



AGENDA

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If you would like to comment on a public hearing agenda item, please select the best option for your participation:

1. Join us via Zoom (*please mute your device until called upon*).
Join the Webinar: <https://ketchumidaho-org.zoom.us/j/85154523377>
Webinar ID: 851 5452 3377
2. Address the Commission in person at City Hall.
3. Submit your comments in writing at (*by noon the day of the meeting*)

This agenda is subject to revisions. All revisions will be underlined.

CALL TO ORDER:

ROLL CALL:

COMMUNICATIONS FROM COMMISSIONERS:

1. Public Comment - YMCA

CONSENT AGENDA:

ALL ACTION ITEMS - The Commission is asked to approve the following listed items by a single vote, except for any items that a commissioner asks to be removed from the Consent Agenda and considered separately.

2. Approval of the February 18, 2026 Meeting Minutes
3. Approval of the Amended September 09, 2025 meeting minutes.
4. Recommendation to review and approve the Findings of Fact, Conclusions of Law, and Decision for the Sun Valley Company Greyhawk Ski Event Storage Conditional Use Permit Application (P25-058).

PUBLIC HEARING:

- [5.](#) Recommendation to review and provide feedback on P25-068: A Pre-Application Design Review for an addition located at the Wood River YMCA at 101 Saddle Road.
- [6.](#) Recommendation to review and approve the 233 Parkway Drive Variance application, as conditioned, and direct staff to return with Findings of Fact.

NEW BUSINESS:

7. Discussion of Historic Preservation Roles and Responsibilities.

ADJOURNMENT:

Dawn Hofheimer

From: Sandra Flattery <ssflattery@aol.com>
Sent: Thursday, February 26, 2026 11:20 AM
To: Participate
Cc: Sandra Flattery
Subject: COMMENT: Planning & Zoning Meeting March 5, 2026. YMCA Expansion

Follow Up Flag: Follow up
Flag Status: Flagged

Dear P&Z,

I am writing to comment on the YMCA expansion plans.

There is considerable concern about the relocation and subsequent allocation of new parking spaces near the facility:

- How many parking spaces are there currently on the facility north parking lot by the Fire Station?
- How many spaces are there currently on the Park & Ride lot south of the facility? Are all of those spaces public or are some reserved for Park and Ride vehicles?
- If some parking is moved to Saddle Road, how many spaces can be created? parallel parking?
- If some parking is moved to the Industrial District by creating a pathway, how many spaces can be created?
- Where will Handicap & Accessibility parking be moved to? YMCA users include wheelchair and walking cane participants.
They are not able to walk safely in ice and snow conditions for any distance.
- Feasibility of creating underground parking underneath the proposed Field House?

I was informed that the city of Ketchum is responsible for the parking Accessibility compliance issues, not the YMCA. Is this correct?

Thank you for including my comments into the public record for the March 5, 2026 P&Z meeting.

Sandra Flattery

Sandra Flattery
ssflattery@aol.com
sandraflattery.com



CITY OF KETCHUM, IDAHO
MEETING MINUTES OF THE
PLANNING AND ZONING COMMISSION
Wednesday, February 18, 2026
191 5th Street West, Ketchum, Idaho 83340

CALL TO ORDER:

Chair Tim Carter called the meeting to order at 4:31 PM *(00.0.09 in video)*

ROLL CALL:

Tim Carter
Brenda Moczygemba
Hannah Harris
Joey Stevenson
Alex Monge

ALSO PRESENT:

Paige Nied – Associate Planner
Morgan Landers – Director of Planning and Building

COMMUNICATIONS FROM COMMISSIONERS:

- Question from Commissioner Monge *(00.00.38 in video)*

CONSENT AGENDA: *(00.01.09 in video)*

1. Approval of the February 5, 2026 meeting minutes.
2. Approval of the Amended September 09, 2025 meeting minutes.
 - Consent Agenda Item Removed.

Motion to approve consent agenda item # 1 at 4:35 PM:

MOVER: Brenda Moczygemba

SECONDER: Hannah Harris

AYES: Tim Carter, Brenda Moczygemba, Hannah Harris, Joey Stevenson, & Alex Monge

NAYS:

RESULT: UNANIMOUSLY ADOPTED

PUBLIC HEARING: *(00.03.32 in video)*

3. Recommendation to review and approve the Conditional Use Permit for storage containers at the Greyhawk Parking Lot, as conditioned, and direct staff to return with Findings of Fact.
 - Presentation from Paige Nied, Associate Planner.

- Presentation from Victor Schoessler, Executive Director of Operations for Sun Valley Company. *(00.11.00 in video)*
- Questions from Commissioners. *(00.17.00 in video)*
- Responses from James Grant, Director of Mountain Operations, Paige Nied, Associate Planner, Morgan Landers, Director of Planning and Building, and Riley Berman Competition Services Manager. *(00.17.50 in video)*
- Commissioner Deliberation. *(00.28.10 in video)*

Motion to approve the Conditional Use Permit for storage containers at the Greyhawk Parking Lot, as conditioned, at 5:02 PM:

MOVER: Alex Monge

SECONDER: Brenda Moczygemba

AYES: Tim Carter, Brenda Moczygemba, Hannah Harris, Joey Stevenson, & Alex Monge

NAYS:

RESULT: UNANIMOUSLY ADOPTED

4. Recommendation to review and provide feedback on the Design Review and Townhouse Preliminary Plat applications for the townhouse development at 310 Georgia Rd. *(00.31.55 in video)*

- Presentation from Paige Nied, Associate Planner.
- Questions from Commissioners. *(00.42.26 in video)*
- Presentation from Bobbie Jordan, Applicant. *(00.47.51 in video)*
- Questions from Commissioners. *(00.50.35 in video)*
- Public Comment from Ben Verge. *(00.58.12 in video)*
- Public Comment from Warren Taylor. *(01.02.00 in video)*
- Public Comment from Walt Coiner. *(01.06.05 in video)*
- Commissioner Deliberation. *(01.07.19 in video)*

Motion to continue the Liftview Townhomes Preliminary Plat and Design Review to March 18, 2026, at 6:08 PM:

MOVER: Brenda Moczygemba

SECONDER: Hannah Harris

AYES: Tim Carter, Brenda Moczygemba, Hannah Harris, Joey Stevenson, & Alex Monge

NAYS:

RESULT: UNANIMOUSLY ADOPTED

ADJOURNMENT: *(01.27.38 in video)*

Motion to adjourn at 6:09 PM:

MOVER: Brenda Moczygemba

SECONDER: N/A

AYES: Tim Carter, Brenda Moczygemba, Hannah Harris, Joey Stevenson, & Alex Monge

NAYS:

RESULT: UNANIMOUSLY ADOPTED

Tim Carter – Chairman of the Planning and Zoning Commission

Morgan Landers – Director of Planning and Building



City of Ketchum

PLANNING AND ZONING COMMISSION MEETING AGENDA MEMO

Meeting Date: Staff Member/Dept:

Agenda Item:

Recommended Motion:

Reasons for Recommendation:

- City staff found a few errors in the previously approved September 9, 2025 Planning and Zoning Commission meeting minutes. These errors have been corrected for the record.

Attachments:



**CITY OF KETCHUM, IDAHO
MEETING MINUTES OF THE
PLANNING AND ZONING COMMISSION**

Tuesday, September 9, 2025
191 5th Street West, Ketchum, Idaho 83340

CALL TO ORDER:

Chairman Neil Morrow called to order at 4:30 pm *(00.00.10 in video)*

ROLL CALL:

Neil Morrow
Susan Passovoy
Matthew McGraw
Brenda Moczygemba
Tim Carter

ALSO PRESENT:

Morgan Landers – Director of Planning and Building
Abby Rivin – Senior Planner

COMMUNICATIONS FROM COMMISSIONERS: *(00.00.26 in video)*

1. Public Comments Submitted
2. Comment from Commissioner Susan Passovoy *(00.00.32 in video)*

CONSENT AGENDA: *(00.01.33 in video)*

3. Approval of the April 08, 2025 meeting minutes.
4. Approval of the April 22, 2025 meeting minutes.
5. Approval of the June 10, 2025 meeting minutes.
6. Approval of the August 26, 2025 meeting minutes.

Motion to approve consent agenda at 4:32 p.m.

MOVER: Susan Passovoy

SECONDER: Matthew McGraw

AYES: Susan Passovoy, Matthew McGraw, Brenda Moczygemba, Tim Carter, & Neil Morrow

NAYS:

RESULT: UNANIMOUSLY ADOPTED

Public Hearing: *(00.01.50 in video)*

7. Recommendation to review and approve the Knob Hill Residence Mountain Overlay Design Review and Lot Consolidation Subdivision Preliminary Plat with Waiver Request applications and direct staff to return with Findings of Fact (continued from August 26, 2025).
 - Staff Presentation – Abby Rivin, Senior Planner *(00.02.13 in video)*

- Applicant Presentation – Zac Rockett, Principal Architect, Ro Rockett Design (00.08.35 in video)
- Commissioner Questions (00.19.08 in video)
- Public Comment – Heidi Schernthanner (00.35.20 in video)
- Public Comment – Ed McDermott (00.36.30 in video)
- Public Comment – Perry Boyle (00.43.46 in video)
- Close Public Hearing (00.44.58 in video)
- Commissioner Deliberation (00.45.05 in video)

Motion to approve Knob Hill Residence Mountain Overlay Design Review Application subject to conditions 1 through 12 and direct staff to return with draft findings at 5:40 pm:

MOVER: Susan Passovoy

SECONDER: Brenda Moczygemba

AYES: Susan Passovoy, Matthew McGraw, Brenda Moczygemba, Tim Carter, & Neil Morrow

NAYS:

RESULT: UNANIMOUSLY ADOPTED

Motion to approve Knob Hill Residence Lot Consolidation Subdivision Preliminary Plat Application and Waiver Requests to the City Council subject to conditions 1 through 3 and direct staff to return with draft findings at 5:40 pm:

MOVER: Susan Passovoy

SECONDER: Brenda Moczygemba

AYES: Susan Passovoy, Matthew McGraw, Brenda Moczygemba, Tim Carter, & Neil Morrow

NAYS:

RESULT: UNANIMOUSLY ADOPTED

8. Recommendation to conduct a public hearing and provide feedback on the Warm Springs Townhomes Design Review Application (P24-063) located at 108 Ritchie Dr. (01.10.40 in video)

- Staff Presentation – Morgan Landers, Director of Planning and Building (01.11.00 in video)
- Applicant Presentation – Jason Ro, Principal Architect, Ro Rockett Design (01.26.40 in video)
- Commissioner Questions (01.39.55 in video)
- Public Comment – Perry Boyle (01.44.50 in video)
- Public Comment – Patricia Battersby (01.46.36 in video)
- Public Comment – Aimée Christensen (01.48.30 in video)
- Public Comment – Jeff Small (01.50.19 in video)
- Public Comment – Brad Matthews (01.53.12 in video)
- Public Comment – Jeff Oak (01.54.53 in video)
- Public Comment – Jeff Oak (01.54.53 in video)
- Close Public Hearing (01.58.30 in video)

- Applicant Responses – Jason Ro, Principal Architect, Ro Rockett Design (01.58.54 in video)
- Commissioner Deliberation (02.08.00 in video)

Motion to continue to the 10/14/25 meeting with a request to see modifications consistent with commissioner comments at 7:11 pm:

MOVER: Matthew McGraw

SECONDER: Tim Carter

AYES: Susan Passovoy, Matthew McGraw, Brenda Moczygemba, Tim Carter, & Neil Morrow

NAYS:

RESULT: UNANIMOUSLY ADOPTED

9. Recommendation to review and provide feedback on the Bitterroot Mixed-Use Pre-Application Design Review for the proposed redevelopment of the property located at 200 N Spruce Avenue. (02.41.45 in video)

- Staff Presentation – Abby Rivin, Senior Planner (02.42.00 in video)
- Applicant Presentation – Christian Thomas, Architect, Christian Thomas Studio (03.02.15 in video)
- Public Comment – Jennifer Case (03.30.20 in video)
- Public Comment – Aimée Christensen (03.37.32 in video)
- Public Comment – Linda Sisson (03.41.52 in video)
- Public Comment – Carole King Klein (03.44.00 in video)
- Public Comment – Perry Boyle (03.49.51 in video)
- Close Public Hearing (03.53.23 in video)
- Commissioner Deliberation (03.54.25 in video)

Announcement: (04.19.05 in video)

10. Announcement regarding PEG Mariott, Comprehensive Plan, Consolidated Land Development Code, and next Planning and Zoning Commission agenda from Morgan Landers, Director of planning and Building.

ADJOURNMENT: (04.21.05 in video)

Motion to adjourn at 9:00 pm:

MOVER: Neil Morrow

SECONDER: Matthew McGraw

AYES: Susan Passovoy, Matthew McGraw, Brenda Moczygemba, Tim Carter, & Neil Morrow

NAYS:

RESULT: UNANIMOUSLY ADOPTED

Tim Carter – Chairman of P & Z Commission

Morgan Landers – Director of Planning & Building



City of Ketchum
Planning & Building

IN RE:)	
)	
Sun Valley Company Ski Event Storage))	KETCHUM PLANNING AND ZONING COMMISSION
Conditional Use Permit)	FINDINGS OF FACT, CONCLUSIONS OF LAW, AND
Date: March 5, 2026)	DECISION
)	
File Number: P25-058)	

PROJECT: Sun Valley Company Ski Event Storage

APPLICATION TYPE: Conditional Use Permit

FILE NUMBER: P25-058

ASSOCIATED APPLICATIONS: None

REPRESENTATIVE: Victor Schoessler, Sun Valley Company Operations Manager

OWNER: Sun Valley Company

LOCATION: Greyhawk Upper Parking Lot (Greyhawk II Sub FR Lot 16 Blk 2
Inside City Warm Springs Parking Lot)

ZONING: Agriculture and Forestry District (AF)

OVERLAY: None

RECORD OF PROCEEDINGS

The Planning and Building Department received the CUP application for the project on November 6, 2025. Following the receipt of the application, staff routed the application materials to all city departments for review and was scheduled for hearing on January 23, 2026. As of the date of this letter, all department comments have been resolved or addressed through the conditions of approval recommended below.

Notice was published in the Idaho Mountain Express and was mailed to properties within a 300-foot radius on January 28, 2026. Notice was posted at the subject location and on the city website on February 11, 2026.

The Planning and Zoning Commission (the “Commission”) considered the Conditional Use Permit (Application No. P24-078) application for the Sun Valley Resort for the continued use of a ski facility for the storage of ski race equipment during a regular meeting on February 18, 2026. After considering staff’s analysis, and public comment, the Commission unanimously approved the Conditional Use Permit application with conditions.

BACKGROUND

The Applicant is requesting a Conditional Use Permit (CUP) for the continued use of a ski facility use for 10 storage containers for the storage of ski race equipment in the Greyhawk Upper lot (“subject property”).

The subject property is zoned Agricultural & Forestry (AF), which allows “Ski Facility” only through the approval of a Conditional Use Permit. The Ketchum Municipal Code defines “Ski Facility” as:

“Ski facility: An establishment or area containing the necessary elements to facilitate the use of ski runs and trails. Typical uses include, but are not limited to, powered conveyors for transporting skiers or sightseers, training facilities and associated administrative offices, retail trade, food service and parking.

The city considers the storage of ski race materials as a necessary element to facilitate the use of the ski runs during the event. The ski facility use is for the storage of ski race equipment such as netting and gates for the FIS World Cup Finals event in 2027 and potentially in 2029. The ski facility includes 10 dark green storage containers that are screened by existing mature and temporary vegetation. The storage container location is over 300ft from the nearest residential neighborhood along Gates Road. The timeline for removal of the ski facility is Labor Day 2029.

FINDINGS OF FACT

The Commission, having reviewed the entire project record, provided notice, and conducted the required public hearing, does hereby make and set forth these Findings of Fact, Conclusions of Law, and Decision as follows:

FINDINGS REGARDING CONDITIONAL USE PERMIT CRITERIA

Conditional Use Requirements				
EVALUATION STANDARDS: 17.116.030 and § 67-6512 of Idaho Code				
A conditional use permit shall be granted by the commission only if the applicant demonstrates the following:				
Compliance and Analysis				
Ye s	No	N/A	City Code	City Standards

<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.116.030(A)	The characteristics of the conditional use will not be unreasonably incompatible with the types of uses permitted in the applicable zoning district.
			Finding	<p><i>The property is zoned Agriculture and Forestry (AF) which is this city’s most restrictive zone district. Permitted uses include one-family dwellings, agriculture and farming uses, nature preserves, and public uses Ski facilities require a Conditional Use Permit. As stated in the application, the proposed use is for ten storage containers to help support special events, in particular the World Cup event which will occur in winter 2027 and potentially 2029. As such, the applicant has requested a timeline for removal of the storage containers by summer 2029. The Commission was supportive of a removal date of Labor Day 2029, as stated in condition of approval #3. If Sun Valley is to host additional World CUP events beyond 2029, staff advises the Commission to reopen discussions with Sun Valley Company on establishing a permanent storage solution.</i></p> <p><i>The subject property is already improved as a ski facility with ski runs and parking. As stated previously, there is some existing screening and new screening installed that blocks views of the storage containers from the residential neighborhoods to the north. Approval of the ski facility would not alter the property in a way that would deter current and future permitted uses to occur. The proposed ski facility use would be in line with what the use of the property currently is as there are multiple ski runs and parking for skiers present today. As conditioned, staff believes the ski facility to be compatible with permitted uses in the zone district and the immediate vicinity.</i></p>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.116.030(B)	The conditional use will not materially endanger the health, safety and welfare of the community.
			Finding	<i>The Commission had no concerns that the use will endanger the health, safety and welfare of the community. The storage containers are located far away (300 feet) from pedestrian travel and is used for the storage of ski event materials. The City did not receive any complaints from neighbors regarding the initial CUP application (P24-078) for the storage containers.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.116.030(C)	The conditional use is such that pedestrian and vehicular traffic associated with the use will not be hazardous or conflict with existing and anticipated traffic in the neighborhood.
			Finding	<i>No public vehicular or pedestrian traffic will be associated with the use. The location for the ski facility is over 300 feet away from the nearest road, limiting any potential conflicts with traffic. The use is also positioned clear of general pedestrian and vehicular traffic associated with the use of Warm Springs ski area.</i>

<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.116.030(D)	The conditional use will be supported by adequate public facilities or services and will not adversely affect public services to the surrounding area or conditions can be established to mitigate adverse impacts.
			<i>Finding</i>	<i>No public facilities or services are required for the ski facility use.</i>
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	17.116.030(E)	The conditional use is not in conflict with the policies of the Comprehensive Plan or the basic purposes of this section.
			<i>Finding</i>	<p><i>The Ketchum Comprehensive Plan outlines 10 core values of the community, one of which is “Exceptional Recreational Opportunities”. The community recognizes that “outdoor recreation amenities and lifestyle are a key stimulus to our economy.” As outlined in the application materials, this ski facility is for the storage of ski event materials for events such as the upcoming World Cup. The 2025 FIS World Cup event was extremely successful and was a wonderful opportunity for residents and visitors to interact with.</i></p> <p><i>Policy OS 1.7 states the city should “Continue to work with private and public interests, such as the Sun Valley Company and Blaine County to ensure the continuance of alpine and Nordic skiing, golf, and other recreational/cultural activities at reasonable costs”. The World Cup event is one that takes place across many mountainous regions across the world. Bringing this event to the residents and visitors in Ketchum is a great opportunity.</i></p> <p><i>The future land use map (FLUM) designation for the property is “Open Space, Parks and Recreation”. Primary uses include public and private open space, trails, parks, and golf courses. Some public utilities and facilities may be appropriate as secondary uses. The use of the property is currently private open space and is owned and maintained by the Sun Valley Company. The proposed ski facility use would be in line with what the use of the property currently is as there are multiple ski runs and parking for skiers present today.</i></p> <p><i>The Commission found that the continued allowance of the storage containers does not change the long term use of the property which complies with the FLUM designation. Additionally, the Commission found approval of the event meets the goals and objectives of the comprehensive plan related to open space and recreation.</i></p>

CONCLUSIONS OF LAW

1. The City of Ketchum is a municipal corporation established in accordance with Article XII of the Constitution of the State of Idaho and Title 50 Idaho Code and is required and has exercised its authority pursuant to the Local Land Use Planning Act codified at Chapter 65

of Title 67 Idaho Code and pursuant to Chapters 3, 9 and 13 of Title 50 Idaho Code to enact the ordinances and regulations, which ordinances are codified in the Ketchum Municipal Code (“KMC”) and are identified in the Findings of Fact and which are herein restated as Conclusions of Law by this reference and which City Ordinances govern the applicant’s Conditional Use Permit application for the development and use of the project site.

2. The Commission has authority to hear the applicant’s Conditional Use Permit Application pursuant to Chapter 17.116 of Ketchum Municipal Code Title 17.
3. The City of Ketchum Planning Department provided notice for the review of this application in accordance with Ketchum Municipal Code §17.116.040.
4. The Conditional Use Permit application is governed under Ketchum Municipal Code Chapter 17.116.
5. The Conditional Use Permit application meets all applicable standards specified in Title 17 of Ketchum Municipal Code.

DECISION

THEREFORE, the Commission **approves** this Conditional Use Permit Application File No. P25-058 this Thursday, March 5, 2026, subject to the following conditions of approval.

CONDITIONS OF APPROVAL

1. This conditional use permit approval is based on the project plans presented at the February 18, 2026, Planning and Zoning Commission meeting. Any building or site discrepancies which do not conform to the approved plans will be subject to removal.
2. This Conditional Use Permit is not transferable from one parcel of land to another.
3. The storage containers shall be removed from the subject property by Labor Day 2029.

Findings of Fact **adopted** this 3rd day of March 2026.

Tim Carter, Chair
City of Ketchum - Planning and Zoning Commission



STAFF REPORT
KETCHUM PLANNING AND ZONING COMMISSION
March 5, 2026

PROJECT: Wood River YMCA (Addition)

FILE NUMBER: P25-068

APPLICATION: Pre-Application Design Review

PROPERTY OWNER: City owned Land and YMCA Lease
Matt Neilson, deChase Miksis Development (Owners representative)

ARCHITECT: Michael Bulls, RLB Architecture

REQUEST: Pre-Application Design Review for proposed addition of a new main entrance lobby, teen center, fieldhouse with upper-level running track and exterior improvements including: heated sidewalks, landscaped areas, removal of stairs leading up to the existing main entrance. In addition, the reconfigured parking and fire/emergency access located on the north side of the proposed addition. (Internal remodel not subject to design review)

LOCATION: 101 Saddle Road

ZONING: Tourist Zoning District (T)

OVERLAY: None

REVIEWER: Allison Kennedy – Senior Planner

NOTICE: A courtesy notice for the public meeting on the project was mailed to all property owners within 300 feet of the project site on February 5, 2026. The notice was published in the Idaho Mountain Express on February 11, 2026 and materials were posted on the City website on February 18, 2026. A notice was posted on premises February 25, 2026.

I. EXECUTIVE SUMMARY

The Wood River YMCA is proposing an interior remodel, addition, and exterior improvements within their leased boundary located at 101 Saddle Road within the Tourist District. (See Fig.1). A Pre-Application Design Review is required for the 26,464sf addition which includes a new main entrance lobby, teen center, and a field house with an upper-level running track proposed to be added to the north end of the facility. Exterior improvements are also proposed requiring design review; including a heated sidewalk, landscaped areas adjacent to addition, removal of existing main entrance stairs, and parking and fire/emergency access reconfiguration. See Attachment A.III Pre-Design Review Drawings.

The application was submitted after the adoption of the new comprehensive plan, but prior to adoption of the updated code. Therefore, the application was reviewed under the previous Title 17 and 2025

Comprehensive Plan. Based on staff's review of the application, staff believe the application meets the design review criteria and that no changes to the design are necessary to proceed to final design review.

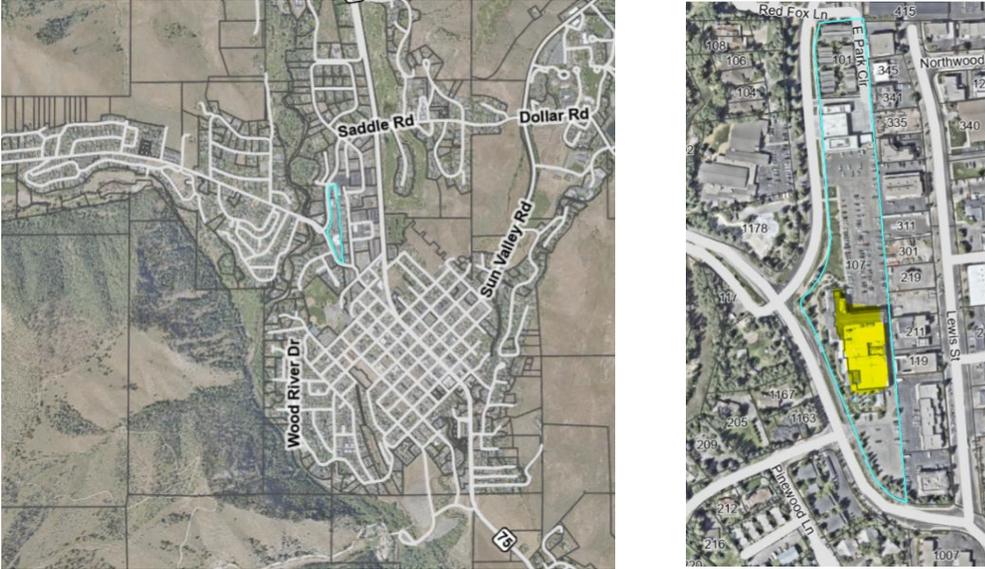


Figure 1 Vicinity Map

II. BACKGROUND

A Pre-Application for Design review was received by the City Planning Department on December 31, 2025. Following receipt of the application, staff routed the application materials to all city departments for review. No significant changes were requested at this time. The Planning Department deemed the application complete on January 26, 2026.

A Planned Unit Development Conditional Use Permit (PUD-CUP) was approved by the city of Ketchum in October 2005 for a 84,155 square foot recreation complex to include: a ice rink approximately 32,500sf, a pool area approximately 12,000sf, a gymnasium approximately 6,000sf, locker rooms approximately 3,800 sf, climbing wall area approximately 2,000sf, aerobics room approximately 1,200 sf, community conference rooms approximately 3,000sf, staff offices approximately 2,000sf, and a daycare area approximately 1,400 sf.

The Wood River YMCA built 56,438sf of the recreation complex but have yet to build the 32,500sf addition on the northern portion of the complex which was originally contemplated and approved as part of the PUD-CUP. Since the original 2005 approval, an indoor ice rink arena was built in Hailey. With this community recreation need met and after operating within the Valley for 20 years, the Wood River YMCA is proposing to fully realize its approved square footage by instead creating a 26,464 sf addition on the north side of the existing structure to include the above referenced additions.

III. PROCESS

The Planning and Zoning Commission gives general feedback to the applicant during a Preapplication Design Review Meeting. This allows the applicant to present their general concept and the Planning and Zoning to provide initial feedback and direction.

Pursuant to KMC §17.96.010.D.2.:

The purpose of preapplication review is to allow the Commission to exchange ideas and give direction to the applicant on the "design concept", keeping in mind the purpose of this chapter and the application of the evaluation standards.

After the Preapplication Design Review, the applicant may then choose to incorporate the feedback and submit a full design review proposal as laid out in KMC §17.96 to be scheduled for a public hearing once city

staff deem their application complete and pertinent city departments review the application. Per SKMC 17.96.010D.5. if the applicant proceeds, the Design Review application must be submitted within 180 days of the pre-application meeting with the Commission.

IV. CONFORMANCE WITH ZONING AND DESIGN REVIEW STANDARDS

Pursuant to KMC §17.96.050.A, the Commission shall determine the following before granting Design Review approval:

1. The project does not jeopardize the health, safety or welfare of the public.
2. The project generally conforms with the goals, policies, and objectives of the adopted comprehensive plan.
3. The project conforms to all applicable standards and criteria as set forth in this chapter, this title, and any other standards as adopted or amended by the City of Ketchum from time to time.

Criteria 1 & Criteria 2

The application generally conforms with the goals, policies, and objectives of the Comprehensive Plan. Specifically, the Distinct Built and Natural Environment Chapter which focuses on Land Use and Community Character. Policy BNE-1.5: Context Sensitive Development highlights the objective that the built environment should blend with the adjacent materials, mix of uses, massing, scale, building heights, and densities permitted within the underlying zoning districts. The application proposes extending the existing recreational facility with similar materials, uses, and heights that blend within the context of that particular built environment and therefore is in congruent with the character of the existing area. In addition, the footprint of the proposed development is less than what was originally approved and therefore fits within the density planned for the site.

The application also meets Policy BNE-1.2: Neighborhood Characteristics which states the desire to reinforce the distinct characteristics of Ketchum's neighborhoods, encouraging creativity and innovation over uniformity. This innovation and flexibility of design over uniformity is highlighted with the proposed alteration of the underutilized main stairs and entrance way and the multitude of details such as the varied roof lines and a mix of building materials which is unusual for a large recreational facility. This YMCA design is unique to Ketchum and is not typical for this organization's usual utilitarian recreational facilities.

Criteria 3

A. ZONING STANDARDS

The proposed application conforms with the zoning and dimensional standards of the underlying Tourist District (T) and the pre-existing site approvals. Planning staff have highlighted pertinent standards below for the Commission to better understand the context of the application and the standards it is subject to.

Height-The required building height per 2005 CUP-PUD is 45'6". Both proposed elevations are below the maximum height requirement.

Proposed elevations (Refer to Attachment A III. Sheet A.4):

- Northern addition of 34'7"
- West elevation lobby roof top 39'7"

Setbacks- The required setbacks for the Tourist Zone:

Front: 30' however a waiver to 25' was approved in 2005 CUP-PUD for Warm Springs Rd. & 15' for Saddle Rd.

Side: 1' for every 3' of building height or 16.5'

(A waiver to a side setback to Saddle Road to 0' for one portion of the building was approved in 2005 CUP-PUD for the anticipated ice rink. The new structure is setback ~50' from that line.)

Rear: 15'

The proposed addition maintains these required setbacks.

2005 CUP Ice Rink Square Footage Footprint Approval

The ice rink was approved at "approximately 32,500 square feet" in the 2005 CUP. The addition proposed is 26,464sf. The application proposes approximately 6,036sf less square footage than what was approved and therefore meets the standard in regard to size of footprint.

Floor Area Ratio Tourist Zone: The FAR associated with this 5.72 acre parcel located within the Tourist Zoning District is .5 up to 1.6. (82,902 sf total) = .33 FAR. The city of Ketchum had this lot surveyed and it is 5.72 acres. This standard is met and no action is required.

B. DESIGN REVIEW STANDARDS

The applicant will submit a full design review application to be reviewed per KMC §17.96.060 Improvements and Standards after it receives feedback from the Commission. A full list of Design Review Evaluation Standards are attached for reference. (See Attachment B.) The following will be reviewed in more detail for compliance during the full design review, including: drainage, utilities, snow storage, emergency and fire vehicular access, pedestrian and traffic circulation, refined landscaping plans, & architectural renderings.

Fire & Parking

The YMCA is working with the City of Ketchum Fire Department to solidify unobstructed fire and emergency access routes. In addition, the YMCA is working with the city to create temporary parking lot(s) for YMCA patrons that would be utilized to replace the parking spots that would be vacated with the proposed addition. A more formalized plan will be made available as part of the full design review application.

Compatibility of Design & Architectural Features

Staff reviewed standards as it relates to a predesign review application and found the standards were met in regard to design.

1) KMC §17.96.060.E. *Compatibility of design* states; 1. *The project's materials, colors and signing shall be complementary with the townscape, surrounding neighborhoods and adjoining structures.* The materials proposed on Sheet A.4(See Image below) match the existing structural materials including the shingled roof, brick, horizontal board siding, metal finishes, wood columns and beams, masonry, board and batten, and the existing metal and glass doors. The colors of the proposed materials are designed to match the existing building therefore the addition would appear to be fully integrated into the existing architectural palate and materials. Refer to Sheet A4: Building Elevation and Materials for Specific locations of building materials.



BRICK VENEER
RUNNING BOND,
TO MATCH EXISTING



COMPOSITE BOARD SIDING
HORIZONTAL SHIPLAP,
TO MATCH EXISTING



METAL CLADDING/FLASHING
MATTE BRONZE,
TO MATCH EXISTING



SHINGLE SLOPED ROOFING
FINISH TO MATCH EXISTING



WOOD COLUMNS AND BEAMS
STAINED, PLAIN SAWN, TO
MATCH EXISTING



BOARD AND BATTEN SIDING
TO MATCH EXISTING



METAL DOORS AND WINDOWS
BRONZE FINISH TO MATCH
EXISTING



SPLIT FACED MASONRY
TO MATCH EXISTING

2) Architectural Standards KMC §17.96.060.F.2,3,&5 state:

- The building character shall be clearly defined by use of architectural features.
- There shall be continuity of materials, colors and signing within the project.
- Building walls shall provide undulation/relief, thus reducing the appearance of bulk and flatness.

The proposed addition (See gray portion of Fig 2 below) continues the existing character as seen in the use of the same materials and color palettes. The continuation of the flat roof and three sloped roofed areas add interest within the addition and mirror the roof design existing throughout the structure.

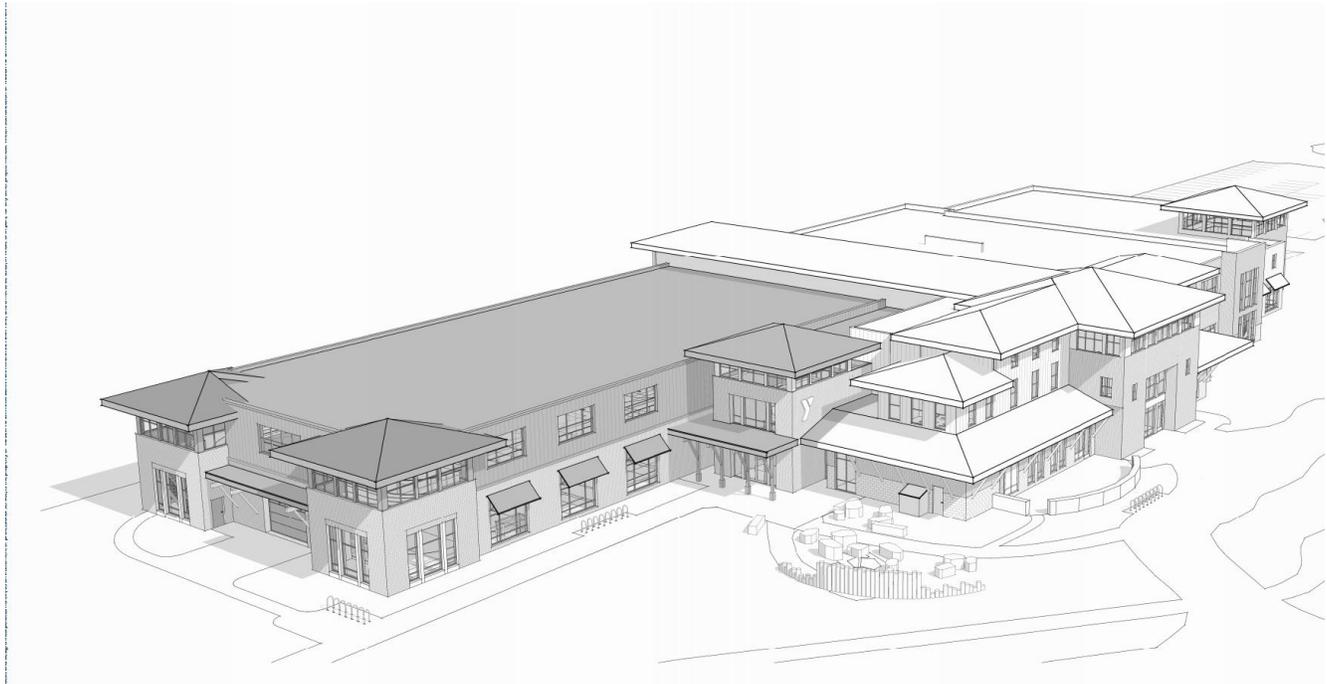


Figure 2 3D Perspective of Addition

The western elevation (Fig 3) of the addition which faces Warm Spring Road has continued the design of predominate brick veneer with the hardboard on the second level. The bulk and mass is broken up by mirroring pedestrian scale awnings seen on the southern end of the building, inclusion of a variety of roof lines, and the large windows with panes to break up the flatness of the large walls. (See Sheet A.4 Building Elevations & Materials)

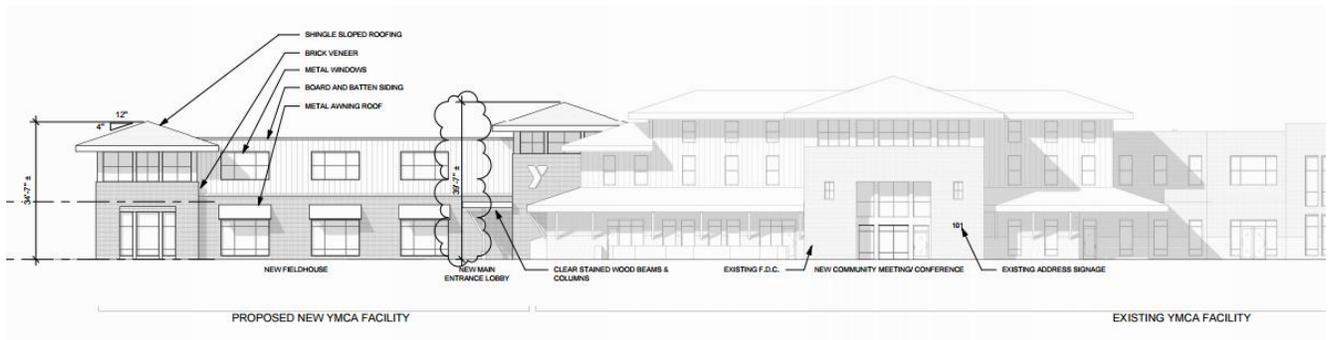


Figure 3 Western Elevation



Figure 4 Eastern Elevation

The eastern façade is facing the service alley and matches the existing rear façade with composite board horizontal siding on the majority, split masonry on the bottom ~10, and a brick veneer building pop out. The two sloped roof lines and windows add interest and break up the large nature of the fieldhouse building.

Siting

The §17.96.060F. Architectural design standards have a variety of siting requirements that will be evaluated during design review. The following staff has pulled below for initial review are key for locational siting and all have been met as seen in Sheet A 1.2 Site Plan :

- 1. Building(s) shall provide unobstructed pedestrian access to the nearest sidewalk and the entryway shall be clearly defined.
- 6. Building(s) shall orient toward their primary street frontage.
- 7. Garbage storage areas and satellite receivers shall be screened from public view and located off alleys.

The YMCA fronts both Warm Springs & Saddle Road with a new front entrance proposed oriented toward Saddle Road. The majority of the building is existing and fronts Warm Springs. The address signage and YMCA logo will remain fronting the intersection of Saddle & Warm Springs. In addition, the use of large windows and undulation will help keep the eastern façade welcoming and prominent as the front of the building to the street.

The proposed garbage and recycling area is labeled behind Bonnie’s Garden on the Southern end of the service alley of the existing facility. This location would be screened from Warm Springs Road.

The City ROW including sidewalks are outside the leased boundary and will not be altered. However proposed sidewalks for the facility will be connected to the existing sidewalks as seen on the Site Plan Sheet A.1.2. The lobby entrance rooflines, signage, and (heated) walkway would clearly delineate the entryway.

3) Landscaping

The applicant appears to be proposing general landscaping design concepts that are analogous with the standards:

- KMC §17.96.060.F4 Accessory structures, fences, walls and landscape features within the project shall match or complement the principal building.
- KMC §17.96.060.I.4. Expands on this concept that Landscaping shall provide a substantial buffer between land uses, including, but not limited to, structures, streets and parking lots. The development of landscaped public courtyards, including trees and shrubs where appropriate, shall be encouraged.

The applicant is proposing to move the existing front entrance to the northern addition creating a new front lobby. The proposal on site plan Sheet A.1 describes landscaping features to be installed to buffer the northeast and west portions of the new structure with the surrounding hardscape . New bike racks are also shown. The existing “Wendy’s Garden” will remain and is surrounded currently by permeable landscaping that also serves as 2,557sf of snow storage area. More specific

details will be required at time of design review regarding anticipated plant types and location as required in the landscaping section I: 1-4. of the evaluation standards.

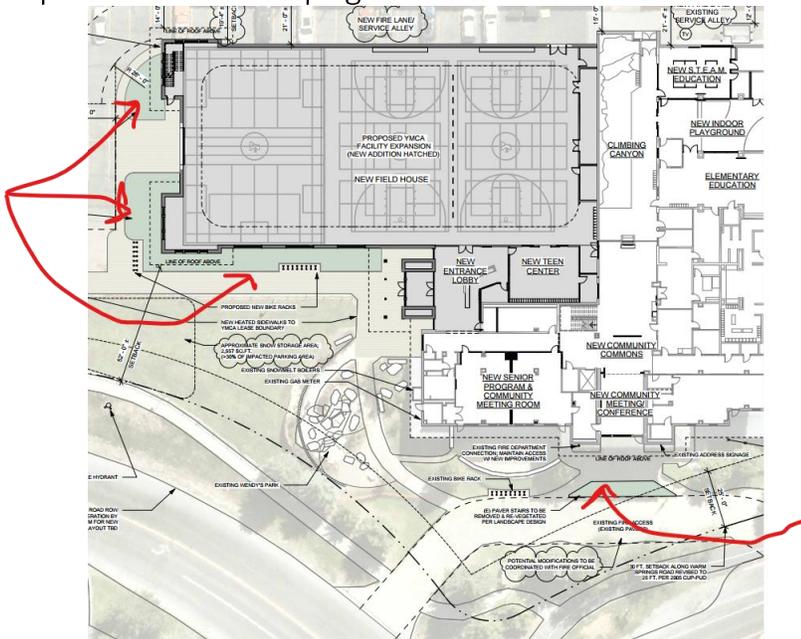


Figure 5: Site Plan(Sheet A1) highlighting areas to be landscaped

V. STAFF RECOMMENDATION

As this is a Pre-Application meeting, there is no formal action taken by the Commission. Staff recommends the Commission review the comments outlined by staff and provide any additional feedback to the applicant for consideration as they move through the Design Review process.

ATTACHMENTS:

- A. I. Application
- II. Project Narrative
- III. Design Review Drawings
- B. Design Review Evaluation Standards KMC S17.96
- C. Public Comment-No Public Comment on the date of this Staff Report

ATTACHMENT A

I. Project Application

II. Project Narrative

III. Pre-Application Drawings



**City of Ketchum
Planning & Building**

Pre-Application Design Review

OFFICIAL USE ONLY
File Number:
Date Received:
By:
Pre-Application Fee Paid:
Design Review Fee Paid:
By:

Submit completed application and documentation to planningandbuilding@ketchumidaho.org. If you have questions, please contact the Planning and Building Department at (208) 726-7801. Design Review criteria, zoning regulations, and development standards are specified in Title 17 of Ketchum Municipal Code, which may be viewed by clicking the link [here](#). You will be contacted and invoiced once your application package is complete.

APPLICANT INFORMATION			
Project Name:	Wood River Community YMCA	Phone:	520.270.6846
Owner:	Matt Neilson, deChase Miksis Development (Owner's Rep)	Mailing Address:	PO Box 733 Boise, ID 83701
Email:	Matt@dechase.com		
Architect/Representative:	Michael Bulls, RLB Architectura	Phone:	208.726.5608
Email:	mbulls@rlb-sv.com	Mailing Address:	PO Box 5619 Ketchum, ID 83340
Architect License Number:	AR 984243		
Engineer of Record:	Matt Walker, RLB Architectura	Phone:	208.726.5608
Email:	matt@rlb-sv.com	Mailing Address:	PO Box 5619 Ketchum, ID 83340
Engineer License Number:	12593		
Primary Contact Name and Phone Number: Michael Bulls, Architect, Office: 208.726.5608			
PROJECT INFORMATION			
Legal Land Description: TL 6689, SEC 12 4N 17E, Exempted App received 2025		Street Address: 101 Saddle Road	
Lot Area (Square Feet):	248,385	Zoning District: Tourist (T)	RPK #: LRK4N170121200
Overlay District:	<input type="checkbox"/> Floodplain	<input type="checkbox"/> Avalanche	<input type="checkbox"/> Mountain <input checked="" type="checkbox"/> None
Type of Construction:	<input type="checkbox"/> New	<input checked="" type="checkbox"/> Addition	<input checked="" type="checkbox"/> Remodel <input type="checkbox"/> Other
Anticipated Use:	Wellness & Recreation	Number of Residential Units:	NA
GROSS FLOOR AREA			
	Proposed	Existing	
Basements	NA Sq. Ft.	NA	Sq. Ft.
1 st Floor	18,860 Sq. Ft.	38,262	Sq. Ft.
2 nd Floor	7,604 Sq. Ft.	18,176	Sq. Ft.
3 rd Floor	NA Sq. Ft.	NA	Sq. Ft.
Mezzanine	NA Sq. Ft.	NA	Sq. Ft.
Total	26,464 Sq. Ft.	56,438	Sq. Ft.
FLOOR AREA RATIO			
Community Core:	Tourist: 0.34	General Residential-High:	
BUILDING COVERAGE/OPEN SPACE			
Percent of Building Coverage: Building coverage/open space for entire Development Parcel to be confirmed with City of Ketchum			
DIMENSIONAL STANDARDS/PROPOSED SETBACKS			
Front: 52'-0"± (SADDLE ROAD)	Side: NA	Side: NA	Rear: 14'-0"± (SERVICE ALLEY)
Building Height: 45'-6" (EXISTING) , 39'-7"± (PROPOSED ADDITION)			
OFF STREET PARKING			
Parking Spaces Provided:	Coordinate with City of Ketchum	Curb Cut: NA Sq. Ft. NA	% NA

The Applicant agrees in the event of a dispute concerning the interpretation or enforcement of the Design Review Application in which the city of Ketchum is the prevailing party, to pay the reasonable attorney fees, including attorney fees on appeal and expenses of the city of Ketchum. I, the undersigned, certify that all information submitted with and upon this application form is true and accurate to the best of my knowledge and belief.


12.30.2025

 Signature of Owner/Representative Date

PRE-APPLICATION DESIGN REVIEW SUBMITTAL CHECKLIST

A Pre-Application Design Review submittal shall include the materials listed in the following table in digital format. Please fill out the checklist in the table below and include the sheet number(s) where the required submittal material may be found in the project plan set. The applicant may elect to provide more details, additional plan sheets, and other supplemental materials not listed in the checklist at their discretion.

PRE-APPLICATION DESIGN REVIEW SUBMITTAL CHECKLIST		
Submitted by Applicant	Plan Sheet Number(s)	Pre-Application Required Materials
<input checked="" type="checkbox"/>	N/A	Project Narrative: A project narrative describing the approach and concept of the project and how the project meets the applicable design review criteria. <i>(narrative shall include a response to each applicable criteria)</i>
<input checked="" type="checkbox"/>	A1.0 - A1.2	Conceptual Site Plan: A conceptual site plan showing proposed on and off-site improvements. Site plan shall include conceptual landscaping and public amenities. Detailed plant list not required.
<input checked="" type="checkbox"/>	A2.0 - A2.2 A4.0	Conceptual Elevations and Floor Plans: Elevations and floor plans for all facades and all levels shall be provided. Elevations shall depict materiality, however, colored renderings not required.
<input checked="" type="checkbox"/>	A4.0	Conceptual Materials and Color Palette: Materials and colors sample board shall be provided for all facades. Photos of materials, representative imagery, and other digital representation of concept is acceptable. Specifications of materials and colors are not required.
<input checked="" type="checkbox"/>	A5.0 - A5.1	3D Perspectives: A minimum of two perspectives, one from a street view and one from bird's eye view, showing the massing of the proposed project within the context of the surrounding neighborhood. Adjacent properties and structures must be included. Full color renderings or photo-realistic perspectives are not required.

Preapplication Design Review Standards

Preapplication review is required for all new non-residential and multi-family residential developments with four or more stories and all new developments on a lot or lots totaling 11,000 square feet or more. Applicants of projects exempt from preapplication design review may request a preapplication design review at their discretion.

Please note that projects that conducted a Pre-Application Design Review meeting with the Planning & Zoning Commission, as required or voluntary, must file a complete Design Review application and pay all required fees within 180 calendar days of the last review meeting on the Pre-Application with the Commission, otherwise the Pre-Application review will become null and void.

The purpose of preapplication review is to allow the Commission to exchange ideas and give direction to the applicant on the "design concept", keeping in mind the purpose of this chapter and the application of the evaluation standards.

Design Review Evaluation Standards

17.96.060: Improvements and Standards

A. Streets:

1. The applicant shall be responsible for all costs associated with providing a connection from an existing city street to their development.
2. All street designs shall be approved by the City Engineer.

B. Sidewalks:

1. All projects under 17.96.010(A) that qualify as a "Substantial Improvement" shall install sidewalks as required by the Public Works Department.
2. Sidewalk width shall conform to the city's right of way standards; however the city engineer may reduce or increase the sidewalk width and design standard requirements at their discretion.
3. Sidewalks may be waived if one of the following criteria is met:
 - i. The project comprises an addition of less than two hundred fifty (250) square feet of conditioned space.
 - ii. The city engineer finds that sidewalks are not necessary because of existing geographic limitations, pedestrian traffic on the street does not warrant a sidewalk, or if a sidewalk would not be beneficial to the general welfare and safety of the public.
4. The length of sidewalk improvements constructed shall be equal to the length of the subject property line(s) adjacent to any public street or private street.
5. New sidewalks shall be planned to provide pedestrian connections to any existing or future sidewalks adjacent to the site. In addition, sidewalks shall be constructed to provide safe pedestrian access to and around a building.

6. The city may approve and accept voluntary cash contributions in lieu of the above described improvements, which contributions must be segregated by the city and not used for any purpose other than the provision of these improvements. The contribution amount shall be one hundred ten percent (110%) of the estimated costs of concrete sidewalk and drainage improvements provided by a qualified contractor, plus associated engineering costs, as approved by the city engineer. Any approved in lieu contribution shall be paid before the city issues a certificate of occupancy.

C. Drainage:

1. All storm water shall be retained on site.
2. Drainage improvements constructed shall be equal to the length of the subject property lines adjacent to any public street or private street.
3. The City Engineer may require additional drainage improvements as necessary, depending on the unique characteristics of a site.
4. Drainage facilities shall be constructed per city standards.

D. Utilities:

1. All utilities necessary for the development shall be improved and installed at the sole expense of the applicant.
2. Utilities shall be located underground and above grade utility, power and communication equipment within the development site shall be concealed from public view.
3. When extension of utilities is necessary all developers will be required to pay for and install two (2") inch SDR11 fiber optical conduit. The placement and construction of the fiber optical conduit shall be done in accordance with City of Ketchum standards and at the discretion of the City Engineer.

E. Compatibility of Design:

1. The project's materials, colors and signing shall be complementary to the townscape, surrounding neighborhoods and adjoining structures.
2. Preservation of significant landmarks shall be encouraged and protected, where applicable. A significant landmark is one which gives historical and/or cultural importance to the neighborhood and/or community.
3. Additions to existing buildings, built prior to 1940, shall be complementary in design and use similar material and finishes of the building being added to.

F. Architectural:

1. Building(s) shall provide unobstructed pedestrian access to the nearest sidewalk and the entryway shall be clearly defined.
2. The building character shall be clearly defined by use of architectural features.
3. There shall be continuity of materials, colors and signing within the project.
4. Accessory structures, fences, walls, and landscape features within the project shall match or complement the principal building.
5. Building walls shall provide undulation/relief, thus reducing the appearance of bulk and flatness.
6. Building(s) shall orient towards their primary street frontage.

7. Garbage storage areas and satellite receivers shall be screened from public view and located off alleys.
8. Building design shall include weather protection which prevents water to drip or snow to slide on areas where pedestrians gather and circulate or onto adjacent properties.

G. Circulation Design:

1. Pedestrian, equestrian, and bicycle access shall be located to connect with existing and anticipated easements and pathways.
2. Awnings extending over public sidewalks shall extend five (5') feet or more across the public sidewalk but shall not extend within two (2') feet of parking or travel lanes within the right of way.
3. Traffic shall flow safely within the project and onto adjacent streets. Traffic includes vehicle, bicycle, pedestrian, and equestrian use. Consideration shall be given to adequate sight distances and proper signage.
4. Curb cuts and driveway entrances shall be no closer than twenty (20') feet to the nearest intersection of two or more streets, as measured along the property line adjacent to the right of way. Due to site conditions or current/projected traffic levels or speed, the City Engineer may increase the minimum distance requirements.
5. Unobstructed access shall be provided for emergency vehicles, snowplows, garbage trucks and similar service vehicles to all necessary locations within the proposed project.

H. Snow Storage:

1. Snow storage areas shall not be less than thirty percent (30%) of the improved parking and pedestrian circulation areas.
2. Snow storage areas shall be provided on-site.
3. A designated snow storage area shall not have any dimension less than five (5') feet and shall be a minimum of twenty-five (25) square feet.
4. In lieu of providing snow storage areas, snow melt and hauling of snow may be allowed.

I. Landscaping:

1. Landscaping is required for all projects.
2. Landscape materials and vegetation types specified shall be readily adaptable to a site's microclimate, soil conditions, orientation and aspect, and shall serve to enhance and complement the neighborhood and townscape.
3. All plant species shall be drought tolerant. Native species are recommended but not required.
4. Landscaping shall provide a substantial buffer between land uses, including, but not limited to, structures, streets and parking lots. The development of landscaped public courtyards, including trees and shrubs where appropriate, shall be encouraged.

J. Public Amenities:

1. Where sidewalks are required, pedestrian amenities shall be installed. Amenities may include, but are not limited to, benches and other seating, kiosks, bus shelters, trash receptacles, restrooms, fountains, art, etc. All public amenities shall be approved by the Public Works Director prior to design review approval from the Commission.

17.96.070: COMMUNITY CORE (CC) PROJECTS

In addition to the requirements of section 17.96.060, unless otherwise specified, the below standards apply to projects in the Community Core district.

A. Streets:

1. Street trees, streetlights, street furnishings, and all other street improvements shall be installed or constructed as determined by the Public Works Department.
2. Street trees with a minimum caliper size of three inches (3"), shall be placed in tree grates.
3. Due to site constraints, the requirements of this subsection A may be modified by the Public Works Department.

B. Architectural:

1. Facades facing a street or alley or located more than five (5') feet from an interior side property line shall be designed with both solid surfaces and window openings to avoid the creation of blank walls and employ similar architectural elements, materials and colors as the front façade.
2. For nonresidential portions of buildings, front building facades and facades fronting a pedestrian walkway shall be designed with ground floor storefront windows and doors with clear transparent glass. Landscaping planters shall be incorporated into facades fronting pedestrian walkways.
3. For nonresidential portions of buildings, front facades shall be designed to not obscure views into windows.
4. Roofing forms and materials shall be compatible with the overall style and character of the structure. Reflective materials are prohibited.
5. All pitched roofs shall be designed to sufficiently hold all snow with snow clips, gutters and downspouts.
6. Roof overhangs shall not extend more than three (3') feet over a public sidewalk. Roof overhangs that extend over the public sidewalk shall be approved by the Public Works Director.
7. Front porches and stoops shall not be enclosed on the ground floor by permanent or temporary walls, windows, window screens, or plastic or fabric materials.

C. Service Areas and Mechanical/Electrical Equipment:

1. Trash disposal areas and shipping and receiving areas shall be located within parking garages or to the rear of buildings. Trash disposal areas shall not be located within the public right of way and shall be screened from public views.
2. Roof and ground mounted mechanical and electrical equipment shall be fully screened from public view. Screening shall be compatible with the overall building design.

D. Landscaping:

1. When a healthy and mature tree is removed from a site, it shall be replaced with a new tree. Replacement trees may occur on or off site.
2. Trees that are placed within a courtyard, plaza or pedestrian walkway shall be placed within tree wells that are covered by tree grates.
3. The city arborist shall approve all parking lot and replacement trees.

E. Surface Parking Lots:

1. Surface parking lots shall be accessed from off the alley and shall be fully screened from the street.
2. Surface parking lots shall incorporate at least one (1) tree and one (1) additional tree per ten (10) onsite parking spaces. Trees shall be planted in landscaped planters, tree wells and/or diamond shaped planter boxes located between parking rows. Planter boxes shall be designed so as not to impair vision or site distance of the traveling public.
3. Ground cover, low lying shrubs, and trees shall be planted within the planters and planter boxes. Tree grates or landscaping may be used in tree wells located within pedestrian walkways.

F. Bicycle Parking:

1. One (1) bicycle rack, able to accommodate at least two (2) bicycles, shall be provided for every four (4) parking spaces as required by the proposed use. At a minimum, one (1) bicycle rack shall be required per development on private property. Bike racks shall not be located in the public right-of-way.
2. When the calculation of the required number of bicycle racks called for in this section results in a fractional number, a fraction equal to or greater than one-half (1/2) shall be adjusted to the next highest whole number.
3. Bicycle racks shall be clearly visible from the building entrance they serve and not mounted less than fifty (50') feet from said entrance or as close as the nearest non-ADA parking space, whichever is closest.

WOOD RIVER COMMUNITY YMCA
CITY OF KETCHUM PRE-APPLICATION
PROJECT NARRATIVE
12.30.2025



PROJECT SCOPE OF WORK:

The scope of work includes alterations and an addition to the existing Wood River Community YMCA.

Interior Alterations

Main level interior alterations consist of expanding and renovating existing spaces including the senior program & community meeting room, community commons area, and youth education spaces. Upper level interior alterations include the relocation of health partner offices and the expansion of wellness and functional training areas.

Addition

The proposed addition consists of a new main entrance lobby, teen center, and fieldhouse with upper level running track located on the north end of the existing facility.

Exterior Improvements

Proposed exterior improvements will be located within the YMCA lease boundary and consist of new heated sidewalks, landscaped areas adjacent to the proposed addition, and the removal of stairs leading up to the existing main entrance. Reconfigured parking and fire/emergency access located on the north side of the proposed addition and within the YMCA lease boundary are included in the scope of work.

DESIGN APPROACH AND CONCEPT:

The proposed addition to the YMCA makes use of architectural features and materials similar to the existing facility to create an aesthetically cohesive building. Architectural elements are used to soften the perceived massing of the proposed fieldhouse. The new main entrance lobby is designed to complement the adjacent architectural forms with lofted clerestory glass and sloped roof.

DESIGN REVIEW EVALUATION STANDARDS:

17.96.060: Improvements and Standards

A. Streets

Existing street access from Saddle Road and Warm Springs Road are located adjacent to City of Ketchum parking areas to the north and south, respectively, of the YMCA lease area and are outside of the project scope. No new city street connections or curb cuts are proposed for the project.

B. Sidewalks

Existing sidewalks adjacent to Warm Springs and Saddle roads will remain and are outside of the project scope of work. A new section of sidewalk within the YMCA lease area and adjacent to the west edge of the north parking lot is proposed to accommodate the proposed addition. The new sidewalk section will extend to the new main entrance lobby and will tie into the existing sidewalks and paths within the YMCA lease area.

C. Drainage

Stormwater from the proposed addition, new sidewalks and hardscape, new planting areas, and the portion of the new parking area to the North of the proposed addition and within the YMCA lease area will be retained on site.

Existing stormwater drainage systems outside of the project scope of work will remain.

D. Utilities

All new utilities and connections required to service the proposed improvements will be installed at the sole expense of the applicant and will be installed underground, concealed from public view.

E. Compatibility of Design

The project's materials, colors, and signage will match the existing YMCA facility where possible. Exterior materials including brick, composite wood wall siding, metal windows and doors, stained timber beams and details, and sloped shingle roofing are consistent with the various existing architecture seen in surrounding neighborhoods and across Ketchum. The proposed addition will serve to conceal the currently exposed structural concrete masonry units along the North elevation of the existing YMCA facility; thus, creating a proper architectural design for the north façade.

F. Architectural

The proposed addition to the YMCA makes use of architectural features and materials similar to the existing facility to create an aesthetically cohesive building. Architectural features including sloped roofs, awning roofs, stepped exterior walls, and differing materials are used to soften the perceived massing of the proposed fieldhouse addition. New and existing sidewalks will connect parking areas to a new main entrance lobby that is designed to complement the adjacent architectural forms with lofted clerestory glass and sloped roof. The length of the proposed addition and the overall YMCA facility orients toward Saddle and Warm Springs roads.

Garbage storage areas, where provided, shall be screened from public view and located off the service alley.

Sloped roofs with engineered snow retention devices, gutters, and downspouts as well as flat roofs with internal roof drains will protect pedestrian areas below from snow sliding and ice accumulation.

G. Circulation Design

Vehicle access to the site occurs on parking areas to the north and south of the existing YMCA facility. These parking areas connect to Saddle Road and Warm Springs Road, respectively, on property owned by the City of Ketchum outside of the YMCA lease area. The south parking area is entirely outside of the project scope of work. The portion of the north

parking area within the YMCA lease area will be partially reconfigured as part of the project scope of work. The YMCA shall coordinate with the City of Ketchum so that the greater parking layout meets fire apparatus access and accessible parking requirements. No new city street connections or curb cuts are included in the proposed scope of work.

New pedestrian and bicycle paths within the YMCA lease area will be snow-melted to provide safe approach to proposed addition and main entrance lobby. These paths will tie into the existing pathways within the YMCA lease area as well as into the existing sidewalks along Saddle Road and Warm Springs Roads. Existing bicycle racks will remain in place and new bicycle racks are proposed adjacent to the proposed addition.

H. Snow Storage

The lawn area to the west of the proposed addition, currently used as snow storage, will continue to serve as the snow storage area for the north parking area and new service alley.

New pedestrian and bicycle paths within the YMCA lease area are proposed to be snow-melted.

I. Landscaping

New landscaped areas are proposed along the north and west elevations of the new addition. Existing stairs to the west of the current main entry are to be removed and landscaped.

New landscaping shall follow City of Ketchum design standards, will be readily adaptable to the site's microclimate, soil conditions, orientation, and aspect, and shall be drought tolerant.

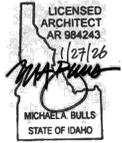
New landscaping is proposed to serve as a buffer between streets and parking areas and the YMCA facility.

J. Public Amenities

The existing bus stop and associated benches, existing sidewalks and light fixtures, existing bicycle racks, existing historical plaque, and Wendy's Park are outside of the project scope of work and will remain. New bicycle racks will be installed adjacent to the proposed addition.



ISSUED	
12.30.2025	PRE-APP. DESIGN REVIEW
01.27.2026	DELTA 1 SET



RUSCITTO LATHAM BLANTON
ARCHITECTURE P.A.
PO BOX 5619 KETCHUM, ID 83340
208.726.5608
www.rlb-arc.com



**WOOD RIVER
COMMUNITY YMCA**
101 SADDLE ROAD | KETCHUM, IDAHO

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DRAWING

VICINITY PLAN

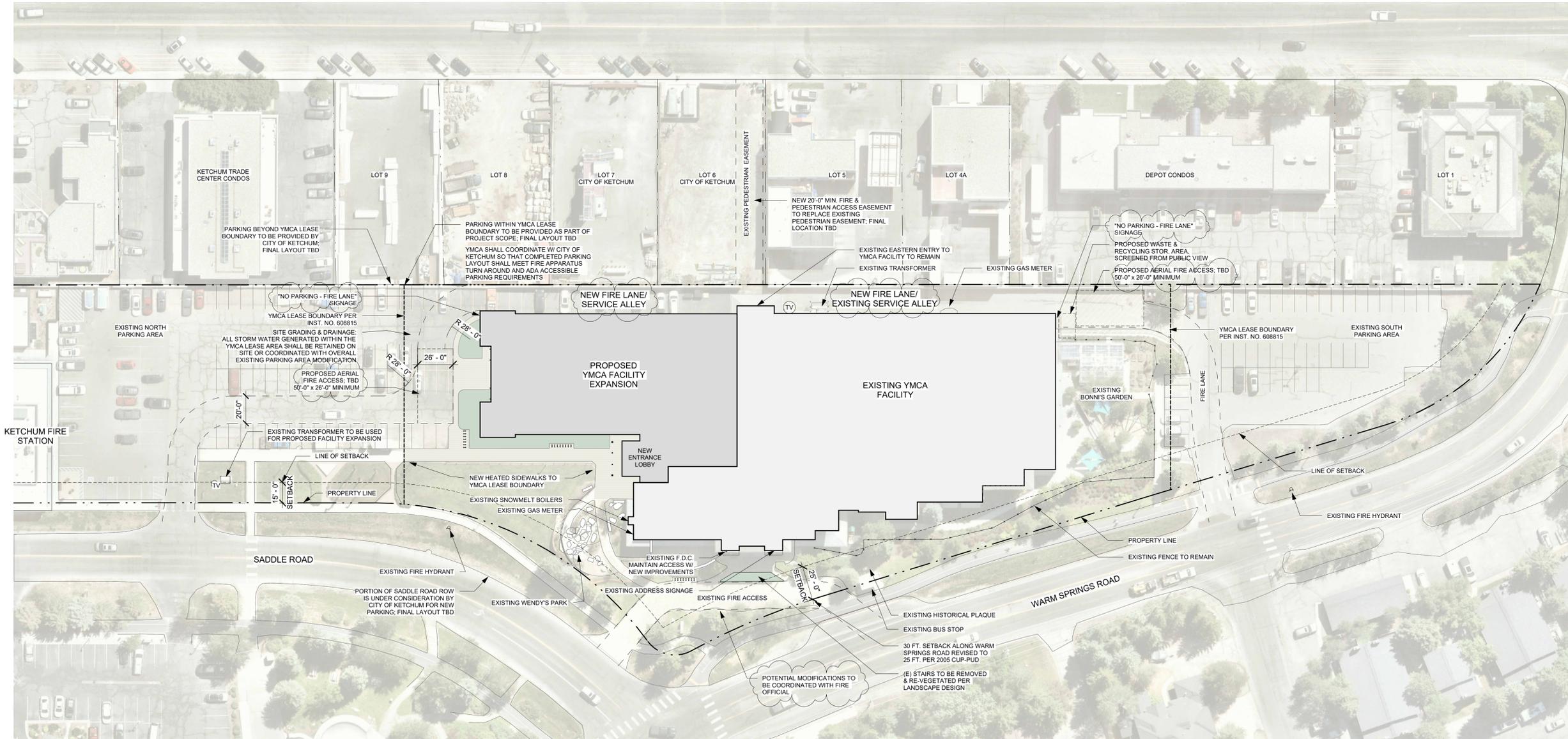
DWG. #

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OWNERSHIP OF DOCUMENTS: This document, and the ideas and designs incorporated herein, as an instrument of professional service, is the property of RUSCITTO LATHAM BLANTON ARCHITECTURA P.A., and is not to be used, in whole or in part, for any other project without written authorization of RUSCITTO LATHAM BLANTON ARCHITECTURA P.A.

ISSUED	PRE-APP. DESIGN REVIEW
12.30.2025	DELTA 1 SET
01.27.2026	



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WOOD RIVER
COMMUNITY YMCA
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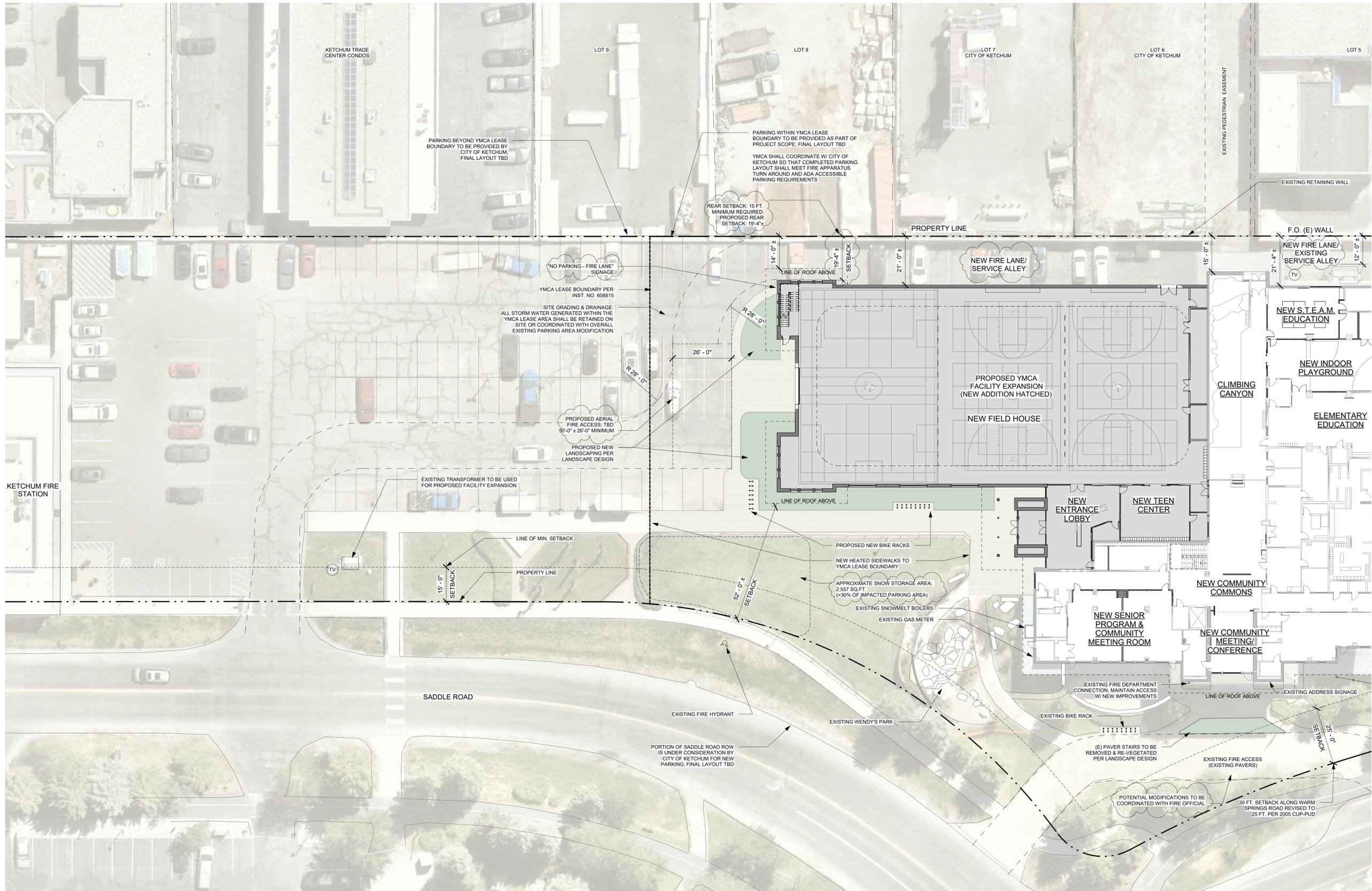
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01.27.2026	DELTA 1 SET



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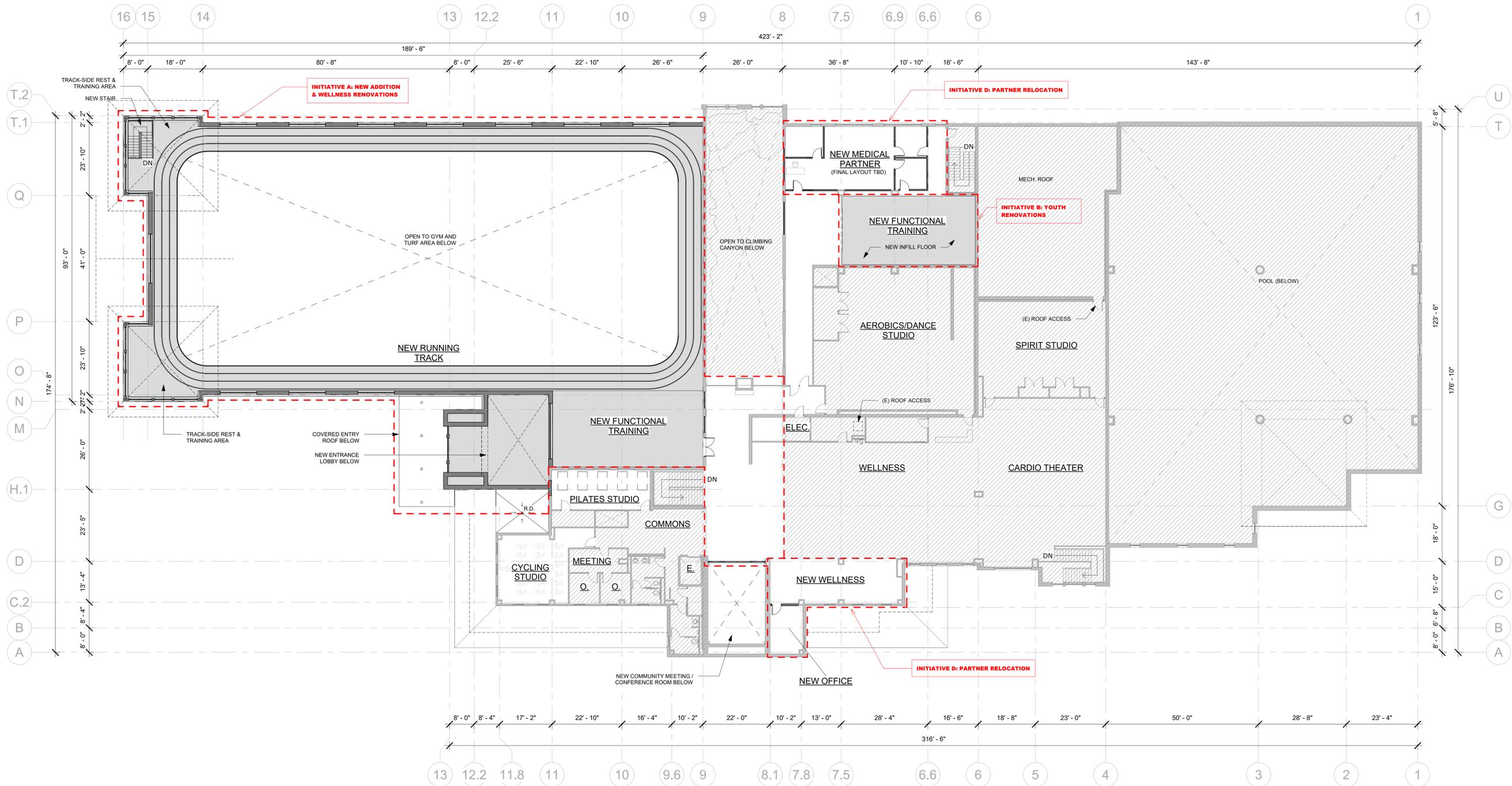
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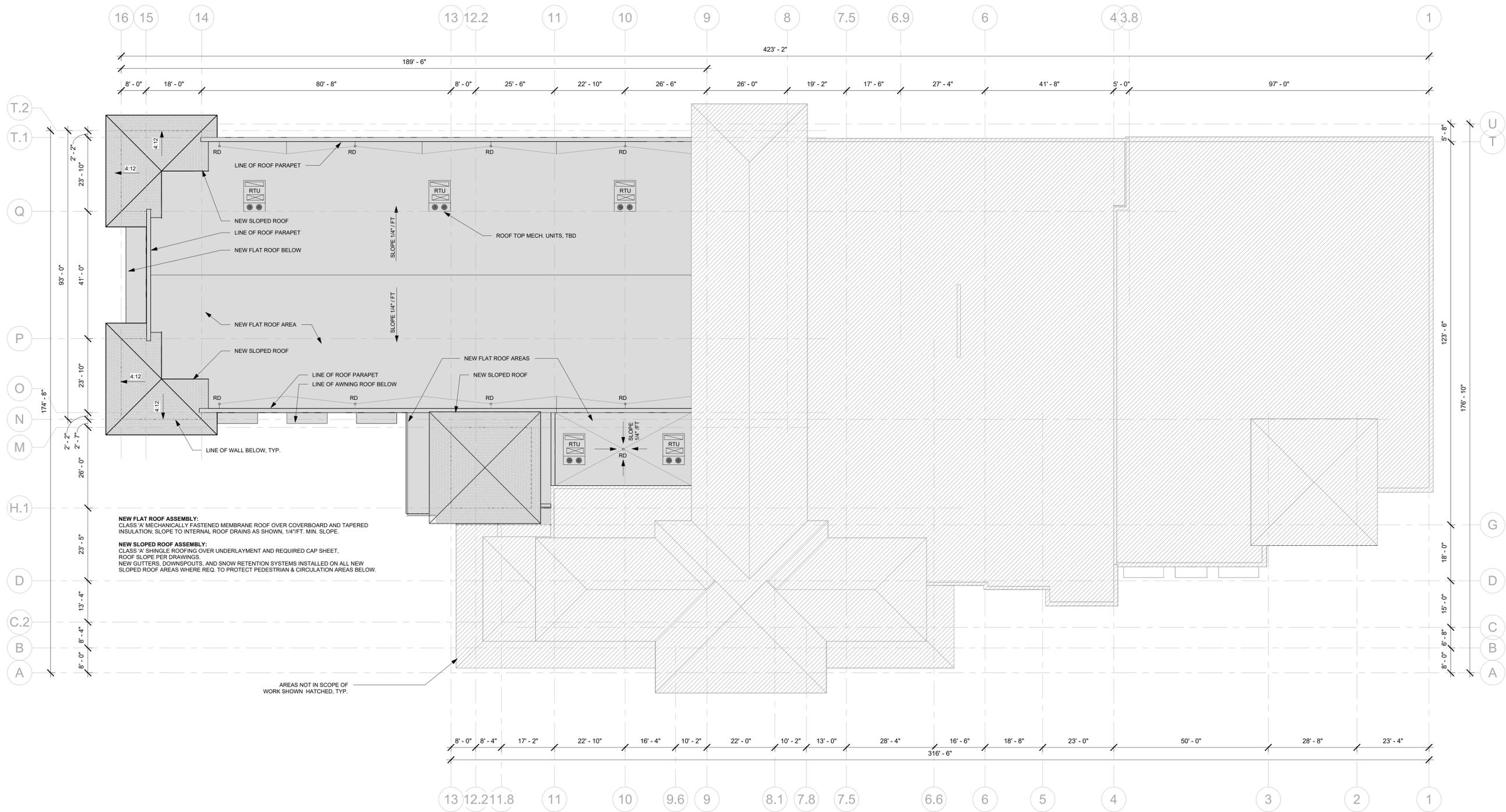
UPPER LEVEL FLOOR PLAN - OVERALL
 SCALE: 1/16" = 1'-0"

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ROOF PLAN

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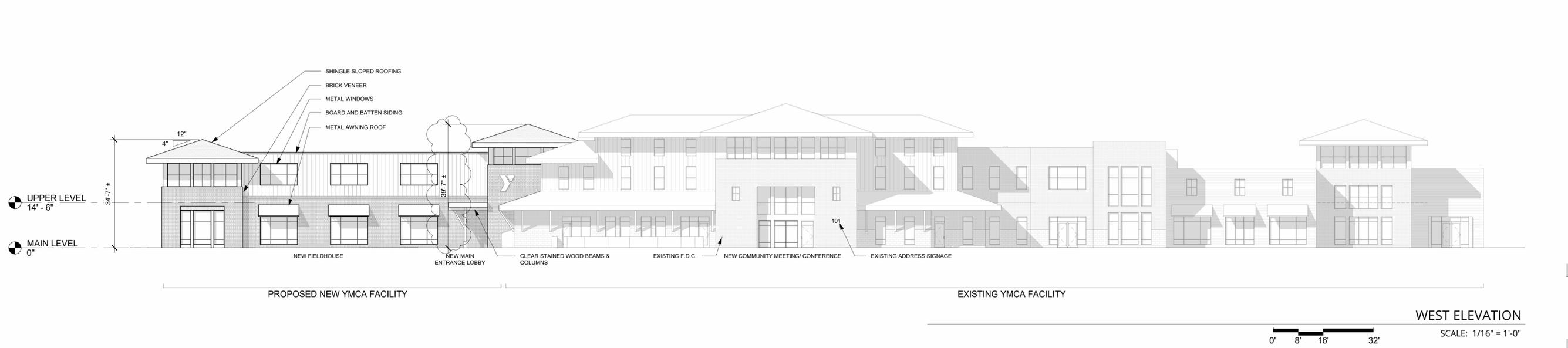
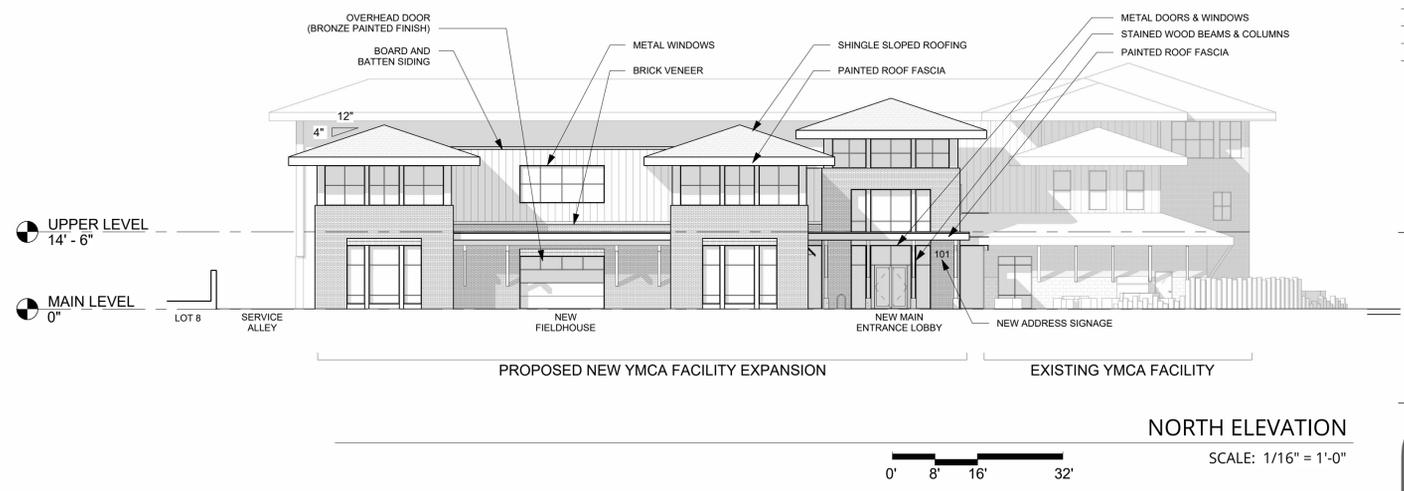
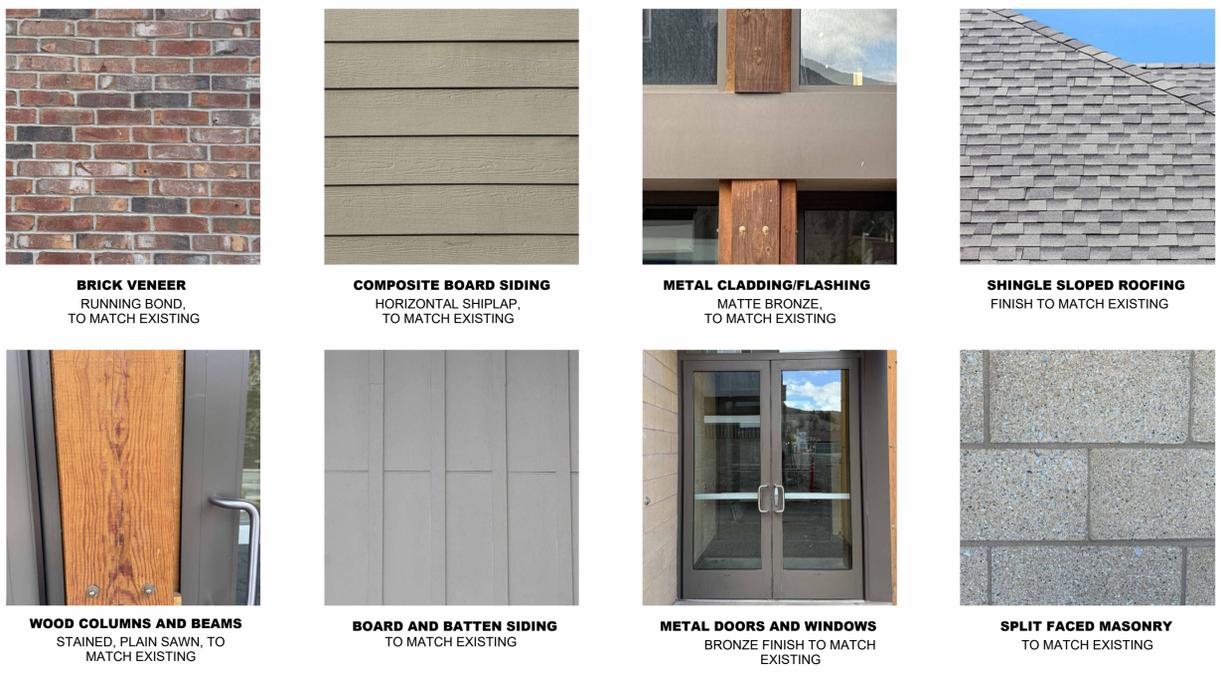
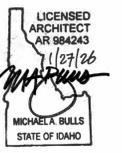
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ROOF PLAN
SCALE: 1/16" = 1'-0"



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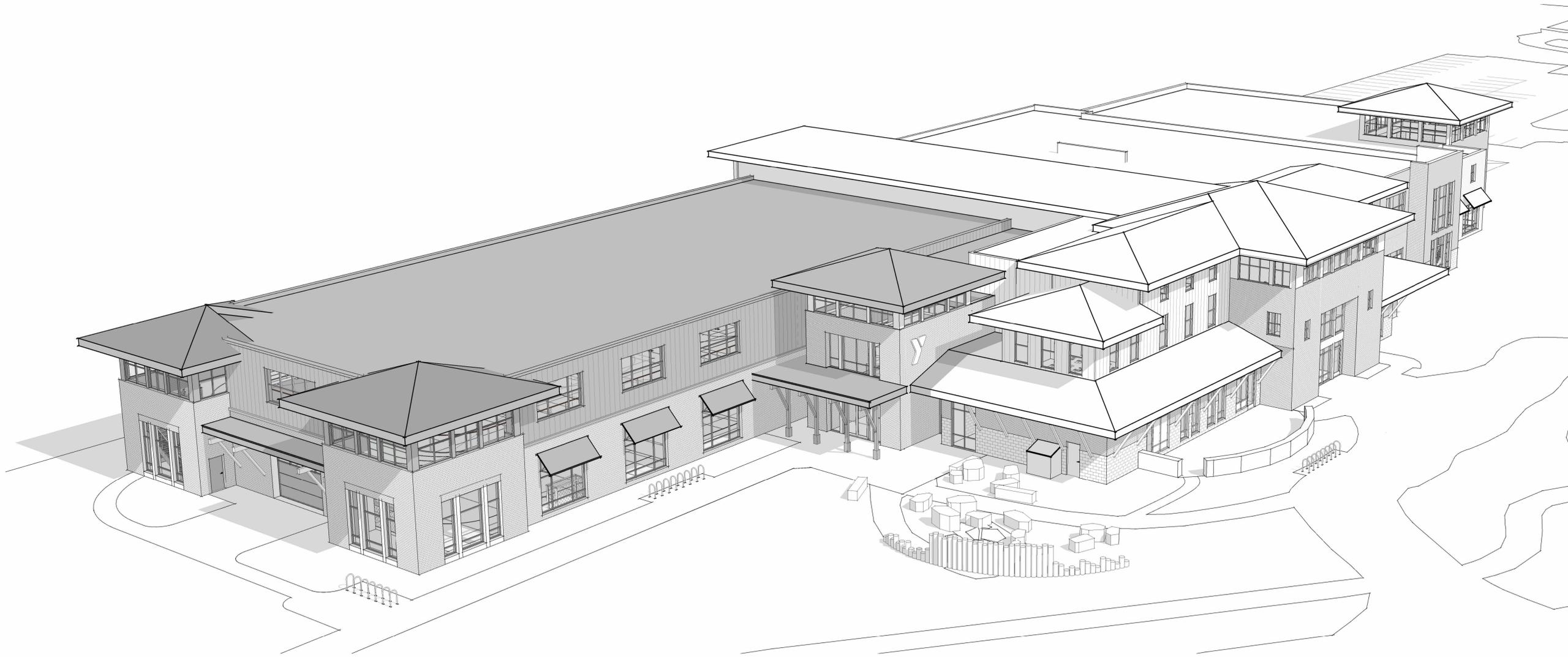
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VIEW OF YMCA NORTH ELEVATION AND PARKING LOT - LOOKING SOUTH WEST



VIEW OF YMCA NORTH ELEVATION AND EXTERIOR LAWN AREA - LOOKING SOUTH

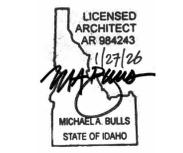


VIEW FROM LOT 6 TOWARD YMCA CLIMBING CANYON ENTRANCE DOOR - LOOKING WEST



VIEW OF YMCA EXISTING MAIN ENTRANCE - LOOKING SOUTH EAST

ISSUED	
12.30.2025	PRE-APP. DESIGN REVIEW
01.27.2026	DELTA 1 SET



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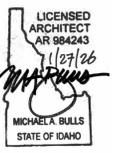
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SITE IMAGES

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12.30.2025	PRE-APP. DESIGN REVIEW
01.27.2026	DELTA 1 SET



VIEW OF EXISTING ARCHITECTURAL TOWER AT YMCA STAIR - LOOKING EAST



VIEW OF EXISTING ARCHITECTURAL TOWER AT YMCA POOL - LOOKING EAST

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ATTACHMENT B

Design Review Evaluation Standards

KMC §17.96.060 Improvements and Standards

KETCHUM CITY CODE

to the completion of the improvements and complete construction of the improvements. Following completion of required improvements the City shall return excess funds to the applicant.

(Ord. 1148, 2016; Ord. No. 1249, § 17, 10-2-2023)

17.96.060 Improvements and standards.

Improvements and standards for all projects listed in subsection 17.96.010.A of this chapter:

A. *Streets.*

1. The applicant shall be responsible for all costs associated with providing a connection from an existing City street to their development.
2. All street designs shall be approved by the City Engineer.

B. *Sidewalks.*

1. All projects under subsection 17.96.010.A of this chapter that qualify as a "substantial improvement" shall install sidewalks as required by the Public Works Department.
2. Sidewalk width shall conform to the City's right-of-way standards, however the City Engineer may reduce or increase the sidewalk width and design standard requirements at their discretion.
3. Sidewalks may be waived if one of the following criteria is met:
 - a. The project comprises an addition of less than 250 square feet of conditioned space.
 - b. The City Engineer finds that sidewalks are not necessary because of existing geographic limitations, pedestrian traffic on the street does not warrant a sidewalk, or if a sidewalk would not be beneficial to the general welfare and safety of the public.
4. The length of sidewalk improvements constructed shall be equal to the length of the subject property line(s) adjacent to any public street or private street.
5. New sidewalks shall be planned to provide pedestrian connections to any existing or future sidewalks adjacent to the site. In addition, sidewalks shall be constructed to provide safe pedestrian access to and around a building.
6. The City may approve and accept voluntary cash contributions in lieu of the above described improvements, which contributions must be segregated by the City and not used for any purpose other than the provision of these improvements. The contribution amount shall be 110 percent of the estimated costs of concrete sidewalk and drainage improvements provided by a qualified contractor, plus associated engineering costs, as approved by the City Engineer. Any approved in lieu contribution shall be paid before the City issues a certificate of occupancy.

C. *Drainage.*

1. All stormwater shall be retained on site.

ZONING REGULATIONS

2. Drainage improvements constructed shall be equal to the length of the subject property lines adjacent to any public street or private street.
3. The City Engineer may require additional drainage improvements as necessary, depending on the unique characteristics of a site.
4. Drainage facilities shall be constructed per City standards.

D. *Utilities.*

1. All utilities necessary for the development shall be improved and installed at the sole expense of the applicant.
2. Utilities shall be located underground and utility, power, and communication lines within the development site shall be concealed from public view.
3. When extension of utilities is necessary all developers will be required to pay for and install two-inch SDR11 fiber optical conduit. The placement and construction of the fiber optical conduit shall be done in accordance with City of Ketchum standards and at the discretion of the City Engineer.

E. *Compatibility of design.*

1. The project's materials, colors and signing shall be complementary with the townscape, surrounding neighborhoods and adjoining structures.
2. Preservation of significant landmarks shall be encouraged and protected, where applicable. A significant landmark is one which gives historical and/or cultural importance to the neighborhood and/or community.
3. Additions to existing buildings, built prior to 1940, shall be complementary in design and use similar material and finishes of the building being added to.

F. *Architectural.*

1. Building(s) shall provide unobstructed pedestrian access to the nearest sidewalk and the entryway shall be clearly defined.
2. The building character shall be clearly defined by use of architectural features.
3. There shall be continuity of materials, colors and signing within the project.
4. Accessory structures, fences, walls and landscape features within the project shall match or complement the principal building.
5. Building walls shall provide undulation/relief, thus reducing the appearance of bulk and flatness.
6. Building(s) shall orient toward their primary street frontage.
7. Garbage storage areas and satellite receivers shall be screened from public view and located off alleys.
8. Building design shall include weather protection which prevents water to drip or snow to slide on areas where pedestrians gather and circulate or onto adjacent properties.

KETCHUM CITY CODE

G. *Circulation design.*

1. Pedestrian, equestrian and bicycle access shall be located to connect with existing and anticipated easements and pathways.
2. Awnings extending over public sidewalks shall extend five feet or more across the public sidewalk but shall not extend within two feet of parking or travel lanes within the right-of-way.
3. Traffic shall flow safely within the project and onto adjacent streets. Traffic includes vehicle, bicycle, pedestrian and equestrian use. Consideration shall be given to adequate sight distances and proper signage.
4. Curb cuts and driveway entrances shall be no closer than 20 feet to the nearest intersection of two or more streets, as measured along the property line adjacent to the right-of-way. Due to site conditions or current/projected traffic levels or speed, the City Engineer may increase the minimum distance requirements.
5. Unobstructed access shall be provided for emergency vehicles, snowplows, garbage trucks and similar service vehicles to all necessary locations within the proposed project.

H. *Snow storage.*

1. Snow storage areas shall not be less than 30 percent of the improved parking and pedestrian circulation areas.
2. Snow storage areas shall be provided on site.
3. A designated snow storage area shall not have any dimension less than five feet and shall be a minimum of 25 square feet.
4. In lieu of providing snow storage areas, snowmelt and hauling of snow may be allowed.

I. *Landscaping.*

1. Landscaping is required for all projects.
2. Landscape materials and vegetation types specified shall be readily adaptable to a site's microclimate, soil conditions, orientation and aspect, and shall serve to enhance and complement the neighborhood and townscape.
3. All trees, shrubs, grasses and perennials shall be drought tolerant. Native species are recommended but not required.
4. Landscaping shall provide a substantial buffer between land uses, including, but not limited to, structures, streets and parking lots. The development of landscaped public courtyards, including trees and shrubs where appropriate, shall be encouraged.

J. *Public amenities.*

1. Where sidewalks are required, pedestrian amenities shall be installed. Amenities may include, but are not limited to, benches and other seating, kiosks, bus shelters, trash

ZONING REGULATIONS

receptacles, restrooms, fountains, art, etc. All public amenities shall receive approval from the Public Works Department prior to design review approval from the Commission.

K. *Underground encroachments.*

1. Encroachments of underground building(s) or portions of building(s) into required setbacks are subject to subsection 17.128.020.K of this title and shall not conflict with any applicable easements, existing underground structures, sensitive ecological areas, soil stability, drainage, other sections of this Code or other regulating codes such as adopted International Code Council Codes, or other site features concerning health, safety, and welfare.
2. No below grade structure shall be permitted to encroach into the riparian setback.
(Ord. 1148, 2016; Ord. 1186, 2018; Ord. No. 1249, § 17, 10-2-2023)

ATTACHMENT C

Public Comment



**City of Ketchum
Planning & Building**

**STAFF REPORT
KETCHUM PLANNING AND ZONING COMMISSION
MARCH 5, 2026 MEETING**

- PROJECT:** 233 Parkway Variance
- FILE NUMBER:** P25-059
- REPRESENTATIVE:** Darshan Amrit, A2 Studio (Architect)
- OWNER:** Jeff and Michelle Bruner
- REQUEST:** Variance to enlarge and extend an existing nonconforming garage into the front setback area and outside of the platted building envelope of the subject property on Parkway Drive.
- LOCATION:** 233 Parkway Drive
- ZONING:** Limited Residential (LR)
- OVERLAY:** Floodplain
- NOTICE:** A public meeting notice for the project was mailed to all owners of property within 300 feet of the project site and all political subdivisions on February 11, 2026. The notice was published in the Idaho Mountain Express on February 11, 2026. A notice was posted on the city's website on February 11, 2026, and posted on the project site on February 26, 2026.
- REVIEWERS:** Paige Nied – Associate Planner

EXECUTIVE SUMMARY

The applicant submitted a Variance Application for approval to demolish the existing nonconforming single vehicle garage attached to the residence and increase the degree of nonconformity by constructing a two-vehicle garage in its place that further encroaches into the minimum required front setback area and marginally into the side setback. The subject property is located at 233 Parkway Drive and is within the Limited Residential (LR) Zone District. The LR zone has a minimum front setback requirement of 15 feet and side setback of 1 foot for every 2 feet in building height. The existing encroachment is a 15-foot span of the building that extends into the front setback by 9 feet 5 inches at the north end and 5 feet 6 inches at the south end. The proposed encroachment is a roughly 24-foot span of the building that extends into the front setback by 10 feet 2 inches at the north end and 3 feet 8 inches at the south end. See Figure 1 for a head-on view of the existing encroachment and see Figure

2 for an aerial view. The garage would encroach approximately 3 ½ inches into the side setback area. The existing and proposed encroachment extends outside of the platted building envelope on the property.

Since the residence was constructed in 1972, there have been various additions to the structure by previous owners. The existing 2,997 square foot single-story residence is considered nonconforming due to the front portion of the attached garage encroaching into the setback area.

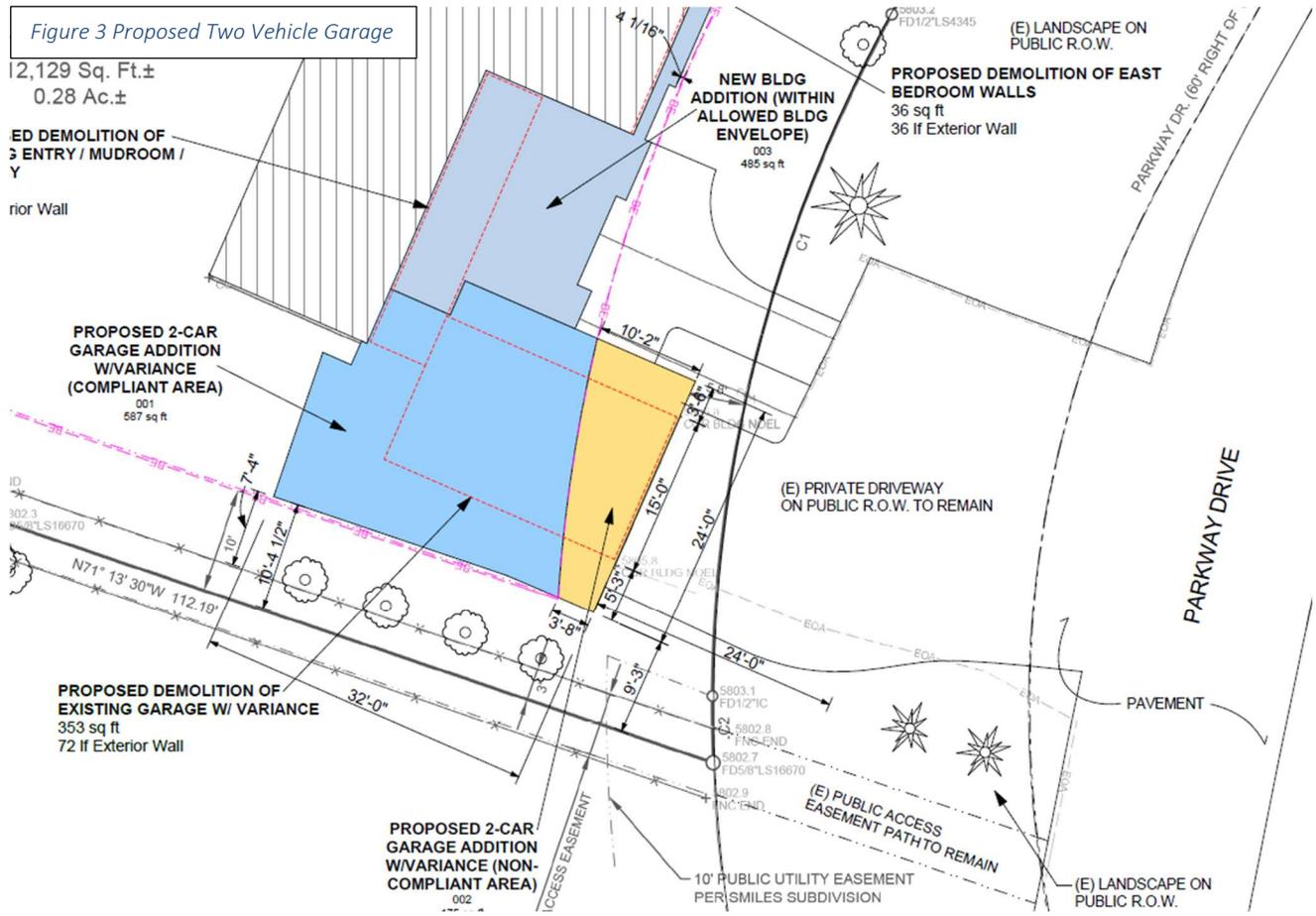


Figure 1 Existing non-conforming portion in question (head-on view)

The variance request is for the garage portion of the project only. However, to provide additional context for the design of the new garage, the applicant submitted a complete renovation design with the application materials for reference. The property owners have explored 5 options (A-E outlined on Sheet G-002 in the project plans) and concluded that Option E, which proposes to demolish and replace the garage in its current location would yield the least increase of nonconformity. As shown in Figure 3 below, the proposed encroachment is a roughly 24-foot span of the building that extends into the front setback by 10 feet 2 inches at the north end and 3 feet 8 inches at the south end.



Figure 2 Existing non-conforming portion in question (aerial view)



The siting and configuration of the existing residence create unique challenges for redesign and redevelopment. The subject property is constrained by the public access river easement along the southern boundary and the 25 foot scenic easement along the rear. The subject property also has a relatively shallow depth compared to the other lots in the neighborhood. During department review of the application, the City Engineer noted that the centerline of Parkway Drive is severely offset from the center of the right-of-way. As a result, although the garage is nonconforming, it remains farther from the roadway edge of asphalt than neighboring properties that comply with the 15 foot front setback requirement.

A portion along the rear of the property is within the Floodplain Overlay. The existing encroachment of the garage within the front setback area is not within the Floodplain and no development associated with the remodel and addition of the residence is proposed in the Floodplain. Therefore, a Floodplain Development Permit would not be required with the proposed redevelopment.

Only one off-street parking space exists for the residence within the one-car garage. Due to the alignment of the centerline of the road and the length of the driveway, all other parking for the residence occurs within the right-of-way. A minimum of two off-street parking spaces is required for single-family residences as outlined in the Off Street Parking Matrix (KMC 17.125.040.B). The two-vehicle garage proposed with Option E addresses the off-street parking requirements.

Non-conforming buildings are regulated by Ketchum Municipal Code (KMC) 17.136. Pursuant to KMC 17.136.050, a nonconforming building shall not be enlarged or extended so as to increase the degree of nonconformity. As the applicant is requesting to increase the degree of nonconformity, a Variance is

required. Staff has provided a review of the variance criteria outlined in KMC 17.148.010 below. Staff believe the request meets all the criteria and recommends approval of the Variance application subject to one condition.

The subject property has a platted building envelope, which the existing garage encroaches over. If the variance application is approved by the Planning & Zoning Commission, a plat amendment application will be required to modify the building envelope to contain the residence within the building envelope. As stated in condition of approval #1, the plat amendment application must be reviewed and approved by City Council prior to submitting a building permit application for the renovation and addition.

BACKGROUND

The Planning and Building Department received the Variance Application for the project on November 10, 2025. Following the receipt of the application, staff route the application materials to all city departments for review. The application was scheduled for a public hearing on January 23, 2026. All department comments have been resolved or addressed through the conditions of approval recommended below.

ANALYSIS

The KMC allows for the granting of variances to provisions of the code provided the applicant can demonstrate the following to be true:

A. The strict enforcement of the provisions of this Code creates an undue hardship to the property owner; however, economic feasibility shall not be considered an undue hardship.

Staff Analysis: Strict enforcement of the provisions of the Code would require redesigning the home to bring the non-conforming portion into compliance with the front setback requirement of the LR Zone. This creates an undue hardship, as that would require a significant portion of the existing home to be demolished and redesigned. The variance allows the applicant to preserve much of the livable space of the existing structure. Further, there would be constructability issues due to the existing site constraints of the public access river easement along the southern boundary and the 25 foot scenic easement along the rear.

B. The variance is necessary because of the unique size, shape, topography or location of the subject property.

Staff Analysis: The siting and configuration of the existing residence create unique challenges for redesign and redevelopment. The subject property is constrained by the public access river easement along the southern boundary and the 25' scenic easement along the rear. The subject property also has a relatively shallow depth compared to the other lots in the neighborhood. Single family lots in this neighborhood have an average depth of approximately 207.27 feet, and the subject property has an average depth of 121.62 feet. The existing residence stretches from the north to the south side setbacks which restricts the developable area of site. Also, as mentioned previously, the centerline of Parkway Drive is severely offset from the center of the right-of-way and that the garage is farther from the roadway edge of asphalt than neighboring properties that comply with the 15 foot front setback requirement.

C. The subject property is deprived, by provision of this Code, of rights and privileges enjoyed legally by other properties in the vicinity and under an identical zone.

Staff Analysis: As noted under subsection B above, the centerline of Parkway Drive is severely offset from the center of the right-of-way, so even in the garage's nonconforming condition, the garage is still further away from the roadway edge of asphalt than neighboring properties which are conforming to the 15-foot front setback. Further, out of the 16 parcels on Parkway Drive, only two parcels do not have two-vehicle garages. One of the parcels is the subject property and the other is the adjacent lot 227 Parkway Drive, both of which only have a one-vehicle garage.

This property lies in the LR Zone. Single-family residences with garages are permitted in the LR Zone. The proposed two-vehicle garage in conjunction with the existing single-family residence will comply with the minimum of two off-street parking spaces required for single-family residences as outlined in the Off Street Parking Matrix (KMC 17.125.040.B). The variance for a two-vehicle garage is in line with other properties in the vicinity which are also in the LR. However, as mentioned above, there are existing site conditions, due to the lot being previously developed and in its current condition, that other properties in the vicinity and under an identical zone are not impeded by.

D. The need for the variance is not the result of actions of the applicant or property owner.

Staff Analysis: The existing footprint and configuration of the residence are the results of the original construction and additions that occurred prior to the current property owners purchase of the property.

E. The variance does not create health and safety hazards.

Staff Analysis: The variance does not create health and safety hazards to adjacent properties or the general public. Conversely, the variance will create safer conditions for the property owners and for street maintenance operations by allowing the property owners to park two vehicles within the garage onsite and out of the right-of-way. Further, there is an existing water line access port/riser located in the driveway where the non-garaged vehicles park. By creating a condition where the vehicles would be parked onsite and out of the driveway, city departments would have unobstructed access to the water line if/when needed.

F. The variance does not relieve an applicant from any of the procedural provisions of this Code.

Staff Analysis: The applicant team met with city staff on numerous occasions where a variance was discussed and suggested as an option for a path forward. Variances can be approved provided the request is reviewed and considered following the procedures outlined in KMC 17.148.020. This application has been reviewed pursuant to those requirements. This application does not require design review and conformance with all other requirements of the code will be reviewed at the time of building permit.

G. The variance does not relieve an applicant from any standard or provision that specifically states that no variance from such standard or provision is permitted.

Staff Analysis: The KMC allows for variance to the setback requirements and there is no restriction on the consideration of the variance request for setbacks referenced in other sections of the KMC.

H. The variance does not relieve an applicant from conditions established during prior permit review.

Staff Analysis: The granting of this variance would not relieve the applicant of conditions as there were no special conditions of approval associated with the original building permit or subsequent permits for additions or remodels.

I. The variance does not allow establishment of a use that is not otherwise permitted in the zone in which the subject property is located.

Staff Analysis: As noted under subsection C above, this lot is zoned LR and the proposed two-vehicle garage in conjunction with the existing single-family residence will comply with the minimum of two off-street parking spaces required for single-family residences as outlined in the Off Street Parking Matrix (KMC 17.125.040.B).

J. The variance is the minimum necessary to grant relief to the applicant.

Staff Analysis: As outlined on Sheet G-002 in the application materials included as Attachment A, the applicant team developed several alternative studies to provide a two-vehicle garage and determined that the proposed design of the two-vehicle garage is the least impactful to the existing structure and surrounding neighborhood of options A-E they explored. While the proposed garage would increase the degree of nonconformity in regard to the front setback, it would also decrease the degree of nonconformity as it would also bring the residence into conformance with off-street parking requirements.

STAFF RECOMMENDATION

Recommendation to review and approve the 233 Parkway Variance (P25-059) for the encroachment into the front setback and building envelope subject to conditions.

RECOMMENDEND MOTIONS

The P&Z Commission may move to continue, approve, approve with conditions, or deny the application based on the information presented. If the Commission is inclined to approve the requested Variance application, staff recommend the following condition of approval to address next steps:

1. Following Planning & Zoning Commission approval of the Variance Application, the applicant shall submit a plat amendment application to modify the platted building envelope to contain the front of the residence within the envelope.

Recommended Motion Language: "I move to approve Variance Application P25-004 with conditions of approval as outlined by staff and direct staff to return with Findings of Fact, Conclusions of Law, and Decision."

ATTACHEMENTS

1. Variance Application & Supplemental Documents



City of Ketchum

Attachment 1: Variance Application & Supplemental Documents

VARIANCE EVALUATION CRITERIA

- The strict enforcement of the provisions of this Code creates an undue hardship to the property owner; however, economic feasibility shall not be considered an undue hardship.
- The variance is necessary because of the unique size, shape, topography or location of the subject property.
- The subject property is deprived, by provision of this Code, of rights and privileges enjoyed legally by other properties in the vicinity and under an identical zone.
- The need for the variance is not the result of actions of the applicant or property owner.
- The variance does not create health and safety hazards.
- The variance does not relieve an applicant from any of the procedural provisions of this Code.
- The variance does not relieve an applicant from any standard or provision that specifically states that no variance from such standard or provision is permitted.
- The variance does not relieve an applicant from conditions established during prior permit review.
- The variance does not allow establishment of a use that is not otherwise permitted in the zone in which the subject property is located.
- The variance is the minimum necessary to grant relief to the applicant.



Re: 233 Parkway Dr. Variance Application

Owner Information:

Jeff and Michelle Bruner
(805) 453-0839
mesbruner@gmail.com
PO Box 1891, Ketchum, ID 83340

Applicant Information:

Darshan Amrit, AIA
(510) 967-2159
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PO Box 6908, Ketchum, ID 83340

Property Information:

Parcel: RPK0500000013C
Legal Description: PARKWOOD SUB TL 8629

Table of Contents:

- I. Introduction
- II. Application for Variance
- III. Variance Application Criteria
- IV. Exhibit A – Variance Drawings dated 10.23.2025
- V. Exhibit B – Existing Conditions documentation

I. Introduction:

The applicants and residents, Jeff and Michelle Bruner, of 233 Parkway Drive Ketchum, Idaho have called this their home for over 30 years. Originally built in 1972, this home has had several owners and has seen many small additions by the previous owners. This home has served them and their family well, but the existing single vehicle garage has not. Over the past 7 years they have hired several local architects to develop schematic plans for a garage addition to provide them with a much needed two-vehicle garage to safely navigate daily trips into town or to the mountain during our long winters as well as to store their vehicles.

The existing 2,727 square foot, single-story, single-family structure with attached 270 square garage is considered nonconforming as the front portion of the attached garage encroaches into the front setback. Because the garage portion of the structure is nonconforming, per Ketchum Zoning Code, it cannot be rebuilt or expanded to provide garage parking for second vehicle. The remaining portion of the home, which is almost entirely conforming, stretches nearly from side setback to side setback and is close to the front yard setback which effectively restricts the only feasible garage location its current non-conforming location. After an extensive analysis and multiple studies, the proposed project that includes a variance application to demolish the non-conforming existing single vehicle garage and construction of a new two vehicle garage in its place where most of the garage structure is conforming but the front of the structure would remain as non-conforming. This approach is the minimum required to grant relief as it results in the least amount of required increase to non-conforming square footage. This approach is also consistent with the current 2025 Ketchum Comprehensive Plan where preservation, character and scale have been identified as goals and objectives. This is a context-sensitive design (Policy BNE-1.5), where more of the existing building is being preserved to allow for the new structure to meet the current and future needs of its occupants as well as keeping with the scale, context and materials of the neighborhood. In choosing to remodel and build an addition over a complete tear down, the project is also responding to the wider context of resource use responsibility, reduction of the structure's carbon footprint, and reduction of construction waste. The proposed addition and exterior scope of work is also deliberately focused on the east street facing side of the property and structure to lessen the impact of construction on edge of the western property where it faces the Big Wood River and the scenic setback. Less building and landscape construction impact in this zone is beneficial to the river edge ecosystem. With the proposed design, inclusive of the variance application and conforming addition, the project can also better manage stormwater runoff from the new proposed roof and addition, provide dark sky compliant lighting, fire resistant materials and details on the exterior to aid in fire protection for the structure and neighborhood.

II. Application for Variance:

a. Requirement(s) of the Zoning Code title 17 to be Varied:

i. 17.12.030 – Dimensional standards, districts matrix.

1. LR Front Setback: 15'
2. Side Setback: The greater of 1ft for every 2' in building height or 10'.

Variance Request: We are requesting a variance to construct a new two-vehicle garage with an encroachment into the front setback in the same location and plane as the existing nonconforming garage encroachment. Since the front setback line and existing encroachment are not parallel, for purposes of this variance request, the existing plane of the nonconforming east facing garage façade shall be maintained and establish the proposed location for the new two-vehicle garage with extensions to the north and south to allow for width to park two vehicles.

The variance request is for the garage portion of the project only. However, to provide context to the design of the new garage, a complete renovation design has been presented for reference. Other than the two-vehicle garage (proposed in the variance application) all other proposed work conforms to the dimensional standards in 17.12.030 as well as other applicable zoning code regulations. Since the variance application for the two-vehicle garage is based on the existing building conditions (including height, setbacks and lot coverage) and inclusive of the proposed garage itself, the 10' side setback shall be used for the garage. The concurrent and pending 2nd floor addition proposed partially above the garage meets all dimensional standards including the additional side setback based on the 26' roof height of the addition. The side setback based on roof height shall only apply to the 2nd floor addition portion of the proposed project and shall not force the new garage or any other portion of the existing building into nonconformance.

ii. 17.136.050 Enlargement of a nonconforming building or nonconforming use.

A nonconforming use shall not be enlarged or extended, and a nonconforming building shall not be enlarged or extended to increase the degree of nonconformity, except in the community core district where one-family dwellings may increase their original square footage by 20 percent. Additions and/or enlargements to existing buildings are not considered to be nonconforming or to increase the degree of nonconformity, so long as the additions and/or enlargements comply with the following:

- A. Any additional square footage may be subject to the current requirements of the underlying zone district at the discretion of the planning and zoning department head.
- B. Fifty percent of the building footprint and exterior walls of a nonconforming structure must remain unaltered.

Variance Request: We are requesting a variance enlarge and extend the nonconforming portion of the building to allow for a new two-vehicle garage to replace the existing nonconforming single-vehicle garage. Due to the existing dimensions of the garage (15' in width) and location on the property, we studied the possible addition of another single car garage and concluded that it would either more substantially increase the degree of nonconformance or substantially impact the existing structure and use of the structure creating further undue hardship. Out of the five options (A thru E)

studied- Option E, which proposes to demolish and replace the garage would yield the least increase of nonconformity, have the least impact to the existing structure and use of that structure, and have the least impact on the neighboring community and public. Please refer to the attached exhibit A diagrams 2/G-002 and 3/G-002.

b. Undue Hardship you believe would result from the strict enforcement of this requirement:

17.125.040 – Off Street parking and loading calculations. Residential, one-family dwellings require 2.0 parking spaces per one-family dwelling. Out of the 16 single-family residences on Parkway Drive, all but two properties have two-vehicle garage, 233 Parkway Dr and the adjacent parcel at 227 Parkway Drive. Many have three or more off street parking spaces either in a garage or in a driveway. This is not a right or privilege that is available at 233 Parkway where a vehicle cannot park in the driveway without parking in the right of way.



Survey of existing single-family homes with garage parking on Parkway Dr.

- (#) Homes that have at least two-vehicle garage (many have three to four vehicle garages)
- (#) Homes that have a single-vehicle garage

Further undue hardships are described in the variance criteria evaluated below.

c. Unique Characteristics of the site, i.e. unique size, shape, topography, or location of the property.:

The common and approved definition of Topography includes man-made or artificial conditions of the property. Meriam Webster defines topography as follows:

- i. the configuration of a surface including its relief and the position of its natural and man-made features
- ii. the physical or natural features of an object or entity and their structural relationships

The existing single-family residence is in the small Parkwood Subdivision towards the end of a dead-end Parkway Drive. It is bounded by a public access river easement to the south of the property and 25' foot scenic easement to the west bounds the property to the back. With a relatively shallow depth of the lot, as compared to the rest of the lots on the street, the existing building stretches from the South to the North side yard setbacks. Several years ago, the current owners purchased a portion of the adjacent lot to the south to allow for public access river easement to run unobstructed along their property as the corner of the existing garage would have been in the easement and potentially caused more issues with their nonconforming garage.

The existing building and siting create a severely restricted buildable area at the front of the property between the building and front setback, with not enough area to provide off-street parking spaces, let alone a two-vehicle garage.

III. Variance Evaluation Criteria:

Some of the information provided below may be applicable to multiple criteria and attempts have been made to include the information where most relevant.

- a. The strict enforcement of the provisions of this title creates an undue hardship to the property owner; however, economic feasibility shall not be considered an undue hardship;
 - i. The denial of being able to park a 2nd vehicle off-street as required by 17.125.040 and not in the public right of way due to the constraints of the existing site makes the property lesser than its neighbors that have the privilege of parking at least two vehicles safely in a garage on their own property.
 - ii. Demolition of the existing non-conforming garage to build a new two-vehicle in its place with the front of the new garage to remain as nonconforming allows the applicant to maximize use and minimize impact on the current neighborhood, street context and public view by preserving the surrounding environment with the existing mature trees and landscaping that screen the home and garage from the public river access easement and from the public right of way.
 - iii. While financial hardship shall not be considered, potentially being denied the right to legally park two-vehicles off-street or in a garage has significant financial impacts on the property's value and use, denying the owner full use of the property by right.

- iv. Compliant non-garage vehicle off-street parking for a second vehicle or guest vehicle cannot currently be accommodated. Parking space dimensional requirements per zoning section 17.125.030 have a minimum width of 9' by minimum length of 18' and are located entirely on the property, not in the public right of way. The existing maximum depth of driveway in front of the existing garage is 10'-2", as a result, any vehicles parked in the driveway are parked in the public right of way.

- b. The variance is necessary because of the unique size, shape, topography or location of the subject property;
 - i. The subject property has unique topography considering the definition of topography being the "arrangement of the natural and artificial physical features of an area. These include the following:
 - 1. The existing single-vehicle garage encroaches into the front setback.
 - 2. The existing building spans from the north to south setback lines and where it does not encroach in the front yard setback, developable area for a compliant garage is not sufficient.
 - 3. To the rear of the building, the property is further bounded by the 25' scenic easement. Any proposed addition towards the rear of the lot would detrimentally impact the existing structure and where it does not impact the structure river views would be blocked as well as detrimentally impact the river's edge and public river access environment by replacing trees and landscaping with new building structure.

- c. The subject property is deprived, by provision of this title, of rights and privileges enjoyed legally by other properties in the vicinity and under an identical zone;
 - i. Out of the 16 single family parcels on Parkway Drive, not including the lone undeveloped lot, only two parcels do not have two-vehicle garages. One of the parcels is the subject property at 233 Parkway Drive and the other is next door at 227 Parkway Drive, both with single vehicle garages. It is likely that both sites were developed prior to the adoption of zoning code requirements for off-street parking greater than a single vehicle or even the adoption of the local zoning code in the case of 227 Parkway Drive.

- d. The need for the variance is not the result of actions of the applicant or property owner;
 - i. The existing footprint, square footage and height of the structure, including the nonconforming garage, are the results of the original construction and previous remodels that occurred prior to the current owners.
 - ii. The current owners have not constructed with or without building permit(s) any portion of the building that has resulted in nonconformance.

- e. The variance does not create health and safety hazards;
 - i. The variance is requested to allow for owners to safely access their property and park two vehicles in an off-street in a garage. No health or safety hazards are created by this variance.

- ii. The existing condition of the garage encroaching into the front yard setback has been in existence since its original construction in 1972, which has resulted in the current and previous owners parking their vehicles in their driveway almost completely within the public right of way. While this has not resulted in any hazard or inconvenience for the public or City of Ketchum departments that we are aware of, the variance would allow for a significant safety improvement in the public right of way. Currently, the existing water main runs just a few feet beyond the properties eastern (street facing) property line. There is an existing water line access port/riser (see Figure 1) located in the current driveway where the non-garaged vehicles parks. Should any City departments need access to this, there is a high probability that a vehicle would be parked over it and restrict access. In the case of an emergency this could cause delays. There have been instances where the access to the water line has been required and has resulted in digging up the owner's driveway almost entirely to complete the work. While that incident may have been an inconvenience and potential safety hazard, the proposed variance application, if approved, will significantly reduce if not eliminate that inconvenience or safety risk when future work or access is required.



Figure 1 – Water line @ Driveway

- iii. The owners are proposing to provide a hydronic snow melt system, with a separate meter and zone, for the portion of their driveway located in the public right of way in addition to their on-site hardscape and driveway. This will greatly increase the safety in the public right of way as existing driveway does have a low point which often turns the entire driveway (in the public right of way) into a sheet of ice.
- f. The variance does not relieve an applicant from any of the procedural provisions of this title;
 - i. Acknowledged and understood. The variance application was discussed and suggested as an option by planning staff and the owners and applicants have met with planning staff on numerous occasions to review the proposed project and the criteria for granting a variance.

- g. The variance does not relieve an applicant from any standard or provision that specifically states that no variance from such standard or provision is permitted;
 - i. Acknowledged and understood. The owners and applicant are not aware of any specific standard or provision in their application that could not be granted with a variance.
 - ii. The variance application was suggested as an option by planning staff and the owners and applicants have met with planning staff on numerous occasions to review the proposed project and the criteria for granting a variance.

- h. The variance does not relieve an applicant from conditions established during prior permit review;
 - i. Acknowledged and understood. The owners and applicant are not aware of any prior conditions that may have been established.
 - ii. The owners and applicants have met numerous times with planning staff and would hope that any prior conditions would have been brought to their attention.

- i. The variance does not allow establishment of a use that is not otherwise permitted in the zone in which the subject property is located; and
 - i. The garage and use as a garage for the portion of the nonconforming structure is existing. The nonconforming use as a garage would continue to remain with an approved variance application.

- j. The variance is the minimum necessary to grant relief to the applicant.
 - i. Yes. The applicant has developed several alternate studies to provide the owners with a two-vehicle garage solution and has concluded, including review and input by planning staff, that the approach included in this application is the least impactful to the existing structure, the surrounding environment, the neighborhood and the wider community. Please refer to diagram study on sheet G-002 in the attached exhibit.
 - ii. The proposed variance is for the two-vehicle garage only. The 24ft wide by 32ft deep proposed garage is a standard two-vehicle garage size and the minimum in dimension and use needed to provide relief. The existing single vehicle garage encroachment is a total of 116 square feet with the existing southeast corner encroaching 5'-3" and the northeast corner encroaching 9'-2" into the front setback. The portion of the new garage that would be nonconforming would total 175 square feet with the southeast corner encroaching 3'-8" and the northeast corner encroaching 10'-2" into the front setback.
 - iii. With the presence of an existing single vehicle garage, the provision of a second vehicle garage space (separate or attached) would require a minimum of 288 SF (12'x24'). It is not possible to provide a second vehicle garage fully conforming to development standards of 17.12.030 in the LR Zoning District based on the existing site constraints without detrimental impact and creating undue hardship as outlined with the existing site and analysis diagrams provided on sheet G-002. The analysis and various studies concluded that with the existing garage structure encroaching into the front setback, the minimum necessary to grant relief and result in the least impact to the site and neighborhood would

be the demolition of the entire existing attached garage to build a new two vehicle garage in the same location, illustrated as Option E on sheet G-002. This option proposes that the existing non-conforming condition is marginally increased by 59 SF resulting in a total of 175 SF of non-conforming building area. This is substantially less than 288 SF required for a second vehicle garage and not accounting for the existing 116 SF of non-conforming garage that would be remaining are the justifications that Option E should be considered as the minimum SF needed to grant relief.

- iv. Concurrent with the variance application and conditional on the approval of the variance application, the applicant has provided for reference a complete design that incorporates a new partial 2nd floor addition above the garage, a new entry and small guest bedroom addition that are all conforming with the applicable development standards 17.12.030 in the LR Zoning District. The side setback (the greater of 1' for every 2' in building height or 10') for the 2nd floor addition is 13' based on a proposed 26' roof height. The increased side setback based on the 2nd floor addition shall not create further nonconformance with the new garage as proposed in the variance. The side setback for the garage in the variance application is 10' as the garage is less than 20' in height. Also as part of this application the applicant is requesting review and approval of a minor side yard encroachment for the proposed elevated deck at the new garage roof. The less than 1'-6" proposed deck encroachment can be allowed with Design Review Approval per 17.128.020.I. The deck encroachment would provide an overhang to allow for better drainage from the deck with a snow melt system which would protect the new garage structure below from water intrusion due to ice buildup.
- v. The design in its entirety has been presented in this application to illustrate how the variance portion of the design will be integrated with the existing structure and pending additions at the 2nd level, entry and north guest bedroom wing.

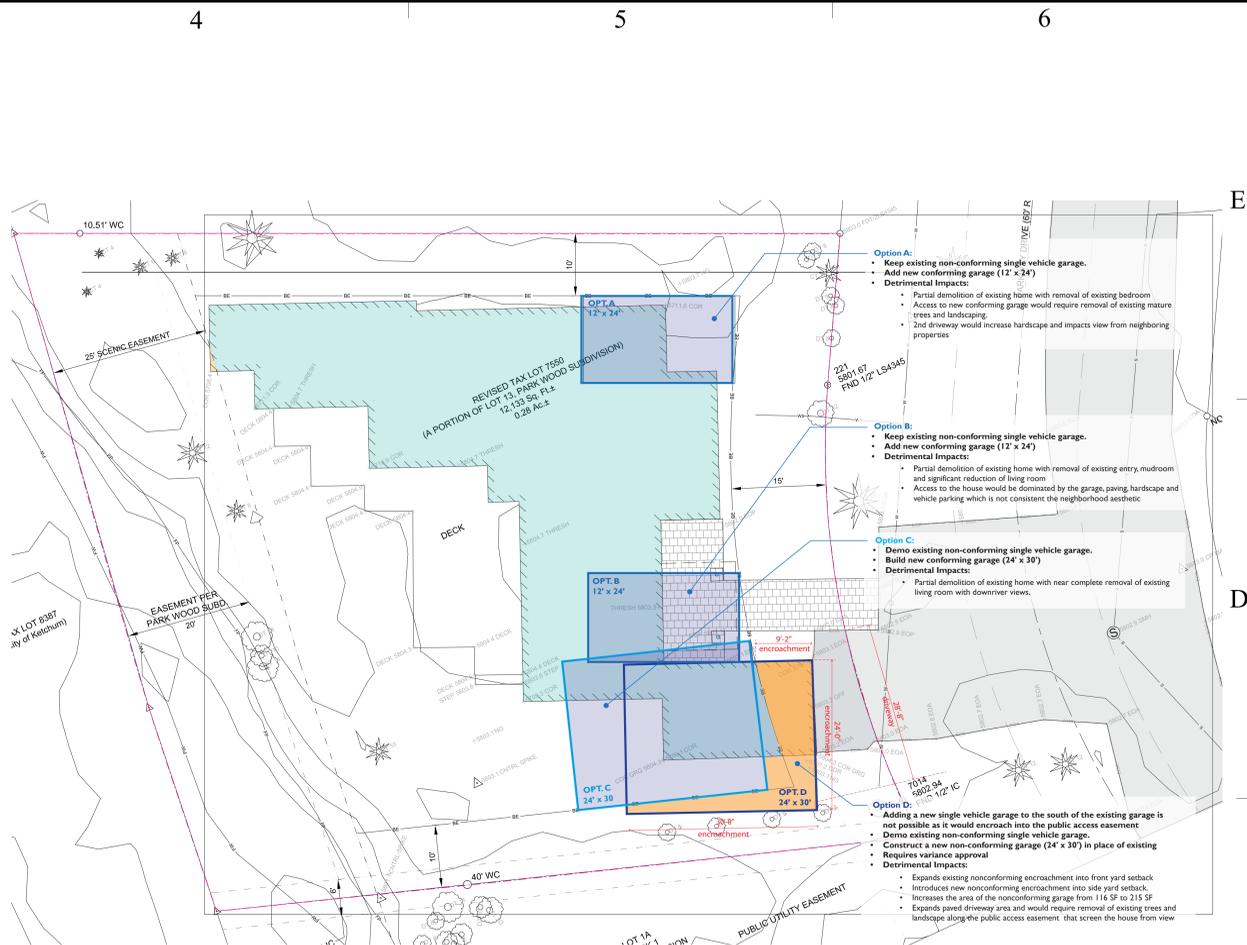
We look forward to any questions or comments that you may have about the requested variance or the proposed design.

Warm regards,

Darshan Amrit, AIA
A2 Studio
131 6th St
Ketchum, ID 83340

darshanamrit@me.com
510.967.2159

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A2 Studio, LLC
 A2 studio
 architecture design
 131 6TH. W, Ketchum, Idaho, 83340

CONSULTANTS
 Jeff & Michelle Bruner
 233 Parkway Drive, Ketchum, Idaho, 83340

Bruner Residence
 Garage Addition

TAX LOT 8629 Section 13, T.4N., R.17 E., B.M. Ketchum, Blaine County, Idaho

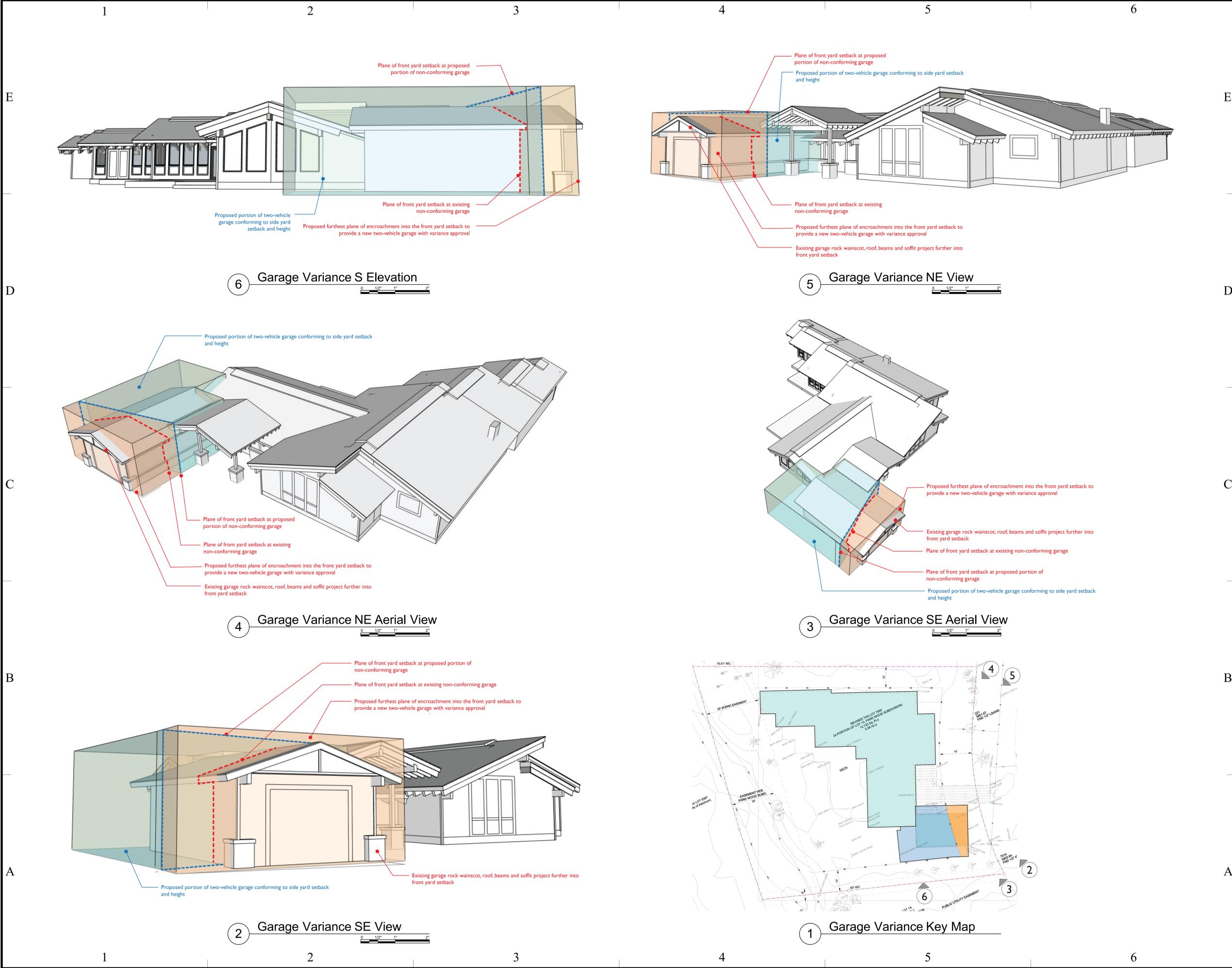
LICENSED ARCHITECT
 AR 987650
 DARSHAN AMRIT
 STATE OF IDAHO

MARK	DATE	DESCRIPTION
01	10/23/25	Variance Application Set

PROJECT NO: 2402
 MODEL FILE:
 251023_233-Parkway-Variance-Application.pin
 DRAWN BY: Darshan A. / Dan M.
 COPYRIGHT:

SHEET TITLE
 Variance Site Diagrams

G-002



6 Garage Variance S Elevation

5 Garage Variance NE View

4 Garage Variance NE Aerial View

3 Garage Variance SE Aerial View

2 Garage Variance SE View

1 Garage Variance Key Map

01	10/23/25	Variance Application Set
MARK	DATE	DESCRIPTION

PROJECT NO: 2402
 MODEL FILE:
 251023_233-Parkway-Variance-Application.pln
 DRAWN BY: Darshan A. / Dan M.
 COPYRIGHT:

SHEET TITLE
 Variance Massing Diagrams

G-003

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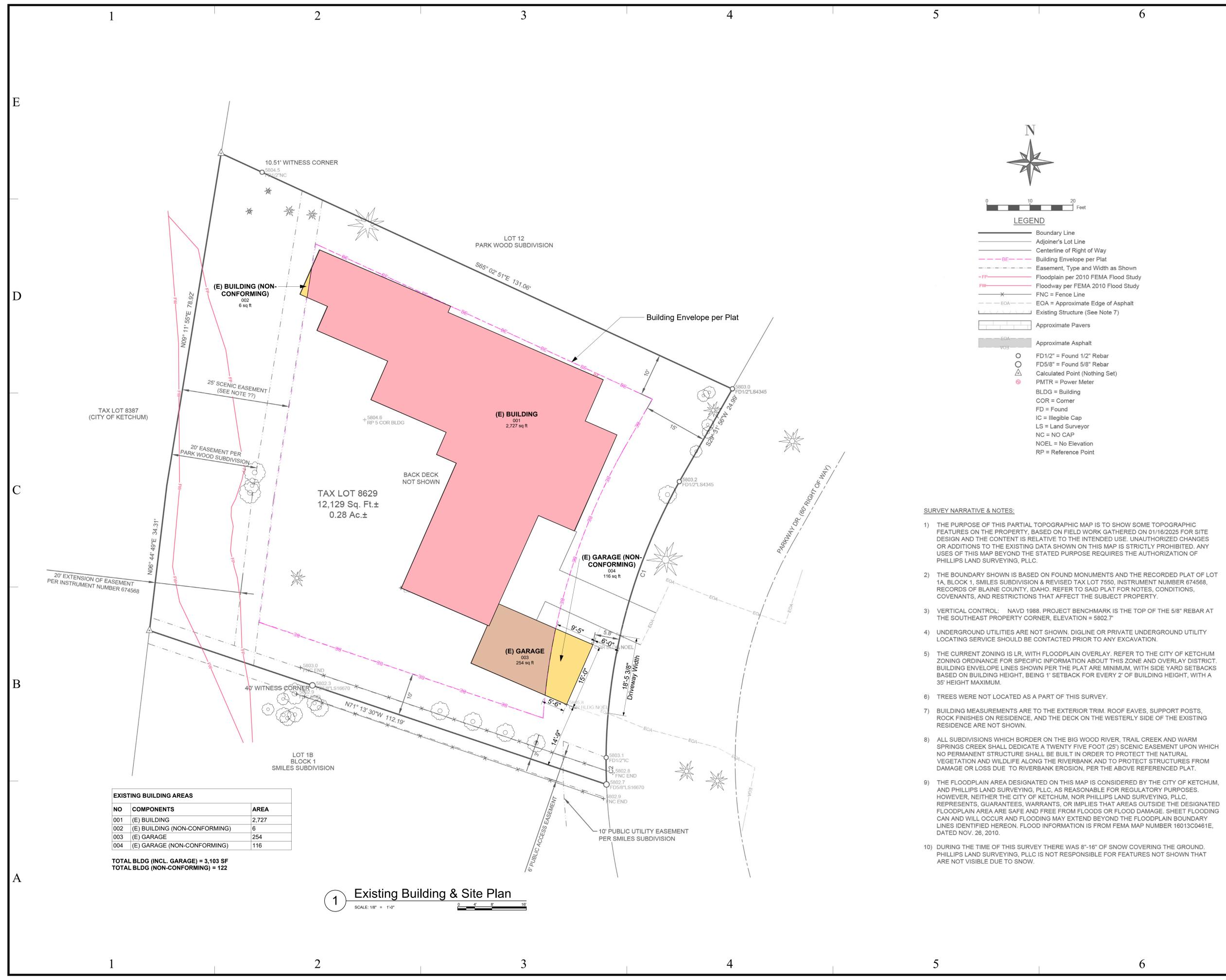


LEGEND

- Boundary Line
- Adjoiner's Lot Line
- Centerline of Right of Way
- Building Envelope per Plat
- Easement, Type and Width as Shown
- Floodplain per 2010 FEMA Flood Study
- Floodway per FEMA 2010 Flood Study
- FNC = Fence Line
- EOA = Approximate Edge of Asphalt
- Existing Structure (See Note 7)
- Approximate Pavers
- Approximate Asphalt
- FD1/2" = Found 1/2" Rebar
- FD5/8" = Found 5/8" Rebar
- Calculated Point (Nothing Set)
- PMTR = Power Meter
- BLDG = Building
- COR = Corner
- FD = Found
- IC = Illegible Cap
- LS = Land Surveyor
- NC = NO CAP
- NOEL = No Elevation
- RP = Reference Point

SURVEY NARRATIVE & NOTES:

- 1) THE PURPOSE OF THIS PARTIAL TOPOGRAPHIC MAP IS TO SHOW SOME TOPOGRAPHIC FEATURES ON THE PROPERTY, BASED ON FIELD WORK GATHERED ON 01/16/2025 FOR SITE DESIGN AND THE CONTENT IS RELATIVE TO THE INTENDED USE. UNAUTHORIZED CHANGES OR ADDITIONS TO THE EXISTING DATA SHOWN ON THIS MAP IS STRICTLY PROHIBITED. ANY USES OF THIS MAP BEYOND THE STATED PURPOSE REQUIRES THE AUTHORIZATION OF PHILLIPS LAND SURVEYING, PLLC.
- 2) THE BOUNDARY SHOWN IS BASED ON FOUND MONUMENTS AND THE RECORDED PLAT OF LOT 1A, BLOCK 1, SMILES SUBDIVISION & REVISED TAX LOT 7550, INSTRUMENT NUMBER 674568, RECORDS OF BLAINE COUNTY, IDAHO. REFER TO SAID PLAT FOR NOTES, CONDITIONS, COVENANTS, AND RESTRICTIONS THAT AFFECT THE SUBJECT PROPERTY.
- 3) VERTICAL CONTROL: NAVD 1988. PROJECT BENCHMARK IS THE TOP OF THE 5/8" REBAR AT THE SOUTHEAST PROPERTY CORNER, ELEVATION = 5802.7'
- 4) UNDERGROUND UTILITIES ARE NOT SHOWN. DIGLINE OR PRIVATE UNDERGROUND UTILITY LOCATING SERVICE SHOULD BE CONTACTED PRIOR TO ANY EXCAVATION.
- 5) THE CURRENT ZONING IS LR, WITH FLOODPLAIN OVERLAY. REFER TO THE CITY OF KETCHUM ZONING ORDINANCE FOR SPECIFIC INFORMATION ABOUT THIS ZONE AND OVERLAY DISTRICT. BUILDING ENVELOPE LINES SHOWN PER THE PLAT ARE MINIMUM, WITH SIDE YARD SETBACKS BASED ON BUILDING HEIGHT, BEING 1' SETBACK FOR EVERY 2' OF BUILDING HEIGHT, WITH A 35' HEIGHT MAXIMUM.
- 6) TREES WERE NOT LOCATED AS A PART OF THIS SURVEY.
- 7) BUILDING MEASUREMENTS ARE TO THE EXTERIOR TRIM. ROOF EAVES, SUPPORT POSTS, ROCK FINISHES ON RESIDENCE, AND THE DECK ON THE WESTERLY SIDE OF THE EXISTING RESIDENCE ARE NOT SHOWN.
- 8) ALL SUBDIVISIONS WHICH BORDER ON THE BIG WOOD RIVER, TRAIL CREEK AND WARM SPRINGS CREEK SHALL DEDICATE A TWENTY FIVE FOOT (25') SCENIC EASEMENT UPON WHICH NO PERMANENT STRUCTURE SHALL BE BUILT IN ORDER TO PROTECT THE NATURAL VEGETATION AND WILDLIFE ALONG THE RIVERBANK AND TO PROTECT STRUCTURES FROM DAMAGE OR LOSS DUE TO RIVERBANK EROSION, PER THE ABOVE REFERENCED PLAT.
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- 10) DURING THE TIME OF THIS SURVEY THERE WAS 8"-16" OF SNOW COVERING THE GROUND. PHILLIPS LAND SURVEYING, PLLC IS NOT RESPONSIBLE FOR FEATURES NOT SHOWN THAT ARE NOT VISIBLE DUE TO SNOW.



EXISTING BUILDING AREAS		
NO	COMPONENTS	AREA
001	(E) BUILDING	2,727
002	(E) BUILDING (NON-CONFORMING)	6
003	(E) GARAGE	254
004	(E) GARAGE (NON-CONFORMING)	116

TOTAL BLDG (INCL. GARAGE) = 3,103 SF
 TOTAL BLDG (NON-CONFORMING) = 122

1 Existing Building & Site Plan
 SCALE: 1/8" = 1'-0"

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SHEET TITLE
 Existing Building & Site Plan

AS-001

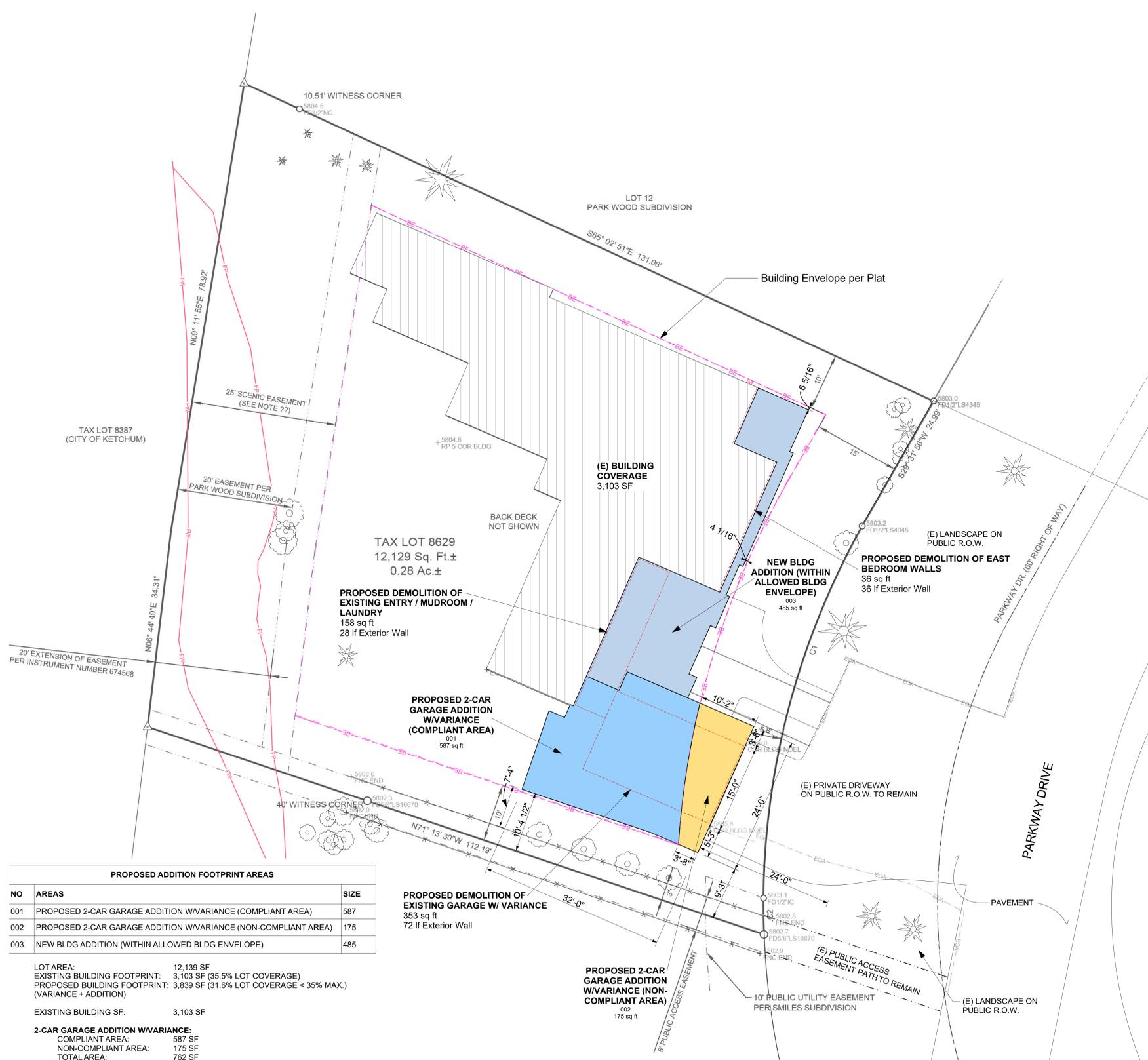


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PROPOSED ADDITION FOOTPRINT AREAS		
NO	AREAS	SIZE
001	PROPOSED 2-CAR GARAGE ADDITION W/VARIANCE (COMPLIANT AREA)	587
002	PROPOSED 2-CAR GARAGE ADDITION W/VARIANCE (NON-COMPLIANT AREA)	175
003	NEW BLDG ADDITION (WITHIN ALLOWED BLDG ENVELOPE)	485

LOT AREA: 12,139 SF
 EXISTING BUILDING FOOTPRINT: 3,103 SF (35.5% LOT COVERAGE)
 PROPOSED BUILDING FOOTPRINT: 3,839 SF (31.6% LOT COVERAGE < 35% MAX.)
 (VARIANCE + ADDITION)

EXISTING BUILDING SF: 3,103 SF

2-CAR GARAGE ADDITION W/VARIANCE:
 COMPLIANT AREA: 587 SF
 NON-COMPLIANT AREA: 175 SF
 TOTAL AREA: 762 SF

ADDITION (SEPARATE OF VARIANCE):
 1ST FLOOR AREA: 485 SF
 2ND FLOOR AREA: 780 SF
 TOTAL AREA: 1,265 SF

PROPOSED BUILDING SF: 4,619 SF

PROPOSED BUILDING DEMO (SF): 547 SF (18% < 50% MAX.)
 EXISTING BUILDING (LF): 359 LF
 PROPOSED BUILDING DEMO (LF): 136 LF (38% < 50% MAX.)

PROPOSED DEMOLITION OF EXISTING GARAGE W/ VARIANCE		
NO	AREAS	SIZE
001	PROPOSED 2-CAR GARAGE ADDITION W/VARIANCE (COMPLIANT AREA)	587
002	PROPOSED 2-CAR GARAGE ADDITION W/VARIANCE (NON-COMPLIANT AREA)	175
003	NEW BLDG ADDITION (WITHIN ALLOWED BLDG ENVELOPE)	485

1 Proposed Building & Site Plan
 SCALE: 1/8" = 1'-0"

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SHEET TITLE
Proposed Building & Site Plan

AS-002

MARK	DATE	DESCRIPTION
01	10/23/25	Variance Application Set

PROJECT NO: 2402
 MODEL FILE:
 251023_233-Parkway-Variance-Application.pln
 DRAWN BY: Darshan A. / Dan M.
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SHEET TITLE
 Existing Public R.O.W. Areas

AS-003



LEGEND

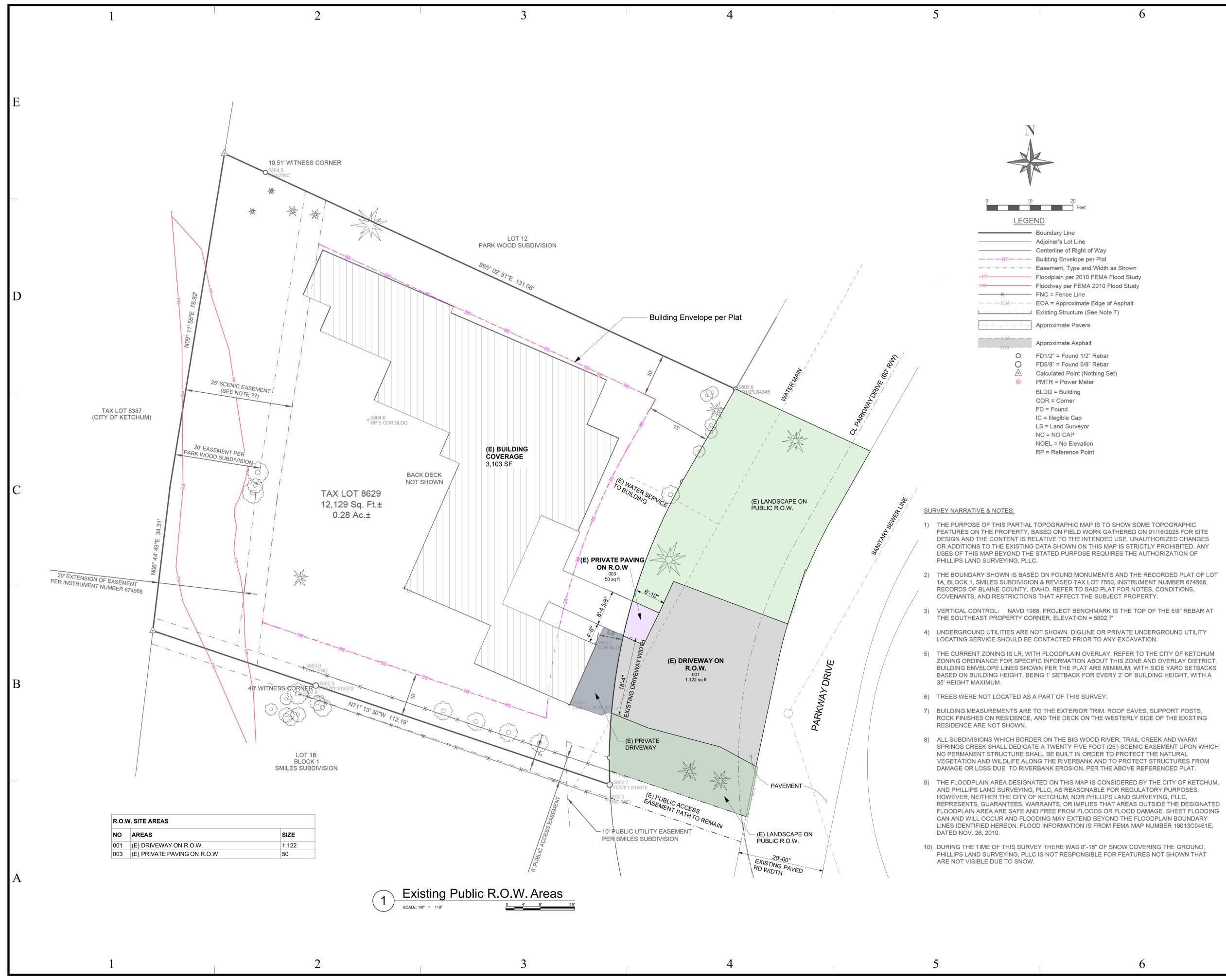
- Boundary Line
- Adjoiner's Lot Line
- Centerline of Right of Way
- Building Envelope per Plat
- Easement, Type and Width as Shown
- Floodplain per 2010 FEMA Flood Study
- Floodway per FEMA 2010 Flood Study
- FNC = Fence Line
- EOA = Approximate Edge of Asphalt
- Existing Structure (See Note 7)
- Approximate Pavers
- Approximate Asphalt
- FD1/2" = Found 1/2" Rebar
- FD5/8" = Found 5/8" Rebar
- Calculated Point (Nothing Set)
- PMTR = Power Meter
- BLDG = Building
- COR = Corner
- FD = Found
- IC = Illegible Cap
- LS = Land Surveyor
- NC = NO CAP
- NOEL = No Elevation
- RP = Reference Point

SURVEY NARRATIVE & NOTES:

- 1) THE PURPOSE OF THIS PARTIAL TOPOGRAPHIC MAP IS TO SHOW SOME TOPOGRAPHIC FEATURES ON THE PROPERTY, BASED ON FIELD WORK GATHERED ON 01/16/2025 FOR SITE DESIGN AND THE CONTENT IS RELATIVE TO THE INTENDED USE. UNAUTHORIZED CHANGES OR ADDITIONS TO THE EXISTING DATA SHOWN ON THIS MAP IS STRICTLY PROHIBITED, ANY USES OF THIS MAP BEYOND THE STATED PURPOSE REQUIRES THE AUTHORIZATION OF PHILLIPS LAND SURVEYING, PLLC.
- 2) THE BOUNDARY SHOWN IS BASED ON FOUND MONUMENTS AND THE RECORDED PLAT OF LOT 1A, BLOCK 1, SMILES SUBDIVISION & REVISED TAX LOT 7550, INSTRUMENT NUMBER 674568, RECORDS OF BLAINE COUNTY, IDAHO. REFER TO SAID PLAT FOR NOTES, CONDITIONS, COVENANTS, AND RESTRICTIONS THAT AFFECT THE SUBJECT PROPERTY.
- 3) VERTICAL CONTROL: NAVD 1988. PROJECT BENCHMARK IS THE TOP OF THE 5/8" REBAR AT THE SOUTHEAST PROPERTY CORNER, ELEVATION = 5802.7'
- 4) UNDERGROUND UTILITIES ARE NOT SHOWN. DIGLINE OR PRIVATE UNDERGROUND UTILITY LOCATING SERVICE SHOULD BE CONTACTED PRIOR TO ANY EXCAVATION.
- 5) THE CURRENT ZONING IS LR, WITH FLOODPLAIN OVERLAY. REFER TO THE CITY OF KETCHUM ZONING ORDINANCE FOR SPECIFIC INFORMATION ABOUT THIS ZONE AND OVERLAY DISTRICT. BUILDING ENVELOPE LINES SHOWN PER THE PLAT ARE MINIMUM, WITH SIDE YARD SETBACKS BASED ON BUILDING HEIGHT, BEING 1' SETBACK FOR EVERY 2' OF BUILDING HEIGHT, WITH A 35' HEIGHT MAXIMUM.
- 6) TREES WERE NOT LOCATED AS A PART OF THIS SURVEY.
- 7) BUILDING MEASUREMENTS ARE TO THE EXTERIOR TRIM, ROOF EAVES, SUPPORT POSTS, ROCK FINISHES ON RESIDENCE, AND THE DECK ON THE WESTERLY SIDE OF THE EXISTING RESIDENCE ARE NOT SHOWN.
- 8) ALL SUBDIVISIONS WHICH BORDER ON THE BIG WOOD RIVER, TRAIL CREEK AND WARM SPRINGS CREEK SHALL DEDICATE A TWENTY FIVE FOOT (25') SCENIC EASEMENT UPON WHICH NO PERMANENT STRUCTURE SHALL BE BUILT IN ORDER TO PROTECT THE NATURAL VEGETATION AND WILDLIFE ALONG THE RIVERBANK AND TO PROTECT STRUCTURES FROM DAMAGE OR LOSS DUE TO RIVERBANK EROSION, PER THE ABOVE REFERENCED PLAT.
- 9) THE FLOODPLAIN AREA DESIGNATED ON THIS MAP IS CONSIDERED BY THE CITY OF KETCHUM, AND PHILLIPS LAND SURVEYING, PLLC, AS REASONABLE FOR REGULATORY PURPOSES. HOWEVER, NEITHER THE CITY OF KETCHUM, NOR PHILLIPS LAND SURVEYING, PLLC, REPRESENTS, GUARANTEES, WARRANTS, OR IMPLIES THAT AREAS OUTSIDE THE DESIGNATED FLOODPLAIN AREA ARE SAFE AND FREE FROM FLOODS OR FLOOD DAMAGE. SHEET FLOODING CAN AND WILL OCCUR AND FLOODING MAY EXTEND BEYOND THE FLOODPLAIN BOUNDARY LINES IDENTIFIED HEREON. FLOOD INFORMATION IS FROM FEMA MAP NUMBER 16013C0461E, DATED NOV. 26, 2010.
- 10) DURING THE TIME OF THIS SURVEY THERE WAS 8"-16" OF SNOW COVERING THE GROUND. PHILLIPS LAND SURVEYING, PLLC IS NOT RESPONSIBLE FOR FEATURES NOT SHOWN THAT ARE NOT VISIBLE DUE TO SNOW.

R.O.W. SITE AREAS		
NO	AREAS	SIZE
001	(E) DRIVEWAY ON R.O.W.	1,122
003	(E) PRIVATE PAVING ON R.O.W.	50

1 Existing Public R.O.W. Areas
 SCALE: 1/8" = 1'-0"



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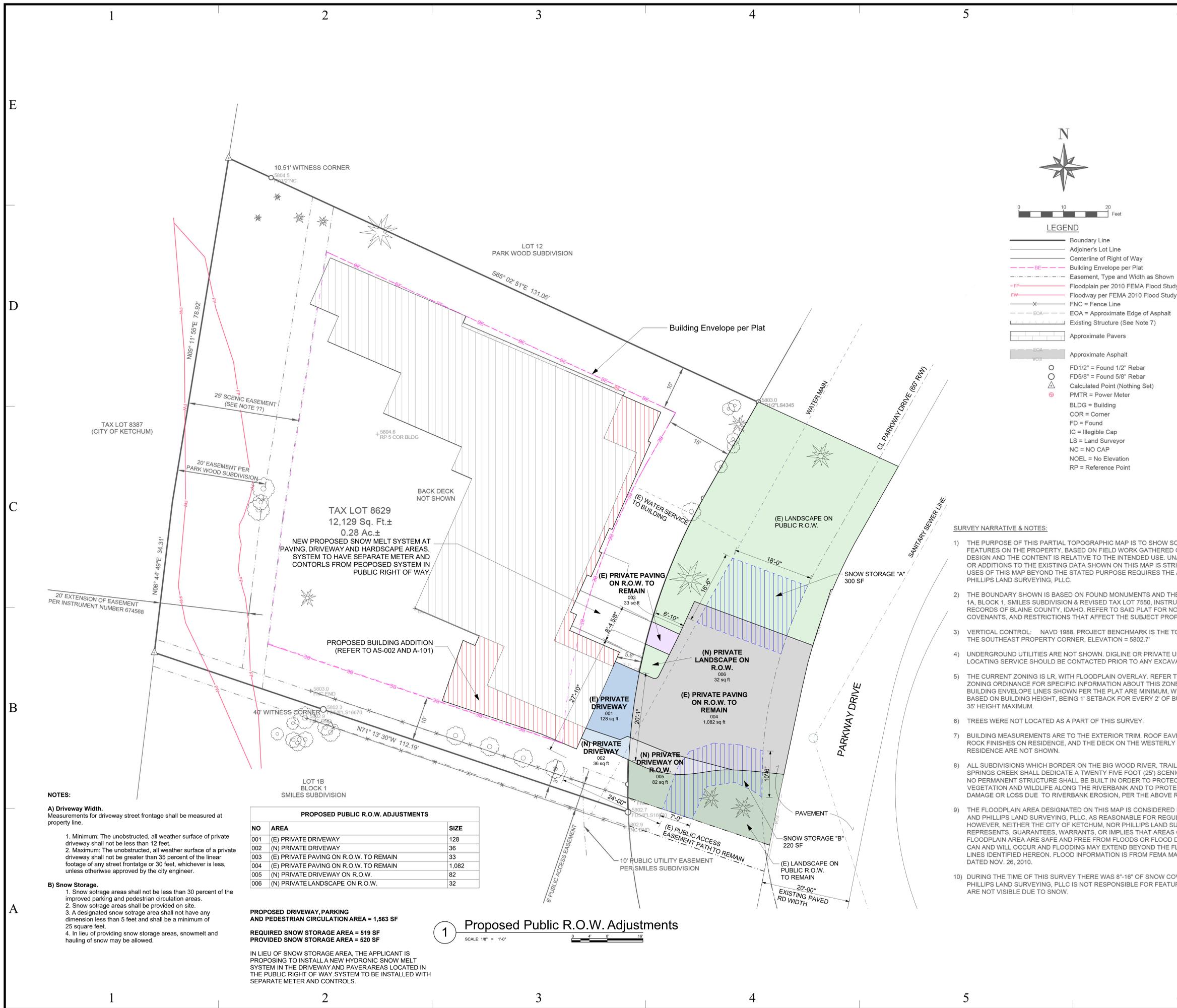
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MARK	DATE	DESCRIPTION

PROJECT NO: 2402
 MODEL FILE:
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SHEET TITLE

Proposed Public R.O.W. Adjustments

AS-004



SURVEY NARRATIVE & NOTES:

- 1) THE PURPOSE OF THIS PARTIAL TOPOGRAPHIC MAP IS TO SHOW SOME TOPOGRAPHIC FEATURES ON THE PROPERTY, BASED ON FIELD WORK GATHERED ON 01/16/2025 FOR SITE DESIGN AND THE CONTENT IS RELATIVE TO THE INTENDED USE. UNAUTHORIZED CHANGES OR ADDITIONS TO THE EXISTING DATA SHOWN ON THIS MAP IS STRICTLY PROHIBITED, ANY USES OF THIS MAP BEYOND THE STATED PURPOSE REQUIRES THE AUTHORIZATION OF PHILLIPS LAND SURVEYING, PLLC.
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NOTES:

A) Driveway Width.
 Measurements for driveway street frontage shall be measured at property line.

1. Minimum: The unobstructed, all weather surface of private driveway shall not be less than 12 feet.
2. Maximum: The unobstructed, all weather surface of a private driveway shall not be greater than 35 percent of the linear footage of any street frontage or 30 feet, whichever is less, unless otherwise approved by the city engineer.

B) Snow Storage.

1. Snow storage areas shall not be less than 30 percent of the improved parking and pedestrian circulation areas.
2. Snow storage areas shall be provided on site.
3. A designated snow storage area shall not have any dimension less than 5 feet and shall be a minimum of 25 square feet.
4. In lieu of providing snow storage areas, snowmelt and hauling of snow may be allowed.

PROPOSED PUBLIC R.O.W. ADJUSTMENTS		
NO	AREA	SIZE
001	(E) PRIVATE DRIVEWAY	128
002	(N) PRIVATE DRIVEWAY	36
003	(E) PRIVATE PAVING ON R.O.W. TO REMAIN	33
004	(E) PRIVATE PAVING ON R.O.W. TO REMAIN	1,082
005	(N) PRIVATE DRIVEWAY ON R.O.W.	82
006	(N) PRIVATE LANDSCAPE ON R.O.W.	32

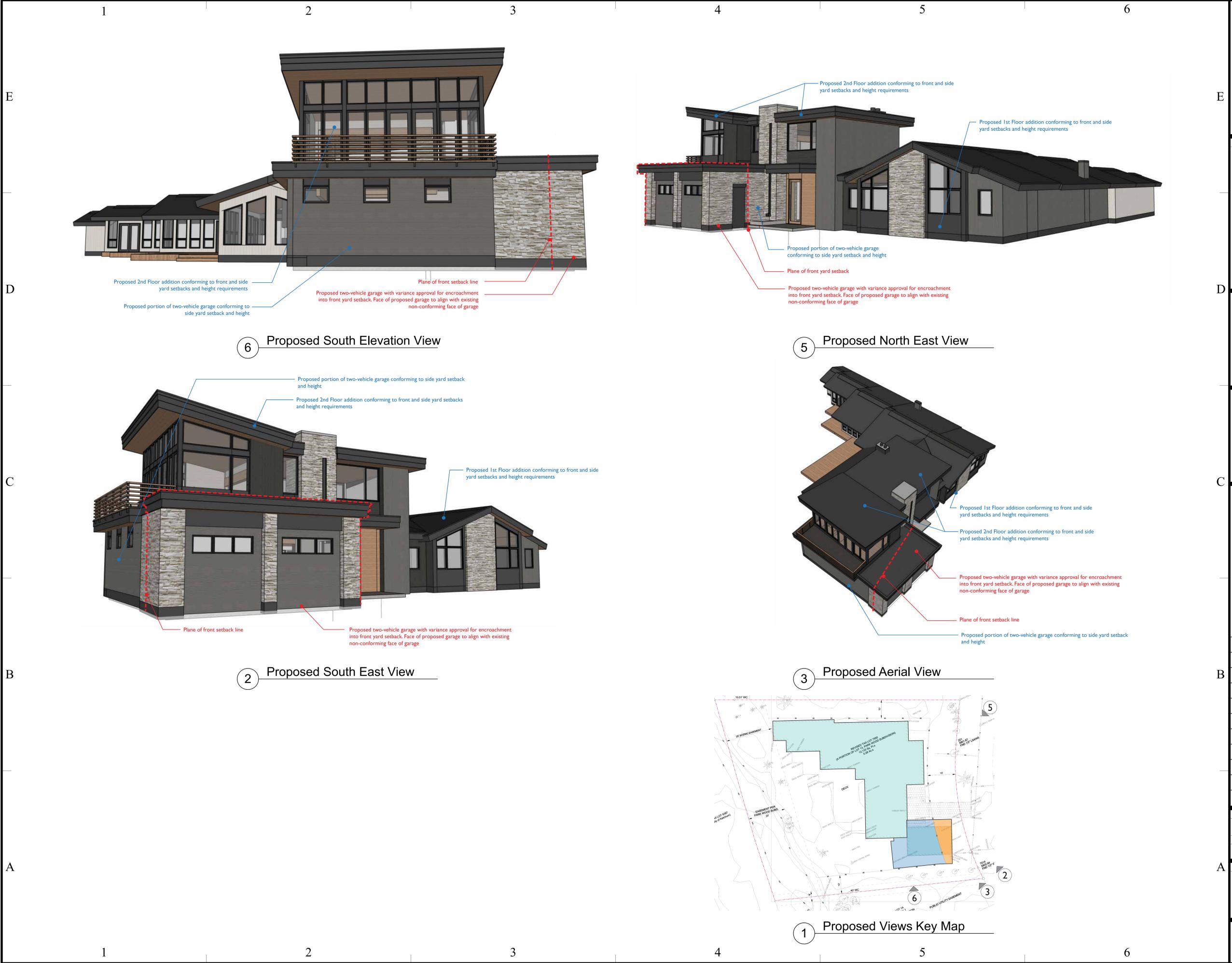
PROPOSED DRIVEWAY, PARKING AND PEDESTRIAN CIRCULATION AREA = 1,563 SF
REQUIRED SNOW STORAGE AREA = 519 SF
PROVIDED SNOW STORAGE AREA = 520 SF

IN LIEU OF SNOW STORAGE AREA, THE APPLICANT IS PROPOSING TO INSTALL A NEW HYDRONIC SNOW MELT SYSTEM IN THE DRIVEWAY AND PAVER AREAS LOCATED IN THE PUBLIC RIGHT OF WAY. SYSTEM TO BE INSTALLED WITH SEPARATE METER AND CONTROLS.

1 Proposed Public R.O.W. Adjustments
 SCALE: 1/8" = 1'-0"



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A2 Studio, LLC

A2 studio architecture design

131 6TH. W, Ketchum, Idaho, 83340

CONSULTANTS

Jeff & Michelle Bruner

233 Parkway Drive, Ketchum, Idaho, 83340

Bruner Residence Garage Addition

TAX LOT 8629 Section 13, T.4N., R.17 E., B.M. Ketchum, Blaine County, Idaho

LICENSED ARCHITECT AR 987650

DARSHAN AMRIT STATE OF IDAHO

MARK	DATE	DESCRIPTION
01	10/23/25	Variance Application Set

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SHEET TITLE
Proposed Addition Views

AS-005

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1 First Floor
SCALE: 1/8" = 1'-0"

2 Second Floor
SCALE: 1/8" = 1'-0"

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83340

CONSULTANTS

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Idaho, 83340

**Bruner Residence
Garage Addition**

TAX LOT 8629 Section 13, T.4N.,
R.17 E., B.M. Ketchum, Blaine
County, Idaho

LICENSED
ARCHITECT
AR 987650

[Signature]
DARSHAN AMRIT
STATE OF IDAHO

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Floor Plans

A-101

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CONSULTANTS

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 233 Parkway Drive, Ketchum,
 Idaho, 83340

**Bruner Residence
 Garage Addition**
 TAX LOT 8629 Section 13, T.4N.,
 R.17 E., B.M. Ketchum, Blaine
 County, Idaho

LICENSED
 ARCHITECT
 AR 987650

 DARSHAN AMRIT
 STATE OF IDAHO

MARK	DATE	DESCRIPTION
01	10/23/25	Variance Application Set

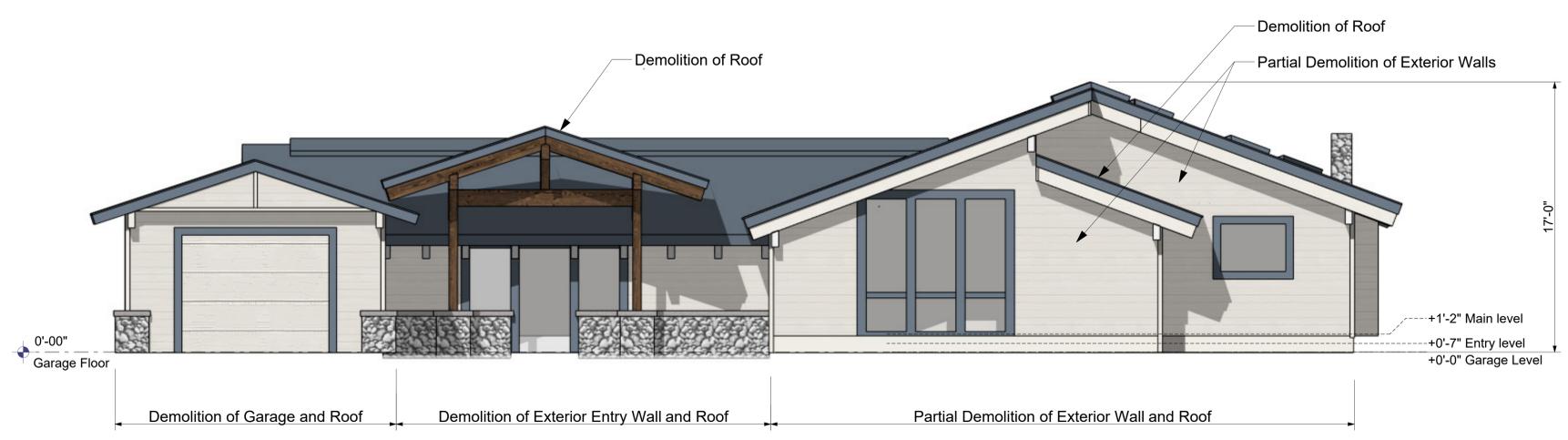
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SHEET TITLE
East Elevation

A-201



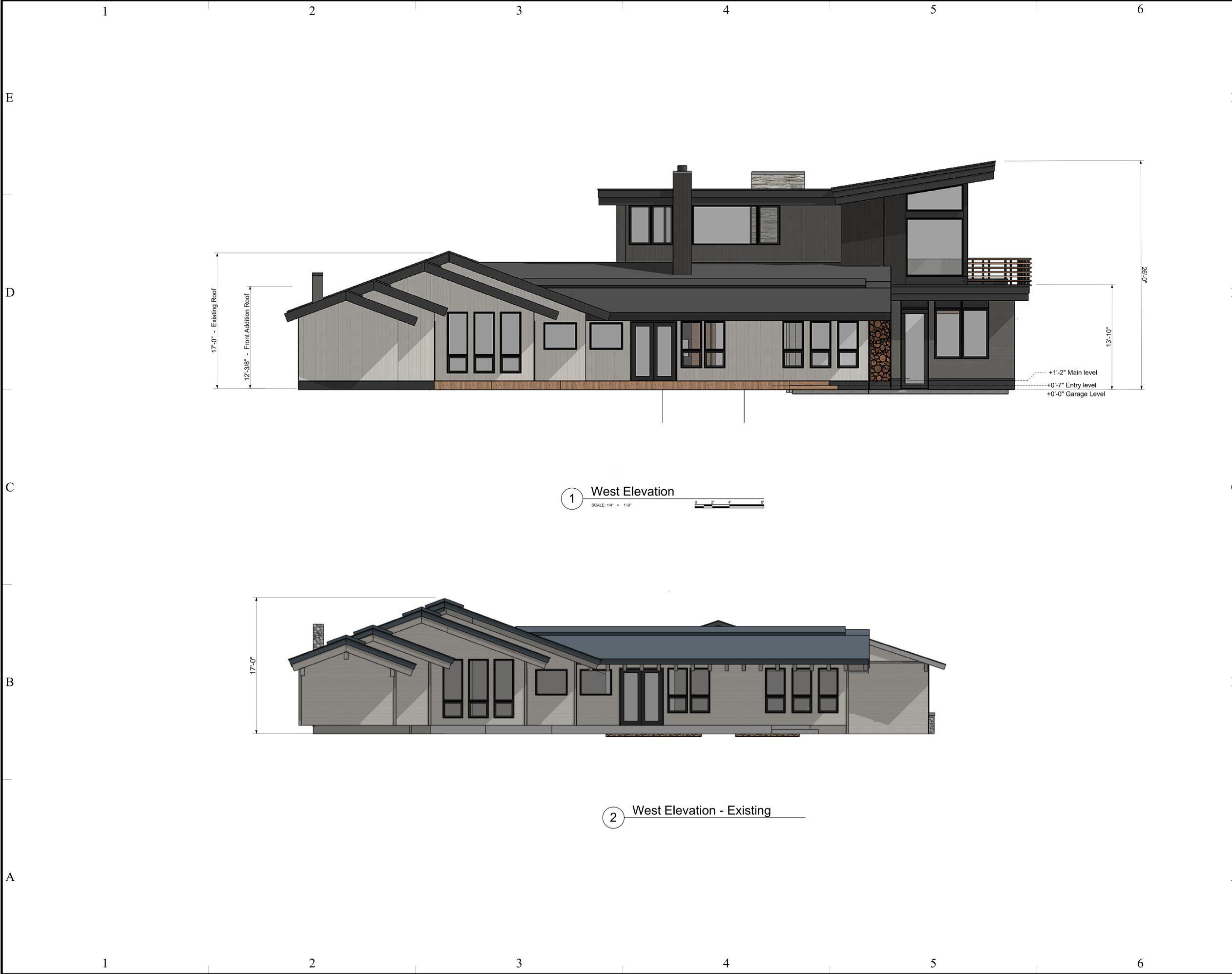
1 East Elevation
 SCALE: 1/4" = 1'-0"

2 East Elevation - Existing

Grid lines 1 through 6 are marked along the top and bottom edges. Grid lines E through A are marked along the left and right edges.

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1 West Elevation
SCALE: 1/4" = 1'-0"

2 West Elevation - Existing

A2 Studio, LLC
 A2 studio
 architecture design
 131 6TH. W, Ketchum, Idaho, 83340

CONSULTANTS

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 233 Parkway Drive, Ketchum, Idaho, 83340

Bruner Residence
 Garage Addition
 TAX LOT 8629 Section 13, T.4N., R.17 E., B.M. Ketchum, Blaine County, Idaho

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 STATE OF IDAHO

MARK	DATE	DESCRIPTION
01	10/23/25	Variance Application Set

PROJECT NO: 2402
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SHEET TITLE
 West Elevation

A-203

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A2 Studio, LLC

A2 studio
architecture
design

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83340

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Idaho, 83340

Bruner Residence
Garage Addition

TAX LOT 8629 Section 13, T.4N.,
R.17 E., B.M. Ketchum, Blaine
County, Idaho

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ARCHITECT
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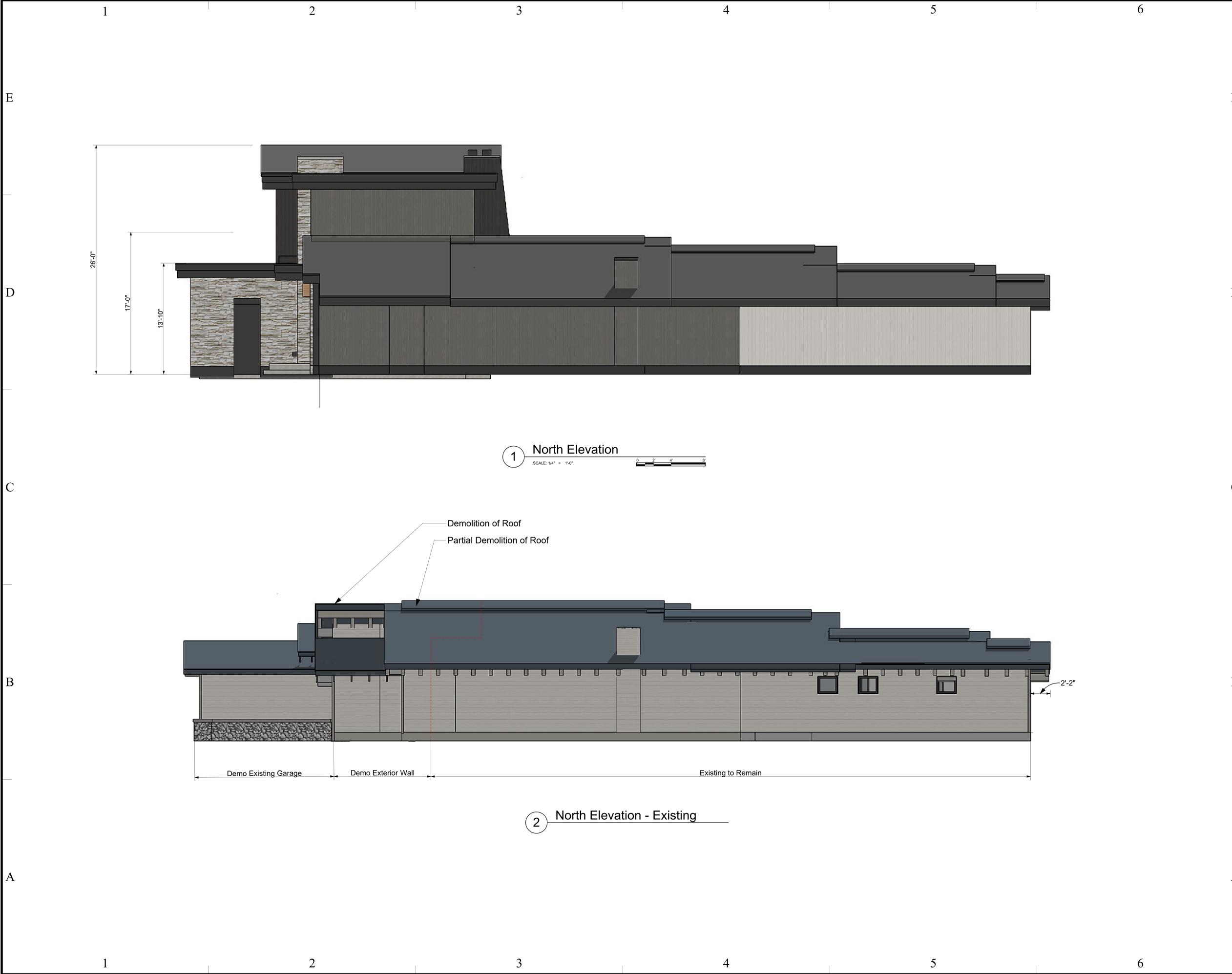
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MARK	DATE	DESCRIPTION
01	10/23/25	Variance Application Set

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SHEET TITLE
North Elevation

A-204



1 North Elevation
SCALE: 1/4" = 1'-0"

2 North Elevation - Existing

1

2

3

4

5

6

E

E

D

D

C

C

B

B

A

A

A2 Studio, LLC

A2
studio
architecture
design

131 6TH. W, Ketchum, Idaho,
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R.17 E., B.M. Ketchum, Blaine
County, Idaho

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AR 987650

DARSHAN AMRIT
STATE OF IDAHO



1 View 1 - Approach
SCALE: 1:2.97



2 View 2 - Entry
SCALE: 1:2.95

MARK	DATE	DESCRIPTION
01	10/23/25	Variance Application Set

PROJECT NO: 2402
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SHEET TITLE
3D Views

A-901

1

2

3

4

5

6

1 2 3 4 5 6

E

E

D

D

C

C



② View 4 - SE Corner
SCALE: 1:3.07



① View 3 - East Elevation
SCALE: 1:3.06

B

B

A

A

1 2 3 4 5 6

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A2
 studio
 architecture
 design
 131 6TH. W, Ketchum, Idaho,
 83340

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 233 Parkway Drive, Ketchum,
 Idaho, 83340

**Bruner Residence
 Garage Addition**
 TAX LOT 8629 Section 13, T.4N.,
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 County, Idaho

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 STATE OF IDAHO

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SHEET TITLE
3D Views

A-902

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Re: 233 Parkway Dr. Variance Application

Exhibit B



A. Aerial Site View



B. Public Access River Easement



C. Existing Garage and Driveway



D. Existing Garage Encroachment into Front Yard Setback