

PLANNING AND ZONING BOARD MEETING

CITY OF LAKE CITY

July 06, 2022 at 5:30 PM

Venue: City Hall

AGENDA

The meeting will be held in the City Council Chambers on the second floor of City Hall located at 205 North Marion Avenue, Lake City, FL 32055. Members of the public may also view the meeting on our YouTube channel. YouTube channel information is located at the end of this agenda.

INVOCATION

ROLL CALL

MINUTES

- [i.](#) June 7, 2022

OLD BUSINESS - None

NEW BUSINESS

- [ii.](#) Site Plan Review - SPR-22-15 - Daniel Hotte of GWC Development Partners, LLC (Agent: Jarod Stubbs, P.E.) Circle K on US 90
- [iii.](#) Site Plan Review - SPR-22-14 - Parker Neely (Agent: Chris Potts, P.E.) Sonic Drive In

WORKSHOP - None

ADJOURNMENT

YouTube Channel Information

Members of the public may also view the meeting on our YouTube channel at: <https://www.youtube.com/c/CityofLakeCity>

Pursuant to 286.0105, Florida Statutes, the City hereby advises the public if a person decides to appeal any decision made by the City Council with respect to any matter considered at its meeting or hearings, he or she will need a record of the proceedings, and that, for such purpose, he or she may need to ensure that a verbatim record of the proceedings is made, which record includes the testimony and evidence upon which the appeal is to be based.

Pursuant to 286.26, Florida Statutes, persons needing special accommodations to participate in this meeting should contact the City Manager's Office at (386) 719-5768.

File Attachments for Item:

i. June 7, 2022

**Meeting Minutes
Planning and Zoning**

Date: 06/07/2022

Roll Call:

Mr. Lydick-Present
Mr. Cooper-Present
Mr. Nelson-Present
Ms. Georgalis-Present

Mr. Carter-Present
Mrs. McKellum-Present
Mr. McMahon-Present

Approval of Past Minutes-Approve the minutes of the 05/03/2022 Meeting.

Motion By: Mr. Lydick

Seconded By: Mr. McMahon

Comments or Revisions:

Mr. Lydick motioned to move the invocation from the historic preservation meeting to the planning and zoning meeting.

Old Business: None

New Business:

Petition # CPA 22-04 and Z22-03 Presented By: **Daniel Crapps**

As owner or agent and gives address of: **2806 W Hwy 90 Lake City FL 32025**

Petitioner is Sworn in by: Ms.Georgalis

Discussion:

Mr. Crapps talked about the need for housing in the area and that there is a housing shortage in Lake City. Mr. Angelo asked if we were talking about the same property because Mr. Crapps said that the property was 19 acres and Mr. Angelo called out that the application said 1.63 acres. Mr. Crapps confirmed that this was the right property. Mr. Lydick asked if lots 47 and 48 were merged. Mr. Crapps said that on the property appraisers web site they were but not legally yet. **Approved unanimously.**

Motion to close Public Hearing: Mr. Lydick

Motion Seconded By: **Mr. Carter**

Motion to Approve/Deny By: Mr. Nelson

Motion Seconded By: **Mr. Carter**

Petition # CPA22-05 and Z22-04 Presented by **Isaac Schlimmer**

As owner or agent and gives address of: **187 Old Cypress Way Lake City FL 32024**

Petitioner is Sworn in by: Ms. Georgalis

Discussion:

Mr. Schlimmer is proposing to build a 12-unit multi housing with 1000 square feet per unit. He talked about the need for housing in the area. Mr. Lydick asked if the current RMF-1 parcels in the area were being used for multifamily or single family. Mr. Schlimmer did not know. Mrs. McKellum asked if there were any other apartments in the area. Mr. Schlimmer did not know. Barbara Carrington of 1097 Tustenuggee said that her parents live there and when they bought the property it was all single family and that is what the people there want. She and other put a petition together to not have this rezoned because of drainage issues and increase in traffic and noise. Mr. Cooper said that he did not agree with multifamily being in the middle of single family but did agree that we need more housing. Kirsinda Byrd of 905 NW Folwer Said that she and her family lived two parcels down from this property and that currently the neighborhood is quiet and they want to keep it that way. Mr. Lydick asked about a possible zoning island but also mentioned that there is multifamily zoning in the area. Ms. Georgalis said that they have to take in to count the possible drainage issues and the change in the life style in the area that this would bring. **Petition passed with a vote of 4 yes to 3 no.**

Motion to close Public Hearing: Mr. McMahan

Motion Seconded By: **Mr. Lydick**

Motion to Approve/Deny By: Mr. Nelson

Motion Seconded By: **Mr. Lydick**

Petition # SPR22-13 Presented By: **Carol Chadwick**

As owner or agent and gives address of: **1208 SW Fairfax Glen Lake City FL 32025**

Discussion:

Carol Chadwick presented the site plan for Frank and Lane's Heating and Air. Mr. Lydick asked why only one tree was being saved. Ms. Chadwick said that as many trees as possible were going to be saved.

Approved unanimously.

Motion to close Public Hearing: Mr. Carter

Motion Seconded By: **Mr. Lydick**

Motion to Approve/Deny By: Mr. Carter

Motion Seconded By: **Mr. Nelson**

Petition # V22-01, SE22-01, and SPR22-02 Presented By: **Brian Pitman**

As owner or agent and gives address of: **206 S Marion Ave Lake City FL 32025**

Discussion:

Mr. Pitman presented the site plan and the need for the variance and special exception. Mr. Lydick asked why we were hearing a variance is the planning and zoning meeting. Mr. Angelo said that was being heard concurrently with the special exception due to the fact that if the variance was not approved the others would not be approved either. Brian said that the reason for the variance was due to the seven-foot encroachment of the setback. Mr. McMahan asked if the lot was clear. Mr. Pitman said that it was not. Mr. Nelson asked what was behind the property if it was residential. Mr. Pitman said that it was a large retention pond used for the commercial properties around it. **All three petitions were approved unanimously.**

Motion to close Public Hearing: Mr. Carter
Motion Seconded By: **Mr. Nelson**

Motion to Approve/Deny By: Mr. Lydick
Motion Seconded By: **Mr. Nelson**

Motion to Adjourn by: Mr. Lydick
Time: **6:12**
Motion Seconded By: **Mr. Carter**

Mavis Georgalis, Board Chairperson

Date Approved

Robert Angelo, Secretary

Date Approved

File Attachments for Item:

ii. Site Plan Review - SPR-22-15 - Daniel Hotte of GWC Development Partners, LLC (Agent: Jarod Stubbs, P.E.) Circle K on US 90



GROWTH MANAGEMENT
 205 North Marion Ave.
 Lake City, FL 32055
 Telephone: (386)719-5750
 E-Mail:
 growthmanagement@lcfla.com

FOR PLANNING USE ONLY
 Application # SPR22-15
 Application Fee: \$200.00
 Receipt No. _____
 Filing Date 4/1/22
 Completeness Date _____

Site Plan Application

A. PROJECT INFORMATION

1. Project Name: CIRCLE K - US 90 & I-75
2. Address of Subject Property: 143 NW Centurion Ct., Lake City, FL 32055
3. Parcel ID Number(s): 35-3S-16-02524-001, 35-3S-16-02524-102, 35-3S-16-02524-111
4. Future Land Use Map Designation: Commercial
5. Zoning Designation: CHI - Commercial Highway Interchange
6. Acreage: ±3.46
7. Existing Use of Property: Existing Circle K gas station and convenience store
8. Proposed use of Property: Circle K gas station and high speed diesel station
9. Type of Development (Check All That Apply):
 - Increase of floor area to an existing structure: Total increase of square footage ±652 SF
 - New construction: Total square footage ±54,470 SF
 - Relocation of an existing structure: Total square footage _____

B. APPLICANT INFORMATION

1. Applicant Status Owner (title holder) **Agent**
2. Name of Applicant(s): Jarod Stubbs P.E. Title: Civil Engineer
 Company name (if applicable): Kimley-Horn
 Mailing Address: 189 S. Orange Ave. Suite 1000
 City: Orlando State: FL Zip: 32801
 Telephone: (407) 409-7002 Fax: () Email: jarod.stubbs@kimley-horn.com
PLEASE NOTE: Florida has a very broad public records law. Most written communications to or from government officials regarding government business is subject to public records requests. Your e-mail address and communications may be subject to public disclosure.
3. If the applicant is agent for the property owner*.
 Property Owner Name (title holder): Daniel Hotte of GWC Development Partners, LLC
 Mailing Address: 2682 W Noegel Rd
 City: Lake City State: FL Zip: 32055
 Telephone: (407) 580-5173 Fax: () Email: dberry@shafferconst.com
PLEASE NOTE: Florida has a very broad public records law. Most written communications to or from government officials regarding government business is subject to public records requests. Your e-mail address and communications may be subject to public disclosure.
***Must provide an executed Property Owner Affidavit Form authorizing the agent to act on behalf of the property owner.**

C. ADDITIONAL INFORMATION

- 1. Is there any additional contract for the sale of, or options to purchase, the subject property?
If yes, list the names of all parties involved: _____
If yes, is the contract/option contingent or absolute: Contingent Absolute
- 2. Has a previous application been made on all or part of the subject property? Yes No
Future Land Use Map Amendment: Yes _____ No _____
Future Land Use Map Amendment Application No. _____
Site Specific Amendment to the Official Zoning Atlas (Rezoning): Yes _____ No _____
Site Specific Amendment to the Official Zoning Atlas (Rezoning) Application No. _____
Variance: Yes _____ No _____
Variance Application No. _____
Special Exception: Yes _____ No _____
Special Exception Application No. _____

D. ATTACHMENT/SUBMITTAL REQUIREMENTS

- 1. Vicinity Map – Indicating general location of the site, abutting streets, existing utilities, complete legal description of the property in question, and adjacent land use.
- 2. Site Plan – Including, but not limited to the following:
 - a. Name, location, owner, and designer of the proposed development.
 - b. Present zoning for subject site.
 - c. Location of the site in relation to surrounding properties, including the means of ingress and egress to such properties and any screening or buffers on such properties.
 - d. Date, north arrow, and graphic scale not less than one inch equal to 50 feet.
 - e. Area and dimensions of site (Survey).
 - f. Location of all property lines, existing right-of-way approaches, sidewalks, curbs, and gutters.
 - g. Access to utilities and points of utility hook-up.
 - h. Location and dimensions of all existing and proposed parking areas and loading areas.
 - i. Location, size, and design of proposed landscaped areas (including existing trees and required landscaped buffer areas).
 - j. Location and size of any lakes, ponds, canals, or other waters and waterways.
 - k. Structures and major features fully dimensioned including setbacks, distances between structures, floor area, width of driveways, parking spaces, property or lot lines, and percent of property covered by structures.
 - l. Location of trash receptacles.
 - m. For multiple-family, hotel, motel, and mobile home park site plans:
 - i. Tabulation of gross acreage.
 - ii. Tabulation of density.
 - iii. Number of dwelling units proposed.
 - iv. Location and percent of total open space and recreation areas.
 - v. Percent of lot covered by buildings.

- vi. Floor area of dwelling units.
 - vii. Number of proposed parking spaces.
 - viii. Street layout.
 - ix. Layout of mobile home stands (for mobile home parks only).
3. Stormwater Management Plan—Including the following:
 - a. Existing contours at one foot intervals based on U.S. Coast and Geodetic Datum.
 - b. Proposed finished elevation of each building site and first floor level.
 - c. Existing and proposed stormwater management facilities with size and grades.
 - d. Proposed orderly disposal of surface water runoff.
 - e. Centerline elevations along adjacent streets.
 - f. Water management district surface water management permit.
 4. Fire Department Access and Water Supply Plan: The Fire Department Access and Water Supply Plan must demonstrate compliance with Chapter 18 of the Florida Fire Prevention Code, be located on a separate signed and sealed plan sheet, and must be prepared by a professional fire engineer licensed in the State of Florida. The Fire Department Access and Water Supply Plan must contain fire flow calculations in accordance with the Guide for Determination of Required Fire Flow, latest edition, as published by the Insurance Service Office ("ISO") and/or Chapter 18, Section 18.4 of the Florida Fire Prevention Code, whichever is greater.
 5. Concurrency Impact Analysis: Concurrency Impact Analysis of impacts to public facilities. For commercial and industrial developments, an analysis of the impacts to Transportation, Potable Water, Sanitary Sewer, and Solid Waste impacts are required.
 6. Comprehensive Plan Consistency Analysis: An analysis of the application's consistency with the Comprehensive Plan (analysis must identify specific Goals, Objectives, and Policies of the Comprehensive Plan and detail how the application complies with said Goals, Objectives, and Policies).
 7. Legal Description with Tax Parcel Number (In Word Format).
 8. Proof of Ownership (i.e. deed).
 9. Agent Authorization Form (signed and notarized).
 10. Proof of Payment of Taxes (can be obtained online via the Columbia County Tax Collector's Office).
 11. Fee. The application fee for a Site and Development Plan Application is \$200.00. No application shall be accepted or processed until the required application fee has been paid.

NOTICE TO APPLICANT

All eleven (11) attachments are required for a complete application. Once an application is submitted and paid for, a completeness review will be done to ensure all the requirements for a complete application have been met. If there are any deficiencies, the applicant will be notified in writing. If an application is deemed to be incomplete, it may cause a delay in the scheduling of the application before the Planning & Zoning Board.

A total of ten (10) copies of proposed site plan application and all support materials must be submitted along with a PDF copy on a CD. See City of Lake City submittal guidelines for additional submittal requirements.

THE APPLICANT ACKNOWLEDGES THAT THE APPLICANT OR AGENT MUST BE PRESENT AT THE PUBLIC HEARING BEFORE THE PLANNING AND ZONING BOARD, AS ADOPTED IN THE BOARD RULES AND PROCEDURES, OTHERWISE THE REQUEST MAY BE CONTINUED TO A FUTURE HEARING DATE.

I hereby certify that all of the above statements and statements contained in any documents or plans submitted herewith are true and accurate to the best of my knowledge and belief.

Jared Stubbs
Applicant/Agent Name (Type or Print)

[Signature]
Applicant/Agent Signature

6/8/2022
Date

Applicant/Agent Name (Type or Print)

Applicant/Agent Signature

Date

STATE OF FLORIDA
COUNTY OF Orange

The foregoing instrument was acknowledged before me this 8th day of June 2022 by (name of person acknowledging) Jared Stubbs.

(NOTARY SEAL or )

[Signature]
Signature of Notary

Printed Name of Notary

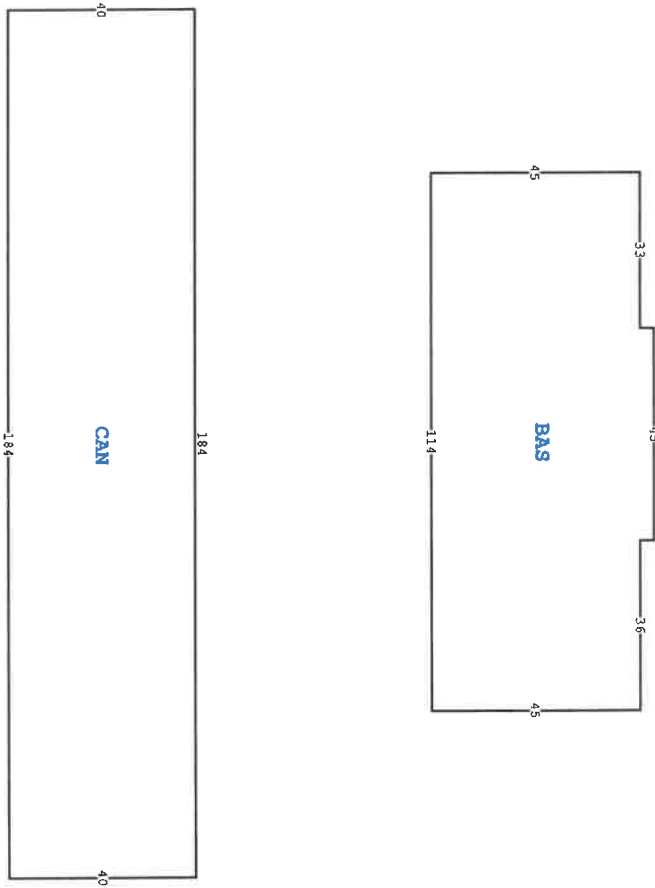
Personally Known OR Produced Identification
Type of Identification Produced

2022

35-3S-16-02524-001

BUILDING CHARACTERISTICS
CD CONSTRUCTION
MARKET ADJUSTMENTS
COLUMBIA COUNTY PROPERTY VALUATION SUMMARY

ELEMENT	CD	CONSTRUCTION	TYPE	MDL	EFF. AREA	TOT ADJ PTS	EFF. BASE RATE	REPL COST NEW	AVG	EVB	ECON FNCT	NORM	% COND
Exterior Wall	17	MSTRY STCC	4300	04	7,473	161,1610	97.50	728,618	2017	2017	0	3.00	97.00
Exterior Wall	21	STONE	1	NBHD CONVE	- 0%	- 0							
Roof Structure	09	RIDGE FRMR	100										
Roof Cover	04	BUILT-UP	100										
Interior Wall	08	DECORATIVE	100										
Interior Floor	15	HARDTILE	100										
Ceiling	01	FIN. SUSPD	100										
Air Condition	06	ENG CENTRL	100										
Heating Type	09	ENG F AIR	100										
Fixtures	12	100											
Frame	03	MASONRY	100										
Story Height	12	100											
RMS	0	100											
Stories	1.	1.100											
Units	0	100											
Condition Adj	04	04	100										
Quality	08	08											
DOR CODE	1126	CONV STORE/GAS	06										
MAP NUM	35316.040	MKT AREA	1.00										
RHOOD													
TOTAL GROSS AREA	5,265	PCT OF BASE	100										
TOTAL GROSS AREA	7,360	TOT ADJ AREA	5,265										
TOTAL GROSS AREA	30	SUBAREA MARKET VALUE	497,938										
TOTAL GROSS AREA	30	SUBAREA MARKET VALUE	208,822										



TOTALS 12,625 7,473 706,759 143 NW CENTURION CT, LAKE CITY

L N	OBXF	DESCRIPTION	BLD CAP	L	W	UNITS	UT	ADJ R	ADJ UNIT PRICE	ORIG COND	YEAR ON	YEAR ACTUAL	Q	% COND	OBXF MKT VALUE	BLD DATE	XF DATE	INC DATE	LGJ DATE	LAND DATE	NOTES																																																	
1	0260	PAVEMENT-A	0	0	0	36,400.00	UT 1.60		1.60	100	2017	2017	3	100	58,240																																																							
2	0166	CONC. PAYMT	0	0	0	11,300.00	UT 2.25		2.25	100	2017	2017	3	100	25,425																																																							
3	0253	LIGHTING	0	0	0	10.00	UT 1,500.00		1,500.00	100	2017	2017	3	100	15,000																																																							
4	0164	CONC BIN	0	0	0	308.00	UT 11.00		11.00	100	2017	2017	3	100	3,388																																																							
5	59	FENCE/WOOD	0	0	0	74.00	UT 15.50		15.50	100	2017	2017	3	100	1,147																																																							
<p>LAND DESCRIPTION</p> <table border="1"> <thead> <tr> <th>L</th> <th>USE</th> <th>LAND USE DESCRIPTION</th> <th>CAP</th> <th>D</th> <th>LOC ZONE</th> <th>FRONT</th> <th>DEPTH</th> <th>TOT LND UTS</th> <th>UNIT TYPE</th> <th>D</th> <th>DPTH</th> <th>FACT</th> <th>% COND</th> <th>TOT ADJ</th> <th>UNIT PRICE</th> <th>ADJ UNIT PRICE</th> <th>LAND VALUE</th> <th>OTHER ADJUSTMENTS AND NOTES</th> <th>YEAR</th> <th>DENSITY</th> <th>DECL</th> <th>FRZ</th> <th>YR</th> <th>CONSVY</th> </tr> </thead> <tbody> <tr> <td>1</td> <td>1410</td> <td>CONV STORE</td> <td>0</td> <td></td> <td></td> <td>0.00</td> <td>0.00</td> <td>86,206.00</td> <td>SF</td> <td>1.00</td> <td>1.00</td> <td>1.25</td> <td></td> <td>11.50</td> <td>14.38</td> <td>1,239,211</td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> </tbody> </table> <p>TOTAL OBXF 103,200</p>																					L	USE	LAND USE DESCRIPTION	CAP	D	LOC ZONE	FRONT	DEPTH	TOT LND UTS	UNIT TYPE	D	DPTH	FACT	% COND	TOT ADJ	UNIT PRICE	ADJ UNIT PRICE	LAND VALUE	OTHER ADJUSTMENTS AND NOTES	YEAR	DENSITY	DECL	FRZ	YR	CONSVY	1	1410	CONV STORE	0			0.00	0.00	86,206.00	SF	1.00	1.00	1.25		11.50	14.38	1,239,211								
L	USE	LAND USE DESCRIPTION	CAP	D	LOC ZONE	FRONT	DEPTH	TOT LND UTS	UNIT TYPE	D	DPTH	FACT	% COND	TOT ADJ	UNIT PRICE	ADJ UNIT PRICE	LAND VALUE	OTHER ADJUSTMENTS AND NOTES	YEAR	DENSITY	DECL	FRZ	YR	CONSVY																																														
1	1410	CONV STORE	0			0.00	0.00	86,206.00	SF	1.00	1.00	1.25		11.50	14.38	1,239,211																																																						
<p>REVIEW DATE 11/01/2018 BY MBBC Total Acres: 1.98 Total Land Value: 1,239,211 Market: 0 Agricultural: 0 Common: 1,239,211 PRINTED 02/10/2022</p>																																																																						

BUILDING NOTES

BAS= W36 N3 W45 S3 W33 S45 E114 PTR=SS0 E35 CAN= W184 S40
 B184 N40S W35 N50S N45S.

BUILDING DIMENSIONS

OFF RECORD	DATE	TYPE	Q	V	INST	I	CD	RSN	SALE PRICE
1339/0654	6/20/2017	WD	Q	I	I		01		2,487,200

SALES DATA

PERMIT NUM	DESCRIPTION	AMT	ISSUED

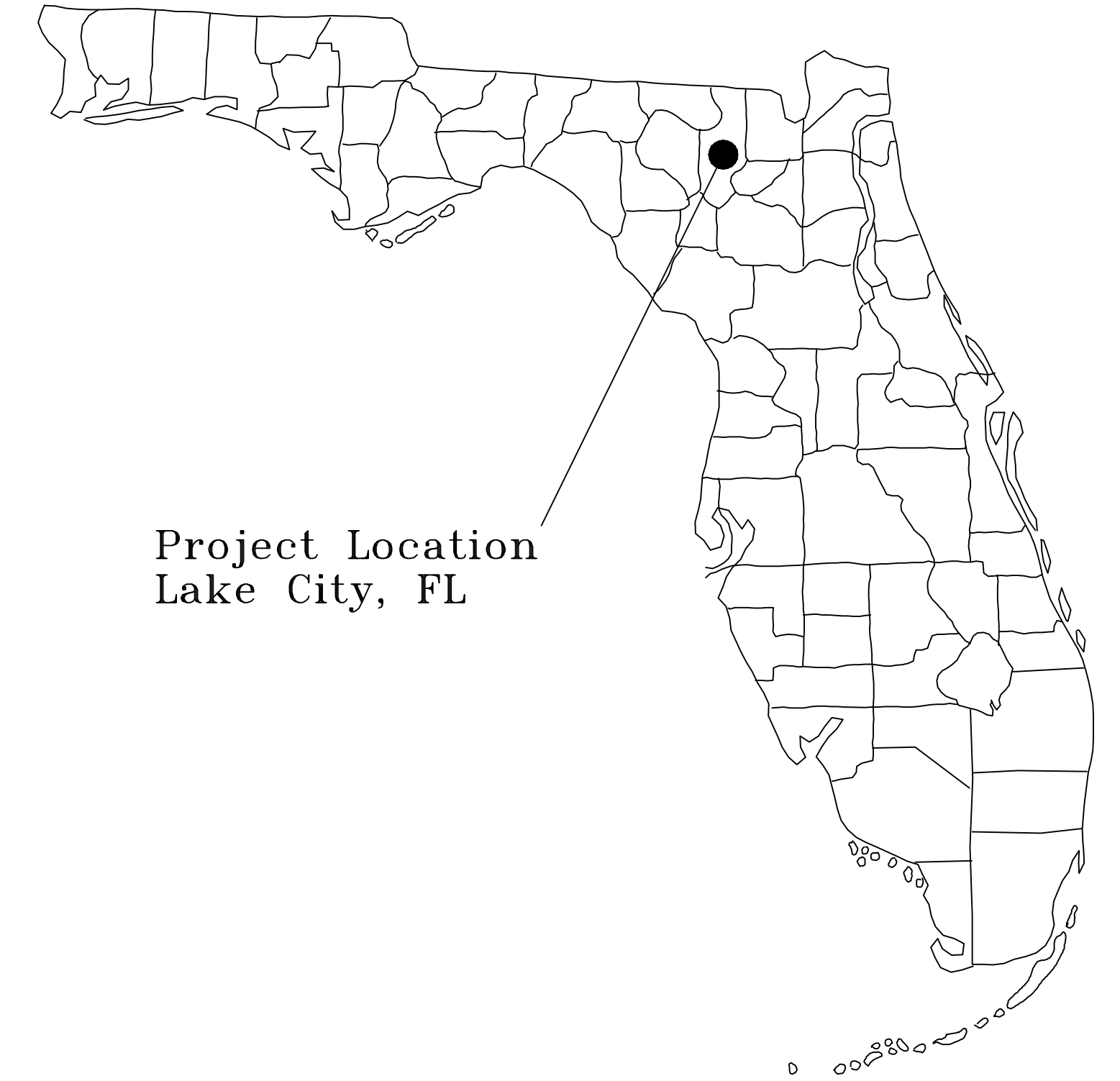
VALUATION BY

VALUATION BY	Tax Dist:	STANDARD
BUILDING MARKET VALUE		706,759
TOTAL MARKET OBXF VALUE		103,200
TOTAL LAND VALUE - MARKET		1,239,211
TOTAL MARKET VALUE		2,049,170
SOH/AGL Deduction		0
ASSESSED VALUE		2,049,170
TOTAL EXEMPTION VALUE		0
BASE TAXABLE VALUE		2,049,170
TOTAL JUST VALUE		2,049,170
INCOME VALUE		2,056,457
PREVIOUS YEAR MKT VALUE		

CONSTRUCTION PLANS FOR CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION

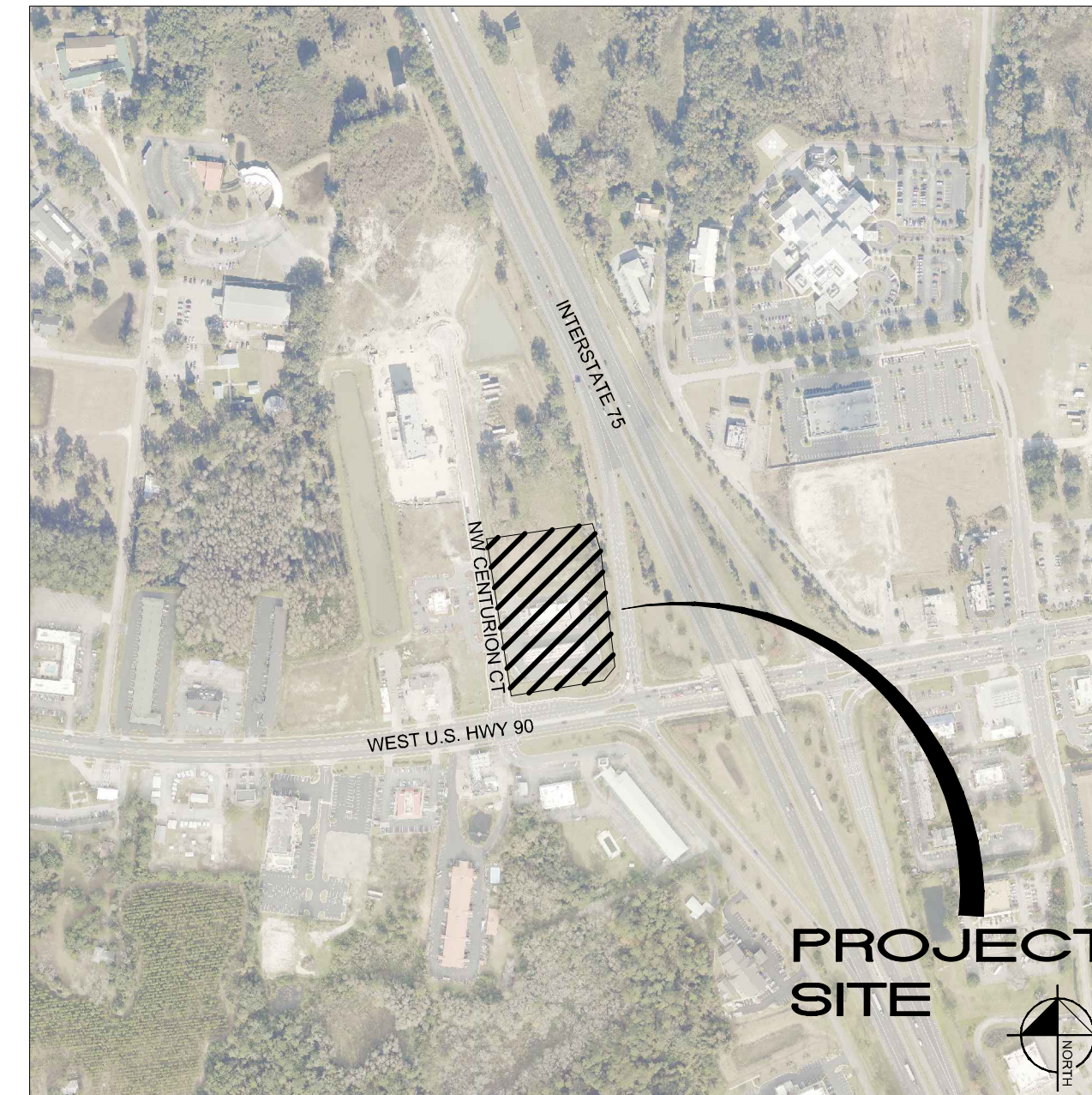
143 NW CENTURION COURT
LAKE CITY, FLORIDA 32055
MAY 4, 2022

PARCEL IDs: 35-3S-16-02524-001,
35-3S-16-02524-102 AND 35-3S-16-02524-111



LEGAL DESCRIPTION

LOT 2 AND THE NORTH 34.55 FEET OF LOT 11 OF GATEWAY CROSSING A REPLAT OF LOTS 2 & 3, ACCORDING TO PLAT THEREOF RECORDED IN PLAT BOOK 9, PAGE 151, PUBLIC RECORDS OF COLUMBIA COUNTY, FLORIDA.



VICINITY MAP

PROJECT TEAM

SHEET INDEX

C0.0	COVER SHEET
C1.0-C1.1	GENERAL NOTES
C2.0	STORMWATER POLLUTION PREVENTION PLAN
C3.0-C3.1	EXISTING CONDITIONS & DEMOLITION PLAN
C4.0	OVERALL SITE PLAN
C4.1	SITE PLAN
C4.2	INTERSECTION MODIFICATION PLAN
C4.3-C4.5	TRUCK TURNING MOVEMENTS
C5.0	PAVING, GRADING AND DRAINAGE PLAN
C6.0	UTILITY PLAN
C7.0-C7.1	GENERAL CONSTRUCTION DETAILS
L1.00	LANDSCAPE PLAN
L1.50	LANDSCAPE DETAILS
L1.51	LANDSCAPE SPECIFICATIONS
L2.00	SCHEMATIC IRRIGATION PLAN
L2.50	IRRIGATION DETAILS
L2.51	IRRIGATION NOTES

UTILITY PROVIDERS

WATER/SEWER:

CITY OF LAKE CITY UTILITIES
692 SW SAINT MARGARETS ST
LAKE CITY, FL 32025
CONTACT:
PHONE:

ELECTRIC :

FLORIDA POWER & LIGHT
2618 NE BASCOM NORRIS DRIVE
LAKE CITY, FL 32055
CONTACT: SHANE EUBANK
PHONE: (386) 754-2020

FIBER OPTIC :

HARGRAY OF FLORIDA, INC.
8324 BAYMEADOWS WAY, STE. 102
JACKSONVILLE, FL 32256
CONTACT: EDWARD HARDING
PHONE: (904) 652-9934

CABLE:

COMCAST CABLE
5934 RICHARD STREET
JACKSONVILLE, FL 32216
CONTACT: ANDREW SWEENEY
PHONE: (904) 738-6898

TELEPHONE:

AT&T
6628 LAKESIDE ROAD
WEST PALM BEACH, FL 33411
CONTACT: DINO FARRUGGIO
EMAIL: G27896@ATT.COM
PHONE: (561) 683-2729

GAS :

CITY OF LAKE CITY GAS/PUBLIC WORKS
180 NE GUM SWAMP ROAD
LAKE CITY, FL 32055
CONTACT: THOMAS HENRY
EMAIL: HENRYT@CFLA.COM
PHONE: (386) 758-5425

OWNER:

GWC DEVELOPMENT PARTNERS LLC
2682 W NOEGEL ROAD
LAKE CITY, FL 32055
CONTACT: DIANE BERRY
PHONE: (407) 580-5173
EMAIL: DBERRY@SCHAFERCONST.COM

DEVELOPER:

CIRCLE K STORES, INC
3802 CORPOREX PARK DRIVE, SUITE 413
TAMPA, FL 33619
CONTACT: EDWARD GIUNTA
PHONE: (407) 580-5173

CIVIL ENGINEER:

KIMLEY-HORN AND ASSOCIATES, INC.
189 SOUTH ORANGE AVENUE, SUITE 1000
ORLANDO, FL 32801
CONTACT: JAROD C. STUBBS, P.E.
PHONE: (407) 409-7002
EMAIL: JAROD.STUBBS@KIMLEY-HORN.COM

ARCHITECT:

RDC COLLABORATIVE
11921 FREEDOM DRIVE, SUITE #1110
RESTON, VA 20190
CONTACT: MEGAN LARGENT
PHONE: (703) 668-0086
FAX: (703) 668-0085

SURVEYOR:

JBPRO
3530 NW 43RD STREET
GAINESVILLE, FL 32606
CONTACT: TROY V. WRIGHT
PHONE: (352) 375-8999

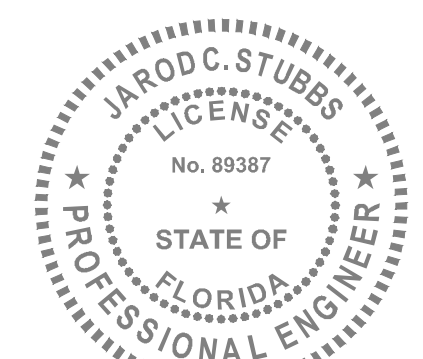
LANDSCAPE ARCHITECT:

KIMLEY-HORN AND ASSOCIATES, INC.
189 SOUTH ORANGE AVENUE, SUITE 1000
ORLANDO, FL 32801
CONTACT: MATTHEW FRANKO
PHONE: (407) 427-1629
EMAIL: MATT.FRANKO@KIMLEY-HORN.COM

PREPARED BY

Kimley»Horn

© 2022 KIMLEY-HORN AND ASSOCIATES, INC.
189 S. ORANGE AVE, SUITE 1000, ORLANDO, FL 32801
PHONE: 407-898-1511
WWW.KIMLEY-HORN.COM REGISTRY No. 35106



JAROD C. STUBBS, P.E.
FL P.E. # 89387
05/04/2022

CIRCLE K - US HWY 90 & I-75 HIGH
SPEED DIESEL FUEL EXPANSION
MAY 4, 2022
ENGINEER'S PROJECT No. 149880040
REVISIONS:
DATE

Plotted By: Parks, Ethan - Sheet Set: CIRCLE K - US HWY 30 & I-75 High Speed Diesel Fuel Expansion - Layout: C1.1 - GENERAL NOTES - June 08, 2022 - 04:28:56pm - K:\ORL\Civil\149880040-Circle K US90 & I75\CADD\CONSTR\PlanSheets\C1.0-NOTES.dwg
 This document, together with the concepts and designs presented herein, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.

SANITARY SYSTEM

1. ALL PVC PIPE SHALL BE SOLID WALL POLYVINYL CHLORIDE PIPE AND COMPLY WITH ASTM D 3034 AND ALL APPLICABLE ASTM DOCUMENTS AS COVERED IN SECTION NO. 2 OF ASTM D 3034. MAIN LINES SHALL BE A MINIMUM OF 8" DIAMETER, AND LATERALS SHALL BE A MINIMUM 6" DIAMETER.
2. ALL GRAVITY SEWERS MUST BE SDR 26 PVC. ELASTOMERIC GASKET JOINTS SHALL BE UTILIZED FOR PVC PIPE, AND SHALL COMPLY WITH ASTM F477, ASTM D3034 & ASTM F679. JOINTS SHALL COMPLY WITH ASTM D3212.
3. ALL SLOPES FOR GRAVITY SEWER MAINS AND SERVICE CONNECTIONS SHALL COMPLY WITH THE FOLLOWING MINIMUM GRADES: 4" @ 2.00%; 6" @ 1.00% ; AND 8" @ 0.40%.
4. ALL SANITARY SEWER WORK SHALL CONFORM WITH APPLICABLE CITY OF LAKE CITY WATER UTILITIES DEPARTMENT STANDARDS AND SPECIFICATIONS.
5. PRIOR TO COMMENCING WORK WHICH REQUIRES CONNECTING PROPOSED FACILITIES TO EXISTING LINES OR APPURTENANCES, THE CONTRACTOR SHALL VERIFY THE LOCATION AND ELEVATION(S) OF EXISTING CONNECTION POINT(S) AND NOTIFY THE OWNER'S ENGINEER OF ANY CONFLICTS OR DISCREPANCIES.

SANITARY TESTING AND INSPECTION

1. ALL GRAVITY SEWER PIPING SHALL BE SUBJECT TO A VISUAL INSPECTION BY THE OWNER'S ENGINEER AND APPLICABLE MUNICIPALITY/AGENCY. THE CONTRACTOR SHALL NOTIFY THE ENGINEER 48 HOURS IN ADVANCE TO SCHEDULE INSPECTION(S). THE CONTRACTOR SHALL BE RESPONSIBLE FOR COSTS ASSOCIATED WITH A LAMPING INSPECTION OF THE PROPOSED GRAVITY SEWER LINE CONSTRUCTION. THE CONTRACTOR SHALL PROVIDE COPIES OF THE LAMPING INSPECTION TO THE ENGINEER, THE OWNER AND THE APPLICABLE MUNICIPALITY/AGENCY.
2. THE CONTRACTOR SHALL PERFORM AN INFILTRATION/EXFILTRATION TEST ON ALL GRAVITY SEWERS IN ACCORDANCE WITH THE REGULATORY AGENCY HAVING JURISDICTION. SAID TESTS ARE TO BE CERTIFIED BY THE ENGINEER OF RECORD AND SUBMITTED TO THE REGULATORY AGENCY FOR APPROVAL. THE SCHEDULING, COORDINATION AND NOTIFICATION OF ALL PARTIES IS THE CONTRACTOR'S RESPONSIBILITY.
3. LEAKAGE TESTS ARE SPECIFIED REQUIRING THAT:
 - A. THE LEAKAGE EXFILTRATION OR INFILTRATION DOES NOT EXCEED 200 GALLONS PER INCH OF PIPE DIAMETER PER MILE PER DAY FOR ANY SECTION OF THE SYSTEM.
 - B. EXFILTRATION OR INFILTRATION TESTS BE PERFORMED WITH A MINIMUM POSITIVE HEAD OF 2 FEET
 - C. AIR TESTS, AS A MINIMUM, CONFORM TO THE TEST PROCEDURE DESCRIBED IN ASTM C-828 FOR CLAY PIPE, ASTM C 924 FOR CONCRETE PIPE, ASTM F-1417 FOR PLASTIC PIPE, AND FOR OTHER MATERIALS APPROPRIATE TEST PROCEDURES.
4. CONTRACTOR TO PERFORM APPROPRIATE DEFLECTION TESTS FOR ALL FLEXIBLE PIPE. TESTING IS REQUIRED AFTER THE FINAL BACKFILL HAS BEEN IN PLACE AT LEAST 30 DAYS TO PERMIT STABILIZATION OF THE SOIL-PIPE SYSTEM. TESTING REQUIREMENTS SPECIFY:
 - A. NO PIPE SHALL EXCEED A DEFLECTION OF 5%.
 - B. USING A RIGID BALL OR MANDREL FOR THE DEFLECTION TEST WITH A DIAMETER NOT LESS THAN 95% OF THE BASE INSIDE DIAMETER OR AVERAGE INSIDE DIAMETER OF THE PIPE, DEPENDING ON WHICH IS SPECIFIED IN THE ASTM SPECIFICATION, INCLUDING THE APPENDIX, TO WHICH THE PIPE IS MANUFACTURED.
 - C. PERFORMING THE TEST WITHOUT MECHANICAL PULLING DEVICES.
5. CONTRACTOR TO INSPECT & TEST MANHOLE FOR WATERTIGHTNESS OR DAMAGE PRIOR TO PLACING INTO SERVICE. AIR TESTING, IF SPECIFIED FOR CONCRETE SEWER MANHOLES, SHALL CONFORM TO THE TEST PROCEDURES DESCRIBED IN ASTM C-1244.

POTABLE WATER SYSTEM

1. ALL DIP PIPE SHALL BE CLASS 50 OR HIGHER, REFER TO NOTE #4 BELOW FOR ADDITIONAL DIP SPECIFICATIONS. ADEQUATE MEASURES (PER AWWA, FDEP, AND POLK COUNTY CRITERIA) AGAINST CORROSION SHALL BE UTILIZED.
2. ALL WATER MAIN PIPE FITTINGS AND APPURTENANCES SHALL BE INSTALLED TO COMPLY WITH POLK COUNTY STANDARDS AND SPECIFICATIONS.
3. ALL WATER SERVICE LINES, VALVES AND METERS SHALL BE INSTALLED TO COMPLY WITH APPLICABLE MUNICIPALITY/AGENCY DEPARTMENT STANDARDS AND SPECIFICATIONS.
4. ALL DUCTILE IRON PIPE, 4" TO 24", SHALL BE MANUFACTURED IN ACCORDANCE WITH THE LATEST EDITION OF AWWA C151/A21.51. PIPE SHALL BE FURNISHED IN 18 OR 20 FOOT SECTIONS, PIPE THICKNESS SHALL BE CLASS 50, UNLESS OTHERWISE SPECIFIED.
5. ALL WATER SYSTEM CONSTRUCTION, FROM THE POINT OF CONNECTION IN THE RIGHT OF WAY UP TO AND INCLUDING POINT OF METERING AND BACK FLOW PREVENTION (IF REQUIRED), SHALL BE BUILT ACCORDING TO POLK COUNTY STANDARDS AND SPECIFICATIONS.
6. CONTRACTOR TO INSTALL TEMPORARY BLOWOFFS, AT THE END(S) OF PROPOSED WATER MAINS AND SERVICE LATERALS TO BUILDING(S), TO ASSURE ADEQUATE (PER AWWA, FDEP, AND POLK COUNTY CRITERIA) FLUSHING AND DISINFECTION/CHLORINATION.
7. ALL WATER MAINS SHALL BE STERILIZED IN ACCORDANCE WITH THE APPLICABLE SECTION OF THE LATEST AWWA SPECIFICATION C651 AND CITY OF Land City WATER DEPARTMENT SPECIFICATIONS.
8. ALL PVC WATER MAIN, 6" TO 12" DIAMETER PIPING, SHALL BE AWWA C-900 DR-18. JOINTS SHALL BE RUBBER GASKETED PUSH-ON CONFORMING TO ASTM D1869.
9. POTABLE WATER MAINS WILL BE PVC SDR 21 (200 PSI) FOR PIPES LESS THEN 4". SCHEDULE 40 AND SCHEDULE 80 PIPING MATERIAL ARE ALSO ACCEPTABLE FOR PIPES SIZES LESS THAN 4". THE ABOVE TYPE INSTALLATIONS MUST BEAR THE "NFS" STAMP FOR COMPATIBILITY WITH POTABLE WATER USE.
10. ALL POLYVINYL CHLORIDE PIPE SHALL BE LAID WITH AN INSULATED 10 GAUGE A.W.G. SOLID STRAND COPPER WIRE ON TOP OF THE PIPE. THIS WIRE IS TO BE CONTINUOUS WITH SPLICES MADE ONLY BY METHODS APPROVED BY THE ENGINEER. THIS WIRE IS TO BE SECURED TO ALL VALVES, TEES AND ELBOWS.
11. ALL POTABLE WATER WORK SHALL CONFORM WITH APPLICABLE POLK COUNTY UTILITIES DEPARTMENT STANDARDS AND SPECIFICATIONS.
12. PVC PIPE BURIED BENEATH ROADWAYS, PARKING LOTS OR PARKING LOT ENTRANCES SHALL MEET AWWA SPECIFICATION C900 OR C905, LATEST REVISION. ALL 6" TO 12" PIPE IN SUCH LOCATIONS SHALL BE A MINIMUM OF CLASS 200, DR-14, AND ALL 14" TO 36" PIPE SHALL BE A MINIMUM OF CLASS 235, DR-18.

POTABLE WATER TESTING AND INSPECTION

1. ALL COMPONENTS OF THE WATER SYSTEM, INCLUDING FITTINGS, HYDRANTS, CONNECTIONS, AND VALVES SHALL BE PROPERLY PRESSURE TESTED AND ACCEPTED BY THE OWNER'S ENGINEER. PRESSURE TESTS TO BE IN ACCORDANCE WITH POLK COUNTY UTILITIES DEPARTMENT SPECIFICATIONS. CONTRACTOR TO NOTIFY THE OWNER'S ENGINEER AND APPLICABLE AGENCY INSPECTORS 2 FULL BUSINESS DAYS IN ADVANCE OF PERFORMING TESTS.
2. CONTRACTOR TO PERFORM CHLORINATION AND BACTERIOLOGICAL SAMPLING, AND OBTAIN CLEARANCE OF DOMESTIC AND FIRE LINE WATER SYSTEM(S). COPIES OF ALL BACTERIOLOGICAL TEST RESULTS ARE TO BE SUBMITTED TO THE OWNER'S ENGINEER FOR CERTIFICATION PURPOSES.
3. ALL WATER MAINS SHALL BE PRESSURE TESTED IN ACCORDANCE WITH AWWA MANUAL M23, CONCERNING HYDROSTATIC TESTING OF PVC PIPING. OFF-SITE UTILITIES HYDROSTATIC TESTING TO BE WITNESSED BY THE CITY OF Land City WATER DEPARTMENT INSPECTOR.

FDOT GENERAL NOTES

1. MAINTENANCE OF TRAFFIC TO BE SUPERVISED BY A CERTIFIED PERSON.
2. THE CONTRACTOR SHALL NOTIFY THE DEPARTMENT A MINIMUM OF TWO BUSINESS DAYS PRIOR TO ANY LANE CLOSURES OR BEGINNING ANY CONSTRUCTION WITHIN THE FDOT RIGHT-OF-WAY.
3. ALL WORK PERFORMED WITHIN THE FDOT RIGHT OF WAY SHALL BE IN ACCORDANCE WITH THE FY2021-22 OR CURRENT EDITION OF FDOT STANDARD PLANS.
4. IF THE DEPARTMENT DETERMINES THAT AS-BUILT CONDITIONS VARY SIGNIFICANTLY FROM THE APPROVED PLANS, THE PERMITTEE SHALL PROVIDE AS-BUILT PLANS, ALONG WITH A RECORD DRAWINGS REPORT BY PERMITTEE'S PROFESSIONAL ENGINEER, FORM 850-040-19, WITHIN 30 DAYS.
5. IT WILL BE THE RESPONSIBILITY OF THE PERMITTEE TO REPAIR ANY DAMAGE TO FDOT FACILITIES CAUSED BY CONSTRUCTION OF THE PROJECT.
6. TEST RESULTS OF ANY TESTS TAKEN FOR OR DURING CONSTRUCTION OF THE PERMITTED WORK SHALL BE PROVIDED TO THE FDOT UPON REQUEST.
7. ALL CONCRETE TO BE REMOVED SHALL BE SAW CUT AT THE NEAREST JOINT IN GOOD CONDITION, SO AS TO PRODUCE A CONNECTION WITH NEW CONCRETE THAT IS FREE OF CRACKS, DEFORMITY IN SHAPE, NOTICEABLE VOIDS, SURFACE IRREGULARITIES, AND OTHER DEFECTS.
8. ALL CONCRETE SHALL BE AN APPROVED FDOT MIX DESIGN OF 3,000 PSI MINIMUM.
9. ALL MATERIALS INSTALLED WITHIN FDOT RIGHT-OF-WAY SHALL BE LIMITED TO THOSE ON THE FDOT'S QUALIFIED PRODUCTS LIST OR APPROVED PRODUCT LIST OF TRAFFIC CONTROL SIGNALS AND DEVICES.
10. THE PERMITTEE SHALL CONTACT THE CITY OF LAKE CITY TRAFFIC DEPT. (386) 758-5400.
11. ALL CONSTRUCTION IN THE FDOT ROW SHALL CONFIRM TO THE LATEST EDITIONS OF THE FDOT DESIGN STANDARDS, THE FDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, AND THE FDOT UTILITY ACCOMMODATION MANUAL.
12. ALL DISTURBED AREAS IN FDOT ROW SHALL BE SODDED.
13. ALL WORK PERFORMED WITHIN THE FDOT RIGHT-OF-WAY SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE FDOT DESIGN STANDARDS, THE LATEST EDITION OF THE SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, AND THE 2017 UTILITY ACCOMMODATION MANUAL.
14. PLEASE NOTIFY JACKSONVILLE OPERATIONS TWO BUSINESS DAYS BEFORE BEGINNING WORK @ (904) 306-7500.

No.	REVISIONS	DATE	BY

Kimley-Horn
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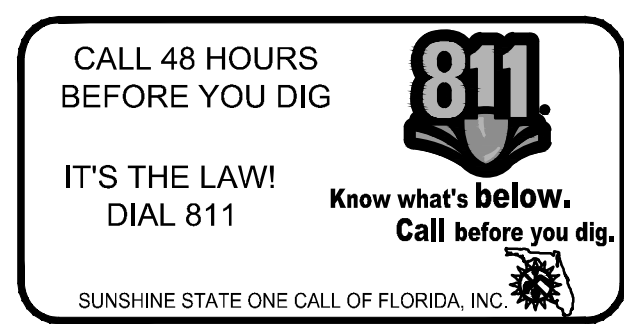
LICENSED PROFESSIONAL
 CIVIL ENGINEER
 STATE OF FLORIDA
 PROFESSIONAL NUMBER: 49083
 NAME: JIM TUMM

KHA PROJECT 149880040	DATE 05/04/2022	SCALE AS SHOWN	DESIGNED BY TJM	DRAWN BY TJM	CHECKED BY JCS	DATE 05/04/2022
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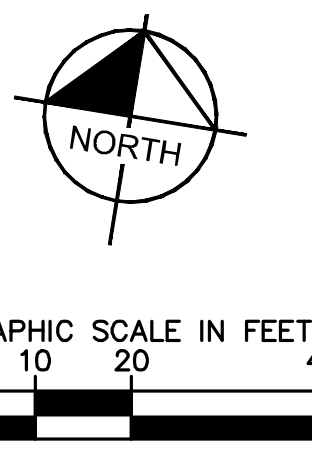
GENERAL NOTES

CIRCLE K - US HWY
 90 & I-75 FUEL
 EXPANSION
 CITY OF LAKE CITY FLORIDA

SHEET NUMBER
C1.1

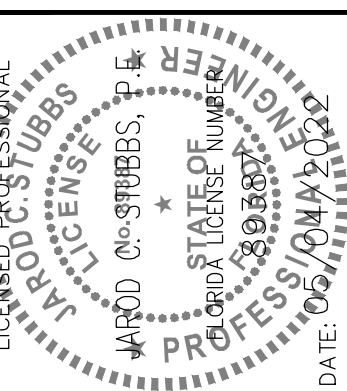


Plotted By: Parks, Etham - Sheet Set: CIRCLE K - US HWY 30 & I-75 High Speed Diesel Fuel Expansion - Layout: C3.0 EXISTING CONDITIONS & DEMOLITION PLAN - June 08, 2022 - 04:29:23pm - K:\ORL\Chil\149880040-Circle K USBD & I-75 CAD\CONSTR\PlanSheets\C3.0 - EXISTING CONDITIONS & DEMO PLAN.dwg
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- NOTES:**
- CONTRACTOR TO COORDINATE WITH UTILITY COMPANY FOR DEMOLITION REQUIREMENTS OF ELECTRICAL SYSTEMS.
 - CONTRACTOR SHALL UTILIZE BEST MANAGEMENT PRACTICES AS NEEDED TO PREVENT SYSTEM POLLUTION DURING TIME OF CONSTRUCTION.
 - REFER TO SWPPP SHEET C2.0 FOR ADDITIONAL NOTES AND DETAILS.
 - INSTALL AND MAINTAIN SILT FENCE AT LIMITS OF CONSTRUCTION. SEE DETAIL SHEET C7.0.
 - PROVIDE SOIL TRACKING PREVENTION DEVICE AT ALL CONSTRUCTION ACCESS POINTS. SEE DETAIL SHEET C7.1.
 - UTILIZE PERFORATED SOCK DRAIN (OR EQUIVALENT) IN FRONT OF EXISTING/PROPOSED CURB INLETS ADJACENT TO CONSTRUCTION ACTIVITIES. SEE DETAIL SHEET C7.1.
 - INSTALL AND MAINTAIN FILTER FABRIC UNDER GRATES OF EXISTING/PROPOSED INLETS. SEE SHEET C7.1.
 - CONTRACTOR TO ENSURE ADEQUATE COVER REMAINS OVER ALL EXISTING UTILITIES.
 - CONTRACTOR TO VERIFY EXISTING COVER OVER ALL UTILITIES BEFORE START OF CONSTRUCTION AND TO COORDINATE WITH THE ENGINEER OF RECORD PRIOR TO START OF CONSTRUCTION IF DESIGN DOES NOT PROVIDE 36" COVER.
 - CONTRACTOR IS TO VERIFY EXISTING SANITARY, STORM, WATER, ELECTRIC, PHONE, CABLE, AND NATURAL GAS SERVICES TO BUILDINGS SCHEDULED FOR DEMOLITION. SERVICES ARE TO BE ISOLATED FROM THE MAIN UTILITY SERVICE CONNECTIONS, AND CAPPED AND/OR REMOVED AS REQUIRED BY THE UTILITY PROVIDER. UTILITY SERVICES ARE TO BE ISOLATED IN A MANNER THAT WILL INSURE THAT ADJACENT PROPERTIES REMAIN CONNECTED WITHOUT EXPERIENCING AN INTERRUPTION OF SERVICE.
 - THERE MAY BE ON-SITE UNDERGROUND UTILITIES (INCLUDING BUT NOT LIMITED TO IRRIGATION, SANITARY SEWER, POTABLE WATER LINES, NATURAL GAS LINES, FIBER OPTIC, ELECTRIC, TELEPHONE AND CABLE LINES) THAT WERE NOT LOCATED OR IDENTIFIED BY THE PROJECT SURVEYOR. PRIOR TO CONSTRUCTION START, CONTRACTOR SHALL FIELD VERIFY ALL EXISTING ON-SITE UTILITIES.
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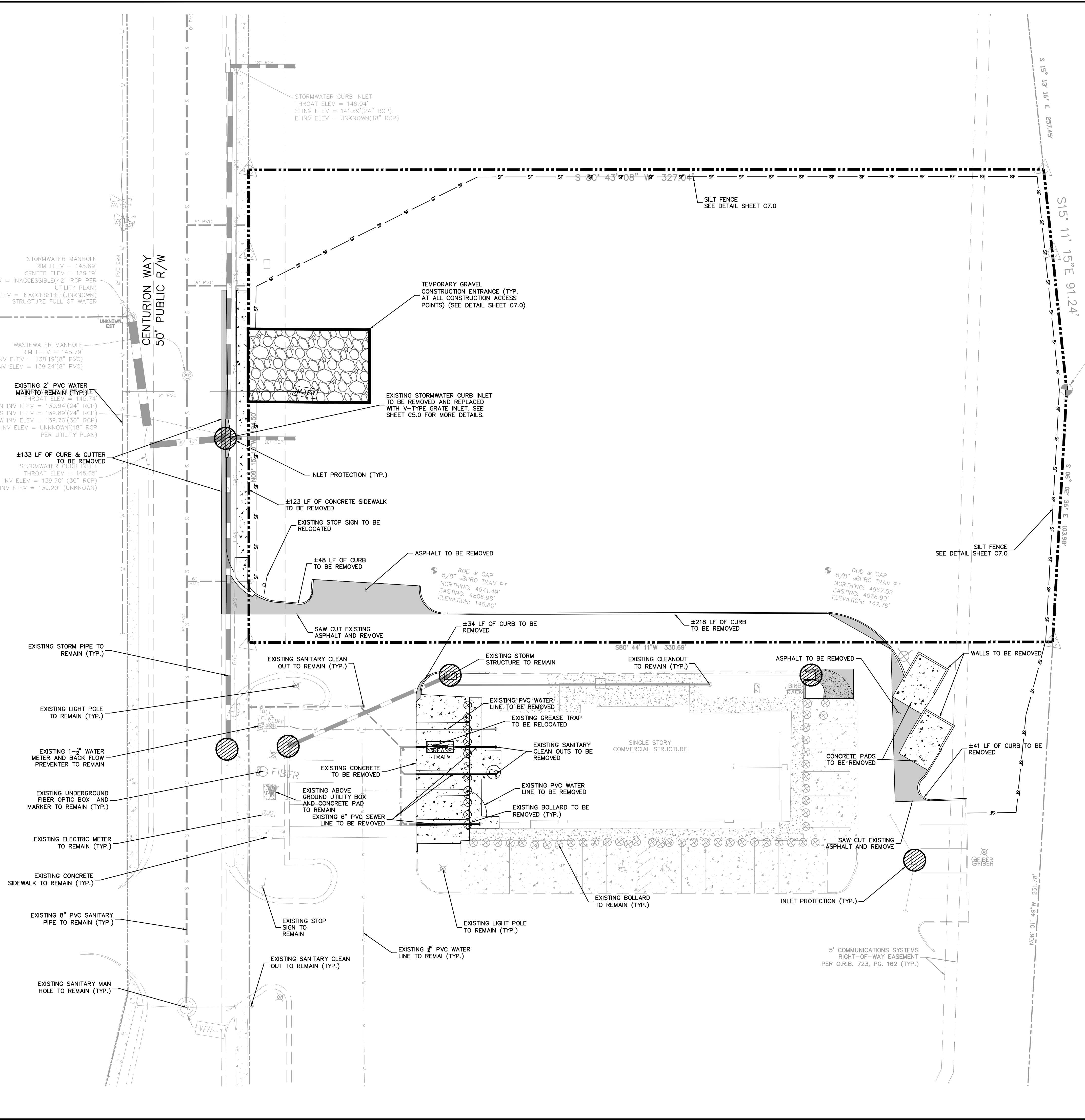
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KHA PROJECT
 149880040
 DATE
 05/04/2022
 SCALE AS SHOWN
 DESIGNED BY EJJ
 DRAWN BY EJJ
 CHECKED BY JCS
 DATE: 05/04/2022

EXISTING CONDITIONS & DEMOLITION PLAN

CIRCLE K - US HWY 30 & I-75 FUEL EXPANSION
 CITY OF LAKE CITY, FLORIDA
 SHEET NUMBER
C3.0

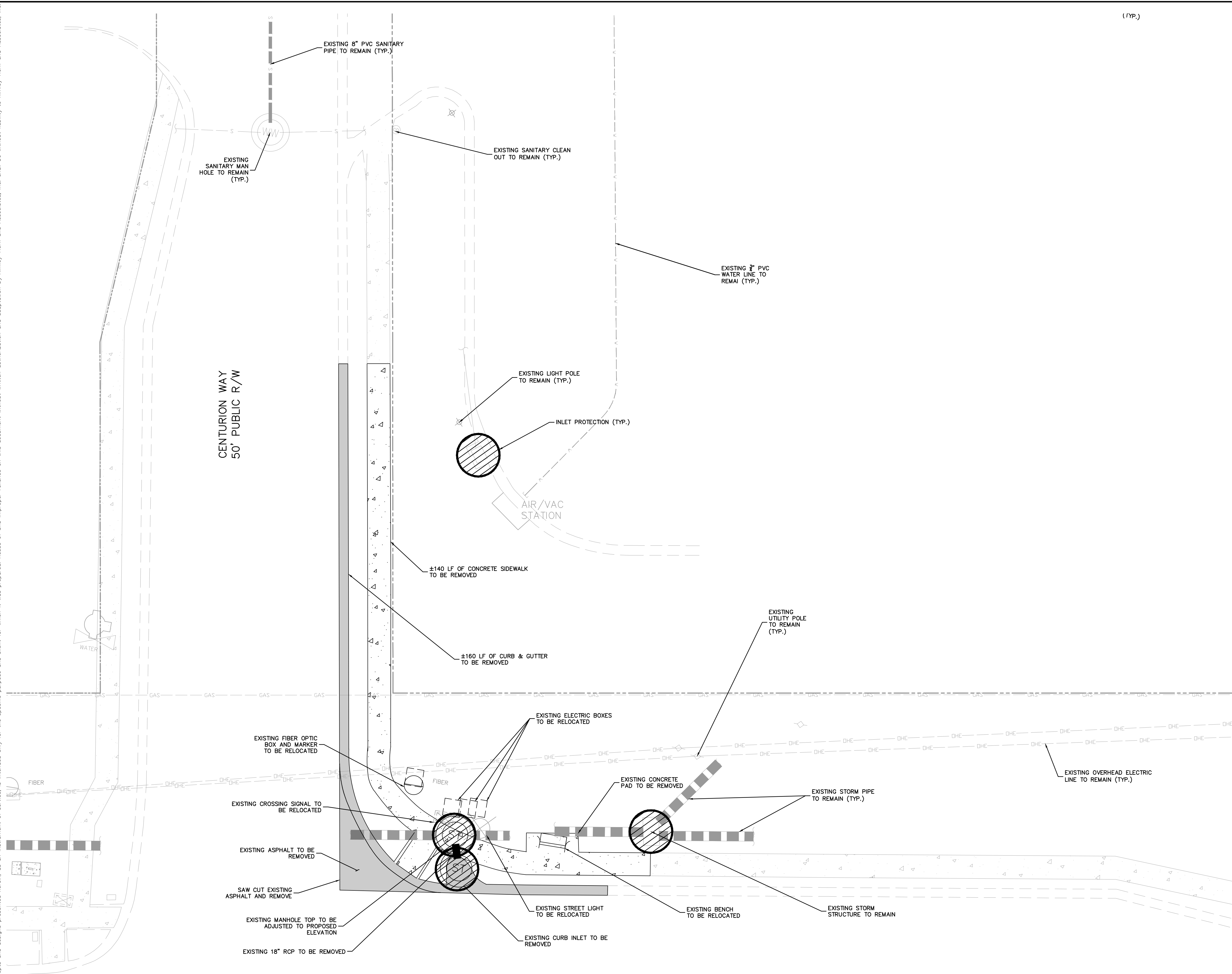


- LEGEND**
- PROPERTY LINE (TYP.)
 - INLET PROTECTION (SEE DETAIL SHEET C7.0)
 - Sf-Sf- TYPE III SILT FENCE (SEE DETAIL SHEET C7.0)
 - EXISTING TO BE DEMOLISHED
 - S-S- EXISTING SEWER MAIN
 - W-W- EXISTING WATER MAIN
 - S-S- EXISTING STORM PIPE
 - GAS-GAS- EXISTING GAS MAIN
 - ONE-ONE- EXISTING OVERHEAD ELECTRIC LINE
 - ⊗ EXISTING TREE TO REMAIN
 - ⊗ EXISTING TREE TO BE REMOVED
 - FW-FW- EXISTING FORCE MAIN

VERTICAL DATUM:
 ELEVATIONS ARE REFERENCED TO PLAT BENCHMARKS AS SHOWN ON THE FACE OF THE PLAT OF GATEWAY CROSSING REPLAT OF LOTS 2 AND 3, PLAT BOOK 9, PAGE 151

CALL 48 HOURS BEFORE YOU DIG
811
 IT'S THE LAW!
 DIAL 811
 Know what's below.
 Call before you dig.
 SUNSHINE STATE ONE CALL OF FLORIDA, INC.

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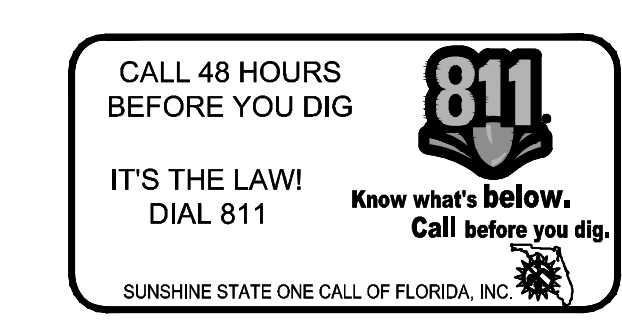


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LEGEND

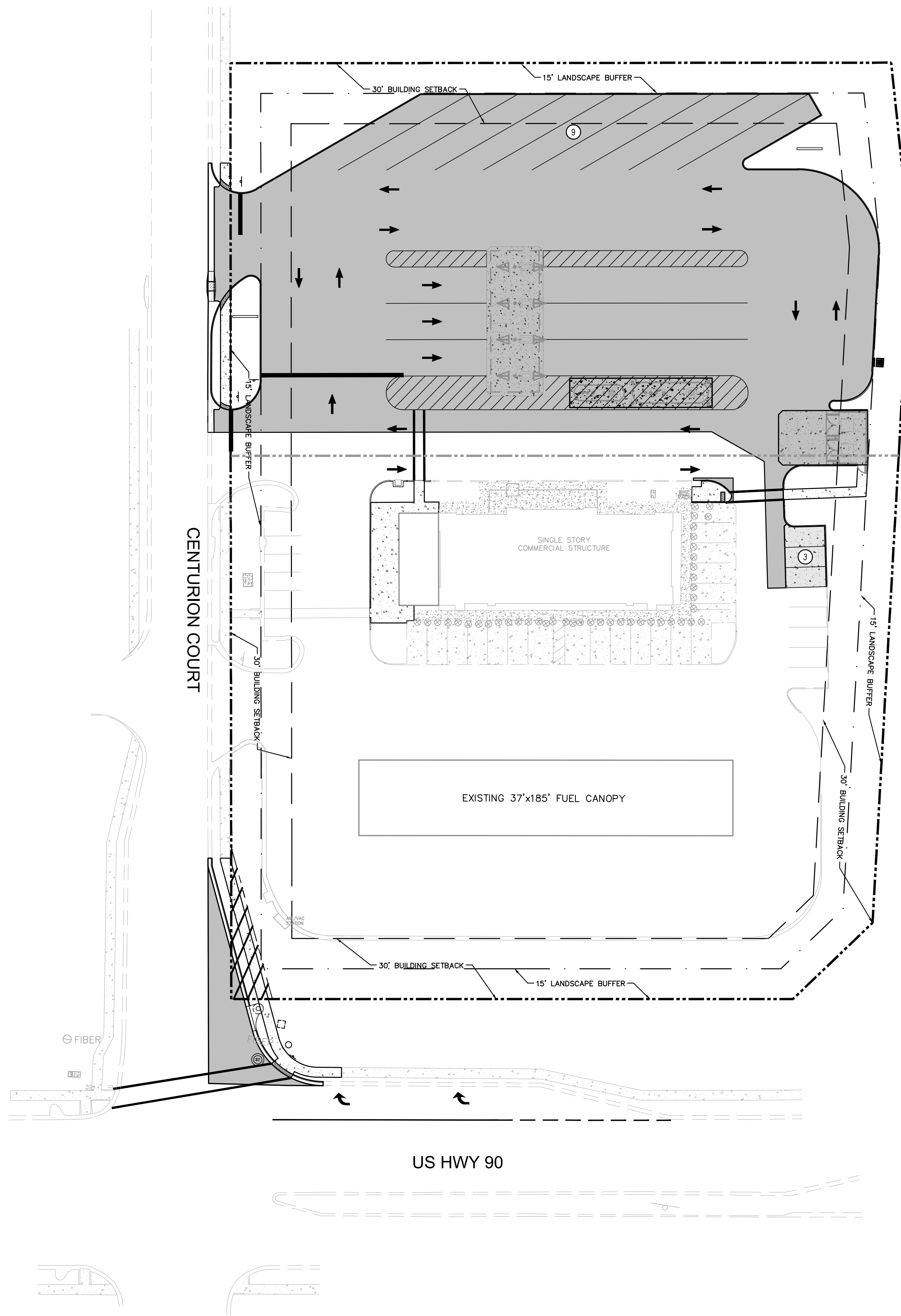
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	EXISTING GAS MAIN
	EXISTING OVERHEAD ELECTRIC LINE
	EXISTING TREE TO REMAIN
	EXISTING TREE TO BE REMOVED
	EXISTING FORCE MAIN

STATE ROAD 10
 U.S. HIGHWAY 90
 R/W VARIES



KHA PROJECT 149880040		DATE 05/04/2022		SCALE AS SHOWN		DRAWN BY EJF		CHECKED BY JCS	
EXISTING CONDITIONS & DEMOLITION PLAN		CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION		FLORIDA		CITY OF LAKE CITY		SHEET NUMBER C3.1	
KIMLEY-HORN		© 2022 KIMLEY-HORN AND ASSOCIATES, INC. 189 S. ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801 PHONE: 407-898-1511 WWW.KIMLEY-HORN.COM		REGISTRY NO. 35106		REVISIONS		DATE	

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NOTES:

1. ALL CURB DIMENSIONS ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
2. BUILDING DIMENSIONS ARE TO THE OUTSIDE FACE OF BUILDING UNLESS OTHERWISE NOTED.
3. REFER TO ARCHITECTURAL AND STRUCTURAL PLANS TO VERIFY ALL BUILDING DIMENSIONS.
4. REFER TO SIGNAGE PLANS FOR MONUMENT SIGN DETAILS.
5. SEE MEP PLANS FOR ELECTRICAL DRAWINGS.
6. ALL PROPOSED ON-SITE STRIPING AND PAVEMENT MARKING WILL BE PAINTED UNLESS OTHERWISE NOTED AND IN ACCORDANCE WITH FDOT INDEX 711-001.
7. REFER TO ARCHITECTURAL PLANS FOR PROPOSED TRASH CAN LOCATIONS AND DESIGN.
8. BOLLARDS IN SIDEWALK ADJACENT TO BUILDING SHALL BE COVERED WITH RED PLASTIC COVERS TO BE SUPPLIED BY CONTRACTOR.
9. BOLLARDS UNDER CANOPY SHALL BE COVERED WITH GRAY PLASTIC COVERS TO BE SUPPLIED BY CONTRACTOR (SEE FUEL PUMP DESIGNER PLANS FOR MORE DETAIL).
10. REFER TO ARCHITECTURAL PLANS FOR SITE LIGHTING AND ELECTRICAL PLANS.
11. ALL SIGNAGE AND PAVEMENT MARKINGS SHALL MEET MUTCD AND FDOT STANDARDS.
12. ALL SIGNAGE SHALL MEET THE REQUIREMENTS OF CITY OF LAKE CITY LAND DEVELOPMENT CODE, CHAPTER 7, SEC. 760

SITE DATA:

PROJECT AREA:	3.46± ACRES (150,953 SF)
FUTURE LAND USE:	COMMERCIAL
EXISTING ZONING:	CHI - COMMERCIAL, HIGHWAY INTERCHANGE
EXISTING USE:	UNDEVELOPED
PROPOSED USE:	COMMERCIAL
BUILDING HEIGHT:	1 STORY / <35 FT
FAR:	0.0388
EXISTING PAVEMENT AREA:	99,714.2 SF (2.29 AC) (66%)
PROPOSED PAVEMENT AREA:	34,981 SF (0.803 AC) (23%)
PROPOSED IMPERVIOUS AREA:	5,863 SF (0.135 AC) (3.88%)
(EXISTING BLDG+EXPANSION)	
ASPHALT/CONCRETE AREA:	106,401.5 SF (2.44 AC) (70.5%)
(EXISTING+HSD EXPANSION)	
TOTAL IMPERVIOUS AREA:	112,264.5 SF (2.58 AC) (65%)

PARKING REQUIRED

CONVENIENCE STORE (WITH GAS STATION)	
1 SPACES / 150 SF NON-STORAGE AREA (5,043 SF)	34
TOTAL REQUIRED PARKING	34

PARKING PROVIDED

PROPOSED HANDICAP SPACES:	2
PROPOSED REGULAR SPACES:	31
PROPOSED SEMI TRUCK SPACES:	9
PROPOSED ON-SITE SPACES:	42

BICYCLE PARKING

REQUIRED SPACES:	0
PROVIDED SPACES:	4

BUILDING SETBACKS

	REQUIRED	PROVIDED
SIDE (WEST):	30 FT	83 FT
REAR (NORTH):	30 FT	220 FT
FRONT (SOUTH):	30 FT	196 FT
SIDE (EAST):	30 FT	107 FT

LANDSCAPE SETBACKS

	REQUIRED	PROVIDED
SIDE (WEST):	15 FT	15 FT
REAR (NORTH):	15 FT	15 FT
FRONT (SOUTH):	N/A	0 FT
SIDE (EAST):	15 FT	15 FT

LEGEND

- PROPERTY LINE (TYP.)
- PROPOSED ASPHALT PAVEMENT (SEE DETAIL SHEET C7.0)
- PROPOSED CONCRETE SIDEWALK (SEE DETAIL SHEET C7.0)
- PROPOSED MEDIUM DUTY CONCRETE (SEE DETAIL SHEET C7.0)
- PROPOSED HEAVY DUTY CONCRETE (SEE DETAIL SHEET C7.0)

VERTICAL DATUM:

ELEVATIONS ARE REFERENCED TO PLAT BENCHMARKS AS SHOWN ON THE FACE OF THE PLAT OF GATEWAY CROSSING REPLAT OF LOTS 2 AND 3, PLAT BOOK 9, PAGE 151

CALL 48 HOURS BEFORE YOU DIG
811
 IT'S THE LAW!
 DIAL 811
 Know what's below.
 Call before you dig.
 SUNSHINE STATE ONE CALL OF FLORIDA, INC.

NO.	REVISIONS	DATE	BY

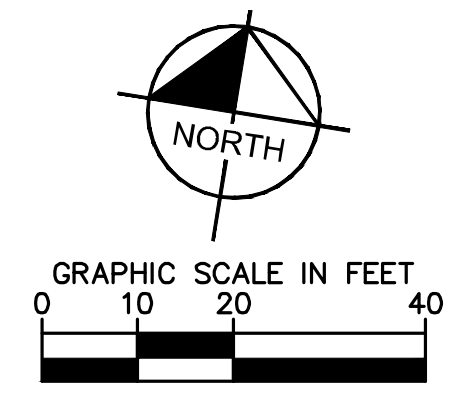
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 PHONE: 407-898-1511
 WWW.KIMLEY-HORN.COM REGISTRY No. 35106

LICENSED PROFESSIONAL ENGINEER
 STATE OF FLORIDA
 PROFESSIONAL ENGINEERING
 NO. 149880040
 DATE: 05/04/2022
 SCALE: AS SHOWN
 DESIGNED BY: EJJF
 DRAWN BY: EJJF
 CHECKED BY: JCS
 DATE: 06/08/2022

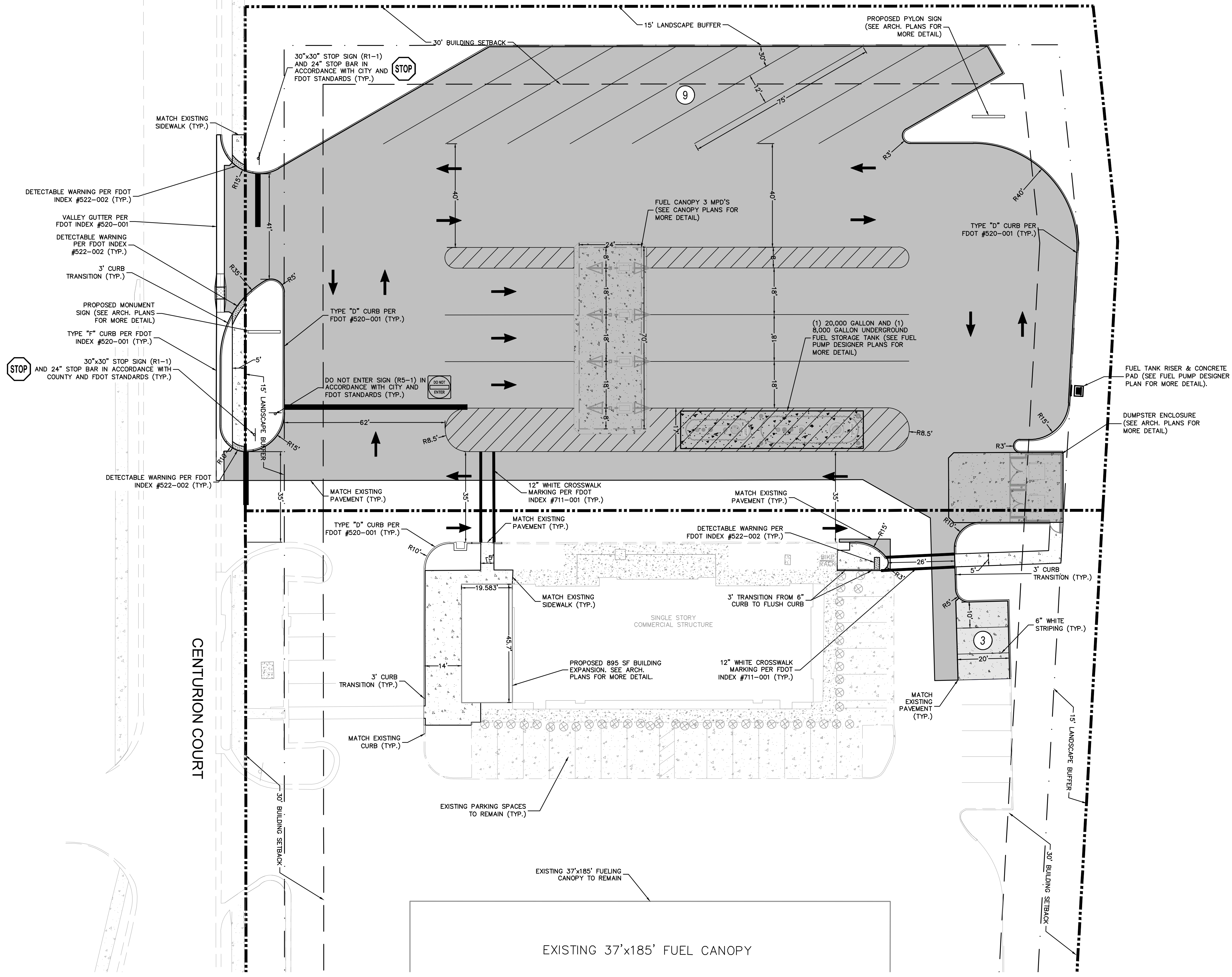
OVERALL SITE PLAN

CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION
 CITY OF LAKE CITY, FLORIDA
 SHEET NUMBER
C4.0

Plotted By: Parks, Etham - Sheet Set: CIRCLE K - US HWY 30 & I-75 High Speed Diesel Fuel Expansion - Layout: C4.1 - SITE PLAN - June 08, 2022 - 04:29:35pm - K:\ORL\CIVIL\49880040-Circle K US90 & I75 CARDACONST\PlanSheets\C4.0 - SITE PLAN.dwg
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- NOTES:**
1. ALL CURB DIMENSIONS ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
 2. BUILDING DIMENSIONS ARE TO THE OUTSIDE FACE OF BUILDING UNLESS OTHERWISE NOTED.
 3. REFER TO ARCHITECTURAL AND STRUCTURAL PLANS TO VERIFY ALL BUILDING DIMENSIONS.
 4. REFER TO SIGNAGE PLANS FOR MONUMENT SIGN DETAILS.
 5. SEE MEP PLANS FOR ELECTRICAL DRAWINGS.
 6. ALL PROPOSED ON-SITE STRIPING AND PAVEMENT MARKING WILL BE PAINTED UNLESS OTHERWISE NOTED AND IN ACCORDANCE WITH FDOT INDEX 711-001.
 7. REFER TO ARCHITECTURAL PLANS FOR PROPOSED TRASH CAN LOCATIONS AND DESIGN.
 8. BOLLARDS IN SIDEWALK ADJACENT TO BUILDING SHALL BE COVERED WITH RED PLASTIC COVERS TO BE SUPPLIED BY CONTRACTOR.
 9. BOLLARDS UNDER CANOPY SHALL BE COVERED WITH GRAY PLASTIC COVERS TO BE SUPPLIED BY CONTRACTOR (SEE FUEL PUMP DESIGNER PLANS FOR MORE DETAIL).
 10. REFER TO ARCHITECTURAL PLANS FOR SITE LIGHTING AND ELECTRICAL PLANS.
 11. ALL SIGNAGE AND PAVEMENT MARKINGS SHALL MEET MUTCD AND FDOT STANDARDS.
 12. ALL SIGNAGE SHALL MEET THE REQUIREMENTS OF CITY OF LAKE CITY LAND DEVELOPMENT CODE, CHAPTER 7, SEC. 760



INTERSTATE 75 OFF-RAMP

LEGEND

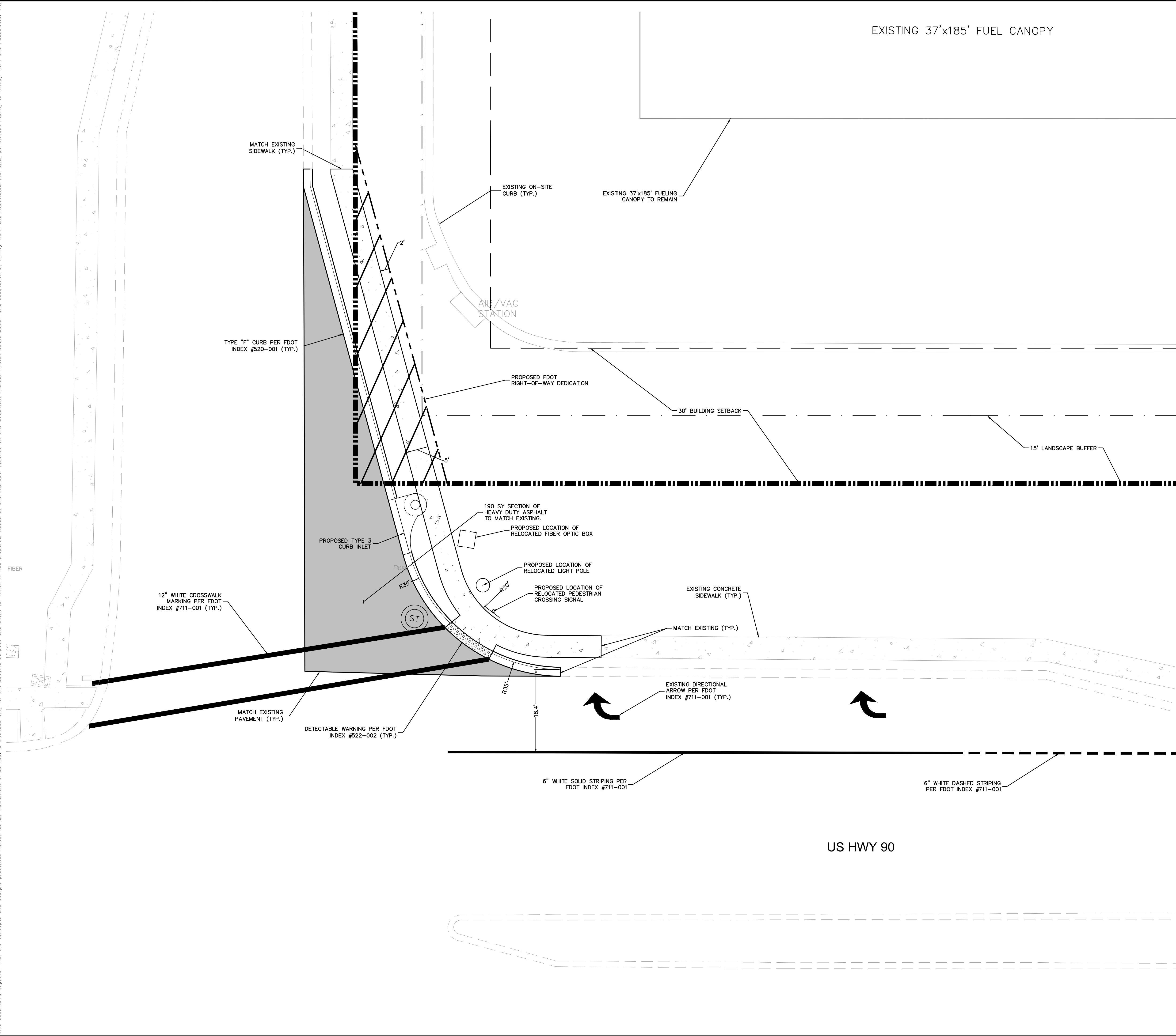
	PROPERTY LINE (TYP.)
	PROPOSED ASPHALT PAVEMENT (SEE DETAIL SHEET C7.0)
	PROPOSED CONCRETE SIDEWALK (SEE DETAIL SHEET C7.0)
	PROPOSED MEDIUM DUTY CONCRETE (SEE DETAIL SHEET C7.0)
	PROPOSED HEAVY DUTY CONCRETE (SEE DETAIL SHEET C7.0)

VERTICAL DATUM:
 ELEVATIONS ARE REFERENCED TO PLAT BENCHMARKS AS SHOWN ON THE FACE OF THE PLAT OF GATEWAY CROSSING REPLAT OF LOTS 2 AND 3, PLAT BOOK 9, PAGE 151

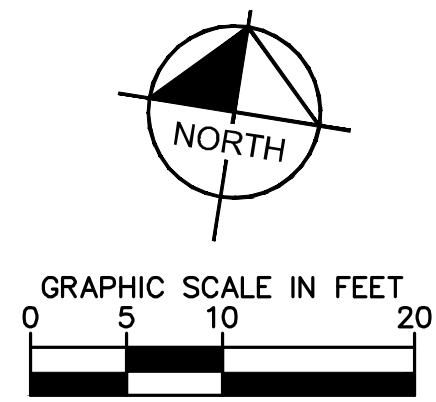
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 811
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 © 2022 KIMLEY-HORN AND ASSOCIATES, INC. 189 S. ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801 PHONE: 407-898-1511 WWW.KIMLEY-HORN.COM REGISTRY No. 35106		REVISIONS	DATE
		No.	
KHA PROJECT 149880040 DATE 05/04/2022 SCALE AS SHOWN DESIGNED BY E.J.F. DRAWN BY E.J.F. CHECKED BY JCS		SITE PLAN	
CIRCLE K - US HWY 30 & I-75 FUEL EXPANSION		FLORIDA CITY OF LAKE CITY	
SHEET NUMBER C4.1		20	

Plotted By: Parks, Ethan - Sheet Set: CIRCLE K - US HWY 30 & I-75 High-Speed Diesel Fuel Expansion - Layout: CA.2 INTERCHANGE MODIFICATIONS PLAN - June 08, 2022 - 04:28:37pm - K:\ORL_CIVIL\49880040-Circle K US90 & I75\CADD\CONSTR\PlanSheets\C4.0 - SITE PLAN.dwg
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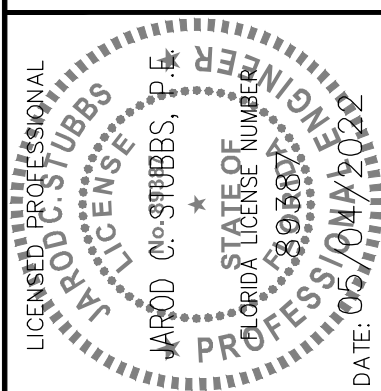
EXISTING 37'x185' FUEL CANOPY



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 2. BUILDING DIMENSIONS ARE TO THE OUTSIDE FACE OF BUILDING UNLESS OTHERWISE NOTED.
 3. ALL PROPOSED ON-SITE STRIPING AND PAVEMENT MARKING WILL BE PAINTED UNLESS OTHERWISE NOTED AND IN ACCORDANCE WITH FDOT INDEX 711-001.
 4. ALL SIGNAGE AND PAVEMENT MARKINGS SHALL MEET MUTCD AND FDOT STANDARDS.

No.	REVISIONS	DATE	BY

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DATE	05/04/2022
SCALE	AS SHOWN
DESIGNED BY	EJF
DRAWN BY	EJF
CHECKED BY	JCS

**INTERSECTION
 MODIFICATION PLAN**

**CIRCLE K - US HWY
 90 & I-75 FUEL
 EXPANSION**

CITY OF LAKE CITY FLORIDA

SHEET NUMBER
C4.2

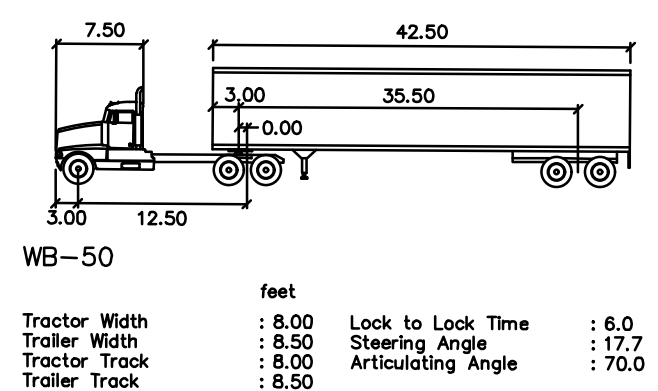
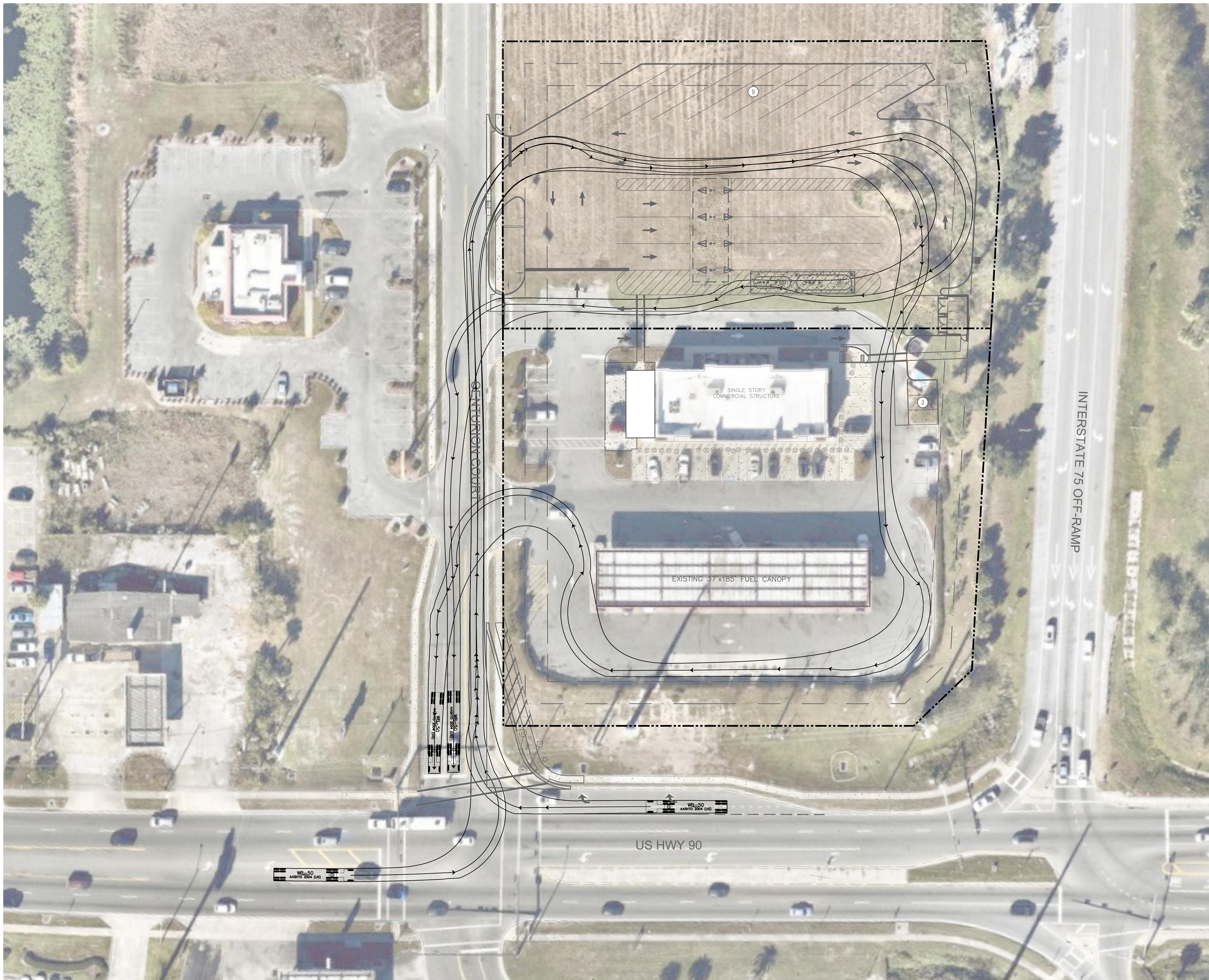
LEGEND

	PROPERTY LINE (TYP.)
	PROPOSED ASPHALT PAVEMENT (SEE DETAIL SHEET C7.0)
	PROPOSED CONCRETE SIDEWALK (SEE DETAIL SHEET C7.0)
	PROPOSED MEDIUM DUTY CONCRETE (SEE DETAIL SHEET C7.0)
	PROPOSED HEAVY DUTY CONCRETE (SEE DETAIL SHEET C7.0)

VERTICAL DATUM:
 ELEVATIONS ARE REFERENCED TO PLAT BENCHMARKS AS SHOWN ON THE FACE OF THE PLAT OF GATEWAY CROSSING REPLAT OF LOTS 2 AND 3, PLAT BOOK 9, PAGE 151

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Plotted By: Parks, Ethan - Sheet Set: CIRCLE K - US HWY 90 & I-75 High Speed Diesel Fuel Expansion - Layout: C4.3 FUEL TRUCK MOVEMENTS - June 08, 2022 04:29:40pm - K:\VDR\Civil\149880040-Circle K US90 & I75\CAD\CONST\PlanSheets\C4.0 - SITE PLAN.dwg
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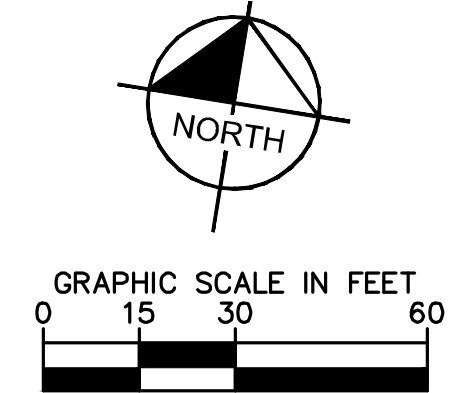
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CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION

TRUCK TURNING MOVEMENTS

CITY OF LAKE CITY FLORIDA

SHEET NUMBER
C4.3

Kimley»Horn

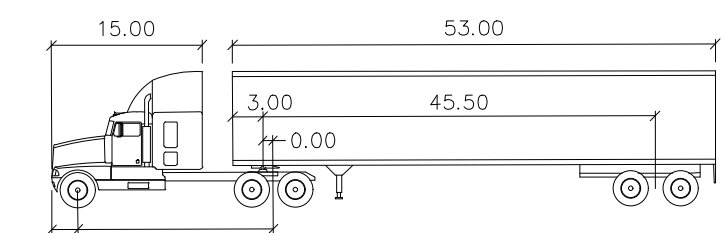
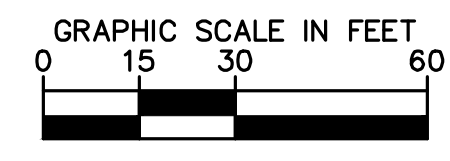
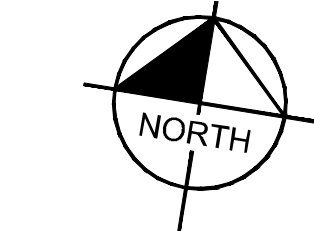
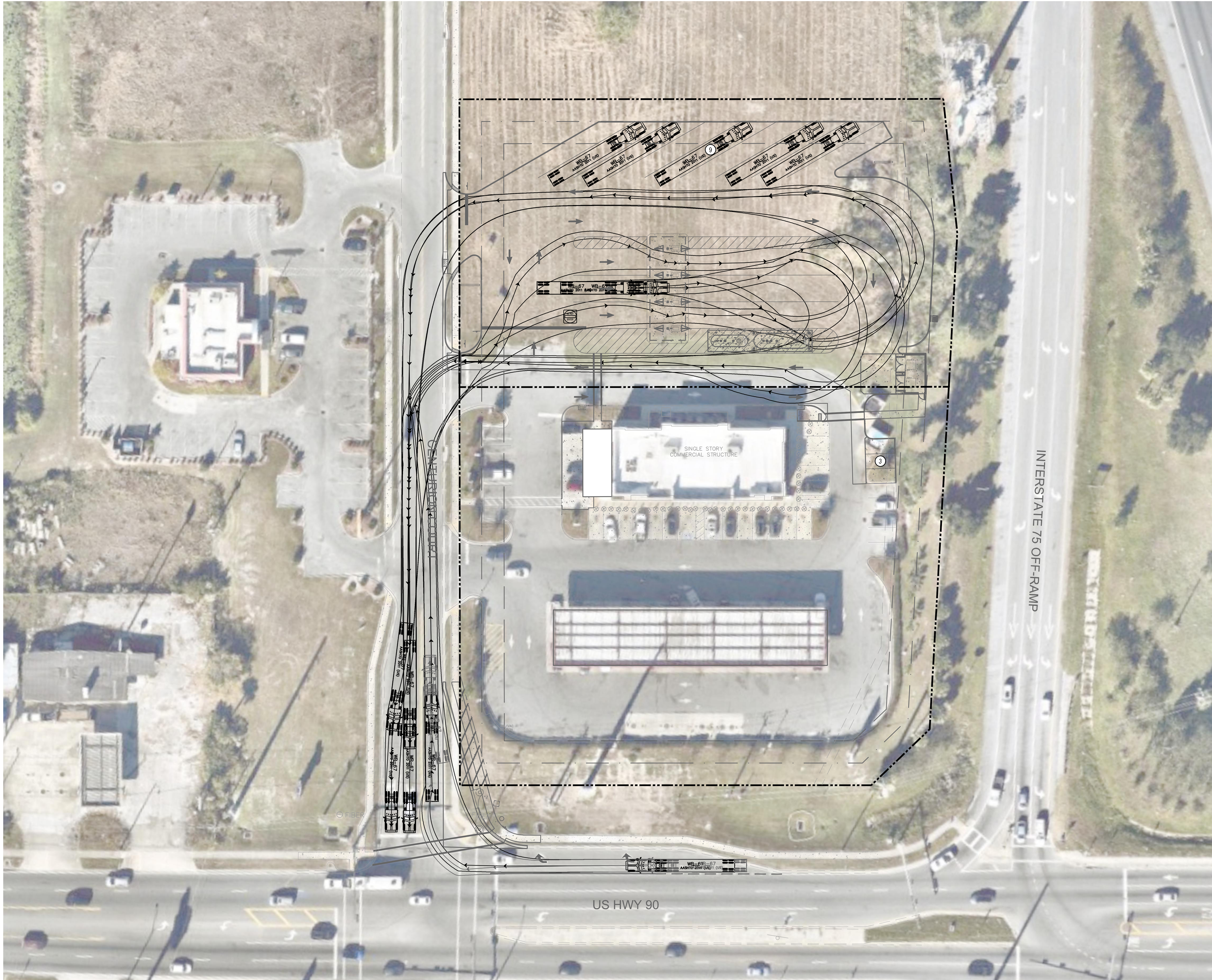
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 DATE 05/04/2022
 SCALE AS SHOWN
 DESIGNED BY E.J.F.
 DRAWN BY E.J.F.
 CHECKED BY JCS DATE 05/04/2022

LICENSED PROFESSIONAL ENGINEER
 STATE OF FLORIDA
 PROFESSIONAL ENGINEERING NO. 12998
 EXPIRES 12/31/2024

No.	REVISIONS	DATE	BY

Plotted By: Parks, Ethan - Sheet Set: CIRCLE K - US HWY 30 & I-75 High Speed Diesel Fuel Expansion - Layout: C4.4 FUELING TRUCK MOVEMENTS - June 08, 2022 04:29:45pm - K:\DR\Civil\149880040-Circle K US90 & I75\CADD\CONST\PlanSheets\C4.0 - SITE PLAN.dwg
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Tractor Width	: 4.00	Lock to Lock Time	: 6.0
Tractor Track	: 19.50	Steering Angle	: 28.4
Trailer Width	: 15.00	Articulating Angle	: 75.0
Trailer Track	: 53.00		

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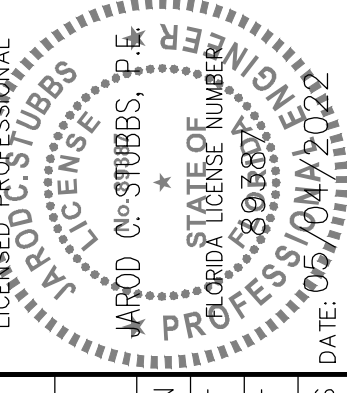
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KHA PROJECT	149880040
DATE	05/04/2022
SCALE	AS SHOWN
DESIGNED BY	EJF
DRAWN BY	EJF
CHECKED BY	JCS
JCS DATE:	06/08/2022

TRUCK TURNING MOVEMENTS

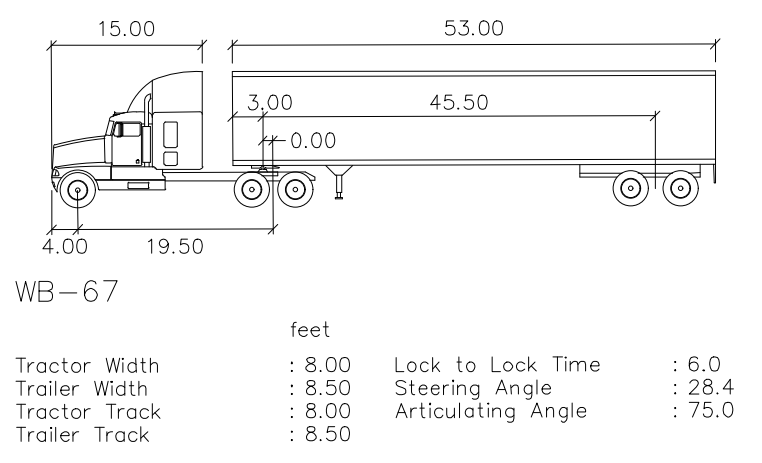
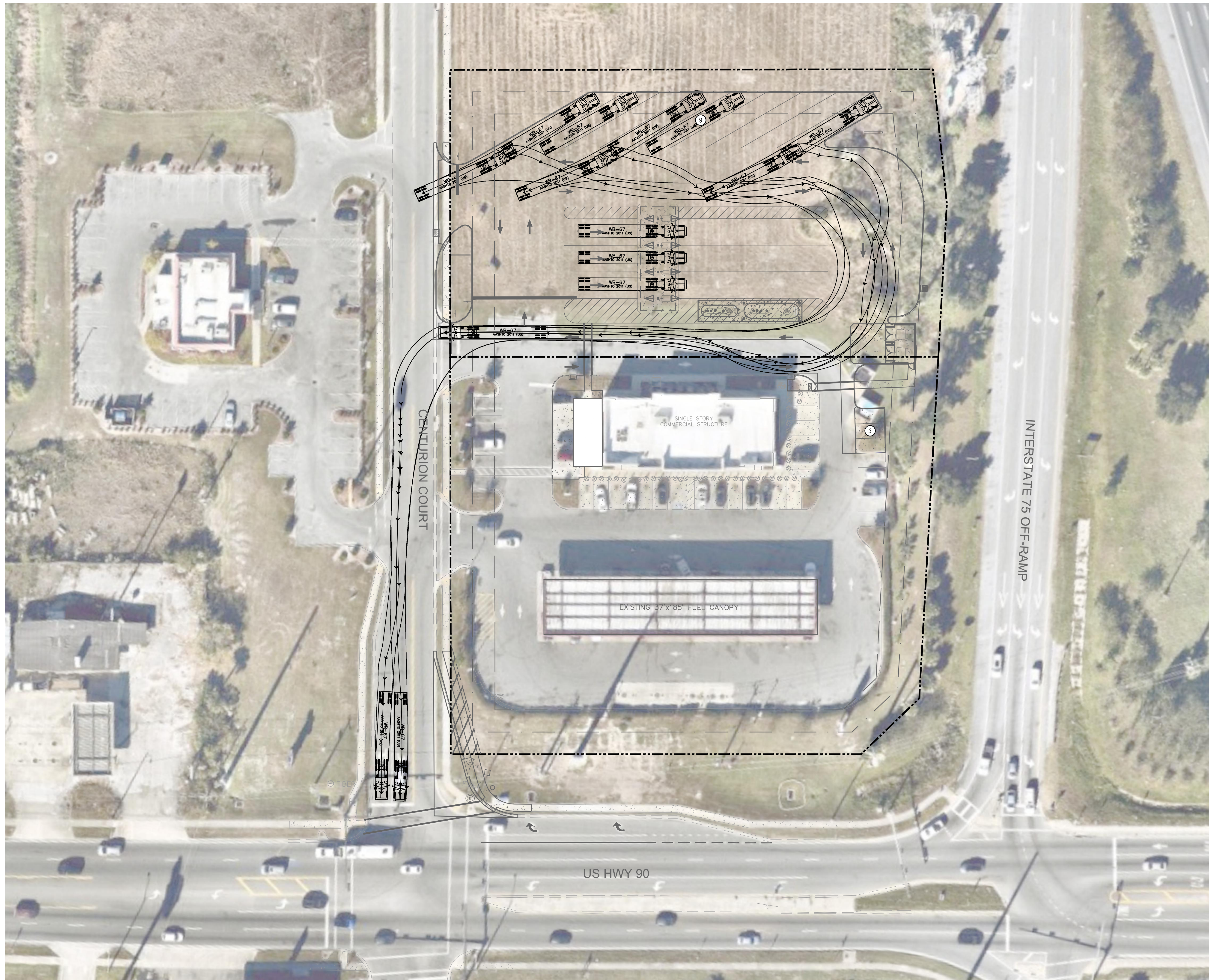
CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION

CITY OF LAKE CITY FLORIDA

SHEET NUMBER
C4.4

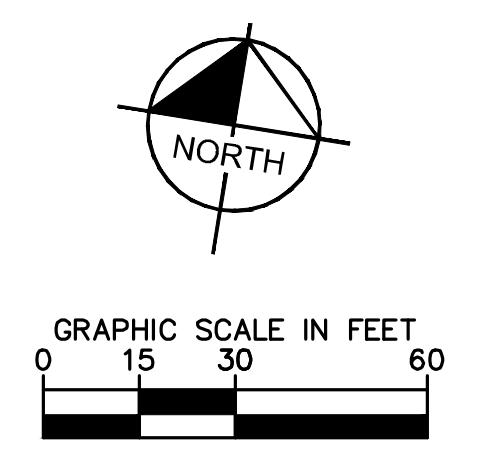
No.	REVISIONS	DATE	BY

Plotted By: Parks, Ethan - Sheet Set: CIRCLE K - US HWY 90 & I-75 High Speed Diesel Fuel Expansion - Layout: C4.5 - TRUCK MOVEMENTS - June 06, 2022 - 04:29:47pm - K:\ORL_Civil\149880040 - Circle K US90 & I75 CADD\CONSTR\PlanSheets\C4.0 - SITE PLAN.dwg
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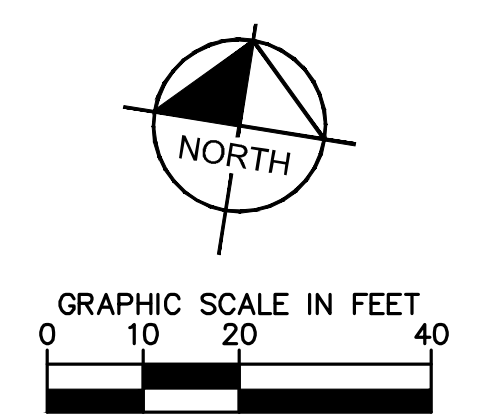
WB-67
 feet
 Tractor Width : 4.00
 Trailer Width : 19.50
 Tractor Track : 8.00
 Lock to Lock Time : 6.0
 Steering Angle : 28.4
 Articulating Angle : 75.0

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CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION CITY OF LAKE CITY FLORIDA	C4.5 - TRUCK TURNING MOVEMENTS		KHA PROJECT 149880040	DATE 05/04/2022	SCALE AS SHOWN	DESIGNED BY EJF	DRAWN BY EJF	CHECKED BY JCS	JCS DATE 05/06/2022
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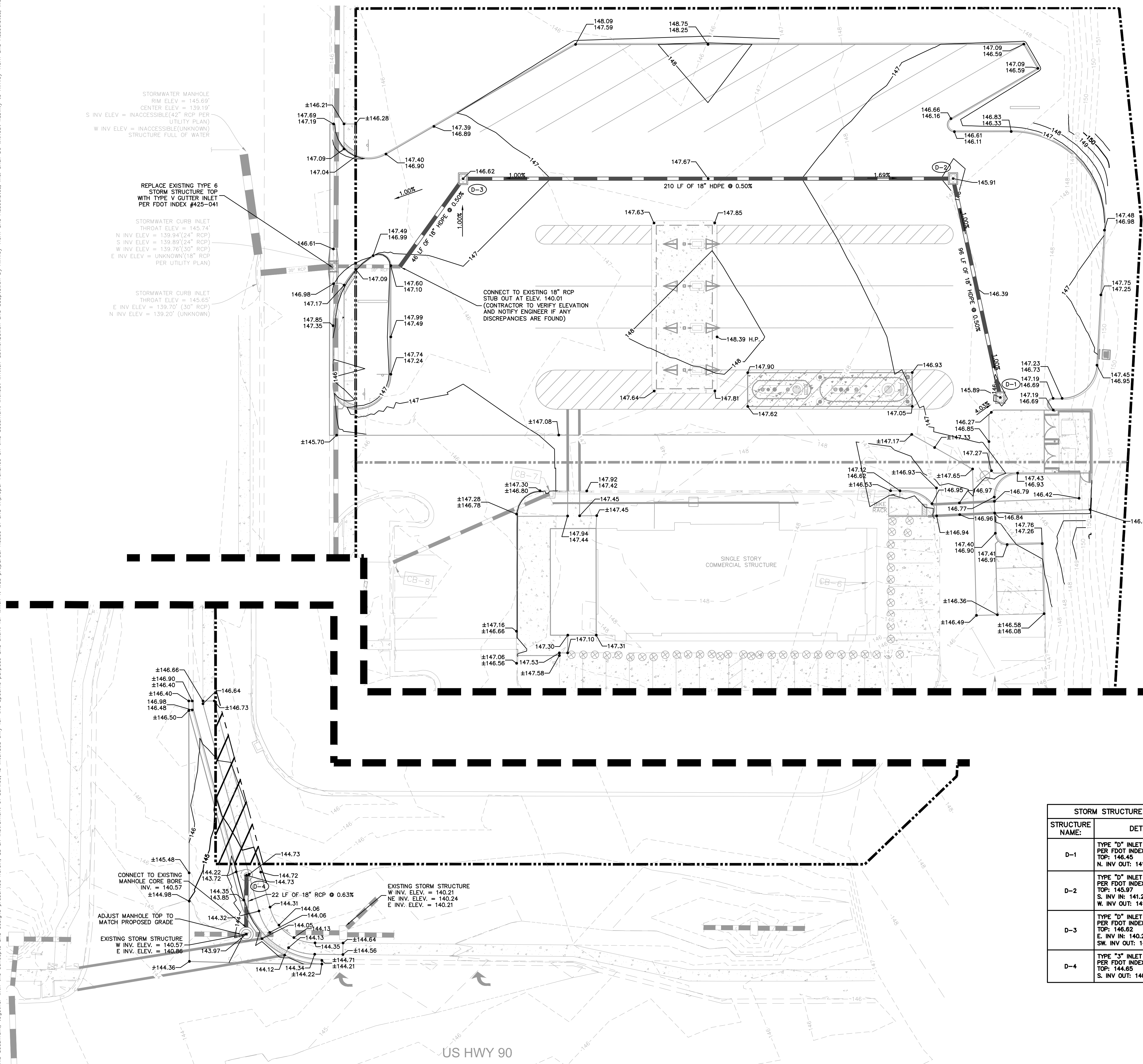
STORMWATER MANHOLE
 RIM ELEV = 145.69'
 CENTER ELEV = 139.19'
 S INV ELEV = INACCESSIBLE (42" RCP PER UTILITY PLAN)
 W INV ELEV = INACCESSIBLE (UNKNOWN) STRUCTURE FULL OF WATER

REPLACE EXISTING TYPE 6 STORM STRUCTURE TOP WITH TYPE W CUTTER INLET PER FDOT INDEX #425-041

STORMWATER CURB INLET
 THROAT ELEV = 145.74'
 N INV ELEV = 139.94 (24" RCP)
 S INV ELEV = 139.89 (24" RCP)
 W INV ELEV = 139.76 (30" RCP)
 E INV ELEV = UNKNOWN (18" RCP PER UTILITY PLAN)

STORMWATER CURB INLET
 THROAT ELEV = 145.65'
 E INV ELEV = 139.70 (30" RCP)
 N INV ELEV = 139.20 (UNKNOWN)

CONNECT TO EXISTING 18" RCP STUB OUT AT ELEV. 140.01 (CONTRACTOR TO VERIFY ELEVATION AND NOTIFY ENGINEER IF ANY DISCREPANCIES ARE FOUND)



NOTES:

- ALL STORM PIPES LABELED "HDPE" SHALL BE ADS HIGH PERFORMANCE (HP) POLYPROPYLENE STORM SEWER PIPE.
- CURB RAMPS ALONG PUBLIC STREETS AND IN THE PUBLIC RIGHT-OF-WAY SHALL BE CONSTRUCTED IN ACCORDANCE WITH FDOT STANDARDS AND SPECIFICATIONS.
- PRIVATE CURB RAMPS ON THE SITE (I.E. OUTSIDE PUBLIC STREET RIGHT-OF-WAY) SHALL CONFORM TO ADA STANDARDS AND SHALL HAVE A DETECTABLE WARNING SURFACE THAT IS FULL WIDTH AND FULL DEPTH OF THE CURB RAMP, NOT INCLUDING FLARES.
- ALL ACCESSIBLE ROUTES, GENERAL SITE AND BUILDING ELEMENTS, RAMPS, CURB RAMPS, STRIPING, AND PAVEMENT MARKINGS SHALL CONFORM TO ADA STANDARDS FOR ACCESSIBLE DESIGN, LATEST EDITION.
- BEFORE PLACING PAVEMENT, CONTRACTOR SHALL VERIFY THAT SUITABLE ACCESSIBLE PEDESTRIAN ROUTES (PER ADA AND FHA) EXIST TO AND FROM EVERY DOOR AND ALONG SIDEWALKS, ACCESSIBLE PARKING SPACES, ACCESSIBLE AISLES, AND ACCESSIBLE ROUTES. IN NO CASE SHALL AN ACCESSIBLE RAMP SLOPE EXCEED 1 VERTICAL TO 12 HORIZONTAL. IN NO CASE SHALL LONGITUDINAL CROSS SLOPE EXCEED 2.0 PERCENT. IN NO CASE SHALL LONGITUDINAL SIDEWALK SLOPE EXCEED 5.0 PERCENT. ACCESSIBLE PARKING SPACES AND ACCESS AISLES SHALL NOT EXCEED 2.0 PERCENT SLOPE IN ANY DIRECTION.
- CONTRACTOR SHALL TAKE FIELD SLOPE MEASUREMENTS ON FINISHED SUBGRADE AND FORM BOARDS PRIOR TO PLACING PAVEMENT TO VERIFY THAT ADA SLOPE REQUIREMENTS ARE PROVIDED. CONTRACTOR SHALL CONTACT ENGINEER PRIOR TO PAVING IF ANY EXCESSIVE SLOPES ARE ENCOUNTERED. NO CONTRACTOR CHANGE ORDERS WILL BE ACCEPTED FOR ADA SLOPE COMPLIANCE ISSUES.
- ALL PEDESTRIAN SIDEWALKS, PATHWAYS, AND CROSSWALKS SHALL BE CONSTRUCTED NOT TO EXCEED MAX. 2.0% CROSS SLOPE, MAX. 5.0% RUNNING SLOPE.
- ALL HANDICAP ACCESSIBLE PARKING SPACES SHALL BE CONSTRUCTED NOT TO EXCEED MAX. 2.0% CROSS SLOPE IN ALL DIRECTIONS.
- PROPOSED GRADES TO MATCH EXISTING ELEVATIONS AT PROPERTY LINE
- CONTRACTOR TO FIELD VERIFY ELEVATIONS AT ALL EXISTING SIDEWALK AND ROAD CONNECTION POINTS WITH ENGINEER OF RECORD BEFORE CONSTRUCTION OF ANY IMPROVEMENTS.
- FOR EROSION CONTROL NOTES REFER TO SHEET C2.0.
- ALL DRAINAGE PIPES SHALL BE FILTER FABRIC WRAPPED PER FDOT STANDARD PLAN #430-001.
- CONTRACTOR TO ENSURE ADEQUATE COVER REMAINS OVER ALL EXISTING UTILITIES.
- CONTRACTOR TO VERIFY EXISTING COVER OVER ALL UTILITIES BEFORE START OF CONSTRUCTION AND TO COORDINATE WITH THE ENGINEER OF RECORD PRIOR TO START OF CONSTRUCTION IF DESIGN DOES NOT PROVIDE 36" COVER.
- ALL EXISTING VALVES, BOXES, MANHOLE LIDS, COVERS, AND SIMILAR APPURTENANCES MUST BE ADJUSTED ACCORDINGLY TO MATCH FINISHED GRADE.
- ALL PAVEMENT MUST BE SOURCED FROM AN FDOT APPROVED PLANT.

STORM STRUCTURE TABLE	
STRUCTURE NAME:	DETAILS:
D-1	TYPE "D" INLET PER FDOT INDEX #425-052 TOP: 146.45 N. INV. OUT: 141.77 (18" HDPE)
D-2	TYPE "D" INLET PER FDOT INDEX #425-052 TOP: 145.97 S. INV. IN: 141.29 (18" HDPE) W. INV. OUT: 141.29 (18" HDPE)
D-3	TYPE "D" INLET PER FDOT INDEX #425-052 TOP: 146.62 E. INV. IN: 140.24 (18" HDPE) SW. INV. OUT: 140.24 (18" HDPE)
D-4	TYPE "3" INLET PER FDOT INDEX #425-020 TOP: 144.65 S. INV. OUT: 140.71 (18" RCP)

LEGEND

- PROPERTY LINE
- PROPOSED STORM PIPE
- ⊙ PROPOSED STORM MANHOLE
- ⊠ PROPOSED STORM INLET
- XX.XX PROPOSED ELEV. TOP OF CURB
- XX.XX PROPOSED ELEV. BOT. OF CURB
- XX.XX PROPOSED SPOT ELEVATION
- EXISTING STORM PIPE
- ⊙ EXISTING STORM MANHOLE
- ⊠ EXISTING STORM INLET

VERTICAL DATUM:
 ELEVATIONS ARE REFERENCED TO PLAT BENCHMARKS AS SHOWN ON THE FACE OF THE PLAT OF GATEWAY CROSSING REPLAT OF LOTS 2 AND 3, PLAT BOOK 9, PAGE 151

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No.	REVISIONS	DATE	BY

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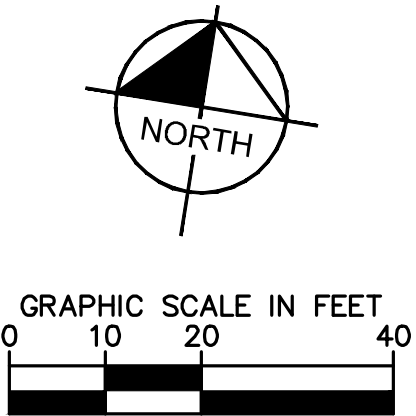
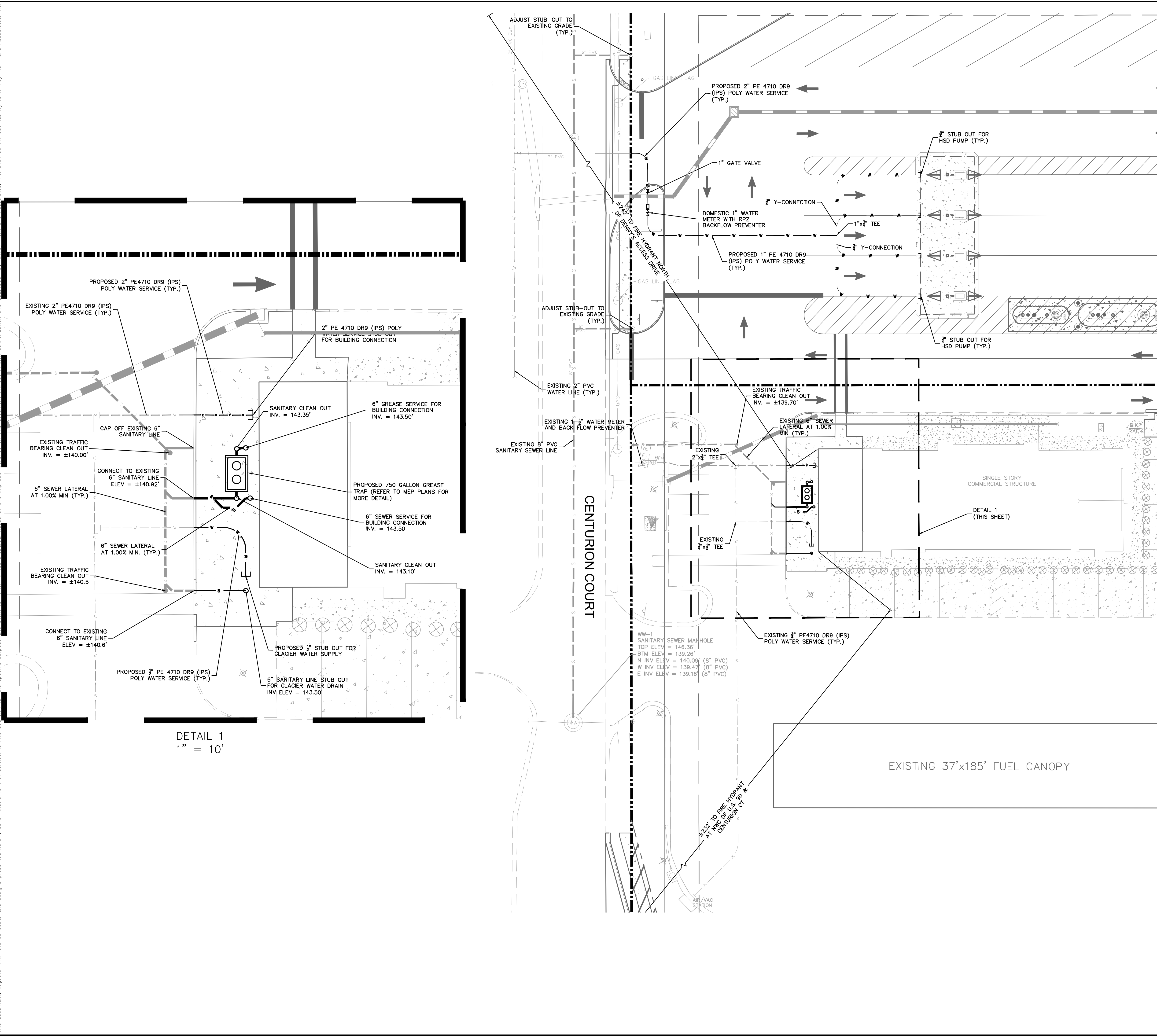
LICENSED PROFESSIONAL ENGINEER
 STATE OF FLORIDA
 No. 149880040
 DATE 05/04/2022
 SCALE AS SHOWN
 DESIGNED BY E.J.F.
 DRAWN BY E.J.F.
 CHECKED BY J.C.S.
 PROFESSIONAL SEAL

KHA PROJECT 149880040
 DATE 05/04/2022
 SCALE AS SHOWN
 DESIGNED BY E.J.F.
 DRAWN BY E.J.F.
 CHECKED BY J.C.S.

PAVING, GRADING AND DRAINAGE PLAN

CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION
 CITY OF LAKE CITY, FLORIDA
 SHEET NUMBER C5.0

Plotted By: Parks, Etham - Sheet Set: CIRCLE K - US HWY 30 & I-75 High-Speed Diesel Fuel Expansion - Layout: CEG UTILITY PLAN - June 08, 2022 - 04:30:24pm - K:\ORL_CAD\149880040-Circle K USRD & 75VACADD\CONSTR\PlanSheets\C6.0 - UTILITIES PLAN.dwg
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- NOTES:**
- ALL ONSITE UTILITIES SHALL BE PRIVATELY OWNED AND MAINTAINED.
 - MAINTAIN A MINIMUM OF 3-FT OF COVER OVER ALL PROPOSED WATER LINES AND 4-FT MINIMUM OF COVER FOR WASTEWATER LINES.
 - REFER TO ADDITIONAL UTILITY NOTES AND DETAILS ON SHEETS C1.0 AND C7.0.
 - ALL WATER MAINS, AND DOMESTIC LINES SHALL BE CONSTRUCTED IN ACCORDANCE WITH CITY OF LAKE CITY UTILITY STANDARDS.
 - BEDDING AND BACKFILL SHALL BE INSTALLED PER CITY OF LAKE CITY. GRANULAR BACKFILL IS REQUIRED UNDER PAVEMENT AND WITHIN 5 FEET OF PAVEMENT.
 - ALL SANITARY SEWER LINES SHALL BE GREEN C-900 PVC MEETING, ASTM D-3034 SDR 26.
 - CONTRACTOR SHALL COORDINATE ANY DISRUPTIONS TO EXISTING UTILITY SERVICES WITH ADJACENT PROPERTY OWNERS.
 - ALL ELECTRIC AND TELEPHONE EXTENSIONS INCLUDING SERVICE LINES SHALL BE CONSTRUCTED TO THE APPROPRIATE UTILITY COMPANY SPECIFICATIONS. ALL UTILITY DISCONNECTIONS SHALL BE COORDINATED WITH THE DESIGNATED UTILITY COMPANIES.
 - CONTRACTOR TO CALL "SUNSHINE STATE ONE CALL OF FLORIDA" (1-800-432-4770) TO COORDINATE FIELD LOCATIONS OF EXISTING UNDERGROUND UTILITIES BEFORE ORDERING MATERIALS OR COMMENCING CONSTRUCTION. NOTIFY ENGINEER OF ANY DISCREPANCIES IMMEDIATELY.
 - PRIOR TO THE CONSTRUCTION OF OR CONNECTION TO ANY STORM DRAIN, SANITARY SEWER, WATER MAIN OR ANY OTHER UTILITIES, THE CONTRACTOR SHALL EXCAVATE, VERIFY AND CALCULATE ALL POINTS OF CONNECTION AND ALL UTILITY CROSSINGS AND INFORM THE ENGINEER AND THE OWNER/ DEVELOPER OF ANY CONFLICT OR REQUIRED DEVIATIONS FROM THE PLAN. NOTIFICATION SHALL BE MADE A MINIMUM OF 72 HOURS PRIOR TO CONSTRUCTION. THE ENGINEER AND ITS CLIENTS SHALL BE HELD HARMLESS IN THE EVENT THAT THE CONTRACTOR FAILS TO MAKE SUCH NOTIFICATION. CITY OF LAKE CITY SHALL BE NOTIFIED OF ANY AND ALL CHANGES TO THE DESIGN PLANS.
 - CONTRACTOR SHALL COMPLY COMPLETELY WITH THE LATEST STANDARDS OF OSHA DIRECTIVES OR ANY OTHER AGENCY HAVING JURISDICTION FOR EXCAVATION AND TRENCHING PROCEDURES. THE CONTRACTOR SHALL USE SUPPORT SYSTEMS, SLOPING, BENCHING AND OTHER MEANS OF PROTECTION. THIS IS TO INCLUDE, BUT NOT LIMITED TO ACCESS AND EGRESS FROM ALL EXCAVATION AND TRENCHING. CONTRACTOR IS RESPONSIBLE FOR COMPLYING WITH PERFORMANCE CRITERIA AS REQUIRED BY OSHA.
 - CONTRACTOR TO AVOID DISRUPTION OF ANY ADJACENT TENANT'S TRAFFIC OPERATIONS, TO THE MAXIMUM EXTENT, DURING INSTALLATION OF UTILITIES. IF TRAFFIC IS TO BE OBSTRUCTED CONTRACTOR SHALL COORDINATE TRAFFIC CONTROL PLAN WITH THE LOCAL MUNICIPALITY.
 - ALL DIMENSIONS ARE TO CENTERLINE OF PIPE OR CENTER OF MANHOLE UNLESS NOTED OTHERWISE.
 - SEE PLUMBING PLANS FOR EXACT UTILITY CONNECTION LOCATIONS AT BUILDING.
 - LIGHT POLES SHOWN FOR COORDINATION PURPOSES ONLY AND DO NOT REPRESENT ACTUAL SIZE. SEE SITE LIGHTING PLANS BY OTHERS FOR MORE INFORMATION.
 - ELECTRIC, TELEPHONE, AND OTHER DRY UTILITIES SHALL BE PLACED WITHIN CONDUIT, MEETING PRIVATE UTILITY STANDARDS, WHEN ROUTE CROSSES PAVED DRIVES AND PARKING AREAS.
 - ALL UNDERGROUND WATER MAINS AND HYDRANTS SHALL BE INSTALLED, COMPLETED AND IN SERVICE PRIOR TO ANY COMBUSTIBLES BEING BROUGHT ONSITE.
 - CONTRACTOR TO ENSURE ADEQUATE COVER REMAINS OVER ALL EXISTING UTILITIES.
 - CONTRACTOR TO VERIFY EXISTING COVER OVER ALL UTILITIES BEFORE START OF CONSTRUCTION AND TO COORDINATE WITH THE ENGINEER OF RECORD PRIOR TO START OF CONSTRUCTION IF DESIGN DOES NOT PROVIDE 36" COVER.
 - ALL ELECTRIC, CABLE, AND TELECOMMUNICATION UTILITIES FOR BUILDING SERVICE TO BE INSTALLED UNDERGROUND.

LEGEND

	PROPERTY BOUNDARY
	PROPOSED STORM PIPE
	PROPOSED SANITARY SEWER LINE
	EXISTING WATER LINE
	EXISTING SANITARY SEWER LINE
	EXISTING WATER LINE
	EXISTING STORM PIPE
	EXISTING GAS LINE
	EXISTING OVERHEAD ELECTRIC LINE
	EXISTING FORCE MAIN
	EXISTING STORM MANHOLE
	EXISTING SANITARY SEWER MANHOLE
	PROPOSED FIRE HYDRANT

VERTICAL DATUM:
 ELEVATIONS ARE REFERENCED TO PLAT BENCHMARKS AS SHOWN ON THE FACE OF THE PLAT OF GATEWAY CROSSING REPLAT OF LOTS 2 AND 3, PLAT BOOK 9, PAGE 151

CALL 48 HOURS BEFORE YOU DIG

811

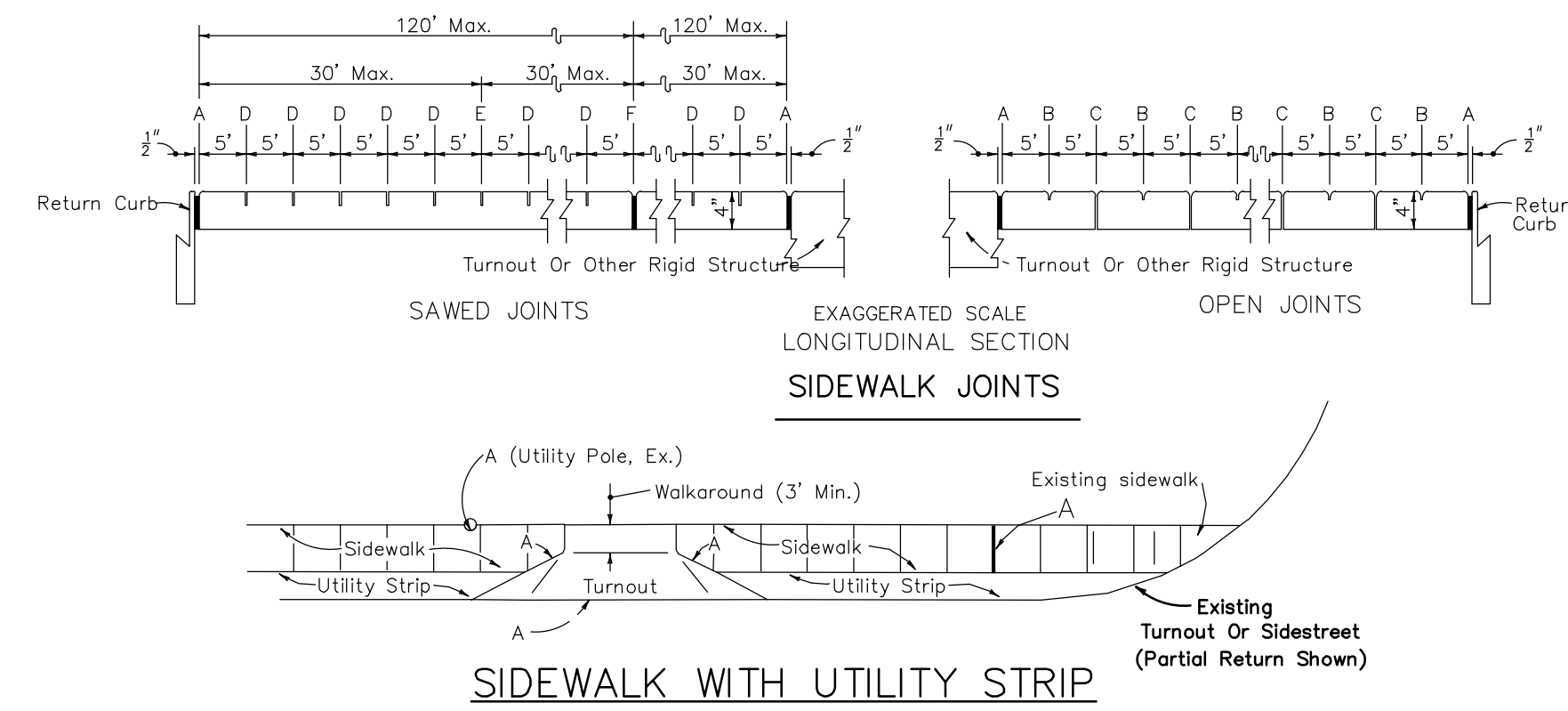
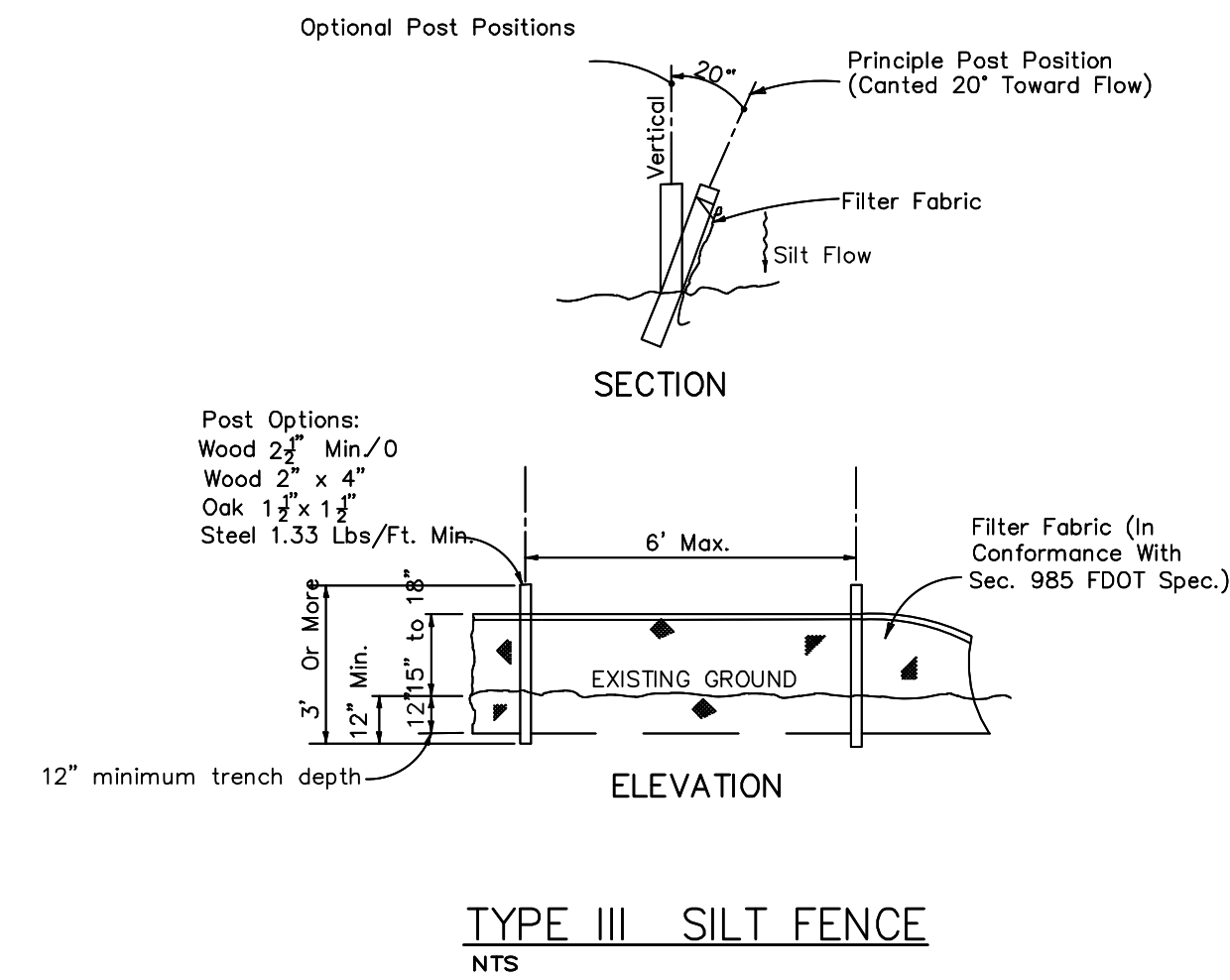
IT'S THE LAW!
 DIAL 811

Know what's below.
 Call before you dig.

SUNSHINE STATE ONE CALL OF FLORIDA, INC.

<h1 style="margin: 0;">UTILITY PLAN</h1>	<p>FLORIDA</p> <p>CITY OF LAKE CITY</p>						
<p>CIRCLE K - US HWY 30 & I-75 FUEL EXPANSION</p>	<p>SHEET NUMBER</p> <p>C6.0</p>						
<p>149880040</p> <p>DATE: 05/04/2022</p> <p>SCALE: AS SHOWN</p> <p>DESIGNED BY: EJJ</p> <p>DRAWN BY: EJJ</p> <p>CHECKED BY: JCS</p>	<p>PROJECT NO. 149880040</p> <p>DATE: 05/04/2022</p> <p>SCALE: AS SHOWN</p> <p>DESIGNED BY: EJJ</p> <p>DRAWN BY: EJJ</p> <p>CHECKED BY: JCS</p>						
<p>Kimley»Horn</p> <p>© 2022 KIMLEY-HORN AND ASSOCIATES, INC. 189 S. ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801 PHONE: 407-898-1511 WWW.KIMLEY-HORN.COM REGISTRY NO. 35106</p>	<p>REVISIONS</p> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>No.</th> <th>DATE</th> <th>BY</th> </tr> </thead> <tbody> <tr> <td> </td> <td> </td> <td> </td> </tr> </tbody> </table>	No.	DATE	BY			
No.	DATE	BY					

Plotted By: Parks, Ethom - Sheet Set: CIRCLE K - US HWY 30 & I-75 High Speed Diesel Fuel Expansion - Layout: C7.0 GENERAL CONSTRUCTION DETAILS - June 08, 2022 - 04:30:35pm - K:\VORL-Civil\148880040-Circle K US30 & I75\CAADD\CONST\PlanSheets\C7.0 - DETAILS.dwg
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JOINT LEGEND

- A - 1/2" EXPANSION JOINTS (PERFORMED JOINT FILLER)
- B - 1/2" DUMMY JOINTS, TOOLED
- C - 1/2" FORMED OPEN JOINTS
- D - 1/8" SAW CUT JOINTS, 1/8" DEEP (96 HOUR) MAX. 5' CENTERS
- E - 1/8" SAW CUT JOINTS, 1/8" DEEP (12 HOUR) MAX. 30' CENTERS
- F - 1/8" EXPANSION JOINT WHEN RUN OF SIDEWALK EXCEEDS 120'
- G - COLD JOINT WITH BOND BREAKER, TOOLED

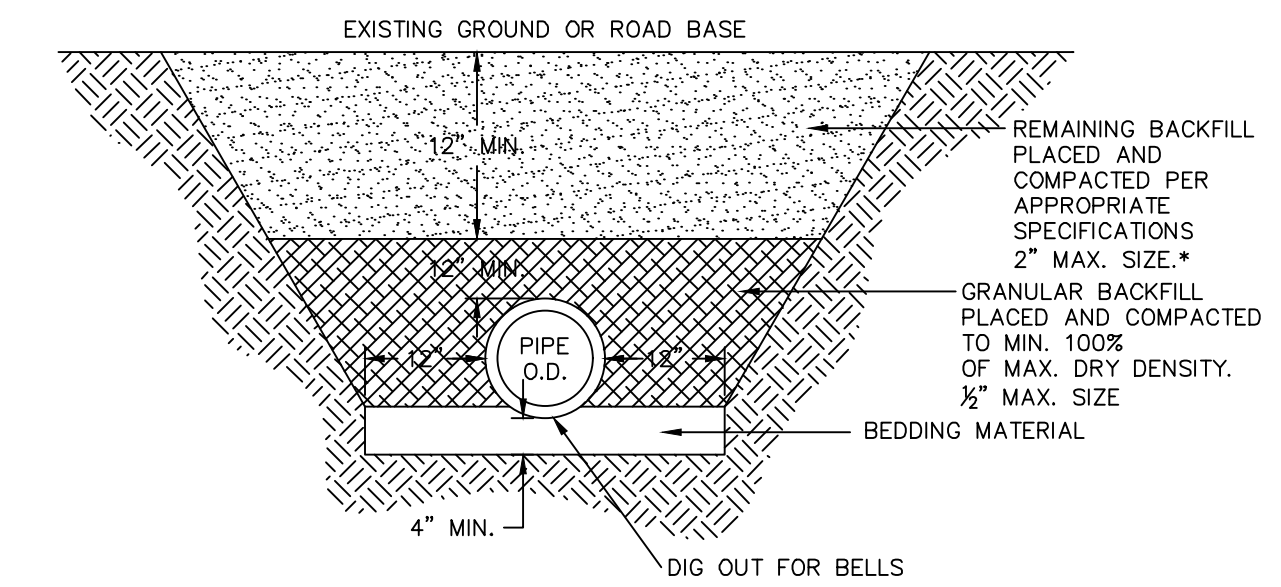
FOOTNOTES:

- ALL 1/2" JOINTS SHALL BE CONSTRUCTED WITH PREFORMED JOINT FILLER.
- * 1/8" OPEN JOINTS PLACED AT EQUAL (20' MAX.) INTERVALS FOR DRIVEWAYS OVER 20' WIDE. JOINTS IN CURB AND GUTTER TO MATCH JOINTS IN DRIVEWAYS.
- Ø DRIVEWAYS (6" CONCRETE) SHALL BE OF A UNIFORM WIDTH (W) AS SHOWN ON SHI. P-1.

GENERAL NOTES

SIDEWALKS SHALL BE CONSTRUCTED IN ACCORDANCE WITH FDOT STANDARD SPECIFICATION NO. 522 EXCEPT FOR CURB CUT RAMP RUNS WHICH SHALL BE FINISHED IN ACCORDANCE WITH FDOT INDEX 522-002.

BOND BREAKER MATERIAL CAN BE ANY IMPERMEABLE COATED OR SHEET MEMBRANE OR PREFORMED MATERIAL HAVING A THICKNESS OF NOT LESS THAN 6 MILS NOR MORE THAN 1/2".



NOTES:

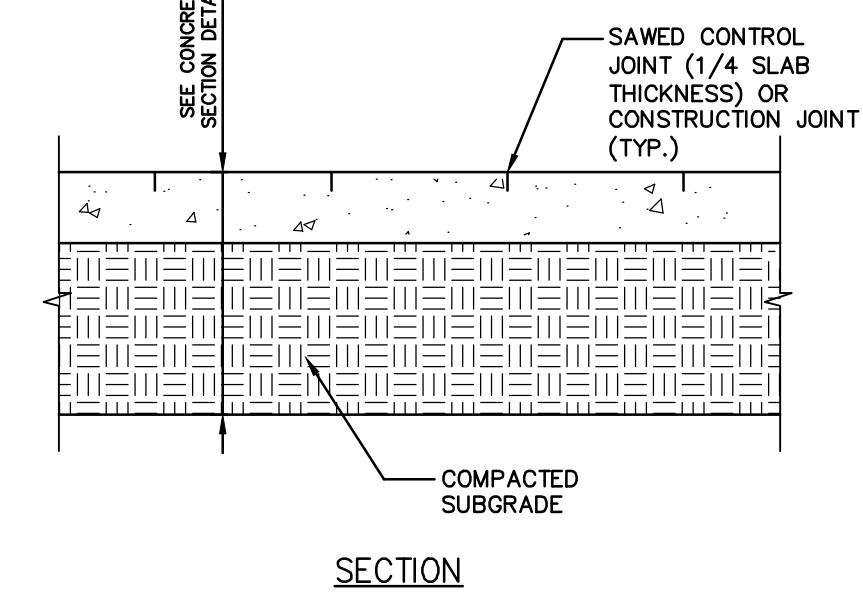
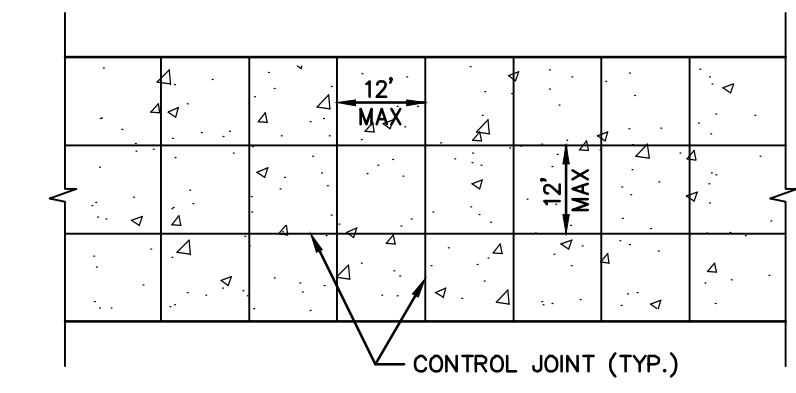
- BEDDING SHALL CONSIST OF IN-SITU GRANULAR MATERIAL (2" MAX. SIZE). UNSUITABLE IN-SITU MATERIALS SUCH AS MUCK, DEBRIS AND LARGER ROCK SHALL BE REMOVED.
- THE PIPE SHALL BE FULLY SUPPORTED FOR ITS ENTIRE LENGTH WITH APPROPRIATE COMPACTION UNDER THE PIPE HAUNCHES.
- THE PIPE SHALL BE PLACED IN A DRY TRENCH.
- BACKFILL SHALL BE FREE OF UNSUITABLE MATERIAL SUCH AS LARGER ROCK, MUCK AND DEBRIS.
- ALL WORK SHALL CONFORM TO STATE OF FLORIDA TRENCH SAFETY ACT.

* COMPACT TO 98% OF AASHTO T-180 MAXIMUM DRY DENSITY UNDER EXISTING AND PROPOSED PAVEMENT AREAS. COMPACT TO 95% ON ALL OTHER AREAS UNLESS OTHERWISE NOTED..

CONCRETE SIDEWALK DETAIL
NTS

SIDEWALKS SHALL BE CONSTRUCTED IN ACCORDANCE WITH FDOT STANDARD SPECIFICATION NO. 522 EXCEPT FOR CURB CUT RAMP RUNS WHICH SHALL BE FINISHED IN ACCORDANCE WITH FDOT INDEX 522-002.

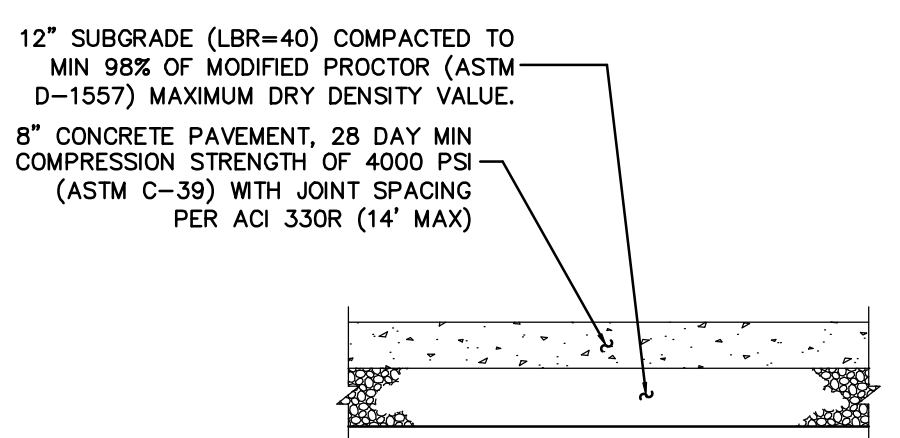
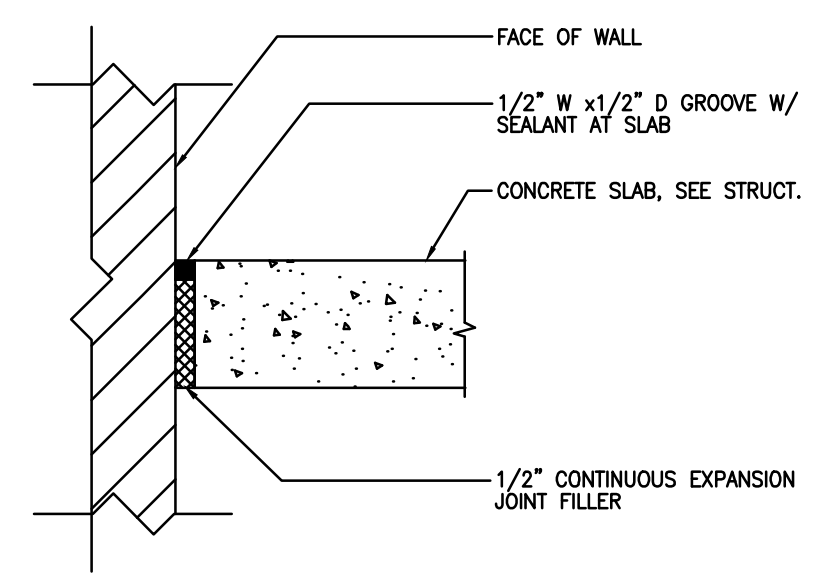
BOND BREAKER MATERIAL CAN BE ANY IMPERMEABLE COATED OR SHEET MEMBRANE OR PREFORMED MATERIAL HAVING A THICKNESS OF NOT LESS THAN 6 MILS NOR MORE THAN 1/2".



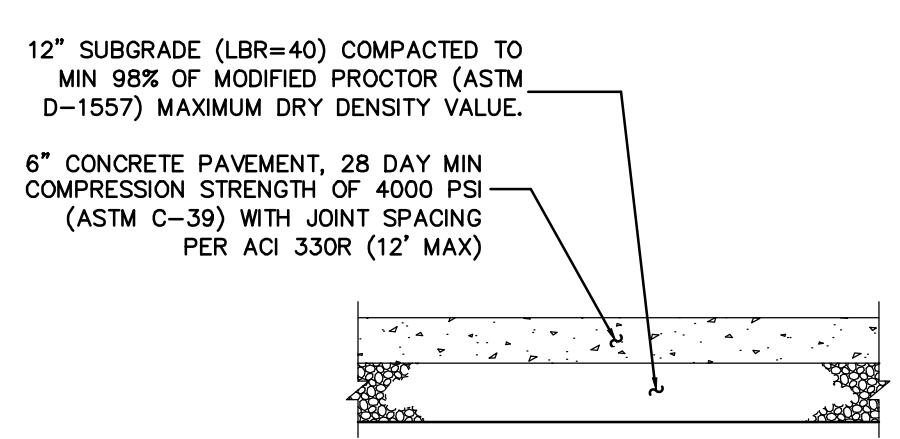
NOTES:

- LONGITUDINAL AND TRANSVERSE JOINTS SHALL BE SAWED AS INDICATED.
- THE JOINT SEAL MANUFACTURER'S SPECIFICATIONS SHALL BE COMPATIBLE WITH THE JOINT CONFIGURATION. PROVIDE TYPICAL DETAILS FOR CONSTRUCTION AND EXPANSION JOINTS.
- FOR ALL JOINTS THE BACKER ROD MATERIAL SHALL BE COMPATIBLE WITH THE COLD POUR SEALANT AND SLIGHTLY OVERSIZED TO PREVENT MOVEMENT DURING THE JOINT SEALANT OPERATION.
- THE WIDTH OF ALL JOINTS SHALL BE CORRECTED FOR 68 DEGREE (F).
- EXPANSION JOINTS SHALL BE USED AT ALL LOCATIONS WHERE PAVEMENT ABUTS A PERMANENT STRUCTURE.
- DOWELS AND TIE BARS FOR CONSTRUCTION JOINTS SHALL BE CAST IN PLACE.

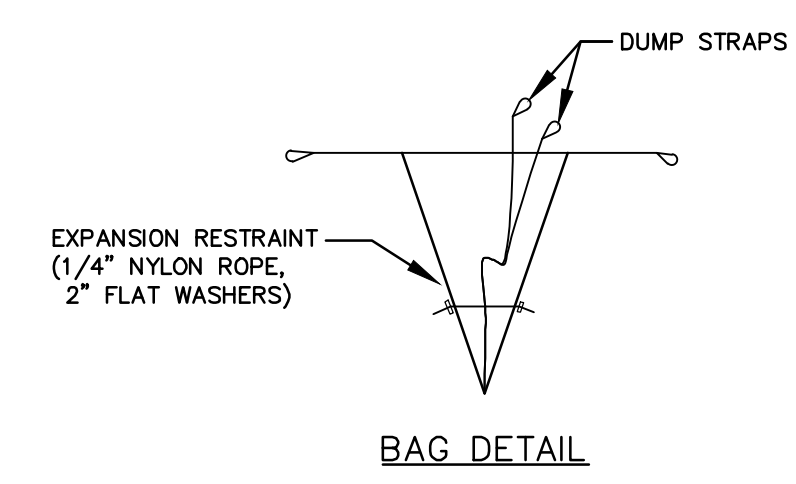
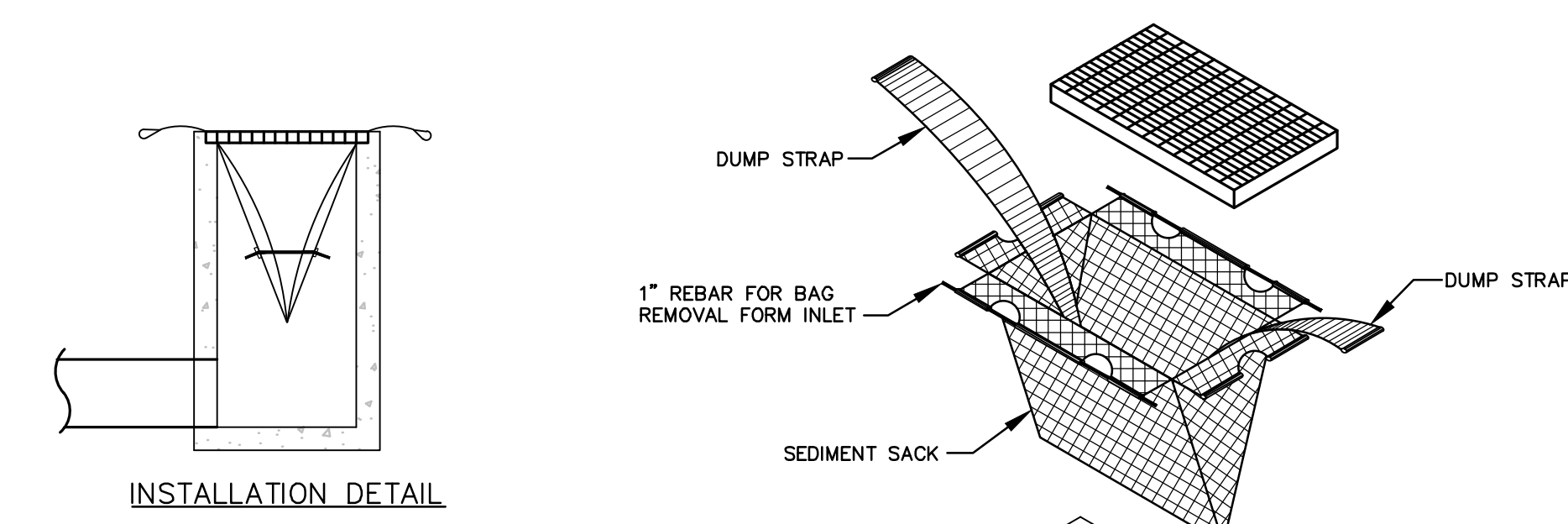
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NOT TO SCALE



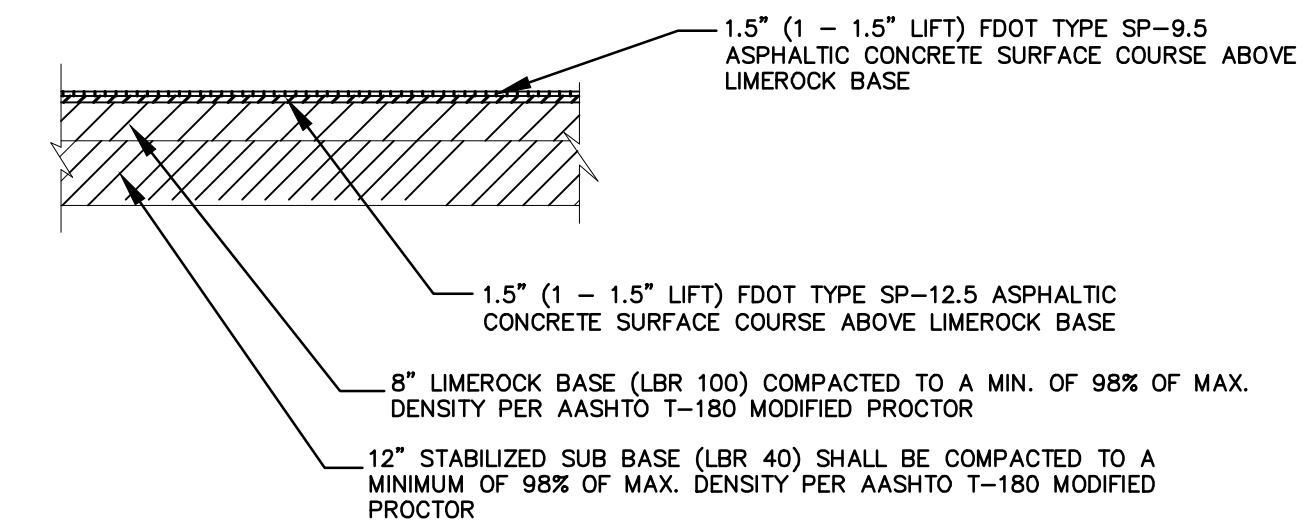
HEAVY DUTY CONCRETE PAVEMENT DETAIL
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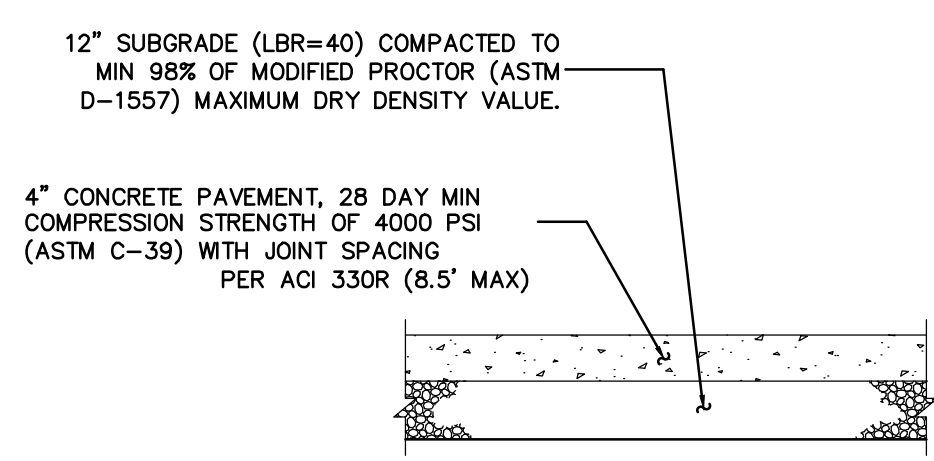
MEDIUM DUTY CONCRETE PAVEMENT DETAIL
NTS



STANDARD INLET SEDIMENT CONTROL DEVICE
NTS



ASPHALT PAVEMENT SECTION
NTS



CONCRETE SIDEWALK DETAIL
NTS

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LICENSED PROFESSIONAL ENGINEER
STATE OF FLORIDA
KIMLEY-HORN AND ASSOCIATES, INC.
189 S. ORANGE AVENUE, SUITE 1000, ORLANDO, FL 32801
PHONE: 407-898-1511
WWW.KIMLEY-HORN.COM REGISTRY NO. 35106

KHA PROJECT	148880040
DATE	05/04/2022
SCALE	AS SHOWN
DESIGNED BY	EJF
DRAWN BY	EJF
CHECKED BY	JCS

GENERAL CONSTRUCTION DETAILS

CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION

CITY OF LAKE CITY FLORIDA

SHEET NUMBER
C7.0

SHEET NUMBER
C8.0

**CIRCLE K - US HWY
90 & I-75 FUEL
EXPANSION**
CITY OF LAKE CITY FLORIDA

**CITY OF LAKE CITY
STANDARD DETAILS**

KHA PROJECT 149880040	LICENSED PROFESSIONAL
DATE 05/04/2022	
SCALE AS SHOWN	
DESIGNED BY EJF	
DRAWN BY EJF	
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Circle K – Lake City, FL

Traffic Impact Analysis

March 2022

Kimley»Horn

TRAFFIC IMPACT ANALYSIS

Circle K – US 90 & Centurion Court Lake City, FL

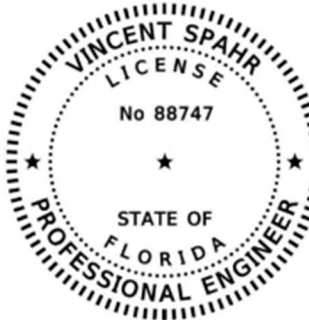
Prepared for:

Circle K

Prepared by:

Kimley-Horn and Associates, Inc.

March 2022



Vincent Spahr, P.E.
Florida Registration Number 88747
Kimley-Horn and Associates, Inc.
800 SW 2nd Avenue, Suite 100
Gainesville, Florida 32601
Registry 35106

This document has been digitally signed and sealed by Vincent Spahr, P.E. on the date adjacent to the seal.

Vincent E Spahr
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Appendix D: Synchro Output Reports

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Appendix F: FDOT *Trend* Worksheet

1.0 INTRODUCTION

Kimley-Horn has been retained by Circle K to analyze and document the traffic impacts associated with the expansion of a gas station and Circle K convenience market on the northeast quadrant of the intersection of US Highway 90 (US 90) and Centurion Court/SW Florida Gateway Drive in Lake City, Florida.

There is an existing 4,968 square-foot convenience market with 24 vehicle fueling positions (VFP) on the site. The project location is shown in Figure 1.

The applicant is proposing to add a 900-square foot expansion to the convenience market and 3 vehicle fueling positions designed for diesel trucks. The conceptual site plan is provided in Appendix A.

The study area for this traffic impact analysis includes the project driveways and the signalized intersection of US 90 and Centurion Court/SW Florida Gateway Drive, as shown in Figure 1.



Figure 1: Project Location Map

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2.0 EXISTING CONDITIONS ANALYSIS

2.1 EXISTING TRAFFIC DATA

Turning movement counts (TMCs) were collected at the study intersection on Thursday, September 2, 2021 during the AM (7:00AM – 9:00 AM) and PM (4:00PM – 6:00PM) peak periods. Raw turning movement counts are provided in Appendix B.

Turning movement volumes were adjusted using the peak season conversion factor (PSCF) from the Florida Department of Transportation (FDOT) Florida Traffic Online (FTO). Seasonal factor data is included in Appendix B. Existing signal timings were provided by Lake City staff for use in the analysis, signal timing worksheets are included in Appendix B.

Figure 2 illustrates turning movement volumes for existing peak season conditions at the study intersection. The intersection volume development worksheet can be found in Appendix C.

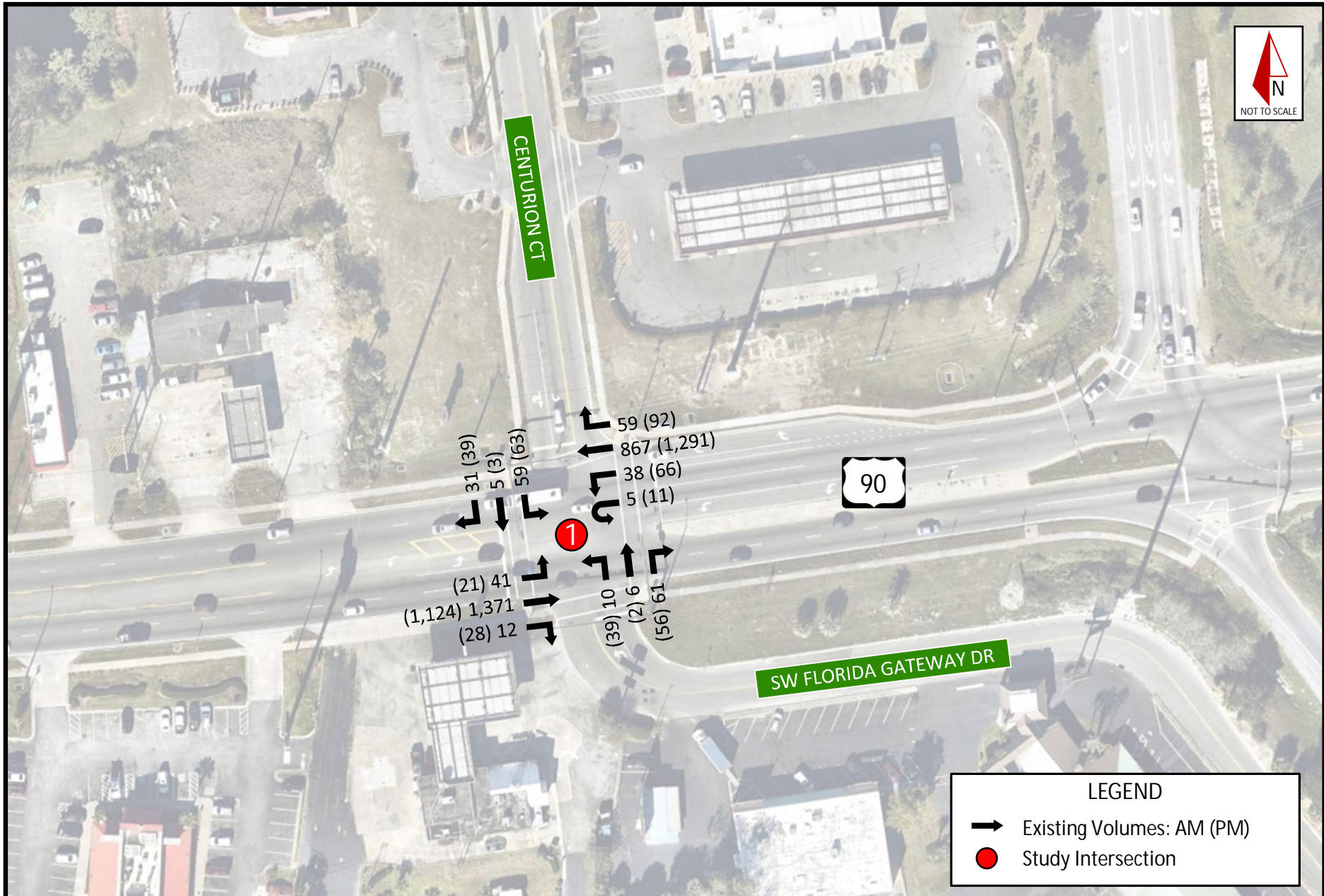
2.2 EXISTING INTERSECTION CONDITIONS

Intersection capacity analyses were performed for existing (2021) conditions using the operational analysis procedures outlined in the latest *Highway Capacity Manual, 6th Edition* (HCM 6). Specifically, *Synchro* (v11) software was used to evaluate existing operational conditions at study area intersections by reporting delay, level of service (LOS), volume-to-capacity (v/c) ratios, and the 95th percentile queue for each movement. Table 1 summarizes the operational analyses for the existing AM and PM peak hour conditions at the study intersection. Synchro outputs are provided in Appendix D.

Table 1: Existing Intersection Conditions

		AM Peak Hour				PM Peak Hour			
		Delay (sec/veh)	LOS	v/c Ratio	95th percentile queue (veh)	Delay (sec/veh)	LOS	v/c Ratio	95th percentile queue (veh)
US 90 & Centurion Court	Overall Intersection	13.2	B	-	-	13.2	B	-	-
	Eastbound	11.5	B	-	-	9.6	A	-	-
	EBL	5.5	A	0.10	0.5	6.9	A	0.08	0.3
	EBT	11.7	B	0.58	14.6	9.7	A	0.48	12.5
	EBT/R	11.7	B	0.58	15.2	9.6	A	0.48	12.9
	Westbound	7.7	A	-	-	8.7	A	-	-
	WBL	7.9	A	0.16	0.5	6.7	A	0.24	1.1
	WBT	7.9	A	0.38	8.0	9.0	A	0.54	13.7
	WBR	5.8	A	0.06	0.9	5.4	A	0.09	1.5
	Northbound	55.6	E	-	-	65.1	E	-	-
	NBL	54.9	D	0.06	0.6	66.7	E	0.28	2.9
	NBT/R	55.7	E	0.42	4.1	64.0	E	0.38	4.2
	Southbound	58.8	E	-	-	68.1	E	-	-
	SBL	61.9	E	0.45	3.8	71.6	E	0.51	5.0
	SBT/R	53.6	D	0.23	2.1	62.7	E	0.27	3.0

The intersection of US 90 and Centurion Court operates with LOS B during existing (2021) AM peak hour and PM peak hour conditions. All movements operate with v/c ratios less than 1.00 under existing (2021) AM and PM peak hour conditions. The northbound and southbound approaches operate with LOS E during the AM and PM peak hour due to the prioritization of green time for the mainline US 90 movements.



LEGEND

- Existing Volumes: AM (PM)
- Study Intersection

Figure 2: Existing (2021) Intersection Volumes

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3.0 PROJECT DEVELOPMENT

The existing site currently has 24 VFPs and a 4,968-square foot Circle K convenience store. The proposed expansion will add approximately 900-square feet to the existing convenience market and 3 VFPs north of the existing site. The latest industry standards were referenced to evaluate the amount of new external trips to be generated by the site at buildout.

3.1 SITE ACCESS

Access to the site is proposed via two existing driveways and one new driveway along Centurion Court, as shown in the site plan provided in Appendix A.

3.2 TRIP GENERATION

Trip generation and pass-by rates for the proposed development were calculated using the 11th Edition of the Institute of Transportation Engineers' (ITE) *Trip Generation Manual*. Land Use Code (LUC) 945 (Gasoline Station with Convenience Market) was used to calculate the trip generation potential for the existing and proposed development.

The trip generation potential of the existing Circle K convenience store and gas station was compared to observed traffic volumes on Centurion Court north of US 90. Table 2 summarizes the comparison of the calculated trip generation potential of the existing development and the observed peak hour volumes on Centurion Court.

Table 2: Existing Site Trip Generation Comparison

	AM Peak Hour			PM Peak Hour		
	Total	In (NB)	Out (SB)	Total	In (NB)	Out (SB)
ITE <i>Trip Generation Manual</i>	649	325	324	546	273	273
Observed Peak Season Traffic	201	106	95	220	115	105

Since the existing AM and PM peak hour traffic volumes were significantly less than the trip generation potential of the existing development, the trip generation calculations for the proposed expansion to the convenience store and gas station were adjusted proportionately to reflect actual conditions anticipated at the site under buildout conditions.

Table 3 provides the AM peak hour, and PM peak hour trip generation calculations for the proposed expansion and the adjustment applied based on the existing trip generation comparison. A factor of 0.31 (201/649) was applied to the AM peak hour trip generation calculations and a factor of 0.40 (220/546) was applied to the PM peak hour trip generation calculations in accordance with the comparison illustrated in Table 2.

As summarized in Table 3, the proposed expansion is anticipated to generate 16 net new AM peak hour trips (8 inbound and 8 outbound), and 18 net new PM peak hour trips (9 inbound and 9 outbound) to the external roadway network at buildout. In addition, the proposed expansion is anticipated to generate 48 AM peak hour pass-by trips (24 inbound and 24 outbound), and 54 PM peak hour pass-by trips (27 inbound

and 27 outbound). A detailed table including all trip generation calculations and adjustments is provided in Appendix E.

Table 3: Trip Generation Summary

	AM Peak Hour			PM Peak Hour		
	Total	In (NB)	Out (SB)	Total	In (NB)	Out (SB)
ITE <i>Trip Generation Manual</i> (Net New)	50	25	25	46	23	23
ITE <i>Trip Generation Manual</i> (Pass-by)	204	102	102	180	90	90
Adjustment Factor	0.31			0.40		
Adjusted Net New Trips	16	8	8	18	9	9
Adjusted Pass-by Trips	48	24	24	54	27	27

3.3 TRIP DISTRIBUTION

The project's trip distribution was developed based on observed traffic patterns within the study area roadway network and engineering judgement. Figure 3 displays the anticipated trip distribution for the proposed Circle K gas station expansion at buildout.

3.4 TRIP ASSIGNMENT

Site distribution percentages were used to assign anticipated project trips to the study area intersection and driveways. Figure 4 shows the anticipated AM and PM peak hour project movements at the study area intersection and project driveways.

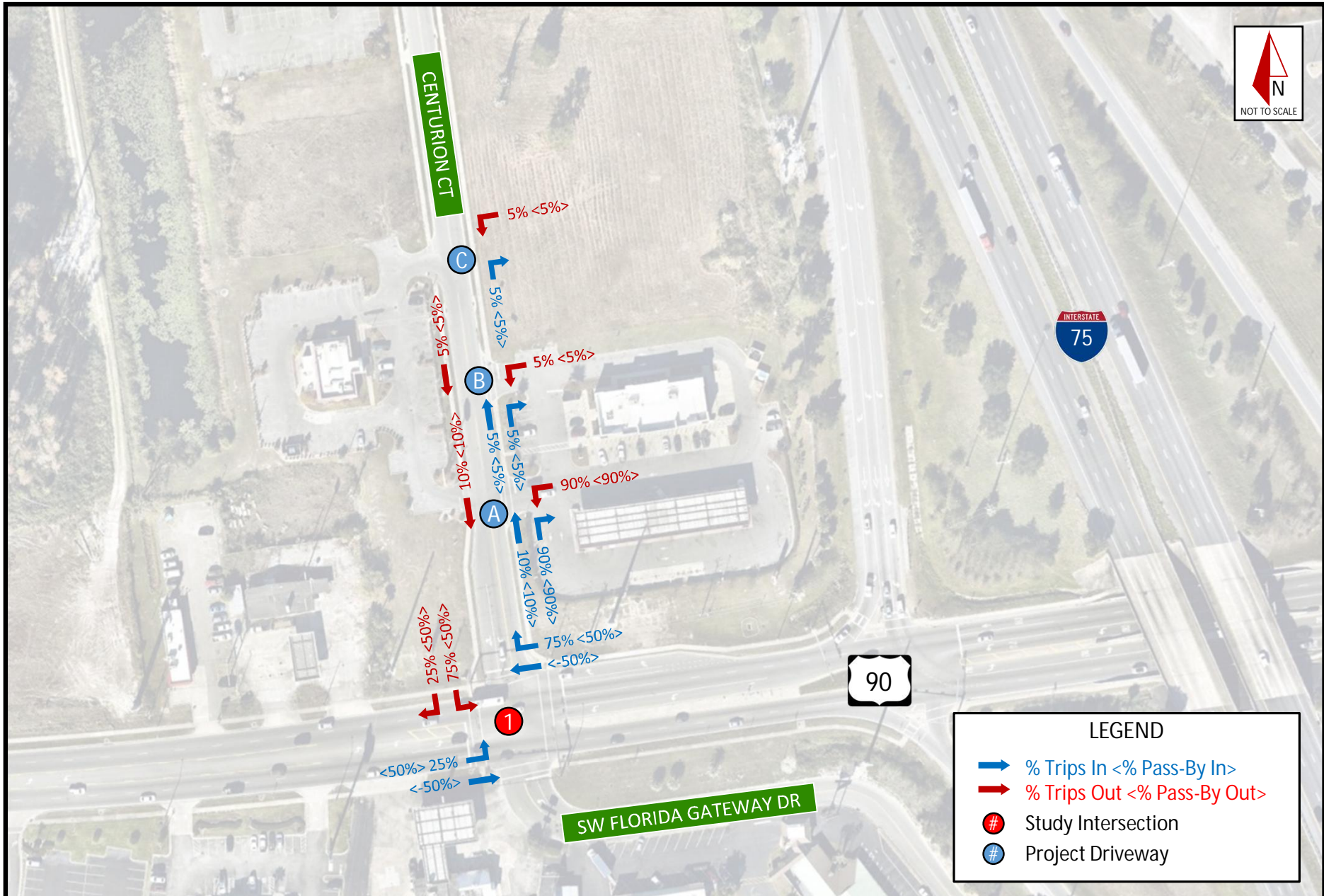


Figure 3: Project Trip Distribution

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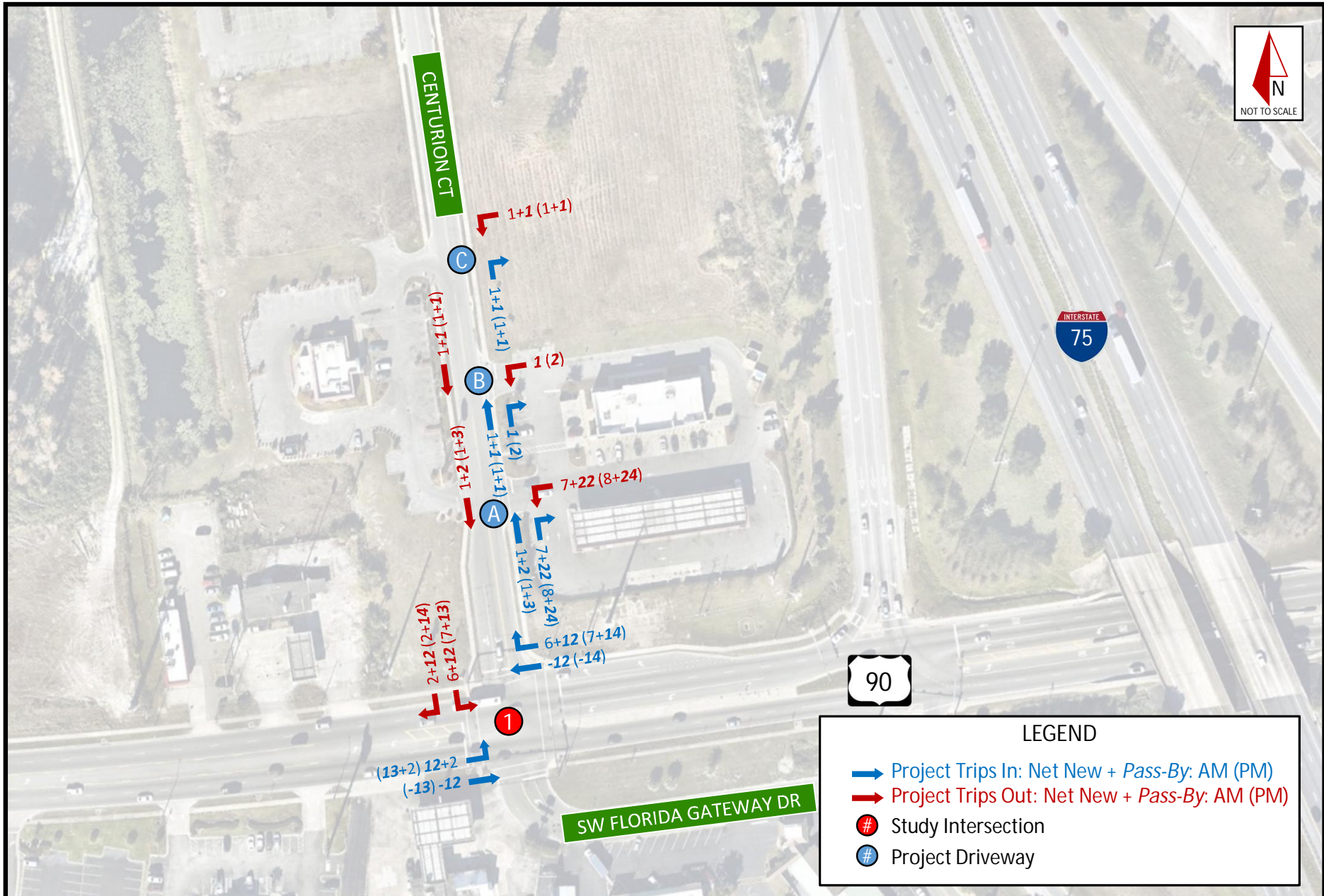


Figure 4: Project Trip Assignment

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4.0 BACKGROUND CONDITIONS ANALYSIS – YEAR 2023

4.1 HISTORICAL TRAFFIC GROWTH

A historical traffic growth rate was calculated based upon the nearest historical Annual Average Daily Traffic (AADT) data available from FTO. A 2.11% annual historical growth rate was calculated based on the average traffic growth exhibited over the past five (5) years from an FDOT count station located east of the project site on US 90. The growth trend worksheet can be found in Appendix F.

4.2 BACKGROUND TRAFFIC

Traffic conditions were evaluated for year 2023 background conditions prior to the addition of project traffic. Background volumes at study area intersections were derived by applying 2.11% annual growth to existing (2021) traffic counts. Figure 5 illustrates AM peak hour and PM peak hour turning movement volumes for background conditions at the study intersection. The intersection volume development worksheet can be found in Appendix C.

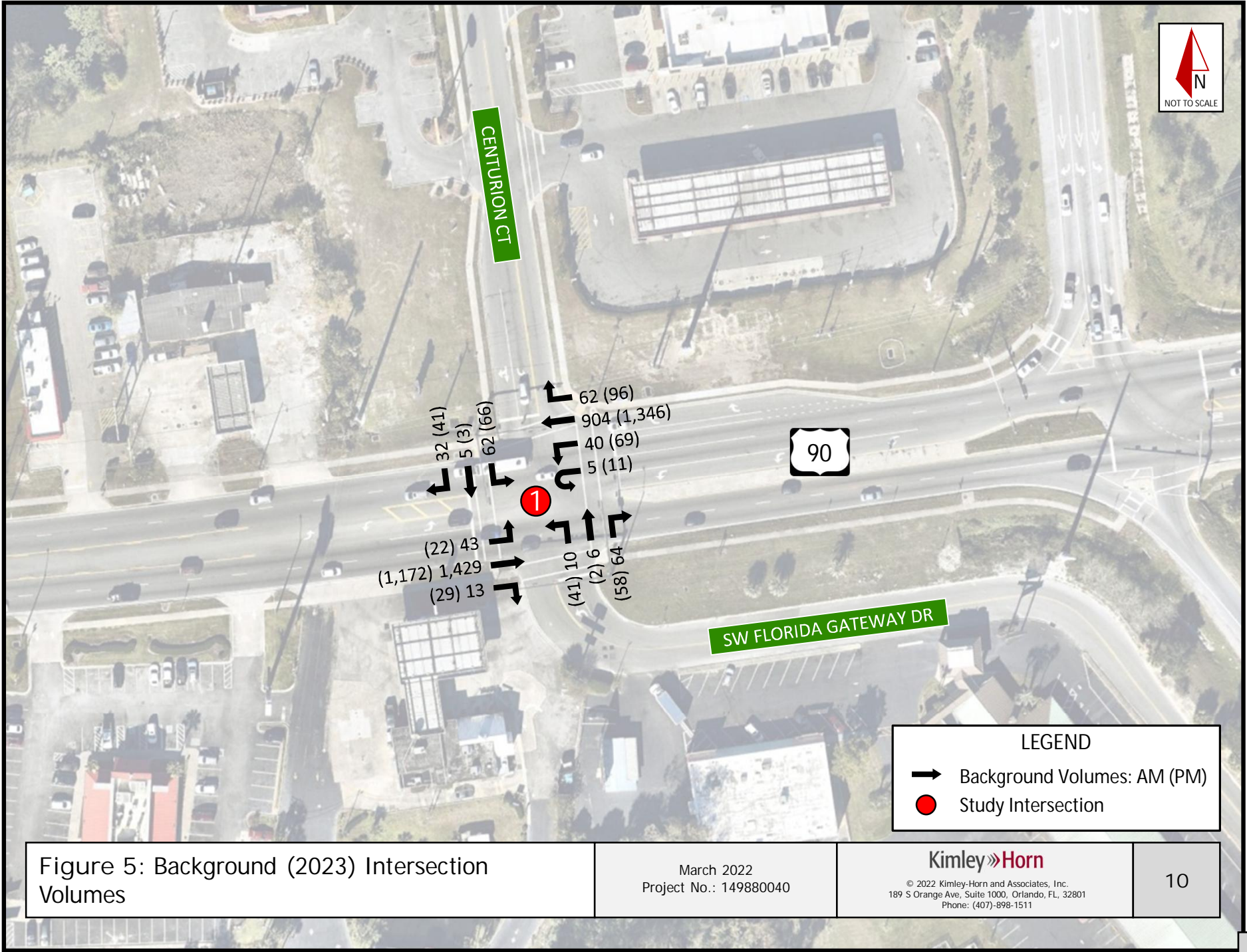
4.3 BACKGROUND INTERSECTION ANALYSIS

Intersection operational analyses were performed for 2023 background conditions in the AM and PM peak hours using procedures outlined in the *Highway Capacity Manual 6* with *Synchro* (v11) software. Table 4 summarizes the operational analyses for the 2023 background AM and PM peak hour conditions at the study intersection. Synchro outputs are provided in Appendix D.

Table 4: Background Intersection Conditions

		AM Peak Hour				PM Peak Hour			
		Delay (sec/veh)	LOS	v/c Ratio	95th percentile queue (veh)	Delay (sec/veh)	LOS	v/c Ratio	95th percentile queue (veh)
US 90 & Centurion Court	Overall Intersection	13.8	B	-	-	13.7	B	-	-
	Eastbound	12.4	B	-	-	10.2	B	-	-
	EBL	5.8	A	0.11	0.5	7.5	A	0.09	0.3
	EBT	12.6	B	0.61	15.8	10.2	B	0.50	13.4
	EBT/R	12.5	B	0.61	16.4	10.2	B	0.51	13.8
	Westbound	8.2	A	-	-	9.3	A	-	-
	WBL	8.8	A	0.17	0.6	7.3	A	0.26	1.1
	WBT	8.3	A	0.39	8.6	9.7	A	0.57	14.9
	WBR	6.0	A	0.06	0.9	5.6	A	0.09	1.6
	Northbound	55.1	E	-	-	64.8	E	-	-
	NBL	54.4	D	0.06	0.6	66.6	E	0.29	3.1
	NBT/R	55.2	E	0.42	4.3	63.5	E	0.38	4.4
	Southbound	58.6	E	-	-	67.8	E	-	-
	SBL	61.8	E	0.47	4.1	71.5	E	0.52	5.2
SBT/R	53.1	D	0.22	2.2	62.3	E	0.28	3.2	

The intersection of US 90 and Centurion Court is expected to operate with LOS B during background (2023) AM peak hour and PM peak hour conditions. All movements are expected to operate with v/c ratios less than 1.00 under background (2023) AM and PM peak hour conditions. The northbound and southbound approaches are expected to continue to operate with LOS E during the AM and PM peak hour due to the prioritization of green time for the mainline US 90 movements.



LEGEND

- Background Volumes: AM (PM)
- Study Intersection

Figure 5: Background (2023) Intersection Volumes

March 2022
Project No.: 149880040

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5.0 BUILDOUT CONDITIONS ANALYSIS – YEAR 2023

5.1 BUILDOUT TRAFFIC

Future traffic conditions for the proposed development were evaluated for year 2023 conditions with the inclusion of project traffic. Buildout volumes were developed by adding anticipated project trips to background (2023) volumes. Figure 6 illustrates the projected turning movement volumes under buildout AM and PM peak hour conditions at the study intersection and the proposed driveways. The intersection volume development worksheet can be found in Appendix C.

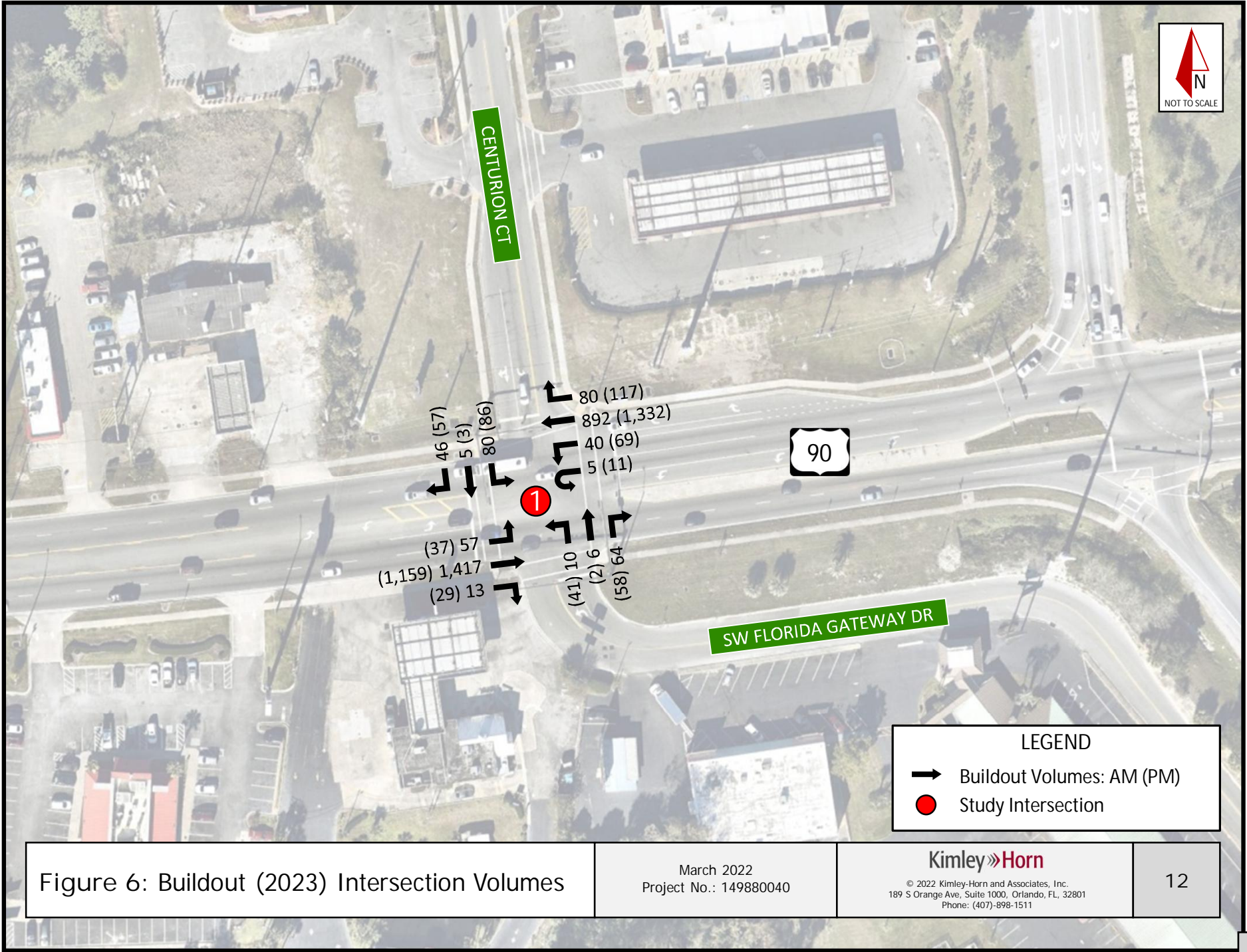
5.2 BUILDOUT INTERSECTION ANALYSIS

Intersection operational analyses were performed for 2023 buildout conditions in the AM and PM peak hour conditions using procedures outlined in the *Highway Capacity Manual 6* with *Synchro* (v11) software. Table 5 summarizes the operational analyses for the 2023 buildout AM and PM peak hour conditions at the study intersection. Synchro outputs are provided in Appendix D.

Table 5: Buildout Intersection Conditions

		AM Peak Hour				PM Peak Hour			
		Delay (sec/veh)	LOS	v/c Ratio	95th percentile queue (veh)	Delay (sec/veh)	LOS	v/c Ratio	95th percentile queue (veh)
US 90 & Centurion Court	Overall Intersection	15.0	B	-	-	15.5	B	-	-
	Eastbound	13.3	B	-	-	11.3	B	-	-
	EBL	6.4	A	0.14	0.8	8.9	A	0.16	0.6
	EBT	13.6	B	0.62	16.5	11.4	B	0.51	14.2
	EBT/R	13.5	B	0.62	17.1	11.4	B	0.51	14.7
	Westbound	9.0	A	-	-	10.8	B	-	-
	WBL	9.5	A	0.18	0.6	8.3	A	0.27	1.3
	WBT	9.2	A	0.4	9.0	11.3	B	0.58	16.2
	WBR	6.8	A	0.08	1.3	6.8	A	0.12	2.2
	Northbound	53.2	D	-	-	62.7	E	-	-
	NBL	53.8	D	0.06	0.6	65.5	E	0.28	3.1
	NBT/R	53.1	D	0.37	4.2	60.7	E	0.33	4.3
	Southbound	57.8	E	-	-	66.4	E	-	-
	SBL	61.3	E	0.54	5.2	70.4	E	0.58	6.8
SBT/R	52.1	D	0.28	3.0	60.7	E	0.33	4.2	

The intersection of US 90 and Centurion Court is expected to operate with LOS B during buildout (2023) AM peak hour and PM peak hour conditions. All movements are expected to operate with v/c ratios less than 1.00 under buildout (2023) AM and PM peak hour conditions. The northbound and southbound approaches are expected to continue to operate with LOS E during the AM and PM peak hour due to the prioritization of green time for the mainline US 90 movements.



LEGEND

- ➔ Buildout Volumes: AM (PM)
- Study Intersection

Figure 6: Buildout (2023) Intersection Volumes

March 2022
Project No.: 149880040

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6.0 CONCLUSION

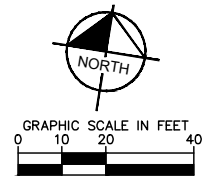
This traffic impact analysis was performed to assess the transportation impacts of the proposed expansion of a gas station and Circle K convenience market located in the northwest quadrant of the intersection of US Highway 90 (SR 10) and Centurion Court/SW Florida Gateway Drive. The expansion, proposed for buildout in year 2023, will include the addition of 3 vehicle fueling positions designed for diesel trucks and a 900-square foot expansion to the existing Circle K convenience market. Access to the site will be provided via two existing driveways and one new driveway to the north on Centurion Court.

Accounting for the observed trip generation of the existing site, the proposed expansion is anticipated to generate 16 net new AM peak hour trips and 18 net new PM peak hour trips at buildout. An additional 48 new AM peak hour pass-by trips and 54 new PM peak hour pass-by trips are expected at the site as well.

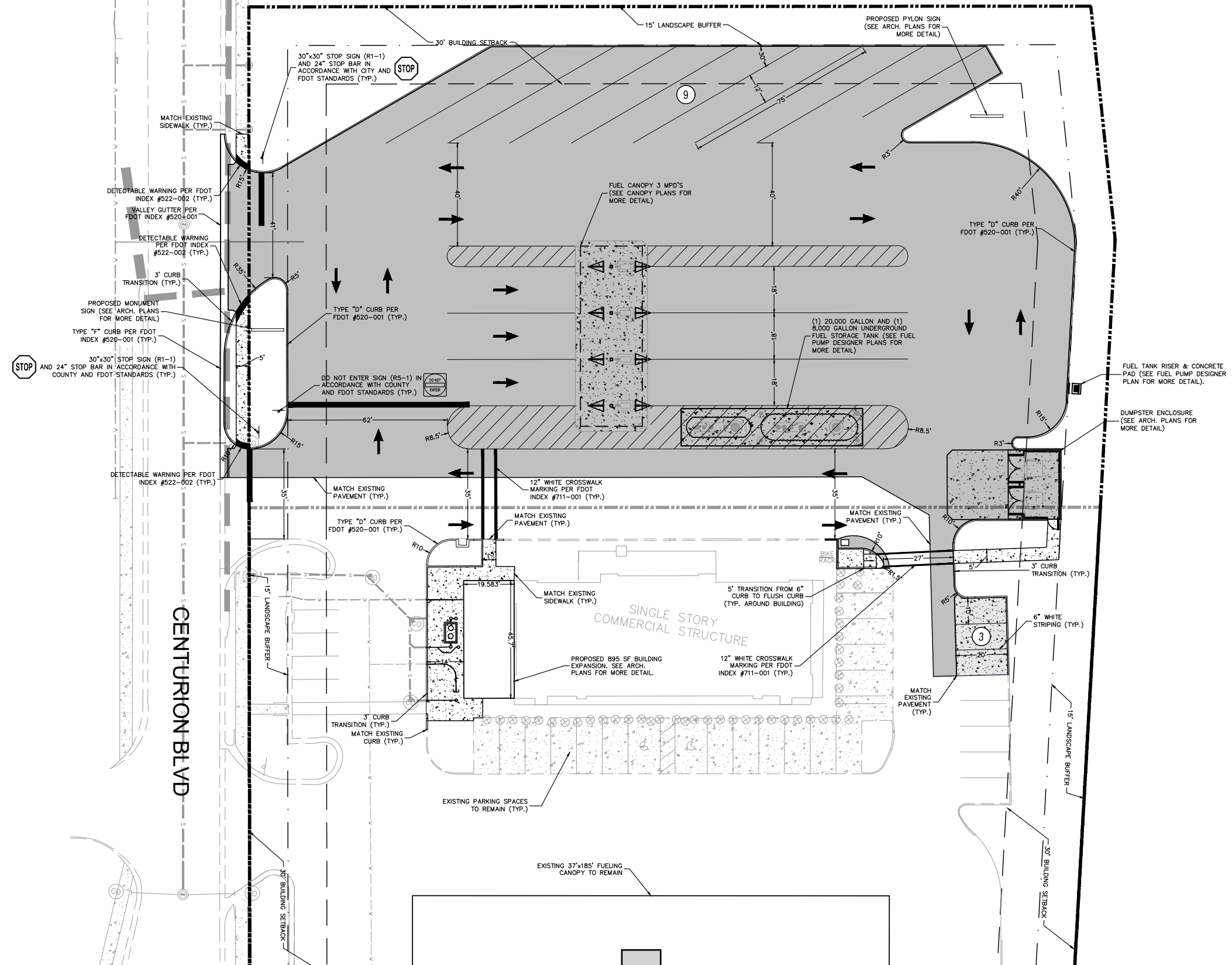
Operational analyses were performed utilizing *Synchro* software for the existing (2021), background (2023), and buildout (2023) conditions at the study intersection of US 90 and Centurion Court/SW Florida Gateway Drive during the AM peak hour and the PM peak hour. Results indicated that the study intersection is expected to operate at LOS B through the buildout year. No operational deficiencies are expected at the study intersection with the inclusion of project traffic under buildout (2023) conditions.

APPENDIX A
Conceptual Site Plan

Plotted By: Fitch, Elliot - Sheet Set: CIRCLE K - US HWY 90 & I-75 High Speed Diesel Fuel Expansion - Layout: C4.1 SITE PLAN - March 14, 2022 - 07:56:28am - K:\ORL\Civil\149880040-Circle K US90 & I75\CADD\CONSTR\PlanSheets\C4.0 - SITE PLAN.dwg
 This document, together with the concepts and designs presented herein, is intended only for the specific purpose and client for which it was prepared. Reuse of and improper reliance on this document without written authorization and adaptation by Kimley-Horn and Associates, Inc. shall be without liability to Kimley-Horn and Associates, Inc.

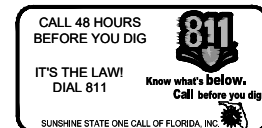


- NOTES:**
1. ALL CURB DIMENSIONS ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
 2. BUILDING DIMENSIONS ARE TO THE OUTSIDE FACE OF BUILDING UNLESS OTHERWISE NOTED.
 3. REFER TO ARCHITECTURAL AND STRUCTURAL PLANS TO VERIFY ALL BUILDING DIMENSIONS.
 4. REFER TO SIGNAGE PLANS FOR MONUMENT SIGN DETAILS.
 5. SEE MEP PLANS FOR ELECTRICAL DRAWINGS.
 6. ALL PROPOSED ON-SITE STRIPING AND PAVEMENT MARKING WILL BE PAINTED UNLESS OTHERWISE NOTED AND IN ACCORDANCE WITH FDOT INDEX 711-001.
 7. REFER TO ARCHITECTURAL PLANS FOR PROPOSED TRASH CAN LOCATIONS AND DESIGN.
 8. BOLLARDS IN SIDEWALK ADJACENT TO BUILDING SHALL BE COVERED WITH RED PLASTIC COVERS TO BE SUPPLIED BY CONTRACTOR.
 9. BOLLARDS UNDER CANOPY SHALL BE COVERED WITH GRAY PLASTIC COVERS TO BE SUPPLIED BY CONTRACTOR (SEE FUEL PUMP DESIGNER PLANS FOR MORE DETAIL).
 10. REFER TO ARCHITECTURAL PLANS FOR SITE LIGHTING AND ELECTRICAL PLANS.
 11. ALL SIGNAGE AND PAVEMENT MARKINGS SHALL MEET MUTCD AND FDOT STANDARDS.
 12. ALL SIGNAGE SHALL MEET THE REQUIREMENTS OF POLK COUNTY LAND DEVELOPMENT CODE, CHAPTER 7, SEC. 760



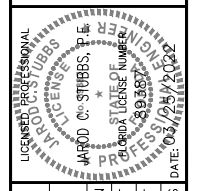
- LEGEND**
- PROPERTY LINE (TYP.)
 - PROPOSED ASPHALT PAVEMENT (SEE DETAIL SHEET C7.0)
 - PROPOSED CONCRETE SIDEWALK (SEE DETAIL SHEET C7.0)
 - PROPOSED MEDIUM DUTY CONCRETE (SEE DETAIL SHEET C7.0)
 - PROPOSED HEAVY DUTY CONCRETE (SEE DETAIL SHEET C7.0)

VERTICAL DATUM:
 ELEVATIONS ARE BASED ON BENCHMARK DESIGNATION BM 32 BEING: 117.497 FEET, (NGVD 29), PUBLISHED BY FLORIDA DEPARTMENT OF TRANSPORTATION. ORTHOMETRIC HEIGHT CONVERSION PROVIDED BY VERTCON; DATUM SHIFT (NAVD-NGVD) = -0.883 FEET



No.	REVISIONS	DATE	BY

Kimley-Horn
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 WWW.KIMLEY-HORN.COM REGISTRY No. 35106



KHA PROJECT: 149880040
 DATE: 03/25/2022
 SCALE: AS SHOWN
 DESIGNED BY: EJJF
 DRAWN BY: EJJF
 CHECKED BY: JCS

SITE PLAN

CIRCLE K - US HWY 90 & I-75 FUEL EXPANSION
 FLORIDA
 CITY OF LAKE CITY
 SHEET NUMBER
C4.1

APPENDIX B

Traffic Data

APPENDIX 8 : TRAFFIC COUNT

Florida

LEASE - NTI

US 90 and Florida Gateway Drive
Lake City FL



TRAFFIC COUNT AT THE SITE PROPOSED

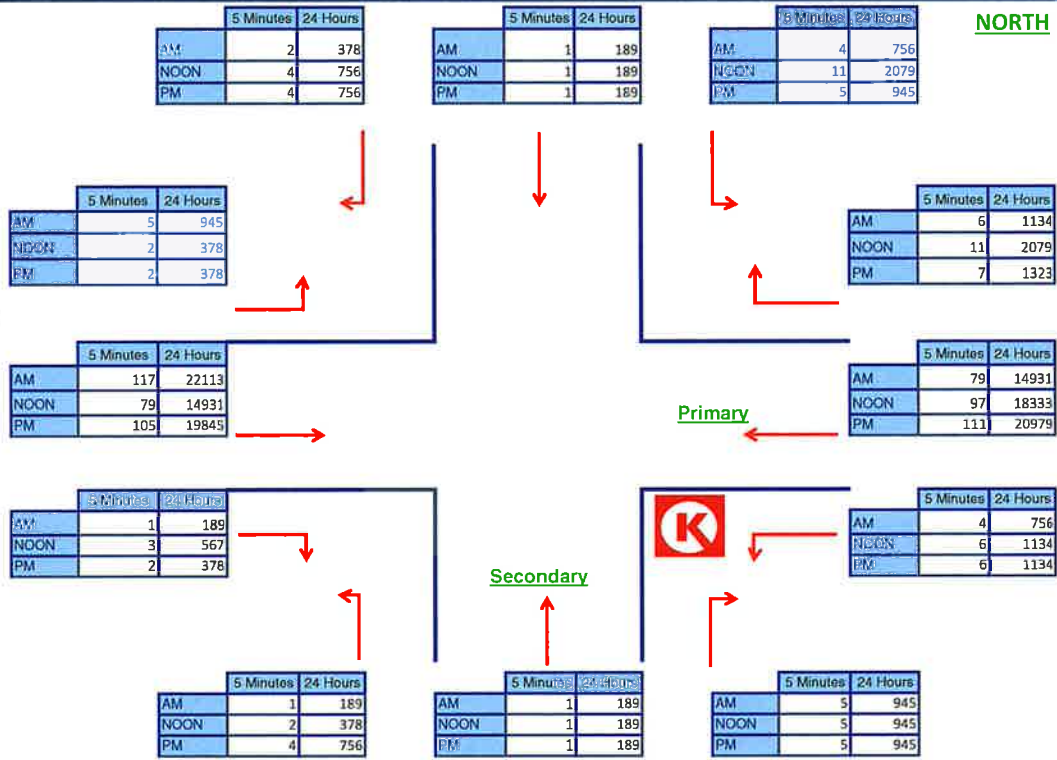
MANUAL

Traffic counts taken during COVID-19

Total AM:	42,714
Total NOON:	41,958
Total PM:	47,817

NOON COUNT

Primary	37,422
Secondary	4,536
TOTAL:	41,958 cars/day



D.O.T.

	PRIMARY	SECONDARY	TOTAL	YEAR
DAILY TRAFFIC TOTALS	27,000		27,000	2020

DIRECT TRAFFIC

IS ACCESS TO THIS SITE AFFECTED BY MEDIAN	NO
---	----

	PRIMARY	SECONDARY	TOTAL
DIRECT TRAFFIC TOTALS			0

TRUCK TRAFFIC COUNT

	PRIMARY	SECONDARY	TOTAL	SOURCE
TRUCK TRAFFIC TOTALS			0	



National Data & Surveying Services

Site Code: 21-120370-001

Date: 09/02/2021

Weather: Sunny

City: Lake City

County: Columbia

Count Times: 07:00 - 09:00

12:00 - 14:00

16:00 - 18:00

Control: Signalized

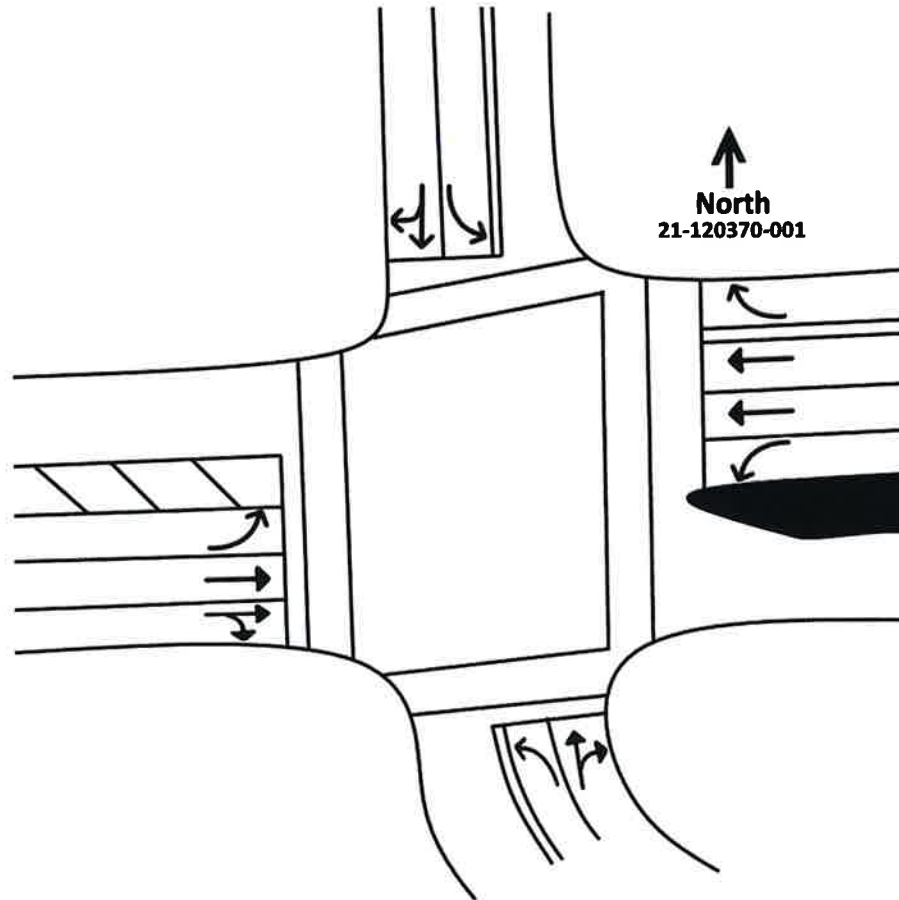
SIGNAL TIMING

PHASES	1	2	3
NT/ST	00:25	00:33	00:20
EL/WL	00:15	-	-
WL/WT	-	00:13	-
ET/WT	01:42	01:34	01:59



N/S Street: Florida Gateway Dr

Speed: N/A



E/W Street: US Hwy 90

Speed: 45 MPH

National Data & Surveying Services Intersection Turning Movement Count

Location: Florida Gateway Dr & US Hwy 90
City: Lake City
Control: Signalized

Project ID: 21-120370-001
Date: 9/2/2021

Data - Total

NS/EW Streets:	Florida Gateway Dr				Florida Gateway Dr				US Hwy 90				US Hwy 90				TOTAL
	NORTHBOUND				SOUTHBOUND				EASTBOUND				WESTBOUND				
AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	TOTAL
	NL	NT	NR	NU	SL	ST	SR	SU	EL	ET	ER	EU	WL	WT	WR	WU	
7:00 AM	4	1	7	0	16	0	7	0	6	215	1	0	2	149	8	1	417
7:15 AM	1	0	16	0	7	2	8	0	6	348	2	0	4	182	11	1	588
7:30 AM	2	2	14	0	14	2	4	0	8	378	1	0	10	215	11	0	661
7:45 AM	1	1	15	0	13	1	6	0	15	350	4	0	11	236	18	2	673
8:00 AM	6	3	14	0	23	0	12	0	11	255	5	0	12	209	17	2	569
8:15 AM	2	1	16	0	22	0	10	0	10	214	5	0	16	196	16	0	508
8:30 AM	4	0	12	0	19	3	11	0	7	239	6	0	16	211	21	1	550
8:45 AM	6	2	12	0	17	4	9	0	7	211	6	0	7	203	11	1	496
TOTAL VOLUMES :	26	10	106	0	131	12	67	0	70	2210	30	0	78	1601	113	8	4462
APPROACH %'s :	18.31%	7.04%	74.65%	0.00%	62.38%	5.71%	31.90%	0.00%	3.03%	95.67%	1.30%	0.00%	4.33%	88.94%	6.28%	0.44%	
PEAK HR :	07:15 AM - 08:15 AM																TOTAL
PEAK HR VOL :	10	6	59	0	57	5	30	0	40	1331	12	0	37	842	57	5	2491
PEAK HR FACTOR :	0.417	0.500	0.922	0.000	0.620	0.625	0.625	0.000	0.667	0.880	0.600	0.000	0.771	0.892	0.792	0.625	0.925
	0.815				0.657				0.893				0.881				
NOON	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	TOTAL
	NL	NT	NR	NU	SL	ST	SR	SU	EL	ET	ER	EU	WL	WT	WR	WU	
12:00 PM	5	0	10	0	23	0	8	0	6	318	4	0	16	230	26	3	649
12:15 PM	7	0	18	0	27	0	8	0	7	237	4	0	19	261	25	4	617
12:30 PM	4	2	13	0	21	0	13	0	6	290	3	1	15	252	21	0	641
12:45 PM	7	1	12	0	18	1	13	0	8	234	4	0	15	300	34	2	649
1:00 PM	7	1	16	0	34	1	13	0	5	236	9	0	16	291	34	2	665
1:15 PM	8	1	14	0	28	1	5	0	4	252	7	0	17	291	22	2	652
1:30 PM	3	0	12	0	30	0	5	0	5	243	9	0	21	273	26	4	631
1:45 PM	2	1	18	0	25	1	4	0	5	254	1	0	11	290	20	0	632
TOTAL VOLUMES :	43	6	113	0	206	4	69	0	46	2064	41	1	130	2188	208	17	5136
APPROACH %'s :	26.54%	3.70%	69.75%	0.00%	73.84%	1.43%	24.73%	0.00%	2.14%	95.91%	1.91%	0.05%	5.11%	86.04%	8.18%	0.67%	
PEAK HR :	12:30 PM - 01:30 PM																TOTAL
PEAK HR VOL :	26	5	55	0	101	3	44	0	23	1012	23	1	63	1134	111	6	2607
PEAK HR FACTOR :	0.813	0.625	0.859	0.000	0.743	0.750	0.846	0.000	0.719	0.872	0.639	0.250	0.926	0.945	0.816	0.750	0.980
	0.896				0.771				0.883				0.936				
PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	TOTAL
	NL	NT	NR	NU	SL	ST	SR	SU	EL	ET	ER	EU	WL	WT	WR	WU	
4:00 PM	4	0	14	0	16	0	7	0	3	273	3	0	11	349	30	3	713
4:15 PM	4	0	16	0	18	0	12	0	5	237	3	0	15	314	12	1	637
4:30 PM	4	0	13	0	21	2	7	0	7	239	7	0	11	295	21	0	627
4:45 PM	5	1	14	0	15	0	6	0	5	287	6	0	17	310	19	3	688
5:00 PM	13	1	15	0	16	2	13	0	6	316	6	0	14	334	21	4	761
5:15 PM	7	0	12	0	13	1	9	0	3	290	7	0	15	265	24	4	650
5:30 PM	13	0	13	0	17	0	10	0	6	198	8	0	18	344	25	0	652
5:45 PM	5	0	27	0	18	1	7	0	4	223	8	0	22	265	28	4	612
TOTAL VOLUMES :	55	2	124	0	134	6	71	0	39	2063	48	0	123	2476	180	19	5340
APPROACH %'s :	30.39%	1.10%	68.51%	0.00%	63.51%	2.84%	33.65%	0.00%	1.81%	95.95%	2.23%	0.00%	4.40%	88.49%	6.43%	0.68%	
PEAK HR :	04:45 PM - 05:45 PM																TOTAL
PEAK HR VOL :	38	2	54	0	61	3	38	0	20	1091	27	0	64	1253	89	11	2751
PEAK HR FACTOR :	0.731	0.500	0.900	0.000	0.897	0.375	0.731	0.000	0.833	0.863	0.844	0.000	0.889	0.911	0.890	0.688	0.904
	0.810				0.823				0.867				0.915				

National Data & Surveying Services Intersection Turning Movement Count

Location: Florida Gateway Dr & US Hwy 90
 City: Lake City
 Control: Signalized

Project ID: 21-120370-001
 Date: 9/2/2021

Data - Cars

NS/EW Streets:	Florida Gateway Dr				Florida Gateway Dr				US Hwy 90				US Hwy 90				TOTAL
	NORTHBOUND				SOUTHBOUND				EASTBOUND				WESTBOUND				
AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	TOTAL
	NL	NT	NR	NU	SL	ST	SR	SU	EL	ET	ER	EU	WL	WT	WR	WU	
7:00 AM	4	1	6	0	16	0	7	0	6	207	1	0	2	146	8	1	405
7:15 AM	1	0	16	0	6	2	8	0	6	343	2	0	4	175	9	1	573
7:30 AM	2	2	14	0	14	2	4	0	7	371	1	0	10	206	10	0	643
7:45 AM	1	1	15	0	11	1	5	0	12	342	4	0	10	229	17	2	650
8:00 AM	6	3	14	0	20	0	12	0	11	249	5	0	12	202	17	2	553
8:15 AM	2	1	16	0	22	0	9	0	9	205	5	0	16	186	16	0	487
8:30 AM	4	0	12	0	19	3	10	0	7	234	6	0	14	200	20	1	530
8:45 AM	6	2	12	0	17	3	7	0	6	203	6	0	7	191	10	1	471
TOTAL VOLUMES :	26	10	105	0	125	11	62	0	64	2154	30	0	75	1535	107	8	4312
APPROACH %'s :	18.44%	7.09%	74.47%	0.00%	63.13%	5.56%	31.31%	0.00%	2.85%	95.82%	1.33%	0.00%	4.35%	88.99%	6.20%	0.46%	
PEAK HR :	07:15 AM - 08:15 AM																TOTAL
PEAK HR VOL :	10	6	59	0	51	5	29	0	36	1305	12	0	36	812	53	5	2419
PEAK HR FACTOR :	0.417	0.500	0.922	0.000	0.638	0.625	0.604	0.000	0.750	0.879	0.600	0.000	0.750	0.886	0.779	0.625	0.930
	0.815				0.664				0.892				0.878				
NOON	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	TOTAL
	NL	NT	NR	NU	SL	ST	SR	SU	EL	ET	ER	EU	WL	WT	WR	WU	
12:00 PM	5	0	9	0	19	0	8	0	6	307	4	0	15	225	25	3	626
12:15 PM	7	0	17	0	27	0	7	0	7	231	4	0	17	258	22	4	601
12:30 PM	4	1	13	0	19	0	13	0	6	281	2	1	15	242	21	0	618
12:45 PM	7	0	9	0	18	1	12	0	7	226	4	0	15	293	32	2	626
1:00 PM	5	1	14	0	33	1	12	0	5	232	8	0	16	279	34	2	642
1:15 PM	8	1	13	0	27	1	5	0	4	246	5	0	17	281	21	2	631
1:30 PM	3	0	12	0	30	0	5	0	5	233	8	0	20	267	25	4	612
1:45 PM	2	1	17	0	23	1	4	0	3	247	1	0	10	279	20	0	608
TOTAL VOLUMES :	41	4	104	0	196	4	66	0	43	2003	36	1	125	2124	200	17	4964
APPROACH %'s :	27.52%	2.68%	69.80%	0.00%	73.68%	1.50%	24.81%	0.00%	2.06%	96.16%	1.73%	0.05%	5.07%	86.13%	8.11%	0.69%	
PEAK HR :	12:30 PM - 01:30 PM																TOTAL
PEAK HR VOL :	24	3	49	0	97	3	42	0	22	985	19	1	63	1095	108	6	2517
PEAK HR FACTOR :	0.750	0.750	0.875	0.000	0.735	0.750	0.808	0.000	0.786	0.876	0.594	0.250	0.926	0.934	0.794	0.750	0.980
	0.864				0.772				0.885				0.930				
PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	TOTAL
	NL	NT	NR	NU	SL	ST	SR	SU	EL	ET	ER	EU	WL	WT	WR	WU	
4:00 PM	4	0	14	0	16	0	7	0	3	262	3	0	11	336	29	3	688
4:15 PM	4	0	16	0	17	0	12	0	5	231	3	0	14	303	11	1	617
4:30 PM	4	0	12	0	21	2	6	0	7	233	7	0	10	284	21	0	607
4:45 PM	5	1	13	0	15	0	6	0	5	282	6	0	16	302	19	3	673
5:00 PM	11	1	15	0	15	2	13	0	6	312	6	0	13	330	19	4	747
5:15 PM	7	0	12	0	13	1	9	0	3	282	7	0	14	257	24	4	633
5:30 PM	13	0	13	0	17	0	10	0	6	196	8	0	18	340	25	0	646
5:45 PM	5	0	27	0	18	1	7	0	4	218	8	0	22	258	24	4	596
TOTAL VOLUMES :	53	2	122	0	132	6	70	0	39	2016	48	0	118	2410	172	19	5207
APPROACH %'s :	29.94%	1.13%	68.93%	0.00%	63.46%	2.88%	33.65%	0.00%	1.85%	95.86%	2.28%	0.00%	4.34%	88.64%	6.33%	0.70%	
PEAK HR :	04:45 PM - 05:45 PM																TOTAL
PEAK HR VOL :	36	2	53	0	60	3	38	0	20	1072	27	0	61	1229	87	11	2699
PEAK HR FACTOR :	0.692	0.500	0.883	0.000	0.882	0.375	0.731	0.000	0.833	0.859	0.844	0.000	0.847	0.904	0.870	0.688	0.903
	0.843				0.842				0.863				0.906				

National Data & Surveying Services Intersection Turning Movement Count

Location: Florida Gateway Dr & US Hwy 90
City: Lake City
Control: Signalized

Project ID: 21-120370-001
Date: 9/2/2021

Data - HT

NS/EW Streets:	Florida Gateway Dr				Florida Gateway Dr				US Hwy 90				US Hwy 90				
AM	NORTHBOUND				SOUTHBOUND				EASTBOUND				WESTBOUND				TOTAL
	NL	NT	NR	NU	SL	ST	SR	SU	EL	ET	ER	EU	WL	WT	WR	WU	
7:00 AM	0	0	1	0	0	0	0	0	0	8	0	0	0	3	0	0	12
7:15 AM	0	0	0	0	1	0	0	0	0	5	0	0	0	7	2	0	15
7:30 AM	0	0	0	0	0	0	0	0	1	7	0	0	0	9	1	0	18
7:45 AM	0	0	0	0	2	0	1	0	3	8	0	0	1	7	1	0	23
8:00 AM	0	0	0	0	3	0	0	0	0	6	0	0	0	7	0	0	16
8:15 AM	0	0	0	0	0	0	1	0	1	9	0	0	0	10	0	0	21
8:30 AM	0	0	0	0	0	0	1	0	0	5	0	0	2	11	1	0	20
8:45 AM	0	0	0	0	0	1	2	0	1	8	0	0	0	12	1	0	25
TOTAL VOLUMES :	0	0	1	0	6	1	5	0	6	56	0	0	3	66	6	0	150
APPROACH %'s :	0.00%	0.00%	100.00%	0.00%	50.00%	8.33%	41.67%	0.00%	9.68%	90.32%	0.00%	0.00%	4.00%	88.00%	8.00%	0.00%	
PEAK HR :	07:15 AM - 08:15 AM																TOTAL
PEAK HR VOL :	0	0	0	0	6	0	1	0	4	26	0	0	1	30	4	0	72
PEAK HR FACTOR :	0.000	0.000	0.000	0.000	0.500	0.000	0.250	0.000	0.333	0.813	0.000	0.000	0.250	0.833	0.500	0.000	0.783
					0.583				0.682				0.875				
NOON	NORTHBOUND				SOUTHBOUND				EASTBOUND				WESTBOUND				TOTAL
	NL	NT	NR	NU	SL	ST	SR	SU	EL	ET	ER	EU	WL	WT	WR	WU	
12:00 PM	0	0	1	0	4	0	0	0	0	11	0	0	1	5	1	0	23
12:15 PM	0	0	1	0	0	0	1	0	0	6	0	0	2	3	3	0	16
12:30 PM	0	1	0	0	2	0	0	0	0	9	1	0	0	10	0	0	23
12:45 PM	0	1	3	0	0	0	1	0	1	8	0	0	0	7	2	0	23
1:00 PM	2	0	2	0	1	0	1	0	0	4	1	0	0	12	0	0	23
1:15 PM	0	0	1	0	1	0	0	0	0	6	2	0	0	10	1	0	21
1:30 PM	0	0	0	0	0	0	0	0	0	10	1	0	1	6	1	0	19
1:45 PM	0	0	1	0	2	0	0	0	2	7	0	0	1	11	0	0	24
TOTAL VOLUMES :	2	2	9	0	10	0	3	0	3	61	5	0	5	64	8	0	172
APPROACH %'s :	15.38%	15.38%	69.23%	0.00%	76.92%	0.00%	23.08%	0.00%	4.35%	88.41%	7.25%	0.00%	6.49%	83.12%	10.39%	0.00%	
PEAK HR :	12:30 PM - 01:30 PM																TOTAL
PEAK HR VOL :	2	2	6	0	4	0	2	0	1	27	4	0	0	39	3	0	90
PEAK HR FACTOR :	0.250	0.500	0.500	0.000	0.500	0.000	0.500	0.000	0.250	0.750	0.500	0.000	0.000	0.813	0.375	0.000	0.978
					0.625				0.750				0.800				
									0.800								
PM	NORTHBOUND				SOUTHBOUND				EASTBOUND				WESTBOUND				TOTAL
	NL	NT	NR	NU	SL	ST	SR	SU	EL	ET	ER	EU	WL	WT	WR	WU	
4:00 PM	0	0	0	0	0	0	0	0	0	11	0	0	0	13	1	0	25
4:15 PM	0	0	0	0	1	0	0	0	0	6	0	0	1	11	1	0	20
4:30 PM	0	0	1	0	0	0	1	0	0	6	0	0	1	11	0	0	20
4:45 PM	0	0	1	0	0	0	0	0	0	5	0	0	1	8	0	0	15
5:00 PM	2	0	0	0	1	0	0	0	0	4	0	0	1	4	2	0	14
5:15 PM	0	0	0	0	0	0	0	0	0	8	0	0	1	8	0	0	17
5:30 PM	0	0	0	0	0	0	0	0	0	2	0	0	0	4	0	0	6
5:45 PM	0	0	0	0	0	0	0	0	0	5	0	0	0	7	4	0	16
TOTAL VOLUMES :	2	0	2	0	2	0	1	0	0	47	0	0	5	66	8	0	133
APPROACH %'s :	50.00%	0.00%	50.00%	0.00%	66.67%	0.00%	33.33%	0.00%	0.00%	100.00%	0.00%	0.00%	6.33%	83.54%	10.13%	0.00%	
PEAK HR :	04:45 PM - 05:45 PM																TOTAL
PEAK HR VOL :	2	0	1	0	1	0	0	0	0	19	0	0	3	24	2	0	52
PEAK HR FACTOR :	0.250	0.000	0.250	0.000	0.250	0.000	0.000	0.000	0.000	0.594	0.000	0.000	0.750	0.750	0.250	0.000	0.765
					0.375				0.250				0.594				
									0.594				0.806				

National Data & Surveying Services Intersection Turning Movement Count

Location: Florida Gateway Dr & US Hwy 90
 City: Lake City
 Control: Signalized

Project ID: 21-120370-001
 Date: 9/2/2021

Data - Bikes

NS/EW Streets:	Florida Gateway Dr				Florida Gateway Dr				US Hwy 90				US Hwy 90					
AM	NORTHBOUND				SOUTHBOUND				EASTBOUND				WESTBOUND				TOTAL	
	NL	NT	NR	NU	SL	ST	SR	SU	EL	ET	ER	EU	WL	WT	WR	WU		
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
7:15 AM	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	1
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1
7:45 AM	0	1	0	0	0	0	1	0	0	0	0	0	1	0	0	0	0	3
8:00 AM	0	0	0	0	1	0	0	0	0	1	0	0	0	0	0	0	0	2
8:15 AM	0	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	1
8:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
8:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
TOTAL VOLUMES :	0	1	0	0	1	0	1	0	0	3	0	0	1	1	0	0	0	8
APPROACH %'s :	0.00%	100.00%	0.00%	0.00%	50.00%	0.00%	50.00%	0.00%	0.00%	100.00%	0.00%	0.00%	50.00%	50.00%	0.00%	0.00%	0.00%	
PEAK HR :	07:15 AM - 08:15 AM																TOTAL	
PEAK HR VOL :	0	1	0	0	1	0	1	0	0	2	0	0	1	1	0	0	0	7
PEAK HR FACTOR :	0.000	0.250	0.000	0.000	0.250	0.000	0.250	0.000	0.000	0.500	0.000	0.000	0.250	0.250	0.000	0.000	0.000	0.583
				0.250			0.500			0.500					0.500			
NOON	NORTHBOUND				SOUTHBOUND				EASTBOUND				WESTBOUND				TOTAL	
NL	NT	NR	NU	SL	ST	SR	SU	EL	ET	ER	EU	WL	WT	WR	WU			
12:00 PM	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1
12:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
12:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1
12:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
1:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
TOTAL VOLUMES :	0	0	0	0	0	0	0	0	1	0	0	0	1	0	0	0	0	2
APPROACH %'s :	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	100.00%	0.00%	0.00%	0.00%	0.00%	100.00%	0.00%	0.00%	0.00%	
PEAK HR :	12:30 PM - 01:30 PM																TOTAL	
PEAK HR VOL :	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1
PEAK HR FACTOR :	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.000	0.250	0.000	0.000	0.000	0.000	0.250
													0.250					
PM	NORTHBOUND				SOUTHBOUND				EASTBOUND				WESTBOUND				TOTAL	
NL	NT	NR	NU	SL	ST	SR	SU	EL	ET	ER	EU	WL	WT	WR	WU			
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1
4:15 PM	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	1
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	0	1
5:30 PM	0	0	0	0	0	0	1	0	0	0	0	0	1	0	0	0	0	2
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
TOTAL VOLUMES :	0	0	0	0	0	0	2	0	1	0	0	0	3	0	0	0	0	6
APPROACH %'s :	0.00%	0.00%	0.00%	0.00%	0.00%	0.00%	100.00%	0.00%	0.00%	100.00%	0.00%	0.00%	0.00%	100.00%	0.00%	0.00%	0.00%	
PEAK HR :	04:45 PM - 05:45 PM																TOTAL	
PEAK HR VOL :	0	0	0	0	0	0	2	0	0	0	0	0	2	0	0	0	0	4
PEAK HR FACTOR :	0.000	0.000	0.000	0.000	0.000	0.000	0.500	0.000	0.000	0.000	0.000	0.000	0.500	0.000	0.000	0.000	0.000	0.500
							0.500						0.500					

National Data & Surveying Services Intersection Turning Movement Count

Location: Florida Gateway Dr & US Hwy 90
City: Lake City

Project ID: 21-120370-001
Date: 9/2/2021

Data - Pedestrians (Crosswalks)

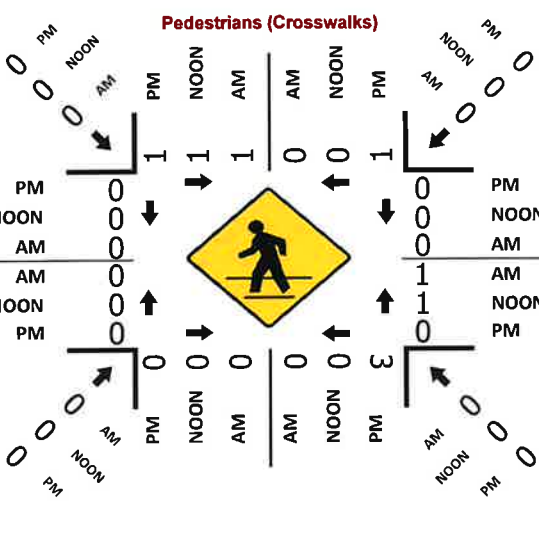
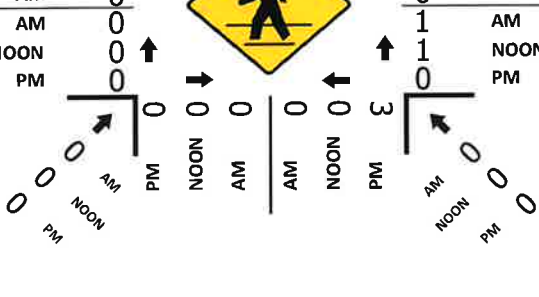
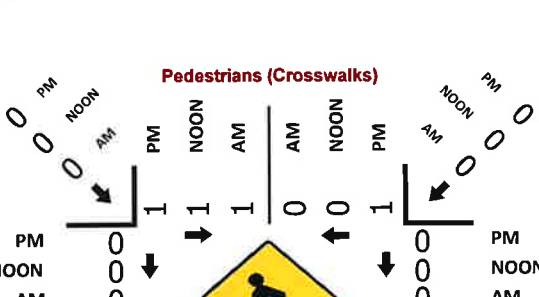
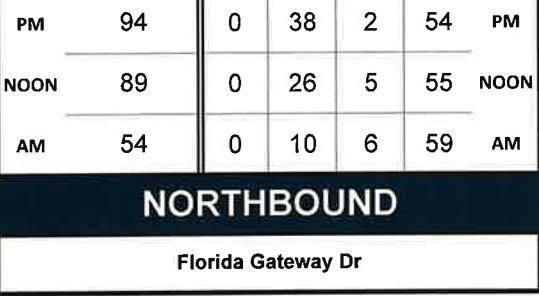
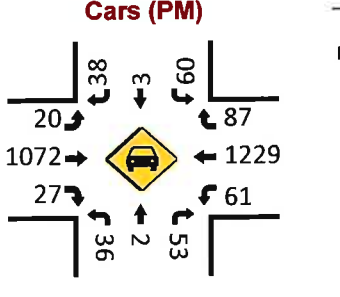
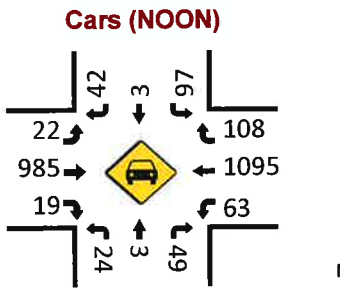
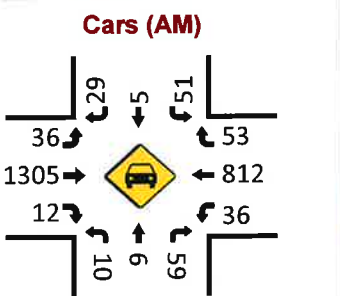
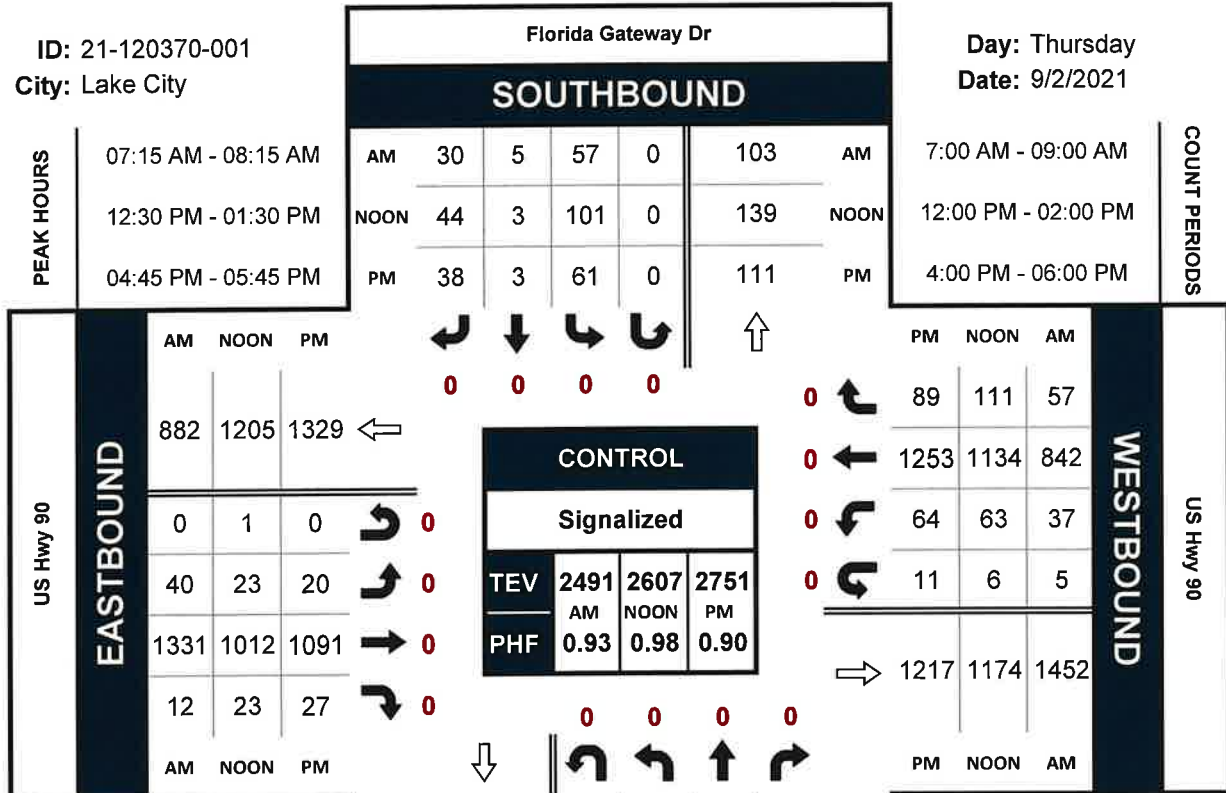
NS/EW Streets:	Florida Gateway Dr	Florida Gateway Dr	US Hwy 90	US Hwy 90					
AM	NORTH LEG		SOUTH LEG		EAST LEG		WEST LEG		TOTAL
	EB	WB	EB	WB	NB	SB	NB	SB	
7:00 AM	0	0	1	0	0	0	0	0	1
7:15 AM	0	0	0	0	0	0	0	0	0
7:30 AM	0	0	0	0	0	0	0	0	0
7:45 AM	1	0	0	0	0	0	0	0	1
8:00 AM	0	0	0	0	1	0	0	0	1
8:15 AM	0	0	0	0	0	0	0	0	0
8:30 AM	1	0	0	0	0	0	0	0	1
8:45 AM	0	0	0	0	0	0	0	0	0
TOTAL VOLUMES :	EB 2	WB 0	EB 1	WB 0	NB 1	SB 0	NB 0	SB 0	TOTAL 4
APPROACH %'s :	100.00%	0.00%	100.00%	0.00%	100.00%	0.00%			
PEAK HR :	07:15 AM - 08:15 AM								TOTAL 2
PEAK HR VOL :	1	0	0	0	1	0	0	0	
PEAK HR FACTOR :	0.250				0.250				0.500
	0.250				0.250				
NOON	NORTH LEG		SOUTH LEG		EAST LEG		WEST LEG		TOTAL
	EB	WB	EB	WB	NB	SB	NB	SB	
12:00 PM	0	1	0	0	0	0	0	0	1
12:15 PM	0	0	0	0	1	0	0	1	2
12:30 PM	0	0	0	0	0	0	0	0	0
12:45 PM	0	0	0	0	1	0	0	0	1
1:00 PM	0	0	0	0	0	0	0	0	0
1:15 PM	1	0	0	0	0	0	0	0	1
1:30 PM	0	0	0	0	0	0	0	0	0
1:45 PM	0	0	0	0	0	0	0	0	0
TOTAL VOLUMES :	EB 1	WB 1	EB 0	WB 0	NB 2	SB 0	NB 0	SB 1	TOTAL 5
APPROACH %'s :	50.00%	50.00%			100.00%	0.00%	0.00%	100.00%	
PEAK HR :	12:30 PM - 01:30 PM								TOTAL 2
PEAK HR VOL :	1	0	0	0	1	0	0	0	
PEAK HR FACTOR :	0.250				0.250				0.500
	0.250				0.250				
PM	NORTH LEG		SOUTH LEG		EAST LEG		WEST LEG		TOTAL
	EB	WB	EB	WB	NB	SB	NB	SB	
4:00 PM	0	0	0	1	0	0	0	0	1
4:15 PM	1	1	0	0	0	0	0	0	2
4:30 PM	1	0	2	1	0	0	0	0	4
4:45 PM	0	1	0	1	0	0	0	0	2
5:00 PM	0	0	0	2	0	0	0	0	2
5:15 PM	0	0	0	0	0	0	0	0	0
5:30 PM	1	0	0	0	0	0	0	0	1
5:45 PM	0	0	0	0	0	0	0	0	0
TOTAL VOLUMES :	EB 3	WB 2	EB 2	WB 5	NB 0	SB 0	NB 0	SB 0	TOTAL 12
APPROACH %'s :	60.00%	40.00%	28.57%	71.43%					
PEAK HR :	04:45 PM - 05:45 PM								TOTAL 5
PEAK HR VOL :	1	1	0	3	0	0	0	0	
PEAK HR FACTOR :	0.250	0.250		0.375					0.625
	0.500		0.375						

Florida Gateway Dr & US Hwy 90

Peak Hour Turning Movement Count

ID: 21-120370-001
City: Lake City

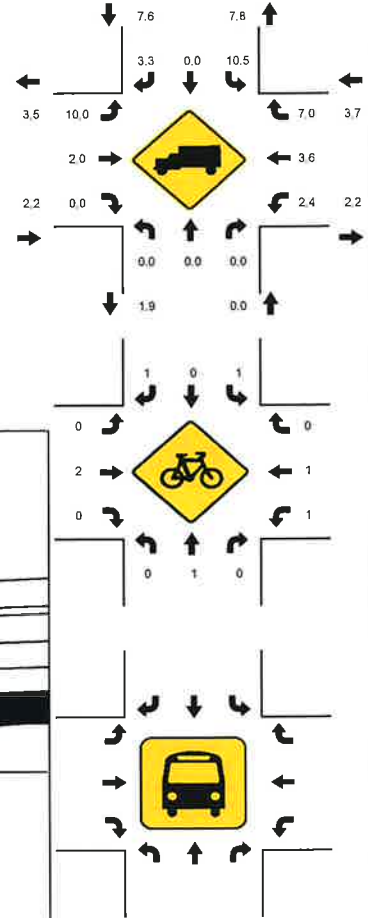
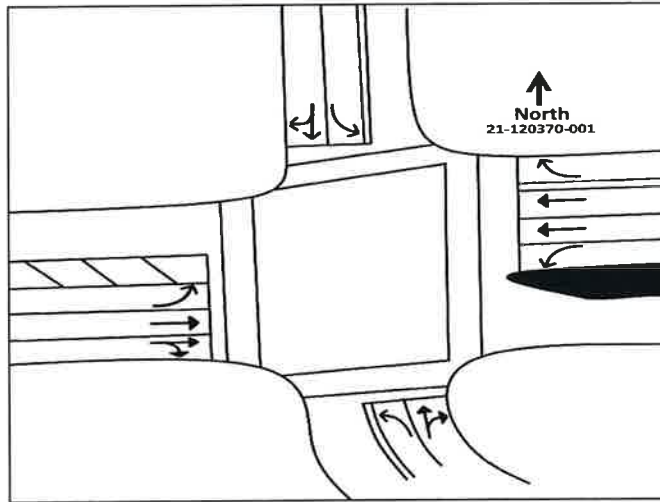
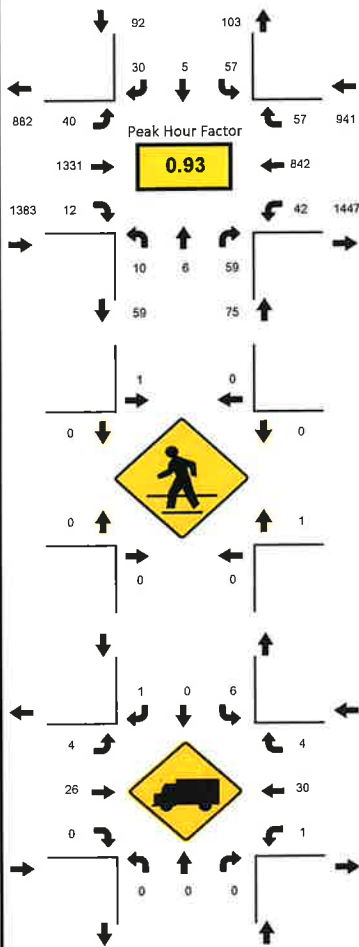
Day: Thursday
Date: 9/2/2021



Peak-Hour: 07:15 AM - 08:15 AM
 Peak 15-Minute: 07:45 AM - 08:00 AM

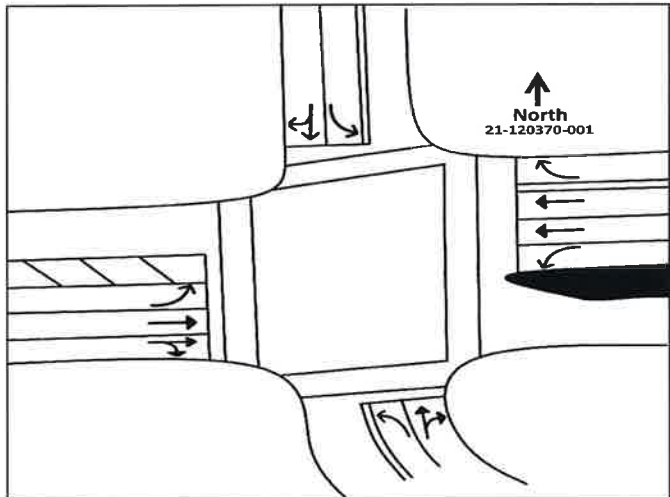
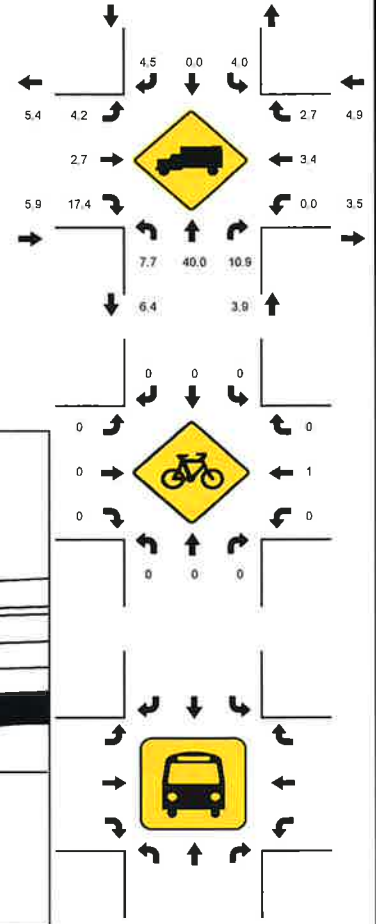
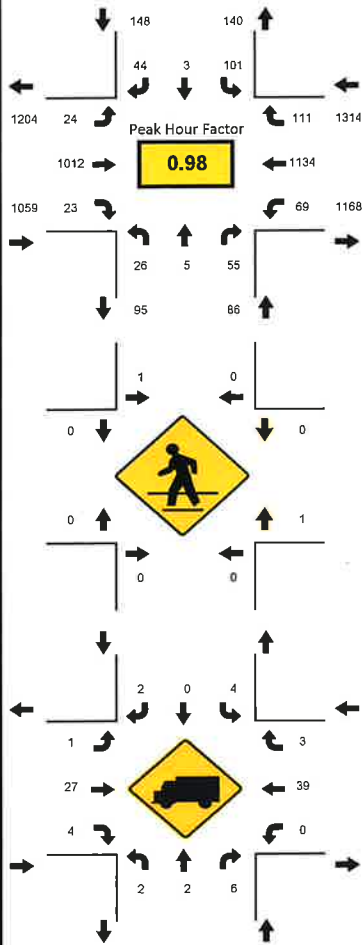


National Data & Surveying Services



15-Min Count Period Beginning At	Florida Gateway Dr Northbound					Florida Gateway Dr Southbound					US Hwy 90 Eastbound					US Hwy 90 Westbound					Total	Hourly Total
	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*		
07:00 AM	4	1	7	0		16	0	7	0		6	215	1	0		2	149	8	1		417	2339
07:15 AM	1	0	16	0		7	2	8	0		6	348	2	0		4	182	11	1		588	2491
07:30 AM	2	2	14	0		14	2	4	0		8	378	1	0		10	215	11	0		661	2411
07:45 AM	1	1	15	0		13	1	6	0		15	350	4	0		11	236	18	2		673	2300
08:00 AM	6	3	14	0		23	0	12	0		11	255	5	0		12	209	17	2		569	2123
08:15 AM	2	1	16	0		22	0	10	0		10	214	5	0		16	196	16	0		508	1554
08:30 AM	4	0	12	0		19	3	11	0		7	239	6	0		16	211	21	1		550	1046
08:45 AM	6	2	12	0		17	4	9	0		7	211	6	0		7	203	11	1		496	496
Peak 15-Min Flowrates	Northbound					Southbound					Eastbound					Westbound					Total	
All Vehicles	24	12	64	0		92	8	48	0		60	1512	20	0		48	944	72	8		2912	
Heavy Trucks	0	0	0	0		12	0	4	0		12	32	0	0		4	36	8	0		108	
Pedestrians							4					0					4				8	
Bicycles	0	4	0	0		4	0	4	0		0	4	0	0		4	4	0	0		24	
Buses																						
Stopped Buses																						

Peak-Hour: 12:30 PM - 01:30 PM
 Peak 15-Minute: 01:00 PM - 01:15 PM

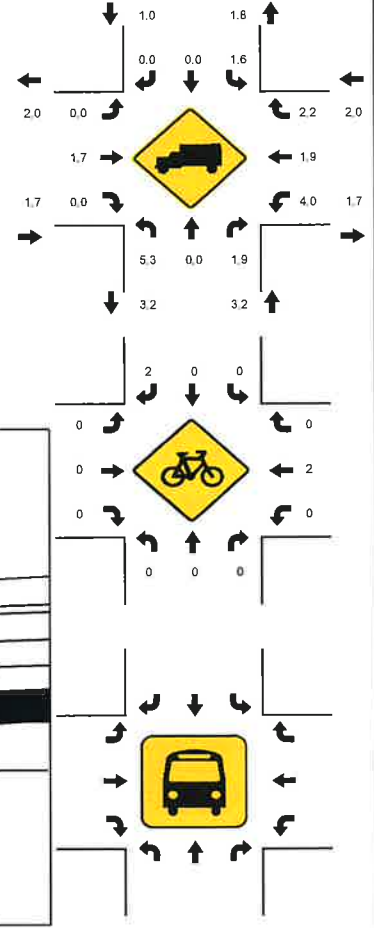
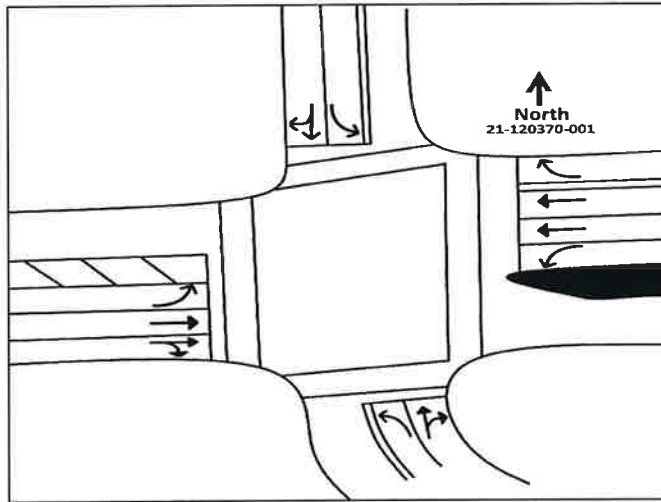
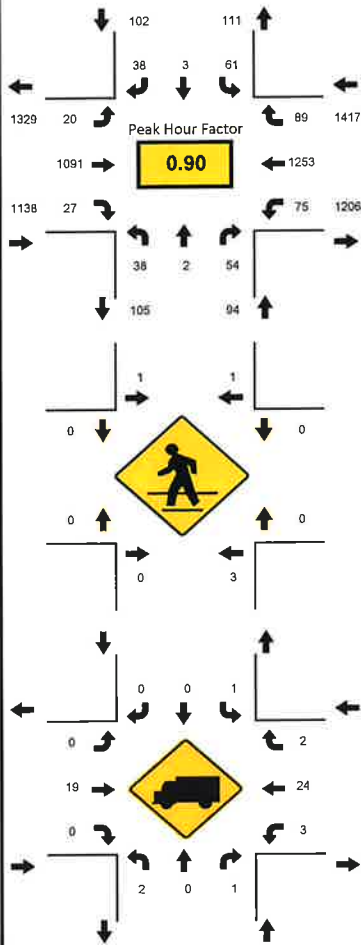


15-Min Count Period Beginning At	Florida Gateway Dr Northbound					Florida Gateway Dr Southbound					US Hwy 90 Eastbound					US Hwy 90 Westbound					Total	Hourly Total
	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*		
12:00 PM	5	0	10	0		23	0	8	0		6	318	4	0		16	230	26	3		649	2556
12:15 PM	7	0	18	0		27	0	8	0		7	237	4	0		19	261	25	4		617	2572
12:30 PM	4	2	13	0		21	0	13	0		6	290	3	1		15	252	21	0		641	2607
12:45 PM	7	1	12	0		18	1	13	0		8	234	4	0		15	300	34	2		649	2597
01:00 PM	7	1	16	0		34	1	13	0		5	236	9	0		16	291	34	2		665	2580
01:15 PM	8	1	14	0		28	1	5	0		4	252	7	0		17	291	22	2		652	1915
01:30 PM	3	0	12	0		30	0	5	0		5	243	9	0		21	273	26	4		631	1263
01:45 PM	2	1	18	0		25	1	4	0		5	254	1	0		11	290	20	0		632	632
Peak 15-Min Flowrates	Northbound					Southbound					Eastbound					Westbound					Total	
All Vehicles	32	8	64	0		136	4	52	0		32	1160	36	4		68	1200	136	8		2940	
Heavy Trucks	8	4	12	0		8	0	4	0		4	36	8	0		0	48	8	0		140	
Pedestrians		0					4					0					4				8	
Bicycles	0	0	0	0		0	0	0	0		0	0	0	0		0	4	0	0		4	
Buses																						
Stopped Buses																						

Peak-Hour: 04:45 PM - 05:45 PM
 Peak 15-Minute: 05:00 PM - 05:15 PM



National Data & Surveying Services



15-Min Count Period Beginning At	Florida Gateway Dr Northbound					Florida Gateway Dr Southbound					US Hwy 90 Eastbound					US Hwy 90 Westbound					Total	Hourly Total
	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*	Left	Thru	Rgt	U	R*		
04:00 PM	4	0	14	0		16	0	7	0		3	273	3	0		11	349	30	3		713	2665
04:15 PM	4	0	16	0		18	0	12	0		5	237	3	0		15	314	12	1		637	2713
04:30 PM	4	0	13	0		21	2	7	0		7	239	7	0		11	295	21	0		627	2726
04:45 PM	5	1	14	0		15	0	6	0		5	287	6	0		17	310	19	3		688	2751
05:00 PM	13	1	15	0		16	2	13	0		6	316	6	0		14	334	21	4		761	2675
05:15 PM	7	0	12	0		13	1	9	0		3	290	7	0		15	265	24	4		650	1914
05:30 PM	13	0	13	0		17	0	10	0		6	198	8	0		18	344	25	0		652	1264
05:45 PM	5	0	27	0		18	1	7	0		4	223	8	0		22	265	28	4		612	612
Peak 15-Min Flowrates	Northbound					Southbound					Eastbound					Westbound					Total	
All Vehicles	52	4	60	0		68	8	52	0		24	1264	32	0		72	1376	100	16		3128	
Heavy Trucks	8	0	4	0		4	0	0	0		0	32	0	0		4	32	8	0		92	
Pedestrians	8					4					0					0					12	
Bicycles	0	0	0	0		0	0	4	0		0	0	0	0		0	4	0	0		8	
Buses																						
Stopped Buses																						

2019 PEAK SEASON FACTOR CATEGORY REPORT - REPORT TYPE: ALL
 CATEGORY: 2900 COLUMBIA COUNTYWIDE

WEEK	DATES	SF	MOCF: 0.97 PSCF
1	01/01/2019 - 01/05/2019	1.02	1.05
2	01/06/2019 - 01/12/2019	1.05	1.08
3	01/13/2019 - 01/19/2019	1.08	1.11
4	01/20/2019 - 01/26/2019	1.06	1.09
5	01/27/2019 - 02/02/2019	1.04	1.07
6	02/03/2019 - 02/09/2019	1.03	1.06
7	02/10/2019 - 02/16/2019	1.01	1.04
8	02/17/2019 - 02/23/2019	1.00	1.03
9	02/24/2019 - 03/02/2019	0.99	1.02
10	03/03/2019 - 03/09/2019	0.98	1.01
*11	03/10/2019 - 03/16/2019	0.97	1.00
*12	03/17/2019 - 03/23/2019	0.97	1.00
*13	03/24/2019 - 03/30/2019	0.97	1.00
*14	03/31/2019 - 04/06/2019	0.97	1.00
*15	04/07/2019 - 04/13/2019	0.97	1.00
*16	04/14/2019 - 04/20/2019	0.97	1.00
*17	04/21/2019 - 04/27/2019	0.97	1.00
*18	04/28/2019 - 05/04/2019	0.97	1.00
*19	05/05/2019 - 05/11/2019	0.97	1.00
*20	05/12/2019 - 05/18/2019	0.97	1.00
*21	05/19/2019 - 05/25/2019	0.97	1.00
*22	05/26/2019 - 06/01/2019	0.97	1.00
*23	06/02/2019 - 06/08/2019	0.98	1.01
24	06/09/2019 - 06/15/2019	0.98	1.01
25	06/16/2019 - 06/22/2019	0.99	1.02
26	06/23/2019 - 06/29/2019	1.00	1.03
27	06/30/2019 - 07/06/2019	1.00	1.03
28	07/07/2019 - 07/13/2019	1.01	1.04
29	07/14/2019 - 07/20/2019	1.02	1.05
30	07/21/2019 - 07/27/2019	1.02	1.05
31	07/28/2019 - 08/03/2019	1.01	1.04
32	08/04/2019 - 08/10/2019	1.01	1.04
33	08/11/2019 - 08/17/2019	1.01	1.04
34	08/18/2019 - 08/24/2019	1.01	1.04
35	08/25/2019 - 08/31/2019	1.01	1.04
36	09/01/2019 - 09/07/2019	1.00	1.03
37	09/08/2019 - 09/14/2019	1.00	1.03
38	09/15/2019 - 09/21/2019	1.00	1.03
39	09/22/2019 - 09/28/2019	1.00	1.03
40	09/29/2019 - 10/05/2019	1.00	1.03
41	10/06/2019 - 10/12/2019	1.00	1.03
42	10/13/2019 - 10/19/2019	1.00	1.03
43	10/20/2019 - 10/26/2019	1.01	1.04
44	10/27/2019 - 11/02/2019	1.01	1.04
45	11/03/2019 - 11/09/2019	1.02	1.05
46	11/10/2019 - 11/16/2019	1.02	1.05
47	11/17/2019 - 11/23/2019	1.02	1.05
48	11/24/2019 - 11/30/2019	1.02	1.05
49	12/01/2019 - 12/07/2019	1.02	1.05
50	12/08/2019 - 12/14/2019	1.02	1.05
51	12/15/2019 - 12/21/2019	1.02	1.05
52	12/22/2019 - 12/28/2019	1.05	1.08
53	12/29/2019 - 12/31/2019	1.08	1.11

* PEAK SEASON

14-FEB-2020 15:39:21

830UPD

2_2900_PKSEASON.TXT

Location Details			
Signal ID:	1002	Date:	November 20, 2021
Major Street:	US 90	Orientation:	E-W
Minor Street:	FL Gateway Dr	Orientation:	N-S

Controller Timings (seconds)

Movement # (Controller Phase Ø)	Ø1	Ø2	Ø3	Ø4	Ø5	Ø6	Ø7	Ø8	Ø9	Ø10	Ø11	Ø12	Ø13	Ø14	Ø15	Ø16	Notes
Direction	EBLT	WB		NB	WBLT	EB		SB									
Turn Type	Prot Perm				Prot Perm												
Min Green	5	15		7	5	15		7									
Ext	3.0	4.0		3.0	3.0	4.0		3.0									
Yellow	4.8	4.9		3.8	4.9	4.9		3.8									
All Red	2.0	2.0		2.0	2.0	2.0		2.0									
Max I	15	75		20	15	75		20									
Max II																	
Walk		7		7		7		7									
Flashing Don't Walk		18		29		18		22									
Detector Memory																	
Det. Switching to:	Ø6				Ø2												
Recall		MIN				MIN											
CNA																	

Coordination Timings (seconds)

Pattern	C-S-O	Cycle Length	Splits																Offset	Seq	Coord Ø
			Ø1	Ø2	Ø3	Ø4	Ø5	Ø6	Ø7	Ø8	Ø9	Ø10	Ø11	Ø12	Ø13	Ø14	Ø15	Ø16			
1		130	15	91 MAX		24	16	90 MAX		24								24	1	2	
2		130	15	70 MAX		45	20	65 MAX		45								15	1	2	
3		150	15	88 MAX		47	25	78 MAX		47								20	1	2	
4		110	16	64 MAX		30	23	57 MAX		30								18	1	2	
5		100	15	59 MAX		26	17	57 MAX		26								22	1	2	
6		140	15	75 MAX		50	23	67 MAX		50								7	1	2	
7		110	17	58 MAX		35	18	57 MAX		35								63	1	2	
8		100	15	59 MAX		26	17	57 MAX		26								22	1	2	
9		140	15	75 MAX		50	23	67 MAX		50								7	1	2	
10		110	17	58 MAX		35	18	57 MAX		35								63	1	2	

Offset Reference Point	Phase Mode
End of Green of first through movement	STD8

- Notes:
- 1) Use 'Max I' during FREE Operation.
 - 2) Program phase restriction to omit Ø1 during Ø2 green and omit Ø5 during Ø6 green.

SEQ 1		
Ring - 1	1	2
Ring - 2	5	6

APPENDIX C
Intersection Volume Development Worksheets

TRAFFIC VOLUMES AT STUDY INTERSECTIONS

INTERSECTION: US 90 & Centurion Ct/Florida Gateway Dr
COUNT DATE: September 2, 2021
AM PEAK HOUR FACTOR: 0.93
PM PEAK HOUR FACTOR: 0.9

"AM EXISTING TRAFFIC"	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
AM Raw Turning Movements		40	1,331	12	5	37	842	57		10	6	59		57	5	30
Peak Season Conversion Factor	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03

AM EXISTING CONDITIONS		41	1,371	12	5	38	867	59		10	6	61		59	5	31
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"PM EXISTING TRAFFIC"	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
PM Raw Turning Movements		20	1,091	27	11	64	1,253	89		38	2	54		61	3	38
Peak Season Conversion Factor	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03	1.03

PM EXISTING CONDITIONS		21	1,124	28	11	66	1,291	92		39	2	56		63	3	39
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"AM BACKGROUND TRAFFIC"	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Years To Buildout	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2
Yearly Growth Rate	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%
AM BACKGROUND TRAFFIC GROWTH		2	58	1	0	2	37	3		0	0	3		3	0	1

AM NON-PROJECT TRAFFIC		43	1,429	13	5	40	904	62		10	6	64		62	5	32
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"PM BACKGROUND TRAFFIC"	EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
Years To Buildout	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2	2
Yearly Growth Rate	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%	2.1%
PM BACKGROUND TRAFFIC GROWTH		1	48	1	0	3	55	4		2	0	2		3	0	2

PM NON-PROJECT TRAFFIC		22	1,172	29	11	69	1,346	96		41	2	58		66	3	41
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"AM PROJECT DISTRIBUTION"		EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
LAND USE	TYPE																
Pass-By Distribution	Entering		50.0%	-50.0%				-50.0%	50.0%								
	Exiting														50.0%		50.0%
Net New Distribution	Entering		25.0%					75.0%									
	Exiting													75.0%			25.0%

"PM PROJECT DISTRIBUTION"		EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
LAND USE	TYPE																
Pass-By Distribution	Entering		50.0%	-50.0%				-50.0%	50.0%								
	Exiting														50.0%		50.0%
Net New Distribution	Entering		25.0%					75.0%									
	Exiting													75.0%			25.0%

"AM PROJECT TRAFFIC"		EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
LAND USE	TYPE																
Project Trips	Pass - By		12	-12				-12	12						12		12
	Net New		2					6							6		2
AM TOTAL PROJECT TRAFFIC			14	-12	0	0	0	-12	18		0	0	0		18	0	14

AM TOTAL TRAFFIC		57	1,417	13	5	40	892	80		10	6	64		80	5	46
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"PM PROJECT TRAFFIC"		EBU	EBL	EBT	EBR	WBU	WBL	WBT	WBR	NBU	NBL	NBT	NBR	SBU	SBL	SBT	SBR
LAND USE	TYPE																
Project Trips	Pass - By		13	-13				-14	14						13		14
	Net New		2					7							7		2
PM TOTAL PROJECT TRAFFIC			15	-13	0	0	0	-14	21		0	0	0		20	0	16

PM TOTAL TRAFFIC		37	1,159	29	11	69	1,332	117		41	2	58		86	3	57
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APPENDIX D
Synchro Output Reports

Lanes, Volumes, Timings
 1: SW Florida Gateway Dr/Centurion Ct & US 90

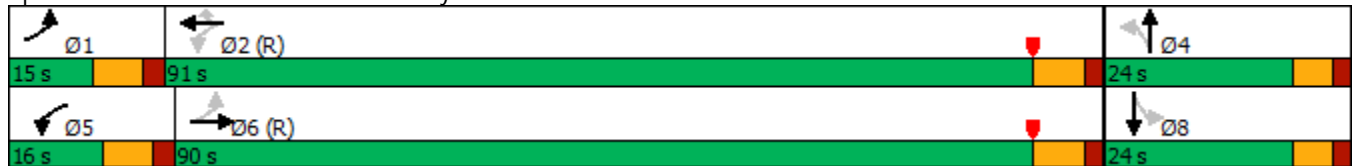
Circle K - I-75 & US 90
 Existing (2021) Conditions, AM Peak Hour

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	41	1371	12	43	867	59	10	6	61	59	5	31
Future Volume (vph)	41	1371	12	43	867	59	10	6	61	59	5	31
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	150		0	250		125	50		0	0		110
Storage Lanes	1		0	1		1	1		0	1		1
Taper Length (ft)	25			50			25			25		
Right Turn on Red			Yes			Yes			Yes			Yes
Link Speed (mph)		45			45			30				30
Link Distance (ft)		398			433			442				282
Travel Time (s)		6.0			6.6			10.0				6.4
Confl. Peds. (#/hr)	1						1			1	1	
Confl. Bikes (#/hr)			2				1			1		
Peak Hour Factor	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93
Heavy Vehicles (%)	2%	2%	2%	4%	4%	4%	2%	2%	2%	8%	8%	8%
Shared Lane Traffic (%)												
Turn Type	pm+pt	NA		pm+pt	NA	Perm	Perm	NA		Perm	NA	
Protected Phases	1	6		5	2			4				8
Permitted Phases	6			2		2	4			8		
Detector Phase	1	6		5	2	2	4	4		8		8
Switch Phase												
Minimum Initial (s)	5.0	15.0		5.0	15.0	15.0	7.0	7.0		7.0		7.0
Minimum Split (s)	11.8	31.9		11.9	31.9	31.9	41.8	41.8		34.8		34.8
Total Split (s)	15.0	90.0		16.0	91.0	91.0	24.0	24.0		24.0		24.0
Total Split (%)	11.5%	69.2%		12.3%	70.0%	70.0%	18.5%	18.5%		18.5%		18.5%
Yellow Time (s)	4.8	4.9		4.9	4.9	4.9	3.8	3.8		3.8		3.8
All-Red Time (s)	2.0	2.0		2.0	2.0	2.0	2.0	2.0		2.0		2.0
Lost Time Adjust (s)	0.0	0.0		0.0	0.0	0.0	0.0	0.0		0.0		0.0
Total Lost Time (s)	6.8	6.9		6.9	6.9	6.9	5.8	5.8		5.8		5.8
Lead/Lag	Lead	Lag		Lead	Lag	Lag						
Lead-Lag Optimize?	Yes	Yes		Yes	Yes	Yes						
Recall Mode	None	C-Min		None	C-Min	C-Min	None	None		None		None

Intersection Summary

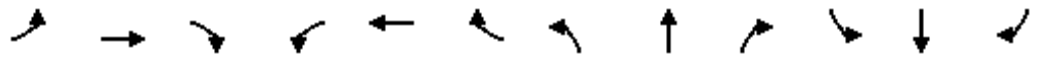
Area Type: Other
 Cycle Length: 130
 Actuated Cycle Length: 130
 Offset: 24 (18%), Referenced to phase 2:WBTL and 6:EBTL, Start of Yellow
 Natural Cycle: 100
 Control Type: Actuated-Coordinated

Splits and Phases: 1: SW Florida Gateway Dr/Centurion Ct & US 90



HCM 6th Signalized Intersection Summary
 1: SW Florida Gateway Dr/Centurion Ct & US 90

Circle K - I-75 & US 90
 Existing (2021) Conditions, AM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗	↗	↖	↗		↖	↗	
Traffic Volume (veh/h)	41	1371	12	43	867	59	10	6	61	59	5	31
Future Volume (veh/h)	41	1371	12	43	867	59	10	6	61	59	5	31
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		0.98	1.00		0.98	1.00		0.98	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1841	1841	1841	1870	1870	1870	1781	1781	1781
Adj Flow Rate, veh/h	44	1474	13	46	932	63	11	6	66	63	5	33
Peak Hour Factor	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93
Percent Heavy Veh, %	2	2	2	4	4	4	2	2	2	8	8	8
Cap, veh/h	453	2533	22	296	2485	1085	173	14	158	139	22	145
Arrive On Green	0.03	0.70	0.70	0.04	0.71	0.71	0.11	0.11	0.11	0.11	0.11	0.11
Sat Flow, veh/h	1781	3609	32	1753	3497	1527	1366	132	1449	1263	202	1335
Grp Volume(v), veh/h	44	725	762	46	932	63	11	0	72	63	0	38
Grp Sat Flow(s),veh/h/ln	1781	1777	1864	1753	1749	1527	1366	0	1580	1263	0	1537
Q Serve(g_s), s	0.9	26.7	26.8	0.9	13.7	1.6	1.0	0.0	5.5	6.4	0.0	2.9
Cycle Q Clear(g_c), s	0.9	26.7	26.8	0.9	13.7	1.6	3.9	0.0	5.5	11.9	0.0	2.9
Prop In Lane	1.00		0.02	1.00		1.00	1.00		0.92	1.00		0.87
Lane Grp Cap(c), veh/h	453	1247	1308	296	2485	1085	173	0	172	139	0	167
V/C Ratio(X)	0.10	0.58	0.58	0.16	0.38	0.06	0.06	0.00	0.42	0.45	0.00	0.23
Avail Cap(c_a), veh/h	510	1247	1308	351	2485	1085	216	0	221	178	0	215
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(l)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	5.4	9.8	9.8	7.7	7.4	5.7	54.7	0.0	54.1	59.6	0.0	52.9
Incr Delay (d2), s/veh	0.1	2.0	1.9	0.2	0.4	0.1	0.2	0.0	1.6	2.3	0.0	0.7
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	0.5	14.6	15.2	0.5	8.0	0.9	0.6	0.0	4.1	3.8	0.0	2.1
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	5.5	11.7	11.7	7.9	7.9	5.8	54.9	0.0	55.7	61.9	0.0	53.6
LnGrp LOS	A	B	B	A	A	A	D	A	E	E	A	D
Approach Vol, veh/h		1531			1041			83				101
Approach Delay, s/veh		11.5			7.7			55.6				58.8
Approach LOS		B			A			E				E
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	10.8	99.3		19.9	11.9	98.2		19.9				
Change Period (Y+Rc), s	6.8	6.9		* 5.8	6.9	6.9		* 5.8				
Max Green Setting (Gmax), s	8.2	84.1		* 18	9.1	83.1		* 18				
Max Q Clear Time (g_c+I1), s	2.9	15.7		7.5	2.9	28.8		13.9				
Green Ext Time (p_c), s	0.0	7.7		0.2	0.0	13.9		0.1				

Intersection Summary

HCM 6th Ctrl Delay	13.2
HCM 6th LOS	B

Notes

- User approved pedestrian interval to be less than phase max green.
- * HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Lanes, Volumes, Timings
 1: SW Florida Gateway Dr/Centurion Ct & US 90

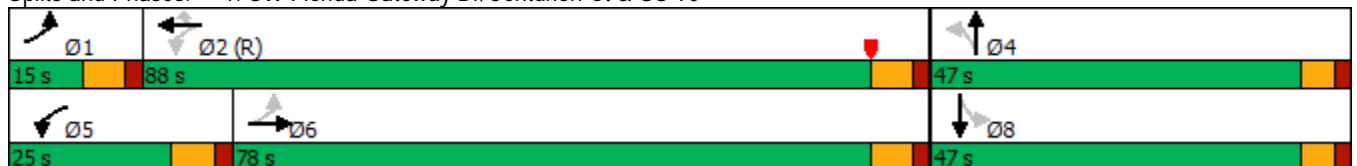
Circle K - I-75 & US 90
 Existing (2021) Conditions, PM Peak Hour

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	21	1124	28	77	1291	92	39	2	56	63	3	39
Future Volume (vph)	21	1124	28	77	1291	92	39	2	56	63	3	39
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	150		0	250		125	50		0	0		110
Storage Lanes	1		0	1		1	1		0	1		1
Taper Length (ft)	25			50			25			25		
Right Turn on Red			Yes			Yes			Yes			Yes
Link Speed (mph)		45			45			30				30
Link Distance (ft)		398			433			442				282
Travel Time (s)		6.0			6.6			10.0				6.4
Confl. Peds. (#/hr)	2		3	3		2						
Confl. Bikes (#/hr)						2						
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90
Heavy Vehicles (%)	2%	2%	2%	2%	2%	2%	3%	3%	3%	2%	2%	2%
Shared Lane Traffic (%)												
Turn Type	pm+pt	NA		pm+pt	NA	Perm	Perm	NA		Perm	NA	
Protected Phases	1	6		5	2			4				8
Permitted Phases	6			2		2	4			8		
Detector Phase	1	6		5	2	2	4	4		8		8
Switch Phase												
Minimum Initial (s)	5.0	15.0		5.0	15.0	15.0	7.0	7.0		7.0		7.0
Minimum Split (s)	11.8	31.9		11.9	31.9	31.9	41.8	41.8		34.8		34.8
Total Split (s)	15.0	78.0		25.0	88.0	88.0	47.0	47.0		47.0		47.0
Total Split (%)	10.0%	52.0%		16.7%	58.7%	58.7%	31.3%	31.3%		31.3%		31.3%
Yellow Time (s)	4.8	4.9		4.9	4.9	4.9	3.8	3.8		3.8		3.8
All-Red Time (s)	2.0	2.0		2.0	2.0	2.0	2.0	2.0		2.0		2.0
Lost Time Adjust (s)	0.0	0.0		0.0	0.0	0.0	0.0	0.0		0.0		0.0
Total Lost Time (s)	6.8	6.9		6.9	6.9	6.9	5.8	5.8		5.8		5.8
Lead/Lag	Lead	Lag		Lead	Lag	Lag						
Lead-Lag Optimize?	Yes	Yes		Yes	Yes	Yes						
Recall Mode	None	Max		None	C-Max	C-Max	None	None		None		None

Intersection Summary

Area Type: Other
 Cycle Length: 150
 Actuated Cycle Length: 150
 Offset: 20 (13%), Referenced to phase 2:WBTL, Start of Yellow
 Natural Cycle: 100
 Control Type: Actuated-Coordinated

Splits and Phases: 1: SW Florida Gateway Dr/Centurion Ct & US 90



HCM 6th Signalized Intersection Summary
 1: SW Florida Gateway Dr/Centurion Ct & US 90

Circle K - I-75 & US 90
 Existing (2021) Conditions, PM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗	↗	↖	↗		↖	↗	
Traffic Volume (veh/h)	21	1124	28	77	1291	92	39	2	56	63	3	39
Future Volume (veh/h)	21	1124	28	77	1291	92	39	2	56	63	3	39
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		0.98	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1856	1856	1856	1870	1870	1870
Adj Flow Rate, veh/h	23	1249	31	86	1434	102	43	2	62	70	3	43
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90
Percent Heavy Veh, %	2	2	2	2	2	2	3	3	3	2	2	2
Cap, veh/h	278	2589	64	360	2644	1152	154	5	161	138	11	158
Arrive On Green	0.02	0.73	0.73	0.03	0.74	0.74	0.11	0.11	0.11	0.11	0.11	0.11
Sat Flow, veh/h	1781	3543	88	1781	3554	1549	1349	49	1531	1338	104	1497
Grp Volume(v), veh/h	23	626	654	86	1434	102	43	0	64	70	0	46
Grp Sat Flow(s),veh/h/ln	1781	1777	1854	1781	1777	1549	1349	0	1580	1338	0	1601
Q Serve(g_s), s	0.5	22.0	22.0	1.8	26.0	2.7	4.5	0.0	5.7	7.7	0.0	4.0
Cycle Q Clear(g_c), s	0.5	22.0	22.0	1.8	26.0	2.7	8.5	0.0	5.7	13.4	0.0	4.0
Prop In Lane	1.00		0.05	1.00		1.00	1.00		0.97	1.00		0.93
Lane Grp Cap(c), veh/h	278	1298	1355	360	2644	1152	154	0	167	138	0	169
V/C Ratio(X)	0.08	0.48	0.48	0.24	0.54	0.09	0.28	0.00	0.38	0.51	0.00	0.27
Avail Cap(c_a), veh/h	338	1298	1355	515	2644	1152	383	0	434	365	0	440
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	6.8	8.4	8.4	6.4	8.2	5.3	65.7	0.0	62.6	68.8	0.0	61.8
Incr Delay (d2), s/veh	0.1	1.3	1.2	0.3	0.8	0.2	1.0	0.0	1.4	2.8	0.0	0.9
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	0.3	12.5	12.9	1.1	13.7	1.5	2.9	0.0	4.2	5.0	0.0	3.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	6.9	9.7	9.6	6.7	9.0	5.4	66.7	0.0	64.0	71.6	0.0	62.7
LnGrp LOS	A	A	A	A	A	A	E	A	E	E	A	E
Approach Vol, veh/h		1303			1622			107				116
Approach Delay, s/veh		9.6			8.7			65.1				68.1
Approach LOS		A			A			E				E
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	9.9	118.5		21.6	11.9	116.5		21.6				
Change Period (Y+Rc), s	6.8	6.9		* 5.8	6.9	6.9		* 5.8				
Max Green Setting (Gmax), s	8.2	81.1		* 41	18.1	71.1		* 41				
Max Q Clear Time (g_c+I1), s	2.5	28.0		10.5	3.8	24.0		15.4				
Green Ext Time (p_c), s	0.0	15.2		0.5	0.1	10.3		0.4				

Intersection Summary

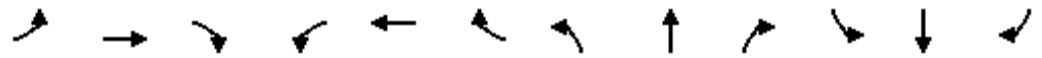
HCM 6th Ctrl Delay	13.2
HCM 6th LOS	B

Notes

- User approved pedestrian interval to be less than phase max green.
- * HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Lanes, Volumes, Timings
 1: SW Florida Gateway Dr/Centurion Ct & US 90

Circle K - I-75 & US 90
 Background (2023) Conditions, AM Peak Hour

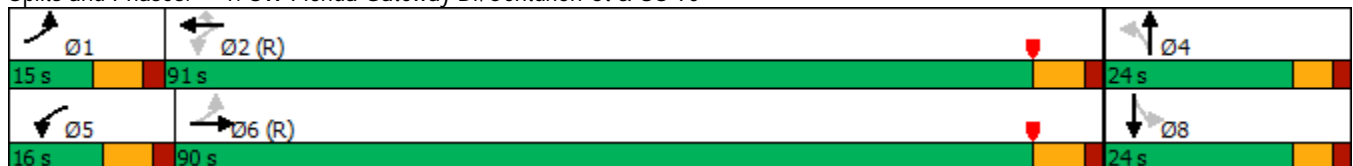


Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↕		↖	↕	↗	↖	↗		↖	↗	
Traffic Volume (vph)	43	1429	13	45	904	62	10	6	64	62	5	32
Future Volume (vph)	43	1429	13	45	904	62	10	6	64	62	5	32
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	150		0	250		125	50		0	0		110
Storage Lanes	1		0	1		1	1		0	1		1
Taper Length (ft)	25			50			25			25		
Right Turn on Red			Yes			Yes			Yes			Yes
Link Speed (mph)		45			45			30				30
Link Distance (ft)		398			433			442				282
Travel Time (s)		6.0			6.6			10.0				6.4
Confl. Peds. (#/hr)	1						1			1	1	
Confl. Bikes (#/hr)			2						1			
Peak Hour Factor	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93
Heavy Vehicles (%)	2%	2%	2%	4%	4%	4%	2%	2%	2%	8%	8%	8%
Shared Lane Traffic (%)												
Turn Type	pm+pt	NA		pm+pt	NA	Perm	Perm	NA		Perm	NA	
Protected Phases	1	6		5	2			4				8
Permitted Phases	6			2		2	4			8		
Detector Phase	1	6		5	2	2	4	4		8		8
Switch Phase												
Minimum Initial (s)	5.0	15.0		5.0	15.0	15.0	7.0	7.0		7.0		7.0
Minimum Split (s)	11.8	31.9		11.9	31.9	31.9	41.8	41.8		34.8		34.8
Total Split (s)	15.0	90.0		16.0	91.0	91.0	24.0	24.0		24.0		24.0
Total Split (%)	11.5%	69.2%		12.3%	70.0%	70.0%	18.5%	18.5%		18.5%		18.5%
Yellow Time (s)	4.8	4.9		4.9	4.9	4.9	3.8	3.8		3.8		3.8
All-Red Time (s)	2.0	2.0		2.0	2.0	2.0	2.0	2.0		2.0		2.0
Lost Time Adjust (s)	0.0	0.0		0.0	0.0	0.0	0.0	0.0		0.0		0.0
Total Lost Time (s)	6.8	6.9		6.9	6.9	6.9	5.8	5.8		5.8		5.8
Lead/Lag	Lead	Lag		Lead	Lag	Lag						
Lead-Lag Optimize?	Yes	Yes		Yes	Yes	Yes						
Recall Mode	None	C-Min		None	C-Min	C-Min	None	None		None		None

Intersection Summary

Area Type: Other
 Cycle Length: 130
 Actuated Cycle Length: 130
 Offset: 24 (18%), Referenced to phase 2:WBTL and 6:EBTL, Start of Yellow
 Natural Cycle: 100
 Control Type: Actuated-Coordinated

Splits and Phases: 1: SW Florida Gateway Dr/Centurion Ct & US 90



HCM 6th Signalized Intersection Summary
 1: SW Florida Gateway Dr/Centurion Ct & US 90

Circle K - I-75 & US 90
 Background (2023) Conditions, AM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗	↗	↖	↗		↖	↗	
Traffic Volume (veh/h)	43	1429	13	45	904	62	10	6	64	62	5	32
Future Volume (veh/h)	43	1429	13	45	904	62	10	6	64	62	5	32
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		0.98	1.00		1.00	1.00		0.98	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1841	1841	1841	1870	1870	1870	1781	1781	1781
Adj Flow Rate, veh/h	46	1537	14	48	972	67	11	6	69	67	5	34
Peak Hour Factor	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93
Percent Heavy Veh, %	2	2	2	4	4	4	2	2	2	8	8	8
Cap, veh/h	432	2515	23	278	2467	1099	179	14	165	143	22	152
Arrive On Green	0.03	0.70	0.70	0.04	0.71	0.71	0.11	0.11	0.11	0.11	0.11	0.11
Sat Flow, veh/h	1781	3608	33	1753	3497	1559	1365	126	1453	1259	197	1339
Grp Volume(v), veh/h	46	757	794	48	972	67	11	0	75	67	0	39
Grp Sat Flow(s),veh/h/ln	1781	1777	1864	1753	1749	1559	1365	0	1580	1259	0	1536
Q Serve(g_s), s	0.9	29.2	29.2	1.0	14.7	1.7	1.0	0.0	5.7	6.8	0.0	3.0
Cycle Q Clear(g_c), s	0.9	29.2	29.2	1.0	14.7	1.7	4.0	0.0	5.7	12.5	0.0	3.0
Prop In Lane	1.00		0.02	1.00		1.00	1.00		0.92	1.00		0.87
Lane Grp Cap(c), veh/h	432	1239	1299	278	2467	1099	179	0	179	143	0	174
V/C Ratio(X)	0.11	0.61	0.61	0.17	0.39	0.06	0.06	0.00	0.42	0.47	0.00	0.22
Avail Cap(c_a), veh/h	489	1239	1299	333	2467	1099	215	0	221	176	0	215
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	5.7	10.4	10.4	8.5	7.8	5.9	54.2	0.0	53.6	59.5	0.0	52.4
Incr Delay (d2), s/veh	0.1	2.2	2.2	0.3	0.5	0.1	0.1	0.0	1.5	2.4	0.0	0.6
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	0.5	15.8	16.4	0.6	8.6	0.9	0.6	0.0	4.3	4.1	0.0	2.2
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	5.8	12.6	12.5	8.8	8.3	6.0	54.4	0.0	55.2	61.8	0.0	53.1
LnGrp LOS	A	B	B	A	A	A	D	A	E	E	A	D
Approach Vol, veh/h		1597			1087			86			106	
Approach Delay, s/veh		12.4			8.2			55.1			58.6	
Approach LOS		B			A			E			E	
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	10.9	98.6		20.6	11.9	97.5		20.6				
Change Period (Y+Rc), s	6.8	6.9		* 5.8	6.9	6.9		* 5.8				
Max Green Setting (Gmax), s	8.2	84.1		* 18	9.1	83.1		* 18				
Max Q Clear Time (g_c+I1), s	2.9	16.7		7.7	3.0	31.2		14.5				
Green Ext Time (p_c), s	0.0	8.2		0.2	0.0	15.0		0.1				

Intersection Summary

HCM 6th Ctrl Delay	13.8
HCM 6th LOS	B

Notes

- User approved pedestrian interval to be less than phase max green.
- * HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Lanes, Volumes, Timings
 1: SW Florida Gateway Dr/Centurion Ct & US 90

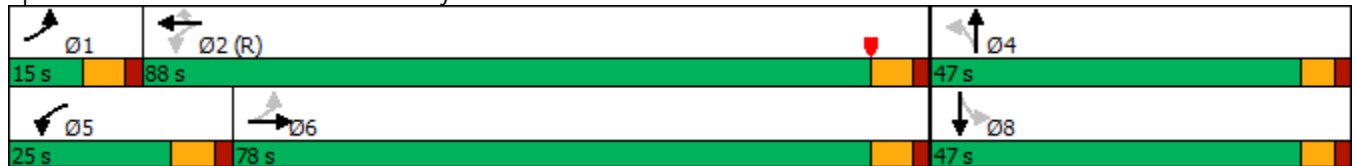
Circle K - I-75 & US 90
 Background (2023) Conditions, PM Peak Hour

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	22	1172	29	80	1346	96	41	2	58	66	3	41
Future Volume (vph)	22	1172	29	80	1346	96	41	2	58	66	3	41
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	150		0	250		125	50		0	0		110
Storage Lanes	1		0	1		1	1		0	1		1
Taper Length (ft)	25			50			25			25		
Right Turn on Red			Yes			Yes			Yes			Yes
Link Speed (mph)		45			45			30				30
Link Distance (ft)		398			433			442				282
Travel Time (s)		6.0			6.6			10.0				6.4
Confl. Peds. (#/hr)	2		3	3		2						
Confl. Bikes (#/hr)						2						
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90
Heavy Vehicles (%)	2%	2%	2%	2%	2%	2%	3%	3%	3%	2%	2%	2%
Shared Lane Traffic (%)												
Turn Type	pm+pt	NA		pm+pt	NA	Perm	Perm	NA		Perm	NA	
Protected Phases	1	6		5	2			4				8
Permitted Phases	6			2		2	4			8		
Detector Phase	1	6		5	2	2	4	4		8		8
Switch Phase												
Minimum Initial (s)	5.0	15.0		5.0	15.0	15.0	7.0	7.0		7.0		7.0
Minimum Split (s)	11.8	31.9		11.9	31.9	31.9	41.8	41.8		34.8		34.8
Total Split (s)	15.0	78.0		25.0	88.0	88.0	47.0	47.0		47.0		47.0
Total Split (%)	10.0%	52.0%		16.7%	58.7%	58.7%	31.3%	31.3%		31.3%		31.3%
Yellow Time (s)	4.8	4.9		4.9	4.9	4.9	3.8	3.8		3.8		3.8
All-Red Time (s)	2.0	2.0		2.0	2.0	2.0	2.0	2.0		2.0		2.0
Lost Time Adjust (s)	0.0	0.0		0.0	0.0	0.0	0.0	0.0		0.0		0.0
Total Lost Time (s)	6.8	6.9		6.9	6.9	6.9	5.8	5.8		5.8		5.8
Lead/Lag	Lead	Lag		Lead	Lag	Lag						
Lead-Lag Optimize?	Yes	Yes		Yes	Yes	Yes						
Recall Mode	None	Max		None	C-Max	C-Max	None	None		None		None

Intersection Summary

Area Type: Other
 Cycle Length: 150
 Actuated Cycle Length: 150
 Offset: 20 (13%), Referenced to phase 2:WBTL, Start of Yellow
 Natural Cycle: 100
 Control Type: Actuated-Coordinated

Splits and Phases: 1: SW Florida Gateway Dr/Centurion Ct & US 90



HCM 6th Signalized Intersection Summary
 1: SW Florida Gateway Dr/Centurion Ct & US 90

Circle K - I-75 & US 90
 Background (2023) Conditions, PM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗	↗	↖	↗		↖	↗	
Traffic Volume (veh/h)	22	1172	29	80	1346	96	41	2	58	66	3	41
Future Volume (veh/h)	22	1172	29	80	1346	96	41	2	58	66	3	41
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		0.98	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1856	1856	1856	1870	1870	1870
Adj Flow Rate, veh/h	24	1302	32	89	1496	107	46	2	64	73	3	46
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90
Percent Heavy Veh, %	2	2	2	2	2	2	3	3	3	2	2	2
Cap, veh/h	260	2577	63	341	2630	1146	157	5	167	142	11	164
Arrive On Green	0.02	0.73	0.73	0.03	0.74	0.74	0.11	0.11	0.11	0.11	0.11	0.11
Sat Flow, veh/h	1781	3544	87	1781	3554	1549	1345	48	1532	1335	98	1502
Grp Volume(v), veh/h	24	652	682	89	1496	107	46	0	66	73	0	49
Grp Sat Flow(s),veh/h/ln	1781	1777	1854	1781	1777	1549	1345	0	1580	1335	0	1600
Q Serve(g_s), s	0.5	23.8	23.8	1.9	28.4	2.9	4.9	0.0	5.8	8.1	0.0	4.2
Cycle Q Clear(g_c), s	0.5	23.8	23.8	1.9	28.4	2.9	9.1	0.0	5.8	13.9	0.0	4.2
Prop In Lane	1.00		0.05	1.00		1.00	1.00		0.97	1.00		0.94
Lane Grp Cap(c), veh/h	260	1292	1348	341	2630	1146	157	0	172	142	0	174
V/C Ratio(X)	0.09	0.50	0.51	0.26	0.57	0.09	0.29	0.00	0.38	0.52	0.00	0.28
Avail Cap(c_a), veh/h	320	1292	1348	496	2630	1146	380	0	434	363	0	439
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	7.3	8.8	8.8	6.9	8.8	5.4	65.6	0.0	62.1	68.6	0.0	61.4
Incr Delay (d2), s/veh	0.2	1.4	1.4	0.4	0.9	0.2	1.0	0.0	1.4	2.9	0.0	0.9
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	0.3	13.4	13.8	1.1	14.9	1.6	3.1	0.0	4.4	5.2	0.0	3.2
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	7.5	10.2	10.2	7.3	9.7	5.6	66.6	0.0	63.5	71.5	0.0	62.3
LnGrp LOS	A	B	B	A	A	A	E	A	E	E	A	E
Approach Vol, veh/h		1358			1692			112				122
Approach Delay, s/veh		10.2			9.3			64.8				67.8
Approach LOS		B			A			E				E
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	10.0	117.9		22.1	11.9	116.0		22.1				
Change Period (Y+Rc), s	6.8	6.9		* 5.8	6.9	6.9		* 5.8				
Max Green Setting (Gmax), s	8.2	81.1		* 41	18.1	71.1		* 41				
Max Q Clear Time (g_c+I1), s	2.5	30.4		11.1	3.9	25.8		15.9				
Green Ext Time (p_c), s	0.0	16.3		0.5	0.1	11.0		0.5				

Intersection Summary

HCM 6th Ctrl Delay	13.7
HCM 6th LOS	B

Notes

- User approved pedestrian interval to be less than phase max green.
- * HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Lanes, Volumes, Timings
1: SW Florida Gateway Dr/Centurion Ct & US 90

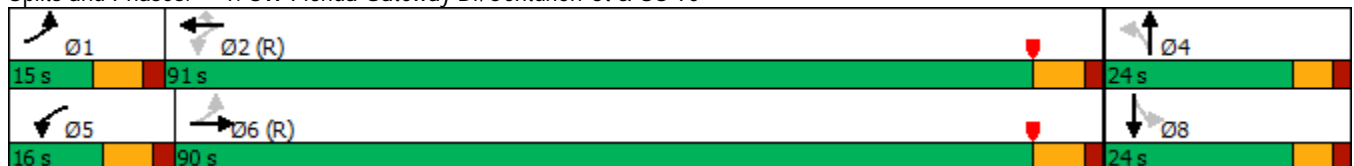
Circle K - I-75 & US 90
Buildout (2023) Conditions, AM Peak Hour

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	57	1417	13	45	892	80	10	6	64	80	5	46
Future Volume (vph)	57	1417	13	45	892	80	10	6	64	80	5	46
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	150		0	250		125	50		0	0		110
Storage Lanes	1		0	1		1	1		0	1		1
Taper Length (ft)	25			50			25			25		
Right Turn on Red			Yes			Yes			Yes			Yes
Link Speed (mph)		45			45			30				30
Link Distance (ft)		398			433			442				282
Travel Time (s)		6.0			6.6			10.0				6.4
Confl. Peds. (#/hr)	1						1			1	1	
Confl. Bikes (#/hr)			2							1		
Peak Hour Factor	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93
Heavy Vehicles (%)	2%	2%	2%	4%	4%	4%	2%	2%	2%	8%	8%	8%
Shared Lane Traffic (%)												
Turn Type	pm+pt	NA		pm+pt	NA	Perm	Perm	NA		Perm	NA	
Protected Phases	1	6		5	2			4				8
Permitted Phases	6			2		2	4			8		
Detector Phase	1	6		5	2	2	4	4		8		8
Switch Phase												
Minimum Initial (s)	5.0	15.0		5.0	15.0	15.0	7.0	7.0		7.0		7.0
Minimum Split (s)	11.8	31.9		11.9	31.9	31.9	41.8	41.8		34.8		34.8
Total Split (s)	15.0	90.0		16.0	91.0	91.0	24.0	24.0		24.0		24.0
Total Split (%)	11.5%	69.2%		12.3%	70.0%	70.0%	18.5%	18.5%		18.5%		18.5%
Yellow Time (s)	4.8	4.9		4.9	4.9	4.9	3.8	3.8		3.8		3.8
All-Red Time (s)	2.0	2.0		2.0	2.0	2.0	2.0	2.0		2.0		2.0
Lost Time Adjust (s)	0.0	0.0		0.0	0.0	0.0	0.0	0.0		0.0		0.0
Total Lost Time (s)	6.8	6.9		6.9	6.9	6.9	5.8	5.8		5.8		5.8
Lead/Lag	Lead	Lag		Lead	Lag	Lag						
Lead-Lag Optimize?	Yes	Yes		Yes	Yes	Yes						
Recall Mode	None	C-Min		None	C-Min	C-Min	None	None		None		None

Intersection Summary

Area Type: Other
 Cycle Length: 130
 Actuated Cycle Length: 130
 Offset: 24 (18%), Referenced to phase 2:WBTL and 6:EBTL, Start of Yellow
 Natural Cycle: 100
 Control Type: Actuated-Coordinated

Splits and Phases: 1: SW Florida Gateway Dr/Centurion Ct & US 90



HCM 6th Signalized Intersection Summary
 1: SW Florida Gateway Dr/Centurion Ct & US 90

Circle K - I-75 & US 90
 Buildout (2023) Conditions, AM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗	↗	↖	↗		↖	↗	
Traffic Volume (veh/h)	57	1417	13	45	892	80	10	6	64	80	5	46
Future Volume (veh/h)	57	1417	13	45	892	80	10	6	64	80	5	46
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		0.98	1.00		1.00	1.00		0.98	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1841	1841	1841	1870	1870	1870	1781	1781	1781
Adj Flow Rate, veh/h	61	1524	14	48	959	86	11	6	69	86	5	49
Peak Hour Factor	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93
Percent Heavy Veh, %	2	2	2	4	4	4	2	2	2	8	8	8
Cap, veh/h	425	2466	23	273	2409	1074	184	16	185	161	18	176
Arrive On Green	0.03	0.68	0.68	0.04	0.69	0.69	0.13	0.13	0.13	0.13	0.13	0.13
Sat Flow, veh/h	1781	3607	33	1753	3497	1559	1347	126	1454	1259	141	1387
Grp Volume(v), veh/h	61	750	788	48	959	86	11	0	75	86	0	54
Grp Sat Flow(s),veh/h/ln	1781	1777	1864	1753	1749	1559	1347	0	1581	1259	0	1528
Q Serve(g_s), s	1.3	30.1	30.1	1.0	15.3	2.4	1.0	0.0	5.7	8.7	0.0	4.2
Cycle Q Clear(g_c), s	1.3	30.1	30.1	1.0	15.3	2.4	5.1	0.0	5.7	14.4	0.0	4.2
Prop In Lane	1.00		0.02	1.00		1.00	1.00		0.92	1.00		0.91
Lane Grp Cap(c), veh/h	425	1215	1274	273	2409	1074	184	0	201	161	0	194
V/C Ratio(X)	0.14	0.62	0.62	0.18	0.40	0.08	0.06	0.00	0.37	0.54	0.00	0.28
Avail Cap(c_a), veh/h	476	1215	1274	328	2409	1074	201	0	221	177	0	214
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(l)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	6.3	11.3	11.3	9.2	8.7	6.7	53.7	0.0	52.0	58.6	0.0	51.3
Incr Delay (d2), s/veh	0.2	2.4	2.3	0.3	0.5	0.1	0.1	0.0	1.1	2.7	0.0	0.8
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	0.8	16.5	17.1	0.6	9.0	1.3	0.6	0.0	4.2	5.2	0.0	3.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	6.4	13.6	13.5	9.5	9.2	6.8	53.8	0.0	53.1	61.3	0.0	52.1
LnGrp LOS	A	B	B	A	A	A	D	A	D	E	A	D
Approach Vol, veh/h		1599			1093			86			140	
Approach Delay, s/veh		13.3			9.0			53.2			57.8	
Approach LOS		B			A			D			E	
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	11.2	96.4		22.3	11.9	95.8		22.3				
Change Period (Y+Rc), s	6.8	6.9		* 5.8	6.9	6.9		* 5.8				
Max Green Setting (Gmax), s	8.2	84.1		* 18	9.1	83.1		* 18				
Max Q Clear Time (g_c+I1), s	3.3	17.3		7.7	3.0	32.1		16.4				
Green Ext Time (p_c), s	0.0	8.1		0.2	0.0	14.7		0.1				

Intersection Summary

HCM 6th Ctrl Delay	15.0
HCM 6th LOS	B

Notes

- User approved pedestrian interval to be less than phase max green.
- * HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Lanes, Volumes, Timings
1: SW Florida Gateway Dr/Centurion Ct & US 90

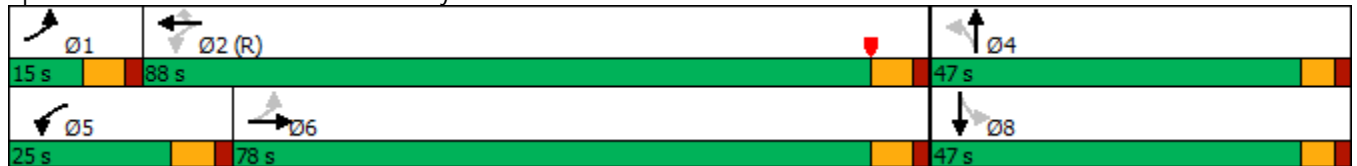
Circle K - I-75 & US 90
Buildout (2023) Conditions, PM Peak Hour

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	37	1159	29	80	1332	117	41	2	58	86	3	57
Future Volume (vph)	37	1159	29	80	1332	117	41	2	58	86	3	57
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Storage Length (ft)	150		0	250		125	50		0	0		110
Storage Lanes	1		0	1		1	1		0	1		1
Taper Length (ft)	25			50			25			25		
Right Turn on Red			Yes			Yes			Yes			Yes
Link Speed (mph)		45			45			30				30
Link Distance (ft)		398			433			442				282
Travel Time (s)		6.0			6.6			10.0				6.4
Confl. Peds. (#/hr)	2		3	3		2						
Confl. Bikes (#/hr)						2						
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90
Heavy Vehicles (%)	2%	2%	2%	2%	2%	2%	3%	3%	3%	2%	2%	2%
Shared Lane Traffic (%)												
Turn Type	pm+pt	NA		pm+pt	NA	Perm	Perm	NA		Perm	NA	
Protected Phases	1	6		5	2			4				8
Permitted Phases	6			2		2	4			8		
Detector Phase	1	6		5	2	2	4	4		8		8
Switch Phase												
Minimum Initial (s)	5.0	15.0		5.0	15.0	15.0	7.0	7.0		7.0		7.0
Minimum Split (s)	11.8	31.9		11.9	31.9	31.9	41.8	41.8		34.8		34.8
Total Split (s)	15.0	78.0		25.0	88.0	88.0	47.0	47.0		47.0		47.0
Total Split (%)	10.0%	52.0%		16.7%	58.7%	58.7%	31.3%	31.3%		31.3%		31.3%
Yellow Time (s)	4.8	4.9		4.9	4.9	4.9	3.8	3.8		3.8		3.8
All-Red Time (s)	2.0	2.0		2.0	2.0	2.0	2.0	2.0		2.0		2.0
Lost Time Adjust (s)	0.0	0.0		0.0	0.0	0.0	0.0	0.0		0.0		0.0
Total Lost Time (s)	6.8	6.9		6.9	6.9	6.9	5.8	5.8		5.8		5.8
Lead/Lag	Lead	Lag		Lead	Lag	Lag						
Lead-Lag Optimize?	Yes	Yes		Yes	Yes	Yes						
Recall Mode	None	Max		None	C-Max	C-Max	None	None		None		None

Intersection Summary

Area Type: Other
 Cycle Length: 150
 Actuated Cycle Length: 150
 Offset: 20 (13%), Referenced to phase 2:WBTL, Start of Yellow
 Natural Cycle: 100
 Control Type: Actuated-Coordinated

Splits and Phases: 1: SW Florida Gateway Dr/Centurion Ct & US 90



HCM 6th Signalized Intersection Summary
 1: SW Florida Gateway Dr/Centurion Ct & US 90

Circle K - I-75 & US 90
 Buildout (2023) Conditions, PM Peak Hour



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗	↗	↖	↗		↖	↗	
Traffic Volume (veh/h)	37	1159	29	80	1332	117	41	2	58	86	3	57
Future Volume (veh/h)	37	1159	29	80	1332	117	41	2	58	86	3	57
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		0.98	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1856	1856	1856	1870	1870	1870
Adj Flow Rate, veh/h	41	1288	32	89	1480	130	46	2	64	96	3	63
Peak Hour Factor	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90	0.90
Percent Heavy Veh, %	2	2	2	2	2	2	3	3	3	2	2	2
Cap, veh/h	258	2515	62	334	2546	1110	165	6	193	166	9	192
Arrive On Green	0.03	0.71	0.71	0.03	0.72	0.72	0.13	0.13	0.13	0.13	0.13	0.13
Sat Flow, veh/h	1781	3543	88	1781	3554	1548	1325	48	1532	1335	73	1524
Grp Volume(v), veh/h	41	646	674	89	1480	130	46	0	66	96	0	66
Grp Sat Flow(s),veh/h/ln	1781	1777	1854	1781	1777	1548	1325	0	1580	1335	0	1596
Q Serve(g_s), s	0.9	24.8	24.9	2.0	30.3	3.9	4.9	0.0	5.7	10.6	0.0	5.7
Cycle Q Clear(g_c), s	0.9	24.8	24.9	2.0	30.3	3.9	10.6	0.0	5.7	16.3	0.0	5.7
Prop In Lane	1.00		0.05	1.00		1.00	1.00		0.97	1.00		0.95
Lane Grp Cap(c), veh/h	258	1261	1316	334	2546	1110	165	0	199	166	0	201
V/C Ratio(X)	0.16	0.51	0.51	0.27	0.58	0.12	0.28	0.00	0.33	0.58	0.00	0.33
Avail Cap(c_a), veh/h	307	1261	1316	489	2546	1110	362	0	434	364	0	438
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	0.00	1.00	1.00	0.00	1.00
Uniform Delay (d), s/veh	8.6	9.9	9.9	7.8	10.3	6.6	64.6	0.0	59.8	67.2	0.0	59.7
Incr Delay (d2), s/veh	0.3	1.5	1.4	0.4	1.0	0.2	0.9	0.0	1.0	3.2	0.0	0.9
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(95%),veh/ln	0.6	14.2	14.6	1.3	16.2	2.2	3.1	0.0	4.3	6.8	0.0	4.2
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	8.9	11.4	11.4	8.3	11.3	6.8	65.5	0.0	60.7	70.4	0.0	60.7
LnGrp LOS	A	B	B	A	B	A	E	A	E	E	A	E
Approach Vol, veh/h		1361			1699			112				162
Approach Delay, s/veh		11.3			10.8			62.7				66.4
Approach LOS		B			B			E				E
Timer - Assigned Phs	1	2		4	5	6		8				
Phs Duration (G+Y+Rc), s	10.9	114.4		24.7	11.9	113.4		24.7				
Change Period (Y+Rc), s	6.8	6.9		* 5.8	6.9	6.9		* 5.8				
Max Green Setting (Gmax), s	8.2	81.1		* 41	18.1	71.1		* 41				
Max Q Clear Time (g_c+I1), s	2.9	32.3		12.6	4.0	26.9		18.3				
Green Ext Time (p_c), s	0.0	16.0		0.5	0.1	10.8		0.6				

Intersection Summary

HCM 6th Ctrl Delay	15.5
HCM 6th LOS	B

Notes

- User approved pedestrian interval to be less than phase max green.
- * HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

APPENDIX E
Trip Generation Calculations

Table 1: Trip Generation

Land Use	Intensity	AM Peak Hour of Adjacent Street			PM Peak Hour of Adjacent Street		
		Total	In	Out	Total	In	Out
Existing Development Convenience Store/Gas Station (4-5.5k)	24 VFP	649	325	324	546	273	273
Existing Development Pass-By Convenience Store/Gas Station (4-5.5k)	<u>Daily</u> <u>AM</u> <u>PM</u> 75% 76% 75%	494	247	247	410	205	205
EXISTING SITE - POTENTIAL TOTAL DRIVEWAY VOLUMES		649	325	324	546	273	273
EXISTING SITE - POTENTIAL PASS-BY TRIPS		494	247	247	410	205	205
EXISTING SITE - POTENTIAL NEW EXTERNAL TRIPS		155	78	77	136	68	68
OBSERVED DRIVEWAY VOLUMES		201	106	95	220	115	105
ACTUAL/POTENTIAL DRIVEWAY VOLUMES ADJUSTMENT FACTOR		0.31			0.40		
Proposed Development Convenience Store/Gas Station (5.5-10k)	27 VFP	853	427	426	726	363	363
Proposed Development Pass-By Convenience Store/Gas Station (5.5-10k)	<u>Daily</u> <u>AM</u> <u>PM</u> 75% 76% 75%	648	324	324	544	272	272
PROPOSED SITE - POTENTIAL TOTAL DRIVEWAY VOLUMES		853	427	426	726	363	363
PROPOSED SITE - POTENTIAL TOTAL PASS-BY TRIPS		648	324	324	544	272	272
PROPOSED SITE - POTENTIAL TOTAL NEW EXTERNAL TRIPS		205	103	102	182	91	91
POTENTIAL NET NEW TOTAL DRIVEWAY VOLUMES (PROPOSED - EXISTING)		204	102	102	180	90	90
POTENTIAL NET NEW PASS-BY TRIPS (PROPOSED - EXISTING)		154	77	77	134	67	67
POTENTIAL NET NEW EXTERNAL TRIPS (PROPOSED - EXISTING)		50	25	25	46	23	23
ADJUSTED NET NEW TOTAL DRIVEWAY VOLUMES		64	32	32	72	36	36
ADJUSTED NET NEW PASS-BY TRIPS		48	24	24	54	27	27
ADJUSTED NET NEW EXTERNAL TRIPS		16	8	8	18	9	9

Trip generation and pass-by reductions were calculated using the following data from ITE's Trip Generation Manual, 11th Edition.

Convenience Store/ Gas Station (4-5.5k) [ITE 945]

Daily: $T = 257.13*(X)$; X is vehicle fueling positions
 AM Peak Hour of Adjacent Street: $T = 27.04*(X)$; X is vehicle fueling positions; (50% in, 50% out)
 PM Peak Hour of Adjacent Street: $T = 22.76*(X)$; X is vehicle fueling positions; (50% in, 50% out)

Convenience Store/ Gas Station (5.5-10k) [ITE 945]

Daily: $T = 345.75*(X)$; X is vehicle fueling positions
 AM Peak Hour of Adjacent Street: $T = 31.60*(X)$; X is vehicle fueling positions; (50% in, 50% out)
 PM Peak Hour of Adjacent Street: $T = 26.90*(X)$; X is vehicle fueling positions; (50% in, 50% out)

K:\ORL_Civil\149880040-Circle K US90 & I75\TPTO\03_Calcs\2022-03 - CK Lake City.xlsx\TG (2)

3/17/2022

APPENDIX F
FDOT *Trend* Worksheet

FLORIDA DEPARTMENT OF TRANSPORTATION
 TRANSPORTATION STATISTICS OFFICE
 2020 HISTORICAL AADT REPORT

COUNTY: 29 - COLUMBIA

SITE: 0278 - SR 10 400' W. OF I-75

YEAR	AADT		DIRECTION 1		DIRECTION 2	*K FACTOR	D FACTOR	T FACTOR
2020	27000	C	E 13500		W 13500	9.00	54.80	6.80
2019	30000	C	E 15000		W 15000	9.00	54.80	6.20
2018	28000	C	E 14000		W 14000	9.00	54.70	6.20
2017	27500	C	E 14000		W 13500	9.00	55.50	5.80
2016	27000	C	E 13500		W 13500	9.00	53.90	5.40
2015	27500	C	E 14000		W 13500	9.00	54.50	5.70
2014	27000	C	E 13500		W 13500	9.00	54.40	5.90
2013	25000	C	E 12500		W 12500	9.00	55.30	6.40
2012	26000	C	E 13000		W 13000	9.00	54.70	5.50
2011	26000	C	E 13000		W 13000	9.00	53.70	5.30
2010	25500	C	E 12500		W 13000	9.94	54.40	4.90
2009	25000	C	E 12500		W 12500	9.78	54.18	5.30
2008	27000	C	E 13500		W 13500	9.82	54.63	6.20
2007	27500	C	E 13500		W 14000	9.99	54.46	6.40
2006	27000	C	E 13500		W 13500	10.01	55.64	7.00
2005	31500	C	E 15500		W 16000	9.90	56.60	9.80

AADT FLAGS: C = COMPUTED; E = MANUAL ESTIMATE; F = FIRST YEAR ESTIMATE
 S = SECOND YEAR ESTIMATE; T = THIRD YEAR ESTIMATE; R = FOURTH YEAR ESTIMATE
 V = FIFTH YEAR ESTIMATE; 6 = SIXTH YEAR ESTIMATE; X = UNKNOWN

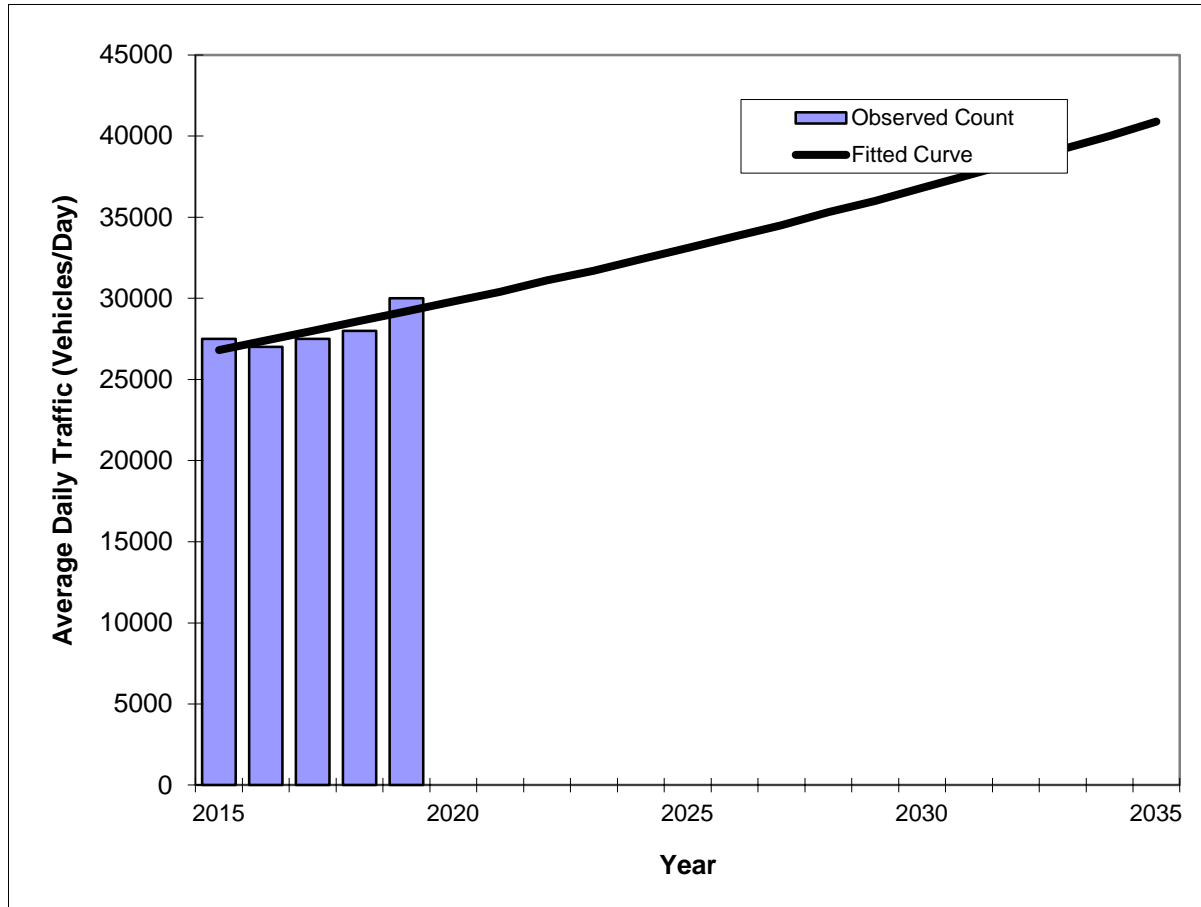
*K FACTOR: STARTING WITH YEAR 2011 IS STANDARDK, PRIOR YEARS ARE K30 VALUES

Traffic Trends - V03.a

SR 10 -- 400' W OF I-75

FIN#	429193-1
Location	1

County:	Columbia (29)
Station #:	0278
Highway:	SR 10



Year	Traffic (ADT/AADT)	
	Count*	Trend**
2015	27500	26800
2016	27000	27400
2017	27500	28000
2018	28000	28600
2019	30000	29200
2023 Opening Year Trend		
2023	N/A	31700
2024 Mid-Year Trend		
2024	N/A	32400
2025 Design Year Trend		
2025	N/A	33100
TRANPLAN Forecasts/Trends		

Trend R-squared:	65.77%
Compounded Annual Historic Growth Rate:	2.17%
Compounded Growth Rate (2019 to Design Year):	2.11%
Printed:	3-Mar-22
Exponential Growth Option	

*Axle-Adjusted

This Instrument Was Prepared By,
Record and Return to:

John Hotte, Esquire
Krinzman, Huss & Lubetsky
110 SE 6th Street, 20th Floor
Fort Lauderdale, FL 33301
Telephone: (954) 761-3454

Property Appraiser
Identification No.:
Consideration:\$ _____

Inst:201612000647 Date:1/14/2016 Time:10:37 AM
Doc: Stamp-Deed:19775.00
DC,P.DeWitt Cason,Columbia County Page 1 of 4 B:1307 P:1888

SPECIAL WARRANTY DEED

THIS SPECIAL WARRANTY DEED made this 12th day of January, 2016, by **Inn of Lake City, Inc.**, a Florida corporation ("Grantor"), whose mailing address is 1000 Red Fern Place, Flowood, MS 39232 in favor of **GWC Development Partners, LLC**, a Florida limited liability company ("Grantee"), whose mailing address is 2682 West Noegel Road, Lake City, FL 32055.

WITNESSETH:

That Grantor, for and in consideration of the sum of TEN DOLLARS (\$10.00) and other good and valuable consideration paid by Grantee, the receipt and sufficiency whereof are hereby acknowledged, does hereby grant, bargain, sell, alien, remise, release, convey and confirm unto Grantee the real property (the "Property") located in Columbia County, Florida, and more particularly described in Exhibit "A" attached hereto and made a part hereof.

SUBJECT ONLY TO the matters set forth in Exhibit "B" attached hereto and made a part hereof.

TOGETHER with all the tenements, hereditaments and appurtenances belonging or in any way appertaining to the Property, including, without limitation, all of Grantor's right, title and interest, if any, in and to all of the easements, rights, and privileges belonging or in any way appertaining to the Property and/or improvements located thereon.

TO HAVE AND TO HOLD the same in fee simple forever.

AND GRANTOR hereby covenants with Grantee that Grantor is lawfully seized of the Property in fee simple; that Grantor has good right and lawful authority to sell and convey the Property; that, subject to the matters described on Exhibit "B" attached hereto, Grantor does hereby warrant specially the title to the Property; and that Grantor and its successors and assigns will forever warrant and defend the same against the lawful claims of all persons claiming by, through or under Grantor, but against none other.

IN WITNESS WHEREOF, Grantor has caused this Special Warranty Deed to be executed by its duly authorized representative on the day and year first above written.

Two Witnesses:

Inn of Lake City, Inc., a Florida corporation

Debra D. Hardwick
Printed Name: Debra D. Hardwick

By: *Michael J. Hart*
Michael J. Hart, Vice President, Treasurer
and Assistant Secretary

John M. Stephens IV
Printed Name: John M. Stephens IV

STATE OF Mississippi
COUNTY OF Hinds

The foregoing instrument was acknowledged before me this 12 day of January, 2016, by Michael J. Hart, as Vice President, Treasurer and Assistant Secretary of Inn of Lake City, Inc., a Florida corporation, on behalf of the corporation. He is personally known to me.

Suzanna Baker
Notary Public, State of Mississippi
Print Name: Suzanna Baker
Commission No.: 83877
My Commission Expires: January 21, 2019
[Affix Notary Seal]



Signature Page of Special Warranty Deed

EXHIBIT "A"

Real Property Description

COMMENCE at the Northwest corner of Section 35, Township 3 South, Range 16 East, Columbia County, Florida as established by B.G. Moore, PLS No. 439 and run thence S 06°22'00" W, along the West line of said Section 35, 1894.50 feet to the West Limited Access Right of Way of Interstate No. 75, thence run Southerly and Westerly along said West Limited Access Right of Way the following courses. S 24°54'32" E, 472.32 feet to the POINT OF BEGINNING, S 24°54'32" E, 940.25 feet; S 15°12'50" E, 512.06 feet; S 06°01'43" E, 335.81 feet; S 36°55'36" W, 54.60 feet to the Northerly Right of Way of West U.S. Highway 90 and the end of said courses; thence S 80°47'35" W, along said Northerly Right of Way, 371.77 feet; thence S 08°51'10" E, along said Northerly Right of Way, 22.18 feet; thence S 80°47'36" W, along said Northerly Right of Way, 73.15 feet; thence N 08°55'17" W, 150.09 feet; thence S 80°42'55" W, 150.25 feet; thence N 08°52'22" W, 60.12 feet; thence S 80°53'59" W, 79.99 feet; thence S 08°59'18" E, 210.15 feet to the aforesaid Northerly Right of Way; thence S 80°47'36" W, along said Northerly Right of Way, 26.39 feet to a point of a curve; thence run Westerly along the arc of said curve concave to the North having a radius of 3224.04 feet, a central angle of 05°24'20", a chord bearing and distance of S 83°26'26" W 304.06 feet, an arc distance of 304.18 feet to the aforesaid West line of Section 35; thence N 06°22'00" E, along said West line, 1784.01 feet; thence N 65°09'42" E, 286.69 feet to the POINT OF BEGINNING.

LESS AND EXCEPT the parcel described in O.R. Book 1284, Page 229, of the Official Records of Columbia County, Florida

EXHIBIT "B"

Exceptions

1. Taxes and assessments for the year 2016 and subsequent years, which are not yet due and payable.
2. Any land use, zoning and building laws and ordinances.
3. Any declaration of covenants, conditions and restrictions, or other recorded restrictions.
4. Any right, title, interest, claim, violation, variation, encumbrance, encroachment, fact, matters or other adverse circumstance affecting title revealed, or that should have been revealed, by that certain ALTA/ACSM survey of the Property by JBrown Professional Group Inc. dated October 16, 2015, as revised (Proj. No. 366-15-01).
5. Any obligations, rights and other matters related to, and any agreements with and requirements of the State of Florida or other governmental agency regarding, the remediation of certain environmental issues on the Property by or on behalf of the State of Florida or a political subdivision thereof under a state-funded cleanup program(s).
6. Rights-of-way, utility easements, other easements, restrictions and other restrictive and/or use covenants filed of record and other matters which are revealed by a title search or title commitments, including the following:
 - a. Easement(s) in favor of Mississippi Management, Inc. set forth in instrument(s) recorded in Official Records Book 634, Page 338.
 - b. Easement(s) in favor of Shell Oil Company set forth in instrument(s) recorded in Official Records Book 674, Page 104.
 - c. Easement(s) in favor of American Telephone and Telegraph Company set forth in instrument(s) recorded in Official Records Book 723, Page 162.
 - d. Easement(s) in favor of The City of Lake City, Florida set forth in instrument(s) recorded in Official Records Book 776, Page 1724.
 - e. Easement contained in Deed recorded in Official Records Book 685, Page 38.
 - f. Easement recorded in Official Records Book 960, Page 1492.
 - g. Easement recorded in Official Records 104, Page 118, and in Official Records Book 361, Page 499.
 - h. Easement for ingress and egress recorded in Official Records Book 370, Page 337.
 - i. Easement(s) in favor of Florida Power and Light Company set forth in instrument(s) recorded in Official Records Book 361, Page 499.
 - j. Easement(s) in favor of The City of Lake City, Florida set forth in instrument(s) recorded in Official Records Book 559, Page 229.

Columbia County Property Appraiser

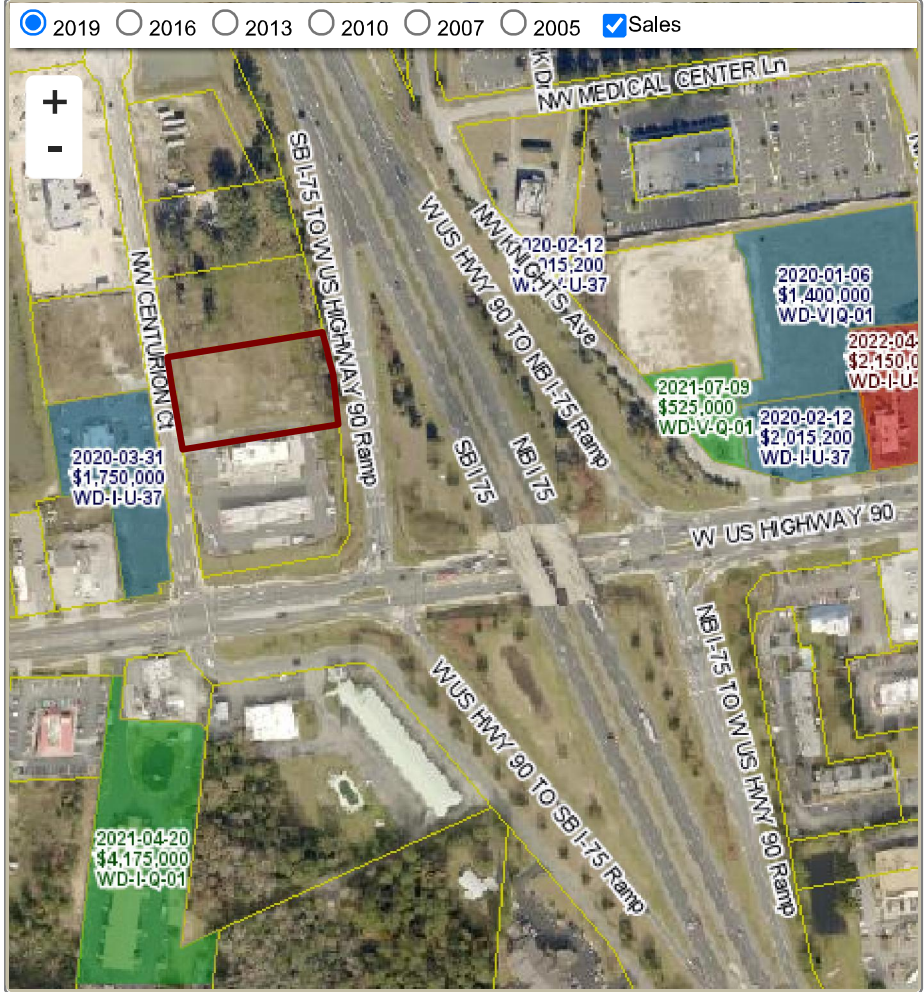
Jeff Hampton

2022 Working Values

updated: 6/9/2022

Parcel: << **35-3S-16-02524-102 (10494)** >>

Aerial Viewer Pictometry Google Maps



Owner & Property Info

Owner	GWC DEVELOPMENT PARTNERS LLC 2682 NW NOEGEL RD LAKE CITY, FL 32055		
Site			
Description*	LOT 2 GATEWAY CROSSING S/D A REPLAT OF LOTS 2 & 3. NKA LOT 2 A REPLAT OF LOTS 2, 3 & 11 OF GATEWAY CROSSING IN PLAT BK 9 PGS 176 & 177.		
Area	1.49 AC	S/T/R	35-3S-16
Use Code**	VACANT COMMERCIAL (1000)	Tax District	1

*The Description above is not to be used as the Legal Description for this parcel in any legal transaction.
 **The Use Code is a FL Dept. of Revenue (DOR) code and is not maintained by the Property Appraiser's office. Please contact your city or county Planning & Zoning office for specific zoning information.

Property & Assessment Values

2021 Certified Values		2022 Working Values	
Mkt Land	\$515,968	Mkt Land	\$746,396
Ag Land	\$0	Ag Land	\$0
Building	\$0	Building	\$0
XFOB	\$0	XFOB	\$0
Just	\$515,968	Just	\$746,396
Class	\$0	Class	\$0
Appraised	\$515,968	Appraised	\$746,396
SOH Cap [?]	\$0	SOH Cap [?]	\$178,831
Assessed	\$515,968	Assessed	\$746,396
Exempt	\$0	Exempt	\$0
Total Taxable	county:\$515,968 city:\$515,968 other:\$0 school:\$515,968	Total Taxable	county:\$567,565 city:\$567,565 other:\$0 school:\$746,396

Sales History

Sale Date	Sale Price	Book/Page	Deed	V/I	Qualification (Codes)	RCode
NONE						

Building Characteristics

Bldg Sketch	Description*	Year Blt	Base SF	Actual SF	Bldg Value
NONE					

▼ **Extra Features & Out Buildings** [\(Codes\)](#)

Code	Desc	Year Blt	Value	Units	Dims
N O N E					

▼ **Land Breakdown**

Code	Desc	Units	Adjustments	Eff Rate	Land Value
1000	VACANT COMMERCIAL (MKT)	64,904.000 SF (1.490 AC)	1.0000/1.0000 1.0000/ /	\$12 /SF	\$746,396



April 1, 2022

Suwanee River Water Management District
9225 CR 49
Live Oak, FL 32060

Subject: Circle K – US 90 & I-75 De-Minimis Exemption Letter
Project Name: Circle K – Circle K – US 90 & I-75
County: Columbia
Sec/Twp/Rge: S35 T3S R16E

To Whom it May Concern:

The proposed 3.47-acre Circle K – US 90 & I-75 project lies within the previously issued ERP No. 023-226410 and connects into the master project “Gateway Crossing” stormwater system. The project site is located at the northeast corner of the intersection of US Highway 90 and Centurion Court in the City of Lake City, Columbia County, Florida. We understand that this development is under SRWMD ERP No. 023-226410, and is shown as a portion of Basin DA-1 of the Gateway Crossing project. The proposed development will contain the addition of a 2,064 square foot Circle K high speed diesel canopy, with fueling stations, and associated infrastructure to the existing Circle K project constructed in 2016.

The proposed 3.47-acre Circle K – US 90 & I-75 lies within the previously issued ERP No. 023-226410 as stated above. We understand that this site is permitted up to 75% impervious area per ERP No. 023-226410.

As seen in the attached construction plans the Circle K – US 90 & I-75 project is proposing 2.63 acres (+/-114,580 SF) of impervious surface area to discharge into the system permitted under SRWMD ERP No. 023-226410. As the Circle K – US 90 & I-75 project is proposing impervious area less than or equal to the maximum allowed impervious surface area, and is connected to the master stormwater system that was approved in the Gateway Crossing project, ERP No. 023-226410, the project meets the requirements for a De-Minimis Exemption.

If you have any questions, or if you require additional information, please do not hesitate to contact our office at (407) 409-7002.

Very truly yours,

KIMLEY-HORN AND ASSOCIATES, INC.

Jarod C. Stubbs, P.E.
Project Engineer

Columbia County Tax Collector

generated on 6/9/2022 1:59:44 PM EDT

Tax Record

Last Update: 6/9/2022 1:58:23 PM EDT



Ad Valorem Taxes and Non-Ad Valorem Assessments

The information contained herein does not constitute a title search and should not be relied on as such.

Account Number	Tax Type	Tax Year			
R02524-102	REAL ESTATE	2021			
Mailing Address GWC DEVELOPMENT PARTNERS LLC 2682 NW NOEGEL RD LAKE CITY FL 32055		Property Address GEO Number 353S16-02524-102			
Exempt Amount	Taxable Value				
See Below	See Below				
Exemption Detail	Millage Code	Escrow Code			
NO EXEMPTIONS	001				
<u>Legal Description (click for full description)</u>					
35-3S-16 1000/10001.03 Acres LOT 2 GATEWAY CROSSING S/D A REPLAT OF LOTS 2 & 3.					
Ad Valorem Taxes					
Taxing Authority	Rate	Assessed Value	Exemption Amount	Taxable Value	Taxes Levied
BOARD OF COUNTY COMMISSIONERS	7.8150	515,968	0	\$515,968	\$4,032.29
CITY OF LAKE CITY	4.9000	515,968	0	\$515,968	\$2,528.24
COLUMBIA COUNTY SCHOOL BOARD					
DISCRETIONARY	0.7480	515,968	0	\$515,968	\$385.95
LOCAL	3.6430	515,968	0	\$515,968	\$1,879.67
CAPITAL OUTLAY	1.5000	515,968	0	\$515,968	\$773.95
SUWANNEE RIVER WATER MGT DIST	0.3615	515,968	0	\$515,968	\$186.52
LAKE SHORE HOSPITAL AUTHORITY	0.0000	515,968	0	\$515,968	\$0.00
Total Millage	18.9675	Total Taxes		\$9,786.62	
Non-Ad Valorem Assessments					

Code	Levying Authority	Amount
XLCF	CITY FIRE ASSESSMENT	\$50.40
Total Assessments		\$50.40
Taxes & Assessments		\$9,837.02
If Paid By		Amount Due
		\$0.00

Date Paid	Transaction	Receipt	Item	Amount Paid
12/29/2021	PAYMENT	1200971.0006	2021	\$9,541.91

Prior Years Payment History

Prior Year Taxes Due
NO DELINQUENT TAXES



DEPARTMENT OF GROWTH MANAGEMENT
205 North Marion Avenue
Lake City, Florida 32055
Telephone: (386) 719-5750
growthmanagement@lcfla.com

REVIEW REPORT TO PLANNING AND ZONING, BOARD OF ADJUSTMENT AND
HISTORICAL COMMITTEES' BY STAFF
FOR SITE PLAN REVIEW, SPECIAL EXCEPTIONS, VARIANCES, COMPREHENSIVE
PLAN AMENDMENTS/ ZONING AND CERTIFICATE OF APPROPRIATENESS

Date: 06/15/2022

Request Type: Site Plan Review (SPR) Special Exception (SE) Variances (V)

Comprehensive Plan Amendment/Zoning (CPA/Z) Certificate of Appropriateness (COA)

Project Number: SPR22-15

Project Name: Circle K-US 90 and I75 (Gateway Crossings)

Project Address: 143 NW Centurion CT, Lake City FL

Project Parcel Number: 35-3S-16-02524-001,102, and 111

Owner Name: Daniel Hotte of GWC Development Partners, LLC

Owner: Address: 2682 W Noegel RD

Owner Contact Information: telephone number 407-580-5173 e-mail dberry@shafferconst.com

Owner Agent Name: Jarod Stubbs P.E.

Owner Agent Address: 180 S. Orange Ave, Suite 1000 Orlando FL 32801

Owner Agent Contact Information: telephone 407-409-7002 e-mail jarod.stubbs@kimley-horne.com

The City of Lake City staff has reviewed the application and documents provided for the above request and have determined the following:

Growth Management – Building Department, Planning and Zoning, Code Enforcement, Permitting

Building Department: Approved Disapproved Reviewed by: _____

Comments: N/A

Planning and Zoning: Approve Disapprove Reviewed by: Robert Angelo

Comments: No Concerns at this time

No Concerns at this time

Business License: Approve Disapprove Reviewed by: Marshall Sova

Comments: No Concerns at this time

Code Enforcement: Approve Disapprove Reviewed by: Marshall Sova

Comments: No Concerns at this time

Permitting: Approve Disapprove Reviewed by: Ann Jones

Comments: No Concerns at this time

No Concerns at this time

Utilities – Water, Sewer, Gas, Water Distribution/Collections, Customer Service

Water Department: Approved Disapproved Reviewed by: _____

Comments: N/A

Sewer Department: Approved Disapproved Reviewed by: _____

Comments: N/A

Gas Department: Approved Disapproved Reviewed by: Steve Brown

Comments: No Concerns at this time

Water Distribution/Collection: Approved Disapproved Reviewed by: Brian Scott

Comments: _____
If they do not use the taps in place they will be required to make new ones and
cut and cap sewer and dig to water main and shut off before construction.

Customer Service: Approved Disapproved Reviewed by: Shasta Pelham

Comments: Utility Plan 6.0 dated 05/04/22 references a 1" water meter and an existing 6" sewer tap. A tap application would be required to access city utilities.
The tap fees, impact fees and utility deposits will be calculated upon approval of the tap application. A floor plan with detailed fixture units of the restroom addition
is required. City utilities border the property; locates must be obtained to ensure that the utility infrastructure is not damaged or obstructed.

Public Safety – Public Works, Fire Department, Police Department

Public Works: Approved Disapproved Reviewed by: Steve Brown

Comments: No Concerns at this time

Fire Department: Approve Disapprove Reviewed by: Assistant Chief Boozer

Comments: No Concerns at this time

Police Department: Approve Disapprove Reviewed by: Assistant Chief Andy

Comments: No Concerns at this time

Please provide separate pages for comments that will not fit in provided spaces and please label the pages for your department and for the project.

File Attachments for Item:

iii. Site Plan Review - SPR-22-14 - Parker Neely (Agent: Chris Potts, P.E.) Sonic Drive In



GROWTH MANAGEMENT
 205 North Marion Ave.
 Lake City, FL 32055
 Telephone: (386)719-5750
 E-Mail:
 growthmanagement@lcfla.com

FOR PLANNING USE ONLY

Application # _____
 Application Fee \$200.00
 Receipt No. _____
 Filing Date _____
 Completeness Date _____

Site Plan Application

A. PROJECT INFORMATION

1. Project Name: SONIC DRIVE-IN
2. Address of Subject Property: NW CENTURION CT, LAKE CITY, FL, 32055
3. Parcel ID Number(s): 35-3S-16-02524-103
4. Future Land Use Map Designation: COMMERCIAL
5. Zoning Designation: COMMERCIAL HIGHWAY INTERCHANGE
6. Acreage: 1.21
7. Existing Use of Property: VACANT
8. Proposed use of Property: COMMERICAL RESTAURANT
9. Type of Development (Check All That Apply):
 - Increase of floor area to an existing structure: Total increase of square footage _____
 - New construction: Total square footage 1,227
 - Relocation of an existing structure: Total square footage _____

B. APPLICANT INFORMATION

1. Applicant Status Owner (title holder) Agent
2. Name of Applicant(s): Chris Potts, PE Title: Director of Civil Engineering
 Company name (if applicable): JBPro
 Mailing Address: 3530 NW 43rd ST
 City: Gainesville State: FL Zip: 32606
 Telephone: (352) 375-8999 Fax: () Email: chris.potts@jbpro.com

PLEASE NOTE: Florida has a very broad public records law. Most written communications to or from government officials regarding government business is subject to public records requests. Your e-mail address and communications may be subject to public disclosure.

3. If the applicant is agent for the property owner*.
 - Property Owner Name (title holder): Parker Neely
 - Mailing Address: 2682 NW NOEGEL ROAD
 - City: LAKE CITY State: FL Zip: 32055
 - Telephone: (704) 577-2475 Fax: () Email: DNEELY@HIGHCOTTON-CEP.COM

PLEASE NOTE: Florida has a very broad public records law. Most written communications to or from government officials regarding government business is subject to public records requests. Your e-mail address and communications may be subject to public disclosure.

***Must provide an executed Property Owner Affidavit Form authorizing the agent to act on behalf of the property owner.**

C. ADDITIONAL INFORMATION

1. Is there any additional contract for the sale of, or options to purchase, the subject property?
If yes, list the names of all parties involved: _____
If yes, is the contract/option contingent or absolute: Contingent Absolute
2. Has a previous application been made on all or part of the subject property? Yes No
Future Land Use Map Amendment: Yes _____ No _____
Future Land Use Map Amendment Application No. _____
Site Specific Amendment to the Official Zoning Atlas (Rezoning): Yes _____ No
Site Specific Amendment to the Official Zoning Atlas (Rezoning) Application No. _____
Variance: Yes _____ No _____
Variance Application No. _____
Special Exception: Yes _____ No _____
Special Exception Application No. _____

D. ATTACHMENT/SUBMITTAL REQUIREMENTS

1. Vicinity Map – Indicating general location of the site, abutting streets, existing utilities, complete legal description of the property in question, and adjacent land use.
2. Site Plan – Including, but not limited to the following:
 - a. Name, location, owner, and designer of the proposed development.
 - b. Present zoning for subject site.
 - c. Location of the site in relation to surrounding properties, including the means of ingress and egress to such properties and any screening or buffers on such properties.
 - d. Date, north arrow, and graphic scale not less than one inch equal to 50 feet.
 - e. Area and dimensions of site (Survey).
 - f. Location of all property lines, existing right-of-way approaches, sidewalks, curbs, and gutters.
 - g. Access to utilities and points of utility hook-up.
 - h. Location and dimensions of all existing and proposed parking areas and loading areas.
 - i. Location, size, and design of proposed landscaped areas (including existing trees and required landscaped buffer areas).
 - j. Location and size of any lakes, ponds, canals, or other waters and waterways.
 - k. Structures and major features fully dimensioned including setbacks, distances between structures, floor area, width of driveways, parking spaces, property or lot lines, and percent of property covered by structures.
 - l. Location of trash receptacles.
 - m. For multiple-family, hotel, motel, and mobile home park site plans:
 - i. Tabulation of gross acreage.
 - ii. Tabulation of density.
 - iii. Number of dwelling units proposed.
 - iv. Location and percent of total open space and recreation areas.
 - v. Percent of lot covered by buildings.

- vi. Floor area of dwelling units.
 - vii. Number of proposed parking spaces.
 - viii. Street layout.
 - ix. Layout of mobile home stands (for mobile home parks only).
3. Stormwater Management Plan—Including the following:
 - a. Existing contours at one foot intervals based on U.S. Coast and Geodetic Datum.
 - b. Proposed finished elevation of each building site and first floor level.
 - c. Existing and proposed stormwater management facilities with size and grades.
 - d. Proposed orderly disposal of surface water runoff.
 - e. Centerline elevations along adjacent streets.
 - f. Water management district surface water management permit.
 4. Fire Department Access and Water Supply Plan: The Fire Department Access and Water Supply Plan must demonstrate compliance with Chapter 18 of the Florida Fire Prevention Code, be located on a separate signed and sealed plan sheet, and must be prepared by a professional fire engineer licensed in the State of Florida. The Fire Department Access and Water Supply Plan must contain fire flow calculations in accordance with the Guide for Determination of Required Fire Flow, latest edition, as published by the Insurance Service Office (“ISO”) and/or Chapter 18, Section 18.4 of the Florida Fire Prevention Code, whichever is greater.
 5. Concurrency Impact Analysis: Concurrency Impact Analysis of impacts to public facilities. For commercial and industrial developments, an analysis of the impacts to Transportation, Potable Water, Sanitary Sewer, and Solid Waste impacts are required.
 6. Comprehensive Plan Consistency Analysis: An analysis of the application’s consistency with the Comprehensive Plan (analysis must identify specific Goals, Objectives, and Policies of the Comprehensive Plan and detail how the application complies with said Goals, Objectives, and Policies).
 7. Legal Description with Tax Parcel Number (In Word Format).
 8. Proof of Ownership (i.e. deed).
 9. Agent Authorization Form (signed and notarized).
 10. Proof of Payment of Taxes (can be obtained online via the Columbia County Tax Collector’s Office).
 11. Fee. The application fee for a Site and Development Plan Application is \$200.00. No application shall be accepted or processed until the required application fee has been paid.

NOTICE TO APPLICANT

All eleven (11) attachments are required for a complete application. Once an application is submitted and paid for, a completeness review will be done to ensure all the requirements for a complete application have been met. If there are any deficiencies, the applicant will be notified in writing. If an application is deemed to be incomplete, it may cause a delay in the scheduling of the application before the Planning & Zoning Board.

A total of ten (10) copies of proposed site plan application and all support materials must be submitted along with a PDF copy on a CD. See City of Lake City submittal guidelines for additional submittal requirements.

THE APPLICANT ACKNOWLEDGES THAT THE APPLICANT OR AGENT MUST BE PRESENT AT THE PUBLIC HEARING BEFORE THE PLANNING AND ZONING BOARD, AS ADOPTED IN THE BOARD RULES AND PROCEDURES, OTHERWISE THE REQUEST MAY BE CONTINUED TO A FUTURE HEARING DATE.

I hereby certify that all of the above statements and statements contained in any documents or plans submitted herewith are true and accurate to the best of my knowledge and belief.

Christopher Potts

Applicant/Agent Name (Type or Print)

[Signature]

Applicant/Agent Signature

6/6/22

Date

Applicant/Agent Name (Type or Print)

Applicant/Agent Signature

Date

STATE OF FLORIDA
COUNTY OF Alachua

The foregoing instrument was acknowledged before me this 6th day of June, 2022, by (name of person acknowledging) Christopher Potts.



(NOTARY SEAL or SIGNATURE)

Jennifer Newbegin
Notary Public
State of Florida
Comm# HH075654
Expires 12/29/2024

Jennifer Newbegin

Signature of Notary

Jennifer Newbegin

Printed Name of Notary

Personally Known X OR Produced Identification _____
Type of Identification Produced

GENERAL DEVELOPMENT NOTES

1. PROPOSED NAME OF PROJECT: SONIC DRIVE-IN
2. PROJECT DESCRIPTION: RESTAURANT
3. OWNER: PARKER NEELY
4. DEVELOPER: HIGH COTTON EQUITIES; 2682 NW NOEGEL ROAD, LAKE CITY, FL 32055
5. CIVIL ENGINEER: JBROWN PROFESSIONAL GROUP INC.; CONTACT: CHRIS POTTS, P.E. (352) 375-8999.
E-MAIL: CHRIS.POTTS@JBPRO.COM
6. SURVEYOR: JBROWN PROFESSIONAL GROUP INC.; CONTACT: TROY WRIGHT, PSM (352) 375-8999
7. VICINITY MAP: SEE MAP THIS SHEET
8. TAX PARCEL NO'S.: 35-3S-16-02524-103
9. PARCEL AREA: 1.21 ACRES IN SIZE
10. PROJECT AREA: 1.21 ACRES
11. SECTION: 35
12. TOWNSHIP: 3S
13. RANGE: 16
14. FUTURE LAND USE: COMMERCIAL HIGHWAY INTERCHANGE (CHI)
15. ZONING: COMMERCIAL HIGHWAY INTERCHANGE (CHI)
16. FLOODPLAIN: THIS PROJECT IS NOT LOCATED IN A FEDERAL FLOOD ZONE.
17. DEVELOPMENT DATA:
 - A. TOTAL SITE AREA: 1.21 AC / 52708 SF
 - B. BUILDING AREA: 1227 SF
 - C. PAVEMENT & SIDEWALK AREA: 29989 SF
 - D. TOTAL IMPERVIOUS AREA: 31216 SF
 - E. OPEN AREA: 21492 SF
18. PARKING REQUIREMENTS:
 - A. 1 SPACE/EA 3 SEATS OF SEATING AREA
 - B. PROPOSED SEATING: 24 OUTDOOR SEATS / NO INDOOR SEATING
 - C. PARKING REQUIRED: 8 SPOTS
 - D. PARKING PROVIDED: 39 SPOTS
19. REQUIRED SETBACKS:
 - A. PER CHI ZONING CATEGORY, THE MINIMUM YARD REQUIREMENTS PER LAKE CITY LDR'S SECTION 4.15.7 ARE AS FOLLOWS:
 1. FRONT YARD = 30 FT
 2. SIDE YARD = 30 FT
 3. REAR YARD = 30 FT
 - B. ALL BUILDINGS ARE SETBACK GREATER THAN 30 FT FROM FRONT, SIDE, AND REAR PROPERTY LINES.
 - C. FRONT YARD SHALL INCLUDE MINIMUM 15 FT LANDSCAPED AREA.

CONSTRUCTION PLANS

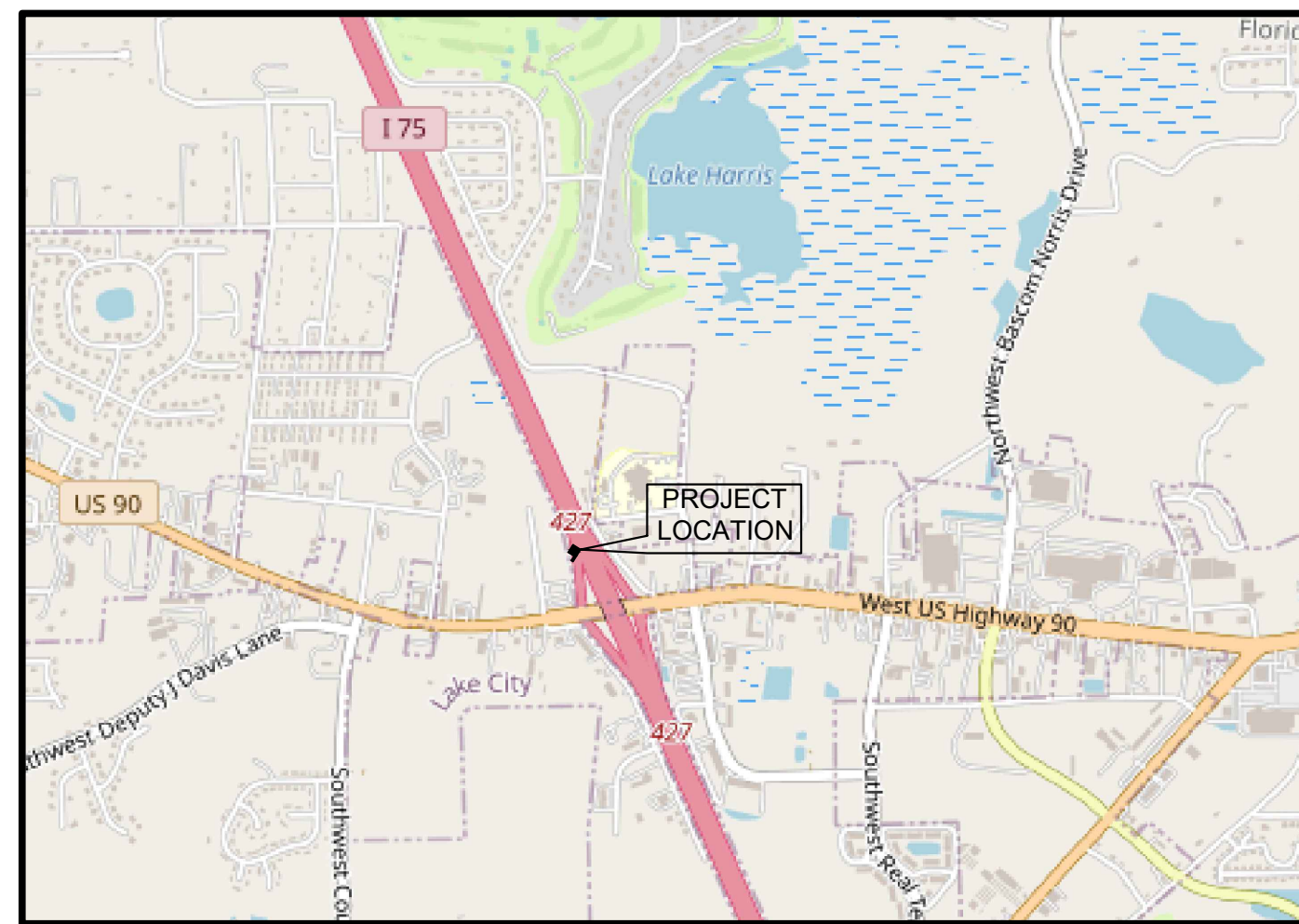
OF

GATEWAY CROSSING LOT 3

SONIC DRIVE - IN

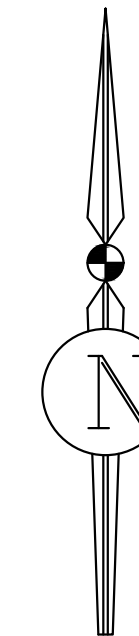
FOR

HIGH COTTON EQUITIES, LLC



LOCATION MAP

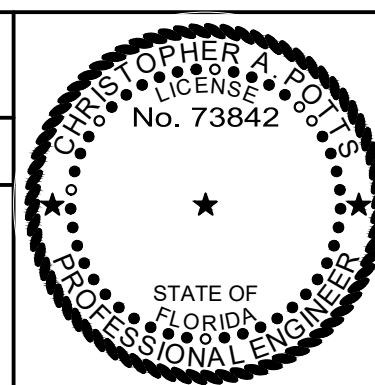
N.T.S.



GATEWAY CROSSING LOT 3 SONIC RESTAURANT	
SHEET INDEX	
SHEET NO.	TITLE
C0.0	COVER SHEET
C0.1	LEGEND, ABBREVIATIONS, AND NOTES
C1.0	DEMOLITION PLAN
C1.1	EROSION CONTROL PLAN
C2.0	DIMENSION PLAN
C3.0	PAVING GRADING & DRAINAGE PLAN
C4.0	UTILITY PLAN
C5.0	DETAILS AND NOTES
V-1	BOUNDARY AND TOPOGRAPHIC SURVEY

REVISIONS				
NO.	DATE	DESCRIPTION	DRWN	APPR

Christopher A Potts
Digital signature of Christopher A Potts, PE, No. 73842, State of Florida. License expires 12/31/2024.
 ENGINEER OF RECORD: CHRISTOPHER A. POTTS, P.E. FLORIDA LICENSE NO. 73842
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SHEET TITLE:
COVER SHEET
 CLIENT:
 HIGH COTTON EQUITIES, LLC

PROJECT:
SONIC RESTAURANT
 LAKE CITY, FLORIDA

DATE:
JUNE 2022
 PROJECT NO:
366-22-02
 SHEET NO:
C0.0

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GENERAL CONSTRUCTION NOTES

- 1. CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL EXISTING UTILITIES WITHIN THE WORK LIMITS OF ALL IMPROVEMENTS. THE CONTRACTOR IS ALSO RESPONSIBLE FOR PROTECTING ALL EXISTING UTILITIES DURING CONSTRUCTION. IN THE EVENT THAT A UTILITY CONFLICT OCCURS, CONTRACTOR SHALL NOTIFY THE ENGINEER AND UTILITY PROVIDER TO COORDINATE REMEDIAL ACTION.

NPDES EROSION AND SEDIMENTATION CONTROL PERMITTING NOTES

- 1. THE CONTRACTOR IS REQUIRED TO FILE THE FLORIDA DEPT. OF ENVIRONMENTAL PROTECTION (FDEP) NOTICE OF INTENT (NOI) TO USE GENERIC PERMIT FOR STORMWATER DISCHARGE FROM LARGE AND SMALL CONSTRUCTION ACTIVITIES (CGP) (RULE 62-621.300(4), F.A.C.). CONSTRUCTION ACTIVITY SHALL NOT COMMENCE PRIOR TO OBTAINING AN ACKNOWLEDGEMENT LETTER FROM FDEP VERIFYING THE NOI IS COMPLETE, THE PROJECT IS COVERED BY THE GENERIC PERMIT, AND THE PROJECT IDENTIFICATION NO. IS ESTABLISHED. CONTRACTOR SHALL PROVIDE THE EOR AND PROPERTY OWNER WITH A COPY OF THE FDEP ACKNOWLEDGEMENT LETTER VERIFYING COVERAGE.

WORK WITHIN THE CITY RIGHT-OF-WAY

- 1. THE METHOD AND MANNER OF PERFORMING THE WORK AND THE QUALITIES OF MATERIAL FOR CONSTRUCTION WITHIN THE RIGHT-OF-WAY SHALL CONFORM TO THE REQUIREMENTS SPECIFIED BY CITY OF LAKE CITY FOR WORK WITHIN THEIR RESPECTIVE RIGHT-OF-WAYS.

STANDARD ABBREVIATIONS

Table with 3 columns: Abbreviation, Description, and Abbreviation. Lists various symbols and their corresponding terms like ALACHUA COUNTY PUBLIC WORKS, ADJACENT, ALUMINUM, etc.

LEGEND

Table with 2 columns: Symbol and Description. Lists symbols for existing and proposed lines like EST, EWW, EGAS, ECATV, etc.

PAVING, GRADING AND DRAINAGE SPECIFICATIONS

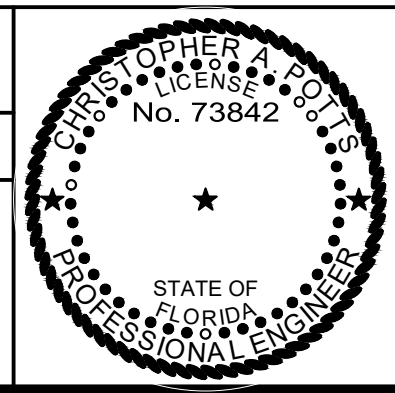
- 1. ALL DRAINAGE CONSTRUCTION, INCLUDING MATERIALS, CONSTRUCTION TECHNIQUES, AND TECHNICAL STANDARDS, SHALL BE IN ACCORDANCE WITH THE FDOT FY 2020-21 STANDARD PLANS.

REQUIRED SUBMITTALS

- 1. PRIOR TO CONSTRUCTION INSTALLATION OF SPECIFIC WORK SCOPE ITEMS THE CONTRACTOR SHALL SUBMIT SHOP DRAWINGS AND SUBMITTAL INFORMATION TO THE EOR FOR REVIEW AND APPROVAL. THESE SUBMITTALS SHALL INCLUDE SOURCE PROVIDER, MANUFACTURER LITERATURE, CUT SHEETS, DIMENSIONED DRAWINGS, FABRICATION DRAWINGS, SKETCHES, DETAILS AND ANY OTHER INFORMATION NECESSARY FOR THE EOR TO REVIEW AND APPROVE AS TO COMPLIANCE WITH THE DESIGN DRAWINGS AND SPECIFICATIONS.

Table with 4 columns: NO., DATE, DESCRIPTION, DRWN, APPR. Contains revision notes.

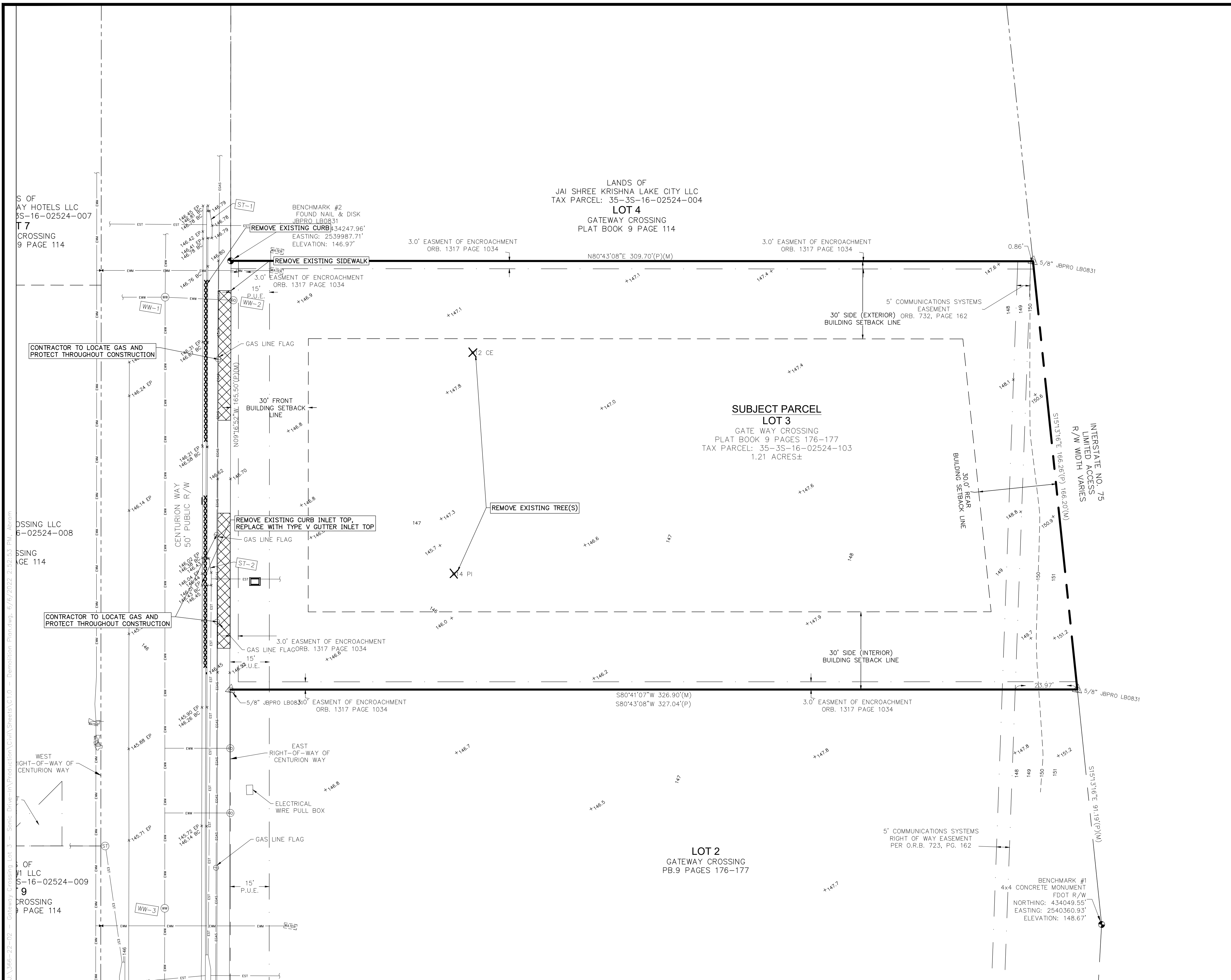
REVISIONS table content and signature block for Christopher A. Potts, P.E. Florida License No. 73842.



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SHEET TITLE: LEGEND ABBREVIATIONS AND NOTES. CLIENT: HIGH COTTON EQUITIES, LLC. PROJECT: SONIC RESTAURANT LAKE CITY, FLORIDA. DATE: JUNE 2022. SHEET NO: C0.1



- GENERAL DEMOLITION NOTES**
1. PRIOR TO DEMOLITION AND CONSTRUCTION, THE CONTRACTOR SHALL CONTACT THE APPROPRIATE UTILITY COMPANIES TO VERIFY THE LOCATION OF ALL ABOVE GROUND AND UNDERGROUND UTILITIES TO BE REMOVED OR RELOCATED.
 2. CONTRACTOR IS RESPONSIBLE FOR ALL SITE DEMOLITION AND MATERIAL REMOVAL. IN ADDITION TO THE DEMOLITION SHOWN, THE CONTRACTOR SHALL REMOVE ALL MISCELLANEOUS TRASH, DEBRIS, FENCING ETC., IN ORDER TO PROVIDE A CLEAN CONSTRUCTION SITE.
 3. CONTRACTOR IS RESPONSIBLE FOR DISPOSING OF ALL DEMOLITION MATERIAL OFF SITE. DISPOSAL METHODS AND LOCATIONS SHALL BE IN ACCORDANCE WITH ALL APPLICABLE LOCAL, STATE, AND FEDERAL REQUIREMENTS.
 4. REMOVAL OF ALL CONSTRUCTION DEBRIS, LIMEROCK, EXCESS OF BUILDERS SAND, CONCRETE AND MORTAR DEBRIS, EXISTING WEEDS AND GRASSES, AND ALL FOREIGN MATERIALS IN THE PLANTING BED AND SOD AREAS IS THE RESPONSIBILITY OF THE SITE WORK CONTRACTOR. SOIL IN AREAS TO BE LANDSCAPED SHALL BE UNCOMPACTED, SUITABLE FOR ROOT GROWTH WITH APPROPRIATE AMOUNTS OF ORGANIC MATTER, AND OF PH RANGE OF 5.5 - 6.5.
 5. ALL OFF SITE TREES SHALL REMAIN UNLESS OTHERWISE NOTED.
 6. PRIOR TO CONSTRUCTION THE CONTRACTOR SHALL DESIGNATE A TEMPORARY CONSTRUCTION ENTRANCE AND STOCKPILE AREA.
 7. EXISTING SIGNS SHALL BE REMOVED AND STORED DURING DEMOLITION SO THAT THEY CAN BE REINSTALLED DURING CONSTRUCTION.

- LEGEND**
- EXISTING HARDSCAPE TO BE REMOVED
 - EXISTING CURB TO BE REMOVED
 - EXISTING TREE TO BE REMOVED

REVISIONS

NO.	DATE	DESCRIPTION	DRWN	APPR

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SHEET TITLE: **DEMOLITION PLAN**

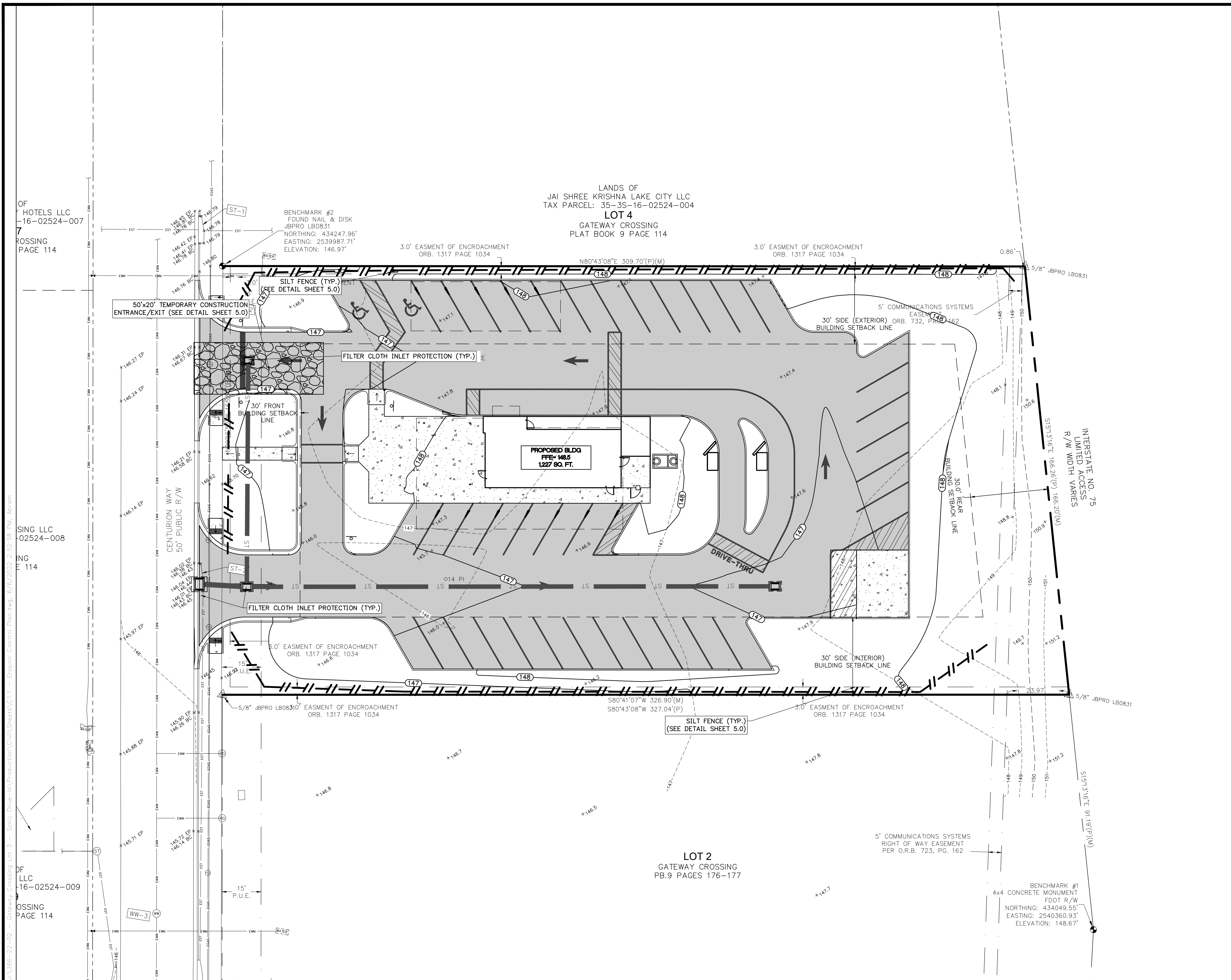
CLIENT: **HIGH COTTON EQUITIES, LLC**

PROJECT: **SONIC RESTAURANT
 LAKE CITY, FLORIDA**

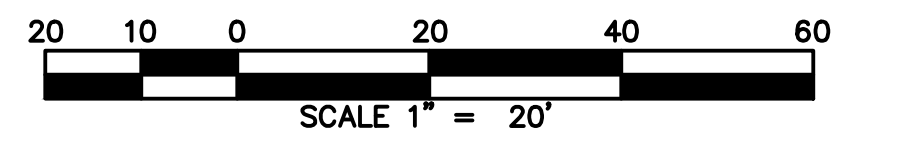
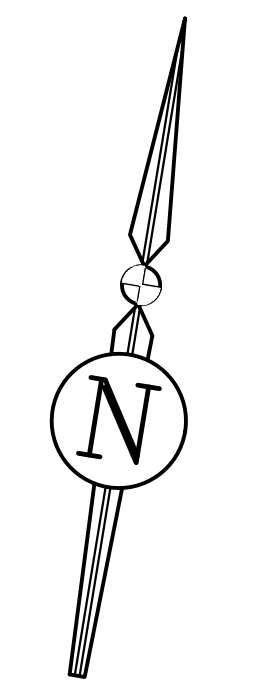
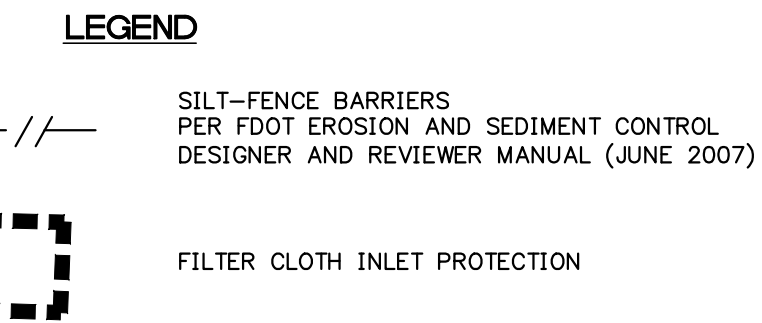
DATE: **JUNE 2022**

PROJECT NO.: **366-22-02**

SHEET NO.: **C1.0**



- EROSION AND SEDIMENTATION CONTROL NOTES**
1. THE CONTRACTOR SHALL UTILIZE THE EROSION AND SEDIMENTATION CONTROL PLAN AS A GENERAL GUIDE AND DIRECTION FOR MINIMUM CONTROL MEASURES. CONTRACTOR SHALL UTILIZE EROSION CONTROL BEST MANAGEMENT PRACTICES (BMP'S) AS REQUIRED TO MINIMIZE EROSION AND SEDIMENTATION BUILDUP ONSITE AND TO PREVENT EROSION AND SEDIMENTATION DISCHARGE OFF PROPERTY.
 2. CONTRACTOR SHALL EMPLOY AN FDEP CERTIFIED EROSION AND SEDIMENTATION CONTROL INSPECTOR TO MONITOR THE CONTRACTOR'S EROSION AND SEDIMENTATION CONTROL WORK EFFORT THROUGHOUT CONSTRUCTION.
 3. DURING CONSTRUCTION, NO DIRECT DISCHARGE OF WATER TO DOWNSTREAM RECEIVING WATERS WILL BE ALLOWED. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING WATER QUALITY, AND SHALL ROUTE DISCHARGE WATER IN SUCH A MANNER TO ADEQUATELY REMOVE SILT PRIOR TO RUNOFF FROM THE SITE.
 4. SILT FENCING AND OTHER SEDIMENT CONTROL MEASURES SHALL BE CONSTRUCTED WHERE SHOWN ON THE DRAWINGS PRIOR TO STARTING CONSTRUCTION. ALL STORM DRAIN INLETS SHALL BE PROTECTED DURING CONSTRUCTION IN ACCORDANCE WITH FDOT EROSION AND SEDIMENTATION CONTROL DESIGNER AND REVIEWER MANUAL (JUNE 2007) TO PREVENT SEDIMENT DISCHARGE TO THE EXISTING STORMWATER MANAGEMENT SYSTEM.
 5. THE RETENTION BASINS SHALL BE PROTECTED FROM SEDIMENT DISCHARGE BY SILT FENCE AT THE DISCHARGE STORM STRUCTURE. CAREFUL ATTENTION SHALL BE PAID TO PREVENT EROSION FROM ENTERING THE BASIN BOTTOM BEYOND THE IMMEDIATE EXIT OF THE STRUCTURE. REMOVE ALL SEDIMENT BUILDUP AT THE DISCHARGE STRUCTURE REGULARLY THROUGHOUT CONSTRUCTION.
 6. PROTECT EXISTING STORMWATER INLET STRUCTURES WITH FILTER FABRIC OR OTHER EROSION CONTROL DEVICE TO PREVENT SEDIMENTS FROM ENTERING STORM SEWER SYSTEM.
 7. ALL EROSION AND SEDIMENTATION CONTROL STRUCTURES SHALL BE INSPECTED AT LEAST ONCE PER WEEK AND AFTER 1/2" OR GREATER RAINFALL. ANY DEFICIENCIES DISCOVERED SHALL BE REPAIRED, ADJUSTED, OR IMPROVED AS REQUIRED.
 8. ALL EROSION AND SEDIMENTATION CONTROL STRUCTURES SHALL BE MAINTAINED UNTIL CONSTRUCTION IS COMPLETE AND APPROVED, AND VEGETATION IS FULLY ESTABLISHED.
 9. CONTRACTOR SHALL MINIMIZE AIR POLLUTION FROM PARTICULATES AND DUST. WATER SHALL BE APPLIED AS NEEDED TO REDUCE DUST DURING CONSTRUCTION. USE OTHER STABILIZATION METHODS SUCH AS HYDROMULCHING AS NEEDED.
 10. CONTRACTOR SHALL STOCKPILE AND STORE MATERIALS SUCH THAT STORMWATER FLOW IS NOT IMPEDED DURING CONSTRUCTION AND MATERIAL EROSION DOES NOT OCCUR.
 11. ALL DISTURBED OPEN AREAS WITHIN THE PROJECT CONSTRUCTION LIMITS SHALL BE COMPLETELY GRASSSED BY COMPLETION OF CONSTRUCTION. GRASS SEEDING RATES AND MIXTURES SHALL BE PER FDOT EROSION AND SEDIMENTATION CONTROL DESIGNER AND REVIEWER MANUAL (JULY 2013). EVIDENCE OF GROWTH MUST BE PRESENT PRIOR TO FINAL RELEASE.
 12. THE STORM SEWER SYSTEM SHALL BE FLUSHED OUT TO REMOVE ALL ACCUMULATED DEBRIS AND SEDIMENT UPON COMPLETION OF CONSTRUCTION.



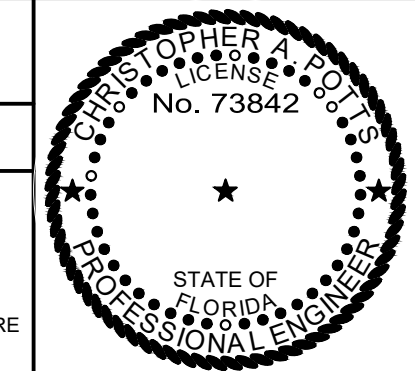
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SHEET TITLE: **EROSION AND SEDIMENTATION CONTROL PLAN**

CLIENT: **HIGH COTTON EQUITIES, LLC**

PROJECT: **SONIC RESTAURANT
LAKE CITY, FLORIDA**

DATE: **JUNE 2022**

PROJECT NO: **366-22-02**

SHEET NO: **C1.1**

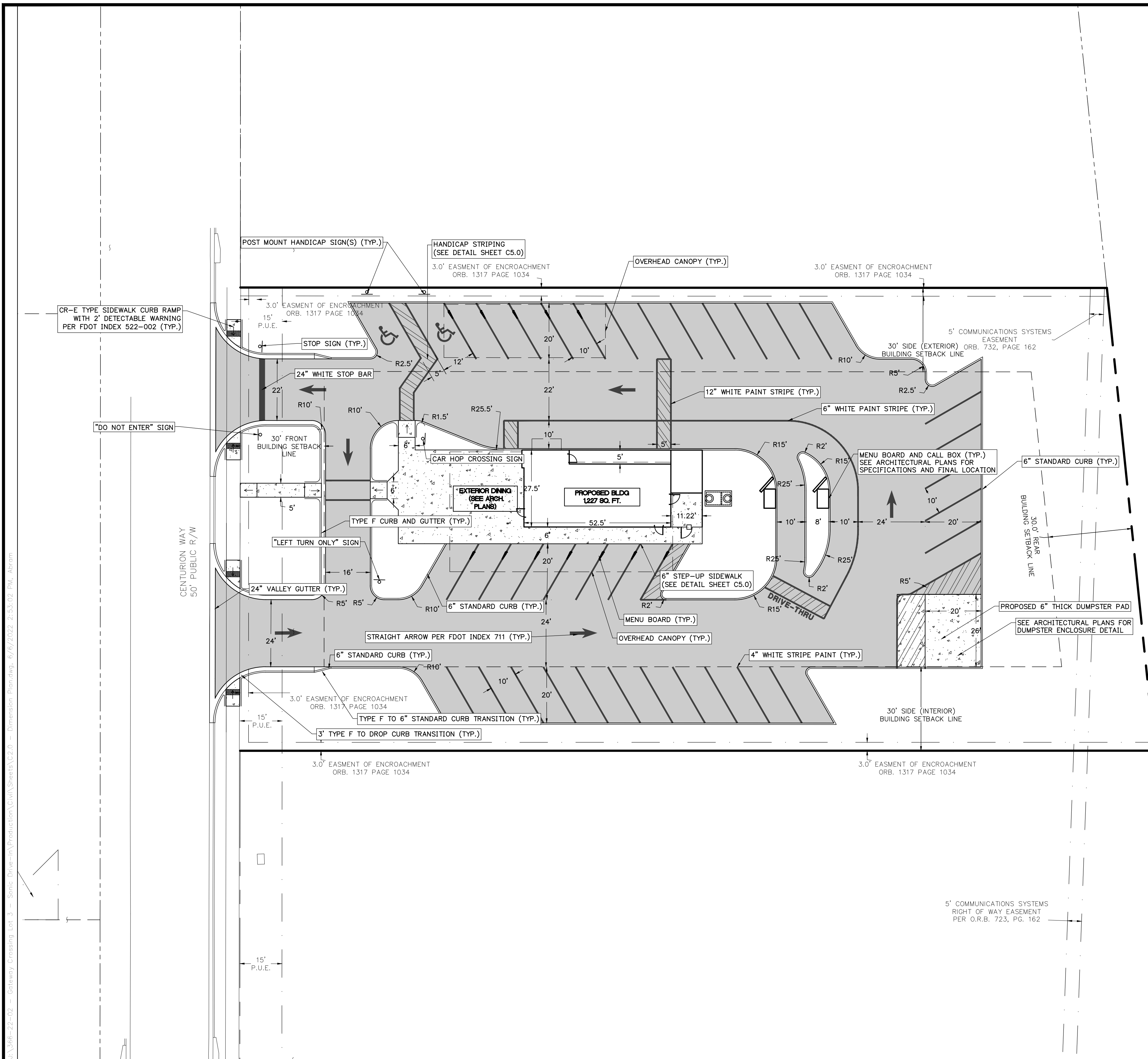
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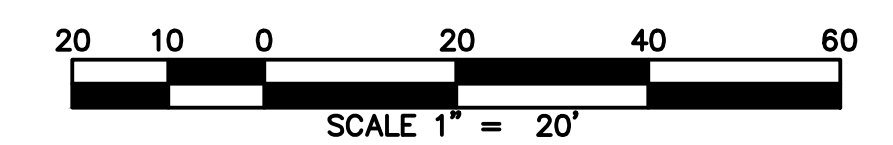
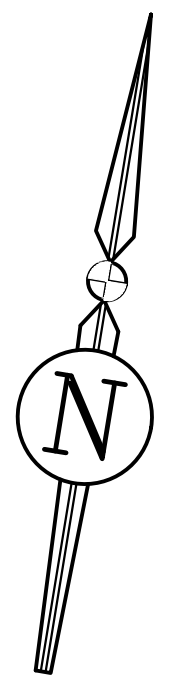
DIMENSION NOTES

1. CONTRACTOR SHALL FAMILIARIZE THEMSELVES WITH THE PROJECT SURVEY AND DESIGN DRAWINGS, AND SHALL VERIFY EXISTING SITE CONDITIONS. ANY APPARENT DISCREPANCIES OR ADDITIONAL PERTINENT INFORMATION SHALL BE BROUGHT TO THE PROJECT ENGINEER'S ATTENTION IMMEDIATELY.
2. ALL DIMENSIONS AND RADII SHOWN ARE DIMENSIONED TO EDGE OF PAVEMENT OR FACE OF CURB, UNLESS OTHERWISE SPECIFIED.
3. ALL STRIPING SHALL CONFORM TO FDOT STANDARD INDEX 711-001.
4. TOTAL NUMBER OF PARKING SPACES = 42 (2 HANDICAP PARKING SPACES)



LEGEND

- NEW ASPHALT PAVEMENT
- NEW CONCRETE

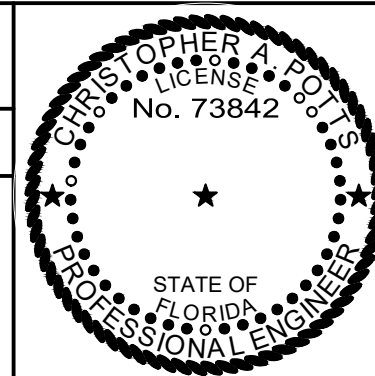


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SHEET TITLE: **DIMENSION PLAN**

CLIENT: **HIGH COTTON EQUITIES, LLC**

PROJECT: **SONIC RESTAURANT
LAKE CITY, FLORIDA**

DATE: **JUNE 2022**

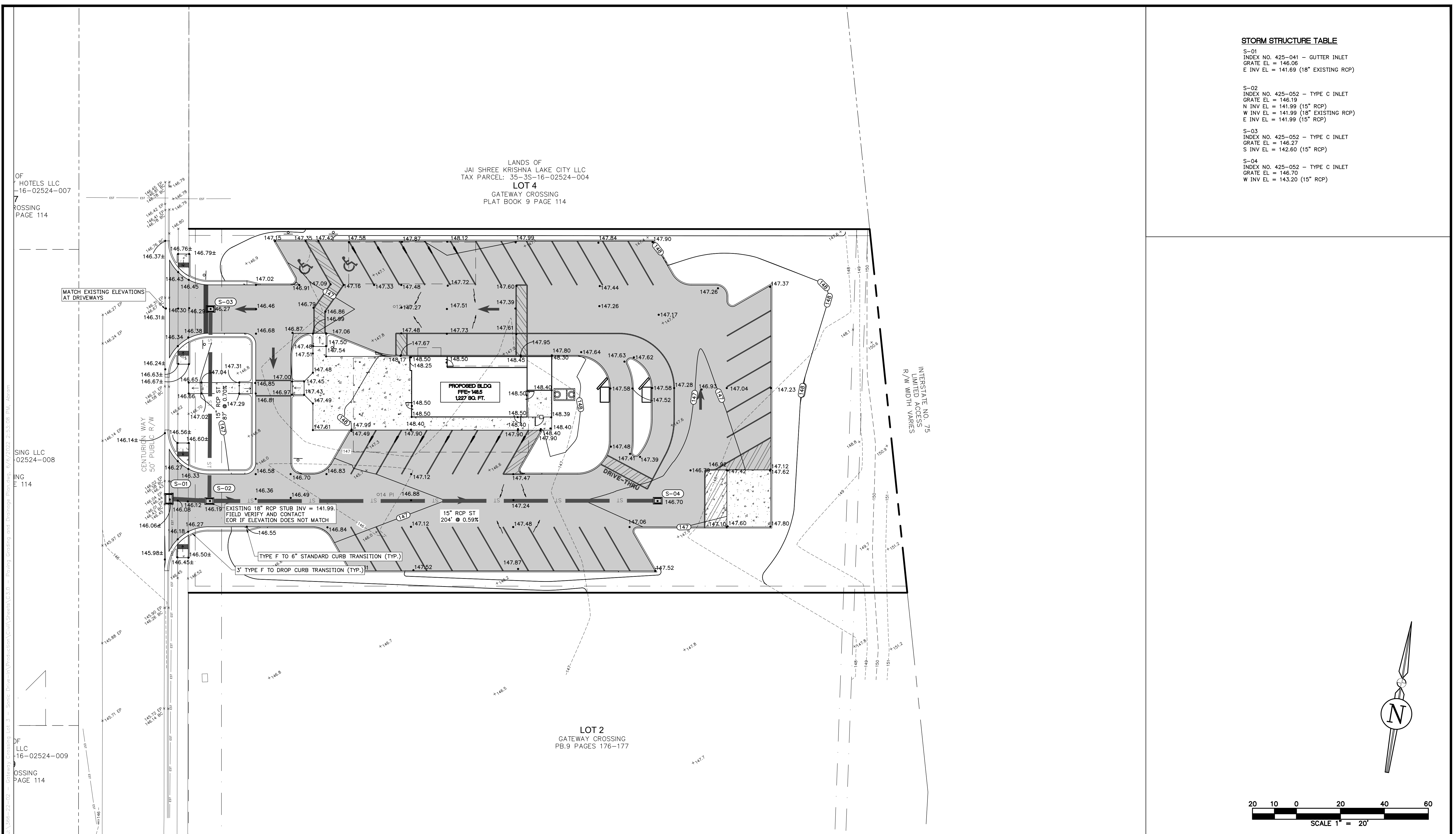
PROJECT NO.: **366-22-02**

SHEET NO.: **C2.0**

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STORM STRUCTURE TABLE

- S-01
INDEX NO. 425-041 - GUTTER INLET
GRATE EL = 146.06
E INV EL = 141.69 (18" EXISTING RCP)
- S-02
INDEX NO. 425-052 - TYPE C INLET
GRATE EL = 146.19
N INV EL = 141.99 (15" RCP)
W INV EL = 141.99 (18" EXISTING RCP)
E INV EL = 141.99 (15" RCP)
- S-03
INDEX NO. 425-052 - TYPE C INLET
GRATE EL = 146.27
S INV EL = 142.60 (15" RCP)
- S-04
INDEX NO. 425-052 - TYPE C INLET
GRATE EL = 146.70
W INV EL = 143.20 (15" RCP)



OF
HOTELS LLC
-16-02524-007
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CROSSING
PAGE 114

LANDS OF
JAI SHREE KRISHNA LAKE CITY LLC
TAX PARCEL: 35-35-16-02524-004
LOT 4
GATEWAY CROSSING
PLAT BOOK 9 PAGE 114

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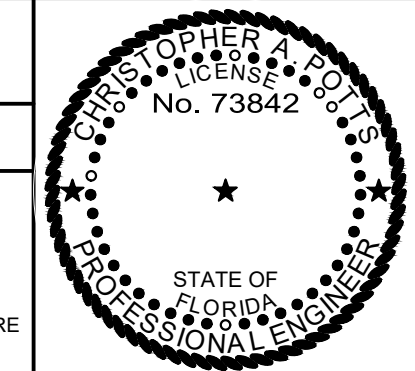
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NO.	DATE	DESCRIPTION		

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SHEET TITLE:
PAVING, GRADING, AND DRAINAGE PLAN

CLIENT:
HIGH COTTON EQUITIES, LLC

PROJECT:
**SONIC RESTAURANT
LAKE CITY, FLORIDA**

DATE:
JUNE 2022

PROJECT NO:
366-22-02

SHEET NO:
C3.0

GENERAL NOTES:

1. ALL POTABLE WATER MAINS SHALL BE AWWA DR-18, C900 PVC, SCH 80 PVC OR DR-11 HDPE.
2. ALL WASTEWATER GRAVITY MAINS SHALL BE ASTM D-1785, SDR 26 PVC. ALL WASTEWATER FORCEMAINS SHALL BE AWWA DR-18, C900 PVC, SCH 80 PVC OR DR-11 HDPE.
3. ALL WATER MAINS SHALL HAVE AT LEAST 30" OF COVER.
4. THE WATER METER SHALL BE THE END OF CITY OF LIVE OAK MAINTENANCE FOR POTABLE WATER.
5. CONTRACTOR TO VERIFY DEPTH OF EXISTING SEWER AT THE PROPOSED CONNECTION POINT PRIOR TO COMMENCEMENT OF UTILITY WORK. IF THE ELEVATION OF THE EXISTING PIPE IS NOT A MINIMUM DEPTH OF 115.76, CONTRACTOR TO CONTACT EOR.

LIGHTING NOTES:

1. ALL EXTERIOR LIGHTING WILL BE DIRECTED SO THAT NO GLARE IS VISIBLE FROM ANY STREET ROW OR FROM ANY ADJACENT RESIDENTIAL PROPERTIES.

WATER FITTING SCHEDULE

- ① 1 - 1" WATER METER ASSEMBLY
1 - 1.5" RP2BFP
- ② 1 - 2" PVC 90 DEGREE BEND

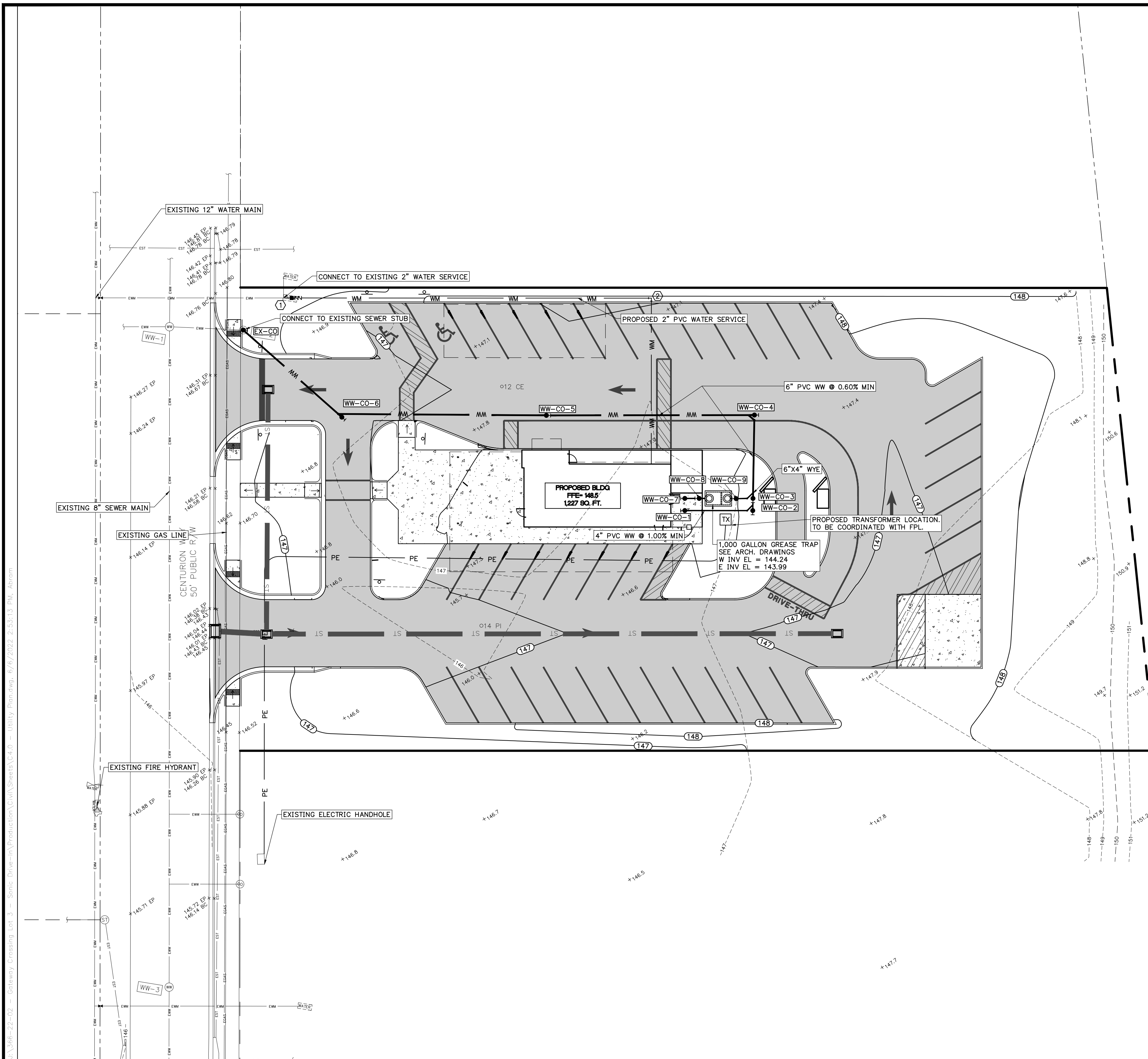
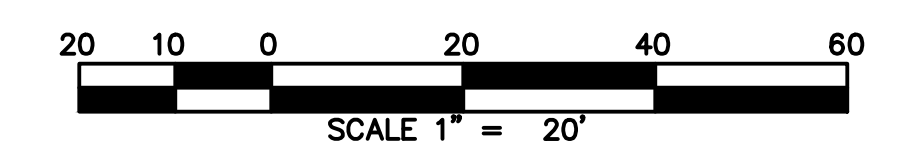
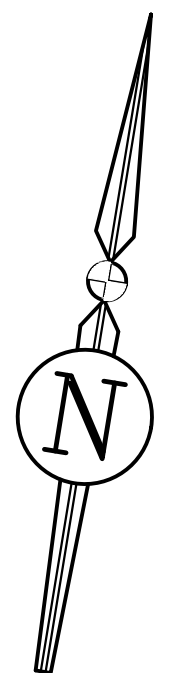
WW CLEANOUT TABLE

WWCO	TOP EL	INV EL
EX-CO	146.78	140.29
WW-CO-1	148.46	144.35
WW-CO-2	147.84	144.02
WW-CO-3	147.84	143.87
WW-CO-4	147.42	143.59
WW-CO-5	147.59	142.82
WW-CO-6	146.72	142.07
WW-CO-7	148.46	144.35
WW-CO-8	148.41	144.27
WW-CO-9	148.02	143.96

WASTEWATER PIPE TABLE

UPSTREAM STRUCTURE	DOWNSTREAM STRUCTURE	SIZE & MATERIAL	LENGTH	SLOPE
WW-CO-1	WW-CO-2	4" PVC	22'	1.48%
WW-CO-2	WW-CO-3	4" PVC	4'	3.45%
WW-CO-3	WW-CO-4	6" PVC	28'	1.00%
WW-CO-4	WW-CO-5	6" PVC	74'	1.04%
WW-CO-5	WW-CO-6	6" PVC	74'	1.01%
WW-CO-6	EX-CO	6" PVC	47'	3.79%
WW-CO-7	WW-CO-8	4" PVC	5'	1.56%
WW-CO-8	WW-GT-IN	4" PVC	2'	1.50%
WW-CO-9	WW-CO-3	4" PVC	4'	2.29%
WW-GT-OUT	WW-CO-9	4" PVC	2'	1.75%

GENERAL NOTE: CLEANOUTS LOCATED IN PAVED AREA(S) WILL HAVE TRAFFIC LOAD BEARING COVERS



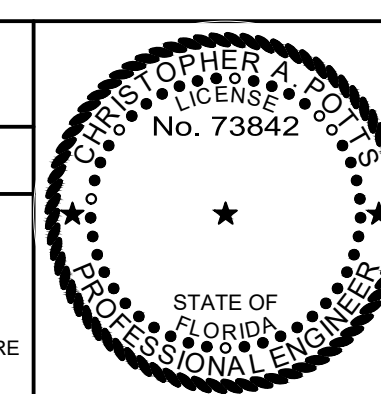
REVISIONS

NO.	DATE	DESCRIPTION	DRWN	APPR

ENGINEER OF RECORD: **CHRISTOPHER A. POTTS, P.E.**
FLORIDA LICENSE NO. 73842

THIS DOCUMENT HAS BEEN ELECTRONICALLY SIGNED AND SEALED BY CHRISTOPHER A. POTTS, PE ON JUNE 06, 2022 USING A DIGITAL SIGNATURE.

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SHEET TITLE: **UTILITY PLAN**

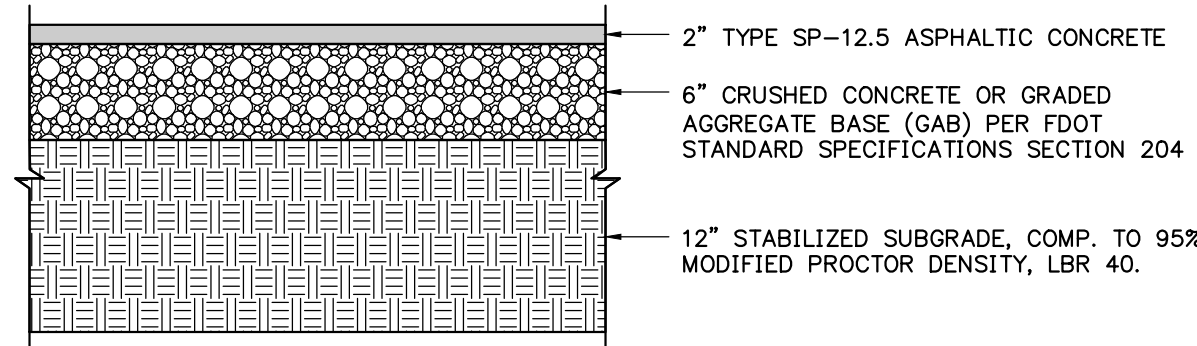
CLIENT: **HIGH COTTON EQUITIES, LLC**

PROJECT: **SONIC RESTAURANT
LAKE CITY, FLORIDA**

DATE: **JUNE 2022**

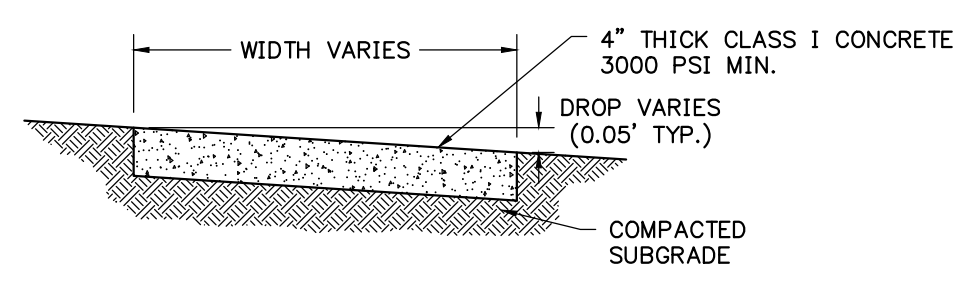
PROJECT NO: **366-22-02**

SHEET NO: **C4.0**



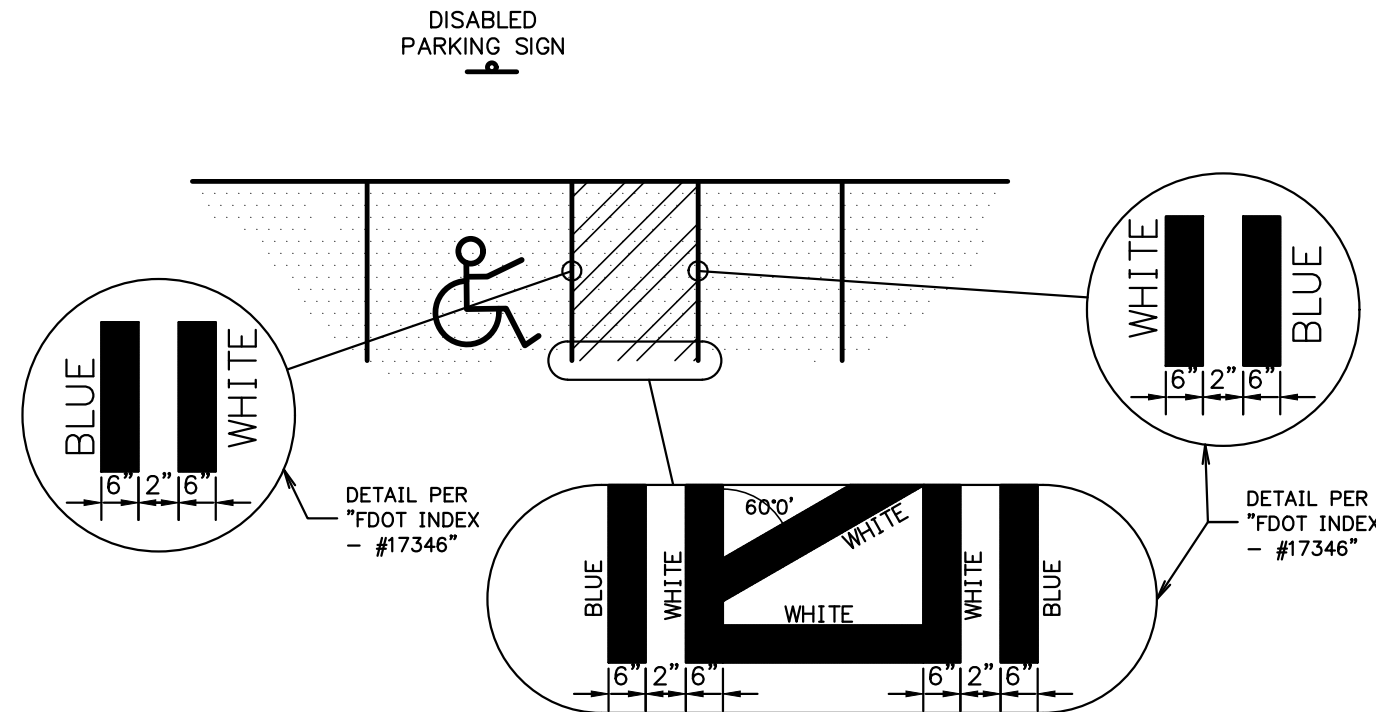
(LIGHT DUTY) TYPICAL ASPHALT PAVEMENT DETAIL

N.T.S.



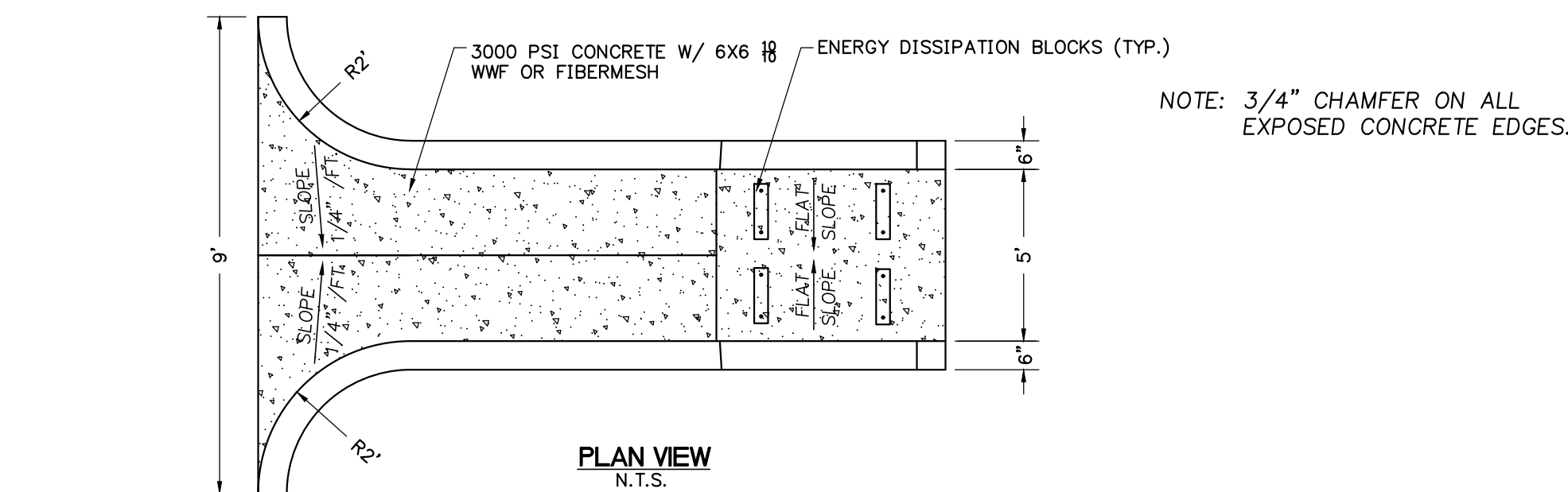
TYPICAL AT GRADE SIDEWALK DETAIL

N.T.S.



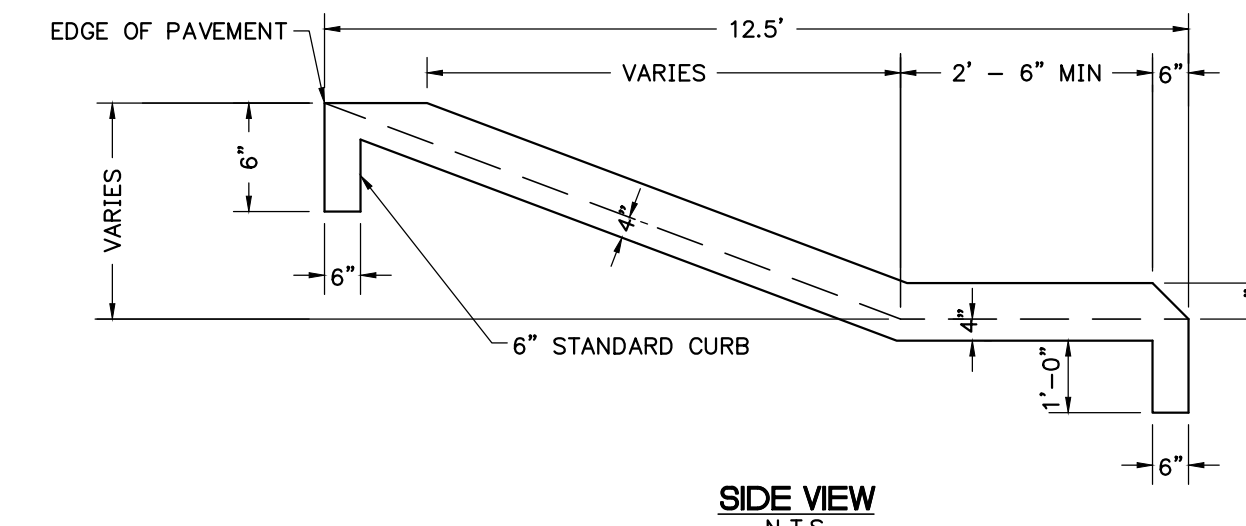
HANDICAP STRIPING DETAIL

N.T.S.



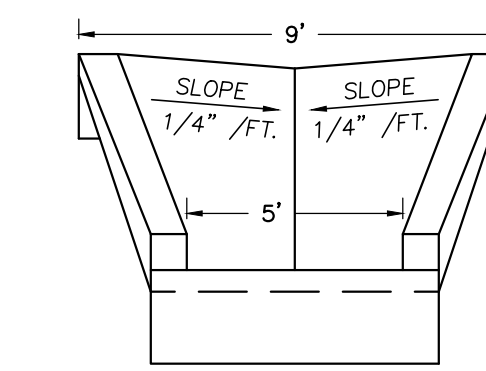
PLAN VIEW

N.T.S.



SIDE VIEW

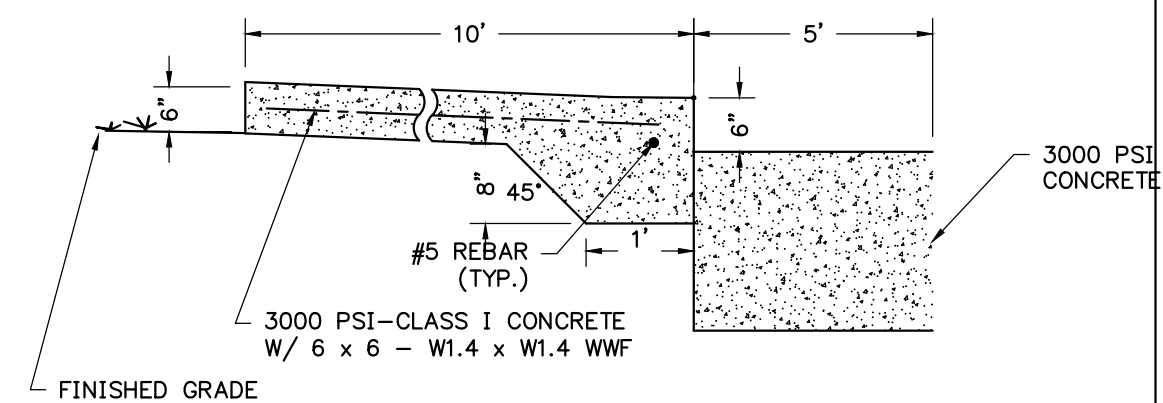
N.T.S.



FRONT VIEW

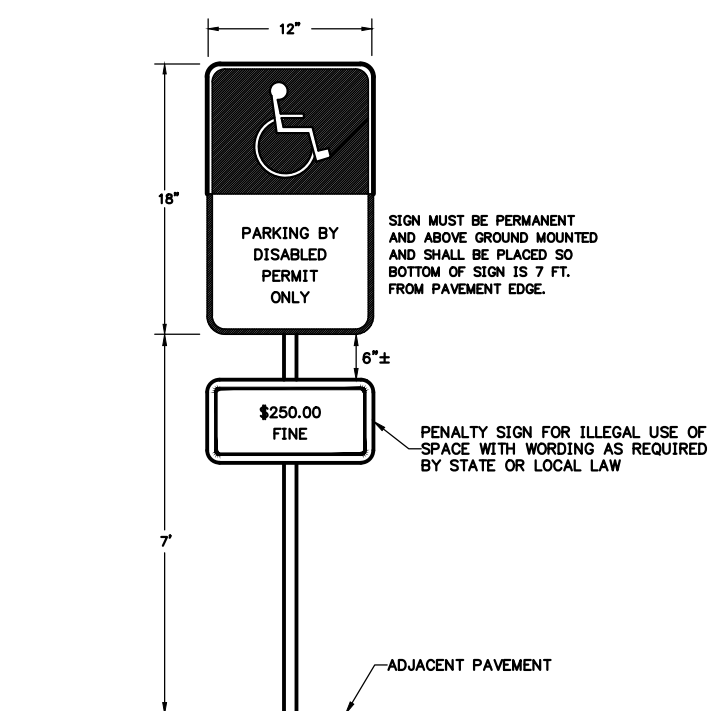
N.T.S.

DRAINAGE/EROSION STANDARD FLUME



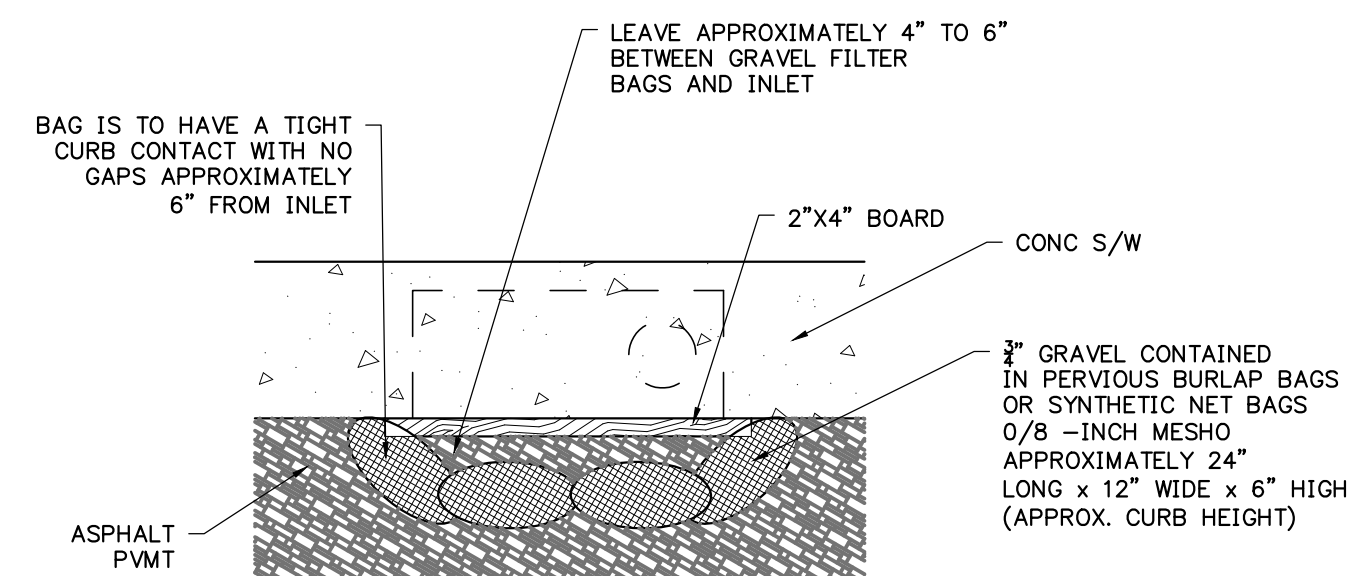
CONCRETE DUMPSTER PAD DETAIL

N.T.S.



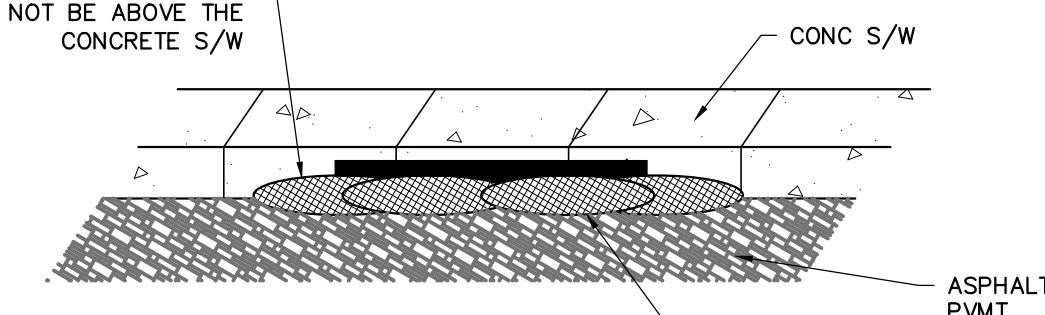
DISABLED PARKING SPACE SIGN DETAIL

N.T.S.



PLAN VIEW

N.T.S.

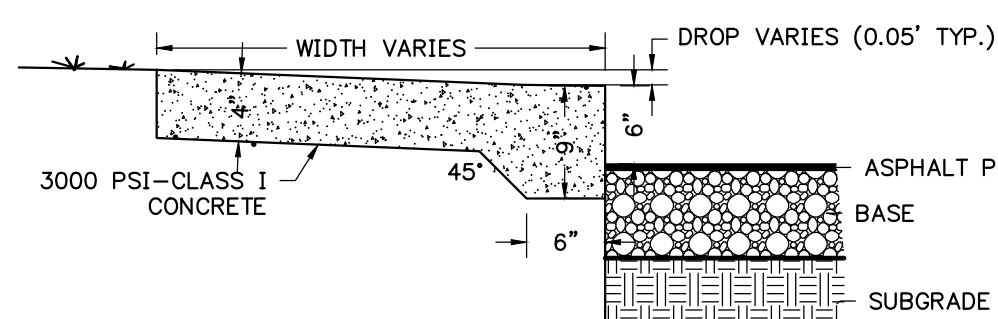


ELEVATION

N.T.S.

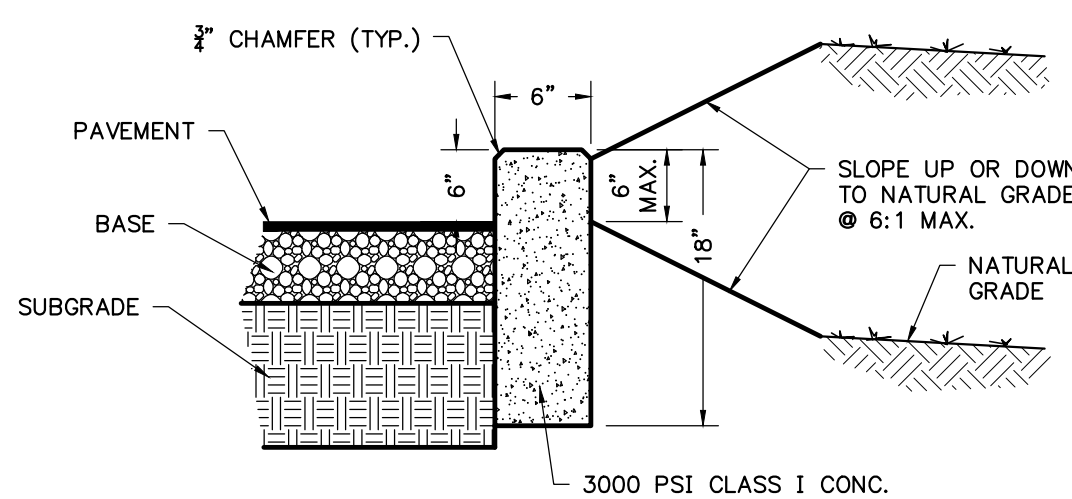
GRAVEL FILTER BAGS DETAIL

N.T.S.



TYPICAL STEP-UP SIDEWALK DETAIL

N.T.S.



STANDARD CURB DETAIL

N.T.S.

NOTE: GRAVEL FILTERS CAN BE USED ON PAVEMENT OR BARE GROUND.

PLACE GRAVEL FILTER BAGS SUCH THAT NO GAPS ARE EVIDENT

HEIGHT OF GRAVEL FILTER BAGS SHOULD NOT BE ABOVE THE CONCRETE S/W

BAG IS TO HAVE A TIGHT CURB CONTACT WITH NO GAPS APPROXIMATELY 6" FROM INLET

LEAVE APPROXIMATELY 4" TO 6" BETWEEN GRAVEL FILTER BAGS AND INLET

CONC S/W

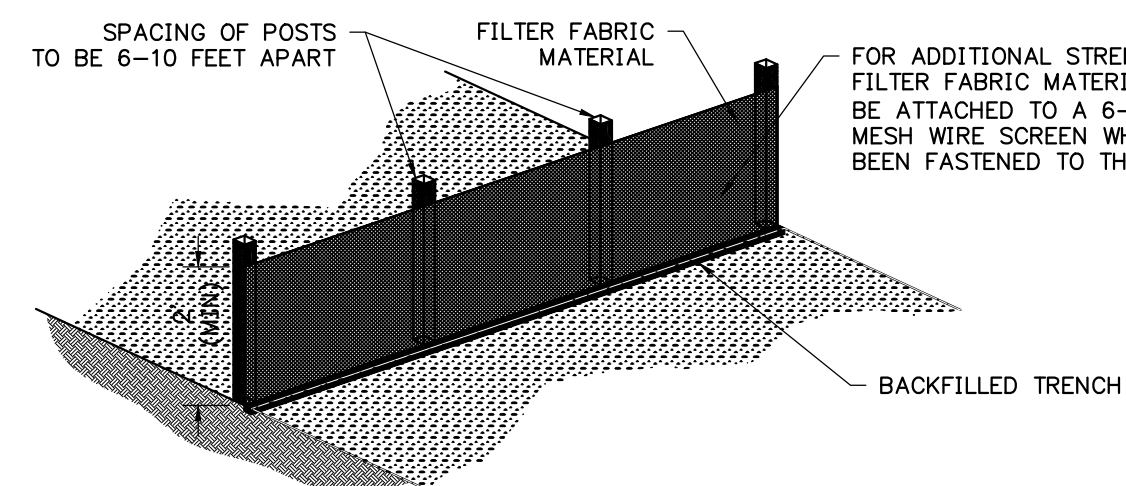
2"x4" BOARD

GRAVEL CONTAINED IN PERVIOUS BURLAP BAGS OR SYNTHETIC NET BAGS 0/8 - INCH MESH

APPROXIMATELY 24" LONG x 12" WIDE x 6" HIGH (APPROX. CURB HEIGHT)

ASPHALT PWMT

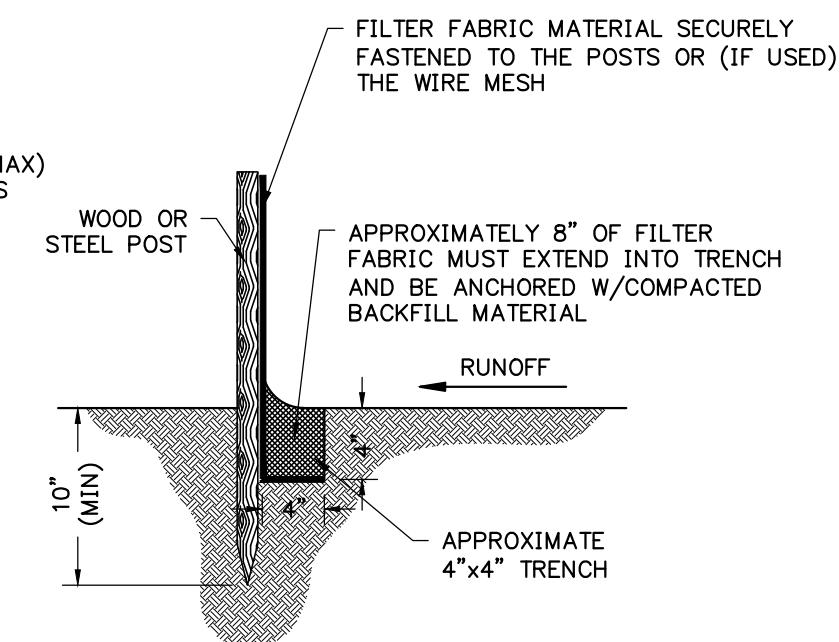
ASPHALT PWMT



TYPE III SILT FENCE

N.T.S.

PER PLATE 4.06D FLORIDA EROSION AND SEDIMENTATION CONTROL INSPECTOR'S MANUAL



STANDARD

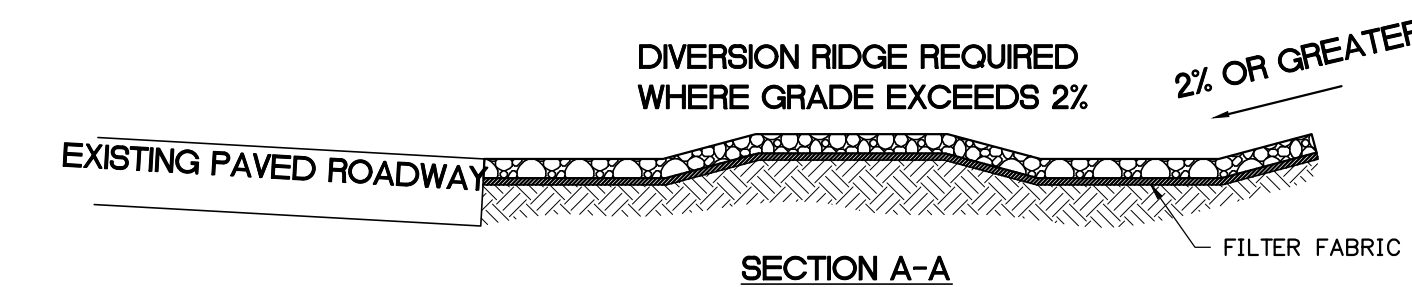
SPILL-OUT

N.T.S.

NOTE: WHEN USED ON THE HIGH SIDE OF ROADWAYS, THE CROSS-SLOPE OF THE GUTTER SHALL MATCH THE CROSS-SLOPE OF THE ADJACENT PAVEMENT AND THE THICKNESS OF THE LIP SHALL BE 6", UNLESS OTHERWISE SHOWN ON THE PLANS.

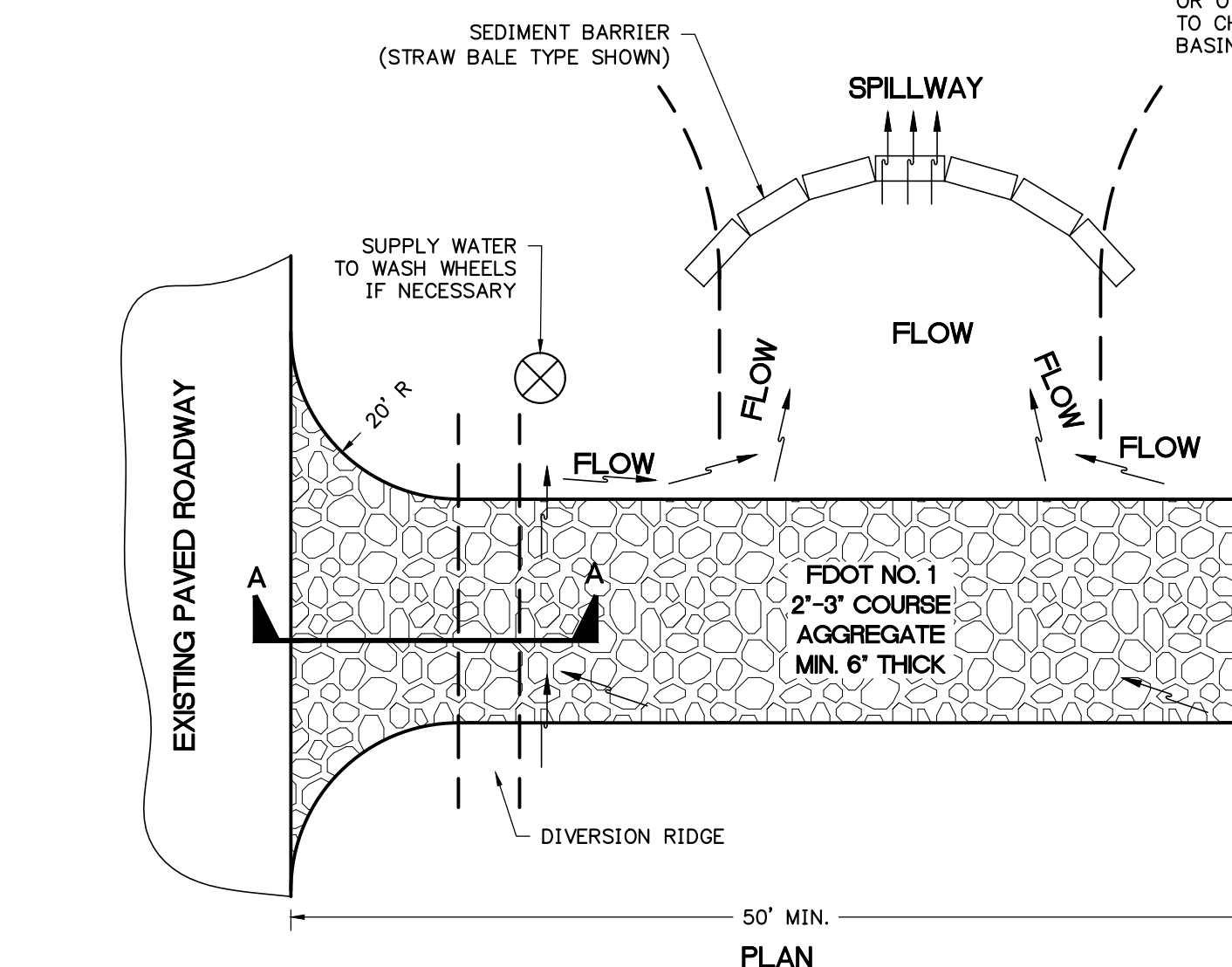
TYPE 'F' CONCRETE CURB AND GUTTER

N.T.S.



SECTION A-A

N.T.S.



TEMPORARY GRAVEL CONSTRUCTION ENTRANCE

N.T.S.

- NOTES:
1. THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION THAT WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHTS-OF-WAY. THIS MAY REQUIRE TOP DRESSING, REPAIR AND/OR CLEANOUT OF ANY MEASURES USED TO TRAP SEDIMENT.
 2. WHEN NECESSARY, WHEELS SHALL BE CLEANED PRIOR TO ENTRANCE ONTO PUBLIC RIGHT-OF-WAY.
 3. WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA STABILIZED WITH CRUSHED STONE THAT DRAINS INTO AN APPROVED SEDIMENT TRAP OR SEDIMENT BASIN.

REVISIONS			
NO.	DATE	DESCRIPTION	DRWN/ APPR

ENGINEER OF RECORD: CHRISTOPHER A. POTTS, P.E. FLORIDA LICENSE NO. 73842

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SHEET TITLE: **DETAILS AND NOTES**

CLIENT: HIGH COTTON EQUITIES, LLC

PROJECT: SONIC RESTAURANT LAKE CITY, FLORIDA

DATE: JUNE 2022

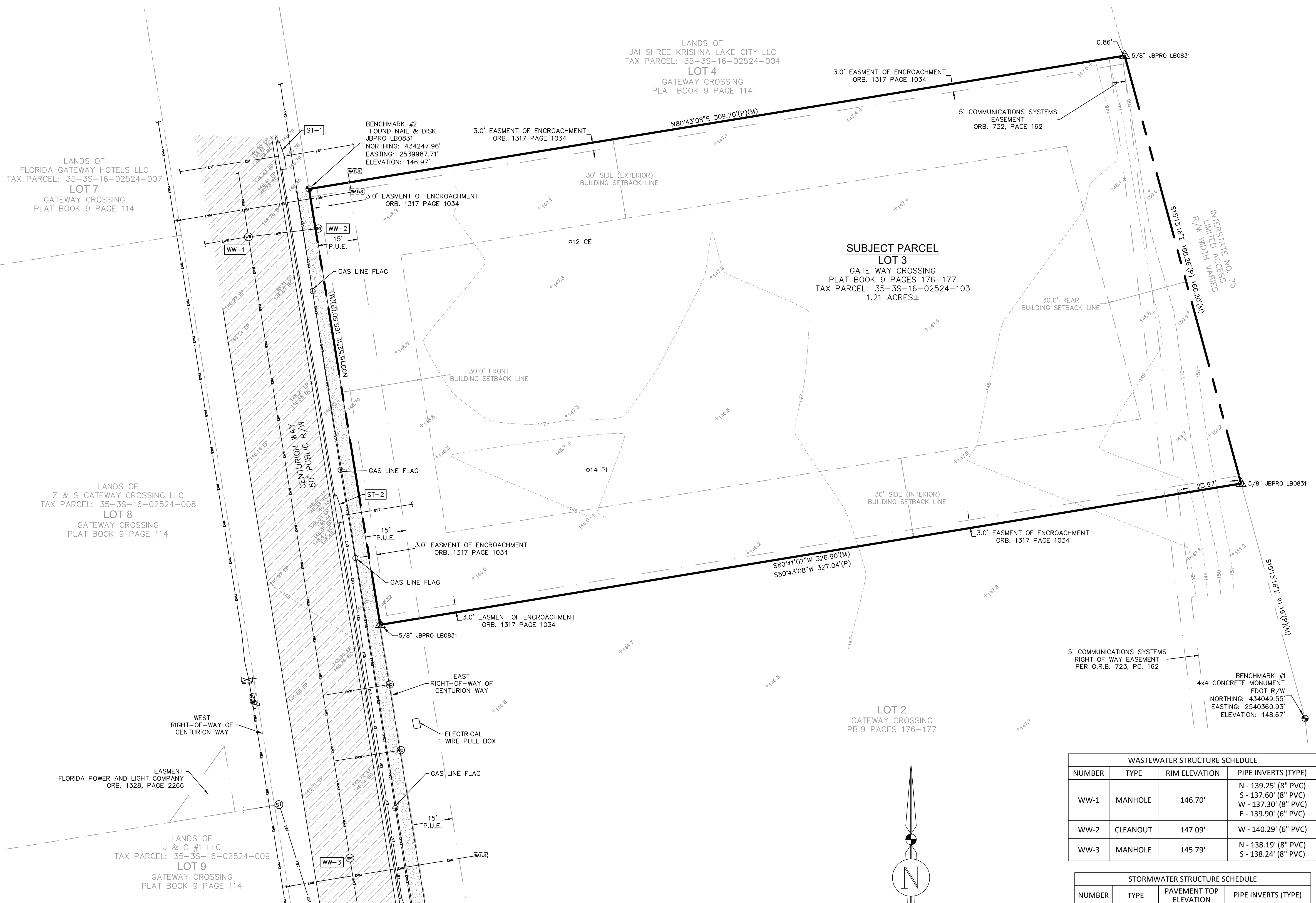
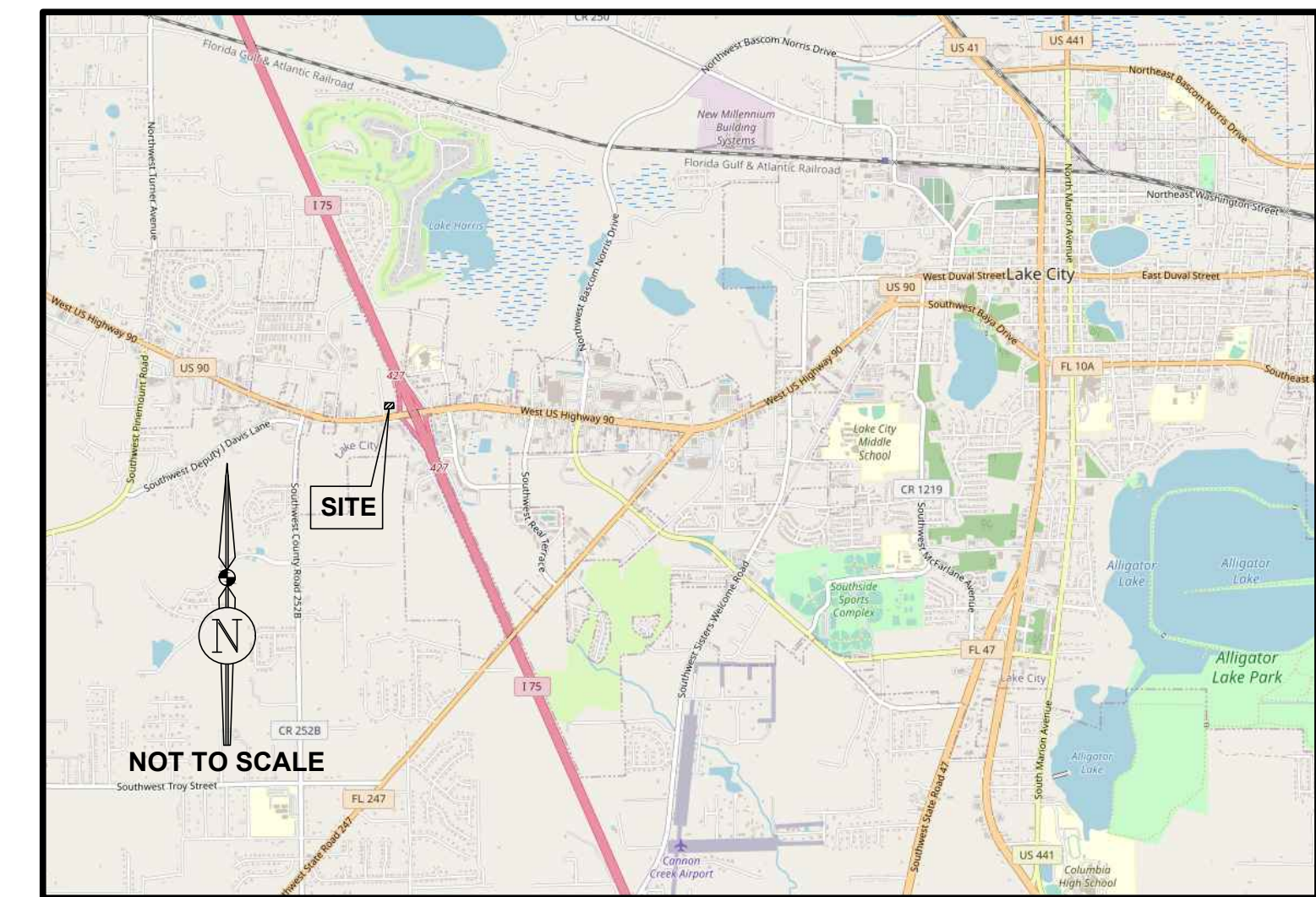
PROJECT NO: 366-22-02

SHEET NO: **C5.0**

BOUNDARY & TOPOGRAPHIC SURVEY

IN SECTION 35, TOWNSHIP 03 SOUTH, RANGE 16 EAST, COLUMBIA COUNTY, FLORIDA

LOCATION MAP



PROPERTY DESCRIPTION

LOT 3 OF A REPLAT OF LOTS 2, 3, & 11 OF GATEWAY CROSSING A REPLAT OF LOTS 2 & 3, ACCORDING TO THE PLAT THEREOF, RECORDED IN PLAT BOOK 9, PAGES 176-177, PUBLIC RECORDS OF COLUMBIA COUNTY, FLORIDA.

ABBREVIATIONS

BC = BACK OF CURB
 EP = EDGE OF PAVEMENT
 LB = LICENSED BUSINESS
 LS = LICENSED SURVEYOR
 (M) = MEASURED
 NAVD83(2011) = NORTH AMERICAN DATUM OF 1983, 2011 ADJUSTMENT
 NAVD88 = NORTH AMERICAN VERTICAL DATUM OF 1988
 (P) = PLATTED
 P.B. = PLAT BOOK
 O.R.B. = OFFICIAL RECORDS BOOK
 PG. = PAGE
 PLS = PROFESSIONAL LAND SURVEYOR
 R/W = RIGHT-OF-WAY

TREES:

CE = CEDAR
 PI = PINE

SYMBOL LEGEND

- BOUNDARY LINE
- - - TAX PARCEL LINE
- - - RIGHT-OF-WAY LINE
- - - STORM SEWER LINE
- - - WASTEWATER LINE
- BENCHMARK
- ▲ CONCRETE MONUMENT
- ▲ IRON ROD - CAPPED
- ▲ NAIL AND DISK
- STORM SEWER MANHOLE
- WASTEWATER MANHOLE
- CLEANOUT
- WATER VALVE COVER
- WATER METER
- ELECTRIC METER
- FIRE HYDRANT
- PUMP
- MARKER FOR UNDERGROUND UTILITY
- TREE-SIZE(INCHES) AND SPECIES
- SPOT ELEVATION - SOFT SURFACE
- SPOT ELEVATION - HARD SURFACE
- CONTOUR LINES
- ASPHALT SURFACE
- CONCRETE SURFACE

SCHEDULE B-II ITEMS

1. NOT A SURVEY MATTER.
2. SEE SURVEY SHOWN HEREON.
3. NOT A SURVEY MATTER.
4. NOT A SURVEY MATTER.
5. EASEMENTS DEPICTED ON THIS SURVEY WERE PROVIDED BY THE CLIENT OR ARE RECORDED IN AN ASSOCIATED PLAT. NO EASEMENT RESEARCH WAS CONDUCTED DURING THE COURSE OF THIS SURVEY.
6. NOT A SURVEY MATTER.
7. SEE SURVEY SHOWN HEREON.
8. THIS ITEM REFERENCES AN OUT OF DATE PLAT, AND THEREFORE IS SUPERSEDED BY B-II ITEM 7.
9. SEE SURVEY SHOWN HEREON.
10. THE MASTER DECLARATION OF EASEMENTS, COVENANTS, CONDITIONS AND RESTRICTIONS FOR GATEWAY CROSSING, RECORDED IN OFFICIAL RECORDS BOOK 1317, PAGE 1034; AS AFFECTED BY AMENDMENT TO DECLARATION OF COVENANTS, CONDITIONS AND RESTRICTIONS GATEWAY CROSSING, RECORDED IN OFFICIAL RECORDS BOOK 1334, PAGE 275, OF THE PUBLIC RECORDS OF COLUMBIA COUNTY, FLORIDA, CONTAINS PROVISIONS FOR THE CREATION OF SEVERAL BLANKET EASEMENTS THAT ARE UNABLE TO BE DRAWN, AND AS SUCH, ARE NOT DEPICTED ON THIS SURVEY MAP. THE EASEMENT OF ENCROACHMENT RESERVES A MAXIMUM OF 3 FEET ON EITHER SIDE OF THE BOUNDARY BETWEEN A PARCEL AND ANY ADJACENT PARCEL OR COMMON AREA FOR MAINTENANCE USE AND ANY PERMITTED ENCROACHMENT. THE PORTION OF THIS EASEMENT AFFECTING THE SUBJECT PARCEL IS SHOWN ON THIS SURVEY MAP.
11. EASEMENT CONTAINED IN INSTRUMENT RECORDED AT OFFICIAL RECORDS BOOK 104, PAGE 118, OF THE PUBLIC RECORDS OF COLUMBIA COUNTY, FLORIDA, ESTABLISHED AN EASEMENT ENCOMPASSING THE ENTIRETY OF THE SUBJECT PARCEL IN FAVOR OF FLORIDA POWER AND LIGHT COMPANY FOR THE CONSTRUCTION OF A UTILITY LINE. THIS DOCUMENT STATES THAT THIS EASEMENT IS TO CONFORM TO FUTURE DEVELOPMENT BY THE OWNERS.
12. EASEMENT CONTAINED IN INSTRUMENT RECORDED AT OFFICIAL RECORDS BOOK 1328, PAGE 2266, OF THE PUBLIC RECORDS OF COLUMBIA COUNTY, IS DEPICTED ON THIS SURVEY MAP.
13. NOT A SURVEY MATTER.
14. NOT A SURVEY MATTER.

SURVEYOR'S NOTES

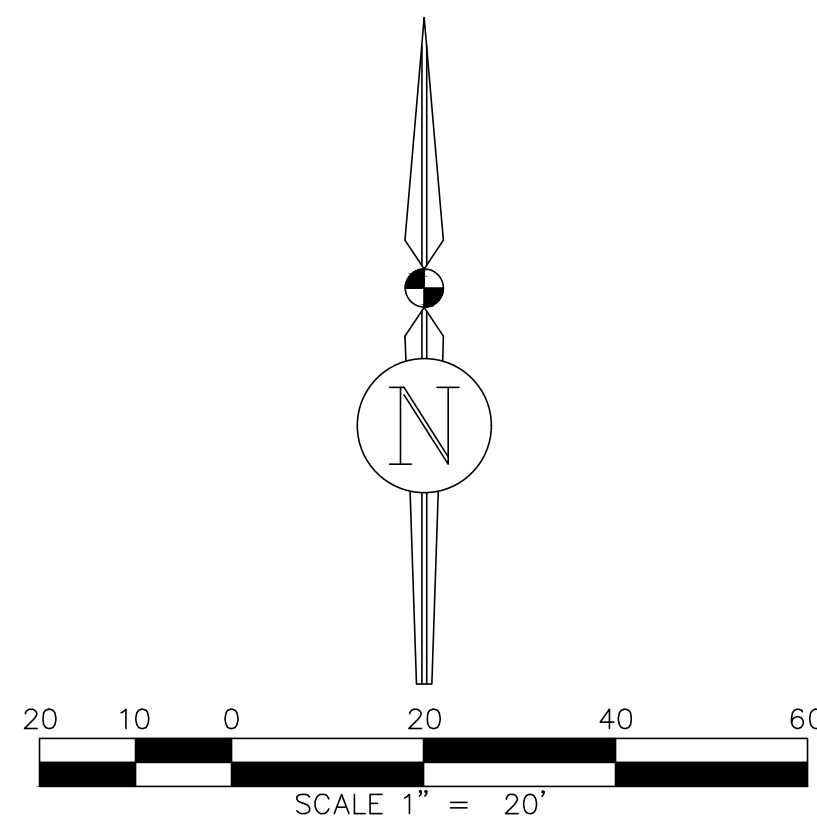
1. BEARINGS ARE BASED ON THE WEST LINE OF THE SUBJECT PARCEL, SAID LINE ALSO BEING THE EAST RIGHT OF WAY LINE OF NW CENTURION COURT BEING N09°16'52"W.
2. ELEVATIONS ARE REFERENCED TO PLAT BENCHMARK #2 HAVING PUBLISHED ELEVATION OF 146.97 FEET PER PLAT BOOK 9, PAGES 176-177, OF THE OFFICIAL RECORDS OF COLUMBIA COUNTY, FLORIDA.
3. STATE PLANE COORDINATES (FLORIDA-NORTH 0903) ARE REFERENCED TO THE NORTH AMERICAN DATUM OF 1983, 2011 ADJUSTMENT AND BASED ON GPS OBSERVATIONS OF THE FLORIDA DEPARTMENT OF TRANSPORTATION CONTINUOUSLY OPERATING REFERENCE STATION(CORS) NETWORK.
4. THIS SURVEY IS BASED ON MEASUREMENTS CONDUCTED ON APRIL 12, 2022.
5. NO UNDERGROUND UTILITIES WERE LOCATED IN THE COURSE OF THIS SURVEY. UNDERGROUND UTILITY LINE LOCATIONS ARE APPROXIMATE.
6. ADDITIONAL ENCUMBRANCES MAY AFFECT THE SUBJECT PARCEL THAT DO NOT APPEAR ON THIS MAP.
7. EASEMENTS DEPICTED ON THIS SURVEY WERE PROVIDED BY THE CLIENT OR ARE RECORDED IN AN ASSOCIATED PLAT. NO EASEMENT RESEARCH WAS CONDUCTED DURING THE COURSE OF THIS SURVEY.
8. REPRODUCED COPIES THAT ARE NOT AT 24"X36" MAY NOT BE TO SCALE.
9. CORNER MARKERS SHOWN HEREON WERE FOUND DURING THE COURSE OF THIS SURVEY UNLESS OTHERWISE NOTED.

BUILDING SETBACKS PER P.B. 9, PG. 177

1. FRONT = 30'
2. REAR = 30'
3. SIDE = 30'

PARKING SPACES

1. TOTAL = 0
2. REGULAR = 0
3. HANDICAPPED = 0



WASTEWATER STRUCTURE SCHEDULE			
NUMBER	TYPE	RIM ELEVATION	PIPE INVERTS (TYPE)
WW-1	MANHOLE	146.70'	N - 139.25' (8" PVC)
			S - 137.60' (8" PVC)
			W - 137.30' (8" PVC)
WW-2	CLEANOUT	147.09'	W - 140.29' (6" PVC)
WW-3	MANHOLE	145.79'	N - 138.19' (8" PVC)
			S - 138.24' (8" PVC)

STORMWATER STRUCTURE SCHEDULE			
NUMBER	TYPE	PAVEMENT TOP ELEVATION	PIPE INVERTS (TYPE)
ST-1	CURB INLET	146.79'	THROAT - 146.42'
			W - 141.57' (24" RCP)
ST-2	CURB INLET	146.43'	THROAT - 146.04'
			S - 141.69' (24" RCP)

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CERTIFICATION

To: 1) HIGH COTTON EQUITIES, LLC. 2) OLD REPUBLIC NATIONAL TITLE INSURANCE COMPANY
 3) KRINZMAN, HUSS & LUBETSKY, LLP

This is to certify that this map or plat and the survey on which it is based were made in accordance with the 2021 Minimum Standard Detail Requirements for ALTA/NSPS Land Title Surveys, jointly established and adopted by ALTA and NSPS, and includes Items 1, 4-6, 8, 9, 13, 18 of Table A thereof. The fieldwork was completed on 12 April 2022.

THE MAP OF THE PROPERTY DESCRIBED HEREON WAS MADE UNDER MY SUPERVISION AND THIS MAP OF SURVEY FURTHER MEETS THE STANDARDS OF PRACTICE SET FORTH BY THE STATE OF FLORIDA BOARD OF PROFESSIONAL SURVEYORS & MAPPERS IN CHAPTER SJ-17.05, FLORIDA ADMINISTRATIVE CODE, PURSUANT TO SECTION 472.027, FLORIDA STATUTES, AND THE MAP OF SURVEY SHOWN HEREON IS A TRUE AND ACCURATE REPRESENTATION THEREOF TO THE BEST OF MY KNOWLEDGE, BEING SUBJECT TO NOTES AND NOTATIONS SHOWN HEREON.

Florida License No. LS4816
 Certificate of Authorization No. LB8031

NOT VALID WITHOUT THE SIGNATURE & ORIGINAL RAISED SEAL OF A FLORIDA LICENSED SURVEYOR & MAPPER OR VALID DIGITAL SIGNATURE IN ELECTRONIC FORM

Richard L. White
 Professional Land Surveyor

ALTA/NSPS LAND TITLE SURVEY

CERTIFIED TO: SEE ALTA CERTIFICATION AT LEFT

Scale: 1"=20'
 Proj. No.: 366-22-02
 Drawn: J. Trisham
 Checked: R. White
 Dwg. Name: 366-22-02-BT
 Dwg. Date: 05-06-2022
 Field Book: N/A
 Pages: N/A

Sheet: 1 of 1



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Surveying | Construction Services

Gainesville | St. Augustine
3530 NW 43rd Street
Gainesville, FL 32606

Suwannee River Water Management District
9225 CR 45
Live Oak, FL 32060

June 6, 2022

Subject: Sonic Drive-In restaurant

The Sonic Drive-In restaurant project is located on parcel number 35-3S-16-02524-103 located on NW Centurion Ct, approximately 600 feet north of the intersection of NW Centurion Ct and US HWY 90. The project proposes a new 1,227 SF restaurant building with associated parking and utilities.

The project site is part of a larger development which was previously permitted with Suwannee River Water Management District (SRWMD) under the project name Gateway Crossing and permit number 226410-1.

The proposed site will direct water to inlet structures in the parking lot, which will direct runoff to the master stormwater facility. The permit allows for 75% impervious on lots 1-4 and 7-10. Please see the attached post development exhibit of the current master development.

The Sonic Restaurant project proposes 31,216 SF or 59.22% of impervious area on the 1.21+/- Ac. Site located on Lot 3. We are requesting a letter of conformance with permit number 226410-1.

Sincerely,

Christopher A. Potts, P.E.
Director of Civil Engineering, JBPro



EXISTING STORM SEWER STRUCTURE SCHEDULE

STRUCTURE NO.	DESCRIPTION	LENGTH	DEPTH	DIAMETER	INVERT ELEVATION	OUTLET ELEVATION	MANHOLE ELEVATION
DA-1	18.77 AC	18.77	3.0	36"	111.22	110.22	111.22
DA-2	7.52 AC	7.52	3.0	36"	111.11	110.11	111.11
DA-W	20.83 AC	20.83	3.0	36"	111.11	110.11	111.11

STORM SEWER STRUCTURE SCHEDULE

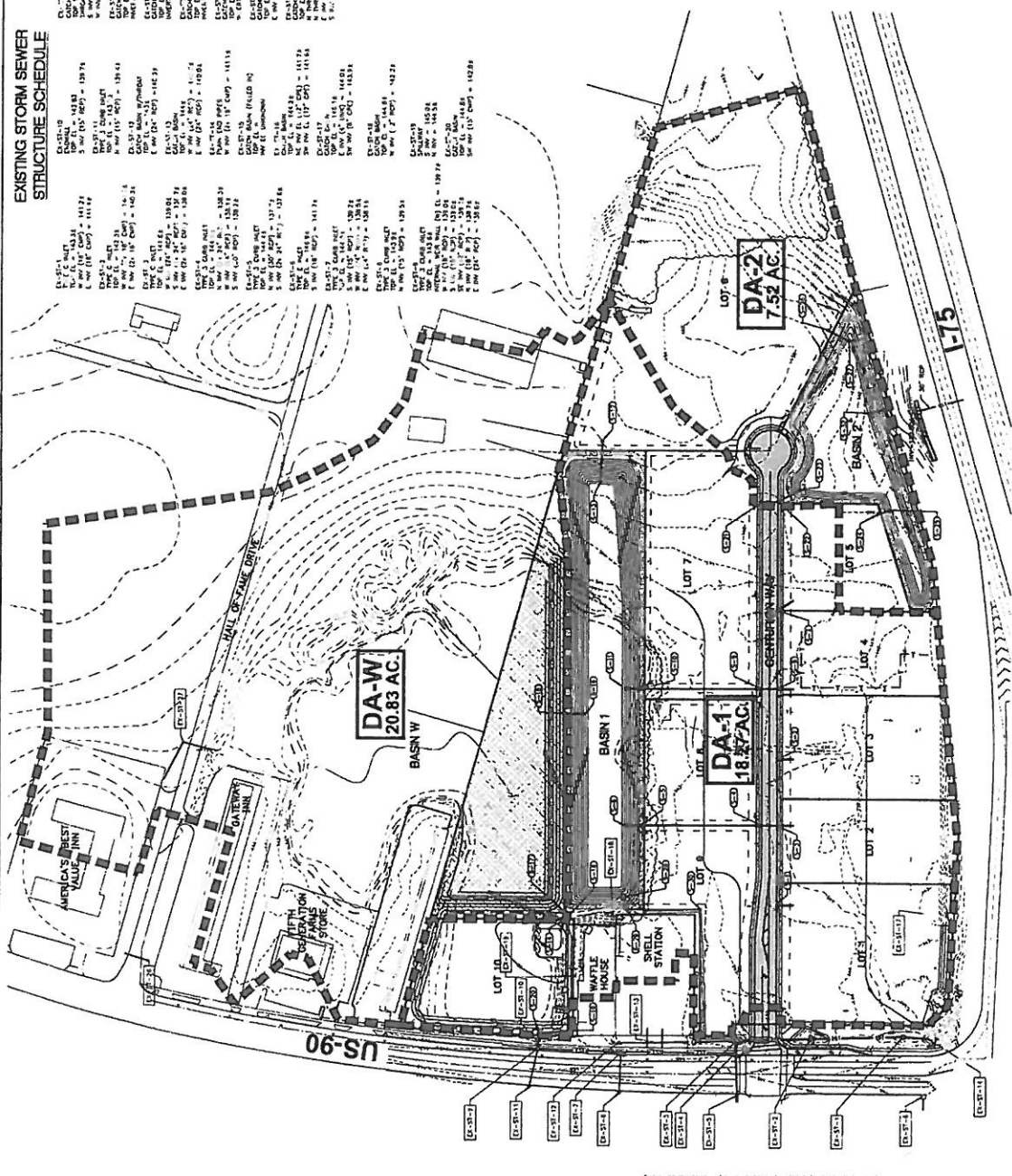
STRUCTURE NO.	DESCRIPTION	LENGTH	DEPTH	DIAMETER	INVERT ELEVATION	OUTLET ELEVATION	MANHOLE ELEVATION
DA-1	18.77 AC	18.77	3.0	36"	111.22	110.22	111.22
DA-2	7.52 AC	7.52	3.0	36"	111.11	110.11	111.11
DA-W	20.83 AC	20.83	3.0	36"	111.11	110.11	111.11

STORM SEWER PIPING TABLE

PIPE NO.	START	END	LENGTH	DEPTH	DIAMETER	INVERT ELEVATION	OUTLET ELEVATION	MANHOLE ELEVATION
1	DA-1	DA-2	11.25	3.0	36"	111.22	110.22	111.22
2	DA-2	DA-W	7.52	3.0	36"	111.11	110.11	111.11

STORM SEWER PIPING TABLE

PIPE NO.	START	END	LENGTH	DEPTH	DIAMETER	INVERT ELEVATION	OUTLET ELEVATION	MANHOLE ELEVATION
3	DA-W	DA-1	20.83	3.0	36"	111.11	110.11	111.11
4	DA-1	DA-2	11.25	3.0	36"	111.22	110.22	111.22



REVISIONS	DATE	DESCRIPTION

PROJECT MANAGER	DATE	SCALE

PRELIMINARY FOR REVIEW ONLY

38251 NW 40th Street • Corvallis, Oregon 97330
Professional Group Inc
 503.755.1111
 www.jbrown-engineers.com
 P.E. Board of Professional Engineers CA No. 30496

GATEWAY CROSSING
GWC DEVELOPMENT PARTNERS, LLC

POST-DEVELOPMENT DRAINAGE AREA MAP

SHEET NUMBER C4.2

LEGAL DESCRIPTION (35-3S-16-02524-103)

LOT 3 GATEWAY CROSSING S/D A REPLAT OF LOTS 2 & 3. NKA LOT 3 A REPLAT OF LOTS 2,3 & 11 OF GATEWAY CROSSING IN PLAT BK 9 PGS 176 & 177.

This Instrument Was Prepared By,
Record and Return to:

John Hotte, Esquire
Krinzman, Huss & Lubetsky
110 SE 6th Street, 20th Floor
Fort Lauderdale, FL 33301
Telephone: (954) 761-3454

Property Appraiser
Identification No.:
Consideration:\$ _____

Inst:201612000647 Date:1/14/2016 Time:10:37 AM

Doc: Stamp-Deed:19775.00

DC,P.DeWitt Cason,Columbia County Page 1 of 4 B:1307 P:1888

SPECIAL WARRANTY DEED

THIS SPECIAL WARRANTY DEED made this 12th day of January, 2016, by **Inn of Lake City, Inc.**, a Florida corporation ("Grantor"), whose mailing address is 1000 Red Fern Place, Flowood, MS 39232 in favor of **GWC Development Partners, LLC**, a Florida limited liability company ("Grantee"), whose mailing address is 2682 West Noegel Road, Lake City, FL 32055.

WITNESSETH:

That Grantor, for and in consideration of the sum of TEN DOLLARS (\$10.00) and other good and valuable consideration paid by Grantee, the receipt and sufficiency whereof are hereby acknowledged, does hereby grant, bargain, sell, alien, remise, release, convey and confirm unto Grantee the real property (the "Property") located in Columbia County, Florida, and more particularly described in Exhibit "A" attached hereto and made a part hereof.

SUBJECT ONLY TO the matters set forth in Exhibit "B" attached hereto and made a part hereof.

TOGETHER with all the tenements, hereditaments and appurtenances belonging or in any way appertaining to the Property, including, without limitation, all of Grantor's right, title and interest, if any, in and to all of the easements, rights, and privileges belonging or in any way appertaining to the Property and/or improvements located thereon.

TO HAVE AND TO HOLD the same in fee simple forever.

AND GRANTOR hereby covenants with Grantee that Grantor is lawfully seized of the Property in fee simple; that Grantor has good right and lawful authority to sell and convey the Property; that, subject to the matters described on Exhibit "B" attached hereto, Grantor does hereby warrant specially the title to the Property; and that Grantor and its successors and assigns will forever warrant and defend the same against the lawful claims of all persons claiming by, through or under Grantor, but against none other.

IN WITNESS WHEREOF, Grantor has caused this Special Warranty Deed to be executed by its duly authorized representative on the day and year first above written.

Two Witnesses:

Inn of Lake City, Inc., a Florida corporation

Debra D. Hardwick
Printed Name: Debra D. Hardwick

By: *Michael J. Hart*
Michael J. Hart, Vice President, Treasurer
and Assistant Secretary

John M. Stephens IV
Printed Name: John M. Stephens IV

STATE OF Mississippi
COUNTY OF Hinds

The foregoing instrument was acknowledged before me this 12 day of January, 2016, by Michael J. Hart, as Vice President, Treasurer and Assistant Secretary of Inn of Lake City, Inc., a Florida corporation, on behalf of the corporation. He is personally known to me.

Suzanna Baker
Notary Public, State of Mississippi
Print Name: Suzanna Baker
Commission No.: 83877
My Commission Expires: January 21, 2019
[Affix Notary Seal]



Signature Page of Special Warranty Deed

EXHIBIT "A"

Real Property Description

COMMENCE at the Northwest corner of Section 35, Township 3 South, Range 16 East, Columbia County, Florida as established by B.G. Moore, PLS No. 439 and run thence S 06°22'00" W, along the West line of said Section 35, 1894.50 feet to the West Limited Access Right of Way of Interstate No. 75, thence run Southerly and Westerly along said West Limited Access Right of Way the following courses. S 24°54'32" E, 472.32 feet to the POINT OF BEGINNING, S 24°54'32" E, 940.25 feet; S 15°12'50" E, 512.06 feet; S 06°01'43" E, 335.81 feet; S 36°55'36" W, 54.60 feet to the Northerly Right of Way of West U.S. Highway 90 and the end of said courses; thence S 80°47'35" W, along said Northerly Right of Way, 371.77 feet; thence S 08°51'10" E, along said Northerly Right of Way, 22.18 feet; thence S 80°47'36" W, along said Northerly Right of Way, 73.15 feet; thence N 08°55'17" W, 150.09 feet; thence S 80°42'55" W, 150.25 feet; thence N 08°52'22" W, 60.12 feet; thence S 80°53'59" W, 79.99 feet; thence S 08°59'18" E, 210.15 feet to the aforesaid Northerly Right of Way; thence S 80°47'36" W, along said Northerly Right of Way, 26.39 feet to a point of a curve; thence run Westerly along the arc of said curve concave to the North having a radius of 3224.04 feet, a central angle of 05°24'20", a chord bearing and distance of S 83°26'26" W 304.06 feet, an arc distance of 304.18 feet to the aforesaid West line of Section 35; thence N 06°22'00" E, along said West line, 1784.01 feet; thence N 65°09'42" E, 286.69 feet to the POINT OF BEGINNING.

LESS AND EXCEPT the parcel described in O.R. Book 1284, Page 229, of the Official Records of Columbia County, Florida

EXHIBIT "B"

Exceptions

1. Taxes and assessments for the year 2016 and subsequent years, which are not yet due and payable.
2. Any land use, zoning and building laws and ordinances.
3. Any declaration of covenants, conditions and restrictions, or other recorded restrictions.
4. Any right, title, interest, claim, violation, variation, encumbrance, encroachment, fact, matters or other adverse circumstance affecting title revealed, or that should have been revealed, by that certain ALTA/ACSM survey of the Property by JBrown Professional Group Inc. dated October 16, 2015, as revised (Proj. No. 366-15-01).
5. Any obligations, rights and other matters related to, and any agreements with and requirements of the State of Florida or other governmental agency regarding, the remediation of certain environmental issues on the Property by or on behalf of the State of Florida or a political subdivision thereof under a state-funded cleanup program(s).
6. Rights-of-way, utility easements, other easements, restrictions and other restrictive and/or use covenants filed of record and other matters which are revealed by a title search or title commitments, including the following:
 - a. Easement(s) in favor of Mississippi Management, Inc. set forth in instrument(s) recorded in Official Records Book 634, Page 338.
 - b. Easement(s) in favor of Shell Oil Company set forth in instrument(s) recorded in Official Records Book 674, Page 104.
 - c. Easement(s) in favor of American Telephone and Telegraph Company set forth in instrument(s) recorded in Official Records Book 723, Page 162.
 - d. Easement(s) in favor of The City of Lake City, Florida set forth in instrument(s) recorded in Official Records Book 776, Page 1724.
 - e. Easement contained in Deed recorded in Official Records Book 685, Page 38.
 - f. Easement recorded in Official Records Book 960, Page 1492.
 - g. Easement recorded in Official Records 104, Page 118, and in Official Records Book 361, Page 499.
 - h. Easement for ingress and egress recorded in Official Records Book 370, Page 337.
 - i. Easement(s) in favor of Florida Power and Light Company set forth in instrument(s) recorded in Official Records Book 361, Page 499.
 - j. Easement(s) in favor of The City of Lake City, Florida set forth in instrument(s) recorded in Official Records Book 559, Page 229.

Columbia County Tax Collector

generated on 5/31/2022 3:59:36 PM EDT

Tax Record

Last Update: 5/31/2022 3:58:20 PM EDT



Ad Valorem Taxes and Non-Ad Valorem Assessments

The information contained herein does not constitute a title search and should not be relied on as such.

Account Number		Tax Type		Tax Year	
R02524-103		REAL ESTATE		2021	
Mailing Address			Property Address		
GWC DEVELOPMENT PARTNERS LLC 2682 NW NOEGEL RD LAKE CITY FL 32055			GEO Number 353S16-02524-103		
Exempt Amount		Taxable Value			
See Below		See Below			
Exemption Detail		Millage Code		Escrow Code	
NO EXEMPTIONS		001			
Legal Description (click for full description)					
35-3S-16 1000/1000.82 Acres LOT 3 GATEWAY CROSSING S/D A REPLAT OF LOTS 2 & 3.					
Ad Valorem Taxes					
Taxing Authority	Rate	Assessed Value	Exemption Amount	Taxable Value	Taxes Levied
BOARD OF COUNTY COMMISSIONERS	7.8150	412,274	0	\$412,274	\$3,221.92
CITY OF LAKE CITY	4.9000	412,274	0	\$412,274	\$2,020.14
COLUMBIA COUNTY SCHOOL BOARD					
DISCRETIONARY	0.7480	412,274	0	\$412,274	\$308.38
LOCAL	3.6430	412,274	0	\$412,274	\$1,501.91
CAPITAL OUTLAY	1.5000	412,274	0	\$412,274	\$618.41
SUWANNEE RIVER WATER MGT DIST	0.3615	412,274	0	\$412,274	\$149.04
LAKE SHORE HOSPITAL AUTHORITY	0.0000	412,274	0	\$412,274	\$0.00
Total Millage		18.9675	Total Taxes		\$7,819.80
Non-Ad Valorem Assessments					
Code	Levying Authority				Amount
XLCF	CITY FIRE ASSESSMENT				\$50.40
Total Assessments					\$50.40
Taxes & Assessments					\$7,870.20
If Paid By				Amount Due	
				\$0.00	

Date Paid	Transaction	Receipt	Item	Amount Paid
12/29/2021	PAYMENT	1200971.0005	2021	\$7,634.09

Prior Years Payment History

Prior Year Taxes Due
NO DELINQUENT TAXES

Gateway Crossing – Lot 3 Sonic Drive-In Lake City, FL

Fire Flow Calculations

June 2022



Christopher A. Potts, P.E.
FL Registration No. 73842

3530 NW 43rd Street
Gainesville, FL 32606
(352) 375-8999
www.jbpro.com

JBPro



Attachments

Attachment A: NFPA Fire Flow Calculations

Attachment B: ISO Fire Flow Calculations

Attachments C: City of Lake City Fire Flow



Attachment A
NFPA Fire Flow Calculations

(Exhibit on Next Page)



FIRE FLOW CALCULATIONS PER NFPA 2009

BUILDING CONSTRUCTION:	Type V (000)
FIRE FLOW AREA (Total Floor Area):	1,227 SF (1 Floor)
FIRE FLOW REQUIRED:	1,500 GPM
BUILDING FULLY SPRINKLED?	No
FLOW DURATION:	2 Hours
AVAILABLE FIRE FLOW (@ 20 psi):	2,874 GPM @ 20 psig

Table 18.4.5.1.2 Minimum Required Fire Flow and Flow Duration for Buildings

Fire Flow Area ft ² (× 0.0929 for m ²)					Fire Flow gpm [†] (× 3.785 for L/min)	Flow Duration (hours)
I(443), I(332), II(222)*	II(111), III(211)*	IV(2HH), V(111)*	II(000), III(200)*	V(000)*		
0-22,700	0-12,700	0-8200	0-5900	0-3600	1500	2
22,701-30,200	12,701-17,000	8201-10,900	5901-7900	3601-4800	1750	
30,201-38,700	17,001-21,800	10,901-12,900	7901-9800	4801-6200	2000	
38,701-48,300	21,801-24,200	12,901-17,400	9801-12,600	6201-7700	2250	
48,301-59,000	24,201-33,200	17,401-21,300	12,601-15,400	7701-9400	2500	
59,001-70,900	33,201-39,700	21,301-25,500	15,401-18,400	9401-11,300	2750	3
70,901-83,700	39,701-47,100	25,501-30,100	18,401-21,800	11,301-13,400	3000	
83,701-97,700	47,101-54,900	30,101-35,200	21,801-25,900	13,401-15,600	3250	
97,701-112,700	54,901-63,400	35,201-40,600	25,901-29,300	15,601-18,000	3500	
112,701-128,700	63,401-72,400	40,601-46,400	29,301-33,500	18,001-20,600	3750	
128,701-145,900	72,401-82,100	46,401-52,500	33,501-37,900	20,601-23,300	4000	4
145,901-164,200	82,101-92,400	52,501-59,100	37,901-42,700	23,301-26,300	4250	
164,201-183,400	92,401-103,100	59,101-66,000	42,701-47,700	26,301-29,300	4500	
183,401-203,700	103,101-114,600	66,001-73,300	47,701-53,000	29,301-32,600	4750	
203,701-225,200	114,601-126,700	73,301-81,100	53,001-58,600	32,601-36,000	5000	
225,201-247,700	126,701-139,400	81,101-89,200	58,601-65,400	36,001-39,600	5250	4
247,701-271,200	139,401-152,600	89,201-97,700	65,401-70,600	39,601-43,400	5500	
271,201-295,900	152,601-166,500	97,701-106,500	70,601-77,000	43,401-47,400	5750	
Greater than 295,900	Greater than 166,500	106,501-115,800	77,001-83,700	47,401-51,500	6000	
		115,801-125,500	83,701-90,600	51,501-55,700	6250	
		125,501-135,500	90,601-97,900	55,701-60,200	6500	
		135,501-145,800	97,901-106,800	60,201-64,800	6750	
		145,801-156,700	106,801-113,200	64,801-69,600	7000	
156,701-167,900	113,201-121,300	69,601-74,600	7250	4		
167,901-179,400	121,301-129,600	74,601-79,800	7500			
179,401-191,400	129,601-138,300	79,801-85,100	7750			
		Greater than 191,400	Greater than 138,300	Greater than 85,100	8000	

*Types of construction are based on NFPA 220.

†Measured at 20 psi (139.9 kPa).

18.4 Fire Flow Requirements for Buildings.

18.4.1* Scope.

18.4.1.1* The procedure determining fire flow requirements for buildings hereafter constructed shall be in accordance with Section 18.4.

18.4.1.2 Section 18.4 does not apply to structures other than buildings.

18.4.2 Definitions. See definitions 3.3.13.6 (Fire Flow Area) and 3.3.108 (Fire Flow).

18.4.3 Modifications.

18.4.3.1 Decreases. Fire flow requirements shall be permitted to be modified downward by the AHJ for isolated buildings or a group of buildings in rural areas or small communities where the development of full fire flow requirements is impractical.

18.4.3.2 Increases. Fire flow shall be permitted to be modified upward by the AHJ where conditions indicate an unusual susceptibility to group fires or conflagrations. An upward modification shall not be more than twice that required for the building under consideration.

18.4.4 Fire Flow Area.

18.4.4.1 General. The fire flow area shall be the total floor area of all floor levels of a building except as modified in 18.4.4.1.1.

18.4.4.1.1 Type I (443), Type I (332), and Type II (222) Construction. The fire flow area of a building constructed of Type I (443), Type I (332), and Type II (222) construction shall be the area of the three largest successive floors.

18.4.5 Fire Flow Requirements for Buildings.

18.4.5.1 One- and Two-Family Dwellings.

18.4.5.1.1 The minimum fire flow and flow duration requirements for one- and two-family dwellings having a fire flow area that does not exceed 5000 ft² (334.5 m²) shall be 1000 gpm (3785 L/min) for 1 hour.

18.4.5.1.1.1 A reduction in required fire flow of 50 percent shall be permitted when the building is provided with an approved automatic sprinkler system.

18.4.5.1.1.2 A reduction in the required fire flow of 25 percent shall be permitted when the building is separated from other buildings by a minimum of 30 ft (9.1 m).

18.4.5.1.1.3 The reduction in 18.4.5.1.1.1 and 18.4.5.1.1.2 shall not reduce the required fire flow to less than 500 gpm (1900 L/min).

18.4.5.1.2 Fire flow and flow duration for dwellings having a fire flow area in excess of 5000 ft² (334.5 m²) shall not be less than that specified in Table 18.4.5.1.2.

18.4.5.1.2.1 A reduction in required fire flow of 50 percent shall be permitted when the building is provided with an approved automatic sprinkler system.

18.4.5.2 Buildings Other Than One- and Two-Family Dwellings. The minimum fire flow and flow duration for buildings other than one- and two-family dwellings shall be as specified in Table 18.4.5.1.2.

18.4.5.2.1 A reduction in required fire flow of 75 percent shall be permitted when the building is protected throughout by an approved automatic sprinkler system. The resulting fire flow shall not be less than 1000 gpm (3785 L/min).

18.4.5.2.2 A reduction in required fire flow of 75 percent shall be permitted when the building is protected throughout by an approved automatic sprinkler system, which utilizes quick response sprinklers throughout. The resulting fire flow shall not be less than 600 gpm (2270 L/min).



Attachment B
ISO Calculations

(Exhibit on Next Page)

ISO Needed Fire Flow (NFF) Worksheet

(Page references are to the appropriate sections in the ISO Guide for Determination of Needed Fire Flow)

Petition Number:		Date:	6/14/2022
Project:	Gateway Crossings Lot 3 - Sonic	Engineer:	GAL
		Checked By:	CAP
Location:	NW Centurion Blvd. Lake City, FL		

Subject Building

Construction Class (p. 4): Wood Frame Construction **construction coefficient (F) (p. 2):** 1.5

Area of largest floor in the building (if modifications are made for division walls (p. 8), the division walls must be shown on the site plan.): 1227 sq.ft.

Total area of all other floors (if modifications are made for division walls (p. 8), the division walls must be shown on the site plan.): sq. ft.

Effective Area (A_i) (p. 9) : 1,227 sq. ft. (Show calculations below)

Needed Fire Flow attributed to construction (C_i) (per formula (p. 2)): 945.771114

(Round to the nearest 250 gpm. See p. 10 for maximum and minimum values of C_i)

Type of Occupancy: Combustible (C-3) **Occupancy Factor (O_i) (p. 11):** 1

Exposures (p. 16)

Front: construction of facing wall of exposure building (p. 4):

Distance (ft.) to the exposure building:	Length of exposure wall:
Number of stories of exposure wall:	Length x number of stories: 0
Opening Protection in exposure wall:	
Factor for exposure (X_i) from Table 330.A (p. 17):	0

Back: construction of facing wall of exposure building (p. 4):

Distance (ft.) to the exposure building:	Length of exposure wall:
Number of stories of exposure wall:	Length x number of stories: 0
Opening Protection in exposure wall:	
Factor for exposure (X_i) from Table 330.A (p. 17):	0

Left: construction of facing wall of exposure building (p. 4):

Distance (ft.) to the exposure building:	Length of exposure wall:
Number of stories of exposure wall:	Length x number of stories: 0
Opening Protection in exposure wall:	
Factor for exposure (X_i) from Table 330.A (p. 17):	0

Right: construction of facing wall of exposure building (p. 4):

Distance (ft.) to the exposure building:	Length of exposure wall:
Number of stories of exposure wall:	Length x number of stories: 0
Opening Protection in exposure wall:	
Factor for exposure (X_i) from Table 330.A (p. 17):	0

Communications (p. 18)

Passageway Opening Protection:
 Construction class of communication (Table 330.B):
 Is communication open or enclosed?
 Length of communication (in feet):
 Factor for Communications (P_i) from Table 330.B on p.19):

Calculation of Needed Fire Flow (p. 1)

NFF=(C_i)(O_i)[1.0+(X+P)_i] (substitute values as determined above. For exposures and communications use the single side with the highest charge.)

$$NFF= 1000 \quad \times \quad 1 \quad \times \quad [1 + (0 + 0)$$

$$NFF= 1000 \quad \text{gpm}$$

$$NFF= 1000 \quad \text{gpm (rounded to nearest 250 gpm per ISO requirements)}$$

Note: ISO evaluates hydrant distribution by examining the number and type of hydrants within 1,000 feet of each representative building. They also look at the distance from each such hydrant to the subject building, measured as apparatus can lay hose.

Hydrants with at least one large pumper outlet may receive credit for up to 1,000 gpm. Hydrants with at least two hose outlets, but no pumper outlet, may receive credit for up to 750 gpm. And hydrants with only one hose outlet may receive credit for up to 500 gpm.

Hydrants within 300 feet of the subject building may receive credit for up to 1,000 gpm (but not more than the credit that would apply based on the number and type of outlets). Hydrants from 301 feet to 600 feet from the subject building may receive credit for up to 670 gpm (but not more than the credit that would apply based on the number and type of outlets). And hydrants from 601 feet to 1,000 feet from the subject building receive credit for 250 gpm. Under certain circumstances, when all fire department pumpers carry sufficient large-diameter hose, ISO may allow maximum credit for hydrants up to 1,000 feet from the subject building.

More than one fire hydrant may be required for proper distribution of water per ISO requirements.



Attachment C

City of Lake City Fire Flow Test Results

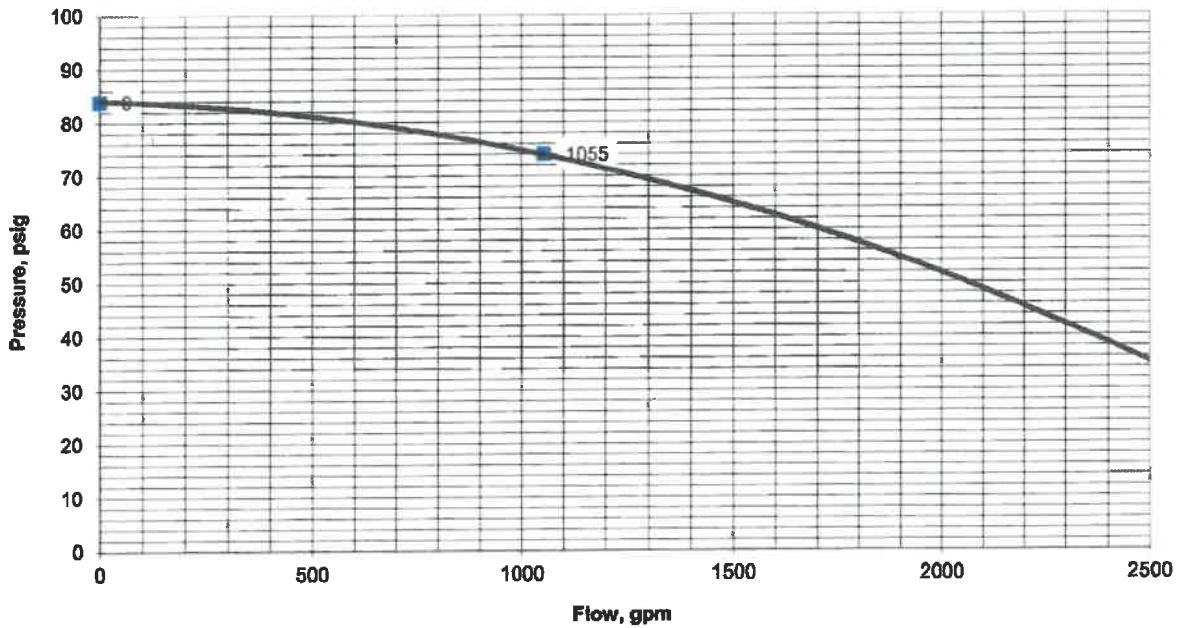
(Exhibit on Next Page)

City of Lake City Water flow report

HYDRANT # & LOCATION: 1st hydrant north of Denny's on NW Centurion Drive DATE: 6/14/2022
 TEST BY: Al/Lavonte Day: Tuesday Time: 9-10 Minutes: 2
 WATER SUPPLIED BY: Municipal
 PURPOSE OF TEST: request

DATA

FLOW HYDRANT(S)	A1	A2	A3
SIZE OPENING:	<u>2.5</u>	<u>2.5</u>	<u>2.5</u>
COEFFICIENT:	<u>0.8</u>	<u> </u>	<u> </u>
PITOT READING:	<u>50</u>	<u> </u>	<u> </u>
GPM:	<u>1055</u>	<u>0</u>	<u>0</u>
TOTAL FLOW DURING TEST:	<u>1055</u> GPM		
STATIC READING:	<u>84</u> PSI	RESIDUAL:	<u>74</u> PSI
RESULTS: AT 20 PSI RESIDUAL	<u>2874</u> GPM	AT 0 PSI	<u>3328</u> GPM
ESTIMATED CONSUMPTION:	<u>2109</u> GAL.		
REMARKS:			

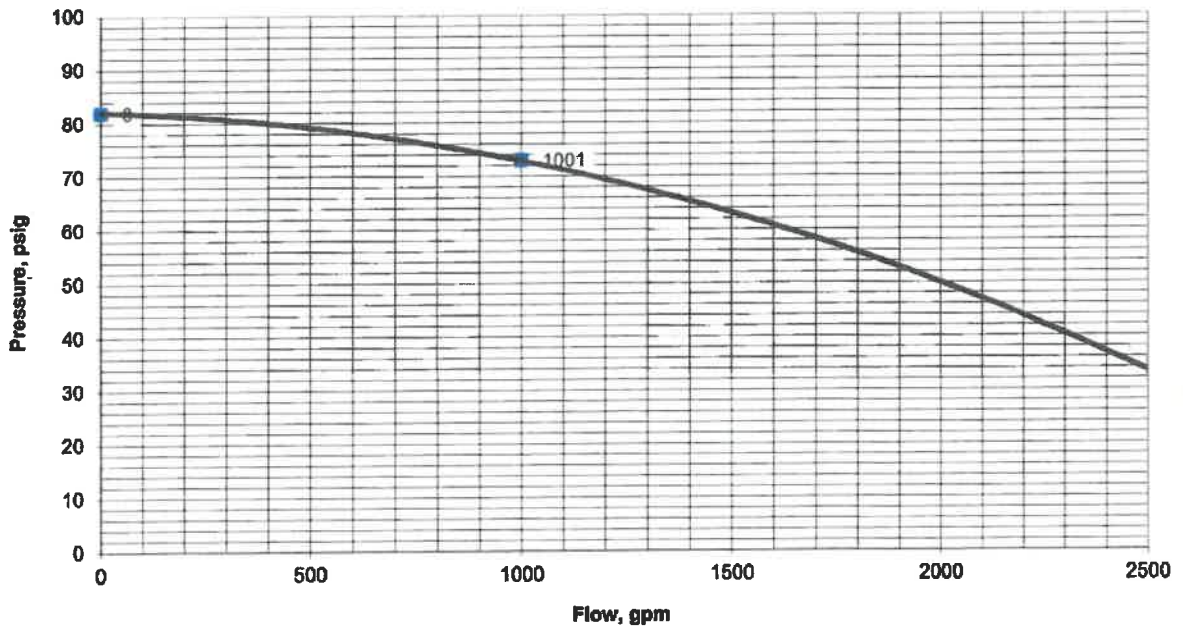


City of Lake City Water flow report

HYDRANT # & LOCATION: **2nd hydrant north of Denny's on NW Centurion Driv** DATE: **6/14/2022**
 TEST BY: **AI/Lavonte** Day: **Tuesday** Time: **9:10** Minutes: **2**
 WATER SUPPLIED BY: **Municipal**
 PURPOSE OF TEST: **request**

DATA

FLOW HYDRANT(S)	A1	A2	A3
SIZE OPENING:	2.5	2.5	2.5
COEFFICIENT:	0.8	 	
PITOT READING:	45	 	
GPM:	1001	0	0
TOTAL FLOW DURING TEST:	1001 GPM		
STATIC READING:	82 PSI	RESIDUAL:	73 PSI
RESULTS: AT 20 PSI RESIDUAL	2837 GPM		AT 0 PSI 3299 GPM
ESTIMATED CONSUMPTION:	2001 GAL.		
REMARKS:			





DEPARTMENT OF GROWTH MANAGEMENT
205 North Marion Avenue
Lake City, Florida 32055
Telephone: (386) 719-5750
growthmanagement@lcfla.com

REVIEW REPORT TO PLANNING AND ZONING, BOARD OF ADJUSTMENT AND
HISTORICAL COMMITTEES' BY STAFF
FOR SITE PLAN REVIEW, SPECIAL EXCEPTIONS, VARIANCES, COMPREHENSIVE
PLAN AMENDMENTS/ ZONING AND CERTIFICATE OF APPROPRIATENESS

Date: 06/15/2025

Request Type: Site Plan Review (SPR) Special Exception (SE) Variances (V)

Comprehensive Plan Amendment/Zoning (CPA/Z) Certificate of Appropriateness (COA)

Project Number: SPR22-14

Project Name: Sonic (Centurion Loop)

Project Address: NW Centurion Ct, Lake City FL

Project Parcel Number: 35-3S-16-02524-103

Owner Name: Parker Neely

Owner: Address: 2682 NW Noegel Rd

Owner Contact Information: telephone number 704-577-2475 e-mail Dneely@highcotton-cep.com

Owner Agent Name: Chris Potts, PE

Owner Agent Address: 3530 NW 43rd St Gainesville FL

Owner Agent Contact Information: telephone 352-375-8999 e-mail chris.potts@jbpro.com

The City of Lake City staff has reviewed the application and documents provided for the above request and have determined the following:

Growth Management – Building Department, Planning and Zoning, Code Enforcement, Permitting

Building Department: Approved Disapproved Reviewed by: _____

Comments: N/A

Planning and Zoning: Approve Disapprove Reviewed by: Robert Angelo

Comments: Zoning is consistent with the LDR

Zoning is consistent with the LDR

Business License: Approve Disapprove Reviewed by: Marshall Sova

Comments: Will need to apply for a business license

Code Enforcement: Approve Disapprove Reviewed by: Marshall Sova

Comments: No concerns at this time

Permitting: Approve Disapprove Reviewed by: Ann Jones

Comments: No concerns at this time

No concerns at this time

Utilities – Water, Sewer, Gas, Water Distribution/Collections, Customer Service

Water Department: Approved Disapproved Reviewed by: _____

Comments: N/A

Sewer Department: Approved Disapproved Reviewed by: _____

Comments: N/A

Gas Department: Approved Disapproved Reviewed by: Steve Brown

Comments: Need BTU totals

Water Distribution/Collection: Approved Disapproved Reviewed by: Brian Scott

Comments: _____
If they do not use the taps in place they will be required to make new ones and
cut and cap sewer and dig to water main and shut off before construction.

Customer Service: Approved Disapproved Reviewed by: Shasta Pelham

Comments: A tap application would be required to access city utilities. The tap fees, impact fees and utility deposits
will be calculated upon approval of the tap application. City utilities border the property; locates must be obtained to
ensure that the utility infrastructure is not damaged or obstructed. An appliance list with the btu loads are required for natural gas.

Public Safety – Public Works, Fire Department, Police Department

Public Works: Approved Disapproved Reviewed by: Steve Brown

Comments: No concerns at this time

Fire Department: Approve Disapprove Reviewed by: Assistant Chief Boozer

Comments: Key access box and lightweight truss placard needed

Police Department: Approve Disapprove Reviewed by: Assistant Chief Andy

Comments: No concerns at this time.

Please provide separate pages for comments that will not fit in provided spaces and please label the pages for your department and for the project.



06/21/2022

Sonic (Centurion Loop)

Parcel **35-3S-16-02524-103**

To Whom it May Concern

Parcel **35-3S-16-02524-103** is commercial highway interchange and is conducive for the proposed use of the land. Per the City of Lake City Land Development Regulations, restaurants are permitted in this district.

Thank You

Robert Angelo

Planning and Zoning Tech

386-719-5820

Project Summary

Project Name: Sonic- Centurion Loop

Project Number: SPR22-14

Parcel Number: 02524-103

Project Notes

- Project type: Site Plan Review
- Future land use is: Commercial
- Zoning designation is: Commercial Highway Interchange
- Proposed use of the property: New construction of Sonic
- Land is conducive for use: Yes, per the LDR section 4.15.2.3
- See staff review for notes from directors and city staff for their comments.

Project Summary

Project SPR22-14 is for a site plan review and has been reviewed by city staff. Application is sufficient for review. After review of the petition the city staff has determined that the petition is consistent with the land development regulations and the comprehensive plan. At this time the City has not concerns.