



# REGULAR PLANNING COMMISSION AGENDA

March 25, 2025 at 6:00 PM

Assembly Chambers/Zoom Webinar

<https://juneau.zoom.us/j/85421744892> or 1-253-215-8782 Webinar ID: 854 2174 4892

**A. LAND ACKNOWLEDGEMENT**

We would like to acknowledge that the City and Borough of Juneau is on Tlingit land, and wish to honor the indigenous people of this land. For more than ten thousand years, Alaska Native people have been and continue to be integral to the well-being of our community. We are grateful to be in this place, a part of this community, and to honor the culture, traditions, and resilience of the Tlingit people. Gunalchéesh!

**B. ROLL CALL**

**C. REQUEST FOR AGENDA CHANGES AND APPROVAL OF AGENDA**

**D. APPROVAL OF MINUTES**

**E. BRIEF REVIEW OF THE RULES FOR PUBLIC PARTICIPATION**

**F. PUBLIC PARTICIPATION ON NON-AGENDA ITEMS**

**G. ITEMS FOR RECONSIDERATION**

**H. CONSENT AGENDA**

**1. USE2025 0006: A Conditional Use Permit to operate a Pet Grooming Salon. - **APPROVED AS RECOMMENDED****

Applicant: Anna Hay

Location: 2203 Dunn Street

**DIRECTOR'S REPORT**

The applicant requests a Conditional Use Permit to operate a pet grooming salon with 1 to 2 employees by appointment. Hours of operation would be Sunday to Thursday 8:00am – 5:00pm. A small retail space will be located at the front of the store. Pet boarding is not permitted. The proposed use would occur in one (1) unit of an eight (8) unit mixed-use structure.

**STAFF RECOMMENDATION**

Staff recommends the Planning Commission adopt the Director's analysis and findings and approve the Conditional Use Permit USE2025 0006 with the requested condition.

**2. USE2025 0007: Conditional Use Permit for a three-story dental clinic. - **APPROVED AS AMENDED****

Applicant: Dawson Construction

Location: 3063 Vintage Blvd.

**DIRECTOR'S REPORT**

The applicant requests a Conditional Use Permit to construct a three-story, 19,635 square foot dental clinic in a Light Commercial zoning district. This facility will become a part of the Southeast Alaska Regional Health Consortium’s medical campus in the Vintage Park subdivision.

**STAFF RECOMMENDATION**

Staff recommends the Planning Commission adopt the Director’s analysis and findings and approve Conditional Use Permit USE2025 0007 with conditions.

- I. **UNFINISHED BUSINESS**
- J. **REGULAR AGENDA**
- K. **OTHER BUSINESS**
- L. **STAFF REPORTS**
- M. **COMMITTEE REPORTS**
- N. **LIAISON REPORT**
- O. **CONTINUATION OF PUBLIC PARTICIPATION ON NON-AGENDA ITEMS**
- P. **PLANNING COMMISSION COMMENTS AND QUESTIONS**
- Q. **EXECUTIVE SESSION**
- R. **SUPPLEMENTAL MATERIALS**
  - [3.](#) Additional Materials
- S. **ADJOURNMENT**

ADA accommodations available upon request: Please contact the Clerk's office 36 hours prior to any meeting so arrangements can be made for closed captioning or sign language interpreter services depending on the meeting format. The Clerk's office telephone number is 586-5278, TDD 586-5351, e-mail: city.clerk@juneau.gov.





PLANNING COMMISSION STAFF  
CONDITIONAL USE PERMIT USE2025 0006  
HEARING DATE: MARCH 25, 2025

(907) 586-0715  
CDD\_Admin@juneau.gov  
www.juneau.org/community-development  
155 Heritage Way • Juneau, AK 99801

**DATE:** March 13, 2025  
**TO:** Mandy Cole, Chair, Planning Commission  
**BY:** Jolene Murphy, Planner I *Jolene Murphy*  
**THROUGH:** Jill Lawhorne, Director, AICP

**PROPOSAL:** A Conditional Use Permit to operate a pet grooming salon with 1 to 2 employees by appointment

**STAFF RECOMMENDATION:** Approval with condition

**KEY CONSIDERATIONS FOR REVIEW:**

- Proposed use is on the lower-level of a mixed-use, residential and commercial building.
- Use conforms with the land use code.

**ALTERNATIVE ACTIONS:**

1. **Amend:** require additional conditions, or delete or modify the recommended conditions.
2. **Deny:** deny the permit and adopt new findings for items 1-6 below that support the denial.
3. **Continue:** to a future meeting date if determined that additional information or analysis is needed to make a decision, or if additional testimony is warranted.

**ASSEMBLY ACTION REQUIRED:**

Assembly action is not required for this permit.

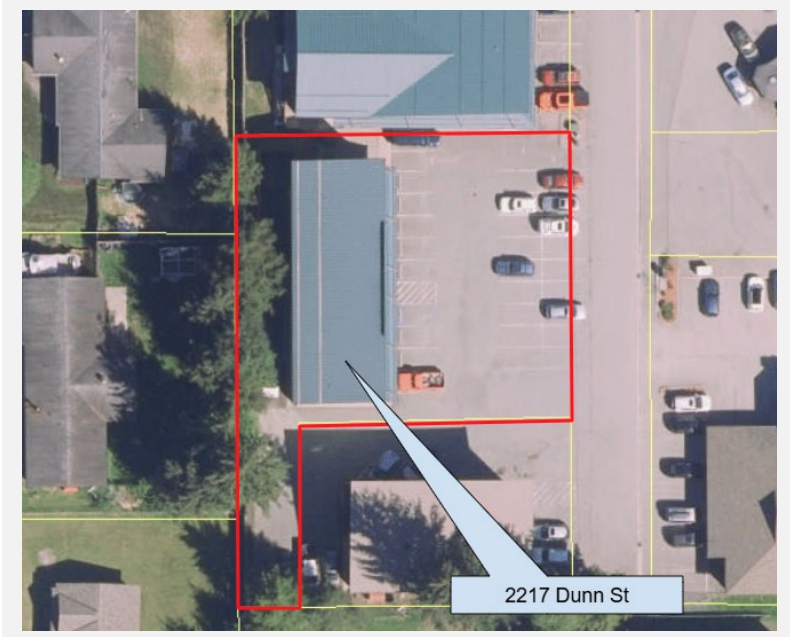
**STANDARD OF REVIEW:**

- Quasi-judicial decision
- Requires five (5) affirmative votes for approval
- Code Provisions:
  - 49.15.330
  - 49.40.210(b)
  - 49.40.300(a)(3)
  - 49.80

GENERAL INFORMATION	
Property Owner	RH Rentals LLC
Applicant	Anna Hay
Property Address	2203 Dunn Street
Legal Description	J W MCKINLEY BL 32 LT 8A
Parcel Number	5B1601130046
Zoning	LC – Light Commercial
Land Use Designation	Commercial
Lot Size	16,045 square feet; 0.37 acres
Space Used	720 square feet
Water/Sewer	CBJ / CBJ
Access	Dunn St
Existing Land Use	Commercial, Residential
Associated Applications	PAC25-04

**The Commission shall hear and decide the case per 49.15.330(a) Conditional Use Permit.** A conditional use is a use that may or may not be appropriate in a particular zoning district according to the character, intensity, or size of that or surrounding uses. The conditional use permit procedures is intended to afford the commission the flexibility necessary to make determinations appropriate to individual sites. The commission may attach to the permit those conditions listed in subsection (g) of this section as well as any further conditions necessary to mitigate external adverse impacts. If the commission determines that these impacts cannot be satisfactorily overcome, the permit shall be denied.

**SITE FEATURES AND ZONING**



SURROUNDING ZONING AND LAND USES	
North (LC)	Commercial Office
South (LC)	2-Unit Commercial Office
East (ROW)	Dunn St
West (D5)	3 Single-Family Structures

SITE FEATURES	
Anadromous	None
Flood Zone	None
Hazard	None Mapped
Hillside	N/A
Wetlands	None
Parking District	None
Historic District	None
Overlay Districts	Mining & Exploration Surface Exclusion District

**BACKGROUND INFORMATION**

Applicant is proposing to operate a pet grooming salon from 2217 Dunn Street with 1 to 2 employees from Sunday to Thursday 8:00am – 5:00pm. A small retail space will be located at the front of the store. Pet boarding is not permitted. The proposed use would occur in one (1) unit of an eight (8) unit mixed-use structure.

The 5,000 square foot mixed-use building was constructed in 2006 (**Attachment F**). USE2003-49 permitted construction of four commercial buildings across four lots on the western half of Dunn Street. Prior to construction, VAR2005-39 was approved, which permitted backout parking in the Dunn Street ROW. This variance was granted because Dunn Street is a privately maintained, narrow, low-speed street. Under SUB2007-17, the current lot was created out of previous portions of Lots 8, 9 and 10 (**Attachment H**).

In 2008, a vacant unit on the second floor was converted to a yoga studio. In 2009, a certificate of occupancy was issued for a massage therapist on the lower level. In 2011, the yoga studio was then converted into two, 1-bedroom apartments (**Attachment J**). An additional efficiency unit was created out of former storage space on the second story in 2013 (**Attachment K**).

As of 2013, the 2<sup>nd</sup> floor consists of 2 one-bedroom apartments, an office and an efficiency unit, while the lower level has remained commercial (**Attachment K**). Site visit indicates that at least two of the lower retail units are currently vacant (**Attachment N**). Preparation for use as a pet grooming salon in Unit 2213 was approved under BLD2024-811 (**Attachment M**).

**Background** – The table below summarizes the relevant history for the parcel and proposed development.

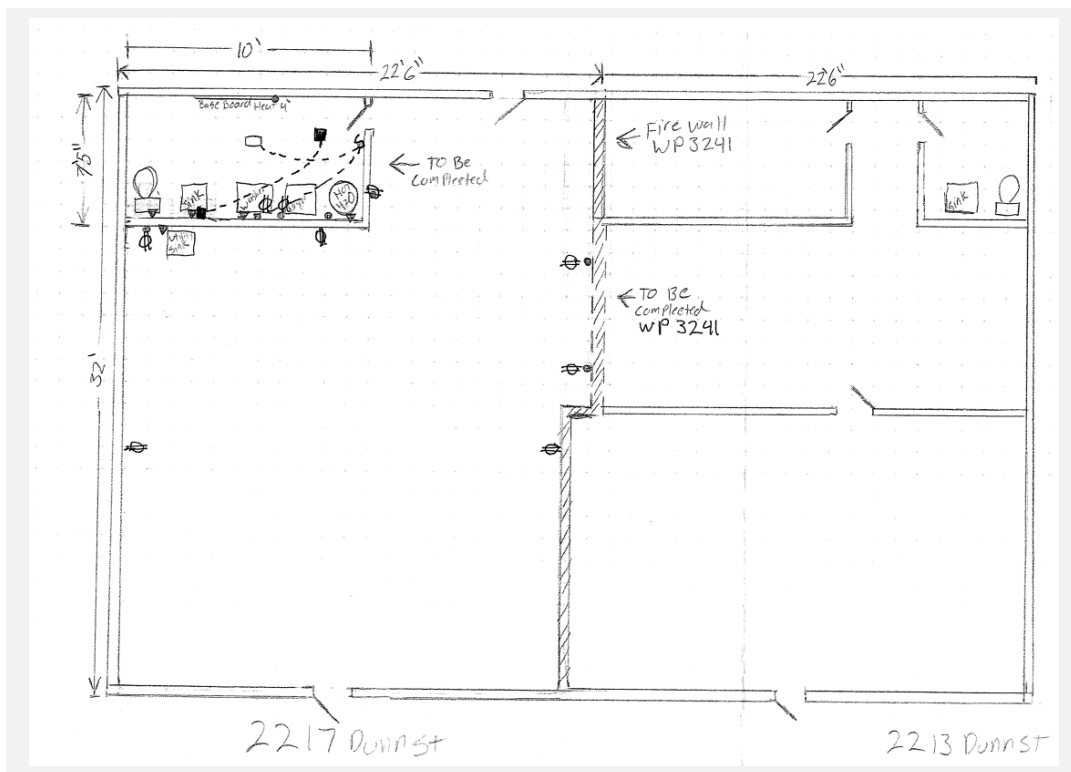
Date	Item	Summary
2003	USE2003-49	Allowable Use Permit to construct 20,000 square feet of mixed-use development across lots 5A-10. Lot lines and legal descriptions later changed. Approved in 2006. USE permit approved with Dunn Street as a privately maintained right-of-way, which was allowable as the applicant owned all lots accessed by Dunn Street. Discussion about improving Dunn Street at a later date to create a dedicated right-of-way were included in the staff report. <b>(Attachment E)</b> .
2003	VAR2003-41	Lot 5A backout parking onto Dunn St ROW, approved.
2006	BLD2006-679	Building Construction, 2-stories, outstanding permit. Modified in 2007 to change 1 <sup>st</sup> and 2 <sup>nd</sup> floor layout. Modified 2008 for Unit 2213 improvements <b>(Attachment F)</b> .
2005	VAR2005-35	Lot 7A backout parking onto Dunn St ROW, approved.
2005	VAR2005-39	Approval to allow backout parking in Dunn St ROW for Lot 9A & 10A, requested concurrently with USE2003-49. Former lot 9A is current lot <b>(Attachment G)</b> .
2005	4 Variance Requests	VAR2005-33, VAR2005-36, VAR2005-37, VAR2005-40. Concurrent requests to permit 0-foot lot lines between Lots 7A, 8 and 9. All 4 variances denied.
2006	VAR2006-19	Denial of request to reduce rear yard setback from 20 to 13 feet.
2007	SUB2007-07 Plan	Subdivision to current lot size, includes drainage easement, lot dimensions. Current lot 8A was previously portions of Lot 8, Lot 9 and Lot 10 <b>(Attachment H)</b> .
2007	Drainage Plan	Proposed drainage plan <b>(Attachment I)</b> .
2008	BLD2008-264	Convert vacant 2 <sup>nd</sup> floor retail to Yoga Studio.
2009	BLD2009-146	Convert lower-level unit into massage studio.
2011	BLD2011-712	Convert 2 <sup>nd</sup> floor Yoga Studio to two, 1-bedroom apartments <b>(Attachment J)</b> .
2013	BLD2013-030	Change of use from storage space to efficiency apartment on the 2 <sup>nd</sup> story <b>(Attachment K)</b> .
2016	Dunn Street Agreement	Ownership and Maintenance Agreement between CBJ Lands & Resources, RH Rentals and Duck Creek Properties. Dunn Street was paved in 2016 by the CBJ with contributions of property owners. This was approved by the CBJ Assembly under Ordinance 2016-21. <b>(Attachment L)</b> .
2021	PAC21-40	Daycare proposed in 2 lower-level units <b>(Attachment M)</b> .
2024	BLD2024-811	Current project site preparation and layout. Partition wall added between Unit 2217 and Unit 2213 <b>(Attachment N)</b> .
2025	Assessor Data	Photo, year constructed, lot size <b>(Attachment O)</b> .
2025	Site Visit Photos	Current tenants, parking <b>(Attachment P)</b> .

**ZONING REQUIREMENTS**

Standard		Requirement	Existing	Code Reference
Lot	Size	2,000 square feet	16,045 square feet	CBJ 49.25.400
	Width	20 feet	108 feet	CBJ 49.25.400
Setbacks	Front (E)	25 feet	110 feet	CBJ 49.25.400

Standard		Requirement	Existing	Code Reference
	Rear (W)	20 feet	20.01 feet	CBJ 49.25.400
	Side (N)	10 feet	10 feet	CBJ 49.25.400
	Side (S)	10 feet	10 feet	CBJ 49.25.400
<b>Lot Coverage Maximum</b>		None	~23%	CBJ 49.25.400
<b>Vegetative Cover Minimum</b>		15%	<b>8%</b>	CBJ 49.50.300
<b>Height</b>	Permissible	45 feet	2-story structure	CBJ 49.25.400
	Accessory	35 feet	N/A	CBJ 49.25.400
<b>Maximum Dwelling Units (30/Acre)</b>		11 units	3 units	CBJ 49.25.500
<b>Use</b>		Mixed-Use	Mixed-Use	CBJ 49.25.300

**SITE PREPARATION PLANS FROM BLD24-811**



**ANALYSIS**

**Project Site** – Unit 2217 is located on the ground floor on the eastern half of the property. The structure is at the end of Dunn Street and was developed in tandem with the existing structures on Lot 5A and Lot 7A. No exterior changes are proposed as part of this change in use.

**Project Design** – The proposed use will occupy a 720 square foot space, with room for a small pet-related retail selection at the front of the store. Sinks completed under BLD2024-811 are located in the rear. Hours of operation will be Sunday through Thursday 8:00 am – 5:00 pm.

**Traffic** – Traffic is estimated using the Institute of Traffic Engineer’s Trip Generation Manual, 9<sup>th</sup> Edition (ITE). The “Hair Salon with 1,000 square feet of GFA” use most closely resembles the possible traffic generated for a

pet grooming salon with 1-2 employees. A metric for AADT is not provided in the ITE. Only peak hour traffic estimates are included in the table below.

The proposed use is estimated to generate fewer than 250 average daily trips and will not require a traffic impact analysis [CBJ 49.40.300(a)(3)].

Use	Volume/Page	Metric	Approximate	Total AADT Using Peak Hour Estimates
Hair Salon	3:1857	1.21 trips/AM peak hour	4 hours	4.84
	3:1857	1.49 trips/AM peak hour	4 hours	5.96
<b>Total AADT (rounded)</b>				<b>10.8</b>

**Vehicle Parking & Circulation** – Three parking spaces are required for the proposed use. There will be three designated parking spaces in front of the salon and access to the remaining unreserved spaces in the lot.



Parking History:

USE2003-49 required 23 parking spaces for the 5,000 square foot structure. Eighteen spaces were needed for the 3,600 square feet of retail on the first floor, five were required for the 1,000 square feet of retail on the upper level. Under VAR2005-39, backout parking along Dunn Street was approved by the commission. BLD06-679 permitted construction with 19 spaces, one being accessible.

In PAC21-40, a proposal to operate a childcare center in the lower-level units 2213 and 2205 was proposed. This increased the parking requirement to 20 spaces. According to BLD2021-45, a conversion to this use was finalized. However, according to 2024 Assessor Photos Unit 2213 is currently for lease. The remaining lower-level retail spaces are calculated below.

Use	Unit/Total Sq. Ft.	Spaces Required
<b>Hair Salon</b>	1 per 300 square feet	3 / 720 square feet
<b>Remaining Lower-Level Retail</b>	1 per 300 square feet	7.2 / 2,160 square feet
<b>Upper-Level Owners Office</b>	1 per 300 square feet	2.4
<b>Two, Upper-Level One-bedroom Apartments</b>	1 space/bedroom	2

Use	Unit/Total Sq. Ft.	Spaces Required
Upper-Level Efficiency Apartment	1 space/ bedroom	1
<b>Total Parking Requirement:</b>		<b>15</b>
<b>ADA Accessible Spaces Required:</b>		<b>2</b>
<b>Total Provided Spaces:</b>		<b>29</b>

A total of 29 spaces, one being accessible, are currently provided according to Site Visit. Per CBJ 49.40.210(b), the 3 upper-level apartments do not require accessible spaces. With 29 spaces provided in the lot, 3 of which are exempt, a total of 2 accessible spaces are required.

**Condition 1: Prior to commencement of the proposed use, proof of the required on-site ADA spaces shall be provided to Community Development.**

**Noise** – Noise generated by this use is not expected to be out of character for the area.

**Lighting** - Exterior lighting may not shed light or glare above the roofline of the building or beyond the property line of the site. Industrial and exterior lighting shall not be used in a manner that produces glare on public highways or neighboring property. No changes to exterior lighting are proposed.

**Vegetative Cover & Landscaping** – The LC zoning district requires 15% vegetative coverage. Site visit and PAC21-40 indicate subject parcel has approximately 8% vegetative coverage. Any further reduction in coverage will not be permitted (**Attachment A & N**).

**Habitat** - No anadromous waterbodies are on the subject parcel, or within 50 feet.

**Drainage and Snow Storage** – There are no proposed grading or drainage changes.

**Historic District** – This property is not located within the historic district.

**Hazard Zones** – The property is not within a mapped hazard zone.

**Public Health, Safety, and Welfare** – The proposed pet grooming use is not anticipated to impact public health, safety and welfare.

**Property Value or Neighborhood Harmony** – There is no evidence to suggest that the proposed development will be out of character with the existing neighborhood. Since construction was completed in 2007, the subject lot has had a variety of commercial and residential tenants.

**AGENCY REVIEW**

CDD conducted an agency review comment period between February 19th and March 10th and received the following responses:

Agency	Summary
Building	No concerns from Building
General Engineering	No concerns from GE
Fire	No concerns from Fire

**PUBLIC COMMENTS**

CDD conducted a public comment period between February 11th and March 6th. Public notice was mailed to property owners within 500 feet of the proposed development. A public notice sign was also posted on-site two weeks prior to the scheduled hearing. No public comments have been received at this time.

**CONFORMITY WITH ADOPTED PLANS**

The proposed development is in general conformity with the 2013 Comprehensive Plan & 2015 Juneau Economic Development Plan.

PLAN	Chapter	Page No.	Item	Summary
2013 Comprehensive Plan	5	45	Policy 5.2	Develop facilities and other services to enhance the state capital for all state residents
	5	47	Policy 5.4 – SOP7	Strengthen Juneau’s role as a regional services hub
	5	65	Policy 5.20 – SOP2	Encourage local purchases of goods by local industries
2015 Juneau Economic Development Plan	6	66	Action 4-B	Support amenities that attract and retain Juneau’s workforce

**FINDINGS**

**Conditional Use Permit Criteria** – Per CBJ 49.15.330 (e) & (f), Review of Director's & Commission’s Determinations, the Director makes the following findings on the proposed development:

**1. Is the application for the requested Conditional Use Permit complete?**

**Analysis:** No further analysis required.

**Finding: Yes.** The application contains the information necessary to conduct a full review of the proposal. The application submittal by the Applicant, including the appropriate fees substantially conforms to the requirements of CBJ 49.15.

**2. Is the proposed use appropriate according to the Table of Permissible Uses?**

**Analysis:** The application is for a pet grooming salon with 1 to 2 employees in an LC zoning district.

**Finding: Yes.** The proposed use is appropriate for the zoning district with a USE permit per CBJ 49.25.300 paragraph 12.250.

**3. Will the proposed development comply with the other requirements of this chapter?**

**Analysis:** No further analysis required.

**Finding: Yes.** With the recommended condition, the proposed development will comply with Title 49, including parking, lighting, vegetative cover and noise.

**4. Will the proposed development materially endanger public health, safety, or welfare?**

**Analysis:** No further analysis required.

**Finding: No.** There is no evidence to suggest that the requested pet grooming salon in a LC zoning district will materially endanger the public health or safety.

**5. Will the proposed development substantially decrease the value of or be out of harmony with property in the neighboring area?**

**Analysis:** No additional analysis required.

**Finding: No.** There is no evidence to suggest that the requested pet grooming salon in a LC zoning district will substantially decrease the value or be out of harmony with the property in the neighboring area.

**6. Will the proposed development be in general conformity with the Land Use Plan, Thoroughfare Plan, or other officially adopted plans?**

**Analysis:** No additional analysis required.

**Finding: Yes.** The proposed pet grooming salon, with the recommended condition, will be in general conformity with the 2013 Comprehensive Plan and 2015 Juneau Economic Development Plan.

**RECOMMENDATION**

Staff recommends the Planning Commission adopt the Director's analysis and findings and APPROVE the requested Conditional Use Permit. The permit would allow the development of pet grooming, walking and pet daycare services in a D3 zone.

The approval is subject to the following conditions:

1. Prior to commencement of the proposed use, proof of the required on-site ADA spaces shall be provided to Community Development.

**STAFF REPORT ATTACHMENTS**

Item	Description
Attachment A	Application Packet
Attachment B	Abutters Notice and Public Notice Sign Photo
Attachment C	Agency Comments
Attachment D	USE2003-049
Attachment E	BLD2006-679
Attachment F	VAR2005-039 NOD
Attachment G	SUB2007-007 Plan
Attachment H	2007 Drainage Plan



<b>Item</b>	<b>Description</b>
<b>Attachment I</b>	<i>BLD2011-712</i>
<b>Attachment J</b>	<i>PAC21-040</i>
<b>Attachment K</b>	<i>2016 Dunn Street Agreement</i>
<b>Attachment L</b>	<i>BLD2013-030</i>
<b>Attachment M</b>	<i>BLD2024-811</i>
<b>Attachment N</b>	<i>2025 Assessor Data &amp; Site Photos</i>



# DEVELOPMENT PERMIT APPLICATION

**NOTE: Development Permit Application forms must accompany all other Community Development Department land use applications. This form and all documents associated with it are public record once submitted.**

To be completed by Applicant	<b>PROPERTY LOCATION</b>									
	Physical Address <u>2217 DUNN ST</u>									
	Legal Description(s) (Subdivision, Survey, Block, Tract, Lot) <u>SW McCLuskey BL 32 LT 8A</u>									
	Parcel Number(s) <u>581601130096</u>									
	<input type="checkbox"/> This property is located in the downtown historic district									
	<input type="checkbox"/> This property is located in a mapped hazard area, if so, which _____									
	<b>LANDOWNER/ LESSEE</b>									
	Property Owner <u>RH Rentals LLC</u>	Contact Person <u>RICH HARRIS</u>	Phone Number(s) <u>907-723-4791 (Rich cell)</u>							
	Mailing Address <u>P.O. Box 32403 JUNEAU, AK 99803</u>	E-mail Address <u>N/A</u>								
	<b>LANDOWNER/ LESSEE CONSENT</b>									
Required for Planning Permits, not needed on Building/ Engineering Permits. Consent is required of all landowners/ lessees. If submitted with the application, alternative written approval may be sufficient. Written approval must include the property location, landowner/ lessee's printed name, signature, and the applicant's name.										
I am (we are) the owner(s) or lessee(s) of the property subject to this application and I (we) consent as follows: A. This application for a land use or activity review for development on my (our) property is made with my complete understanding and permission. B. I (we) grant permission for the City and Borough of Juneau officials/employees to inspect my property as needed for purposes of this application.										
<table style="width:100%; border: none;"> <tr> <td style="width:50%;"><u>Jesse Hay</u> Landowner/Lessee (Printed Name)</td> <td style="width:50%;"><u>Lessee</u> Title (e.g.: Landowner, Lessee)</td> </tr> <tr> <td><u>[Signature]</u> Landowner/Lessee (Signature)</td> <td><u>2.5.25</u> Date</td> </tr> <tr> <td><u>Anna L. Hay</u> Landowner/Lessee (Printed Name)</td> <td><u>Lessee</u> Title (e.g.: Landowner, Lessee)</td> </tr> <tr> <td><u>[Signature]</u> Landowner/Lessee (Signature)</td> <td><u>2.5.25</u> Date</td> </tr> </table>			<u>Jesse Hay</u> Landowner/Lessee (Printed Name)	<u>Lessee</u> Title (e.g.: Landowner, Lessee)	<u>[Signature]</u> Landowner/Lessee (Signature)	<u>2.5.25</u> Date	<u>Anna L. Hay</u> Landowner/Lessee (Printed Name)	<u>Lessee</u> Title (e.g.: Landowner, Lessee)	<u>[Signature]</u> Landowner/Lessee (Signature)	<u>2.5.25</u> Date
<u>Jesse Hay</u> Landowner/Lessee (Printed Name)	<u>Lessee</u> Title (e.g.: Landowner, Lessee)									
<u>[Signature]</u> Landowner/Lessee (Signature)	<u>2.5.25</u> Date									
<u>Anna L. Hay</u> Landowner/Lessee (Printed Name)	<u>Lessee</u> Title (e.g.: Landowner, Lessee)									
<u>[Signature]</u> Landowner/Lessee (Signature)	<u>2.5.25</u> Date									
NOTICE: The City and Borough of Juneau staff may need access to the subject property during regular business hours. We will make every effort to contact you in advance, but may need to access the property in your absence and in accordance with the consent above. Also, members of the Planning Commission may visit the property before a scheduled public hearing date.										
<b>APPLICANT</b> <span style="float: right;">If same as LANDOWNER, write "SAME"</span>										
Applicant (Printed Name) <u>ANNA L HAY</u>	Contact Person <u>ANNA HAY</u>	Phone Number(s) <u>907.967.8102-work</u> <u>907-713-9215-personal</u>								
Mailing Address <u>P.O. Box 32339 JUNEAU, AK 99803</u>	E-mail Address <u>AKGLD907@gmail.com</u>									
<u>[Signature]</u> Applicant's Signature	<u>2.5.25</u> Date of Application									

DEPARTMENT USE ONLY BELOW THIS LINE

Intake Initials  
[Signature] 2/6/25

**INCOMPLETE APPLICATIONS WILL NOT BE ACCEPTED**

For assistance filling out this form, contact the Permit Center at 586-0770.

Case Number <u>USE25-06</u>	Date Received <u>2-6-25</u>
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# ALLOWABLE/CONDITIONAL USE PERMIT APPLICATION

See reverse side for more information regarding the permitting process and the materials required for a complete application.

**NOTE: Must be accompanied by a DEVELOPMENT PERMIT APPLICATION form.**

To be completed by Applicant

<b>PROJECT SUMMARY</b>	
Set up and operation of a Dog grooming Salon / <i>Noder night animal Boarding</i>	
<b>TYPE OF ALLOWABLE OR CONDITIONAL USE PERMIT REQUESTED</b>	
<input type="radio"/> Accessory Apartment – Accessory Apartment Application (AAP) <input checked="" type="radio"/> Use Listed in 49.25.300 – Table of Permissible Uses (USE) Table of Permissible Uses Category: <u>12.250</u>	
<b>IS THIS A MODIFICATION or EXTENSION OF AN EXISTING APPROVAL?</b> <input type="radio"/> YES – Case # _____ <input checked="" type="radio"/> NO	
<b>UTILITIES PROPOSED</b> WATER: <input checked="" type="checkbox"/> Public <input type="checkbox"/> On Site                      SEWER: <input checked="" type="checkbox"/> Public <input type="checkbox"/> On Site	
<b>SITE AND BUILDING SPECIFICS</b>	
Total Area of Lot <u>16,045 - 0.37 acres</u> square feet                      Total Area of Existing Structure(s) <u>16,045</u> square feet	
Total Area of Proposed Structure(s) <u>720</u> square feet	
<b>EXTERNAL LIGHTING</b>	
Existing to remain <input type="radio"/> No <input checked="" type="radio"/> Yes – Provide fixture information, cutoff sheets, and location of lighting fixtures	
Proposed <input type="radio"/> No <input checked="" type="radio"/> Yes – Provide fixture information, cutoff sheets, and location of lighting fixtures	
<b>ALL REQUIRED DOCUMENTS ATTACHED</b>	
<input checked="" type="checkbox"/> Narrative including: → <i>in PAC notes</i> <input checked="" type="checkbox"/> Current use of land or building(s) <input checked="" type="checkbox"/> Description of project, project site, circulation, traffic etc. <input checked="" type="checkbox"/> Proposed use of land or building(s) <input type="checkbox"/> How the proposed use complies with the Comprehensive Plan  <input type="checkbox"/> Plans including: <input type="checkbox"/> Site plan <input checked="" type="checkbox"/> Floor plan(s) <input checked="" type="checkbox"/> Elevation view of existing and proposed buildings <input checked="" type="checkbox"/> Proposed vegetative cover <i>existing</i> <input checked="" type="checkbox"/> Existing and proposed parking areas and proposed traffic circulation <i>ps 3 of Pac</i> <input checked="" type="checkbox"/> Existing physical features of the site (e.g.: drainage, habitat, and hazard areas) <i>existing</i>	If this is a modification or extension include: <input type="checkbox"/> Notice of Decision and case number <input type="checkbox"/> Justification for the modification or extension <input type="checkbox"/> Application submitted at least 30 days before expiration date  → <i>N/A</i>

DEPARTMENT USE ONLY BELOW THIS LINE

ALLOWABLE/CONDITIONAL USE FEES	Fees	Check No.	Receipt	Date
Application Fees	\$ <u>500.00</u>			
Admin. of Guarantee	\$ _____			
Adjustment	\$ _____			
Pub. Not. Sign Fee	\$ <u>50.00</u>			
Pub. Not. Sign Deposit	\$ <u>100.00</u>			
<b>Total Fee</b>	<b>\$ <u>650.00</u></b>			

This form and all documents associated with it are public record once submitted.

**INCOMPLETE APPLICATIONS WILL NOT BE ACCEPTED**

For assistance filling out this form, contact the Permit Center at 586-0770.

Case Number	Date Received
<u>USE25-06</u>	<u>2/6/25</u>

# NARRATIVE

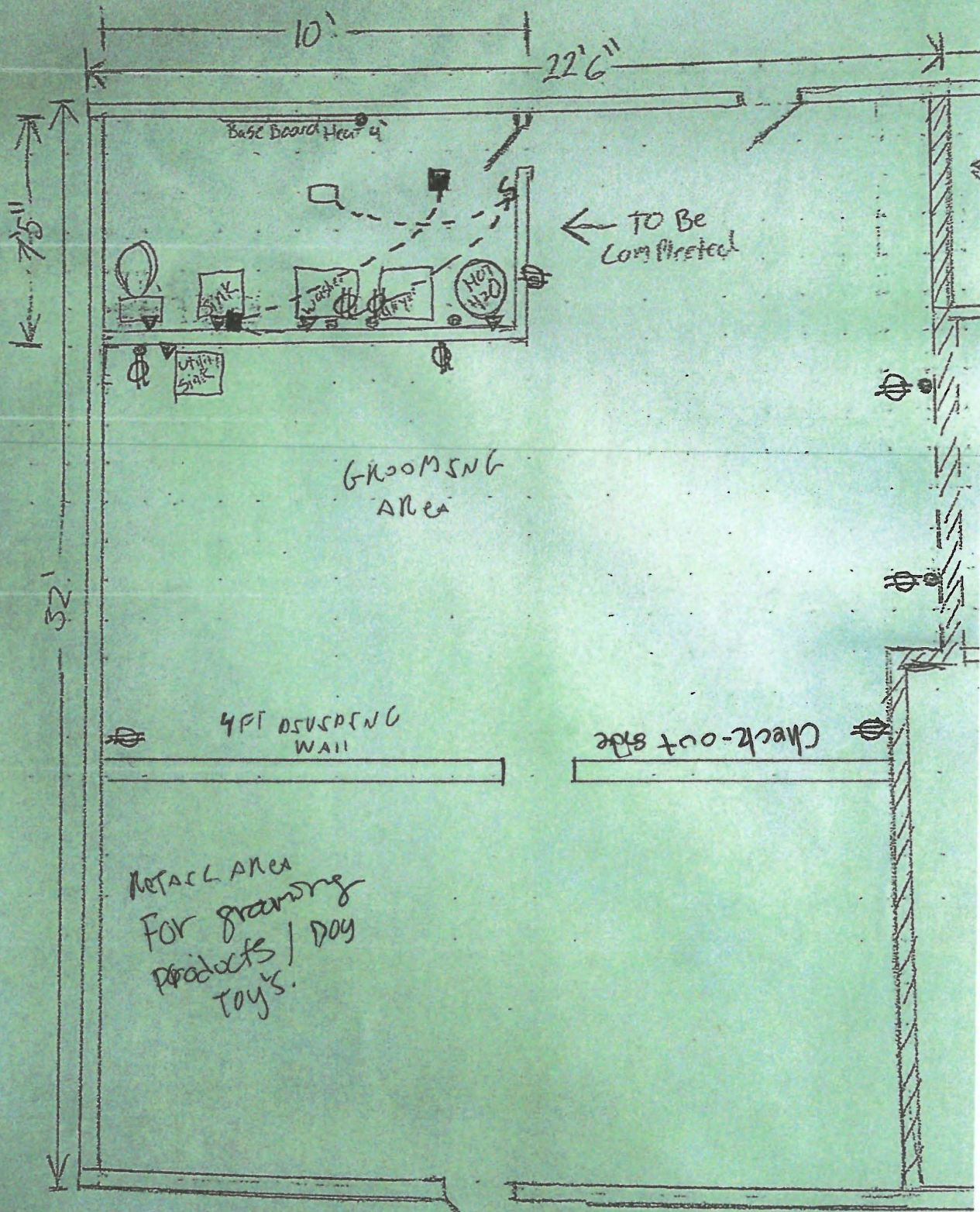
Pampered Paws Grooming is a one-on-one family operated dog grooming salon that provides bathing, hair cutting, and nail trims. Pet boarding is not offered, no animals will be left overnight. The hours of the operation will be Sunday through Thursday 8am till 5pm and Friday & Saturday noon till 5pm. A small retail section will be in the front section of the store offering treats, toys etc. Staff will be 1 to 2 employees.

Pampered paws grooming will be located at 2217 Dunn st, the building currently has 2 spaces on the street level occupied by Anchor Ak Real estate & RH rentals office along with 4 apartments on the upper floor. The space Pampered paws is going into is currently being divided in half under building permit 20240811, the remaining half of this space will currently be vacant. There will be no changes to the overall footprint of the building or any surface disruption to the existing site.

---



FLOOR PLAN



Galaxy S23 Ultra

2217 Down St





Feb 06, 2025 10:10:29 AM

Erin Vinton Elm



SITE PLAN





(907) 586-0715  
 CDD\_Admin@juneau.gov  
 www.juneau.org/community-development  
 155 Heritage Way • Juneau, AK 99801

**Pampered Paws Grooming**

Case Number: PAC2025 0004  
 Applicant: ~~Anne Hay~~ Anne Hay  
 Property Owner: R H Rentals LLC  
 Property Address: 2217 Dunn St  
 Parcel Code Number: 5B1601130046

Site Size: Lot size: 16,045 square feet, 0.37 acres  
 Space used: 720 square feet  
 Zoning: LC  
 Existing Land Use: Commercial

Conference Date: January 29, 2025  
 Report Issued: February 5, 2025  
**DISCLAIMER: Pre-application conferences are conducted for the purpose of providing applicants with a preliminary review of a project and timeline. Pre-application conferences are not based on a complete application and are not a guarantee of final project approval.**

**List of Attendees**

Note: Copies of the Pre-Application Conference Report will be emailed, instead of mailed, to participants who have provided their email address below.

Name	Title	Email address
Jessie Hay Anne Hay	Applicant	akgldn907@gmail.com
Jolene Murphy	Planning	Jolene.Murphy@juneau.gov
Jeff Hedges	Building	Jeffrey.Hedges@juneau.gov
Sydney Hawkins	Permit Tech III	Sydney.Hawkins@juneau.gov



## Pre-Application Conference Final Report

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### Conference Summary

#### Questions/issues/agreements identified at the conference that weren't identified in the attached reports.

The following is a list of issues, comments and proposed actions, and requested technical submittal items that were discussed at the pre-application conference.

- Q: Are you adding any signs? A: No plans at this time.

### Project Overview

(Provide a brief description of the proposed project. *Note to Planners be aware if there have been any previous PACs for this applicant or site.*)

Applicant is proposing to operate a dog grooming salon from 2217 Dunn Street with 1-2 employees from Sunday-Thursday 8:00am – 5:00pm. A small retail space will be in the front of the store. Overnight pet boarding is not offered.

BLD2024-811 permitted a conversion of the existing space to include 2 additional sinks, a washer and dryer and additional water line hookups to prepare the site.

A Conditional Use Permit will be required (CBJ 49.25.300, paragraph 12.250).

### Summary of the Conditional Use Permit Process

- Submit the application and back-up materials (listed on ten back of the application).
  - Electronic submissions accepted at [Permits@juneau.gov](mailto:Permits@juneau.gov). Note that the permit center will call you for payment when the application is processed. Applications are submitted in the order in which they are received, and it may be a few days before you get a call.
- The project will be assigned to a planner. They will review submitted materials, and coordinate where necessary. When the planner assesses the file is complete, they will schedule a hearing before the Planning Commission.
  - A notice will be sent to property owners within 500 feet of the project.
  - There will be two newspaper ads for the case.
  - The Applicant is required to post a Public Notice sign, which will be provided by CDD. The sign must be posted two weeks before the hearing.
  - Staff will prepare a report analyzing the project and make a recommendation to the Commission. The report will be publicly available the week before the hearing.
- At the Planning Commission meeting, the project can be:
  - On the Consent Agenda, where it will be passed without discussion.
  - On the Regular Agenda:
    - The Director will briefly describe the project.
    - The Applicant has 15 minutes to describe the project.
    - The public has the opportunity to comment. There is usually a time limit of two to three minutes.
    - The Applicant has time to respond to issues raised.
    - Public comment is closed and there is no additional opportunity to participate.
- The Planning Commission will:
  - Approve the project
  - Approve the project with conditions (the most common outcome)
  - Deny the project

Pre-Application Conference Final Report

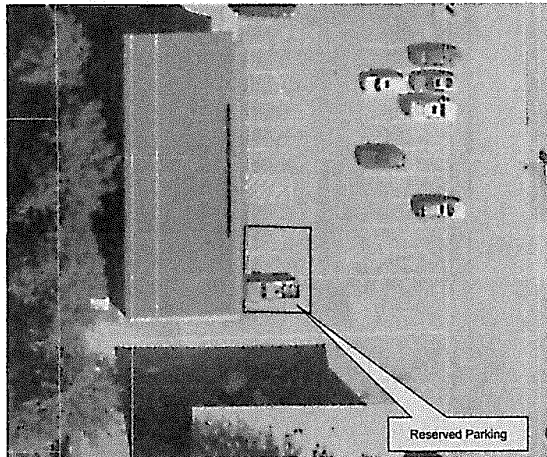
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- Continue the project – if more information is required or if the Commission runs out of time.
- The decision can be appealed for 20 days after the Notice of Decision is filed with the City Clerk. If the decision is appealed, the Applicant can continue with their project at their own risk.

Videos of the Planning Commission activities are posted on Assembly’s Minutes and Agendas site.  
<https://juneau-ak.municodemeetings.com/>

**Planning Division**

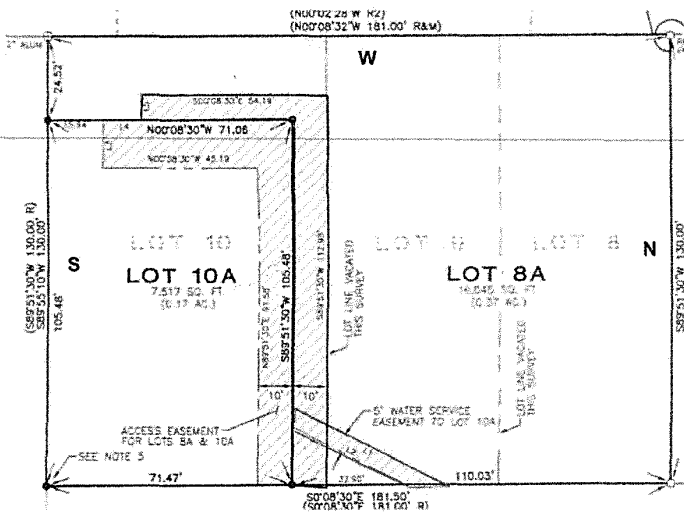
1. **Zoning** – LC (Light Commercial)
2. **Table of Permissible Uses**
  - a. Paragraph 12.250 - day animal services, grooming, walking, daycare. The TPU requires all pet grooming facilities in LC zoning to get a conditional use permit.
3. **Subdivision** – N/A
4. **Setbacks** – No increase in structure footprint is anticipated as part of this development.
  - a. Front (East) – 25 feet
  - b. Rear (West) – 10 feet
  - c. Side (North/South) – 10 feet
5. **Height** – No increase in structure height is anticipated as part of this development.
  - a. 35 feet for permissible uses
  - b. 25 feet for accessory uses
6. **Access** – Dunn Street
7. **Parking & Circulation** – 1 parking space is required per 300 square feet of gross floor area for a total of 3 spaces per the “salon” use in CBJ 49.40.210. There will be 3 designated parking spaces directly in front of the first-floor salon and access to the remaining unreserved spaces in the lot.



8. **Lot Coverage** – None
9. **Vegetative Coverage** – 15%

Pre-Application Conference Final Report

- 10. **Lighting** – Exterior lighting may not shed light or glare above the roofline of the building or beyond the property line of the site. Industrial and exterior lighting shall not be used in a manner that produces glare on public highways or neighboring property.
- 11. **Noise** – Noise generated by this use is not expected to be out of character for the area.
- 12. **Flood** – The property is not located in a flood zone.
- 13. **Hazard/Mass Wasting/Avalanche/Hillside Endorsement** – None mapped.
- 14. **Wetlands** – None.
- 15. **Habitat** – No anadromous waterbodies are on the subject parcel, or within 50 feet.
- 16. **Plat or Covenant Restrictions** – 10-foot access Easement along southern lot line and eastern portion of panhandle: 5-foot water service easement across eastern and southern lot lines.



- 17. **Traffic** – Traffic is estimated using the Institute of Traffic Engineer’s Trip Generation Manual, 9<sup>th</sup> Edition (ITE). The “Hair Salon with 1,000 square feet of GFA” use most closely resembles the possible traffic generated for a dog grooming salon with 1-2 employees. A metric for AADT is not provided in the ITE. Only peak hour traffic estimates are included in the table below.

The proposed use is estimated to generate fewer than 250 average daily trips and will not require a traffic impact analysis [CBJ 49.40.300(a)(3)].

Use	Volume/Page	Metric	Approximate	Total AADT Using Peak Hour Estimates
Hair Salon	3:1857	1.21 trips/AM peak hour	4 hours	4.84
Hair Salon	3:1857	1.49 trips/AM peak hour	4 hours	5.96
<b>Total (rounded)</b>				<b>10.8</b>

- 18. **Nonconforming situations** – No nonconforming situations exist.

Building Division

Pre-Application Conference Final Report

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- 19. **Building** – No issues with this project.
- 20. **Outstanding Permits** – BLD2006-00679 Construct the Building. BLD20110712 Convert Yoga Studio to Apartments. BLD20240811 This Project. Need to Final all three of these permits as soon as possible.

**General Engineering/Public Works**

- 21. **Engineering/Drainage/Utilities** – No comments at this time.

**Fire Marshal**

- 22. **Fire Items/Access** – N/A

**Other Applicable Agency Review**

- 23. N/A

**List of required applications**

Based upon the information submitted for pre-application review, the following list of applications must be submitted in order for the project to receive a thorough and speedy review.

- 1. Development Permit Application
- 2. Conditional Use Permit Application

**Additional Submittal Requirements**

Submittal of additional information, given the specifics of the development proposal and site, are listed below. These items will be required in order for the application to be determined Counter Complete.

- 1. A copy of this pre-application conference report.

**Exceptions to Submittal Requirements**

Submittal requirements that staff has determined **not** to be applicable or **not** required, given the specifics of the development proposal, are listed below. These items will **not** be required in order for the application to be reviewed.

- 1. N/A

**Fee Estimates**

The preliminary plan review fees listed below can be found in the CBJ code section 49.85.

Based upon the project plan submitted for pre-application review, staff has attempted to provide an accurate estimate for the permits and permit fees which will be triggered by your proposal.

- 1. Conditional Use Permit, Class II: \$500
- 2. Public Notice Sign: \$150, with \$100 refunded if the sign is returned by Monday following the Planning Commission meeting.

For informational handouts with submittal requirements for development applications, please visit our website at [www.juneau.org/community-development](http://www.juneau.org/community-development).

**Submit your Completed Application**

You may submit your application(s) online via email to [permits@juneau.gov](mailto:permits@juneau.gov)  
OR in person with payment made to:

Pre-Application Conference Final Report

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City & Borough of Juneau, Permit Center  
230 South Franklin Street  
Fourth Floor Marine View Center  
Juneau, AK 99801

Phone: (907) 586-0715  
Web: [www.juneau.org/community-development](http://www.juneau.org/community-development)

Attachments:

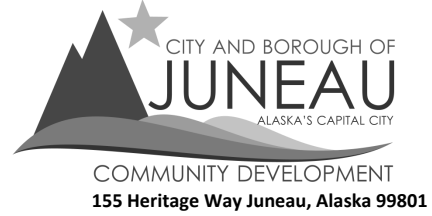
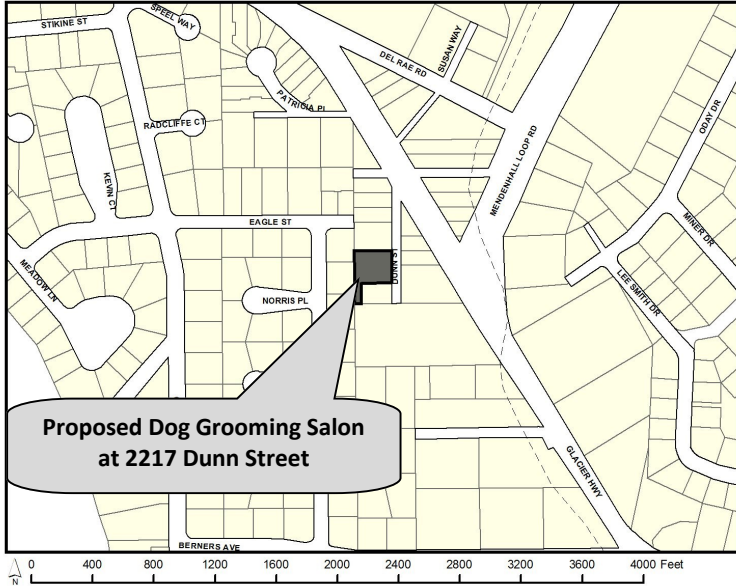
Development Permit Application  
Conditional Use Permit Application  
Sign Permit Application

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# Invitation to Comment

On a proposal to be heard by the CBJ Planning Commission

*Your Community, Your Voice*



TO

**A Conditional Use Permit** has been submitted for consideration and public hearing by the Planning Commission for a **Dog Grooming Salon** at **2217 Dunn Street** in a **LC zone**.

**PROJECT INFORMATION:**

Project Information can be found at:  
<https://juneau.org/community-development/short-term-projects>

**PLANNING COMMISSION DOCUMENTS:**

Staff Report expected to be posted **Monday, March 17, 2025** at  
<https://juneau.org/community-development/planning-commission>  
Find hearing results, meeting minutes, and more here, as well.

Now through March 13	March 14— noon, March 21	HEARING DATE & TIME: 6:00 pm, March 25, 2025	March 26
Comments received during this period will be sent to the Planner, Jolene Murphy, to be included as an attachment in the staff report.	Comments received during this period will be sent to Commissioners to read in preparation for the hearing.	This meeting will be held in person and by remote participation. For remote participation: join the Webinar by visiting <a href="https://juneau.zoom.us/j/85421744892">https://juneau.zoom.us/j/85421744892</a> and use the Webinar ID: 854 2174 4892 OR join by telephone, calling: 1-253-215-8782 and enter the Webinar ID (above).  You may also participate in person in City Hall Assembly Chambers, 155 Heritage Way Juneau, Alaska.	The results of the hearing will be posted online.

**FOR DETAILS OR QUESTIONS,**

Phone: (907)586-0753 ext. 4131  
Email: [pc\\_comments@juneau.gov](mailto:pc_comments@juneau.gov) or [Jolene.Murphy@juneau.gov](mailto:Jolene.Murphy@juneau.gov)  
Mail: Community Development, 155 Heritage Way, Juneau AK 99801

Case No.: USE2025 0006  
Parcel No.: 5B1601130046  
CBJ Parcel Viewer: <http://epv.juneau.org>





**NOTICE** For more information: 586-0715  
re-computer@cityofwv.org

**CONDITIONAL USE PERMIT**  
CASE: USE 2025 0006  
**OPERATE A PET GROOMING SALON.**  
HEARING DATE: 3/25/2025

Feb 24, 2025 03:51:39 PM



**From:** [Shawn Williams](#)  
**To:** [Jolene Murphy](#); [Theresa Ross](#); [Jeffrey Hedges](#)  
**Subject:** RE: USE25-06: Dog Grooming Salon Agency Comments  
**Date:** Monday, March 10, 2025 3:54:22 PM  
**Attachments:** [image002.png](#)

---

Looks good on the building side of things. Thanks!

**Shawn Williams | Building Inspector III**

[Community Development Department](#) | City & Borough of Juneau, AK

Location: 230 S. Franklin Street, 4<sup>th</sup> Floor Marine View Building

**Office: 907.586.0767 ext. 4136**



*Fostering excellence in development for this generation and the next.*

How are we doing? Provide feedback here: <https://juneau.org/community-development/how-are-we-doing>

---

**From:** Jolene Murphy <Jolene.Murphy@juneau.gov>  
**Sent:** Monday, March 10, 2025 2:31 PM  
**To:** Theresa Ross <Theresa.Ross@juneau.gov>; Jeffrey Hedges <Jeffrey.Hedges@juneau.gov>; Shawn Williams <Shawn.Williams@juneau.gov>  
**Subject:** RE: USE25-06: Dog Grooming Salon Agency Comments

Good afternoon,

Just wanted to follow up on this Use Permit. If you have any comments or have no concerns, please let me know.

Thank you,

**Jolene Murphy | Planner I**

[Community Development Department](#) | City & Borough of Juneau, AK

Location: 230 S. Franklin Street | 4<sup>th</sup> Floor Marine View Building

Office: 907.586.0753 x4131



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---

**From:** Jolene Murphy  
**Sent:** Wednesday, February 19, 2025 12:00 PM  
**To:** Theresa Ross <[Theresa.Ross@juneau.gov](mailto:Theresa.Ross@juneau.gov)>; General Engineering <[General\\_Engineering@juneau.gov](mailto:General_Engineering@juneau.gov)>; Jeffrey Hedges <[Jeffrey.Hedges@juneau.gov](mailto:Jeffrey.Hedges@juneau.gov)>  
**Subject:** USE25-06: Dog Grooming Salon Agency Comments

Hello,

We recently received a USE permit application to start a dog grooming salon at 2217 Dunn St. I've attached the application and pre-application report to this email.

If you have any questions or concerns, please reach out to me by March 6th. Let me know if you need an extension.

Thank you!

March 20th

**Jolene Murphy | Planner I**  
[Community Development Department](#) | City & Borough of Juneau, AK  
Location: 230 S. Franklin Street | 4<sup>th</sup> Floor Marine View Building  
Office: 907.586.0753 x4131



*Fostering excellence in development for this generation and the next.*

**From:** [Theresa Ross](#)  
**To:** [Jolene Murphy](#)  
**Subject:** RE: USE25-06: Dog Grooming Salon Agency Comments  
**Date:** Monday, March 10, 2025 3:43:18 PM  
**Attachments:** [image002.png](#)

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No concerns from fire at this time.

Theresa Ross, Fire Marshal  
Capital City Fire Rescue  
820 Glacier Avenue  
Juneau AK 99801  
907-586-5322 ext. 4323  
<https://www.juneau.org/fire>



---

**From:** Jolene Murphy <Jolene.Murphy@juneau.gov>  
**Sent:** Monday, March 10, 2025 2:31 PM  
**To:** Theresa Ross <Theresa.Ross@juneau.gov>; Jeffrey Hedges <Jeffrey.Hedges@juneau.gov>; Shawn Williams <Shawn.Williams@juneau.gov>  
**Subject:** RE: USE25-06: Dog Grooming Salon Agency Comments

Good afternoon,

Just wanted to follow up on this Use Permit. If you have any comments or have no concerns, please let me know.

Thank you,

**Jolene Murphy | Planner I**

[Community Development Department](#) | City & Borough of Juneau, AK  
Location: 230 S. Franklin Street | 4<sup>th</sup> Floor Marine View Building  
Office: 907.586.0753 x4131



*Fostering excellence in development for this generation and the next.*

---

**From:** Jolene Murphy

**Sent:** Wednesday, February 19, 2025 12:00 PM  
**To:** Theresa Ross <[Theresa.Ross@juneau.gov](mailto:Theresa.Ross@juneau.gov)>; General Engineering <[General\\_Engineering@juneau.gov](mailto:General_Engineering@juneau.gov)>; Jeffrey Hedges <[Jeffrey.Hedges@juneau.gov](mailto:Jeffrey.Hedges@juneau.gov)>  
**Subject:** USE25-06: Dog Grooming Salon Agency Comments

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Thank you!

March 20th

**Jolene Murphy | Planner I**

[Community Development Department](#) | City & Borough of Juneau, AK  
Location: 230 S. Franklin Street | 4<sup>th</sup> Floor Marine View Building  
Office: 907.586.0753 x4131



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**From:** [Bridget LaPenter](#)  
**To:** [Jolene Murphy](#); [Theresa Ross](#); [General Engineering](#); [Jeffrey Hedges](#)  
**Subject:** RE: USE25-06: Dog Grooming Salon Agency Comments  
**Date:** Wednesday, February 19, 2025 12:11:41 PM

---

General engineering has no comments.

**Bridget LaPenter, P.E. | Chief GE Engineer**  
General Engineering Department | City & Borough of Juneau, AK  
Location: 230 S. Franklin Street, 4<sup>th</sup> Floor Marine View Building  
**Office: 907.586.0800 ext. 4187 Cell: 907.500.4170**

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**From:** Jolene Murphy <Jolene.Murphy@juneau.gov>  
**Sent:** Wednesday, February 19, 2025 12:00 PM  
**To:** Theresa Ross <Theresa.Ross@juneau.gov>; General Engineering <General\_Engineering@juneau.gov>; Jeffrey Hedges <Jeffrey.Hedges@juneau.gov>  
**Subject:** USE25-06: Dog Grooming Salon Agency Comments

Hello,

We recently received a USE permit application to start a dog grooming salon at 2217 Dunn St. I've attached the application and pre-application report to this email.

If you have any questions or concerns, please reach out to me by March 6th. Let me know if you need an extension.

Thank you!

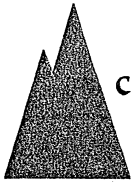
March 20th

**Jolene Murphy | Planner I**

[Community Development Department](#) | City & Borough of Juneau, AK  
Location: 230 S. Franklin Street | 4<sup>th</sup> Floor Marine View Building  
Office: 907.586.0753 x4131



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**CITY/BOROUGH OF JUNEAU**  
**ALASKA'S CAPITAL CITY**

**PLANNING COMMISSION**  
**NOTICE OF DECISION**

Date: April 18, 2006

File No.: USE2003-00049

Richard Harris  
PO Box 32403  
Juneau, AK 99803

Application For: An Allowable Use permit for approximately 20,000 square feet of retail and commercial development on four lots.

Legal Description: JW McKinley Block 32 JW McKinley Block 32 Lots 5A-10

Parcel Code Nos.: 5-B16-0-113-003-1, 5-B16-0-113-004-2 through 004-5

Hearing Date: April 18, 2006

The Planning Commission, at its regular public meeting, adopted the analysis and findings listed in the attached memorandum dated April 5, 2006 and approved the construction of approximately 20,000 square feet of retail and commercial development on four lots to be conducted as described in the project description and project drawing submitted with the application and with the following modified conditions:

- 1) Prior to issuance of a building permit, the applicant shall submit a planting plan for the project to be approved by the Director of the Community Development Department. The planting plan shall maintain existing mature evergreen trees on the applicant's property and include trees between buildings where practical.
- 2) Prior to issuance of a Certificate of Occupancy, or Temporary Certificate of Occupancy, plants and landscaping described in the approved planting plan described above shall be installed.
- 3) Prior to issuance of a Building Permit, the applicant shall submit a lighting plan which clearly shows that light fixtures in this development will not shed glare on adjacent residential properties.
- 4) Prior to issuance of a Certificate of Occupancy, easements for permanent parking and pedestrian access to these parking spaces must be recorded on Lots 5A and 9A to provide required parking for Lots 7A and 10A.
- 5) Prior to issuance of a Certificate of Occupancy, a new plat must be recorded depicting required access easements and adjusting property lines as needed.
- 6) The applicant shall submit a drainage plan to be reviewed and approved by the CBJ Engineering Department prior to issuance of a Building Permit. This drainage plan shall clearly show how all surface drainage for lots 5A through 10A will be directed to an approved drainage way.
- 7) Prior to issuance of a Building Permit, the applicant shall submit plans to CDD staff for the design of a minimum eight-foot high, sight-obscuring fence to extend along the rear property lines of Lots 5A, 7A, 9A and 10A. The developer may leave an opening for pedestrians in the fence where the

155 So. Seward Street, Juneau, Alaska 99801-1397

Richard Harris  
File No.: USE2003-00049  
April 18, 2006  
Page 2 of 2

developer's property abuts the Eagle Street right-of-way. Prior to issuance of a Certificate of Occupancy, or Temporary Certificate of Occupancy, this fence must be in place.

- 8) Prior to issuance of a Building Permit, the applicant shall revise the site plan to reflect a 20-foot setback along the entire western property line of the development.

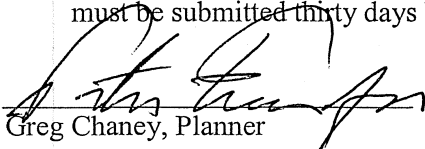
Attachments: April 5, 2006 memorandum from Greg Chaney, Community Development to the CBJ Planning Commission regarding USE2003-00049.

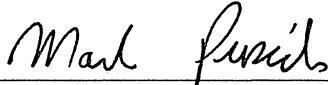
This Notice of Decision constitutes a final decision of the CBJ Planning Commission. Appeals must be brought to the CBJ Assembly in accordance with CBJ §01.50.030. Appeals must be filed by 4:30 P.M. on the day twenty days from the date the decision is filed with the City Clerk, pursuant to CBJ §01.50.030 (c). Any action by the applicant in reliance on the decision of the Planning Commission shall be at the risk that the decision may be reversed on appeal (CBJ Sec. 49.20.120).

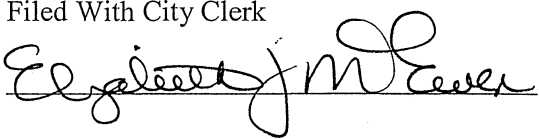
This Notice of Decision does not authorize any construction activity. Prior to starting any development project, it is the applicants responsibility to obtain a building permit for any, and all improvements requiring such.

Effective Date: The permit is effective upon approval by the Commission, April 18, 2006.

Expiration Date: The permit will expire 18 months after the effective date or October 18, 2007, if no Building Permit has been issued and substantial construction progress has not been made in accordance with the plans for which the development permit was authorized. Application for permit extension must be submitted thirty days before the permit expires.

Project Planner:   
Greg Chaney, Planner

  
Mark Pusich, Chairman  
Planning Commission

Filed With City Clerk  
  
April 28, 2006

cc: Plan Review

NOTE: The Americans with Disabilities Act (ADA) is a federal civil rights law that may affect this development project. ADA regulations have access requirements above and beyond CBJ - adopted regulations. Owners and designers are responsible for compliance with ADA. Contact an ADA - trained architect or other ADA trained personnel with questions about the ADA: Department of Justice (202) 272-5434, or fax (202) 272-5447, NW Disability Business Technical Center 1 (800) 949-4232, or fax (360) 438-3208.

# MEMORANDUM

CITY/BOROUGH OF JUNEAU  
155 South Seward Street, Juneau, Alaska 99801

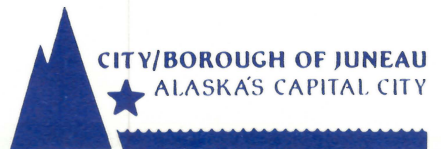
**DATE:** April 5, 2006  
**TO:** Planning Commission  
**FROM:** Greg Chaney, Planner  
Community Development Department  
**FILE NO.:** USE2003-00049 - Allowable Use



**PROPOSAL:** An Allowable Use permit for a four building complex with mixed office, retail and commercial uses and 2 residential units. Variance requests for back-out parking on Dunn Street are also associated with this permit.

### GENERAL INFORMATION

**Applicant:** RH Development  
**Property Owner:** Richard Harris  
**Property Addresses:** 9421 Glacier Highway and 2203, 2219, 2221, 2223, 2225 Dunn Street  
**Legal Descriptions:** JW McKinley Subdivision, Block 32, Lots 5A-10  
**Parcel Code Numbers:** 5-B16-0-113-003-1; 5-B16-0-113-004-2 through 004-4  
**Site Size:** 33,930 square feet  
**Zoning:** Light Commercial  
**Utilities:** CBJ Water and Sewer  
**Access:** Dunn Street  
**Existing Land Use:** Maintenance Shop, Retail Space, and 2 Caretaker Style Apartments  
**Surrounding Land Use:** North - Light Commercial, Muffler Shop  
South - Light Commercial  
East - Light Commercial, Dunn Street  
West - D-5, Single Family Residential



Planning Commission  
File No.: USE2003-00049  
April 5, 2006  
Page 2 of 7

**PROJECT DESCRIPTION**

The applicant requests an Allowable Use permit for the development of a complex of mixed use buildings, which will incorporate a variety of uses including retail, commercial and offices (Attachments 1, 2 & 3). Two existing caretaker-style units, which are already present, will also remain in the complex. The applicant also desires to incorporate a restaurant in the building on Lot 7A, however, this use requires a Conditional Use Permit. Adequate public notice was not provided for a Conditional Use Permit application. Therefore, the applicant is proceeding with the proposal with the understanding that the restaurant use will require a separate Conditional Use in the future.

A further complication is that the existing lots are relatively small for the proposal and the buildings are large enough that there is very limited space available for parking. The applicant proposes to address this problem by installing parking spaces which back-out onto the Dunn Street right-of-way. Since back-out parking is not allowed for commercial or multifamily uses under the Land Use Code, the applicant has submitted Variance requests for back-out parking to be considered concurrently with this proposal. Since two of the lots are not large enough to accommodate required parking, parking will be provided on adjacent properties. Access easements will also be required to ensure access to parking on adjacent lots.

As proposed, the application will require minor subdivision to adjust lot lines. A significant component of this subdivision will be establishing access easements between lots 5A and 7A as well as lots 9A and 10A.

**BACKGROUND**

The J.W. McKinley subdivision is one of the oldest small-lot subdivisions in the Mendenhall Valley. Although they were subdivided into 15 separate parcels, they have remained primarily under common ownership until the present day. Lots being considered under this proposal are accessed via Dunn Street which was originally platted as a 30-foot wide alley. This alley was later named “Dunn Street” and although it is the sole legal access to the property, the area primarily consists of a hard packed dirt/gravel surface. The right-of-way generally functions as a dirt access aisle through a parking lot.

**ANALYSIS**

**Project Site** – As mentioned above, the project site consists of four lots. Access to the project will be partially provided through access easements between lots 5A and 7A as well as lots 9A and 10A.

Another major feature of this project is the Dunn Street right-of-way. Since Dunn Street was originally platted as an alley, it is only 30 feet wide. Thirty three parking spaces are proposed to back-out into Dunn Street. Several lots are located across Dunn Street. If this proposal is approved, lots on the other side of Dunn Street are likely to use this project as a template and request variances to allow back out parking.

Adjacent property owners have called staff expressing concerns about the intensity of the project and one letter has been submitted (See Attachment 4). Along the western edge of the project is a wooden fence. Large gaps are present between the boards so the fence does not function as a significant sight buffer.



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Property on the other side of the fence is zoned D-5, single-family residential. An established residential neighborhood has been built here. If parking lots are constructed as proposed, headlights will shine into the neighboring yards of established single family residences. In order to reduce impacts to adjacent properties, staff is recommending a condition that prior to issuance of a building permit, the applicant shall submit plans to CDD staff for the design of a minimum eight foot high sight obscuring fence to extend along the rear property lines of Lots 5A, 7A, 9A and 10A. Prior to issuance of a Certificate of Occupancy or Temporary Certificate of Occupancy, this fence must be in place.

**Project Design** –The existing properties consist of relatively small lots, which make significant development on individual lots difficult. When the required 10 foot side yard setbacks, 15% vegetative cover and parking requirements are taken into account, very little land is available for construction.

Normally in a Light Commercial zone the rear setback is 10 feet. However CBJ 49.25.400 Table of Dimensional Standards; Footnote 3 stipulates: *Where one district abuts another, the greater of the two setbacks is required for both uses on the common property line.* In this case, the rear setback for a D-5 single-family residential district is 20 feet, so the rear setback for the project along the common property line is 20 feet. Based on the drawing submitted (Attachment 1), it appears that the proposed buildings are currently designed to be closer than 20 feet to the rear property line. Therefore, staff is recommending a condition requiring that prior to issuance of a building permit, the applicant shall revise the site plan to reflect the required 20 foot rear yard setback.

Adjacent residents in the D-5 residential zone have expressed the concern that their yards will be in the direct view plane of occupants looking out of west facing windows in the upper stories of commercial/residential structures. Therefore, staff is recommending a condition requiring that second story windows facing west along the residential property line must be glazed with frosted or other similarly translucent material.

**Drainage** – Currently the site is graded so that all surface runoff is directed to a ditch which runs along the rear property line. This ditch does not currently connect to an approved drainage way and during heavy precipitation events, standing water overflows onto adjacent properties. In order to address this issue, staff is recommending a condition that prior to issuance of a building permit, the applicant shall submit a drainage plan to be reviewed and approved by the CBJ Engineering Department. This drainage plan shall clearly show how all surface drainage will be directed to an approved drainage way.

**Traffic** – Traffic from the proposed development will enter and exit the site on the Dunn Street right-of-way. Dunn Street is not maintained by the CBJ. It is constructed to a rather low standard since it currently serves only a few scattered buildings. Since the applicant owns all of the other properties which front on Dunn Street, no other individuals will suffer consequences from additional traffic along this substandard right-of-way. Staff has recommended that the applicant consider vacating this right-of-way since no other owners share frontage on this road. The applicant wishes to maintain the option of selling existing lots to individual purchasers and this could only be accomplished if the existing lots continue to front on a dedicated right-of-way.

Dunn Street connects to Glacier Highway. Glacier Highway has enough capacity to handle the potential traffic load generated by this proposal.

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**Parking** – Total parking demand for the project has been calculated below:

Residential Parking Requirements  
 2 bedroom = 1.75 parking spaces

Retail                1 parking space per 200 square feet  
 Offices                1 parking space per 300 square feet

**Parking Requirements**

	<u>Office</u>	<u>Retail</u>	<u>Residential</u>	<u>Total</u>
Lot 5A	7.4	8.5	0	15.9
Lot 7A	7.3	20.0	0	27.3
Lot 9A	0	23.0	0	23.0
Lot 10A	0	11.4	3.5	15.0

**Parking Distribution**

	<u>Provided</u>	<u>Required</u>	<u>Difference</u>
Lot 5A	19.0	15.9	+3.0
Lot 7A	19.0	27.3	-8.3
Lot 8	38.0	23.0	+15.0
Lot 9	12.0	15.0	-3.0
Project Total	88	81	+7 Project Total

Based on the calculations above, parking demand for the project, as a whole, will be provided. However, parking requirements will be met for Lots 7A and 10A on spaces available on Lots 5A and 9A. Therefore, staff is recommending a condition that prior to issuance of a Certificate of Occupancy, permanent parking easements must be recorded on Lots 5A and 7A sufficient to provide required parking. Pedestrian access from property to the required off-site parking spaces must also be provided.

**Circulation** – Access easements will be required for shared parking lot entrances. Needed vehicular access easements are listed below:

<b>Property</b>	<b>Required Access Easements</b>
Lot 5A	Lot 7A
Lot 7A	Lot 5A
Lot 9A	Lot 10A
Lot 10A	Lot 9A

Due to the complex nature of these overlapping easements, staff is recommending a condition that prior to issuance of a Certificate of Occupancy, a new plat must be recorded depicting required access easements. Re-platting will also enable the developer to adjust property lines to accommodate vegetative cover requirements.

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Parking, which backs out directly into the Dunn Street right-of-way, is a significant project design feature of the proposal. Back-out parking for commercial and multifamily uses is not allowed under Section 49.40.230 (b)(1) of the Land Use Code. The request for approval of this parking configuration is being considered separately under cases VAR2003-00041, VAR2005-00035 and VAR2005-00039. Significant parking lot redesign will be required if these Variances are not granted.

**Landscaping** – All lots are required to have 15% vegetative cover. As proposed, all lots meet this requirement. Along the rear property line, staff is recommending a sight obscuring fence be constructed to provide a buffer between this commercial development and the adjacent residential property. To enhance this buffer, staff is recommending a condition requiring the applicant to plant spruce or hemlock trees every 15 feet along the rear property line. Although these trees will not provide a buffer in the short term, as they mature, they will provide an enhanced buffer.

**Exterior Lighting** – A lighting plan was not submitted with the application. The parking lots are relatively small and near the main structures, building-mounted lighting will be sufficient to illuminate the parking lots. Since a lighting plan was not provided, staff is recommending a condition that prior to building permit approval, the applicant shall submit a lighting plan. This plan must be approved by staff to ensure that light fixtures in this development do not shed glare on adjacent properties.

**Signs** - Signs shown on the submittal are for illustrative purposes only. A separate permit process per CBJ §49.45, requires that all signs proposed by the development be submitted to CDD staff for review and approval.

**Juneau Coastal Management Program** - The proposed development is located on a relatively level dirt/gravel parking lot. It is not within 50 feet of an anadromous stream, eagle nest or mapped wetlands. The application was reviewed for compliance with CBJ §49.70.900, the Juneau Coastal Management Program. The analysis reveals that no aspect of the Juneau Coastal Management Program applies to this project.

**FINDINGS**

CBJ §49.15.320 (e), Decision, states that the Planning Commission shall consider the Allowable Use permit application and shall review the Community Development Director's recommendation with respect to:

1. Whether the application is complete;
2. Whether the requested permit is appropriate according to the Table of Permissible Uses;
3. Whether the development as proposed will comply with the other requirements of this chapter; and,
4. Whether conditions are necessary for approval<sup>1</sup>.

The Commission shall approve the application and grant the permit unless it finds, by a preponderance of the evidence, that one or more of the criteria have not been met. In either case the Commission shall adopt written findings setting forth the basis for its decision.

<sup>1</sup>CBJ §49.15.320 (f)(1 through 12), Conditions on Approval, lists a set of conditions that may be placed on the requested allowable use permit approval

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**Per CBJ §49.15.320 (e)(1 through 4), Decision, the Director makes the following findings on the criteria for granting the requested Allowable Use approval:**

**1. *Is the application for the requested allowable use permit complete?***

**Yes.** We find the application contains the information necessary to conduct a full review of the proposed operations. The application submittal by the applicant, including the appropriate fees, substantially conforms to the requirements of CBJ Chapter 49.15.

**2. *Is the requested permit appropriate according to the Table of Permissible Uses?***

**Yes.** The requested Allowable Use permit is appropriate according to the Table of Permissible Uses. The permit is listed at CBJ §49.25.300 Section 1.300 Multifamily dwellings at a rate of 18 units per acre, Section 2.120 Miscellaneous Indoor Retail and Section 3.400 Office Uses for the Light Commercial Zoning District.

**3. *Will the proposed development comply with the other requirements of this chapter?***

**If Associated Variances Are Granted: Yes.** This proposal will require Variances from CBJ §49.40.230 (b)(1) prohibiting back out parking in order to be permitted. These Variances include:

- VAR2003-00041 – Lot 5A Back out parking on to Dunn Street right-of-way
- VAR2005-00035 – Lot 7A Back out parking on to Dunn Street right-of-way
- VAR2005-00039 – Lots 9A & 10A Back out parking on to Dunn Street right-of-way

These Variance applications have been submitted for review concurrently with this Allowable Use application.

Other than the issues listed above, the proposal complies with the requirements of the Land Use Code and could be approved. The proposed development complies with other requirements of this chapter as a notice was published in the Juneau Empire under “Your Municipality” on March 31 and April 10, 2006. A meeting notice of this proposal was mailed to all property owners within 500 feet of the subject property.

If the above Variances are granted, staff recommends approval of the project with appropriate conditions.

**4. *Are conditions necessary for approval of the requested Allowable Use permit?***

**Yes.** Based on the preceding staff analysis, it is found that conditions are necessary for approval of the requested permit. The conditions are listed below under the “Recommendation” section.

**Per CBJ §49.70.900 (b)(3), General Provisions, the Director makes the following Juneau Coastal Management Program consistency determination:**

**5. *Will the proposed development comply with the Juneau Coastal Management Program?***

**Not Applicable.** Based on the preceding staff analysis, it is found that no provisions of the Juneau Coastal Management Program apply to the proposed development.

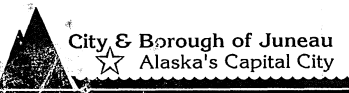
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### **RECOMMENDATION**

If the requested Variances, VAR2003-00041, VAR2005-00035 and VAR2005-00039 are granted, then staff recommends granting the Allowable Use permit for development of a four-building complex with mixed office, retail and commercial uses and 2 residential units.

Approval would be subject to the following conditions:

- 1) Prior to Certificate of Occupancy, the applicant shall plant spruce or hemlock trees every 15 feet along the west property line.
- 2) Prior to issuance of a Building Permit, the applicant shall submit a lighting plan which clearly shows that light fixtures in this development will not shed glare on adjacent residential properties.
- 3) Prior to issuance of a Certificate of Occupancy, easements for permanent parking and pedestrian access to these parking spaces must be recorded on Lots 5A and 9A to provide required parking for Lots 7A and 10A.
- 4) Prior to issuance of a Certificate of Occupancy, a new plat must be recorded depicting required access easements and adjusting property lines as needed.
- 5) The applicant shall submit a drainage plan to be reviewed and approved by the CBJ Engineering Department prior to issuance of a Building Permit. This drainage plan shall clearly show how all surface drainage for lots 5A through 10A will be directed to an approved drainage way.
- 6) Prior to issuance of a Building Permit, the applicant shall submit plans to CDD staff for the design of a minimum eight-foot high, sight-obscuring fence to extend along the rear property lines of Lots 5A, 7A, 9A and 10A. Prior to issuance of a Certificate of Occupancy or Temporary Certificate of Occupancy, this fence must be in place.
- 7) Prior to issuance of a Building Permit, the applicant shall revise the site plan to reflect the required 20-foot rear yard setback.
- 8) Second story windows facing west along the residential property line must be glazed with frosted or other similarly translucent material.



# BUILDING PERMIT

Section H, Item 1.

Permit No.  
**BLD2006-00679**

Proj #: PRJ2003-00226

NOTE: "Building Permit" is a generic term which includes Building Safety Inspection, Grading Permits, and permits for Electrical, Plumbing and Mechanical work.

**Your special attention is called to the following:**

This permit is granted on the express conditions that the construction shall, in all respects, conform to the ordinances of the City and Borough of Juneau. It may be revoked at any time upon violation of any of said ordinances.

The granting of this permit does not authorize the violation of any federal, state or local law regulating construction for the violation of the terms of any deed or covenant or any zoning or other regulation.

If plan review was required, this permit must be attached to the approved drawings. The permit, plans and record of inspections must be available on site at all times while the construction is in progress and before final inspection.

The yellow posting notice must be prominently displayed to show a permit has been issued and to assist the inspectors in location of the project. This permit becomes null and void if work or construction authorized is not commenced within one year or if work or construction is suspended or abandoned for a period of one year at any time after work has commenced.

Note: City Ordinances REQUIRE a Final Inspection be approved for every Building Permit.

**Inspections**

Inspections can be arranged by telephoning 586-1703 or by written or faxed notification.

The Online Building Inspection Request Form is at: [www.juneau.org/permits/inspect\\_request.php](http://www.juneau.org/permits/inspect_request.php).

Work shall not proceed until the inspector has approved the various stages of construction. An approved Final Inspection is required.

Call before 7:30 AM for same day inspections.

Please provide the following information: 1 Permit Number, 2 Address, 3 Type of Inspection, 4 Date and Time and 5 Contact Name and Phone Number.

Job Address: <b>2203 DUNN ST</b>	Issued Date: <b>7/8/2008</b>
Permit Number: <b>BLD2006-00679</b>	Parcel No: <b>5B1601130046</b>
Project Description: <b>Construct a new two-story commercial building and an 8 foot fence. Modified 12/17/2007 to change layout on first and second floors. Modified 7/8/08 Tenant improvement for unit 2213, Rain Tree Quilting retail store.</b>	

Parcel Identification: **J W MCKINLEY BL 32 LT 8A**

Setbacks:	Zone: LC	Firm Zone: C
Front: 25 Ft. E	Side 1: 10 Ft. N	
Rear: 20 Ft. W	Side 2: 10 Ft. S	
Comments: Rear setback reduced per USE2003-00049		

Owner: **R H DEVELOPMENT LLC**  
 Address: **PO BOX 32403**  
 City: **JUNEAU, AK 99803**

Applicant: **RICHARD HARRIS**  
 Address: **PO BOX 32403**  
 City: **JUNEAU, AK 99803**

Fee Type	Date	Receipt	Amount Paid
1 Commercial Plan Revi	0/31/2006	23534	2,045.90
1 Permit Fee	8/7/2007	24961	3,147.54
1 Additional Plan Review	1/22/2008	25696	75.00
Total Fees Paid:			<b>5,268.44</b>

Valuation for Permit Fee Calculations:		
BUILDING PERMIT	MERC - Retail	<u>614,246.40</u>
Total Valuation:		<b>614,246.40</b>

- Project Conditions and Holds:
- 1 Pans required for all water heaters if damage to construction can occur due to leakage.
  - 2 Plumbing per 2003 UPC & Mechanical per 2003 IMC. See 1/8/08 stamped plans.
  - 3 Special inspection by qualified engineer for awning welds. And provide final letter of approval before TCO.
  - 4 Foundation Setback Verification (yellow form) must be on site when pour inspection or placement of other foundation systems occurs.
  - 5 Grade to drain - field verify.
  - 6 Emergency lighting inspection.
  - 7 Hydrants - 2,000 GPM available within 400 feet of the building.
  - 8 Modified 7/30/07 for construction of an 8 ft fence.
  - 9 Prior to issuance of CO or TCO, buffer fence must be in place as designed in the fence plans.
  - 10 Prior to issuance of CO, easements for permanent parking and pedestrian access to these parking spaces must be recorded on Lots 5A & 9A to provide required parking for Lots 7A & 10A.
  - 11 Prior to issuance of CO or TCO, plants and landscaping must be installed as described in the Planting Plan.
  - 12 Asbuilt Survey Required before final inspection approval.
  - 13 All tub & shower valves must be anti-scald and set not to exceed 120 degrees F. Hot water shall be equal or greater to 110 degrees F.
  - 14 Seismic strap upper 1/3 of water heater to wall.
  - 15 All steam & hot water boilers SHALL be protected with a low-water cutoff control per 2003 IMC 1007 & 2003 IRC M2002.5

JUNEAU PERMIT CENTER - 230 S. Franklin Street - 4th Floor, Marine View Center - Mail: 155 S. Seward Street, Juneau, AK 99801  
 Phone: 586-0770 - FAX: 586-3365 - Inspection Requests: 586-1703 - Email: [permits@ci.juneau.ak.us](mailto:permits@ci.juneau.ak.us)  
 Web Site: [www.juneau.org/permits](http://www.juneau.org/permits)



# BUILDING PERMIT\*

Permit No. **BLD2006-00679**  
Page No. 2

\* NOTE: "Building Permit" is a generic term which includes Building Safety Inspection, Grading Permits, and permits for Electrical, Plumbing and Mechanical work.

- 16 Floor-ciling hatch to mechanical room to be 1-HR rated self-closing assembly.
- 17 Draft stop in attic per plans.
- 18 Exterior stairs to be Heavy Timber per approved plans(metal grates are non-combustible, but do not qualify as heavy timber).
- 19 special inspect'n for awning welds unless waived by engineer due to welded at certified shop
- 20 Service sink(s) outside of restroom or janitor room cannot be over 34" high.
- 21 Retail must be separated by permanent wall in center of building per approved plans - otherwise occupant load exceeds 50 and exit doors would have to swing out.
- 22 Interior stairwell lid to be built as shaft, per plan: layer of rock above and below its ceiling joists.
- 23 At final inspection have approved plans at site. Check for Maneuvering Clearances at Doors on back landings, min 48" between handrails, etc.
- 24 Retail doors can only swing in if the area is permanently partitioned in half as shown on the approved plans (partition keeps occupant load under 50).
- 25 Blue parking - Min two blue spaces this lot - and one for adjacent lot. Min one on this lot to be Van-Accessible parking space: 8' space with 8' aisle adjacent. Post sign 48"-72" high ; include words "Van accessible. Paint; or if unpaved, secure 2 blue wheelstopes - one in front of space and other in front of aisle. Space and aisle to be level.
- 26 Wall must separate first floor into two separate rooms as shown, or if not then two exit doors separated by at least 1/2 the building's diagonal must swing out.
- 27 Minimum 48" clear between handrails for both stairs (see IBC 1007.6).
- 28 Two-way communication from boht areas of refuge upstairs (see IBC 1007.6.3 and .4).
- 29 Corridor upstairs: doors 20-minute assemblies (except the two 1-HR doors at top of interior stairs).
- 30 Interior 1-HR stairwell: both doors 1-HR assemblies at top of stairs.
- 31 Sidewalk must be such that all tenant spaces have sidewalk route to the restrooms.
- 32 Objects protruding over 4" in depth into a path of travel whose bottom edge is over 27" high and less than 80" high require a cane-detectable surface below them that's not over 27" high. A few examples: paper towel dispensers, hand dryers, extinguishers, high sides of water fountains, counters, light fixtures, etc.
- 33 Operable Parts: min 15" and not over 48" high: thermostats, switches, outlets, phones, fan controls, restroom accessories, audio controls, etc. At least one coat hook not over 48" high where hooks are installed.
- 34 Sales/Service counter, if provided, requires a section at least 36" long that's not over 36" high.
- 35 Fire Extinguishers mounted not over 5' high, max 75' OC, and not in a path of travel unless bottom of FE is not over 27" high.
- 36 ADA "EXIT" sign beside each exterior exit door: on wall on latch side 5' high OC.
- 37 Lever hardware all doors (or panic/or push-pull); thresholds not over 1/2"; Exter level landing outside doors min 5' x 5'.
- 38 ADA sign beside restroom doors: on wall on latch side 5' high OC.
- 39 Curb cuts for all sidewalks. Thresholds to be FLUSH - no bump or edge. Cross slopes max 2%. Direction of travel max 1:12.
- 40 Fire department needs documentation of Fire Hydrant Locations and GPM available within 400' of building.

Inspections Required: Call for inspection before covering or concealing any of the work described below. Inspections may be combined.

100	Insp - Setbacks	105	Foundation Setback form	110	Insp - Excavation/Footing
	Insp - Building Service	120	Insp - Forms and Rebar	130	Insp - Steel/Masonry
140	Insp - Framing	145	Insp - Special Inspection	150	Insp - Insulation/Drywall
160	Insp - Rated Walls/Assem		Insp - Bonding		Insp - Rated Walls/Assem
	Insp - Bonding	170	Insp - Smoke Detectors/Alarms		Insp - Grounding
	Insp - Smoke Detectors/Alarms		Insp - Grounding	175	Insp - Carbon Monoxide
180	Insp - Underground Utilities		Insp - Devices/Equipment	190	Insp - Water Piping Test
210	Insp - DWV Test	215	Insp - Hydronic System	220	Insp - Plumbing Fixtures Final
250	Insp - Cross Connections	255	Insp - Meter Yoke with Meter	265	Insp - Oil & Gas Piping/Tanks
300	Insp - Appliance/Chimney	310	Insp - Combustion Air	400	Insp - Ventilation/Plenums
430	Insp - Temporary Power	440	Insp - Service/Panel	450	Insp - Conduits raceways
470	Insp - Grading/Drainage	500	Insp - Fire Final		Insp - Electrical Final
600	Insp - Com Zoning Final	700	Insp - Commercial Fire Final	800	Insp - Commercial Final

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Phone: 586-0770 - FAX: 586-3365 - Inspection Requests: 586-1703 - Email: [permits@ci.juneau.ak.us](mailto:permits@ci.juneau.ak.us)  
Web Site: [www.juneau.org/permits](http://www.juneau.org/permits)



# Staff Review Sheet

Planner

7/8/2008

Permit Intake Initials:

FIRE ZONE ENG WAT SEW ARCH STRUC PLUM MECH ELEC ACCESS SPEC/INSP

Project Name: **J W MCKINLEY BL 32 LT 8A NEW**

Case No: **BLD2006-00679**

- Site Address: 2203 DUNN ST
- Site Address: 2205 DUNN ST
- Site Address: 2207 DUNN ST
- Site Address: 2209 DUNN ST
- Site Address: 2211 DUNN ST
- Site Address: 2213 DUNN ST
- Site Address: 2215 DUNN ST
- Site Address: 2217 DUNN ST

Parcel No: 5B1601130046

Desc: Construct a new two-story commercial building and an 8 foot fence. Modified 12/17/2007 to change layout on first and second floors. Modified 7/8/08 Tenant improvement for unit 2213, Rain Tree Quilting retail store.

Commercial NEW BU          New Dwelling Units: 0  
 FCC Code: 327 Existing Dwelling Units:           
 Type of Construction: VA Occupancy Class: M  
 Sprinkler Substitute for Type A Construction: YES NO  
 Sprinkler System: FULL PARTIAL NONE Required: YES NO  
 Alarm System: FULL PARTIAL NONE Required: YES NO  
 Code Edition:          Code Review by:          Date:         

**Fees Assessed at Application:**

Fee Type	Amount
1 Commercial Plan Review	2,045.90
1 Permit Fee	3,147.54
1 Additional Plan Review Fe	75.00
<b>Total Fees:</b>	<b>5,268.44</b>

**Valuation at Application:**

S.F.	Type	Rate	Amount
5760	MERC - Retail	106.64	614,246.40
<b>Total Valuation:</b>			<b>614,246.40</b>

**Fees Paid at Application:**

Date Paid	Receipt	Check	Paid
<b>10/31/2006</b>	23534	V080351	2,045.90
1 Commercial Plan Review			2,045.90
<b>8/7/2007</b>	24961	11463	3,147.54
1 Permit Fee			3,147.54
<b>1/22/2008</b>	25696	V037869	75.00
1 Additional Plan Review Fee			75.00
<b>Total Fees Paid:</b>			<b>5,268.44</b>

**LAND USE**

ZONE/UNIT Sight Comm /  
 WETLAND           
 FLOOD ELEV           
 FIRM ZONE C  
 FIRM MAP           
 LOT SIZE 16,045 SF  
0.4 ACRES  
 SETBACKS  
 Front 25  
 Rear 20  
 Side 10  
 Other           
 PARKING           
 ANADROMOUS           
 EAGLES NEST         

**ENGINEERING/PUB WORKS**

CITY WATER Units:           
 Case No:           
 Service Size:           
 Fire Line Size:           
 Metered:           
 Extended Pmt #:           
 CITY SEWER Units:           
 Case No:           
 Bond Case No:           
 Other Case No:           
 ADEC Sewer No:         

**PLAN REVIEW APPROVALS**

	Initials	Date
<input checked="" type="checkbox"/> Fire	<u>TC</u>	<u>7/8/08</u>
<input checked="" type="checkbox"/> Zoning	<u>TC</u>	<u>7/8/08</u>
<input type="checkbox"/> Engineering	<u>        </u>	<u>        </u>
<input type="checkbox"/> Water	<u>        </u>	<u>        </u>
<input type="checkbox"/> Sewer	<u>        </u>	<u>        </u>
<input checked="" type="checkbox"/> Architectural	<u>JRP</u>	<u>7-8-08</u>
<input checked="" type="checkbox"/> Structural	<u>JRP</u>	<u>7-8-08</u>
<input type="checkbox"/> Plumbing	<u>        </u>	<u>        </u>
<input type="checkbox"/> Mechanical	<u>        </u>	<u>        </u>
<input checked="" type="checkbox"/> Electrical	<u>JRP</u>	<u>7-8-08</u>
<input checked="" type="checkbox"/> Access	<u>CRF</u>	<u>7/8/08</u>
<input type="checkbox"/> Spcl Insp Form	<u>        </u>	<u>        </u>

**PERMIT ISSUANCE FEES**

<input type="checkbox"/> Grading Plan Review Fee	\$ <u>        </u>
<input type="checkbox"/> Adjusted Plan Review Fee	\$ <u>        </u>
<input type="checkbox"/> Fast Track Fee	\$ <u>        </u>
<input type="checkbox"/> Early Start Fee	\$ <u>        </u>
<input type="checkbox"/> Building Permit Fee	\$ <u>        </u>
<input type="checkbox"/> Water Assessment Fee	\$ <u>        </u>
<input type="checkbox"/> Sewer Assessment Fee	\$ <u>        </u>
<input type="checkbox"/> Sewer Inspection Fee	\$ <u>        </u>
<input type="checkbox"/> Grading Permit Fee	\$ <u>        </u>
<input type="checkbox"/> Driveway Permit Fee	\$ <u>        </u>
<input type="checkbox"/> Bond for <u>        </u>	\$ <u>        </u>
<input type="checkbox"/> Other <u>        </u>	\$ <u>        </u>
<b>Total Issuance Fees</b>	\$ <u>        </u>

**LAND USE PERMITS**

- SUB2007-00007
- USE2003-00049
- VAR2003-00041
- VAR2005-00033
- VAR2005-00034
- VAR2005-00035
- VAR2005-00036
- VAR2005-00037
- VAR2005-00038
- VAR2005-00039
- VAR2005-00040
- VAR2005-00041
- VAR2006-00019

I hereby acknowledge receipt of a          inch meter yoke.  
           
 Date         

**APPROVED FOR ISSUANCE**

JRP  
 Signature  
7-8-08  
 Date

**PERMIT ISSUANCE PAYMENTS**

Date	Receipt	Check	Amount
<u>        </u>	<u>        </u>	<u>        </u>	\$ <u>        </u>
<u>        </u>	<u>        </u>	<u>        </u>	\$ <u>        </u>
<u>        </u>	<u>        </u>	<u>        </u>	\$ <u>        </u>

**PARCEL TAGS:**

CONDITIONS AND HOLDS ON PERMIT: (Continued on back of sheet)





**CITY/BOROUGH OF JUNEAU  
ALASKA'S CAPITAL CITY**

**BOARD OF ADJUSTMENT  
NOTICE OF DECISION**

Date: April 19, 2006

File Nos.: VAR2003-00041;  
VAR2005-00035; and  
VAR2005-00039

Richard Harris  
PO Box 32403  
Juneau, AK 99803

Application For: Three variances to allow back-out parking on the Dunn Street right-of-way.  
Legal Description: JW McKinley Block 32 Lots 5A-10  
Parcel Code No.: 5-B16-0-113-003-1, 5-B16-0-113-004-2 through 004-5  
Hearing Date: April 18, 2006

The Board of Adjustment, at its regular public meeting, adopted the analysis and findings listed in the attached memorandum dated April 5, 2006 and approved the Variances with the following modifications:

**(C) *Be unnecessarily burdensome because unique physical features of the property render compliance with the standards unreasonably expensive;***

The site is level and open. Even though this is an old subdivision, all lots exceed minimum dimensional standards for lots in a Light Commercial zone. No unique physical features are present on the site.

Based on the analysis above, this sub-criterion **is not met.**

*or*

**(D) *Because of preexisting nonconforming conditions on the subject parcel the grant of variances would not result in a net decrease in overall compliance with the Land Use Code, CBJ Title 49, or the building code, CBJ Title 19, or both.***

Dunn Street is a substandard, non-conforming right-of-way which has not been accepted for maintenance by the City and Borough of Juneau. Since the non-conforming configuration of Dunn Street is a narrow, dead-end, low speed street and all properties served by Dunn Street are currently owned by the applicant and are under the applicant's direct control; backing out into the Dunn Street right-of-way would not reduce public safety requirements in the Land Use Code, CBJ Title 49, or the building code, CBJ Title 19.

Based on the analysis above, this sub-criterion **is met.**

Since sub-criterion D was met, **criterion 5 is met.**

155 So. Seward Street, Juneau, Alaska 99801-1397

Richard Harris  
File Nos.: VAR2003-00041, VAR2005-00035 and VAR2005-00039  
April 19, 2006  
Page 2 of 2

Back-out parking is to be conducted as described in the project description and project drawing submitted with the application and with the following conditions:

1. Prior to issuance of a building permit, the applicant shall post sufficient 15 mph speed limit signs on the Dunn Street right-of-way. The location of signage is to be approved prior to posting by Community Development and Engineering Department staff.
2. Prior to issuance of a certificate of occupancy, the applicant shall install speed bumps on the Dunn Street right-of-way. These shall be located along each lot line perpendicular to Dunn Street, for a total of three speed bumps.

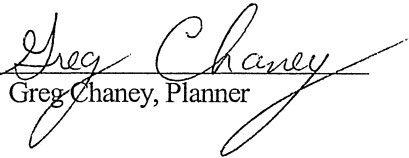
Attachment: April 5, 2006 memorandum from Greg Chaney, Community Development to the Board of Adjustment regarding VAR2003-00041; VAR2005-00035 and VAR2005-00039.

This Notice of Decision constitutes a final decision of the CBJ Planning Commission. Appeals must be brought to the CBJ Assembly in accordance with CBJ §01.50.030. Appeals must be filed by 4:30 P.M. on the day twenty days from the date the decision is filed with the City Clerk, pursuant to CBJ §01.50.030 (c). Any action by the applicant in reliance on the decision of the Planning Commission shall be at the risk that the decision may be reversed on appeal (CBJ Sec. 49.20.120).

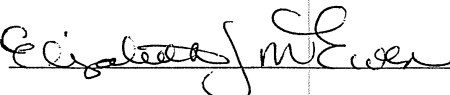
This Notice of Decision does not authorize any construction activity. Prior to starting a development project, it is the applicant's responsibility to obtain a building permit for any and all improvements requiring such.

Effective Date: The permit is effective upon approval by the Commission, April 18, 2006.

Expiration Date: The permit will expire 18 months after the effective date or October 18, 2007, if no Building Permit has been issued and substantial construction progress has not been made in accordance with the plans for which the development permit was authorized. Application for permit extension must be submitted thirty days before the permit expires.

Project Planner:   
Greg Chaney, Planner

  
Mark Pusich, Chairman  
Planning Commission

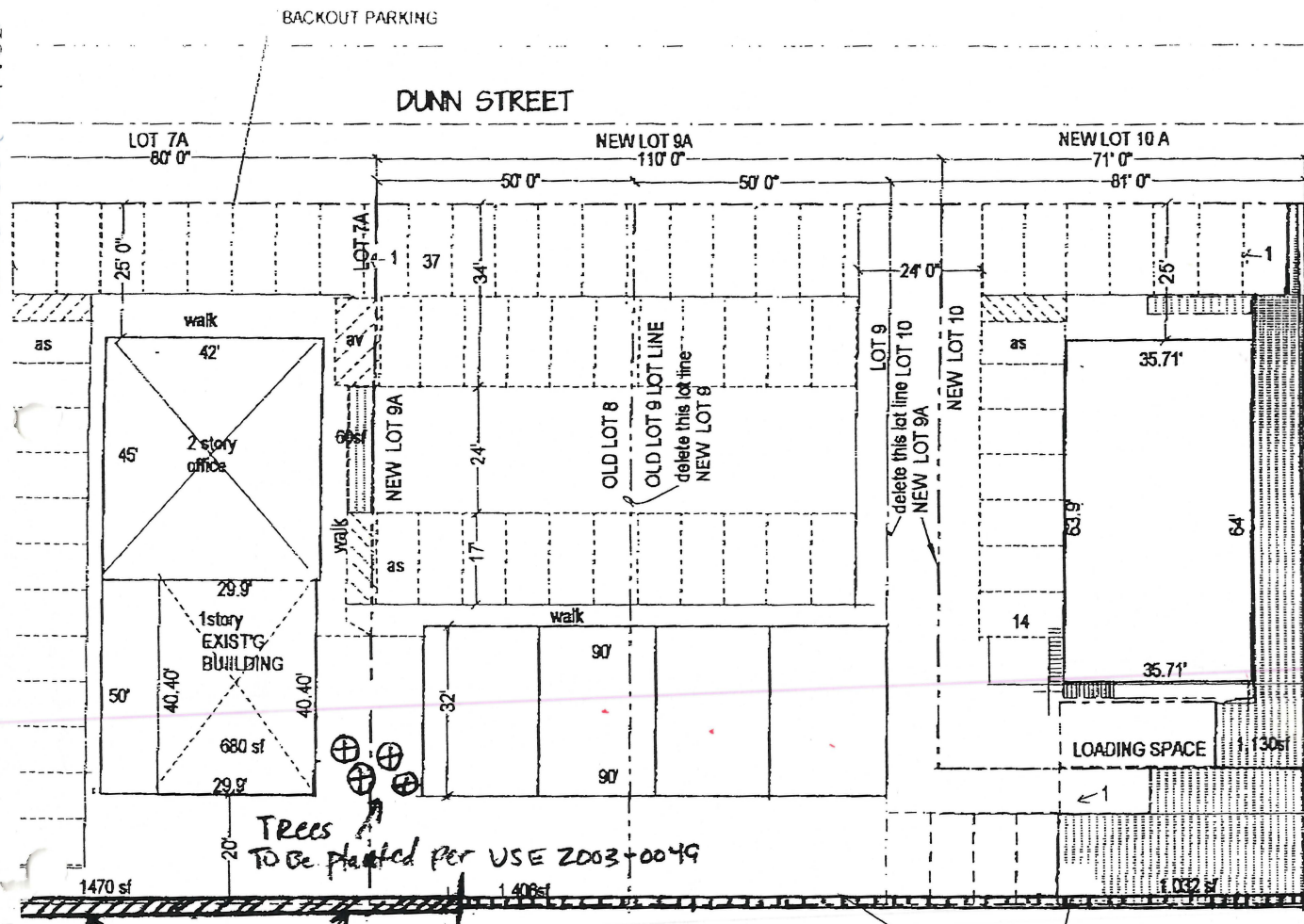
Filed With City Clerk  
  
May 2, 2006

cc: Plan Review

**NOTE:** The Americans with Disabilities Act (ADA) is a federal civil rights law that may affect this development project. ADA regulations have access requirements above and beyond CBJ-adopted regulations. Owners and designers are responsible for compliance with ADA. Contact an ADA-trained architect or other ADA trained personnel with questions about the ADA: Department of Justice - 202-272-5434, or fax 202-272-5447, NW Disability Business Technical Center - 1-800-949-4232, or fax 360-438-3208.

RECEIVED  
 MAR 23 2007  
 PERMIT CENTER / CDD

DUNN-4-16217



REVISED LOT 10 A  
 7,514 sf  
 vegetation required @ 15% = 1,127sf  
 provided 1,130sf

Parking Accessible  
 Required 4 spaces  
 (3)-as accessible space  
 (1) av av accessible space

Trees  
 TO be planted per USE 2003-0049

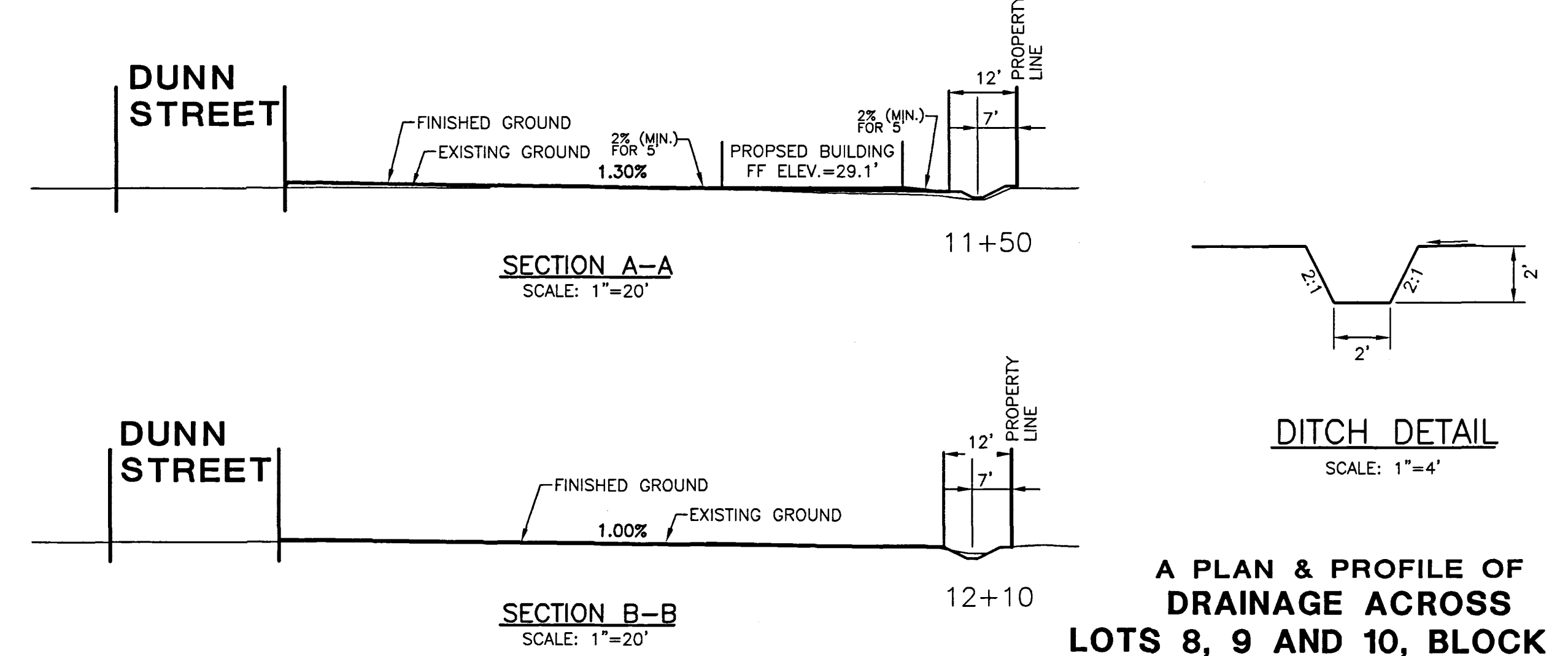
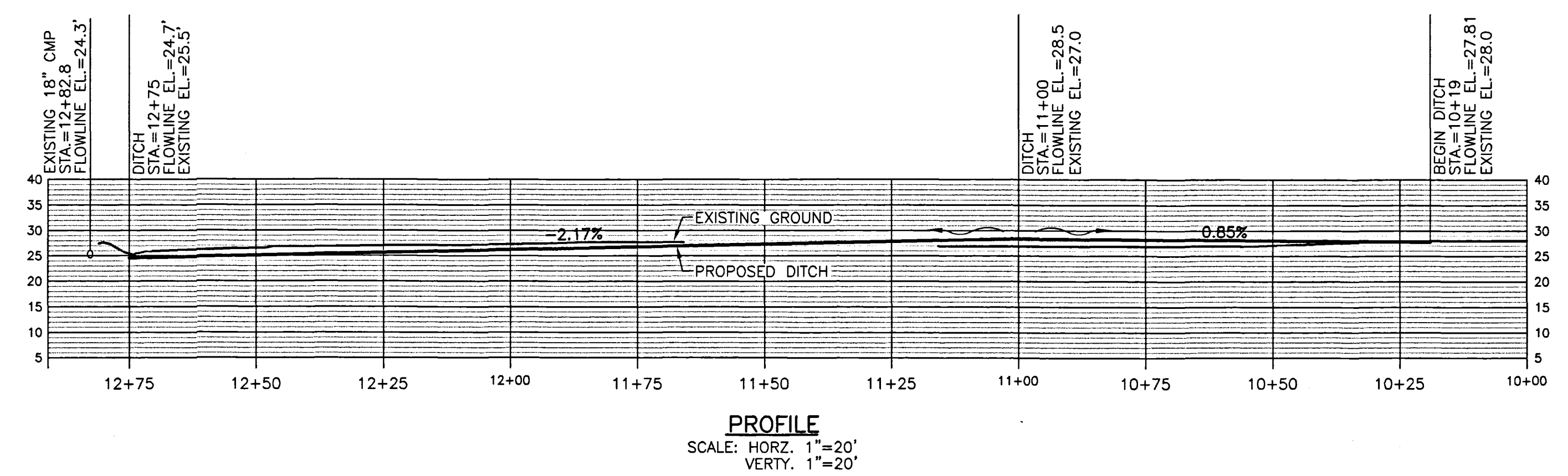
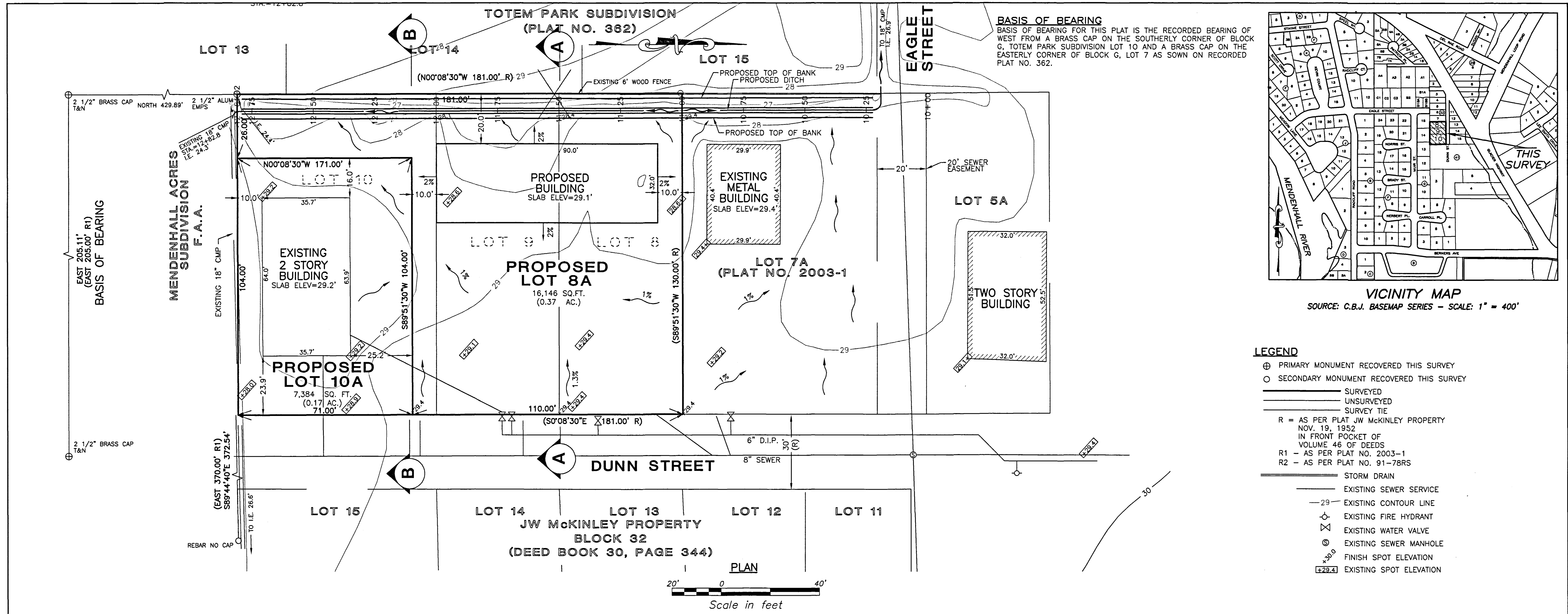
Rebuild Damaged  
 Fence to match  
 EXISTING section

ADD SLATS  
 Between EXISTING SLATS  
 To make sight obscuring

PHK-24-2007 05:39 AM  
 VTS

3/22/07

DUNN ST Lots-89  
 Fence plan Repair, Replace  
 AND TREE Planting per USE-2003  
 00049



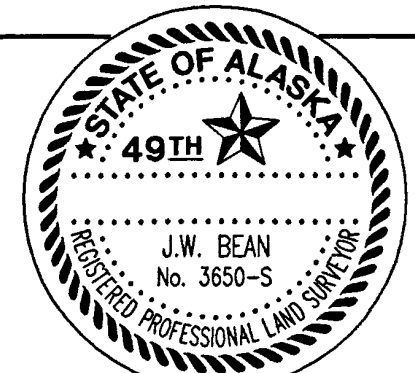
**A PLAN & PROFILE OF DRAINAGE ACROSS LOTS 8, 9 AND 10, BLOCK 32 JW MCKINLEY PROPERTY WITHIN U.S. SURVEY NO. 381 JUNEAU RECORDING DISTRICT CITY & BOROUGH OF JUNEAU, ALASKA**

OWNER: R.H. DEVELOPMENT, LLC. PO BOX 32403 JUNEAU, ALASKA 99803

SURVEYOR: JW BEAN, INC. PROFESSIONAL SURVEYOR 1070 ARCTIC CIRCLE JUNEAU, ALASKA 99801

SHEET 1 OF 1  
SCALE: AS SHOWN DATE: 6-02-07 PROJ: 10982-HARRIS-AIRPORT-P&P2

**CERTIFICATE OF REGISTERED LAND SURVEYOR**  
I HEREBY CERTIFY THAT THIS IS A TRUE AND CORRECT PLAT; THAT ALL WALKS, ROADS, EASEMENTS AND IMPROVEMENTS THEREON ARE AS SHOWN AND THAT ALL OVERLAPS AND ENCROACHMENTS ARE AS SHOWN TO THE BEST OF MY KNOWLEDGE.



**J.W. BEAN INC.**  
PROFESSIONAL SURVEYOR  
1070 ARCTIC CIRCLE  
JUNEAU - ALASKA  
(907) 789-0390  
SURVEYOR - PLANNER

- NOTES:**
1. ALL PLAT BEARINGS SHOWN ARE TRUE BEARINGS AS ORIENTED TO THE BASIS OF BEARINGS.
  2. ALL DISTANCES SHOWN ARE REDUCED TO HORIZONTAL.
  3. RECORDED BEARINGS AND DISTANCES ARE SHOWN ENCLOSED IN PARENTHESIS. MEASURED OR CALCULATED BEARINGS AND/OR DISTANCES ARE SHOWN WITHOUT PARENTHESIS.
  4. DOMESTIC WATER AND SANITARY SEWER DISPOSAL PROVIDED BY THE CITY AND BROUGH OF JUNEAU PUBLIC UTILITIES
  5. ALL SEWER AND WATER UTILITIES ARE EXISTING

RECEIVED  
JUN 04 2007  
PERMIT CENTER / CDD



# BUILDING PERMIT

Section H, Item 1.

Permit No.  
BLD20110712

\* NOTE: "Building Permit" is a generic term which includes Building Safety Inspection, Grading Permits, and permits for Electrical, Plumbing and Mechanical work.

**Your special attention is called to the following:**

This permit is granted on the express conditions that the construction shall, in all respects, conform to the ordinances of the City and Borough of Juneau. It may be revoked at any time upon violation of any of said ordinances.

The granting of this permit does not authorize the violation of any federal, state or local law regulating construction for the violation of the terms of any deed or covenant or any zoning or other regulation.

If plan review was required, this permit must be attached to the approved drawings. The permit, plans and record of inspections must be available on site at all times while the construction is in progress and before final inspection.

The yellow posting notice must be prominently displayed to show a permit has been issued and to assist the inspectors in location of the project. This permit becomes null and void if work or construction authorized is not commenced within one year or if work or construction is suspended or abandoned for a period of one year at any time after work has commenced.

Note: City Ordinances REQUIRE a Final Inspection be approved for every Building Permit.

**Inspections**

Inspections can be arranged by telephoning 586-1703 or by written or faxed notification.

The Online Building Inspection Request Form is at: [www.juneau.org/permits/inspect\\_request.php](http://www.juneau.org/permits/inspect_request.php).

Work shall not proceed until the inspector has approved the various stages of construction. An approved Final Inspection is required.

Call before 7:30 AM for same day inspections.

Please provide the following information: 1 Permit Number, 2 Address, 3 Type of Inspection, 4 Date and Time and 5 Contact Name and Phone Number.

<b>Job Address: 2203 DUNN ST</b> <b>Permit Number: BLD20110712</b> <b>Project Description: Convert portion of second floor retail building into two, 1 bedroom apartment units.</b>	<b>Issued Date : 12/20/2011</b> <b>Parcel No: 5B1601130046</b>
---	---

**Parcel Information : J W MCKINLEY BL 32 LT 8A**

<b>Setbacks:</b> Front: 25.00 Ft. E Rear: 20.00 Ft. W Street Side:	<b>Zone: LC:</b> Side 1: 10.00 Ft. N Side 2: 10.00 Ft. S	<b>Firm Zone: C</b>  <b>Comments: Rear yard setback 20ft instead of 10ft due to abutting D-5 rear yard setback - applies per CBJ 49.25.400 (3). 10' Access easement exist along southern lot line, and an electrical easement exists at the southeastern corner. See 2007 plat for details.</b>
---	--	---

<b>Owner :</b>	<b>R H RENTALS LLC</b> <b>PO BOX 032403</b> <b>JUNEAU AK 99803-2403</b>
----------------	---

<b>Applicant :</b>	<b>R H RENTALS LLC</b> <b>PO BOX 032403</b> <b>JUNEAU AK 99803-2403</b>
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Fee Type	Date	Receipt	Amount Paid
BLD- Bldg Permit Fee	12/09/2011	02301	\$224.28
BLD- Comm Plan Review	12/09/2011	02301	\$145.78
Total Fees Paid:			\$370.06

<b>Valuation for Permit Fee Calculations:</b>			
S.F.	Type	Rate	Amount
			12,521.16
Total Valuation:			\$12,521.16

**Project Conditions and Holds:**

**Approved Plans On Site** - CBJ approved plans must be on site and available to the inspector. Inspections will not be performed and additional fees may apply if approved plans are not available to the inspectors.

**WATER FIXTURE COUNT** - Minimum 1" Street Service and 1 1/4" Building Supply and branches for up to 78 fixture units.

**Shower Anti-scald Valve** - All shower valves and combination tub/shower valves shall provide scald and thermal shock protection by means of an approved valve that conforms to ASSE 1016 or ASME A112.18.1 / CSA B125.1. The maximum water temperature supplied to these fixtures shall be 120 degrees fahrenheit per 2009 UPC section 418.

**Seismic Strap Water Heater** - Water heaters shall be strapped within the upper 1/3 and lower 1/3 of its vertical dimension per 2009 UPC section 508.2.

**Inspections Required:** Call for inspection before covering or concealing any of the work described below. Inspections may be combined.

B-Rough Electrical	B-Service/Panel	B-Rough Plumbing
B-Vents (Bath, Dryer, Kitchen, etc.)	B-Smoke /Carbon Monoxide Detectors	B-Cross Connection Control

**JUNEAU PERMIT CENTER - 230 S. Franklin Street - 4th Floor, Marine View Center - Mail: 155 S. Seward Street, Juneau, AK 99801**  
**Phone: 586-0770 - FAX: 586-3365 - Inspection Requests: 586-1703 - Email: [permits@ci.juneau.ak.us](mailto:permits@ci.juneau.ak.us)**  
**Web Site: [www.juneau.org/permits](http://www.juneau.org/permits)**



# Staff Review Sheet

Planner  
*ESF*

Permit Intake Initials: *MS*

FIRE ZONE ENG WAT SEW ARCH STRUC PLUM MECH ELEC ACCESS SPEC/INSP

12/9/2011

Case No: **BLD20110712**

Site Address: 2203 DUNN ST

Parcel No: 5B1601130046

Desc: Convert portion of second floor retail building into two apartment units.

Commercial REP New Dwelling Units: 2  
 FCC Code: 437 Existing Dwelling Units: 0  
 Type of Construction: VA Occupancy Class: M, R-3\*  
 Sprinkler Substitute for Type A Construction: YES NO  
 Sprinkler System: FULL PARTIAL NONE Required: YES NO  
 Alarm System: FULL PARTIAL NONE Required: YES NO  
 Code Edition: *JBC 2006* Code Review by: *ESF* Date: *12/20/11*

**Valuation for Permit Fee Calculations:**

S.F.	Type	Rate	Amount
			49,536.00
			<i>3,000</i>
Total Valuation:			\$49,536.00

*7.25,900  
note 3  
\* 1 b  
Apf.*

<p><b>LAND USE</b></p> <p>ZONE/UNITS <u>LC /</u>                  WETLAND <u>/</u>                  FLOOD ELEV <u>/</u>                  FIRM ZONE <u>C</u>                  FIRM MAP <u>880C</u>                  LOT SIZE <u>16,045 SF</u>  <u>0.4 ACRES</u></p> <p><b>SETBACKS</b></p> <p>Front <u>25 E</u>                  Rear <u>20 10 W</u>                  Side <u>10 N/S</u>                  Other <u>/</u></p> <p><b>PARKING</b> <u>17 Overall</u>                  ANADROMOUS <u>/</u>                  EAGLES NEST <u>/</u></p>	<p><b>ENGINEERING/PUB WORKS</b></p> <p>Dwelling Units: _____                  CITY WATER:                  Permit #: _____                  Use: _____                  Assessment: _____                  Service Size: _____                  Line Size: _____                  CITY SEWER:                  Permit #: _____                  Use: _____                  Assessment: _____                  Fixture Units: _____</p>	<p><b>PLAN REVIEW APPROVALS</b></p> <table border="0"> <tr> <td></td> <td>Initials</td> <td>Date</td> </tr> <tr> <td><input checked="" type="checkbox"/> Fire</td> <td><i>DMS</i></td> <td><i>12/9/11</i></td> </tr> <tr> <td><input checked="" type="checkbox"/> Zoning</td> <td><i>ESF</i></td> <td><i>12-16-11</i></td> </tr> <tr> <td><input type="checkbox"/> Engineering</td> <td></td> <td></td> </tr> <tr> <td><input checked="" type="checkbox"/> Water</td> <td><i>N/A</i></td> <td><i>MRM</i></td> </tr> <tr> <td><input checked="" type="checkbox"/> Sewer</td> <td><i>N/A</i></td> <td></td> </tr> <tr> <td><input checked="" type="checkbox"/> Architectural</td> <td><i>ESF</i></td> <td><i>12/20/11</i></td> </tr> <tr> <td><input checked="" type="checkbox"/> Structural</td> <td><i>ESF</i></td> <td><i>12/20/11</i></td> </tr> <tr> <td><input checked="" type="checkbox"/> Plumbing</td> <td><i>STB</i></td> <td><i>12-13-11</i></td> </tr> <tr> <td><input type="checkbox"/> Mechanical</td> <td></td> <td></td> </tr> <tr> <td><input checked="" type="checkbox"/> Electrical</td> <td></td> <td></td> </tr> <tr> <td><input checked="" type="checkbox"/> Access</td> <td><i>ESF</i></td> <td><i>12/20/11</i></td> </tr> <tr> <td><input type="checkbox"/> Spcl Insp Form</td> <td></td> <td></td> </tr> </table>		Initials	Date	<input checked="" type="checkbox"/> Fire	<i>DMS</i>	<i>12/9/11</i>	<input checked="" type="checkbox"/> Zoning	<i>ESF</i>	<i>12-16-11</i>	<input type="checkbox"/> Engineering			<input checked="" type="checkbox"/> Water	<i>N/A</i>	<i>MRM</i>	<input checked="" type="checkbox"/> Sewer	<i>N/A</i>		<input checked="" type="checkbox"/> Architectural	<i>ESF</i>	<i>12/20/11</i>	<input checked="" type="checkbox"/> Structural	<i>ESF</i>	<i>12/20/11</i>	<input checked="" type="checkbox"/> Plumbing	<i>STB</i>	<i>12-13-11</i>	<input type="checkbox"/> Mechanical			<input checked="" type="checkbox"/> Electrical			<input checked="" type="checkbox"/> Access	<i>ESF</i>	<i>12/20/11</i>	<input type="checkbox"/> Spcl Insp Form			<p><b>PERMIT ISSUANCE FEES</b></p> <table border="0"> <tr><td>Grading Plan Review Fee</td><td>\$</td></tr> <tr><td>Adjusted Plan Review Fee</td><td>\$</td></tr> <tr><td>Fast Track Fee</td><td>\$</td></tr> <tr><td>Early Start Fee</td><td>\$</td></tr> <tr><td>Building Permit Fee</td><td>\$</td></tr> <tr><td>Water Assessment Fee</td><td>\$</td></tr> <tr><td>Sewer Assessment Fee</td><td>\$</td></tr> <tr><td>Sewer Inspection Fee</td><td>\$</td></tr> <tr><td>Grading Permit Fee</td><td>\$</td></tr> <tr><td>Driveway Permit Fee</td><td>\$</td></tr> <tr><td>Bond for _____</td><td>\$</td></tr> <tr><td>Other _____</td><td>\$</td></tr> <tr><td><b>Total Issuance Fees</b></td><td>\$</td></tr> </table>	Grading Plan Review Fee	\$	Adjusted Plan Review Fee	\$	Fast Track Fee	\$	Early Start Fee	\$	Building Permit Fee	\$	Water Assessment Fee	\$	Sewer Assessment Fee	\$	Sewer Inspection Fee	\$	Grading Permit Fee	\$	Driveway Permit Fee	\$	Bond for _____	\$	Other _____	\$	<b>Total Issuance Fees</b>	\$
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<p><b>LAND USE PERMITS</b></p>	<p><b>WATER CONNECTION PERMITS</b></p> <p>I hereby acknowledge receipt of a _____ inch meter yoke.</p> <p>X _____                  Date _____</p>	<p><b>APPROVED FOR ISSUANCE</b></p> <p><i>Charlita Ford</i>                  Signature                  12/20/11                  Date</p>	<p><b>PERMIT ISSUANCE PAYMENTS</b></p> <table border="0"> <tr> <th>Date</th> <th>Receipt</th> <th>Check</th> <th>Amount</th> </tr> <tr> <td>_____</td> <td>_____</td> <td>_____</td> <td>\$ _____</td> </tr> <tr> <td>_____</td> <td>_____</td> <td>_____</td> <td>\$ _____</td> </tr> <tr> <td>_____</td> <td>_____</td> <td>_____</td> <td>\$ _____</td> </tr> </table>	Date	Receipt	Check	Amount	_____	_____	_____	\$ _____	_____	_____	_____	\$ _____	_____	_____	_____	\$ _____																																																	
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Parcel Tags:

CONDITIONS AND HOLDS ON PERMIT: (Continued on back of sheet)

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# Staff Review Sheet

Planner

Permit Intake Initials: MS

FIRE ZONE ENG WAT SEW ARCH STRUC PLUM MECH ELEC ACCESS SPECINSP

12/9/2011

Case No: <b>BLD20110712</b>																																																																																				
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<b>Commercial REPI</b> New Dwelling Units: <u>2</u> FCC Code: <u>437</u> Existing Dwelling Units: <u>0</u> Type of Construction: <u>VA</u> Occupancy Class: <u>M, R-3*</u> Sprinkler Substitute for Type A Construction: YES <input type="checkbox"/> NO <input type="checkbox"/> Sprinkler System: FULL PARTIAL NONE Required: YES NO Alarm System: FULL PARTIAL NONE Required: YES NO Code Edition: _____ Code Review by: _____ Date: _____																																																																																				
<b>Valuation for Permit Fee Calculations:</b> <table style="width:100%; border-collapse: collapse;"> <thead> <tr> <th style="text-align: left;">S.F.</th> <th style="text-align: left;">Type</th> <th style="text-align: left;">Rate</th> <th style="text-align: left;">Amount</th> </tr> </thead> <tbody> <tr> <td colspan="3"></td> <td style="text-align: right;"><u>49,536.00</u></td> </tr> <tr> <td colspan="3" style="text-align: right;">Total Valuation:</td> <td style="text-align: right;"><u>\$49,536.00</u></td> </tr> </tbody> </table>		S.F.	Type	Rate	Amount				<u>49,536.00</u>	Total Valuation:			<u>\$49,536.00</u>																																																																							
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<b>LAND USE</b> ZONE/UNITS <u>LC /</u> WETLAND _____ FLOOD ELEV. _____ FIRM ZONE <u>C</u> FIRM MAP _____ LOT SIZE <u>16,045 SF</u> <u>0.4 ACRES</u> SETBACKS Front <u>25</u> Rear <u>10</u> Side <u>10</u> Other _____ PARKING _____ ANADROMOUS _____ EAGLES NEST _____ <b>LAND USE PERMITS</b> _____ _____ _____ _____ _____	<b>ENGINEERING/PUB WORKS</b> Dwelling Units: _____ CITY WATER: Permit #: _____ Use: _____ Assessment: _____ Service Size: _____ Line Size: _____ CITY SEWER: Permit #: _____ Use: _____ Assessment: _____ Fixture Units: _____ <b>WATER CONNECTION PERMITS</b> I hereby acknowledge receipt of a _____ inch meter yoke. X _____ Date _____	<b>PLAN REVIEW APPROVALS</b> <table style="width:100%; border-collapse: collapse;"> <thead> <tr> <th></th> <th style="text-align: center;">Initials</th> <th style="text-align: center;">Date</th> </tr> </thead> <tbody> <tr> <td><input checked="" type="checkbox"/> Fire</td> <td style="text-align: center;"><u>MS</u></td> <td style="text-align: center;"><u>11/9/11</u></td> </tr> <tr> <td><input type="checkbox"/> Zoning</td> <td></td> <td></td> </tr> <tr> <td><input type="checkbox"/> Engineering</td> <td></td> <td></td> </tr> <tr> <td><input type="checkbox"/> Water</td> <td></td> <td></td> </tr> <tr> <td><input type="checkbox"/> Sewer</td> <td></td> <td></td> </tr> <tr> <td><input type="checkbox"/> Architectural</td> <td></td> <td></td> </tr> <tr> <td><input type="checkbox"/> Structural</td> <td></td> <td></td> </tr> <tr> <td><input type="checkbox"/> Plumbing</td> <td></td> <td></td> </tr> <tr> <td><input type="checkbox"/> Mechanical</td> <td></td> <td></td> </tr> <tr> <td><input type="checkbox"/> Electrical</td> <td></td> <td></td> </tr> <tr> <td><input type="checkbox"/> Access</td> <td></td> <td></td> </tr> <tr> <td><input type="checkbox"/> Spcl Insp Form</td> <td></td> <td></td> </tr> </tbody> </table> <b>APPROVED FOR ISSUANCE</b> _____ Signature _____ Date		Initials	Date	<input checked="" type="checkbox"/> Fire	<u>MS</u>	<u>11/9/11</u>	<input type="checkbox"/> Zoning			<input type="checkbox"/> Engineering			<input type="checkbox"/> Water			<input type="checkbox"/> Sewer			<input type="checkbox"/> Architectural			<input type="checkbox"/> Structural			<input type="checkbox"/> Plumbing			<input type="checkbox"/> Mechanical			<input type="checkbox"/> Electrical			<input type="checkbox"/> Access			<input type="checkbox"/> Spcl Insp Form			<b>PERMIT ISSUANCE FEES</b> <table style="width:100%; border-collapse: collapse;"> <tbody> <tr><td>Grading Plan Review Fee</td><td style="text-align: right;">\$</td></tr> <tr><td>Adjusted Plan Review Fee</td><td style="text-align: right;">\$</td></tr> <tr><td>Fast Track Fee</td><td style="text-align: right;">\$</td></tr> <tr><td>Early Start Fee</td><td style="text-align: right;">\$</td></tr> <tr><td>Building Permit Fee</td><td style="text-align: right;">\$</td></tr> <tr><td>Water Assessment Fee</td><td style="text-align: right;">\$</td></tr> <tr><td>Sewer Assessment Fee</td><td style="text-align: right;">\$</td></tr> <tr><td>Sewer Inspection Fee</td><td style="text-align: right;">\$</td></tr> <tr><td>Grading Permit Fee</td><td style="text-align: right;">\$</td></tr> <tr><td>Driveway Permit Fee</td><td style="text-align: right;">\$</td></tr> <tr><td>Bond for</td><td style="text-align: right;">\$</td></tr> <tr><td>Other</td><td style="text-align: right;">\$</td></tr> <tr><td><b>Total Issuance Fees</b></td><td style="text-align: right;"><b>\$</b></td></tr> </tbody> </table> <b>PERMIT ISSUANCE PAYMENTS</b> <table style="width:100%; border-collapse: collapse;"> <thead> <tr> <th>Date</th> <th>Receipt</th> <th>Check</th> <th>Amount</th> </tr> </thead> <tbody> <tr><td>_____</td><td>_____</td><td>_____</td><td style="text-align: right;">\$ _____</td></tr> <tr><td>_____</td><td>_____</td><td>_____</td><td style="text-align: right;">\$ _____</td></tr> <tr><td>_____</td><td>_____</td><td>_____</td><td style="text-align: right;">\$ _____</td></tr> </tbody> </table>	Grading Plan Review Fee	\$	Adjusted Plan Review Fee	\$	Fast Track Fee	\$	Early Start Fee	\$	Building Permit Fee	\$	Water Assessment Fee	\$	Sewer Assessment Fee	\$	Sewer Inspection Fee	\$	Grading Permit Fee	\$	Driveway Permit Fee	\$	Bond for	\$	Other	\$	<b>Total Issuance Fees</b>	<b>\$</b>	Date	Receipt	Check	Amount	_____	_____	_____	\$ _____	_____	_____	_____	\$ _____	_____	_____	_____	\$ _____
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Application Date: December 9, 2011

# BUILDING PERMIT APPLICATION

NOTE: THIS IS NOT A BUILDING PERMIT

\* NOTE: "Building Permit" is a generic term which includes Building Safety Inspections, Grading Permits, and permits for Electrical, Plumbing and Mechanical work.

Case No: **BLD20110712**

Case Description: Convert portion of second floor retail building into two apartment units.

Site Address: **2203 DUNN ST**

Check No. of Existing Dwelling Units:

Parcel No: 5B1601130046

No. of New Dwelling Units:

Legal Description: J W MCKINLEY BL 32 LT 8A

No. of Removed Dwelling Units:

**Applicant :** R H RENTALS LLC  
PO BOX 032403  
JUNEAU AK 99803-2403

e-mail: rhdevelopment@gci.net

WK 790-4146  
FAX 790-4147

**Owner:** R H RENTALS LLC  
PO BOX 032403  
JUNEAU AK 99803-2403

**Contractor:** OWNER/BUILDER

PH: \_\_\_\_\_ FAX \_\_\_\_\_

**Valuation for Permit Fee Calculations:**

S.F. Type

Rate

Amount

49,536.00

13,000

Total Valuation:

~~49,536.00~~  
\$49,536.00

**Associated Cases:**

None.

**Parcel Tags:**

**Notes and Conditions:**

**Applicant's Signature**  
(Owner, Contractor or Authorized Agent)

12/9/11

**Date**

**Staff Acceptance**

I hereby certify that I have read and examined this application and know the same to be true and correct. I further certify that all provisions of laws and ordinances governing this type of work will be complied with whether specified herein or not. I understand that the granting of a permit does not presume to give authority to violate or cancel the provisions of any other federal, state or local law regulating construction or the performance of construction.

JUNEAU PERMIT CENTER - 230 S. Franklin Street - 4th Floor, Marine View Center - Mail: 155 S. Seward Street, Juneau, AK 99801  
Phone: 586-0770 - FAX: 586-3365 - Inspection Requests: 586-1703 - Email: permits@ci.juneau.ak.us  
Web Page: HTTP://WWW.JUNEAU.ORG/PERMITS



11-712

**Jay Srader**

---

**From:** RICHARD HARRIS [RHDevelopment@gci.net]  
**Sent:** Wednesday, January 11, 2012 9:48 AM  
**To:** Jay Srader  
**Cc:** Charlie Ford  
**Subject:** RE: inspection comments for your review

The doors that are used are solid maple doors that are not readily available in Juneau, I will need to order one with a stamp on it, could be 2-3 weeks at best to get here, the Gfi's have been installed. I will forward a cross section drawing on the other apartment 1 hour wall later today.

Rich

---

**From:** Jay Srader [mailto:Jay\_Srader@ci.juneau.ak.us]  
**Sent:** Wednesday, January 11, 2012 7:47 AM  
**To:** 'rhdevelopment@gci.net'  
**Cc:** Charlie Ford  
**Subject:** inspection comments for your review  
**Importance:** High

Rich,

Good morning, after looking over the approved plans and the building code, the doors into the apartments do need to be labeled and listed 20min fire rated assemblies similar to the one you currently have across the hall. Also the Bld Dept will need to review and approve the fire/sound wall design you are adding to the 2<sup>nd</sup> unit prior to its construction. I apologies for these oversights during the review of your project, please contact me or your plan reviewer (John Young) if you have any questions about these code requirements.

For record; these comments have been added to your permit file.

Thank you,

**Jay Srader**

**Building Inspector IV**

**Community Development**

**Phone: 586-0768 Fax: 586-3365**

**E-mail: [jay\\_srader@ci.juneau.ak.us](mailto:jay_srader@ci.juneau.ak.us)**

*"How you handle mistakes is a much better judgment of character than any award or citation" Me (1970-present)*

1/11/2012

Case No: BLD20110712

JUNEAU PERMIT CENTER  
INSPECTION RESULTS

Inspect on: 04/30/2012  
Time: 9:00

Issued On: 12/20/2011

*E*  
*4-30-12*  
*JJS*

Applicant: R H RENTALS LLC

Parcel No: 5B1601130046

Address: 2203 DUNN ST

Legal: J W MCKINLEY BL 32 LT 8A

Project Description: Convert portion of second floor retail building into two, 1 bedroom apartment units.

Notes: shawn 209-7019 reinspect

Inspection: B-Rough Electrical

Result: *C*

Inspected by: *JJS*

Date: *4/30/12*

SETBACKS:

Front: 25.00 E    Rear: 20.00 W    Side1: 10.00 N    Side2: 10.00 S

Parking Spaces: 17

Zoning: LC

Comments: Rear yard setback 20ft instead of 10ft due to abutting D-5 rear yard setback - applies per CBJ 49.25.400 (3). 10' Access easement exist along southern lot line, and an electrical easement exists at the southeastern corner. See 2007 plat for details.

INSPECTION COMMENTS:

*Kitchen Lights Remain On Small Appliance Circuit NOT Approved. Branch Circuits Outside Of Unit Remain In Unit Load Center.*  
*Fire Wall Approved*  
*All Other Corrections Made From Previous Inspections.*

CONDITIONS / HOLDS:

- CBJ approved plans must be on site and available to the inspector. Inspections will not be performed and additional fees may apply if approved plans are not available to the inspectors.-
- Entrance doors to the apartments must be labled/listed 20Min fire rated assemblies, JJS-
- Fire/Sound rated wall design for unit #2 must be received and approved prior to any framing inspections, the approved design must be on site at time of inspection JJS-
- Minimum 1" Street Service and 1 1/4" Building Supply and branches for up to 78 fixture units.-
- All shower valves and combination tub/shower valves shall provide scald and thermal shock protection by means of an approved valve that conforms to ASSE 1016 or ASME A112.18.1 / CSA B125.1. The maximum water temperature supplied to these fixtures shall be 120 degrees fahrenheit per 2009 UPC section 418.-
- Water heaters shall be strapped within the upper 1/3 and lower 1/3 of its vertical dimension per 2009 UPC section 508.2.-

Case No: BLD20110712

JUNEAU PERMIT CENTER  
INSPECTION RESULTS

Inspect on: 04/27/2012  
Time: Anytime

*(E)*  
*4-30-12*  
*AA*

Issued On: 12/20/2011  
Address: 2203 DUNN ST

Applicant: R H RENTALS LLC

Parcel No: 5B1601130046

Legal: J W MCKINLEY BL 32 LT 8A

Project Description: Convert portion of second floor retail building into two, 1 bedroom apartment units.

Notes: shawn 209-7019 reinspect

Inspection: B-Building Final

Result: *C*

Inspected by: *AA*

Date: *4/27/12*

SETBACKS:

Front: 25.00 E    Rear: 20.00 W    Side1: 10.00 N    Side2: 10.00 S

Parking Spaces: 17

Zoning: LC

Comments: Rear yard setback 20ft instead of 10ft due to abutting D-5 rear yard setback - applies per CBJ 49.25.400 (3). 10' Access easement exist along southern lot line, and an electrical easement exists at the southeastern corner. See 2007 plat for details.

INSPECTION COMMENTS:

*Cancelled No Keys To Enter Unit.*

CONDITIONS / HOLDS:

- \_\_\_ CBJ approved plans must be on site and available to the inspector. Inspections will not be performed and additional fees may apply if approved plans are not available to the inspectors.-
- \_\_\_ Entrance doors to the apartments must be labled/listed 20Min fire rated assemblies, JJS-
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**NOTE: Prior to issuance of a Certificate of Occupancy or Temporary Certificate of Occupancy, Zoning, Engineering and Buildings items must be approved.**

Case No: BLD20110712

JUNEAU PERMIT CENTER  
INSPECTION RESULTS

Inspect on: 04/17/2012  
Time: 10:00

3966

⑤  
4-17-12  
JH

Issued On: 12/20/2011

Applicant: R H RENTALS LLC

Parcel No: 5B1601130046

Address: 2203 DUNN ST

Legal: J W MCKINLEY BL 32 LT 8A

Project Description: Convert portion of second floor retail building into two, 1 bedroom apartment units.

Notes: Rich 723-4791 Final

Inspection: B-Building Final

Result: C

Inspected by: JH

Date: 4-17-12

SETBACKS:

Front: 25.00 E Rear: 20.00 W Side1: 10.00 N Side2: 10.00 S

Parking Spaces: 17

Zoning: LC

Comments: Rear yard setback 20ft instead of 10ft due to abutting D-5 rear yard setback - applies per CBJ 49.25.400 (3). 10' Access easement exist along southern lot line, and an electrical easement exists at the southeastern corner. See 2007 plat for details.

INSPECTION COMMENTS:

~~Arch~~ Arc Fault Protection Required In Living Rm  
Kitchen Lights NOT Permitted On Kitchen Small  
Appliance Circuit. Identify Electrical Circuits In Load  
Center NOT Associated With Unit  
Raise Threshold Of Entry Door To Tight Fit  
Provide Inspection Report For Common Wall Fire  
Rating & Sound Proofing.  
Call For Reinspection - Have CBJ Approved  
Plans On Site At Time Of Inspection  
Provide Address Numbers For Unit AT Front Entry

CONDITIONS / HOLDS:

- \_\_\_ CBJ approved plans must be on site and available to the inspector. Inspections will not be performed and additional fees may apply if approved plans are not available to the inspectors.-
- \_\_\_ Entrance doors to the apartments must be labeled/listed 20Min fire rated assemblies, JJS-
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- \_\_\_ Water heaters shall be strapped within the upper 1/3 and lower 1/3 of its vertical dimension per 2009 UPC section 508.2.-

NOTE: Prior to issuance of a Certificate of Occupancy or Temporary Certificate of Occupancy, Zoning, Engineering and Buildings items must be approved.



Case No: BLD20110712

JUNEAU PERMIT CENTER  
INSPECTION RESULTS

Inspect on: 03/06/2012  
Time: 1:00

*E*  
*3-7-12*  
*JH*

Issued On: 12/20/2011

Applicant: R H RENTALS LLC

Parcel No: 5B1601130046

Address: 2203 DUNN ST

Legal: J W MCKINLEY BL 32 LT 8A

Project Description: Convert portion of second floor retail building into two, 1 bedroom apartment units.

Notes: Rich 723-4791. Electrical.

Inspection: B-Rough Electrical

Result: *P*

Inspected by: *JH*

Date: *3-6-12*

SETBACKS:

Front: 25.00 E    Rear: 20.00 W    Side1: 10.00 N    Side2: 10.00 S

Parking Spaces: 17

Zoning: LC

Comments: Rear yard setback 20ft instead of 10ft due to abutting D-5 rear yard setback - applies per CBJ 49.25.400 (3). 10' Access easement exist along southern lot line, and an electrical easement exists at the southeastern corner. See 2007 plat for details.

INSPECTION COMMENTS:

*Follow Manufacturers Instructions For Smoke Detector Location*  
*& Installation Provide Paperwork at Final*  
*Arch Fault Protection Required*  
*OK To Cover*

CONDITIONS / HOLDS:

- CBJ approved plans must be on site and available to the inspector. Inspections will not be performed and additional fees may apply if approved plans are not available to the inspectors.-
- Entrance doors to the apartments must be labled/listed 20Min fire rated assemblies, JJS-
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- Water heaters shall be strapped within the upper 1/3 and lower 1/3 of its vertical dimension per 2009 UPC section 508.2.-



Case No: BLD20110712

**JUNEAU PERMIT CENTER**  
INSPECTION RESULTS

Inspect on: 02/29/2012  
Time: 2:30

*E  
3-1-12  
STW*

Issued On: 12/20/2011

Applicant: R H RENTALS LLC

Parcel No: 5B1601130046

Address: 2203 DUNN ST

Legal: J W MCKINLEY BL 32 LT 8A

Project Description: Convert portion of second floor retail building into two, 1 bedroom apartment units.

Notes: Harbor plumbing 789-7222. Plumbing.

Inspection: B-Rough Plumbing

Result: *A*

Inspected by: *STW*

Date: *2-29-12*

**SETBACKS:**

Front: 25.00 E    Rear: 20.00 W    Side1: 10.00 N    Side2: 10.00 S

Parking Spaces: 17

Zoning: LC

Comments: Rear yard setback 20ft instead of 10ft due to abutting D-5 rear yard setback - applies per CBJ 49.25.400 (3). 10' Access easement exist along southern lot line, and an electrical easement exists at the southeastern corner. See 2007 plat for details.

**INSPECTION COMMENTS:**

*DWV rough-in with 5 psi air test approved.  
Domestic water rough-in with live test approved.*

**CONDITIONS / HOLDS:**

- CBJ approved plans must be on site and available to the inspector. Inspections will not be performed and additional fees may apply if approved plans are not available to the inspectors.-
- Entrance doors to the apartments must be labled/listed 20Min fire rated assemblies, JJS-
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- Water heaters shall be strapped within the upper 1/3 and lower 1/3 of its vertical dimension per 2009 UPC section 508.2.-

Case No: BLD20110712

JUNEAU PERMIT CENTER  
INSPECTION RESULTS

Inspect on: 01/10/2012  
Time: 9:00

*MFS*

Issued On: 12/20/2011

Applicant: R H RENTALS LLC

Parcel No: 5B1601130046

Address: 2203 DUNN ST

Legal: J W MCKINLEY BL 32 LT 8A

Project Description: Convert portion of second floor retail building into two, 1 bedroom apartment units.

Notes: Rich 723-4791. Final for 1 unit.

Inspection: B-Building Final      Result: C      Inspected by: MFS      Date: 1-10-12

SETBACKS:      Parking Spaces: 17      Zoning: LC  
Front: 25.00 E      Rear: 20.00 W      Side1: 10.00 N      Side2: 10.00 S

Comments: Rear yard setback 20ft instead of 10ft due to abutting D-5 rear yard setback - applies per CBJ 49.25.400 (3). 10' Access easement exist along southern lot line, and an electrical easement exists at the southeastern corner. See 2007 plat for details.

INSPECTION COMMENTS:

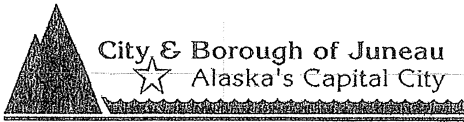
- Kitchen counter needs GFCI's.  
- Entry door is in question - at minimum 20 min fire rated door. Will call with specs

*E 1-10-12  
MFS*

CONDITIONS / HOLDS:

- \_\_\_ CBJ approved plans must be on site and available to the inspector. Inspections will not be performed and additional fees may apply if approved plans are not available to the inspectors.-
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NOTE: Prior to issuance of a Certificate of Occupancy or Temporary Certificate of Occupancy, Zoning, Engineering and Buildings items must be approved.



Community Development & Engineering Departments  
 Mail: 155 South Seward Street  
 Juneau, Alaska 99801  
 Telephone: 586-0770 FAX: 586-3365

**JUNEAU PERMIT CENTER, 4TH FLOOR MARINE VIEW CENTER, 230 South Franklin Street**

## Water Line Information

To be provided for any new building, addition, or remodel with added plumbing fixtures

1. **NEW FIXTURES:** List the number of each type of fixture that is new to this building. (Where no fixtures were before—this does not include fixtures being replaced.)

WFU	No. of Fixtures		WFU	No. of Fixtures	
<u>4</u>	<u>1</u>	Bathtub (w/ or w/o showerhead)	<u>1.5</u>	<u>1</u>	Dishwasher
___	___	Bathtub with 3/4" valve	<u>4</u>	<u>1</u>	Clotheswasher hook-up
___	___	Bidet	___	___	Bar Sink
___	___	Lavatory (bathroom sink)	<u>1.5</u>	<u>1</u>	Kitchen Sink
___	___	Shower	___	___	Laundry Sink
<u>2.5</u>	<u>1</u>	Toilet	___	___	Hose Bibb (for hose connection)
___	___	Urinal	STAFF USE Total Additional WFU: <u>13.5</u>		

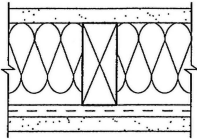
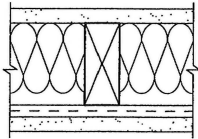
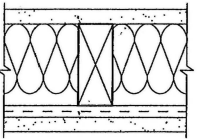
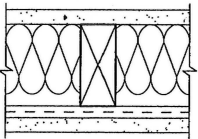
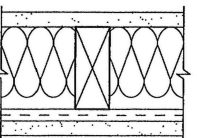
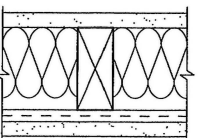
2. **EXISTING FIXTURES:** List the number of each type of existing fixture in the building. (Fixtures to be left unchanged or replaced.)

WFU	No. of Fixtures		WFU	No. of Fixtures	
<u>4</u>	<u>1</u>	Bathtub (w/ or w/o showerhead)	<u>1.5</u>	<u>1</u>	Dishwasher
___	___	Bathtub with 3/4" valve	___	___	Clotheswasher hook-up
___	___	Bidet	___	___	Bar Sink
<u>5</u>	<u>5</u>	Lavatory (bathroom sink)	<u>1.5</u>	<u>1</u>	Kitchen Sink
___	___	Shower	___	___	Laundry Sink
<u>7.5</u>	<u>3</u>	Toilet	<u>3.5</u>	<u>2</u>	Hose Bibb (for hose connection)
___	___	Urinal	STAFF USE Total Existing WFU: <u>23</u>		
STAFF USE Grand Total WFU: <u>36.5</u>					

3. What is the **size** of the incoming water street service line? (pipe size from city main to curb-stop at property line.) 1" Inches
4. What is the **size** of the building service line? (pipe size from curb-stop entering the building.) 1" Inches
5. What is the **distance** from the water shutoff box at the property line or well pump to the farthest plumbing fixture fed by this water service? 65 Feet
6. If known, what is the water **pressure** at the house, or low water pressure setting on the well pump? \_\_\_ PSI
7. What is the **height** measured vertically (above + or below -) of the highest fixture (new or existing) measured from the street level at the water shutoff box or well pump? 13 Feet

STAFF USE Hydrant Pressure (PSI)	<u>73</u>	STAFF USE PSI Adjustment:	<u>-6.5</u>
STAFF USE Total PSI:		<u>66.5</u>	

**WALLS AND INTERIOR PARTITIONS, WOOD FRAMED**

GA FILE NO. WP 3241	PROPRIETARY*	1 HOUR FIRE	50 to 54 STC SOUND																								
<p align="center"><b>GYPSUM WALLBOARD, RESILIENT CHANNELS, MINERAL FIBER INSULATION, WOOD STUDS</b></p> <p>Resilient channels 24" o.c. attached at right angles to ONE SIDE of 2 x 4 wood studs 16" or 24" o.c. with 1 1/4" Type S drywall screws. One layer 5/8" proprietary type X gypsum wallboard or gypsum veneer base applied parallel to channels with 1" Type S drywall screws 12" o.c. End joints backblocked with resilient channels. 3" mineral fiber insulation, 2.0 or 2.3 pcf, in stud space.</p> <p>OPPOSITE SIDE: One layer 5/8" proprietary type X gypsum wallboard or gypsum veneer base applied at right angles to studs with 1 1/4" Type W drywall screws 12" o.c.</p> <p>Vertical joints staggered 48" on opposite sides. Sound tested with studs 16" o.c. and open face of mineral fiber insulation blankets toward resilient channel-side of stud space. <b>(LOAD-BEARING)</b></p> <p align="center"><b>PROPRIETARY GYPSUM BOARD</b></p> <table border="0"> <tr> <td>American Gypsum Company</td> <td>-</td> <td>5/8" FireBloc® TYPE C</td> </tr> <tr> <td>BPB America Inc.</td> <td>-</td> <td>5/8" ProRoc® Type C Gypsum Panels</td> </tr> <tr> <td>BPB Canada Inc.</td> <td>-</td> <td>5/8" ProRoc® Type C Gypsum Panels</td> </tr> <tr> <td>G-P Gypsum</td> <td>-</td> <td>5/8" ToughRock® Fireguard® C</td> </tr> <tr> <td>Lafarge North America Inc.</td> <td>-</td> <td>5/8" Firecheck® Type C</td> </tr> <tr> <td>National Gypsum Company</td> <td>-</td> <td>5/8" Gold Bond® Brand FIRE-SHIELD C™</td> </tr> <tr> <td>PABCO Gypsum</td> <td>-</td> <td>5/8" FLAME CURB® Super 'C'™</td> </tr> <tr> <td>Temple-Inland Forest Products Corporation</td> <td>-</td> <td>5/8" TG-C</td> </tr> </table>		American Gypsum Company	-	5/8" FireBloc® TYPE C	BPB America Inc.	-	5/8" ProRoc® Type C Gypsum Panels	BPB Canada Inc.	-	5/8" ProRoc® Type C Gypsum Panels	G-P Gypsum	-	5/8" ToughRock® Fireguard® C	Lafarge North America Inc.	-	5/8" Firecheck® Type C	National Gypsum Company	-	5/8" Gold Bond® Brand FIRE-SHIELD C™	PABCO Gypsum	-	5/8" FLAME CURB® Super 'C'™	Temple-Inland Forest Products Corporation	-	5/8" TG-C	 <p>Thickness: 5 3/8"                  Approx. Weight: 7 psf                  Fire Test: Based on UL R3660-7, 11-12-87; UL R2717-61, 8-18-87; UL Design U311                  Sound Test: Estimated</p>	
American Gypsum Company	-	5/8" FireBloc® TYPE C																									
BPB America Inc.	-	5/8" ProRoc® Type C Gypsum Panels																									
BPB Canada Inc.	-	5/8" ProRoc® Type C Gypsum Panels																									
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<p align="center"><b>GYPSUM WALLBOARD, RESILIENT CHANNELS, MINERAL OR GLASS FIBER INSULATION, WOOD STUDS</b></p> <p>Resilient channels 16" o.c. attached at right angles to ONE SIDE of 2 x 4 wood studs 16" or 24" o.c. with 1 1/4" Type S drywall screws. One layer 5/8" type X gypsum wallboard or gypsum veneer base applied at right angles to channels with 1" Type S drywall screws 8" o.c. with vertical joints located midway between studs End joints backblocked with resilient channels. 3" mineral or glass fiber insulation in stud space.</p> <p>OPPOSITE SIDE: One layer 5/8" type X gypsum wallboard or gypsum veneer base applied at parallel or at right angles to studs with 6d cement coated nails, 1 7/8" long, 0.0915" shank, 1 5/16" heads, 7" o.c.</p> <p>Vertical joints staggered 24" on opposite sides. Sound tested with studs spaced 24" o.c. (STC=50). Also sound tested with studs spaced 16" o.c. and with two layers of 5/8" type X gypsum board on the resilient channel side (STC=50). <b>(LOAD-BEARING)</b></p>		 <p>Thickness: 5 3/8"                  Approx. Weight: 7 psf                  Fire Test: Based on UL R14196, 05NK05371, 2-15-05, UL Design U305                  Sound Test: NRCC TL93-103, 3-98                  NRCC TL93-118, 3-98</p>																									
<p align="center"><b>GYPSUM WALLBOARD, RESILIENT CHANNELS, MINERAL OR GLASS FIBER INSULATION, WOOD STUDS</b></p> <p>Resilient channels 24" o.c. attached at right angles to ONE SIDE of 2 x 4 wood studs 16" or 24" o.c. with 1 1/4" Type S drywall screws. One layer 5/8" type X gypsum wallboard or gypsum veneer base applied at right angles to channels with 1" Type S drywall screws 8" o.c. with vertical joints located midway between studs End joints backblocked with resilient channels. 3" mineral or glass fiber insulation in stud space.</p> <p>OPPOSITE SIDE: One layer 5/8" type X gypsum wallboard or gypsum veneer base applied at parallel or at right angles to studs with 6d cement coated nails, 1 7/8" long, 0.0915" shank, 1 5/16" heads, 7" o.c.</p> <p>Vertical joints staggered 24" on opposite sides. Sound tested with studs spaced 24" o.c. (STC=50). Also sound tested with studs spaced 16" o.c. and with two layers of 5/8" type X gypsum board on the side opposite the resilient channels (STC=53). <b>(LOAD-BEARING)</b></p>		 <p>Thickness: 5 3/8"                  Approx. Weight: 7 psf                  Fire Test: Based on UL R14196, 05NK05371, 2-15-05, UL Design U305                  Sound Test: NRCC TL93-116, 3-98</p>																									

*BLD 20110712  
 approved  
 3/1/12*

\*Contact the manufacturer for more detailed information on proprietary products.





(907) 586-0715  
CDD\_Admin@juneau.org  
www.juneau.org/CDD  
155 S. Seward Street • Juneau, AK 99801

### Little Moon Nursery

Case Number: PAC2021 0040  
Applicant: Kayla Svinicki  
Property Owner: RH Rentals, LLC  
Property Address: 2213 Dunn Street  
Parcel Code Number: 5B1601130046  
Site Size: 16,045 Square Feet  
Zoning: Light Commercial  
Existing Land Use:

Conference Date: June 16, 2021  
Report Issued: June 23, 2021  
**DISCLAIMER: Pre-application conferences are conducted for purposes of providing applicants with a preliminary review of a project and timeline. Pre-application conferences are not based on a complete application, and are not a guarantee of final project approval.**

### List of Attendees

Note: Copies of the Pre-Application Conference Report will be emailed, instead of mailed, to participants who have provided their email address below.

Name	Title	Email address
Kayla Svinicki	Applicant	<a href="mailto:littlemoonak@gmail.com">littlemoonak@gmail.com</a>
Irene Gallion	Planning	<a href="mailto:Irene.Gallion@juneau.org">Irene.Gallion@juneau.org</a>
Charlie Ford	Building	<a href="mailto:Charlie.Ford@juneau.org">Charlie.Ford@juneau.org</a>
Not Applicable	General Engineering	<a href="mailto:xxx.xxx@juneau.org">xxx.xxx@juneau.org</a>
Kyle Paw	Permit Technician	<a href="mailto:Kyle.paw@juneau.org">Kyle.paw@juneau.org</a>

## Conference Summary

### Questions/issues/agreements identified at the conference that weren't identified in the attached reports.

The following is a list of issues, comments and proposed actions, and requested technical submittal items that were discussed at the pre-application conference.

### Project Overview

The applicant is proposing a nursery for infants and toddlers. The facility will accommodate 40 children and 10 staff. Actual capacity may be reduced after state inspectors assess "useable space," which will not include offices, kitchen, diapering rooms, etc.

Minor development is a nonresidential use using less than 10,000 square feet or using less than half an acre.

- The proposed usable space for daycare is 2,185. Total space (including offices) is ~2,800.
- The size of the lot is 0.37 acres.
- This is minor development, approvable by the Director [Table of Permissible Uses, 7.300]. A Conditional Use Permit IS NOT required.

All operations are on the ground floor of the building.

Two suites will be used for daycare:

- 2205 will accommodate up to 10 infants
- 2213 will accommodate up to 30 mobile toddlers.

### Planning Division

1. **Zoning** – Light Commercial
2. **Setbacks** – Existing structure
  - a. **Front** – 25' front, 77 provided
  - b. **Side** – 10' feet required and provided
  - c. **Rear** – 10' required, 20 provided
3. **Height** – 45' primary, 35' accessory.
4. **Access** – Dunn Street.
5. **Parking & Circulation**– Variance 2005-00039 provides for back-out parking onto Dunn Street.

CBJ 49.65.1120 requires one parking space per on-shift employee, plus one space per ten children served.

Employees = 10

Children = 40 /4 = 10

20 spaces are required

29 spaces are provided, with two ADA spaces.

6. **Lot Coverage** – No maximum, 2880 square feet existing.
7. **Vegetative Coverage** – 15% required, approximately 1350 square feet provided along the rear of the property, or 8%. A reduction of vegetative cover will not be permitted.
8. **Lighting** – Not applicable.

9. **Noise** – Noise anticipated would be compatible with Light Commercial zoning.
10. **Flood** – Zone X – no mapped flood hazard.
11. **Hazard/Mass Wasting/Avalanche/Hillside Endorsement** – Not applicable.
12. **Wetlands** – Not applicable. Lot is fully developed, and this proposal does not propose further development.
13. **Habitat** – Not applicable.
14. **Plat or Covenant Restrictions** – No applicable plat notes.
15. **Traffic** – A traffic study is not required. The average number of trips generated per 1,000 square feet of gross floor area is 74.06 [Institute of Traffic Engineers Trip Generation Manual Volume 3, page 1125], for a total of 161.8211 trips per day. This is less than the 250 that triggers a traffic study [CBJ 49.40.30(a) (2)].
16. **Nonconforming situations** not applicable.

### Building Division

17. The maximum capacity will be 50. Under building code, max capacity of up to 100 can be “educational use” since there are two classrooms.

**General Engineering/Public Works** – Not applicable.

### Fire Marshal

18. **Fire Items/Access** – Sprinklers are not needed for the single story ground floor project. Two fire extinguishers will be needed, about 75’ apart. CO detectors will be required since parking is less than 25 feet away.

### Other Applicable Agency Review

19. Not applicable.
- 20.

### List of required applications

Based upon the information submitted for pre-application review, the following list of applications must be submitted in order for the project to receive a thorough and speedy review.

1. Development Permit Application: <https://juneau.org/index.php?gf-download=2019%2F12%2FDPA.pdf&form-id=22&field-id=11&hash=81202ef96484e0b859d1c4855a6b7ae4b18cb229e8824059dbb0d96082e74d6d>
2. Building Permit Application: [https://juneau.org/index.php?gf-download=2019%2F07%2Ffill-in\\_bldpermit\\_app.pdf&form-id=22&field-id=11&hash=70f9f7a7b5d8faa14d63b1b841c8e0880e2642714885adcf767a69546126a9d4](https://juneau.org/index.php?gf-download=2019%2F07%2Ffill-in_bldpermit_app.pdf&form-id=22&field-id=11&hash=70f9f7a7b5d8faa14d63b1b841c8e0880e2642714885adcf767a69546126a9d4)
3. Note: These forms can be sent to [permits@juneau.org](mailto:permits@juneau.org) a permit technician will contact you for payment.

### Additional Submittal Requirements

Submittal of additional information, given the specifics of the development proposal and site, are listed below. These items will be required in order for the application to be determined Counter Complete.

1. A copy of this pre-application conference report.

### Exceptions to Submittal Requirements

Submittal requirements staff has determined **not** to be applicable or **not** required, given the specifics of the development proposal, are listed below. These items will **not** be required in order for the application to be reviewed.

1. Not applicable

### Fee Estimates

The preliminary plan review fees listed below can be found in the CBJ code section 49.85.

Based upon the project plan submitted for pre-application review, staff has attempted to provide an accurate estimate for the permits and permit fees which will be triggered by your proposal.

1. Fees will depend on the value of work being done.

For informational handouts with submittal requirements for development applications, please visit our website at [www.juneau.org/cdd](http://www.juneau.org/cdd).

### Submit your Completed Application

Phone: (907) 586-0770  
Web: [www.juneau.org/cdd](http://www.juneau.org/cdd)



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Recording District 101 Juneau  
08/12/2016 10:48 AM Page 1 of 11



**Juneau Recording District**  
**When recorded, please return to:**  
City and Borough of Juneau  
Attn: Engineering Department  
155 South Seward Street  
Juneau, AK 99801  
Phone: (907) 586-0876

**DUNN STREET OWNERSHIP AND MAINTENANCE AGREEMENT**

---

**PART I. PARTIES.** In exchange for the valuable consideration described herein, this agreement is made between the (1) City and Borough of Juneau, Alaska a municipal corporation in the State of Alaska, hereinafter “CBJ,” (2) RH Rentals LLC, an Alaska limited liability company, hereinafter “RHR” and (3) Duck Creek Properties LLC, an Alaska limited liability company, hereinafter “DCP.” The term “owners” refer to RHR and DCP. The term “parties” refers to the CBJ, RHR, and DCP.

---

**PART II. ADMINISTRATION.** All communications about this agreement shall be directed as follows, any reliance on communication with a person other than that listed below is at the party’s own risk.

**CBJ:**  
City and Borough of Juneau  
Attn: Lands and Resource Manager  
155 S. Seward Street  
Juneau, Alaska 99801  
Fax: 907-463-2606

**RHR:**  
RH Rentals LLC  
Attn: Richard Harris  
P.O. Box 32403  
Juneau, Alaska 99803

**DCP:**  
Duck Creek Properties, LLC  
Attn: Charles Schultz  
16275 Point Lena Loop Rd  
Juneau, AK 99801

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**PART III. DESCRIPTION.** The following are attached and are considered a part of this agreement as well as anything incorporated by reference or attached to those attachments.

- Appendix A: Property Description & Specific Agreement Provisions
- Exhibit 1: 2015 Dunn Street Improvements

Dunn Street Ownership & Maintenance Agreement 1  
Ord. 2016-21 - Exhibit A

**PART IV. EXECUTION.** The parties agree and sign below. This agreement is not effective until signed by the CBJ, and until the CBJ's share of the cost is fully appropriated by the City and Borough Assembly (Home Rule Charter of the City and Borough of Juneau, Sec. 9.13). The parties represent that the person signing below has the authority to do so and that it is a valid and binding contract enforceable in accordance with its terms.

**CITY & BOROUGH OF JUNEAU**

Date: 8/9/16

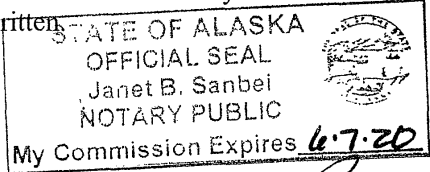
By: *[Signature]*  
 Duncan Rorie Watt  
 CBJ Manager

**CBJ ACKNOWLEDGMENT**

STATE OF ALASKA )  
 ) : ss.  
 First Judicial District )

This is to certify that on the 9<sup>th</sup> day of August, 2016, before the undersigned, a Notary Public in and for the State of Alaska, duly commissioned and sworn, personally appeared DUNCAN RORIE WATT, to me known to be the Manager of the City and Borough of Juneau, Alaska a municipal corporation, who on oath stated that he was duly authorized to execute said instrument on behalf of said corporation, who acknowledged to me that he signed the same freely and voluntarily on behalf of said corporation for the uses and purposes therein mentioned.

WITNESS my hand and official seal on the day and year in this certificate first above written



*[Signature]*  
 Notary Public in and for the State of Alaska  
 My Commission Expires: 6-7-2020

Content Approved by: *[Signature]*, Engineering and Public Works Dept.  
 Form Approved by: Amy Mead, Municipal Attorney Digitally signed by Amy Mead, Municipal Attorney  
 DN: cn=Amy Mead, Municipal Attorney, ou=CBJ Law Department, ou,  
 email=amy.mead@juneau.org, c=US  
 Date: 2016.07.14 12:45:39 -0500, Law Department  
 Risk Management Review: *[Signature]* 8/5/16, Risk Management

Dunn Street Ownership & Maintenance Agreement 2  
 Ord. 2016-21 - Exhibit A

Page 2 of 11  
 2016-003874-0

Duck Creek Properties, LLC  
Date: 8/3/2016  
By: [Signature]  
Charles Schultz  
Co-owner of DCP

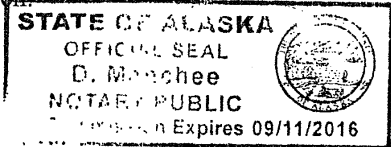
Duck Creek Properties, LLC  
Date: 8/3/16  
By: [Signature]  
Kristen Schultz  
Co-owner of DCP

**DCP ACKNOWLEDGMENT**

STATE OF ALASKA)  
) : ss.  
First Judicial District )

This is to certify that on the 3 day of August, 2016, before the undersigned, a Notary Public in and for the State of Alaska, duly commissioned and sworn, personally appeared to me known to be the identical individual described in and who executed the foregoing instrument as CHARLES SCHULTZ, who on oath stated that s/he was duly authorized to execute said instrument on behalf of Duck Creek Properties, LLC, and who acknowledged to me that s/he signed the same freely and voluntarily for the uses and purposes therein mentioned.

WITNESS my hand and official seal on the day and year in this certificate first above written.

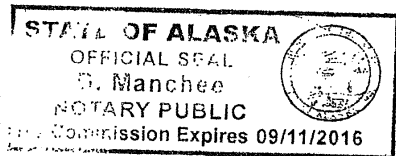


[Signature]  
Notary Public in and for the State of Alaska  
My Commission Expires: 9/11/2016

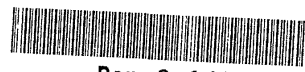
STATE OF ALASKA)  
) : ss.  
First Judicial District )

This is to certify that on the 3 day of August, 2016, before the undersigned, a Notary Public in and for the State of Alaska, duly commissioned and sworn, personally appeared to me known to be the identical individual described in and who executed the foregoing instrument as KRISTEN SCHULTZ, who on oath stated that s/he was duly authorized to execute said instrument on behalf of Duck Creek Properties, LLC, and who acknowledged to me that s/he signed the same freely and voluntarily for the uses and purposes therein mentioned.

WITNESS my hand and official seal on the day and year in this certificate first above written.



[Signature]  
Notary Public in and for the State of Alaska  
My Commission Expires: 9/11/2016



R.H. Rentals LLC

Date: 8/3/16

By: [Signature]  
Richard Harris  
Co-owner of RHR

R.H. Rentals LLC

Date: 8/3/16

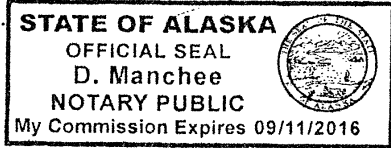
By: [Signature]  
Katherine M. Harris  
Co-owner of RHR

**RHR ACKNOWLEDGMENT**

STATE OF ALASKA )  
 ) : ss.  
First Judicial District )

This is to certify that on the 3<sup>rd</sup> day of August, 2016, before the undersigned, a Notary Public in and for the State of Alaska, duly commissioned and sworn, personally appeared to me known to be the identical individual described in and who executed the foregoing instrument as RICHARD HARRIS, who on oath stated that s/he was duly authorized to execute said instrument on behalf of R.H. Rentals LLC, and who acknowledged to me that s/he signed the same freely and voluntarily for the uses and purposes therein mentioned.

WITNESS my hand and official seal on the day and year in this certificate first above written.

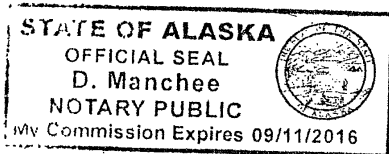


[Signature]  
Notary Public in and for the State of Alaska  
My Commission Expires: 9/11/2016

STATE OF ALASKA )  
 ) : ss.  
First Judicial District )

This is to certify that on the 3<sup>rd</sup> day of August, 2016, before the undersigned, a Notary Public in and for the State of Alaska, duly commissioned and sworn, personally appeared to me known to be the identical individual described in and who executed the foregoing instrument as KATHERINE M. HARRIS, who on oath stated that s/he was duly authorized to execute said instrument on behalf of R.H. Rentals LLC, and who acknowledged to me that s/he signed the same freely and voluntarily for the uses and purposes therein mentioned.

WITNESS my hand and official seal on the day and year in this certificate first above written.



[Signature]  
Notary Public in and for the State of Alaska  
My Commission Expires: 9/11/2016





**APPENDIX A: PROPERTY DESCRIPTION &  
SPECIFIC AGREEMENT PROVISIONS**

**A. Identity of Parties**

**1. R.H. Rentals LLC**

RHR is the owner of record for the following properties<sup>1</sup>

Parcel #	Abbreviated Legal Description	Address
5B1601130031	JW McKinley BL32 LT5A	1997 Dunn St
5B1601130042	JW McKinley BL32 LT8A	2201 Dunn St
5B1601130046	JW McKinley BL32 LT8A	2203 Dunn St
5B1601130047	JW McKinley BL32 LT10A	2221 Dunn St
5B1601130050	McKinley Lundwall TR1	9355 Glacier Hwy
5B1601130060	McKinley Lundwall TR2&3	9351 Glacier Hwy

**2. Duck Creek Properties LLC**

DCP is the owner of record for the following property:

Parcel #	Abbreviated Legal Description	Address
5B1601130071	J.W. McKinley BL32 LTS 14 &15 <sup>2</sup>	2220 Dunn St

**3. City and Borough of Juneau**

The CBJ owns the land comprising the right-of-way underlying Dunn Street, but the CBJ has not accepted ownership or maintenance of Dunn Street. Dunn Street is currently a privately maintained road in a public right-of-way.

**B. Recitals**

- Dunn Street was platted in the J.W. McKinley subdivision in the early 1950s, which predates adoption of subdivision right-of-way and street standards.<sup>3</sup>
- The Dunn Street right-of-way (“ROW”) is 30 feet wide.
- The Dunn Street ROW was dedicated for public use during the J.W. McKinley subdivision, but it was not developed to public use standards.

<sup>1</sup> Quit Claim Deed, JRD 2003-000379-0 (Jan. 15, 2003)

<sup>2</sup> Statutory Warranty Deed, JRD 2010-000905-0 (Feb. 2, 22, 2010).

<sup>3</sup> Front pocket of Deed Book 46, Juneau Recording District, State of Alaska (Nov. 19, 1952); Deed Book 58 at Page 254, Juneau Recording District, State of Alaska.



4. The Dunn Street ROW has not been constructed to any acceptable development standard and remains an unimproved gravel road not accepted for maintenance by the CBJ.

5. The properties along Dunn Street have been developed to the fullest extent without providing the necessary infrastructure within the Dunn Street ROW for the CBJ to accept Dunn Street for maintenance.

6. The Owners want to improve Dunn Street and submitted a petition requesting the City and Borough pave Dunn Street.

7. In December 2013, the CBJ Public Works and Facilities Committee (PWFC) directed staff to look into the costs of providing paving improvements.

8. The CBJ Assembly appropriated \$250,000 in the FY 2015 Capital Improvement Project as partial funding for a Dunn Street paving project.

9. On March 23, 2015, the PWFC passed a motion supporting a combined owner contribution of \$50,000 towards the project cost.

10. On May 28, 2016, the owners requested the installation of a street light at the intersection of Dunn Street and Glacier Highway and agreed to contribute \$6,250 toward the cost of installation.

11. By Ordinance 2016-21, the CBJ Assembly authorized the Manager to execute this agreement.

12. Pursuant to CBJ 15.10.290, when the owners of one hundred percent of the property bearing the cost of improvements agree, the assembly may authorize a contract for provision of such improvements in lieu of special assessments where the amounts due the City and Borough are a lien upon all real property involved in the same manner and with the same priority as special assessments, subject to penalty and interest as provided for special assessments.

**C. Agreement**

The parties agree as follows:

1. **Agreement duration.** The parties execute this agreement with the intent for it to be perpetual and run with the land binding all heirs, successors, and assigns.

2. **Ownership of land.** The land underlying Dunn Street has been dedicated and accepted for public use.

3. **Character of Roadway.** Dunn Street is a private maintained road in a public ROW.



4. **Access.** The parties agree that Dunn Street will remain open for vehicle and pedestrian public access.

5. **2015 Improvements.** The Owners will collectively contribute \$56,250 towards the Dunn Street paving improvements as described in Exhibit 1 (pavement, road base, curb and gutter, storm drainage, water system, street light at the intersection of Dunn Street with Old Glacier Highway, etc.). The CBJ will cover all other improvement costs consistent with Exhibit 1. CBJ estimates its contributions will exceed \$400,000. The parties intend for the Exhibit 1 improvements to be completed in 2017, but this agreement does not require the improvements to occur in 2015.

6. **Financial Guarantee.** This agreement shall be deemed an adequate financial guarantee of improvements pursuant to CBJ 49.55.010.

7. **RHR Contribution.** RHR will contribute \$48,214.30 towards the Exhibit 1 improvements.

8. **DCP Contribution.** DCP will contribute \$8,035.70 towards the Exhibit 1 improvements.

9. **Lien.** Any amounts due the City and Borough shall be a lien upon all real property involved, paramount and superior to any other lien created before or after the Agreement is executed, except a lien for a prior assessment or for property taxes.

10. **Owner Contribution and Financing Terms:** Owner contributions are due in full, or a financing agreement signed and first payment made, within 60 days of the Assembly appropriating the remaining funds necessary to complete the Exhibit 1 improvements. If not paid in full, the remaining amount shall be paid in ten equal annual installments, payable in the same manner and at the same time as property taxes. All installments shall include interest on the unpaid balance at the rate of 3.09 percent per annum. The penalty and interest that apply to the delinquent payment of property taxes shall apply to the delinquent payment of the annual installment and interest.

11. **CBJ Maintenance:** Upon completion of the improvements described in Exhibit 1, the CBJ has a duty to maintain the asphalt and drainage system and street light at the intersection of Dunn Street with Old Glacier Highway within the Dunn Street ROW.

12. **Winter maintenance.** The Owners have a duty to provide all winter maintenance of the Dunn Street ROW, including but not limited to snow removal and traction enhancements (i.e. sand, salt, etc.). The Owners are responsible for any damage caused by or related to winter maintenance.

13. **Owners' Association:** Prior to either Owner conveying title to their property, the Owners shall create an owner's association for the purpose of continuing the duties contained in this agreement. The Owners agree that such owner's association shall have the power to assess the lots subject to this agreement for their share of the maintenance costs of Dunn Street.



**14. Indemnification.** The Owners agrees to defend, indemnify, and hold harmless the CBJ, its employees, volunteers, consultants, and insurers, with respect to any action, claim, or lawsuit arising out of or related to the Owners’ use, occupancy, and maintenance of the Dunn Street ROW, without limitation as to the amount of fees, and without limitation as to any damages, cost or expense resulting from settlement, judgment, or verdict, and includes the award of any attorneys’ fees even if in excess of Alaska Civil Rule 82. This indemnification provision applies to the fullest extent permitted by law and is in full force and effect whenever and wherever any action, claim, or lawsuit is initiated, filed, or otherwise brought against CBJ relating to this agreement. The obligations of the Owners arise immediately upon actual or constructive notice of any action, claim, or lawsuit. The CBJ shall notify the Owners in a timely manner of the need for indemnification, but such notice is not a condition precedent to the Owners obligations and is waived where the Owners have actual notice.

**15. Governing Law.** The laws of the State of Alaska shall govern this agreement. The venue for any proceedings brought by either party to enforce the terms of this agreement shall be in the First Judicial District at Juneau, Alaska.

**16. Integration and Modification.** This agreement, including all appendices, exhibits, and references, constitutes the entire agreement between parties. Any modification, amendment, or waiver of any provisions of this agreement shall not be effective, unless it is mutually agreed to in writing and signed by the Parties.

**17. Severability.** If one or more of the provisions of this agreement is held invalid, illegal or unenforceable in any respect, such holding will not impair the validity, legality, or enforceability of the remaining provisions.

**18. Construction.** The parties have reviewed and participated in the preparation of this agreement. The parties agree that any ambiguities will not be construed against a party.

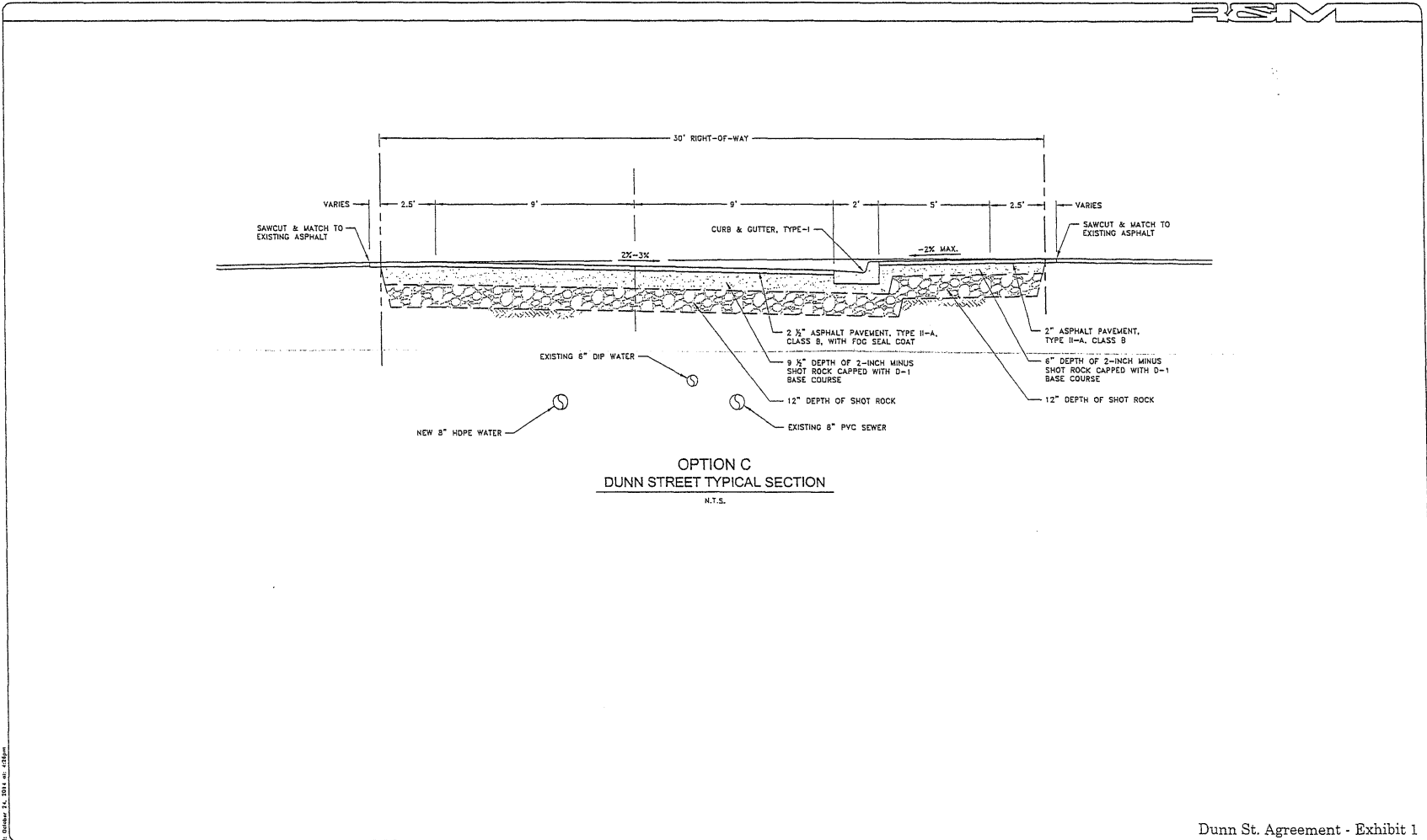
**19. Recordation.** The CBJ will record a copy of this agreement, at the Owner’s expense, with the state recorder’s office for each lot subject to this agreement.

**20. Effective Date.** This agreement is not effective until signed by the CBJ, and until the CBJ’s share of the cost is fully appropriated by the City and Borough Assembly, as required by the City and Borough of Juneau’s Home Rule Charter, Sec. 9.13.

End of Agreement.





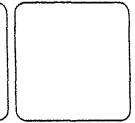


Dunn St. Agreement - Exhibit 1

DESIGN	JMP
DRAWN	KAP
CHECK	JMP
APPROVED	JMP
FILE	

NO.	DATE	REVISION	BY	APPROV.

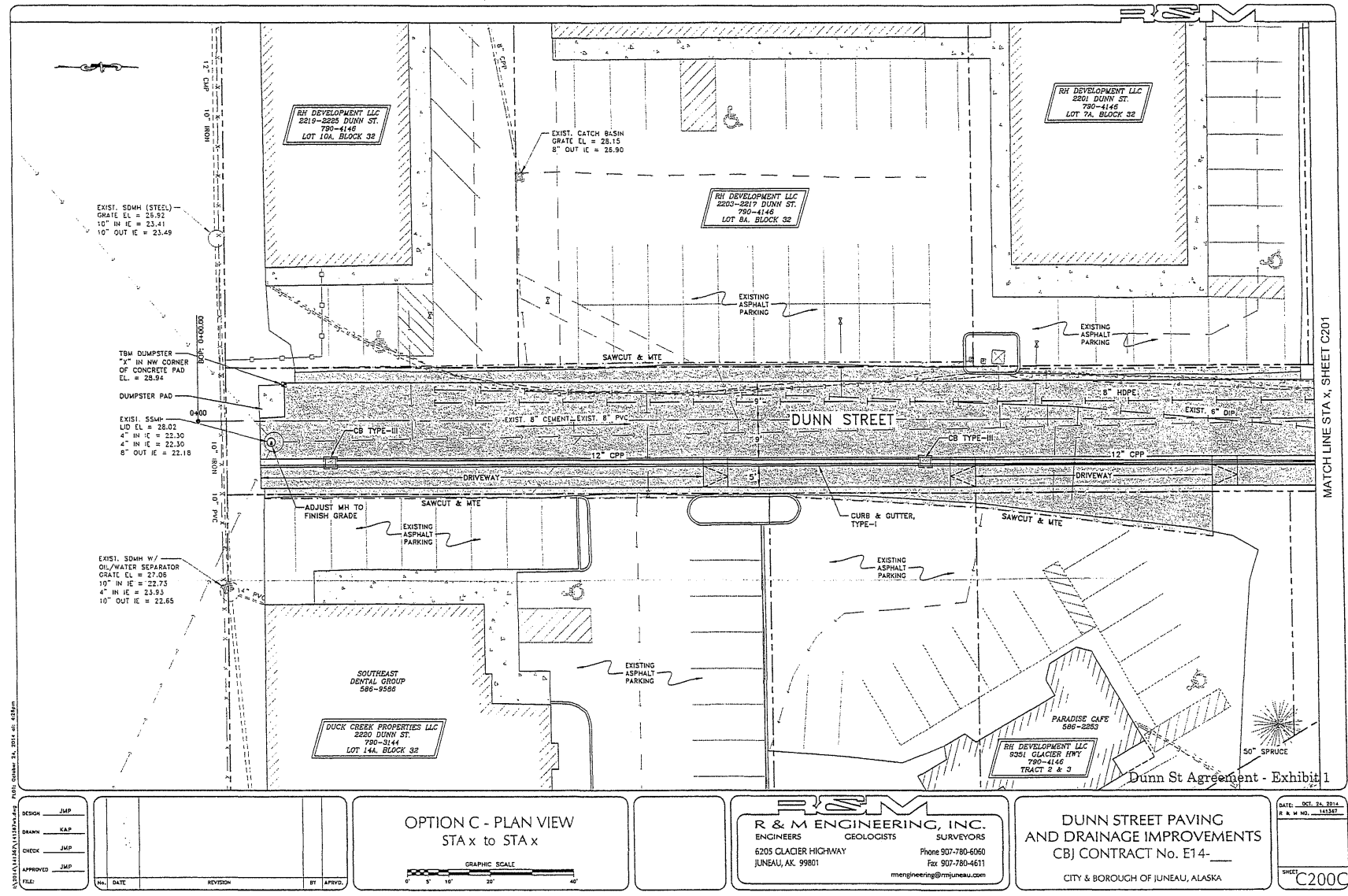
TYPICAL SECTIONS



**R & M ENGINEERING, INC.**  
ENGINEERS      GEOLOGISTS      SURVEYORS  
6205 GLACIER HIGHWAY      Phone 907-780-6060  
JUNEAU, AK. 99801      Fax 907-780-4611  
rmengineering@rjunEAU.com

**DUNN STREET PAVING  
AND DRAINAGE IMPROVEMENTS**  
CBJ CONTRACT No. E14-\_\_\_\_  
CITY & BOROUGH OF JUNEAU, ALASKA

DATE	OCT. 14, 2014
P & H NO.	141367
SHEET	C101



DESIGN	JMP
DRAWN	KAP
CHECK	JMP
APPROVED	JMP
FILE	

NO.	DATE	REVISION	BY	APPROV.

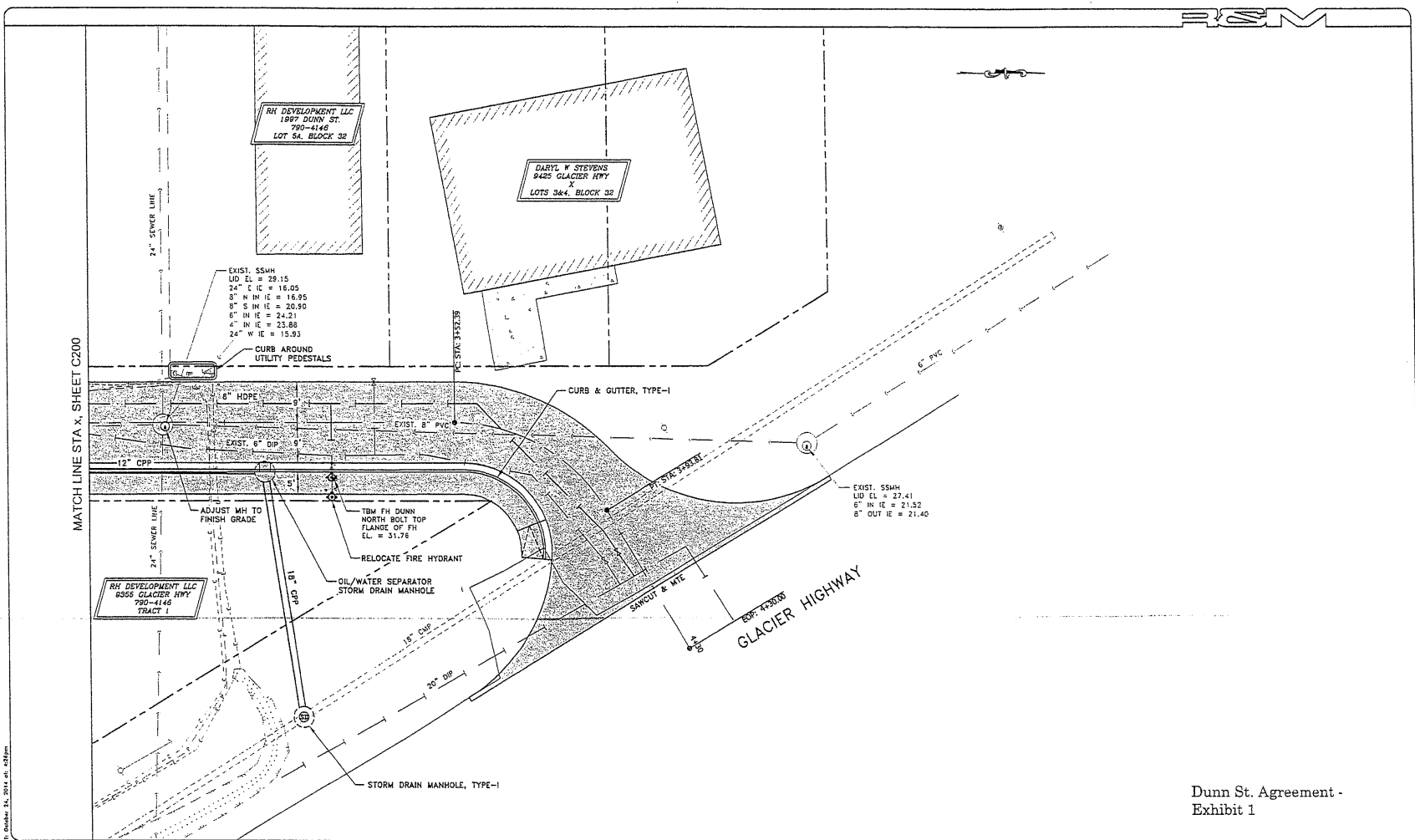
OPTION C - PLAN VIEW  
STA x to STA x

GRAPHIC SCALE  
0' 5' 10' 20' 40'

**R & M ENGINEERING, INC.**  
ENGINEERS GEOLOGISTS SURVEYORS  
6205 CLACIER HIGHWAY Phone 907-780-6060  
JUNEAU, AK. 99801 Fax 907-780-4611  
mengineering@mjuneau.com

DUNN STREET PAVING AND DRAINAGE IMPROVEMENTS  
CBJ CONTRACT No. E14-\_\_\_  
CITY & BOROUGH OF JUNEAU, ALASKA

DATE: OCT. 24, 2014  
P. & M. NO. 141387  
SHEET C200C



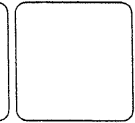
Dunn St. Agreement - Exhibit 1

DESIGN	JMP
DRAWN	KAP
CHECK	JMP
APPROVED	JMP
TITLE	

NO.	DATE	REVISION	BY	APPROV.

OPTION C - PLAN VIEW  
STA x to STA x

GRAPHIC SCALE  
0 5 10 20 40



**R & M ENGINEERING, INC.**  
ENGINEERS GEOLOGISTS SURVEYORS

6205 GLACIER HIGHWAY Phone 907-780-6060  
JUNEAU, AK 99801 Fax 907-780-4611  
rmengineering@rmljuneau.com

DUNN STREET PAVING  
AND DRAINAGE IMPROVEMENTS  
CBJ CONTRACT No. E14-\_\_\_

CITY & BOROUGH OF JUNEAU, ALASKA

DATE	OCT 24, 2014
R & M NO.	141387
SHEET	C201C



# BUILDING PERMIT

Permit No.  
BLD20130030

Section H, Item 1.

\* NOTE: "Building Permit" is a generic term which includes Building Safety Inspection, Grading Permits, and permits for Electrical, Plumbing and Mechanical work.

**Your special attention is called to the following:**

This permit is granted on the express conditions that the construction shall, in all respects, conform to the ordinances of the City and Borough of Juneau. It may be revoked at any time upon violation of any of said ordinances.

The granting of this permit does not authorize the violation of any federal, state or local law regulating construction for the violation of the terms of any deed or covenant or any zoning or other regulation.

If plan review was required, this permit must be attached to the approved drawings. The permit, plans and record of inspections must be available on site at all times while the construction is in progress and before final inspection.

The yellow posting notice must be prominently displayed to show a permit has been issued and to assist the inspectors in location of the project. This permit becomes null and void if work or construction authorized is not commenced within one year or if work or construction is suspended or abandoned for a period of one year at any time after work has commenced.

Note: City Ordinances REQUIRE a Final Inspection be approved for every Building Permit.

**Inspections**

Inspections can be arranged by telephoning 586-1703 or by written or faxed notification.

The Online Building Inspection Request Form is at: [www.juneau.org/permits/inspect\\_request.php](http://www.juneau.org/permits/inspect_request.php).

Work shall not proceed until the inspector has approved the various stages of construction. An approved Final Inspection is required.

Call before 7:30 AM for same day inspections.

Please provide the following information: 1 Permit Number, 2 Address, 3 Type of Inspection, 4 Date and Time and 5 Contact Name and Phone Number.

<b>Job Address: 2203 DUNN ST</b> <b>Permit Number: BLD20130030</b> <b>Project Description: Change of use from office to efficiency apartment</b>	<b>Issued Date : 02/01/2013</b> <b>Parcel No: 5B1601130046</b>
--	---

**Parcel Information : J W MCKINLEY BL 32 LT 8A**

Setbacks:	Zone: LC:	Firm Zone: C
	Front: 25.00 Ft. E	Side 1: 10.00 Ft. N
	Rear: 20.00 Ft. W	Side 2: 10.00 Ft. S
	Street Side:	
Comments: 10' Access easement exist along southern lot line, and an electrical easement exists at the southeastern corner. See 2007 plat for details.		

Owner :	R H RENTALS LLC PO BOX 032403 JUNEAU AK 99803-2403
---------	--

Applicant :	ALPINE CONSTRUCTION PO BOX 32684 JUNEAU AK 99803
-------------	--

Fee Type	Date	Receipt	Amount Paid
BLD- Comm Plan Review	01/16/2013	03282	\$76.28
BLD- Bldg Permit Fee	01/16/2013	03282	\$117.36
Total Fees Paid:			\$193.64

Valuation for Permit Fee Calculations:		
S.F.	Type	Amount
		3,850.00
Total Valuation:		\$3,850.00

**Project Conditions and Holds:**  
**Custom Condition as a Warning** - Conditions from the original building permit (BLD06-679) for the existing 2-story building may apply. See conditions list in new apartment case folder.

Inspections Required: Call for inspection before covering or concealing any of the work described below. Inspections may be combined.

B-Framing	B-Rough Electrical	B-Rough Plumbing
B-Building Final		

**JUNEAU PERMIT CENTER - 230 S. Franklin Street - 4th Floor, Marine View Center - Mail: 155 S. Seward Street, Juneau, AK 99801**  
 Phone: 586-0770 - FAX: 586-3365 - Inspection Requests: 586-1703 - Email: [permits@ci.juneau.ak.us](mailto:permits@ci.juneau.ak.us)  
 Web Site: [www.juneau.org/permits](http://www.juneau.org/permits)



# Staff Review Sheet

Permit Intake Initials: JK

FIRE ZONE ENG WAT SEW ARCH STRUC PLUM MECH ELEC ACCESS SPEC/INSP

1/16/2013

Case No: **BLD20130030**

Site Address: 2203 DUNN ST

Parcel No: 5B1601130046

Desc: Change of use from office to apartment

Commercial REPI New Dwelling Units: 1  
 FCC Code: 437 Existing Dwelling Units: 0  
 Type of Construction: \_\_\_\_\_ Occupancy Class: R-2  
 Sprinkler Substitute for Type A Construction: YES NO  
 Sprinkler System: FULL PARTIAL NONE Required: YES NO  
 Alarm System: FULL PARTIAL NONE Required: YES NO  
 Code Edition: TBC 2006 Code Review by: [Signature] Date: 2/11/13

Sent to Fire  
Date 01/17/13

**Valuation for Permit Fee Calculations:**

S.F.	Type	Rate	Amount
			3,850.00
Total Valuation:			<b>\$3,850.00</b>

**LAND USE**  
 ZONE/UNITS LC1  
 WETLAND \_\_\_\_\_  
 FLOOD ELEV. 7  
 FIRM ZONE C  
 FIRM MAP 880C  
 LOT SIZE 16,045 SF  
0.4 ACRES

**ENGINEERING/PUB WORKS**

Dwelling Units: \_\_\_\_\_  
 CITY WATER:  
 Permit #: \_\_\_\_\_  
 Use: \_\_\_\_\_  
 Assessment: \_\_\_\_\_  
 Service Size: \_\_\_\_\_  
 Line Size: \_\_\_\_\_  
 CITY SEWER:  
 Permit #: \_\_\_\_\_  
 Use: \_\_\_\_\_  
 Assessment: \_\_\_\_\_  
 Fixture Units: \_\_\_\_\_

**PLAN REVIEW APPROVALS**

	Initials	Date
<input checked="" type="checkbox"/> Fire	<u>SP</u>	<u>1/22/13</u>
<input checked="" type="checkbox"/> Zoning	<u>BSF</u>	<u>1/23/13</u>
<input type="checkbox"/> Engineering		
<input type="checkbox"/> Water	<u>N/A</u>	
<input type="checkbox"/> Sewer	<u>N/A</u>	
<input checked="" type="checkbox"/> Architectural	<u>DS</u>	<u>2/1/13</u>
<input checked="" type="checkbox"/> Structural	<u>MS</u>	<u>1/10</u>
<input checked="" type="checkbox"/> Plumbing	<u>CF</u>	<u>2/1/13</u>
<input type="checkbox"/> Mechanical		
<input type="checkbox"/> Electrical		
<input checked="" type="checkbox"/> Access	<u>DS</u>	<u>2/1/13</u>
<input type="checkbox"/> Spcl Insp Form		

**PERMIT ISSUANCE FEES**

Grading Plan Review Fee	\$ _____
Adjusted Plan Review Fee	\$ _____
Fast Track Fee	\$ _____
Early Start Fee	\$ _____
Building Permit Fee	\$ _____
Water Assessment Fee	\$ _____
Sewer Assessment Fee	\$ _____
Sewer Inspection Fee	\$ _____
Grading Permit Fee	\$ _____
Driveway Permit Fee	\$ _____
Bond for _____	\$ _____
Other _____	\$ _____
<b>Total Issuance Fees</b>	<b>\$ _____</b>

**LAND USE PERMITS**

**WATER CONNECTION PERMITS**

I hereby acknowledge receipt of a \_\_\_\_\_ inch meter yoke.  
 X \_\_\_\_\_  
 Date \_\_\_\_\_

**APPROVED FOR ISSUANCE**

[Signature]  
 Signature  
2/11/13  
 Date

**PERMIT ISSUANCE PAYMENTS**

Date	Receipt	Check	Amount
_____	_____	_____	\$ _____
_____	_____	_____	\$ _____
_____	_____	_____	\$ _____

**Parcel Tags:**

As of BLD11-712 the parking requirement is 17 spaces:  
 1st Fl: 90' x 32' = 2,880/ Retail 300 = 9.6 sp  
 2nd Fl: 16 x 90= 1,440/ 300 = 4.8 sp + 16 x 14 = 224/ Office 300 = .75 sp + 4.8 = 5.5  
 2, 1 bedroom Apts. = 2 sp = Total 7.5 sp  
 Total Parking Req. = 17 sp

CONDITIONS AND HOLDS ON PERMIT: (Continued on back of sheet)

Adj to D-5  
 All Interior Work for project

# Staff Review Sheet

Permit Intake Initials:   K  

FIRE ZONE ENG WAT SEW ARCH STRUC PLUM MECH ELEC ACCESS SPEC/INSP

1/16/2013

Case No: **BLD20130030**

Site Address: 2203 DUNN ST

Parcel No: 5B1601130046

Desc: Change of use from office to apartment

Commercial \_\_\_\_\_ REPI \_\_\_\_\_ New Dwelling Units:   1    
 FCC Code:   437   Existing Dwelling Units:   0    
 Type of Construction: \_\_\_\_\_ Occupancy Class:   R-2    
 Sprinkler Substitute for Type A Construction: YES NO  
 Sprinkler System: FULL PARTIAL NONE Required: YES NO  
 Alarm System: FULL PARTIAL NONE Required: YES NO  
 Code Edition: \_\_\_\_\_ Code Review by: \_\_\_\_\_ Date: \_\_\_\_\_

**Valuation for Permit Fee Calculations:**

S.F.	Type	Rate	Amount
			3,850.00
Total Valuation:			\$3,850.00

<p><b>LAND USE</b></p> <p>ZONE/UNITS <u>  LC /  </u>                  WETLAND _____                  FLOOD ELEV _____                  FIRM ZONE <u>  C  </u>                  FIRM MAP _____                  LOT SIZE <u>  16,045 SF  </u>  <u>  0.4 ACRES  </u></p> <p>SETBACKS                  Front <u>  25  </u>                  Rear <u>  10  </u>                  Side <u>  10  </u>                  Other _____</p> <p>PARKING _____                  ANADROMOUS _____                  EAGLES NEST _____</p> <p><b>LAND USE PERMITS</b></p>	<p><b>ENGINEERING/PUB WORKS</b></p> <p>Dwelling Units: _____                  CITY WATER:                  Permit #: _____                  Use: _____                  Assessment: _____                  Service Size: _____                  Line Size: _____                  CITY SEWER:                  Permit #: _____                  Use: _____                  Assessment: _____                  Fixture Units: _____</p> <p><b>WATER CONNECTION PERMITS</b></p> <p>I hereby acknowledge receipt of a _____ inch meter yoke.                  X _____                  Date _____</p>	<p><b>PLAN REVIEW APPROVALS</b></p> <table border="0"> <tr> <td></td> <td>Initials</td> <td>Date</td> </tr> <tr> <td><input checked="" type="checkbox"/> Fire</td> <td><u>  K  </u></td> <td><u>  1/24  </u></td> </tr> <tr> <td><input checked="" type="checkbox"/> Zoning</td> <td>_____</td> <td>_____</td> </tr> <tr> <td>  Engineering</td> <td>_____</td> <td>_____</td> </tr> <tr> <td><input checked="" type="checkbox"/> Water</td> <td>_____</td> <td>_____</td> </tr> <tr> <td><input checked="" type="checkbox"/> Sewer</td> <td>_____</td> <td>_____</td> </tr> <tr> <td><input checked="" type="checkbox"/> Architectural</td> <td>_____</td> <td>_____</td> </tr> <tr> <td><input checked="" type="checkbox"/> Structural</td> <td>_____</td> <td>_____</td> </tr> <tr> <td><input checked="" type="checkbox"/> Plumbing</td> <td>_____</td> <td>_____</td> </tr> <tr> <td>  Mechanical</td> <td>_____</td> <td>_____</td> </tr> <tr> <td>  Electrical</td> <td>_____</td> <td>_____</td> </tr> <tr> <td><input checked="" type="checkbox"/> Access</td> <td>_____</td> <td>_____</td> </tr> <tr> <td>  Spcl Insp Form</td> <td>_____</td> <td>_____</td> </tr> </table> <p><b>APPROVED FOR ISSUANCE</b></p> <p>_____                  Signature                  _____                  Date</p>		Initials	Date	<input checked="" type="checkbox"/> Fire	<u>  K  </u>	<u>  1/24  </u>	<input checked="" type="checkbox"/> Zoning	_____	_____	Engineering	_____	_____	<input checked="" type="checkbox"/> Water	_____	_____	<input checked="" type="checkbox"/> Sewer	_____	_____	<input checked="" type="checkbox"/> Architectural	_____	_____	<input checked="" type="checkbox"/> Structural	_____	_____	<input checked="" type="checkbox"/> Plumbing	_____	_____	Mechanical	_____	_____	Electrical	_____	_____	<input checked="" type="checkbox"/> Access	_____	_____	Spcl Insp Form	_____	_____	<p><b>PERMIT ISSUANCE FEES</b></p> <table border="0"> <tr><td>Grading Plan Review Fee</td><td>\$ _____</td></tr> <tr><td>Adjusted Plan Review Fee</td><td>\$ _____</td></tr> <tr><td>Fast Track Fee</td><td>\$ _____</td></tr> <tr><td>Early Start Fee</td><td>\$ _____</td></tr> <tr><td>Building Permit Fee</td><td>\$ _____</td></tr> <tr><td>Water Assessment Fee</td><td>\$ _____</td></tr> <tr><td>Sewer Assessment Fee</td><td>\$ _____</td></tr> <tr><td>Sewer Inspection Fee</td><td>\$ _____</td></tr> <tr><td>Grading Permit Fee</td><td>\$ _____</td></tr> <tr><td>Driveway Permit Fee</td><td>\$ _____</td></tr> <tr><td>Bond for _____</td><td>\$ _____</td></tr> <tr><td>Other _____</td><td>\$ _____</td></tr> <tr><td><b>Total Issuance Fees</b></td><td>\$ _____</td></tr> </table> <p><b>PERMIT ISSUANCE PAYMENTS</b></p> <table border="0"> <tr> <th>Date</th> <th>Receipt</th> <th>Check</th> <th>Amount</th> </tr> <tr> <td>_____</td> <td>_____</td> <td>_____</td> <td>\$ _____</td> </tr> <tr> <td>_____</td> <td>_____</td> <td>_____</td> <td>\$ _____</td> </tr> <tr> <td>_____</td> <td>_____</td> <td>_____</td> <td>\$ _____</td> </tr> </table>	Grading Plan Review Fee	\$ _____	Adjusted Plan Review Fee	\$ _____	Fast Track Fee	\$ _____	Early Start Fee	\$ _____	Building Permit Fee	\$ _____	Water Assessment Fee	\$ _____	Sewer Assessment Fee	\$ _____	Sewer Inspection Fee	\$ _____	Grading Permit Fee	\$ _____	Driveway Permit Fee	\$ _____	Bond for _____	\$ _____	Other _____	\$ _____	<b>Total Issuance Fees</b>	\$ _____	Date	Receipt	Check	Amount	_____	_____	_____	\$ _____	_____	_____	_____	\$ _____	_____	_____	_____	\$ _____
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CONDITIONS AND HOLDS ON PERMIT: (Continued on back of sheet)

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Application Date: January 16, 2013

# BUILDING PERMIT APPLICATION

NOTE: THIS IS NOT A BUILDING PERMIT

\* NOTE: "Building Permit" is a generic term which includes Building Safety Inspections, Grading Permits, and permits for Electrical, Plumbing and Mechanical work.

Case No: **BLD20130030**

Case Description: Change of use from office to apartment

Site Address: **2203 DUNN ST**

Check No. of Existing Dwelling Units:

Parcel No: 5B1601130046

No. of New Dwelling Units:

Legal Description: J W MCKINLEY BL 32 LT 8A

No. of Removed Dwelling Units:

**Applicant :** ALPINE CONSTRUCTION  
PO BOX 32684  
JUNEAU AK 99803

PRI 723-6473

**Owner:** R H RENTALS LLC  
PO BOX 032403  
JUNEAU AK 99803-2403

**Contractor:** ALPINE CONSTRUCTION  
PO BOX 32684  
JUNEAU AK 99803

PH: \_\_\_\_\_ FAX \_\_\_\_\_

**Valuation for Permit Fee Calculations:**

S.F.	Type	Rate	Amount
			3,850.00
Total Valuation:			\$3,850.00

**Associated Cases:**

None.

**Parcel Tags:**

As of BLD11-712 the parking requirement is 17 spaces:

1st Fl: 90' x 32' = 2,880/ Retail 300 = 9.6 sp

2nd Fl: 16 x 90 = 1,440/ 300 = 4.8 sp + 16 x 14 = 224/ Office 300 = .75 sp + 4.8 = 5.5

2, 1 bedroom Apts. = 2 sp = Total 7.5 sp

Total Parking Req. = 17 sp

**Notes and Conditions:**

  
\_\_\_\_\_  
**Applicant's Signature**  
(Owner, Contractor or Authorized Agent)

1/16/13  
\_\_\_\_\_  
**Date**

\_\_\_\_\_  
**Staff Acceptance**

I hereby certify that I have read and examined this application and know the same to be true and correct. I further certify that all provisions of laws and ordinances governing this type of work will be complied with whether specified herein or not. I understand that the granting of a permit does not presume to give authority to violate or cancel the provisions of any other federal, state or local law regulating construction or the performance of construction.

JUNEAU PERMIT CENTER - 230 S. Franklin Street - 4th Floor, Marine View Center - Mail: 155 S. Seward Street, Juneau, AK 99801  
Phone: 586-0770 - FAX: 586-3365 - Inspection Requests: 586-1703 - Email: [permits@ci.juneau.ak.us](mailto:permits@ci.juneau.ak.us)  
Web Page: [HTTP://WWW.JUNEAU.ORG/PERMITS](http://www.juneau.org/permits)

Parking Requirement for Lot 9A

1/24/13

New Building Permit # BLD2013-30

- New Efficiency Apt. on second floor that replaces storage space

1<sup>st</sup> Floor: Did not calculate parking requirement at this time

- RH Office
- Rain Tree Quilting (retail)
- Massage Therapy
- other unknown uses at this time

2<sup>nd</sup> Floor: 6.9 spaces = 7 spaces

- 2-1 Bedroom Apartments = 3 parking spaces
- Office = 2.4 spaces (1/4 space of second floor area)
- New efficiency Apartment = 1.5 spaces

**Original parking requirement under Building Permit # BLD06-679 = 19 Spaces**

- Eric Feldt  
1/24/13



# BUILDING PERMIT

Permit No. Section H, Item 1.  
 BLD20240811

\* NOTE: "Building Permit" is a generic term which includes Building Safety Inspection, Grading Permits, and permits for Electrical, Plumbing and Mechanical work.

**Your special attention is called to the following:**

This permit is granted on the express conditions that the construction shall, in all respects, conform to the ordinances of the City and Borough of Juneau. It may be revoked at any time upon violation of any of said ordinances.

The granting of this permit does not authorize the violation of any federal, state or local law regulating construction for the violation of the terms of any deed or covenant or any zoning or other regulation.

If plan review was required, this permit must be attached to the approved drawings. The permit, plans and record of inspections must be available on site at all times while the construction is in progress and before final inspection.

The yellow posting notice must be prominently displayed to show a permit has been issued and to assist the inspectors in location of the project. This permit becomes null and void if work or construction authorized is not commenced within one year or if work or construction is suspended or abandoned for a period of one year at any time after work has commenced.

Note: City Ordinances REQUIRE a Final Inspection be approved for every Building Permit.

**Inspections**

Inspections can be arranged by telephoning **586-1703** or by written or by online form or Email.

The Online Building Inspection Request Form is at: [www.juneau.org/community-development/cdd-inspection-request](http://www.juneau.org/community-development/cdd-inspection-request).

Work shall not proceed until the inspector has approved the various stages of construction. An approved Final Inspection is required.

All inspections must be requested before noon the business day prior. Same day inspections must be requested by calling 586-0770

Please provide the following information: 1 Permit Number, 2 Address, 3 Type of Inspection, 4 Date and Time and 5 Contact Name and Phone Number.

Job Address: <b>2203 DUNN ST</b> Permit Number: <b>BLD20240811</b> Project Description: Complete partition wall to create an additional retail unit and finish bathroom.	Issued Date : <b>01/02/2025</b> Parcel No: <b>5B1601130046</b>
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Parcel Information : J W MCKINLEY BL 32 LT 8A

Setbacks:	Zone: :	Front: 25.00 Ft.	Side 1: 10.00 Ft.	Rear: 10.00 Ft.	Side 2: 10.00 Ft.	Street Side: 17.00 Ft.
Comments:						

Owner :	R H RENTALS LLC PO BOX 032403 JUNEAU AK 99803-2403
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Applicant :	RH RENTALS LLC PO BOX 032403 JUNEAU AK 99803
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Fee Type	Date	Receipt	Amount Paid
BLD- Bldg Permit Fee	12/16/2024	68357	\$188.64
BLD- Comm Plan Review	12/16/2024	68357	\$122.62
Total Fees Paid:			\$311.26

Valuation for Permit Fee Calculations:		
S.F.	Type	Rate
		10,000.00
Total Valuation:		\$10,000.00

Project Conditions and Holds:

**Approved Plans On Site** - CBJ approved plans must be on site and available to the inspector. Inspections will not be performed and additional fees may apply if approved plans are not available to the inspectors.

**WATER FIXTURE COUNT** - 1" Street Service and 1.25" Building supply good for 78wfu. 1.25"=78, 1"=39, 3/4"=20, 1/2"=7

**Shower Anti-scald Valve** - Showers and tub-shower combinations shall be provided with a control valve installed at the point of use that conforms to ASSE 1016/ ASME A112.1016/CSAB125.16 or ASME A112.18.1/CSA B125.1.

**Seismic Strap Wtr Heater** - Water heaters shall be anchored in an approved manner or strapped within the upper 1/3 and lower 1/3 of its vertical dimensions.

**Water Hammer** - Building water supply systems where quick-acting valves are installed shall be provided with water hammer arrester(s) to absorb high pressures resulting from the quick closing of these valves. Water hammer arresters shall be approved mechanical devices that comply with ASSE 1010 or PWI-WH 201 and shall be installed as close as possible to quick-acting valves.

Inspections Required: Call for inspection before covering or concealing any of the work described below. Inspections may be combined.

B-Framing	B-Rough Electrical	B-Rough Plumbing
B-Fire Separation/Rated Assemblies	B-Vents (Bath, Dryer, Kitchen, etc.)	B-Building Final

**JUNEAU PERMIT CENTER - 230 S. Franklin Street - 4th Floor, Marine View Center - Mail: 155 S. Seward Street, Juneau, AK 99801**  
 Phone: 586-0770 - FAX: 586-4529 - Inspection Requests: 586-1703, [Inspections@Juneau.org](mailto:Inspections@Juneau.org) - Questions, Email: [permits@juneau.org](mailto:permits@juneau.org)  
 Web Site: [www.juneau.org/community-development](http://www.juneau.org/community-development)





## Assessor's Database

### Current Owner

R H RENTALS LLC  
 PO BOX 032403, JUNEAU AK 99803

**Parcel #:** 5B1601130046 ([Map](#))

**Address:** 2203 DUNN ST

**Legal Desc. 1:** J W MCKINLEY  
 BL 32 LT 8A

**Legal Desc. 2:**

**Prev. Owner:** R H  
 DEVELOPMENT LLC

**Site Value:** \$312196.00

**Building PV:** \$671406.00

**Total PV:** \$983602.00

**Use Code:** Commercial Retail

**Exempt:** No Data

**Zoning:** Light Commercial

**Tax Year:** 2024

**No. of Units:** 001

**Year Built:** 2008

**Gross Liv. Area:** 005814 sqft

**Garage:** No

**Garage Area:** 000000

**Lot Size:** 16045.00

**Last Trans:** 20030115

**City Water:** Yes

**City Sewer:** Yes

**Exempt Total:** 0

**Road/No Road:** Roaded

**Exempt Land:** 0

**Exempt Building:** 0

## Search the Database

Search the database using the search box below. The field accepts any search parameter (owner's name, address, parcel number, year built, etc.).



Attachment N – 2025 Assessor Data & Site Photos





Attachment N – 2025 Assessor Data & Site Photos









**PLANNING COMMISSION STAFF**  
**CONDITIONAL USE PERMIT USE2025 0007**  
**HEARING DATE: MARCH 25, 2025**

(907) 586-0715  
 CDD\_Admin@juneau.gov  
 www.juneau.org/community-development  
 155 Heritage Way • Juneau, AK 99801

**DATE:** March 17, 2025  
**TO:** Mandy Cole, Chair, Planning Commission  
**BY:** Ilsa Lund, Planner I *Ilsa Lund*  
**THROUGH:** Jill Lawhorne, Director, AICP

**PROPOSAL:** Construct a three-story dental clinic.

**STAFF RECOMMENDATION:** Approval with conditions

**KEY CONSIDERATIONS FOR REVIEW:**

- The applicant owns multiple lots in this subdivision and this facility will become part of the medical complex.
- The lot is not large enough to accommodate the minimum parking requirement for the use, and a shared parking agreement will be implemented with a lot under the same ownership less than 500 feet away.

**ALTERNATIVE ACTIONS:**

1. **Amend:** require additional conditions, or delete or modify the recommended conditions.
2. **Deny:** deny the permit and adopt new findings for items 1-6 below that support the denial.
3. **Continue:** to a future meeting date if determined that additional information or analysis is needed to make a decision, or if additional testimony is warranted.

**ASSEMBLY ACTION REQUIRED:**

Assembly action is not required for this permit.

**STANDARD OF REVIEW:**

- Quasi-judicial decision
- Requires five (5) affirmative votes for approval
- Code Provisions:
  - 49.15.330
  - 49.25.215
  - 49.40.215
  - 49.80

GENERAL INFORMATION	
<b>Property Owner</b>	Southeast Alaska Regional Health Consortium
<b>Applicant</b>	Dawson Construction
<b>Property Address</b>	3063 Vintage Blvd.
<b>Legal Description</b>	VINTAGE III LT B1
<b>Parcel Number</b>	5B1601430016
<b>Zoning</b>	Light Commercial (LC)
<b>Land Use Designation</b>	Traditional Town Center (TTC)
<b>Lot Size</b>	32,689 sq. ft./ 0.75 acre
<b>Water/Sewer</b>	CBJ
<b>Access</b>	Access easement off Vintage Boulevard
<b>Existing Land Use</b>	Undeveloped
<b>Associated Applications</b>	N/A

*The Commission shall hear and decide the case per 49.15.330(a) Conditional Use Permit. A conditional use is a use that may or may not be appropriate in a particular zoning district according to the character, intensity, or size of that or surrounding uses. The conditional use permit procedures is intended to afford the commission the flexibility necessary to make determinations appropriate to individual sites. The commission may attach to the permit those conditions listed in subsection (g) of this section as well as any further conditions necessary to mitigate external adverse impacts. If the commission determines that these impacts cannot be satisfactorily overcome, the permit shall be denied.*



**SITE FEATURES AND ZONING**



<b>SURROUNDING ZONING AND LAND USES</b>	
North (LC)	First Bank
South (LC)	Safeway
East (LC)	Riverside Drive Right-of-Way / Commercial
West (LC)	SEARHC Medical Clinic

<b>SITE FEATURES</b>	
<b>Anadromous</b>	None
<b>Flood Zone</b>	0.2% annual chance flood hazard
<b>Hazard</b>	None
<b>Hillside</b>	No
<b>Wetlands</b>	No
<b>Parking District</b>	No
<b>Historic District</b>	No
<b>Overlay Districts</b>	Mining & Exploration Surface Activities Exclusion District, Urban/ Rural Mining District

**BACKGROUND INFORMATION**

**Project Description** – The applicant proposes to build a three-story, 19,635 sq. ft. dental clinic on an undeveloped lot as part of the Southeast Alaska Regional Health Consortium (SEARHC) Vintage Park Campus (Attachment A). The façade of the building will match the existing SEARHC medical clinic on the adjacent lot (VINTAGE IV TR A2). Lot B1 is not large enough to accommodate the minimum amount of parking required for a clinic of this size, and the applicant will be required to record a shared parking agreement with a lot under the same ownership on Postal Way in accordance with CBJ 49.40.215(a).

**Background** – The Vintage Subdivision was initially platted in 1984 (Attachment B), and re-platted into Lots A and B in 2003. (Attachment C). These lots were re-platted again in 2004 to adjust the lot line and enlarge Lot B1 to its current size (Attachment D).

<b>Date</b>	<b>Item</b>	<b>Summary</b>
1984	Plat	Plat 84-117 Vintage Subdivision (Attachment B).
2003	Plat	Plat 2003-29 (Attachment C).
2004	Plat	Plat 2004-45 enlarged lot and added easements for drainage, utilities, access, and maintenance (Attachment D).

**ZONING REQUIREMENTS**

Standard		Requirement	Existing	Code Reference
Lot	Size	2,000 sq. ft.	32,689 sq. ft.	CBJ 49.25.400
	Width	20 ft.	~120 ft.	CBJ 49.25.400
Setbacks	Front	25 ft.	N/A	CBJ 49.25.400
	Rear	10 ft.	N/A	CBJ 49.25.400
	Side	10 ft.	N/A	CBJ 49.25.400
	Side	10 ft.	N/A	CBJ 49.25.400
	Street Side	17 ft.	N/A	CBJ 49.25.400
Lot Coverage		None	N/A	CBJ 49.25.400
Height	Permissible	45 ft.	N/A	CBJ 49.25.400
	Accessory	35 ft.	N/A	CBJ 49.25.400
Maximum Dwelling Units (30 units/acre)		N/A	Commercial	CBJ 49.25.500
Use		Light Commercial	Undeveloped	CBJ 49.25.300
Vegetative Cover		15% (4,903 sq. ft.)	Unknown	CBJ 49.50.300

**SITE PLAN**



**ANALYSIS**

**Project Site** – This undeveloped lot is located near the intersection of Riverside Drive and Vintage Boulevard next to Safeway. The lot is currently being used to stage construction materials while SEARHC builds out their Vintage Park Campus.

**Condition:** None.

**Project Design** – The exterior design of the building will match the existing façade of the SEARHC medical clinic on the adjacent lot. The materials are transparent resin and metal.

**Condition:** None.

**Traffic** – The Institute of Transportation Trip Generation Manual 9th Edition (TGM) for a facility of this size with approximately 50 staff on site at any given time indicates an average of 446 daily trips. With Lot B1 being in a TTC with a transit station and other amenities close at hand, 446 ADTs does not seem realistic. Adjacent uses, such as a grocery store, bank with drive-through, and post office are likely to generate more traffic.

The Applicant provided a TIA for the SEARHC medical clinic on the adjacent lot in 2022 and it was determined that the four (4) closest intersections did not meet the threshold to require mitigation (Attachment E). Given the results of that recent TIA, combined with the proposed development’s projected traffic based upon 50 employees, a traffic impact analysis is not required per the Director’s determination [CBJ 49.40.300(a)(3)].

Use	Number of Employees	Trips Generated	Total Trips
Medical-Dental Office Building	50	446	446
<b>Total ADTs:</b>			<b>446</b>

**Condition:** None.

**Vehicle Parking & Circulation** – Primary access to the lot will be through the access easement off Vintage Boulevard, also known as Postal Way Extension. According to CBJ 49.40.210(a), 98 parking spaces are required for a facility of this size. Due to space limitations on the primary lot, the applicant has proposed to provide additional parking off-site on a lot under common ownership (Lot 40A) less than 500 feet away on Postal Way. Lot B1 will provide 51 parking spaces including four (4) ADA-compliant parking spaces. An additional 47 parking spaces will be provided on Lot 40A, providing 100 total parking spaces for this facility. The 47 spaces on Lot 40A are in addition to the ten (10) parking spaces required for the workforce housing multifamily units.

Use	Unit/Total Sq. Ft.	Spaces Required	Total Spaces
Dental Clinic	1/200 sq. ft.	98	98
<b>Total Parking Requirement:</b>			<b>98</b>

Use	Unit/Total Sq. Ft.	Spaces Required	Total Spaces
<b>Off-Street Loading Spaces Required:</b>			<b>1</b>
<b>ADA Accessible Spaces Required:</b>			<b>4</b>

**Condition:** Prior to the issuance of a building permit, a joint use parking agreement under CBJ 49.40.200(5) must be approved by the CBJ Community Development Department, and must be recorded in the State Recorder’s Office prior to issuance of a building permit for the structure proposed on VINTAGE III LT B1. The parking agreement must state that the parking agreement may be nullified when the use on VINTAGE III LT B1 is changed and the CBJ Community Development Department Director has approved the nullification of the agreement that is recorded with the State Recorder’s Office.

**Non-motorized Transportation and Proximity to Transit** – The Vintage Park area is designated as a Traditional Town Center in the 2013 Comprehensive Plan, reflecting its strategic location for promoting walkability, mixed-use development, and integration with transit services. Providing secure, covered bicycle parking ensures that bicycles are properly stored and protected from the elements, promoting their use as a mode of transport. With The Valley Transit Station located less than 500 feet away across Riverside Drive, this area is well-positioned to support alternative transportation options, such as bicycling, which aligns with the goals of the plan to foster sustainable, transit-oriented development.

**Condition:** Prior to the issuance of a Temporary Certificate of Occupancy, one or more covered bicycle racks providing spaces for bicycles shall be provided. The rack(s) shall be permanently affixed to the ground, building, or other permanent fixture, and shall be located so that parked bicycles do not encroach into a pedestrian walkway or vehicle area.

**Noise** – The facility will contain outpatient dental services. Adjacent uses include a financial institution, a grocery store and gas station, condominiums, senior housing, a memory care facility, professional offices, a large post office distribution center, and other commercial services. The noise anticipated for this facility is not expected to be out of character with the existing neighborhood and Light Commercial zoning.

**Condition:** None.

**Lighting** - A lighting plan and light fixture data sheet was submitted with the application. According to CBJ 49.40.230(d), parking areas shall be suitably lit. All proposed fixtures shall be full cut-off design. Based on the submitted materials, staff believes that this requirement has been met.

**Condition:** None.

**Vegetative Cover & Landscaping** – The minimum required vegetative coverage for Light Commercial zoning district is 15 percent, or 4,903 sq. ft. for Lot B1. The lot is currently being used as a staging area for construction materials and the majority of the vegetative coverage has been removed or covered with gravel.

**Condition:** Prior to the issuance of a Temporary Certificate of Occupancy, the vegetative cover areas shown on the plans must be planted.

**Habitat** - No anadromous streams run within 200 feet. No other known habitat regulated by the land use code exists on the site.

**Condition:** None.

**Drainage and Snow Storage** – Drainage plans were submitted with the application, but there is no indication of a proposed snow storage location.

**Condition:** Prior to the issuance of a building permit for the proposed project, the applicant shall submit a site plan indicating proposed snow storage location(s).

**Historic District** – Not applicable.

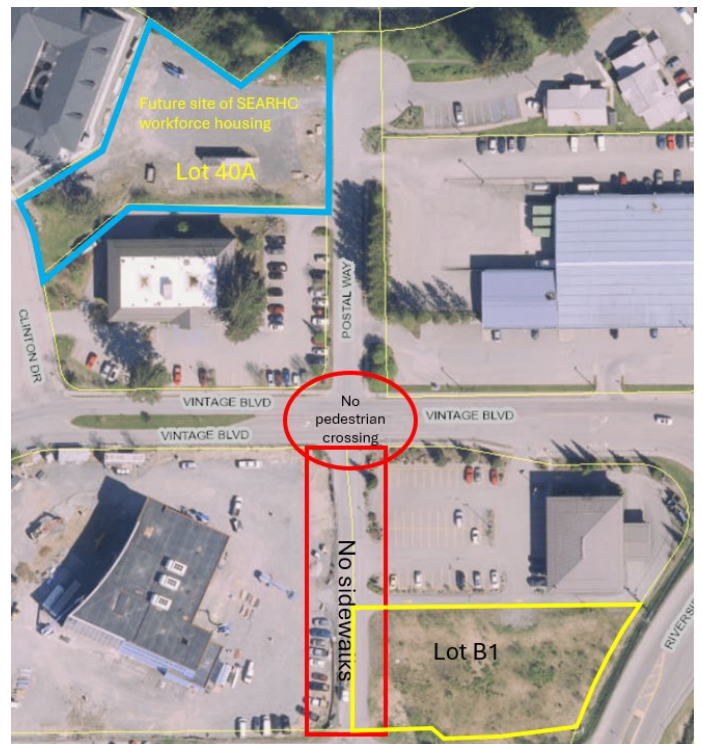
**Condition:** None.

**Hazard Zones** – Lot B1 is in Flood Zone X which, according to the Federal Emergency Management Agency, has only a one (1) percent chance of annual flooding with an average depth of less than one (1) foot. While the Zone X flood zone is not generally considered high risk, staff recommends that the applicant take into account potential future flood risks. A Flood Zone Development Permit is not required for development in this area.

**Condition:** None.

**Public Health, Safety, and Welfare** – The proposed development promotes the public health, safety, and welfare by providing a central location for medical services including primary and urgent care, pediatrics, labs, imaging, behavioral health, optometry, and dental care. This is located within one of Juneau’s most populated areas and within 1,000 feet of the Valley Transit Center.

Public safety and welfare came up as a concern of the Fire Marshall’s during the CBJ agency review period. The intersection of Postal Way and Vintage Boulevard does not have a pedestrian crossing and there are no sidewalks along the access easement leading to Lot B1. While there is a light pole on the east corner of Postal Way, the area is not well lit especially in the wintertime.



**Condition:** At the time of building permit application, fire suppression system plans must be submitted. Suppression system design cannot be deferred.



**Property Value or Neighborhood Harmony** – The proposed development is appropriate for Light Commercial zoning. No information has been submitted that suggests the proposal will reduce property values in the surrounding area.

**Condition:** None.

**AGENCY REVIEW**

CDD conducted an agency review comment period between February 24, 2025, and March 10, 2025, and received the following responses (Attachment F):

Agency	Summary
CDD Building Division	No response received.
General Engineering	No response received.
Capital City Fire & Rescue (CCFR)	The area is not well lit and CCFR responds to frequent requests for assistance at the Riverview Senior Living Center. A crosswalk is recommended. Fire suppression system plans cannot be deferred.

**PUBLIC COMMENTS**

CDD conducted a public comment period between February 24, 2025, and March 6, 2025. Public notice was mailed to property owners within 500 feet of the proposed development (Attachment G). A public notice sign was also posted on-site two weeks prior to the scheduled hearing (Attachment H). No public comments were submitted at time of writing this staff report.

**CONFORMITY WITH ADOPTED PLANS**

The proposed development is in general conformity with the 2009 Non-motorized Transportation Plan, the 2013 Comprehensive Plan, and the 2015 Juneau Economic Development Plan.

PLAN	Chapter	Page No.	Item	Summary
2009 Juneau Non-motorized Transportation Plan	8	73	Policy 10	Provide more bicycle racks. “Secure, covered, well-designed bike racks help encourage residents to take more trips by bicycle and are required for the development of a complete non-motorized network.” Require installation of bike racks as part of new building construction.
2013 Comprehensive Plan	5	44	Policy 5.0	To develop and sustain a diverse economy, providing opportunities for employment for all residents.

PLAN	Chapter	Page No.	Item	Summary
	5	47	Policy 5.4	To encourage and support regional economic development in other cities and locations in Southeast Alaska to create a local environment of services and offerings attractive to commerce originating from outside the CBJ, and to actively participate in development and implementation of regional sustainable development goals.
	10	140	Policy 10.13	To provide for and encourage mixed use development that integrates residential, retail, and office use in downtown areas, shopping centers, along transit corridors, and in other suitable areas.
	11	157	Map 11.8 – TTC	Complies with the TTC designation of the 213 Comprehensive Plan.
	13	213	Policy 13.3	To promote quality medical and social services in the CBJ to ensure the safety, health, well-being, and self-sufficiency of its residents.
2015 Juneau Economic Development Plan	5	58	Objective 2	Increase the breadth and depth of skilled local healthcare workers and services for seniors.
	5	58	Objective 4	Improve senior access to community-based services and activities.

**FINDINGS**

**Conditional Use Permit Criteria** – Per CBJ 49.15.330 (e) & (f), Review of Director's & Commission's Determinations, the Director makes the following findings on the proposed development:

**1. Is the application for the requested Conditional Use Permit complete?**

**Analysis:** No further analysis needed.

**Finding: Yes.** The application contains the information necessary to conduct a review of the proposed operations. The application submittal by the applicant, including the appropriate fees, substantially conforms to the requirements of CBJ Chapter 49.15.

**2. Is the proposed use appropriate according to the Table of Permissible Uses?**

**Analysis:** The application is for a 19,635 square foot, three-story, dental facility. The use is listed at CBJ 49.25.300(7.150) for the LC zoning district.

**Finding: Yes.** The requested permit is appropriate according to the Table of Permissible Uses.

**3. Will the proposed development comply with the other requirements of this chapter?**

**Analysis:** No further analysis needed.

**Finding: Yes.** With the recommended conditions, the proposed development will comply with Title 49, including parking, lighting, vegetative cover, and flood development standards.

**4. Will the proposed development materially endanger the public health, safety, or welfare?**

**Analysis:** The proposed dental offices provide needed medical services in an accessible location. This proposed facility provides an essential community service and promotes the public health, safety, and welfare.

**Finding: No.** There is no information to suggest that with appropriate conditions, the requested medical facility, in an LC zoning district, will materially endanger the public health or safety.

**5. Will the proposed development substantially decrease the value of or be out of harmony with property in the neighboring area?**

**Analysis:** The surrounding area contains a variety of uses. The proposed development is in harmony with the existing developments in the LC zoning district.

**Finding: No.** There is no evidence to suggest that with appropriate conditions, the requested dental facility in an LC zoning district will substantially decrease the value or be out of harmony with the property in the neighboring area.

**6. Will the proposed development be in general conformity with the Land Use Plan, Thoroughfare Plan, or other officially adopted plans?**

**Analysis:** No additional analysis required.

**Finding: Yes.** The proposed dental facility, with the recommended conditions, will be in general conformity with the 2009 Juneau Non-motorized Transportation Plan, the 2013 Comprehensive Plan, and the 2015 Economic Development Plan.

**RECOMMENDATION**

Staff recommends the Planning Commission adopt the Director's analysis and findings and APPROVE WITH CONDITIONS the requested Conditional Use Permit. The permit would allow the development of a 19,635 square foot dental facility in a Light Commercial Zone.

The approval is subject to the following conditions:

1. Prior to the issuance of a building permit, a joint use parking agreement under CBJ 49.40.200(5) must be approved by the CBJ Community Development Department, and must be recorded in the State Recorder's Office prior to issuance of a building permit for the structure proposed on VINTAGE III LT B1. The parking agreement must state that the parking agreement may be nullified when the use on VINTAGE III LT B1 is changed and the CBJ Community Development Department Director has approved the nullification of the agreement that is recorded with the State Recorder's Office.

2. Prior to the issuance of a Temporary Certificate of Occupancy, one or more covered bicycle racks providing spaces for bicycles shall be provided. The rack(s) shall be permanently affixed to the ground, building, or other permanent fixture, and shall be located so that parked bicycles do not encroach into a pedestrian walkway or vehicle area.
3. Prior to the issuance of a Temporary Certificate of Occupancy, the vegetative cover areas shown on the plans must be planted.
4. Prior to the issuance of a building permit for the proposed project, the applicant shall submit a site plan indicating proposed snow storage location(s).
5. At the time of building permit application, fire suppression system plans must be submitted. Suppression system design cannot be deferred.

### **STAFF REPORT ATTACHMENTS**

<b>Item</b>	<b>Description</b>
<b>Attachment A</b>	<i>Application Packet</i>
<b>Attachment B</b>	<i>Plat 84-117 Vintage Subdivision</i>
<b>Attachment C</b>	<i>Plat 2003-29</i>
<b>Attachment D</b>	<i>Plat 2004-45</i>
<b>Attachment E</b>	<i>March 2022 Traffic Impact Analysis</i>
<b>Attachment F</b>	<i>Agency Comments</i>
<b>Attachment G</b>	<i>Abutters Notice</i>
<b>Attachment H</b>	<i>Public Notice Sign Photo</i>
<b>Attachment I</b>	<i>Site Photos</i>





# DEVELOPMENT PERMIT APPLICATION

**NOTE: Development Permit Application forms must accompany all other Community Development Department land use applications. This form and all documents associated with it are public record once submitted.**

To be completed by Applicant	<b>PROPERTY LOCATION</b>	
	Physical Address	None assigned
	Legal Description(s) (Subdivision, Survey, Block, Tract, Lot)	Vintage III Subdivision, Lot B1
	Parcel Number(s)	5B1601430016
	<input type="checkbox"/> This property is located in the downtown historic district <input type="checkbox"/> This property is located in a mapped hazard area, if so, which _____	
	<b>LANDOWNER/ LESSEE</b>	
	Property Owner	Southwest Alaska Regional Health Consortium
	Contact Person	Scott G Martin
	Mailing Address	3100 Channel Drive, Suite 312N, Juneau, AK 99801
	E-mail Address	smartin@searhc.org
Phone Number(s)	907.463.0400	
<b>LANDOWNER/ LESSEE CONSENT</b>		
Required for Planning Permits, not needed on Building/ Engineering Permits.		
Consent is required of all landowners/ lessees. If submitted with the application, alternative written approval may be sufficient. Written approval must include the property location, landowner/ lessee's printed name, signature, and the applicant's name.		
I am (we are) the owner(s) or lessee(s) of the property subject to this application and I (we) consent as follows:		
A. This application for a land use or activity review for development on my (our) property is made with my complete understanding and permission.		
B. I (we) grant permission for the City and Borough of Juneau officials/employees to inspect my property as needed for purposes of this application.		
Southeast Alaska Regional Health Consortium Landowner		
Landowner/Lessee (Printed Name) Title (e.g.: Landowner, Lessee)		
X	 Landowner/Lessee (Signature)	
	1-23-25 Date	
Southeast Alaska Regional Health Consortium Landowner		
Landowner/Lessee (Printed Name) Title (e.g.: Landowner, Lessee)		
X	 Landowner/Lessee (Signature)	
	1-23-25 Date	
NOTICE: The City and Borough of Juneau staff may need access to the subject property during regular business hours. We will make every effort to contact you in advance, but may need to access the property in your absence and in accordance with the consent above. Also, members of the Planning Commission may visit the property before a scheduled public hearing date.		
<b>APPLICANT</b> If same as LANDOWNER, write "SAME"		
Applicant (Printed Name)	Dawson Construction	
Contact Person	Nate Katschke	
Mailing Address	8401 Airport Blvd, Juneau, AK 99801	
E-mail Address	NKatschke@dawson.com	
Phone Number(s)	360.325.5912	
X	 Applicant's Signature	
	1/24/2025 Date of Application	

DEPARTMENT USE ONLY BELOW THIS LINE

**INCOMPLETE APPLICATIONS WILL NOT BE ACCEPTED**

For assistance filling out this form, contact the Permit Center at 586-0770.

Intake Initials	
Case Number	USE 25-007
Date Received	2/13/25



# ALLOWABLE/CONDITIONAL USE PERMIT APPLICATION

See reverse side for more information regarding the permitting process and the materials required for a complete application.

**NOTE: Must be accompanied by a DEVELOPMENT PERMIT APPLICATION form.**

To be completed by Applicant

**PROJECT SUMMARY**

Construction of a Medical Office Building, CBJ Parcel 5B1601430016

**TYPE OF ALLOWABLE OR CONDITIONAL USE PERMIT REQUESTED**

- Accessory Apartment – Accessory Apartment Application (AAP)
- Use Listed in 49.25.300 – Table of Permissible Uses (USE)  
 Table of Permissible Uses Category: 7.150 Health care clinics, other medical treatment facilities

**IS THIS A MODIFICATION or EXTENSION OF AN EXISTING APPROVAL?**

- YES – Case # \_\_\_\_\_  NO

**UTILITIES PROPOSED**

- WATER:  Public  On Site      SEWER:  Public  On Site

**SITE AND BUILDING SPECIFICS**

Total Area of Lot <sup>32,689</sup> \_\_\_\_\_ square feet      Total Area of Existing Structure(s) <sup>0</sup> \_\_\_\_\_ square feet  
 Total Area of Proposed Structure(s) <sup>19,635</sup> \_\_\_\_\_ square feet

**EXTERNAL LIGHTING**

- Existing to remain       No       Yes – Provide fixture information, cutoff sheets, and location of lighting fixtures  
 Proposed                       No       Yes – Provide fixture information, cutoff sheets, and location of lighting fixtures

**ALL REQUIRED DOCUMENTS ATTACHED**

**Narrative including:**

- Current use of land or building(s)
- Description of project, project site, circulation, traffic etc.
- Proposed use of land or building(s)
- How the proposed use complies with the Comprehensive Plan

*If this is a modification or extension include:*

- Notice of Decision and case number
- Justification for the modification or extension
- Application submitted at least 30 days before expiration date

**Plans including:**

- Site plan
- Floor plan(s)
- Elevation view of existing and proposed buildings
- Proposed vegetative cover
- Existing and proposed parking areas and proposed traffic circulation
- Existing physical features of the site (e.g.: drainage, habitat, and hazard areas)

-----DEPARTMENT USE ONLY BELOW THIS LINE-----

ALLOWABLE/CONDITIONAL USE FEES				
	Fees	Check No.	Receipt	Date
Application Fees	\$ <u>750.00</u>			
Admin. of Guarantee	\$ _____			
Adjustment	\$ _____			
Pub. Not. Sign Fee	\$ <u>50.00</u>			
Pub. Not. Sign Deposit	\$ <u>100.00</u>			
<b>Total Fee</b>	<b>\$ <u>400.00</u></b>			

This form and all documents associated with it are public record once submitted.

**INCOMPLETE APPLICATIONS WILL NOT BE ACCEPTED**

For assistance filling out this form, contact the Permit Center at 586-0770.

Case Number	Date Received
<u>USE25-007</u>	<u>2/13/25</u>

### **Allowable/Conditional Use Permit Application Instructions**

Allowable Use permits are outlined in CBJ 49.15.320, Conditional Use permits are outline in CBJ 49.15.330

**Pre-Application Conference:** A pre-application conference is required prior to submitting an application. There is no fee for a pre-application conference. The applicant will meet with City & Borough of Juneau and Agency staff to discuss the proposed development, the permit procedure, and to determine the application fees. To schedule a pre-application conference, please contact the Permit Center at 586-0770 or via e-mail at [permits@juneau.org](mailto:permits@juneau.org).

**Application:** An application for an Allowable/Conditional Use Permit will not be accepted by the Community Development Department until it is determined to be complete. The items needed for a complete application are:

1. **Forms:** Completed Allowable/Conditional Use Permit Application and Development Permit Application forms.
2. **Fees:** Fees generally range from \$350 to \$1,600. Any development, work, or use done without a permit issued will be subject to double fees. All fees are subject to change.
3. **Project Narrative:** A detailed narrative describing the project.
4. **Plans:** All plans are to be drawn to scale and clearly show the items listed below:
  - A. Site plan, floor plan and elevation views of existing and proposed structures
  - B. Existing and proposed parking areas, including dimensions of the spaces, aisle width and driveway entrances
  - C. Proposed traffic circulation within the site including access/egress points and traffic control devices
  - D. Existing and proposed lighting (including cut sheets for each type of lighting)
  - E. Existing and proposed vegetation with location, area, height and type of plantings
  - F. Existing physical features of the site (i.e. drainage, eagle trees, hazard areas, salmon streams, wetlands, etc.)

**Document Format:** All materials submitted as part of an application shall be submitted in either of the following formats:

1. Electronic copies in the following formats: .doc, .txt, .xls, .bmp, .pdf, .jpg, .gif, .xlm, .rtf (other formats may be preapproved by the Community Development Department).
2. Paper copies 11" X 17" or smaller (larger paper size may be preapproved by the Community Development Department).

**Application Review & Hearing Procedure:** Once the application is determined to be complete, the Community Development Department will initiate the review and scheduling of the application. This process includes:

**Review:** As part of the review process the Community Development Department will evaluate the application for consistency with all applicable City & Borough of Juneau codes and adopted plans. Depending on unique characteristics of the permit request the application may be required to be reviewed by other municipal boards and committees. During this review period, the Community Development Department also sends all applications out for a 15-day agency review period. Review comments may require the applicant to provide additional information, clarification, or submit modifications/alterations for the proposed project.

**Hearing:** All Allowable/Conditional Use Permit Applications must be reviewed by the Planning Commission for vote. Once an application has been deemed complete and has been reviewed by all applicable parties the Community Development Department will schedule the requested permit for the next appropriate meeting.

**Public Notice Responsibilities:** Allowable/Conditional Use requests must be given proper public notice as outlined in CBJ 49.15.230:

**The Community Development Department** will give notice of the pending Planning Commission meeting and its agenda in the local newspaper a minimum of 10-days prior to the meeting. Furthermore, CDD will mail notices to all property owners within 500-feet of the project site.

**The Applicant** will post a sign on the site at least 14 days prior to the meeting. The sign shall be visible from a public right-of-way or where determined appropriate by CDD. Signs may be produced by the Community Development Department for a preparation fee of \$50, and a \$100 deposit that will be refunded in full if the sign is returned within seven days of the scheduled hearing date. If the sign is returned between eight and 14 days of the scheduled hearing \$50 may be refunded. The Applicant may make and erect their own sign. Please contact the Community Development Department for more information.

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### **INCOMPLETE APPLICATIONS WILL NOT BE ACCEPTED**



(907) 586-0715  
 CDD\_Admin@juneau.gov  
 www.juneau.org/community-development  
 155 Heritage Way • Juneau, AK 99801

**SEARHC Dental**

Case Number: PAC2025 0002  
 Applicant: RESPEC, Dawson Construction, SEARHC  
 Property Owner: Southeast Alaska Regional Health Consortium  
 Property Address: Unassigned (0 Riverside Drive)  
 Parcel Code Number: 5B1601430016  
 Site Size: 32,689 sq. ft./ 0.75 acre  
 Zoning: Light Commercial (LC)  
 Existing Land Use: Vacant

Conference Date: January 22, 2025  
 Report Issued: January 27, 2025  
***DISCLAIMER: Pre-application conferences are conducted for the purpose of providing applicants with a preliminary review of a project and timeline. Pre-application conferences are not based on a complete application and are not a guarantee of final project approval.***

**List of Attendees**

Note: Copies of the Pre-Application Conference Report will be emailed, instead of mailed, to participants who have provided their email address below.

Name	Title	Email address
Jen Kemp	Applicant	<a href="mailto:Jennifer.Kemp@respec.com">Jennifer.Kemp@respec.com</a>
Kevin Puustinen		<a href="mailto:Kevin.Puustinen@respec.com">Kevin.Puustinen@respec.com</a>
Nate Katschke		<a href="mailto:NKatschke@dawson.com">NKatschke@dawson.com</a>
Scott Veerman		<a href="mailto:Scott@northformak.com">Scott@northformak.com</a>
Ilsa Lund	Planning	<a href="mailto:Ilsa.Lund@juneau.gov">Ilsa.Lund@juneau.gov</a>
Jolene Murphy		<a href="mailto:Jolene.Murphy@juneau.gov">Jolene.Murphy@juneau.gov</a>
Sydney Hawkins	Permitting	<a href="mailto:Sydney.Hawkins@juneau.gov">Sydney.Hawkins@juneau.gov</a>
Jeff Hedges	Building	<a href="mailto:Jeffrey.Hedges@juneau.gov">Jeffrey.Hedges@juneau.gov</a>
Theresa Ross	CCFR, Fire Marshal	<a href="mailto:Theresa.Ross@juneau.gov">Theresa.Ross@juneau.gov</a>



Pre-Application Conference Final Report

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### Conference Summary

#### Questions/issues/agreements identified at the conference that weren't identified in the attached reports.

The following is a list of issues, comments and proposed actions, and requested technical submittal items that were discussed at the pre-application conference.

**Q: How long does the Conditional Use permitting process take? A:** Generally, 5-6 weeks because we are required to provide public notice regarding the meeting according to the Alaska Open Meetings Act.

If a complete application is submitted the week of January 27<sup>th</sup>, the case could make it onto the March 11, 2025 Planning Commission Agenda. If the application is received between February 1-10, the case will be heard at the March 25, 2025 Planning Commission. Below is an overview of the Conditional Use Permit process.

#### Conditional Use Permit Process:

- Submit the application and back-up materials (listed on ten back of the application).
  - Electronic submissions accepted at [Permits@juneau.gov](mailto:Permits@juneau.gov). Note that the permit center will call you for payment when the application is processed. Applications are submitted in the order in which they are received, and it may be a few days before you get a call.
- The project will be assigned to a planner. They will review submitted materials, and coordinate where necessary. When the planner assesses the file is complete, they will schedule a hearing before the Planning Commission.
  - A notice will be sent to property owners within 500 feet of the project.
  - There will be two newspaper ads for the case.
  - The Applicant is required to post a Public Notice sign, which will be provided by CDD. The sign must be posted two weeks before the hearing.
  - Staff will prepare a report analyzing the project, and make a recommendation to the Commission. The report will be publicly available the week before the hearing.
- At the Planning Commission meeting, the project can be:
  - On the Consent Agenda, where it will be passed without discussion.
  - On the Regular Agenda:
    - The Director will briefly describe the project.
    - The Applicant has 15 minutes to describe the project.
    - The public has the opportunity to comment. There is usually a time limit of two to three minutes.
    - The Applicant has time to respond to issues raised.
    - Public comment is closed and there is no additional opportunity to participate.
- The Planning Commission will:
  - Approve the project
  - Approve the project with conditions (the most common outcome)
  - Deny the project
  - Continue the project – if more information is required or if the Commission runs out of time.
- The decision can be appealed for 20 days after the Notice of Decision is filed with the City Clerk. If the decision is appealed, the Applicant can continue with their project at their own risk.

Pre-Application Conference Final Report

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Videos of the Planning Commission activities are posted on Assembly’s Minutes and Agendas site.  
<https://juneau-ak.municodemeetings.com/>

**Project Overview**

Southeast Alaska Regional Health Consortium (SEARHC) is proposing to build a three-story dental clinic in the Vintage Park Subdivision. Some of the parking will be on site, but some will be on a lot across Vintage Boulevard where SEARHC is building workforce housing.

A shared parking agreement will be required for parking to be located off-site, even with the lots being under the same ownership. The agreement will need to be reviewed and approved by the Director of Community Development and, once approved, the agreement will need to be recorded with the Alaska Department of Natural Resources Recorder’s Office. This document will help ensure that all parking requirements are met, particularly if one of the lots undergoes a change of ownership or use.

**Planning Division**

1. **Zoning** – Light Commercial
2. **Table of Permissible Uses** – 7.150- Health care clinics, other medical treatment facilities providing outpatient care
3. **Subdivision** – N/A
4. **Setbacks** –

Yard	Setback minimum (in feet)
Front	25
Rear	10
Side	10
Street side	17

5. **Height** – 45 feet maximum
6. **Access** – Postal Way
7. **Parking & Circulation** – One (1) parking space required for every 200 square feet of gross floor area.
  - a.  $19,635/200 = 98$  parking spaces
  - b. 4 ADA accessible parking spaces required
  - c. 51 provided on site, 47 required in shared parking agreement with lot designated for workforce housing on Postal Way behind True North FCU.

(a) *Joint use.* Joint use occurs when the same off-street parking space is used to meet the parking requirement of different uses at different times. Joint use of off-street parking spaces may be authorized when the developer demonstrates there is no substantial conflict in the principal operating hours of the structures and uses involved and subject to the following requirements:

- (1) Any structure or use sharing the off-street parking facilities of another structure or use must be located within 500 feet of such parking facilities, unless a lesser radius is identified in this chapter. A developer may apply to provide off-street parking in an area greater than 500 feet distant, if approved by the commission.
- (2) The developer demonstrates with appropriate analysis or data that there is no substantial conflict in the principal operating hours of the structures or users for which joint use of off-street parking facilities is proposed.

The developer must present to the director a written instrument, proposed by the parties concerned, providing for joint use of off-street parking facilities. Upon approval by the director, such instrument must be recorded by the developer and documentation of recording provided to the director.

- d. One (1) loading space is required
8. **Lot Coverage** – There are no restrictions on lot coverage in LC except for what is required for parking and vegetative coverage.
9. **Vegetative Coverage** – 15% minimum

## Pre-Application Conference Final Report

10. **Lighting** – Exterior lighting may not shed light or glare above the roofline of the building or beyond the property line of the site.  
Prior to issuance of a building permit, the applicant shall submit a lighting plan illustrating the location and type of exterior lighting proposed for the development. Exterior lighting shall be designed and located to minimize offsite glare. Approval of the plan shall at the discretion of the Community Development Department, according to the requirements at §49.40.230(d)  
All exterior lighting fixtures shall be of a “full cutoff” design.
11. **Noise** – Noise is not expected to be out of character with the surrounding uses in the same zoning district.
12. **Flood** – This property is located within Flood Zone X. No Flood Zone Development Permit will be required.
13. **Hazard/Mass Wasting/Avalanche/Hillside Endorsement** – There are no mapped hazards in this area.
14. **Wetlands** – There are no wetlands on this site according to the National Wetlands Inventory.
15. **Habitat** – The proposed development narrative indicates that there are no eagles’ nests within 600 feet of the development.
16. **Plat or Covenant Restrictions** – N/A
17. **Traffic** – Traffic is not **expected** to be significant enough to require a Traffic Impact Analysis.
18. **Nonconforming situations** – N/A

**Building Division**

19. **Building** – Project requires Alaska licensed design professionals for all elements of the building.
20. **Outstanding Permits** – N/A

**General Engineering/Public Works**

21. **Engineering** – The submittals shall include fixture schedules for water (water fixture units) and for sanitary sewer (drainage fixture units). Any stormwater structures or features shall be shown on the **Site Plan**.

The site is flat, however call out any slopes and retaining structures where applicable.

A **Utility Site Plan** shall be submitted showing the locations of water and sewer lines and shall include sizes and materials, valves and cleanouts, as well as unions, wye’s as well as location of water meter.

Any Right of Way work will require a **ROW permit and Bond**.

22. **Drainage** – The **Site or Grading Plan** shall show how the drainage is managed on the site. The stormwater shall be controlled within the property or divert only to approved drainage ways.

All catch basins culverts and swales shall be shown on the plan and water flow direction to be expressed with squiggled arrows (see Grading Plan checklist).



Pre-Application Conference Final Report

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**23. Utilities –**

- a. **Water** – The submitted **Utility Plan** shall show the water and sewer. It appears that a few water system configurations are being examined. Coordinate with the Water Department and GE for the plan that works best. Configurations that require a new service line will need ROW permit and Bond. This option is only available if there is not an existing service. A fire line will be subject to CBJ Fireline certification.
- b. **Sewer** – It appears that, based on your narrative that the sewer service has been located for connection. The Utility Plan must be submitted with the desired configuration and approved by Water Dept, Wastewater Dept and GE prior to permitting and work.

**Fire Marshal**

1. **Fire Items/Access** – Please verify that access meets IFC Appendix D specifically D104.1
2. Fire (Suppression/alarm) system plans must be submitted with the building permit application. These system designs cannot be deferred. Per the designer this will be a fully sprinklered and alarmed building. Knox Box location will be reviewed and approved during the review process.

**Other Applicable Agency Review**

24. N/A

**List of required applications**

Based upon the information submitted for pre-application review, the following list of applications must be submitted in order for the project to receive a thorough and speedy review.

1. Development Permit Application
2. Conditional Use Permit
3. Any signs are required to be permitted.

**Additional Submittal Requirements**

Submittal of additional information, given the specifics of the development proposal and site, are listed below. These items will be required in order for the application to be determined Counter Complete.

1. A copy of this pre-application conference report.

**Exceptions to Submittal Requirements**

Submittal requirements that staff has determined **not** to be applicable or **not** required, given the specifics of the development proposal, are listed below. These items will **not** be required in order for the application to be reviewed.

1. N/A

**Fee Estimates**

The preliminary plan review fees listed below can be found in the CBJ code section 49.85.

Based upon the project plan submitted for pre-application review, staff has attempted to provide an accurate estimate for the permits and permit fees which will be triggered by your proposal.

Pre-Application Conference Final Report

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1. \$750.00 for Class III Conditional Use Permit (based on size of facility)
2. \$150.00 for public notice sign (\$100 of which is a refundable deposit)

For informational handouts with submittal requirements for development applications, please visit our website at [www.juneau.org/community-development](http://www.juneau.org/community-development).

**Submit your Completed Application**

You may submit your application(s) online via email to [permits@juneau.gov](mailto:permits@juneau.gov)

OR in person with payment made to:

City & Borough of Juneau, Permit Center  
230 South Franklin Street  
Fourth Floor Marine View Center  
Juneau, AK 99801

Phone: (907) 586-0715

Web: [www.juneau.org/community-development](http://www.juneau.org/community-development)

Attachments:

- 49.15.330 – if a Conditional Use Permit
- 49.45 – Signs
- Development Permit Application
- Conditional Use Permit Application



January 2, 2025

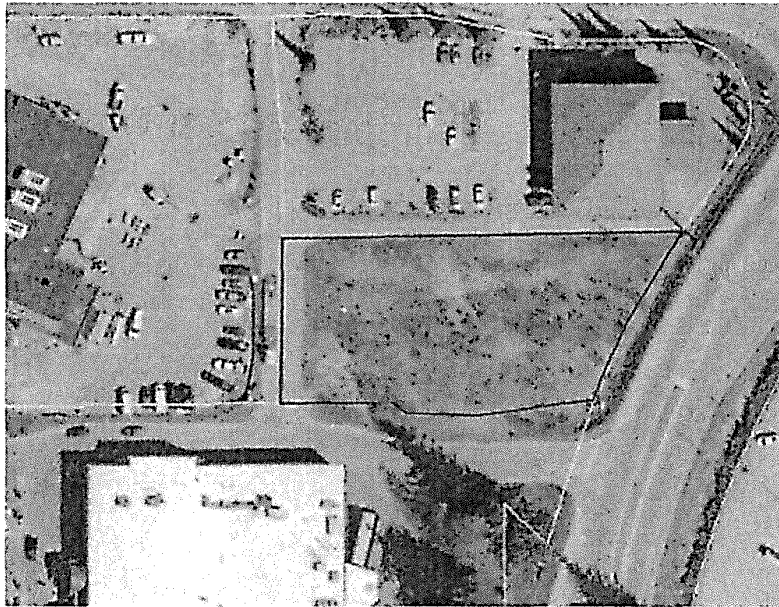
City and Borough of Juneau  
Community Development Department  
155 Heritage Way  
Juneau, AK 99801

Attention: CBJ Building Department

Subject: SEARHC Dental Clinic – Lot B1, Vintage III Subdivision  
Site Grading and Foundation Permit Narrative

To Whom It May Concern,

On behalf of the applicant, Dawson Construction, please consider this permit request to allow for site grading, installation of underground utilities, and foundation construction for a proposed SEARHC Dental Clinic building to be located in Juneau, Alaska. The project site is located on vacant Lot B1, Vintage III Subdivision, identified as O Riverside Drive. The lot is zoned light commercial and is 32,689 square feet. A future building permit application will be submitted for architectural, structural, mechanical, electrical, and civil design.



Project Site Location – SEARHC Dental Clinic

Project Narrative

The proposed project includes the construction of a three-story metal building for SEARHC dental offices. The planned total gross building area is 19,635 square feet. The zoning setbacks for light commercial are as follows:

9109 MENDENHALL MALL RD.  
SUITE 4  
JUNEAU, AK 99801  
907.780.6060

respec.com



- 25' minimum front yard
- 17' minimum street side
- 10' minimum rear and side yard

Site Utilities

Record drawings indicate there is an existing 6" PVC sewer service that is stubbed out and capped near the southeast corner of the project site from a manhole located in Riverside Drive. Sanitary sewer from the dental clinic is planned to be a gravity connection to this existing 6" PVC line.

Two options are being studied for providing domestic/fire protection water to the site. The first option is to extend a new service line approximately 250' north and connect to an existing 12" ductile iron CBJ water main located in Vintage Boulevard near the intersection with Postal Way. The second option is to connect to the Safeway feed line before the Safeway valve, approximately 110' south of the project site. There are 4 existing fire hydrants located within 200' surrounding the project site.

Stormwater runoff from parking surfaces and building roof drains will be collected onsite in a new underground storm drain system. The new storm drain system will connect to an existing storm drain system in Postal Way, located immediately west of the project site. The existing storm drain system drains toward the north for approximately 600' where there is an outfall into the Mendenhall River oxbow.

Electrical and communication utilities for the site will be provided overhead from a utility pole located in Riverside Drive near the northeast corner of the project site.

Parking and Site Access

51 parking stalls (8.5' x 17') are planned onsite, 4 of which will be reserved for ADA. The future SEARHC workforce housing project that will be located 450' north of the proposed dental clinic has 49 parking stalls that have been designated for the dental clinic. A total of 100 parking stalls will be available for the dental clinic building. All Accessible parking stalls and access routes will be graded and signed to meet current ADA guidelines.

Flood Zone

The property is located within Flood Zone X according to the FEMA Flood Zone Panel Maps 02110C1526E and 02110C1527E. The project site is located west of Riverside Drive, between Safeway Grocery and First Bank.

Other Site Discussion Items

No eagle nesting trees are located within 600' radius to this property.

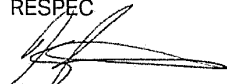
**ATTACHED TO THIS NARRATIVE PLEASE FIND THE FOLLOWING:**

- Conceptual Site Plan
- Conceptual Building Floor Plans and Elevation Views

We appreciate your review of this project narrative. Should you have any questions or need additional information please do not hesitate to contact me at (907) 780-6060.

9109 MENDENHALL MALL RD.  
SUITE 4  
JUNEAU, AK 99801  
907.780.6060

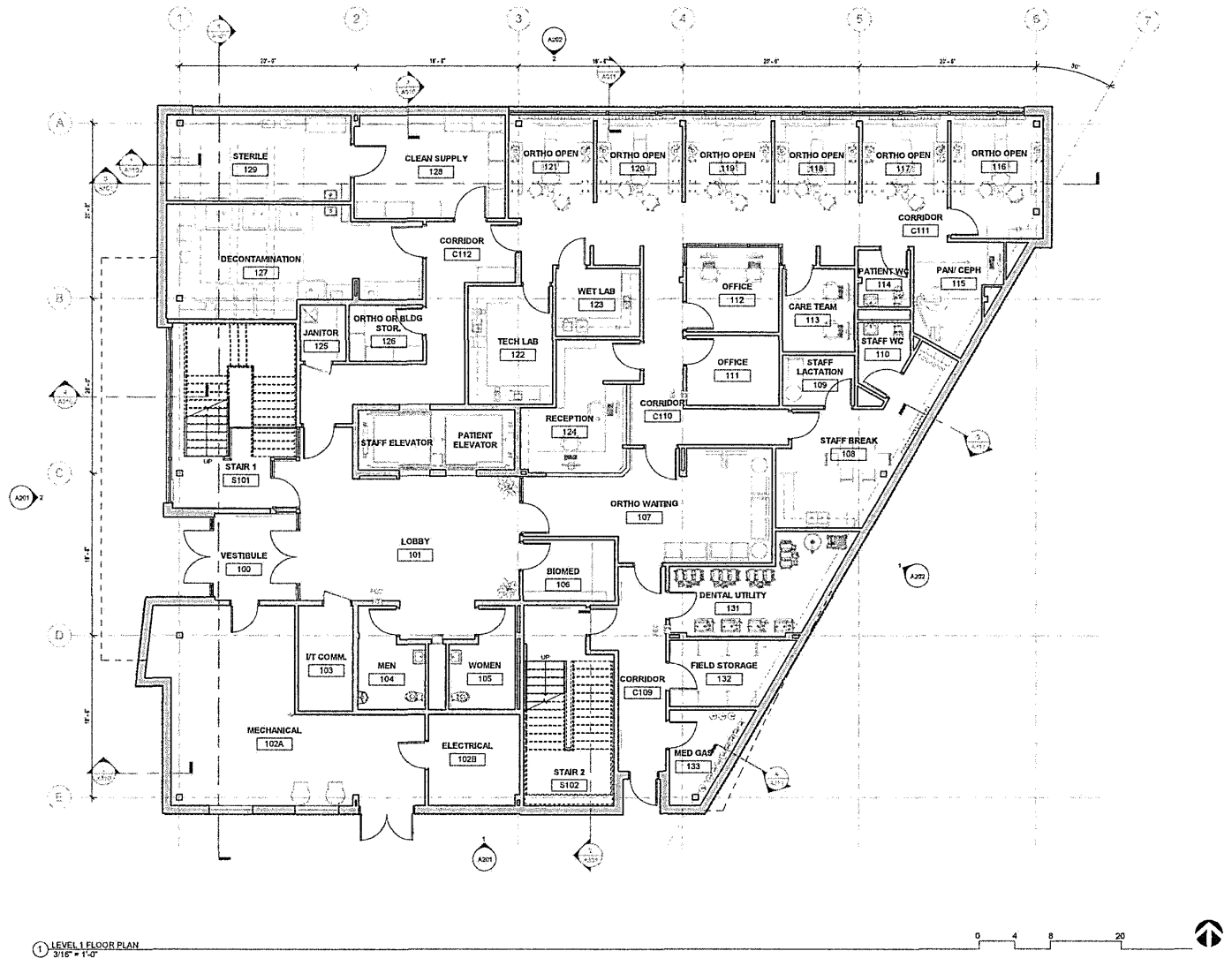
Sincerely,  
RESPEC

  
Kevin Puustinen, PE

Attachments

respec.com





1 LEVEL 1 FLOOR PLAN  
3/16" = 1'-0"

SOUTHEAST ALASKA REGIONAL HEALTH CONSORTIUM  
**VINTAGE PARK  
DENTAL BUILDING**  
JUNEAU, ALASKA

---

**SEARCHC**  
SEARCHC CONSULTING  
FACILITIES MANAGEMENT  
3100 CHAMBERLAIN DRIVE, SUITE 500  
JUNEAU, ALASKA 99801

**Dawson**  
DAWSON CONSTRUCTION  
8421 ARBORVIEW BLVD.  
JUNEAU, ALASKA 99801  
907-760-1500

**NORTHFORM**  
NORTHFORM CONSULTING  
2025 GAMMILL STREET, SUITE 414  
ANCHORAGE, ALASKA 99503  
907-567-9223  
CERT. AUTHORIZATION: 119231

---

Feb. 3, 2025

Rev.	Description	Date

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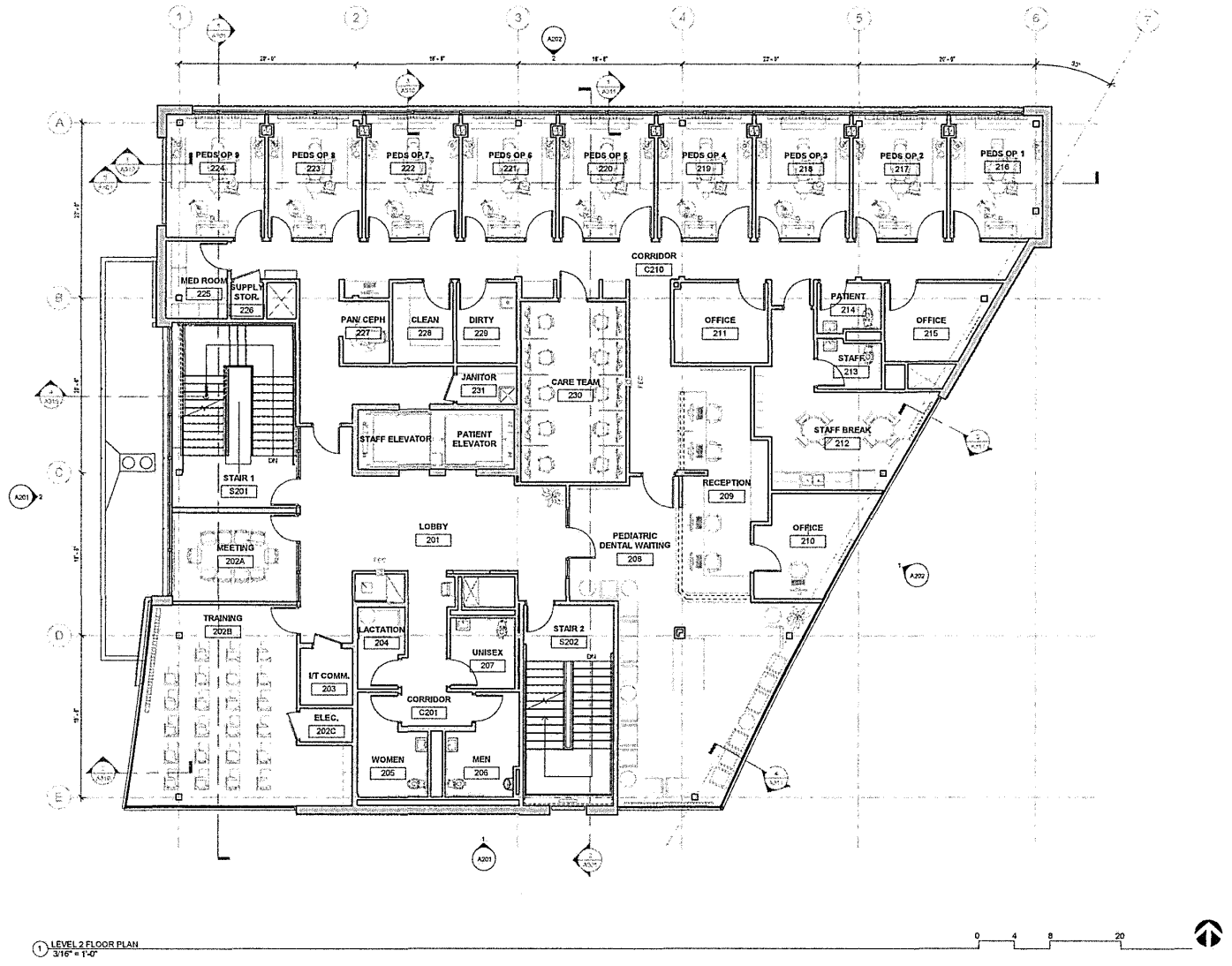
PROJECT NO. 2411.00  
DATE: Feb. 3, 2025  
DRAWN BY: Author  
REVIEWED BY: Checker

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SCHEMATIC DESIGN  
SHEET TITLE  
FLOOR PLAN LEVEL 1

---

SHEET NO.  
**A101**



1 LEVEL 2 FLOOR PLAN  
3/16" = 1'-0"

SOUTHEAST ALASKA REGIONAL HEALTH CONSORTIUM  
VINTAGE PARK  
DENTAL BUILDING  
JUNEAU, ALASKA

**SEARCHC**  
SEARCHC FACILITIES MANAGEMENT  
1100 CHARLES DRIVE, SUITE 300  
JUNEAU, ALASKA 99801

**Dawson**  
DAWSON CONSTRUCTION  
8431 BRIDGE BLVD.  
ANCHORAGE, ALASKA 99503  
207.780.1500

**NORTHFORM**  
ARCHITECTS  
2223 GANIBELL STREET, SUITE 404  
ANCHORAGE, ALASKA 99503  
PO BOX 9000  
CERT. ARCHITECTS 119231

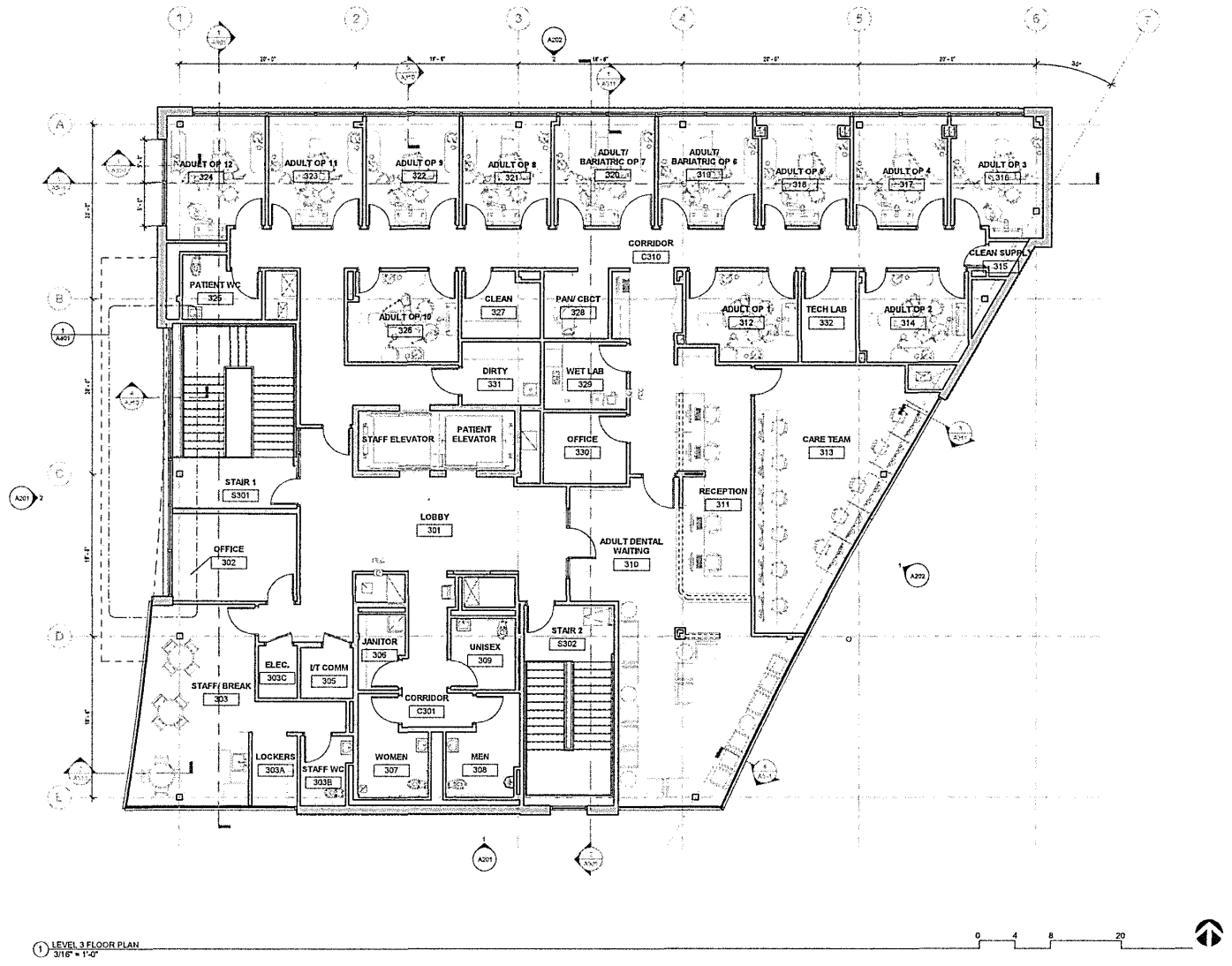
Feb. 3, 2025

Rev.	Description	Date

PROJECT NO. 2411.00  
DATE Feb. 3, 2025  
DRAWN BY: Author  
REVIEWED BY: Checker

SCHEMATIC DESIGN  
SHEET TITLE  
FLOOR PLAN LEVEL 2

SHEET NO.  
A102



1 LEVEL 3 FLOOR PLAN  
3/16" = 1'-0"

SOUTHEAST ALASKA REGIONAL HEALTH CONSORTIUM  
VINTAGE PARK  
DENTAL BUILDING  
JUNEAU, ALASKA

**SEARCHC**  
SEARCHC ARCHITECTURE  
FACILITIES MANAGEMENT  
1100 CHAMBERLAIN DRIVE, SUITE 300  
JUNEAU, ALASKA 99801

**Dawson**  
DAWSON CONSTRUCTION  
803 ARFOOT BLVD.  
ANCHORAGE, ALASKA 99501  
437 769 1503

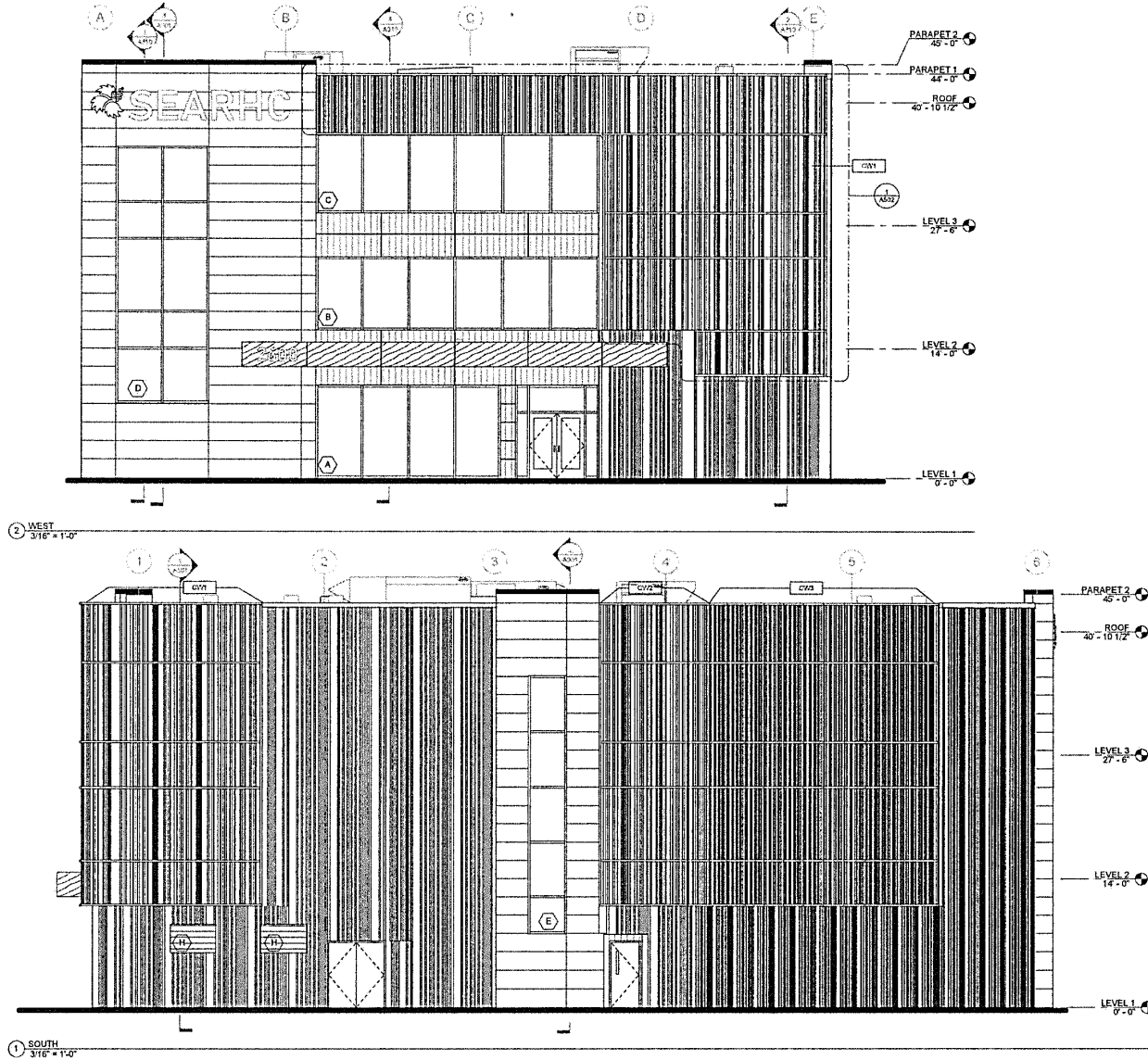
**NORTHFORM**  
NORTHFORM ARCHITECTURE  
2023 GAMMILL STREET, SUITE 414  
ANCHORAGE, ALASKA 99503  
P.O. BOX 90205  
ANCHORAGE, ALASKA 99501

Feb. 3, 2025		
Rev.	Description	Date

PROJECT NO. 2411.00  
DATE Feb. 3, 2025  
DRAWN BY: Author  
REVIEWED BY: Checker

SCHEMATIC DESIGN  
SHEET TITLE  
FLOOR PLAN LEVEL 3

SHEET NO.  
A103



**EXTERIOR ELEVATIONS GENERAL NOTES**

1. ALL MATERIALS SUPPORTS AND EQUIPMENT SHALL BE FINISHED TO MATCH THE MATERIAL.
2. ALL ALUMINUM WINDOW FRAMES, WALL MOUNTED LIGHTS, RECEPTACLES, ETC. SHALL BE FINISHED TO MATCH.
3. AT UPPER FLOOR, MOUNTED FLUSH TO THE PANEL WITH COORDINATE EDGE DETAIL AS SHOWN.
4. AT UPPER FLOOR, MOUNTED TO BLDG. WITH COORDINATE DETAIL AS SHOWN.
5. REFER TO CURTAIN WALL AND WINDOW TYPE FOR FINISH SPECIFICATION. ALUMINUM SHALL BE FINISHED TO MATCH WINDOW TYPE.

**EXTERIOR ELEVATIONS LEGEND**

- METAL PANEL  
M.P. L.P.O. PROCEEDING OR EQUAL  
STYLE: EHV  
COLOR: EHV #11 HAZPET GRAY  
FINISH: UNFIN.  
LENGTH: AS SHOWN ON ELEVATIONS  
THICKNESS: 1/8"
- RESIN PANEL  
R.P. L.P.O. PROCEEDING OR EQUAL, CLIP SYSTEM  
STYLE: EHV  
COLOR: EHV #11 HAZPET GRAY  
FINISH: UNFIN.  
LENGTH: AS SHOWN ON ELEVATIONS  
THICKNESS: 3/8"
- RESIN PANEL  
R.P. L.P.O. PROCEEDING OR EQUAL, CLIP SYSTEM  
STYLE: EHV  
COLOR: EHV #11 HAZPET GRAY  
FINISH: UNFIN.  
LENGTH: AS SHOWN ON ELEVATIONS  
THICKNESS: 3/8"

**KEYNOTES**

Refer to notes on pages 10-11 for details.

SOUTHEAST ALASKA REGIONAL HEALTH CONSORTIUM  
**VINTAGE PARK DENTAL BUILDING**  
 JUNEAU, ALASKA

**SEARCHC**  
 SEARCHC CONSTRUCTION  
 FACILITIES MANAGEMENT  
 3100 CHANNING DRIVE, SUITE 800  
 JUNEAU, ALASKA 99801

**Dawson**  
 DAWSON CONSTRUCTION  
 8431 48TH STREET  
 JUNEAU, ALASKA 99801  
 907-786-1500

**NORTHFORM**  
 5125 CHANNING STREET, SUITE 404  
 ANCHORAGE, ALASKA 99503  
 907-561-9200  
 CERT. ARCHITECTURAL 119221

Feb. 3, 2025

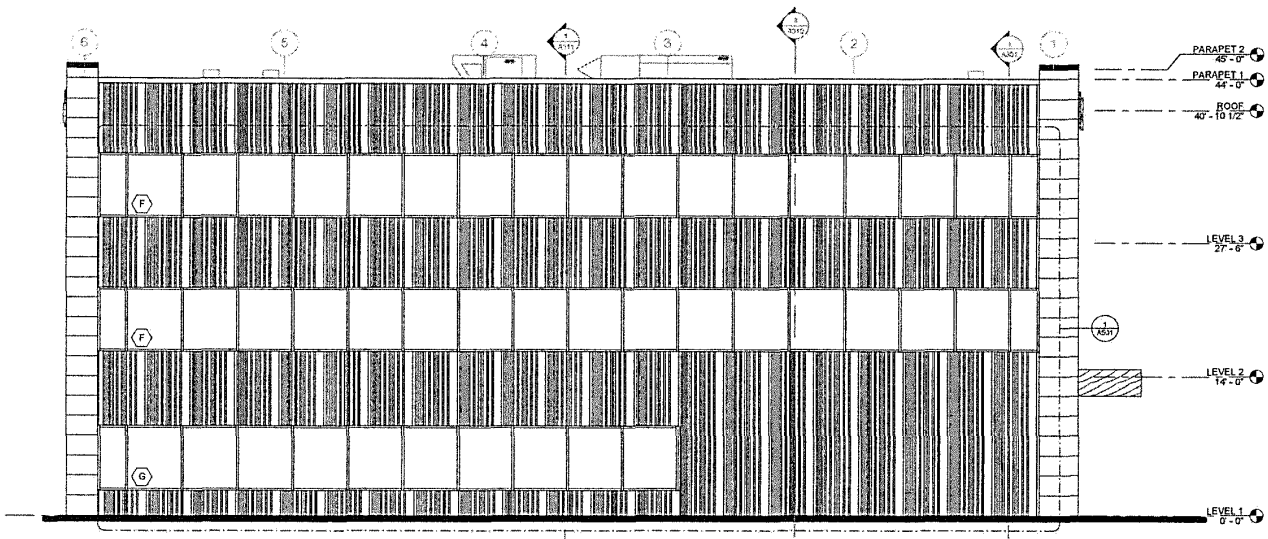
Rev.	Description	Date

PROJECT NO. 2411.00  
 DATE: Feb. 3, 2025  
 DRAWN BY: RSJ  
 REVIEWED BY: SRV

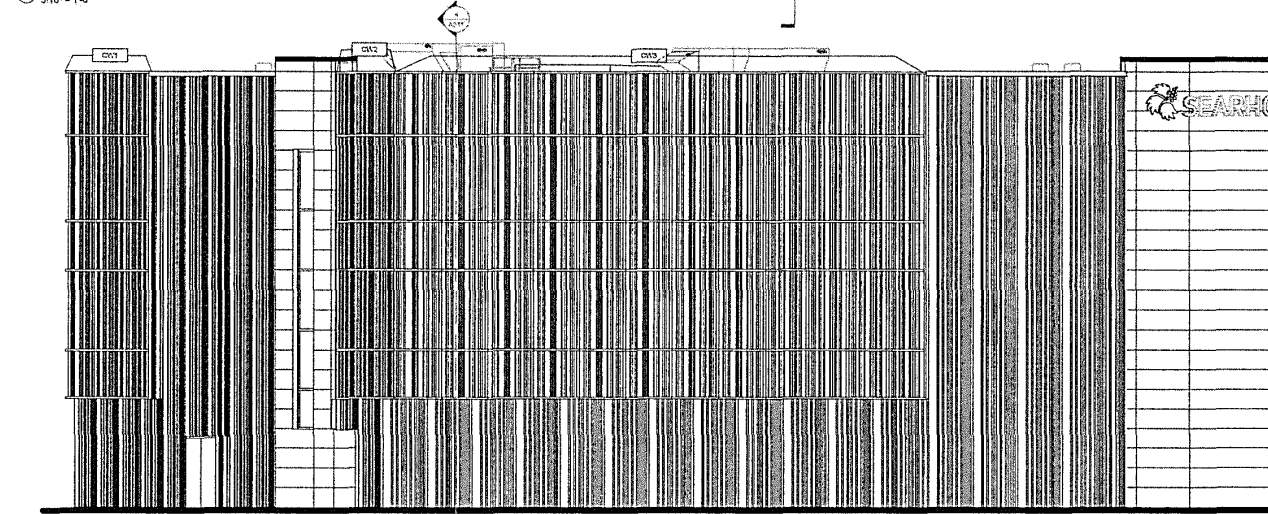
SCHEMATIC DESIGN  
 SHEET TITLE  
 EXTERIOR ELEVATIONS

SHEET NO.  
**A201**





2 NORTH  
3/16" = 1'-0"



1 EAST  
3/16" = 1'-0"

PARAPET 2  
45'-0"

PARAPET 1  
44'-0"

ROOF  
40'-10 1/2"

LEVEL 3  
27'-0"

LEVEL 2  
14'-0"

LEVEL 1  
0'-0"

**EXTERIOR ELEVATIONS GENERAL NOTES**

1. ALL LIGHTING SUPPLY FACILITIES (EQUIPMENT) SHALL BE PAINTED TO MATCH THE BUILDING.
2. ALL BLOBS AND SPACERS (BEANS, WALL MOUNTED LIGHTS, RECEPTACLES, ETC.) SHALL BE PAINTED TO MATCH BUILDING.
3. ALL SPACERS SHALL BE PAINTED TO MATCH BUILDING.
4. ALL SPACERS SHALL BE PAINTED TO MATCH BUILDING.
5. ALL SPACERS SHALL BE PAINTED TO MATCH BUILDING.
6. ALL SPACERS SHALL BE PAINTED TO MATCH BUILDING.
7. ALL SPACERS SHALL BE PAINTED TO MATCH BUILDING.
8. ALL SPACERS SHALL BE PAINTED TO MATCH BUILDING.
9. ALL SPACERS SHALL BE PAINTED TO MATCH BUILDING.
10. ALL SPACERS SHALL BE PAINTED TO MATCH BUILDING.

**EXTERIOR ELEVATIONS LEGEND**

META PANEL  
MATERIAL: METAL PANEL OR EQUAL  
STYLE: 011  
COLOR: 011  
FINISH: 011  
THICKNESS: 1/8"

META PANEL  
MATERIAL: METAL PANEL OR EQUAL CLIP SYSTEM  
STYLE: 011  
COLOR: 011  
FINISH: 011  
THICKNESS: 1/8"

SEAM PANEL  
MATERIAL: SEAM METAL PANEL OR EQUAL  
STYLE: 011  
COLOR: 011  
FINISH: 011  
THICKNESS: 1/8"

SEAM PANEL  
MATERIAL: SEAM METAL PANEL OR EQUAL  
STYLE: 011  
COLOR: 011  
FINISH: 011  
THICKNESS: 1/8"

**KEYNOTES**

1. See Elevation notes attached to this sheet set.

SOUTHEAST ALASKA REGIONAL HEALTH CONSORTIUM

VINTAGE PARK  
DENTAL BUILDING

JUNEAU, ALASKA

**SEARHC**  
SOUTHEAST ALASKA REGIONAL HEALTH CONSORTIUM  
FACILITIES ADMINISTRATION  
3100 CHARLES DRIVE, SUITE 300  
JUNEAU, ALASKA 99801

**Dawson**  
DAWSON CONSTRUCTION  
8431 BIRCH RD.  
JUNEAU, ALASKA 99801  
907-983-5500

**NORTHFORM**  
ARCHITECTS  
2223 GAMBELL STREET, SUITE 404  
ANCHORAGE, ALASKA 99503  
907-561-7000  
CERT. ARCHITECTURE 111931

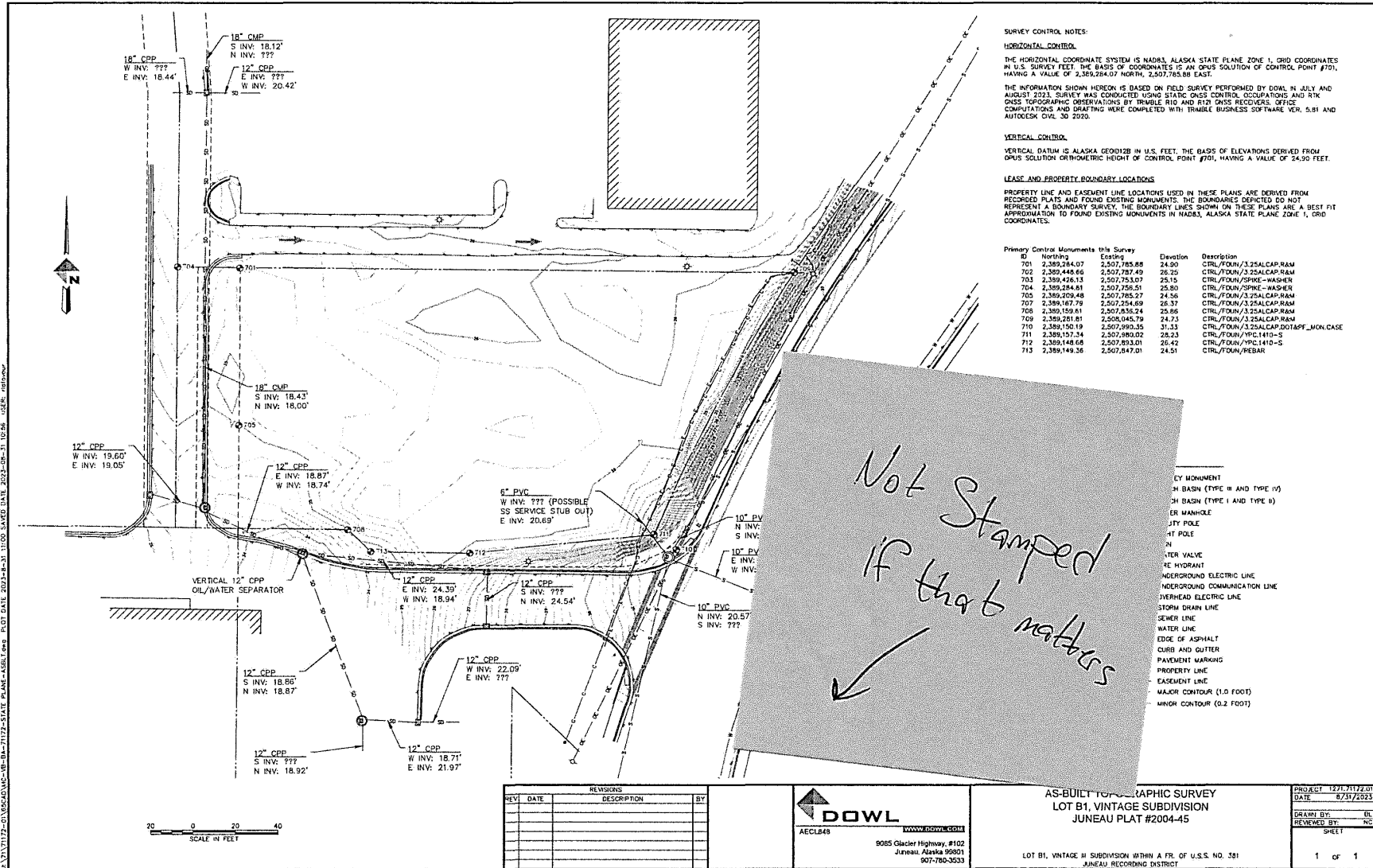
Feb. 3, 2025

Rev.	Description	Date

PROJECT NO. 2411.00  
DATE: Feb. 3, 2025  
DRAWN BY: Author  
REVIEWED BY: Checker

SCHEMATIC DESIGN  
SHEET TITLE  
EXTERIOR ELEVATIONS

SHEET NO.  
A202



**SURVEY CONTROL NOTES:**

**HORIZONTAL CONTROL:**  
 THE HORIZONTAL COORDINATE SYSTEM IS NAD83, ALASKA STATE PLANE ZONE 1, GRID COORDINATES IN U.S. SURVEY FEET. THE BASIS OF COORDINATES IS AN OPUS SOLUTION OF CONTROL POINT #701, HAVING A VALUE OF 2,359,284.07 NORTH, 2,507,765.88 EAST.

THE INFORMATION SHOWN HEREON IS BASED ON FIELD SURVEY PERFORMED BY DOWL IN JULY AND AUGUST 2023. SURVEY WAS CONDUCTED USING STATIC GNSS CONTROL, OCCUPATIONS AND RTK GNSS TOPOGRAPHIC OBSERVATIONS BY TRIMBLE R10 AND R10I GNSS RECEIVERS. OFFICE COMPUTATIONS AND DRAFTING WERE COMPLETED WITH TRIMBLE BUSINESS SOFTWARE VER. 5.81 AND AUTOCAD CIVIL 3D 2020.

**VERTICAL CONTROL:**  
 VERTICAL DATUM IS ALASKA GEOD12B IN U.S. FEET. THE BASIS OF ELEVATIONS DERIVED FROM OPUS SOLUTION ORTHOMETRIC HEIGHT OF CONTROL POINT #701, HAVING A VALUE OF 24.90 FEET.

**LEASE AND EASEMENT BOUNDARY LOCATIONS**  
 PROPERTY LINE AND EASEMENT LINE LOCATIONS USED IN THESE PLANS ARE DERIVED FROM RECORDED PLATS AND FOUND EXISTING MONUMENTS. THE BOUNDARIES DEPICTED DO NOT REPRESENT A BOUNDARY SURVEY. THE BOUNDARY LINES SHOWN ON THESE PLANS ARE A BEST FIT APPROXIMATION TO FOUND EXISTING MONUMENTS IN NAD83, ALASKA STATE PLANE ZONE 1, GRID COORDINATES.

Primary Control Monuments this Survey

ID	Northing	Easting	Elevation	Description
701	2,359,284.07	2,507,765.88	24.90	CTRL/FOUND/3.25ALCAP-R&M
702	2,359,448.66	2,507,787.49	26.75	CTRL/FOUND/3.25ALCAP-R&M
703	2,359,426.13	2,507,753.07	25.15	CTRL/FOUND/SPIKE-WASHER
704	2,359,284.81	2,507,756.51	25.80	CTRL/FOUND/SPIKE-WASHER
705	2,359,209.48	2,507,765.27	24.56	CTRL/FOUND/3.25ALCAP-R&M
707	2,359,167.76	2,507,754.69	26.37	CTRL/FOUND/3.25ALCAP-R&M
708	2,359,159.61	2,507,836.24	25.86	CTRL/FOUND/3.25ALCAP-R&M
709	2,359,281.81	2,508,045.79	24.73	CTRL/FOUND/3.25ALCAP-R&M
710	2,359,150.19	2,507,990.35	21.33	CTRL/FOUND/3.25ALCAP-DOT&FF_MON,CASE
711	2,359,157.34	2,507,980.02	26.23	CTRL/FOUND/PPC1410-S
712	2,359,148.68	2,507,803.01	26.42	CTRL/FOUND/PPC1410-S
713	2,359,149.36	2,507,847.01	24.51	CTRL/FOUND/REBAR

*Not Stamped  
if that matters*

- CH MONUMENT
- 3H BASIN (TYPE III AND TYPE IV)
- 3H BASIN (TYPE I AND TYPE II)
- ER MANHOLE
- JTY POLE
- HT POLE
- NI
- TER VALVE
- RE HYDRANT
- UNDERGROUND ELECTRIC LINE
- UNDERGROUND COMMUNICATION LINE
- OVERHEAD ELECTRIC LINE
- STORM DRAIN LINE
- SEWER LINE
- WATER LINE
- EDGE OF ASPHALT
- CURB AND GUTTER
- PAVEMENT MARKING
- PROPERTY LINE
- EASEMENT LINE
- MAJOR CONTOUR (1.0 FOOT)
- MINOR CONTOUR (0.2 FOOT)

REV	DATE	REVISIONS DESCRIPTION	BY

**DOWL**  
 AECL848  
 www.dowl.com  
 9065 Glacier Highway, #102  
 Juneau, Alaska 99901  
 907-780-3533

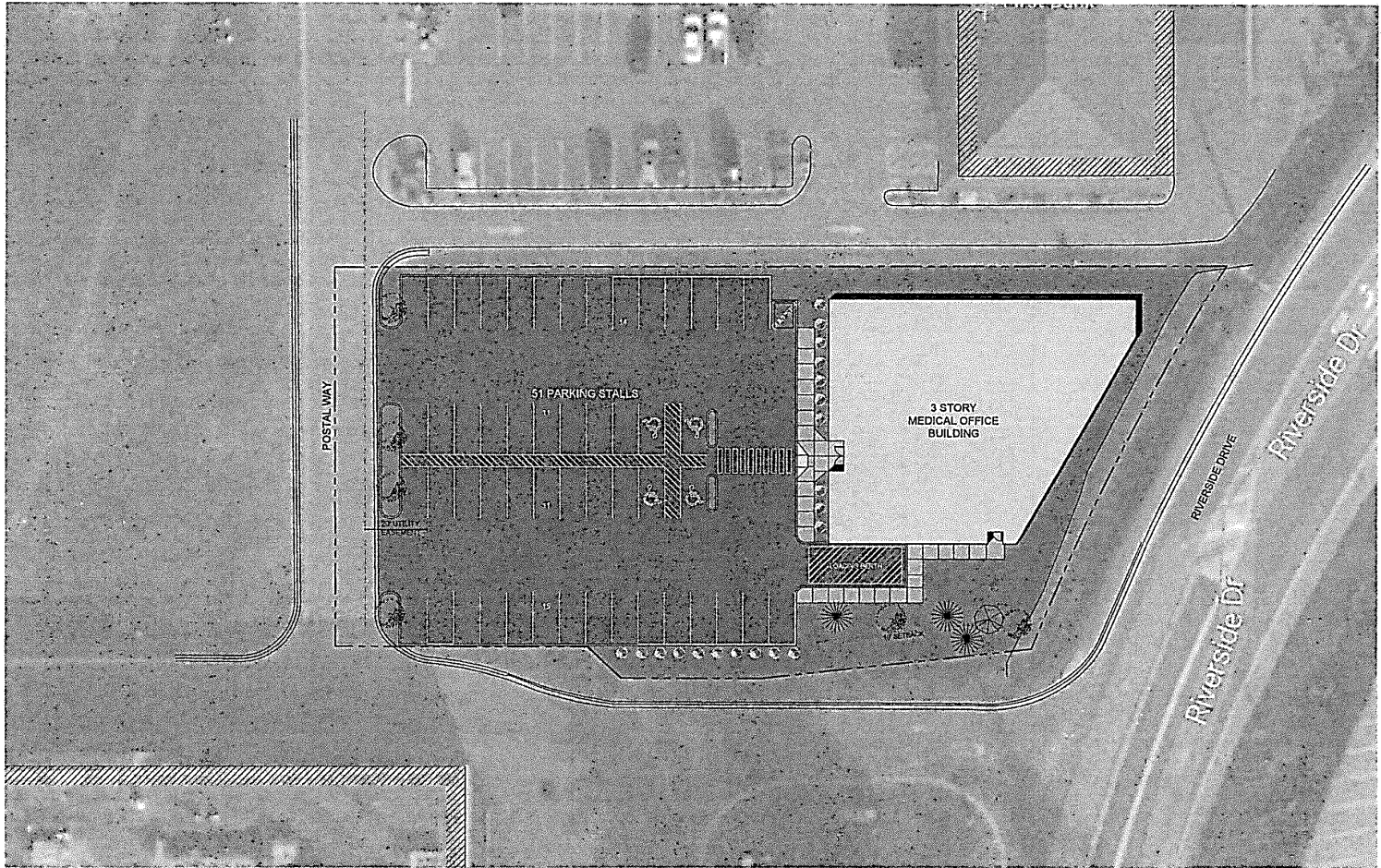
AS-BUILT TOPOGRAPHIC SURVEY  
 LOT B1, VINTAGE SUBDIVISION  
 JUNEAU PLAT #2004-45

LOT B1, VINTAGE III SUBDIVISION WITHIN A FR. OF U.S.S. NO. 381  
 JUNEAU RECORDING DISTRICT

PROJECT	127A-1172-01
DATE	8/31/2023
DRAWN BY:	EL
REVIEWED BY:	NC
SHEET	1 of 1

S:\21\1172-01\654\MC-WB-BA-7112-STATE PLANS-ASBL.dwg PLOT DATE: 2023-08-31 11:00 SAVED DATE: 2023-08-31 10:56 USER: msherman





**VINTAGE III SUBDIVISION, LOT B1**  
LOT SQUARE FEET: 32,689  
ZONE: LIGHT COMMERCIAL (LC)  
VEGETATED COVER: 15% MIN.  
PARKING STALLS: 1 PER 200 GSF



SOUTHEAST ALASKA REGIONAL HEALTH CONSORTIUM

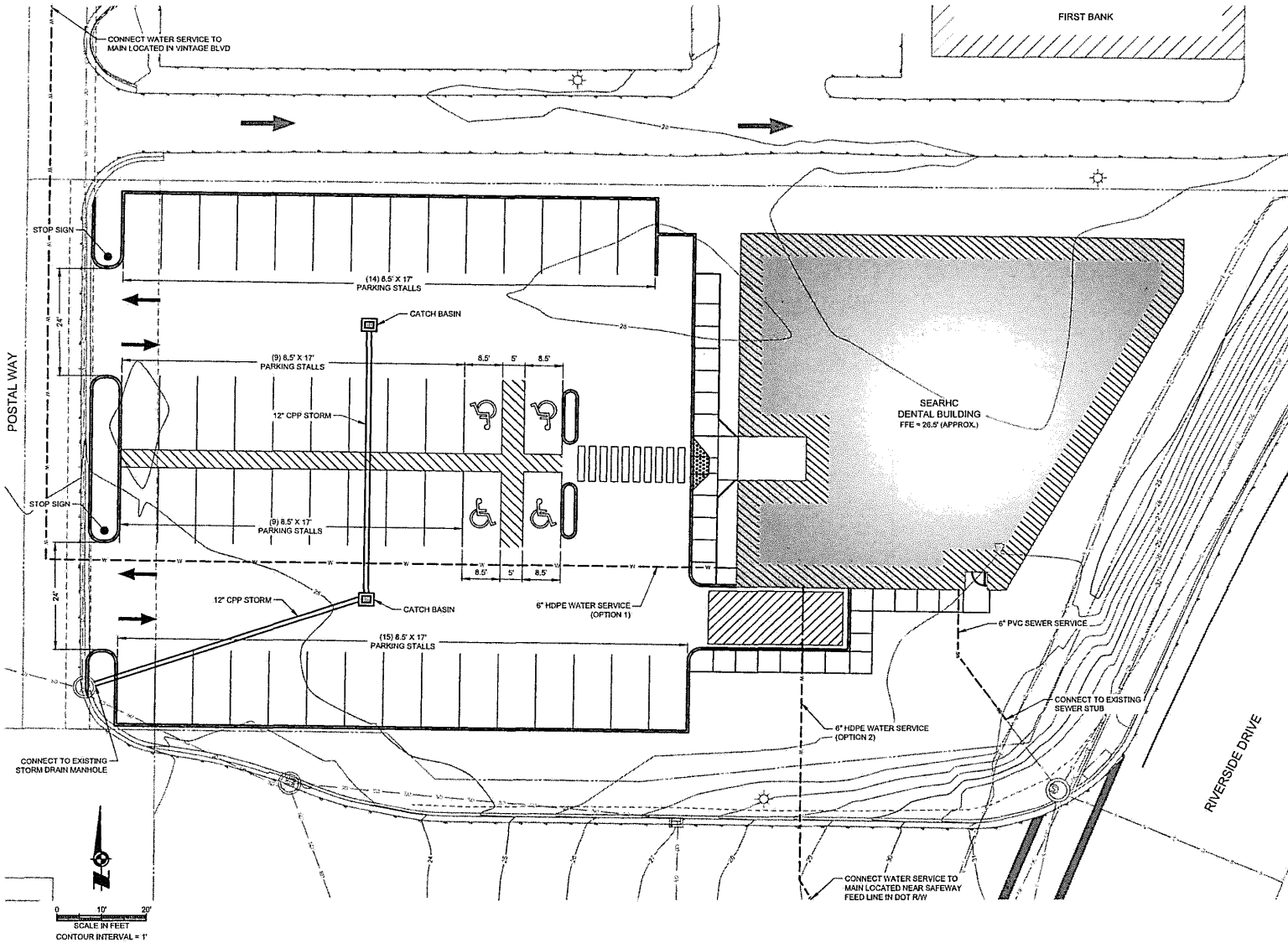


**dawson**

VINTAGE PARK DENTAL BUILDING  
SITE PLAN

8 NOVEMBER 2024





SOUTHEAST ALASKA REGIONAL HEALTH CONSORTIUM  
**VANTAGE PARK  
 DENTAL BUILDING**  
 JUNEAU, ALASKA

---

**SEARHC**  
 SOUTHEAST ALASKA REGIONAL HEALTH CONSORTIUM  
 FACILITIES (MANAGEMENT)  
 3100 CHANDEL DRIVE, SUITE 300  
 JUNEAU, ALASKA 99801

---

**dawson**  
 DAWSON CONSTRUCTION  
 1401 JEFFERSON BLVD.  
 JUNEAU, ALASKA 99801  
 907.583.5500

---

REV.	DESCRIPTION	DATE

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PROJECT NO.: 10407.24.003  
 DATE: JANUARY 24, 2025  
 DRAWN BY: JMS  
 REVIEWED BY: KAP

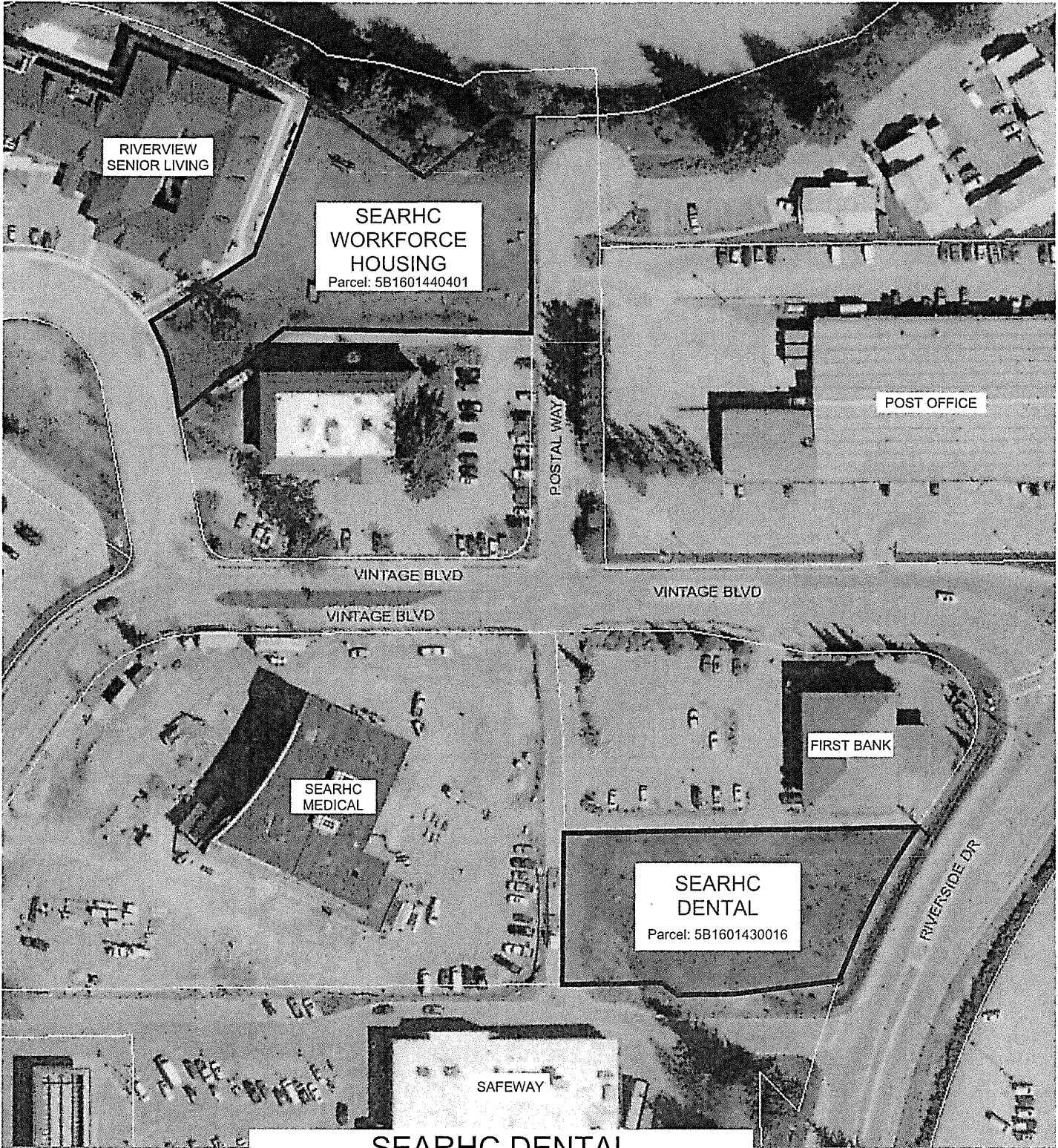
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SCHEMATIC DESIGN  
 SHEET TITLE  
**SITE PLAN**

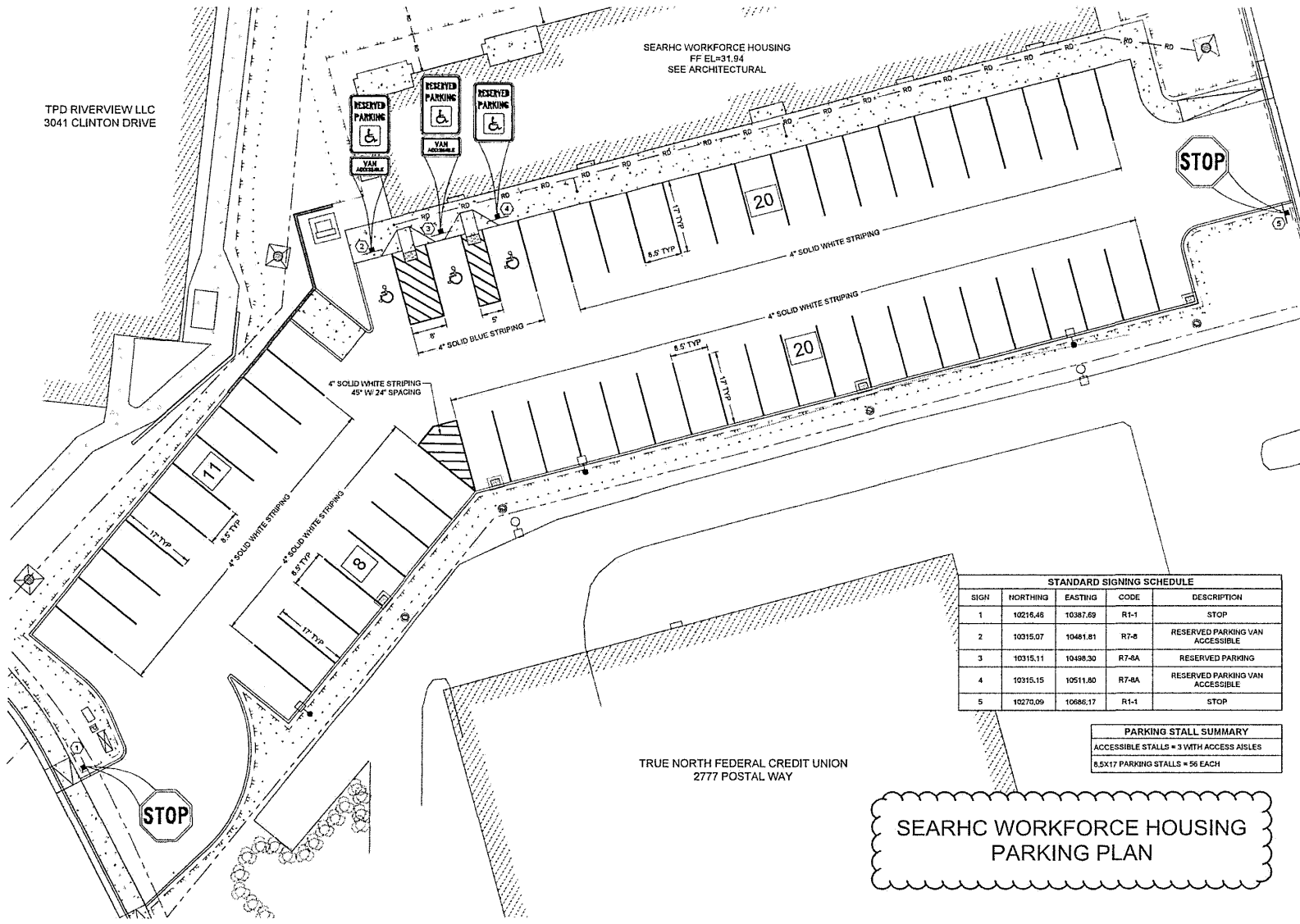
---

SHEET NO.  
**C201**





**SEARHC DENTAL  
PROJECT VICINITY MAP**



STANDARD SIGNING SCHEDULE				
SIGN	NORTHING	EASTING	CODE	DESCRIPTION
1	10216.46	10387.69	R1-1	STOP
2	10315.07	10481.81	R7-B	RESERVED PARKING VAN ACCESSIBLE
3	10315.11	10488.30	R7-6A	RESERVED PARKING
4	10315.15	10511.80	R7-6A	RESERVED PARKING VAN ACCESSIBLE
5	10270.09	10686.17	R1-1	STOP

PARKING STALL SUMMARY	
ACCESSIBLE STALLS = 3 WITH ACCESS AISLES	
8.5X17 PARKING STALLS = 56 EACH	

SEARCHC WORKFORCE HOUSING  
PARKING PLAN

SOUTHEAST ALASKA REGIONAL HEALTH CONSORTIUM  
VINTAGE PARK  
WORKFORCE HOUSING  
JUNEAU, ALASKA

---

SEARCHC

FACILITY MANAGEMENT  
3100 CHARLIE DRIVE, SUITE 300  
JUNEAU, ALASKA 99801

---

dawson

DRAWING CONSULTING  
8421 BRIDGE BLVD.  
JUNEAU, ALASKA 99801  
907.793.1500

---

JUNEAU, AK  
1000 MARSHALL DR. BLDG. 4  
JUNEAU, AK 99801  
PHONE: 907.793.9955  
FAX: 907.793.9971  
WWW.DAWSONAK.COM



STATE OF ALASKA  
49 TH  
JOSEPH W. PUNICH  
GOVERNOR  
1993-2006

REV.	DESCRIPTION	DATE

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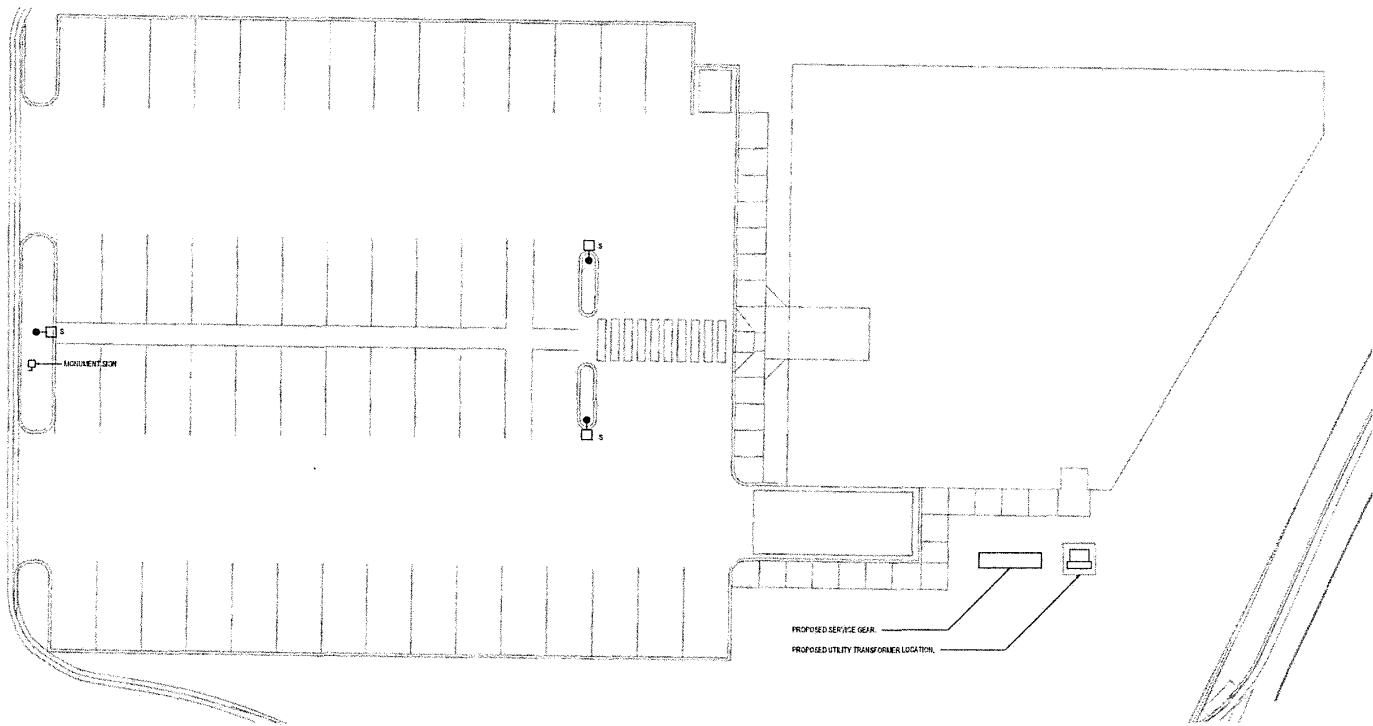
PROJECT NO: 19407.24001  
DATE: NOVEMBER 13, 2024  
DRAWN BY: MSM  
REVIEWED BY: JWP

---

CONSTRUCTION DOCUMENTS  
SHEET TITLE  
SIGNING & STRIPING  
PLAN

---

SHEET NO.  
**C500**



1 SITE PLAN  
SCALE 1" = 10'-0"

LIGHTING FIXTURE SCHEDULE					
TYPE ID	MANUFACTURER MODEL NUMBER	FITTURE DESCRIPTION	LED		MOUNTING TYPE
			LUMENS	WATTAGE	
S	LITHONIA 405X1 LED LS 40K 134 MVOLT SFA 00002	30' LED SITE LIGHT TO MATCH EXISTING AREA MOB TYPE FIXTURES.	16,000	108	30' POLE
			88 W/W		

SOUTHEAST ALASKA REGIONAL HEALTH CONSORTIUM  
VINTAGE PARK  
DENTAL MOB  
JUNEAU, ALASKA

**SEARHC**  
SEARHC HEALTH CARE MANAGEMENT  
FACILITIES MANAGEMENT  
3100 CHANDEL DRIVE, SUITE 200  
JUNEAU, ALASKA 99801

**Dawson**  
DAWSON CONSTRUCTION  
8401 48TH AVENUE  
JUNEAU, ALASKA 99801  
907.740.1530

**ALCAN**  
ELECTRICAL & ENGINEERING

**TS ALASKA**  
TRUCK STOP SERVICES  
11111 11TH AVENUE  
JUNEAU, ALASKA 99801  
907.586.1111

11.13.24

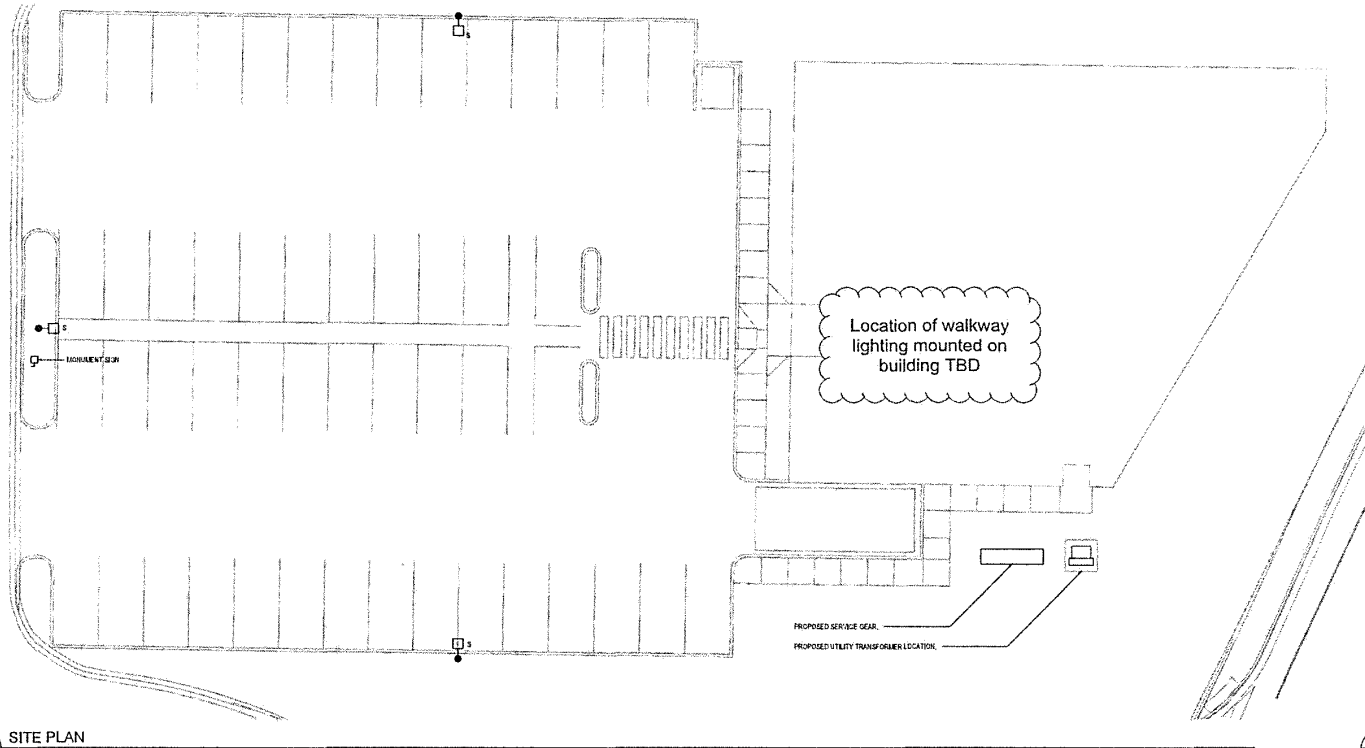
Rev.	Description	Date

PROJECT NO.: 2024-010-0  
DATE: DECEMBER XX, 2024  
DRAWN BY: SR/AMM  
REVIEWED BY: TCA

REVIEW SET

SHEET TITLE  
SITE PLAN

SHEET NO.  
E100



1 SITE PLAN  
SCALE: 1" = 10'-0"

LIGHTING FIXTURE SCHEDULE				
TYPE ID	MANUFACTURER MODEL NUMBER	FEATURE DESCRIPTION	LED LUMENS WATTS	MOUNTING TYPE
S	LITHONIA HOSAY LED L5-40K70M HVOLT SFA 02002	30' LED SITE LIGHT TO MATCH EXISTING AREA MOB TYPE FIXTURES.	18,000 150 85 90W	30' POLE

SOUTHEAST ALASKA REGIONAL HEALTH CONSORTIUM  
VINTAGE PARK  
DENTAL MOB  
JUNEAU, ALASKA

**SEARHC**  
SEARHC HEALTH SERVICES  
FACILITIES MANAGER  
3100 CHARLESTON BLVD. SUITE 200  
ATKINS, ALASKA 99571

**dawson**  
ELECTRICAL CONTRACTORS  
8431 ARPFCT BLVD.  
ANCHORAGE, ALASKA 99507  
P.O. BOX 15397

**ALCAN**  
ELECTRICAL & ENGINEERING

**T3 ALASKA**  
ELECTRICAL CONTRACTORS  
1000 W. 11TH AVE. SUITE 100  
ANCHORAGE, ALASKA 99501

11.13.24

REV.	DESCRIPTION	DATE

PROJECT NO: 2024 010 0  
DATE: DECEMBER XX, 2024  
DRAWN BY: SR/MJM  
REVIEWED BY: TCA

REVIEW SET

SHEET TITLE  
SITE PLAN

SHEET NO.  
E100





## Submittal Cover Sheet

10.07.2022

**Contractor Agency Name:** Southeast Alaska Regional Health Consortium (SEARHC)

**Project Name:** SEARHC Vintage Park Medical Office Building

**Contractor Name:** Dawson Construction, LLC

**Subcontractor Name:** Alcan Electrical & Engineering

**Alcan Job #:** 22-0008

**Supplier:** Graybar Electrical Company, 5501 Anchorage, AK 99518

**Manufacturer:** Various please refer to the product data sheets

**Specification Section:** 26 50 00

**Specification Paragraph:** 1.4.C

**Submittal:** Light Fixtures – Site Lighting & Core and Shell

**Variation:**

**Type:**

**Notes:**


**Variations:**

**Submittal prepared by:** Cassandra Simpson (Barsalou)  
[csimpson@alcanelectric.com](mailto:csimpson@alcanelectric.com)

# SEARCH Juneau Vintage Park

## Alcan Electric



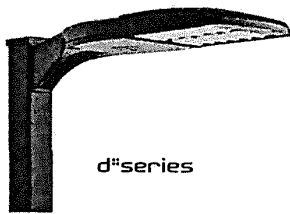
	ALASKA ARCHITECTURAL LIGHTING	Project 22-28191-2 SEARHC Juneau Vintage Park
	Submitted By ALASKA ARCHITECTURAL LIGHTING	

Type	Manufacturer/Brand	Catalog Number
SITE		
SA	ABL-Lithonia Lighting	DSX1 LED P5 40K T5W MVOLT SPA DDBXD
SB	ABL-Lithonia Lighting	DSX1 LED P5 40K T4M MVOLT SPA DDBXD
SC	ABL-Lithonia Lighting	DSX1 LED P5 40K T3M MVOLT SPA DDBXD
SD	ABL-Lithonia Lighting	DSX1 LED P5 40K T5M MVOLT SPA DDBXD
P1	ABL-Lithonia Lighting	SSS 30 4G DM19AS VD DBLXD
P2	ABL-Lithonia Lighting	SSS 30 4G DM28AS VD DBLXD
CORE AND SHELL		
A	ABL-Lithonia Lighting	ZL1N L48 5000LM FST MVOLT 40K 80CRI WH
AE	ABL-Lithonia Lighting	ZL1N L48 5000LM FST MVOLT 40K 80CRI E10WLCP WH
SE	ABL-Lithonia Lighting	WDGE3 LED P2 40K 70CRI RFT MVOLT SRM DDBXD
SK	BEGA/US	55 942-K4
SKE	BEGA/US	55 942-K4
SKE	Evenlite	EMS-55-LC-V3-S
EXW	ABL-Lithonia Lighting	LE S 1 R EL N SD

# Site Lighting



	<b>Project 22-28191-2</b> <b>SEARHC Juneau Vintage Park</b>  Submitted By <b>ALASKA ARCHITECTURAL LIGHTING</b>	Catalog Number: <b>DSX1 LED P5 40K T5W MVOLT SPA DDBXD</b>  Note: <b>VERIFY FINISH ON ALL SITE LIGHTING</b>	Type <b>SA</b>
--	--	---	-------------------



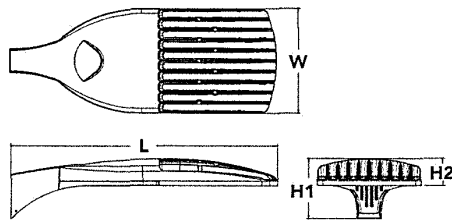
## D-Series Size 1 LED Area Luminaire



Catalog Number	
Notes	
Type	

### Specifications

- EPA:** 1.01 ft<sup>2</sup> (0.09 m<sup>2</sup>)
- Length:** 33" (83.8 cm)
- Width:** 13" (33.0 cm)
- Height H1:** 7-1/2" (19.0 cm)
- Height H2:** 3-1/2"
- Weight (max):** 27 lbs (12.2 kg)



### Introduction

The modern styling of the D-Series is striking yet unobtrusive - making a bold, progressive statement even as it blends seamlessly with its environment. The D-Series distills the benefits of the latest in LED technology into a high performance, high efficacy, long-life luminaire.

The outstanding photometric performance results in sites with excellent uniformity, greater pole spacing and lower power density. It is ideal for replacing up to 750W metal halide in pedestrian and area lighting applications with typical energy savings of 65% and expected service life of over 100,000 hours.

### Ordering Information

EXAMPLE: DSX1 LED P7 40K T3M MVOLT SPA NLTAIR2 PIRHN DDBXD

Series	LEDs	Color temperature	Distribution	Voltage	Mounting	
<b>DSX1 LED</b>	<b>Forward optics</b> P1 P4 <sup>1</sup> P7 <sup>1</sup> P2 P5 <sup>1</sup> P8 P3 P6 <sup>1</sup> P9 <sup>1</sup>  <b>Rotated optics</b> P10 <sup>2</sup> P12 <sup>2</sup> P11 <sup>2</sup> P13 <sup>1,2</sup>	30K 3000 K 40K 4000 K 50K 5000 K	T1S Type I short (Automotive) T2S Type II short T2M Type II medium T3S Type III short T3M Type III medium T4M Type IV medium TFTM Forward throw medium	T5VS Type V very short <sup>1</sup> T5S Type V short <sup>1</sup> T5M Type V medium <sup>1</sup> T5W Type V wide <sup>1</sup> BLC Backlight control <sup>1</sup> LCCO Left corner cutoff <sup>1</sup> RCCO Right corner cutoff <sup>1</sup>	MVOLT <sup>5</sup> XVOLT (277V-480V) <sup>6,24</sup> 120 <sup>9</sup> 208 <sup>9</sup> 240 <sup>9</sup> 277 <sup>9</sup> 347 <sup>9</sup> 480 <sup>9</sup>	<b>Shipped included</b> SPA Square pole mounting RPA Round pole mounting <sup>10</sup> WBA Wall bracket <sup>1</sup> SPUMBA Square pole universal mounting adaptor <sup>11</sup> RPUMBA Round pole universal mounting adaptor <sup>11</sup>  <b>Shipped separately</b> KMA8 DDBXD U Mast arm mounting bracket adaptor (specify finish) <sup>12</sup>


Control options	Other options	Finish (required)
<b>Shipped installed</b> NLTAIR2 nLight AIR generation 2 enabled <sup>14</sup> PIRHN Network, high/low motion/ambient sensor <sup>18</sup> PER NEMA twist-lock receptacle only (controls ordered separate) <sup>15</sup> PER5 Five-pin receptacle only (controls ordered separate) <sup>15,16</sup> PER7 Seven-pin receptacle only (controls ordered separate) <sup>15,16</sup> DMG 0-10v dimming wires pulled outside fixture (for use with an external control, ordered separately) <sup>17</sup> DS Dual switching <sup>14,16,20</sup>	<b>Shipped installed</b> HS House-side shield <sup>22</sup> SF Single fuse (120, 277, 347V) <sup>9</sup> DF Double fuse (208, 240, 480V) <sup>9</sup> L90 Left rotated optics <sup>2</sup> R90 Right rotated optics <sup>2</sup> HA 50°C ambient operations <sup>1</sup> BAA Buy American (n) Act Compliant  <b>Shipped separately</b> BS Bird spikes <sup>24</sup> EGS External glare shield	DDBXD Dark bronze DBLXD Black DNAXD Natural aluminum DWHXD White DDBTXD Textured dark bronze DBLTXD Textured black DNATXD Textured natural aluminum DWHGXD Textured white

PLEASE VERIFY FINISH



One Lithonia Way • Conyers, Georgia 30012 • Phone: 1-800-705-SERV (7378) • www.lithonia.com  
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DSX1-LED  
 Rev. 07/19/21  
 Page 1 of 8

 <p>ALASKA ARCHITECTURAL LIGHTING</p>	<p>Project 22-28191-2 SEARHC Juneau Vintage Park</p> <p>Submitted By ALASKA ARCHITECTURAL LIGHTING</p>	<p>Catalog Number: DSX1 LED P5 40K T5W MVOLT SPA DDBXD</p> <p>Note: VERIFY FINISH ON ALL SITE LIGHTING</p>	<p>Type <b>SA</b></p>
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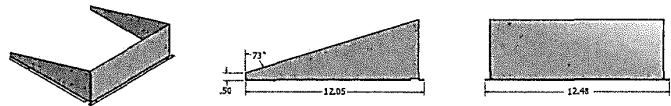
**Ordering Information**

- Accessories**  
Ordered and shipped separately.
- DL1127F 1.5 IU Photocell - SSL twist-lock (120-277V) <sup>23</sup>
  - DL1347F 1.5 CULJU Photocell - SSL twist-lock (847V) <sup>23</sup>
  - DL1480F 1.5 CULJU Photocell - SSL twist-lock (480V) <sup>23</sup>
  - DSH0T SBK U Shorting cap <sup>23</sup>
  - DSKH5 30C U House-side shield for P1, P2, P3, P4 and P5 <sup>24</sup>
  - DSKH5 40C U House-side shield for P6 and P7 <sup>24</sup>
  - DSKH5 60C U House-side shield for P8, P9, P10, P11 and P12 <sup>24</sup>
  - PUMBA DDBXD U\* Square and round pole universal mounting bracket (specify finish) <sup>24</sup>
  - KMAR DDBXD U Mast arm mounting bracket adaptor (specify finish) <sup>24</sup>
  - DSX1EGS (FINISH) U External glare shield
- For more control options, visit [DL1](#) and [ACOM](#) online.*

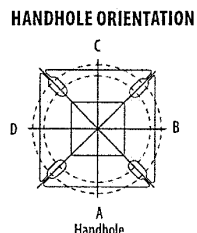
- NOTES**
- 1 RA not available with P4, P5, P6, P7, P9 and P13.
  - 2 P10, P11, P12 and P13 and related options (VSO, RSO) only available together.
  - 3 Any Type 5 distribution with photocell, is not available with WBA.
  - 4 Not available with H4.
  - 5 MVOLT driver operates on any line voltage from 120-277V (50/60 Hz).
  - 6 XVOLT only suitable for use with P3, P5, P6, P7, P9 and P13.
  - 7 XVOLT works with any voltage between 277V and 480V.
  - 8 XVOLT not available with fusing (SF or DF) and not available with PIR, PIRH, PIRFC3V, PIRHFC3V.
  - 9 Single fuse (SF) requires 120V, 277V or 347V. Double fuse (DF) requires 208V, 240V or 480V. XVOLT not available with fusing (SF or DF).
  - 10 Suitable for mounting to round poles between 3.5" and 12" diameter.
  - 11 Universal mounting brackets intended for retrofit on existing, pre-drilled poles only. 1.5 G vibration load rating per ANCI C136.31. Only usable when pole's drill pattern is NOT Lithonia template #8.
  - 12 Must order fixture with SPA option. Must be ordered as a separate accessory; see Accessories information. For use with 2-3/8" diameter mast arm (not included).
  - 13 Must be ordered with PIRHN. Sensor cover available only in dark bronze, black, white and natural aluminum colors.
  - 14 Must be ordered with MLTAR2. For more information on nLight Air 2 visit [this link](#).
  - 15 Photocell ordered and shipped as a separate line item from Acuity Brands Controls. See accessories. Shorting cap included.
  - 16 If ROOMM mode required, it must be ordered and shipped as a separate line item from Acuity Brands Controls. Node with integral dimming.
  - 17 DMG not available with PIRHN, PER5, PER7, PIR, PIRH, PIRFC3V or PIRHFC3V, FAO.
  - 18 Provides 50/50 fixture operation via (2) independent drivers. Not available with PER, PER5, PER7, PIR or PIRH. Not available P1, P2, P3, P4 or P5.
  - 19 Requires (2) separately switched circuits with isolated neutral.
  - 20 Reference Controls Option Default settings table on page 4.
  - 21 Reference Motion Sensor table on page 4 to see functionality.
  - 22 Not available with other dimming controls options.
  - 23 Not available with BLC, LCCO and RCCO distribution. Also available as a separate accessory; see Accessories information.
  - 24 Must be ordered with fixture for factory pre-drilling.
  - 25 Requires luminaire to be specified with PER, PER5 or PER7 option. See Control Option Table on page 4.
  - 26 For retrofit use only. Only usable when pole's drill pattern is NOT Lithonia template #8.

**Options**

EGS - External Glare Shield



**Drilling**

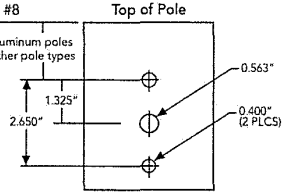


**Tenon Mounting Slipfitter**

Tenon O.D.	Mounting	Single Unit	2 @ 180	2 @ 90	3 @ 90	3 @ 120	4 @ 90
2-3/8"	RPA	AS3-5 190	AS3-5 280	AS3-5 390	AS3-5 320	AS3-5 490	
2-7/8"	RPA	AST25-190	AST25-280	AST25-290	AST25-390	AST25-320	AST25-490
4"	RPA	AST35-190	AST35-280	AST35-290	AST35-390	AST35-320	AST35-490







Mounting Option	Drilling Template	Single	2 @ 180	2 @ 90	3 @ 90	3 @ 120	4 @ 90
Head Location		Side B	Side B & D	Side B & C	Side B, C & D	Round Pole Only	Side A, B, C & D
Drill Nomenclature	#8	DM19AS	DM28AS	DM29AS	DM39AS	DM32AS	DM49AS

Template #8



**DSX1 Area Luminaire - EPA**

\*Includes luminaire and integral mounting arm. Other tenons, arms, brackets or other accessories are not included in this EPA data.

Fixture Quantity & Mounting Configuration	Single DM19	2 @ 180 DM28	2 @ 90 DM29	3 @ 90 DM39	3 @ 120 DM32	4 @ 90 DM49
Mounting Type						
DSX1 LED	1.013	2.025	1.945	3.038	2.850	3.749

**QUOTING BOTH OPTIONS**

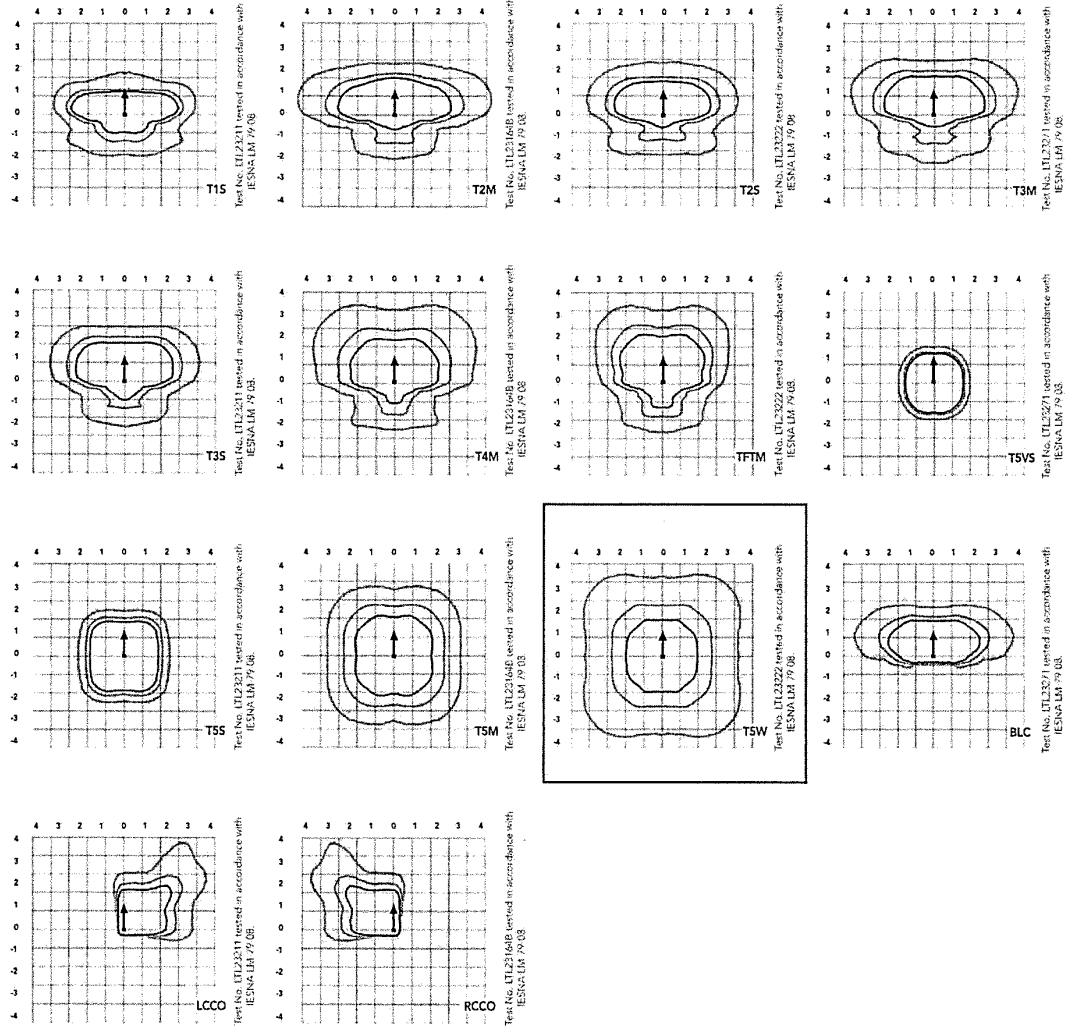
	Drilling Template	Minimum Acceptable Outside Pole Dimension					
SPA	#8	2-7/8"	2-7/8"	3.5"	3.5"	3"	3.5"
RPA	#8	2-7/8"	2-7/8"	3.5"	3.5"	3"	3.5"
SPUMBA	#5	2-7/8"	3"	4"	4"	3.5"	4"
RPUMBA	#5	2-7/8"	3.5"	5"	5"	3.5"	5"


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**Photometric Diagrams** To see complete photometric reports or download .ies files for this product, visit Lithonia Lighting's D-Series Area Size 1 homepage.

Isofootcandle plots for the DSX1 LED 60C 1000 40K. Distances are in units of mounting height (25')

- LEGEND**
- 0.1 fc
  - 0.5 fc
  - 1.0 fc



	ALASKA ARCHITECTURAL LIGHTING Project 22-28191-2 SEARHC Juneau Vintage Park	Catalog Number: DSX1 LED P5 40K T5W MVOLT SPA DDBXD	Type <b>SA</b>
	Submitted By ALASKA ARCHITECTURAL LIGHTING	Note: VERIFY FINISH ON ALL SITE LIGHTING	

**Performance Data**

**Lumen Ambient Temperature (LAT) Multipliers**

Use these factors to determine relative lumen output for average ambient temperatures from 0-40°C (32-104°F).

Ambient	Lumen Multiplier
0°C	1.04
5°C	1.04
10°C	1.03
15°C	1.02
20°C	1.01
25°C	1.00
30°C	0.99
35°C	0.98
40°C	0.97

**Projected LED Lumen Maintenance**

Data references the extrapolated performance projections for the platforms noted in a 25°C ambient, based on 10,000 hours of LED testing (tested per IESNA LM-80-08 and projected per IESNA TM-21-11).

To calculate LLF, use the lumen maintenance factor that corresponds to the desired number of operating hours below. For other lumen maintenance values, contact factory.

Operating Hours	Lumen Maintenance Factor
0	1.00
25,000	0.96
50,000	0.92
100,000	0.85

**Motion Sensor Default Settings**

Option	Dimmed State	High Level (when triggered)	Photocell Operation	Dwell Time	Ramp-up Time	Ramp-down Time
PIR or PIRH	3V (37%) Output	10V (100%) Output	Enabled @ 5FC	5 min	3 sec	5 min
*PIRHFC3V or PIRH1FC3V	3V (37%) Output	10V (100%) Output	Enabled @ 1FC	5 min	3 sec	5 min

\*For use when motion sensor is used as dusk to dawn control.

**Electrical Load**

Performance Package	LED Count	Drive Current	Wattage	Current (A)						
				120	208	240	277	347	480	
Forward Optics (Non-Rotated)	P1	30	530	54	0.45	0.26	0.23	0.19	0.10	0.12
	P2	30	700	70	0.59	0.34	0.30	0.25	0.20	0.16
	P3	30	1050	102	0.86	0.50	0.44	0.38	0.30	0.22
	P4	30	1250	125	1.06	0.60	0.52	0.46	0.37	0.27
	P5	30	1400	138	1.16	0.67	0.58	0.51	0.40	0.29
	P6	40	1250	163	1.36	0.78	0.68	0.59	0.47	0.34
	P7	40	1400	183	1.53	0.88	0.76	0.66	0.53	0.38
	P8	60	1050	207	1.74	0.98	0.87	0.76	0.64	0.49
	P9	60	1250	241	2.01	1.16	1.01	0.89	0.70	0.51
Rotated Optics (Requires L90 or R90)	P10	60	530	106	0.90	0.52	0.47	0.43	0.33	0.27
	P11	60	700	137	1.15	0.67	0.60	0.53	0.42	0.32
	P12	60	1050	207	1.74	0.99	0.87	0.76	0.60	0.46
	P13	60	1250	231	1.93	1.12	0.97	0.86	0.67	0.49

**Controls Options**

Nomenclature	Description	Functionality	Primary control device	Notes
FAO	Field adjustable output device installed inside the luminaire; wired to the driver dimming leads.	Allows the luminaire to be manually dimmed, effectively trimming the light output.	FAO device	Cannot be used with other controls options that need the 0-10V leads
DS	Drivers wired independently for 50/50 luminaire operation	The luminaire is wired to two separate circuits, allowing for 50/50 operation.	Independently wired drivers	Requires two separately switched circuits. Consider nLight AIR as a more cost effective alternative.
PERS or PER7	Twist-lock photocell receptacle	Compatible with standard twist-lock photocells for dusk to dawn operation, or advanced control nodes that provide 0-10V dimming signals.	Twist-lock photocells such as DLL Elite or advanced control nodes such as ROAM.	Pins 4 & 5 to dimming leads on driver, Pins 6 & 7 are capped inside luminaire
PIR or PIRH	Motion sensors with integral photocell. PIR for 8-15' mounting; PIRH for 15-30' mounting	Luminaires dim when no occupancy is detected.	Acuity Controls SBGR	Also available with PIRHFC3V when the sensor photocell is used for dusk-to-dawn operation.
NLTAIR2 PIRHN	nLight AIR enabled luminaire for motion sensing, photocell and wireless communication.	Motion and ambient light sensing with group response. Scheduled dimming with motion sensor over-ride when wirelessly connected to the nLight Eclipse.	nLight Air rSDGR	nLight AIR sensors can be programmed and commissioned from the ground using the CIAIRity Pro app.

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	Submitted By ALASKA ARCHITECTURAL LIGHTING	Note: VERIFY FINISH ON ALL SITE LIGHTING	

**Performance Data**

**Lumen Output**

Lumen values are from photometric tests performed in accordance with IESNA LM-79-08. Data is considered to be representative of the configurations shown, within the tolerances allowed by Lighting Facts Contact factory for performance data on any configurations not shown here.


Forward Optics																							
LED Count	Drive Current	Power Package	System Watts	Dist. Type	30K (3000 K/70 CRI)					40K (4000 K/70 CRI)					50K (5000 K/70 CRI)								
					Lumens	B	U	G	LPW	Lumens	B	U	G	LPW	Lumens	B	U	G	LPW				
30	530	P1	54W	T1S	6,457	2	0	2	120	6,956	2	0	2	129	7,044	2	0	2	130				
				T2S	6,450	2	0	2	119	6,949	2	0	2	129	7,037	2	0	2	130				
				T2M	6,483	1	0	1	120	6,984	2	0	2	129	7,073	2	0	2	131				
				T3S	6,279	2	0	2	116	6,764	2	0	2	125	6,850	2	0	2	127				
				T3M	6,468	1	0	2	120	6,967	1	0	2	129	7,056	1	0	2	131				
				T4M	6,327	1	0	2	117	6,816	1	0	2	126	6,902	1	0	2	128				
				FTFM	6,464	1	0	2	120	6,963	1	0	2	129	7,051	1	0	2	131				
				TSVS	6,722	2	0	0	124	7,242	3	0	0	134	7,334	3	0	0	136				
				TSS	6,728	2	0	1	125	7,248	2	0	1	134	7,340	2	0	1	136				
				TSM	6,711	3	0	1	124	7,229	3	0	1	134	7,321	3	0	1	136				
				TSW	6,667	3	0	2	123	7,182	3	0	2	133	7,273	3	0	2	135				
				BLC	5,299	1	0	1	98	5,709	1	0	2	106	5,781	1	0	2	107				
				LCCO	3,943	1	0	2	73	4,248	1	0	2	79	4,302	1	0	2	80				
				RCCO	3,943	1	0	2	73	4,248	1	0	2	79	4,302	1	0	2	80				
				30	700	P2	70W	T1S	8,249	2	0	2	118	8,886	2	0	2	127	9,999	2	0	2	129
								T2S	8,240	2	0	2	118	8,877	2	0	2	127	9,989	2	0	2	128
T2M	8,283	2	0					2	118	8,923	2	0	2	127	9,036	2	0	2	129				
T3S	8,021	2	0					2	115	8,641	2	0	2	123	8,751	2	0	2	125				
T3M	8,263	2	0					2	118	8,901	2	0	2	127	9,014	2	0	2	129				
T4M	8,083	2	0					2	115	8,708	2	0	2	124	8,818	2	0	2	126				
FTFM	8,257	2	0					2	118	8,896	2	0	2	127	9,008	2	0	2	129				
TSVS	8,588	3	0					0	123	9,252	3	0	0	132	9,369	3	0	0	134				
TSS	8,595	3	0					1	123	9,259	3	0	1	132	9,376	3	0	1	134				
TSM	8,573	3	0					2	122	9,236	3	0	2	132	9,353	3	0	2	134				
TSW	8,517	3	0					2	122	9,175	4	0	2	131	9,291	4	0	2	133				
BLC	6,770	1	0					2	97	7,293	1	0	2	104	7,386	1	0	2	106				
LCCO	5,038	1	0					2	72	5,427	1	0	2	78	5,496	1	0	2	79				
RCCO	5,038	1	0					2	72	5,427	1	0	2	78	5,496	1	0	2	79				
30	1050	P3	102W					T1S	11,661	2	0	2	114	12,562	3	0	3	123	12,721	3	0	3	125
								T2S	11,648	2	0	2	114	12,548	3	0	3	123	12,707	3	0	3	125
				T2M	11,708	2	0	2	115	12,613	2	0	2	124	12,773	2	0	2	125				
				T3S	11,339	2	0	2	111	12,215	3	0	3	120	12,370	3	0	3	121				
				T3M	11,680	2	0	2	115	12,582	2	0	2	123	12,742	2	0	2	125				
				T4M	11,426	2	0	3	112	12,309	2	0	3	121	12,465	2	0	3	122				
				FTFM	11,673	2	0	2	114	12,575	2	0	3	123	12,734	2	0	3	125				
				TSVS	12,140	3	0	1	119	13,078	3	0	1	128	13,244	3	0	1	130				
				TSS	12,150	3	0	1	119	13,089	3	0	1	128	13,254	3	0	1	130				
				TSM	12,119	4	0	2	118	13,056	4	0	2	128	13,221	4	0	2	130				
				TSW	12,040	4	0	3	118	12,970	4	0	3	127	13,134	4	0	3	129				
				BLC	9,570	1	0	2	94	10,310	1	0	2	101	10,440	1	0	2	102				
				LCCO	7,121	1	0	3	70	7,671	1	0	3	75	7,768	1	0	3	76				
				RCCO	7,121	1	0	3	70	7,671	1	0	3	75	7,768	1	0	3	76				
				30	1250	P4	125W	T1S	13,435	3	0	3	107	14,473	3	0	3	116	14,657	3	0	3	117
								T2S	13,421	3	0	3	107	14,458	3	0	3	116	14,641	3	0	3	117
T2M	13,490	2	0					2	108	14,532	3	0	3	116	14,716	3	0	3	118				
T3S	13,064	3	0					3	105	14,074	3	0	3	113	14,252	3	0	3	114				
T3M	13,457	2	0					2	108	14,497	2	0	2	116	14,681	2	0	2	117				
T4M	13,165	2	0					3	105	14,182	2	0	3	113	14,362	2	0	3	115				
FTFM	13,449	2	0					3	108	14,488	2	0	3	116	14,672	2	0	3	117				
TSVS	13,987	4	0					1	112	15,068	4	0	1	121	15,259	4	0	1	122				
TSS	13,999	3	0					1	112	15,080	3	0	1	121	15,271	3	0	1	122				
TSM	13,963	4	0					2	112	15,042	4	0	2	120	15,233	4	0	2	122				
TSW	13,872	4	0					3	111	14,944	4	0	3	120	15,133	4	0	3	121				
BLC	11,027	1	0					2	88	11,879	1	0	2	95	12,029	1	0	2	96				
LCCO	8,205	1	0					3	66	8,839	1	0	3	71	8,951	1	0	3	72				
RCCO	8,205	1	0					3	66	8,839	1	0	3	71	8,951	1	0	3	72				
30	1400	P5	138W					T1S	14,679	3	0	3	106	15,814	3	0	3	115	16,014	3	0	3	116
								T2S	14,664	3	0	3	106	15,797	3	0	3	114	15,997	3	0	3	116
				T2M	14,739	3	0	3	107	15,878	3	0	3	115	16,079	3	0	3	117				
				T3S	14,274	3	0	3	103	15,377	3	0	3	111	15,572	3	0	3	113				
				T3M	14,704	2	0	3	107	15,840	3	0	3	115	16,040	3	0	3	116				
				T4M	14,384	2	0	3	104	15,496	3	0	3	112	15,692	3	0	3	114				
				FTFM	14,695	2	0	3	106	15,830	3	0	3	115	16,030	3	0	3	116				
				TSVS	15,283	4	0	1	111	16,464	4	0	1	119	16,672	4	0	1	121				
				TSS	15,295	3	0	1	111	16,477	4	0	1	119	16,686	4	0	1	121				
				TSM	15,257	4	0	2	111	16,435	4	0	2	119	16,644	4	0	2	121				
				TSW	15,157	4	0	3	110	16,328	4	0	3	118	16,534	4	0	3	120				
				BLC	12,048	1	0	2	87	12,979	1	0	2	94	13,143	1	0	2	95				
				LCCO	8,965	1	0	3	65	9,657	1	0	3	70	9,780	1	0	3	71				
				RCCO	8,965	1	0	3	65	9,657	1	0	3	70	9,780	1	0	3	71				



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	ALASKA ARCHITECTURAL LIGHTING Project 22-28191-2 SEARHC Juneau Vintage Park	Catalog Number: DSX1 LED P5 40K T5W MVOLT SPA DDBXD	Type <b>SA</b>
	Submitted By ALASKA ARCHITECTURAL LIGHTING	Note: VERIFY FINISH ON ALL SITE LIGHTING	

**Performance Data**

**Lumen Output**

Lumen values are from photometric tests performed in accordance with IESNA LM-79-08. Data is considered to be representative of the configurations shown, within the tolerances allowed by Lighting Facts. Contact factory for performance data on any configurations not shown here.

Forward Optics																							
LED Count	Drive Current	Power Package	System Watts	Dist. Type	30k (3000 K, 70 CRI)					40k (4000 K, 70 CRI)					50k (5000 K, 70 CRI)								
					Lumens	B	U	G	LPW	Lumens	B	U	G	LPW	Lumens	B	U	G	LPW				
40	1250	P6	163W	T1S	17,654	3	0	3	108	19,018	3	0	3	117	19,259	3	0	3	118				
				T2S	17,635	3	0	3	108	18,998	3	0	3	117	19,238	3	0	3	118				
				T2M	17,726	3	0	3	109	19,096	3	0	3	117	19,337	3	0	3	119				
				T3S	17,167	3	0	3	105	18,493	3	0	3	113	18,727	3	0	3	115				
				T3M	17,683	3	0	3	108	19,049	3	0	3	117	19,290	3	0	3	118				
				T4M	17,299	3	0	3	106	18,635	3	0	4	114	18,871	3	0	4	116				
				TF1M	17,672	3	0	3	108	19,038	3	0	4	117	19,279	3	0	4	118				
				TSVS	18,379	4	0	1	113	19,800	4	0	1	121	20,050	4	0	1	123				
				TSS	18,394	4	0	2	113	19,816	4	0	2	122	20,066	4	0	2	123				
				TSM	18,348	4	0	2	113	19,766	4	0	2	121	20,016	4	0	2	123				
				TSW	18,228	5	0	3	112	19,636	5	0	3	120	19,885	5	0	3	122				
				BLC	14,489	2	0	2	89	15,609	2	0	3	96	15,806	2	0	3	97				
				LCCO	10,781	1	0	3	66	11,614	1	0	3	71	11,761	2	0	3	72				
				RCCO	10,781	1	0	3	66	11,614	1	0	3	71	11,761	2	0	3	72				
				40	1400	P7	183W	T1S	19,227	3	0	3	105	20,712	3	0	3	113	20,975	3	0	3	115
								T2S	19,206	3	0	3	105	20,690	3	0	3	113	20,952	3	0	3	114
T2M	19,305	3	0					3	105	20,797	3	0	3	114	21,060	3	0	3	115				
T3S	18,696	3	0					3	102	20,141	3	0	3	110	20,396	3	0	4	111				
T3M	19,258	3	0					3	105	20,746	3	0	3	113	21,009	3	0	3	115				
T4M	18,840	3	0					4	103	20,296	3	0	4	111	20,553	3	0	4	112				
TF1M	19,246	3	0					4	105	20,734	3	0	4	113	20,996	3	0	4	115				
TSVS	20,017	4	0					1	109	21,564	4	0	1	118	21,837	4	0	1	119				
TSS	20,033	4	0					2	109	21,581	4	0	2	118	21,854	4	0	2	119				
TSM	19,983	4	0					2	109	21,527	5	0	3	118	21,799	5	0	3	119				
TSW	19,852	5	0					3	108	21,386	5	0	3	117	21,656	5	0	3	118				
BLC	15,780	2	0					3	86	16,999	2	0	3	93	17,214	2	0	3	94				
LCCO	11,742	2	0					3	64	12,649	2	0	3	69	12,809	2	0	3	70				
RCCO	11,742	2	0					3	64	12,649	2	0	3	69	12,809	2	0	3	70				
60	1050	P8	207W					T1S	22,490	3	0	3	109	24,228	3	0	3	117	24,535	3	0	3	119
								T2S	22,466	3	0	4	109	24,202	3	0	4	117	24,509	3	0	4	118
				T2M	22,582	3	0	3	109	24,327	3	0	3	118	24,635	3	0	3	119				
				T3S	21,870	3	0	4	106	23,560	3	0	4	114	23,858	3	0	4	115				
				T3M	22,527	3	0	4	109	24,268	3	0	4	117	24,575	3	0	4	119				
				T4M	22,038	3	0	4	106	23,741	3	0	4	115	24,041	3	0	4	116				
				TF1M	22,513	3	0	4	109	24,253	3	0	4	117	24,560	3	0	4	119				
				TSVS	23,415	5	0	1	113	25,224	5	0	1	122	25,543	5	0	1	123				
				TSS	23,434	4	0	2	113	25,244	4	0	2	122	25,564	4	0	2	123				
				TSM	23,374	5	0	3	113	25,181	5	0	3	122	25,499	5	0	3	123				
				TSW	23,221	5	0	4	112	25,016	5	0	4	121	25,332	5	0	4	122				
				BLC	18,458	2	0	3	89	19,885	2	0	3	96	20,136	2	0	3	97				
				LCCO	13,735	2	0	3	66	14,796	2	0	4	71	14,983	2	0	4	72				
				RCCO	13,735	2	0	3	66	14,796	2	0	4	71	14,983	2	0	4	72				
				60	1250	P9	241W	T1S	25,575	3	0	3	106	27,551	3	0	3	114	27,900	3	0	3	116
								T2S	25,548	3	0	4	106	27,522	3	0	4	114	27,871	3	0	4	116
T2M	25,680	3	0					3	107	27,664	3	0	3	115	28,014	3	0	3	116				
T3S	24,870	3	0					4	103	26,791	3	0	4	111	27,130	3	0	4	113				
T3M	25,617	3	0					4	106	27,597	3	0	4	115	27,946	3	0	4	116				
T4M	25,061	3	0					4	104	26,997	3	0	4	112	27,339	3	0	4	113				
TF1M	25,602	3	0					4	106	27,580	3	0	4	114	27,929	3	0	4	116				
TSVS	26,626	5	0					1	110	28,684	5	0	1	119	29,047	5	0	1	121				
TSS	26,648	4	0					2	111	28,707	5	0	2	119	29,070	5	0	2	121				
TSM	26,581	5	0					3	110	28,635	5	0	3	119	28,997	5	0	3	120				
TSW	26,406	5	0					4	110	28,447	5	0	4	118	28,807	5	0	4	120				
BLC	20,990	2	0					3	87	22,612	2	0	3	94	22,898	2	0	3	95				
LCCO	15,619	2	0					4	65	16,825	2	0	4	70	17,038	2	0	4	71				
RCCO	15,619	2	0					4	65	16,825	2	0	4	70	17,038	2	0	4	71				


 <b>ALASKA ARCHITECTURAL LIGHTING</b>	Project 22-28191-2 <b>SEARHC Juneau Vintage Park</b>	Catalog Number: DSX1 LED P5 40K T5W MVOLT SPA DDBXD	Type <b>SA</b>
	Submitted By <b>ALASKA ARCHITECTURAL LIGHTING</b>	Note: VERIFY FINISH ON ALL SITE LIGHTING	

**Performance Data**

**Lumen Output**

Lumen values are from photometric tests performed in accordance with IESNA LM-79-08. Data is considered to be representative of the configurations shown, within the tolerances allowed by Lighting Facts. Contact factory for performance data on any configurations not shown here.

Rotated Optics																							
LED Count	Drive Current	Power Package	System Watts	Dist. Type	30K (3000 K, 70 CRI)					40K (4000 K, 70 CRI)					50K (5000 K, 70 CRI)								
					Lumens	B	U	G	LPW	Lumens	B	U	G	LPW	Lumens	B	U	G	LPW				
60	530	P10	106W	T1S	13,042	3	0	3	123	14,050	3	0	3	133	14,228	3	0	3	134				
				T2S	12,967	4	0	4	122	13,969	4	0	4	132	14,146	4	0	4	133				
				T2M	13,201	3	0	3	125	14,221	3	0	3	134	14,401	3	0	3	136				
				T3S	12,766	4	0	4	120	13,752	4	0	4	130	13,926	4	0	4	131				
				T3M	13,193	4	0	4	124	14,213	4	0	4	134	14,393	4	0	4	136				
				T4M	12,944	4	0	4	122	13,945	4	0	4	132	14,121	4	0	4	133				
				FTM	13,279	4	0	4	125	14,305	4	0	4	135	14,486	4	0	4	137				
				TSVS	13,372	3	0	1	126	14,405	4	0	1	136	14,588	4	0	1	138				
				TSS	13,260	3	0	1	125	14,284	3	0	1	135	14,465	3	0	1	136				
				TSM	13,256	4	0	2	125	14,281	4	0	2	135	14,462	4	0	2	136				
				TSW	13,137	4	0	3	124	14,153	4	0	3	134	14,332	4	0	3	135				
				BLC	10,906	3	0	3	103	11,749	3	0	3	111	11,898	3	0	3	112				
				LCCO	7,789	1	0	3	73	8,391	1	0	3	79	8,497	1	0	3	80				
				RCCO	7,779	4	0	4	73	8,380	4	0	4	79	8,486	4	0	4	80				
				60	700	P11	137W	T1S	16,356	3	0	3	121	17,835	3	0	3	130	18,061	4	0	4	132
								T2S	16,461	4	0	4	120	17,733	4	0	4	129	17,957	4	0	4	131
T2M	16,758	4	0					4	122	18,053	4	0	4	132	18,281	4	0	4	133				
T3S	16,205	4	0					4	118	17,457	4	0	4	127	17,678	4	0	4	129				
T3M	16,748	4	0					4	122	18,042	4	0	4	132	18,271	4	0	4	133				
T4M	16,432	4	0					4	120	17,702	4	0	4	129	17,926	4	0	4	131				
FTM	16,857	4	0					4	123	18,159	4	0	4	133	18,389	4	0	4	134				
TSVS	16,975	4	0					1	124	18,287	4	0	1	133	18,518	4	0	1	135				
TSS	16,832	4	0					1	123	18,133	4	0	2	132	18,362	4	0	2	134				
TSM	16,828	4	0					2	123	18,128	4	0	2	132	18,358	4	0	2	134				
TSW	16,677	4	0					3	122	17,966	5	0	3	131	18,193	5	0	3	133				
BLC	13,845	3	0					3	101	14,915	3	0	3	109	15,103	3	0	3	110				
LCCO	9,888	1	0					3	72	10,652	2	0	3	78	10,787	2	0	3	79				
RCCO	9,875	4	0					4	72	10,638	4	0	4	78	10,773	4	0	4	79				
60	1050	P12	207W					T1S	22,996	4	0	4	111	24,773	4	0	4	120	25,087	4	0	4	121
								T2S	22,864	4	0	4	110	24,631	5	0	5	119	24,943	5	0	5	120
				T2M	23,277	4	0	4	112	25,075	4	0	4	121	25,393	4	0	4	123				
				T3S	22,509	4	0	4	109	24,248	5	0	5	117	24,555	5	0	5	119				
				T3M	23,263	4	0	4	112	25,061	4	0	4	121	25,378	4	0	4	123				
				T4M	22,824	5	0	5	110	24,588	5	0	5	119	24,899	5	0	5	120				
				FTM	23,414	5	0	5	113	25,223	5	0	5	122	25,543	5	0	5	123				
				TSVS	23,579	5	0	1	114	25,401	5	0	1	123	25,722	5	0	1	124				
				TSS	23,380	4	0	2	113	25,187	4	0	2	122	25,506	4	0	2	123				
				TSM	23,374	5	0	3	113	25,181	5	0	3	122	25,499	5	0	3	123				
				TSW	23,165	5	0	4	112	24,955	5	0	4	121	25,271	5	0	4	122				
				BLC	19,231	4	0	4	93	20,717	4	0	4	100	20,979	4	0	4	101				
				LCCO	13,734	2	0	3	66	14,796	2	0	4	71	14,983	2	0	4	72				
				RCCO	13,716	4	0	4	66	14,776	4	0	4	71	14,963	4	0	4	72				
				60	1250	P13	231W	T1S	25,400	4	0	4	110	27,363	4	0	4	118	27,709	4	0	4	120
								T2S	25,254	5	0	5	109	27,205	5	0	5	118	27,550	5	0	5	119
T2M	25,710	4	0					4	111	27,696	4	0	4	120	28,047	4	0	4	121				
T3S	24,862	5	0					5	108	26,783	5	0	5	116	27,122	5	0	5	117				
T3M	25,695	5	0					5	111	27,680	5	0	5	120	28,031	5	0	5	121				
T4M	25,210	5	0					5	109	27,158	5	0	5	118	27,502	5	0	5	119				
FTM	25,861	5	0					5	112	27,860	5	0	5	121	28,212	5	0	5	122				
TSVS	26,043	5	0					1	113	28,056	5	0	1	121	28,411	5	0	1	123				
TSS	25,824	4	0					2	112	27,819	5	0	2	120	28,172	5	0	2	122				
TSM	25,818	5	0					3	112	27,813	5	0	3	120	28,165	5	0	3	122				
TSW	25,586	5	0					4	111	27,563	5	0	4	119	27,912	5	0	4	121				
BLC	21,241	4	0					4	92	22,882	4	0	4	99	23,172	4	0	4	100				
LCCO	15,170	2	0					4	66	16,342	2	0	4	71	16,549	2	0	4	72				
RCCO	15,150	5	0					5	66	16,321	5	0	5	71	16,527	5	0	5	72				

	<b>ALASKA ARCHITECTURAL LIGHTING</b>	<b>Project 22-28191-2</b> <b>SEARHC Juneau Vintage Park</b>  Submitted By <b>ALASKA ARCHITECTURAL LIGHTING</b>	<b>Catalog Number: DSX1 LED P5 40K T5W MVOLT SPA</b> <b>DDBXD</b>  Note: VERIFY FINISH ON ALL SITE LIGHTING	Type <b>SA</b>
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**FEATURES & SPECIFICATIONS**

**INTENDED USE**

The sleek design of the D-Series Size 1 reflects the embedded high performance LED technology. It is ideal for many commercial and municipal applications, such as parking lots, plazas, campuses, and streetscapes.

**CONSTRUCTION**

Single-piece die-cast aluminum housing has integral heat sink fins to optimize thermal management through conductive and convective cooling. Modular design allows for ease of maintenance and future light engine upgrades. The LED drivers are mounted in direct contact with the casting to promote low operating temperature and long life. Housing is completely sealed against moisture and environmental contaminants (IP65). Low EPA (1.01 ft³) for optimized pole wind loading.

**FINISH**

Exterior parts are protected by a zinc-infused Super Durable TGIC thermoset powder coat finish that provides superior resistance to corrosion and weathering. A tightly controlled multi-stage process ensures a minimum 3 mils thickness for a finish that can withstand extreme climate changes without cracking or peeling. Available in both textured and non-textured finishes.

**OPTICS**

Precision-molded proprietary acrylic lenses are engineered for superior area lighting distribution, uniformity, and pole spacing. Light engines are available in standard 3000 K, 4000 K and 5000 K (70 CRI) configurations. The D-Series Size 1 has zero uplight and qualifies as a Nighttime Friendly™ product, meaning it is consistent with the LEED® and Green Globes™ criteria for eliminating wasteful uplight.

**ELECTRICAL**

Light engine configurations consist of high-efficacy LEDs mounted to metal-core circuit boards to maximize heat dissipation and promote long life (up to L85/100,000 hours at 25°C). Class 1 electronic drivers are designed to have a power factor >90%, THD <20%, and an expected life of 100,000 hours with <1% failure rate. Easily serviceable 10kV surge protection device meets a minimum Category C Low operation (per ANSI/IEEE C62.41.2).

**STANDARD CONTROLS**

The DSX1 LED area luminaire has a number of control options. DSX Size 1, comes standard with 0-10V dimming drivers. Dusk to dawn controls can be utilized via optional NEMA twist-lock photocell receptacles. Integrated motion sensors with on-board photocells feature field-adjustable programming and are suitable for mounting heights up to 30 feet.

**nLIGHT AIR CONTROLS**

The DSX1 LED area luminaire is also available with nLight® AIR for the ultimate in wireless control. This powerful controls platform provides out-of-the-box basic motion sensing and photocontrol functionality and is suitable for mounting heights up to 40 feet. Once commissioned using a smartphone and the easy-to-use CLAIRITY app, nLight AIR equipped luminaires can be grouped, resulting in motion sensor and photocell group response without the need for additional equipment. Scheduled dimming with motion sensor over-ride can be achieved when used with the nLight Eclipse. Additional information about nLight Air can be found here.

**INSTALLATION**

Included mounting block and integral arm facilitate quick and easy installation. Stainless steel bolts fasten the mounting block securely to poles and walls, enabling the D-Series Size 1 to withstand up to a 3.0 G vibration load rating per ANSI C136.31. The D-Series Size 1 utilizes the AERIS™ series pole drilling pattern (template #8). NEMA photocontrol receptacle are also available.

**LISTINGS**

UL listed to meet U.S. and Canadian standards. UL Listed for wet locations. Light engines are IP66 rated; luminaire is IP65 rated. Rated for -40°C minimum ambient. U.S. Patent No. D672,492 S. International patent pending.

DesignLights Consortium® (DLC) Premium qualified product and DLC qualified product. Not all versions of this product may be DLC Premium qualified or DLC qualified. Please check the DLC Qualified Products List at [www.designlights.org/](http://www.designlights.org/) QPL to confirm which versions are qualified.

International Dark-Sky Association (IDA) Fixture Seal of Approval (FSA) is available for all products on this page utilizing 3000K color temperature only.

**BUY AMERICAN**

Product with the BAA option is assembled in the USA and meets the Buy America(n) government procurement requirements under FAR, DFARS and DOT. Please refer to [www.acuitybrands.com/buy-american](http://www.acuitybrands.com/buy-american) for additional information.

**WARRANTY**

5-year limited warranty. Complete warranty terms located at: [www.acuitybrands.com/support/customer-support/terms-and-conditions](http://www.acuitybrands.com/support/customer-support/terms-and-conditions)

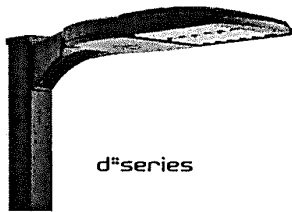
**Note:** Actual performance may differ as a result of end-user environment and application.

All values are design or typical values, measured under laboratory conditions at 25 °C.

Specifications subject to change without notice.



	Project 22-28191-2 SEARHC Juneau Vintage Park  Submitted By ALASKA ARCHITECTURAL LIGHTING	Catalog Number: DSX1 LED P5 40K T4M MVOLT SPA DDBXD  Note:	Type <b>SB</b>
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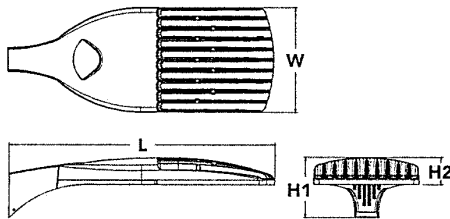
## D-Series Size 1 LED Area Luminaire



Catalog Number	
Notes	
Type	

### Specifications

EPA:	1.01 ft <sup>2</sup> (0.09 m <sup>2</sup> )
Length:	33" (83.8 cm)
Width:	13" (33.0 cm)
Height H1:	7-1/2" (19.0 cm)
Height H2:	3-1/2"
Weight (max):	27 lbs (12.2 kg)



### Introduction

The modern styling of the D-Series is striking yet unobtrusive - making a bold, progressive statement even as it blends seamlessly with its environment. The D-Series distills the benefits of the latest in LED technology into a high performance, high efficacy, long-life luminaire.

The outstanding photometric performance results in sites with excellent uniformity, greater pole spacing and lower power density. It is ideal for replacing up to 750W metal halide in pedestrian and area lighting applications with typical energy savings of 65% and expected service life of over 100,000 hours.

### Ordering Information

EXAMPLE: DSX1 LED P7 40K T3M MVOLT SPA NLTAIR2 PIRHN DDBXD

DSX1 LED						
Series	LEDs	Color temperature	Distribution		Voltage	Mounting
DSX1 LED	<b>Forward optics</b> P1 P4 <sup>1</sup> P7 <sup>1</sup> P2 P5 <sup>1</sup> P8 P3 P6 <sup>1</sup> P9 <sup>1</sup>  <b>Rotated optics</b> P10 <sup>2</sup> P12 <sup>2</sup> P11 <sup>2</sup> P13 <sup>1,2</sup>	30K 3000 K 40K 4000 K 50K 5000 K	T1S Type I short (Automotive) T2S Type II short T2M Type II medium T3S Type III short T3M Type III medium T4M Type IV medium TTFM Forward throw medium	T5VS Type V very short <sup>1</sup> T5S Type V short <sup>1</sup> T5M Type V medium <sup>1</sup> T5W Type V wide <sup>1</sup> BLC Backlight control <sup>4</sup> LCCO Left corner cutoff <sup>4</sup> RCCO Right corner cutoff <sup>4</sup>	MVOLT <sup>5</sup> XVOLT (277V-480V) <sup>6,7,8</sup> 120 <sup>9</sup> 208 <sup>9</sup> 240 <sup>9</sup> 277 <sup>9</sup> 347 <sup>9</sup> 480 <sup>9</sup>	<b>Shipped included</b> SPA Square pole mounting RPA Round pole mounting <sup>10</sup> WBA Wall bracket <sup>1</sup> SPUMBA Square pole universal mounting adaptor <sup>11</sup> RPUMBA Round pole universal mounting adaptor <sup>1</sup>  <b>Shipped separately</b> KMA8 DDBXD U Mast arm mounting bracket adaptor (specify finish) <sup>12</sup>

Control options	Other options	Finish (required)
<b>Shipped installed</b> NLTAIR2 nLight AIR generation 2 enabled <sup>14</sup> PIRHN Network, high/low motion/ambient sensor <sup>14</sup> PER NEMA twist-lock receptacle only (controls ordered separate) <sup>15</sup> PER5 Five-pin receptacle only (controls ordered separate) <sup>15,16</sup> PER7 Seven-pin receptacle only (controls ordered separate) <sup>15,16</sup> DMG 0-10v dimming wires pulled outside fixture (for use with an external control, ordered separately) <sup>17</sup> DS Dual switching <sup>18,19,20</sup>	<b>Shipped installed</b> HS House-side shield <sup>21</sup> SF Single fuse (120, 277, 347V) <sup>9</sup> DF Double fuse (208, 240, 480V) <sup>9</sup> L90 Left rotated optics <sup>2</sup> R90 Right rotated optics <sup>2</sup> HA 50°C ambient operations <sup>1</sup> BAA Buy America(n) Act Compliant  <b>Shipped separately</b> BS Bird spikes <sup>24</sup> EGS External glare shield	DDBXD Dark bronze DBLXD Black DNAXD Natural aluminum DWHXD White DDBTXD Textured dark bronze DBLTXD Textured black DNATXD Textured natural aluminum DWHGXD Textured white

PLEASE VERIFY FINISH

	<b>ALASKA ARCHITECTURAL LIGHTING</b>	Project 22-28191-2 <b>SEARCHC Juneau Vintage Park</b>  Submitted By <b>ALASKA ARCHITECTURAL LIGHTING</b>	Catalog Number: <b>DSX1 LED P5 40K T4M MVOLT SPA DDBXD</b>  Note:	Type <b>SB</b>
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**Ordering Information**

**Accessories**

Ordered and shipped separately.

DLL127F 1.5 JU	Photocell - SSL twist-lock (120-277V) <sup>15</sup>
DLL1347F 1.5 CUL/JU	Photocell - SSL twist-lock (347V) <sup>15</sup>
DLL1480F 1.5 CUL/JU	Photocell - SSL twist-lock (480V) <sup>15</sup>
DSHORT SBK U	Shorting cap <sup>22</sup>
DSX1HS 30C U	House-slide shield for P1, P2, P3, P4 and P5 <sup>14</sup>
DSX1HS 40C U	House-slide shield for P6 and P7 <sup>14</sup>
DSX1HS 60C U	House-slide shield for P8, P9, P10, P11 and P12 <sup>14</sup>
PUMBA DDBXD U*	Separate and round pole universal mounting bracket (specify finish) <sup>16</sup>
XMA8 DDBXD U	Mast arm mounting bracket adaptor (specify finish) <sup>12</sup>
DSX1EGS (FINISH) U	External glare shield

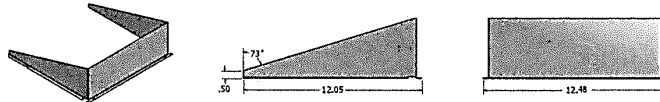
For more control options, visit [LUM](#) and [RQCM](#) online.

**NOTES**

- 1 HA not available with P4, P5, P6, P7, P9 and P13.
- 2 P10, P11, P12 or P13 and related optics (RFO, RFX) only available together.
- 3 Any Type 5 distribution with photocell, is not available with WBA.
- 4 Not available with HS.
- 5 MVOLT driver operates on any line voltage from 120-277V (50/60 Hz).
- 6 XVOLT only suitable for use with P3, P5, P6, P7, P9 and P13.
- 7 XVOLT works with any voltage between 277V and 480V.
- 8 XVOLT not available with fusing (SF or DF) and not available with PIR, PIRH, PIRFC3V, PIRHFC3V.
- 9 Single fuse (SF) requires 120V, 277V or 347V. Double fuse (DF) requires 208V, 240V or 480V. XVOLT not available with fusing (SF or DF).
- 10 Suitable for mounting to round poles between 3.5" and 12" diameter.
- 11 Universal mounting brackets intended for retrofit on existing, pre-drilled poles only. 1.5 G vibration load rating per ANGI C136.31. Only usable when pole's drill pattern is NOT Lithonia template #8.
- 12 Must order fixture with SPA option. Must be ordered as a separate accessory; see Accessories information. For use with 2-3/8" diameter mast arm (not included).
- 13 Must be ordered with PIRHN. Sensor cover available only in dark bronze, black, white and natural aluminum colors.
- 14 Must be ordered with NTAIRZ. For more information on nLight AIR Z, visit [this link](#).
- 15 Photocell ordered and shipped as a separate line item from Acuity Brands Controls. See accessories. Shorting cap included.
- 16 If ROAM\* node required, it must be ordered and shipped as a separate line item from Acuity Brands Controls. Node with integral dimming.
- 17 DMG not available with PIRHN, PER5, PER7, PIR, PIRH, PIRFC3V or PIRHFC3V, FAO.
- 18 Provides 50/50 fixture operation via (2) independent drivers. Not available with PER, PER5, PER7, PIR or PIRH. Not available P1, P2, P3, P4 or P5.
- 19 Requires (2) separately switched circuits with isolated neutral.
- 20 Reference Controls Option Default settings table on page 4.
- 21 Reference Motion Sensor table on page 4 to see functionality.
- 22 Not available with other dimming controls options.
- 23 Not available with BLC, LCCO and RCCO distribution. Also available as a separate accessory; see Accessories information.
- 24 Must be ordered with fixture for factory pre-drilling.
- 25 Requires luminaire to be specified with PER, PER5 or PER7 option. See Control Option Table on page 4.
- 26 For retrofit use only. Only usable when pole's drill pattern is NOT Lithonia template #8.

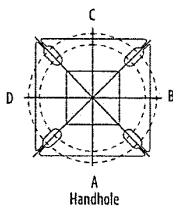
**Options**

EGS - External Glare Shield



**Drilling**

**HANDHOLE ORIENTATION**

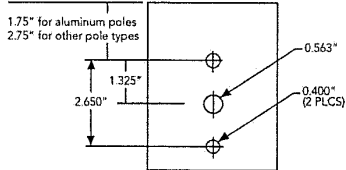


**Tenon Mounting Slipfitter**

Tenon O.D.	Mounting	Single Unit	2 @ 180	2 @ 90	3 @ 90	3 @ 120	4 @ 90
2-3/8"	RPA	AS3-5190	AS3-5280	AS3-5290	AS3-5390	AS3-5320	AS3-5490
2-7/8"	RPA	AST25-190	AST25-280	AST25-290	AST25-390	AST25-320	AST25-490
4"	RPA	AST35-190	AST35-280	AST35-290	AST35-390	AST35-320	AST35-490

Mounting Option	Drilling Template	Single	2 @ 180	2 @ 90	3 @ 90	3 @ 120	4 @ 90
Head Location		Side B	Side B & D	Side B & C	Side B, C & D	Round Pole Only	Side A, B, C & D
Drill Nomenclature	#8	DM19AS	DM28AS	DM29AS	DM39AS	DM32AS	DM49AS

Template #8



**DSX1 Area Luminaire - EPA**


\*Includes luminaire and integral mounting arm. Other tenons, arms, brackets or other accessories are not included in this EPA data.

Fixture Quantity & Mounting Configuration	Single DM19	2 @ 180 DM28	2 @ 90 DM29	3 @ 90 DM39	3 @ 120 DM32	4 @ 90 DM49
Mounting Type						
DSX1 LED	1.013	2.025	1.945	3.038	2.850	3.749

**QUOTING BOTH OPTIONS**

	Drilling Template	Minimum Acceptable Outside Pole Dimension					
SPA	#8	2-7/8"	2-7/8"	3.5"	3.5"	3"	3.5"
RPA	#8	2-7/8"	2-7/8"	3.5"	3.5"	3"	3.5"
SPUMBA	#5	2-7/8"	3"	4"	4"	3.5"	4"
RPUMBA	#5	2-7/8"	3.5"	5"	5"	3.5"	5"



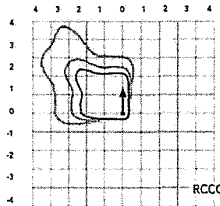
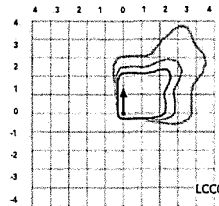
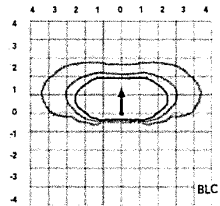
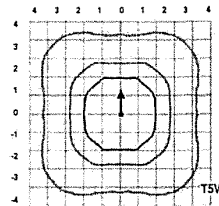
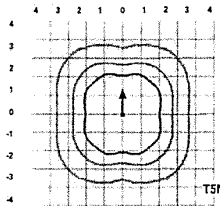
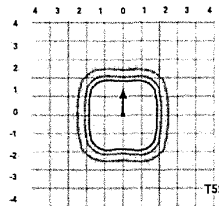
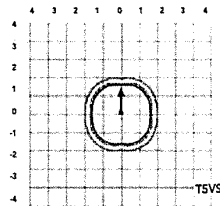
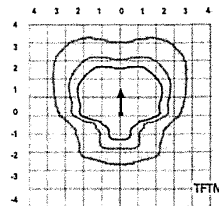
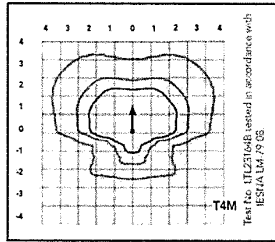
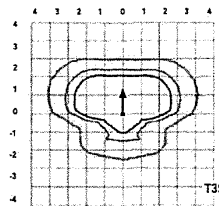
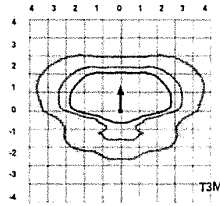
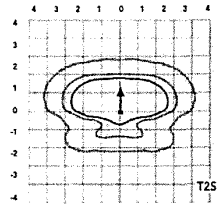
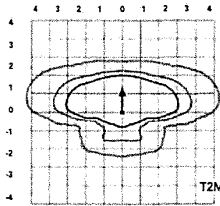
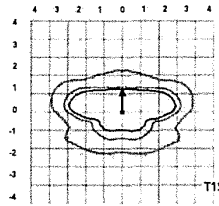
	<b>ALASKA ARCHITECTURAL LIGHTING</b>	<b>Project 22-28191-2</b> <b>SEARHC Juneau Vintage Park</b>  Submitted By <b>ALASKA ARCHITECTURAL LIGHTING</b>	<b>Catalog Number: DSX1 LED P5 40K T4M MVOLT SPA DDBXD</b>  Note:	Type <b>SB</b>
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**Photometric Diagrams**

To see complete photometric reports or download .ies files for this product, visit Lithonia Lighting's D-Series Area Size 1 homepage.

Isofootcandle plots for the DSX1 LED 60C 1000 40K. Distances are in units of mounting height (25').

**LEGEND**



 <b>ALASKA ARCHITECTURAL LIGHTING</b>	Project 22-28191-2 <b>SEARHC Juneau Vintage Park</b>	Catalog Number: DSX1 LED P5 40K T4M MVOLT SPA DDBXD	Type <b>SB</b>
	Submitted By ALASKA ARCHITECTURAL LIGHTING	Note:	

**Performance Data**

**Lumen Ambient Temperature (LAT) Multipliers**

Use these factors to determine relative lumen output for average ambient temperatures from 0-40°C (32-104°F).

Ambient		Lumen Multiplier
0°C	32°F	1.04
5°C	41°F	1.04
10°C	50°F	1.03
15°C	59°F	1.02
20°C	68°F	1.01
25°C	77°F	1.00
30°C	86°F	0.99
35°C	95°F	0.98
40°C	104°F	0.97

**Projected LED Lumen Maintenance**

Data references the extrapolated performance projections for the platforms noted in a 25°C ambient, based on 10,000 hours of LED testing (tested per IESNA LM-60-08 and projected per IESNA TM-21-11).

To calculate LLF, use the lumen maintenance factor that corresponds to the desired number of operating hours below. For other lumen maintenance values, contact factory.

Operating Hours	Lumen Maintenance Factor
0	1.00
25,000	0.96
50,000	0.92
100,000	0.85


Motion Sensor Default Settings						
Option	Dimmed State	High Level (when triggered)	Photocell Operation	Dwell Time	Ramp-up Time	Ramp-down Time
PIR or PIRH	3V (37%) Output	10V (100%) Output	Enabled @ 5FC	5 min	3 sec	5 min
*PIR1FC3V or PIRH1FC3V	3V (37%) Output	10V (100%) Output	Enabled @ 1FC	5 min	3 sec	5 min

\*for use when motion sensor is used as dusk to dawn control.

**Electrical Load**

Performance Package	LED Count	Drive Current	Wattage	Current (A)							
				120	208	240	277	347	480		
Forward Optics (Non-Rotated)	P1	30	530	54	0.45	0.26	0.23	0.19	0.10	0.12	
	P2	30	700	70	0.59	0.34	0.30	0.25	0.20	0.16	
	P3	30	1050	102	0.86	0.50	0.44	0.38	0.30	0.22	
	P4	30	1250	125	1.06	0.60	0.52	0.46	0.37	0.27	
	Rotated Optics (Requires L90 or R90)	P5	30	1400	138	1.16	0.67	0.58	0.51	0.40	0.29
		P6	40	1250	163	1.36	0.78	0.68	0.59	0.47	0.34
		P7	40	1400	183	1.53	0.88	0.76	0.66	0.53	0.38
		P8	60	1050	207	1.74	0.98	0.87	0.76	0.64	0.49
		P9	60	1250	241	2.01	1.16	1.01	0.89	0.70	0.51
Rotated Optics (Requires L90 or R90)	P10	60	530	106	0.90	0.52	0.47	0.43	0.33	0.27	
	P11	60	700	137	1.15	0.67	0.60	0.53	0.42	0.32	
	P12	60	1050	207	1.74	0.99	0.87	0.76	0.60	0.46	
	P13	60	1250	231	1.93	1.12	0.97	0.86	0.67	0.49	

Controls Options				
Nomenclature	Description	Functionality	Primary control device	Notes
FAO	Field adjustable output device installed inside the luminaire; wired to the driver dimming leads.	Allows the luminaire to be manually dimmed, effectively trimming the light output.	FAO device	Cannot be used with other controls options that need the 0-10V leads
DS	Drivers wired independently for 50/50 luminaire operation	The luminaire is wired to two separate circuits, allowing for 50/50 operation.	Independently wired drivers	Requires two separately switched circuits. Consider nLight AIR as a more cost effective alternative.
PER5 or PER7	Twist-lock photocell receptacle	Compatible with standard twist-lock photocells for dusk to dawn operation, or advanced control nodes that provide 0-10V dimming signals.	Twist-lock photocells such as DLL Elite or advanced control nodes such as ROAM.	Pins 4 & 5 to dimming leads on driver, Pins 6 & 7 are capped inside luminaire
PIR or PIRH	Motion sensors with integral photocell. PIR for 8-15' mounting; PIRH for 15-30' mounting	Luminaires dim when no occupancy is detected.	Acuity Controls SBGR	Also available with PIRH1FC3V when the sensor photocell is used for dusk-to-dawn operation.
NLTAIR2 PIRHN	nLight AIR enabled luminaire for motion sensing, photocell and wireless communication.	Motion and ambient light sensing with group response. Scheduled dimming with motion sensor over-ride when wirelessly connected to the nLight Eclipse.	nLight Air rSDGR	nLight AIR sensors can be programmed and commissioned from the ground using the CIAIRity Pro app.

 <b>ALASKA ARCHITECTURAL LIGHTING</b>	Project 22-28191-2 <b>SEARHC Juneau Vintage Park</b>	Catalog Number: DSX1 LED P5 40K T4M MVOLT SPA DDBXD	<b>SB</b>
	Submitted By <b>ALASKA ARCHITECTURAL LIGHTING</b>	Note:	

**Performance Data**

**Lumen Output**


Lumen values are from photometric tests performed in accordance with IESNA LM-79-08. Data is considered to be representative of the configurations shown, within the tolerances allowed by Lighting Facts Contact factory for performance data on any configurations not shown here.

Forward Optics																							
LED Count	Drive Current	Power Package	System Watts	Dist. Type	30K (6000 K, 70 CRI)					40K (4000 K, 70 CRI)					50K (5000 K, 70 CRI)								
					Lumens	B	U	G	LPW	Lumens	B	U	G	LPW	Lumens	B	U	G	LPW				
30	530	P1	54W	T1S	6,457	2	0	2	120	6,956	2	0	2	129	7,044	2	0	2	130				
				T2S	6,450	2	0	2	119	6,949	2	0	2	129	7,037	2	0	2	130				
				T2M	6,483	1	0	1	120	6,984	2	0	2	129	7,073	2	0	2	131				
				T3S	6,279	2	0	2	116	6,764	2	0	2	125	6,850	2	0	2	127				
				T3M	6,468	1	0	2	120	6,967	1	0	2	129	7,056	1	0	2	131				
				T4M	6,327	1	0	2	117	6,816	1	0	2	126	6,902	1	0	2	128				
				FTFM	6,464	1	0	2	120	6,963	1	0	2	129	7,051	1	0	2	131				
				TSVS	6,722	2	0	0	124	7,242	3	0	0	134	7,334	3	0	0	136				
				TSS	6,728	2	0	1	125	7,248	2	0	1	134	7,340	2	0	1	136				
				TSM	6,711	3	0	1	124	7,229	3	0	1	134	7,321	3	0	2	136				
				TSW	6,667	3	0	2	123	7,182	3	0	2	133	7,273	3	0	2	135				
				BLC	5,299	1	0	1	98	5,709	1	0	2	106	5,781	1	0	2	107				
				LCCO	3,943	1	0	2	73	4,248	1	0	2	79	4,302	1	0	2	80				
				RCCO	3,943	1	0	2	73	4,248	1	0	2	79	4,302	1	0	2	80				
				30	700	P2	70W	T1S	8,249	2	0	2	118	8,886	2	0	2	127	8,999	2	0	2	129
								T2S	8,240	2	0	2	118	8,877	2	0	2	127	8,989	2	0	2	128
								T2M	8,283	2	0	2	118	8,923	2	0	2	127	9,036	2	0	2	129
								T3S	8,021	2	0	2	115	8,641	2	0	2	123	8,751	2	0	2	125
								T3M	8,263	2	0	2	118	8,901	2	0	2	127	9,014	2	0	2	129
								T4M	8,083	2	0	2	115	8,708	2	0	2	124	8,818	2	0	2	126
FTFM	8,257	2	0					2	118	8,896	2	0	2	127	9,008	2	0	2	129				
TSVS	8,588	3	0					0	123	9,252	3	0	0	132	9,369	3	0	0	134				
TSS	8,595	3	0					1	123	9,259	3	0	1	132	9,376	3	0	1	134				
TSM	8,573	3	0					2	122	9,236	3	0	2	132	9,353	3	0	2	134				
TSW	8,517	3	0					2	122	9,175	4	0	2	131	9,291	4	0	2	133				
BLC	6,770	1	0					2	97	7,293	1	0	2	104	7,386	1	0	2	106				
LCCO	5,038	1	0					2	72	5,427	1	0	2	78	5,496	1	0	2	79				
RCCO	5,038	1	0					2	72	5,427	1	0	2	78	5,496	1	0	2	79				
30	1050	P3	102W					T1S	11,661	2	0	2	114	12,562	3	0	3	123	12,721	3	0	3	125
								T2S	11,648	2	0	2	114	12,548	3	0	3	123	12,707	3	0	3	125
								T2M	11,708	2	0	2	115	12,613	2	0	2	124	12,773	2	0	2	125
								T3S	11,339	2	0	2	111	12,215	3	0	3	120	12,370	3	0	3	121
								T3M	11,680	2	0	2	115	12,582	2	0	2	123	12,742	2	0	2	125
								T4M	11,426	2	0	3	112	12,309	2	0	3	121	12,465	2	0	3	122
				FTFM	11,673	2	0	2	114	12,575	2	0	3	123	12,734	2	0	3	125				
				TSVS	12,140	3	0	1	119	13,078	3	0	1	128	13,244	3	0	1	130				
				TSS	12,150	3	0	1	119	13,089	3	0	1	128	13,254	3	0	1	130				
				TSM	12,119	4	0	2	119	13,056	4	0	2	128	13,221	4	0	2	130				
				TSW	12,040	4	0	3	118	12,970	4	0	3	127	13,134	4	0	3	129				
				BLC	9,570	1	0	2	94	10,310	1	0	2	101	10,440	1	0	2	102				
				LCCO	7,121	1	0	3	70	7,671	1	0	3	75	7,768	1	0	3	76				
				RCCO	7,121	1	0	3	70	7,671	1	0	3	75	7,768	1	0	3	76				
				30	1250	P4	125W	T1S	13,435	3	0	3	107	14,473	3	0	3	116	14,657	3	0	3	117
								T2S	13,421	3	0	3	107	14,458	3	0	3	116	14,641	3	0	3	117
								T2M	13,490	2	0	2	108	14,532	3	0	3	116	14,716	3	0	3	118
								T3S	13,064	3	0	3	105	14,074	3	0	3	113	14,252	3	0	3	114
								T3M	13,457	2	0	2	108	14,497	2	0	2	116	14,681	2	0	2	117
								T4M	13,165	2	0	3	105	14,182	2	0	3	113	14,362	2	0	3	115
FTFM	13,449	2	0					3	108	14,488	2	0	3	116	14,672	2	0	3	117				
TSVS	13,987	4	0					1	112	15,068	4	0	1	121	15,259	4	0	1	122				
TSS	13,999	3	0					1	112	15,080	3	0	1	121	15,271	3	0	1	122				
TSM	13,963	4	0					2	112	15,042	4	0	2	120	15,233	4	0	2	122				
TSW	13,872	4	0					3	111	14,944	4	0	3	120	15,133	4	0	3	121				
BLC	11,027	1	0					2	88	11,879	1	0	2	95	12,029	1	0	2	96				
LCCO	8,205	1	0					3	66	8,839	1	0	3	71	8,951	1	0	3	72				
RCCO	8,205	1	0					3	66	8,839	1	0	3	71	8,951	1	0	3	72				
30	1400	P5	138W					T1S	14,679	3	0	3	106	15,814	3	0	3	115	16,014	3	0	3	116
								T2S	14,664	3	0	3	106	15,797	3	0	3	114	15,997	3	0	3	116
								T2M	14,739	3	0	3	107	15,878	3	0	3	115	16,079	3	0	3	117
								T3S	14,274	3	0	3	103	15,377	3	0	3	111	15,572	3	0	3	113
								T3M	14,704	2	0	3	107	15,840	3	0	3	115	16,040	3	0	3	116
								T4M	14,384	2	0	3	104	15,496	3	0	3	112	15,692	3	0	3	114
				FTFM	14,695	2	0	3	106	15,830	3	0	3	115	16,030	3	0	3	116				
				TSVS	15,283	4	0	1	111	16,464	4	0	1	119	16,672	4	0	1	121				
				TSS	15,295	3	0	1	111	16,477	4	0	1	119	16,686	4	0	1	121				
				TSM	15,257	4	0	2	111	16,435	4	0	2	119	16,644	4	0	2	121				
				TSW	15,157	4	0	3	110	16,328	4	0	3	118	16,534	4	0	3	120				
				BLC	12,048	1	0	2	87	12,979	1	0	2	94	13,143	1	0	2	95				
LCCO	8,965	1	0	3	65	9,657	1	0	3	70	9,780	1	0	3	71								
RCCO	8,965	1	0	3	65	9,657	1	0	3	70	9,780	1	0	3	71								



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DSX1-LED  
 Rev. 07/19/21  
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	ALASKA ARCHITECTURAL LIGHTING Project 22-28191-2 SEARHC Juneau Vintage Park	Catalog Number: DSX1 LED P5 40K T4M MVOLT SPA DDBXD	Type <b>SB</b>
	Submitted By ALASKA ARCHITECTURAL LIGHTING	Note:	

**Performance Data**

**Lumen Output**


Lumen values are from photometric tests performed in accordance with IESNA LM-79-08. Data is considered to be representative of the configurations shown, within the tolerances allowed by Lighting Facts. Contact factory for performance data on any configurations not shown here.

Forward Optics																							
LED Count	Drive Current	Power Package	System Watts	Dist. Type	30K (3000 K, 70 CRI)					40K (4000 K, 70 CRI)					50K (5000 K, 70 CRI)								
					Lumens	B	U	G	LPW	Lumens	B	U	G	LPW	Lumens	B	U	G	LPW				
40	1250	P6	163W	T1S	17,654	3	0	3	108	19,018	3	0	3	117	19,259	3	0	3	118				
				T2S	17,635	3	0	3	108	18,998	3	0	3	117	19,238	3	0	3	118				
				T2M	17,226	3	0	3	109	19,096	3	0	3	117	19,337	3	0	3	119				
				T3S	17,167	3	0	3	105	18,493	3	0	3	113	18,227	3	0	3	115				
				T3M	17,683	3	0	3	108	19,049	3	0	3	117	19,290	3	0	3	118				
				T4M	17,299	3	0	3	106	18,635	3	0	4	114	18,871	3	0	4	116				
				TFTM	17,672	3	0	3	108	19,038	3	0	4	117	19,279	3	0	4	118				
				TSVS	18,379	4	0	1	113	19,800	4	0	1	121	20,050	4	0	1	123				
				TSS	18,394	4	0	2	113	19,816	4	0	2	122	20,066	4	0	2	123				
				TSM	18,348	4	0	2	113	19,766	4	0	2	121	20,016	4	0	2	123				
				TSW	18,228	5	0	3	112	19,636	5	0	3	120	19,885	5	0	3	122				
				BLC	14,489	2	0	2	89	15,609	2	0	3	96	15,806	2	0	3	97				
				LCCO	10,781	1	0	3	66	11,614	1	0	3	71	11,761	2	0	3	72				
				RCCO	10,781	1	0	3	66	11,614	1	0	3	71	11,761	2	0	3	72				
				40	1400	P7	183W	T1S	19,227	3	0	3	105	20,712	3	0	3	113	20,975	3	0	3	115
								T2S	19,206	3	0	3	105	20,690	3	0	3	113	20,952	3	0	3	114
T2M	19,305	3	0					3	105	20,797	3	0	3	114	21,060	3	0	3	115				
T3S	18,696	3	0					3	102	20,141	3	0	3	110	20,396	3	0	4	111				
T3M	19,258	3	0					3	105	20,746	3	0	3	113	21,009	3	0	3	115				
T4M	18,840	3	0					4	103	20,296	3	0	4	111	20,553	3	0	4	112				
TFTM	19,246	3	0					4	105	20,734	3	0	4	113	20,996	3	0	4	115				
TSVS	20,017	4	0					1	109	21,564	4	0	1	118	21,837	4	0	1	119				
TSS	20,033	4	0					2	109	21,581	4	0	2	118	21,854	4	0	2	119				
TSM	19,983	4	0					2	109	21,527	5	0	3	118	21,799	5	0	3	119				
TSW	19,852	5	0					3	108	21,386	5	0	3	117	21,656	5	0	3	118				
BLC	15,780	2	0					3	86	16,999	2	0	3	93	17,214	2	0	3	94				
LCCO	11,742	2	0					3	64	12,649	2	0	3	69	12,809	2	0	3	70				
RCCO	11,742	2	0					3	64	12,649	2	0	3	69	12,809	2	0	3	70				
60	1050	P8	207W					T1S	22,490	3	0	3	109	24,228	3	0	3	117	24,535	3	0	3	119
								T2S	22,466	3	0	4	109	24,202	3	0	4	117	24,509	3	0	4	118
				T2M	22,582	3	0	3	109	24,327	3	0	3	118	24,635	3	0	3	119				
				T3S	21,870	3	0	4	106	23,560	3	0	4	114	23,858	3	0	4	115				
				T3M	22,527	3	0	4	109	24,268	3	0	4	117	24,575	3	0	4	119				
				T4M	22,038	3	0	4	106	23,741	3	0	4	115	24,041	3	0	4	116				
				TFTM	22,513	3	0	4	109	24,253	3	0	4	117	24,560	3	0	4	119				
				TSVS	23,415	5	0	1	113	25,224	5	0	1	122	25,543	5	0	1	123				
				TSS	23,434	4	0	2	113	25,244	4	0	2	122	25,564	4	0	2	123				
				TSM	23,374	5	0	3	113	25,181	5	0	3	122	25,499	5	0	3	123				
				TSW	23,221	5	0	4	112	25,016	5	0	4	121	25,332	5	0	4	122				
				BLC	18,458	2	0	3	89	19,885	2	0	3	96	20,136	2	0	3	97				
				LCCO	13,735	2	0	3	66	14,796	2	0	4	71	14,983	2	0	4	72				
				RCCO	13,735	2	0	3	66	14,796	2	0	4	71	14,983	2	0	4	72				
				60	1250	P9	241W	T1S	25,575	3	0	3	106	27,551	3	0	3	114	27,900	3	0	3	116
								T2S	25,548	3	0	4	106	27,522	3	0	4	114	27,871	3	0	4	116
T2M	25,680	3	0					3	107	27,664	3	0	3	115	28,014	3	0	3	116				
T3S	24,870	3	0					4	103	26,791	3	0	4	111	27,130	3	0	4	113				
T3M	25,617	3	0					4	106	27,597	3	0	4	115	27,946	3	0	4	116				
T4M	25,061	3	0					4	104	26,997	3	0	4	112	27,339	3	0	4	113				
TFTM	25,602	3	0					4	106	27,580	3	0	4	114	27,929	3	0	4	116				
TSVS	26,626	5	0					1	110	28,684	5	0	1	119	29,047	5	0	1	121				
TSS	26,648	4	0					2	111	28,707	5	0	2	119	29,070	5	0	2	121				
TSM	26,581	5	0					3	110	28,635	5	0	3	119	28,997	5	0	3	120				
TSW	26,406	5	0					4	110	28,447	5	0	4	118	28,807	5	0	4	120				
BLC	20,990	2	0					3	87	22,612	2	0	3	94	22,898	2	0	3	95				
LCCO	15,619	2	0					4	65	16,825	2	0	4	70	17,038	2	0	4	71				
RCCO	15,619	2	0					4	65	16,825	2	0	4	70	17,038	2	0	4	71				



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	ALASKA ARCHITECTURAL LIGHTING Project 22-28191-2 SEARHC Juneau Vintage Park	Catalog Number: DSX1 LED P5 40K T4M MVOLT SPA DDBXD	Type <b>SB</b>
	Submitted By ALASKA ARCHITECTURAL LIGHTING	Note:	

**Performance Data**

**Lumen Output**

Lumen values are from photometric tests performed in accordance with IESNA LM-79-08. Data is considered to be representative of the configurations shown, within the tolerances allowed by Lighting Facts. Contact factory for performance data on any configurations not shown here.


Rotated Optics																			
LED Count	Drive Current	Power Package	System Watts	Dist. Type	30K (3000 K, 70 CRI)					40K (4000 K, 70 CRI)					50K (5000 K, 70 CRI)				
					Lumens	B	U	G	LPW	Lumens	B	U	G	LPW	Lumens	B	U	G	LPW
60	530	P10	106W	T1S	13,042	3	0	3	123	14,050	3	0	3	133	14,228	3	0	3	134
				T2S	12,967	4	0	4	122	13,969	4	0	4	132	14,146	4	0	4	133
				T2M	13,201	3	0	3	125	14,221	3	0	3	134	14,401	3	0	3	136
				T3S	12,766	4	0	4	120	13,752	4	0	4	130	13,926	4	0	4	131
				T3M	13,193	4	0	4	124	14,213	4	0	4	134	14,393	4	0	4	136
				T4M	12,944	4	0	4	122	13,945	4	0	4	132	14,121	4	0	4	133
				FTFM	13,279	4	0	4	125	14,305	4	0	4	135	14,486	4	0	4	137
				TSVS	13,372	3	0	1	126	14,405	4	0	1	136	14,588	4	0	1	138
				TSS	13,260	3	0	1	125	14,284	3	0	1	135	14,465	3	0	1	136
				TSM	13,256	4	0	2	125	14,281	4	0	2	135	14,462	4	0	2	136
				TSW	13,137	4	0	3	124	14,153	4	0	3	134	14,332	4	0	3	135
				BLC	10,906	3	0	3	103	11,749	3	0	3	111	11,898	3	0	3	112
				LCCO	7,789	1	0	3	73	8,391	1	0	3	79	8,497	1	0	3	80
				RCCO	7,779	4	0	4	73	8,380	4	0	4	79	8,486	4	0	4	80
				60	700	P11	137W	T1S	16,556	3	0	3	121	17,835	3	0	3	130	18,061
T2S	16,461	4	0					4	120	17,733	4	0	4	129	17,957	4	0	4	131
T2M	16,758	4	0					4	122	18,053	4	0	4	132	18,281	4	0	4	133
T3S	16,205	4	0					4	118	17,457	4	0	4	127	17,678	4	0	4	129
T3M	16,748	4	0					4	122	18,042	4	0	4	132	18,271	4	0	4	133
T4M	16,432	4	0					4	120	17,702	4	0	4	129	17,926	4	0	4	131
FTFM	16,857	4	0					4	123	18,159	4	0	4	133	18,389	4	0	4	134
TSVS	16,975	4	0					1	124	18,287	4	0	1	133	18,518	4	0	1	135
TSS	16,832	4	0					1	123	18,133	4	0	2	132	18,362	4	0	2	134
TSM	16,828	4	0					2	123	18,128	4	0	2	132	18,358	4	0	2	134
TSW	16,677	4	0					3	122	17,966	5	0	3	131	18,193	5	0	3	133
BLC	13,845	3	0					3	101	14,915	3	0	3	109	15,103	3	0	3	110
LCCO	9,888	1	0					3	72	10,652	2	0	3	78	10,787	2	0	3	79
RCCO	9,875	4	0					4	72	10,638	4	0	4	78	10,773	4	0	4	79
60	1050	P12	207W					T1S	22,896	4	0	4	111	24,773	4	0	4	120	25,087
				T2S	22,864	4	0	4	110	24,631	5	0	5	119	24,943	5	0	5	120
				T2M	23,277	4	0	4	112	25,075	4	0	4	121	25,393	4	0	4	123
				T3S	22,509	4	0	4	109	24,248	5	0	5	117	24,555	5	0	5	119
				T3M	23,263	4	0	4	112	25,061	4	0	4	121	25,378	4	0	4	123
				T4M	22,824	5	0	5	110	24,588	5	0	5	119	24,899	5	0	5	120
				FTFM	23,414	5	0	5	113	25,223	5	0	5	122	25,543	5	0	5	123
				TSVS	23,579	5	0	1	114	25,401	5	0	1	123	25,722	5	0	1	124
				TSS	23,380	4	0	2	113	25,187	4	0	2	122	25,506	4	0	2	123
				TSM	23,374	5	0	3	113	25,181	5	0	3	122	25,499	5	0	3	123
				TSW	23,165	5	0	4	112	24,955	5	0	4	121	25,271	5	0	4	122
				BLC	19,231	4	0	4	93	20,717	4	0	4	100	20,979	4	0	4	101
				LCCO	13,734	2	0	3	66	14,796	2	0	4	71	14,983	2	0	4	72
				RCCO	13,716	4	0	4	66	14,776	4	0	4	71	14,963	4	0	4	72
				60	1250	P13	231W	T1S	25,400	4	0	4	110	27,363	4	0	4	118	27,709
T2S	25,254	5	0					5	109	27,205	5	0	5	118	27,550	5	0	5	119
T2M	25,710	4	0					4	111	27,696	4	0	4	120	28,047	4	0	4	121
T3S	24,862	5	0					5	108	26,783	5	0	5	116	27,122	5	0	5	117
T3M	25,695	5	0					5	111	27,680	5	0	5	120	28,031	5	0	5	121
T4M	25,210	5	0					5	109	27,158	5	0	5	118	27,502	5	0	5	119
FTFM	25,861	5	0					5	112	27,860	5	0	5	121	28,212	5	0	5	122
TSVS	26,043	5	0					1	113	28,056	5	0	1	121	28,411	5	0	1	123
TSS	25,824	4	0					2	112	27,819	5	0	2	120	28,172	5	0	2	122
TSM	25,818	5	0					3	112	27,813	5	0	3	120	28,165	5	0	3	122
TSW	25,586	5	0					4	111	27,563	5	0	4	119	27,912	5	0	4	121
BLC	21,241	4	0					4	92	22,882	4	0	4	99	23,172	4	0	4	100
LCCO	15,170	2	0					4	66	16,342	2	0	4	71	16,549	2	0	4	72
RCCO	15,150	5	0					5	66	16,321	5	0	5	71	16,527	5	0	5	72



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	<b>ALASKA ARCHITECTURAL LIGHTING</b>	<b>Project 22-28191-2</b> <b>SEARHC Juneau Vintage Park</b>  Submitted By <b>ALASKA ARCHITECTURAL LIGHTING</b>	Catalog Number: <b>DSX1 LED P5 40K T4M MVOLT SPA</b> <b>DDBXD</b>  Note:	Type <b>SB</b>
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**FEATURES & SPECIFICATIONS**

**INTENDED USE**

The sleek design of the D-Series Size 1 reflects the embedded high performance LED technology. It is ideal for many commercial and municipal applications, such as parking lots, plazas, campuses, and streetscapes.

**CONSTRUCTION**

Single-piece die-cast aluminum housing has integral heat sink fins to optimize thermal management through conductive and convective cooling. Modular design allows for ease of maintenance and future light engine upgrades. The LED drivers are mounted in direct contact with the casting to promote low operating temperature and long life. Housing is completely sealed against moisture and environmental contaminants (IP65). Low EPA (1.01 ft<sup>2</sup>) for optimized pole wind loading.

**FINISH**

Exterior parts are protected by a zinc-infused Super Durable TGIC thermostat powder coat finish that provides superior resistance to corrosion and weathering. A tightly controlled multi-stage process ensures a minimum 3 mils thickness for a finish that can withstand extreme climate changes without cracking or peeling. Available in both textured and non-textured finishes.

**OPTICS**

Precision-molded proprietary acrylic lenses are engineered for superior area lighting distribution, uniformity, and pole spacing. Light engines are available in standard 3000 K, 4000 K and 5000 K (70 CRI) configurations. The D-Series Size 1 has zero uplight and qualifies as a Nighttime Friendly™ product, meaning it is consistent with the LEED® and Green Globes™ criteria for eliminating wasteful uplight.

**ELECTRICAL**

Light engine configurations consist of high-efficacy LEDs mounted to metal-core circuit boards to maximize heat dissipation and promote long life (up to L85/100,000 hours at 25°C). Class 1 electronic drivers are designed to have a power factor >90%, THD <20%, and an expected life of 100,000 hours with <1% failure rate. Easily serviceable 10kV surge protection device meets a minimum Category C Low operation (per ANSI/IEEE C62.41.2).

**STANDARD CONTROLS**

The DSX1 LED area luminaire has a number of control options. DSX Size 1, comes standard with 0-10V dimming drivers. Dusk to dawn controls can be utilized via optional NEMA twist-lock photocell receptacles. Integrated motion sensors with on-board photocells feature field-adjustable programming and are suitable for mounting heights up to 30 feet.

**nLIGHT AIR CONTROLS**

The DSX1 LED area luminaire is also available with nLight® AIR for the ultimate in wireless control. This powerful controls platform provides out-of-the-box basic motion sensing and photocontrol functionality and is suitable for mounting heights up to 40 feet. Once commissioned using a smartphone and the easy-to-use CLAIRITY app, nLight AIR equipped luminaires can be grouped, resulting in motion sensor and photocell group response without the need for additional equipment. Scheduled dimming with motion sensor over-ride can be achieved when used with the nLight Eclipse. Additional information about nLight Air can be found here.

**INSTALLATION**

Included mounting block and integral arm facilitate quick and easy installation. Stainless steel bolts fasten the mounting block securely to poles and walls, enabling the D-Series Size 1 to withstand up to a 3.0 G vibration load rating per ANSI C136.31. The D-Series Size 1 utilizes the AERIS™ series pole drilling pattern (template #8). NEMA photocontrol receptacle are also available.

**LISTINGS**

UL listed to meet U.S. and Canadian standards. UL Listed for wet locations. Light engines are IP66 rated; luminaire is IP65 rated. Rated for -40°C minimum ambient. U.S. Patent No. D672,492 S. International patent pending.

DesignLights Consortium® (DLC) Premium qualified product and DLC qualified product. Not all versions of this product may be DLC Premium qualified or DLC qualified. Please check the DLC Qualified Products List at [www.designlights.org/](http://www.designlights.org/) QPL to confirm which versions are qualified.

International Dark-Sky Association (IDA) Fixture Seal of Approval (FSA) is available for all products on this page utilizing 3000K color temperature only.

**BUY AMERICAN**

Product with the BAA option is assembled in the USA and meets the Buy America(n) government procurement requirements under FAR, DFARS and DOT. Please refer to [www.acuitybrands.com/buy-american](http://www.acuitybrands.com/buy-american) for additional information.

**WARRANTY**

5-year limited warranty. Complete warranty terms located at: [www.acuitybrands.com/support/customer-support/terms-and-conditions](http://www.acuitybrands.com/support/customer-support/terms-and-conditions)

**Note:** Actual performance may differ as a result of end-user environment and application.

All values are design or typical values, measured under laboratory conditions at 25 °C. Specifications subject to change without notice.

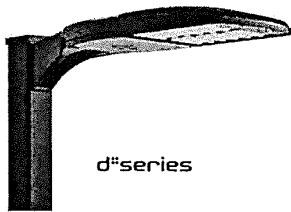


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COMMERCIAL OUTDOOR

	Project 22-28191-2 SEARHC Juneau Vintage Park  Submitted By ALASKA ARCHITECTURAL LIGHTING	Catalog Number: DSX1 LED P5 40K T3M MVOLT SPA DDBXD  Note:	Type <b>SC</b>
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## D-Series Size 1 LED Area Luminaire



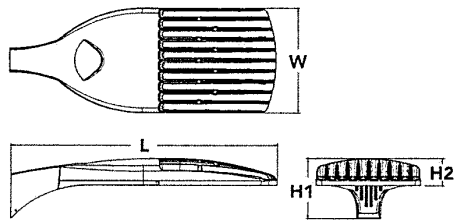
Catalog Number \_\_\_\_\_

Notes \_\_\_\_\_

Type \_\_\_\_\_

### Specifications

- EPA:** 1.01 ft<sup>2</sup> (0.09 m<sup>2</sup>)
- Length:** 33" (83.8 cm)
- Width:** 13" (33.0 cm)
- Height H1:** 7-1/2" (19.0 cm)
- Height H2:** 3-1/2"
- Weight (max):** 27 lbs (12.2 kg)



### Introduction

The modern styling of the D-Series is striking yet unobtrusive - making a bold, progressive statement even as it blends seamlessly with its environment. The D-Series distills the benefits of the latest in LED technology into a high performance, high efficacy, long-life luminaire.

The outstanding photometric performance results in sites with excellent uniformity, greater pole spacing and lower power density. It is ideal for replacing up to 750W metal halide in pedestrian and area lighting applications with typical energy savings of 65% and expected service life of over 100,000 hours.

### Ordering Information EXAMPLE: DSX1 LED P7 40K T3M MVOLT SPA NLTAIR2 PIRHN DDBXD

Series		LEDs	Color temperature	Distribution	Voltage	Mounting	
DSX1 LED		<b>Forward optics</b> P1 P4 <sup>1</sup> P7 <sup>1</sup> P2 P5 <sup>1</sup> P8 P3 P6 <sup>1</sup> P9 <sup>1</sup>  <b>Rotated optics</b> P10 <sup>2</sup> P12 <sup>2</sup> P11 <sup>2</sup> P13 <sup>1,2</sup>	30K 3000 K 40K 4000 K 50K 5000 K	T1S Type I short (Automotive) T2S Type II short T2M Type II medium T3S Type III short T3M Type III medium T4M Type IV medium TFTM Forward throw medium	T5VS Type V very short <sup>4</sup> T5S Type V short <sup>4</sup> T5M Type V medium <sup>4</sup> T5W Type V wide <sup>4</sup> BLC Backlight control <sup>4</sup> LCCO Left corner cutoff <sup>4</sup> RCCO Right corner cutoff <sup>4</sup>	MVOLT <sup>3</sup> XVOLT (277V-480V) <sup>6,7,8</sup> 120 <sup>9</sup> 208 <sup>9</sup> 240 <sup>9</sup> 277 <sup>9</sup> 347 <sup>9</sup> 480 <sup>9</sup>	<b>Shipped included</b> SPA Square pole mounting RPA Round pole mounting <sup>6</sup> WBA Wall bracket <sup>1</sup> SPUMBA Square pole universal mounting adaptor <sup>11</sup> RPUMBA Round pole universal mounting adaptor <sup>9</sup>  <b>Shipped separately</b> KMA8 DDBXD U Mast arm mounting bracket adaptor (specify finish) <sup>12</sup>


Control options	Other options	Finish (see table)
<b>Shipped installed</b> NLTAIR2 nLight AIR generation 2 enabled <sup>11</sup> PIRHN Network, high/low motion/ambient sensor <sup>14</sup> PER NEMA twist-lock receptacle only (controls ordered separate) <sup>15</sup> PER5 Five-pin receptacle only (controls ordered separate) <sup>15,16</sup> PER7 Seven-pin receptacle only (controls ordered separate) <sup>15,16</sup> DMG 0-10v dimming wires pulled outside fixture (for use with an external control, ordered separately) <sup>17</sup> DS Dual switching <sup>18,19,20</sup>	<b>Shipped installed</b> HS House-side shield <sup>21</sup> SF Single fuse (120, 277, 347V) <sup>9</sup> DF Double fuse (208, 240, 480V) <sup>9</sup> L90 Left rotated optics <sup>4</sup> R90 Right rotated optics <sup>4</sup> HA 50°C ambient operations <sup>1</sup> BAA Buy America(n) Act Compliant  <b>Shipped separately</b> BS Bird spikes <sup>24</sup> EGS External glare shield	DDBXD Dark bronze DBLXD Black DNAXD Natural aluminum DWHXD White DDBTXD Textured dark bronze DBLBXD Textured black DNATXD Textured natural aluminum DWHGXD Textured white

PLEASE VERIFY FINISH



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 <p>ALASKA ARCHITECTURAL LIGHTING</p>	<p>Project 22-28191-2  <b>SEARHC Juneau Vintage Park</b></p> <p>Submitted By  <b>ALASKA ARCHITECTURAL LIGHTING</b></p>	<p>Catalog Number: <b>DSX1 LED P5 40K T3M MVOLT SPA DDBXD</b></p> <p>Note:</p>	<p>Type  <b>SC</b></p>
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**Ordering Information**

**Accessories**

Ordered and shipped separately.

DLL127F 1.5 JU	Photocell - SSL twist-lock (120-277V) <sup>21</sup>
DLL347F 1.5 CUL JU	Photocell - SSL twist-lock (347V) <sup>21</sup>
DLL480F 1.5 CUL JU	Photocell - SSL twist-lock (480V) <sup>21</sup>
DSHORT SBK U	Shirting cap <sup>22</sup>
DSX1HS 30C U	House-side shield for P1, P2, P3, P4 and P5 <sup>23</sup>
DSX1HS 40C U	House-side shield for P6 and P7 <sup>23</sup>
DSX1HS 60C U	House-side shield for P8, P9, P10, P11 and P12 <sup>23</sup>
PUMBA DDBXD U*	Square and round pole universal mounting bracket (specify finish) <sup>24</sup>
KXAR DDBXD U	Must arm mounting bracket adaptor (specify finish) <sup>24</sup>
DSX1EGS (FINISH) U	External glare shield

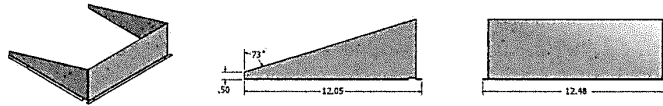
For more control options, visit [ULC](#) and [PDAV](#) online.

**NOTES**

- 1 HA not available with P4, P5, P6, P7, P9 and P13.
- 2 P11, P11T, P12 & P13 are rotated 90 degrees (LVL), RMI only available together.
- 3 Any Type 5 distribution with photocell, is not available with WBA.
- 4 Not available with T5.
- 5 MVOLT driver operates on any line voltage from 120-277V (50/60 Hz).
- 6 XVOLT only suitable for use with P3, P5, P6, P7, P9 and P13.
- 7 XVOLT works with any voltage between 277V and 480V.
- 8 XVOLT not available with fusing (SF or DF) and not available with PIR, PIRH, PIR1FC3V, PIRH1FC3V.
- 9 Single fuse (SF) requires 120V, 277V or 347V. Double fuse (DF) requires 208V, 240V or 480V. XVOLT not available with fusing (SF or DF).
- 10 Suitable for mounting to round poles between 3.5" and 12" diameter.
- 11 Universal mounting brackets intended for retrofit on existing, pre-drilled poles only. 1.5 G vibration load rating per ANCI C136.31. Only usable when pole's drill pattern is NOT Lithonia template #8.
- 12 Must order fixture with SPA option. Must be ordered as a separate accessory; see Accessories information. For use with 2-3/8" diameter mast arm (not included).
- 13 Must be ordered with PIRHNN. Sensor cover available only in dark bronze, black, white and natural aluminum colors.
- 14 Must be ordered with NLTARZ. For more information on nLight AIR 2 visit [this link](#).
- 15 Photocell ordered and shipped as a separate line item from Acuity Brands Controls. See accessories. Shirting cap included.
- 16 If ROAM\* node required, it must be ordered and shipped as a separate line item from Acuity Brands Controls. Node with integral dimming.
- 17 DMG not available with PIRHNN, PER5, PER7, PIR, PIRH, PIR1FC3V or PIRH1FC3V, FAO.
- 18 Provides 50/50 fixture operation via (2) independent drivers. Not available with PER, PER5, PER7, PIR or PIRH. Not available P1, P2, P3, P4 or P5.
- 19 Requires (2) separately switched circuits with isolated neutral.
- 20 Reference Controls Option Default settings table on page 4.
- 21 Reference Motion Sensor table on page 4 to see functionality.
- 22 Not available with other dimming controls options.
- 23 Not available with BLC, LCCO and RCCO distribution. Also available as a separate accessory; see Accessories information.
- 24 Must be ordered with fixture for factory pre-drilling.
- 25 Requires luminaire to be specified with PER, PER5 or PER7 option. See Control Option Table on page 4.
- 26 For retrofit use only. Only usable when pole's drill pattern is NOT Lithonia template #8.

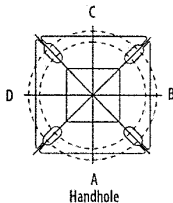
**Options**

**EGS - External Glare Shield**



**Drilling**

**HANDHOLE ORIENTATION**

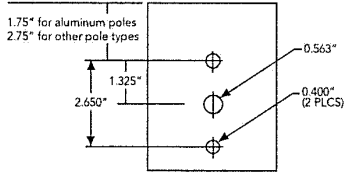


**Tenon Mounting Slipfitter**

Tenon O.D.	Mounting	Single Unit	2 @ 180	2 @ 90	3 @ 90	3 @ 120	4 @ 90
2-3/8"	RPA	AS3-5-190	AS3-5-280	AS3-5-290	AS3-5-390	AS3-5-320	AS3-5-490
2-7/8"	RPA	AST25-190	AST25-280	AST25-290	AST25-390	AST25-320	AST25-490
4"	RPA	AST35-190	AST35-280	AST35-290	AST35-390	AST35-320	AST35-490

Mounting Option	Drilling Template	Single	2 @ 180	2 @ 90	3 @ 90	3 @ 120	4 @ 90
Head Location		Side B	Side B & D	Side B & C	Side B, C & D	Round Pole Only	Side A, B, C & D
Drill Nomenclature	#8	DM19AS	DM28AS	DM29AS	DM39AS	DM32AS	DM49AS

**Template #8**



**DSX1 Area Luminaire - EPA**


\*Includes luminaire and integral mounting arm. Other tenons, arms, brackets or other accessories are not included in this EPA data.

Fixture Quantity & Mounting Configuration	Single DM19	2 @ 180 DM28	2 @ 90 DM29	3 @ 90 DM39	3 @ 120 DM32	4 @ 90 DM49
Mounting Type						
DSX1 LED	1.013	2.025	1.945	3.038	2.850	3.749

**QUOTING BOTH OPTIONS**

	Drilling Template	Minimum Acceptable Outside Pole Dimension					
SPA	#8	2-7/8"	2-7/8"	3.5"	3.5"	3"	3.5"
RPA	#8	2-7/8"	2-7/8"	3.5"	3.5"	3"	3.5"
SPUMBA	#5	2-7/8"	3"	4"	4"	3.5"	4"
RPUMBA	#5	2-7/8"	3.5"	5"	5"	3.5"	5"



 <b>ALASKA ARCHITECTURAL LIGHTING</b>	Project 22-28191-2 <b>SEARHC Juneau Vintage Park</b>	Catalog Number: DSX1 LED P5 40K T3M MVOLT SPA DDBXD	Type <b>SC</b>
	Submitted By ALASKA ARCHITECTURAL LIGHTING	Note:	

**Performance Data**

**Lumen Ambient Temperature (LAT) Multipliers**

Use these factors to determine relative lumen output for average ambient temperatures from 0-40°C (32-104°F).

Ambient		Lumen Multiplier
0°C	32°F	1.04
5°C	41°F	1.04
10°C	50°F	1.03
15°C	59°F	1.02
20°C	68°F	1.01
25°C	77°F	1.00
30°C	86°F	0.99
35°C	95°F	0.98
40°C	104°F	0.97

**Electrical Load**

	Performance Package	LED Count	Drive Current	Wattage	Current (A)					
					120	208	240	277	347	480
Forward Optics (Non-Rotated)	P1	30	530	54	0.45	0.26	0.23	0.19	0.10	0.12
	P2	30	700	70	0.59	0.34	0.30	0.25	0.20	0.16
	P3	30	1050	102	0.86	0.50	0.44	0.38	0.30	0.22
	P4	30	1250	125	1.06	0.60	0.52	0.46	0.37	0.27
	P5	30	1400	138	1.16	0.67	0.58	0.51	0.40	0.29
	P6	40	1250	163	1.36	0.78	0.68	0.59	0.47	0.34
	P7	40	1400	183	1.53	0.88	0.76	0.66	0.53	0.38
	P8	60	1050	207	1.74	0.98	0.87	0.76	0.64	0.49
	P9	60	1250	241	2.01	1.16	1.01	0.89	0.70	0.51
Rotated Optics (Requires L90 or R90)	P10	60	530	106	0.90	0.52	0.47	0.43	0.33	0.27
	P11	60	700	137	1.15	0.67	0.60	0.53	0.42	0.32
	P12	60	1050	207	1.74	0.99	0.87	0.76	0.60	0.46
	P13	60	1250	231	1.93	1.12	0.97	0.86	0.67	0.49

**Projected LED Lumen Maintenance**

Data references the extrapolated performance projections for the platforms noted in a 25°C ambient, based on 10,000 hours of LED testing (tested per IESNA LM-80-08 and projected per IESNA TM-21-11).

To calculate LLF, use the lumen maintenance factor that corresponds to the desired number of operating hours below. For other lumen maintenance values, contact factory.

Operating Hours	Lumen Maintenance Factor
0	1.00
25,000	0.96
50,000	0.92
100,000	0.85

**Motion Sensor Default Settings**

Option	Dimmed State	High Level (when triggered)	Photocell Operation	Dwell Time	Ramp-up Time	Ramp-down Time
PIR or PIRH	3V (37%) Output	10V (100%) Output	Enabled @ 5FC	5 min	3 sec	5 min
*PIRHFC3V or PIRHFC3V	3V (37%) Output	10V (100%) Output	Enabled @ 1FC	5 min	3 sec	5 min

\*for use when motion sensor is used as dusk to dawn control.

**Controls Options**

Nomenclature	Description	Functionality	Primary control device	Notes
FAO	Field adjustable output device installed inside the luminaire; wired to the driver dimming leads.	Allows the luminaire to be manually dimmed, effectively trimming the light output.	FAO device	Cannot be used with other controls options that need the 0-10V leads
DS	Drivers wired independently for 50/50 luminaire operation	The luminaire is wired to two separate circuits, allowing for 50/50 operation.	Independently wired drivers	Requires two separately switched circuits. Consider nLight AIR as a more cost effective alternative.
PERS or PER7	Twist-lock photocell receptacle	Compatible with standard twist-lock photocells for dusk to dawn operation, or advanced control nodes that provide 0-10V dimming signals.	Twist-lock photocells such as DLL Elite or advanced control nodes such as ROAM.	Pins 4 & 5 to dimming leads on driver, Pins 6 & 7 are capped inside luminaire
PIR or PIRH	Motion sensors with integral photocell. PIR for 8-15' mounting; PIRH for 15-30' mounting	Luminaires dim when no occupancy is detected.	Acuity Controls SBGR	Also available with PIRHFC3V when the sensor photocell is used for dusk-to-dawn operation.
NLTAIR2 PIRHN	nLight AIR enabled luminaire for motion sensing, photocell and wireless communication.	Motion and ambient light sensing with group response. Scheduled dimming with motion sensor over-ride when wirelessly connected to the nLight Eclipse.	nLight Air rSDGR	nLight AIR sensors can be programmed and commissioned from the ground using the CLARITY Pro app.





	Project 22-28191-2 SEARHC Juneau Vintage Park	Catalog Number: DSX1 LED P5 40K T3M MVOLT SPA DDBXD	Type <b>SC</b>
	Submitted By ALASKA ARCHITECTURAL LIGHTING	Note:	

**Performance Data**

**Lumen Output**

Lumen values are from photometric tests performed in accordance with IESNA LM-79-08. Data is considered to be representative of the configurations shown, within the tolerances allowed by Lighting Facts Contact factory for performance data on any configurations not shown here.

Forward Optics																							
LED Count	Drive Current	Power Package	System Watts	Dist. Type	30K (3000 K/70 CR)					40K (4000 K/70 CR)					50K (5000 K/70 CR)								
					Lumens	R	U	G	LPW	Lumens	R	U	G	LPW	Lumens	R	U	G	LPW				
30	530	P1	54W	T1S	6,457	2	0	2	120	6,956	2	0	2	129	7,044	2	0	2	130				
				T2S	6,450	2	0	2	119	6,949	2	0	2	129	7,037	2	0	2	130				
				T2M	6,483	1	0	1	120	6,984	2	0	2	129	7,073	2	0	2	131				
				T3S	6,279	2	0	2	116	6,764	2	0	2	125	6,850	2	0	2	127				
				T3M	6,468	1	0	2	120	6,967	1	0	2	129	7,056	1	0	2	131				
				T4M	6,327	1	0	2	117	6,816	1	0	2	126	6,902	1	0	2	128				
				TF1M	6,464	1	0	2	120	6,963	1	0	2	129	7,051	1	0	2	131				
				TSVS	6,722	2	0	0	124	7,242	3	0	0	134	7,334	3	0	0	136				
				TSS	6,728	2	0	1	125	7,248	2	0	1	134	7,340	2	0	1	136				
				TSM	6,711	3	0	1	124	7,229	3	0	1	134	7,321	3	0	2	136				
				TSW	6,667	3	0	2	123	7,182	3	0	2	133	7,273	3	0	2	135				
				BLC	5,299	1	0	1	98	5,709	1	0	2	106	5,781	1	0	2	107				
				LCCO	3,943	1	0	2	73	4,248	1	0	2	79	4,302	1	0	2	80				
				RCCO	3,943	1	0	2	73	4,248	1	0	2	79	4,302	1	0	2	80				
				30	700	P2	70W	T1S	8,249	2	0	2	118	8,886	2	0	2	127	8,999	2	0	2	129
								T2S	8,240	2	0	2	118	8,877	2	0	2	127	8,989	2	0	2	128
								T2M	8,283	2	0	2	118	8,923	2	0	2	127	9,036	2	0	2	129
								T3S	8,021	2	0	2	115	8,641	2	0	2	123	8,751	2	0	2	125
T3M	8,263	2	0					2	118	8,901	2	0	2	127	9,014	2	0	2	129				
T4M	8,083	2	0					2	115	8,708	2	0	2	124	8,818	2	0	2	126				
TF1M	8,257	2	0					2	118	8,896	2	0	2	127	9,008	2	0	2	129				
TSVS	8,588	3	0					0	123	9,252	3	0	0	132	9,369	3	0	0	134				
TSS	8,595	3	0					1	123	9,259	3	0	1	132	9,376	3	0	1	134				
TSM	8,573	3	0					2	122	9,236	3	0	2	132	9,353	3	0	2	134				
TSW	8,517	3	0					2	122	9,175	4	0	2	131	9,291	4	0	2	133				
BLC	6,770	1	0					2	97	7,293	1	0	2	104	7,386	1	0	2	106				
LCCO	5,038	1	0					2	72	5,427	1	0	2	78	5,496	1	0	2	79				
RCCO	5,038	1	0					2	72	5,427	1	0	2	78	5,496	1	0	2	79				
30	1050	P3	102W					T1S	11,661	2	0	2	114	12,562	3	0	3	123	12,721	3	0	3	125
								T2S	11,648	2	0	2	114	12,548	3	0	3	123	12,707	3	0	3	125
								T2M	11,708	2	0	2	115	12,613	2	0	2	124	12,773	2	0	2	125
								T3S	11,339	2	0	2	111	12,215	3	0	3	120	12,370	3	0	3	121
				T3M	11,680	2	0	2	115	12,582	2	0	2	123	12,742	2	0	2	125				
				T4M	11,426	2	0	3	112	12,309	2	0	3	121	12,465	2	0	3	122				
				TF1M	11,673	2	0	2	114	12,575	2	0	3	123	12,734	2	0	3	125				
				TSVS	12,140	3	0	1	119	13,078	3	0	1	128	13,244	3	0	1	130				
				TSS	12,150	3	0	1	119	13,089	3	0	1	128	13,254	3	0	1	130				
				TSM	12,119	4	0	2	119	13,056	4	0	2	128	13,221	4	0	2	130				
				TSW	12,040	4	0	3	118	12,970	4	0	3	127	13,134	4	0	3	129				
				BLC	9,570	1	0	2	94	10,310	1	0	2	101	10,440	1	0	2	102				
				LCCO	7,121	1	0	3	70	7,671	1	0	3	75	7,768	1	0	3	76				
				RCCO	7,121	1	0	3	70	7,671	1	0	3	75	7,768	1	0	3	76				
				30	1250	P4	125W	T1S	13,435	3	0	3	107	14,473	3	0	3	116	14,657	3	0	3	117
								T2S	13,421	3	0	3	107	14,458	3	0	3	116	14,641	3	0	3	117
								T2M	13,490	2	0	2	108	14,532	3	0	3	116	14,716	3	0	3	118
								T3S	13,064	3	0	3	105	14,074	3	0	3	113	14,252	3	0	3	114
T3M	13,457	2	0					2	108	14,497	2	0	2	116	14,681	2	0	2	117				
T4M	13,165	2	0					3	105	14,182	2	0	3	113	14,362	2	0	3	115				
TF1M	13,449	2	0					3	108	14,488	2	0	3	116	14,672	2	0	3	117				
TSVS	13,987	4	0					1	112	15,068	4	0	1	121	15,259	4	0	1	122				
TSS	13,999	3	0					1	112	15,080	3	0	1	121	15,271	3	0	1	122				
TSM	13,963	4	0					2	112	15,042	4	0	2	120	15,233	4	0	2	122				
TSW	13,872	4	0					3	111	14,944	4	0	3	120	15,133	4	0	3	121				
BLC	11,027	1	0					2	88	11,879	1	0	2	95	12,029	1	0	2	96				
LCCO	8,205	1	0					3	66	8,839	1	0	3	71	8,951	1	0	3	72				
RCCO	8,205	1	0					3	66	8,839	1	0	3	71	8,951	1	0	3	72				
30	1400	P5	138W					T1S	14,679	3	0	3	106	15,814	3	0	3	115	16,014	3	0	3	116
								T2S	14,664	3	0	3	106	15,797	3	0	3	114	15,997	3	0	3	116
								T2M	14,739	3	0	3	107	15,878	3	0	3	115	16,079	3	0	3	117
								T3S	14,274	3	0	3	103	15,377	3	0	3	111	15,572	3	0	3	113
				T3M	14,704	2	0	3	107	15,840	3	0	3	115	16,040	3	0	3	116				
				T4M	14,384	2	0	3	104	15,496	3	0	3	112	15,692	3	0	3	114				
				TF1M	14,695	2	0	3	106	15,830	3	0	3	115	16,030	3	0	3	116				
				TSVS	15,283	4	0	1	111	16,464	4	0	1	119	16,672	4	0	1	121				
				TSS	15,295	3	0	1	111	16,477	4	0	1	119	16,686	4	0	1	121				
				TSM	15,257	4	0	2	111	16,435	4	0	2	119	16,644	4	0	2	121				
				TSW	15,157	4	0	3	110	16,328	4	0	3	118	16,534	4	0	3	120				
				BLC	12,048	1	0	2	87	12,979	1	0	2	94	13,143	1	0	2	95				
				LCCO	8,965	1	0	3	65	9,657	1	0	3	70	9,780	1	0	3	71				
				RCCO	8,965	1	0	3	65	9,657	1	0	3	70	9,780	1	0	3	71				



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DSX1-LED  
 Rev 07/19/21  
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	<b>Project 22-28191-2</b> <b>SEARHC Juneau Vintage Park</b>  Submitted By <b>ALASKA ARCHITECTURAL LIGHTING</b>	Catalog Number: <b>DSX1 LED P5 40K T3M MVOLT SPA DDBXD</b>  Note:	Type <b>SC</b>
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**Performance Data**

**Lumen Output**


Lumen values are from photometric tests performed in accordance with IESNA LM-79-08. Data is considered to be representative of the configurations shown, within the tolerances allowed by Lighting Facts. Contact factory for performance data on any configurations not shown here.

Forward Optics																			
LED Count	Drive Current	Power Package	System Watts	Dist. Type	10K (8000 K/70 CRI)					40K (4000 K/70 CRI)					50K (5000 K/70 CRI)				
					Lumens	B	U	G	LPW	Lumens	B	U	G	LPW	Lumens	B	U	G	LPW
40	1250	P6	163W	T1S	17,654	3	0	3	108	19,018	3	0	3	117	19,259	3	0	3	118
				T2S	17,635	3	0	3	108	18,998	3	0	3	117	19,238	3	0	3	118
				T2M	17,726	3	0	3	109	19,096	3	0	3	117	19,337	3	0	3	119
				T3S	17,167	3	0	3	105	18,493	3	0	3	113	18,727	3	0	3	115
				T3M	17,683	3	0	3	108	19,049	3	0	3	117	19,290	3	0	3	118
				T4M	17,399	3	0	3	106	18,635	3	0	4	114	18,871	3	0	4	116
				TFTM	17,672	3	0	3	108	19,038	3	0	4	117	19,279	3	0	4	118
				TSVS	18,379	4	0	1	113	19,800	4	0	1	121	20,050	4	0	1	123
				TSS	18,394	4	0	2	113	19,816	4	0	2	122	20,066	4	0	2	123
				TSM	18,348	4	0	2	113	19,766	4	0	2	121	20,016	4	0	2	123
				TSW	18,228	5	0	3	112	19,636	5	0	3	120	19,885	5	0	3	122
				BLC	14,489	2	0	2	89	15,609	2	0	3	96	15,806	2	0	3	97
				LCCO	10,781	1	0	3	66	11,614	1	0	3	71	11,761	2	0	3	72
				RCCO	10,781	1	0	3	66	11,614	1	0	3	71	11,761	2	0	3	72
				40	1400	P7	183W	T1S	19,227	3	0	3	105	20,712	3	0	3	113	20,975
T2S	19,206	3	0					3	105	20,690	3	0	3	113	20,952	3	0	3	114
T2M	19,305	3	0					3	105	20,797	3	0	3	114	21,060	3	0	3	115
T3S	18,696	3	0					3	102	20,141	3	0	3	110	20,396	3	0	4	111
T3M	19,258	3	0					3	105	20,746	3	0	3	113	21,009	3	0	3	115
T4M	18,840	3	0					4	103	20,296	3	0	4	111	20,553	3	0	4	112
TFTM	19,246	3	0					4	105	20,734	3	0	4	113	20,996	3	0	4	115
TSVS	20,017	4	0					1	109	21,564	4	0	1	118	21,837	4	0	1	119
TSS	20,033	4	0					2	109	21,581	4	0	2	118	21,854	4	0	2	119
TSM	19,983	4	0					2	109	21,527	5	0	3	118	21,799	5	0	3	119
TSW	19,852	5	0					3	108	21,385	5	0	3	117	21,656	5	0	3	118
BLC	15,780	2	0					3	86	16,999	2	0	3	93	17,214	2	0	3	94
LCCO	11,742	2	0					3	64	12,649	2	0	3	69	12,809	2	0	3	70
RCCO	11,742	2	0					3	64	12,649	2	0	3	69	12,809	2	0	3	70
60	1050	P8	207W					T1S	22,490	3	0	3	109	24,228	3	0	3	117	24,535
				T2S	22,466	3	0	4	109	24,202	3	0	4	117	24,509	3	0	4	118
				T2M	22,582	3	0	3	109	24,327	3	0	3	118	24,635	3	0	3	119
				T3S	21,870	3	0	4	106	23,560	3	0	4	114	23,858	3	0	4	115
				T3M	22,527	3	0	4	109	24,268	3	0	4	117	24,575	3	0	4	119
				T4M	22,038	3	0	4	106	23,741	3	0	4	115	24,041	3	0	4	116
				TFTM	22,513	3	0	4	109	24,253	3	0	4	117	24,560	3	0	4	119
				TSVS	23,415	5	0	1	113	25,224	5	0	1	122	25,543	5	0	1	123
				TSS	23,434	4	0	2	113	25,244	4	0	2	122	25,564	4	0	2	123
				TSM	23,374	5	0	3	113	25,181	5	0	3	122	25,499	5	0	3	123
				TSW	23,221	5	0	4	112	25,016	5	0	4	121	25,332	5	0	4	122
				BLC	18,458	2	0	3	89	19,885	2	0	3	96	20,136	2	0	3	97
				LCCO	13,735	2	0	3	66	14,796	2	0	4	71	14,983	2	0	4	72
				RCCO	13,735	2	0	3	66	14,796	2	0	4	71	14,983	2	0	4	72
				60	1250	P9	241W	T1S	25,575	3	0	3	106	27,551	3	0	3	114	27,900
T2S	25,548	3	0					4	106	27,522	3	0	4	114	27,871	3	0	4	116
T2M	25,680	3	0					3	107	27,664	3	0	3	115	28,014	3	0	3	116
T3S	24,870	3	0					4	103	26,791	3	0	4	111	27,130	3	0	4	113
T3M	25,617	3	0					4	106	27,597	3	0	4	115	27,946	3	0	4	116
T4M	25,061	3	0					4	104	26,997	3	0	4	112	27,339	3	0	4	113
TFTM	25,602	3	0					4	106	27,580	3	0	4	114	27,929	3	0	4	116
TSVS	26,626	5	0					1	110	28,684	5	0	1	119	29,047	5	0	1	121
TSS	26,648	4	0					2	111	28,707	5	0	2	119	29,070	5	0	2	121
TSM	26,581	5	0					3	110	28,635	5	0	3	119	28,997	5	0	3	120
TSW	26,406	5	0					4	110	28,447	5	0	4	118	28,807	5	0	4	120
BLC	20,990	2	0					3	87	22,612	2	0	3	94	22,898	2	0	3	95
LCCO	15,619	2	0					4	65	16,825	2	0	4	70	17,038	2	0	4	71
RCCO	15,619	2	0					4	65	16,825	2	0	4	70	17,038	2	0	4	71



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	ALASKA ARCHITECTURAL LIGHTING Project 22-28191-2 SEARHC Juneau Vintage Park	Catalog Number: DSX1 LED P5 40K T3M MVOLT SPA DDBXD	Type <b>SC</b>
	Submitted By ALASKA ARCHITECTURAL LIGHTING	Note:	

**Performance Data**

**Lumen Output**

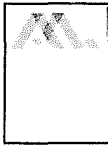
Lumen values are from photometric tests performed in accordance with IESNA LM-79-08. Data is considered to be representative of the configurations shown, within the tolerances allowed by Lighting Facts. Contact factory for performance data on any configurations not shown here.

Rotated Optics																							
LED Count	Drive Current	Power Package	System Watts	Dist. Type	30K (5000 K, 70 CRI)					40K (4000 K, 70 CRI)					50K (5000 K, 70 CRI)								
					Lumens	B	U	G	LPW	Lumens	B	U	G	LPW	Lumens	B	U	G	LPW				
60	530	P10	106W	T1S	13,042	3	0	3	123	14,050	3	0	3	133	14,228	3	0	3	134				
				T2S	12,967	4	0	4	122	13,969	4	0	4	132	14,146	4	0	4	133				
				T2M	13,201	3	0	3	125	14,221	3	0	3	134	14,401	3	0	3	136				
				T3S	12,766	4	0	4	120	13,752	4	0	4	130	13,926	4	0	4	131				
				T3M	13,193	4	0	4	124	14,213	4	0	4	134	14,393	4	0	4	136				
				T4M	12,944	4	0	4	122	13,945	4	0	4	132	14,121	4	0	4	133				
				IFTM	13,279	4	0	4	125	14,305	4	0	4	135	14,486	4	0	4	137				
				TSVS	13,372	3	0	1	126	14,405	4	0	1	136	14,588	4	0	1	138				
				TSS	13,260	3	0	1	125	14,284	3	0	1	135	14,465	3	0	1	136				
				TSM	13,256	4	0	2	125	14,281	4	0	2	135	14,462	4	0	2	136				
				TSW	13,137	4	0	3	124	14,153	4	0	3	134	14,332	4	0	3	135				
				BLC	10,906	3	0	3	103	11,749	3	0	3	111	11,898	3	0	3	112				
				LCCO	7,789	1	0	3	73	8,391	1	0	3	79	8,497	1	0	3	80				
				RCCO	7,779	4	0	4	73	8,380	4	0	4	79	8,486	4	0	4	80				
				60	700	P11	137W	T1S	16,556	3	0	3	121	17,835	3	0	3	130	18,061	4	0	4	132
								T2S	16,461	4	0	4	120	17,733	4	0	4	129	17,957	4	0	4	131
								T2M	16,758	4	0	4	122	18,053	4	0	4	132	18,281	4	0	4	133
								T3S	16,205	4	0	4	118	17,457	4	0	4	127	17,678	4	0	4	129
T3M	16,748	4	0					4	122	18,042	4	0	4	132	18,271	4	0	4	133				
T4M	16,432	4	0					4	120	17,702	4	0	4	129	17,926	4	0	4	131				
IFTM	16,857	4	0					4	123	18,159	4	0	4	133	18,389	4	0	4	134				
TSVS	16,975	4	0					1	124	18,287	4	0	1	133	18,518	4	0	1	135				
TSS	16,832	4	0					1	123	18,133	4	0	2	132	18,362	4	0	2	134				
TSM	16,828	4	0					2	123	18,128	4	0	2	132	18,358	4	0	2	134				
TSW	16,677	4	0					3	122	17,966	5	0	3	131	18,193	5	0	3	133				
BLC	13,845	3	0					3	101	14,915	3	0	3	109	15,103	3	0	3	110				
LCCO	9,888	1	0					3	72	10,652	2	0	3	78	10,787	2	0	3	79				
RCCO	9,875	4	0					4	72	10,638	4	0	4	78	10,773	4	0	4	79				
60	1050	P12	207W					T1S	22,996	4	0	4	111	24,773	4	0	4	120	25,087	4	0	4	121
								T2S	22,864	4	0	4	110	24,631	5	0	5	119	24,943	5	0	5	120
								T2M	23,277	4	0	4	112	25,075	4	0	4	121	25,393	4	0	4	123
								T3S	22,509	4	0	4	109	24,248	5	0	5	117	24,555	5	0	5	119
				T3M	23,263	4	0	4	112	25,061	4	0	4	121	25,376	4	0	4	123				
				T4M	22,824	5	0	5	110	24,588	5	0	5	119	24,899	5	0	5	120				
				IFTM	23,414	5	0	5	113	25,223	5	0	5	122	25,543	5	0	5	124				
				TSVS	23,579	5	0	1	114	25,401	5	0	1	123	25,722	5	0	1	124				
				TSS	23,380	4	0	2	113	25,187	4	0	2	122	25,506	4	0	2	123				
				TSM	23,374	5	0	3	113	25,181	5	0	3	122	25,499	5	0	3	123				
				TSW	23,165	5	0	4	112	24,955	5	0	4	121	25,271	5	0	4	122				
				BLC	19,231	4	0	4	93	20,717	4	0	4	100	20,979	4	0	4	101				
				LCCO	13,734	2	0	3	66	14,796	2	0	4	71	14,983	2	0	4	72				
				RCCO	13,716	4	0	4	66	14,776	4	0	4	71	14,963	4	0	4	72				
				60	1250	P13	231W	T1S	25,400	4	0	4	110	27,363	4	0	4	118	27,709	4	0	4	120
								T2S	25,254	5	0	5	109	27,205	5	0	5	118	27,550	5	0	5	119
								T2M	25,710	4	0	4	111	27,696	4	0	4	120	28,047	4	0	4	121
								T3S	24,862	5	0	5	108	26,783	5	0	5	116	27,122	5	0	5	117
T3M	25,695	5	0					5	111	27,680	5	0	5	120	28,031	5	0	5	121				
T4M	25,210	5	0					5	109	27,158	5	0	5	118	27,502	5	0	5	119				
IFTM	25,861	5	0					5	112	27,860	5	0	5	121	28,212	5	0	5	122				
TSVS	26,043	5	0					1	113	28,056	5	0	1	121	28,411	5	0	1	123				
TSS	25,824	4	0					2	112	27,819	5	0	2	120	28,172	5	0	2	122				
TSM	25,818	5	0					3	112	27,813	5	0	3	120	28,165	5	0	3	122				
TSW	25,586	5	0					4	111	27,563	5	0	4	119	27,912	5	0	4	121				
BLC	21,241	4	0					4	92	22,882	4	0	4	99	23,172	4	0	4	100				
LCCO	15,170	2	0					4	66	16,342	2	0	4	71	16,549	2	0	4	72				
RCCO	15,150	5	0					5	66	16,321	5	0	5	71	16,527	5	0	5	72				



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	<b>ALASKA ARCHITECTURAL LIGHTING</b>	<b>Project 22-28191-2</b> <b>SEARHC Juneau Vintage Park</b>  Submitted By <b>ALASKA ARCHITECTURAL LIGHTING</b>	<b>Catalog Number: DSX1 LED P5 40K T3M MVOLT SPA DDBXD</b>  Note:	Type <b>SC</b>
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**FEATURES & SPECIFICATIONS**

**INTENDED USE**

The sleek design of the D-Series Size 1 reflects the embedded high performance LED technology. It is ideal for many commercial and municipal applications, such as parking lots, plazas, campuses, and streetscapes.

**CONSTRUCTION**

Single-piece die-cast aluminum housing has integral heat sink fins to optimize thermal management through conductive and convective cooling. Modular design allows for ease of maintenance and future light engine upgrades. The LED drivers are mounted in direct contact with the casting to promote low operating temperature and long life. Housing is completely sealed against moisture and environmental contaminants (IP65). Low EPA (1.01 ft<sup>3</sup>) for optimized pole wind loading.

**FINISH**

Exterior parts are protected by a zinc-infused Super Durable TGIC thermostat powder coat finish that provides superior resistance to corrosion and weathering. A tightly controlled multi-stage process ensures a minimum 3 mils thickness for a finish that can withstand extreme climate changes without cracking or peeling. Available in both textured and non-textured finishes.

**OPTICS**

Precision-molded proprietary acrylic lenses are engineered for superior area lighting distribution, uniformity, and pole spacing. Light engines are available in standard 3000 K, 4000 K and 5000 K (70 CRI) configurations. The D-Series Size 1 has zero uplight and qualifies as a Nighttime Friendly™ product, meaning it is consistent with the LEED® and Green Globes™ criteria for eliminating wasteful uplight.

**ELECTRICAL**

Light engine configurations consist of high-efficacy LEDs mounted to metal-core circuit boards to maximize heat dissipation and promote long life (up to L85/100,000 hours at 25°C). Class 1 electronic drivers are designed to have a power factor >90%, THD <20%, and an expected life of 100,000 hours with <1% failure rate. Easily serviceable 10kV surge protection device meets a minimum Category C Low operation (per ANSI/IEEE C62.41.2).

**STANDARD CONTROLS**

The DSX1 LED area luminaire has a number of control options. DSX Size 1, comes standard with 0-10V dimming drivers. Dusk to dawn controls can be utilized via optional NEMA twist-lock photocell receptacles. Integrated motion sensors with on-board photocells feature field-adjustable programming and are suitable for mounting heights up to 30 feet.

**nLIGHT AIR CONTROLS**

The DSX1 LED area luminaire is also available with nLight® AIR for the ultimate in wireless control. This powerful controls platform provides out-of-the-box basic motion sensing and photocontrol functionality and is suitable for mounting heights up to 40 feet. Once commissioned using a smartphone and the easy-to-use CLAIRITY app, nLight AIR equipped luminaires can be grouped, resulting in motion sensor and photocell group response without the need for additional equipment. Scheduled dimming with motion sensor over-ride can be achieved when used with the nLight Eclipse. Additional information about Light Air can be found here.

**INSTALLATION**

Included mounting block and integral arm facilitate quick and easy installation. Stainless steel bolts fasten the mounting block securely to poles and walls, enabling the D-Series Size 1 to withstand up to a 3.0 G vibration load rating per ANSI C136.31. The D-Series Size 1 utilizes the AERIS™ series pole drilling pattern (template #8). NEMA photocontrol receptacle are also available.

**LISTINGS**

UL listed to meet U.S. and Canadian standards. UL Listed for wet locations. Light engines are IP66 rated; luminaire is IP65 rated. Rated for -40°C minimum ambient. U.S. Patent No. D672,492 S. International patent pending.

DesignLights Consortium® (DLC) Premium qualified product and DLC qualified product. Not all versions of this product may be DLC Premium qualified or DLC qualified. Please check the DLC Qualified Products List at [www.designlights.org/](http://www.designlights.org/) QPL to confirm which versions are qualified.

International Dark-Sky Association (IDA) Fixture Seal of Approval (FSA) is available for all products on this page utilizing 3000K color temperature only.

**BUY AMERICAN**

Product with the BAA option is assembled in the USA and meets the Buy America(n) government procurement requirements under FAR, DFARS and DOT. Please refer to [www.acuitybrands.com/buy-american](http://www.acuitybrands.com/buy-american) for additional information.

**WARRANTY**

5-year limited warranty. Complete warranty terms located at: [www.acuitybrands.com/support/customer-support/terms-and-conditions](http://www.acuitybrands.com/support/customer-support/terms-and-conditions)

**Note:** Actual performance may differ as a result of end-user environment and application.

All values are design or typical values, measured under laboratory conditions at 25 °C.

Specifications subject to change without notice.

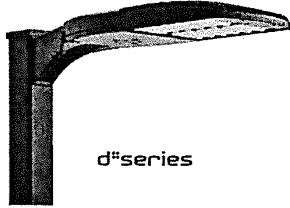


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	ALASKA ARCHITECTURAL LIGHTING Project 22-28191-2 SEARHC Juneau Vintage Park	Catalog Number: DSX1 LED P5 40K T5M MVOLT SPA DDBXD	Type <b>SD</b>
	Submitted By ALASKA ARCHITECTURAL LIGHTING	Note:	



## D-Series Size 1 LED Area Luminaire



Buy American

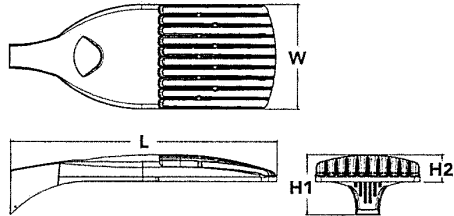
Catalog Number \_\_\_\_\_

Notes \_\_\_\_\_

Type \_\_\_\_\_

### Specifications

- EPA: 1.01 ft<sup>2</sup> (0.09 m<sup>2</sup>)
- Length: 33" (83.8 cm)
- Width: 13" (33.0 cm)
- Height H1: 7-1/2" (19.0 cm)
- Height H2: 3-1/2"
- Weight (max): 27 lbs (12.2 kg)



### Introduction

The modern styling of the D-Series is striking yet unobtrusive - making a bold, progressive statement even as it blends seamlessly with its environment. The D-Series distills the benefits of the latest in LED technology into a high performance, high efficacy, long-life luminaire.

The outstanding photometric performance results in sites with excellent uniformity, greater pole spacing and lower power density. It is ideal for replacing up to 750W metal halide in pedestrian and area lighting applications with typical energy savings of 65% and expected service life of over 100,000 hours.

### Ordering Information

EXAMPLE: DSX1 LED P7 40K T3M MVOLT SPA NLTAIR2 PIRHN DDBXD

Series	LEDs	Color temperature	Distribution	Voltage	Mounting	
DSX1 LED	Forward optics P1 P4 <sup>1</sup> P7 <sup>1</sup> P2 P5 <sup>1</sup> P8 P3 P6 <sup>1</sup> P9 <sup>1</sup> Rotated optics P10 <sup>2</sup> P12 <sup>2</sup> P11 <sup>2</sup> P13 <sup>1,2</sup>	30K 3000 K 40K 4000 K 50K 5000 K	T1S Type I short (Automotive) T2S Type II short T2M Type II medium T3S Type III short T3M Type III medium T4M Type IV medium TFTM Forward throw medium	TSVS Type V very short <sup>4</sup> TSS Type V short <sup>4</sup> TSM Type V medium <sup>4</sup> TSW Type V wide <sup>4</sup> BLC Backlight control <sup>4</sup> LCCO Left corner cutoff <sup>4</sup> RCCO Right corner cutoff <sup>4</sup>	MVOLT <sup>3</sup> XVOLT (277V-480V) <sup>6,7,8</sup> 120 <sup>9</sup> 208 <sup>9</sup> 240 <sup>9</sup> 277 <sup>9</sup> 347 <sup>9</sup> 480 <sup>9</sup>	Shipped included SPA Square pole mounting RPA Round pole mounting <sup>10</sup> WBA Wall bracket <sup>1</sup> SPUMBA Square pole universal mounting adaptor <sup>11</sup> RPUMBA Round pole universal mounting adaptor <sup>2</sup> Shipped separately KMA8 DDBXD U Mast arm mounting bracket adaptor (specify finish) <sup>12</sup>

Control options	Other options	Finish
Shipped installed NLTAIR2 nLight AIR generation 2 enabled <sup>13</sup> PIRHN Network, high/low motion/ambient sensor <sup>14</sup> PER NEMA twist-lock receptacle only (controls ordered separate) <sup>15,16</sup> PER5 Five-pin receptacle only (controls ordered separate) <sup>15,16</sup> PER7 Seven-pin receptacle only (controls ordered separate) <sup>15,16</sup> DMG 0-10v dimming wires pulled outside fixture (for use with an external control, ordered separately) <sup>17</sup> DS Dual switching <sup>18,19,20</sup>	Shipped installed HS House-side shield <sup>4</sup> SF Single fuse (120, 277, 347V) <sup>9</sup> DF Double fuse (208, 240, 480V) <sup>9</sup> L90 Left rotated optics <sup>2</sup> R90 Right rotated optics <sup>2</sup> HA 50°C ambient operations <sup>2</sup> BAA Buy America(n) Act Compliant Shipped separately BS Blind spikes <sup>24</sup> EGS External glare shield	DDBXD Dark bronze DBLXD Black DNAXD Natural aluminum DWHXD White DDBTXD Textured dark bronze DBLBXD Textured black DNATXD Textured natural aluminum DWHGXD Textured white

PLEASE VERIFY FINISH



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	<b>Project 22-28191-2</b> <b>SEARCH Juneau Vintage Park</b>  Submitted By <b>ALASKA ARCHITECTURAL LIGHTING</b>	Catalog Number: <b>DSX1 LED P5 40K T5M MVOLT SPA DDBXD</b>  Note:	Type <b>SD</b>
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**Ordering Information**

**Accessories**  
Ordered and shipped separately.

DLL127F 1.5JU	Photocell - SSL twist-lock (120-277V) <sup>21</sup>
DLL347F 1.5 CULJU	Photocell - SSL twist-lock (347V) <sup>21</sup>
DLL480F 1.5 CULJU	Photocell - SSL twist-lock (480V) <sup>21</sup>
DSHORT SBK U	Shorting cap <sup>22</sup>
DSX1HS 30C U	House-side shield for P1, P2, P3, P4 and P5 <sup>23</sup>
DSX1HS 40C U	House-side shield for P6 and P7 <sup>23</sup>
DSX1HS 60C U	House-side shield for P8, P9, P10, P11 and P12 <sup>23</sup>
PUMBA DDBXD U*	Square and round pole universal mounting bracket (specify finish) <sup>24</sup>
KMA8 DDBXD U	Mast arm mounting bracket adaptor (specify finish) <sup>25</sup>
DSX1EGS (FINISH) U	External glare shield

For more control options, visit [LTL](#) and [PLM](#) online.

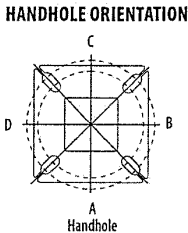
- NOTES**
- HA not available with P4, P5, P6, P7, P9 and P13.
  - P10, P11, P12, P13 not available with P5L, B50L, B50V, available together.
  - Any Type 5 distribution with photocell, is not available with WBA.
  - Not available with HS.
  - MVOLT driver operates on any line voltage from 120-277V (50/60 Hz).
  - XVOLT only suitable for use with P3, P5, P6, P7, P9 and P13.
  - XVOLT works with any voltage between 277V and 480V.
  - XVOLT not available with fusing (SF or DF) and not available with PIR, PIRH, PIR1FC3V, PIRH1FC3V.
  - Single fuse (SF) requires 120V, 277V or 347V. Double fuse (DF) requires 208V, 240V or 480V. XVOLT not available with fusing (SF or DF).
  - Suitable for mounting to round poles between 3.5" and 12" diameter.
  - Universal mounting brackets intended for retrofit on existing, pre-drilled poles only. 1.5 G vibration load rating per ANCI C136.31. Only usable when pole's drill pattern is NOT Lithonia template #8.
  - Must order fixture with SPA option. Must be ordered as a separate accessory; see Accessories information. For use with 2-3/8" diameter mast arm (not included).
  - Must be ordered with PIRHN. Sensor cover available only in dark bronze, black, white and natural aluminum colors.
  - Must be ordered with NEDAR2. For more information on night air 2, visit [LTL](#).
  - Photocell ordered and shipped as a separate line item from Acuity Brands Controls. See accessories. Shorting cap included.
  - If ROAM\* node required, it must be ordered and shipped as a separate line item from Acuity Brands Controls. Node with integral dimming.
  - DMG not available with PIRHN, PERS, PER7, PIR, PIRH, PIR1FC3V or PIRH1FC3V, FAC.
  - Provides 50/50 fixture operation via (2) independent drivers. Not available with PER, PERS, PER7, PIR or PIRH. Not available P1, P2, P3, P4 or P5.
  - Requires (2) separately switched circuits with isolated neutral.
  - Reference Controls Option Default settings table on page 4.
  - Reference Motion Sensor table on page 4 to see functionality.
  - Not available with other dimming controls options.
  - Not available with BLC, LCCO and RCCO distribution. Also available as a separate accessory; see Accessories information.
  - Must be ordered with fixture for factory pre-drilling.
  - Requires luminaire to be specified with PER, PERS or PER7 option. See Control Option Table on page 4.
  - For retrofit use only. Only usable when pole's drill pattern is NOT Lithonia template #8.

**Options**

**EGS - External Glare Shield**



**Drilling**

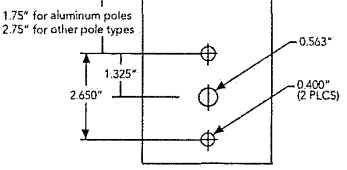


**Tenon Mounting Slipfitter**

Tenon O.D.	Mounting	Single Unit	2 @ 180	2 @ 90	3 @ 90	3 @ 120	4 @ 90
2-3/8"	RPA	AS3-5 190	AS3-5 280	AS3-5 290	AS3-5 390	AS3-5 320	AS3-5 490
2-7/8"	RPA	AST25-190	AST25-280	AST25-290	AST25-390	AST25-320	AST25-490
4"	RPA	AST35-190	AST35-280	AST35-290	AST35-390	AST35-320	AST35-490

Mounting Option	Drilling Template	Single	2 @ 180	2 @ 90	3 @ 90	3 @ 120	4 @ 90
Head Location		Side B	Side B & D	Side B & C	Side B, C & D	Round Pole Only	Side A, B, C & D
Drill Nomenclature	#8	DM19AS	DM28AS	DM29AS	DM39AS	DM32AS	DM49AS

**Template #8**



**DSX1 Area Luminaire - EPA**

\*Includes luminaire and integral mounting arm. Other tenons, arms, brackets or other accessories are not included in this EPA data.

Fixture Quantity & Mounting Configuration	Single DM19	2 @ 180 DM28	2 @ 90 DM29	3 @ 90 DM39	3 @ 120 DM32	4 @ 90 DM49
Mounting Type						
DSX1 LED	1.013	2.025	1.945	3.038	2.850	3.749


**QUOTING BOTH OPTIONS**

	Drilling Template	Minimum Acceptable Outside Pole Dimension					
SPA	#8	2-7/8"	2-7/8"	3.5"	3.5"	3"	3.5"
RPA	#8	2-7/8"	2-7/8"	3.5"	3.5"	3"	3.5"
SPUMBA	#5	2-7/8"	3"	4"	4"	3.5"	4"
RPUMBA	#5	2-7/8"	3.5"	5"	5"	3.5"	5"



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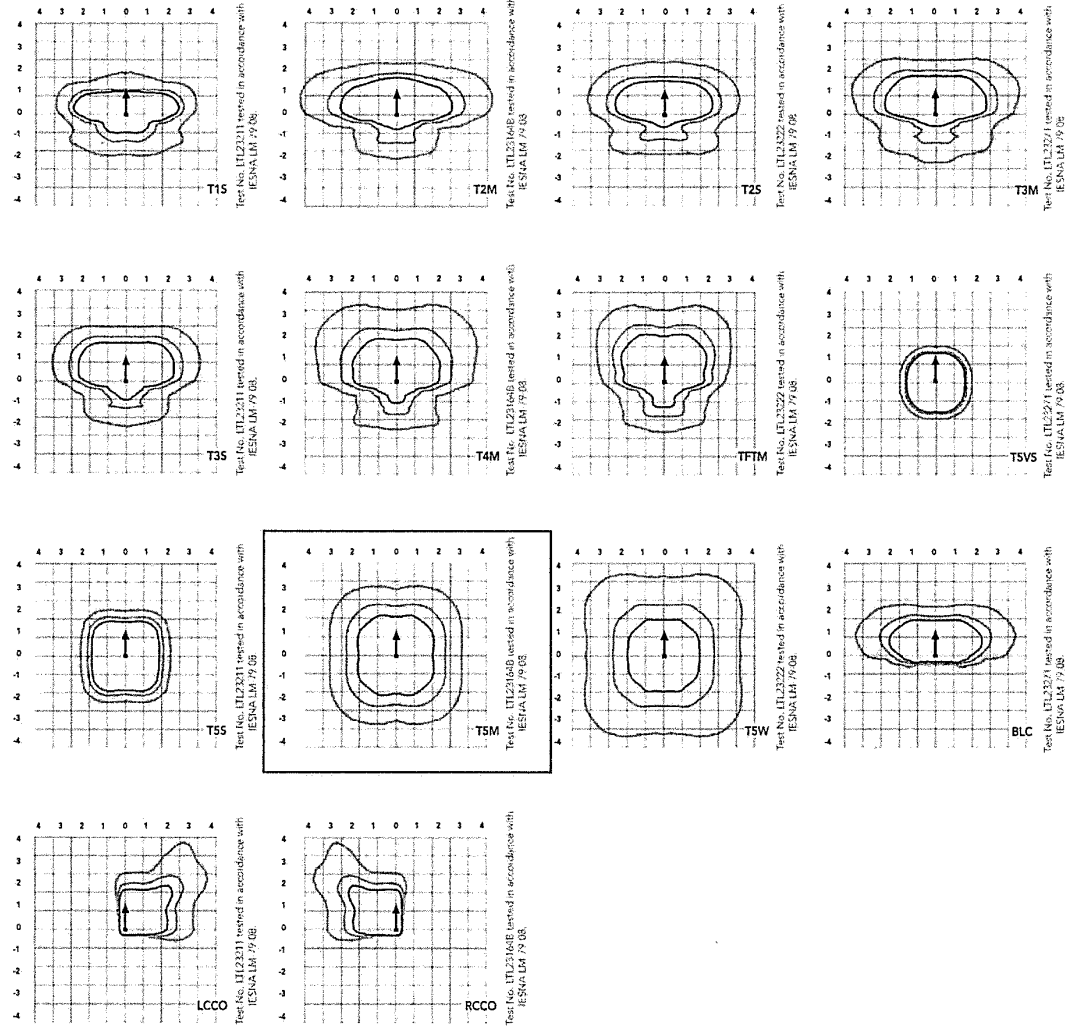
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 <p>ALASKA ARCHITECTURAL LIGHTING</p>	<p>Project 22-28191-2 SEARHC Juneau Vintage Park</p> <p>Submitted By ALASKA ARCHITECTURAL LIGHTING</p>	<p>Catalog Number: DSX1 LED P5 40K T5M MVOLT SPA DDBXD</p> <p>Note:</p>	<p>Type <b>SD</b></p>
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**Photometric Diagrams** To see complete photometric reports or download .ies files for this product, visit Lithonia Lighting's D-Series Area Size 1 homepage.

Isofootcandle plots for the DSX1 LED 60C 1000 40K. Distances are in units of mounting height (25').


- LEGEND**
- 0.1 fc
  - 0.5 fc
  - 1.0 fc



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	ALASKA ARCHITECTURAL LIGHTING Project 22-28191-2 SEARHC Juneau Vintage Park	Catalog Number: DSX1 LED P5 40K T5M MVOLT SPA DDBXD	Type <b>SD</b>
	Submitted By ALASKA ARCHITECTURAL LIGHTING	Note:	

**Performance Data**

**Lumen Ambient Temperature (LAT) Multipliers**

Use these factors to determine relative lumen output for average ambient temperatures from 0-40°C (32-104°F).

Ambient		Lumen Multiplier
0°C	32°F	1.04
5°C	41°F	1.04
10°C	50°F	1.03
15°C	59°F	1.02
20°C	68°F	1.01
25°C	77°F	1.00
30°C	86°F	0.99
35°C	95°F	0.98
40°C	104°F	0.97

**Projected LED Lumen Maintenance**

Data references the extrapolated performance projections for the platforms noted in a 25°C ambient, based on 10,000 hours of LED testing (tested per IESNA LM-80-08 and projected per IESNA TM-21-11).

To calculate LLF, use the lumen maintenance factor that corresponds to the desired number of operating hours below. For other lumen maintenance values, contact factory.

Operating Hours	Lumen Maintenance Factor
0	1.00
25,000	0.96
50,000	0.92
100,000	0.85

**Motion Sensor Default Settings**

Option	Dimmed State	High Level (when triggered)	Photocell Operation	Dwell Time	Ramp-up Time	Ramp-down Time
PIR or PIRH	3V (37%) Output	10V (100%) Output	Enabled @ 5FC	5 min	3 sec	5 min
*PIR1FC3V or PIRH1FC3V	3V (37%) Output	10V (100%) Output	Enabled @ 1FC	5 min	3 sec	5 min

\*for use when motion sensor is used as dusk to dawn control.


**Electrical Load**

	Performance Package	LED Count	Drive Current	Wattage	Current (A)					
					120	208	240	277	347	480
Forward Optics (Non-Rotated)	P1	30	530	54	0.45	0.26	0.23	0.19	0.10	0.12
	P2	30	700	70	0.59	0.34	0.30	0.25	0.20	0.16
	P3	30	1050	102	0.86	0.50	0.44	0.38	0.30	0.22
	P4	30	1250	125	1.06	0.60	0.52	0.46	0.37	0.27
	P5	30	1400	138	1.16	0.67	0.58	0.51	0.40	0.29
	P6	40	1250	163	1.36	0.78	0.68	0.59	0.47	0.34
	P7	40	1400	183	1.53	0.88	0.76	0.66	0.53	0.38
	P8	60	1050	207	1.74	0.98	0.87	0.76	0.64	0.49
	P9	60	1250	241	2.01	1.16	1.01	0.89	0.70	0.51
Rotated Optics (Requires L90 or R90)	P10	60	530	106	0.90	0.52	0.47	0.43	0.33	0.27
	P11	60	700	137	1.15	0.67	0.60	0.53	0.42	0.32
	P12	60	1050	207	1.74	0.99	0.87	0.76	0.60	0.46
	P13	60	1250	231	1.93	1.12	0.97	0.86	0.67	0.49

**Controls Options**

Nomenclature	Description	Functionality	Primary control device	Notes
FAO	Field adjustable output device installed inside the luminaire; wired to the driver dimming leads.	Allows the luminaire to be manually dimmed, effectively trimming the light output.	FAO device	Cannot be used with other controls options that need the 0-10V leads
DS	Drivers wired independently for 50/50 luminaire operation	The luminaire is wired to two separate circuits, allowing for 50/50 operation.	Independently wired drivers	Requires two separately switched circuits. Consider nLight AIR as a more cost effective alternative.
PER5 or PER7	Twist-lock photocell receptacle	Compatible with standard twist-lock photocells for dusk to dawn operation, or advanced control nodes that provide 0-10V dimming signals.	Twist-lock photocells such as DLL Elite or advanced control nodes such as ROAM.	Pins 4 & 5 to dimming leads on driver; Pins 6 & 7 are capped inside luminaire
PIR or PIRH	Motion sensors with integral photocell. PIR for 8-15' mounting; PIRH for 15-30' mounting	Luminaires dim when no occupancy is detected.	Acuity Controls SBGR	Also available with PIRH1FC3V when the sensor photocell is used for dusk-to-dawn operation.
NLAIR2 PIRHN	nLight AIR enabled luminaire for motion sensing, photocell and wireless communication.	Motion and ambient light sensing with group response. Scheduled dimming with motion sensor over-ride when wirelessly connected to the nLight Eclipse.	nLight Air rSDGR	nLight AIR sensors can be programmed and commissioned from the ground using the CLARITY Pro app.



	ALASKA ARCHITECTURAL LIGHTING Project 22-28191-2 SEARHC Juneau Vintage Park	Catalog Number: DSX1 LED P5 40K T5M MVOLT SPA DDBXD	Type <b>SD</b>
	Submitted By ALASKA ARCHITECTURAL LIGHTING	Note:	

**Performance Data**

**Lumen Output**


Lumen values are from photometric tests performed in accordance with IESNA LM-79-08. Data is considered to be representative of the configurations shown, within the tolerances allowed by Lighting Facts Contact factory for performance data on any configurations not shown here.

Forward Optics																							
LED Count	Drive Current	Power Package	System Watts	Dist. Type	30K (3000 K, 70 CRI)					40K (4000 K, 70 CRI)					50K (5000 K, 70 CRI)								
					Lumens	B	U	G	LPW	Lumens	B	U	G	LPW	Lumens	B	U	G	LPW				
30	530	P1	54W	T1S	6,457	2	0	2	120	6,956	2	0	2	129	7,044	2	0	2	130				
				T2S	6,450	2	0	2	119	6,949	2	0	2	129	7,037	2	0	2	130				
				T2M	6,483	1	0	1	120	6,984	2	0	2	129	7,073	2	0	2	131				
				T3S	6,279	2	0	2	116	6,764	2	0	2	125	6,850	2	0	2	127				
				T3M	6,468	1	0	2	120	6,967	1	0	2	129	7,056	1	0	2	131				
				T4M	6,327	1	0	2	117	6,816	1	0	2	126	6,902	1	0	2	128				
				TF1M	6,464	1	0	2	120	6,963	1	0	2	129	7,051	1	0	2	131				
				TSVS	6,722	2	0	0	124	7,242	3	0	0	134	7,334	3	0	0	136				
				TSS	6,728	2	0	1	125	7,248	2	0	1	134	7,340	2	0	1	136				
				TSM	6,711	3	0	1	124	7,229	3	0	1	134	7,321	3	0	2	136				
				TSW	6,667	3	0	2	123	7,182	3	0	2	133	7,273	3	0	2	135				
				BLC	5,299	1	0	1	98	5,709	1	0	2	106	5,781	1	0	2	107				
				LCCO	3,943	1	0	2	73	4,248	1	0	2	79	4,302	1	0	2	80				
				RCCO	3,943	1	0	2	73	4,248	1	0	2	79	4,302	1	0	2	80				
				30	700	P2	70W	T1S	8,249	2	0	2	118	8,886	2	0	2	127	8,999	2	0	2	129
								T2S	8,240	2	0	2	118	8,877	2	0	2	127	8,989	2	0	2	128
								T2M	8,283	2	0	2	118	8,923	2	0	2	127	9,036	2	0	2	129
T3S	8,021	2	0					2	115	8,641	2	0	2	123	8,751	2	0	2	125				
T3M	8,263	2	0					2	118	8,901	2	0	2	127	9,014	2	0	2	129				
T4M	8,083	2	0					2	115	8,708	2	0	2	124	8,818	2	0	2	126				
TF1M	8,257	2	0					2	118	8,896	2	0	2	127	9,008	2	0	2	129				
TSVS	8,588	3	0					0	123	9,252	3	0	0	132	9,369	3	0	0	134				
TSS	8,595	3	0					1	123	9,259	3	0	1	132	9,376	3	0	1	134				
TSM	8,573	3	0					2	122	9,236	3	0	2	132	9,353	3	0	2	134				
TSW	8,517	3	0					2	122	9,175	4	0	2	131	9,291	4	0	2	133				
BLC	6,770	1	0					2	97	7,293	1	0	2	104	7,386	1	0	2	106				
LCCO	5,038	1	0					2	72	5,427	1	0	2	78	5,496	1	0	2	79				
RCCO	5,038	1	0					2	72	5,427	1	0	2	78	5,496	1	0	2	79				
30	1050	P3	102W					T1S	11,661	2	0	2	114	12,562	3	0	3	123	12,721	3	0	3	125
								T2S	11,648	2	0	2	114	12,548	3	0	3	123	12,707	3	0	3	125
								T2M	11,708	2	0	2	115	12,613	2	0	2	124	12,773	2	0	2	125
				T3S	11,339	2	0	2	111	12,215	3	0	3	120	12,370	3	0	3	121				
				T3M	11,680	2	0	2	115	12,582	2	0	2	123	12,742	2	0	2	125				
				T4M	11,426	2	0	3	112	12,309	2	0	3	121	12,465	2	0	3	122				
				TF1M	11,673	2	0	2	114	12,575	2	0	3	123	12,734	2	0	3	125				
				TSVS	12,140	3	0	1	119	13,078	3	0	1	128	13,244	3	0	1	130				
				TSS	12,150	3	0	1	119	13,089	3	0	1	128	13,254	3	0	1	130				
				TSM	12,119	4	0	2	119	13,056	4	0	2	128	13,221	4	0	2	130				
				TSW	12,040	4	0	3	118	12,970	4	0	3	127	13,134	4	0	3	129				
				BLC	9,570	1	0	2	94	10,310	1	0	2	101	10,440	1	0	2	102				
				LCCO	7,121	1	0	3	70	7,671	1	0	3	75	7,768	1	0	3	76				
				RCCO	7,121	1	0	3	70	7,671	1	0	3	75	7,768	1	0	3	76				
				30	1250	P4	125W	T1S	13,435	3	0	3	107	14,473	3	0	3	116	14,657	3	0	3	117
								T2S	13,421	3	0	3	107	14,458	3	0	3	116	14,641	3	0	3	117
								T2M	13,490	2	0	2	108	14,532	3	0	3	116	14,716	3	0	3	118
T3S	13,064	3	0					3	105	14,074	3	0	3	113	14,252	3	0	3	114				
T3M	13,457	2	0					2	108	14,497	2	0	2	116	14,681	2	0	2	117				
T4M	13,165	2	0					3	105	14,182	2	0	3	113	14,362	2	0	3	115				
TF1M	13,449	2	0					3	108	14,488	2	0	3	116	14,672	2	0	3	117				
TSVS	13,987	4	0					1	112	15,068	4	0	1	121	15,259	4	0	1	122				
TSS	13,999	3	0					1	112	15,080	3	0	1	121	15,271	3	0	1	122				
TSM	13,963	4	0					2	112	15,042	4	0	2	120	15,233	4	0	2	122				
TSW	13,872	4	0					3	111	14,944	4	0	3	120	15,133	4	0	3	121				
BLC	11,027	1	0					2	88	11,879	1	0	2	95	12,029	1	0	2	96				
LCCO	8,205	1	0					3	66	8,839	1	0	3	71	8,951	1	0	3	72				
RCCO	8,205	1	0					3	66	8,839	1	0	3	71	8,951	1	0	3	72				
30	1400	P5	138W					T1S	14,679	3	0	3	106	15,814	3	0	3	115	16,014	3	0	3	116
								T2S	14,664	3	0	3	106	15,797	3	0	3	114	15,997	3	0	3	116
								T2M	14,739	3	0	3	107	15,878	3	0	3	115	16,079	3	0	3	117
				T3S	14,274	3	0	3	103	15,377	3	0	3	111	15,572	3	0	3	113				
				T3M	14,704	2	0	3	107	15,840	3	0	3	115	16,040	3	0	3	116				
				T4M	14,384	2	0	3	104	15,496	3	0	3	112	15,692	3	0	3	114				
				TF1M	14,695	2	0	3	106	15,830	3	0	3	115	16,030	3	0	3	116				
				TSVS	15,283	4	0	1	111	16,464	4	0	1	119	16,672	4	0	1	121				
				TSS	15,295	3	0	1	111	16,477	4	0	1	119	16,686	4	0	1	121				
				TSM	15,257	4	0	2	111	16,435	4	0	2	119	16,644	4	0	2	121				
				TSW	15,157	4	0	3	110	16,328	4	0	3	118	16,534	4	0	3	120				
				BLC	12,048	1	0	2	87	12,979	1	0	2	94	13,143	1	0	2	95				
				LCCO	8,965	1	0	3	65	9,657	1	0	3	70	9,780	1	0	3	71				
				RCCO	8,965	1	0	3	65	9,657	1	0	3	70	9,780	1	0	3	71				



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	ALASKA ARCHITECTURAL LIGHTING Project 22-28191-2 SEARHC Juneau Vintage Park	Catalog Number: DSX1 LED P5 40K T5M MVOLT SPA DDBXD	Type <b>SD</b>
	Submitted By ALASKA ARCHITECTURAL LIGHTING	Note:	

**Performance Data**

**Lumen Output**

Lumen values are from photometric tests performed in accordance with IESNA LM-79-08. Data is considered to be representative of the configurations shown, within the tolerances allowed by Lighting Facts. Contact factory for performance data on any configurations not shown here.


Forward Optics																							
LED Count	Drive Current	Power Package	System Watts	Dist. Type	30K (3000 K, 70 CRI)					40K (4000 K, 70 CRI)					50K (5000 K, 70 CRI)								
					Lumens	B	U	G	LPW	Lumens	B	U	G	LPW	Lumens	B	U	G	LPW				
40	1250	P6	163W	T1S	17,654	3	0	3	108	19,018	3	0	3	117	19,259	3	0	3	118				
				T2S	17,635	3	0	3	108	18,998	3	0	3	117	19,238	3	0	3	118				
				T2M	17,726	3	0	3	109	19,096	3	0	3	117	19,337	3	0	3	119				
				T3S	17,167	3	0	3	105	18,493	3	0	3	113	18,727	3	0	3	115				
				T3M	17,683	3	0	3	108	19,049	3	0	3	117	19,290	3	0	3	118				
				T4M	17,299	3	0	3	106	18,635	3	0	4	114	18,871	3	0	4	116				
				IFTM	17,672	3	0	3	108	19,038	3	0	4	117	19,279	3	0	4	118				
				TSVS	18,379	4	0	1	113	19,800	4	0	1	121	20,050	4	0	1	123				
				TSS	18,394	4	0	2	113	19,816	4	0	2	122	20,066	4	0	2	123				
				TSM	18,348	4	0	2	113	19,766	4	0	2	121	20,016	4	0	2	123				
				TSW	18,228	5	0	3	112	19,636	5	0	3	120	19,885	5	0	3	122				
				BLC	14,489	2	0	2	89	15,609	2	0	3	96	15,806	2	0	3	97				
				LCCO	10,781	1	0	3	66	11,614	1	0	3	71	11,761	2	0	3	72				
				RCCO	10,781	1	0	3	66	11,614	1	0	3	71	11,761	2	0	3	72				
				40	1400	P7	183W	T1S	19,227	3	0	3	105	20,712	3	0	3	113	20,975	3	0	3	115
								T2S	19,206	3	0	3	105	20,690	3	0	3	113	20,952	3	0	3	114
T2M	19,305	3	0					3	105	20,797	3	0	3	114	21,060	3	0	3	115				
T3S	18,696	3	0					3	102	20,141	3	0	3	110	20,396	3	0	4	111				
T3M	19,258	3	0					3	105	20,746	3	0	3	113	21,009	3	0	3	115				
T4M	18,840	3	0					4	103	20,296	3	0	4	111	20,553	3	0	4	112				
IFTM	19,246	3	0					4	105	20,734	3	0	4	113	20,996	3	0	4	115				
TSVS	20,017	4	0					1	109	21,564	4	0	1	118	21,837	4	0	1	119				
TSS	20,033	4	0					2	109	21,581	4	0	2	118	21,854	4	0	2	119				
TSM	19,983	4	0					2	109	21,527	4	0	3	118	21,799	4	0	3	119				
TSW	19,852	5	0					3	108	21,386	5	0	3	117	21,656	5	0	3	118				
BLC	15,780	2	0					3	86	16,999	2	0	3	93	17,214	2	0	3	94				
LCCO	11,742	2	0					3	64	12,649	2	0	3	69	12,809	2	0	3	70				
RCCO	11,742	2	0					3	64	12,649	2	0	3	69	12,809	2	0	3	70				
60	1050	P8	207W					T1S	22,490	3	0	3	109	24,228	3	0	3	117	24,535	3	0	3	119
								T2S	22,466	3	0	4	109	24,202	3	0	4	117	24,509	3	0	4	118
				T2M	22,582	3	0	3	109	24,327	3	0	3	118	24,635	3	0	3	119				
				T3S	21,870	3	0	4	106	23,560	3	0	4	114	23,858	3	0	4	115				
				T3M	22,527	3	0	4	109	24,288	3	0	4	117	24,575	3	0	4	119				
				T4M	22,038	3	0	4	106	23,741	3	0	4	115	24,041	3	0	4	116				
				IFTM	22,513	3	0	4	109	24,253	3	0	4	117	24,560	3	0	4	119				
				TSVS	23,415	5	0	1	113	25,224	5	0	1	122	25,543	5	0	1	123				
				TSS	23,434	4	0	2	113	25,244	4	0	2	122	25,564	4	0	2	123				
				TSM	23,374	5	0	3	113	25,181	5	0	3	122	25,499	5	0	3	123				
				TSW	23,221	5	0	4	112	25,016	5	0	4	121	25,332	5	0	4	122				
				BLC	18,458	2	0	3	89	19,885	2	0	3	96	20,136	2	0	3	97				
				LCCO	13,735	2	0	3	66	14,796	2	0	4	71	14,983	2	0	4	72				
				RCCO	13,735	2	0	3	66	14,796	2	0	4	71	14,983	2	0	4	72				
				60	1250	P9	241W	T1S	25,575	3	0	3	106	27,551	3	0	3	114	27,900	3	0	3	116
								T2S	25,548	3	0	4	106	27,522	3	0	4	114	27,871	3	0	4	116
T2M	25,680	3	0					3	107	27,664	3	0	3	115	28,014	3	0	3	116				
T3S	24,870	3	0					4	103	26,791	3	0	4	111	27,130	3	0	4	113				
T3M	25,617	3	0					4	106	27,597	3	0	4	115	27,946	3	0	4	116				
T4M	25,061	3	0					4	104	26,997	3	0	4	112	27,339	3	0	4	113				
IFTM	25,602	3	0					4	106	27,580	3	0	4	114	27,929	3	0	4	116				
TSVS	26,626	5	0					1	110	28,684	5	0	1	119	29,047	5	0	1	121				
TSS	26,648	4	0					2	111	28,707	5	0	2	119	29,070	5	0	2	121				
TSM	26,581	5	0					3	110	28,635	5	0	3	119	28,997	5	0	3	120				
TSW	26,406	5	0					4	110	28,447	5	0	4	118	28,807	5	0	4	120				
BLC	20,990	2	0					3	87	22,612	2	0	3	94	22,898	2	0	3	95				
LCCO	15,619	2	0					4	65	16,825	2	0	4	70	17,038	2	0	4	71				
RCCO	15,619	2	0					4	65	16,825	2	0	4	70	17,038	2	0	4	71				



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 <b>ALASKA ARCHITECTURAL LIGHTING</b>	Project 22-28191-2 <b>SEARHC Juneau Vintage Park</b>	Catalog Number: DSX1 LED P5 40K T5M MVOLT SPA <b>DDBXD</b>	Type <b>SD</b>
	Submitted By <b>ALASKA ARCHITECTURAL LIGHTING</b>	Note:	

**Performance Data**

**Lumen Output**


Lumen values are from photometric tests performed in accordance with IESNA LM-79-08. Data is considered to be representative of the configurations shown, within the tolerances allowed by Lighting Facts. Contact factory for performance data on any configurations not shown here.

Rotated Optics																							
LED Count	Drive Current	Power Package	System Watts	Dist. Type	30K (3000K, 70 CRI)					40K (4000K, 70 CRI)					50K (5000K, 70 CRI)								
					Lumens	B	U	G	LPW	Lumens	B	U	G	LPW	Lumens	B	U	G	LPW				
60	530	P10	106W	T1S	13,042	3	0	3	123	14,050	3	0	3	133	14,228	3	0	3	134				
				T2S	12,967	4	0	4	122	13,969	4	0	4	132	14,146	4	0	4	133				
				T2M	13,201	3	0	3	125	14,221	3	0	3	134	14,401	3	0	3	136				
				T3S	12,766	4	0	4	120	13,752	4	0	4	130	13,926	4	0	4	131				
				T3M	13,193	4	0	4	124	14,213	4	0	4	134	14,393	4	0	4	136				
				T4M	12,944	4	0	4	122	13,945	4	0	4	132	14,121	4	0	4	133				
				FTFM	13,279	4	0	4	125	14,305	4	0	4	135	14,486	4	0	4	137				
				TSVS	13,372	3	0	1	126	14,405	4	0	1	136	14,588	4	0	1	138				
				TSS	13,260	3	0	1	125	14,284	3	0	1	135	14,465	3	0	1	136				
				TSM	13,256	4	0	2	125	14,281	4	0	2	135	14,462	4	0	2	136				
				TSW	13,137	4	0	3	124	14,153	4	0	3	134	14,332	4	0	3	135				
				BLC	10,906	3	0	3	103	11,749	3	0	3	111	11,898	3	0	3	112				
				LCCO	7,789	1	0	3	73	8,391	1	0	3	79	8,497	1	0	3	80				
				RCCO	7,779	4	0	4	73	8,380	4	0	4	79	8,486	4	0	4	80				
				60	700	P11	137W	T1S	16,556	3	0	3	121	17,835	3	0	3	130	18,061	4	0	4	132
								T2S	16,461	4	0	4	120	17,733	4	0	4	129	17,957	4	0	4	131
								T2M	16,758	4	0	4	122	18,053	4	0	4	132	18,281	4	0	4	133
								T3S	16,205	4	0	4	118	17,457	4	0	4	127	17,678	4	0	4	129
								T3M	16,748	4	0	4	122	18,042	4	0	4	132	18,271	4	0	4	133
								T4M	16,432	4	0	4	120	17,702	4	0	4	129	17,926	4	0	4	131
FTFM	16,857	4	0					4	123	18,159	4	0	4	133	18,389	4	0	4	134				
TSVS	16,975	4	0					1	124	18,287	4	0	1	133	18,518	4	0	1	135				
TSS	16,832	4	0					1	123	18,133	4	0	2	132	18,362	4	0	2	134				
TSM	16,828	4	0					2	123	18,128	4	0	2	132	18,358	4	0	2	134				
TSW	16,677	4	0					3	122	17,966	5	0	3	131	18,193	5	0	3	133				
BLC	13,845	3	0					3	101	14,915	3	0	3	109	15,103	3	0	3	110				
LCCO	9,888	1	0					3	72	10,652	2	0	3	78	10,767	2	0	3	79				
RCCO	9,875	4	0					4	72	10,638	4	0	4	78	10,773	4	0	4	79				
60	1050	P12	207W					T1S	22,996	4	0	4	111	24,773	4	0	4	120	25,087	4	0	4	121
								T2S	22,864	4	0	4	110	24,631	5	0	5	119	24,943	5	0	5	120
								T2M	23,277	4	0	4	112	25,075	4	0	4	121	25,393	4	0	4	123
								T3S	22,509	4	0	4	109	24,248	5	0	5	117	24,555	5	0	5	119
								T3M	23,263	4	0	4	112	25,061	4	0	4	121	25,378	4	0	4	123
								T4M	22,924	5	0	5	110	24,588	5	0	5	119	24,899	5	0	5	120
				FTFM	23,414	5	0	5	113	25,223	5	0	5	122	25,543	5	0	5	123				
				TSVS	23,579	5	0	1	114	25,401	5	0	1	123	25,722	5	0	1	124				
				TSS	23,380	4	0	2	113	25,187	4	0	2	122	25,506	4	0	2	123				
				TSM	23,374	5	0	3	113	25,181	5	0	3	122	25,499	5	0	3	123				
				TSW	23,165	5	0	4	112	24,955	5	0	4	121	25,271	5	0	4	122				
				BLC	19,231	4	0	4	93	20,717	4	0	4	100	20,979	4	0	4	101				
				LCCO	13,734	2	0	3	66	14,795	2	0	4	71	14,983	2	0	4	72				
				RCCO	13,716	4	0	4	66	14,776	4	0	4	71	14,963	4	0	4	72				
				60	1250	P13	231W	T1S	25,400	4	0	4	110	27,363	4	0	4	118	27,709	4	0	4	120
								T2S	25,254	5	0	5	109	27,205	5	0	5	118	27,550	5	0	5	119
								T2M	25,710	4	0	4	111	27,696	4	0	4	120	28,047	4	0	4	121
								T3S	24,862	5	0	5	108	26,783	5	0	5	116	27,122	5	0	5	117
								T3M	25,695	5	0	5	111	27,680	5	0	5	120	28,031	5	0	5	121
								T4M	25,210	5	0	5	109	27,158	5	0	5	118	27,502	5	0	5	119
FTFM	25,861	5	0					5	112	27,860	5	0	5	121	28,212	5	0	5	122				
TSVS	26,043	5	0					1	113	28,056	5	0	1	121	28,411	5	0	1	123				
TSS	25,824	4	0					2	112	27,819	5	0	2	120	28,172	5	0	2	122				
TSM	25,818	5	0					3	112	27,813	5	0	3	120	28,165	5	0	3	122				
TSW	25,586	5	0					4	111	27,563	5	0	4	119	27,912	5	0	4	121				
BLC	21,241	4	0					4	92	22,882	4	0	4	99	23,172	4	0	4	100				
LCCO	15,170	2	0					4	66	16,342	2	0	4	71	16,549	2	0	4	72				
RCCO	15,150	5	0					5	66	16,321	5	0	5	71	16,527	5	0	5	72				



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	<b>ALASKA ARCHITECTURAL LIGHTING</b>	<b>Project 22-28191-2</b> <b>SEARHC Juneau Vintage Park</b>  Submitted By <b>ALASKA ARCHITECTURAL LIGHTING</b>	<b>Catalog Number: DSX1 LED P5 40K T5M MVOLT SPA</b> <b>DDBXD</b>  Note:	Type <b>SD</b>
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**FEATURES & SPECIFICATIONS**

**INTENDED USE**

The sleek design of the D-Series Size 1 reflects the embedded high performance LED technology. It is ideal for many commercial and municipal applications, such as parking lots, plazas, campuses, and streetscapes.

**CONSTRUCTION**

Single-piece die-cast aluminum housing has integral heat sink fins to optimize thermal management through conductive and convective cooling. Modular design allows for ease of maintenance and future light engine upgrades. The LED drivers are mounted in direct contact with the casting to promote low operating temperature and long life. Housing is completely sealed against moisture and environmental contaminants (IP65). Low EPA (1.01 ft³) for optimized pole wind loading.

**FINISH**

Exterior parts are protected by a zinc-infused Super Durable TGIC thermoset powder coat finish that provides superior resistance to corrosion and weathering. A tightly controlled multi-stage process ensures a minimum 3 mils thickness for a finish that can withstand extreme climate changes without cracking or peeling. Available in both textured and non-textured finishes.

**OPTICS**

Precision-molded proprietary acrylic lenses are engineered for superior area lighting distribution, uniformity, and pole spacing. Light engines are available in standard 3000 K, 4000 K and 5000 K (70 CRI) configurations. The D-Series Size 1 has zero uplight and qualifies as a Nighttime Friendly™ product, meaning it is consistent with the LEED® and Green Globes™ criteria for eliminating wasteful uplight.

**ELECTRICAL**

Light engine configurations consist of high-efficacy LEDs mounted to metal-core circuit boards to maximize heat dissipation and promote long life (up to L85/100,000 hours at 25°C). Class 1 electronic drivers are designed to have a power factor >90%, THD <20%, and an expected life of 100,000 hours with <1% failure rate. Easily serviceable 10kV surge protection device meets a minimum Category C Low operation (per ANSI/IEEE C62.41.2).

**STANDARD CONTROLS**

The DSX1 LED area luminaire has a number of control options. DSX Size 1, comes standard with 0-10V dimming drivers. Dusk to dawn controls can be utilized via optional NEMA twist-lock photocell receptacles. Integrated motion sensors with on-board photocells feature field-adjustable programming and are suitable for mounting heights up to 30 feet.

**nLIGHT AIR CONTROLS**

The DSX1 LED area luminaire is also available with nLight® AIR for the ultimate in wireless control. This powerful controls platform provides out-of-the-box basic motion sensing and photocontrol functionality and is suitable for mounting heights up to 40 feet. Once commissioned using a smartphone and the easy-to-use CLAIRITY app, nLight AIR equipped luminaires can be grouped, resulting in motion sensor and photocell group response without the need for additional equipment. Scheduled dimming with motion sensor over-ride can be achieved when used with the nLight Eclipse. Additional information about nLight Air can be found here.

**INSTALLATION**

Included mounting block and integral arm facilitate quick and easy installation. Stainless steel bolts fasten the mounting block securely to poles and walls, enabling the D-Series Size 1 to withstand up to a 3.0 G vibration load rating per ANSI C136.31. The D-Series Size 1 utilizes the AERIS™ series pole drilling pattern (template #8). NEMA photocontrol receptacle are also available.

**LISTINGS**

UL listed to meet U.S. and Canadian standards. UL Listed for wet locations. Light engines are IP66 rated; luminaire is IP65 rated. Rated for -40°C minimum ambient. U.S. Patent No. D672,492 S. International patent pending.

DesignLights Consortium® (DLC) Premium qualified product and DLC qualified product. Not all versions of this product may be DLC Premium qualified or DLC qualified. Please check the DLC Qualified Products List at [www.designlights.org/](http://www.designlights.org/) QPL to confirm which versions are qualified.

International Dark-Sky Association (IDA) Fixture Seal of Approval (FSA) is available for all products on this page utilizing 3000K color temperature only.

**BUY AMERICAN**

Product with the BAA option is assembled in the USA and meets the Buy America(n) government procurement requirements under FAR, DFARS and DOT. Please refer to [www.acuitybrands.com/buy-american](http://www.acuitybrands.com/buy-american) for additional information.

**WARRANTY**


5-year limited warranty. Complete warranty terms located at: [www.acuitybrands.com/support/customer-support/terms-and-conditions](http://www.acuitybrands.com/support/customer-support/terms-and-conditions)

**Note:** Actual performance may differ as a result of end-user environment and application.

All values are design or typical values, measured under laboratory conditions at 25 °C.

Specifications subject to change without notice.



	ALASKA ARCHITECTURAL LIGHTING	Project 22-28191-2 SEARHC Juneau Vintage Park  Submitted By ALASKA ARCHITECTURAL LIGHTING	Catalog Number: SSS 30 4G DM19AS VD DBLXD  Note:	Type <b>P1</b>
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**FEATURES & SPECIFICATIONS**

**INTENDED USE** — These specifications are for USA standards only. Square Straight Steel is a general purpose light pole for up to 39-foot mounting heights. This pole provides a robust yet cost effective option for mounting area lights and floodlights.

**CONSTRUCTION** — **Pole Shaft:** The pole shaft is of uniform dimension and wall thickness and is made of a weldable-grade, hot-rolled, commercial-quality steel tubing with a minimum yield of 55 KSI (71-gauge, .1196"), or 50 KSI (7-gauge, .1793"). Shaft is one-piece with a full-length longitudinal high-frequency electric resistance weld. Uniformly square in cross-section with flat sides, small corner radii and excellent torsional qualities. Available shaft widths are 4", 5" and 6".

**Pole Top:** A flush non-metallic black top cap is provided for all poles that will receive drilling patterns for side-mount luminaire arm assemblies or when ordered with PT option.

**Handhole:** A reinforced handhole with grounding provision is provided at 18" from the base on side A. Positioning the handhole lower may not be possible and requires engineering review; consult Tech Support-Outdoor for further information. Every handhole includes a cover and cover attachment hardware. The handhole has a nominal dimension of 2.5" x 5".

**Base Cover:** A durable ABS plastic two-piece full base cover, finished to match the pole, is provided with each pole assembly. Additional base cover options are available upon request.

**Anchor Base/ Bolts:** Anchor base is fabricated from steel that meets ASTM A36 standards and can be altered to match existing foundations; consult factory for modifications. Anchor bolts are manufactured to ASTM F1554 Standards grade 55, (55 KSI minimum yield strength and tensile strength of 75-95 KSI). Top threaded portion (nominal 12") is hot-dipped galvanized per ASTM A-153.

**HARDWARE** — All structural fasteners are high-strength galvanized carbon steel. All non-structural fasteners are galvanized or zinc-plated carbon steel or stainless steel.

**FINISH** — Extra durable standard powder-coat finishes include Dark Bronze, White, Black, Medium Bronze and Natural Aluminum colors. Classic finishes include Sandstone, Charcoal Gray, Tennis Green, Bright Red and Steel Blue colors. Architectural Colors and Special Finishes are available by quote and include, but are not limited to Hot-dipped Galvanized, Paint over Hot-dipped Galvanized, RAL Colors, Custom Colors and Extended Warranty Finishes. Factory-applied primer paint finish is available for customer field-paint applications.

**WARRANTY** — 1-year limited warranty. Complete warranty terms located at: [www.acuitybrands.com/support/warranty/terms-and-conditions](http://www.acuitybrands.com/support/warranty/terms-and-conditions)

**NOTE:** Actual performance may differ as a result of end-user environment and application. Specifications subject to change without notice.

Catalog Number
Notes
Type



Anchor Base Poles

**SSS**

SQUARE STRAIGHT STEEL

See footnotes next page.

	Project 22-28191-2 SEARHC Juneau Vintage Park	Catalog Number: SSS 30 4G DM19AS VD DBLXD	Type <b>P1</b>
	Submitted By ALASKA ARCHITECTURAL LIGHTING	Note:	

## SSS Square Straight Steel Poles

**ORDERING INFORMATION** Lead times will vary depending on options selected. Consult with your sales representative. **Example: SSS 20 5C DM19 DDB**

SSS Series	Nominal fixture mounting height	Nominal shaft base size/wall thickness <sup>1</sup>	Mounting <sup>1</sup>	Options	Finish <sup>10</sup>
SSS	10'-39' (for 1/2 ft increments, add -6 to the pole height. Ex: 20'-6 equals 20ft 6in.)  See technical information table for complete ordering information.)  <div style="border: 1px solid black; padding: 2px; display: inline-block;">30'</div>	4C 4" 11g (.1196") <div style="border: 1px solid black; padding: 2px; display: inline-block;">4G 4" 7g (.1793")</div> 5C 5" 11g (.1196") 5G 5" 7g (.1793") 6G 6" 7g (.1793")  See technical information table for complete ordering information.)	<b>Tenon mounting</b> PT Open top (includes top cap) T20 2-3/8" O.D. (2" NPS) T25 2-7/8" O.D. (2-1/2" NPS) T30 3-1/2" O.D. (3" NPS) T35 4" O.D. (3-1/2" NPS)  <b>CSX/KAD/KSE/KSE/KVR/KVE Drill mounting<sup>3</sup></b> DM19 1 at 90° DM28 2 at 180° DM28 PL 2 at 180° with one side plugged DM29 2 at 90° DM39 3 at 90° DM49 4 at 90°  <b>HLA/KAX Drill mounting<sup>3</sup></b> <div style="border: 1px solid black; padding: 2px; display: inline-block;">DM19AS 1 at 90°</div> DM28AS 2 at 180° DM29AS 2 at 90° DM39AS 3 at 90° DM49AS 4 at 90°  <b>RAD drill mounting<sup>3</sup></b> DM19RAD 1 at 90° DM28RAD 2 at 180° DM29RAD 2 at 90° DM32RAD 3 at 120° DM39RAD 3 at 90° DM49RAD 4 at 90°  <b>ESX Drill mounting<sup>3</sup></b> DM19ESX 1 at 90° DM28ESX 2 at 180° DM29ESX 2 at 90° DM39ESX 3 at 90° DM49ESX 4 at 90°	<b>AERIS™ Suspend drill mounting<sup>1,4</sup></b> DM19AST_ 1 at 90° DM28AST_ 2 at 180° DM29AST_ 2 at 90° DM39AST_ 3 at 90° DM49AST_ 4 at 90°  <b>OMERO™ Suspend drill mounting<sup>1,4</sup></b> DM19MRT_ 1 at 90° DM28MRT_ 2 at 180° DM29MRT_ 2 at 90° DM39MRT_ 3 at 90° DM49MRT_ 4 at 90°  <b>Shipped installed</b> L/AB Less anchor bolts (include when anchor bolts are not needed)  <div style="border: 1px solid black; padding: 2px; display: inline-block;">VD Vibration damper</div> TP Tamper resistant handhole cover fasteners HAXy Horizontal arm bracket (1 fixture) <sup>5,6</sup> FDLxy Festoon outlet less electrical <sup>8</sup> CPL12/xy 1/2" coupling <sup>9</sup> CPL34/xy 3/4" coupling <sup>9</sup> CPL1/xy 1" coupling <sup>9</sup> NPL12/xy 1/2" threaded nipple <sup>5</sup> NPL34/xy 3/4" threaded nipple <sup>5</sup> NPL1/xy 1" threaded nipple <sup>5</sup> EHXy Extra handhole <sup>5,7</sup> MAEX Match existing <sup>4</sup> USPOM United States point of manufacture <sup>9</sup> IC Interior coating <sup>9</sup> UL UL listed with label (Includes NEC compliant cover) NEC NEC 410.30 compliant gasketed handhole (Not UL Labeled)  <b>Shipped separately (replacement kit available)</b> (blank) FBC Full base cover (plastic) (blank) TC Top cap (blank) HHC Handhole cover	<b>Standard colors</b> DDBXD Dark bronze DWHXD White <div style="border: 1px solid black; padding: 2px; display: inline-block;">DBLXD Black</div> DMBXD Medium bronze DNAXD Natural aluminum  <b>Classic colors</b> DSS Sandstone DGC Charcoal gray DTG Tennis green DBR Bright red DSB Steel blue  <b>Architectural Colors and Special Finishes<sup>11</sup></b> Galvanized, Paint over Galvanized, RAL Colors, Custom Colors and Extended Warranty Finishes available.

**NOTES:**

- Wall thickness will be signified with a "C" (11 Gauge) or a "G" (7-Gauge) in nomenclature. "C" - 0.1196" | "G" - 0.1793".
- PT open top poles include top cap. When ordering tenon mounting and drill mounting for the same pole, follow this example: DM28T20. The combination includes a required extra handhole.
- Refer to the fixture spec sheet for the correct drilling template pattern and orientation compatibility.
- Insert "1" or "2" to designate fixture size; e.g. DM19AS12.
- Specify location and orientation when ordering option.  
 For "x": Specify the height above the base of pole in feet or feet and inches; separate feet and inches with a "-".  
 Example: 5ft = 5 and 20ft 3in = 20-3  
 For "y": Specify orientation from handhole (A,B,C,D)  
 Refer to the Handhole Orientation diagram below.  
 Example: 1/2" coupling at 5'8", orientation C = CPL12/5-8C


- Horizontal arm is 18" x 2-3/8" O.D. tenon standard, with radius curve providing 12" rise and 2-3/8" O.D. If ordering two horizontal arm at the same height, specify with HAXxy. Example: HA20BD.
- Combination of tenon-top and drill mount includes extra handhole.
- Must add original order number of existing pole(s).
- Use when mill certifications are required.
- Provides enhanced corrosion resistance.
- Additional colors available; see [www.lithonia.com/archcolors](http://www.lithonia.com/archcolors) or Architectural Colors brochure (Form No. 794.3). Available by formal quote only, consult factory for details.



POLE-SSS

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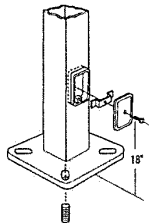
 <b>ALASKA ARCHITECTURAL LIGHTING</b>	Project 22-28191-2 <b>SEARHC Juneau Vintage Park</b>	Catalog Number: SSS 30 4G DM19AS VD DBLXD	Type <b>P1</b>
	Submitted By ALASKA ARCHITECTURAL LIGHTING	Note:	

**SSS Square Straight Steel Poles**

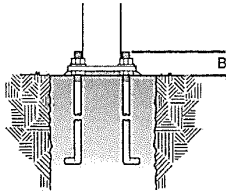
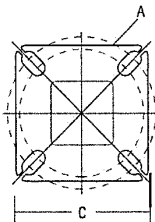
TECHNICAL INFORMATION — EPA (ft2) with 1.3 gust													
Catalog Number	Nominal Shaft Length (ft.)*	Pole Shaft Size (Base in. x Top in. x ft.)	Wall thick (in)	Gauge	EPA (ft²) with 1.3 gust						Bolt circle (in)	Bolt size (in. x in. x in.)	Approximate ship weight (lbs.)
					80 MPH	Max. weight	90 MPH	Max. weight	100 MPH	Max. weight			
SSS 10 4C	10	4.0 x 10.0	0.1196	11	30.6	765	23.8	595	18.9	473	8-9	3/4 x 18 x 3	75
SSS 12 4C	12	4.0 x 12.0	0.1196	11	24.4	610	18.8	470	14.8	370	8-9	3/4 x 18 x 3	90
SSS 14 4C	14	4.0 x 14.0	0.1196	11	19.9	498	15.1	378	11.7	293	8-9	3/4 x 18 x 3	100
SSS 16 4C	16	4.0 x 16.0	0.1196	11	15.9	398	11.8	295	8.9	223	8-9	3/4 x 18 x 3	115
SSS 18 4C	18	4.0 x 18.0	0.1196	11	12.6	315	9.2	230	6.7	168	8-9	3/4 x 18 x 3	125
SSS 20 4C	20	4.0 x 20.0	0.1196	11	9.6	240	6.7	167	4.5	150	8-9	3/4 x 18 x 3	140
SSS 20 4G	20	4.0 x 20.0	0.1793	7	14	350	11	275	8	200	8-9	3/4 x 30 x 3	198
SSS 20 5C	20	5.0 x 20.0	0.1196	11	17.7	443	12.7	343	9.4	235	10-12	1 x 36 x 4	185
SSS 20 5G	20	5.0 x 20.0	0.1793	7	28.1	703	21.4	535	16.2	405	10-12	1 x 36 x 4	265
SSS 25 4C	25	4.0 x 25.0	0.1196	11	4.8	150	2.6	100	1	50	8-9	3/4 x 18 x 3	170
SSS 25 4G	25	4.0 x 25.0	0.1793	7	10.8	270	7.7	188	5.4	135	8-9	3/4 x 30 x 3	245
SSS 25 5C	25	5.0 x 25.0	0.1196	11	9.8	245	6.3	157	3.7	150	10-12	1 x 36 x 4	225
SSS 25 5G	25	5.0 x 25.0	0.1793	7	18.5	463	13.3	333	9.5	238	10-12	1 x 36 x 4	360
SSS 30 4G	30	4.0 x 30.0	0.1793	7	6.7	168	4.4	110	2.6	65	8-9	3/4 x 30 x 3	295
SSS 30 5C	30	5.0 x 30.0	0.1196	11	4.7	150	2	50	-	-	10-12	1 x 36 x 4	265
SSS 30 5G	30	5.0 x 30.0	0.1793	7	10.7	267	6.7	167	3.9	100	10-12	1 x 36 x 4	380
SSS 30 6G	30	6.0 x 30.0	0.1793	7	19	475	13.2	330	9	225	11-13	1 x 36 x 4	520
SSS 35 5G	35	5.0 x 35.0	0.1793	7	5.9	150	2.5	100	-	-	10-12	1 x 36 x 4	440
SSS 35 6G	35	6.0 x 35.0	0.1793	7	12.4	310	7.6	190	4.2	105	11-13	1 x 36 x 4	540
SSS 39 6G	39	6.0 x 39.0	0.1793	7	7.2	180	3	75	-	-	11-13	1 x 36 x 4	605

\* EPA values are based ASCE 7-93 wind map. For 1/2 ft increments, add -6 to the pole height. Ex: 20-6 equals 20ft 6in.

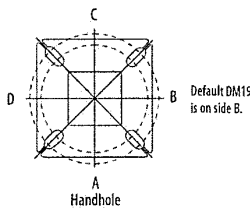
**BASE DETAIL**



POLE DATA								
Shaft base size	Bolt circle A	Bolt projection B	Base diameter C	Base plate thickness	Template description	Anchor bolt description	Anchor bolt and template number	Anchor bolt description
4"C	8" - 9"	3.25" - 3.75"	8" - 8.25"	0.75"	ABTEMPLATE PJ50004	AB18-0	ABSSS-4C	3/4"x18"x3"
4"G	8" - 9"	3.38" - 3.75"	8" - 8.25"	0.875"	ABTEMPLATE PJ50004	AB30-0	ABSSS-4G	3/4"x30"x3"
5"	10" - 12"	3.5" - 4"	11"	1"	ABTEMPLATE PJ50010	AB36-0	ABSSS-5	1"x36"x4"
6"	11" - 13"	4" - 4.50"	12.5"	1"	ABTEMPLATE PJ50011	AB36-0	N/A	1"x36"x4"




**HANDHOLE ORIENTATION**



**IMPORTANT INSTALLATION NOTES:**

- Do not erect poles without having fixtures installed.
- Factory-supplied templates must be used when setting anchor bolts. Lithonia Lighting will not accept claim for incorrect anchorage placement due to failure to use Lithonia Lighting factory templates.
- If poles are stored outside, all protective wrapping must be removed immediately upon delivery to prevent finish damage.
- Lithonia Lighting is not responsible for the foundation design.



	<p>Project 22-28191-2 SEARHC Juneau Vintage Park</p> <p>Submitted By ALASKA ARCHITECTURAL LIGHTING</p>	<p>Catalog Number: SSS 30 4G DM28AS VD DBLXD</p> <p>Note:</p>	<p>Type <b>P2</b></p>
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**FEATURES & SPECIFICATIONS**

**INTENDED USE** — These specifications are for USA standards only. Square Straight Steel is a general purpose light pole for up to 39-foot mounting heights. This pole provides a robust yet cost effective option for mounting area lights and floodlights.

**CONSTRUCTION** — Pole Shaft: The pole shaft is of uniform dimension and wall thickness and is made of a weldable-grade, hot-rolled, commercial-quality steel tubing with a minimum yield of 55 KSI (11-gauge, .1196"), or 50 KSI (7-gauge, .1793"). Shaft is one-piece with a full-length longitudinal high-frequency electric resistance weld. Uniformly square in cross-section with flat sides, small corner radii and excellent torsional qualities. Available shaft widths are 4", 5" and 6".

**Pole Top:** A flush non-metallic black top cap is provided for all poles that will receive drilling patterns for side-mount luminaire arm assemblies or when ordered with PT option.

**Handhole:** A reinforced handhole with grounding provision is provided at 18" from the base on side A. Positioning the handhole lower may not be possible and requires engineering review; consult Tech Support-Outdoor for further information. Every handhole includes a cover and cover attachment hardware. The handhole has a nominal dimension of 2.5" x 5".

**Base Cover:** A durable ABS plastic two-piece full base cover, finished to match the pole, is provided with each pole assembly. Additional base cover options are available upon request.

**Anchor Base/ Bolts:** Anchor base is fabricated from steel that meets ASTM A36 standards and can be altered to match existing foundations; consult factory for modifications. Anchor bolts are manufactured to ASTM F1554 Standards grade 55, (55 KSI minimum yield strength and tensile strength of 75-95 KSI). Top threaded portion (nominal 12") is hot-dipped galvanized per ASTM A-153.

**HARDWARE** — All structural fasteners are high-strength galvanized carbon steel. All non-structural fasteners are galvanized or zinc-plated carbon steel or stainless steel.

**FINISH** — Extra durable standard powder-coat finishes include Dark Bronze, White, Black, Medium Bronze and Natural Aluminum colors. Classic finishes include Sandstone, Charcoal Gray, Tennis Green, Bright Red and Steel Blue colors. Architectural Colors and Special Finishes are available by quote and include, but are not limited to Hot-dipped Galvanized, Paint over Hot-dipped Galvanized, RAL Colors, Custom Colors and Extended Warranty Finishes. Factory-applied primer paint finish is available for customer field-paint applications.

**WARRANTY** — 1-year limited warranty. Complete warranty terms located at: [www.acuitybrands.com/support/warranty/terms-and-conditions](http://www.acuitybrands.com/support/warranty/terms-and-conditions)

**NOTE:** Actual performance may differ as a result of end-user environment and application. Specifications subject to change without notice.

Catalog Number
Notes
Type



Anchor Base Poles

**SSS**

SQUARE STRAIGHT STEEL

See footnotes next page.

OUTDOOR

POLE-SSS

	Project 22-28191-2 <b>SEARHC Juneau Vintage Park</b>	Catalog Number: SSS 30 4G DM28AS VD DBLXD	Type <b>P2</b>
	Submitted By ALASKA ARCHITECTURAL LIGHTING	Note:	

### SSS Square Straight Steel Poles

**ORDERING INFORMATION** Lead times will vary depending on options selected. Consult with your sales representative. **Example: SSS 20 5C DM19 DDB**


SSS	Nominal fixture mounting height	Nominal shaft base size/wall thickness <sup>1</sup>	Mounting <sup>2</sup>	Options	Finish <sup>10</sup>
SSS	10'-39' (for 1/2 ft increments, add -6 to the pole height. Ex: 20-6 equals 20ft 6in.)  See technical information table for complete ordering information.)  <div style="border: 1px solid black; padding: 2px; display: inline-block;">30'</div>	4C 4" 11g (.1196") 4G 4" 7g (.1793") 5C 5" 11g (.1196") 5G 5" 7g (.1793") 6G 6" 7g (.1793")  See technical information table for complete ordering information.)	<b>Tenon mounting</b> PT Open top (includes top cap) T20 2-3/8" O.D. (2" NPS) T25 2-7/8" O.D. (2-1/2" NPS) T30 3-1/2" O.D. (3" NPS) T35 4" O.D. (3-1/2" NPS)  <b>CSX/KAX/KSE/KVE/KVR/KVE Drill mounting<sup>3</sup></b> DM19 1 at 90° DM28 2 at 180° DM28 PL 2 at 180° with one side plugged DM29 2 at 90° DM39 3 at 90° DM49 4 at 90°  <b>CSX/DSX/BSX/AERIS™/OMERO™/HLA/KAX Drill mounting<sup>3</sup></b> DM19AS 1 at 90° <div style="border: 1px solid black; padding: 2px; display: inline-block;">DM28AS 2 at 180°</div> DM29AS 2 at 90° DM39AS 3 at 90° DM49AS 4 at 90°  <b>RAD drill mounting<sup>3</sup></b> DM19RAD 1 at 90° DM28RAD 2 at 180° DM29RAD 2 at 90° DM32RAD 3 at 120° DM39RAD 3 at 90° DM49RAD 4 at 90°  <b>ESX Drill mounting<sup>3</sup></b> DM19ESX 1 at 90° DM28ESX 2 at 180° DM29ESX 2 at 90° DM39ESX 3 at 90° DM49ESX 4 at 90°	<b>AERIS™ Suspend drill mounting<sup>3,4</sup></b> DM19AST_ 1 at 90° DM28AST_ 2 at 180° DM29AST_ 2 at 90° DM39AST_ 3 at 90° DM49AST_ 4 at 90°  <b>OMERO™ Suspend drill mounting<sup>3,4</sup></b> DM19MRT_ 1 at 90° DM28MRT_ 2 at 180° DM29MRT_ 2 at 90° DM39MRT_ 3 at 90° DM49MRT_ 4 at 90°  <b>Shipped installed</b> L/AB Less anchor bolts (include when anchor bolts are not needed)  <div style="border: 1px solid black; padding: 2px; display: inline-block;">VD Vibration damper</div> TP Tamper resistant handhole cover fasteners HAXy Horizontal arm bracket (1 fixture) <sup>5,6</sup> FDLxy Festoon outlet less electrical <sup>5</sup> CPL12/xy 1/2" coupling <sup>9</sup> CPL34/xy 3/4" coupling <sup>9</sup> CPL1/xy 1" coupling <sup>9</sup> NPL12/xy 1/2" threaded nipple <sup>5</sup> NPL34/xy 3/4" threaded nipple <sup>5</sup> NPL1/xy 1" threaded nipple <sup>5</sup> EHHxy Extra handhole <sup>5,7</sup> MAEX Match existing <sup>8</sup> USPOM United States point of manufacture <sup>9</sup> IC Interior coating <sup>9</sup> UL UL listed with label (Includes NEC compliant cover) NEC NEC 410.30 compliant gasketed handhole (Not UL Labeled)  <b>Shipped separately (replacement kit available)</b> (blank) FBC Full base cover (plastic) (blank) TC Top cap (blank) HHC Handhole cover	<b>Standard colors</b> DDBXD Dark bronze DWHXD White <div style="border: 1px solid black; padding: 2px; display: inline-block;">DBLXD Black</div> DMBXD Medium bronze DNAXD Natural aluminum  <b>Classic colors</b> DSS Sandstone DGC Charcoal gray DTG Tennis green DBR Bright red DSB Steel blue  <b>Architectural Colors and Special Finishes<sup>11</sup></b> Galvanized, Paint over Galvanized, RAL Colors, Custom Colors and Extended Warranty Finishes available.

**NOTES:**

- Wall thickness will be signified with a "C" (11 Gauge) or a "G" (7-Gauge) in nomenclature. "C" - 0.1196" | "G" - 0.1793".
- PT open top poles include top cap. When ordering tenon mounting and drill mounting for the same pole, follow this example: DM28/T20. The combination includes a required extra handhole.
- Refer to the fixture spec sheet for the correct drilling template pattern and orientation compatibility.
- Insert "1" or "2" to designate fixture size; e.g. DM19AST2.
- Specify location and orientation when ordering option.  
 For "x": Specify the height above the base of pole in feet or feet and inches; separate feet and inches with a "-".  
 Example: 5ft = 5 and 20ft 3in = 20-3  
 For "y": Specify orientation from handhole (A,B,C,D)  
 Refer to the Handhole Orientation diagram below.  
 Example: 1/2" coupling at 5'8"; orientation C = CPL12/5-8C
- Horizontal arm is 18" x 2-3/8" O.D. tenon standard, with radius curve providing 12" rise and 2-3/8" O.D. If ordering two horizontal arm at the same height, specify with HAXxy. Example: HA20BD.
- Combination of tenon-top and drill mount includes extra handhole.
- Must add original order number of existing pole(s).
- Use when mill certifications are required.
- Provides enhanced corrosion resistance.
- Additional colors available; see [www.lithonia.com/archcolors](http://www.lithonia.com/archcolors) or Architectural Colors brochure (Form No. 794-3). Available by formal quote only, consult factory for details.



POLE-SSS

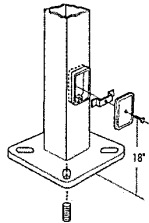
	ALASKA ARCHITECTURAL LIGHTING Project 22-28191-2 SEARHC Juneau Vintage Park	Catalog Number: SSS 30 4G DM28AS VD DBLXD	Type <b>P2</b>
	Submitted By ALASKA ARCHITECTURAL LIGHTING	Note:	

### SSS Square Straight Steel Poles

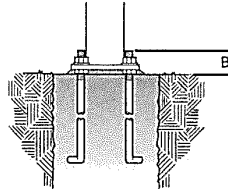
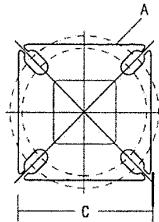
TECHNICAL INFORMATION — EPA (ft <sup>2</sup> ) with 1.3 gust													
Catalog Number	Nominal Shaft Length (ft.)*	Pole Shaft Size (Base in. x Top in. x ft.)	Wall thick (in)	Gauge	EPA (ft <sup>2</sup> ) with 1.3 gust						Bolt circle (in)	Bolt size (in. x in. x in.)	Approximate ship weight (lbs.)
					80 MPH	Max. weight	90 MPH	Max. weight	100 MPH	Max. weight			
SSS 10 4C	10	4.0 x 10.0	0.1196	11	30.6	765	23.8	595	18.9	473	8-9	3/4 x 18 x 3	75
SSS 12 4C	12	4.0 x 12.0	0.1196	11	24.4	610	18.8	470	14.8	370	8-9	3/4 x 18 x 3	90
SSS 14 4C	14	4.0 x 14.0	0.1196	11	19.9	498	15.1	378	11.7	293	8-9	3/4 x 18 x 3	100
SSS 16 4C	16	4.0 x 16.0	0.1196	11	15.9	398	11.8	295	8.9	223	8-9	3/4 x 18 x 3	115
SSS 18 4C	18	4.0 x 18.0	0.1196	11	12.6	315	9.2	230	6.7	168	8-9	3/4 x 18 x 3	125
SSS 20 4C	20	4.0 x 20.0	0.1196	11	9.6	240	6.7	167	4.5	150	8-9	3/4 x 18 x 3	140
SSS 20 4G	20	4.0 x 20.0	0.1793	7	14	350	11	275	8	200	8-9	3/4 x 30 x 3	198
SSS 20 5C	20	5.0 x 20.0	0.1196	11	17.7	443	12.7	343	9.4	235	10-12	1 x 36 x 4	185
SSS 20 5G	20	5.0 x 20.0	0.1793	7	28.1	703	21.4	535	16.2	405	10-12	1 x 36 x 4	265
SSS 25 4C	25	4.0 x 25.0	0.1196	11	4.8	150	2.6	100	1	50	8-9	3/4 x 18 x 3	170
SSS 25 4G	25	4.0 x 25.0	0.1793	7	10.8	270	7.7	188	5.4	135	8-9	3/4 x 30 x 3	245
SSS 25 5C	25	5.0 x 25.0	0.1196	11	9.8	245	6.3	157	3.7	150	10-12	1 x 36 x 4	225
SSS 25 5G	25	5.0 x 25.0	0.1793	7	18.5	463	13.3	333	9.5	238	10-12	1 x 36 x 4	360
SSS 30 4G	30	4.0 x 30.0	0.1793	7	6.7	168	4.4	110	2.6	65	8-9	3/4 x 30 x 3	295
SSS 30 5C	30	5.0 x 30.0	0.1196	11	4.7	150	2	50	--	--	10-12	1 x 36 x 4	265
SSS 30 5G	30	5.0 x 30.0	0.1793	7	10.7	267	6.7	167	3.9	100	10-12	1 x 36 x 4	380
SSS 30 6G	30	6.0 x 30.0	0.1793	7	19	475	13.2	330	9	225	11-13	1 x 36 x 4	520
SSS 35 5G	35	5.0 x 35.0	0.1793	7	5.9	150	2.5	100	--	--	10-12	1 x 36 x 4	440
SSS 35 6G	35	6.0 x 35.0	0.1793	7	12.4	310	7.6	190	4.2	105	11-13	1 x 36 x 4	540
SSS 39 6G	39	6.0 x 39.0	0.1793	7	7.2	180	3	75	--	--	11-13	1 x 36 x 4	605

\* EPA values are based ASCE 7-93 wind map. For 1/2 ft increments, add -6 to the pole height. Ex: 20-6 equals 20ft 6in.

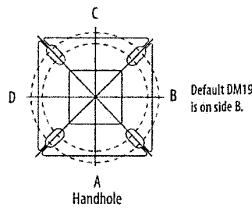
### BASE DETAIL



POLE DATA								
Shaft base size	Bolt circle A	Bolt projection B	Base diameter C	Base plate thickness	Template description	Anchor bolt description	Anchor bolt and template number	Anchor bolt description
4"C	8" - 9"	3.25" - 3.75"	8" - 8.25"	0.75"	ABTEMPLATE PJ50004	AB18-0	ABSSS-4C	3/4"x18"x3"
4"G	8" - 9"	3.38" - 3.75"	8" - 8.25"	0.875"	ABTEMPLATE PJ50004	AB30-0	ABSSS-4G	3/4"x30"x3"
5"	10" - 12"	3.5" - 4"	11"	1"	ABTEMPLATE PJ50010	AB36-0	ABSSS-5	1"x36"x4"
6"	11" - 13"	4" - 4.50"	12.5"	1"	ABTEMPLATE PJ50011	AB36-0	N/A	1"x36"x4"




### HANDHOLE ORIENTATION



### IMPORTANT INSTALLATION NOTES:

- Do not erect poles without having fixtures installed.
- Factory-supplied templates must be used when setting anchor bolts. Lithonia Lighting will not accept claim for incorrect anchorage placement due to failure to use Lithonia Lighting factory templates.
- If poles are stored outside, all protective wrapping must be removed immediately upon delivery to prevent finish damage.
- Lithonia Lighting is not responsible for the foundation design.

# Core and Shell

	ALASKA ARCHITECTURAL LIGHTING	Project 22-28191-2 SEARHC Juneau Vintage Park  Submitted By ALASKA ARCHITECTURAL LIGHTING	Catalog Number: ZL1N L48 5000LM FST MVOLT 40K 80CRI WH  Note:	Type <b>A</b>
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**FEATURES & SPECIFICATIONS**

**INTENDED USE** — Built on the compact, low-profile Z strip channel, this LED strip offers long maintenance-free life, several color temperatures, lumen outputs and lengths. Ideal for new construction and retrofit applications in T5 and T8 lengths. Ideal for uplight and downlight in commercial, retail, manufacturing, warehouse, cove and display applications. **Certain airborne contaminants can diminish the integrity of acrylic and/or polycarbonate. Click here for Acrylic/Polycarbonate Compatibility table for suitable uses.**

**CONSTRUCTION** — Compact-design channel and cover are formed from code-gauge cold-rolled steel. Easy to install row aligner included for continuous row mounting.

**Finish:** Paint options include high-gloss, baked white enamel (WH), galvanized (GALV), matte black (MB) and smoke gray (SKGY). After fabrication, five-stage iron phosphate pre-treatment ensures superior paint adhesion and rust resistance.

**OPTICS** — Standard diffuse snap on/snap off lens eliminates pixels, improves uniformity and minimizes glare. L/LENS option available.

**ELECTRICAL** — L70>60,000hours. Utilizes high-output LEDs integrated on a two-layer circuit board, ensuring cool-running operation. Optional internal pluggable wiring harness for reduced labor cost in row mounting applications. (See PLR\_ordering information on page 2.) Electronic LED driver is rated for 75 input watts maximum (see Operational Data on page 4 for actual wattage consumption), **multi-volt input and 0-10V dimming standard.** This fixture is designed to withstand a maximum line surge of 2.5kV at 0.75kA combination wave for indoor locations, for applications requiring higher level of protection additional surge protection must be provided.

LEDs provide nominal 80 CRI at 3000 K, 3500 K, 4000 K, or 5000 K.

Lumen output up to 2,000 lumens per foot. In 86°F (30°C) ambient environments. Luminaire should be installed in applications where ambient temperatures do not exceed 86°F (30°C).

**INSTALLATION** — Tool-less channel cover for easy installation.

Fixture may be surface mounted (with or without ZSPRG hanger), pendant or stem mounted with appropriate mounting options. Three-point aligner locks in place for easy continuous row mounting.

**LISTINGS** — CSA certified to US and Canadian safety standards. For use in damp locations between -40°F (-40°C) and 86°F (30°C).

DesignLights Consortium® (DLC) Premium qualified product and DLC qualified product. Not all versions of this product may be DLC Premium qualified or DLC qualified. Please check the DLC Qualified Products List at [www.designlights.org](http://www.designlights.org) to confirm which versions are qualified.

**BUY AMERICAN** — Product with the BAA option is assembled in the USA and meets the Buy America(n) government procurement requirements under FAR, DFARS and DOT. Please refer to [www.acuitybrands.com/buy-american](http://www.acuitybrands.com/buy-american) for additional information.

**WARRANTY** — 5-year limited warranty. Complete warranty terms located at: [www.acuitybrands.com/support/warranty/terms-and-conditions](http://www.acuitybrands.com/support/warranty/terms-and-conditions)

**Note:** Actual performance may differ as a result of end-user environment and application.

All values are design or typical values, measured under laboratory conditions at 25 °C.

Specifications subject to change without notice.

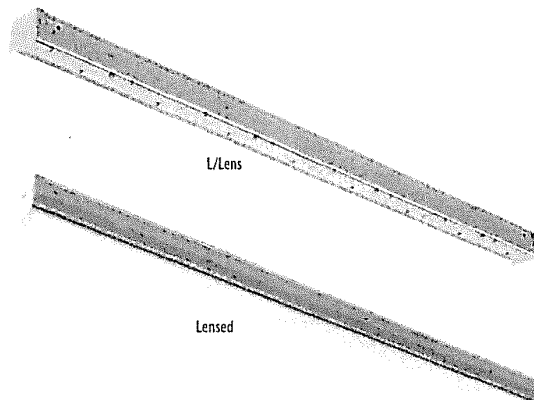
Catalog Number
Notes
Type



LED Striplight

**ZL1N**

24", 48" and 96" Lengths



**A+ Capable Luminaire**

This item is an A+ capable luminaire, which has been designed and tested to provide consistent color appearance and out-of-the-box control compatibility with simple commissioning.

- All configurations of this luminaire meet the Acuity Brands' specification for chromatic consistency
- This luminaire is part of an A+ Certified solution for nLight® control networks marked by a shaded background\*

To learn more about A+, visit [www.acuitybrands.com/aplus](http://www.acuitybrands.com/aplus).

\*See ordering tree for details



	Project 22-28191-2 SEARHC Juneau Vintage Park	Catalog Number: ZL1N L48 5000LM FST MVOLT 40K 80CRI WH	Type <b>A</b>
	Submitted By ALASKA ARCHITECTURAL LIGHTING	Note:	

## ZL1N LED Striplight

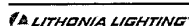


**ORDERING INFORMATION** Lead times will vary depending on options selected. Consult with your sales representative. **Example:** ZL1N L48 3000LM FST MVOLT 40K 80CRI WH


Series	Length	Reflectors‡	Nominal lumens‡	Diffuser	Voltage
ZL1N LED striplight	L24 24" ‡	(blank) Less reflector	1500LM 1,500 lumens	FST Snap on frosted, diffuse	MVOLT 120-277V
		SMR Symmetric	2500LM 2,500 lumens	L/LENS No diffuser	120 120V
			3500LM 3,500 lumens	SBL FST Straight blade louver with snap on frosted, diffuse	208 208V
	L46 46"	(blank) Less reflector	3000LM 3,000 lumens		240 240V
	L48 48"	ASR Asymmetric (L48 only)	5000LM 5,000 lumens		277 277V
TZL1N LED striplight ‡	L92 92"	SMR Symmetric	7000LM 7,000 lumens		347 347V ‡
		(blank) Less reflector	6000LM 6,000 lumens		480 480V ‡
	L96 96"	SMR Symmetric	10000LM 10,000 lumens		
		(blank) Less reflector	14000LM 14,000 lumens		

Color temperature	Color rendering index	Options	Individual Controls‡	Paint finish
30K 3000 K	80CRI 80 CRI	PLR_ Plug-in wiring‡	LBOZU 360° low mount LSXR PIR sensor, (7-15' mounting heights), ON/OFF occupancy, pre-wired (LINK) ‡	WH White
.35K 3500 K	90CRI 90 CRI	PLR1LVG Plug-in wiring-low voltage‡	LBOHOSZU 360° low mount LSXR PIR sensor, (7-15' mounting heights), high/low occupancy dimming, pre-wired (LINK) ‡	GALV Galvanized
40K 4000 K		E7W Power Sentry® PS750L, 7 watt emergency battery, CA Title 20 non-compliant (LINK) ‡	LBPZU 360° low mount LSXR PIR sensor, (7-15' mounting heights), ON/OFF photocell, pre-wired (LINK) ‡	MB Matte black
50K 5000 K		ZE7W Two Power Sentry® PS750L, 7 watt emergency batteries, CA Title 20 non-compliant (LINK) ‡	LBMOSZU 360° low mount LSXR PIR sensor, (7-15' mounting heights), dimming and switching photocell, pre-wired (LINK) ‡	SKGY Smoke gray
		E10WLC Power Sentry® PS1055LCP, 10 watt emergency battery, Certified in CA Title 20 MAEDBS (LINK) ‡	<b>Cord sets:‡</b>	
		ZE10WLC Two Power Sentry® PS1055LCP, 10 watt emergency batteries, Certified in CA Title 20 MAEDBS (LINK) ‡	CS1W Cord with NEMA 5-15P, 120V straight blade plug, 18 gauge, 3 conductors, white, 6ft	
		E15WLC Power Sentry® PS1555LCP, 15 watt emergency battery, Certified in CA Title 20 MAEDBS (LINK) ‡	CS3W Cord with NEMA 5-15P, 120V twist lock plug, 18 gauge, 3 conductor, white, 6ft	
		OUTEND Cord set to exit endplate of fixture	CS7W Cord with NEMA 7-15P, 277V straight blade plug, 18 gauge, 3 conductors, white, 6ft	
		BAA Buy America(n) Act Compliant	CS11W Cord with NEMA L7-15P, 277V twist lock plug, 18 gauge, 3 conductors, white, 6ft	
		<b>nLight® Wireless ‡</b>	CS25W Cord with NEMA L24-20P, 347V twist lock plug, 18 gauge, 3 conductors, white, 6ft	
		NLTAIR2 nLight® Air Generation 2 enabled, 360° low mount sensor, (7 to 15' heights) (LINK) ‡	CS97W Cord with NEMA L8-20P, 480V twist lock plug, 18 gauge, 3 conductors, white, 6ft	
		RLSXR10 nLight® Air Generation 2 enabled, 360° low mount sensor, (7 to 15' heights), UL 924 Emergency Operation, via power interrupt detection (not available with battery pack) (LINK) ‡	CS93W Cord only (no plug), 18 gauge, 3 conductors, white, 6ft	

Accessories: Order as separate catalog number.			
HC36 M12	Hanger chain, 36" (1 pair)	ZLR L24 SYM UPL WH	24" symmetric reflector with uplight, white finish
ZACVH M100	Adjustable 10' aircraft cable with Y hanger (1 pair)	ZLR L24 SYM WH	24" symmetric reflector, white finish
ZLANGBKT	Luma-tilt™ angle bracket for shelf or ledge mounting only	ZLR L46 SYM UPL WH	46" symmetric reflector with uplight, white finish
SQ_	Swivel stem hanger (specify length in 2" increments up to 48")	ZLR L46 SYM WH	46" symmetric reflector, white finish
NPP16D	nLight® wired power/relay pack, 0-10VDC dimming output (LINK)	ZLR L48 ASY WH	48" asymmetric reflector, white finish
RPP20D	nLight® air Generation 2 enabled, power/relay pack, 0-10V dimming output (LINK)	ZLR L48 SYM UPL WH	48" symmetric reflector with uplight, white finish
LSXR	Sensor Switch® LSXR occupancy sensor (LINK)	ZLR L48 SYM WH	48" symmetric reflector, white finish
ZSPRG J2	Tong and T-grid hanger, for 15/16" T-grid (Order quantity required in multiples of 2)	ZLR L92 SYM UPL WH	92" symmetric reflector with uplight, white finish
WGZ24	24" wireguard, white ‡	ZLR L92 SYM WH	92" symmetric reflector, white finish
WGZ48	48" wireguard, white ‡	ZLR L96 SYM UPL WH	96" symmetric reflector with uplight, white finish
		ZLR L96 SYM WH	96" symmetric reflector, white finish
		UNIVERSAL REFL ALIGNER	Universal reflector aligners, quantity 1



ZL1N

	ALASKA ARCHITECTURAL LIGHTING Project 22-28191-2 SEARHC Juneau Vintage Park	Catalog Number: ZL1N L48 5000LM FST MVOLT 40K 80CRI WH Type <b>A</b>
	Submitted By ALASKA ARCHITECTURAL LIGHTING	Note:

## ZL1N LED Striplight

‡ Option Value Ordering Restrictions	
Option value	Restriction
2E7W	MVOLT required. Available with L92 or L96 only. Not available with L24, L46, L48, L92 6000LM, L96 6000LM or any cordset that includes a plug.
2E10WLCP	MVOLT required. Available with L92 or L96 with 10,000LM or 14000LM only. Not available with L24, L46, L48, L92 6000LM, L96 6000LM or any cordset that includes a plug.
347, 480	Utilizes step down transformer.
Cord Sets	Must specify voltage when plug is included. Cordsets exit back of fixture unless OUTEND option is specified.
E7W	MVOLT required. Available with L46, L48, L92 or L96 with 10000LM or 14000LM only. Not available with L24 or any cordset that includes a plug.
E10WLCP	MVOLT required. Available with L46, L48, L92 or L96 only. Not available with L24 or any cordset that includes a plug.
E15WLCP	MVOLT required. Available with L92 or L96 only. Not available with L24, L46, L48 or any cordset that includes a plug.
HTZL1N	Tandem fixture ships as two L46 or L48 fixtures.
Individual Controls	Available with MVOLT, 347 or 480 only. See ordering information on page 6 for more configurations. This sensor configuration is suitable for minimum ambient temperature of 14°F (-10°C). See page 5 for low temperature option providing -4°F (-20°C) minimum ambient. When choosing Sensor and PLR for same fixture, consult the factory. Sensors come prewired, they must be snapped into place at time of installation.
L24	Not available with 347V, 480V or emergency batteries.
nLight® Wireless	See LINK in sensor description to RLSXR specification sheet for more configurations. When choosing a sensor and PLR for same fixture, consult the factory. Sensors come prewired, they must be snapped into place at time of installation.
NLTAIR2 RLSXR10EM	Not available with 347 or 480. MVOLT required.
Nominal Lumens	See Operational Data on page 2 for actual lumens.
PLR	Not available with cordset options. See ordering information on page 5. When choosing a sensor and PLR for the same fixture, consult the factory.
PLR1LVG	Not available with cordset options.
Reflectors	Optional. Reflectors ship separately.
WGHZ24	Not available with ASR or SMR reflector options.
WGHZ48	Not available with ASR or SMR reflector options. Order a Qty of 2 for L92 or L96 tandem fixtures.

	Project 22-28191-2 SEARHC Juneau Vintage Park	Catalog Number: ZL1N L48 5000LM FST MVOLT 40K 80CRI WH	Type <b>A</b>
	Submitted By ALASKA ARCHITECTURAL LIGHTING	Note:	

### ZL1N LED Striplight

OPERATIONAL DATA												
	Nominal lumen package	Length (inches)	Delivered Lumens 3000 K CCT @ 77°F (25°C) ambient temperature		Delivered Lumens 3500 K CCT @ 77°F (25°C) ambient temperature		Delivered Lumens 4000 K CCT @ 77°F (25°C) ambient temperature		Delivered Lumens 5000 K CCT @ 77°F (25°C) ambient temperature		Wattage @ 120V/277V	Comparable Light Source
			80 CRI	90 CRI	80 CRI	90 CRI	80 CRI	90 CRI	80 CRI	90 CRI		
Lensed	1500LM	24	1738	1409	1777	1467	1804	1494	1871	1528	15	1-lamp 17W T8
	2500LM	24	2265	1846	2315	1900	2351	1947	2438	1991	19	1-lamp 17W T8
	3500LM	24	3586	2924	3666	3026	3723	3084	3860	3152	31	1-lamp 32W T8, 1-lamp 54W TSHO, 50W HID
	3000LM	46 or 48	3172	2586	3243	2677	3293	2728	3415	2788	25	1-lamp 32W T8, 1-lamp 54W TSHO, 50W HID
	5000LM	46 or 48	4417	3601	4515	3727	4585	3798	4754	3882	34	2-lamp 32W T8, 1-lamp 54W TSHO, 70W HID
	7000LM	46 or 48	6535	5328	6681	5515	6785	5619	7035	5744	52	3-lamp 32W T8, 2-lamp 54W TSHO, 100W HID
	6000LM	92 or 96	6561	5349	6708	5537	6812	5642	7063	5767	48	3-lamp 32W T8, 2-lamp 54W TSHO, 100W HID
	10000LM	92 or 96	8687	7082	8881	7331	9019	7470	9351	7636	68	4-lamp 32W T8, 2-lamp 54W TSHO, 100W HID
	14000LM	92 or 96	12457	10513	12735	10665	12933	10711	13409	10949	104	4-lamp 32W T8, 3-lamp 54W TSHO, 150W HID
	Unlensed	1500LM	24	1881	1534	1923	1588	1953	1618	2025	1654	15
2500LM		24	2452	1999	2506	2069	2545	2108	2639	2155	19	1-lamp 17W T8
3500LM		24	3882	3165	3969	3276	4031	3338	4179	3412	31	1-lamp 32W T8, 1-lamp 54W TSHO, 50W HID
3000LM		46 or 48	3434	2800	3511	2898	3565	2953	3697	3019	25	1-lamp 32W T8, 1-lamp 54W TSHO, 50W HID
5000LM		46 or 48	4781	3898	4888	4035	4964	4111	5147	4203	34	2-lamp 32W T8, 1-lamp 54W TSHO, 70W HID
7000LM		46 or 48	7075	5768	7233	5971	7345	6083	7616	6219	52	3-lamp 32W T8, 2-lamp 54W TSHO, 100W HID
6000LM		92 or 96	7103	5791	7261	5995	7374	6108	7646	6243	48	3-lamp 32W T8, 2-lamp 54W TSHO, 100W HID
10000LM		92 or 96	9404	7667	9614	7937	9764	8087	10123	8266	68	4-lamp 32W T8, 2-lamp 54W TSHO, 100W HID
14000LM		92 or 96	13485	10994	13786	11381	14001	11596	14516	11853	104	4-lamp 32W T8, 3-lamp 54W TSHO, 150W HID

### PROJECTED LUMEN MAINTENANCE

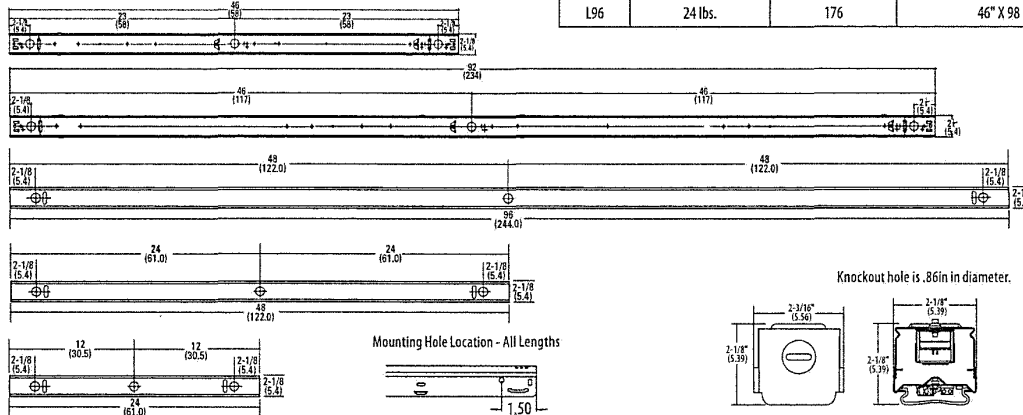
Operating Hours	0	15,000	30,000	45,000	60,000	100,000
Lumen Maintenance Factor	1	0.94	0.89	0.83	0.79	0.67

### DIMENSIONS

All dimensions are shown in inches (centimeters) unless otherwise noted. Specifications subject to change without notice.

#### PALLET DIMENSIONS

Length	Approximate weight	Fixtures per pallet	Approximate pallet dimensions (L x W x H)
L24	7 lbs.	408	46" X 51" X 32 1/16"
L46	11 lbs.	176	46" X 51" X 32 1/16"
L48	12 lbs.	176	46" X 51" X 31 3/8"
L92	22 lbs.	176	46" X 98 1/2" X 31 1/16"
L96	24 lbs.	176	46" X 98 1/2" X 31 1/16"




### PHOTOMETRICS

Please see [www.lithonia.com](http://www.lithonia.com)

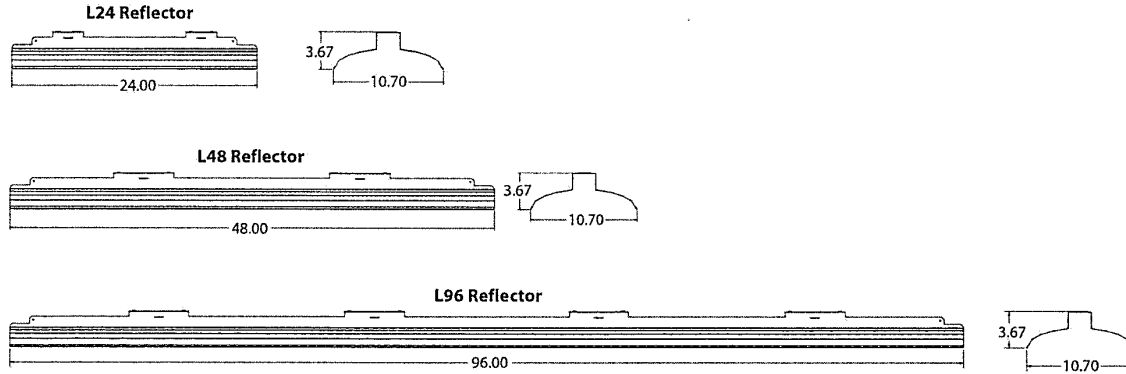


ZL1N

	ALASKA ARCHITECTURAL LIGHTING Project 22-28191-2 SEARHC Juneau Vintage Park	Catalog Number: ZL1N L48 5000LM FST MVOLT 40K 80CRI WH	Type <b>A</b>
	Submitted By ALASKA ARCHITECTURAL LIGHTING	Note:	

## ZL1N LED Striplight

### REFLECTORS (Optional)



### PHOTOMETRICS

Please see [www.lithonia.com](http://www.lithonia.com)

### PLUG-IN WIRING INFORMATION

Advanced plug-in system with two-circuit capability. Available on industrial and strip products and a variety of architectural products mounted in continuous. PLR22 (2-circuit) and crossover harness switches hot circuit serving next fixture in row. Reduces fixture types on job for alternating circuit applications (see example below).

Easy one-step installation, saves up to 35% on labor costs. Expanded switching flexibility helps save energy.

Rows can be 50% longer with two-circuit systems. Polarized, lock-together nylon connectors prevent miswiring in the field. #12 THHN conductor, rated 600V, 90°C. White neutral wire included. Grounding accomplished by fixture in-row connectors.

CSA certified systems available with up to 2 circuits. G ground required.

Not for use with dedicated emergency circuits.

Note: Specifications subject to change without notice.

Wiring



**PLR**


Advanced 1 or 2-Circuit Plug-In

**ORDERING INFORMATION** Lead times will vary depending on options selected. Consult with your sales representative.

Series	Number of hot wires	Branch circuits	Dimming	Ground
PLR	(blank) Not required for PLR22	Circuits to which driver is connected	LV Low-voltage dimming	G Ground (required)
PLR22	1 Black	(blank) Not required for PLR22 or PLR1		
	2 Black and red	A Black wire		
		B Red wire		
		Battery charging circuit (must be unswitched)		
		(blank) No battery charging circuit		
		ELA Battery pack wired to black wire		
		ELB Battery pack wired to red wire		

#### Typical Applications

- Multiple-circuit and single-circuit for longer continuous rows
- Multiple-circuit with alternating fixtures on separate circuits, 2-circuit (PLR22)
- Multiple circuit with night-lights located along row as desired

 <b>ALASKA ARCHITECTURAL LIGHTING</b>	Project 22-28191-2 <b>SEARHC Juneau Vintage Park</b>	Catalog Number: ZL1N L48 5000LM FST MVOLT 40K 80CRI WH	Type <b>A</b>
	Submitted By <b>ALASKA ARCHITECTURAL LIGHTING</b>	Note:	

## ZL1N LED Striplight

**LSXR** — Fixture Mount Occupancy Sensor (see [www.AcuityControls.com](http://www.AcuityControls.com) for additional information)

- Three interchangeable lens options to satisfy multiple mounting heights and coverage pattern requirements.
- Integrated mounting bracket drops lens down 3" from chase nipple.
- Single or dual relay versions — designed with robust protection from the harsh switching requirements of TS and LED loads.
- Photocell and 0-10VDC dimming options.
- No PIR field calibration or sensitivity adjustments required.
- Sensor ambient temperature rating of 14°F (-10°C) to 131°F (55°C).

LSXR configuration	Comparable CMRB sensor	Old style sensor nomenclature
<b>For shortest lead times use one of the following LSXR configurations</b>		
LCOZU	CMRB 50	MSI
LCHOSZU	CMRB 50 D	MSID
LCPZU	CMRB 50 P	MSIPED
LAOZU	CMRB 6	MSI360
LAHOSZU	CMRB 6 D	MSI360D
LAPZU	CMRB 6 P	MSI360PED

**SELECTIONS BELOW WILL EXTEND ORDER LEAD TIME. CONSULT YOUR SALES REPRESENTATIVE FOR DETAILS.**

### SINGLE RELAY

#### ORDERING INFORMATION

Example: LAHOSZU

Series	Lens option	Dimming/Photocell	Max. dim level	Min. dim level	Temp/Humidity	Default occupancy time delay
L LSXR passive infrared indoor occupancy sensor	A High mount, 360°	O None <sup>1</sup>	0 10VDC	5 Minimum dim level of ballast	Z None	I 30 sec
	B Low mount, 360°	H High/low occupancy operation	9 9VDC	1 1VDC	T Low temperature <sup>2</sup>	D 2.5 min
	C High mount aisleway	P Switching photocell (on/off)	8 8VDC	2 2VDC		X 5.0 min
		M Dimming and switching photocell	7 7VDC	3 3VDC		R 7.5 min
		G Dimming and switching photocell with high/low occupancy operation	4 4VDC	4 4VDC		U 10.0 min (with minimum 15 minute on time)
			5 5VDC	5 5VDC		V 15.0 min
6 6VDC	6 6VDC	6 6VDC	W 20.0 min			
						Y 30.0 min

**Notes**

- 1 Max and min dim levels not applicable with this option.
- 2 Ambient temperature rating of -4°F (-20°C) to 131°F (55°C).

### DUAL RELAY (Available with 120, 277, and 347V only)

#### ORDERING INFORMATION

Example: LA2KZU

Series	Lens option	Poles	Operating mode	Temp/Humidity	Default occupancy time delay
L LSXR passive infrared indoor occupancy sensor	A High mount, 360°	2 Dual relay	J None	Z None	I 30 sec
	B Low mount, 360°		K Alternating off relays (promotes even lamp wear)	T Low temperature <sup>1</sup>	D 2.5 min
	C High mount aisleway		O Alternating off relays w/photocell		X 5.0 min
P Switching photocell(on/off)		R 7.5 min			
	E Photocell on/off (pole 1 only)	U 10.0 min (with minimum 15 minute on time)			
	F Photocell on/off - both poles (dual set-point)	V 15.0 min			
		W 20.0 min			
		Y 30.0 min			


**Notes**

- 1 Ambient temperature rating of -4°F (-20°C) to 131°F (55°C).

Example: LENS 50 J100

Replacement lenses: Order as separate catalog number.		
Series	Lens type	Package quantity
LENS	6 High mount 360°	[blank] Single Lens
	10 Low mount 360°	J10 10-pack
	50 High mount aisleway	J100 100-pack

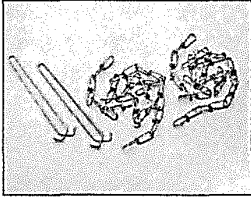


	ALASKA ARCHITECTURAL LIGHTING	Project 22-28191-2 SEARHC Juneau Vintage Park  Submitted By ALASKA ARCHITECTURAL LIGHTING	Catalog Number: ZL1N L48 5000LM FST MVOLT 40K 80CRI WH  Note:	Type <b>A</b>
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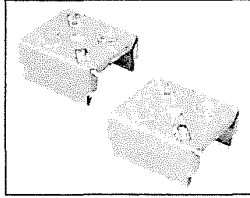
## ZL1N LED Striplight

### OPTIONS AND ACCESSORIES

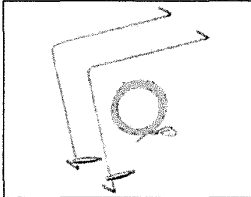
The Z Series fixture offers numerous options for almost every electrical and optical component, including a long list of field-installable accessories.



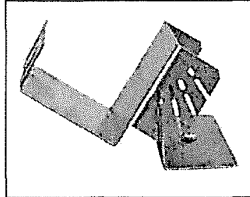
**HANGER CHAIN**  
 36" chain with Y hanger.  
  
 Order as:  
 HC36



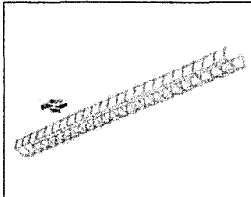
**Z SPRING HANGER**  
 Snap 'n' lock design requires no fasteners and can be used on T-grid ceiling or universal mounting systems.  
  
 Order as:  
 ZSPRG J2




**ZACVH HANGER**  
 10' Aircraft cable with Y hanger.  
  
 Order as:  
 ZACVH



**ANGLE MOUNTING BRACKET**  
 Luma-tilt™ angle bracket ships as a pair  
  
 Order as:  
 ZLANGBKT



**WIRE GUARD**  
  
 Order as:  
 WGZ24  
 WGZ48

 <b>ALASKA ARCHITECTURAL LIGHTING</b>	Project 22-28191-2 <b>SEARHC Juneau Vintage Park</b>	Catalog Number: ZL1N L48 5000LM FST MVOLT 40K 80CRI E10WLCP WH	Type <b>AE</b>
	Submitted By <b>ALASKA ARCHITECTURAL LIGHTING</b>	Note:	



**FEATURES & SPECIFICATIONS**

**INTENDED USE** — Built on the compact, low-profile Z strip channel, this LED strip offers long maintenance-free life, several color temperatures, lumen outputs and lengths. Ideal for new construction and retrofit applications in T5 and T8 lengths. Ideal for uplight and downlight in commercial, retail, manufacturing, warehouse, cove and display applications. **Certain airborne contaminants can diminish the integrity of acrylic and/or polycarbonate. Click here for Acrylic-Polycarbonate Compatibility table for suitable uses.**

**CONSTRUCTION** — Compact-design channel and cover are formed from code-gauge cold-rolled steel. Easy to install row aligner included for continuous row mounting.

**Finish:** Paint options include high-gloss, baked white enamel (WH), galvanized (GALV), matte black (MB) and smoke gray (SKGY). After fabrication, five-stage iron phosphate pre-treatment ensures superior paint adhesion and rust resistance.

**OPTICS** — Standard diffuse snap on/snap off lens eliminates pixels, improves uniformity and minimizes glare. L/LENS option available.

**ELECTRICAL** — L70>60,000hours. Utilizes high-output LEDs integrated on a two-layer circuit board, ensuring cool-running operation. Optional internal pluggable wiring harness for reduced labor cost in row mounting applications. (See PLR ordering information on page 2.) Electronic LED driver is rated for 75 input watts maximum (see Operational Data on page 4 for actual wattage consumption), **multi-volt input and 0-10V dimming standard.** This fixture is designed to withstand a maximum line surge of 2.5kV at 0.75kA combination wave for indoor locations, for applications requiring higher level of protection additional surge protection must be provided.

LEDs provide nominal 80 CRI at 3000 K, 3500 K, 4000 K, or 5000 K.

Lumen output up to 2,000 lumens per foot. In 86°F (30°C) ambient environments. Luminaire should be installed in applications where ambient temperatures do not exceed 86°F (30°C).

**INSTALLATION** — Tool-less channel cover for easy installation.

Fixture may be surface mounted (with or without ZSPRG hanger), pendant or stem mounted with appropriate mounting options. Three-point aligner locks in place for easy continuous row mounting.

**LISTINGS** — CSA certified to US and Canadian safety standards. For use in damp locations between -40°F (-40°C) and 86°F (30°C).

DesignLights Consortium® (DLC) qualified product. Not all versions of this product may be DLC qualified. Please check the DLC Qualified Products List at [www.designlights.org/QPL](http://www.designlights.org/QPL) to confirm which versions are qualified.

**BUY AMERICAN** — Product with the BAA option is assembled in the USA and meets the Buy America(n) government procurement requirements under FAR, DFARS and DOT. Please refer to [www.acuitybrands.com/buy-american](http://www.acuitybrands.com/buy-american) for additional information.

**WARRANTY** — 5-year limited warranty. This is the only warranty provided and no other statements in this specification sheet create any warranty of any kind. All other express and implied warranties are disclaimed. Complete warranty terms located at:

[www.acuitybrands.com/support/warranty/terms-and-conditions](http://www.acuitybrands.com/support/warranty/terms-and-conditions)

**Note:** Actual performance may differ as a result of end-user environment and application.

All values are design or typical values, measured under laboratory conditions at 25 °C.

Specifications subject to change without notice.

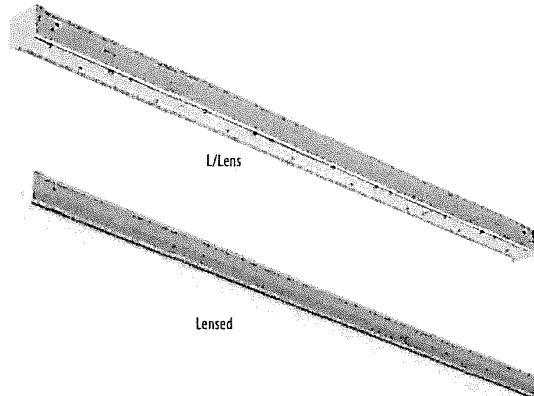
Catalog Number
Notes
Type



LED Striplight

**ZL1N**

24", 48" and 96" Lengths



**A+ Capable Luminaire**

This item is an A+ capable luminaire, which has been designed and tested to provide consistent color appearance and out-of-the-box control compatibility with simple commissioning.

- All configurations of this luminaire meet the Acuity Brands' specification for chromatic consistency
- This luminaire is part of an A+ Certified solution for nLight® control networks marked by a shaded background\*

To learn more about A+, visit [www.acuitybrands.com/aplus](http://www.acuitybrands.com/aplus).

\*See ordering tree for details

	ALASKA ARCHITECTURAL LIGHTING Project 22-28191-2 SEARHC Juneau Vintage Park	Catalog Number: ZL1N L48 5000LM FST MVOLT 40K 80CRI E10WLCP WH Type <b>AE</b>
	Submitted By ALASKA ARCHITECTURAL LIGHTING	Note:

## ZL1N LED Striplight


A+ Capable options indicated by this color background.

**ORDERING INFORMATION** Lead times will vary depending on options selected. Consult with your sales representative. **Example:** ZL1N L48 3000LM FST MVOLT 40K 80CRI WH

Series	Length	Reflectors†	Nominal lumens‡	Diffuser	Voltage
ZL1N LED striplight	L24 24" ‡	(blank) Less reflector	1500LM 1,500 lumens	FST Snap on frosted, diffuse	MVOLT 120-277V
		SMR Symmetric	2500LM 2,500 lumens	L/LENS No diffuser	120 120V
			3500LM 3,500 lumens	SBL FST Straight blade louver with snap on frosted, diffuse	208 208V
	L46 46"	(blank) Less reflector	3000LM 3,000 lumens		240 240V
	L48 48"	ASR Asymmetric (L48 only)	5000LM 5,000 lumens		277 277V
		SMR Symmetric	7000LM 7,000 lumens		347 347V ‡
TZL1N LED striplight ‡	L92 92"	(blank) Less reflector	6000LM 6,000 lumens		480 480V ‡
		SMR Symmetric	10000LM 10,000 lumens		
	L96 96"		14000LM 14,000 lumens		

Color temperature	Color rendering index	Options	Individual Controls ‡	Paint finish
30K 3000 K	80CRI 80 CRI	PLR__ Plug-in wiring ‡	LBOZU 360° low mount L5XR PIR sensor, (7-15' mounting heights), ON/OFF occupancy, pre-wired (LINK) ‡	WH White
35K 3500 K	90CRI 90 CRI	PLR1LVG Plug-in wiring-low voltage ‡	LBHOSZU 360° low mount L5XR PIR sensor, (7-15' mounting heights), high/low occupancy dimming, pre-wired (LINK) ‡	GALV Galvanized
40K 4000 K		E7W Power Sentry® PS750L, 7 watt emergency battery, CA Title 20 non-compliant (LINK) ‡	LBPZU 360° low mount L5XR PIR sensor, (7-15' mounting heights), ON/OFF photocell, pre-wired (LINK) ‡	MB Matte black
50K 5000 K		2E7W Two Power Sentry® PS750L, 7 watt emergency batteries, CA Title 20 non-compliant (LINK) ‡	LBMOSZU 360° low mount L5XR PIR sensor, (7-15' mounting heights), dimming and switching photocell, pre-wired (LINK) ‡	SKGY Smoke gray
		E10WLCP Power Sentry® PS1055LCP, 10 watt emergency battery, Certified in CA Title 20 MAEDBS (LINK) ‡	<b>Cord sets: ‡</b>	
		2E10WLCP Two Power Sentry® PST055LCP, 10 watt emergency batteries, Certified in CA Title 20 MAEDBS (LINK) ‡	CS1W Cord with NEMA 5-15P, 120V straight blade plug, 18 gauge, 3 conductors, white, 6ft	
		E15WLCP Power Sentry® PS1555LCP, 15 watt emergency battery, Certified in CA Title 20 MAEDBS (LINK) ‡	CS3W Cord with NEMA L5-15P, 120V twist lock plug, 18 gauge, 3 conductor, white, 6ft	
		OUTEND Cord set to exit endplate of fixture	CS7W Cord with NEMA 7-15P, 277V straight blade plug, 18 gauge, 3 conductors, white, 6ft	
		BAA Buy America(n) Act Compliant	CS11W Cord with NEMA L7-15P, 277V twist lock plug, 18 gauge, 3 conductors, white, 6ft	
		<b>nLight® Wireless ‡</b>	CS25W Cord with NEMA L24-20P, 347V twist lock plug, 18 gauge, 3 conductors, white, 6ft	
		NLTAIR2 nLight® Air Generation 2 enabled, 360° low mount sensor, (7 to 15' heights) (LINK)	CS97W Cord with NEMA L8-20P, 480V twist lock plug, 18 gauge, 3 conductors, white, 6ft	
		RLSXR10 nLight® Air Generation 2 enabled, 360° low mount sensor, (7 to 15' heights), UL 924 Emergency Operation, via power interrupt detection (not available with battery pack) (LINK) ‡	CS93W Cord only (no plug), 18 gauge, 3 conductors, white, 6ft	

Accessories: Order as separate catalog number.			
HC36 M12	Hanger chain, 36" (1 pair)	ZLR L24 SYM UPL WH	24" symmetric reflector with uplight, white finish
ZACVH M100	Adjustable 10' aircraft cable with Y hanger (1 pair)	ZLR L24 SYM WH	24" symmetric reflector, white finish
ZLANGBKT	Luma-tilt™ angle bracket for shelf or ledge mounting only	ZLR L46 SYM UPL WH	46" symmetric reflector with uplight, white finish
SQ_	Swivel stem hanger (specify length in 2" increments up to 48")	ZLR L46 SYM WH	46" symmetric reflector, white finish
NPP16D	nLight® wired power/relay pack, 0-10VDC dimming output (LINK)	ZLR L48 ASY WH	48" asymmetric reflector, white finish
RPP20D	nLight® air Generation 2 enabled, power/relay pack, 0-10V dimming output (LINK)	ZLR L48 SYM UPL WH	48" symmetric reflector with uplight, white finish
L5XR	Sensor Switch™ L5XR occupancy sensor (LINK)	ZLR L48 SYM WH	48" symmetric reflector, white finish
Z5PRG J2	Tong and T-grid hanger, for 15/16" T-grid (Order quantity required in multiples of 2)	ZLR L92 SYM UPL WH	92" symmetric reflector with uplight, white finish
WGZ24	24" wireguard, white ‡	ZLR L92 SYM WH	92" symmetric reflector, white finish
WGZ48	48" wireguard, white ‡	ZLR L96 SYM UPL WH	96" symmetric reflector with uplight, white finish
		ZLR L96 SYM WH	96" symmetric reflector, white finish
		UNIVERSAL REFL ALIGNER	Universal reflector aligners, quantity 1

 <b>ALASKA ARCHITECTURAL LIGHTING</b>	Project 22-28191-2 <b>SEARHC Juneau Vintage Park</b>	Catalog Number: ZL1N L48 5000LM FST MVOLT 40K 80CRI E10WLCP WH	Type <b>AE</b>
	Submitted By ALASKA ARCHITECTURAL LIGHTING	Note:	

## ZL1N LED Striplight

‡ Option Value Ordering Restrictions	
Option value	Restriction
2E7W	MVOLT required. Available with L92 or L96 only. Not available with L24, L46, L48, L92 6000LM, L96 6000LM or any cordset that includes a plug.
2E10WLCP	MVOLT required. Available with L92 or L96 with 10,000LM or 14000LM only. Not available with L24, L46, L48, L92 6000LM, L96 6000LM or any cordset that includes a plug.
347, 480	Utilizes step down transformer.
Cord Sets	Must specify voltage when plug is included. Cordsets exit back of fixture unless OUTEND option is specified.
E7W	MVOLT required. Available with L46, L48, L92 or L96 with 10000LM or 14000LM only. Not available with L24 or any cordset that includes a plug.
E10WLCP	MVOLT required. Available with L46, L48, L92 or L96 only. Not available with L24 or any cordset that includes a plug.
E15WLCP	MVOLT required. Available with L92 or L96 only. Not available with L24, L46, L48 or any cordset that includes a plug.
HTZL1N	Tandem fixture ships as two L46 or L48 fixtures.
Individual Controls	Available with MVOLT, 347 or 480 only. See ordering information on page 6 for more configurations. This sensor configuration is suitable for minimum ambient temperature of 14°F (-10°C). See page 5 for low temperature option providing -4°F (-20°C) minimum ambient. When choosing Sensor and PLR for same fixture, consult the factory. Sensors come prewired, they must be snapped into place at time of installation.
L24	Not available with 347V, 480V or emergency batteries.
nLight* Wireless	See LINK in sensor description to RLSXR specification sheet for more configurations. When choosing a sensor and PLR for same fixture, consult the factory. Sensors come prewired, they must be snapped into place at time of installation.
NLTAIR2 RLSXR10EM	Not available with 347 or 480. MVOLT required.
Nominal Lumens	See Operational Data on page 2 for actual lumens.
PLR	Not available with cordset options. Not available with sensor options. See ordering information on page 5.
PLR1LVG	Not available with cordset options. Not available with sensor options.
Reflectors	Optional. Reflectors ship separately.
WGHZ24	Not available with ASR or SMR reflector options.
WGHZ48	Not available with ASR or SMR reflector options. Order a Qty of 2 for L92 or L96 tandem fixtures.

	ALASKA ARCHITECTURAL LIGHTING Project 22-28191-2 SEARHC Juneau Vintage Park	Catalog Number: ZL1N L48 5000LM FST MVOLT 40K 80CRI E10WLCP WH Type <b>AE</b>
	Submitted By ALASKA ARCHITECTURAL LIGHTING	Note:

### ZL1N LED Striplight

OPERATIONAL DATA												
	Nominal lumen package	Length (inches)	Delivered Lumens 3000 K CCT @ 77°F (25°C) ambient temperature		Delivered Lumens 3500 K CCT @ 77°F (25°C) ambient temperature		Delivered Lumens 4000 K CCT @ 77°F (25°C) ambient temperature		Delivered Lumens 5000 K CCT @ 77°F (25°C) ambient temperature		Wattage @ 120V/277V	Comparable Light Source
			80 CRI	90 CRI	80 CRI	90 CRI	80 CRI	90 CRI	80 CRI	90 CRI		
Lensed	1500LM	24	1738	1409	1777	1467	1804	1494	1871	1528	15	1-lamp 17W T8
	2500LM	24	2265	1846	2315	1900	2351	1947	2438	1991	19	1-lamp 17W T8
	3500LM	24	3586	2924	3666	3026	3723	3084	3860	3152	31	1-lamp 32W T8, 1-lamp 54W TSHO, 50W HID
	3000LM	46 or 48	3172	2586	3243	2677	3293	2728	3415	2788	25	1-lamp 32W T8, 1-lamp 54W TSHO, 50W HID
	5000LM	46 or 48	4417	3601	4515	3727	4585	3798	4754	3882	34	2-lamp 32W T8, 1-lamp 54W TSHO, 70W HID
	7000LM	46 or 48	6535	5328	6681	5515	6785	5619	7035	5744	52	3-lamp 32W T8, 2-lamp 54W TSHO, 100W HID
	6000LM	92 or 96	6561	5349	6708	5537	6812	5642	7063	5767	48	3-lamp 32W T8, 2-lamp 54W TSHO, 100W HID
	10000LM	92 or 96	8687	7082	8881	7331	9019	7470	9351	7636	68	4-lamp 32W T8, 2-lamp 54W TSHO, 100W HID
Unlensed	14000LM	92 or 96	12457	10513	12735	10665	12933	10711	13409	10949	104	4-lamp 32W T8, 3-lamp 54W TSHO, 150W HID
	1500LM	24	1881	1534	1923	1588	1953	1618	2025	1654	15	1-lamp 17W T8
	2500LM	24	2452	1999	2506	2069	2545	2108	2639	2155	19	1-lamp 17W T8
	3500LM	24	3882	3165	3969	3276	4031	3338	4179	3412	31	1-lamp 32W T8, 1-lamp 54W TSHO, 50W HID
	3000LM	46 or 48	3434	2800	3511	2898	3565	2953	3697	3019	25	1-lamp 32W T8, 1-lamp 54W TSHO, 50W HID
	5000LM	46 or 48	4781	3898	4888	4035	4964	4111	5147	4203	34	2-lamp 32W T8, 1-lamp 54W TSHO, 70W HID
	7000LM	46 or 48	7075	5768	7233	5971	7345	6083	7616	6219	52	3-lamp 32W T8, 2-lamp 54W TSHO, 100W HID
	6000LM	92 or 96	7103	5791	7261	5995	7374	6108	7646	6243	48	3-lamp 32W T8, 2-lamp 54W TSHO, 100W HID
10000LM	92 or 96	9404	7667	9614	7937	9764	8087	10123	8266	68	4-lamp 32W T8, 2-lamp 54W TSHO, 100W HID	
14000LM	92 or 96	13485	10994	13786	11381	14001	11596	14516	11853	104	4-lamp 32W T8, 3-lamp 54W TSHO, 150W HID	

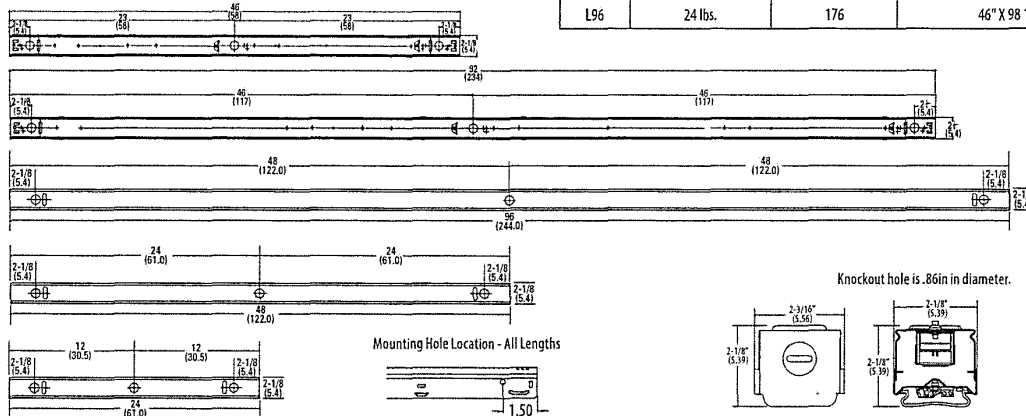
### PROJECTED LUMEN MAINTENANCE

Operating Hours	0	15,000	30,000	45,000	60,000	100,000
Lumen Maintenance Factor	1	0.94	0.89	0.83	0.79	0.67

### DIMENSIONS

All dimensions are shown in inches (centimeters) unless otherwise noted. Specifications subject to change without notice.

PALLET DIMENSIONS			
Length	Approximate weight	Fixtures per pallet	Approximate pallet dimensions (L x W x H)
L24	7 lbs.	408	46" X 51" X 32 11/16"
L46	11 lbs.	176	46" X 51" X 32 11/16"
L48	12 lbs.	176	46" X 51" X 31 3/8"
L92	22 lbs.	176	46" X 98 1/2" X 31 1/16"
L96	24 lbs.	176	46" X 98 1/2" X 31 1/16"




### PHOTOMETRICS

Please see [www.lithonia.com](http://www.lithonia.com)



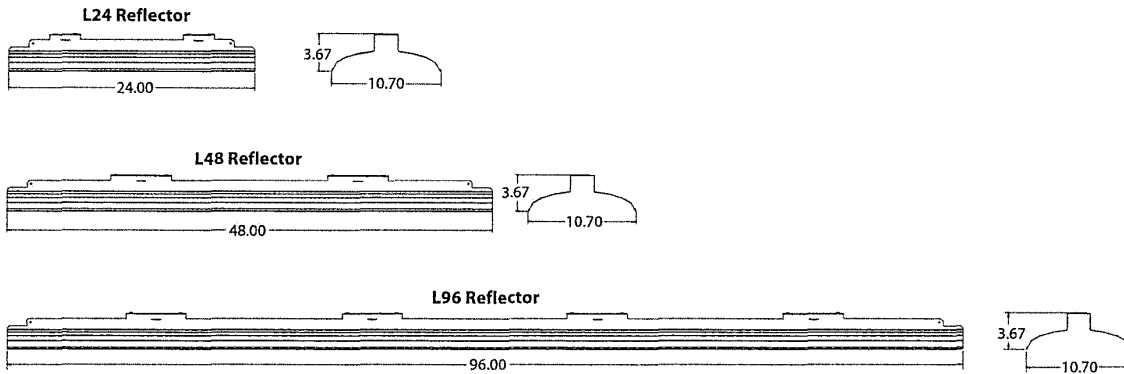
ZL1N



	ALASKA ARCHITECTURAL LIGHTING Project 22-28191-2 SEARHC Juneau Vintage Park	Catalog Number: ZL1N L48 5000LM FST MVOLT 40K 80CRI E10WLCP WH	Type <b>AE</b>
	Submitted By ALASKA ARCHITECTURAL LIGHTING	Note:	

## ZL1N LED Striplight

### REFLECTORS (Optional)



### PHOTOMETRICS

Please see [www.lithonia.com](http://www.lithonia.com)

### PLUG-IN WIRING INFORMATION

Advanced plug-in system with two-circuit capability. Available on industrial and strip products and a variety of architectural products mounted in continuous. PLR22 (2-circuit) and crossover harness switches hot circuit serving next fixture in row. Reduces fixture types on job for alternating circuit applications (see example below.)

Easy one-step installation, saves up to 35% on labor costs. Expanded switching flexibility helps save energy.

Rows can be 50% longer with two-circuit systems. Polarized, lock-together nylon connectors prevent miswiring in the field. #12 THHN conductor, rated 600V, 90°C. White neutral wire included. Grounding accomplished by fixture in-row connectors.

CSA certified systems available with up to 2 circuits. G ground required.

Not for use with dedicated emergency circuits.

Note: Specifications subject to change without notice.

Wiring



**PLR**

Advanced 1 or 2-Circuit Plug-In


#### ORDERING INFORMATION

Lead times will vary depending on options selected. Consult with your sales representative.

Series	Number of hot wires	Branch circuits	Dimming	Ground
PLR	(blank) Not required for PLR22	Circuits to which driver is connected	LV Low-voltage dimming	G Ground (required)
PLR22	1 Black	(blank) Not required for PLR22 or PLR1		
	2 Black and red	A Black wire		
		B Red wire		
		Battery charging circuit (must be unswitched)		
		(blank) No battery charging circuit		
		ELA Battery pack wired to black wire		
		ELB Battery pack wired to red wire		

#### Typical Applications

- Multiple-circuit and single-circuit for longer continuous rows
- Multiple-circuit with alternating fixtures on separate circuits, 2-circuit (PLR22)
- Multiple circuit with night-lights located along row as desired

	ALASKA ARCHITECTURAL LIGHTING Project 22-28191-2 SEARHC Juneau Vintage Park	Catalog Number: ZL1N L48 5000LM FST MVOLT 40K 80CRI E10WLCP WH	Type <b>AE</b>
	Submitted By ALASKA ARCHITECTURAL LIGHTING	Note:	

## ZL1N LED Strilight

**LSXR** — Fixture Mount Occupancy Sensor (see [www.AcuityControls.com](http://www.AcuityControls.com) for additional information)

- Three interchangeable lens options to satisfy multiple mounting heights and coverage pattern requirements.
- Integrated mounting bracket drops lens down 3" from chase nipple.
- Single or dual relay versions — designed with robust protection from the harsh switching requirements of TS and LED loads.
- Photozell and 0-10VDC dimming options.
- No PIR field calibration or sensitivity adjustments required.
- Sensor ambient temperature rating of 14°F (-10°C) to 131°F (55°C).

LSXR configuration	Comparable CMRB sensor	Old style sensor nomenclature
For shortest lead times use one of the following LSXR configurations		
LCOZU	CMRB 50	MSI
LCHOSZU	CMRB 50 D	MSID
LCPZU	CMRB 50 P	MSIPED
LAOZU	CMRB 6	MSI360
LAHOSZU	CMRB 6 D	MSI360D
LAPZU	CMRB 6 P	MSI360PED

**SELECTIONS BELOW WILL EXTEND ORDER LEAD TIME. CONSULT YOUR SALES REPRESENTATIVE FOR DETAILS.**

### SINGLE RELAY

#### ORDERING INFORMATION

Example: LAHOSZU

Series	Lens option	Dimming/Photocell	Max. dim level	Min. dim level	Temp/Humidity	Default occupancy time delay
L LSXR passive infrared indoor occupancy sensor	A High mount, 360°	O None <sup>1</sup>	0 10 VDC	5 Minimum dim level of ballast	Z None T Low temperature <sup>2</sup>	I 30 sec
		H High/low occupancy operation	9 9 VDC	1 1 VDC		D 2.5 min
	B Low mount, 360°	P Switching photocell (on/off) <sup>1</sup>	8 8 VDC	2 2 VDC		X 5.0 min
		C High mount aisleway	M Dimming and switching photocell	7 7 VDC		3 3 VDC
	G Dimming and switching photocell with high/low occupancy operation		4 4 VDC	4 4 VDC		U 10.0 min (with minimum 15 minute on time)
			5 5 VDC	5 5 VDC		V 15.0 min
	6 6 VDC	6 6 VDC	W 20.0 min			
				Y 30.0 min		

**Notes**

- 1 Max and min dim levels not applicable with this option.
- 2 Ambient temperature rating of -4°F (-20°C) to 131°F (55°C).

### DUAL RELAY (Available with 120, 277, and 347V only)

#### ORDERING INFORMATION

Example: LA2KZU


Series	Lens option	Poles	Operating mode	Temp/Humidity	Default occupancy time delay
L LSXR passive infrared indoor occupancy sensor	A High mount, 360° B Low mount, 360° C High mount aisleway	2 Dual relay	J None	Z None T Low temperature <sup>1</sup>	I 30 sec
			K Alternating off relays (promotes even lamp wear)		D 2.5 min
			O Alternating off relays w/photocell		X 5.0 min
			P Switching photocell(on/off)		R 7.5 min
			E Photocell on/off (pole 1 only)		U 10.0 min (with minimum 15 minute on time)
			F Photocell on/off - both poles (dual set-point)		V 15.0 min
					W 20.0 min
	Y 30.0 min				

**Notes**

- 1 Ambient temperature rating of -4°F (-20°C) to 131°F (55°C).

Example: LENS 50 J100

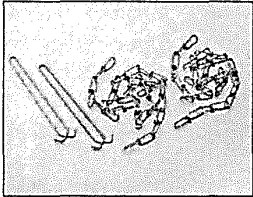
Replacement lenses: Order as separate catalog number.		
Series	Lens type	Package quantity
LENS	6 High mount 360°	[blank] Single Lens
	10 Low mount 360°	J10 10-pack
	50 High mount aisleway	J100 100-pack

	ALASKA ARCHITECTURAL LIGHTING	Project 22-28191-2 SEARHC Juneau Vintage Park  Submitted By ALASKA ARCHITECTURAL LIGHTING	Catalog Number: ZL1N L48 5000LM FST MVOLT 40K 80CRI E10WLCP WH  Note:	Type <b>AE</b>
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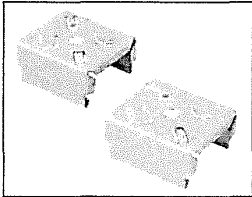
**ZL1N LED Striplight**

**OPTIONS AND ACCESSORIES**

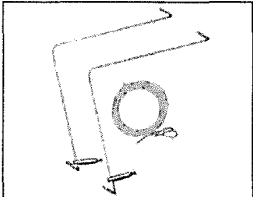
The Z Series fixture offers numerous options for almost every electrical and optical component, including a long list of field-installable accessories.



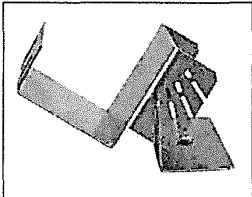
**HANGER CHAIN**  
 36" chain with Y hanger.  
  
 Order as:  
 HC36



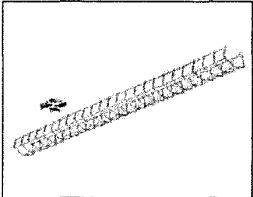
**Z SPRING HANGER**  
 Snap 'n' lock design requires no fasteners and can be used on T-grid ceiling or universal mounting systems.  
  
 Order as:  
 ZSPRG J2



**ZACVH HANGER**  
 10' Aircraft cable with Y hanger.  
  
 Order as:  
 ZACVH

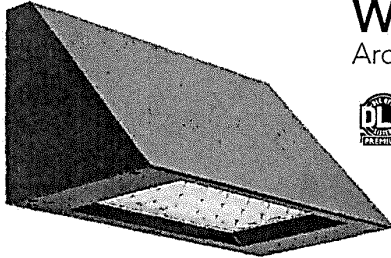


**ANGLE MOUNTING BRACKET**  
 Luma-tilt™ angle bracket ships as a pair  
  
 Order as:  
 ZLANGBKT



**WIRE GUARD**  
  
 Order as:  
 WGZ24  
 WGZ48

	Project 22-28191-2 SEARHC Juneau Vintage Park	Catalog Number: WDGE3 LED P2 40K 70CRI RFT MVOLT SRM DDBXD	Type <b>SE</b>
	Submitted By ALASKA ARCHITECTURAL LIGHTING	Note:	



## WDGE3 LED Architectural Wall Sconce



Catalog Number	
Notes	
Type	

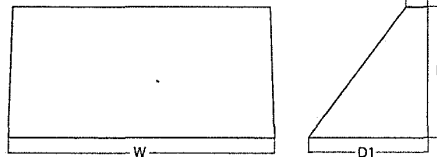
### Introduction

The WDGE LED family is designed to meet specifier's every wall-mounted lighting need in a widely accepted shape that blends with any architecture. The clean rectilinear design comes in four sizes with lumen packages ranging from 1,200 to 25,000 lumens, providing a true site-wide solution. Embedded with nLight® AIR wireless controls, the WDGE family provides additional energy savings and code compliance.

WDGE3 has been designed to deliver up to 12,000 lumens through a precision refractive lens with wide distribution, perfect for augmenting the lighting from pole mounted luminaires.

### Specifications

- Depth (D1): 8"
- Depth (D2): 1.5"
- Height: 9"
- Width: 18"
- Weight: 19.5 lbs (without options)



### WDGE LED Family Overview

Luminaire	Standard EM, 0°C	Cold EM, -20°C	Sensor	Lumens (4000K)					
				P1	P2	P3	P4	P5	P6
WDGE1 LED	4W	--	--	1,200	2,000	--	--	--	--
WDGE2 LED	10W	18W	Standalone / nLight	1,200	2,000	3,000	4,500	6,000	--
WDGE3 LED	15W	18W	Standalone / nLight	7,500	8,500	10,000	12,000	--	--
WDGE4 LED	--	--	Standalone / nLight	12,000	16,000	18,000	20,000	22,000	25,000

### Ordering Information

EXAMPLE: WDGE3 LED P3 40K 70CRI R3 MVOLT SRM DDBXD

Series	Package	Color Temperature	CRI	Distribution	Voltage	Mounting
WDGE3 LED	P1	30K 3000K	70CRI	R2 Type 2	MVOLT	Shipped included SRM Surface mounting bracket ICW Indirect Canopy/Ceiling Washer bracket (dry/damp locations only) <sup>4</sup>
	P2	40K 4000K	80CRI	R3 Type 3	347 <sup>1</sup>	
	P3	50K 5000K		R4 Type 4	480 <sup>1</sup>	
	P4			RFT Forward Throw		
						Shipped separately AWS 3/8inch Architectural wall spacer PBBW Surface-mounted back box (top, left, right conduit entry). Use when there is no junction box available.

Options	Finish
<b>E15WH</b> Emergency battery backup, Certified in CA Title 20 MAEDBS (15W, 5°C min) <b>E20WC</b> Emergency battery backup, Certified in CA Title 20 MAEDBS (18W, -20°C min) <b>PE<sup>2</sup></b> Photocell, Button type <b>DMG<sup>3</sup></b> 0-10V dimming wires pulled outside fixture (for use with an external control, ordered separately) <b>BCE</b> Bottom conduit entry for back box (PBBW). Total of 4 entry points. <b>SPD10KV</b> 10kV Surge pack <b>BAA</b> Buy America(n) Act Compliant	<b>Standalone Sensors/Controls</b> <b>PIR</b> Bi-level (100/35%) motion sensor for 8-15' mounting heights. Intended for use on switched circuits with external dusk to dawn switching. <b>PIRH</b> Bi-level (100/35%) motion sensor for 15-30' mounting heights. Intended for use on switched circuits with external dusk to dawn switching. <b>PIR1FC3V</b> Bi-level (100/35%) motion sensor for 8-15' mounting heights with photocell pre-programmed for dusk to dawn operation. <b>PIRH1FC3V</b> Bi-level (100/35%) motion sensor for 15-30' mounting heights with photocell pre-programmed for dusk to dawn operation. <b>Networked Sensors/Controls</b> <b>NLTAIR2 PIR</b> nLightAIR Wireless enabled bi-level motion/ambient sensor for 8-15' mounting heights. <b>NLTAIR2 PIRH</b> nLightAIR Wireless enabled bi-level motion/ambient sensor for 15-30' mounting heights. See page 4 for out of box functionality.
	<b>DDBXD</b> Dark bronze <b>DBLXD</b> Black <b>DNAXD</b> Natural aluminum <b>DWHXD</b> White <b>DSSXD</b> Sandstone <b>DDBTXD</b> Textured dark bronze <b>DBLBXD</b> Textured black <b>DNATXD</b> Textured natural aluminum <b>DWHGXD</b> Textured white <b>DSSTXD</b> Textured sandstone

### Accessories

Ordered and shipped separately.

- WDGEAWS DDBXD WDGE 3/8inch Architectural Wall Spacer (specify finish)
- WDGE3PBBW DDBXD U WDGE3 surface-mounted back box (specify finish)

### NOTES


- 347V and 480V not available with E15WH and E20WC.
- PE not available in 480V and with sensors/controls.
- DMG option not available with sensors/controls.
- Not qualified for DLC. Not available with emergency battery backup or sensors/controls



COMMERCIAL OUTDOOR

One Lithonia Way • Conyers, Georgia 30012 • Phone: 1-800-705-SERV (7378) • www.lithonia.com  
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WDGE3 LED  
 Rev. 03/01/22

 <b>ALASKA ARCHITECTURAL LIGHTING</b>	Project 22-28191-2 <b>SEARHC Juneau Vintage Park</b>	Catalog Number: <b>WDGE3 LED P2 40K 70CRI RFT MVOLT SRM DDBXD</b>	Type <b>SE</b>
	Submitted By <b>ALASKA ARCHITECTURAL LIGHTING</b>	Note:	

**Performance Data**

**Lumen Output**

Lumen values are from photometric tests performed in accordance with IESNA LM-79-08. Data is considered to be representative of the configurations shown, within the tolerances allowed by Lighting Facts. Contact factory for performance data on any configurations not shown here.

Performance Package	System Watts	Dist. Type	30K (3000K, 70 CRI)					40K (4000K, 70 CRI)					50K (5000K, 70 CRI)				
			Lumens	LPW	B	U	G	Lumens	LPW	B	U	G	Lumens	LPW	B	U	G
P1	52W	R2	7,037	136	1	0	1	7,649	148	2	0	1	7,649	148	2	0	1
		R3	6,922	134	1	0	2	7,524	145	1	0	2	7,524	145	1	0	2
		R4	7,133	138	1	0	2	7,753	150	1	0	2	7,753	150	1	0	2
		RFT	6,985	135	1	0	2	7,592	147	1	0	2	7,592	147	1	0	2
P2	59W	R2	7,968	135	2	0	1	8,661	147	2	0	1	8,661	147	2	0	1
		R3	7,838	133	1	0	2	8,519	144	1	0	2	8,519	144	1	0	2
		R4	8,077	137	1	0	2	8,779	149	1	0	2	8,779	149	1	0	2
		RFT	7,909	134	1	0	2	8,597	146	2	0	2	8,597	146	2	0	2
P3	71W	R2	9,404	132	2	0	1	10,221	143	2	0	1	10,221	143	2	0	1
		R3	9,250	130	2	0	2	10,054	141	2	0	2	10,054	141	2	0	2
		R4	9,532	134	2	0	2	10,361	145	2	0	2	10,361	145	2	0	2
		RFT	9,334	131	2	0	2	10,146	142	2	0	2	10,146	142	2	0	2
P4	88W	R2	11,380	129	2	0	1	12,369	140	2	0	1	12,369	140	2	0	1
		R3	11,194	127	2	0	2	12,167	138	2	0	2	12,167	138	2	0	2
		R4	11,535	131	2	0	2	12,538	142	2	0	2	12,538	142	2	0	2
		RFT	11,295	128	2	0	2	12,277	139	2	0	2	12,277	139	2	0	2

**Electrical Load**

Performance Package	System Watts	Current (A)					
		120V	208V	240V	277V	347V	480V
P1	52W	0.437	0.246	0.213	0.186	0.150	0.110
P2	59W	0.498	0.287	0.251	0.220	0.175	0.126
P3	71W	0.598	0.344	0.300	0.262	0.210	0.152
P4	88W	0.727	0.424	0.373	0.333	0.260	0.190

**Lumen Output in Emergency Mode (4000K, 70 CRI)**

Option	Dist. Type	Lumens
E15WH	R2	3,185
	R3	3,133
	R4	3,229
	RFT	3,162
E20WC	R2	3,669
	R3	3,609
	R4	3,719
	RFT	3,642

**Lumen Multiplier for 80CRI**

CCT	Multiplier
30K	0.891
40K	0.906
50K	0.906

**Lumen Ambient Temperature (LAT) Multipliers**

Use these factors to determine relative lumen output for average ambient temperatures from 0-40°C (32-104°F).

Ambient	Lumen Multiplier
0°C / 32°F	1.05
10°C / 50°F	1.03
20°C / 68°F	1.01
25°C / 77°F	1.00
30°C / 86°F	0.99
40°C / 104°F	0.97

**Projected LED Lumen Maintenance**

Data references the extrapolated performance projections for the platforms noted in a 25°C ambient, based on 10,000 hours of LED testing (tested per IESNA LM-80-08 and projected per IESNA TM-21-11).

To calculate LLF, use the lumen maintenance factor that corresponds to the desired number of operating hours below. For other lumen maintenance values, contact factory.

Operating Hours	0	25,000	50,000	100,000
Lumen Maintenance Factor	1.0	>0.98	>0.97	>0.92

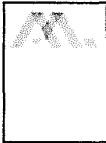


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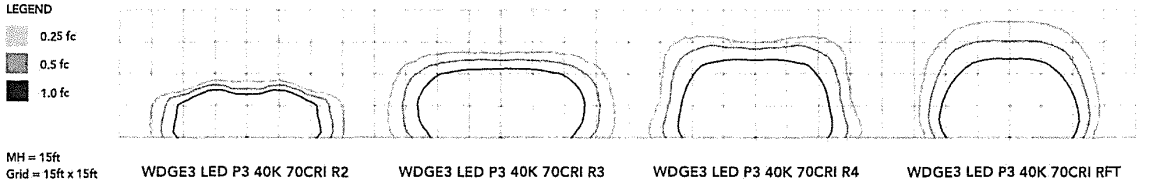
WDGE3 LED  
 Rev. 03/01/22



	<p>Project 22-28191-2 SEARHC Juneau Vintage Park</p> <p>Submitted By ALASKA ARCHITECTURAL LIGHTING</p>	<p>Catalog Number: WDGE3 LED P2 40K 70CRI RFT MVOLT SRM DDBXD</p> <p>Note:</p>	<p>Type <b>SE</b></p>
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**Photometric Diagrams** To see complete photometric reports or download .ies files for this product, visit the Lithonia Lighting WDGE LED homepage. Tested in accordance with IESNA LM-79 and LM-80 standards.

LEGEND  
 0.25 fc  
 0.5 fc  
 1.0 fc



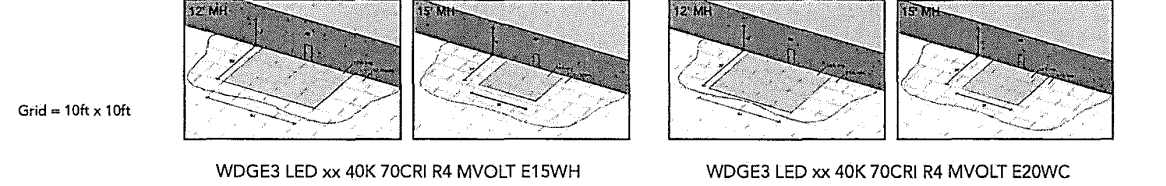
**Emergency Egress Options**

**Emergency Battery Backup**

The emergency battery backup is integral to the luminaire — no external housing required! This design provides reliable emergency operation while maintaining the aesthetics of the product. All emergency battery backup configurations include an independent secondary driver with an integral relay to immediately detect loss of normal power and automatically energize the luminaire. The emergency battery will power the luminaire for a minimum duration of 90 minutes (maximum duration of three hours) from the time normal power is lost and maintain, minimum of 60% of the light output at the end of 90minutes.

Applicable codes: NFPA 70/NEC – section 700.16, NFPA 101 Life Safety Code Section 7.9


The examples below show illuminance of 1 fc average and 0.1 fc minimum in emergency mode with E15WH or E20WC and R4 distribution.



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WDGE3 LED  
 Rev. 03/01/22

 <b>ALASKA ARCHITECTURAL LIGHTING</b>	Project 22-28191-2 <b>SEARHC Juneau Vintage Park</b>	Catalog Number: WDG3 LED P2 40K 70CRI RFT MVOLT SRM DDBXD	Type <b>SE</b>
	Submitted By ALASKA ARCHITECTURAL LIGHTING	Note:	

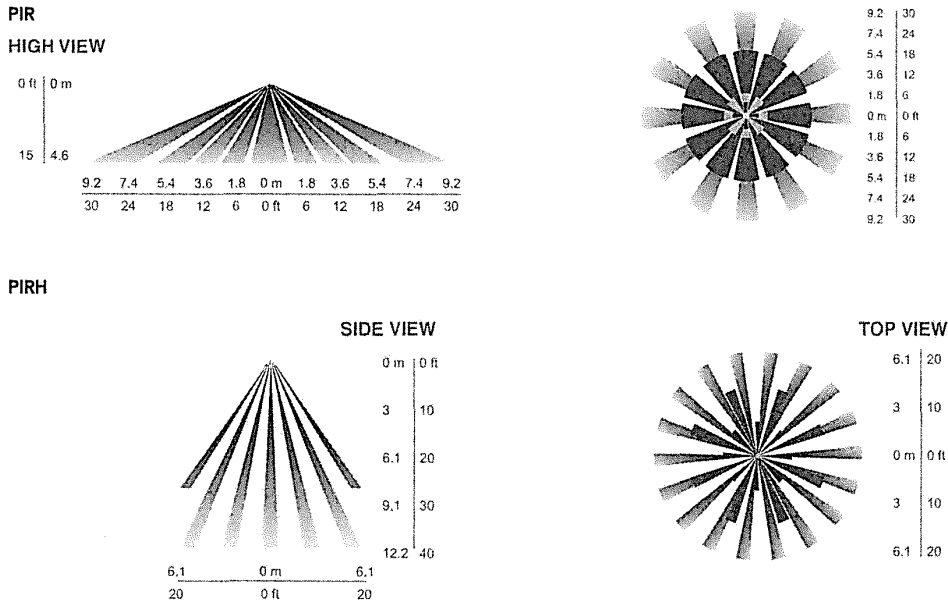
**Control / Sensor Options**

**Motion/Ambient Sensor (PIR, PIRH)**

Motion/Ambient sensor (Sensor Switch MSOD) is integrated into the the luminaire. The sensor provides both Motion and Daylight based dimming of the luminaire. For motion detection, the sensor utilizes 100% Digital Passive Infrared (PIR) technology that is tuned for walking size motion while preventing false tripping from the environment. The integrated photocell enables additional energy savings during daytime periods when there is sufficient daylight. Optimize sensor coverage by either selecting PIR or PIRH option. PIR option comes with a sensor lens that is optimized to provide maximum coverage for mounting heights between 8-15ft, while PIRH is optimized for 15-40ft mounting height.

**Networked Control (NLTAIR2)**

nLight® AIR is a wireless lighting controls platform that allows for seamless integration of both indoor and outdoor luminaires. Five-tier security architecture, 900 MHz wireless communication and app (CLAIRITY™ Pro) based configurability combined together make nLight® AIR a secure, reliable and easy to use platform.



**Motion/Ambient Sensor Default Settings**


Option	Dim Level	High Level (When triggered)	Photocell Operation	Motion Time Delay	Ramp-down Time	Ramp-up Time
PIR or PIRH	Motion - 3V (37% of full output) Photocell - 0V (turned off)	10V (100% output)	Enabled @ 5fc	5 min	5 min	Motion - 3 sec Photocell - 45 sec
PIR1FC3V, PIRH1FC3V	Motion - 3V (37% of full output) Photocell - 0V (turned off)	10V (100% output)	Enabled @ 1fc	5 min	5 min	Motion - 3 sec Photocell - 45 sec
NLTAIR2 PIR, NLTAIR2 PIRH (out of box)	Motion - 3V (37% of full output) Photocell - 0V (turned off)	10V (100% output)	Enabled @ 5fc	7.5 min	5 min	Motion - 3 sec Photocell - 45 sec



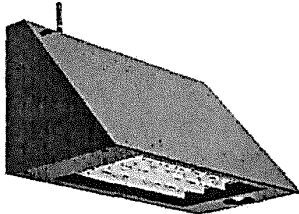
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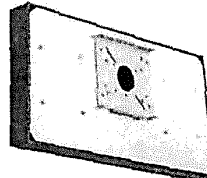
 <p>ALASKA ARCHITECTURAL LIGHTING</p>	<p>Project 22-28191-2 SEARHC Juneau Vintage Park</p> <p>Submitted By ALASKA ARCHITECTURAL LIGHTING</p>	<p>Catalog Number: WDGE3 LED P2 40K 70CRI RFT MVOLT SRM DDBXD</p> <p>Note:</p>	<p>Type <b>SE</b></p>
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**Mounting, Options & Accessories**



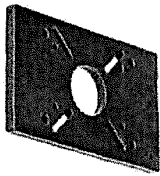
**NLTAIR2 PIR – nLight AIR Motion/Ambient Sensor**

D = 8"  
H = 11"  
W = 18"



**PBBW – Surface-Mounted Back Box**  
Use when there is no junction box available.

D = 1.75"  
H = 9"  
W = 18"



**AWS – 3/8inch Architectural Wall Spacer**

D = 0.38"  
H = 4.4"  
W = 7.5"

**FEATURES & SPECIFICATIONS**

**INTENDED USE**

Common architectural look, with clean rectilinear shape, of the WDGE LED was designed to blend with any type of construction, whether it be tilt-up, frame or brick. Applications include commercial offices, warehouses, hospitals, schools, malls, restaurants, and other commercial buildings.

**CONSTRUCTION**

The single-piece die-cast aluminum housing to optimize thermal transfer from the light engine and promote long life. The driver is mounted in direct contact with the casting for a low operating temperature and long life. The die-cast door frame is fully gasketed with a one-piece solid silicone gasket to keep out moisture and dust, providing an IP65 rating for the luminaire.

**FINISH**

Exterior painted parts are protected by a zinc-infused Super Durable TGIC thermoset powder coat finish that provides superior resistance to corrosion and weathering. A tightly controlled multi-stage process ensures a 3 mils thickness for a finish that can withstand extreme climate changes without cracking or peeling. Standard Super Durable colors include dark bronze, black, natural aluminum, sandstone and white. Available in textured and non-textured finishes.

**OPTICS**

Individually formed acrylic lenses are engineered for superior application efficiency which maximizes the light in the areas where it is most needed. Light engines are available in 3000 K, 4000 K or 5000 K configurations. The WDGE LED has zero uplight and qualifies as a Nighttime Friendly™ product, meaning it is consistent with the LEED® and Green Globes™ criteria for eliminating wasteful uplight.

**ELECTRICAL**

Light engine consists of high-efficacy LEDs mounted to metal-core circuit boards to maximize heat dissipation and promote long life (up to L92/100,000 hours at 25°C). The electronic driver has a power factor of >90%, THD <20%. Luminaire comes with built in 6kV surge protection, which meets a minimum Category C low exposure (per ANSI/IEEE C62.41.2). Fixture ships standard with 0-10v dimmable driver.

**INSTALLATION**

A universal mounting plate with integral mounting support arms allows the fixture to hinge down for easy access while making wiring connections. The 3/8" Architectural Wall Spacer (AWS) can be used to create a floating appearance or to accommodate small imperfections in the wall surface. The ICW option can be used to mount the luminaire inverted for indirect lighting in dry and damp locations. Design can withstand up to a 1.5 G vibration load rating per ANSI C136.31.

**LISTINGS**

CSA certified to U.S. and Canadian standards. Light engines are IP66 rated; luminaire is IP65 rated. PIR options are rated for wet location. Rated for -40°C minimum ambient. DesignLights Consortium® (DLC) Premium qualified product and DLC qualified product. Not all versions of this product may be DLC Premium qualified or DLC qualified. Please check the DLC Qualified Products List at [www.designlights.org/DQL](http://www.designlights.org/DQL) to confirm which versions are qualified. International Dark-Sky Association (IDA) Fixture Seal of Approval (FSA) is available for all products on this page utilizing 3000K color temperature and SRM mounting only.

**BUY AMERICAN**

Product with the BAA option is assembled in the USA and meets the Buy America(n) government procurement requirements under FAR, DFARS and DOT. Please refer to [www.acuitybrands.com/support/warranty/terms-and-conditions](http://www.acuitybrands.com/support/warranty/terms-and-conditions) for additional information.

**WARRANTY**

5-year limited warranty. This is the only warranty provided and no other statements in this specification sheet create any warranty of any kind. All other express and implied warranties are disclaimed. Complete warranty terms located at: [www.acuitybrands.com/support/warranty/terms-and-conditions](http://www.acuitybrands.com/support/warranty/terms-and-conditions)

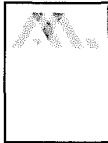
**Note:** Actual performance may differ as a result of end-user environment and application. All values are design or typical values, measured under laboratory conditions at 25 °C. Specifications subject to change without notice.



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WDGE3 LED  
Rev. 03/01/22

	<p>Project 22-28191-2 SEARHC Juneau Vintage Park</p> <p>Submitted By ALASKA ARCHITECTURAL LIGHTING</p>	<p>Catalog Number: 55 942-K4</p> <p>Note:</p>	<p>Type <b>SK</b></p>
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LED semi-recessed ceiling downlight - partially frosted crystal glass

BEGA

**Application**

LED semi-recessed ceiling luminaire with partially frosted crystal glass and symmetrical wide beam light distribution designed for downlighting atriums, passages and other interior and exterior locations.

**Materials**

Luminaire housing constructed of die-cast marine grade, copper free (≤0.3% copper content) A360.0 aluminum alloy  
Stainless steel trim ring  
Partially frosted crystal glass  
Reflector made of pure anodized aluminum  
High temperature silicone gasket  
Stainless steel screw clamps  
Galvanized steel rough in ceiling pan with through wiring box

**NRTL** listed to North American Standards, suitable for wet locations  
Protection class IP 65  
Weight: 1.4 lbs

**Electrical**

Operating voltage	120-277VAC
Minimum start temperature	-30° C
LED module wattage	8.7 W
System wattage	11 W
Controllability	0-10V dimming down to 0.1%
Color rendering index	Ra > 85
Luminaire lumens	744 lumens (3000K)
Lifetime at Ta = 15° C	420,000 h (L70)
Lifetime at Ta = 40° C	260,000 h (L70)

**LED color temperature**

- 4000K - Product number + **K4**
- 3500K - Product number + **K35**
- 3000K - Product number + **K3**
- 2700K - Product number + **K27**

**BEGA** can supply you with suitable LED replacement modules for up to 20 years after the purchase of LED luminaires - see website for details

**Finish**

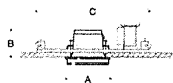
#4 brushed stainless steel.  
Custom colors are not available.  
Stainless steel requires regular cleaning and maintenance, much like household appliances to maintain its luster and prevent tarnishing or the appearance of rust like stains.

Type:

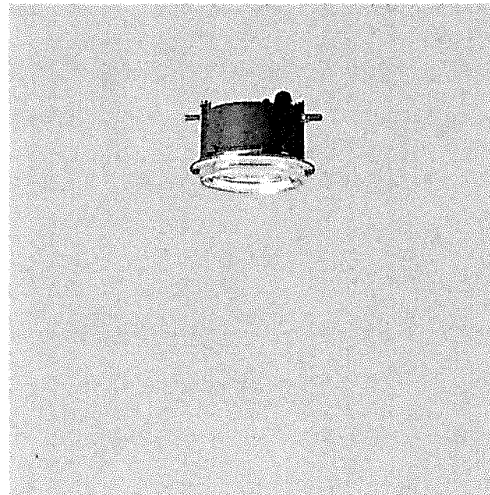
BEGA Product:

Project:

Modified:




LED semi-recessed ceiling downlight - partially frosted crystal glass				
	LED	A	B	C
55942	8.7 W	4 1/2"	2 3/4"	18"



**BEGA** 1000 BEGA Way, Carpinteria, CA 93013 (805) 684-0533 info@bega-us.com

Due to the dynamic nature of lighting products and the associated technologies, luminaire data on this sheet is subject to change at the discretion of BEGA North America. For the most current technical data, please refer to bega-us.com  
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 <p>ALASKA ARCHITECTURAL LIGHTING</p>	<p>Project 22-28191-2 SEARHC Juneau Vintage Park</p> <p>Submitted By ALASKA ARCHITECTURAL LIGHTING</p>	<p>Catalog Number: 55 942-K4</p> <p>Note: USE INVERTER BELOW</p>	<p>Type <b>SKE</b></p>
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LED semi-recessed ceiling downlight - partially frosted crystal glass

BEGA

**Application**

LED semi-recessed ceiling luminaire with partially frosted crystal glass and symmetrical wide beam light distribution designed for downlighting atriums, passages and other interior and exterior locations.

**Materials**

Luminaire housing constructed of die-cast marine grade, copper free (≤0.3% copper content) A360.0 aluminum alloy  
Stainless steel trim ring  
Partially frosted crystal glass  
Reflector made of pure anodized aluminum  
High temperature silicone gasket  
Stainless steel screw clamps  
Galvanized steel rough in ceiling pan with through wiring box  
NRTL listed to North American Standards, suitable for wet locations  
Protection class IP65  
Weight: 1.4 lbs

**Electrical**

Operating voltage	120-277VAC
Minimum start temperature	-30° C
LED module wattage	8.7W
System wattage	11W
Controllability	0-10V dimming down to 0.1%
Color rendering index	Ra > 85
Luminaire lumens	744 lumens (3000K)
Lifetime at Ta = 15° C	420,000 h (L70)
Lifetime at Ta = 40° C	260,000 h (L70)

**LED color temperature**

- 4000K - Product number + **K4**
- 3500K - Product number + **K35**
- 3000K - Product number + **K3**
- 2700K - Product number + **K27**

BEGA can supply you with suitable LED replacement modules for up to 20 years after the purchase of LED luminaires - see website for details

**Finish**

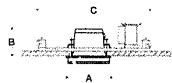
#4 brushed stainless steel.  
Custom colors are not available.  
Stainless steel requires regular cleaning and maintenance, much like household appliances to maintain its luster and prevent tarnishing or the appearance of rust like stains.

Type:

BEGA Product:

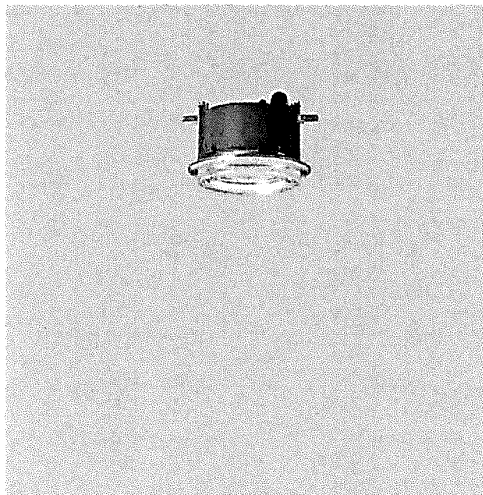
Project:

Modified:




LED semi-recessed ceiling downlight - partially frosted crystal glass

	LED	A	B	C
55942	8.7W	4 1/2	2 3/4	18



BEGA 1000 BEGA Way, Carpinteria, CA 93013 (805) 684-0533 info@bega-us.com

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 <p>ALASKA ARCHITECTURAL LIGHTING</p>	<p>Project 22-28191-2 SEARHC Juneau Vintage Park</p> <p>Submitted By ALASKA ARCHITECTURAL LIGHTING</p>	<p>Catalog Number: EMS-55-LC-V3-S</p> <p>Note:</p>	<p>Type <b>SKE</b></p>
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# EMS

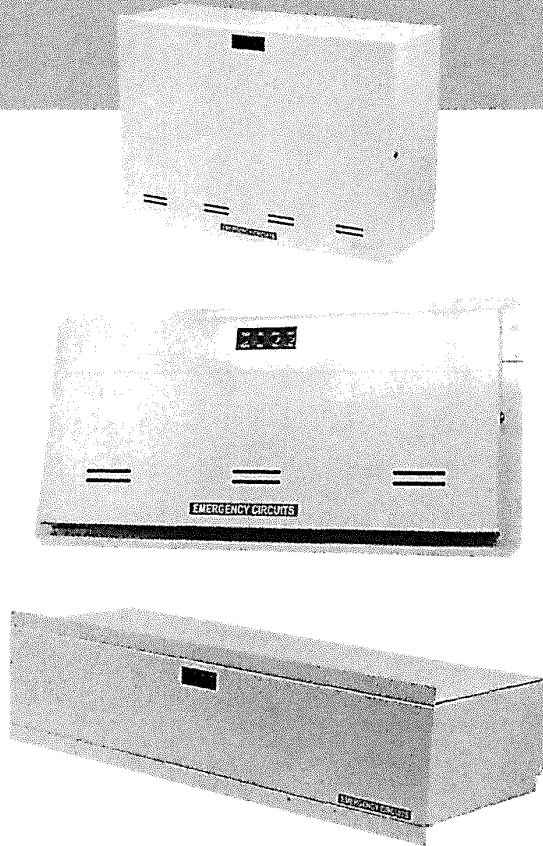
## Emergency Micro Power Inverters

The EMS Series is designed to provide 20- to 55- Watts of emergency power to incandescent, fluorescent, and/or LED fixtures. The EMS unit provides clean, sinusoidal AC output power allowing it to be remotely mounted up to 1,000 feet away from the controlled fixture(s).

Unlike a ballast fluorescent emergency pack, the EMS provides power to the input side of the fixture, (including the ballast) eliminating any chance of incompatibility. EMS Series models are available for surface, recessed or ceiling T-Grid mounting if required. All EMS systems will provide emergency power output for a minimum of 90 minutes.

### FEATURES

- For powering incandescent, fluorescent, and LED fixtures
- True sinusoidal AC pulse width modulated (PWM) design provides clean 60 Hz. emergency output
- Universal 120/277 VAC, 60Hz. input/output
- Unit capacities of 20 to 55 Watts
- "Soft Start" design reduces fixture inrush current
- Surface, recessed or T-Grid mount models
- Lumen output from fixture is 100% of nominal
- Unique design eliminates compatibility problems with LED drivers as well as fluorescent ballasts
- Normally ON, Normally OFF, or switched outputs
- Temperature compensated, dual-mode charger includes low voltage disconnect feature to provide protection against battery deep discharge



- Maintenance-free Lead-Calcium and premium grade Nickel-Cadmium battery models offered
- Control panel with momentary test switch, AC-ON, Charge-ON and Inverter-ON LED indicators
- Battery circuit fuse protected
- Reverse battery and AC lockout protection
- Knockouts in back
- White powder coat finish




 **EVENLITE**  
LIFE SAFETY LIGHTING SOLUTIONS

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TEL: (800) 872 0879 • FAX: (215) 244 4208 • www.evenlite.com

Project name:	Approved By:
Catalog No:	Type No:



 <b>ALASKA ARCHITECTURAL LIGHTING</b>	Project 22-28191-2 <b>SEARHC Juneau Vintage Park</b>	Catalog Number: EMS-55-LC-V3-S	Type <b>SKE</b>
	Submitted By <b>ALASKA ARCHITECTURAL LIGHTING</b>	Note:	

## EMS - Emergency Micro Power Inverters

### Specifications

#### INPUT

Voltage: 120 or 277VAC ± 10%  
 Frequency: 120 or 277VAC ± 10%  
 Protection: Provided by Service Panel, Rated 20A max.

#### OUTPUT

Voltage: 120 or 277VAC (60Hz)  
 Efficiency Rating: 98% at full rate load (line)  
 Waveform: Sinusoidal (digitally controlled, PWM design)  
 Static Voltage: ± 5% during battery discharge. 0-100% linear load  
 Output Frequencies: 60 Hz ± 0.3Hz during emergency cycle  
 Output Distortion: Less than 3% THD (linear load)  
 Transfer Time: Less than 1.0 second  
 Load Power Factor Range: 0.44 Lead to 0.44 Lag  
 Minimum Loading: 0% of rated system capacity  
 Output Protection: Inverter fuse  
 Power Consumption (max): 9W

### Mounting

- **Surface Mount** Surface mount models are designed for mounting to walls by means of keyhole slots provided in the back of the unit housing.
- **Recess Mount** Recess models provide recess mounting holes on both sides of the enclosure.
- **T-Grid Mount** Housing design allows simple drop-in installation between t-grid runs. Safety wires (supplied by others) are required for attachment to building structure.

### Warranty / Listing

- **Unit:** 3 years limited coverage against defects in materials and workmanship from date of shipment.
- **Battery:** Lead-Acid 3 years, one full and two pro-rated  
Nickel cadmium 5 years one full and four pro-rated
- All models are UL924 Listed and meet NFPA 101 Life Safety Code, NEC, OSHA, Local and State Codes. Optional T-Grid models are plenum rated.

### Housing

- Heavy duty steel cabinet has a white powder coat finish providing scratch and corrosion resistance.

### SYSTEM SPECIFICATIONS

Model	System Weight Lbs.	Battery Type	Temp. Range (°C)	DC Input Current (VDC)	Input Current		Thermal Output in BTUs	
					120VAC (max)	277VAC (max)	Standby	Emergency
EMS-32	14.0	Lead-Calc	20-30°	3.4	0.34A	0.15A	7	32
EMS-55	18.0	Lead-Calc	20-30°	5.7	0.54A	0.23A	7	47
EMS-20	11.0	NiCad	0-50°	2.1	0.25A	0.11A	31	22
EMS-35	12.0	NiCad	0-50°	3.8	0.37A	0.16A	31	35

\*System weights show include installed batteries

### ORDERING GUIDE

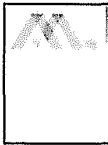
EMS				
Model	VA Rating	Battery Type	Input/Output	Options
EMS	32	LC	V3 120/277	S Surface Mount
	55	LC		RE Recess Mount
	20	NiCad		TB T-Grid Mount
	35	NiCad		

\*System weights show include installed batteries



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	<p>ALASKA ARCHITECTURAL LIGHTING</p> <p>Project 22-28191-2 SEARHC Juneau Vintage Park</p> <p>Submitted By ALASKA ARCHITECTURAL LIGHTING</p>	<p>Catalog Number: EMS-55-LC-V3-S</p> <p>Note:</p>	<p>Type <b>SKE</b></p>
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## EMS - Emergency Micro Power Inverters

### Batteries and Charger

#### BATTERY

**Battery:** Choice of Maintenance Free Sealed Lead Calcium or Sealed Nickel-Cadmium

**Battery Voltage:** 12VDC for all EMS models

**Runtime:** 90 minutes standard. Other runtimes available, consult factory.

**Battery:** Low Voltage Battery Disconnect protects the battery from being severely damaged by deep discharge during prolonged power failures.

DC Overload and Short Circuit Protection provided by a DC input fuse.

Battery voltage (VDC) 12

#### CHARGER

**Charger Type:** Fully automatic, temperature compensated, dual-mode charger

**Power Consumption:** 9W max (All models)

**Recharge Duty Cycle:** Meets UL924 requirements

**Controls:** Momentary test switch, AC-On, Charge-On and inverter-On LED indicator lights

**Safety Circuitry:** AC lockout prevents battery discharge prior to initial unit power-up.

Brownout Protection automatically switches the unit to emergency mode when utility voltage is significantly reduced.

#### ENVIRONMENTAL

**Altitude:** < 10,000 feet (3,000m) above sea level without derating.

**Operating Temperature Range:**  
Lead-Calcium Models: 68°F to 86°F (20°C to 30°C)  
Nickel-Cadmium Models: 32°F to 122°F (0°C to 50°C)

**NOTE:** Optimum system performance between 20°C (68°F) and 30°C (86°F); temperatures outside of the range will affect battery performance and life.

**Relative Humidity:** 95% non-condensing

#### OPERATION

Upon failure of the normal utility power the EMS unit is automatically turned on by a solid state switching circuit and provides a minimum of 90 minutes of emergency power to the connected load. Lumen output will be maintained at 100% of the lamp's rating throughout the entire duration.

A solid state low voltage disconnect circuit is used to protect the battery from being severely damaged by a deep discharge. When normal utility power is restored, the unit switches the load back to normal utility operation and the fully automatic, temperature compensated, dual mode charger begins to restore the battery; bringing it to full charge within UL 924 specified parameters. A brownout sensing circuit insures proper operation during "low line" conditions.

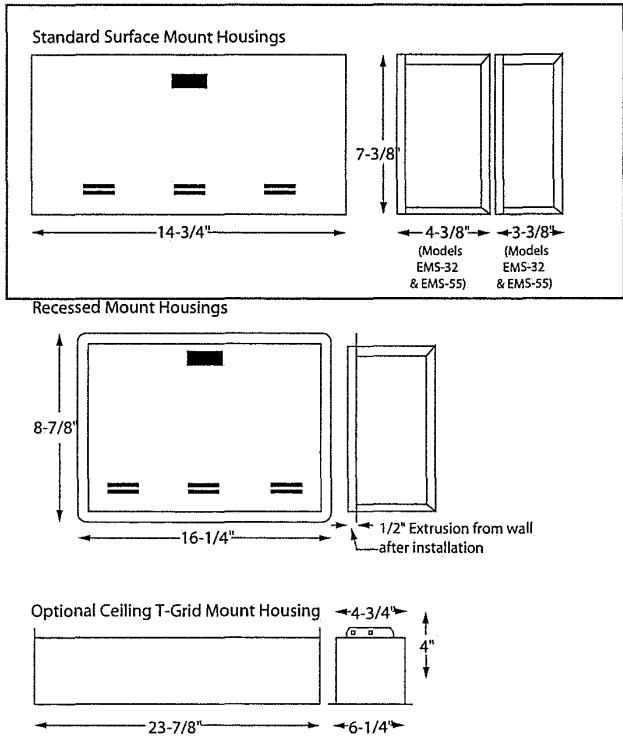
### Improved Aesthetics

The EMS system's sinusoidal AC output design eliminates voltage drop and proximity concerns. This allows added flexibility in installation location as EMS units can be installed hundreds of feet from the units they power. This means EMS units to be located conveniently out of sight in closets or utility rooms without interrupting architectural aesthetics.

### EMS System Advantages


Compared to traditional discrete emergency lighting units, the EMS Series provides emergency illumination from a single power source resulting in lower maintenance overhead and routine testing expenses. EMS units lower installation costs by powering existing lighting fixtures during emergencies. And because connected fixtures are driven at full brilliance, they provide far superior egress lighting and deliver improved occupant safety

### Dimensions



2572 Metropolitan Drive, Treveose, PA 19053 USA  
TEL: (800) 872 0879 • FAX: (215) 244 4208 • www.evenlite.com



 <p>ALASKA ARCHITECTURAL LIGHTING</p>	<p>Project 22-28191-2  <b>SEARHC Juneau Vintage Park</b></p> <p>Submitted By                  ALASKA ARCHITECTURAL LIGHTING</p>	<p>Catalog Number: EMS-55-LC-V3-S</p> <p>Note:</p>	<p>Type  <b>SKE</b></p>
--	---	--	-----------------------------

## EMS - Emergency Micro Power Inverters

### Suggested Specifications

An inverter system with sinusoidal output shall be supplied capable of powering any combination of lighting fixtures, including incandescent, fluorescent, induction and/or LED light sources without compatibility problems.

The system shall transfer in less than 1.0 second to reliably back up lighting fixtures without loss of illumination and operate any and all connected lighting fixtures at full lumen output during the complete 90 minute discharge cycle.

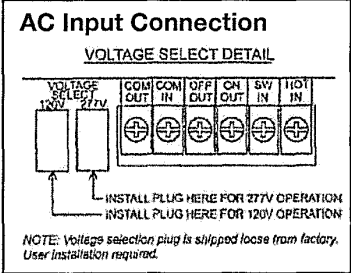
The input voltage shall be the same as the output voltage and shall be single phase 120/127/277 volts, 60 Hz. Output capacity will be 20/35 VA with NiCad Batteries or 32/55 VA with lead Acid Batteries to order for a minimum duration of 90 minutes.

The design shall be a standby, off-line inverter with on-line efficiency of 98%; on-line double conversion UPS systems shall not be considered acceptable alternatives EMS system output shall be a PWM generated sine wave with less than 3% total harmonic distortion. The system shall also provide short circuit and overload protection as standard.

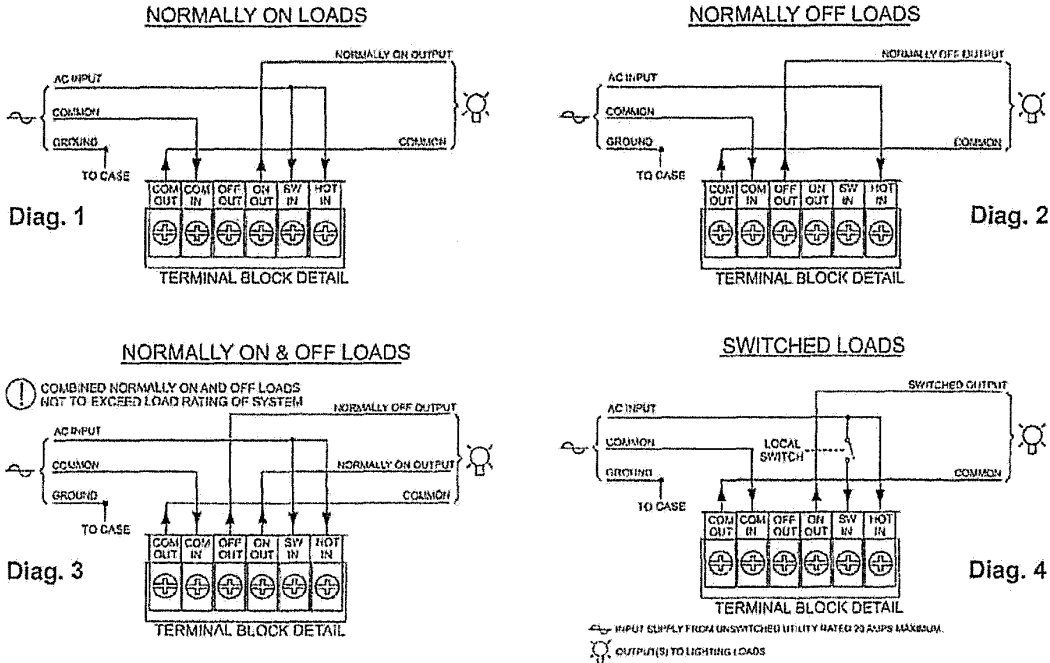
An intuitive three LED display shall provide system operational information at a glance and alert user to any malfunction in system performance. Authorized maintenance personnel shall have access to the system's controls while being protected from any live exposed connections.

Protective devices shall include DC input fuse, AC input overcurrent protection for live circuits to be provided by service panel rated 20A maximum. AC lockout, reverse battery connection, low voltage battery disconnect (LVD), short circuit and overload protection shall be provided standard on all models. The entire EMS system, including batteries, shall be provided in compact cabinetry which shall have provisions or (surface)(recessed)(T-Grid) mounting.

System shall utilize a (sealed lead calcium battery with a 10 year design life)(sealed Nickel-Cadmium battery with a 15 year design life). The charger shall be temperature compensated, dual mode type, and recharge the batteries as per UL 924 guidelines. Entire system shall be tested, approved, and labeled to UL924 Emergency Lighting and Power Systems standards. T-Grid models will be plenum rated.



### Wiring Diagrams



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EMS 10/15

	Project 22-28191-2 SEARHC Juneau Vintage Park	Catalog Number: LE S 1 R EL N SD	Type <b>EXW</b>
	Submitted By ALASKA ARCHITECTURAL LIGHTING	Note:	



**FEATURES & SPECIFICATIONS**

**INTENDED USE** — Ideal for applications requiring attractive die-cast aluminum signage, superior illumination and low energy consumption.

**CONSTRUCTION** — Precision-molded, die-cast aluminum construction — ultra-slim, compact housing. Fine-grain brushed aluminum faceplate with matte black electrostatic polymeric trim. Clear lacquer finish on brushed face inhibits fingerprints and other surface contaminants.

All electronics located inside housing.

Fully overlapping light seal prevents light leaks. Universal directional chevron knockouts are completely concealed and easily removed. Hinged faceplate and spring latches for easy lamp compartment access, no exposed hardware.

Letters 6" high with 3/4" stroke, with 100 ft viewing distance rating, based upon UL924 standards.

U.S. Patent No. 5,739,639, 5,954,423 and 6,502,044. Canada Patent No. 2,204,218. Other patents pending.

**OPTICS** — Lamp is constructed using new LED technology. Provides perfectly uniform illumination to meet 3/4" letter stroke required by code.

The typical life of the exit LED lamp is 10 years, based on continuous operation. Unique LED lamp platform accommodates both single-face and double-face exits.

Low energy consumption — red exit consumes std. 81W, 1.3W (120V), green exit consumes std 1W, 1.5W (120V). Universal input voltage capabilities (120V through 277V, 50 or 60 Hz).

**ELECTRICAL** — Solid-state electronic elements to eliminate risk of electromechanical failures.

Surge protection meets ANSI/IEEE C62.41 category B and IEC 1000 immunity standards for high voltage surges, electrostatic discharges, high frequency electrical fast transients and line voltage dips/swells.

**Emergency Operation (for EL N option only):** Battery: Sealed, maintenance-free nickel-cadmium battery delivers 90 minutes capacity to lamp.

**Self-diagnostics (SD option only):** Two-state constant-current charger maximizes battery life and automatically recharges after battery discharge. Test switch provided for manual testing.

Self-diagnostic testing for five minutes every 30 days, 30 minutes at 180-day interval, and 90 minutes annually.

Diagnostic evaluation of LED light source, AC to DC transfer, charging and battery condition. Continuously monitors AC functionality.

Low voltage disconnect prevents excessive deep discharge that can permanently damage the battery.

Single-point microcomputer control for all electronic features.

Crystal oscillator timing system with watchdog protection for precision accuracy.

AC/LVD reset allows battery connection before AC power is applied and prevents battery damage from deep discharge.

Brownout protection is automatically switched to emergency mode when supply voltage drops below 80% of nominal.

Single multi-chromatic LED indicator to display two-state charging, test activation and three-state diagnostic status.

Test switch provides manual activation of 30-second diagnostic testing for on-demand visual inspection.

Catalog Number
Notes
Type



LE surface



LRE recessed



Die-Cast Aluminum Exits

**LE and LRE**



**INSTALLATION** — Universal mounting (top, end or back). Double face available with top or end mounting only. LRE: Trim ring has 3/4" depth adjustment to ensure a flush fit against the surface. Protrudes 1/16" from the surface. No exposed hardware.

Die-cast aluminum canopy provided for surface mount only.

**LISTINGS** — UL damp location listed 50°F - 104°F (10°C - 40°C). Meets UL 924, NFPA 101 (current Life Safety Code), NEC and OSHA illumination standards. North Carolina Department of Insurance. NEMA Premium certified.

**WARRANTY** — 5-year limited warranty. (Battery is prorated.) Complete warranty terms located at [www.acuitybrands.com/CustomResources/Terms\\_and\\_conditions](http://www.acuitybrands.com/CustomResources/Terms_and_conditions)

Actual performance may differ as a result of end-user environment and application.

All values are design or typical values, measured under laboratory conditions at 25 °C.

Note: Specifications subject to change without notice.


**ORDERING INFORMATION** For shortest lead times, configure products using bolded options.

Example: LE S 1 R EL N SD

Series	Face type	Housing color	Number of faces	Letter color	Input voltage	Operation	Options
LE LED, surface mount	<b>S</b> Stencil P Panel <sup>1</sup>	(blank) Matte black, brushed aluminum face	<b>1</b> Single face <b>2</b> Double face <sup>2</sup>	<b>R</b> Red <b>G</b> Green	(blank) Universal input voltage (120-277V, 50 or 60 Hz)	(blank) AC only EL N Nickel-cadmium battery back-up XZ Lamp wired on two separate AC circuits <sup>3</sup>	(blank) None TP Two tamper proof Torx-head screws VR Vandal-resistant shield (1/8" thick polycarbonate) <sup>4</sup> FI FA Field selectable fire alarm interface or flashing emergency operation with intermittent audible alarm (one flash per minute) <sup>5</sup> FI Fire alarm flashing interface <sup>6</sup> FA Flashing emergency operation and intermittent audible alarm <sup>7</sup> <b>SD</b> Self-diagnostics <sup>7</sup>
LRE LED, recessed		BZ Dark bronze W White B Matte black					

Accessories: Order as separate catalog number.			
ELA US12	12" stem kit (see spec sheet ELA-StemKits) <sup>2,8</sup>	ELA LEHO 120/277 N	Remote-capable exit with black canopy; provides 90 minutes of 11.1W capacity for remote head (see spec sheet ELA-LEHO) <sup>2,4</sup>
ELA WG1	Back-mount wire guard (see spec sheet ELA-WG) <sup>2</sup>	ELA ERK	Recess mounting rough-in kit for LRE only (see spec sheet ELA-ERK)
ELA WGEXT	Top-mount wire guard (see spec sheet ELA-WG) <sup>2</sup>		
ELA WGEXE	End-mount wire guard (see spec sheet ELA-WG) <sup>2</sup>		

- Notes**
- Panel face available for special wording only (see Custom Signage spec sheet).
  - Not available with LRE models.
  - UL Listed as emergency lighting.
  - VR contains tamper proof screws.
  - Available with SD option only.
  - Available with AC only or EL N operation only.
  - Available with EL N option only.
  - Add W for white.

 <p>ALASKA ARCHITECTURAL LIGHTING</p>	<p>Project 22-28191-2 SEARHC Juneau Vintage Park</p> <p>Submitted By ALASKA ARCHITECTURAL LIGHTING</p>	<p>Catalog Number: LE S 1 R EL N SD</p> <p>Note:</p>	<p>Type <b>EXW</b></p>
--	--	--	----------------------------

## LE-LRE LED, Signature

### SPECIFICATIONS

ELECTRICAL				
Primary circuit				
Type	Typical LED life <sup>1</sup>	Supply voltage	Input watts	Max. amps
Red LED AC only	10 Years	120	0.81	0.05
		277	1.2	0.06
Green LED AC only	10 Years	120	1.05	0.05
		277	1.32	0.06
Red LED emergency	10 Years	120	1.3	0.06
		277	1.4	0.07
Green LED emergency	10 Years	120	1.5	0.07
		277	1.7	0.07

BATTERY			
Sealed Nickel-Cadmium			
Shelf life <sup>2</sup>	Typical life <sup>2</sup>	Maintenance <sup>3</sup>	Optimum temperature <sup>4</sup>
3 years	7-9 years	none	50°F – 104°F (10°C – 40°C)

**Notes**

- The typical life of the exit LED lamp is 10 years, based on continuous operation.
- At 77°F (25°C).
- All life safety equipment, including emergency lighting for path of egress must be maintained, serviced, and tested in accordance with all National Fire Protection Association (NFPA) and local codes. Failure to perform the required maintenance, service, or testing could jeopardize the safety of occupants and will void all warranties.
- Optimum ambient temperature range where unit will provide capacity for 90 minutes. Higher and lower temperatures affect life and capacity.

### MOUNTING

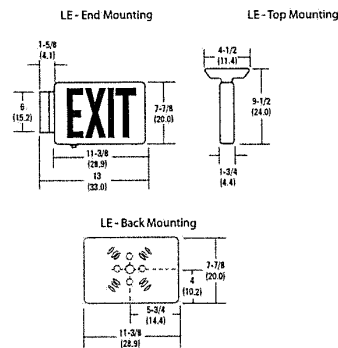
All dimensions are in inches (centimeters). For VR option, add 1/4" to height and width. Add 1/8" depth for single face; 1/4" depth for double face.

Shipping weight: LE - 4 lbs (1.8 kgs)

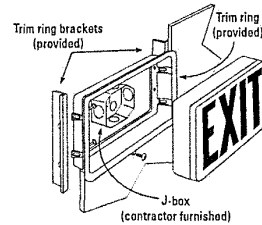
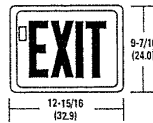
LE EL N - 5 lbs (2.3 kgs)

LRE - 4 lbs (1.8 kgs)

LRE EL N - 5 lbs (2.3 kgs)

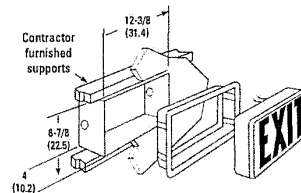


### STANDARD MOUNTING



Wall opening dimensions: 8-3/4" H x 12-3/8" W x 1-3/4" D

### MOUNTING WITH OPTIONAL ROUGH-IN KIT (ELA ERK)



Wall opening dimensions: 8-7/8" H x 12-3/8" W x 4" D

### SELF-DIAGNOSTICS

(SD option only)

- Five-minute test every 30 days
- 30-minute test every six months
- 90-minute test annually
- Diagnostics evaluate the battery, lamp, charger and AC to DC transfer.

Condition	Indication
Normal mode	Steady green
Self-testing	Flashing green
Emergency mode	Off
Hi-charge	Steady red
Battery failure	Single-flash red
Lamp failure	Double-flash red
Circuit failure	Triple-flash red

### KEY FEATURE



The typical life of the exit LED lamp is 10 years.





84-117

CERTIFICATION OF PAYMENT OF TAXES  
AND SPECIAL ASSESSMENTS

I, the undersigned, being duly appointed, qualified Treasurer or Deputy Treasurer for the City and Borough of Juneau, First Judicial District, State of Alaska, do hereby certify that, according to the records of the City and Borough of Juneau, the following described property is carried on the tax records in the name of:

CQ Enterprises/ Vintage Construction Company

Description: Fraction of U.S. Survey 1193

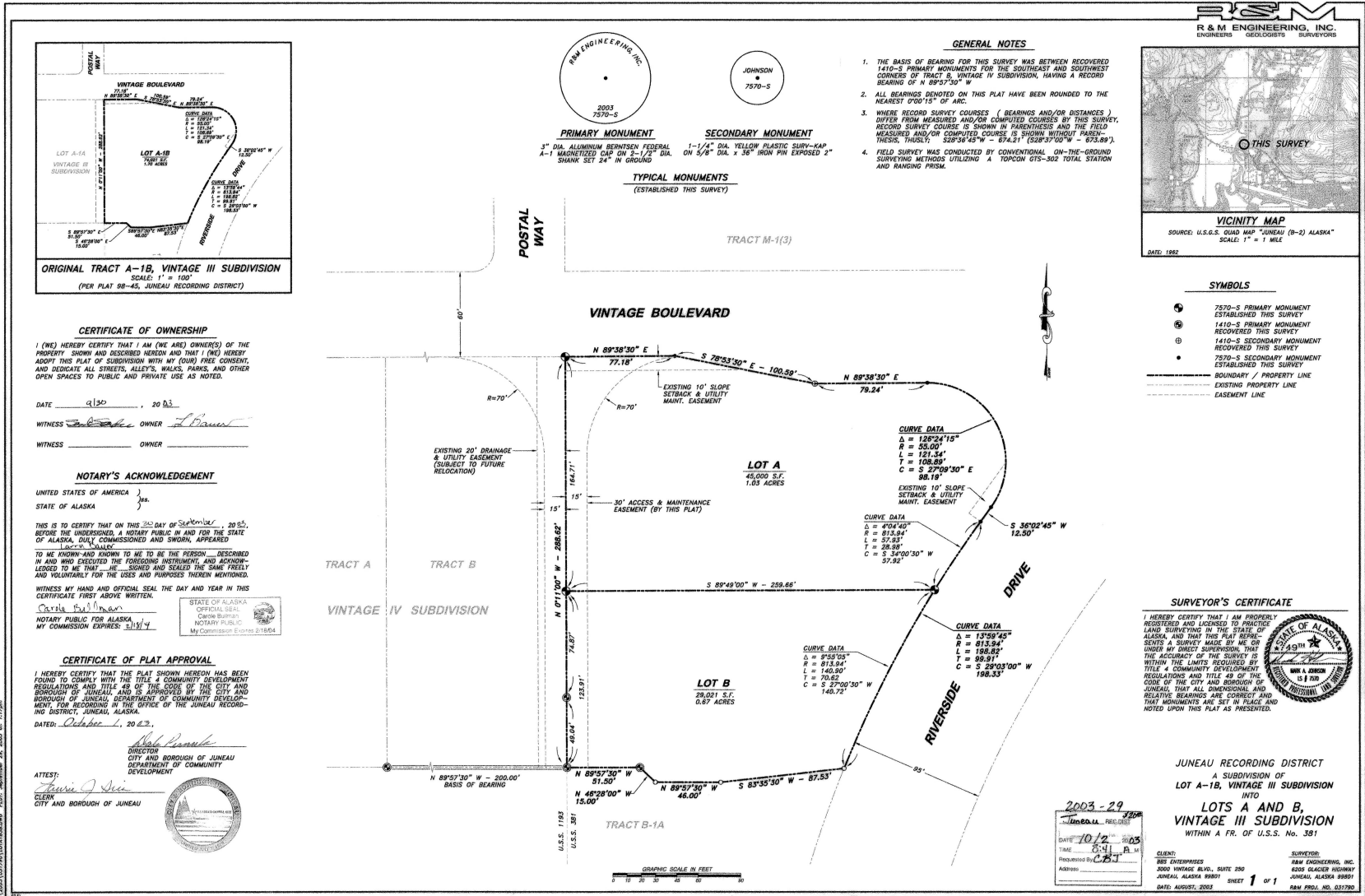
Parcel Code Number: 5-2-022-000-00A-1193

and that, according to the records in my possession, all taxes and special assessments assessed against said lands and in favor of the City and Borough of Juneau are paid in full; that current taxes of the year 1985 will be due on or before 09-30-85, \_\_\_\_\_.

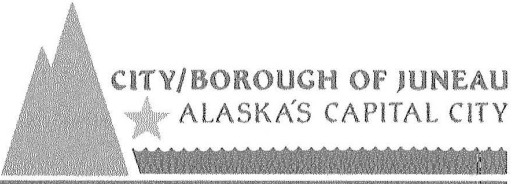
Malcolm Byg  
Deputy Treasurer or Treasurer  
City and Borough of Juneau

84-007122

10.00  
RECORDED-FILED  
JUNEAU REC.  
DISTRICT  
JUL 26 4 24 PM '84  
REQUESTED BY JCEB  
ADDRESS \_\_\_\_\_



Juneau 2003-29



**CERTIFICATION OF PAYMENT OF TAXES**

I, the undersigned, being duly appointed, qualified Treasurer for the City and Borough of Juneau, First Federal District, State of Alaska, do hereby certify that, according to the records of the City and Borough of Juneau, the following described property is carried on the tax records in the name of:

BBS Enterprises  
 \_\_\_\_\_  
**Current Owner**

Vintage 3 L A-1B  
 \_\_\_\_\_  
**Description**

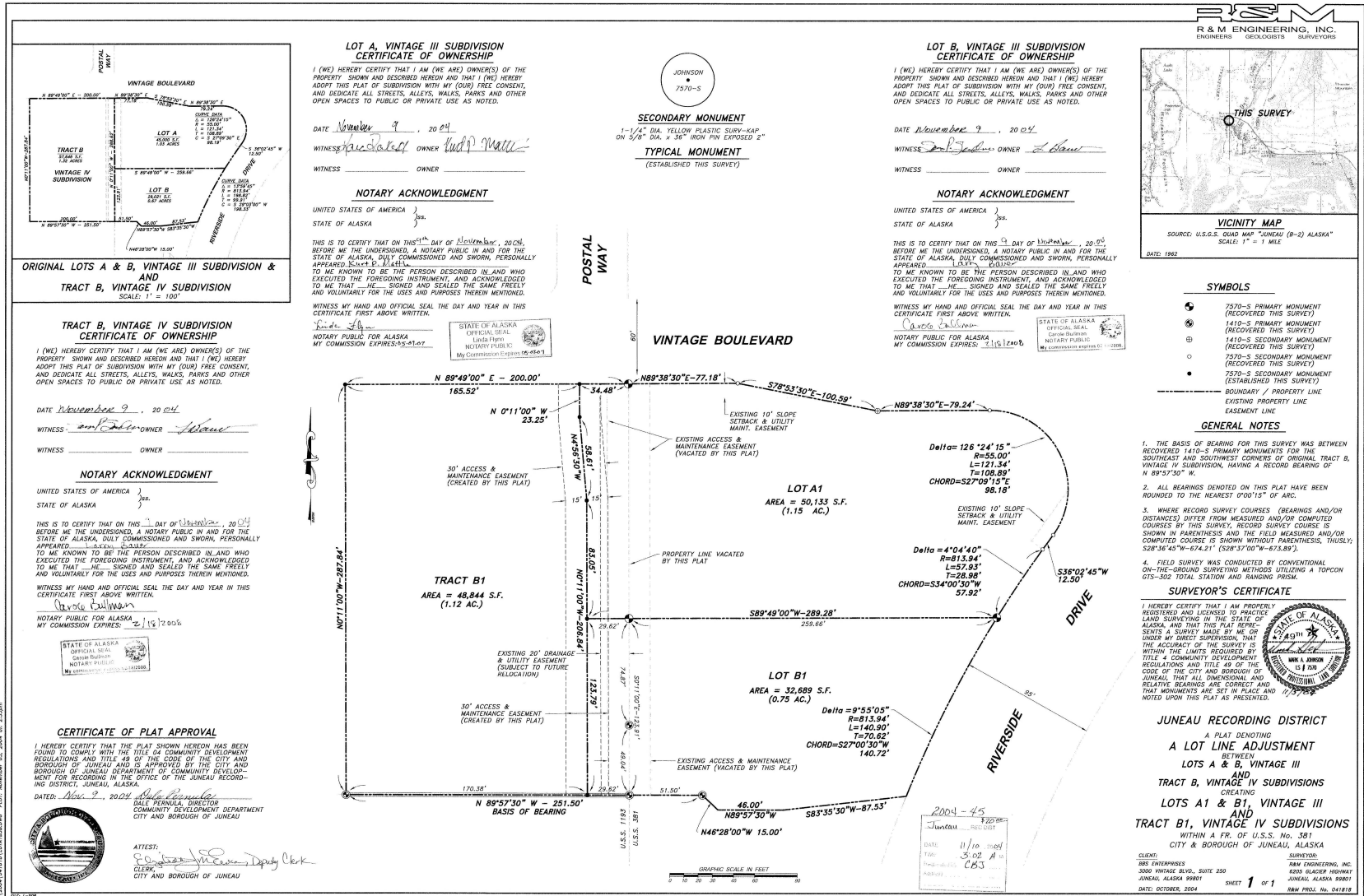
5-B16-0-143-001-2  
 \_\_\_\_\_  
**Parcel Code Number**

and that, according to the records in my possession, all taxes assessed against said lands and in favor of the City and Borough of Juneau are paid in full; that current 2003 taxes due on or before September 30, 2003, have been paid.

*Barbara J. Rolfe*  
 \_\_\_\_\_  
**Barbara J. Rolfe, Treasurer**

September 29, 2003  
 \_\_\_\_\_  
**Date**

155 So. Seward Street, Juneau, Alaska 99801-1397



*Juneau 2004-45*

**CITY/BOROUGH OF JUNEAU  
ALASKA'S CAPITAL CITY**

**CERTIFICATION OF PAYMENT OF TAXES**

I, the undersigned, being duly appointed, qualified Deputy Treasurer for the City and Borough of Juneau, First Federal District, State of Alaska, do hereby certify that, according to the records of the City and Borough of Juneau, the following described property is carried on the tax records in the name of:

BBS Enterprises

Current Owner

VINTAGE 3 L B

Description

5-B16-0-143-001-6

Parcel Code Number

and that, according to the records in my possession, all taxes assessed against said lands and in favor of the City and Borough of Juneau are paid in full; that current 2004 taxes due on or before September 30, 2004, have been paid.



Calvin L. Kubota, Deputy Treasurer

November 8, 2004

Date

*Juneau 2004-45*

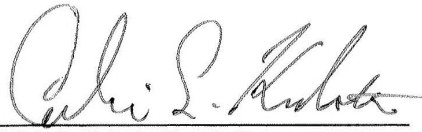
**CITY/BOROUGH OF JUNEAU  
ALASKA'S CAPITAL CITY**

**CERTIFICATION OF PAYMENT OF TAXES**

I, the undersigned, being duly appointed, qualified Deputy Treasurer for the City and Borough of Juneau, First Federal District, State of Alaska, do hereby certify that, according to the records of the City and Borough of Juneau, the following described property is carried on the tax records in the name of:

Dock Street Building Corporation	_____
Current Owner	
VINTAGE 3 L A	_____
Description	
5-B16-0-143-001-5	_____
Parcel Code Number	

and that, according to the records in my possession, all taxes assessed against said lands and in favor of the City and Borough of Juneau are paid in full; that current 2004 taxes due on or before September 30, 2004, have been paid.

  
 \_\_\_\_\_  
 Calvin L. Kubota, Deputy Treasurer  
 November 8, 2004  
 \_\_\_\_\_  
 Date



*Juneau 2004-45*

**CITY/BOROUGH OF JUNEAU  
ALASKA'S CAPITAL CITY**

**CERTIFICATION OF PAYMENT OF TAXES**

I, the undersigned, being duly appointed, qualified Deputy Treasurer for the City and Borough of Juneau, First Federal District, State of Alaska, do hereby certify that, according to the records of the City and Borough of Juneau, the following described property is carried on the tax records in the name of:

Vintage Enterprises  
 \_\_\_\_\_  
 Current Owner

VINTAGE 4 TR B  
 \_\_\_\_\_  
 Description

5-B16-0-143-001-4  
 \_\_\_\_\_  
 Parcel Code Number

and that, according to the records in my possession, all taxes assessed against said lands and in favor of the City and Borough of Juneau are paid in full; that current 2004 taxes due on or before September 30, 2004, have been paid.

*Calvin L. Kubota*  
 \_\_\_\_\_  
 Calvin L. Kubota, Deputy Treasurer

November 9, 2004  
 \_\_\_\_\_  
 Date



# Juneau SEARHC Medical Office Traffic Impact Analysis Final Report

May 2022



Prepared By:  
Randy Kinney, PE, PTOE  
Kinney Engineering, LLC  
3909 Arctic Blvd, Ste 400  
Anchorage, AK 99503  
907-346-2373  
AECL1102

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## Abbreviations

AADT	Annual Average Daily Traffic
AASHTO	American Association of State Highway and Transportation Officials
ASCT	Adaptive Signal Control Technology
CBJ	City and Borough of Juneau
CCS	Continuous Count Station
DOT&PF	Alaska Department of Transportation and Public Facilities
HCM	Highway Capacity Manual
ISD	Intersection Sight Distance
ITE	Institution of Transportation Engineers
KE	Kinney Engineering
LOS	Level of Service (intersection performance grade)
mph	miles per hour
PHF	Peak hour factor
SSD	Stopping Sight Distance
ST	Short term station
RIRO	Right-in-right-out
TIA	Traffic Impact Analysis
V/C	Volume to capacity ratio

N, S, E, W are north, south, east, and west

NB, SB, EB, WB are northbound, southbound, eastbound, and westbound

R, L, T are right, left, through (thru)

RT and LT, right (-) turn(ing) and left (-) turn(ing)

## Definition of Terms

**Access:** Ability to enter and exit a given location from a public roadway.

**Actuated Pedestrian Signal:** Signal, which is activated, usually with a button, by pedestrians and bicyclists.

**Adaptive Signal Control:** Type of traffic signal that uses sensors and algorithms to give green time in a continuous and equitable manner.

**Annual Average Daily Traffic (AADT):** Measurement of the number of vehicles traveling on a segment of highway each day, averaged over the year.

**Capacity:** Value of the maximum flow rate

**Continental Style Cross Walk:** Style of pedestrian crossing which enhances the visibility of pedestrians to drivers. A series of stripes are installed perpendicular to the intersection creating a pattern similar to a zebra or ladder.

**Crash Rate:** Number of crashes per a unit of exposure. Common units of exposure include million vehicle miles traveled for roadway segments and million entering vehicles for intersections.

**Crash Severity:** Scale of bodily harm up to and including death, suffered by the occupants of a vehicle involved in a crash. There are four level of crash severity used: property damage only non-incapacitating/possible injury, incapacitating injury, and fatal.

**Daily Delay:** Sum of delay all drivers undergo in a day, given in vehicle-hours per day.

**Flow:** Rate which describes how many vehicles pass a given point within a set amount of time, usually an hour.

**Interchange:** Set of ramps and intersections used to allow traffic to travel to and from a controlled access freeway facility.

**Level of Service (LOS):** Performance measure concept used to quantify the operational performance of a facility and present the information to users and operating agencies. The actual performance measure used varies by the type of facility; however, all use a scale of A (best conditions for individual users) to F (worst conditions). Often, LOS C or D in the most congested hours of the day will provide the optimal societal benefits for the required construction and maintenance costs

**Mobility:** Ability of people and goods to move from one place to another.

**Off-peak:** Any period of the day which is not the peak hour

**Peak Hour:** Hour long period in which the volume of a given road is the highest for the day or given period. Morning, evening, and noon peak hours are often used for analysis, although peak hours may occur at other times, such as school dismissals.

**Peak Hour Factor:** Measure of traffic variability over an hour period calculated by dividing the hourly flowrate by the peak 15-minute flowrate. PHF values can vary from 0.25 (all traffic for the hour arrives in the same 15-minute period) to 1.00 (traffic is spread evenly throughout the hour).

**Permanent Traffic Recorder:** Vehicle counting device that remains in the same place throughout the years.

**Safety:** Count of crashes by severity at a given location

**Upper Control Limit (UCL):** Statistical measure used in crash rate analysis to determine statistical significance. If the crash rate of the location in question is above the upper control limit for that location, the crash rate is above the average crash rate for similar facilities to a statistically significant level.

**Volume to Capacity Ratio (v/c):** Measure of how much of the available capacity of a facility is being used, calculated by dividing the demand volume by the capacity of a facility. Values of 0.85 or less indicate adequate capacity to serve the demand volume. When v/c is greater than 0.85, drivers begin to feel uncomfortably crowded.

## Executive Summary

*This final report, completed in May 2022, revised the draft report submitted in March 2022. The final report retains computations, calculations, and conclusions, as well as addresses the minor comments or observations resulting in reviews and as described in letter from PND Engineers, Inc. to City and Borough of Juneau Community Development. The final report also includes the most recent 65% site plan, updated from the draft report site plan.*

This Traffic Impact Analysis (TIA) Report addresses the potential traffic impacts of the proposed SEARHC medical office building development to be located along Vintage Boulevard in an area of Juneau listed as Traditional Town Center in the City and Borough of Juneau 2013 Comprehensive Plan Update. The property, shown in Exhibit 1 Figure 3, is located on an empty 2.9-acre lot north of Safeway and across the street from the Post Office. This TIA evaluates the following labeled intersections during the AM and PM peak time period for the build year of 2023.

1. Egan Drive and Vintage Boulevard/Glacier Highway
2. Riverside Drive and Vintage Boulevard/ Mendenhall Mall Road
3. Mendenhall Loop Road and Mendenhall Mall Road/Atlin Drive
4. Mendenhall Loop Road and Egan Drive
5. Riverside Drive and Egan Drive



**Exhibit 1: TIA Study Area**

Traffic counts taken in 2012, 2015 and 2016 were used to determine traffic movements for each of the studied intersections. These counts were factored to the peak month of May in 2023 as a base condition by using the Juneau-Riverside Drive TMAS 000807 continuous count station annual average daily traffic (AADT) and monthly average daily traffic (ADT) data to calculate the estimated annual traffic growth rate as well as factor the counts from the month they were

taken to the peak month. Also, there is a proposed 11 condominium complex with 11 commercial garages development with an estimated ADT of 108 at 3011 Clinton Drive as well as a proposed 98-bed senior housing development at 3041 Clinton Drive with an estimated ADT of 269, both of which are planned for construction in 2022. All traffic generated by these facilities will use Vintage Boulevard and flow through the study area. The Institute of Transportation (ITE) Trip Generation Manual 10<sup>th</sup> edition was used to determine the estimated traffic generated by the proposed medical office building.

The intersection of Egan Drive and Vintage Boulevard/Glacier Highway is a stop controlled, right-in, right-out intersection with minor differences in queue length in the PM peak hour, there is no change in the AM peak hour and no change to level of service (LOS) with the additional site traffic. No mitigation is required by the CBJ Code of Ordinances.

The intersection of Riverside Drive and Vintage Boulevard/ Mendenhall Mall Road is a four-way signalized intersection with one left, thru and right turn lane in each direction. There are pedestrian signals and crosswalks across all four approaches of the intersection. The left turns phases on all approaches are permissive-protected. With the additional site traffic, there is a slight increase in the queue lengths for some of the approach lanes and a one second delay increase in the AM and PM peak hours. There is no LOS change. No mitigation is required by the CBJ Code of Ordinances.

Mendenhall Loop Road and Mendenhall Mall Road/Atlin Drive is a four-way signalized intersection. The northbound approach has a thru lane, a thru/right turn lane and a left turn lane. The southbound approach has two thru lanes, a left turn lane and a right turn lane. The westbound approach has a shared thru/left turn lane and a right turn lane. The eastbound approach has a shared thru/left turn lane and a right turn lane. The northbound and southbound left turn phases are protected. The eastbound and westbound left turn phases are permissive. There are pedestrian signals and crossings across three of the four approaches. There is a 1 second delay change and no LOS change to the AM peak hour and a 6 second delay change and a LOS change from C to D for the entire intersection for the PM peak hour. No mitigation is required by the CBJ Code of Ordinances.

The intersection of Egan Drive and Mendenhall Loop Road is a four-way signalized intersection with left, thru and right turn lanes. The left turns are all protected split phase. There are pedestrian signals and crossings across all four approaches of the intersection. The LOS for this intersection is F for the AM peak hour and E for the PM peak hour both for pre-build and build conditions. The site traffic impact is minimal with a one second increase for both the AM and PM peak hours. There is no LOS change for the intersection with the additional traffic. The CBJ Code of Ordinances requires mitigation for this intersection because it is currently operating below a level of service of D. However, the site traffic has a negligible impact on the operations of the intersection and the CBJ Code of Ordinances provides conditions for mitigation waiver.

Riverside Drive and Egan Drive is a signalized T-intersection with two thru eastbound and westbound lanes, westbound left turn phasing, eastbound channelized right turn and two southbound left turn lanes and one right turn lane. There are pedestrian signals and crosswalks on two of the three approaches at this intersection. There are no changes to the LOS or queue delays for this intersection caused the proposed site traffic. No mitigation is required by the CBJ Code of Ordinances.

In addition to intersections, site impacts were evaluated for transit, pedestrians and bicycles, and traffic safety.

Mitigations for impacts include the following:

- CBJ Code of Ordinances would require mitigation of the Egan Drive and Mendenhall Loop Road intersection as it has a LOS less than D prior to the additional site traffic. Reconstruction would include require expansion of northbound and southbound approaches, and cost millions of dollars. However, the site traffic has only a negligible impact on operations, and a mitigation waiver may be granted due to the excessive cost. Moreover, the adaptive signal control at this intersection will provide optimization of timing and serve as a mitigation.
- There is a missing sidewalk segment on Vintage Boulevard directly fronting the SEARHC medical office site. Installation of the sidewalk improves pedestrian mobility and safety impacts and would promote walking and transit as alternative commuting modes, thus reducing site vehicular demand. Note that this improvement is now presented in the 65% Design Site Plan (May 2022).
- Demand management policies may promote mode changes from vehicles to active transportation modes (biking, walking, transit) or reducing demand by promoting carpools. These include:
  - Incentivize transit use (potential 4% to 5% vehicle trip reduction, see Section 3.4)
  - Incentivize carpooling (potential 14% to 15% vehicle trip reduction, see Section 3.4)
  - Provide bicycle racks. (Vehicle trip reduction isn't quantifiable).

DOT&PF and CBJ have reviewed this report when in draft. CBJ had no comments. DOT&PF's comments are addressed in a summary letter that is under Appendix G. CBJ and DOT&PF have concurred with the above stated mitigations.



# 1 Introduction and Development Background

*This final report, completed in May 2022, revised the draft report submitted in March 2022. The final report retains computations, calculations, and conclusions, as well as addresses the minor comments or observations resulting in reviews and as described in letter from PND Engineers, Inc. to City and Borough of Juneau Community Development. The final report also includes the most recent 65% site plan, updated from the draft report site plan.*

## 1.1 Development Description

Southeast Alaska Regional Health Consortium (SEARHC) proposes a 45,924 square-foot gross floor area (GFA, area as of March 2022) medical office facility located on Vintage Boulevard between the intersections with Riverside Drive and Egan Drive in Juneau, Alaska. The facility is located on a 2.9-acre empty tract with access only by Vintage Boulevard. The vicinity map is presented in the Figure 2.



Background Photo Source- Google Earth

**Figure 2: SEARHC Medical Office Location Map**

## 1.2 City and Borough of Juneau Traffic Impact Analysis Requirements

City and Borough of Juneau (CBJ) Code of Ordinances, 49.40.300 Applicability, states that a traffic impact analysis (TIA) shall be required when a development is projected to generate 500 or more average daily trips (ADT).

The proposed SEARHC medical office facility aligns with the Institute of Transportation Engineer’s (ITE) Trip Generation Manual Land Use 720- Medical/Dental Office Building. Based on the gross floor area of 45,924 gross floor area, the Trip Generation Manual, and the companion Trip Generation Handbook 3<sup>rd</sup> Ed. estimates that the traffic generated upon full build out of the facility exceeds 1,600 ADT, as shown in Table 1, below. As such, a TIA for the facility is required.

**Table 1: Weekday ADT for Proposed SEARHC Medical Offices**

Land Use Classification	Gross Floor Area (1000 square feet)	Equation Type	Equation and Trip Computations	DD% (In/Out)	Trips In	Trips Out
720 – Medical-Dental Office Building	X=45.924	Regression	T=38.42(X) – 87.62 T=38.42(45.924) – 87.62= <b>1,677</b>	50/50	838	838

The street system around the facility is urbanized with movements through intersections controlled by a signs or signals. As such, it is within an interrupted flow regime, and traffic operation quality is generally dependent upon intersection operations and performance. For this TIA, vehicular impact analyses are focused on intersections.

CBJ Code of Ordinances, 49.40.305 Traffic impact analysis (TIA) requirements indicates that the study area for the TIA shall include those intersections with approaches where the proposed development will increase ADT by five percent or more. Required mitigation for intersections within the study area is addressed by CBJ Code of Ordinances as follows:

- CBJ Code of Ordinances, 49.40.310 Traffic; minimum standards.
  - (a) *The minimum acceptable (level of service) LOS for a roadway segment or intersection within the area affected by the development, on the projected opening date of the development, or full build out of the development, is LOS D.*
- CBJ Code of Ordinances, 49.40.340 Traffic impact mitigation.
  - (a) *Except as provided in 49.40.340, an applicant shall make improvements to a roadway or intersection to achieve or maintain an acceptable LOS if a roadway or intersection has an:*
    - (1) *LOS D without traffic generated by the development; and would drop below LOS D with traffic generated by the development at the opening date of the development or full build out;*
    - (2) *If a roadway has an LOS below D without traffic generated by the development at the opening date of the development; or*



(3) *If the intersection or roadway segment has a pattern of accidents resulting in personal injuries, and the development would aggravate this accident pattern, then mitigation shall be required regardless of the LOS.*

The term “level of service” or LOS is defined in Section 7.1 on page 39 and Table 14 on page 41.

In a July 2021 meeting between Randy Kinney, Kinney Engineering, LLC (KE) and Allison Eddins from CBJ’s Community Development Department, the study area presented in Figure 3 was discussed. This study area was later presented in a second meeting focusing on traffic in January 2022 that included CBJ and the Department of Transportation and Public Facilities (DOT&PF) staff; and address a wider range of traffic impact issues. In addition, there have been two pre-application meetings with CBJ, but these meeting only addressed traffic issues in general.



Background Photo Source- Google Earth

**Figure 3: TIA Study Area**

As proposed in these meetings, an initial analysis of impacts would include the following intersections (numbered as shown in the figure):

1. Egan Drive and Vintage Boulevard/Glacier Highway
2. Riverside Drive and Vintage Boulevard/ Mendenhall Mall Road
3. Mendenhall Loop Road and Mendenhall Mall Road/Atlin Drive
4. Mendenhall Loop Road and Egan Drive
5. Riverside Drive and Egan Drive

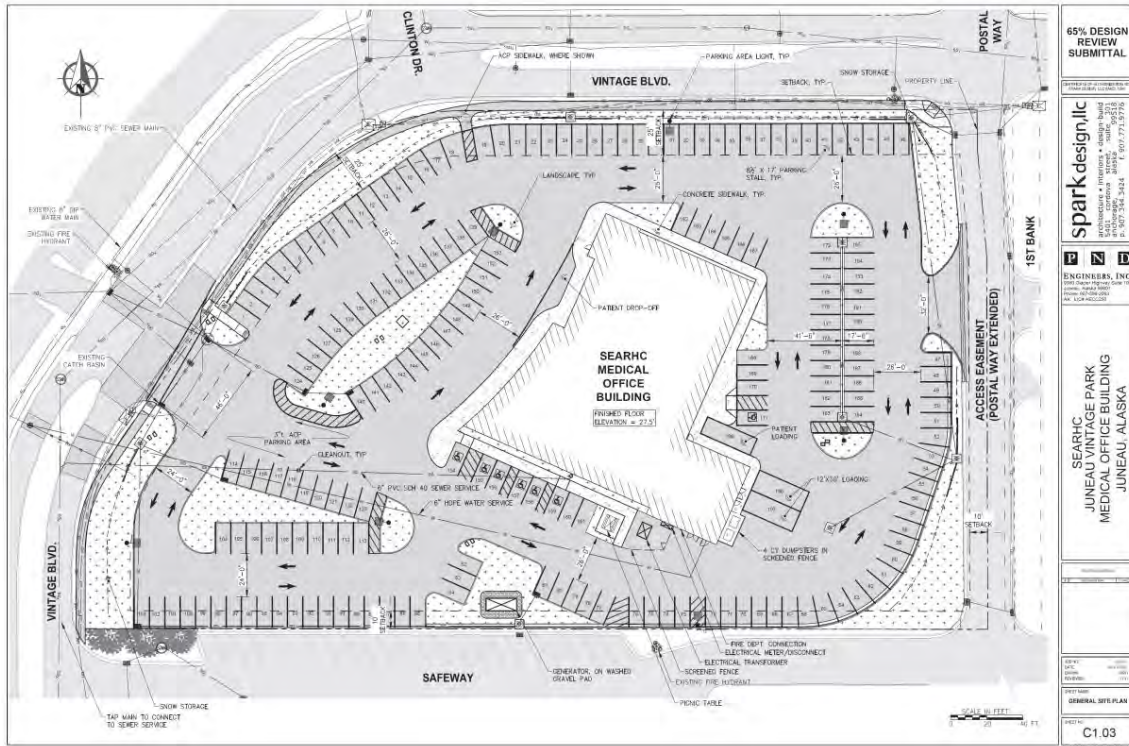
This starting point was deemed sufficient although it preceded the detail analysis that would form a final study area required by CBJ Code of Ordinances, 49.40.305. The process for trip generation and distribution of site traffic is an iterative process, and thus cannot be completed without a substantial effort. However, in meeting discussions, KE indicated that adding any intersections (expanding the study area) would occur as the analysis progressed to evaluate and confirm the link ADT thresholds in CBJ Code of Ordinances, 49.40.305.

The completion of the analyses of site traffic distribution to other intersection approaches outside of the study area found all others be under the 5% threshold discussed above. As such, the study area shown satisfies CBJ Code of Ordinances.

Ultimately, KE chose to prepare operational and safety analysis for all five of the listed intersections with and without site traffic, irrespective of the 5% ADT increase requirements in CBJ Code of Ordinances, 49.40.305.

### 1.3 Site Plan

Figure 4 below presents the conceptual site plan (current as of 2-22-22) showing parking layout, access aisle, and site access locations. There will be minor changes to this plan as the design develops.



Source- PND Engineers Inc.

Figure 4: Proposed Site Plan (revised May 2022)



The site plan presents two access points onto Vintage Boulevard, both of which are existing and established as shown in Figure 5 below. Both of these have auxiliary left-turn lanes, a significant benefit for crash prevention and roadway capacity by removing stopped or slowing left turn cars from the through lane.



Background Photo Source- Google Earth

**Figure 5: Existing Site Conditions**

## 1.4 Design Year and Design Hour

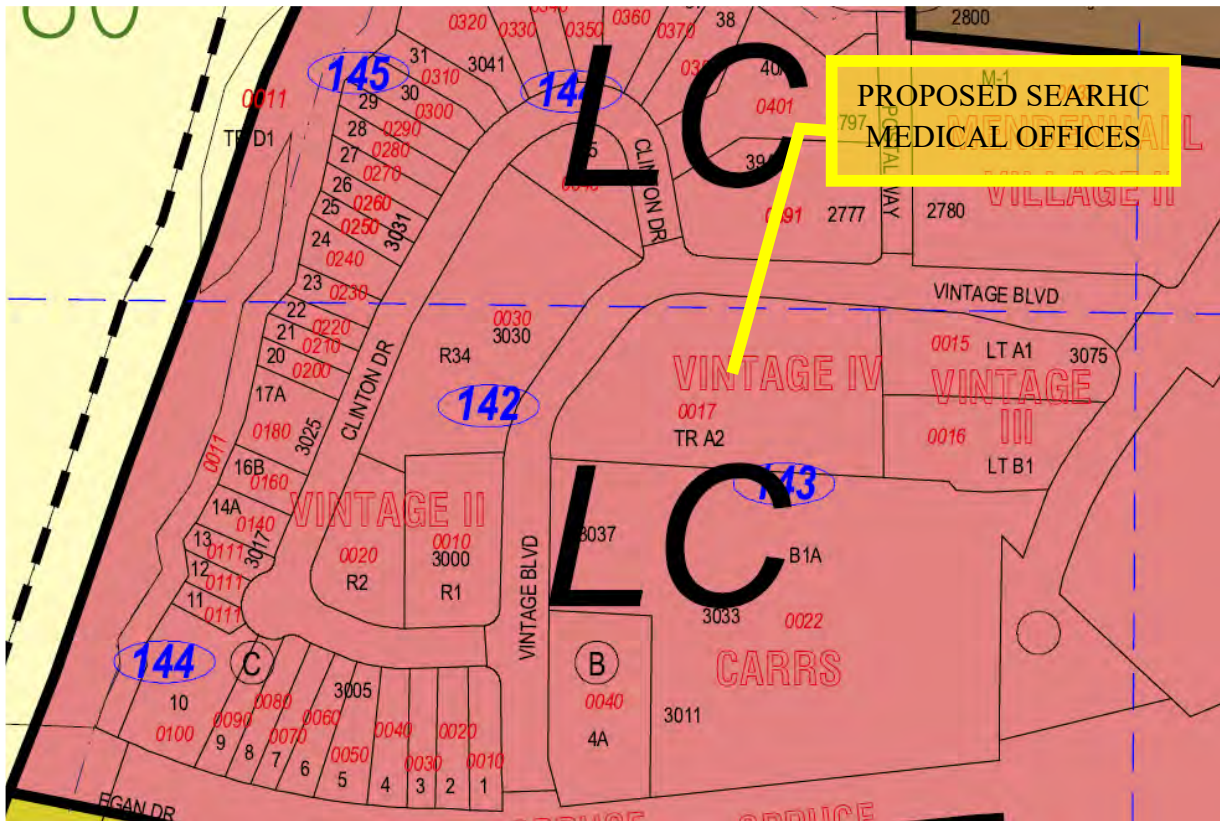
The SEARHC medical office facility will be completed in 2023. As agreed upon in the traffic meetings with CBJ and DOT&PF, 2023 will be considered the design year for the analysis. The design hours for the facility will include morning and evening commuting peak hours. Additional analyses period for weekend traffic was discussed and dismissed.

Because of seasonal traffic fluctuations, KE decided to further define the design hours as those expected during the summer season instead of an average condition.

## 2 Project Area Background

### 2.1 Surrounding Land Zoning

The proposed SEARHC medical office facility is located within areas zoned Light Commercial, or LC. The zoning in the immediate area is presented in the Figure 6.



Source-Zoning Atlas City and Borough of Juneau

**Figure 6: Area Zoning**

The CBJ Code of Ordinances, 49.25.230 - Commercial districts states:

*“The LC, light commercial district, is intended to accommodate commercial development that is less intensive than that permitted in the general commercial district. Light commercial districts are primarily located adjacent to existing residential areas. Although many of the uses allowed in this district are also allowed in the GC, general commercial district, they are listed as conditional uses in this district and therefore require commission review to determine compatibility with surrounding land uses. A lower level of intensity of development is also achieved by stringent height and setback restrictions. Residential development is allowed in mixed- and single-use developments in the light commercial district.”*

The proposed SEARHC medical office facility is consistent with the existing zoning. As such, no changes in zoning will be required.



## 2.2 Surrounding Land Uses And Site Land Use

The adjacent, existing development is largely commercial. A Safeway grocery store and related shopping center businesses, including an automotive fuel station, are located directly south of the site. Other immediate or nearby land uses include a bank, credit union, US Post Office, and numerous private and agency offices. Residential developments are also nearby.

The City and Borough of Juneau 2013 Comprehensive Plan Update indicates that the site is within a Traditional Town Center, described as follows:

*"These lands are characterized by high density residential and non-residential land uses in downtown areas and around shopping centers, the University, major employment centers and public transit corridors, as well as other areas suitable for a mixture of retail, office, general commercial, and high-density residential uses at densities at 18 or more residential units per acre. Residential and non-residential uses could be combined within a single structure, including off-street parking. Ground floor retail space facing roads with parking behind the retail and housing above would be an appropriate and efficient use of the land."*

The proposed medical office facility is consistent with the adjoining development and Tradition Town Center land use.

## 2.3 Traffic Improvements Already Funded, Programmed, Or Planned

A survey of agency websites found the following committed or planning projects within the immediate area of the proposed facility.

### 2.3.1 CBJ Capital Improvement Program 2021 to 2026

This six-year plan shows a total of about \$1.5 million planned for Vintage Boulevard Street Reconstruction in FY23. A scan of the document found no other CBJ projects planned for the immediate area.

### 2.3.2 DOT&PF State Transportation Improvement Program (STIP)

The April 2020 Original STIP and Amendment 3 issued in November 2021 have been scanned with no projects planned in the immediate area of the proposed facility.

### 2.3.3 Juneau Egan Drive & Riverside Intersection Improvements

KE completed this study in the spring of 2016 for the DOT&PF. The study focused on the Egan Drive and Riverside Intersection and prepared an analysis of several signalized and roundabout alternatives, including extending Riverside Drive to the south of Egan Drive. The proposed medical offices do not affect the recommendations of this study. Moreover, if any of the alternatives were to be implemented, site travel patterns and routes would not be changed over the current system travel patterns.

This traffic impact analysis uses morning and evening peak hour intersection traffic count data from this study. The 2020-2022 pandemic has depressed travel, meaning that current counts may be underrepresenting actual or future demands. As such the counts in the 2016 study were the base of this analysis and factored to estimate current and future demand conditions.

### **2.3.4 West Egan Drive Corridor Study**

This study, completed in 2003 for DOT&PF, proposes grade separation interchanges for the Egan Drive intersections with Mendenhall Loop Road and with Riverside Drive. It is not clear if Vintage Drive inbound and outbound access from Egan Drive would be maintained as is now, because Vintage Boulevard connects to the Riverside Interchange westbound on-ramp.

Regardless of the Vintage Boulevard connection to Egan Drive, the proposed SEARHC medical offices would be compatible with the improvements proposed in the West Egan Drive Corridor Study.

## **2.4 Other Committed Projects**

CBJ Community Development planners provided additional developments that are or will be completed prior to the design year 2023, but whose traffic was not captured during the intersection counts that provide the basis of background traffic (see Section 4.3 on page 23).

There is a proposed 11 condominium complex with 11 commercial garages development with an estimated ADT of 108 at 3011 Clinton Drive. Also, there is a proposed 98-bed senior housing development at 3041 Clinton Drive with an estimated ADT of 269 planned for construction by 2022. All traffic generated by these facilities will use Vintage Boulevard (Both ends of Clinton Drive connects to Vintage Boulevard) and flow through the study area.

### 3 Transportation System Elements

The traffic study area for this report, shown in Figure 3 on page 6, consist of five intersections and the connecting streets. The streets are discussed in more detail in Section 3.1. There are four signalized intersections (intersections 2-5) and one stop-controlled intersection (intersection 1), which are discussed further in Section 3.2.

#### 3.1 Streets

Egan Drive is a principal arterial roadway which primarily serves to carry traffic between downtown Juneau, the Mendenhall Valley, Auke Bay, and other outlying areas of the CBJ. It is Route 4 on the National Highway System, connecting the Juneau International Airport and the Auke Bay Ferry Terminal to other areas of the CBJ. Between downtown Juneau and the Mendenhall Valley, Egan Drive is a four-lane divided highway with a posted speed limit of 50 mph.

Riverside Drive is a two-lane minor arterial roadway that connects Egan Drive with businesses, residences, schools, and community centers in the Mendenhall Valley. Riverside Drive is posted at 35 mph.

Mendenhall Loop Road is a minor arterial roadway with two lanes and a center turn lane south of Egan Drive, four lanes with a designated left turn lane between Egan Drive and Mendenhall Mall Road, and four lanes north of Mendenhall Mall Road to Nancy Street. Mendenhall Loop Road has a posted speed limit of 40 mph.

Mendenhall Mall Road is a two-lane local access roadway with no curbing connecting Riverside Drive and Mendenhall Loop Road, serving as access to the businesses on both sides of the roadway. Mendenhall Mall Road has a posted speed limit of 20 mph.

Atlin Drive is a two-lane local access roadway that provides access to a church and residential neighborhood with a posted speed limit of 20 mph.

Vintage Boulevard is a two-lane divided local access roadway connecting Egan Drive and Riverside Drive that provides access to businesses and residences. Vintage Boulevard posted speed limit is 20 mph.

Glacier Highway is a two-lane minor collector roadway from Egan Drive/Vintage Boulevard to Mendenhall Loop Road with a right turn only at the intersection with Egan Drive. Glacier Highway is posted at 35 mph.

##### 3.1.1 Functional Classification

The three major functional classes include:

- **Local Road** - Local roads are oriented towards access to homes and businesses at the terminal ends of a trip.
- **Collector** - Collector roads gather and distribute trips between local streets and arterials.

- **Arterial** - Arterials emphasize mobility and are designed to carry large volumes at an efficient speed.

Table 2 presents the DOT&PF functional classifications for the roads in the study area.

<b>Roadway</b>	<b>Classification</b>
Egan Drive	Principal Arterial
Glacier Highway (Vintage Boulevard to Mendenhall Loop Road)	Minor Collector
Mendenhall Loop Road	Minor Arterial
Riverside Drive	Minor Arterial
Vintage Boulevard	Major Collector
Atlin Drive	Local Road
Mendenhall Mall Road	Local Road

**Table 2: Functional Classifications**

### 3.2 Intersections

The study area contains four signalized intersections and one stop-controlled intersection. The following four signals are coordinated:

- Egan Drive and Riverside Drive
- Egan Drive and Mendenhall Loop Road
- Riverside Drive and Vintage Boulevard/ Mendenhall Mall Road
- Mendenhall Loop Road and Mendenhall Mall Road/Atlin Drive

The intersections are discussed in the following sections. Most of the information presented in these sections, including the diagram with background aerial photography was extracted from the 2016 Juneau Egan Drive & Riverside Intersection Improvements Study.

### 3.2.1 Egan Drive / Riverside Drive



The intersection of Egan Drive with Riverside Drive is a signalized T-intersection (Figure 7). On westbound Egan Drive, the approach to the signal has two through lanes and a channelized right turn lane. For eastbound Egan Drive, the approach has two through lanes and a left turn lane/U-turn, with protected left turn phasing. The Riverside Drive approach has two left turn lanes and one right turn lane. There are also pedestrian signals and crosswalks across the inbound Egan Drive and Riverside Drive approaches; however, there are no pedestrian crossings of the outbound Egan Drive approach.

Figure 7: Egan Drive and Riverside Drive

### 3.2.2 Egan Drive / Mendenhall Loop Road



The intersection of Egan Drive and Mendenhall Loop Road (Figure 8) is a four-way signalized intersection. The southbound approach has a right turn, left turn and center shared left turn/through lane. The southbound approach consists of a left turn, a center shared left/through, and a right turn lane. The eastbound approach has two through lanes, a right turn lane and a left turn lane. The westbound approach also has two through lanes, a channelized right turn lane and a left turn lane. All left turns have protected left turn split phasing. There are pedestrian signals and crosswalks across all four approaches of the intersection.

Figure 8: Egan Drive and Mendenhall Loop Road



### 3.2.3 Riverside Drive / Vintage Boulevard / Mendenhall Mall Road



The intersection of Riverside Drive with Vintage Boulevard and Mendenhall Mall Road (Figure 9) is a four-way signalized intersection. Each approach has a left turn, a through lane and a separated right turn lane. The left turns phases on all approaches are permissive-protected. There are pedestrian signals and crosswalks across all four approaches of the intersection.

Figure 9: Riverside Drive and Vintage Boulevard/Mendenhall Mall Road

### 3.2.4 Mendenhall Loop Road / Mendenhall Mall Road / Atlin Road



The intersection of Mendenhall Loop Road with Mendenhall Mall Road and Atlin Road (Figure 10) is a four-way signalized intersection. The northbound approach has two through lanes and a left turn lane. The southbound approach has two through lanes, a left turn lane and a right turn lane. The westbound approach from Atlin Road has a shared through/left turn lane and a right turn lane. The eastbound approach from Mendenhall Mall Road has a shared through/left turn lane and a right turn lane. The northbound and southbound left turn phases are protected. The eastbound and westbound left turn phases are signed as permissive. There are pedestrian signals and crosswalks across the southbound, eastbound, and westbound approaches to the intersection. There is no pedestrian crossing allowed on the northbound approach.

Figure 10: Mendenhall Loop Road and Mendenhall Mall Road/Atlin Drive



### 3.2.5 Egan Drive / Vintage Boulevard / Glacier Highway



The intersection of Egan Drive and Vintage Boulevard/Glacier Highway (Figure 11) is stop controlled with right-in-right-out restricted movements on the north and southbound approaches. The eastbound approach along Egan Drive has two through lanes and a right turn lane, while the westbound approach has two through lanes, a left turn lane and a right turn lane. There are unsignalized pedestrian crosswalks along the northbound and southbound approaches.

Figure 11: Egan Drive and Vintage Boulevard/Glacier Highway

## 3.3 Pedestrian and Bicycle Facilities

The CBJ has been named a bronze level bicycle friendly community since 2011 and has also received honorable mention as a walk friendly community.

### 3.3.1 Pedestrian Facilities

The pedestrian facilities for the main streets in the study area include:

- A separated multiuse pathway on the north side of Egan Drive;
- Continuous attached pavement pathways on both sides of Riverside Drive;
- Separated paved pathways on both sides of Mendenhall Loop Road, and;
- A separated sidewalk on the west and north sides of Vintage Boulevard and a discontinuous separated sidewalk on the east and south side of Vintage Boulevard.

There is no sidewalk immediately fronting the parcel that will contain the proposed SEARHC medical facilities. Also, there are no sidewalks on Mendenhall Mall Road and pedestrians are required to use the roadway shoulders or parking lots.

There are push-button actuated pedestrian signals and crosswalks at all the signalized intersections in the study area, with pedestrians prohibited from crossing the inbound approach of the intersection of Egan Drive and Glacier Highway intersection, the outbound approach of the Egan Drive/Riverside Drive intersection, and the northbound approach of the Atlin Road/Mendenhall Mall Road/Mendenhall Loop Road intersection.

**3.3.2 Bicycle Facilities**

Riverside Drive has bicycle lanes. On other streets, bicyclists ride in the travel lanes, on shoulders or on pathways where provided.

**3.4 Transit**

The proposed Medical Center will be located in close proximity to 10 CBJ Capitol Transit routes. Boarding and alighting for all routes would be within ¼- to ½-mile walking distance to and from the site, depending upon walking patterns of the transit patron. A survey of the state of the practice on acceptable walking distances indicate that most people are willing to walk ¼- to ½- miles to a transit stop (see FHWA’s Pedestrian Safety Guide for Transit Agencies [https://safety.fhwa.dot.gov/ped\\_bike/ped\\_transit/ped\\_transguide/index.cfm#toc](https://safety.fhwa.dot.gov/ped_bike/ped_transit/ped_transguide/index.cfm#toc)).

Table 3 summarizes, in general, nearby Capital Transit routes and their service frequency daily and during commuting peak hours.

**Table 3: Capital Transit Routes and Service Near Proposed SEARHC Medical Office Facility**

Route	Service	Nearest Bus Stop	Morning Commute Peak (7 AM to 9 AM)		Evening Commute Peak (4 PM to 6 PM)	
			Headway	Buses	Headway	Buses
3 Mendenhall Loop Counterclockwise	Monday-Saturday, Early Commute to Late Evening Sunday 8 AM hour through 6 PM Hour	431- Mendenhall Mall Road and Riverside Drive	60 minutes	2	60 minutes	2
4 Mendenhall Loop Clockwise	Monday-Saturday, Early Commute to Late Evening Sunday 8 AM hour through 6 PM Hour	477- Mendenhall Mall Road at Mendenhall Mall	90-60 minutes	2	60 minutes	2
5 University Express	Monday-Friday, 7 AM Hour through 6 PM Hour	431- Mendenhall Mall and Riverside 477- Mendenhall Mall Road at Mendenhall Mall	60 minutes	4	60 minutes	4
6 Riverside Express	Monday-Friday, 8 AM Hour through 6 PM Hour	477- Mendenhall Mall Road at Mendenhall Mall	60 minutes	1	60 minutes	2
8 Valley Express	Monday-Friday, 2 PM Hour through 5 PM hour	431- Mendenhall	NA	NA	30 minutes	2

Route	Service	Nearest Bus Stop	Morning Commute Peak (7 AM to 9 AM)		Evening Commute Peak (4 PM to 6 PM)	
			Headway	Buses	Headway	Buses
		Mall Road and Riverside Drive				
9 Mendenhall Valley Express	Monday-Friday, 7 AM Hour	477-Mendenhall Mall Road at Mendenhall Mall	NA	1	NA	NA
10 Mendenhall Valley Commuter	Monday-Friday, 6 AM and 7 AM Hours	477-Mendenhall Mall Road at Mendenhall Mall	NA	1	NA	NA
14 Mendenhall Riverside Commuter	Monday-Friday, 6 AM Hour	On Street Riverside at Vintage	NA	1	NA	NA
15 Valley/UAS Express	Monday-Friday, 7 AM and 8 AM Hours	477-Mendenhall Mall Road at Mendenhall Mall	NA	1	NA	NA
16 Taku Express	Monday-Friday, 6 AM Hour	On Street Riverside at Vintage	NA	1	NA	NA
				14		
					12	

Source: [juneaucapitaltransit.org](http://juneaucapitaltransit.org)

As indicated in the table above, there are 14 buses providing morning commuting service and 12 buses providing evening commuting service, making public transit a viable work trip alternative for employees. In addition, there is consistent and continuous service during business hours and on weekends, with at least one bus per hour in each inbound and outbound direction for patients to use instead of driving individual cars.

The US Census Bureau American Community Survey *Table S0802 Means Of Transportation To Work By Selected Characteristics* for 2013 through 2019 (5 most recent years published) indicates that the average public transportation work trip mode share is about 4.6% of commuting trips. (Source of this data is: <https://data.census.gov/cedsci/table?q=Juneau%20city%20and%20borough,%20Alaska&t=Commuting&tid=ACSST5Y2019.S0802>). The margin of error for this estimate is 1.3% (meaning the actual mode share likely falls between 3.3% and 5.9%). The data indicates that public transportation users tend to be within lower income brackets as well.

The American Community Survey also provides data on carpooling for Juneau, indicating that carpools comprise about 14.6% of the 2013 to 2019 work trips with a margin of error of about 2%.

## 4 Traffic Data

### 4.1 Average Annual Daily Traffic

Average annual daily traffic (AADT) is obtained from the DOT&PF websites:

<https://alaskatraficdata.drakewell.com/publicmultinodemap.asp> and

<https://akdot.maps.arcgis.com/home/index.html>

AADT will be used in computing crash rates in determining the study area for the TIA. Also, AADT will be used to finalize those intersections within the study area that require mitigation. CBJ Code of Ordinances, 49.40.305, indicates that the study area for the TIA shall include those intersections with approaches where the proposed development will increase ADT by five percent or more.

The following table presents street system AADT in the proximity of the proposed SEARHC medical office facility. The 2013-2017 average AADT will be applied to the crash rate computation.

**Table 4: AADT for Streets Within Study Area**

Street/Segment	2013	2014	2015	2016	2017	2018	2019	2020	2013-2017 average
<b>Egan Drive</b>									
Glacier-Mendenhall Loop	26,195	26,332	26,776	27,844	26,884	25,115	26,134	21,700	26,806
Between Mendenhall Loop Road & Riverside Drive	14,315	14,479	14,698	15,284	14,757	16,126	15,769	12,500	14,707
Between Riverside Drive and Vintage Blvd	13,500	13,654	13,861	14,514	16,904	13,524	15,752	13,200	14,364
Between Vintage Boulevard & Engineers Cutoff Road	11,715	11,850	12,030	15,480	15,709	13,524	16,454	13,200	13,357
<b>Mendenhall Loop Road</b>									
Glacier Highway North-Egan Drive	8,655	-	8,655	9,192	9,095	8,410	9,334	-	8,890
Between Egan Drive/Glacier Highway & Atlin Drive	21,485	20,950	21,268	22,729	22,489	18,565	18,073	14,800	21,784
Mall Access-James Road	15,440	-	-	16,789	16,612	15,361	16,476	-	16,136
<b>Glacier Highway North</b>									
Glacier Highway North - Between Glacier Nugget & Del Rae Road	2,820	2,317	2,363	1,775	1,997	1,889	2,085	1,690	2,254

Street/Segment	2013	2014	2015	2016	2017	2018	2019	2020	2013-2017 average
<b>Vintage Blvd</b>									
Vintage Boulevard - Between Clinton Drive & Egan Drive/Glacier Highway	2,135	-	2,135	1,965	2,181	2,380	2,305	1,980	2,104
Vintage Boulevard - Near Junction with Riverside Drive	5,597	5,604	5,694	6,109	5,762	5,181	6,175	5,070	5,753
<b>Mendenhall Mall Road</b>									
Mendenhall Mall Access Road - Near Junction with Riverside Drive	6,445	6,217	6,340	6,498	7,153	4,502	4,700	3,860	6,531
<b>Riverside Drive</b>									
Egan Drive to Vintage Blvd	13,225	-	13,225	14,275	14,124	13,060	14,495	-	13,712
Riverside Drive - Between Vintage Boulevard & James Boulevard	8,110	7,962	7,961	10,074	9,967	8,449	8,619	7,080	8,815

## 4.2 Riverside Drive Continuous Count Station

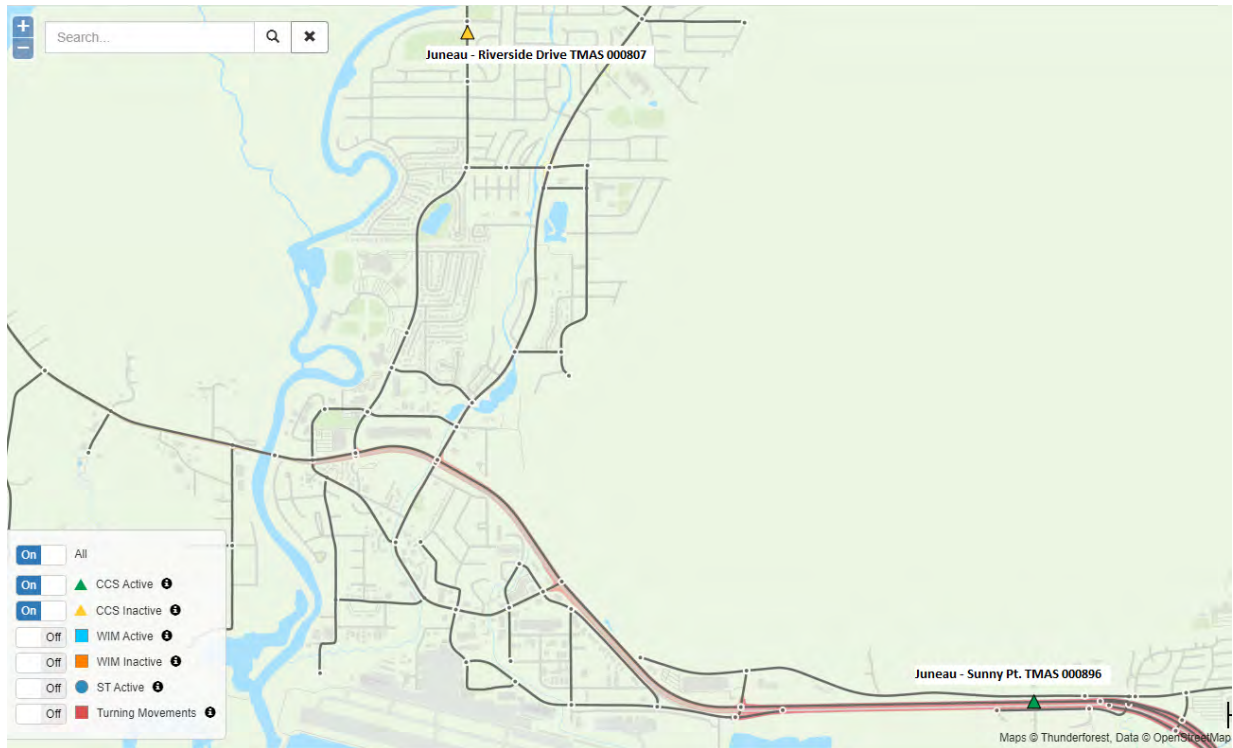
Continuous count stations (CCS) provide continuous data that can be used to factor temporary counts collected at separate times to a count that represents a design year and hour condition.

Source: <https://alaskatraficdata.drakewell.com/publicmultinodemap.asp>

Figure 12 presents the two closest CCS to the project site.

The closest CCS, Juneau - Riverside Drive TMAS 000807, was used to convert past counts to a common year and month. In addition to being the closest in proximity to the project, it is also on the street system that is most alike to the local, collector, and minor arterial streets of the study area and probably shares similar seasonal and diurnal traffic patterns. Annual AADT for the Riverside Drive CCS is shown in Table 5, and the average annual growth rate is calculated as 1.30%.





Source: <https://alaskatraficdata.drakewell.com/publicmultinodemap.asp>

**Figure 12: Continuous Count Station Locations**

**Table 5: AADT by Year at CCS Juneau-Riverside Drive**

Year	AADT
2012	3,920
2013	3,969
2014	3,967
2015	3,861
2016	3,915
2017	4,042
2018	4,045
2019	4,291
Estimated Annual Traffic Growth Rate= 1.30%	

To develop estimated base condition traffic volumes for each horizon year, the estimated annual traffic growth rate was applied to existing intersection traffic volume counts that were collected from 2012, 2015 and 2016. Because the intersection traffic flow counts were taken in non-peak months, the values were factored to the peak month of May using the ADT values in

Table 6. The cells highlighted in yellow represent the year and month that the counts were conducted. The cells highlighted in green are the corresponding May ADT values.

**Table 6: Riverside Drive CCS Monthly ADT by Year**

Juneau - Riverside Drive TMAS 000807								
Year → Month	2012	2013	2014	2015	2016	2017	2018	2019
Jan	3,561	3,485	3,542	3,566	3,509	3,390	3,554	3,578
Feb	3,754	3,590	3,699	3,653	3,718	3,594	3,636	3,938
Mar	3,787	3,668	3,654	3,639	3,819	3,399	3,574	4,015
Apr	4,160	3,944	3,964	3,980	4,110	4,164	4,117	4,426
<b>May</b>	<b>4,312</b>	<b>4,441</b>	<b>4,608</b>	<b>4,547</b>	<b>4,445</b>	<b>4,479</b>	<b>4,563</b>	<b>4,740</b>
Jun	4,283	4,280	4,306	4,013	4,222	4,258	4,347	4,424
Jul	4,196	4,164	4,267	3,819	4,001	4,347	4,399	4,370
Aug	4,240	4,294	4,217	4,100	4,190	4,428	4,408	4,523
Sep	4,037	4,194	4,094	3,950	4,047	4,514	4,333	4,291
Oct	3,842	4,235	4,025	3,899	3,890	4,440	4,046	4,153
Nov	3,459	3,735	3,668	3,568	3,561	3,775	3,807	4,027
Dec	3,407	3,596	3,643	3,603	3,470	3,719	3,758	4,129
<b>AADT →</b>	3,920	3,969	3,967	3,861	3,915	4,042	4,045	4,291

Source: <https://alaskatraficdata.drakewell.com/publicmultinodemap.asp>

From the Riverside CCS data, the month of May experiences the highest monthly AADT. As such, the design hour for the TIA analyses is expected to follow the same monthly ADT patterns as occurred at the CCS, and intersection movements will be factored to a summer peak condition occurring in May.

### 4.3 Intersection Counts Used For Background Traffic Estimates

Turning movement counts were collected by DOT&PF Southcoast Region between 2012 and 2016 during different months for each of the intersections in the study area, as shown in Figure 13 and Figure 14. These intersection movements were presented in the Juneau Egan Drive & Riverside Intersection Improvements study prepared by KE in 2016. The 2020-2022 pandemic has depressed travel, meaning that conducting current counts may be underrepresenting actual or future demands. As such the counts in the 2016 study were the base of this analysis and factored to estimate current and future demand conditions.

The AM and PM commuting peak hours were found to occur, generally, between the hours of 7:15 to 8:15 in the morning and 4:30 to 5:30 in the evening. Traffic primarily travels inbound in the AM peak hour, and outbound during the PM peak hour.

These counts in Figure 13 and Figure 14 will be factored to represent a common design period for the summer peak of May and for 2023.

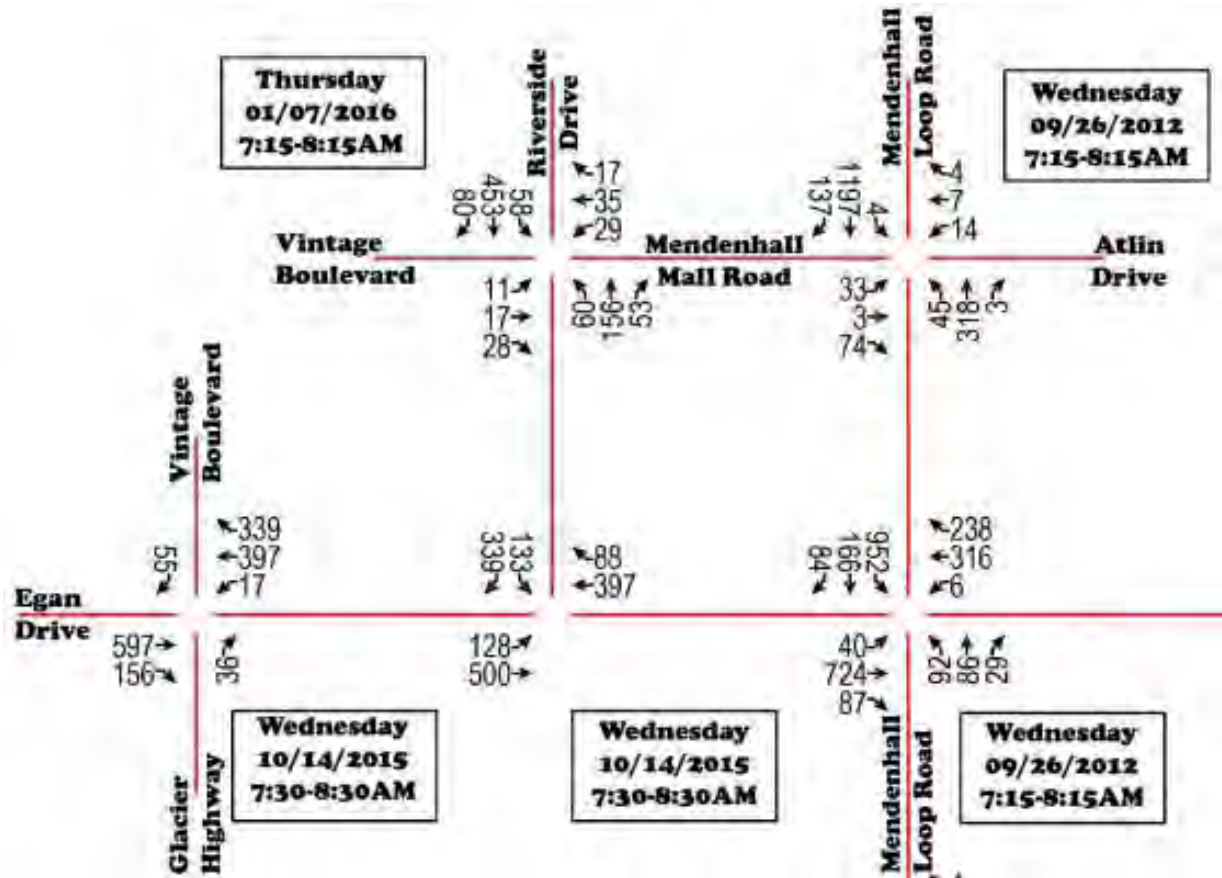


Figure 13: Observed AM Study Area Intersection Movement Traffic Volumes

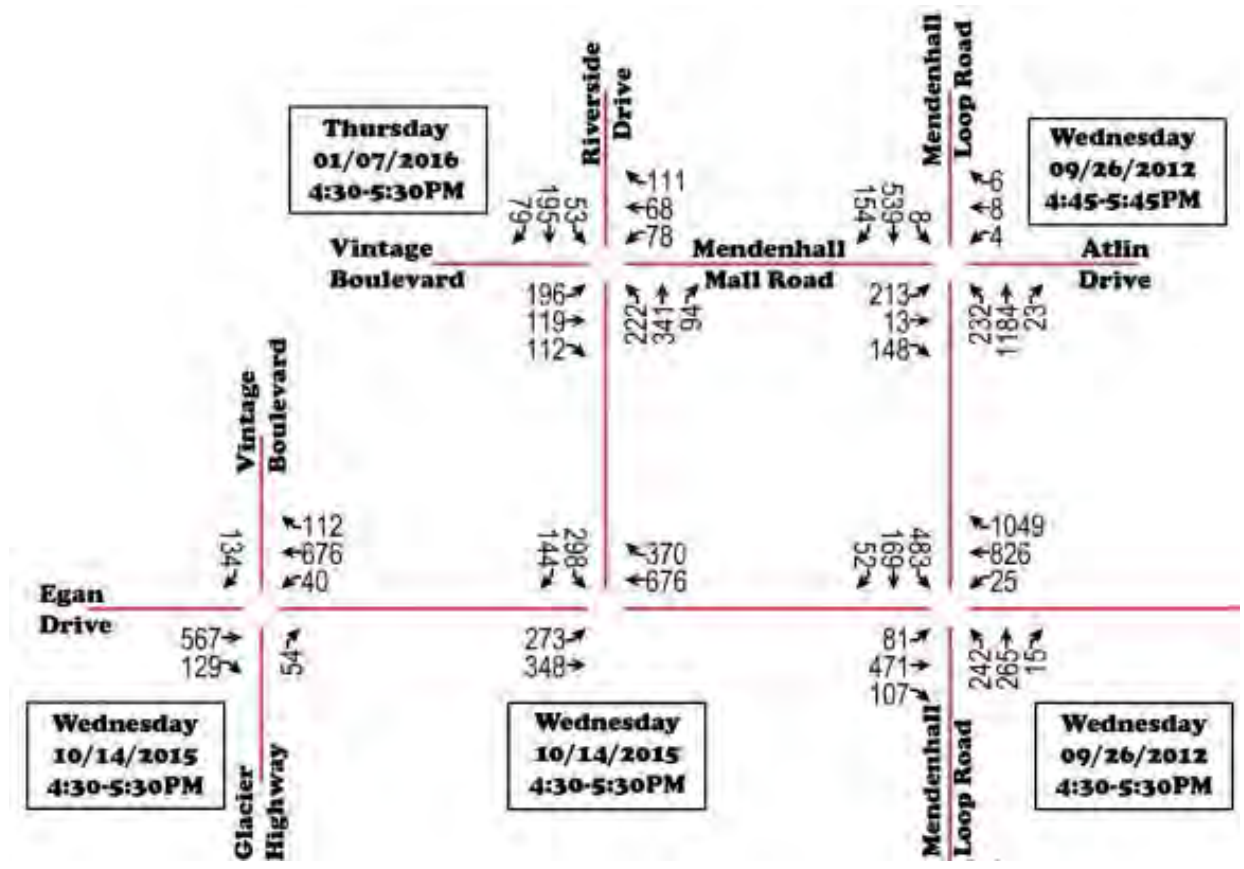


Figure 14: Observed PM Study Area Intersection Movement Traffic Volumes

## 5 Crash Analysis

DOT&PF provided crash data for the five individual intersections in the study area for the 5-year period from 2013 through 2017 (most recent years available). Crash types are summarized for each of the intersections in the study area in Table 7 below. The proportion of crash types, with majority as rear-end (front to rear) and angle are consistent with expectations for intersections.

**Table 7: Intersections and Crash Types, 2013 to 2017**

Crash Type	Egan Dr. and Mendenhall Loop Road	Egan Dr. and Riverside Dr.	Egan Dr. and Vintage Blvd.	Mendenhall Loop Road and Mall Access Road	Riverside Dr. and Vintage Blvd.	Grand Total
Angle	8	7		6	4	25
Front-To-Front	3	1			2	6
Front-To-Rear	34	5	4	10	3	56
Not a Collision with a Motor Vehicle In-Transport						
<i>Ditch</i>	1		1			2
<i>Guardrail Face</i>	1					1
<i>Motor Vehicle In-Transport Strikes or is Struck by Cargo /Persons/ Objects Set-In-Motion From/ By Another Motor Vehicle In-Transport</i>				1		1
<i>Other Post/Other Pole/Other Support</i>	1					1
<i>Other Traffic Barrier</i>		1				1
<i>Pedalcycle</i>	3					3
<i>Traffic Sign Support</i>			1			1
<i>Utility Pole/Light Support</i>	1				1	2
Other		2			2	4
Sideswipe - Opposite Direction	1			1		2
Sideswipe - Same Direction	1			1	1	3
Unknown	2	1				3
Grand Total	56	17	6	19	13	111

Table 8 summarizes crash severity for the intersections between 2013 and 2017. In general, this severity is low since there was only one major injury crash and no fatalities.

**Table 8: Intersection Crash Severity, 2013 to 2017**

Intersection Location		Property Damage Only	Minor Injury	Major Injury	Fatal
Street 1	Street 2				
Egan Drive	Vintage Boulevard	4	2		
Riverside Drive	Vintage Boulevard	9	4		
Mendenhall Loop Road	Mall Access Road	15	3	1	
Egan Drive	Mendenhall Loop Road	36	20		
Egan Drive	Riverside Drive	13	4		

KE calculated intersection crash rates based on the number of crashes, the number of years of analysis, and entering AADT. KE then compared the actual crash rates to average statewide crash rates for similar facilities and calculated upper control limits (UCLs) for each facility. The upper control limit, or critical limit, is a threshold above which the observed rate is considered statistically significant at a 95% confidence level.

**Table 9: Intersection Crash Rates, 2013 to 2017**

Intersection Location		Entering AADT 5 Year Average	Total Crashes	Millions of Entering Vehicles (MEV) in Study Period	Crash Rate (Crashes per MEV)	*State Average Crash Rate (Crashes per MEV)	Upper Control Limit Rate (Crashes per MEV)	Critical Rate Exceeded?
Street 1	Street 2							
Egan Drive	Vintage Boulevard	16,039	6	29.272	0.20	0.55	0.80	No, Actual is 26% of UCL
Riverside Drive	Vintage Boulevard	17,405	13	31.765	0.41	1.57	1.95	No, Actual is 21% of UCL
Mendenhall Loop Road	Mall Access Road	22,225	19	40.561	0.47	1.57	1.91	No, Actual is 25% of UCL
Egan Drive	Mendenhall Loop Road	36,094	56	65.871	0.85	1.57	1.83	No, Actual is 46% of UCL
Egan Drive	Riverside Drive	21,391	17	39.039	0.44	1.02	1.30	No, Actual is 33% of UCL

\*Population rates are from the Intersection HSIP Handbook FY 18 Screening Process Spreadsheets

In summary, traffic safety performance of the intersections is good. The crash rates for intersections are below upper critical limit by a significant margin, and actually are less than the average rates for similar intersections. Also, crash severities for the intersections are low.



## 6 Traffic Forecasting

### 6.1 Design Conditions

Of most concern regarding site traffic generation by the proposed SEARHC medical office facility is the impacts during peak hours of the street network, typically commuter peak periods. Operationally, these periods are the most congestive, and in fact are the design condition.

In order to estimate traffic impacts of the development, a no-build condition background traffic is determined. Since we are concerned with a future design year, the background traffic is expected to increase at an annual growth rate, which, for this project, is assumed to be similar to the recent past year's observed growth rate from the Riverside Drive CCS. Site-generated traffic is projected onto the background traffic to estimate the build condition and the incremental impacts of the new facility. Unlike background traffic, which is assumed to increase over time, site traffic levels are static and will remain the same for entire life of the facility absent of additions or renovations.

The anticipated build year for the proposed SEARHC medical office building is 2023. As previously discussed, intersection traffic counts were not performed in 2020-2022 because of pandemic depressed travel. Instead, historic traffic counts were for the peak AM and peak PM time periods, and past data from the Riverside CCS was used to factor past traffic to a design year and hour condition. Institute of Transportation Engineers Trip Generation (10<sup>th</sup> edition) website was used to calculate the ADT and peak hour traffic generated by the proposed development.

### 6.2 Background Traffic

#### 6.2.1 Normalization to 2019

The traffic counts collected in 2012, 2015 and 2016, discussed in Section 4.3 on page 23, were used as a basis for the normalization of 2019 traffic flow counts, that were then used to calculate the 2023 base traffic flow counts. 2019 was used as the normalization year, as it is pre-pandemic and is the last year published for the CCS Juneau - Riverside Drive TMS 000807, which is the closest CCS (see Section 4.2, on page 21). The CCS AADT between 2012 and 2019, shown in Table 5 on page 22, yields an average annual growth rate of 1.30%.

To develop the estimated traffic volumes for 2019, the estimated annual traffic growth rate of 1.30% was applied to existing traffic volumes gathered from 2012 through 2016 to derive the annual growth factor, which was then applied to the movement volumes presented in in Figure 13 and Figure 14. The factors are computed as:

$$\text{Factor for converting 2012 counts to 2019 baseline: } F = (1.013)^{(2019-2012)} = 1.095$$

$$\text{Factor for converting 2015 counts to 2019 baseline: } F = (1.013)^{(2019-2015)} = 1.053$$

$$\text{Factor for converting 2016 counts to 2019 baseline: } F = (1.013)^{(2019-2016)} = 1.040$$

To further explain, the turning movement volumes collected in 2012 would be factored to 2019 by multiplying the 2012 volume by 1.095.

An additional step is required to factor volumes collected during the year to a summer “peak” condition. The apex of the monthly ADT is May as is clearly exhibited in Table 6 on page 23. Table 10, below, shows how monthly factors are computed. The factored traffic flow counts for 2019 are shown in Figure 15 and Figure 16.

**Table 10: Monthly Factor to Adjust 2019 Original Traffic Count Month to Peak Month**

<b>Intersection</b>	<b>Year</b>	<b>Original Traffic Count Month</b>	<b>CCS Count Month ADT</b>	<b>Design Summer Month</b>	<b>CCS May ADT</b>	<b>Monthly Factor</b>
Riverside/Vintage	2016	January	3,509	May	4,445	4445/3509=1.267
Egan/Mendenhall Loop	2012	September	4,037	May	4,312	4312/4037=1.068
Egan/Riverside	2015	October	3,899	May	4,547	4547/3899=1.166
Egan/Vintage	2015	October	3,899	May	4,547	4547/3899=1.166
Mendenhall Loop/Mendenhall Mall/Atlin	2012	September	4,037	May	4,312	4312/4037=1.068

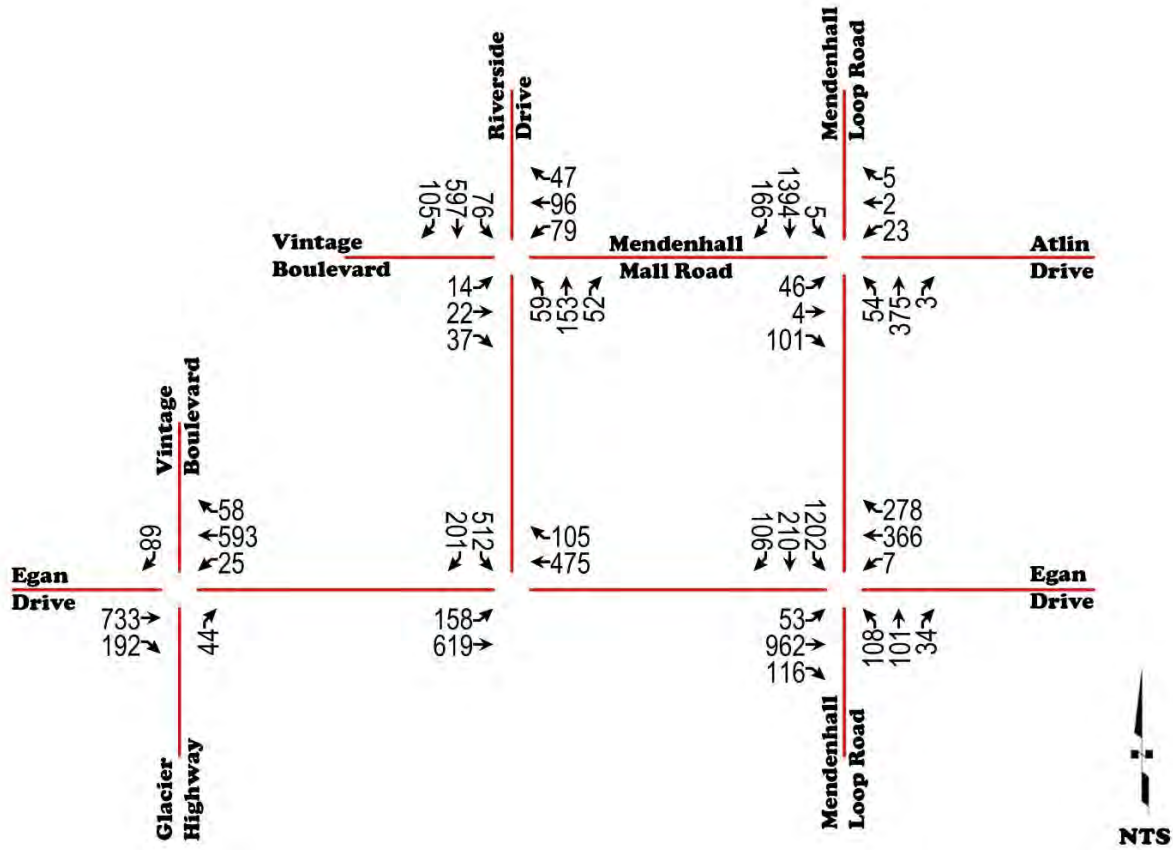


Figure 15: AM Peak TMVs – Factored Base Condition 2019

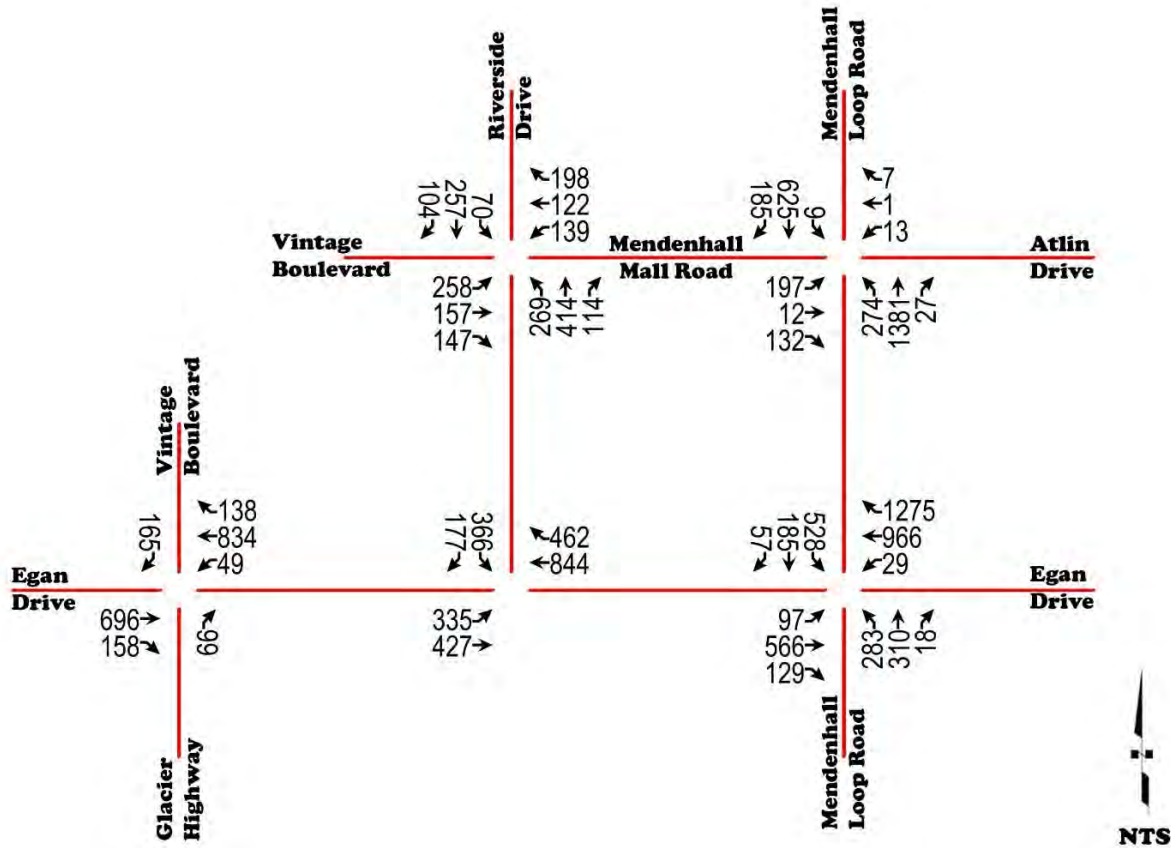


Figure 16: PM Peak TMVs – Factored Base Condition 2019

**6.2.2 Estimated Background Traffic Growth for Design Year (2023)**

The 2023 intersection movement volumes were calculated by growing the factored 2019 counts by the historical trend of 1.30%. The factor is computed as:

$$\text{Factor for converting 2019 counts to 2023 counts: } F = (1.013)^{(2023-2019)} = 1.053$$

**6.2.3 Traffic Generated by Committed Developments**

According to CBJ planners, there is a proposed 11 condominium complex with 11 commercial garages development with an estimated ADT of 108, and a proposed senior housing development with 98 beds and an estimated ADT of 269 planned for construction in 2022. Both facilities are located on Clinton drive, which exits on both ends to Vintage Boulevard. CBJ planners have requested that these facilities be added to the background traffic for intersection traffic movements occurring without the proposed SEARHC medical offices.

In order to develop AM and PM peak hour volumes to apply to the design hours, we need average rates or a function to which we would input independent variables to estimate trips.

However, CBJ did not provide weekday hourly traffic counts, nor did they provide independent variables for the developments that can be used to calculate peak hour traffic. As such, an alternative method was used to convert ADT volumes to the AM and PM hours that will be added to background traffic. Institute of Transportation Engineers Trip Generation (10<sup>th</sup> edition) website was used to find the weekday peak hour average rate and the weekday average rate based on the land use classification. These values were then used in the below formula to estimate the weekday AM and PM peak hour traffic trips:

$$\text{Hourly Traffic} = (\text{Weekday Peak Hour Average Rate} / \text{Weekday Average Rate}) / \text{ADT}$$

Table 11 and Table 12, below, show the calculated values for the trips in and out of the proposed developments based on the directional distribution (DD%) also found from the Institute of Transportation Engineers Trip Generation (10<sup>th</sup> edition) website. More information from ITE Trip Generation website can be found in Appendix B.

**Table 11: 98 bed Senior Housing Trip Generation Calculation**

Land Use Classification	ADT	Peak Hour	Equation	DD% (In/Out)	Trips In	Trips Out
252 - Senior Adult Housing	269	AM	$(0.2 / 3.7) / 269 = 15$	35/65	$15 * 35\% = 5$	$15 * 65\% = 10$
		PM	$(0.26 / 3.7) / 269 = 19$	55/45	$19 * 55\% = 10$	$19 * 45\% = 9$

**Table 12: 11 Condos with 11 Commercial Garages Trip Generation Calculation**

Land Use Classification	ADT	Peak Hour	Equation	DD% (In/Out)	Trips In	Trips Out
221 – Multifamily Housing (mid-rise)	108	AM	$(0.36 / 5.44) / 108 = 7$	26/74	$7 * 26\% = 2$	$7 * 74\% = 5$
		PM	$(0.44 / 5.44) / 108 = 9$	61/39	$9 * 61\% = 5$	$9 * 39\% = 3$

**6.2.3.1 Trip Distribution for Condominiums and Senior Housing**

The AM and PM peak hour traffic from these facilities are not accounting for the turning movement volumes presented in Figure 15 and Figure 16 shown above. The state of practice for site traffic trip distribution methodology is to assume the inbound and outbound trips will follow the existing background traffic travel patterns. As such, trip distribution for the proposed senior housing and condominiums was calculated assuming traffic divides at intersections in the inbound and outbound proportions exhibited in Figure 15 and Figure 16 would be similar to existing traffic flows throughout the traffic network. Figure 17 and Figure 18 presents the AM and PM trip distribution of the condominium and senior housing traffic for the intersections of Vintage Boulevard with Egan Drive and Riverside Drive, based on 2019 base traffic. The remaining three intersections in the study area used this methodology to overlay site trips on the base traffic movements.

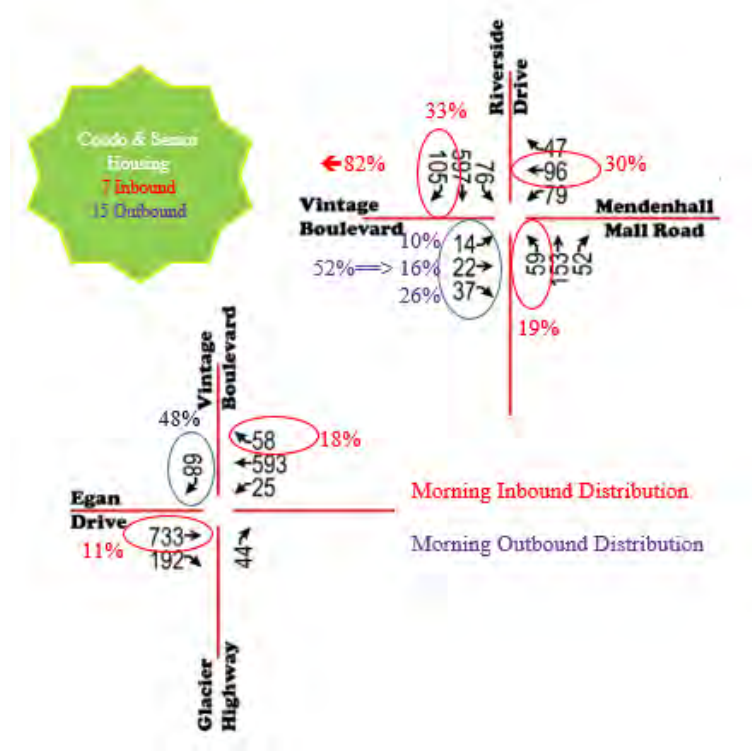


Figure 17: AM Condo and Senior Site Trip Distribution to Near Intersections

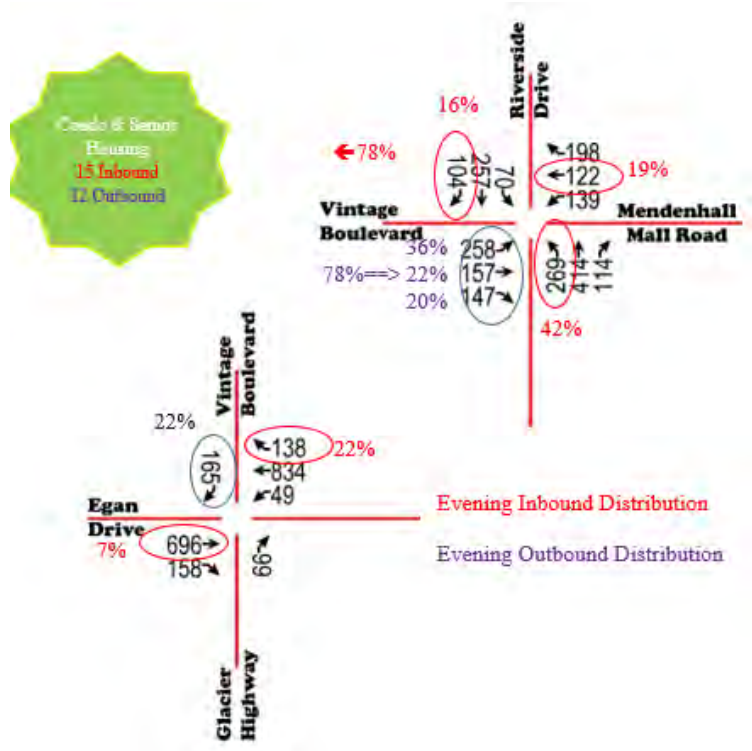


Figure 18: PM Condo and Senior Site Trip Distribution to Near Intersections



**6.2.4 Composite Design Year 2023 without Medical Office**

Opening year, 2023, base traffic conditions are presented as AM and PM peak hour turning movements in Figure 19 and Figure 20. As stated in previous sections, these turning movement counts were calculated by taking the original counts and projecting them to year 2019 using the historical AADT and then factoring the counts from the month they were taken to the peak month of 2019, which was May. The 2019 turning movement volumes were then factored to 2023 as discussed in Section 6.2.2 on page 31. The site traffic from the proposed condominiums and senior housing turning movements expected to be built before the medical offices are open was distributed to the intersections as discussed in Section 6.2.3.1 above.

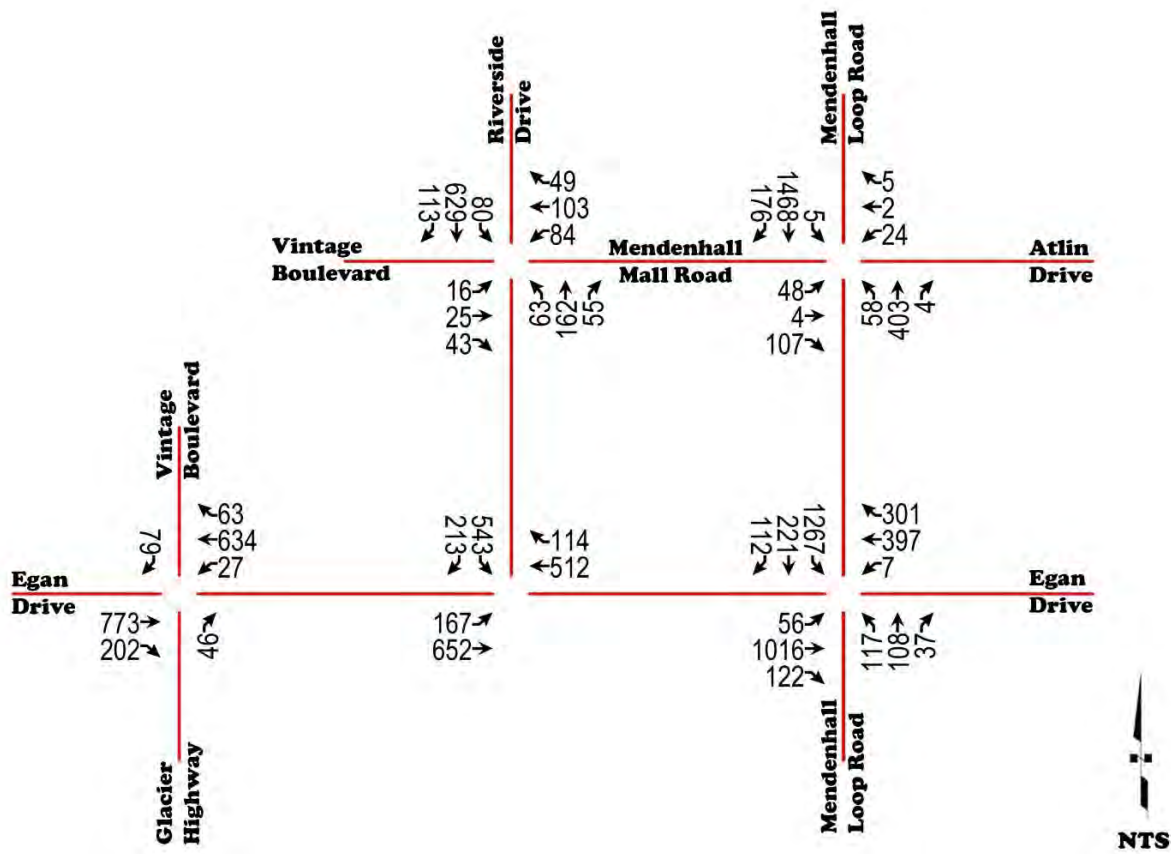


Figure 19: AM Peak TMVs – Base Condition 2023

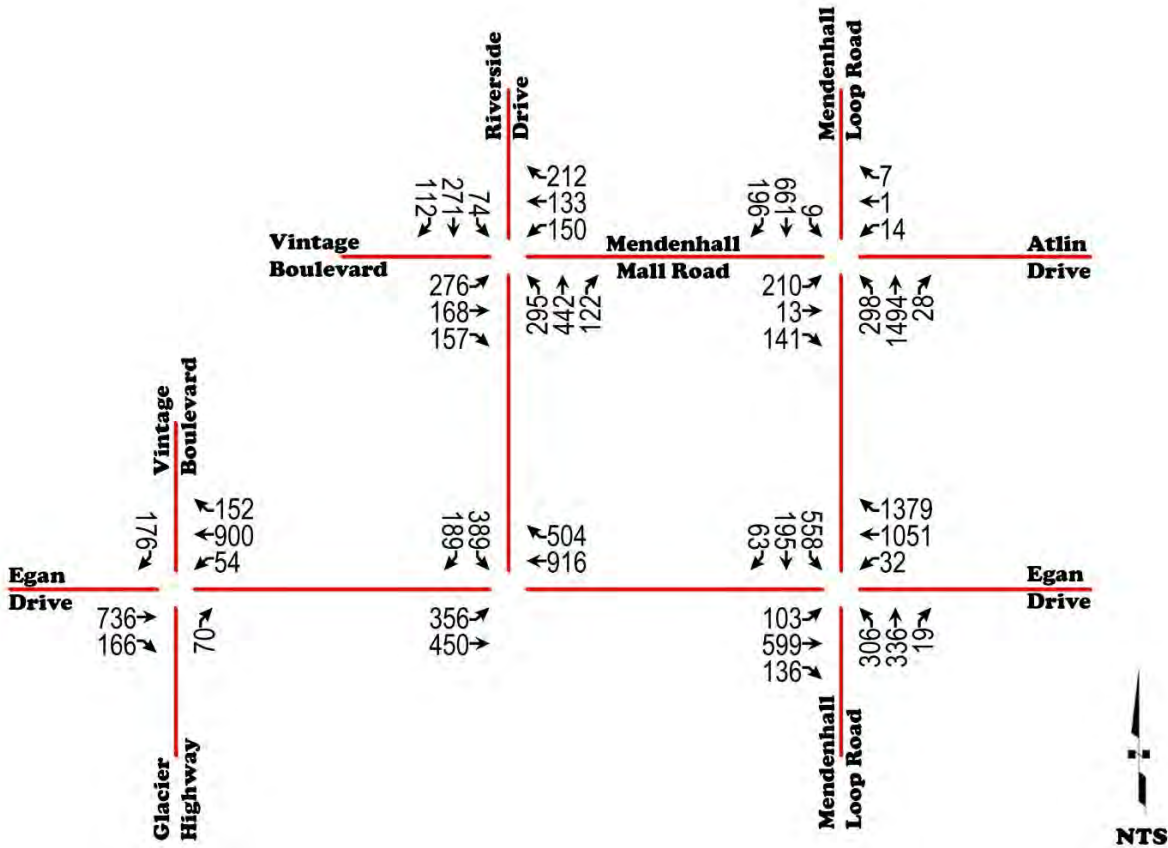


Figure 20: PM Peak TMVs – Base Condition 2023

### 6.3 Site Development Traffic

#### 6.3.1 Trip Generation

The Institute of Transportation Engineers (ITE) *Trip Generation Manual*, 10th Edition, provides trip generation for Land Use 720, Medical Dental Offices using gross floor area as the independent variable in an average trip rate or using a fitted curve. The methodology in the *Trip Generation Handbook*, 3<sup>rd</sup> Edition, was followed the fitted curve equation (regression) to calculate estimated trips. The trip generation time periods were the AM peak hour of the adjacent street (7AM to 9 AM) and the PM peak hour of adjacent street (4PM to 6PM) which would occur during the commuting peak and is the typical design condition for streets and intersections. Peak hour trip generation rates are presented in Table 13.

**Table 13: 45,924 square foot Medical Office Building AM and PM Trip Generation Calculation**

Land Use Classification	Gross Floor Area (1000 square feet)	Peak Hour	Equation Type	Equation and Trip Computations	DD% (In/Out)	Trips In	Trips Out
720 – Medical-Dental Office Building	X=45.924	AM Peak (7AM - 9AM)	Regression	$\text{Ln}(T)=0.89\text{Ln}(X)+1.31$ $\text{Ln}(T)=0.89\text{Ln}(45.924)+1.31 =$ <b>112</b>	78/22	87	25
		PM Peak (4PM - 6PM)	Regression	$T=3.39(X)+2.02$ $T=3.39(45.924)+2.02 =$ <b>158</b>	28/72	44	114

**6.3.1.1 Trip Distribution**

Trip distribution for the proposed medical office building was calculated assuming the site traffic patterns are the same as the background base traffic flows throughout the traffic network. Distribution patterns and assignments are essentially the same as discussed for the condominium and senior housing traffic discussed in Section 6.2.3.1.

**6.3.2 Composite Design Year 2023 with Medical Offices**

Figure 21 and Figure 22, below, show the traffic flow counts at the adjacent five intersections for build year 2023 with the addition to the anticipated increase in traffic flow from the medical office building. These turning movement counts were calculated by taking the 2023 traffic count turn percentages and factoring the anticipated medical center trips accordingly.

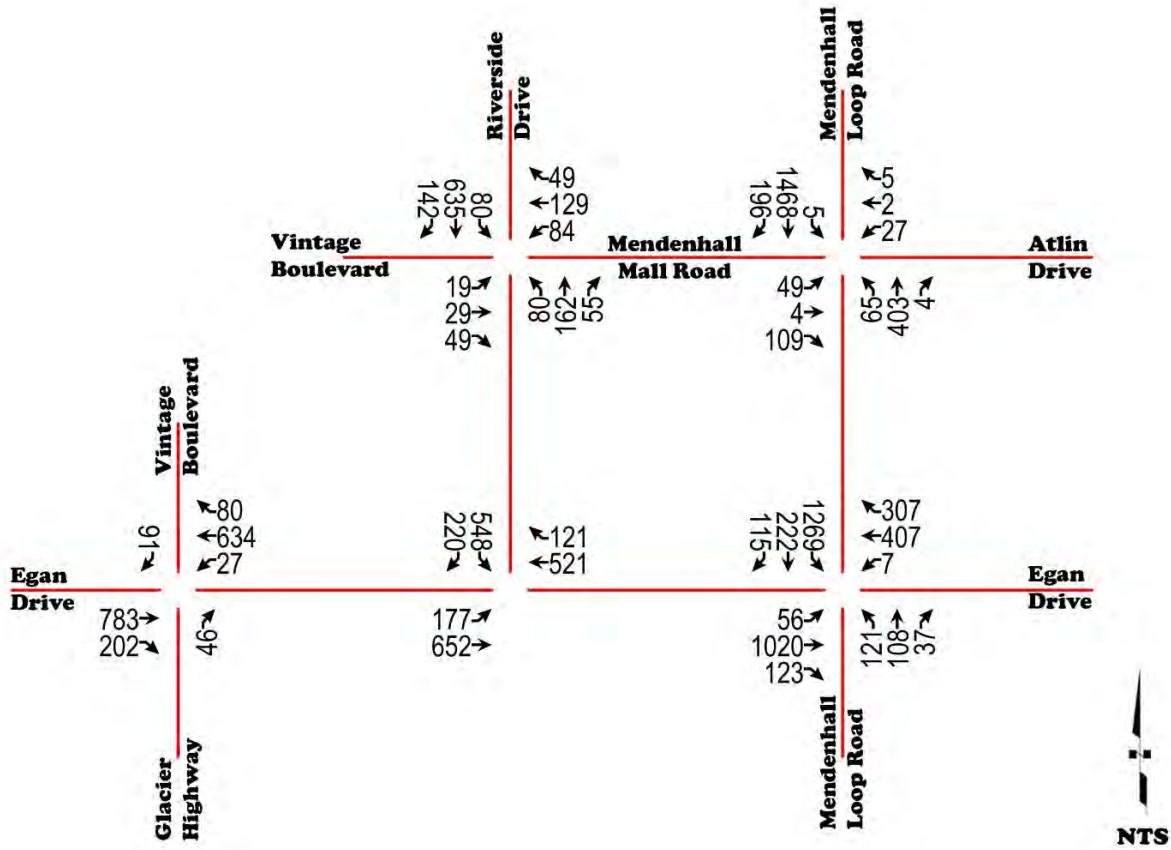


Figure 21: AM Peak TMVs – Build Condition 2023

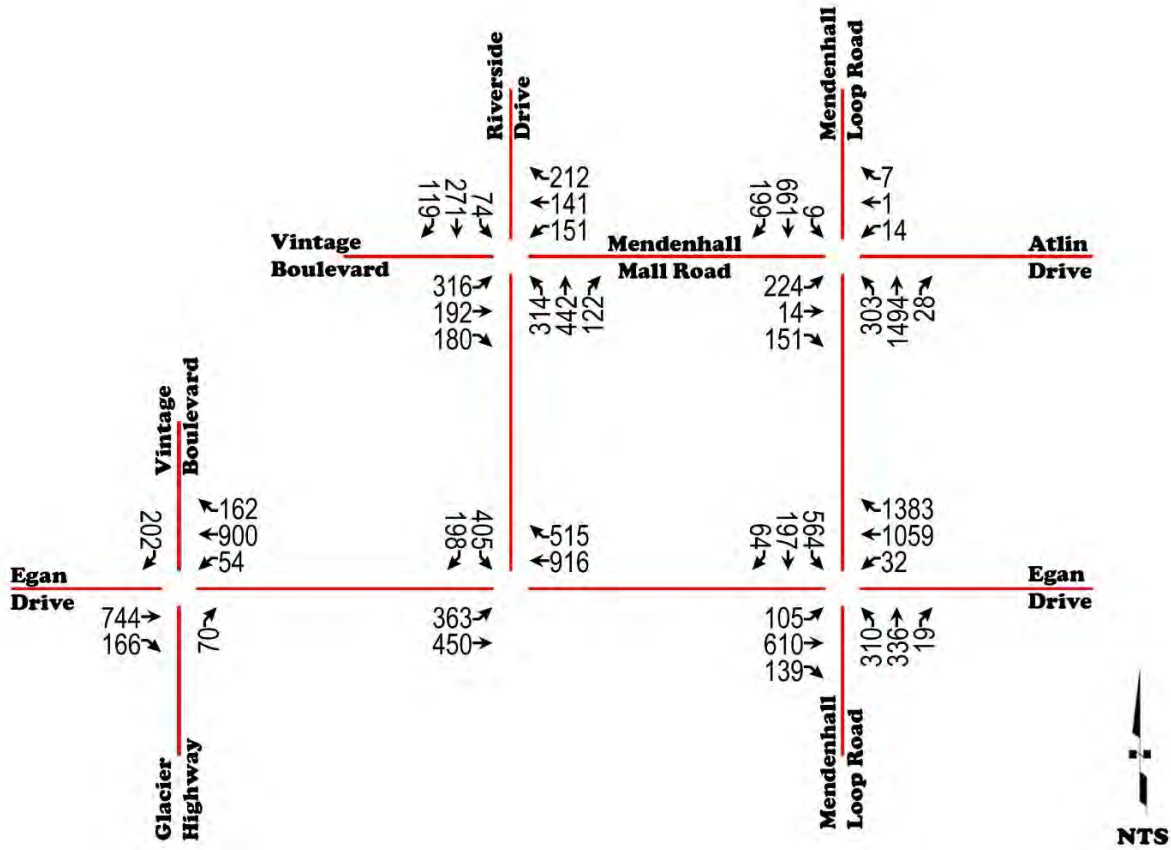


Figure 22: PM Peak TMVs – Build Condition 2023

## 7 Traffic and Safety Analysis

### 7.1 Intersection Operation Impacts

The 2023 base condition AM and PM peak hour traffic flow counts for the five adjacent intersections were analyzed using Synchro 11 software, using Highway Capacity Manual (HCM) 2000 methodologies for the signalized intersections and HCM 2010 methodologies for the unsignalized intersection. HCM 2010 requires National Electrical Manufacturers Association (NEMA) phasing to analyze signal operations. The Egan Drive and Mendenhall Loop Road intersection cannot be modeled as NEMA phasing because of the split-phased timing. Additionally, the Egan Drive and Riverside Drive intersection has an exclusive pedestrian phase which cannot be modeled as NEMA phasing. For equal comparison among the signalized intersections in the study area, HCM 2000 was used to analyze all signals.

The signalized intersections were analyzed using existing signal timings provided by DOT&PF for the 2016 Juneau Egan Drive & Riverside Intersection Improvements project. The Egan Drive at Mendenhall Loop Road intersection and the Riverside Drive at Vintage Boulevard / Mendenhall Mall Road intersection are configured with at least one channelized right-turn movement. It was assumed that channelized right-turn movements at an intersection do not affect signal operations. Therefore, these right-turn movements were removed from the signal models at the Egan Drive and were analyzed separately as an unsignalized intersection. Approach and intersection delays at these intersections consider delays at the signal and delays for the channelized right-turn lanes.

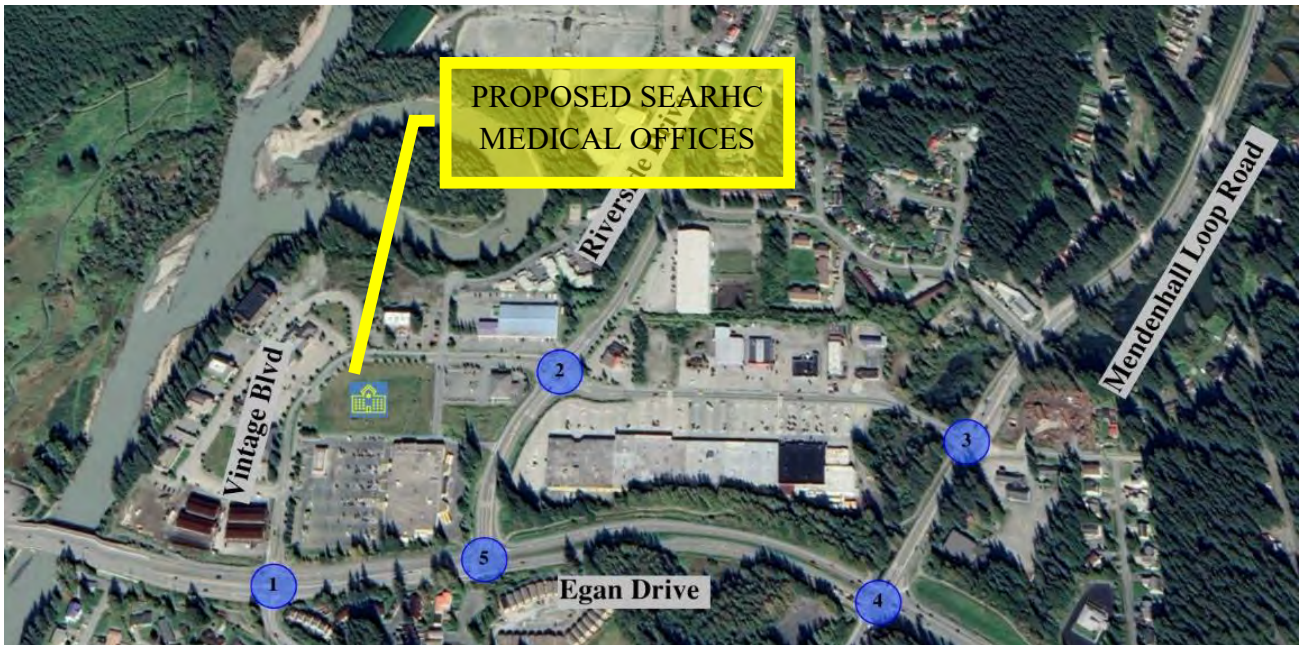
It should be noted that this analysis may not be using the most current signal timing plans. DOT&PF stated in their review of this report that adaptive signal control is used at the study intersections. However, DOT&PF did not require reevaluation of the signalized intersection operations since, as present in the sections below, the additional delay impact caused by the development is not significant. Moreover, adaptive signal control is a mitigation to decrease delay.

Observed intersection AM and PM peak hour factors (PHFs) from the collected turning movement counts discussed in Section 4.3 on page 23. PHFs ranged from 0.77 to 0.91 during the AM peak and 0.91 to 0.97 during the PM peak.

Average heavy vehicle percentages were estimated using continuous count stations and short-term stations with historical vehicle classification data gathered from the DOT&PF traffic data portal. Heavy vehicles made up approximately 5% of traffic on Egan Drive, 2% of traffic on Riverside Drive, and 7% of traffic on Vintage Boulevard. Roadways without vehicle classification data were given a default heavy vehicle percentage of 2%.

The intersections that are evaluated in this section are presented in Figure 23 and listed below.





Background Photo Source- Google Earth

**Figure 23: TIA Study Area Intersections**

The analyzed intersections (numbered as shown in the figure) include:

1. Egan Drive and Vintage Boulevard/Glacier Highway
2. Riverside Drive and Vintage Boulevard/ Mendenhall Mall Road
3. Mendenhall Loop Road and Mendenhall Mall Road/Atlin Drive
4. Mendenhall Loop Road and Egan Drive
5. Riverside Drive and Egan Drive

The site traffic distributed to other intersection approaches outside of the study area were found to be under the 5% threshold discussed in Section 1.2. As such, the study area shown satisfies CBJ Code of Ordinances.

Under each subsection below, the operation performance measures are presented for the 2023 AM and PM peak hours, for both cases of with and without the SEARHC medical offices.

Operational performance measures include:

- Movement and intersection average control delay in seconds per vehicle (sec/veh);
- Movement and Overall Level of Service (LOS) based on control delay, and as presented in the HCM for signalized and unsignalized intersection;
- Volume to Capacity Ratio (v/c) for movements, where  $v/c > 1$  indicates demand exceeds capacity, and;
- Queue Lengths in feet, 95<sup>th</sup> percentile queue is reported, signifying the approximate length exceeded by queues about 5% of the time.

Table 14 presents level of service (LOS) criteria for signalized and unsignalized intersections. Level of service for signals is applied to individual movements, approaches, and the intersection. Unsignalized LOS is applied only to movements and approaches that yield to other movements and unimpeded movements are not analyzed since they do not experience delay.

**Table 14: Signalized and Unsignalized Intersection LOS Based on Control Delay**

Level of Service	Signalized Intersection Control Delay (seconds/vehicle)	Unsignalized Intersection Control Delay (seconds/vehicle)
A	0-10	0-10
B	>10-20	>10-15
C	>20-35	>15-25
D	>35-55	>25-35
E	>55-80	>35-50
F	>80	>50

Queue lengths (95<sup>th</sup> percentile) for controlled turning movements are evaluated to determine if the queues exceed available auxiliary lane storage lane. Signalized through movement queues are evaluated to determine if they block entrances to auxiliary lanes, denying turning vehicle access to the auxiliary lane. Queue storage for all movements may be limited by upstream intersections, which is whether they will spill back through an upstream intersection and interfere with the upstream intersection operations.

**7.1.1 Egan Drive and Vintage Boulevard/Glacier Highway**

This intersection, discussed in detail under Section 3.2.5, on page 16 and depicted below in Figure 24 is a two-way stop-controlled intersection.



Figure 24: Egan Drive and Vintage Boulevard/Glacier Highway

7.1.1.1 Egan-Vintage-Glacier AM Peak Hour

The following tables summarize intersection LOS performance measures for the AM peak hour in the design year without the SEARHC medical offices and with the medical offices.

Table 15: AM Peak at Egan Drive-Vintage Boulevard Intersection, Base Condition 2023

Existing Base Condition AM Peak	WB	NB	SB
	LT	R	R
Delay (sec/veh)	10	13	12
LOS	B	B	B
v/c Ratio	0.05	0.10	0.15
Queue Length (ft)	<25	<25	<25

Table 16: AM Peak at Egan Drive-Vintage Boulevard Intersection, Build Condition 2023

2023 Build Condition AM Peak	WB	NB	SB
	LT	R	R
Delay (sec/veh)	10	13	13
LOS	B	B	B
v/c Ratio	0.05	0.10	0.20
Queue Length (ft)	<25	<25	<25

There are only minor differences in performance measures for the AM intersection operations with and without the development. The following table summarizes queue impacts.

**Table 17: AM Peak at Egan Drive-Vintage Boulevard Intersection Queue Lengths**

2023 Build Condition AM Peak	WB			EB		SB
	<i>LT</i>	<i>Thru</i>	<i>R</i>	<i>Thru</i>	<i>R</i>	<i>R</i>
Available Storage Length (ft)	200	Free	400	Free	360	240
No-Build Queue Length (ft)	<25	Free	Free	Free	<25	<25
Build Queue Length (ft)	<25	Free	Free	Free	<25	<25

The are no queue impacts caused by the proposed development.

**7.1.1.2 Egan-Vintage-Glacier PM Peak Hour**

The following tables summarize intersection LOS performance measures for the PM peak hour in the design year without the SEARHC medical offices and with the medical offices.

**Table 18: PM Peak at Egan Drive-Vintage Boulevard Intersection, Base Condition 2023**

Existing Base Condition PM Peak	WB	NB	SB
	<i>LT</i>	<i>R</i>	<i>R</i>
Delay (sec/veh)	10	12	15
LOS	A	B	C
v/c Ratio	0.05	0.15	0.35
Queue Length (ft)	<25	<25	50

**Table 19: PM Peak at Egan Drive-Vintage Boulevard Intersection, Build Condition 2023**

2023 Build Condition PM Peak	WB	NB	SB
	<i>LT</i>	<i>R</i>	<i>R</i>
Delay (sec/veh)	10	12	17
LOS	A	B	C
v/c Ratio	0.05	0.15	0.40
Queue Length (ft)	<25	<25	50

There are only minor differences in performance measures for the PM intersection operations with and without the development. The following table summarizes queue impacts.

**Table 20: PM Peak at Egan Drive-Vintage Boulevard Intersection Queue Lengths**

	WB			EB		SB
	<i>LT</i>	<i>Thru</i>	<i>R</i>	<i>Thru</i>	<i>R</i>	<i>R</i>
Available Queue Length (ft)	200	Free	400	Free	360	240
No-Build Queue Length (ft)	<25	Free	Free	Free	<25	50



	WB			EB		SB
	<i>LT</i>	<i>Thru</i>	<i>R</i>	<i>Thru</i>	<i>R</i>	<i>R</i>
Build Queue Length (ft)	<25	Free	Free	Free	<25	50

The are no queue impacts caused by the proposed development.

**7.1.1.3 Egan-Vintage-Glacier Summary**

Intersection operation measures and queue lengths are not significantly impacted by the traffic generated by the proposed SEARHC medical office development. The operational performance measures with the development do not exceed CBJ Code of Ordinance thresholds requiring mitigations (see Section 1.2 on page 5 for discussion of requirements).

**7.1.2 Riverside Drive and Vintage Boulevard/ Mendenhall Mall Road**

This intersection, discussed in detail under Section 3.2.3 on page 15 and depicted below in Figure 25 is a signalized intersection.



Figure 25: Riverside Drive and Vintage Boulevard/Mendenhall Mall Road

**7.1.2.1 Riverside Drive and Vintage Boulevard/ Mendenhall Mall Road AM Peak Hour**

The following tables summarize intersection LOS performance measures for the AM peak hour in the design year without the SEARHC medical offices and with the medical offices.

**Table 21: AM Peak at Riverside Drive-Vintage Boulevard Intersection, Base Condition 2023**

2023 Base Condition AM Peak	EB			WB			NB			SB		
	LT	Thru	R (Yield)	LT	Thru	R (Yield)	LT	Thru	R (Yield)	LT	Thru	R (Yield)
Delay (sec/veh)	36	38	18	29	36	10	12	10	9	7	23	10
LOS	D	D	C	C	D	A	B	A	A	A	C	B
v/c Ratio	0.15	0.25	0.15	0.45	0.55	0.10	0.30	0.20	0.10	0.15	0.85	0.20
Queue Length (ft)	25	50	25	100	125	< 25	50	100	< 25	50	575	25
Intersection	20 sec/veh											
	LOS B											

**Table 22: AM Peak at Riverside Drive-Vintage Boulevard Intersection, Build Condition 2023**

2023 Build Condition AM Peak	EB			WB			NB			SB		
	LT	Thru	R (Yield)	LT	Thru	R (Yield)	LT	Thru	R (Yield)	LT	Thru	R (Yield)
Delay (sec/veh)	36	38	19	29	37	10	14	10	9	8	26	11
LOS	D	D	C	C	D	A	B	B	A	A	C	B
v/c Ratio	0.15	0.25	0.20	0.40	0.60	0.10	0.40	0.20	0.10	0.15	0.85	0.25
Queue Length (ft)	50	50	25	100	150	< 25	50	125	< 25	50	625	25
Intersection	21 sec/veh											
	LOS C											

Although the intersection LOS is B without development traffic and C with it, the average delay only increases by one second in the build case. As such, individual movement and overall intersection performance measures will not change significantly with the additional traffic generated by the SEARHC medical offices. The following table summarizes queue impacts.

**Table 23: AM Peak at Riverside Drive-Vintage Boulevard Intersection Queue Lengths**

	EB			WB			NB			SB		
	LT	Thru	R (Yield)	LT	Thru	R (Yield)	LT	Thru	R (Yield)	LT	Thru	R (Yield)
Available Storage Length (ft)	260	190*	190	140	110*	110	170	140*	140	180	140*	140
No-Build Queue Length (ft)	25	50	25	100	125	< 25	50	100	< 25	50	575	25
Build Queue Length (ft)	50	50	25	100	150	< 25	50	125	< 25	50	625	25

\*Through queue storage is unlimited, but the lengths presented in the table are the shortest length of the adjoining auxiliary lanes to address the likelihood of through queues blocking the auxiliary lane.



With site development traffic the westbound 150-foot through queue will occasionally block the entrance of the westbound right turn lane. However, that is not a significant issue, since right-turns and throughs will proceed on the same signal phase. Right-turns on other phases may be impacted on occasion. The westbound left-turn lane is 140 feet, and the adjacent westbound through queue is 150 feet. However, the channelization is formed by striping, and vehicles may enter the auxiliary left-turn lane behind the through queue by driving over the painted taper.

The no-build southbound queues will block access to the adjacent auxiliary turn lanes. For the right-turn lane, this may not be a significant issue as discussed above, but the blockage to the left-turn lane may starve the left-turn phase during green indications. The build condition lengthens the southbound through queue by approximately 50 feet but does not create any additional impacts.

**7.1.2.2 Riverside Drive and Vintage Boulevard/ Mendenhall Mall Road PM Peak Hour**

The following tables summarize intersection LOS performance measures for the PM peak hour in the design year without the SEARHC medical offices and with the medical offices.

**Table 24: PM Peak at Riverside Drive-Vintage Boulevard Intersection, Base Condition 2023**

2023 Base Condition PM Peak	EB			WB			NB			SB		
	LT	Thru	R (Yield)	LT	Thru	R (Yield)	LT	Thru	R (Yield)	LT	Thru	R (Yield)
Delay (sec/veh)	22	30	13	21	30	25	15	23	11	18	27	13
LOS	C	C	B	C	C	D	B	C	B	B	C	B
v/c Ratio	0.65	0.60	0.30	0.40	0.50	0.55	0.65	0.70	0.15	0.25	0.65	0.20
Queue Length (ft)	225	175	50	125	150	100	175	375	25	50	250	25
Intersection	21 sec/veh											
	LOS C											

**Table 25: PM Peak at Riverside Drive-Vintage Boulevard Intersection, Build Condition 2023**

2023 Build Condition PM Peak	EB			WB			NB			SB		
	LT	Thru	R (Yield)	LT	Thru	R (Yield)	LT	Thru	R (Yield)	LT	Thru	R (Yield)
Delay (sec/veh)	23	30	14	22	32	28	17	25	11	19	28	13
LOS	C	C	B	C	C	D	B	C	B	B	C	B
v/c Ratio	0.70	0.60	0.30	0.40	0.55	0.60	0.70	0.70	0.15	0.25	0.65	0.20
Queue Length (ft)	250	200	50	125	150	100	200	375	25	50	250	25
Intersection	23 sec/veh											
	LOS C											

There are only minor differences of two seconds in vehicle delay in performance measures for the PM intersection operations with and without the development and the LOS remains C in both cases. The following table summarizes queue impacts.

**Table 26: PM Peak at Riverside Drive-Vintage Boulevard Intersection Queue Lengths**

	EB			WB			NB			SB		
	<i>LT</i>	<i>Thru</i>	<i>R (Yield)</i>	<i>LT</i>	<i>Thru</i>	<i>R (Yield)</i>	<i>LT</i>	<i>Thru</i>	<i>R (Yield)</i>	<i>LT</i>	<i>Thru</i>	<i>R (Yield)</i>
Available Storage Length (ft)	260	190*	190	140	110*	110	170	140*	140	180	140*	140
No-Build Queue Length (ft)	225	175	50	125	150	100	175	375	25	50	250	25
Build Queue Length (ft)	250	200	50	125	150	100	200	375	25	50	250	25

*\*Through queue storage is unlimited, but the lengths presented in the table are the shortest length of the adjoining auxiliary lanes to address the likelihood of through queues blocking the auxiliary lane.*

Upon opening of the development, the eastbound right-turn lane may be blocked on occasion by the through queue, but as mentioned previously it is not expected to be an issue. Similarly, the westbound left-turn may have to be accessed by vehicle entering the lane behind through queue over the painted channelization. Both northbound and southbound through queues block access to the adjoining acceleration lanes, but the additional site traffic does not worsen the queuing conditions.

**7.1.2.3 Riverside Drive and Vintage Boulevard/ Mendenhall Mall Road Summary**

Intersection operation measures and queue lengths are not significantly impacted by the traffic generated by the proposed SEARHC medical office development. The operational performance measures with the development do not exceed CBJ Code of Ordinance thresholds requiring mitigations (see Section 1.2 on page 5 for discussion of requirements).

**7.1.3 Mendenhall Mall Road and Mendenhall Loop Road/Atlin Drive**

This intersection, discussed in detail under Section 3.2.4 on page 15 and depicted below (Figure 26) is a signalized intersection.



Figure 26: Mendenhall Loop Road and Mendenhall Mall Road/Atlin Drive

**7.1.3.1 Mendenhall Loop Road and Mendenhall Mall Road/Atlin Drive AM Peak Hour**

The following tables summarize intersection LOS performance measures for the AM peak hour in the design year without the SEARHC medical offices and with the medical offices.

**Table 27: AM Peak at Mendenhall Loop Road-Mendenhall Mall Road Intersection, Base Condition 2023**

2023 Base Condition AM Peak	EB			WB			NB			SB		
	LT	Thru	R)	LT	Thru	R	LT	Thru	R)	LT	Thru	R)
Delay (sec/veh)	53		50	51		49	65		6	64	12	6
LOS	D		D	D		D	E		A	E	B	A
v/c Ratio	0.50		0.15	0.25		0.00	0.65		0.20	0.35	0.70	0.20
Queue Length (ft)	100		75	50		< 25	125		200	25	550	75
Intersection	15 sec/veh											
	LOS B											

**Table 28: AM Peak at Mendenhall Loop Road-Mendenhall Mall Road Intersection, Build Condition 2023**

2023 Build Condition AM Peak	EB			WB			NB			SB		
	LT	Thru	R (Yield)	LT	Thru	R (Yield)	LT	Thru	R (Yield)	LT	Thru	R (Yield)
Delay (sec/veh)	53		50	51		49	53	6		64	13	7
LOS	D		D	D		D	D	A		E	B	A
v/c Ratio	0.50		0.20	0.30		0.00	0.60	0.20		0.35	0.75	0.20
Queue Length (ft)	100		75	50		< 25	125	175		25	550	75
Intersection	16 sec/veh											
	LOS B											

There is only a minor difference of 1 second in vehicle delay in performance measures for the AM intersection operations with and without the development and the LOS is B in both cases. The following table summarizes queue impacts.

**Table 29: AM Peak at Mendenhall Loop Road-Mendenhall Mall Road Intersection Queue Lengths**

	EB			WB			NB			SB		
	LT	Thru	R	LT	Thru	R	LT	Thru	R	LT	Thru	R
Available Storage Length (ft)	250		120	70		40	190	590		110	110*	130
No-Build Queue Length (ft)	100		75	50		< 25	125	200		25	550	75
Build Queue Length (ft)	100		75	50		< 25	125	175		25	550	75

*\*Through queue storage is unlimited, but the lengths presented in the table are the shortest length of the adjoining auxiliary lanes to address the likelihood of through queues blocking the auxiliary lane.*

With site development traffic the southbound 550-foot through queue will occasionally block the entrance of the southbound left and right turn lanes. However, the build condition does not lengthen the southbound through queue.

**7.1.3.2 Mendenhall Loop Road-Mendenhall Mall Road/Atlin Drive PM Peak Hour**

The following tables summarize intersection LOS performance measures for the PM peak hour in the design year without the SEARHC medical offices and with the medical offices.

**Table 30: PM Peak at Riverside Drive-Vintage Boulevard Intersection, Base Condition 2023**

2023 Base Condition AM Peak	EB			WB			NB			SB		
	LT	Thru	R (Yield)	LT	Thru	R (Yield)	LT	Thru	R (Yield)	LT	Thru	R (Yield)
Delay (sec/veh)	63		32	31		31	145		15	54	19	16

2023 Base Condition AM Peak	EB			WB			NB			SB		
	LT	Thru	R (Yield)	LT	Thru	R (Yield)	LT	Thru	R (Yield)	LT	Thru	R (Yield)
LOS	E		C	C		C	F		B	D	B	B
v/c Ratio	0.90		0.20	0.05		0.00	1.15		0.80	0.45	0.45	0.15
Queue Length (ft)	275		75	25		< 25	525		625	25	200	50
Intersection	33 sec/veh											
	LOS C											

**Table 31: PM Peak at Riverside Drive-Vintage Boulevard Intersection, Build Condition 2023**

2023 Build Condition AM Peak	EB			WB			NB			SB		
	LT	Thru	R (Yield)	LT	Thru	R (Yield)	LT	Thru	R (Yield)	LT	Thru	R (Yield)
Delay (sec/veh)	66		32	31		30	198		16	54	19	16
LOS	E		C	C		C	F		B	D	B	B
v/c Ratio	0.90		0.20	0.05		0.00	1.30		0.80	0.45	0.45	0.15
Queue Length (ft)	300		100	25		< 25	525		625	25	200	50
Intersection	39 sec/veh											
	LOS D											

For the PM peak hour, there is a six second vehicle delay increase between the 2023 no build and build conditions. The LOS changes from C to D, but does not exceed CBJ Code of Ordinances mitigation thresholds.

**Table 32: PM Peak at Mendenhall Loop Road-Atlin Road Intersection Queue Lengths**

	EB			WB			NB			SB		
	LT	Thru	R	LT	Thru	R	LT	Thru	R	LT	Thru	R
Available Storage Length (ft)	120*		120	70		40	190	590		110	110*	130
No-Build Queue Length (ft)	275		75	25		< 25	525	625		25	200	50
Build Queue Length (ft)	300		100	25		< 25	525	625		25	200	50

*\*Through queue storage is unlimited, but the lengths presented in the table are the shortest length of the adjoining auxiliary lanes to address the likelihood of through queues blocking the auxiliary lane.*

Upon opening of the development, the eastbound approach queue lengths are increased for the shared left turn/thru and right turn lanes. The eastbound right-turn lane may be blocked on occasion by the left turn/through queue. While the eastbound shared left turn/thru lane

anticipated queue length for no-build and build conditions is longer than the storage length to where it would impact the entrance for the right turn lane, the queue lengths for the eastbound lanes are only increased by 25 feet. This is mitigated in part because the right turn movements occur simultaneously with the shared left-through phase. Both northbound and southbound through queues block access to the adjoining acceleration lanes, but the additional site traffic does not worsen the queuing conditions. The eastbound, westbound, and southbound right turn lane available queue length storages are all longer than the expected no build and build queue lengths.

### 7.1.3.3 Mendenhall Loop Road-Mendenhall Mall Road/Atlin Drive Summary

The operational performance measures with the development do not exceed the CBJ Code of Ordinance thresholds requiring mitigations (see Section 1.2 on page 5 for discussion of requirements).

### 7.1.4 Egan Drive and Mendenhall Loop Road

This intersection, discussed in detail under Section 3.2.2 and depicted below in Figure 27 is a signalized intersection.



Figure 27: Egan Drive and Mendenhall Loop Road



**7.1.4.1 Egan Drive and Mendenhall Loop Road AM Peak Hour**

The following tables summarize intersection LOS performance measures for the AM peak hour in the design year without the SEARHC medical offices and with the medical offices.

**Table 33: AM Peak at Egan Drive-Mendenhall Loop Road Intersection, Base Condition 2023**

2023 Base Condition AM Peak	EB			WB			NB			SB		
	LT	Thru	R	LT	Thru	R (Yield)	LT	Thru	R	LT	Thru	R
Delay (sec/veh)	64	141	34	90	45	0	65	57	49	81	84	25
LOS	E	F	C	F	D	Free	E	E	D	F	F	C
v/c Ratio	0.65	1.20	0.10	0.55	0.55	-	0.75	0.65	0.05	1.10	1.10	0.10
Queue Length (ft)	100	800	75	25	250	-	175	150	< 25	950	975	50
Intersection	82 sec/veh											
	LOS F											

**Table 34: AM Peak at Egan Drive-Mendenhall Loop Road Intersection, Build Condition 2023**

2023 Build Condition AM Peak	EB			WB			NB			SB		
	LT	Thru	R	LT	Thru	R (Yield)	LT	Thru	R	LT	Thru	R
Delay (sec/veh)	64	147	34	90	46	0	66	57	48	80	83	24
LOS	E	F	C	F	D	Free	E	E	D	F	F	C
v/c Ratio	0.65	1.20	0.10	0.55	0.60	-	0.75	0.65	0.05	1.10	1.10	0.10
Queue Length (ft)	100	800	75	25	250	-	175	150	< 25	950	975	50
Intersection	83 sec/veh											
	LOS F											

The Mendenhall Loop and Mendenhall Mall Road/Atlin Drive intersection has a no-build intersection LOS of F. The build condition is also LOS F, with a 1 second increase in delay. The following table summarizes queue impacts.

**Table 35: AM Peak at Egan Drive-Mendenhall Loop Road Intersection Queue Lengths**

	EB			WB			NB			SB		
	LT	Thru	R	LT	Thru	R (Yield)	LT	Thru	R	LT	Thru	R
Available Storage Length (ft)	700	150*	150	720	720*	980	300	300*	470	960	280*	280
No-Build Queue Length (ft)	100	800	75	25	250	-	175	150	< 25	950	975	50
Build Queue Length (ft)	100	800	75	25	250	-	175	150	< 25	950	975	50

*\*Through queue storage is unlimited, but the lengths presented in the table are the shortest length of the adjoining auxiliary lanes to address the likelihood of through queues blocking the auxiliary lane.*

Queues are not increased with the additional site traffic.

**7.1.4.2 Egan Drive and Mendenhall Loop Road PM Peak Hour**

The following tables summarize intersection LOS performance measures for the PM peak hour in the design year without the SEARHC medical offices and with the medical offices.

**Table 36: PM Peak at Egan Drive and Mendenhall Loop Road Intersection, Base Condition 2023**

2023 Base Condition PM Peak	EB			WB			NB			SB		
	LT	Thru	R	LT	Thru	R (Yield)	LT	Thru	R	LT	Thru	R
Delay (sec/veh)	42	24	19	49	168	Free	75	85	33	75	71	30
LOS	D	C	B	D	F	Free	E	F	C	E	E	C
v/c Ratio	0.65	0.60	0.10	0.50	1.25	Free	0.95	1.00	0.00	1.00	1.00	0.05
Queue Length (ft)	125	250	50	75	625	Free	375	425	< 25	475	475	< 25
Intersection	65 sec/veh											
	LOS E											

**Table 37: PM Peak at Egan Drive and Mendenhall Loop Road Intersection, Build Condition 2023**

2023 Build Condition PM Peak	EB			WB			NB			SB		
	LT	Thru	R	LT	Thru	R (Yield)	LT	Thru	R	LT	Thru	R
Delay (sec/veh)	43	24	18	49	172	Free	79	85	33	78	74	30
LOS	D	C	B	D	F	Free	E	F	C	E	E	C
v/c Ratio	0.65	0.60	0.10	0.50	1.30	Free	0.95	1.00	0.00	1.05	1.00	0.05
Queue Length (ft)	125	225	75	75	625	Free	375	425	< 25	500	500	< 25
Intersection	66 sec/veh											
	LOS E											

There is a one second vehicle delay increase at this intersection; however, the LOS remains E for both the no-build and build conditions. Through queue will occasionally block the entrance of the left and right turn lanes. The following table summarizes queue impacts.

**Table 38: PM Peak at Egan Drive-Mendenhall Loop Road Queue Lengths**

	EB			WB			NB			SB		
	LT	Thru	R	LT	Thru	R (Yield)	LT	Thru	R	LT	Thru	R
Available Storage Length (ft)	700	150*	150	720	720*	980	300	300*	470	960	280*	280
No-Build Queue Length (ft)	125	250	50	75	625	Free	375	425	< 25	475	475	< 25
Build Queue Length (ft)	125	225	75	75	625	Free	375	425	< 25	500	500	< 25

*\*Through queue storage is unlimited, but the lengths presented in the table are the shortest length of the adjoining auxiliary lanes to address the likelihood of through queues blocking the auxiliary lane.*

Upon opening of the development, the eastbound, northbound, and southbound right-turn lane may be blocked on occasion by the through queue, but the additional site traffic does not significantly worsen the queuing conditions. There is a 25-foot increase in southbound left and through turning movements as well as a 25-foot increase in length in the eastbound right turn queue.

**7.1.4.3 Egan Drive and Mendenhall Loop Road Summary**

If interpreted correctly, the operational performance measures with the development may exceed CBJ Code of Ordinance thresholds requiring mitigations (see Section 1.2 on page 5 for discussion of requirements) simply because the base condition without site traffic falls below

LOS D. However, intersection operation measures and queue lengths are not significantly impacted by the traffic generated by additional traffic from the proposed SEARHC medical office development.

### 7.1.5 Egan Drive and Riverside Drive

This intersection, discussed in detail under Section 3.2.1 on page 14 and depicted below in Figure 28, is a signalized intersection.



Figure 28: Egan Drive and Riverside Drive

#### 7.1.5.1 Egan Drive and Riverside Drive AM Peak Hour

The following tables summarize intersection LOS performance measures for the AM peak hour in the design year without the SEARHC medical offices and with the medical offices.

**Table 39: AM Peak at Egan Drive and Riverside Drive Intersection, Base Condition 2023**

2023 Base Condition AM Peak	EB		WB		SB	
	<i>LT</i>	<i>Thru</i>	<i>Thru</i>	<i>R (Yield)</i>	<i>LT</i>	<i>R (Yield)</i>
Delay (sec/veh)	27	9	24	18	22	18
LOS	C	A	C	B	C	B
v/c Ratio	0.60	0.45	0.70	0.10	0.65	0.30
Queue Length (ft)	175	150	225	50	225	100
Intersection	19 sec/veh					
	LOS B					

**Table 40: AM Peak at Egan Drive and Riverside Drive Road Intersection, Build Condition 2023**

2023 Base Condition AM Peak	EB		WB		SB	
	<i>LT</i>	<i>Thru</i>	<i>Thru</i>	<i>R (Yield)</i>	<i>LT</i>	<i>R (Yield)</i>
Delay (sec/veh)	28	9	25	19	23	19
LOS	C	A	C	B	C	B
v/c Ratio	0.65	0.45	0.70	0.10	0.70	0.30
Queue Length (ft)	175	175	225	50	225	100
Intersection	19 sec/veh					
	LOS B					

The intersection LOS is B with both no-build and build conditions and no change in average delay. The following table summarizes queue impacts.

**Table 41: AM Peak at Egan Drive and Riverside Drive Intersection Queue Lengths**

	EB		WB		SB	
	<i>LT</i>	<i>Thru</i>	<i>Thru</i>	<i>R (Yield)</i>	<i>LT</i>	<i>R(Yield)</i>
Available Queue Length (ft)	370	370*	530*	530	460	230
Pre-Build	175	150	225	50	225	100
Post-Build	175	175	225	50	225	100

\*Through queue storage is unlimited, but the lengths presented in the table are the shortest length of the adjoining auxiliary lanes to address the likelihood of through queues blocking the auxiliary lane.

Through queue should not block the entrance of the left or right turn lanes. The build condition has insignificant impact on any of the left, right or through queues.

**7.1.5.2 Egan Drive and Riverside Drive PM Peak Hour**

The following tables summarize intersection LOS performance measures for the PM peak hour in the design year without the SEARHC medical offices and with the medical offices.

**Table 42: PM Peak at Egan Drive and Riverside Drive Intersection, Base Condition 2023**

2023 Base Condition AM Peak	EB		WB		SB	
	LT	Thru	Thru	R (Yield)	LT	R (Yield)
Delay (sec/veh)	38	5	15	34	44	36
LOS	D	A	B	C	D	D
v/c Ratio	0.75	0.20	0.75	0.35	0.75	0.15
Queue Length (ft)	325	75	175	< 25	175	75
Intersection	25 sec/veh					
	LOS C					

**Table 43: PM Peak at Egan Drive and Riverside Drive Intersection, Build Condition 2023**

2023 Base Condition AM Peak	EB		WB		SB	
	LT	Thru	Thru	R (Yield)	LT	R (Yield)
Delay (sec/veh)	38	5	15	38	44	35
LOS	D	A	B	D	D	D
v/c Ratio	0.75	0.20	0.80	0.35	0.75	0.15
Queue Length (ft)	350	75	175	< 25	175	75
Intersection	26 sec/veh					
	LOS C					

There are only minor differences in performance measures for the PM intersection operations with and without the development. The following table summarizes queue impacts.

**Table 44: PM Peak at Egan Drive and Riverside Drive Queue Lengths**

	EB		WB		SB	
	LT	THRU	LT	R (Yield)	LT	R
Available Queue Length (ft)	370	740	1200	530	460	230
Pre-Build	325	75	175	< 25	175	75
Post-Build	350	75	175	< 25	175	75

*\*Through queue storage is unlimited, but the lengths presented in the table are the shortest length of the adjoining auxiliary lanes to address the likelihood of through queues blocking the auxiliary lane.*

Through queue should not block the entrance of the left or right turn lanes. The build condition has insignificant impact on any of the left, right or through queues.



### **7.1.5.3 Egan Drive and Riverside Drive Summary**

Intersection operation measures and queue lengths are not significantly impacted by the traffic generated by the proposed SEARHC medical office development. The operational performance measures with the development do not exceed CBJ Code of Ordinance thresholds requiring mitigations (see Section 1.2 on page 5 for discussion of requirements).

## **7.2 Safety Impacts**

Crash experience is discussed under Section 5 on page 26. The crash rates and severity are low for the intersections within the study area. The site traffic may increase crash frequency (number per year) for the study intersections, but crash rates (crashes per volume exposure) is not expected to increase and in fact, may be reduced (rates tend to fall as traffic increases). Similarly, crash severity (major injuries and fatalities) are low, and would not be subject to an increase with the additional site traffic.

## **7.3 Transit System Impacts**

Section 3.4 discusses transit service in the area. As a community, Juneau has an average public transportation work trip mode share is about 4.6% of commuting trips. As US Census Bureau American Community Survey *Table S0802* shows CBJ transit patrons tend to be lower income. As such, there is a potential and benefit of incentivizing transit so that all income levels employed by the medical office consider transit as alternative mode for commutes.. If so, and applying the community 4.6% usage, we would expect to see about 4 to 10 work trips use transit during the morning and evening commuting hours.

Moreover, about 70 to 80 trips daily (of 1677 total) would probably use transit if the mode share were applied to the total site traffic.

Section 3.4 indicates that the current transit system routes and schedules have adequate capacity to carry the patrons generated by the proposed SEARHC medical offices.

## **7.4 Pedestrian and Bicycle Impacts**

The proposed SEARHC medical offices will generate additional pedestrian and bicycle use. In addition to the commuter and patient mode share (in place of automobiles), there will be short-trip opportunities for staff walkers or cyclists during the day to shop or visit restaurants.

The study area has pedestrian and bicycle facilities in place, as discussed under Section 3.3, although not every street has sidewalks or pathways. However, the study area pedestrian and bicycle network will accommodate walking or biking between the proposed medical offices and study area destinations study area or external origins/destinations.

Within the immediate area of the proposed offices, there is a missing sidewalk segment fronting the medical office parcel which, if left unimproved, impacts pedestrian circulation and affects the

quality of walking experience. Completion of this sidewalk would enhance the connectivity of the non-motorized network facilities for pedestrian staff and patients, reduce unsignalized mid-block street crossings, and provide an improved walking route for commuters or patients who walk between the offices and the Mendenhall Mall stops to board or alight transit buses. However, the walking route along Mendenhall Mall road is on shoulders or parking areas.

## 7.5 Driveway Impacts

### 7.5.1 Operational Impacts

Composite driveway volumes were estimated for the AM and PM peak hours in 2023 using data extracted and interpreted from Figure 21, Figure 22, and Table 13 (counts are not available). Of primary concerns are the left-turn queues for traffic entering the site be contained in the existing storage lanes, and that delay for the driveway egress traffic is not excessive, which if so, may result in drivers accepting inadequate gaps and increase the likelihood of crashes.

Figure 29, below, presents these performance measures. Queues will be contained in the left-turn storage lanes and delays for the outbound approach traffic onto Vintage will not be excessive.



Background Photo Source- Google Earth

Figure 29: Driveway Performance Measures

### 7.5.2 Driveway Sight Distance

Sight distance is required for main street left-turning vehicles and for driveway egress vehicles. The American Association of State Highway and Transportation Officials (AASHTO) *A Policy on the Geometric Design of Highways and Streets (GDHS)* provides guidance on intersection and driveway sight distance. The sight distance requirements are based upon Vintage Boulevard’s posted speed limit of 20 mph.

Sight distance will have to be evaluated and incorporated during site design. The discussion below only presents the criteria that should be observed.

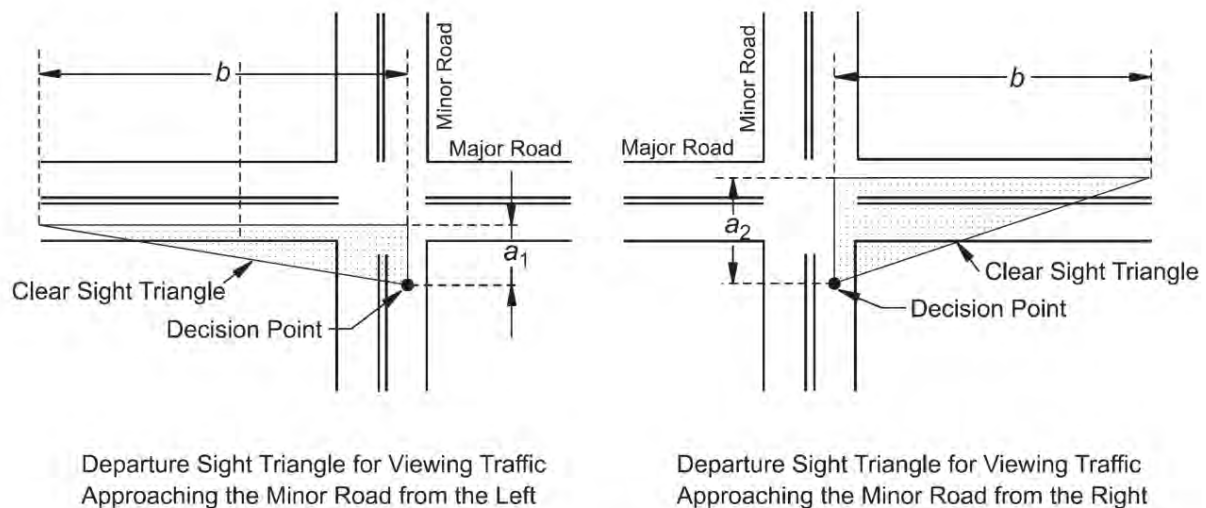
#### 7.5.2.1 Vintage Boulevard Left-Turns Into Site

AASHTO requires 115 feet minimum for sight distance that will allow vehicles traveling in the opposing direction enough sight distance to come to a full stop from 20 mph, and avoid a left turning vehicle. However, a desirable sight distance is 165 feet, allowing the left-turning vehicle enough sight distance to judge oncoming gaps and complete the turn without impeding the opposite direction vehicle.

#### 7.5.2.2 Driveway Departure Sight Distance

AASHTO requires 115 feet minimum for departure sight distance that will allow vehicles on Vintage Boulevard enough sight distance to come to a full stop from 20 mph, and avoid the driveway departure (egress) vehicle. The desirable sight distance is 225 feet.

Departure sight triangles are shown below.



Source: AASHTO GDHS Figure 9-17

Notes: The distance  $a_1$  is distance from center of near lane to the decision point located 15 feet from edge of travel way. The distance  $a_2$  is distance from center of far lane to the decision point located 15 feet from edge of travel way. The distance  $b$  is the sight distance, minimum 115 feet or desirable 225 feet.

**Figure 30: Departure Sight Triangles**

All objects within the clear sight triangle should be less than the plane formed by the driver's eye at 3.5 feet above the driveway surface and at the lane vertices, 3.5 above the roadway surfaces.

## 8 Mitigation and Recommendations

### 8.1 Egan Drive and Mendenhall Loop Road

Without site traffic, intersection AM average delay per vehicle is 82 seconds with a LOS F and the PM average delay per vehicle is 65 seconds with LOS E. Adding site traffic only increases delay by 1 second per vehicle in both AM and PM peak hours with no changes in LOS.

If interpreted correctly, the operational performance measures with the development may exceed CBJ Code of Ordinance thresholds requiring mitigations (see Section 1.2 on page 5 for discussion of requirements) simply because the base condition without site traffic falls below LOS D. However, intersection operation measures and queue lengths are not significantly impacted by the traffic generated by additional traffic from the proposed SEARHC medical office development. Furthermore, DOT&PF uses adaptive signal control at this intersection, and that capability to respond to changing demands is a mitigation.

A sketch-level evaluation of the intersection capacity analysis indicates that engineering and construction mitigation would likely require expanding the northbound and southbound approaches with additional left-turn and through capacity. This essentially involves an entire intersection and signal reconstruction, with likely unforeseen utility and right-of-way impacts, and would likely cost several million dollars. Moreover, the West Egan Drive Corridor Study, discussed in Section 2.3.4, indicates that the long-term solution for this intersection is a grade separated interchange. As such, any reconstruction efforts required by this project would eventually be superseded.

CBJ Code of Ordinances, 49.40.340 Mitigation waiver, discusses mitigation waivers:

- (a) *The planning commission or community development department director may, in their discretion, waive or partially waive the requirements for mitigation under this section if the planning commission finds at a public hearing, or the director finds in writing after reviewing a permit which does not require planning commission approval, that either of the following circumstances is true:*
  - (1) (A) *Existing roadway facilities are only marginally achieving an LOS D without the traffic generated by the development, and would likely fall below LOS D within five years;*
  - (B) *Traffic generated by the development would result in an LOS below D without mitigation; and*
  - (C) *The costs of mitigating the impacts outweighs the benefits; or*
  - (2) (A) *If the LOS is below D. before the development's opening date;*
  - (B) *If the operation of the roadway or intersection, within the affected area, would not deteriorate more than five percent in terms of delay time, a minimum LOS, LOS E may be acceptable;*
  - (C) *Does not result in an LOS below E; and*

*(D) The costs of mitigating the impacts outweighs the benefits.*

The CBJ Code of Ordinances will allow mitigation to be waived, based on clause (D).

## **8.2 Completion of the Vintage Boulevard Sidewalk Fronting the SEARHC Medical Offices**

As part of the site design and construction for the SEARHC medical offices, the missing sidewalk segment fronting this parcel could be completed, thus minimizing impacts to pedestrians generated by the facility. This segment completion enhances pedestrian mobility and safety and promotes transit use by improving the walking route between the bus stops and the medical offices. It should be noted that part of the transit walking route along Mendenhall Mall Road between the bus stops and the intersection of Riverside-Vintage-Mendenhall Mall is on roadway shoulders.

This improvement would mitigate the safety and mobility impacts caused by the missing sidewalk segment. It provides continuous and connecting pedestrian walking routes in the vicinity of the medical offices and reduce the need for mid-block unsignalized pedestrian crossings on Vintage Boulevard.

This improvement could also be a mitigation for the Egan Drive and Mendenhall Loop Road operational issues discussed above. The improvement may promote mode changes to walking and transit, and reduce vehicular demand generated by the site.

## **8.3 Demand Management Policies By SEARHC**

Site vehicular traffic and parking demand may be reduced by SEARHC with the implementation of policies that promote changes to active the active transportation modes of transit, walking, or biking, and by encouraging car-pools. These include:

- Incentivize transit use (potential 4% to 5% vehicle trip reduction, see Section 3.4)
- Incentivize carpooling (potential 7% to 8% vehicle trip reduction, see Section 3.4)
- Provide bicycle racks. (Vehicle trip reduction isn't quantifiable).



## 8.4 Recommendations

Both DOT&PF and CBJ have reviewed the draft TIA and concur with the above discussed mitigations. Recommendations are as follows:

- Adaptive signal control capability at Egan Drive and Mendenhall Loop Road will be adequate to address the computed 1 second of delay increase at this intersection because of site traffic, and no other mitigation is required.
- Construct the missing segment of the Vintage Boulevard sidewalk that fronts the SEARHC Medical Offices (this has been adopted and shown in the May 2022 site plan).
- Include demand management policies that include incentivizing transit use and carpooling, and provide bicycle racks.

## **Appendix A- Excerpts from ITE Trip Generation Manual for Project Land Uses**

# Land Use: 720

## Medical-Dental Office Building

### Description

A medical-dental office building is a facility that provides diagnoses and outpatient care on a routine basis but is unable to provide prolonged in-house medical and surgical care. One or more private physicians or dentists generally operate this type of facility. Clinic (Land Use 630) is a related use.

### Additional Data

Time-of-day distribution data for this land use for a weekday, Saturday, and Sunday are presented in Appendix A. For the 19 general urban/suburban sites with data, the overall highest vehicle volumes during the AM and PM on a weekday were counted between 9:30 and 10:30 a.m. and 2:15 and 3:15 p.m., respectively.

The sites were surveyed in the 1980s, the 1990s, the 2000s, and the 2010s in Alberta (CAN), California, Connecticut, Kentucky, Maryland, Minnesota, New Jersey, New York, Ohio, Oregon, Pennsylvania, South Dakota, Texas, Virginia, Washington, and Wisconsin.

### Source Numbers

104, 109, 120, 157, 184, 209, 211, 253, 287, 294, 295, 304, 357, 384, 404, 407, 423, 444, 509, 601, 715, 867, 879, 901, 902, 908, 959, 972

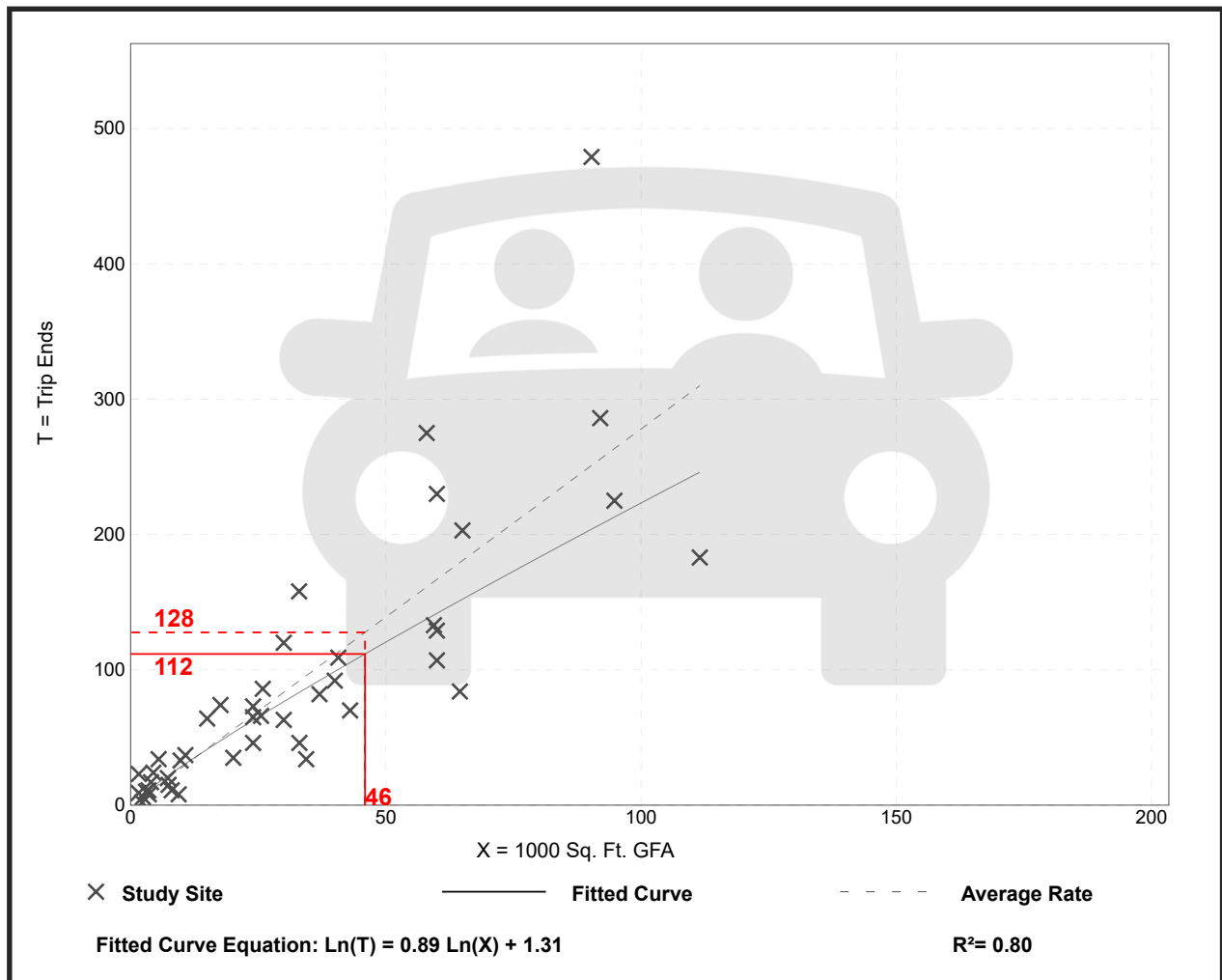
# Medical-Dental Office Building (720)

Vehicle Trip Ends vs: 1000 Sq. Ft. GFA  
 On a: Weekday,  
 Peak Hour of Adjacent Street Traffic,  
 One Hour Between 7 and 9 a.m.  
 Setting/Location: General Urban/Suburban  
 Number of Studies: 44  
 Avg. 1000 Sq. Ft. GFA: 32  
 Directional Distribution: 78% entering, 22% exiting

## Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
2.78	0.85 - 14.30	1.28

## Data Plot and Equation



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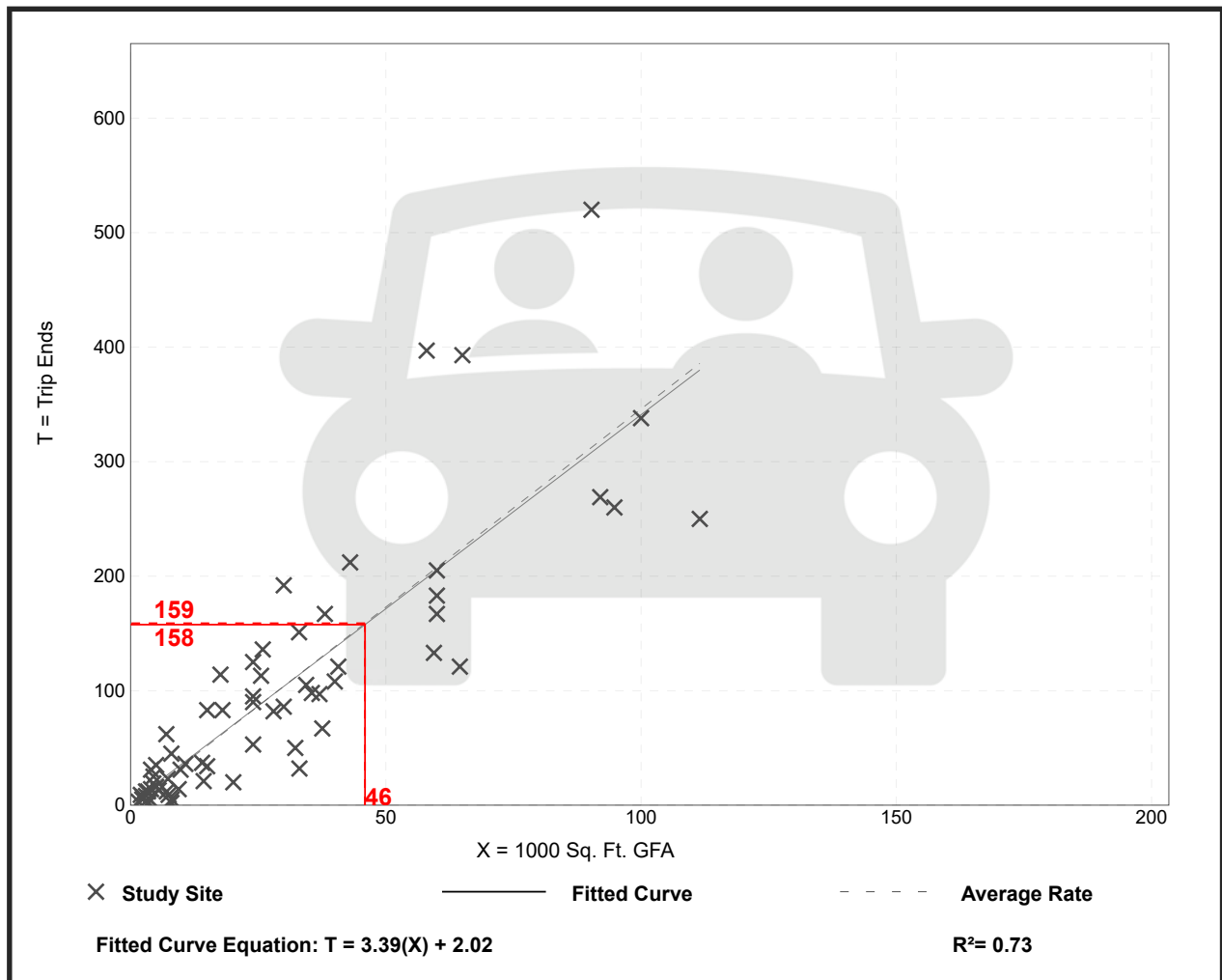
# Medical-Dental Office Building (720)

**Vehicle Trip Ends vs: 1000 Sq. Ft. GFA**  
**On a: Weekday,**  
**Peak Hour of Adjacent Street Traffic,**  
**One Hour Between 4 and 6 p.m.**  
**Setting/Location: General Urban/Suburban**  
 Number of Studies: 65  
 Avg. 1000 Sq. Ft. GFA: 28  
 Directional Distribution: 28% entering, 72% exiting

## Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
3.46	0.25 - 8.86	1.58

## Data Plot and Equation



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# Medical-Dental Office Building (720)

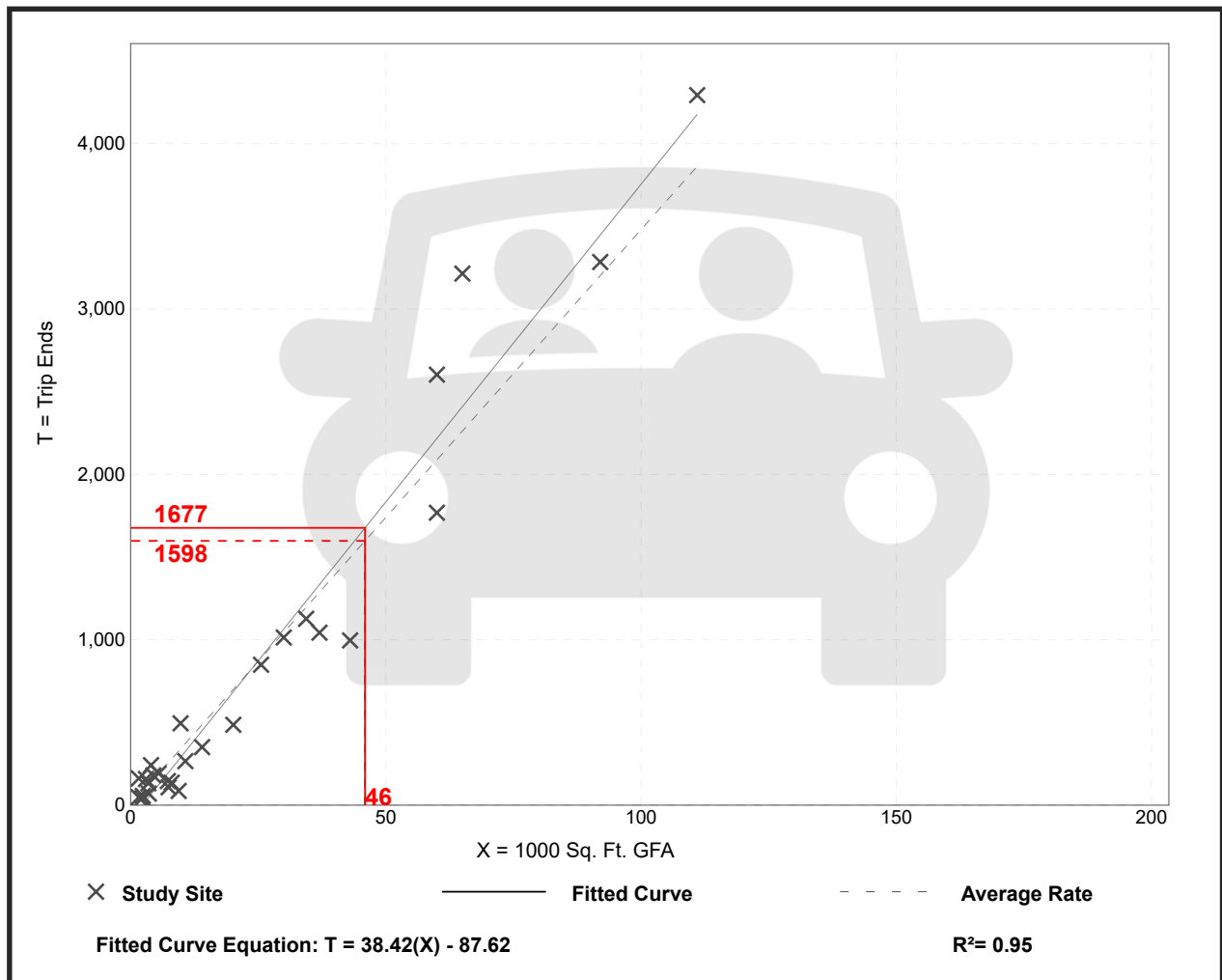
Vehicle Trip Ends vs: 1000 Sq. Ft. GFA  
On a: Weekday

Setting/Location: General Urban/Suburban  
Number of Studies: 28  
Avg. 1000 Sq. Ft. GFA: 24  
Directional Distribution: 50% entering, 50% exiting

## Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
34.80	9.14 - 100.75	9.79

## Data Plot and Equation



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# Land Use: 221

## Multifamily Housing (Mid-Rise)

### Description

Mid-rise multifamily housing includes apartments, townhouses, and condominiums located within the same building with at least three other dwelling units and that have between three and 10 levels (floors). Multifamily housing (low-rise) (Land Use 220), multifamily housing (high-rise) (Land Use 222), off-campus student apartment (Land Use 225), and mid-rise residential with 1st-floor commercial (Land Use 231) are related land uses.

### Additional Data

In prior editions of *Trip Generation Manual*, the mid-rise multifamily housing sites were further divided into rental and condominium categories. An investigation of vehicle trip data found no clear differences in trip making patterns between the rental and condominium sites within the ITE database. As more data are compiled for future editions, this land use classification can be reinvestigated.

For the six sites for which both the number of residents and the number of occupied dwelling units were available, there were an average of 2.46 residents per occupied dwelling unit.

For the five sites for which the numbers of both total dwelling units and occupied dwelling units were available, an average of 95.7 percent of the total dwelling units were occupied.

Time-of-day distribution data for this land use are presented in Appendix A. For the eight general urban/suburban sites with data, the overall highest vehicle volumes during the AM and PM on a weekday were counted between 7:00 and 8:00 a.m. and 4:45 and 5:45 p.m., respectively.

For the four dense multi-use urban sites with 24-hour count data, the overall highest vehicle volumes during the AM and PM on a weekday were counted between 7:15 and 8:15 a.m. and 4:15 and 5:15 p.m., respectively. For the three center city core sites with 24-hour count data, the overall highest vehicle volumes during the AM and PM on a weekday were counted between 6:45 and 7:45 a.m. and 5:00 and 6:00 p.m., respectively.

For the six sites for which data were provided for both occupied dwelling units and residents, there was an average of 2.46 residents per occupied dwelling unit.

For the five sites for which data were provided for both occupied dwelling units and total dwelling units, an average of 95.7 percent of the units were occupied.

The average numbers of person trips per vehicle trip at the five center city core sites at which both person trip and vehicle trip data were collected were as follows:

- 1.84 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 7 and 9 a.m.
- 1.94 during Weekday, AM Peak Hour of Generator
- 2.07 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 4 and 6 p.m.
- 2.59 during Weekday, PM Peak Hour of Generator

The average numbers of person trips per vehicle trip at the 32 dense multi-use urban sites at which both person trip and vehicle trip data were collected were as follows:

- 1.90 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 7 and 9 a.m.
- 1.90 during Weekday, AM Peak Hour of Generator
- 2.00 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 4 and 6 p.m.
- 2.08 during Weekday, PM Peak Hour of Generator

The average numbers of person trips per vehicle trip at the 13 general urban/suburban sites at which both person trip and vehicle trip data were collected were as follows:

- 1.56 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 7 and 9 a.m.
- 1.88 during Weekday, AM Peak Hour of Generator
- 1.70 during Weekday, Peak Hour of Adjacent Street Traffic, one hour between 4 and 6 p.m.
- 2.07 during Weekday, PM Peak Hour of Generator

The sites were surveyed in the 1980s, the 1990s, the 2000s, and the 2010s in Alberta (CAN), British Columbia (CAN), California, Delaware, District of Columbia, Florida, Georgia, Illinois, Maryland, Massachusetts, Minnesota, New Hampshire, New Jersey, Ontario, Oregon, Pennsylvania, South Carolina, South Dakota, Tennessee, Utah, Virginia, and Wisconsin.

### Source Numbers

168, 188, 204, 305, 306, 321, 357, 390, 436, 525, 530, 579, 638, 818, 857, 866, 901, 904, 910, 912, 918, 934, 936, 939, 944, 947, 948, 949, 959, 963, 964, 966, 967, 969, 970

# Multifamily Housing (Mid-Rise) (221)

Vehicle Trip Ends vs: Dwelling Units  
 On a: Weekday,  
 Peak Hour of Adjacent Street Traffic,  
 One Hour Between 7 and 9 a.m.

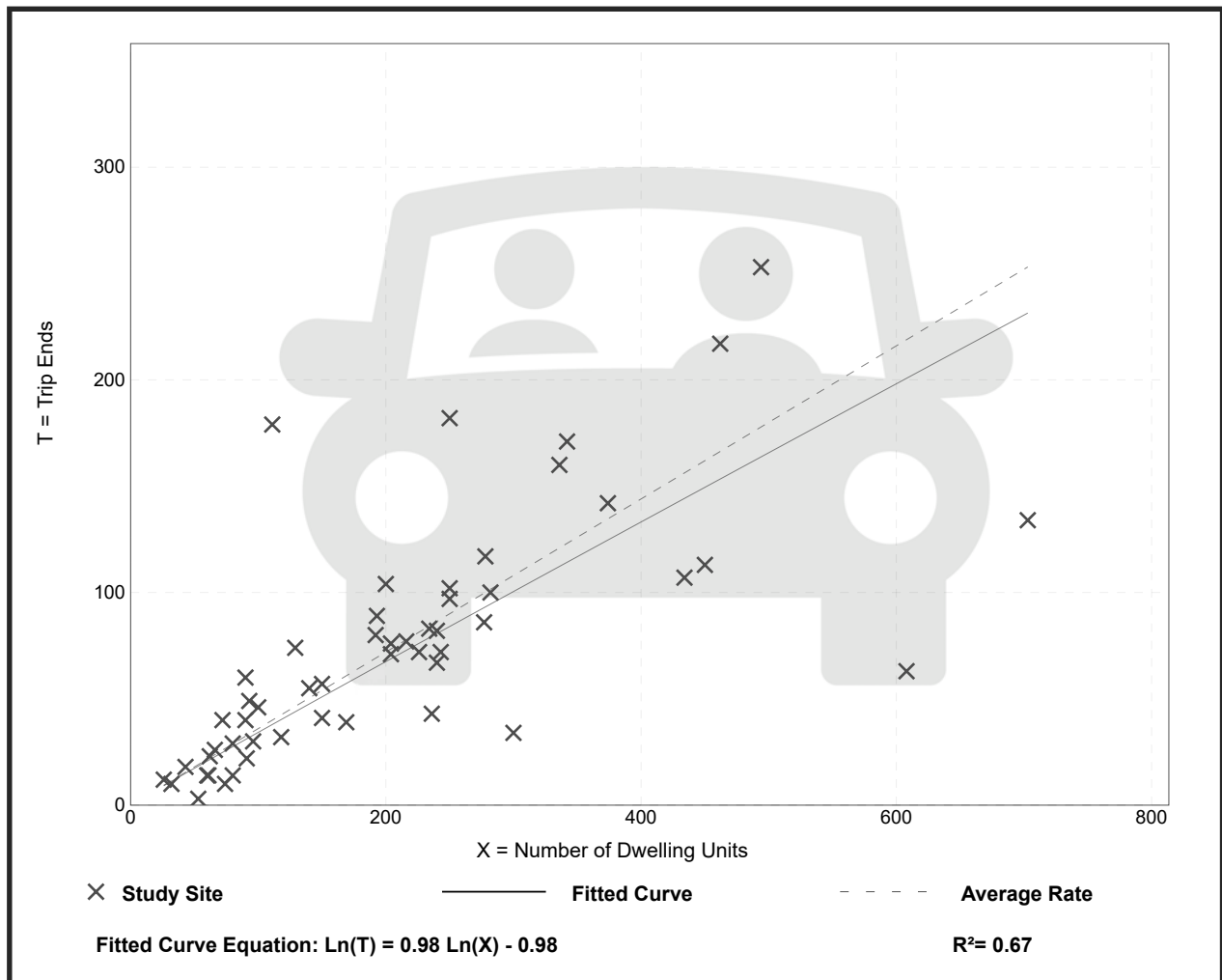
Setting/Location: General Urban/Suburban

Number of Studies: 53  
 Avg. Num. of Dwelling Units: 207  
 Directional Distribution: 26% entering, 74% exiting

## Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.36	0.06 - 1.61	0.19

## Data Plot and Equation



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# Multifamily Housing (Mid-Rise) (221)

Vehicle Trip Ends vs: Dwelling Units  
 On a: Weekday,  
 Peak Hour of Adjacent Street Traffic,  
 One Hour Between 4 and 6 p.m.

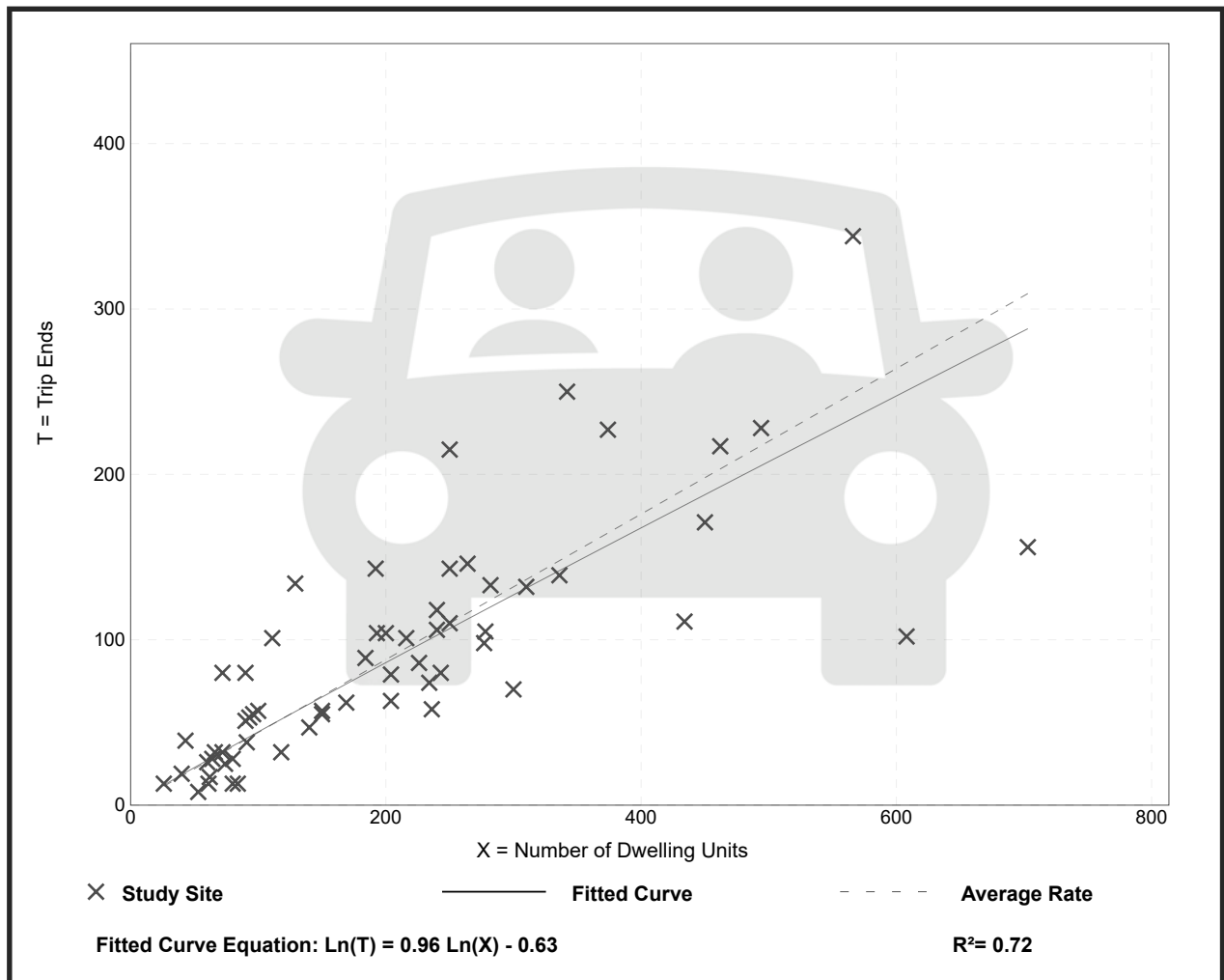
Setting/Location: General Urban/Suburban

Number of Studies: 60  
 Avg. Num. of Dwelling Units: 208  
 Directional Distribution: 61% entering, 39% exiting

## Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.44	0.15 - 1.11	0.19

## Data Plot and Equation



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# Multifamily Housing (Mid-Rise) (221)

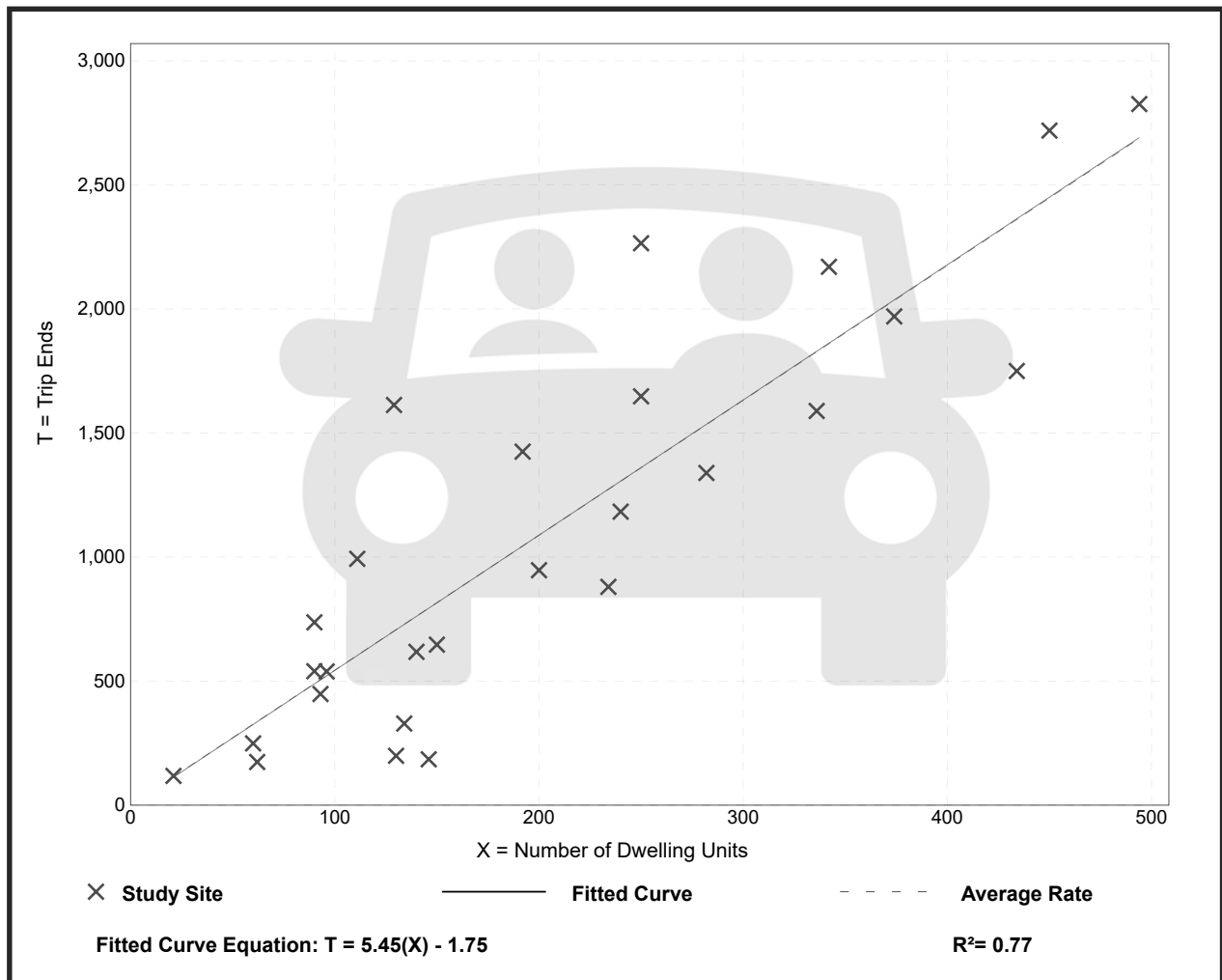
Vehicle Trip Ends vs: Dwelling Units  
On a: Weekday

Setting/Location: General Urban/Suburban  
Number of Studies: 27  
Avg. Num. of Dwelling Units: 205  
Directional Distribution: 50% entering, 50% exiting

## Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
5.44	1.27 - 12.50	2.03

## Data Plot and Equation



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## Land Use: 252

### Senior Adult Housing—Attached

#### Description

Senior adult housing consists of attached independent living developments, including retirement communities, age-restricted housing, and active adult communities. These developments may include limited social or recreational services. However, they generally lack centralized dining and onsite medical facilities. Residents in these communities live independently, are typically active (requiring little to no medical supervision) and may or may not be retired. Senior adult housing—detached (Land Use 251), congregate care facility (Land Use 253), assisted living (Land Use 254), and continuing care retirement community (Land Use 255) are related uses.

#### Additional Data

Time-of-day distribution data for this land use are presented in Appendix A. For the one general urban/suburban site with data, the overall highest vehicle volumes during the AM and PM on a weekday were counted between 11:45 a.m. and 12:45 p.m. and 12:00 and 1:00 p.m., respectively.

The sites were surveyed in the 1980s, the 1990s, and the 2000s in Alberta (CAN), California, Illinois, New Hampshire, New Jersey, New York, and Pennsylvania.

#### Source Numbers

272, 501, 576, 602, 703, 734, 741, 902, 970



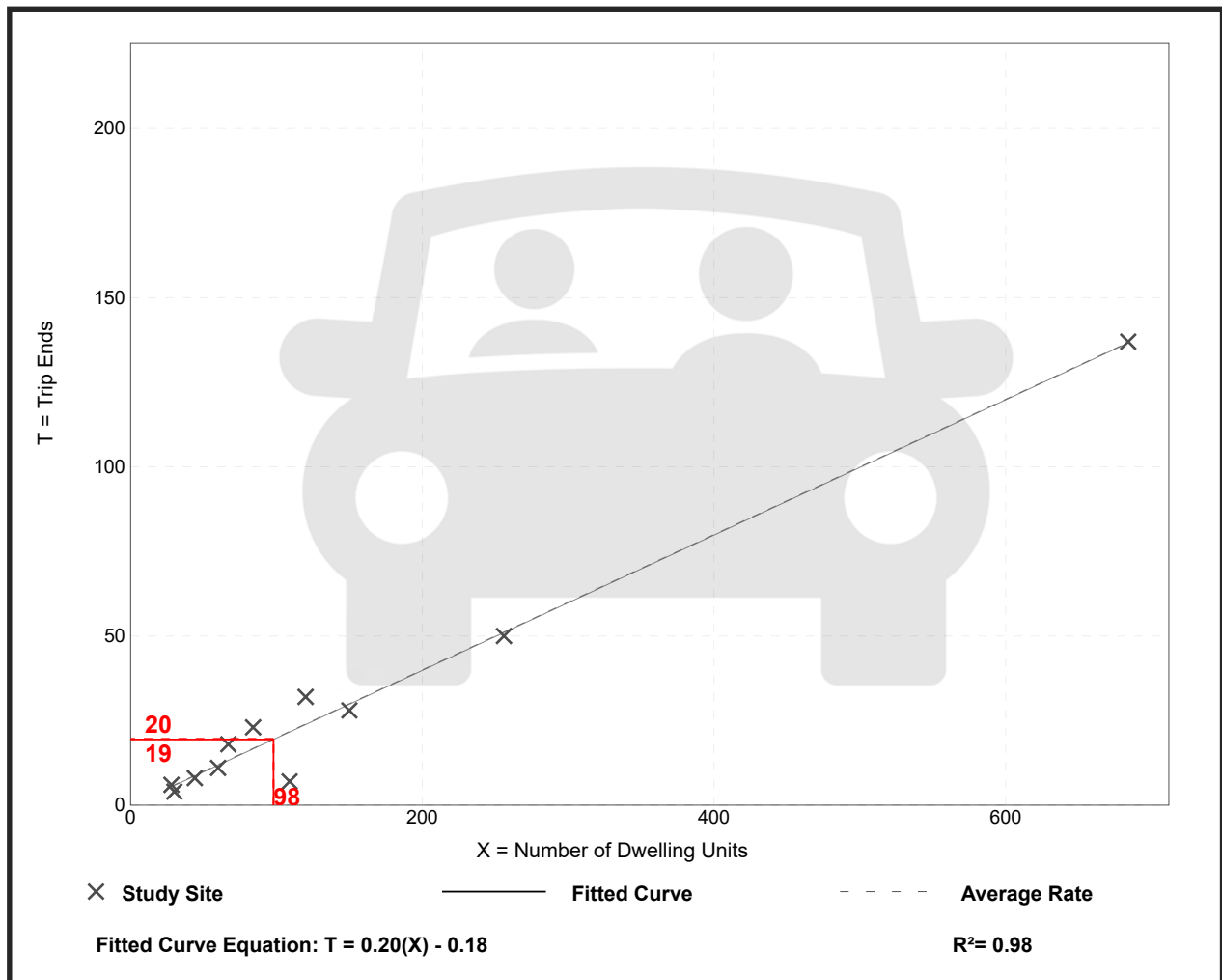
## Senior Adult Housing - Attached (252)

**Vehicle Trip Ends vs: Dwelling Units**  
**On a: Weekday,**  
**Peak Hour of Adjacent Street Traffic,**  
**One Hour Between 7 and 9 a.m.**  
**Setting/Location: General Urban/Suburban**  
 Number of Studies: 11  
 Avg. Num. of Dwelling Units: 148  
 Directional Distribution: 35% entering, 65% exiting

### Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.20	0.06 - 0.27	0.05

### Data Plot and Equation



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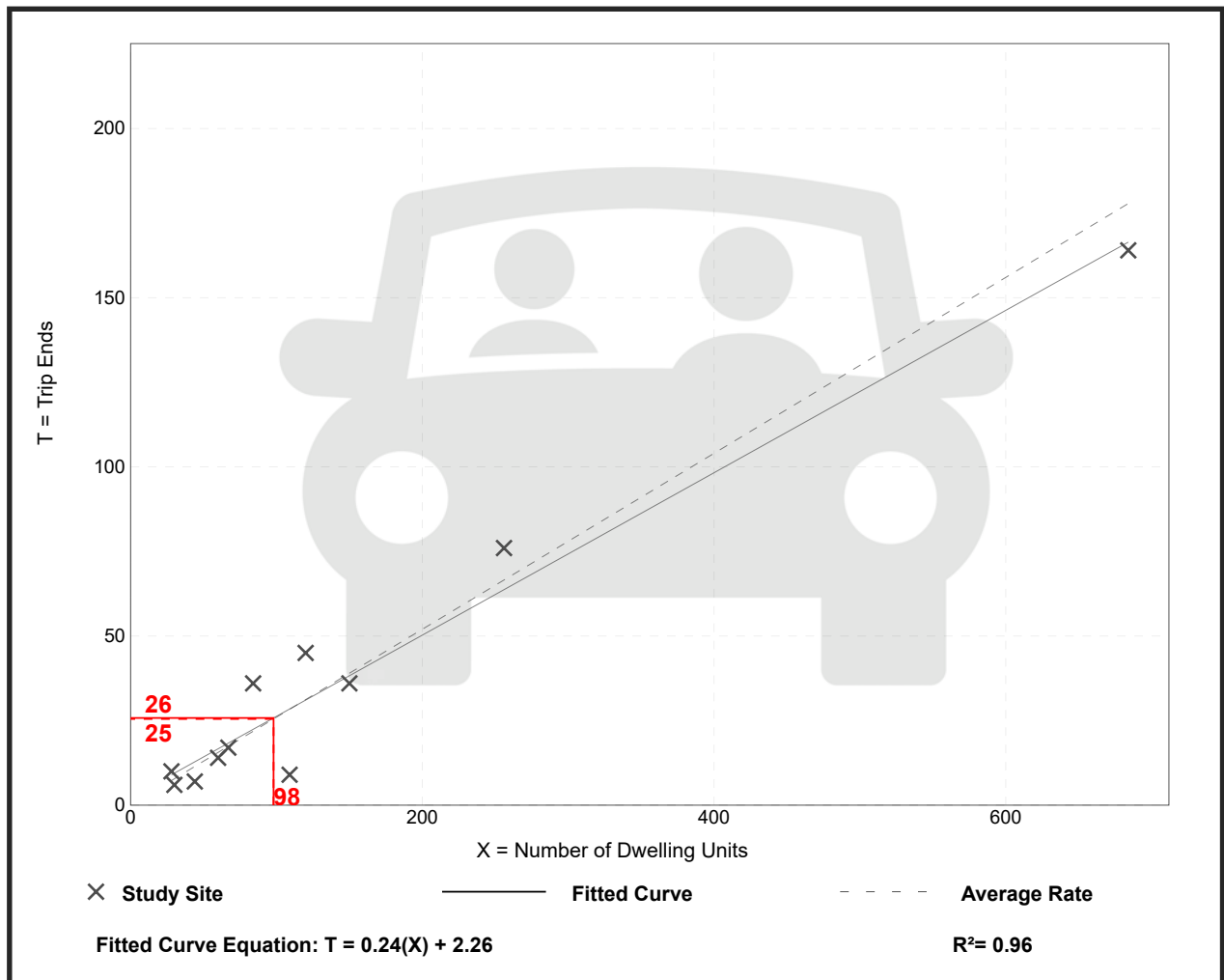
## Senior Adult Housing - Attached (252)

**Vehicle Trip Ends vs: Dwelling Units**  
**On a: Weekday,**  
**Peak Hour of Adjacent Street Traffic,**  
**One Hour Between 4 and 6 p.m.**  
**Setting/Location: General Urban/Suburban**  
 Number of Studies: 11  
 Avg. Num. of Dwelling Units: 148  
 Directional Distribution: 55% entering, 45% exiting

### Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
0.26	0.08 - 0.43	0.08

### Data Plot and Equation



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# Senior Adult Housing - Attached (252)

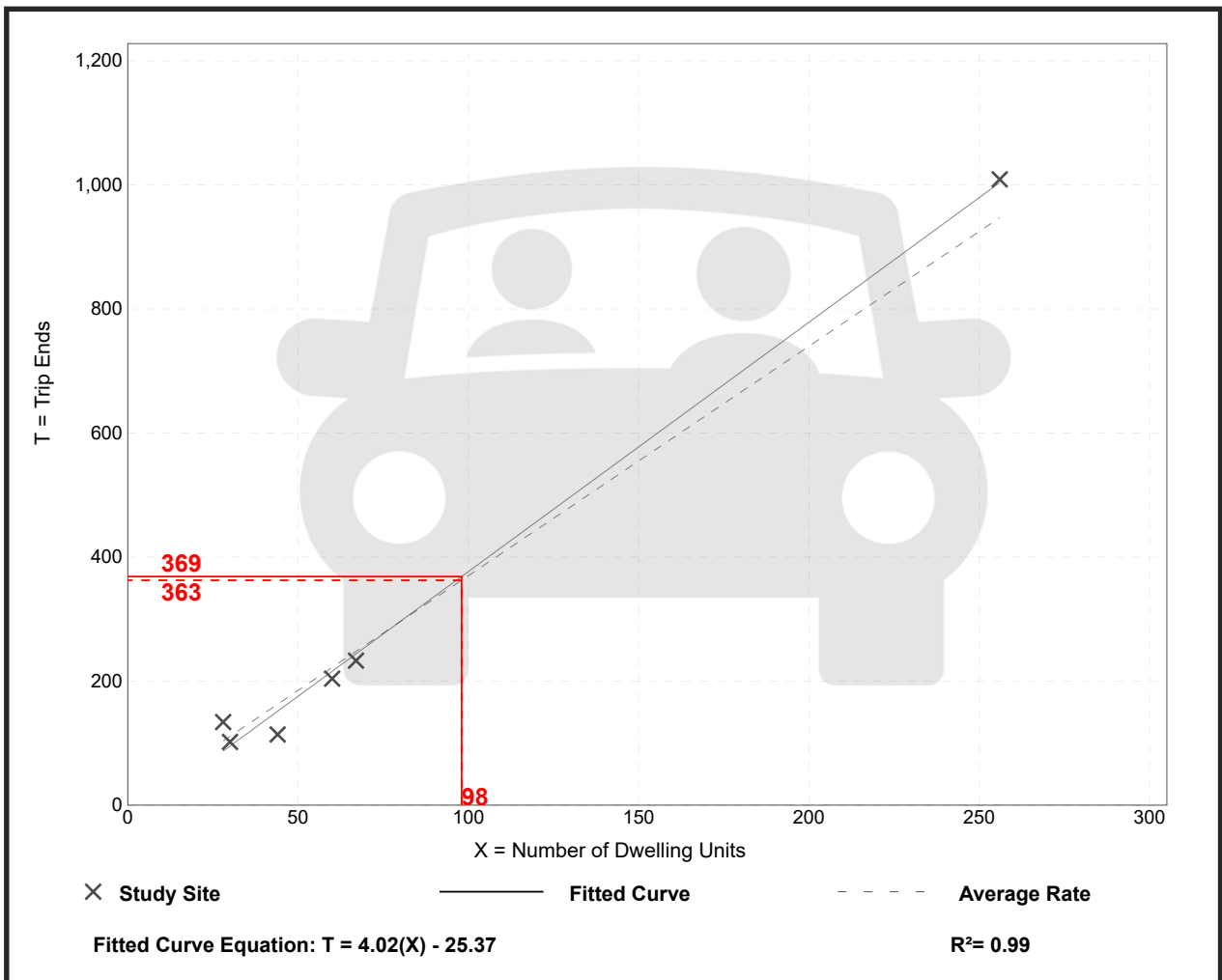
**Vehicle Trip Ends vs: Dwelling Units**  
**On a: Weekday**

**Setting/Location: General Urban/Suburban**  
Number of Studies: 6  
Avg. Num. of Dwelling Units: 81  
Directional Distribution: 50% entering, 50% exiting

## Vehicle Trip Generation per Dwelling Unit

Average Rate	Range of Rates	Standard Deviation
3.70	2.59 - 4.79	0.53

## Data Plot and Equation



*Trip Gen Manual, 10th Edition* • Institute of Transportation Engineers

## **Appendix B- AM Base Traffic Capacity Analysis Report**

HCM 2010 TWSC  
1: Glacier Highway/Vintage Boulevard & Egan Drive

03/02/2022

**Intersection**

Int Delay, s/veh 1.2

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↑↑	↑	↑	↑↑	↑			↑			↑
Traffic Vol, veh/h	0	773	202	27	634	63	0	0	46	0	0	79
Future Vol, veh/h	0	773	202	27	634	63	0	0	46	0	0	79
Conflicting Peds, #/hr	5	0	5	5	0	5	5	0	5	5	0	5
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	Free	-	-	Free	-	-	Stop	-	-	Stop
Storage Length	-	-	375	300	-	475	-	-	0	-	-	0
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	80	80	80	80	80	80	80	80	80	80	80	80
Heavy Vehicles, %	2	5	2	0	5	2	0	0	2	0	0	7
Mvmt Flow	0	966	253	34	793	79	0	0	58	0	0	99

Major/Minor	Major1	Major2	Minor1	Minor2
Conflicting Flow All	- 0	- 971	0 0	- 493
Stage 1	- -	- -	- -	- -
Stage 2	- -	- -	- -	- -
Critical Hdwy	- -	- 4.1	- -	- 6.94
Critical Hdwy Stg 1	- -	- -	- -	- -
Critical Hdwy Stg 2	- -	- -	- -	- -
Follow-up Hdwy	- -	- 2.2	- -	- 3.32
Pot Cap-1 Maneuver	0 -	0 718	- 0 0	0 522
Stage 1	0 -	0 -	- 0 0	- 0 0
Stage 2	0 -	0 -	- 0 0	- 0 0
Platoon blocked, %	-	-	-	-
Mov Cap-1 Maneuver	- -	- 715	- -	- 518
Mov Cap-2 Maneuver	- -	- -	- -	- -
Stage 1	- -	- -	- -	- -
Stage 2	- -	- -	- -	- -

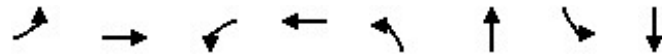
Approach	EB	WB	NB	SB
HCM Control Delay, s	0	0.4	12.8	12.4
HCM LOS			B	B

Minor Lane/Major Mvmt	NBLn1	EBT	WBL	WBT	SBLn1
Capacity (veh/h)	518	-	715	-	582
HCM Lane V/C Ratio	0.111	-	0.047	-	0.17
HCM Control Delay (s)	12.8	-	10.3	-	12.4
HCM Lane LOS	B	-	B	-	B
HCM 95th %tile Q(veh)	0.4	-	0.1	-	0.6

Queues

2: Riverside Drive & Vintage Boulevard/Mendenhall Mall Drive

03/02/2022



Lane Group	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Lane Group Flow (vph)	21	32	109	134	82	210	104	817
v/c Ratio	0.11	0.22	0.42	0.51	0.25	0.20	0.13	0.81
Control Delay	28.0	42.9	33.4	41.5	7.6	11.9	6.2	27.3
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	28.0	42.9	33.4	41.5	7.6	11.9	6.2	27.3
Queue Length 50th (ft)	9	17	49	61	14	58	17	382
Queue Length 95th (ft)	24	41	81	113	30	99	36	#573
Internal Link Dist (ft)		693		1080		707		271
Turn Bay Length (ft)	200		75		150		100	
Base Capacity (vph)	374	421	357	442	449	1030	890	1005
Starvation Cap Reductn	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.06	0.08	0.31	0.30	0.18	0.20	0.12	0.81

Intersection Summary

# 95th percentile volume exceeds capacity, queue may be longer.  
 Queue shown is maximum after two cycles.



## HCM Signalized Intersection Capacity Analysis 2: Riverside Drive & Vintage Boulevard/Mendenhall Mall Drive

03/02/2022

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	16	25	0	84	103	0	63	162	0	80	629	0
Future Volume (vph)	16	25	0	84	103	0	63	162	0	80	629	0
Ideal Flow (vphpl)	1850	1850	1850	1850	1850	1850	1850	1850	1850	1850	1850	1850
Lane Width	12	12	16	12	12	16	12	12	16	12	12	16
Total Lost time (s)	4.3	4.5		4.3	4.5		4.3	4.7		4.3	4.7	
Lane Util. Factor	1.00	1.00		1.00	1.00		1.00	1.00		1.00	1.00	
Frpb, ped/bikes	1.00	1.00		1.00	1.00		1.00	1.00		1.00	1.00	
Flpb, ped/bikes	0.99	1.00		0.99	1.00		1.00	1.00		0.99	1.00	
Frt	1.00	1.00		1.00	1.00		1.00	1.00		1.00	1.00	
Flt Protected	0.95	1.00		0.95	1.00		0.95	1.00		0.95	1.00	
Satd. Flow (prot)	1705	1729		1710	1814		1723	1762		1706	1762	
Flt Permitted	0.67	1.00		0.44	1.00		0.14	1.00		0.63	1.00	
Satd. Flow (perm)	1204	1729		798	1814		260	1762		1125	1762	
Peak-hour factor, PHF	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77
Adj. Flow (vph)	21	32	0	109	134	0	82	210	0	104	817	0
RTOR Reduction (vph)	0	0	0	0	0	0	0	0	0	0	0	0
Lane Group Flow (vph)	21	32	0	109	134	0	82	210	0	104	817	0
Confl. Peds. (#/hr)	5		5	5		5	5		5	5		5
Heavy Vehicles (%)	2%	7%	2%	2%	2%	2%	2%	5%	2%	2%	5%	2%
Turn Type	pm+pt	NA	Perm	pm+pt	NA	Perm	pm+pt	NA	Perm	pm+pt	NA	Perm
Protected Phases	3	8		7	4		5	2		1	6	
Permitted Phases	8		8	4		4	2		2	6		6
Actuated Green, G (s)	8.7	6.5		18.7	12.2		56.0	48.8		53.6	47.6	
Effective Green, g (s)	8.7	6.5		18.7	12.2		56.0	48.8		53.6	47.6	
Actuated g/C Ratio	0.10	0.07		0.21	0.14		0.64	0.56		0.62	0.55	
Clearance Time (s)	4.3	4.5		4.3	4.5		4.3	4.7		4.3	4.7	
Vehicle Extension (s)	2.0	2.0		2.0	2.0		3.0	2.0		3.0	2.0	
Lane Grp Cap (vph)	133	129		254	254		288	988		733	964	
v/s Ratio Prot	0.00	0.02		c0.04	c0.07		c0.02	0.12		0.01	c0.46	
v/s Ratio Perm	0.01			0.05			0.16			0.08		
v/c Ratio	0.16	0.25		0.43	0.53		0.28	0.21		0.14	0.85	
Uniform Delay, d1	35.7	37.9		28.8	34.7		11.4	9.5		6.8	16.6	
Progression Factor	1.00	1.00		1.00	1.00		1.00	1.00		1.00	1.00	
Incremental Delay, d2	0.2	0.4		0.4	0.9		0.5	0.0		0.1	6.7	
Delay (s)	35.9	38.3		29.2	35.6		12.0	9.6		6.9	23.4	
Level of Service	D	D		C	D		B	A		A	C	
Approach Delay (s)		37.3			32.7			10.2			21.5	
Approach LOS		D			C			B			C	

Intersection Summary			
HCM 2000 Control Delay	21.7	HCM 2000 Level of Service	C
HCM 2000 Volume to Capacity ratio	0.74		
Actuated Cycle Length (s)	87.0	Sum of lost time (s)	17.8
Intersection Capacity Utilization	61.7%	ICU Level of Service	B
Analysis Period (min)	15		
c Critical Lane Group			

Queues

3: Mendenhall Loop Road & Mendenhall Mall Road/Atlin Road

03/02/2022



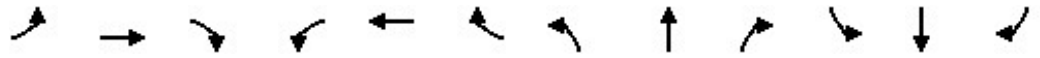
Lane Group	EBT	EBR	WBT	WBR	NBL	NBT	SBL	SBT	SBR
Lane Group Flow (vph)	62	127	31	6	69	485	6	1748	210
v/c Ratio	0.51	0.52	0.26	0.03	0.57	0.18	0.07	0.70	0.19
Control Delay	64.1	18.1	53.1	0.2	68.8	6.5	56.2	13.2	4.8
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.2	0.0
Total Delay	64.1	18.2	53.1	0.2	68.8	6.5	56.2	13.4	4.8
Queue Length 50th (ft)	47	9	23	0	44	18	5	371	27
Queue Length 95th (ft)	78	52	46	0	#107	152	18	530	64
Internal Link Dist (ft)	362		281			230		391	
Turn Bay Length (ft)		75		50	150		75		90
Base Capacity (vph)	245	379	236	338	125	2750	100	2494	1079
Starvation Cap Reductn	0	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	6	0	0	0	0	0	203	0
Storage Cap Reductn	0	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.25	0.34	0.13	0.02	0.55	0.18	0.06	0.76	0.19

Intersection Summary

# 95th percentile volume exceeds capacity, queue may be longer.  
 Queue shown is maximum after two cycles.

HCM Signalized Intersection Capacity Analysis  
 3: Mendenhall Loop Road & Mendenhall Mall Road/Atlin Road

03/02/2022



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕	↗		↕	↗	↖	↕↗		↖	↕↗	↖
Traffic Volume (vph)	48	4	107	24	2	5	58	403	4	5	1468	176
Future Volume (vph)	48	4	107	24	2	5	58	403	4	5	1468	176
Ideal Flow (vphpl)	1850	1850	1850	1850	1850	1850	1850	1850	1850	1850	1850	1850
Total Lost time (s)		5.3	5.3		5.3	5.3	5.0	5.2		5.0	5.2	5.2
Lane Util. Factor		1.00	1.00		1.00	1.00	1.00	0.95		1.00	0.95	1.00
Frbp, ped/bikes		1.00	0.97		1.00	0.98	1.00	1.00		1.00	1.00	0.95
Flpb, ped/bikes		0.99	1.00		0.99	1.00	1.00	1.00		1.00	1.00	1.00
Frt		1.00	0.85		1.00	0.85	1.00	1.00		1.00	1.00	0.85
Flt Protected		0.96	1.00		0.96	1.00	0.95	1.00		0.95	1.00	1.00
Satd. Flow (prot)		1722	1499		1721	1511	1723	3439		1723	3446	1460
Flt Permitted		0.72	1.00		0.69	1.00	0.95	1.00		0.95	1.00	1.00
Satd. Flow (perm)		1298	1499		1251	1511	1723	3439		1723	3446	1460
Peak-hour factor, PHF	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84
Adj. Flow (vph)	57	5	127	29	2	6	69	480	5	6	1748	210
RTOR Reduction (vph)	0	0	104	0	0	5	0	0	0	0	0	24
Lane Group Flow (vph)	0	62	23	0	31	1	69	485	0	6	1748	186
Confl. Peds. (#/hr)	5		5	5		5	5		5	5		5
Turn Type	Perm	NA	Perm	Perm	NA	Perm	Prot	NA		Prot	NA	Perm
Protected Phases		8			4		5	2		1		6
Permitted Phases	8		8	4		4						6
Actuated Green, G (s)		11.3	11.3		11.3	11.3	7.3	92.0		1.2	85.9	85.9
Effective Green, g (s)		11.3	11.3		11.3	11.3	7.3	92.0		1.2	85.9	85.9
Actuated g/C Ratio		0.09	0.09		0.09	0.09	0.06	0.77		0.01	0.72	0.72
Clearance Time (s)		5.3	5.3		5.3	5.3	5.0	5.2		5.0	5.2	5.2
Vehicle Extension (s)		1.5	1.5		1.5	1.5	2.0	2.0		2.0	2.0	2.0
Lane Grp Cap (vph)		122	141		117	142	104	2636		17	2466	1045
v/s Ratio Prot							c0.04	0.14		0.00	c0.51	
v/s Ratio Perm		c0.05	0.02		0.02	0.00						0.13
v/c Ratio		0.51	0.16		0.26	0.00	0.66	0.18		0.35	0.71	0.18
Uniform Delay, d1		51.7	50.0		50.5	49.3	55.1	3.8		59.0	9.8	5.6
Progression Factor		1.00	1.00		1.00	1.00	0.94	1.64		1.00	1.00	1.00
Incremental Delay, d2		1.2	0.2		0.4	0.0	11.6	0.2		4.5	1.8	0.4
Delay (s)		52.9	50.2		50.9	49.3	63.5	6.4		63.6	11.6	5.9
Level of Service		D	D		D	D	E	A		E	B	A
Approach Delay (s)		51.1			50.7			13.5			11.1	
Approach LOS		D			D			B			B	

Intersection Summary		
HCM 2000 Control Delay	14.9	HCM 2000 Level of Service
HCM 2000 Volume to Capacity ratio	0.68	B
Actuated Cycle Length (s)	120.0	Sum of lost time (s)
Intersection Capacity Utilization	69.2%	15.5
Analysis Period (min)	15	ICU Level of Service
c Critical Lane Group		C

Queues

4: Mendenhall Loop Road & Egan Drive

03/02/2022



Lane Group	EBL	EBT	EBR	WBL	WBT	NBL	NBT	NBR	SBL	SBT	SBR
Lane Group Flow (vph)	62	1116	134	8	436	129	119	41	807	828	123
v/c Ratio	0.55	1.06	0.25	0.11	0.55	0.73	0.64	0.15	1.08	1.09	0.17
Control Delay	72.1	84.3	9.3	58.9	46.4	74.6	66.3	1.2	81.6	84.6	4.7
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.6	0.0
Total Delay	72.1	84.3	9.3	58.9	46.4	74.6	66.3	1.2	81.6	85.2	4.7
Queue Length 50th (ft)	47	428	5	6	154	99	90	0	~699	~724	15
Queue Length 95th (ft)	#100	#778	61	23	#238	156	144	0	#932	#954	m27
Internal Link Dist (ft)		1356			1654		696			316	
Turn Bay Length (ft)	540		120	550		290		390	280		
Base Capacity (vph)	123	1057	532	72	792	277	291	348	749	762	727
Starvation Cap Reductn	0	0	0	0	0	0	0	0	0	1	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.50	1.06	0.25	0.11	0.55	0.47	0.41	0.12	1.08	1.09	0.17

Intersection Summary

- ~ Volume exceeds capacity, queue is theoretically infinite.  
Queue shown is maximum after two cycles.
- # 95th percentile volume exceeds capacity, queue may be longer.  
Queue shown is maximum after two cycles.
- m Volume for 95th percentile queue is metered by upstream signal.

# HCM Signalized Intersection Capacity Analysis

## 4: Mendenhall Loop Road & Egan Drive

03/02/2022



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↘	↗↗	↗	↘	↗↗	↗	↘	↗	↗	↘	↗	↗
Traffic Volume (vph)	56	1016	122	7	397	0	117	108	37	1267	221	112
Future Volume (vph)	56	1016	122	7	397	0	117	108	37	1267	221	112
Ideal Flow (vphpl)	1850	1850	1850	1850	1850	1850	1850	1850	1850	1850	1850	1850
Total Lost time (s)	5.2	5.9	5.9	5.2	5.9		5.7	5.7	5.7	5.8	5.8	5.8
Lane Util. Factor	1.00	*1.00	1.00	1.00	*1.00		1.00	1.00	1.00	*1.00	*1.00	1.00
Frbp, ped/bikes	1.00	1.00	0.96	1.00	1.00		1.00	1.00	0.98	1.00	1.00	0.98
Flpb, ped/bikes	1.00	1.00	1.00	1.00	1.00		1.00	1.00	1.00	1.00	1.00	1.00
Frt	1.00	1.00	0.85	1.00	1.00		1.00	1.00	0.85	1.00	1.00	0.85
Flt Protected	0.95	1.00	1.00	0.95	1.00		0.95	1.00	1.00	0.95	0.97	1.00
Satd. Flow (prot)	1723	3524	1481	1723	3524		1723	1814	1509	1723	1752	1511
Flt Permitted	0.95	1.00	1.00	0.95	1.00		0.95	1.00	1.00	0.95	0.97	1.00
Satd. Flow (perm)	1723	3524	1481	1723	3524		1723	1814	1509	1723	1752	1511
Peak-hour factor, PHF	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91
Adj. Flow (vph)	62	1116	134	8	436	0	129	119	41	1392	243	123
RTOR Reduction (vph)	0	0	92	0	0	0	0	0	37	0	0	69
Lane Group Flow (vph)	62	1116	42	8	436	0	129	119	4	807	828	54
Confl. Peds. (#/hr)	5		5	5		5	5		5	5		5
Heavy Vehicles (%)	2%	5%	2%	2%	5%	2%	2%	2%	2%	2%	2%	2%
Turn Type	Prot	NA	Perm	Prot	NA	Perm	Split	NA	Perm	Split	NA	Perm
Protected Phases	1	6		5	2		3	3		4	4	
Permitted Phases			6			2			3			4
Actuated Green, G (s)	6.9	31.9	31.9	1.0	26.0		12.3	12.3	12.3	52.2	52.2	52.2
Effective Green, g (s)	6.9	31.9	31.9	1.0	26.0		12.3	12.3	12.3	52.2	52.2	52.2
Actuated g/C Ratio	0.06	0.27	0.27	0.01	0.22		0.10	0.10	0.10	0.44	0.44	0.44
Clearance Time (s)	5.2	5.9	5.9	5.2	5.9		5.7	5.7	5.7	5.8	5.8	5.8
Vehicle Extension (s)	2.0	0.5	0.5	2.0	0.5		0.5	0.5	0.5	2.0	2.0	2.0
Lane Grp Cap (vph)	99	936	393	14	763		176	185	154	749	762	657
v/s Ratio Prot	c0.04	c0.32		0.00	0.12		c0.07	0.07		0.47	c0.47	
v/s Ratio Perm			0.03						0.00			0.04
v/c Ratio	0.63	1.19	0.11	0.57	0.57		0.73	0.64	0.03	1.08	1.09	0.08
Uniform Delay, d1	55.3	44.0	33.3	59.3	42.0		52.3	51.7	48.5	33.9	33.9	19.9
Progression Factor	1.00	1.00	1.00	1.00	1.00		1.00	1.00	1.00	0.88	0.88	1.26
Incremental Delay, d2	8.6	97.2	0.6	30.5	3.1		12.7	5.6	0.0	51.5	54.6	0.0
Delay (s)	63.9	141.2	33.8	89.8	45.1		65.0	57.4	48.5	81.2	84.3	24.9
Level of Service	E	F	C	F	D		E	E	D	F	F	C
Approach Delay (s)		126.6			45.9			59.5			78.7	
Approach LOS		F			D			E			E	

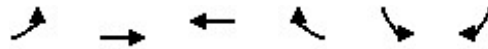
Intersection Summary		
HCM 2000 Control Delay	89.9	HCM 2000 Level of Service
HCM 2000 Volume to Capacity ratio	1.09	F
Actuated Cycle Length (s)	120.0	Sum of lost time (s)
Intersection Capacity Utilization	101.1%	22.6
Analysis Period (min)	15	ICU Level of Service
		G

c Critical Lane Group

Queues

5: Egan Drive & Riverside Drive

03/02/2022



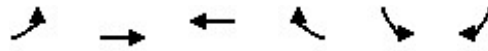
Lane Group	EBL	EBT	WBT	WBR	SBL	SBR
Lane Group Flow (vph)	201	786	617	137	654	257
v/c Ratio	0.62	0.44	0.71	0.29	0.68	0.45
Control Delay	36.1	10.5	28.9	6.6	26.4	10.9
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	36.1	10.5	28.9	6.6	26.4	10.9
Queue Length 50th (ft)	73	90	115	0	116	24
Queue Length 95th (ft)	159	150	204	33	206	83
Internal Link Dist (ft)		694	1356		707	
Turn Bay Length (ft)	350			900	225	145
Base Capacity (vph)	793	3218	2115	960	2111	626
Starvation Cap Reductn	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0
Reduced v/c Ratio	0.25	0.24	0.29	0.14	0.31	0.41

Intersection Summary



HCM Signalized Intersection Capacity Analysis  
 5: Egan Drive & Riverside Drive

03/02/2022



Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations	↖	↑↑	↑↑	↗	↖↗	↘
Traffic Volume (vph)	167	652	512	114	543	213
Future Volume (vph)	167	652	512	114	543	213
Ideal Flow (vphpl)	1850	1850	1850	1850	1850	1850
Total Lost time (s)	5.3	5.8	5.8	5.8	5.3	5.3
Lane Util. Factor	1.00	0.95	0.95	1.00	0.97	1.00
Frbp, ped/bikes	1.00	1.00	1.00	0.97	1.00	1.00
Flpb, ped/bikes	1.00	1.00	1.00	1.00	1.00	1.00
Frt	1.00	1.00	1.00	0.85	1.00	0.85
Flt Protected	0.95	1.00	1.00	1.00	0.95	1.00
Satd. Flow (prot)	1674	3348	3348	1453	3343	1542
Flt Permitted	0.95	1.00	1.00	1.00	0.95	1.00
Satd. Flow (perm)	1674	3348	3348	1453	3343	1542
Peak-hour factor, PHF	0.83	0.83	0.83	0.83	0.83	0.83
Adj. Flow (vph)	201	786	617	137	654	257
RTOR Reduction (vph)	0	0	0	100	0	123
Lane Group Flow (vph)	201	786	617	37	654	134
Confl. Peds. (#/hr)	5			5	5	5
Heavy Vehicles (%)	5%	5%	5%	5%	2%	2%
Turn Type	Prot	NA	NA	Perm	Prot	Prot
Protected Phases	1	6	2		4	8
Permitted Phases				2		
Actuated Green, G (s)	13.0	36.0	17.7	17.7	19.3	19.3
Effective Green, g (s)	13.0	36.0	17.7	17.7	19.3	19.3
Actuated g/C Ratio	0.20	0.54	0.27	0.27	0.29	0.29
Clearance Time (s)	5.3	5.8	5.8	5.8	5.3	5.3
Vehicle Extension (s)	1.0	0.5	0.5	0.5	2.0	2.0
Lane Grp Cap (vph)	327	1815	892	387	971	448
v/s Ratio Prot	c0.12	0.23	c0.18		c0.20	0.09
v/s Ratio Perm				0.03		
v/c Ratio	0.61	0.43	0.69	0.09	0.67	0.30
Uniform Delay, d1	24.4	9.1	21.9	18.3	20.8	18.3
Progression Factor	1.00	1.00	1.00	1.00	1.00	1.00
Incremental Delay, d2	2.4	0.1	1.9	0.0	1.5	0.1
Delay (s)	26.8	9.2	23.8	18.4	22.2	18.4
Level of Service	C	A	C	B	C	B
Approach Delay (s)		12.8	22.8		21.2	
Approach LOS		B	C		C	

Intersection Summary			
HCM 2000 Control Delay	18.5	HCM 2000 Level of Service	B
HCM 2000 Volume to Capacity ratio	0.69		
Actuated Cycle Length (s)	66.4	Sum of lost time (s)	18.4
Intersection Capacity Utilization	53.6%	ICU Level of Service	A
Analysis Period (min)	15		

c Critical Lane Group

HCM 2010 TWSC  
6: Riverside & EBR Vintage

03/02/2022

**Intersection**

Int Delay, s/veh 1

**Movement** EBL EBR NBL NBT SBT SBR

Lane Configurations		↗			↖	
Traffic Vol, veh/h	0	43	0	0	713	0
Future Vol, veh/h	0	43	0	0	713	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Free	Free
RT Channelized	-	Yield	-	None	-	None
Storage Length	-	0	-	-	-	-
Veh in Median Storage, #	0	-	-	-	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	77	77	77	77	77	77
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	56	0	0	926	0

**Major/Minor** Minor2 Major2

Conflicting Flow All	-	926	-	0
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Critical Hdwy	-	6.22	-	-
Critical Hdwy Stg 1	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-
Follow-up Hdwy	-	3.318	-	-
Pot Cap-1 Maneuver	0	326	-	0
Stage 1	0	-	-	0
Stage 2	0	-	-	0
Platoon blocked, %			-	
Mov Cap-1 Maneuver	-	326	-	-
Mov Cap-2 Maneuver	-	-	-	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-

**Approach** EB SB

HCM Control Delay, s 18.3 0  
HCM LOS C

**Minor Lane/Major Mvmt** EBLn1 SBT

Capacity (veh/h)	326	-
HCM Lane V/C Ratio	0.171	-
HCM Control Delay (s)	18.3	-
HCM Lane LOS	C	-
HCM 95th %tile Q(veh)	0.6	-

HCM 2010 TWSC  
7: Riverside & WBR Mendenhall Mall

03/02/2022

**Intersection**

Int Delay, s/veh 2.1

**Movement** WBL WBR NBT NBR SBL SBT

Lane Configurations		↗	↖			
Traffic Vol, veh/h	0	49	178	0	0	0
Future Vol, veh/h	0	49	178	0	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Stop	Stop
RT Channelized	-	Yield	-	None	-	None
Storage Length	-	0	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	-
Grade, %	0	-	0	-	-	0
Peak Hour Factor	77	77	77	77	77	77
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	64	231	0	0	0

**Major/Minor** Minor1 Major1

Conflicting Flow All	-	231	0	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Critical Hdwy	-	6.22	-	-
Critical Hdwy Stg 1	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-
Follow-up Hdwy	-	3.318	-	-
Pot Cap-1 Maneuver	0	808	-	0
Stage 1	0	-	-	0
Stage 2	0	-	-	0
Platoon blocked, %			-	
Mov Cap-1 Maneuver	-	808	-	-
Mov Cap-2 Maneuver	-	-	-	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-

**Approach** WB NB

HCM Control Delay, s	9.8	0
HCM LOS	A	

**Minor Lane/Major Mvmt** NBTWBLn1

Capacity (veh/h)	-	808
HCM Lane V/C Ratio	-	0.079
HCM Control Delay (s)	-	9.8
HCM Lane LOS	-	A
HCM 95th %tile Q(veh)	-	0.3

HCM 2010 TWSC  
8: NBR Riverside & Vintage/Mendenhall Mall

03/02/2022

**Intersection**

Int Delay, s/veh 3.2

**Movement** EBT EBR WBL WBT NBL NBR

Lane Configurations	↑					↑
Traffic Vol, veh/h	105	0	0	0	0	55
Future Vol, veh/h	105	0	0	0	0	55
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Stop	Stop	Stop	Stop
RT Channelized	-	None	-	None	-	Yield
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	0	-	-	-	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	77	77	77	77	77	77
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	136	0	0	0	0	71

**Major/Minor** Major1 Minor1

Conflicting Flow All	0	-	-	136
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Critical Hdwy	-	-	-	6.22
Critical Hdwy Stg 1	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-
Follow-up Hdwy	-	-	-	3.318
Pot Cap-1 Maneuver	-	0	0	913
Stage 1	-	0	0	-
Stage 2	-	0	0	-
Platoon blocked, %	-	-	-	-
Mov Cap-1 Maneuver	-	-	-	913
Mov Cap-2 Maneuver	-	-	-	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-

**Approach** EB NB

HCM Control Delay, s	0	9.3
HCM LOS		A

**Minor Lane/Major Mvmt** NBLn1 EBT

Capacity (veh/h)	913	-
HCM Lane V/C Ratio	0.078	-
HCM Control Delay (s)	9.3	-
HCM Lane LOS	A	-
HCM 95th %tile Q(veh)	0.3	-

HCM 2010 TWSC  
9: Vintage/Mendenhall Mall & SBR Riverside

03/02/2022

**Intersection**

Int Delay, s/veh 4.2

**Movement** EBL EBT WBT WBR SBL SBR

Lane Configurations			↑			↑
Traffic Vol, veh/h	0	0	167	0	0	113
Future Vol, veh/h	0	0	167	0	0	113
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	Yield
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	7405568		0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	77	77	77	77	77	77
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	0	217	0	0	147

**Major/Minor** Major2 Minor2

Conflicting Flow All	-	0	-	217
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Critical Hdwy	-	-	-	6.22
Critical Hdwy Stg 1	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-
Follow-up Hdwy	-	-	-	3.318
Pot Cap-1 Maneuver	-	0	0	823
Stage 1	-	0	0	-
Stage 2	-	0	0	-
Platoon blocked, %	-	-	-	-
Mov Cap-1 Maneuver	-	-	-	823
Mov Cap-2 Maneuver	-	-	-	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-

**Approach** WB SB

HCM Control Delay, s	0	10.3
HCM LOS		B

**Minor Lane/Major Mvmt** WBT SBLn1

Capacity (veh/h)	-	823
HCM Lane V/C Ratio	-	0.178
HCM Control Delay (s)	-	10.3
HCM Lane LOS	-	B
HCM 95th %tile Q(veh)	-	0.6

## **Appendix C- PM Base Traffic Capacity Analysis Reports**



HCM 2010 TWSC  
1: Glacier Highway/Vintage Boulevard & Egan Drive

03/02/2022

**Intersection**

Int Delay, s/veh 2.1

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↑↑	↑	↑	↑↑	↑			↑			↑
Traffic Vol, veh/h	0	736	166	54	900	152	0	0	70	0	0	176
Future Vol, veh/h	0	736	166	54	900	152	0	0	70	0	0	176
Conflicting Peds, #/hr	5	0	5	5	0	5	5	0	5	5	0	5
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	Free	-	-	Free	-	-	Stop	-	-	Stop
Storage Length	-	-	375	300	-	475	-	-	0	-	-	0
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	94	94	94	94	94	94	94	94	94	94	94	94
Heavy Vehicles, %	2	5	2	2	5	2	2	2	2	0	0	7
Mvmt Flow	0	783	177	57	957	162	0	0	74	0	0	187

Major/Minor	Major1	Major2	Minor1	Minor2
Conflicting Flow All	- 0	- 788	0 0	- 402
Stage 1	- -	- -	- -	- -
Stage 2	- -	- -	- -	- -
Critical Hdwy	- -	4.14	- -	6.94
Critical Hdwy Stg 1	- -	- -	- -	- -
Critical Hdwy Stg 2	- -	- -	- -	- -
Follow-up Hdwy	- -	2.22	- -	3.32
Pot Cap-1 Maneuver	0 -	0 827	- 0	0 598
Stage 1	0 -	0 -	- 0	0 0
Stage 2	0 -	0 -	- 0	0 0
Platoon blocked, %	-	-	-	-
Mov Cap-1 Maneuver	- -	- 824	- -	- 593
Mov Cap-2 Maneuver	- -	- -	- -	- -
Stage 1	- -	- -	- -	- -
Stage 2	- -	- -	- -	- -

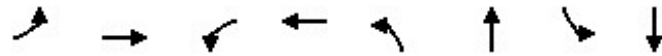
Approach	EB	WB	NB	SB
HCM Control Delay, s	0	0.5	11.9	16
HCM LOS			B	C

Minor Lane/Major Mvmt	NBLn1	EBT	WBL	WBT	SBLn1
Capacity (veh/h)	593	-	824	-	514
HCM Lane V/C Ratio	0.126	-	0.07	-	0.364
HCM Control Delay (s)	11.9	-	9.7	-	16
HCM Lane LOS	B	-	A	-	C
HCM 95th %tile Q(veh)	0.4	-	0.2	-	1.7

Queues

2: Riverside Drive & Vintage Boulevard/Mendenhall Mall Drive

03/02/2022



Lane Group	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Lane Group Flow (vph)	297	181	161	143	317	475	80	291
v/c Ratio	0.64	0.59	0.38	0.52	0.64	0.71	0.24	0.67
Control Delay	25.9	39.4	20.2	38.9	19.2	29.2	13.8	34.6
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	25.9	39.4	20.2	38.9	19.2	29.2	13.8	34.6
Queue Length 50th (ft)	97	76	48	62	86	192	19	124
Queue Length 95th (ft)	213	175	116	140	175	364	48	229
Internal Link Dist (ft)		693		1080		707		271
Turn Bay Length (ft)	200		75		150		100	
Base Capacity (vph)	515	490	526	514	528	1158	536	1158
Starvation Cap Reductn	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.58	0.37	0.31	0.28	0.60	0.41	0.15	0.25

Intersection Summary

## HCM Signalized Intersection Capacity Analysis 2: Riverside Drive & Vintage Boulevard/Mendenhall Mall Drive

03/02/2022

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	276	168	0	150	133	0	295	442	0	74	271	0
Future Volume (vph)	276	168	0	150	133	0	295	442	0	74	271	0
Ideal Flow (vphpl)	1850	1850	1850	1850	1850	1850	1850	1850	1850	1850	1850	1850
Lane Width	12	12	16	12	12	16	12	12	16	12	12	16
Total Lost time (s)	4.3	4.5		4.3	4.5		4.3	4.7		4.3	4.7	
Lane Util. Factor	1.00	1.00		1.00	1.00		1.00	1.00		1.00	1.00	
Frpb, ped/bikes	1.00	1.00		1.00	1.00		1.00	1.00		1.00	1.00	
Flpb, ped/bikes	0.99	1.00		0.99	1.00		1.00	1.00		1.00	1.00	
Frt	1.00	1.00		1.00	1.00		1.00	1.00		1.00	1.00	
Flt Protected	0.95	1.00		0.95	1.00		0.95	1.00		0.95	1.00	
Satd. Flow (prot)	1713	1729		1707	1814		1723	1814		1718	1814	
Flt Permitted	0.52	1.00		0.62	1.00		0.33	1.00		0.39	1.00	
Satd. Flow (perm)	940	1729		1108	1814		591	1814		700	1814	
Peak-hour factor, PHF	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93
Adj. Flow (vph)	297	181	0	161	143	0	317	475	0	80	291	0
RTOR Reduction (vph)	0	0	0	0	0	0	0	0	0	0	0	0
Lane Group Flow (vph)	297	181	0	161	143	0	317	475	0	80	291	0
Confl. Peds. (#/hr)	5		5	5		5	5		5	5		5
Heavy Vehicles (%)	2%	7%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%
Turn Type	pm+pt	NA	Perm	pm+pt	NA	Perm	pm+pt	NA	Perm	pm+pt	NA	Perm
Protected Phases	3	8		7	4		5	2		1	6	
Permitted Phases	8		8	4		4	2		2	6		6
Actuated Green, G (s)	26.0	13.2		22.2	11.3		37.0	27.3		24.5	19.1	
Effective Green, g (s)	26.0	13.2		22.2	11.3		37.0	27.3		24.5	19.1	
Actuated g/C Ratio	0.35	0.18		0.30	0.15		0.50	0.37		0.33	0.26	
Clearance Time (s)	4.3	4.5		4.3	4.5		4.3	4.7		4.3	4.7	
Vehicle Extension (s)	2.0	2.0		3.0	2.0		2.0	2.0		2.0	2.0	
Lane Grp Cap (vph)	460	305		417	274		499	663		303	464	
v/s Ratio Prot	c0.11	0.10		0.06	0.08		c0.12	c0.26		0.02	0.16	
v/s Ratio Perm	c0.11			0.06			0.20			0.07		
v/c Ratio	0.65	0.59		0.39	0.52		0.64	0.72		0.26	0.63	
Uniform Delay, d1	19.2	28.2		20.3	29.2		12.6	20.3		17.8	24.6	
Progression Factor	1.00	1.00		1.00	1.00		1.00	1.00		1.00	1.00	
Incremental Delay, d2	2.3	2.1		0.6	0.8		1.9	3.1		0.2	1.9	
Delay (s)	21.6	30.3		20.9	30.0		14.6	23.4		17.9	26.5	
Level of Service	C	C		C	C		B	C		B	C	
Approach Delay (s)		24.9			25.2			19.9			24.7	
Approach LOS		C			C			B			C	

Intersection Summary			
HCM 2000 Control Delay	22.8	HCM 2000 Level of Service	C
HCM 2000 Volume to Capacity ratio	0.73		
Actuated Cycle Length (s)	74.6	Sum of lost time (s)	17.8
Intersection Capacity Utilization	69.5%	ICU Level of Service	C
Analysis Period (min)	15		
c Critical Lane Group			

Queues

3: Mendenhall Loop Road & Mendenhall Mall Road/Atlin Road

03/02/2022



Lane Group	EBT	EBR	WBT	WBR	NBL	NBT	SBL	SBT	SBR
Lane Group Flow (vph)	245	155	16	8	327	1673	10	726	215
v/c Ratio	0.88	0.37	0.06	0.02	1.16	0.74	0.10	0.45	0.27
Control Delay	68.1	11.9	29.8	0.1	145.1	13.9	46.4	18.3	3.2
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.4	0.0	0.0	0.0
Total Delay	68.1	11.9	29.8	0.1	145.1	14.4	46.4	18.3	3.2
Queue Length 50th (ft)	148	17	8	0	~347	262	6	138	5
Queue Length 95th (ft)	#263	68	25	0	m#508	m#623	23	181	40
Internal Link Dist (ft)	362		281			230		391	
Turn Bay Length (ft)		75		50	150		75		90
Base Capacity (vph)	323	465	325	433	283	2262	120	1819	868
Starvation Cap Reductn	0	0	0	0	0	203	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.76	0.33	0.05	0.02	1.16	0.81	0.08	0.40	0.25

Intersection Summary

- ~ Volume exceeds capacity, queue is theoretically infinite.  
Queue shown is maximum after two cycles.
- # 95th percentile volume exceeds capacity, queue may be longer.  
Queue shown is maximum after two cycles.
- m Volume for 95th percentile queue is metered by upstream signal.

### HCM Signalized Intersection Capacity Analysis 3: Mendenhall Loop Road & Mendenhall Mall Road/Atlin Road

03/02/2022



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕	↗		↕	↗	↖	↕↗		↖	↕↗	↖
Traffic Volume (vph)	210	13	141	14	1	7	298	1494	28	9	661	196
Future Volume (vph)	210	13	141	14	1	7	298	1494	28	9	661	196
Ideal Flow (vphpl)	1850	1850	1850	1850	1850	1850	1850	1850	1850	1850	1850	1850
Total Lost time (s)		5.3	5.3		5.3	5.3	5.0	5.2		5.0	5.2	5.2
Lane Util. Factor		1.00	1.00		1.00	1.00	1.00	0.95		1.00	0.95	1.00
Frbp, ped/bikes		1.00	0.98		1.00	0.98	1.00	1.00		1.00	1.00	0.95
Flpb, ped/bikes		0.99	1.00		1.00	1.00	1.00	1.00		1.00	1.00	1.00
Frt		1.00	0.85		1.00	0.85	1.00	1.00		1.00	1.00	0.85
Flt Protected		0.95	1.00		0.96	1.00	0.95	1.00		0.95	1.00	1.00
Satd. Flow (prot)		1721	1513		1726	1513	1723	3434		1723	3446	1469
Flt Permitted		0.73	1.00		0.73	1.00	0.95	1.00		0.95	1.00	1.00
Satd. Flow (perm)		1310	1513		1319	1513	1723	3434		1723	3446	1469
Peak-hour factor, PHF	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91
Adj. Flow (vph)	231	14	155	15	1	8	327	1642	31	10	726	215
RTOR Reduction (vph)	0	0	95	0	0	6	0	1	0	0	0	105
Lane Group Flow (vph)	0	245	60	0	16	2	327	1672	0	10	726	110
Confl. Peds. (#/hr)	5		5	5		5	5		5	5		5
Turn Type	Perm	NA	Perm	Perm	NA	Perm	Prot	NA		Prot	NA	Perm
Protected Phases		8			4		5	2		1	6	
Permitted Phases	8		8	4		4						6
Actuated Green, G (s)		21.3	21.3		21.3	21.3	16.5	61.9		1.3	46.7	46.7
Effective Green, g (s)		21.3	21.3		21.3	21.3	16.5	61.9		1.3	46.7	46.7
Actuated g/C Ratio		0.21	0.21		0.21	0.21	0.16	0.62		0.01	0.47	0.47
Clearance Time (s)		5.3	5.3		5.3	5.3	5.0	5.2		5.0	5.2	5.2
Vehicle Extension (s)		1.5	1.5		1.5	1.5	2.0	2.0		2.0	2.0	2.0
Lane Grp Cap (vph)		279	322		280	322	284	2125		22	1609	686
v/s Ratio Prot							c0.19	c0.49		0.01	0.21	
v/s Ratio Perm		c0.19	0.04		0.01	0.00						0.07
v/c Ratio		0.88	0.19		0.06	0.01	1.15	0.79		0.45	0.45	0.16
Uniform Delay, d1		38.1	32.2		31.4	31.0	41.8	14.1		49.0	18.0	15.4
Progression Factor		1.00	1.00		1.00	1.00	1.07	0.85		1.00	1.00	1.00
Incremental Delay, d2		24.6	0.1		0.0	0.0	100.5	3.0		5.3	0.9	0.5
Delay (s)		62.7	32.3		31.4	31.0	145.2	15.1		54.3	18.9	15.9
Level of Service		E	C		C	C	F	B		D	B	B
Approach Delay (s)		50.9			31.3			36.3			18.6	
Approach LOS		D			C			D			B	

Intersection Summary			
HCM 2000 Control Delay	33.0	HCM 2000 Level of Service	C
HCM 2000 Volume to Capacity ratio	0.90		
Actuated Cycle Length (s)	100.0	Sum of lost time (s)	15.5
Intersection Capacity Utilization	80.6%	ICU Level of Service	D
Analysis Period (min)	15		
c Critical Lane Group			

Queues

4: Mendenhall Loop Road & Egan Drive

03/02/2022



Lane Group	EBL	EBT	EBR	WBL	WBT	NBL	NBT	NBR	SBL	SBT	SBR
Lane Group Flow (vph)	106	618	140	33	1084	315	346	20	385	391	65
v/c Ratio	0.66	0.56	0.24	0.32	1.27	0.95	0.99	0.05	1.02	1.00	0.14
Control Delay	53.5	23.3	4.2	53.3	163.5	79.4	87.1	0.2	77.8	73.8	1.8
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	53.5	23.3	4.2	53.3	163.5	79.4	87.1	0.2	77.8	73.8	1.8
Queue Length 50th (ft)	47	204	20	21	~474	200	222	0	~267	~267	4
Queue Length 95th (ft)	#101	227	50	51	#606	#366	#403	0	#471	#471	11
Internal Link Dist (ft)		1356			1654		696			316	
Turn Bay Length (ft)	540		120	550		290		390	280		
Base Capacity (vph)	186	1104	590	117	854	332	350	414	379	390	466
Starvation Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.57	0.56	0.24	0.28	1.27	0.95	0.99	0.05	1.02	1.00	0.14

Intersection Summary

- ~ Volume exceeds capacity, queue is theoretically infinite.  
Queue shown is maximum after two cycles.
- # 95th percentile volume exceeds capacity, queue may be longer.  
Queue shown is maximum after two cycles.



HCM Signalized Intersection Capacity Analysis  
4: Mendenhall Loop Road & Egan Drive

03/02/2022

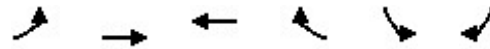
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	103	599	136	32	1051	0	306	336	19	558	195	63
Future Volume (vph)	103	599	136	32	1051	0	306	336	19	558	195	63
Ideal Flow (vphpl)	1850	1850	1850	1850	1850	1850	1850	1850	1850	1850	1850	1850
Total Lost time (s)	5.2	5.9	5.9	5.2	5.9		5.7	5.7	5.7	5.8	5.8	5.8
Lane Util. Factor	1.00	0.95	1.00	1.00	0.95		1.00	1.00	1.00	0.95	0.95	1.00
Frbp, ped/bikes	1.00	1.00	0.96	1.00	1.00		1.00	1.00	0.98	1.00	1.00	0.98
Flpb, ped/bikes	1.00	1.00	1.00	1.00	1.00		1.00	1.00	1.00	1.00	1.00	1.00
Frt	1.00	1.00	0.85	1.00	1.00		1.00	1.00	0.85	1.00	1.00	0.85
Flt Protected	0.95	1.00	1.00	0.95	1.00		0.95	1.00	1.00	0.95	0.98	1.00
Satd. Flow (prot)	1723	3348	1486	1723	3348		1723	1814	1511	1637	1682	1513
Flt Permitted	0.95	1.00	1.00	0.95	1.00		0.95	1.00	1.00	0.95	0.98	1.00
Satd. Flow (perm)	1723	3348	1486	1723	3348		1723	1814	1511	1637	1682	1513
Peak-hour factor, PHF	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97
Adj. Flow (vph)	106	618	140	33	1084	0	315	346	20	575	201	65
RTOR Reduction (vph)	0	0	97	0	0	0	0	0	16	0	0	50
Lane Group Flow (vph)	106	618	43	33	1084	0	315	346	4	385	391	15
Confl. Peds. (#/hr)	5		5	5		5	5		5	5		5
Heavy Vehicles (%)	2%	5%	2%	2%	5%	2%	2%	2%	2%	2%	2%	2%
Turn Type	Prot	NA	Perm	Prot	NA	Perm	Split	NA	Perm	Split	NA	Perm
Protected Phases	1	6		5	2		3	3		4	4	
Permitted Phases			6			2			3			4
Actuated Green, G (s)	9.4	30.9	30.9	4.0	25.5		19.3	19.3	19.3	23.2	23.2	23.2
Effective Green, g (s)	9.4	30.9	30.9	4.0	25.5		19.3	19.3	19.3	23.2	23.2	23.2
Actuated g/C Ratio	0.09	0.31	0.31	0.04	0.26		0.19	0.19	0.19	0.23	0.23	0.23
Clearance Time (s)	5.2	5.9	5.9	5.2	5.9		5.7	5.7	5.7	5.8	5.8	5.8
Vehicle Extension (s)	2.0	0.5	0.5	2.0	0.5		0.5	0.5	0.5	2.0	2.0	2.0
Lane Grp Cap (vph)	161	1034	459	68	853		332	350	291	379	390	351
v/s Ratio Prot	c0.06	0.18		0.02	c0.32		0.18	c0.19		c0.24	0.23	
v/s Ratio Perm			0.03						0.00			0.01
v/c Ratio	0.66	0.60	0.09	0.49	1.27		0.95	0.99	0.01	1.02	1.00	0.04
Uniform Delay, d1	43.7	29.3	24.6	47.0	37.2		39.9	40.2	32.6	38.4	38.4	29.8
Progression Factor	0.79	0.73	0.77	1.00	1.00		1.00	1.00	1.00	0.67	0.67	1.00
Incremental Delay, d2	7.0	2.5	0.4	2.0	131.1		35.4	44.5	0.0	48.9	44.8	0.0
Delay (s)	41.8	23.9	19.2	49.0	168.3		75.2	84.7	32.7	74.6	70.6	29.8
Level of Service	D	C	B	D	F		E	F	C	E	E	C
Approach Delay (s)		25.4			164.8			78.8			69.3	
Approach LOS		C			F			E			E	
<b>Intersection Summary</b>												
HCM 2000 Control Delay			90.8			HCM 2000 Level of Service			F			
HCM 2000 Volume to Capacity ratio			1.05									
Actuated Cycle Length (s)			100.0			Sum of lost time (s)			22.6			
Intersection Capacity Utilization			93.8%			ICU Level of Service			F			
Analysis Period (min)			15									

c Critical Lane Group

Queues

5: Egan Drive & Riverside Drive

03/02/2022



Lane Group	EBL	EBT	WBT	WBR	SBL	SBR
Lane Group Flow (vph)	371	469	954	525	405	197
v/c Ratio	0.76	0.19	0.75	0.60	0.73	0.47
Control Delay	43.2	5.0	16.5	4.2	47.4	9.0
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	43.2	5.0	16.5	4.2	47.4	9.0
Queue Length 50th (ft)	215	43	71	5	127	0
Queue Length 95th (ft)	316	72	m165	m0	169	57
Internal Link Dist (ft)		694	1356		707	
Turn Bay Length (ft)	350			900	225	145
Base Capacity (vph)	489	2420	1264	872	1026	420
Starvation Cap Reductn	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0
Reduced v/c Ratio	0.76	0.19	0.75	0.60	0.39	0.47

Intersection Summary

m Volume for 95th percentile queue is metered by upstream signal.

# HCM Signalized Intersection Capacity Analysis

## 5: Egan Drive & Riverside Drive

03/02/2022



Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations	↶	↷	↷	↷	↶	↷
Traffic Volume (vph)	356	450	916	504	389	189
Future Volume (vph)	356	450	916	504	389	189
Ideal Flow (vphpl)	1850	1850	1850	1850	1850	1850
Total Lost time (s)	5.3	5.8	5.8	5.8	5.3	5.3
Lane Util. Factor	1.00	0.95	0.95	1.00	0.97	1.00
Frbp, ped/bikes	1.00	1.00	1.00	0.97	1.00	1.00
Fllp, ped/bikes	1.00	1.00	1.00	1.00	1.00	1.00
Frt	1.00	1.00	1.00	0.85	1.00	0.85
Flt Protected	0.95	1.00	1.00	1.00	0.95	1.00
Satd. Flow (prot)	1674	3348	3348	1446	3343	1542
Flt Permitted	0.95	1.00	1.00	1.00	0.95	1.00
Satd. Flow (perm)	1674	3348	3348	1446	3343	1542
Peak-hour factor, PHF	0.96	0.96	0.96	0.96	0.96	0.96
Adj. Flow (vph)	371	469	954	525	405	197
RTOR Reduction (vph)	0	0	0	327	0	164
Lane Group Flow (vph)	371	469	954	198	405	33
Confl. Peds. (#/hr)	5			5	5	5
Heavy Vehicles (%)	5%	5%	5%	5%	2%	2%
Turn Type	Prot	NA	NA	Perm	Prot	Prot
Protected Phases	1	6	2		4	8
Permitted Phases				2		
Actuated Green, G (s)	29.2	72.3	37.8	37.8	16.6	16.6
Effective Green, g (s)	29.2	72.3	37.8	37.8	16.6	16.6
Actuated g/C Ratio	0.29	0.72	0.38	0.38	0.17	0.17
Clearance Time (s)	5.3	5.8	5.8	5.8	5.3	5.3
Vehicle Extension (s)	1.0	0.5	0.5	0.5	2.0	2.0
Lane Grp Cap (vph)	488	2420	1265	546	554	255
v/s Ratio Prot	c0.22	0.14	c0.28		c0.12	0.02
v/s Ratio Perm				0.14		
v/c Ratio	0.76	0.19	0.75	0.36	0.73	0.13
Uniform Delay, d1	32.2	4.5	27.1	22.4	39.6	35.5
Progression Factor	1.00	1.00	0.53	1.50	1.00	1.00
Incremental Delay, d2	6.2	0.2	0.4	0.2	4.3	0.1
Delay (s)	38.4	4.6	14.7	33.9	43.8	35.6
Level of Service	D	A	B	C	D	D
Approach Delay (s)		19.6	21.5		41.2	
Approach LOS		B	C		D	

Intersection Summary			
HCM 2000 Control Delay	25.0	HCM 2000 Level of Service	C
HCM 2000 Volume to Capacity ratio	0.77		
Actuated Cycle Length (s)	100.0	Sum of lost time (s)	18.4
Intersection Capacity Utilization	71.3%	ICU Level of Service	C
Analysis Period (min)	15		

c Critical Lane Group

HCM 2010 TWSC  
6: Riverside & EBR Vintage

03/02/2022

**Intersection**

Int Delay, s/veh 3.6

**Movement** EBL EBR NBL NBT SBT SBR

Lane Configurations		↗			↖	
Traffic Vol, veh/h	0	157	0	0	420	0
Future Vol, veh/h	0	157	0	0	420	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Free	Free
RT Channelized	-	Yield	-	None	-	None
Storage Length	-	0	-	-	-	-
Veh in Median Storage, #	0	-	-	-	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	93	93	93	93	93	93
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	169	0	0	452	0

**Major/Minor** Minor2 Major2

Conflicting Flow All	-	452	-	0
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Critical Hdwy	-	6.22	-	-
Critical Hdwy Stg 1	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-
Follow-up Hdwy	-	3.318	-	-
Pot Cap-1 Maneuver	0	608	-	0
Stage 1	0	-	-	0
Stage 2	0	-	-	0
Platoon blocked, %			-	-
Mov Cap-1 Maneuver	-	608	-	-
Mov Cap-2 Maneuver	-	-	-	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-

**Approach** EB SB

HCM Control Delay, s	13.2	0
HCM LOS	B	

**Minor Lane/Major Mvmt** EBLn1 SBT

Capacity (veh/h)	608	-
HCM Lane V/C Ratio	0.278	-
HCM Control Delay (s)	13.2	-
HCM Lane LOS	B	-
HCM 95th %tile Q(veh)	1.1	-

HCM 2010 TWSC  
7: Riverside & WBR Mendenhall Mall

03/02/2022

**Intersection**

Int Delay, s/veh 5.8

**Movement** WBL WBR NBT NBR SBL SBT

Lane Configurations		↗	↖			
Traffic Vol, veh/h	0	212	718	0	0	0
Future Vol, veh/h	0	212	718	0	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Stop	Stop
RT Channelized	-	Yield	-	None	-	None
Storage Length	-	0	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	-
Grade, %	0	-	0	-	-	0
Peak Hour Factor	93	93	93	93	93	93
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	228	772	0	0	0

**Major/Minor** Minor1 Major1

Conflicting Flow All	-	772	0	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Critical Hdwy	-	6.22	-	-
Critical Hdwy Stg 1	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-
Follow-up Hdwy	-	3.318	-	-
Pot Cap-1 Maneuver	0	400	-	0
Stage 1	0	-	-	0
Stage 2	0	-	-	0
Platoon blocked, %			-	
Mov Cap-1 Maneuver	-	400	-	-
Mov Cap-2 Maneuver	-	-	-	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-

**Approach** WB NB

HCM Control Delay, s	25.3	0
HCM LOS	D	

**Minor Lane/Major Mvmt** NBTWBLn1

Capacity (veh/h)	-	400
HCM Lane V/C Ratio	-	0.57
HCM Control Delay (s)	-	25.3
HCM Lane LOS	-	D
HCM 95th %tile Q(veh)	-	3.4

HCM 2010 TWSC  
8: NBR Riverside & Vintage/Mendenhall Mall

03/02/2022

**Intersection**

Int Delay, s/veh 3.6

**Movement** EBT EBR WBL WBT NBL NBR

Lane Configurations	↑					↑
Traffic Vol, veh/h	242	0	0	0	0	122
Future Vol, veh/h	242	0	0	0	0	122
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Stop	Stop	Stop	Stop
RT Channelized	-	None	-	None	-	Yield
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	0	-	-	-	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	93	93	93	93	93	93
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	260	0	0	0	0	131

**Major/Minor** Major1 Minor1

Conflicting Flow All	0	-	-	260
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Critical Hdwy	-	-	-	6.22
Critical Hdwy Stg 1	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-
Follow-up Hdwy	-	-	-	3.318
Pot Cap-1 Maneuver	-	0	0	779
Stage 1	-	0	0	-
Stage 2	-	0	0	-
Platoon blocked, %	-	-	-	-
Mov Cap-1 Maneuver	-	-	-	779
Mov Cap-2 Maneuver	-	-	-	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-

**Approach** EB NB

HCM Control Delay, s	0	10.6
HCM LOS		B

**Minor Lane/Major Mvmt** NBLn1 EBT

Capacity (veh/h)	779	-
HCM Lane V/C Ratio	0.168	-
HCM Control Delay (s)	10.6	-
HCM Lane LOS	B	-
HCM 95th %tile Q(veh)	0.6	-



HCM 2010 TWSC  
9: Vintage/Mendenhall Mall & SBR Riverside

03/02/2022

**Intersection**

Int Delay, s/veh 2.6

**Movement** EBL EBT WBT WBR SBL SBR

Lane Configurations			↑			↑
Traffic Vol, veh/h	0	0	428	0	0	112
Future Vol, veh/h	0	0	428	0	0	112
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	Yield
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	-	-	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	93	93	93	93	93	93
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	0	460	0	0	120

**Major/Minor** Major2 Minor2

Conflicting Flow All	-	0	-	460
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Critical Hdwy	-	-	-	6.22
Critical Hdwy Stg 1	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-
Follow-up Hdwy	-	-	-	3.318
Pot Cap-1 Maneuver	-	0	0	601
Stage 1	-	0	0	-
Stage 2	-	0	0	-
Platoon blocked, %	-	-	-	-
Mov Cap-1 Maneuver	-	-	-	601
Mov Cap-2 Maneuver	-	-	-	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-

**Approach** WB SB

HCM Control Delay, s	0	12.5
HCM LOS		B

**Minor Lane/Major Mvmt** WBT SBLn1

Capacity (veh/h)	-	601
HCM Lane V/C Ratio	-	0.2
HCM Control Delay (s)	-	12.5
HCM Lane LOS	-	B
HCM 95th %tile Q(veh)	-	0.7

## **Appendix D- AM Base + Site Traffic Capacity Analysis Report**

HCM 2010 TWSC  
1: Glacier Highway/Vintage Boulevard & Egan Drive

03/02/2022

**Intersection**

Int Delay, s/veh 1.3

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↑↑	↑	↑	↑↑	↑			↑			↑
Traffic Vol, veh/h	0	783	202	27	634	80	0	0	46	0	0	91
Future Vol, veh/h	0	783	202	27	634	80	0	0	46	0	0	91
Conflicting Peds, #/hr	5	0	5	5	0	5	5	0	5	5	0	5
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	Free	-	-	Free	-	-	Stop	-	-	Stop
Storage Length	-	-	375	300	-	475	-	-	0	-	-	0
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	80	80	80	80	80	80	80	80	80	80	80	80
Heavy Vehicles, %	2	5	2	0	5	2	0	0	2	0	0	7
Mvmt Flow	0	979	253	34	793	100	0	0	58	0	0	114

Major/Minor	Major1	Major2	Minor1	Minor2
Conflicting Flow All	- 0	- 984	0 0	- 500
Stage 1	- -	- -	- -	- -
Stage 2	- -	- -	- -	- -
Critical Hdwy	- -	4.1	- -	6.94
Critical Hdwy Stg 1	- -	- -	- -	- -
Critical Hdwy Stg 2	- -	- -	- -	- -
Follow-up Hdwy	- -	2.2	- -	3.32
Pot Cap-1 Maneuver	0 -	0 710	- 0 0	0 516
Stage 1	0 -	0 -	- 0 0	- 0 0
Stage 2	0 -	0 -	- 0 0	- 0 0
Platoon blocked, %	-	-	-	-
Mov Cap-1 Maneuver	- -	707	- -	512
Mov Cap-2 Maneuver	- -	- -	- -	- -
Stage 1	- -	- -	- -	- -
Stage 2	- -	- -	- -	- -

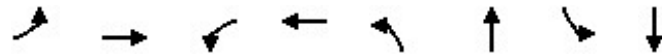
Approach	EB	WB	NB	SB
HCM Control Delay, s	0	0.4	12.9	12.7
HCM LOS			B	B

Minor Lane/Major Mvmt	NBLn1	EBT	WBL	WBT	SBLn1
Capacity (veh/h)	512	-	707	-	582
HCM Lane V/C Ratio	0.112	-	0.048	-	0.195
HCM Control Delay (s)	12.9	-	10.3	-	12.7
HCM Lane LOS	B	-	B	-	B
HCM 95th %tile Q(veh)	0.4	-	0.1	-	0.7

Queues

2: Riverside Drive & Vintage Boulevard/Mendenhall Mall Drive

03/02/2022



Lane Group	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Lane Group Flow (vph)	25	38	109	168	104	210	104	825
v/c Ratio	0.12	0.22	0.40	0.58	0.33	0.20	0.14	0.83
Control Delay	28.4	42.6	32.7	43.4	9.0	12.6	6.9	29.6
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	28.4	42.6	32.7	43.4	9.0	12.6	6.9	29.6
Queue Length 50th (ft)	11	21	50	80	18	58	18	391
Queue Length 95th (ft)	27	46	83	140	39	105	39	#623
Internal Link Dist (ft)		693		1080		707		271
Turn Bay Length (ft)	200		75		150		100	
Base Capacity (vph)	379	416	364	436	419	1043	869	999
Starvation Cap Reductn	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.07	0.09	0.30	0.39	0.25	0.20	0.12	0.83

Intersection Summary

# 95th percentile volume exceeds capacity, queue may be longer.  
 Queue shown is maximum after two cycles.

## HCM Signalized Intersection Capacity Analysis 2: Riverside Drive & Vintage Boulevard/Mendenhall Mall Drive

03/02/2022

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	19	29	0	84	129	0	80	162	0	80	635	0
Future Volume (vph)	19	29	0	84	129	0	80	162	0	80	635	0
Ideal Flow (vphpl)	1850	1850	1850	1850	1850	1850	1850	1850	1850	1850	1850	1850
Lane Width	12	12	16	12	12	16	12	12	16	12	12	16
Total Lost time (s)	4.3	4.5		4.3	4.5		4.3	4.7		4.3	4.7	
Lane Util. Factor	1.00	1.00		1.00	1.00		1.00	1.00		1.00	1.00	
Frpb, ped/bikes	1.00	1.00		1.00	1.00		1.00	1.00		1.00	1.00	
Flpb, ped/bikes	0.99	1.00		0.99	1.00		1.00	1.00		0.99	1.00	
Frt	1.00	1.00		1.00	1.00		1.00	1.00		1.00	1.00	
Flt Protected	0.95	1.00		0.95	1.00		0.95	1.00		0.95	1.00	
Satd. Flow (prot)	1706	1729		1709	1814		1723	1814		1706	1814	
Flt Permitted	0.65	1.00		0.48	1.00		0.12	1.00		0.63	1.00	
Satd. Flow (perm)	1169	1729		857	1814		213	1814		1125	1814	
Peak-hour factor, PHF	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77
Adj. Flow (vph)	25	38	0	109	168	0	104	210	0	104	825	0
RTOR Reduction (vph)	0	0	0	0	0	0	0	0	0	0	0	0
Lane Group Flow (vph)	25	38	0	109	168	0	104	210	0	104	825	0
Confl. Peds. (#/hr)	5		5	5		5	5		5	5		5
Heavy Vehicles (%)	2%	7%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%
Turn Type	pm+pt	NA	Perm	pm+pt	NA	Perm	pm+pt	NA	Perm	pm+pt	NA	Perm
Protected Phases	3	8		7	4		5	2		1	6	
Permitted Phases	8		8	4		4	2		2	6		6
Actuated Green, G (s)	10.3	8.0		20.4	13.8		57.2	49.1		53.2	47.1	
Effective Green, g (s)	10.3	8.0		20.4	13.8		57.2	49.1		53.2	47.1	
Actuated g/C Ratio	0.12	0.09		0.23	0.15		0.64	0.55		0.60	0.53	
Clearance Time (s)	4.3	4.5		4.3	4.5		4.3	4.7		4.3	4.7	
Vehicle Extension (s)	2.0	2.0		2.0	2.0		3.0	2.0		3.0	2.0	
Lane Grp Cap (vph)	148	155		273	280		274	999		711	958	
v/s Ratio Prot	0.00	0.02		c0.04	c0.09		c0.03	0.12		0.01	c0.45	
v/s Ratio Perm	0.02			0.05			0.21			0.08		
v/c Ratio	0.17	0.25		0.40	0.60		0.38	0.21		0.15	0.86	
Uniform Delay, d1	35.4	37.7		28.4	35.1		13.3	10.2		7.7	18.2	
Progression Factor	1.00	1.00		1.00	1.00		1.00	1.00		1.00	1.00	
Incremental Delay, d2	0.2	0.3		0.4	2.3		0.9	0.0		0.1	7.8	
Delay (s)	35.6	38.0		28.8	37.4		14.2	10.2		7.8	25.9	
Level of Service	D	D		C	D		B	B		A	C	
Approach Delay (s)		37.1			34.0			11.5			23.9	
Approach LOS		D			C			B			C	

### Intersection Summary

HCM 2000 Control Delay	23.7	HCM 2000 Level of Service	C
HCM 2000 Volume to Capacity ratio	0.75		
Actuated Cycle Length (s)	89.1	Sum of lost time (s)	17.8
Intersection Capacity Utilization	66.5%	ICU Level of Service	C
Analysis Period (min)	15		
c Critical Lane Group			

Queues

3: Mendenhall Loop Road & Mendenhall Mall Road/Atlin Road

03/02/2022



Lane Group	EBT	EBR	WBT	WBR	NBL	NBT	SBL	SBT	SBR
Lane Group Flow (vph)	63	130	34	6	77	485	6	1748	233
v/c Ratio	0.51	0.53	0.29	0.03	0.59	0.18	0.07	0.73	0.22
Control Delay	64.2	19.0	53.8	0.2	67.5	6.6	56.2	14.5	5.1
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.3	0.0
Total Delay	64.2	19.0	53.8	0.2	67.5	6.6	56.2	14.8	5.1
Queue Length 50th (ft)	48	11	25	0	46	18	5	387	32
Queue Length 95th (ft)	79	55	50	0	#124	151	18	530	71
Internal Link Dist (ft)	362		281			230		391	
Turn Bay Length (ft)		75		50	150		75		90
Base Capacity (vph)	244	379	236	338	133	2747	100	2409	1049
Starvation Cap Reductn	0	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	6	0	0	0	0	0	207	0
Storage Cap Reductn	0	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.26	0.35	0.14	0.02	0.58	0.18	0.06	0.79	0.22

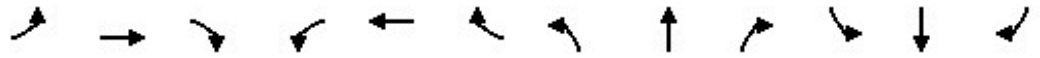
Intersection Summary

# 95th percentile volume exceeds capacity, queue may be longer.  
 Queue shown is maximum after two cycles.



HCM Signalized Intersection Capacity Analysis  
 3: Mendenhall Loop Road & Mendenhall Mall Road/Atlin Road

03/02/2022



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕	↗		↕	↗	↖	↕↗		↖	↕↗	↖
Traffic Volume (vph)	49	4	109	27	2	5	65	403	4	5	1468	196
Future Volume (vph)	49	4	109	27	2	5	65	403	4	5	1468	196
Ideal Flow (vphpl)	1850	1850	1850	1850	1850	1850	1850	1850	1850	1850	1850	1850
Total Lost time (s)		5.3	5.3		5.3	5.3	5.0	5.2		5.0	5.2	5.2
Lane Util. Factor		1.00	1.00		1.00	1.00	1.00	0.95		1.00	0.95	1.00
Frbp, ped/bikes		1.00	0.97		1.00	0.98	1.00	1.00		1.00	1.00	0.95
Flpb, ped/bikes		0.99	1.00		0.99	1.00	1.00	1.00		1.00	1.00	1.00
Frt		1.00	0.85		1.00	0.85	1.00	1.00		1.00	1.00	0.85
Flt Protected		0.96	1.00		0.96	1.00	0.95	1.00		0.95	1.00	1.00
Satd. Flow (prot)		1721	1499		1720	1511	1723	3439		1723	3446	1460
Flt Permitted		0.72	1.00		0.69	1.00	0.95	1.00		0.95	1.00	1.00
Satd. Flow (perm)		1293	1499		1248	1511	1723	3439		1723	3446	1460
Peak-hour factor, PHF	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84	0.84
Adj. Flow (vph)	58	5	130	32	2	6	77	480	5	6	1748	233
RTOR Reduction (vph)	0	0	104	0	0	5	0	0	0	0	0	28
Lane Group Flow (vph)	0	63	26	0	34	1	77	485	0	6	1748	205
Confl. Peds. (#/hr)	5		5	5		5	5		5	5		5
Turn Type	Perm	NA	Perm	Perm	NA	Perm	Prot	NA		Prot	NA	Perm
Protected Phases		8			4		5	2		1	6	
Permitted Phases	8		8	4		4						6
Actuated Green, G (s)		11.4	11.4		11.4	11.4	9.2	91.9		1.2	83.9	83.9
Effective Green, g (s)		11.4	11.4		11.4	11.4	9.2	91.9		1.2	83.9	83.9
Actuated g/C Ratio		0.10	0.10		0.10	0.10	0.08	0.77		0.01	0.70	0.70
Clearance Time (s)		5.3	5.3		5.3	5.3	5.0	5.2		5.0	5.2	5.2
Vehicle Extension (s)		1.5	1.5		1.5	1.5	2.0	2.0		2.0	2.0	2.0
Lane Grp Cap (vph)		122	142		118	143	132	2633		17	2409	1020
v/s Ratio Prot							c0.04	0.14		0.00	c0.51	
v/s Ratio Perm		c0.05	0.02		0.03	0.00						0.14
v/c Ratio		0.52	0.18		0.29	0.00	0.58	0.18		0.35	0.73	0.20
Uniform Delay, d1		51.7	50.0		50.5	49.2	53.5	3.8		59.0	11.0	6.3
Progression Factor		1.00	1.00		1.00	1.00	0.92	1.64		1.00	1.00	1.00
Incremental Delay, d2		1.5	0.2		0.5	0.0	4.2	0.2		4.5	1.9	0.4
Delay (s)		53.2	50.2		51.0	49.2	53.2	6.4		63.6	13.0	6.8
Level of Service		D	D		D	D	D	A		E	B	A
Approach Delay (s)		51.2			50.7			12.8			12.4	
Approach LOS		D			D			B			B	

Intersection Summary			
HCM 2000 Control Delay	15.7	HCM 2000 Level of Service	B
HCM 2000 Volume to Capacity ratio	0.69		
Actuated Cycle Length (s)	120.0	Sum of lost time (s)	15.5
Intersection Capacity Utilization	69.4%	ICU Level of Service	C
Analysis Period (min)	15		
c Critical Lane Group			

Queues

4: Mendenhall Loop Road & Egan Drive

03/02/2022



Lane Group	EBL	EBT	EBR	WBL	WBT	NBL	NBT	NBR	SBL	SBT	SBR
Lane Group Flow (vph)	62	1121	135	8	447	133	119	41	809	830	126
v/c Ratio	0.55	1.07	0.26	0.11	0.57	0.74	0.63	0.15	1.08	1.09	0.17
Control Delay	72.1	87.6	9.5	58.9	46.9	75.1	65.4	1.2	80.9	83.9	4.7
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.6	0.0
Total Delay	72.1	87.6	9.5	58.9	46.9	75.1	65.4	1.2	80.9	84.5	4.7
Queue Length 50th (ft)	47	432	5	6	159	102	90	0	~702	~727	13
Queue Length 95th (ft)	#100	#782	63	23	#247	160	144	0	#933	#959	m27
Internal Link Dist (ft)		1356			1654		696			316	
Turn Bay Length (ft)	540		120	550		290		390	280		
Base Capacity (vph)	123	1051	529	72	787	277	291	348	749	762	727
Starvation Cap Reductn	0	0	0	0	0	0	0	0	0	1	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.50	1.07	0.26	0.11	0.57	0.48	0.41	0.12	1.08	1.09	0.17

Intersection Summary

- ~ Volume exceeds capacity, queue is theoretically infinite.  
Queue shown is maximum after two cycles.
- # 95th percentile volume exceeds capacity, queue may be longer.  
Queue shown is maximum after two cycles.
- m Volume for 95th percentile queue is metered by upstream signal.

HCM Signalized Intersection Capacity Analysis  
4: Mendenhall Loop Road & Egan Drive

03/02/2022

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	56	1020	123	7	407	0	121	108	37	1269	222	115
Future Volume (vph)	56	1020	123	7	407	0	121	108	37	1269	222	115
Ideal Flow (vphpl)	1850	1850	1850	1850	1850	1850	1850	1850	1850	1850	1850	1850
Total Lost time (s)	5.2	5.9	5.9	5.2	5.9		5.7	5.7	5.7	5.8	5.8	5.8
Lane Util. Factor	1.00	*1.00	1.00	1.00	*1.00		1.00	1.00	1.00	*1.00	*1.00	1.00
Frbp, ped/bikes	1.00	1.00	0.96	1.00	1.00		1.00	1.00	0.98	1.00	1.00	0.98
Flpb, ped/bikes	1.00	1.00	1.00	1.00	1.00		1.00	1.00	1.00	1.00	1.00	1.00
Frt	1.00	1.00	0.85	1.00	1.00		1.00	1.00	0.85	1.00	1.00	0.85
Flt Protected	0.95	1.00	1.00	0.95	1.00		0.95	1.00	1.00	0.95	0.97	1.00
Satd. Flow (prot)	1723	3524	1481	1723	3524		1723	1814	1509	1723	1752	1511
Flt Permitted	0.95	1.00	1.00	0.95	1.00		0.95	1.00	1.00	0.95	0.97	1.00
Satd. Flow (perm)	1723	3524	1481	1723	3524		1723	1814	1509	1723	1752	1511
Peak-hour factor, PHF	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91
Adj. Flow (vph)	62	1121	135	8	447	0	133	119	41	1395	244	126
RTOR Reduction (vph)	0	0	92	0	0	0	0	0	37	0	0	71
Lane Group Flow (vph)	62	1121	43	8	447	0	133	119	4	809	830	55
Confl. Peds. (#/hr)	5		5	5		5	5		5	5		5
Heavy Vehicles (%)	2%	5%	2%	2%	5%	2%	2%	2%	2%	2%	2%	2%
Turn Type	Prot	NA	Perm	Prot	NA	Perm	Split	NA	Perm	Split	NA	Perm
Protected Phases	1	6		5	2		3	3		4	4	
Permitted Phases			6			2			3			4
Actuated Green, G (s)	6.9	31.7	31.7	1.0	25.8		12.5	12.5	12.5	52.2	52.2	52.2
Effective Green, g (s)	6.9	31.7	31.7	1.0	25.8		12.5	12.5	12.5	52.2	52.2	52.2
Actuated g/C Ratio	0.06	0.26	0.26	0.01	0.22		0.10	0.10	0.10	0.44	0.44	0.44
Clearance Time (s)	5.2	5.9	5.9	5.2	5.9		5.7	5.7	5.7	5.8	5.8	5.8
Vehicle Extension (s)	2.0	0.5	0.5	2.0	0.5		0.5	0.5	0.5	2.0	2.0	2.0
Lane Grp Cap (vph)	99	930	391	14	757		179	188	157	749	762	657
v/s Ratio Prot	c0.04	c0.32		0.00	0.13		c0.08	0.07		0.47	c0.47	
v/s Ratio Perm			0.03						0.00			0.04
v/c Ratio	0.63	1.21	0.11	0.57	0.59		0.74	0.63	0.03	1.08	1.09	0.08
Uniform Delay, d1	55.3	44.1	33.5	59.3	42.4		52.2	51.6	48.3	33.9	33.9	19.9
Progression Factor	1.00	1.00	1.00	1.00	1.00		1.00	1.00	1.00	0.83	0.83	1.19
Incremental Delay, d2	8.6	102.6	0.6	30.5	3.4		13.5	5.0	0.0	52.0	55.1	0.0
Delay (s)	63.9	146.8	34.0	89.8	45.7		65.7	56.6	48.3	80.2	83.4	23.6
Level of Service	E	F	C	F	D		E	E	D	F	F	C
Approach Delay (s)		131.3			46.5			59.6			77.6	
Approach LOS		F			D			E			E	

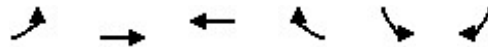
Intersection Summary		
HCM 2000 Control Delay	91.0	HCM 2000 Level of Service
HCM 2000 Volume to Capacity ratio	1.09	F
Actuated Cycle Length (s)	120.0	Sum of lost time (s)
Intersection Capacity Utilization	101.5%	22.6
Analysis Period (min)	15	ICU Level of Service
		G

c Critical Lane Group

Queues

5: Egan Drive & Riverside Drive

03/02/2022



Lane Group	EBL	EBT	WBT	WBR	SBL	SBR
Lane Group Flow (vph)	213	786	628	146	660	265
v/c Ratio	0.64	0.43	0.72	0.30	0.69	0.46
Control Delay	37.0	10.5	29.8	6.6	27.3	11.3
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	37.0	10.5	29.8	6.6	27.3	11.3
Queue Length 50th (ft)	80	91	122	0	121	27
Queue Length 95th (ft)	171	154	214	35	215	88
Internal Link Dist (ft)		694	1356		707	
Turn Bay Length (ft)	350			900	225	145
Base Capacity (vph)	772	3181	2060	942	2057	622
Starvation Cap Reductn	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0
Reduced v/c Ratio	0.28	0.25	0.30	0.15	0.32	0.43

Intersection Summary

# HCM Signalized Intersection Capacity Analysis

## 5: Egan Drive & Riverside Drive

03/02/2022



Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Volume (vph)	177	652	521	121	548	220
Future Volume (vph)	177	652	521	121	548	220
Ideal Flow (vphpl)	1850	1850	1850	1850	1850	1850
Total Lost time (s)	5.3	5.8	5.8	5.8	5.3	5.3
Lane Util. Factor	1.00	0.95	0.95	1.00	0.97	1.00
Frbp, ped/bikes	1.00	1.00	1.00	0.97	1.00	1.00
Fipb, ped/bikes	1.00	1.00	1.00	1.00	1.00	1.00
Frt	1.00	1.00	1.00	0.85	1.00	0.85
Flt Protected	0.95	1.00	1.00	1.00	0.95	1.00
Satd. Flow (prot)	1674	3348	3348	1453	3343	1542
Flt Permitted	0.95	1.00	1.00	1.00	0.95	1.00
Satd. Flow (perm)	1674	3348	3348	1453	3343	1542
Peak-hour factor, PHF	0.83	0.83	0.83	0.83	0.83	0.83
Adj. Flow (vph)	213	786	628	146	660	265
RTOR Reduction (vph)	0	0	0	107	0	126
Lane Group Flow (vph)	213	786	628	39	660	139
Confl. Peds. (#/hr)	5			5	5	5
Heavy Vehicles (%)	5%	5%	5%	5%	2%	2%
Turn Type	Prot	NA	NA	Perm	Prot	Prot
Protected Phases	1	6	2		4	8
Permitted Phases				2		
Actuated Green, G (s)	13.8	37.4	18.3	18.3	19.9	19.9
Effective Green, g (s)	13.8	37.4	18.3	18.3	19.9	19.9
Actuated g/C Ratio	0.20	0.55	0.27	0.27	0.29	0.29
Clearance Time (s)	5.3	5.8	5.8	5.8	5.3	5.3
Vehicle Extension (s)	1.0	0.5	0.5	0.5	2.0	2.0
Lane Grp Cap (vph)	337	1830	895	388	972	448
v/s Ratio Prot	c0.13	0.23	c0.19		c0.20	0.09
v/s Ratio Perm				0.03		
v/c Ratio	0.63	0.43	0.70	0.10	0.68	0.31
Uniform Delay, d1	25.0	9.2	22.6	18.9	21.4	18.9
Progression Factor	1.00	1.00	1.00	1.00	1.00	1.00
Incremental Delay, d2	2.8	0.1	2.1	0.0	1.5	0.1
Delay (s)	27.8	9.2	24.6	18.9	22.9	19.0
Level of Service	C	A	C	B	C	B
Approach Delay (s)		13.2	23.6		21.8	
Approach LOS		B	C		C	

Intersection Summary			
HCM 2000 Control Delay	19.1	HCM 2000 Level of Service	B
HCM 2000 Volume to Capacity ratio	0.70		
Actuated Cycle Length (s)	68.4	Sum of lost time (s)	18.4
Intersection Capacity Utilization	54.6%	ICU Level of Service	A
Analysis Period (min)	15		

c Critical Lane Group

HCM 2010 TWSC  
6: Riverside & EBR Vintage

03/02/2022

**Intersection**

Int Delay, s/veh 1.2

**Movement** EBL EBR NBL NBT SBT SBR

Lane Configurations		↗			↖	
Traffic Vol, veh/h	0	49	0	0	719	0
Future Vol, veh/h	0	49	0	0	719	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Free	Free
RT Channelized	-	Yield	-	None	-	None
Storage Length	-	0	-	-	-	-
Veh in Median Storage, #	0	-	-	-	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	77	77	77	77	77	77
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	64	0	0	934	0

**Major/Minor** Minor2 Major2

Conflicting Flow All	-	934	-	0
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Critical Hdwy	-	6.22	-	-
Critical Hdwy Stg 1	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-
Follow-up Hdwy	-	3.318	-	-
Pot Cap-1 Maneuver	0	322	-	0
Stage 1	0	-	-	0
Stage 2	0	-	-	0
Platoon blocked, %			-	-
Mov Cap-1 Maneuver	-	322	-	-
Mov Cap-2 Maneuver	-	-	-	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-

**Approach** EB SB

HCM Control Delay, s	18.9	0
HCM LOS	C	

**Minor Lane/Major Mvmt** EBLn1 SBT

Capacity (veh/h)	322	-
HCM Lane V/C Ratio	0.198	-
HCM Control Delay (s)	18.9	-
HCM Lane LOS	C	-
HCM 95th %tile Q(veh)	0.7	-



HCM 2010 TWSC  
7: Riverside & WBR Mendenhall Mall

03/02/2022

**Intersection**

Int Delay, s/veh 2.1

**Movement** WBL WBR NBT NBR SBL SBT

Lane Configurations		↗	↖			
Traffic Vol, veh/h	0	49	181	0	0	0
Future Vol, veh/h	0	49	181	0	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Stop	Stop
RT Channelized	-	Yield	-	None	-	None
Storage Length	-	0	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	-
Grade, %	0	-	0	-	-	0
Peak Hour Factor	77	77	77	77	77	77
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	64	235	0	0	0

**Major/Minor** Minor1 Major1

Conflicting Flow All	-	235	0	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Critical Hdwy	-	6.22	-	-
Critical Hdwy Stg 1	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-
Follow-up Hdwy	-	3.318	-	-
Pot Cap-1 Maneuver	0	804	-	0
Stage 1	0	-	-	0
Stage 2	0	-	-	0
Platoon blocked, %			-	
Mov Cap-1 Maneuver	-	804	-	-
Mov Cap-2 Maneuver	-	-	-	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-

**Approach** WB NB

HCM Control Delay, s	9.9	0
HCM LOS	A	

**Minor Lane/Major Mvmt** NBTWBLn1

Capacity (veh/h)	-	804
HCM Lane V/C Ratio	-	0.079
HCM Control Delay (s)	-	9.9
HCM Lane LOS	-	A
HCM 95th %tile Q(veh)	-	0.3

HCM 2010 TWSC  
8: NBR Riverside & Vintage/Mendenhall Mall

03/02/2022

**Intersection**

Int Delay, s/veh 3.1

**Movement** EBT EBR WBL WBT NBL NBR

Lane Configurations	↑					↑
Traffic Vol, veh/h	109	0	0	0	0	55
Future Vol, veh/h	109	0	0	0	0	55
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Stop	Stop	Stop	Stop
RT Channelized	-	None	-	None	-	Yield
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	0	-	-	-	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	77	77	77	77	77	77
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	142	0	0	0	0	71

**Major/Minor** Major1 Minor1

Conflicting Flow All	0	-	-	142
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Critical Hdwy	-	-	-	6.22
Critical Hdwy Stg 1	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-
Follow-up Hdwy	-	-	-	3.318
Pot Cap-1 Maneuver	-	0	0	906
Stage 1	-	0	0	-
Stage 2	-	0	0	-
Platoon blocked, %	-	-	-	-
Mov Cap-1 Maneuver	-	-	-	906
Mov Cap-2 Maneuver	-	-	-	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-

**Approach** EB NB

HCM Control Delay, s	0	9.3
HCM LOS		A

**Minor Lane/Major Mvmt** NBLn1 EBT

Capacity (veh/h)	906	-
HCM Lane V/C Ratio	0.079	-
HCM Control Delay (s)	9.3	-
HCM Lane LOS	A	-
HCM 95th %tile Q(veh)	0.3	-

HCM 2010 TWSC  
9: Vintage/Mendenhall Mall & SBR Riverside

03/02/2022

**Intersection**

Int Delay, s/veh 4.5

**Movement** EBL EBT WBT WBR SBL SBR

Lane Configurations			↑			↑
Traffic Vol, veh/h	0	0	209	0	0	142
Future Vol, veh/h	0	0	209	0	0	142
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	Yield
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	9306112		0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	77	77	77	77	77	77
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	0	271	0	0	184

**Major/Minor** Major2 Minor2

Conflicting Flow All	-	0	-	271
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Critical Hdwy	-	-	-	6.22
Critical Hdwy Stg 1	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-
Follow-up Hdwy	-	-	-	3.318
Pot Cap-1 Maneuver	-	0	0	768
Stage 1	-	0	0	-
Stage 2	-	0	0	-
Platoon blocked, %	-	-	-	-
Mov Cap-1 Maneuver	-	-	-	768
Mov Cap-2 Maneuver	-	-	-	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-

**Approach** WB SB

HCM Control Delay, s	0	11.2
HCM LOS		B

**Minor Lane/Major Mvmt** WBT SBLn1

Capacity (veh/h)	-	768
HCM Lane V/C Ratio	-	0.24
HCM Control Delay (s)	-	11.2
HCM Lane LOS	-	B
HCM 95th %tile Q(veh)	-	0.9

## **Appendix E- PM Base + Site Traffic Capacity Analysis Report**

HCM 2010 TWSC  
1: Glacier Highway/Vintage Boulevard & Egan Drive

03/02/2022

**Intersection**

Int Delay, s/veh 2.4

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↑↑	↑	↑	↑↑	↑			↑			↑
Traffic Vol, veh/h	0	744	166	54	900	162	0	0	70	0	0	202
Future Vol, veh/h	0	744	166	54	900	162	0	0	70	0	0	202
Conflicting Peds, #/hr	5	0	5	5	0	5	5	0	5	5	0	5
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	Free	-	-	Free	-	-	Stop	-	-	Stop
Storage Length	-	-	375	300	-	475	-	-	0	-	-	0
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	94	94	94	94	94	94	94	94	94	94	94	94
Heavy Vehicles, %	2	5	2	2	5	2	2	2	2	0	0	7
Mvmt Flow	0	791	177	57	957	172	0	0	74	0	0	215

Major/Minor	Major1	Major2	Minor1	Minor2								
Conflicting Flow All	-	0	-	796	0	0	-	-	406	-	-	484
Stage 1	-	-	-	-	-	-	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-	-	-	-	-	-	-
Critical Hdwy	-	-	-	4.14	-	-	-	-	6.94	-	-	7.04
Critical Hdwy Stg 1	-	-	-	-	-	-	-	-	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-	-	-	-	-	-	-	-	-
Follow-up Hdwy	-	-	-	2.22	-	-	-	-	3.32	-	-	3.37
Pot Cap-1 Maneuver	0	-	0	822	-	0	0	0	594	0	0	516
Stage 1	0	-	0	-	-	0	0	0	-	0	0	-
Stage 2	0	-	0	-	-	0	0	0	-	0	0	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	-	819	-	-	-	-	589	-	-	514
Mov Cap-2 Maneuver	-	-	-	-	-	-	-	-	-	-	-	-
Stage 1	-	-	-	-	-	-	-	-	-	-	-	-
Stage 2	-	-	-	-	-	-	-	-	-	-	-	-

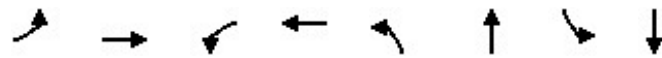
Approach	EB	WB	NB	SB
HCM Control Delay, s	0	0.6	12	16.9
HCM LOS			B	C

Minor Lane/Major Mvmt	NBLn1	EBT	WBL	WBT	SBLn1
Capacity (veh/h)	589	-	819	-	514
HCM Lane V/C Ratio	0.126	-	0.07	-	0.418
HCM Control Delay (s)	12	-	9.7	-	16.9
HCM Lane LOS	B	-	A	-	C
HCM 95th %tile Q(veh)	0.4	-	0.2	-	2

Queues

2: Riverside Drive & Vintage Boulevard/Mendenhall Mall Drive

03/02/2022



Lane Group	EBL	EBT	WBL	WBT	NBL	NBT	SBL	SBT
Lane Group Flow (vph)	340	206	162	152	338	475	80	291
v/c Ratio	0.71	0.60	0.39	0.54	0.70	0.72	0.25	0.68
Control Delay	29.5	38.9	20.3	39.9	22.3	30.2	14.4	35.8
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	29.5	38.9	20.3	39.9	22.3	30.2	14.4	35.8
Queue Length 50th (ft)	115	88	48	67	97	198	19	127
Queue Length 95th (ft)	#241	197	116	148	187	364	48	229
Internal Link Dist (ft)		693		1080		707		271
Turn Bay Length (ft)	200		75		150		100	
Base Capacity (vph)	494	467	525	490	507	1103	516	1103
Starvation Cap Reductn	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.69	0.44	0.31	0.31	0.67	0.43	0.16	0.26

Intersection Summary

# 95th percentile volume exceeds capacity, queue may be longer.  
 Queue shown is maximum after two cycles.



## HCM Signalized Intersection Capacity Analysis 2: Riverside Drive & Vintage Boulevard/Mendenhall Mall Drive

03/02/2022

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	316	192	0	151	141	0	314	442	0	74	271	0
Future Volume (vph)	316	192	0	151	141	0	314	442	0	74	271	0
Ideal Flow (vphpl)	1850	1850	1850	1850	1850	1850	1850	1850	1850	1850	1850	1850
Lane Width	12	12	16	12	12	16	12	12	16	12	12	16
Total Lost time (s)	4.3	4.5		4.3	4.5		4.3	4.7		4.3	4.7	
Lane Util. Factor	1.00	1.00		1.00	1.00		1.00	1.00		1.00	1.00	
Frpb, ped/bikes	1.00	1.00		1.00	1.00		1.00	1.00		1.00	1.00	
Flpb, ped/bikes	1.00	1.00		0.99	1.00		1.00	1.00		1.00	1.00	
Frt	1.00	1.00		1.00	1.00		1.00	1.00		1.00	1.00	
Flt Protected	0.95	1.00		0.95	1.00		0.95	1.00		0.95	1.00	
Satd. Flow (prot)	1715	1729		1706	1814		1723	1814		1718	1814	
Flt Permitted	0.44	1.00		0.63	1.00		0.32	1.00		0.38	1.00	
Satd. Flow (perm)	801	1729		1126	1814		573	1814		690	1814	
Peak-hour factor, PHF	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93	0.93
Adj. Flow (vph)	340	206	0	162	152	0	338	475	0	80	291	0
RTOR Reduction (vph)	0	0	0	0	0	0	0	0	0	0	0	0
Lane Group Flow (vph)	340	206	0	162	152	0	338	475	0	80	291	0
Confl. Peds. (#/hr)	5		5	5		5	5		5	5		5
Heavy Vehicles (%)	2%	7%	2%	2%	2%	2%	2%	2%	2%	2%	2%	2%
Turn Type	pm+pt	NA	Perm	pm+pt	NA	Perm	pm+pt	NA	Perm	pm+pt	NA	Perm
Protected Phases	3	8		7	4		5	2		1	6	
Permitted Phases	8		8	4		4	2		2	6		6
Actuated Green, G (s)	29.8	15.4		22.8	11.9		37.8	28.1		24.8	19.4	
Effective Green, g (s)	29.8	15.4		22.8	11.9		37.8	28.1		24.8	19.4	
Actuated g/C Ratio	0.38	0.20		0.29	0.15		0.49	0.36		0.32	0.25	
Clearance Time (s)	4.3	4.5		4.3	4.5		4.3	4.7		4.3	4.7	
Vehicle Extension (s)	2.0	2.0		3.0	2.0		2.0	2.0		2.0	2.0	
Lane Grp Cap (vph)	477	343		412	278		488	656		292	453	
v/s Ratio Prot	c0.13	0.12		0.06	0.08		c0.13	c0.26		0.02	0.16	
v/s Ratio Perm	c0.14			0.06			0.21			0.07		
v/c Ratio	0.71	0.60		0.39	0.55		0.69	0.72		0.27	0.64	
Uniform Delay, d1	18.6	28.3		21.4	30.4		13.8	21.4		19.0	26.0	
Progression Factor	1.00	1.00		1.00	1.00		1.00	1.00		1.00	1.00	
Incremental Delay, d2	4.2	2.0		0.6	1.2		3.4	3.4		0.2	2.3	
Delay (s)	22.8	30.3		22.0	31.5		17.2	24.8		19.2	28.3	
Level of Service	C	C		C	C		B	C		B	C	
Approach Delay (s)		25.6			26.6			21.6			26.4	
Approach LOS		C			C			C			C	

Intersection Summary			
HCM 2000 Control Delay	24.3	HCM 2000 Level of Service	C
HCM 2000 Volume to Capacity ratio	0.78		
Actuated Cycle Length (s)	77.6	Sum of lost time (s)	17.8
Intersection Capacity Utilization	73.2%	ICU Level of Service	D
Analysis Period (min)	15		
c Critical Lane Group			

Queues

3: Mendenhall Loop Road & Mendenhall Mall Road/Atlin Road

03/02/2022



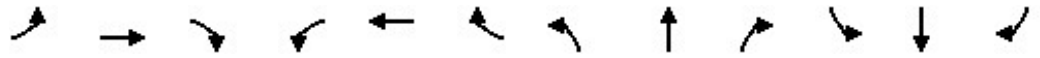
Lane Group	EBT	EBR	WBT	WBR	NBL	NBT	SBL	SBT	SBR
Lane Group Flow (vph)	261	166	16	8	333	1673	10	726	219
v/c Ratio	0.90	0.39	0.05	0.02	1.28	0.75	0.10	0.45	0.27
Control Delay	70.6	12.8	29.5	0.1	192.2	14.5	46.4	18.0	3.2
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.4	0.0	0.0	0.0
Total Delay	70.6	12.8	29.5	0.1	192.2	14.9	46.4	18.0	3.2
Queue Length 50th (ft)	157	22	8	0	~372	274	6	138	5
Queue Length 95th (ft)	#288	77	25	0	m#518	m#623	23	181	40
Internal Link Dist (ft)	362		281			230		391	
Turn Bay Length (ft)		75		50	150		75		90
Base Capacity (vph)	323	465	325	433	260	2233	120	1819	870
Starvation Cap Reductn	0	0	0	0	0	183	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.81	0.36	0.05	0.02	1.28	0.82	0.08	0.40	0.25

Intersection Summary

- ~ Volume exceeds capacity, queue is theoretically infinite.  
Queue shown is maximum after two cycles.
- # 95th percentile volume exceeds capacity, queue may be longer.  
Queue shown is maximum after two cycles.
- m Volume for 95th percentile queue is metered by upstream signal.

HCM Signalized Intersection Capacity Analysis  
 3: Mendenhall Loop Road & Mendenhall Mall Road/Atlin Road

03/02/2022



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕	↗		↕	↗	↖	↕		↖	↕	↗
Traffic Volume (vph)	224	14	151	14	1	7	303	1494	28	9	661	199
Future Volume (vph)	224	14	151	14	1	7	303	1494	28	9	661	199
Ideal Flow (vphpl)	1850	1850	1850	1850	1850	1850	1850	1850	1850	1850	1850	1850
Total Lost time (s)		5.3	5.3		5.3	5.3	5.0	5.2		5.0	5.2	5.2
Lane Util. Factor		1.00	1.00		1.00	1.00	1.00	0.95		1.00	0.95	1.00
Frbp, ped/bikes		1.00	0.98		1.00	0.98	1.00	1.00		1.00	1.00	0.95
Flpb, ped/bikes		0.99	1.00		1.00	1.00	1.00	1.00		1.00	1.00	1.00
Frt		1.00	0.85		1.00	0.85	1.00	1.00		1.00	1.00	0.85
Flt Protected		0.95	1.00		0.96	1.00	0.95	1.00		0.95	1.00	1.00
Satd. Flow (prot)		1721	1513		1726	1513	1723	3434		1723	3446	1469
Flt Permitted		0.73	1.00		0.73	1.00	0.95	1.00		0.95	1.00	1.00
Satd. Flow (perm)		1310	1513		1317	1513	1723	3434		1723	3446	1469
Peak-hour factor, PHF	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91
Adj. Flow (vph)	246	15	166	15	1	8	333	1642	31	10	726	219
RTOR Reduction (vph)	0	0	95	0	0	6	0	1	0	0	0	106
Lane Group Flow (vph)	0	261	71	0	16	2	333	1672	0	10	726	113
Confl. Peds. (#/hr)	5		5	5		5	5		5	5		5
Turn Type	Perm	NA	Perm	Perm	NA	Perm	Prot	NA		Prot	NA	Perm
Protected Phases		8			4		5	2		1	6	
Permitted Phases	8		8	4		4						6
Actuated Green, G (s)		22.2	22.2		22.2	22.2	15.1	61.0		1.3	47.2	47.2
Effective Green, g (s)		22.2	22.2		22.2	22.2	15.1	61.0		1.3	47.2	47.2
Actuated g/C Ratio		0.22	0.22		0.22	0.22	0.15	0.61		0.01	0.47	0.47
Clearance Time (s)		5.3	5.3		5.3	5.3	5.0	5.2		5.0	5.2	5.2
Vehicle Extension (s)		1.5	1.5		1.5	1.5	2.0	2.0		2.0	2.0	2.0
Lane Grp Cap (vph)		290	335		292	335	260	2094		22	1626	693
v/s Ratio Prot							c0.19	c0.49		0.01	0.21	
v/s Ratio Perm		c0.20	0.05		0.01	0.00						0.08
v/c Ratio		0.90	0.21		0.05	0.01	1.28	0.80		0.45	0.45	0.16
Uniform Delay, d1		37.8	31.8		30.6	30.3	42.5	14.8		49.0	17.7	15.1
Progression Factor		1.00	1.00		1.00	1.00	1.07	0.85		1.00	1.00	1.00
Incremental Delay, d2		28.2	0.1		0.0	0.0	152.3	3.3		5.3	0.9	0.5
Delay (s)		66.0	31.9		30.7	30.3	197.7	15.9		54.3	18.6	15.6
Level of Service		E	C		C	C	F	B		D	B	B
Approach Delay (s)		52.7			30.5			46.1			18.3	
Approach LOS		D			C			D			B	

Intersection Summary			
HCM 2000 Control Delay	39.0	HCM 2000 Level of Service	D
HCM 2000 Volume to Capacity ratio	0.93		
Actuated Cycle Length (s)	100.0	Sum of lost time (s)	15.5
Intersection Capacity Utilization	81.4%	ICU Level of Service	D
Analysis Period (min)	15		
c Critical Lane Group			

Queues

4: Mendenhall Loop Road & Egan Drive

03/02/2022



Lane Group	EBL	EBT	EBR	WBL	WBT	NBL	NBT	NBR	SBL	SBT	SBR
Lane Group Flow (vph)	108	629	143	33	1092	320	346	20	389	395	66
v/c Ratio	0.67	0.57	0.24	0.32	1.28	0.96	0.99	0.05	1.03	1.01	0.14
Control Delay	53.9	23.2	4.2	53.3	168.1	82.6	87.1	0.2	80.9	76.7	1.8
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	53.9	23.2	4.2	53.3	168.1	82.6	87.1	0.2	80.9	76.7	1.8
Queue Length 50th (ft)	47	208	22	21	~481	204	222	0	~282	~273	4
Queue Length 95th (ft)	#106	225	53	51	#612	#375	#403	0	#477	#478	11
Internal Link Dist (ft)		1356			1654		696			316	
Turn Bay Length (ft)	540		120	550		290		390	280		
Base Capacity (vph)	186	1104	590	117	853	332	350	414	379	390	466
Starvation Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0	0	0	0	0	0
Reduced v/c Ratio	0.58	0.57	0.24	0.28	1.28	0.96	0.99	0.05	1.03	1.01	0.14

Intersection Summary

- ~ Volume exceeds capacity, queue is theoretically infinite.  
Queue shown is maximum after two cycles.
- # 95th percentile volume exceeds capacity, queue may be longer.  
Queue shown is maximum after two cycles.

# HCM Signalized Intersection Capacity Analysis

## 4: Mendenhall Loop Road & Egan Drive

03/02/2022

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	105	610	139	32	1059	0	310	336	19	564	197	64
Future Volume (vph)	105	610	139	32	1059	0	310	336	19	564	197	64
Ideal Flow (vphpl)	1850	1850	1850	1850	1850	1850	1850	1850	1850	1850	1850	1850
Total Lost time (s)	5.2	5.9	5.9	5.2	5.9		5.7	5.7	5.7	5.8	5.8	5.8
Lane Util. Factor	1.00	0.95	1.00	1.00	0.95		1.00	1.00	1.00	0.95	0.95	1.00
Frbp, ped/bikes	1.00	1.00	0.96	1.00	1.00		1.00	1.00	0.98	1.00	1.00	0.98
Flpb, ped/bikes	1.00	1.00	1.00	1.00	1.00		1.00	1.00	1.00	1.00	1.00	1.00
Frt	1.00	1.00	0.85	1.00	1.00		1.00	1.00	0.85	1.00	1.00	0.85
Flt Protected	0.95	1.00	1.00	0.95	1.00		0.95	1.00	1.00	0.95	0.98	1.00
Satd. Flow (prot)	1723	3348	1486	1723	3348		1723	1814	1511	1637	1682	1513
Flt Permitted	0.95	1.00	1.00	0.95	1.00		0.95	1.00	1.00	0.95	0.98	1.00
Satd. Flow (perm)	1723	3348	1486	1723	3348		1723	1814	1511	1637	1682	1513
Peak-hour factor, PHF	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97	0.97
Adj. Flow (vph)	108	629	143	33	1092	0	320	346	20	581	203	66
RTOR Reduction (vph)	0	0	99	0	0	0	0	0	16	0	0	51
Lane Group Flow (vph)	108	629	44	33	1092	0	320	346	4	389	395	15
Confl. Peds. (#/hr)	5		5	5		5	5		5	5		5
Heavy Vehicles (%)	2%	5%	2%	2%	5%	2%	2%	2%	2%	2%	2%	2%
Turn Type	Prot	NA	Perm	Prot	NA	Perm	Split	NA	Perm	Split	NA	Perm
Protected Phases	1	6		5	2		3	3		4	4	
Permitted Phases			6			2			3			4
Actuated Green, G (s)	9.4	30.9	30.9	4.0	25.5		19.3	19.3	19.3	23.2	23.2	23.2
Effective Green, g (s)	9.4	30.9	30.9	4.0	25.5		19.3	19.3	19.3	23.2	23.2	23.2
Actuated g/C Ratio	0.09	0.31	0.31	0.04	0.26		0.19	0.19	0.19	0.23	0.23	0.23
Clearance Time (s)	5.2	5.9	5.9	5.2	5.9		5.7	5.7	5.7	5.8	5.8	5.8
Vehicle Extension (s)	2.0	0.5	0.5	2.0	0.5		0.5	0.5	0.5	2.0	2.0	2.0
Lane Grp Cap (vph)	161	1034	459	68	853		332	350	291	379	390	351
v/s Ratio Prot	c0.06	0.19		0.02	c0.33		0.19	c0.19		c0.24	0.23	
v/s Ratio Perm			0.03						0.00			0.01
v/c Ratio	0.67	0.61	0.10	0.49	1.28		0.96	0.99	0.01	1.03	1.01	0.04
Uniform Delay, d1	43.8	29.4	24.6	47.0	37.2		40.0	40.2	32.6	38.4	38.4	29.8
Progression Factor	0.79	0.72	0.73	1.00	1.00		1.00	1.00	1.00	0.68	0.68	1.00
Incremental Delay, d2	8.1	2.6	0.4	2.0	135.1		39.3	44.5	0.0	51.7	47.4	0.0
Delay (s)	42.6	23.8	18.3	49.0	172.3		79.3	84.7	32.7	77.9	73.6	29.8
Level of Service	D	C	B	D	F		E	F	C	E	E	C
Approach Delay (s)		25.2			168.7			80.7			72.2	
Approach LOS		C			F			F			E	

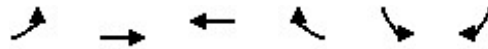
Intersection Summary		
HCM 2000 Control Delay	92.8	HCM 2000 Level of Service
HCM 2000 Volume to Capacity ratio	1.06	F
Actuated Cycle Length (s)	100.0	Sum of lost time (s)
Intersection Capacity Utilization	94.4%	22.6
Analysis Period (min)	15	ICU Level of Service
		F

c Critical Lane Group

Queues

5: Egan Drive & Riverside Drive

03/02/2022



Lane Group	EBL	EBT	WBT	WBR	SBL	SBR
Lane Group Flow (vph)	378	469	954	536	422	206
v/c Ratio	0.75	0.20	0.78	0.62	0.74	0.47
Control Delay	42.6	5.2	17.1	4.6	47.1	8.7
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	42.6	5.2	17.1	4.6	47.1	8.7
Queue Length 50th (ft)	217	44	72	5	132	0
Queue Length 95th (ft)	#329	74	m157	m0	174	58
Internal Link Dist (ft)		694	1356		707	
Turn Bay Length (ft)	350			900	225	145
Base Capacity (vph)	501	2403	1223	868	1026	434
Starvation Cap Reductn	0	0	0	0	0	0
Spillback Cap Reductn	0	0	0	0	0	0
Storage Cap Reductn	0	0	0	0	0	0
Reduced v/c Ratio	0.75	0.20	0.78	0.62	0.41	0.47

Intersection Summary

- # 95th percentile volume exceeds capacity, queue may be longer.  
Queue shown is maximum after two cycles.
- m Volume for 95th percentile queue is metered by upstream signal.



HCM Signalized Intersection Capacity Analysis  
5: Egan Drive & Riverside Drive

03/02/2022



Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations	↙	↕↕	↕↕	↘	↙↘	↘
Traffic Volume (vph)	363	450	916	515	405	198
Future Volume (vph)	363	450	916	515	405	198
Ideal Flow (vphpl)	1850	1850	1850	1850	1850	1850
Total Lost time (s)	5.3	5.8	5.8	5.8	5.3	5.3
Lane Util. Factor	1.00	0.95	0.95	1.00	0.97	1.00
Frbp, ped/bikes	1.00	1.00	1.00	0.97	1.00	1.00
Fllp, ped/bikes	1.00	1.00	1.00	1.00	1.00	1.00
Frt	1.00	1.00	1.00	0.85	1.00	0.85
Flt Protected	0.95	1.00	1.00	1.00	0.95	1.00
Satd. Flow (prot)	1674	3348	3348	1446	3343	1542
Flt Permitted	0.95	1.00	1.00	1.00	0.95	1.00
Satd. Flow (perm)	1674	3348	3348	1446	3343	1542
Peak-hour factor, PHF	0.96	0.96	0.96	0.96	0.96	0.96
Adj. Flow (vph)	378	469	954	536	422	206
RTOR Reduction (vph)	0	0	0	340	0	171
Lane Group Flow (vph)	378	469	954	196	422	35
Confl. Peds. (#/hr)	5			5	5	5
Heavy Vehicles (%)	5%	5%	5%	5%	2%	2%
Turn Type	Prot	NA	NA	Perm	Prot	Prot
Protected Phases	1	6	2		4	8
Permitted Phases				2		
Actuated Green, G (s)	29.9	71.8	36.6	36.6	17.1	17.1
Effective Green, g (s)	29.9	71.8	36.6	36.6	17.1	17.1
Actuated g/C Ratio	0.30	0.72	0.37	0.37	0.17	0.17
Clearance Time (s)	5.3	5.8	5.8	5.8	5.3	5.3
Vehicle Extension (s)	1.0	0.5	0.5	0.5	2.0	2.0
Lane Grp Cap (vph)	500	2403	1225	529	571	263
v/s Ratio Prot	c0.23	0.14	c0.28		c0.13	0.02
v/s Ratio Perm				0.14		
v/c Ratio	0.76	0.20	0.78	0.37	0.74	0.13
Uniform Delay, d1	31.7	4.6	28.1	23.3	39.3	35.2
Progression Factor	1.00	1.00	0.53	1.64	1.00	1.00
Incremental Delay, d2	5.7	0.2	0.5	0.2	4.3	0.1
Delay (s)	37.5	4.8	15.4	38.3	43.6	35.3
Level of Service	D	A	B	D	D	D
Approach Delay (s)		19.4	23.6		40.9	
Approach LOS		B	C		D	

Intersection Summary			
HCM 2000 Control Delay	26.1	HCM 2000 Level of Service	C
HCM 2000 Volume to Capacity ratio	0.78		
Actuated Cycle Length (s)	100.0	Sum of lost time (s)	18.4
Intersection Capacity Utilization	72.2%	ICU Level of Service	C
Analysis Period (min)	15		

c Critical Lane Group

HCM 2010 TWSC  
6: Riverside & EBR Vintage

03/02/2022

**Intersection**

Int Delay, s/veh 4.1

**Movement** EBL EBR NBL NBT SBT SBR

Lane Configurations		↗			↖	
Traffic Vol, veh/h	0	180	0	0	421	0
Future Vol, veh/h	0	180	0	0	421	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Free	Free
RT Channelized	-	Yield	-	None	-	None
Storage Length	-	0	-	-	-	-
Veh in Median Storage, #	0	-	-	-	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	93	93	93	93	93	93
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	194	0	0	453	0

**Major/Minor** Minor2 Major2

Conflicting Flow All	-	453	-	0
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Critical Hdwy	-	6.22	-	-
Critical Hdwy Stg 1	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-
Follow-up Hdwy	-	3.318	-	-
Pot Cap-1 Maneuver	0	607	-	0
Stage 1	0	-	-	0
Stage 2	0	-	-	0
Platoon blocked, %			-	-
Mov Cap-1 Maneuver	-	607	-	-
Mov Cap-2 Maneuver	-	-	-	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-

**Approach** EB SB

HCM Control Delay, s	13.7	0
HCM LOS	B	

**Minor Lane/Major Mvmt** EBLn1 SBT

Capacity (veh/h)	607	-
HCM Lane V/C Ratio	0.319	-
HCM Control Delay (s)	13.7	-
HCM Lane LOS	B	-
HCM 95th %tile Q(veh)	1.4	-

HCM 2010 TWSC  
7: Riverside & WBR Mendenhall Mall

03/02/2022

**Intersection**

Int Delay, s/veh 6.1

**Movement** WBL WBR NBT NBR SBL SBT

Lane Configurations		↗	↖			
Traffic Vol, veh/h	0	212	758	0	0	0
Future Vol, veh/h	0	212	758	0	0	0
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Stop	Stop
RT Channelized	-	Yield	-	None	-	None
Storage Length	-	0	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	-
Grade, %	0	-	0	-	-	0
Peak Hour Factor	93	93	93	93	93	93
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	228	815	0	0	0

**Major/Minor** Minor1 Major1

Conflicting Flow All	-	815	0	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Critical Hdwy	-	6.22	-	-
Critical Hdwy Stg 1	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-
Follow-up Hdwy	-	3.318	-	-
Pot Cap-1 Maneuver	0	377	-	0
Stage 1	0	-	-	0
Stage 2	0	-	-	0
Platoon blocked, %			-	
Mov Cap-1 Maneuver	-	377	-	-
Mov Cap-2 Maneuver	-	-	-	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-

**Approach** WB NB

HCM Control Delay, s	28.1	0
HCM LOS	D	

**Minor Lane/Major Mvmt** NBTWBLn1

Capacity (veh/h)	-	377
HCM Lane V/C Ratio	-	0.605
HCM Control Delay (s)	-	28.1
HCM Lane LOS	-	D
HCM 95th %tile Q(veh)	-	3.8

HCM 2010 TWSC  
 8: NBR Riverside & Vintage/Mendenhall Mall

03/02/2022

Intersection						
Int Delay, s/veh	3.4					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑					↑
Traffic Vol, veh/h	266	0	0	0	0	122
Future Vol, veh/h	266	0	0	0	0	122
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Stop	Stop	Stop	Stop
RT Channelized	-	None	-	None	-	Yield
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	0	-	-	-	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	93	93	93	93	93	93
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	286	0	0	0	0	131

Major/Minor	Major1	Minor1
Conflicting Flow All	0	- 286
Stage 1	-	-
Stage 2	-	-
Critical Hdwy	-	- 6.22
Critical Hdwy Stg 1	-	-
Critical Hdwy Stg 2	-	-
Follow-up Hdwy	-	- 3.318
Pot Cap-1 Maneuver	- 0	0 753
Stage 1	- 0	0 -
Stage 2	- 0	0 -
Platoon blocked, %	-	
Mov Cap-1 Maneuver	-	- 753
Mov Cap-2 Maneuver	-	-
Stage 1	-	-
Stage 2	-	-

Approach	EB	NB
HCM Control Delay, s	0	10.8
HCM LOS		B

Minor Lane/Major Mvmt	NBLn1	EBT
Capacity (veh/h)	753	-
HCM Lane V/C Ratio	0.174	-
HCM Control Delay (s)	10.8	-
HCM Lane LOS	B	-
HCM 95th %tile Q(veh)	0.6	-

HCM 2010 TWSC  
9: Vintage/Mendenhall Mall & SBR Riverside

03/02/2022

**Intersection**

Int Delay, s/veh 2.7

**Movement** EBL EBT WBT WBR SBL SBR

Lane Configurations			↑			↑
Traffic Vol, veh/h	0	0	455	0	0	119
Future Vol, veh/h	0	0	455	0	0	119
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	Yield
Storage Length	-	-	-	-	-	0
Veh in Median Storage, #	-	-	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	93	93	93	93	93	93
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	0	489	0	0	128

**Major/Minor** Major2 Minor2

Conflicting Flow All	-	0	-	489
Stage 1	-	-	-	-
Stage 2	-	-	-	-
Critical Hdwy	-	-	-	6.22
Critical Hdwy Stg 1	-	-	-	-
Critical Hdwy Stg 2	-	-	-	-
Follow-up Hdwy	-	-	-	3.318
Pot Cap-1 Maneuver	-	0	0	579
Stage 1	-	0	0	-
Stage 2	-	0	0	-
Platoon blocked, %	-	-	-	-
Mov Cap-1 Maneuver	-	-	-	579
Mov Cap-2 Maneuver	-	-	-	-
Stage 1	-	-	-	-
Stage 2	-	-	-	-

**Approach** WB SB

HCM Control Delay, s	0	13
HCM LOS		B

**Minor Lane/Major Mvmt** WBT SBLn1

Capacity (veh/h)	-	579
HCM Lane V/C Ratio	-	0.221
HCM Control Delay (s)	-	13
HCM Lane LOS	-	B
HCM 95th %tile Q(veh)	-	0.8

**Appendix F- Meeting Summary; CBJ and Kinney Engineering,  
LLC; July 27, 2021**





# MEETING SUMMARY

**Project:** Juneau Medical Office Building

**Meeting Purpose:** Traffic Impact Analysis (TIA) Requirements

**Date/Time:** 10:30, July 27, 2021

**Location:** Zoom

**Attendees:** **City & Borough of Juneau, AK (CBJ):** Allison Eddins Planner II Community Development Department  
**Kinney Engineering, LLC on behalf of JL Properties, Inc (Developer):** Randy Kinney

Topic Discussed:	Is TIA required?
<b>CBJ Requirements</b>	<ul style="list-style-type: none"> <li>• Discussions with DOT&amp;PF (David Epstein) indicate that CBJ is primary approving agency for a TIA (if required) since proposed site fronts Vintage Boulevard.</li> <li>• In this meeting, Ms. Eddins indicates CBJ requires TIAs as part of a Conditional Use Permit or Building Permit if traffic thresholds are exceeded. TIA will be reviewed by CBJ staff, but approved by CBJ Planning Commission.</li> <li>• DOT&amp;PF review will be coordinated by CBJ.</li> <li>• CBJ Code of Ordinances, 49.40.300 Applicability. (a) A traffic impact analysis (TIA) shall be required as follows:               <ul style="list-style-type: none"> <li>(1) A development projected to generate 500 or more average daily trips (ADT) shall be required to have a traffic impact analysis.</li> <li>(2) A development projected to generate fewer than 250 ADT shall not be required to have a traffic impact analysis.</li> <li>(3) A development projected to generate more than 250 ADT but fewer than 500 ADT shall be required to have a traffic impact analysis if the Community Development Department Director determines that an analysis is necessary based on the type of development, its location, the likelihood of future expansion, and other factors found relevant by the director.</li> </ul> </li> </ul>
<b>Proposed Site</b>	<ul style="list-style-type: none"> <li>• Legal Description: Vintage IV TR A2</li> <li>• Tax ID: 5B1601430017</li> <li>• 2.9 Acres, 127,336 SF</li> <li>• Zoning: (LC) Light Commercial</li> <li>• Street Fronting: Vintage Boulevard, CBJ owned, LOCATION below.</li> </ul>



<b>Development</b>	3-level, 41,000 SF Medical Office Building (source: Sparks Design LLC plans)
<b>Trip Generation</b>	<ul style="list-style-type: none"> <li>• ITE Trip Generation Manual Land Use: LU 720 (Medical-Dental Offices, see attached).</li> <li>• Choose trip estimation method for site generated weekday ADT (a. Line at Cluster, b. Average Rate, c. Fitted Curve Equation) per Trip Generation Manual, 3<sup>rd</sup> Ed: Result is Fitted Curve Equation.</li> <li>• Weekday ADT <math>T=38.42(X)-87.62</math>; for <math>X=41</math>, <math>T=1,488</math> trips in and out per day.</li> <li>• 50%/50% entering/exiting.</li> </ul>
<b>Conclusion</b>	<ul style="list-style-type: none"> <li>• TIA will be required.</li> </ul>
<b>Topic Discussed:</b>	<b>Design Year/Design Hour</b>
<b>CBJ Requirements</b>	<ul style="list-style-type: none"> <li>• CBJ Code of Ordinances, 49.40.330 Traffic impact mitigation (a) (1) Summarized as:             <ul style="list-style-type: none"> <li>◦ Design Year: Opening year and at full build out (if phased).</li> </ul> </li> <li>• Design Hour: Not discussed at meeting, but would likely be the morning and evening commuting peak hours.</li> </ul>
<b>Topic Discussed:</b>	<b>Study Area</b>
<b>CBJ Requirements</b>	<ul style="list-style-type: none"> <li>• CBJ Code of Ordinances, 49.40.305 Traffic impact analysis (TIA) requirements.             <ul style="list-style-type: none"> <li>(a) <i>The study area for the TIA shall be that area in which it is anticipated that the proposed development will increase ADT by five percent or more.</i></li> </ul> </li> <li>• CBJ Code of Ordinances, 49.40.305 (c)</li> </ul>

(1) Intersections and segments of roadways where the ADT on any approach to an intersection is anticipated to increase by five percent or more due to the proposed development.....

**Study Area Methodology**

- Not discussed at the meeting.
- Propose to distribute traffic to/from site Juneau based on AADT weight. DOT&PF has an AADT map of area, <https://akdot.maps.arcgis.com/apps/mapviewer/index.html?webmap=7c1e1029fdb64d7a86449d55ef05e21c>.
- We will cordon that map as shown and assign site inbound and outbound trips according to AADT weight were cordon line crosses major links.

$\Sigma$ AADT at cordon crossing = 15752 + 8619 + 17049 + 26134 + 9334 + 2085 = 78973

**Topic Discussed:** LOS Standards

**CBJ Requirements**

- CBJ Code of Ordinances, 49.40.310 Traffic; minimum standards.
  - (a) The minimum acceptable LOS for a roadway segment or intersection within the area affected by the development, on the projected opening date of the development, or full build out of the development, is LOS D.
- CBJ Code of Ordinances, 49.40.340 Traffic impact mitigation.
  - (a) Except as provided in 49.40.340, an applicant shall make improvements to a roadway or intersection to achieve or maintain an acceptable LOS if a roadway or intersection has an:
    - (1) LOS D without traffic generated by the development; and would drop below LOS D with traffic generated by the development at the opening date of the development or full build out;
    - (2) If a roadway has an LOS below D without traffic generated by the development at the opening date of the development; or
    - (3) If the intersection or roadway segment has a pattern of accidents resulting in personal injuries, and the development would aggravate this accident pattern, then mitigation shall be required regardless of the LOS.



	<ul style="list-style-type: none"> <li>• CBJ Code of Ordinances, 49.40.340 Mitigation waiver. Section indicates conditions in which the requirements above may be waived.</li> </ul>
<b>Topic Discussed:</b>	<b>Material Provided by Ms. Eddins</b>
	<ul style="list-style-type: none"> <li>• CBJ applicable Codes</li> <li>• Example TIA</li> </ul>
<b>Additional Topic:</b>	Traffic Data for Intersections .
<b>Discussion</b>	<ul style="list-style-type: none"> <li>• Because of pandemic, we will use the existing counts published in the Juneau Egan Drive and Riverside Intersection Improvements Traffic Analysis Report. Counts were conducted between 2012 and 2016. They will be factored to 2019 volumes using link AADTs for the respective years, and 2019 AADT. The volumes will then be expanded to the design year using 0.25% per year (DOT&amp;PF recommendations).</li> <li>• Observed PM volumes are:</li> </ul> <ul style="list-style-type: none"> <li>• Observed AM volumes are:</li> </ul>

Attachments

# Land Use: 720 Medical-Dental Office Building

## Description

A medical-dental office building is a facility that provides diagnoses and outpatient care on a routine basis but is unable to provide prolonged in-house medical and surgical care. One or more private physicians or dentists generally operate this type of facility. Clinic (Land Use 630) is a related use.

## Additional Data

Time-of-day distribution data for this land use for a weekday, Saturday, and Sunday are presented in Appendix A. For the 19 general urban/suburban sites with data, the overall highest vehicle volumes during the AM and PM on a weekday were counted between 9:30 and 10:30 a.m. and 2:15 and 3:15 p.m., respectively.

The sites were surveyed in the 1980s, the 1990s, the 2000s, and the 2010s in Alberta (CAN), California, Connecticut, Kentucky, Maryland, Minnesota, New Jersey, New York, Ohio, Oregon, Pennsylvania, South Dakota, Texas, Virginia, Washington, and Wisconsin.

## Source Numbers

104, 109, 120, 157, 184, 209, 211, 253, 287, 294, 295, 304, 357, 384, 404, 407, 423, 444, 509, 601, 715, 867, 879, 901, 902, 908, 959, 972

**Query** **Filter**

DATA SOURCE:  
Trip Gen Manual, 10th Ed

SEARCH BY LAND USE CODE:  
720

LAND USE GROUP:  
(700-799) Office

LAND USE:  
720 - Medical-Dental Office Building

LAND USE SUBCATEGORY:  
All Sites

INDEPENDENT VARIABLE (IV):  
1000 Sq. Ft. GFA

TIME PERIOD:  
Weekday

SETTING/LOCATION:  
General Urban/Suburban

TRIP TYPE:  
Vehicle

ENTER IV VALUE TO CALCULATE TRIPS:  
41 Calculate

**Data Plot and Equation**

X = 1000 Sq. Ft. GFA

Reset Zoom Restore

X Study Site      — Fitted Curve      - - - Average Rate

Use the mouse wheel to Zoom Out or Zoom In.  
Hover the mouse pointer on data points to view X and T values.

**DATA STATISTICS**

Land Use:  
Medical-Dental Office Building (720) [Click for more details](#)

Independent Variable:  
1000 Sq. Ft. GFA

Time Period:  
Weekday

Setting/Location:  
General Urban/Suburban

Trip Type:  
Vehicle

Number of Studies:  
28

Avg. 1000 Sq. Ft. GFA:  
24

Average Rate:  
34.80

Range of Rates:  
9.14 - 100.75

Standard Deviation:  
9.79

Fitted Curve Equation:  
 $T = 38.42(X) - 87.62$

R<sup>2</sup>:  
0.95

Directional Distribution:  
50% entering, 50% exiting

Calculated Trip Ends:  
Average Rate: 1427 (Total), 713 (Entry), 714 (Exit)  
Fitted Curve: 1488 (Total), 744 (Entry), 744 (Exit)

## **Appendix G- Review Meeting Summary**



April 7, 2022

PND No. 222016

Irene Gallion  
Senior Planner  
City and Borough of Juneau Community Development Dept.  
230 S. Franklin St. Juneau, AK 99801

Re: SEARHC Vintage Park Medical Office Building, Conditional Use Permit Application – Additional Information on Traffic Impact Analysis

Dear Ms. Gallion,

On Thursday, March 31, 2022, PND submitted to CBJ a conditional use permit application package for the subject project. The permit package included a draft Traffic Impact Analysis, (TIA) developed by Kinney Engineering, (KE). It was noted in the application Project Narrative that the TIA is in draft form due to ongoing coordination with AKDOT. Previously, CBJ requested we review the draft with AKDOT prior to submitting the final TIA. On April 5th, KE and PND met with AKDOT stakeholders to review the TIA and discuss the results and potential traffic impact mitigations that may be warranted. You requested PND summarize the meeting discussion and provide the summary to CBJ as additional information to the CUP application.

PND and KE met with AKDOT on April 5, 2022. In attendance were myself, representing PND and the Owner, Randy Kinney and Jeanne Bowie from KE, and Nathan Purves, Steven Thater and Chrissy McNally from AKDOT.

The TIA was discussed with only minor comments and no objections to the method or results of the analysis. AKDOT voiced no objections to the proposed mitigations and indicated they would not request additional intersection improvements as a result of the proposed project. A main point of discussion was the intersection of Egan Drive and Mendenhall Loop Road. As noted in the TIA, this is the only intersection which may warrant impact mitigation in accordance with the CBJ municipal code, however, the level of service (LOS) under existing conditions is within the CBJ threshold for mitigation and the proposed traffic impact is minimal; there is no LOS change for the intersection with the additional proposed site traffic. As described in the TIA Section 8.1. *“Without site traffic, intersection AM average delay per vehicle is 82 seconds with a LOS F and the PM average delay per vehicle is 65 seconds with LOS E. Adding site traffic only increases delay by 1 second per vehicle in both AM and PM peak hours with no changes in LOS.”*

Mr. Thater observed that under AKDOT criteria as specified in 17 AAC 10.070, Traffic Impact Analysis, mitigation of impacts from the proposed SEARHC Medical Facility would likely not be required as the change in operational performance resulting from the site is minimal and appears to be beneath the AKDOT threshold triggering code required mitigation. He went on to make the additional points that, the existing traffic signals currently employ adaptive software technology, which will mitigate potential impacts proactively and the Department regularly reviews and updates signalization parameters, further minimizing project impacts.

Additional comments, included support for the proposed pedestrian mitigations, including the addition of the perimeter sidewalk along Vintage Boulevard.

During our telephone conversation on April 5<sup>th</sup>, you requested that the TIA remain in draft form until CBJ Engineering has reviewed the CUP application and draft TIA, then update and provide the final TIA prior to the Planning Commission meeting, yet to be scheduled. As requested, the final TIA is pending CBJ internal review.

Please contact me if you have any questions or require further information.

Sincerely,  
PND Engineers, Inc. | Juneau Office



Tyler Bradshaw, P.E. | Senior Engineer

- cc:
- Ed Zernia, SEARHC Director of Construction, Facilities Admin
- Deanna Wlad, AIA, Architect, Spark Design
- Randy Kinney, PE, Kinney Engineering
- Dick Somerville, PE, PND Engineers
- Steven Thater, Alaska Department of Transportation
- Nathan Purves, Alaska Department of Transportation
- Chrissy McNally, Alaska Department of Transportation



(907) 586-0715  
 CDD\_Admin@juneau.gov  
 www.juneau.org/community-development  
 155 Heritage Way • Juneau, AK 99801

**COMMUNITY DEVELOPMENT DEPARTMENT - REQUEST FOR AGENCY COMMENT**

**DEPARTMENT:**

**STAFF PERSON/TITLE:**

**DATE:**

**APPLICANT:** Dawson Construction on behalf of SEARHC

**TYPE OF APPLICATION:** Conditional Use Permit

**PROJECT DESCRIPTION:**

The applicant proposes to build a three-story, 19,635 sq. ft. dental clinic in the Vintage Park area between Safeway and First Bank.

**LEGAL DESCRIPTION:** VINTAGE III LT BL 1

**PARCEL NUMBER(S):** 5B1601430016

**PHYSICAL ADDRESS:** 3063 Vintage Blvd.

**SPECIFIC QUESTIONS FROM PLANNER:**

The site is not large enough to accommodate the minimum parking requirement for a facility of this size. The applicant intends to use Title 49 provisions for a shared parking agreement with SEARHC workforce housing located across Vintage Blvd off Postal Way (behind True North). Are there any concerns about pedestrian safety with clients potentially having to cross Vintage Blvd at the Postal Way intersection?

**AGENCY COMMENTS:**

**AGENCY COMMENTS (CONTINUED):**

**Ilsa Lund**

---

**From:** Theresa Ross  
**Sent:** Tuesday, February 25, 2025 6:36 AM  
**To:** Ilsa Lund  
**Subject:** Re: Please comment- USE25-07 SEARHC dental clinic in Vintage Park

Good morning,

Just so you are in the loop, I am trying to work with the new program we are going to, Tyler Tech, to find a way to allow deferred submittals for fire systems. We are not there yet but I am pretty confident we will be able to find a way to track them so the applicants can submit projects without full fire systems plans like past practice. My concern is that projects are being approved with a deferred submittal and there is no way to track that those system come in for review and approval before installation.

I am just anticipating that concerns to come up with this project. You can send them my way if they have any questions.

Thank you,

Theresa Ross, Fire Marshal  
Capital City Fire Rescue  
820 Glacier Avenue  
Juneau AK 99801  
907-586-5322 ext. 4323  
<https://www.juneau.org/fire>



---

**From:** Ilsa Lund <Ilsa.Lund@juneau.gov>  
**Sent:** Monday, February 24, 2025 2:44 PM  
**To:** Theresa Ross <Theresa.Ross@juneau.gov>  
**Subject:** RE: Please comment- USE25-07 SEARHC dental clinic in Vintage Park

Thanks for your input from afar!

*Gunalchéesh!*

**Ilsa Lund | Planner I**

[Community Development Department](#) | City & Borough of Juneau, AK  
Location: 230 S. Franklin Street, 4<sup>th</sup> Floor Marine View Building  
Office: 907.586.0753 ext. 4128



*Fostering excellence in development for this generation and the next.*

---

**From:** Theresa Ross <Theresa.Ross@juneau.gov>  
**Sent:** Monday, February 24, 2025 1:43 PM  
**To:** Ilsa Lund <Ilsa.Lund@juneau.gov>  
**Subject:** Re: Please comment- USE25-07 SEARHC dental clinic in Vintage Park

Awesome. That worries me, we run over to the Riverview senior center frequently and the thought of people darting out across the road is a bit unnerving. Not to mention just regular traffic through that area.

Theresa Ross, Fire Marshal  
Capital City Fire Rescue  
820 Glacier Avenue  
Juneau AK 99801  
907-586-5322 ext. 4323  
<https://www.juneau.org/fire>



---

**From:** Ilsa Lund <Ilsa.Lund@juneau.gov>  
**Sent:** Monday, February 24, 2025 2:36 PM  
**To:** Theresa Ross <Theresa.Ross@juneau.gov>  
**Subject:** RE: Please comment- USE25-07 SEARHC dental clinic in Vintage Park

Hi Theresa!

I was wondering the same thing. There is nothing in the plans submitted that indicates they are. It will likely be a recommended condition of the conditional use permit.

*Gunalchéesh!*

**Ilsa Lund** | Planner I

Community Development Department | City & Borough of Juneau, AK

Location: 230 S. Franklin Street, 4<sup>th</sup> Floor Marine View Building

Office: 907.586.0753 ext. 4128





*Fostering excellence in development for this generation and the next.*

---

**From:** Theresa Ross <[Theresa.Ross@juneau.gov](mailto:Theresa.Ross@juneau.gov)>  
**Sent:** Monday, February 24, 2025 1:28 PM  
**To:** Ilsa Lund <[Ilsa.Lund@juneau.gov](mailto:Ilsa.Lund@juneau.gov)>  
**Subject:** Re: Please comment- USE25-07 SEARHC dental clinic in Vintage Park

Are they adding a crosswalk? I am just curious, it is really dark right there during winter months and the area is pretty busy.

Theresa Ross, Fire Marshal  
Capital City Fire Rescue  
820 Glacier Avenue  
Juneau AK 99801  
907-586-5322 ext. 4323  
<https://www.juneau.org/fire>



---

**From:** Ilsa Lund <[Ilsa.Lund@juneau.gov](mailto:Ilsa.Lund@juneau.gov)>  
**Sent:** Monday, February 24, 2025 12:59 PM  
**Subject:** Please comment- USE25-07 SEARHC dental clinic in Vintage Park



Hello CBJ Team,  
We have received an application from Dawson Construction to build a three-story, 19,635 sq. ft. SEARHC dental clinic between Safeway and First Bank in the Vintage Park area. As part of the review

process, we are circulating the application amongst CBJ departments for input that will be presented to the Planning Commission for review.

Attached is the application. Later this week, you can also find information at the short-term planning web site: <https://juneau.org/community-development/short-term-projects>

We have the case scheduled for the Planning Commission meeting on March 25th. If you could provide feedback by **March 10, 2025**, that would be very helpful. I've attached an Agency Comment Form for your use.

Please let me know if you have any questions.

Warm regards,

**Ilsa Lund** | Planner I

Community Development Department | City & Borough of Juneau, AK

Location: 230 S. Franklin Street, 4<sup>th</sup> Floor Marine View Building

Office: 907.586.0753 ext. 4128

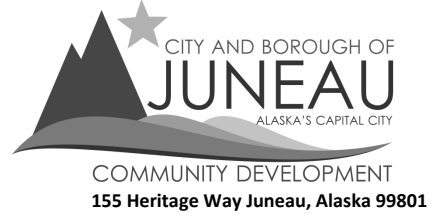
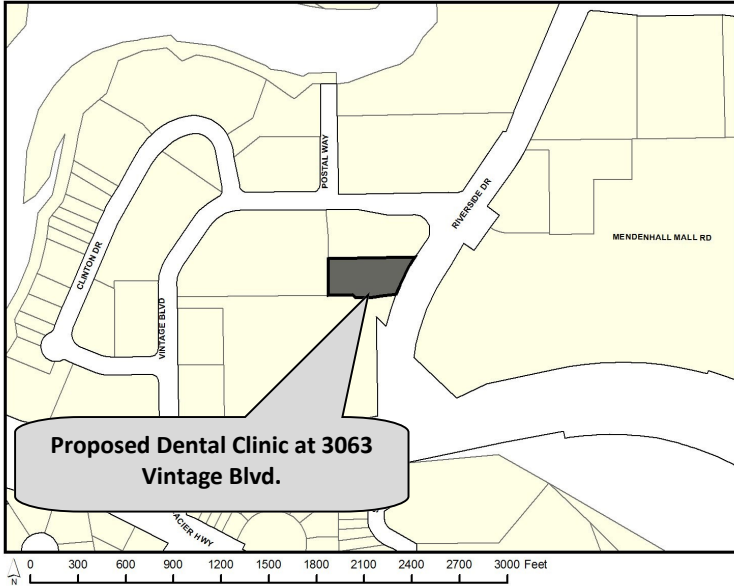


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# Invitation to Comment

On a proposal to be heard by the CBJ Planning Commission

*Your Community, Your Voice*



TO

**A Conditional Use Permit application** has been submitted for consideration and public hearing by the Planning Commission for **a three-story dental clinic at 3063 Vintage Blvd.** in a **Light Commercial zoning district.**

### PROJECT INFORMATION:

Project Information can be found at:

<https://juneau.org/community-development/short-term-projects>

### PLANNING COMMISSION DOCUMENTS:

Staff Report expected to be posted Monday, March 17, 2025 at

<https://juneau.org/community-development/planning-commission>

Find hearing results, meeting minutes, and more here, as well.

<b>Now through March 6</b>	<b>March 7— noon, March 21</b>	<b>HEARING DATE &amp; TIME: 6:00 pm, March 25, 2025</b>	<b>March 26</b>
Comments received during this period will be sent to the Planner, <b>Ilsa Lund</b> , to be included as an attachment in the staff report.	Comments received during this period will be sent to Commissioners to read in preparation for the hearing.	This meeting will be held in person and by remote participation. For remote participation: join the Webinar by visiting <a href="https://juneau.zoom.us/j/85421744892">https://juneau.zoom.us/j/85421744892</a> and use the Webinar ID: 854 2174 4892 OR join by telephone, calling: 1-253-215-8782 and enter the Webinar ID (above).  You may also participate in person in City Hall Assembly Chambers, 155 Heritage Way Juneau, Alaska.	The results of the hearing will be posted online.

### FOR DETAILS OR QUESTIONS,

Phone: (907)586-0753 ext. 4128

Email: [pc\\_comments@juneau.gov](mailto:pc_comments@juneau.gov) or [Ilsa.Lund@juneau.gov](mailto:Ilsa.Lund@juneau.gov)

Mail: Community Development, 155 Heritage Way, Juneau AK 99801

Case No.: **USE2025 0007**

Parcel No.: **5B1601430016**

CBJ Parcel Viewer: <http://epv.juneau.org>





Attachment H - Public Notice Sign Photo









Attachment I - Site Photos













Attachment I - Site Photos









Attachment I - Site Photos



# Additional Materials

## Regular Planning Commission Meeting

Assembly Chambers  
6:00pm  
Meeting Date: 3/25/2025

---

**1. AME2025 0002:**

- a. Memo: Jill Lawhorne, with attached Comprehensive Plan Advisory Committee applications





(907) 586-0715  
 CDD\_Admin@juneau.gov  
 www.juneau.org/community-development  
 155 Heritage Way • Juneau, AK 99801

**Date:** March 17, 2025

**From:** Jill Lawhorne, AICP, Director  
 Community Development Department

**To:** Mandy Cole, Chair, Planning Commission

**Case Number:** AME2025-0002 Comprehensive Plan Rewrite

**RE: Appointments to Comprehensive Plan Advisory Committee**

The Comprehensive Plan Rewrite project has officially kicked off. One of the first tasks is to assemble an Advisory Committee (AC) to be a sounding board for key decisions and policy recommendations for the Plan update, on topics such as land use, transportation, infrastructure, housing, utilities, and recreation.

Members will attend public events and every-other-month Committee meetings over the next two plus years. Members will act as champions of the Plan in their community, organization, or area of influence to help build community involvement and consensus.

The AC will function more as a think tank than a traditional steering committee, providing guidance and feedback to project staff. The AC is envisioned as a diverse body reflective of Juneau. Like Juneau, the AC should include people from multiple demographics, occupations and areas of expertise, and lived experiences.

The City Clerk’s Office received 27 applications to fill the 21 spots available. Staff and project consultant Cascadia Partners reviewed the applications and recommends the following people be appointed:

- |                       |                       |
|-----------------------|-----------------------|
| 1. Amara Enciso       | 14. Lorraine DeAsis   |
| 2. Bailey Gent        | 15. Meghan Tabacek    |
| 3. Charles Harrington | 16. Michael Heumann   |
| 4. Craig Dahl         | 17. Nimmy Philips     |
| 5. Deborah Johnston   | 18. Tari Stage-Harvey |
| 6. Franklyn Delaney   | 19. Tonja Moser       |
| 7. Griffin Plush      | 20. Travis Eckhoff    |
| 8. Judith Ament       | 21. Tyson Fick        |
| 9. Katie Thomas       |                       |
| 10. Kelly Sorensen    |                       |
| 11. Lia Domke         |                       |
| 12. Logan Miller      |                       |
| 13. Lori Sowa         |                       |



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[Home](#) > [CBJ Board Application](#) > [Webform results](#) > CBJ Board Application

Submission information

Form: [CBJ Board Application](#) [1]  
Submitted by Visitor (not verified)  
Fri, 03/07/2025 - 3:31pm  
65.74.54.33

**First Name**  
Charles

**Last Name**  
Herrington

**Residence Address**  
[REDACTED]

**Mailing Address**  
[REDACTED]

**Primary Phone Number**  
[REDACTED]

**Secondary Phone Number**

**Email**  
[REDACTED]

**Board, Commission, Committee**  
Other Board

**Current of Prior Experience on CBJ Boards/Commissions/Committees**  
I have never served on a CBJ Board. I served for two years on the Downtown Business Association board in 2017 and 2018. As an Eaglecrest employee (September 2018 and October 2021), I attended most Board meetings.

**Reasons for Applying**  
Comp Plan AC  
Note: attached resume doesn't include my employment at Southeast Conference from March 2024 to present.

I believe the city needs to work deliberately to curb population decline and maintain a bright economic future. The Alaska Department of Labor projects Juneau to lose another 1,350 people by 2035 with the average resident aging from 39 to 43 years old. I would like to contribute to this planning effort and turn this trend around. I think it's essential that we invest infrastructure that reduces our cost living and amenities that improve our quality of life. We need to make Juneau a more desirable place to live and raise a family. Additionally, Juneau's economy and workforce complexion is changing. We seeing an in flow of summer seasonal jobs and out flow of good paying, year round jobs, leading to less money circulating spent in our local businesses and trades and fewer kids in the school district. I'd like to better understand how growing

summer seasonal economy impacts our year-round resident experience, particularly housing. As our only growing economic sector, I think tourism is a good thing for Juneau, but I want to find solutions that work for everyone and minimize economic leakage.

### Qualifications

I've worked in business and marketing for most of my career, largely in the outdoor recreation and entertainment sectors. Last year, I pivoted to work in economic development. I spent some time at JEDC and now work at Southeast Conference, the regional economic development organization for Southeast Alaska. Given my experience with business development, project management, marketing, and outdoor recreation, I think I can contribute in a variety of ways to advance the Planning Commission's goals.

### Civic Activities, Memberships or Non-profits Involved With

Southeast Conference  
Sustainable Southeast Partnership  
Alaska Mariculture Cluster

### Working with a Group

I believe a successful group must have a strong foundation of mutual respect and kindness more than anything else. You need to have empathy to work through conflicting perspectives. I try to approach group work with a solutions-oriented mindset. A lot of my thoughts on teamwork are actually informed by being a lifelong basketball player. It feels sappy to say it out loud, but a team can only succeed when it's working together.

### Meeting Schedule and Attendance

Yes

### Date of Birth (Optional)

[REDACTED]

### Ethnicity (Optional)

White

### Gender (Optional)

Male

### Acknowledgment/Certification

Charles B Herrington

### Resume, Education, etc. (Optional)

[herrington\\_resume\\_2.22.24.pdf](#) [2]

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- [Dashboard](#)

[Municode - Connecting You and Your Community](#)

**Source URL:** <https://juneau-ak.municodemeetings.com/node/791/submission/163>

### Links

[1] <https://juneau-ak.municodemeetings.com/bc/application> [2] [https://juneau-ak.municodemeetings.com/system/files/webform/herrington\\_resume\\_2.22.24.pdf](https://juneau-ak.municodemeetings.com/system/files/webform/herrington_resume_2.22.24.pdf)



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[Home](#) > [CBJ Board Application](#) > [Webform results](#) > CBJ Board Application

Submission information

Form: [CBJ Board Application](#) [1]  
Submitted by Visitor (not verified)  
Thu, 03/06/2025 - 9:16pm  
107.77.205.144

**First Name**  
Daniel

**Last Name**  
Parks

**Residence Address**  
[Redacted]

**Mailing Address**  
Same

**Primary Phone Number**  
[Redacted]

**Secondary Phone Number**  
[Redacted]

**Email**  
[Redacted]

**Board, Commission, Committee**  
Other Board

**Current of Prior Experience on CBJ Boards/Commissions/Committees**  
None

**Reasons for Applying**  
Comp Plan AC. I was referred to apply by Dianna Robinson after presenting on Food Waste to the Sustainability Commission. After 3 years of advocacy work in SEAK I'd simply like to offer my perspective, especially regarding Food Security. SEAK has uniquely tenuous and challenging logistics around its food supply and I'd like to help policy makers understand how that impacts our diverse populations.

**Qualifications**  
I have served as the General Manager of the Southeast Alaska Food Bank for the past year. Prior to that I spent 2 years working in Disability advocacy for Southeast Alaska Independent Living (SAIL). Before becoming involved in SEAK Social Services, I moved to AK in 2019 to manage guides for a tourism company. I have volunteered a fair bit with Mix in that time. I studied English in college and trained to be an English teacher before taking an alternative career I

am mostly interested in contributing to the AC regarding my current work at the SEAFB, but think I have a wide variety of relevant experience.

Section R, Item 3.

**Civic Activities, Memberships or Non-profits Involved With**

SEAFB: Spring 2024-current  
SAIL: Winter 2021-current  
Trail Mix: 2022-2024

Through my work at SEAFB & SAIL, I'm in collaboration/communication with many non-profit organizations like St. Vincent's, JAMHI, Juneau Housing First, Juneau Community Foundation, Catholic Community Services etc.

**Working with a Group**

I have managed staff of roughly 25 and many volunteers. I currently answer to a Board of 11. My approach is to recognize that each member of the group has their own individual interests and perspective and that each is equally worthy of consideration. Sometimes those are at conflict with my own, so I approach each group looking for collaboration at best or compromise at worst. I try to look through the noise and bluster at the surface level of any discussion or conflict to see what's at stake and what's actually important, then weigh it against what I think is most important. Then, the hope is to bring clarity to what's vital.

**Meeting Schedule and Attendance**

I can be available for all meetings

**Date of Birth (Optional)**

[Redacted]

**Ethnicity (Optional)**

Caucasian

**Gender (Optional)**

Male

**Acknowledgment/Certification**

Daniel Parks

**Resume, Education, etc. (Optional)**

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**Links**

[1] <https://juneau-ak.municodemeetings.com/bc/application>





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Submission information

Form: [CBJ Board Application](#) [1]  
Submitted by Visitor (not verified)  
Sun, 03/09/2025 - 12:25am  
24.237.1.82

**First Name**  
Griffin

**Last Name**  
Plush

**Residence Address**  
[REDACTED]

**Mailing Address**  
same

**Primary Phone Number**  
[REDACTED]

**Secondary Phone Number**

**Email**  
[REDACTED]

**Board, Commission, Committee**  
Other Board

**Current of Prior Experience on CBJ Boards/Commissions/Committees**  
8/2024 - 1/2025 | Member, Juneau Commission on Sustainability  
1/2025 - Present | Co-Chair, Juneau Commission on Sustainability

**Reasons for Applying**  
Comp Plan AC

I am interested in supporting CBJ's planning work, especially as it relates to improving our housing market and long-term social, economic, and environmental sustainability (especially w/r/t greater energy independence).

**Qualifications**  
~2 years researching and communicating local government best practices & working on statewide energy planning with the Alaska Municipal League.  
~4 years experience chairing boards of The Alaska Center and The Alaska Center Education Fund

**Civic Activities, Memberships or Non-profits Involved With**

The Alaska Center, 2015 - Present

Alaska Municipal League (work), 2021 - Present

**Working with a Group**

I have spent lots of time facilitating meetings, including work on executive transitions and other complex topics, as leadership of The Alaska Center board. I've also spent a lot of time educating diverse audiences about climate and energy issues through my career.

**Meeting Schedule and Attendance**

Yes, re: Comp Plan website.

**Date of Birth (Optional)****Ethnicity (Optional)**

White

**Gender (Optional)**

Man

**Acknowledgment/Certification**

Griffin Plush

**Resume, Education, etc. (Optional)**

[plush\\_resume\\_3\\_2025.pdf](#) [2]

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**Links**

[1] <https://juneau-ak.municodemeetings.com/bc/application> [2] [https://juneau-ak.municodemeetings.com/system/files/webform/plush\\_resume\\_3\\_2025.pdf](https://juneau-ak.municodemeetings.com/system/files/webform/plush_resume_3_2025.pdf)



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Submission information

Form: [CBJ Board Application](#) [1]  
Submitted by Visitor (not verified)  
Fri, 03/07/2025 - 2:21pm  
65.74.30.66

**First Name**

Lori

**Last Name**

Sowa

**Residence Address**

[REDACTED]

**Mailing Address**

[REDACTED]

**Primary Phone Number**

[REDACTED]

**Secondary Phone Number**

**Email**

[REDACTED]

**Board, Commission, Committee**

Other Board

**Current of Prior Experience on CBJ Boards/Commissions/Committees**

Wetlands Review Board 2005-2007

**Reasons for Applying**

I'm applying for a seat on the Advisory Committee for the Comprehensive Plan update. The comp plan outlines the long-term land use and development guide for the community and therefore is the lens through which future projects and initiatives will be evaluated. I see this as a very challenging and worthwhile endeavor, with the goal being production of guidelines that are clear, user-friendly, and that reflect our community goals and vision.

**Qualifications**

I've enjoyed living and working in Juneau for over 20 years. I've raised my kids here, rented an apartment as a newcomer to town and later bought and sold homes, experienced childcare challenges firsthand as a young working mother, and have relied on community services for medical and emergency care. I very much appreciate the arts and culture that we are fortunate to have in such a small town and use our recreational facilities and trails often. My educational background and professional experience (described in more detail in my attached resume) includes environmental and engineering

the private sector, at the state, the university, and at the municipal level. I am well-versed in our local utilities including municipal water and sewer as well as our electrical system. I've managed design and construction projects through the public process and the decision-making that goes along with it.

### **Civic Activities, Memberships or Non-profits Involved With**

I am currently the treasurer for the non-profit organization Alaska Heat Smart (2022-present) and am a participating member of the Southeast Conference Energy Committee.

### **Working with a Group**

I've had significant experience working with groups with diverse perspectives through membership on various boards (including Alaska Heat Smart, Juneau Douglas Ice Association, Juneau Montessori School) and through my work at the CBJ as a project manager and utilities engineer. Very few issues that are important to the community are black and white, and understanding the views and opinions of others is a vital component of any planning effort that impacts the broader community. My approach to working with a group is to strive to actively listen and understand those around me and to maintain respectful dialogue while focusing on reaching the established goals.

### **Meeting Schedule and Attendance**

Yes

### **Date of Birth (Optional)**

### **Ethnicity (Optional)**

### **Gender (Optional)**

Female

### **Acknowledgment/Certification**

Agree

### **Resume, Education, etc. (Optional)**

[sowa\\_resume\\_2025.pdf](#) [2]

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### **Links**

[1] <https://juneau-ak.municodemeetings.com/bc/application> [2] [https://juneau-ak.municodemeetings.com/system/files/webform/sowa\\_resume\\_2025.pdf](https://juneau-ak.municodemeetings.com/system/files/webform/sowa_resume_2025.pdf)



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Submission information

Form: [CBJ Board Application](#) [1]  
Submitted by Visitor (not verified)  
Sun, 03/09/2025 - 6:20pm  
24.237.25.112

**First Name**  
Lorrai

**Last Name**  
DeAsis

**Residence Address**  
[REDACTED]

**Mailing Address**  
same

**Primary Phone** [REDACTED]

**Secondary Phone Number**

**Email**  
[REDACTED]

**Board, Commission, Committee**  
Building Code Advisory Committee

**Current of Prior Experience on CBJ Boards/Commissions/Committees**  
Title 49 Advisory Committee - current

**Reasons for Applying**  
There are many important and exciting projects and opportunities being discussed. It's important to be fully informed and have input in the future direction of the City.

**Qualifications**  
See attached resume. I have over 30 years of accounting experience, as well as management and construction management experience. I have worked for Tlingit Haida Regional Housing Authority, which is a non-profit Tribal organization, for 17 years. I have extensive grant experience and understand policies, regulations & statues as well as construction code.

**Civic Activities, Memberships or Non-profits Involved With**  
Tlingit Haida Regional Housing Authority 2008-current  
CBJ Title 49 Advisory Committee



**Working with a Group**

I have worked as a member of the executive management group for several organizations for 30 years. I have also served on several boards/committees, including Mt. Edgecumbe High School Advisory Committee, CBJ Title 49 Advisory Committee and have been serving as a board member for Alaska Heat Smart for 3 years.

I appreciate diverse and conflicting perspectives. I like to understand the issue and what is driving the problem or issue. I enjoy researching issues and solutions.

**Meeting Schedule and Attendance**

Yes

**Date of Birth (Optional)**

**Ethnicity (Optional)**

Juneau

**Gender (Optional)**

Female

**Acknowledgment/Certification**

Lorraine DeAsis

**Resume, Education, etc. (Optional)**

[lorraine\\_resume.pdf](#) [2]



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Submission information

Form: [CBJ Board Application](#) [1]  
Submitted by Visitor (not verified)  
Thu, 03/06/2025 - 10:15am  
66.230.81.101

**First Name**

Lucas

**Last Name**

Chambers

**Residence Address**

[REDACTED]

**Mailing Address**

Same

**Primary Phone Number**

[REDACTED]

**Secondary Phone Number**

**Email**

[REDACTED]

**Board, Commission, Committee**

Other Board

**Current of Prior Experience on CBJ Boards/Commissions/Committees**

Wetlands Review Board Chair, public member, 2019-2021.

**Reasons for Applying**

I would like to apply for the CBJ Comprehensive Plan Update AC. I am deeply interested in the Comprehensive Plan Update Advisory Committee because it offers an opportunity to contribute to shaping the future of our community in a thoughtful, strategic way. I am passionate about the long-term sustainability and growth of our area, and I believe that participating in this committee will allow me to help create policies and strategies that balance development, environmental preservation, and the needs of residents.

**Qualifications**

I received a BS in Civil Engineering from Northern Arizona University. I have over 16 years of experience here in Juneau working as a professional civil engineer. I am the owner of a local civil engineering firm that employees over 25 people. Please see my attached resume for additional information.

**Civic Activities, Memberships or Non-profits Involved With**

Recently was a member of the local Juneau Branches American Society of Engineers Board. I held the roles of Vice President, President and Past President between 2018-2022.

**Working with a Group**

I have extensive experience working with diverse groups from my time in college through my professional career. I approach working with groups that have diverse or conflicting perspectives by fostering an environment of respect and open communication, where all voices are heard. I actively listen to different viewpoints and encourage team members to express their underlying values and concerns. When conflicts arise, I focus on finding common ground and creative solutions that address everyone's needs. I also emphasize the value of diverse perspectives, highlighting how differences can lead to more innovative and well-rounded outcomes. My goal is to create an inclusive environment where collaboration thrives, even in the face of differing opinions.

**Meeting Schedule and Attendance**

Yes.

**Date of Birth (Optional)****Ethnicity (Optional)**

White

**Gender (Optional)**

Male

**Acknowledgment/Certification**

Lucas Chambers

**Resume, Education, etc. (Optional)**

[lucas chambers resume - principal engineer and manager - march 2025.pdf](#) [2]

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**Links**

[1] <https://juneau-ak.municodemeetings.com/bc/application> [2] [https://juneau-ak.municodemeetings.com/system/files/webform/lucas\\_chambers\\_resume\\_-\\_principal\\_engineer\\_and\\_manager\\_-\\_march\\_2025.pdf](https://juneau-ak.municodemeetings.com/system/files/webform/lucas_chambers_resume_-_principal_engineer_and_manager_-_march_2025.pdf)



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Submission information

Form: [CBJ Board Application](#) [1]  
Submitted by Visitor (not verified)  
Fri, 03/07/2025 - 11:43am  
66.230.94.182

**First Name**  
Meghan

**Last Name**  
Tabacek

**Residence Address**  
[Redacted]

**Mailing Address**  
[Redacted]

**Primary Phone Number**  
[Redacted]

**Secondary Phone Number**

**Email**  
[Redacted]

**Board, Commission, Committee**  
Other Board

**Current of Prior Experience on CBJ Boards/Commissions/Committees**  
I have served as the Executive Director of Trail Mix, Inc. since April 1, 2024, during which I have attended and organized all Trail Mix board meetings. In this role, I sit on all Trail Mix board committees, including finance, investing, board development, and outreach.  
Additionally, I served on the UAS Outdoor Studies Hiring Committee from February to March of 2025, assisting in the recruitment of a new faculty member by attending interviews and providing feedback.  
I have also engaged with the CBJ Parks and Recreation Advisory Committee as a presenter on two occasions, sharing insights related to trails, recreation, and community engagement.

**Reasons for Applying**  
I would be honored to serve on the Comprehensive Plan Advisory Committee and believe my experience as the Executive Director of Trail Mix, Inc. makes me a strong candidate for this role.

Through my work at Trail Mix, I have been deeply involved in Juneau’s recreation and infrastructure planning, including working on the Montana Creek Master Plan and Trail Mix’s Annual Work Plan with the CBJ. This experience has given me



valuable insight into long-term land use planning, stakeholder collaboration, and balancing environmental stewardship with community needs.

I have strong connections with numerous community groups that are invested in outdoor recreation, including the Juneau Off-Road Association, Juneau Mountain Bike Alliance, Rotarians, SheJumps, Girl Scouts, Rock Dump/Climbing Community, Juneau Trail Runners, and SAIL. These relationships allow me to understand and represent the diverse recreational interests of Juneau residents.

Recreation and trails are deeply important to Juneau residents, with 89% of Juneau residents using trails in some capacity. I am committed to advocating for these users and ensuring that recreation remains a central consideration in future planning efforts. Additionally, as a graduate of UAS's Environmental Studies program and an instructor of two outdoor studies courses annually at UAS, I am deeply connected to students and understand how they engage with Juneau's trails and public lands.

Trail Mix also has a strong focus on workforce development, particularly among seasonal staff. A portion of our seasonal crew members move to Juneau for the summer and leave in the winter, but we have seen an increasing number choose to stay. Over the past three years, we've had a growing trend of seasonal staff transitioning into full-time residents, contributing to Juneau's workforce. In 2024, four of our six out-of-state staff remained in Juneau, with three securing full-time jobs and one enrolling at UAS. This experience gives me a unique perspective on housing, employment, and community retention, all of which are critical to Juneau's long-term planning.

Outside of my professional work, I am also a recent first-time homebuyer, a bike commuter, and a music and arts enthusiast. My personal experiences help me understand the everyday needs of residents from housing affordability and accessibility to sustainable transportation and the importance of a vibrant arts scene.

I would be honored to contribute my expertise and perspective to this committee and help shape the future of Juneau's development.

### **Qualifications**

I am currently a graduate student in the Arctic and Northern Studies program at the University of Alaska Fairbanks, with an expected graduation in Spring 2026. I also hold an M.A. in Environment, Geography, and Outdoor Studies from the University of Alaska Southeast (2021), where I was honored as the Outstanding Graduate.

I have been deeply involved with Trail Mix, Inc. since 2020, starting as a crew member before advancing to Engagement Coordinator, Trail Manager, and ultimately Executive Director in April 2024. Through these roles, I have gained hands-on experience in trail development, public engagement, workforce development, and collaboration with local, state, and federal partners. My work has given me a strong understanding of land use, outdoor recreation planning, and community needs, all of which are critical to the Comprehensive Plan Advisory Committee's mission.

Additionally, I am an adjunct faculty member at UAS, teaching Backpacking and Backcountry Navigation (Fall 2023, Fall 2024), providing me with direct experience in outdoor education and an understanding of how students engage with Juneau's trails and public lands.

My commitment to community involvement extends beyond my professional roles. I have volunteered as a Girls on the Run coach at Sit' Eeti Shaanáx Glacier Valley Elementary School (2022) and as a ski mentor for SAIL's ORCA (Outdoor Recreation and Community Access) program (2022-2023), helping to make outdoor recreation accessible to all.

### **Civic Activities, Memberships or Non-profits Involved With**

- Volunteer with SheJumps (2022-current)
- Membership with AWARE (2024-current)
- Girls on the Run Coarch (2022)
- SAIL ORCA program volunteer (2022-2023)
- Tongass Recreation Partnership (2024-current)
- SEAGLA Pride Partner (2023, 2024, and will continue for years to come)

### **Working with a Group**

I believe that effective collaboration starts with ensuring that everyone has a chance to speak and feel heard. As the Executive Director of Trail Mix, I have facilitated conversations between diverse user groups, often navigating different perspectives on trail use and recreation. I championed our tagline, "Trails for all and all for trails," because I truly believe that all forms of recreation are valid and that everyone deserves access to outdoor spaces. Over the past two years as the Trails Manager and now Executive Director, I have made a concerted effort to reach out to groups that have been excluded or underrepresented on the trails and in recreation. Bringing people together only makes our community stronger. In my leadership role, I have worked to mediate discussions between different stakeholders, ensuring that all voices are acknowledged while striving to find solutions that make everyone feel like a winner. My approach is rooted in kindness, compassion, and active listening, all skills I have also honed as a college instructor. I have found that when people feel respected and heard, discussions are more productive and lead to stronger, more inclusive outcomes. One of my favorite strategies to get through a conflict is to go on a hike with representatives from the groups who feel their perspective is not heard. By spending time outside, we're able to focus on common goals and talk in a less stressful environment, as many people can feel intimidated and out of place in assembly chambers or board rooms. By changing the environment, letting the conversation flow, and finding common ground helps solutions emerge. By fostering open communication and encouraging collaboration, I aim to create an environment where diverse perspectives are valued and meaningful solutions can be reached.

**Meeting Schedule and Attendance**

Yes

**Date of Birth (Optional)**

[Redacted]

**Ethnicity (Optional)**

White

**Gender (Optional)**

Female

**Acknowledgment/Certification**

By signing/e-signing this document, I understand that this is a volunteer position appointed by the City and Borough of Juneau A

**Resume, Education, etc. (Optional)**

[meghan tabacek resume - meghan tabacek.pdf](#) [2]

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**Links**

[1] <https://juneau-ak.municodemeetings.com/bc/application> [2] [https://juneau-ak.municodemeetings.com/system/files/webform/meghan\\_tabacek\\_resume\\_-\\_meghan\\_tabacek.pdf](https://juneau-ak.municodemeetings.com/system/files/webform/meghan_tabacek_resume_-_meghan_tabacek.pdf)



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Submission information

Form: [CBJ Board Application](#) [1]  
Submitted by Visitor (not verified)  
Fri, 03/07/2025 - 11:59am  
24.237.2.184

**First Name**

Michael

**Last Name**

Heumann

**Residence Address**

[REDACTED]

**Mailing Address**

[REDACTED]

**Primary Phone Number**

[REDACTED]

**Secondary Phone Number**

**Email**

[REDACTED]

**Board, Commission, Committee**

Other Board

**Current of Prior Experience on CBJ Boards/Commissions/Committees**

I was part of the Blueprint Downtown Steering Committee for its entire duration which stretched over 4 years. Rarely missed a meeting.

**Reasons for Applying**

I'm applying for the Comp Plan AC because I see the Comprehensive Plan as a foundational element of Juneau's future, and I believe I have a lot of relevant experience to offer from my work as a real estate developer. I'd like to make an impact by sharing my real-world experience interacting with the CBJ on a frequent basis. I'm willing to share my thinking about how various changes to the plan would impact decision making by developers in general.

**Qualifications**

I grew up in Juneau, left for my education/early career and returned in 2018. My background is in engineering and business. I've been developing and building in Juneau full-time since I moved back. My work has given me a lot of experience working on multi-faceted projects. As an engineer I started my career with the State of Alaska, working on representing people's interests in managing oil and gas properties. I have a lot of experience working with writing regulations,

administering code, and working with diverse groups of staff to provide recommendations to the Commissioner, Governor and Legislature. I believe this experience will translate well to this committee.

Section R, Item 3.

**Civic Activities, Memberships or Non-profits Involved With**

I'm not presently involved with any groups relating to this.

**Working with a Group**

My past experience as an engineer required me to work with planners, geologists, geophysicists, economists, commercial negotiators, lawyers etc. etc. My current job requires me to work with constructions workers of all types, planners, engineers, bankers, neighbors, etc. Getting to work with such a wide range of people is a big part of why I enjoy my work. I love learning things and listening to people with a totally different perspective is the best way I've found to improve my understanding of things.

**Meeting Schedule and Attendance**

Yes, I'm aware. I have no problem making the meetings

**Date of Birth (Optional)**

**Ethnicity (Optional)**

**Gender (Optional)**

**Acknowledgment/Certification**

I, Michael Heumann, Agree

**Resume, Education, etc. (Optional)**

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**Links**

[1] <https://juneau-ak.municodemeetings.com/bc/application>





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Submission information

Form: [CBJ Board Application](#) [1]  
Submitted by Visitor (not verified)  
Sat, 03/08/2025 - 10:56am  
24.237.24.182

**First Name**  
Nimmy

**Last Name**  
Philips

**Residence Address**  
[REDACTED]

**Mailing Address**  
same

**Primary Phone Number**  
[REDACTED]

**Secondary Phone Number**

**Email**  
[REDACTED]

**Board, Commission, Committee**  
Other Board

**Current of Prior Experience on CBJ Boards/Commissions/Committees**  
Wayfinding Artist selection committee as a JAHc representative in 2020.

**Reasons for Applying**  
i left my partnership and the firm 2021 and became full time restaurant owner. Being an Engineer is my profession, being a restaurant owner is my passion. Being a restaurant owner allows me to serve the community directly. My role in the DBA provides me with a unique perspective on community building and improvement initiatives.

**Qualifications**  
Juneau is home, I have been in Juneau for past 14 years. With a background in computer science and electrical engineering, I worked as an electrical engineer and partner at a local firm for 11 years. my projects at Haight & Associates included private, non-profit, and public sectors. I continue to maintain a good reputation as a team builder with the client, vendors and implementing contractors. As a member of AC, my experience and knowledge of the power distribution, low voltage system and lighting controls in the past 14 years through various downtown projects will provide a background to future downtown projects.

**Civic Activities, Memberships or Non-profits Involved With**

I joined Juneau Arts and Humanities Council in 2016 and served as a board member for 6 years and served on the art design committee. I joined the Juneau -Gastineau Rotary Club in 2018 and serving presently. I joined the S.P.E.M. program in 2017 and served for 2 years. I joined the DBA board in June of 2022 and serving presently. As a member of AC, my experience and knowledge at various boards serving our beautiful community brought great relationships, connections and this will help and guide me as a future member on the Advisory Committee for the Comprehensive Plan Update project.

**Working with a Group**

I learned at a very young age to work with a group and overcome conflicts as a group. Working with a group, understanding the conflict and working together to overcome was a daily experience during my engineering career, board and committees and now at the restaurant.

**Meeting Schedule and Attendance**

I will attend all meetings in person

**Date of Birth (Optional)****Ethnicity (Optional)**

JUNEAU

**Gender (Optional)**

Female

**Acknowledgment/Certification**

Nimmy J Philips

**Resume, Education, etc. (Optional)**

[nimmy\\_philips\\_resume.pdf](#) [2]

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**Source URL:**<https://juneau-ak.municodemeetings.com/node/791/submission/165>

**Links**

[1] <https://juneau-ak.municodemeetings.com/bc/application> [2] [https://juneau-ak.municodemeetings.com/system/files/webform/nimmy\\_philips\\_resume.pdf](https://juneau-ak.municodemeetings.com/system/files/webform/nimmy_philips_resume.pdf)



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Submission information

Form: [CBJ Board Application](#) [1]  
Submitted by Visitor (not verified)  
Thu, 03/06/2025 - 8:49pm  
104.28.116.190

**First Name**

Tari

**Last Name**

Stage-Harvey

**Residence Address**

[REDACTED]

**Mailing Address**

same

**Primary Phone Number**

[REDACTED]

**Secondary Phone Number**

**Email**

[REDACTED]

**Board, Commission, Committee**

Other Board

**Current of Prior Experience on CBJ Boards/Commissions/Committees**

None

**Reasons for Applying**

I am mainly applying because someone reached out, but this is the one board I would consider giving time to. I've lived in Juneau for 15 years and experienced this community through many lenses. As a pastor in the valley, I've interacted with the intergenerational congregation I serve, the neighborhood youth at summer lunch for the past thirteen years that we've fed, the wide mix of people who use the food pantry, and the wonderful crew of women who are part of mom's group. My time in Angoon 30 years ago and the continuation of those relationships, has also given me a different perspective on the community. I'm married to a JPD detective and that has opened my eyes to many parts of the community I didn't know. We raised three children in the school district and one returned to Juneau to work at the teen shelter. The other two are still in college. We love Juneau and want to see the best for this community.

I am a big picture thinker without the ego of demanding my own way. Long range planning has been an intentional part of our life at Shepherd of the Valley and has made it possible for us to accomplish some ambitious projects in a small congregation. Family Promise also came out of planning and collaboration.

**Qualifications**

1993-1994 Volunteer in Mission at Angoon Teen Center

1999-2026 Lutheran pastor, 10 years in the Upper Peninsula of Michigan and 15 years in Juneau.

Helped develop Project HOPE in the UP where all the churches worked together to run a clothing resale shop and funnel the money back into the community by helping pay utilities.

Coordinated the gathering of food pantries in Juneau in 2011 to discuss gaps and weird competition. The beginning of the expansion of the food bank, summer lunches, and an awareness of food insecurity at schools and UAS grew out of that.

Spearheaded Family Promise of Juneau to use underutilized worship buildings to provide hospitality for families experiencing homelessness

Trained with Juneau Police Spouse Peer Support program to work through crises with families

Trained life coach and grief coach

Trained facilitator for Our Whole Lives, comprehensive sexuality education

**Civic Activities, Memberships or Non-profits Involved With**

Summer Lunch at SOV 2012-present: Serve 300 different youth a summer, kick off with a bike rodeo, and hire a staff of teens who might need a little assistance in vocational development

Family Promise Board - 2013-2021 Board Chair

Cluster Dean for Southeast Alaska Lutherans - 2024-present

**Working with a Group**

I'm curious and enjoy expanding my worldview. I do get rattled with arrogance, but am happy to ask better questions and push back when needed. I run a church so we are always navigating a wide mix of opinions and relationships.

**Meeting Schedule and Attendance**

Kind of, I am unaware of length of meetings. I don't sit still past 1.5 hours well

**Date of Birth (Optional)****Ethnicity (Optional)****Gender (Optional)****Acknowledgment/Certification**

Tari Stage-Harvey

**Resume, Education, etc. (Optional)**

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**Source URL:** <https://juneau-ak.municodemeetings.com/node/791/submission/158>

**Links**

[1] <https://juneau-ak.municodemeetings.com/bc/application>





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Submission information

Form: [CBJ Board Application](#) [1]  
Submitted by Visitor (not verified)  
Thu, 03/06/2025 - 5:26pm  
24.237.13.212

**First Name**  
Tonja

**Last Name**  
Moser

**Residence Address**  
[REDACTED]

**Mailing Address**  
Same

**Primary Phone Number**  
[REDACTED]

**Secondary Phone Number**

**Email**  
[REDACTED]

**Board, Commission, Committee**  
Planning Commission

**Current of Prior Experience on CBJ Boards/Commissions/Committees**  
I don't have any CBJ Board experience, but I have participated in numerous boards and committees as a teacher (1996-2019), including our union president. Most recently (2019-2024), I was a member of the local REALTOR® association, SEABR, and served as president in 2023.

**Reasons for Applying**  
My reasons are three-fold. One, I am a local REALTOR® and have had numerous experiences with the CBJ planning commission and a plethora of interactions with its various committees, boards, and employees. Two, I am currently building a house and have considerable insight into the system's idiosyncrasies and concerns about the blockades and difficulties a citizen might experience in the process. My third reason is that I would like to see Juneau continue to grow and be a viable place for young people to live and raise a family, as well as a place for older people to age into.

**Qualifications**  
REALTOR® 2017-current  
Juneau Resident, on and off, from 1976-present

Multiple experiences with boards, committees, and volunteer activities

**Civic Activities, Memberships or Non-profits Involved With**

2020-current: Organizing board member of 100 Women Who Care, Juneau  
2019-2023: Board member and president of SEABR  
1996-2019: Various positions as a board member for JEA,  
1996-2015: Board member for JCP, JDIA, and JAHA  
2010-2019: Advisor for JDHS Student Council, LGBTQ Alliance, and member of the JDHS Principal's Cabinet

**Working with a Group**

As a former teacher, I have a very good understanding of what it looks like to both manage and participate on a board or committee. My current job as a real estate agent exposes me to a wide variety of people and one core part of my job is to listen to people, communicate clearly, and work to resolve conflicts, and bring all sides together for a win/win situation. I believe that individual voices and opinions are necessary, but respect and compromise are key components to getting things done. I am a good mediator, a good listener, and someone who brings people together.

**Meeting Schedule and Attendance**

yes

**Date of Birth (Optional)**

[REDACTED]

**Ethnicity (Optional)**

Caucasian

**Gender (Optional)**

Female

**Acknowledgment/Certification**

Tonja Moser

**Resume, Education, etc. (Optional)**

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**Links**

[1] <https://juneau-ak.municodemeetings.com/bc/application>



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Submission information

Form: [CBJ Board Application](#) [1]  
 Submitted by Visitor (not verified)  
 Sun, 03/02/2025 - 6:34pm  
 98.97.43.203

**First Name**  
Amara

**Last Name**  
Enciso

**Residence Address**  
[REDACTED]

**Mailing Address**  
Same

**Primary Phone Number**  
[REDACTED]

**Secondary Phone Number**

**Email**  
[REDACTED]

**Board, Commission, Committee**  
Other Board

**Current of Prior Experience on CBJ Boards/Commissions/Committees**  
None

**Reasons for Applying**  
My goal with submitting my application for the City of Juneau's Comprehensive Planning Advisory Committee, is to make a positive and impactful contribution in the further development of this committees vision and goals. I am a large advocate to the growth and diversification of the culinary and hospitality demographic of Juneau. I am also very interested in the opportunity to help create and nurture platforms for community enrichment and leisure.

**Qualifications**  
I have been a part of the culinary, hospitality and public sector industry in Juneau for eight years and have worked as a private chef, bartender, substitute teacher and Unemployment Services Technician. I have been exposed to people from our community that are involved with, land use, transportation, infrastructure, housing, utilities, and recreation in a number of different settings. Being able to personally engage with community members that are directly affiliated with the aforementioned components of Juneau's infrastructure, has allowed for me to understand the workings of these con[REDACTED]ts

and identify certain opportunities for their positive growth.

In addition to my employment history in Juneau, I also volunteer with the Juneau School district by giving ~~giving~~ <sup>giving</sup> culinary arts classes to students. While living in San Diego, CA I was a flight training coordinator for a private helicopter flight training facility, at which I took part in the private airport's Letter of Agreement improvement committee. My participation in this committee exposed me to an incredibly diverse body of committee members, which allowed for me to gain experience in working with many conflicting perspectives.

### **Civic Activities, Memberships or Non-profits Involved With**

X'aaká Hít Point House Revitalization-2022-Present

Indigenous Food Lab-2025-Present

Alaska Culinary Alliance-2020-Present

### **Working with a Group**

As a culinary professional I have been exposed to working with people from all walks of life in high stress environments. From language barriers, to cultural differences, I have learned how to adapt to constant change in minimal time. I asses goals and navigate to those goals with full reciprocated contribution from diverse team members. I have experienced many conflicting personalities in a professional work setting and have been able to calmly and successfully lead them to a goal.

### **Meeting Schedule and Attendance**

Yes

### **Date of Birth (Optional)**

[REDACTED]

### **Ethnicity (Optional)**

Hispanic

### **Gender (Optional)**

Female

### **Acknowledgment/Certification**

Amara Lopez Enciso

### **Resume, Education, etc. (Optional)**

[a. enciso\\_resume\\_2025.pdf](#) [2]

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### **Links**

[1] <https://juneau-ak.municodemeetings.com/bc/application> [2] [https://juneau-ak.municodemeetings.com/system/files/webform/a.\\_enciso\\_resume\\_2025.pdf](https://juneau-ak.municodemeetings.com/system/files/webform/a._enciso_resume_2025.pdf)





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Submission information

Form: [CBJ Board Application](#) [1]  
Submitted by Visitor (not verified)  
Mon, 02/17/2025 - 3:10pm  
24.237.5.66

**First Name**

Bailey

**Last Name**

Gent

**Residence Address**

[REDACTED]

**Mailing Address**

Same

**Primary Phone Number**

[REDACTED]

**Secondary Phone Number**

**Email**

[REDACTED]

**Board, Commission, Committee**

Other Board

**Current of Prior Experience on CBJ Boards/Commissions/Committees**

None

**Reasons for Applying**

Comp Plan AC

I am applying for the CBJ Comprehensive Plan Advisory Committee because I am deeply committed to advancing equitable and community-driven solutions to local challenges. The opportunity to contribute to the development and refinement of a comprehensive plan that reflects Juneau’s values, priorities, and vision for the future aligns with my passion for creating meaningful, sustainable change.

My professional background in nonprofit administration, housing advocacy, and community partnerships equips me to offer insight into addressing housing stability, economic growth, and social equity areas I believe are critical to a thriving Juneau. I hope to make an impact by helping the committee identify actionable strategies that balance diverse stakeholder interests while fostering inclusive development.

**Qualifications**

My qualifications to serve on the Comprehensive Plan Advisory Committee stem from a combination of experience, education, and community involvement.

**Education:** I hold an M.A. in Nonprofit Administration and Development and dual B.A. and B.S. degrees in Peace & Justice Studies and Business Administration, which have provided me with a strong foundation in strategy, governance, and community-oriented decision-making.

**Experience:** As the Executive Director of Family Promise of Juneau, I have worked extensively on strategic initiatives such as Rapid Rehousing, Diversion Services, and landlord mediation, helping prevent child homelessness and stabilize at-risk households. My prior roles in marketing, project management, and corporate partnerships demonstrate my ability to engage diverse groups, manage complex projects, and build effective systems for long-term impact.

**Certifications:** I am a Project Management Professional (PMP), ensuring that I can effectively manage and contribute to the committee's objectives with a structured and results-driven approach.

**Civic Activities, Memberships or Non-profits Involved With**

I am currently a board member of the Alaska Coalition on Housing and Homelessness, where I collaborate with other leaders across the state to address housing instability and homelessness.

As Executive Director of Family Promise of Juneau I work closely with national organizations to enhance resources for families in need. My involvement in these organizations reflects my commitment to addressing systemic issues and finding solutions that benefit the community.

**Working with a Group**

My approach to working with diverse groups is rooted in active listening, fostering mutual respect, and creating spaces for all voices to be heard. Throughout my career, I have facilitated collaborations among individuals with varying perspectives, including government agencies, nonprofits, corporate partners, and community members.

One example is my leadership at Family Promise of Juneau, where I engage with stakeholders who hold differing opinions about the best ways to address homelessness. By grounding discussions in shared goals and emphasizing data-driven solutions, I have successfully built consensus around impactful strategies.

When encountering conflicting perspectives, I focus on identifying common ground and reframing challenges as opportunities for innovation. I believe in maintaining a solutions-oriented mindset and ensuring that all contributions are valued, which I will bring to the CBJ Comprehensive Plan Advisory Committee.

**Meeting Schedule and Attendance**

Yes

**Date of Birth (Optional)****Ethnicity (Optional)****Gender (Optional)****Acknowledgment/Certification**

Bailey Abigail Gent

**Resume, Education, etc. (Optional)**

[bailey\\_gent\\_resume\\_2025.docx .pdf](#) [2]

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[1] <https://juneau-ak.municodemeetings.com/bc/application> [2] [https://juneau-ak.municodemeetings.com/system/files/webform/bailey\\_gent\\_resume\\_2025.docx .pdf](https://juneau-ak.municodemeetings.com/system/files/webform/bailey_gent_resume_2025.docx.pdf)

Section R, Item 3.



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Submission information

Form: [CBJ Board Application](#) [1]  
Submitted by Visitor (not verified)  
Sat, 03/01/2025 - 1:07pm  
24.237.0.72

**First Name**  
Craig

**Last Name**  
Dahl

**Residence Address**  
[REDACTED]

**Mailing Address**  
[REDACTED]

**Primary Phone Number**  
[REDACTED]

**Secondary Phone Number**

**Email**  
[REDACTED]

**Board, Commission, Committee**  
Other Board

**Current of Prior Experience on CBJ Boards/Commissions/Committees**  
I served as co-chair of the Mayor's Visitor Industry Task Force (VITF).

**Reasons for Applying**  
I am applying for a seat on the "Comp Plan AC"  
I have been involved in the health of the Juneau economy for all of my professional career - 40 plus years in banking and 10 years with the Juneau Chamber. My wife and I both attended JDHS, raised our family, and enjoyed careers that directly affected the local economy. We now look forward to our grandchildren having a healthy, vibrant community to grow, live and work. There are many factors that drive local economic conditions but at the foundation is a local government that provides the necessary framework for businesses and citizens to succeed and enjoy living here. The Comprehensive Plan serves as one critical guidance document that sets the tone for the coming years for other plans and policies. I would like to utilize my years of experience to help shape this next iteration of the plan.

**Qualifications**



42-year regional banking career - the last 18 years as President & CEO and member of the bank board of directors. 9 years on the board (vice-chair) of the Federal Home Loan Bank Seattle with a mission of funding affordable housing throughout the Pacific Northwest. The last 10 years working in a leadership position with the Juneau Chamber of Commerce.

**Civic Activities, Memberships or Non-profits Involved With**

I have served on many boards and worked with a variety of organizations over the years. Juneau Volunteer firefighter for six years, board of the Alaska Housing Development Corporation and American Red Cross. I am currently on the board of the Alaska Committee, member of the Kensington Gold Mine Community Advisory Committee and Hecla Greens Creek Community Advisory Committee, board member of The Capitol Fund (subset of the Juneau Community Foundation), and a 42-year member of the Glacier Valley Rotary Club.

**Working with a Group**

Having worked with a variety of boards, and by virtue of career demands, I am able to listen to all input with a level of objectivity to assess relevancy to the stated objectives. Whether the input is in line with the goals, people deserve the opportunity to express their opinions and should be listened to without disagreement. Questions can be asked, but in a manner that respects their efforts to come forward and comment.

**Meeting Schedule and Attendance**

I understand the proposed time commitment for the Comp Plan AC

**Date of Birth (Optional)**

**Ethnicity (Optional)**

**Gender (Optional)**

**Acknowledgment/Certification**

Craig E. Dahl

**Resume, Education, etc. (Optional)**

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**Links**

[1] <https://juneau-ak.municodemeetings.com/bc/application>



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Submission information

Form: [CBJ Board Application](#) [1]  
Submitted by Visitor (not verified)  
Fri, 02/14/2025 - 2:36pm  
24.237.7.42

**First Name**

David

**Last Name**

Ringle

**Residence Address**

[REDACTED]

**Mailing Address**

[REDACTED]

**Primary Phone Number**

[REDACTED]

**Secondary Phone Number**

[REDACTED]

**Email**

[REDACTED]

**Board, Commission, Committee**

Other Board

**Current of Prior Experience on CBJ Boards/Commissions/Committees**

none, but I participated extensively in the CBJ Emergency Response to COVID from March 2020 until the response ended in 2021.

**Reasons for Applying**

I feel my experiences as a 30 year resident of Juneau will bring a valuable perspective to the Comprehensive plan. I have two children living in this community as young adults and see the need to keep Juneau. My years as a teacher exposed me to all the levels of individuals and households within our community both ethnically and economically. My 5 years of leadership with St. Vincent de Paul has familiarized myself with the challenges of homelessness, the difficulties of running low income housing and

**Qualifications**

For five years since February of 2020, I have been the Executive Director of St. Vincent de Paul, managing 5 low income housing buildings as well as an emergency shelter. I am co-chair of the Juneau Coalition on Housing and Homeless

was on DOT's WEDCOR transportation planning for the West Corridor of Egan Drive in the early 2000's. I lobbied for bike facilities with America Bikes from 2002 through 2004 when Don Young was chairman of the House Transportation Committee. I taught in the Juneau School District for over 23 years. I have proven collaborative experience collaborating with the city on the Emergency Cold Weather Shelter, low income housing, and the homeless coalition's and CBJ's response to COVID.

I've managed

### **Civic Activities, Memberships or Non-profits Involved With**

Board Member and four year President, Kluane Chilkat International Bike Relay, 1999-2004. Coached middle school basketball and ran the Fastbreak Middle School Basketball Tournament from 1995 to 2003. Co-chair of the Juneau Coalition on Housing and Homelessness, 2023-present. Organized and led Catholic/ecumenical retreats in communities as varied as Juneau, Soldotna, and Wasilla between 1997 and 2012. Board member of St. Vincent de Paul from 2018 to February of 2020 when I became the General Manager/Executive Director.

### **Working with a Group**

I have proven experience working with and leading groups in situations as diverse as basketball tournaments and bike races to Middle School teaching teams to the Juneau Homeless Coalition. I'm a person who listens, considers all possible options, and makes sure to follow through with competent execution when a decision is made. In areas as diverse as middle schools basketball, state-wide bike advocacy campaigns, and getting Juneau and Wasilla to collaborate on retreat leadership, I've handle very diverse perspectives.

### **Meeting Schedule and Attendance**

agreed

### **Date of Birth (Optional)**

### **Ethnicity (Optional)**

caucasian

### **Gender (Optional)**

Male

### **Acknowledgment/Certification**

Dave Ringle

### **Resume, Education, etc. (Optional)**

[dave\\_ringle\\_resume.pdf](#) [2]

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**Source URL:** <https://juneau-ak.municodemeetings.com/node/791/submission/138>

### **Links**

[1] <https://juneau-ak.municodemeetings.com/bc/application> [2] [https://juneau-ak.municodemeetings.com/system/files/webform/dave\\_ringle\\_resume.pdf](https://juneau-ak.municodemeetings.com/system/files/webform/dave_ringle_resume.pdf)



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Submission information

Form: [CBJ Board Application](#) [1]  
Submitted by Visitor (not verified)  
Thu, 02/27/2025 - 11:22pm  
24.237.7.42

**First Name**

Deborah

**Last Name**

Johnston

**Residence Address**

[REDACTED]

**Mailing Address**

Same

**Primary Phone Number**

[REDACTED]

**Secondary Phone Number**

N/A

**Email**

[REDACTED]

**Board, Commission, Committee**

Other Board

**Current of Prior Experience on CBJ Boards/Commissions/Committees**

I recently began my 7th year on the Bartlett Regional Hospital Board and am currently serving as board president.

**Reasons for Applying**

I am applying for the Comp Plan AC. I am interested assisting this advisory committee to develop a plan for Juneau's future that incorporates development and land use options to address the shortage of attainable housing options without compromising recreational and other open spaces; provides support for a sustainable commercial economic base; protects environmentally sensitive areas; and minimizes potential conflicts between land uses. In a nutshell, I would like to see a long-term plan that considers the requirements needed to attract and retain a sustainable workforce while respecting the needs of the current population base.

**Qualifications**

In my role as CFO at Alaska Pacific University, I assisted in leading the development of a comprehensive master p campus land use and facilities, including classroom and administrative spaces, campus housing and dining facilities



as recreational spaces.

As chair of the BRH Planning Committee, I initiated the process of updating the updating the Facility M intent to integrate the hospital's strategic plan into the project. We are currently interviewing consultants healthcare and hospital specific experience.

### **Civic Activities, Memberships or Non-profits Involved With**

The Society of St Vincent de Paul - June 2023 to current  
Brother Francis Shelter Advisory Council - 2014-2018 Anchorage

### **Working with a Group**

Throughout my career and volunteer activities I have worked with numerous groups including boards and advisory councils, strategic planning committees as well as software implementation and data governance workgroups. Each of these groups consisted of individuals with different viewpoints and experience. The most successful of these groups established a set of ground rules, or "rules of the road" that include collaboration, respect and healthy conflict.

### **Meeting Schedule and Attendance**

I am aware of the commitment that participation on this committee will require of me.

### **Date of Birth (Optional)**

### **Ethnicity (Optional)**

White

### **Gender (Optional)**

Female

### **Acknowledgment/Certification**

I agree to the terms of the above statement and certify that the information I provided is accurate. - Deborah Johnston

### **Resume, Education, etc. (Optional)**

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**Source URL:**<https://juneau-ak.municodemeetings.com/node/791/submission/149>

### **Links**

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Submission information

Form: [CBJ Board Application](#) [1]  
 Submitted by Visitor (not verified)  
 Fri, 02/28/2025 - 5:12pm  
 24.237.28.185

**First Name**  
Frank

**Last Name**  
Delaney

**Residence Address**  
[REDACTED]

**Mailing Address**  
same

**Primary Phone Number**  
[REDACTED]

**Secondary Phone Number**

**Email**  
[REDACTED]

**Board, Commission, Committee**  
Other Board

**Current of Prior Experience on CBJ Boards/Commissions/Committees**  
I have been on the Douglas Steering Committee since it was formed a few years ago.

**Reasons for Applying**  
I am applying to be on the Comprehensive Plan Advisory Committee. I'm interested in the long term development and growth of Juneau, and this committee seems like it will be an excellent way to support that growth. As the Managing Director for Perseverance Theatre I am deeply invested in the community. I was told that my experience and point of view as a leader in the arts community would be valuable.

**Qualifications**  
I am currently serving on the Douglas Steering Committee which has been a valuable learning experience. In my working life I regularly facilitate board meetings. I have served on various search committees throughout my professional life. Additionally the bulk of my professional life has been spent in I.T. and theater. As an I.T. professional I primarily participated in or lead implementation teams, including plan and proposal development. As a theater professional I

extensive training in working collaboratively. It is often expected in the professional theater world for artists to come together and collaborate in very short time periods.

### **Civic Activities, Memberships or Non-profits Involved With**

I am a member of the Sons of Norway. As I stated previously, I'm the Managing Director for Perseverance Theatre. In that capacity I interact with many different non-profits around Juneau, not only in the arts sector. We partner regularly with other groups to help lift up their missions.

### **Working with a Group**

Prior to my current position I worked in I.T. for 25 years, primarily on implementation and design teams. I worked with people all across the state. In my role at Perseverance, I regularly collaborate with artists with very different cultural backgrounds than my own. I am adept at talking through issues, and one of my strengths as an I.T. professional was to distill complex concepts in such a way that laypeople could easily understand them. I believe in respectful discourse and welcome a variety of perspectives.

### **Meeting Schedule and Attendance**

yes

### **Date of Birth (Optional)**

### **Ethnicity (Optional)**

white

### **Gender (Optional)**

nonbinary

### **Acknowledgment/Certification**

Frank Delaney

### **Resume, Education, etc. (Optional)**

[delaney\\_artistic\\_cv\\_2-27-25.pdf](#) [2]

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**Source URL:** <https://juneau-ak.municodemeetings.com/node/791/submission/150>

### **Links**

[1] <https://juneau-ak.municodemeetings.com/bc/application> [2] [https://juneau-ak.municodemeetings.com/system/files/webform/delaney\\_artistic\\_cv\\_2-27-25.pdf](https://juneau-ak.municodemeetings.com/system/files/webform/delaney_artistic_cv_2-27-25.pdf)



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Submission information

Form: [CBJ Board Application](#) [1]  
Submitted by Visitor (not verified)  
Thu, 02/13/2025 - 11:16am  
24.237.30.3

**First Name**  
Joyce

**Last Name**  
Vick

**Residence Address**  
[REDACTED]

**Mailing Address**  
Same as above

**Primary Phone Number**  
[REDACTED]

**Secondary Phone Number**

**Email**  
[REDACTED]

**Board, Commission, Committee**  
Other Board

**Current of Prior Experience on CBJ Boards/Commissions/Committees**  
Youth Activity Board (I think 9 years)  
Douglas Advisory Board (I think 12 years)  
Douglas Steering Committee (From when it was formed)

**Reasons for Applying**  
I would like to apply for the CDD Comprehensive Board. I am currently involved with the Douglas Steering Committee and the Douglas Advisory Board, and I would like to make sure that Douglas is not forgotten in the Area-wide plan.

**Qualifications**  
I have lived in Douglas for almost 40 years. My husband and I have raised 2 sons here, of which one lives here. I am on the Douglas Advisory Board, Douglas 4th of July Committee, and Douglas Steering Committee. In the past I was on the Juneau Douglas Ice Association (JDIA) and high school hockey Blue Line Club, which I was one of the founding members. I was born in Central Alaska and met my husband while working at Prudhoe Bay moved to Douglas and love it here. I received my bachelors degree in Business Administration with an emphasis in Accounting while working at Juneau Ready N [REDACTED] a



bookkeeper. I have also worked as an accountant for Tlingit Haida Regional Housing Authority and the State of Alaska for the Department of Administration. At my state job, I worked on state budgets and the Financial Statement for the State of Alaska. I also organize and volunteer for DOT's road cleanup in two different areas of Juneau and Douglas. I have also organized three Toastmaster Conferences and help with registration, auction and education sessions with one.

### **Civic Activities, Memberships or Non-profits Involved With**

Currently I am on the board of the Douglas Advisory Board, Douglas 4th of July Committee and four Toastmaster Clubs of which I am on board for three of them. In the past, I have been on the Youth Activity Board and other non-profits including the Elks, Moose, and the Masons.

### **Working with a Group**

It is important that each member have an opportunity to speak and be listened to respectfully. I do not believe that as a member of a board, I should go to public members and speak ill of decisions that have been voted on and passed whether I personally agree with them or not.

### **Meeting Schedule and Attendance**

Yes

### **Date of Birth (Optional)**

### **Ethnicity (Optional)**

### **Gender (Optional)**

### **Acknowledgment/Certification**

Joyce D Vick

### **Resume, Education, etc. (Optional)**

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**Source URL:** <https://juneau-ak.municodemeetings.com/node/791/submission/137>

### **Links**

[1] <https://juneau-ak.municodemeetings.com/bc/application>



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Submission information

Form: [CBJ Board Application](#) [1]  
Submitted by Visitor (not verified)  
Thu, 02/20/2025 - 11:41am  
24.237.13.239

**First Name**

Judith

**Last Name**

Ament

**Residence Address**

[REDACTED]

**Mailing Address**

[REDACTED]

**Primary Phone Number**

[REDACTED]

**Secondary Phone Number**

**Email**

[REDACTED]

**Board, Commission, Committee**

Other Board

**Current of Prior Experience on CBJ Boards/Commissions/Committees**

N/A

**Reasons for Applying**

Applying for the Comprehensive Plan Advisory Committee "Comp Plan AC"

As a permanent resident born in The Netherlands, I have chosen Juneau as my long-term home. I am deeply committed to the well-being and prosperity of this community. My husband, born and raised in Juneau, has shared the rich history and strong sense of community that defines these beautiful lands and waters. We will raise our children here, with the hope that they will grow up in a thriving community that offers ample opportunities for educational, social, environmental, and economic development.

I'm excited about the chance to serve on the Comprehensive Plan Advisory Committee because I believe in the importance of planning for the future. I want to help shape the decisions and policies that will impact our land use, transportation infrastructure, housing, utilities, and recreation. I look forward to contributing to safeguarding the natural beauty that surrounds

people here while allowing people and businesses to thrive.

By bringing my perspective as an immigrant to the table, I hope to ensure that diverse voices are heard and considered in the planning process. I'm committed to making Juneau a better place for all residents, and I'm enthusiastic about working with fellow community members to achieve that goal.

### **Qualifications**

I hold a PhD in Environmental Science with a focus on Conservation Biology. Throughout my career, I've gained vast experience in visualizing and interpreting quantitative and qualitative data, making maps, and driving data-driven decisions. This expertise has equipped me with the skills to contribute effectively to the planning process.

Having lived in several countries, including The Netherlands, the United Kingdom, South Africa, and the US, I bring a diverse perspective and a wealth of ideas for what Juneau can aspire to be. Each of these experiences has provided me with unique insights and inspiration, which I am eager to apply in shaping Juneau's future.

With my strong background in environmental science and a deep appreciation for data-driven decision-making, I am well-prepared to support the Comprehensive Plan Advisory Committee in making informed and impactful choices for our community.

### **Civic Activities, Memberships or Non-profits Involved With**

Founder and lead of the Devil's Club Running Club - a group of runners that meets weekly at Devil's Club Brewery for a social run on the downtown streets and trails.

Member of the Juneau Nordic Ski Club

### **Working with a Group**

Through my professional work, I collaborate with stakeholders across many countries (e.g., Kenya, Angola, Mexico) on land use planning, carbon accounting, and other conservation issues. This includes working with government officials, indigenous peoples and local communities, which has allowed me to develop a broad understanding of different approaches to sustainable development, land conservation, and ways of working more generally.

My approach to collaborative work is grounded in active listening and empathy. I focus on understanding diverse perspectives, can quickly summarize key points, and find common ground to drive consensus. By fostering open communication and emphasizing shared goals, I can maintain and provide an overview, ensuring different voices are heard while driving for results.

My ability to listen, summarize, and find common ground has been instrumental in resolving conflicts and making informed decisions in my professional work. I am confident that these skills will be valuable in contributing to the success of the Comprehensive Plan Advisory Committee.

### **Meeting Schedule and Attendance**

Yes

### **Date of Birth (Optional)**

### **Ethnicity (Optional)**

### **Gender (Optional)**

### **Acknowledgment/Certification**

Judith Ament

### **Resume, Education, etc. (Optional)**

[cv\\_judith\\_ament.pdf](#) [2]

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Section R, Item 3.

**Source URL:** <https://juneau-ak.municodemeetings.com/node/791/submission/140>

### Links

[1] <https://juneau-ak.municodemeetings.com/bc/application> [2] [https://juneau-ak.municodemeetings.com/system/files/webform/cv\\_judith\\_ament.pdf](https://juneau-ak.municodemeetings.com/system/files/webform/cv_judith_ament.pdf)





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Submission information

Form: [CBJ Board Application](#) [1]  
Submitted by Visitor (not verified)  
Thu, 02/27/2025 - 2:02pm  
24.237.6.57

**First Name**

Katie

**Last Name**

Thomas-Canfield

**Residence Address**

[Redacted]

**Mailing Address**

Same

**Primary Phone Number**

[Redacted]

**Secondary Phone Number**

**Email**

[Redacted]

**Board, Commission, Committee**

Other Board

**Current of Prior Experience on CBJ Boards/Commissions/Committees**

I have not previously served on a CBJ board or commission

**Reasons for Applying**

I am interested in joining the board of the Comp Plan AC because I am passionate about this town and the sustainability of its tourism industry balancing our community commitment to nature, and to contribute a younger family (I am 30 years old and moved up to Juneau 5 years ago) perspective. I had the opportunity to contribute to the various community events around and commenting on the commercial use plan starting in the summer of 2024 and continuing until November 2024, which really piqued my interest in these issues and raised my awareness about the immense work the city is undertaking to update its policy around land use in a way that balances our economy, different community interests/needs, and our commitment to preservation of our unique natural environment. I am an economist by training, so have a keen eye on supporting local businesses; however, I am also deeply personally and professionally committed to equity, including ensuring our town remains affordable and accessible to marginalized and lower income households. As a board member, I hope I would be able to shed a younger voice to these discussions, and could help us brainstorm different approaches to these questions, drawing on my 10+ years of community development experience. In addition, it would be a unique

opportunity for me to get more involved in local activities, and to apply my extensive professional and leadership experience to this town that I love so much.

**Qualifications**

I have worked with nonprofits on community mobilizing and organizing for the last 13 years. I have primarily worked with nonprofits abroad, working on complex issues during or immediately after conflict (including Iraq, South Sudan, Uganda, Yemen, Afghanistan, and Haiti). In my work, I have built skills that would serve me well as a Board member of the Comp Plan AC, including coalition-building, conflict-resolution and exchange across diverse interest groups, best practices in communications for social and behavior change, and advocacy. In many of these cases, I have worked on "housing, land and property" issues or the complicated milieu of disputed land titles and interests in land development following conflict, where records were lost or destroyed, and government systems are fledgling or weak so struggle to arbitrate such disputes. Notably, while working with the International Rescue Committee, I have supported in defining legal support to disputants, and to working with local governments (particularly in Iraq) to set up plans for land use as well as disputes. While the context in Juneau is very different than the contexts where I've worked professionally, I would be able to draw on these skills and apply them here. I have also risen to senior management positions (managing \$10-50 million budgets and teams of up to 500 people) in my professional roles, experience which has given me strong leadership, budget management, and soft skills which will be relevant to this Board position.

**Civic Activities, Memberships or Non-profits Involved With**

- Ukrainian family sponsor circle 2 (January 2023-present)
- American Red Cross volunteer (February 2023-February 2024)
- SE Alaska Food Bank volunteer (2023-present)

**Working with a Group**

I have worked in over 13 different countries, with dozens of different nationalities, religions, and ethnicities, during particularly high-stress moments where inter-personal conflict is more likely. As a manager, I have worked with very diverse teams to build inroads and better coordination across varying communication styles, values/priorities, and perspectives more generally, rooted in their different cultural contexts/backgrounds. In many cases, this required an active and empathetic listening style, airing on the side of over communication, and creating and adapting spaces for different interest groups to provide their insights. Part of the reason I love my work so much is that it gives me the opportunity to interact with really different groups of people, challenging me to remain flexible, empathetic, and a continued learner; I would look forward to this experience in Juneau and getting more opportunity to interact with the many different communities and interest groups who call this place home.

**Meeting Schedule and Attendance**

Yes

**Date of Birth (Optional)**

[Redacted]

**Ethnicity (Optional)**

White

**Gender (Optional)**

Woman

**Acknowledgment/Certification**

Read and agree: Katie Thomas-Canfield

**Resume, Education, etc. (Optional)**

[katie thomas-canfield cv.pdf](#) [2]

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**Links**

[1] <https://juneau-ak.municodemeetings.com/bc/application> [2] [https://juneau-ak.municodemeetings.com/system/files/webform/katie\\_thomas-canfield\\_cv.pdf](https://juneau-ak.municodemeetings.com/system/files/webform/katie_thomas-canfield_cv.pdf)



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Submission information

Form: [CBJ Board Application](#) [1]  
Submitted by Visitor (not verified)  
Wed, 02/26/2025 - 12:45pm  
216.67.27.5

**First Name**  
Kelly

**Last Name**  
Sorensen

**Residence Address**  
[REDACTED]

**Mailing Address**  
[REDACTED]

**Primary Phone Number**  
[REDACTED]

**Secondary Phone Number**

**Email**  
[REDACTED]

**Board, Commission, Committee**  
Other Board

**Current of Prior Experience on CBJ Boards/Commissions/Committees**  
I do not have any prior experience on a CBJ board.

**Reasons for Applying**  
I was invited to apply to the public-member Advisory Committee for the City and Borough of Juneau’s Comprehensive Plan Update

**Qualifications**  
Most of my recent experience has been working with Discovery Southeast, a local nature education non-profit. I spend most of time organizing and running programs taking kids outside to connect with nature. We regularly (daily?) visit CBJ parks, shelters/cabins, and trails all around CBJ. I also own a trailer park in North Douglas and have some experience and a lot of interest in the future of low-income housing in Juneau. I have an undergraduate degree in Environmental Studies and a Masters in Teaching. I have been a teacher in the district and have worked as the Education Technology Director for the state.



**Working with a Group**

I like to listen first, check for understanding, and provide my thoughts and opinions when asked or when I feel it is necessary. I believe that progress comes from understanding and thoughtful and purposeful compromise and problem solving.

**Meeting Schedule and Attendance**

yes

**Date of Birth (Optional)**

[REDACTED]

**Ethnicity (Optional)**

**Gender (Optional)**

Female

**Acknowledgment/Certification**

Kelly Sorensen

**Resume, Education, etc. (Optional)**

[kelly\\_sorensen\\_resume.pdf](#) [2]

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**Links**

[1] <https://juneau-ak.municodemeetings.com/bc/application> [2] [https://juneau-ak.municodemeetings.com/system/files/webform/kelly\\_sorensen\\_resume.pdf](https://juneau-ak.municodemeetings.com/system/files/webform/kelly_sorensen_resume.pdf)



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Submission information

Form: [CBJ Board Application](#) [1]  
Submitted by Visitor (not verified)  
Sun, 03/02/2025 - 7:05pm  
24.237.8.240

**First Name**  
Lia

**Last Name**  
Domke

**Residence Address**  
[REDACTED]

**Mailing Address**  
same

**Primary Phone Number**  
[REDACTED]

**Secondary Phone Number**

**Email**  
[REDACTED]

**Board, Commission, Committee**  
Other Board

**Current of Prior Experience on CBJ Boards/Commissions/Committees**  
I have not participated in CBJ specific boards, commissions or committees. However, I have organized and run scientific symposiums (2 years), served as a representative for student interests in department faculty meetings during my graduate education (2 years), and represented department student interests at a college-wide student association meetings (2 years).

**Reasons for Applying**  
Comp plan AC  
I am a lifelong Juneau resident who is invested in the long-term success, health, and development of our Juneau community. As someone who would like to remain in Juneau throughout my career, I would like to support positive development and provide a unique perspective on how I and others view that development. In this position, I look forward to providing input in the formation of key policy decisions and plans.

**Qualifications**

I earned a PhD in fisheries from the University of Alaska Fairbanks based in Juneau. Through this experience, I gained transferable skills that I believe will serve me as part of the committee. I can quickly synthesize different information, critically review testimony and documentation, and am consistent and dependable.

### Civic Activities, Memberships or Non-profits Involved With

Juneau Soccer Club coach (2024 - present)

### Working with a Group

I recognize that all people have different preferred ways of communicating and different lived experiences that influence their perspectives. I attempt to approach most scenarios and conversations by assuming positive intent first and strive to clarify and gain deeper understanding when facing conflicts. I recognize that I also make mistakes and I take responsibility for my actions. However, I am firm that mutual respect should be shown across all groups and conversations regardless of differing perspectives.

### Meeting Schedule and Attendance

Yes

### Date of Birth (Optional)

### Ethnicity (Optional)

White

### Gender (Optional)

Female

### Acknowledgment/Certification

Lia Domke

### Resume, Education, etc. (Optional)

[domke\\_cv\\_2-27-25.pdf](#) [2]

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**Source URL:** <https://juneau-ak.municodemeetings.com/node/791/submission/154>

### Links

[1] <https://juneau-ak.municodemeetings.com/bc/application> [2] [https://juneau-ak.municodemeetings.com/system/files/webform/domke\\_cv\\_2-27-25.pdf](https://juneau-ak.municodemeetings.com/system/files/webform/domke_cv_2-27-25.pdf)



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Submission information

Form: [CBJ Board Application](#) [1]  
Submitted by Visitor (not verified)  
Sun, 02/23/2025 - 2:12pm  
24.237.12.203

**First Name**

Logan

**Last Name**

Miller

**Residence Address**

[REDACTED]

**Mailing Address**

same

**Primary Phone Number**

[REDACTED]

**Secondary Phone Number**

**Email**

[REDACTED]

**Board, Commission, Committee**

- Juneau Commission on Aging
- Other Board

**Current of Prior Experience on CBJ Boards/Commissions/Committees**

n/a

**Reasons for Applying**

I grew up in Juneau and recently moved back with the intention of living here long-term. I love this community and I want to work collaboratively with other community members to continue improving Juneau. I benefited so much from growing up here, and am so grateful for all the opportunities I was offered -- many of which were available because of dedicated citizens making things happen. I now feel it's my turn to take part in that process.

I am particularly interested in being part of the Advisory Committee for the Comprehensive Plan Update. I am interested in working with other Juneauites to continue creating an evolving vision for our community. I think there is so many wonderful things about Juneau, and also many things that could be improved. I'm interested in having thoughtful conversations with community members, learning about community needs, creatively designing possible solutions, and working toward implementation. I would like to be part of creating a more caring, harmonious, cooperative community.



I am also very interested in the committee on aging. I believe that healthy intergenerational relationships and involvement of elders is essential for community and individual health, for people of all ages. As a younger person, I feel I could make a positive impact by helping to facilitate intergenerational connections and understanding. Also, I have an elderly grandma I have spent time caring for, and I know there are many unique needs and challenges associated with aging, and I would like to help the community navigate those.

**Qualifications**

In addition to having the experience of growing up in Juneau, I am also an attorney. I think my skills with communication, writing, organization, research, and strategic thinking, as well as legal insight, could be useful to the committee.

I have also lived in many different places, such as: Anchorage, Fairbanks, Ketchikan, Los Angeles, Vermont, Geneva, Stockholm, and Detroit. This experiences gives me invaluable perspective and a sense for what is possible in different communities, and helps me think outside the box.

I am currently an attorney with Alaska Legal Services, where I serve low-income survivors of domestic violence. I have also worked as an advocate with foster youth. These experiences help me understand the needs of the most vulnerable people in our community. Before this job, I worked for the Alaska Supreme Court, which gave me experience in being fair, neutral, and considering all sides of issues in a nuanced way -- skills invaluable for working on committees.

**Civic Activities, Memberships or Non-profits Involved With**

I currently work for Alaska Legal Services.

**Working with a Group**

First, I listen for understanding, not to respond. I reflectively listen, trying to ensure people feel respected and heard. Second, I assume positive intentions; I assume we are all doing our best. Third, I focus on finding common ground and shared views and experiences. Fourth, I practice patience. Things often take more time when there are many viewpoints, and that's okay. I try to build that into the process. Finally, I use "I" statements and speak from the heart, remembering that we're all connected and there's no one right answer.

**Meeting Schedule and Attendance**

Yes

**Date of Birth (Optional)**

[Redacted]

**Ethnicity (Optional)**

White

**Gender (Optional)**

Male

**Acknowledgment/Certification**

Logan Miller

**Resume, Education, etc. (Optional)**

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## Links

[1] <https://juneau-ak.municodem meetings.com/bc/application>

Section R, Item 3.



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Submission information

Form: [CBJ Board Application](#) [1]  
Submitted by Visitor (not verified)  
Wed, 02/26/2025 - 9:43am  
158.145.15.25

**First Name**  
Matt

**Last Name**  
Catterson

**Residence Address**  
[REDACTED]

**Mailing Address**  
[REDACTED]

**Primary Phone Number**  
[REDACTED]

**Secondary Phone Number**  
[REDACTED]

**Email**  
[REDACTED]

**Board, Commission, Committee**  
Other Board

**Current of Prior Experience on CBJ Boards/Commissions/Committees**  
DD/WJ Area Plan Steering Committee (~4 years, resigned), Douglas Advisory Board (1 year), Douglas Community Garden Board (~5 years)

**Reasons for Applying**  
I am applying for the Comprehensive Plan Advisory Committee.

I am actively looking for ways to contribute substantively to our community. This committee interests me because I am very interested in planning and structural processes that guide land use and development in Juneau. I understand these plans to be the foundation upon which our community grows and matures, and they are critical opportunities for the public to provide guidance and actionable input to CBJ on how community development should occur. I place great importance on public participation and representation in these types processes, which I know from first hand professional experience can be very challenging. I strongly believe that AC and board members should serve to represent public opinion, rather than ad

personal agendas- this is important to me. I am a process and detail oriented person, and I know that preparation and hard work are necessary to provide good substantive input during public meetings.

**Qualifications**

I am a fisheries biologist for ADF&G and have been working for the State of Alaska for over 15 years. I have a Masters Degree in Fisheries Science which I received from UAF in Juneau. In my current job I coordinate programs that improve and diversify sport fishing opportunity and access through salmon enhancement (hatcheries) and through infrastructure development (boat ramps, docks, etc...). These programs require detailed planning and coordination with the public and other municipal and federal government agencies to be successful.

I have lived and worked in Southeast Alaska since 2003, in the communities of Yakutat, Sitka, and Juneau. My wife and I have owned our home in Douglas since 2019. My diverse professional and lived experience in these 3 communities in northern SEAK has greatly informed my understanding of the role of government and the important of public engagement in these types of processes.

**Civic Activities, Memberships or Non-profits Involved With**

Douglas Community Garden

**Working with a Group**

In my professional and lived experiences in Juneau, Sitka, and Yakutat, I have had numerous diverse experiences working in group settings with community members who often have conflicting perspectives- this is something we regularly encounter in my work in fisheries management. Navigating these situations, where folks have strongly held personal beliefs, can be very challenging. My approach to these situations centers around clear, respectful communication and being prepared so I can contribute meaningfully and with good information. I have come to understand that many disagreements or misunderstandings are due to poor communication, so I have worked purposefully to develop strong communication skills that help navigate these situations- often the first step is to really listen to other people. I strive to approach these situations with empathy, and try to really understand and appreciate perspectives that may be different from mine.

**Meeting Schedule and Attendance**

Yes, this is not a problem for me and support this type of accountability

**Date of Birth (Optional)**

[REDACTED]

**Ethnicity (Optional)**

caucasian

**Gender (Optional)**

male

**Acknowledgment/Certification**

Matt Catterson

**Resume, Education, etc. (Optional)**

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**Links**

[1] <https://juneau-ak.municodemeetings.com/bc/application>





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Submission information

Form: [CBJ Board Application](#) [1]  
Submitted by Visitor (not verified)  
Sun, 03/02/2025 - 10:20am  
24.237.22.189

**First Name**  
Travis

**Last Name**  
Eckhoff

**Residence Address**  
[Redacted]

**Mailing Address**  
[Redacted]

**Primary Phone Number**  
[Redacted]

**Secondary Phone Number**

**Email**  
[Redacted]

**Board, Commission, Committee**  
Other Board

**Current of Prior Experience on CBJ Boards/Commissions/Committees**  
N/A

**Reasons for Applying**  
I am applying to serve on the City and Borough of Juneau (CBJ) Comprehensive Plan Advisory Committee. The Comprehensive Plan provides the framework to support planning and land-use decisions within CBJ. As the largest community in Southeast Alaska and the State’s capitol, CBJ’s Comprehensive Plan also helps shape development across the region and potentially affects economic sectors across the state. CBJ is currently updating the comprehensive plan. Updating the Comprehensive Plan requires robust public engagement and input from technical experts to ensure the process results in feasible solutions based on the community's vision for the future. While on the Committee I would leverage my background in transportation infrastructure, geologic hazards, and community planning to evaluate the long-term benefits and implications of proposed updates to the Comprehensive Plan and provide unbiased feedback to the team.

**Qualifications**

I am a professional civil engineer with over a decade of experience in the transportation and infrastructure sector including the planning, design, and construction of capital improvement projects and asset management. I specialize in civil engineering with a focus on issues related to landslide response, recovery, and mitigation. As a former Haines Borough Planning Commissioner I have practical experience with the complexities of local planning, interpretation and implementation of comprehensive plans, and application of local land-use and zoning ordinances.

### **Civic Activities, Memberships or Non-profits Involved With**

Haines Borough Planning Commissioner, January 2023 - October 2023;  
Southeast Alaska Landslide Working Group Steering Committee Member, December 2024-present;  
USGS Landslide Response & Recover Interest Group Co-Leader; February 2025-present;

### **Working with a Group**

I regularly work on multidisciplinary teams with members from different communities, agencies, and organizations. My approach to working with a group is to listen first without judgement or bias. This helps develop an understanding of other's perspectives, concerns, and goals while fostering open communication. When confronted with challenges or disagreements I work towards mutually beneficial solutions by avoiding a "No, because..." approach and focusing on "Yes, if...".

### **Meeting Schedule and Attendance**

Yes.

### **Date of Birth (Optional)**

### **Ethnicity (Optional)**

### **Gender (Optional)**

### **Acknowledgment/Certification**

Travis Eckhoff

### **Resume, Education, etc. (Optional)**

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**Source URL:**<https://juneau-ak.municodemeetings.com/node/791/submission/152>

### **Links**

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Submission information

Form: [CBJ Board Application](#) [1]  
Submitted by Visitor (not verified)  
Mon, 02/24/2025 - 12:37pm  
24.237.25.73

**First Name**  
Tyson

**Last Name**  
Fick

**Residence Address**  
[REDACTED]

**Mailing Address**  
same

**Primary Phone Number**  
[REDACTED]

**Secondary Phone Number**

**Email**  
[REDACTED]

**Board, Commission, Committee**  
Other Board

**Current of Prior Experience on CBJ Boards/Commissions/Committees**  
NA

**Reasons for Applying**  
I'm applying for the Comprehensive Plan Advisory Committee because as a homeowner, business owner, parent, and super user of CBJ services such as Eaglecrest Ski Area, Trails, harbors, Melvin Park, and the Dimond Park Field House, I can offer valuable input on the potential future of our great city.

**Qualifications**  
I offer a history of state and private sector employment including the Alaska State Legislature and ASMI. More recently I have been commercial fishing and part owner of Yakobi Fisheries in Pelican and Juneau. I coach baseball, ski, and recreate all over the city and borough from downtown to outer areas like Hawk Inlet.

**Civic Activities, Memberships or Non-profits Involved With**  
Baseball

**Working with a Group**

I worked in fish politics including, ASMI Board and committees, United Fishermen of Alaska, Board of Fishery Management Council, Alaska State Legislature, and US Congress, for several years. If that doesn't qualify as groups with diverse and/or conflicting perspectives I don't know what does. I've learned it tends to be in everyone's best interest to have board members who can disagree without being disagreeable.

**Meeting Schedule and Attendance**

yes

**Date of Birth (Optional)**

[REDACTED]

**Ethnicity (Optional)**

oldish white guy

**Gender (Optional)**

male

**Acknowledgment/Certification**

Tyson Fick

**Resume, Education, etc. (Optional)**

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**Source URL:** <https://juneau-ak.municodemeetings.com/node/791/submission/143>

**Links**

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