# PLANNING AND ZONING BOARD MEETING

**CITY OF LAKE CITY** 

July 06, 2022 at 5:30 PM Venue: City Hall

# AGENDA

The meeting will be held in the City Council Chambers on the second floor of City Hall located at 205 North Marion Avenue, Lake City, FL 32055. Members of the public may also view the meeting on our YouTube channel. YouTube channel information is located at the end of this agenda.

#### INVOCATION

#### **ROLL CALL**

#### MINUTES

i. June 7, 2022

#### **OLD BUSINESS** - None

#### **NEW BUSINESS**

- ii. Site Plan Review SPR-22-15 Daniel Hotte of GWC Development Partners, LLC (Agent: Jarod Stubbs, P.E.) Circle K on US 90
- iii. Site Plan Review SPR-22-14 Parker Neely (Agent: Chris Potts, P.E.) Sonic Drive In

#### WORKSHOP - None

#### ADJOURNMENT

#### YouTube Channel Information

Members of the public may also view the meeting on our YouTube channel at: https://www.youtube.com/c/CityofLakeCity

Pursuant to 286.0105, Florida Statutes, the City hereby advises the public if a person decides to appeal any decision made by the City Council with respect to any matter considered at its meeting or hearings, he or she will need a record of the proceedings, and that, for such purpose, he or she may need to ensure that a verbatim record of the proceedings is made, which record includes the testimony and evidence upon which the appeal is to be based.

Pursuant to 286.26, Florida Statutes, persons needing special accommodations to participate in this meeting should contact the City Manager's Office at (386) 719-5768.

#### File Attachments for Item:

i. June 7, 2022

#### Meeting Minutes Planning and Zoning

Date: 06/07/2022

**Roll Call:** 

Mr. Lydick-Present Mr. Cooper-Present Mr. Nelson-Present Ms. Georgalis-Present Mr. Carter-Present Mrs. McKellum-Present Mr. McMahon-Present

Approval of Past Minutes-Approve the minutes of the 05/03/2022 Meeting. Motion By: Mr. Lydick Seconded By: Mr. McMahon

#### **Comments or Revisions**:

Mr. Lydick motioned to move the invocation from the historic preservation meeting to the planning and zoning meeting.

#### **Old Business: None**

**New Business:** 

Petition # CPA 22-04 and Z22-03 Presented By: Daniel Crapps As owner or agent and gives address of: 2806 W Hwy 90 Lake City FL 32025

#### Petitioner is Sworn in by: Ms.Georgalis

#### **Discussion:**

Mr. Crapps talked about the need for housing in the area and that there is a housing shortage in Lake City. Mr. Angelo asked if we were talking about the same property because Mr. Crapps said that the property was 19 acres and Mr. Angelo called out that the application said 1.63 acres. Mr. Crapps confirmed that this was the right property. Mr. Lydick asked if lots 47 and 48 were merged. Mr. Crapps said that on the property appraisers web site they were but not legally yet. **Approved unanimously**.

Motion to close Public Hearing: Mr. Lydick Motion Seconded By: Mr. Carter

Motion to Approve/Deny By: Mr. Nelson Motion Seconded By: Mr. Carter

Petition # CPA22-05 and Z22-04 Presented by Isaac Schlimmer As owner or agent and gives address of: 187 Old Cypress Way Lake City FL 32024

Petitioner is Sworn in by: Ms. Georgalis

#### Discussion:

Mr. Schlimmer is proposing to build a 12-unit multi housing with 1000 square feet per unit. He talked about the need for housing in the area. Mr. Lydick asked if the current RMF-1 parcels in the area were being used for multifamily of single family. Mr. Schlimmer did not know. Mrs. McKellum asked if the were any other apartments in the area. Mr. Schlimmer did not know. Barbara Carrington of 1097 Tustenuggee said that her parents live there and when they bought the property it was all single family and that is what the people there want. She and other put a petition together to not have this rezoned because of drainage issues and increase in traffic and noise. Mr. Cooper said that he did not agree with multifamily being in the middle of single family but di degree that we need more housing. Kirsinda Byrd of 905 NW Folwer Said that she and her family lived two parcels down from this property and that currently the neighborhood is quiet and they want to keep it that way. Mr. Lydick asked about a possible zoning island but also mentioned that there is multifamily zoning in the area. Ms. Georgalis said that they have to take in to count the possible drainage issues and the change in the life style in the area that this would bring. **Petition passed with a vote of 4 yes to 3 no. Motion to close Public Hearing: Mr. McMahon** 

Motion Seconded By: Mr. Lydick

Motion to Approve/Deny By: Mr. Nelson Motion Seconded By: Mr. Lydick

#### Petition # SPR22-13 Presented By: Carol Chadwick

As owner or agent and gives address of: 1208 SW Fairfax Glen Lake City FL 32025

#### Discussion:

Carol Chadwick presented the site plan for Frank and Lane's Heating and Air. Mr. Lydick asked why only one tree was being saved. Ms. Chadwick said that as many trees as possible were going to be saved. **Approved unanimously.** 

Motion to close Public Hearing: Mr. Carter Motion Seconded By: Mr. Lydick

Motion to Approve/Deny By: Mr. Carter Motion Seconded By: Mr. Nelson

#### Petition # V22-01, SE22-01, and SPR22-02 Presented By: Brian Pitman As owner or agent and gives address of: 206 S Marion Ave Lake City FL 32025

#### **Discussion:**

Mr. Pitman presented the site plan and the need for the variance and special exception. Mr. Lydick asked why we were hearing a variance is the planning and zoning meeting. Mr. Angelo said that was being heard concurrently with the special exception due to the fact that if the variance was not approved the others would not be approved either. Brian said that the reason for the variance was due to the seven-foot encroachment of the setback. Mr. McMahon asked if the lot was clear. Mr. Pitman said that it was not. Mr. Nelson asked what was behind the property if it was residential. Mr. Pitman said that id was a large retention pond used for the commercial properties around it. All three petitions were approved unanimously.

Motion to close Public Hearing: Mr. Carter Motion Seconded By: Mr. Nelson

Motion to Approve/Deny By: Mr. Lydick Motion Seconded By: Mr. Nelson

Motion to Adjourn by: Mr. Lydick Time: 6:12 Motion Seconded By: Mr. Carter

Mavis Georgalis, Board Chairperson

Date Approved

**Robert Angelo, Secretary** 

Date Approved

#### File Attachments for Item:

ii. Site Plan Review - SPR-22-15 - Daniel Hotte of GWC Development Partners, LLC (Agent: Jarod Stubbs, P.E.) Circle K on US 90



GROWTH MANAGEMENT 205 North Marion Ave. Lake City, FL 32055 Telephone: (386)719-5750 E-Mail: growthmanagement@lcfla.com

A 18	
FOR P	LANNING USE ONLY
Applic	cation # <u>SPR22-15</u>
Applic	cation Fee: <u>\$200.00</u>
Receij	ptNo
Filing	Date 4/1/22
Comp	leteness Date
-	

# **Site Plan Application**

#### A. PROJECT INFORMATION

- 1. Project Name: <u>CIRCLE K US 90 & I-75</u>
- 2. Address of Subject Property: <u>143 NW Centurion Ct., Lake City, FL 32055</u>
- 3. Parcel ID Number(s): 35-35-16-02524-001, 35-35-16-02524-102, 35-35-16-02524-111
- 4. Future Land Use Map Designation: Commercial
- 5. Zoning Designation: CHI Commercial Highway Interchange
- 6. Acreage: ±3.46
- 7. Existing Use of Property: Existing Circle K gas station and convenience store
- 8. Proposed use of Property: Circle K gas station and high speed diesel station
- 9. Type of Development (Check All That Apply):
  - [X] Increase of floor area to an existing structure: Total increase of square footage <u>±652 SF</u>
  - [X] New construction: Total square footage <u>±54,470 SF</u>
    - Relocation of an existing structure: Total square footage \_\_\_\_

#### **B. APPLICANT INFORMATION**

- 1. Applicant Status 🗆 Owner (title holder)
- Name of Applicant(s): Jarod Stubbs P.E. Company name (if applicable): <u>Kimley-Horn</u>

Mailing Address: 189 S. Orange Ave, Suite 1000

City: Orlando		_State: FL	Zip: <u>32801</u>
Talambana ( 407) 400 7007	Fourt	)	Empilizered stubbe@kimley horn com

Telephone: <u>(407) 409-7002</u> Fax: <u>Email: Jarod.stubbs@kimley-horn.com</u> PLEASE NOTE: Florida has a very broad public records law. Most written communications to or from government officials regarding government business is subject to public records requests. Your e-mail address and communications may be subject to public disclosure.

X Agent

Title: <u>Civil Engineer</u>

3. If the applicant is agent for the property owner\*.

Property Owner Name (title holder): <u>Daniel Hotte of GWC Development Partners, LLC</u>	
Mailing Address: 2682 W Noegel Rd	

City: Lake City	State: <u>FL</u>	Zip: <u>32055</u>

#### C. ADDITIONAL INFORMATION

 Is there any additional contract for the sale of, or options to purchase, the subject property? If yes, list the names of all parties involved:
 If yes is the contract /option contingent or absolute:

	in yes, is the conduct option condingeneor abcorator a condingent and bolato
2.	Has a previous application been made on all or part of the subject property? □Yes X No
	Future Land Use Map Amendment:
	Future Land Use Map Amendment Application No.
	Site Specific Amendment to the Official Zoning Atlas (Rezoning): □Yes□No
	Site Specific Amendment to the Official Zoning Atlas (Rezoning) Application No.
	Variance: 🗆 Yes
	Variance Application No
	Special Exception:  □Yes □No
	Special Exception Application No

#### D. ATTACHMENT/SUBMITTAL REQUIREMENTS

1. Vicinity Map – Indicating general location of the site, abutting streets, existing utilities, complete legal description of the property in question, and adjacent land use.

#### 2. Site Plan - Including, but not limited to the following:

- a. Name, location, owner, and designer of the proposed development.
- b. Present zoning for subject site.
- Location of the site in relation to surrounding properties, including the means of ingress and egress to such properties and any screening or buffers on such properties.
- X. Date, north arrow, and graphic scale not less than one inch equal to 50 feet.
- e. Area and dimensions of site (Survey).
- f Location of all property lines, existing right-of-way approaches, sidewalks, curbs, and gutters.
- Access to utilities and points of utility hook-up.
- b. Location and dimensions of all existing and proposed parking areas and loading areas.
- Location, size, and design of proposed landscaped areas (including existing trees and required landscaped buffer areas).
- ★ Location and size of any lakes, ponds, canals, or other waters and waterways.
- Structures and major features fully dimensioned including setbacks, distances between structures, floor area, width of driveways, parking spaces, property or lot lines, and percent of property covered by structures.
- $\mathcal{V}$  Location of trash receptacles.
- m. For multiple-family, hotel, motel, and mobile home park site plans:
  - i. Tabulation of gross acreage.
  - ii. Tabulation of density.
  - iii. Number of dwelling units proposed.
  - iv. Location and percent of total open space and recreation areas.
  - v. Percent of lot covered by buildings.

City of Lake City – Growth Management Department 205 North Marion Ave, Lake City, FL 32055 ♦ (386) 719-5750

- vi. Floor area of dwelling units.
- vii. Number of proposed parking spaces.
- viii. Street layout.
- ix. Layout of mobile home stands (for mobile home parks only).
- 8. Stormwater Management Plan—Including the following:
  - a. Existing contours at one foot intervals based on U.S. Coast and Geodetic Datum.
  - b. Proposed finished elevation of each building site and first floor level.
  - c. Existing and proposed stormwater management facilities with size and grades.
  - d. Proposed orderly disposal of surface water runoff.
  - e. Centerline elevations along adjacent streets.
  - f. Water management district surface water management permit.

Fire Department Access and Water Supply Plan: The Fire Department Access and Water Supply Plan must demonstrate compliance with Chapter 18 of the Florida Fire Prevention Code, be located on a separate signed and sealed plan sheet, and must be prepared by a professional fire engineer licensed in the State of Florida. The Fire Department Access and Water Supply Plan must contain fire flow calculations in accordance with the Guide for Determination of Required Fire Flow, latest edition, as published by the Insurance Service Office ("ISO") and/or Chapter 18, Section 18.4 of the Florida Fire Prevention Code, whichever is greater.

- 5. Concurrency Impact Analysis: Concurrency Impact Analysis of impacts to public facilities. For commercial and industrial developments, an analysis of the impacts to Transportation, Potable Water, Sanitary Sewer, and Solid Waste impacts are required.
- 6. Comprehensive Plan Consistency Analysis: An analysis of the application's consistency with the Comprehensive Plan (analysis must identify specific Goals, Objectives, and Policies of the Comprehensive Plan and detail how the application complies with said Goals, Objectives, and Policies).

Legal Description with Tax Parcel Number (In Word Format).

8. Proof of Ownership (i.e. deed).

- 9. Agent Authorization Form (signed and notarized).
- 10. Proof of Payment of Taxes (can be obtained online via the Columbia County Tax Collector's Office).
- 1. Fee. The application fee for a Site and Development Plan Application is \$200.00. No application shall be accepted or processed until the required application fee has been paid.

City of Lake City – Growth Management Department 205 North Marion Ave, Lake City, FL 32055 ♦ (386) 719-5750

#### NOTICE TO APPLICANT

All eleven (11) attachments are required for a complete application. Once an application is submitted and paid for, a completeness review will be done to ensure all the requirements for a complete application have been met. If there are any deficiencies, the applicant will be notified in writing. If an application is deemed to be incomplete, it may cause a delay in the scheduling of the application before the Planning & Zoning Board.

A total of ten (10) copies of proposed site plan application and all support materials must be submitted along with a PDF copy on a CD. See City of Lake City submittal guidelines for additional submittal requirements.

THE APPLICANT ACKNOWLEDGES THAT THE APPLICANT OR AGENT MUST BE PRESENT AT THE PUBLIC HEARING BEFORETHE PLANNING AND ZONING BOARD, AS ADOPTED IN THE BOARD RULES AND PROCEDURES, OTHERWISE THE REQUEST MAY BE CONTINUED TO A FUTURE HEARING DATE.

I hereby certify that all of the above statements and statements contained in any documents or plans submitted herewith are true and accurate to the best of my knowledge and belief.

Applicant/Agent Name (Type or Print)

Applicant/Agent Signature

Applicant/Agent Name (Type or Print)

Applicant/Agent Signature

STATE OF FLORIDA COUNTY OF Orange

The foregoing instrument was acknowledged before me this  $\frac{8^{11}}{20}$  day of  $\frac{5}{20}$  by (name of person acknowledging).



Personally Known \_\_\_\_\_ OR Produced Identification \_\_\_\_\_ Type of Identification Produced

> City of Lake City – Growth Management Department 205 North Marion Ave, Lake City, FL 32055 ♦ (386) 719-5750

Date

Date

REVIEW DATE	LAND DESCR USE 1 1410 COPE	EXTRA FEAT	AREA BAS 5,265 CAN 7,360 TOTALS 12,62	Stories Units Condition Adj Quality DOR CODE MAP NUM NI RHOOD NI RHOOD TOTAL	LOT 1 GATEWA WD 1339-654, ELEMENT Exterior Wall Roof Structur Roof Structur Interior Wall Interior Wall Interior Wall Interior Wall Air Condition Heating Type Fixtures Frame
11/01/2018	IPTION LAND USE DESCRIPTION IV STORE	JRES CRIPTION BLD CA MENT-A 0 , PAVMT 0 TING 0 BIN 0 E/WOOD 0	BASE 5,26 30 2,20 7,47	0 100 1. 1. 100 04 04 100 08 08 08 08 1126 CONV STOI 1126 CONV STOI 1126 CONV STOI 967 MRT AREA 967 AREAD	Y CROSSING CD CONSTRUCTION CONSTRUCTION 17 MSNRY STUC 21 STONE 10 04 BUILT-UP 04 BUILT-UP 05 ENG FAIR 09 ENG FAIR 09 ENG FAIR 12 100 12 100
ВҮ МЕВС			value 5 497,938 8 208,822 3 706,759	RE/GAS A SUBARET MARKET	S/D. FRUCTION 2 90 100 100 100 100 100 100 100 00
Total Acres: 1.98	FRONT DEPTH 0.00 0.00	143 NW CENTURIO UNITS UT 16,400.00 UT 1.6 1,300.00 UT 2.2 10.00 UT 1.5 308.00 UT 11. 74.00 UT 15.			ASPRI INVEST PROPERTY TAX PHOENIX, AZ 1300 04 7,47
Total Land V	LINDUTS UNIT 1799 86,206.00 SF	NN Ct, LAKE CIT 0 0 5 5 00.00 1,5 50 50			15 15 15 15 15 16 16 16 16 16 16 16 16 16 16
alue: 1,239,211		Y         NC DATE FUNIT         Office         Y           RICE         COMD         100         2           2.25         100         2         100           11.60         100         100         2           15.50         100         100         2			DX 52085 MARKET A 97.50 REL 0 45
Mark	0 1.25	Image: Peak Peak Actual 2017         YEAR 2017         Control 2017 <thc< td=""><td></td><td></td><td>28, 618 2017</td></thc<>			28, 618 2017
et: 0	103,20 ce PRCE	AG DATE S. OBX# WICT 100 58,24 100 25,42 100 15,00 100 3,38 100 1,14			2017 ECON FRO
Agricultural: 0	14.38 1,239,21:	-71 8 0 5 0 NOTES	<u> </u>		2022 TT NORM % COND 0 3.00 97.00 HX Base Yr
Common: 1	OTHER ADJUSTMENTS AND NOTES	BAS= W36 N3 W45 S3 W3 E184 N40\$ W35 N50\$ N45	OFF RECORD Number 1339/0654 GRANTOR: GWC DEVELOP GRANTEE: ASPRI INVES	PERMIT NUM DESC	COLUMBIA COUNTY VALUATION BY Tax Group: 1 BUILDIN MARKET VALUE TOTAL MARKET VALUE TOTAL LAND VALUE - MARI TOTAL LAND VALUE - MARI TOTAL LAND VALUE - MARI SOHIAGL Deduction SOHIAGL DEduction SOHIAGL DEDUCTOR SOHIAGL DEDUCTOR SOHIAGL DEDUCTOR SOHIAGL DEDUCTOR SOHIAGL DEDUCTOR SOHIAGL DEDUCTOR SOHIAGL DEDUCTOR SOHIAGL DEDUCTOR SOHIAGL DEDUCTOR TOTAL EXEMPTION VALUE TOTAL LEXEMPTION VALUE TOTAL LEXEMPTION VALUE
,239,211	YEAR DENSITY DE	JILDING DIMENSIO 3 845 E114 PTR-SSO E \$.	SALES DATA TYPE Q / V / R 2017 WD Q I C MENT PARTN MENT PARTN MENT S L	RIPTION AM	PROPERTY PAG
PRINTED 02/10/2022	SL FRZ YR CONSRV	NS 35 CAN= W184 S40	NN SALE D PRICE 11 2,487,200	r	-02524-001 = 1 of 1 2 1 \$

# CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION

# LEGAL DESCRIPTION

COLUMBIA COUNTY, FLORID

OT 2 AND THE NORTH 34.55 FEET OF LOT 11 OF GATEWAY CROSSING A REPLAT OF LOTS 2 & 3. ACCORDING TO PLAT THEREOF RECORDED IN PLAT BOOK 9. PAGE 151. PUBLIC RECORDS

# UTILITY PROVIDERS

WATER/SEWER: CITY OF LAKE CITY UTILITIES 692 SW SAINT MARGARETS ST LAKE CITY, FL 32025 CONTACT: PHONE:

ELECTRIC : FLORIDA POWER & LIGHT 2618 NE BASCOM NORRIS DRIVE LAKE CITY, FL 32055 CONTACT: SHANE EUBANK PHONE: (386) 754-2020

FIBER OPTIC : HARGRAY OF FLORIDA, INC. 8324 BAYMEADOWS WAY, STE. 102 JACKSONVILLE, FL 32256 CONTACT: EDWARD HARDING PHONE: (904) 652-9934

### CABLE:

COMCAST CABLE 5934 RICHARD STREET JACKSONVILLE, FL 32216 CONTACT: ANDREW SWEENEY PHONE: (904) 738-6898

**TELEPHONE:** 

AT&T 6628 LAKESIDE ROAD WEST PALM BEACH, FL 33411 CONTACT: DINO FARRUGGIO EMAIL: G27896@ATT.COM PHONE: (561) 683-2729

GAS :

CITY OF LAKE CITY GAS/PUBLIC WORKS 180 NE GUM SWAMP ROAD LAKE CITY, FL 32055 CONTACT: THOMAS HENRY EMAIL: HENRYT@LCFLA.COM PHONE: (386) 758-5425

OWNER:

**DEVELOPER:** CIRCLE K STORES, INC 3802 CORPOREX PARK DRIVE, SUITE 413 TAMPA, FL 33619 CONTACT: EDWARD GIUNTA PHONE: (407) 580-5173

# **CONSTRUCTION PLANS** FOR

**143 NW CENTURION COURT** LAKE CITY, FLORIDA 32055 MAY 4, 2022

PARCEL IDs: 35-3S-16-02524-001,

35-3S-16-02524-102 AND 35-3S-16-02524-111



# VICINITY MAP

**PROJECT TEAM** 

**CIVIL ENGINEER:** 

KIMLEY-HORN AND ASSOCIATES, INC. 189 SOUTH ORANGE AVENUE, SUITE 1000 ORLANDO, FL 32801 CONTACT: JAROD C. STUBBS, P.E. PHONE: (407) 409-7002 EMAIL: JAROD.STUBBS@KIMLEY-HORN.COM

ARCHITECT: RDC COLLABORATIVE 11921 FREEDOM DRIVE, SUITE #1110 **RESTON**, VA 20190 CONTACT: MEGAN LARGENT PHONE: (703) 668-0086 FAX: (703) 668-0085

# PREPARED BY © 2022 KIMLEY-HORN AND ASSOCIATES, INC.

189 S. ORANGE AVE, SUITE 1000, ORLANDO, FL 32801 PHONE: 407-898-1511 WWW.KIMLEY-HORN.COM REGISTRY No. 35106

1" = 500'

SURVEYOR:

JBPRO 3530 NW 43RD STREET GAINESVILLE, FL 32606 CONTACT: TROY V. WRIGHT PHONE: (352) 375-8999

LANDSCAPE ARCHITECT:

KIMLEY-HORN AND ASSOCIATES, INC. 189 SOUTH ORANGE AVENUE, SUITE 1000 ORLANDO, FL 32801 CONTACT: MATTHEW FRANKO PHONE: (407) 427-1629 EMAIL: MATT.FRANKO@KIMLEY-HORN.COM

GWC DEVELOPMENT PARTNERS LLC 2682 W NOEGEL ROAD LAKE CITY, FL 32055 CONTACT: DIANE BERRY PHONE: (407) 580-5173 EMAIL: DBERRY@SCHAFFERCONST.COM Project Location Lake City, FL

# SHEET INDEX

C0.0	COVER SHEET
C1.0-C1.1	GENERAL NOTES
C2.0	STORMWATER POLLUTION PREVENTION PLAN
C3.0-C3.1	EXISTING CONDITIONS & DEMOLITION PLAN
C4.0	OVERALL SITE PLAN
C4.1	SITE PLAN
C4.2	INTERSECTION MODIFICATION PLAN
C4.3-C4.5	TRUCK TURNING MOVEMENTS
C5.0	PAVING, GRADING AND DRAINAGE PLAN
C6.0	UTILITY PLAN
C7.0-C7.1	GENERAL CONSTRUCTION DETAILS
L1.00	LANDSCAPE PLAN
L1.50	LANDSCAPE DETAILS
L1.51	LANDSCAPE SPECIFICATIONS
L2.00	SCHEMATIC IRRIGATION PLAN
L2.50	IRRIGATION DETAILS
L2.51	IRRIGATION NOTES





	GENERAL
1. LOCAT	IONS, ELEVATIONS, AND DIMENSIONS OF EXISTING UTILITIES, STRUCTURES AND OTHER FEATURES ARE SHOWN
ACCORDIN	IG TO THE BEST INFORMATION AVAILABLE AT THE TIME OF PREPARATION OF THESE PLANS. THE EXISTING UTILITY
INFORMA <sup>-</sup>	TION SHOWN IS BASED ON THE TOPOGRAPHIC SURVEY PROVIDED BY ALTAMAX SURVEYING. THE CONTRACTOR SHALL
VERIFY T	HE LOCATIONS, ELEVATIONS, AND DIMENSIONS OF ALL EXISTING UTILITIES, STRUCTURES AND OTHER FEATURES,
AFFECTIN	G THIS AREA PRIOR TO CONSTRUCTION WORK.
2. PRIOR	TO THE INITIATION OF SITE CONSTRUCTION, THE CONTRACTOR SHALL VERIFY ANY EXISTING UTILITIES INCLUDING GA
WATER, E	LECTRIC, CABLE TV, COMMUNICATIONS, SANITARY SEWERS AND STORM DRAINAGE SYSTEMS, ON AND / OR ADJA
TO THE S	SITE. REMOVE OR CAP AS NECESSARY.
3. THE C 1—800—4	ONTRACTOR SHALL EXERCISE CAUTION IN AREAS OF BURIED UTILITIES AND SHALL CALL "SUNSHINE" AT 32–4770 AT LEAST 48 HOURS PRIOR TO CONSTRUCTION TO ARRANGE FOR FIELD LOCATIONS OF BURIED UTILITIE:
4. THE C	ONTRACTOR IS RESPONSIBLE FOR REPAIRING ANY DAMAGE TO EXISTING FACILITIES, ABOVE OR BELOW GROUND, TI
MAY OCC	UR AS A RESULT OF THE WORK PERFORMED, BY THE CONTRACTOR OR SUB-CONTRACTORS, AS CALLED FOR IN
THESE C(	ONTRACT DOCUMENTS.
5. IT IS <sup>-</sup>	THE CONTRACTOR'S RESPONSIBILITY TO BECOME FAMILIAR WITH THE PERMIT AND INSPECTION REQUIREMENTS SPEC
BY THE <sup>v</sup>	/ARIOUS GOVERNMENTAL AGENCIES AND THE ENGINEER. THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERM
PRIOR TC	O CONSTRUCTION, AND SCHEDULE INSPECTIONS ACCORDING TO AGENCY INSTRUCTION/REQUIREMENTS.
6. THE C	ONTRACTOR SHALL SUBMIT SHOP DRAWINGS, ON ALL PRECAST AND MANUFACTURED ITEMS, TO THE OWNER'S
ENGINEER	FOR REVIEW. FAILURE TO OBTAIN APPROVAL BEFORE INSTALLATION MAY RESULT IN REMOVAL AND REPLACEMI
AT THE (	CONTRACTOR'S EXPENSE.
7. ALL U	TILITY SERVICE STUB-OUTS (WATER, SANITARY SEWER, etc.) ARE TO BE INSTALLED WITHIN 5' OF THE POINT ON
CONNECT	ION TO THE BUILDING(S), UNLESS OTHERWISE NOTED ON PLANS.
8. CONTR	ACTOR TO COORDINATE WITH THE APPLICABLE ELECTRIC UTILITY SUPPLIER REGARDING ANY NECESSARY
RELOCATI	ON(S) OF UNDERGROUND AND/OR OVERHEAD ELECTRIC FACILITIES, AND FOR THE LOCATION AND INSTALLATION O
TRANSFO	RMER PAD(S) AND ASSOCIATED ELECTRIC FACILITIES.
9. SAFE A. DL THE	TY: JRING THE CONSTRUCTION AND/OR MAINTENANCE OF THIS PROJECT, ALL SAFETY REGULATIONS ARE TO BE ENFOR CONTRACTOR OR HIS REPRESENTATIVE SHALL BE RESPONSIBLE FOR THE CONTROL AND SAFETY OF THE TRAVELIN IC AND THE SAFETY OF HIS (HER PERSONNEL
B. LA	BOR SAFETY REGULATIONS SHALL CONFORM TO THE PROVISIONS SET FORTH BY OSHA.
C. TH	IE MINIMUM STANDARDS AS SET FORTH IN THE CURRENT EDITION OF "THE STATE OF FLORIDA, MANUAL ON TRAFF
CONT	ROL AND SAFE PRACTICES FOR STREET AND HIGHWAY CONSTRUCTION, MAINTENANCE AND UTILITY OPERATIONS" S
BE FI	DLLOWED IN THE DESIGN, APPLICATION, INSTALLATION, MAINTENANCE AND REMOVAL OF ALL TRAFFIC CONTROL DEV
WARN	ING DEVICES AND BARRIERS NECESSARY TO PROTECT THE PUBLIC AND CONSTRUCTION PERSONNEL FROM HAZAI
WITHI D. AL UNIF(	N THE PROJECT LIMITS. L TRAFFIC CONTROL MARKINGS AND DEVICES SHALL CONFORM TO THE PROVISIONS SET FORTH IN THE MANUAL ( )RM TRAFFIC CONTROL DEVICES PREPARED BY THE U.S. DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY NISTRATION.
E. AL	L SUBSURFACE CONSTRUCTION SHALL COMPLY WITH THE "TRENCH SAFETY ACT". THE CONTRACTOR SHALL INSUR
THAT	THE METHOD OF TRENCH PROTECTION AND CONSTRUCTION IS IN COMPLIANCE WITH THE OCCUPATIONAL SAFETY
HEAL	TH ADMINISTRATION (OSHA) REGULATIONS.
F. IT	SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR TO COMPLY AND ENFORCE ALL APPLICABLE SAFETY
REGU	LATIONS. THE ABOVE INFORMATION HAS BEEN PROVIDED FOR THE CONTRACTOR'S INFORMATION ONLY AND DOES
IMPLN	'THAT THE OWNER OR ENGINEER WILL INSPECT AND/OR ENFORCE SAFETY REGULATIONS.
10. IT SH CONSTRU (OR CLAS REVISION: PERMIT.	ALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO OBTAIN AN R-O-W UTILIZATION PERMIT (IF REQUIRED) FOR CTION OF THE PROPOSED UTILITIES. THIS PERMIT MUST BE OBTAINED BY A DULY LICENSED PLUMBING CONTRAC SS A GENERAL CONTRACTOR) PRIOR TO THE START OF CONSTRUCTION. THESE PLANS AND ANY SUBSEQUENT S TO THESE PLANS, THAT ARE ISSUED BY THE ENGINEER, WILL BE SUBJECT TO THE APPROVAL CONDITIONS OF
11. THE	GRAPHIC INFORMATION DEPICTED ON THESE PLANS HAS BEEN COMPILED TO PROPORTION BY SCALE AS ACCURA
AS POSS	BLE. HOWEVER, DUE TO REPRODUCTIVE DISTORTION, REDUCTION, AND/OR REVISIONS, INFORMATION CONTAINED
HEREIN IS	S NOT INTENDED TO BE SCALED FOR CONSTRUCTION PURPOSES.
12. ALL	SPECIFICATIONS AND DOCUMENTS REFERENCED HEREIN SHALL BE OF THE LATEST REVISION. UNDERGROUND UTILITIES WITHIN BASE AND SURFACE MUST BE IN-PLACE, TESTED AND INSPECTED PRIOR TO BASE
14. WOR	K PERFORMED UNDER THIS CONTRACT SHALL INTERFACE SMOOTHLY WITH ANY OTHER WORK BEING PERFORMED C
SITE BY	OTHER CONTRACTORS/SUBCONTRACTORS AND UTILITY COMPANIES. IT WILL BE NECESSARY FOR THE GENERAL
CONTRAC	TOR TO COORDINATE AND SCHEDULE HIS/HER ACTIVITIES ACCORDINGLY.
15. ALL	DIMENSIONS ARE TO FACE OF CURB UNLESS OTHERWISE NOTED.
17. ANY	DISCREPANCIES ON THE DRAWINGS SHALL BE IMMEDIATELY BROUGHT TO THE ATTENTION OF THE OWNER AND
ENGINEEF	BEFORE COMMENCING WORK. NO FIELD CHANGES OR DEVIATIONS FROM DESIGN ARE TO BE MADE WITHOUT PRIOR
APPROVA	L OF THE OWNER AND NOTIFICATION TO THE ENGINEER
18. FIRE	LINE IS DESIGNED BY OTHERS AND IS SHOWN FOR COORDINATION PURPOSES ONLY. FIRE LINES SHALL BE INSTAL
BY A CO	NTRACTOR, DULY LICENSED BY THE STATE OF FLORIDA FIRE MARSHALL'S OFFICE. CONTRACTOR TO VERIFY
REQUIREN	MENTS PRIOR TO CONSTRUCTION OF THE FIRE PROTECTION SYSTEM.
19. ALL	CONCRETE SIDEWALKS SHALL BE CONSTRUCTED PER FDOT DESIGN INDEX (ED. 2021) #522-001.
20. SITE	WORK SHALL COMPLY WITH 2017 FLORIDA BUILDING CODE AND 2012 FLORIDA ACCESSIBILITY CODE.
	STORM DRAINAGE SYSTEM
1. STAND	ARD INDEXES REFER TO THE 2021 EDITION OF F.D.O.T. "ROADWAY AND TRAFFIC DESIGN STANDARDS."
2. ALL S	TORM SEWER PIPE SHALL BE REINFORCED CONCRETE CLASS III (ASTM C-76) UNLESS OTHERWISE NOTED ON PLANS
3. PIPE I	ENGTHS SHOWN ARE APPROXIMATE AND TO CENTER OF DRAINAGE STRUCTURES, WITH THE EXCEPTION OF MITEREE
AND FLAF	RED END SECTIONS, WHICH ARE NOT INCLUDED IN LENGTHS.
5. CONSI	THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY NECESSARY UPGRADES TO EXISTING DRAINAGE STRUCTURE RUCTION OF THE STORMWATER MANAGEMENT SYSTEM MUST BE COMPLETE AND ALL DISTURBED AREAS STABILIZE
ACCORDA	THE WITH THE PERMITTED PLANS AND CONDITIONS PRIOR TO ANY OF THE FULLOWING: ISSUANCE OF THE FIRST
CERTIFICA	ATE OF OCCUPANCY; INITIATION OF INTENDED USE OF THE INFRASTRUCTURE; OR TRANSFER OF RESPONSIBILITY FO
MAINTENA	ANCE OF THE SYSTEM TO A LOCAL GOVERNMENT OR OTHER RESPONSIBLE ENTITY.
6. THE C RECOMME	ONTRACTOR SHALL INSTALL ALL UNDERGROUND STORM WATER PIPING PER JURISDICTION REGULATIONS (MANUFACTION STORM SHALL BE UTILITIZED IF MORE STRINGENT).
7. STORM	WATER PIPES, STRUCTURES, MINIMUM COVER AND INSTALLATION PROCEDURES TO BE IN ACCORDANCE WITH POLK
COUNTY	ENGINEERING STANDARDS.
<b>.</b> .	RAINAGE PIPES SHALL BE FILTER FABRIC WRAPPED PER FDOT STANDARD DESIGN INDEX (ED. 2021) #430–001.

# DRAINAGE SYSTEM TESTING AND INSPECTION

1. THE CONTRACTOR SHALL MAINTAIN AND PROTECT FROM MUD, DIRT, DEBRIS, ETC. THE STORM DRAINAGE SYSTEM UNTIL FINAL ACCEPTANCE OF THE PROJECT. THE STORM SYSTEM WILL BE REINSPECTED BY THE OWNER'S ENGINEER PRIOR TO APPROVAL FOR CERTIFICATE OF OCCUPANCY PURPOSES. THE CONTRACTOR MAY BE REQUIRED TO RECLEAN PIPES AND INLETS AT THE CONTRACTORS EXPENSE AND PRIOR TO FINAL ACCEPTANCE.

2. THE STORM DRAINAGE PIPING SYSTEM SHALL BE SUBJECT TO A VISUAL INSPECTION BY THE OWNER'S ENGINEER PRIOR TO THE PLACEMENT OF BACKFILL. CONTRACTOR TO NOTIFY THE ENGINEER 2 FULL BUSINESS DAYS IN ADVANCE TO SCHEDULE INSPECTION.

## PAVING, GRADING AND DRAINAGE

1. ALL PAVING SHALL BE PERFORMED IN ACCORDANCE WITH THE FLORIDA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.

2. ALL DELETERIOUS SUBSURFACE MATERIAL (I.E. MUCK, PEAT, BURIED DEBRIS, ETC.) IS TO BE EXCAVATED AND REPLACED WITH SUITABLE/COMPACTED SOILS, AS DIRECTED BY THE GEOTECHNICAL ENGINEER OF RECORD. DELETERIOUS MATERIAL IS TO BE STOCKPILED OR REMOVED FROM THE SITE AS DIRECTED BY THE OWNER OR OWNER'S ENGINEER. EXCAVATED AREAS ARE TO BE BACKFILLED WITH APPROVED MATERIALS AND COMPACTED AS SHOWN ON THESE PLANS AND PER THE GEOTECHNICAL REPORT. CONTRACTOR IS RESPONSIBLE FOR ACQUIRING ANY PERMITS THAT ARE NECESSARY FOR REMOVING DELETERIOUS MATERIAL FROM THE SITE.

3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING EXCAVATIONS AGAINST COLLAPSE AND WILL PROVIDE BRACING, SHEETING OR SHORING AS NECESSARY. DEWATERING METHODS SHALL BE USED AS REQUIRED TO KEEP TRENCHES DRY WHILE PIPE AND APPURTENANCES ARE BEING PLACED.

4. ALL NECESSARY FILL AND EMBANKMENT THAT IS PLACED DURING CONSTRUCTION SHALL CONSIST OF MATERIAL SPECIFIED BY THE OWNER'S SOILS TESTING COMPANY OR ENGINEER AND BE PLACED AND COMPACTED ACCORDING TO THESE PLANS.

5. PROPOSED SPOT ELEVATIONS REPRESENT FINISHED PAVEMENT OR GROUND SURFACE GRADES, UNLESS OTHERWISE NOTED. 6. IT MAY BE NECESSARY TO FIELD ADJUST PAVEMENT ELEVATIONS TO PRESERVE THE ROOT SYSTEMS OF TREES SHOWN TO BE

SAVED. CONTRACTOR TO COORDINATE WITH OWNER'S ENGINEER PRIOR TO ANY ELEVATION CHANGES.

7. CONTRACTOR SHALL TRIM, TACK AND MATCH EXISTING PAVEMENT AT LOCATIONS WHERE NEW PAVEMENT MEETS EXISTING PAVEMENT.

8. CURBING SHALL BE PLACED AT THE EDGES OF ALL PAVEMENT, UNLESS OTHERWISE NOTED. REFER TO THE 2021 EDITION OF F.D.O.T. "ROADWAY AND TRAFFIC DESIGN STANDARDS" FOR DETAILS AND SPECIFICATIONS OF ALL F.D.O.T. TYPE CURB AND GUTTERS CALLED FOR IN THESE PLANS.

9. PRIOR TO CONSTRUCTING CONCRETE PAVEMENT, THE CONTRACTOR IS TO SUBMIT A PROPOSED JOINTING PATTERN TO THE SOILS ENGINEER FOR APPROVAL.

10. CONTRACTOR TO PROVIDE A 1/2" TO 1" BITUMINOUS EXPANSION JOINT MATERIAL WITH SEALER AT ABUTMENT OF CONCRETE AND OTHER MATERIALS (STRUCTURES, OTHER POURED)

11. ALL PAVEMENT MARKINGS SHALL BE MADE IN ACCORDANCE WITH F.D.O.T. STANDARD INDEX #711-001.

12. THE CONTRACTOR WILL STABILIZE BY SEED AND MULCH, SOD, OR OTHER APPROVED MATERIALS ANY DISTURBED AREAS WITHIN ONE WEEK FOLLOWING CONSTRUCTION OF THE UTILITY SYSTEMS AND PAVEMENT AREAS. CONTRACTOR SHALL MAINTAIN SUCH AREAS UNTIL FINAL ACCEPTANCE BY OWNER. CONTRACTOR TO COORDINATE WITH OWNER REGARDING TYPE OF MATERIAL, LANDSCAPING AND IRRIGATION REQUIREMENTS.

13. THE CONTRACTOR SHALL RESTORE OFF-SITE CONSTRUCTION AREAS TO EQUAL AND/OR BETTER CONDITION THAN EXISTING PRIOR TO START OF CONSTRUCTION.

14. UNLESS OTHERWISE NOTED, GRADE TO MEET EXISTING ELEVATION AT PROPERTY LINES.

15. SURVEY MONUMENTS OR BENCHMARKS, WHICH HAVE TO BE DISTURBED BY THIS WORK, SHALL BE REPLACED UPON COMPLETION OF WORK BY A REGISTERED LAND SURVEYOR AT CONTRACTORS EXPENSE.

16. FINAL GRADES SHOWN INCLUDE SOD HEIGHT. ALL AREAS SHALL BE GRADED TO DRAIN AWAY FROM THE BUILDINGS.

17. ALL WORK TO BE PERFORMED IN ACCORDANCE WITH ALL LOCAL, STATE AND JURISDICTIONAL PERMITTING AGENCIES.

18. CONTRACTOR IS TO ADJUST ANY UTILITY ELEMENT MEANT TO BE FLUSH WITH GRADE (CLEAN-OUTS, MANHOLES, CATCH BASINS, INLETS, ETC.) THAT IS AFFECTED BY SITE WORK OR GRADE CHANGES, WHETHER SPECIFICALLY NOTED ON PLANS OR NOT.

19. ALL WORK SHALL COMPLY WITH THE GEOTECHNICAL REPORT BY UNIVERSAL ENGINEERING SCIENCES ON JULY 9, 2021.

20. CONTRACTOR SHALL SOD ALL DISTURBED AREAS WITH BAHIA UNLESS OTHERWISE NOTED.

# **PAVING/GRADING TESTING AND INSPECTION**

1. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING APPLICABLE TESTING WITH THE SOILS ENGINEER. TESTS WILL BE REQUIRED PURSUANT WITH THE SOILS REPORT. UPON COMPLETION OF WORK THE SOILS ENGINEER WILL SUBMIT CERTIFICATIONS TO THE OWNER AND OWNER'S ENGINEER STATING THAT ALL REQUIREMENTS HAVE BEEN MET.

2. A QUALIFIED TESTING LABORATORY SHALL PERFORM ALL TESTING NECESSARY TO ASSURE COMPLIANCE OF THE IN-PLACE MATERIALS AS REQUIRED BY THESE PLANS AND GEOTECHNICAL REPORT. THE VARIOUS AGENCIES AND PERMIT CONDITIONS. SHOULD ANY RETESTING BE REQUIRED DUE TO THE FAILURE OF ANY TESTS TO MEET THESE REQUIREMENTS, THE CONTRACTOR WILL BEAR ALL COSTS OF SAID RETESTING.

### EARTHWORK / DEMUCKING PROCEDURES

- 1. A GEOTECHNICAL ENGINEERING INVESTIGATION REPORT HAS BEEN PREPARED FOR PURPOSES OF STORM WATER DESIGN, OF WHICH COPIES ARE AVAILABLE THROUGH THE OWNER OR THEIR SOIL TESTING COMPANY. A GEOTECHNICAL ENGINEER SHALL BE RETAINED BY THE CONTRACTOR TO PROVIDE ON-SITE INSPECTIONS DURING EXCAVATION/FILL OPERATIONS AND TESTING OF THE COMPACTED FILL SO THAT PROPER DOCUMENTATION OF THE REQUIRED COMPACTING CRITERIA CAN BE PROVIDED.
- 2. ALL EXISTING DEBRIS (ABOVE OR BELOW GROUND), CONSTRUCTION DEBRIS AND OTHER WASTE MATERIAL SHALL BE DISPOSED OF OFF-SITE BY THE CONTRACTOR, IN ACCORDANCE WITH APPLICABLE REGULATORY AGENCY REQUIREMENTS IN A LEGAL MANNER.
- 3. UNLESS OTHERWISE NOTED, GRADE TO MEET EXISTING ELEVATION AT PROPERTY LINES. FINAL GRADES SHOWN INCLUDE SOD HEIGHT. PROPOSED SPOT ELEVATIONS REPRESENT FINISHED PAVEMENT OR GROUND SURFACE GRADES, UNLESS OTHERWISE NOTED. IT MAY BE NECESSARY TO FIELD ADJUST PAVEMENT ELEVATIONS TO PRESERVE THE ROOT SYSTEMS OF TREES SHOWN TO BE SAVED. CONTRACTOR TO COORDINATE WITH OWNER'S ENGINEER PRIOR TO ANY ELEVATION CHANGES. ALL AREAS SHALL BE GRADED TO DRAIN AWAY FROM THE BUILDINGS.
- 4. THE CONTRACTOR SHALL INSURE THAT PROPER SOIL DENSITIES ARE ACHIEVED FOR PLACEMENT OF ALL HEADWALL/ENDWALL FOOTINGS, RETAINING WALL FOOTINGS, AND IN GENERAL, ANY FOOTING SUPPORT DESCRIBED ON THESE PLANS. IT WILL ALSO BE THE RESPONSIBILITY OF THE CONTRACTOR TO INSURE THAT SUFFICIENT SOILS TESTING HAS BEEN PERFORMED PRIOR TO FINAL INSTALLATION OF IMPROVEMENTS.
- 5. ANY UNSUITABLE ORGANIC SOIL SHALL BE EXCAVATED TO A MINIMUM MARGIN OF 6 FEET BEYOND ITS PERIPHERY EXCAVATED TO EXPOSE THE UNDERLYING NON-ORGANIC FINE SAND.
- 6. IF DETERMINED NECESSARY, DEWATERING DURING EXCAVATING/BACKFILLING OPERATIONS MAY BE ACCOMPLISHED BY DITCHING AND THE USE OF SUMP PUMPS AND/OR OTHER METHODS (WELL POINTS), AS NECESSARY. CONTRACTOR TO OBTAIN ALL REQUIRED PERMITS FOR DEWATERING ACTIVITIES THAT MAY BE REQUIRED.
- 7. UPON APPROVAL OF THE GEOTECHNICAL ENGINEER, THE EXCAVATED AREAS MAY BE BACKFILLED WITH CLEAN FINE SAND FREE OF UNSUITABLE OR DELETERIOUS MATERIAL. HOWEVER, THE FILL SHOULD NOT BE PLACED IN MORE THAN 6 INCHES OF STANDING WATER. ONCE THE FILL IS AT LEAST 2 FEET ABOVE THE DEWATERED LEVEL, BACKFILLING MAY PROCEED AS DIRECTED BY THE GEOTECHNICAL FNGINFFR.
- 8. CONTRACTOR TO FOLLOW THE GUIDANCE OF THE REFERENCED GEOTECHNICAL ENGINEERING INVESTIGATION REPORT OR INDICATE WHETHER ON-SITE GEOTECHNICAL ENGINEER SHALL DETERMINE DEPTH OF DEMUCKING AND/OR REMOVAL OF UNSUITABLE FILL.
- 9. ALL NECESSARY FILL AND EMBANKMENT THAT IS PLACED DURING CONSTRUCTION SHALL CONSIST OF MATERIAL SPECIFIED BY THE OWNER'S SOILS TESTING COMPANY OR ENGINEER AND BE PLACED AND COMPACTED ACCORDING TO THESE PLANS.

- STANDARDS.

- "EXISTING TO REMAIN".
- JURISDICTION.
- OFF-SITE IN A LEGAL MANNER.
- REMOVED".

ENGINEER.

# **DEWATERING NOTES**

DURING THE EXCAVATION OF THE STORMWATER FACILITIES, AND IF GROUNDWATER IS ENCOUNTERED, THE CONTRACTOR SHALL CONSTRUCT A SEDIMENT BASIN TO PROVIDE A DISCHARGE POINT FOR DEWATERING. THE SEDIMENT BASIN CAN BE CELL IN THE PROPOSED EXCAVATION AREA OF A POND OR IT CAN BE A BERMED AREA ABOVE GROUND. ALL DEWATERING MUST BE HELD IN THE SEDIMENT AREA UNTIL THE WATER IS CLEAN SUCH THAT THERE WOULD BE NO TURBID DISCHARGE. AFTER THE WATER IN THE SEDIMENT BASIN IS CLEAN, THE WATER MAY BE RELEASED INTO THE ON-SITE POND PROVIDED THERE IS NO ADVERSE IMPACT TO THE EXISTING WATER QUALITY.

2. UNDER NO CIRCUMSTANCES WILL THE DISCHARGE FROM THE ON-SITE DEWATERING BE DIRECTLY DISCHARGED OFFSITE.

3. IF CONTRACTOR ENCOUNTERS SILTY/CLAY SAND, WHICH CAUSE THE WATER TO BECOME TURBID, HE/SHE SHALL TREAT THE SEDIMENT BASIN WITH CHEMICAL ADDITIVE SUCH AS ALLUM IN ORDER TO PROMOTE THE COAGULATION OF THE PARTICLES WHICH ALLOW THE TO SETTLE AND THE WATER TO BECOME LESS TURBID. IF TURBID WATER ENCOUNTERED DURING EXCAVATION OF THE PONDS, THE CONTRACTOR SHALL NOTIFY THE ENGINEER OF RECORD IMMEDIATELY TO DETERMINE THE COURSE OF ACTION THAT IS APPROPRIATE TO ELIMINATE THE TURBITY AND ALLOW DISCHARGE THAT MEET WATER QUALITY

4. THE CONTRACTOR SHALL SEQUENCE THE EXCAVATION OF THE STORMWATER PONDS SUCH THAT A SEDIMENT BASIN WILL BE AVAILABLE AT ALL TIMES. THE SEDIMENT BASIN CAN BE RELOCATED AS NECESSARY SUBJECT TO THE WATER WITHIN THE SEDIMENT BASIN BEING NON-TURBID AND ACCEPTABLE FOR DISCHARGE OFF-SITE.

# DEMOLITION

1. CONTRACTOR SHALL SUBMIT DEMOLITION SCHEDULE TO OWNER PRIOR TO PROCEEDING WITH DEMOLITION ACTIVITIES.

2. EXTENT OF SITE CLEARING IS SHOWN ON DRAWINGS.

3. CONTRACTOR SHALL CONDUCT SITE DEMOLITION OPERATIONS TO ENSURE MINIMUM INTERFERENCE WITH ROADS, STREETS, WALKS, AND OTHER ADJACENT OCCUPIED OR USED FACILITIES. DO NOT CLOSE OR OBSTRUCT STREETS, WALKS OR OTHER OCCUPIED OR USED FACILITIES WITHOUT PERMISSION FROM AUTHORITIES HAVING JURISDICTION.

4. CONTRACTOR SHALL PROVIDE PROTECTION NECESSARY TO PREVENT DAMAGE TO EXISTING IMPROVEMENTS INDICATED ON PLAN

5. CONTRACTOR SHALL RESTORE DAMAGED IMPROVEMENTS TO THEIR ORIGINAL CONDITION, AS ACCEPTABLE TO PARTIES HAVING

6. CONTRACTOR SHALL REMOVE WASTE MATERIALS AND UNSUITABLE AND EXCESS TOPSOIL FROM PROPERTY AND DISPOSE OF

7. CONTRACTOR SHALL DEMOLISH AND COMPLETELY REMOVE FROM SITE MATERIAL INDICATED ON PLAN OR NOTES "TO BE

8. CONTRACTOR SHALL PROTECT STRUCTURES, UTILITIES, SIDEWALKS, PAVEMENTS, AND OTHER FACILITIES FROM DAMAGE CAUSED BY SETTLEMENT, LATERAL MOVEMENT, UNDERMINING, WASHOUT AND OTHER HAZARDS CREATED BY THE DEMOLITION OPERATION.

# TREES AND VEGETATION

1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE MAINTENANCE OF ALL LANDSCAPE BUFFERS AND RETENTION AND DETENTION FACILITIES UNTIL THE WORK HAS BEEN ACCEPTED BY THE OWNER. ALL DISTURBED AREAS SHALL BE RETURNED TO THEIR ORIGINAL CONDITION.

# AS BUILT

1. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR SHALL FURNISH THE OWNER'S ENGINEER WITH COMPLETE "AS-BUILT" INFORMATION, CERTIFIED BY A REGISTERED LAND SURVEYOR. THIS "AS-BUILT" INFORMATION SHALL INCLUDE INVERT ELEVATIONS, LOCATIONS OF STRUCTURES FOR ALL UTILITIES INSTALLED, AS WELL AS GRADE BREAK LOCATIONS AND ELEVATIONS FOR PROPOSED CONSTRUCTION. NO ENGINEER'S CERTIFICATIONS FOR CERTIFICATE OF OCCUPANCY (C.O.) PURPOSES WILL BE MADE UNTIL THIS INFORMATION HAS BEEN RECEIVED AND ACCEPTED BY THE OWNER'S

2. ALL "AS-BUILT" ELEVATIONS SHALL BE BASED ON THE NATIONAL GEODETIC VERTICAL DATUM OF 1929 (NGVD29).

## PAVEMENT MARKING AND SIGNAGE

THE INSTALLATION, SHAPE, AND SIZE OF ALL SIGNS AND THEIR LETTERING SHALL COMPLY WITH THE LATEST EDITIONS OF THE U.S. DEPARTMENT OF TRANSPORTATION "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, ED. 2009" (MUTCD). AND THE F.D.O.T. "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, ED. 2021", AND THE F.D.O.T. "DESIGN STANDARDS FOR DESIGN, CONSTRUCTION, MAINTENANCE, AND UTILITY OPERATIONS ON THE STATE HIGHWAY SYSTEM, ED. 2016". WHERE CONFLICTS EXIST BETWEEN THE PLANS AND THE ABOVE MENTIONED SPECIFICATIONS, THE MORE STRINGENT CRITERIA SHALL PREVAIL.

2. STOP BARS AND STOP SIGNS ARE TO BE PROVIDED AT ALL INTERNAL, ONSITE INTERSECTIONS, WITH THE EXCEPTION OF SIGNALIZED INTERSECTIONS (UNLESS OTHERWISE NOTED).

3. ALL PAVEMENT MARKINGS SHALL COMPLY WITH THE 2021 F.D.O.T. STANDARD INDEX (ED. 2021) #711-001.

	GENERAL NOTES
--	---------------

C1.0

CALL 48 HOURS BEFORE YOU DI	
IT'S THE LAW! DIAL 811	Know what's below.

SUNSHINE STATE ONE CALL OF FLORIDA, INC.

# SANITARY SYSTEM

1. ALL PVC PIPE SHALL BE SOLID WALL POLYVINYL CHLORIDE PIPE AND COMPLY WITH ASTM D 3034 AND ALL APPLICABLE ASTM DOCUMENTS AS COVERED IN SECTION NO. 2 OF ASTM D 3034. MAIN LINES SHALL BE A MINIMUM OF 8" DIAMETER, AND LATERALS SHALL BE A MINIMUM 6" DIAMETER.

2. ALL GRAVITY SEWERS MUST BE SDR 26 PVC. ELASTOMERIC GASKET JOINTS SHALL BE UTILIZED FOR PVC PIPE, AND SHALL COMPLY WITH ASTM F477, ASTM D3034 & ASTM F679. JOINTS SHALL COMPLY WITH ASTM D3212.

3. ALL SLOPES FOR GRAVITY SEWER MAINS AND SERVICE CONNECTIONS SHALL COMPLY WITH THE FOLLOWING MINIMUM GRADES: 4" @ 2.00%; 6" @ 1.00%; AND 8" @ 0.40%.

4. ALL SANITARY SEWER WORK SHALL CONFORM WITH APPLICABLE CITY OF LAKE CITY WATER UTILITIES DEPARTMENT STANDARDS AND SPECIFICATIONS.

5. PRIOR TO COMMENCING WORK WHICH REQUIRES CONNECTING PROPOSED FACILITIES TO EXISTING LINES OR APPURTENANCES, THE CONTRACTOR SHALL VERIFY THE LOCATION AND ELEVATION(S) OF EXISTING CONNECTION POINT(S) AND NOTIFY THE OWNER'S ENGINEER OF ANY CONFLICTS OR DISCREPANCIES.

# SANITARY TESTING AND INSPECTION

1. ALL GRAVITY SEWER PIPING SHALL BE SUBJECT TO A VISUAL INSPECTION BY THE OWNER'S ENGINEER AND APPLICABLE MUNICIPALITY/AGENCY. THE CONTRACTOR SHALL NOTIFY THE ENGINEER 48 HOURS IN ADVANCE TO SCHEDULE INSPECTION(S). THE CONTRACTOR SHALL BE RESPONSIBLE FOR COSTS ASSOCIATED WITH A LAMPING INSPECTION OF THE PROPOSED GRAVITY SEWER LINE CONSTRUCTION. THE CONTRACTOR SHALL PROVIDE COPIES OF THE LAMPING INSPECTION TO THE ENGINEER, THE OWNER AND THE APPLICABLE MUNICIPALITY/AGENCY.

2. THE CONTRACTOR SHALL PERFORM AN INFILTRATION/EXFILTRATON TEST ON ALL GRAVITY SEWERS IN ACCORDANCE WITH THE REGULATORY AGENCY HAVING JURISDICTION. SAID TESTS ARE TO BE CERTIFIED BY THE ENGINEER OF RECORD AND SUBMITTED TO THE REGULATORY AGENCY FOR APPROVAL. THE SCHEDULING, COORDINATION AND NOTIFICATION OF ALL PARTIES IS THE CONTRACTOR'S RESPONSIBILITY.

3. LEAKAGE TESTS ARE SPECIFIED REQUIRING THAT:

A. THE LEAKAGE EXFILTRATION OR INFILTRATION DOES NOT EXCEED 200 GALLONS PER INCH OF PIPE DIAMETER PER MILE PER DAY FOR ANY SECTION OF THE SYSTEM. B. EXFILTRATION OR INFILTRATION TESTS BE PERFORMED WITH A MINIMUM POSITIVE HEAD OF 2 FEET

C. AIR TESTS, AS A MINIMUM, CONFORM TO THE TEST PROCEDURE DESCRIBED IN ASTM C-828 FOR CLAY PIPE, ASTM C 924 FOR CONCRETE PIPE, ASTM F-1417 FOR PLASTIC PIPE, AND FOR OTHER MATERIALS APPROPRIATE TEST PROCEDURES.

4. CONTRACTOR TO PERFORM APPROPRIATE DEFLECTION TESTS FOR ALL FLEXIBLE PIPE. TESTING IS REQUIRED AFTER THE FINAL BACKFILL HAS BEEN IN PLACE AT LEAST 30 DAYS TO PERMIT STABILIZATION OF THE SOIL-PIPE SYSTEM. TESTING REQUIREMENTS SPECIFY:

A. NO PIPE SHALL EXCEED A DEFLECTION OF 5%. B. USING A RIGID BALL OR MANDREL FOR THE DEFLECTION TEST WITH A DIAMETER NOT LESS THAN 95% OF THE BASE INSIDE DIAMETER OR AVERAGE INSIDE DIAMETER OF THE PIPE, DEPENDING ON WHICH IS SPECIFIED IN THE ASTM SPECIFICATION, INCLUDING THE APPENDIX, TO WHICH THE PIPE IS MANUFACTURED. C. PERFORMING THE TEST WITHOUT MECHANICAL PULLING DEVICES.

5. CONTRACTOR TO INSPECT & TEST MANHOLE FOR WATERTIGHTNESS OR DAMAGE PRIOR TO PLACING INTO SERVICE. AIR TESTING, IF SPECIFIED FOR CONCRETE SEWER MANHOLES, SHALL CONFORM TO THE TEST PROCEDURES DESCRIBED IN ASTM C-1244.

# POTABLE WATER SYSTEM

1. ALL DIP PIPE SHALL BE CLASS 50 OR HIGHER. REFER TO NOTE #4 BELOW FOR ADDITIONAL DIP SPECIFICATIONS. ADEQUATE MEASURES (PER AWWA, FDEP, AND POLK COUNTY CRITERIA) AGAINST CORROSION SHALL BE UTILIZED.

2. ALL WATER MAIN PIPE FITTINGS AND APPURTENANCES SHALL BE INSTALLED TO COMPLY WITH POLK COUNTY STANDARDS AND SPECIFICATIONS.

3. ALL WATER SERVICE LINES, VALVES AND METERS SHALL BE INSTALLED TO COMPLY WITH APPLICABLE MUNICIPALITY/AGENCY DEPARTMENT STANDARDS AND SPECIFICATIONS.

4. ALL DUCTILE IRON PIPE, 4" TO 24", SHALL BE MANUFACTURED IN ACCORDANCE WITH THE LATEST EDITION OF AWWA C151/A21.51. PIPE SHALL BE FURNISHED IN 18 OR 20 FOOT SECTIONS, PIPE THICKNESS SHALL BE CLASS 50, UNLESS OTHERWISE SPECIFIED.

5. ALL WATER SYSTEM CONSTRUCTION, FROM THE POINT OF CONNECTION IN THE RIGHT OF WAY UP TO AND INCLUDING POINT OF METERING AND BACK FLOW PREVENTION (IF REQUIRED), SHALL BE BUILT ACCORDING TO POLK COUNTY STANDARDS AND SPECIFICATIONS.

6. CONTRACTOR TO INSTALL TEMPORARY BLOWOFFS, AT THE END(S) OF PROPOSED WATER MAINS AND SERVICE LATERALS TO BUILDING(S), TO ASSURE ADEQUATE (PER AWWA, FDEP, AND POLK COUNTY CRITERIA) FLUSHING AND DISINFECTION/CHLORINATION.

7. ALL WATER MAINS SHALL BE STERILIZED IN ACCORDANCE WITH THE APPLICABLE SECTION OF THE LATEST AWWA SPECIFICATION C651 AND CITY OF Land City WATER DEPARTMENT SPECIFICATIONS.

8. ALL PVC WATER MAIN, 6" TO 12" DIAMETER PIPING, SHALL BE AWWA C-900 DR-18. JOINTS SHALL BE RUBBER GASKETED PUSH-ON CONFORMING TO ASTM D1869.

9. POTABLE WATER MAINS WILL BE PVC SDR 21 (200 PSI) FOR PIPES LESS THEN 4". SCHEDULE 40 AND SCHEDULE 80 PIPING MATERIAL ARE ALSO ACCEPTABLE FOR PIPES SIZES LESS THAN 4". THE ABOVE TYPE INSTALLATIONS MUST BEAR THE "NFS" STAMP FOR COMPATIBILITY WITH POTABLE WATER USE.

10. ALL POLYVINYL CHLORIDE PIPE SHALL BE LAID WITH AN INSULATED 10 GAUGE A.W.G. SOLID STRAND COPPER WIRE ON TOP OF THE PIPE. THIS WIRE IS TO BE CONTINUOUS WITH SPLICES MADE ONLY BY METHODS APPROVED BY THE ENGINEER. THIS WIRE IS TO BE SECURED TO ALL VALVES, TEES AND ELBOWS.

11. ALL POTABLE WATER WORK SHALL CONFORM WITH APPLICABLE POLK COUNTY UTILITIES DEPARTMENT STANDARDS AND SPECIFICATIONS.

12. PVC PIPE BURIED BENEATH ROADWAYS, PARKING LOTS OR PARKING LOT ENTRANCES SHALL MEET AWWA SPECIFICATION C900 OR C905, LATEST REVISION. ALL 6" TO 12" PIPE IN SUCH LOCATIONS SHALL BE A MINIMUM OF CLASS 200, DR-14, AND ALL 14" TO 36" PIPE SHALL BE A MINIMUM OF CLASS 235, DR-18.

## POTABLE WATER TESTING AND INSPECTION

1. ALL COMPONENTS OF THE WATER SYSTEM, INCLUDING FITTINGS, HYDRANTS, CONNECTIONS, AND VALVES SHALL BE PROPERLY PRESSURE TESTED AND ACCEPTED BY THE OWNER'S ENGINEER. PRESSURE TESTS TO BE IN ACCORDANCE WITH POLK COUNTY UTILITIES DEPARTMENT SPECIFICATIONS. CONTRACTOR TO NOTIFY THE OWNER'S ENGINEER AND APPLICABLE AGENCY INSPECTORS 2 FULL BUSINESS DAYS IN ADVANCE OF PERFORMING TESTS.

2. CONTRACTOR TO PERFORM CHLORINATION AND BACTERIOLOGICAL SAMPLING, AND OBTAIN CLEARANCE OF DOMESTIC AND FIRE LINE WATER SYSTEM(S). COPIES OF ALL BACTERIOLOGICAL TEST RESULTS ARE TO BE SUBMITTED TO THE OWNER'S ENGINEER FOR CERTIFICATION PURPOSES.

3. ALL WATER MAINS SHALL BE PRESSURE TESTED IN ACCORDANCE WITH AWWA MANUAL M23, CONCERNING HYDROSTATIC TESTING OF PVC PIPING. OFF-SITE UTILITIES HYDROSTATIC TESTING TO BE WITNESSED BY THE CITY OF Land City WATER DEPARTMENT INSPECTOR.

# FDOT GENERAL NOTES

1. MAINTENANCE OF TRAFFIC TO BE SUPERVISED BY A CERTIFIED PERSON.

2. THE CONTRACTOR SHALL NOTIFY THE DEPARTMENT A MINIMUM OF TWO BUSINESS DAYS PRIOR TO ANY LANE CLOSURES OR BEGINNING ANY CONSTRUCTION WITHIN THE FDOT RIGHT-OF-WAY.

3. ALL WORK PERFORMED WITHIN THE FDOT RIGHT OF WAY SHALL BE IN ACCORDANCE WITH THE FY2021-22 OR CURRENT EDITION OF FDOT STANDARD PLANS.

4. IF THE DEPARTMENT DETERMINES THAT AS-BUILT CONDITIONS VARY SIGNIFICANTLY FROM THE APPROVED PLANS, THE PERMITTEE SHALL PROVIDE AS-BUILT PLANS, ALONG WITH A RECORD DRAWINGS REPORT BY PERMITTEE'S PROFESSIONAL ENGINEER, FORM 850-040-19, WITHIN 30 DAYS.

5. IT WILL BE THE RESPONSIBILITY OF THE PERMITTEE TO REPAIR ANY DAMAGE TO FDOT FACILITIES CAUSED BY CONSTRUCTION OF THE PROJECT.

6. TEST RESULTS OF ANY TESTS TAKEN FOR OR DURING CONSTRUCTION OF THE PERMITTED WORK SHALL BE PROVIDED TO THE FDOT UPON REQUEST.

7. ALL CONCRETE TO BE REMOVED SHALL BE SAW CUT AT THE NEAREST JOINT IN GOOD CONDITION, SO AS TO PRODUCE A CONNECTION WITH NEW CONCRETE THAT IS FREE OF CRACKS, DEFORMITY IN SHAPE, NOTICEABLE VOIDS, SURFACE IRREGULARITIES, AND OTHER DEFECTS.

8. ALL CONCRETE SHALL BE AN APPROVED FDOT MIX DESIGN OF 3,000 PSI MINIMUM.

9. ALL MATERIALS INSTALLED WITHIN FDOT RIGHT-OF-WAY SHALL BE LIMITED TO THOSE ON THE FDOT'S QUALIFIED PRODUCTS LIST OR APPROVED PRODUCT LIST OF TRAFFIC CONTROL SIGNALS AND DEVICES.

10. THE PERMITTEE SHALL CONTACT THE CITY OF LAKE CITY TRAFFIC DEPT. (386) 758-5400.

11. ALL CONSTRUCTION IN THE FDOT ROW SHALL CONFIRM TO THE LATEST EDITIONS OF THE FDOT DESIGN STANDARDS, THE FDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, AND THE FDOT UTILITY ACCOMMODATION MANUAL. 12. ALL DISTURBED AREAS IN FDOT ROW SHALL BE SODDED.

13. ALL WORK PERFORMED WITHIN THE FDOT RIGHT-OF-WAY SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE FDOT DESIGN STANDARDS, THE LATEST EDITION OF THE SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, AND THE 2017 UTILITY ACCOMMODATION MANUAL.

14. PLEASE NOTIFY JACKSONVILLE OPERATIONS TWO BUSINESS DAYS BEFORE BEGINNING WORK @ (904) 306-7500.

			KHA PROJECT LICENSED PROFESSIONAL				
			149880040				
			DATE				
			US/U4/ZUZZ AROD COSTUBBS, P.E.				
1.		GENERAL NOIEU	SCALE AS SHOWN & STATE OF US	© 2022 KIMLEY-HORN AND ASSOCIATES, INC.			
1			DESIGNED BY TJM DICKIDA LICENSE NUMBER	189 S. ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801			
			DRAWN BY TJM	PHONE: 407-898-1511 www.kimiev_hodn.com decistdy no 35106			
1	CITY OF LAKE CITY FLORIDA		CHECKED BY JCS DATE: 05/04/2022		No.	DATE BY	
5							1

CALL 48 HOUR BEFORE YOU D	s <b>811</b> .
IT'S THE LAW! DIAL 811	Know what's below, Call before you
SUNSHINE STATE ONE (	

# **STORMWATER POLLUTION PREVENTION PLAN**

#### SITE DESCRIPTION

PROJECT NAME AND LOCATION

CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION TAX PARCEL: 24-29-11-281016-000020 CITY OF LAKE CITY, FLORIDA

\*SEE COVER SHEET FOR LOCATION MAP

DEVELOPER NAME AND ADDRESS

SCHAFFER CONSTRUCTION, LLC 2601 NETWORK BLVD., SUITE 413 FRISCO, TX 75034 CONTACT: DIANE BERRY PHONE: (407) 580-5173 EMAIL: DBERRY@SCHAFFERCONST.COM

#### **PROJECT DESCRIPTION**

THE PROJECT WILL CONSIST OF CONSTRUCTING A CIRCLE K CONVENIENCE STORE BUILDING EXPANSION WITH HIGH SPEED DIESEL FUELING STATIONS AND SEMI-TRUCK PARKING ON A PREVIOUSLY MASS GRADED SITE. THE PROJECT IS 3.46 ± ACRES LOCATED ON THE NORTHEAST CORNER OF US HIGHWAY 90 AND CENTURION COURT IN LAKE CITY, FLORIDA.

PROJECT AREA: 3.46 ACRES CONTRIBUTING DRAINAGE AREA: 3.46 ACRES LONGITUDE : W 82° 41' 26.2" LATITUDE: N 30° 10' 51.1"

ACTIVITIES THAT REQUIRE EROSION CONTROL

PROVIDING A STABILIZED CONSTRUCTION ENTRANCE, PERIMETER, AND OTHER EROSION AND SEDIMENT CONTROLS; DEMOLITION; SITE GRADING; INSTALLATION OF STORM WATER; CURB, DRIVEWAYS, AND ROADWAY FACILITIES.

\*SEE PLANS FOR THE LOCATION OF TEMPORARY SEDIMENT BARRIERS AND OTHER EROSION CONTROL METHODS.

SOIL PARAMETERS

SOIL TYPES:

SERIES NAME	HYDROLOGIC GROUP
BLANTON FINE SAND, 0-5% SLOPES	A

#### **SEQUENCE OF MAJOR ACTIVITIES**

THE ORDER OF CONSTRUCTION IS AS FOLLOWS:

1.	PROVIDE STABILIZED CONSTRUCTION ENTRANCE
2	INSTALL SUIT FENCES AND OTHER EDOSION CONTROL METHODS

- 2. INSTALL SILT FENCES AND OTHER EROSION CONTROL METHODS 3. DEMOLITION
- 4. CLEAR AND GRUB FOR SEDIMENT BASIN AND EARTH DIKE
- CONSTRUCT EARTH DIKE AND SEDIMENT BASIN
- 6. FINISH CLEARING AND GRUBBING
- 7. REMOVE AND STORE TOPSOIL 8. PROVIDE INITIAL GRADING AS REQUIRED
- 9. STABILIZE ALL DISTURBED AREAS AS SOON AS POSSIBLE
- 10. INSTALL UTILITIES, STORM SEWER, CURB AND GUTTER
- 11. INSTALL BASE TO ROAD AND DRIVEWAY AREA
- 12. FINISH GRADING ENTIRE SITE
- 13. CONSTRUCT FINAL PAVING
- 14. REMOVE ACCUMULATED SEDIMENT
- 15. REMOVE ANY ITEMS THAT ARE NOT REQUIRED

TIMING OF CONTROL MEASURES

THE INSTALLATION OF SILT FENCE (AND OTHER EROSION CONTROL MEASURES), A STABILIZED ENTRANCE AND SEDIMENT BASIN SHALL OCCUR PRIOR TO CLEARING AND GRUBBING ACTIVITY. AFTER CONSTRUCTION IS COMPLETE, THE ACCUMULATED SEDIMENT SHALL BE REMOVED AND THE AREAS SHALL BE REGRADED AND PERMANENTLY STABILIZED AS SHOWN ON THE PLANS.

#### **EROSION AND SEDIMENT CONTROLS**

BEST MANAGEMENT PRACTICES SHALL BE USED FOR THIS PROJECT TO CONTROL EROSION AND TURBIDITY CAUSED BY STORM WATER RUN-OFF. THE LOCATION AND DETAILS OF EROSION CONTROL METHODS ARE SHOWN ON THE PLANS. THE CONTRACTOR IS RESPONSIBLE FOR PLACING AND MAINTAINING THESE CONTROL METHODS AS SHOWN ON THE PLANS OR AS REQUIRED. HE/SHE SHALL ALSO PROVIDE THE REQUIRED EROSION PROTECTION AS REQUIRED BY LOCAL, STATE AND FEDERAL LAW.

STORM WATER MANAGEMENT

STORMWATER COLLECTION SHALL BE PROVIDED BY DRAINAGE INLETS WITHIN THE PROPOSED DRIVE AISLES. THE PROPOSED DRAINAGE INLETS WILL CONNECT INTO THE EXISTING OFFSITE STORM DRAINAGE COLLECTION SYSTEM, WHICH DRAINS TO AN OFFSITE MASTER STORMWATER POND THAT PROVIDES ATTENUATION FOR THIS SITE. THE POND IS DESIGNED IN ACCORDANCE WITH SRWMD AND LAKE CITY CODE.

STABILIZATION PRACTICES:

TEMPORARY STABILIZATION - TOPSOIL STOCK PILES AND DISTURBED PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITY TEMPORARILY OR PERMANENTLY CEASE, SHALL BE STABILIZED WITH TEMPORARY SEED AND MULCH WITHIN 7 DAYS OF THE LAST CONSTRUCTION ACTIVITY IN THAT AREA. THE TEMPORARY SEED REQUIRED CAN BE FOUND IN TABLE 1.65 A OF THE FLORIDA DEVELOPMENT MANUAL. PRIOR TO SEEDING, WHERE SOILS ARE ACIDIC 2 TONS OF PULVERIZED AGRICULTURAL LIMESTONE SHOULD BE ADDED PER ACRE AND 450 POUNDS OF 10-20-20 FERTILIZER SHALL BE APPLIED TO EACH ACRE. AFTER SEEDING, EACH AREA SHALL BE IMMEDIATELY MULCHED WITH STRAW OR EQUIVALENT EQUAL. AREAS OF THE SITE WHICH ARE TO BE PAVED SHALL BE TEMPORARILY STABILIZED BY APPLYING GEOTEXTILE AND STONE SUB-BASE UNTIL BITUMINOUS PAVEMENT CAN BE APPLIED.

PERMANENT STABILIZATION - DISTURBED PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES PERMANENTLY CEASE SHALL BE STABILIZED WITH PERMANENT SEED NO LATER THAN 7 DAYS AFTER THE LAST CONSTRUCTION ACTIVITY. THE APPROPRIATE PERMANENT SEED MIX CAN BE FOUND IN TABLES 1.66A. 1.66B AND 1.66C OF THE FLORIDA DEVELOPMENT MANUAL. PRIOR TO SEEDING, 2 TONS/ACRE OF FINELY GROUND AGRICULTURAL LIMESTONE AND THE PROPER FERTILIZER BASED ON THE TYPE OF SEEDING SHALL BE APPLIED TO EACH ACRE TO PROVIDE PLANT NUTRIENTS. AFTER SEEDING, EACH AREA SHALL BE MULCHED IMMEDIATELY.

STRUCTURAL PRACTICES

EARTH DIKE - IF REQUIRED, AN EARTH DIKE SHALL BE CONSTRUCTED ALONG THE SITE PERIMETER. A PORTION OF THE DIKE SHALL DIVERT RUN-ON AROUND THE CONSTRUCTION SITE. THE REMAINING PORTION OF THE DIKE SHALL COLLECT RUNOFF FROM THE DISTURBED AREA AND DIRECT THE RUNOFF TO THE SEDIMENT BASIN.

SEDIMENT BASIN - A SEDIMENT BASIN SHALL BE CONSTRUCTED IN THE COMMON DRAINAGE AREA FOR THE SITE. ALL SEDIMENT COLLECTED IN THE BASIN MUST BE REMOVED FROM THE BASIN UPON COMPLETION OF CONSTRUCTION. SEDIMENT FROM THE BASIN MAY BE USED AS FILL ON THE SITE IF IT IS SUITABLE SOIL.

WASTE DISPOSAL

WASTE MATERIALS - ALL WASTE MATERIALS SHALL BE COLLECTED AND STORED IN A METAL DUMPSTER WITH A SECURE LID IN ACCORDANCE WITH ALL LOCAL AND STATE LAWS. ALL TRASH AND CONSTRUCTION DEBRIS FROM THE SITE SHALL BE DEPOSITED IN THE DUMPSTER. THE SUPERINTENDENT SHALL COORDINATE WITH THE LOCAL UTILITIES TO HAVE THE DUMPSTER EMPTIED AT LEAST TWICE A WEEK AND THE WASTE TAKEN TO AN APPROPRIATE LANDFILL. NO CONSTRUCTION WASTE MATERIALS SHALL BE BURIED ON SITE. THE SUPERINTENDENT SHALL ORGANIZE TRAINING FOR THE EMPLOYEES IN THE PROPER PRACTICES WHEN DEALING WITH WASTE MATERIALS. THE SUPERINTENDENT SHALL BE RESPONSIBLE FOR POSTING AND ENFORCING WASTE MATERIAL PROCEDURES.

HAZARDOUS WASTE - HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN ACCORDANCE WITH ALL LOCAL AND STATE LAWS OR AS DIRECTED BY THE MANUFACTURER. THE SUPERINTENDENT SHALL ORGANIZE THE PROPER TRAINING FOR EMPLOYEES IN THE PROPER PRACTICES WHEN DEALING WITH HAZARDOUS WASTE MATERIALS. THESE PROCEDURES SHALL BE POSTED ON THE SITE. THE PERSON WHO MANAGES THE SITE SHALL BE RESPONSIBLE FOR ENFORCING THE PROCEDURES.

SANITARY WASTE - SANITARY WASTE SHALL BE COLLECTED AND DISPOSED OF IN ACCORDANCE WITH ALL LOCAL AND STATE LAWS. THE SUPERINTENDENT SHALL COORDINATE WITH THE LOCAL UTILITY FOR COLLECTION OF THE SANITARY WASTE AT LEAST THREE TIMES A WEEK TO PREVENT SPILLAGE ONTO THE SITE.

#### OFF-SITE TRACKING

A STABILIZED CONSTRUCTION ENTRANCE SHALL BE PROVIDED TO REDUCE SEDIMENT TRACKING OFFSITE. THE MAJOR ROAD CONNECTED TO THE PROJECT SHALL BE CLEANED ONCE A DAY TO REMOVE ANY EXCESS MUD, DIRT OR ROCK RESULTING FROM CONSTRUCTION TRAFFIC. ALL TRUCKS HAULING MATERIALS OFFSITE SHALL BE COVERED WITH A TARPAULIN

#### **ITEMS REQUIRING POLLUTION PREVENTION**

THE FOLLOWING ITEMS ARE EXPECTED TO BE PRESENT ON THE PROJECT SITE:

-ASPHALT -CONCRETE -FERTILIZERS -METAL PIECES -PETROLEUM BASED PRODUCTS -TAR

-CLEANING SUPPLIES -DETERGENTS -MASONARY BLOCK/BRICKS -PAINT -WOOD

THE FOLLOWING ARE NON-STORM WATER SOURCES THAT WILL BE ENCOUNTERED AT THE SITE AND SHOULD BE DIRECTED TO THE SEDIMENT BASIN PRIOR TO DISCHARGE:

-UNCONTAMINATED GROUNDWATER EXPOSED DURING EXCAVATION -WATER FROM WATER LINE FLUSHING -PAVEMENT WASH WATERS (WHERE NO SPILLS OR LEAKS OF TOXIC OR HAZARDOUS MATERIALS HAVE OCCURRED).

#### SPILL PREVENTION AND CONTROL

THE FOLLOWING ARE THE MATERIAL MANAGEMENT PRACTICES THAT WILL BE USED TO REDUCE THE RISK OF SPILLS OR OTHER ACCIDENTAL EXPOSURE OF MATERIALS AND SUBSTANCES TO STORM WATER RUNOFF.

#### GOOD HOUSEKEEPING

SUPERINTENDENT SHALL INSPECT PROJECT AREA DAILY FOR PROPER STORAGE, USE, AND DISPOSAL OF CONSTRUCTION MATERIALS.

STORE ONLY ENOUGH MATERIAL ON SITE FOR PROJECT COMPLETION.

ALL SUBSTANCES SHOULD BE USED BEFORE DISPOSAL OF CONTAINER.

ALL CONSTRUCTION MATERIALS STORED SHALL BE ORGANIZED AND IN THE PROPER CONTAINER AND IF POSSIBLE, STORED UNDER A ROOF OR PROTECTIVE COVER.

PRODUCTS SHALL NOT BE MIXED UNLESS DIRECTED BY THE MANUFACTURER.

ALL PRODUCTS SHALL BE USED AND DISPOSED OF ACCORDING TO THE MANUFACTURER'S RECOMMENDATIONS.

#### HAZARDOUS PRODUCTS

MATERIALS SHOULD BE KEPT IN ORIGINAL CONTAINER WITH LABELS UNLESS THE ORIGINAL CONTAINERS CANNOT BE RESEALED. IF ORIGINAL CONTAINERS CANNOT BE USED, LABELS AND PRODUCT INFORMATION SHALL BE SAVED.

PROPER DISPOSAL PRACTICES SHALL ALWAYS BE FOLLOWED IN ACCORDANCE WITH MANUFACTURER AND LOCAL/STATE REGULATIONS.

#### PRODUCT SPECIFIC PRACTICES

PETROLEUM PRODUCTS MUST BE STORED IN PROPER CONTAINERS AND CLEARLY LABELED. VEHICLES CONTAINING PETROLEUM PRODUCTS SHALL BE PERIODICALLY INSPECTED FOR LEAKS. PRECAUTIONS SHALL BE TAKEN TO AVOID LEAKAGE OF PETROLEUM PRODUCTS ON SITE.

THE MINIMUM AMOUNT OF FERTILIZER SHALL BE USED AND MIXED INTO THE SOIL IN ORDER TO LIMIT EXPOSURE TO STORM WATER. FERTILIZERS SHALL BE STORED IN A COVERED SHED. THE CONTENTS OF ANY PARTIALLY USED BAGS OF FERTILIZER SHALL BE TRANSFERRED TO A SEALABLE PLASTIC BIN TO AVOID SPILLS.

PAINT CONTAINERS SHALL BE SEALED AND STORED WHEN NOT IN USE. EXCESS PAINT MUST BE DISPOSED OF IN AN APPROVED MANNER.

CONCRETE TRUCKS SHALL NOT BE ALLOWED TO WASH OUT OR DISCHARGE SURPLUS CONCRETE OR DRUM WASH WATER ON THE SITE.

#### SPILL CONTROL PRACTICES

IN ADDITION TO THE GOOD HOUSEKEEPING AND MATERIAL MANAGEMENT PRACTICES DISCUSSED IN THE PREVIOUS SECTIONS OF THIS PLAN, THE FOLLOWING PRACTICES SHALL BE FOLLOWED FOR SPILL PREVENTION AND CLEANUP:

-SPILL CLEANUP INFORMATION SHALL BE POSTED ON SITE TO INFORM EMPLOYEES ABOUT CLEANUP PROCEDURES AND RESOURCES.

-THE FOLLOWING CLEAN-UP EQUIPMENT MUST BE KEPT ON-SITE NEAR THE MATERIAL STORAGE AREA: GLOVES, MOPS, RAGS, BROOMS, DUST PANS, SAND, SAWDUST, LIQUID ABSORBER, GOGGLES, AND TRASH CONTAINERS.

-ALL SPILLS SHALL BE CLEANED UP AS SOON AS POSSIBLE.

-WHEN CLEANING A SPILL, THE AREA SHOULD BE WELL VENTILATED AND THE EMPLOYEE SHALL WEAR PROPER PROTECTIVE COVERING TO PREVENT INJURY.

-TOXIC SPILLS MUST BE REPORTED TO THE PROPER AUTHORITY REGARDLESS OF THE SIZE OF THE SPILL.

-AFTER A SPILL, THE PREVENTION PLAN SHALL BE REVIEWED AND CHANGED TO PREVENT FURTHER SIMILAR SPILLS FROM OCCURRING. THE CAUSE OF THE SPILL, MEASURES TO PREVENT IT, AND HOW TO CLEAN THE SPILL UP SHALL BE RECORDED.

-THE SUPERINTENDENT SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR AND IS RESPONSIBLE FOR THE DAY TO DAY SITE OPERATIONS. THE SUPERINTENDENT ALSO OVERSEES THE SPILL PREVENTION PLAN AND SHALL BE RESPONSIBLE FOR EDUCATING THE EMPLOYEES ABOUT SPILL PREVENTION AND CLEANUP PROCEDURES.

#### MAINTENANCE AND INSPECTION PRACTICES

THE FOLLOWING ARE MAINTENANCE AND INSPECTION PRACTICES THAT SHALL BE COMPLETED BY THE CONTRACTOR:

-ALL SEDIMENT AND EROSION CONTROL METHODS SHALL BE INSPECTED AT LEAST ONCE EVERY SEVEN (7) CALENDAR DAYS AND WITHIN 24 HOURS OF THE END OF A STORM THAT IS 0.50 INCHES OR GREATER BY A QUALIFIED INSPECTOR

-ALL SEDIMENT AND EROSION CONTROL METHODS SHALL BE KEPT IN GOOD CONDITION. REPAIRS MUST BE MADE WITHIN 7 CALENDAR DAYS OF INSPECTION.

-THE SILT FENCE SHALL BE INSPECTED PERIODICALLY FOR HEIGHT OF SEDIMENT AND CONDITION OF FENCE.

-THE SILT FENCE SHALL BE CLEARED OF SEDIMENT WHEN SEDIMENT MEASURES ONE-THIRD THE HEIGHT OF THE FENCE.

-THE SEDIMENT BASINS/DITCHES SHALL BE CHECKED PERIODICALLY FOR DEPTH OF SEDIMENT. THEY SHALL BE CLEANED WHEN SEDIMENT REACHES 10% OF TOTAL CAPACITY AND AFTER CONSTRUCTION IS COMPLETE.

-ALL SEEDING SHALL BE CHECKED FOR PROPER GROWTH AND UNIFORMITY. UNSTABALIZED AREAS SHALL BE RE-SODDED.

-A MAINTENANCE REPORT SHALL BE COMPLETED DAILY AFTER EACH INSPECTION OF THE SEDIMENT AND EROSION CONTROL METHODS. THE REPORTS SHALL BE FILED IN AN ORGANIZED MANNER AND RETAINED ON-SITE DURING CONSTRUCTION. AFTER CONSTRUCTION IS COMPLETED, THE REPORTS SHALL BE SAVED FOR AT LEAST THREE YEARS. THE REPORTS SHALL BE AVAILABLE FOR ANY AGENCY THAT HAS JURISDICTION OVER EROSION CONTROL.

-THE SUPERINTENDENT SHALL ORGANIZE THE TRAINING FOR INSPECTION PROCEDURES AND PROPER EROSION CONTROL METHODS FOR EMPLOYEES THAT COMPLETE INSPECTIONS AND REPORTS.

#### POLLUTION PREVENTION PLAN CERTIFICATION

I CERTIFY UNDER PENALTY OF LAW THAT THIS DOCUMENT AND ALL ATTACHMENTS WERE PREPARED UNDER MY DIRECTION OR SUPERVISION IN ACCORDANCE WITH A SYSTEM DESIGNED TO ASSURE THAT QUALIFIED PERSONNEL PROPERLY GATHERED AND EVALUATED THE INFORMATION SUBMITTED. BASED ON MY INQUIRY OF THE PERSON OR PERSONS WHO MANAGE THE SYSTEM, OR THOSE PERSONS DIRECTLY RESPONSIBLE FOR GATHERING THE INFORMATION, THE INFORMATION SUBMITTED IS, TO THE BEST OF MY KNOWLEDGE AND BELIEF, TRUE, ACCURATE, AND COMPLETE, I AM AWARE THAT THERE ARE SIGNIFICANT PENALTIES FOR SUBMITTING FALSE INFORMATION, INCLUDING THE POSSIBILITY OF FINE AND IMPRISONMENT FOR KNOWING VIOLATIONS.

SIGNED:

DATE:

JAROD C. STUBBS, P.E. FLORIDA REGISTRATION NUMBER: 89387 PROFESSIONAL ENGINEER

#### **CONTRACTOR'S CERTIFICATION**

I CERTIFY UNDER PENALTY OF LAW THAT I UNDERSTAND, SHALL COMPLY WITH, THE TERMS AND CONDITIONS OF THE STATE OF FLORIDA GENERIC PERMIT FOR STORMWATER DISCHARGE FORM LARGE AND SMALL CONSTRUCTION ACTIVITIES AND THIS STORMWATER POLLUTION PREVENTION PLAN PREPARED THEREUNDER.

SIGNATURE AND DATE	NAME AND TITLE, COMPANY / ADDRESS AND TELEPHONE NUMBER	RESPONSIBILITY





							В≺
	NORTH						DATE
<u>NC</u> 1.	OTES: CONTRACTOR TO COORDINATE WITH UTILITY COMPANY FOR DEMOLITION REQUIREMENTS OF						REVISIONS
2.	ELECTRICAL SYSTEMS. CONTRACTOR SHALL UTILIZE BEST MANAGEMENT PRACTICES AS NEEDED TO PREVENT						
.3	SYSTEM POLLUTION DURING TIME OF CONSTRUCTION.						
о. 4.	INSTALL AND MAINTAIN SILT FENCE AT LIMITS OF CONSTRUCTION. SEE DETAIL SHEET C7.0.						No.
5.	PROVIDE SOIL TRACKING PREVENTION DEVICE AT ALL CONSTRUCTION ACCESS POINTS. SEE DETAIL SHEET C7.1.						
6.	UTILIZE PERFORATED SOCK DRAIN (OR EQUIVALENT) IN FRONT OF EXISTING/PROPOSED CURB INLETS ADJACENT TO CONSTRUCTION ACTIVITIES. SEE DETAIL SHEET C7.1.		r D		INC. FL 32801	901	00
7.	INSTALL AND MAINTAIN FILTER FABRIC UNDER GRATES OF EXISTING/PROPOSED INLETS, SEE SHEET C7.1.		C		TES, NDO.		) ) )
8.	CONTRACTOR TO ENSURE ADEQUATE COVER REMAINS OVER ALL EXISTING UTILITIES.				DCIA DRLA	Z 7	-
9.	CONTRACTOR TO VERIFY EXISTING COVER OVER ALL UTILITIES BEFORE START OF CONSTRUCTION AND TO COORDINATE WITH THE ENGINEER OF RECORD PRIOR TO START OF CONSTRUCTION IF DESIGN DOES NOT PROVIDE 36" COVER.	1			ND ASS( 1000. (	898-15 PECINTE	
10.	CONTRACTOR IS TO VERIFY EXISTING SANITARY, STORM, WATER, ELECTRIC, PHONE, CABLE, AND NATURAL GAS SERVICES TO BUILDINGS SCHEDULED FOR DEMOLITION. SERVICES ARE TO BE ISOLATED FROM THE MAIN UTILITY SERVICE CONNECTIONS, AND CAPPED AND/OR REMOVED AS REQUIRED BY THE UTILITY PROVIDER. UTILITY SERVICES ARE TO BE ISOLATED IN A MANNER THAT WILL INSURE THAT ADJACENT PROPERTIES REMAIN CONNECTED WITHOUT EXPERIENCING AN INTERRUPTION OF SERVICE.				AVENUE. SUITE	PHONE: 407-	
11.	THERE MAY BE ON-SITE UNDERGROUND UTILITIES (INCLUDING BUT NOT LIMITED TO IRRIGATION, SANITARY SEWER, POTABLE WATER LINES, NATURAL GAS LINES, FIBER OPTIC, ELECTRIC, TELEPHONE AND CABLE LINES) THAT WERE NOT LOCATED OR IDENTIFIED BY THE PROJECT SURVEYOR. PRIOR TO CONSTRUCTION START, CONTRACTOR SHALL FIELD VERIFY ALL EXISTING ON-SITE UTILITIES.	1			© 2022 k S. ORANGE		
12.	CONTRACTOR TO COORDINATE WITH UTILITY COMPANY FOR DEMOLITION REQUIREMENTS OF ELECTRICAL SYSTEMS.	1			189		
13.	CONTRACTOR SHALL UTILIZE BEST MANAGEMENT PRACTICES AS NEEDED TO PREVENT SYSTEM POLLUTION DURING TIME OF CONSTRUCTION.	NAL	11111 25	ціў Q	EFE	V1.5	
		LICENSED PROFESSIO	S CENSE	JAROD C. STUBBS,	DLORIDA LICENSE NUM	108268 1	<sub>ате:</sub> 05 /04 / 2023
						L L	D CS
		JECT 040		202	<u>5</u>  Ш		Ъ
		PRO DARO	DATE	04/	ED BY	В≺	ED BY
		KHA 140	É	190 / <u>3</u> 0	DESIGN	DRAWN	CHECKE

Ζ

Ζ

O

OLIO

Σ

Ш

 $\square$ 

Ζ

Ο

S

ЧX

Š

Ζ

C

 $\square$ 

Ζ

Ο

C

Ш

Š

0 Ō

SHEET NUMBER

C3.0

Г

Ž

Ś

 $\overline{\times}$ 

Ш

Ĩ ►

S

 $\Box$ 

 $\mathbf{X}$ 

202

-

 $\mathbf{O}$ 

**М** П

ΕĒ

# <u>LEGEND</u> PROPERTY LINE (TYP.) INLET PROTECTION (SEE DETAIL SHEET C7.0) (SEE DETAIL SHEET C7.0)

X

\_\_\_\_\_ FM \_\_\_\_\_

EXISTING TO BE DEMOLISHED -----s ------ EXISTING SEWER MAIN — w — w — EXISTING WATER MAIN EXISTING STORM PIPE ----- GAS----- EXISTING GAS MAIN EXISTING OVERHEAD ELECTRIC LINE \_\_\_\_\_ OHE\_\_\_\_ EXISTING TREE TO REMAIN

EXISTING TREE TO BE REMOVED EXISTING FORCE MAIN

CALL 48 HOURS

**BEFORE YOU DIG** 

IT'S THE LAW!

DIAL 811



Know what's **below.** Call before you di SUNSHINE STATE ONE CALL OF FLORIDA, INC.

- BENCHMARK #1 4"x4" CMON FDOT R/W MONUMENT ELEV = 151.10'NORTHING: 434049.36' EASTING: 2540361.06'



	NORTH	DATE BY
	GRAPHIC SCALE IN FEET 0 5 10 20	REVISIONS
<ol> <li>CONTRACTOR TO COORDINATE WITH ELECTRICAL SYSTEMS.</li> <li>CONTRACTOR SHALL UTILIZE BEST M SYSTEM POLLUTION DURING TIME OF</li> <li>REFER TO SWPPP SHEET C2.0 FOR</li> <li>INSTALL AND MAINTAIN SILT FENCE</li> <li>PROVIDE SOIL TRACKING PREVEN POINTS. SEE DETAIL SHEET C7.1.</li> <li>UTILIZE PERFORATED SOCK DRAI</li> </ol>	UTILITY COMPANY FOR DEMOLITION REQUIREMENTS OF ANAGEMENT PRACTICES AS NEEDED TO PREVENT CONSTRUCTION. ADDITIONAL NOTES AND DETAILS. AT LIMITS OF CONSTRUCTION. SEE DETAIL SHEET C7.0. ITION DEVICE AT ALL CONSTRUCTION ACCESS N (OR EQUIVALENT) IN FRONT OF	2801 No.
<ul> <li>EXISTING/PROPOSED CURB INLET DETAIL SHEET C7.1.</li> <li>INSTALL AND MAINTAIN FILTER FAE INLETS, SEE SHEET C7.1.</li> <li>CONTRACTOR TO ENSURE ADEQU</li> <li>CONTRACTOR TO VERIFY EXISTIN CONSTRUCTION AND TO COORDIN START OF CONSTRUCTION IF DES</li> <li>CONTRACTOR IS TO VERIFY EXIS CABLE, AND NATURAL GAS SER' SERVICES ARE TO BE ISOLATED AND CAPPED AND/OR REMOVED SERVICES ARE TO BE ISOLATED PROPERTIES REMAIN CONNECTED SERVICE.</li> <li>THERE MAY BE ON-SITE UNDER</li> </ul>	TS ADJACENT TO CONSTRUCTION ACTIVITIES. SEE BRIC UNDER GRATES OF EXISTING/PROPOSED JATE COVER REMAINS OVER ALL EXISTING UTILITIES. G COVER OVER ALL UTILITIES BEFORE START OF NATE WITH THE ENGINEER OF RECORD PRIOR TO SIGN DOES NOT PROVIDE 36" COVER. TING SANITARY, STORM, WATER, ELECTRIC, PHONE, VICES TO BUILDINGS SCHEDULED FOR DEMOLITION. FROM THE MAIN UTILITY SERVICE CONNECTIONS, AS REQUIRED BY THE UTILITY PROVIDER. UTILITY O IN A MANNER THAT WILL INSURE THAT ADJACENT O WITHOUT EXPERIENCING AN INTERRUPTION OF GROUND UTILITIES (INCLUDING BUT NOT LIMITED TO	2 KIMLEY-HORN AND ASSOCIATES, INC. GE AVENUE, SUITE 1000, ORLANDO, FL 3 PHONE: 407–898–1511 IMLEY-HORN.COM REGISTRY No. 35106
IRRIGATION, SANITARY SEWER, P FIBER OPTIC, ELECTRIC, TELEPHO OR IDENTIFIED BY THE PROJECT CONTRACTOR SHALL FIELD VERIF 12. CONTRACTOR TO COORDINATE W REQUIREMENTS OF ELECTRICAL S 13. CONTRACTOR SHALL UTILIZE BES PREVENT SYSTEM POLLUTION DU	OTABLE WATER LINES, NATURAL GAS LINES, DNE AND CABLE LINES) THAT WERE NOT LOCATED SURVEYOR. PRIOR TO CONSTRUCTION START, TY ALL EXISTING ON-SITE UTILITIES. ITH UTILITY COMPANY FOR DEMOLITION SYSTEMS. ST MANAGEMENT PRACTICES AS NEEDED TO IRING TIME OF CONSTRUCTION.	ENSED PROFESSIONAL OD CO-STUBBS, P.E. CENS OD CO-STUBBS, P.E. CINE OD CO-STUBBS, P.E. CO CO-STUBBS, P.E. CO CO-STUBBS, P.E. CO-STUBBS, P.E. CO-STUBS, P.E. CO-STUBS, P.E. CO-S
		KHA PROJECT 149880040 DATE 05/04/2022 SCALE AS SHOWN DESIGNED BY EJF DRAWN BY EJF DRAWN BY EJF DRAWN BY UCS DATE:
LEGEND         Image: Constraint of the second sec	PROPERTY LINE (TYP.) INLET PROTECTION (SEE DETAIL SHEET C7.0) TYPE III SILT FENCE (SEE DETAIL SHEET C7.0) EXISTING TO BE DEMOLISHED EXISTING SEWER MAIN EXISTING SEWER MAIN EXISTING WATER MAIN EXISTING STORM PIPE EXISTING GAS MAIN EXISTING OVERHEAD ELECTRIC LINE	EXISTING CONDITIONS & DEMOLITION PLAN
EXAMPLE OF THE OFFICE O	EXISTING TREE TO BE REMOVED   EXISTING FORCE MAIN	CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION OF LAKE CITY FLORIDA

CALL 48 HOURS **BEFORE YOU DIG** IT'S THE LAW! Know what's **below.** DIAL 811 Call before you dig SUNSHINE STATE ONE CALL OF FLORIDA, INC.

SHEET NUMBER

C3.1



Π C RAMP



#### GRAPHIC SCALE IN FEET 0 15 30 60 60

### NOTES:

1. ALL CURB DIMENSIONS ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED. 2. BUILDING DIMENSIONS ARE TO THE OUTSIDE FACE OF BUILDING UNLESS OTHERWISE NOTED.

3. REFER TO ARCHITECTURAL AND STRUCTURAL PLANS TO VERIFY ALL BUILDING DIMENSIONS.

- 4. REFER TO SIGNAGE PLANS FOR MONUMENT SIGN DETAILS.
- 5. SEE MEP PLANS FOR ELECTRICAL DRAWINGS.

6. ALL PROPOSED ON-SITE STRIPING AND PAVEMENT MARKING WILL BE PAINTED UNLESS OTHERWISE NOTED AND IN ACCORDANCE WITH FDOT INDEX 711-001. 7. REFER TO ARCHITECTURAL PLANS FOR PROPOSED TRASH CAN LOCATIONS AND DESIGN.

8. BOLLARDS IN SIDEWALK ADJACENT TO BUILDING SHALL BE COVERED WITH RED PLASTIC COVERS TO BE SUPPLIED BY CONTRACTOR.

9. BOLLARDS UNDER CANOPY SHALL BE COVERED WITH GRAY PLASTIC COVERS TO BE SUPPLIED BY CONTRACTOR (SEE FUEL PUMP DESIGNER PLANS FOR MORE DETAIL). 10. REFER TO ARCHITECTURAL PLANS FOR SITE LIGHTING AND ELECTRICAL PLANS. 11. ALL SIGNAGE AND PAVEMENT MARKINGS SHALL MEET MUTCD AND FDOT STANDARDS.

12. ALL SIGNAGE SHALL MEET THE REQUIREMENTS OF CITY OF LAKE CITY LAND DEVELOPMENT CODE, CHAPTER 7, SEC. 760

SITE DATA:			
PROJECT AREA: FUTURE LAND USE: EXISTING ZONING:	CHI – CON	3.46± ACRES ( ( //////////////////////////////////	150,953 SF) COMMERCIAL ITERCHANGE
EXISTING USE: PROPOSED USE:		UI	NDEVELOPED COMMERCIAL
BUILDING HEIGHT: PROPOSED:		1 ST(	DRY/<35 FT
FAR: EXISTING PERVIOUS A	REA:	99,714.2 SF (2.29	0.0388 AC) (66%)
PROPOSED PERVIOUS	AREA:	34,981 SF (0.803	AC) (23%)
PROPOSED IMPERVIOU BUILDING AREA : (EXISTING BLD	S AREA:	5,863 SF (0.135 /	AC) (3.88%)
ASPHALT/CONCRETI (EXISTING+HSE	E AREA: D EXPANSION)	106401.5 SF (2.44 /	AC) (70.5%)
TOTAL IMPERVIOUS	AREA:	112,264.5 SF (2.58	AC) (65%)
PARKING REQUIRED CONVENIENCE STORE 1 SPACES / 150 S TOTAL REQUIRED P	(WTH GAS STAT F NON-STORAG ARKING	TION) E AREA (5,043 SF)	34 34
PARKING PROVIDED			0
PROPOSED HANDICA PROPOSED REGULAI PROPOSED SEMI TR PROPOSED ON-SITE	R SPACES: UCK SPACES: SPACES:		2 31 9 42
BICYCLE PARKING REQUIRED SPACES: PROVIDED SPACES:			0 4
BUILDING SETBACKS			
SIDE (WEST):		30 FT	83 FT
REAR (NORIH):		30 FT	220 FT
SIDE (FAST):		30 FT	190 FT 107 FT
LANDSCAPE SETBACKS		REQUIRED	PROVIDED
SIDE (WEST):		15 FT	15 FT
FRONT (SOUTH):		13 FT N/A	0 FT
SIDE (EAST):		15 FT	15 FT
LEGEND			
, <u> </u>	PROPERTY LINE	: (TYP.)	
	PROPOSED ASF (SEE DETAIL SF	PHALT PAVEMENT HEET C7.0)	
	PROPOSED CON (SEE DETAIL SI	ICRETE SIDEWALK HEET C7.0)	
а. а б р а	PROPOSED MED (SEE DETAIL SI	DUM DUTY CONCRETE HEET C7.0)	
	PROPOSED HEA (SEE DETAIL SI	VY DUTY CONCRETE HEET C7.0)	

CALL 48 HOURS BEFORE YOU DIG

SUNSHINE STATE ONE CALL OF FLORIDA, INC.

IT'S THE LAW! DIAL 811

**T**OL

Know what's **below.** Call before you dig

0  $\approx$ E O  $\leq$ AN SITE OVERALL Ч М Н П П SION SU Ζ  $\mathbf{X}$ CIRCLE 90 & I EXP ЧX SHEET NUMBER C4.0

VERTICAL DATUM: ELEVATIONS ARE REFERENCED TO PLAT BENCHMARKS AS SHOWN ON THE FACE OF THE PLAT OF GATEWAY CROSSING REPLAT OF LOTS 2 AND 3, PLAT BOOK 9, PAGE 151



	REVISIONS
Kimley       Horn         © 2022 KIMLEY-HORN AND ASSOCIATES, INC.         189 S. ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801         PHONE: 407-898-1511         WMW KIMI EY-HORN COM PECISTEY NO. 35106	
KHA PROJECTLICENSED PROFESSIONAL149880040DATEDATEDATE05/04/2022JAROD COSTBBBS, P.E.SCALEAS SHOWNSCALEAS SHOWNDESIGNED BYEJFDRAWN BYEJFDRAWN BYEJF	CHECKED BY JCS DATE: 05/04/2022
SITE PLAN	
CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION	CITY OF LAKE CITY FLORIDA

INTE フ S Π  $\sim$ S O RAMP

<u>LEGEND</u> , <del>1</del> ₽:·· ₹ 

VERTICAL DATUM:

LOTS 2 AND 3, PLAT

BOOK 9, PAGE 151

ELEVATIONS ARE REFERENCED TO PLAT BENCHMARKS AS SHOWN ON THE FACE OF THE PLAT OF GATEWAY CROSSING REPLAT OF

PROPERTY LINE (TYP.)

PROPOSED CONCRETE SIDEWALK (SEE DETAIL SHEET C7.0)

PROPOSED MEDIUM DUTY CONCRETE

PROPOSED HEAVY DUTY CONCRETE

(SEE DETAIL SHEET C7.0)

(SEE DETAIL SHEET C7.0)

CALL 48 HOURS **BEFORE YOU DIG** 

Know what's **below**.

Call before you dir

C4.1

IT'S THE LAW!

DIAL 811

SUNSHINE STATE ONE CALL OF FLORIDA, INC.

(SEE DETAIL SHEET C7.0)

PROPOSED ASPHALT PAVEMENT

FUEL TANK RISER & CONCRETE

NOTES:

OTHERWISE NOTED.

DIMENSIONS.

AND DESIGN.

DETAIL).

PLANS.

STANDARDS.

NOTED.

1. ALL CURB DIMENSIONS ARE TO THE FACE OF CURB UNLESS OTHERWISE

2. BUILDING DIMENSIONS ARE TO THE OUTSIDE FACE OF BUILDING UNLESS

4. REFER TO SIGNAGE PLANS FOR MONUMENT SIGN DETAILS.

RED PLASTIC COVERS TO BE SUPPLIED BY CONTRACTOR.

5. SEE MEP PLANS FOR ELECTRICAL DRAWINGS.

3. REFER TO ARCHITECTURAL AND STRUCTURAL PLANS TO VERIFY ALL BUILDING

6. ALL PROPOSED ON-SITE STRIPING AND PAVEMENT MARKING WILL BE PAINTED UNLESS OTHERWISE NOTED AND IN ACCORDANCE WITH FDOT INDEX 711-001. 7. REFER TO ARCHITECTURAL PLANS FOR PROPOSED TRASH CAN LOCATIONS

8. BOLLARDS IN SIDEWALK ADJACENT TO BUILDING SHALL BE COVERED WITH

9. BOLLARDS UNDER CANOPY SHALL BE COVERED WITH GRAY PLASTIC COVERS TO BE SUPPLIED BY CONTRACTOR (SEE FUEL PUMP DESIGNER PLANS FOR MORE

10. REFER TO ARCHITECTURAL PLANS FOR SITE LIGHTING AND ELECTRICAL

11. ALL SIGNAGE AND PAVEMENT MARKINGS SHALL MEET MUTCD AND FDOT

12. ALL SIGNAGE SHALL MEET THE REQUIREMENTS OF CITY OF LAKE CITY LAND DEVELOPMENT CODE, CHAPTER 7, SEC. 760

GRAPHIC SCALE IN FEET



	GRAPHIC SCALE IN FEET	DATE BY
NOTES: 1. ALL CURB DIMENSIONS ARE TO 2. BUILDING DIMENSIONS ARE TO NOTED. 3. ALL PROPOSED ON-SITE STRIF UNLESS OTHERWISE NOTED AND IN 4. ALL SIGNAGE AND PAVEMENT STANDARDS.	THE FACE OF CURB UNLESS OTHERWISE NOTED. THE OUTSIDE FACE OF BUILDING UNLESS OTHERWISE PING AND PAVEMENT MARKING WILL BE PAINTED ACCORDANCE WITH FDOT INDEX 711-001. MARKINGS SHALL MEET MUTCD AND FDOT	No. REVISIONS
		<b>Kimley Morn</b> © 2022 KIMLEY-HORN AND ASSOCIATES, INC. 189 S. ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801 PHONE: 407–898–1511 WWW.KIMLEY-HORN.COM REGISTRY No. 35106
		KHA PROJECTLICENSED PROFESSIONAL149880040DATEDATEDATEDATEDATEC5/04/2022JAROD C: STBBBS, P.E.SCALE AS SHOWNCRIDA LICENSE NUMBERDESIGNED BYEJFDESIGNED BYEJFDESIGNED BYEJFDESIGNED BYEJFDESIGNED BYEJFDRAWN BYEJFCHECKED BYJCSDATE:05/04/Y2022
LEGEND	PROPERTY LINE (TYP.) PROPOSED ASPHALT PAVEMENT (SEE DETAIL SHEET C7.0)	INTERSECTION MODIFICATION PLAN
VERTICAL DATUM: ELEVATIONS ARE REFERENCED TO PLAT BENCHMARKS AS SHOWN ON THE EACE OF THE DATE OF	<text><text><text><text><text></text></text></text></text></text>	CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION ITY OF LAKE CITY FLORIDA
THE FACE OF THE PLAT OF GATEWAY CROSSING REPLAT OF LOTS 2 AND 3, PLAT BOOK 9, PAGE 151	DIAL 811 Know what's below. Call before you dig. SUNSHINE STATE ONE CALL OF FLORIDA, INC.	SHEET NUMBER

a b b a b b b b b b b	
a baran	

# VERTICAL DATUM:

ELEVATIONS ARE REFERENCED TO PLAT BENCHMARKS AS SHOWN ON THE FACE OF THE PLAT OF GATEWAY CROSSING REPLAT OF LOTS 2 AND 3, PLAT BOOK 9, PAGE 151



<u>7.50</u>	42.50	
3.00 12.50		
WB-50	fack	
Tractor Width Trailer Width Tractor Track Trailer Track	: 8.00 Lock to Lock Time : 6.0 : 8.50 Steering Angle : 17.7 : 8.00 Articulating Angle : 70.0 : 8.50	
ſ	CALL 48 HOURS BEFORE YOU DIG	
	IT'S THE LAW! DIAL 811 Know what's below. Call before you dig.	
	SUNSHINE STATE ONE CALL OF FLORIDA, INC.	

DATE BY	
REVISIONS	
<b>Kimley Morn</b> © 2022 KIMLEY-HORN AND ASSOCIATES, INC. 189 S. ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801 PHONE: 407–898–1511 WWW.KIMLEY-HORN.COM REGISTRY No. 35106	
CT     LICENSED PROFESSIONAL       0     CENSIONAL       22     JAROD CO.STUBBS, P.E.       OWN     CENSIONAL       CURIDA LICENSE NUMBER     CENSIONAL       CURIDA LICENSE NUMBER     CENSIONAL       CURIDA LICENSE NUMBER     COLINAL       CURIDA LICENSE NUMBER     COLINAL       CURIDA LICENSE NUMBER     COLINAL       CURIDA LICENSE NUMBER     COLINAL       CURIDA LICENSE NUMBER     COLINAL	
KHA PROJEC 14988004 DATE DATE SALE AS SH( SIGNED BY SIGNED BY AWN BY HECKED BY	
TRUCK TURNING MOVEMENTS	
CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION OTY OF LAKE OTY A FLORIDA	
SHEET NUMBER C4.3	1

GRAPHIC SCALE IN FEET 0 15 30 60



GRAPHIC SCALE IN FEET	REVISIONS DATE BY
	Simpley       Horn         © 2022 KIMLEY-HORN AND ASSOCIATES, INC.         189 S. ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801         PHONE: 407-898-1511         WWW.KIMLEY-HORN.COM REGISTRY No. 35106
	KHA PROJECT 149880040LICENSED PROFESSIONAL 149880040DATE DATEDATE CEN05/04/2022 SCALE AS SHOWNJAROD C:-STBBBS, P.E. CLORID BY EJF DATE DATEDESIGNED BY EJF DRAWN BY EJF CHECKED BY JCSJAROD C:-STBBBS, P.E. ADD C:-STBBBS, P.E. DATE DATE
	TRUCK TURNING MOVEMENTS
15.00       53.00         15.00       45.50         10.00       0.00	CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION ITY OF LAKE CITY A FLORIDA
DIAL 811 Know what's below. Call before you dig. SUNSHINE STATE ONE CALL OF FLORIDA, INC.	SHEET NUMBER

4.00 19.50



EVISIONS DATE BY	
32801 No. RI	
<b>Kimley Morian Content</b> © 2022 KIMLEY-HORN AND ASSOCIATES, INC. 189 S. ORANGE AVENUE, SUITE 1000, ORLANDO, FL 3 PHONE: 407–898–1511 WWW.KIMLEY-HORN.COM REGISTRY No. 35106	
KHA PROJECT 149880040LICENSED PROFESSIONAL 149880040DATE DATE 05/04/2022DATE CEND5/04/2022 DESIGNED BYLICENSE NUMBER CLORIDA LICENSE NUMBER CLORIDA LICENSE NUMBER COS/04/2022KHA PROJECT DATELICENSED PROFESSIONAL CENDESIGNED BYEJF CLORIDA LICENSE NUMBER COS/04/2022DRAWN BYEJF DATE: 05/04/2022	
C4.5 - TRUCK TURNING MOVEMENTS	
CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION DITY OF LAKE CITY FLORIDA	
SHEET NUMBER C4.5	



STOR
STRUCTURE NAME:
D—1
D-2
D-3
D-4



0  $\approx$ D Ζ C Ň ם 4 C M AN C \_\_\_\_ 4 C DR NIN N  $\square$ Ζ Δ  $\bigcirc$ σ  $\geq$ Ζ SIO Ш S  $\mathbf{X}$ Ω Ш Š Ш C C CIR SHEET NUMBER C5.0

#### NOTES:

147.48

146.98

147.75

147.25

147.45 🔳

146.95

-146.43

- 1. ALL STORM PIPES LABELED "HDPE" SHALL BE ADS HIGH PERFORMANCE (HP) POLYPROPYLENE STORM SEWER PIPE.
- 2. CURB RAMPS ALONG PUBLIC STREETS AND IN THE PUBLIC RIGHT-OF-WAY SHALL BE CONSTRUCTED IN ACCORDANCE WITH FDOT STANDARDS AND SPECIFICAITONS.
- 3. PRIVATE CURB RAMPS ON THE SITE (I.E. OUTSIDE PUBLIC STREET RIGHT-OF-WAY) SHALL CONFORM TO ADA STANDARDS AND SHALL HAVE A DETECTABLE WÁRNING SURFACE THAT IS FULL WIDTH AND FULL DEPTH OF THE CURB RAMP, NOT INCLUDING FLARES.
- 4. ALL ACCESSIBLE ROUTES, GENERAL SITE AND BUILDING ELEMENTS, RAMPS, CURB RAMPS, STRIPING, AND PAVEMENT MARKINGS SHALL CONFORM TO ADA STANDARDS FOR ACCESSIBLE DESIGN, LATEST EDITION.
- 5. BEFORE PLACING PAVEMENT, CONTRACTOR SHALL VERIFY THAT SUITABLE ACCESSIBLE PEDESTRIAN ROUTES (PER ADA AND FHA) EXIST TO AND FROM EVERY DOOR AND ALONG SIDEWALKS, ACCESSIBLE PARKING SPACES, ACCESS AISLES, AND ACCESSIBLE ROUTES. IN NO CASE SHALL AN ACCESSIBLE RAMP SLOPE EXCEED 1 VERTICAL TO 12 HORIZONTAL. IN NO CASE SHALL SIDEWALK CROSS SLOPE EXCEED 2.0 PERCENT. IN NO CASE SHALL LONGITUDINAL SIDEWALK SLOPE EXCEED 5.0 PERCENT. ACCESSIBLE PARKING SPACES AND ACCESS AISLES SHALL NOT EXCEED 2.0 PERCENT SLOPE IN ANY DIRECTION.
- CONTRACTOR SHALL TAKE FIELD SLOPE MEASUREMENTS ON FINISHED 6. SUBGRADE AND FORM BOARDS PRIOR TO PLACING PAVEMENT TO VERIFY THAT ADA SLOPE REQUIREMENTS ARE PROVIDED. CONTRACTOR SHALL CONTACT ENGINEER PRIOR TO PAVING IF ANY EXCESSIVE SLOPES ARE ENCOUNTERED. NO CONTRACTOR CHANGE ORDERS WILL BE ACCEPTED FOR ADA SLOPE COMPLIANCE ISSUES.
- ALL PEDESTRIAN SIDEWALKS, PATHWAYS, AND CROSSWALKS SHALL BE CONSTRUCTED NOT TO EXCEED MAX. 2.0% CROSS SLOPE, MAX. 5.0% RUNNING SLOPE.
- 8. ALL HANDICAP ACCESSIBLE PARKING SPACES SHALL BE CONSTRUCTED NOT TO EXCEED MAX. 2.0% CROSS SLOPE IN ALL DIRECTIONS.
- 9. PROPOSED GRADES TO MATCH EXISTING ELEVATIONS AT PROPERTY LINE
- 10. CONTRACTOR TO FIELD VERIFY ELEVATIONS AT ALL EXISTING SIDEWALK AND ROAD CONNECTION POINTS WITH ENGINEER OF RECORD BEFORE CONSTRUCTION OF ANY IMPROVEMENTS.
- 11. FOR EROSION CONTROL NOTES REFER TO SHEET C2.0.
- 12. ALL DRAINAGE PIPES SHALL BE FILTER FABRIC WRAPPED PER FDOT STANDARD PLAN #430-001.
- 13. CONTRACTOR TO ENSURE ADEQUATE COVER REMAINS OVER ALL EXISTING UTILITIES.
- 14. CONTRACTOR TO VERIFY EXISTING COVER OVER ALL UTILITIES BEFORE START OF CONSTRUCTION AND TO COORDINATE WITH THE ENGINEER OF RECORD PRIOR TO START OF CONSTRUCTION IF DESIGN DOES NOT PROVIDE 36" COVER.
- 15. ALL EXISTING VALVES, BOXES, MANHOLE LIDS, COVERS, AND SIMILAR APPURTENANCES MUST BE ADJUSTED ACCORDINGLY TO MATCH FINISHED GRADE.
- 16. ALL PAVEMENT MUST BE SOURCED FROM AN FDOT APPROVED PLANT.

#### LEGEND

	PROPERTY LINE
	PROPOSED STORM PIPE
$\bigcirc$	PROPOSED STORM MANHOLE
	PROPOSED STORM INLET
×XX.XX XX.XX	PROPOSED ELEV. TOP OF CURB PROPOSED ELEV. BOTT. OF CURB
<u> </u>	PROPOSED SPOT ELEVATION
	EXISTING STORM PIPE
$\bigcirc$	EXISTING STORM MANHOLE
	EXISTING STORM INLET

CALL 48 HOURS

**BEFORE YOU DIG** 

Know what's **below.** 

SUNSHINE STATE ONE CALL OF FLORIDA, INC.

Call before you di

IT'S THE LAW!

DIAL 811

VERTICAL DATUM: ELEVATIONS ARE REFERENCED TO PLAT BENCHMARKS AS SHOWN ON THE FACE OF THE PLAT OF GATEWAY CROSSING REPLAT OF LOTS 2 AND 3, PLAT BOOK 9, PAGE 151









GRAPHIC SCALE IN FEET

10 20

### NOTES:

- ALL ONSITE UTILITIES SHALL BE PRIVATELY OWNED AND MAINTAINED.
- MAINTAIN A MINIMUM OF 3-FT OF COVER OVER ALL PROPOSED WATER LINES 2. AND 4-FT MINIMUM OF COVER FOR WASTEWATER LINES.
- REFER TO ADDITIONAL UTILITY NOTES AND DETAILS ON SHEETS C1.0 AND C7.0.
- ALL WATER MAINS, AND DOMESTIC LINES SHALL BE CONSTRUCTED IN ACCORDANCE WITH CITY OF LAKE CITY UTILITY STANDARDS.
- BEDDING AND BACKFILL SHALL BE INSTALLED PER CITY OF LAKE CITY. GRANULAR 5. BACKFILL IS REQUIRED UNDER PAVEMENT AND WITHIN 5 FEET OF PAVEMENT.
- ALL SANITARY SEWER LINES SHALL BE GREEN C-900 PVC MEETING, ASTM D-3034 SDR 26.
- 7. CONTRACTOR SHALL COORDINATE ANY DISRUPTIONS TO EXISTING UTILITY SERVICES WITH ADJACENT PROPERTY OWNERS.
- ALL ELECTRIC AND TELEPHONE EXTENSIONS INCLUDING SERVICE LINES SHALL BE CONSTRUCTED TO THE APPROPRIATE UTILITY COMPANY SPECIFICATIONS. ALL 8. UTILITY DISCONNECTIONS SHALL BE COORDINATED WITH THE DESIGNATED UTILITY COMPANIES.
- 9. CONTRACTOR TO CALL "SUNSHINE STATE ONE CALL OF FLORIDA" (1-800-432-4770) TO COORDINATE FIELD LOCATIONS OF EXISTING UNDERGROUND UTILITIES BEFORE ORDERING MATERIALS OR COMMENCING CONSTRUCTION. NOTIFY ENGINEER OF ANY DISCREPANCIES IMMEDIATELY.
- PRIOR TO THE CONSTRUCTION OF OR CONNECTION TO ANY STORM DRAIN, 10. SANITARY SEWER. WATER MAIN OR ANY OTHER UTILITIES, THE CONTRACTOR SHALL EXCAVATE, VERIFY AND CALCULATE ALL POINTS OF CONNECTION AND ALL UTILITY CROSSINGS AND INFORM THE ENGINEER AND THE OWNER/ DEVELOPER OF ANY CONFLICT OR REQUIRED DEVIATIONS FROM THE PLAN. NOTIFICATION SHALL BE MADE A MINIMUM OF 72 HOURS PRIOR TO CONSTRUCTION. THE ENGINEER AND ITS CLIENTS SHALL BE HELD HARMLESS IN THE EVENT THAT THE CONTRACTOR FAILS TO MAKE SUCH NOTIFICATION. CITY OF LAKE CITY SHALL BE NOTIFIED OF ANY AND ALL CHANGES TO THE DESIGN PLANS.
- 11. CONTRACTOR SHALL COMPLY COMPLETELY WITH THE LATEST STANDARDS OF OSHA DIRECTIVES OR ANY OTHER AGENCY HAVING JURISDICTION FOR EXCAVATION AND TRENCHING PROCEDURES. THE CONTRACTOR SHALL USE SUPPORT SYSTEMS, SLOPING, BENCHING AND OTHER MEANS OF PROTECTION. THIS IS TO INCLUDE, BUT NOT LIMITED FOR ACCESS AND EGRESS FROM ALL EXCAVATION AND TRENCHING. CONTRACTOR IS RESPONSIBLE FOR COMPLYING WITH PERFORMANCE CRITERIA AS REQUIRED BY OSHA.
- 12. CONTRACTOR TO AVOID DISRUPTION OF ANY ADJACENT TENANT'S TRAFFIC OPERATIONS, TO THE MAXIMUM EXTENT, DURING INSTALLATION OF UTILITIES. IF TRAFFIC IS TO BE OBSTRUCTED CONTRACTOR SHALL COORDINATE TRAFFIC CONTROL PLAN WITH THE LOCAL MUNICIPALITY.
- 13. ALL DIMENSIONS ARE TO CENTERLINE OF PIPE OR CENTER OF MANHOLE UNLESS NOTED OTHERWISE.
- 14. SEE PLUMBING PLANS FOR EXACT UTILITY CONNECTION LOCATIONS AT BUILDING. 15. LIGHT POLES SHOWN FOR COORDINATION PURPOSES ONLY AND DO NOT REPRESENT ACTUAL SIZE. SEE SITE LIGHTING PLANS BY OTHERS FOR MORE INFORMATION.
- 16. ELECTRIC, TELEPHONE, AND OTHER DRY UTILITIES SHALL BE PLACED WITHIN CONDUIT, MEETING PRIVATE UTILITY STANDARDS, WHEN ROUTE CROSSES PAVED DRIVES AND PARKING AREAS.
- 17. ALL UNDERGROUND WATER MAINS AND HYDRANTS SHALL BE INSTALLED. COMPLETED AND IN SERVICE PRIOR TO ANY COMBUSTIBLES BEING BROUGHT ONSITE.
- 18. CONTRACTOR TO ENSURE ADEQUATE COVER REMAINS OVER ALL EXISTING UTILITIES.
- 19. CONTRACTOR TO VERIFY EXISTING COVER OVER ALL UTILITIES BEFORE START OF CONSTRUCTION AND TO COORDINATE WITH THE ENGINEER OF RECORD PRIOR TO START OF CONSTRUCTION IF DESIGN DOES NOT PROVIDE 36" COVER.
- 20. ALL ELECTRIC, CABLE, AND TELECOMMUNICATION UTILITIES FOR BUILDING SERVICE TO BE INSTALLED UNDERGROUND.



	PROPERTY BOUNDARY
	PROPOSED STORM PIPE
s	PROPOSED SANITARY SEWER LINE
— w — w —	EXISTING WATER LINE
S	EXISTING SANITARY SEWER LINE
— w — w —	EXISTING WATER LINE
	EXISTING STORM PIPE
G	EXISTING GAS LINE
OHE	EXISTING OVERHEAD ELECTRIC LINE
FM	EXISTING FORCE MAIN
$\bigcirc$	EXISTING STORM MANHOLE
	EXISTING SANITARY SEWER MANHOLE
X	PROPOSED FIRE HYDRANT

VERTICAL DATUM: ELEVATIONS ARE REFERENCED TO PLAT BENCHMARKS AS SHOWN ON THE FACE OF THE PLAT OF GATEWAY CROSSING REPLAT OF LOTS 2 AND 3, PLAT BOOK 9, PAGE 151



Z 4 

0

 $\otimes$ 

**D** 







REMAINING BACKFILL PLACED AND COMPACTED PER APPROPRIATE SPECIFICATIONS 2" MAX. SIZE.\* GRANULAR BACKFILL PLACED AND COMPACTED TO MIN. 100% OF MAX. DRY DENSITY. ½"MAX. SIZE

> Horn       > 151         000, ORLANDO, FL 32801       98-1511	REVISIONS DATE BY DATE BY		
KHA PROJECT       LICENSED PROFESSIONAL         149880040       DATE         DATE       DATE         05/04/2022       JAROD CI-STUBBS, P.E.         SCALE AS SHOWN       SCALE AS SHOWN         DESIGNED BY       EJORIDA IGENSE NUMBER         DESIGNED BY       EJORIDA IGENSE NUMBER         DRAWN BY       EJORIDA IGENSE NUMBER         DRAWN BY       EJORIDA IGENSE NUMBER	CHECKED BY JCS DATE: 05/04/2022		
GENERAL CONSTRUCTION DETAILS			
CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION	CITY OF LAKE CITY FLORIDA		



2010 FDOT Design Standards	Last Revision	Sheet No.
SOIL TRACKING PREVENTION DEVICE	07/01/07	1 of 1
TYPEA	106	



# SOCK DRAIN INLET SEDIMENT FILTER

NTS NOTE: THE PERFORATED PIPE MUST EXTEND AT LEAST 1' BEYOND THE CURB OPENING ON EACH SIDE AND BE ANCHORED WITH GRAVEL BAGS, OR SIMILAR, ON EACH END. A SPACER MUST BE PROVIDED FOR BETWEEN THE INLET OPENING AND THE PIPE TO ALLOW FOR OVERFLOW, PREVENT FLOODING AND TO PREVENT THE PIPE FROM FALLING INTO THE INLET.



KHA PROJECT LICENSED PROFESSIONAL 149880040	/ OF LAKE CITY 05/04/2022 NUMEV >> HOI	SCALE AS SHOWN	NUARUUEIAILS DESIGNED BY EJF 189 S. ORANGE AVENUE, SUITE 1000, ORLANDO, F	DRAWN BY EJF WWW KIMI FY-HORN COM REGISTRY No. 351	CHECKED BY JCS DATE:
					CITY OF LAKE CITY FLORIDA



## Circle K – Lake City, FL

Traffic Impact Analysis

March 2022





# TRAFFIC IMPACT ANALYSIS

# Circle K – US 90 & Centurion Court

Lake City, FL

Prepared for:

Circle K

Prepared by:

Kimley-Horn and Associates, Inc.



Vincent Spahr, P.E. Florida Registration Number 88747 Kimley-Horn and Associates, Inc. 800 SW 2<sup>nd</sup> Avenue, Suite 100 Gainesville, Florida 32601 Registry 35106

This document has been digitally signed and sealed by Vincent Spahr, P.E. on the date adjacent to the seal.

Vincent E Spahr 2022.03.18 09:03:21 -04'00'

Printed copies of this document are not considered signed and sealed and the signature must be verified on any electronic copies.

March 2022

©Kimley-Horn and Associates, Inc. 2022 K:\ORL\_Civil\149880040-Circle K US90 & I75\TPTO\04\_Doc\Circle K I75 US90 TIA\_2022-03-18.docx

#### Table of Contents

1.0	INTRODUCTION1
2.0	EXISTING CONDITIONS ANALYSIS
2.1	Existing Traffic Data3
2.2	Existing Intersection Conditions3
3.0	PROJECT DEVELOPMENT
3.1	Site Access
3.2	Trip Generation5
3.3	Trip Distribution6
3.4	Trip Assignment
4.0	BACKGROUND CONDITIONS ANALYSIS – YEAR 2023
4.1	Historical Traffic Growth9
4.2	Background Traffic9
4.3	Background Intersection Analysis9
5.0	BUILDOUT CONDITIONS ANALYSIS – YEAR 2023 11
5.1	Buildout Traffic
5.2	Buildout Intersection Analysis11
6.0	CONCLUSION

#### Figures

Figure 1: Project Location and Study Area	2
Figure 2: Existing (2021) Intersection Volumes	4
Figure 3: Project Trip Distribution	7
Figure 4: Project Trip Assignment	8
Figure 5: Background Intersection Volumes1	0
Figure 6: Buildout Intersection Volumes1	2

#### Tables

Table 1: Existing Intersection Conditions	3
Table 2: Existing Site Trip Generation Comparison	5
Table 3: Trip Generation Summary	6
Table 4: Background Intersection Conditions	9
Table 5: Buildout Intersection Conditions    1	1

#### Appendices

- Appendix A: Conceptual Site Plan
- Appendix B: Traffic Data
- Appendix C: Intersection Volume Development Worksheets
- Appendix D: Synchro Output Reports
- Appendix E: Trip Generation Calculations
- Appendix F: FDOT *Trend* Worksheet

#### 1.0 INTRODUCTION

Kimley-Horn has been retained by Circle K to analyze and document the traffic impacts associated with the expansion of a gas station and Circle K convenience market on the northeast quadrant of the intersection of US Highway 90 (US 90) and Centurion Court/SW Florida Gateway Drive in Lake City, Florida.

There is an existing 4,968 square-foot convenience market with 24 vehicle fueling positions (VFP) on the site. The project location is shown in Figure 1.

The applicant is proposing to add a 900-square foot expansion to the convenience market and 3 vehicle fueling positions designed for diesel trucks. The conceptual site plan is provided in Appendix A.

The study area for this traffic impact analysis includes the project driveways and the signalized intersection of US 90 and Centurion Court/SW Florida Gateway Drive, as shown in Figure 1.

35


## 2.0 EXISTING CONDITIONS ANALYSIS

### 2.1 EXISTING TRAFFIC DATA

Turning movement counts (TMCs) were collected at the study intersection on Thursday, September 2, 2021 during the AM (7:00AM – 9:00 AM) and PM (4:00PM – 6:00PM) peak periods. Raw turning movement counts are provided in Appendix B.

Turning movement volumes were adjusted using the peak season conversion factor (PSCF) from the Florida Department of Transportation (FDOT) Florida Traffic Online (FTO). Seasonal factor data is included in Appendix B. Existing signal timings were provided by Lake City staff for use in the analysis, signal timing worksheets are included in Appendix B.

Figure 2 illustrates turning movement volumes for existing peak season conditions at the study intersection. The intersection volume development worksheet can be found in Appendix C.

### 2.2 EXISTING INTERSECTION CONDITIONS

Intersection capacity analyses were performed for existing (2021) conditions using the operational analysis procedures outlined in the latest *Highway Capacity Manual*, 6<sup>th</sup> Edition (HCM 6). Specifically, Synchro (v11) software was used to evaluate existing operational conditions at study area intersections by reporting delay, level of service (LOS), volume-to-capacity (v/c) ratios, and the 95<sup>th</sup> percentile queue for each movement. Table 1 summarizes the operational analyses for the existing AM and PM peak hour conditions at the study intersection. Synchro outputs are provided in Appendix D.

			AM F	Peak Hour		PM Peak Hour					
		Delay (sec/veh)	LOS	v/c Ratio	95th percentile queue (veh)	Delay (sec/veh)	LOS	v/c Ratio	95th percentile queue (veh)		
	Overall Intersection	13.2	в	-	-	13.2	в	-	-		
	Eastbound	11.5	В	-	-	9.6	Α	-	-		
	EBL	5.5	А	0.10	0.5	6.9	А	0.08	0.3		
	EBT	11.7	В	0.58	14.6	9.7 A		0.48	12.5		
	EBT/R	11.7	В	0.58	15.2	9.6	А	0.48	12.9		
	Westbound	7.7	Α	-	-	8.7	Α	-	-		
US 90	WBL	7.9	Α	0.16	0.5	6.7	Α	0.24	1.1		
&	WBT	7.9	А	0.38	8.0	9.0	А	0.54	13.7		
Centurion Court	WBR	5.8	А	0.06	0.9	5.4	А	0.09	1.5		
	Northbound	55.6	Е	-	-	65.1	Е	-	-		
	NBL	54.9	D	0.06	0.6	66.7	Е	0.28	2.9		
	NBT/R	55.7	Е	0.42	4.1	64.0	E	0.38	4.2		
	Southbound	58.8	E	-	-	68.1	E	-	-		
	SBL	61.9	Е	0.45	3.8	71.6	Е	0.51	5.0		
	SBT/R	53.6	D	0.23	2.1	62.7	Е	0.27	3.0		

### Table 1: Existing Intersection Conditions

The intersection of US 90 and Centurion Court operates with LOS B during existing (2021) AM peak hour and PM peak hour conditions. All movements operate with v/c ratios less than 1.00 under existing (2021) AM and PM peak hour conditions. The northbound and southbound approaches operate with LOS E during the AM and PM peak hour due to the prioritization of green time for the mainline US 90 movements.



### 3.0 PROJECT DEVELOPMENT

The existing site currently has 24 VFPs and a 4,968-square foot Circle K convenience store. The proposed expansion will add approximately 900-square feet to the existing convenience market and 3 VFPs north of the existing site. The latest industry standards were referenced to evaluate the amount of new external trips to be generated by the site at buildout.

### 3.1 SITE ACCESS

Access to the site is proposed via two existing driveways and one new driveway along Centurion Court, as shown in the site plan provided in Appendix A.

### 3.2 TRIP GENERATION

Trip generation and pass-by rates for the proposed development were calculated using the 11<sup>th</sup> Edition of the Institute of Transportation Engineers' (ITE) *Trip Generation Manual*. Land Use Code (LUC) 945 (Gasoline Station with Convenience Market) was used to calculate the trip generation potential for the existing and proposed development.

The trip generation potential of the existing Circle K convenience store and gas station was compared to observed traffic volumes on Centurion Court north of US 90. Table 2 summarizes the comparison of the calculated trip generation potential of the existing development and the observed peak hour volumes on Centurion Court.

	ļ	AM Peak H	our	PM Peak Hour			
	Total	In (NB)	Out (SB)	Total	In (NB)	Out (SB)	
ITE Trip Generation Manual	649	325	324	546	273	273	
Observed Peak Season Traffic	201	106	95	220	115	105	

### Table 2: Existing Site Trip Generation Comparison

Since the existing AM and PM peak hour traffic volumes were significantly less than the trip generation potential of the existing development, the trip generation calculations for the proposed expansion to the convenience store and gas station were adjusted proportionately to reflect actual conditions anticipated at the site under buildout conditions.

Table 3 provides the AM peak hour, and PM peak hour trip generation calculations for the proposed expansion and the adjustment applied based on the existing trip generation comparison. A factor of 0.31 (201/649) was applied to the AM peak hour trip generation calculations and a factor of 0.40 (220/546) was applied to the PM peak hour trip generation calculations in accordance with the comparison illustrated in Table 2.

As summarized in Table 3, the proposed expansion is anticipated to generate 16 net new AM peak hour trips (8 inbound and 8 outbound), and 18 net new PM peak hour trips (9 inbound and 9 outbound) to the external roadway network at buildout. In addition, the proposed expansion is anticipated to generate 48 AM peak hour pass-by trips (24 inbound and 24 outbound), and 54 PM peak hour pass-by trips (27 inbound

and 27 outbound). A detailed table including all trip generation calculations and adjustments is provided in Appendix E.

	A	AM Peak H	our	PM Peak Hour				
	Total	In (NB)	Out (SB)	Total	In (NB)	Out (SB)		
ITE Trip Generation Manual (Net New)	50	25	25	46	23	23		
ITE Trip Generation Manual (Pass-by)	204	102	102	180	90	90		
Adjustment Factor		0.31			0.40			
Adjusted Net New Trips	16	8	8	18	9	9		
Adjusted Pass-by Trips	48	24	24	54	27	27		

Table 3: Trip Generation Summary

### 3.3 TRIP DISTRIBUTION

The project's trip distribution was developed based on observed traffic patterns within the study area roadway network and engineering judgement. Figure 3 displays the anticipated trip distribution for the proposed Circle K gas station expansion at buildout.

### 3.4 TRIP ASSIGNMENT

Site distribution percentages were used to assign anticipated project trips to the study area intersection and driveways. Figure 4 shows the anticipated AM and PM peak hour project movements at the study area intersection and project driveways.





## 4.0 BACKGROUND CONDITIONS ANALYSIS – YEAR 2023

### 4.1 HISTORICAL TRAFFIC GROWTH

A historical traffic growth rate was calculated based upon the nearest historical Annual Average Daily Traffic (AADT) data available from FTO. A 2.11% annual historical growth rate was calculated **based on the average traffic growth exhibited over the past five (5) years** from an FDOT count station located east of the project site on US 90. The growth trend worksheet can be found in Appendix F.

### 4.2 BACKGROUND TRAFFIC

Traffic conditions were evaluated for year 2023 background conditions prior to the addition of project traffic. Background volumes at study area intersections were derived by applying 2.11% annual growth to existing (2021) traffic counts. Figure 5 illustrates AM peak hour and PM peak hour turning movement volumes for background conditions at the study intersection. The intersection volume development worksheet can be found in Appendix C.

### 4.3 BACKGROUND INTERSECTION ANALYSIS

Intersection operational analyses were performed for 2023 background conditions in the AM and PM peak hours using procedures outlined in the *Highway Capacity Manual 6* with *Synchro* (v11) software. Table 4 summarizes the operational analyses for the 2023 background AM and PM peak hour conditions at the study intersection. Synchro outputs are provided in Appendix D.

			AM F	Peak Hour		PM Peak Hour					
		Delay (sec/veh)	LOS	v/c Ratio	95th percentile queue (veh)	Delay (sec/veh)	LOS	v/c Ratio	95th percentile queue (veh)		
	Overall Intersection	13.8	в	-	-	13.7	В	-	-		
	Eastbound	12.4	В	-	-	10.2	В	-	-		
	EBL	5.8	А	0.11	0.5	7.5	А	0.09	0.3		
	EBT	12.6	В	0.61	15.8	10.2	В	0.50	13.4		
	EBT/R	12.5	В	0.61	16.4	10.2	В	0.51	13.8		
	Westbound	8.2	Α	-	-	9.3	Α	-	-		
US 90	WBL	8.8	А	0.17	0.6	7.3	А	0.26	1.1		
&	WBT	8.3	А	0.39	8.6	9.7	А	0.57	14.9		
Centurion Court	WBR	6.0	А	0.06	0.9	5.6	А	0.09	1.6		
	Northbound	55.1	Е	-	-	64.8	ш	-	-		
	NBL	54.4	D	0.06	0.6	66.6	E	0.29	3.1		
	NBT/R	55.2	Е	0.42	4.3	63.5	Е	0.38	4.4		
	Southbound	58.6	E	-	-	67.8	E	-	-		
	SBL	61.8	E	0.47	4.1	71.5	E	0.52	5.2		
	SBT/R	53.1	D	0.22	2.2	62.3	E	0.28	3.2		

#### Table 4: Background Intersection Conditions

The intersection of US 90 and Centurion Court is expected to operate with LOS B during background (2023) AM peak hour and PM peak hour conditions. All movements are expected to operate with v/c ratios less than 1.00 under background (2023) AM and PM peak hour conditions. The northbound and southbound approaches are expected to continue to operate with LOS E during the AM and PM peak hour due to the prioritization of green time for the mainline US 90 movements.



## 5.0 BUILDOUT CONDITIONS ANALYSIS – YEAR 2023

### 5.1 BUILDOUT TRAFFIC

Future traffic conditions for the proposed development were evaluated for year 2023 conditions with the inclusion of project traffic. Buildout volumes were developed by adding anticipated project trips to background (2023) volumes. Figure 6 illustrates the projected turning movement volumes under buildout AM and PM peak hour conditions at the study intersection and the proposed driveways. The intersection volume development worksheet can be found in Appendix C.

### 5.2 BUILDOUT INTERSECTION ANALYSIS

Intersection operational analyses were performed for 2023 buildout conditions in the AM and PM peak hour conditions using procedures outlined in the *Highway Capacity Manual 6* with *Synchro* (v11) software. Table 5 summarizes the operational analyses for the 2023 buildout AM and PM peak hour conditions at the study intersection. Synchro outputs are provided in Appendix D.

			AM F	Peak Hour		PM Peak Hour					
		Delay (sec/veh)	LOS	v/c Ratio	95th percentile queue (veh)	Delay (sec/veh)	LOS	v/c Ratio	95th percentile queue (veh)		
	Overall Intersection	15.0	В	-	-	15.5	в	-	-		
	Eastbound	13.3	В	-	-	11.3	В	-	-		
	EBL	6.4	А	0.14	0.8	8.9	А	0.16	0.6		
	EBT	13.6	В	0.62	16.5	11.4	В	0.51	14.2		
	EBT/R	13.5	В	0.62	17.1	11.4	В	0.51	14.7		
	Westbound	9.0	Α	-	-	10.8	В	-	-		
US 90	WBL	9.5	А	0.18	0.6	8.3	А	0.27	1.3		
&	WBT	9.2	А	0.4	9.0	11.3	В	0.58	16.2		
Centurion Court	WBR	6.8	А	0.08	1.3	6.8	А	0.12	2.2		
	Northbound	53.2	D	-	-	62.7	Е	-	-		
	NBL	53.8	D	0.06	0.6	65.5	E	0.28	3.1		
	NBT/R	53.1	D	0.37	4.2	60.7	Е	0.33	4.3		
	Southbound	57.8	E	-	-	66.4	Е	-	-		
	SBL	61.3	E	0.54	5.2	70.4	E	0.58	6.8		
	SBT/R	52.1	D	0.28	3.0	60.7	E	0.33	4.2		

#### Table 5: Buildout Intersection Conditions

The intersection of US 90 and Centurion Court is expected to operate with LOS B during buildout (2023) AM peak hour and PM peak hour conditions. All movements are expected to operate with v/c ratios less than 1.00 under buildout (2023) AM and PM peak hour conditions. The northbound and southbound approaches are expected to continue to operate with LOS E during the AM and PM peak hour due to the prioritization of green time for the mainline US 90 movements.



## 6.0 CONCLUSION

This traffic impact analysis was performed to assess the transportation impacts of the proposed expansion of a gas station and Circle K convenience market located in the northwest quadrant of the intersection of US Highway 90 (SR 10) and Centurion Court/SW Florida Gateway Drive. The expansion, proposed for buildout in year 2023, will include the addition of 3 vehicle fueling positions designed for diesel trucks and a 900-square foot expansion to the existing Circle K convenience market. Access to the site will be provided via two existing driveways and one new driveway to the north on Centurion Court.

Accounting for the observed trip generation of the existing site, the proposed expansion is anticipated to generate 16 net new AM peak hour trips and 18 net new PM peak hour trips at buildout. An additional 48 new AM peak hour pass-by trips and 54 new PM peak hour pass-by trips are expected at the site as well.

Operational analyses were performed utilizing *Synchro* software for the existing (2021), background (2023), and buildout (2023) conditions at the study intersection of US 90 and Centurion Court/SW Florida Gateway Drive during the AM peak hour and the PM peak hour. Results indicated that the study intersection is expected to operate at LOS B through the buildout year. No operational deficiencies are expected at the study intersection with the inclusion of project traffic under buildout (2023) conditions.

# APPENDIX A Conceptual Site Plan



INTERSTATE 75 OFF-RAMP



NOTES:

1. ALL CURB DIMENSIONS ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.

2. BUILDING DIMENSIONS ARE TO THE OUTSIDE FACE OF BUILDING UNLESS OTHERWISE NOTED.

3. REFER TO ARCHITECTURAL AND STRUCTURAL PLANS TO VERIFY ALL BUILDING DIMENSIONS.

- 4. REFER TO SIGNAGE PLANS FOR MONUMENT SIGN DETAILS.
- 5. SEE MEP PLANS FOR ELECTRICAL DRAWINGS.

6. ALL PROPOSED ON-SITE STRIPING AND PAVEMENT MARKING WILL BE PAINTED UNLESS OTHERWISE NOTED AND IN ACCORDANCE WITH FDOT INDEX 711-001.

7. REFER TO ARCHITECTURAL PLANS FOR PROPOSED TRASH CAN LOCATIONS AND DESIGN.

8. BOLLARDS IN SIDEWALK ADJACENT TO BUILDING SHALL BE COVERED WITH RED PLASTIC COVERS TO BE SUPPLIED BY CONTRACTOR.

9. BOLLARDS UNDER CANOPY SHALL BE COVERED WITH GRAY PLASTIC COVERS TO BE SUPPLIED BY CONTRACTOR (SEE FUEL PUMP DESIGNER PLANS FOR MORE DETAIL).

10. REFER TO ARCHITECTURAL PLANS FOR SITE LIGHTING AND ELECTRICAL PLANS.

11. ALL SIGNAGE AND PAVEMENT MARKINGS SHALL MEET MUTCD AND FDOT STANDARDS.

12. ALL SIGNAGE SHALL MEET THE REQUIREMENTS OF POLK COUNTY LAND DEVELOPMENT CODE, CHAPTER 7, SEC. 760



PROPERTY LINE (TYP.) PROPOSED ASPHALT PAVEMENT (SEE DETAIL SHEET C7.0) PROPOSED CONCRETE SIDEWALK (SEE DETAIL SHEET C7.0) PROPOSED MEDIUM DUTY CONCRETE (SEE DETAIL SHEET C7.0) PROPOSED HEAVY DUTY CONCRETE (SEE DETAIL SHEET C7.0)

VERTICAL DATUM: LELVATIONS ARE BASED ON BENCHMARK DESIGNATION BM 32 BEING: 117.497 FEET, (NGVD 29), PUBLISHED BY FLORIDA DEPARTIMENT OF TRANSPORTATION. ORTHOWETRIC HEIGHT CONVERSION PROVIDED BY VERTCON: DATUM SHIFT (NAVD-NGVD= -0.883 FEET)



E K - US HWY I -75 FUEL PANSION PANSION PANSION PANSION FLORIDA FLOR
E K - US HWY I-75 FUEL PANSION I - 76 FUEL I - 76 FUEL PANSION I - 76 FUEL PANSIO
E K - US HWY I 175 FUEL PANSION I 175 FUEL I
E K - US HWY : I-75 FUEL PANSION : I-76 FUEL PANSION : I-76 FUEL PANSION : I-76 FUEL : I
E K - US HWY I-75 FUEL PANSION
E K - US HWY I-75 FUEL PANSION
CIRCLE 90 & EXI

# APPENDIX B Traffic Data





National Data & Surveying Services

Site Code:	21-120370-001
Date:	09/02/2021
Weather:	Sunny
City:	Lake City
County:	Columbia
Count Times:	07:00 - 09:00
	12:00 - 14:00
	16:00 - 18:00
Control:	Signalized

#### SIGNAL TIMING

PHASES	1	2	3
NT/ST	00:25	00:33	00:20
EL/WL	00:15	+	*
WL/WT	-	00:13	120
ET/WT	01:42	01:34	01:59





Location: Florida Gateway Dr & US Hwy 90 City: Lake City Control: Signalized

Project ID: 21-120370-001 Date: 9/2/2021

	-							Data -	Total								
NS/EW Streets:		Florida Gat	teway Dr			Florida Gat	eway Dr			US Hw	y 90			US Hw	y 90		
AM	0	NORTH 0	BOUND O	0	0	SOUTHE 0	BOUND O	0	0	EASTB 0 ET	OUND 0 ER	0 EU	0	WESTB 0	OUND 0	0	TOTAL
7:00 AM 7:15 AM 7:30 AM	4 1 2	1 0 2	7 16 14	0 0 0 0	16 7 14	0 2 2	7 8 4	0 0 0 0	6 6 8	215 348 378	1 2 1	0 0 0	2 4 10	149 182 215	8 11 11	1 1 0	417 588 661
7:45 AM 8:00 AM 8:15 AM 8:30 AM	1 6 2 4	1 3 1 0	15 14 16 12	0 0 0 0	13 23 22 19	1 0 0 3	6 12 10 11	0 0 0 0 0 0	15 11 10 7	350 255 214 239	4 5 5 6	0	11 12 16 16	236 209 196 211	18 17 16 21	2 2 0 1	673 569 508 550
8:45 AM	6 NL 26	2 NT 10	12 NR 106	0 NU 0	17 SL 131	4 ST 12	9 SR 67	U SU 0	EL 70	ET 2210	ER 30	EU 0	WL 78	203 WT 1601	11 WR 113	1 WU 8	496 TOTAL 4462
APPROACH %'s : PEAK HR :	18.31%	7.04% 07:15 AM -	74.65% 08:15 AM	0.00%	62.38%	5./1%	31.90%	0.00%	3.03%	95.67%	1.30%	0.00%	4.33%	88.94%	6.28%	0.44%	TOTAL
PEAK HR VOL : PEAK HR FACTOR :	10 0.417	6 0.500 0.8	59 0.922 15	0.000	57 0.620	5 0.625 0.65	30 0.625 57	0.000	40 0.667	1331 0.880 0.89	0.600 93	0.000	37 0.771	842 0.892 0.88	57 0.792 31	5 0.625	2491 0.925
										EACTR				WECTE			
NOON	0 NL	0 NT	0 NR	0 NU	0 SL	0 	0 SR	0 SU	0 EL	0 ET	0 ER	0 EU	0 WL	0 WT	0 WR	0 WU	TOTAL
12:00 PM 12:15 PM 12:30 PM	5 7 4	0 0 2	10 18 13	0 0 0	23 27 21	0 0 0	8 8 13	0	6 7 6	318 237 290	4 4 3	0 0 1	16 19 15	230 261 252	26 25 21	3 4 0	649 617 641
12:45 PM 1:00 PM 1:15 PM 1:30 PM	7 8 3	1 1 0	12 16 14 12	0 0 0	34 28 30	1 1 0	13 5 5	0 0 0 0	8 5 4 5	236 252 243	9 7 9	0 0 0 0	15 16 17 21	291 291 273	34 22 26	2 2 2 4	665 652 631
1:45 PM	2	1	18	0	25	1	4	0	5	254	1	0	11	290	20	0	632
TOTAL VOLUMES : APPROACH %'s :	43 26.54%	6 3.70%	113 69.75%	0 0.00%	206 73.84%	4 1.43%	69 24.73%	0 0 0.00%	46 2.14%	2064 95.91%	41 1.91%	1 0.05%	130 5.11%	2188 86.04%	208 8.18%	17 0.67%	5136
PEAK HR VOL : PEAK HR FACTOR :	26 0,813	5 0.625 0.8	55 0.859 96	0 0.000	101 0.743	3 0.750 0.77	44 0.846 71	0 0.000	23 0.719	1012 0.872 0.88	23 0.639 33	1 0.250	63 0.926	1134 0.945 0.93	111 0.816 36	6 0.750	2607 0.980
		NORTH	BOUND			SOLITH	BOLIND			FASTR				WESTE			
PM	0 NL	0 NT	0 NR	0 NU	0 SL	0 ST	0 SR	0 SU	0 EL	0 ET	0 ER	0 EU	0 WL	0 WT	0 WR	0 WU	TOTAL
4:00 PM 4:15 PM 4:30 PM	4 4 4	0 0	14 16 13	0 0 0	16 18 21	0 2	7 12 7	0	3 5 7	273 237 239	3 3 7	0	11 15 11	349 314 295	30 12 21	3 1 0	713 637 627
5:00 PM 5:15 PM 5:30 PM	5 13 7 13	1 0 0	14 15 12 13	0	15 16 13 17	2	13 9 10	0	6	316 290 198	6 7 8	0 0 0 0	17 14 15 18	334 265 344	21 24 25	3 4 4 0	761 650 652
5:45 PM	5	0	27	Ő	18	1	7	0	4	223	8	0	22	265	28	4	612
TOTAL VOLUMES : APPROACH %'s :	NL 55 30.39%	NI 2 1.10%	NR 124 68.51%	NU 0 0.00%	SL 134 63.51%	51 6 2.84%	5R 71 33.65%	0 0 0.00%	EL 39 1.81%	2063 95.95%	ER 48 2.23%	0 0 0.00%	WL 123 4.40%	2476 88.49%	wк 180 6.43%	19 0.68%	5340
PEAK HR : PEAK HR VOL : PEAK HR FACTOR :	38 0.731	2 0.500	54 0.900	0 0.000	61 0,897	3 0.375	38 0.731	0 0.000	20 0.833	1091 0.863	27 0.844	0 0.000	64 0.889	1253 0.911	89 0.890	11 0.688	2751 0.904

Location: Florida Gateway Dr & US Hwy 90 City: Lake City Control: Signalized

Project ID: 21-120370-001 Date: 9/2/2021

	5							Data -	Cars								
NS/EW Streets:		Florida Gat	eway Dr			Florida Gate	eway Dr			US Hwy	y 90			US Hwy	/ 90		
AM	0 NI	NORTH 0 NT	BOUND 0 NR	0 NU	0 SI	SOUTHE 0 ST	BOUND 0 SR	0 SU	0 EL	EASTBO 0 ET	DUND 0 ER	0 EU	0 WL	WESTB 0 WT	OUND 0 WR	0 WU	TOTAL
7:00 AM	4	1	6	0	16	0	7	0	6	207	1	0	2	146	8	1	405
7:15 AM	1	0	16	0	6	2	8	0	6	343	2	0	4	175	9	1	573
7:30 AM	2	2	14	0	14	2	4	0	12	3/1	1	0	10	205	10	0	643
7.45 AM 8.00 AM	6	3	15	0	20	0	12	0	11	249	5	0	12	202	17	2	553
8 15 AM	2	1	16	õ	22	õ	9	Õ	9	205	5	0	16	186	16	0	487
8:30 AM	4	0	12	0	19	3	10	0	7	234	6	0	14	200	20	1	530
8:45 AM	6	2	12	0	17	3	7	0	6	203	6	0	7	191	10	1	471
	NL	NT	NR	NU	SL	ST	SR	SU	EL	ET	ER	EU	WL	WT	WR	WU	TOTAL
TOTAL VOLUMES :	26	10	105	0	125	11	62 21 2104	0	54	2154	30	0	75 4 35%	1535	107	8	4312
DFAK HP	18.44%	07:15 AM -	08:15 AM	0.00%	03.13%	5.30%	31.3170	0.00%	2.03%	93.62.70	1.5570	0.00 %	0, 55 1	00.9970	0.2070	0.10 /0	TOTAL
PEAK HR VOL :	10	6	59	0	51	5	29	0	36	1305	12	0	36	812	53	5	2419
PEAK HR FACTOR :	0.417	0.500	0.922	0.000	0.638	0.625	0.604	0.000	0.750	0.879	0.600	0.000	0.750	0.886	0.779	0.625	0.930
		0.8:	15			0,66	54		_	0.89	92			0,8/	8	_	
		NORTHBOUND SOUTHBOUND								EASTB	OUND			WESTB	OUND		
NOON	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	NL	NT	NR	NU	SL	ST	SR	SU	EL	ET	ER	EU	WL	WT	WR	WU	TOTAL
12:00 PM	5	0	9	0	19	0	8	0	0	231	4	0	15	225	25	3	626
12:15 PM	4	1	13	0	19	0	13	õ	6	281	2	1	15	242	21	0	618
12:45 PM	7	0	9	0	18	1	12	0	7	226	4	0	15	293	32	2	626
1:00 PM	5	1	14	0	33	1	12	0	5	232	8	0	16	279	34	2	642
1:15 PM	8	1	13	0	2/	1	5	0	4	240	5	0	20	281	25	4	612
1:45 PM	2	1	17	ő	23	1	4	0	3	247	1	õ	10	279	20	ò	608
	_																
	NL	NT	NR	NU	SL	ST	SR	SU	EL	ET	ER	EU	WL 12E	WT 2124	WR	WU 17	TOTAL
TOTAL VOLUMES :	41	4	104	0 00%	196 73.68%	4	00 24 81%	0 0.00%	43	2003	30 173%	0.05%	5.07%	212 <del>4</del> 86.13%	200	0.69%	4904
PEAK HR :	27.3270	12:30 PM -	01:30 PM	0.00 /0	73.0070	1.50 /0	21.0170	0.0070	2.0070	50.1070	211010	0.00 /0	5107 10	0011070	OTAL /C	0102.10	TOTAL
PEAK HR VOL :	24	3	49	0	97	3	42	0	22	985	19	1	63	1095	108	6	2517
PEAK HR FACTOR :	0.750	0,750	0.875	0,000	0,735	0.750	0.808	0.000	0.786	0.876	0.594	0.250	0.926	0.934	0.794	0.750	0.980
		0,8	04			0.71	12			0.80	50			0.9.	0		
10000		NORTH	BOUND			SOUTH	BOUND			EASTB	OUND			WESTE	OUND	_	
PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	TOTAL
4:00 PM	NL 4	0	14	NU	5L 16	0	<u>5R</u> 7	<u></u> 0	2 EL	262	3	0	11	336	29	3	688
4:15 PM	4	õ	16	ŏ	17	0	12	ŏ	5	231	3	Ő	14	303	11	1	617
4:30 PM	4	0	12	0	21	2	6	0	7	233	7	0	10	284	21	0	607
4:45 PM	5	1	13	0	15	0	6	0	5	282	6	0	16	302	19	3	673
5:00 PM	11	1	15	0	15	2	9	U O	2	282	7	0	13	257	24	4	633
5:30 PM	13	0	13	0	17	ō	10	ŏ	6	196	8	ŏ	18	340	25	ò	646
5:45 PM	5	0	27	0	18	1	7	0	4	218	8	0	22	258	24	4	596
	NL	NT	NR	NU	SL	ST	SR	SU	EL	ET	ER	EU	WL	WT	WR	WU	TOTAL
TOTAL VOLUMES :	53	2	122	0	132	6	70	0	39	2016	48	0	118	2410	172	19	5207
APPROACH %'s :	29.94%	1.13%	68.93%	0.00%	63.46%	2.88%	33.65%	0.00%	1.85%	95.86%	2.28%	0.00%	4.34%	88.64%	6.33%	0.70%	TOTAL
PEAK HR :	36	04:45 PM -	53	0	60	3	38	0	20	1072	27	0	61	1229	87	11	2699
PEAK HR FACTOR :	0.692	0.500	0.883	0.000	0.882	0.375	0.731	0.000	0.833	0.859	0.844	0.000	0.847	0.904	0.870	0.688	0,903
		0.8	43			0.84	42	1.0		0.8	63	100		0.9	06		0.505

Appendix B: Traffic Data Page 4 of 14

1

Location: Florida Gateway Dr & US Hwy 90 Project ID: 21-120370-001 City: Lake City Date: 9/2/2021 Control: Signalized Data - HT US Hwy 90 US Hwy 90 Florida Gateway Dr NS/EW Streets: Florida Gateway Dr WESTBOUND SOUTHBOUND EASTBOUND NORTHBOUND AM TOTAL EU WL WT WR WU NL NT NU SI ST SR SU EL ΕT ER NR 7:00 AM 7:15 AM 7:30 AM 7:45 AM 8:00 AM 8:15 AM 8:30 AM 8:45 AM WR WU TOTAL ER EU WL. WT NL NT NR NU SL ST SR SU EL ET TOTAL VOLUMES 0.00% 4.00% 88.00% 8.00% 0.00% 50.00% 8.33% 41.67% 0.00% 9.68% 90.32% 0.00% APPROACH %'s : 0.00% 0.00% 100.00% 0.00% TOTAL 07:15 AM - 08:15 AM **PEAK HR: PEAK HR VOL :** 0.000 0.000 0.250 0.833 0,500 0.000 0.000 0.000 0.000 0.000 0.500 0.000 0.250 0.000 0.333 0.813 PEAK HR FACTOR : 0.783 0.583 0.682 0.875 SOUTHBOUND EASTBOUND WESTBOUND NORTHBOUND NOON ER WL WΤ WR WU TOTAL SU EL ET EU NT NU SI ST SR NL NR 12:00 PM 12:15 PM 12:30 PM 12:45 PM Ω 1:00 PM Ō 1:15 PM 1:30 PM 1:45 PM EU WL WT WR WU TOTAL NU ST SR SU EL ET ER NT 2 NR SL NL Π **TOTAL VOLUMES:** 88.41% 7.25% 0.00% 6.49% 83.12% 10.39% 0.00% 69.23% 0.00% 76.92% 0.00% 23.08% 0.00% 4.35% APPROACH %'s : 15.38% 15.38% TOTAL 12:30 PM - 01:30 PM **PEAK HR:** Ω PEAK HR VOL : 0.250 0.750 0.500 0.000 0.000 0.813 0.375 0.000 0.500 0.500 0.000 0.500 0.000 0.500 0.000 0.250 **PEAK HR FACTOR :** 0.978 0.800 0.875 0.625 0.750 EASTBOUND WESTBOUND SOUTHBOUND NORTHBOUND PM EU WL WR WU TOTAL WT NT NU SI ST SR SU EL ΕT ER NI NR 4:00 PM 4:15 PM Ω 4:30 PM - 1 4:45 PM -0 5:00 PM 5:15 PM 5:30 PM Ω 5:45 PM D WL. WT WR WU TOTAL SU EL ET ER EU ST SR NL NT NR NU SL TOTAL VOLUMES 0.00% 100.00% 0.00% 0.00% 6.33% 83.54% 10.13% 0.00% 0.00% 50.00% 0.00% 50.00% 0.00% 66.67% 0.00% 33.33% APPROACH %'s : TOTAL PEAK HR : 04:45 PM - 05:45 PM PEAK HR VOL : 0.750 0.250 0.000 0.594 0.000 0.000 0.750 0.000 0.250 0.000 0.000 0.000 0.000 PEAK HR FACTOR : 0.250 0.000 0.250 0.765 0.806 0.250 0.594 0.375

Location: Florida Gateway Dr & US Hwy 90 City: Lake City Control: Signalized

5

Project ID: 21-120370-001 Date: 9/2/2021

								Data -	Bikes								
NS/EW Streets:		Florida Ga	teway Dr			Florida Gat	eway Dr			US Hw	y 90			US Hwy	/ 90		
AM	0	NORTH 0		0	0	SOUTHE 0	BOUND 0 SP	0 SU	0 El	EASTBO	OUND O	0 FU	0	WESTB 0 WT	OUND 0 WR	0 W11	TOTAL
7.00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7 15 AM 7 30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1
7:45 AM 8:00 AM	0	0	0	0	1	0	0	0	0	1	0	0	0	0	0	0	2
8:15 AM 8:30 AM	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	1 0
8:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
TOTAL VOLUMES :	NL O	NT 1	NR 0	NU 0	SL 1	ST 0	SR 1	SU 0	EL 0	ET 3	ER 0	EU 0	WL 1	WT 1	WR 0	WU 0	TOTAL 8
APPROACH %'s :	0.00%	100.00%	0.00%	0.00%	50.00%	0.00%	50.00%	0.00%	0.00%	100.00%	0.00%	0.00%	50.00%	50.00%	0.00%	0.00%	TOTAL
PEAK HR VOL : PEAK HR FACTOR :	0 0.000	1 0.250	0.000	0 0.000	1 0.250	0	1 0.250	0 0.000	0 0.000	2 0.500	0.000	0 0.000	1 0.250	1 0.250	0.000	0 0.000	7 0.583
		0,2	.50			0.20	0			0.50				0.54			
NOON	0	NORTH 0	IBOUND 0	0	0	SOUTH 0	BOUND 0	0	0	EASTB 0	OUND 0	0	0	WESTB 0	OUND 0	0	
12:00 PM	NL	<u>NT</u>	NR 0	NU	<u>SL</u>	<u>ST</u>	SR 0	<u>SU</u>	EL 0	<u>ET</u>	ER 0	<u>EU</u>	WL 0	0 0	<u>WR</u> 0	0	TOTAL 1
12:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
12:30 PM 12:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	Ó
1:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0 0	0
1:30 PM	0	Ő	0	Ö	0	Ő	0	0	0	0	0	0	0	0	0	0	0
1:45 PM	0	U	U	0	U	U	U	0	0	0		0			<u> </u>		0
TOTAL VOLUMES :	NL O	NT 0	NR 0	NU O	SL 0	ST 0	SR 0	SU 0	EL 0	ET 1	ER 0	EU 0	WL 0	WT 1 100.00%	0 0	0 0 0,00%	2
APPROACH %'S : PEAK HR :		12:30 PM	01:30 PM						0.00%	100.00%	0.00%	0.00%	0.00%	100.00%	0.00 %	0.00%	TOTAL
PEAK HR VOL : PEAK HR FACTOR :	0 0.000	0 0.000	0 0.000	0 0.000	0 0.000	0 0.000	0 0.000	0 0.000	0 0.000	0 0.000	0 0.000	0 0.000	0 0.000	1 0.250 0.25	0 0.000 50	0 0.000	1 0.250
		NOPTI				SOLITH	BOUND			FASTE	OUND			WESTE	BOUND		
PM	0	0	0	0	0	0	0 SR	0 SU	0	0 ET	0 FR	0 EU	0 WI	0 WT	0 WR	0 WU	TOTAL
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1
4:15 PM 4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	ŏ	Ő	0
4:45 PM 5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1 2
5:45 PM	0	ŏ	ŏ	Ő	0	0	ō	ŏ	Ö	0	õ	Ő	0	ō	Ō	0	0
TOTAL VOLUMES :	NL 0	NT 0	NR 0	NU O	SL 0	ऽा 0	SR 2	SU 0	EL 0	ET 1	ER 0	EU 0	WL 0	WT 3	WR 0	WU 0	TOTAL 6
APPROACH %'s :		04:45 PM	- 05:45 PM		0.00%	0.00%	100.00%	0.00%	0.00%	100.00%	0.00%	0.00%	0.00%	100.00%	0.00%	0.00%	TOTAL
PEAK HR VOL : PEAK HR FACTOR :	0 0.000	0	0	0 0.000	0 0.000	0 0.000	2 0.500	0 0.000	0 0.000	0 0.000	0 0.000	0 0.000	0 0.000	2 0.500	0 0.000	0 0.000	4
						0.5	00							0.5	00		



Location: Florida Gateway Dr & US Hwy 90 Project ID: 21-120370-001											
City: 1	lake City		Data - P	edestria	ns (Cross	swalks)	9/2/2021				
NS/EW Streets:	Florida Ga	iteway Dr	Florida Ga	teway Dr	US Hw	ry 90	US Hv	vy 90			
0.04	NORT	H LEG	SOUTH	1 LEG	EAST	LEG	WEST	LEG			
	EB	WB	EB	WB	NB	SB	NB	SB	TOTAL		
7:00 AM	0	0	1	0	0	0	0	0	1		
7:15 AM	0	0	0	0	0	0	0	0	0		
7:30 AM	0	0	0	0	0	0	0	0	1		
7:45 AM	1	0	0	0	1	0	0	0	1		
8:00 AM	0	0	0	0	0	0	0	0	0		
8:10 AM	1	0	ő	0	0	ő	õ	ŏ	1		
8:45 AM	Ō	Ő	õ	õ	Ő	0	0	Ō	Ō		
	EB	WB	EB	WB	NB	SB	NB	SB	TOTAL		
TOTAL VOLUMES :	2	0	1	0	1	0	0	0	4		
APPROACH %'s :	100.00%	0.00%	100.00%	0.00%	100.00%	0.00%			TOTAL		
PEAK HR :	07:15 AM	- 08:15 AM							TOTAL		
PEAK HR VOL :	1	0	0	0	1	0	0	0	2		
PEAK HR FACTOR :	0.250	250			0.250	50			0.500		
	NODT		SOUT		EAST	LEC	WES	TIEG			
NOON		H LEG	FR	I LEG	NB	SR	NB	SB			
12:00 PM	0	1	0	0	0	0	0	0	1		
12:00 PM	ő	Ô	Ö	õ	1	Ō	0	1	2		
12:30 PM	õ	õ	0	0	0	0	0	0	0		
12:45 PM	0	0	0	0	1	0	0	0	1		
1:00 PM	0	0	0	0	0	0	0	0	0		
1:15 PM	1	0	0	0	0	0	0	0	1		
1:30 PM	0	0	0	0	0	0	0	0	0		
1:45 PM	0	0	0	0	U	U	U	U	0		
	EB	WB	EB	WB	NB	SB	NB	SB	TOTAL		
TOTAL VOLUMES :	1	1	0	0	100,000/	0	0	100.009/	5		
APPROACH %'s :	50.00%	50.00%			100.00%	0.00%	0.00%	100.00%	TOTAL		
PEAK HR :	12:30 PM	- 01:30 PM	0	0	1	٥	0	0			
PEAK HR VOL :	0.250	0	0	U	0.250	U	0	U	<u> </u>		
PEAK HK PACTOR :	0,230	250			0.250	50			0.500		
200	NORT	THIEG	SOUT	HLEG	EAST	LEG	WES	T LEG			
PIM	EB	WB	EB	WB	NB	SB	NB	SB	TOTAL		
4:00 PM	0	0	0	1	0	0	0	0	1		
4:15 PM	1	1	0	0	0	0	0	0	2		
4:30 PM	1	0	2	1	0	0	0	0	4		
4:45 PM	0	1	U	1	U	0	0	0	2		
5:00 PM	0	0	0	2		0	0	0	n n		
2:12 PM	1	n	0	ő	0	õ	ŏ	õ	1		
5:45 PM	ô	ŏ	ō	0	0	Ō	ō	0	0		
	EB	WB	EB	WB	NB	SB	NB	SB	TOTAL		
TOTAL VOLUMES :	3	2	2	5	0	0	0	0	12		
APPROACH %'s :	60.00% 40.00%		28.57%	71.43%							
PEAK HR :	04:45 PM	- 05:45 PM							TOTAL		
PEAK HR VOL :	1	1	0	3	0	0	0	U	5		
PEAK HR FACTOR :	0.250	0.250		0.375					0.625		
	0.	500	0	2/3							

Prepared by National Data & Surveying Services

## Florida Gateway Dr & US Hwy 90

### Peak Hour Turning Movement Count



Appendix B: Traffic Data Page 8 of 14







R

011

WEEK	DATES	SF	MOCF: 0.97 PSCF	
===== 1 2 3 4 5 6 7 8 9 10 11 2 3 4 5 6 7 8 9 10 11 2 3 4 5 6 7 8 9 10 11 2 3 4 5 6 7 8 9 10 11 2 3 4 5 6 7 8 9 10 11 2 3 4 5 6 7 8 9 10 11 2 3 4 5 6 7 8 9 10 11 2 3 4 5 6 7 8 9 10 11 2 3 4 5 6 7 8 9 10 11 2 3 4 5 6 7 8 9 10 11 2 3 4 5 6 7 8 9 10 11 2 3 4 5 6 7 8 9 10 11 2 3 4 5 6 7 8 9 10 11 2 3 4 5 6 7 8 9 10 11 2 3 4 5 6 7 8 9 10 11 2 3 4 5 6 7 8 9 10 11 2 3 4 5 6 7 8 9 10 1 12 3 4 5 6 7 8 9 10 1 12 3 4 5 6 7 8 9 10 1 12 3 4 5 6 7 8 9 10 1 2 2 12 2 2 2 2 2 2 2 2 2 2 2 2 2 2	$\begin{array}{c} 01/01/2019 & - & 01/05/2019\\ 01/06/2019 & - & 01/12/2019\\ 01/13/2019 & - & 01/19/2019\\ 01/20/2019 & - & 01/26/2019\\ 01/27/2019 & - & 02/02/2019\\ 02/03/2019 & - & 02/03/2019\\ 02/10/2019 & - & 02/03/2019\\ 02/17/2019 & - & 02/23/2019\\ 02/17/2019 & - & 03/02/2019\\ 03/03/2019 & - & 03/09/2019\\ 03/10/2019 & - & 03/09/2019\\ 03/10/2019 & - & 03/09/2019\\ 03/11/2019 & - & 03/23/2019\\ 03/24/2019 & - & 03/23/2019\\ 03/24/2019 & - & 03/23/2019\\ 03/31/2019 & - & 04/06/2019\\ 04/07/2019 & - & 04/13/2019\\ 04/21/2019 & - & 04/20/2019\\ 04/21/2019 & - & 04/27/2019\\ 04/21/2019 & - & 05/04/2019\\ 05/05/2019 & - & 05/11/2019\\ 05/12/2019 & - & 05/18/2019\\ 05/12/2019 & - & 05/18/2019\\ 05/26/2019 & - & 06/08/2019\\ 06/09/2019 & - & 06/08/2019\\ 06/09/2019 & - & 06/22/2019\\ 06/30/2019 & - & 06/22/2019\\ 06/30/2019 & - & 07/23/2019\\ 07/14/2019 & - & 07/23/2019\\ 07/21/2019 & - & 07/27/2019\\ 07/21/2019 & - & 07/27/2019\\ 07/21/2019 & - & 08/03/2019\\ 08/04/2019 & - & 08/17/2019\\ 08/11/2019 & - & 08/24/2019\\ 08/11/2019 & - & 08/24/2019\\ 08/04/2019 & - & 08/24/2019\\ 08/11/2019 & - & 08/24/2019\\ 08/11/2019 & - & 08/24/2019\\ 08/04/2019 & - & 08/24/2019\\ 08/04/2019 & - & 08/24/2019\\ 08/04/2019 & - & 08/24/2019\\ 08/04/2019 & - & 08/24/2019\\ 08/04/2019 & - & 08/24/2019\\ 08/11/2019 & - & 08/24/2019\\ 08/04/201$	1.02 1.05 1.08 1.06 1.04 1.01 1.00 0.99 0.98 0.97 0.98 0.99 1.00 1.02 1.02 1.01	1.05 1.08 1.11 1.09 1.07 1.06 1.04 1.03 1.02 1.01 1.00	
35 <mark>36</mark> 37	08/25/2019 - 08/31/2019 09/01/2019 - 09/07/2019 09/08/2019 - 09/14/2019	1.01 1.00 1.00	1.04 1.03 1.03	
3 3 4 4 4 4 4 4 4 4 4 4 5 4 4 5 5 2 5 3 5 3 5 3 5 3 5 5 5 5 5 5 5 5 5 5 5 5 5	09/15/2019 - 09/21/2019 09/22/2019 - 09/28/2019 09/29/2019 - 10/05/2019 10/06/2019 - 10/12/2019 10/13/2019 - 10/19/2019 10/20/2019 - 10/26/2019 10/27/2019 - 11/02/2019 11/03/2019 - 11/09/2019 11/10/2019 - 11/09/2019 11/10/2019 - 11/23/2019 11/24/2019 - 11/30/2019 12/01/2019 - 12/07/2019 12/08/2019 - 12/14/2019 12/15/2019 - 12/21/2019 12/22/2019 - 12/28/2019 12/29/2019 - 12/31/2019	1.00 1.00 1.00 1.00 1.01 1.01 1.01 1.02	1.03 1.03 1.03 1.03 1.03 1.04 1.04 1.04 1.05 1.05 1.05 1.05 1.05 1.05 1.05 1.05 1.05 1.05 1.05 1.08 1.11	

\* PEAK SEASON

14-FEB-2020 15:39:21

830UPD

2\_2900\_PKSEASON.TXT

Location Details											
Signal ID:	1002	Date:	November 20, 2021								
Major Street:	US 90	Orientation:	E-W								
Minor Street:	FL Gateway Dr	Orientation:	N-S								

Movement # (Controller Phase Ø )	Ø1	Ø2	Ø3	Ø4	Ø5	Ø6	Ø7	Ø8	Ø9	Ø10	Ø11	Ø12	Ø13	Ø14	Ø15	Ø16	Notes
Direction	EBLT	WB		NB	WBLT	EB		SB									
Turn Type	Prot Perm				Prot Perm												
Min Green	5	15		7	5	15		7									
Ext	3.0	4.0		3.0	3.0	4.0		3.0									
Yellow	4.8	4.9		3.8	4.9	4.9		3.8									
All Red	2.0	2.0		2.0	2.0	2.0		2.0									
Max I	15	75		20	15	75		20									
Max II																	
Walk		7		7		7		7									
Flashing Don't Walk		18		29		18		22									
Detector Memory																	
Det. Switching to:	Ø6				Ø2												
Recall		MIN				MIN											
CNA																	

#### Controller Timings (seconds)

#### **Coordination Timings (seconds)**

Battorn	<b>C S O</b>	Cycle								Sp	lits								Offeet	800	Coord Ø
Falleni	0-3-0	Length	Ø1	Ø2	Ø3	Ø4	Ø5	Ø6	Ø7	Ø8	Ø9	Ø10	Ø11	Ø12	Ø13	Ø14	Ø15	Ø16	Unset	Seq	coord
1		<mark>13</mark> 0	<mark>15</mark>	91 MAX		<mark>24</mark>	16	90 MAX		24									24	1	2
2		130	15	70 MAX		45	20	65 MAX		45									15	1	2
3		<mark>150</mark>	<mark>15</mark>	88 MAX		47	25	78 MAX		47									20	1	2
4		110	16	64 MAX		30	23	57 MAX		30									18	1	2
5		100	15	59 MAX		26	17	57 MAX		26									22	1	2
6		140	15	75 MAX		50	23	67 MAX		50									7	1	2
7		110	17	58 MAX		35	18	57 MAX		35									63	1	2
8		100	15	59 MAX		26	17	57 MAX		26									22	1	2
9		140	15	75 MAX		50	23	67 MAX		50									7	1	2
10		110	17	58 MAX		35	18	57 MAX		35									63	1	2

Offset Reference Point	Phase Mode
End of Green of first through movement	STD8



Notes: 1) Use 'Max I' during FREE Operation. 2) Program phase restriction to omit Ø1 during Ø2 green and omit Ø5 during Ø6 green.

Iteris,

#### Signal ID: 1002

Major Street: US 90

Minor Street: FL Gateway Dr

Day Plans

Monday-Thursday			1 1		Cat				0				E vit			
IVIO	nday-	inurs	uay			Satu	raay			Sur	uay				uay	
	Day F	vian 1	-		<u> </u>	Day I	lan 2	-		Day F	lan 3	_		Day F	lan 4	_
Hr	Min	Patt	Cycl		Hr	Min	Patt	Cycl	Hr	Min	Patt	Cycl	Hr	Min	Patt	Cycl
00	00	254	Free		00	00	254	Free	00	00	254	Free	00	00	254	Free
6	30	1	130		8	00	5	100	9	30	8	100	6	30	1	130
10	00	2	130		10	00	6	140	11	00	9	140	10	00	2	130
15	00	3	150		1/	00	/	110	16	30	10	110	11	30	3	150
18	30	4	110		22	00	254	⊦ree	21	00	254	Free	19	00	4	110
21	00	254	Free										22	00	254	Free
					_											
				_												
	Day F	Plan 5				Day I	Plan 6			Day F	Plan 7			Day F	Plan 8	
Hr	Day F Min	Plan 5 Patt	Cvcl		Hr	Day I Min	Plan 6 Patt	Cvcl	Hr	Day F Min	Plan 7 Patt	Cvcl	Hr	Day F Min	Plan 8 Patt	Cvcl
Hr	Day F Min	Plan 5 Patt	Cycl		Hr	Day I Min	Plan 6 Patt	Cycl	Hr	Day F Min	Plan 7 Patt	Cycl	Hr	Day F Min	Plan 8 Patt	Cycl
Hr	Day F Min	Plan 5 Patt	Cycl		Hr	Day I Min	Plan 6 Patt	Cycl	Hr	Day F Min	Plan 7 Patt	Cycl	Hr	Day F Min	Plan 8 Patt	Cycl
Hr	Day F Min	Plan 5 Patt	Cycl		Hr	Day I Min	Plan 6 Patt	Cycl	Hr	Day F Min	Plan 7 Patt	Cycl	Hr	Day F Min	Plan 8 Patt	Cycl
Hr	Day F Min	Plan 5 Patt	Cycl		Hr	Day I Min	Plan 6 Patt	Cycl	Hr	Day F Min	Plan 7 Patt	Cycl	Hr	Day F Min	Plan 8 Patt	Cycl
Hr	Day F Min	Plan 5 Patt	Cycl		Hr	Day I Min	Plan 6 Patt	Cycl	Hr	Day F Min	Plan 7 Patt	Cycl	Hr	Day F Min	Plan 8 Patt	Cycl
Hr	Day F Min	Plan 5 Patt	Cycl		Hr	Day I Min	Plan 6 Patt	Cycl	Hr	Day F Min	Plan 7 Patt	Cycl	Hr	Day F Min	Plan 8 Patt	Cycl
Hr	Day F Min	Plan 5 Patt	Cycl		Hr	Day I Min	Plan 6 Patt	Cycl	Hr	Day F Min	Plan 7 Patt	Cycl	Hr	Day F Min	Plan 8 Patt	Cycl
Hr	Day F Min	Plan 5 Patt	Cycl		Hr	Day I Min	Plan 6 Patt	Cycl	Hr	Day F	Plan 7 Patt	Cycl	Hr	Day F Min	Plan 8 Patt	Cycl
Hr	Day F Min	Plan 5 Patt	Cycl		Hr	Day I Min	Plan 6 Patt	Cycl		Day F	Plan 7 Patt	Cycl	Hr	Day F Min	Plan 8 Patt	Cycl
Hr	Day F Min	Plan 5 Patt	Cycl		Hr	Day I Min	Plan 6 Patt	Cycl	Hr	Day F Min	Plan 7 Patt	Cycl	Hr	Day F Min	Plan 8 Patt	Cycl
Hr	Day F Min	Plan 5 Patt	Cycl		Hr	Day I Min	Plan 6 Patt		Hr	Day F Min	Plan 7 Patt	Cycl	Hr	Day F Min	Plan 8 Patt	Cycl
Hr	Day F Min	Plan 5 Patt			Hr	Day I Min	Plan 6 Patt		Hr	Day F Min	Plan 7 Patt	Cycl	Hr	Day F	Plan 8 Patt	Cycl
Hr	Day F Min	Plan 5 Patt			Hr	Day I	Plan 6 Patt	Cycl	Hr	Day F Min	Plan 7 Patt	Cycl	Hr	Day F Min	Plan 8 Patt	Cycl
Hr	Day F Min	Plan 5 Patt			Hr	Day I	Plan 6 Patt	Cycl		Day F Min	Plan 7 Patt		Hr	Day F Min	Plan 8 Patt	Cycl
Hr	Day F Min	Plan 5 Patt			Hr	Day I	Plan 6 Patt			Day F Min	Plan 7 Patt		Hr	Day F Min	Patt	Cycl
Hr	Day F Min	Plan 5 Patt				Day I	Plan 6 Patt			Day F Min	Plan 7 Patt			Day F Min	Plan 8 Patt	
Hr	Day F Min	Plan 5 Patt				Day H	Plan 6 Patt	Cycl		Day F	Plan 7 Patt		Hr	Day F Min	Plan 8 Patt	

Datt	Force	Alt Opt	Alt Time	Coord					Α	lt Tim	e Tab	le Max	x Valu	ies (Se	econd	ls)				
rall	Mode	Table	Table	Max Plan	Ø1	Ø2	Ø3	Ø4	Ø5	Ø6	Ø7	Ø8	Ø9	Ø10	Ø11	Ø12	Ø13	Ø14	Ø15	Ø16
1	FIXED	None	None	Max Inh																
2	FIXED	None	None	Max Inh																
3	FIXED	None	None	Max Inh																1
4	FIXED	None	None	Max Inh																1
5	FIXED	None	None	Max Inh																1
6	FIXED	None	None	Max Inh																
7	FIXED	None	None	Max Inh																1
8	FIXED	None	None	Max Inh																1
9	FIXED	None	None	Max Inh																
10	FIXED	None	None	Max Inh																1

Iteris,

# APPENDIX C Intersection Volume Development Worksheets

### TRAFFIC VOLUMES AT STUDY INTERSECTIONS

INTERSECTION: COUNT DATE: AM PEAK HOUR FACTOR: PM PEAK HOUR FACTOR: US 90 & Centurion Ct/Florida Gateway Dr September 2, 2021 0.93 0.9

"AM EXISTIN	NG TRAFFIC"	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
AM Raw Turni	ng Movements		40	1,331	12	5	37	842	57		10	6	59		57	5	30
Peak Season Co	onversion Factor	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03
AM EXISTING	CONDITIONS		41	1,371	12	5	38	867	59		10	6	61		59	5	31
"PM EXISTIN	NG TRAFFIC"	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
PM Raw Turni	ng Movements		20	1,091	27	11	64	1,253	89		38	2	54		61	3	38
Peak Season Co	onversion Factor	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03
PM EXISTING	CONDITIONS		21	1,124	28	11	66	1,291	92		39	2	56		63	3	39
"AM BACKGRO	UND TRAFFIC"	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Years To	Buildout	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2
Yearly Gr	Yearly Growth Rate		2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%
AM BACKGROUND	TRAFFIC GROWTH		2	58	1	0	2	37	3		0	0	3		3	0	1
AM NON-PRO	JECT TRAFFIC		43	1,429	13	5	40	904	62		10	6	64		62	5	32
"PM BACKGRO	UND TRAFFIC"	FBU	FBI	FBT	FBR	WBU	WBI	WRT	WBR	NBU	NBI	NBT	NBR	SBU	SBI	SBT	SBR
Years To	Buildout	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2
Yearly Gr	owth Rate	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%
PM BACKGROUND	TRAFFIC GROWTH		1	48	1	0	3	55	4		2	0	2		3	0	2
			22	1 172	20	11	60	1 246	06		41	2	50		66	2	44
FWINON-FILO			22	1,172	29		09	1,340	90		41	2	30		00	3	41
"AM PROJECT	DISTRIBUTION"																
LAND USE	TYPE	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Pass-By	Entering		50.0%	-50.0%				-50.0%	50.0%								
Distribution	Exiting														50.0%		50.0%
Net New	Entering		25.0%						75.0%								
Distribution	Exiting														75.0%		25.0%
			-			-	-	-	-	-	-	-	-	-	-		
"PM PROJECT	DISTRIBUTION"																
LAND USE	TYPE	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Pass-By	Entering		50.0%	-50.0%				-50.0%	50.0%								
Distribution	Exiting														50.0%		50.0%
Net New Distribution	Entering		25.0%		-				75.0%						75.00/		05.00/
Distribution	Exiting														75.0%		25.0%
"AM PROJE	CT TRAFFIC"																
LAND USE	TYPE	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Project	Pass - By		12	-12				-12	12						12		12
Trips	Net New		2						6						6		2
AM TOTAL PRO	DJECT TRAFFIC		14	-12	0	0	0	-12	18		0	0	0		18	0	14
		r	57	1 417	12	5	40	002	80		10	6	64		00	5	46
			57	1,417	13	5	40	092	00		10	0	04		80	5	40
"PM PROJE	CT TRAFFIC"																
LAND USE	TYPE	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Project	Pass - By	_	13	-13		-		-14	14	_				-	13		14
Trips	Net New		2	İ 👘					7						7		2
PM TOTAL PRO	DJECT TRAFFIC		15	-13	0	0	0	-14	21		0	0	0		20	0	16
DM TOTAL		r		4.455				4 000	447			_	50			•	
PIVITOTAL			31	1,159	29	11	69	1,332	117	1	41	2	58	l I	86	3	5/

# APPENDIX D Synchro Output Reports

### Lanes, Volumes, Timings 1: SW Florida Gateway Dr/Centurion Ct & US 90

	٦	-	$\mathbf{\hat{z}}$	4	+	*	1	1	۲	1	ŧ	~
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	5	<b>≜1</b> ≽		5	44	1	5	ĥ		5	î,	
Traffic Volume (vph)	41	1371	12	43	867	59	10	6	61	59	5	31
Future Volume (vph)	41	1371	12	43	867	59	10	6	61	59	5	31
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	150		0	250		125	50		0	0		110
Storage Lanes	1		0	1		1	1		0	1		1
Taper Length (ft)	25			50			25			25		
Right Turn on Red			Yes			Yes			Yes			Yes
Link Speed (mph)		45			45			30			30	
Link Distance (ft)		398			433			442			282	
Travel Time (s)		6.0			6.6			10.0			6.4	
Confl. Peds. (#/hr)	1					1			1	1		
Confl. Bikes (#/hr)			2			1			1			
Peak Hour Factor	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93
Heavy Vehicles (%)	2%	2%	2%	4%	4%	4%	2%	2%	2%	8%	8%	8%
Shared Lane Traffic (%)												
Turn Type	pm+pt	NA		pm+pt	NA	Perm	Perm	NA		Perm	NA	
Protected Phases	1	6		5	2			4			8	
Permitted Phases	6			2		2	4			8		
Detector Phase	1	6		5	2	2	4	4		8	8	
Switch Phase												
Minimum Initial (s)	5.0	15.0		5.0	15.0	15.0	7.0	7.0		7.0	7.0	
Minimum Split (s)	11.8	31.9		11.9	31.9	31.9	41.8	41.8		34.8	34.8	
Total Split (s)	15.0	90.0		16.0	91.0	91.0	24.0	24.0		24.0	24.0	
Total Split (%)	11.5%	69.2%		12.3%	70.0%	70.0%	18.5%	18.5%		18.5%	18.5%	
Yellow Time (s)	4.8	4.9		4.9	4.9	4.9	3.8	3.8		3.8	3.8	
All-Red Time (s)	2.0	2.0		2.0	2.0	2.0	2.0	2.0		2.0	2.0	
Lost Time Adjust (s)	0.0	0.0		0.0	0.0	0.0	0.0	0.0		0.0	0.0	
Total Lost Time (s)	6.8	6.9		6.9	6.9	6.9	5.8	5.8		5.8	5.8	
Lead/Lag	Lead	Lag		Lead	Lag	Lag						
Lead-Lag Optimize?	Yes	Yes		Yes	Yes	Yes						
Recall Mode	None	C-Min		None	C-Min	C-Min	None	None		None	None	
Intersection Summary												
Area Type:	Other											
Cycle Length: 130												
Actuated Cycle Length: 13	30											
Offset: 24 (18%), Referen	ced to phase	e 2:WBTL	and 6:E	BTL, Stai	rt of Yello	W						
Natural Cycle: 100												
Control Type: Actuated-Co	oordinated											
Splits and Phases: 1: S	SW Florida G	ateway D	r/Centuri	on Ct & l	JS 90							

▶ Ø1		•	<b>₼</b> ø4
15 s	91s		24 s
<b>√</b> Ø5	∞6 (R)		Ø8
16 s	90 s		24 s

Kimley-Horn March 2022

### HCM 6th Signalized Intersection Summary 1: SW Florida Gateway Dr/Centurion Ct & US 90

	٭	-	$\mathbf{\hat{z}}$	4	+	×	1	1	۲	1	ŧ	~
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	ľ	<b>∱1</b> ≱		ľ	<b>^</b>	1	ľ	el 🕴		ľ	el el	
Traffic Volume (veh/h)	41	1371	12	43	867	59	10	6	61	59	5	31
Future Volume (veh/h)	41	1371	12	43	867	59	10	6	61	59	5	31
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		0.98	1.00		0.98	1.00		0.98	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1841	1841	1841	1870	1870	1870	1781	1781	1781
Adj Flow Rate, veh/h	44	1474	13	46	932	63	11	6	66	63	5	33
Peak Hour Factor	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93
Percent Heavy Veh, %	2	2	2	4	4	4	2	2	2	8	8	8
Cap, veh/h	453	2533	22	296	2485	1085	173	14	158	139	22	145
Arrive On Green	0.03	0.70	0.70	0.04	0.71	0.71	0.11	0.11	0.11	0.11	0.11	0.11
Sat Flow, veh/h	1781	3609	32	1753	3497	1527	1366	132	1449	1263	202	1335
Grp Volume(v), veh/h	44	725	762	46	932	63	11	0	72	63	0	38
Grp Sat Flow(s), veh/h/ln	1781	1777	1864	1753	1749	1527	1366	0	1580	1263	0	1537
Q Serve(q_s), s	0.9	26.7	26.8	0.9	13.7	1.6	1.0	0.0	5.5	6.4	0.0	2.9
Cycle Q Clear(q_c), s	0.9	26.7	26.8	0.9	13.7	1.6	3.9	0.0	5.5	11.9	0.0	2.9
Prop In Lane	1.00		0.02	1.00		1.00	1.00		0.92	1.00		0.87
Lane Grp Cap(c), veh/h	453	1247	1308	296	2485	1085	173	0	172	139	0	167
V/C Ratio(X)	0.10	0.58	0.58	0.16	0.38	0.06	0.06	0.00	0.42	0.45	0.00	0.23
Avail Cap(c_a), veh/h	510	1247	1308	351	2485	1085	216	0	221	178	0	215
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	5.4	9.8	9.8	7.7	7.4	5.7	54.7	0.0	54.1	59.6	0.0	52.9
Incr Delay (d2), s/veh	0.1	2.0	1.9	0.2	0.4	0.1	0.2	0.0	1.6	2.3	0.0	0.7
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	0.5	14.6	15.2	0.5	8.0	0.9	0.6	0.0	4.1	3.8	0.0	2.1
Unsig. Movement Delay, s/veh	ı											
LnGrp Delay(d),s/veh	5.5	11.7	11.7	7.9	7.9	5.8	54.9	0.0	55.7	61.9	0.0	53.6
LnGrp LOS	А	В	В	А	А	А	D	А	Е	E	А	D
Approach Vol. veh/h		1531			1041			83			101	
Approach Delay, s/veh		11.5			7.7			55.6			58.8	
Approach LOS		В			А			E			E	
Timor - Assigned Phs	1	2		Λ	5	6		Q				
Phys Duration $(G_+V_+R_c)$ s	10.8	 2		10.0	11 0	98.2		19.9				
Change Period ( $V_+R_c$ ) s	6.8	6 Q		* 5 8	6.9	6.9		* 5 8				
May Green Setting (Gmay) s	0.0 8.2	8/L1		* 18	0.7	83 1		* 18				
Max O Clear Time ( $q_{c+11}$ ) s	2.0	15.7		7.5	2.1	28.8		12.0				
Green Ext Time ( $y_c + (1)$ , s	2.7	77		0.0	2.7 0.0	20.0 12.0		0.1				
	0.0	1.1		0.2	0.0	13.7		0.1				
Intersection Summary			10.0									
HCM 6th Ctrl Delay			13.2									
HUM 6th LUS			В									

#### Notes

User approved pedestrian interval to be less than phase max green. \* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Kimley-Horn March 2022

### Lanes, Volumes, Timings 1: SW Florida Gateway Dr/Centurion Ct & US 90

Lane Group         EBL         EBT         EBR         WBL         WBR         NBL         NBL         NBR         SBL         SBR         SBR           Lane Configurations         1         14         28         77         1291         92         39         2         56         63         3         39           Future Volume (vph)         21         1124         28         77         1291         92         39         2         56         63         3         39           Future Volume (vph)         110         100         1900         1111         11         10         11         11         10         11         11         10         111         11		٦	-	$\mathbf{F}$	¥	+	•	•	Ť	۲	1	Ļ	~
Lane Configurations <b>1 1 1 1 2 8 77 1 1 79 7 7 7 7 7 7 7 7 7 7</b>	Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Traffic Oxlume (vph)       21       1124       28       77       1291       92       39       2       56       63       3       39         Future Volume (vph)       21       1124       28       77       1291       92       39       2       56       63       3       39         Future Volume (vph)       1100       1900       1111       1       0       1       1       1       0       1       1       1       0       1       1       1       0       1       1       1       0       1       1       1       0       1       1       1       0       1       1       1       0       1       1       1       0       1       1       1       1       1       1       0       1       1       1       1       1       1       1       1       1       1       1       1       1       1       1       1 </td <td>Lane Configurations</td> <td>ľ</td> <td><b>≜</b>†}</td> <td></td> <td>ľ</td> <td><u></u></td> <td>1</td> <td><u>۲</u></td> <td>el el</td> <td></td> <td>۲</td> <td>el el</td> <td></td>	Lane Configurations	ľ	<b>≜</b> †}		ľ	<u></u>	1	<u>۲</u>	el el		۲	el el	
Fulure (vph)       21       1124       28       77       1291       92       39       2       56       63       3       39         ideal Flow (vphp)       1900       110       11       1	Traffic Volume (vph)	21	1124	28	77	1291	92	39	2	56	63	3	39
Ideal Flow (vphp)       1900       19	Future Volume (vph)	21	1124	28	77	1291	92	39	2	56	63	3	39
Storage Length (ft)       150       0       250       125       50       0       0       110         Storage Lanes       1       0       1       1       1       0       1       1         Taper Length (ft)       25       50       25       25       25       Yes	Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Lanes       1       0       1       1       1       0       1       1       1       1       0       1       <	Storage Length (ft)	150		0	250		125	50		0	0		110
Tape Length (ft)         25         25         25           Right Turn on Red         Yes         Yes <td>Storage Lanes</td> <td>1</td> <td></td> <td>0</td> <td>1</td> <td></td> <td>1</td> <td>1</td> <td></td> <td>0</td> <td>1</td> <td></td> <td>1</td>	Storage Lanes	1		0	1		1	1		0	1		1
Right Turn on Red         Yes         Yes         Yes         Yes         Yes         Yes         Yes           Link Speed (mph)         45         45         30         30         30         1111           Link Distance (ft)         398         4433         442         282         11111         11111         11111         11111         11111         11111         11111         11111         11111         11111         11111         11111         11111         11111         11111         11111         11111         11111         11111	Taper Length (ft)	25			50			25			25		
Link Speed (mph) 45 45 30 30 10 11 11 12 12 12 12 12 12 12 12 12 12 12	Right Turn on Red			Yes			Yes			Yes			Yes
Link Distance (tt) 398 433 442 282 Travel Time (s) 6.0 6.6 10.0 6.4 Confl. Peds. (#hr) 2 3 3 2 Confl. Biks (#hr) 2 3 3 2 Peak Hour Factor 0.90 0.90 0.90 0.90 0.90 0.90 0.90 0.9	Link Speed (mph)		45			45			30			30	
Travel Time (s)       6.0       6.6       10.0       6.4         Confl. Biks (#/hr)       2       3       3       2         Peak Hour Factor       0.90	Link Distance (ft)		398			433			442			282	
Confl. Peds. (#/hr)         2         3         3         2           Confl. Bikes (#/hr)         -2           Peak Hour Factor         0.90         P2%         2%         2%         2%         2%         2%         2%         2%         2%         2%         2%         2%         2%         2%         16         15.0 </td <td>Travel Time (s)</td> <td></td> <td>6.0</td> <td></td> <td></td> <td>6.6</td> <td></td> <td></td> <td>10.0</td> <td></td> <td></td> <td>6.4</td> <td></td>	Travel Time (s)		6.0			6.6			10.0			6.4	
Confl. Bikes (#/hr)         2           Peak Hour Factor         0.90	Confl. Peds. (#/hr)	2		3	3		2						
Peak Hour Factor       0.90       0.9	Confl. Bikes (#/hr)						2						
Heavy Vehicles (%)       2%       2%       2%       2%       3%       3%       3%       2%       2%       2%       3%       3%       2%       2%       2%       3%       3%       2%       2%       2%       3%       3%       2%       2%       2%       3%       3%       2%       2%       2%       2%       3%       3%       2%       2%       2%       2%       3%       3%       2%       2%       2%       2%       3%       3%       2%       2%       2%       3%       3%       2%       2%       2%       3%       3%       2%       2%       2%       3%       3%       3%       2%       3%       3%       3%       3%       2%       2%       2%       2%       2%       2%       2%       2%       2%       2%       2%       2%       3%       3%       3%       3%       3%       3%       3%       3%       3%       3%       3%       3	Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90
Shared Lane Traffic (%)         Turn Type       pm+pt       NA       pm+pt       NA       Perm       NA       Perm       NA         Protected Phases       1       6       5       2       4       8         Detector Phases       6       2       2       4       4       8         Detector Phase       1       6       5       2       2       4       4       8         Switch Phase       1       6       5       2       2       4       4       8       8         Switch Phase	Heavy Vehicles (%)	2%	2%	2%	2%	2%	2%	3%	3%	3%	2%	2%	2%
Turn Type         pm+pt         NA         pm+pt         NA         Perm         Perm         NA         Perm         NA           Protected Phases         1         6         5         2         4         8           Permitted Phases         6         2         2         4         4         8           Detector Phase         1         6         5         2         2         4         4         8           Detector Phase         1         6         5         2         2         4         4         8           Switch Phase	Shared Lane Traffic (%)												
Protected Phases       1       6       5       2       4       8         Permitted Phases       6       2       2       4       8         Detector Phase       1       6       5       2       2       4       8         Switch Phase       1       6       5       2       2       4       4       8         Switch Phase       1       6       5       2       2       4       4       8       8         Switch Phase       1       6       5       2       2       4       4       8       8         Switch Phase       11.6       5.0       15.0       15.0       7.0       7.0       7.0       7.0       7.0         Minimum Initial (s)       5.0       15.0       78.0       25.0       88.0       88.0       47.0       47.0       47.0       47.0       17.0       17.0       17.0       17.0       17.0       17.0       13.3%       31.3%       31.3%       31.3%       31.3%       31.3%       31.3%       31.3%       31.3%       31.3%       31.3%       31.3%       31.3%       31.3%       31.3%       31.3%       31.3%       31.3%       31.3%       3	Turn Type	pm+pt	NA		pm+pt	NA	Perm	Perm	NA		Perm	NA	
Permitted Phases       6       2       2       4       8         Detector Phase       1       6       5       2       2       4       4       8       8         Switch Phase	Protected Phases	1	6		5	2			4			8	
Detector Phase         1         6         5         2         2         4         4         8         8           Switch Phase         Switch Phase         5.0         15.0         5.0         15.0         7.0         7.0         7.0         7.0           Minimum Initial (s)         5.0         15.0         7.0         7.0         7.0         7.0         7.0           Minimum Split (s)         11.8         31.9         11.9         31.9         41.8         41.8         34.8         34.8           Total Split (s)         15.0         78.0         25.0         88.0         87.0         47.0         47.0         47.0           Total Split (%)         10.0%         52.0%         16.7%         58.7%         51.3%         31.5%         5.8 <td>Permitted Phases</td> <td>6</td> <td></td> <td></td> <td>2</td> <td></td> <td>2</td> <td>4</td> <td></td> <td></td> <td>8</td> <td></td> <td></td>	Permitted Phases	6			2		2	4			8		
Switch Phase         Minimum Initial (s)       5.0       15.0       5.0       15.0       7.0       7.0       7.0         Minimum Split (s)       11.8       31.9       11.9       31.9       31.9       41.8       41.8       34.8       34.8         Total Split (s)       15.0       78.0       25.0       88.0       88.0       47.0       47.0       47.0       47.0         Total Split (%)       10.0%       52.0%       16.7%       58.7%       31.3%       31.5%       31.6%       36.9       6.9 <td>Detector Phase</td> <td>1</td> <td>6</td> <td></td> <td>5</td> <td>2</td> <td>2</td> <td>4</td> <td>4</td> <td></td> <td>8</td> <td>8</td> <td></td>	Detector Phase	1	6		5	2	2	4	4		8	8	
Minimum Initial (s)       5.0       15.0       5.0       15.0       7.0       7.0       7.0       7.0         Minimum Split (s)       11.8       31.9       11.9       31.9       31.9       41.8       41.8       34.8       34.8         Total Split (s)       15.0       78.0       25.0       88.0       88.0       47.0       47.0       47.0       47.0         Total Split (%)       10.0%       52.0%       16.7%       58.7%       58.7%       31.3%	Switch Phase												
Minimum Split (s)       11.8       31.9       11.9       31.9       31.9       41.8       41.8       34.8       34.8         Total Split (s)       15.0       78.0       25.0       88.0       88.0       47.0       47.0       47.0       47.0         Total Split (s)       10.0%       52.0%       16.7%       58.7%       58.7%       31.3%	Minimum Initial (s)	5.0	15.0		5.0	15.0	15.0	7.0	7.0		7.0	7.0	
Total Split (s)       15.0       78.0       25.0       88.0       87.0       47.0       47.0       47.0         Total Split (%)       10.0%       52.0%       16.7%       58.7%       58.7%       31.3%	Minimum Split (s)	11.8	31.9		11.9	31.9	31.9	41.8	41.8		34.8	34.8	
Total Split (%)       10.0%       52.0%       16.7%       58.7%       31.3%	Total Split (s)	15.0	78.0		25.0	88.0	88.0	47.0	47.0		47.0	47.0	
Yellow Time (s)       4.8       4.9       4.9       4.9       4.9       3.8       3.8       3.8       3.8       3.8         All-Red Time (s)       2.0	Total Split (%)	10.0%	52.0%		16.7%	58.7%	58.7%	31.3%	31.3%		31.3%	31.3%	
All-Red Time (s)       2.0 <td>Yellow Time (s)</td> <td>4.8</td> <td>4.9</td> <td></td> <td>4.9</td> <td>4.9</td> <td>4.9</td> <td>3.8</td> <td>3.8</td> <td></td> <td>3.8</td> <td>3.8</td> <td></td>	Yellow Time (s)	4.8	4.9		4.9	4.9	4.9	3.8	3.8		3.8	3.8	
Lost Time Adjust (s)       0.0	All-Red Time (s)	2.0	2.0		2.0	2.0	2.0	2.0	2.0		2.0	2.0	
Total Lost Time (s)6.86.96.96.95.85.85.85.8Lead/LagLeadLag <td>Lost Time Adjust (s)</td> <td>0.0</td> <td>0.0</td> <td></td> <td>0.0</td> <td>0.0</td> <td>0.0</td> <td>0.0</td> <td>0.0</td> <td></td> <td>0.0</td> <td>0.0</td> <td></td>	Lost Time Adjust (s)	0.0	0.0		0.0	0.0	0.0	0.0	0.0		0.0	0.0	
Lead/Lag       Lag       Lag       Lag         Lead-Lag Optimize?       Yes       Yes       Yes       Yes         Recall Mode       None       Max       None       C-Max       C-Max       None       None       None       None         Intersection Summary	Total Lost Time (s)	6.8	6.9		6.9	6.9	6.9	5.8	5.8		5.8	5.8	
Lead-Lag Optimize?       Yes       Yes       Yes       Yes       Yes         Recall Mode       None       Max       None       C-Max       C-Max       None       None       None       None         Intersection Summary	Lead/Lag	Lead	Lag		Lead	Lag	Lag						
Recall Mode       None       Max       None       C-Max       None       None <td>Lead-Lag Optimize?</td> <td>Yes</td> <td>Yes</td> <td></td> <td>Yes</td> <td>Yes</td> <td>Yes</td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td>	Lead-Lag Optimize?	Yes	Yes		Yes	Yes	Yes						
Intersection Summary         Area Type:       Other         Cycle Length: 150         Actuated Cycle Length: 150         Offset: 20 (13%), Referenced to phase 2:WBTL, Start of Yellow         Natural Cycle: 100         Control Type: Actuated-Coordinated         Splits and Phases:       1: SW Florida Gateway Dr/Centurion Ct & US 90	Recall Mode	None	Max		None	C-Max	C-Max	None	None		None	None	
Area Type:       Other         Cycle Length: 150	Intersection Summary												
Cycle Length: 150 Actuated Cycle Length: 150 Offset: 20 (13%), Referenced to phase 2:WBTL, Start of Yellow Natural Cycle: 100 Control Type: Actuated-Coordinated Splits and Phases: 1: SW Florida Gateway Dr/Centurion Ct & US 90	Area Type:	Other											
Actuated Cycle Length: 150 Offset: 20 (13%), Referenced to phase 2:WBTL, Start of Yellow Natural Cycle: 100 Control Type: Actuated-Coordinated Splits and Phases: 1: SW Florida Gateway Dr/Centurion Ct & US 90	Cycle Length: 150												
Offset: 20 (13%), Referenced to phase 2:WBTL, Start of Yellow Natural Cycle: 100 Control Type: Actuated-Coordinated Splits and Phases: 1: SW Florida Gateway Dr/Centurion Ct & US 90	Actuated Cycle Length: 1	50											
Natural Cycle: 100 Control Type: Actuated-Coordinated Splits and Phases: 1: SW Florida Gateway Dr/Centurion Ct & US 90	Offset: 20 (13%), Referen	nced to phase	e 2:WBTL	, Start of	Yellow								
Control Type: Actuated-Coordinated Splits and Phases: 1: SW Florida Gateway Dr/Centurion Ct & US 90	Natural Cycle: 100	·											
Splits and Phases: 1: SW Florida Gateway Dr/Centurion Ct & US 90	Control Type: Actuated-C	oordinated											
	Splits and Phases: 1: S	SW Florida G	Sateway D	r/Centur	ion Ct & l	JS 90							

∕ ø1	₩ Ø2 (R)	■ 1 04
15 s	88 s	47 s
<b>√</b> Ø5	<u></u> ∞6	<b>↓</b> ∞8
25 s	78 s	47 s

Kimley-Horn March 2022

### HCM 6th Signalized Intersection Summary 1: SW Florida Gateway Dr/Centurion Ct & US 90

	≯	-	$\mathbf{i}$	4	+	•	1	1	1	1	ŧ	~
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	5	<b>∱1</b> ≱		۲	<b>†</b> †	1	٦	eţ.		1	el 🕴	
Traffic Volume (veh/h)	21	1124	28	77	1291	92	39	2	56	63	3	39
Future Volume (veh/h)	21	1124	28	77	1291	92	39	2	56	63	3	39
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		0.98	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1856	1856	1856	1870	1870	1870
Adj Flow Rate, veh/h	23	1249	31	86	1434	102	43	2	62	70	3	43
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90
Percent Heavy Veh, %	2	2	2	2	2	2	3	3	3	2	2	2
Cap, veh/h	278	2589	64	360	2644	1152	154	5	161	138	11	158
Arrive On Green	0.02	0.73	0.73	0.03	0.74	0.74	0.11	0.11	0.11	0.11	0.11	0.11
Sat Flow, veh/h	1781	3543	88	1781	3554	1549	1349	49	1531	1338	104	1497
Grp Volume(v), veh/h	23	626	654	86	1434	102	43	0	64	70	0	46
Grp Sat Flow(s), veh/h/ln	1781	1777	1854	1781	1777	1549	1349	0	1580	1338	0	1601
Q Serve(g_s), s	0.5	22.0	22.0	1.8	26.0	2.7	4.5	0.0	5.7	7.7	0.0	4.0
Cycle Q Clear(g_c), s	0.5	22.0	22.0	1.8	26.0	2.7	8.5	0.0	5.7	13.4	0.0	4.0
Prop In Lane	1.00		0.05	1.00		1.00	1.00		0.97	1.00		0.93
Lane Grp Cap(c), veh/h	278	1298	1355	360	2644	1152	154	0	167	138	0	169
V/C Ratio(X)	0.08	0.48	0.48	0.24	0.54	0.09	0.28	0.00	0.38	0.51	0.00	0.27
Avail Cap(c_a), veh/h	338	1298	1355	515	2644	1152	383	0	434	365	0	440
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	6.8	8.4	8.4	6.4	8.2	5.3	65.7	0.0	62.6	68.8	0.0	61.8
Incr Delay (d2), s/veh	0.1	1.3	1.2	0.3	0.8	0.2	1.0	0.0	1.4	2.8	0.0	0.9
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/In	0.3	12.5	12.9	1.1	13.7	1.5	2.9	0.0	4.2	5.0	0.0	3.0
Unsig. Movement Delay, s/veh	l .											
LnGrp Delay(d),s/veh	6.9	9.7	9.6	6.7	9.0	5.4	66.7	0.0	64.0	71.6	0.0	62.7
LnGrp LOS	Α	А	А	А	А	А	E	Α	E	E	А	<u> </u>
Approach Vol, veh/h		1303			1622			107			116	
Approach Delay, s/veh		9.6			8.7			65.1			68.1	
Approach LOS		А			А			E			E	
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	9.9	118.5		21.6	11.9	116.5		21.6				
Change Period (Y+Rc), s	6.8	6.9		* 5.8	6.9	6.9		* 5.8				
Max Green Setting (Gmax), s	8.2	81.1		* 41	18.1	71.1		* 41				
Max Q Clear Time (g_c+I1), s	2.5	28.0		10.5	3.8	24.0		15.4				
Green Ext Time (p_c), s	0.0	15.2		0.5	0.1	10.3		0.4				
Intersection Summary												
HCM 6th Ctrl Delay			13.2									
HCM 6th LOS			В									

#### Notes

User approved pedestrian interval to be less than phase max green. \* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Kimley-Horn March 2022

### Lanes, Volumes, Timings 1: SW Florida Gateway Dr/Centurion Ct & US 90

	≯	-	$\mathbf{F}$	4	-	•	•	Ť	1	1	Ļ	~
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	1	<b>∱1</b> ≽		ľ	<u></u>	1	ľ	el el		<u>۲</u>	eî 👘	
Traffic Volume (vph)	43	1429	13	45	904	62	10	6	64	62	5	32
Future Volume (vph)	43	1429	13	45	904	62	10	6	64	62	5	32
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	150		0	250		125	50		0	0		110
Storage Lanes	1		0	1		1	1		0	1		1
Taper Length (ft)	25			50			25			25		
Right Turn on Red			Yes			Yes			Yes			Yes
Link Speed (mph)		45			45			30			30	
Link Distance (ft)		398			433			442			282	
Travel Time (s)		6.0			6.6			10.0			6.4	
Confl. Peds. (#/hr)	1					1			1	1		
Confl. Bikes (#/hr)			2						1			
Peak Hour Factor	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93
Heavy Vehicles (%)	2%	2%	2%	4%	4%	4%	2%	2%	2%	8%	8%	8%
Shared Lane Traffic (%)												
Turn Type	pm+pt	NA		pm+pt	NA	Perm	Perm	NA		Perm	NA	
Protected Phases	1	6		5	2			4			8	
Permitted Phases	6			2		2	4			8		
Detector Phase	1	6		5	2	2	4	4		8	8	
Switch Phase												
Minimum Initial (s)	5.0	15.0		5.0	15.0	15.0	7.0	7.0		7.0	7.0	
Minimum Split (s)	11.8	31.9		11.9	31.9	31.9	41.8	41.8		34.8	34.8	
Total Split (s)	15.0	90.0		16.0	91.0	91.0	24.0	24.0		24.0	24.0	
Total Split (%)	11.5%	69.2%		12.3%	70.0%	70.0%	18.5%	18.5%		18.5%	18.5%	
Yellow Time (s)	4.8	4.9		4.9	4.9	4.9	3.8	3.8		3.8	3.8	
All-Red Time (s)	2.0	2.0		2.0	2.0	2.0	2.0	2.0		2.0	2.0	
Lost Time Adjust (s)	0.0	0.0		0.0	0.0	0.0	0.0	0.0		0.0	0.0	
Total Lost Time (s)	6.8	6.9		6.9	6.9	6.9	5.8	5.8		5.8	5.8	
Lead/Lag	Lead	Lag		Lead	Lag	Lag						
Lead-Lag Optimize?	Yes	Yes		Yes	Yes	Yes						
Recall Mode	None	C-Min		None	C-Min	C-Min	None	None		None	None	
Intersection Summary												
Area Type:	Other											
Cycle Length: 130												
Actuated Cycle Length: 13	0											
Offset: 24 (18%), Reference	ced to phase	e 2:WBTL	and 6:E	BTL, Star	rt of Yello	W						
Natural Cycle: 100												
Control Type: Actuated-Co	ordinated											
Splits and Phases: 1. SV	W Florida G	ateway D	r/Centuri	on Ct & I	JS 90							

▶ <sub>Ø1</sub>		<b>≜</b> <b>1</b> Ø4
15 s	91s	24 s
Ø5	Ø6 (R)	<b>↓</b> Ø8
16 s	90 s	24 s

Kimley-Horn March 2022
Movement         EBL         EBT         EBR         WBL         WBT         WBR         NBL         NBT         NBR         SBL         SBT         SBR           Lane Configurations         1 <t< th=""></t<>
Lane Configurations         1
Traffic Volume (veh/h)       43       1429       13       45       904       62       10       6       64       62       5       32         Future Volume (veh/h)       43       1429       13       45       904       62       10       6       64       62       5       32         nitial Q (Qb), veh       0
Future Volume (veh/h)       43       1429       13       45       904       62       10       6       64       62       5       32         nitial Q (Qb), veh       0
nitial Q (Qb), veh       0
Ped-Bike Adj(A_pbT)       1.00       0.98       1.00
Parking Bus, Adj         1.00
Nork Zone On Approach         No         No         No         No           Adj Sat Flow, veh/h/ln         1870         1870         1870         1870         1841         1841         1841         1870         1870         1870         1781         1781         1781         1781           Adj Flow Rate, veh/h         46         1537         14         48         972         67         11         6         69         67         5         34           Peak Hour Factor         0.93 <t< td=""></t<>
Adj Sat Flow, veh/h/ln187018701870187018701870187017811781Adj Flow Rate, veh/h4615371448972671166967534Peak Hour Factor0.93
Adj Flow Rate, veh/h4615371448972671166967534Peak Hour Factor0.930
Peak Hour Factor0.930.9
Percent Heavy Veh, %222444222888Cap, veh/h432251523278246710991791416514322152Arrive On Green0.030.700.700.040.710.710.111.30 <td< td=""></td<>
Cap, veh/h432251523278246710991791416514322152Arrive On Green0.030.700.700.040.710.710.110
Arrive On Green       0.03       0.70       0.70       0.04       0.71       0.71       0.15       0       0.15
Sat Flow, veh/h         1781         3608         33         1753         3497         1559         1365         126         1453         1259         197         1339           Grp Volume(v), veh/h         46         757         794         48         972         67         11         0         75         67         0         39           Grp Volume(v), veh/h         1781         1777         1864         1753         1749         1559         1365         0         1580         1259         0         1536           Q Serve(g_s), s         0.9         29.2         29.2         1.0         14.7         1.7         1.0         0.0         5.7         6.8         0.0         3.0           Cycle Q Clear(g_c), s         0.9         29.2         29.2         1.0         14.7         1.7         4.0         0.0         5.7         6.8         0.0         3.0           Cycle Q Clear(g_c), s         0.9         29.2         29.2         1.0         14.7         1.7         4.0         0.0         5.7         12.5         0.0         3.0           Crop In Lane         1.00         0.02         1.00         1.00         1.00         0.92         1.
Grp Volume(v), veh/h       46       757       794       48       972       67       11       0       75       67       0       39         Grp Sat Flow(s), veh/h/ln       1781       1777       1864       1753       1749       1559       1365       0       1580       1259       0       1536         2 Serve(g_s), s       0.9       29.2       29.2       1.0       14.7       1.7       1.0       0.0       5.7       6.8       0.0       3.0         Cycle Q Clear(g_c), s       0.9       29.2       29.2       1.0       14.7       1.7       4.0       0.0       5.7       6.8       0.0       3.0         Cycle Q Clear(g_c), s       0.9       29.2       29.2       1.0       14.7       1.7       4.0       0.0       5.7       12.5       0.0       3.0         Cycle Q Clear(g_c), veh/h       432       1239       1299       278       2467       1099       179       0       179       143       0       174         .ane Grp Cap(c), veh/h       432       1239       1299       278       2467       1099       179       0       179       143       0       174         //C Ratio(X)
Grp Sat Flow(s),veh/h/ln178117771864175317491559136501580125901536Q Serve(g_s), s0.929.229.21.014.71.71.00.05.76.80.03.0Cycle Q Clear(g_c), s0.929.229.21.014.71.74.00.05.712.50.03.0Prop In Lane1.000.021.001.001.001.000.921.000.87.ane Grp Cap(c), veh/h432123912992782467109917901791430174//C Ratio(X)0.110.610.610.170.390.060.060.000.420.470.000.22//vail Cap(c_a)yeb/h489123912993332467109921502211760215
Q Serve(g_s), s       0.9       29.2       29.2       1.0       14.7       1.7       1.0       0.0       5.7       6.8       0.0       3.0         Cycle Q Clear(g_c), s       0.9       29.2       29.2       1.0       14.7       1.7       4.0       0.0       5.7       12.5       0.0       3.0         Prop In Lane       1.00       0.02       1.00       1.00       1.00       0.92       1.00       0.87         Lane Grp Cap(c), veh/h       432       1239       1299       278       2467       1099       179       0       179       143       0       174         //C Ratio(X)       0.11       0.61       0.61       0.17       0.39       0.06       0.00       0.42       0.47       0.00       0.22         Lyail Cap(c, a) yeb/h       489       1239       1299       333       2467       1099       215       0       221       176       0       215
Cycle Q Clear(g_c), s         0.9         29.2         29.2         1.0         14.7         1.7         4.0         0.0         5.7         12.5         0.0         3.0           Prop In Lane         1.00         0.02         1.00         1.00         1.00         0.92         1.00         0.87           Lane Grp Cap(c), veh/h         432         1239         1299         278         2467         1099         179         0         179         143         0         174           //C Ratio(X)         0.11         0.61         0.61         0.17         0.39         0.06         0.00         0.42         0.47         0.00         0.22           Lyail Cap(c, a) yeb/h         489         1239         1299         333         2467         1099         215         0         221         176         0         215
Prop In Lane         1.00         0.02         1.00         1.00         1.00         0.92         1.00         0.87           .ane Grp Cap(c), veh/h         432         1239         1299         278         2467         1099         179         0         179         143         0         174           //C Ratio(X)         0.11         0.61         0.61         0.17         0.39         0.06         0.00         0.42         0.47         0.00         0.22           vail Cap(c, a) veb/h         489         1239         1299         333         2467         1099         215         0         221         176         0         215
Lane Grp Cap(c), veh/h         432         1239         1299         278         2467         1099         179         0         179         143         0         174           //C Ratio(X)         0.11         0.61         0.17         0.39         0.06         0.00         0.42         0.47         0.00         0.22           vali         Cap(c, a) veb/h         489         1239         1299         333         2467         1099         215         0         221         176         0         215
//C Ratio(X) 0.11 0.61 0.61 0.17 0.39 0.06 0.06 0.00 0.42 0.47 0.00 0.22
Viail Can(c a) veh/h / 189 1239 1209 333 2767 1000 215 0 221 176 0 215
waii σαρίο_α), volim 407 1237 1277 333 2407 1077 213 0 221 170 0 213
HCM Platoon Ratio         1.00
Jpstream Filter(I) 1.00 1.00 1.00 1.00 1.00 1.00 1.00 0.00 1.00 1.00 0.00 1.00
Jniform Delay (d), s/veh         5.7         10.4         10.4         8.5         7.8         5.9         54.2         0.0         53.6         59.5         0.0         52.4
ncr Delay (d2), s/veh 0.1 2.2 2.2 0.3 0.5 0.1 0.1 0.0 1.5 2.4 0.0 0.6
nitial Q Delay(d3),s/veh 0.0 0.0 0.0 0.0 0.0 0.0 0.0 0.0 0.0 0.
%ile BackOfQ(95%),veh/ln 0.5 15.8 16.4 0.6 8.6 0.9 0.6 0.0 4.3 4.1 0.0 2.2
Jnsig. Movement Delay, s/veh
nGrp Delay(d),s/veh 5.8 12.6 12.5 8.8 8.3 6.0 54.4 0.0 55.2 61.8 0.0 53.1
<u>nGrpLOS A B B A A D A E E A D</u>
Approach Vol, veh/h 1597 1087 86 106
Approach Delay, s/veh 12.4 8.2 55.1 58.6
Approach LOS B A E E
limer - Assigned Phs 1 2 4 5 6 8
Phs Duration (G+Y+Rc), s 10.9 98.6 20.6 11.9 97.5 20.6
Change Period (Y+Rc), s 6.8 6.9 * 5.8 6.9 6.9 * 5.8
Vax Green Setting (Gmax), s 8.2 84.1 * 18 9.1 83.1 * 18
Max Q Clear Time (g_c+I1), s 2.9 16.7 7.7 3.0 31.2 14.5
Green Ext Time (p_c), s 0.0 8.2 0.2 0.0 15.0 0.1
ntersection Summary
HCM 6th Ctrl Delay 13.8
ICM 6th LOS B

### Notes

User approved pedestrian interval to be less than phase max green. \* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Kimley-Horn March 2022

## Lanes, Volumes, Timings 1: SW Florida Gateway Dr/Centurion Ct & US 90

	٦	-	$\mathbf{F}$	4	←	•	1	Ť	1	1	ţ	~
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	1	<b>≜</b> †ĵ≽		۲	<u></u>	1	<u>۲</u>	eî Î		1	eî 👘	
Traffic Volume (vph)	22	1172	29	80	1346	96	41	2	58	66	3	41
Future Volume (vph)	22	1172	29	80	1346	96	41	2	58	66	3	41
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	150		0	250		125	50		0	0		110
Storage Lanes	1		0	1		1	1		0	1		1
Taper Length (ft)	25			50			25			25		
Right Turn on Red			Yes			Yes			Yes			Yes
Link Speed (mph)		45			45			30			30	
Link Distance (ft)		398			433			442			282	
Travel Time (s)		6.0			6.6			10.0			6.4	
Confl. Peds. (#/hr)	2		3	3		2						
Confl. Bikes (#/hr)						2						
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90
Heavy Vehicles (%)	2%	2%	2%	2%	2%	2%	3%	3%	3%	2%	2%	2%
Shared Lane Traffic (%)												
Turn Type	pm+pt	NA		pm+pt	NA	Perm	Perm	NA		Perm	NA	
Protected Phases	1	6		5	2			4			8	
Permitted Phases	6			2		2	4			8		
Detector Phase	1	6		5	2	2	4	4		8	8	
Switch Phase												
Minimum Initial (s)	5.0	15.0		5.0	15.0	15.0	7.0	7.0		7.0	7.0	
Minimum Split (s)	11.8	31.9		11.9	31.9	31.9	41.8	41.8		34.8	34.8	
Total Split (s)	15.0	78.0		25.0	88.0	88.0	47.0	47.0		47.0	47.0	
Total Split (%)	10.0%	52.0%		16.7%	58.7%	58.7%	31.3%	31.3%		31.3%	31.3%	
Yellow Time (s)	4.8	4.9		4.9	4.9	4.9	3.8	3.8		3.8	3.8	
All-Red Time (s)	2.0	2.0		2.0	2.0	2.0	2.0	2.0		2.0	2.0	
Lost Time Adjust (s)	0.0	0.0		0.0	0.0	0.0	0.0	0.0		0.0	0.0	
Total Lost Time (s)	6.8	6.9		6.9	6.9	6.9	5.8	5.8		5.8	5.8	
Lead/Lag	Lead	Lag		Lead	Lag	Lag						
Lead-Lag Optimize?	Yes	Yes		Yes	Yes	Yes						
Recall Mode	None	Мах		None	C-Max	C-Max	None	None		None	None	
Intersection Summary												
Area Type:	Other											
Cycle Length: 150												
Actuated Cycle Length: 1	50											
Offset: 20 (13%), Referen	nced to phase	e 2:WBTL	, Start of	Yellow								
Natural Cycle: 100												
Control Type: Actuated-C	oordinated											
Splits and Phases: 1: S	SW Florida G	Sateway D	r/Centuri	on Ct & l	JS 90							
								_ <b>▲</b>				

	▶ Ø1		▼ <b>1</b> Ø4
1	5s	88 s	47 s
	Ø5	<u>↓</u> <sub>Ø6</sub>	
2	5 s	78 s	47 s

Kimley-Horn March 2022

	≯	-	$\rightarrow$	•	-	•	1	1	1	1	Ŧ	~
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	۲.	A1≱		٦ ۲	<b>^</b>	1	٦	el el		۲	el el	
Traffic Volume (veh/h)	22	1172	29	80	1346	96	41	2	58	66	3	41
Future Volume (veh/h)	22	1172	29	80	1346	96	41	2	58	66	3	41
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		0.98	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1856	1856	1856	1870	1870	1870
Adj Flow Rate, veh/h	24	1302	32	89	1496	107	46	2	64	73	3	46
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90
Percent Heavy Veh, %	2	2	2	2	2	2	3	3	3	2	2	2
Cap, veh/h	260	2577	63	341	2630	1146	157	5	167	142	11	164
Arrive On Green	0.02	0.73	0.73	0.03	0.74	0.74	0.11	0.11	0.11	0.11	0.11	0.11
Sat Flow, veh/h	1781	3544	87	1781	3554	1549	1345	48	1532	1335	98	1502
Grp Volume(v), veh/h	24	652	682	89	1496	107	46	0	66	73	0	49
Grp Sat Flow(s), veh/h/ln	1781	1777	1854	1781	1777	1549	1345	0	1580	1335	0	1600
Q Serve(q_s), s	0.5	23.8	23.8	1.9	28.4	2.9	4.9	0.0	5.8	8.1	0.0	4.2
Cycle Q Clear(q_c), s	0.5	23.8	23.8	1.9	28.4	2.9	9.1	0.0	5.8	13.9	0.0	4.2
Prop In Lane	1.00		0.05	1.00		1.00	1.00		0.97	1.00		0.94
Lane Grp Cap(c), veh/h	260	1292	1348	341	2630	1146	157	0	172	142	0	174
V/C Ratio(X)	0.09	0.50	0.51	0.26	0.57	0.09	0.29	0.00	0.38	0.52	0.00	0.28
Avail Cap(c_a), veh/h	320	1292	1348	496	2630	1146	380	0	434	363	0	439
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	7.3	8.8	8.8	6.9	8.8	5.4	65.6	0.0	62.1	68.6	0.0	61.4
Incr Delay (d2), s/veh	0.2	1.4	1.4	0.4	0.9	0.2	1.0	0.0	1.4	2.9	0.0	0.9
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/In	0.3	13.4	13.8	1.1	14.9	1.6	3.1	0.0	4.4	5.2	0.0	3.2
Unsig. Movement Delay, s/veh	I											
LnGrp Delay(d),s/veh	7.5	10.2	10.2	7.3	9.7	5.6	66.6	0.0	63.5	71.5	0.0	62.3
LnGrp LOS	А	В	В	Α	А	А	E	А	E	E	Α	E
Approach Vol, veh/h		1358			1692			112			122	
Approach Delay, s/veh		10.2			9.3			64.8			67.8	
Approach LOS		В			А			E			E	
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	10.0	117.9		22.1	11.9	116.0		22.1				
Change Period (Y+Rc), s	6.8	6.9		* 5.8	6.9	6.9		* 5.8				
Max Green Setting (Gmax), s	8.2	81.1		* 41	18.1	71.1		* 41				
Max Q Clear Time (g_c+I1), s	2.5	30.4		11.1	3.9	25.8		15.9				
Green Ext Time (p_c), s	0.0	16.3		0.5	0.1	11.0		0.5				
Intersection Summary												
HCM 6th Ctrl Delay			13.7									
HCM 6th LOS			В									

### Notes

User approved pedestrian interval to be less than phase max green. \* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Kimley-Horn March 2022

## Lanes, Volumes, Timings 1: SW Florida Gateway Dr/Centurion Ct & US 90

	≯	-	$\mathbf{F}$	4	-	•	1	t	1	1	ţ	~
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	٦	A		ሻ	<b>^</b>	1	۲	4Î		<u> </u>	eî 🗍	
Traffic Volume (vph)	57	1417	13	45	892	80	10	6	64	80	5	46
Future Volume (vph)	57	1417	13	45	892	80	10	6	64	80	5	46
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	150		0	250		125	50		0	0		110
Storage Lanes	1		0	1		1	1		0	1		1
Taper Length (ft)	25			50			25			25		
Right Turn on Red			Yes			Yes			Yes			Yes
Link Speed (mph)		45			45			30			30	
Link Distance (ft)		398			433			442			282	
Travel Time (s)		6.0			6.6			10.0			6.4	
Confl. Peds. (#/hr)	1					1			1	1		
Confl. Bikes (#/hr)			2						1			
Peak Hour Factor	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93
Heavy Vehicles (%)	2%	2%	2%	4%	4%	4%	2%	2%	2%	8%	8%	8%
Shared Lane Traffic (%)												
Turn Type	pm+pt	NA		pm+pt	NA	Perm	Perm	NA		Perm	NA	
Protected Phases	1	6		5	2			4			8	
Permitted Phases	6			2		2	4			8		
Detector Phase	1	6		5	2	2	4	4		8	8	
Switch Phase												
Minimum Initial (s)	5.0	15.0		5.0	15.0	15.0	7.0	7.0		7.0	7.0	
Minimum Split (s)	11.8	31.9		11.9	31.9	31.9	41.8	41.8		34.8	34.8	
Total Split (s)	15.0	90.0		16.0	91.0	91.0	24.0	24.0		24.0	24.0	
Total Split (%)	11.5%	69.2%		12.3%	70.0%	70.0%	18.5%	18.5%		18.5%	18.5%	
Yellow Time (s)	4.8	4.9		4.9	4.9	4.9	3.8	3.8		3.8	3.8	
All-Red Time (s)	2.0	2.0		2.0	2.0	2.0	2.0	2.0		2.0	2.0	
Lost Time Adjust (s)	0.0	0.0		0.0	0.0	0.0	0.0	0.0		0.0	0.0	
Total Lost Time (s)	6.8	6.9		6.9	6.9	6.9	5.8	5.8		5.8	5.8	
Lead/Lag	Lead	Lag		Lead	Lag	Lag						
Lead-Lag Optimize?	Yes	Yes		Yes	Yes	Yes						
Recall Mode	None	C-Min		None	C-Min	C-Min	None	None		None	None	
Intersection Summary												
Area Type:	Other											
Cycle Length: 130												
Actuated Cycle Length: 13	0											
Offset: 24 (18%), Reference	ced to phase	e 2:WBTL	and 6:E	BTL, Stai	rt of Yello	W						
Natural Cycle: 100												
Control Type: Actuated-Co	ordinated											
Splits and Phases: 1: S	W Florida G	ateway D	r/Centuri	on Ct & l	JS 90							

Ø1	∮ Ø2 (R)	<b>1</b> ø₄
15 s	91s	24 s
Ø5	∞6 (R)	Ø8
16 s	90 s	24 s

Kimley-Horn March 2022

## HCM 6th Signalized Intersection Summary 1: SW Florida Gateway Dr/Centurion Ct & US 90

	۶	-	$\mathbf{r}$	•	-	•	1	1	۲	1	Ŧ	~
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	٦	A		ľ	<u></u>	1	۲	eî 🗧		۲	eî 🗧	
Traffic Volume (veh/h)	57	1417	13	45	892	80	10	6	64	80	5	46
Future Volume (veh/h)	57	1417	13	45	892	80	10	6	64	80	5	46
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		0.98	1.00		1.00	1.00		0.98	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1841	1841	1841	1870	1870	1870	1781	1781	1781
Adj Flow Rate, veh/h	61	1524	14	48	959	86	11	6	69	86	5	49
Peak Hour Factor	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93
Percent Heavy Veh, %	2	2	2	4	4	4	2	2	2	8	8	8
Cap, veh/h	425	2466	23	273	2409	1074	184	16	185	161	18	176
Arrive On Green	0.03	0.68	0.68	0.04	0.69	0.69	0.13	0.13	0.13	0.13	0.13	0.13
Sat Flow, veh/h	1781	3607	33	1753	3497	1559	1347	126	1454	1259	141	1387
Grp Volume(v), veh/h	61	750	788	48	959	86	11	0	75	86	0	54
Grp Sat Flow(s), veh/h/ln	1781	1777	1864	1753	1749	1559	1347	0	1581	1259	0	1528
Q Serve(q_s), s	1.3	30.1	30.1	1.0	15.3	2.4	1.0	0.0	5.7	8.7	0.0	4.2
Cycle Q Clear(q_c), s	1.3	30.1	30.1	1.0	15.3	2.4	5.1	0.0	5.7	14.4	0.0	4.2
Prop In Lane	1.00		0.02	1.00		1.00	1.00		0.92	1.00		0.91
Lane Grp Cap(c), veh/h	425	1215	1274	273	2409	1074	184	0	201	161	0	194
V/C Ratio(X)	0.14	0.62	0.62	0.18	0.40	0.08	0.06	0.00	0.37	0.54	0.00	0.28
Avail Cap(c_a), veh/h	476	1215	1274	328	2409	1074	201	0	221	177	0	214
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	6.3	11.3	11.3	9.2	8.7	6.7	53.7	0.0	52.0	58.6	0.0	51.3
Incr Delay (d2), s/veh	0.2	2.4	2.3	0.3	0.5	0.1	0.1	0.0	1.1	2.7	0.0	0.8
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/In	0.8	16.5	17.1	0.6	9.0	1.3	0.6	0.0	4.2	5.2	0.0	3.0
Unsig. Movement Delay, s/vel	۱											
LnGrp Delay(d),s/veh	6.4	13.6	13.5	9.5	9.2	6.8	53.8	0.0	53.1	61.3	0.0	52.1
LnGrp LOS	А	В	В	А	А	А	D	А	D	E	А	D
Approach Vol, veh/h		1599			1093			86			140	
Approach Delay, s/veh		13.3			9.0			53.2			57.8	
Approach LOS		В			А			D			E	
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	11.2	96.4		22.3	11.9	95.8		22.3				
Change Period (Y+Rc), s	6.8	6.9		* 5.8	6.9	6.9		* 5.8				
Max Green Setting (Gmax), s	8.2	84.1		* 18	9.1	83.1		* 18				
Max Q Clear Time (g c+l1), s	3.3	17.3		7.7	3.0	32.1		16.4				
Green Ext Time (p c), s	0.0	8.1		0.2	0.0	14.7		0.1				
Intersection Summary												
HCM 6th Ctrl Dolay			15.0									
HCM 6th LOS			13.0 R									
			D									

### Notes

User approved pedestrian interval to be less than phase max green. \* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Kimley-Horn March 2022

## Lanes, Volumes, Timings 1: SW Florida Gateway Dr/Centurion Ct & US 90

	٦	-	$\mathbf{F}$	4	←	•	1	1	1	1	ţ	~
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	1	¥î≽		ኘ	<u></u>	1	<u>۲</u>	el el		1	el el	
Traffic Volume (vph)	37	1159	29	80	1332	117	41	2	58	86	3	57
Future Volume (vph)	37	1159	29	80	1332	117	41	2	58	86	3	57
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	150		0	250		125	50		0	0		110
Storage Lanes	1		0	1		1	1		0	1		1
Taper Length (ft)	25			50			25			25		
Right Turn on Red			Yes			Yes			Yes			Yes
Link Speed (mph)		45			45			30			30	
Link Distance (ft)		398			433			442			282	
Travel Time (s)		6.0			6.6			10.0			6.4	
Confl. Peds. (#/hr)	2		3	3		2						
Confl. Bikes (#/hr)						2						
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90
Heavy Vehicles (%)	2%	2%	2%	2%	2%	2%	3%	3%	3%	2%	2%	2%
Shared Lane Traffic (%)												
Turn Type	pm+pt	NA		pm+pt	NA	Perm	Perm	NA		Perm	NA	
Protected Phases	1	6		5	2			4			8	
Permitted Phases	6			2		2	4			8		
Detector Phase	1	6		5	2	2	4	4		8	8	
Switch Phase												
Minimum Initial (s)	5.0	15.0		5.0	15.0	15.0	7.0	7.0		7.0	7.0	
Minimum Split (s)	11.8	31.9		11.9	31.9	31.9	41.8	41.8		34.8	34.8	
Total Split (s)	15.0	78.0		25.0	88.0	88.0	47.0	47.0		47.0	47.0	
Total Split (%)	10.0%	52.0%		16.7%	58.7%	58.7%	31.3%	31.3%		31.3%	31.3%	
Yellow Time (s)	4.8	4.9		4.9	4.9	4.9	3.8	3.8		3.8	3.8	
All-Red Time (s)	2.0	2.0		2.0	2.0	2.0	2.0	2.0		2.0	2.0	
Lost Time Adjust (s)	0.0	0.0		0.0	0.0	0.0	0.0	0.0		0.0	0.0	
Total Lost Time (s)	6.8	6.9		6.9	6.9	6.9	5.8	5.8		5.8	5.8	
Lead/Lag	Lead	Lag		Lead	Lag	Lag						
Lead-Lag Optimize?	Yes	Yes		Yes	Yes	Yes	NI	NLava		N	N I	
Recall Mode	None	Max		None	C-Max	C-Max	None	None		None	None	
Intersection Summary												
Area Type:	Other											
Cycle Length: 150												
Actuated Cycle Length: 1	50											
Offset: 20 (13%), Referen	nced to phase	e 2:WBTL	., Start of	Yellow								
Natural Cycle: 100												
Control Type: Actuated-C	oordinated											
Splits and Phases 1. 9	SW Florida G	Sateway D	r/Centuri	on Ct & I	IS 90							
			., contun					I ⊸†				

▶ Ø1		■ 1 Ø4	
15 s	88 s	47 s	
<b>√</b> Ø5	<u>₩</u> 26	↓ Ø8	
25 s	78 s	47 s	

Kimley-Horn March 2022

## HCM 6th Signalized Intersection Summary 1: SW Florida Gateway Dr/Centurion Ct & US 90

	≯	-	$\mathbf{r}$	•	-	*	1	1	1	1	Ŧ	~
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	5	<b>∱1</b> ≱		ľ	<u></u>	1	ľ	eî 👘		ľ	eî 👘	
Traffic Volume (veh/h)	37	1159	29	80	1332	117	41	2	58	86	3	57
Future Volume (veh/h)	37	1159	29	80	1332	117	41	2	58	86	3	57
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		0.98	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1856	1856	1856	1870	1870	1870
Adj Flow Rate, veh/h	41	1288	32	89	1480	130	46	2	64	96	3	63
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90
Percent Heavy Veh, %	2	2	2	2	2	2	3	3	3	2	2	2
Cap, veh/h	258	2515	62	334	2546	1110	165	6	193	166	9	192
Arrive On Green	0.03	0.71	0.71	0.03	0.72	0.72	0.13	0.13	0.13	0.13	0.13	0.13
Sat Flow, veh/h	1781	3543	88	1781	3554	1548	1325	48	1532	1335	73	1524
Grp Volume(v), veh/h	41	646	674	89	1480	130	46	0	66	96	0	66
Grp Sat Flow(s),veh/h/ln	1781	1777	1854	1781	1777	1548	1325	0	1580	1335	0	1596
Q Serve(g_s), s	0.9	24.8	24.9	2.0	30.3	3.9	4.9	0.0	5.7	10.6	0.0	5.7
Cycle Q Clear(g_c), s	0.9	24.8	24.9	2.0	30.3	3.9	10.6	0.0	5.7	16.3	0.0	5.7
Prop In Lane	1.00		0.05	1.00		1.00	1.00		0.97	1.00		0.95
Lane Grp Cap(c), veh/h	258	1261	1316	334	2546	1110	165	0	199	166	0	201
V/C Ratio(X)	0.16	0.51	0.51	0.27	0.58	0.12	0.28	0.00	0.33	0.58	0.00	0.33
Avail Cap(c_a), veh/h	307	1261	1316	489	2546	1110	362	0	434	364	0	438
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	8.6	9.9	9.9	7.8	10.3	6.6	64.6	0.0	59.8	67.2	0.0	59.7
Incr Delay (d2), s/veh	0.3	1.5	1.4	0.4	1.0	0.2	0.9	0.0	1.0	3.2	0.0	0.9
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/In	0.6	14.2	14.6	1.3	16.2	2.2	3.1	0.0	4.3	6.8	0.0	4.2
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	8.9	11.4	11.4	8.3	11.3	6.8	65.5	0.0	60.7	70.4	0.0	60.7
LnGrp LOS	Α	В	В	Α	В	А	E	Α	E	E	А	E
Approach Vol, veh/h		1361			1699			112			162	
Approach Delay, s/veh		11.3			10.8			62.7			66.4	
Approach LOS		В			В			E			E	
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	10.9	114.4		24.7	11.9	113.4		24.7				
Change Period (Y+Rc), s	6.8	6.9		* 5.8	6.9	6.9		* 5.8				
Max Green Setting (Gmax), s	8.2	81.1		* 41	18.1	71.1		* 41				
Max Q Clear Time (g_c+I1), s	2.9	32.3		12.6	4.0	26.9		18.3				
Green Ext Time (p_c), s	0.0	16.0		0.5	0.1	10.8		0.6				
Intersection Summary												
HCM 6th Ctrl Delay			15.5									
HCM 6th LOS			В									

### Notes

User approved pedestrian interval to be less than phase max green. \* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Kimley-Horn March 2022

## **APPENDIX E** Trip Generation Calculations

#### Table 1: Trip Generation

Land Use		AM Peak I	Hour of Adjac	ent Street	PM Peak I	PM Peak Hour of Adjacent Street			
		Total	In	Out	Total	In	Out		
Existing Development Convenience Store/Gas Station (4-5.5k)	24	I VFP		649	325	324	546	273	273
Existing Development Pass-By	isting Development Pass-By Daily AM PM								
Convenience Store/Gas Station (4-5.5k)	75%	76%	75%	494	247	247	410	205	205
EXISTING SITE - POTENTIAL TOTAL DRIVE		649	325	324	546	273	273		
EXISTING SITE - POTENTIAL PASS-	BY TRIPS			494	247	247	410	205	205
EXISTING SITE - POTENTIAL NEW EXT	155	78	77	136	68	68			
OBSERVED DRIVEWAY VOLUI		201	106	95	220	115	105		
ACTUAL/POTENTIAL DRIVEWAY VOLUMES AD			0.31			0.40			
Proposed Development Convenience Store/Gas Station (5.5-10k)	27	' VFP		853	427	426	726	363	363
Proposed Development Pass-By Convenience Store/Gas Station (5.5-10k)	<u>Daily</u> 75%	<u>AM</u> 76%	<u>PM</u> 75%	648	324	324	544	272	272
PROPOSED SITE - POTENTIAL TOTAL DRIV	EWAY VOLU	JMES		853	427	426	726	363	363
PROPOSED SITE - POTENTIAL TOTAL P	ASS-BY TRI	PS		648	324	324	544	272	272
PROPOSED SITE - POTENTIAL TOTAL NEW	EXTERNAL	TRIPS		205	103	102	182	91	91
POTENTIAL NET NEW TOTAL DRIVEWAY VOLUMES	(PROPOSE	D - EXISTI	VG)	204	102	102	180	90	90
POTENTIAL NET NEW PASS-BY TRIPS (PROF	POSED - EXI	STING)		154	77	77	134	67	67
POTENTIAL NET NEW EXTERNAL TRIPS (PRO		50	25	25	46	23	23		
ADJUSTED NET NEW TOTAL DRIVEWA		64	32	32	72	36	36		
ADJUSTED NET NEW PASS-BY	ADJUSTED NET NEW PASS-BY TRIPS						54	27	27
ADJUSTED NET NEW EXTERNAL		16	8	8	18	9	9		

Trip generation and pass-by reductions were calculated using the following data from ITE's Trip Generation Manual, 11th Edition.

### Convenience Store/ Gas Station (4-5.5k) [ITE 945]

Daily: AM Peak Hour of Adjacent Street: PM Peak Hour of Adjacent Street:  $T = 257.13^{*}(X)$ ; X is vehicle fueling positions

T = 27.04\*(X); X is vehicle fueling positions; (50% in, 50% out)

T = 22.76\*(X); X is vehicle fueling positions; (50% in, 50% out)

Convenience Store/ Gas Station (5.5-10k) [ITE 945]

Daily: AM Peak Hour of Adjacent Street: PM Peak Hour of Adjacent Street:  $T = 345.75^{*}(X)$ ; X is vehicle fueling positions

 $T = 31.60^{*}(X)$ ; X is vehicle fueling positions; (50% in, 50% out)

 $T = 26.90^{*}(X)$ ; X is vehicle fueling positions; (50% in, 50% out)

K:\ORL\_Civil\149880040-Circle K US90 & I75\TPTO\03\_Calcs\[2022-03 - CK Lake City.xlsx]TG (2)

3/17/2022



## APPENDIX F FDOT *Trend* Worksheet

#### FLORIDA DEPARTMENT OF TRANSPORTATION TRANSPORTATION STATISTICS OFFICE 2020 HISTORICAL AADT REPORT

COUNTY: 29 - COLUMBIA

SITE: 0278 - SR 10 400' W. OF I-75

YEAR	AADT	DIRECTION 1	DIRECTION 2	*K FACTOR	D FACTOR	T FACTOR
2020	27000 C	E 13500	W 13500	9.00	54.80	6.80
2019	30000 C	E 15000	W 15000	9.00	54.80	6.20
2018	28000 C	E 14000	W 14000	9.00	54.70	6.20
2017	27500 C	E 14000	W 13500	9.00	55.50	5.80
2016	27000 C	E 13500	W 13500	9.00	53.90	5.40
2015	27500 C	E 14000	W 13500	9.00	54.50	5.70
2014	27000 C	E 13500	W 13500	9.00	54.40	5.90
2013	25000 C	E 12500	W 12500	9.00	55.30	6.40
2012	26000 C	E 13000	W 13000	9.00	54.70	5.50
2011	26000 C	E 13000	W 13000	9.00	53.70	5.30
2010	25500 C	E 12500	W 13000	9.94	54.40	4.90
2009	25000 C	E 12500	W 12500	9.78	54.18	5.30
2008	27000 C	E 13500	W 13500	9.82	54.63	6.20
2007	27500 C	E 13500	W 14000	9.99	54.46	6.40
2006	27000 C	E 13500	W 13500	10.01	55.64	7.00
2005	31500 C	E 15500	W 16000	9.90	56.60	9.80

AADT FLAGS: C = COMPUTED; E = MANUAL ESTIMATE; F = FIRST YEAR ESTIMATE S = SECOND YEAR ESTIMATE; T = THIRD YEAR ESTIMATE; R = FOURTH YEAR ESTIMATE V = FIFTH YEAR ESTIMATE; 6 = SIXTH YEAR ESTIMATE; X = UNKNOWN \*K FACTOR: STARTING WITH YEAR 2011 IS STANDARDK, PRIOR YEARS ARE K30 VALUES



Inst. Number: 201612000647 Book: 1307 Page: 1888 Date: 1/14/2016 Time: 10:37:15 AM Page 1 of 4 Doc Deed: 19775.00 P.DeWitt Cason Clerk of Courts, Columbia County, Florida

This Instrument Was Prepared By, Record and Return to:

John Hotte, Esquire Krinzman, Huss & Lubetsky 110 SE 6<sup>th</sup> Street, 20<sup>th</sup> Floor Fort Lauderdale, FL 33301 Telephone: (954) 761-3454

Property Appraiser Identification No.: Consideration:\$ A Jast:201612000647 Date:1/14/2016 Time:10:37 AM Do: Stamp-Deed:19775.00 \_\_\_\_\_DC,P.DeWitt Cason,Columbia County Page 1 of 4 B:1307 P:1888

### SPECIAL WARRANTY DEED

THIS SPECIAL WARRANTY DEED made this 12th day of January, 2016, by Inn of Lake City, Inc., a Florida corporation ("Grantor"), whose mailing address is 1000 Red Fern Place, Flowood, MS 39232 in favor of GWC Development Partners, LLC, a Florida limited liability company ("Grantee"), whose mailing address is 2682 West Noegel Road, Lake City, FL 32055.

### WITNESSETH:

That Grantor, for and in consideration of the sum of TEN DOLLARS (\$10.00) and other good and valuable consideration paid by Grantee, the receipt and sufficiency whereof are hereby acknowledged, does hereby grant, bargain, sell, alien, remise, release, convey and confirm unto Grantee the real property (the "Property") located in Columbia County, Florida, and more particularly described in Exhibit "A" attached hereto and made a part hereof.

SUBJECT ONLY TO the matters set forth in Exhibit "B" attached hereto and made a part hereof.

TOGETHER with all the tenements, hereditaments and appurtenances belonging or in any way appertaining to the Property, including, without limitation, all of Grantor's right, title and interest, if any, in and to all of the easements, rights, and privileges belonging or in any way appertaining to the Property and/or improvements located thereon.

TO HAVE AND TO HOLD the same in fee simple forever.

AND GRANTOR hereby covenants with Grantee that Grantor is lawfully seized of the Property in fee simple; that Grantor has good right and lawful authority to sell and convey the Property; that, subject to the matters described on <u>Exhibit "B"</u> attached hereto, Grantor does hereby warrant specially the title to the Property; and that Grantor and its successors and assigns will forever warrant and defend the same against the lawful claims of all persons claiming by, through or under Grantor, but against none other.

PD.18666487.2

Inst. Number: 201612000647 Book: 1307 Page: 1889 Date: 1/14/2016 Time: 10:37:15 AM Page 2 of 4 Doc Deed: 19775.00 P.DeWitt Cason Clerk of Courts, Columbia County, Florida

IN WITNESS WHEREOF, Grantor has caused this Special Warranty Deed to be executed by its duly authorized representative on the day and year first above written.

Two Witnesses:

Printed Name:

<u>1</u> 12 12 12 Name: inted

Inn of Lake City, Inc., a Florida corporation

By:

Michael J. Hart, Nice President, Treasurer and Assistant Secretary

STATE OF Mississippi COUNTY OF Hinds

The foregoing instrument was acknowledged before me this  $\frac{1}{2}$  day of  $\frac{1}{2}$  day of

Notary Public, State of Mississippi

Print Name: <u>Suzanna Baker</u> Commission No.: <u>83871</u> My Commission Expires: <u>Januar</u> 21, 2019 [Affix Notary Seal]



Signature Page of Special Warranty Deed

Inst. Number: 201612000647 Book: 1307 Page: 1890 Date: 1/14/2016 Time: 10:37:15 AM Page 3 of 4 Doc Deed: 19775.00 P.DeWitt Cason Clerk of Courts, Columbia County, Florida

### **EXHIBIT "A"**

### **Real Property Description**

COMMENCE at the Northwest corner of Section 35, Township 3 South, Range 16 East, Columbia County, Florida as established by B.G. Moore, PLS No. 439 and run thence S 06°22'00" W. along the West line of said Section 35, 1894.50 feet to the West Limited Access Right of Way of Interstate No. 75, thence run Southerly and Westerly along said West Limited Access Right of Way the following courses. S 24°54'32" E, 472.32 feet to the POINT OF BEGINNING, S 24°54'32" E, 940.25 feet; S 15°12'50" E, 512.06 feet; S 06°01'43" E, 335.81 feet; S 36°55'36" W, 54.60 feet to the Northerly Right of Way of West U.S. Highway 90 and the end of said courses; thence S 80°47'35" W, along said Northerly Right of Way, 371.77 feet; thence S 08°51'10" E, along said Northerly Right of Way, 22.18 feet; thence S 80°47'36" W, along said Northerly Right of Way, 73.15 feet; thence N 08°55'17" W, 150.09 feet; thence S 80°42'55" W, 150.25 feet; thence N 08°52'22" W, 60.12 feet; thence S 80°53'59" W, 79.99 feet; thence S 08°59'18" E, 210.15 feet to the aforesaid Northerly Right of Way; thence S 80°47'36" W, along said Northerly Right of Way, 26.39 feet to a point of a curve; thence run Westerly along the arc of said curve concave to the North having a radius of 3224.04 feet, a central angle of 05°24'20", a chord bearing and distance of S 83°26'26" W 304.06 feet, an arc distance of 304.18 feet to the aforesaid West line of Section 35; thence N 06°22'00" E, along said West line, 1784.01 feet; thence N 65°09'42" E. 286.69 feet to the POINT OF BEGINNING.

LESS AND EXCEPT the parcel described in O.R. Book 1284, Page 229, of the Official Records of Columbia County, Florida

Inst. Number: 201612000647 Book: 1307 Page: 1891 Date: 1/14/2016 Time: 10:37:15 AM Page 4 of 4 Doc Deed: 19775.00 P.DeWitt Cason Clerk of Courts, Columbia County, Florida

### EXHIBIT "B"

### Exceptions

- 1. Taxes and assessments for the year 2016 and subsequent years, which are not yet due and payable.
- 2. Any land use, zoning and building laws and ordinances.
- 3. Any declaration of covenants, conditions and restrictions, or other recorded restrictions.
- 4. Any right, title, interest, claim, violation, variation, encumbrance, encroachment, fact, matters or other adverse circumstance affecting title revealed, or that should have been revealed, by that certain ALTA/ACSM survey of the Property by JBrown Professional Group Inc. dated October 16, 2015, as revised (Proj. No. 366-15-01).
- 5. Any obligations, rights and other matters related to, and any agreements with and requirements of the State of Florida or other governmental agency regarding, the remediation of certain environmental issues on the Property by or on behalf of the State of Florida or a political subdivision thereof under a state-funded cleanup program(s).
- 6. Rights-of-way, utility easements, other easements, restrictions and other restrictive and/or use covenants filed of record and other matters which are revealed by a title search or title commitments, including the following:
  - a. Easement(s) in favor of Mississippi Management, Inc. set forth in instrument(s) recorded in Official Records Book 634, Page 338.
  - b. Easement(s) in favor of Shell Oil Company set forth in instrument(s) recorded in Official Records Book 674, Page 104.
  - c. Easement(s) in favor of American Telephone and Telegraph Company set forth in instrument(s) recorded in Official Records Book 723, Page 162.
  - d. Easement(s) in favor of The City of Lake City, Florida set forth in instrument(s) recorded in Official Records Book 776, Page 1724.
  - e. Easement contained in Deed recorded in Official Records Book 685, Page 38.
  - f. Easement recorded in Official Records Book 960, Page 1492.
  - g. Easement recorded in Official Records 104, Page 118, and in Official Records Book 361, Page 499.
  - h. Easement for ingress and egress recorded in Official Records Book 370, Page 337.
  - i. Easement(s) in favor of Florida Power and Light Company set forth in instrument(s) recorded in Official Records Book 361, Page 499.
  - j. Easement(s) in favor of The City of Lake City, Florida set forth in instrument(s) recorded in Official Records Book 559, Page 229.

## **Columbia County Property Appraiser**

2022 Working Values updated: 6/9/2022

2020-01-06 \$1,400,000 WD-VIQ-01

\$525,000 WD-V-Q-01 \$2,015,200 WD-I+U-37

W US HIGHWAY 90

RCode

Bldg Value

022 \$2,150.0 WD-I-U-

Jeff Hampton	county	Troperty A	ppi ais										2024	up
Parcel: «	35-3S-16-0	02524-102 (1049	94) >>>				Aerial	Viewer	Pictometer	y Googl	e Maps			
Owner & Pr	operty Info	0					<b>O</b> 20 <sup>2</sup>	19 0 201	6 0 2013	0 2010	0 2007	0 2005	Sales	
Owner	GWC DEV 2682 NW N LAKE CITY	<b>ELOPMENT PAR</b> NOEGEL RD Y, FL 32055	TNERS LI	-C			+	1				KON	EDICAL G	ENTERLO
Site									ALAN-1	8		-	A second	
Description*	LOT 2 GATE REPLAT OF 176 & 177.	EWAY CROSSING LOTS 2, 3 & 11 O	S/D A REP F GATEWA	LAT OF LOTS 2 Y CROSSING II	2 & 3. NKA LO N PLAT BK 9	OT 2 A PGS	T.	Z		75 TOW	ANIS IN	12-120-02-11 12-120-02-11	2	
Area	1.49 AC			S/T/R	35 <b>-</b> 3S	-16	. Low hit	20		US	31	W. >> U.3	7	2(
Use Code**	VACANT C	OMMERCIAL (10	00)	Tax Distr	rict 1		17	뮠	water and	章	11116	2 V.	A. and	M
*The <u>Description</u> **The <u>Use Code</u> office. Please co	above is not to is a FL Dept. of ntact your city o	be used as the Legal [ Revenue (DOR) code r county Planning & Zo	Description fo and is not ma ning office for	r this parcel in any aintained by the Pro r specific zoning in	legal transactio operty Appraise formation.	on. er's		URIONICI		MAY 90		Nalual C	2024 \$52:	1-07-09 5,000
Property &	Assessme	ent Values					2	020-03-31		Ram	8	6	8	\$2.0 WD
202	21 Certified	Values		2022 Working	g Values		v - V	VD-I-U-37		P	SL	13	1.25	Carter I.
Mkt Land		\$515,968	Mkt Land		\$7	46,396			1	-1.1	Contra Par			W US HIG
Ag Land		\$0	Ag Land			\$0				a	-			Receipt
Building		\$0	Building			\$0	12-				ST.	A MARINE	STOR N	高一
XFOB		\$0	XFOB			\$0	-		1			2 81		T
Just		\$515,968	Just		\$7	46,396		11.1-	1	- Phy	2 Co.	6 18	10 1	018
Class		\$0	Class			\$0				ALV .	12	2	1. 1.	Z.
Appraised		\$515,968	Appraise	d	\$7	46,396	- AVIII - CALIFORNIA		APT2	1		8	1 8 F	JSI
SOH Cap [?]		\$0	SOH Cap	» [?]	\$1	78,831	FR			1		0	18 18	多
Assessed		\$515,968	Assessed	k	\$7	46,396	1	2021-04-2		BAN THE	- P.S.	012		93
Exempt		\$0	Exempt			\$0	A Mar	WD-1-0-0	AL .	14		P +	Par	Ran
Total Taxable	other:\$(	county:\$515,968 city:\$515,968 0 school:\$515,968	Total Taxable	other:	county:\$5 city:\$5 \$0 school:\$7	567,565 567,565 746,396							10	
💌 Sales Hi	story													
Sale [	Date	Sale Pric	e	Book/F	⊃age	Dee	əd	V/I		Quali	fication (Co	odes)		R
		1	I			NON	E							
🔽 Building	Characte	ristics												
Blo	la Sketch		Descripti	on*	Year	r Blt		Base S	F	β	ctual SF			Bldg Valu
	<u> </u>	I				NON	E							

89

#### Columbia County Property Appraiser

### Extra Features & Out Buildings (Codes)

C	Code	Desc	Year Blt	Value	Units	Dims
			NONE			
Land E	Breakdown					
Code		Desc	Units	Adj	ustments Eff	f Rate Land Val
1000	VACANT	COMMERCIAL (MKT)	64,904.000 SF (1.490	AC) 1.0000/1.	.0000 1.0000/ / \$1	2 /SF \$746,39
) Columbia Cou	unty Property Apprai	ser   Jeff Hampton   Lake City, Flo	rida   386-758-1083			by: GrizzlyLc

# Kimley »Horn

April 1, 2022

Suwanee River Water Management District 9225 CR 49 Live Oak, FL 32060

### Subject: Circle K – US 90 & I-75 De-Minimis Exemption Letter Project Name: Circle K – Circle K – US 90 & I-75 County: Columbia Sec/Twp/Rge: S35 T3S R16E

To Whom it May Concern:

The proposed 3.47-acre Circle K – US 90 & I-75 project lies within the previously issued ERP No. 023-226410 and connects into the master project "Gateway Crossing" stormwater system. The project site is located at the northeast corner of the intersection of US Highway 90 and Centurion Court in the City of Lake City, Columbia County, Florida. We understand that this development is under SRWMD ERP No. 023-226410, and is shown as a portion of Basin DA-1 of the Gateway Crossing project. The proposed development will contain the addition of a 2,064 square foot Circle K high speed diesel canopy, with fueling stations, and associated infrastructure to the existing Circle K project constructed in 2016.

The proposed 3.47-acre Circle K – US 90 & I-75 lies within the previously issued ERP No. 023-226410 as stated above. We understand that this site is permitted up to 75% impervious area per ERP No. 023-226410.

As seen in the attached construction plans the Circle K – US 90 & I-75 project is proposing 2.63 acres (+/-114,580 SF) of impervious surface area to discharge into the system permitted under SRWMD ERP No. 023-226410. As the Circle K – US 90 & I-75 project is proposing impervious area less than or equal to the maximum allowed impervious surface area, and is connected to the master stormwater system that was approved in the Gateway Crossing project, ERP No. 023-226410, the project meets the requirements for a De-Minims Exemption.

If you have any questions, of if you require additional information, please do not hesitate to contact our office at (407) 409-7002.

Very truly yours,

KIMLEY-HORN AND ASSOCIATES, INC.

Jarod C. Stubbs, P.E. Project Engineer 6/9/22, 1:59 PM

## Columbia County Tax Collector

generated on 6/9/2022 1:59:44 PM EDT

92

**Tax Record** 

Last Update: 6/9/2022 1:58:23 PM EDT

Register for eBill

Columbia County Tax Collector

### Ad Valorem Taxes and Non-Ad Valorem Assessments

The information contained herein does not constitute a title search and should not be relied on as such.

		Tax T	уре	Tax	Year
R02524-102		REAL E	2021		
Mailing Address GWC DEVELOPMENT PARTNER	S LLC	Propert	y Address		
LOOZ NW NOEGEL KD LAKE CITY EI 32055					
LARE CITI PL 52055		GEO Num	<b>ber</b>		
		202210-	02524-102		
Exempt Amount		Taxable	Value		
See Below		See B	elow		
Exemption Detail	Mi 11	age Code	E	scrow Code	<b>`</b>
NO EXEMPTIONS	001	age code			-
	1	docarintic	221		
Legal Description (clic)	K IOT IUII	GESCLIDLIC	,,,,,		
Legal Description (clic	<u>k for full</u>		<u>, , , , , , , , , , , , , , , , , , , </u>		OFIOTO
Legal Description (clic 35-38-16 1000/10001.03	<u>k för full</u> Acres LOT	2 GATEWAY C	ROSSING S/D	A REPLAT	OF LOTS
Legal Description (clic) 35-38-16 1000/10001.03 2 & 3.	<b>k for full</b> Acres LOT	2 GATEWAY (	ROSSING S/D	A REPLAT	OF LOTS
<u>Legal Description (clic</u> 35-38-16 1000/10001.03 2 & 3.	Acres LOT Ad Va	2 GATEWAY C	ROSSING S/D	A REPLAT	OF LOTS
Legal Description (clic) 35-3S-16 1000/10001.03 2 & 3.	Acres LOT Ad Val	2 GATEWAY C	ROSSING S/D	A REPLAT	OF LOTS
<u>Legal Description (clic</u> 35-3S-16 1000/10001.03 2 & 3. Taxing Authority	Acres LOT Ad Va Rate	2 GATEWAY ( lorem Taxes Assessed Value	ROSSING S/D Exemption Amount	A REPLAT	OF LOTS Taxes Levied
Legal Description (clic 35-3S-16 1000/10001.03 2 & 3. Taxing Authority BOARD OF COUNTY COMMISSIONERS	Acres LOT Ad Va Rate 7.8150	Corem Taxes Assessed Value 515,968	ROSSING S/D Exemption Amount	A REPLAT Taxable Value \$515,968	OF LOTS Taxes Levied \$4,032.29
Legal Description (clic 35-3S-16 1000/10001.03 2 & 3. Taxing Authority BOARD OF COUNTY COMMISSIONERS CITY OF LAKE CITY	k for full Acres LOT Ad Va Rate 7.8150 4.9000	Corem Taxes Assessed Value 515,968 515,968	ROSSING S/D Exemption Amount	A REPLAT <b>Taxable</b> <b>Value</b> \$515,968 \$515,968	OF LOTS <b>Taxes</b> <b>Levied</b> \$4,032.29 \$2,528.24
Legal Description (clic 35-3S-16 1000/10001.03 2 & 3. Taxing Authority BOARD OF COUNTY COMMISSIONERS CITY OF LAKE CITY COLUMBIA COUNTY SCHOOL BOARD	<b>Ad Val</b> <b>Ad Val</b> <b>Rate</b> 7.8150 4.9000	Corem Taxes Assessed Value 515,968 515,968	ROSSING S/D Exemption Amount	A REPLAT <b>Taxable</b> <b>Value</b> \$515,968 \$515,968	OF LOTS <b>Taxes</b> <b>Levied</b> \$4,032.29 \$2,528.24
Legal Description (clic 35-3S-16 1000/10001.03 2 & 3. Taxing Authority BOARD OF COUNTY COMMISSIONERS CITY OF LAKE CITY COLUMBIA COUNTY SCHOOL BOARD DISCRETIONARY	<b>Ad Val</b> Acres LOT <b>Ad Val</b> <b>Rate</b> 7.8150 4.9000 0.7480	Corem Taxes Assessed Value 515,968 515,968 515,968	ROSSING S/D Exemption Amount 0 0	A REPLAT <b>Taxable</b> <b>Value</b> \$515,968 \$515,968 \$515,968	OF LOTS Taxes Levied \$4,032.29 \$2,528.24 \$385.95
Legal Description (clic 35-3S-16 1000/10001.03 2 & 3. Taxing Authority BOARD OF COUNTY COMMISSIONERS CITY OF LAKE CITY COLUMBIA COUNTY SCHOOL BOARD DISCRETIONARY LOCAL	<b>Ad Val</b> Acres LOT <b>Ad Val</b> <b>Rate</b> 7.8150 4.9000 0.7480 3.6430	2 GATEWAY C lorem Taxes Assessed Value 515,968 515,968 515,968 515,968	ROSSING S/D Exemption Amount 0 0 0	A REPLAT <b>Taxable</b> <b>Value</b> \$515,968 \$515,968 \$515,968	OF LOTS Taxes Levied \$4,032.29 \$2,528.24 \$385.95 \$1,879.67
Legal Description (clic 35-3S-16 1000/10001.03 2 & 3. Taxing Authority BOARD OF COUNTY COMMISSIONERS CITY OF LAKE CITY COLUMBIA COUNTY SCHOOL BOARD DISCRETIONARY LOCAL CAPITAL OUTLAY	k for full Acres LOT Ad Val Rate 7.8150 4.9000 0.7480 3.6430 1.5000	2 GATEWAY C lorem Taxes Assessed Value 515,968 515,968 515,968 515,968 515,968	Exemption Amount	A REPLAT <b>Taxable</b> <b>Value</b> \$515,968 \$515,968 \$515,968 \$515,968 \$515,968	OF LOTS <b>Taxes</b> <b>Levied</b> \$4,032.29 \$2,528.24 \$385.95 \$1,879.67 \$773.95
Legal Description (clic 35-3S-16 1000/10001.03 2 & 3. Taxing Authority BOARD OF COUNTY COMMISSIONERS CITY OF LAKE CITY COLUMBIA COUNTY SCHOOL BOARD DISCRETIONARY LOCAL CAPITAL OUTLAY SUWANNEE RIVER WATER MGT DIST	k for full Acres LOT Ad Val Rate 7.8150 4.9000 0.7480 3.6430 1.5000 0.3615	2 GATEWAY C lorem Taxes Assessed Value 515,968 515,968 515,968 515,968 515,968 515,968	ROSSING S/D Exemption Amount 0 0 0 0 0 0 0 0 0 0	A REPLAT <b>Taxable</b> <b>Value</b> \$515,968 \$515,968 \$515,968 \$515,968 \$515,968 \$515,968	OF LOTS <b>Taxes</b> <b>Levied</b> \$4,032.29 \$2,528.24 \$385.95 \$1,879.67 \$773.95 \$186.52
Legal Description (clic 35-3S-16 1000/10001.03 2 & 3. Taxing Authority BOARD OF COUNTY COMMISSIONERS CITY OF LAKE CITY COLUMBIA COUNTY SCHOOL BOARD DISCRETIONARY LOCAL CAPITAL OUTLAY SUWANNEE RIVER WATER MGT DIST LAKE SHORE HOSPITAL AUTHORITY	k for full Acres LOT Ad Va Rate 7.8150 4.9000 0.7480 3.6430 1.5000 0.3615 0.0000	2 GATEWAY C lorem Taxes Assessed Value 515,968 515,968 515,968 515,968 515,968 515,968 515,968	Exemption Amount 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	A REPLAT           Taxable           Value           \$515,968           \$515,968           \$515,968           \$515,968           \$515,968           \$515,968           \$515,968           \$515,968           \$515,968           \$515,968           \$515,968           \$515,968	OF LOTS Taxes Levied \$4,032.29 \$2,528.24 \$385.95 \$1,879.67 \$773.95 \$186.52 \$0.00
Legal Description (clic 35-3S-16 1000/10001.03 2 & 3. Taxing Authority BOARD OF COUNTY COMMISSIONERS CITY OF LAKE CITY COLUMBIA COUNTY SCHOOL BOARD DISCRETIONARY LOCAL CAPITAL OUTLAY SUWANNEE RIVER WATER MGT DIST LAKE SHORE HOSPITAL AUTHORITY Total Millage	k for full Acres LOT Ad Va Rate 7.8150 4.9000 0.7480 3.6430 1.5000 0.3615 0.0000 18.96	2 GATEWAY C lorem Taxes Assessed Value 515,968 516,968 516,968 516,968 516,9	CROSSING S/D CROSSING S/D Exemption Amount 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	A REPLAT <b>Taxable</b> <b>Value</b> \$515,968 \$515,968 \$515,968 \$515,968 \$515,968 \$515,968 \$515,968 \$515,968 \$515,968	OF LOTS Taxes Levied \$4,032.29 \$2,528.24 \$385.95 \$1,879.67 \$773.95 \$186.52 \$0.00 9,786.62
Legal Description (clic 35-3S-16 1000/10001.03 2 & 3. Taxing Authority BOARD OF COUNTY COMMISSIONERS CITY OF LAKE CITY COLUMBIA COUNTY SCHOOL BOARD DISCRETIONARY LOCAL CAPITAL OUTLAY SUWANNEE RIVER WATER MGT DIST LAKE SHORE HOSPITAL AUTHORITY Total Millage	k for full Acres LOT Ad Va Rate 7.8150 4.9000 0.7480 3.6430 1.5000 0.3615 0.0000 18.96	2 GATEWAY C lorem Taxes Assessed Value 515,968 516,968 516,968 516,968 516,9	Exemption Amount 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0	A REPLAT <b>Taxable</b> <b>Value</b> \$515,968 \$515,968 \$515,968 \$515,968 \$515,968 \$515,968 \$515,968 \$515,968 \$515,968 \$515,968	OF LOTS Taxes Levied \$4,032.29 \$2,528.24 \$385.95 \$1,879.67 \$773.95 \$186.52 \$0.00 9,786.62

Columbia County Tax Collector

Code XLCF	<b>Levying Authority</b> CITY FIRE ASSESSMENT			<b>Amount</b> \$50.40
		Total Assessme	ents	\$50.40
		Taxes & Assessm	ents	\$9,837.02
		If Paid By		Amount Due
				\$0.00

Date Paid	Transaction	Receipt	Item	Amount Paid
12/29/2021	PAYMENT	1200971.0006	2021	\$9,541.91

Prior Years Payment History

	Prior Year Taxes Due
NO DELINQUENT TAXES	

93



## REVIEW REPORT TO PLANNING AND ZONING, BOARD OF ADJUSTMENT AND HISTORICAL COMMITTEES' BY STAFF FOR SITE PLAN REVIEW, SPECIAL EXCEPTIONS, VARIANCES, COMPREHENSIVE PLAN AMENDMENTS/ ZONING AND CERTIFICATE OF APPROPRIATENESS

Date: 06/15/2022

Request Type: Site Plan Review (SPR) 🗹 Special Exception (SE) 🗌 Variances (V)
Comprehensive Plan Amendment/Zoning (CPA/Z) Certificate of Appropriateness (COA)
Project Number: SPR22-15
Project Name: Circle K-US 90 and I75 (Gateway Crossings)
Project Address: 143 NW Centurion CT, Lake City FL
Project Parcel Number: 35-3S-16-02524-001,102, and 111
Owner Name: Daniel Hotte of GWC Development Partners, LLC
Owner: Address: 2682 W Noegel RD
Owner Contact Information: telephone number 407-580-5173 e-mail dberry@shafferconst.com
Owner Agent Name: Jarod Stubbs P.E.
Owner Agent Address: 180 S. Orange Ave, Suite 1000 Orlando FL 32801
Owner Agent Contact Information: telephone 407-409-7002 e-mail jarod.stubbs@kimley-horne.com

The City of Lake City staff has reviewed the application and documents provided for the above request and have determined the following:

## Growth Management – Building Department, Planning and Zoning, Code Enforcement, Permitting

Building Department: Approved Disapproved Reviewed by:				
Planning and Zoning: Approve Disapprove Reviewed by: Robert Angelo				
Comments: No Concerns at this time				
No Concerns at this time				
Business License: Approve Disapprove Reviewed by: Marshall Sova				
Comments: No Concerns at this time				
Code Enforcement: Approve Disapprove Reviewed by: Marshall Sova				
Comments: No Concerns at this time				
Permitting: Approve Disapprove Reviewed by: Ann Jones				
Comments: No Concerns at this time				
No Concerns at this time				

## Utilities - Water, Sewer, Gas, Water Distribution/Collections, Customer Service

Water Department: Approved Disapproved Reviewed by:
Sewer Department: Approved Disapproved Reviewed by:
Gas Department: Approved Disapproved Reviewed by: Steve Brown
WaterDistribution/Collection:Approved
Comments:
If they do not use the taps in place they will be required to make new ones and
cut and cap sewer and dig to water main and shut off before construction.
Customer Service: Approved Disapproved Reviewed by: Shasta Pelham
Utility Plan 6.0 dated 05/04/22 references a 1" water meter and an existing 6" sewer tap. A tap application would be required to access city utilities.
The tap fees, impact fees and utility deposits will be calculated upon approval of the tap application. A floor plan with detailed fixture units of the restroom addition
is required. City utilities border the property; locates must be obtained to ensure that the utility infrastructure is not damaged or obstructed.

Public Safety – Public Works, Fire Department, Police Department
Public Works: Approved Disapproved Reviewed by: Steve Brown
Comments: No Concerns at this time
Fire Department: Approve Disapprove Reviewed by. Assistant Chief Boozer
Comments: No Concerns at this time
Police Department: Approve Disapprove Reviewed by Assistant Chief Andy
Comments: No Concerns at this time

Please provide separate pages for comments that will not fit in provided spaces and please label the pages for your department and for the project.

### File Attachments for Item:

iii. Site Plan Review - SPR-22-14 - Parker Neely (Agent: Chris Potts, P.E.) Sonic Drive In



GROWTH MANAGEMENT 205 North Marion Ave. Lake City, FL 32055 Telephone: (386)719-5750 E-Mail: growthmanagement@lcfla.com

FOR PLANNING USE ONLY	
Application #	
Application Fee \$200.00	
ReceiptNo	
Filing Date	
Completeness Date	
-	

# **Site Plan Application**

## A. PROJECT INFORMATION

- 1. Project Name: SONIC DRIVE-IN
- 2. Address of Subject Property:\_\_\_\_NW CENTURION CT, LAKE CITY, FL, 32055
- 3. Parcel ID Number(s): 35-3S-16-02524-103
- 4. Future Land Use Map Designation: <u>COMMERCIAL</u>
- 5. Zoning Designation: COMMERCIAL HIGHWAY INTERCHANGE
- 6. Acreage: 1.21
- 7. Existing Use of Property: VACANT
- 8. Proposed use of Property: COMMERICAL RESTAURANT
- 9. <u>Typ</u>e of Development (Check All That Apply):
  - () Increase of floor area to an existing structure: Total increase of square footage\_\_\_

  - ) Relocation of an existing structure: Tc square footage \_

## B. APPLICANT INFORMATION

- 1. Applicant Status

   □ Owner (title holder)

   ☑ Agent
- 2. Name of Applicant(s): Chris Potts, PE
   Title: Director of Civil Engineering

   Company name (if applicable): JBPro
   Mailing Address: 3530 NW 43rd ST

   City: Gainesville
   State: FL
   Zip: 32606

   Telephone: (352) 375-8999
   Fax: ()
   Email: chris.potts@jbpro.com

 Telephone: (352) 375-8999
 Fax: ()
 Email: chris.potts@jbpro.com

 PLEASE NOTE: Florida has a very broad public records law. Most written communications to or from government officials regarding government business is subject to public records requests. Your e-mail address and communications may be subject to public disclosure.

3. If the applicant is agent for the property owner\*.

 Property Owner Name (title holder):
 Parker Neely

 Mailing Address:
 2682 NW NOEGEL ROAD

 City:
 LAKE CITY

 State:
 FL

 Zip:
 32055

 Telephone:
 (704)
 577-2475

 Fax:
 \_\_\_\_\_
 Email:
 DNEELY@HIGHCOTTON-CEP.COM

PLEASE NOTE: Florida has a very broad public records law. Most written communications to or from government officials regarding government business is subject to public records requests. Your e-mail address and communications may be subject to public disclosure. \*Must provide an executed Property Owner Affidavit Form authorizing the agent to act on behalf of the property owner.

### C. ADDITIONAL INFORMATION

	if yes, is the contract/option contingent of absolute.
2.	Has a previous application been made on all or part of the subject property? □Yes  凶No_
	Future Land Use Map Amendment:    □Yes
	Future Land Use Map Amendment Application No.
	Site Specific Amendment to the Official Zoning Atlas (Rezoning): $\Box$ Yes/NoX
	Site Specific Amendment to the Official Zoning Atlas (Rezoning) Application No.
	Variance:√Yes□No
	Variance Application No
	Special Exception:  _Yes  \scale No
	Special Exception Application No

### D. ATTACHMENT/SUBMITTAL REQUIREMENTS

- 1. Vicinity Map Indicating general location of the site, abutting streets, existing utilities, complete legal description of the property in question, and adjacent land use.
- 2. Site Plan Including, but not limited to the following:
  - a. Name, location, owner, and designer of the proposed development.
  - b. Present zoning for subject site.
  - c. Location of the site in relation to surrounding properties, including the means of ingress and egress to such properties and any screening or buffers on such properties.
  - d. Date, north arrow, and graphic scale not less than one inch equal to 50 feet.
  - e. Area and dimensions of site (Survey).
  - f. Location of all property lines, existing right-of-way approaches, sidewalks, curbs, and gutters.
  - g. Access to utilities and points of utility hook-up.
  - h. Location and dimensions of all existing and proposed parking areas and loading areas.
  - i. Location, size, and design of proposed landscaped areas (including existing trees and required landscaped buffer areas).
  - j. Location and size of any lakes, ponds, canals, or other waters and waterways.
  - k. Structures and major features fully dimensioned including setbacks, distances between structures, floor area, width of driveways, parking spaces, property or lot lines, and percent of property covered by structures.
  - l. Location of trash receptacles.
  - m. For multiple-family, hotel, motel, and mobile home park site plans:
    - i. Tabulation of gross acreage.
    - ii. Tabulation of density.
    - iii. Number of dwelling units proposed.
    - iv. Location and percent of total open space and recreation areas.
    - v. Percent of lot covered by buildings.

- vi. Floor area of dwelling units.
- vii. Number of proposed parking spaces.
- viii. Street layout.
- ix. Layout of mobile home stands (for mobile home parks only).
- 3. Stormwater Management Plan—Including the following:
  - a. Existing contours at one foot intervals based on U.S. Coast and Geodetic Datum.
  - b. Proposed finished elevation of each building site and first floor level.
  - c. Existing and proposed stormwater management facilities with size and grades.
  - d. Proposed orderly disposal of surface water runoff.
  - e. Centerline elevations along adjacent streets.
  - f. Water management district surface water management permit.
- 4. Fire Department Access and Water Supply Plan: The Fire Department Access and Water Supply Plan must demonstrate compliance with Chapter 18 of the Florida Fire Prevention Code, be located on a separate signed and sealed plan sheet, and must be prepared by a professional fire engineer licensed in the State of Florida. The Fire Department Access and Water Supply Plan must contain fire flow calculations in accordance with the Guide for Determination of Required Fire Flow, latest edition, as published by the Insurance Service Office ("ISO") and/or Chapter 18, Section 18.4 of the Florida Fire Prevention Code, whichever is greater.
- 5. Concurrency Impact Analysis: Concurrency Impact Analysis of impacts to public facilities. For commercial and industrial developments, an analysis of the impacts to Transportation, Potable Water, Sanitary Sewer, and Solid Waste impacts are required.
- 6. Comprehensive Plan Consistency Analysis: An analysis of the application's consistency with the Comprehensive Plan (analysis must identify specific Goals, Objectives, and Policies of the Comprehensive Plan and detail how the application complies with said Goals, Objectives, and Policies).
- 7. Legal Description with Tax Parcel Number (In Word Format).
- 8. Proof of Ownership (i.e. deed).
- 9. Agent Authorization Form (signed and notarized).
- 10. Proof of Payment of Taxes (can be obtained online via the Columbia County Tax Collector's Office).
- 11. Fee. The application fee for a Site and Development Plan Application is \$200.00. No application shall be accepted or processed until the required application fee has been paid.

### **NOTICE TO APPLICANT**

All eleven (11) attachments are required for a complete application. Once an application is submitted and paid for, a completeness review will be done to ensure all the requirements for a complete application have been met. If there are any deficiencies, the applicant will be notified in writing. If an application is deemed to be incomplete, it may cause a delay in the scheduling of the application before the Planning & Zoning Board.

A total of ten (10) copies of proposed site plan application and all support materials must be submitted along with a PDF copy on a CD. See City of Lake City submittal guidelines for additional submittal requirements.

THE APPLICANT ACKNOWLEDGES THAT THE APPLICANT OR AGENT MUST BE PRESENT AT THE PUBLIC HEARING BEFORETHE PLANNING AND ZONING BOARD, AS ADOPTED IN THE BOARD RULES AND PROCEDURES, OTHERWISE THE REQUEST MAY BE CONTINUED TO A FUTURE HEARING DATE.

I hereby certify that all of the above statements and statements contained in any documents or plans submitted herewith are true and accurate to the best of my knowledge and belief.

CHRISTOONER Tots

Applicant/Agent Name (Type or Print)

Applicant/Agent Signature

Applicant/Agent Name (Type or Print)

Applicant/Agent Signature

STATE OF FLORIDA COUNTY OF Alochum

The foregoing instrument was acknowledged before me this  $\underline{0^{\text{th}}}$  day of  $\underline{1000}$ ,  $20\underline{22}$ , by (name of person acknowledging).

(NOTARY SEAL or State of Florida Comm# HH075654 Expires 12/29/2024

Personally Known <u>X</u> OR Produced Identification <u></u>Type of Identification Produced

ature of Notary Jennifer Newbea

6/6/22

Date

Date

Printed Name of Notary

City of Lake City – Growth Management Department 205 North Marion Ave, Lake City, FL 32055 **(386)** 719-5750

# GENERAL DEVELOPMENT NOTES

1. PROPOSED NAME OF PROJECT:	
	SONIC DRIVE-IN
2. PROJECT DESCRIPTION:	RESTAURANT
3. <u>OWNER</u> :	PARKER NEELY
4. <u>DEVELOPER</u> :	HIGH COTTON EQUITIES; 2682 NW NOEGEL ROAD, LAKE CITY, FL 32055
5. <u>CIVIL ENGINEER</u> :	JBROWN PROFESSIONAL GROUP INC.; CONTACT: CHRIS POTTS, P.E. (352) 375-8999,
	UBROWN PROFESSIONAL GROUP INC CONTACT: TROY WRIGHT PSM (352) 375-8999
7. VICINITY MAP:	SEE MAP THIS SHEET
8. TAX PARCEL NO'S.:	35-3S-16-02524-103
9. PARCEL AREA:	1.21 ACRES IN SIZE
10. PROJECT AREA:	1.21 ACRES
11. <u>SECTION</u> :	35
12. TOWNSHIP:	3S
13. <u>RANGE</u> :	16
14. <u>FUTURE LAND USE:</u>	COMMERCIAL HIGHWAY INTERCHANGE (CHI)
15. <u>ZONING</u> :	COMMERCIAL HIGHWAY INTERCHANGE (CHI)
16. <u>FLOODPLAIN</u> :	THIS PROJECT IS NOT LOCATED IN A FEDERAL FLOOD ZONE.
17. <u>DEVELOPMENT DATA</u> :	A. TOTAL SITE AREA: 1.21 AC / 52708 SF
	B. BUILDING AREA: 1227 SF
	C. PAVEMENT & SIDEWALK AREA: 29989 SF
	D. TOTAL IMPERVIOUS AREA: 31216 SF
	E. UPEN AREA: 21492 SF
IO. <u>PARKING REQUIREMENTS:</u>	A. I SPACE/EA S SEATS OF SEATING AREA
	B. PROPOSED SEATING: 24 OUTDOOR SEATS / NO INDOOR SEATING
	C. PARKING REQUIRED: 8 SPUTS
19. REQUIRED SETBACKS:	A. PER CHI ZONING CATEGORY. THE MINIMUM YARD REQUIREMENTS PER LAKE CITY
	LDR'S SECTION 4.15.7 ARE AS FOLLOWS:
	1. FRONT YARD = 30 FT
	2. SIDE YARD = 30 FT
	3. REAR YARD = 30 FT
	B. ALL BUILDINGS ARE SETBACK GREATER THAN 30 FT FROM FRONT, SIDE, AND REAR
	PROPERTY LINES.
REVISI	
REVISI DESCRIPTION	ONS DRWN APPR A Potts LINGINGER CHRISTOPHER A POTTS, P.E.
REVISI DESCRIPTION	ONS       DRWN APPR       Christopher       Chritopher       C
	ONS       DRWN APPR       Christopher





N.T.S.



## SONIC RESTAURANT LAKE CITY, FLORIDA

**JUNE 2022** PROJECT NO: 366-22-02

DATE:

SHEET NO:

# COVER SHEET

GATEWAY CROSSING LOT 3 SONIC RESTAURANT			
SHEET INDEX			
SHEET NO.	TITLE		
C0.0	COVER SHEET		
C0.1	LEGEND, ABBREVIATIONS, AND NOTES		
C1.0	C1.0 DEMOLITION PLAN		
C1.1	C1.1 EROSION CONTROL PLAN		
C2.0	DIMENSION PLAN		
C3.0	PAVING GRADING & DRAINAGE PLAN		
C4.0	UTILITY PLAN		
C5.0	DETAILS AND NOTES		
V-1	BOUNDARY AND TOPOGRAPHIC SURVEY		

		PRI CONSIDE MUS	INTED COPIES ERED SIGNED A ST BE VERIFIED	DF THIS DOCUMENT ARE NOT ND SEALED, AND THE SIGNATURE ON ANY ELECTRONIC COPIES.	STATE OF SCORIDA SSIONALENGE	CIVIL ENGINEER
		THI SIGNEI ON	IIS DOCUMENT ED AND SEALED JUNE 06, 2022	HAS BEEN ELECTRONICALLY BY CHRISTOPHER A. POTTS, PE JSING A DIGITAL SIGNATURE.		
		ENGINE OF REC	EER CORD:	CHRISTOPHER A. POTTS, P.E. FLORIDA LICENSE NO. 73842	No. 73842 %	
NO. DATE DESCRIPTION	<u>REVIJIUNJ</u>	DRWN APPR			OPHER A. A. A. LICENSE	
<del>ن</del>	REVISIONS					Í
7990						
-22-0.						
0 -						
ateway C						
ossing La						
10. ALL SLOPES OF 4:1 OR G	REATER SHALL BE SODDED.					
9. HIGH DENSITY POLY ETHY	LENE (HDPE) SHALL CONFORM TO SECION 4	431–2.3 AND 431–3.3	3.			
7. CONCRETE CURB AND GUT	TER SHALL BE CONSTRUCTED IN CONFORMA	NCE TO SECTION 520	0.			
6. CONCRETE SIDEWALKS AN SECTION 522.	D DRIVEWAYS SHALL BE CONSTRUCTED IN A	ACCORDANCE WITH				
5. ALL CONCRETE USED FOR SPILLWAYS, CURBING, ETC	CONSTRUCTION OF DRAINAGE STRUCTURES, CONFORMING TO SECT	, SIDEWALKS, FION 347.				
STRUCTURAL COURSE CONFORM TO SECTION CONFORM TO SECTION	TYPE & THICKNESS PER DESIGN SECTION 334. ALL ASPHALTIC CONCRETE CONSTRU 330.	) AND SHALL JCTION SHALL				
MODIFIED PROCTOR M SECTION 300.	ETHOD (AASHTO T-180). THE PRIME COAT	SHALL CONFORM TO				
D. BASE COURSE: ALL M. PLACED ACCORDING T (PER DESIGN SECTION	ATERIAL SHALL BE LIMEROCK CONFORMING O SECTION 200 IN A SINGLE OR TWO EQUA I). BASE MATERIAL SHALL BE COMPACTED	TO SECTION 911 ANI AL COMPACTED LIFTS TO 98% DENSITY BY	ID			
+ SUBGRADE SHALL BE SECTION.	NG TO SECTION 160 IN ONE 12" MINIMUM STABILIZED TO A MINIMUM LBR VALUE OF	COMPACTED LIFT. 40 PER DESIGN				
LOCATIONS OF SUBSO OAK OR THE GEOTECH	IL EXCAVATION SHALL BE AS DIRECTED BY INICAL ENGINEER.	THE CITY OF LIVE	R	JOT UNSIDLE CHARGE OF PER	FURMING INE WORK IN TH	L FIELD.
95% DENSITY USING M B. SUBSOIL EXCAVATION: ACCORDANCE WITH TH	AODIFIED PROCTOR METHOD (AASHTO T-180 THE LIMITS OF SUBSOIL EXCAVATION SHA E RECOMMENDATIONS OF THE GEOTECHNICA	)). ALL BE IN AL REPORT. EXACT	IN A EI	CLUDE NAME OF COMPANY, DRESSES OF INDIVIDUALS F FORT AND THE CONTACT IN	ADDRESS, PHONE NUMBERS RESPONSIBLE FOR THE SUB FORMATION FOR THE INDIV	, AND EMAIL CONSULTANT WORK IDUALS IN F FIFLD
A. EARTHWORK: FILL MA	TERIALS SHALL CONFORM TO AASHTO SOIL HALL BE PLACED IN 6" – 12" LOOSE LIFTS	GROUPS A-1, A-2, S AND COMPACTED TC	A 5. Ti D B	E CONTRACTOR SHALL PROV PERFORMING ANY PHASES	/IDE A LIST OF SUB-CONT	RACTORS THAT WILL T. THE LIST SHALL
STABILIZATION OF THE R 4. ALL NEW ASPHALT PAVEM SPECIFICATIONS	DADWAY SUBGRADE.	IE FOLLOWING	W A P <sup>I</sup>	DRK COMPLETED PRIOR TO S RE NOT RESPONSIBLE FOR C JRCHASED, DELIVERED OR IN	SUBMITTAL APPROVAL. THE OSTS INCURRED IF ANY MA ISTALLED PRIOR TO SUBMI	L OWNER OR EOR ATERIALS ARE ITAL REVIEW AND
3. ALL PROPOSED UNDERGRO SUBGRADE, INCLUDING ST CONDUIT, AND SLEEVES F	UND UTILITY INSTALLATIONS WITHIN THE L ORMWATER, POTABLE WATER, WASTEWATER, OR FUTURE UTILITIES, SHALL BE INSTALLED	.IMITS OF ROADWAY GAS, ELECTRICAL ) PRIOR TO	3. M SI 4. CI	JERIALS SHALL NOT BE DEL JBMITTAL APPROVAL IS PRO ONTRACTOR IS RESPONSIBLE	VIDED BY THE EOR OR EOF FOR ANY COST INCURRED	CONTREMESENTATIVE.
SUITABLE. THE CONTRACT EXCESS OR UNSUITABLE REQUIREMENTS. TREES TO CONSTRUCTION AND SHAL	OR SHALL FURNISH ALL FILL REQUIRED AND MATERIAL OFFSITE IN ACCORDANCE WITH A REMAIN SHALL BE PROTECTED PRIOR TO S L BE PROTECTED THROUGHOUT SITE CONST	D DISPOSE OF ALL ALL REGULATORY STARTING RUCTION.	Di Ri Si	TAILS AND ANY UTHER INFO VIEW AND APPROVE AS TO PECIFICATIONS.	COMPLIANCE WITH THE DE	ITE LOK IO SIGN DRAWINGS AND
STANDARD PLANS. 2. ALL AREAS OF NEW CONS REMAINING ONSITE MAY E	TRUCTION SHALL BE CLEANED AND GRUBBE	D. TOP SOIL DSCAPED AREAS, IF	IN SI SI	FORMATION TO THE EOR FOR IALL INCLUDE SOURCE PROV IEETS, DIMENSIONED DRAWIN	R REVIEW AND APPROVAL. IDER, MANUFACTURER LITE IGS, FABRICATION DRAWING	THESE SUBMITTALS RATURE, CUT GS, SKETCHES, THE FOR TO
PAVING, GRADING AND D 1. ALL DRAINAGE CONSTRUC TECHNICAL STANDARDS,	RAINAGE SPECIFICATIONS TION, INCLUDING MATERIALS, CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE FDOT	N TECHNIQUES, AND FY 2020-21	1. PI	IHED SUBMITTALS RIOR TO CONSTRUCTION INS IE CONTRACTOR SHALL SUBM	TALLATION OF SPECIFIC WO MIT SHOP DRAWINGS AND S	DRK SCOPE ITEMS SUBMITTAL
			הבי			
NATIONAL ELECTRIC CODE	AND THE 2020 FLORIDA FURE PREVENTION	CODE, AS AMENDED.	2. TE LC	STING REPORTS WITH GEOTE CATIONS.	CHNICAL TESTING RESULTS	SHALL DENOTE THE TESTING
DISCREPANCIES PRIOR TO CONDITIONS RESULT UPON	CONSTRUCTION TO ASSURE PROPER GRADING CONSTRUCTION.	NG AND RUNOFF	SE IN EN TC	RVICES CONSTRUCTION AND SPECTION STANDARDS. TEST GINEER FOR REVIEW AND AF CONSTRUCTION OF THE SUF	INSPECTION STANDARDS A ING RESULTS SHALL BE PR PPROVAL OF CONFORMANCE BSEQUENT COURSE.	ND FDOT CONSTRUCTION AND OVIDED TO THE OWNER AND THE TO DESIGN SPECIFICATIONS PRIOF
SURVEY CONDUCTED BY JE EXISTING ELEVATION GRAD WILL AFFECT FINISH GRAD CONSTRUCTION. JBPRO W	JERU UN 12/08/2021. THE CONTRACTOR SH JES AND NOTIFY THE ENGINEER OF ANY DIS JE DESIGN FOR PROPER RUNOFF CONDITION JELL ASSIST THE CONTRACTOR IN REVISING	IALL VERIFY ALL SCREPANCIES THAT IS PRIOR TO ANY GRADING	1. TH SF RE	E CONTRACTOR SHALL BE RE ECIFIED DESIGN COURSE, SU QUIREMENTS SHALL BE PERF	ESPONSIBLE FOR PROVIDIN JBGRADE, BASE COURSE, AI FORMED IN ACCORDANCE W	G GEOTECHNICAL TESTING FOR EAC ND PAVEMENT. TESTING ITH THE CITY OF LIVE OAK PUBLIC
11. EXISTING TOPOGRAPHY IN	STRUCTION IN A FORM ACCEPTABLE TO THE	CITY OF LIVE OAK.	<u>GE</u> OTE	R AND PROPERTY OWNER.	NERAL NOTE	
3,000 PSI COMPRESSIVE S	STRENGTH. SEGMENTS NOT ADJACENT TO LOT DRIVEWA	YS WILL BE	62 SC DI PF	-621.300(6), F.A.C.) WITH F IL DISTURBING CONSTRUCTION STURBED OPEN AREAS HAVE RMANENT STABILIZATION IS	DEP. THE PERMIT COVERA ON ACTIVITIES ARE COMPL REACHED AT LEAST 70% \ IN PLACE. A COPY OF THI	GE IS DEEMED COMPLETE WHÉN AL ETE AND STABILIZATION OF ALL ÆGETATIVE COVER OR OTHER E NOT SHALL BE PROVIDED TO THE
SIDEWALK CROSSING SHAL SIDEWALK CROSSING. 9. CONCRETE SIDEWALK CROS	L MEET CURRENT ADA STANDARDS FOR THE	E LENGTH OF THE	AN 5. UF TH	U THE COP WHEN THE REPO ON SATISFACTORY COMPLET E CONTRACTOR SHALL FILE	ION OF THE PROJECT CONS THE NPDES STORMWATER N	TINUIDENTS OF NON-COMPLIANCE. STRUCTION AND SITE STABILIZATION NOTICE OF TERMINATION (NOT) (RUI
8. DRIVEWAYS SHALL BE DES AND CITY STANDARDS. DR INTERSECTIONS (50FT) AN	IGNED IN ACCORDANCE WITH THE MOST RE IVEWAYS WILL MEET SETBACK REQUIREMENT ID UTILITY INFRASTRUCTURE (5FT MIN.). DR	CENT FDOT INDEX TS FROM RIVEWAYS WITH	OC DC SH	CUR WITHIN SEVEN (7) CAL CUMENTED AND SIGNED BY ALL CONTAIN A CERTIFICAT	ENDAR DAYS OF THE INSPE A QUALIFIED INSPECTOR A ION THAT THE FACILITY IS	ECTION. THESE INSPECTIONS MUST S DEFINED BY THE CCP. THE REPO IN COMPLIANCE WITH THE SWPPP
7. DRIVEWAYS SHALL BE CON DRIVEWAY AND/OR DRIVEN	FIGURED TO ENSURE NO PORTION OF CURE WAY TAPER.	3 INLET FALLS WITHIN	CC N <sup>1/2</sup> NC CC	NSTRUCTION ENTRANCES/EX INCH OR GREATER STORM E N-COMPLIANCE SHOULD BE RRECTIVE ACTIONS AND MA	ITS AT LEAST ONCE EVERY VENT. MAJOR OBSERVATION RECORDED IN THE INSPEC INTENANCE. UNLESS ADVISI	SEVEN (7) DAYS AND AFTER EVER S AND INCIDENTS OF TION REPORT, AS WELL AS ED OTHERWISE, MAINTENANCF MUST
<ol> <li>ALL PAVEMENT CUT/REPAI AND REQUIRES APPROVAL LIVE OAK.</li> </ol>	IR WILL BE MADE THE FULL ROAD WIDTH AT FOR CUT AND INSPECTION OF THE REPAIR	T LEAST 6 FT WIDE BY THE CITY OF	T. IF CC PE PC	NTROL (ESC) INSPECTOR DU RMIT COVERAGE. AT A MINI INTS, DISTURBED AREAS, MA	RING CONSTRUCTION TO A MUM THE CERTIFIED INSPE ATERIAL STORAGE AREAS, S	BIDE BY THE TERMS OF THE NOI CTOR SHALL INSPECT ALL DISCHAR STRUCTURAL CONTROLS AND
5. THE CONTRACTOR SHALL F RESPONSIBLE FOR THE IN	URNISH/INSTALL ALL STREET SIGNS AND S STALLATION OF ALL REQUIRED STRIPING, S	SHALL BE TOP BARS, ETC.	NE AN 4. TH	CESSARY THROUGHOUT CONS D METHODS DEEMED APPROF E CONTRACTOR SHALL UTTUT	STRUCTION, AND MAY UTILI PRIATE TO CONTROL EROSI ZED A STATE OF FLORIDA	ZE MULTIPLE BMP'S OR OTHER MEA ON AND SEDIMENTATION DISCHARGE CERTIFIED EROSION & SEDIMENTAT
<ol> <li>CONTRACTOR IS RESPONSI OFFSITE. DISPOSAL METHO APPLICABLE LOCAL. STATE</li> </ol>	BLE FOR DISPOSING OF ALL FOREIGN DEBR DS AND LOCATION SHALL BE IN ACCORDAN , AND FEDERAL REGULATIONS AND REQUIRE	<pre>IS AND MATERIAL ICE WITH ALL EMENTS.</pre>	3. SE FL JU	DIMENTATION AND EROSION ORIDA EROSION AND SEDIME LY 2013, OR LATEST UPDATE	CONTROL BMP'S SHALL BE NTATION CONTROL DESIGN THE CONTRACTOR IS RES	IN ACCORDANCE WITH THE STATE ER AND REVIEWER MANUAL, DATED SPONSIBLE TO UTILIZE ALL BMP'S,
3. CONTRACTOR SHALL DISPO OFF SITE TO A PERMITTED LOCATION SITE	)SE OF SUBSOIL EXCAVATED MATERIALS (UN ) LOCATION OR PER AN APPROVED DISPOS/	NSUITABLE FOR FILL) AL PLAN AND	Z. IF PF AL AN	EVENTION PLAN (SWPPP) AN L APPROPRIATE BEST MANAG D SEDIMENTATION DISCHARG	ID CGP AT ALL TIMES THRU GEMENT PRACTICES (BMP'S GE OFFSITE DURING CONST	DUGHOUT CONSTRUCTION, AND UTIL ) AS REQUIRED TO PREVENT EROSI RUCTION.
2. WHERE SUBSOIL EXCAVATI ACCORDANCE WITH THE RI LOCATIONS OF SUBSOIL E OR THE GEOTECHNICAL FN	ON IS REQUIRED, THE LIMITS OF EXCAVATI ECOMMENDATIONS OF THE GEOTECHNICAL RE XCAVATION SHALL BE AS DIRECTED BY THE IGINEER.	ON SHALL BE IN EPORT. EXACT CITY OF LIVE OAK	SH A( 2 TI	ALL PROVIDE THE EOR AND KNOWLEDGEMENT LETTER VER	PROPERTY OWNER WITH A RIFYING COVERAGE.	COPY OF THE FDEP
PROTECTING ALL EXISTING UTILITY CONFLICT OCCURS PROVIDER TO COORDINATE	; UTILITIES DURING CONSTRUCTION. IN TH 3, CONTRACTOR SHALL NOTIFY THE ENGINEE 2 REMEDIAL ACTION.	E EVENT THAT A		OM LARGE AND SMALL CONS NSTRUCTION ACTIVITY SHAL TTER FROM FDEP VERIFYING NERIC PERMIT AND THE PO	TRUCTION ACTIVITIES (CGF L NOT COMMENCE PRIOR T THE NOI IS COMPLETE, TH OJECT IDENTIFICATION NO	P) (RULE 62-621.300(4), F.A.C.). O OBTAINING AN ACKNOWLEDGEMEN IE PROJECT IS COVERED BY THE IS ESTABLISHED CONTRACTOR
GENERAL CONSTRUCTION 1. CONTRACTOR SHALL BE RE WORK LIMITS OF ALL IMPR	<u>IN NOTES</u> ESPONSIBLE FOR LOCATING ALL EXISTING U ROVEMENTS. THE CONTRACTOR IS ALSO RE	TILITIES WITHIN THE	<u>NPDES E</u> 1. TH (F	TOSION AND SEDIMEN E CONTRACTOR IS REQUIRED DEP) NOTICE OF INTENT (N	TATION CONTROL PE TO FILE THE FLORIDA DE OI) TO USE GENERIC PERM	MILLING NOTES PT. OF ENVIRONMENTAL PROTECTIO IT FOR STORMWATER DISCHARGE
	NN NOTES					

## TROL PERMITTING NOTES

REMENTS OF THE STORMWATER POLLUTION TIMES THROUGHOUT CONSTRUCTION, AND UTILIZE CES (BMP'S) AS REQUIRED TO PREVENT EROSION ING CONSTRUCTION.

SHALL BE IN ACCORDANCE WITH THE STATE OF OL DESIGNER AND REVIEWER MANUAL, DATED TOR IS RESPONSIBLE TO UTILIZE ALL BMP'S, AS MAY UTILIZE MULTIPLE BMP'S OR OTHER MEANS ROL EROSION AND SEDIMENTATION DISCHARGE.

FLORIDA CERTIFIED EROSION & SEDIMENTATION TION TO ABIDE BY THE TERMS OF THE NOI TED INSPECTOR SHALL INSPECT ALL DISCHARGE AREAS, STRUCTURAL CONTROLS AND NCE EVERY SEVEN (7) DAYS AND AFTER EVERY SERVATIONS AND INCIDENTS OF

E INSPECTION REPORT, AS WELL AS ESS ADVISED OTHERWISE, MAINTENANCE MUST THE INSPECTION. THESE INSPECTIONS MUST BE SPECTOR AS DEFINED BY THE CGP. THE REPORT ACILITY IS IN COMPLIANCE WITH THE SWPPP

DJECT CONSTRUCTION AND SITE STABILIZATION, RMWATER NOTICE OF TERMINATION (NOT) (RULE IT COVERAGE IS DEEMED COMPLETE WHEN ALL ARE COMPLETE AND STABILIZATION OF ALL EAST 70% VEGETATIVE COVER OR OTHER COPY OF THE NOT SHALL BE PROVIDED TO THE

PROVIDING GEOTECHNICAL TESTING FOR EACH COURSE, AND PAVEMENT. TESTING ORDANCE WITH THE CITY OF LIVE OAK PUBLIC ANDARDS AND FDOT CONSTRUCTION AND ALL BE PROVIDED TO THE OWNER AND THE FORMANCE TO DESIGN SPECIFICATIONS PRIOR

WORK WITHIN THE CITY RIGHT-OF-WAY

1. THE METHOD AND MANNER OF PERFORMING THE WORK AND THE QUALITIES OF MATERIAL FOR CONSTRUCTION WITHIN THE RIGHT-OF-WAY SHALL CONFORM TO THE REQUIREMENTS SPECIFIED BY CITY OF LAKE CITY FOR WORK WITHIN THEIR RESPECTIVE RIGHT-OF-WAYS.

ACPW

ADJ

ALUM

ASPH

B&cJ

BC

BCCMP

BLDG

BM

BOP

BSL

C**&**G

C1

CET

CF

CI

CIP

CL

CLF

CMP

CMPA

CMU

CO

COLO

COMM

CONC

CPE

CY

DBI

DCBP

DEG

DHWL

DI

DIA

DIM

DIP

E/M

EL

ELEC

EOP

EΡ

ERCP

ΕX

FC

FDC

FDEP

FDOT

FF

FG

FL

FLL

FM

FO

FP

FΤ FUT

GALV

GRU GV

HC

HR

INV IRR

ΙF

L1

LAT

LBR LONG

LTG

HORIZ ΗP

CATV

APT

- 2. TRAFFIC CONTROL WITHIN EXISTING R/W SHALL BE IN ACCORDANCE WITH LATEST MUTCD OR FDOT STANDARDS FOR CONTROL OF TRAFFIC THROUGH WORK ZONES.
- 3. NO WORK SHALL BE DONE NOR MATERIALS USED IN THE RIGHT-OF-WAY WITHOUT INSPECTION BY THE CITY. THE CONTRACTOR SHALL FURNISH THE CITY WITH EVERY REASONABLE FACILITY FOR ASCERTAINING WHETHER THE WORK PERFORMED AND MATERIALS USED ARE IN ACCORDANCE WITH THE REQUIREMENTS AND INTENT OF THE PLANS AND SPECIFICATIONS.
- 4. CITY OF LAKE CITY RESERVES THE RIGHT TO MODIFY THE PROPOSED WORK WITHIN THEIR RIGHT-OF-WAY TO ENSURE COMPATIBILITY WITH EXISTING IMPROVEMENTS. SUCH MODIFICATION COSTS SHALL BE BORN BY THE CONTRACTOR.
- 5. ALL DISTURBED AREAS WITHIN THE RIGHT-OF-WAY SHALL BE SEEDED UNLESS SODDING IS OTHERWISE SPECIFIED. SODDING IS REQUIRED ON ALL SLOPES 4:1 OR STEEPER.
- 6. ALL WORK WITHIN OR ON CITY OF LAKE CITY OWNED AND MAINTAINED FACILITIES, ROW OR EASEMENTS WILL REQUIRE AS-BUILT PLANS. AS-BUILT PLANS SHOULD SHOW THE CONSTRUCTED CONDITIONS OF THE CITY OWNED OR MAINTAINED AREA AND SHALL BE PER FDOT STANDARDS AND PROCEDURES AND BE PERFORMED BY A FLORIDA LICENSED PROFESSIONAL SURVEYOR AND MAPPER.
- 7. INSTALLATION OR CONSTRUCTION OF MAILBOXES, PRIVATE IRRIGATION PIPING, OR PRIVATE UTILITY CHASES, ETC. IN PUBLIC RIGHT OF WAYS UNDER CITY OWNERSHIP SHALL REQUIRE A RIGHT OF WAY USE PERMIT PRIOR TO BEGINNING.

STANDARD ABBREVIATIONS ALACHUA COUNTY PUBLIC WORKS ADJACENT ALUMINUM APARTMENT ASPHALT BORE & JACK BACK OF CURB BITUNIMOUS COATED CORRUGATED METAL PIPE BUILDING BENCHMARK BEGINNING OF PROFILE BUILDING SETBACK LINE CURB & GUTTER CURVE ONE CABLE TELEVISION CURB END TAPER CUBIC FEET CAST IRON CAST-IN-PLACE CENTER LINE CHAIN LINK FENCE CORRUGATED METAL PIPE CORRUGATED METAL PIPE ARCHED CONCRETE MASONRY UNIT CLEAN OUT CITY OF LIVE OAK COMMUNICATIONS CONCRETE CORRUGATED POLYETHYLENE PIPE CUBIC YARD DITCH BOTTOM INLET DOUBLE CHECK BACKFLOW PREVENTER DEGREES DESIGN HIGH WATER LEVEL DUCTILE IRON DIAMETER DIMENSION DUCTILE IRON PIPE EAST EASEMENT ELEVATION ELECTRIC END OF PROFILE EDGE OF PAVEMENT ELLIPTICAL REINFORCED CONCRETE PIPE EXISTING FACE OF CURB FIRE DEPARTMENT CONNECTION FLORIDA DEPARTMENT OF ENVIRONMENTAL PROTECTION FLORIDA DEPARTMENT OF TRANSPORTATION FINISH FLOOR FINISH GRADE FIRE HYDRAN FIRE LINE FLOW LINE FORCE MAIN FIBER OPTIC FLOOD PLAIN FEET FUTURE GALVANIZED GAINESVILLE REGIONAL UTILITIES GATE VALVE HANDICAP HORIZONTAL HIGH POINT HOUR INVERT IRRIGATION LINEAR FEET LINE ONE LATITUDE LIMEROCK BEARING RATIO LONGITUDE



SHEET TITLE:

CLIENT:

LIGHT



**CIVIL ENGINEERING | LAND PLANNING** SURVEYING | CONSTRUCTION SERVICES

Gainesville: (352) 375-8999 | St. Augustine: (904) 789-8999 Toll Free: (844) Go-JBPro | E-mail: contact@jbpro.com

HIGH COTTON EQUITIES, LLC

					LEGEND		
	LP LT MAINT MAX MES MH MIN MO N/A NE NG NJC NO NTS NW OC PCPE POB POE PDE POB POE PL PRVT PRVT PRVT PRVT PSI PT PUD PVC PVMT P&P R/W R1' REF REP REF REF REF SG SJ SSL ST SSL ST STL STL SV TBC TEL TFMR TV TW	LOW POIN LEFT MAINTENA MAXIMUM MITERED E MANHOLE MINIMUM MONTH NORTH NOT APPL NORTH EA NOT TO SU NORTH EA NOT TO SU NORTH WE ON CENTEI PERFORAT POLYETHYI POINT OF POLYETHYI POINT OF PROPERTY PRIVATE PRIMARY POUNDS P PRESSURE PLANNED POINT OF POLYVINYI PAVEMENT PLAN & P RIGHT OF ONE FOOT REACTION REINFORCE REDUCED REQUIRED REGUICED REQUIRED RIGHT RESILIENT SOUTH SIDEWALK SOUTH EA SUTANDARD STATION STREET STATION STREET STATION STREET STATION STEEL SOUTH WE SQUARE Y TRAFFIC E TEMPORAR	r NCE ND SECTION CABLE ST GRADE DNTRACT CALE ST R ED CORRUGATED POLYETHYLENE F ENE PIPE BEGINNING ENDING LINE ER SQUARE INCH TREATED (NON ARSENIC) JRBAN DEVELOPMENT VERTICAL INTERSECTION CHLORIDE ROFILE WAY RADIUS BLOCK ED CONCRETE PIPE PRESSURE BACKFLOW PREVENTER WEDGE ST Y EET CAR NLET RIVER WATER MANAGEMENT DIS PENETRATION TEST SEWER STEEL	TRICT	EST EWW EGAS ECATV EUE EWM 	EXISTING STO EXISTING GAS EXISTING CAT EXISTING CAT EXISTING CAT EXISTING CAT EXISTING CAT EXISTING CAT EXISTING FEN NATURAL GR EXISTING TRE PROPOSED A PROPOSED A PROPOSED A PROPOSED FI PROPOSED FI PROPOSED FI PROPOSED TI PROPOSED S PROPOSED S PROPOSED S SERVICE CON PROPOSED A PROPOSED G PROPOSED G PROPOSED G PROPOSED G PROPOSED G PROPOSED C PROPOSED C	ARM SEWER LINE NITARY SEWER LINE S SERVICE LINE TV & TELEPHONE LINE DERGROUND ELECTRIC LINE TER MAIN NITOUR LINE ICE OUND SPOT ELEVATION TE TO REMAIN TE TO BE REMOVED SPHALT PAVEMENT ONC PAVEMENT/SIDEWALK INISH ELEVATION CONTOUR INISH ELEVATION CONTOUR INISH SPOT ELEVATION IRECTIONAL FLOW ARROW REA DIVIDE ILT FENCE REE BARRICADE FENCE ENTERLINE TORM SEWER LINE ATER MAIN RIMARY ELECTRIC DUIT ECONDARY ELECTRIC DUIT AS MAIN ATV & TELEPHONE SERVICE ATV & TELEPHONE SERVICE ATV & TELEPHONE SERVICE ANITARY SEWER FORCE MAIN UE RAINAGE SWALE AY VC SANITARY SEWER ER BAGS
Ν	RT RW S S/W SE SEC SF SG SI SPA SRWMD SPT SS SSL ST SSL ST SSL ST STA STD STL STL SW SY TBC TBM	RIGHT RESILIENT SOUTH SIDEWALK SOUTH EA SECONDAR SQUARE F SWITCH GE SURFACE SUWANNEE STANDARD STANDARD STAINLESS STORM STREET STATION STANDARD STEEL SOUTH WE SQUARE Y TRAFFIC E TEMPORAR	WEDGE ST Y EET AR INLET RIVER WATER MANAGEMENT DIS PENETRATION TEST SEWER STEEL STEEL	TRICT	GAS TEL CATV SSFM SSFM	PROPOSED G PROPOSED C PROPOSED W PROPOSED W PROPOSED P PROPOSED P PROPOSED D RIGHT-OF-W PROPOSED P SERVICE LATI GRAVEL FILT	AS MAIN ATV & TELEPHONE SERVICE ATV & TELEPHONE SERVICE ASTEWATER ANITARY SEWER FORCE MAIN UE RAINAGE SWALE AY VC SANITARY SEWER ERAL WITH CLEANOUT ER BAGS
	TEL TFMR TV TW TYP UD UTIL VCP VEH W W/ W/ W/ WM WS WM YR	TELEPHONI TRANSFOR TELEVISIO TOP OF W TYPICAL UNDER DR UTILITIES VITRIFIED VEHICLE WEST WITH WATER MA WATER MA WATER SEI WASTEWAT YEAR	E MER N ALL AIN CLAY PIPE STEWATER IN RVICE ER				
D	ABB	REV	TATIONS AN	ID	NOTES		DATE: <b>JUNE 2022</b> PROJECT NO: <b>366-22-02</b> SHEET NO:
'IE	ES, LLC		SONIC LAKE	≺E; CIT\	<b>5 I AUKAN</b> ′, FLORIDA		C0.1

# JUNIC RESTAURANT LAKE CITY, FLORIDA

104



	PLAN $PLAN$
	20 10 0 20 40 60 $300 5 CALE 1" = 20' DATE: JUNE 2022$
	$(\overline{N})$
	$(\parallel)$
$\times$	EXISTING TREE TO BE REMOVED
××××× ×××××× -	EXISTING HARDSCAPE TO BE REMOVED
<u>LEGEND</u>	
<ul><li>7. EXISTING S BE REINSTA</li></ul>	TON ENTRANCE AND STOCKPILE AREA. SIGNS SHALL BE REMOVED AND STORED DURING DEMOLITION SO THAT THEY CAN ALLED DURING CONSTRUCTION.
SUITABLE F PH RANGE 5. ALL OFF SI	FOR ROOT GROWTH WITH APPROPRIATE AMOUNTS OF ORGANIC MATTER, AND OF OF 5.5 – 6.5.
4. REMOVAL O CONCRETE MATERIALS WORK CONT	TE, AND FEDERAL REQUIREMENTS. IF ALL CONSTRUCTION DEBRIS, LIMEROCK, EXCESS OF BUILDERS SAND, AND MORTAR DEBRIS, EXISTING WEEDS AND GRASSES, AND ALL FOREIGN IN THE PLANTING BED AND SOD AREAS IS THE RESPONSIBILITY OF THE SITE TRACTOR. SOIL IN AREAS TO BE LANDSCAPED SHALL BE UNCOMPACTED,
<ul> <li>ADDITION I MISCELLANE CONSTRUCT</li> <li>CONTRACTO DISPOSAL M</li> </ul>	EOUS TRASH, DEBRIS, FENCING ETC., IN ORDER TO PROVIDE A CLEAN TON SITE. IN IS RESPONSIBLE FOR DISPOSING OF ALL DEMOLITION MATERIAL OFF SITE. METHODS AND LOCATIONS SHALL BE IN ACCORDANCE WITH ALL APPLICABLE
<ol> <li>PRIOR TO I APPROPRIA UNDERGROU</li> <li>CONTRACTO ADDITION J</li> </ol>	DEMOLITION AND CONSTRUCTION, THE CONTRACTOR SHALL CONTACT THE TE UTILITY COMPANIES TO VERIFY THE LOCATION OF ALL ABOVE GROUND AND JND UTILITIES TO BE REMOVED OR RELOCATED. OR IS RESPONSIBLE FOR ALL SITE DEMOLITION AND MATERIAL REMOVAL. IN
<u>GENERAL I</u>	DEMOLITION NOTES
	GENERAL 1. PRIOR TO A APPROPRIA UNDERGROU 2. CONTRACTO ADDITION MISCELLANI CONSTRUCT 3. CONTRACTO DISPOSAL I LOCAL, STA 4. REMOVAL O CONCRETE MATERIALS WORK CONT SUITABLE F PH RANGE 5. ALL OFF SI 6. PRIOR TO A CONSTRUCT 7. EXISTING S BE REINSTA LEGEND X X X X X X X X X X X X X

![](_page_105_Figure_0.jpeg)

	E	ROSION	AND SEDIMENTATION CONTROL NOTES				
	1.	THE CON PLAN AS CONTRAC (BMP'S) ONSITE PROPERT	TRACTOR SHALL UTILIZE THE EROSION AND SEDIMENTATION CONTROL A GENERAL GUIDE AND DIRECTION FOR MINIMUM CONTROL MEASURES. CTOR SHALL UTILIZE EROSION CONTROL BEST MANAGEMENT PRACTICES AS REQUIRED TO MINIMIZE EROSION AND SEDIMENTATION BUILDUP AND TO PREVENT EROSION AND SEDIMENTATION DISCHARGE OFF TY.				
	2.	CONTRAC CONTROL SEDIMEN	TOR SHALL EMPLOY AN FDEP CERTIFIED EROSION AND SEDIMENTATION INSPECTOR TO MONITOR THE CONTRACTOR'S EROSION AND TATION CONTROL WORK EFFORT THROUGHOUT CONSTRUCTION.				
	3. DURING CONSTRUCTION, NO DIRECT DISCHARGE OF WATER TO DOWNSTREAM RECEIVING WATERS WILL BE ALLOWED. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING WATER QUALITY, AND SHALL ROUTE DISCHARGE WATER IN SUCH A MANNER TO ADEQUATELY REMOVE SILT PRIOR TO RUNOFE FROM THE SITE						
	4.	SILT FEN CONSTRU CONSTRU CONSTRU CONTROL SEDIMEN	ICING AND OTHER SEDIMENT CONTROL MEASURES SHALL BE ICTED WHERE SHOWN ON THE DRAWINGS PRIOR TO STARTING ICTION. ALL STORM DRAIN INLETS SHALL BE PROTECTED DURING ICTION IN ACCORDANCE WITH FDOT EROSION AND SEDIMENTATION . DESIGNER AND REVIEWER MANUAL (JUNE 2007) TO PREVENT T DISCHARGE TO THE EXISTING STORMWATER MANAGEMENT SYSTEM.				
	5.	THE RET SILT FEN SHALL B BEYOND BUILDUP CONSTRU	ENTION BASINS SHALL BE PROTECTED FROM SEDIMENT DISCHARGE BY ICE AT THE DISCHARGE STORM STRUCTURE. CAREFUL ATTENTION E PAID TO PREVENT EROSION FROM ENTERING THE BASIN BOTTOM THE IMMEDIATE EXIT OF THE STRUCTURE. REMOVE ALL SEDIMENT AT THE DISCHARGE STRUCTURE REGULARLY THROUGHOUT JCTION.				
	6.	PROTECT OTHER E STORM S	EXISTING STORMWATER INLET STRUCTURES WITH FILTER FABRIC OR ROSION CONTROL DEVICE TO PREVENT SEDIMENTS FROM ENTERING WER SYSTEM.				
	7.	ALL ERO AT LEAS DEFICIEN	SION AND SEDIMENTATION CONTROL STRUCTURES SHALL BE INSPECTED T ONCE PER WEEK AND AFTER 1/2" OR GREATER RAINFALL. ANY ICIES DISCOVERED SHALL BE REPAIRED, ADJUSTED, OR IMPROVED AS				
	8.	ALL ERO MAINTAI VEGETAT	SION AND SEDIMENTATION CONTROL STRUCTURES SHALL BE NED UNTIL CONSTRUCTION IS COMPLETE AND APPROVED, AND ION IS FULLY ESTABLISHED.				
	9.	CONTRAC WATER S USE OTH	CTOR SHALL MINIMIZE AIR POLLUTION FROM PARTICULATES AND DUST. CHALL BE APPLIED AS NEEDED TO REDUCE DUST DURING CONSTRUCTION. ER STABILIZATION METHODS SUCH AS HYDROMULCHING AS NEEDED.				
	10.	CONTRAC FLOW IS NOT OCC	TOR SHALL STOCKPILE AND STORE MATERIALS SUCH THAT STORMWATER NOT IMPEDED DURING CONSTRUCTION AND MATERIAL EROSION DOES CUR.				
	11.	ALL DIS SHALL B SEEDING SEDIMEN EVIDENC	TURBED OPEN AREAS WITHIN THE PROJECT CONSTRUCTION LIMITS E COMPLETELY GRASSED BY COMPLETION OF CONSTRUCTION. GRASS RATES AND MIXTURES SHALL BE PER FDOT EROSION AND TATION CONTROL DESIGNER AND REVIEWER MANUAL (JULY 2013). E OF GROWTH MUST BE PRESENT PRIOR TO FINAL RELEASE.				
	12.	THE STO ACCUMU	RM SEWER SYSTEM SHALL BE FLUSHED OUT TO REMOVE ALL ATED DEBRIS AND SEDIMENT UPON COMPLETION OF CONSTRUCTION.				
			LEGEND				
		—//	SILT-FENCE BARRIERS — PER FDOT EROSION AND SEDIMENT CONTROL DESIGNER AND REVIEWER MANUAL (JUNE 2007)				
		<b>(</b> -)	FILTER CLOTH INLET PROTECTION				
		• • •					
				$\overline{\mathbf{N}}$			
			20 10 0 20	40 60			
			SCALE $1^{"} = 20$	D,			
ND	) SED	DIME	NTATION CONTROL PLAN	DATE: <b>JUNE 2022</b> PROJECT NO: <b>366-22-02</b>			
TIE	S, LLC		SONIC RESTAURANT LAKE CITY, FLORIDA	SHEET NO:			

![](_page_106_Figure_0.jpeg)

DIMENSION NOTE	ES
----------------	----

LEGEND

![](_page_106_Picture_8.jpeg)

![](_page_107_Figure_0.jpeg)






# **LOCATION MAP**



### **PROPERTY DESCRIPTION**

LOT 3 OF A REPLAT OF LOTS 2, 3, & 11 OF GATEWAY CROSSING A REPLAT OF LOTS 2 & 3, ACCORDING TO THE PLAT THEREOF, RECORDED IN PLAT BOOK 9, PAGES 176-177, PUBLIC RECORDS OF COLUMBIA COUNTY, FLORIDA.

SYMBOL LEGEND

----- EST------ STORM SEWER LINE

------ EWW ------ WASTEWATER LINE

**BOUNDARY LINE** 

TAX PARCEL LINE

BENCHMARK

NAIL AND DISK

CLEANOUT

WATER METER

FIRE HYDRANT

PUMP

ELECTRIC METER

WATEF

WATER

ELEC

 $\langle \rangle$ 

\_\_\_\_

× 132.2

× 132.21

**RIGHT-OF-WAY LINE** 

CONCRETE MONUMENT

STORM SEWER MANHOLE

WASTEWATER MANHOLE

MARKER FOR UNDERGROUND UTILITY

SPOT ELEVATION - SOFT SURFACE

SPOT ELEVATION - HARD SURFACE

WATER VALVE COVER

• 9 SYC TREE-SIZE(INCHES) AND SPECIES

CONTOUR LINES

ASPHALT SURFACE

CONCRETE SURFACE

IRON ROD - CAPPED

### ABBREVIATIONS

BC = BACK OF CURB EP = EDGE OF PAVEMENT LB = LICENSED BUSINESS LS = LICENSED SURVEYOR (M) = MEASURED NAD83(2011) = NORTH AMERICAN DATUM OF 1983 2011 ADJUSTMENT NAVD88 = NORTH AMERICAN VERTICAL DATUM OF 1988 (P) = PLATTED P.B. = PLAT BOOK O.R.B. = OFFICIAL RECORDS BOOK

PG. = PAGE PLS = PROFESSIONAL LAND SURVEYOR R/W = RIGHT-OF-WAY

TREES:

CE = CEDAR PI = PINE

### SCHEDULE B-II ITEMS

- 1. NOT A SURVEY MATTER.
- 2. SEE SURVEY SHOWN HEREON.
- 3. NOT A SURVEY MATTER.
- 4. NOT A SURVEY MATTER.
- 5. EASEMENTS DEPICTED ON THIS SURVEY WERE PROVIDED BY THE CLIENT OR ARE RECORDED IN AN ASSOCIATED PLAT. NO EASEMENT RESEARCH WAS CONDUCTED DURING THE COURSE OF THIS SURVEY.
- 6. NOT A SURVEY MATTER.
- 7. SEE SURVEY SHOWN HEREON.
- 8. THIS ITEM REFERENCES AN OUT OF DATE PLAT, AND THEREFORE IS SUPERSEDED BY B-II ITEM 7.
- 9. SEE SURVEY SHOWN HEREON.
- 10. THE MASTER DECLARATION OF EASEMENTS, COVENANTS, CONDITIONS AND RESTRICTIONS FOR GATEWAY CROSSING, RECORDED IN OFFICIAL RECORDS BOOK 1317, PAGE 1034; AS AFFECTED BY AMENDMENT TO DECLARATION OF COVENANTS, CONDITIONS AND RESTRICTIONS GATEWAY CROSSING, RECORDED IN OFFICIAL RECORDS BOOK 1334, PAGE 275, OF THE PUBLIC RECORDS OF COLUMBIA COUNTY, FLORIDA, CONTAINS PROVISIONS FOR THE CREATION OF SEVERAL BLANKET EASEMENTS THAT ARE UNABLE TO BE DRAWN, AND AS SUCH, ARE NOT DEPICTED ON THIS SURVEY MAP. THE EASEMENT OF ENCROACHMENT RESERVES A MAXIMUM OF 3 FEET ON EITHER SIDE OF THE BOUNDARY BETWEEN A PARCEL AND ANY ADJACENT PARCEL OR COMMON AREA FOR MAINTENANCE USE AND ANY PERMITTED ENCROACHMENT. THE PORTION OF THIS EASEMENT AFFECTING THE SUBJECT PARCEL IS SHOWN ON THIS SURVEY MAP.
- 11. EASEMENT CONTAINED IN INSTRUMENT RECORDED AT OFFICIAL RECORDS BOOK 104, PAGE 118, OF THE PUBLIC RECORDS OF COLUMBIA COUNTY, FLORIDA, ESTABLISHED AN EASEMENT ENCOMPASSING THE ENTIRETY OF THE SUBJECT PARCEL IN FAVOR OF FLORIDA POWER AND LIGHT COMPANY FOR THE CONSTRUCTION OF A UTILITY LINE. THIS DOCUMENT STATES THAT THIS EASEMENT IS TO CONFORM TO FUTURE DEVELOPMENT BY THE OWNERS.
- 12. EASEMENT CONTAINED IN INSTRUMENT RECORDED AT OFFICIAL RECORDS BOOK 1328, PAGE 2266, OF THE PUBLIC RECORDS OF COLUMBIA COUNTY, IS DEPICTED ON THIS SURVEY MAP.
- 13. NOT A SURVEY MATTER.
- 14. NOT A SURVEY MATTER.

THE MAP OF THE PROPERTY DESCRIBED HEREON WAS MADE UNDER MY SUPERVISION AND THIS MAP OF SURVEY FURTHER MEETS THE STANDARDS OF PRACTICE SET FORTH BY THE STATE OF FLORIDA BOARD OF PROFESSIONAL SURVEYORS & MAPPERS IN CHAPTER 5J-17.05, FLORIDA ADMINISTRATIVE CODE, PURSUANT TO SECTION 472,027, FLORIDA STATUTES, AND THE MAP OF SURVEY SHOWN HEREON IS A TRUE AND ACCURATE REPRESENTATION THEREOF TO THE BEST OF MY KNOWLEDGE, BEING SUBJECT TO

> Richard L Digitally signed by Richard L White DN: cn=Richard L White, c=US, o=Florida, email=richard.white@jbpro.com Date: 2022.05.09 17:18:27 -04'00' White RICHARD L. WHITE, PLS Professional Land Surveyor



### **ALTA/NSPS LAND T**

SEE ALTA CERTIFICATION AT LEFT

CERTIFIED TO:

ITLE SU	RVEY
Scale:	1"=20'
Proj. No.	366-22-02
Drawn:	J. Trahan
Checked:	R. White
Dwg. Name:	366-22-02-BT
Dwg. Date:	05-06-2022
Field Book:	N/A
Pages:	N/A
Sheet:	1 of 1



Gainesville | St. Augustine 3530 NW 43rd Street Gainesville, FL 32606

Suwannee River Water Management District 9225 CR 45 Live Oak, FL 32060

June 6, 2022

Subject: Sonic Drive-In restaurant

The Sonic Drive-In restaurant project is located on parcel number 35-3S-16-02524-103 located on NW Centurion Ct, approximately 600 feet north of the intersection of NW Centurion Ct and US HWY 90. The project proposes a new 1,227 SF restaurant building with associated parking and utilities.

The project site is part of a larger development which was previously permitted with Suwannee River Water Management District (SRWMD) under the project name Gateway Crossing and permit number 226410-1.

The proposed site will direct water to inlet structures in the parking lot, which will direct runoff to the master stormwater facility. The permit allows for 75% impervious on lots 1-4 and 7-10. Please see the attached post development exhibit of the current master development.

The Sonic Restaurant project proposes 31,216 SF or 59.22% of impervious area on the 1.21+/-Ac. Site located on Lot 3. We are requesting a letter of conformance with permit number 226410-1.

Sincerely,

Christepher Potts

Christopher A. Potts, P.E. Director of Civil Engineering, JBPro





### LEGAL DESCRIPTION (35-3S-16-02524-103)

LOT 3 GATEWAY CROSSING S/D A REPLAT OF LOTS 2 & 3. NKA LOT 3 A REPLAT OF LOTS 2,3 & 11 OF GATEWAY CROSSING IN PLAT BK 9 PGS 176 & 177.

Inst. Number: 201612000647 Book: 1307 Page: 1888 Date: 1/14/2016 Time: 10:37:15 AM Page 1 of 4 Doc Deed: 19775.00 P.DeWitt Cason Clerk of Courts, Columbia County, Florida

This Instrument Was Prepared By, Record and Return to:

John Hotte, Esquire Krinzman, Huss & Lubetsky 110 SE 6<sup>th</sup> Street, 20<sup>th</sup> Floor Fort Lauderdale, FL 33301 Telephone: (954) 761-3454

Property Appraiser Identification No.: Consideration:\$ A Jast:201612000647 Date:1/14/2016 Time:10:37 AM Do: Stamp-Deed:19775.00 \_\_\_\_\_DC,P.DeWitt Cason,Columbia County Page 1 of 4 B:1307 P:1888

#### SPECIAL WARRANTY DEED

THIS SPECIAL WARRANTY DEED made this 12th day of January, 2016, by Inn of Lake City, Inc., a Florida corporation ("Grantor"), whose mailing address is 1000 Red Fern Place, Flowood, MS 39232 in favor of GWC Development Partners, LLC, a Florida limited liability company ("Grantee"), whose mailing address is 2682 West Noegel Road, Lake City, FL 32055.

#### $\underline{WITNESSETH}$ :

That Grantor, for and in consideration of the sum of TEN DOLLARS (\$10.00) and other good and valuable consideration paid by Grantee, the receipt and sufficiency whereof are hereby acknowledged, does hereby grant, bargain, sell, alien, remise, release, convey and confirm unto Grantee the real property (the "Property") located in Columbia County, Florida, and more particularly described in Exhibit "A" attached hereto and made a part hereof.

SUBJECT ONLY TO the matters set forth in Exhibit "B" attached hereto and made a part hereof.

TOGETHER with all the tenements, hereditaments and appurtenances belonging or in any way appertaining to the Property, including, without limitation, all of Grantor's right, title and interest, if any, in and to all of the easements, rights, and privileges belonging or in any way appertaining to the Property and/or improvements located thereon.

TO HAVE AND TO HOLD the same in fee simple forever.

AND GRANTOR hereby covenants with Grantee that Grantor is lawfully seized of the Property in fee simple; that Grantor has good right and lawful authority to sell and convey the Property; that, subject to the matters described on <u>Exhibit "B"</u> attached hereto, Grantor does hereby warrant specially the title to the Property; and that Grantor and its successors and assigns will forever warrant and defend the same against the lawful claims of all persons claiming by, through or under Grantor, but against none other.

PD.18666487.2

Inst. Number: 201612000647 Book: 1307 Page: 1889 Date: 1/14/2016 Time: 10:37:15 AM Page 2 of 4 Doc Deed: 19775.00 P.DeWitt Cason Clerk of Courts, Columbia County, Florida

IN WITNESS WHEREOF, Grantor has caused this Special Warranty Deed to be executed by its duly authorized representative on the day and year first above written.

Two Witnesses:

Printed Name:

<u>1</u> 12 12 12 Name: inted

Inn of Lake City, Inc., a Florida corporation

By:

Michael J. Hart, Nice President, Treasurer and Assistant Secretary

STATE OF Mississippi COUNTY OF Hinds

The foregoing instrument was acknowledged before me this  $\frac{1}{2}$  day of  $\frac{1}{2}$  day of

Notary Public, State of Mississippi

Print Name: <u>Suzanna Baker</u> Commission No.: <u>83871</u> My Commission Expires: <u>Januar</u> 21, 2019 [Affix Notary Seal]



Signature Page of Special Warranty Deed

Inst. Number: 201612000647 Book: 1307 Page: 1890 Date: 1/14/2016 Time: 10:37:15 AM Page 3 of 4 Doc Deed: 19775.00 P.DeWitt Cason Clerk of Courts, Columbia County, Florida

#### **EXHIBIT "A"**

#### **Real Property Description**

COMMENCE at the Northwest corner of Section 35, Township 3 South, Range 16 East, Columbia County, Florida as established by B.G. Moore, PLS No. 439 and run thence S 06°22'00" W. along the West line of said Section 35, 1894.50 feet to the West Limited Access Right of Way of Interstate No. 75, thence run Southerly and Westerly along said West Limited Access Right of Way the following courses. S 24°54'32" E, 472.32 feet to the POINT OF BEGINNING, S 24°54'32" E, 940.25 feet; S 15°12'50" E, 512.06 feet; S 06°01'43" E, 335.81 feet; S 36°55'36" W, 54.60 feet to the Northerly Right of Way of West U.S. Highway 90 and the end of said courses; thence S 80°47'35" W, along said Northerly Right of Way, 371.77 feet; thence S 08°51'10" E, along said Northerly Right of Way, 22.18 feet; thence S 80°47'36" W, along said Northerly Right of Way, 73.15 feet; thence N 08°55'17" W, 150.09 feet; thence S 80°42'55" W, 150.25 feet; thence N 08°52'22" W, 60.12 feet; thence S 80°53'59" W. 79.99 feet; thence S 08°59'18" E, 210.15 feet to the aforesaid Northerly Right of Way; thence S 80°47'36" W, along said Northerly Right of Way, 26.39 feet to a point of a curve; thence run Westerly along the arc of said curve concave to the North having a radius of 3224.04 feet, a central angle of 05°24'20", a chord bearing and distance of S 83°26'26" W 304.06 feet, an arc distance of 304.18 feet to the aforesaid West line of Section 35; thence N 06°22'00" E, along said West line, 1784.01 feet; thence N 65°09'42" E. 286.69 feet to the POINT OF BEGINNING.

LESS AND EXCEPT the parcel described in O.R. Book 1284, Page 229, of the Official Records of Columbia County, Florida

Inst. Number: 201612000647 Book: 1307 Page: 1891 Date: 1/14/2016 Time: 10:37:15 AM Page 4 of 4 Doc Deed: 19775.00 P.DeWitt Cason Clerk of Courts, Columbia County, Florida

#### EXHIBIT "B"

#### Exceptions

- 1. Taxes and assessments for the year 2016 and subsequent years, which are not yet due and payable.
- 2. Any land use, zoning and building laws and ordinances.
- 3. Any declaration of covenants, conditions and restrictions, or other recorded restrictions.
- 4. Any right, title, interest, claim, violation, variation, encumbrance, encroachment, fact, matters or other adverse circumstance affecting title revealed, or that should have been revealed, by that certain ALTA/ACSM survey of the Property by JBrown Professional Group Inc. dated October 16, 2015, as revised (Proj. No. 366-15-01).
- 5. Any obligations, rights and other matters related to, and any agreements with and requirements of the State of Florida or other governmental agency regarding, the remediation of certain environmental issues on the Property by or on behalf of the State of Florida or a political subdivision thereof under a state-funded cleanup program(s).
- 6. Rights-of-way, utility easements, other easements, restrictions and other restrictive and/or use covenants filed of record and other matters which are revealed by a title search or title commitments, including the following:
  - a. Easement(s) in favor of Mississippi Management, Inc. set forth in instrument(s) recorded in Official Records Book 634, Page 338.
  - b. Easement(s) in favor of Shell Oil Company set forth in instrument(s) recorded in Official Records Book 674, Page 104.
  - c. Easement(s) in favor of American Telephone and Telegraph Company set forth in instrument(s) recorded in Official Records Book 723, Page 162.
  - d. Easement(s) in favor of The City of Lake City, Florida set forth in instrument(s) recorded in Official Records Book 776, Page 1724.
  - e. Easement contained in Deed recorded in Official Records Book 685, Page 38.
  - f. Easement recorded in Official Records Book 960, Page 1492.
  - g. Easement recorded in Official Records 104, Page 118, and in Official Records Book 361, Page 499.
  - h. Easement for ingress and egress recorded in Official Records Book 370, Page 337.
  - i. Easement(s) in favor of Florida Power and Light Company set forth in instrument(s) recorded in Official Records Book 361, Page 499.
  - j. Easement(s) in favor of The City of Lake City, Florida set forth in instrument(s) recorded in Official Records Book 559, Page 229.

### Columbia County Tax Collector

119

**Tax Record** 

Last Update: 5/31/2022 3:58:20 PM EDT

Register for eBill

#### Ad Valorem Taxes and Non-Ad Valorem Assessments

The information contained herein does not constitute a title search and should not be relied on as such.

Account Number		Тах Туре Тах Ү				
R02524-103		REAL ES	2021			
<b>Mailing Address</b> GWC DEVELOPMENT PARTNEF 2682 NW NOEGEL RD	RS LLC	Property	y Address			
LAKE CITY FL 32055		GEO Numl	ber			
		353816-0	02524-103			
Exempt Amount		Taxable	Value			
See Below		See Be	elow			
Exemption Detail NO EXEMPTIONS	Milla 001	illage Code Escrow Code				
35-3S-16 1000/1000.82 Z	Acres LOT 3	GATEWAY CR	<u>n)</u> OSSING S/D	A REPL	AT OF LOT	'S
2 & 3.		rom Taxos				
	Au vait		Exemption	Таха	hle T	aves
Taxing Authority	Rate	Value	Amount	Val	ue Lev	ried
BOARD OF COUNTY COMMISSIONERS	7.8150	412,274 0		\$412,	274 \$3,22	21.92
CITY OF LAKE CITY	4.9000	412,274 0 \$		\$412,	\$2,02	20.14
COLUMBIA COUNTY SCHOOL BOARD	0 7490	410 074	0	6410	274 620	0 20
JISCREIIONARI	3 6430	412,274	0	\$412 <b>,</b>	274	10.30
CAPITAL OUTLAY	1.5000	412,274	0	\$412,	274 \$1,50 274 \$61	8.41
SUWANNEE RIVER WATER MGT DIST	0.3615	412,274	0	\$412.	274 \$14	19.04
LAKE SHORE HOSPITAL AUTHORITY	0.0000	412,274	0	\$412,	274	50.00
Total Millage	18.967	5 <b>T</b>	otal Taxes		\$7,819.80	
	Ion-Ad Valor	rem Assess	ments			
Code Levying Auth	ority				Amo	unt
XLCF CITY FIRE AS	SESSMENT				\$50	.40
Code Levying Auth XLCF CITY FIRE AS	ority SESSMENT				<b>Amc</b> \$50	<b>) .</b> 4
		Tota	l Assessme	nts	\$50	.40
		Taxes	& Assessme	nts	\$7 <b>,</b> 870	.20
		If Paid By Amount Due				
					\$0	.00

Date Paid	Transaction	Receipt	Item	Amount Paid
12/29/2021	PAYMENT	1200971.0005	2021	\$7,634.09

Prior Years Payment History

#### **Prior Year Taxes Due**

NO DELINQUENT TAXES

120

# Gateway Crossing – Lot 3 Sonic Drive-In Lake City, FL



June 2022

**Fire Flow Calculations** 

Christopher A. Potts, P.E. FL Registration No. 73842

3530 NW 43<sup>rd</sup> Street Gainesville, FL 32606 (352) 375-8999 www.jbpro.com





### Attachments

Attachment A:	NFPA Fire Flow Calculations
Attachment B:	ISO Fire Flow Calculations
Attachments C:	City of Lake City Fire Flow



. .

### Attachment A NFPA Fire Flow Calculations

(Exhibit on Next Page)



a.

### FIRE FLOW CALCULATIONS PER NFPA 2009

BUILDING CONSTRUCTION:	Type V (000)
FIRE FLOW AREA (Total Floor Area):	1,227 SF (1 Floor)
FIRE FLOW REQUIRED:	1,500 GPM
BUILDING FULLY SPRINKLED?	No
FLOW DURATION:	2 Hours
AVAILABLE FIRE FLOW (@ 20 psi):	2,874 GPM @ 20 psig

	Fire Flow	Arca ft <sup>2</sup> (× 0.0929 fo	r m²)			
I(443), I(332), II(222)*	II(111), III(211)*	IV(2HH), V(111)*	II(000), III(200)* V(000)*		Fire Flow gpm <sup>†</sup> (× 3.785 for L/min)	Flow Duration (bours)
0-22,700	0-12,700	0-8200	0-5900	0-3600	1500	
22,701-30,200	12,701-17,000	8201-10,900	5901-7900	3601-4800	1750	]
30,201-38,700	17,001-21,800	10,901-12,900	7901-9800	4801-6200	2000	
38,701-18,300	21,801-24,200	12,901-17,400	9801-12,600	6201-7700	2250	2
48,301-59,000	24,201-33,200	17,401-21,300	12,601-15,400	7701-9400	2500	]
59,001-70,900	33,201-39,700	21,301-25,500	15,401-18,400	9401-11,300	2750	1
70,901-83,700	39,701-47,100	25,501-30,100	18,401-21,800	11,301-13,400	3000	
83,701-97,700	47,101-54.900	30,101-35,200	21,801-25,900	13,401-15,600	3250	1
97,701-112,700	54,90163,400	35,201-40,600	25,901-29,300	15,601-18,000	3500	3
112,701-128,700	63,401-72,400	40,601-46,400	29,301-33,500	18,001-20,600	3750	
128,701-145,900	72,401-82,100	46,401-52,500	33,501-37,900	20,601-23,300	4000	
145,901-164,200	82,101-92,400	52,501-59,100	37,901-42,700	23,301-26,300	4250	1
164,201-183,400	92,401-103,100	59,101-66,000	42,701-47,700	26,301-29,300	4500	1
183,401-203,700	103,101-114,600	66,001-73,300	47,701-53,000	29,301-32,600	4750	1
203,701-225,200	114,601-126,700	73,301-81,100	58,001-58,600	32,601-36,000	5000	1
225,201-247,700	126,701-139,400	81,101-89,200	58,601-65,400	36,00139,600	5250	1
247,701-271,200	139,401-152,600	89,201-97,700	65,401-70,600	39,601-43,400	5500	
271.201-295,900	152,601-166,500	97,701-106,500	70,601-77,000	43,401-47,400	5750	
Greater than 295,900	Greater than 166,500	106,501-115,800	77,001-83,700	47,401-51,500	6000	А
		115,801-125,500	83,701-90,600	51,501-55,700	6250	-
		125,501-135,500	90,601-97,900	55,70160,200	6500	
		135,501-145,800	97,901-106,800	60,201-64,800	6750	
		145,801-156,700	106,801-113,200	64,801-69,600	7000	
		156,701-167,900	113,201-121,300	69,601-74,600	7250	
		167,901-179,400	121,301-129,600	74,601-79,800	7500	
		179,401-191,400	129,601-138,300	79,801-85,100	7750	
		Greater than 191,400	Greater than 138,300	Greater than 85,100	8000	

Table 18.4.5.1.2 Minimum Required Fire Flow and Flow Duration for Buildings

\*Types of construction are based on NFPA 220.

.

•

<sup>†</sup>Measured at 20 psi (139.9 kPa).

.

, ,

ł

#### 18.4 Fire Flow Requirements for Buildings.

#### 18.4.1\* Scope.

З

18.4.1.1\* The procedure determining fire flow requirements for buildings hereafter constructed shall be in accordance with Section 18.4.

**18.4.1.2** Section 18.4 does not apply to structures other than buildings.

18.4.2 Definitions. Scc definitions 3.3.13.6 (Fire Flow Arca) and 3.3.108 (Fire Flow).

#### 18.4.3 Modifications.

18.4.3.1 Decreases. Fire flow requirements shall be permitted to be modified downward by the AHJ for isolated buildings or a group of buildings in rural areas or small communities where the development of full fire flow requirements is impractical.

18.4.3.2 Increases. Fire flow shall be permitted to be modified upward by the AHJ where conditions indicate an unusual susceptibility to group fires or conflagrations. An upward modification shall not be more than twice that required for the building under consideration.

#### 18.4.4 Fire Flow Area.

18.4.4.1 General. The fire flow area shall be the total floor area of all floor levels of a building except as modified in 18.4.4.1.1.

18.4.4.1.1 Type I (443), Type I (332), and Type II (222) Construction. The fire flow area of a building constructed of Type I (443), Type I (332), and Type II (222) construction shall be the area of the three largest successive floors.

18.4.5 Fire Flow Requirements for Buildings.

18.4.5.1 One- and Two-Family Dwellings.

18.4.5.1.1 The minimum fire flow and flow duration requirements for one- and two-family dwellings having a fire flow area that does not exceed 5000 ft<sup>2</sup> ( $334.5 \text{ m}^2$ ) shall be 1000 gpm (3785 L/min) for 1 hour.

18.4.5.1.1.1 A reduction in required fire flow of 50 percent shall be permitted when the building is provided with an approved automatic sprinkler system.

18.4.5.1.1.2 A reduction in the required fire flow of 25 percent shall be permitted when the building is separated from other buildings by a minimum of 30 ft (9.1 m).

**18.4.5.1.1.3** The reduction in 18.4.5.1.1.1 and 18.4.5.1.1.2 shall not reduce the required fire flow to less than 500 gpm (1900 L/min).

**18.4.5.1.2** Fire flow and flow duration for dwellings having a fire flow area in excess of 5000 ft<sup>2</sup> (334.5 m<sup>2</sup>) shall not be less than that specified in Table 18.4.5.1.2.

18.4.5.1.2.1 A reduction in required fire flow of 50 percent shall be permitted when the building is provided with an approved automatic sprinkler system.

18.4.5.2 Buildings Other Than One- and Two-Family Dwellings. The minimum fire flow and flow duration for buildings other than one- and two-family dwellings shall be as specified in Table 18.4.5.1.2.

18.4.5.2.1 A reduction in required fire flow of 75 percent shall be permitted when the building is protected throughout by an approved automatic sprinkler system. The resulting fire flow shall not be less than 1000 gpm (3785 L/min).

18.4.5.2.2 A reduction in required fire flow of 75 percent shall be permitted when the building is protected throughout by an approved automatic sprinkler system, which utilizes quick response sprinklers throughout. The resulting fire flow shall not be less than 600 gpm (2270 L/min).



4

## Attachment B ISO Colculations

(Exhibit on Next Page)

### ISO Needed Fire Flow (NFF) Worksheet

4

.

(Page references are to the appropriate sections in the ISO Guide for Determination of Needed Fire Flow)

Petition N	lumber:	Date:		6/14/2022	
<b>Project:</b>	Gateway Crossings Lot 3 - Sonic	Engineer:		GAL	
		Checked	By:	САР	
Location:	NW Centurion Blvd.		8		
	Lake City, FL	]			
	Subj	ect Buildi	ng		
Construct	tion Class (p. 4): Wood Frame Construction	n <b>T</b>	con	struction coefficient (F) (p. 2):	1.5
Area of la	rgest floor in the building (if modific	ations are i	made	e for division walls (p. 8), the	
division w	valls must be shown on the site plan.):	: 122	27	sq.ft.	
Total area	a of all other floors (if modifications a	are made fo	or div	vision walls (p. 8), the division	
walls mus	st be shown on the site plan.):		] sq.	. ft.	
Effective	Area (A <sub>i</sub> ) (p. 9) : 1,227	sq. ft.	(Sho	ow calculations below)	
Needed F	ire Flow attributed to construction (C	;) (per form	nula	(p. 2)): 945.771114	
(	Round to the nearest 250 gpm. See p	. 10 for ma	ximu	im and minimum values of C <sub>i</sub> )	
Type of C	ccupancy: Combustible (C-3)		0	ccupancy Factor (O <sub>i</sub> ) (p. 11):	1
	_				
	Expos	sures (p.	16)	f 1112	
Front:	construction of facing wall of exposu	ire building	g (p.	4):	- <b>-</b>
	Distance (ft.) to the exposure building	ıg:	•	Length of exposure wall:	
	Number of stories of exposure wall:			Length x number of stories:	0
	<b>Opening Protection in exposure wall</b>	l:			•
	Factor for exposure (X <sub>i</sub> ) from Table	e 330.A (p.	17):	0	
<b>D</b> 1			,	0	
Back:	construction of facing wall of exposu	ire building	g (p.	4):	-
	Distance (ft.) to the exposure buildin	lg:	•	Length of exposure wall:	
	Number of stories of exposure wall:		)	Length x number of stories:	0
	Opening Protection in exposure wall				_
	Factor for exposure $(X_i)$ from Table	330.A (p. 1	17):	0	
I oft.	construction of facing wall of array	wo huildin.	~ (~	<b>A</b> . [	-1
Lett:	Distance (ft) to the exposure building	ire bulluli	g (þ.	4):	
	Number of stories of exposure wells	lg:		Length of exposure wall:	
	Quanting Protection in exposure wall:			Length x humber of stories:	0
	Easter for exposure (X) from Table	1;   	( <b>7</b> ). [	2	-
	Factor for exposure $(X_i)$ from Table	550.A (p. 1	. /):	0	
Right	construction of facing wall of exposi	ure building	a (n	4)•	<b>v</b> l
***Ent+	Distance (ft ) to the exposure huildin	a o bananij	ь (P.	Length of exposure well-	— <u> </u>
	Number of stories of evocure wall.	<b>.</b>		Length y number of stories.	
	Onening Protection in exposure wall	  •		sengur a number of stories.	<u> </u>
	Eactor for exposure (X.) from Table		17)+	0	Ť
	ration for exposure (A) from Table	220.14 (h. 1	· / J•	U	

#### **Communications (p. 18)**

Passageway Opening Protection: Construction class of communication (Table 330.B) :		•
Is communication open or enclosed?		•
Length of communication (in feet):	·	*
Factor for Communications (P <sub>i</sub> ) from Table 330.B on p.19):	0	

### Calculation of Needed Fire Flow (p. 1)

 $NFF=(C_i)(O_i)[1.0+(X+P)_i]$  (substitute values as determined above. For exposures and communications use the single side with the highest charge.)

NFF=	1000	x	1	х	[	1	+	(	0	+	0	)
NFF=	1000	gpm										
NFF=	1000	gpm (ro	ounded t	o nearest		250	gpn	n per I	SO requ	irements	5)	

Note: ISO evaluates hydrant distribution by examining the number and type of hydrants within 1,000 feet of each representative building. They also look at the distance from each such hydrant to the subject building, measured as apparatus can lay hose.

Hydrants with at least one large pumper outlet may receive credit for up to 1,000 gpm. Hydrants with at least two hose outlets, but no pumper outlet, may receive credit for up to 750 gpm. And hydrants with only one hose outlet may receive credit for up to 500 gpm.

Hydrants within 300 feet of the subject building may receive credit for up to 1,000 gpm (but not more than the credit that would apply based on the number and type of outlets). Hydrants from 301 feet to 600 feet from the subject building may receive credit for up to 670 gpm (but not more than the credit that would apply based on the number and type of outlets). And hydrants from 601 feet to 1,000 feet from the subject building receive credit for 250 gpm. Under certain circumstances, when all fire department pumpers carry sufficient largediameter hose, ISO may allow maximum credit for hydrants up to 1,000 feet from the subject building.

More than one fire hydrant may be required for proper distribution of water per ISO requirements.



### Attachment C

# **City of Lake City Fire Flow Test Results**

(Exhibit on Next Page)

1

### City of Lake City Water flow report





### City of Lake City Water flow report







### REVIEW REPORT TO PLANNING AND ZONING, BOARD OF ADJUSTMENT AND HISTORICAL COMMITTEES' BY STAFF FOR SITE PLAN REVIEW, SPECIAL EXCEPTIONS, VARIANCES, COMPREHENSIVE PLAN AMENDMENTS/ ZONING AND CERTIFICATE OF APPROPRIATENESS

Date: 06/15/2025

Request Type: Site Plan Review (SPR) 🗹 Special Exception (SE) 🗌 Variances (V)
Comprehensive Plan Amendment/Zoning (CPA/Z) Certificate of Appropriateness (COA)
Project Number: SPR22-14
Project Name: Sonic (Centurion Loop)
Project Address: NW Centurion Ct, Lake City FL
Project Parcel Number: 35-3S-16-02524-103
Owner Name: Parker Neely
Owner: Address: 2682 NW Noegel Rd
Owner Contact Information: telephone number 704-577-2475 e-mail Dneely@highcotton-cep.com
Owner Agent Name: Chris Potts, PE
Owner Agent Address: 3530 NW 43rd St Gainesville FL
Owner Agent Contact Information: telephone 352-375-8999 e-mail chris.potts@jbpro.com

The City of Lake City staff has reviewed the application and documents provided for the above request and have determined the following:

### Growth Management – Building Department, Planning and Zoning, Code Enforcement, Permitting

Building Department: Approved Disapproved Reviewed by:
Planning and Zoning: Approve Disapprove Reviewed by: Robert Angelo
Zoning is consistent with the LDR
Business License: Approve Disapprove Reviewed by: Marshall Sova Comments: Will need to apply for a business license
Code Enforcement: Approve Disapprove Reviewed by: Marshall Sova
Permitting: Approve Disapprove Reviewed by: Ann Jones Comments: No concerns at this time
No concerns at this time

### Utilities - Water, Sewer, Gas, Water Distribution/Collections, Customer Service

Water Department: Approved Disapproved Reviewed by:
Comments: N/A
Sewer Department: Approved Disapproved Reviewed by:
Gas Department: Approved Disapproved Reviewed by: Steve Brown
Commenter Need BTU totals
WaterDistribution/Collection:Approved
Comments:
If they do not use the taps in place they will be required to make new ones and
cut and cap sewer and dig to water main and shut off before construction.
Customer Service: Approved Disapproved Reviewed by: Shasta Pelham
A tap application would be required to access city utilities. The tap fees, impact fees and utility deposits
will be calculated upon approval of the tap application. City utilities border the property: locates must be obtained to
ensure that the utility infrastructure is not demaged or obstructed. An appliance list with the htu loads are required for natural gas

Public Safety – Public Works, Fire Department, Police Department
Public Works: Approved Disapproved Reviewed by: Steve Brown
Comments: No concerns at this time
Fire Department: Approve Disapprove Reviewed by: Assistant Chief Boozer
Comments: Key access box and lightweight truss placard needed
Police Department: Approve Disapprove Reviewed by Assistant Chief Andy
Comments: No concerns at this time.

Please provide separate pages for comments that will not fit in provided spaces and please label the pages for your department and for the project.



06/21/2022

Sonic (Centurion Loop)

Parcel 35-3S-16-02524-103

To Whom it May Concern

Parcel **35-3S-16-02524-103** is commercial highway interchange and is conducive for the proposed use of the land. Per the City of Lake City Land Development Regulations, restaurants are permitted in this district.

Thank You

**Robert Angelo** 

Planning and Zoning Tech

386-719-5820

## **Project Summary**

Project Name: Sonic- Centurion Loop

Project Number: SPR22-14

Parcel Number: 02524-103

### **Project Notes**

- Project type: Site Plan Review
- Future land use is: Commercial
- Zoning designation is: Commercial Highway Interchange
- Proposed use of the property: New construction of Sonic
- Land is conducive for use: Yes, per the LDR section 4.15.2.3
- See staff review for notes from directors and city staff for their comments.

### **Project Summary**

Project SPR22-14 is for a site plan review and has been reviewed by city staff. Application is sufficient for review. After review of the petition the city staff has determined that the petition is consistent with the land development regulations and the comprehensive plan. At this time the City has not concerns.