soil information presented in this report. Moreover, UES will not be responsible for the performance of any site improvement so designed and constructed.

2.0 PURPOSE

The purposes of this exploration were:

- to explore and evaluate the subsurface conditions at the site with special attention to potential problems that may impact the proposed development,
- to provide our estimates of the seasonal high groundwater level at the boring locations and
- to provide geotechnical engineering recommendations for foundation design, pavement design, and site preparation.

This report presents an evaluation of site conditions on the basis of geotechnical procedures for site characterization. The recovered samples were not examined, either visually or analytically, for chemical composition or environmental hazards. We would be glad to provide you with a proposal for these services at your request.

Our exploration was not designed to specifically address the potential for surface expression of deep geological conditions, such as sinkhole development related to karst activity. This evaluation requires a more extensive range of field services than those performed in this study. We would be pleased to conduct an exploration to evaluate the probable effect of the regional geology upon the proposed construction, if you so desire.

3.0 SITE DESCRIPTION

The subject site is located within Section 35, Township 3 South, Range 16 East in Columbia County, Florida. More specifically, the site is located on the northwest of the intersection of US Highway 90 and I-75 as shown on the attached Figure B-1. At the time of drilling, the site consists of a vacant grassed field, north of an existing Circle K gas station.

3.1 SOIL SURVEY

There are one (1) native soil type mapped within the site area according to the USDA NRCS Soil Survey of Columbia County. A brief summary of the mapped surficial (native) soil type(s) is presented in Table I.

TABLE I
SUMMARY OF PUBLISHED SOIL DATA

Soil Symbol	Soil Type	Hydrologic Group	Drainage Characteristics	Depth of Published Seasonal High GWT (feet)
8	Blanton fine sand, 0 to 5 percent slopes	A	Moderately well drained	3½ to 6

3.2 TOPOGRAPHY

According to information obtained from the United States Geologic Survey (USGS) "Lake City, Florida" quadrangle map, the native ground surface elevation across the site area is approximately +140 to +145 feet National Geodetic Vertical Datum (NGVD). A copy of a portion of the USGS Map is included in Appendix A.

4.0 SCOPE OF SERVICES

The services conducted by UES during our geotechnical exploration were as follows:

- Drilled four (4) Standard Penetration Test (SPT) borings within the proposed fueling station, UST pit area, canopy to depths of 10 to 25 feet below existing land surface (bls).
- Secured samples of representative soils encountered in the soil borings for review, laboratory analysis and classification by a Geotechnical Engineer.
- Measured the existing site groundwater levels and provide an estimate of the seasonal high groundwater level at the boring locations.
- Assessed the existing soil conditions with respect to the proposed construction.

- Prepared a report which documents the results of our exploration and analysis with geotechnical engineering recommendations.

5.0 FIELD EXPLORATION

The SPT soil borings were performed with a track mounted drilling rig. Horizontal and vertical survey control was not provided for the test locations prior to our field exploration program. UES located the test borings by using the provided site plan, measuring from existing on-site landmarks shown on an aerial photograph, and by using handheld GPS devices. The indicated test locations should be considered accurate to the degree of the methodologies used. The approximate boring locations are shown in Appendix B.

The SPT borings, designated B-1 through B-4 on the attached Boring Location Plan in Appendix B, were performed in general accordance with the procedures of ASTM D 1586 “Standard Method for Penetration Test and Split-Barrel Sampling of Soils”. SPT sampling was performed continuously to 10 feet to detect variations in the near surface soil profile and on approximate 5 feet centers thereafter.

6.0 SUBSURFACE CONDITIONS

The results of our field exploration and laboratory analysis, together with pertinent information obtained from the SPT borings, such as soil profiles, penetration resistance and groundwater levels are shown on the boring logs included in Appendix B. The Key to Boring Logs, Soil Classification Chart is also included in Appendix B. The soil profiles were prepared from field logs after the recovered soil samples were examined by a Geotechnical Engineer. The stratification lines shown on the boring logs represent the approximate boundaries between soil types, and may not depict exact subsurface soil conditions. The actual soil boundaries may be more transitional than depicted. A generalized profile of the soils encountered at our boring locations is presented in Table II. For detailed soil profiles, please refer to the attached boring logs.

**TABLE II
 GENERALIZED SOIL PROFILE**

Typical Depth (feet, bls)		Soil Description	Range of SPT “N” Values (blows/ft)
From	To		
Surface	2	Loose to medium dense fine SAND [SP] to fine SAND with clay [SP-SP]	7 to 15
2	25*	Very loose to dense clayey SAND [SC] to firm CLAY with varying amounts of sand [CL]	3 to 40

* denotes maximum termination depth of the borings

7.0 GROUNDWATER CONDITIONS

7.1 EXISTING GROUNDWATER LEVEL

We measured the water levels in the boreholes on September 30, 2021 during drilling operations. The encountered groundwater levels at the boring locations ranged from approximately 3 to 3 ½ feet bls. The encountered water levels at the boring locations are shown on the individual boring logs in Appendix B. Fluctuations in groundwater levels should be anticipated throughout the year, primarily due to seasonal variations in rainfall, surface runoff, and other factors that may vary from the time the borings were conducted.

7.2 SEASONAL HIGH GROUNDWATER LEVEL

Based on historical data, the rainy season in Central Florida is between June and October of the year. In order to estimate the seasonal high water level at the boring locations, many factors are examined, including the following:

- Measured groundwater level
- Drainage characteristics of existing soil types
- Current & historical rainfall data
- Natural relief points (such as lakes, rivers, wetlands, etc.)
- Man-made drainage systems (ditches, canals, retention basins, etc.)
- On-site types of vegetation
- Review of available data (soil surveys, USGS maps, etc.)
- Redoximorphic features (mottling, stripping, etc.)

Please note that the presence of hydraulically restrictive clayey sands (SC) encountered at depths on the order of 2 to 6 feet throughout the site may form a transient perched groundwater condition, especially after periods of heavy rainfall and/or irrigation. Perched groundwater levels can generally be expected to occur about 6 inches to 2 feet above the top of hydraulically restrictive soils, where present, if the groundwater table is unable to drain and/or percolate into a more pervious layer. It should be noted that undercutting of the hydraulically restrictive materials will impact the depth of the perched water table. The potential for groundwater to perch will be directly related to rainfall and irrigation amounts, as well as site grading. The potential for transient perched groundwater levels should be considered during the design of the site grades and during construction.

Based on the results of our field exploration and the factors listed above, we estimate that the seasonal high groundwater level at the boring locations should occur roughly 2 to 2 ½ feet bls or 6 inches above hydraulically restrictive clayey sand (SC), whichever comes first. The estimated seasonal high groundwater table at each boring location is shown on the attached boring logs in Appendix B.

It should be noted that the estimated seasonal high water levels provided should be considered accurate to approximately ±½ foot and do not provide any assurance that groundwater levels will not exceed these estimated levels during any given year in the future. Should the impediments to surface water drainage be present, or should rainfall intensity and duration, or total rainfall

quantities, exceed the normally anticipated rainfall quantities, groundwater levels might exceed our seasonal high estimates. Further, it should be understood that changes in the surface hydrology and subsurface drainage from on-site and/or off-site improvements could have significant effects on the normal and seasonal high groundwater levels.

8.0 SEISMIC SITE CLASSIFICATION

The project site is located within a municipality that employs the Florida Building Code (FBC) which has jurisdiction in the State of Florida. Since seismic design is not part of the FBC, we consulted the 2015 International Building Code® (IBC). As part of this Code, the design of structures must consider dynamic forces resulting from seismic events. These forces are dependent upon the magnitude of the earthquake event, as well as the properties of the soils that underlie the site. As part of the procedure to evaluate seismic forces, the Code requires the evaluation of the Seismic Site Class, which categorizes the site based upon the characteristics of the subsurface profile within the upper 100 feet of the ground surface.

To define the Site Class for this project, we first interpreted the results of SPT soil borings drilled within the project site and estimated appropriate soil properties below the base of the borings to a depth of 100 feet, as permitted by Section 1615.1.1 of the Code. The estimated soil properties were based upon our experience with subsurface conditions in the general site area.

Based upon the SPT N-values recorded during the field exploration and our experience in the vicinity of the subject site, the subsurface conditions within the site are consistent with the characteristics of a **Site Class “D”** as defined in Chapter 20 of ASCE 7.

9.0 FOUNDATION DESIGN RECOMMENDATIONS

The following recommendations are made based upon a review of the attached soil test data, our understanding of the proposed construction, and experience with similar projects and subsurface conditions. The applicability of geotechnical recommendations is very dependent upon project characteristics such as improvement locations, and grade alterations. UES must review the final site and grading plans to validate all recommendations rendered herein.

Additionally, if subsurface conditions are encountered during construction, which were not encountered in the borings, report those conditions immediately to us for observation and recommendations.

9.1 STRUCTURAL AND GRADING INFORMATION

It is our understanding that the project will include the construction of a new fueling station addition to an existing Circle K gas station and convenience store in Lake City, Florida. We understand from Circle K’s Geotechnical Investigation Work Scope document, the maximum loads will not exceed 60 kips for individual columns and 3.5 kips/ft for structural walls. Floor loads will not exceed 175 psf. We assume that the finished floor elevation of the new construction will be near existing grades.

Prior to finalizing any design, the structural/grading information outlined above should be confirmed by the project structural/civil engineer. This is crucial to our evaluation and estimates

of settlements. If any of this information is incorrect or if you anticipate any changes, please inform UES immediately so that we may review and modify our recommendations as appropriate.

9.2 ANALYSIS

Based on the results of the soil borings, the near surface soils within the proposed construction area appear to be mostly loose to medium dense sands [SP, SP-SM] to a depth of 2 to 6 feet, followed by very loose to dense clayey sands [SC] and firm clays [CL] extending to 25 feet. It is our opinion that proposed fueling station addition can be supported on properly designed and constructed shallow foundation systems. Provided that the site preparation recommendations outlined in this report are followed, the parameters outlined below may be used for foundation design.

9.3 BEARING PRESSURE

Provided our suggested site preparation procedures are followed, we recommend designing shallow footing foundations for a **maximum allowable net soil bearing pressure of 2,500 pounds per square foot (psf)**. The allowable net bearing pressure is that pressure that may be transmitted to the soil in excess of the minimum surrounding overburden pressure. The allowable bearing pressure should include dead load plus sustained live load. The foundations should be designed for the most unfavorable effects due to the combinations of loads specified in the FLBC.

9.4 FOUNDATION SIZE

The minimum width recommended for an isolated column footing is 24 inches. For continuous wall or slab on grade foundations, the minimum footing width should comply with the current FLBC, but under no circumstances should be less than 12 inches. Even though the maximum allowable soil bearing pressure may not be achieved, these width recommendations should control the size of the foundations.

9.5 BEARING DEPTH

The base of all footings should be at least 12 inches below finished grade elevation in accordance with the FLBC. We recommend stormwater and surface water be diverted away from the proposed fueling station footprint area, both during and after construction, to reduce the possibility of erosion beneath the exterior footings.

9.6 BEARING MATERIAL

The bearing level soils should exhibit a density of at least 95 percent of the maximum dry density as determined by ASTM D 1557 (Modified Proctor) to a depth of at least **2 feet below foundation level** as described in this report. In addition to compaction, the bearing soils must exhibit stability and be free of "pumping" conditions.

9.7 SETTLEMENT ESTIMATES

Post-construction settlement of the structures will be influenced by several interrelated factors, such as (1) subsurface stratification and strength/compressibility characteristics of the bearing soils to a depth of approximately twice the width of the footing; (2) footing size, bearing level, applied loads, and resulting bearing pressures beneath the foundation; (3) site preparation and

earthwork construction techniques used by the contractor, and (4) external factors, including but not limited to vibration from off site sources and groundwater fluctuations beyond those normally anticipated for the naturally-occurring site and soil conditions which are present.

Our settlement estimates for the structures are based upon adherence to our recommended site preparation procedures presented in this report. Any deviation from these recommendations could result in an increase in the estimated post-construction settlement of the structures. Furthermore, should structural loads change from those assumed by us, greater settlements may be expected.

Due to the sandy nature of the surficial soils following the compaction operations, we expect the majority of settlement to be elastic in nature and occur relatively quickly, on application of the loads, during and immediately following construction. Using the recommended maximum allowable bearing pressure, the assumed maximum structural loads, and the field and laboratory test data which we have correlated into the strength and compressibility characteristics of the subsurface soils, **we estimate the total vertical settlement of the proposed structure to be on the order of 1 inch or less.**

Differential settlement results from differences in applied bearing pressures and the variations in the compressibility characteristics of the subsurface soils. Assuming our site preparation recommendations are followed, **we anticipate differential settlement of less than ½ inch.**

9.8 FLOOR SLABS

If required for new structures, a conventional floor slabs may be supported upon the compacted fill and should be structurally isolated from other foundation elements or adequately reinforced to prevent distress due to differential movements. For the slab design, we recommend using a subgrade modulus (k) of 100 pounds per cubic inch, which can be achieved by compacting the subgrade soils as recommended in this report. We recommend using a sheet vapor barrier (in accordance with Florida Building Code requirements) beneath the slab-on-grade to help control moisture migration through the slab.

10.0 PAVEMENT RECOMMENDATIONS

10.1 GENERAL

We understand that a combination of flexible asphaltic and rigid concrete pavement sections will be used on this project. We understand from Circle K's Geotechnical Investigation Work Scope document (dated Revised February 24, 2020) that the following ESALs should be used as the basis of pavement designs:

- Normal/Light Duty 250,000 ESALs
- Heavy Duty 1,800,000 ESALs
- Expected Pavement Service Life 20 years

In addition, the following assumptions have been made:

- Reliability of 85 percent

- Standard Deviation of 0.45
- Subgrade Resilient Modulus of 7,500 psi
- Initial Serviceability of 4.5
- Terminal Serviceability of 2.5

Our recommendations for minimum section thicknesses and subgrade preparation for both pavement types are listed in the following sections.

10.2 ASPHALTIC PAVEMENTS

10.2.1 Layer Components

Based on the results of our soil borings and review of the 2020 FDOT Flexible Pavement Design Manual, our minimum recommended pavement component thicknesses are presented in Table III.

**TABLE III
 MINIMUM ASPHALTIC PAVEMENT COMPONENT THICKNESSES**

Service Level	Maximum Traffic Loading	Layer Component			Estimated Structural Number
		Surface Course (inches)	Base Course (inches)	Stabilized Subgrade (inches)	
Normal/ Light Duty	up to 250,000 E ₁₈ SAL	2	6	12	2.7
Heavy Duty	up to 1,800,000 E ₁₈ SAL	3	8	12	3.5

10.2.2 Stabilized Subgrade

We recommend that the stabilized subgrade materials immediately beneath the base course exhibit a minimum Limerock Bearing Ratio (LBR) of 40 as specified by FDOT compacted to at least 98 percent of the Modified Proctor maximum dry density (ASTM D 1557) value.

Stabilized subgrade can be imported materials or a blend of on-site and imported materials. If a blend is proposed, we recommend that the contractor perform a mix design to find the optimum mix proportions.

Compaction testing of the stabilized subgrade should be performed to full depth at a frequency of at least one (1) test per 10,000 square feet, or a minimum of 4 tests, whichever is greater.

10.2.3 Base Course

Based on the results of our exploration and our experience in the project area, limerock and crushed concrete are suitable base course materials for this project. However, local municipality standards may govern the use of crushed concrete use as an alternative base course material. We recommend the civil engineer consult with the local municipalities prior to selecting the base course material for this project.

For a limerock base, the base course should be compacted to a minimum density of 98 percent of the Modified Proctor maximum dry density and exhibit a minimum LBR of 100. The limerock material should comply with the latest edition of the Florida Department of Transportation (FDOT) Road and Bridge Construction specifications.

Recycled concrete aggregate (RCA) may provide a cost-effective alternative material in lieu of a limerock base course. Local availability, along with municipality standards, typically governs the use of crushed concrete use as an alternative base course material. The advantages of using RCA as a pavement base course include its high strength (stronger than limerock), resistance to groundwater related distress, and lack of reflection cracking caused by thermal expansion and contraction.

If a RCA base is used, the base course material should be sourced from an FDOT approved supplier. The base should be compacted to a minimum density of 98 percent of the Modified Proctor maximum dry density and exhibit a minimum LBR of 150. The base material should comply with the criteria listed in the latest edition of the FDOT Road and Bridge Construction Specifications.

Compaction testing of the base course should be performed to full depth at a frequency of at least one (1) test per 10,000 square feet.

10.2.4 Surface Course

For the pavements, we recommend that the surfacing consist of FDOT SuperPave (SP) asphaltic concrete. The surface course should consist of FDOT SP-9.5 fine mix for light-duty areas and FDOT SP-12.5 topped with SP-9.5 fine mix for heavy duty areas. The asphalt concrete should be placed within the allowable lift thicknesses for fine Type SP mixes per the latest edition of FDOT, Standard Specifications for Road and Bridge Construction.

The asphaltic concrete should be compacted to an average field density of 93 percent of the laboratory maximum density determined from specific gravity (G_{mm}) methods, with an individual test tolerance of **+2 percent and -1.2% of the design G_{mm}** . Specific requirements for the SuperPave asphaltic concrete structural course are outlined in the latest edition of FDOT, Standard Specifications for Road and Bridge Construction.

Note: If the Designer (or Contract Documents) limits compaction to the static mode only or lifts are placed one-inch thick, then the average field density should be 92 percent, with an individual test tolerance of + 3 percent, and -1.2% of the design G_{mm} .

After placement and field compaction, the wearing surface should be cored to evaluate material thickness and density. Cores should be obtained at frequencies of at least one (1) core per 10,000 square feet of placed pavement, or a minimum of two (2) cores per day's production.

10.2.5 Effects of Groundwater

One of the most critical influences on the pavement performance in Central Florida is the relationship between the pavement base course and the seasonal high groundwater level. Sufficient separation will need to be maintained between the bottom of base course and the

anticipated seasonal high groundwater level. We recommend that the seasonal high groundwater and the bottom of the base course be separated by at least 12 inches for crushed concrete base course, and at least 18 inches for a limerock base course. **Based on the groundwater conditions encountered, the separation should not be an issue for pavements constructed at existing grade.**

10.2.6 Landscape Areas

In the event that landscape areas adjacent to the pavements include large mounds (>1 foot) of poorly draining organic topsoils or silty/clayey sands, we recommend that landscape drains be provided to protect the roadway against adverse effects from over-irrigation or excess rainfall. Poorly draining silty and clayey material causes the irrigation and rainwater to perch and migrate laterally into the pavement components, which eventually compromises the integrity of the pavement section.

10.3 CONCRETE “RIGID” PAVEMENTS

Concrete pavement is a rigid pavement that is strong, durable and handles the heavy loads more effectively than asphalt pavement. We assume that concrete pavement may be used in the canopy, driveway and tank mat areas. In addition, concrete pavement is recommended under the dumpster area, and 10 feet in front of the trash enclosure, at a minimum.

We understand from Circle K’s Geotechnical Investigation Work Scope document (dated Revised 02-24-20) that the following ESALs should be used as the basis of pavement designs:

- Normal/Light Duty 250,000 ESALs
- Heavy Duty 1,800,000 ESALs
- Expected Pavement Service Life 20 years

In addition, the following assumptions have been made:

- Concrete Elastic Modulus of 4,000,000 psi
- Concrete Modulus of Rupture of 650 psi
- Reliability of 85 percent
- Standard Deviation of 0.45
- Modulus of Subgrade Reaction of 100 pci
- Initial Serviceability of 4.5
- Terminal Serviceability of 2.5

We recommend preparing the proposed concrete pavement areas as recommend in Section 13.0 of this report with the following stipulations:

1. The subgrade immediately beneath the concrete should be compacted to at least 98 percent of the Modified Proctor maximum dry density (ASTM D 1557) value.
2. The surface of the subgrade soils must be smooth, and any disturbances or wheel rutting corrected prior to placement of concrete.

3. The subgrade soils must be moistened prior to placement of concrete.
4. Concrete pavement thickness should be uniform throughout, with exception to the thickened edges (curb or footing).
5. The bottom of the pavement should be separated from the seasonal high groundwater level by at least 12 Inches.

Based on the results of the soil borings and review of the FDOT Rigid Pavement Design Manual, we recommend using the minimum design shown in Table IV for concrete pavements.

**TABLE IV
MINIMUM CONCRETE PAVEMENT THICKNESSES**

Service Level	Minimum Pavement Thickness	Maximum Control Joint Spacing	Recommended Saw Cut Depth
Normal/Light Duty	6 inches	12 feet x 12 feet	2 inches
Heavy Duty	8 inches	14 feet x 14 feet	2-2/3 inches

We recommend using concrete with a minimum 28-day compressive strength of at least 4,000 pounds per square inch and contain fiber reinforcement. Layout of the Saw cut control joints should form square panels, and the depth of Saw cut joints should be $\frac{1}{3}$ of the concrete slab thickness.

We recommend allowing UES to review and comment on the final concrete pavement design, including section and joint details (type of joints, joint spacing, etc.), prior to the start of construction.

For further details on concrete pavement construction, please reference the "Guide to Jointing of Non-Reinforced Concrete Pavements" published by the Florida Concrete and Products Association, Inc., and "Building Quality Concrete Parking Areas", published by the Portland Cement Association.

Specimens to verify the compressive strength of the pavement concrete should be obtained for at least every 50 cubic yards, or at least once for each day's placement, whichever is greater.

11.0 EARTH RETAINING WALLS

At this time, Universal is **not** aware of any planned retaining walls at the site. The following recommendations are provided in the event low-level (i.e. less than 4 feet) walls are required.

Earth pressures on retaining walls are influenced by the structural design of walls, conditions of wall restraint, construction methods, and the strength of the materials being restrained. The most common conditions assumed for earth retaining wall design are the active and at-rest conditions.

Active conditions apply to relatively flexible earth retention structures, such as freestanding walls, where some movement and rotation may occur to mobilize shear strength. Walls which are rigidly restrained should be designed for the at-rest condition. However, if the walls will be backfilled before they are braced, they should also be designed to withstand active earth pressures as self-supporting cantilever walls. The wall designer must select the appropriate earth pressure based upon site and design constraints.

Development of the full active earth pressure case requires a magnitude of horizontal wall movement that often cannot be tolerated or cannot occur due to the rigidity of the wall and other design restrictions such as the impact on adjacent structures. In such cases, walls are often designed for either the at-rest condition or a condition intermediate of the active and at-rest conditions, depending on the amount of permissible wall movement.

Passive earth pressure represents the maximum possible pressure when a structure is pushed against the soil, and is used in wall foundation design to help resist active or at-rest pressures. Because significant wall movements are required to develop the passive pressure, the total calculated passive pressure is usually reduced by one-half for design purposes.

Our recommendations assume that the ground surface behind the earth retaining structures is level and that native or imported soils consisting of relatively clean sandy soils containing less than 12 percent passing the No. 200. We recommend that the soils selected for use as backfill be tested as specified prior to commencement of wall construction. Recommended soil parameters for design of earth retaining structures have been presented in Table V below.

**TABLE V
 LATERAL EARTH PRESSURE DESIGN PARAMETERS (LEVEL BACKFILL)***

Design Parameter	Recommended Value
At-rest Earth Pressure Coefficient, K_0	0.50
Active Earth Pressure Coefficient, K_a	0.33
Passive Earth Pressure Coefficient, K_p	3.0
Moist Unit Soil Weight (pcf)	115 for SP, SP-SM
Submerged Unit Weight of Soil (pcf)	52
Coefficient of Friction (sliding)	0.4
Angle of Internal Friction, ϕ	30
Table Notes: * For sloping backfill the table values must be adjusted. **Hydrostatic pressure should be accounted for based on seasonal high water table estimates and other site drainage considerations	

Positive wall drainage must be provided for all earth retaining structures to prevent the build-up of excess hydrostatic pressures. These drainage systems can be constructed of open-graded

washed stone isolated from the soil backfill with a geosynthetic filter fabric and drained by perforated pipe, or with one of several wall drainage products made specifically for this application.

Lateral earth pressures arising from surcharge loading (i.e. traffic loading, building/structure loads, etc.) should be added to the above earth pressures to determine the total lateral pressure. Additional consideration must also be given for sloped backfill at the top of the wall. In each circumstance the earth pressures for active and at-rest conditions will increase based upon the amount of surcharge and angle above horizontal of the sloped backfill. Retaining walls should also be analyzed for both internal and global stability.

12.0 SITE PREPARATION

We recommend normal, good practice site preparation procedures for the new construction areas. These procedures include: stripping/clearing of the site to remove existing improvements, vegetation, roots, organic topsoils, debris, etc. Following stripping, the exposed subgrade soils should be proof-rolled, and all subgrade and subsequent fill/backfill soils should be properly densified. A more detailed description of this work is presented in this section.

1. Prior to construction, existing underground utility lines and other below grade structures within the construction area should be located. Provisions should be made to relocate interfering utilities to appropriate locations. It should be noted that if underground improvements are not properly removed or plugged, they may serve as conduits for subsurface erosion which may lead to excessive settlement of overlying structures.
2. Strip the proposed construction limits of vegetation, topsoil, existing improvements, roots, debris and other deleterious materials within and 5 feet beyond the perimeter of the new construction areas. Expect clearing and grubbing to depths of 6 to 12 inches. Deeper clearing and grubbing depths should be anticipated within the developed areas to remove buried improvements. We strongly recommend that the stripped/excavated surfaces be observed and probed by representatives of UES.
3. Proof-roll the exposed subsurface soils under the observation of UES, to locate any soft areas of unsuitable soils, and to increase the density of the shallow loose fine sand soils. If deemed necessary by UES, in areas that continue to "yield", remove any deleterious materials and replace with a clean, compacted sand backfill.
4. Place fill as necessary. All fill should consist of clean sand with less than 12 percent soil fines and be free of organics, debris and other deleterious materials. Fill soils containing between 5 and 12 percent fines may require strict moisture control. Place fill in maximum 12-inch loose, uniform lifts and compact each lift at least 95 percent of the Modified Proctor maximum dry density.
5. Within the at-grade (or below grade) foundation areas, subgrade compaction of at least 95 percent of the Modified Proctor should be achieved to a depth of at least 2 feet below bottom of foundation/slab levels.

6. Within the pavement areas, the upper 12 inches of subgrade beneath the base course or concrete slabs (sub-base) should be stabilized and compacted to at least 98 percent of the Modified Proctor maximum dry density.
7. Test the subgrade and each lift of fill for compaction at a frequency of not less than one test per 2,500 square feet in the building areas and one test per 10,000 square feet in the pavement areas, with a minimum of 4 tests in each area.
8. Prior to the placement of reinforcing steel and concrete, verify compaction within the footing trenches to a depth of 2 feet. We recommend testing every column footing and at least one test every 100 feet of wall footing, with a minimum of 4 tests per building. Re-compaction of the foundation excavation bearing level soils, if loosened by the excavation process, can typically be achieved by making several passes with a walk-behind vibratory sled or jumping jack.

Stability of the compacted soils is essential and independent of compaction and density control. If the near surface soils or the structural fill experience “pumping” conditions, terminate all earthwork activities in that area. Pumping conditions occur when there is too much water present in the soil-water matrix. Earthwork activities are actually attempting to compact the water and not the soil. The disturbed soils should be dried in place by scarification and aeration prior to any additional earthwork activities.

Vibrations produced during vibratory compaction operations at the site may be significantly noticeable within 100 feet and may cause distress to adjacent structures if not properly regulated. Provisions should be made to monitor these vibrations so that any necessary modifications in the compaction operations can be made in the field before potential damages occur. UES can provide vibration monitoring services to help document and evaluate the effects of the surface compaction operation on existing structures. It is recommended that large vibratory rollers remain a minimum of 50 feet from existing structures. Within this zone, the use of a static roller or small hand guided plate compactors is recommended.

13.0 UST PIT AREA – GENERAL COMMENTS

We assume the excavation for the proposed UST pit area will be on the order of 10 to 20 feet below the ground surface. Based on the results of Boring B-1 (performed within the proposed pit area), the subsoils at this level appear to be very loose to medium dense clayey sands (SC). Based on the subsurface conditions encountered, it is our opinion the subgrade soils are suitable for supporting the proposed underground tanks.

The groundwater table was encountered at a depth of 3½ feet bls at the UST tank pit location. Temporary dewatering will be necessary to achieve the necessary excavation and compaction within the tank area. Excavation procedures should conform to the OSHA regulations (Please see section 16.0 of this report).

After the excavation for the tanks is complete, we recommend that the bottom of the excavation be compacted by small hand guided equipment to achieve at least 95 percent of the Modified Proctor maximum dry density (ASTM D-1557) to a depth of 1 foot. If the bottom of excavation is

unstable due to excessive fines and/or wet conditions, graded aggregate (FDOT 57 stone) can be placed in 3 to 6 inch lifts in the bottom of the over-excavation with compaction equipment (i.e. jumping jack) until a firm, non-yielding subgrade is achieved. Pea gravel or approved free-draining bedding soils should be placed below tanks in accordance with tank manufacturer's specifications.

After completion of the tank installation, backfill should be placed in uniform 12 inch (or less) lifts and compacted to at least 95 percent of Modified Proctor Test maximum dry density (ASTM D 1557), with small hand guided equipment. Backfill should consist of clean sand with less than 12 percent soil fines and be free of organics, debris and other deleterious materials.

When the fluid level within the fuel tank structure is maintained at or above the surrounding groundwater level, no net buoyancy will occur. However, when these structures are drained for maintenance or as fluid levels fluctuate within the tanks, a positive means of uplift protection may be necessary, depending on the future groundwater levels in order to prevent hydrostatic uplift forces moving the tank. Since groundwater was encountered near and above the bottom of the pit we recommend this protection be added several ways this can be accomplished include the following:

- Addition of dead weight to the structure.
- Mobilizing the dead weight of the soil surrounding the structure through extension of footings outside the perimeter of the structure.
- Use of a permanent gravity or mechanical dewatering system that is operated only when the structure is to be drained.

14.0 DEWATERING AND EXCAVATION CONSIDERATIONS

Based on the groundwater level conditions encountered, temporary dewatering will be required for the successful construction of this project. Where excavations will extend only a few feet below the groundwater table, a sump pump may be sufficient to control the groundwater table. Deeper excavations may require well points and/or sock drains to control the groundwater table. Regardless of the method(s) used, we recommend drawing down the water level at least 2 feet below the bottom of the excavation. The actual method(s) of dewatering should be determined by the contractor. The design and discharge of the dewatering system must be performed in accordance with applicable regulatory criteria (i.e. water management district, etc.) and compliance with such criteria is the sole responsibility of the contractor.

Excavations should be sloped as necessary to prevent slope failure and to allow backfilling. As a minimum, temporary excavations below 4-foot depth should be sloped in accordance with OSHA regulations. Where lateral confinement will not permit slopes to be laid back, the excavation should be shored in accordance with OSHA requirements. During excavation, excavated material should not be stockpiled at the top of the slope within a horizontal distance equal to the excavation depth. Provisions for maintaining workman safety within excavations is the sole responsibility of the contractor.

15.0 CONSTRUCTION RELATED SERVICES

We recommend the owner retain UES to provide inspection services during the site preparation procedures for confirmation of the adequacy of the earthwork operations. Field tests and observations include verification of foundation and pavement subgrades by monitoring earthwork operations and performing quality assurance tests of the placement of compacted structural fill courses.

The geotechnical engineering design does not end with the advertisement of the construction documents. The design is an on-going process throughout construction. Because of our familiarity with the site conditions and the intent of the engineering design, we are most qualified to address site problems or construction changes, which may arise during construction, in a timely and cost-effective manner.

16.0 LIMITATIONS

This report has been prepared for the exclusive use of **Circle K Florida** and other designated members of their design/construction team associated with the proposed construction for the specific project discussed in this report. No other site or project facilities should be designed using the soil information contained in this report. As such, UES will not be responsible for the performance of any other site improvement designed using the data in this report.

This report should not be relied upon for final design recommendations or professional opinions by unauthorized third parties without the expressed written consent of UES. Unauthorized third parties that rely upon the information contained herein without the expressed written consent of UES assume all risk and liability for such reliance.

The recommendations submitted in this report are based upon the data obtained from the soil borings performed at the locations indicated on the Boring Location Plan and from other information as referenced. This report does not reflect any variations which may occur between the boring locations. The nature and extent of such variations may not become evident until the course of construction. If variations become evident, it will then be necessary for a re-evaluation of the recommendations of this report after performing on-site observations during the construction period and noting the characteristics of the variations.

Borings for a typical geotechnical report are widely spaced and generally not sufficient for reliably detecting the presence of isolated, anomalous surface or subsurface conditions, or reliably estimating unsuitable or suitable material quantities. Accordingly, UES does not recommend relying on our boring information for estimation of material quantities unless our contracted services **specifically** include sufficient exploration for such purpose(s) and within the report we so state that the level of exploration provided should be sufficient to detect anomalous conditions or estimate such quantities. Therefore, UES will not be responsible for any extrapolation or use of our data by others beyond the purpose(s) for which it is applicable or intended.

All users of this report are cautioned that there was no requirement for UES to attempt to locate any man-made buried objects or identify any other potentially hazardous conditions that may exist at the site during the course of this exploration. Therefore, no attempt was made by UES to locate

or identify such concerns. UES cannot be responsible for any buried man-made objects or environmental hazards which may be subsequently encountered during construction that are not discussed within the text of this report. We can provide this service if requested.

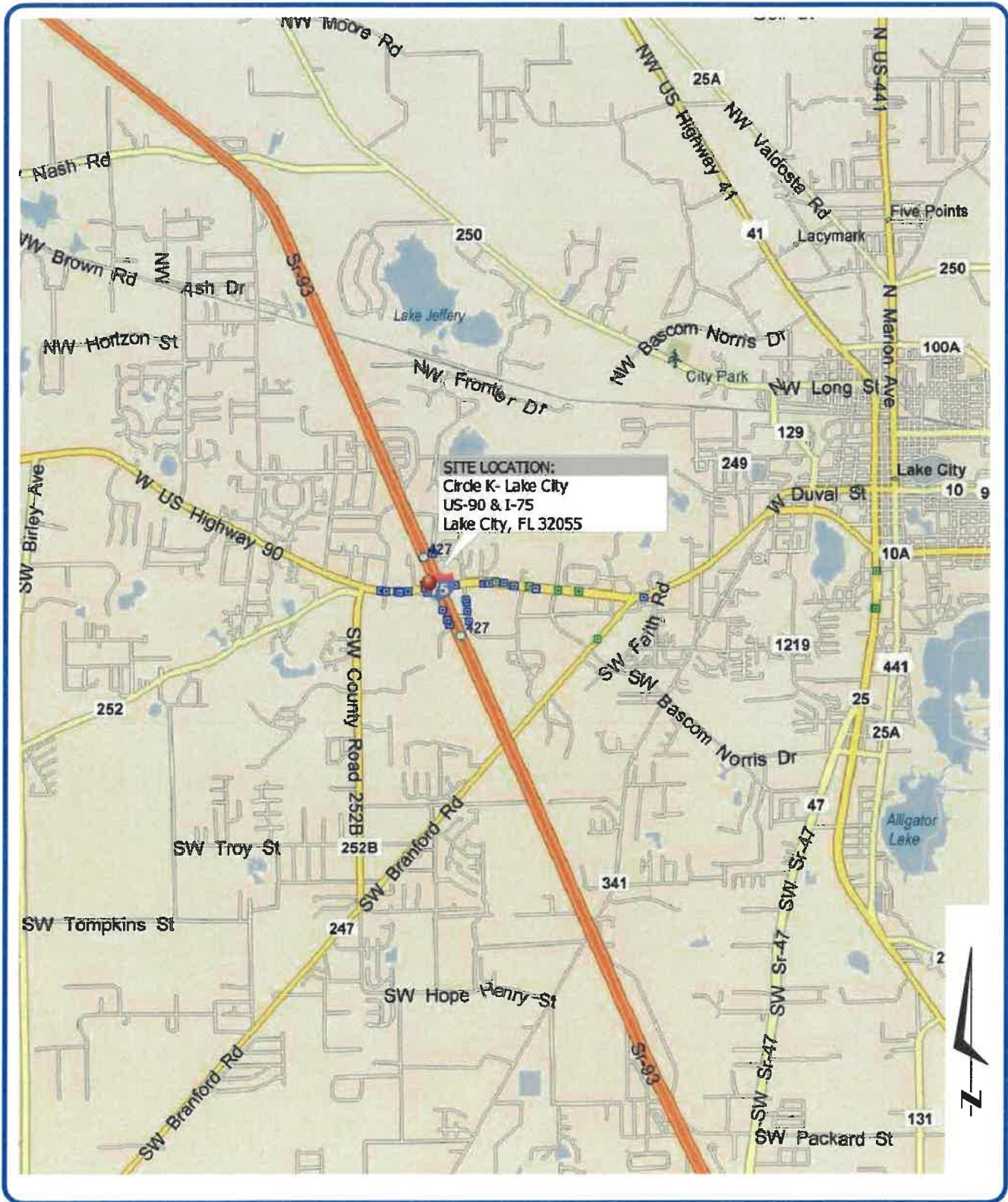
During the early stages of most construction projects, geotechnical issues not addressed in this report may arise. Because of the natural limitations inherent in working with the subsurface, it is not possible for a geotechnical engineer to predict and address all possible problems. A Geotechnical Business Council (GBC) publication, "Important Information About This Geotechnical Engineering Report" appears in Appendix C, and will help explain the nature of geotechnical issues.

Further, we present documents in Appendix C: Constraints and Restrictions, to bring to your attention the potential concerns and the basic limitations of a typical geotechnical report.

* * * * *

APPENDIX A





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CIRCLE K - LAKE CITY
US HIGHWAY 90 & I-75
LAKE CITY, COLUMBIA COUNTY, FLORIDA

SITE LOCATION MAP

CLIENT: CIRCLE K STORES, INC

DRAWN BY: SC

DATE: SEP. 8, 2021

SCALE:

PROJECT NO: 0730.2100190

REVIEWED BY: VD

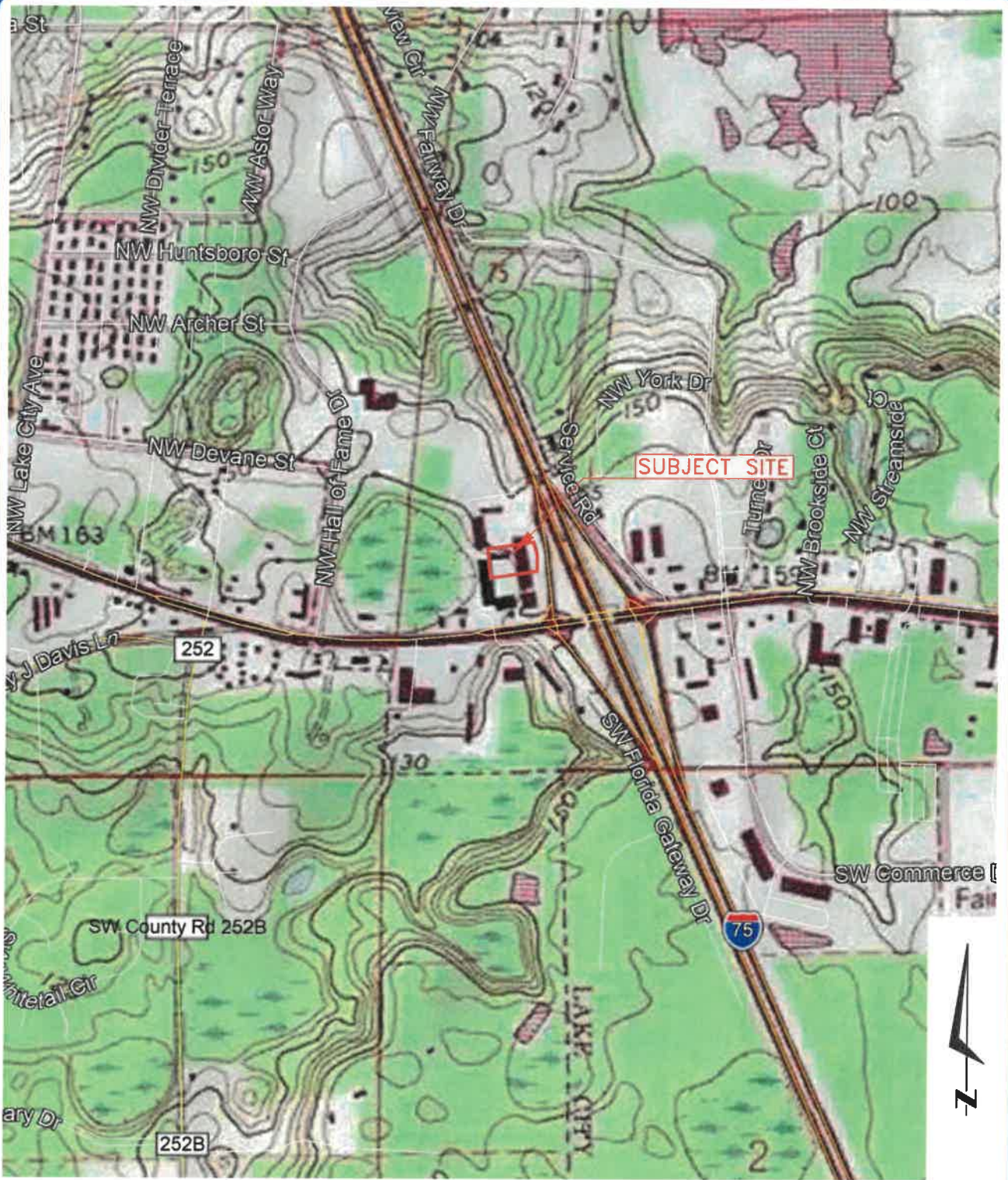
APPENDIX: A



CIRCLE K - LAKE CITY
US HIGHWAY 90 & I-75
LAKE CITY, COLUMBIA COUNTY, FLORIDA

SITE AERIAL PHOTOGRAPH

CLIENT: CIRCLE K STORES, INC	DRAWN BY: SC	DATE: SEP. 8, 2021
SCALE:	PROJECT NO: 0730.2100190	REVIEWED BY: VD
		APPENDIX: A



CIRCLE K - LAKE CITY
US HIGHWAY 90 & I-75
LAKE CITY, COLUMBIA COUNTY, FLORIDA

SITE TOPOGRAPHIC MAP

CLIENT: CIRCLE K STORES, INC

DRAWN BY: SC

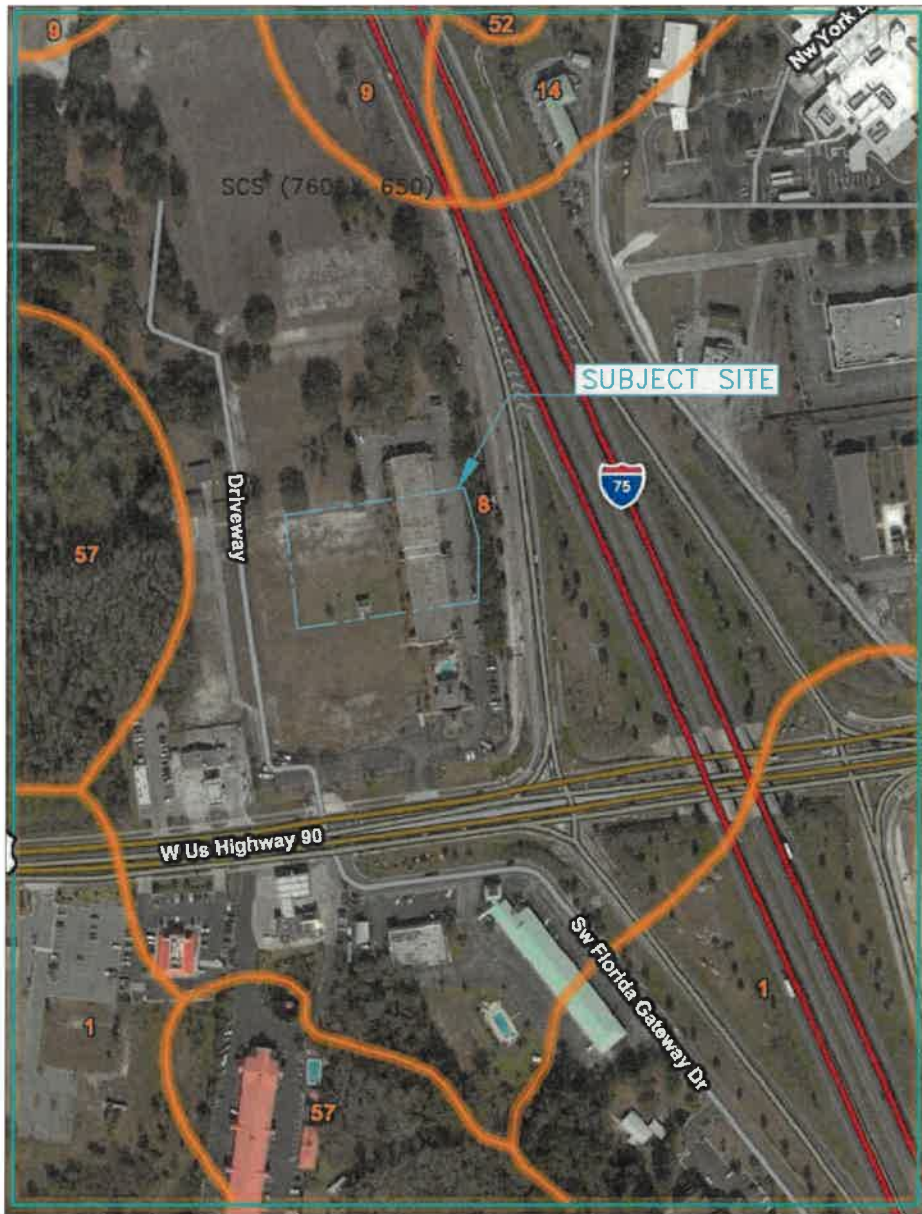
DATE: SEP. 8, 2021

SCALE:

PROJECT NO: 0730.2100190

REVIEWED BY: VD

APPENDIX: A



Map Unit Symbol	Map Unit Name	Acres in AOI	Percent of AOI
1	Albany fine sand, 0 to 5 percent slopes	16.9	19.4%
8	Benton fine sand, 0 to 5 percent slopes	55.8	63.7%
9	Benton fine sand, 5 to 8 percent slopes	2.2	2.5%
14	Bonnesu fine sand, 5 to 8 percent slopes	2.9	3.3%
52	Plummer fine sand, depositional	0.1	0.2%
57	Surrency fine sand	9.6	11.0%
Totals for Area of Interest		87.3	100.0%



**CIRCLE K - LAKE CITY
US HIGHWAY 90 & I-75
LAKE CITY, COLUMBIA COUNTY, FLORIDA**

SOIL SURVEY MAP

CLIENT: CIRCLE K STORES, INC

DRAWN BY: SC

DATE: SEP. 8, 2021

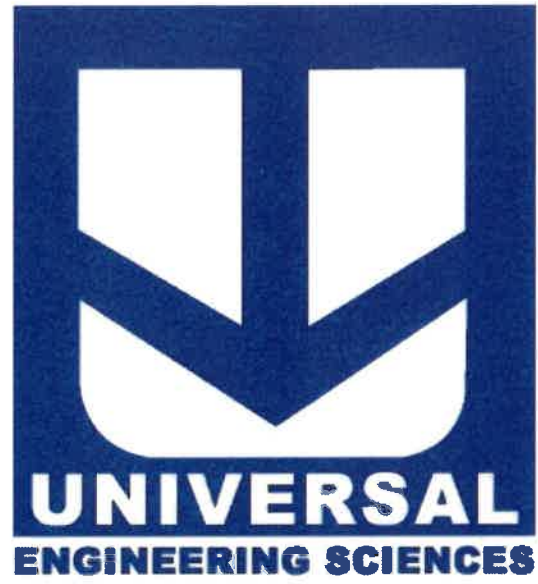
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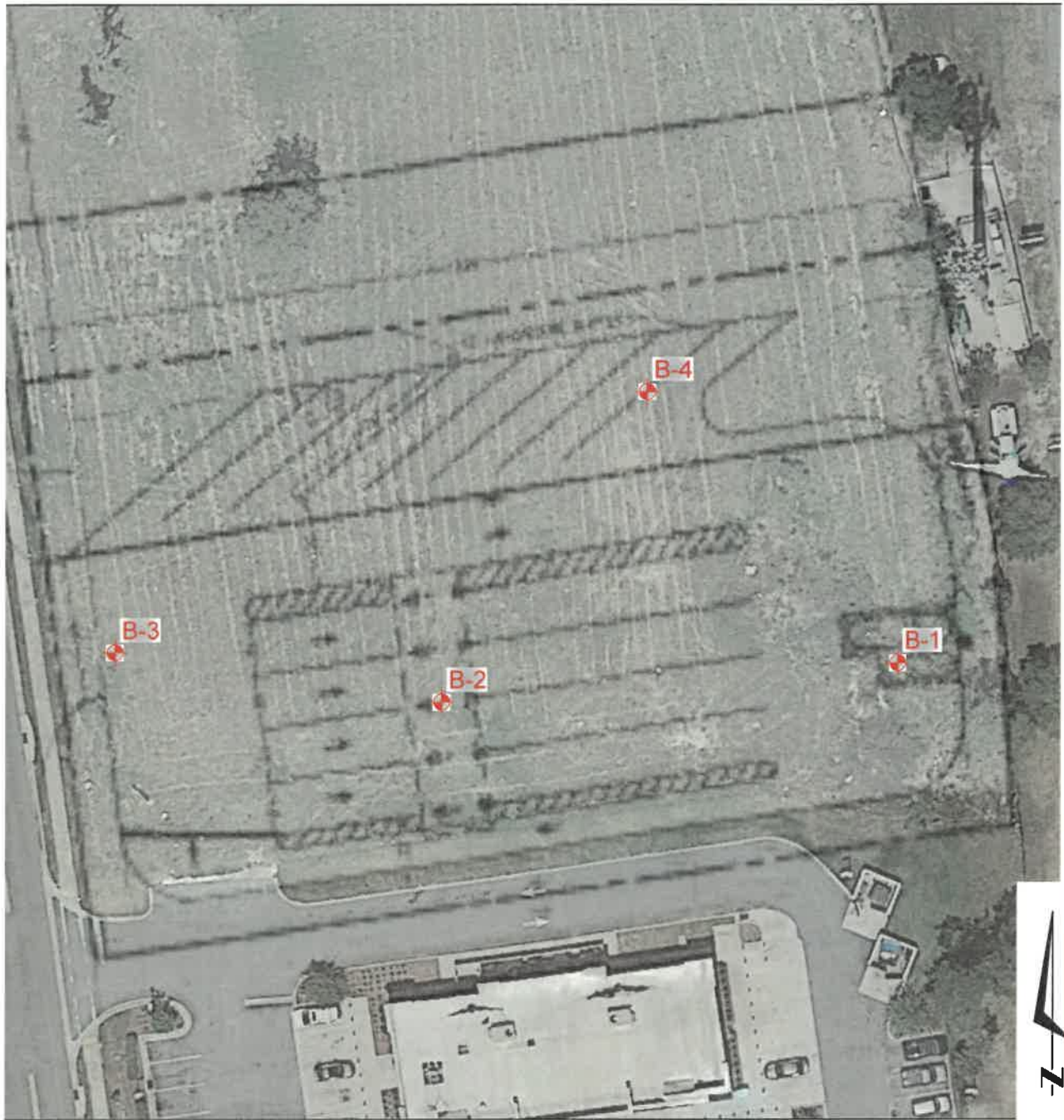
PROJECT NO: 0730.2100190

REVIEWED BY: VD

APPENDIX: A

APPENDIX B





LEGEND

 B-2 Approximate SPT boring location



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CIRCLE K - LAKE CITY
US HIGHWAY 90 & I-75
LAKE CITY, COLUMBIA COUNTY, FLORIDA

BORING LOCATION PLAN

CLIENT: CIRCLE K STORES, INC	DRAWN BY: SC	DATE: SEP. 8, 2021
SCALE:	PROJECT NO: 0730.2100190	REVIEWED BY: VD
		APPENDIX: B



UNIVERSAL ENGINEERING SCIENCES
 9802 Palm River Road
 Tampa, Florida 33619
 (813) 740-8506

BORING LOG

PROJECT NO.: 0730.2100190

APPENDIX:

PAGE: 1

PROJECT: Circle K - Lake City
 US Highway 90 & 1-75
 Lake City, Columbia County, Florida

BORING DESIGNATION: **B-1** SHEET: **1 of 1**
 SECTION: TOWNSHIP: RANGE:

ENGINEER: Veronica De Freitas, P.E.

ELEVATION: DATE STARTED: 9/30/2021

CLIENT: Circle K Store, Inc

WATER TABLE (ft): 3.5 DATE FINISHED: 9/30/2021

LOCATION: SEE BORING LOCATION PLAN

DATE OF READING: 9/30/2021 DRILLED BY: Universal Engineering

REMARKS:

EST. W.S.W.T. (ft): TYPE OF SAMPLING: SPT

DEPTH (ft)	SAMPLING	BLOWS PER FT	N (bpf)	SPT-N vs DEPTH (bpf)			GWT	SYMBOL	DESCRIPTION	-200 (%)	MC (%)	ATTERBERG LIMITS			ORG (%)
				0	25	50						LL	PL	PI	
0															
3-7-8-10		15	15					Light gray sand (SF)							
3-2-2-2		4	4					Brown clayey sand (SC)							
2-4-4-7		8	8												
5-8-11-10		19	19					Light brown clayey sand (SC)							
11-12-15-13		27	27												
6-7-8		15	15					Brown clayey sand (SC)							
6-8-13		21	21					Light brown clayey sand (SC)							
6-10-22		32	32					Boring terminated at 25 ft.							

ALT UES BORING LOG 0730.2100190 - CIRCLE K - LAKE CITY.GPJ UES_NEW.GDT 10/8/21



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BORING LOG

PROJECT NO.: 0730.2100190

APPENDIX:

PAGE: 2

PROJECT: Circle K - Lake City
 US Highway 90 & 1-75
 Lake City, Columbia County, Florida

BORING DESIGNATION: **B-2** SHEET: **1 of 1**
 SECTION: TOWNSHIP: RANGE:

ENGINEER: Veronica De Freitas, P.E.

ELEVATION: DATE STARTED: 9/30/2021

CLIENT: Circle K Store, Inc

WATER TABLE (ft): 3.0 DATE FINISHED: 9/30/2021

LOCATION: SEE BORING LOCATION PLAN

DATE OF READING: 9/30/2021 DRILLED BY: Universal Engineering

REMARKS:

EST. W.S.W.T. (ft): TYPE OF SAMPLING: SPT

DEPTH (ft)	SAMPLER	BLOWS PER FT	N (bpf)	SPT-N vs DEPTH (bpf)	GWT	SYMBOL	DESCRIPTION	-200 (%)	MC (%)	ATTERBERG LIMITS			ORG (%)
										LL	PL	PI	
0							Gray sand (SP)						
3-3-4-5			7				Brown clayey sand (SC)						
4-2-2-2			4										
5													
1-1-2-4			3										
3-11-20-24			31										
10													
16-20-20-17			40										
							Brown clay with sand (CL-CH)						
15			5										
2-2-3													
20			4				Boring terminated at 20 ft.						
2-2-2													

ALT UES BORING LOG - 0730.2100190 - CIRCLE K - LAKE CITY.GPJ UES_NEW.GDT 10/8/21



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BORING LOG

PROJECT NO.: 0730.2100190

APPENDIX:

PAGE: 3

PROJECT: Circle K - Lake City
 US Highway 90 & 1-75
 Lake City, Columbia County, Florida

BORING DESIGNATION: **B-3**
 SECTION: TOWNSHIP:

SHEET: **1 of 1**
 RANGE:

ENGINEER: Veronica De Freitas, P.E.

ELEVATION:

DATE STARTED: 9/30/2021

CLIENT: Circle K Store, Inc

WATER TABLE (ft): 3.0

DATE FINISHED: 9/30/2021

LOCATION: SEE BORING LOCATION PLAN

DATE OF READING: 9/30/2021

DRILLED BY: Universal Engineering

REMARKS:

EST. W.S.W.T. (ft):

TYPE OF SAMPLING: SPT

DEPTH (ft)	SAMPLE	BLOWS PER 6"	N (bpf)	SPT-N vs DEPTH (bpf)			GWT	SYMBOL	DESCRIPTION	-200 (%)	MC (%)	ATTERBERG LIMITS			ORG (%)
				0	25	50						LL	PL	PI	
0															
4-6-6-8		12						Dark brown sand with clay (SP-SC)							
4-7-7-11		14						Brown sand (SP)							
2-6-11-10		17						Brownish gray sandy clay (CL)							
2-3-3-4		6						Brown clayey sand (SC)							
5-6-12-11		18						Boring terminated at 10 ft.							

ALT UES BORING LOG 0730.2100190 - CIRCLE K - LAKE CITY.GPJ UES_NEW.GDT 10/8/21



UNIVERSAL ENGINEERING SCIENCES
 9802 Palm River Road
 Tampa, Florida 33619
 (813) 740-8506

BORING LOG

PROJECT NO.: 0730.2100190

APPENDIX:

PAGE: 4

PROJECT: Circle K - Lake City
 US Highway 90 & 1-75
 Lake City, Columbia County, Florida

BORING DESIGNATION: **B-4**
 SECTION: TOWNSHIP:

SHEET: **1 of 1**
 RANGE:

ENGINEER: Veronica De Freitas, P.E.

ELEVATION:

DATE STARTED: 9/30/2021

CLIENT: Circle K Store, Inc

WATER TABLE (ft): 3.0

DATE FINISHED: 9/30/2021

LOCATION: SEE BORING LOCATION PLAN

DATE OF READING: 9/30/2021

DRILLED BY: Universal Engineering

REMARKS:

EST. W.S.W.T. (ft):






TYPE OF SAMPLING: SPT

DEPTH (ft)	SAMPLING	BLOWS PER FT	N (bpf)	SPT-N vs DEPTH (bpf)			GWT	SYMBOL	DESCRIPTION	-200 (%)	MC (%)	ATTERBERG LIMITS			ORG (%)
				0	25	50						LL	PL	PI	
0															
4-7-8-6		15						Light gray sand (SP)							
3-5-6-11		11						Brown clayey sand (SC)							
2-3-12-11		15						Brownish orange sand with clay (SP-SC)							
4-6-10-10		16													
8-11-15-9		26						Boring terminated at 10 ft.							

ALT UES BORING LOG 0730.2100190 - CIRCLE K - LAKE CITY.GPJ UES_NEW.GDT 10/8/21



SYMBOLS AND ABBREVIATIONS

<u>SYMBOL</u>	<u>DESCRIPTION</u>
N-Value	No. of Blows of a 140-lb. Weight Falling 30 Inches Required to Drive a Standard Spoon 1 Foot
WOR	Weight of Drill Rods
WOH	Weight of Drill Rods and Hammer
	Sample from Auger Cuttings
	Standard Penetration Test Sample
	Thin-wall Shelby Tube Sample (Undisturbed Sampler Used)
RQD	Rock Quality Designation
	Stabilized Groundwater Level
	Seasonal High Groundwater Level (also referred to as the W.S.W.T.)
NE	Not Encountered
GNE	Groundwater Not Encountered
BT	Boring Terminated
-200 (%)	Fines Content or % Passing No. 200 Sieve
MC (%)	Moisture Content
LL	Liquid Limit (Atterberg Limits Test)
PI	Plasticity Index (Atterberg Limits Test)
NP	Non-Plastic (Atterberg Limits Test)
K	Coefficient of Permeability
Org. Cont.	Organic Content
G.S. Elevation	Ground Surface Elevation

UNIFIED SOIL CLASSIFICATION SYSTEM

MAJOR DIVISIONS		GROUP SYMBOLS	TYPICAL NAMES
COARSE GRAINED SOILS More than 50% retained on the No. 200 sieve*	GRAVELS 50% or more of coarse fraction retained on No. 4 sieve	CLEAN GRAVELS	GW Well-graded gravels and gravel-sand mixtures, little or no fines
			GP Poorly graded gravels and gravel-sand mixtures, little or no fines
		GRAVELS WITH FINES	GM Silty gravels and gravel-sand-silt mixtures
			GC Clayey gravels and gravel-sand-clay mixtures
	SANDS More than 50% of coarse fraction passes No. 4 sieve	CLEAN SANDS 5% or less passing No. 200 sieve	SW** Well-graded sands and gravelly sands, little or no fines
			SP** Poorly graded sands and gravelly sands, little or no fines
SANDS with 12% or more passing No. 200 sieve		SM** Silty sands, sand-silt mixtures	
		SC** Clayey sands, sand-clay mixtures	
FINE-GRAINED SOILS 50% or more passes the No. 200 sieve*	SILTS AND CLAYS Liquid limit 50% or less	ML Inorganic silts, very fine sands, rock flour, silty or clayey fine sands	
		CL Inorganic clays of low to medium plasticity, gravelly clays, sandy clays, lean clays	
		OL Organic silts and organic silty clays of low plasticity	
	SILTS AND CLAYS Liquid limit greater than 50%	MH Inorganic silts, micaceous or diamicaceous fine sands or silts, elastic silts	
		CH Inorganic clays or clays of high plasticity, fat clays	
		OH Organic clays of medium to high plasticity	
		PT Peat, muck and other highly organic soils	

*Based on the material passing the 3-inch (75 mm) sieve
 ** Use dual symbol (such as SP-SM and SP-SC) for soils with more than 5% but less than 12% passing the No. 200 sieve

RELATIVE DENSITY
(Sands and Gravels)

Very loose – Less than 4 Blow/Foot
 Loose – 4 to 10 Blows/Foot
 Medium Dense – 11 to 30 Blows/Foot
 Dense – 31 to 50 Blows/Foot
 Very Dense – More than 50 Blows/Foot

CONSISTENCY
(Silts and Clays)

Very Soft – Less than 2 Blows/Foot
 Soft – 2 to 4 Blows/Foot
 Firm – 5 to 8 Blows/Foot
 Stiff – 9 to 15 Blows/Foot
 Very Stiff – 16 to 30 Blows/Foot
 Hard – More than 30 Blows/Foot

RELATIVE HARDNESS
(Limestone)

Soft – 100 Blows for more than 2 Inches
 Hard – 100 Blows for less than 2 Inches

MODIFIERS

These modifiers Provide Our Estimate of the Amount of Minor Constituents (Silt or Clay Size Particles) in the Soil Sample

Trace – 5% or less
 With Silt or With Clay – 6% to 11%
 Silty or Clayey – 12% to 30%
 Very Silty or Very Clayey – 31% to 50%

These Modifiers Provide Our Estimate of the Amount of Organic Components in the Soil Sample

Trace – Less than 3%
 Few – 3% to 4%
 Some – 5% to 8%
 Many – Greater than 8%

These Modifiers Provide Our Estimate of the Amount of Other Components (Shell, Gravel, Etc.) in the Soil Sample

Trace – 5% or less
 Few – 6% to 12%
 Some – 13% to 30%
 Many – 31% to 50%

APPENDIX C



Important Information about This

Geotechnical-Engineering Report

Subsurface problems are a principal cause of construction delays, cost overruns, claims, and disputes.

While you cannot eliminate all such risks, you can manage them. The following information is provided to help.

Geotechnical Services Are Performed for Specific Purposes, Persons, and Projects

Geotechnical engineers structure their services to meet the specific needs of their clients. A geotechnical-engineering study conducted for a civil engineer may not fulfill the needs of a constructor — a construction contractor — or even another civil engineer. Because each geotechnical-engineering study is unique, each geotechnical-engineering report is unique, prepared *solely* for the client. No one except you should rely on this geotechnical-engineering report without first conferring with the geotechnical engineer who prepared it. *And no one — not even you — should apply this report for any purpose or project except the one originally contemplated.*

Read the Full Report

Serious problems have occurred because those relying on a geotechnical-engineering report did not read it all. Do not rely on an executive summary. Do not read selected elements only.

Geotechnical Engineers Base Each Report on a Unique Set of Project-Specific Factors

Geotechnical engineers consider many unique, project-specific factors when establishing the scope of a study. Typical factors include: the client's goals, objectives, and risk-management preferences; the general nature of the structure involved, its size, and configuration; the location of the structure on the site; and other planned or existing site improvements, such as access roads, parking lots, and underground utilities. Unless the geotechnical engineer who conducted the study specifically indicates otherwise, do not rely on a geotechnical-engineering report that was:

- not prepared for you;
- not prepared for your project;
- not prepared for the specific site explored; or
- completed before important project changes were made.

Typical changes that can erode the reliability of an existing geotechnical-engineering report include those that affect:

- the function of the proposed structure, as when it's changed from a parking garage to an office building, or from a light-industrial plant to a refrigerated warehouse;
- the elevation, configuration, location, orientation, or weight of the proposed structure;
- the composition of the design team; or
- project ownership.

As a general rule, *always* inform your geotechnical engineer of project changes—even minor ones—and request an

assessment of their impact. *Geotechnical engineers cannot accept responsibility or liability for problems that occur because their reports do not consider developments of which they were not informed.*

Subsurface Conditions Can Change

A geotechnical-engineering report is based on conditions that existed at the time the geotechnical engineer performed the study. *Do not rely on a geotechnical-engineering report whose adequacy may have been affected by:* the passage of time; man-made events, such as construction on or adjacent to the site; or natural events, such as floods, droughts, earthquakes, or groundwater fluctuations. *Contact the geotechnical engineer before applying this report to determine if it is still reliable.* A minor amount of additional testing or analysis could prevent major problems.

Most Geotechnical Findings Are Professional Opinions

Site exploration identifies subsurface conditions only at those points where subsurface tests are conducted or samples are taken. Geotechnical engineers review field and laboratory data and then apply their professional judgment to render an opinion about subsurface conditions throughout the site. Actual subsurface conditions may differ — sometimes significantly — from those indicated in your report. Retaining the geotechnical engineer who developed your report to provide geotechnical-construction observation is the most effective method of managing the risks associated with unanticipated conditions.

A Report's Recommendations Are Not Final

Do not overrely on the confirmation-dependent recommendations included in your report. *Confirmation-dependent recommendations are not final*, because geotechnical engineers develop them principally from judgment and opinion. Geotechnical engineers can finalize their recommendations *only* by observing actual subsurface conditions revealed during construction. *The geotechnical engineer who developed your report cannot assume responsibility or liability for the report's confirmation-dependent recommendations if that engineer does not perform the geotechnical-construction observation required to confirm the recommendations' applicability.*

A Geotechnical-Engineering Report Is Subject to Misinterpretation

Other design-team members' misinterpretation of geotechnical-engineering reports has resulted in costly

problems. Confront that risk by having your geotechnical engineer confer with appropriate members of the design team after submitting the report. Also retain your geotechnical engineer to review pertinent elements of the design team's plans and specifications. Constructors can also misinterpret a geotechnical-engineering report. Confront that risk by having your geotechnical engineer participate in prebid and preconstruction conferences, and by providing geotechnical construction observation.

Do Not Redraw the Engineer's Logs

Geotechnical engineers prepare final boring and testing logs based upon their interpretation of field logs and laboratory data. To prevent errors or omissions, the logs included in a geotechnical-engineering report should *never* be redrawn for inclusion in architectural or other design drawings. Only photographic or electronic reproduction is acceptable, *but recognize that separating logs from the report can elevate risk.*

Give Constructors a Complete Report and Guidance

Some owners and design professionals mistakenly believe they can make constructors liable for unanticipated subsurface conditions by limiting what they provide for bid preparation. To help prevent costly problems, give constructors the complete geotechnical-engineering report, *but* preface it with a clearly written letter of transmittal. In that letter, advise constructors that the report was not prepared for purposes of bid development and that the report's accuracy is limited; encourage them to confer with the geotechnical engineer who prepared the report (a modest fee may be required) and/or to conduct additional study to obtain the specific types of information they need or prefer. A prebid conference can also be valuable. *Be sure constructors have sufficient time to perform additional study. Only then might you be in a position to give constructors the best information available to you, while requiring them to at least share some of the financial responsibilities stemming from unanticipated conditions.*

Read Responsibility Provisions Closely

Some clients, design professionals, and constructors fail to recognize that geotechnical engineering is far less exact than other engineering disciplines. This lack of understanding has created unrealistic expectations that have led to disappointments, claims, and disputes. To help reduce the risk of such outcomes, geotechnical engineers commonly include a variety of explanatory provisions in their reports. Sometimes labeled "limitations," many of these provisions indicate where geotechnical engineers' responsibilities begin and end, to help

others recognize their own responsibilities and risks. *Read these provisions closely.* Ask questions. Your geotechnical engineer should respond fully and frankly.

Environmental Concerns Are Not Covered

The equipment, techniques, and personnel used to perform an *environmental* study differ significantly from those used to perform a *geotechnical* study. For that reason, a geotechnical-engineering report does not usually relate any environmental findings, conclusions, or recommendations; e.g., about the likelihood of encountering underground storage tanks or regulated contaminants. *Unanticipated environmental problems have led to numerous project failures.* If you have not yet obtained your own environmental information, ask your geotechnical consultant for risk-management guidance. *Do not rely on an environmental report prepared for someone else.*

Obtain Professional Assistance To Deal with Mold

Diverse strategies can be applied during building design, construction, operation, and maintenance to prevent significant amounts of mold from growing on indoor surfaces. To be effective, all such strategies should be devised for the *express purpose* of mold prevention, integrated into a comprehensive plan, and executed with diligent oversight by a professional mold-prevention consultant. Because just a small amount of water or moisture can lead to the development of severe mold infestations, many mold-prevention strategies focus on keeping building surfaces dry. While groundwater, water infiltration, and similar issues may have been addressed as part of the geotechnical-engineering study whose findings are conveyed in this report, the geotechnical engineer in charge of this project is not a mold prevention consultant; *none of the services performed in connection with the geotechnical engineer's study were designed or conducted for the purpose of mold prevention. Proper implementation of the recommendations conveyed in this report will not of itself be sufficient to prevent mold from growing in or on the structure involved.*

Rely, on Your GBC-Member Geotechnical Engineer for Additional Assistance

Membership in the Geotechnical Business Council of the Geoprofessional Business Association exposes geotechnical engineers to a wide array of risk-confrontation techniques that can be of genuine benefit for everyone involved with a construction project. Confer with your GBC-Member geotechnical engineer for more information.



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CONSTRAINTS & RESTRICTIONS

The intent of this document is to bring to your attention the potential concerns and the basic limitations of a typical geotechnical report.

WARRANTY

Universal Engineering Sciences has prepared this report for our client for his exclusive use, in accordance with generally accepted soil and foundation engineering practices, and makes no other warranty either expressed or implied as to the professional advice provided in the report.

UNANTICIPATED SOIL CONDITIONS

The analysis and recommendations submitted in this report are based upon the data obtained from soil borings performed at the locations indicated on the Boring Location Plan. This report does not reflect any variations which may occur between these borings.

The nature and extent of variations between borings may not become known until excavation begins. If variations appear, we may have to re-evaluate our recommendations after performing on-site observations and noting the characteristics of any variations.

CHANGED CONDITIONS

We recommend that the specifications for the project require that the contractor immediately notify Universal Engineering Sciences, as well as the owner, when subsurface conditions are encountered that are different from those present in this report.

No claim by the contractor for any conditions differing from those anticipated in the plans, specifications, and those found in this report, should be allowed unless the contractor notifies the owner and Universal Engineering Sciences of such changed conditions. Further, we recommend that all foundation work and site improvements be observed by a representative of Universal Engineering Sciences to monitor field conditions and changes, to verify design assumptions and to evaluate and recommend any appropriate modifications to this report.

MISINTERPRETATION OF SOIL ENGINEERING REPORT

Universal Engineering Sciences is responsible for the conclusions and opinions contained within this report based upon the data relating only to the specific project and location discussed herein. If the conclusions or recommendations based upon the data presented are made by others, those conclusions or recommendations are not the responsibility of Universal Engineering Sciences.

CHANGED STRUCTURE OR LOCATION

This report was prepared in order to aid in the evaluation of this project and to assist the architect or engineer in the design of this project. If any changes in the design or location of the structure as outlined in this report are planned, or if any structures are included or added that are not discussed in the report, the conclusions and recommendations contained in this report shall not be considered valid unless the changes are reviewed and the conclusions modified or approved by Universal Engineering Sciences.

USE OF REPORT BY BIDDERS

Bidders who are examining the report prior to submission of a bid are cautioned that this report was prepared as an aid to the designers of the project and it may affect actual construction operations.

Bidders are urged to make their own soil borings, test pits, test caissons or other investigations to determine those conditions that may affect construction operations. Universal Engineering Sciences cannot be responsible for any interpretations made from this report or the attached boring logs with regard to their adequacy in reflecting subsurface conditions which will affect construction operations.

STRATA CHANGES

Strata changes are indicated by a definite line on the boring logs which accompany this report. However, the actual change in the ground may be more gradual. Where changes occur between soil samples, the location of the change must necessarily be estimated using all available information and may not be shown at the exact depth.

OBSERVATIONS DURING DRILLING

Attempts are made to detect and/or identify occurrences during drilling and sampling, such as: water level, boulders, zones of lost circulation, relative ease or resistance to drilling progress, unusual sample recovery, variation of driving resistance, obstructions, etc.; however, lack of mention does not preclude their presence.

WATER LEVELS

Water level readings have been made in the drill holes during drilling and they indicate normally occurring conditions. Water levels may not have been stabilized at the last reading. This data has been reviewed and interpretations made in this report. However, it must be noted that fluctuations in the level of the groundwater may occur due to variations in rainfall, temperature, tides, and other factors not evident at the time measurements were made and reported. Since the probability of such variations is anticipated, design drawings and specifications should accommodate such possibilities and construction planning should be based upon such assumptions of variations.

LOCATION OF BURIED OBJECTS

All users of this report are cautioned that there was no requirement for Universal Engineering Sciences to attempt to locate any man-made buried objects during the course of this exploration and that no attempt was made by Universal Engineering Sciences to locate any such buried objects. Universal Engineering Sciences cannot be responsible for any buried man-made objects which are subsequently encountered during construction that are not discussed within the text of this report.

TIME

This report reflects the soil conditions at the time of exploration. If the report is not used in a reasonable amount of time, significant changes to the site may occur and additional reviews may be required.





December 18, 2023

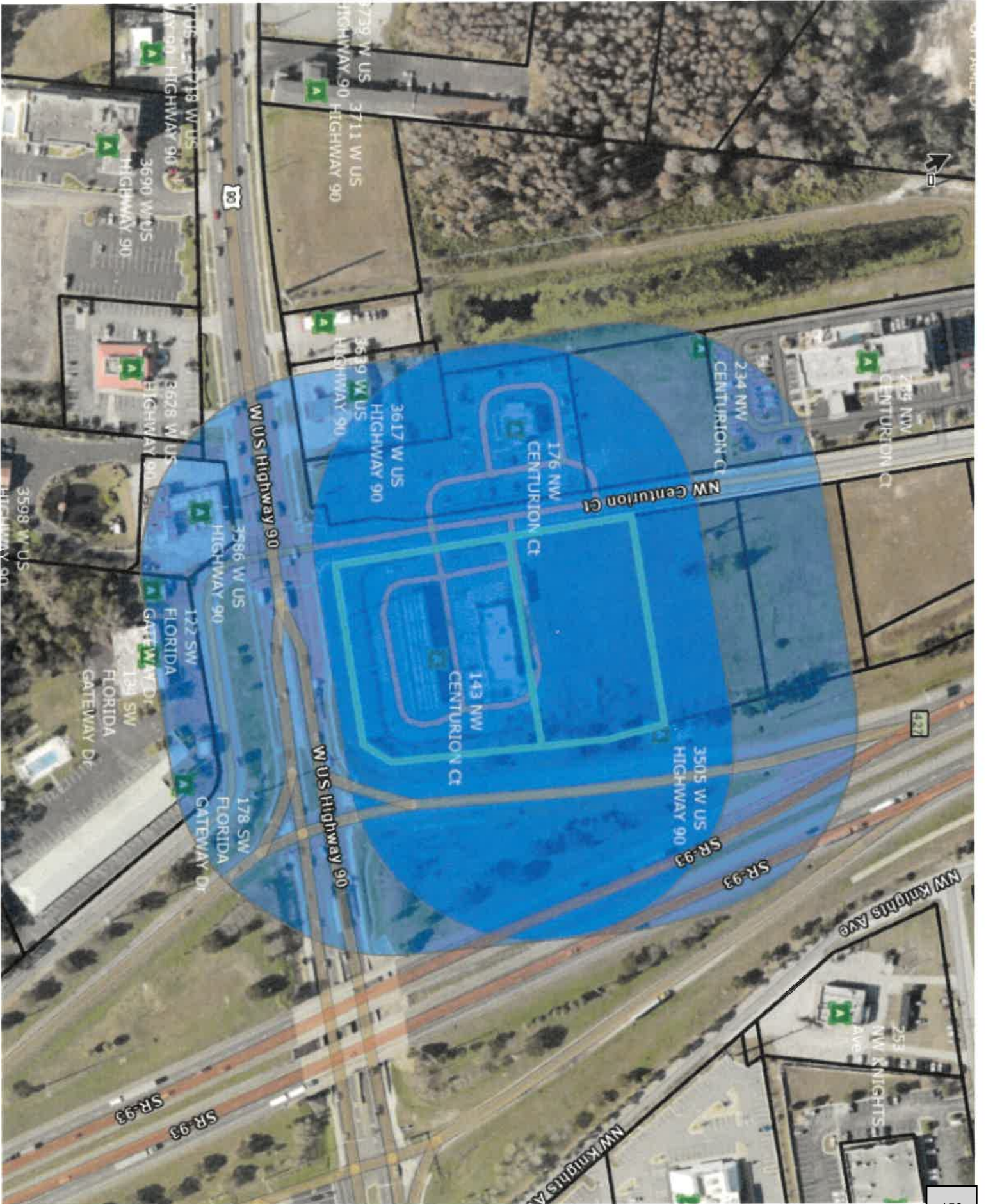
To Whom it May Concern

On January 10, 2024, the Planning and Zoning Board will be having a Special Called Planning and Zoning hearing at 5:30pm, at 205 N. Marion Ave., Lake City, FL 32025. At this hearing the Planning and Zoning Board will be hearing petition SPR22-15, a site plan review for the expansion of the existing Circle K for the property located on parcels 35-3S-16-02524-001 and 35-3S-16-02524-102. The hearing on January 17, 2024 will only be held as a continuation if needed due to the length of the hearing on January 10, 2024.

If you have any questions or concerns please call 386-752-2031 ext. 820 or email growthmanagement@lcfla.com.

Robert Angelo

Planning and Zoning Tech
City of Lake City



Address within 300' of parcel#02524-102

Full Address	City	PARCEL ID	ZIP CODE
3505 W US HIGHWAY 90	LAKE CITY		32055 UTILITY ADDRESS
176 NW CENTURION Ct	LAKE CITY	02524-009	32055 657 W MINTON DRIVE, TEMPE, AZ, 85282
143 NW CENTURION Ct	LAKE CITY	02524-001	32055 PROPOSED SITE
234 NW CENTURION Ct	LAKE CITY	02524-008	32055 UTILITY ADDRESS
284 NW CENTURION CT	LAKE CITY	02524-007	32055 3696 W US HWY 90, LAKE CITY, FL 32055
3617 W US HIGHWAY 90	LAKE CITY	02534-000	32055 6867 SOUTH POINT DR N STE 101, JACKSONVILLE, FL 32216
228 NW CENTURION CT	LAKE CITY	02524-008	32055 105 TALLAPOOSA ST, MONTGOMERY, AL, 36104
211 NW CENTURION CT	LAKE CITY	02524-103	32055 105 TALLAPOOSA ST, MONTGOMERY, AL, 36104
3628 W US HIGHWAY 90	LAKE CITY	02537-000	32055 8111 SMTHS MILL ROAD, NEW ALBANY, OH, 43054
3586 W US HIGHWAY 90	LAKE CITY	02541-000	32055 3586 W US HIGHWAY 90, LAKE CITY, FL 32055
122 SW FLORIDA GATEWAY DR	LAKE CITY	02543-000	32055 178 SW FLORIDA GATEWAY DR, LAKE CITY, FL 32024
134 SW FLORIDA GATEWAY DR	LAKE CITY	02543-000	32055 178 SW FLORIDA GATEWAY DR, LAKE CITY, FL 32024
178 SW FLORIDA GATEWAY DR	LAKE CITY	02543-000	32055 178 SW FLORIDA GATEWAY DR, LAKE CITY, FL 32024

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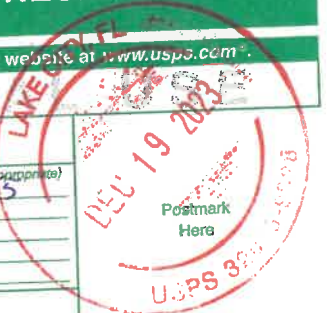
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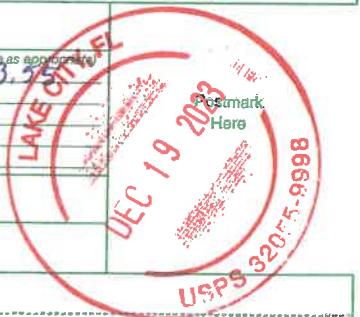
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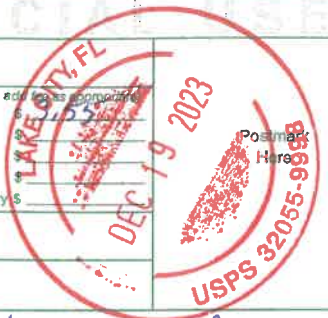
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<input type="checkbox"/> Certified Mail Restricted Delivery	\$
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<input type="checkbox"/> Adult Signature Restricted Delivery	\$
Postage	.63
Total Postage and Fees	8.53

Sent To: First Coast Energy, LLP
 Street and Apt. No., or PO Box No. 1867 South Point Dr. N Ste 101
 City, State, ZIP+4® Jacksonville, FL 32216

PS Form 3800, January 2023 PSN 7530-02-000-9047 See Reverse for Instructions



9589 0710 5270 1427 8364 39

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<input type="checkbox"/> Certified Mail Restricted Delivery	\$
<input type="checkbox"/> Adult Signature Required	\$
<input type="checkbox"/> Adult Signature Restricted Delivery	\$
Postage	.63
Total Postage and Fees	8.53

Sent To: Florida Gateway Hotels LLC
 Street and Apt. No., or PO Box No. 3696 W US Highway 90
 City, State, ZIP+4® Lake City, FL 32055

PS Form 3800, January 2023 PSN 7530-02-000-9047 See Reverse for Instructions



NOTICE OF PUBLIC HEARING CITY OF LAKE CITY SPECIAL CALLED PLANNING AND ZONING BOARD

THIS SERVES AS PUBLIC NOTICE the Planning and Zoning Board will hold a hearing on Wednesday, January 10, 2024 at 5:30 PM and Wednesday, January 17, 2024. The hearing on January 17, 2024 will only be held as a continuation, if needed due to length on the hearing on January 10, 2024.

Agenda items-

1. SPR22-15, a petition submitted by Jarod Stubbs, as agent for Daniel Hotte of GWC Development Partners, LLC, owner, for a site plan review application for a property located in the commercial highway interchange zoning district. Parcels 35-3S-16-02524-001 and 35-3S-16-02524-102.

Hearing Location: City Council Chambers located on the 2nd Floor of City Hall at 205 North Marion Avenue, Lake City, FL 32055.

Members of the public may also view the meeting on our YouTube channel at:

<https://www.youtube.com/c/CityofLakeCity>

Pursuant to 286.0105, Florida Statutes, the City hereby advises the public if a person decides to appeal any decision made by the City with respect to any matter considered at its meetings or hearings, he or she will need a record of the proceedings, and that, for such purpose, he or she may need to ensure that a verbatim record of the proceedings is made, which record includes the testimony and evidence upon which the appeal is to be based.

SPECIAL REQUIREMENTS: Pursuant to 286.26, Florida Statutes, persons needing special accommodations to participate in this hearing should contact the City Manager's Office at (386) 719-5768.

Robert Angelo, Planning and Zoning Tech.

**NOTICE OF PUBLIC HEARING
CITY OF LAKE CITY**

SPECIAL CALLED PLANNING AND ZONING BOARD

THIS SERVES AS PUBLIC NOTICE the Planning and Zoning Board will hold a hearing on Wednesday, January 10, 2024 at 5:30 PM and Wednesday, January 17, 2024. The hearing on January 17, 2024 will only be held as a continuation, if needed due to length on the hearing on January 10, 2024.

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Robert Angelo, Planning and Zoning Tech.

Angelo, Robert

From: LCR-Classifieds <classifieds@lakecityreporter.com>
Sent: Tuesday, December 19, 2023 4:52 PM
To: Angelo, Robert
Subject: RE: 76227 RE: Special Called Planning and Zoning Hearing for 01-10-2024 and 01-17-2024

Confirmed

Thank you

Kym Harrison • 386-754-0401

Lake City Reporter • Currents Magazine • HomeSeller Magazine
1086 SW Main Blvd. Suite 103, Lake City, FL 32025
Serving Columbia, Suwannee, Hamilton & Lafayette

From: Angelo, Robert <AngeloR@lcfla.com>
Sent: Tuesday, December 19, 2023 4:50 PM
To: LCR-Classifieds <classifieds@lakecityreporter.com>
Subject: RE: 76227 RE: Special Called Planning and Zoning Hearing for 01-10-2024 and 01-17-2024

Looks good.

Thank You
Robert Angelo
City of Lake City
Growth Management
growthmanagement@lcfla.com
386-719-5820



PLEASE NOTE: Florida has a very broad public records law. Most written communications to or from City officials regarding City business are public records available to the public and media upon request. Your email communications may be subject to public disclosure.

From: LCR-Classifieds <classifieds@lakecityreporter.com>
Sent: Tuesday, December 19, 2023 4:37 PM
To: Angelo, Robert <AngeloR@lcfla.com>
Subject: 76227 RE: Special Called Planning and Zoning Hearing for 01-10-2024 and 01-17-2024

Hi Robert! Proof attached for approval
3x14.75 \$235.13

Thank you

Kym Harrison • 386-754-0401

Lake City Reporter • Currents Magazine • HomeSeller Magazine
1086 SW Main Blvd. Suite 103, Lake City, FL 32025
Serving Columbia, Suwannee, Hamilton & Lafayette

From: Angelo, Robert <AngeloR@lcfla.com>
Sent: Tuesday, December 19, 2023 3:52 PM
To: LCR-Classifieds <classifieds@lakecityreporter.com>
Subject: Special Called Planning and Zoning Hearing for 01-10-2024 and 01-17-2024

Kym

Please publish this ad in the body of the paper as a display ad in the **December 27, 2023** paper.

Thank You
Robert Angelo
City of Lake City
Growth Management
growthmanagement@lcfla.com
386-719-5820



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**CITY OF LAKE CITY
NOTICE
LAND USE ACTION**

A PUBLIC HEARING IS SCHEDULED TO CONSIDER A REQUEST FOR:

SPR22-15, a petition by Jarod Stubbs, as agent, for Daniel Hotte of GWC Development Partners, LLC, owner to request a Site Plan Review approval to be granted as provided for in Section 4.15 of the Land Development Regulations, to get approval on site plan for Expansion of Circle K for a property located in the Commercial Highway Interchange zoning district, in accordance with the submittal of the petition dated April 1, 2022, to be located on parcels 35-3S-16-02524-001 and 35-3S-16-02524-102.

WHEN: January 10, 2023
5:30 p.m.
January 17, 2023 (only if needed due to length of hearing on January 10, 2023)
5:30pm

WHERE: City Council Meeting Room, Second Floor, City Hall, located at 205 North Marion Avenue, Lake City, Florida.
Members of the public may also view the meeting on our YouTube channel at:
<https://www.youtube.com/c/CityofLakeCity>.

Copies of the site plan review application are available for public inspection by contacting the Office of Growth Management at growthmanagement@lcfla.com or by calling 386.719.5820.

At the aforementioned public hearing, all interested parties may be heard with respect to the Site Plan Review.

**FOR MORE INFORMATION CONTACT
ROBERT ANGELO
PLANNING & ZONING TECHNICIAN
AT 386.719.5820**

CIRCLE K

PUBLIC NOTICE

[Faded public notice document]

PUBLIC NOTICE

Public Notice
[Faded text from a document posted on the sign]

NOTICE OF PUBLIC HEARING CITY OF LAKE CITY SPECIAL CALLED PLANNING AND ZONING BOARD

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Agenda items-

1. SPR22-15, a petition submitted by Jarod Stubbs, as agent for Daniel Hotte of GWC Development Partners, LLC, owner, for a site plan review application for a property located in the commercial highway interchange zoning district. Parcels 35-3S-16-02524-001 and 35-3S-16-02524-102.

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Robert Angelo, Planning and Zoning Tech.

**NOTICE OF PUBLIC HEARING
CITY OF LAKE CITY
SPECIAL CALLED PLANNING AND ZONING BOARD**

THIS SPECIAL PUBLIC HEARING will be held by the Planning and Zoning Board on Wednesday, January 10, 2024 at 5:30 P.M. in the City Council Chambers located on the 2nd Floor of City Hall at 205 North Marion Avenue, Lake City, FL 32055. The hearing on January 17, 2024 will be a continuation, if needed due to length on the hearing on January 10, 2024.

Agenda Item

1. SPR2024-001, a petition submitted by Jarod Stubbs, as agent for Daniel Hotte of GWC Development Partners, LLC, owner, for a site plan review application for a property located in the commercial highway interchange zoning district. Parcels 35-3S-16-02524-001 and 35-3S-16-02524-102.

Hearing Location: City Council Chambers located on the 2nd Floor of City Hall at 205 North Marion Avenue, Lake City, FL 32055.

Members of the public may also view the meeting on our YouTube channel at: <https://www.youtube.com/c/CityofLakeCity>

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SPECIAL REQUIREMENTS: Pursuant to 286.26, Florida Statutes, persons needing special accommodations to participate in this hearing should contact the City Manager's Office at (386) 719-5768.

Robert Angelo, Planning and Zoning Tech.

R. Rainey
mrainey@burr.com
Direct Dial: (813) 367-5761

One Tampa City Center, Suite 3200
201 North Franklin Street
Tampa, FL 33602

Office (813) 221-2626
Fax (813) 221-7335

December 18, 2023

BURR.COM

Lake City, Florida Planning and Zoning Board
205 N. Marion Avenue
Lake City, FL 32055

Re: Application No. SPR 22-15
Owner - GWC Development Partners, LLC ("GWC")
Tenant/Operator - Circle K Stores, Inc. ("Circle K")
Location - Lot 2 Gateway Crossings (143 NW Centurion Court)

To the Lake City, Florida Planning and Zoning Board:

Our law firm represents Circle K Stores Inc., and for purposes of these proceedings will also be representing the interests of GWC Development Partners, LLC. This memorandum is provided in advance of a specially set hearing scheduled before the Lake City Planning and Zoning Board ("P&Z") for Wednesday, January 10, 2024, and Wednesday, January 17, 2024. The general overview of what this proceeding concerns is as follows:

- 1) Circle K seeks to expand its existing location at U.S. Hwy. 90 and I-75, to provide high speed diesel ("HSD") fueling in addition to its existing gasoline fueling and convenience store operations. The HSD facility will have three bays and three diesel pumps. It is not, nor is it intended to be, a "truck stop," which is defined in the Lake City LDR (Sec 2.1, Pg 2-23) as follows:

"A truck stop is an establishment where the principal use is primarily the refueling and servicing of trucks and tractor trailer rigs. Such establishments may have restaurants or snack bars and sleeping accommodations for the drivers of such over-the-road equipment and may provide facilities for the repair and maintenance of such equipment."

- There will be no restaurant, snack bar, or sleeping accommodations.
- There will be no facilities for repair and servicing of trucks.
- There will be no overnight parking.

- There will only be six parking bays to allow the driver to enter the store for a short period of time for the purchase of goods or the use of restrooms.
- 2) Circle K and GWC have complied with all regulatory and legal requirements throughout the process of applying for and obtaining the original approval of the expanded Circle K development. This includes the necessary approvals of Lake City, Columbia County and the FDOT.
- The site plan for the expanded HSD facility was unanimously approved by the Lake City Planning and Zoning Board on **July 6, 2022**.
 - An appeal of the plan was not filed within 30 days following the decision, as required by the LDR.
 - The Construction Permit for the expanded HSD facility was issued **February 28, 2023**.

The currently scheduled hearing comes before P&Z as a *de novo* rehearing of a Site Plan Application that was approved in favor of GWC (“Owner”) and Circle K. (“Tenant” or “Operator”) on July 6, 2022, as highlighted above. P&Z approved the Site Plan Application in accordance with the City of Lake City’s Land Development Regulations. Copies of the Site Plan Application and P&Z’s approval letter are attached as **Exhibits “A” and “B,”** respectively. Also attached as **Exhibit “C”** is a copy of the June 22, 2022, Review Report submitted by City staff in advance of the hearing, which raised no issue in opposition to the application.

No appeal was taken from the decision by P&Z, which must be filed with City Council within thirty (30) days following the decision at issue. See, LDR 11.1.1 and 11.1.2. Instead, Gateway Hotels, LLC, the “Appellant” in these proceedings, and a neighboring hotel operator in the Gateway Crossings development, waited more than eight (8) months to challenge the P&Z decision by filing a Notice of Appeal with the Lake City, Florida Board of Adjustment challenging the issuance of Circle K’s New Commercial Construction Permit, #000046609 (the “Construction Permit”) dated February 28, 2023. For procedural reasons discussed in more detail below, the matter is now being presented for a second time to P&Z.

The project in question involves Circle K’s expansion of an existing convenience store operation to include a high speed diesel fueling facility (“HSD Facility”) consisting of three fueling bays and three diesel pumps, immediately behind and to the north of the existing convenience store. The plan for the project is included with the attached Site Plan Application, **Exhibit “A,”** and is further detailed in the construction Plans attached as **Exhibit “A-1.”** A copy of the Construction Permit issued by the City of Lake City is attached as **Exhibit “D.”** Appellant has purported to appeal the issuance of this Construction Permit based on the notion that Circle K’s expanded convenience store operation amounts to a “Truck Stop” under the applicable LDRs, and requires a “special use” exception under those regulations. The Appellant has repeatedly referred to the Construction Permit (issued February 28, 2023) as a “Development Order,” apparently in an effort to extend its appellate rights and gloss over the fact that the appeal was lodged eight months after the substantive decision by P&Z. . It is the position of both GWC and Circle K that the issuance

of the Construction Permit was a ministerial act, meaning a non-discretionary, non-judgmental decision by the City staff, prompted by the approval of Circle K's site plan. The site plan was submitted and approved in accordance with the City's applicable Land Development Regulations. A copy of the Minutes of the July 6, 2022, meeting are attached hereto as **Exhibit "E."**

After issuance of the Construction Permit, and in reliance on the City's land development procedures, Circle K finalized a long term Ground Lease with GWC for the new expansion, and entered into contract with U.S. General Construction, Inc. for the construction of its expansion project. Simply put, the Appellant missed its opportunity to challenge this project as the time for appeal had long since expired, and then attempted to use the issuance of the Construction Permit as way to revive its procedural posture. A plain reading of the 27 page Notice of Appeal reflects a detailed challenge to the Board's July 2022 decision, issues that should have been presented at the public hearing, or at a minimum through a timely appeal of that decision. The permit itself involved no discretionary decision making by the City or any of its boards, but again, was a ministerial act, i.e. a non-discretionary action, that required no decision. In fact, the Appellant acknowledges the site plan approval in its Notice of Appeal, but then argues that there was a modification that was not properly noticed for public hearing. This is a complete "red herring" as there was no material change from the original site plan to the modified version (see attached **Exhibit "F"**) which is included in the Notice of Appeal

In preparing for the upcoming hearing, it appears that the Appellant has been looking at the wrong issue and applicable regulations as they relate to the July 2022 hearing. Appellant has argued since they launched their untimely appeal that a "special use exception" was required for Circle K's HSD Facility, and that the requirements for such an exception were not met. But the reliance on LDR 12.4 is misplaced. This case involves site plan review and approval. Therefore, as a site plan review, as opposed to a "special use exception," no notice was required to be published. The applicable LDR specifically states:

13.11.3 Action on Site and Development Plan. The Land Development Regulation Administrator shall forward the application for site and development plan approval along with any comments or criticisms to the Planning and Zoning Board for consideration. The Planning and Zoning Board shall handle such matters in a public session as part of a previously prepared agenda, however, no public notice and hearing is required. All matters relating to Planning and Zoning Board consideration of site and development plans shall be a public record and approval, approval with conditions, or denial shall require formal action of the Planning and Zoning Board. A petition for a zoning amendment and an application for site and development plan approval shall not be handled concurrently. Rather, an application for site and development plan approval shall be heard only after the applicant has secured the appropriate zoning on the subject parcel. Appeals from decisions of the Planning and Zoning Board shall be heard as set out in Article 12 of these land development regulations.

Appeals of such decisions are then governed by Article 12 of the LDRs, and the 30 day limitation period is applicable. Thus, we would again submit that this appeal is untimely as filed, and should certainly not be given an opportunity for reconsideration contrary to Lake City's regulations.

For these reasons alone, Circle K and GWC would submit that a rejection or dismissal of the appeal is the appropriate response in this instance. Furthermore, a plain reading of the Site Plan Application filed for GWC on April 1, 2022, reflects a “Proposed use of Property” as a “Circle K gas station and high speed diesel station.” There was no reference to a “truck stop” which the Appellant seems intent on arguing, and which is defined by the LDRs and referenced in their Notice of Appeal.

A truck stop is an establishment where the principal use is primarily the refueling and servicing of trucks and tractor trailer rigs. Such establishments may have restaurants or snack bars and sleeping accommodations for the drivers of such over-the-road equipment and may provide facilities for the repair and maintenance of such equipment.

There will be no “servicing of trucks” at this site, there will be no “restaurant or snack bar,” there will be no “sleeping accommodations,” and there will be no “repair and maintenance” of trucks. The crux of the issue is that this Appellant is dissatisfied with a substantive decision that the Planning and Zoning Board made in July of 2022. They aren’t taking issue with a construction permit, there is no flaw in that piece of paper, except that Appellant argues with the underlying decision that led to its issuance.

Accordingly, as the time for that appeal expired eight months before it was filed, Circle K and GWC would strongly urge dismissal or rejection of the Notice of Appeal and confirmation of the July 2022 site plan approval to be the appropriate remedy in this instance.

Notwithstanding the procedural objections made by GWC and Circle K, and assuming P&Z intends to rehear the application and reconsider the proposed site plan, there are several substantive points that should be highlighted in advance of that presentation. First, an updated Traffic Impact Analysis has been conducted to include current traffic counts and estimates as to the volume impact this expanded facility will have on local traffic. A copy of that analysis is included for your use and convenience with this memorandum as **Exhibit “G.”** As one can see, the impacts to traffic on U.S. Hwy 90 will be minimal and of no appreciable adverse impact. In fact, the expansion of Circle K’s facility would likely generate significantly less traffic volume going in and out of NW Centurion Court than the new restaurants (Sonic and Rib Crib), the self-storage facility (U-Haul) or a planned second hotel (on a parcel owned by Gateway Hotels, the “Appellant”). Copies of the concurrency analyses, including trip generation estimates, for Sonic, Rib Crib and U-Haul are attached as **Composite Exhibit “H.”**

Circle K had submitted a public records request to the City for copies of any Traffic Impact Analyses for other developments in the Gateway Crossings development. On October 18, 2023, the City produced three Site Plan Applications prepared by JB Pro for U-Haul, Rib Crib and Sonic, and nothing for the Appellant’s hotel. All of the applications included a brief Concurrency Impact Analysis, and the Transportation Mobility section of these analyses were all based on the City’s Level of Service Standards (“LOS”) for traffic impacts. No independent Traffic Impact Analyses were conducted for these other sites. This fact suggests that the other site plan approvals in Gateway Crossing were not put to the same rigorous review that has been required of Circle

K. Nevertheless, given the marginal increased traffic that is expected to be added due to the HSD expansion, Circle K and GWC would submit that they have carried the burden of establishing, now twice, that the estimated traffic impacts are insufficient grounds for the denial of this site plan application.

Circle K would also add that the FDOT approved this expansion project, and had expressed no concern in connection with the traffic or drainage impacts. Copies of the Driveway Connection Permit and the Drainage Connection Permit issued by FDOT on or about May 18, 2022, are attached as **Composite Exhibit "I."** Whether the FDOT has changed its position due to communications from the Appellant remains an open issue, but as of the date of this memorandum GWC and Circle K remain in open communication with FDOT concerning the extent and anticipated impact of the proposed HSD facility, and the two permits remain in place.

Gateway Hotels also argues that P&Z somehow failed to apply the design standards from Section 4.2.6 of the LDRs relating to Automotive Service and Self-Service Stations, but fails to state how those standards were violated. The Appellant's position appears to be based on the conclusion that the provision of diesel fuel that can be accessed by a semi-tractor trailer truck eliminates the location's standing as an Automotive Service Station. This convenient argument ignores the fact that the overwhelming majority of traffic at the combined location is anticipated to be automobiles and that the anticipated truck traffic that will be generated will not materially impact the traffic counts already measured at this intersection. The Appellant ignores the key wording in LDR Section 2.1, which defines Automotive Service Station as "**primarily**" for automobiles. That is exactly what this combined location will be after completion of the HSD Facility, which facility will be ancillary to the primary convenience store and gasoline fueling functions.

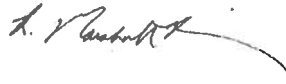
Gateway Hotels then attacks the proposed expansion as a "special use" (a Truck Stop) requiring a "special exception" under the LDRs for construction of the HSD Facility in the CHI District. This point is addressed in some detail above, but additional analysis is warranted here. First, the addition of the HSD Facility is an expansion of an existing operation, the primary function of which is a retail convenience store and automobile fueling location. The Appellant goes into a tortured analysis of what City staff must have been thinking (as there is no evidence in the record to support this conjecture) and then tries to break down what is meant by "servicing of trucks." As stated above, and as will be testified to at the hearing, there will be no servicing of trucks at this location, no restaurant, no showers, no overnight parking etc., all things one associates with a Truck Stop.

And finally, Gateway Hotels appeals to the emotions of this body and the general public, and assails the HSD Facility as incompatible with the character of the CHI district, a use that will somehow endanger the health and safety of persons within the area. Gateway Hotels claims that trucks entering NW Centurion Court will endanger pedestrians and other drivers, will block traffic on both sides of the road, and will create more "smoke, odor, noise ... fumes, gas, vibration, ... and emission of particulate matter," all in violation of the LDRs. . But the Appellant has ignored that Section 4.15 of the LDRs, relating to a Commercial Highway Interchange, specifically allows for service stations, truck rental, wholesale distribution activities, and light manufacturing, all of

which create truck traffic. As such, Circle K would respectfully submit that the objections here are not only untimely, but quite selective in their application.

For the foregoing reasons, Circle K Stores Inc. and GWC Development Partners, LLC would respectfully request that the Planning and Zoning Board approve SPR 22-15 based upon the competent substantial evidence provided to the Board, and find that the expanded use at this location is consistent with the CHI zoning for the subject area and that the traffic impacts to the surrounding property owners and general public are consistent with the level of service standards that have been adopted by the City of Lake City.

Sincerely,



R. Marshall Rainey, Esq.
Counsel for Circle K Stores Inc.

RR/pt

EXHIBIT

A

EXHIBIT A



CITY OF LAKE CITY

APPLICATION FOR
NEW DEVELOPMENT AND
SITE REVIEW COMMITTEE MEETING

FOR OFFICIAL USE ONLY

MEETING DATE: _____

MEETING TIME: _____

APPLICANT INFORMATION

Date: 09/01/2021

Name: Jarod C. Stubbs, P.E.
Address: 189 S Orange Ave., Ste 1000, Orlando, FL
Phone: (407) 409-7002
Email: jarod.stubbs@kimley-horn.com

Business Name:

Kimley-Horn and Associates

Business Address:

189 South Orange Ave., Suite 1000
Orlando, FL 32801

SUBJECT PROPERTY INFORMATION

Address: NE corner of US Hwy 90 and NW Centurion Ct (behind the Circle K)
Parcel ID#: 35-3s-16-02524-102; and 35-3s-16-02524-111
Existing Use: Vacant Commercial
Zoning District: CHI Commercial Highway Intensive

Property Owner:

GWC Development Partners, LLC

Owner Address:

2682 NW Noegel Rd
Lake City, FL 32055

DESCRIPTION OF REQUEST (may be attached, separately)

PLEASE PROVIDE AS MUCH DETAIL AS POSSIBLE SO THAT STAFF CAN BE PREPARED TO ADDRESS YOUR QUESTIONS.

Please include information regarding:

- Proposed use
- Proposed improvements to building and/or site

The proposed project is to be a high speed diesel expansion to the existing Circle K with related parking, underground fuel storage tanks, and other necessary improvements. The project is anticipated to take up space on both parcels listed in this application. Expected new impervious area for the project is +/- 49,850 square feet. The existing Circle K will also have improvements including a building expansion for additional restrooms and an adjustment to the parking spaces to allow space for said expansion. See attached site plan for more detail.

SUBMIT WITH THIS FORM

- Copy of survey or sketch of location/building
- Sketch of any proposed improvements
- Any other information that will help in review of the proposal

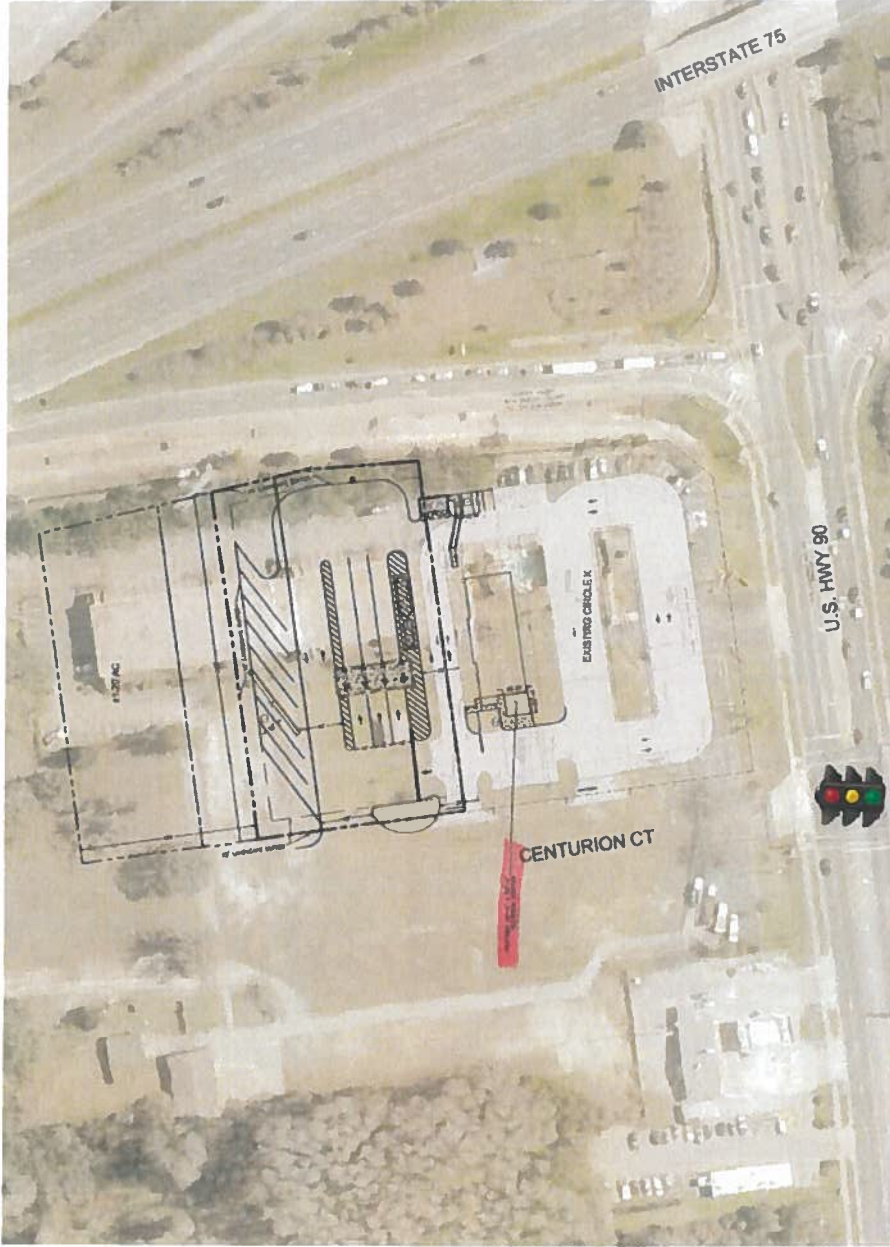
SUBMIT COMPLETED FORM AND DOCUMENTS TO:

Mail: Lake City Growth Management Department, 205 N Marion Ave, Lake City, FL 32055

Email: growthmanagement@lcfia.com

Fax: 386-758-5426

If you have any further questions, please contact Growth Management, 386-719-5750



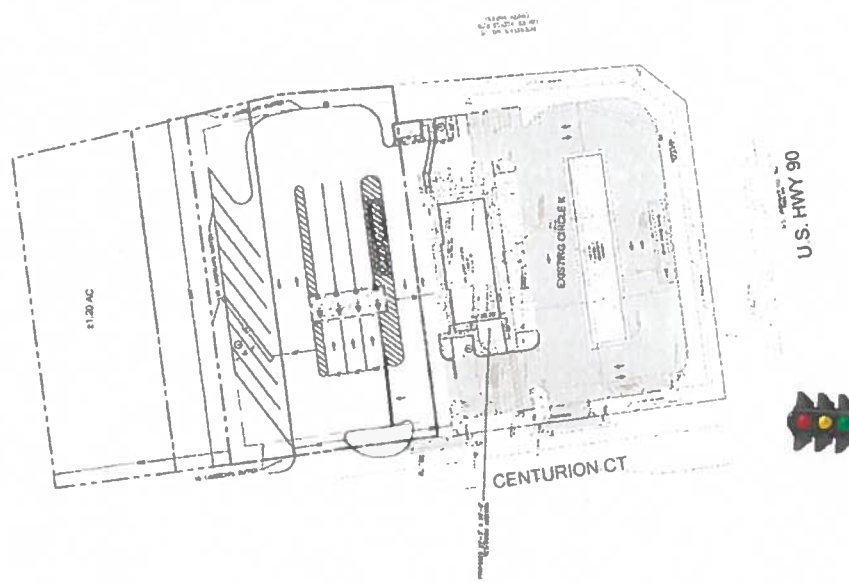
1.00 10' = 1" (VERTICAL)
 2.00 1" = 100' (HORIZONTAL)
 3.00 1" = 100' (HORIZONTAL)
 4.00 1" = 100' (HORIZONTAL)
 5.00 1" = 100' (HORIZONTAL)
 6.00 1" = 100' (HORIZONTAL)
 7.00 1" = 100' (HORIZONTAL)
 8.00 1" = 100' (HORIZONTAL)
 9.00 1" = 100' (HORIZONTAL)
 10.00 1" = 100' (HORIZONTAL)

CONCEPTUAL SKETCH
 10/10/2017 10:00 AM

CIRCLE K - U.S. 90 & I-75

5-D-07

INTERSTATE 75



ALL DIMENSIONS
 UNLESS OTHERWISE
 SPECIFIED SHALL BE
 IN FEET AND INCHES
 (FRACTIONS SHALL BE
 IN 16THS OF AN INCH)
 DIMENSIONS SHALL BE
 TO FACE UNLESS
 OTHERWISE SPECIFIED
 DIMENSIONS SHALL BE
 TO FACE UNLESS
 OTHERWISE SPECIFIED



CONCEPTUAL SKETCH

EXTENSION OF CENTURION HWY FROM I-75 TO I-85

CIRCLE K - U.S. 90 & I-75

3/1/07



GROWTH MANAGEMENT

205 North Marion Ave.
Lake City, FL 32055
Telephone: (386)719-5750
E-Mail:
growthmanagement@lcfla.com

FOR PLANNING USE ONLY

Application # SPR22-19
Application Fee: \$200.00
Receipt No. _____
Filing Date 4/1/22
Completeness Date _____

Site Plan Application

A. PROJECT INFORMATION

1. Project Name: CIRCLE K - US 90 & I-75
2. Address of Subject Property: 143 NW Centurion Ct., Lake City, FL 32055
3. Parcel ID Number(s): 35-3S-16-02524-001, 35-3S-16-02524-102, 35-3S-16-02524-111
4. Future Land Use Map Designation: Commercial
5. Zoning Designation: CHI - Commercial Highway Interchange
6. Acreage: ±3.46
7. Existing Use of Property: Existing Circle K gas station and convenience store
8. Proposed use of Property: Circle K gas station and high speed diesel station
9. Type of Development (Check All That Apply):
 - Increase of floor area to an existing structure: Total increase of square footage ±652 SF
 - New construction: Total square footage ±54,470 SF
 - Relocation of an existing structure: Total square footage _____

B. APPLICANT INFORMATION

1. Applicant Status Owner (title holder) **Agent**
2. Name of Applicant(s): Jarod Stubbs P.E. Title: Civil Engineer
 Company name (if applicable): Kimley-Horn
 Mailing Address: 189 S. Orange Ave. Suite 1000
 City: Orlando State: FL Zip: 32801
 Telephone: (407) 409-7002 Fax: () Email: jarod.stubbs@kimley-horn.com

PLEASE NOTE: Florida has a very broad public records law. Most written communications to or from government officials regarding government business is subject to public records requests. Your e-mail address and communications may be subject to public disclosure.

3. If the applicant is agent for the property owner*.
 Property Owner Name (title holder): Daniel Hotte of GWC Development Partners, LLC
 Mailing Address: 2682 W Noegel Rd
 City: Lake City State: FL Zip: 32055
 Telephone: (407) 580-5173 Fax: () Email: dberry@shafferconst.com

PLEASE NOTE: Florida has a very broad public records law. Most written communications to or from government officials regarding government business is subject to public records requests. Your e-mail address and communications may be subject to public disclosure.

***Must provide an executed Property Owner Affidavit Form authorizing the agent to act on behalf of the property owner.**

C. ADDITIONAL INFORMATION

1. Is there any additional contract for the sale of, or options to purchase, the subject property?
 If yes, list the names of all parties involved: _____
 If yes, is the contract/option contingent or absolute: Contingent Absolute
2. Has a previous application been made on all or part of the subject property? Yes No
 Future Land Use Map Amendment: Yes _____ No _____
 Future Land Use Map Amendment Application No. _____
 Site Specific Amendment to the Official Zoning Atlas (Rezoning): Yes _____ No _____
 Site Specific Amendment to the Official Zoning Atlas (Rezoning) Application No. _____
 Variance: Yes _____ No _____
 Variance Application No. _____
 Special Exception: Yes _____ No _____
 Special Exception Application No. _____

D. ATTACHMENT/SUBMITTAL REQUIREMENTS

1. Vicinity Map – Indicating general location of the site, abutting streets, existing utilities, complete legal description of the property in question, and adjacent land use.
2. Site Plan – Including, but not limited to the following:
 - a. Name, location, owner, and designer of the proposed development.
 - b. Present zoning for subject site.
 - c. Location of the site in relation to surrounding properties, including the means of ingress and egress to such properties and any screening or buffers on such properties.
 - d. Date, north arrow, and graphic scale not less than one inch equal to 50 feet.
 - e. Area and dimensions of site (Survey).
 - f. Location of all property lines, existing right-of-way approaches, sidewalks, curbs, and gutters.
 - g. Access to utilities and points of utility hook-up.
 - h. Location and dimensions of all existing and proposed parking areas and loading areas.
 - i. Location, size, and design of proposed landscaped areas (including existing trees and required landscaped buffer areas).
 - j. Location and size of any lakes, ponds, canals, or other waters and waterways.
 - k. Structures and major features fully dimensioned including setbacks, distances between structures, floor area, width of driveways, parking spaces, property or lot lines, and percent of property covered by structures.
 - l. Location of trash receptacles.
 - m. For multiple-family, hotel, motel, and mobile home park site plans:
 - i. Tabulation of gross acreage.
 - ii. Tabulation of density.
 - iii. Number of dwelling units proposed.
 - iv. Location and percent of total open space and recreation areas.
 - v. Percent of lot covered by buildings.

City of Lake City – Growth Management Department
 205 North Marion Ave, Lake City, FL 32055 ♦ (386) 719-5750

- vi. Floor area of dwelling units.
 - vii. Number of proposed parking spaces.
 - viii. Street layout.
 - ix. Layout of mobile home stands (for mobile home parks only).
8. Stormwater Management Plan—Including the following:
 - a. Existing contours at one foot intervals based on U.S. Coast and Geodetic Datum.
 - b. Proposed finished elevation of each building site and first floor level.
 - c. Existing and proposed stormwater management facilities with size and grades.
 - d. Proposed orderly disposal of surface water runoff.
 - e. Centerline elevations along adjacent streets.
 - f. Water management district surface water management permit.
 9. Fire Department Access and Water Supply Plan: The Fire Department Access and Water Supply Plan must demonstrate compliance with Chapter 18 of the Florida Fire Prevention Code, be located on a separate signed and sealed plan sheet, and must be prepared by a professional fire engineer licensed in the State of Florida. The Fire Department Access and Water Supply Plan must contain fire flow calculations in accordance with the Guide for Determination of Required Fire Flow, latest edition, as published by the Insurance Service Office (“ISO”) and/or Chapter 18, Section 18.4 of the Florida Fire Prevention Code, whichever is greater.
 10. Concurrency Impact Analysis: Concurrency Impact Analysis of impacts to public facilities. For commercial and industrial developments, an analysis of the impacts to Transportation, Potable Water, Sanitary Sewer, and Solid Waste impacts are required.
 11. Comprehensive Plan Consistency Analysis: An analysis of the application's consistency with the Comprehensive Plan (analysis must identify specific Goals, Objectives, and Policies of the Comprehensive Plan and detail how the application complies with said Goals, Objectives, and Policies).
 12. Legal Description with Tax Parcel Number (In Word Format).
 13. Proof of Ownership (i.e. deed).
 14. Agent Authorization Form (signed and notarized).
 15. Proof of Payment of Taxes (can be obtained online via the Columbia County Tax Collector's Office).
 16. Fee. The application fee for a Site and Development Plan Application is \$200.00. No application shall be accepted or processed until the required application fee has been paid.

City of Lake City – Growth Management Department
 205 North Marion Ave, Lake City, FL 32055 ♦ (386) 719-5750

NOTICE TO APPLICANT

All eleven (11) attachments are required for a complete application. Once an application is submitted and paid for, a completeness review will be done to ensure all the requirements for a complete application have been met. If there are any deficiencies, the applicant will be notified in writing. If an application is deemed to be incomplete, it may cause a delay in the scheduling of the application before the Planning & Zoning Board.

A total of ten (10) copies of proposed site plan application and all support materials must be submitted along with a PDF copy on a CD. See City of Lake City submittal guidelines for additional submittal requirements.

THE APPLICANT ACKNOWLEDGES THAT THE APPLICANT OR AGENT MUST BE PRESENT AT THE PUBLIC HEARING BEFORE THE PLANNING AND ZONING BOARD, AS ADOPTED IN THE BOARD RULES AND PROCEDURES, OTHERWISE THE REQUEST MAY BE CONTINUED TO A FUTURE HEARING DATE.

I hereby certify that all of the above statements and statements contained in any documents or plans submitted herewith are true and accurate to the best of my knowledge and belief.

Jared Stubbs

Applicant/Agent Name (Type or Print)

[Signature]

Applicant/Agent Signature

6/8/2022

Date

Applicant/Agent Name (Type or Print)

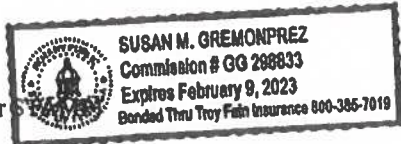
Applicant/Agent Signature

Date

STATE OF FLORIDA
COUNTY OF Orange

The foregoing instrument was acknowledged before me this 8th day of June 2022 by (name of person acknowledging) Jared Stubbs.

(NOTARY SEAL OF



Susan M Gremontrez
Signature of Notary

Printed Name of Notary

Personally Known OR Produced Identification _____
Type of Identification Produced

City of Lake City - Growth Management Department
205 North Marion Ave, Lake City, FL 32055 ♦ (386) 719-5750

Kimley»Horn
 9 2222 KIMLEY-HORN AND ASSOCIATES, INC.
 128 S. GLENDALE AVENUE, SUITE 1000, ORLANDO, FL 32809
 407.321.1111
 WWW.KIMLEY-HORN.COM

PROJECT NO. 18000000
 DATE 05/04/2012
 SCALE AS SHOWN
 DESIGNED BY L.F.
 DRAWN BY J.S.
 CHECKED BY J.S.
 CITY OF LAKE CITY

SITE PLAN

**CIRCLE K - US HWY
 90 & I-75 FUEL
 EXPANSION**

FLORIDA
 CITY OF LAKE CITY

SHEET NUMBER
C4.1

INTERSTATE 75 OFF-RAMP

CENTURION COURT

EXISTING 37'x185' FUEL CANOPY

NOTES

1. ALL DIMENSIONS ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
2. ALL DIMENSIONS ARE TO THE CENTERLINE OF PAVING UNLESS OTHERWISE NOTED.
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19. ALL DIMENSIONS ARE TO THE CENTERLINE OF PAVING UNLESS OTHERWISE NOTED.
20. ALL DIMENSIONS ARE TO THE CENTERLINE OF PAVING UNLESS OTHERWISE NOTED.

LEGEND

PROPOSED ASPHALT PAVEMENT (SEE DETAIL SHEET C-1)

PROPOSED GRAVEL PAVEMENT (SEE DETAIL SHEET C-1)

PROPOSED CONCRETE PAVEMENT (SEE DETAIL SHEET C-1)

PROPOSED ASPHALT DRIVEWAY (SEE DETAIL SHEET C-1)

PROPOSED ASPHALT DRIVEWAY (SEE DETAIL SHEET C-1)

PROPOSED ASPHALT DRIVEWAY (SEE DETAIL SHEET C-1)

GRAPHIC SCALE IN FEET

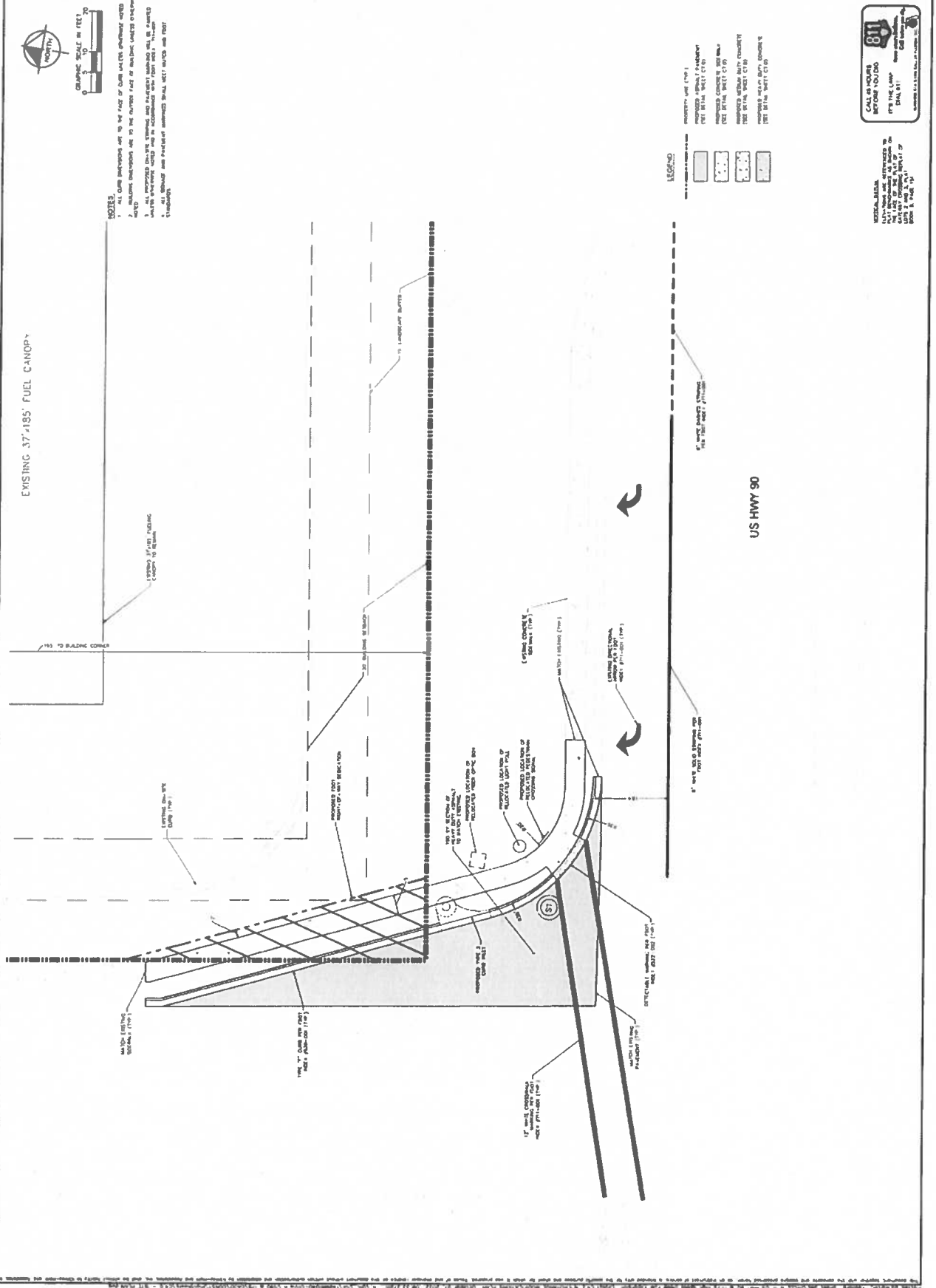
0 10 20 40

NORTH

811
 CALL 811 HOURS BEFORE YOU DIG
 IT'S THE LAW
 24 HOURS
 800.480.4804
 WWW.811FLORIDA.COM

811
 CALL 811 HOURS BEFORE YOU DIG
 IT'S THE LAW
 24 HOURS
 800.480.4804
 WWW.811FLORIDA.COM

<p>DATE: 05/29/2023</p> <p>PROJECT: MONITORING STATION</p>		<p>DATE: 05/29/2023</p> <p>PROJECT: MONITORING STATION</p>		<p>DATE: 05/29/2023</p> <p>PROJECT: MONITORING STATION</p>		<p>DATE: 05/29/2023</p> <p>PROJECT: MONITORING STATION</p>		<p>DATE: 05/29/2023</p> <p>PROJECT: MONITORING STATION</p>	
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Kimley-Horn

901222 AVENUE, SUITE 1000, GAITHERSBURG, MD 20878-4101
 PHONE: 410-286-1511

DATE: 05/29/2023
 PROJECT: MONITORING STATION

INTERSECTION PLAN

MODIFICATION PLAN

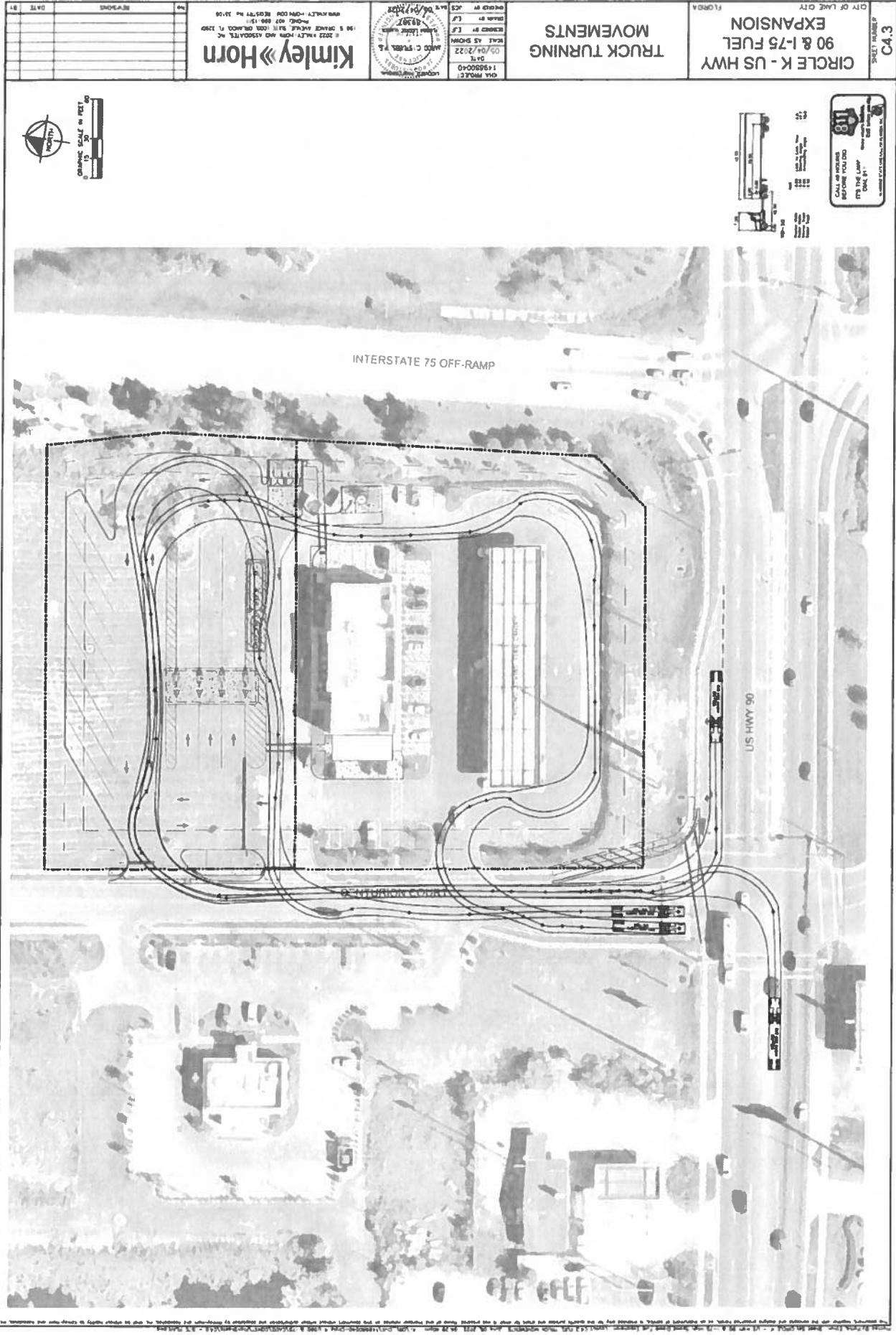
CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION

FLORIDA

SHEET NUMBER: C4.2



FOR INFORMATION ON THE LOCATION OF UNDERGROUND UTILITIES, CALL 811 AT LEAST 48 HOURS BEFORE YOU DIG. CALL 811 AT LEAST 48 HOURS BEFORE YOU DIG. CALL 811 AT LEAST 48 HOURS BEFORE YOU DIG.



SHEET NUMBER
C4.3

CIRCLE K - US HWY
90 & I-75 FUEL
EXPANSION
FLORIDA
CITY OF LAKE CITY

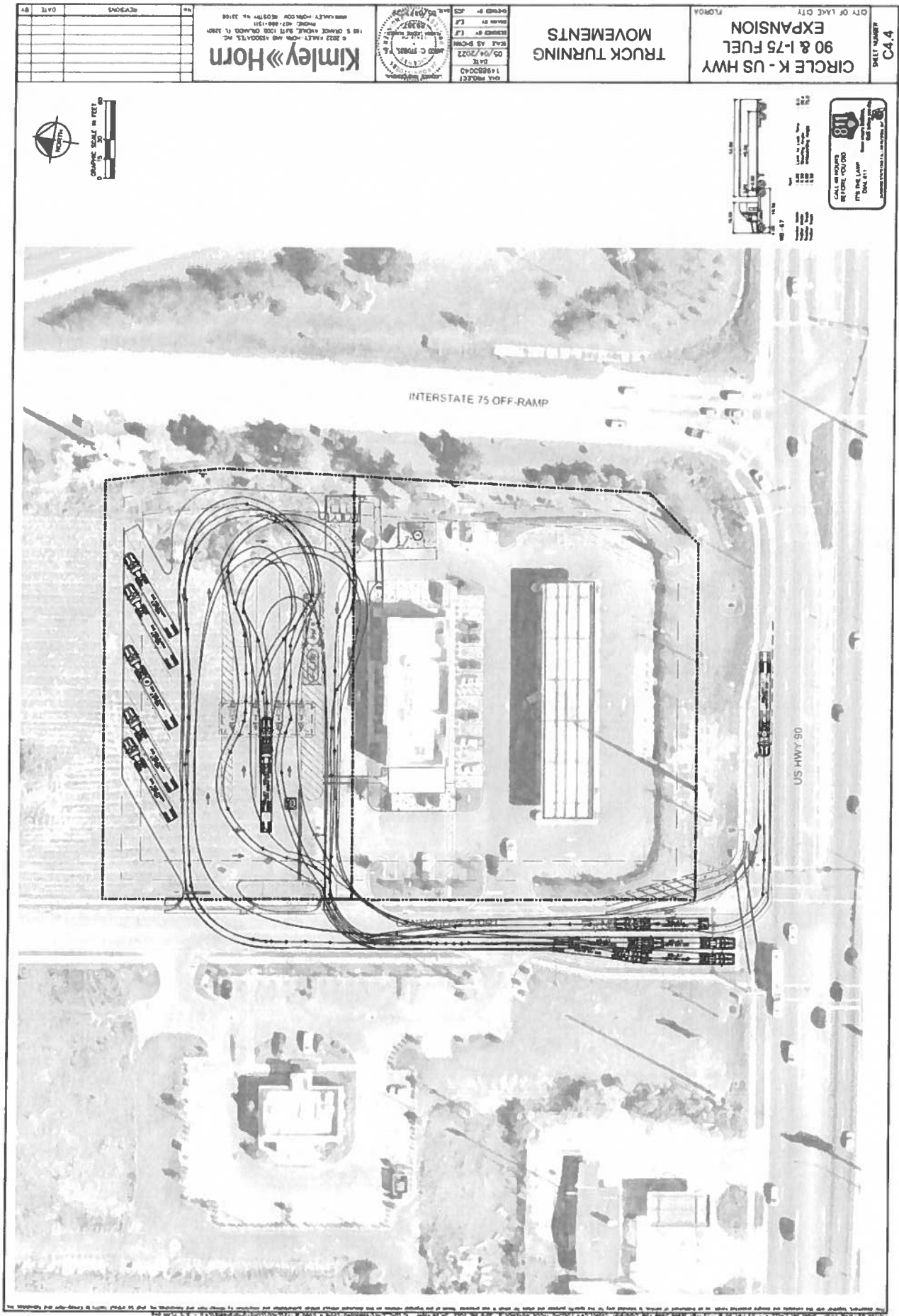
TRUCK TURNING
MOVEMENTS

DATE: 09/04/2022
SCALE: AS SHOWN
PROJECT: 114390040
DESIGNED BY: JCS
DRAWN BY: L.F.
CHECKED BY: L.F.
APPROVED BY: J.A.
LAKELAND DISTRICT ENGINEER

Kimley»Horn
1400 S. DIXIE AVENUE, SUITE 1000, ORLANDO, FL 32809
PHONE: 407.888.1511
WWW.KIMLEY-HORN.COM, INFO@KH.COM

NO.	REVISIONS	DATE





NOTICE OF APPEAL 012

COLUMBIA COUNTY PROPERTY VALUATION SUMMARY PAGE 1 of 1

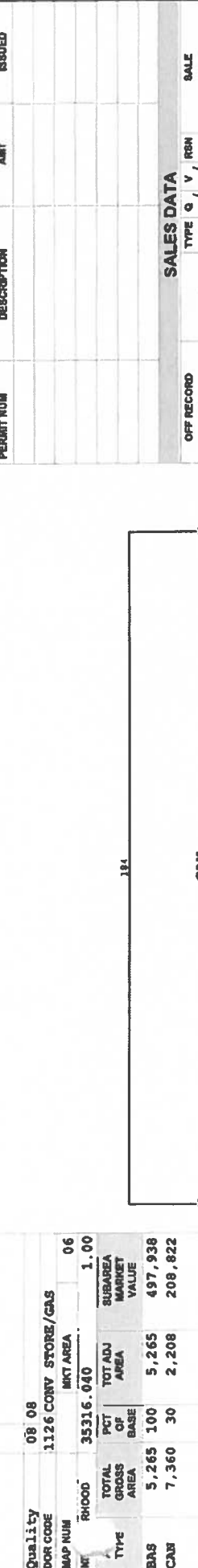
STANDARD

VALUATION BY Tax Dist

BUILDING MARKET VALUE	706,759
TOTAL LAND VALUE - MARKET	103,200
TOTAL MARKET VALUE	1,239,211
SOH/AGL Deduction	2,049,170
ASSESSED VALUE	2,049,170
TOTAL EXEMPTION VALUE	0
BASE TAXABLE VALUE	2,049,170
TOTAL JUST VALUE	2,049,170
INCOME VALUE	2,049,170
PREVIOUS YEAR MKT VALUE	2,056,437

MARKET ADJUSTMENTS

TYPE	MKT	EFF. AREA	TOT ADJPTS	EFF. RATE	REFL. COST NEW	AVG.	2017	2017	EYS	ECON. F	FRNT	NORM	% COND
4300	04	7,473	163.1610	97.50	728,618	2017	2017	0	0	0	0	3.00	97.00
1 RIBED CONVE - 0% - 0													



BUILDING CHARACTERISTICS

CD CONSTRUCTION

EXterior Wall	17	SHEX	STOC	90	
Roof Structure	09	RIDGE	FRME	100	
Roof Cover	04	BUILT-UP	100		
Interior Wall	08	DECORATIVE	100		
Interior Floor	15	HARDTILE	100		
Ceiling	01	FIN.	SUSD	100	
Air Condition	06	ENG	CENTRL	100	
Heating Type	09	ENG	F	AIR	100
Fixtures	12	100			
Frames	03	MASONRY	100		
Story Height	12	100			
RMS	0	100			
Stories	1	1	100		
Units	0	100			
Condition Adj	04	04	100		

Quality 08 08

DOR CODE 1126 CONV STORE/GAS

MAP NUM 06

MT RHOOD 35316.040

PCT OF AREA 1.00

TOT ADJ MARKET VALUE 706,759

GROSS AREA 5,265 100 5,265 497,938

NET AREA 7,360 30 2,208 209,822

TOTALS	12,625	7,473	706,759												
EXTRA FEATURES															
L	CBSE	DESCRIPTION	BLD CAP	L	W										
1	0260	PAVEMENT - A	0	0	0	36,400.00	UT	1.60					LGL DATE	LAND DATE	AG DATE
2	0166	CONC. PAVMT	0	0	0	11,300.00	UT	2.25	100	2017	2017	3	100	58,240	
3	0253	LIGHTING	0	0	0	10,000.00	UT	1,500.00	100	2017	2017	3	100	25,425	
4	0164	CONC BIN	0	0	0	308.00	UT	11.00	100	2017	2017	3	100	15,000	
99		FENCE/WOOD	0	0	0	74.00	UT	15.50	100	2017	2017	3	100	3,388	
						74.00		UT		15.50				1,147	

143 NW CENTURION CT, LAKE CITY

BLD DATE	2F DATE	INC DATE	GRD	YEAR	YEAR	YEAR	%	NOTES
			COND	ON	ACTUAL	COND	COND	
			1.60	100	2017	2017	3	100
			2.25	100	2017	2017	3	100
			10.00	100	2017	2017	3	100
			308.00	100	2017	2017	3	100
			74.00	100	2017	2017	3	100

TOTAL OBX/F

L	USE	LAND USE	DESCRIPTION	CAP	R	LOC	ZONE	TOT	LMD UTS	DEPTH	FRONT	DEPTH	TOT	COND	%	TOT	ADJ	UNIT	PRICE	LAND	VALUE
1	1410	CONV	STORE	0	R	MESC		1.00	0.00	86,206.00	0.00	0.00	1.00	1.00	1.00	1.25	11.50	14.38	1,239,211		
TOTALS																					
																103,200		14.38	1,239,211		

OTHER ADJUSTMENTS AND NOTES

YEAR DENSITY DECL FRZ YR CONSVY

11/01/2018 BY MESC Total Acres: 1.98 Total Land Value: 1,239,211 Market: 0 Agricultural: 0

Common: 1,239,211 PRINTED 02/10/2022

99

EXHIBIT

A-1

This document, together with the concepts and designs presented herein, is an instrument of service, to be used only for the specific purpose and shall not be used for any other purpose without the written authorization and approval of KIMLEY-HORN AND ASSOCIATES, INC. shall be without liability to KIMLEY-HORN AND ASSOCIATES, INC.

LEGAL DESCRIPTION

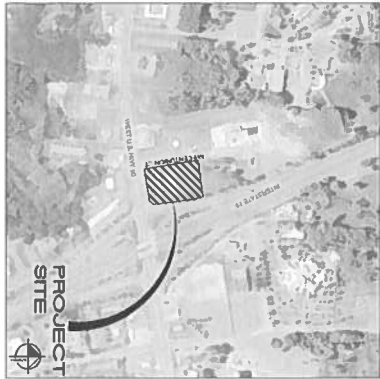
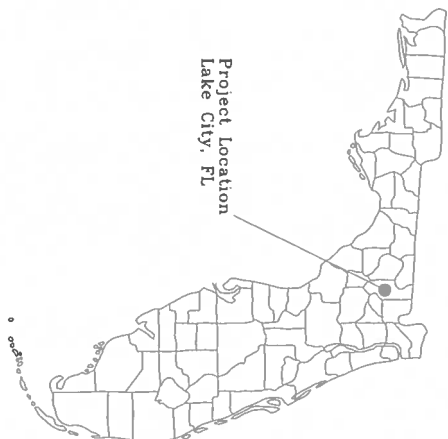
UNIT 1, PART OF THE PLAT AND PART OF LOT 11, OF "CIRCLE K CENTER (CONTRACT NO. 2007) 1/4 ACRES, ACCORDING TO PART 1, "CIRCLE K CENTER (CONTRACT NO. 2007) 1/4 ACRES, RECORDED IN PUBLIC BOOK 5, PAGE 214, PUBLIC RECORDS OF DADE COUNTY, FLORIDA.

LEGAL DESCRIPTION

CONSTRUCTION PLANS FOR CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION

143 NW CENTURION COURT
LAKE CITY, FLORIDA 32055

MAY 4, 2022
PARCEL IDS: 35-3S-16-02524-001,
35-3S-16-02524-102 AND 35-3S-16-02524-111



VICINITY MAP
PROJECT TEAM

WATERSEWER:
LAKE CITY WATER DEPARTMENT
500 HIGHLAND STREET
LAKE CITY, FL 32055
PHONE: (904) 735-8888

CABLE:
LAKE CITY CABLE
500 HIGHLAND STREET
LAKE CITY, FL 32055
PHONE: (904) 735-8888

UTILITY PROVIDERS

ELECTRIC:
FLORIDA POWER & LIGHT
7000 UNIVERSITY BLVD
LAKE CITY, FL 32055
CONTACT: JIMMY STUBBS
PHONE: (904) 398-2200

TELEPHONE:
AT&T
180 N. GAITHER ROAD
WEST PALM BEACH, FL 33411
CONTACT: JIMMY STUBBS
PHONE: (904) 398-2200

OWNER:
GVC DEVELOPMENT PARTNERS LLC
2882 W. HODGEL ROAD
LAKE CITY, FL 32055
CONTACT: EDWARD GUANTA
EMAIL: EGUANTA@GVCDEVELOPMENT.COM

CIVIL ENGINEER:
KIMLEY-HORN AND ASSOCIATES, INC.
188 SOUTH ORANGE AVENUE, SUITE 1000
ORLANDO, FL 32801
CONTACT: JAMES C. STUBBS, P.E.
EMAIL: JAMES.STUBBS@KIMLEY-HORN.COM

SURVEYOR:
JERRY
3630 NW 43RD STREET
GAINESVILLE, FL 32606
CONTACT: MATTHEW FRANKO
PHONE: (352) 375-9899

FIBER OPTIC:
FLORIDA FIBER OPTIC, INC.
1000 UNIVERSITY BLVD, STE 100
LAKE CITY, FL 32055
CONTACT: EDWARD GUANTA
PHONE: (904) 398-2200

GAS:
GTR OF LAKE CITY GASPLANT WORKS
180 N. GAITHER ROAD
WEST PALM BEACH, FL 33411
CONTACT: JIMMY STUBBS
PHONE: (904) 398-2200

DEVELOPER:
CIRCLE K STORES, INC.
3882 CORPORATE PARK DRIVE, SUITE 413
TAMPA, FL 33619
CONTACT: EDWARD GUANTA
PHONE: (407) 568-5175

ARCHITECT:
ROC COLLABORATIVE
11921 FREEDOM DRIVE, SUITE #1110
RESTON, VA 20190
CONTACT: MEGAN LARGENT
PHONE: (703) 506-0086
FAX: (703) 506-0089

LANDSCAPE ARCHITECT:
KIMLEY-HORN AND ASSOCIATES, INC.
188 SOUTH ORANGE AVENUE, SUITE 1000
ORLANDO, FL 32801
CONTACT: MATTHEW FRANKO
PHONE: (352) 375-9899
EMAIL: MATTHEW.FRANKO@KIMLEY-HORN.COM

PREPARED BY



© 2022 KIMLEY-HORN AND ASSOCIATES, INC.
188 SOUTH ORANGE AVENUE, SUITE 1000
ORLANDO, FL 32801
PHONE: (352) 375-9899
WWW.KIMLEY-HORN.COM REGISTERED PROFESSIONAL ENGINEER # 28186

SHEET INDEX

000	COVER SHEET
C1.0-C1.1	GENERAL NOTES
C2.0	STORMWATER POLLUTION PREVENTION PLAN
C3.0-C3.1	EXISTING CONDITIONS & DEMOLITION PLAN
C4.0	OVERALL SITE PLAN
C4.1	SITE PLAN
C4.2	INTERSECTION MODIFICATION PLAN
C4.3-C4.5	TRUCK TURNING MOVEMENTS
C5.0	PAVING, GRADING AND DRAINAGE PLAN
C6.0	UTILITY PLAN
C7.0-C7.1	GENERAL CONSTRUCTION DETAILS
L1.00	LANDSCAPE PLAN
L1.50	LANDSCAPE DETAILS
L1.51	LANDSCAPE SPECIFICATIONS
L2.00	SCHEMATIC IRRIGATION PLAN
L2.50	IRRIGATION DETAILS
L2.51	IRRIGATION NOTES

CIRCLE K - US HWY 90 & I-75 HIGH
SPEED FUEL EXPANSION
MAY 4, 2022
ENGINEER'S PROJECT No. 149880040

NO.	REVISION	DATE

STORMWATER POLLUTION PREVENTION PLAN

<p>PROJECT NAME AND LOCATION: PROJECT K - US HWY 90 & I-75 FUEL EXPANSION 2801 NETWORK BLVD, SUITE 413 CORP OFFICE BLDG 1000 W. GARDNER ST, LAKE CITY, FL 32113 SHEET COVER SHEET FOR LOCATION MAP</p>	<p>PROJECT OPERATOR AND ADDRESS: SOUTHWEST CONSTRUCTION, LLC 2801 NETWORK BLVD, SUITE 413 CORP OFFICE BLDG 1000 W. GARDNER ST, LAKE CITY, FL 32113 PHONE: (407) 592-1511 FAX: (407) 592-1511 EMAIL: INFO@SOUTHWESTCON.COM</p>	<p>PROJECT DESCRIPTION: THE PROJECT WILL CONSIST OF CONSTRUCTING A CORP OFFICE BUILDING, STORMWATER POLLUTION PREVENTION MEASURES, TRUCK WASH STATIONS AND STORMWATER COLLECTION SYSTEMS. THE PROJECT IS SCHEDULED TO BEGIN CONSTRUCTION IN LATE 2022 AND WILL BE COMPLETED BY LATE 2023.</p>	<p>REGULATORY AGENCIES: FEDERAL: U.S. ENVIRONMENTAL PROTECTION AGENCY (EPA) STATE: FLORIDA DEPARTMENT OF ENVIRONMENTAL PROTECTION (FDEP) LOCAL: LAKE COUNTY DEPARTMENT OF ENVIRONMENTAL PROTECTION (LCPD)</p>
<p>REGULATORY AGENCIES: FEDERAL: U.S. ENVIRONMENTAL PROTECTION AGENCY (EPA) STATE: FLORIDA DEPARTMENT OF ENVIRONMENTAL PROTECTION (FDEP) LOCAL: LAKE COUNTY DEPARTMENT OF ENVIRONMENTAL PROTECTION (LCPD)</p>	<p>REGULATORY AGENCIES: FEDERAL: U.S. ENVIRONMENTAL PROTECTION AGENCY (EPA) STATE: FLORIDA DEPARTMENT OF ENVIRONMENTAL PROTECTION (FDEP) LOCAL: LAKE COUNTY DEPARTMENT OF ENVIRONMENTAL PROTECTION (LCPD)</p>	<p>REGULATORY AGENCIES: FEDERAL: U.S. ENVIRONMENTAL PROTECTION AGENCY (EPA) STATE: FLORIDA DEPARTMENT OF ENVIRONMENTAL PROTECTION (FDEP) LOCAL: LAKE COUNTY DEPARTMENT OF ENVIRONMENTAL PROTECTION (LCPD)</p>	<p>REGULATORY AGENCIES: FEDERAL: U.S. ENVIRONMENTAL PROTECTION AGENCY (EPA) STATE: FLORIDA DEPARTMENT OF ENVIRONMENTAL PROTECTION (FDEP) LOCAL: LAKE COUNTY DEPARTMENT OF ENVIRONMENTAL PROTECTION (LCPD)</p>
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CIRCLE K - US
 HWY 90 & I-75
 FUEL EXPANSION
 CITY OF LAKE CITY FLORIDA

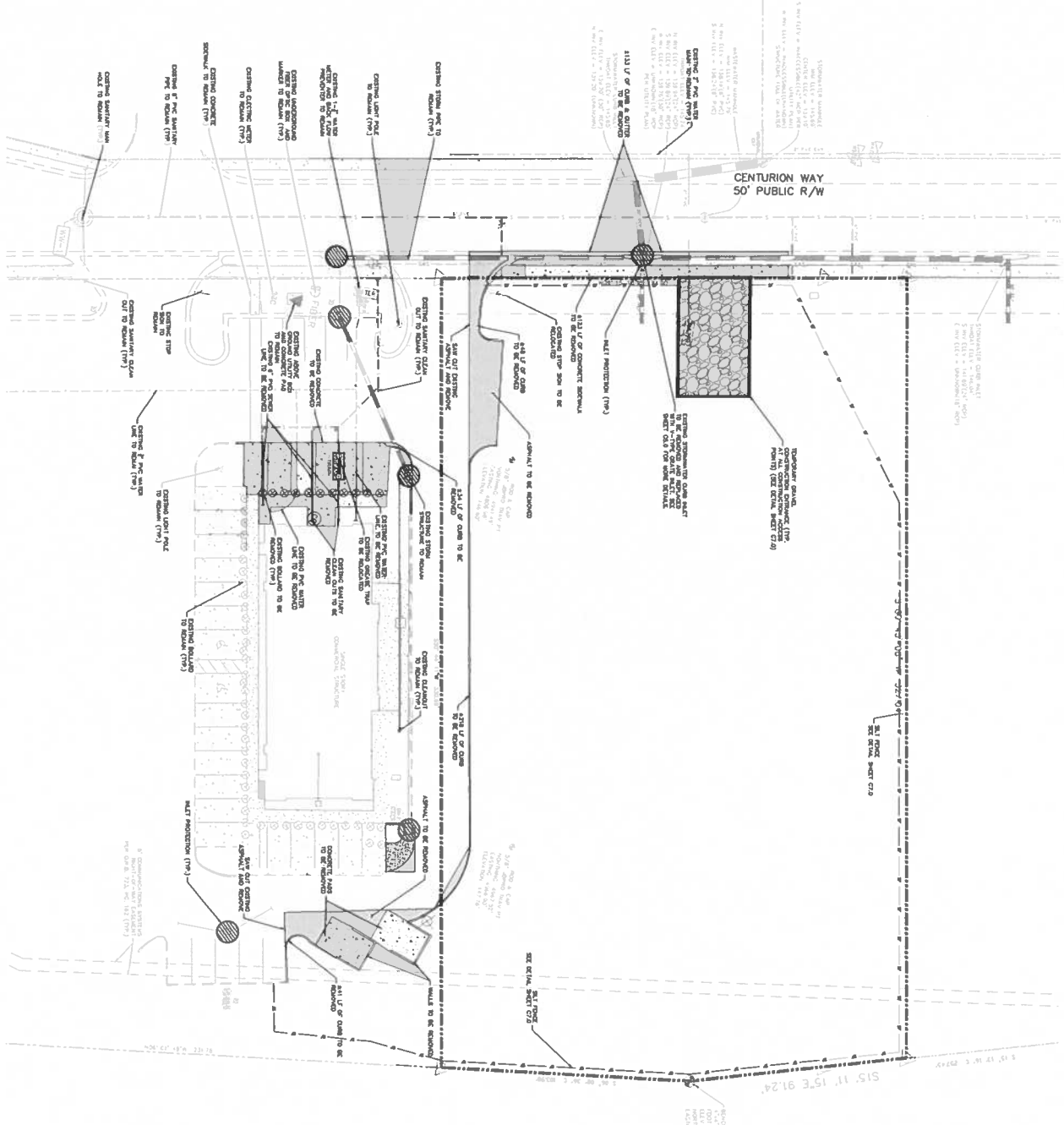
**STORMWATER
 POLLUTION
 PREVENTION PLAN**

PROJECT NUMBER 14880040	DATE 05/04/2022
SCALE AS SHOWN	DESIGNED BY J.C.P.
DRAWN BY J.C.P.	CHECKED BY J.C.P.

Kimley Horn

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 100 S. ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801
 PHONE: 407-859-1511
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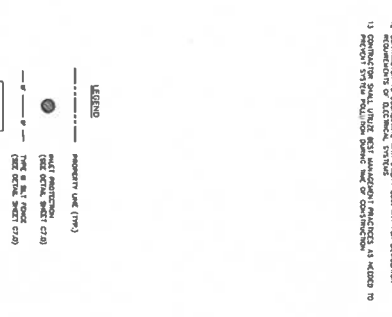
NO.	REVISIONS	DATE



SECTION: 3000
DATE: 05/04/2022
DRAWN BY: JCS
CHECKED BY: JCS

CALL FOR BIDDING
REVISIONS
DATE: 05/04/2022
DRAWN BY: JCS
CHECKED BY: JCS

- NOTES**
1. CONTRACTOR TO coordinate with utility company for location, identification and marking of all existing utility lines.
 2. CONTRACTOR TO verify all existing utility lines are marked and labeled as required by the applicable code.
 3. CONTRACTOR TO verify all existing utility lines are marked and labeled as required by the applicable code.
 4. CONTRACTOR TO verify all existing utility lines are marked and labeled as required by the applicable code.
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 13. CONTRACTOR TO verify all existing utility lines are marked and labeled as required by the applicable code.



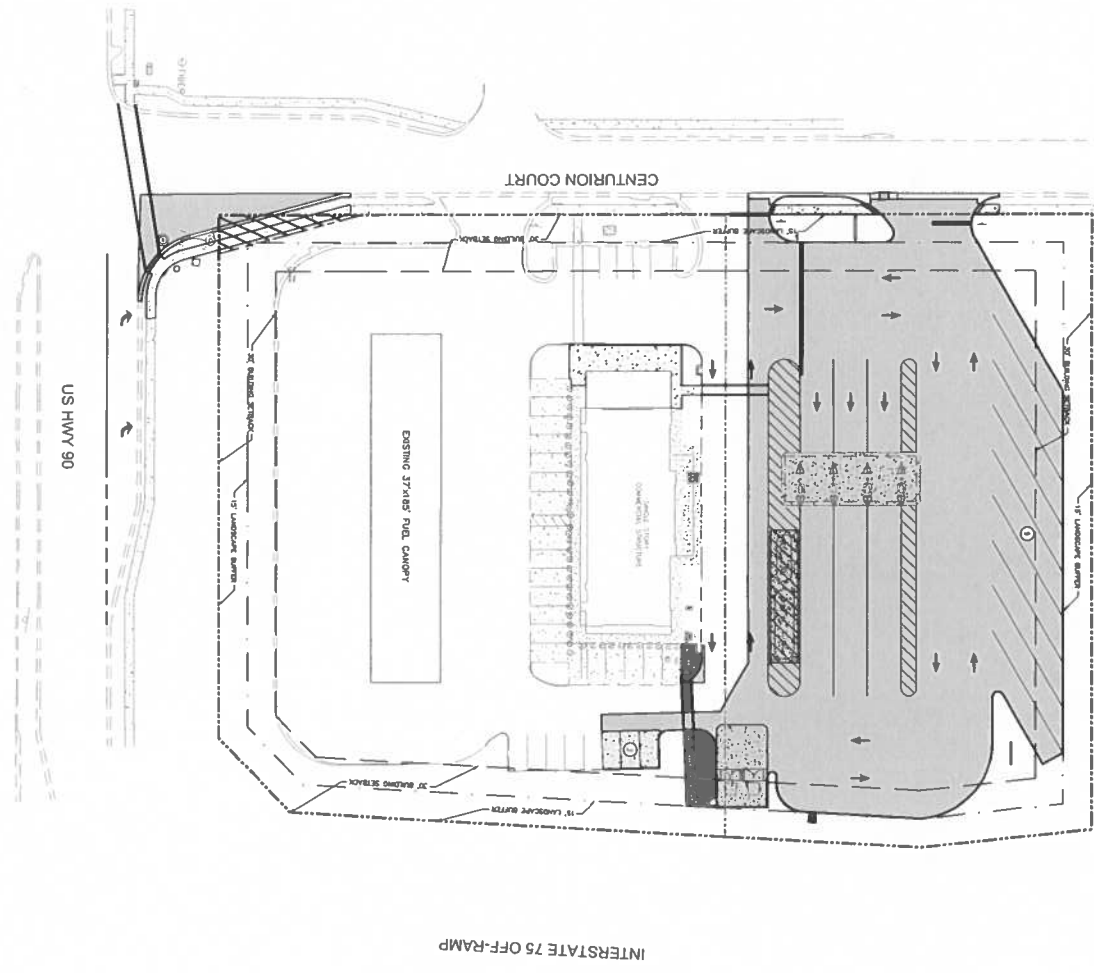
LEGEND

- PROPERTY LINE (THIN)
- EXISTING DEMOLITION (THIN)
- EXISTING CONCRETE (THICK)
- EXISTING STRUCTURE (THICK)
- EXISTING ASPHALT (THICK)
- EXISTING DRIVE (THICK)
- EXISTING SIDEWALK (THICK)
- EXISTING CURB (THICK)
- EXISTING DRIVE (THICK)
- EXISTING SIDEWALK (THICK)
- EXISTING CURB (THICK)

Graphic Scale in Feet
0 10 20 30 40 50

North Arrow

Kimley-Horn
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105 S ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801
PHONE: 407-888-1511
WWW.KIMLEY-HORN.COM PROJECT: No 35106



- NOTES:**
1. ALL CURB AND GUTTERS ARE TO BE FACE OF CURB UNLESS OTHERWISE NOTED.
 2. ALL CURB AND GUTTERS ARE TO BE EXTERIOR FACE OF SLOPING WALLS UNLESS OTHERWISE NOTED.
 3. SLOPE SHALL BE AS SHOWN OR SUFFICIENT TO PREVENT ALL STAGNATION.
 4. NOTES TO SPECIFIC PLAN ARE TO SUPERSEDE THIS GENERAL NOTE.
 5. ALL PROPOSED CURB, GUTTERS AND MANHOLES SHALL BE PERMITTED TO BE CONSTRUCTED AT ANY TIME.
 6. SEE ALL NOTES TO SPECIFIC PLAN FOR ALL ELECTRICAL, PLUMBING AND MECHANICAL REQUIREMENTS.
 7. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF ALL APPLICABLE CODES AND ORDINANCES.
 8. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF ALL APPLICABLE CODES AND ORDINANCES.
 9. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF ALL APPLICABLE CODES AND ORDINANCES.
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 12. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF ALL APPLICABLE CODES AND ORDINANCES.

SITE DATA:

PROJECT AREA: 3.481 ACRES (152,814 SQ. FT.)
 TOTAL SITE AREA: 3.481 ACRES (152,814 SQ. FT.)
 EXISTING IMPAVMENT: ASPHALT
 EXISTING UTILITIES: GAS, WATER, SANITARY SEWER, STORM SEWER
 EXISTING STRUCTURES: EXISTING 37-SPACE FUEL CANOPY, EXISTING OFFICE BUILDING, EXISTING RESTROOM, EXISTING LIGHT FIXTURES
 PROPOSED STRUCTURES: PROPOSED 102-SPACE FUEL CANOPY, PROPOSED OFFICE BUILDING, PROPOSED RESTROOM, PROPOSED LIGHT FIXTURES
 PROPOSED UTILITIES: PROPOSED GAS, WATER, SANITARY SEWER, STORM SEWER
 PROPOSED SITE AREA: 3.481 ACRES (152,814 SQ. FT.)
 PROPOSED IMPAVMENT: ASPHALT
 PROPOSED UTILITIES: GAS, WATER, SANITARY SEWER, STORM SEWER
 PROPOSED STRUCTURES: PROPOSED 102-SPACE FUEL CANOPY, PROPOSED OFFICE BUILDING, PROPOSED RESTROOM, PROPOSED LIGHT FIXTURES
 TOTAL PROPOSED AREA: 3.481 ACRES (152,814 SQ. FT.)

LEGEND:

DESCRIPTION	NOTATION
PROPOSED ASPHALT PAVEMENT	ASPH
PROPOSED CONCRETE PAVEMENT	CONC
PROPOSED CEMENT CURB	CURB
PROPOSED GUTTER	GUTTER
PROPOSED MANHOLE	MANHOLE
PROPOSED ELECTRICAL SERVICE	ELECT
PROPOSED WATER SERVICE	WATER
PROPOSED SANITARY SEWER SERVICE	SEWER
PROPOSED STORM SEWER SERVICE	STORM
PROPOSED LIGHT FIXTURES	LIGHT
PROPOSED RESTROOM	RESTROOM
PROPOSED OFFICE BUILDING	OFFICE
PROPOSED 102-SPACE FUEL CANOPY	FUEL CANOPY
EXISTING 37-SPACE FUEL CANOPY	FUEL CANOPY
EXISTING OFFICE BUILDING	OFFICE
EXISTING RESTROOM	RESTROOM
EXISTING LIGHT FIXTURES	LIGHT
EXISTING ELECTRICAL SERVICE	ELECT
EXISTING WATER SERVICE	WATER
EXISTING SANITARY SEWER SERVICE	SEWER
EXISTING STORM SEWER SERVICE	STORM

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CALL AHEAD OF ANY EXCAVATION TO LOCATE UTILITIES

1-811-3333

ORANGE COUNTY

FLORIDA

11811 11811 11811

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11811 11811 11811



GRAPHIC SCALE IN FEET

0 10 20

30 40

50 60

70 80

90

NO.	REVISIONS	DATE	BY

Kimley Horn

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 PHONE: 407-898-1511
 WWW.KIMLEY-HORN.COM REGISTERED NO. 31006

PROJECT NO. 149880040
 SCALE AS SHOWN
 DESIGNED BY C.F.
 DRAWN BY C.F.
 CHECKED BY C.F.
 DATE 05/04/2022

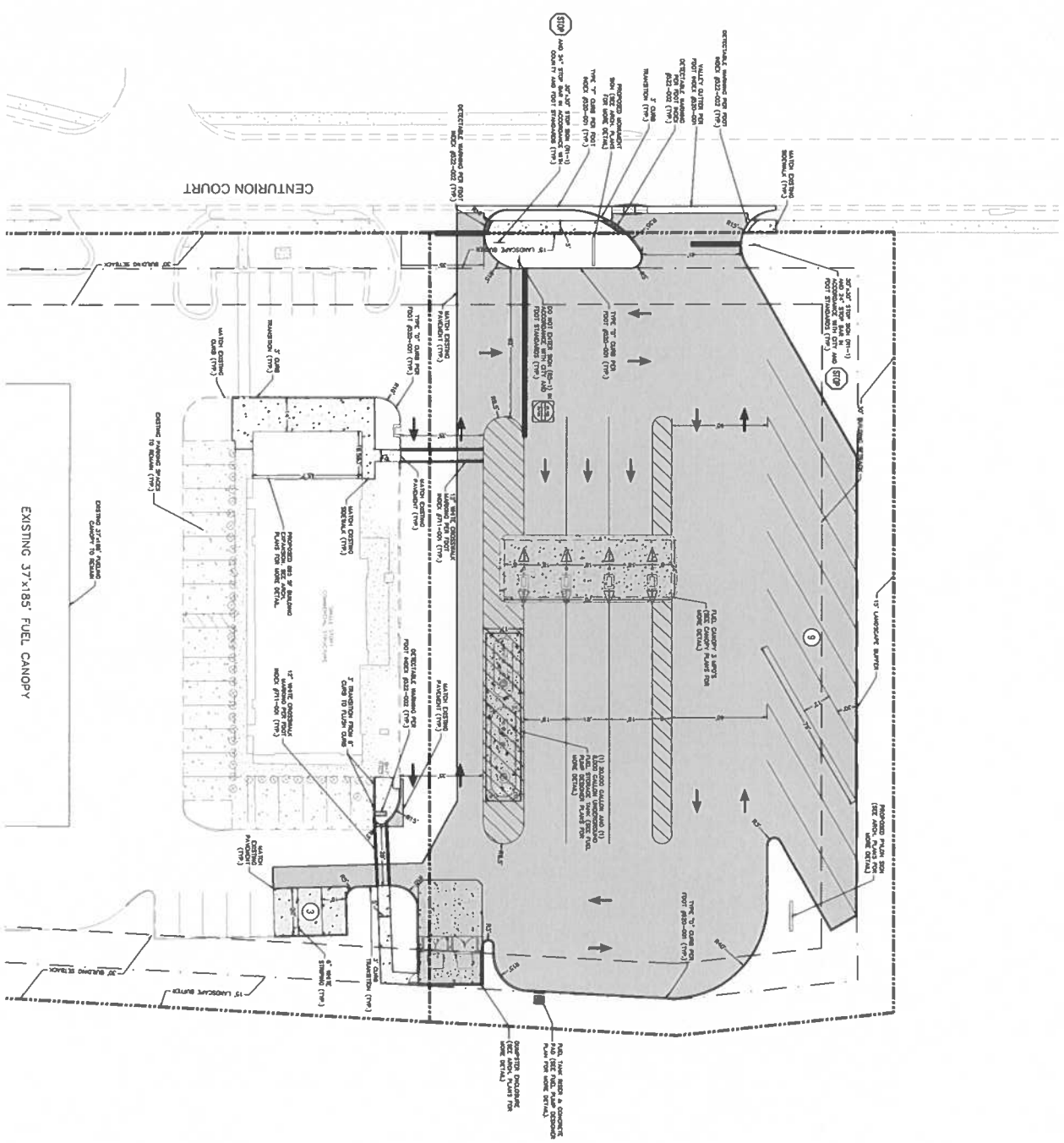
OVERALL SITE PLAN

FLORIDA

CITY OF LAKE CITY

CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION

SHEET NUMBER CA.0



LEGEND

- Proposed Lot (75')
- Proposed Asphalt Paved Area (See Detail Sheet C74)
- Proposed Concrete Driveway (See Detail Sheet C75)
- Proposed Asphalt Drive Concrete (See Detail Sheet C76)
- Proposed Concrete (See Detail Sheet C77)
- Proposed Concrete Slab (See Detail Sheet C78)
- Proposed Concrete Slab (See Detail Sheet C79)
- Proposed Concrete Slab (See Detail Sheet C80)

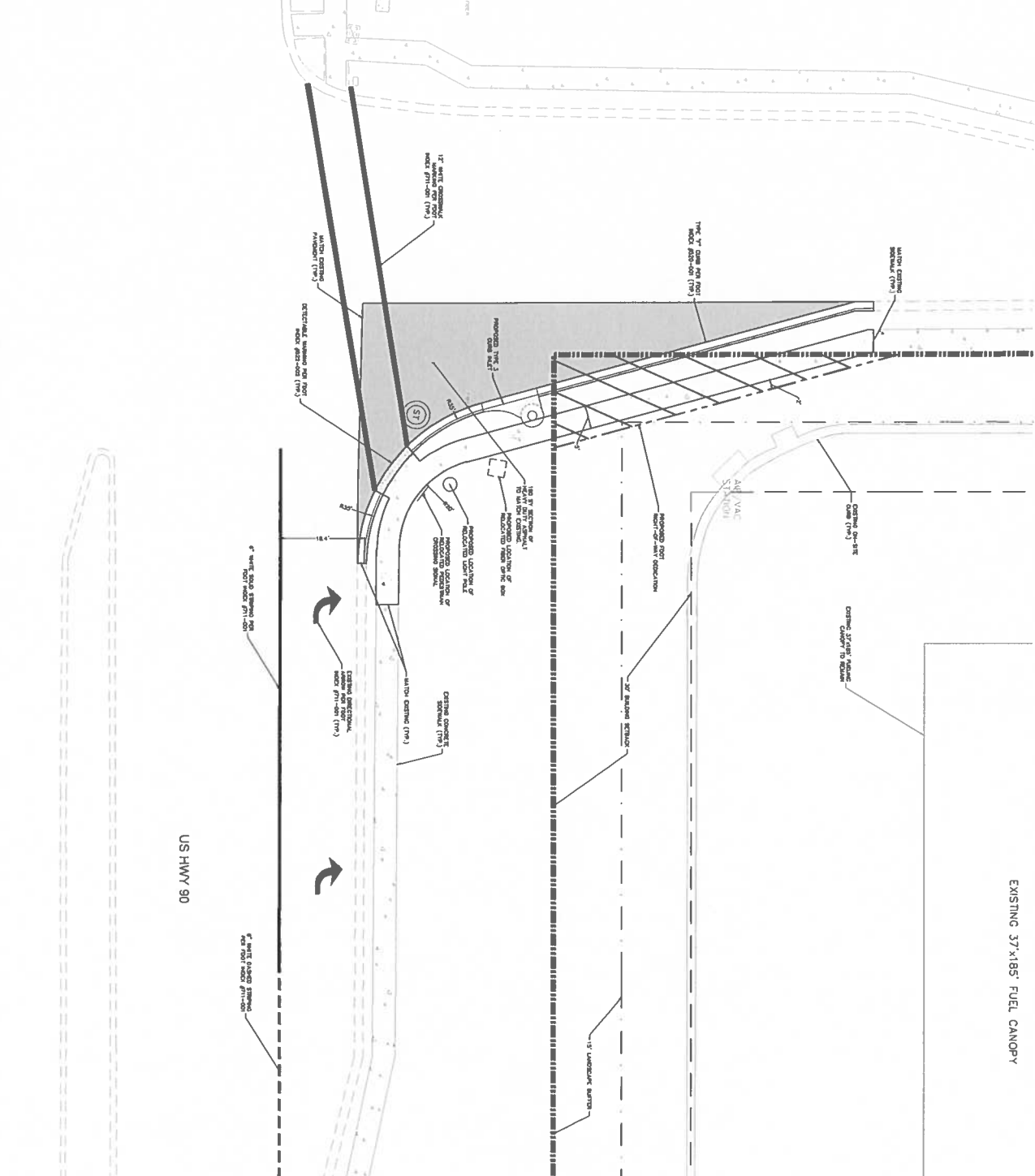
NOTES

1. All curb cutouts are to the face of curb unless otherwise indicated.
2. All dimensions are to the center line of building unless otherwise noted.
3. All dimensions are to the face of building unless otherwise noted.
4. All dimensions are to the face of building unless otherwise noted.
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12. All dimensions are to the face of building unless otherwise noted.

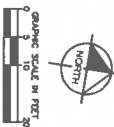
GRAPHIC SCALE IN FEET
0 10 20 40

811
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CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION CITY OF LAKE CITY FLORIDA SHEET NUMBER CA.1	SITE PLAN		KHA PROJECT 14880040 DATE 05/04/2022 SCALE AS SHOWN DESIGNED BY C.P. DRAWN BY C.P. CHECKED BY JCS	Kimley & Horn 189 S. ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801 WWW.KIMLEY-HORN.COM REGISTRY NO. 35108
	DATE BY	NO.	REVISED	DATE BY



- NOTES**
1. CANOPY DIMENSIONS ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
 2. BALLOON DIMENSIONS ARE TO THE OUTSIDE FACE OF BALLOON UNLESS OTHERWISE NOTED.
 3. DIMENSIONS SHOWN ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
 4. ALL SPACING AND FINISHES UNLESS SHOWN SHALL BE AS SHOWN AND NOT TO SCALE.



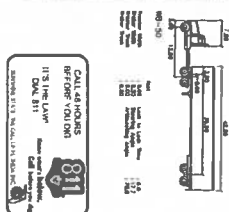
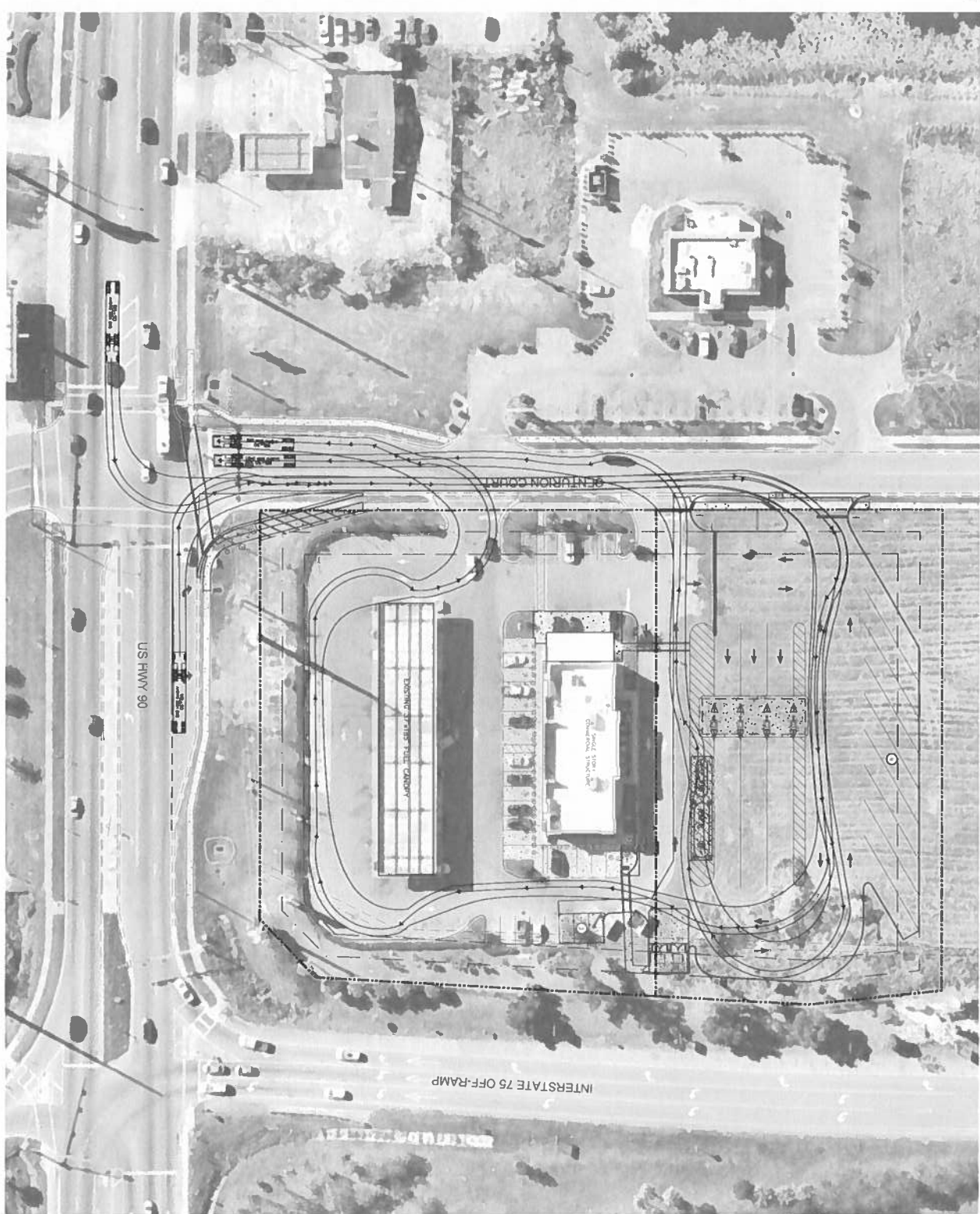
LEGEND

[Symbol]	PROPOSED LANE (11'6")
[Symbol]	PROPOSED ASPHALT PAVEMENT (SEE DETAIL SHEET C7.20)
[Symbol]	PROPOSED ASPHALT DRIVEWAY (SEE DETAIL SHEET C7.20)
[Symbol]	PROPOSED ASPHALT DRIVEWAY CONCRETE (SEE DETAIL SHEET C7.20)
[Symbol]	PROPOSED ASPHALT DRIVEWAY CONCRETE (SEE DETAIL SHEET C7.20)

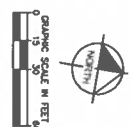
GENERAL NOTES:
 DIMENSIONS ARE REFERENCED TO THE FACE OF THE CURB UNLESS OTHERWISE NOTED.
 ALL DIMENSIONS ARE TO THE FACE OF THE CURB UNLESS OTHERWISE NOTED.
 ALL DIMENSIONS ARE TO THE FACE OF THE CURB UNLESS OTHERWISE NOTED.

CALL TO ACTIONS:
 BEFORE YOU BID
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<p>CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION</p> <p>CITY OF LAKE CITY FLORIDA</p>	<p>INTERSECTION MODIFICATION PLAN</p>	<p>KHA PROJECT 149880040 DATE 05/04/2022 SCALE AS SHOWN DESIGNED BY J.F. DRAWN BY J.F. CHECKED BY J.C.F.</p>		<p>Kimley-Horn</p> <p>© 2022 KIMLEY-HORN AND ASSOCIATES, INC. 183 S. ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801 PHONE: 407-882-1511 WWW.KIMLEY-HORN.COM REGISTERED No. 35106</p>	<table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th style="width: 5%;">No.</th> <th style="width: 70%;">REVISIONS</th> <th style="width: 10%;">DATE</th> <th style="width: 15%;">BY</th> </tr> </thead> <tbody> <tr> <td> </td> <td> </td> <td> </td> <td> </td> </tr> </tbody> </table>	No.	REVISIONS	DATE	BY				
No.	REVISIONS	DATE	BY										



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 DIAL 811
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 ORANGE COUNTY
 FLORIDA



CIRCLE K - US HWY 90 & I-75 FUEL TRUCK EXPANSION
 CITY OF LAKE CITY FLORIDA
 SHEET NUMBER: C4.3

TRUCK TURNING MOVEMENTS

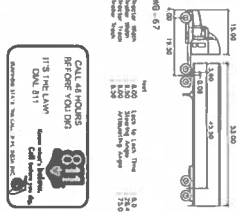
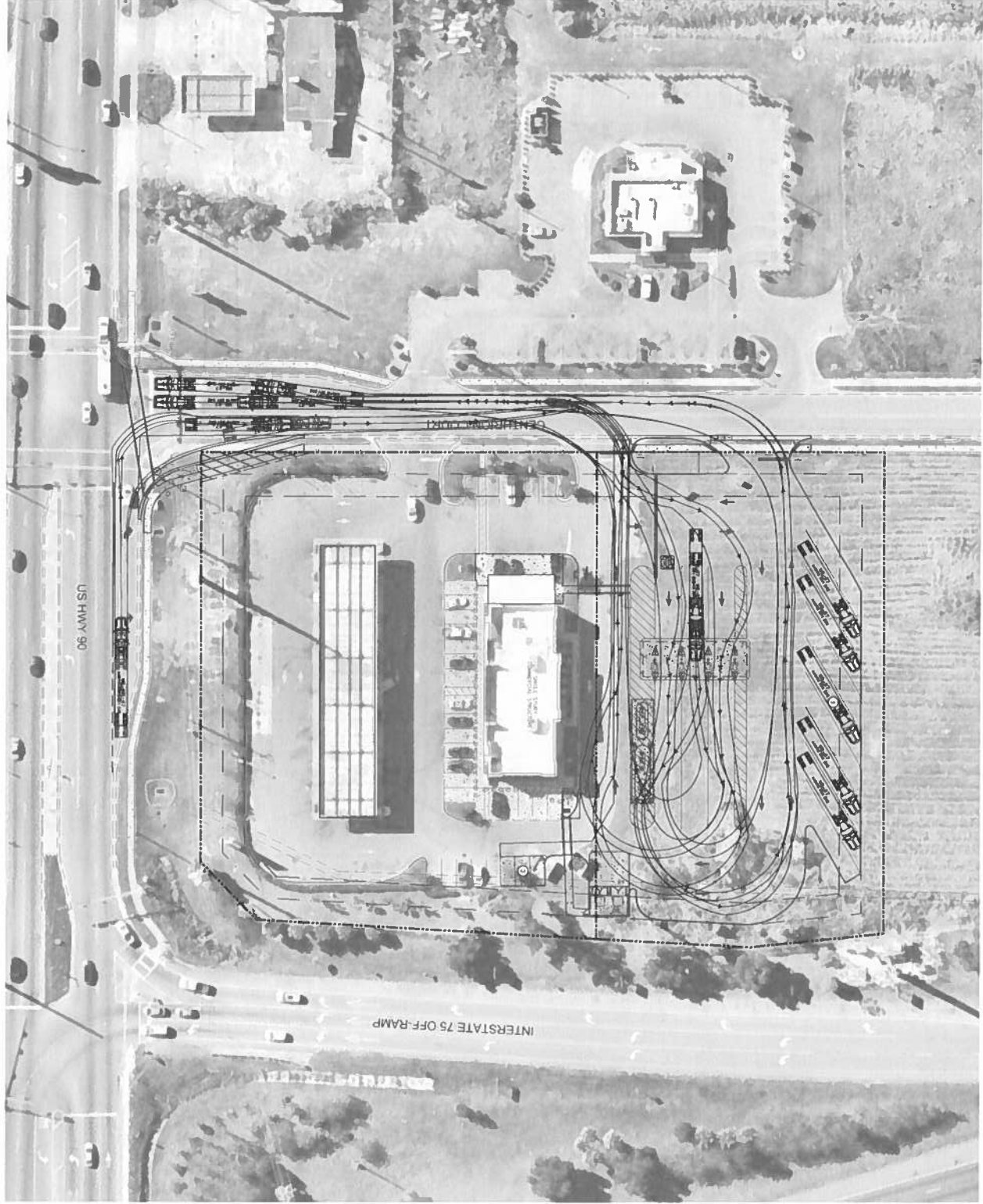
PROJECT: 148880040
 DATE: 05/04/2022
 SCALE: AS SHOWN
 DESIGNED BY: E.F.
 DRAWN BY: E.F.
 CHECKED BY: JCS
 DATE: 05/04/2022



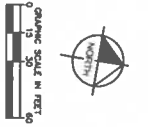
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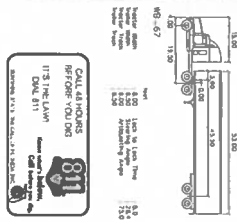
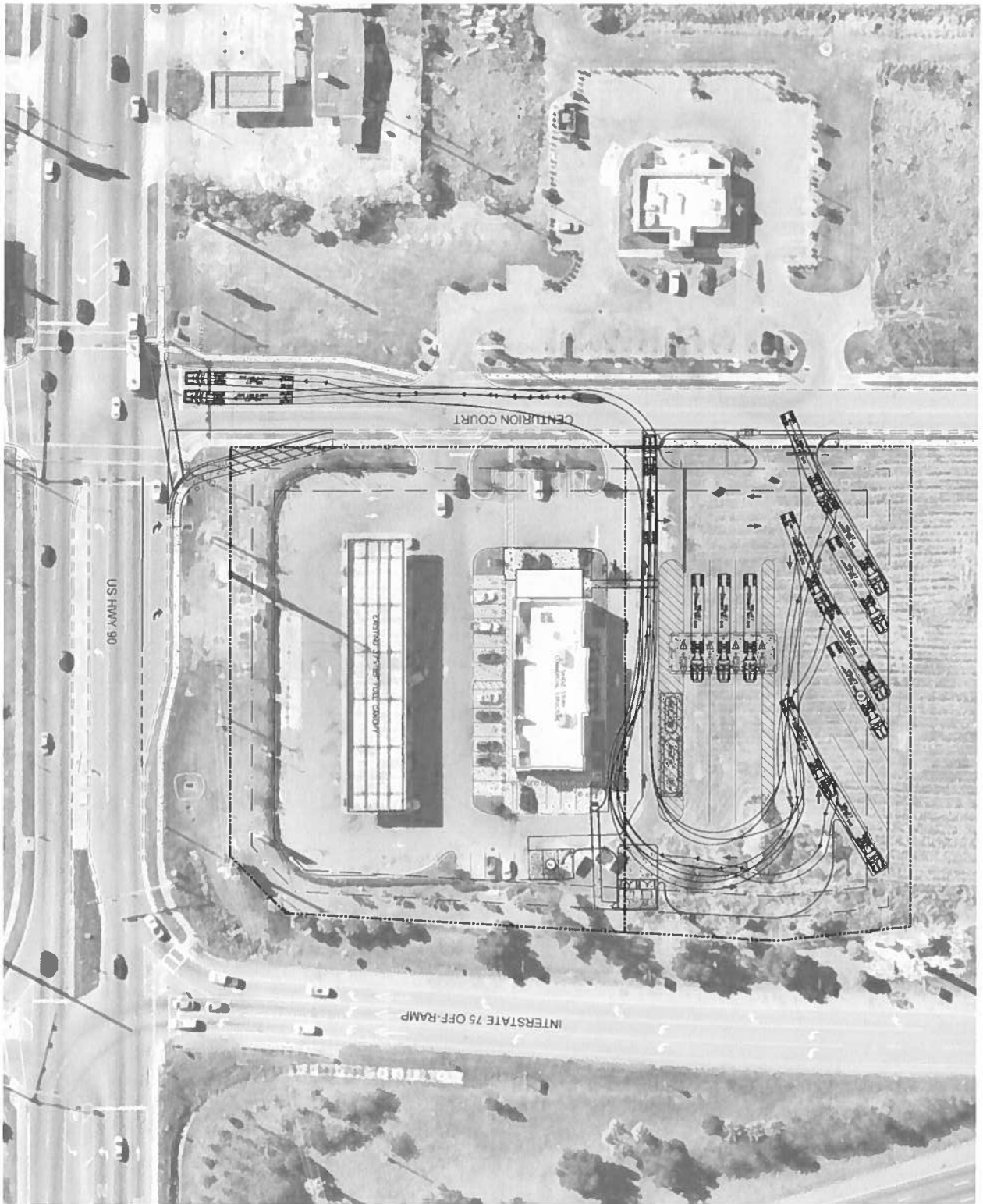
**CIRCLE K - US HWY
90 & I-75 FUEL
EXPANSION**
CITY OF LAKE CITY FLORIDA

**TRUCK TURNING
MOVEMENTS**

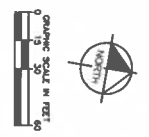
100% PROJECT
149880040
DATE
05/04/2022
SCALE AS SHOWN
DESIGNED BY E.J.F.
DRAWN BY E.J.F.
CHECKED BY JCS
DATE 05/04/2022

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No.	REVISIONS	DATE	BY



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 DALLAS, TX 75201
 214.520.5000



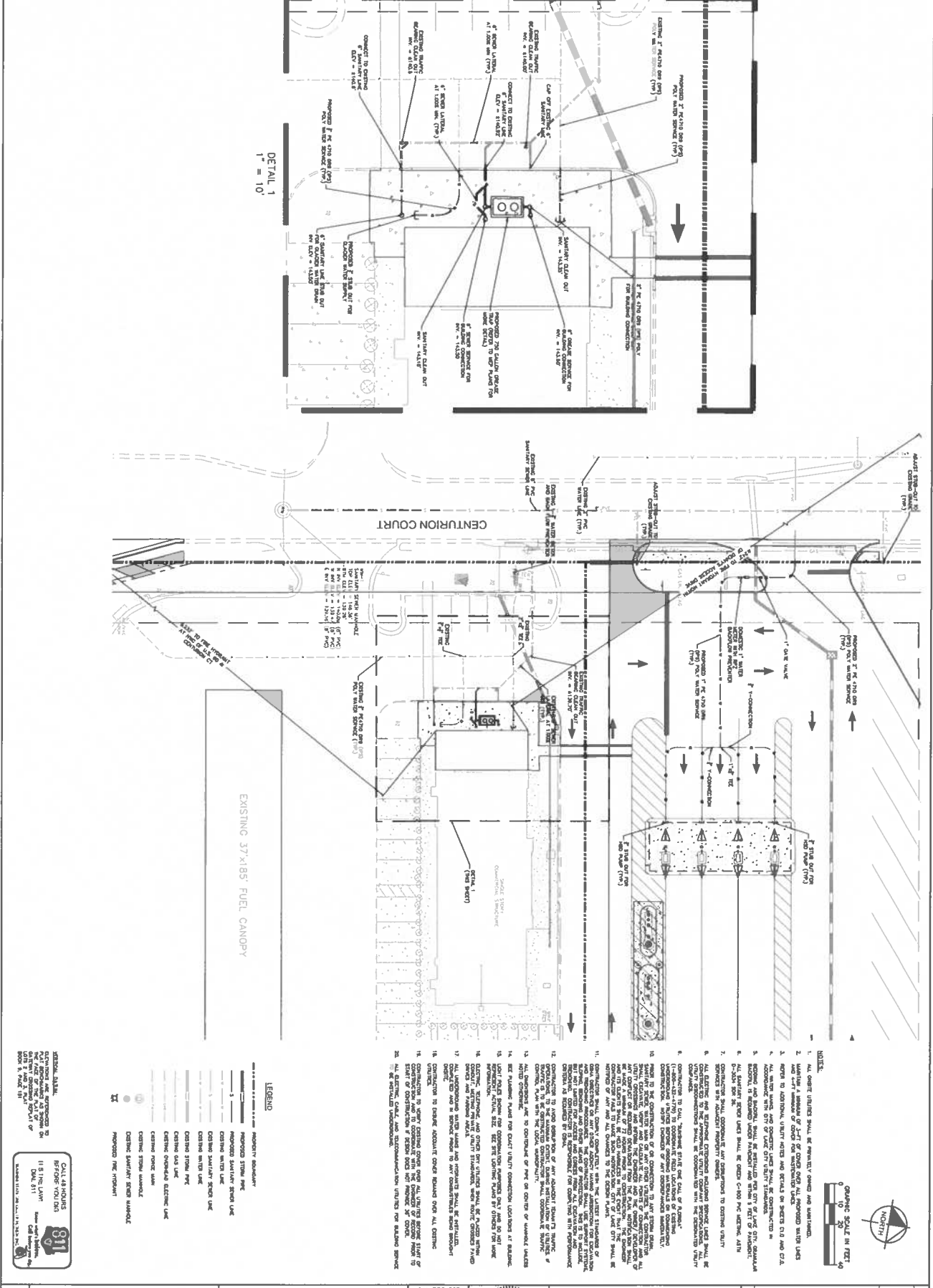
CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION
 CITY OF LAKE CITY FLORIDA

C4.5 - TRUCK TURNING MOVEMENTS

KHA PROJECT 148880040
 DATE 05/04/2022
 SCALE AS SHOWN
 DESIGNED BY E.F.
 DRAWN BY E.F.
 CHECKED BY JCS

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No.	REVISIONS	DATE	BY



DETAIL 1
1" = 10'

EXISTING 37'x185' FUEL CANOPY

- NOTES:**
1. ALL EXISTING UTILITIES SHALL BE IDENTIFIED, DEPTH, AND MATERIALS.
 2. MATERIALS SHALL BE IDENTIFIED BY PIPE SIZE AND DEPTH.
 3. ALL UTILITIES SHALL BE IDENTIFIED BY PIPE SIZE AND DEPTH.
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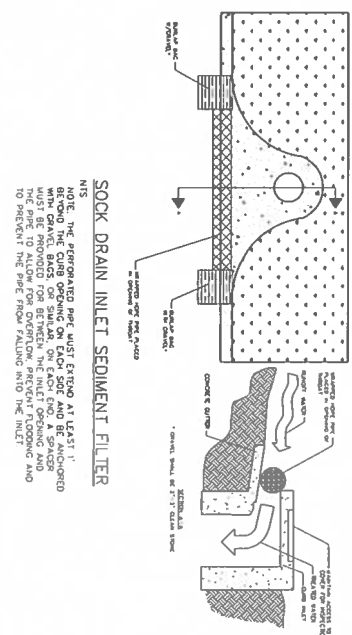
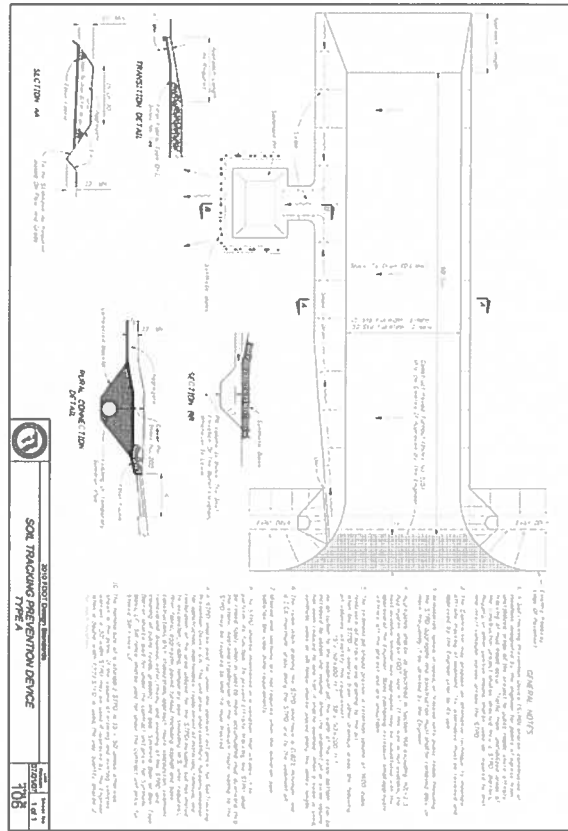
LEGEND

- PROPOSED BOUNDARY
- PROPOSED STORM LINE
- PROPOSED SANITARY STORM LINE
- EXISTING WATER LINE
- EXISTING SANITARY SEWER LINE
- EXISTING STORM PIPE
- EXISTING OVERHEAD ELECTRICAL LINE
- EXISTING POWER LINE
- EXISTING STORM MAINLINE
- EXISTING SANITARY SEWER MAINLINE
- PROPOSED FUEL MAINLINE


REVISIONS:

NO.	DESCRIPTION	DATE
1	ISSUED FOR PERMIT	08/22/2022

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OR VISIT WWW.811.FLORIDA.GOV



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SHEET NUMBER C8.0	CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION CITY OF LAKE CITY FLORIDA	CITY OF LAKE CITY STANDARD DETAILS	JOB: PROJECT 148880040	LICENSED PROFESSIONAL					
			DATE 05/04/2022						
			SCALE AS SHOWN	© 2022 KIMLEY-HORN AND ASSOCIATES, INC. 1815 ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801 PHONE: 407-882-1511 WWW.KIMLEY-HORN.COM REGISTRY NO. 35106					
			DESIGNED BY: E.F.						
			DRAWN BY: E.F.						
			CHECKED BY: JCS						
			DATE						
						No.	REVISIONS	DATE	BY

EXHIBIT

B



DEPARTMENT OF GROWTH MANAGEMENT

205 North Marion Avenue

Lake City, FL 32055

Telephone: (386) 719-5750

growthmanagement@lcfla.com

July 7, 2022

Circle K - US 90 & I-75
143 NW Centurion Ct
Lake City FL 32055

To Whom it May Concern

This is to inform you that Petition # SPR22-15 requesting a Site Plan Review on parcel 35-3S-16-02524-001, 102, and 111 which is in a Commercial Highway Interchange (CHI) zoning district was approved by the Planning and Zoning/Board of Adjustment on July 6, 2022.

If I can be of further assistance to you, please feel free to contact me at 386-752-2031 ext 820 or email at angelor@lcfla.com.

Sincerely,

Robert Angelo

Planning and Zoning Tech.

EXHIBIT

C



DEPARTMENT OF GROWTH MANAGEMENT
205 North Marion Avenue
Lake City, Florida 32055
Telephone: (386) 719-5750
growthmanagement@lcfla.com

REVIEW REPORT TO PLANNING AND ZONING, BOARD OF ADJUSTMENT AND
HISTORICAL COMMITTEES' BY STAFF
FOR SITE PLAN REVIEW, SPECIAL EXCEPTIONS, VARIANCES, COMPREHENSIVE
PLAN AMENDMENTS/ ZONING AND CERTIFICATE OF APPROPRIATENESS

Date: 6/15/22

Request Type: Site Plan Review (SPR) Special Exception (SE) Variances (V)

Comprehensive Plan Amendment/Zoning (CPA/Z) Certificate of Appropriateness (COA)

Project Number: SPR22-15

Project Name: Circle K-US 90 and I75 (Gateway Crossings)

Project Address: 143 NW Centurion CT, Lake City FL

Project Parcel Number: 35-3S-16-02524-001, 102, and 111

Owner Name: Daniel Hotte of GWC Development Partners, LLC

Owner Address: 2682 W Noegel RD

Owner Contact Information: telephone number 407-580-5173 e-mail dberry@shafferconst.com

Owner Agent Name: Jarod Stubbs P.E.

Owner Agent Address: 180 S. Orange Ave, Suite 1000 Orlando FL 32801

Owner Agent Contact Information: telephone 407-409-7002 e-mail jarod.stubbs@kimley-horne.com

The City of Lake City staff has reviewed the application and documents provided for the above request and have determined the following:

Growth Management – Building Department, Planning and Zoning, Code Enforcement, Permitting

Building Department: Approved Disapproved Reviewed by: _____

Comments: N/A

Planning and Zoning: Approve Disapprove Reviewed by: Robert Angelo

Comments: No Concerns at this time

No Concerns at this time

Business License: Approve Disapprove Reviewed by: Marshall Sova

Comments: No Concerns at this time

Code Enforcement: Approve Disapprove Reviewed by: Marshall Sova

Comments: No Concerns at this time

Permitting: Approve Disapprove Reviewed by: Ann Jones

Comments: No Concerns at this time

No Concerns at this time

Utilities – Water, Sewer, Gas, Water Distribution/Collections, Customer Service

Water Department: Approved Disapproved Reviewed by: _____

Comments: N/A

Sewer Department: Approved Disapproved Reviewed by: _____

Comments: N/A

Gas Department: Approved Disapproved Reviewed by: Steve Brown

Comments: No Concerns at this time

WaterDistribution/Collection: Approved Disapproved Reviewed by: Brian Scott

Comments: _____
If they do not use the taps in place they will be required to make new ones and cut and cap sewer and dig to water main and shut off before construction.

Customer Service: Approved Disapproved Reviewed by: Shasta Pelham

Comments: Utility Plan 6.0 dated 05/04/22 references a 1" water meter and an existing 6" sewer tap. A tap application would be required to access city utilities.

The tap fees, impact fees and utility deposits will be calculated upon approval of the tap application. A floor plan with detailed fixture units of the restroom addition is required. City utilities border the property; locates must be obtained to ensure that the utility infrastructure is not damaged or obstructed.

Public Safety – Public Works, Fire Department, Police Department

Public Works: Approved Disapproved Reviewed by: Steve Brown

Comments: No Concerns at this time

Fire Department: Approve Disapprove Reviewed by: Assistant Chief Boozer

Comments: No Concerns at this time

Police Department: Approve Disapprove Reviewed by: Assistant Chief Andy

Comments: No Concerns at this time

Please provide separate pages for comments that will not fit in provided spaces and please label the pages for your department and for the project.

EXHIBIT

D



City of Lake City, FL.
New Commercial Construction Permit #000046609
Issued February 28, 2023



* Request inspections by calling 386-719-2023 or visiting <https://www.columbiacountyfla.com/PermitSearch/InspectionCalendar.aspx>

OWNER: GWC DEVELOPMENT PARTNERS LLC **PHONE:** 800-280-0780 **ADDRESS:** ,

PARCEL: 35-3S-16-02524-102 **ZONING:** **FLOOD ZONE:** X **Coords:** 30.18,-82.69

SUBDIVISION: GATEWAY CROSSING A REPLAT OF LOTS 2,3 & 11 **LOT:** 2 **BLK:** **PHASE:** **UNIT:**
ACRES: 1.03

CONTRACTORS

ADDRESS: 11245 OLD ROSWELL RD **PHONE:** 770-595-4317

NAME: CHRISTOPHER PEDEN **ALPHARETTA, GA 30009** **LICENSE:** CBC1265254 -

BUSINESS: US GENERAL CONSTRUCTION INC

License	License Title	Contractor	Business
EC0001861	CERT. ELECTRICAL	BILLY J PARMER	K & D ELECTRIC
CFC1427145	PLUMBING CONTRACTOR	CODY BARRS	BARRS PLUMBING INC
CCC1333195	CERT. ROOFING	JAMES M HORSLEY	HORSLEY CONSTRUCTION GROUP INC

PROJECT DETAILS

THIS IS THE CONSTRUCTION OF A:: Diesel Canopy and Underground Storage Tanks
DESCRIBE COMMERCIAL USE:: Convenience Store with Fuel & Diesel
HEATED AREA (SQFT): 0
TOTAL AREA (SQFT): 1680
STORIES: 0
BUILDING HEIGHT: 20
DRIVEWAY ACCESS TO PROPERTY: D.O.T. Permit
IS THERE A FIRE SPRINKLER SYSTEM?: No
DEV PERMIT #: F023-
SERVICE AMPS: 800
BUILDING CODE EDITION: 2020 Florida Building Code 7th Edition and 2017 National Electrical Code
FEMA MAP NUMBER : 12023C0290D
SEALED ROOF DECKING OPTIONS. (MUST SELECT ONE.): Other (explain)

NOTICE: Addition to the requirements of this permit, there may be restrictions applicable to this property that may be found in the public records of this county and there may be additional permits required from other governmental entities such as water management districts, state or federal agencies.

"WARNING TO OWNER: YOUR FAILURE TO RECORD A NOTICE OF COMMENCEMENT MAY RESULT IN YOUR PAYING TWICE FOR IMPROVEMENTS TO YOUR PROPERTY. A NOTICE OF COMMENCEMENT MUST BE POSTED ON THE JOB SITE BEFORE THE FIRST INSPECTION. IF YOU INTEND TO OBTAIN FINANCING, CONSULT WITH YOUR LENDER OR AN ATTORNEY BEFORE RECORDING YOUR NOTICE OF COMMENCEMENT."

NOTICE: All work is to be completed in accordance with the permitted plans and applicable codes of Columbia County, Florida. In order to maintain a valid permit the work authorized must commence within 180 days of issuance and have an approved inspection within every 180 days thereafter.

MUST POST ON THE JOBSITE: Copies of the Permit and Recorded Notice of Commencement for inspection.

2/28/2023 11:53 AM



City of Lake City, FL.
Additions Permit #000046606
Issued February 28, 2023



* Request inspections by calling 386-719-2023 or visiting <https://www.columbiacountyfla.com/PermitSearch/InspectionCalendar.aspx>

OWNER: ASPRI INVESTMENTS LLC **PHONE:** 800-280-0780 **ADDRESS:** 143 NW CENTURION CT LAKE CITY, FL 32055

PARCEL: 35-3S-16-02524-001 **ZONING:** **FLOOD ZONE:** X **Coords:** 30.18,-82.69

SUBDIVISION: GATEWAY CROSSING **LOT:** 1 **BLK:** **PHASE:** **UNIT:** **ACRES:** 1.97

CONTRACTORS

NAME: CHRISTOPHER PEDEN

ADDRESS:
11245 OLD ROSWELL RD
ALPHARETTA, GA 30009

PHONE: 770-595-4317

LICENSE: CBC1265254 -

BUSINESS: US GENERAL CONSTRUCTION INC

License	License Title	Contractor	Business
EC0001861	CERT. ELECTRICAL	BILLY J PARMER	K & D ELECTRIC
CFC1427145	PLUMBING CONTRACTOR	CODY BARRS	BARRS PLUMBING INC
CCC1333195	CERT. ROOFING	JAMES M HORSLEY	HORSLEY CONSTRUCTION GROUP INC

PROJECT DETAILS

IS THIS AN ADDITION FOR COMMERCIAL OR RESIDENTIAL USE?:	Commercial
DESCRIPTION OF ADDITION::	Restroom expansion & dumpster enclosure
TOTAL ESTIMATED COST:	344500
HEATED AREA (SQFT):	6327
TOTAL AREA (SQFT):	6327
STORIES:	1
BUILDING HEIGHT:	10
SELECT DRIVEWAY ACCESS TO PROPERTY:	D.O.T. Permit
FIRE SPRINKLERS?:	No
SETBACKS FRONT:	30'
SETBACK SIDE 1:	30'
SETBACK SIDE 2:	30'
SETBACK REAR:	30'
SERVICE AMPS:	800
DEV PERMIT #:	F023-
BUILDING CODE EDITION:	2020 Florida Building Code 7th Edition and 2017 National Electrical Code
FEMA MAP NUMBER :	12023C0290D

NOTICE: Addition to the requirements of this permit, there may be restrictions applicable to this property that may be found in the public records of this county and there may be additional permits required from other governmental entities such as water management districts, state or federal agencies.

"WARNING TO OWNER: YOUR FAILURE TO RECORD A NOTICE OF COMMENCEMENT MAY RESULT IN YOUR PAYING TWICE FOR IMPROVEMENTS TO YOUR PROPERTY. A NOTICE OF COMMENCEMENT MUST BE POSTED ON THE JOB SITE BEFORE THE FIRST INSPECTION. IF YOU INTEND TO OBTAIN FINANCING, CONSULT WITH YOUR LENDER OR AN ATTORNEY BEFORE RECORDING YOUR NOTICE OF COMMENCEMENT."

NOTICE: All work is to be completed in accordance with the permitted plans and applicable codes of Columbia County, Florida. In order to maintain a valid permit the work authorized must commence within 180 days of issuance and have an approved inspection within every 180 days thereafter.

MUST POST ON THE JOBSITE: Copies of the Permit and Recorded Notice of Commencement for inspection.

2/28/2023 11:41 AM

EXHIBIT

E

**Meeting Minutes
Planning and Zoning**

Date: 07/06/2022

Roll Call:

Mr. Lydick-Present
Mr. Cooper-Present
Mr. Nelson-Present
Ms. Georgalis-Present

Mr. Carter-Present
Mrs. McKellum-Present
Mr. McMahon-Not Present

Approval of Past Minutes-Approve the minutes of the 07/06/2022 Meeting.

Motion By: Mr. Carter

Seconded By: Mr. Lydick

Comments or Revisions:

Move approval tally to after the motion to approve. Fix Mr. McMahon attendance from not present to present.

Old Business: None

New Business:

Petition # SPR22-15 Presented By: Theodore Martell

As owner or agent and gives address of: Kimley Horne of Orlando

Petitioner is Sworn in by: Ms.Georgalis

Discussion:

Robert introduced the project at the request of Ms. Georgalis. Robert stated that the project was to add high flow diesel pumps behind Circle K on Hwy 90 and I-75. Robert stated that all the director and staff were ok with the project at the current time. Robert stated that is met the requirements of the LDR section 4.15.2.1. Theodore presented the project to the board. He stated that the project was to add high flow diesel pumps to the rear of the building.

Motion to close Public Hearing: Mr. Lydick

Motion Seconded By: Mr. Carter

Motion to Approve/Deny By: Mr. Carter

Motion Seconded By: Mr. Nelson

Voted Approved/Denied: Approved unanimously

EXHIBIT

F

CIRCLE K - US HWY
90 & I-75 FUEL
EXPANSION

SITE PLAN



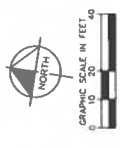
Kimley-Horn
185 S. DAVENPORT BLVD. SUITE 1000, RALEIGH, NC 27601
P: 919.488.8200
WWW.KIMLEY-HORN.COM

NO.	DESCRIPTION
1	DATE
2	BY
3	CHKD BY
4	DATE
5	BY
6	CHKD BY
7	DATE
8	BY
9	CHKD BY
10	DATE

CALL 48 HOURS BEFORE YOU DIG
ITS THE LAW!
DIAL 811
800.487.4848
www.811.com

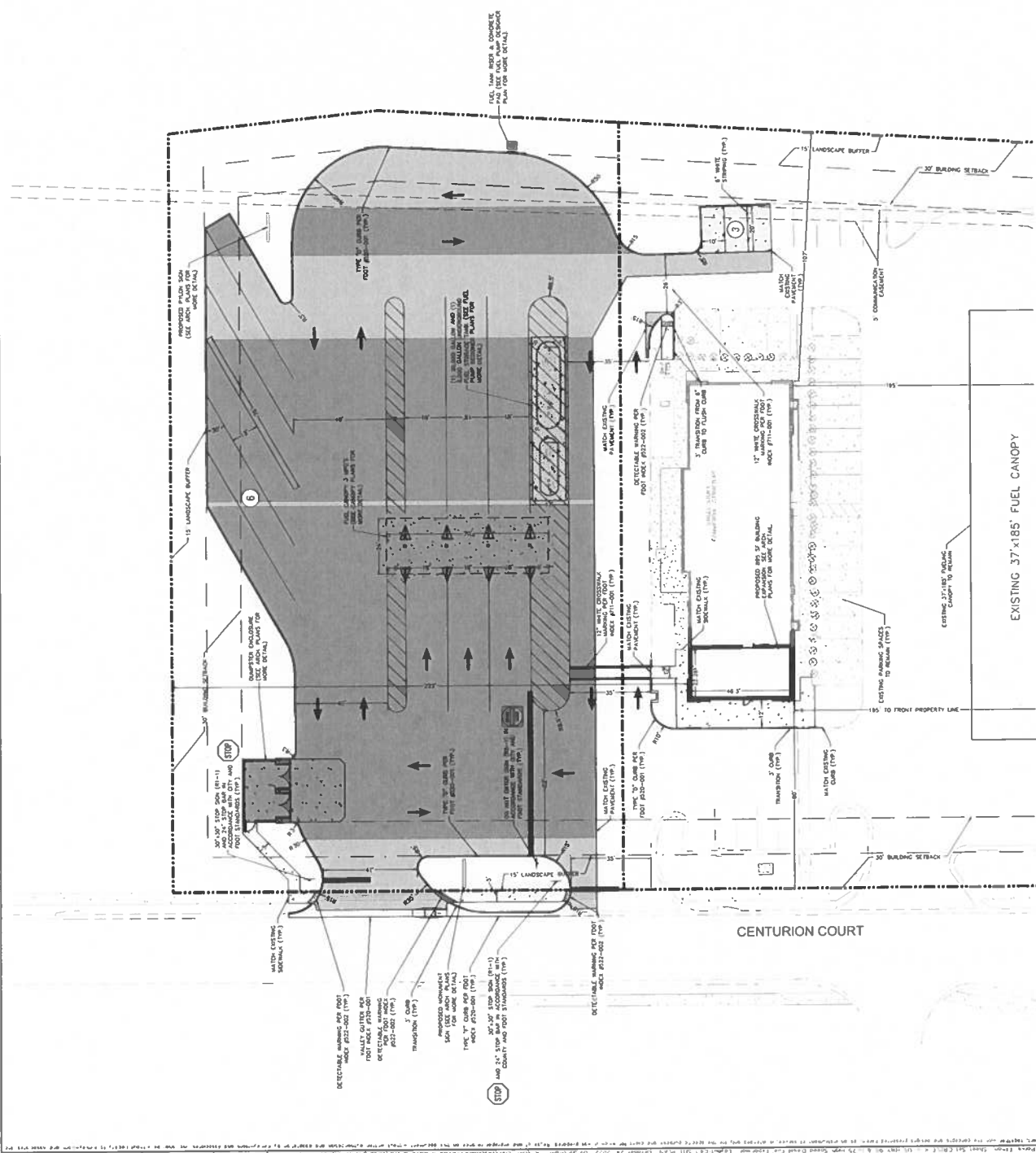
RECORD DRAWING
FOR RECORDING AND
AS-BUILT RECORDING
CITY OF LAKE CHARLES
PLANNING DEPARTMENT
BOOK 9, PAGE 131

- LEGEND**
- PROPOSED CONC. SIDEWALK (SEE DETAIL SHEET C7.9)
 - PROPOSED CONC. SIDEWALK (SEE DETAIL SHEET C7.9)
 - PROPOSED MEDIUM DUTY CONCRETE (SEE DETAIL SHEET C7.9)
 - PROPOSED HEAVY DUTY CONCRETE (SEE DETAIL SHEET C7.9)



- NOTES:**
- ALL CURB DIMENSIONS ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
 - ALL DIMENSIONS ARE TO THE OUTSIDE FACE OF BUILDING UNLESS OTHERWISE NOTED.
 - REFER TO ARCHITECTURAL AND STRUCTURAL PLANS TO VERIFY ALL BUILDING DIMENSIONS.
 - NOTE TO STORAGE PLANS FOR FOUNDATION SOIL DETAILS.
 - SEE MEP PLANS FOR ELECTRICAL DIMENSIONS.
 - ALL DIMENSIONS ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
 - REFER TO ARCHITECTURAL PLANS FOR PROPOSED TRUCK CAN LOCATIONS AND DIMENSIONS.
 - ALL DIMENSIONS ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
 - REFER TO ARCHITECTURAL PLANS FOR THE LIGHTING AND ELECTRICAL DIMENSIONS.
 - ALL DIMENSIONS ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
 - REFER TO ARCHITECTURAL PLANS FOR THE LIGHTING AND ELECTRICAL DIMENSIONS.
 - ALL DIMENSIONS ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
 - REFER TO ARCHITECTURAL PLANS FOR THE LIGHTING AND ELECTRICAL DIMENSIONS.
 - ALL DIMENSIONS ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
 - REFER TO ARCHITECTURAL PLANS FOR THE LIGHTING AND ELECTRICAL DIMENSIONS.
 - ALL DIMENSIONS ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.

INTERSTATE 75 OFF-RAMP



EXHIBIT

G

TRAFFIC IMPACT ANALYSIS

Circle K – US 90 & Centurion Court

Lake City, FL

Prepared for:

Circle K

Prepared by:

Kimley-Horn and Associates, Inc.

October 2023

VINCENT E. SPAHR, P.E.

STATE OF FLORIDA,
PROFESSIONAL ENGINEER,
LICENSE NO. 88747

THIS ITEM HAS BEEN DIGITALLY SIGNED
AND SEALED BY VINCENT E. SPAHR, P.E.
ON THE DATE INDICATED HERE.

PRINTED COPIES OF THIS DOCUMENT ARE
NOT CONSIDERED SIGNED AND SEALED
AND THE SIGNATURE MUST BE VERIFIED
ON ANY ELECTRONIC COPIES.

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K:\ORL_Civil\149880040-Circle K US90 & I75\TPTO\04_Doc\Circle K I75 US90 TIA_2023-10-16.docx

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Appendix B: Traffic Data

Appendix C: Intersection Volume Development Worksheets

Appendix D: Synchro Output Reports

Appendix E: Trip Generation Calculations

Appendix F: FDOT *Trend* Worksheet

1.0 INTRODUCTION

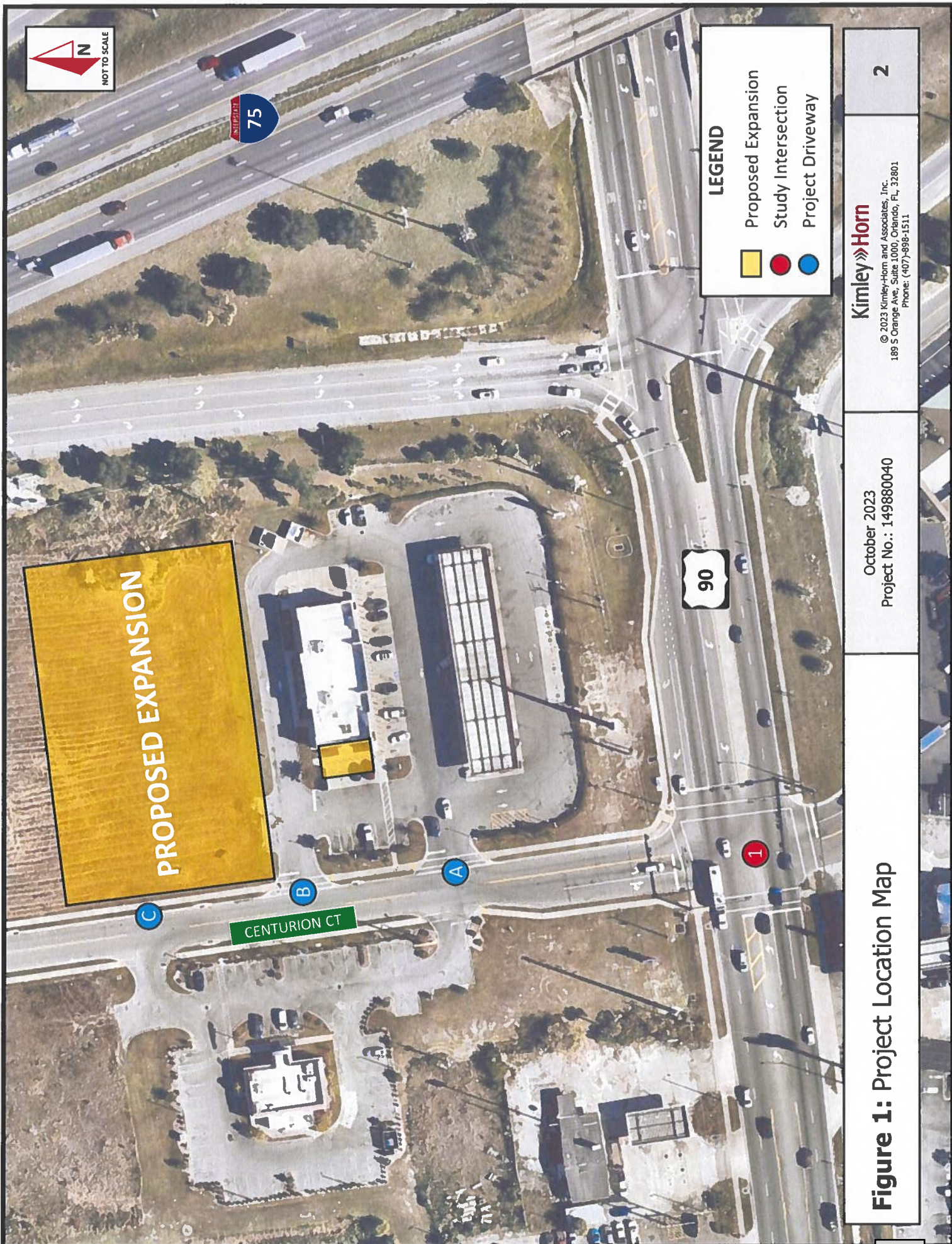
Kimley-Horn has been retained by Circle K to analyze and document the traffic impacts associated with the expansion of a gas station and Circle K convenience market on the northeast quadrant of the intersection of US Highway 90 (US 90) and Centurion Court/SW Florida Gateway Drive in Lake City, Florida.

This Traffic Impact Analysis (TIA) was originally submitted in March 2022 and approved in September 2022. At the request of Lake City staff, the TIA has been updated to reflect existing (2023) conditions and a revised buildout year 2024.

There is an existing 4,968 square foot convenience market with 24 vehicle fueling positions (VFP) on the site. The project location is shown in **Figure 1**.

The applicant is proposing to add a 900 square foot expansion to the convenience market and 3 vehicle fueling positions designed for diesel trucks. The conceptual site plan is provided in **Appendix A**.

The study area for this traffic impact analysis includes the project driveways and the signalized intersection of US 90 and Centurion Court/SW Florida Gateway Drive, as shown in **Figure 1**.



75
AUTOSTRIP

90

LEGEND

- Proposed Expansion
- Study Intersection
- Project Driveway

PROPOSED EXPANSION

CENTURION CT

<p>Kimley»Horn © 2023 Kimley-Horn and Associates, Inc. 189 S Orange Ave. Suite 1000, Orlando, FL, 32801 Phone: (407)898-1511</p>	<p>October 2023 Project No.: 149880040</p>	<p>Figure 1: Project Location Map</p>
--	---	--

2.0 EXISTING CONDITIONS ANALYSIS

2.1 EXISTING TRAFFIC DATA

Turning movement counts (TMCs) were collected at the study intersection on Thursday, October 5, 2023, during the AM (7:00 AM – 9:00 AM) and PM (4:00 PM – 6:00 PM) peak periods. Raw turning movement counts are provided in **Appendix B**.

Turning movement volumes were adjusted using the peak season conversion factor (PSCF) from the Florida Department of Transportation (FDOT) Florida Traffic Online (FTO). Seasonal factor data is included in **Appendix B**. Existing signal timings were provided by Lake City staff for use in the analysis. Signal timing worksheets are included in **Appendix B**.

Figure 2 illustrates turning movement volumes for existing peak season conditions at the study intersection. The intersection volume development worksheet can be found in **Appendix C**.

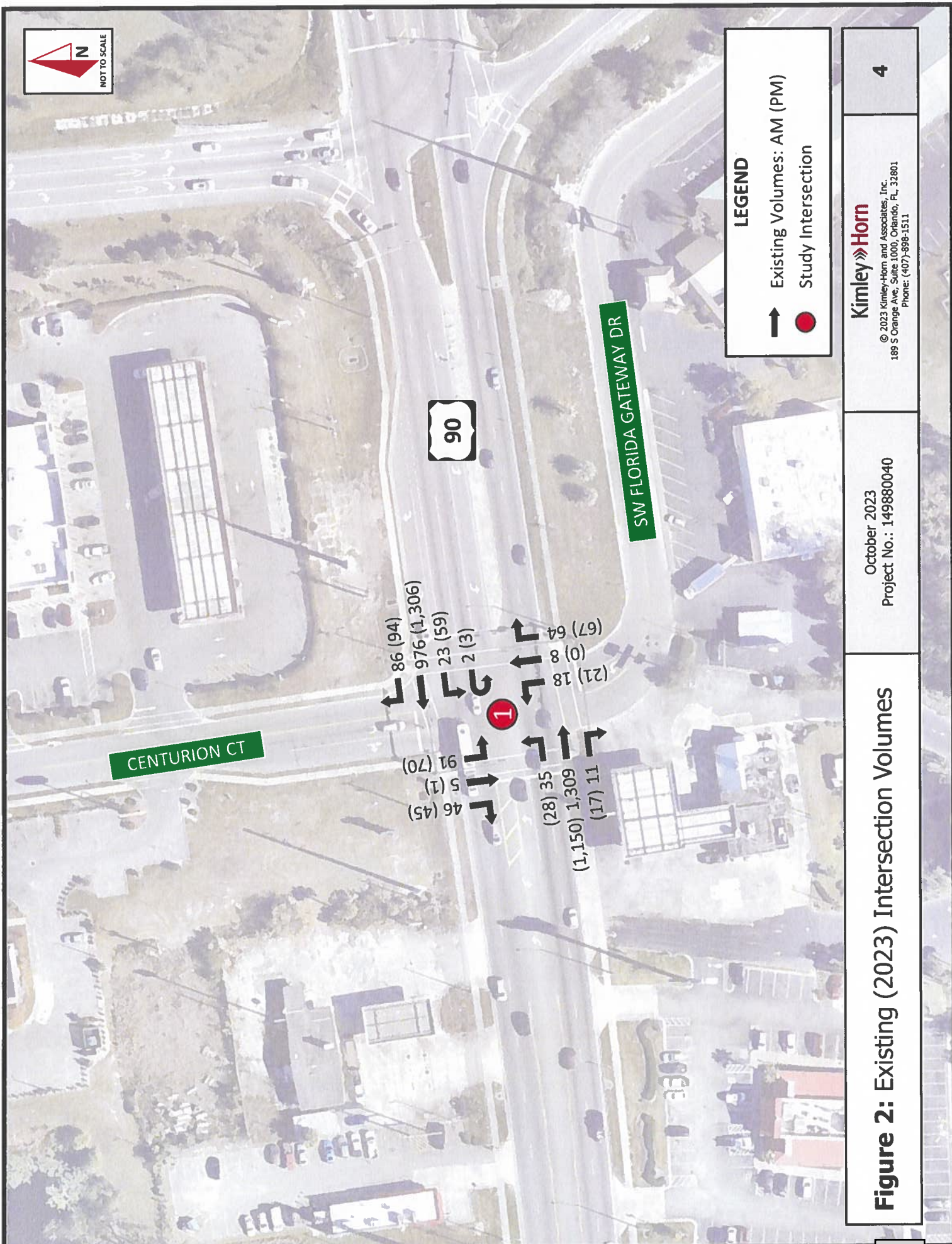
2.2 EXISTING INTERSECTION CONDITIONS

Intersection capacity analyses were performed for existing (2023) conditions using the operational analysis procedures outlined in the latest *Highway Capacity Manual, 6th Edition* (HCM 6). Specifically, *Synchro* (v11) software was used to evaluate existing operational conditions at the study area intersection by reporting delay, level of service (LOS), volume-to-capacity (v/c) ratios, and the 95th percentile queue for each movement. **Table 1** summarizes the operational analyses for the existing AM and PM peak hour conditions at the study intersection. Synchro outputs are provided in **Appendix D**.

Table 1: Existing Intersection Conditions

		AM Peak Hour				PM Peak Hour			
		Delay (sec/veh)	LOS	v/c Ratio	95th percentile queue (veh)	Delay (sec/veh)	LOS	v/c Ratio	95th percentile queue (veh)
US 90 & Centurion Court	Overall Intersection	14.4	B	-	-	10.6	B	-	-
	Eastbound	13.1	B	-	-	8.0	A	-	-
	EBL	6.8	A	0.11	0.5	5.6	A	0.1	0.3
	EBT	13.3	B	0.61	16.1	8.1	A	0.45	10.9
	EBT/R	13.2	B	0.61	16.7	8.0	A	0.45	11.3
	Westbound	9.3	A	-	-	7.4	A	-	-
	WBL	8.8	A	0.10	0.4	5.2	A	0.17	0.7
	WBT	9.5	A	0.46	10.5	7.6	A	0.51	11.9
	WBR	6.5	A	0.07	1.0	4.6	A	0.07	1.1
	Northbound	52.1	D	-	-	65.4	E	-	-
	NBL	51.9	D	0.10	1.1	66.0	E	0.15	1.5
	NBT/R	52.2	D	0.27	2.9	64.8	E	0.2	1.8
	Southbound	59.8	E	-	-	70.3	E	-	-
	SBL	61.4	E	0.58	6.4	71.8	E	0.52	5.3
	SBT/R	50.5	D	0.09	1.0	64.2	E	0.14	1.3

The intersection of US 90 and Centurion Court operates with LOS B during existing (2023) AM peak hour and PM peak hour conditions. All movements operate with v/c ratios less than 1.00 under existing (2023) AM and PM peak hour conditions. The northbound approach operates with LOS D during the AM peak hour and LOS E during the PM peak hour. The southbound approach operates with LOS E during the AM and PM peak hours. The higher delay on the northbound and southbound approaches is due to the prioritization of green time for the mainline US 90 movements.



LEGEND

↑ Existing Volumes: AM (PM)

● Study Intersection

Kimley»Horn

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Phone: (407) 898-1511

October 2023
Project No.: 149880040

Figure 2: Existing (2023) Intersection Volumes

4

3.0 PROJECT DEVELOPMENT

The existing site currently has 24 VFPs and a 4,968 square foot Circle K convenience store. The proposed expansion will add approximately 900 square feet to the existing convenience market and 3 VFPs north of the existing site. The latest industry standards were referenced to evaluate the amount of new external trips to be generated by the site at buildout.

3.1 SITE ACCESS

Access to the site is proposed via two existing driveways and one new driveway along Centurion Court, as shown in the site plan provided in **Appendix A**.

3.2 TRIP GENERATION

Trip generation and pass-by rates for the proposed development were calculated using the 11th Edition of the Institute of Transportation Engineers' (ITE) *Trip Generation Manual*. Land Use Code (LUC) 945 (Gas Station with Convenience Market) was used to calculate the trip generation potential for the existing and proposed development.

The trip generation potential of the existing Circle K convenience store and gas station was compared to observed traffic volumes on Centurion Court north of US 90 in the reviewed and approved TIA dated March 2022. **Table 2** summarizes the comparison of the calculated trip generation potential of the existing development and the observed peak hour volumes on Centurion Court.

Table 2: Existing Site Trip Generation Comparison

	AM Peak Hour			PM Peak Hour		
	Total	In (NB)	Out (SB)	Total	In (NB)	Out (SB)
ITE <i>Trip Generation Manual</i>	649	325	324	546	273	273
Observed Peak Season Traffic	201	106	95	220	115	105

Since the existing AM and PM peak hour traffic volumes were significantly less than the trip generation potential of the existing development, the trip generation calculations for the proposed expansion to the convenience store and gas station were adjusted proportionately to reflect actual conditions anticipated at the site under buildout conditions.

Table 3 provides the AM peak hour and PM peak hour trip generation calculations for the proposed expansion and the adjustment applied based on the existing trip generation comparison. A factor of 0.31 (201/649) was applied to the AM peak hour trip generation calculations, and a factor of 0.40 (220/546) was applied to the PM peak hour trip generation calculations in accordance with the comparison illustrated in **Table 2**.

As summarized in **Table 3**, the proposed expansion is anticipated to generate 16 net new AM peak hour trips (8 inbound and 8 outbound) and 18 net new PM peak hour trips (9 inbound and 9 outbound) to the external roadway network at buildout. In addition, the proposed expansion is anticipated to generate 48 AM peak hour pass-by trips (24 inbound and 24 outbound) and 54 PM peak hour pass-by trips (27 inbound and 27 outbound). A detailed table, including all trip generation calculations and adjustments, is provided in **Appendix E**.

Table 3: Trip Generation Summary

	AM Peak Hour			PM Peak Hour		
	Total	In (NB)	Out (SB)	Total	In (NB)	Out (SB)
ITE <i>Trip Generation Manual</i> (Net New)	50	25	25	46	23	23
ITE <i>Trip Generation Manual</i> (Pass-by)	204	102	102	180	90	90
Adjustment Factor	0.31			0.40		
Adjusted Net New Trips	16	8	8	18	9	9
Adjusted Pass-by Trips	48	24	24	54	27	27

3.3 TRIP DISTRIBUTION

The project’s trip distribution was developed based on observed traffic patterns within the study area roadway network and engineering judgment. **Figure 3** displays the anticipated trip distribution for the proposed Circle K gas station expansion at buildout.

3.4 TRIP ASSIGNMENT

Site distribution percentages were used to assign anticipated project trips to the study area intersection and driveways. **Figure 4** shows the anticipated AM and PM peak hour project movements at the study area intersection and project driveways.

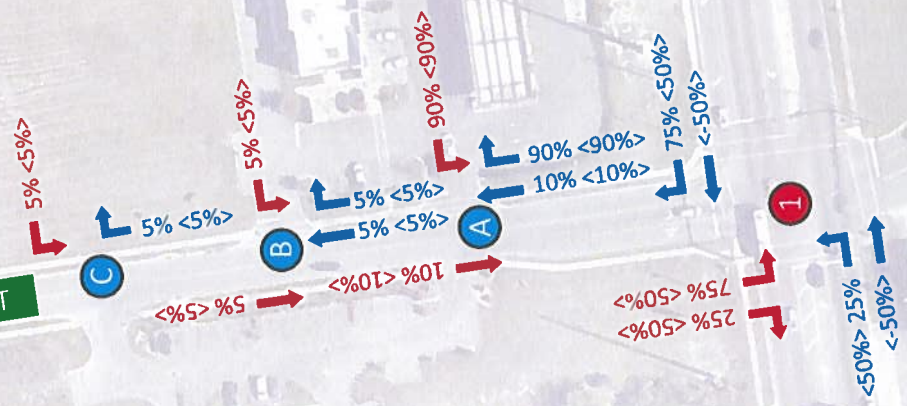


INTERSTATE
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SW FLORIDA GATEWAY DR

CENTURION CT



LEGEND

- ↑ % Trips In <% Pass-By In>
- ↑ % Trips Out <% Pass-By Out>
- # Study Intersection
- # Project Driveway

<p>October 2023 Project No.: 149880040</p>	<p>Kimley»Horn © 2023 Kimley-Horn and Associates, Inc. 189 S Orange Ave., Suite 1000, Orlando, FL, 32801 Phone: (407) 898-1511</p>	<p>7</p>
--	---	----------

Figure 3: Project Trip Distribution



LEGEND

- ➔ Project Trips In: Net New + *Pass-By*: AM (PM)
- ➔ Project Trips Out: Net New + *Pass-By*: AM (PM)
- # Study Intersection
- # Project Driveway

CENTURION CT

SW FLORIDA GATEWAY DR

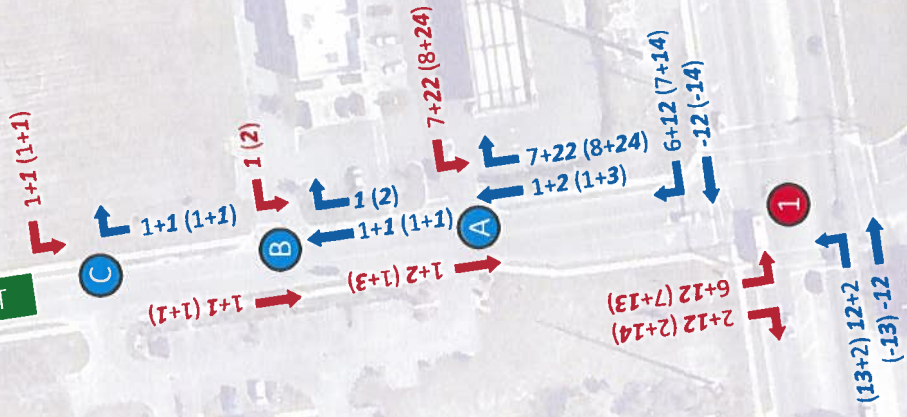


Figure 4: Project Trip Assignment

October 2023
Project No.: 149880040

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Phone: (407)898-1511

4.0 BACKGROUND CONDITIONS ANALYSIS – YEAR 2024

4.1 HISTORICAL TRAFFIC GROWTH

A historical traffic growth rate was calculated based upon the nearest historical Annual Average Daily Traffic (AADT) data available from FTO. A 3.61% annual historical growth rate was calculated based on the average traffic growth exhibited over the past five (5) years from an FDOT count station located east of the project site on US 90; 2020 and 2021 AADT data were removed from the calculation due to the COVID-19 pandemic effect on travel patterns. The growth trend worksheet can be found in **Appendix F**.

4.2 BACKGROUND TRAFFIC

Traffic conditions were evaluated for the year 2024 background conditions prior to the addition of project traffic. Background volumes at the study area intersection were derived by applying 3.61% annual growth to existing (2023) traffic counts. **Figure 5** illustrates AM peak hour and PM peak hour turning movement volumes for background conditions at the study intersection. The intersection volume development worksheet can be found in **Appendix C**.

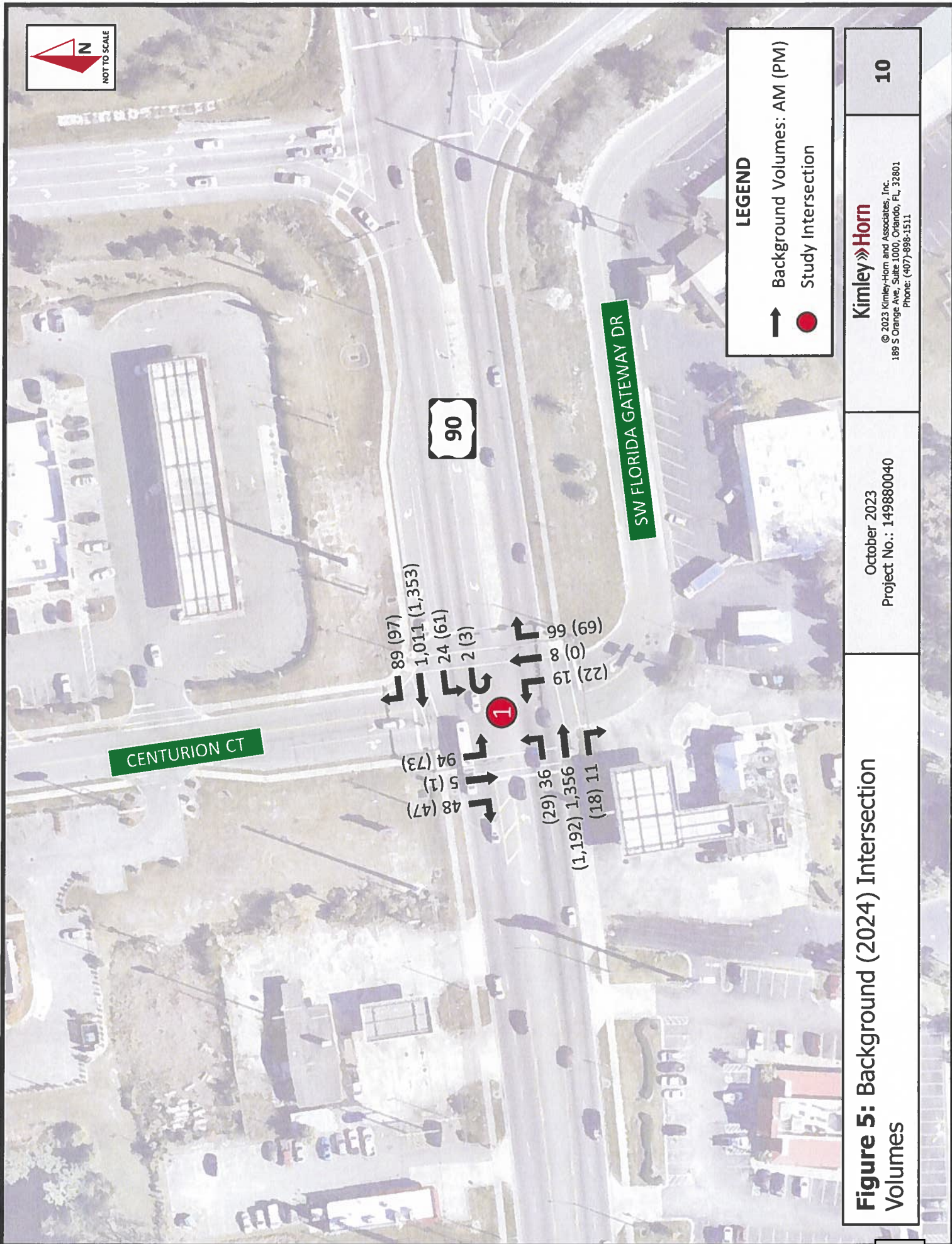
4.3 BACKGROUND INTERSECTION ANALYSIS

Intersection operational analyses were performed for 2024 background conditions in the AM and PM peak hours using procedures outlined in the *Highway Capacity Manual 6* with *Synchro* (v11) software. **Table 4** summarizes the operational analyses for the 2024 background AM and PM peak hour conditions at the study intersection. Synchro outputs are provided in **Appendix D**.

Table 4: Background Intersection Conditions

		AM Peak Hour				PM Peak Hour			
		Delay (sec/veh)	LOS	v/c Ratio	95th percentile queue (veh)	Delay (sec/veh)	LOS	v/c Ratio	95th percentile queue (veh)
US 90 & Centurion Court	Overall Intersection	15.0	B	-	-	11.1	B	-	-
	Eastbound	13.9	B	-	-	8.5	A	-	-
	EBL	7.1	A	0.12	0.5	6.1	A	0.11	0.4
	EBT	14.1	B	0.63	17.1	8.6	A	0.47	11.6
	EBT/R	14.0	B	0.63	17.8	8.5	A	0.47	12.0
	Westbound	9.7	A	-	-	7.9	A	-	-
	WBL	9.6	A	0.11	0.4	5.7	A	0.18	0.7
	WBT	9.9	A	0.48	11.1	8.1	A	0.53	12.8
	WBR	6.6	A	0.07	1.1	4.8	A	0.07	1.1
	Northbound	51.8	D	-	-	64.9	E	-	-
	NBL	51.7	D	0.11	1.2	65.6	E	0.15	1.5
	NBT/R	51.9	D	0.28	3.0	64.3	E	0.21	1.9
	Southbound	59.9	E	-	-	70.0	E	-	-
	SBL	61.8	E	0.59	6.7	71.7	E	0.54	5.6
SBT/R	50.2	D	0.1	1.1	63.7	E	0.15	1.4	

The intersection of US 90 and Centurion Court is expected to operate with LOS B during background (2024) AM peak hour and PM peak hour conditions. All movements are expected to operate with v/c ratios less than 1.00 under background (2024) AM and PM peak hour conditions. The northbound and southbound approaches are expected to continue to operate with LOS E or better during the AM and PM peak hours due to the prioritization of green time for the mainline US 90 movements.



LEGEND

- ↑ Background Volumes: AM (PM)
- Study Intersection

Figure 5: Background (2024) Intersection Volumes

October 2023
Project No.: 149880040

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5.0 BUILDOUT CONDITIONS ANALYSIS – YEAR 2024

5.1 BUILDOUT TRAFFIC

Future traffic conditions for the proposed development were evaluated for the year 2024 conditions with the inclusion of project traffic. Buildout volumes were developed by adding anticipated project trips to background (2024) volumes. **Figure 6** illustrates the projected turning movement volumes under buildout AM and PM peak hour conditions at the study intersection. The intersection volume development worksheet can be found in **Appendix C**.

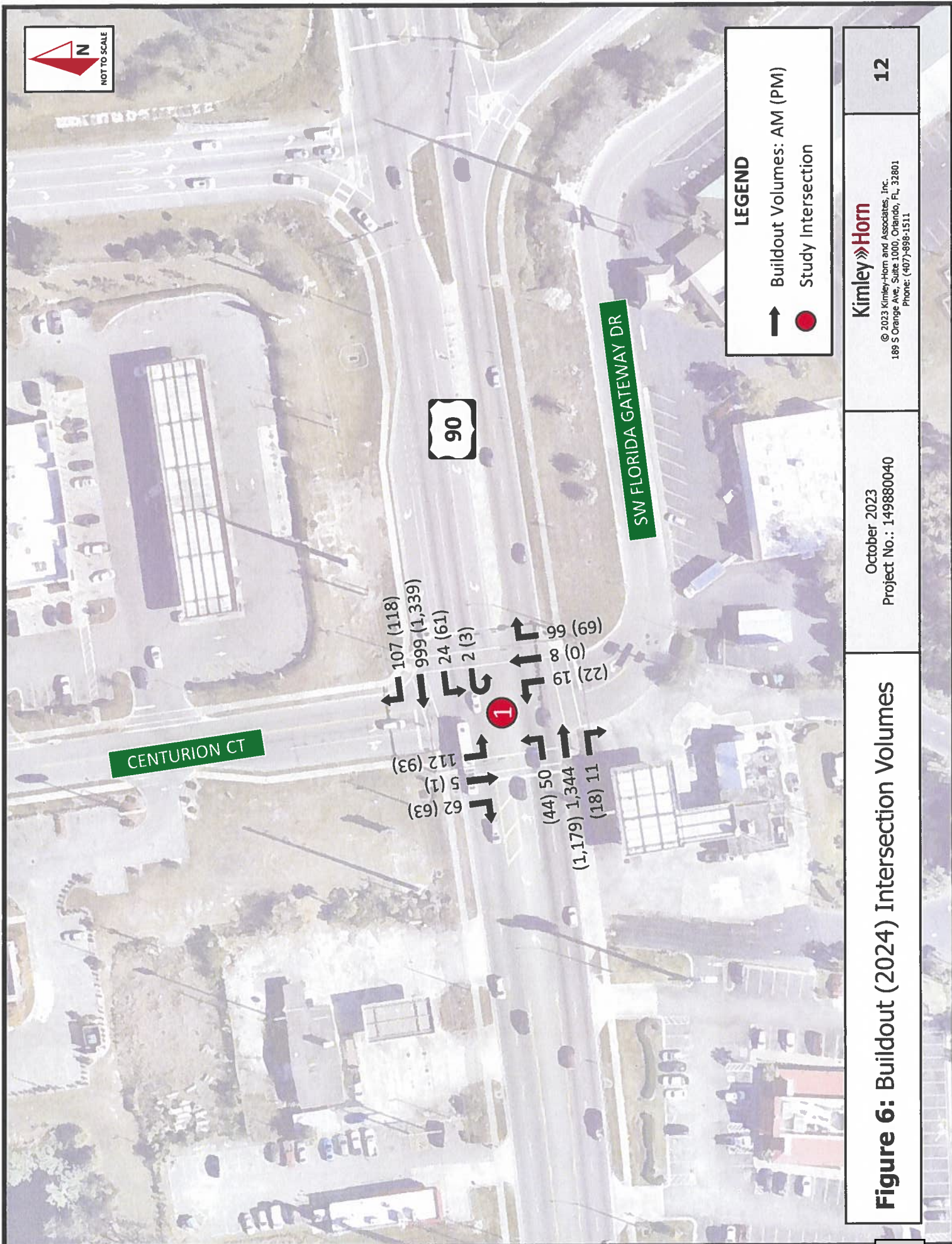
5.2 BUILDOUT INTERSECTION ANALYSIS

Intersection operational analyses were performed for 2024 buildout conditions in the AM and PM peak hour conditions using procedures outlined in the *Highway Capacity Manual 6* with *Synchro* (v11) software. **Table 5** summarizes the operational analyses for the 2024 buildout AM and PM peak hour conditions at the study intersection. Synchro outputs are provided in **Appendix D**.

Table 5: Buildout Intersection Conditions

		AM Peak Hour				PM Peak Hour			
		Delay (sec/veh)	LOS	v/c Ratio	95th percentile queue (veh)	Delay (sec/veh)	LOS	v/c Ratio	95th percentile queue (veh)
US 90 & Centurion Court	Overall Intersection	16.3	B	-	-	12.7	B	-	-
	Eastbound	14.6	B	-	-	9.4	A	-	-
	EBL	7.8	A	0.16	0.8	7.2	A	0.16	0.6
	EBT	14.9	B	0.64	17.6	9.5	A	0.48	12.3
	EBT/R	14.8	B	0.64	18.3	9.5	A	0.48	12.7
	Westbound	10.5	B	-	-	9.0	A	-	-
	WBL	10.1	B	0.11	0.4	6.4	A	0.19	0.8
	WBT	10.8	B	0.48	11.5	9.4	A	0.54	14.0
	WBR	7.4	A	0.09	1.6	5.7	A	0.10	1.7
	Northbound	50.8	D	-	-	63.2	E	-	-
	NBL	51.5	D	0.11	1.2	64.7	E	0.14	1.5
	NBT/R	50.5	D	0.25	2.9	61.9	E	0.18	1.9
	Southbound	61.3	E	-	-	68.4	E	-	-
	SBL	64.7	E	0.65	8.1	70.6	E	0.59	7.0
SBT/R	49.6	D	0.17	2.0	62.4	E	0.23	2.5	

The intersection of US 90 and Centurion Court is expected to operate with LOS B during buildout (2024) AM peak hour and PM peak hour conditions. All movements are expected to operate with v/c ratios less than 1.00 under buildout (2024) AM and PM peak hour conditions. The northbound and southbound approaches are expected to continue to operate with LOS E or better during the AM and PM peak hour due to the prioritization of green time for the mainline US 90 movements.



LEGEND

↑ Buildout Volumes: AM (PM)

● Study Intersection

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Figure 6: Buildout (2024) Intersection Volumes		

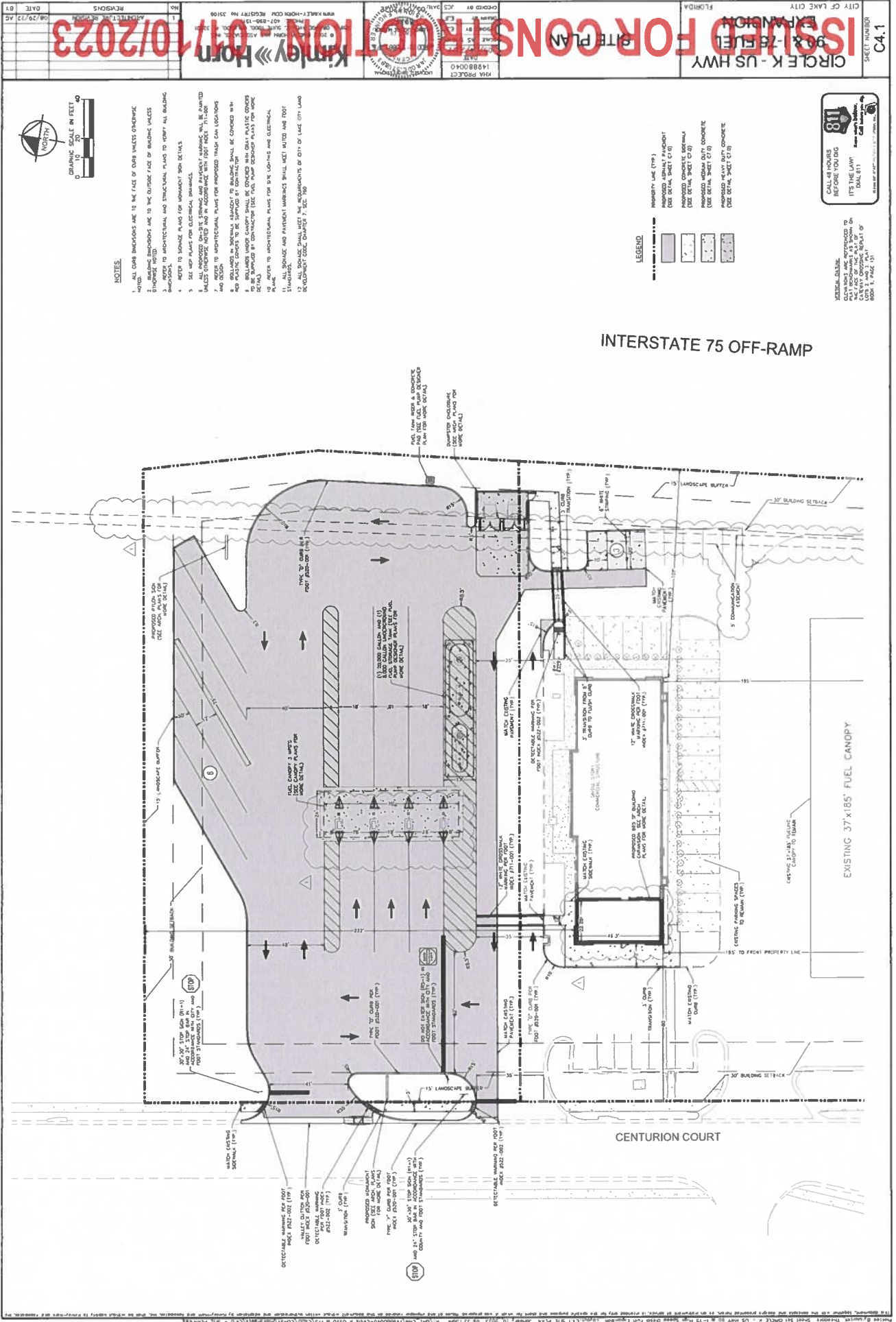
6.0 CONCLUSION

This traffic impact analysis was performed to assess the transportation impacts of the proposed expansion of a gas station and Circle K convenience market located in the northwest quadrant of the intersection of US Highway 90 (SR 10) and Centurion Court/SW Florida Gateway Drive. The expansion, proposed for buildout in year 2024, will include the addition of 3 vehicle fueling positions designed for diesel trucks and a 900 square foot expansion to the existing Circle K convenience market. Access to the site will be provided via two existing driveways and one new driveway to the north on Centurion Court.

Accounting for the observed trip generation of the existing site, the proposed expansion is anticipated to generate 16 net new AM peak hour trips and 18 net new PM peak hour trips at buildout. An additional 48 new AM peak hour pass-by trips and 54 new PM peak hour pass-by trips are expected at the site as well.

Operational analyses were performed utilizing *Synchro* software for the existing (2023), background (2024), and buildout (2024) conditions at the study intersection of US 90 and Centurion Court/SW Florida Gateway Drive during the AM peak hour and the PM peak hour. Results indicated that the study intersection is expected to operate at LOS B through the buildout year. No operational deficiencies are expected at the study intersection with the inclusion of project traffic under buildout (2024) conditions.

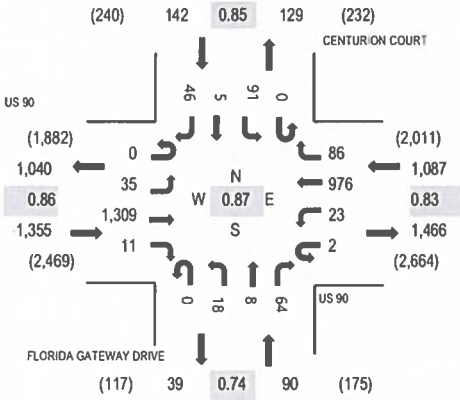
APPENDIX A
Conceptual Site Plan



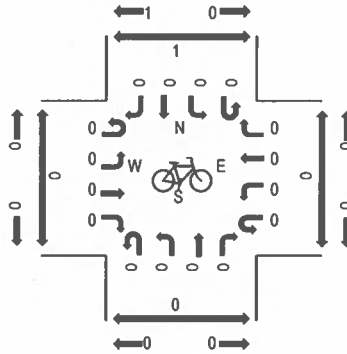
APPENDIX B

Traffic Data

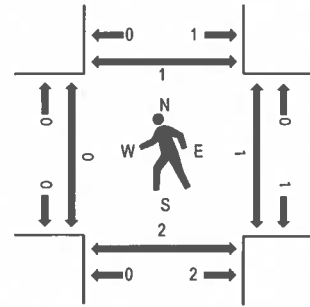
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	US 90 Eastbound				US 90 Westbound				FLORIDA GATEWAY DRIVE Northbound				CENTURION COURT Southbound				Rolling Hour Total	Pedestrian Crossings				
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right		West	East	South	North	
7:00 AM	0	10	272	1	0	6	143	20	0	3	3	19	0	17	2	5	501	2,567	0	0	1	1
7:15 AM	0	8	307	1	1	5	191	27	0	5	3	11	0	30	1	11	601	2,674	0	0	0	0
7:30 AM	0	12	380	0	0	2	234	22	0	3	2	10	0	18	0	12	695	2,668	0	1	0	1
7:45 AM	0	6	353	5	1	11	306	22	0	6	3	24	0	20	4	9	770	2,530	0	0	0	0
8:00 AM	0	9	269	5	0	5	245	15	0	4	0	19	0	23	0	14	608	2,328	0	0	2	0
8:15 AM	0	5	274	3	0	14	235	14	0	5	0	15	0	17	2	11	595		0	0	1	0
8:30 AM	0	6	255	1	0	21	210	16	0	4	1	17	0	14	2	10	557		0	0	0	0
8:45 AM	0	6	271	10	1	15	208	21	0	3	1	14	0	12	1	5	568		0	0	0	0

Peak Rolling Hour Flow Rates

Vehicle Type	Eastbound				Westbound				Northbound				Southbound				Total
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	
Articulated Trucks	0	0	8	0	0	0	9	0	0	0	0	0	0	2	0	0	19
Lights	0	34	1,281	11	2	22	944	83	0	15	6	60	0	87	5	43	2,593
Mediums	0	1	20	0	0	1	23	3	0	3	2	4	0	2	0	3	62
Total	0	35	1,309	11	2	23	976	86	0	18	8	64	0	91	5	46	2,674

Heavy Vehicle Percentage and Peak Hour Factor

	Eastbound				Westbound				Northbound				Southbound				Total
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	
Heavy Vehicle %																	
Heavy Vehicle %	0.0%	2.9%	2.1%	0.0%	0.0%	4.3%	3.3%	4.9%	0.0%	16.7%	25.0%	5.4%	0.0%	4.4%	0.0%	0.0%	3.0%
Peak Hour Factor		0.86				0.83				0.74				0.85			0.87
Peak Hour Factor	0.00	0.75	0.86	0.48	0.50	0.65	0.83	0.85	0.00	0.79	0.92	0.70	0.00	0.76	0.50	0.75	0.87

All Traffic Data Services

1. FLORIDA GATEWAY DRIVE & US 90 AM
Thursday, October 5, 2023

Peak Hour
07:15 AM - 08:15 AM
Peak 15-Minutes
07:45 AM - 08:00 AM

Traffic Counts - All Vehicles

Time	US 90 Eastbound					US 90 Westbound					FLORIDA GATEWAY DRIVE Northbound					CENTURION COURT Southbound					Total	Rolling Hour	
	U-Turn	Left	Thru	Right	RTOR	U-Turn	Left	Thru	Right	RTOR	U-Turn	Left	Thru	Right	RTOR	U-Turn	Left	Thru	Right	RTOR			
	7:00 AM	0	10	272	1	0	0	0	6	143	11	9	0	3	3	10	9	0	17	2			2
7:15 AM	0	8	307	1	0	1	5	191	15	12	0	5	3	7	4	0	30	1	3	8	601	2,674	
7:30 AM	0	12	380	0	0	0	2	234	14	8	0	3	2	8	2	0	18	0	3	9	695	2,888	
7:45 AM	0	6	353	5	0	1	11	306	18	4	0	6	3	14	10	0	20	4	4	5	770	2,530	
8:00 AM	0	9	269	5	0	0	5	245	14	1	0	4	0	8	11	0	23	0	1	13	608	2,328	
8:15 AM	0	5	274	3	0	0	14	235	9	5	0	5	0	8	7	0	17	2	3	8	595	0	
8:30 AM	0	6	255	1	0	0	21	210	12	4	0	4	1	6	11	0	14	2	3	7	557	0	
8:45 AM	0	6	271	10	0	1	15	208	19	2	0	3	1	3	11	0	12	1	0	5	568	0	

Peak Rolling Hour Flow Rates

Vehicle Type	Eastbound					Westbound					Northbound					Southbound					Total	
	U-Turn	Left	Thru	Right	RTOR	U-Turn	Left	Thru	Right	RTOR	U-Turn	Left	Thru	Right	RTOR	U-Turn	Left	Thru	Right	RTOR		
Articulated Trucks	0	0	8	0	0	0	0	9	0	0	0	0	0	0	0	0	2	0	0	0	0	0
Lights	0	34	1,281	11	0	2	22	944	58	25	0	15	6	35	25	0	87	5	11	32	2,593	3.0%
Mediums	0	1	20	0	0	0	1	23	3	0	0	3	2	2	2	0	2	0	0	3	62	0.87
Total	0	35	1,309	11	0	2	23	976	61	25	0	18	8	37	27	0	91	5	11	35	2,674	3.2%
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Bicycles on Crosswalk																						
Heavy Vehicle Percentage			2.1%					3.3%					10.0%					4.9%				3.0%
Peak Hour Factor (PHF)	0.00	0.75	0.86	0.48	0.00	0.50	0.65	0.83	0.85	0.69	0.00	0.79	0.92	0.70	0.91	0.00	0.76	0.50	0.75	0.67	0.87	0.87

Traffic Counts by Vehicle Type

Time	Eastbound					Westbound					Northbound					Southbound					Total	
	U-Turn	Left	Thru	Right	RTOR	U-Turn	Left	Thru	Right	RTOR	U-Turn	Left	Thru	Right	RTOR	U-Turn	Left	Thru	Right	RTOR		
Articulated Trucks	0	0	2	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Lights	0	9	262	1	0	0	5	137	11	8	0	1	2	10	9	0	14	2	2	3	476	3.0%
Mediums	0	1	8	0	0	0	1	5	0	1	0	2	1	0	0	0	3	0	0	0	22	0.28
Bicycles on Road	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Bicycles on Crosswalk																						

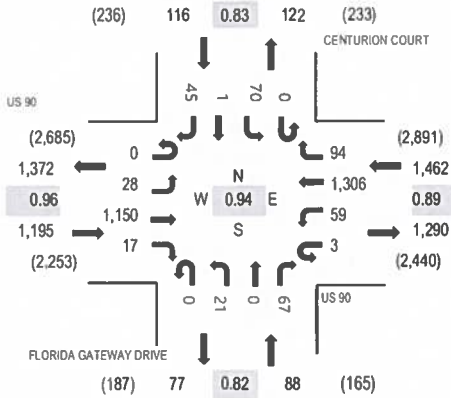
Bicycles on Crosswalk

Time	Eastbound			Westbound			Northbound			Southbound		
	CCW	CW	Total	CCW	CW	Total	CCW	CW	Total	CCW	CW	Total
7:00 AM	0	0	0	0	0	0	0	0	0	0	1	1
7:15 AM	0	0	0	0	0	0	0	0	0	0	1	1
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	0
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	0
8:15 AM	0	0	0	0	0	0	0	0	0	0	0	0
8:30 AM	0	0	0	1	0	1	0	0	0	0	0	0
8:45 AM	0	0	0	0	0	0	0	0	0	0	0	0

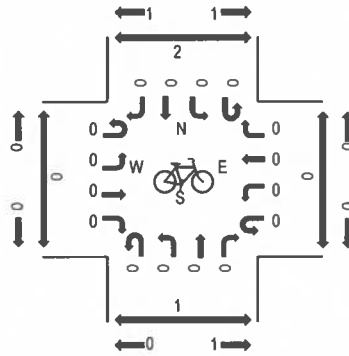
Pedestrians

Time	Eastbound			Westbound			Northbound			Southbound		
	CCW	CW	Total	CCW	CW	Total	CCW	CW	Total	CCW	CW	Total
7:00 AM	0	0	0	0	0	0	1	0	1	0	1	1
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0
7:30 AM	0	0	0	1	1	2	0	0	0	1	1	2
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	0
8:00 AM	0	0	0	0	0	0	2	0	2	0	0	0
8:15 AM	0	0	0	0	0	0	1	0	1	0	0	0
8:30 AM	0	0	0	0	0	0	0	0	0	0	0	0
8:45 AM	0	0	0	0	0	0	0	0	0	0	0	0

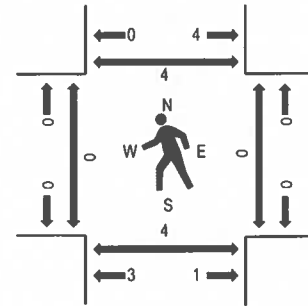
Peak Hour - Motorized Vehicles



Peak Hour - Bicycles



Peak Hour - Pedestrians



Note: Total study counts contained in parentheses.

Traffic Counts - Motorized Vehicles

Interval Start Time	US 90 Eastbound				US 90 Westbound				FLORIDA GATEWAY DRIVE Northbound				CENTURION COURT Southbound				Total	Rolling Hour	Pedestrian Crossings			
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right			West	East	South	North
4:00 PM	2	6	247	7	3	19	347	22	0	9	1	16	0	27	1	8	715	2,808	1	0	1	1
4:15 PM	0	14	251	10	1	30	366	24	0	1	0	12	0	19	3	7	738	2,850	0	0	0	0
4:30 PM	0	8	278	6	1	14	306	25	0	6	0	10	0	21	0	12	687	2,861	0	0	0	1
4:45 PM	0	7	270	5	0	17	307	17	0	8	0	15	0	12	1	9	668	2,831	0	0	0	0
5:00 PM	0	4	306	1	0	11	351	25	0	4	0	23	0	20	0	12	757	2,737	0	0	4	0
5:15 PM	0	9	296	5	2	17	342	27	0	3	0	19	0	17	0	12	749		0	0	0	3
5:30 PM	0	3	261	1	2	19	301	18	0	2	1	14	0	17	3	15	657		0	0	0	0
5:45 PM	0	4	247	5	5	10	245	17	0	4	1	16	0	12	2	6	574		0	1	0	2

Peak Rolling Hour Flow Rates

Vehicle Type	Eastbound				Westbound				Northbound				Southbound				Total
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	
Articulated Trucks	0	0	9	0	0	0	8	0	0	0	0	1	0	0	0	0	18
Lights	0	28	1,126	17	3	59	1,279	92	0	21	0	66	0	70	1	45	2,807
Mediums	0	0	15	0	0	0	19	2	0	0	0	0	0	0	0	0	36
Total	0	28	1,150	17	3	59	1,306	94	0	21	0	67	0	70	1	45	2,861

Heavy Vehicle Percentage and Peak Hour Factor

	Eastbound				Westbound				Northbound				Southbound				Total
	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	U-Turn	Left	Thru	Right	
Heavy Vehicle %																	1.9%
Heavy Vehicle %	0.0%	0.0%	2.1%	0.0%	0.0%	0.0%	2.1%	2.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	1.9%
Peak Hour Factor																	0.94
Peak Hour Factor	0.25	0.63	0.94	0.70	0.45	0.67	0.91	0.90	0.00	0.67	0.50	0.78	0.00	0.73	0.42	0.63	0.94

2022 PEAK SEASON FACTOR CATEGORY REPORT - REPORT TYPE: ALL
 CATEGORY: 2900 COLUMBIA COUNTYWIDE

MOCF: 0.98
 PSCF

WEEK	DATES	SF	PSCF
1	01/01/2022 - 01/01/2022	1.02	1.04
2	01/02/2022 - 01/08/2022	1.05	1.07
3	01/09/2022 - 01/15/2022	1.08	1.10
4	01/16/2022 - 01/22/2022	1.07	1.09
5	01/23/2022 - 01/29/2022	1.05	1.07
6	01/30/2022 - 02/05/2022	1.03	1.05
7	02/06/2022 - 02/12/2022	1.02	1.04
8	02/13/2022 - 02/19/2022	1.00	1.02
9	02/20/2022 - 02/26/2022	1.00	1.02
10	02/27/2022 - 03/05/2022	0.99	1.01
11	03/06/2022 - 03/12/2022	0.99	1.01
*12	03/13/2022 - 03/19/2022	0.98	1.00
*13	03/20/2022 - 03/26/2022	0.98	1.00
*14	03/27/2022 - 04/02/2022	0.98	1.00
*15	04/03/2022 - 04/09/2022	0.97	0.99
*16	04/10/2022 - 04/16/2022	0.97	0.99
*17	04/17/2022 - 04/23/2022	0.97	0.99
*18	04/24/2022 - 04/30/2022	0.97	0.99
*19	05/01/2022 - 05/07/2022	0.97	0.99
*20	05/08/2022 - 05/14/2022	0.97	0.99
*21	05/15/2022 - 05/21/2022	0.98	1.00
*22	05/22/2022 - 05/28/2022	0.98	1.00
*23	05/29/2022 - 06/04/2022	0.99	1.01
*24	06/05/2022 - 06/11/2022	0.99	1.01
25	06/12/2022 - 06/18/2022	1.00	1.02
26	06/19/2022 - 06/25/2022	1.00	1.02
27	06/26/2022 - 07/02/2022	1.01	1.03
28	07/03/2022 - 07/09/2022	1.02	1.04
29	07/10/2022 - 07/16/2022	1.03	1.05
30	07/17/2022 - 07/23/2022	1.02	1.04
31	07/24/2022 - 07/30/2022	1.01	1.03
32	07/31/2022 - 08/06/2022	1.01	1.03
33	08/07/2022 - 08/13/2022	1.00	1.02
34	08/14/2022 - 08/20/2022	0.99	1.01
35	08/21/2022 - 08/27/2022	1.00	1.02
36	08/28/2022 - 09/03/2022	1.00	1.02
37	09/04/2022 - 09/10/2022	1.01	1.03
38	09/11/2022 - 09/17/2022	1.01	1.03
39	09/18/2022 - 09/24/2022	1.00	1.02
40	09/25/2022 - 10/01/2022	0.99	1.01
41	10/02/2022 - 10/08/2022	0.98	1.00
42	10/09/2022 - 10/15/2022	0.97	0.99
43	10/16/2022 - 10/22/2022	0.98	1.00
44	10/23/2022 - 10/29/2022	0.99	1.01
45	10/30/2022 - 11/05/2022	1.00	1.02
46	11/06/2022 - 11/12/2022	1.01	1.03
47	11/13/2022 - 11/19/2022	1.02	1.04
48	11/20/2022 - 11/26/2022	1.02	1.04
49	11/27/2022 - 12/03/2022	1.02	1.04
50	12/04/2022 - 12/10/2022	1.02	1.04
51	12/11/2022 - 12/17/2022	1.02	1.04
52	12/18/2022 - 12/24/2022	1.05	1.07
53	12/25/2022 - 12/31/2022	1.08	1.10

* PEAK SEASON

23-FEB-2023 09:11:19

830UPD

2_2900_PKSEASON.TXT

Location Details			
Signal ID:	1002	Date:	November 20, 2021
Major Street:	US 90	Orientation:	E-W
Minor Street:	FL Gateway Dr	Orientation:	N-S

Controller Timings (seconds)

Movement # (Controller Phase Ø)	Ø1	Ø2	Ø3	Ø4	Ø5	Ø6	Ø7	Ø8	Ø9	Ø10	Ø11	Ø12	Ø13	Ø14	Ø15	Ø16	Notes
Direction	EBLT	WB		NB	WBLT	EB		SB									
Turn Type	Prot Perm				Prot Perm												
Min Green	5	15		7	5	15		7									
Ext	3.0	4.0		3.0	3.0	4.0		3.0									
Yellow	4.8	4.9		3.8	4.9	4.9		3.8									
All Red	2.0	2.0		2.0	2.0	2.0		2.0									
Max I	15	75		20	15	75		20									
Max II																	
Walk		7		7		7		7									
Flashing Don't Walk		18		29		18		22									
Detector Memory																	
Det. Switching to:	Ø6				Ø2												
Recall		MIN				MIN											
CNA																	

Coordination Timings (seconds)

Pattern	C-S-O	Cycle Length	Splits														Offset	Seq	Coord Ø	
			Ø1	Ø2	Ø3	Ø4	Ø5	Ø6	Ø7	Ø8	Ø9	Ø10	Ø11	Ø12	Ø13	Ø14				Ø15
1		130	15	91 MAX		24	16	90 MAX		24								24	1	2
2		130	15	70 MAX		45	20	65 MAX		45								15	1	2
3		150	15	88 MAX		47	25	78 MAX		47								20	1	2
4		110	16	64 MAX		30	23	57 MAX		30								18	1	2
5		100	15	59 MAX		26	17	57 MAX		26								22	1	2
6		140	15	75 MAX		50	23	67 MAX		50								7	1	2
7		110	17	58 MAX		35	18	57 MAX		35								63	1	2
8		100	15	59 MAX		26	17	57 MAX		26								22	1	2
9		140	15	75 MAX		50	23	67 MAX		50								7	1	2
10		110	17	58 MAX		35	18	57 MAX		35								63	1	2

Offset Reference Point	Phase Mode
End of Green of first through movement	STD8

SEQ 1			
Ring - 1	1	2	4
Ring - 2	5	6	8

Notes:

- 1) Use 'Max I' during FREE Operation.
- 2) Program phase restriction to omit Ø1 during Ø2 green and omit Ø5 during Ø6 green.

Signal ID:	1002
Major Street:	US 90
Minor Street:	FL Gateway Dr

Day Plans

Monday-Thursday Day Plan 1				Saturday Day Plan 2				Sunday Day Plan 3				Friday Day Plan 4			
Hr	Min	Patt	Cycl	Hr	Min	Patt	Cycl	Hr	Min	Patt	Cycl	Hr	Min	Patt	Cycl
00	00	254	Free	00	00	254	Free	00	00	254	Free	00	00	254	Free
6	30	1	130	8	00	5	100	9	30	8	100	6	30	1	130
10	00	2	130	10	00	6	140	11	00	9	140	10	00	2	130
15	00	3	150	17	00	7	110	16	30	10	110	11	30	3	150
18	30	4	110	22	00	254	Free	21	00	254	Free	19	00	4	110
21	00	254	Free									22	00	254	Free

Day Plan 5				Day Plan 6				Day Plan 7				Day Plan 8			
Hr	Min	Patt	Cycl	Hr	Min	Patt	Cycl	Hr	Min	Patt	Cycl	Hr	Min	Patt	Cycl

Patt	Force Mode	Alt Opt Table	Alt Time Table	Coord Max Plan	Alt Time Table Max Values (Seconds)															
					Ø1	Ø2	Ø3	Ø4	Ø5	Ø6	Ø7	Ø8	Ø9	Ø10	Ø11	Ø12	Ø13	Ø14	Ø15	Ø16
1	FIXED	None	None	Max Inh																
2	FIXED	None	None	Max Inh																
3	FIXED	None	None	Max Inh																
4	FIXED	None	None	Max Inh																
5	FIXED	None	None	Max Inh																
6	FIXED	None	None	Max Inh																
7	FIXED	None	None	Max Inh																
8	FIXED	None	None	Max Inh																
9	FIXED	None	None	Max Inh																
10	FIXED	None	None	Max Inh																

APPENDIX C
Intersection Volume Development Worksheets

TRAFFIC VOLUMES AT STUDY INTERSECTIONS

INTERSECTION: US 90/SR 10 & Centurion C/SW Florida Gateway Dr
COUNT DATE: October 5, 2023
AM PEAK HOUR FACTOR: 0.87
PM PEAK HOUR FACTOR: 0.94

"AM EXISTING TRAFFIC"																	
	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR	
AM Raw Turning Movements	0	35	1,309	11	2	23	976	86	0	18	8	64	0	91	5	46	
Peak Season Conversion Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	
AM EXISTING CONDITIONS																	
	0	35	1,309	11	2	23	976	86	0	18	8	64	0	91	5	46	
"PM EXISTING TRAFFIC"																	
	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR	
PM Raw Turning Movements	0	28	1,150	17	3	59	1,306	94	0	21	0	67	0	70	1	45	
Peak Season Conversion Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	
PM EXISTING CONDITIONS																	
	0	28	1,150	17	3	59	1,306	94	0	21	0	67	0	70	1	45	
"AM BACKGROUND TRAFFIC"																	
	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR	
Years To Buildout	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	
Yearly Growth Rate	3.6%	3.6%	3.6%	3.6%	3.6%	3.6%	3.6%	3.6%	3.6%	3.6%	3.6%	3.6%	3.6%	3.6%	3.6%	3.6%	
AM BACKGROUND TRAFFIC GROWTH	0	1	47	0	0	1	35	3	0	1	0	2	0	3	0	2	
AM NON-PROJECT TRAFFIC																	
	0	36	1,356	11	2	24	1,011	89	0	19	8	66	0	94	5	48	
"PM BACKGROUND TRAFFIC"																	
	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR	
Years To Buildout	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	1	
Yearly Growth Rate	3.6%	3.6%	3.6%	3.6%	3.6%	3.6%	3.6%	3.6%	3.6%	3.6%	3.6%	3.6%	3.6%	3.6%	3.6%	3.6%	
PM BACKGROUND TRAFFIC GROWTH	0	1	42	1	0	2	47	3	0	1	0	2	0	3	0	2	
PM NON-PROJECT TRAFFIC																	
	0	29	1,192	18	3	61	1,353	97	0	22	0	69	0	73	1	47	
"AM PROJECT DISTRIBUTION"																	
LAND USE	TYPE	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Pass-By Distribution	Entering		50.0%	-50.0%				-50.0%	50.0%								
	Exiting														50.0%		50.0%
Net New Distribution	Entering		25.0%						75.0%								
	Exiting														75.0%		25.0%
"PM PROJECT DISTRIBUTION"																	
LAND USE	TYPE	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Pass-By Distribution	Entering		50.0%	-50.0%				-50.0%	50.0%								
	Exiting														50.0%		50.0%
Net New Distribution	Entering		25.0%						75.0%								
	Exiting														75.0%		25.0%
"AM PROJECT TRAFFIC"																	
LAND USE	TYPE	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Project Trips	Pass - By		12	-12				-12	12						12		12
	Net New		2						6						6		2
AM TOTAL PROJECT TRAFFIC		0	14	-12	0	0	0	-12	18	0	0	0	0	0	18	0	14
AM TOTAL TRAFFIC																	
	0	50	1,344	11	2	24	999	107	0	19	8	66	0	112	5	62	
"PM PROJECT TRAFFIC"																	
LAND USE	TYPE	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Project Trips	Pass - By		13	-13				-14	14						13		14
	Net New		2						7						7		2
PM TOTAL PROJECT TRAFFIC		0	15	-13	0	0	0	-14	21	0	0	0	0	0	20	0	16
PM TOTAL TRAFFIC																	
	0	44	1,179	18	3	61	1,339	118	0	22	0	69	0	93	1	63	

APPENDIX D
Synchro Output Reports

Timings
1: SW Florida Gateway Dr/Centurion Ct & US 90/SR 10

Circle K - I-75 & US 90
Existing (2023) Conditions, AM Peak Hour



Lane Group	EBL	EBT	WBL	WBT	WBR	NBL	NBT	SBL	SBT
Lane Configurations	↖	↗	↖	↗	↗	↖	↗	↖	↗
Traffic Volume (vph)	35	1309	25	976	86	18	8	91	5
Future Volume (vph)	35	1309	25	976	86	18	8	91	5
Turn Type	pm+pt	NA	pm+pt	NA	Perm	Perm	NA	Perm	NA
Protected Phases	1	6	5	2			4		8
Permitted Phases	6		2		2	4		8	
Detector Phase	1	6	5	2	2	4	4	8	8
Switch Phase									
Minimum Initial (s)	5.0	15.0	5.0	15.0	15.0	7.0	7.0	7.0	7.0
Minimum Split (s)	11.8	31.9	11.9	31.9	31.9	41.8	41.8	34.8	34.8
Total Split (s)	15.0	90.0	16.0	91.0	91.0	24.0	24.0	24.0	24.0
Total Split (%)	11.5%	69.2%	12.3%	70.0%	70.0%	18.5%	18.5%	18.5%	18.5%
Yellow Time (s)	4.8	4.9	4.9	4.9	4.9	3.8	3.8	3.8	3.8
All-Red Time (s)	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	6.8	6.9	6.9	6.9	6.9	5.8	5.8	5.8	5.8
Lead/Lag	Lead	Lag	Lead	Lag	Lag				
Lead-Lag Optimize?	Yes	Yes	Yes	Yes	Yes				
Recall Mode	None	C-Min	None	C-Min	C-Min	None	None	None	None
Act Effct Green (s)	97.5	93.4	95.7	90.8	90.8	15.9	15.9	15.9	15.9
Actuated g/C Ratio	0.75	0.72	0.74	0.70	0.70	0.12	0.12	0.12	0.12
v/c Ratio	0.11	0.60	0.12	0.46	0.09	0.14	0.34	0.68	0.25
Control Delay	4.9	12.0	5.4	10.7	2.8	50.2	16.6	74.7	17.0
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	4.9	12.0	5.4	10.7	2.8	50.2	16.6	74.7	17.0
LOS	A	B	A	B	A	D	B	E	B
Approach Delay		11.9		9.9			23.4		53.9
Approach LOS		B		A			C		D

Intersection Summary

Cycle Length: 130
 Actuated Cycle Length: 130
 Offset: 24 (18%), Referenced to phase 2:WBT and 6:EBT, Start of Yellow
 Natural Cycle: 100
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 0.68
 Intersection Signal Delay: 13.7
 Intersection Capacity Utilization 58.8%
 Analysis Period (min) 15
 Intersection LOS: B
 ICU Level of Service B

Splits and Phases: 1: SW Florida Gateway Dr/Centurion Ct & US 90/SR 10



Timings
1: SW Florida Gateway Dr/Centurion Ct & US 90/SR 10

Circle K - I-75 & US 90
Existing (2023) Conditions, PM Peak Hour



Lane Group	EBL	EBT	WBL	WBT	WBR	NBL	NBT	SBL	SBT
Lane Configurations	↖	↕	↖	↕	↖	↖	↕	↖	↕
Traffic Volume (vph)	28	1150	62	1306	94	21	0	70	1
Future Volume (vph)	28	1150	62	1306	94	21	0	70	1
Turn Type	pm+pt	NA	pm+pt	NA	Perm	Perm	NA	Perm	NA
Protected Phases	1	6	5	2			4		8
Permitted Phases	6		2		2	4		8	
Detector Phase	1	6	5	2	2	4	4	8	8
Switch Phase									
Minimum Initial (s)	5.0	15.0	5.0	15.0	15.0	7.0	7.0	7.0	7.0
Minimum Split (s)	11.8	31.9	11.9	31.9	31.9	41.8	41.8	34.8	34.8
Total Split (s)	15.0	78.0	25.0	88.0	88.0	47.0	47.0	47.0	47.0
Total Split (%)	10.0%	52.0%	16.7%	58.7%	58.7%	31.3%	31.3%	31.3%	31.3%
Yellow Time (s)	4.8	4.9	4.9	4.9	4.9	3.8	3.8	3.8	3.8
All-Red Time (s)	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	6.8	6.9	6.9	6.9	6.9	5.8	5.8	5.8	5.8
Lead/Lag	Lead	Lag	Lead	Lag	Lag				
Lead-Lag Optimize?	Yes	Yes	Yes	Yes	Yes				
Recall Mode	None	Max	None	C-Max	C-Max	None	None	None	None
Act Effct Green (s)	117.5	112.5	119.9	115.7	115.7	13.8	13.8	13.8	13.8
Actuated g/C Ratio	0.78	0.75	0.80	0.77	0.77	0.09	0.09	0.09	0.09
v/c Ratio	0.10	0.47	0.19	0.51	0.08	0.18	0.23	0.61	0.26
Control Delay	3.9	8.9	4.3	8.5	2.6	63.8	1.8	85.4	18.9
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	3.9	8.9	4.3	8.5	2.6	63.8	1.8	85.4	18.9
LOS	A	A	A	A	A	E	A	F	B
Approach Delay		8.8		7.9			16.4		58.9
Approach LOS		A		A			B		E

Intersection Summary

Cycle Length: 150
 Actuated Cycle Length: 150
 Offset: 20 (13%), Referenced to phase 2:WBTL, Start of Yellow
 Natural Cycle: 100
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 0.61
 Intersection Signal Delay: 10.6
 Intersection LOS: B
 Intersection Capacity Utilization 67.1%
 ICU Level of Service C
 Analysis Period (min) 15

Splits and Phases: 1: SW Florida Gateway Dr/Centurion Ct & US 90/SR 10



HCM 6th Signalized Intersection Summary
 1: SW Florida Gateway Dr/Centurion Ct & US 90/SR 10

Circle K - I-75 & US 90
 Existing (2023) Conditions, PM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↕	↗	↖	↕	↗	↖	↕	↗	↖	↕	↗
Traffic Volume (veh/h)	28	1150	17	62	1306	94	21	0	67	70	1	45
Future Volume (veh/h)	28	1150	17	62	1306	94	21	0	67	70	1	45
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		0.98	1.00		0.98	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	30	1223	18	66	1389	84	22	0	27	74	1	18
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	310	2696	40	387	2710	1180	149	0	132	142	7	126
Arrive On Green	0.02	0.75	0.75	0.03	0.76	0.76	0.08	0.00	0.08	0.08	0.08	0.08
Sat Flow, veh/h	1781	3584	53	1781	3554	1548	1393	0	1585	1383	84	1514
Grp Volume(v), veh/h	30	606	635	66	1389	84	22	0	27	74	0	19
Grp Sat Flow(s), veh/h/ln	1781	1777	1859	1781	1777	1548	1393	0	1585	1383	0	1598
Q Serve(g_s), s	0.6	19.2	19.2	1.2	22.8	2.0	2.2	0.0	2.4	7.9	0.0	1.7
Cycle Q Clear(g_c), s	0.6	19.2	19.2	1.2	22.8	2.0	3.9	0.0	2.4	10.3	0.0	1.7
Prop In Lane	1.00		0.03	1.00		1.00	1.00		1.00	1.00		0.95
Lane Grp Cap(c), veh/h	310	1337	1399	387	2710	1180	149	0	132	142	0	133
V/C Ratio(X)	0.10	0.45	0.45	0.17	0.51	0.07	0.15	0.00	0.20	0.52	0.00	0.14
Avail Cap(c_a), veh/h	365	1337	1399	543	2710	1180	415	0	435	406	0	439
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	5.5	7.0	7.0	5.0	6.9	4.5	65.6	0.0	64.1	68.9	0.0	63.7
Incr Delay (d2), s/veh	0.1	1.1	1.1	0.2	0.7	0.1	0.5	0.0	0.8	3.0	0.0	0.5
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	0.3	10.9	11.3	0.7	11.9	1.1	1.5	0.0	1.8	5.3	0.0	1.3
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	5.6	8.1	8.0	5.2	7.6	4.6	66.0	0.0	64.8	71.8	0.0	64.2
LnGrp LOS	A	A	A	A	A	A	E	A	E	E	A	E
Approach Vol, veh/h	1271			1539			49			93		
Approach Delay, s/veh	8.0			7.4			65.4			70.3		
Approach LOS	A			A			E			E		
Timer - Assigned Phs	1	2	4		5	6	8					
Phs Duration (G+Y+Rc), s	10.4	121.3	18.3		11.9	119.8	18.3					
Change Period (Y+Rc), s	6.8	6.9	*5.8		6.9	6.9	*5.8					
Max Green Setting (Gmax), s	8.2	81.1	*41		18.1	71.1	*41					
Max Q Clear Time (g_c+I1), s	2.6	24.8	5.9		3.2	21.2	12.3					
Green Ext Time (p_c), s	0.0	14.4	0.2		0.1	9.8	0.3					

Intersection Summary

HCM 6th Ctrl Delay 10.6
 HCM 6th LOS B

Notes

User approved pedestrian interval to be less than phase max green.
 * HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Timings
1: SW Florida Gateway Dr/Centurion Ct & US 90/SR 10

Circle K - I-75 & US 90
Background (2024) Conditions, AM Peak Hour



Lane Group	EBL	EBT	WBL	WBT	WBR	NBL	NBT	SBL	SBT
Lane Configurations	↖	↖↗	↖	↖↗	↖	↖	↗	↖	↗
Traffic Volume (vph)	36	1356	26	1011	89	19	8	94	5
Future Volume (vph)	36	1356	26	1011	89	19	8	94	5
Turn Type	pm+pt	NA	pm+pt	NA	Perm	Perm	NA	Perm	NA
Protected Phases	1	6	5	2			4		8
Permitted Phases	6		2		2	4		8	
Detector Phase	1	6	5	2	2	4	4	8	8
Switch Phase									
Minimum Initial (s)	5.0	15.0	5.0	15.0	15.0	7.0	7.0	7.0	7.0
Minimum Split (s)	11.8	31.9	11.9	31.9	31.9	41.8	41.8	34.8	34.8
Total Split (s)	15.0	90.0	16.0	91.0	91.0	24.0	24.0	24.0	24.0
Total Split (%)	11.5%	69.2%	12.3%	70.0%	70.0%	18.5%	18.5%	18.5%	18.5%
Yellow Time (s)	4.8	4.9	4.9	4.9	4.9	3.8	3.8	3.8	3.8
All-Red Time (s)	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	6.8	6.9	6.9	6.9	6.9	5.8	5.8	5.8	5.8
Lead/Lag	Lead	Lag	Lead	Lag	Lag				
Lead-Lag Optimize?	Yes	Yes	Yes	Yes	Yes				
Recall Mode	None	C-Min	None	C-Min	C-Min	None	None	None	None
Act Effct Green (s)	97.1	93.1	95.5	90.5	90.5	16.3	16.3	16.3	16.3
Actuated g/C Ratio	0.75	0.72	0.73	0.70	0.70	0.13	0.13	0.13	0.13
v/c Ratio	0.12	0.62	0.13	0.48	0.09	0.14	0.34	0.68	0.25
Control Delay	5.1	12.7	5.7	11.1	3.0	50.0	16.3	74.7	16.7
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	5.1	12.7	5.7	11.1	3.0	50.0	16.3	74.7	16.7
LOS	A	B	A	B	A	D	B	E	B
Approach Delay		12.5		10.3			23.2		53.7
Approach LOS		B		B			C		D

Intersection Summary


Cycle Length: 130
 Actuated Cycle Length: 130
 Offset: 24 (18%), Referenced to phase 2:WBT and 6:EBTL, Start of Yellow
 Natural Cycle: 110
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 0.68
 Intersection Signal Delay: 14.2
 Intersection LOS: B
 Intersection Capacity Utilization 60.3%
 ICU Level of Service B
 Analysis Period (min) 15

Splits and Phases: 1: SW Florida Gateway Dr/Centurion Ct & US 90/SR 10



HCM 6th Signalized Intersection Summary
 1: SW Florida Gateway Dr/Centurion Ct & US 90/SR 10

Circle K - I-75 & US 90
 Background (2024) Conditions, AM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↕		↖	↕	↖	↖	↕		↖	↕	
Traffic Volume (veh/h)	36	1356	11	26	1011	89	19	8	66	94	5	48
Future Volume (veh/h)	36	1356	11	26	1011	89	19	8	66	94	5	48
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		0.98	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1856	1856	1856	1752	1752	1752	1826	1826	1826
Adj Flow Rate, veh/h	41	1559	13	30	1162	73	22	9	45	108	6	15
Peak Hour Factor	0.87	0.87	0.87	0.87	0.87	0.87	0.87	0.87	0.87	0.87	0.87	0.87
Percent Heavy Veh, %	2	2	2	3	3	3	10	10	10	5	5	5
Cap, veh/h	352	2462	21	266	2437	1063	208	33	163	183	60	149
Arrive On Green	0.03	0.68	0.68	0.04	0.69	0.69	0.13	0.13	0.13	0.13	0.13	0.13
Sat Flow, veh/h	1781	3612	30	1767	3526	1538	1300	253	1267	1315	461	1154
Grp Volume(v), veh/h	41	767	805	30	1162	73	22	0	54	108	0	21
Grp Sat Flow(s), veh/h/ln	1781	1777	1865	1767	1763	1538	1300	0	1520	1315	0	1615
Q Serve(g_s), s	0.9	31.4	31.4	0.6	19.7	2.0	2.0	0.0	4.2	10.5	0.0	1.5
Cycle Q Clear(g_c), s	0.9	31.4	31.4	0.6	19.7	2.0	3.5	0.0	4.2	14.7	0.0	1.5
Prop In Lane	1.00		0.02	1.00		1.00	1.00		0.83	1.00		0.71
Lane Grp Cap(c), veh/h	352	1211	1271	266	2437	1063	208	0	196	183	0	208
V/C Ratio(X)	0.12	0.63	0.63	0.11	0.48	0.07	0.11	0.00	0.28	0.59	0.00	0.10
Avail Cap(c_a), veh/h	411	1211	1271	322	2437	1063	222	0	213	197	0	226
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	7.0	11.6	11.6	9.4	9.2	6.5	51.5	0.0	51.1	57.8	0.0	50.0
Incr Delay (d2), s/veh	0.1	2.5	2.4	0.2	0.7	0.1	0.2	0.0	0.8	4.0	0.0	0.2
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	0.5	17.1	17.8	0.4	11.1	1.1	1.2	0.0	3.0	6.7	0.0	1.1
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	7.1	14.1	14.0	9.6	9.9	6.6	51.7	0.0	51.9	61.8	0.0	50.2
LnGrp LOS	A	B	B	A	A	A	D	A	D	E	A	D
Approach Vol, veh/h		1613			1265			76			129	
Approach Delay, s/veh		13.9			9.7			51.8			59.9	
Approach LOS		B			A			D			E	
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	10.7	96.8		22.6	11.9	95.5		22.6				
Change Period (Y+Rc), s	6.8	6.9		* 5.8	6.9	6.9		* 5.8				
Max Green Setting (Gmax), s	8.2	84.1		* 18	9.1	83.1		* 18				
Max Q Clear Time (g_c+1), s	2.9	21.7		6.2	2.6	33.4		16.7				
Green Ext Time (p_c), s	0.0	10.7		0.2	0.0	15.2		0.1				

Intersection Summary												
HCM 6th Ctrl Delay											15.0	
HCM 6th LOS											B	

Notes

User approved pedestrian interval to be less than phase max green.

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Timings

1: SW Florida Gateway Dr/Centurion Ct & US 90/SR 10

Circle K - I-75 & US 90

Background (2024) Conditions, PM Peak Hour

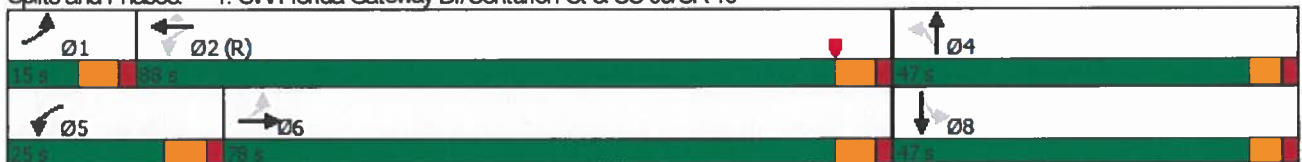


Lane Group	EBL	EBT	WBL	WBT	WBR	NBL	NBT	SBL	SBT
Lane Configurations	↖	↕	↖	↕	↗	↖	↗	↖	↗
Traffic Volume (vph)	29	1192	64	1353	97	22	0	73	1
Future Volume (vph)	29	1192	64	1353	97	22	0	73	1
Turn Type	pr+pt	NA	pr+pt	NA	Perm	Perm	NA	Perm	NA
Protected Phases	1	6	5	2			4		8
Permitted Phases	6		2		2	4		8	
Detector Phase	1	6	5	2	2	4	4	8	8
Switch Phase									
Minimum Initial (s)	5.0	15.0	5.0	15.0	15.0	7.0	7.0	7.0	7.0
Minimum Split (s)	11.8	31.9	11.9	31.9	31.9	41.8	41.8	34.8	34.8
Total Split (s)	15.0	78.0	25.0	88.0	88.0	47.0	47.0	47.0	47.0
Total Split (%)	10.0%	52.0%	16.7%	58.7%	58.7%	31.3%	31.3%	31.3%	31.3%
Yellow Time (s)	4.8	4.9	4.9	4.9	4.9	3.8	3.8	3.8	3.8
All-Red Time (s)	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	6.8	6.9	6.9	6.9	6.9	5.8	5.8	5.8	5.8
Lead/Lag	Lead	Lag	Lead	Lag	Lag				
Lead-Lag Optimize?	Yes	Yes	Yes	Yes	Yes				
Recall Mode	None	Max	None	C-Max	C-Max	None	None	None	None
Act Effct Green (s)	117.0	112.0	119.5	115.2	115.2	14.2	14.2	14.2	14.2
Actuated g/C Ratio	0.78	0.75	0.80	0.77	0.77	0.09	0.09	0.09	0.09
v/c Ratio	0.11	0.49	0.21	0.53	0.09	0.18	0.24	0.62	0.26
Control Delay	4.1	9.4	4.6	9.0	2.7	63.3	1.8	85.5	18.3
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	4.1	9.4	4.6	9.0	2.7	63.3	1.8	85.5	18.3
LOS	A	A	A	A	A	E	A	F	B
Approach Delay		9.2		8.4			16.5		58.9
Approach LOS		A		A			B		E

Intersection Summary

Cycle Length: 150
 Actuated Cycle Length: 150
 Offset: 20 (13%), Referenced to phase 2:WBT, Start of Yellow
 Natural Cycle: 100
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 0.62
 Intersection Signal Delay: 11.0
 Intersection LOS: B
 Intersection Capacity Utilization 68.5%
 ICU Level of Service C
 Analysis Period (min) 15

Splits and Phases: 1: SWFlorida Gateway Dr/Centurion Ct & US 90/SR 10



HCM 6th Signalized Intersection Summary
 1: SW Florida Gateway Dr/Centurion Ct & US 90/SR 10

Circle K - I-75 & US 90
 Background (2024) Conditions, PM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↖↗		↖	↖↗	↖	↖	↖		↖	↖	
Traffic Volume (veh/h)	29	1192	18	64	1353	97	22	0	69	73	1	47
Future Volume (veh/h)	29	1192	18	64	1353	97	22	0	69	73	1	47
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		0.98	1.00		0.98	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	31	1268	19	68	1439	87	23	0	29	78	1	20
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94	0.94
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	294	2680	40	369	2694	1173	153	0	139	146	7	134
Arrive On Green	0.02	0.75	0.75	0.03	0.76	0.76	0.09	0.00	0.09	0.09	0.09	0.09
Sat Flow, veh/h	1781	3582	54	1781	3554	1548	1391	0	1585	1381	76	1521
Grp Volume(v), veh/h	31	629	658	68	1439	87	23	0	29	78	0	21
Grp Sat Flow(s),veh/h/ln	1781	1777	1859	1781	1777	1548	1391	0	1585	1381	0	1597
Q Serve(g_s), s	0.6	20.7	20.7	1.3	24.7	2.2	2.3	0.0	2.6	8.3	0.0	1.8
Cycle Q Clear(g_c), s	0.6	20.7	20.7	1.3	24.7	2.2	4.2	0.0	2.6	10.9	0.0	1.8
Prop In Lane	1.00		0.03	1.00		1.00	1.00		1.00	1.00		0.95
Lane Grp Cap(c), veh/h	294	1329	1391	369	2694	1173	153	0	139	146	0	140
V/C Ratio(X)	0.11	0.47	0.47	0.18	0.53	0.07	0.15	0.00	0.21	0.54	0.00	0.15
Avail Cap(c_a), veh/h	348	1329	1391	525	2694	1173	413	0	435	404	0	439
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	5.9	7.4	7.4	5.4	7.4	4.7	65.2	0.0	63.6	68.6	0.0	63.2
Incr Delay (d2), s/veh	0.2	1.2	1.2	0.2	0.8	0.1	0.4	0.0	0.7	3.0	0.0	0.5
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	0.4	11.6	12.0	0.7	12.8	1.1	1.5	0.0	1.9	5.6	0.0	1.4
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	6.1	8.6	8.5	5.7	8.1	4.8	65.6	0.0	64.3	71.7	0.0	63.7
LnGrp LOS	A	A	A	A	A	A	E	A	E	E	A	E
Approach Vol, veh/h		1318			1594			52				99
Approach Delay, s/veh		8.5			7.9			64.9				70.0
Approach LOS		A			A			E				E
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	10.4	120.6		19.0	11.9	119.1		19.0				
Change Period (Y+Rc), s	6.8	6.9		*5.8	6.9	6.9		*5.8				
Max Green Setting (Gmax), s	8.2	81.1		*41	18.1	71.1		*41				
Max Q Clear Time (g_c+I1), s	2.6	26.7		6.2	3.3	22.7		12.9				
Green Ext Time (p_c), s	0.0	15.3		0.2	0.1	10.4		0.3				

Intersection Summary		
HCM6th Ctrl Delay		11.1
HCM 6th LOS		B

Notes
 User approved pedestrian interval to be less than phase max green.
 * HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Timings
1: SW Florida Gateway Dr/Centurion Ct & US 90/SR 10

Circle K - I-75 & US 90
Buildout (2024) Conditions, AM Peak Hour



Lane Group	EBL	EBT	WBL	WBT	WBR	NBL	NBT	SBL	SBT
Lane Configurations	↙	↕	↙	↕	↗	↙	↕	↙	↕
Traffic Volume (vph)	50	1344	26	999	107	19	8	112	5
Future Volume (vph)	50	1344	26	999	107	19	8	112	5
Turn Type	pm+pt	NA	pm+pt	NA	Perm	Perm	NA	Perm	NA
Protected Phases	1	6	5	2			4		8
Permitted Phases	6		2		2	4		8	
Detector Phase	1	6	5	2	2	4	4	8	8
Switch Phase									
Minimum Initial (s)	5.0	15.0	5.0	15.0	15.0	7.0	7.0	7.0	7.0
Minimum Split (s)	11.8	31.9	11.9	31.9	31.9	41.8	41.8	34.8	34.8
Total Split (s)	15.0	90.0	16.0	91.0	91.0	24.0	24.0	24.0	24.0
Total Split (%)	11.5%	69.2%	12.3%	70.0%	70.0%	18.5%	18.5%	18.5%	18.5%
Yellow Time (s)	4.8	4.9	4.9	4.9	4.9	3.8	3.8	3.8	3.8
All-Red Time (s)	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	6.8	6.9	6.9	6.9	6.9	5.8	5.8	5.8	5.8
Lead/Lag	Lead	Lag	Lead	Lag	Lag				
Lead-Lag Optimize?	Yes	Yes	Yes	Yes	Yes				
Recall Mode	None	C-Min	None	C-Min	C-Min	None	None	None	None
Act Effct Green (s)	95.4	91.0	93.1	88.1	88.1	18.3	18.3	18.3	18.3
Actuated g/C Ratio	0.73	0.70	0.72	0.68	0.68	0.14	0.14	0.14	0.14
v/c Ratio	0.17	0.63	0.14	0.48	0.12	0.13	0.31	0.72	0.28
Control Delay	5.7	13.7	6.2	12.1	3.2	48.3	15.4	75.2	14.5
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	5.7	13.7	6.2	12.1	3.2	48.3	15.4	75.2	14.5
LOS	A	B	A	B	A	D	B	E	B
Approach Delay		13.4		11.1			22.2		52.5
Approach LOS		B		B			C		D

Intersection Summary

Cycle Length: 130
 Actuated Cycle Length: 130
 Offset: 24 (18%), Referenced to phase 2:WBTL and 6:EBTL, Start of Yellow
 Natural Cycle: 100
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 0.72
 Intersection Signal Delay: 15.3
 Intersection LOS: B
 Intersection Capacity Utilization 65.0%
 ICU Level of Service C
 Analysis Period (min) 15

Splits and Phases: 1: SW Florida Gateway Dr/Centurion Ct & US 90/SR 10



HCM 6th Signalized Intersection Summary
 1: SW Florida Gateway Dr/Centurion Ct & US 90/SR 10

Circle K - I-75 & US 90
 Buildout (2024) Conditions, AM Peak Hour

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	50	1344	11	26	999	107	19	8	66	112	5	62
Future Volume (veh/h)	50	1344	11	26	999	107	19	8	66	112	5	62
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		0.98	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1870	1870	1870	1856	1856	1856	1752	1752	1752	1826	1826	1826
Adj Flow Rate, veh/h	57	1545	13	30	1148	94	22	9	45	129	6	31
Peak Hour Factor	0.87	0.87	0.87	0.87	0.87	0.87	0.87	0.87	0.87	0.87	0.87	0.87
Percent Heavy Veh, %	2	2	2	3	3	3	10	10	10	5	5	5
Cap, veh/h	348	2422	20	263	2385	1040	208	35	177	198	36	186
Arrive On Green	0.03	0.67	0.67	0.04	0.68	0.68	0.14	0.14	0.14	0.14	0.14	0.14
Sat Flow, veh/h	1781	3611	30	1767	3526	1538	1282	253	1267	1316	257	1327
Grp Volume(v), veh/h	57	760	798	30	1148	94	22	0	54	129	0	37
Grp Sat Flow(s),veh/h/ln	1781	1777	1865	1767	1763	1538	1282	0	1521	1316	0	1584
Q Serve(g_s), s	1.3	32.0	32.0	0.7	20.3	2.7	2.0	0.0	4.1	12.6	0.0	2.7
Cycle Q Clear(g_c), s	1.3	32.0	32.0	0.7	20.3	2.7	4.7	0.0	4.1	16.7	0.0	2.7
Prop In Lane	1.00		0.02	1.00		1.00	1.00		0.83	1.00		0.84
Lane Grp Cap(c), veh/h	348	1192	1251	263	2385	1040	208	0	213	198	0	222
V/C Ratio(X)	0.16	0.64	0.64	0.11	0.48	0.09	0.11	0.00	0.25	0.65	0.00	0.17
Avail Cap(c_a), veh/h	401	1192	1251	318	2385	1040	208	0	213	198	0	222
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(l)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	7.6	12.3	12.3	9.9	10.1	7.2	51.3	0.0	49.8	57.3	0.0	49.2
Incr Delay (d2), s/veh	0.2	2.6	2.5	0.2	0.7	0.2	0.2	0.0	0.6	7.4	0.0	0.4
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	0.8	17.6	18.3	0.4	11.5	1.6	1.2	0.0	2.9	8.1	0.0	2.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	7.8	14.9	14.8	10.1	10.8	7.4	51.5	0.0	50.5	64.7	0.0	49.6
LnGrp LOS	A	B	B	B	B	A	D	A	D	E	A	D
Approach Vol, veh/h	1615			1272			76			166		
Approach Delay, s/veh	14.6			10.5			50.8			61.3		
Approach LOS	B			B			D			E		
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	11.2	94.8		24.0	11.9	94.1		24.0				
Change Period (Y+Rc), s	6.8	6.9		* 5.8	6.9	6.9		* 5.8				
Max Green Setting (Gmax), s	8.2	84.1		* 18	9.1	83.1		* 18				
Max Q Clear Time (g_c+I1), s	3.3	22.3		6.7	2.7	34.0		18.7				
Green Ext Time (p_c), s	0.0	10.6		0.2	0.0	14.9		0.0				
Intersection Summary												
HCM 6th Ctrl Delay	16.3											
HCM 6th LOS	B											
Notes												
User approved pedestrian interval to be less than phase max green.												
* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.												

Timings
1: SW Florida Gateway Dr/Centurion Ct & US 90/SR 10

Circle K - I-75 & US 90
Buildout (2024) Conditions, PM Peak Hour



Lane Group	EBL	EBT	WBL	WBT	WBR	NBL	NBT	SBL	SBT
Lane Configurations	↖	↕	↖	↕	↗	↖	↗	↖	↗
Traffic Volume (vph)	44	1179	64	1339	118	22	0	93	1
Future Volume (vph)	44	1179	64	1339	118	22	0	93	1
Turn Type	pm+pt	NA	pm+pt	NA	Perm	Perm	NA	Perm	NA
Protected Phases	1	6	5	2			4		8
Permitted Phases	6		2		2	4		8	
Detector Phase	1	6	5	2	2	4	4	8	8
Switch Phase									
Minimum Initial (s)	5.0	15.0	5.0	15.0	15.0	7.0	7.0	7.0	7.0
Minimum Split (s)	11.8	31.9	11.9	31.9	31.9	41.8	41.8	34.8	34.8
Total Split (s)	15.0	78.0	25.0	88.0	88.0	47.0	47.0	47.0	47.0
Total Split (%)	10.0%	52.0%	16.7%	58.7%	58.7%	31.3%	31.3%	31.3%	31.3%
Yellow Time (s)	4.8	4.9	4.9	4.9	4.9	3.8	3.8	3.8	3.8
All-Red Time (s)	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	6.8	6.9	6.9	6.9	6.9	5.8	5.8	5.8	5.8
Lead/Lag	Lead	Lag	Lead	Lag	Lag				
Lead-Lag Optimize?	Yes	Yes	Yes	Yes	Yes				
Recall Mode	None	Max	None	C-Max	C-Max	None	None	None	None
Act Effct Green (s)	114.9	109.5	115.6	110.0	110.0	16.6	16.6	16.6	16.6
Actuated g/C Ratio	0.77	0.73	0.77	0.73	0.73	0.11	0.11	0.11	0.11
v/c Ratio	0.17	0.49	0.21	0.55	0.11	0.16	0.22	0.68	0.29
Control Delay	5.2	10.5	5.3	11.1	3.8	60.0	1.5	85.5	15.6
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	5.2	10.5	5.3	11.1	3.8	60.0	1.5	85.5	15.6
LOS	A	B	A	B	A	E	A	F	B
Approach Delay		10.3		10.3			15.6		57.0
Approach LOS		B		B			B		E

Intersection Summary

Cycle Length: 150
 Actuated Cycle Length: 150
 Offset: 20 (13%), Referenced to phase 2:WBTL, Start of Yellow
 Natural Cycle: 100
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 0.68
 Intersection Signal Delay: 12.9
 Intersection LOS: B
 Intersection Capacity Utilization 69.2%
 ICU Level of Service C
 Analysis Period (min) 15

Splits and Phases: 1: SW Florida Gateway Dr/Centurion Ct & US 90/SR 10



APPENDIX E

Trip Generation Calculations

Table 1: Trip Generation

Land Use	Intensity	AM Peak Hour of Adjacent Street			PM Peak Hour of Adjacent Street		
		Total	In	Out	Total	In	Out
Existing Development Convenience Store/Gas Station (4-5.5k)	24 VFP	649	325	324	546	273	273
Existing Development Pass-By Convenience Store/Gas Station (4-5.5k)	Daily 75% AM 76% PM 75%	494	247	247	410	205	205
EXISTING SITE - POTENTIAL TOTAL DRIVEWAY VOLUMES		649	325	324	546	273	273
EXISTING SITE - POTENTIAL PASS-BY TRIPS		494	247	247	410	205	205
EXISTING SITE - POTENTIAL NEW EXTERNAL TRIPS		155	78	77	136	68	68
OBSERVED DRIVEWAY VOLUMES		201	106	95	220	115	105
ACTUAL/POTENTIAL DRIVEWAY VOLUMES ADJUSTMENT FACTOR		0.31			0.40		
Proposed Development Convenience Store/Gas Station (5.5-10k)	27 VFP	853	427	426	726	363	363
Proposed Development Pass-By Convenience Store/Gas Station (5.5-10k)	Daily 75% AM 76% PM 75%	648	324	324	544	272	272
PROPOSED SITE - POTENTIAL TOTAL DRIVEWAY VOLUMES		853	427	426	726	363	363
PROPOSED SITE - POTENTIAL TOTAL PASS-BY TRIPS		648	324	324	544	272	272
PROPOSED SITE - POTENTIAL TOTAL NEW EXTERNAL TRIPS		205	103	102	182	91	91
POTENTIAL NET NEW TOTAL DRIVEWAY VOLUMES (PROPOSED - EXISTING)		204	102	102	180	90	90
POTENTIAL NET NEW PASS-BY TRIPS (PROPOSED - EXISTING)		154	77	77	134	67	67
POTENTIAL NET NEW EXTERNAL TRIPS (PROPOSED - EXISTING)		50	25	25	46	23	23
ADJUSTED NET NEW TOTAL DRIVEWAY VOLUMES		64	32	32	72	36	36
ADJUSTED NET NEW PASS-BY TRIPS		48	24	24	54	27	27
ADJUSTED NET NEW EXTERNAL TRIPS		16	8	8	18	9	9

Trip generation and pass-by reductions were calculated using the following data from ITE's Trip Generation Manual, 11th Edition.

Convenience Store/ Gas Station (4-5.5k) [ITE 945]

Daily: T = 257.13*(X); X is vehicle fueling positions
 AM Peak Hour of Adjacent Street: T = 27.04*(X); X is vehicle fueling positions; (50% in, 50% out)
 PM Peak Hour of Adjacent Street: T = 22.76*(X); X is vehicle fueling positions; (50% in, 50% out)

Convenience Store/ Gas Station (5.5-10k) [ITE 945]

Daily: T = 345.75*(X); X is vehicle fueling positions
 AM Peak Hour of Adjacent Street: T = 31.60*(X); X is vehicle fueling positions; (50% in, 50% out)
 PM Peak Hour of Adjacent Street: T = 26.90*(X); X is vehicle fueling positions; (50% in, 50% out)

K:\ORL_Civil\149880040-Circle K US90 & I75\TPTO\03_Calcs\2022-03 - CK Lake City.xlsx\TG (2)

3/17/2022



APPENDIX F
FDOT *Trend Worksheet*

FLORIDA DEPARTMENT OF TRANSPORTATION
 TRANSPORTATION STATISTICS OFFICE
 2022 HISTORICAL AADT REPORT

COUNTY: 29 - COLUMBIA

SITE: 0278 - SR 10 400' W. OF I-75

YEAR	AADT	DIRECTION 1	DIRECTION 2	*K FACTOR	D FACTOR	T FACTOR
2022	32500 C	E 16500	W 16000	9.00	54.70	6.10
2021	28000 F	E 14000	W 14000	9.00	54.20	5.90
2020	27000 C	E 13500	W 13500	9.00	54.80	6.80
2019	30000 C	E 15000	W 15000	9.00	54.80	6.20
2018	28000 C	E 14000	W 14000	9.00	54.70	6.20
2017	27500 C	E 14000	W 13500	9.00	55.50	5.80
2016	27000 C	E 13500	W 13500	9.00	53.90	5.40
2015	27500 C	E 14000	W 13500	9.00	54.50	5.70
2014	27000 C	E 13500	W 13500	9.00	54.40	5.90
2013	25000 C	E 12500	W 12500	9.00	55.30	6.40
2012	26000 C	E 13000	W 13000	9.00	54.70	5.50
2011	26000 C	E 13000	W 13000	9.00	53.70	5.30
2010	25500 C	E 12500	W 13000	9.94	54.40	4.90
2009	25000 C	E 12500	W 12500	9.78	54.18	5.30
2008	27000 C	E 13500	W 13500	9.82	54.63	6.20
2007	27500 C	E 13500	W 14000	9.99	54.46	6.40

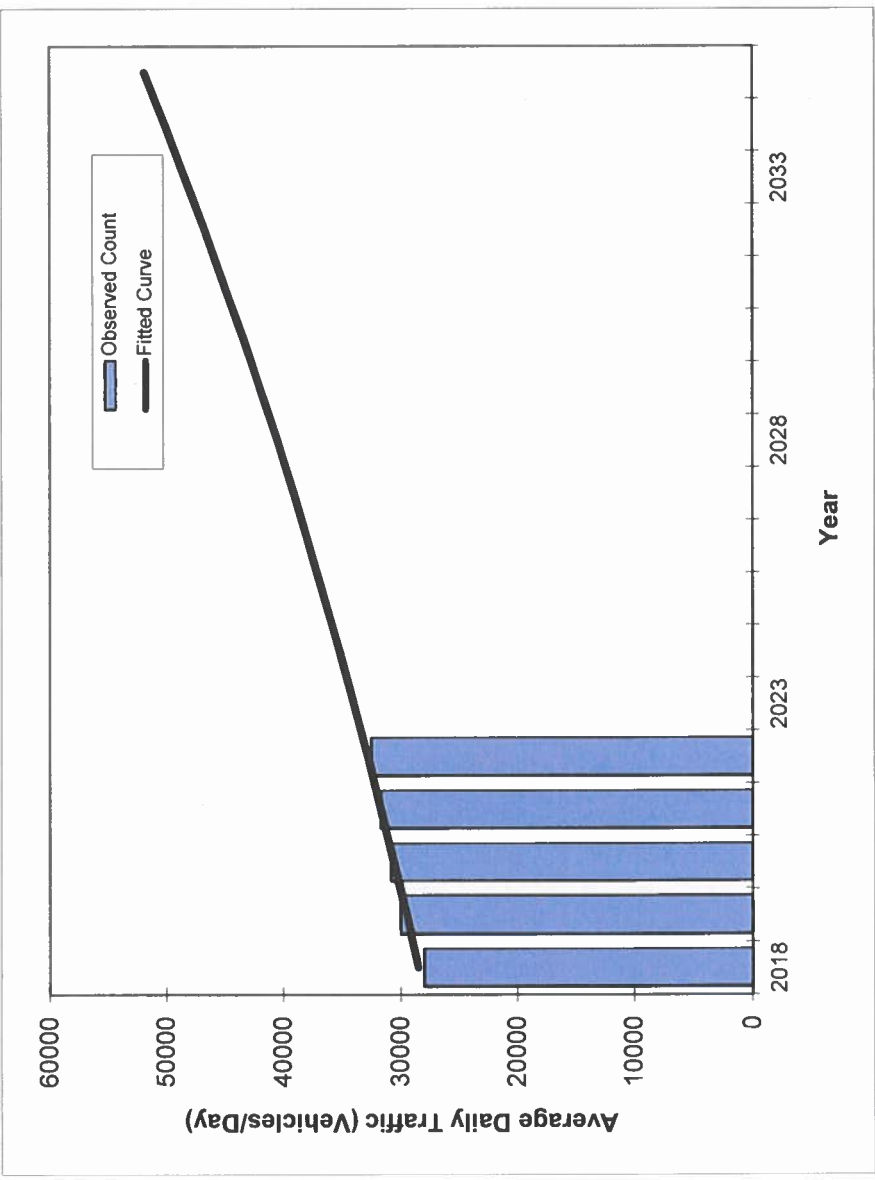
AADT FLAGS: C = COMPUTED; E = MANUAL ESTIMATE; F = FIRST YEAR ESTIMATE
 S = SECOND YEAR ESTIMATE; T = THIRD YEAR ESTIMATE; R = FOURTH YEAR ESTIMATE
 V = FIFTH YEAR ESTIMATE; 6 = SIXTH YEAR ESTIMATE; X = UNKNOWN
 *K FACTOR: STARTING WITH YEAR 2011 IS STANDARDK, PRIOR YEARS ARE K30 VALUES

Traffic Trends - V03.a

US 90/SR 10 -- 400' W. OF I-75

County:	Columbia (29)
Station #:	0278
Highway:	US 90/SR 10

FIN#	0
Location	1



Trend R-squared:	94.60%
Compounded Annual Historic Growth Rate:	3.58%
Compounded Growth Rate (2022 to Design Year):	3.61%
Printed:	12-Oct-23
Exponential Growth Option	

Year	Traffic (ADT/AADT)	
	Count*	Trend**
2018	28000	28500
2019	30000	29500
2020	30800	30600
2021	31700	31700
2022	32500	32800
2024 Opening Year Trend		
2024	N/A	35200
2025 Mid-Year Trend		
2025	N/A	36500
2026 Design Year Trend		
2026	N/A	37800
TRANPLAN Forecasts/Trends		

*Axle-Adjusted

COMPOSITE EXHIBIT H



Gateway Crossing Lot 6 U-Haul Storage Facility Site Plan Application

April 6, 2023
First Submittal

Logan B. Peters, PE
3530 NW 43rd Street
Gainesville, FL 32606
(352) 375-8999
www.jbpro.com

JBPro

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- IV. Conclusions10

I. Statement of Proposed Change

The proposed project is a site plan application for new construction to be located on Lot 6 of Gateway Crossing commercial subdivision, on 5.96 acres near the intersection of I-75 and US90. The proposed site plan proposes a three story 39,000 SF footprint primary U-Haul storage facility and a single story 13,700 SF footprint U-Box storage facility along with associated parking and utilities.

Parcel:

35-3S-16-02524-006

As shown on Tables 1 and 2 and Maps 1 and 2, the site plan is consistent with the execution of the established land use and zoning designations and is consistent with surrounding uses. The Commercial land use category has an intensity of ≤ 1 FAR and the proposed FAR of 0.20 meets this standard. The proposed storage facility is a permitted use in the property's Commercial future land use designation and Commercial Highway Interchange (CHI) zoning district.

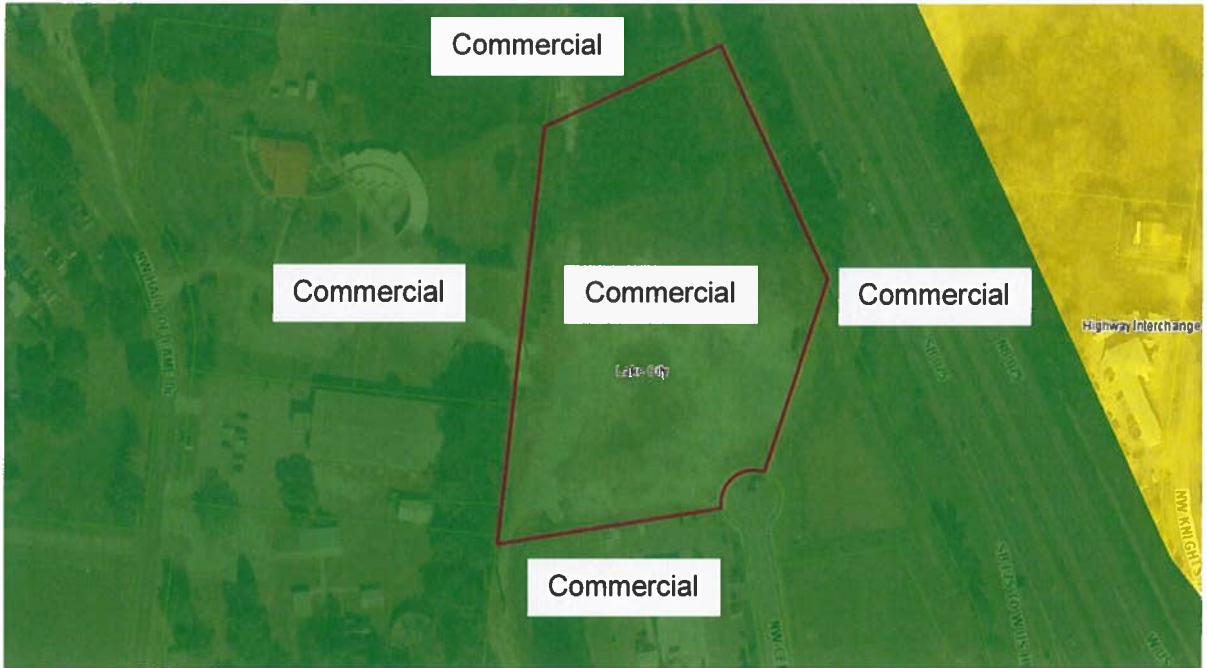
Table 1: Land Use and Zoning

Location	Land Use	Zoning
Proposed Property	Commercial	Commercial Highway Interchange
North	Commercial	General
South	Commercial	Commercial Highway Interchange
East	Commercial	Commercial Highway Interchange
West	Commercial	General

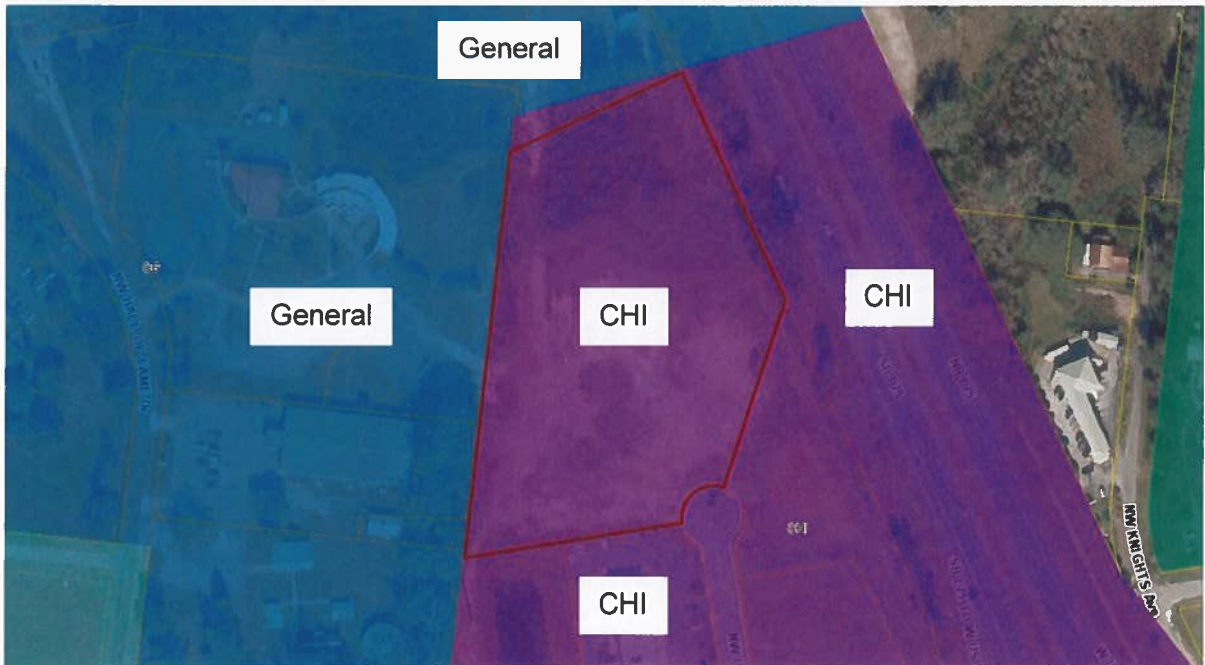
Table 2: Allowable – Proposed Dwelling Units

	Land Use	Zoning
Intensity Standard	1 FAR	1 FAR
Maximum Units Allowed	1 FAR	1 FAR
Proposed Project	0.20 FAR	0.20 FAR

Map 1: Existing Land Use Designations



Map 2: Zoning Designations



II. Concurrency Impact Analysis

The State of Florida growth management legislation establishes concurrency standards that ensure that local governments can adequately provide public facilities without constraining adopted local levels of service. In the following paragraphs, the proposed Comprehensive Plan Amendment will discuss how the proposed comprehensive plan amendment application impacts public service demands related to transportation, potable water, sanitary sewage, solid waste, stormwater, open space, recreation, and public school facilities.

Transportation Mobility

The Lake City Comprehensive Plan Capital Improvements Element Policy VIII.1.1 establishes level of service standards (LOS) for Motor Vehicle Transportation at a LOS A. Table 3 shows the impact 52,700 square feet of office space has on motor vehicle transportation. The total trips per day generated by this development is 207.

Table 3 – Motor Vehicle Transportation ¹		
Roadway Segment	Level of Service	
US 90/ Duval St From I-75 to SW Bascom	D	
Daily Trip Generation ²	Square Footage	Total Development
Weekday Trips Per 1,000 square feet = 3.93	52,700	207 Trips Per Day
AM Peak Hour Per 1,000 square feet = 0.62	52,700	33 Trips Per Day
PM Peak Hour Per 1,000 square feet = 0.67	52,700	35 Trips Per Day

Source:

- 1) Lake City Comprehensive Plan
- 2) ITE Trip Generation, Manual 10th Edition

Potable Water

The Lake City Comprehensive Plan Capital Improvements Element Policy VIII.1.1 establishes level of service standards (LOS) for Potable Water. Table 4 shows the total gallons per day of potable water is 662.

Table 4 - Potable Water	Units	Gallons Per Day
Warehouse Use	1,124	662
Based off a LOS standard established by Lake City and Florida Department of Health.		

Note: Calculation based on formula for mini warehouse units established by Florida Department of Health Standards of 1 gallon per unit up to 200 units + 1 gallon per unit for each unit over 200.

Sanitary Sewer

The Lake City Comprehensive Plan Capital Improvements Element Policy VIII.1.1 establishes level of service standards (LOS) for Sanitary Sewer. Table 5 shows the total gallons per day of sanitary sewer is 662.

Table 5- Sanitary Sewer	Units	Gallons Per Day
Mini Warehouse Use	1,124	662
Based off a LOS standard established by Lake City and Florida Department of Health.		

Note: Calculation based on formula for mini warehouse units established by Florida Department of Health Standards of 1 gallon per unit up to 200 units + 1 gallon per unit for each unit over 200.

Solid Waste

The Lake City Comprehensive Plan Capital Improvements Element Policy VIII.1.1 establishes level of service standards (LOS) for Sanitary Sewer. Table 4 shows the total tons of solid waste per year is 955.4 for 1,124 units.

Table 6 – Solid Waste	Units	Tons Per Year
Mini Warehouse Use	1,124	955.4
Based off a LOS of .85 tons per year per unit for residential.		

Note: Calculation based on formula for mini warehouse units established by Florida Department of Health Standards

Stormwater

The Lake City Comprehensive Plan Capital Improvements Element Policy VIII.1.1 establishes a level of service standards (LOS) for stormwater not within a stream or open lake watershed. The LOS standard states that such developments shall adhere to the standards as specified in Chapter 62-330(4)(b)2, Florida Administrative Code (Rules of the Florida Department of Environmental Regulation) and Chapter 40B-4, Florida Administrative Code (Rules of the Suwannee River Water Management District).

Recreation

The Lake City Comprehensive Plan Improvements Element Policy VIII.1.1 establishes a level of service standards (LOS) for recreation. As the subject property is part of a nonresidential development with existing facilities, this proposed CPA application does not impact the recreation LOS.

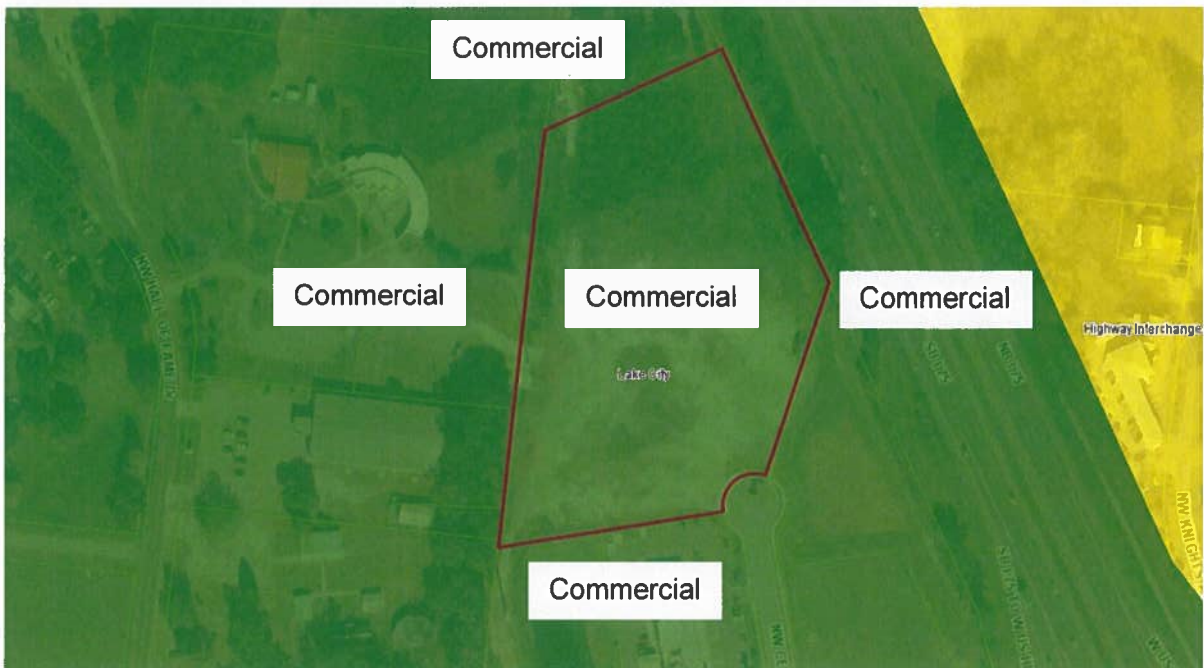
Public School Facilities

The Lake City Comprehensive Plan Improvements Element Policy VIII.1.1 establishes a level of service standards (LOS) for public school facilities. As the subject property is part of a nonresidential development with existing facilities, this proposed CPA application does not impact the public school facilities LOS.

III. Comprehensive Plan Consistency

The proposed project is located within the Commercial Land Use Category (FLU). Developments within this FLU are limited to a density of 1 FAR. As shown on Map 3 below, the adjoining land use categories are Commercial to the north, east, south, and west. The following comprehensive plan consistency assessment shows how this proposed project is consistent with Lake City's adopted comprehensive plan goals, objectives, and policies.

Map 3: Future Land Use Designations



A. Future Land Use Element

Policy I.1.2: The land development regulations of the City shall be based on and be consistent with the following land use classifications and corresponding standards for densities and intensities and shall establish the following floor area ratio(s) to be applied to each classification of land use: ...

COMMERCIAL

Lands classified as commercial use consist of areas used for the sale, rental, and distribution of products or performance of services, as well as public, charter and private elementary, middle and high schools. In addition, off-site signs, churches and other houses of worship, private clubs and lodges, residential dwelling units, which existed within this category on the date of adoption of this objective, and other similar uses compatible with commercial uses may be approved as special exceptions and be subject to an intensity of less than or equal to 0.25 floor area ratio except within the (CG) Commercial, General, (CI) Commercial, Intensive, (C-CBD) Commercial-Central Business District and (CHI) Commercial, Highway Interchange districts being subject to an intensity of less than or equal to 1.0 floor area ratio.

(CN) Commercial, Neighborhood uses shall be limited to an intensity of less than or equal to 0.25 floor area ratio. (CG) Commercial, General, (CI) Commercial, Intensive, (C-CBD) Commercial-Central Business District and (CHI) Commercial, Highway Interchange districts shall be limited to an intensity of less than or equal to 1.0 floor area ratio....

- **Comprehensive Plan Consistency: The proposed use is consistent with the standards established for the Commercial future land use designation.**

Policy I.1.3: The City shall continue to allocate amounts and types of land uses for residential, commercial, industrial, public, and recreation to meet the needs of the existing and projected future populations and to locate urban land uses in a manner where public facilities may be provided to serve such urban land uses. (Urban land uses shall be herein defined as residential, commercial and industrial land use categories).

- **Comprehensive Plan Consistency: The proposed property has available public facilities.**

Objective 1.3: The City shall require that all proposed development be approved only where the public facilities meet or exceed the adopted level of service standard.

- **Comprehensive Plan Consistency: The proposed property has available public facilities.**

Policy I.3.1 The City shall limit the issuance of development orders and permits to areas where the adopted level of service standards for the provision of public facilities found within the Comprehensive Plan are maintained. This provision also includes areas where development orders were issued prior to the adoption of the Comprehensive Plan.

- **Comprehensive Plan Consistency: The proposed property has available public facilities.**

B. Transportation Element

Policy II.1.1 Establish the Service Standards as noted below at peak hour for the following roadway segments within the City as defined within the most recent version of the Florida Department of Transportation Quality/Level of Service Handbook.

- **Comprehensive Plan Consistency: The proposed property meets adopted LOS standards for transportation.**

C. Sanitary Sewer, Solid Waste, Drainage, Potable Water, and Natural Groundwater Aquifer Recharge Element

Goal IV-1 Sanitary Sewer, Solid Waste, Drainage, Potable Water, and Natural Groundwater Aquifer Recharge Goals, Objectives, and Policies. Ensure the provision of public facilities in a timely, orderly, efficient, and environmentally sound manner at an acceptable level of service for the population of the county.

- **Comprehensive Plan Consistency: The provision of public facilities and infrastructure systems for sanitary sewer, solid waste, drainage, potable water, and natural groundwater aquifer recharge is provided according to the adopted comprehensive plan LOS standards for such services and infrastructure systems.**

D. Conservation Element

Policy V.2.5 The County shall, through the development review process, require that post-development runoff rates and pollutant loads do not exceed pre-development conditions.

- **Comprehensive Plan Consistency: As a result of the execution of this site plan and the development of this project, the development will provide for the runoff rates and pollutant loads that are consistent with this comprehensive plan policy.**

IV. Conclusions

The site plan application request is consistent with and serves to implement the Goals, Objectives, and Policies of the Lake City Comprehensive Plan. The request meets all the review criteria and standards for rezoning applications found in the Lake City Land Development Code, including consistency, compatibility, similarity of development patterns in the area of the subject property, suitability, adequacy of public services, access, and promotion of the public health, safety and welfare. The applicant would request approval of the application based upon the demonstrated consistency and implementation of the applicable Plan Goals, Objectives, and Policies as well as the conformance to all applicable provisions of the land development code.

Sonic Drive-In

Site Plan Application

City of Lake City

June 23, 2022

Kathie Ebaugh, AICP
Director of Planning
3530 NW 43rd Street
Gainesville, FL 32606
(352) 375-8999
www.jbpro.com

The logo for JBPro, featuring the letters 'JB' in a large, bold, blue font, followed by 'Pro' in a smaller, blue font with a white outline.



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I. Statement of Proposed Change:

The proposed project is a site plan application for new construction to be located on 1.2 acres near I-75 exit 427. The proposed site plan proposes to build an 1,226 SF restaurant building with associated parking on the following parcel

Parcels:

35-3S-at-02524-103

As shown on Tables 1 and 2 and Maps 1 and 2, the site plan is consistent with the execution of the established land use and zoning designations and is consistent with surrounding uses. The Commercial land use category has an intensity of ≤ 1 FAR and the proposed FAR of .023 meets this standard. The Commercial Highway Interchange (CHI) zoning category is intended to provide for developments that primarily serve the traveling public including fast food restaurants as proposed by this site development plan.

Table 1: Land Use and Zoning

Location	Land Use	Zoning
Proposed Property	Commercial	Commercial Highway Interchange
North	Commercial	Commercial Highway Interchange
South	Commercial	Commercial Highway Interchange
East	Commercial	Commercial Highway Interchange
West	Commercial	Commercial Highway Interchange

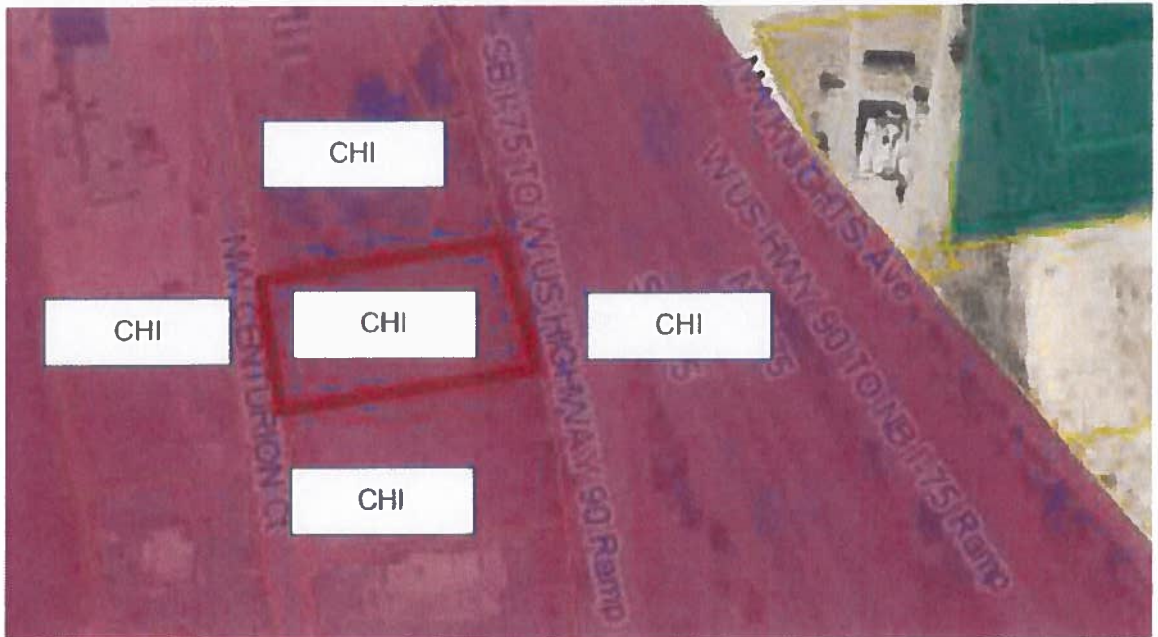
Table 2: Allowable—Proposed Dwelling Units

	Land Use	Zoning
Intensity Standard	1 FAR	1 FAR
Maximum Units Allowed	1 FAR	1 FAR
Proposed Project	.023 FAR	.023 FAR

Map 1: Existing Land Use Designation



Table 3: Zoning Designations





II. Concurrency Impact Analysis

The State of Florida growth management legislation establishes concurrency standards that ensure that local governments adequately provide public facilities to new developments without constraining adopted local levels of service. The following assessment examines how this proposed rezone application impacts public service demands related to transportation, potable water, sanitary sewage, solid waste, stormwater, open space, recreation, and public school facilities.

Transportation Mobility

The Columbia County Comprehensive Plan Transportation Element Objective II.1 establishes level of service standards (LOS) for all roadways.

Table 4: Transportation LOS Impact

Land Use Code	Land Use	SF. GFA	AADT		AM Peak				PM Peak			
			Rate ⁽¹⁾	Trips	Rate ⁽¹⁾	Trips	In	Out	Rate ⁽¹⁾	Trips	In	Out
935	Fast-Food Restaurant with Drive-Through Window and No Indoor Seating	1226	459.2	563	65.81	81	42	39	67.44	83	42	41

(1) - Rate of Vehicle Trip per 1000 Square Feet of Gross Floor Area based on the ITE Trip Generation Manual, 10th Edition, Volume 2

Concurrency Assessment: As shown on Table 4 above the proposed project will generate 65.81 trips per day AM Peak and 67.44 PM Peak. As such it will not place undue demand on the roadway and will maintain its current LOS.

Potable Water

The Columbia County Comprehensive Plan Sanitary Sewer, Solid Waste, Drainage, Portable Water, and Natural Groundwater Aquifer Recharge Element Objective IV.5 establishes LOS for Potable Water. Table 5 below shows that the impact the proposed will have on potable water.

Table 5: Potable Water Impact

System Category	Gallons Per Day
Current Permitted Capacity ⁽¹⁾	4,192,000
Less actual Potable Water Flows ⁽¹⁾	3,400,000
Reserved Capacity	0
Residual Capacity	792,000
Projected Potable Water Demand from Proposed Project ⁽²⁾	1,950
Percentage Utilization Including Proposed Project	81%

(1) Source: City of Lake City Public Services Department; FDEP Permitted Capacity is 9 MGPD, Current SRWMD Capacity is 4.192 MGPD

(2) Source: F.A.C. 64E-6.008, Table 1, Food Operations (f) "Drive-In restaurant car space". 50 GPD per car space. 39 spaces x 50 GPD/space = 1950 GPD

Concurrency Assessment: As shown on Table 5 above the proposed project will generate demand for 1,950 gallons per day. The remaining capacity will be 81%. As such, the County LOS will be maintained so to that it will be able to continue providing for the potable water demands of the community.



Sanitary Sewage

The Columbia County Comprehensive Plan Sanitary Sewer, Solid Waste, Drainage, Portable Water, and Natural Groundwater Aquifer Recharge Element Objective IV.2 LOS for Sanitary Sewage. Table 6 shows that the impact the proposed 1,226 square foot commercial restaurant building will have on sanitary sewerage.

Table 6: Sanitary Sewer Impact

System Category	Gallons Per Day
Current Permitted Capacity ⁽¹⁾	3,000,000
Less actual Treatment Plant Flows ⁽¹⁾	2,530,000
Reserved Capacity	0
Residual Capacity	470,000
Projected Sanitary Sewer Demand from Proposed Project ⁽²⁾	1,950
Percentage Utilization Including Proposed Project	84%

Concurrency Assessment: As shown on Table 6 above the proposed project will generate 1,950 gallons per day. The remaining capacity will be 84%. As such, the County LOS will be maintained so to that it will be able to continue providing for the sanitary sewer demands of the community.

The Columbia County Comprehensive Plan Sanitary Sewer, Solid Waste, Drainage, Portable Water, and Natural Groundwater Aquifer Recharge Element Objective IV.4 establishes LOS for Stormwater. The policy establishes the standard as follows: for all projects which fall totally within a stream, or open lake watershed, detention systems must be installed such that the peak rate of post-development runoff will not exceed the peak-rate of pre-development runoff for storm events up through and including either:

1. A design storm with a 10-year, 24-hour rainfall depth with Soil Conservation Service type II distribution falling on average antecedent moisture conditions for projects serving exclusively agricultural, forest, conservation, or recreational uses; or
2. A design storm with 100-year critical duration rainfall depth for projects serving any land use other than agricultural, silvicultural, conservation, or recreational uses.

Concurrency Assessment: This project has been designed as part of a master stormwater system that was designed to meet a 10-year, 24-hour rainfall depth. As such, the County LOS will be maintained so to that it will be able to continue providing for the stormwater demands the community.



Open Space

The Columbia County Comprehensive Plan Recreation and Open Space Element Objective 5.2 establishes LOS for open space. The standard directs that as applicable and appropriate, open space standards shall be established in the implementing land development code.

Concurrency Assessment: This land development regulations for the CHI zoning classification does not included standards for specific open space aside from the established FAR, buffering standards, and building setbacks. The proposed site plan meets these standards. As such, this concurrency standard is not applicable to this proposed project as there is no impact.

Recreation

The Columbia County Comprehensive Plan Recreation and Open Space Element Objective 1.1 establishes LOS for recreation based on residents to be served. Additionally, Objective VI.3 states this LOS requirements is for new subdivisions or re-subdivisions of land.

Concurrency Assessment: This site plan application is for the development of a commercial property that does not generate new residents. As such, this concurrency standard is not applicable to this proposed project as there is no impact.

Public School Facilities

The Columbia County Comprehensive Plan Recreation and Open Space Element Objective XI.1 establishes LOS for recreation based on number of students and available capacity for educational facilities. Additionally, Objective IX.3 states this LOS requirements is to be applied concurrent with the development of new residential projects.

Concurrency Assessment: This site plan application is for the development of a commercial property that does not generate new student populations. As such, this concurrency standard is not applicable to this proposed project as there is no impact.

III. Comprehensive Plan Consistency

The proposed project is located within the Commercial Land Use Category (FLU). Developments within this FLU are limited to a density of 1 FAR. As shown on Map 4 below, the adjoining land use categories are Commercial to the north, east, south, and west. The following comprehensive plan consistency



assessment shows how this proposed project is consistent with Columbia County's adopted comprehensive plan goals, objectives, and policies.

Map 4: Future Land Use Map



Future Land Use Element

Goal 1: Future Land Use. In recognition of the importance of conserving the natural resources and enhancing the quality of life, the county shall direct development to those areas which have in place, or have agreements to prove, the land and water resources, fiscal abilities, and service capacity to accommodate growth in an environmentally acceptable manner.

Comprehensive Plan Consistency: The location of this proposed site plan application is within an established development area where there is the funding and infrastructure capacity to provide for the demands of the proposed development.

Objective 1.2: Urban Development Areas. The County shall continue to direct future population growth and associated urban development to urban development areas as established within this Comprehensive Plan.

Comprehensive Plan Consistency: The location of this proposed site plan is consistent with the County's urban development areas.



Policy I.1.1: Public Facility Availability. The County shall limit the location of higher density residential and high intensity commercial and industrial uses to areas adjacent to arterial or collector roads where public facilities are available to support such higher density or intensity

Comprehensive Plan Consistency: The location of this proposed development with an I75 Interchange area is consistent with the policy to locate new development in areas that have the public facilities and infrastructure needed to support higher intensities.

Policy I.1.5 Development—Public Facility Coordinated Locations. The County shall continue to regulate govern future urban development within designated urban development areas in conformance with the land topography and soil conditions, and within an area which is or will be served by public facilities and services.

Comprehensive Plan Consistency: The location of this proposed site plan is able to be served by public facilities and services consistent with this policy.

Policy I.1.6 Land Use Classifications. The County's land development regulations shall be based on and be consistent with the following land use classifications and corresponding standards for densities and intensities within the designated urban development areas of the County. For the purpose of this policy and Comprehensive Plan, the phrase "other similar uses compatible with" shall mean land uses that can co-exist in relative proximity to other uses in a stable fashion over time such that no other uses within the same land use classification are negatively impacted directly or indirectly by the use....

COMMERCIAL LAND USE

Highway interchange uses shall be permitted within the urban and rural area of the County.

Highway interchange uses shall be permitted within areas surrounding Interstates 75 and 10, which shall be limited to the following:

1. Tourist oriented facilities, such as restaurants, automotive service stations, motels and campgrounds;
2. Retail outlets;
3. Truck stops;
4. Light manufacturing, assembling, processing, packaging or fabricating in completely enclosed building; and
5. Facilities for the storage and distribution of foods and products including wholesale activity.

Commercial uses shall be limited to an intensity of 1.0 floor area ratio.

Comprehensive Plan Consistency: The development of this property is consistent with the future land use requires for the development of residential properties in general and the Commercial FLU category in specific.

Objective I.3 Compatibility of Adjacent Land Uses: The County shall include within the site plan review process to be adopted as part of the land development regulations, that adjacent land uses shall not be adversely impacted by any change in land use.



Comprehensive Plan Consistency: The proposed site plan is located in an area that is compatible with highway interchange commercial uses consistent with this policy.

OBJECTIVE I.11 Public Facilities and Developable Land: The County shall require that proposed development be approved only where the public facilities meet or exceed the adopted level of service standard.

Comprehensive Plan Consistency: The location of this proposed development is an area the County is able to provide public services consistent with this policy.

Policy I.11.1 Level of Service Standards The County shall establish procedures for the review of proposed development to determine its impact on level of service standards for public facilities so that such public facilities will meet the County's level of service standards and are available concurrently with the impacts of development.

Comprehensive Plan Consistency: As proven be the Concurrence Analysis, the development of this property is consistent with establish LOS standards and the proposed impacts do not unduly impact the ability for the County to provide public infrastructure facilities and services.

Policy I.12.1 Land Development Standards and Regulations. The County's land development regulations shall contain specific and detailed provisions to manage future growth and development to implement the Comprehensive Plan which shall contain at a minimum the following provisions to:

1. Regulate the subdivision of land;
2. Regulate the use of land and water consistent with this Element and ensure the compatibility of adjacent land uses and provide for open space;
3. Protect environmentally sensitive lands identified within the Conservation Element;
4. Regulate areas subject to seasonal and periodic flooding and provide for drainage and stormwater management;
5. Protect potable water wellfields and aquifer recharge areas;
6. Regulate signage;
7. Ensure safe and convenient onsite traffic flow and vehicle parking needs; and
8. Provide that development orders and permits shall not be issued which result in a reduction of the level of service standards adopted in this Comprehensive Plan

Comprehensive Plan Consistency: As shown through this application and attached proposed site development plan, the proposal to development this site is consistent with the County's adopted land development standards and regulations.

Transportation Element

Policy II.1.1 Establish the Service. Standards as noted below at peak hour for the following roadway segments within the County as defined within the most recent version of the Florida Department of Transportation Quality/Level of Service Handbook.



Comprehensive Plan Consistency: The provision of roadway services is provided according to the adopted comprehensive plan LOS standards for such services and infrastructure systems.

OBJECTIVE II.2 Traffic Circulation System. The County shall require that all traffic circulation system improvements be consistent with the land uses shown on the future land use plan map by limiting higher density and higher intensity land use locations to be adjacent to collector or arterial roads.

Comprehensive Plan Consistency: The location of this development in a highway interchange area near I75 is consistent with the County policy to provide for higher intensity development areas adjacent to major roadways.

Sanitary Sewer, Solid Waste, Drainage, Potable Water, and Natural Groundwater Aquifer Recharge Element

Goal IV-1 Sanitary Sewer, Solid Waste, Drainage, Potable Water, and Natural Groundwater Aquifer Recharge Goals, Objectives, and Policies. Ensure the provision of public facilities in a timely, orderly, efficient, and environmentally sound manner at an acceptable level of service for the population of the county.

Comprehensive Plan Consistency: The provision of public facilities and infrastructure systems for sanitary sewer, solid waste, drainage, potable water, and natural groundwater aquifer recharge is provided according to the adopted comprehensive plan LOS standards for such services and infrastructure systems.

Conservation Element

Policy V.2.5 Runoff Standards. The County shall, through the development review process, require that post-development runoff rates and pollutant loads do not exceed pre-development conditions.

Comprehensive Plan Consistency: As a result of the execution of this site plan and the development of this project, the development will provide for the runoff rates and pollutant loads that are consistent with this comprehensive plan policy.

IV. Conclusions:

The site plan application request is consistent with and serves to implement the Goals, Objectives and Policies of the Columbia County Comprehensive Plan. The request meets all of the review criteria and standards for rezoning applications found in the Columbia County Land Development Code, including consistency, compatibility, similarity of development patterns in the area of the subject property, suitability, adequacy of public services, access, and promotion of the public health, safety and welfare. The applicant would request approval of the application based upon the demonstrated consistency and



implementation of the applicable Plan Goals, Objectives and Policies as well as the conformance to all applicable provisions of the land development code.



Rib City

Site Plan Application

City of Lake City

June 28, 2022

Kathie Ebaugh, AICP
Director of Planning
3530 NW 43rd Street
Gainesville, FL 32606
(352) 375-8999
www.jbpro.com

JBPro



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I. **Statement of Proposed Change:**

The proposed project is a site plan application for new construction to be located on 1.064 acres near I-75 exit 427. The proposed site plan proposes to build a 3,428 SF restaurant building with associated parking and utilities on the following parcel

Parcels:

35-3S-16-02524-008

As shown on Tables 1 and 2 and Maps 1 and 2, the site plan is consistent with the execution of the established land use and zoning designations and is consistent with surrounding uses. The Commercial land use category has an intensity of ≤ 1 FAR and the proposed FAR of .07 meets this standard. The Commercial Highway Interchange (CHI) zoning category is intended to provide for developments that primarily serve the traveling public including fast food restaurants as proposed by this site development plan.

Table 1: Land Use and Zoning

Location	Land Use	Zoning
Proposed Property	Commercial	Commercial Highway Interchange
North	Commercial	Commercial Highway Interchange
South	Commercial	Commercial Highway Interchange
East	Commercial	Commercial Highway Interchange
West	Commercial	Commercial Highway Interchange

Table 2: Allowable—Proposed Dwelling Units

	Land Use	Zoning
Intensity Standard	1 FAR	1 FAR
Maximum Units Allowed	1 FAR	1 FAR
Proposed Project	.07 FAR	.07 FAR

Map 1: Existing Land Use Designation



Map 2: Zoning Designations





II. Concurrency Impact Analysis

The State of Florida growth management legislation establishes concurrency standards that ensure that local governments adequately provide public facilities to new developments without constraining adopted local levels of service. The following assessment examines how this proposed rezone application impacts public service demands related to transportation, potable water, sanitary sewage, solid waste, stormwater, open space, recreation, and public school facilities.

Transportation Mobility

The Columbia County Comprehensive Plan Transportation Element Objective II.1 establishes level of service standards (LOS) for all roadways.

Table 4: Transportation LOS Impact

Land Use Code	Land Use	SF GFA	AADT		AM Peak			PM Peak				
			Rate ⁽¹⁾	Trips	Rate ⁽¹⁾	Trips	In	Out	Rate ⁽¹⁾	Trips	In	Out
930	Fast Casual	3428	315.17	1080	36.21	124	77	47	43.79	150	69	81

(1) - Rate of Vehicle Trip per 1000 Square Feet of Gross Floor Area based on the ITE Trip Generation Manual, 10th Edition, Volume 2

Concurrency Assessment: As shown on Table 4 above the proposed project will generate 1080 trips per day including 124 trips per day AM Peak and 150 PM Peak. As such it will not place undue demand on the roadway and will maintain its current LOS.

Potable Water

The Columbia County Comprehensive Plan Sanitary Sewer, Solid Waste, Drainage, Potable Water, and Natural Groundwater Aquifer Recharge Element Objective IV.5 establishes LOS for Potable Water. Table 5 below shows that the impact the proposed will have on potable water.

Table 5: Potable Water Impact

System Category	Gallons Per Day
Current Permitted Capacity ⁽¹⁾	4,192,000
Less actual Potable Water Flows ⁽¹⁾	3,400,000
Reserved Capacity	0
Residual Capacity	792,000
Projected Potable Water Demand from Proposed Project ⁽²⁾	3,560
Percentage Utilization Including Proposed Project	81%

(1) Source: City of Lake City Public Services Department; FDEP Permitted Capacity is 9 MGD, Current SRWMD Capacity is 4.192 MGD

(2) Source: F.A.C. 64E-6.008, Table 1, Food Operations (a) "Restaurant operating 16 hours or less per day per seat". 89 Seats x 40 GPD/seat = 3560 GPD

Concurrency Assessment: As shown on Table 5 above the proposed project will generate demand for 3,560 gallons per day. The remaining capacity will be 19%. As such, the County LOS will be maintained so to that it will be able to continue providing for the potable water demands of the community.



Sanitary Sewage

The Columbia County Comprehensive Plan Sanitary Sewer, Solid Waste, Drainage, Portable Water, and Natural Groundwater Aquifer Recharge Element Objective IV.2 LOS for Sanitary Sewage. Table 6 shows that the impact the proposed 1,226 square foot commercial restaurant building will have on sanitary sewerage.

Table 6: Sanitary Sewer Impact

System Category	Gallons Per Day
Current Permitted Capacity ⁽¹⁾	3,000,000
Less actual Treatment Plant Flows ⁽¹⁾	2,530,000
Reserved Capacity	0
Residual Capacity	470,000
Projected Sanitary Sewer Demand from Proposed Project ⁽²⁾	1,950
Percentage Utilization Including Proposed Project	84%

(1) Source: City of Lake City Public Services Department; FDEP Permitted Capacity is 9 MGD, Current SRWMD Capacity is 4.192 MGD

(2) Source: F.A.C. 64E-6.008, Table 1, Food Operations (a) "Restaurant operating 16 hours or less per day per seat". 89 Seats x 40 GPD/seat = 3560 GPD

Concurrency Assessment: As shown on Table 6 above the proposed project will generate 1,950 gallons per day. The remaining capacity will be 16%. As such, the County LOS will be maintained so to that it will be able to continue providing for the sanitary sewer demands of the community.

The Columbia County Comprehensive Plan Sanitary Sewer, Solid Waste, Drainage, Portable Water, and Natural Groundwater Aquifer Recharge Element Objective IV.4 establishes LOS for Stormwater. The policy establishes the standard as follows: for all projects which fall totally within a stream, or open lake watershed, detention systems must be installed such that the peak rate of post-development runoff will not exceed the peak-rate of pre-development runoff for storm events up through and including either:

1. A design storm with a 10-year, 24-hour rainfall depth with Soil Conservation Service type II distribution falling on average antecedent moisture conditions for projects serving exclusively agricultural, forest, conservation, or recreational uses; or
2. A design storm with 100-year critical duration rainfall depth for projects serving any land use other than agricultural, silvicultural, conservation, or recreational uses.

Concurrency Assessment: This project has been designed as part of a master stormwater system that was designed to meet a 10-year, 24-hour rainfall depth. As such, the County LOS will be maintained so to that it will be able to continue providing for the stormwater demands the community.



Open Space

The Columbia County Comprehensive Plan Recreation and Open Space Element Objective 5.2 establishes LOS for open space. The standard directs that as applicable and appropriate, open space standards shall be established in the implementing land development code.

Concurrency Assessment: This land development regulations for the CHI zoning classification does not included standards for specific open space aside from the established FAR, buffering standards, and building setbacks. The proposed site plan meets these standards. As such, this concurrency standard is not applicable to this proposed project as there is no impact.

Recreation

The Columbia County Comprehensive Plan Recreation and Open Space Element Objective 1.1 establishes LOS for recreation based on residents to be served. Additionally, Objective VI.3 states this LOS requirements is for new subdivisions or re-subdivisions of land.

Concurrency Assessment: This site plan application is for the development of a commercial property that does not generate new residents. As such, this concurrency standard is not applicable to this proposed project as there is no impact.

Public School Facilities

The Columbia County Comprehensive Plan Recreation and Open Space Element Objective XI.1 establishes LOS for recreation based on number of students and available capacity for educational facilities. Additionally, Objective IX.3 states this LOS requirements is to be applied concurrent with the development of new residential projects.

Concurrency Assessment: This site plan application is for the development of a commercial property that does not generate new student populations. As such, this concurrency standard is not applicable to this proposed project as there is no impact.

III. Comprehensive Plan Consistency

The proposed project is located within the Commercial Land Use Category (FLU). Developments within this FLU are limited to a density of 1 FAR. As shown on Map 4 below, the adjoining land use categories are Commercial to the north, east, south, and west. The following comprehensive plan consistency assessment shows how this proposed project is consistent with Columbia County's adopted comprehensive plan goals, objectives, and policies.

Map 4: Future Land Use Map



Future Land Use Element

Goal 1: Future Land Use. In recognition of the importance of conserving the natural resources and enhancing the quality of life, the county shall direct development to those areas which have in place, or have agreements to prove, the land and water resources, fiscal abilities, and service capacity to accommodate growth in an environmentally acceptable manner.

Comprehensive Plan Consistency: The location of this proposed site plan application is within an established development area where there is the funding and infrastructure capacity to provide for the demands of the proposed development.

Objective 1.2: Urban Development Areas. The County shall continue to direct future population growth and associated urban development to urban development areas as established within this Comprehensive Plan.

Comprehensive Plan Consistency: The location of this proposed site plan is consistent with the County’s urban development areas.

Policy I.1.1: Public Facility Availability. The County shall limit the location of higher density residential and high intensity commercial and industrial uses to areas adjacent to arterial or collector roads where public facilities are available to support such higher density or intensity



Comprehensive Plan Consistency: The location of this proposed development with an I75 Interchange area is consistent with the policy to locate new development in areas that have the public facilities and infrastructure needed to support higher intensities.

Policy I.1.5 Development—Public Facility Coordinated Locations. The County shall continue to regulate govern future urban development within designated urban development areas in conformance with the land topography and soil conditions, and within an area which is or will be served by public facilities and services.

Comprehensive Plan Consistency: The location of this proposed site plan is able to be served by public facilities and services consistent with this policy.

Policy I.1.6 Land Use Classifications. The County's land development regulations shall be based on and be consistent with the following land use classifications and corresponding standards for densities and intensities within the designated urban development areas of the County. For the purpose of this policy and Comprehensive Plan, the phrase "other similar uses compatible with" shall mean land uses that can co-exist in relative proximity to other uses in a stable fashion over time such that no other uses within the same land use classification are negatively impacted directly or indirectly by the use...

COMMERCIAL LAND USE

Highway interchange uses shall be permitted within the urban and rural area of the County.

Highway interchange uses shall be permitted within areas surrounding Interstates 75 and 10, which shall be limited to the following:

1. Tourist oriented facilities, such as restaurants, automotive service stations, motels and campgrounds;
2. Retail outlets;
3. Truck stops;
4. Light manufacturing, assembling, processing, packaging or fabricating in completely enclosed building; and
5. Facilities for the storage and distribution of foods and products including wholesale activity.

Commercial uses shall be limited to an intensity of 1.0 floor area ratio.

Comprehensive Plan Consistency: The development of this property is consistent with the future land use requires for the development of residential properties in general and the Commercial FLU category in specific.

Objective I.3 Compatibility of Adjacent Land Uses: The County shall include within the site plan review process to be adopted as part of the land development regulations, that adjacent land uses shall not be adversely impacted by any change in land use.

Comprehensive Plan Consistency: The proposed site plan is located in an area that is compatible with highway interchange commercial uses consistent with this policy.



OBJECTIVE I.11 Public Facilities and Developable Land: The County shall require that proposed development be approved only where the public facilities meet or exceed the adopted level of service standard.

Comprehensive Plan Consistency: The location of this proposed development is an area the County is able to provide public services consistent with this policy.

Policy I.11.1 Level of Service Standards The County shall establish procedures for the review of proposed development to determine its impact on level of service standards for public facilities so that such public facilities will meet the County's level of service standards and are available concurrently with the impacts of development.

Comprehensive Plan Consistency: As proven by the Concurrency Analysis, the development of this property is consistent with establish LOS standards and the proposed impacts do not unduly impact the ability for the County to provide public infrastructure facilities and services.

Policy I.12.1 Land Development Standards and Regulations. The County's land development regulations shall contain specific and detailed provisions to manage future growth and development to implement the Comprehensive Plan which shall contain at a minimum the following provisions to:

1. Regulate the subdivision of land;
2. Regulate the use of land and water consistent with this Element and ensure the compatibility of adjacent land uses and provide for open space;
3. Protect environmentally sensitive lands identified within the Conservation Element;
4. Regulate areas subject to seasonal and periodic flooding and provide for drainage and stormwater management;
5. Protect potable water wellfields and aquifer recharge areas;
6. Regulate signage;
7. Ensure safe and convenient onsite traffic flow and vehicle parking needs; and
8. Provide that development orders and permits shall not be issued which result in a reduction of the level of service standards adopted in this Comprehensive Plan

Comprehensive Plan Consistency: As shown through this application and attached proposed site development plan, the proposal to development this site is consistent with the County's adopted land development standards and regulations.

Transportation Element

Policy II.1.1 Establish the Service. Standards as noted below at peak hour for the following roadway segments within the County as defined within the most recent version of the Florida Department of Transportation Quality/Level of Service Handbook.

Comprehensive Plan Consistency: The provision of roadway services is provided according to the adopted comprehensive plan LOS standards for such services and infrastructure systems.



OBJECTIVE II.2 Traffic Circulation System. The County shall require that all traffic circulation system improvements be consistent with the land uses shown on the future land use plan map by limiting higher density and higher intensity land use locations to be adjacent to collector or arterial roads.

Comprehensive Plan Consistency: The location of this development in a highway interchange area near I75 is consistent with the County policy to provide for higher intensity development areas adjacent to major roadways.

Sanitary Sewer, Solid Waste, Drainage, Potable Water, and Natural Groundwater Aquifer Recharge Element

Goal IV-1 Sanitary Sewer, Solid Waste, Drainage, Potable Water, and Natural Groundwater Aquifer Recharge Goals, Objectives, and Policies. Ensure the provision of public facilities in a timely, orderly, efficient, and environmentally sound manner at an acceptable level of service for the population of the county.

Comprehensive Plan Consistency: The provision of public facilities and infrastructure systems for sanitary sewer, solid waste, drainage, potable water, and natural groundwater aquifer recharge is provided according to the adopted comprehensive plan LOS standards for such services and infrastructure systems.

Conservation Element

Policy V.2.5 Runoff Standards. The County shall, through the development review process, require that post-development runoff rates and pollutant loads do not exceed pre-development conditions.

Comprehensive Plan Consistency: As a result of the execution of this site plan and the development of this project, the development will provide for the runoff rates and pollutant loads that are consistent with this comprehensive plan policy.

IV. Conclusions:

The site plan application request is consistent with and serves to implement the Goals, Objectives and Policies of the Columbia County Comprehensive Plan. The request meets all of the review criteria and standards for rezoning applications found in the Columbia County Land Development Code, including consistency, compatibility, similarity of development patterns in the area of the subject property, suitability, adequacy of public services, access, and promotion of the public health, safety and welfare. The applicant would request approval of the application based upon the demonstrated consistency and implementation of the applicable Plan Goals, Objectives and Policies as well as the conformance to all applicable provisions of the land development code.

COMPOSITE EXHIBIT

I

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION
**DRIVEWAY CONNECTION PERMIT
FOR ALL CATEGORIES**

PART 1: PERMIT INFORMATION

APPLICATION NUMBER: 2022-A-292-00008

Permit Category: H - Safety Upgrade Access Classification: _____

Project: Circle K expansion

Permittee: JAROD STUBBS

Section/Mile Post: / State Road: _____

Section/Mile Post: / State Road: _____

PART 2: PERMITTEE INFORMATION

Permittee Name: JAROD STUBBS

Permittee Mailing Address: 189 South Orange Ave, Suite 1000

City, State, Zip: Orlando, Florida 32801

Telephone: (407) 409-7002 ext. _____

Engineer/Consultant/or Project Manager: _____

Engineer responsible for construction inspection: _____
NAME P.E. #

Mailing Address: _____

City, State, Zip: _____

Telephone: _____ FAX, Mobile Phone, etc. Fax: / Mobile: _____

PART 3: PERMIT APPROVAL

The above application has been reviewed and is hereby approved subject to all Provisions as attached.

Permit Number: 2022-A-292-00008
Department of Transportation

Signature: Troy Register Title: MAINTENANCE MANAGER/PERMITS

Department Representative's Printed Name Troy Register

Temporary Permit YES NO (If temporary, this permit is only valid for 6 months)

Special provisions attached YES NO

Date of Issuance: 5/18/2022

If this is a normal (non-temporary) permit it authorizes construction for one year from the date of issuance. This can only be extended by the Department as specified in 14-96.007(6).

Approved
 2022-A-292-00008
 Troy Register
 5/18/2022

See following pages for General and Special Provisions

PART 4: GENERAL PROVISIONS

1. Notify the Department of Transportation Maintenance Office at least 48 hours in advance of starting proposed work.
Phone: 3869617153 , Attention: Troy Register
2. A copy of the approved permit must be displayed in a prominent location in the immediate vicinity of the connection construction.
3. Comply with Rule 14-96.008(1), F.A.C., Disruption of Traffic.
4. Comply with Rule 14-96.008(7), F.A.C., on Utility Notification Requirements.
5. All work performed in the Department's right of way shall be done in accordance with the most current Department standards, specifications and the permit provisions.
6. The permittee shall not commence use of the connection prior to a final inspection and acceptance by the Department.
7. Comply with Rule 14-96.003(3)(a), F.A.C., Cost of Construction.
8. If a Significant Change of the permittee's land use, as defined in Section 335.182, Florida Statutes, occurs, the Permittee must contact the Department.
9. Medians may be added and median openings may be changed by the Department as part of a Construction Project or Safety Project. The provision for a median might change the operation of the connection to be for right turns only.
10. All conditions in NOTICE OF INTENT WILL APPLY unless specifically changed by the Department.
11. All approved connection(s) and turning movements are subject to the Department's continuing authority to modify such connection(s) or turning movements in order to protect safety and traffic operations on the state highway or State Highway System.
12. **Transportation Control Features and Devices in the State Right of Way.** Transportation control features and devices in the Department's right of way, including, but not limited to, traffic signals, medians, median openings, or any other transportation control features or devices in the state right of way, are operational and safety characteristics of the State Highway and are not means of access. The Department may install, remove or modify any present or future transportation control feature or device in the state right of way to make changes to promote safety in the right of way or efficient traffic operations on the highway.
13. The Permittee for him/herself, his/her heirs, his/her assigns and successors in interest, binds and is bound and obligated to save and hold the State of Florida, and the Department, its agents and employees harmless from any and all damages, claims, expense, or injuries arising out of any act, neglect, or omission by the applicant, his/her heirs, assigns and successors in interest that may occur by reason of this facility design, construction, maintenance, or continuing existence of the connection facility, except that the applicant shall not be liable under this provision for damages arising from the sole negligence of the Department.
14. The Permittee shall be responsible for determining and notify all other users of the right of way.
15. Starting work on the State Right of Way means that I am accepting all conditions on the Permit.

Approved
2022-A-292-00008
Troy Register
5/18/2022

PART 5: SPECIAL PROVISIONSNON-CONFORMING CONNECTIONS: YES NO

If this is a non-conforming connection permit, as defined in Rule Chapters 14-96 and 14-97, then the following shall be a part of this permit.

1. The non-conforming connection(s) described in this permit is (are) not permitted for traffic volumes exceeding the Permit Category on page 1 of this permit, or as specified in "Other Special Provisions" below.
2. All non-conforming connections will be subject to closure or relocation when reasonable access becomes available in the future.

OTHER SPECIAL PROVISIONS:

Pre construction meeting requested. 48hr notification required before work in FDOT R/W begins.

PART 6: APPEAL PROCEDURES

You may petition for an administrative hearing pursuant to sections 120.569 and 120.57, Florida Statutes. If you dispute the facts stated in the foregoing Notice of Intended Department Action (hereinafter Notice), you may petition for a formal administrative hearing pursuant to section 120.57 (1), Florida Statutes. If you agree with the facts stated in the Notice, you may petition for an informal administrative hearing pursuant to section 120.57(2), Florida Statutes. You must file the petition with:

Clerk of Agency Proceedings
Department of Transportation
Haydon Burns Building
605 Suwannee Street, M.S. 58
Tallahassee, Florida 32399-0458

The petition for an administrative hearing must conform to the requirements of Rule 28-106.201(2) or Rule 28-106.301(2), Florida Administrative Code, and be filed with the Clerk of Agency Proceedings by 5:00 p.m. no later than 21 days after you received the Notice. The petition must include a copy of the Notice, be legible, on 8 1/2 by 11 inch white paper, and contain:

1. Your name, address, telephone number, any Department of Transportation identifying number on the Notice, if known, the name and identification number of each agency affected, if known, and the name, address, and telephone number of your representative, if any, which shall be the address for service purposes during the course of the proceeding.
2. An explanation of how your substantial interests will be affected by the action described in the Notice;
3. A statement of when and how you received the Notice;
4. A statement of all disputed issues of material fact. If there are none, you must so indicate;
5. A concise statement of the ultimate facts alleged, including the specific facts you contend warrant reversal or modification of the agency's proposed action, as well as an explanation of how the alleged facts relate to the specific rules and statutes you contend require reversal or modification of the agency's proposed action;
6. A statement of the relief sought, stating precisely the desired action you wish the agency to take in respect to the agency's proposed action.

If there are disputed issues of material fact a formal hearing will be held, where you may present evidence and argument on all issues involved and conduct cross-examination. If there are no disputed issues of material fact an informal hearing will be held, where you may present evidence or a written statement for consideration by the Department.

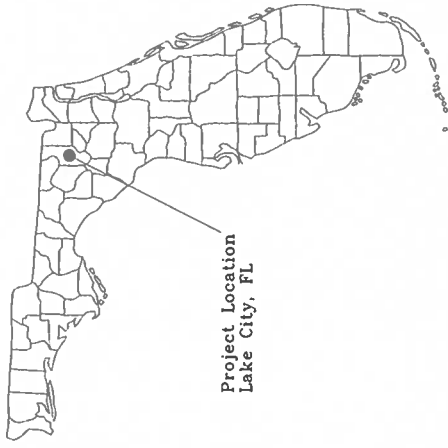
Mediation, pursuant to section 120.573, Florida Statutes, may be available if agreed to by all parties, and on such terms as may be agreed upon by all parties. The right to an administrative hearing is not affected when mediation does not result in a settlement.

Your petition for an administrative hearing shall be dismissed if it is not in substantial compliance with the above requirements of Rule 28-106.201(2) or Rule 28-106.301(2), Florida Administrative Code. If you fail to timely file your petition in accordance with the above requirements, you will have waived your right to have the intended action reviewed pursuant to chapter 120, Florida Statutes, and the action set forth in the Notice shall be conclusive and final.

Approved
000008
Troy Register
5/18/2022

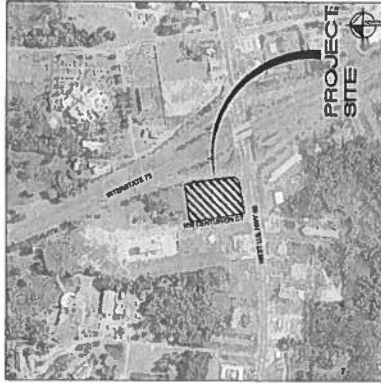
CONSTRUCTION PLANS FOR CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION

143 NW CENTURION COURT
LAKE CITY, FLORIDA 32055
MAY 4, 2022
PARCEL IDS: 35-3S-16-02524-001,
35-3S-16-02524-102 AND 35-3S-16-02524-111



LEGAL DESCRIPTION

LOT 1 AND 2, PART OF THE CITY OF LAKE CITY, FLORIDA, COMMUNITY DEVELOPMENT, PARCELS 35-3S-16-02524-001, 35-3S-16-02524-102 AND 35-3S-16-02524-111, ACCORDING TO PLAT NUMBER RECORDED IN PLAT BOOK 5, PLAT 111, PAGE 102223-29, COUNTY OF SUWANNEE, FLORIDA.



1" = 500'

UTILITY PROVIDERS

WATERSEWER:
CITY OF LAKE CITY UTILITIES
1400 W. WINDYBROOK BLVD
LAKE CITY, FL 32055
CONTACT: ANDREW BWEENEY
PHONE: (813) 751-1311

ELECTRIC:
CITY OF LAKE CITY
2118 W. SANDS HOPKINS DRIVE
LAKE CITY, FL 32055
CONTACT: JAMES HILBANK
PHONE: (813) 751-1311

FIBER OPTIC:
HIGHWAY OF FLORIDA, INC.
1000 W. WINDYBROOK BLVD
LAKE CITY, FL 32055
CONTACT: EDWARD HARRING
PHONE: (813) 751-1311

OWNER:
GWC DEVELOPMENT PARTNERS LLC
5822 W. PINNACEL ROAD
LAKE CITY, FL 32055
CONTACT: DIANE BERRY
PHONE: (407) 560-5173
EMAIL: DBERRY@SCHAFERCONST.COM

DEVELOPER:
CIRCLE K STORES, INC.
3602 CONFOREX PARK DRIVE, SUITE 413
TAMPA, FL 33611
CONTACT: EDWARD OLUNTA
PHONE: (407) 560-5173

VICINITY MAP
PROJECT TEAM
CIVIL ENGINEER:
KIMLEY-HORN AND ASSOCIATES, INC.
188 SOUTH ORANGE AVENUE, SUITE 1000
ORLANDO, FL 32801
CONTACT: JAROD C. STUBBS, P.E.
PHONE: (407) 409-7002
EMAIL: JAROD.STUBBS@KIMLEY-HORN.COM

ARCHITECT:
RDC COLLABORATIVE
11821 FREEDOM DRIVE, SUITE #1110
RESTON, VA 20190
CONTACT: JEFFREY JURGENT
PHONE: (703) 668-0086
FAX: (703) 668-0085

SURVEYOR:
JBPRO
2530 NW 43RD STREET
LAKE CITY, FL 32055
CONTACT: TROY V. WRIGHT
PHONE: (352) 375-8999

LANDSCAPE ARCHITECT:
KIMLEY-HORN AND ASSOCIATES, INC.
188 SOUTH ORANGE AVENUE, SUITE 1000
ORLANDO, FL 32801
CONTACT: MATT FRANKO
PHONE: (407) 427-1829
EMAIL: MATT.FRANKO@KIMLEY-HORN.COM

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Digitally signed by Jarod C. Stubbs, P.E. Date: 2022.05.04 08:51:23 -0400

JAROD C. STUBBS, P.E.
FL. P.E. # 14987
05/04/2022

PREPARED BY
Kimley»Horn
180 S. GORHAM AVE., SUITE 1000, TAMPA, FL 33606
PHONE: 407-898-1311
WWW.KIMLEY-HORN.COM REGISTRY NO. 33198

No.	REVISIONS	DATE

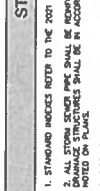
<p>GENERAL</p> <ol style="list-style-type: none"> LOCATIONS, ELEVATIONS, AND DIMENSIONS OF EXISTING UTILITIES, STRUCTURES AND OTHER FEATURES ARE SHOWN ACCORDING TO THE BEST INFORMATION AVAILABLE AT THE TIME OF PREPARATION OF THESE PLANS. THE EXISTING UTILITY LOCATION SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL VERIFY ANY EXISTING UTILITIES INCLUDING GAS, WATER, ELECTRIC, AND TELEPHONE. THE CONTRACTOR SHALL VERIFY ANY EXISTING UTILITIES INCLUDING GAS, WATER, ELECTRIC, AND TELEPHONE. THE CONTRACTOR SHALL VERIFY ANY EXISTING UTILITIES INCLUDING GAS, WATER, ELECTRIC, AND TELEPHONE. THE CONTRACTOR SHALL VERIFY ANY EXISTING UTILITIES INCLUDING GAS, WATER, ELECTRIC, AND TELEPHONE. 	<p>PAVING, GRADING AND DRAINAGE</p> <ol style="list-style-type: none"> ALL PAVING SHALL BE PERFORMED IN ACCORDANCE WITH THE FLORIDA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION. ALL EXISTING SUBSURFACE MATERIAL (SIC, MUCK, REAL BARED DEBRIS, ETC.) IS TO BE EXCAVATED AND REPLACED WITH SAND/GRAVELL COMPACTED SOILS AS DIRECTED BY THE GEOTECHNICAL ENGINEER OF RECORD. SOLIDIFIED MATERIAL IS TO BE EXCAVATED AND REPLACED WITH SAND/GRAVELL COMPACTED SOILS AS DIRECTED BY THE GEOTECHNICAL ENGINEER OF RECORD. 	<p>DRAINAGE SYSTEM TESTING AND INSPECTION</p> <ol style="list-style-type: none"> THE CONTRACTOR SHALL MAINTAIN AND PROTECT FROM HITS, DRIFT, DEBRIS, ETC. THE STORM DRAINAGE SYSTEM UNTIL APPROVAL OF CONTRACTOR OF RECORD BY THE CITY ENGINEER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING AND REPLACING ALL DAMAGE TO THE DRAINAGE SYSTEM CAUSED BY THE CONTRACTOR'S OPERATIONS AND EQUIPMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING AND REPLACING ALL DAMAGE TO THE DRAINAGE SYSTEM CAUSED BY THE CONTRACTOR'S OPERATIONS AND EQUIPMENT. 	<p>DEWATERING NOTES</p> <ol style="list-style-type: none"> DURING THE EXCAVATION OF THE STORMWATER FACILITIES, AND IF GROUNDWATER IS ENCOUNTERED, THE CONTRACTOR SHALL MAINTAIN A DRY WORK AREA. THE CONTRACTOR SHALL MAINTAIN A DRY WORK AREA. THE CONTRACTOR SHALL MAINTAIN A DRY WORK AREA. THE CONTRACTOR SHALL MAINTAIN A DRY WORK AREA.
<p>STORM DRAINAGE SYSTEM</p> <ol style="list-style-type: none"> STANDARD NOTES REFER TO THE 2001 EDITION OF F.D.A.T. "ROADWAY AND TRAFFIC DESIGN STANDARD". ALL STORM SEWER PIPE SHALL BE REINFORCED CONCRETE CLASS B (ASTM C-76) UNLESS OTHERWISE NOTED ON PLANS. ALL STORM SEWER PIPE SHALL BE REINFORCED CONCRETE CLASS B (ASTM C-76) UNLESS OTHERWISE NOTED ON PLANS. PIPE LENGTHS SHALL BE APPROXIMATE AND TO CENTER OF DRAINAGE STRUCTURES, WITH THE EXCEPTION OF WETTED END AND FLARED END SECTIONS, WHICH ARE NOT INCLUDED IN LENGTHS. ALL DRAINAGE STRUCTURE DATES AND COVERS, OTHER EXISTING OR PROPOSED SHALL BE TRAFFIC RATED FOR 10-20 TONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY NECESSARY UPGRADES TO EXISTING DRAINAGE STRUCTURES. ALL DRAINAGE STRUCTURES SHALL BE CONSTRUCTED AND FINISHED TO THE ELEVATION AND FINISH GRADE SPECIFIED IN THE PLANS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION AND MAINTENANCE OF ALL EXISTING UTILITIES AND STRUCTURES. 	<p>PAVING/GRADING TESTING AND INSPECTION</p> <ol style="list-style-type: none"> THE CONTRACTOR IS RESPONSIBLE FOR THE TESTING AND RECORDING OF ALL SOILS TO BE USED IN THE CONSTRUCTION OF THE PAVEMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE TESTING AND RECORDING OF ALL SOILS TO BE USED IN THE CONSTRUCTION OF THE PAVEMENT. A QUALIFIED TESTING LABORATORY SHALL PERFORM ALL TESTING NECESSARY TO ASSESS COMPLIANCE OF THE IN-PLACE PAVEMENT WITH THE SPECIFICATIONS OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE TESTING AND RECORDING OF ALL SOILS TO BE USED IN THE CONSTRUCTION OF THE PAVEMENT. 	<p>PAVEMENT MARKING AND SIGNAGE</p> <ol style="list-style-type: none"> THE INSTALLATION, SHAPE, AND SIZE OF ALL SIGNS AND THEIR LETTERING SHALL COMPLY WITH THE LATEST EDITIONS OF THE F.D.A.T. "ROADWAY AND TRAFFIC DESIGN STANDARD". STOP SIGNS AND STOP SIGNS ARE TO BE PROVIDED AT ALL INTERNAL, ON-SITE INTERSECTIONS, WITH THE EXCEPTION OF SIGNALIZED INTERSECTIONS (UNLESS OTHERWISE NOTED). 	<p>DEMOLITION</p> <ol style="list-style-type: none"> CONTRACTOR SHALL SUBMIT DEMOLITION SCHEDULE TO OWNER PRIOR TO PROCEEDING WITH DEMOLITION ACTIVITIES. DETAIL OF SITE CLEANING IS SHOWN ON DRAWINGS. CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL EXISTING UTILITIES, STRUCTURES, AND OTHER FEATURES.
<p>GENERAL</p> <ol style="list-style-type: none"> ALL DIMENSIONS ARE TO FACE OF CURB UNLESS OTHERWISE NOTED. ALL STRUCTURES AND DOCUMENTS REFERENCED HEREON SHALL BE OF THE LATEST EDITION. ALL DIMENSIONS ARE TO FACE OF CURB UNLESS OTHERWISE NOTED. ALL DIMENSIONS ARE TO FACE OF CURB UNLESS OTHERWISE NOTED. 	<p>TREES AND VEGETATION</p> <ol style="list-style-type: none"> THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE MAINTENANCE OF ALL LANDSCAPE TREES AND SHRUBS AND OTHER VEGETATION ON THE PROJECT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE MAINTENANCE OF ALL LANDSCAPE TREES AND SHRUBS AND OTHER VEGETATION ON THE PROJECT. 	<p>AS BUILT</p> <ol style="list-style-type: none"> UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR SHALL FURNISH THE OWNER'S ENGINEER WITH COMPLETE "AS-BUILT" DRAWINGS OF ALL EXISTING AND NEW UTILITIES, STRUCTURES, AND OTHER FEATURES. THE "AS-BUILT" DRAWINGS SHALL BE MADE UPON THE ORIGINAL DRAWINGS AND SHALL BE ACCURATE AND COMPLETE. 	<p>GENERAL NOTES</p> <ol style="list-style-type: none"> ALL DIMENSIONS ARE TO FACE OF CURB UNLESS OTHERWISE NOTED. ALL DIMENSIONS ARE TO FACE OF CURB UNLESS OTHERWISE NOTED.



189 S. ORANGE AVENUE, SUITE 1000, AUSTIN, TEXAS 78704
 PHONE: 402-888-1511
 FAX: 402-888-1511
 WWW.KH-INC.COM REC-1787 11/11 2010

DATE: 05/04/2022
 SCALE: AS SHOWN
 DRAWN BY: TAD
 CHECKED BY: TAD
 PROJECT: C-10110000

CITY OF LAKE CITY
90 & I-75 FUEL EXPANSION
 CIRCLE K - US HWY



CALL 811 HOURS BEFORE YOU DIG
 IT'S THE LAW
 DIAL 811
 811 TEXAS



DATE	05/04/2022
SCALE	AS SHOWN
DRAWN BY	JAM
CHECKED BY	JCS

149899040
PROJECT NO.

Kimley-Horn and Associates, Inc.
189 S. Orange Ave., Suite 1000, Orlando, FL 32801
www.kimley-horn.com REG-12023-11-12-04

NO.	REVISIONS	DATE

FDOT GENERAL NOTES

1. MAINTENANCE OF TRAFFIC TO BE SUPERVISED BY A CERTIFIED PERSON.
2. THE CONTRACTOR SHALL NOTIFY THE DEPARTMENT A MINIMUM OF TWO BUSINESS DAYS PRIOR TO ANY LAKE CLOSURES OR BECOMING ANY CONSTRUCTION WITHIN THE FOOT RIGHT-OF-WAY.
3. ALL WORK PERFORMED WITHIN THE FOOT RIGHT OF WAY SHALL BE IN ACCORDANCE WITH THE FY2021-22 OR CURRENT EDITION OF FOOT STANDARD PLANS.
4. CONTRACTOR DETERMINES THAT AS-BUILT CONDITIONS VARY SIGNIFICANTLY FROM THE APPROVED PLANS, THE PERMITS FROM 800-000-18, WITHIN 30 DAYS.
5. IT WILL BE THE RESPONSIBILITY OF THE PERMITTEE TO REPAIR ANY DAMAGE TO FOOT FACILITIES CAUSED BY CONSTRUCTION OF THE PROJECT.
6. TEST RESULTS OF ANY TESTS TAKEN FOR OR DURING CONSTRUCTION OF THE PERMITTED WORK SHALL BE PROVIDED TO THE FOOT USER REQUEST.
7. ALL CONCRETE SHALL BE SET OUT AT THE EARLIEST TIME IN GOOD WEATHER, SO AS TO PRODUCE A CONNECTION WITH THE EXISTING THAT IS FREE OF SPALLS, DISINTEGRATION OR SHAPE, HOLES, SURFACE IRREGULARITIES, AND OTHER DEFECTS.
8. ALL MATERIALS INSTALLED WITHIN FOOT RIGHT-OF-WAY SHALL BE LIMITED TO THOSE ON THE FOOT'S QUALIFIED PRODUCTS LIST OR APPROVED PRODUCT LIST OF TRAFFIC CONTROL, SIGNALS AND DEVICES.
9. THE PERMITTEE SHALL CONTACT THE CITY OF LAKE CITY TRAFFIC DEPT. (304) 748-5400.
10. ALL CONSTRUCTION IN THE FOOT ROW SHALL CONFORM TO THE LATEST EDITIONS OF THE FOOT DESIGN STANDARDS, THE FOOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, AND THE FOOT UTILITY ACCOMMODATION MANUAL.
11. ALL DISTURBED AREAS IN FOOT ROW SHALL BE SOODED.
12. ALL WORK PERFORMED WITHIN THE FOOT RIGHT-OF-WAY SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE FOOT DESIGN STANDARDS, THE LATEST EDITION OF THE SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, AND THE FOOT UTILITY ACCOMMODATION MANUAL.
13. PLEASE NOTIFY JACKSONVILLE OPERATIONS TWO BUSINESS DAYS BEFORE BECOMING WORK @ (904) 308-7500.

SANITARY SYSTEM

1. ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE CITY OF LAKE CITY AND ALL APPLICABLE STATE AND FEDERAL REGULATIONS AND STANDARDS. ALL WORK SHALL BE IN ACCORDANCE WITH THE CITY OF LAKE CITY AND ALL APPLICABLE STATE AND FEDERAL REGULATIONS AND STANDARDS. ALL WORK SHALL BE IN ACCORDANCE WITH THE CITY OF LAKE CITY AND ALL APPLICABLE STATE AND FEDERAL REGULATIONS AND STANDARDS.
2. ALL GRANTY SEWERS MUST BE 24" P.C. DIAMETER. ALL OTHERS MUST BE 18" OR 24" P.C. DIAMETER. ALL OTHERS MUST BE 18" OR 24" P.C. DIAMETER. ALL OTHERS MUST BE 18" OR 24" P.C. DIAMETER.
3. ALL SIZES FOR GRANTY SEWER MAINS AND SERVICE CONNECTIONS SHALL COMPLY WITH THE FOLLOWING MINIMUM SIZES:
4. ALL SIZES FOR GRANTY SEWER MAINS AND SERVICE CONNECTIONS SHALL COMPLY WITH THE FOLLOWING MINIMUM SIZES:
5. WORK TO COMBINE WORK WHICH REQUIRES CONNECTING PROPOSED FACILITIES TO EXISTING LINES OR APPURTENANCES. THE CONTRACTOR SHALL VERIFY THE LOCATION AND ELEVATIONS OF EXISTING CONNECTION POINT(S) AND NOTIFY THE OWNER'S ENGINEER OF ANY CONTACTS OR INTERFERENCES.

POTABLE WATER SYSTEM

1. ALL DWV PIPE SHALL BE CLASS 50 OR HIGHER, NOT TO EXCEED 200 CALIBER PER INCH OF PIPE DIAMETER PER A.S.T.M. SPECIFICATION. ALL OTHERS SHALL BE CLASS 50 OR HIGHER, NOT TO EXCEED 200 CALIBER PER INCH OF PIPE DIAMETER PER A.S.T.M. SPECIFICATION.
2. ALL DWV PIPE SHALL BE CLASS 50 OR HIGHER, NOT TO EXCEED 200 CALIBER PER INCH OF PIPE DIAMETER PER A.S.T.M. SPECIFICATION.
3. ALL DWV PIPE SHALL BE CLASS 50 OR HIGHER, NOT TO EXCEED 200 CALIBER PER INCH OF PIPE DIAMETER PER A.S.T.M. SPECIFICATION.
4. ALL DWV PIPE SHALL BE CLASS 50 OR HIGHER, NOT TO EXCEED 200 CALIBER PER INCH OF PIPE DIAMETER PER A.S.T.M. SPECIFICATION.
5. ALL DWV PIPE SHALL BE CLASS 50 OR HIGHER, NOT TO EXCEED 200 CALIBER PER INCH OF PIPE DIAMETER PER A.S.T.M. SPECIFICATION.

SANITARY TESTING AND INSPECTION

1. ALL GRANTY SEWER PIPING SHALL BE SUBJECT TO A VISUAL INSPECTION BY THE OWNER'S ENGINEER AND APPLICABLE MUNICIPALITY/AGENCY. THE CONTRACTOR SHALL NOTIFY THE ENGINEER 48 HOURS IN ADVANCE TO SCHEDULE INSPECTIONS. THE CONTRACTOR SHALL PROVIDE CONES OF THE LAMPING PROTECTION TO THE ENGINEER, THE OWNER AND THE APPLICABLE MUNICIPALITY/AGENCY.
2. THE CONTRACTOR SHALL PERFORM AN INFILTRATION/EXFILTRATION TEST ON ALL GRANTY SEWERS IN ACCORDANCE WITH THE FOLLOWING MINIMUM SIZES:
3. ALL GRANTY SEWER PIPING SHALL BE SUBJECT TO A VISUAL INSPECTION BY THE OWNER'S ENGINEER AND APPLICABLE MUNICIPALITY/AGENCY. THE CONTRACTOR SHALL NOTIFY THE ENGINEER 48 HOURS IN ADVANCE TO SCHEDULE INSPECTIONS.
4. ALL GRANTY SEWER PIPING SHALL BE SUBJECT TO A VISUAL INSPECTION BY THE OWNER'S ENGINEER AND APPLICABLE MUNICIPALITY/AGENCY. THE CONTRACTOR SHALL NOTIFY THE ENGINEER 48 HOURS IN ADVANCE TO SCHEDULE INSPECTIONS.
5. ALL GRANTY SEWER PIPING SHALL BE SUBJECT TO A VISUAL INSPECTION BY THE OWNER'S ENGINEER AND APPLICABLE MUNICIPALITY/AGENCY. THE CONTRACTOR SHALL NOTIFY THE ENGINEER 48 HOURS IN ADVANCE TO SCHEDULE INSPECTIONS.

POTABLE WATER TESTING AND INSPECTION

1. ALL COMPONENTS OF THE WATER SYSTEM, INCLUDING FITTINGS, HYDRANTS, CONNECTIONS, AND VALVES SHALL BE PROPERLY PRESSURE TESTED AND ACCEPTED BY THE OWNER'S ENGINEER. PRESSURE TESTS TO BE IN ACCORDANCE WITH POLK COUNTY SPECIFICATIONS AND ALL APPLICABLE STATE AND FEDERAL REGULATIONS AND STANDARDS.
2. CONTRACTOR TO PERFORM DISINFECTION AND BACTERIOLOGICAL SAMPLING, AND OBTAIN CLEARANCE OF DOMESTIC AND INDUSTRIAL WATER SUPPLIES IN ACCORDANCE WITH ALL APPLICABLE STATE AND FEDERAL REGULATIONS AND STANDARDS.
3. ALL WATER MAINS SHALL BE PRESURE TESTED IN ACCORDANCE WITH A.S.T.M. STANDARD M33, CONFORMANCE CRITERIA: TESTING OF P.C. PIPING. 907-STEEL UTILITIES HYDROSTATIC TESTING TO BE WITNESSED BY THE CITY OF LAKE CITY WATER DEPARTMENT INSPECTOR.

POTABLE WATER SYSTEM

1. ALL DWV PIPE SHALL BE CLASS 50 OR HIGHER, NOT TO EXCEED 200 CALIBER PER INCH OF PIPE DIAMETER PER A.S.T.M. SPECIFICATION.
2. ALL DWV PIPE SHALL BE CLASS 50 OR HIGHER, NOT TO EXCEED 200 CALIBER PER INCH OF PIPE DIAMETER PER A.S.T.M. SPECIFICATION.
3. ALL DWV PIPE SHALL BE CLASS 50 OR HIGHER, NOT TO EXCEED 200 CALIBER PER INCH OF PIPE DIAMETER PER A.S.T.M. SPECIFICATION.
4. ALL DWV PIPE SHALL BE CLASS 50 OR HIGHER, NOT TO EXCEED 200 CALIBER PER INCH OF PIPE DIAMETER PER A.S.T.M. SPECIFICATION.
5. ALL DWV PIPE SHALL BE CLASS 50 OR HIGHER, NOT TO EXCEED 200 CALIBER PER INCH OF PIPE DIAMETER PER A.S.T.M. SPECIFICATION.
6. CONTRACTOR TO INSTALL TEMPORARY BLOWOFFS, AT THE ENDS OF PROPOSED WATER MAINS AND SERVICE LATERALS TO MAINTAIN POSITIVE (TOP) AIR FLOW, AND POLK COUNTY (CRITICAL) FLOWING AND DISINFECTION/CHLORINATION.
7. ALL WATER MAINS SHALL BE STERILIZED IN ACCORDANCE WITH THE APPLICABLE SECTION OF THE LATEST AWWA SPECIFICATION (501) AND CITY OF LAKE CITY WATER DEPARTMENT SPECIFICATIONS.
8. ALL P.C. WATER MAINS, 6" TO 12" DIAMETER PIPING, SHALL BE AWWA C-900 DR-18. JOINTS SHALL BE RUBBER GASKET PUSH-IN CONFORMING TO A.S.T.M. D1888.
9. POTABLE WATER MAINS WILL BE P.C. 24" (200 PSI) FOR PIPES LESS THAN 4', SCHEDULE 40 AND SCHEDULE 80 PIPING SHALL BE USED FOR PIPES 4' AND GREATER THAN 4'. THE ABOVE TYPICAL INSTALLATIONS MUST BEAR THE "P" STAMP FOR COMPATIBILITY WITH POTABLE WATER USE.
10. ALL POLYETHYLENE GLASS REINFORCED FIBER (FRP) SHALL BE MANUFACTURED IN ACCORDANCE WITH A.S.T.M. C-2668 FOR CLAY PIPE, A.S.T.M. C-1155 FOR POLYETHYLENE GLASS REINFORCED FIBER (FRP) PIPE, AND A.S.T.M. C-1155 FOR POLYETHYLENE GLASS REINFORCED FIBER (FRP) PIPE.
11. ALL WATER SYSTEM CONSTRUCTION FROM THE POINT OF CONNECTION IN THE RIGHT OF WAY UP TO AND INCLUDING POINT OF METERING AND EACH FLOW PREVENTION (IF REQUIRED), SHALL BE BUILT ACCORDING TO POLK COUNTY STANDARDS AND SPECIFICATIONS.
12. P.C. PIPE BURIED BENEATH ROADWAYS, PARKING LOTS OR PARKING LOT DRIVEWAYS SHALL MEET AWWA SPECIFICATION C-900 DR-18. ALL OTHER PIPING SHALL BE IN ACCORDANCE WITH A.S.T.M. C-900 DR-18.
13. ALL PIPING SHALL BE CLASS 50 OR HIGHER, NOT TO EXCEED 200 CALIBER PER INCH OF PIPE DIAMETER PER A.S.T.M. SPECIFICATION.

STORMWATER POLLUTION PREVENTION PLAN

DATE: _____ **PROJECT NAME AND LOCATION:** CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION, CITY OF LAKE CITY, FLORIDA. **DEVELOPER NAME AND ADDRESS:** SCHAFFER CONSTRUCTION, LLC, 2801 NETWORK BLVD., SUITE 413, LAKE CITY, FLORIDA 32115. **CONTRACTOR NAME AND ADDRESS:** KIMLEY-HORN AND ASSOCIATES, INC., 200 S. ORANGE AVENUE, SUITE 1000, LAKE CITY, FLORIDA 32115.

EROSION AND SEDIMENT CONTROL: BEST MANAGEMENT PRACTICES SHALL BE USED FOR THIS PROJECT TO CONTROL EROSION AND SEDIMENTATION. THE FOLLOWING ARE THE BEST MANAGEMENT PRACTICES TO BE USED TO CONTROL EROSION AND SEDIMENTATION: 1. CONSTRUCTION OF EROSION CONTROL STRUCTURES SHALL BE SHOWN ON THE PLAN. THE CONTRACTOR IS RESPONSIBLE FOR THE DESIGN AND CONSTRUCTION OF ALL EROSION CONTROL STRUCTURES. 2. THE CONTRACTOR SHALL MAINTAIN ALL EROSION CONTROL STRUCTURES THROUGHOUT THE PROJECT. 3. THE CONTRACTOR SHALL MAINTAIN ALL EROSION CONTROL STRUCTURES THROUGHOUT THE PROJECT. 4. THE CONTRACTOR SHALL MAINTAIN ALL EROSION CONTROL STRUCTURES THROUGHOUT THE PROJECT.

SPILL CONTROL PRACTICES: IN ADDITION TO THE GOOD HOUSEKEEPING AND MATERIAL MANAGEMENT PRACTICES DISCUSSED IN THE PREVIOUS SECTIONS OF THIS PLAN, THE FOLLOWING SPILL CONTROL PRACTICES SHALL BE FOLLOWED FOR SPILL PREVENTION AND CLEANUP: 1. SPILL CLEANUP INFORMATION SHALL BE POSTED ON SITE TO INFORM EMPLOYEES ABOUT CLEANUP PROCEDURES AND RESOURCES. 2. THE FOLLOWING CLEANUP EQUIPMENT MUST BE KEPT ON SITE AS PART OF THE MATERIAL STORAGE AREA: GLOVES, MOPS, BRUSHES, DUST PALS, SAND, SAND/ST. LIQUID ABSORBER, GOGGLES, AND TRASH CONTAINERS. 3. ALL SPILLS SHALL BE CLEANED UP AS SOON AS POSSIBLE. 4. HAZARDOUS SPILLS SHALL BE REPORTED TO THE PROPER AGENCY AND THE EMPLOYEE SHALL WEAR PROPER PROTECTIVE CLOTHING TO PREVENT INJURY. 5. SPILLS SHALL BE REPORTED TO THE PROPER AGENCY REGARDLESS OF THE SIZE OF THE SPILL. 6. AFTER A SPILL, THE PREVENTION PLAN SHALL BE REVIEWED AND CHANGED TO PREVENT REPEATED SPILLS. 7. SPILLS SHALL BE REPORTED TO THE PROPER AGENCY. 8. THE SUPERINTENDENT SHALL BE RESPONSIBLE FOR THE DAY TO DAY SITE OPERATIONS. 9. THE SUPERINTENDENT SHALL BE RESPONSIBLE FOR EDUCATING THE EMPLOYEES ABOUT SPILL PREVENTION AND CLEANUP PROCEDURES.

HAZARDOUS WASTE: HAZARDOUS WASTE MATERIALS SHALL BE STORED IN A SECURE DUMPSTER WITH ALL LOCAL AND STATE LAWS OR AS DIRECTED BY THE MANUFACTURER. THE SUPERINTENDENT SHALL ORGANIZE THE PROPER HAZARDOUS WASTE MATERIALS. THESE PROCEDURES SHALL BE POSTED ON THE SITE. THE PERSON WHO MANAGES THE SITE SHALL BE RESPONSIBLE FOR ENFORCEING THE PROCEDURES.

SOIL EROSION CONTROL: THE ORDER OF CONSTRUCTION IS AS FOLLOWS: 1. PROVIDE STABILIZED CONSTRUCTION ENTRANCE 2. INSTALL SILT FENCES AND OTHER EROSION CONTROL METHODS 3. CLEAR AND GRUB FOR SEDIMENT BASH AND EARTH DIRT 4. CONSTRUCT EARTH DIRT AND SEDIMENT BASH 5. FINISH CLEARING AND GRUBBING 6. PROVIDE INITIAL GRADING AS REQUIRED 7. STABILIZE ALL DISTURBED AREAS AS SOON AS POSSIBLE 8. FINISH GRADING ENTIRE SITE 9. REMOVE ACCUMULATED SEDIMENT 10. REMOVE ANY ITEMS THAT ARE NOT REQUIRED 11. FINISH GRADING ENTIRE SITE 12. REMOVE ANY ITEMS THAT ARE NOT REQUIRED 13. FINISH GRADING ENTIRE SITE 14. REMOVE ANY ITEMS THAT ARE NOT REQUIRED

SPILL CONTROL PRACTICES: IN ADDITION TO THE GOOD HOUSEKEEPING AND MATERIAL MANAGEMENT PRACTICES DISCUSSED IN THE PREVIOUS SECTIONS OF THIS PLAN, THE FOLLOWING SPILL CONTROL PRACTICES SHALL BE FOLLOWED FOR SPILL PREVENTION AND CLEANUP: 1. SPILL CLEANUP INFORMATION SHALL BE POSTED ON SITE TO INFORM EMPLOYEES ABOUT CLEANUP PROCEDURES AND RESOURCES. 2. THE FOLLOWING CLEANUP EQUIPMENT MUST BE KEPT ON SITE AS PART OF THE MATERIAL STORAGE AREA: GLOVES, MOPS, BRUSHES, DUST PALS, SAND, SAND/ST. LIQUID ABSORBER, GOGGLES, AND TRASH CONTAINERS. 3. ALL SPILLS SHALL BE CLEANED UP AS SOON AS POSSIBLE. 4. HAZARDOUS SPILLS SHALL BE REPORTED TO THE PROPER AGENCY AND THE EMPLOYEE SHALL WEAR PROPER PROTECTIVE CLOTHING TO PREVENT INJURY. 5. SPILLS SHALL BE REPORTED TO THE PROPER AGENCY REGARDLESS OF THE SIZE OF THE SPILL. 6. AFTER A SPILL, THE PREVENTION PLAN SHALL BE REVIEWED AND CHANGED TO PREVENT REPEATED SPILLS. 7. SPILLS SHALL BE REPORTED TO THE PROPER AGENCY. 8. THE SUPERINTENDENT SHALL BE RESPONSIBLE FOR THE DAY TO DAY SITE OPERATIONS. 9. THE SUPERINTENDENT SHALL BE RESPONSIBLE FOR EDUCATING THE EMPLOYEES ABOUT SPILL PREVENTION AND CLEANUP PROCEDURES.

HAZARDOUS WASTE: HAZARDOUS WASTE MATERIALS SHALL BE STORED IN A SECURE DUMPSTER WITH ALL LOCAL AND STATE LAWS OR AS DIRECTED BY THE MANUFACTURER. THE SUPERINTENDENT SHALL ORGANIZE THE PROPER HAZARDOUS WASTE MATERIALS. THESE PROCEDURES SHALL BE POSTED ON THE SITE. THE PERSON WHO MANAGES THE SITE SHALL BE RESPONSIBLE FOR ENFORCEING THE PROCEDURES.

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NO.	REVISIONS	DATE

Kimley-Horn

200 S. ORANGE AVENUE, SUITE 1000, LAKE CITY, FLORIDA 32115
 P: 407.500.5173
 F: 407.500.5173
 WWW.KIMLEY-HORN.COM

PROFESSIONAL ENGINEER
 JAROD C. STUBBS, P.E.
 FLORENCE, FLORIDA 32205
 LICENSE NO. 20343
 DATE: _____

STORMWATER POLLUTION PREVENTION PLAN

CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION
 CITY OF LAKE CITY, FLORIDA

SIGNATURE AND DATE	NAME AND TITLE, INCLUDING ADDRESS AND TELEPHONE NUMBER	RESPONSIBILITY

811
 CALL 48 HOURS BEFORE YOU DIG
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 800.368.5871
 www.811.com
 MEMBER STATE OF FLORIDA

MUNICIPAL WATER UTILITIES ARE REQUIRED TO BE LOCATED AND MARKED PRIOR TO ANY EXCAVATION. THE CITY OF LAKE CITY, FLORIDA IS NOT RESPONSIBLE FOR THE LOCATION OF UTILITIES. SEE SHEET C-10 FOR THE LOCATION OF UTILITIES. PAGE 4, PLAN 101.

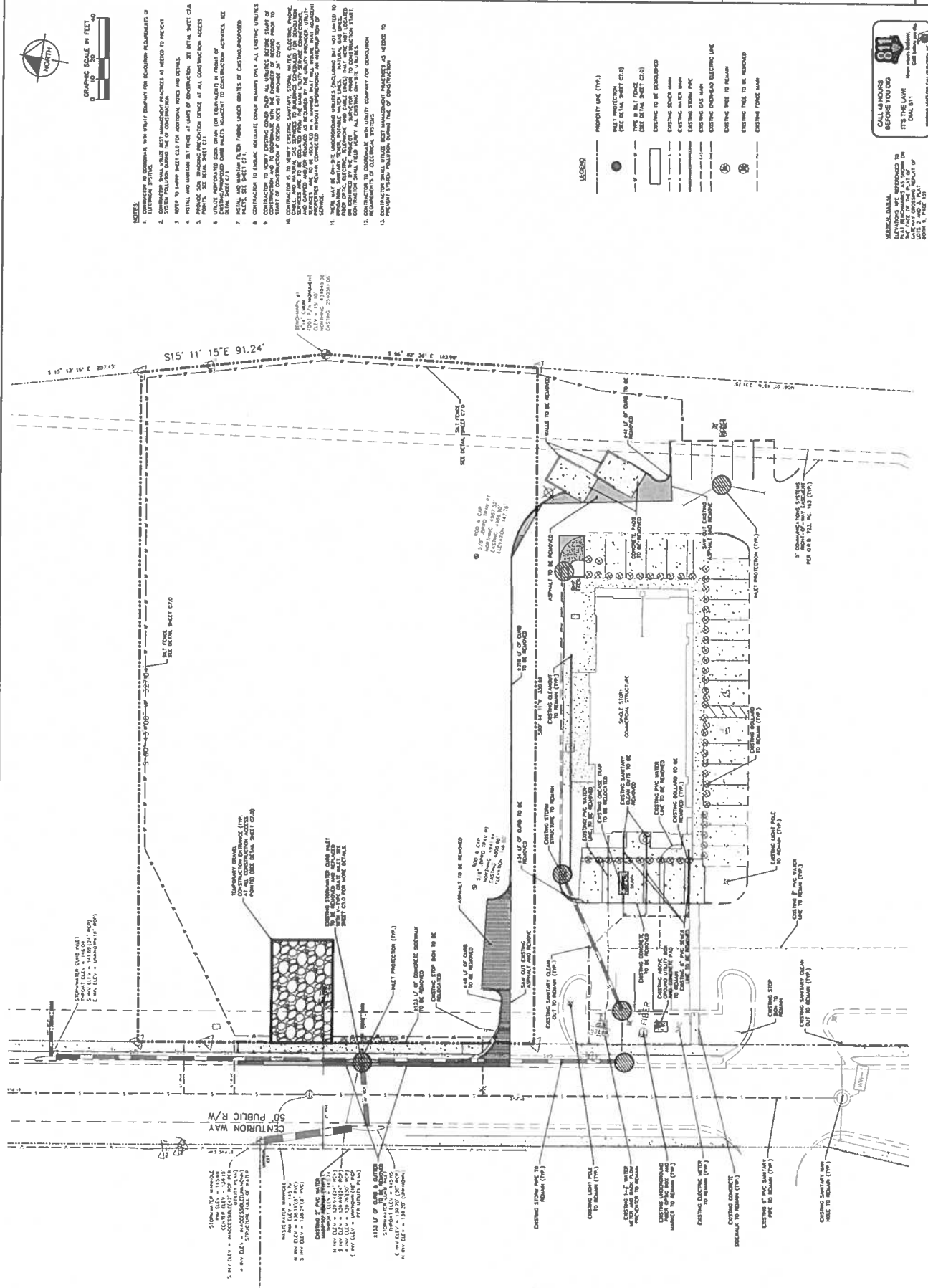
CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION

EXISTING CONDITIONS & DEMOLITION PLAN

PROJ: 149580040
 DATE: 05/04/2023
 SCALE: AS SHOWN
 DRAWN BY: C.F.
 CHECKED BY: C.F.
 DESIGNED BY: C.F.
 PROJECT NO: 23

Kimley-Horn
 193 S. ORANGE AVENUE, SUITE 1000, GAINESVILLE, FL 32601
 WWW.KIMLEY-HORN.COM REGISTERED PROFESSIONAL ENGINEERS

No.	REVISIONS	DATE



- NOTES:**
1. CONTRACTOR SHALL VERIFY ALL UTILITIES WITH UTILITY COMPANIES PRIOR TO ANY EXCAVATION. THE CITY OF LAKE CITY, FLORIDA IS NOT RESPONSIBLE FOR THE LOCATION OF UTILITIES. SEE SHEET C-10 FOR THE LOCATION OF UTILITIES. PAGE 4, PLAN 101.
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CITY OF LAKE CITY
EXPANSION
90 & I-75 FUEL
FLORIDA

EXISTING
CONDITIONS &
DEMOLITION PLAN

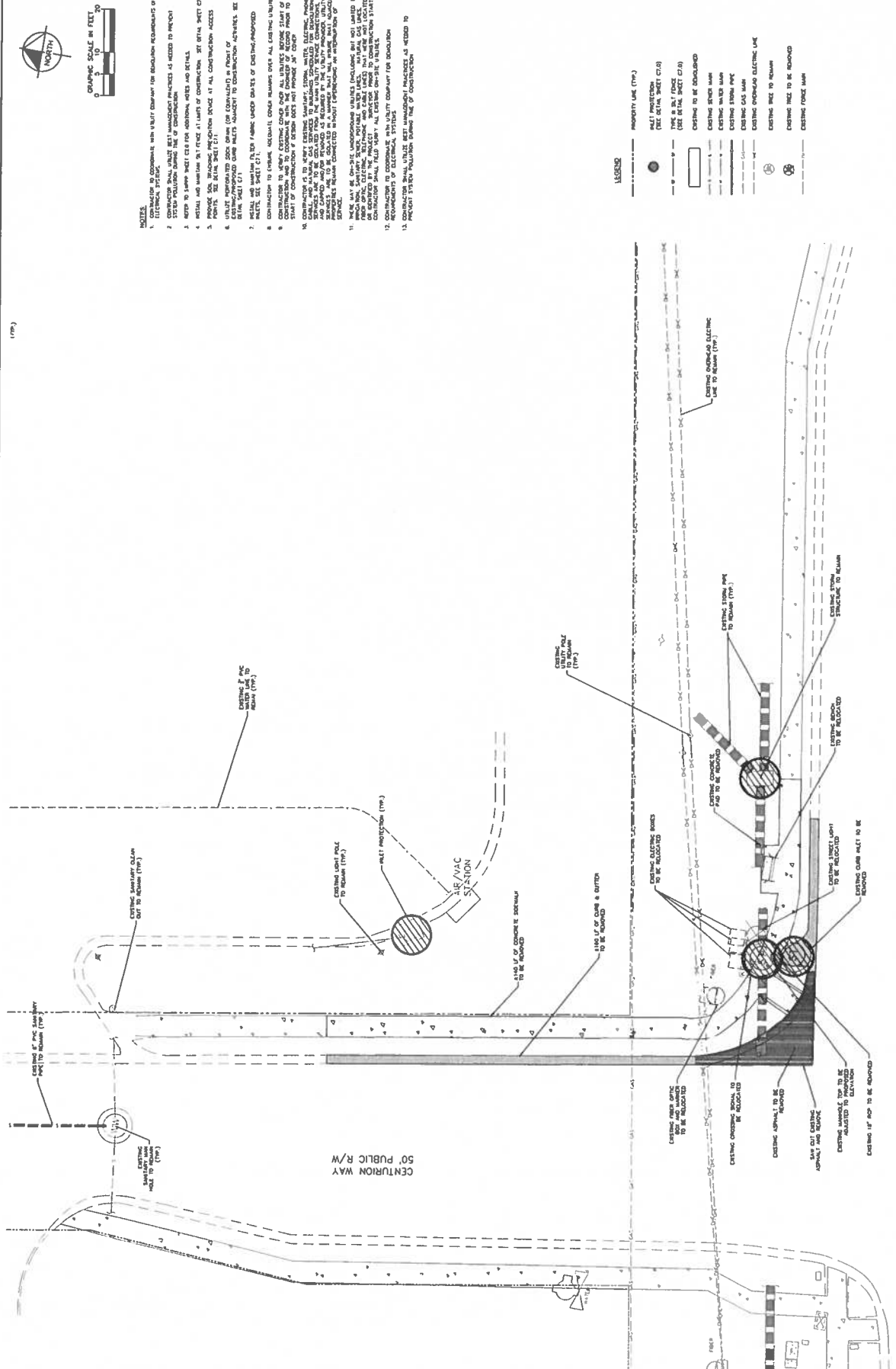
K&L PROJECT	149880040
DATE	05/04/2022
SCALE	AS SHOWN
DRAWN BY	CF
CHECKED BY	CF
DESIGNED BY	CF
DATE	05/04/2022

Kimley-Horn
181 E. CHANCE AVENUE, SUITE 1000, GAINESVILLE, FL 32601
NEWCASTLE, WICHITA, MISSOURI
REGISTERED PROFESSIONAL ENGINEERS
PROFESSIONAL ENGINEER NO. 12568

NO.	REVISIONS	DATE

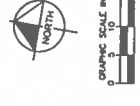


STATE ROAD 10
U.S. HIGHWAY 90
R/W VARIES



- LEGEND**
- PROPERTY LINE (TYP.)
 - UTILITY POLE TO REMAIN (TYP.)
 - TIME & BULK FORCE (SEE DETAIL SHEET C7-3)
 - EXISTING TO BE DEMOLISHED (SEE DETAIL SHEET C7-4)
 - EXISTING OVERHEAD ELECTRIC LINE TO REMAIN (TYP.)
 - EXISTING UNDERGROUND ELECTRIC LINE TO REMAIN (TYP.)
 - EXISTING OVERHEAD ELECTRIC LINE TO BE REMOVED (TYP.)
 - EXISTING UNDERGROUND ELECTRIC LINE TO BE REMOVED (TYP.)
 - EXISTING FORCE MAIN TO REMAIN (TYP.)
 - EXISTING FORCE MAIN TO BE REMOVED (TYP.)

- NOTES:**
- CONTRACTOR SHALL VERIFY ALL UTILITIES WITH UTILITY COMPANIES PRIOR TO CONSTRUCTION.
 - CONTRACTOR SHALL VERIFY ALL UTILITIES WITH UTILITY COMPANIES PRIOR TO CONSTRUCTION.
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 - CONTRACTOR SHALL VERIFY ALL UTILITIES WITH UTILITY COMPANIES PRIOR TO CONSTRUCTION.



CITY OF LAKE CITY
CIRCLE K - US HWY
90 & I-75 FUEL
EXPANSION

OVERALL SITE PLAN

Kimley»Horn
PLANNERS ENGINEERS ARCHITECTS
183 S DAVENPORT AVE SUITE 1000, CHATTANOOGA, TN 37601
PH: 423-861-1511 FAX: 423-861-1512
WWW.KIMLEY-HORN.COM PERS: 404-392-3950

Professional Engineer Seal: KIMLEY-HORN, MICHAEL R. (STATE OF TENNESSEE) No. 46168-0010000022

Professional Engineer Seal: HORN, MICHAEL R. (STATE OF TENNESSEE) No. 46168-0010000022

DATE: 05/04/2022
SCALE: AS SHOWN
DESIGNED BY: JCF
DRAWN BY: JCF
CHECKED BY: JCF
JOB NO: 14980040

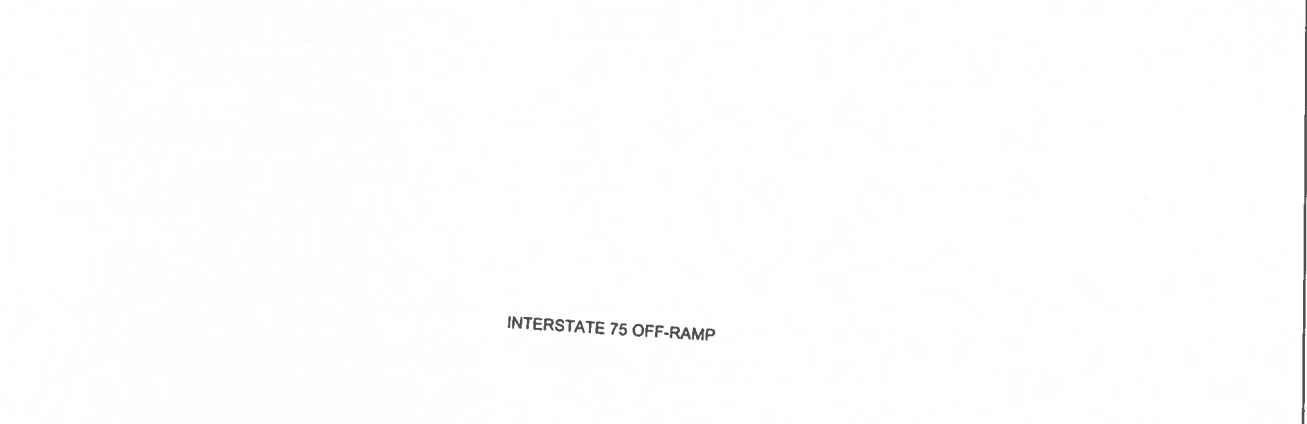
- NOTES:**
1. ALL CURB DIMENSIONS ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
 2. ALL DIMENSIONS ARE TO THE OUTSIDE FACE OF BUILDING UNLESS OTHERWISE NOTED.
 3. REFER TO ARCHITECTURAL AND STRUCTURAL PLANS TO VERIFY ALL BUILDING DIMENSIONS.
 4. REFER TO ARCHITECTURAL PLANS FOR DIMENSIONS AND DETAILS.
 5. ALL PROPOSED AND EXISTING DIMENSIONS SHALL BE PROVIDED IN ARCHITECTURAL PLANS TO THE DIMENSIONS OF THE EXISTING STRUCTURE AND SHALL BE VERIFIED IN THE FIELD PRIOR TO CONSTRUCTION.
 6. ALL PROPOSED DIMENSIONS SHALL BE PROVIDED IN ARCHITECTURAL PLANS TO THE DIMENSIONS OF THE EXISTING STRUCTURE AND SHALL BE VERIFIED IN THE FIELD PRIOR TO CONSTRUCTION.
 7. ALL DIMENSIONS SHALL MEET THE REQUIREMENTS OF CITY OF LAKE CITY LAND DEVELOPMENT CODE, CHAPTER 7, SEC. 700.

SITE DATA:

PROJECT AREA:	1.148 ACRES (1.148333)
EXISTING USE:	COMMERCIAL
PROPOSED USE:	COMMERCIAL
PROPOSED ZONING:	COMMERCIAL
PROPOSED LOT AREA:	1.148 ACRES
PROPOSED PERVIOUS AREA:	96,714.3 SQ FT (2.20 AC)
PROPOSED IMPERVIOUS AREA:	54,087.87 SQ FT (1.24 AC)
PROPOSED BALDING AREA:	5,883.37 SQ FT (0.13 AC) (LAMB)
PROPOSED ADJUSTED BALDING AREA:	10,343.37 SQ FT (0.24 AC) (TYP)
TOTAL IMPERVIOUS AREA:	112,344.37 SQ FT (2.59 AC) (TYP)

LANDSCAPE SETBACKS:

PROPERTY LINE (TYP.)	PROPOSED ADJACENT PLANTING (SEE DETAIL SHEET C-2)	PROPOSED CONCRETE SIDEWALK (SEE DETAIL SHEET C-2)	PROPOSED MEDIUM DUTY CONCRETE SIDEWALK (SEE DETAIL SHEET C-2)	PROPOSED PAVED SIDEWALK (SEE DETAIL SHEET C-2)
30' BUILDING SETBACK	0 FT	0 FT	0 FT	0 FT
15' LANDSCAPE BUTTER	15 FT	15 FT	15 FT	15 FT
15' LANDSCAPE BUTTER	N/A	N/A	N/A	N/A
30' BUILDING SETBACK	0 FT	0 FT	0 FT	0 FT



INTERSTATE 75 OFF-RAMP

LEGEND

PROPOSED ADJACENT PLANTING (SEE DETAIL SHEET C-2)	PROPOSED CONCRETE SIDEWALK (SEE DETAIL SHEET C-2)	PROPOSED MEDIUM DUTY CONCRETE SIDEWALK (SEE DETAIL SHEET C-2)	PROPOSED PAVED SIDEWALK (SEE DETAIL SHEET C-2)
(Symbol)	(Symbol)	(Symbol)	(Symbol)

LANDSCAPE SETBACKS

PROPERTY LINE (TYP.)	PROPOSED ADJACENT PLANTING (SEE DETAIL SHEET C-2)	PROPOSED CONCRETE SIDEWALK (SEE DETAIL SHEET C-2)	PROPOSED MEDIUM DUTY CONCRETE SIDEWALK (SEE DETAIL SHEET C-2)	PROPOSED PAVED SIDEWALK (SEE DETAIL SHEET C-2)
30' BUILDING SETBACK	0 FT	0 FT	0 FT	0 FT
15' LANDSCAPE BUTTER	15 FT	15 FT	15 FT	15 FT
15' LANDSCAPE BUTTER	N/A	N/A	N/A	N/A
30' BUILDING SETBACK	0 FT	0 FT	0 FT	0 FT

REQUIRED

PROPOSED ADJACENT PLANTING (SEE DETAIL SHEET C-2)	PROPOSED CONCRETE SIDEWALK (SEE DETAIL SHEET C-2)	PROPOSED MEDIUM DUTY CONCRETE SIDEWALK (SEE DETAIL SHEET C-2)	PROPOSED PAVED SIDEWALK (SEE DETAIL SHEET C-2)
30' BUILDING SETBACK	0 FT	0 FT	0 FT
15' LANDSCAPE BUTTER	15 FT	15 FT	15 FT
15' LANDSCAPE BUTTER	N/A	N/A	N/A
30' BUILDING SETBACK	0 FT	0 FT	0 FT

PROPOSED

PROPOSED ADJACENT PLANTING (SEE DETAIL SHEET C-2)	PROPOSED CONCRETE SIDEWALK (SEE DETAIL SHEET C-2)	PROPOSED MEDIUM DUTY CONCRETE SIDEWALK (SEE DETAIL SHEET C-2)	PROPOSED PAVED SIDEWALK (SEE DETAIL SHEET C-2)
30' BUILDING SETBACK	0 FT	0 FT	0 FT
15' LANDSCAPE BUTTER	15 FT	15 FT	15 FT
15' LANDSCAPE BUTTER	N/A	N/A	N/A
30' BUILDING SETBACK	0 FT	0 FT	0 FT

EXISTING

PROPOSED ADJACENT PLANTING (SEE DETAIL SHEET C-2)	PROPOSED CONCRETE SIDEWALK (SEE DETAIL SHEET C-2)	PROPOSED MEDIUM DUTY CONCRETE SIDEWALK (SEE DETAIL SHEET C-2)	PROPOSED PAVED SIDEWALK (SEE DETAIL SHEET C-2)
30' BUILDING SETBACK	0 FT	0 FT	0 FT
15' LANDSCAPE BUTTER	15 FT	15 FT	15 FT
15' LANDSCAPE BUTTER	N/A	N/A	N/A
30' BUILDING SETBACK	0 FT	0 FT	0 FT

811
CALL 811 HOURS BEFORE YOU DIG
IT'S THE LAW!
DIAL 111
FOR THE CITY OF LAKE CITY
1111 W. MARKET ST. SUITE 100
LAKE CITY, TN 37902
WWW.CITYOFLAKECITY.COM

SCALE: AS SHOWN

DATE: 05/04/2022

CENTURION COURT

US HWY 90

CENTURION COURT

SITE PLAN

DATE: 05/04/2022
SCALE: AS SHOWN
PROJECT: 1-580040
DRAWN BY: J.S.
CHECKED BY: M.S.
LAKELAND DISTRICT ENGINEER: [Signature]

Kimley»Horn
189 S ORANGE AVENUE, SUITE 1000, GAINESVILLE, FL 32609
PHONE: 352-688-1511
WWW.KIMLEY-HORN.COM | REGISTRATION NO. 33106

NO.	REVISIONS	DATE

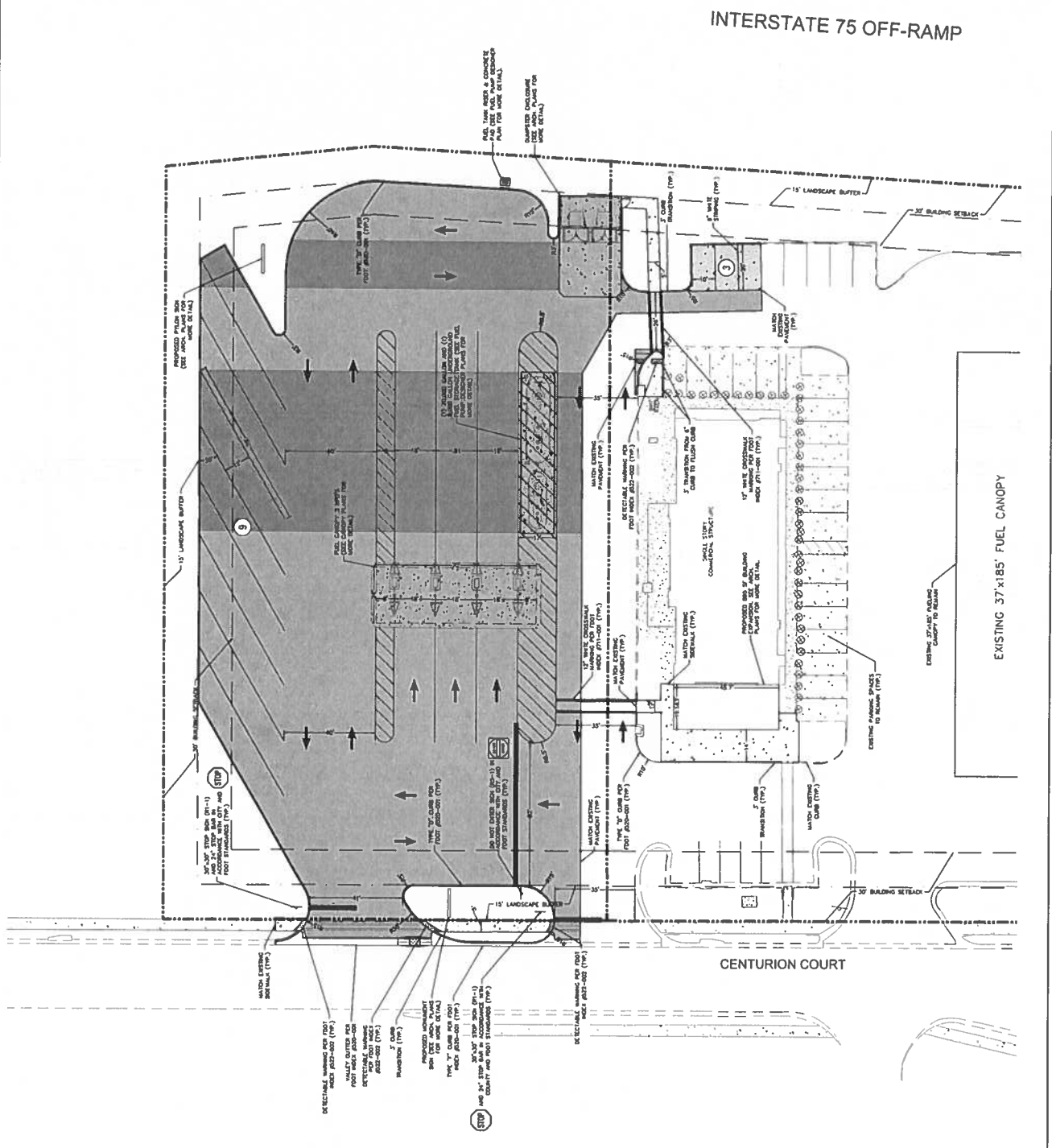
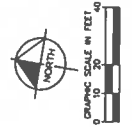
811
CALL US BEFORE YOU DIG
IT'S THE LAW!
DATE 8/11
CITY OF LAKE CITY
UNIVERSITY MICROFILMS INTERNATIONAL, INC.

ADDITIONAL NOTES:
DRAWINGS ARE REFERENCED TO THE PLAN OF THE CITY OF LAKE CITY, FLORIDA, CHAPTER 22A, ARTICLE 10, SECTION 22A.03, AND CHAPTER 22A, ARTICLE 10, SECTION 22A.04.

LEGEND

- PROPERTY LINE (TYP.)
- PROPOSED ASPHALT PAVEMENT (SEE DETAIL SHEET C7.2)
- PROPOSED CONCRETE SIDEWALK (SEE DETAIL SHEET C7.2)
- PROPOSED ASPHALT DRIVEWAY (SEE DETAIL SHEET C7.2)
- PROPOSED ASPHALT DRIVEWAY (SEE DETAIL SHEET C7.2)

- NOTES:**
- ALL CURB DIMENSIONS ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
 - ALL DIMENSIONS ARE TO THE OUTSIDE FACE OF BUILDING UNLESS OTHERWISE NOTED.
 - REFER TO ARCHITECTURAL AND STRUCTURAL PLANS TO VERIFY ALL BUILDING DIMENSIONS.
 - SEE ALL PLANS FOR FINISHES AND DETAILS.
 - ALL PROPOSED DRIVE STRIPS AND PARALLEL MARKINGS SHALL BE PAINTED UNLESS OTHERWISE NOTED AND IN ACCORDANCE WITH FOOT SHEET T11-001.
 - ALL PROPOSED DRIVE STRIPS AND PARALLEL MARKINGS SHALL BE PAINTED UNLESS OTHERWISE NOTED AND IN ACCORDANCE WITH FOOT SHEET T11-001.
 - ALL DIMENSIONS SHALL BE IN ACCORDANCE WITH FOOT SHEET T11-001.
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 - ALL DIMENSIONS SHALL BE IN ACCORDANCE WITH FOOT SHEET T11-001.
 - ALL DIMENSIONS SHALL BE IN ACCORDANCE WITH FOOT SHEET T11-001.



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 DATE: 8/11
 CALL BEFORE YOU DIG
 1-800-4-A- Dig
 (www.callbeforeyoudig.com)

GENERAL NOTES:
 1. ALL DIMENSIONS ARE REFERENCED TO THE FACE OF THE WALL UNLESS OTHERWISE NOTED.
 2. ALL DIMENSIONS ARE TO THE OUTSIDE FACE OF BUILDING UNLESS OTHERWISE NOTED.
 3. ALL DIMENSIONS ARE TO THE CENTERLINE OF THE ROAD UNLESS OTHERWISE NOTED.
 4. ALL DIMENSIONS ARE TO THE CENTERLINE OF THE ROAD UNLESS OTHERWISE NOTED.
 5. ALL DIMENSIONS ARE TO THE CENTERLINE OF THE ROAD UNLESS OTHERWISE NOTED.

CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION
 INTERSECTION PLAN
 FLORIDA

INTERSECTION PLAN
 SCALE: AS SHOWN
 DESIGNED BY: E.S.
 CHECKED BY: J.S.
 DATE: 05/04/2022
 PROJECT NO.: 14980040

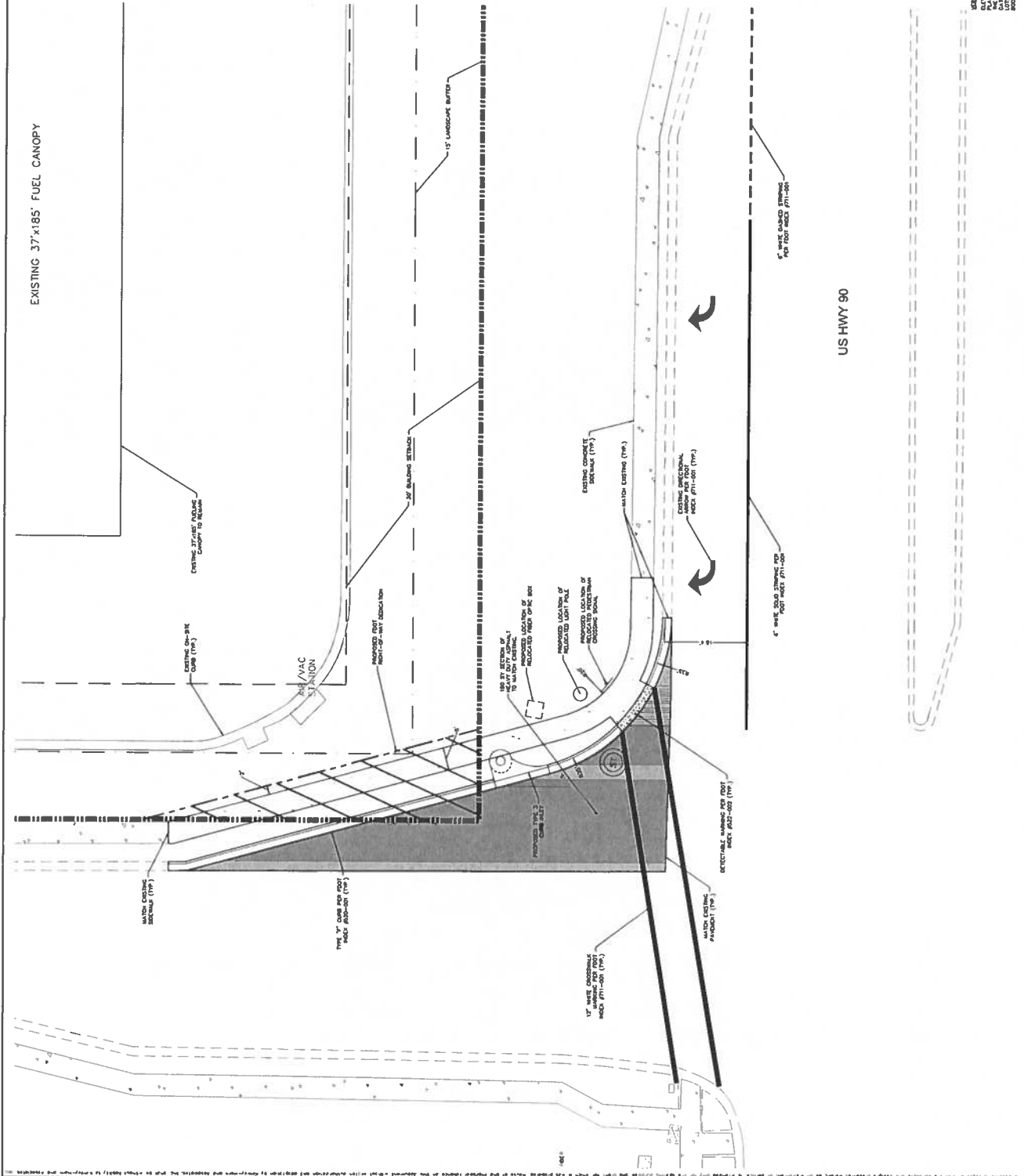
Kimley»Horn
 180 S GRADY AVENUE, SUITE 1000, ORLANDO, FL 32801
 PHONE: (407) 283-1511
 WWW.KIMLEY-HORN.COM REG. NO. 12106

NO.	REVISIONS	DATE

NOTES:
 1. ALL DIMENSIONS ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
 2. ALL DIMENSIONS ARE TO THE OUTSIDE FACE OF BUILDING UNLESS OTHERWISE NOTED.
 3. ALL DIMENSIONS ARE TO THE CENTERLINE OF THE ROAD UNLESS OTHERWISE NOTED.
 4. ALL DIMENSIONS ARE TO THE CENTERLINE OF THE ROAD UNLESS OTHERWISE NOTED.
 5. ALL DIMENSIONS ARE TO THE CENTERLINE OF THE ROAD UNLESS OTHERWISE NOTED.

GRAPHIC SCALE IN FEET:
 0 5 10 20

LEGEND:
 PROPERTY LINE (TYP.)
 PROPOSED CONCRETE DRIVEWAY (SEE DETAIL SHEET C7.6)
 PROPOSED CONCRETE SIDEWALK (SEE DETAIL SHEET C7.6)
 PROPOSED ASPHALT DRIVEWAY (SEE DETAIL SHEET C7.5)
 PROPOSED ASPHALT SIDEWALK (SEE DETAIL SHEET C7.5)



**CIRCLE K - US HWY
90 & I-75 FUEL
EXPANSION**

CITY OF LAKE CITY
FLORIDA

**TRUCK TURNING
MOVEMENTS**

PROJ. NO. 149880040
DATE 05/04/2022
SCALE AS SHOWN
DESIGNED BY E.F.
DRAWN BY E.F.
CHECKED BY JCS

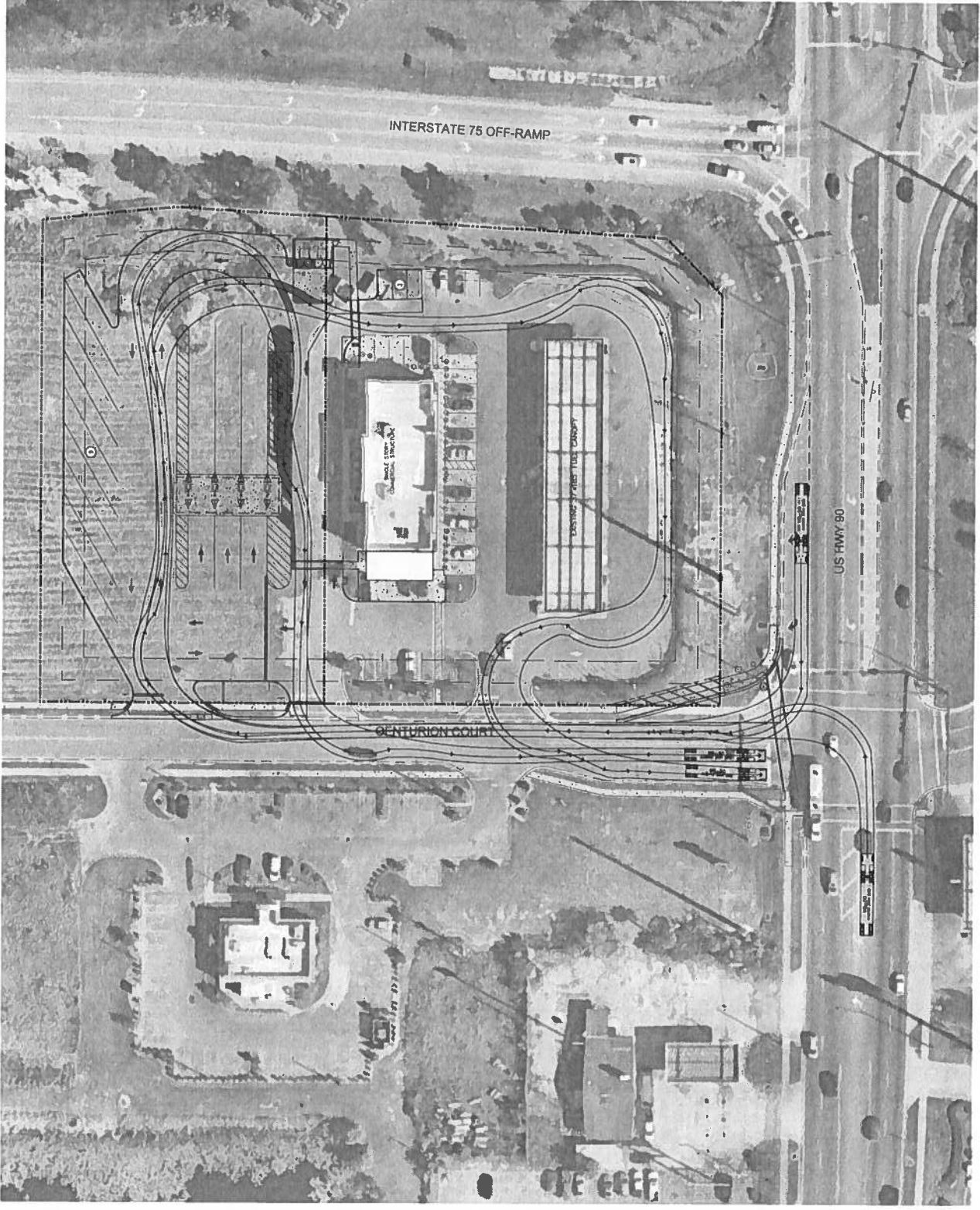
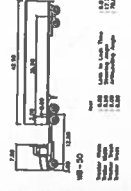


Kimley»Horn
© 2022 KIMLEY-HORN AND ASSOCIATES, INC.
180 S ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801
PHONE: 407-888-1318
WWW.KIMLEY-HORN.COM REGISTRY NO. 33104

NO.	REVISIONS	DATE

GRAPHIC SCALE IN FEET

0 15 30 45



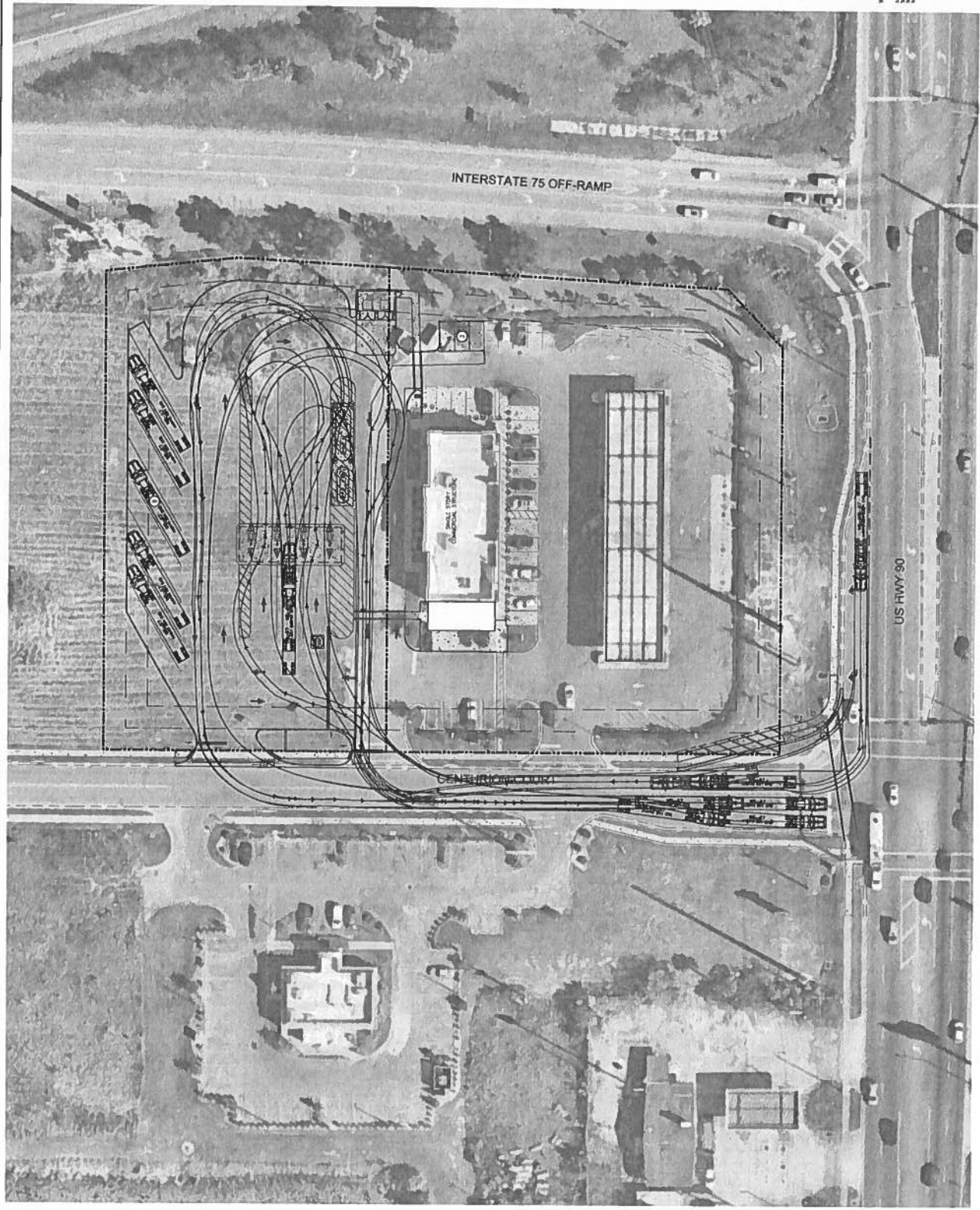
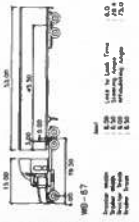
CITY OF LAKE CITY
FLORIDA
**CIRCLE K - US HWY
90 & I-75 FUEL
EXPANSION**

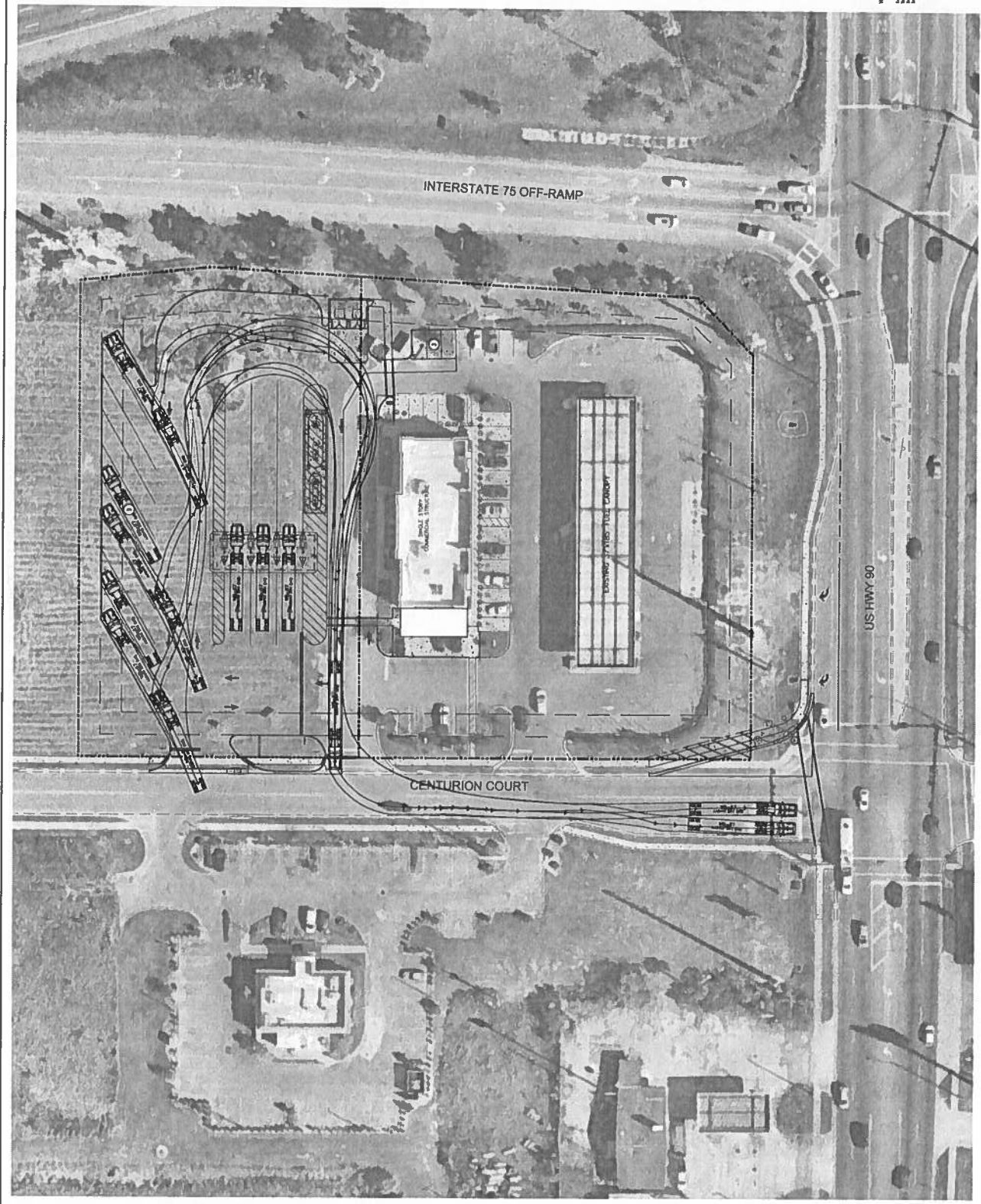
**TRUCK TURNING
MOVEMENTS**

Kimley»Horn
183 S. ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801
PH: 407-881-1111
WWW.KIMLEY-HORN.COM REGISTERED PROFESSIONAL ENGINEERS

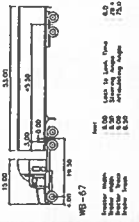
DATE: 05/24/2022
PROJECT: 14988040
DRAWN BY: JCS
CHECKED BY: JCS
DESIGNED BY: JCS
SCALE: AS SHOWN

NO.	REVISIONS	DATE





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CITY OF LAKE CITY
 EXPANSION
 90 & I-75 FUEL
 CIRCLE K - US HWY
 FLORIDA

C4.5 - TRUCK
 TURNING
 MOVEMENTS

DATE: 05/04/2022
 SCALE: AS SHOWN
 DESIGNED BY: CJ
 DRAWN BY: CJ
 CHECKED BY: JCS
 PROJECT: 14980040

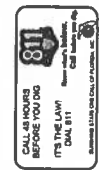
Kimley»Horn
 189 S ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801
 PHONE: 407-888-1211
 WWW.KIMLEY-HORN.COM REGISTERED PROFESSIONAL ENGINEERS

NO.	REVISIONS	DATE	BY

This drawing, together with the reports and other printed forms, is an integral part of the project. It is to be used only in the specific project and shall not be used for any other project. The user of this drawing shall be responsible for any errors and omissions. The user shall verify the accuracy of the data and information provided on this drawing and shall be responsible for any errors and omissions. The user shall verify the accuracy of the data and information provided on this drawing and shall be responsible for any errors and omissions.



NO.	REVISIONS	DATE

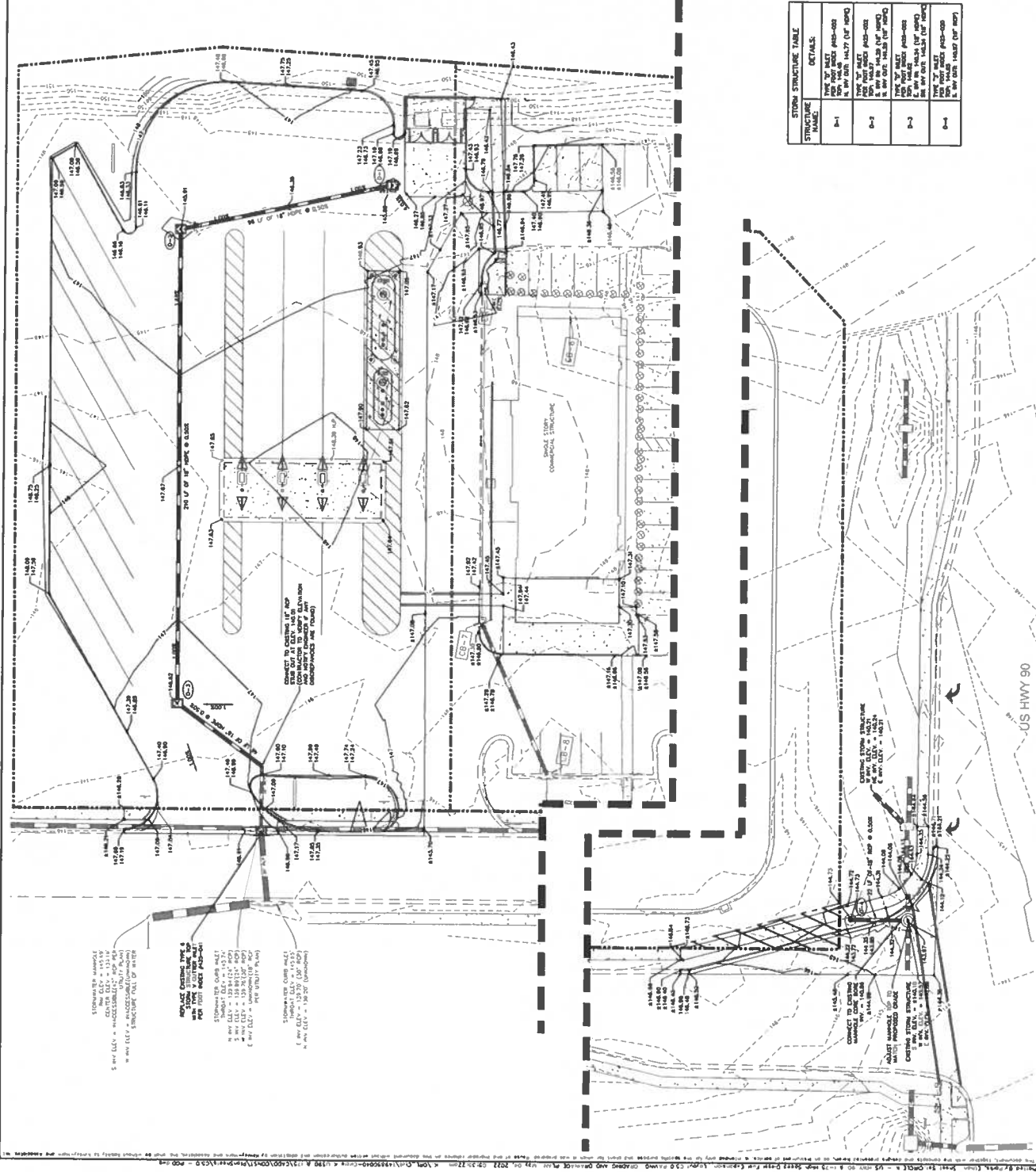
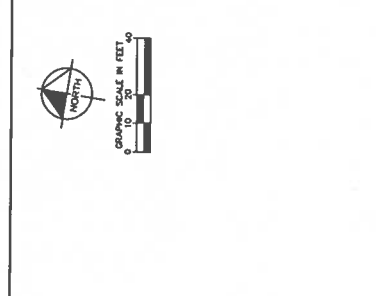


ALL EXISTING UTILITIES ARE APPROXIMATE TO THE 811 RECORDS. CONTRACTOR SHALL VERIFY THE LOCATION AND DEPTH OF ALL UTILITIES PRIOR TO CONSTRUCTION. SEE SHEET 14989040-001 FOR UTILITY LOCATIONS.

- LEGEND**
- PROPOSED STORM PIPE
 - PROPOSED STORM MANHOLE
 - PROPOSED STORM PILET
 - PROPOSED STORM PILET WITH 18" DIA. 10' DEEP
 - PROPOSED STORM PILET WITH 18" DIA. 12' DEEP
 - EXISTING STORM PIPE
 - EXISTING STORM MANHOLE
 - EXISTING STORM PILET

STORM STRUCTURE NAME	DETAILS
S-1	18" DIA. 10' DEEP 18" DIA. 12' DEEP 18" DIA. 14' DEEP
S-2	18" DIA. 10' DEEP 18" DIA. 12' DEEP 18" DIA. 14' DEEP
S-3	18" DIA. 10' DEEP 18" DIA. 12' DEEP 18" DIA. 14' DEEP
S-4	18" DIA. 10' DEEP 18" DIA. 12' DEEP 18" DIA. 14' DEEP

- NOTES:**
- ALL STORM PILES LABELED "NEW" SHALL BE 18" DIA. WITH APPROXIMATELY 10' DEPTH UNLESS OTHERWISE NOTED.
 - ALL STORM PILES LABELED "EXISTING" SHALL BE 18" DIA. WITH APPROXIMATELY 10' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 10' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 12' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 14' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 16' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 18' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 20' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 22' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 24' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 26' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 28' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 30' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 32' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 34' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 36' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 38' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 40' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 42' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 44' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 46' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 48' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 50' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 52' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 54' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 56' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 58' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 60' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 62' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 64' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 66' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 68' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 70' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 72' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 74' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 76' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 78' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 80' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 82' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 84' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 86' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 88' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 90' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 92' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 94' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 96' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 98' DEPTH UNLESS OTHERWISE NOTED.
 - PROPOSED STORM PILES SHALL BE 18" DIA. WITH APPROXIMATELY 100' DEPTH UNLESS OTHERWISE NOTED.



CIRCLE K - US HWY
90 & I-75 FUEL
EXPANSION
UTILITY PLAN

UTILITY PLAN

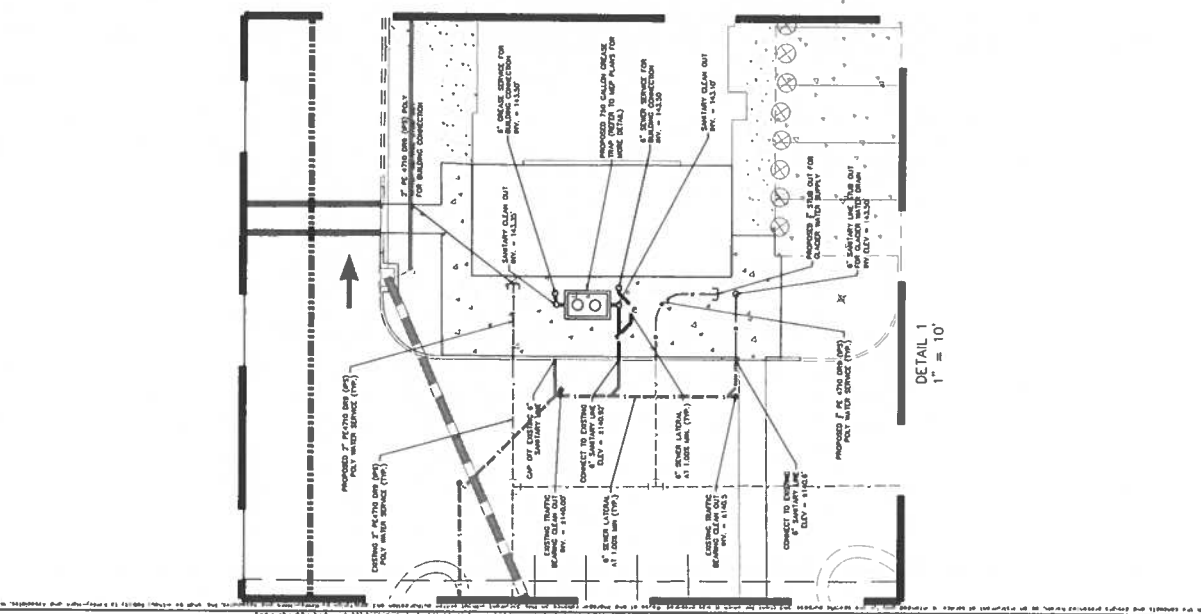
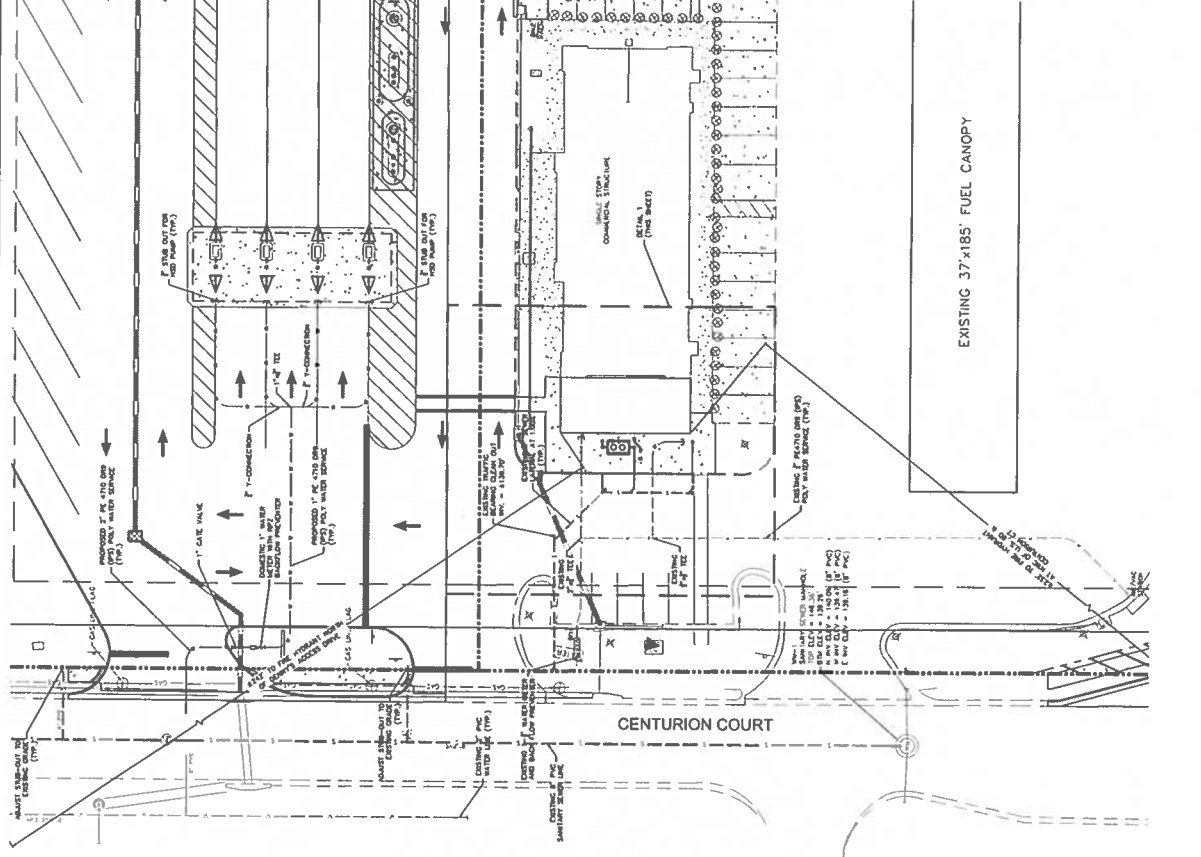
Kimley-Horn
180 S Orange Avenue, Suite 1000, Orlando, FL 32801
www.kimley-horn.com
CDP REGISTRY # 20108

NO.	REVISIONS	DATE

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www.floridaclear.com

SECTION 91.06
CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE LOCATION AND DEPTH OF ALL UTILITIES PRIOR TO CONSTRUCTION OF THE PROJECT.
PAGE 11 OF 11

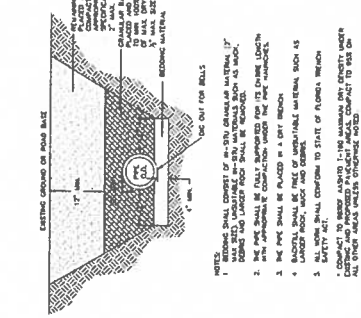
- NOTES:**
1. ALL EXISTING UTILITIES SHALL BE PRIVATELY OWNED AND MAINTAINED.
 2. ALL EXISTING UTILITIES SHALL BE IDENTIFIED BY THE CONTRACTOR AND SHOWN ON THE UTILITY PLAN AND NOT BE SUBJECT TO ANY UNWARRANTED LITIGATION.
 3. REFER TO ADDITIONAL UTILITY NOTES AND DETAILS ON SHEETS C1.6 AND C2.6.
 4. ALL WATER MAINS AND SANITARY LINES SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE LATEST EDITIONS OF THE FLORIDA PLUMBING AND MECHANICAL CODES AND THE CITY OF ORLANDO SPECIFICATIONS.
 5. EXISTING AND PROPOSED UTILITIES SHALL BE IDENTIFIED BY THE CONTRACTOR AND SHOWN ON THE UTILITY PLAN AND NOT BE SUBJECT TO ANY UNWARRANTED LITIGATION.
 6. ALL UTILITIES SHALL BE IDENTIFIED BY THE CONTRACTOR AND SHOWN ON THE UTILITY PLAN AND NOT BE SUBJECT TO ANY UNWARRANTED LITIGATION.
 7. CONTRACTOR SHALL COORDINATE ANY CONFLICTS TO EXISTING UTILITY LOCATIONS WITH ADJACENT PROPERTY OWNERS.
 8. CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE LOCATION AND DEPTH OF ALL UTILITIES PRIOR TO CONSTRUCTION OF THE PROJECT.
 9. CONTRACTOR SHALL CALL "TERRACON" STATE ONE CALL OF FLORIDA TO IDENTIFY ALL EXISTING UTILITIES BEFORE CONSTRUCTION OF THE PROJECT.
 10. CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE LOCATION AND DEPTH OF ALL UTILITIES PRIOR TO CONSTRUCTION OF THE PROJECT.
 11. CONTRACTOR SHALL COMPLY WITH THE LATEST EDITIONS OF THE FLORIDA PLUMBING AND MECHANICAL CODES AND THE CITY OF ORLANDO SPECIFICATIONS.
 12. CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE LOCATION AND DEPTH OF ALL UTILITIES PRIOR TO CONSTRUCTION OF THE PROJECT.
 13. CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE LOCATION AND DEPTH OF ALL UTILITIES PRIOR TO CONSTRUCTION OF THE PROJECT.
 14. SEE PLUMBING PLANS FOR EXACT UTILITY CONNECTION LOCATIONS AT BUILDING.
 15. CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE LOCATION AND DEPTH OF ALL UTILITIES PRIOR TO CONSTRUCTION OF THE PROJECT.
 16. CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE LOCATION AND DEPTH OF ALL UTILITIES PRIOR TO CONSTRUCTION OF THE PROJECT.
 17. CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE LOCATION AND DEPTH OF ALL UTILITIES PRIOR TO CONSTRUCTION OF THE PROJECT.
 18. CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE LOCATION AND DEPTH OF ALL UTILITIES PRIOR TO CONSTRUCTION OF THE PROJECT.
 19. CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE LOCATION AND DEPTH OF ALL UTILITIES PRIOR TO CONSTRUCTION OF THE PROJECT.
 20. ALL EXISTING, GASE, AND TELECOMMUNICATION UTILITIES FOR BUILDING SERVICE SHALL BE IDENTIFIED AND SHOWN ON THE UTILITY PLAN.



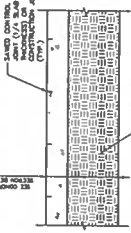
DETAIL 1
1" = 10'

Kimley-Horn
185 S ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801
WWW.KIMLEY-HORN.COM ESCOVR# 1453348

NO.	REVISIONS	DATE

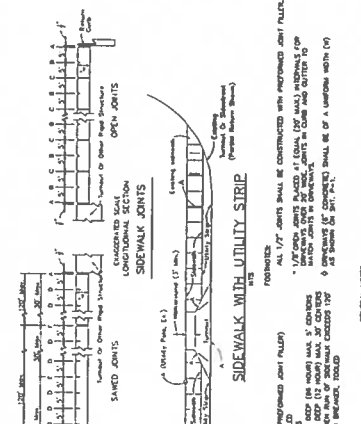


**TRENCH WITHOUT EXFILTRATION
NOT TO SCALE**



**TRENCH WITH EXFILTRATION
NOT TO SCALE**

1. SAND CONTROL SHALL BE PLACED IN 4' LIFTS.
2. ALL EXFILTRATION MATERIAL SHALL BE FINISHED TO STATE OF FLORIDA FINISH.
3. ALL EXFILTRATION MATERIAL SHALL BE FINISHED TO STATE OF FLORIDA FINISH.
4. THE FINISH SHALL BE PLACED IN 4' LIFTS.
5. ALL EXFILTRATION MATERIAL SHALL BE FINISHED TO STATE OF FLORIDA FINISH.
6. ALL EXFILTRATION MATERIAL SHALL BE FINISHED TO STATE OF FLORIDA FINISH.

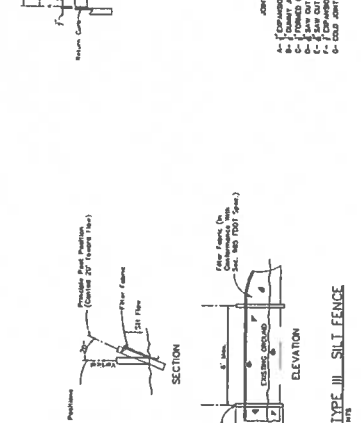


SIDEWALK WITH UTILITY STRIP



PLAN

1. LONGITUDINAL AND TRANSVERSE JOINTS SHALL BE SAVED AS INDICATED.
2. THE JOINT SEAL MANUFACTURER'S SPECIFICATIONS SHALL BE COMPATIBLE WITH THE JOINT CONSTRUCTION METHOD DETAILS FOR CONSTRUCTION AND EXPANSION JOINTS.
3. JOINTS SHALL BE FINISHED TO STATE OF FLORIDA FINISH.
4. THE WIDTH OF ALL JOINTS SHALL BE CORRECTED FOR SHrinkAGE (S).
5. CORRELS AND RE BARS FOR CONSTRUCTION JOINTS SHALL BE CAST IN PLACE.
6. ALL JOINTS SHALL BE FINISHED TO STATE OF FLORIDA FINISH.

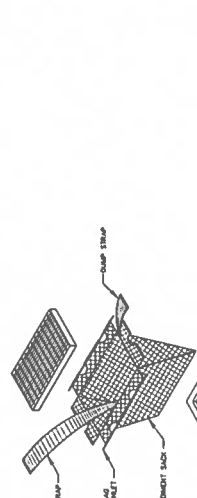


**TYPICAL SLAB AT GRADE
NOT TO SCALE**



HEAVY DUTY CONCRETE PAVEMENT DETAIL

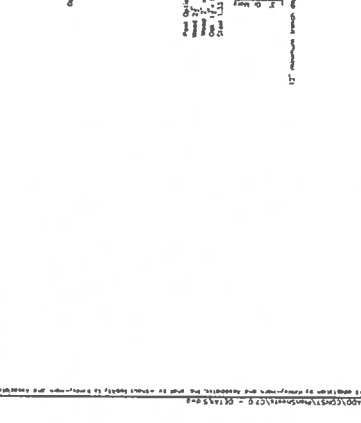
1. 12" (1 = 1 1/2" LF) FOOT TYPE SP-13 ASPHALTIC CONCRETE SHALL BE PLACED ON TOP OF 12" CONTINUOUS DRAINAGE.
2. 12" (1 = 1 1/2" LF) FOOT TYPE SP-13 ASPHALTIC CONCRETE SHALL BE PLACED ON TOP OF 12" CONTINUOUS DRAINAGE.
3. 12" (1 = 1 1/2" LF) FOOT TYPE SP-13 ASPHALTIC CONCRETE SHALL BE PLACED ON TOP OF 12" CONTINUOUS DRAINAGE.



**CONCRETE JOINTING DETAIL
NOT TO SCALE**



**STANDARD INLET SEDIMENT CONTROL DEVICE
NOT TO SCALE**

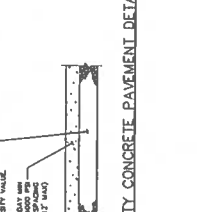


**ASPHALT PAVEMENT SECTION
NOT TO SCALE**

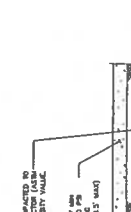


MEDIUM DUTY CONCRETE PAVEMENT DETAIL

1. 12" (1 = 1 1/2" LF) FOOT TYPE SP-13 ASPHALTIC CONCRETE SHALL BE PLACED ON TOP OF 12" CONTINUOUS DRAINAGE.
2. 12" (1 = 1 1/2" LF) FOOT TYPE SP-13 ASPHALTIC CONCRETE SHALL BE PLACED ON TOP OF 12" CONTINUOUS DRAINAGE.
3. 12" (1 = 1 1/2" LF) FOOT TYPE SP-13 ASPHALTIC CONCRETE SHALL BE PLACED ON TOP OF 12" CONTINUOUS DRAINAGE.



**CONCRETE SIDEWALK DETAIL
NOT TO SCALE**



**INSTALLATION DETAIL
NOT TO SCALE**

3/15/2022

PROJECT NUMBER
14980040

SHEET NUMBER
C8.0

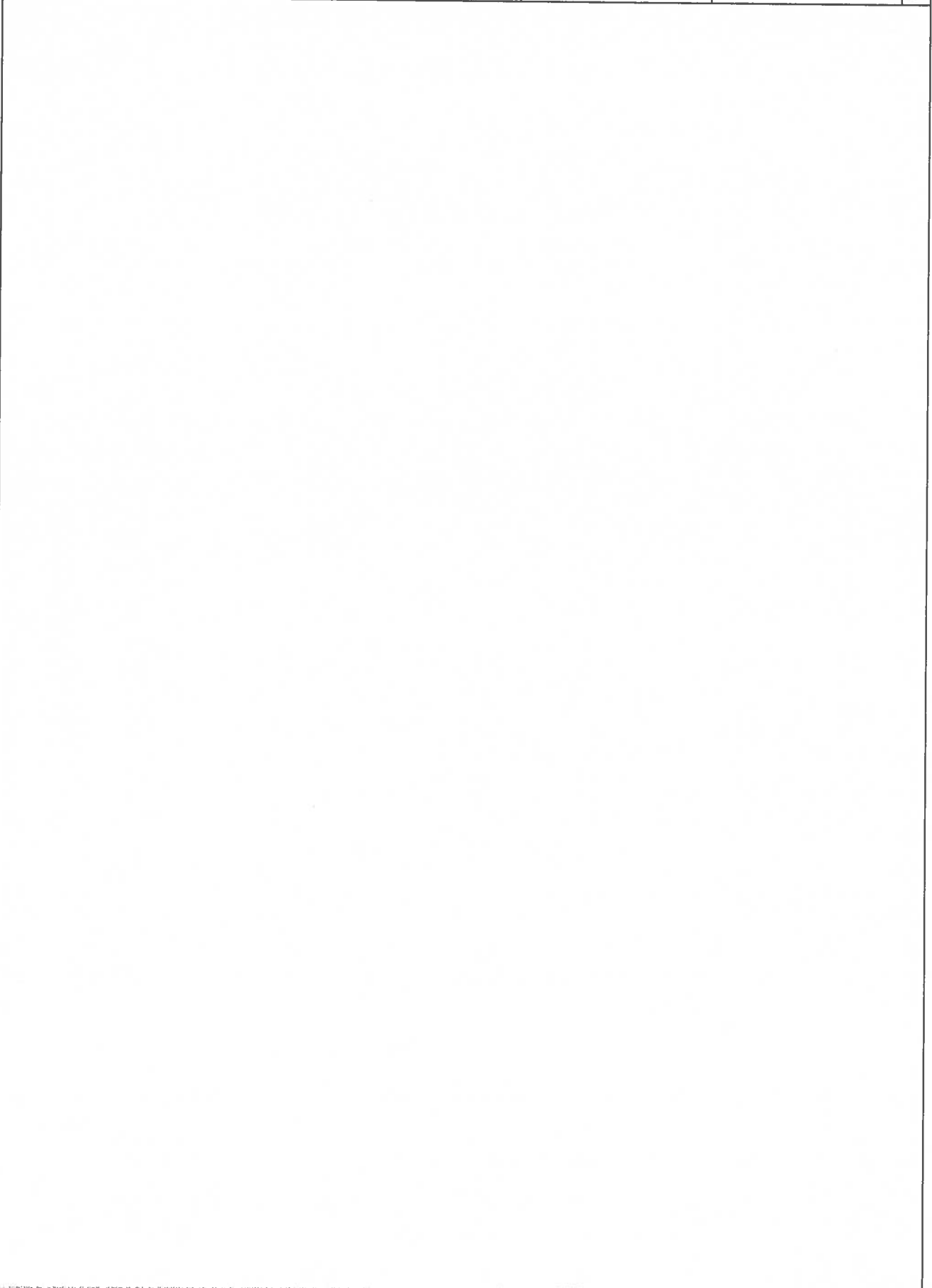
CITY OF LAKE CITY
FLORIDA
CIRCLE K - US HWY
90 & I-75 FUEL
EXPANSION

CITY OF LAKE CITY
STANDARD DETAILS

MARK PROJECT
14980040
DATE
05/04/2022
SCALE AS SHOWN
PROJECT BY: CJF
DRAWN BY: CJF
DATE
05/04/2022

Kimley»Horn
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189 S ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801
PHONE: 407-828-1511
WWW.KIMLEY-HORN.COM REGISTER #12,3108

No	REVISIONS	DATE	BY



STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION
DRAINAGE CONNECTION PERMIT

To be completed by DOT

Drainage Connection Permit No. 2022-D-292-00004 Date 4/1/2022

Received By One-Stop Permitting System Maintenance Unit _____

State Road No. _____ Work Program Project No. _____

Section No. _____ Construction Project No. _____

Milepost _____ Station _____

Instructions for Drainage Connection Permit

Pursuant to 14-86.004(6), F.A.C. "The Drainage Connection Permit form serves as the application. Once approved by the Department, the form and supporting documents become the Drainage Connection Permit."

The applicant shall submit four completed permit packages with original signatures. Each package shall include all required attachments. All required signed and sealed plans and supporting documentation shall be submitted on no larger than (11" X 17") multipurpose paper, unless larger plan sheets are requested by the reviewer. The package will include the following items. If an item does not apply to your project, indicate "Not Applicable" or "N/A."

Included	Part	Title	Completed by:	Special Instructions
	1	Permit Information Sheet	Applicant	
	2	Certification by a Licensed Professional	Licensed Professional	Signed and Sealed
	3	Certification	Applicant	Signature
	4	Owner's Authorization of a Representative	Owner	Signature
	5	Affidavit of Ownership or Control and Statement of Contiguous Interest	Owner	Signature
	6	Permit General Conditions	FDOT	
	7	Permit Special Conditions	FDOT	
	8	As-Built Certification	Licensed Professional	Signed and Sealed – Submit within 15 working days of completion of construction
	Attachment	Legal Description		
	Attachment	Photographs of Existing Conditions		
	Attachment	Location Map		
	Attachment	Grading Plan	Licensed Professional	Signed and Sealed
	Attachment	Soil Borings		
	Attachment	Water Table / Percolation		
	Attachment	Calculations		
	Attachment	CD with Electronic Files of all Submittal Items		Scanned Images in pdf format

Note: Different Licensed Professionals may complete parts of the permit package. For example the Licensed Professional signing and sealing the as-built certification may be different from the Licensed Professional who signed and sealed the calculations for the permit package.

EXCEPTIONS: Activities that qualify for an Exception are listed in Rule 14-86, F.A.C. A permit application to the Department is NOT required. However, if you desire verification whether the work qualifies for an exception, send a completed copy of this permit package with its requested information to the applicable FDOT District Office.

Approved
 2022-D-292-00004
 Troy Register
 5/18/2022

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION
DRAINAGE CONNECTION PERMIT

PART 1 – Permit Information Sheet

Select one: Permit Exception

Pursuant to 14-86.002(2), F.A.C. “Applicant means the owner of the adjacent property or the owner’s authorized representative.”

Applicant

Select one: Property Owner Owner’s Representative (Complete Part 4)

Name: JAROD STUBBS

Title and Company: Civil Engineer, KIMLEY-HORN

Address: 189 South Orange Ave Suite 1000

City: Orlando State: Florida Zip: 32801

Telephone: (407) 409-7002 ext. _____ FAX: _____ Email: jarod.stubbs@kimley-horn.com

Property Owner (If not applicant)

Name: Sammy Virani

Title and Company: N/A, Aspri Investments, LLC

Address: P.O. Box 1206

City: Kemah State: Texas Zip: 77565

Telephone: (407) 580-5173 ext. _____ FAX: _____ Email: dberry@shafferconst.com

Applicant’s Licensed Professional

Name: Jarod Stubbs Florida License Number: 89387

Title and Company: Civil Engineer, Kimley-Horn

Address: 189 South Orange Ave, Suite 1000

City: Orlando State: Florida Zip: 32801

Telephone: (407) 409-7002 ext. _____ FAX: _____ Email: jarod.stubbs@kimley-horn.com

Project Information:

Project Name: Circle K expansion

Location: SR 10

STREET	SR. NO.	US HWY NO.	CITY
<u>Columbia</u>	<u>010</u>		

COUNTY	SECTION(S)	TOWNSHIP(S)	RANGE(S)
<u>Columbia</u>	<u>010</u>		

*Geographic Coordinates: Latitude (DMS.SSS): 30.1795326958566 Longitude (DMS.SSS): -82.6916771395945

Horizontal Datum: (NAD 83 / _____ Adj.)

* State Plane Coordinates: Northing 0 Easting: 0

Projection Zone: Florida North Florida East Florida West

Coordinate shall be the center of the driveway intersection with FDOT R/W, or, if there is no driveway connection, near the center of the property line nearest the state highway.

*Check with the FDOT Office for requirement.

Approved
2022-D-292-00004
Troy Register
5/18/2022

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION
DRAINAGE CONNECTION PERMIT

Brief description of facility and proposed connection:

We are proposing modifications to an existing curb inlet and storm manhole located at the northeast corner of US 90 & Centurion Ct.

Briefly describe why this activity requires a Drainage Connection Permit (Include where the stormwater will discharge to FDOT right of way):

These modifications will be necessary as part of the proposed modification to the curb line and road expansion of the same location to accommodate for truck turning footprints of trucks up to WB-67.

Approved
2022-D-292-00004
Troy Register
5/18/2022

PART 2 – Certification by a Licensed Professional

In accordance with Rule 14-86, Florida Administrative Code (F.A.C.), I hereby certify that the following requirements are and/or will be met.

This project has been designed in compliance with all applicable water quality design standards as required by state governmental agencies.

14-86.004(3)(f) (F.A.C.): Certification by a Licensed Professional that the complete set of plans and computations complies with one of the following Rules Sections:

14-86.003(2)(a) (F.A.C.), or 14-86.003(2)(b) (F.A.C). (check one)

I further certify that a National Pollutant Discharge Elimination System (NPDES) permit for stormwater discharges associated with industrial activity from construction sites

is required is not required. (check one)

I am aware that there are significant penalties for submitting false information, including the possibility of fines and imprisonment.

This certification shall remain valid for any subsequent revision or submittal of plans, computation or other project documents by me.

Name of Licensed Professional: Jarod Stubbs

Florida License Number: 89387

Company Name (if applicable): Kimley-Horn

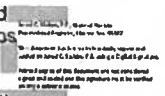
Certificate of Authorization Number (if applicable): _____

Address: 189 South Orange Ave, Suite 1000

City: Orlando State: Florida Zip: 32801

Telephone: (407) 409-7002 ext. _____ Fax: _____ Email: jarod.stubbs@kimley-horn.com

Digitally signed
by Jarod Stubbs
Date:
2022.04.01
13:41:41-04'00'



Signature of Licensed Professional

Date

(Affix Seal)

Approved
2022-D-292-00004
Troy Register
5/18/2022

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION
DRAINAGE CONNECTION PERMIT

PART 3 – Certification by Applicant

I hereby certify that the information in this permit is complete and accurate to the best of my knowledge.

Digitally signed by Jarod Stubbs
Date: 2022.04.01 13:42:07-04'00'

Applicant's Signature: _____ Date: 04/01/2022

Name (Printed): JAROD STUBBS

Title and Company: Civil Engineer, KIMLEY-HORN

Address: 189 South Orange Ave, Suite 1000 Orlando, Florida 32801

Phone Number: (407) 409-7002 ext. _____ E-mail address: jarod.stubbs@kimley-horn.com

PART 4 – Owner's Authorization of a Representative

I (we), the owner, Sammy Virani, do hereby authorize the following person, or entity, as my representative:

Name (Printed): JAROD STUBBS

Title and Company: Civil Engineer, KIMLEY-HORN

Address: 189 South Orange Ave, Suite 1000 Orlando, Florida 32801

Phone Number: (407) 409-7002 ext. _____ E-mail address: jarod.stubbs@kimley-horn.com

Part 5 – Affidavit of Property Ownership or Control and Statement of Contiguous Interest

I, Sammy Virani, certify that I own or lawfully control the following described property:

The property is located on the Northeast corner of the US 90 & Centurion Ct intersection. Parcel #35-3S-16-02524-001

Does the property owner own or have any interests in any adjacent property?

No Yes If yes, please describe. _____

Owner's Signature required for Parts 4 and/or 5

We will not begin on the drainage connection until I receive the Permit and I understand all the conditions of the Permit. When work begins on the connection, I am accepting all conditions listed in the Permit.

Name (Printed): Sammy Virani

Address: P.O. Box 1206, Kemah, Texas 77565

Phone Number: (407) 580-5173 ext. _____

Signature: [Signature] Date: March 31st, 2022

Approved
2022-D-292-00004
Troy Register
5/18/2022

PART 6 – Permit General Conditions

1. This permit is a license for permissive use only and does not convey any property rights either in real estate or material, or any exclusive privilege and it does not authorize any injury to private property or invasion of private rights, or any infringement of Federal, State or local laws, rules or regulations; nor does it obviate the necessity of obtaining any required state or local approvals.
2. The drainage connection as authorized herein shall be constructed and thereafter maintained in accordance with the documents attached hereto and incorporated by reference herein. All work performed in the Department's right of way shall be done in accordance with the most current Department standards, specifications and the permit provisions. Such construction shall be subject to the inspection and approval of the Department, and the Department may at any time make such inspections as it deems necessary to assure that the drainage connection is in compliance with this permit.
3. The entire expense of construction within the Department right of way, including replacement of existing pavement or other existing features, shall be borne by the permittee.
4. The permittee shall maintain that portion of the drainage connection authorized herein located on permittee's property in good condition. The Department shall maintain that portion of the drainage connection authorized herein located within its right of way.
5. If the drainage connection is not constructed, operated or maintained in accordance with this permit, the permit may be suspended or revoked. In this event modification or removal of any portion of the drainage connection from the Department's right of way shall be at the permittee's expense.
6. The Department reserves the right to modify or remove the drainage connection to prevent damage or in conjunction with road improvements.
7. It is understood and agreed that the rights and privileges herein set out are granted only to the extent of the Department's right, title, and interest in the land to be entered upon and used by the permittee, and the permittee will, at all times, assume all risk of and indemnify, defend and save harmless the Department from and against any and all loss, damage, cost or expense arising in any manner on account of the exercise or attempted exercises by said permittee of these rights and privileges, regardless of the respective degrees of fault of the parties.
8. Utilities, including gas lines, may exist within the right of way. Prior to beginning work the permittee shall contact Sunshine State One Call of Florida, Inc at 811 or 800-432-4770, who will notify all utility owners near the scheduled project. The utility owners have two (2) full business days to provide locations of their respective facilities. The permittee shall be solely responsible for any damage to or conflicts with gas lines, utilities and/or third persons.
9. The permittee shall notify the Department of Transportation Maintenance Office located at _____ Phone _____ 48 hours in advance of starting any work on the drainage connection authorized by this permit and also 24 hours prior to any work within the Department's right of way. Construction of any work on the right of way shall be completed within _____ days after such notification. If such construction is not completed within _____ days after such notification, the permittee shall notify the Department of the anticipated completion date.
10. This permit shall expire if construction on the drainage connection is not begun within one year from the date of approval and if construction on the drainage connection is not completed by (Date) 5/18/2023.
11. A permittee may request an extension of the Drainage Connection Permit expiration date by filing a written request for a permit time extension. All requests for time extensions must be received by the Department 15 working days prior to the expiration date.
12. All the provisions of this permit shall be binding on any assignee or successor in interest of the permittee.

Approved
 2022-D-292-00004
 Troy Register
 5/18/2022

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION
DRAINAGE CONNECTION PERMIT

PART 7 – Permit Special Conditions – To be completed by FDOT

The above request has been reviewed and has been found to meet the regulations as prescribed in Rule 14-86, F.A.C., and is hereby approved, subject to the following special conditions:

Department of Transportation:

Signature Troy Register

Title MAINTENANCE MANAGER/PERMITS

Date 5/18/2022

Approved
2022-D-292-00004
Troy Register
5/18/2022

PART 8 – As-Built Certification

Within 15 working days of completion of construction, you must send this certification to the Department office in which you filed your DOT Drainage Permit.

1. STORM WATER FACILITY INFORMATION

Permit No.: _____

Source (Project) Name: _____

Source Location: Street _____

City: _____ County: _____

Source Owner: _____

Owner Address: _____

2. AS-BUILT CERTIFICATION

I hereby certify that this storm water facility has been built substantially in accordance with the certified design plans, and that any substantial deviations (noted below) will not prevent the facility from functioning in compliance with the requirements of Chapter 14-86 F.A.C. when properly maintained and operated. These determinations have been based upon on-site observation of construction, scheduled and conducted by me or by a project representative under my direct supervision.

Name of Licensed Professional: _____

Florida License Number: _____

Company Name (if applicable): _____

Certificate of Authorization Number (if applicable): _____

Address: _____

City: _____ State: _____ Zip: _____

Telephone: _____ Fax: _____ Email: _____

Signature of Licensed Professional

Date

(Affix Seal)

Substantial deviations from the approved plans and specifications (attach additional sheets if required).

004
Troy Register
5/18/2022

PART 2 – Certification by a Licensed Professional

In accordance with Rule 14-86, Florida Administrative Code (F.A.C.), I hereby certify that the following requirements are and/or will be met.

This project has been designed in compliance with all applicable water quality design standards as required by state governmental agencies.

14-86.004(3)(f) (F.A.C.): Certification by a Licensed Professional that the complete set of plans and computations complies with one of the following Rules Sections:

14-86.003(2)(a) (F.A.C.), or 14-86.003(2)(b) (F.A.C). (check one)

I further certify that a National Pollutant Discharge Elimination System (NPDES) permit for stormwater discharges associated with industrial activity from construction sites

is required is not required. (check one)

I am aware that there are significant penalties for submitting false information, including the possibility of fines and imprisonment.

This certification shall remain valid for any subsequent revision or submittal of plans, computation or other project documents by me.

Name of Licensed Professional: Jarod Stubbs

Florida License Number: 89387

Company Name (if applicable): Kimley-Horn

Certificate of Authorization Number (if applicable): _____

Address: 189 South Orange Ave, Suite 1000

City: Orlando State: Florida Zip: 32801

Telephone: (407) 409-7002 ext. _____ Fax: _____ Email: jarod.stubbs@kimley-horn.com

Digitally signed
by Jarod Stubbs
Date:
2022.04.01
13:41:41-04'00'

Signature of Licensed Professional

Date

(Affix Seal)

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STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION
DRAINAGE CONNECTION PERMIT

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Digitally signed by Jarod Stubbs
Date: 2022.04.01
13:42:07-04'00'

Jarod S. Stubbs, P.E., State of Florida
Professional Engineer, License No. 52827
This e-signature has been electronically signed and
certified by Jarod S. Stubbs, P.E., using a digital signature
method compliant with Florida law and the federal
signed and sealed and the signature's true identity
is associated with it.

Applicant's Signature: _____ Date: 04/01/2022

Name (Printed): JAROD STUBBS

Title and Company: Civil Engineer, KIMLEY-HORN

Address: 189 South Orange Ave, Suite 1000 Orlando, Florida 32801

Phone Number: (407) 409-7002 ext. _____ E-mail address: jarod.stubbs@kimley-horn.com

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Title and Company: Civil Engineer, KIMLEY-HORN

Address: 189 South Orange Ave, Suite 1000 Orlando, Florida 32801

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No Yes If yes, please describe. _____

Owner's Signature required for Parts 4 and/or 5

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Name (Printed): Sammy Virani

Address: P.O. Box 1206, Kemah, Texas 77565

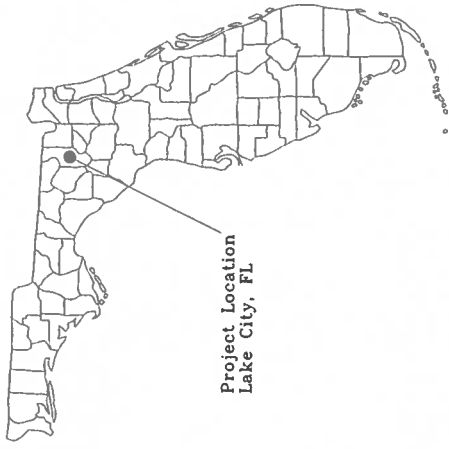
Phone Number: (407) 580-5173 ext. _____

Signature: [Signature] Date: March 31st 2022

Approved
2022-D-292-00004
Troy Register
5/18/2022

CONSTRUCTION PLANS FOR CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION

143 NW CENTURION COURT
LAKE CITY, FLORIDA 32055
MAY 4, 2022
PARCEL IDS: 35-3S-16-02524-001,
35-3S-16-02524-102 AND 35-3S-16-02524-111



LEGAL DESCRIPTION

LOT 1 AND THE SOUTH 1/4 OF LOT 11 OF LAKESHORE CHINA BEECH OF CITY 15, A SUBDIVISION OF LOT 1788000 RECORDED IN PLOT BOOK 6 PAGE 211, PUBLIC RECORDS OF CALHOUN COUNTY, FLORIDA.



CABLE:
CONTACT CABLE
CITY OF LAKE CITY UTILITIES
143 NW CENTURION COURT
LAKE CITY, FL 32055
PHONE: (813) 758-6885

TELEPHONE:
AT&T
823 LANSING ROAD
LAKE CITY, FL 32055
CONTACT: DIANE BERRY
PHONE: (407) 580-5173
EMAIL: DBERRY@SCHAFERCONST.COM

GAS:
CITY OF LAKE CITY GAS/PUBLIC WORKS
143 NW CENTURION COURT
LAKE CITY, FL 32055
CONTACT: KIMBERLY HORN
PHONE: (813) 758-6885

OWNER:
DEVELOPMENT PARTNERS LLC
2642 W HOEGEL BOULEVARD
LAKE CITY, FL 32055
CONTACT: DIANE BERRY
PHONE: (407) 580-5173
EMAIL: DBERRY@SCHAFERCONST.COM

DEVELOPER:
CIRCLE STORES, INC.
1000 W. WASHINGTON AVENUE, SUITE 1110
TAMPA, FL 33619
CONTACT: EDWARD GUNTA
PHONE: (407) 580-5173

UTILITY PROVIDERS

WATERSEWER:
CITY OF LAKE CITY UTILITIES
143 NW CENTURION COURT
LAKE CITY, FL 32055
PHONE: (813) 758-6885

ELECTRIC:
FLORIDA POWER & LIGHT
3915 NE BARON WARRIOR DRIVE
LAKE CITY, FL 32055
CONTACT: SHANE ENBANK
PHONE: (813) 754-3200

FIBER OPTIC:
AT&T
143 NW CENTURION COURT
LAKE CITY, FL 32055
CONTACT: KIMBERLY HORN
PHONE: (813) 758-6885

CIVIL ENGINEER:
KIMLEY-HORN AND ASSOCIATES, INC.
143 NW CENTURION COURT, SUITE 1000
ORLANDO, FL 32801
CONTACT: JAROD C. STUBBS, P.E.
PHONE: (407) 409-7002
EMAIL: JAROD.STUBBS@KIMLEY-HORN.COM

ARCHITECT:
ROC COLLABORATIVE
185 SOUTH ORANGE AVENUE, SUITE 1110
RESTON, VA 20190
CONTACT: MEGAN LARGENT
PHONE: (703) 668-0086
FAX: (703) 668-0085

SURVEYOR:
JERRY W. GIBSON
143 NW CENTURION COURT, SUITE 1000
ORLANDO, FL 32801
CONTACT: TROY V. WRIGHT
PHONE: (352) 375-8899

LANDSCAPE ARCHITECT:
KIMLEY-HORN AND ASSOCIATES, INC.
185 SOUTH ORANGE AVENUE, SUITE 1110
RESTON, VA 20190
CONTACT: MATTHEW FRANKO
PHONE: (407) 427-1029
EMAIL: MATT.FRANKO@KIMLEY-HORN.COM

SHEET INDEX

- C0.0 COVER SHEET
- C1.0-C1.1 GENERAL NOTES
- C2.0 STORMWATER POLLUTION PREVENTION PLAN
- C3.0-C3.1 EXISTING CONDITIONS & DEMOLITION PLAN
- C4.0 OVERALL SITE PLAN
- C4.1 SITE PLAN
- C4.2 INTERSECTION MODIFICATION PLAN
- C4.3-C4.5 TRUCK TURNING MOVEMENTS
- C5.0 PAVING, GRADING AND DRAINAGE PLAN
- C6.0 UTILITY PLAN
- C7.0-C7.1 GENERAL CONSTRUCTION DETAILS
- L1.00 LANDSCAPE PLAN
- L1.50 LANDSCAPE DETAILS
- L1.51 LANDSCAPE SPECIFICATIONS
- L2.00 SCHEMATIC IRRIGATION PLAN
- L2.50 IRRIGATION DETAILS
- L2.51 IRRIGATION NOTES

Professional Engineer Seal for James C. Stubbs, P.E., State of Florida, License No. 14884. Project No. 14884-040.



PREPARED BY
Kimley-Horn
143 NW CENTURION COURT, SUITE 1000
ORLANDO, FL 32801
PHONE: (407) 899-1311
WWW.KIMLEY-HORN.COM

Kimley-Horn & Associates, Inc. 113 S Orange Ave, Suite 1000, Orlando, FL 32801

Table with columns: DATE, REVISIONS



DEWATERING NOTES

- 1. DURING THE EXCAVATION OF THE STORMWATER FACILITIES... 2. UNDER NO CIRCUMSTANCES WILL THE DISCHARGE FROM THE ON-SITE DRAINAGE BE DIRECTLY DISCHARGED OFF-SITE.

DEMOLITION

- 1. CONTRACTOR SHALL SUBMIT DEMOLITION SCHEDULE TO OWNER PRIOR TO PROCEEDING WITH DEMOLITION ACTIVITIES.

TREES AND VEGETATION

- 1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE MAINTENANCE OF ALL LANDSCAPING MATERIALS AND RETENTION AND TO THEIR ORIGINAL CONDITION.

AS BUILT

- 1. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR SHALL SUBMIT THE OWNER'S ENGINEER WITH COMPLETE "AS-BUILT" INFORMATION, CERTIFIED BY A REGISTERED LAND SURVEYOR.

PAVEMENT MARKING AND SIGNAGE

- 1. THE PAINTING, STRIPING, AND SIZE OF ALL SIGNS AND THEIR LETTERING SHALL COMPLY WITH THE LATEST EDITIONS OF THE U.S. DEPARTMENT OF TRANSPORTATION'S MANUAL ON UNIFORM CONTROLLED MUTUAL EXCHANGE (MUTUAL EXCHANGE) AND THE U.S. DEPARTMENT OF TRANSPORTATION'S STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.

PAVING, GRADING AND DRAINAGE

- 1. ALL PAVING SHALL BE PERFORMED IN ACCORDANCE WITH THE FLORIDA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.

PAVING/GRADING TESTING AND INSPECTION

- 1. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING ALL TESTING WITH THE SOILS ENGINEER. TESTS WILL BE REQUIRED PARALLEL WITH THE OWNER'S ENGINEER'S TESTING THAT ALL REQUIREMENTS HAVE BEEN MET.

EARTHWORK/DEMOLITION PROCEDURES

- 1. A GEOTECHNICAL ENGINEERING INVESTIGATION REPORT HAS BEEN PREPARED FOR PURPOSES OF SETTING A MINIMUM LEVEL OF WORKS TO BE AVAILABLE THROUGH THE OWNER ON THEIR SOIL TESTING COMPANY.

STORM DRAINAGE SYSTEM

- 1. STORMWATER DRAINAGE SHALL BE CONSTRUCTED PER THE REQUIREMENTS OF POLY COUNTY LAND DEVELOPMENT CODE, CHAPTER 7.

GENERAL

- 1. LOCATIONS, ELEVATIONS, AND DIMENSIONS OF EXISTING UTILITIES, STRUCTURES AND OTHER FEATURES ARE SHOWN ON THE ATTACHED DRAWINGS AND SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION.

PAVING, GRADING AND DRAINAGE

- 1. ALL PAVING SHALL BE PERFORMED IN ACCORDANCE WITH THE FLORIDA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.

PAVING/GRADING TESTING AND INSPECTION

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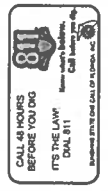
GENERAL NOTES

183 S Orange Ave, Suite 1000, Orlando, FL 32801
 PHONE: 407-848-1111
 WWW.KIMLEY-HORN.COM REGISTRY NO. 35108




DATE: 05/04/2022
 SCALE: AS SHOWN
 DRAWN BY: JLM
 CHECKED BY: JLM
 PROJECT: 14880040

DATE	REVISIONS



FDOT GENERAL NOTES

- MAINTENANCE OF TRAFFIC TO BE SUPERVISED BY A CERTIFIED PERSON.
- THE CONTRACTOR SHALL NOTIFY THE DEPARTMENT A MINIMUM OF TWO BUSINESS DAYS PRIOR TO ANY LANE CLOSURES OR BEGINNING ANY CONSTRUCTION WITHIN THE FOOT RIGHT-OF-WAY.
- ALL WORK PERFORMED WITHIN THE FOOT RIGHT-OF-WAY SHALL BE IN ACCORDANCE WITH THE F7021-22 OR CURRENT EDITION.
- IF THE SPECIALIST RECOMMENDS THAT A SCHEDULE CHANGE BE NECESSARY FROM THE APPROVED PLAN, THE PERMITEE SHALL PROVIDE AS-BUILT PLANS, ALONG WITH A RECORD DRAWING REPORT BY PERMITEE'S PROFESSIONAL ENGINEER, FORM 850-040-19, WITHIN 30 DAYS.
- IT WILL BE THE RESPONSIBILITY OF THE PERMITEE TO REPAIR ANY DAMAGE TO FOOT FACILITIES CAUSED BY THE CONSTRUCTION OF THE PROJECT.
- THE RESULTS OF ANY TESTS TAKEN FOR OR DURING CONSTRUCTION OF THE PERMITTED WORK SHALL BE PROVIDED TO THE CITY OF LAKE CITY.
- ALL CONCRETE TO BE REMOVED SHALL BE SAW CUT IN THE MIDDLE, CUT IN EACH DIRECTION, AS US TO PRODUCE A CONNECTION WITH NEW CONCRETE THAT IS FREE OF CRACKS, IMPURITY IN SHAPE, UNDESIRABLE WORK, SURFACE IRREGULARITIES, AND OTHER DEFECTS.
- ALL CONCRETE SHALL BE AN APPROVED FOOT MIX DESIGN OF 3000 PS MINIMUM.
- ALL MATERIALS INSTALLED WITHIN FOOT RIGHT-OF-WAY SHALL BE LIMITED TO THOSE ON THE FDOT'S QUALIFIED PRODUCTS LIST OR APPROVED PRODUCT LIST OF TRAFFIC CONTROL SIGNALS AND DEVICES.
- THE PERMITEE SHALL CONTACT THE CITY OF LAKE CITY TRAFFIC DEPT. (386) 758-5400.
- ALL CONSTRUCTION IN THE FOOT ROW SHALL CONFORM TO THE LATEST EDITIONS OF THE FDOT DESIGN STANDARDS, THE FDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, AND THE FDOT UTILITY ACCOMMODATION MANUAL.
- ALL DISTURBED AREAS IN FOOT ROW SHALL BE SOOLED.
- ALL WORK PERFORMED WITHIN THE FOOT RIGHT-OF-WAY SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE FDOT UTILITY ACCOMMODATION MANUAL.
- PLEASE NOTIFY JACKSONVILLE OPERATIONS TWO BUSINESS DAYS BEFORE BEGINNING WORK @ (904) 306-7500.

SANITARY SYSTEM

- ALL PIPES SHALL BE 30" OR MORE WITH 10' MINIMUM COVER. ALL 30" AND ALL AVAILABLE ASTM SPECIFICATIONS AS COVERED IN SECTION 901.2 OF ASTM D 3035. JOINTS SHALL BE A MINIMUM OF 8" GASKETED, AND LATERALS SHALL BE A MINIMUM 6" DIAMETER.
- ALL GRAVITY SEWERS MUST BE 30" OR P.V.C. DUCTILE-IRON CASSETTE JOINTS SHALL BE UTILIZED FOR PVC PIPE, AND SHALL CONFORM WITH ASTM F1771, ASTM D3526 & ASTM F979. JOINTS SHALL COMPLY WITH ASTM D3372.
- ALL 30" OR MORE DIAMETER PIPES SHALL BE 10' MINIMUM COVER.
- ALL SANITARY SEWER WORK SHALL CONFORM WITH APPLICABLE CITY OF LAKE CITY UTILITIES DEPARTMENT STANDARDS AND SPECIFICATIONS.
- PRIOR TO COMMENCING WORK WHICH REQUIRES EXISTING FACILITIES TO EXISTING USES OR ADJUSTMENTS, THE CONTRACTOR SHALL VERIFY THE LOCATION AND ELEVATIONS OF EXISTING CONNECTION POINT(S) AND NOTIFY THE OWNER'S ENGINEER OF ANY CORRECTIONS OR DISCREPANCIES.

POTABLE WATER SYSTEM

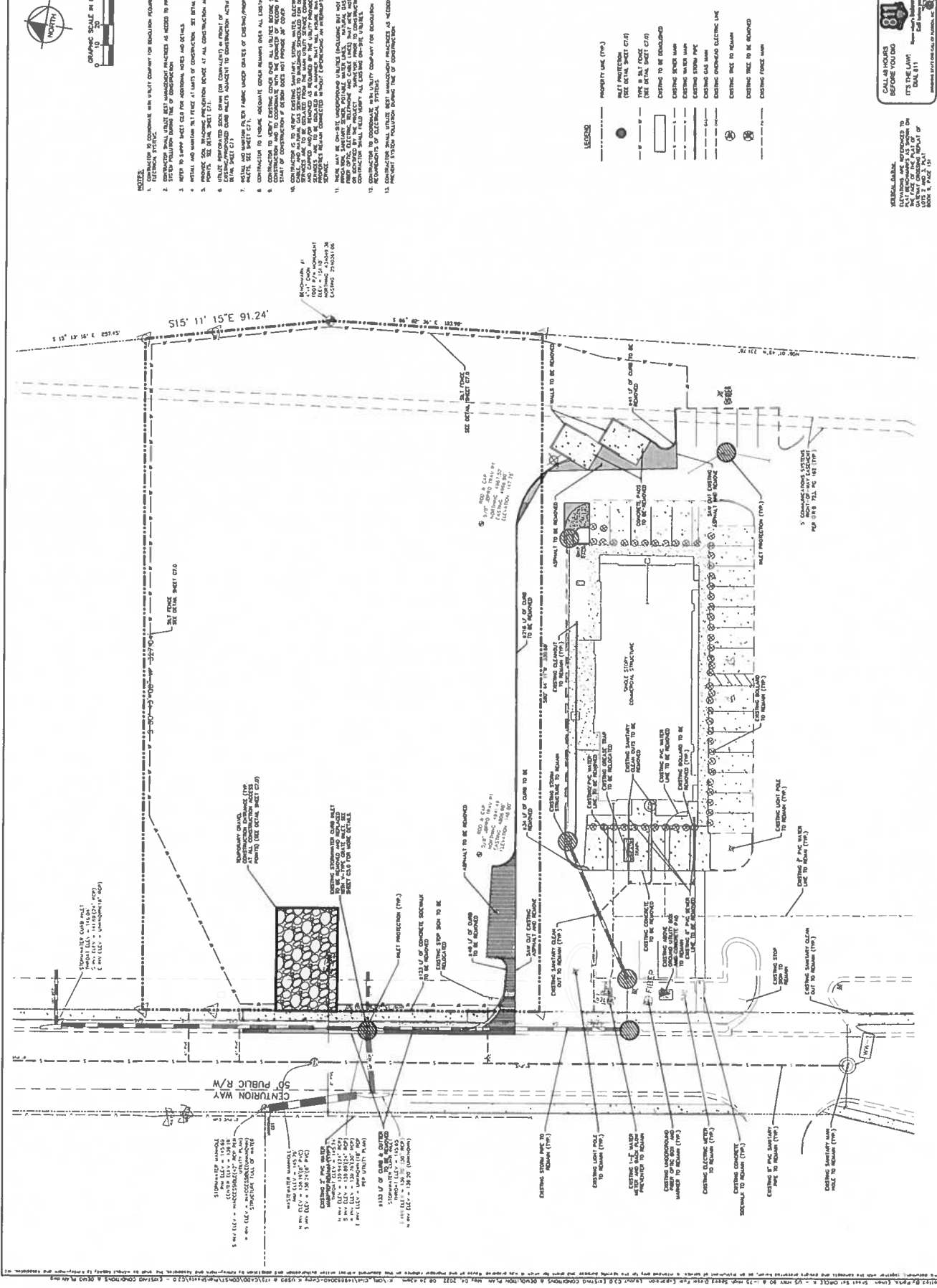
- ALL POTABLE WATER PIPING SHALL BE SUBJECT TO A VISUAL INSPECTION BY THE OWNER'S ENGINEER AND APPLICABLE SPECIFICATIONS. THE CONTRACTOR SHALL NOTIFY THE OWNER 48 HOURS IN ADVANCE TO SCHEDULE INSPECTIONS. THE CONTRACTOR SHALL PROVIDE ACCESS TO ALL EXISTING AND PROPOSED POTABLE WATER PIPING TO THE OWNER AND THE APPLICABLE JURISDICTION/AGENCY.
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POTABLE WATER TESTING AND INSPECTION

- ALL COMPONENTS OF THE WATER SYSTEM, INCLUDING FITTINGS, HYDRANTS, CONNECTIONS, AND VALVES SHALL BE PROPERLY INSPECTED AND TESTED IN ACCORDANCE WITH THE OWNER'S ENGINEER. PRESSURE TESTS TO BE IN ACCORDANCE WITH POLK COUNTY STANDARD SPECIFICATIONS FOR WATER MAINS. PRESSURE TESTS SHALL BE CONDUCTED IN ADVANCE OF ANY OTHER WORK.
- CONTRACTOR TO PROVIDE COLORIMETRY AND BACTERIOLOGICAL SAMPLING, AND OBTAIN CLEARANCE OF DOMESTIC AND INDUSTRIAL WATER USES. CONTRACTOR TO PROVIDE BACTERIOLOGICAL TEST RESULTS AND TO BE SUBMITTED TO THE OWNER'S ENGINEER FOR APPROVAL.
- ALL WATER MAINS SHALL BE PRESSURE TESTED IN ACCORDANCE WITH ANNA MANUAL VOL. 1, CONCERNING HYDROSTATIC TESTING OF PVC PIPING. OFF-SITE UTILITIES HYDROSTATIC TESTING TO BE WITNESSED BY THE CITY OF LAKE CITY WATER DEPARTMENT INSPECTOR.



- NOTES:**
1. CONTRACTOR SHALL VERIFY ALL UTILITIES WITH UTILITY COMPANIES FOR REMOVAL REQUIREMENTS OF ELECTRICAL SYSTEMS.
 2. CONTRACTOR SHALL VERIFY BEST MANAGEMENT PRACTICES AS NEEDED TO PREVENT POLLUTION FROM THIS PROJECT.
 3. CONTRACTOR SHALL VERIFY ALL UTILITIES WITH UTILITY COMPANIES FOR REMOVAL REQUIREMENTS OF ELECTRICAL SYSTEMS.
 4. CONTRACTOR SHALL VERIFY ALL UTILITIES WITH UTILITY COMPANIES FOR REMOVAL REQUIREMENTS OF ELECTRICAL SYSTEMS.
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 13. CONTRACTOR SHALL VERIFY ALL UTILITIES WITH UTILITY COMPANIES FOR REMOVAL REQUIREMENTS OF ELECTRICAL SYSTEMS.

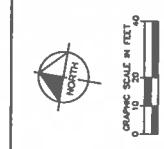
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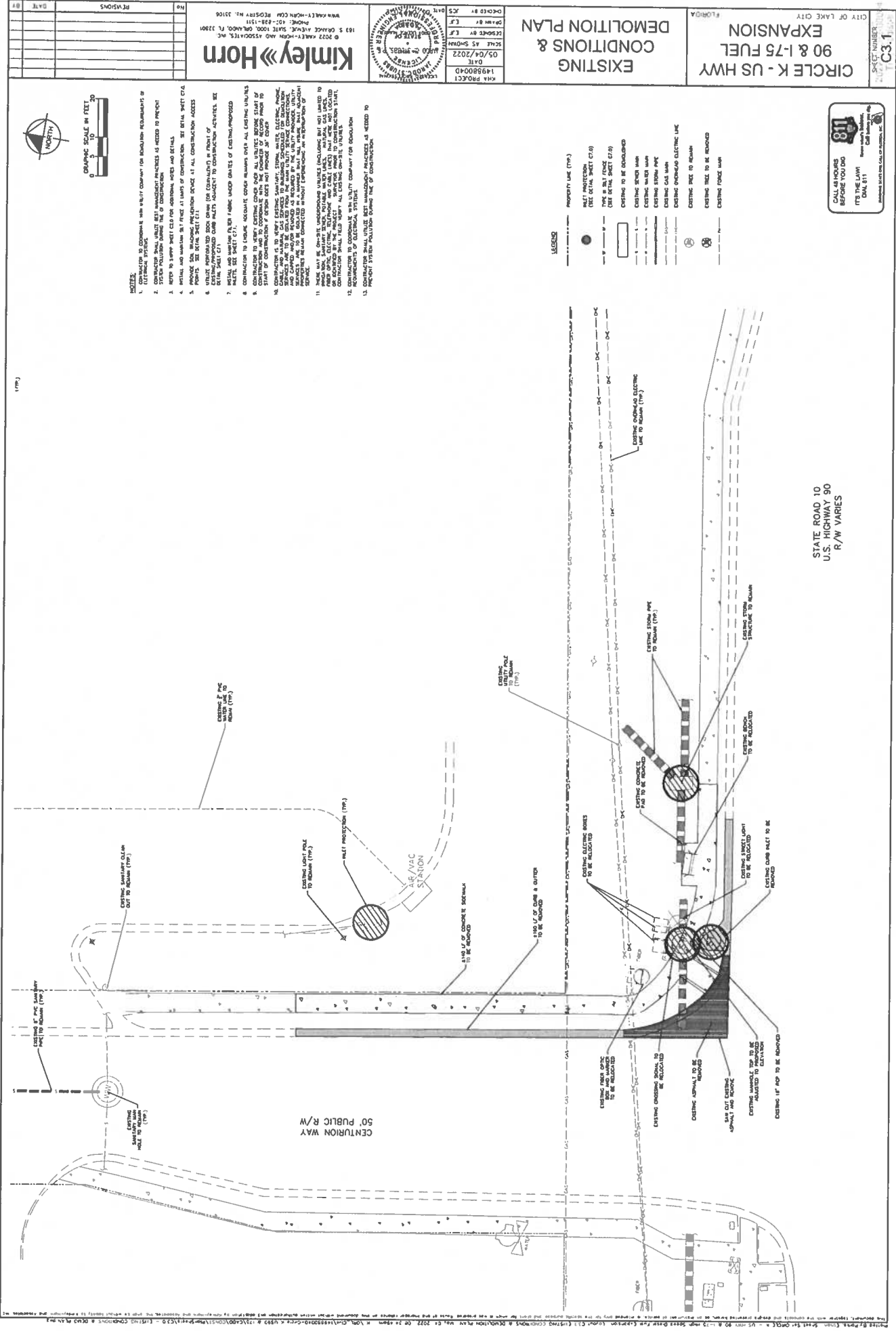
CITY OF LAKE CITY
 EXPANSION
 90 & I-75 FUEL
 CIRCLE K - US HWY

Kimley»Horn
 183 S ORANGE AVENUE, SUITE 1000, GAINESVILLE, FL 32609
 WWW.KIMLEY-HORN.COM DESIGNED BY: 21008

DATE	05/04/2022
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PROJECT	149880040
DRAWN BY	CF
CHECKED BY	CF
DATE	05/04/2022

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CIRCLE K - US HWY
90 & I-75 FUEL
EXPANSION
CITY OF LAKE CITY
FLORIDA

EXISTING
CONDITIONS &
DEMOLITION PLAN

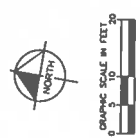
DATE	148980040
DATE	05/04/2022
SCALE	AS SHOWN
DESIGNED BY	E.F.
DRAWN BY	J.S.
CHECKED BY	J.S.



Kimley-Horn
183 S GRACE AVENUE, SUITE 1000, ORLANDO, FL 32801
PHONE: 407-281-1511
WWW.KIMLEY-HORN.COM REGISTERED NO. 21016

NO.	REVISIONS	DATE

- NOTES:**
- CONTRACTOR TO REMOVAL WITH EXISTING CONDITIONS AS SHOWN ON THIS PLAN. ALL DEMOLITION REQUIREMENTS OF THE FLORIDA DEPARTMENT OF TRANSPORTATION SHALL BE FOLLOWED.
 - CONTRACTOR SHALL VERIFY ALL UTILITY LOCATIONS AS SHOWN ON THIS PLAN. ALL UTILITIES SHALL BE PROTECTED AND MARKED PRIOR TO DEMOLITION.
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 - CONTRACTOR SHALL VERIFY ALL UTILITY LOCATIONS AS SHOWN ON THIS PLAN. ALL UTILITIES SHALL BE PROTECTED AND MARKED PRIOR TO DEMOLITION.



OVERALL SITE PLAN

DATE	05/04/2022
SCALE	AS SHOWN
DRAWN BY	E.P.
CHECKED BY	E.P.
DESIGNED BY	E.P.
PROJECT NO.	149880040
REV. PROJECT	



Kimley»Horn
 180 S ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801
 WWW.KIMLEY-HORN.COM REGISTERED NO. 23118

NO	REVISIONS	DATE



- NOTES:**
1. ALL CURB DIMENSIONS ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
 2. ALL DIMENSIONS ARE TO THE OUTSIDE FACE OF BUILDING UNLESS OTHERWISE NOTED.
 3. REFER TO ARCHITECTURAL AND STRUCTURAL PLANS TO VERIFY ALL BUILDING DIMENSIONS.
 4. REFER TO SOUPAGE PLANS FOR MOUNTAIN SIGN DETAILS.
 5. SEE MEP PLANS FOR ELECTRICAL DETAILS.
 6. ALL PROPOSED ONE-SIDE STRIPING AND PAVEMENT MARKING SHALL BE AWAY FROM EXISTING MARKING AND IN ACCORDANCE WITH FOOT CURE 711-000.
 7. REFER TO ARCHITECTURAL PLANS FOR PROPOSED SIGN COA LOCATIONS AND DETAILS.
 8. FOLLOWERS IN GENERAL ADJACENT TO BUILDING SHALL BE CONCRETE WITH RED CURB MARKING TO BE SUPPLIED BY CONTRACTOR.
 9. ALL DIMENSIONS ARE TO FACE UNLESS OTHERWISE NOTED. DIMENSIONS TO BE SUPPLIED BY CONTRACTOR FOR THE LIGHTING AND ELECTRICAL PLANS.
 10. REFER TO ARCHITECTURAL PLANS FOR THE LIGHTING AND ELECTRICAL PLANS.
 11. ALL DIMENSIONS AND PAVEMENT MARKINGS SHALL MEET MUTED AND FOOT CURE 711-000.
 12. ALL DIMENSIONS SHALL MEET THE REQUIREMENTS OF CITY OF LAKE CITY LAND DEVELOPMENT CODE, CHAPTER 12, SEC. 700.

SITE DATA:
 PROJECT AREA:
 TOTAL AREA: 117,284.5 SF (2.69 AC)
 EXISTING ZONING: O4 - COMMERCIAL, HIGHWAY INTERCHANGE DEVELOPMENT
 EXISTING USE: COMMERCIAL
 BUILDING FOOTPRINT: 117,284.5 SF (2.69 AC)
 PROPOSED: 117,284.5 SF (2.69 AC)
 EXISTING PAVEMENT AREA: 94,714.3 SF (2.16 AC)
 PROPOSED PAVEMENT AREA: 34,888.5 SF (0.80 AC)
 PROPOSED IMPROVED AREA: 34,888.5 SF (0.80 AC)
 BUILDING AREA (TOTAL): 34,888.5 SF (0.80 AC)
 APPLICABLE (TOTAL): 34,888.5 SF (0.80 AC)
 TOTAL IMPROVED AREA: 117,284.5 SF (2.69 AC)

LANDSCAPE SETTINGS

TYPE	REQUIRED	PROVIDED
SEE (HEIGHT)	13 FT	13 FT
FRONT (DEPTH)	N/A	0 FT
SEE (DEPTH)	13 FT	13 FT
SEE (WIDTH)	13 FT	13 FT
SEE (LAYOUT)	13 FT	13 FT

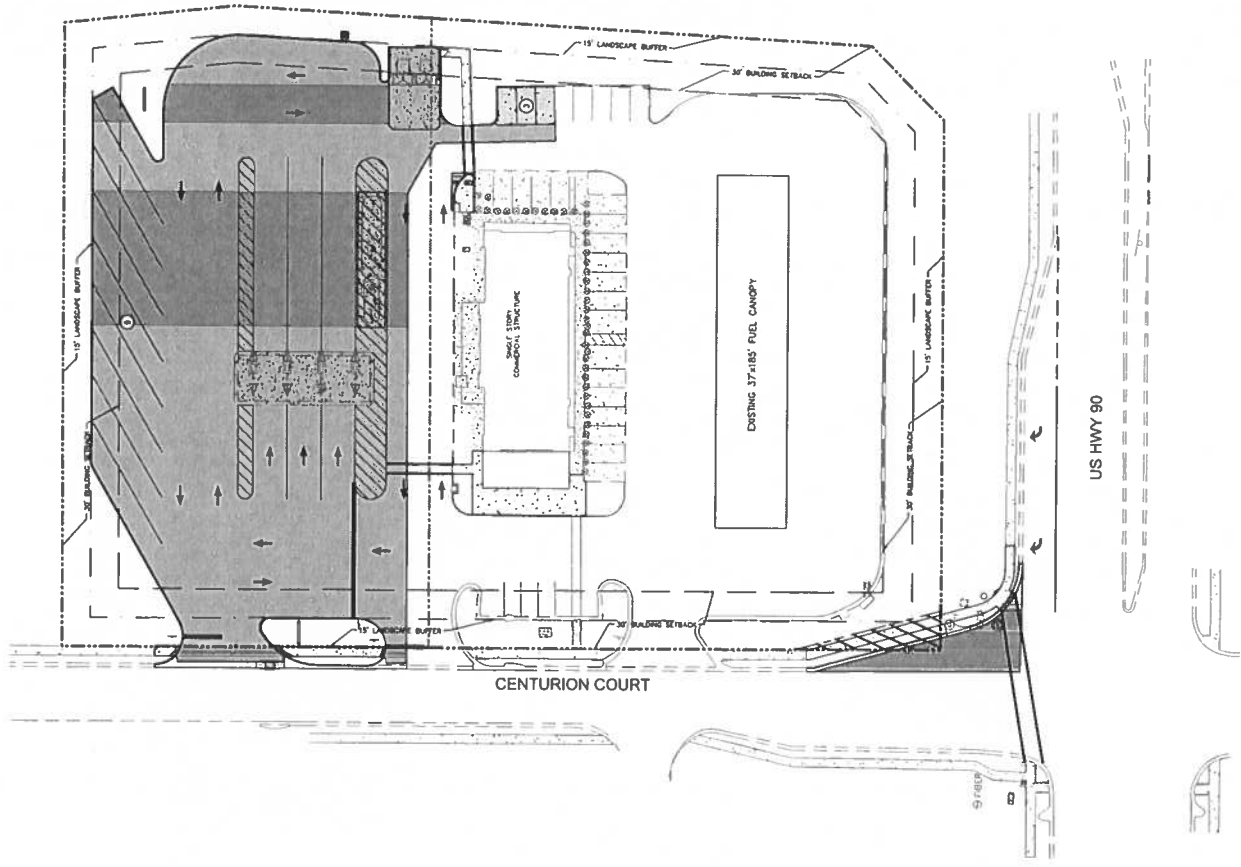
LEGEND

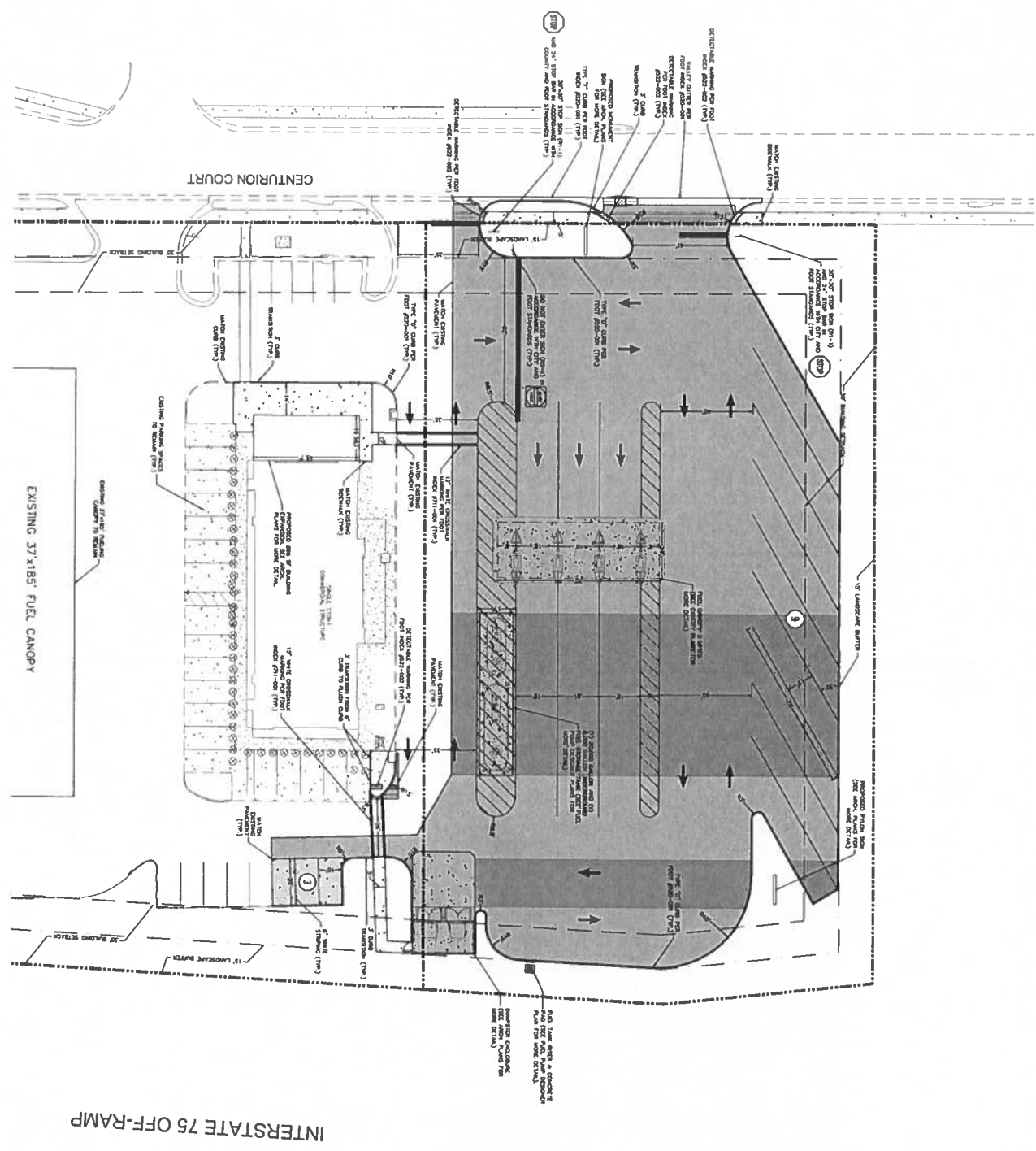
PROPERTY LINE (TYP.)	---
PROPOSED ASPHALT PAVEMENT (SEE DETAIL, SHEET C78)	---
PROPOSED CONCRETE SIDEWALK (SEE DETAIL, SHEET C79)	---
PROPOSED CONCRETE DRIVEWAY (SEE DETAIL, SHEET C79)	---
PROPOSED DRIVEWAY CONCRETE (SEE DETAIL, SHEET C79)	---
PROPOSED DRIVEWAY CONCRETE (SEE DETAIL, SHEET C79)	---

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VERTICAL CURVE
 ELEVATIONS ARE INTRODUCED TO THE FACE OF THE PAVEMENT TO BE CONFORMANT WITH THE CITY OF LAKE CITY ORDINANCE 12-000, SECTION 12-000.01, PAGE 12-000.01

INTERSTATE 75 OFF-RAMP





NOTES:

1. ALL DIMENSIONS ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
2. ALL DIMENSIONS ARE TO THE CENTER LINE OF BUILDING UNLESS OTHERWISE NOTED.
3. REFER TO ARCHITECTURAL AND STRUCTURAL PLANS TO VERIFY ALL BUILDING DIMENSIONS.
4. REFER TO SOILS REPORT FOR FOUNDATION SOIL DETAILS.
5. REFER TO POWER FOR ELECTRICAL SYMBOLS.
6. REFER TO MECHANICAL SYMBOLS FOR MECHANICAL SYMBOLS.
7. REFER TO MECHANICAL PLANS FOR MECHANICAL SYMBOLS.
8. REFER TO MECHANICAL PLANS FOR MECHANICAL SYMBOLS.
9. REFER TO MECHANICAL PLANS FOR MECHANICAL SYMBOLS.
10. REFER TO MECHANICAL PLANS FOR MECHANICAL SYMBOLS.
11. ALL SYMBOLS AND DIMENSIONS SHALL BE TO THE CENTER LINE OF THE CURB UNLESS OTHERWISE NOTED.
12. REFER TO MECHANICAL PLANS FOR MECHANICAL SYMBOLS.
13. REFER TO MECHANICAL PLANS FOR MECHANICAL SYMBOLS.
14. REFER TO MECHANICAL PLANS FOR MECHANICAL SYMBOLS.
15. REFER TO MECHANICAL PLANS FOR MECHANICAL SYMBOLS.

LEGEND

[Symbol]	PROPOSED 15' WEIR CONCRETE
[Symbol]	PROPOSED 15' WEIR CONCRETE WITH STAINLESS STEEL CLADDING
[Symbol]	PROPOSED 15' WEIR CONCRETE WITH STAINLESS STEEL CLADDING
[Symbol]	PROPOSED 15' WEIR CONCRETE WITH STAINLESS STEEL CLADDING
[Symbol]	PROPOSED 15' WEIR CONCRETE WITH STAINLESS STEEL CLADDING
[Symbol]	PROPOSED 15' WEIR CONCRETE WITH STAINLESS STEEL CLADDING
[Symbol]	PROPOSED 15' WEIR CONCRETE WITH STAINLESS STEEL CLADDING
[Symbol]	PROPOSED 15' WEIR CONCRETE WITH STAINLESS STEEL CLADDING
[Symbol]	PROPOSED 15' WEIR CONCRETE WITH STAINLESS STEEL CLADDING
[Symbol]	PROPOSED 15' WEIR CONCRETE WITH STAINLESS STEEL CLADDING

CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION

CITY OF LAKE CITY FLORIDA

SITE PLAN

KHA PROJECT 148880040

DATE 05/04/2022

SCALE AS SHOWN

DESIGNED BY E.P.

CHECKED BY JCS

Kimley»Horn

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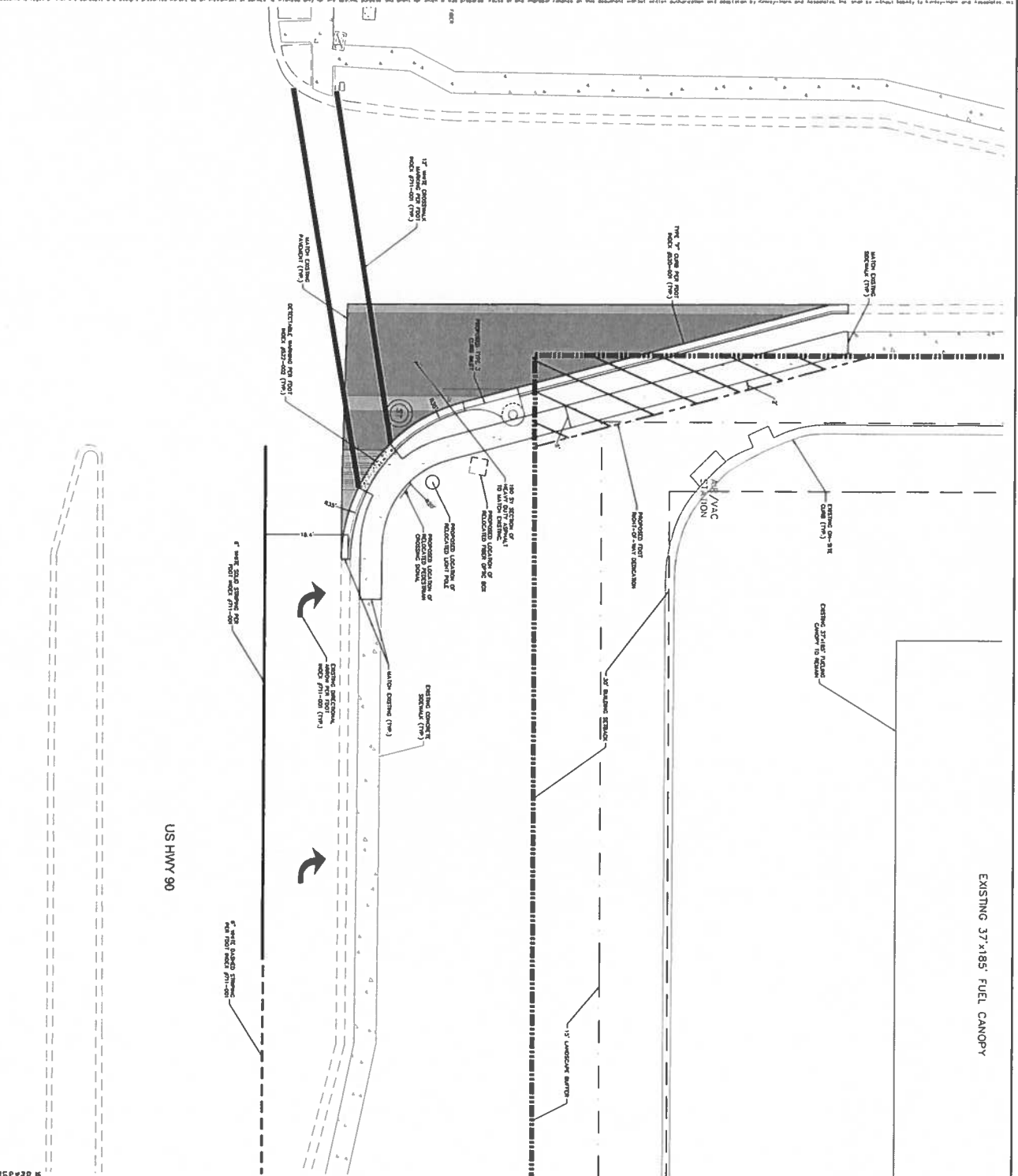
180 S. CHANCE AVENUE, SUITE 1000, GAITHERSBURG, MD 20878

PHONE: 410-497-1511

WWW.KIMLEY-HORN.COM REGDSTR. NO. 35108

DATE

REVISIONS



NOTES:
 1. ALL DIMENSIONS ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
 2. ALL CONCRETE SHALL BE 4000 PSI WITH 3% STEEL FIBERS.
 3. ALL REINFORCING SHALL BE 60,000 PSI YIELD STRENGTH BILLY BILLY REBAR.
 4. ALL STRUCTURE AND FINISHES SHALL BE AS NOTED ON SHEETS D-1000 AND D-1001.
 5. SEE SHEETS D-1000 AND D-1001 FOR FINISHES AND MATERIALS.
 6. SEE SHEETS D-1000 AND D-1001 FOR FINISHES AND MATERIALS.
 7. SEE SHEETS D-1000 AND D-1001 FOR FINISHES AND MATERIALS.

GRAPHIC SCALE IN FEET: 0, 5, 10, 20

LEGEND

PROPOSED FUEL CANOPY	PROPOSED 15' DIA. COLUMN (CONCRETE)	PROPOSED 12' DIA. COLUMN (CONCRETE)	PROPOSED CONCRETE SLAB
EXISTING FUEL CANOPY	EXISTING 15' DIA. COLUMN (CONCRETE)	EXISTING 12' DIA. COLUMN (CONCRETE)	EXISTING CONCRETE SLAB
PROPOSED ASPHALT DRIVEWAY	PROPOSED ASPHALT DRIVEWAY	PROPOSED ASPHALT DRIVEWAY	PROPOSED ASPHALT DRIVEWAY

PROPOSED 12' DIA. COLUMN (CONCRETE) SEE DETAIL SHEET D-1001

PROPOSED 15' DIA. COLUMN (CONCRETE) SEE DETAIL SHEET D-1001

PROPOSED CONCRETE SLAB SEE DETAIL SHEET D-1001

ALL DIMENSIONS ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
 ALL CONCRETE SHALL BE 4000 PSI WITH 3% STEEL FIBERS.
 ALL REINFORCING SHALL BE 60,000 PSI YIELD STRENGTH BILLY BILLY REBAR.
 ALL STRUCTURE AND FINISHES SHALL BE AS NOTED ON SHEETS D-1000 AND D-1001.
 SEE SHEETS D-1000 AND D-1001 FOR FINISHES AND MATERIALS.
 SEE SHEETS D-1000 AND D-1001 FOR FINISHES AND MATERIALS.

CIRCLE K - US HWY 90 & I-75 FUEL STATION EXPANSION

FLORIDA

PROJECT NUMBER: 1999999

SHEET NUMBER: CA2

INTERSECTION MODIFICATION PLAN

KHA PROJECT 149880040

DATE 05/04/2022

SCALE AS SHOWN

DESIGNED BY E.P.

DRAWN BY E.P.

CHECKED BY J.C.S.

CITY OF LAKE CITY

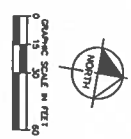
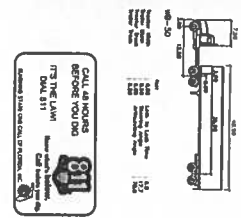
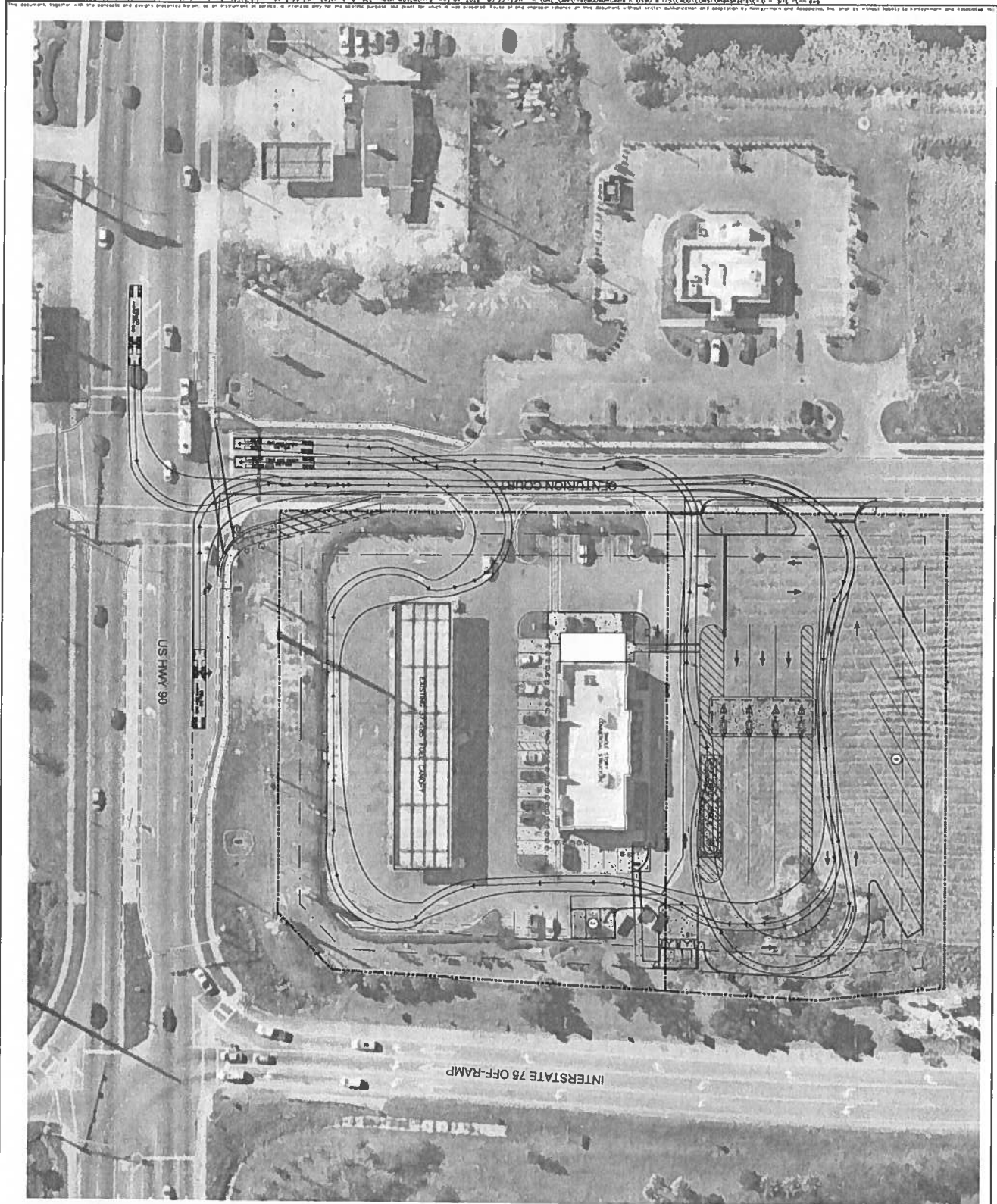
Kimley-Horn

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PHONE: 407-328-1511

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2022
CA 301
REVISIONS

TRUCK TURNING MOVEMENTS

KHA PROJECT
149880040

DATE
05/04/2022

SCALE
AS SHOWN

DESIGNED BY
E.J.

DRAWN BY
E.J.

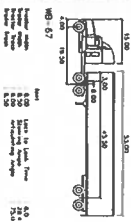
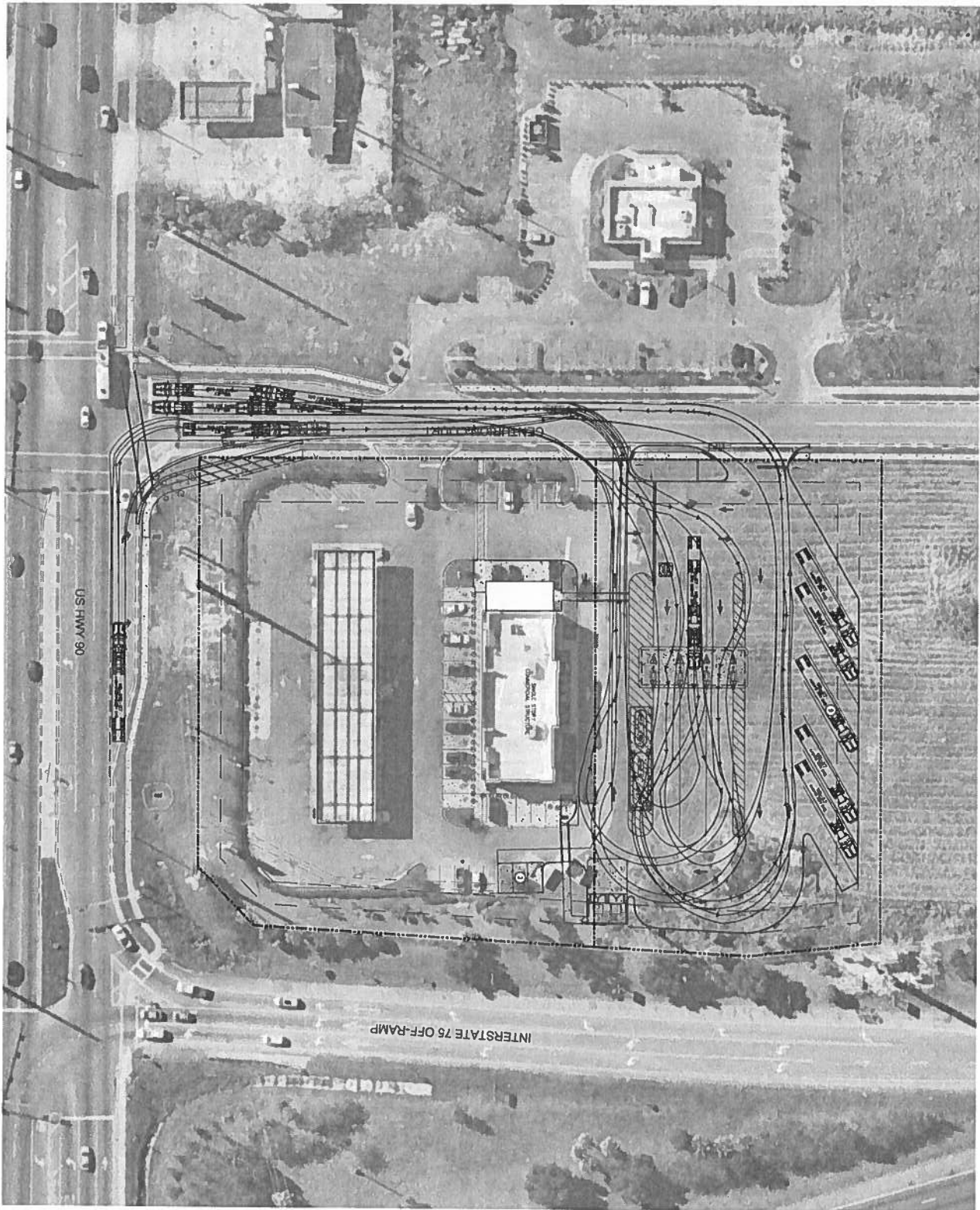
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JCS

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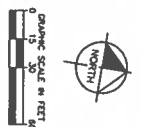


E. J. P. HORN

 PROFESSIONAL ENGINEER

 STATE OF FLORIDA

 LICENSE NO. 149880040



CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION

 CITY OF LAKE CITY FLORIDA

TRUCK TURNING MOVEMENTS

PROJECT	149880040
DATE	05/04/2022
SCALE	AS SHOWN
DESIGNED BY	EJP
DRAWN BY	EJP
CHECKED BY	JCS
DATE	

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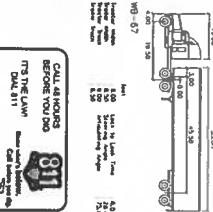
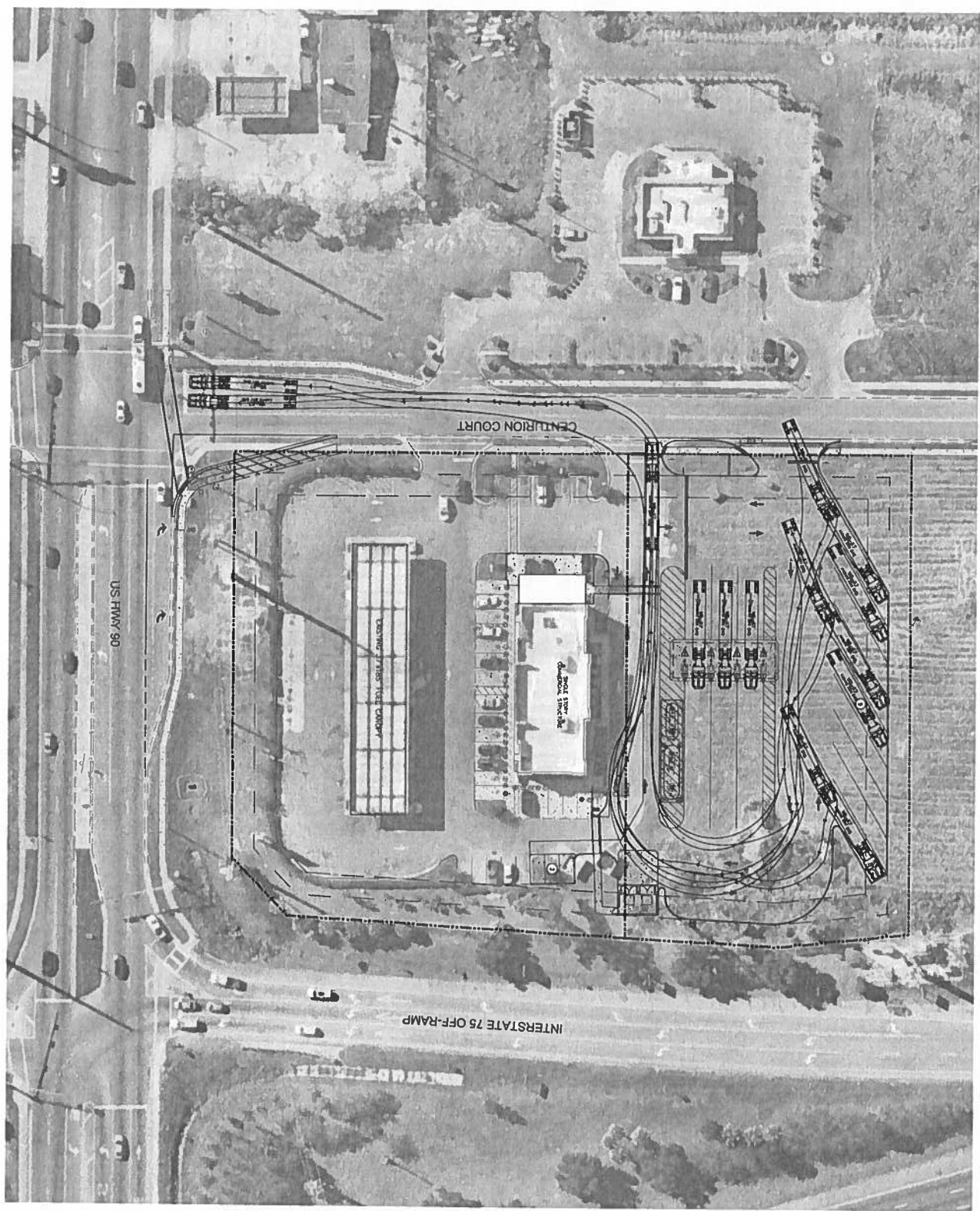
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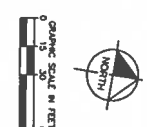
 PHONE: 407-698-1551

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CITY OF LAKE CITY, FLORIDA



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CITY OF LAKE CITY, FLORIDA

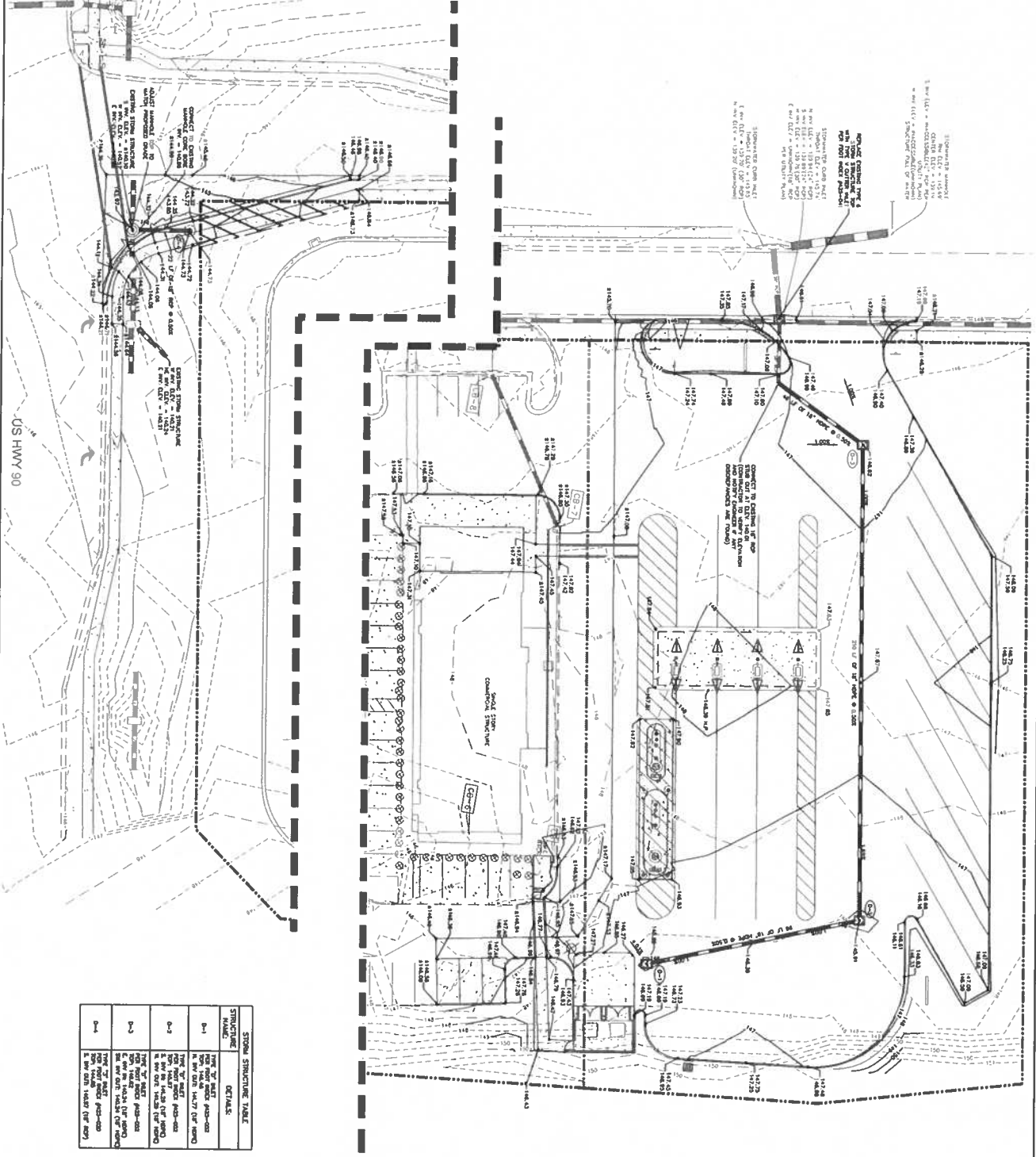
C4.5 - TRUCK TURNING MOVEMENTS

PROJECT: 149880040
DATE: 05/04/2022
SCALE: AS SHOWN
DESIGNED BY: E.J.F.
DRAWN BY: E.J.F.
CHECKED BY: JCS
DATE: 05/04/2022



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100 S. ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801
PHONE: 407-888-1231
WWW.KIMLEY-HORN.COM REGISTRY No. 35106

No.	REVISIONS	DATE	BY



STORM STRUCTURE SYMBOL	STORM STRUCTURE DETAILS
(Symbol)	TYPE 'A' STORM STRUCTURE WITH PUMP
(Symbol)	TYPE 'B' STORM STRUCTURE WITH PUMP
(Symbol)	TYPE 'C' STORM STRUCTURE WITH PUMP
(Symbol)	TYPE 'D' STORM STRUCTURE WITH PUMP
(Symbol)	TYPE 'E' STORM STRUCTURE WITH PUMP
(Symbol)	TYPE 'F' STORM STRUCTURE WITH PUMP
(Symbol)	TYPE 'G' STORM STRUCTURE WITH PUMP

- LEGEND**
- PROPERTY LINE
 - APPROVED STORM PUMP
 - PROPOSED STORM PUMP
 - PROPOSED STORM MATT
 - PROPOSED DETENTION BASIN
 - EXISTING STORM PUMP
 - EXISTING STORM MATT

NOTES:

1. ALL STORM PUMPS SHALL BE DESIGNED TO PUMP TO THE NEAREST MAJOR DRAINAGE POINT.
2. ALL STORM PUMPS SHALL BE INSTALLED AT A MINIMUM OF 3' BELOW FINISHED GRADE.
3. ALL STORM PUMPS SHALL BE INSTALLED WITH A MINIMUM OF 2' CLEARANCE TO ALL ADJACENT STRUCTURES AND UTILITIES.
4. ALL STORM PUMPS SHALL BE INSTALLED WITH A MINIMUM OF 1' CLEARANCE TO ALL ADJACENT DRIVEWAYS AND WALKWAYS.
5. ALL STORM PUMPS SHALL BE INSTALLED WITH A MINIMUM OF 1' CLEARANCE TO ALL ADJACENT SIDEWALKS AND CURBS.
6. ALL STORM PUMPS SHALL BE INSTALLED WITH A MINIMUM OF 1' CLEARANCE TO ALL ADJACENT UTILITIES.
7. ALL STORM PUMPS SHALL BE INSTALLED WITH A MINIMUM OF 1' CLEARANCE TO ALL ADJACENT EGRESS ROUTES.
8. ALL STORM PUMPS SHALL BE INSTALLED WITH A MINIMUM OF 1' CLEARANCE TO ALL ADJACENT SIGNAGE.
9. ALL STORM PUMPS SHALL BE INSTALLED WITH A MINIMUM OF 1' CLEARANCE TO ALL ADJACENT LANDSCAPING.
10. ALL STORM PUMPS SHALL BE INSTALLED WITH A MINIMUM OF 1' CLEARANCE TO ALL ADJACENT LIGHT FIXTURES.
11. ALL STORM PUMPS SHALL BE INSTALLED WITH A MINIMUM OF 1' CLEARANCE TO ALL ADJACENT FURNITURE.
12. ALL STORM PUMPS SHALL BE INSTALLED WITH A MINIMUM OF 1' CLEARANCE TO ALL ADJACENT BICYCLE PATHS.
13. ALL STORM PUMPS SHALL BE INSTALLED WITH A MINIMUM OF 1' CLEARANCE TO ALL ADJACENT TRAILERS.
14. ALL STORM PUMPS SHALL BE INSTALLED WITH A MINIMUM OF 1' CLEARANCE TO ALL ADJACENT TRUCKS.
15. ALL STORM PUMPS SHALL BE INSTALLED WITH A MINIMUM OF 1' CLEARANCE TO ALL ADJACENT BUSES.
16. ALL STORM PUMPS SHALL BE INSTALLED WITH A MINIMUM OF 1' CLEARANCE TO ALL ADJACENT PASSENGERS.

GENERAL NOTES:

1. ALL DIMENSIONS ARE REFERENCED TO THE CENTERLINE OF THE DRIVEWAY OR SIDEWALK UNLESS OTHERWISE NOTED.

2. ALL DIMENSIONS ARE REFERENCED TO THE EXTERIOR FACE OF THE CURB UNLESS OTHERWISE NOTED.

3. ALL DIMENSIONS ARE REFERENCED TO THE FINISHED GRADE UNLESS OTHERWISE NOTED.

4. ALL DIMENSIONS ARE REFERENCED TO THE UNFINISHED GRADE UNLESS OTHERWISE NOTED.

5. ALL DIMENSIONS ARE REFERENCED TO THE EXISTING GRADE UNLESS OTHERWISE NOTED.

6. ALL DIMENSIONS ARE REFERENCED TO THE PROPOSED GRADE UNLESS OTHERWISE NOTED.

7. ALL DIMENSIONS ARE REFERENCED TO THE PROPOSED FINISHED GRADE UNLESS OTHERWISE NOTED.

8. ALL DIMENSIONS ARE REFERENCED TO THE PROPOSED UNFINISHED GRADE UNLESS OTHERWISE NOTED.

9. ALL DIMENSIONS ARE REFERENCED TO THE PROPOSED EXISTING GRADE UNLESS OTHERWISE NOTED.

10. ALL DIMENSIONS ARE REFERENCED TO THE PROPOSED PROPOSED GRADE UNLESS OTHERWISE NOTED.

11. ALL DIMENSIONS ARE REFERENCED TO THE PROPOSED PROPOSED FINISHED GRADE UNLESS OTHERWISE NOTED.

12. ALL DIMENSIONS ARE REFERENCED TO THE PROPOSED PROPOSED UNFINISHED GRADE UNLESS OTHERWISE NOTED.

13. ALL DIMENSIONS ARE REFERENCED TO THE PROPOSED PROPOSED EXISTING GRADE UNLESS OTHERWISE NOTED.

14. ALL DIMENSIONS ARE REFERENCED TO THE PROPOSED PROPOSED PROPOSED GRADE UNLESS OTHERWISE NOTED.

15. ALL DIMENSIONS ARE REFERENCED TO THE PROPOSED PROPOSED PROPOSED FINISHED GRADE UNLESS OTHERWISE NOTED.

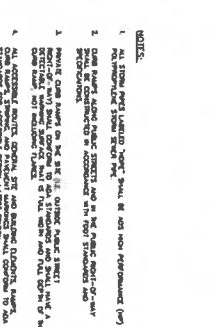
16. ALL DIMENSIONS ARE REFERENCED TO THE PROPOSED PROPOSED PROPOSED UNFINISHED GRADE UNLESS OTHERWISE NOTED.

17. ALL DIMENSIONS ARE REFERENCED TO THE PROPOSED PROPOSED PROPOSED EXISTING GRADE UNLESS OTHERWISE NOTED.

18. ALL DIMENSIONS ARE REFERENCED TO THE PROPOSED PROPOSED PROPOSED PROPOSED GRADE UNLESS OTHERWISE NOTED.

19. ALL DIMENSIONS ARE REFERENCED TO THE PROPOSED PROPOSED PROPOSED PROPOSED FINISHED GRADE UNLESS OTHERWISE NOTED.

20. ALL DIMENSIONS ARE REFERENCED TO THE PROPOSED PROPOSED PROPOSED PROPOSED UNFINISHED GRADE UNLESS OTHERWISE NOTED.



CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION
 CITY OF LAKE CITY FLORIDA

PAVING, GRADING AND DRAINAGE PLAN

PROFESSIONAL SEAL

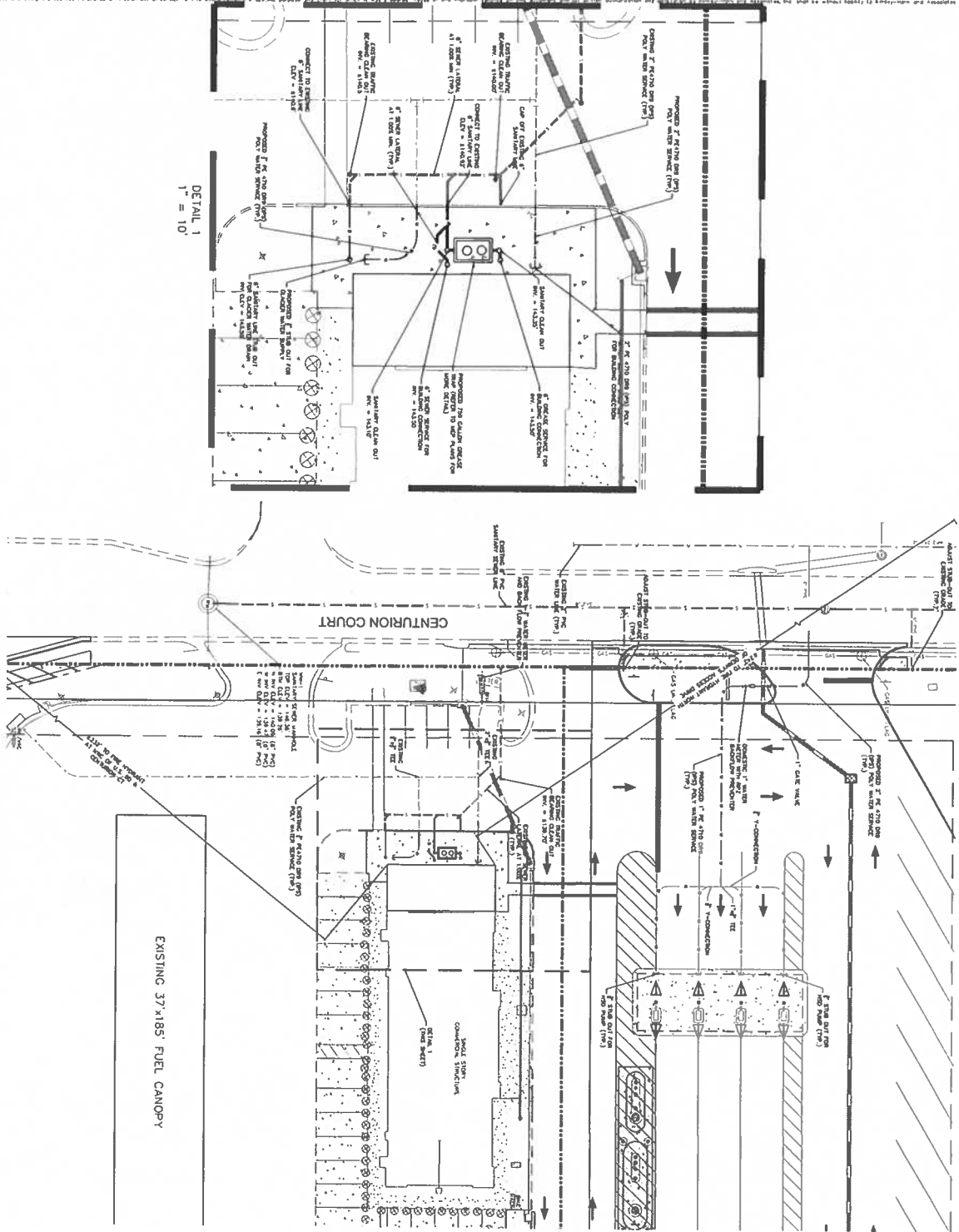
Kimley-Horn and Associates, Inc.
 State of Florida, License No. 11111
 E. F. [Signature]
 Engineer

DATE: 05/04/2022
 SCALE: AS SHOWN
 DESIGNED BY: E.F.
 DRAWN BY: B.J.
 CHECKED BY: M.C.

Kimley-Horn

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 180 S. ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801
 PHONE: 407-688-1211
 WWW.KIMLEY-HORN.COM REDSTRY No. 35106

No.	REVISIONS	DATE	BY



DETAIL 1
1" = 10'

EXISTING 37'x185' FUEL CANOPY

- NOTES:**
1. ALL CONDUIT UTILITIES SHALL BE PERMANENTLY OWNED AND MAINTAINED.
 2. UTILITIES SHALL BE LOCATED AS SHOWN ON THIS PLAN.
 3. UTILITIES SHALL BE LOCATED AS SHOWN ON THIS PLAN UNLESS OTHERWISE NOTED.
 4. ALL UTILITIES SHALL BE LOCATED AS SHOWN ON THIS PLAN UNLESS OTHERWISE NOTED.
 5. ALL UTILITIES SHALL BE LOCATED AS SHOWN ON THIS PLAN UNLESS OTHERWISE NOTED.
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 19. ALL UTILITIES SHALL BE LOCATED AS SHOWN ON THIS PLAN UNLESS OTHERWISE NOTED.
 20. ALL UTILITIES SHALL BE LOCATED AS SHOWN ON THIS PLAN UNLESS OTHERWISE NOTED.

- LEGEND**
- PROPOSED BOUNDARY
 - PROPOSED WATER SERVICE LINE
 - EXISTING WATER SERVICE LINE
 - PROPOSED SEWER SERVICE LINE
 - EXISTING SEWER SERVICE LINE
 - PROPOSED GAS SERVICE LINE
 - EXISTING GAS SERVICE LINE
 - PROPOSED ELECTRIC SERVICE LINE
 - EXISTING ELECTRIC SERVICE LINE
 - PROPOSED FUEL SERVICE LINE
 - EXISTING FUEL SERVICE LINE
 - PROPOSED FUEL TANK
 - EXISTING FUEL TANK
 - PROPOSED FUEL CANOPY
 - EXISTING FUEL CANOPY

811
CALL BEFORE YOU DIG
FOR THE LATEST
STATEWIDE 811
SERVICE

UTILITY PLAN
DATE: 05/04/2022
PROJECT: 149580040
DRAWN BY: AS SHOKH
CHECKED BY: J. F. J.
SCALE: AS SHOWN

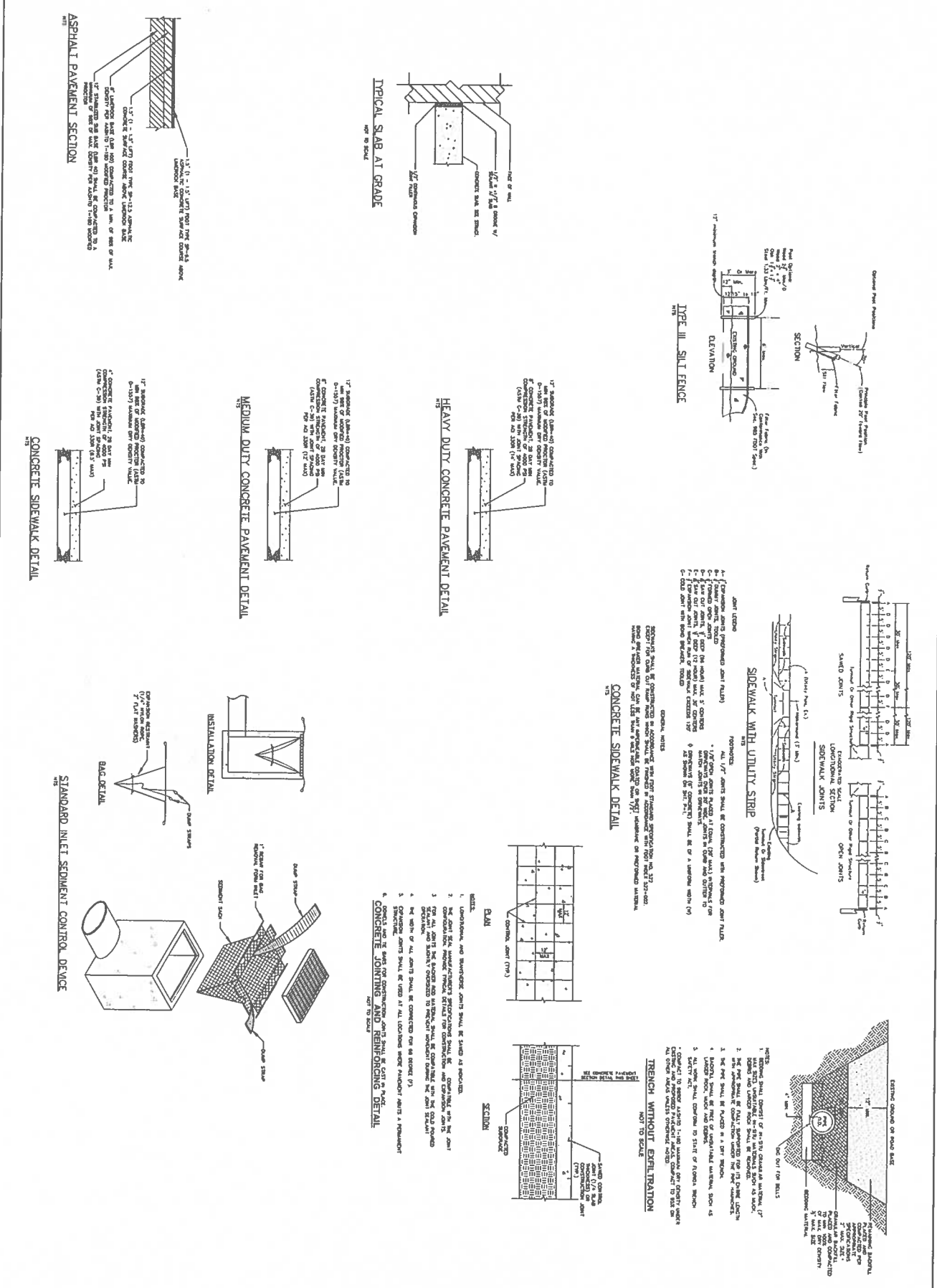
CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION
CITY OF LAKE CITY, FLORIDA

UTILITY PLAN

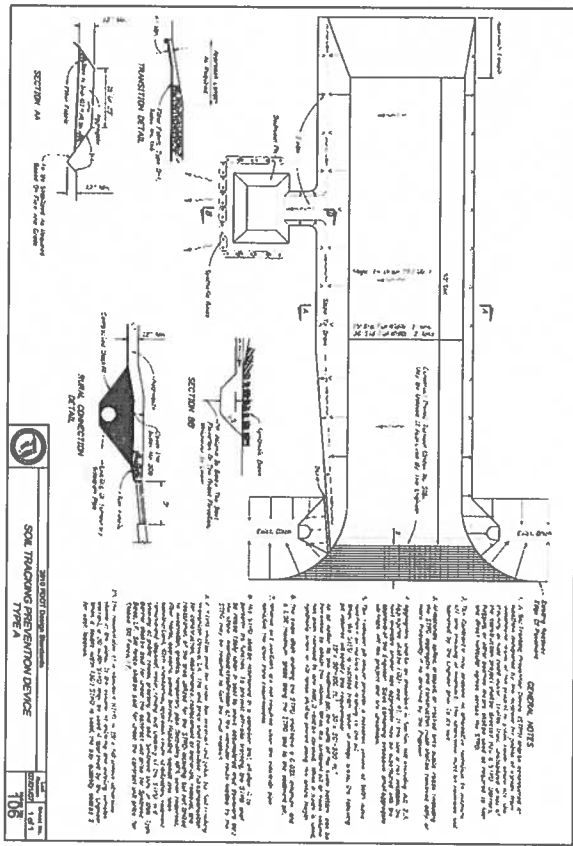
Kimley Horn
2022 KIMLEY-HORN AND ASSOCIATES, INC.
183 S ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801
PHONE: 407-628-1511
WWW.KIMLEY-HORN.COM REGISTRY NO. 35108

DATE: 05/04/2022
PROJECT: 149580040
DRAWN BY: AS SHOKH
CHECKED BY: J. F. J.
SCALE: AS SHOWN

NO.	REVISIONS	DATE



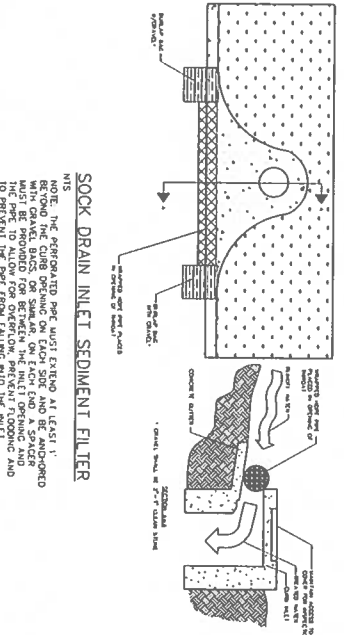
<p>CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION</p> <p>CITY OF LAKE CITY FLORIDA</p>	<p>GENERAL CONSTRUCTION DETAILS</p>	<p>PROJECT: 149880040</p> <p>DATE: 05/04/2012</p> <p>SCALE: AS SHOWN</p> <p>DESIGNED BY: [Signature]</p> <p>DRAWN BY: [Signature]</p> <p>CHECKED BY: [Signature]</p>	<p>Kimley-Horn</p> <p>© 2012 KIMLEY-HORN AND ASSOCIATES, INC. 183 S ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801 PHONE: 407-688-1511 WWW.KIMLEY-HORN.COM PROJECT NO. 12-35106</p>	<table border="1"> <tr> <th>NO.</th> <th>REVISIONS</th> <th>DATE</th> </tr> <tr> <td> </td> <td> </td> <td> </td> </tr> </table>	NO.	REVISIONS	DATE			
		NO.			REVISIONS	DATE				
<p>1. LOCATION, AND SPACING JOINTS SHALL BE SHOWN AS INDICATED.</p> <p>2. JOINTS SHALL BE CONSTRUCTED TO PROVIDE A PROPER SEAM.</p> <p>3. JOINTS SHALL BE CONSTRUCTED TO PROVIDE A PROPER SEAM.</p> <p>4. JOINTS SHALL BE CONSTRUCTED TO PROVIDE A PROPER SEAM.</p> <p>5. JOINTS SHALL BE CONSTRUCTED TO PROVIDE A PROPER SEAM.</p> <p>6. JOINTS SHALL BE CONSTRUCTED TO PROVIDE A PROPER SEAM.</p>	<p>1. JOINTS SHALL BE CONSTRUCTED TO PROVIDE A PROPER SEAM.</p> <p>2. JOINTS SHALL BE CONSTRUCTED TO PROVIDE A PROPER SEAM.</p> <p>3. JOINTS SHALL BE CONSTRUCTED TO PROVIDE A PROPER SEAM.</p> <p>4. JOINTS SHALL BE CONSTRUCTED TO PROVIDE A PROPER SEAM.</p> <p>5. JOINTS SHALL BE CONSTRUCTED TO PROVIDE A PROPER SEAM.</p> <p>6. JOINTS SHALL BE CONSTRUCTED TO PROVIDE A PROPER SEAM.</p>									



SOCK DRAINING PREVENTION DEVICE
TYPE A

GENERAL NOTES

1. See General Notes for details of construction and materials.
2. The concrete base shall be cast in place and finished to a smooth surface.
3. The gravel layer shall be 1/2" (12.5mm) in diameter and shall be placed in a single lift.
4. The fabric filter shall be 1/2" (12.5mm) thick and shall be placed in a single lift.
5. The fabric filter shall be secured to the concrete base with a fabric strap.
6. The fabric filter shall be secured to the gravel layer with a fabric strap.
7. The fabric filter shall be secured to the gravel layer with a fabric strap.
8. The fabric filter shall be secured to the gravel layer with a fabric strap.
9. The fabric filter shall be secured to the gravel layer with a fabric strap.
10. The fabric filter shall be secured to the gravel layer with a fabric strap.



SOCK DRAIN INLET SEDIMENT FILTER

NOTES:
1. THE FABRIC FILTER SHALL BE 1/2" (12.5mm) THICK AND SHALL BE PLACED IN A SINGLE LIFT.
2. THE FABRIC FILTER SHALL BE SECURED TO THE CONCRETE BASE WITH A FABRIC STRAP.
3. THE FABRIC FILTER SHALL BE SECURED TO THE GRAVEL LAYER WITH A FABRIC STRAP.
4. THE FABRIC FILTER SHALL BE SECURED TO THE GRAVEL LAYER WITH A FABRIC STRAP.
5. THE FABRIC FILTER SHALL BE SECURED TO THE GRAVEL LAYER WITH A FABRIC STRAP.
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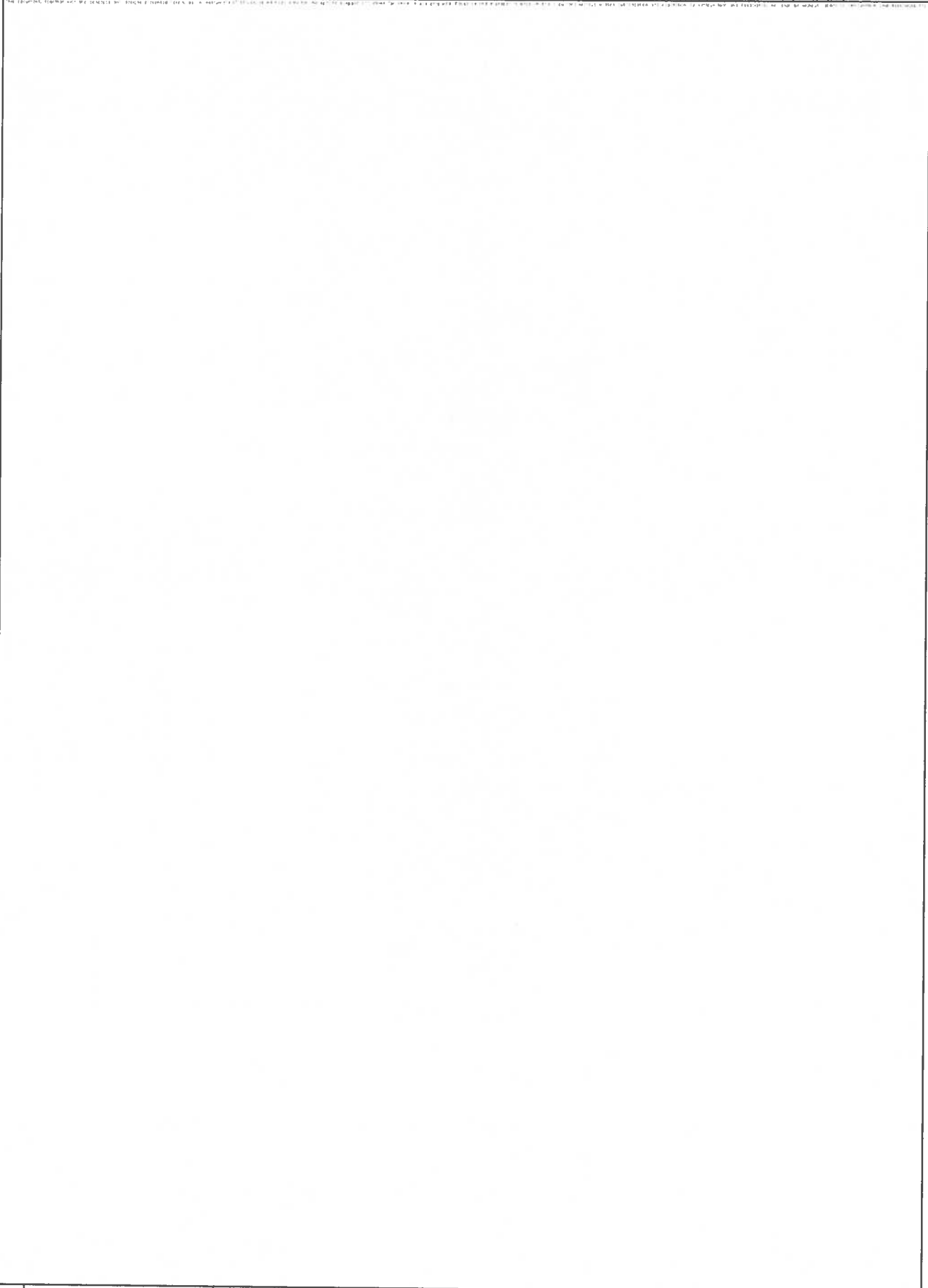
CIRCLE K - US HWY
90 & I-75 FUEL
EXPANSION
CITY OF LAKE CITY FLORIDA

GENERAL
CONSTRUCTION
DETAILS

KHA PROJECT
149880040
DATE
05/04/2022
SCALE AS SHOWN
DESIGNED BY E.J.
DRAWN BY E.J.
CHECKED BY JCS

Kimley»Horn
© 2022 KIMLEY-HORN AND ASSOCIATES, INC.
193 S ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801
407-898-1511
WWW.KIMLEY-HORN.COM REGISTERED NO. 35106

No.	REVISIONS	DATE	BY



2/21/2022
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**CIRCLE K - US HWY
90 & I-75 FUEL
EXPANSION**
 CITY OF LAKE CITY FLORIDA

**CITY OF LAKE CITY
STANDARD DETAILS**

KIM PROJECT 149880040 DATE 05/04/2022 SCALE AS SHOWN DESIGNED BY E.F. DRAWN BY E.F. CHECKED BY JCS DATE	LICENSED PROFESSIONAL
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Kimley»Horn
 © 2022 KIMLEY-HORN AND ASSOCIATES, P.C.
 183 S ORANGE AVENUE, SUITE 1000, GAITHERSBURG, MD 20878
 PHONE: 410-498-1511
 WWW.KIMLEY-HORN.COM REGISTRY No. 35106

No.	REVISIONS	DATE	BY

**BEFORE THE PLANNING AND ZONING BOARD
OF THE CITY OF LAKE CITY, FLORIDA.**

**OBJECTION TO SITE PLAN APPLICATION #SPR 22-15
OF GWC DEVELOPMENT PARTNERS LLC**

Florida Gateway Hotels, LLC and Lake City Hotels, LLC, object to the Site Plan Application SPR22-15, filed April 1, 2022 by GWC Development Partners, LLC (“Developer.”) for Lot 2, Gateway Crossings subdivision. The Site Plan Application, this Objection and public comment will be presented to the City of Lake City, Florida Planning and Zoning Board in public hearing on January 10, 2024, and January 17, 2024 if necessary.

Facts.

Florida Gateway Hotels, LLC, is a Florida limited liability company that owns real property within the Gateway Crossings subdivision, located near the northwest corner of the intersection of I-75 and US-90. Florida Gateway Hotels, LLC developed The Tru by Hilton hotel on Lot 7 located at the end of NW Centurion Court. The Tru is operated by Lake City Hotels, LLC. The principals of Florida Gateway Hotels, LLC and Lake City Hotels, LLC, Nick Patel and P.J. Patel, also have an interest in Lots 4 and 5 in Gateway Crossings, which may be developed as a hotel. They also own and operate 7 other hotels within the I-75/US Highway 90 interchange. Collectively, Florida Gateway Hotels, LLC, Lake City Hotels, LLC and the Patels are referred to herein as “Florida Gateway.”

Below is a picture of The Tru by Hilton hotel owned by Florida Gateway.



GWC Development Partners, LLC, (“Developer”), is a Florida limited liability company that is the developer of Gateway Crossings. Developer owns Lot 2 of Gateway Crossings (“Property”), which it intends to lease to Circle K to construct a new facility with diesel canopy, high flow diesel pumps, truck and tractor-trailer rig parking, and underground storage tanks to serve commercial trucks and tractor-trailer rigs, together with additional restrooms and parking at the Property (“Development”), as an expansion to the current use of Lot 1. Lot 1 of Gateway Crossings is owned by Aspri Investments LLC of Phoenix, Arizona. A

copy of the Site Plan Application (“Application”) is included in Florida Gateway’s Appendix to Objection to Site Plan Application, as Exhibit A.

On March 29, 2023, Florida Gateway timely filed a Notice of Appeal of the development order ultimately issued by the City in February, 2023. Thereafter, the City determined a quasi-judicial hearing on the Developer’s Application is required because the public notice requirements for the July, 2022 Planning and Zoning Board meeting were not satisfied as was shown by Florida Gateway’s Notice of Appeal. The City determined a *de novo* public hearing on the Application is necessary to rectify issues of due process to Florida Gateway and the public, which public hearing on the Application is scheduled to commence January 10, 2024.

The Application seeks expansion of the existing use for Lot 1 as a “Circle K gas station and convenience store,” to develop Lot 2 and make renovations to the building on Lot 1 for a “Circle K gas station and high speed diesel station.” The terms “gas station,” “convenience store,” and “high speed diesel station” are not found in the City of Lake City Land Development Regulations (LDR).

The LDR provides defined terms for the various uses permitted under the LDR. There are two (2) defined uses which include dispensing fuel at retail in the LDR section 2.1, Definitions General. They are *Automotive Service Station*, and *Truck Stop*. LDR section 14.15 “CHI” Commercial, Highway Interchange, is the specialized zoning district in which the Gateway Crossings subdivision is located.

Under LDR Section 14.15.2 Automotive service and self-service stations, restaurants, hotels and motels, and the like, are Permitted Principal Uses and Structures. LDR 4.15.2, 1., 3., and 4.

Truck stops are not a Permitted Principal Use or Structure. Instead, under LDR Section 14.15.5, Truck stops are Special Exceptions. LDR 4.15.5, 1.

Although the Developer's Application expressly states that its proposed new use of the Property is "Circle K gas station and high speed diesel station," only a Site Plan Application was filed. The Developer has not filed a Special Exception Application, the form of which is included in Florida Gateway's Appendix to Objection to Site Plan Application, Exhibit B.

Apparently, the Developer's Application contends the proposed new use described as "Circle K gas station and high speed diesel station," is an "*Automotive service and self-service station*" under LDR 4.15.2.1., though it does not use the LDR defined term. Florida Gateway objects and contends the LDR defined term "*Truck Stop*" applies to a proposed use for high flow diesel pump refueling and servicing of trucks and tractor trailer rigs. The Application clearly shows a proposed new facility with diesel canopy, high flow diesel pumps, truck and tractor-trailer rig parking, and underground storage tanks to serve commercial trucks and tractor-trailer rigs, together with additional restrooms and parking at the Property.

The LDR defines an *Automotive Service Station* as follows:

Automotive Service Station. *An Automotive service station is an establishment whose principal business is the dispensing at retail of motor fuel and oil primarily for automobiles; and where grease, batteries, tires, and automobile accessories may be supplied and dispensed at retail. In addition, an Automotive service station may provide accessory facilities for car washing and polishing (but not commercial car wash facilities) and may render minor repair services. However, major mechanical and body work, straightening of frames or body parts, steam cleaning, painting, tire recapping or re-grooving, storage of automobiles not in operating condition, or other work involving undue noise, glare, fumes, smoke, or other characteristics to an extent greater than normally found in such stations are prohibited. An Automotive service station is not a repair garage, a body shop, truck stop, or a car wash or a combination thereof.*

For the purposes of these land development regulations, where motor fuel pumps are erected for the purpose of dispensing motor fuel at retail primarily for automobiles, such motor fuel pumps shall be considered to constitute an Automotive service station, even where additional services which are customarily associated with an Automotive service station are not provided. Where such motor fuel pumps are erected in conjunction with a use which is not an Automotive service station, each use shall be considered as a separate principal use and as such, each must meet all applicable requirements of these land development regulations (see Article 4 for special design standards for Automotive service stations).

LDR 2.1 Definitions, General (emphasis added.) A copy of this definition is attached in Florida Gateway's Appendix to Objection to Site Plan Application, Exhibit C.

The proposed expansion of the Property provides for the addition of three (3) high flow diesel pump service bays and additional parking for up to six (6) commercial trucks and tractor-trailer rigs.

The LDR defines “*Truck stop*” as follows:

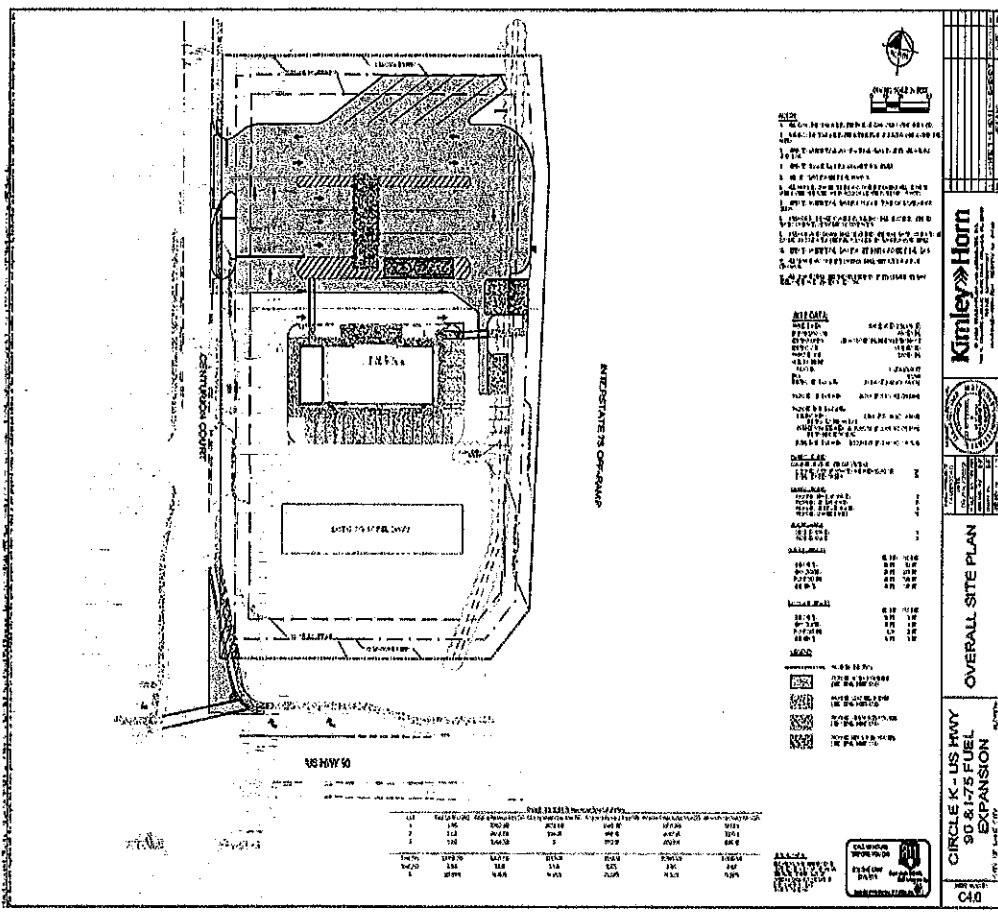
A truck stop is an establishment where the principal use is primarily the refueling and servicing of trucks and tractor trailer rigs. Such establishments may have restaurants or snack bars and sleeping accommodations for the drivers of such over-the-road equipment and may provide facilities for the repair and maintenance of such equipment.

LDR 2.1 Definitions, General (Emphasis added.) See Exhibit D.

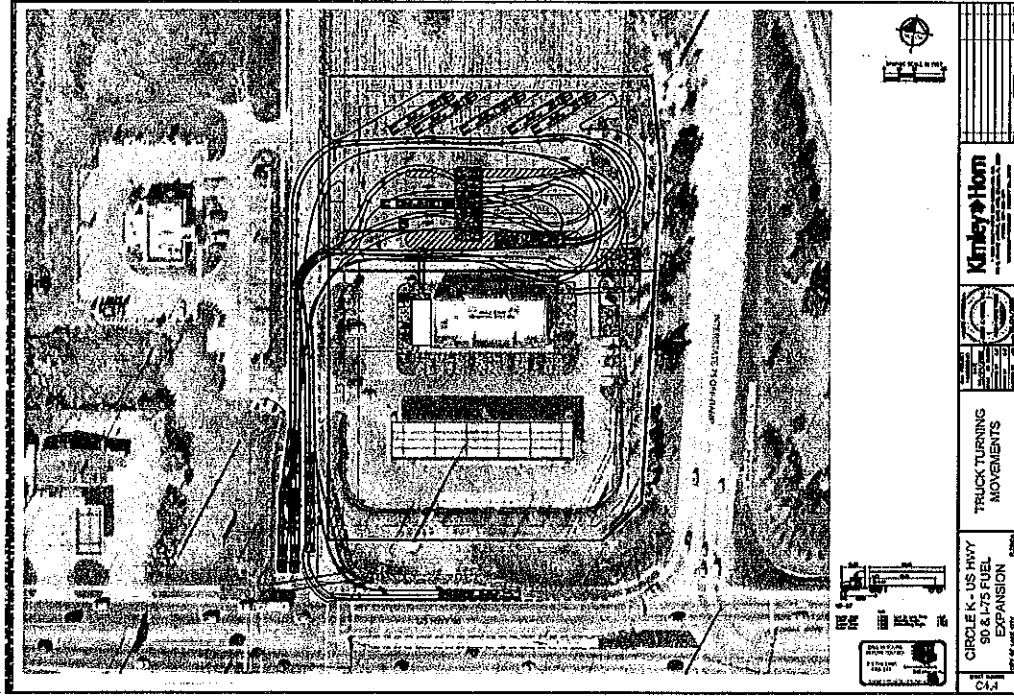
LDR Section 4.15.5(1) provides that a “*Truck stop*” is not permitted as of right in the CHI zoning district, but rather must be approved as a “*Special Exception*.”

The Developer has not applied for a “*Special Exception*.” No “*Special Exception*” has been granted by the City, nor was any hearing held before the Planning and Zoning Board concerning whether the proposed use constitutes a “*Truck stop*” under LDR 14.15.5.1. See Exhibit C.

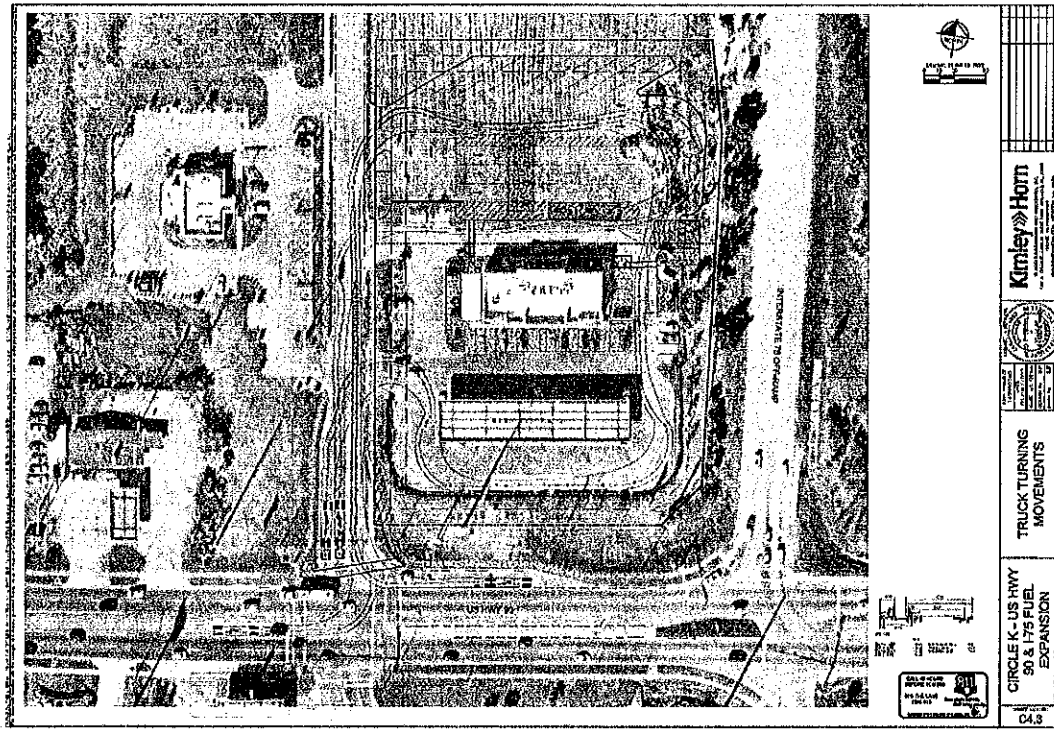
Below is a copy of the current site plan for the proposed Development. The Property is north of the existing Circle K, and east of the existing Denny’s restaurant. The entire Gateway Crossings subdivision is accessed by a cul de sac two-lane road known as NW Centurion Court. The Tru by Hilton hotel is located on Lot 7, and other future developments, two restaurants, a Sonic and a Rib Crib, have previously been approved for construction.



The proposed Site Plan allows trucks and tractor trailer rigs to enter the site and access the high flow diesel pump service bays from NW Centurion Court through the existing NW driveway of the Circle K and also through the Circle K parking lot. Below is a drawing of the expected Truck Turning Movements shown on the site plans for the Development.



Also below is a second drawing of the Truck Turning Movements, which depicts tractor trailer rig access to the high flow diesel pump service bays and alternative egress by exiting through the existing Circle K parking lot and automobile service station area.



The commercial trucks and tractor-trailer rigs solicited by Circle K for refueling at the high flow diesel pump service bays will enter and exit the Property on NW Centurion Court, which is only a two-lane road with concrete curbs and gutters. As shown by the Developer's Truck Turning Movements depictions, these large commercial trucks and tractor-trailer rigs must navigate through the I-75 and US Highway 90 interchange and throughout the existing *Automotive Service Station* traffic to be refueled and serviced in the Gateway Crossings subdivision.

As part of the application, the Developer submitted a traffic study. The Developer's traffic study does not consider the impact of the currently approved development to the north of the Property in Gateway Crossings. Further, despite the purpose of this expansion of the Circle K to construct new high flow diesel fuel

pump service bays, the Developer's traffic study assumes that truck and tractor trailer rig traffic will not increase, and actually be the same after the construction and opening of the Truck stop.

On November 8, 2022, the Columbia County Tourist Development Council expressed opposition to the proposed Truck stop, writing to the then City Manager in part:

"We are concerned how this new semi-trailer truck fueling station will negatively impact the quality of life for residents and visitors alike. The traffic at this intersection is already problematic and this project will only exacerbate the issue and alter the landscape of this exit for years to come."

A copy of the TDC's letter to the City Manager is attached in Florida Gateway's Appendix to Objection to Site Plan Application, Exhibit D.

The Florida Department of Transportation has also expressed concerns regarding this Development. On October 30, 2023, the Permits Manager of the Florida Department of Transportation, Lake City Operations, ("FDOT") wrote to Mayor Stephen Witt, Todd Kennon, City Attorney, and Rebecca Thigpen, Central Construction Manager, Circle K Stores, to advise that "the Department intends to revoke Commercial Access and Signal Connection Permit No. 2015-A-282-0026 (NW Centurion Court) issued on April 18, 2016 to Gateway Crossing development and under subsequent jurisdiction of the City of Lake City; not renew Safety Upgrade Permit No. 2022-a-292-00008 (NW Centurion Court) issued on May 18,

2022 2 Circle K that expires on November 20, 2023; and close the connection to the Gateway Crossing property per section 14-96.01(2), FAC.

The FDOT notice further provides in pertinent part:

The City did not contact the Department to determine if a new permit application and modification of existing connection is required. The City also failed to contact the Department to determine the need for connection modifications or to submit a new application for such modifications prior to initiation of property improvements, land use changes, or traffic flow alteration actions which constitute significant change. The planned construction at the site is significantly different from what was represented during the Department permitting process and there are significant safety concerns given the close proximity of the location to I-75 right-of-way.

A copy of the FDOT notice is attached in Florida Gateway's Appendix to Objection to Site Plan Application, Exhibit E.

Argument.

The proposed Site Plan Application violates four different provisions of the City's LDR. All are of equal importance and sufficient on their own to serve as a basis to deny the Application. First, the proposed project's new use is not an "Automotive Service Station." Even if the Planning and Zoning Board determines the use is an "Automotive Service Station" there is no evidence that the City staff applied the "special design standards for automobile service and self-service stations" required by the LDR. Second, the proposed project's new use is, in fact, a "Truck stop," which is not a permitted use within the CHI, Commercial, Highway

Interchange zoning district. Rather, a “*Truck stop*” may only be approved as a “*Special Exception*,” which it was not. Third, the proposed project's new use is a “*prohibited use*” in the CHI zoning district under LDR 4.15.4.2. Finally, the traffic study done by the Developer to support the Application is fatally flawed and cannot provide competent substantial evidence to properly evaluate concurrency and the impacts of the proposed project's new use on traffic and public safety. For any of these reasons, the Planning and Zoning Board should deny the Application.

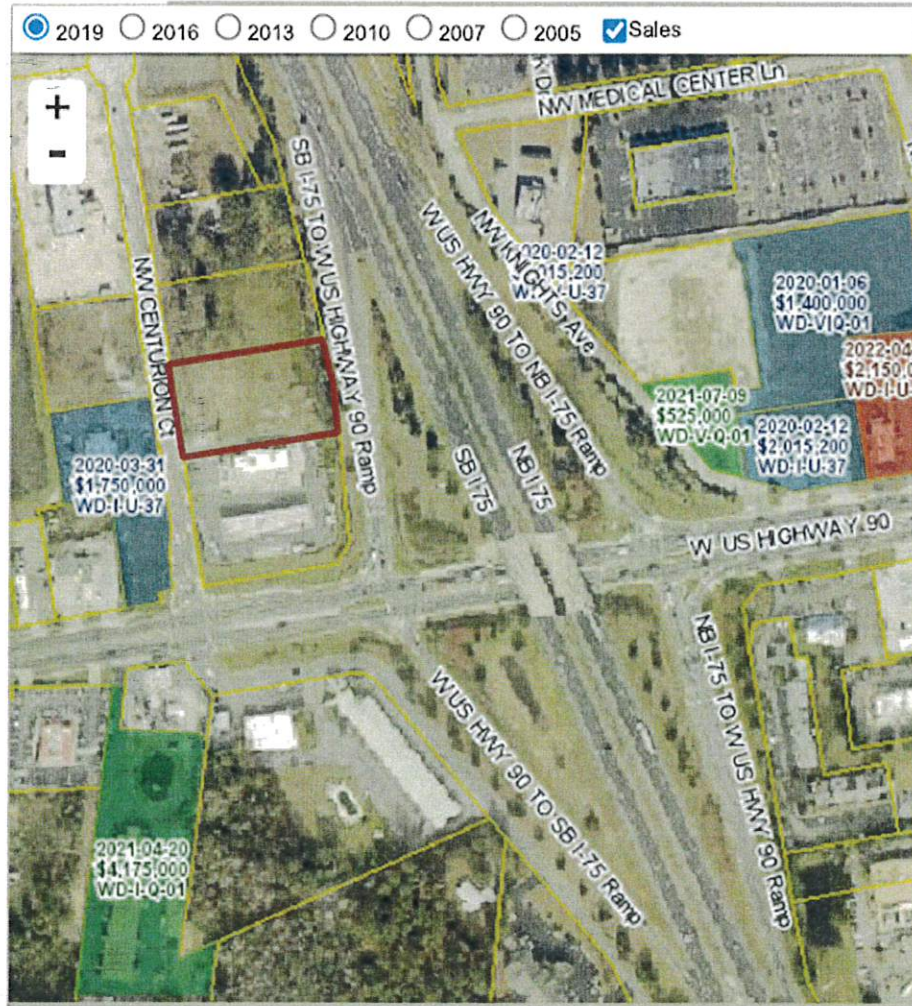
Florida Gateway contends that the two-lane road with limited right-hand turn lane and the short distance from the I-75 exit ramps and US 90 to the Property, will result in significant increase in truck and tractor trailer rig traffic. This will result in an increase in conflicts between traffic serving The Tru hotel and the currently existing and other additional approved high traffic volume businesses (Denny’s, Sonic and Rib Crib restaurants) in Gateway Crossing with the large trucks coming to and leaving the site. The proposed project's new use will also create traffic conflicts within the Circle K existing and expanded Property. Large trucks and tractor trailer rigs will enter the diesel refueling area through the existing Circle K parking lot and will exit the site via NW Centurion Court near the primary driveway to Denny’s restaurant.

Florida Gateway also submits that the proposed use of the Property is incompatible with the character of this zoning district, which is the primary entry

point of travelers to Columbia County and Lake City. Due to the interstate's high-traffic volume and intense local use, this intersection is one of the busiest in all of Columbia County. Florida Gateway contends that the proposed use of the Property in a manner consistent with a "Truck stop" is incompatible with the "Gateway to Florida" commercial profile of Lake City and Columbia County for this zoning district and may attract visitors to this vital area of our community that would negatively impact the visitors to the hotels and the patrons of the surrounding businesses.

1. The Site Plan Application is not an "Automotive Service Station" or a "Permitted Use."

The Application's subject parcel, Lot 2, Gateway Crossings subdivision and the lands surrounding it are located within the City's CHI, Commercial, Highway Interchange Zoning District. Below is a picture of Lot 2 taken from the Columbia County Property Appraiser's website.



The City’s LDR Section 4.15 governs the uses in the CHI Commercial Highway Interchange zoning category. LDR Section 4.15.2 sets forth the “*Permitted Principal Uses and Structures*” and 4.15.2.1 specifically allows “*Automotive service and self-service stations.*” A copy of LDR Section 4.15 is included in Exhibit C.

Under the City’s LDR, developments providing motor vehicle refueling and servicing goods and service are either an “*Automotive service and self-service station*” or a “*Truck stop.*” The LDR make clear that such facilities are one or the

other, as “*An Automotive service station is not a . . . truck stop, . . . or a combination thereof.*” LDR 2.1

Apparently, the City staff determined this Application is for an “*Automotive Service Station.*” However, the definition of “*Automotive Service Station*” belies this finding. The LDR defines this use as follows:

Automotive Service Station. *An Automotive service station is an establishment whose principal business is the dispensing at retail of motor fuel and oil primarily for automobiles; and where grease, batteries, tires, and automobile accessories may be supplied and dispensed at retail. In addition, an Automotive service station may provide accessory facilities for car washing and polishing (but not commercial car wash facilities) and may render minor repair services. However, major mechanical and body work, straightening of frames or body parts, steam cleaning, painting, tire recapping or re-grooving, storage of automobiles not in operating condition, or other work involving undue noise, glare, fumes, smoke, or other characteristics to an extent greater than normally found in such stations are prohibited. An Automotive service station is not a repair garage, a body shop, truck stop, or a car wash or a combination thereof.*

... Where such motor fuel pumps are erected in conjunction with a use which is not an Automotive service station, each use shall be considered as a separate principal use and as such, each must meet all applicable requirements of these land development regulations (see Article 4 for special design standards for Automotive service stations).

LDR 2.1 Definitions, General (emphasis added.)

It is simply illogical, and contrary to LDR 2.1, to construe the proposed project’s use as an “*Automotive Service Station.*” The proposed Application proposes an expansion of an existing “*Automotive Service Station*” to a much larger

establishment with the new, expanded facilities specifically designed to serve only commercial trucks and tractor-trailer rigs for refueling and service at high flow diesel pump service bays. While an “*Automotive Service Station*” may provide diesel fuel for automobiles and other motor vehicles, they are not utilizing high flow diesel pump service bays designed to refuel larger commercial trucks and tractor-trailer rig fuel tanks. According to the Application this proposed project's new use is for a “diesel canopy and underground storage tanks” for use as a “convenience store with fuel and diesel.” See Exhibit A. As noted above the LDR do not provide for any use described as “convenience store with fuel and diesel.” The LDR do, however, specifically provide that an “*Automotive Service Station*” provides for “*the dispensing at retail of motor fuel and oil primarily for automobiles.*” LDR 2.1 also specifically provides: “*An Automotive service station is not a . . . truck stop, . . . or a combination thereof.*”

This proposed project's new use is specifically designed for the purpose of refueling and servicing large trucks and tractor trailer rigs that require diesel fuel dispensed from high flow diesel pump service bays. This new proposed project's new use will only serve trucks and tractor trailer rigs, not “*automobiles.*” For this reason, the Application should be denied because it was based on the incorrect conclusion that the “*permitted use*” to be approved by the Application was an “*Automotive service and self-serve station.*”

Further, LDR Section 4.15.2(1) authorizes “*Automotive service and self-service stations*” as a permitted principal use for the CHI district but requires such use to comply with the “*special design standards for Automotive service and self-service stations*” set forth in LDR 4.2. The Application fails to comply with LDR Section 4.2 for “*special design standards for Automotive service and self-service stations*” set forth in of the LDR. A copy of Section 4.2 is included in Exhibit D.

Section 4.2.6 of the LDR sets forth the “*regulations*” that “*apply to the location, design, construction, operation, and maintenance of automotive service and self-serve stations,*” which references “*gasoline pumps*” but does not mention diesel fuel, much less the **high flow diesel pump service bays** necessary for large trucks and tractor trailer rigs. LDR 4.2.6

Because the Application seeks approval of a proposed project that is not an “*Automotive Service Station*” as a “*permitted use*” and failed to apply the “*special design standards for Automotive service and self-service stations*” set forth in of the LDR, the Planning and Zoning Board must deny the Application.

2. The Proposed Project’s New Use is a “Truck stop” which is not a permitted use within the CHI, Commercial, Highway Interchange zoning district, and which may only be approved as a “Special Exception.”

LDR Section 4.15.5.1. provides that “*Truck stops*” are not permitted as of right in the CHI district, but rather must be approved as a “*Special Exception.*” See Exhibit D. A *Special Exception* requires compliance with LDR Articles 12 and 13.

LDR Article 12 provides the required hearing procedures for *Special Exceptions*.

LDR Article 13 provides the requirements for permitting and concurrency management. This Application was not processed as a *Special Exception*.

The LDR defines “*Truck stop*” as follows:

A truck stop is an establishment where the principal use is primarily the refueling and servicing of trucks and tractor trailer rigs. Such establishments may have restaurants or snack bars and sleeping accommodations for the drivers of such over-the-road equipment and may provide facilities for the repair and maintenance of such equipment.

LDR 2.1 Definitions, General (Emphasis added.)

The issue is whether the proposed use under this Application which adds new diesel only facilities *is primarily the refueling and servicing of trucks and tractor trailer rigs?* The answer can only be yes, as this proposed Development is specifically designed for the purpose of refueling and servicing large trucks that require diesel fuel dispensed from high flow diesel pump service bays. This proposed project’s new use will only serve trucks and tractor trailer rigs. The proposed expansion is not for the purpose of refueling and servicing automobiles.

Upon information and belief, the City’s staff determined this application is for an “*Automotive service and self-serve station*,” and not a “*Truck stop*” because the proposed use is only for “refueling,” but not “servicing” of trucks. Apparently, the City’s staff determined the proposed new use of high flow diesel pump service bays is primarily for automotive use, not primarily for trucks and tractor trailer rigs.

The LDR definitions do not define or otherwise distinguish “refueling and servicing.” It is common knowledge that refueling or recharging is a required form of servicing any motor vehicle. It is also clear the term “servicing” as used by the LDR does not have the same meaning as the term “repair and maintenance” also included in the definition. The definition for “*Truck stop*” states that “facilities for repair and maintenance” is an option by using the conjunction “may have.”

A “*Truck stop*” under the Lake City LDR “*may have*” facilities for repair and maintenance, but it is not required to have those facilities to meet the definition of a “*Truck stop*.” Therefore, the LDR use of the term “*refueling and servicing*” must involve uses that are different than “*repair and maintenance*.”

Florida Gateway submits it is reasonable to construe the term “*servicing*” as including less intense maintenance activities such as checking and adding oil or transmission fluid, or adding air to truck tires, and refueling. The City staff’s determination that this proposed Development is not a “*Truck stop*” requires a tortured conclusion that “*servicing*” a truck or tractor-trailer rig includes adding air to truck tires, but excludes refueling the truck.

What distinguishes a “*Truck stop*” is that the proposed use is “*primarily for refueling and servicing trucks and tractor-trailer rigs,*” which is the exact proposed primary new use of the Property sought by the Application proposing to new install

high flow diesel fuel pump service bays and other “*Truck stop*” facilities in the heart of the busiest intersection of Lake City.

In addition, however, the existing Circle K serves prepared food and provides a few tables and chairs for its customers much like a “*snack bar*.” Also, the amended site plan includes six (6) commercial trucks and tractor-trailer rig parking spaces which are not required for “*refueling*.” These tractor-trailer rig parking spaces may, however, provide “*sleeping accommodations for the drivers of such over-the-road equipment*.” Also, the Application includes adding new restroom facilities for the truck drivers.

All these facts clearly establish the Application for the proposed projects new use adding high flow diesel fuel pump service bays, tractor trailer rig parking, underground diesel storage tanks and a new 100-foot sign advertising TRUCK DIESEL, is a “*Truck stop*” as defined by LDR 2.1. See Exhibits C, F and G. All of the proposed new facilities support the high flow diesel pump service bays which are “*primarily for refueling and servicing trucks and tractor trailer rigs*,” and not suited for use by automobiles. The uncontroverted facts clearly establish this Application under consideration constitutes a “*Truck stop*.” As such, the Developer’s Application required consideration as a “*Special Exception*.” Since there is no disagreement that the Application has not been processed as a “*Special Exception*,” the Planning and Zoning Board must deny the Application.

To be clear, however, while LDR 4.15.5. 1. requires a *“Special Exception”* for any proposed projects new use authorizing a *“Truck stop”* in the CHI district, this Application does not come close to meeting the LDR requirement for granting a *Special Exception* .

LDR 2.1 defines *“Special Exception”* as:

“a use that would not be appropriate generally or without restriction throughout a zoning district but which, if controlled as to number, area, location, or relation to the neighborhood, would promote the public health, safety, welfare, morals, order, comfort, convenience, appearance, prosperity, or the general welfare. ... (For the procedure in securing special exceptions, see Article 12).”

This Application seeks to provide high flow diesel fuel pump service bays for large trucks and tractor-trailer rigs at the busiest, highest traffic intersection in all of Columbia County. This proposed use consistent with a *“Truck stop”* is incompatible with the commercial profile for this CHI district for Lake City and Columbia County. There is nothing about this proposed Development that *“would promote the public health, safety, welfare, morals, order, comfort, convenience, appearance, prosperity, or the general welfare”* in this CHI district. To the contrary, in addition to the traffic nightmare, this proposed use is most likely to attract visitors to this vital area of our community that would negatively impact the patrons of the hotels and the surrounding restaurants and other businesses, and our residents. This is why the

Tourist Development Council took the unusual step to request the City to deny the Application for this use.

Florida Gateway retained Luis N. Serna, AICP, a professional Certified Planner with Calvin, Giordano & Associates, Inc., to evaluate the Developer's Site Plan Application. Mr. Serna concludes "the proposed expansion of the Circle K by the addition of tractor trailer fueling on the adjacent parcel was not properly defined as a truck stop. Therefore, the City failed to properly process the Application as a Special Exception as required by the LDRs. Without processing this as a Special Exception, the review staff did not provide an opportunity for the Planning and Zoning Board, the Board of Adjustment, and the public to review the potential impacts of this use and to address potential controls of the use on the number, area, location, or relation to the neighborhood."

Further, "given the types of uses in the Gateway Crossings subdivision, which include restaurants and a hotel, and the high level of automobile traffic within Gateway Crossings from these uses, there are concerns that an increase in the amount of tractor trailer traffic near the entrance of this subdivision will cause significant adverse impacts to surrounding uses particularly from noise and traffic. Impacts to public roadways will extend beyond Gateway Crossings to also affect U.S. 90 and the offramp for Interstate 75. Without processing this application as a Special Exception, the City did not permit affected property owners and the public to

adequately assess the full impacts of the proposed use and to address any potential mitigating conditions to address these impacts.”

A copy of Mr. Serna’s report is attached in Florida Gateway’s Appendix to Objection to Site Plan Application, as Exhibit F.

This Application required a *Special Exception* which was neither sought nor obtained. This Application does not qualify for a *Special Exception* under LDR 4.15.5.1. under any circumstances. The Planning and Zoning Board has no alternative under the LDR than to deny this Application.

3. The Development is a “prohibited use.”

LDR Section 4.15.4 lists “*Prohibited uses and structures*” in the CHI zoning district. LDR Section 4.15.4.2 includes as a “*Prohibited use or structure*”:

Any other uses or structures not specifically, provisionally or by reasonable implication permitted herein. Any use which is potentially dangerous, noxious or offensive to neighboring uses in the district or to those who pass on public ways by reason of smoke, odor, noise, glare, fumes, gas, vibration, threat of fire or explosion, emission of particulate matter, interference with radio or television reception, radiation or likely for other reasons to be incompatible with the character of the district.

LDR 4.15.4.2 (emphasis supplied.)

Florida Gateway submits the proposed expansion use is a “*prohibited use*” under the LDR 14.15.4. 2. This Application is for Lot 2, Gateway Crossings located on a narrow, 2-lane road, adjacent to the most traffic intense intersection in Lake City, and poses serious traffic problems that are dangerous to drivers and

pedestrians. The long turning radius required for large trucks and tractor-trailer rigs using the proposed Truck stop will block and interfere with both lanes of traffic on NW Centurion Court and within the Circle K Property.

Also, Florida Gateway submits that a “*Truck stop*” is simply incompatible with the character of the surrounding development and will pose negative impacts to the users of the hotels, the restaurants, and to future development. This is the intersection to Lake City and Columbia County known as the “Gateway to Florida.” Clearly large commercial trucks and tractor-trailer rigs, which are the targeted customers for this proposed project's new use, create more “*smoke, odor, noise, ... fumes, gas, vibration, ... [and] emission of particulate matter*” than is otherwise experienced at any automobile service station in the zoning district including the existing Circle K.

The Application is wholly inconsistent and incompatible with the I-75/US 90 intersection. In less than five or 15 minutes of travel time, approximately five (5) miles South of the intersection of I-75 and U.S. 90, at the intersection of I-75 and S.R. 47, and also approximately 15 miles South at the intersection of I-75 and U.S. 441, there are ample locations available and zoned as permitted use for a “*Truck stop.*”

It is important to recognize that comparable CHI districts in neighboring communities do not tolerate developments with a proposed use such as this.

Comparable interchanges to our “Gateway of Florida” CHI district such as Valdosta’s Exit 18, Gainesville’s Newberry Road and Archer Road exits, and Ocala’s Exit 200, while constituting commercial hubs for those communities with numerous hotels, restaurants, businesses and automobile service stations, none of them have any commercial uses even remotely comparable to a “*Truck stop*” or any other use such as this proposed project's new use. This Application, if approved, is likely to cause significant traffic snarls and danger to the public that will adversely affect our community for generations to come.

This proposed project's new use constitutes a “*prohibited use or structure*” under LDR Section 4.15.4.2. Therefore, the Application must be denied as incompatible with the City’s CHI district.

4. The Traffic Study done to support the Application is flawed and does not meet the requirements of the LDR to evaluate the Proposed Project's impact on concurrency, traffic, and public safety.

Section 13.12.3 of the LDR requires that all development shall maintain level of service standards, including traffic and requires “a concurrency review to be made with applications for development approvals and a Certificate of Concurrency issued prior to development.” The Developer provided a flawed traffic study in the Application which was prepared by Kimley-Horn and Associates, Inc. (“traffic study”).

Florida Gateway retained Jeffrey W. Buckholz, PhD, P.E. PTOE, a professional traffic engineer, to evaluate the Developer's traffic study. He has opined that the Developer's traffic study is "incomplete and inaccurate and that the recommendations contained in the report do not ensure safe and efficient access to the proposed site." A copy of Dr. Buckholz's report is attached in Florida Gateway's Appendix to Objection to Site Plan Application, as Exhibit G.

Dr. Buckholz found, in part, that the Developer's traffic study:

1. Ignored the impact of "development that is currently underway along Centurion Court . . . right behind the Circle K" including a Sonic Restaurant and a Rib Crib Restaurant both with drive through windows.
2. The traffic study "calculated trip generation for the expanded site" at a rate that was "reduced by 69% for the weekday AM peak hour and by 60% for the weekday PM peak hour."
3. The traffic study "applies their reduction for pass-by traffic to vehicles using US 90; they do not make the reasonable assumption that a large portion of the pass-by traffic will be drawn from I-75." This error "results in projected site traffic volumes at the US 90/Centurion Court intersection that are too low." It seems reasonable to assume that a new truck stop at this location would draw truck traffic from I-75, which Kimley-Horn ignored.

4. “The Kimley-Horn Synchro analysis contains some input errors,” that “masks the deleterious queue effect.” Dr. Buckholz also found that the traffic study used “incorrect truck percentages.”
5. The “percentage of trucks reflected in Kimley-Horn’s 2023 Build analysis is no different than the percentage in their 2021 existing analysis.” One would assume that once the truck stop is in operation the number of trucks would increase, especially being so near the I-75 US-90 interchange. To assume that truck traffic will be the same after the Development is simply irrational.
6. The traffic study did not provide the “queue length” for the left turn lane on the Northwest Centurion Court approach to US 90. Using Kimley-Horn’s own traffic numbers (which are substantially low) the peak queue length is expected to “extend to the Denny’s driveway and blocks access to the ... thru/right turn lane on Centurion Court.”
7. Finally, Dr. Buckholz opines that the traffic study is not based on the “generally accepted and publicly developed Highway Capacity Software (HCS)” but rather on Synchro – a private “black box” software program. This can result in vastly different queue lengths as in the case for the south approach (Florida Gateway Drive) left turn lane where the HCS predicts a

queue length that is longer than Synchro “by a factor of almost three, blocking access to the adjacent through/right turn lane.”

The City and the Developer must support the Application with “competent substantial evidence,” which the courts say is “evidence that provides a factual basis from which a fact at issue may reasonably be inferred.” See, *Broward Cnty. v. G.B.V. Int'l, Ltd.*, 787 So. 2d 838, 845 (Fla. 2001). Competent substantial evidence “sufficient to sustain a finding of an administrative agency, is evidence that is sufficiently relevant and material that a reasonable mind would accept it as adequate to support the conclusion reached.” See, *Sch. Bd. Of Hillsborough Cnty. v. Tampa Sch. Dev. Corp.*, 113 So. 3d 919, 923 (Fla. 2d DCA 2013); see also *Lee Cnty. v. Sunbelt Equities, II, Ltd. P’ship*, 619 So. 2d 996, 1002–03 (Fla. 2d DCA 1993) (quoting *Town of Indialantic v. Nance*, 400 So. 2d 37, 40 (Fla. 5th DCA 1981), *aff’d*, 419 So. 2d 1041 (Fla. 1982)).

Additionally, Florida Gateway submits that the traffic flow patterns of the trucks and travel trailer rigs traveling through the site of the proposed project is likely to create significant conflicts between trucks and pedestrians inside the project.

Thus, because of these deficiencies, the Developer’s traffic study cannot constitute competent substantial evidence to support the factual conclusion that the proposed Development meets the City’s concurrency standards, or more importantly, would not cause traffic congestion or pose traffic safety concerns.

In addition, upon information and belief, the proposed project's new use constitutes significant change in the traffic volumes on NW Centurion Court and the I-75/ U.S. Highway 90 West interchange requiring approval from FDOT which has neither been sought nor obtained by the City or the Developer.

Conclusion.

State law requires that "*each county and each municipality shall adopt or amend and enforce land development regulations that are consistent with and implement their adopted comprehensive plan.*" § 163.3202, *Fla. Stat.* As shown above, the Application's proposed project violates various provisions of the City's LDR.

The Application's proposed project's new use, which is admittedly designed to serve commercial trucks and tractor-trailer rigs for refueling and service at high flow diesel pump service bays, is not "*Automotive Service Station*" as defined by the LDR. Even if it were, the City did not apply the required "*special design standards for automobile service and self-service stations.*" In fact, the proposed project is a "*Truck stop,*" that must be approved as a "*Special Exception,*" which it was not. The additional commercial truck and tractor-trailer rig traffic from the proposed project's new use in this high traffic area is a prohibited use that poses a threat to public safety, and the project is "*incompatible*" with the surrounding development in violation of the LDR.

Finally, the Developer's traffic study for the Application is flawed, as it does not provide competent substantial evidence that meets the requirements of the LDR to evaluate concurrency and the impacts of proposed development on traffic and public safety.

Remedy Requested.

Florida Gateway respectfully requests that the City of Lake City Planning and Zoning Board deny the Site Plan Application #SPR 22-15.

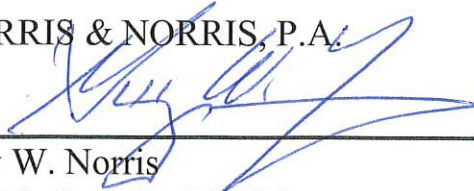
RESPECTFULLY SUBMITTED on this 18th day of December, 2023.

ANSBACHER LAW



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Fax: (386) 755-1577

gnorris@norrissattorneys.com

sregister@norrissattorneys.com

CERTIFICATE OF FILING AND SERVICE

I CERTIFY that a copy of this document has been served on this 18th day of December, 2023, on the City of Lake City, Florida Land Development Regulation Administrator, and a courtesy copy has been provided to the Developer, by electronic mail or regular mail as indicated.

NORRIS & NORRIS, P.A.



Guy W. Norris

Copies furnished:

Land Development Regulation Administrator
205 N. Marion Ave.
Lake City, FL 32055

By hand delivery and electronic mail to growthmanagement@lcfla.com

Todd Kennon, Esquire
City Attorney
582 West Duval Street
Lake City, FL 32055

By electronic mail to tjk@rkkattorneys.com

Scott Walker, Esquire
City Attorney

Courtesy copy provided:
Marshall Rainey, Esquire
mrainey@burr.com
Burr Foreman
Tampa, FL

GWC Development Partners, LLC
Registered Agent Daniel Hotte
8890 West Oakland Park Blvd.
Suite 201
Sunrise, FL 33351
By direct mail.

Circle K Truck stop.

By Florida Gateway Hotels, LLC, and
Lake City Hotels, LLC.

January 10, 2024

Florida Gateway Hotels, LLC, and Lake City Hotels, LLC





Issue for the Board.
What is a Truck Stop?



A truck stop is an establishment where the principal uses primarily the refueling and servicing of trucks and tractor-trailer rigs, LDR 2.1



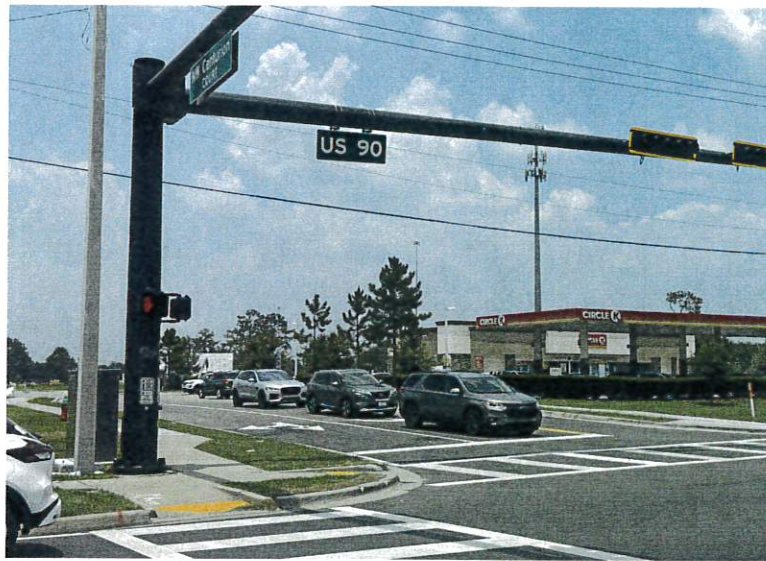
The existing Circle K
is an automotive Service Station.

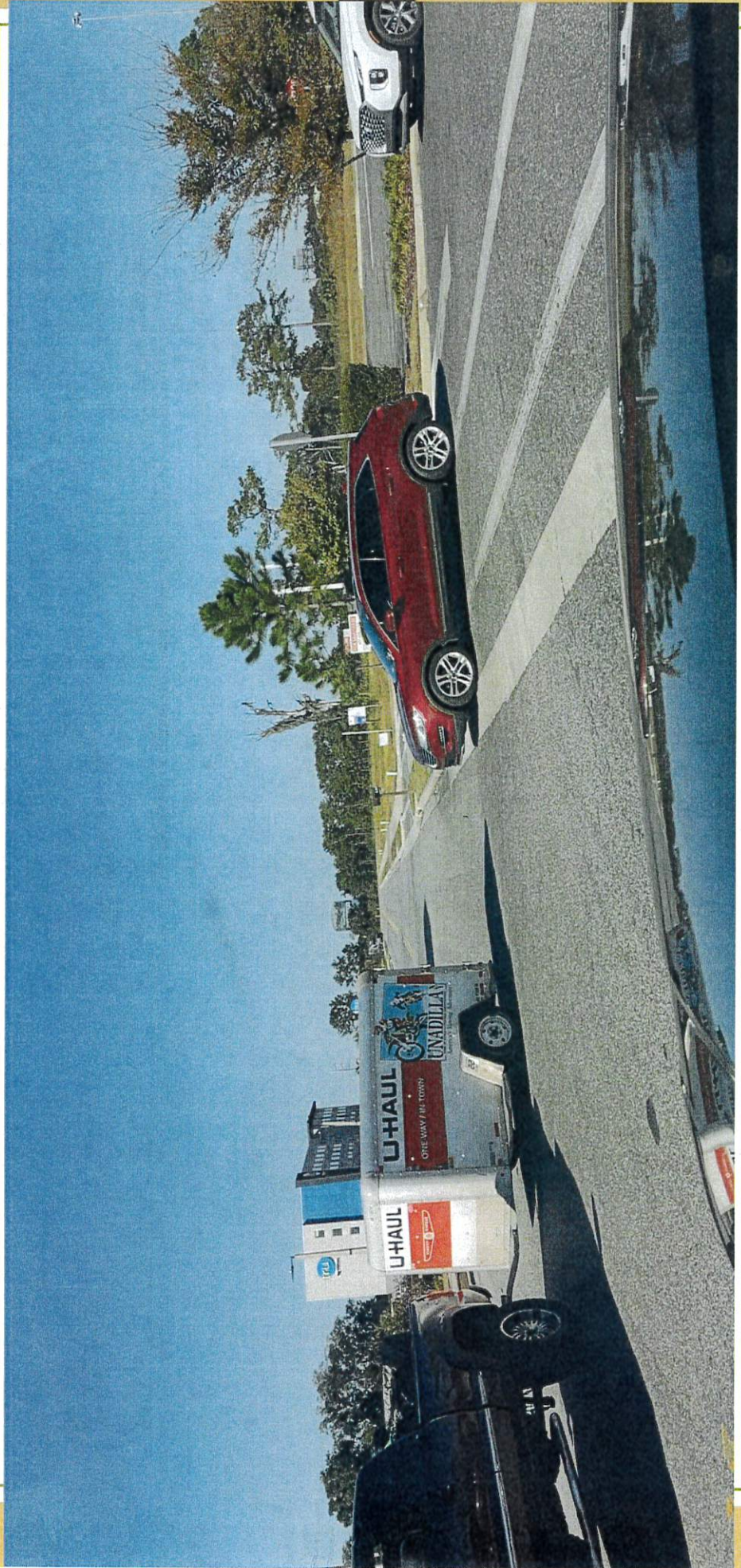


Existing Circle K - an automotive service station is an establishment whose principal business is the dispensing at retail of motor fuel and oil primarily for automobiles, LDR 2.1



Existing Traffic on Northwest Centurion Court.







Circle K Serves Food.

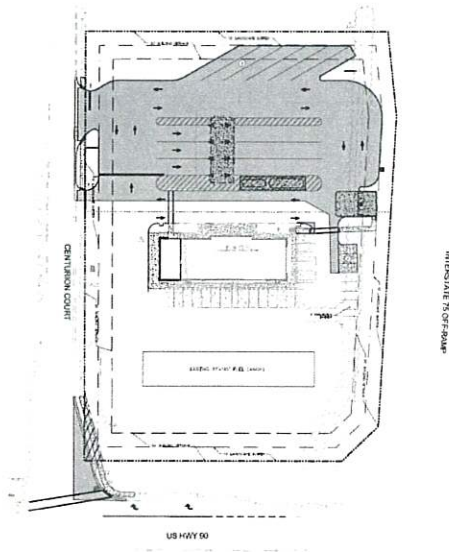


It has dining tables.



Circle K Site Plan

8/29/2022



APPLICANT INFORMATION

Name: Jarod C. Stubbs, P.E.
Address: 189 S Orange Ave., Ste 1000, Orlando, FL
Phone: (407) 409-7002
Email: jarod.stubbs@kimley-horn.com

Date: 09/01/2021

Business Name: Kimley-Horn and Associates
Business Address: 189 South Orange Ave., Suite 1000
Orlando, FL 32801

SUBJECT PROPERTY INFORMATION

Address: NE corner of US Hwy 90 and NW Centurion Ct (behind the Circle K)
Parcel ID#: 35-3s-16-02524-102; and 35-3s-16-02524-111
Existing Use: Vacant Commercial
Zoning District: CHI Commercial Highway Intensive

Property Owner: GWC Development Partners, LLC
Owner Address: 2682 NW Noegel Rd
Lake City, FL 32055

DESCRIPTION OF REQUEST (may be attached, separately)

PLEASE PROVIDE AS MUCH DETAIL AS POSSIBLE SO THAT STAFF CAN BE PREPARED TO ADDRESS YOUR QUESTIONS.

Please include information regarding:

- Proposed use
- Proposed improvements to building and/or site

The proposed project is to be a high speed diesel expansion to the existing Circle K with related parking, underground fuel storage tanks, and other necessary improvements. The project is anticipated to take up space on both parcels listed in this application. Expected new impervious area for the project is +/- 49,850 square feet. The existing Circle K will also have improvements including a building expansion for additional restrooms and an adjustment to the parking spaces to allow space for said expansion. See attached site plan for more detail.

Lake City Zoning Atlas



SECTION 4.15 "CHI" COMMERCIAL, HIGHWAY INTERCHANGE

4.15.1 DISTRICTS AND INTENT

The "CHI" Commercial, Highway Interchange category includes one zone district: CHI. This specialized district is intended for areas where adequate lot depth is available to provide development for vehicular related uses primarily serving the traveling public. Uses in such district are subject to criteria and standards intended to preserve the character of the district and to minimize adverse impacts with abutting and nearby uses. This district shall only be applied to interstate highway interchange areas.

4.15.2 PERMITTED PRINCIPAL USES AND STRUCTURES

1. Automotive service and self-service stations (see Section 4.2 for special design standards for automotive service and self-service stations).
2. Rental of automotive vehicles, trailers and trucks.
3. Restaurants.
4. Hotels and Motels.
5. Retail commercial outlets for sale of fruit, gifts, novelties and similar uses catering to tourists.
6. Light manufacturing, assembling, processing, packaging or fabricating in a completely enclosed building.
7. Facilities for storage and distribution of products including wholesale activity.
8. Retail factory outlets for sale of goods.

Unless otherwise specified, the above uses are subject to the following limitations: (1) products to be sold only at retail; and (2) for all developments, site and development plan approval is required (see Article 13).

Land Development Regulation

Definition of Automotive Service Station.

Automotive Service Station. An automotive service station is an establishment whose principal business is the dispensing at retail of motor fuel and oil primarily for automobiles; and where grease, batteries, tires, and automobile accessories may be supplied and dispensed at retail. In addition, an automotive service station may provide accessory facilities for car washing and polishing (but not commercial car wash facilities) and may render minor repair services. However, major mechanical and body work, straightening of frames or body parts, steam cleaning, painting, tire recapping or regrooving, storage of automobiles not in operating condition, or other work involving undue noise, glare, fumes, smoke, or other characteristics to an extent greater than normally found in such stations are prohibited. An automotive service station is not a repair garage, a body shop, truck stop, or a car wash or a combination thereof

4.2.6 AUTOMOTIVE SERVICE AND SELF-SERVICE STATIONS

The following regulations shall apply to the location, design, construction, operation, and maintenance of automotive service and self-service stations (with the exception that for automobile self-service stations where self-service gasoline pumps in conjunction with retail and commercial outlets for sale of food, hardware and drugs, there shall be no outside sales of oil, grease, parts or accessories for automobiles and no service except for self-service water, air or car wash).

4.2.6.3 Location of pumps and structures. No main or accessory building, no sign of any type, and no gasoline pump shall be located within twenty-five (25) feet of the lot line of any property that is zoned for residential purposes. No gasoline pump shall be located within fifteen (15) feet of any street right-of-way line; where a greater street setback line has been established, no gasoline pump shall be located within fifteen (15) feet of such setback line.

Land Development Regulation

Definition of Truck Stop

Truck Stop. A truck stop is an establishment where the principal use is primarily the refueling and servicing of trucks and tractor trailer rigs. Such establishments may have restaurants or snack bars and sleeping accommodations for the drivers of such over-the-road equipment and may provide facilities for the repair and maintenance of such equipment

Truck stops are to be processed as a special exception.

4.15.5 SPECIAL EXCEPTIONS (See also Articles 12 and 13)

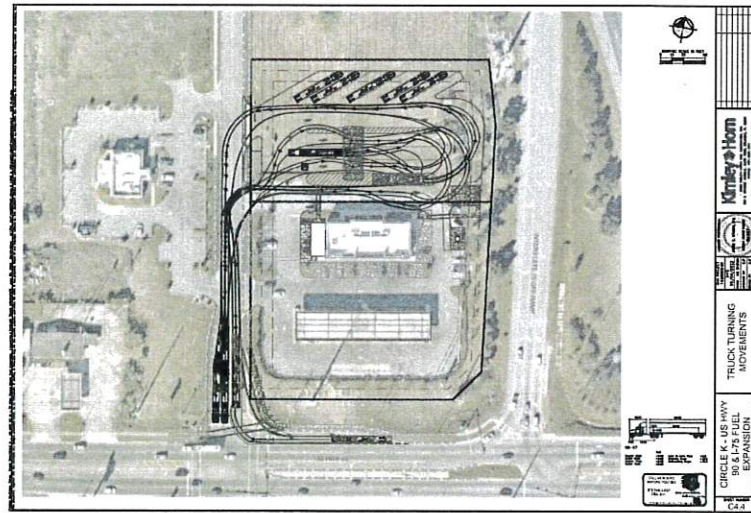
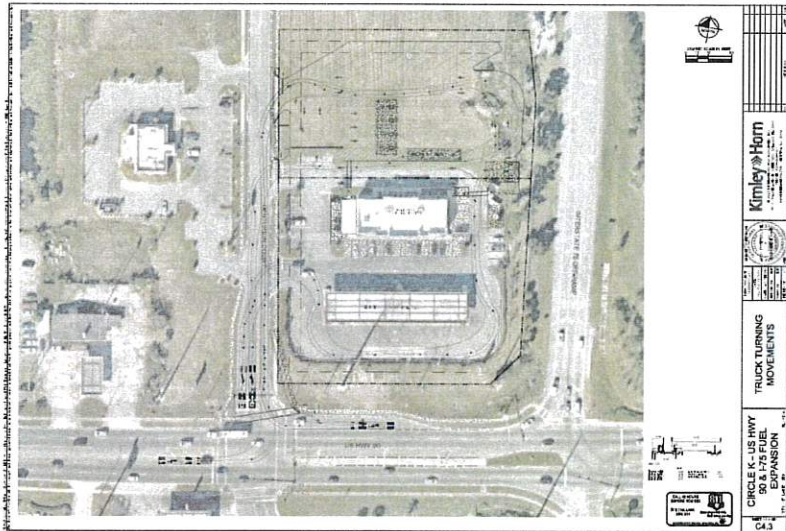
1. Truck stops.
2. Travel trailer parks or campgrounds (see Section 20.24).
3. Commercial tourist attractions.
4. Package store for sale of alcoholic beverages; bar tavern, or cocktail lounge.
5. Retail commercial outlets for sale of new and used automobiles, motorcycles, trucks and tractors, manufactured homes, boats, heavy machinery and equipment, lumber and building supplies, and monuments.
6. Public buildings and facilities.
7. Bed and breakfast Inns (see Section 4.2).
8. Off-site signs (see Section 4.2)

SECTION 11.2 SPECIAL EXCEPTIONS CRITERIA.

h. Considerations relating to general compatibility with adjacent properties and other property in the district including but not limited to:

- (1) Conformity with the Comprehensive Plan and the effects upon the Comprehensive Plan;
- (2) The existing land use pattern;
- (3) The impact of the proposed use upon the load on public facilities such as schools, utilities, and streets;
- (4) Changed or changing conditions which find the proposed use to be advantageous to the community and the neighborhood;
- (5) The impact of the proposed use upon living conditions in the neighborhood;
- (6) The impact of the proposed use upon traffic congestion or other public safety matters;
- (7) The impact of the proposed use upon drainage;
- (8) The impact of the proposed use upon light and air to adjacent area;
- (9) The impact of the proposed use upon property values in the adjacent area;
- (10) The impact of the proposed use upon the improvement or development of adjacent property in accordance with existing regulations; and
- (11) The impact of the proposed use with regard to the scale of needs of the neighborhood or the community.

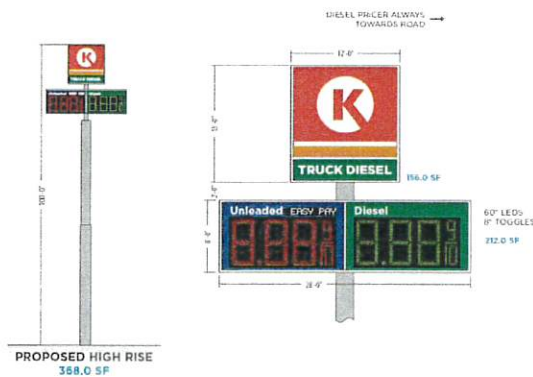
Truck Turning Movements



Circle K 100 ft. Truck Stop Diesel Sign

CIRCLE K | 9831 | LAKE CITY, FL | EXTERIOR SIGN PACKAGE | MID SIGN

PG 4 / 5



CORPORATE
IDENTIFICATION
SOLUTIONS

CUSTOMER: Circle K
OWNER: Circle K

LOCATION: Lake City, FL
PROJECT NO.: 9831

DATE: 10/15/14

PROJECT: 9831

DESIGNER: CIS

CONTRACT NO.: 9831

Answer:
It's a Truck Stop.





COLUMBIA COUNTY
TOURIST DEVELOPMENT COUNCIL

It has recently come to the Tourist Development Council's attention that the Circle K located at the I-75 and Hwy 90 West interchange plans to expand their fueling station to service 18-wheeler cargo trucks. This is incredibly concerning to the Tourist Development Council, and we respectfully request a meeting to discuss these concerns.

November 8, 2022

Florida Department of Transportation's email October 30, 2023.

Steven Witt, Mayor
City of Lake City

Todd Kennon, City Attorney
City of Lake City

Rebecca Thigpen
Central Construction Manager
Circle K Stores

To All,

In accordance with section 14-96, Florida Administrative Code (FAC), you are notified the Department intends to revoke Commercial Access and Signal Connection Permit No. 2015-A-292-0026 (NW Centurion Court) issued on April 18, 2016 to Gateway Crossing development and under subsequent jurisdiction of the City of Lake City, not renew Safety Upgrade Permit No. 2022-A-292-00008 (NW Centurion Court) issued on May 18, 2022 to Circle K that expires on November 20, 2023, and close the connection to the Gateway Crossing property per section 14-96 011(2) FAC.

The City did not contact the Department to determine if a new permit application and modification of existing connection is required. The City also failed to contact the Department to determine the need for connection modifications or to submit a new application for such modifications prior to initiation of property improvements, land use changes, or traffic flow alteration actions which constitute significant change. The planned construction at the site is significantly different from what was represented during the Department permitting process and there are significant safety concerns given the close proximity of the location to I-75 right of way.

If you have any questions or concerns, please contact me

Sincerely,

Troy Register
Permits Manager
Lake City Operations
(386) 961-7153

Requested Motion

1. Find that the proposed use constitutes a “Truck stop” and not an “Automotive Service Station.”
2. Consequently, conclude that the application for the site plan must be denied because it was not been processed as an application for a “special exception” for a “truck stop” under Section 4.15.5 Special Exceptions and Section 11.2 of the Land Development Regulations.

APPENDIX TO OBJECTION TO SITE PLAN APPLICATION
#SPR 22-15

EXHIBIT	DESCRIPTION	BATES NUMBER
A.	Application for New Development and Site Review Committee Meeting (Site Plan Application)	001-013
B.	Form for Special Exception Application	014-018
C.	LDR 2.1 Definitions, General (emphasis added.) A Copy of this Definition is Attached in Florida Gateway's Appendix to Objection to Site Plan Application	019-026
D.	TDC'S Letter to the City Manager	027
E.	FDOT Notice to City and Circle K	028
F.	Serna Planning Analysis Regarding Objection to Site Plan Application	029-032
G.	Buckholz Traffic Technical Evaluation of March 2022 Circle K Traffic Impact Analysis	033-038



CITY OF LAKE CITY

APPLICATION FOR
NEW DEVELOPMENT AND
SITE REVIEW COMMITTEE MEETING

FOR OFFICIAL USE ONLY

MEETING DATE: _____

MEETING TIME: _____

APPLICANT INFORMATION

Date: 09/01/2021

Name: Jarod C. Stubbs, P.E.
 Address: 189 S Orange Ave., Ste 1000, Orlando, FL
 Phone: (407) 409-7002
 Email: jarod.stubbs@kimley-horn.com

Business Name: Kimley-Horn and Associates
 Business Address: 189 South Orange Ave., Suite 1000
Orlando, FL 32801

SUBJECT PROPERTY INFORMATION

Address: NE corner of US Hwy 90 and NW Centurion Ct (behind the Circle K)
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 Please include information regarding:

- Proposed use
- Proposed improvements to building and/or site

The proposed project is to be a high speed diesel expansion to the existing Circle K with related parking, underground fuel storage tanks, and other necessary improvements. The project is anticipated to take up space on both parcels listed in this application. Expected new impervious area for the project is +/- 49,850 square feet. The existing Circle K will also have improvements including a building expansion for additional restrooms and an adjustment to the parking spaces to allow space for said expansion. See attached site plan for more detail.

SUBMIT WITH THIS FORM

- Copy of survey or sketch of location/building
- Sketch of any proposed improvements
- Any other information that will help in review of the proposal

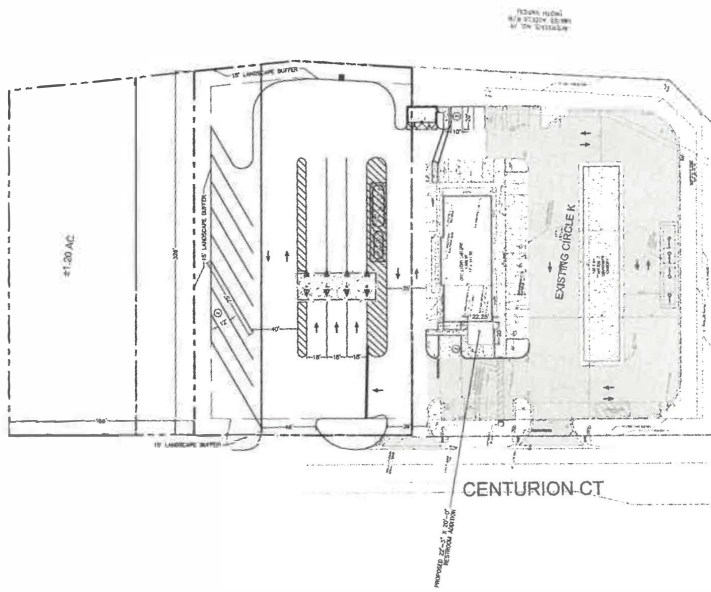
SUBMIT COMPLETED FORM AND DOCUMENTS TO:

Mail: Lake City Growth Management Department, 205 N Marion Ave, Lake City, FL 32055

Email: growthmanagement@lcfla.com

Fax: 386-758-5426

If you have any further questions, please contact Growth Management, 386-719-5750



INTERSTATE 75

SHEET NO. 1
 PROJECT NO. 19-000000-001
 DATE: 08/14/2021
 DRAWN BY: JAROD STUBBS
 CHECKED BY: JAROD STUBBS
 APPROVED BY: JAROD STUBBS
 PROJECT: 19-000000-001
 SHEET: 1 OF 2
 SCALE: AS SHOWN
 DATE: 08/14/2021



U.S. HWY 90

CIRCLE K - U.S. 90 & I-75

SF-07

CONCEPTUAL SKETCH
 8/17/2021 - CONTACT JAROD STUBBS, P.E. (401) 408-7000



Kimley»Horn



GROWTH MANAGEMENT
 205 North Marion Ave.
 Lake City, FL 32055
 Telephone: (386)719-5750
 E-Mail:
 growthmanagement@lcfla.com

FOR PLANNING USE ONLY
 Application # SPR22-15
 Application Fee: \$200.00
 Receipt No. _____
 Filing Date 4/1/22
 Completeness Date _____

Site Plan Application

A. PROJECT INFORMATION

1. Project Name: CIRCLE K - US 90 & I-75
2. Address of Subject Property: 143 NW Centurion Ct., Lake City, FL 32055
3. Parcel ID Number(s): 35-3S-16-02524-001, 35-3S-16-02524-102, 35-3S-16-02524-111
4. Future Land Use Map Designation: Commercial
5. Zoning Designation: CHI - Commercial Highway Interchange
6. Acreage: ±3.46
7. Existing Use of Property: Existing Circle K gas station and convenience store
8. Proposed use of Property: Circle K gas station and high speed diesel station
9. Type of Development (Check All That Apply):
 - Increase of floor area to an existing structure: Total increase of square footage ±652 SF
 - New construction: Total square footage ±54,470 SF
 - Relocation of an existing structure: Total square footage _____

B. APPLICANT INFORMATION

1. Applicant Status Owner (title holder) **Agent**
2. Name of Applicant(s): Jarod Stubbs P.E. Title: Civil Engineer
 Company name (if applicable): Kimley-Horn
 Mailing Address: 189 S. Orange Ave. Suite 1000
 City: Orlando State: FL Zip: 32801
 Telephone: (407) 409-7002 Fax: () Email: jarod.stubbs@kimley-horn.com

PLEASE NOTE: Florida has a very broad public records law. Most written communications to or from government officials regarding government business is subject to public records requests. Your e-mail address and communications may be subject to public disclosure.

3. If the applicant is agent for the property owner*.
 Property Owner Name (title holder): Daniel Hotte of GWC Development Partners, LLC
 Mailing Address: 2682 W Noegel Rd
 City: Lake City State: FL Zip: 32055
 Telephone: (407) 580-5173 Fax: () Email: dberry@shafferconst.com

PLEASE NOTE: Florida has a very broad public records law. Most written communications to or from government officials regarding government business is subject to public records requests. Your e-mail address and communications may be subject to public disclosure.

***Must provide an executed Property Owner Affidavit Form authorizing the agent to act on behalf of the property owner.**

C. ADDITIONAL INFORMATION

- 1. Is there any additional contract for the sale of, or options to purchase, the subject property?
If yes, list the names of all parties involved: _____
If yes, is the contract/option contingent or absolute: Contingent Absolute
- 2. Has a previous application been made on all or part of the subject property? Yes No
Future Land Use Map Amendment: Yes _____ No _____
Future Land Use Map Amendment Application No. _____
Site Specific Amendment to the Official Zoning Atlas (Rezoning): Yes _____ No _____
Site Specific Amendment to the Official Zoning Atlas (Rezoning) Application No. _____
Variance: Yes _____ No _____
Variance Application No. _____
Special Exception: Yes _____ No _____
Special Exception Application No. _____

D. ATTACHMENT/SUBMITTAL REQUIREMENTS

- 1. Vicinity Map – Indicating general location of the site, abutting streets, existing utilities, complete legal description of the property in question, and adjacent land use.
- 2. Site Plan – Including, but not limited to the following:
 - a. Name, location, owner, and designer of the proposed development.
 - b. Present zoning for subject site.
 - c. Location of the site in relation to surrounding properties, including the means of ingress and egress to such properties and any screening or buffers on such properties.
 - d. Date, north arrow, and graphic scale not less than one inch equal to 50 feet.
 - e. Area and dimensions of site (Survey).
 - f. Location of all property lines, existing right-of-way approaches, sidewalks, curbs, and gutters.
 - g. Access to utilities and points of utility hook-up.
 - h. Location and dimensions of all existing and proposed parking areas and loading areas.
 - i. Location, size, and design of proposed landscaped areas (including existing trees and required landscaped buffer areas).
 - j. Location and size of any lakes, ponds, canals, or other waters and waterways.
 - k. Structures and major features fully dimensioned including setbacks, distances between structures, floor area, width of driveways, parking spaces, property or lot lines, and percent of property covered by structures.
 - l. Location of trash receptacles.
 - m. For multiple-family, hotel, motel, and mobile home park site plans:
 - i. Tabulation of gross acreage.
 - ii. Tabulation of density.
 - iii. Number of dwelling units proposed.
 - iv. Location and percent of total open space and recreation areas.
 - v. Percent of lot covered by buildings.

- vi. Floor area of dwelling units.
 - vii. Number of proposed parking spaces.
 - viii. Street layout.
 - ix. Layout of mobile home stands (for mobile home parks only).
3. Stormwater Management Plan—Including the following:
 - a. Existing contours at one foot intervals based on U.S. Coast and Geodetic Datum.
 - b. Proposed finished elevation of each building site and first floor level.
 - c. Existing and proposed stormwater management facilities with size and grades.
 - d. Proposed orderly disposal of surface water runoff.
 - e. Centerline elevations along adjacent streets.
 - f. Water management district surface water management permit.
 4. Fire Department Access and Water Supply Plan: The Fire Department Access and Water Supply Plan must demonstrate compliance with Chapter 18 of the Florida Fire Prevention Code, be located on a separate signed and sealed plan sheet, and must be prepared by a professional fire engineer licensed in the State of Florida. The Fire Department Access and Water Supply Plan must contain fire flow calculations in accordance with the Guide for Determination of Required Fire Flow, latest edition, as published by the Insurance Service Office ("ISO") and/or Chapter 18, Section 18.4 of the Florida Fire Prevention Code, whichever is greater.
 5. Concurrency Impact Analysis: Concurrency Impact Analysis of impacts to public facilities. For commercial and industrial developments, an analysis of the impacts to Transportation, Potable Water, Sanitary Sewer, and Solid Waste impacts are required.
 6. Comprehensive Plan Consistency Analysis: An analysis of the application's consistency with the Comprehensive Plan (analysis must identify specific Goals, Objectives, and Policies of the Comprehensive Plan and detail how the application complies with said Goals, Objectives, and Policies).
 7. Legal Description with Tax Parcel Number (In Word Format).
 8. Proof of Ownership (i.e. deed).
 9. Agent Authorization Form (signed and notarized).
 10. Proof of Payment of Taxes (can be obtained online via the Columbia County Tax Collector's Office).
 11. Fee. The application fee for a Site and Development Plan Application is \$200.00. No application shall be accepted or processed until the required application fee has been paid.

NOTICE TO APPLICANT

All eleven (11) attachments are required for a complete application. Once an application is submitted and paid for, a completeness review will be done to ensure all the requirements for a complete application have been met. If there are any deficiencies, the applicant will be notified in writing. If an application is deemed to be incomplete, it may cause a delay in the scheduling of the application before the Planning & Zoning Board.

A total of ten (10) copies of proposed site plan application and all support materials must be submitted along with a PDF copy on a CD. See City of Lake City submittal guidelines for additional submittal requirements.

THE APPLICANT ACKNOWLEDGES THAT THE APPLICANT OR AGENT MUST BE PRESENT AT THE PUBLIC HEARING BEFORE THE PLANNING AND ZONING BOARD, AS ADOPTED IN THE BOARD RULES AND PROCEDURES, OTHERWISE THE REQUEST MAY BE CONTINUED TO A FUTURE HEARING DATE.

I hereby certify that all of the above statements and statements contained in any documents or plans submitted herewith are true and accurate to the best of my knowledge and belief.

Jared Stubbs
Applicant/Agent Name (Type or Print)

[Signature]
Applicant/Agent Signature

6/8/2022
Date

Applicant/Agent Name (Type or Print)

Applicant/Agent Signature

Date

STATE OF FLORIDA
COUNTY OF Orange

The foregoing instrument was acknowledged before me this 8th day of June 2022, by (name of person acknowledging) Jared Stubbs.

(NOTARY SEAL or



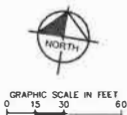
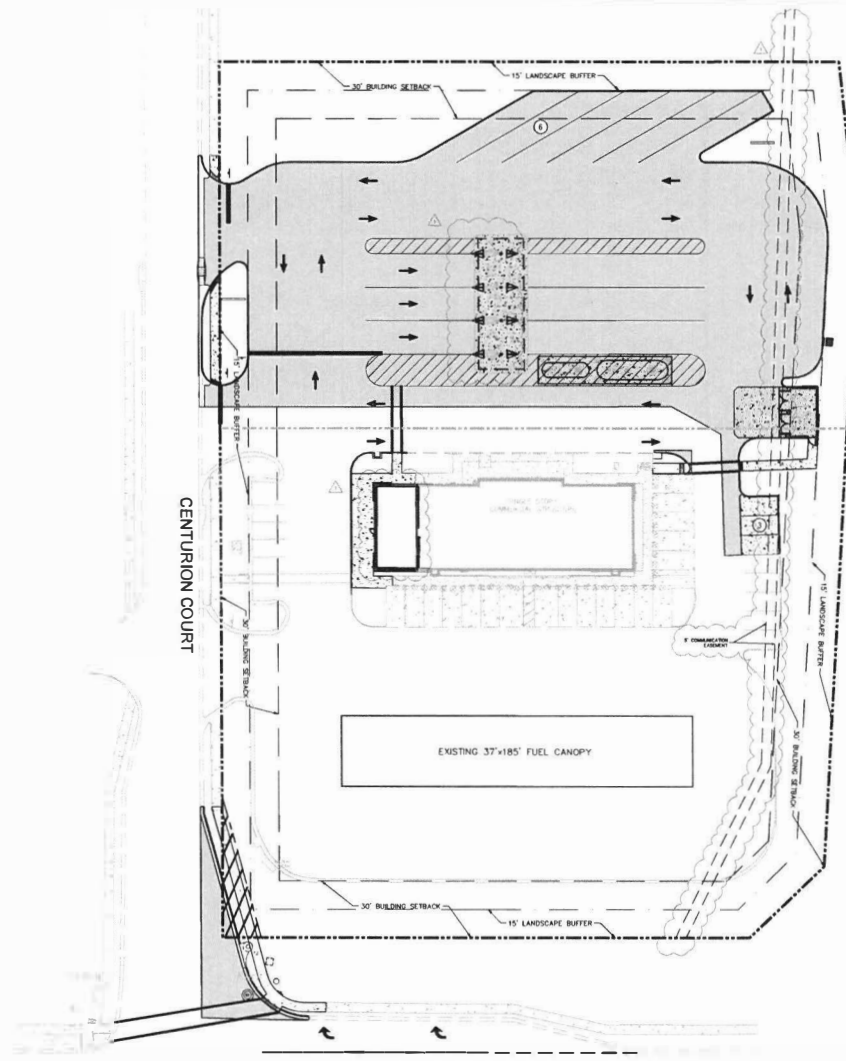
[Signature]
Signature of Notary

Printed Name of Notary

Personally Known OR Produced Identification
Type of Identification Produced

City of Lake City - Growth Management Department
205 North Marion Ave, Lake City, FL 32055 ♦ (386) 719-5750

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- NOTES:**
- ALL CURB DIMENSIONS ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
 - BUILDING DIMENSIONS ARE TO THE OUTSIDE FACE OF BUILDING UNLESS OTHERWISE NOTED.
 - REFER TO ARCHITECTURAL AND STRUCTURAL PLANS TO VERIFY A LIBERALIZING DIMENSIONS.
 - REFER TO SIGNAGE PLAN FOR MONUMENT SIGN DETAILS.
 - SEE MEP PLANS FOR ELECTRICAL DETAILS.
 - ALL PROPOSED ON-SITE STRIPING AND PAVEMENT MARKINGS SHALL BE PAINTED UNLESS OTHERWISE INDICATED IN ACCORDANCE WITH 2007 INDEX: 711-501.
 - REFER TO ARCHITECTURAL PLANS FOR PROPOSED TRASH CAN LOCATIONS AND DESIGN.
 - BOLLARDS IN SIDEWALK ADJACENT TO BUILDING SHALL BE COVERED WITH RED PLASTIC TO VEHICLES TO BE SUPPLIED BY CONTRACTOR.
 - BOLLARDS UNDER CANOPY SHALL BE COVERED WITH GRAY PLASTIC COVERS TO BE SUPPLIED BY CONTRACTOR (SEE FUEL PUMP DESIGNER PLANS FOR MORE DETAIL).
 - REFER TO ARCHITECTURAL PLANS FOR LIGHTING AND ELECTRICAL PLANS.
 - ALL SIGNAGE AND PAVEMENT MARKINGS SHALL MEET MINIMUM AASHTO STANDARDS.
 - ALL SIGNAGE SHALL MEET THE REQUIREMENTS OF CITY OF LAKE CITY LAND DEVELOPMENT CODE, CHAPTER 7, SEC. 760.

SITE DATA

PROJECT AREA:	3.4654 ACRES (150,938 SF)
FUTURE LAND USE:	COMMERCIAL
EXISTING ZONING:	CH - COMMERCIAL, HIGHWAY INTERCHANGE
EXISTING USE:	UNDEVELOPED
PROPOSED USE:	COMMERCIAL
BUILDING HEIGHT:	
PROPOSED:	1 STORY / 25 FT
FAR:	0.0584
EXISTING PAVEMENT AREA:	83,835 SF (1.92 AC) (25.41%)
PROPOSED PAVEMENT AREA:	38,084 SF (0.875 AC) (25.23%)
PROPOSED IMPERVIOUS AREA:	
BUILDING AREA:	5,883 SF (0.134 AC) (3.84%)
EXISTING BLDG. IMPERVIOUS:	
ASPHALT/CONCRETE AREA:	8108,960 SF (2.45 AC) (70.87%)
EXISTING-HIGHWAY EXPANSION:	
TOTAL IMPERVIOUS AREA:	2112,843 SF (2.59 AC) (74.78%)

PARKING PROVIDED

CONVENIENCE STORE (WITH GAS STATION)	
1 SPACE / 150 SF MIN. STORAGE AREA (5,043 SF)	34
TOTAL REQUIRED PARKING:	34

PARKING PROVIDED

PROPOSED HANDICAP SPACES:	2
PROPOSED REGULAR SPACES:	32
PROPOSED SEMI-TRUCK SPACES:	0
PROPOSED ON-SITE SPACES:	38

SETBACKS

REQUIRED SPACES:	0
PROVIDED SPACES:	0

BUILDING SETBACKS

REQUIRED:	PROVIDED:
SIDE (WEST): 30 FT	85 FT
REAR (NORTH): 30 FT	220 FT
FRONT (SOUTH): 30 FT	188 FT
SIDE (EAST): 30 FT	187 FT

LANDSCAPE SETBACKS

REQUIRED:	PROVIDED:
SIDE (WEST): 15 FT	15 FT
REAR (NORTH): 15 FT	15 FT
FRONT (SOUTH): 15 FT	0 FT
SIDE (EAST): 15 FT	15 FT

LEGEND

	PROPERTY LINE (TOP)
	PROPOSED ASPHALT PAVEMENT (SEE DETAIL SHEET C7-D)
	PROPOSED CONCRETE SIDEWALK (SEE DETAIL SHEET C7-D)
	PROPOSED MEDIUM DUTY CONCRETE (SEE DETAIL SHEET C7-D)
	PROPOSED HEAVY DUTY CONCRETE (SEE DETAIL SHEET C7-D)

Circle K - U.S. 90 & I-75 Interchange Area Calculations

Lot #	Total Lot Area (AC)	Existing Impervious Area (SF)	Proposed Impervious Area (SF)	Existing Impervious Area (SF)	Proposed Impervious Area (SF)	Allowed Impervious Area (SF)
1	1.979	2292,000	6254,000	22483,000	6373,000	64904,4
2	1.223	4818,930	4048,000	6000,311	46367,60	40275,2
3	0.763	11464,700	0	8711,37	2751,800	8880,32
Total (SF)	15098,200	8360,130	6732,000	38094,68	11284,530	113580,900
Total (AC)	3.465	1.900	1.545	0.875	2.591	2.807
%	100.00%	55.40%	44.59%	25.23%	74.78%	75.22%

VERTICAL DATUM ELEVATIONS ARE REFERENCED TO PLAT BENCHMARKS AS SHOWN ON THE FACE OF THE PLAT OF LATERAL CROSSING REPLAT OF LOTS 2 AND 3, PLAT BOOK 9, PAGE 151

CALL 48 HOURS BEFORE YOU DIG

IT'S THE LAW! CALL 811

Some states require you to call before you dig.

NO.	DATE	BY	REVISIONS
1	08/29/22	LAG	ARCHITECTURE REVISION, LAYOUT

Kimley Horn

10232 HALEY LANE AND ASSOCIATES, INC.
 789 S. GRANGE AVENUE, SUITE 1000, ORLANDO, FL 32801
 PHONE: 407-888-1511
 WWW.KIMLEY-HORN.COM REGISTRY NO. 35108

Kimley Horn

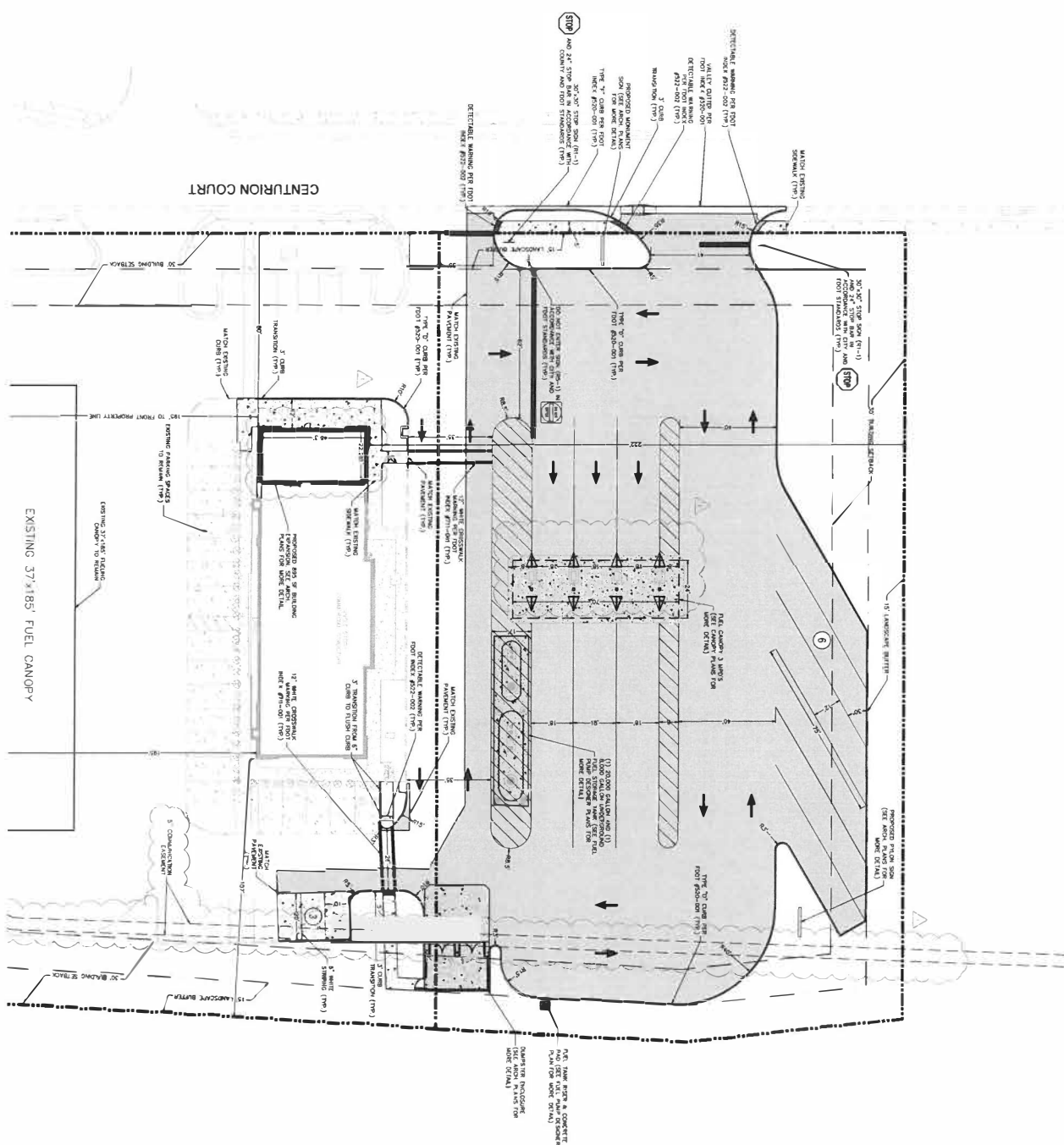
LAKE COUNTY PROJECT 14395504D
 05/01/2022
 SCALE AS SHOWN
 DESIGNED BY ELP
 DRAWN BY JCS
 CHECKED BY JCS
 DATE: 05/01/2022

OVERALL SITE PLAN

CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION

FLORIDA
 CITY OF LAKE CITY

SHEET NUMBER
C4.0



INTERSTATE 75 OFF-RAMP

GENERAL NOTES:
 1. ALL DIMENSIONS ARE TO FACE UNLESS OTHERWISE NOTED.
 2. CONSTRUCTION SHALL BE TO THE CENTER FACE OF BUILDING UNLESS OTHERWISE NOTED.
 3. REFER TO ARCHITECTURAL AND STRUCTURAL PLANS TO VERIFY ALL BUILDING DIMENSIONS.
 4. REFER TO SOUND PLANS FOR FOUNDATION SOIL DETAILS.
 5. SET ASIP PAVES FOR ELECTRICAL SERVICES.
 6. REFER TO ELECTRICAL PLANS FOR ELECTRICAL SERVICE DETAILS.
 7. REFER TO ARCHITECTURAL PLANS FOR FOUNDATION TRAIL CAN LOCATIONS.
 8. BUILDINGS IN SPECIAL ADJACENT TO BUILDING SHALL BE COVERED WITH RIGID FLUOROCARBON TO BE SUPPLIED BY CONTRACTOR.
 9. BUILDING JAMES CANOPY SHALL BE COVERED WITH RIGID FLUOROCARBON TO BE SUPPLIED BY CONTRACTOR.
 10. REFER TO ARCHITECTURAL PLANS FOR SET LIGHTING AND ELECTRICAL SERVICES.
 11. ALL SERVICE AND UTILITY UTILITIES SHALL BE SET WITHIN AND FROM DETAILERS' OFFICE TO VERIFY THE REQUIREMENTS OF CITY OF LAKE CITY LAND DEVELOPMENT CODE.

LEGEND:
 PROPOSED ASPHALT PAVEMENT (DET. DETAIL SHEET C7.0)
 PROPOSED CONCRETE DRIVEWAY (DET. DETAIL SHEET C7.0)
 PROPOSED ASPHALT DRIVEWAY (DET. DETAIL SHEET C7.0)
 PROPOSED ASPHALT DRIVEWAY TO EXISTING ASPHALT DRIVEWAY (DET. DETAIL SHEET C7.0)
 PROPOSED ASPHALT DRIVEWAY TO EXISTING ASPHALT DRIVEWAY (DET. DETAIL SHEET C7.0)
 PROPOSED ASPHALT DRIVEWAY TO EXISTING ASPHALT DRIVEWAY (DET. DETAIL SHEET C7.0)

811 CALL BEFORE YOU DIG
 REFER TO THE LAW
 CALL 811
 811 CALL CENTER
 800.487.4811



CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION
 CITY OF LAKE CITY FLORIDA

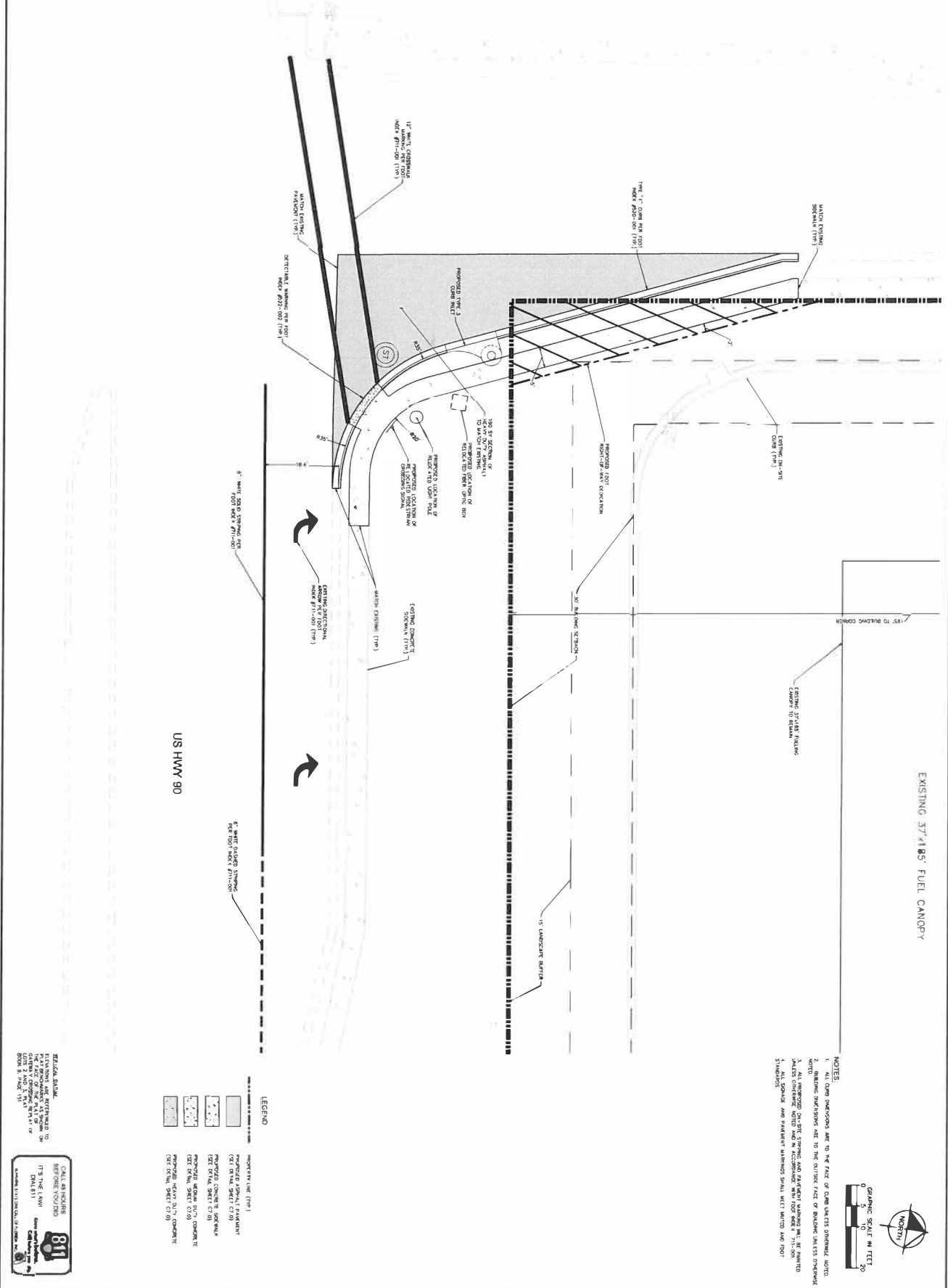
SITE PLAN

KHA PROJECT 149880040
 DATE 05/04/2022
 DRAWN BY E.J.F.
 CHECKED BY JCS

Kimley-Horn
 © 2022 KIMLEY-HORN AND ASSOCIATES, INC.
 189 S. ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801
 PHONE: 407-888-1511
 WWW.KIMLEY-HORN.COM REGISTRY NO. 35108

No.	REVISIONS	DATE	BY
1		08/29/22	AG

This document, together with the concepts and designs presented herein, is an instrument of service, is intended only for the specific purpose and client for which it was prepared. Review and approval reliance on this document without written authorization and validation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.



LEGEND

[Symbol]	PROPOSED CANOPY
[Symbol]	EXISTING CANOPY
[Symbol]	PROPOSED CONCRETE SIDEWALK
[Symbol]	EXISTING CONCRETE SIDEWALK
[Symbol]	PROPOSED ASPHALT DRIVEWAY
[Symbol]	EXISTING ASPHALT DRIVEWAY

NOTES

- ALL CANOPY DIMENSIONS ARE TO THE FACE OF CANOPY TRACKS UNLESS OTHERWISE NOTED.
- SEE ALL DIMENSIONS AND ELEVATIONS ON THE OTHER SIDE OF SHEET FOR DIMENSIONS AND ELEVATIONS.
- ALL DIMENSIONS ARE TO THE FACE OF CANOPY TRACKS UNLESS OTHERWISE NOTED.
- ALL DIMENSIONS ARE TO THE FACE OF CANOPY TRACKS UNLESS OTHERWISE NOTED.

GRAPHIC SCALE IN FEET

0 10 20

US HWY 90

EXISTING 37'x183' FUELING CANOPY TO BE DEMOLISHED

EXISTING CONCRETE SIDEWALK

PROPOSED CONCRETE SIDEWALK

PROPOSED 37'x183' FUEL CANOPY

PROPOSED ASPHALT DRIVEWAY

EXISTING ASPHALT DRIVEWAY

PROPOSED 37'x183' FUELING CANOPY

PROPOSED CONCRETE SIDEWALK

EXISTING CONCRETE SIDEWALK

PROPOSED ASPHALT DRIVEWAY

EXISTING ASPHALT DRIVEWAY

PROPOSED 37'x183' FUELING CANOPY

PROPOSED CONCRETE SIDEWALK

EXISTING CONCRETE SIDEWALK

PROPOSED ASPHALT DRIVEWAY

EXISTING ASPHALT DRIVEWAY

PROPOSED 37'x183' FUELING CANOPY

PROPOSED CONCRETE SIDEWALK

EXISTING CONCRETE SIDEWALK

PROPOSED ASPHALT DRIVEWAY

EXISTING ASPHALT DRIVEWAY

PROPOSED 37'x183' FUELING CANOPY

PROPOSED CONCRETE SIDEWALK

EXISTING CONCRETE SIDEWALK

PROPOSED ASPHALT DRIVEWAY

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PROPOSED 37'x183' FUELING CANOPY

PROPOSED CONCRETE SIDEWALK

EXISTING CONCRETE SIDEWALK

PROPOSED ASPHALT DRIVEWAY

EXISTING ASPHALT DRIVEWAY

PROPOSED 37'x183' FUELING CANOPY

PROPOSED CONCRETE SIDEWALK

EXISTING CONCRETE SIDEWALK

PROPOSED ASPHALT DRIVEWAY

EXISTING ASPHALT DRIVEWAY

CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION
 INTERSECTION MODIFICATION PLAN
 CITY OF LAKE CITY FLORIDA

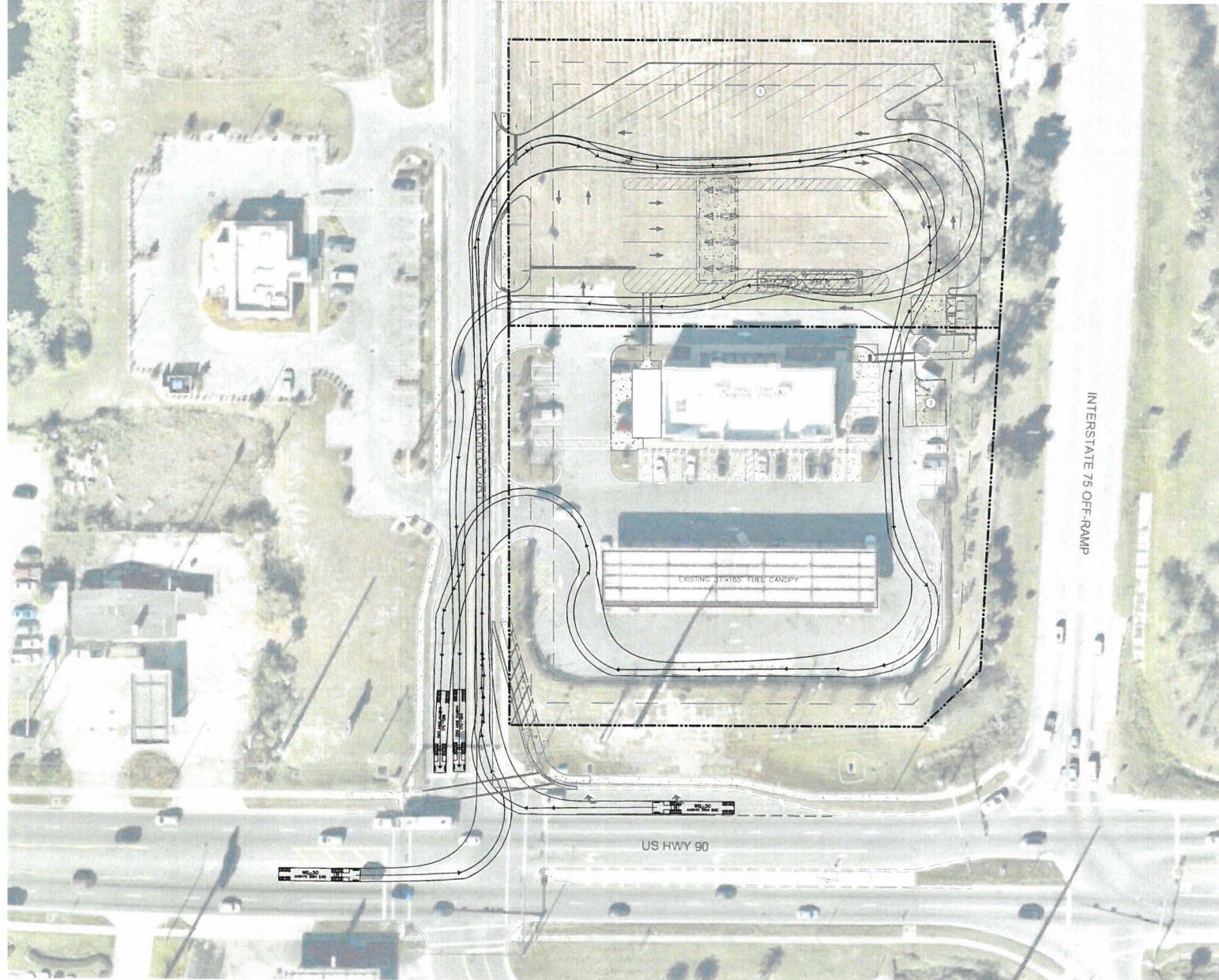
KHA PROJECT: 143880040
 DATE: 05/04/2022
 SCALE: AS SHOWN
 DESIGNED BY: E.S.P.
 DRAWN BY: E.S.P.
 CHECKED BY: JCS

Kimley»Horn
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 189 S GRANGE AVENUE, SUITE 1000, ORLANDO, FL 32801
 PHONE: 407-898-1511
 WWW.KIMLEY-HORN.COM REGISTRY NO. 35106

NO.	REVISIONS	DATE	BY
1		08/29/22	AC

423

PLATTEN, B.J., ENGINEER, 10100 N. W. 17th Ave., Suite 200, Fort Lauderdale, FL 33305. PROJECT NO. 149880040. SHEET NO. C4.3 OF 10. DATE: 05/04/2022. 08:59:45 AM. THIS DOCUMENT IS THE PROPERTY OF KIMLEY-HORN AND ASSOCIATES, INC. IT IS TO BE USED ONLY FOR THE PROJECT AND SITE SPECIFICALLY IDENTIFIED HEREIN. IT IS NOT TO BE REPRODUCED, COPIED, OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, WITHOUT THE WRITTEN PERMISSION OF KIMLEY-HORN AND ASSOCIATES, INC.



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811
 IT'S THE LAW! Dial 811
 Know what's below. Call before you dig.
 SUNSHINE STATE ONE CALL OF FLORIDA, INC.

TRUCK TURNING MOVEMENTS		CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION		CITY OF LAKE CITY, FLORIDA	
Kimley»Horn <small>© 2022 KIMLEY-HORN AND ASSOCIATES, INC. 199 S. DRANNE AVENUE, SUITE 100-1501 WWW.KIMLEY-HORN.COM REGISTRY NO. 33108</small>				NO.	DATE
KHA PROJECT: 149880040 DATE: 05/04/2022 SCALE: AS SHOWN DRAWN BY: ECF CHECKED BY: CJF PREPARED BY: JCS		KHA PROJECT: 149880040 DATE: 05/04/2022 SCALE: AS SHOWN DRAWN BY: ECF CHECKED BY: CJF PREPARED BY: JCS		REVISIONS	DATE
SHEET NUMBER C4.3					

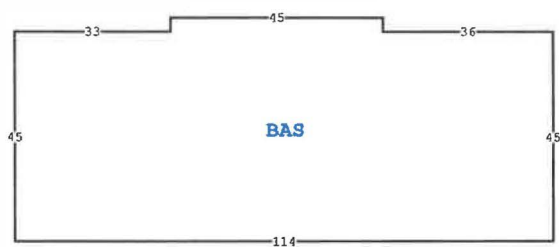
FGH OBJECTION 011



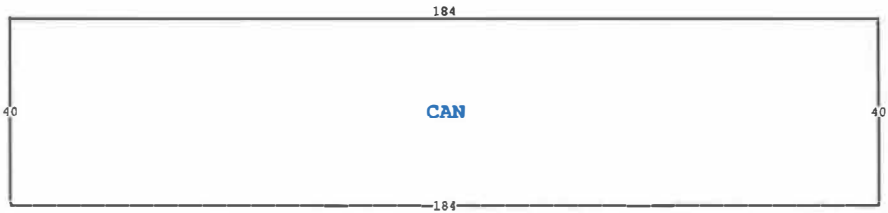
BUILDING CHARACTERISTICS	
ELEMENT	CD CONSTRUCTION
Exterior Wall	17 MSNRY STUC 90
Exterior Wall	21 STONE 10
Roof Structur	09 RIDGE FRME 100
Roof Cover	04 BUILT-UP 100
Interior Wall	08 DECORATIVE 100
Interior Floo	15 HARDTILE 100
Ceiling	01 FIN.SUSPD 100
Air Condition	06 ENG CENTRL 100
Heating Type	09 ENG F AIR 100
Fixtures	12 100
Frame	03 MASONRY 100
Story Height	12 100
RMS	0 100
Stories	1. 1. 100
Units	0 100
Condition Adj	04 04 100

MARKET ADJUSTMENTS											
TYPE	MDL	EFF. AREA	TOT ADJ PTS	EFF. BASE RATE	REPL. COST NEW	AYB	EYB	ECON	FNCT	NORM	% COND
1	NBHD CONVE	- 0%	- 0	97.50	728,618	2017	2017	0	0	3.00	97.00

COLUMBIA COUNTY PROPERTY		PAGE 1 of 1
VALUATION SUMMARY		
VALUATION BY		STANDARD
Tax Group: 1	Tax Dist:	
BUILDING MARKET VALUE		706,759
TOTAL MARKET OB/XF VALUE		103,200
TOTAL LAND VALUE - MARKET		1,239,211
TOTAL MARKET VALUE		2,049,170
SOH/AGL Deduction		0
ASSESSED VALUE		2,049,170
TOTAL EXEMPTION VALUE		0
BASE TAXABLE VALUE		2,049,170
TOTAL JUST VALUE		2,049,170
INCOME VALUE		
PREVIOUS YEAR MKT VALUE		2,056,457



TYPE	TOTAL GROSS AREA	PCT OF BASE	TOT ADJ AREA	SUBAREA MARKET VALUE
BAS	5,265	100	5,265	497,938
CAN	7,360	30	2,208	208,822



TOTALS	12,625	7,473	706,759
--------	--------	-------	---------

EXTRA FEATURES										BLD DATE		LGL DATE				
L N	OB/XF CODE	DESCRIPTION	BLD CAP	L	W	UNITS	UT	Adj R	ADJ UNIT PRICE	ORIG COND	YEAR ON	YEAR ACTUAL	Q	% COND	OB/XF MKT VALUE	NOTES
1	0260	PAVEMENT-A	0	0	0	36,400.00	UT	1.60	1.60	100	2017	2017	3	100	58,240	
2	0166	CONC, PAVMT	0	0	0	11,300.00	UT	2.25	2.25	100	2017	2017	3	100	25,425	
3	0253	LIGHTING	0	0	0	10.00	UT	1,500.00	1,500.00	100	2017	2017	3	100	15,000	
4	0164	CONC BIN	0	0	0	308.00	UT	11.00	11.00	100	2017	2017	3	100	3,388	
	59	FENCE/WOOD	0	0	0	74.00	UT	15.50	15.50	100	2017	2017	3	100	1,147	

143 NW CENTURION Ct, LAKE CITY

LAND DESCRIPTION										TOTAL OB/XF		103,200
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L N	USE CODE	LAND USE DESCRIPTION	CAP	R D	LOC ZONE	FRONT	DEPTH	TOT LND UTS	UNIT TYPE	D	DPTH FACT	% COND	TOT ADJ	UNIT PRICE	ADJ UNIT PRICE	LAND VALUE	OTHER ADJUSTMENTS AND NOTES	YEAR	DENSITY	DECL	FRZ	YR	CONSRV
1	1410	CONV STORE	0			0.00	0.00	86,206.00	SF		1.00	1.00	1.25	11.50	14.38	1,239,211							

PERMIT NUM	DESCRIPTION	AMT	ISSUED

SALES DATA						
OFF RECORD Number	DATE	TYPE INST	Q U	V I	RSN CD	SALE PRICE
1339/0654	6/20/2017	WD Q	Q	I	01	2,487,200

GRANTOR: GWC DEVELOPMENT PARTN
GRANTEE: ASPRI INVESTMENTS L

BUILDING NOTES

BUILDING DIMENSIONS
BAS= W36 N3 W45 S3 W33 S45 E114 PTR=S50 E35 CAN= W184 S40 E184 N40S W35 N50S N45S.

EXHIBIT B



GROWTH MANAGEMENT
205 North Marion Ave
Lake City, FL 32055
Telephone: (386) 719-5750
E-mail: growthmanagement@lcfla.com

FOR PLANNING USE ONLY
Application # _____
Application Fee \$200.00
Receipt No. _____
Filing Date _____
Completeness Date _____

SPECIAL EXCEPTION

A. PROJECT INFORMATION

- 1. Project Name: _____
2. Address of Subject Property: _____
3. Parcel ID Number(s): _____
4. Future Land Use Map Designation: _____
5. Zoning Designation: _____
6. Acreage: _____
7. Existing Use of Property: _____
8. Proposed use of Property: _____
9. Section of the Land Development Regulations ("LDRs") for which a Special Exception is requested (Provide a Detailed Description): _____

B. APPLICANT INFORMATION

- 1. Applicant Status [] Owner (title holder) [] Agent
2. Name of Applicant(s): _____ Title: _____
Company name (if applicable): _____
Mailing Address: _____
City: _____ State: _____ Zip: _____
Telephone (____) _____ Fax: (____) _____ Email: _____

PLEASE NOTE: Florida has a very broad public records law. Most written communications to or from government officials regarding government business is subject to public records requests. Your e-mail address and communications may be subject to public disclosure.

- 3. If the applicant is agent for the property owner*.
Property Owner Name (title holder): _____
Mailing Address: _____
City: _____ State: _____ Zip: _____
Telephone: (____) _____ Fax: (____) _____ Email: _____

PLEASE NOTE: Florida has a very broad public records law. Most written communications to or from government officials regarding government business is subject to public records requests. Your e-mail address and communications may be subject to public disclosure.

*Must provide an executed Property Owner Affidavit Form authorizing the agent to act on behalf of the property owner.

C. ADDITIONAL INFORMATION

- 1. Is there any additional contract for the sale of, or options to purchase, the subject property?
If yes, list the names of all parties involved: _____
If yes, is the contract/option contingent or absolute: Contingent Absolute
- 2. Has a previous application been made on all or part of the subject property? Yes No
Future Land Use Map Amendment: Yes _____ No _____
Future Land Use Map Amendment Application No. _____
Rezoning Amendment: Yes _____ No _____
Rezoning Amendment Application No. _____
Site Specific Amendment to the Official Zoning Atlas (Rezoning): Yes _____ No _____
Site Specific Amendment to the Official Zoning Atlas (Rezoning) Application No. _____
Variance: Yes _____ No _____
Variance Application No. _____
Special Exception: Yes _____ No _____
Special Exception Application No. _____

D. ATTACHMENT/SUBMITTAL REQUIREMENTS

- 1. Analysis of Section 11.3 of the Land Development Regulations (“LDRs”):
 - a. Whether the proposed use would be in conformance with the city's comprehensive plan and would have an adverse effect on the comprehensive plan.
 - b. Whether the proposed use is compatible with the established land use pattern.
 - c. Whether the proposed use would materially alter the population density pattern and thereby increase or overtax the load on public facilities such as schools, utilities, and streets.
 - d. Whether changed or changing conditions find the proposed use to be advantageous to the community and the neighborhood.
 - e. Whether the proposed use will adversely influence living conditions in the neighborhood.
 - f. Whether the proposed use will create or excessively increase traffic congestion or otherwise affect public safety.
 - g. Whether the proposed use will create a drainage problem.
 - h. Whether the proposed use will seriously reduce light and air to adjacent areas.
 - i. Whether the proposed use will adversely affect property values in the adjacent area.
 - j. Whether the proposed use will be a deterrent to the improvement or development of adjacent property in accord with existing regulations.
 - k. Whether the proposed use is out of scale with the needs of the neighborhood or the community

2. Vicinity Map – Indicating general location of the site, abutting streets, existing utilities, complete legal description of the property in question, and adjacent land use.
3. Site Plan – Including, but not limited to the following:
 - a. Name, location, owner, and designer of the proposed development.
 - b. Present zoning for subject site.
 - c. Location of the site in relation to surrounding properties, including the means of ingress and egress to such properties and any screening or buffers on such properties.
 - d. Date, north arrow, and graphic scale not less than one inch equal to 50 feet.
 - e. Area and dimensions of site (Survey).
 - f. Location of all property lines, existing right-of-way approaches, sidewalks, curbs, and gutters.
 - g. Access to utilities and points of utility hook-up.
 - h. Location and dimensions of all existing and proposed parking areas and loading areas.
 - i. Location, size, and design of proposed landscaped areas (including existing trees and required landscaped buffer areas).
 - j. Location and size of any lakes, ponds, canals, or other waters and waterways.
 - k. Structures and major features fully dimensioned including setbacks, distances between structures, floor area, width of driveways, parking spaces, property or lot lines, and percent of property covered by structures.
 - l. Location of trash receptacles.
4. Stormwater Management Plan—Including the following:
 - a. Existing contours at one foot intervals based on U.S. Coast and Geodetic Datum.
 - b. Proposed finished elevation of each building site and first floor level.
 - c. Existing and proposed stormwater management facilities with size and grades.
 - d. Proposed orderly disposal of surface water runoff.
 - e. Centerline elevations along adjacent streets.
 - f. Water management district surface water management permit.
5. Fire Department Access and Water Supply Plan: The Fire Department Access and Water Supply Plan must demonstrate compliance with Chapter 18 of the Florida Fire Prevention Code, be located on a separate signed and sealed plan sheet, and must be prepared by a professional fire engineer licensed in the State of Florida. The Fire Department Access and Water Supply Plan must contain fire flow calculations in accordance with the Guide for Determination of Required Fire Flow, latest edition, as published by the Insurance Service Office (“ISO”) and/or Chapter 18, Section 18.4 of the Florida Fire Prevention Code, whichever is greater.
6. Concurrency Impact Analysis: Concurrency Impact Analysis of impacts to public facilities. For commercial and industrial developments, an analysis of the impacts to Transportation, Potable Water, Sanitary Sewer, and Solid Waste impacts are required.

7. Comprehensive Plan Consistency Analysis: An analysis of the application's consistency with the Comprehensive Plan (analysis must identify specific Goals, Objectives, and Policies of the Comprehensive Plan and detail how the application complies with said Goals, Objectives, and Policies).
8. Legal Description with Tax Parcel Number (In Microsoft Word Format).
9. Proof of Ownership (i.e. deed).
10. Agent Authorization Form (signed and notarized).
11. Proof of Payment of Taxes (can be obtained online via the Columbia County Tax Collector's Office).
12. Fee. The application fee for a Special Exception Application is \$200.00. No application shall be accepted or processed until the required application fee has been paid.

NOTICE TO APPLICANT

All twelve (12) attachments are required for a complete application. Once an application is submitted and paid for, a completeness review will be done to ensure all the requirements for a complete application have been met. If there are any deficiencies, the applicant will be notified in writing. If an application is deemed to be incomplete, it may cause a delay in the scheduling of the application before the Board of Adjustment.

A total of ten (10) copies of proposed Special Exception Application and support material, and a PDF copy on a CD, are required at the time of submittal. See Columbia County submittal requirements for more detail.

Before any Special Exception shall be granted, the Board of Adjustment shall make a specific finding that it is empowered under Article 3 of the Land Development Regulations to grant the Special Exception described in the petition, and that the granting of the Special Exception will not adversely affect the public interest. Before any Special Exception shall be granted, the Board of Adjustment shall further make a determination that the specific rules governing the individual Special Exception, if any, have been met by the petitioner and that, further, satisfactory provision and arrangement has been made.

In granting any Special Exception to the provisions of Article 4 of the Land Development Regulations, the Board of Adjustment may prescribe appropriate conditions and safeguards in conformity with such regulations, including but not limited to, reasonable time limits within which the action for which the Special Exception requested shall be begun or completed, or both. Violation of such conditions and safeguards, when made a part of the terms under which the Special Exception is granted, shall be deemed a violation of the Land Development Regulations.

The Board of Adjustment requires that the applicant or representative be present at the public hearing to address and answer any questions the Board may have during the public hearing. The application may be continued to future dates if the applicant or representative is not present at the hearing.

The City of Lake City Land Development Regulations require that a sign must be posted on the property ten (10) days prior to the Board to Adjustment hearing date. Once a sign has been posted, it is the property owner’s responsibility to notify the Planning and Zoning Department if the sign has been moved, removed from the property, torn down, defaced or otherwise disturbed so the property can be reposted. If the property is not properly posted until all public hearings before the Board of Adjustment are completed, the Board reserves the right to continue such public hearing until such time as the property can be property posted for the required period of time.

There is a thirty (30) day appeal period after the date of the decision. No additional permitting will be issued until that thirty (30) day period has expired.

I (we) hereby certify that all of the above statements and the statements contained in any papers or plans submitted herewith are true and correct to the best of my (our) knowledge and belief.

APPLICANT ACKNOWLEDGES THAT THE APPLICANT OR REPRESENTATIVE MUST BE PESENT AT THE PUBLIC HEARING BEFORE THE BOARD OF ADJUSTMENT, OTHERWISE THE REQUEST MAYBE CONTINUED TO A FUTURE HEARING DATE.

Applicant/Agent Name (Type or Print)

Applicant/Agent Signature

Date

STATE OF FLORIDA
COUNTY OF _____

The foregoing instrument was acknowledged before me this ____ day of _____, 20____, by (name of person acknowledging).

(NOTARY SEAL or STAMP)

Signature of Notary

Printed Name of Notary

Personally Known _____ OR Produced Identification _____
Type of Identification Produced

Adult Care Center. An adult care center is a private home, institution, building, residence, or other place, whether operated for profit or not, including those places operated by units of government, which undertakes through its ownership or management to provide day personal care for three (3) or more adult persons not related by lineal consanguinity or marriage to the operator, who by reason of illness, physical infirmity, or advanced age are unable to care for themselves during the daylight hours. Nursing homes or residential homes for the aged are not adult care centers.

Adult Care Center, Overnight. An overnight adult care center is an establishment as defined above as an adult care center where adults are cared for not only during the day but overnight and such stay does not exceed twenty-four (24) hours at any one time. An overnight adult care center provides full overnight sleeping facilities for such adults. Nursing homes or residential homes for the aged are not overnight adult care centers.

Adverse Effect. Adverse effect means increases in flood elevations on adjacent properties attributed to physical changes in the characteristics of the Official 100-Year Flood Area due to development.

Alley or Service Drive. An alley or service drive is a public or private right-of-way which affords only a secondary means of access to property abutting thereon.

Alter or Alteration of a Stormwater Management System. Alter or alteration of a stormwater management system is work done other than that necessary to maintain the system's original design and function.

Alteration. Alter or alteration shall mean any change in size, shape, occupancy, character, or use of a building or structure.

Aquifer or Aquifer System. Means a geologic formation, group of formations, or part of a formation that contains sufficient saturated permeable material to yield significant quantities of water to wells and springs.

Area of Shallow Flooding. Area of shallow flooding means a designated AO or VO Zone on the incorporated City's Flood Insurance Rate Map (FIRM) with base flood depths from one (1) to three (3) feet where a clearly defined channel does not exist, where the path of flooding is unpredictable and indeterminate and where velocity flow may be evident.

Area of Special Flood Hazard. Area of special flood hazard is the land so designated on the City's Flood Hazard Boundary Map or the Flood Rate Insurance Map.

Arterial Streets. Arterial streets are streets (roads) which conduct large volumes of traffic over long distances and are functionally classified as such on the Future Traffic Circulation Map of the City's Comprehensive Plan.

Automobile Wrecking or Automobile Wrecking Yard. The term automobile wrecking or automobile wrecking yard refers to the dismantling or disassembling of used motor vehicles or trailers, or the storage, sale, or dumping of dismantled, partially dismantled, obsolete, or wrecked vehicles or their parts.

Automotive Service Station. An automotive service station is an establishment whose principal business is the dispensing at retail of motor fuel and oil primarily for automobiles; and where grease, batteries, tires, and automobile accessories may be supplied and dispensed at retail. In addition, an automotive service station may provide accessory facilities for car washing and polishing (but not commercial car wash facilities) and may render minor repair services. However, major mechanical and body work, straightening of frames or body parts, steam cleaning, painting, tire recapping or regrooving, storage of automobiles not in operating condition, or other work involving undue noise, glare, fumes, smoke, or other characteristics to an extent greater than normally found in such stations are prohibited. An automotive service station is not a repair garage, a body shop, truck stop, or a car wash or a combination thereof.

For the purposes of these land development regulations, where motor fuel pumps are erected for the purpose of dispensing motor fuel at retail primarily for automobiles, such motor fuel pumps shall be considered to constitute an automotive service station, even where additional services which are customarily associated with an automotive service station are not provided. Where such motor fuel pumps are erected in conjunction with a use which is not an automotive service station, each use shall be considered as a separate principal use and as such, each must meet all applicable requirements of these land development regulations (see Article 4 for special design standards for automotive service stations).

Bar, Cocktail Lounge, or Tavern. A bar, cocktail lounge, or tavern is any establishment which is devoted primarily to the retailing and on premises drinking of malt, vinous, or other alcoholic beverages, and which is licensed by the State of Florida to dispense or sell alcoholic beverages.

Base Flood. Base flood means the flood having a one (1) percent chance of being equaled or exceeded in any given year.

Basement. A basement means that portion of a building between floor and ceiling, which is partly below and partly above grade, but so located that the vertical distance from the grade to the floor below is less than the vertical distance from the grade to the ceiling provided, however, that the distance from the grade to the ceiling shall be at least four (4) feet six (6) inches. (see Cellar).

Bed and Breakfast Inn. Bed and breakfast inn means an owner occupied structure converted to function as a conventional single family residence providing a limited number of guest rooms available on a daily rental basis. Kitchen facilities are not available in individual rooms.

Bicycle and Pedestrian Ways. Bicycle and pedestrian ways means any road, path or way which is open to bicycle travel and traffic afoot and from which motor vehicles are excluded.

Block. The term block includes tier or group and means a group of lots existing with well-defined and fixed boundaries, usually being an area surrounded by streets or other physical barriers and having an assigned number, letter, or other name through which it may be identified.

Board of Adjustment. The term Board of Adjustment shall mean the Board of Adjustment of the City, as herein provided for within these land development regulations.

Breakaway Wall. A breakaway wall is a wall that is not part of the structural support of the building and is intended through its design and construction to collapse under specific lateral loading forces without causing damage to the elevated portion of the building or the supporting foundation system.

Buildable Area. The buildable area is that portion of a lot remaining after the required yards have been provided. Buildings may be placed in any part of the buildable area, but limitations on the percent of lot which may be covered by buildings may require open space within the buildable area.

Building. A building is any structure, either temporary or permanent, having a roof impervious to weather, and used or built for the enclosure or shelter of persons, animals, vehicles, goods, merchandise, equipment, materials, or property of any kind. This definition shall include tents, dining cars, trailers, mobile homes, sheds, garages, carports, animal kennels, storerooms, or vehicles serving in any way the function of a building as described herein. This definition of a building does not include screened enclosures not having a roof impervious to weather.

Building Front Yard Setback Line. The building front yard setback line is the rear edge of any required front yard as specified within these land development regulations. (See Article 4)

Building, Height of. Height of building is the vertical distance measured from the established grade at the corner of a front of a building to the highest point of the roof surface of a flat roof, to the deck line of a mansard or Bermuda roof, to the mean height level between eaves and ridge of gable, hip, cone, gambrel and shed roofs, and to a height three-fourths (3/4) the distance from the ground to the apex of A-frame and dome roofs, as depicted in the diagram below. (See Article 4, Exclusions from Height Limitations).

Surety Device. A surety device is an agreement by a subdivider with the City Council for the amount of the estimated construction cost guaranteeing the completion of physical improvements according to plans and specifications within the time prescribed by the agreement.

Surface Water. Surface water means water above the surface of the ground whether or not flowing through definite channels, including the following:

1. Any natural or artificial pond, lake, reservoir, or other area which ordinarily or intermittently contains water and which has a discernible shoreline; or
2. Any natural or artificial stream, river, creek, channel, ditch, canal, conduit culvert, drain, waterway, gully, ravine, street, roadway, swale or wash in which water flows in a definite direction, either continuously or intermittently and which has a definite channel, bed or banks; or
3. Any wetland.

Surficial Aquifer System. Surficial aquifer system means the permeable hydrogeologic unit contiguous with land surface that is comprised principally of unconsolidated to poorly indurated clastic deposits. It also includes well-indurated carbonate rocks, other than those of the Floridan Aquifer System where the Floridan is at or near land surface. Rocks making up the surficial aquifer system belong to all or part of the upper Miocene to Holocene Series. It contains the water table and water within it is under mainly unconfined; but, beds of low permeability may cause semi-confined or locally confined conditions to prevail in its deeper parts. The lower limit of the surficial aquifer system coincides with the top of laterally extensive and vertically persistent beds of much lower permeability. Within the surficial aquifer system, one (1) or more aquifers may be designated based on lateral or vertical variations on water bearing properties.

Surveyor, Land. The term land surveyor shall mean a land surveyor registered under Chapter 472, Florida Statutes, as amended, who is in good standing with the Florida State Board of Engineer Examiners and Land Surveyors.

To Plat. The phrase to plat means to divide or subdivide land into lots, blocks, parcels, tracts, sites, or other divisions, however the same may be designated, and the recording of the plat in the office of the County Clerk in the manner provided for in these land development regulations.

Travel Trailer. A travel trailer is a vehicular, portable structure built on a chassis, designed to be a temporary dwelling for travel, recreational, and vacation purposes, which is:

1. Identified on the unit by the manufacturer as a travel trailer;
2. Not more than eight (8) feet in body width; and
3. Of any weight provided its body length does not exceed thirty-five (35) feet.

Truck Stop. A truck stop is an establishment where the principal use is primarily the refueling and servicing of trucks and tractor trailer rigs. Such establishments may have restaurants or snack bars and sleeping accommodations for the drivers of such over-the-road equipment and may provide facilities for the repair and maintenance of such equipment.

Unsafe Building. An unsafe building is a building or structure that has any of the following conditions, such that the life, health, property, or safety of the general public is endangered:

1. Whenever the stress in any material, member or portion thereof, due to all imposed loads including dead load exceeds the working stresses allowed in the City Building Code for new buildings.
2. Whenever a building, structure or portion thereof has been damaged by fire, flood, earthquake, wind or other cause to the extent that the structural integrity of the buildings or structures is less than it was prior to the damage and is less than the minimum requirement established by the City Building Code for new buildings.

SECTION 4.15 "CHI" COMMERCIAL, HIGHWAY INTERCHANGE

4.15.1 DISTRICTS AND INTENT

The "CHI" Commercial, Highway Interchange category includes one zone district: CHI. This specialized district is intended for areas where adequate lot depth is available to provide development for vehicular related uses primarily serving the traveling public. Uses in such district are subject to criteria and standards intended to preserve the character of the district and to minimize adverse impacts with abutting and nearby uses. This district shall only be applied to interstate highway interchange areas.

4.15.2 PERMITTED PRINCIPAL USES AND STRUCTURES

1. Automotive service and self-service stations (see Section 4.2 for special design standards for automotive service and self-service stations).
2. Rental of automotive vehicles, trailers and trucks.
3. Restaurants.
4. Hotels and Motels.
5. Retail commercial outlets for sale of fruit, gifts, novelties and similar uses catering to tourists.
6. Light manufacturing, assembling, processing, packaging or fabricating in a completely enclosed building.
7. Facilities for storage and distribution of products including wholesale activity.
8. Retail factory outlets for sale of goods.

Unless otherwise specified, the above uses are subject to the following limitations: (1) products to be sold only at retail; and (2) for all developments, site and development plan approval is required (see Article 13).

4.15.3 PERMITTED ACCESSORY USES AND STRUCTURES

1. On the same premises and in connection with permitted principal uses and structures, dwelling units only for occupancy by owners or employees of the principal use.
2. Uses and structures which:
 - a. Are customarily accessory and clearly incidental and subordinate to permitted uses and structures.
 - b. Are located on the same lot as the permitted use or structure, or on a contiguous lot in the same ownership.
 - c. Do not involve operations or structures not in keeping with the character of the district.
3. On-site signs (see Section 4.2).

4.15.4 PROHIBITED USES AND STRUCTURES

1. Dwelling units, except as provided under accessory uses.

2. Any other uses or structures not specifically, provisionally or by reasonable implication permitted herein. Any use which is potentially dangerous, noxious or offensive to neighboring uses in the district or to those who pass on public ways by reason of smoke, odor, noise, glare, fumes, gas, vibration, threat of fire or explosion, emission of particulate matter, interference with radio or television reception, radiation or likely for other reasons to be incompatible with the character of the district.

4.15.5 SPECIAL EXCEPTIONS
(See also Articles 12 and 13)

1. Truck stops.
2. Travel trailer parks or campgrounds (see Section 20.24).
3. Commercial tourist attractions.
4. Package store for sale of alcoholic beverages; bar tavern, or cocktail lounge.
5. Retail commercial outlets for sale of new and used automobiles, motorcycles, trucks and tractors, manufactured homes, boats, heavy machinery and equipment, lumber and building supplies, and monuments.
6. Public buildings and facilities.
7. Bed and breakfast Inns (see Section 4.2).
8. Off-site signs (see Section 4.2).

4.15.6 MINIMUM LOT REQUIREMENTS (area, width)

1. All permitted uses (unless otherwise specified):
 - Minimum site area 1 acre
 - Minimum lot width 200 feet

4.15.7 MINIMUM YARD REQUIREMENTS (depth of front and rear yard, width of side yards)

1. All permitted uses (unless otherwise specified):
 - Front 30 feet
 - Side 30 feet
 - Rear 30 feet

Special Provisions:

No less than 15 feet of the depth of the required front yard shall be maintained as a landscaped area. The depth of this landscaped area shall be measured at right angles to property lines and shall be established along the entire length and contiguous to the designated property line or lines. This landscaped area may be penetrated at right angles by driveways. The remainder of the required yard may be used for offstreet parking, but not for buildings.

The location of any structure (except permitted docks, walkways and piers) shall be setback a minimum of thirty-five (35) feet from wetlands.

The location of any structure (except permitted docks, walkways and piers) shall be setback a minimum of thirty-five (35) feet from perennial streams and creeks.

4.15.8 MAXIMUM HEIGHT OF STRUCTURES

1. Structure height for buildings shall be regulated in accordance with Chapter 5 of the Florida Building Code and minimum yard requirements established in these land development regulations;
2. Height requirements for signs shall be as established in Section 4.2.20.4(8); and
3. Heights for structures other than buildings and signs shall be regulated in accordance with standards established by the Federal Aviation Administration codes and any regulations and guidelines as may be established by the City and/or Airport Committee or Authority.

4.15.9 MAXIMUM LOT COVERAGE BY ALL BUILDINGS

35%

Note: In addition to meeting the required lot, yard, building height, lot coverage, landscaped buffering, and offstreet parking requirements of this section, no structure shall exceed a 1.0 floor area ratio.

4.15.10 MINIMUM LANDSCAPED BUFFERING REQUIREMENTS

(See also Section 4.18)

1. All permitted uses (unless otherwise specified):

Where a use listed under (1) above is erected or expanded on land abutting either (a) residential district or (b) property used for residential purposes in a residential/office district, then the proposed use shall provide a landscaped buffer which shall be not less than twenty (20) feet in width along the affected rear and/or side yards as the case may be.

4.15.13 MINIMUM OFFSTREET PARKING REQUIREMENTS

(See also Section 4.2)

1. Commercial (and service establishments (unless otherwise specified): one (1) space for each one hundred fifty (150) square feet of non-storage floor area, plus, where applicable, one (1) space for each one thousand (1,000) square feet of lot or ground area outside buildings used for any type of sales, display, or activity.
2. Restaurants, cocktail lounges, bars, and taverns: one (1) space for each three (3) seats in public rooms.
3. Hotels and motels: one (1) space for each sleeping room, plus two (2) spaces for the owner or manager, plus required number of spaces for each accessory use such as restaurant, bar, etc. as specified.
4. Warehousing and storage only: one (1) space for each one thousand five hundred (1,500) square feet of floor area.
5. Public buildings and facilities.
6. Bed and breakfast inn; in addition to parking required for the residence, one (1) parking space shall be provided for each guest room. The Board of Adjustment may vary the parking requirement for those properties listed on the City's historic landmark or site list based upon site constraints including, but not limited to, small yards, inadequate space for parking, and the availability of on-street parking.
7. For other special exceptions as specified herein: to be determined by findings in the particular case.

Note: Offstreet loading required (see Section 4.2)

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4.2.6 AUTOMOTIVE SERVICE AND SELF-SERVICE STATIONS

The following regulations shall apply to the location, design, construction, operation, and maintenance of automotive service and self-service stations (with the exception that for automobile self-service stations where self-service gasoline pumps in conjunction with retail and commercial outlets for sale of food, hardware and drugs, there shall be no outside sales of oil, grease, parts or accessories for automobiles and no service except for self-service water, air or carwash).

4.2.6.1 Lot dimensions and area. An automotive service station lot shall be of adequate width and depth to meet all setback requirements, but in no case shall a corner lot have less than one hundred fifty (150) feet of frontage on each street side, and an interior lot shall have a minimum width of at least one hundred fifty (150) feet. A corner lot shall have a minimum area of not less than twenty thousand (20,000) square feet and an interior lot a minimum area of not less than fifteen thousand (15,000) square feet.

4.2.6.2 Lighting. All lights and lighting for an automotive service station shall be so designed and arranged that no source of light shall be visible from any residential district.

4.2.6.3 Location of pumps and structures. No main or accessory building, no sign of any type, and no gasoline pump shall be located within twenty-five (25) feet of the lot line of any property that is zoned for residential purposes. No gasoline pump shall be located within fifteen (15) feet of any street right-of-way line; where a greater street setback line has been established, no gasoline pump shall be located within fifteen (15) feet of such setback line.

4.2.6.4 Curb breaks. A curb break is a driveway or any other point of access or opening for vehicles onto a public street. The number of curb breaks for each automotive service station shall not exceed two (2) for each one hundred fifty (150) feet of street frontage, each break having a width of no more than thirty (30) feet exclusive of transitions and located not closer than fifteen (15) feet of right-of-way lines of any intersection. Curb breaks shall not be closer than fifteen (15) feet to any other property line. There shall be a minimum distance of twenty (20) feet between curb breaks.

4.2.6.5 Trash storage. Adequate, enclosed trash storage facilities shall be provided on the site.

4.2.7 ERECTION OF MORE THAN ONE PRINCIPAL STRUCTURE ON A PLATTED LOT

Whenever any land is subdivided, a building permit for the construction of a building or other principal structure (excluding commercial buildings under common ownership or unified control) shall not be issued for any such structure on less than a lot as platted within such subdivided land.

4.2.8 EXCLUSIONS FROM HEIGHT LIMITATIONS

The height limitations contained in the Schedule of District Regulations do not apply to spires, belfries, cupolas, antennae, water tanks, ventilators, chimneys, elevator shaft enclosures, airport control towers, observation towers, or other appurtenances usually required to be placed above the roof level and, excepting airport control towers and observation towers, not intended for human occupancy; however, the heights of these structures or appurtenances thereto shall not exceed any height limitations prescribed by the Federal Aviation Agency or airport zoning regulations within the flight-approach zone of airports.



November 8, 2022

Paul Dyal
City Hall
205 N. Marion Ave.
Lake City, FL 32055

Dear Mr. Dyal,

It has recently come to the Tourist Development Council's attention that the Circle K located at the I-75 and Hwy 90 West interchange plans to expand their fueling station to service 18-wheeler cargo trucks. This is incredibly concerning to the Tourist Development Council, and we respectfully request a meeting to discuss these concerns.

A part of Tourist Development's mission is to strengthen the economy and quality-of-life for residents through visitor spending in our community. We are concerned how this new semi-trailer truck fueling station will negatively impact the quality of life for residents and visitors alike. The traffic at this intersection is already problematic and this project will only exacerbate the issue and alter the landscape of this exit for years to come.

The Tourist Development Council hopes we can meet to discuss these quality-of-life concerns to gain a better understanding of the project and if necessary, to find a solution for our community.

We look forward to hearing from you and thank you for your time.

Sincerely,

A handwritten signature in blue ink that reads "Rocky Ford". The signature is fluid and cursive.

Rocky Ford, Chairman, Columbia County Tourist Development Council

Cc: City of Lake City Council
Columbia County Tourist Development Council
Columbia County Board of County Commissioners
David Kraus, County Manager
Kevin Kirby, Assistant County Manager.

Humphries, Marty

From: Register, Troy
Sent: Monday, October 30, 2023 5:10 PM
To: rebecca.thigpen@circlek.com; witts@lcfcla.com; tjk@rkkattorneys.com
Cc: Dycus, Douglas; Humphries, Marty; Nieto, Carlos A
Subject: Commercial Access and Signal Connection at NW Centurion Court

Steven Witt, Mayor
City of Lake City

Todd Kennon, City Attorney
City of Lake City

Rebecca Thigpen
Central Construction Manager
Circle K Stores

To All,

In accordance with section 14-96, Florida Administrative Code (FAC), you are notified the Department intends to revoke Commercial Access and Signal Connection Permit No. 2015-A-292-0026 (NW Centurion Court) issued on April 18, 2016 to Gateway Crossing development and under subsequent jurisdiction of the City of Lake City; not renew Safety Upgrade Permit No. 2022-A-292-00008 (NW Centurion Court) issued on May 18, 2022 to Circle K that expires on November 20, 2023; and close the connection to the Gateway Crossing property per section 14-96.011(2), FAC.

The City did not contact the Department to determine if a new permit application and modification of existing connection is required. The City also failed to contact the Department to determine the need for connection modifications or to submit a new application for such modifications prior to initiation of property improvements, land use changes, or traffic flow alteration actions which constitute significant change. The planned construction at the site is significantly different from what was represented during the Department permitting process and there are significant safety concerns given the close proximity of the location to I-75 right of way.

If you have any questions or concerns, please contact me.


Sincerely,

Troy Register
Permits Manager
Lake City Operations
(386) 961-7153



MEMORANDUM

TO: Terrell K. Arline, Esquire
Ansbacher Law

FROM: Luis N. Serna, AICP 
Calvin, Giordano & Associates, Inc.

SUBJECT: Planning Analysis Regarding Objection to Site Plan Application
for GWC Development Partners, LLC, SR22-15.

DATE: December 18, 2023

I have reviewed the Objection to Site Plan Application regarding the above referenced permit for the construction of a tractor trailer fueling facility as an expansion of an existing Circle K gas station and convenience store. I also understand that a site plan for this project will be considered on January 10, and January 17, 2024 by the Planning and Zoning Board. Based on my review of the City's Land Development Regulations and other planning documents, it is my opinion that the permit for the facility does not accurately reflect the proposed use of the subject property, and therefore, the correct review and approval process was not followed for this permit.

The applicants are proposing in the permit and site plan the addition of three high flow diesel pump service bays and parking for up to six commercial tractor trailer rigs on a parcel that is adjacent to the existing Circle K. In my opinion, facilities for truck and tractor trailer fueling are clearly defined in the City's Land Development Regulations (LDRs) as a "truck stop" and are therefore, in accordance with Section 4.13.5, subject to the Special Exception review requirements of Article 12 of the LDRs. The fact that the tractor trailer fueling is located on a separate parcel than the existing Circle K further supports the conclusion that the proposed high speed diesel fueling facilities constitute a truck stop because, as a standalone parcel, the facility is only designed to accommodate tractor trailers as the primary use, rather than automobiles.

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“Truck Stop” is defined in Section 2.1 of the LDRs as follows:

A truck stop is an establishment where the principal use is primarily the refueling and servicing of trucks and tractor trailer rigs. Such establishments may have restaurants or snack bars and sleeping accommodations for the drivers of such over-the-road equipment and may provide facilities for the repair and maintenance of such equipment.

In contrast, Section 2.1 of the LDRs define an “Automotive Service Station” as follows:

An automotive service station is an establishment whose principal business is the dispensing at retail of motor fuel and oil primarily for automobiles; and where grease, batteries, tires, and automobile accessories may be supplied and dispensed at retail. In addition, an automotive service station may provide accessory facilities for car washing and polishing (but not commercial car wash facilities) and may render minor repair services. However, major mechanical and body work, straightening of frames or body parts, steam cleaning, painting, tire recapping or regrooving, storage of automobiles not in operating condition, or other work involving undue noise, glare, fumes, smoke, or other characteristics to an extent greater than normally found in such stations are prohibited. An automotive service station is not a repair garage, a body shop, truck stop, or a car wash or a combination thereof. For the purposes of these land development regulations, where motor fuel pumps are erected for the purpose of dispensing motor fuel at retail primarily for automobiles, such motor fuel pumps shall be considered to constitute an automotive service station, even where additional services which are customarily associated with an automotive service station are not provided. Where such motor fuel pumps are erected in conjunction with a use which is not an automotive service station, each use shall be considered as a separate principal use and as such, each must meet all applicable requirements of these land development regulations (see Article 4 for special design standards for automotive service stations).

Note that while this definition includes several uses that are considered accessory to automotive service stations, tractor trailer fueling is not included as an accessory use. In fact, truck stop is specifically excluded from the definition of an automotive service station. By including a separate definition of a truck stop, it is clear that the intent of the LDRs is that tractor trailer fueling is a separate use from an automotive service station.



Additionally, based on my experience, tractor trailer fueling as a retail use is never considered as an accessory use to an automobile service station and is typically classified as a primary use of a truck stop.

Columbia County, in which Lake City is located, includes a definition of automotive service station that is very similar to Lake City's and which also specifically states that truck stops are not included within this definition. Like Lake City, Columbia County includes a separate definition of truck stop that is identical to the City's.

In addition to Columbia County, I also consulted *A Planners Dictionary*, published by the American Planning Association (Planners Advisory Service Report Number 5xx/5xx, 2004) which provides examples of zoning definitions from throughout the United States. This publication provides five definitions of *Automobile Service Station*. In none of these examples are high speed diesel pumps that are designed solely for tractor trailer fueling specifically identified as a permitted or accessory use.

Conclusion

Based on my review of this Application, my opinion is that the proposed expansion of the Circle K by the addition of tractor trailer fueling on the adjacent parcel was not properly defined as a truck stop. Therefore, the City failed to properly process the Application as a Special Exception as required by the LDRs.

According to Section 2.1 of the LDRs, a "Special Exception" is defined as follows:

A special exception is a use that would not be appropriate generally or without restriction throughout a zoning district but which, if controlled as to number, area, location, or relation to the neighborhood, would promote the public health, safety, welfare, morals, order, comfort, convenience, appearance, prosperity, or the general welfare. Such uses may be permissible in a zoning district as a special exception if specific provision for such a special exception is made in these land development regulations. (For the procedure in securing special exceptions, see Article 12).

Without processing this as a Special Exception, the review staff did not provide an opportunity for the Planning and Zoning Board, the Board of Adjustment, and the public to review the potential impacts of this use and to address potential controls of the use on the number, area, location, or relation to the neighborhood.

Given the types of uses in the Gateway Crossings Subdivision, which include restaurants and a hotel, and the high level of automobile traffic within Gateway Crossings from these uses, there are concerns that an increase in the amount of tractor trailer traffic near the entrance of this subdivision will cause significant



adverse impacts to surrounding uses particularly from noise and traffic. Impacts to public roadways will extend beyond Gateway Crossings to also affect U.S. 90 and the offramp for Interstate 75. Without processing this application as a Special Exception, the City did not permit affected property owners and the public to adequately assess the full impacts of the proposed use and to address any potential mitigating conditions to address these impacts.



EXHIBIT G

BUCKHOLZ TRAFFIC
3585 KORI ROAD
JACKSONVILLE, FLORIDA 32257
(904) 886-2171 jwbuckholz@aol.com

February 28, 2023

Mr. Nick Patel, Chief Operating Officer
Lake City Hotels, Inc.
3696 West US Highway 90
Lake City, Florida 32024

Re: Technical Evaluation of March 2022 Circle K Traffic Impact Analysis; Lake City, Florida

Dear Mr. Patel:

Per your request I have reviewed the subject traffic study completed by Kimley-Horn and Associates, Inc. for the expansion of the Circle K gas station with convenience store located in the northeast quadrant of the US 90/Centurion Court intersection. I offer the following comments which are generally in priority order of importance.

1. In Section 4.1 of the report background traffic (future non-site traffic from other developments and general area growth) was calculated using a 2.1% annual growth rate which was calculated using historical daily counts taken on US 90. This approach ignores the development that is currently underway along Centurion Court right behind the Circle K (see Attachment A) where a 1227 sf Sonic Restaurant with dual drive thru windows and a 3428 sf Rib Crib Restaurant with drive-thru window have been approved. Vacant land also exists along Centurion Court behind Circle K which will eventually be used for a 110 room hotel and a 50,000 sf cold storage warehouse. Tables 1 and 2 show that the two restaurants will almost immediately add 103 trips to Centurion Court during the critical weekday PM peak hour while the hotel and cold storage will eventually add another 46 PM peak hour trips (see Tables 3 and 4) for a total of 149 missing peak hour trips. All of the intersection analyses fail to account for these expected trips.
2. In the Kimley-Horn report the calculated trip generation for the expanded site is reduced by 69% for the weekday AM peak hour and by 60% for the weekday PM peak hour. The apparent conjecture is that this is a relatively unpopular gas station based on existing traffic counts. One might derive such reduction factors by comparing the expected peak hour trip generation of the existing site based on ITE formulas to the actual driveway counts. However, this was not done. The trip generation was instead mistakenly compared to all of the trips using Centurion Court which includes trips associated with the existing Tru By Hilton hotel and Denny's restaurant that are located on this road ó resulting in an over count. However, it also fails to take into account the trips between the site and the existing hotel and restaurant ó resulting in an under count. The net result is unclear. In any event, if the popularity of this Circle K increases to just an average level the volume of traffic that it generates during the peak hours can be expected to more than double which will adversely affect all of the intersection evaluations that were conducted.
3. The Kimley-Horn report applies their reduction for pass-by traffic to vehicles using US 90; they do not make the reasonable assumption that a large portion of the pass-by traffic will be drawn from I-75. This results in projected site traffic volumes at the US 90/Centurion Court intersection that are too low. Correcting this mistake will also adversely affect the intersection analyses.

4. The Kimley-Horn Synchro analysis contains some input errors. A.) The 110 foot storage length for the southbound approach is incorrectly assigned to the right turn movement instead of the left turn movement. This error masks the deleterious queue effect on this approach. B.) Incorrect truck percentages are used. For example, the percentage of trucks for the northbound left turn movement during the weekday PM peak hour is 5.3% (see page 11 of Appendix B of the KH report) whereas in Synchro it is coded as only 3% (see page 3 of Appendix D of the KH report).
5. New diesel pumps are being installed at Circle K and the radius on the northeast corner of the US 90/Centurion Court intersection is being increased to service large trucks. However, the percentage of trucks reflected in Kimley-Horn's 2023 Build analysis is no different than the percentage in their 2021 existing analysis. One would expect it to increase for both the southbound left turn movement and the westbound right turn movement.
6. Table 5 in the Kimley-Horn report lists an expected 95th percentile queue length under expected 2023 Build conditions in the left turn lane on the Centurion Court approach to US 90 to be 6.8 vehicles (round to 7). At 25 feet of required storage per vehicle queued this produces a queue of 175 feet -which extends to the Denny's driveway and blocks access to the 110 foot long thru/right turn lane on Centurion Court. If we make the traffic volume corrections discussed in this letter we can expect this queue to be considerably longer.
7. Synchro is privately developed "black box" software which does not always faithfully reproduce the results obtained with the more generally accepted and publicly developed Highway Capacity Software (HCS). Running the PM peak hour intersection analysis using the latest HCS software (but still using the artificially low BUILD traffic volumes contained in the Kimley-Horn report) can produce dramatically different queue, delay, and level of service. For example, the HCS identifies level of service F conditions for the northbound left turn lane (not level of service E) with much higher delay and with a 95th percentile queue of 5.7 vehicles (not 3.1 vehicles). This queue exceeds the available storage length by a factor of almost three, blocking access to the adjacent thru/right turn lane.

It is my professional opinion that the Kimley-Horn traffic study is both incomplete and inaccurate and that the recommendations contained in the report do not ensure safe and efficient access to the proposed site. One can envision a series of improvements that would help traffic flow in the area, including the provision of side street left turn arrows for the traffic signal at the US 90/Centurion Court/Florida Gateway Drive intersection, extension of the existing thru/right turn lane on Centurion Court, extension of the existing left turn lane on Florida Gateway Drive, the addition of a third approach lane on Centurion Court, and the lengthening of the westbound right turn lane on US 90.

Please contact me if you have any questions concerning this matter.

Sincerely,

Jeffrey W. Buckholz, PhD, P.E., PTOE
President

This item has been digitally signed and sealed by Jeffrey W. Buckholz, P.E. on the date adjacent to the seal. Printed copies of this document are not considered signed and sealed and the signature must be verified on any electronic copies.

TABLE 1

TRIP GENERATION CALCULATIONS

HIGH TURNOVER SIT DOWN RESTAURANT

Land Use Code 932

T = Number of Vehicle Trip Ends

Size of Building = 3428 (X = 3.428)

<u>TIME PERIOD</u>	<u>TOTAL</u> <u>TRIP GENERATION</u> <u>EQUATION</u>	<u>TOTAL</u> <u>TRIP</u> <u>ENDS</u>	<u>PERCENT</u> <u>ENTERING</u>	<u>PERCENT</u> <u>EXITING</u>	<u>TOTAL</u> <u>TRIP ENDS</u> <u>ENTERING</u>	<u>TOTAL</u> <u>TRIP ENDS</u> <u>EXITING</u>
AVERAGE WEEKDAY						
Daily	T = 107.2 (X)	352	50%	50%	176	176
AM Peak Hour	T = 9.57 (X)	33	55%	45%	18	15
PM Peak Hour	T = 9.05 (X)	31	61%	39%	19	12

SOURCE: Institute of Transportation Engineers, "Trip Generation", 11th Edition (2021)

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TABLE 2

TRIP GENERATION CALCULATIONS

FAST-FOOD RESTAURANT WITH DRIVE-THRU WINDOW AND NO INDOOR SEATING

Land Use Code 935

T = Number of Vehicle Trip Ends

Size of Building = 1227 (X = 1.227)

<u>TIME PERIOD</u>	<u>TOTAL TRIP GENERATION EQUATION</u>	<u>TOTAL TRIP ENDS</u>	<u>PERCENT ENTERING</u>	<u>PERCENT EXITING</u>	<u>TOTAL TRIP ENDS ENTERING</u>	<u>TOTAL TRIP ENDS EXITING</u>
AVERAGE WEEKDAY						
Daily	T = 467.48 (X)	574	50%	50%	287	287
AM Peak Hour	T = 43.00 (X)	53	47%	53%	25	28
PM Peak Hour	T = 59.50 (X)	72	51%	49%	37	35

SOURCE: Institute of Transportation Engineers, "Trip Generation", 11th Edition (2021)

Estimated Using LUC 934

<u>TIME PERIOD</u>	<u>PERCENT NEW TRIPS</u>	<u>NEW TRIP ENDS</u>	<u>PERCENT ENTERING</u>	<u>PERCENT EXITING</u>	<u>NEW TRIP ENDS ENTERING</u>	<u>NEW TRIP ENDS EXITING</u>
AVERAGE WEEKDAY						
Daily	69%	396	50%	50%	198	198
AM Peak Hour	69%	37	51%	49%	19	18
PM Peak Hour	69%	50	52%	48%	26	24

SOURCE: Institute of Transportation Engineers, "Trip Generation", 11th Edition (2021). Excel Tab1

BUCKHOLZ TRAFFIC

TABLE 3
TRIP GENERATION CALCULATIONS

BUSINESS HOTEL

Land Use Code 312

T = Number of Vehicle Trip Ends

X = Rooms = 110

<u>TIME PERIOD</u>	<u>TOTAL TRIP GENERATION EQUATION</u>	<u>TOTAL TRIP ENDS</u>	<u>PERCENT ENTERING</u>	<u>PERCENT EXITING</u>	<u>TOTAL TRIP ENDS ENTERING</u>	<u>TOTAL TRIP ENDS EXITING</u>
AVERAGE WEEKDAY						
Daily	$T = 2.90 (X) + 151.69$	470	50%	50%	235	235
AM Peak Hour	$T = 0.30 (X) + 6.94$	40	39%	61%	16	24
PM Peak Hour	$T = 0.21 (X) + 12.03$	35	55%	45%	19	16

SOURCE: Institute of Transportation Engineers, "Trip Generation", 11th Edition (2021)

BUCKHOLZ TRAFFIC

TABLE 4

TRIP GENERATION CALCULATIONS

HIGH-CUBE COLD STORAGE WAREHOUSE (SMALL SIZE)

Land Use Code 157

T = Number of Vehicle Trip Ends

Size of Building = 50,000 gsf (X = 50.0)

<u>TIME PERIOD</u>	<u>TOTAL TRIP GENERATION EQUATION</u>	<u>TOTAL TRIP ENDS</u>	<u>PERCENT ENTERING</u>	<u>PERCENT EXITING</u>	<u>TOTAL TRIP ENDS ENTERING</u>	<u>TOTAL TRIP ENDS EXITING</u>
AVERAGE WEEKDAY						
Daily	T = 2.12 (X)	102	50%	50%	51	51
AM Peak Hour	T = 0.11 (X)	10	77%	23%	8	2
PM Peak Hour	T = 0.12 (X)	11	28%	72%	3	8

From LUC 154

SOURCE: Institute of Transportation Engineers, "Trip Generation", 11th Edition (2021)

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