



CITY OF HENDERSONVILLE
PLANNING BOARD - REGULAR MEETING
Operations Center|Assembly Room|305 Williams St.| Hendersonville NC 28792
Thursday, June 11, 2026 – 4:00 PM

AGENDA

1. **CALL TO ORDER**
2. **APPROVAL OF AGENDA**
3. **APPROVAL OF MINUTES**
 - A. Minutes of May 14, 2026
4. **OLD BUSINESS**
5. **NEW BUSINESS**
 - A. Administrative Review: Preliminary Site Plan– Sheetz Upward Rd (26-01-SPR) – *Matthew Manley, AICP / Long-Range Planning Manager*
6. **OTHER BUSINESS**
7. **ADJOURNMENT**

The City of Hendersonville is committed to providing accessible facilities, programs and services for all people in compliance with the Americans with Disabilities Act (ADA). Should you need assistance or an accommodation for this meeting please contact the Community Development Department no later than 24 hours prior to the meeting at 828-697-3010.

**Minutes of the Planning Board
Regular Meeting
May 14, 2026**

Members Present: Jim Robertson, (Chair), Lauren Rippy, Donna Waters, Betsey Zafra, Bob Johnson, Mark Russell, David McKinley, Kyle Gilgis, Tamara Peacock (Vice-Chair),

Members Absent: Laura Flores

Staff Present: Matthew Manley, Long Range Planning Manager, Lew Holloway, Community Development Director, Sam Hayes, Planner II, Angela Beeker, City Attorney

I Call to Order. *The Chair called the meeting to order at 4:00 pm. A quorum was established.*

II Approval of Agenda. *Ms. Gilgis moved to approve the agenda. The motion was seconded by Ms. Waters and passed unanimously.*

III Approval of Minutes for the meeting of March 12, 2026. *Mr. Johnson moved to approve the Planning Board minutes of the meeting of March 12, 2026. The motion was seconded by Ms. Rippy and passed unanimously.*

Approval of Minutes for the meeting of April 9, 2026. Ms. Gilgis moved to approve the Planning Board minutes of the meeting of April 9, 2026. The motion was seconded by Ms. Peacock and passed unanimously.

IV Old Business

V New Business

V(A) Conditional Zoning District – Carolina Village (26-25-CZD) Mr. Hayes gave the following background:

Mr. Hayes stated the City has received a conditional rezoning request for Carolina Village. This is, for those of you that don't know, Carolina Village is a retirement community located to the northeast of downtown Hendersonville. It borders Highway 26. The address is 600 Carolina Village Road. There are four separate parcels that are part of this rezoning. Three of them are R-15 zoned, and the main one, that is Carolina Village today is Planned Residential Development Conditional Zoning District. The total acreage for the property is 96.77 acres, and the proposal is to rezone this property to Urban Village Conditional Zoning District. The Future Land Use designation is Multi-generational Living. Mr. Hayes pointed out a few details on the site plan.

Mr. Hayes stated there are many different uses on the property. There's medical uses, there's restaurants. A bulk of it is residential. There's multifamily, single-family, and duplexes. Mr. Hayes pointed out where the proposed development is going on the site plan. He stated one of those three R-15 properties is located, directly adjacent to where the multifamily is being proposed.

A Neighborhood Compatibility Meeting was held on April 1st. There were four people present in person. There were four city staff and two from the applicant's team. The points of discussion were surrounding water runoff and adequate facilities for stormwater retention.

Mr. Hayes stated the applicant is proposing to rezone the entire property plus those three additional R-15 properties. However, in consultation with legal, we only reviewed the area around the new multifamily, which is located right here, for zoning compliance. So, things like building design, which there are design standards in the Urban Village Zoning District, the parking requirements, landscaping. The exception to that is that we did request for the tree canopy preservation to be calculated for the entire site. They meet that and exceed that requirement, as well as the open space requirement, which is calculated for the entire site. The entire site also met, also exceeded. And so, as we're thinking about this rezoning, we have just reviewed that area directly adjacent to the multifamily.

Site photos were shown and discussed and are included in the staff report and presentation.

The site plan was shown and discussed in detail and is included in the staff report and presentation.

Mr. Hayes stated the landscaping plan has been reviewed, it is compliant with our landscaping ordinance. The applicant is proposing, there's currently an existing single-family residential structure on the property. The applicant is proposing to demolish that structure and then plant some additional trees, and basically leave that property vacant. That will provide more of a buffer from just the buffer requirements that we have.

The developer has proposed one condition that would waive the TIA requirements, so in Urban Village, you have to do a TIA. However, the developer is arguing that the trip generation will be below the 100 peak hour trips and 1,000 daily trip threshold, that would trigger a TIA in other zoning districts, and then they also mentioned that many of the trips will be by golf cart. City staff has no issues with this developer proposed condition.

The city had no proposed conditions.

The Future Land Use map was discussed and is included in the staff report and presentation.

General rezoning standards for Comp Plan Consistency were discussed and are included in the staff report and presentation.

A draft Comprehensive Consistency statement was shown and is included in the staff report and presentation.

A draft reasonableness statement for approval and denial was included in the staff report and presentation.

Chair asked if there were any questions for staff.

Chair asked if staff could point out the area that was zoned R-15. Mr. Hayes pointed this area out on the site plan. He pointed out the three parcels that were R-15. Chair asked if the single family homes on those lots would be demolished. Mr. Hayes stated the applicant can speak to that. They did say that one of the homes would be demolished.

Chair stated and the entire R-15 that's currently zoned on the property is in Multi-generational Living on the comp plan. Mr. Hayes stated yes.

Mr. Johnson asked if the TIA plan, is that based on this project, normally mandated by the size of it? Is this normally waived? Mr. Hayes stated the Urban Village Zoning District that they are requesting to be rezoned to does have a requirement. So the condition is to waive that requirement in the Urban Village Zoning District. The other way that we trigger a TIA is through measuring the number of trips generated, and so that's the 100 peak hour trips, or the 1,000 daily trips. So there are two ways that you can trigger a TIA. So if it's in your zoning district, then you have to do it, unless you get a condition. And then if it triggers with those peak hour trips, then you also may have to do it. So if it's not in your zoning district, but you trigger it with those peak hour trips, then you would have to do a TIA, unless you get a waiver of some sort. Mr. Johnson asked so has one been done? How do we know the number of trips? Mr. Hayes stated so the trip generation, I will let the applicants speak to the trip generation component of this. The way that is done is, you don't do a whole study on that. There are calculations that you do in order to calculate those based off of the number of units that you proposed, the uses that you're proposing to do. I'll let the applicant speak to those calculations in more detail. Chair stated but an 80-unit apartment would not normally generate that number of trips. Mr. Hayes stated he did not think so.

Chair asked what about creating nonconformity, you mentioned that the only thing the city really looked at was the site where the 80-unit apartment's going to go. By rezoning it to Urban Village, would it create nonconformity throughout the project, the rest of the project? Mr. Hayes stated so, the short answer is potentially. We did not do a comprehensive look at every single aspect of this project. The way that that would work is if you all and then ultimately City Council rezoned, this property, and actually if this was ultimately rezoned, there's a threshold that you have to meet in order to bring a nonconforming structure into compliance with our zoning ordinance. And so, if they pass that threshold, then they would have to meet the Urban Village standards. That means it could have to come back to you all. If it's a major modification of the conditional zoning, then they may have to come back and have it reviewed again. If it's not a major modification, which the threshold for that is 10%, then staff could review that, but we would be reviewing it under the Urban Village standards.

Chair stated I guess where I'm going with that is, isn't there a state law that says you can't create nonconformity through rezoning. Lew Holloway, Community Development Director stated yes, there is. We cannot do that unilaterally. In this case, both the property owner and the city are requesting the rezoning. Even in a standard rezoning, if the applicant consents, we can downzone and create a nonconformity. That's what's happening in this case as well.

Ms. Peacock stated we don't have any obligation to notify them that they're doing that. Mr. Holloway stated staff talked with them about that.

Chair asked if the applicant plans on turning this all into one parcel. Mr. Hayes stated he did not know but that could be a great question for the applicant.

Mr. Hayes noted the one parcel that is serving as the extra buffer, is currently going through the annexation process. So, it's currently in the ETJ, if City Council ultimately approves it, then it would be in the city. Chair stated the other two parcels are already in the city? Mr. Hayes stated that's correct.

There were no further questions for staff.

Chair asked if the applicant would like to address the Board.

Kevin Parries stated he is the CEO of Carolina Village. He has been there for 23 years, and so he practically lives at 600 Carolina Village Road, even though that's not his primary residence. He stated

Carolina Village was founded back in the 60's on a vision of one individual that thought about who was going to take care of her when she grew older in Henderson County. There was actually no retirement living or any type of residential living for older adults at that time, so she took her idea to her physician that she trusted really well, Dr. Ken Cosgrove. He took that to the Chamber of Commerce and that process took almost a decade, because we opened our doors in 1974 after their 10-year research and pre-sales, which is part of this project that we had on our campus. We do have the full continuum of care. We have independent living. We do provide an independent living support, so those individuals can stay in their apartment or their cottage longer than they necessarily would need healthcare. Independent living would then move over to an assisted living complex, which we have 60-bed, private room assisted living on our campus and a medical center, which is a skilled nursing facility, which we do rehabilitation as well. Part of that, residents have full guarantee to those access in and out of that continuum as they desire and as they want to have that, as part of their contract in living at Carolina Village. We are a 501C3 not-for-profit. We are one of 69 CCRC's in the state of North Carolina. We're the only Type A retirement community, identified by the Department of Insurance and the Special Entity Program through the General Statutes in Henderson County. There are other retirement assisted livings and skilled nursing. We're the only ones that have that distinction here, and we've been very proud of that for the over 50-year history. Part of our obligation not only to our residents, but to the greater community. This is just some of our outreach that we do. We partner with over 85 other not-for-profits to make Henderson County stronger over the time periods that we all are part of living here and working here.

Mr. Parries stated and then today, we actually have 556 residents that live at Carolina Village. We have 390 employees. We're very proud to be one of the top 15 employers in Henderson County. We have 98 acres, that includes the two acres that is part of what we asked to annex into the city, that we just purchased about two months ago. That's that piece that you saw that Sam brought out and identified. We have 135 independent living cottages and currently 250 with those assisted living and skill beds. Our youngest residence is 65. You must be under the General Statute in the state of North Carolina, 62 years of age, to live at Carolina Village. But our oldest resident is 104, so we serve almost five decades of individuals. We now have over a thousand people on a waiting list to move in, and they put their name on that waiting list, and they have first rights to select the units on that end of it. And we're also very proud, until Sierra Nevada came into town, we were the largest kitchen, now we're the number two sized kitchen, and we do those community outreach programs, like the congregate meal program for the Council on Aging and those other organizations here in town, and working with different agencies to help make sure that we meet the needs here in the community. So I'm going to turn it over to our architect, and he will kind of share our project. We're very excited about this project moving forward so we can meet the needs of our Henderson County residents.

Bud Hart, architect with SFCS stated his name for the record. He stated they have been working with Carolina Village for over a year developing this project. It is a 60-unit independent living that they are developing for them. They have units in this particular building that are going to be anywhere from one-bedroom, around 1,100 square feet, up to 1,800 square feet. They have about fourteen of those. The workhorse of this building is going to be about a 1,400 square foot, two-bedroom, two-bath. They are trying to attract a sort of younger generation of people. It's a modern-type building. On the site that they were offered, it is sitting up a bit on a hill, but cascades sort of down as you follow Carolina Village Road. So, respectful of the site, we tried to also have our building stepping down. That offered us an opportunity to not only develop a wellness center that Carolina Village wanted to see brought onto this particular site, but also the possibility of a rooftop terrace, that would be an amenity that all of the people from the campus could use. This building is intended to be a destination on the campus, because it's not connected to the main legacy buildings that are there right now, or the kitchens or commons. We're hoping that this will draw people also to another part of the campus that they'll want to visit that, and come and not only use the upper terrace, which has amazing views to the west, looking out towards the mountains, but also to the south. But then also use the fitness center to increase the trend of wellness that we're seeing a lot happening as we're moving through CCRC's and developing these types of buildings.

Tom Jones, with WGLA Engineering stated his name for the record. They are the Civil and Site Engineers for the project. He stated the one good thing about the master plan in this case, this plan really does show the buffering from the project to the Country Acres, the neighboring subdivision. So with that parcel that's being annexed, you really can see the large amount of buffering that's going to be provided. He discussed the tree canopy preservation, which he thinks is a really important component, we're still going to have 91% of the existing tree canopy preserved after this project is complete, where 30% would be the requirement. The project does have the Urban Village components, like there's going to be a seven-foot sidewalk along Carolina Village Road. There'll be planting beds and a pergola for the common space along the road. There's golf cart parking, and we're going to have a couple drop-offs.

Mr. Jones stated stormwater management was a concern for some of the neighbors at the Neighborhood Compatibility Meeting, and in this case, we're proposing a subsurface stormwater chamber system to control and treat the runoff per the city's typical requirements. In addition to that, the outlet for the system would be directed away from Country Acres. It'll be going towards the north to the existing wet pond that's already on the campus. So the flow, the water will actually be treated twice, because it'll be treated as part of this project, but then, ultimately, it's going north to the existing pond, and the pond handled Hurricane Helene, with flying collars. There was no issues whatsoever, so we feel really good about stormwater management for the project. This project provides additional housing for Carolina Village, and therefore the City of Hendersonville, which is needed, the senior housing in this case. It's providing adequate buffers in terms of the landscaping, preserving the tree canopy. It's going to provide proper stormwater management. Staff has identified that it meets the city standards established by the Urban Village zoning, and we believe it's consistent with the range of goals in the Gen H plan, and we'd appreciate your approval.

Mr. Jones stated in the Urban Village you are required to do a TIA no matter the size of the building. Most of the zoning districts in the city do not require outright. It's based on the number of actual trips. Urban Village and Urban Residential have the automatic component. He stated 100 peak hour trips is one of the triggers or 1,000 daily trips generated. So if your project generates one of those two peaks, you would have to do a traffic impact analysis. The calculation we provided, signed and sealed, to the city, shows this project would generate 20 peak hour trips in the AM, in the morning peak, and 15 peak hour trips in the PM, the evening peak. So it's like a fifth of the trips that would trigger a TIA, by the normal requirements. The daily trips is 1,988, which is also about a fifth of the 1,000, so it's well below the normal triggers. The other thing is, a lot of the trips in this case will be internal, because we have the facilities on the campus, so you could drive, if you needed to, or wanted to, to the dining hall, or to the restaurants, and so you're not even generating a trip off campus. So these numbers I just gave you don't even capture those numbers, so it's actually lower than that. It also doesn't capture the fact that some of these trips will be by golf carts. So, that's the justification for believing it's very reasonable to waive a requirement for a TIA.

Chair asked if anyone had any questions for the applicant.

Ms. Zafra stated I know there's going to be a Clear Creek Greenway right there. So, will the residents be able to go on the golf carts to the trail? Mr. Parries stated yeah, so we are actually very excited about the Greenway project to be done. So, if you kind of look right here, we have what we call the clubhouse, and there is that road, there's a stormwater road that we work with the city. There's a sidewalk that will go down to that greenway that will connect them to that. There's another entrance here at the very end of this building, which is at the end of our campus, which is our back entrance gate off of Clear Creek. There's access there to the right as soon as you come out that gate, to get onto that greenway. We're ready for that to happen. It got delayed because of Helene, so we're excited about that, and our residents are ready for it.

Chair stated the intersection of Four Seasons Boulevard and Carolina Village Road and Orrs Camp Road is obviously a disaster. It takes a long time, it backs up all the time. Talk about the gate at Clear Creek Road. Who can go in and out of that gate? Mr. Parries stated the entrance of the gate of the back of Carolina Village, that is for staff and employees only, so we encourage them to actually utilize that to come

through. It is a barcode system, so they would have to go in through that barcode. You can't push a code or reach the man gate, which is at the front of that.

Chair asked how many residents are there? Mr. Parries stated there's currently 556 residents. Chair stated they only have one way in and out. Mr. Parries stated they have two ways, they come off of Four Seasons Boulevard or the Clear Creek Road. Chair stated so they can come in off of Clear Creek Road? Mr. Parries stated they can come in and they can go out off of Clear Creek Road as well. Chair stated residents? Mr. Parries stated yes. Chair stated okay, because I thought you just said it was for staff. Mr. Parries stated it's for staff and residents. It's not for general public. The fire department and police and EMS can get through there. They have a code, but they also have siren detection on those gates that it will open. Chair stated every resident has the ability to enter or exit at Clear Creek. They don't have to go out Four Seasons. Mr. Parries stated yes.

Chair asked about the stormwater getting treated twice. Mr. Jones explained this.

Chair asked about the property just recently acquired and asked if the structures would be torn down. Mr. Parries stated we don't have any plans or any goals to do that. There is a single-family dwelling and some storage on there. That was a rental home, and so we're going to plan to demolish that, and that's where we would like to put up additional trees to buffer between Carolina Village and the Country Acres.

Chair stated it seems like a piece of property with structures on it would be fairly expensive to purchase just to buffer. Mr. Parries stated we purchased the five acres that this part of this property was on about ten and a half years ago. There were some estate issues, and so we went through the estate to get that with their successors that own that piece of property just the last couple of months. We just did that just so we can provide privacy for Carolina Village, as well as Country Acres.

Chair asked are you planning to turn this all into one parcel? Mr. Parries stated our goal is we would like to make this all one parcel, ultimately.

Chair asked what the height of the structure would be. Mr. Jones stated 56 feet is what they calculated. Mr. Parries stated the building is four stories.

Discussion was made on the tree canopy for the 5.19 acre site. Mr. Jones stated it is a combination of preservation plus plantings on the five acre site.

Chair opened public comment.

Brett Werner, 706 Goldfinch Drive stated I work for the state of South Carolina, I sit on the City's Board of Adjustment, and I am a candidate for City Council. These comments are my own, do not reflect those of my employer, my fellow board members, nor reflect my campaign. Carolina Village is undoubtedly an asset to our community, but as a city, we have to look at the fiscal reality of how we grow, and how we pay for that growth. Currently, the Carolina Village campus sits at an assessed value of roughly \$112 million. Because of its statutory non-profit status, that entire footprint is completely off of our municipal tax roll. To put that in perspective, Carolina Village alone accounts for over one-third of the City of Hendersonville tax-exempt properties. Last year, the city bypassed over \$580,000 in potential property tax revenue from this single campus. Since its inception, we're talking about tens of millions of dollars. Now, they were asking to rezone and expand. With expansion comes impact. More density means more stormwater runoff to manage. It means increased wear on our local infrastructure. In fact, to your point, the intersection of Four Seasons Boulevard and Carolina Village Road is already ranked by the Hendersonville Police Department as one of the top five crash locations in the entire city. I understand the limits of North Carolina law, and that under Chapter 160D, the board, nor City Council, nor staff can legally mandate a payment in lieu of taxes or a pilot as a condition for rezoning. However, the law does allow for the developer to voluntarily offer one. If

Carolina Village brings forward this massive expansion without voluntarily offering a pilot agreement, they are taking the City of Hendersonville and our municipal services for granted. They are asking to benefit from the safety, infrastructure, and resources of the city while passing the check on to everyone else. While the expansion of the scale pays zero into the tax base, the cost of its services it consumes doesn't just disappear. The financial burden shifts directly onto the shoulders of everyday taxpayers. A true community partner recognizes the strain they place on the system and steps up to offset it. I asked the Planning Board to press the applicant on this issue, and if Carolina Village wants the privilege of an Urban Village Conditional Zoning District, they should voluntarily commit to a pilot agreement to help fund the city's services they rely on.

Chair closed public comment.

Chair stated based on the public comment, can rezoning ever force a non-profit to pay property tax? Angela Beeker, City Attorney stated you can ask. Remember, we were talking about that in the training about the like to have, but, or what you can require? This, you can't require. So if you ask and they said no, you couldn't turn it down on that basis. Chair stated so they could ask for a condition. Ms. Beeker stated you can ask, but if they say no, you can't hold it against them. Ms. Beeker stated they are not exempt from impact fees. They are only exempt from property tax.

Mr. Parries stated Carolina Village does not pay property tax, and we do have that privilege and honor to be able to get that. So we actually do partner with the Hendersonville Fire Department, because we do utilize their services from the city a lot, and so we actually partner with them on many different things. For example, we offer 100% training for all their staff, so we pay tuition in their books for those that would like to do that, and we have done that for about 60 city fire departments, so that helps not just to keep the city from having to pay those, or the individual fire department to be able to do it. We also help buy their equipment, those type of items. The Police Department, thank goodness we don't have to use a lot, but we do kind of partner with them in different aspects on our campus, and through the city as the needs arise. But the other services from the city, we actually do ourselves. We maintain our own roads, we do our own trash, those type things, so we don't ask for the city to provide those services. That would be part of the things that the property tax would help pay for. And we do pay the Hendersonville City for any type of utilities and those type of things, and we pay for those services, water, sewer, gas.

Ms. Zafra moved the Planning Board recommend City Council adopt an ordinance amending the official zoning map of the City of Hendersonville changing the zoning designation of the subject property (PINs: 9579-18-3554, 9579-08-2767, 9579-37-5327, and 9579-37-6200) from PRD CZD (Planned Residential Development Conditional Zoning District) and R-15 (Medium-Density Residential) to UV-CZD (Urban Village Conditional Zoning District) based on the site plan and list of conditions submitted by and agreed to by the applicant [dated 3/24/26], and presented at this meeting and subject to the following: 1. The development shall be consistent with the site plan, including the list of applicable conditions contained therein, and the following permitted uses:

- a) Adult care centers registered with the NC Department of Health and Human Services***
- b) Accessory uses & structures***
- c) Dance, health & fitness facilities***
- d) Neighborhood community centers***
- e) Offices, business, professional and public***
- f) Personal services***
- g) Restaurant***
- h) Residential dwellings, single/two/multi-family***

2. The petition is found to be consistent with the City of Hendersonville Gen H 2045 Comprehensive Plan based on the information from the staff analysis and because: The petition is consistent with a range of Goals, Guiding Principles, and the Future Land Use Designation of Multi-Generational Living as defined in Chapter IV of the Gen H Comprehensive Plan. 3. We find this petition to be

reasonable and in the public interest based on the information from the staff analysis, public hearing and because: 1. The petition provides additional housing. 2. The petition provides an adequate buffer between the multi-family and single family housing on adjacent properties. 3. The petition will adequately address stormwater through the use of existing stormwater retention facilities. Ms. Gilgis seconded the motion which passed unanimously.

VI Other Business – Ms. Beeker stated the Housing Summit would be May 27th at Hendersonville High School at 5:30 PM.

VII Adjournment – *The meeting was adjourned at 4:49 pm.*

Jim Robertson, Chair

DRAFT

The proposed project would establish a southern leg to the intersection of Upward Rd at Ballenger Rd with the creation of a new access street. This street is intended for public dedication to the City of Hendersonville. The remainder of the proposed Sheetz development is private. NCDOT is requiring that future traffic that ties into this intersection from any additional development will be responsible for the construction of a roundabout at the intersection.

Once Preliminary Site Plan approval is granted, the applicant will be responsible for submitting a Final Site Plan for review. Upon issuance of a Final Site Plan approval, construction of the proposed development may commence.

PROJECT/PETITIONER NUMBER:	26-01-SPR
PETITIONER NAME:	<ul style="list-style-type: none"> • Wes Hall of Sheetz and Jesse Hanlin of Blue Ridge Engineering [Applicants] • Quattlebaum Properties, LLC. [Owner]
ATTACHMENTS:	<ol style="list-style-type: none"> 1. Staff Report 2. Preliminary Site Plan Packet (with staff mark-ups) 3. Preliminary Lighting Plan 4. Preliminary Building Elevations 5. NCDOT TIA Comments 6. Developer TIA Supplement 7. Application / Owner Signature Addendum

PRELIMINARY SITE PLAN-SHEETZ UPWARD RD (26-01-SPR)
CITY OF HENDERSONVILLE - COMMUNITY DEVELOPMENT STAFF REPORT

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PROJECT SUMMARY

- Project Name & Case #:
 - Sheetz Upward Rd
 - 26-01-SPR
- Applicant & Property Owner:
 - Jesse Hanlin, Blue Ridge Engineering, PLLC [Applicant]
 - Wes Hall, Sheetz [Applicant]
 - Robert Quattlebaum, Quattlebaum Properties, F LLC [Owner]
- Property Address:
 - 0 Upward Rd
- Project Acreage:
 - 2.65 acres
- Parcel Identification (PIN):
 - 9588-40-7325
- Parcel Zoning:
 - CHMU, Commercial Highway Mixed Use
- Future Land Use Designation:
 - Mixed Use Commercial
- Requested Uses:
 - Convenience Store with Gas Sales
- Type of Review:
 - **Administrative**- Decisions made in the implementation, administration, or enforcement of the Zoning and Subdivision Ordinances that involve the determination of facts and the application of objective standards set forth in the Zoning and Subdivision Ordinances.



SITE VICINITY MAP

The City of Hendersonville is in receipt of an application for site plan review from Quattlebaum Properties, LLC, property owner, and Wes Hall of Sheetz and Jesse Hanlin of Blue Ridge Engineering, applicants. The applicants are proposing to construct a 6,139 sq. ft. convenience store on PIN 9588-40-7325 located on Upward Road at Ballenger Rd on approximately 2.65 acres. The property is currently zoned CHMU, Commercial Highway Mixed Use.

The subject property is the first parcel to be proposed for development out of a larger 16-acre tract and 15-acre tract owned by the same property owner. These adjacent tracts have been graded and have access to public utilities.

The proposed project would establish a southern leg to the intersection of Upward Rd at Ballenger Rd with the creation of a new access street. This street is intended for public dedication to the City of Hendersonville. The remainder of the proposed Sheetz development is private. NCDOT is requiring that future traffic that ties into this intersection from any additional development will be responsible for the construction of a roundabout at Ballenger Rd..

Once Preliminary Site Plan approval is granted, the applicant will be responsible for submitting a Final Site Plan for review. Upon issuance of a Final Site Plan approval, construction of the proposed development may commence.

PRELIMINARY SITE PLAN AND MINOR PRD REVIEW GUIDELINES

Review of Preliminary Site Plans

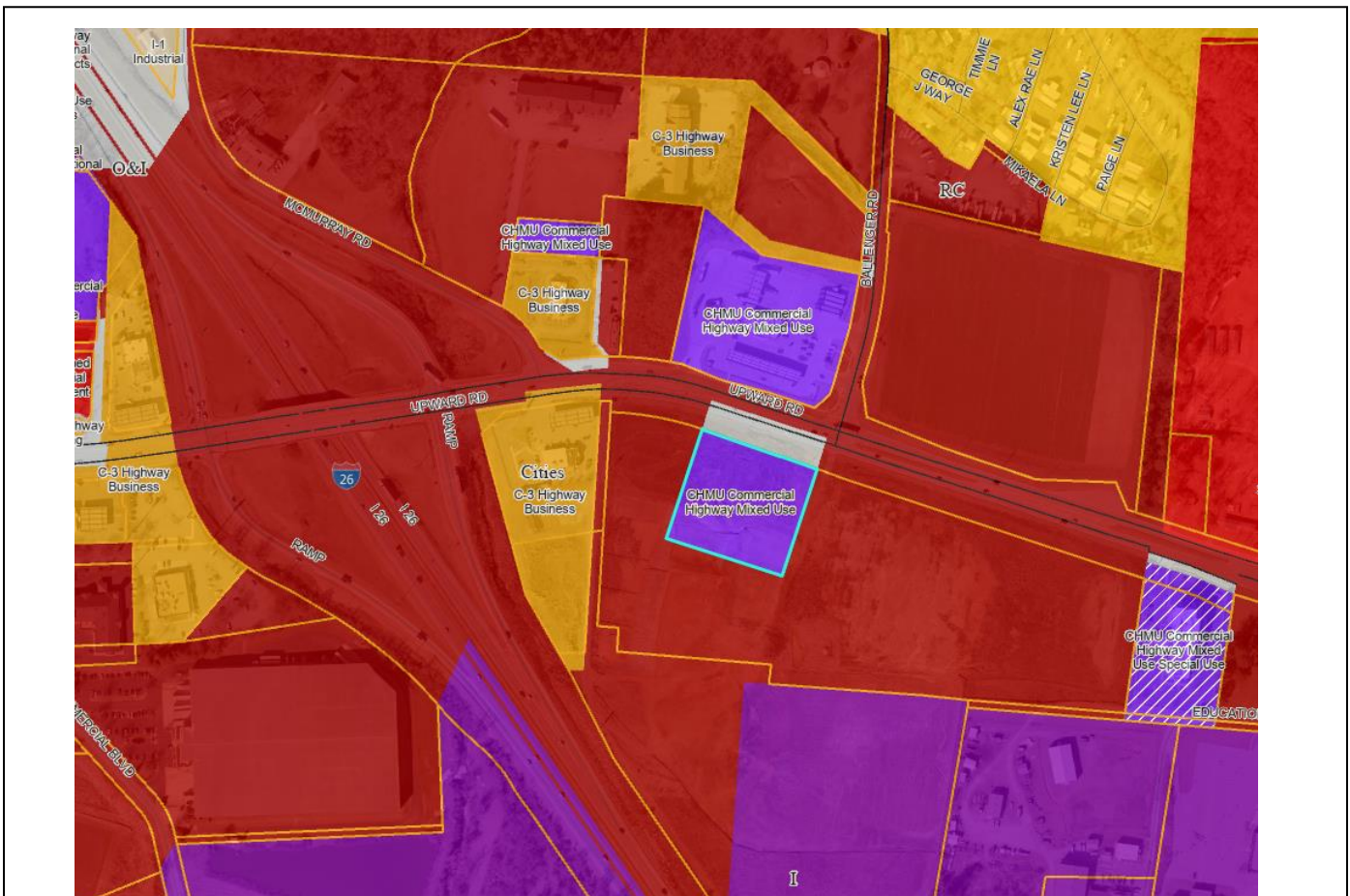
Review of preliminary site plans applies to larger projects where the costs of developing detailed final plans can be substantial. Review of preliminary plans is a function of the Planning Board.

Preliminary plan review is required of all development undergoing Site Plan Review for the following:

- Any commercial, industrial or institutional development consisting of more than 20,000 square feet of floor area.
- Addition of more than 30 parking spaces.
- Any Minor Planned Residential Development consisting of 9 or more dwelling units.

Approval of a preliminary site plan shall **not** entitle the applicant to the issuance of a zoning compliance permit. Upon approval of the preliminary site plan by the Planning Board the applicant must then submit an application and final site plans meeting the requirements of of the zoning ordinance, in addition to any other approval including but not limited to stormwater approval, utility approval, driveway permits, land disturbance approval, etc. The applicant shall have two years from the date of such approval to obtain final site plan approval.

EXISTING ZONING & LAND USE



City of Hendersonville Current Zoning & Land Use Map

The subject property is zoned CHMU as is within the city limits of Hendersonville by way of satellite annexation. CHMU zoning along with C-3 zoning is found elsewhere along the Upward Rd corridor as a result of sporadic annexations. The remaining zoning in the area is under the jurisdiction of Henderson County. Regional Commercial (red) is the most prevalent zoning with Industrial (purple)

SITE IMAGES



View of Site Frontage facing west on Upward Rd from proposed new access road



From Southwest corner of site the property drops off to vacant parcel to the west

SITE IMAGES



View of remaining vacant tract to the south of site



View from site facing east towards additional vacant parcel and existing development in background

PROPOSED DEVELOPMENT DETAILS

- **Proposed Use:**
 - Convenience Store with Gasoline Sales

- **Acres**
 - 2.65 acres

- **Natural Resource**
 - Site has been cleared and graded.
 - Blue Line Stream shown on USGS map will need Jurisdictional Determination or piping

- **Streets**
 - Site Fronts on Upward Rd with new access street tying in to intersection with Ballenger Rd.
 - The site plan proposes a stub-out to adjacent property in the northwest corner of site near Upward Rd.
 - The site plan proposes a Right-In turn off of the proposed access street
 - The site plan proposes a full movement turn near southeastern corner of site.

- **Parking**
 - Required = 31 Spaces
 - Provided = 44 Spaces

- **Sidewalks**
 - The site plan illustrates sidewalks along Upward Rd and the new access street with connections to the building entrance

- **Public Utilities**
 - The site will be served by City of Hendersonville Water and Sewer.

- **Landscaping**
 - Plantings required for vehicle use area (perimeter plantings, planting strip, and street buffer), open space, common space, and street trees for a total of 64 trees and 284 shrubs.

OUTSTANDING PRELIMINARY SITE PLAN COMMENTS

COMMUNITY DEVELOPMENT

Preliminary Site Plan Comments:

The preliminary site plan accompanying this petition meets the standards established by the Zoning Ordinance for Commercial Highway Mixed Use (5-27), General Provisions (Article VI), Landscaping (Article XV), and Preliminary Site Plan (7-3-3) with the exceptions listed below.

Staff has included Conditions for Planning Board to consider that would require the developer to come into compliance with any deficiencies that are not granted a Variance.

Deficiencies:

Article V – Zoning District Standards

1. The Landscaping Plan lacks a note stating that Common Space trees are required to be a minimum of 3” in caliper.
 - o Code Reference:
 - 5-27-4.1.1 b) Trees. One tree shall be planted for each 1,000 square feet of common space. Trees shall have a minimum caliper of three to three and one-half inches measured six inches above ground at the time of planting.
2. The required stub-out at northwest of site is shown to be 5’ short of property line and must abut property line.
 - o Code References:
 - 5-27-5.2 Vehicular access. It is the intent of these regulations to create interconnectivity between development parcels in order to provide alternative transportation routes to existing thoroughfares. Accordingly, when development parcels have potential to provide access between major arterial roadways and adjoining parcels, developers are encouraged to utilize public streets, rather than private streets or driveways. The regulations contained in this section shall apply to all new development and redevelopment within the district.
 - Sec. 5-27-5.2.4 c) Non-residential developments shall provide one stub-out to each adjoining parcel where, considering topography, land use compatibility and future development or redevelopment potential, it is deemed feasible and appropriate.
3. Corner clearance between Upward Rd and right-in entrance proposed on new access street is 80’. Minimum Corner Clearance for side street is 100’ from the point of tangency of the radius curvature of the intersection streets. Furthermore, the insufficient corner clearance would be exacerbated by future construction of a roundabout at Upward Rd @ Ballenger Rd. Applicant intends to seek a variance for this deficiency from the Board of Appeals.
 - o Code Reference:
 - Sec. 5-27-5.2.1 Driveways. Where permitted, driveways shall comply with the requirements of this section and, if applicable,

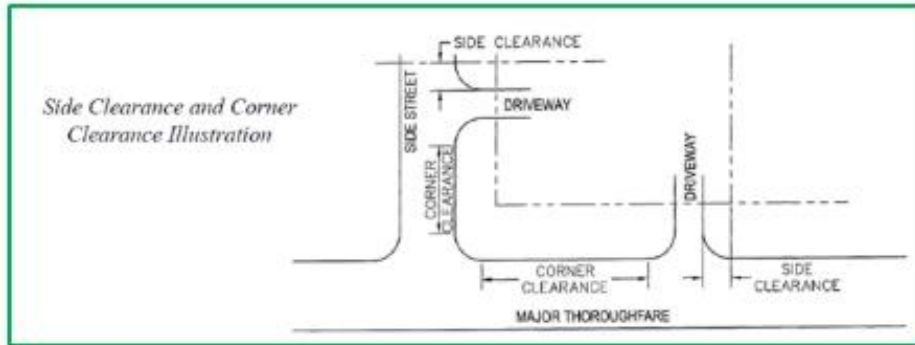
the regulations of the North Carolina Department of Transportation.

b) All driveway approaches for both mid-block and corner lots along major thoroughfares shall have both minimum corner and side clearances as below:

Clearance Type	Along Major Thoroughfare	Along Side Street
Corner clearance ²	250 feet	100 feet
Side clearance ³	30 feet	10 feet

² Corner clearance shall be measured from the point of tangency of the radius curvature of the intersection streets.

³ Side clearance shall represent the distance from the driveway to the side property line.



Article VI – General Provisions

- 4. The Sidewalk along the access street at the southeast corner is 41’ short of the property line. It must be extended to the edge of property line.
 - o Code Reference:
 - Sec. 6-12-2 “In all other [non-residential] zoning district classifications sidewalks shall be required for new construction from property line to property line. In the case of corner lots, sidewalks shall be provided along both streets.”

Article XV. Buffering, Screening and Landscaping

- 5. The landscaping plan shows only 6 trees proposed to be planted in islands. The number of Vehicular Use Area trees required to be planted in islands is 11. Additionally, the islands are to be evenly distributed in the parking lot.
 - o Code Reference:
 - Sec. 15-9. – Landscaping for vehicular use areas. a) Perimeter and interior plantings...When a parking lot contains 20 or more parking spaces, 50 percent of the trees and shrubs required ... must be planted in islands or medians located within the parking lot. Tree islands shall be evenly distributed throughout the parking lot in order to provide an even tree canopy throughout the lot. At a minimum, such tree islands shall consist of an area at least equal in size to two parking places side-by-side. Parking bays shall be broken up with landscaped islands or medians to avoid long monotonous rows of parking.

- 6. Street Trees are required along the proposed access street, but they are not shown in the landscaping calculations.
 - Code Reference:
 - Sec. 15-15. - Street trees. Street trees shall be required at the rate of one tree for every 35 linear feet of property abutting a street.

CITY ENGINEER

Preliminary Site Plan Comments:

- None

WATER / SEWER

Preliminary Site Plan Comments:

- None

FIRE MARSHAL

Preliminary Site Plan Comments:

- None

STORMWATER ADMINISTRATOR

Preliminary Site Plan Comments:

- None

FLOODPLAIN ADMINISTRATOR

Preliminary Site Plan Comments:

- None

PUBLIC WORKS

Preliminary Site Plan Comments:

- None

TRANSPORTATION IMPACT

Preliminary Site Plan Comments:

- A TIA was triggered and reviewed by NCDOT. At the time the TIA was performed the property was not within the jurisdiction of the City of Hendersonville and therefore the TIA was not reviewed by the City.
- NCDOT initially requirement installation of a roundabout
- A traffic signal was later approved on the condition that Sheetz dedicate ROW for a future roundabout and that any additional traffic that is added to the intersection at Ballenger Rd will be responsible for the construction of a roundabout.

CIVIL ENGINEER
 BLUE RIDGE ENGINEERING PLLC
 W. JESSE HANLIN, PE
 924 MAIN STREET, SUITE 200
 NORTH WILKESBORO, NC 28659
 (336) 838-2500

HIGHWAYS
 NCDOT DIVISION 14, DISTRICT 1
 NICK DORATO
 ASSISTANT DISTRICT ENGINEER
 4142 HAYWOOD ROAD
 MILLS RIVER, NC 28742
 (828) 435 7297

SURVEYOR
 SURVEYPROS PLLC
 JASON WALL, PLS
 924 MAIN STREET, SUITE 300
 NORTH WILKESBORO, NC 28659
 (336) 667-5533

EROSION CONTROL
 HENDERSON COUNTY
 DEB JOHNSTON, PE
 100 N. KING STREET, SUITE 200
 HENDERSONVILLE, NC 28792
 (828) 694-6553

PLANNING & ZONING
 CITY OF HENDERSONVILLE
 MATT MANLEY
 160 6TH AVENUE EAST
 HENDERSONVILLE, NC 28792
 828-697-3064

POWER COMPANY
 DUKE ENERGY
 957 SPARTANBURG HWY
 HENDERSONVILLE, NC 28792
 (704) 866-6823

STORM WATER
 CITY OF HENDERSONVILLE
 MIKE HUFFMAN
 305 WILLIAMS STREET
 HENDERSONVILLE, NC 28792
 (828) 458-5693

NATURAL GAS
 ENBRIDGE NATURAL GAS
 BARRY MANN
 15 OVERLAND INDUSTRIAL BLVD
 ASHEVILLE, NC 28806
 (828) 712-8925

WATER & SEWER
 CITY OF HENDERSONVILLE
 BRENDAN SHANAHAN, PE
 305 WILLIAMS STREET
 HENDERSONVILLE, NC 28792
 (828) 697-3000

Sitework Plans for a new



Upward Road
 Hendersonville, NC
 Henderson County

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ORIGINAL ISSUE DATE:	12/15/2025
REVISIONS:	A 02/27/2026 ADDITIONAL GRADING B 03/06/2026 REMOVE CAR WASH/SHARED POND C 07/02/2026 SHEETZ COMMENTS D 09/13/2026 PROGRESS SET E 04/23/2026 REVISED PERMIT SET F 05/19/2026 REVISED SITE PLAN FOR PER APPROVAL G 09/21/2026 PLAN REVISIONS PER CITY/COUNTY H 05/28/2026 PLAN REVISIONS PER CITY/COUNTY
PROJECT #	24039
DRAWN BY:	WH

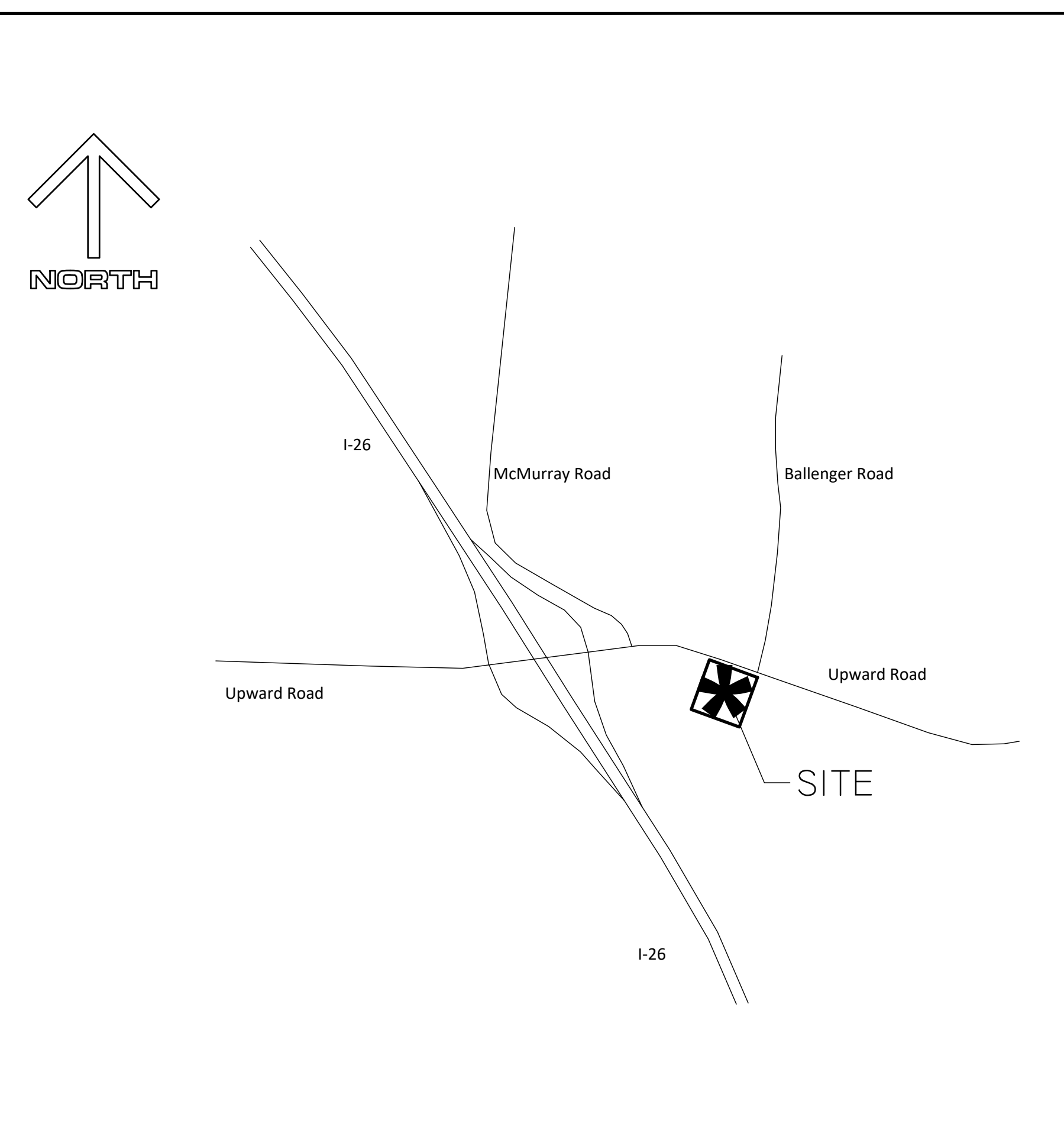


Blue Ridge Engineering PLLC
 924 Main Street - Suite 200
 North Wilkesboro, NC 28659
 336.838.2500
 www.ridge.blue
 Firm #P-0205

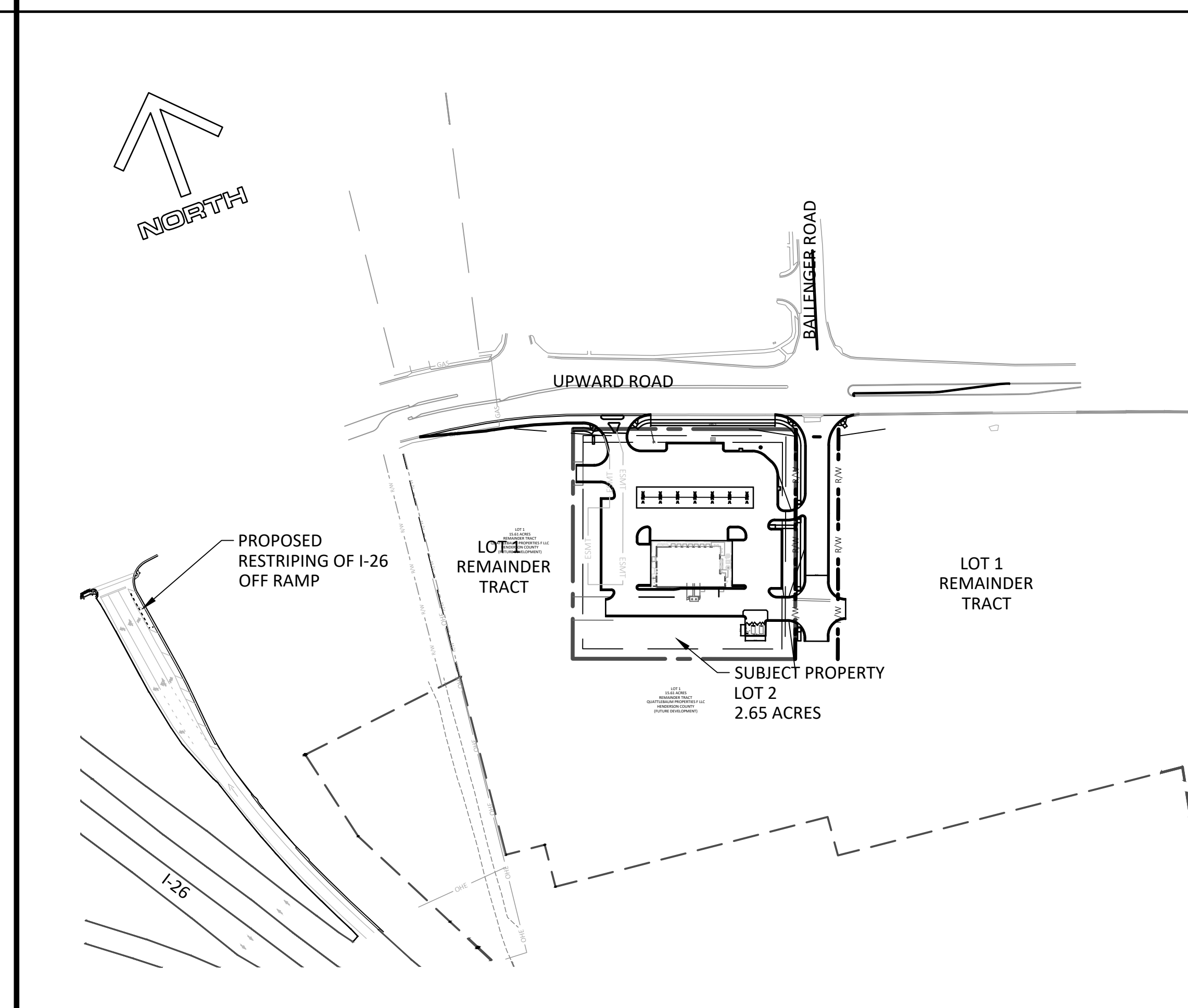
Project Narrative

This project consists of development of a 2.65 acre tract located on Upward Road in Hendersonville, NC. The proposed development includes a new 6,139 sf Sheetz Convenience store with drive-thru, fueling canopy, and a 1,122 sf car wash. Development will also include a new shared access drive, stormwater pipe system, shared stormwater detention pond, water main extensions, other utilities, and landscape. Access will be provided via a new right-in, right out driveway and full access intersection at Ballenger Road (SR 1791) and Upward Road (SR 1783). Roadway work include improvements along Upward Road, Ballenger Road, and the I-26 off ramp. Water and sanitary sewer service is provided by the City of Hendersonville.

Vicinity Map



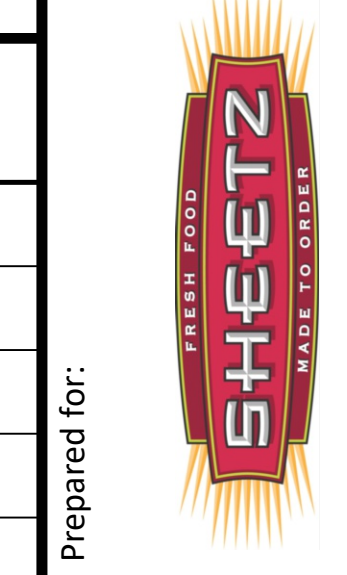
Site Map



Drawing Index

#	Sheet Title
C-1	Cover Sheet
C-2	Existing Conditions & Demolition Plan
C-3	Erosion & Sediment Control Plan - Initial Phase
C-4	Erosion & Sediment Control Plan - Final Phase
C-5	Overall Site Layout Plan
C-6	Site Layout Plan
C-7	Roadway Improvements Plan 1
C-8	Roadway Improvements Plan 2
C-9	Grading Plan
C-10	Storm Drainage Plan
C-11	Utility Plan
C-12	Water Extension Plan
C-13	Stormwater Profiles
C-14	Landscape Planting Plan
C-15	ADS Stormtech Details
C-16	Erosion & Sediment Control Details 1
C-17	Erosion & Sediment Control Details 2
C-18	Miscellaneous Details
C-19	Sheetz Details
C-20	Utility Details
C-21	Utility Details
C-22	NCDOT Details

Prepared for:



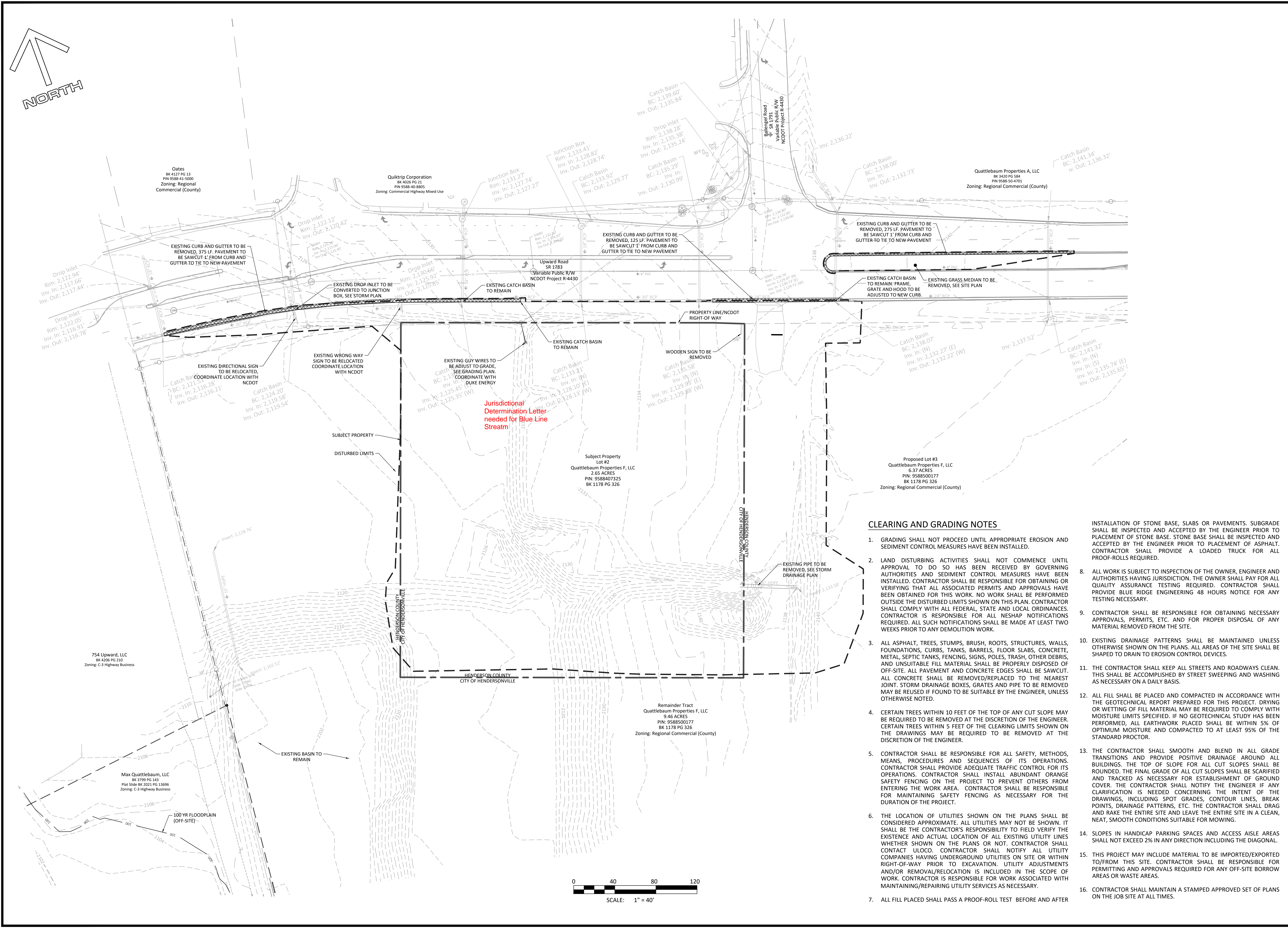
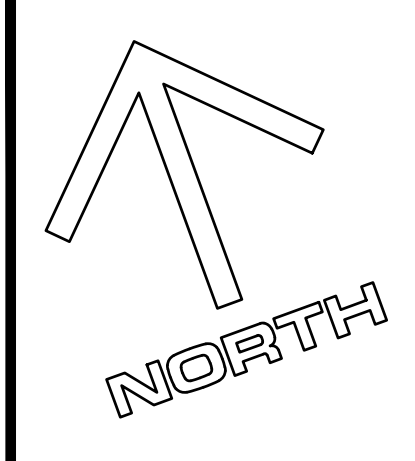
Cover Sheet

Sheetz

Upward Road
 Hendersonville, NC

C-1

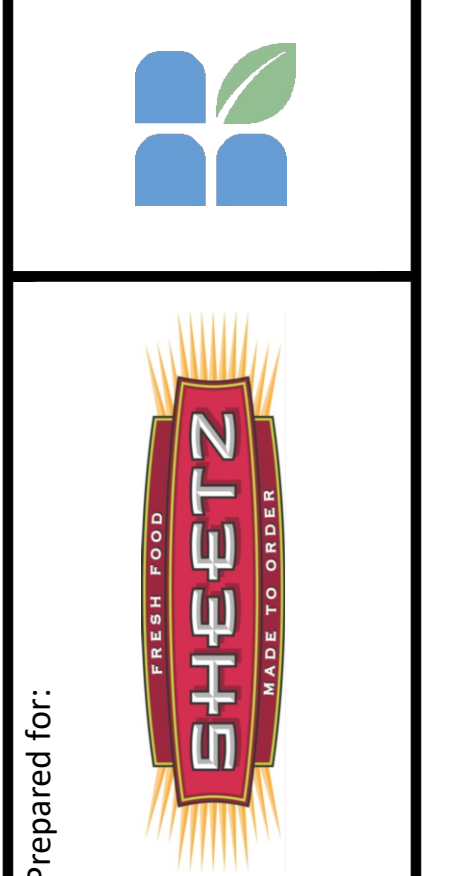
FOR PERMITTING - NOT FOR CONSTRUCTION



ORIGINAL ISSUE DATE:	12/15/2025
REVISIONS:	<ul style="list-style-type: none"> 02/22/2026 03/09/2026 03/12/2026 04/23/2026 05/19/2026 05/21/2026 05/29/2026
PROJECT #:	24039
PROJECT NAME:	ADDITIONAL GRADING REMOVE CAR WASH/SHARED POND
DRAWN BY:	WH
CHECKED BY:	WH
DESIGNED BY:	WH
DATE:	05/28/2026



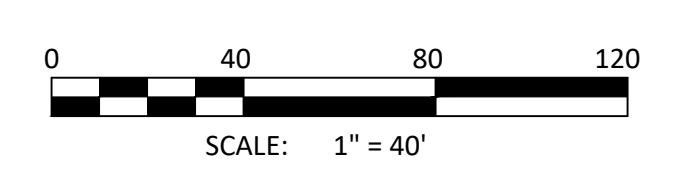
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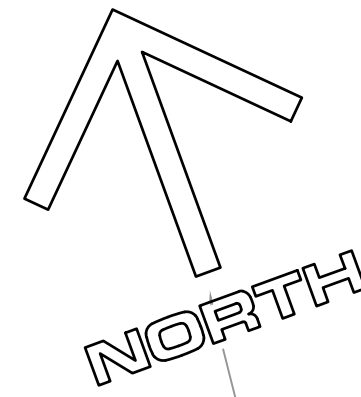
Prepared for:
Existing Conditions and Demolition Plan
 Sheetz
 Upward Road
 Hendersonville, NC

CLEARING AND GRADING NOTES

- GRADING SHALL NOT PROCEED UNTIL APPROPRIATE EROSION AND SEDIMENT CONTROL MEASURES HAVE BEEN INSTALLED.
- LAND DISTURBING ACTIVITIES SHALL NOT COMMENCE UNTIL APPROVAL TO DO SO HAS BEEN RECEIVED BY GOVERNING AUTHORITIES AND SEDIMENT CONTROL MEASURES HAVE BEEN INSTALLED. CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING OR VERIFYING THAT ALL ASSOCIATED PERMITS AND APPROVALS HAVE BEEN OBTAINED FOR THIS WORK. NO WORK SHALL BE PERFORMED OUTSIDE THE DISTURBED LIMITS SHOWN ON THIS PLAN. CONTRACTOR SHALL COMPLY WITH ALL FEDERAL, STATE AND LOCAL ORDINANCES. CONTRACTOR IS RESPONSIBLE FOR ALL NESHAP NOTIFICATIONS REQUIRED. ALL SUCH NOTIFICATIONS SHALL BE MADE AT LEAST TWO WEEKS PRIOR TO ANY DEMOLITION WORK.
- ALL ASPHALT, TREES, STUMPS, BRUSH, ROOTS, STRUCTURES, WALLS, FOUNDATIONS, CURBS, TANKS, BARRELS, FLOOR SLABS, CONCRETE, METAL SEPTIC TANKS, FENCING, SIGNS, POLES, TRASH, OTHER DEBRIS, AND UNSUITABLE FILL MATERIAL SHALL BE PROPERLY DISPOSED OF OFF-SITE. ALL PAVEMENT AND CONCRETE EDGES SHALL BE SAWCUT. ALL CONCRETE SHALL BE REMOVED/REPLACED TO THE NEAREST JOINT. STORM DRAINAGE BOXES, GRATES AND PIPE TO BE REMOVED MAY BE REUSED IF FOUND TO BE SUITABLE BY THE ENGINEER, UNLESS OTHERWISE NOTED.
- CERTAIN TREES WITHIN 10 FEET OF THE TOP OF ANY CUT SLOPE MAY BE REQUIRED TO BE REMOVED AT THE DISCRETION OF THE ENGINEER. CERTAIN TREES WITHIN 5 FEET OF THE CLEARING LIMITS SHOWN ON THE DRAWINGS MAY BE REQUIRED TO BE REMOVED AT THE DISCRETION OF THE ENGINEER.
- CONTRACTOR SHALL BE RESPONSIBLE FOR ALL SAFETY, METHODS, MEANS, PROCEDURES AND SEQUENCES OF ITS OPERATIONS. CONTRACTOR SHALL PROVIDE ADEQUATE TRAFFIC CONTROL FOR ITS OPERATIONS. CONTRACTOR SHALL INSTALL ABUNDANT ORANGE SAFETY FENCING ON THE PROJECT TO PREVENT OTHERS FROM ENTERING THE WORK AREA. CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING SAFETY FENCING AS NECESSARY FOR THE DURATION OF THE PROJECT.
- THE LOCATION OF UTILITIES SHOWN ON THE PLANS SHALL BE CONSIDERED APPROXIMATE. ALL UTILITIES MAY NOT BE SHOWN. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE EXISTENCE AND ACTUAL LOCATION OF ALL EXISTING UTILITY LINES WHETHER SHOWN ON THE PLANS OR NOT. CONTRACTOR SHALL CONTACT ULOCO. CONTRACTOR SHALL NOTIFY ALL UTILITY COMPANIES HAVING UNDERGROUND UTILITIES ON SITE OR WITHIN RIGHT-OF-WAY PRIOR TO EXCAVATION. UTILITY ADJUSTMENTS AND/OR REMOVAL/RELOCATION IS INCLUDED IN THE SCOPE OF WORK. CONTRACTOR IS RESPONSIBLE FOR WORK ASSOCIATED WITH MAINTAINING/REPAIRING UTILITY SERVICES AS NECESSARY.
- ALL FILL PLACED SHALL PASS A PROOF-ROLL TEST BEFORE AND AFTER INSTALLATION OF STONE BASE, SLABS OR PAVEMENTS. SUBGRADE SHALL BE INSPECTED AND ACCEPTED BY THE ENGINEER PRIOR TO PLACEMENT OF STONE BASE. STONE BASE SHALL BE INSPECTED AND ACCEPTED BY THE ENGINEER PRIOR TO PLACEMENT OF ASPHALT. CONTRACTOR SHALL PROVIDE A LOADED TRUCK FOR ALL PROOF-ROLLS REQUIRED.
- ALL WORK IS SUBJECT TO INSPECTION OF THE OWNER, ENGINEER AND AUTHORITIES HAVING JURISDICTION. THE OWNER SHALL PAY FOR ALL QUALITY ASSURANCE TESTING REQUIRED. CONTRACTOR SHALL PROVIDE BLUE RIDGE ENGINEERING 48 HOURS NOTICE FOR ANY TESTING NECESSARY.
- CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING NECESSARY APPROVALS, PERMITS, ETC. AND FOR PROPER DISPOSAL OF ANY MATERIAL REMOVED FROM THE SITE.
- EXISTING DRAINAGE PATTERNS SHALL BE MAINTAINED UNLESS OTHERWISE SHOWN ON THE PLANS. ALL AREAS OF THE SITE SHALL BE SHAPED TO DRAIN TO EROSION CONTROL DEVICES.
- THE CONTRACTOR SHALL KEEP ALL STREETS AND ROADWAYS CLEAN. THIS SHALL BE ACCOMPLISHED BY STREET SWEEPING AND WASHING AS NECESSARY ON A DAILY BASIS.
- ALL FILL SHALL BE PLACED AND COMPACTED IN ACCORDANCE WITH THE GEOTECHNICAL REPORT PREPARED FOR THIS PROJECT. DRYING OR WETTING OF FILL MATERIAL MAY BE REQUIRED TO COMPLY WITH MOISTURE LIMITS SPECIFIED. IF NO GEOTECHNICAL STUDY HAS BEEN PERFORMED, ALL EARTHWORK PLACED SHALL BE WITHIN 5% OF OPTIMUM MOISTURE AND COMPACTED TO AT LEAST 95% OF THE STANDARD PROCTOR.
- THE CONTRACTOR SHALL SMOOTH AND BLEND IN ALL GRADE TRANSITIONS AND PROVIDE POSITIVE DRAINAGE AROUND ALL BUILDINGS. THE TOP OF SLOPE FOR ALL CUT SLOPES SHALL BE ROUNDED. THE FINAL GRADE OF ALL CUT SLOPES SHALL BE SCARIFIED AND TRACKED AS NECESSARY FOR ESTABLISHMENT OF GROUND COVER. THE CONTRACTOR SHALL NOTIFY THE ENGINEER IF ANY CLARIFICATION IS NEEDED CONCERNING THE INTENT OF THE DRAWINGS, INCLUDING SPOT GRADES, CONTOUR LINES, BREAK POINTS, DRAINAGE PATTERNS, ETC. THE CONTRACTOR SHALL DRAG AND RAKE THE ENTIRE SITE AND LEAVE THE ENTIRE SITE IN A CLEAN, NEAT, SMOOTH CONDITIONS SUITABLE FOR MOWING.
- SLOPES IN HANDICAP PARKING SPACES AND ACCESS AISLE AREAS SHALL NOT EXCEED 2% IN ANY DIRECTION INCLUDING THE DIAGONAL.
- THIS PROJECT MAY INCLUDE MATERIAL TO BE IMPORTED/EXPORTED TO/FROM THIS SITE. CONTRACTOR SHALL BE RESPONSIBLE FOR PERMITTING AND APPROVALS REQUIRED FOR ANY OFF-SITE BORROW AREAS OR WASTE AREAS.
- CONTRACTOR SHALL MAINTAIN A STAMPED APPROVED SET OF PLANS ON THE JOB SITE AT ALL TIMES.



FOR PERMITTING - NOT FOR CONSTRUCTION



Oates
BK 4327 PG 13
PW 9588-41-5000
Zoning: Regional
Commercial (County)

Quiktrip Corporation
BK 4026 PG 21
PW 9588-40-8805
Zoning: Commercial Highway Mixed Use

Junction Box
Rim: 2,131.27'
Inv. In: 2,127.74'
Inv. Out: 2,127.27'

Catch Basin
BC: 2,135.92'
Inv. In: (N)
Inv. Out: 2,129.77'

Drop Inlet
Rim: 2,138.28'
Inv. In: 2,135.38'
Inv. Out: 2,135.24'

Catch Basin
BC: 2,135.29'
Inv. In: (N)
Inv. Out: 2,130.99'

Ballew Road
SR 1791
Variable Public R/W
NCDOT Project R-4430

Catch Basin
BC: 2,138.00'
Inv. In: (N)
Inv. Out: 2,132.73'

Quattlebaum Properties A, LLC
BK 3420 PG 584
PW 9588-50-4701
Zoning: Regional Commercial (County)

Catch Basin
BC: 2,137.12'
Inv. In: (N)
Inv. Out: (N)

Catch Basin
BC: 2,121.51'
Inv. In: 2,116.28'
Inv. Out: 2,116.21'

Catch Basin
BC: 2,124.20'
Inv. In: 2,119.58'
Inv. Out: 2,119.54'

Catch Basin
BC: 2,130.98'
Inv. In: (N)
Inv. Out: 2,125.45' (E)
Inv. Out: 2,125.35' (W)

Catch Basin
BC: 2,133.11'
Inv. In: (N)
Inv. Out: 2,128.10' (E)
Inv. Out: 2,128.13' (W)

Catch Basin
BC: 2,135.58'
Inv. In: (N)
Inv. Out: 2,129.88' (E)
Inv. Out: 2,129.88' (W)

Catch Basin
BC: 2,138.97'
Inv. In: (N)
Inv. Out: 2,132.27' (E)
Inv. Out: 2,132.22' (W)

Catch Basin
BC: 2,137.52'
Inv. In: (N)
Inv. Out: (N)

BASIN ADRIANA AREA
3.20 ACRES

BASIN ADRIANA AREA
3.20 ACRES

EXISTING DIVERSION DITCHES
TO REMAIN, CONTRACTOR TO
INSTALL STRAW WATTLE
CHECK DAMS 50' O/C

DISTURBED LIMITS

8' WIDE EMERGENCY
SPILLWAY

PROVIDE RIP-RAP
DISSIPATER PAD

INSTALL SLOPE DRAINS TO
DIVERT DITCH TO BASIN

PROPOSED 38"x76" X3' SKIMMER
BASIN SEE DETAIL
1.5" SKIMMER WITH ORIFICE 100%
OPEN, SEE DETAIL
CONTRACTOR TO ENSURE SITE
CONTINUES TO DRAIN TO BASIN
DURING CONSTRUCTION

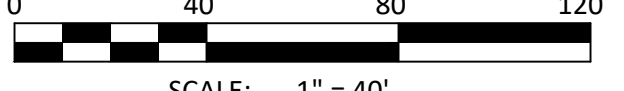
PROPOSED SEDIMENT FENCE,
PROVIDE STONE/WIRE
OUTLETS 100' O/C AND AT
ALL LOW POINTS

PROVIDE BREAK IN SEDIMENT
FENCE TO ALLOW RUNOFF TO
ENTER EXISTING BASIN

AREAS OUTSIDE DISTURBED
LIMITS PERMITTED BY OTHERS

LEGEND

- DISTURBED LIMITS - 3.80 ACRES
- TEMPORARY DIVERSION
- SEDIMENT FENCE
- STONE/WIRE MESH INLET PROTECTION
- TEMPORARY CONSTRUCTION ENTRANCE



EROSION AND SEDIMENT CONTROL NOTES

1. LAND DISTURBING ACTIVITIES SHALL NOT COMMENCE UNTIL APPROVAL TO DO SO HAS BEEN RECEIVED BY GOVERNING AUTHORITIES AND SEDIMENT CONTROL MEASURES HAVE BEEN INSTALLED. CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING OR VERIFYING THAT ALL ASSOCIATED PERMITS AND APPROVALS HAVE BEEN OBTAINED FOR THIS WORK. NO WORK SHALL BE PERFORMED OUTSIDE THE DISTURBED LIMITS SHOWN ON THIS PLAN. CONTRACTOR SHALL COMPLY WITH ALL STATE AND LOCAL ORDINANCES THAT APPLY.
2. ALL EXPOSED AREAS SHALL BE SEEDED AS SPECIFIED WITHIN 7 DAYS OF ESTABLISHING FINAL GRADE OR IF CONSTRUCTION ACTIVITY CEASES FOR LONGER THAN 7 DAYS, CONTRACTOR SHALL BE RESPONSIBLE TO TAKE WHATEVER MEANS NECESSARY TO ESTABLISH PERMANENT SOIL STABILIZATION.
3. CONTRACTOR IS RESPONSIBLE FOR ALL NCDENR INSPECTION REPORTS AND RECORD KEEPING AS REQUIRED. CONTRACTOR SHALL SUPPLY AND INSTALL A RAIN GAUGE ON SITE.
4. SEDIMENT AND EROSION CONTROL MEASURES SHALL BE INSPECTED AT LEAST ONCE EVERY SEVEN (7) DAYS AND WITHIN 24 HOURS OF A RAINFALL EXCEEDING 0.5 INCHES DURING A 24-HOUR PERIOD OR MORE FREQUENTLY IF REQUIRED BY GOVERNING NPDES GENERAL PERMIT. ANY REPAIRS OR MAINTENANCE DETERMINED NECESSARY SHALL COMMENCE WITHIN 24 HOURS AND BE COMPLETED WITHIN 48 HOURS OF INSPECTION AND REPORT.
5. THIS PLAN SHALL NOT BE CONSIDERED ALL INCLUSIVE. CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO PREVENT SOIL SEDIMENT FROM LEAVING THE SITE. CONTRACTOR SHALL TAKE WHATEVER MEANS NECESSARY TO PREVENT SEDIMENT FROM REACHING ANY EXISTING WATER BODIES OR WETLANDS. ADDITIONAL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE INSTALLED IF DEEMED NECESSARY BY ON SITE INSPECTION.
6. THE CONTRACTOR IS RESPONSIBLE FOR ESTABLISHING GROUND COVER ON ALL DISTURBED AREAS, INCLUDING ALL SLOPES. ALL SLOPES SHALL BE HYDRO-SEEDED WITH TACKIFIER ADDED. EROSION CONTROL MATTING SHALL ALSO BE PROVIDED ON ALL SLOPES STEEPER THAN 2:1 AND ON ANY OTHER AREAS AS NECESSARY TO PREVENT EROSION AND ESTABLISH GROUND COVER. ANY AREA THAT SHOWS DISTRESS SHALL BE MATTED.
7. EXISTING DRAINAGE PATTERNS SHALL BE MAINTAINED UNLESS OTHERWISE SHOWN ON THE PLANS. ALL AREAS OF THE SITE SHALL BE SHAPED TO DRAIN TO EROSION CONTROL DEVICES. IF INSTALLATION OF STORM DRAINAGE SYSTEM SHOULD BE INTERRUPTED BY WEATHER OR NIGHTFALL, THE PIPE ENDS SHALL BE COVERED WITH FILTER FABRIC.
8. SILT FENCE STONE OUTLETS SHALL BE INSTALLED IF NECESSARY EVERY 100LF AND AT LOW POINTS ALONG THE SILT FENCE. SEDIMENT SHALL BE REMOVED FROM THE SEDIMENT TRAP(S) OR BASIN(S) BEFORE REACHING 25% FULL AND FROM SILT FENCE BEFORE IT IS 33% FULL.
9. THE CONTRACTOR IS RESPONSIBLE FOR ALL SAFETY, METHODS, PROCEDURES, AND SEQUENCES OF ITS OPERATIONS. CONTRACTOR SHALL PROVIDE ADEQUATE TRAFFIC CONTROL FOR ITS OPERATIONS.
10. LOCATIONS OF UTILITIES SHOWN ARE BASED ON EVIDENCE OBSERVED AT THE TIME OF SURVEY. OTHER UTILITIES MAY EXIST THAT ARE NOT SHOWN. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE EXISTENCE AND ACTUAL LOCATION OF ALL EXISTING UTILITY LINES, AND TO PROTECT THESE LINES DURING CONSTRUCTION.
11. THE CONTRACTOR SHALL KEEP ALL STREETS AND ROADWAYS CLEAN AT ALL TIMES. THIS SHALL BE ACCOMPLISHED BY STREET SWEEPING AND WASHING AS NECESSARY ON A DAILY BASIS.
12. CONTRACTOR SHALL MAINTAIN AN APPROVED SET OF PLANS ON THE JOB SITE AT ALL TIMES.
13. CONTRACTOR SHALL CONTACT EROSION CONTROL DEPARTMENT PRIOR TO REMOVAL OF ANY SEDIMENT CONTROL MEASURES.

EROSION AND SEDIMENT CONTROL SEQUENCE

- INITIAL PHASE
1. PRIOR TO LAND DISTURBING ACTIVITIES A PRE-CONSTRUCTION MEETING SHALL BE HELD ON-SITE WITH THE CONTRACTOR AND SUBCONTRACTOR(S) PERFORMING THE WORK. CONTRACTOR SHALL NOTIFY OWNER AND ENGINEER OF MEETING DATE AND TIME AT LEAST 48 HOURS PRIOR.
 2. CONSTRUCT TEMPORARY CONSTRUCTION ENTRANCE.
 3. INSTALL PERIMETER SEDIMENT FENCE AS SHOWN.
 4. INSTALL SEDIMENT BASIN WITH BAFFLES AND SKIMMER BASIN SHALL BE MAINTAINED DURING CONSTRUCTION.
 5. CONTACT HENDERSON COUNTY SITE DEVELOPMENT STAFF FOR INITIAL EROSION CONTROL MEASURE INSPECTION PRIOR TO ANY CLEARING GRUBBING, OR GRADING.
 6. MAINTAIN EXISTING DRAINAGE PATTERNS, ALLOWING SITE TO DRAIN TO EXISTING SEDIMENT BASIN. CONTRACTOR SHALL KEEP SITE SHAPED TO DRAIN TO SEDIMENT BASIN TO THE MAXIMUM EXTENT POSSIBLE.
 7. COMMENCE CLEARING, STRIPPING, GRUBBING AND DEMOLITION OPERATIONS. THIS WORK SHALL BE PERFORMED ONLY IN AREAS WHERE EARTHWORK WILL COMMENCE WITHIN 7 DAYS.
 8. INSTALL ADDITIONAL EROSION CONTROL MEASURES AS NECESSARY. ALL STORMWATER SHALL DRAIN TO AN APPROVED SEDIMENT CONTROL DEVICE.
 9. IMPLEMENT TEMPORARY OR PERMANENT STABILIZATION FOR ALL AREAS OF THE SITE WHERE ACTIVITY HAS CEASED FOR MORE THAN 7 DAYS.
 10. PROVIDE OWNER WITH COPIES OF ALL INSPECTIONS AND RECORDS, WEEKLY.

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DRAWN BY:	WH
PROJECT #:	24039
ORIGINAL ISSUE DATE:	12/15/2025
REVISIONS:	<ul style="list-style-type: none"> ▲ 02/22/2026 ▲ 03/09/2026 ▲ 03/10/2026 ▲ 03/13/2026 ▲ 04/12/2026 ▲ 05/18/2026 ▲ 05/21/2026 ▲ 05/29/2026
ADDITIONAL GRADING:	REMOVE CAR WASH/SHARED POND
SHEET COMMENTS:	PROGRESS SET
REVISION PERMIT SET:	REVISED SITE PLAN FOR PERMIT APPROVAL
PLAN REVISIONS PER CITY/COUNTY:	PLAN REVISIONS PER CITY



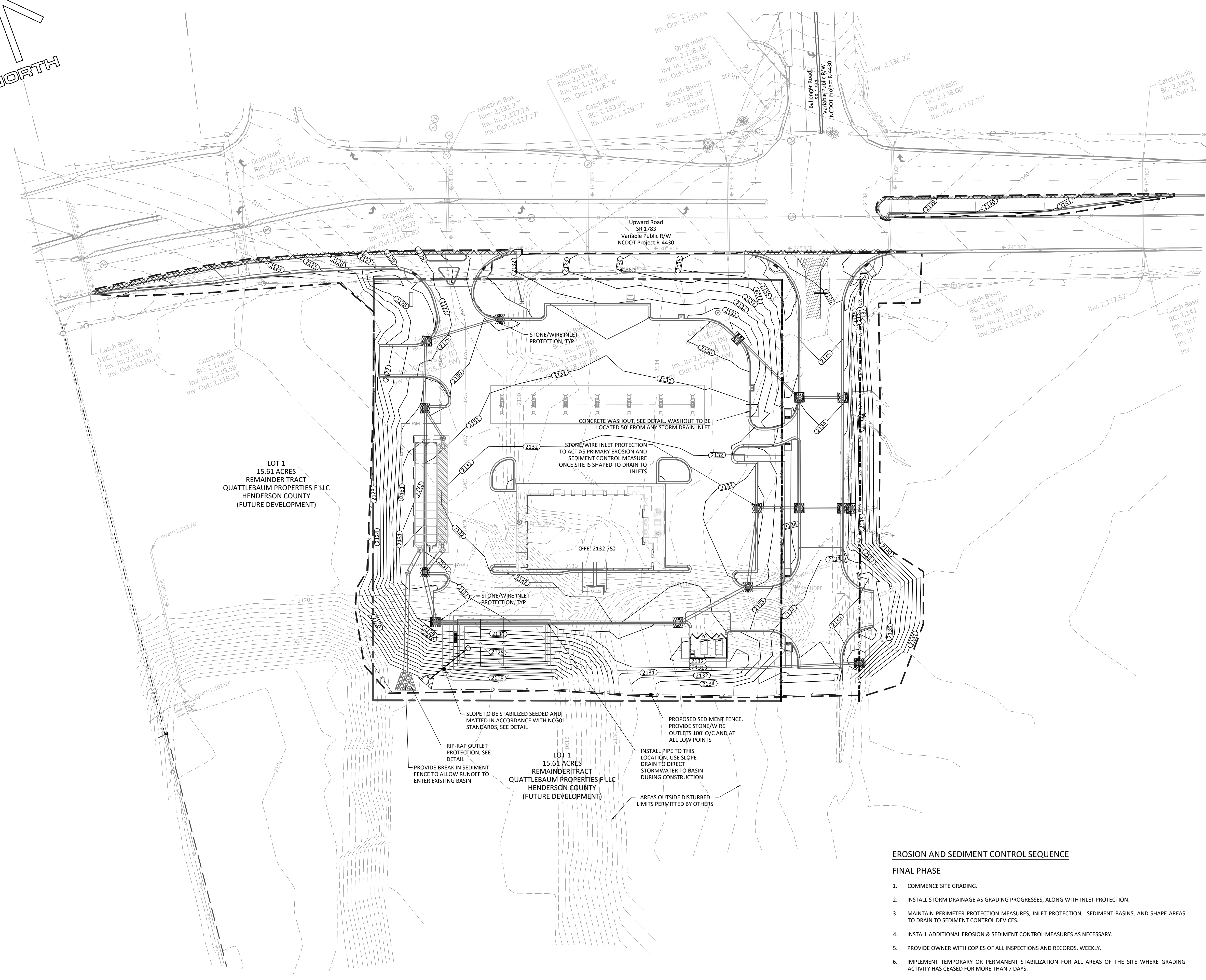
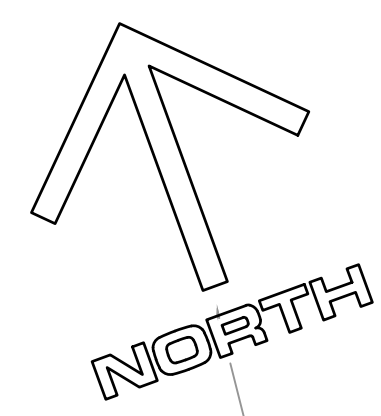
Blue Ridge Engineering PLLC
924 Main Street - Suite 200
North Wilkesboro, NC 28659
336.838.2500
www.ridge.blue
Firm #P-0205



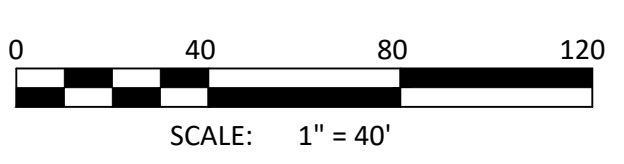
Prepared for:
Erosion & Sediment Control Plan - Initial Phase
Sheetz
Upward Road
Hendersonville, NC

SHEET
C-3

FOR PERMITTING - NOT FOR CONSTRUCTION



LOT 1
15.61 ACRES
REMAINDER TRACT
QUATTLEBAUM PROPERTIES F LLC
HENDERSON COUNTY
(FUTURE DEVELOPMENT)



LEGEND

- DISTURBED LIMITS - 3.80 ACRES
- TEMPORARY DIVERSION
- SEDIMENT FENCE
- STONE/WIRE MESH INLET PROTECTION
- TEMPORARY CONSTRUCTION ENTRANCE

EROSION AND SEDIMENT CONTROL NOTES

1. LAND DISTURBING ACTIVITIES SHALL NOT COMMENCE UNTIL APPROVAL TO DO SO HAS BEEN RECEIVED BY GOVERNING AUTHORITIES AND SEDIMENT CONTROL MEASURES HAVE BEEN INSTALLED. CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING OR VERIFYING THAT ALL ASSOCIATED PERMITS AND APPROVALS HAVE BEEN OBTAINED FOR THIS WORK. NO WORK SHALL BE PERFORMED OUTSIDE THE DISTURBED LIMITS SHOWN ON THIS PLAN. CONTRACTOR SHALL COMPLY WITH ALL STATE AND LOCAL ORDINANCES THAT APPLY.
2. ALL EXPOSED AREAS SHALL BE SEEDED AS SPECIFIED WITHIN 7 DAYS OF ESTABLISHING FINAL GRADE OR IF CONSTRUCTION ACTIVITY CEASES FOR LONGER THAN 7 DAYS. CONTRACTOR SHALL BE RESPONSIBLE TO TAKE WHATEVER MEANS NECESSARY TO ESTABLISH PERMANENT SOIL STABILIZATION.
3. CONTRACTOR IS RESPONSIBLE FOR ALL NCDENR INSPECTION REPORTS AND RECORD KEEPING AS REQUIRED. CONTRACTOR SHALL SUPPLY AND INSTALL A RAIN GAUGE ON SITE.
4. SEDIMENT AND EROSION CONTROL MEASURES SHALL BE INSPECTED AT LEAST ONCE EVERY SEVEN (7) DAYS AND WITHIN 24 HOURS OF A RAINFALL EXCEEDING 0.5 INCHES DURING A 24-HOUR PERIOD OR MORE FREQUENTLY IF REQUIRED BY GOVERNING NPDES GENERAL PERMIT. ANY REPAIRS OR MAINTENANCE DETERMINED NECESSARY SHALL COMMENCE WITHIN 24 HOURS AND BE COMPLETED WITHIN 48 HOURS OF INSPECTION AND REPORT.
5. THIS PLAN SHALL NOT BE CONSIDERED ALL INCLUSIVE. CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO PREVENT SOIL SEDIMENT FROM LEAVING THE SITE. CONTRACTOR SHALL TAKE WHATEVER MEANS NECESSARY TO PREVENT SEDIMENT FROM REACHING ANY EXISTING WATER BODIES OR WETLANDS. ADDITIONAL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE INSTALLED IF DEEMED NECESSARY BY ON SITE INSPECTION.
6. THE CONTRACTOR IS RESPONSIBLE FOR ESTABLISHING GROUND COVER ON ALL DISTURBED AREAS, INCLUDING ALL SLOPES. ALL SLOPES SHALL BE HYDRO-SEEDED WITH TACKIFIER ADDED. EROSION CONTROL MATTING SHALL ALSO BE PROVIDED ON ALL SLOPES STEEPER THAN 2:1 AND ON ANY OTHER AREAS AS NECESSARY TO PREVENT EROSION AND ESTABLISH GROUND COVER. ANY AREA THAT SHOWS DISTRESS SHALL BE MATTED.
7. EXISTING DRAINAGE PATTERNS SHALL BE MAINTAINED UNLESS OTHERWISE SHOWN ON THE PLANS. ALL AREAS OF THE SITE SHALL BE SHAPED TO DRAIN TO EROSION CONTROL DEVICES. IF INSTALLATION OF STORM DRAINAGE SYSTEM SHOULD BE INTERRUPTED BY WEATHER OR NIGHTFALL, THE PIPE ENDS SHALL BE COVERED WITH FILTER FABRIC.
8. SILT FENCE STONE OUTLETS SHALL BE INSTALLED IF NECESSARY EVERY 100LF AND AT LOW POINTS ALONG THE SILT FENCE. SEDIMENT SHALL BE REMOVED FROM THE SEDIMENT TRAP(S) OR BASIN(S) BEFORE REACHING 25% FULL AND FROM SILT FENCE BEFORE IT IS 33% FULL.
9. THE CONTRACTOR IS RESPONSIBLE FOR ALL SAFETY, METHODS, PROCEDURES, AND SEQUENCES OF ITS OPERATIONS. CONTRACTOR SHALL PROVIDE ADEQUATE TRAFFIC CONTROL FOR ITS OPERATIONS.
10. LOCATIONS OF UTILITIES SHOWN ARE BASED ON EVIDENCE OBSERVED AT THE TIME OF SURVEY. OTHER UTILITIES MAY EXIST THAT ARE NOT SHOWN. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE EXISTENCE AND ACTUAL LOCATION OF ALL EXISTING UTILITY LINES, AND TO PROTECT THESE LINES DURING CONSTRUCTION.
11. THE CONTRACTOR SHALL KEEP ALL STREETS AND ROADWAYS CLEAN AT ALL TIMES. THIS SHALL BE ACCOMPLISHED BY STREET SWEEPING AND WASHING AS NECESSARY ON A DAILY BASIS.

EROSION AND SEDIMENT CONTROL SEQUENCE

FINAL PHASE

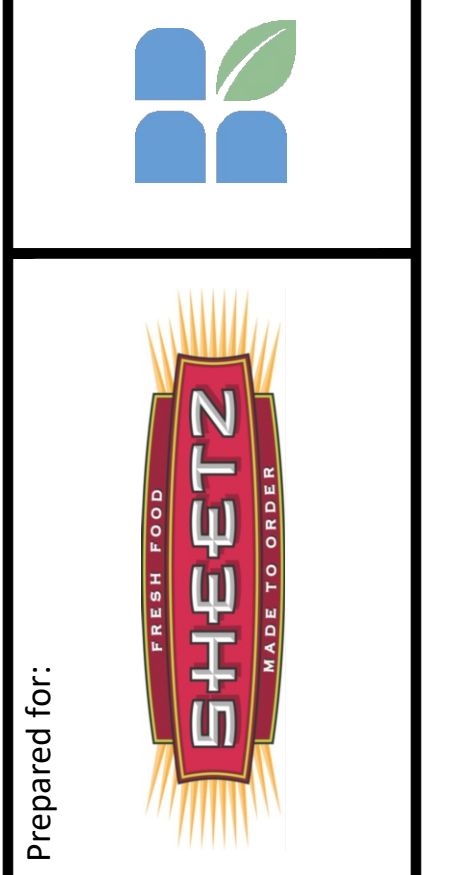
1. COMMENCE SITE GRADING.
2. INSTALL STORM DRAINAGE AS GRADING PROGRESSES, ALONG WITH INLET PROTECTION.
3. MAINTAIN PERIMETER PROTECTION MEASURES, INLET PROTECTION, SEDIMENT BASINS, AND SHAPE AREAS TO DRAIN TO SEDIMENT CONTROL DEVICES.
4. INSTALL ADDITIONAL EROSION & SEDIMENT CONTROL MEASURES AS NECESSARY.
5. PROVIDE OWNER WITH COPIES OF ALL INSPECTIONS AND RECORDS, WEEKLY.
6. IMPLEMENT TEMPORARY OR PERMANENT STABILIZATION FOR ALL AREAS OF THE SITE WHERE GRADING ACTIVITY HAS CEASED FOR MORE THAN 7 DAYS.
7. FINALIZE PAVEMENT SUB-GRADE PREPARATION. MAINTAIN TEMPORARY CONSTRUCTION ENTRANCE UNTIL STONE BASE IS INSTALLED.
8. REMOVE SKIMMER BASIN AFTER APPROVAL BY HENDERSON COUNTY SITE DEVELOPMENT STAFF
9. INSTALL CURB AND GUTTER. INLET PROTECTION MAY BE REMOVED TEMPORARILY FOR THIS CONSTRUCTION. REPLACE ANY REMOVED INLET PROTECTION AT THE END OF EACH DAY.

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ADDITIONAL GRADING:	REMOVE CAR WASH/SHARED POND
SHEETZ COMMENTS:	PROGRESS SET
REVISED PERMIT SET:	REVISED PERMIT SET
PLAN REVISIONS PER CITY/COUNTY:	PLAN REVISIONS PER CITY/COUNTY



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 924 Main Street - Suite 200
 North Wilkesboro, NC 28659
 336.838.2500
 www.ridge.blue
 Firm #P-0205



Erosion & Sediment Control Plan - Final Phase

Sheetz
Upward Road
Hendersonville, NC

SHEET

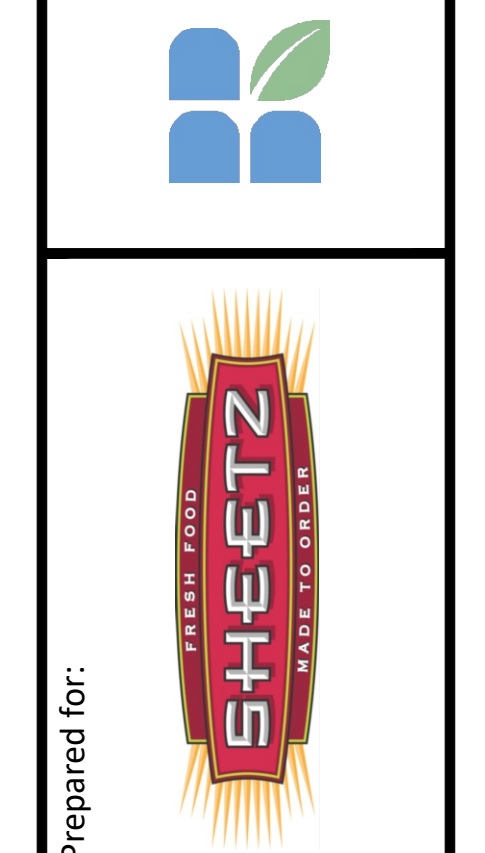
C-4

FOR PERMITTING - NOT FOR CONSTRUCTION

ORIGINAL ISSUE DATE:	12/15/2025	
PROJECT #:	24039	
DRAWN BY:	WIH	
REVISIONS:		
A	02/27/2026	ADDITIONAL GRADING
A	02/09/2026	REMOVE CAR WASH/SHARED POND
A	02/10/2026	SHEETZ COMMENTS
A	02/13/2026	PROGRESS SET
A	04/23/2026	REVISED PERMIT SET
A	05/19/2026	REVISED SITE PLAN FOR PER APPROVAL
A	05/21/2026	PLAN REVISIONS PER CITY/COUNTY
A	05/29/2026	PLAN REVISIONS PER CITY



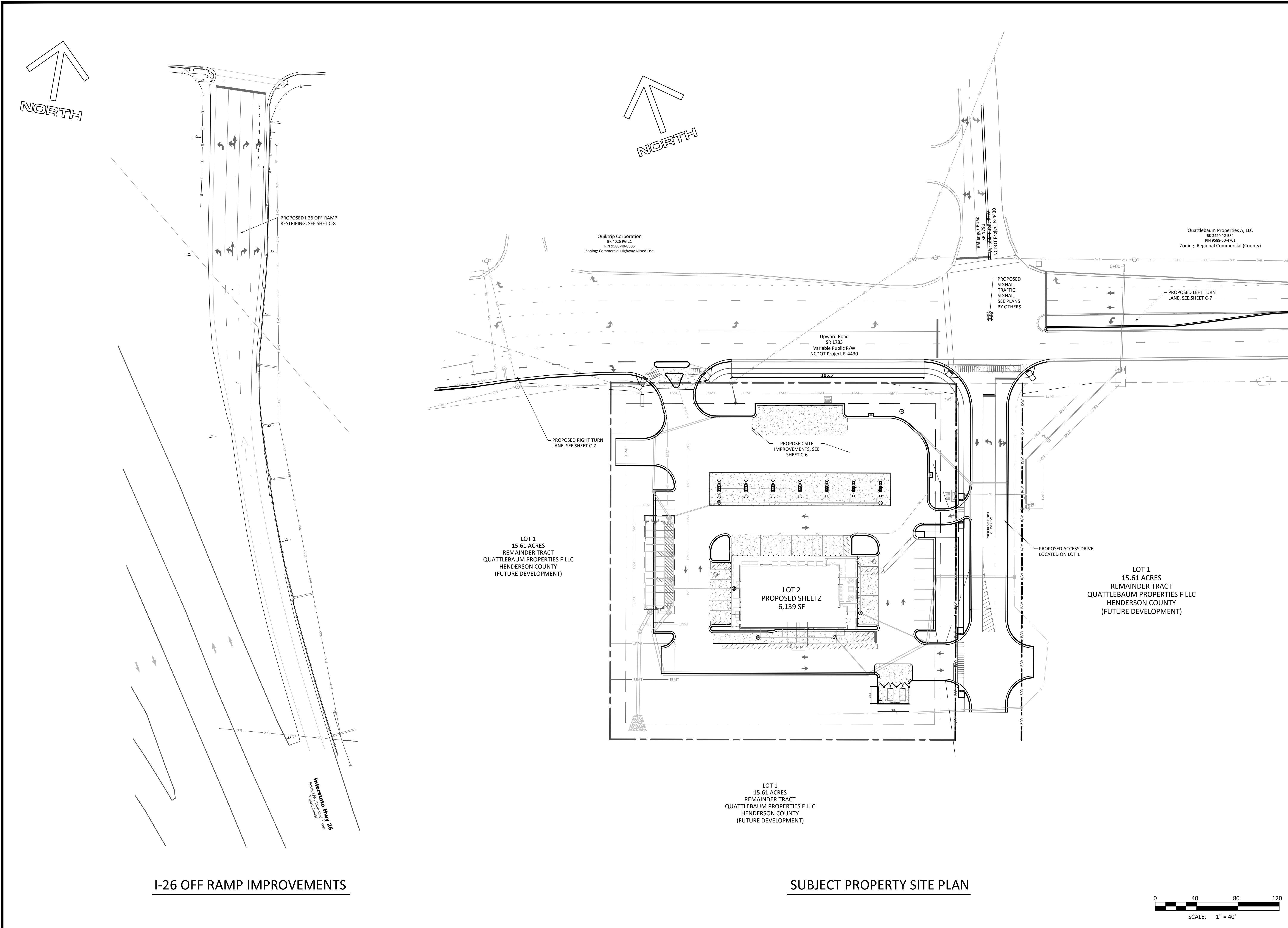
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 336.838.2500
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Prepared for:
Overall Site Layout Plan
 Sheetz
 Upward Road
 Hendersonville, NC

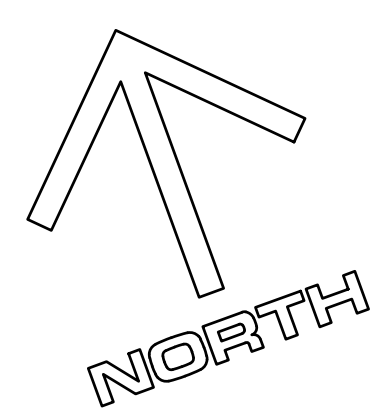
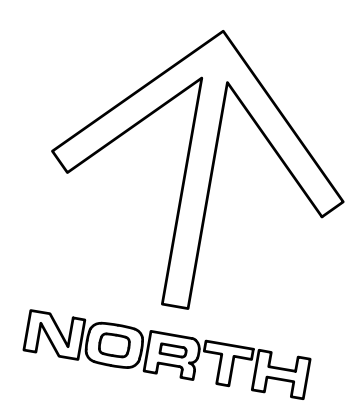
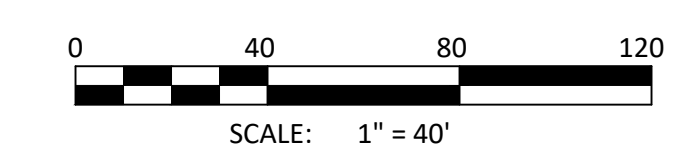
SHEET
C-5

FOR PERMITTING - NOT FOR CONSTRUCTION



I-26 OFF RAMP IMPROVEMENTS

SUBJECT PROPERTY SITE PLAN



LOT 1
 15.61 ACRES
 REMAINDER TRACT
 QUATTLEBAUM PROPERTIES F LLC
 HENDERSON COUNTY
 (FUTURE DEVELOPMENT)

LOT 1
 15.61 ACRES
 REMAINDER TRACT
 QUATTLEBAUM PROPERTIES F LLC
 HENDERSON COUNTY
 (FUTURE DEVELOPMENT)

LOT 1
 15.61 ACRES
 REMAINDER TRACT
 QUATTLEBAUM PROPERTIES F LLC
 HENDERSON COUNTY
 (FUTURE DEVELOPMENT)

Quitrip Corporation
 BK 4026 PG 21
 PIN 9588-40-8805
 Zoning: Commercial Highway Mixed Use

Upward Road
 SR 1783
 Variable Public R/W
 NCDOT Project R-4430

Quattlebaum Properties A, LLC
 BK 3420 PG 584
 PIN 9588-50-4701
 Zoning: Regional Commercial (County)

PROPOSED I-26 OFF-RAMP
 RESTRIPING, SEE SHET C-8

PROPOSED RIGHT TURN
 LANE, SEE SHEET C-7

PROPOSED SITE
 IMPROVEMENTS, SEE
 SHEET C-6

PROPOSED SIGNAL
 TRAFFIC
 SIGNAL,
 SEE PLANS
 BY OTHERS

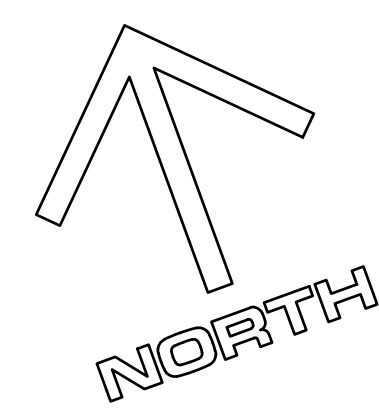
PROPOSED LEFT TURN
 LANE, SEE SHEET C-7

PROPOSED ACCESS DRIVE
 LOCATED ON LOT 1

LOT 2
 PROPOSED SHEETZ
 6,139 SF

Interstate Hwy 26
 1991-1992 AMN Cor. 4430

PROPOSED DRIVEWAY
 LOCATED ON LOT 1



SITE LAYOUT NOTES

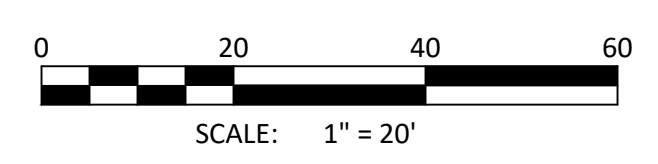
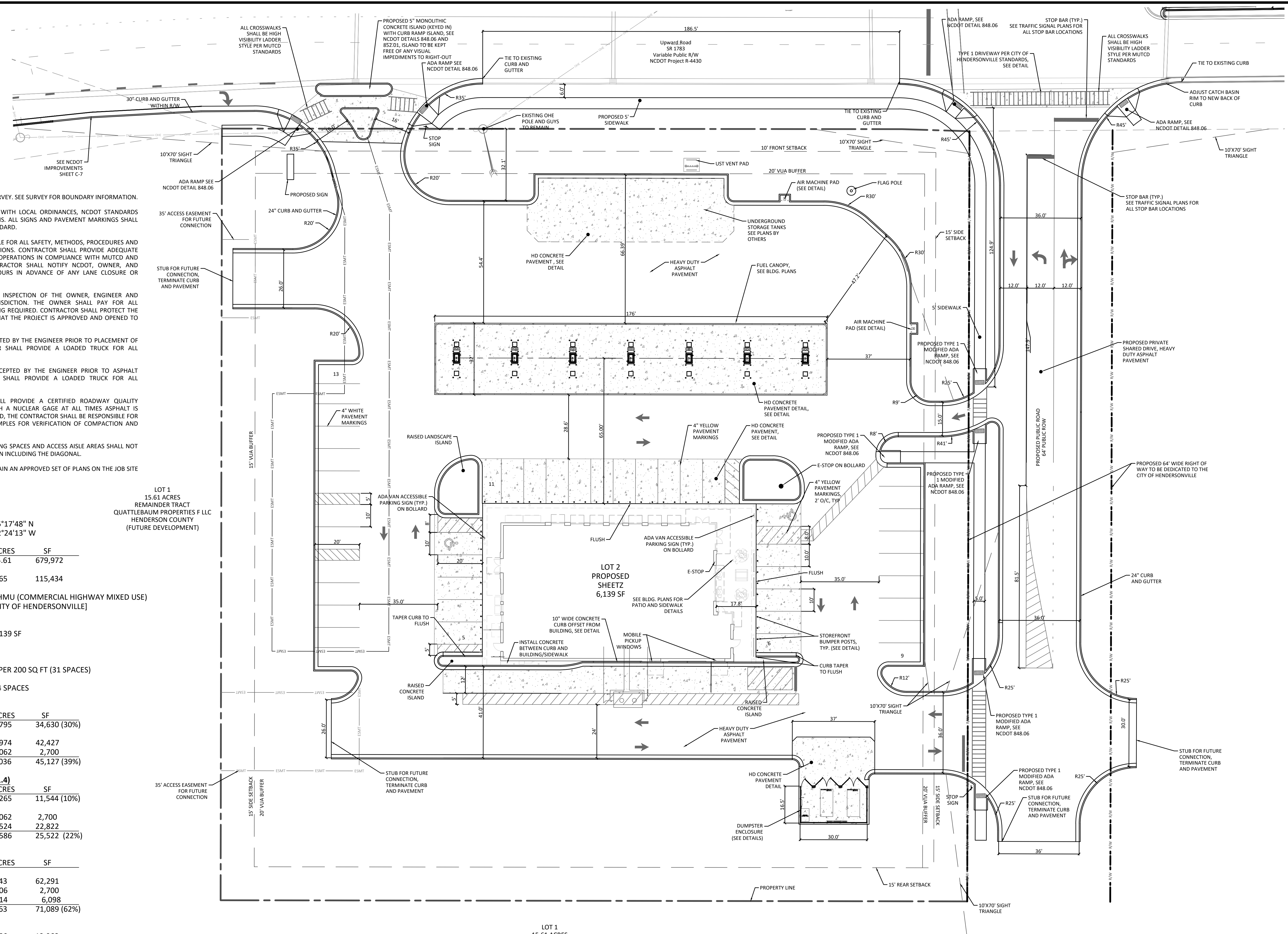
1. THIS IS NOT A PROPERTY SURVEY. SEE SURVEY FOR BOUNDARY INFORMATION.
2. ALL WORK SHALL COMPLY WITH LOCAL ORDINANCES, NCDOT STANDARDS AND OWNER SPECIFICATIONS. ALL SIGNS AND PAVEMENT MARKINGS SHALL COMPLY WITH MUTCD STANDARD.
3. CONTRACTOR IS RESPONSIBLE FOR ALL SAFETY, METHODS, PROCEDURES AND SEQUENCES OF ITS OPERATIONS. CONTRACTOR SHALL PROVIDE ADEQUATE TRAFFIC CONTROL FOR ITS OPERATIONS IN COMPLIANCE WITH MUTCD AND NCDOT STANDARDS. CONTRACTOR SHALL NOTIFY NCDOT, OWNER, AND ENGINEER AT LEAST 72 HOURS IN ADVANCE OF ANY LANE CLOSURE OR TRAFFIC SHIFT.
4. ALL WORK IS SUBJECT TO INSPECTION OF THE OWNER, ENGINEER AND AUTHORITIES HAVING JURISDICTION. THE OWNER SHALL PAY FOR ALL QUALITY ASSURANCE TESTING REQUIRED. CONTRACTOR SHALL PROTECT THE WORK UNTIL SUCH TIME THAT THE PROJECT IS APPROVED AND OPENED TO TRAFFIC.
5. SUBGRADE SHALL BE ACCEPTED BY THE ENGINEER PRIOR TO PLACEMENT OF STONE BASE. CONTRACTOR SHALL PROVIDE A LOADED TRUCK FOR ALL PROOF-ROLLS REQUIRED.
6. STONE BASE SHALL BE ACCEPTED BY THE ENGINEER PRIOR TO ASPHALT PLACEMENT. CONTRACTOR SHALL PROVIDE A LOADED TRUCK FOR ALL PROOF-ROLLS REQUIRED.
7. PAVING CONTRACTOR SHALL PROVIDE A CERTIFIED ROADWAY QUALITY CONTROL TECHNICIAN WITH A NUCLEAR GAGE AT ALL TIMES ASPHALT IS BEING PLACED. IF REQUESTED, THE CONTRACTOR SHALL BE RESPONSIBLE FOR TAKING ASPHALT CORE SAMPLES FOR VERIFICATION OF COMPACTION AND THICKNESS.
8. SLOPES IN HANDICAP PARKING SPACES AND ACCESS AISLE AREAS SHALL NOT EXCEED 2% IN ANY DIRECTION INCLUDING THE DIAGONAL.
9. CONTRACTOR SHALL MAINTAIN AN APPROVED SET OF PLANS ON THE JOB SITE AT ALL TIMES.

SITE DATA

GENERAL		LATITUDE 35°17'48" N	
LONGITUDE 82°24'13" W			
LOT AREA	ACRES	SF	
LOT 1 (ACCESS/FUTURE)	15.61	679,972	
LOT 2 (SHEETZ SITE)	2.65	115,434	
LOT 2 ZONING	CHMU (COMMERCIAL HIGHWAY MIXED USE) [CITY OF HENDERSONVILLE]		
PROPOSED USE	C-STORE		
PARKING:	REQUIRED 6,139 SF		
GAS STATION	1 PER 200 SQ FT (31 SPACES)		
PROVIDED	44 SPACES		
OPEN SPACE (5-27-3.2)			
REQUIRED	ACRES	SF	
PROVIDED	0.795	34,630 (30%)	
LANDSCAPED AREAS	0.974	42,427	
SIDEWALK/SEATING	0.062	2,700	
TOTAL	1.036	45,127 (39%)	
COMMON SPACE (5-27-4-1.4)			
REQUIRED	ACRES	SF	
PROVIDED	0.265	11,544 (10%)	
SIDEWALK/SEATING	0.062	2,700	
PLANTED BEDS	0.524	22,822	
TOTAL	0.586	25,522 (22%)	
BUILT UPON AREA			
LOT 2 (SHEETZ)	ACRES	SF	
VEHICLE USE AREA	1.43	62,291	
SIDEWALK/PATIO	0.06	2,700	
BUILDING	0.14	6,098	
TOTAL	1.63	71,089 (62%)	
OFFSITE			
ACCESS ROAD	0.30	13,068	
SIDEWALK	0.04	1,742	
TOTAL	0.34	14,810	
SETBACKS			
FRONT	10'		
SIDE	15'		
REAR	15'		

LOT 1
15.61 ACRES
REMAINDER TRACT
QUATTLEBAUM PROPERTIES F LLC
HENDERSON COUNTY
(FUTURE DEVELOPMENT)

LOT 1
15.61 ACRES
REMAINDER TRACT
QUATTLEBAUM PROPERTIES F LLC
HENDERSON COUNTY
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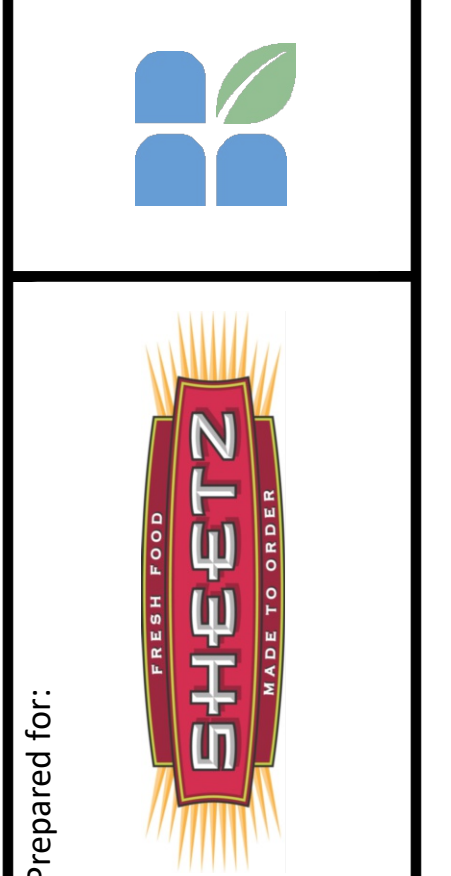


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DRAWN BY: WIH
PROJECT #: 24039
ADDITIONAL GRADING: REMOVE CAR WASH/SHARED POND
REVISIONS:
A 02/22/2026 SHEETZ COMMENTS
B 03/02/2026 PROGRESS SET
C 03/13/2026 REVISED PERMITS
D 04/21/2026 REVISED SITE PLAN FOR PER APPROVAL
E 05/19/2026 PLAN REVISIONS PER CITY/COUNTY
F 05/29/2026 PLAN REVISIONS PER CITY



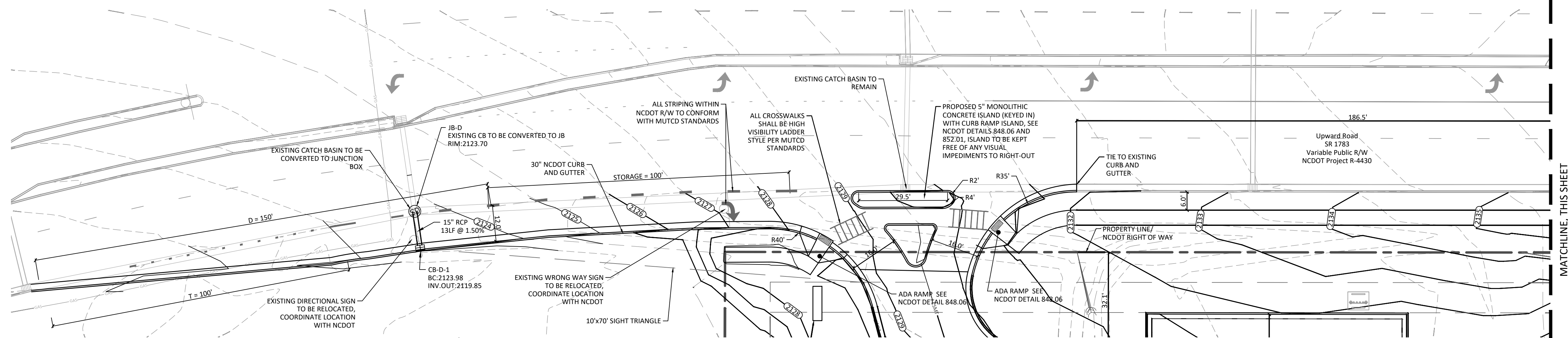
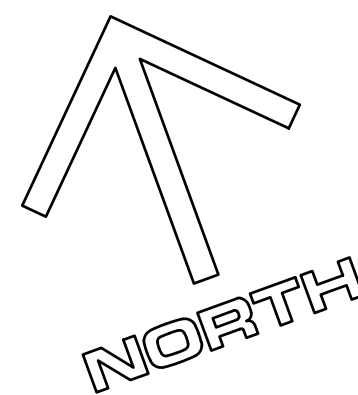
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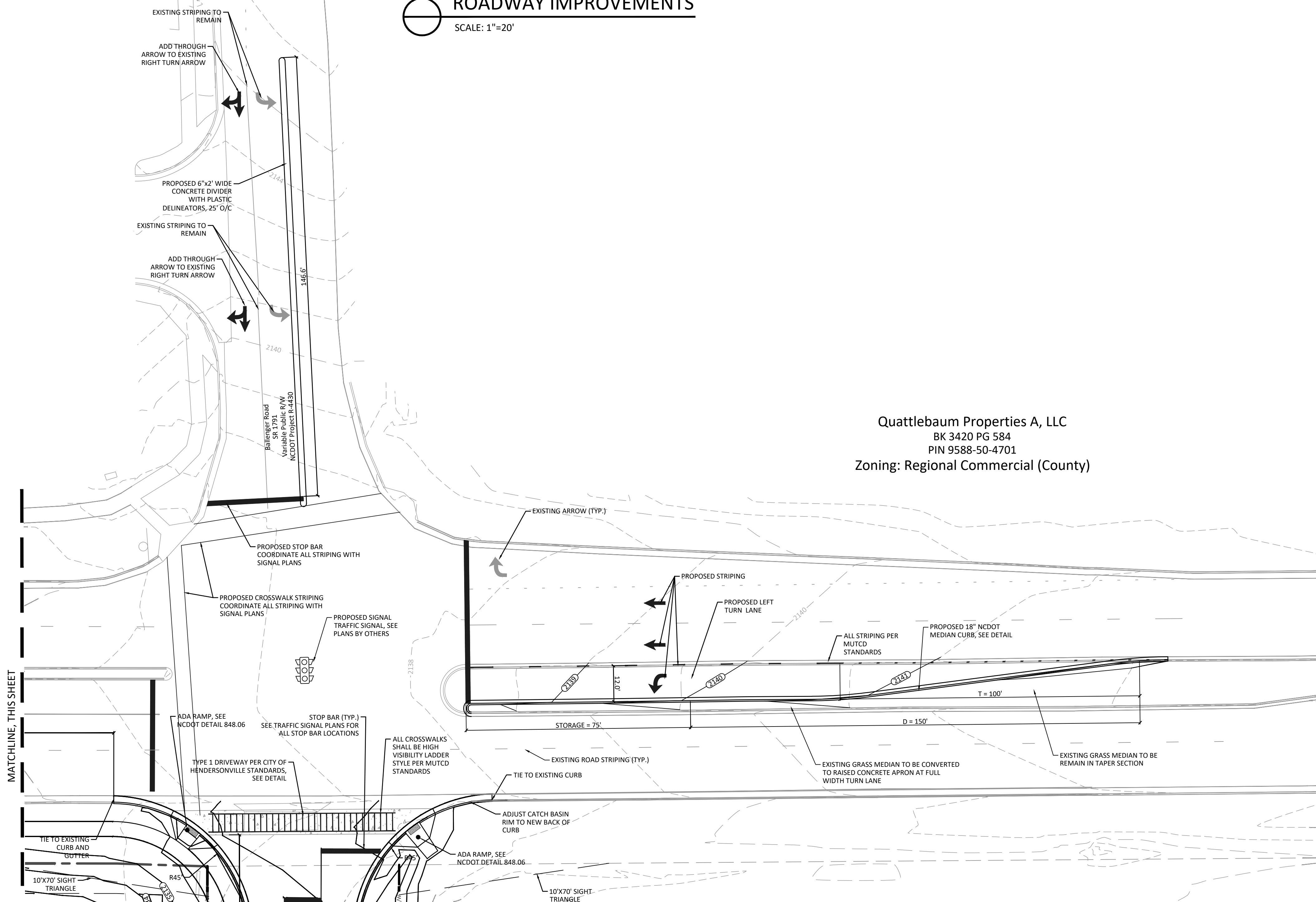
Prepared for:
Detailed Site Layout Plan
Sheetz
Upward Road
Hendersonville, NC

SHEET
C-6

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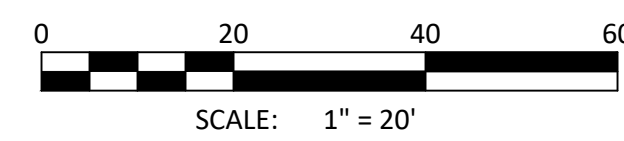


ROADWAY IMPROVEMENTS
SCALE: 1"=20'



Quattlebaum Properties A, LLC
BK 3420 PG 584
PIN 9588-50-4701
Zoning: Regional Commercial (County)

ROADWAY IMPROVEMENTS
SCALE: 1"=20'

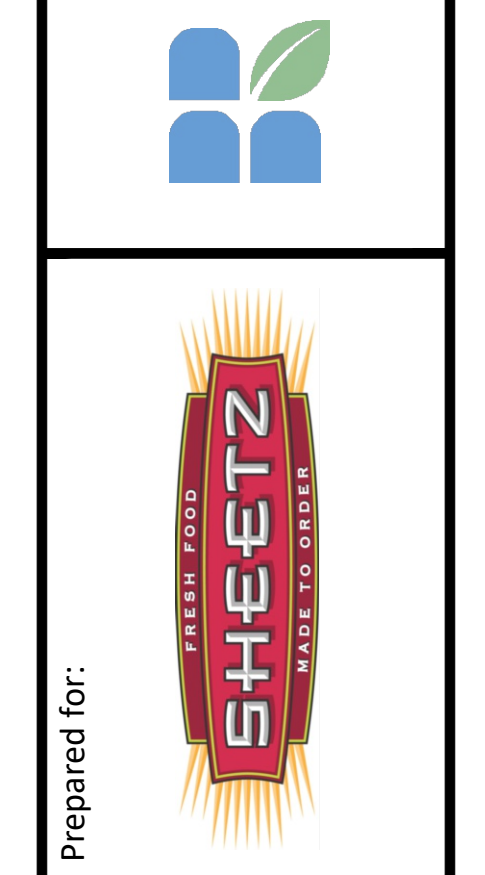


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PROJECT #:	24039
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SHEET COMMENTS:	PROGRESS SET
REVISED PERMIT SET:	REVISED PERMIT SET
REVISED SITE PLAN FOR PER APPROVAL:	REVISED SITE PLAN FOR PER APPROVAL
PLAN REVISIONS PER CITY/COUNTY:	PLAN REVISIONS PER CITY/COUNTY



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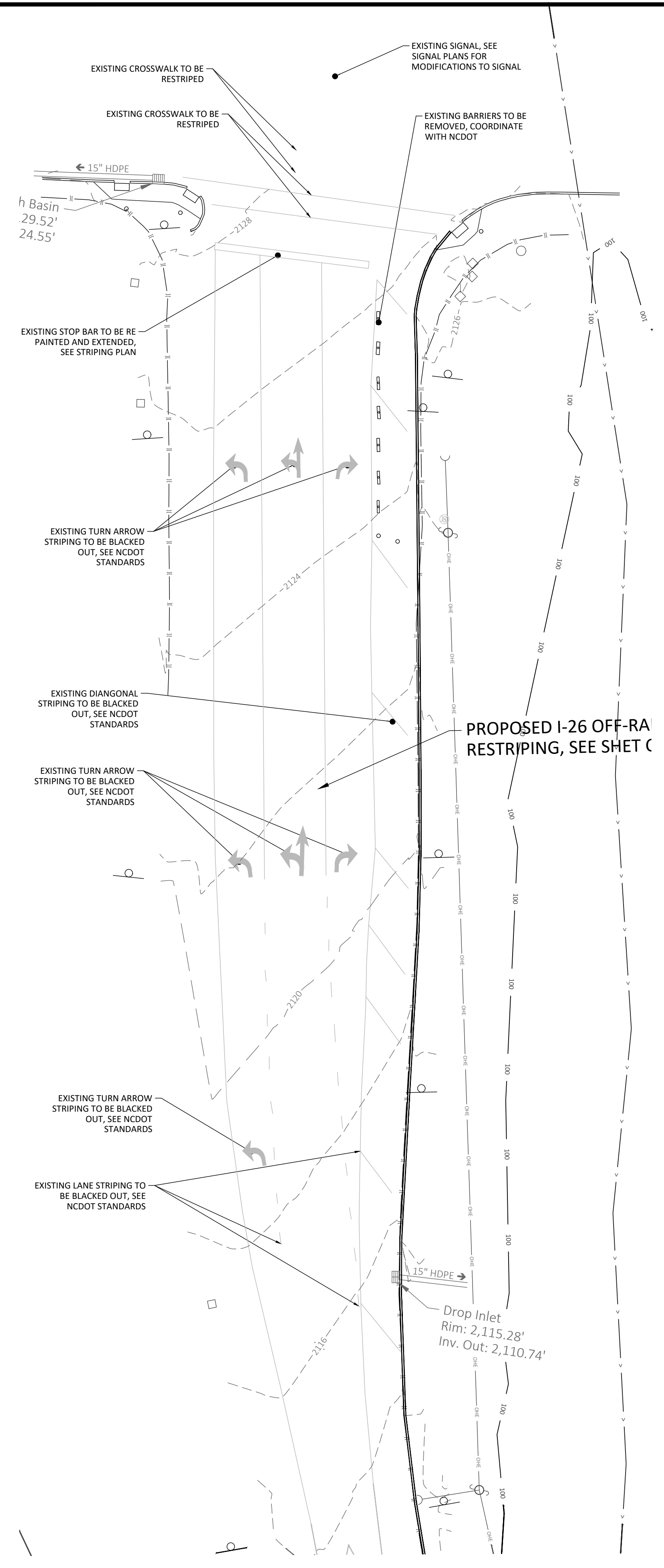
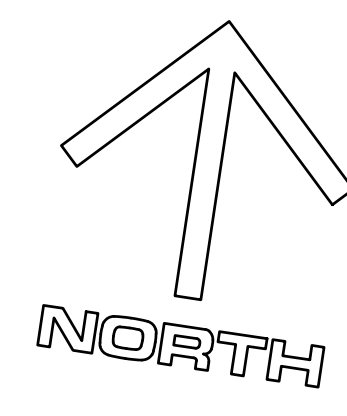


Prepared for:
Quattlebaum Properties A, LLC
BK 3420 PG 584
PIN 9588-50-4701
Zoning: Regional Commercial (County)

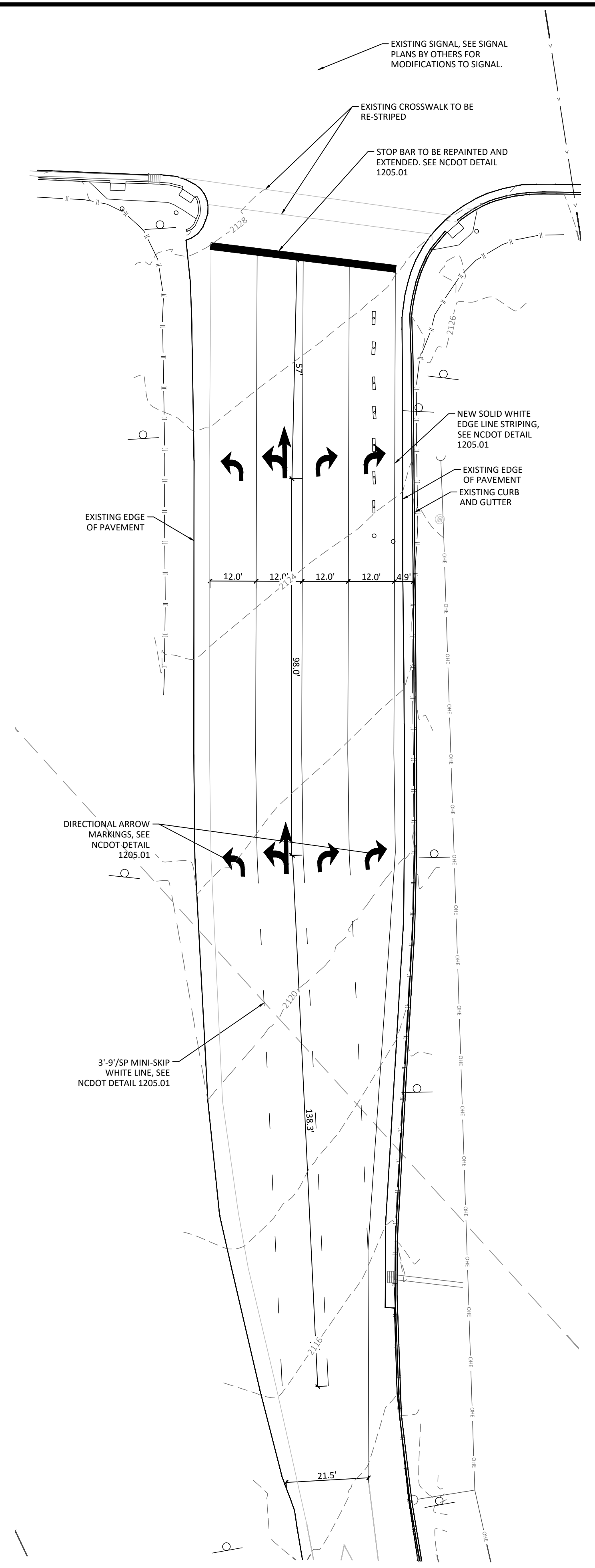
Sheetz
Upward Road
Hendersonville, NC

ROADWAY IMPROVEMENTS Plan 1
Sheetz
C-7

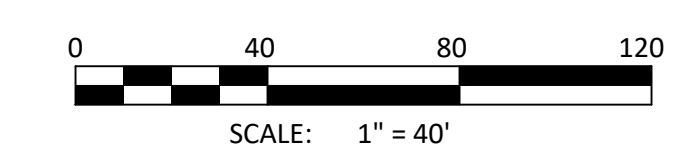
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I-26 OFF-RAMP EXISTING CONDITIONS
SCALE: 1"=20'



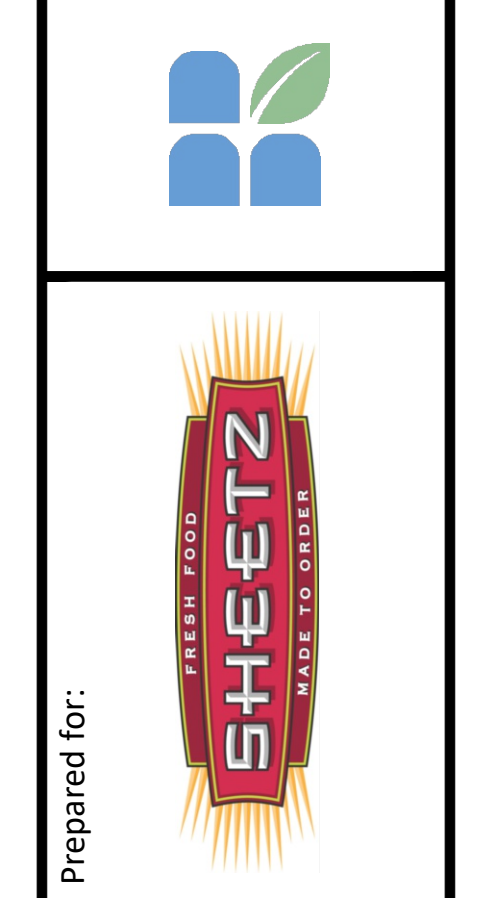
I-26 OFF-RAMP RE-STRIPING PLAN
SCALE: 1"=20'



ORIGINAL ISSUE DATE:	12/15/2025
REVISIONS:	A 02/27/2026 REMOVE CAR WASH/SHARED POND A 03/09/2026 SHEET COMMENTS A 03/13/2026 PROGRESS SET A 04/23/2026 REVISED PERMIT SET A 05/18/2026 REVISED SITE PLAN FOR PER APPROVAL A 05/21/2026 PLAN REVISIONS PER CITY/COUNTY A 05/28/2026 PLAN REVISIONS PER CITY/COUNTY
PROJECT #:	24039
DRAWN BY:	WH



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North Wilkesboro, NC 28659
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Firm #P-0205



Prepared for:
Roadway Improvements Plan 2
Sheetz
Upward Road
Hendersonville, NC

SHEET
C-8

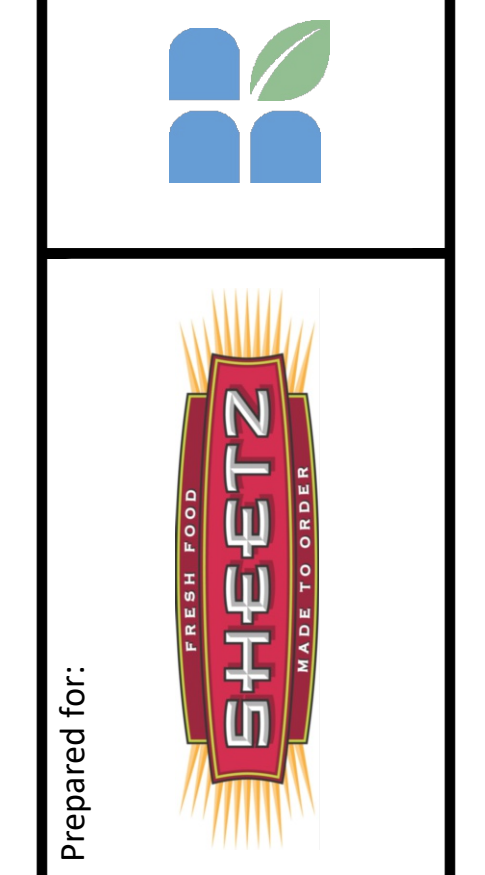
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Grading Plan
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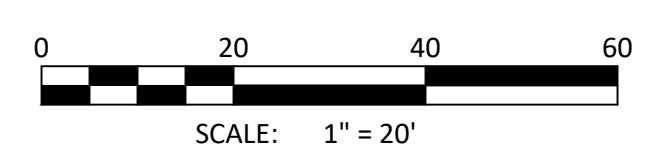
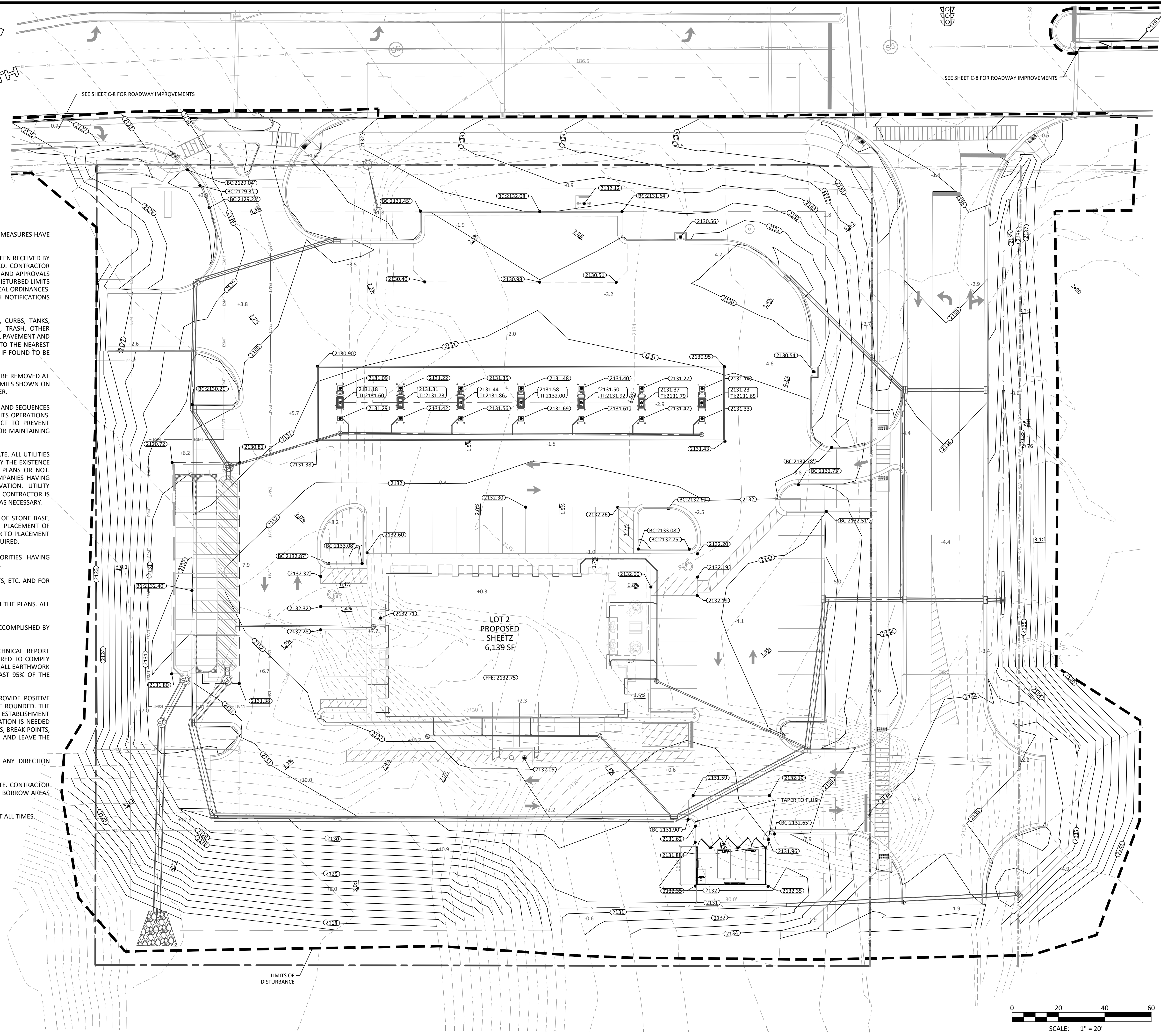
CLEARING AND GRADING NOTES

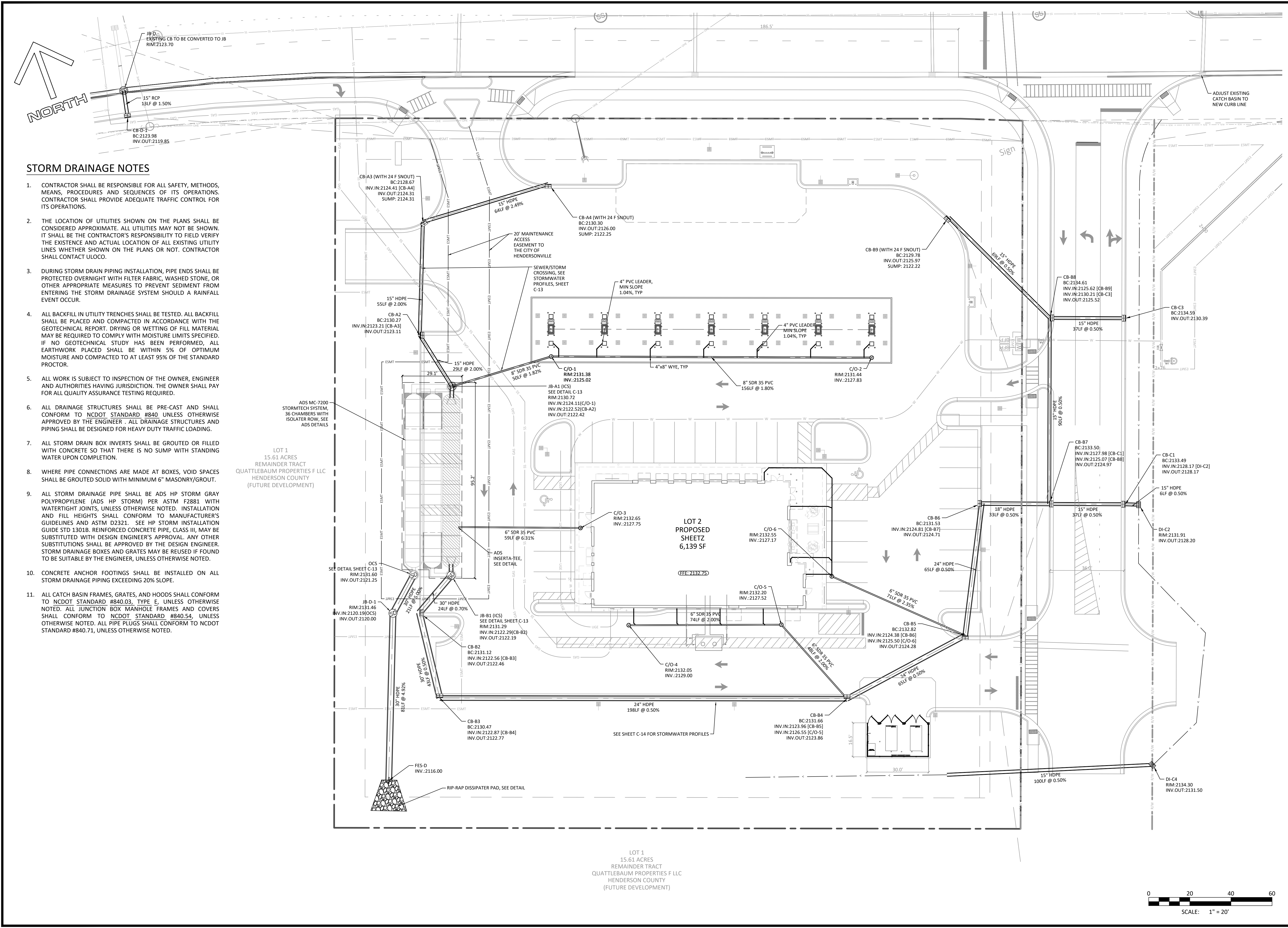
1. GRADING SHALL NOT PROCEED UNTIL APPROPRIATE EROSION AND SEDIMENT CONTROL MEASURES HAVE BEEN INSTALLED.
2. LAND DISTURBING ACTIVITIES SHALL NOT COMMENCE UNTIL APPROVAL TO DO SO HAS BEEN RECEIVED BY GOVERNING AUTHORITIES AND SEDIMENT CONTROL MEASURES HAVE BEEN INSTALLED. CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING OR VERIFYING THAT ALL ASSOCIATED PERMITS AND APPROVALS HAVE BEEN OBTAINED FOR THIS WORK. NO WORK SHALL BE PERFORMED OUTSIDE THE DISTURBED LIMITS SHOWN ON THIS PLAN. CONTRACTOR SHALL COMPLY WITH ALL FEDERAL, STATE AND LOCAL ORDINANCES. CONTRACTOR IS RESPONSIBLE FOR ALL NESHAP NOTIFICATIONS REQUIRED. ALL SUCH NOTIFICATIONS SHALL BE MADE AT LEAST TWO WEEKS PRIOR TO ANY DEMOLITION WORK.
3. ALL ASPHALT, TREES, STUMPS, BRUSH, ROOTS, STRUCTURES, WALLS, FOUNDATIONS, CURBS, TANKS, BARRELS, FLOOR SLABS, CONCRETE, METAL, SEPTIC TANKS, FENCING, SIGNS, POLES, TRASH, OTHER DEBRIS, AND UNSUITABLE FILL MATERIAL SHALL BE PROPERLY DISPOSED OF OFF-SITE. ALL PAVEMENT AND CONCRETE EDGES SHALL BE SAWCUT. ALL CONCRETE SHALL BE REMOVED/REPLACED TO THE NEAREST JOINT. STORM DRAINAGE BOXES, GRATES AND PIPE TO BE REMOVED MAY BE REUSED IF FOUND TO BE SUITABLE BY THE ENGINEER, UNLESS OTHERWISE NOTED.
4. CERTAIN TREES WITHIN 10 FEET OF THE TOP OF ANY CUT SLOPE MAY BE REQUIRED TO BE REMOVED AT THE DISCRETION OF THE ENGINEER. CERTAIN TREES WITHIN 5 FEET OF THE CLEARING LIMITS SHOWN ON THE DRAWINGS MAY BE REQUIRED TO BE REMOVED AT THE DISCRETION OF THE ENGINEER.
5. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL SAFETY, METHODS, MEANS, PROCEDURES AND SEQUENCES OF ITS OPERATIONS. CONTRACTOR SHALL PROVIDE ADEQUATE TRAFFIC CONTROL FOR ITS OPERATIONS. CONTRACTOR SHALL INSTALL ABUNDANT ORANGE SAFETY FENCING ON THE PROJECT TO PREVENT OTHERS FROM ENTERING THE WORK AREA. CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING SAFETY FENCING AS NECESSARY FOR THE DURATION OF THE PROJECT.
6. THE LOCATION OF UTILITIES SHOWN ON THE PLANS SHALL BE CONSIDERED APPROXIMATE. ALL UTILITIES MAY NOT BE SHOWN. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE EXISTENCE AND ACTUAL LOCATION OF ALL EXISTING UTILITY LINES WHETHER SHOWN ON THE PLANS OR NOT. CONTRACTOR SHALL CONTACT ULOCO. CONTRACTOR SHALL NOTIFY ALL UTILITY COMPANIES HAVING UNDERGROUND UTILITIES ON SITE OR WITHIN RIGHT-OF-WAY PRIOR TO EXCAVATION. UTILITY ADJUSTMENTS AND/OR REMOVAL/RELOCATION IS INCLUDED IN THE SCOPE OF WORK. CONTRACTOR IS RESPONSIBLE FOR WORK ASSOCIATED WITH MAINTAINING/REPAIRING UTILITY SERVICES AS NECESSARY.
7. ALL FILL PLACED SHALL PASS A PROOF-ROLL TEST BEFORE AND AFTER INSTALLATION OF STONE BASE, SLABS OR PAVEMENTS. SUBGRADE SHALL BE ACCEPTED BY THE ENGINEER PRIOR TO PLACEMENT OF STONE BASE. STONE BASE SHALL BE INSPECTED AND ACCEPTED BY THE ENGINEER PRIOR TO PLACEMENT OF ASPHALT. CONTRACTOR SHALL PROVIDE A LOADED TRUCK FOR ALL PROOF-ROLLS REQUIRED.
8. ALL WORK IS SUBJECT TO INSPECTION OF THE OWNER, ENGINEER AND AUTHORITIES HAVING JURISDICTION. THE OWNER SHALL PAY FOR ALL QUALITY ASSURANCE TESTING REQUIRED.
9. CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING NECESSARY APPROVALS, PERMITS, ETC. AND FOR PROPER DISPOSAL OF ANY MATERIAL REMOVED FROM THE SITE.
10. EXISTING DRAINAGE PATTERNS SHALL BE MAINTAINED UNLESS OTHERWISE SHOWN ON THE PLANS. ALL AREAS OF THE SITE SHALL BE SHAPED TO DRAIN TO EROSION CONTROL DEVICES.
11. THE CONTRACTOR SHALL KEEP ALL STREETS AND ROADWAYS CLEAN. THIS SHALL BE ACCOMPLISHED BY STREET SWEEPING AND WASHING AS NECESSARY ON A DAILY BASIS.
12. ALL FILL SHALL BE PLACED AND COMPACTED IN ACCORDANCE WITH THE GEOTECHNICAL REPORT PREPARED FOR THIS PROJECT. DRYING OR WETTING OF FILL MATERIAL MAY BE REQUIRED TO COMPLY WITH MOISTURE LIMITS SPECIFIED. IF NO GEOTECHNICAL STUDY HAS BEEN PERFORMED, ALL EARTHWORK PLACED SHALL BE WITHIN 5% OF OPTIMUM MOISTURE AND COMPACTED TO AT LEAST 95% OF THE STANDARD PROCTOR.
13. THE CONTRACTOR SHALL SMOOTH AND BLEND IN ALL GRADE TRANSITIONS AND PROVIDE POSITIVE DRAINAGE AROUND ALL BUILDINGS. THE TOP OF SLOPE FOR ALL CUT SLOPES SHALL BE ROUNDED. THE FINAL GRADE OF ALL CUT SLOPES SHALL BE SCARIFIED AND TRACKED AS NECESSARY FOR ESTABLISHMENT OF GROUND COVER. THE CONTRACTOR SHALL NOTIFY THE ENGINEER IF ANY CLARIFICATION IS NEEDED CONCERNING THE INTENT OF THE DRAWINGS, INCLUDING SPOT GRADES, CONTOUR LINES, BREAK POINTS, DRAINAGE PATTERNS, ETC. THE CONTRACTOR SHALL DRAG AND RAKE THE ENTIRE SITE AND LEAVE THE ENTIRE SITE IN A CLEAN, NEAT, SMOOTH CONDITIONS SUITABLE FOR MOWING.
14. SLOPES IN ADA PARKING SPACES AND ACCESS AISLES SHALL NOT EXCEED 2% IN ANY DIRECTION INCLUDING THE DIAGONAL.
15. THIS PROJECT MAY INCLUDE MATERIAL TO BE IMPORTED/EXPORTED TO/FROM THIS SITE. CONTRACTOR SHALL BE RESPONSIBLE FOR PERMITTING AND APPROVALS REQUIRED FOR ANY OFF-SITE BORROW AREAS OR WASTE AREAS.
16. CONTRACTOR SHALL MAINTAIN A STAMPED APPROVED SET OF PLANS ON THE JOB SITE AT ALL TIMES.

ESTIMATED EARTHWORK QUANTITY

CUT	
CUT TO F/G	8,960 CY
SLABS/PAVEMENT	1,847 CY
TOTAL	10,807 CY
FILL	
FILL TO F/G	11,054 CY
SLABS/PAVEMENT	(1,266CY)
SHRINKAGE, 12%	1,174 CY
TOTAL	10,962 CY

NOTE:
 QUANTITIES LISTED ARE APPROXIMATE AND ARE PROVIDED FOR REFERENCE ONLY. CONTRACTOR SHALL PERFORM ALL NECESSARY INVESTIGATION TO DETERMINE EXACT QUANTITIES OF WORK REQUIRED. STRIPPINGS ASSUMED TO BE WASTED ON SITE IN NON-STRUCTURAL AREAS.



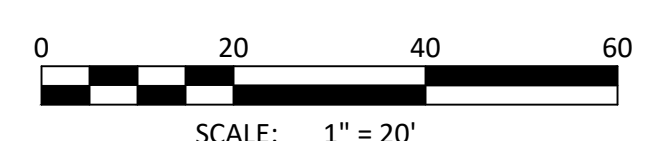


STORM DRAINAGE NOTES

- CONTRACTOR SHALL BE RESPONSIBLE FOR ALL SAFETY, METHODS, MEANS, PROCEDURES AND SEQUENCES OF ITS OPERATIONS. CONTRACTOR SHALL PROVIDE ADEQUATE TRAFFIC CONTROL FOR ITS OPERATIONS.
- THE LOCATION OF UTILITIES SHOWN ON THE PLANS SHALL BE CONSIDERED APPROXIMATE. ALL UTILITIES MAY NOT BE SHOWN. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE EXISTENCE AND ACTUAL LOCATION OF ALL EXISTING UTILITY LINES WHETHER SHOWN ON THE PLANS OR NOT. CONTRACTOR SHALL CONTACT ULOCO.
- DURING STORM DRAIN PIPING INSTALLATION, PIPE ENDS SHALL BE PROTECTED OVRNIGHT WITH FILTER FABRIC, WASHED STONE, OR OTHER APPROPRIATE MEASURES TO PREVENT SEDIMENT FROM ENTERING THE STORM DRAINAGE SYSTEM SHOULD A RAINFALL EVENT OCCUR.
- ALL BACKFILL IN UTILITY TRENCHES SHALL BE TESTED. ALL BACKFILL SHALL BE PLACED AND COMPACTED IN ACCORDANCE WITH THE GEOTECHNICAL REPORT. DRYING OR WETTING OF FILL MATERIAL MAY BE REQUIRED TO COMPLY WITH MOISTURE LIMITS SPECIFIED. IF NO GEOTECHNICAL STUDY HAS BEEN PERFORMED, ALL EARTHWORK PLACED SHALL BE WITHIN 5% OF OPTIMUM MOISTURE AND COMPACTED TO AT LEAST 95% OF THE STANDARD PROCTOR.
- ALL WORK IS SUBJECT TO INSPECTION OF THE OWNER, ENGINEER AND AUTHORITIES HAVING JURISDICTION. THE OWNER SHALL PAY FOR ALL QUALITY ASSURANCE TESTING REQUIRED.
- ALL DRAINAGE STRUCTURES SHALL BE PRE-CAST AND SHALL CONFORM TO NCDOT STANDARD #840 UNLESS OTHERWISE APPROVED BY THE ENGINEER. ALL DRAINAGE STRUCTURES AND PIPING SHALL BE DESIGNED FOR HEAVY DUTY TRAFFIC LOADING.
- ALL STORM DRAIN BOX INVERTS SHALL BE GROUTED OR FILLED WITH CONCRETE SO THAT THERE IS NO SUMP WITH STANDING WATER UPON COMPLETION.
- WHERE PIPE CONNECTIONS ARE MADE AT BOXES, VOID SPACES SHALL BE GROUTED SOLID WITH MINIMUM 6" MASONRY/GROUT.
- ALL STORM DRAINAGE PIPE SHALL BE ADS HP STORM GRAY POLYPROPYLENE (ADS HP STORM) PER ASTM F2881 WITH WATERTIGHT JOINTS, UNLESS OTHERWISE NOTED. INSTALLATION AND FILL HEIGHTS SHALL CONFORM TO MANUFACTURER'S GUIDELINES AND ASTM D2321. SEE HP STORM INSTALLATION GUIDE STD 1301B. REINFORCED CONCRETE PIPE, CLASS III, MAY BE SUBSTITUTED WITH DESIGN ENGINEER'S APPROVAL. ANY OTHER SUBSTITUTIONS SHALL BE APPROVED BY THE DESIGN ENGINEER. STORM DRAINAGE BOXES AND GRATES MAY BE REUSED IF FOUND TO BE SUITABLE BY THE ENGINEER, UNLESS OTHERWISE NOTED.
- CONCRETE ANCHOR FOOTINGS SHALL BE INSTALLED ON ALL STORM DRAINAGE PIPING EXCEEDING 20% SLOPE.
- ALL CATCH BASIN FRAMES, GRATES, AND HOODS SHALL CONFORM TO NCDOT STANDARD #840.03, TYPE E, UNLESS OTHERWISE NOTED. ALL JUNCTION BOX MANHOLE FRAMES AND COVERS SHALL CONFORM TO NCDOT STANDARD #840.54, UNLESS OTHERWISE NOTED. ALL PIPE PLUGS SHALL CONFORM TO NCDOT STANDARD #840.71, UNLESS OTHERWISE NOTED.

LOT 1
15.61 ACRES
REMAINDER TRACT
QUATTLEBAUM PROPERTIES F LLC
HENDERSON COUNTY
(FUTURE DEVELOPMENT)

LOT 1
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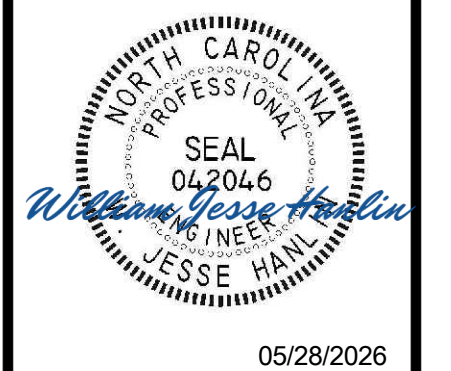
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PROJECT # 24039

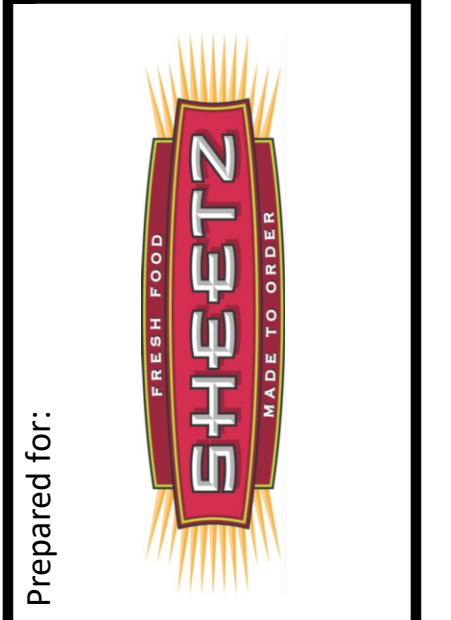
ADDITIONAL CHANGES:
REMOVE CAR WASH/SHARED POND
SHEET COMMENTS
PROGRESS SET

REVISIONS:
A 02/25/2026
B 03/09/2026
C 03/10/2026
D 04/23/2026
E 05/18/2026
F 05/21/2026
G 05/28/2026

REVISED PERMIT SET
REVISED SITE PLAN FOR PERMIT APPROVAL
PLAN REVISIONS PER CITY/COUNTY
PLAN REVISIONS PER CITY



Blue Ridge Engineering PLLC
924 Main Street - Suite 200
North Wilkesboro, NC 28659
336.838.2500
www.ridge.blue
Firm #P-0205



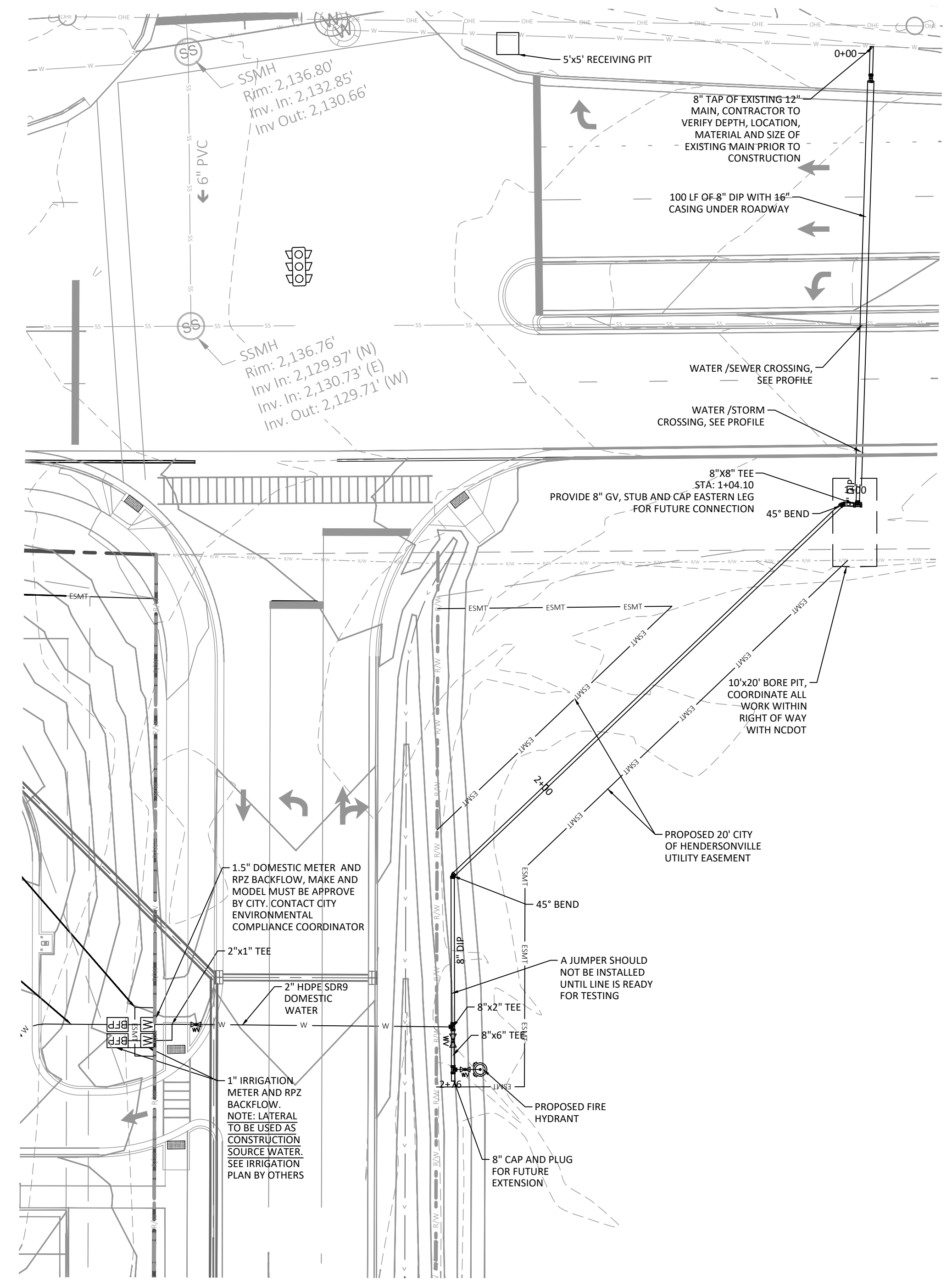
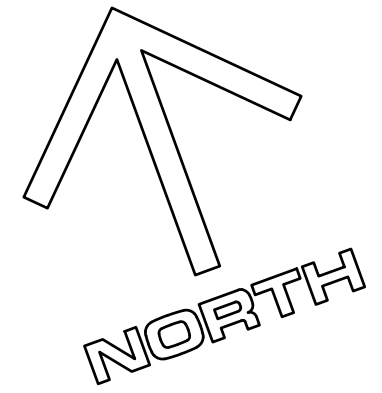
Prepared for:
Stormwater Plan
Sheetz
Upward Road
Hendersonville, NC

SHEET
C-10

FOR PERMITTING - NOT FOR CONSTRUCTION

UTILITY NOTES

- CONSTRUCTION SHALL NOT COMMENCE UNTIL A NON-DISCHARGE PERMIT HAS BEEN ISSUED AND A PRECONSTRUCTION CONFERENCE HAS BEEN HELD WITH THE CONTRACTOR AND CITY OF HENDERSONVILLE INSPECTOR ON SITE.
- ALL SANITARY SEWER CONSTRUCTION SHALL COMPLY WITH CITY OF HENDERSONVILLE TECHNICAL SPECIFICATIONS AND CONSTRUCTION DETAILS. CONTRACTOR IS RESPONSIBLE FOR ALL TESTING AS REQUIRED BY CITY OF HENDERSONVILLE
- CONTRACTOR SHALL BE RESPONSIBLE FOR ALL SAFETY, METHODS, MEANS, PROCEDURES AND SEQUENCES OF ITS OPERATIONS. CONTRACTOR SHALL PROVIDE ADEQUATE TRAFFIC CONTROL FOR ITS OPERATIONS.
- ALL UTILITY WORK SHALL BE PERFORMED BY A NC LICENSED UTILITY CONTRACTOR.
- THE LOCATION OF UTILITIES SHOWN ON THE PLANS SHALL BE CONSIDERED APPROXIMATE. ALL UTILITIES MAY NOT BE SHOWN. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE EXISTENCE AND ACTUAL LOCATION OF ALL EXISTING UTILITY LINES WHETHER SHOWN ON THE PLANS OR NOT. CONTRACTOR SHALL CONTACT ULOCO.
- DURING PIPE LAYING, PIPE ENDS SHALL BE PROTECTED OVERNIGHT WITH APPROPRIATE MEASURES TO PREVENT WATER, DIRT OR OTHER DEBRIS FROM ENTERING THE SYSTEM.
- ALL BACKFILL IN UTILITY TRENCHES SHALL BE TESTED. ALL BACKFILL SHALL BE PLACED AND COMPACTED IN ACCORDANCE WITH THE GEOTECHNICAL REPORT. DRYING OR WETTING OF FILL MATERIAL MAY BE REQUIRED TO COMPLY WITH MOISTURE LIMITS SPECIFIED. IF NO GEOTECHNICAL STUDY HAS BEEN PERFORMED, ALL EARTHWORK PLACED SHALL BE WITHIN 5% OF OPTIMUM MOISTURE AND COMPACTED TO AT LEAST 95% OF THE STANDARD PROCTOR.
- ALL WORK IS SUBJECT TO INSPECTION OF THE OWNER, ENGINEER AND AUTHORITIES HAVING JURISDICTION. THE OWNER SHALL PAY FOR ALL QUALITY ASSURANCE TESTING REQUIRED.
- ALL MANHOLES SHALL CONFORM TO NCDOT STANDARD #1525 UNLESS OTHERWISE NOTED. ALL MANHOLE INVERTS SHALL BE GROUTED OR FILLED WITH CONCRETE SO THAT THERE IS NO SUMP WITH STANDING WATER UPON COMPLETION.
- ALL PIPE MATERIALS SHALL BE AS SPECIFIED ON THE PLANS. NO SUBSTITUTIONS SHALL BE ALLOWED. ALL FIRE LINES SHALL COMPLY WITH NFPA 24 AND STATE BUILDING CODE: FIRE CODE.
- ALL WATER LINE 6" AND LARGER SHALL HAVE RESTRAINED JOINTS, "MEGALUG" OR EQUAL.
- MINIMUM SLOPE FOR ANY SEWER LINE SHALL BE 1.0% UNLESS OTHERWISE SHOWN. CONCRETE ANCHOR FOOTINGS SHALL BE INSTALLED ON ALL SEWER LINES EXCEEDING 20% SLOPE.
- WATER/SEWER CROSSINGS: WHERE SEWER MAINS CROSS OVER WATER MAINS, OR WHERE LESS THAN 18" VERTICAL SEPARATION IS PROVIDED, BOTH WATER MAIN AND SEWER MAIN SHALL BE CONSTRUCTED TO WATER MAIN STANDARDS, SUCH AS THE USE OF DUCTILE IRON PIPE OR C-900, A MINIMUM OF 10 FEET IN EACH DIRECTION.
- SEWER/STORM CROSSINGS: WHERE SEWER MAINS AND STORM DRAINS CROSS WITH LESS THAN 18" VERTICAL SEPARATION PROVIDED, SEWER MAINS SHALL BE CONSTRUCTED TO WATER MAIN STANDARDS, SUCH AS THE USE OF DUCTILE IRON PIPE OR C-900, A MINIMUM OF 10 FEET IN EACH DIRECTION.
- WATER/STORM CROSSINGS: WHERE WATER MAINS AND STORM DRAINS CROSS WITH LESS THAN 12" VERTICAL SEPARATION PROVIDED, WATER MAINS SHALL BE DUCTILE IRON PIPE, A MINIMUM OF 10 FEET IN EACH DIRECTION.
- ALL SERVICE CONNECTIONS SHALL BE INSTALLED IN ACCORDANCE WITH STATE AND LOCAL CODES. CLEANOUTS SHALL BE INSTALLED ON ALL SEWER SERVICE LINES 6" AND SMALLER, AT MINIMUM 75' SPACING.
- CONTRACTOR SHALL BE RESPONSIBLE FOR AND SHALL PROVIDE ALL APPARATUS NECESSARY FOR ALL PRESSURE TESTING, LEAKAGE TESTING, AND BIOLOGICAL TESTING. ALL TESTING SHALL BE PERFORMED IN THE PRESENCE OF THE ENGINEER OR HIS REPRESENTATIVE.
- CONTRACTOR SHALL COORDINATE ALL WORK AS NECESSARY WITH: POWER COMPANY; TELEPHONE COMPANY; GAS COMPANY; ETC.

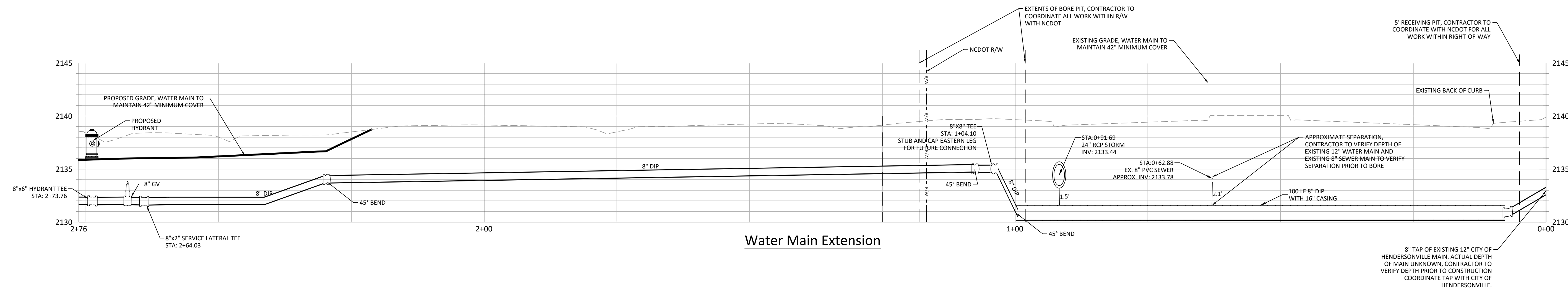


JOINT RESTRAINT TABLE
ALL JOINT RESTRAINTS SHALL COMPLY WITH CITY OF HENDERSONVILLE STANDARDS AND DETAILS, SEE DETAIL

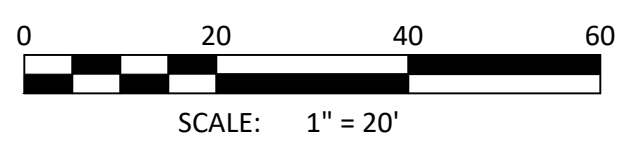
JOINT	JOINT STA	LENGTH(EACH SIDE)
8" 45°	1+00	17'
8"x8" TEE	1+04	72'
8" 45°	1+07	17'
8" 45°	2+30	17'
8"x2" TEE	2+64	72'
8"x6" TEE/END	2+74	72'

WATER QUANTITIES

SIZE	MATERIAL	LENGTH(LF)
8"	CLASS 350 DIP	276
APPURTENANCES	SIZE	QTY
HYDRANTS	-	1
GATE VALVE	8"	2
GATE VALVE	6"	1



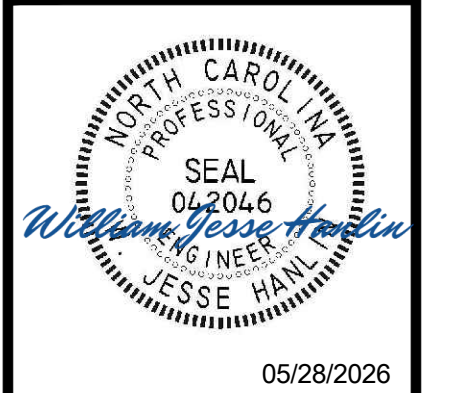
WATER MAIN EXTENSION PROFILE
HORIZONTAL SCALE: 1" = 10'
VERTICAL SCALE: 1" = 5'



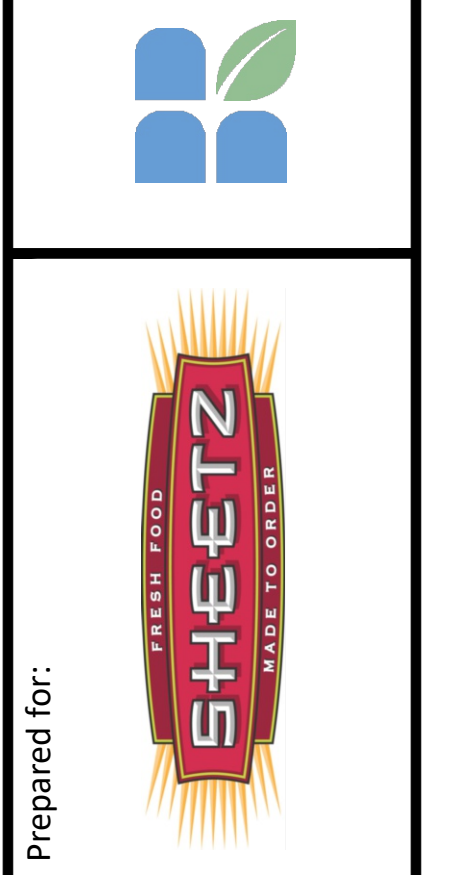
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DRAWN BY: WHH
PROJECT #: 24039
ORIGINAL ISSUE DATE: 12/15/2025

REVISIONS:
A 02/22/2026 ADDITIONAL GRADING
B 03/09/2026 REMOVE CAR WASH/SHARED POND
C 03/10/2026 SHEET COMMENTS
D 03/13/2026 PROGRESS SET
E 04/23/2026 REVISED PERMIT SET
F 05/18/2026 REVISED SITE PLAN FOR PERMIT APPROVAL
G 05/21/2026 PLAN REVISIONS PER CITY/COUNTY
H 05/28/2026 PLAN REVISIONS PER CITY



Blue Ridge Engineering PLLC
924 Main Street - Suite 200
North Wilkesboro, NC 28659
336.838.2500
www.rridge.blue
Firm #P-0205



Water Main Extension Plan and Profile
Sheetz
Upward Road
Hendersonville, NC

SHEET
C-12

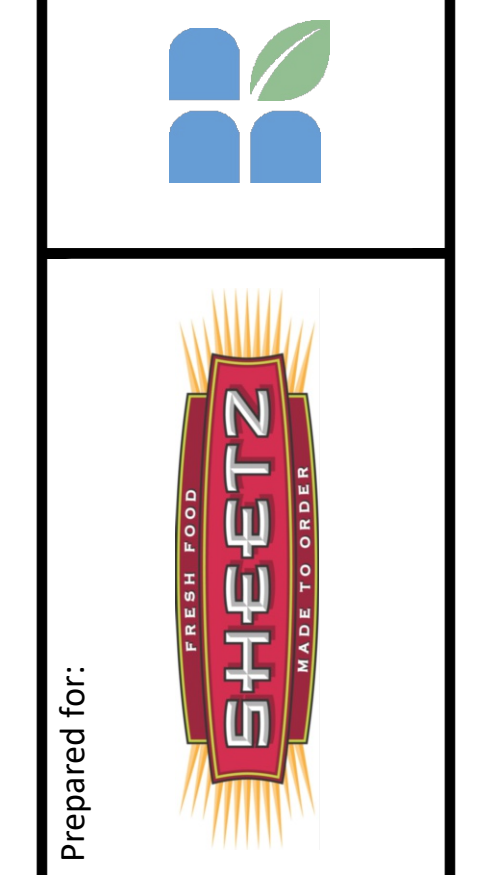
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DRAWN BY: WIH
 PROJECT #: 24039
 ORIGINAL ISSUE DATE: 12/15/2025
 REVISIONS:
 A 02/22/2026 REMOVE CAR WASH/SHARED POND SHEET COMMENTS
 B 03/02/2026 PROGRESS SET
 C 03/12/2026 REVISED PERMIT SET
 D 05/19/2026 REVISED SITE PLAN FOR PER APPROVAL
 E 05/21/2026 PLAN REVISIONS PER CITY/COUNTY
 F 05/29/2026 PLAN REVISIONS PER CITY

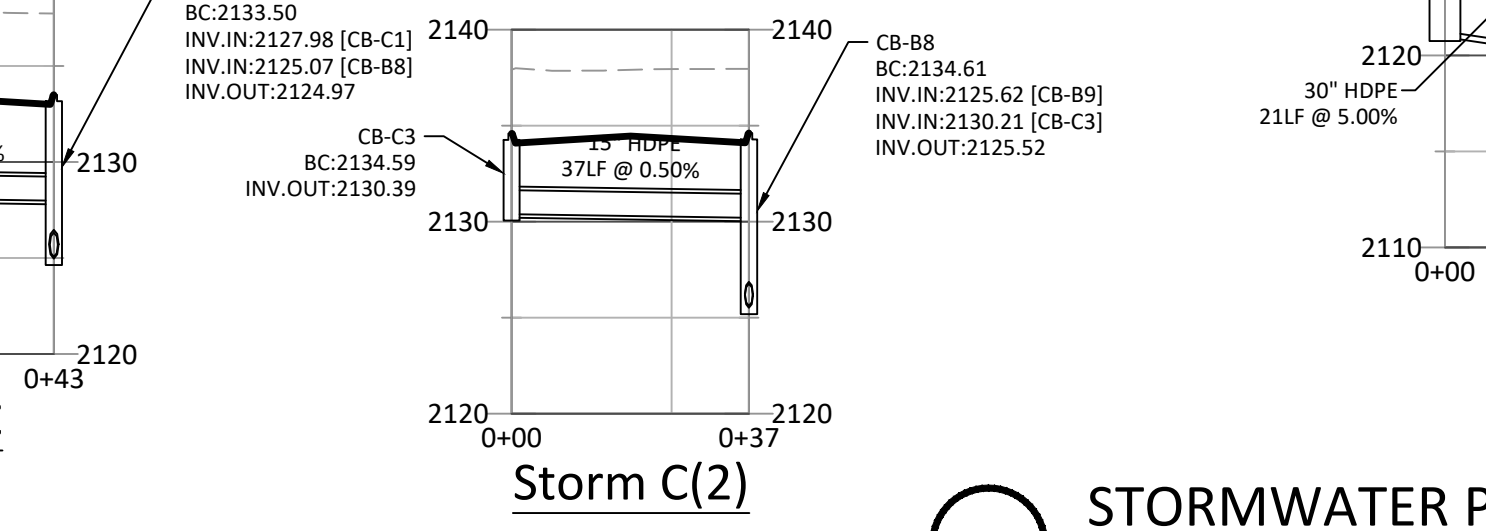
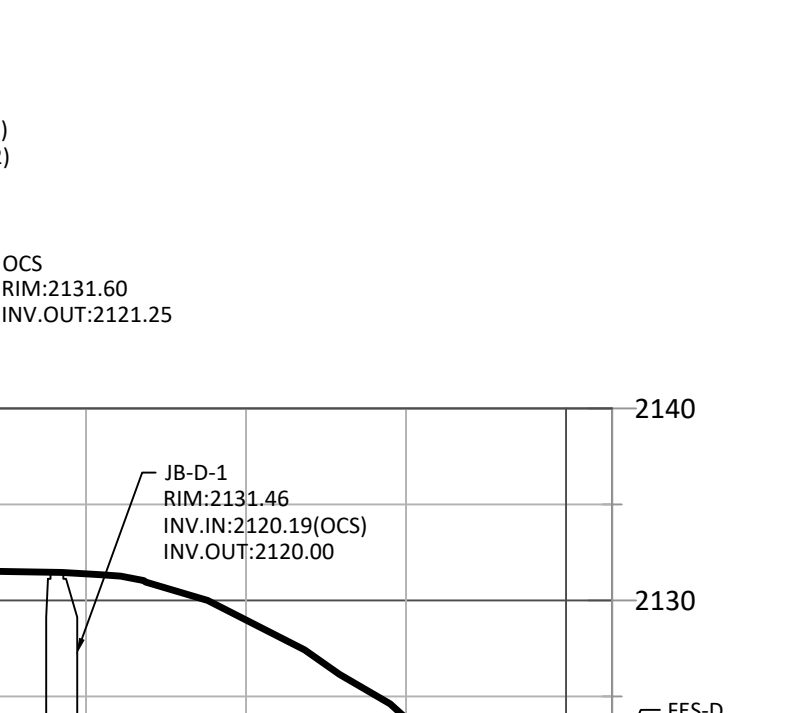
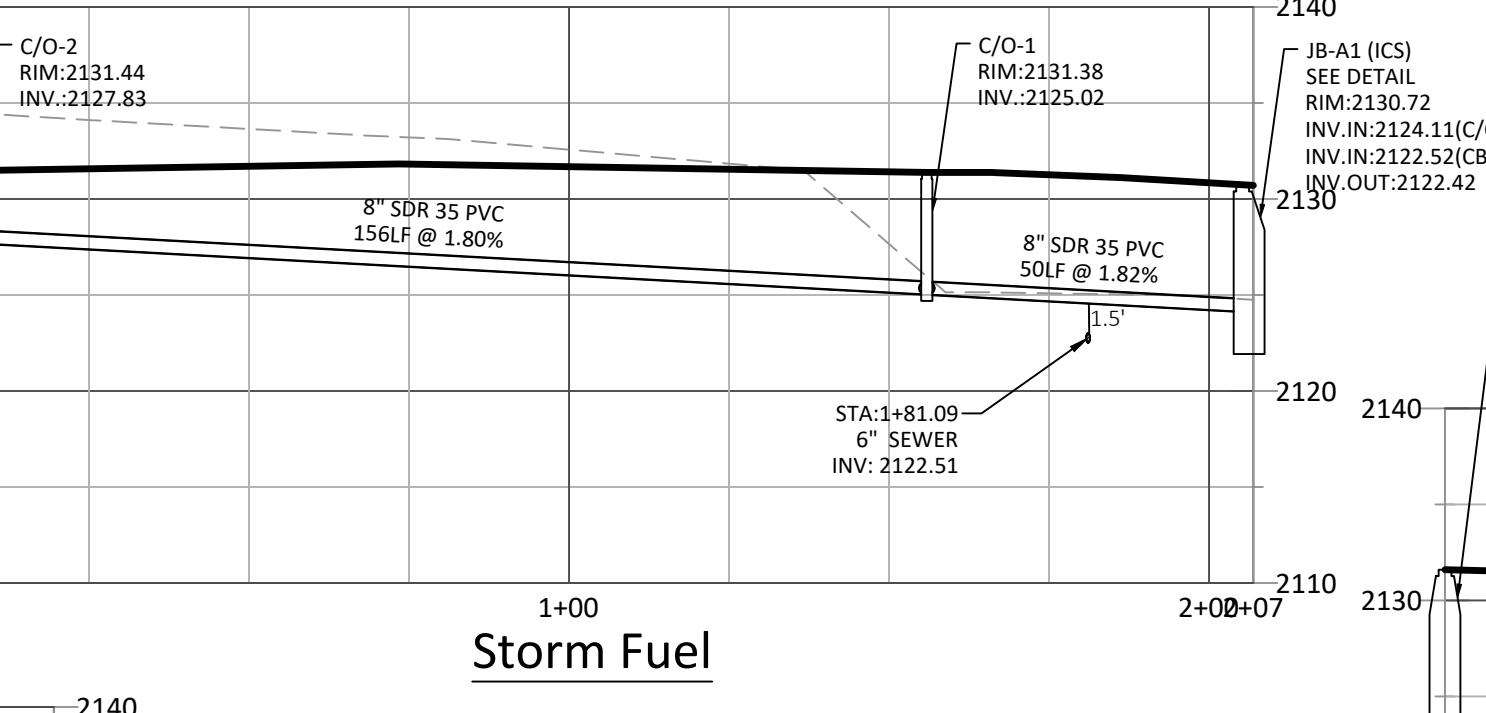
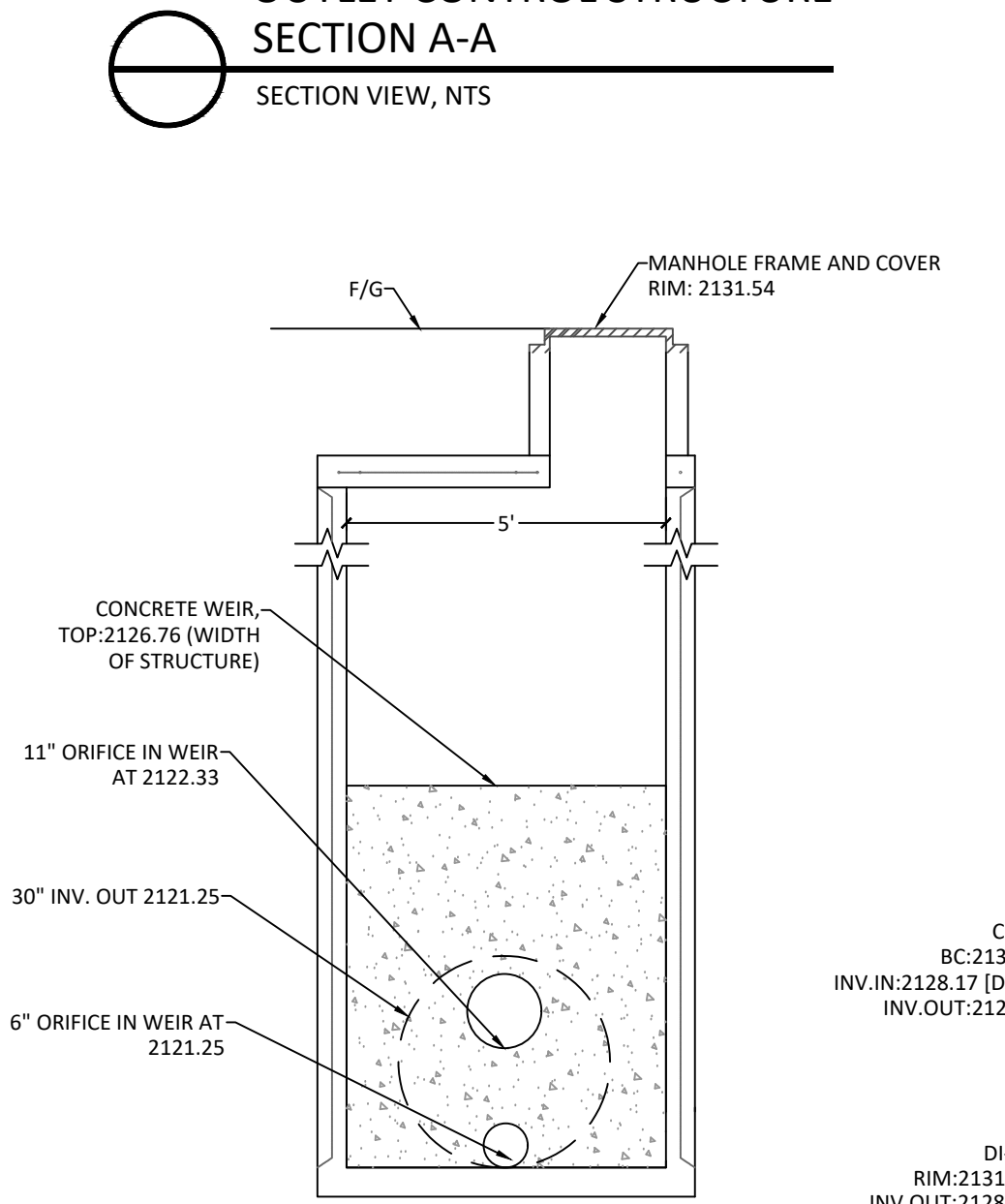
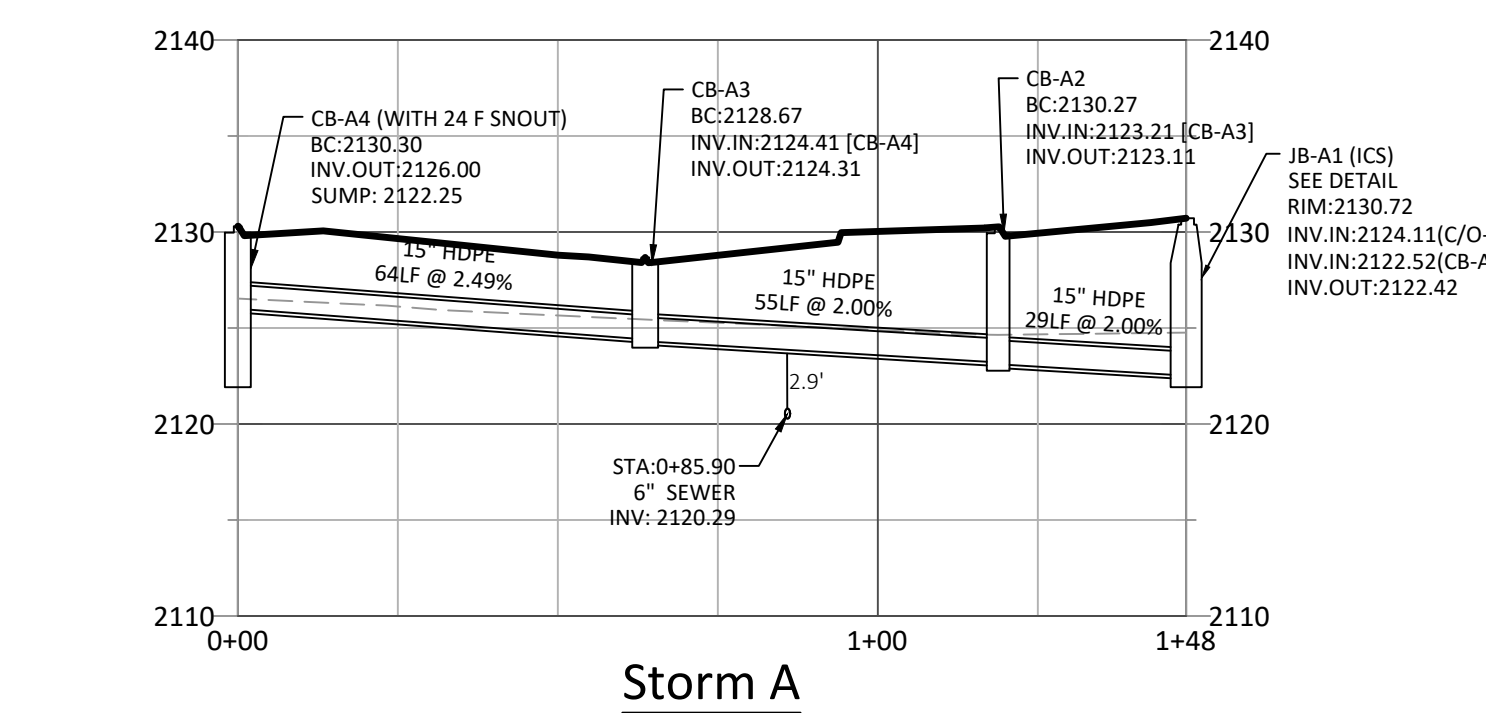
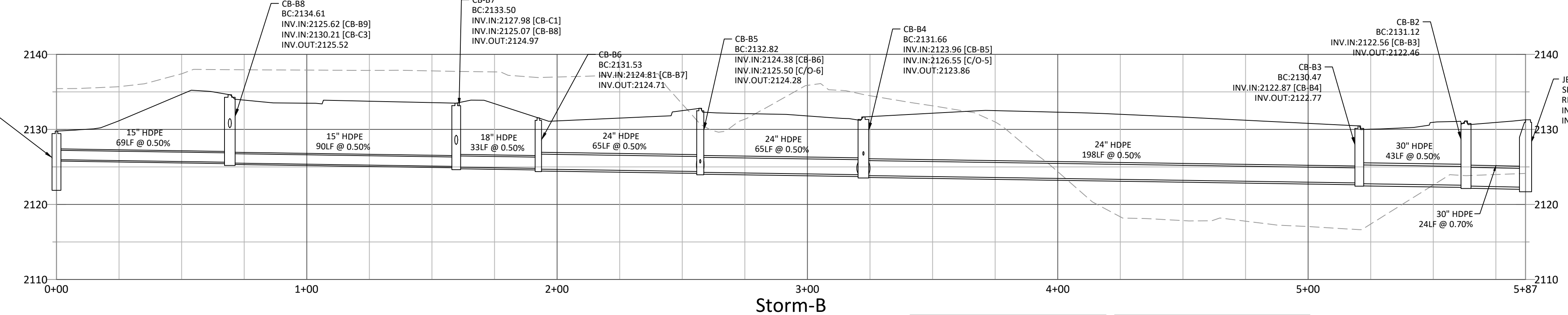
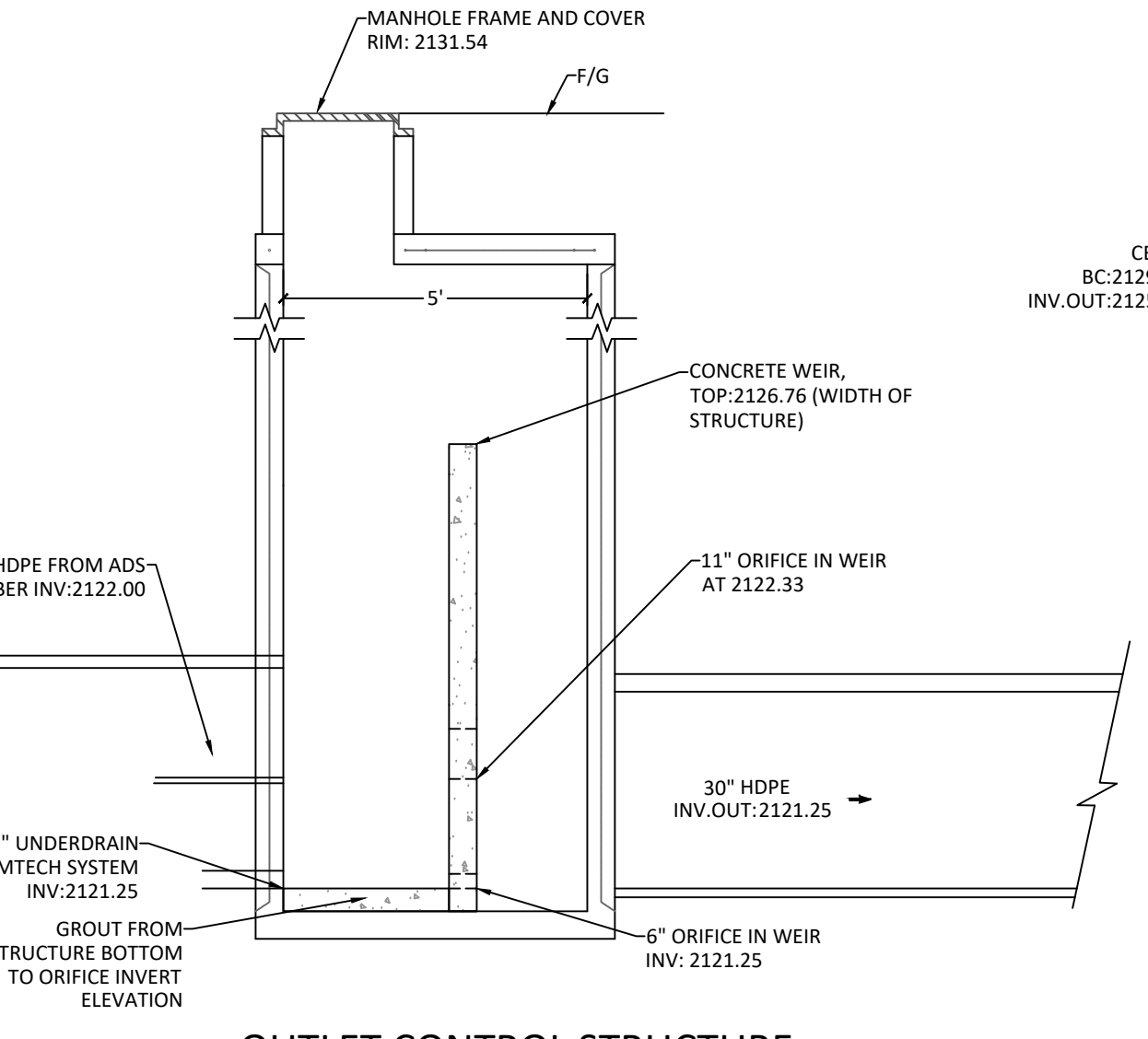
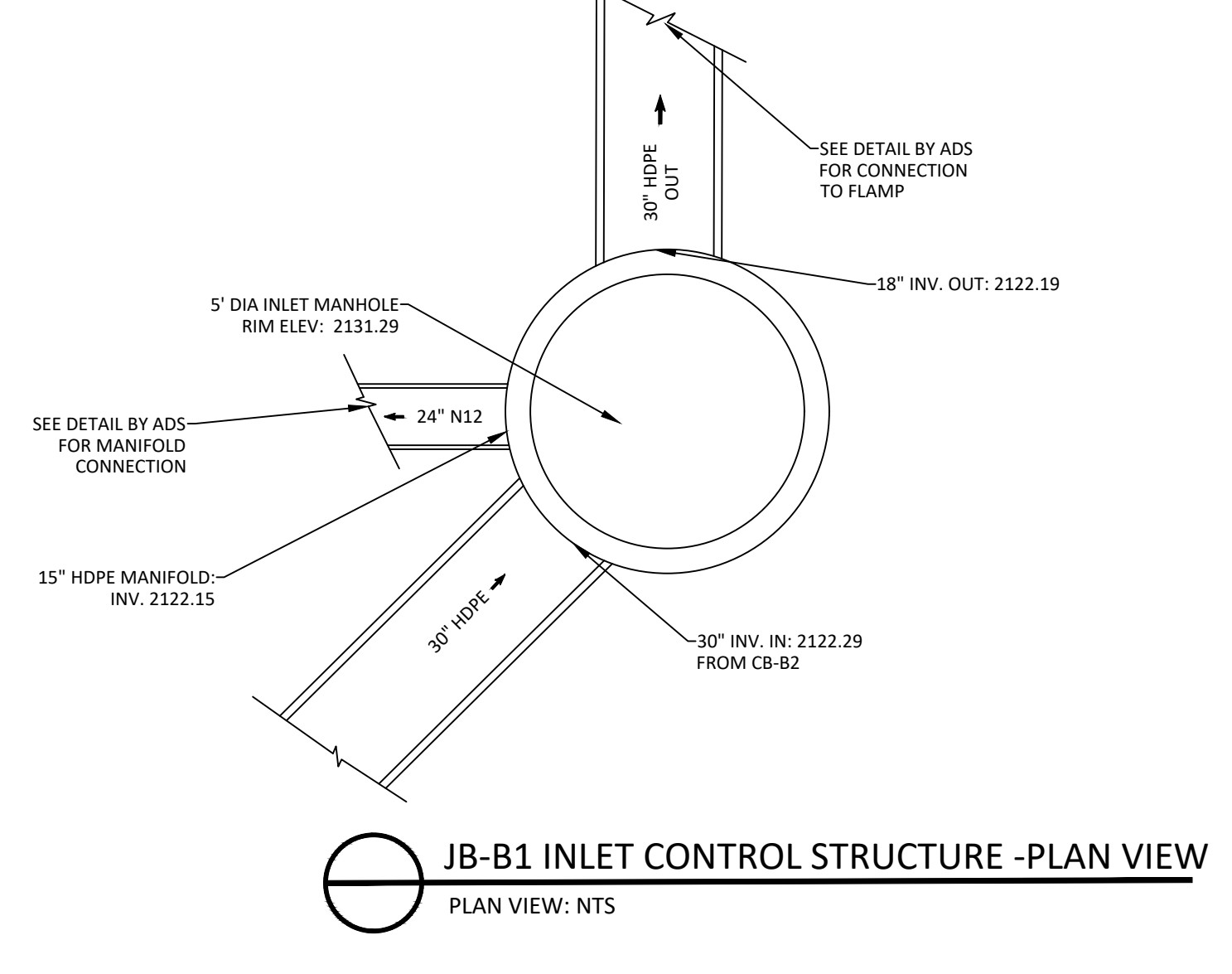
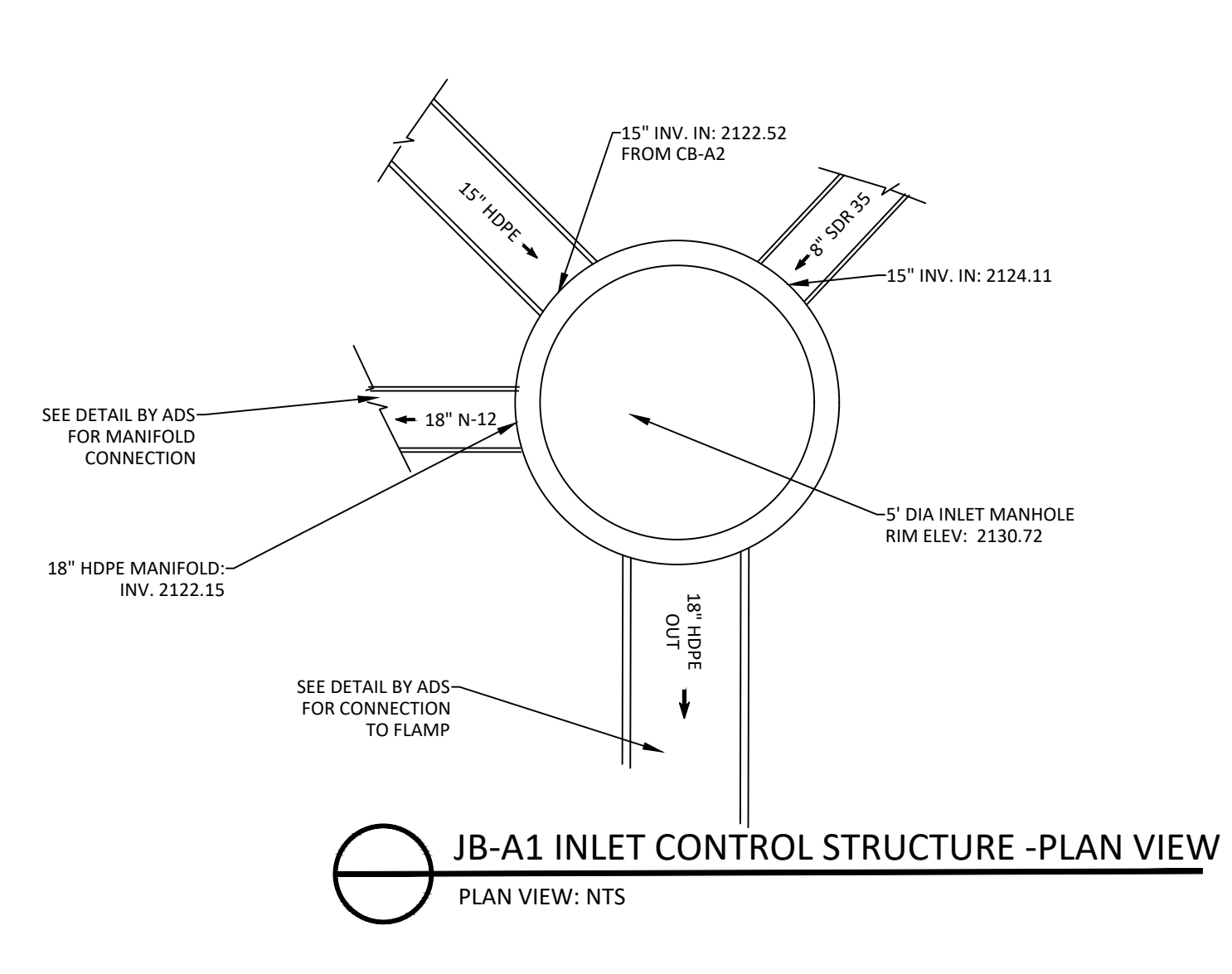
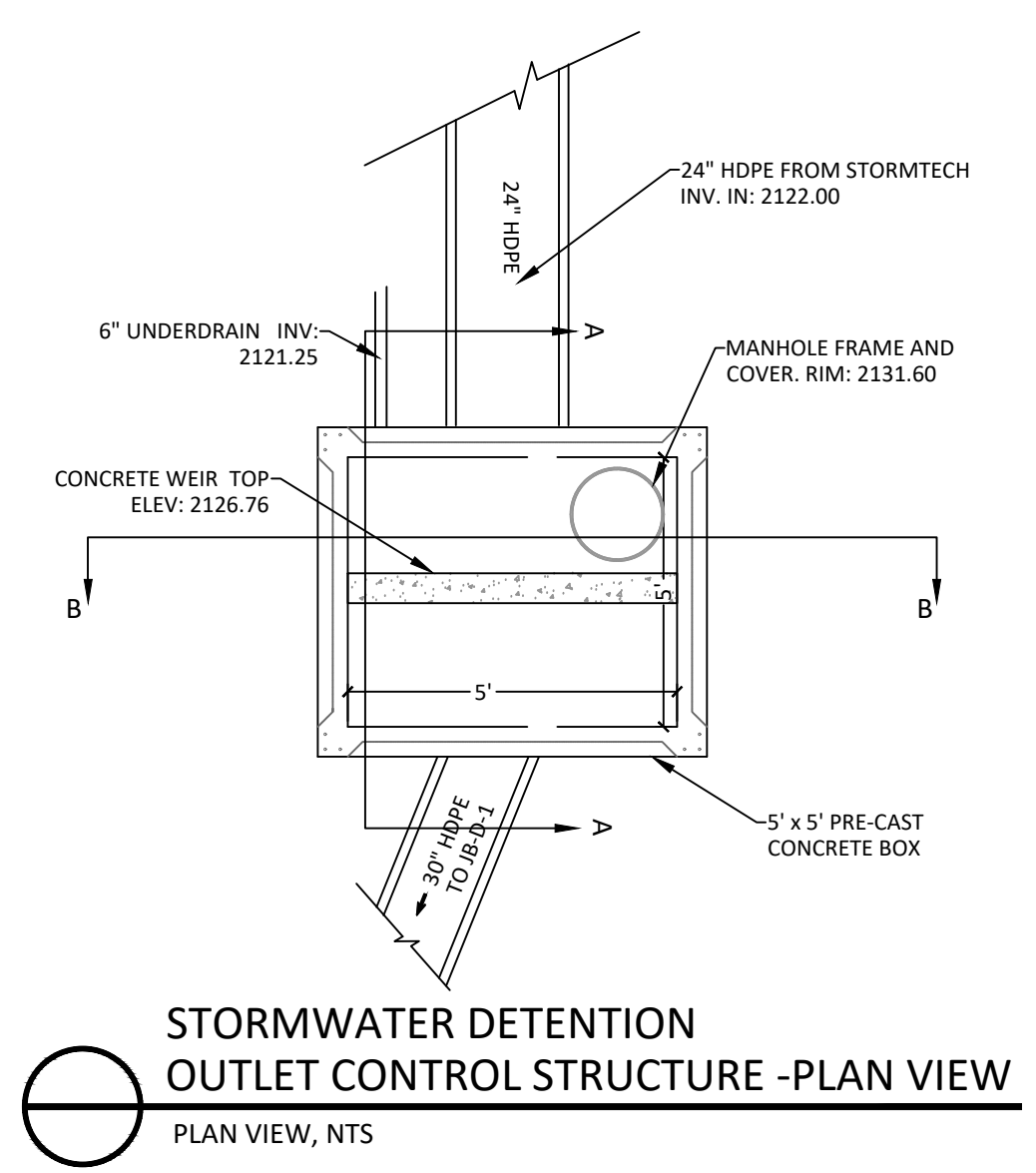


Blue Ridge Engineering PLLC
 924 Main Street, Suite 200
 North Wilkesboro, NC 28659
 www.rridge.blue
 Firm #P-0205



Prepared for:
 Stormwater Profiles
 Sheetz
 Upward Road
 Hendersonville, NC

C-13



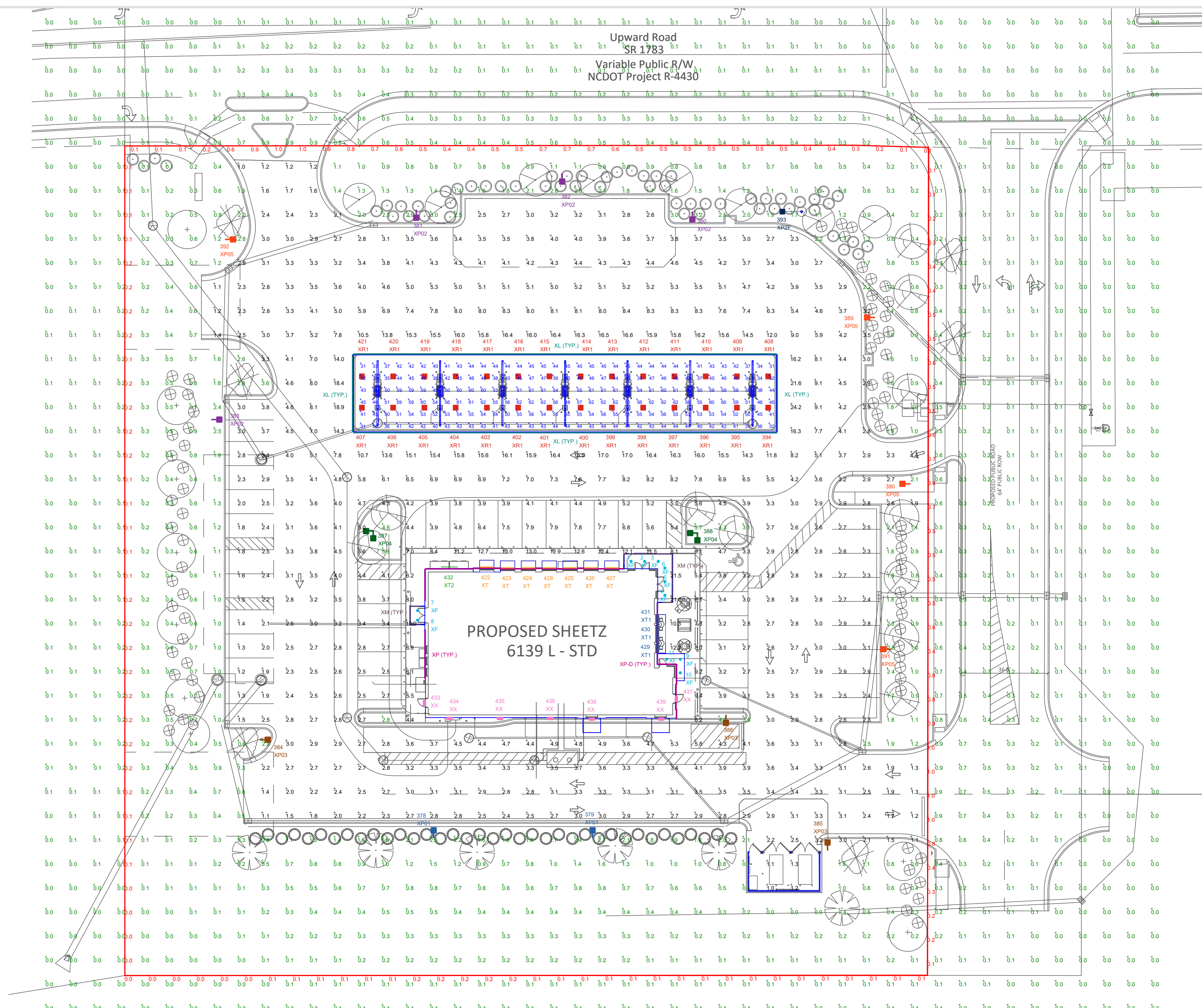
STORMWATER PROFILES
 HORIZONTAL SCALE: 1" = 30'
 VERTICAL SCALE: 1" = 10'

Structure Name	Structure Details
C/O-1	RIM: 2131.38 Pipe-CO2 INV IN = 2125.02 Pipe-CO1 INV OUT = 2125.02
C/O-2	RIM: 2131.44 Pipe-CO2 INV OUT = 2127.83
C/O-3	RIM: 2132.65 Pipe-CO3 INV OUT = 2127.75
C/O-4	RIM: 2132.05 Pipe-CO4 INV OUT = 2129.00
C/O-5	RIM: 2132.20 Pipe-CO4 INV IN = 2127.52 Pipe-CO5 INV OUT = 2127.52
C/O-6	RIM: 2132.55 Pipe-CO6 INV OUT = 2127.17
CB-A2	RIM: 2130.27 Pipe-A3 INV IN = 2123.21 Pipe-A2 INV OUT = 2123.11
CB-A3	RIM: 2128.67 Pipe-A4 INV IN = 2124.41 Pipe-A3 INV OUT = 2124.31
CB-A4	RIM: 2130.30 Pipe-A4 INV OUT = 2126.00
CB-B2	RIM: 2131.12 Pipe-B3 INV IN = 2122.56 Pipe-B2 INV OUT = 2122.46
CB-B3	RIM: 2130.47 Pipe-B4 INV IN = 2122.87 Pipe-B3 INV OUT = 2122.77
CB-B4	RIM: 2131.66 Pipe-B5 INV IN = 2123.95 Pipe-CO5 INV IN = 2126.55 Pipe-B4 INV OUT = 2123.86
CB-B5	RIM: 2132.82 Pipe-B6 INV IN = 2124.38 Pipe-CO6 INV IN = 2125.50 Pipe-B5 INV OUT = 2124.28
CB-B6	RIM: 2131.53 Pipe-B7 INV IN = 2124.81 Pipe-B6 INV OUT = 2124.71
CB-B7	RIM: 2133.50 Pipe-C1 INV IN = 2127.98 Pipe-B8 INV IN = 2125.07 Pipe-B7 INV OUT = 2124.97
CB-B8	RIM: 2134.61 Pipe-B9 INV IN = 2125.62 Pipe-C3 INV IN = 2130.21 Pipe-B8 INV OUT = 2125.52
JB-A1 (CS)	SEE DETAIL RIM: 2130.72 INV IN: 2124.11 (C/O-1) INV IN: 2122.52 (CB-A2) INV OUT: 2122.42
JB-B1 (CS)	SEE DETAIL RIM: 2131.12 INV IN: 2122.56 (CB-B3) INV OUT: 2122.46
JB-B1 (CS)	SEE DETAIL RIM: 2131.29 INV IN: 2122.29 Pipe-B1 INV OUT = 2122.19
JB-D-1	RIM: 2131.46 Pipe-OCS INV IN = 2120.19 Pipe-D1 INV OUT = 2120.00
OCS	RIM: 2131.60 Pipe-OCS INV OUT = 2121.25

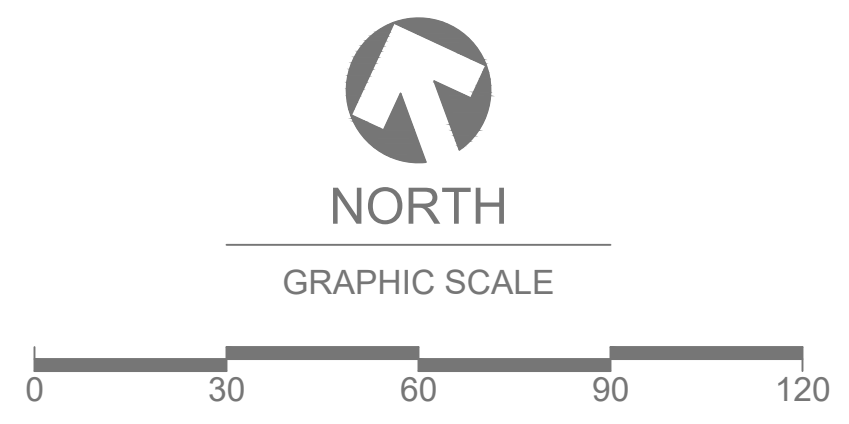
Structure Name	Structure Details
CB-B8	RIM: 2134.61 Pipe-B9 INV IN = 2125.62 Pipe-C3 INV IN = 2130.21 Pipe-B8 INV OUT = 2125.52
CB-B9	RIM: 2129.78 Pipe-B9 INV OUT = 2125.97
CB-C1	RIM: 2133.49 Pipe-C2 INV IN = 2128.17 Pipe-C1 INV OUT = 2128.17
CB-C3	RIM: 2134.59 Pipe-C3 INV OUT = 2130.39
DI-C2	RIM: 2131.91 Pipe-C2 INV OUT = 2128.20
DI-C4	RIM: 2134.44 Pipe-C4 INV OUT = 2131.50
FES-D	RIM: 2119.29 Pipe-D1 INV IN = 2116.00
JB-A1 (CS)	RIM: 2130.72 Pipe-CO1 INV IN = 2124.11 Pipe-A2 INV IN = 2122.52 Pipe-A1 INV OUT = 2122.42
JB-B1 (CS)	RIM: 2131.29 Pipe-B2 INV IN = 2122.29 Pipe-B1 INV OUT = 2122.19
JB-D-1	RIM: 2131.46 Pipe-OCS INV IN = 2120.19 Pipe-D1 INV OUT = 2120.00
OCS	RIM: 2131.60 Pipe-OCS INV OUT = 2121.25

Pipe Name	Size	Length	Slope	Start Structure	End Structure
Pipe-A1	18.000	6'	21.16%	JB-A1 (ICS)	JB-A1 (ICS)
Pipe-A2	15.000	29'	2.00%	CB-A2	JB-A1 (ICS)
Pipe-A3	15.000	55'	2.00%	CB-A3	CB-A2
Pipe-A4	15.000	64'	2.49%	CB-A4	CB-A3
Pipe-B1	18.000	6'	2.00%	JB-B1 (ICS)	CB-C3
Pipe-B2	30.000	24'	0.70%	CB-B2	JB-B1 (ICS)
Pipe-B3	30.000	43'	0.50%	CB-B3	CB-B2
Pipe-B4	24.000	198'	0.50%	CB-B4	CB-B3
Pipe-B5	24.000	65'	0.50%	CB-B5	CB-B4
Pipe-B6	24.000	65'	0.50%	CB-B6	CB-B5
Pipe-B7	18.000	33'	0.50%	CB-B7	CB-B6
Pipe-B8	15.000	90'	0.50%	CB-B8	CB-B7
Pipe-B9	15.000	69'	0.50%	CB-B9	CB-B8
Pipe-C1	15.000	37'	0.50%	CB-C1	CB-B7
Pipe-C2	15.000	6'	0.50%	DI-C2	CB-C1

Pipe Name	Size	Length	Slope	Start Structure	End Structure
Pipe-C3	15.000	37'	0.50%	CB-C3	CB-B8
Pipe-C4	15.000	100'	0.50%	DI-C4	DI-C4
Pipe-CO1	8.000	50'	1.82%	C/O-1	JB-A1 (ICS)
Pipe-CO2	8.000	156'	1.80%	C/O-2	C/O-1
Pipe-CO3	6.000	59'	6.31%	C/O-3	C/O-3
Pipe-CO4	6.000	74'	2.00%	C/O-4	C/O-5
Pipe-CO5	6.000	48'	2.00%	C/O-5	CB-B4
Pipe-CO6	6.000	71'	2.35%	C/O-6	CB-B5
Pipe-D1	30.000	81'	4.92%	JB-D-1	FES-D
Pipe-OCS	30.000	21'	5.00%	OCS	JB-D-1



LUM NO.	LABEL	MTG. HT.	TILT
1 - 11	XF	11.33	0
12	XL	20.875	0
13	XL	18.25	5
14	XL	18.599	5
15	XL	18.948	5
16	XL	19.297	5
17	XL	19.646	5
18	XL	19.995	5
19	XL	20.344	5
20	XL	20.693	5
21	XL	18.25	5
22	XL	18.599	5
23	XL	18.948	5
24	XL	19.297	5
25	XL	19.646	5
26	XL	19.995	5
27	XL	20.344	5
28	XL	20.693	5
29 - 71	XL	20.875	0
72 - 115	XL	18.076	0
116 - 131	XM	12	0
132 - 215	XP	18	0
216 - 226	XP	19	0
227 - 295	XP	18	0
296 - 327	XP-D	18	0
328 - 330	XP-D	19	0
331 - 377	XP-D	18	0
378, 379	XP01	23	0
380 - 383	XP02	23	0
384 - 386	XP03	23	0
387, 388	XP04	23	0
389 - 392	XP05	23	0
393	XP2F	1	166
394 - 407	XR1	17.67	0
408 - 421	XR1	18.83	0
422 - 428	XT	13.5	50
429 - 431	XT1	13.5	50
432	XT2	15	0
433 - 436	XX	15.42	0
437	XX	11.33	0
438, 439	XX	9.33	0



NOTE:
 - AREA LIGHTS ON NEW 30 FT. POLES MOUNTED ON 3 FT. CONCRETE BASES
 - FOOTCANDLE LEVELS CALCULATED AT GRADE USING INITIAL LUMEN VALUES UNLESS NOTED OTHERWISE.

LABEL	AVG	MAX	MIN	AVG/MIN	MAX/MIN
PROPERTY LINES	0.32	1.0	0.0	N.A.	N.A.
SITE PAVED AREA	5.03	24.2	1.0	5.03	24.20
UNDEFINED	0.32	5.7	0.0	N.A.	N.A.
UNDER CANOPY	47.97	62	31	1.55	2.00

THIS SITE IS LOCATED IN A REGION WHERE LIGHTING IS REGULATED BY LOCAL ORDINANCES


SYMBOL	QTY	LABEL	ARRANGEMENT	LUMENS	LLF	BUG RATING	WATTS/LUMINAIRE	TOTAL WATTS	MANUFACTURER	CATALOG LOGIC
⊙	11	XF	Single	1037	1.000	N.A.	10	110	Amerlux, LLC	ORDX4-10-409-M-CSTM-RB-UDM
—	104	XL	SINGLE	136	1.000	N.A.	4.12	428.48	Blair Companies	LINEAR STRIP LIGHT (BY OTHERS, SHOWN FOR CONTRIBUTION)
—	16	XM	SINGLE	136	0.700	N.A.	4.12	65.92	Blair Companies	LB-110-00-XX-025-RD[1]FR4-15
→	164	XP	Single	305	1.000	N.A.	3.66	600.24	P-LED	STREET WRAP FLEX BACK BEND 4000K
↖	82	XP-D	Single	25	1.000	N.A.	0.305	25.01	P-LED	STREET WRAP FLEX BACK BEND 4000K
—	2	XP01	Single	11129	1.020	B2-U0-G3	93	186	Lithonia Lighting	DSX0 LED P4 40K 70CRI T3M MVOLT SPA FAO DXXXX
—	4	XP02	Single	9647	1.020	B1-U0-G3	93	372	LITHONIA LIGHTING	DSX0 LED P4 40K 70CRI T3M MVOLT SPA FAO HS DXXXX
—	3	XP03	Single	11295	1.020	B2-U0-G3	93	279	Lithonia Lighting	DSX0 LED P4 40K 70CRI T4M MVOLT SPA FAO DXXXX
—	2	XP04	2 @ 90 degrees	11295	1.020	B2-U0-G3	93	372	Lithonia Lighting	DSX0 LED P4 40K 70CRI T4M MVOLT SPA FAO DXXXX
—	4	XP05	Single	9717	1.020	B1-U0-G3	93	372	LITHONIA LIGHTING	DSX0 LED P4 40K 70CRI T4M MVOLT SPA FAO HS DXXXX
—	1	XP2F	Single	11830	1.030	N.A.	100	100	LITHONIA LIGHTING	DSXF2 LED P3 40K 70CRI NSP MVOLT IS DXXXX + MOUNTING POST
—	28	XR1	Single	15146	1.000	B3-U0-G2	107	2996	Lithonia Lighting	RCNY LED AL02 50K 90CRI SYMF MVOLT BZS DWHXD SETTING 6
—	7	XT	Single	1840	1.000	N.A.	20	140	SPI Lighting Inc.	SEW12146 5FT L20W AN08 120-277V 4000K PSE SMA OAP (6.00 IN.) FIXT. HEAD FACTORY SET TO 50 DEGREE TILT AWAY FROM BLDG.
—	3	XT1	Single	1472	1.000	N.A.	16	48	SPI Lighting Inc.	SEW12146 4FT L16W AN08 120-277V 4000K PSE SMA OAP (6.00 IN.) FIXT. HEAD FACTORY SET TO 50 DEGREE TILT AWAY FROM BLDG.
—	1	XT2	Single	2208	1.000	N.A.	24	24	SPI Lighting Inc.	SEW12146 6FT L24W AN08 120-277V 4000K PSE SMA OAP12
—	7	XX	Single	1921	1.000	B1-U0-G0	16	112	WILLIAMS OUTDOOR	VWM-H-L17/840-TL-DBZ-CGL-DIM-UNV (BRONZE COLOR 80CRI)

DISCLAIMER

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QTY	LABEL	DESCRIPTION
11	XF	ORDX4-10-409-M-CSTM-RB-UDM



ORDX4
Open Rated Exterior Downlight

Project:

Features and Specs

Construction:

- Cast aluminum top housing with integral driver.
- Dust and water tight (IP66) sealed optical chamber.
- Die-cast aluminum trim and faceplate.
- Stainless steel hardware.

Finish:

- Architectural Grade Polyester Powder Coat.

Options:

- Beam Spread: 30°, 24°, 30°
- CCT: 2700K, 3000K, 4000K
- CRN 50 or 80+ typ.
- Polycarbonate TR optic with white backing for maximum efficiency.

Electrical:

- Integral LED driver included, 120-277V.
- Primary wiring compartment with power supply.
- Standard driver supports 40 different dimming protocols - TRIAC/ELV/DIM/DMX.
- Dimming percentage less than 1% - All dimming methods.

Adjustable from 0.017" to 1.02" ceiling thickness.

Lifespan:

- Estimated 50,000+ hour LED lifespan based on IES LM-80 results and TM-30 colorations.

Lighting:

- UL listed for wet locations (suitable for use in open or unenclosed applications).


Assembled in the USA

5 year limited warranty

5 year limited warranty

5 year limited warranty

QTY	LABEL	DESCRIPTION
164	XP	STREET WRAP FLEX BACK BEND 4000K
82	XP-D	STREET WRAP FLEX BACK BEND 4000K




STREET WRAP™ FLEX BACK-BEND

Our binder solution is perfect for adding an extra punch of color to your building. It's primary customization or set-up. Easy wrapping your building quickly and easily.

Need to understand how this light will emit. May need to be shielded to prevent uplift.

QTY	LABEL	DESCRIPTION
2	XP01	DSXO LED P4 40K 70CRI T3M MVOLT SPA FAO DXXXX
4	XP02	DSXO LED P4 40K 70CRI T3M MVOLT SPA FAO HS DXXXX
3	XP03	DSXO LED P4 40K 70CRI T4M MVOLT SPA FAO DXXXX
2	XP04	DSXO LED P4 40K 70CRI T4M MVOLT SPA FAO HS DXXXX
4	XP05	DSXO LED P4 40K 70CRI T4M MVOLT SPA FAO HS DXXXX

QTY	LABEL	DESCRIPTION
1	XP2F	DSXF2 LED P3 40K 70CRI NSP MVOLT IS DXXXX + MOUNTING POST



DSXF2 LED Floodlight

Additional Fixture Info

Introduction

The D-Series floodlights feature a sleek wide offering to meet specifier's every lighting need in application. The D-Series flood offers three sizes delivering 3,000 to 27,000 lumens. Available with seven precision optics, three mountings and three color temperatures, D-Series floodlights offer vast design capabilities while delivering significant energy savings and long life.

The DSXF2 delivers 7,000 to 17,000 lumens, meeting a large breadth of illumination requirements for design and renovation when replacing 175W, 250W and 400W HID floodlights. All configurations are assembled in the USA allowing for quick delivery.

Specifications

EPA: 0.8 ft² (0.07 m²)

Depth: 4.32" (0.11 m)

Width: 12.87" (0.33 m)

Height: 7.81" (0.20 m)

Overall Height: 15.33" (0.39 m)

Weight: 12.0 lbs

EXAMPLE: DSXF2 LED P1 40K 70CRI MSP MVOLT THK DBXDK

Ordering Information

Series	Performance Package	Color Temperature	CR	Beamwidth	Voltage	Mounting
DSXF2 LED	P1	30K	300K	70X3	120V	DBXDK
	P2	40K	400K	40X3	120V	DBXDK
	P3	50K	500K	30X3	120V	DBXDK


Shipped installed: DBXDK: Dark Bronze, DBXDK: Black, DBXDK: Natural Aluminum, DBXDK: White

Shipped separately: DBXDK: Dark Bronze, DBXDK: Black, DBXDK: Natural Aluminum, DBXDK: White

Accessories:

- DBXDK: Dark Bronze
- DBXDK: Black
- DBXDK: Natural Aluminum
- DBXDK: White

QTY	LABEL	DESCRIPTION
28	XR1	RCNY LED AL02 50K 90CRI SYMF MVOLT BZS DWXHO SETTING 6



RCNY LED Recessed LED Canopy/Ceiling Luminaire

Additional Fixture Info

Introduction

The RCNY LED recessed luminaire is designed to provide exceptional energy savings and versatility in one fixture. With a compact design the RCNY provides superior illumination while blending into the canopy space seamlessly. The RCNY is unique in that as a recessed canopy it can be installed in both 12" and 16" canopy decks. Features such as Adjustable Lumen Output Dial with 8 Settings and forward throw distribution (ASYC) option make the RCNY one of the most versatile gas station canopies in the market. Lumen dial and driver are both accessible from below. With up to 17V LPW the RCNY LED delivers up to 90% in energy savings when replacing metal halide luminaires. With over 100,000 hour life expectancy (12+ years of 24/7 continuous operation), the RCNY LED luminaire provides significant maintenance savings over traditional luminaires. Class 1 Division 2 also available. Wet location junction box available upon request.

Specifications

RCNY LED

Width: 11.5"

Height: 5.9"

Length: 11.5"

Weight: 14lbs

EXAMPLE: RCNY LED AL02 50K 70CRI SYMF MVOLT BZS DWXHO

Ordering Information

Series	Package	Color Temperature	CR	Lens
RCNY LED	AL02	30K/300K	300K	DBXDK
	AL02	40K/400K	400K	DBXDK
	AL02	50K/500K	500K	DBXDK

Shipped installed: DBXDK: Dark Bronze, DBXDK: Black, DBXDK: Natural Aluminum, DBXDK: White

Shipped separately: DBXDK: Dark Bronze, DBXDK: Black, DBXDK: Natural Aluminum, DBXDK: White

Accessories:

- DBXDK: Dark Bronze
- DBXDK: Black
- DBXDK: Natural Aluminum
- DBXDK: White

ORDX4
Open Rated Exterior Downlight

Project:

Features and Specs

Construction:

- Cast aluminum top housing with integral driver.
- Dust and water tight (IP66) sealed optical chamber.
- Die-cast aluminum trim and faceplate.
- Stainless steel hardware.

Finish:

- Architectural Grade Polyester Powder Coat.

Options:

- Beam Spread: 30°, 24°, 30°
- CCT: 2700K, 3000K, 4000K
- CRN 50 or 80+ typ.
- Polycarbonate TR optic with white backing for maximum efficiency.

Electrical:

- Integral LED driver included, 120-277V.
- Primary wiring compartment with power supply.
- Standard driver supports 40 different dimming protocols - TRIAC/ELV/DIM/DMX.
- Dimming percentage less than 1% - All dimming methods.

Adjustable from 0.017" to 1.02" ceiling thickness.

Lifespan:

- Estimated 50,000+ hour LED lifespan based on IES LM-80 results and TM-30 colorations.

Lighting:

- UL listed for wet locations (suitable for use in open or unenclosed applications).

Assembled in the USA

5 year limited warranty

5 year limited warranty

5 year limited warranty

Performance Data (with Medium beam)

Wattage	Delivered Lumens	Color Temp/CR
10	~1040	4000K/90+
30	~3080	4000K/90+

Ordering Information

Model	Wattage	CCT/CR	Beam Spread	Finish	Driver
ORDX4	10 W	27 2700K/80-CRI	SP spot 10°	BLK black	UDM universal 4-in dimming track/ELV-DIM/DMX
	30 W	30 3000K/80-CRI	M narrow 24°	BLK white	
	30 W	40 4000K/90-CRI	M medium 20°	BLK white	
	30 W	50 5000K/90-CRI	M medium 20°	BLK white	
	30 W	50 5000K/90-CRI	M medium 20°	BLK white	
	30 W	50 5000K/90-CRI	M medium 20°	BLK white	
	30 W	50 5000K/90-CRI	M medium 20°	BLK white	
	30 W	50 5000K/90-CRI	M medium 20°	BLK white	
	30 W	50 5000K/90-CRI	M medium 20°	BLK white	
	30 W	50 5000K/90-CRI	M medium 20°	BLK white	

Shield Accessories

External Glare Shield (EGSR)

House Side Shield (HS)

Drilling

HANDHOLE ORIENTATION

DSXO Area Luminaire - EPA

DSXO Area Luminaire - EPA

Mounting, Options and Accessories

Mountings:

- IF-Adjustable Hanger (fits 2.3" x 0.5" lens)
- TKC2-Tab with 1/2" x 3/8" hole
- TKC-Threaded Bracket with 1/2" NPT threaded pipe
- WFF-Flange, Top Mounted
- WFF-Flange, Bottom Mounted
- FF-Full Flange

External Shields:

- WFF-Flange, Top Mounted
- WFF-Flange, Bottom Mounted
- FF-Full Flange

Accessories:

- VC-Tandem Guard
- DSXF2-TIK-Splitter Accessory

Streetstone Design Studio
LIGHTING DESIGN

SHEETZ 2.0

STREET WRAP™ FLEX BACK-BEND

Our binder solution is perfect for adding an extra punch of color to your building. It's primary customization or set-up. Easy wrapping your building quickly and easily.

Specifications

Dimensions: 24" and Custom Lengths Available

Input: 24V

Operating Temp: -47°F to 45°C

Mount: 3/16" x 1/8" Flat Head

Power Supply: P-LED 24V

Production Code: 1007

Start Range: Max. 10'

Features:

- Break free track
- 1.3" max. bending radius
- Die cast extrusion, offset slotted lens and thermal conductive base
- Die cast aluminum
- Applications: street lights, outdoor building and sign lighting, and more lighting applications
- UV resistant
- Stainless steel mounting eye
- 3500° F (1900° C) max. surface temp.

Product Options

Color	Mounting	Socket Color	Size	Color Temperature	CR	Beam Spread	Finish
White	305K	White	305	3000K	300K	30°	DBXDK
White	305K	White	305	4000K	400K	30°	DBXDK
White	305K	White	305	5000K	500K	30°	DBXDK
White	305K	White	305	3000K	300K	40°	DBXDK
White	305K	White	305	4000K	400K	40°	DBXDK
White	305K	White	305	5000K	500K	40°	DBXDK
White	305K	White	305	3000K	300K	50°	DBXDK
White	305K	White	305	4000K	400K	50°	DBXDK
White	305K	White	305	5000K	500K	50°	DBXDK

IMPORTANT WARRANTY INFO

This warranty is void if any cuts are made in the field. Any cuts must be made by Principal LED to maintain warranty.

Ordering Information

Model	Wattage	CCT/CR	Beam Spread	Finish	Driver
STREET WRAP	10 W	27 2700K/80-CRI	SP spot 10°	BLK black	UDM universal 4-in dimming track/ELV-DIM/DMX
	30 W	30 3000K/80-CRI	M narrow 24°	BLK white	
	30 W	40 4000K/90-CRI	M medium 20°	BLK white	
	30 W	50 5000K/90-CRI	M medium 20°	BLK white	
	30 W	50 5000K/90-CRI	M medium 20°	BLK white	
	30 W	50 5000K/90-CRI	M medium 20°	BLK white	
	30 W	50 5000K/90-CRI	M medium 20°	BLK white	
	30 W	50 5000K/90-CRI	M medium 20°	BLK white	
	30 W	50 5000K/90-CRI	M medium 20°	BLK white	
	30 W	50 5000K/90-CRI	M medium 20°	BLK white	

Streetstone Design Studio
LIGHTING DESIGN

SHEETZ 2.0

D-Series Size 0 LED Area Luminaire

Specifications

EPA: 0.44 ft² (0.04 m²)

Length: 26.18" (0.67 m)

Width: 14.06" (0.36 m)

Height H1: 2.26" (0.06 m)

Height H2: 7.46" (0.19 m)

Weight: 23 lbs

Introduction

The modern styling of the D-Series features a highly refined aesthetic that blends seamlessly with its environment. The D-Series offers the benefits of the latest in LED technology into a high performance, high efficiency, long life luminaire.

The photometric performance results in sites with excellent uniformity, greater pole spacing and lower power density. D-Series outstanding photometry aids in reducing the number of poles required in area lighting applications, with typical energy savings of 70% and expected service life of over 100,000 hours.

Ordering Information

Series	Color Temperature	CR	Beamwidth	Voltage	Mounting
DSX0 LED	30K	300K	70X3	120V	DBXDK
	40K	400K	40X3	120V	DBXDK
	50K	500K	30X3	120V	DBXDK

Shipped installed: DBXDK: Dark Bronze, DBXDK: Black, DBXDK: Natural Aluminum, DBXDK: White

Shipped separately: DBXDK: Dark Bronze, DBXDK: Black, DBXDK: Natural Aluminum, DBXDK: White

Accessories:

- DBXDK: Dark Bronze
- DBXDK: Black
- DBXDK: Natural Aluminum
- DBXDK: White

Streetstone Design Studio
LIGHTING DESIGN

SHEETZ 2.0

D-Series DSXF2 LED Floodlight

Specifications

EPA: 0.8 ft² (0.07 m²)

Depth: 4.32" (0.11 m)

Width: 12.87" (0.33 m)

Height: 7.81" (0.20 m)

Overall Height: 15.33" (0.39 m)

Weight: 12.0 lbs

Introduction

The D-Series floodlights feature a sleek wide offering to meet specifier's every lighting need in application. The D-Series flood offers three sizes delivering 3,000 to 27,000 lumens. Available with seven precision optics, three mountings and three color temperatures, D-Series floodlights offer vast design capabilities while delivering significant energy savings and long life.

The DSXF2 delivers 7,000 to 17,000 lumens, meeting a large breadth of illumination requirements for design and renovation when replacing 175W, 250W and 400W HID floodlights. All configurations are assembled in the USA allowing for quick delivery.

Ordering Information

Series	Performance Package	Color Temperature	CR	Beamwidth	Voltage	Mounting
DSXF2 LED	P1	30K	300K	70X3	120V	DBXDK
	P2	40K	400K	40X3	120V	DBXDK
	P3	50K	500K	30X3	120V	DBXDK

Shipped installed: DBXDK: Dark Bronze, DBXDK: Black, DBXDK: Natural Aluminum, DBXDK: White

Shipped separately: DBXDK: Dark Bronze, DBXDK: Black, DBXDK: Natural Aluminum, DBXDK: White

Accessories:

- DBXDK: Dark Bronze
- DBXDK: Black
- DBXDK: Natural Aluminum
- DBXDK: White

Streetstone Design Studio
LIGHTING DESIGN

SHEETZ 2.0

RCNY LED Recessed LED Canopy/Ceiling Luminaire

Specifications

RCNY LED

Width: 11.5"

Height: 5.9"

Length: 11.5"

Weight: 14lbs

Introduction

The RCNY LED recessed luminaire is designed to provide exceptional energy savings and versatility in one fixture. With a compact design the RCNY provides superior illumination while blending into the canopy space seamlessly. The RCNY is unique in that as a recessed canopy it can be installed in both 12" and 16" canopy decks. Features such as Adjustable Lumen Output Dial with 8 Settings and forward throw distribution (ASYC) option make the RCNY one of the most versatile gas station canopies in the market. Lumen dial and driver are both accessible from below. With up to 17V LPW the RCNY LED delivers up to 90% in energy savings when replacing metal halide luminaires. With over 100,000 hour life expectancy (12+ years of 24/7 continuous operation), the RCNY LED luminaire provides significant maintenance savings over traditional luminaires. Class 1 Division 2 also available. Wet location junction box available upon request.

Ordering Information

Series	Package	Color Temperature	CR	Lens
RCNY LED	AL02	30K/300K	300K	DBXDK
	AL02	40K/400K	400K	DBXDK
	AL02	50K/500K	500K	DBXDK

Shipped installed: DBXDK: Dark Bronze, DBXDK: Black, DBXDK: Natural Aluminum, DBXDK: White

Shipped separately: DBXDK: Dark Bronze, DBXDK: Black, DBXDK: Natural Aluminum, DBXDK: White

Accessories:

- DBXDK: Dark Bronze
- DBXDK: Black
- DBXDK: Natural Aluminum
- DBXDK: White

Streetstone Design Studio
LIGHTING DESIGN

SHEETZ 2.0

STREET WRAP™ FLEX BACK-BEND

Installation Accessories

1. Transparent PC Caps

2. 3/16" x 1/8" x 1/2" Aluminum Mill Finish Extrusions for Outdoor Application

3. Aluminum Mill Finish Spacers for Outdoor Application - 1.6667" (42.17mm)

Drilling

HANDHOLE ORIENTATION

DSXO Area Luminaire - EPA

Mounting, Options and Accessories

Mountings:

- IF-Adjustable Hanger (fits 2.3" x 0.5" lens)
- TKC2-Tab with 1/2" x 3/8" hole
- TKC-Threaded Bracket with 1/2" NPT threaded pipe
- WFF-Flange, Top Mounted
- WFF-Flange, Bottom Mounted
- FF-Full Flange

External Shields:

- WFF-Flange, Top Mounted
- WFF-Flange, Bottom Mounted
- FF-Full Flange

Accessories:

- VC-Tandem Guard
- DSXF2-TIK-Splitter Accessory

Streetstone Design Studio
LIGHTING DESIGN

SHEETZ 2.0

D-Series Size 0 LED Area Luminaire

Specifications

EPA: 0.44 ft² (0.04 m²)

Length: 26.18" (0.67 m)

Width: 14.06" (0.36 m)

Height H1: 2.26" (0.06 m)

Height H2: 7.46" (0.19 m)

Weight: 23 lbs

Introduction

The modern styling of the D-Series features a highly refined aesthetic that blends seamlessly with its environment. The D-Series offers the benefits of the latest in LED technology into a high performance, high efficiency, long life luminaire.

The photometric performance results in sites with excellent uniformity, greater pole spacing and lower power density. D-Series outstanding photometry aids in reducing the number of poles required in area lighting applications, with typical energy savings of 70% and expected service life of over 100,000 hours.

Ordering Information

Series	Color Temperature	CR	Beamwidth	Voltage	Mounting
DSX0 LED	30K	300K	70X3	120V	DBXDK
	40K	400K	40X3	120V	DBXDK
	50K	500K	30X3	120V	DBXDK

Shipped installed: DBXDK: Dark Bronze, DBXDK: Black, DBXDK: Natural Aluminum, DBXDK: White

Shipped separately: DBXDK: Dark Bronze, DBXDK: Black, DBXDK: Natural Aluminum, DBXDK: White

Accessories:

- DBXDK: Dark Bronze
- DBXDK: Black
- DBXDK: Natural Aluminum
- DBXDK: White

Streetstone Design Studio
LIGHTING DESIGN

SHEETZ 2.0

D-Series DSXF2 LED Floodlight

Specifications

EPA: 0.8 ft² (0.07 m²)

Depth: 4.32" (0.11 m)

Width: 12.87" (0.33 m)

Height: 7.81" (0.20 m)

Overall Height: 15.33" (0.39 m)

Weight: 12.0 lbs

Introduction

The D-Series floodlights feature a sleek wide offering to meet specifier's every lighting need in application. The D-Series flood offers three sizes delivering 3,000 to 27,000 lumens. Available with seven precision optics, three mountings and three color temperatures, D-Series floodlights offer vast design capabilities while delivering significant energy savings and long life.

The DSXF2 delivers 7,000 to 17,000 lumens, meeting a large breadth of illumination requirements for design and renovation when replacing 175W, 250W and 400W HID floodlights. All configurations are assembled in the USA allowing for quick delivery.

Ordering Information

Series	Performance Package	Color Temperature	CR	Beamwidth	Voltage	Mounting
DSXF2 LED	P1	30K	300K	70X3	120V	DBXDK
	P2	40K	400K	40X3	120V	DBXDK
	P3	50K	500K	30X3	120V	DBXDK

Shipped installed: DBXDK: Dark Bronze, DBXDK: Black, DBXDK: Natural Aluminum, DBXDK: White

Shipped separately: DBXDK: Dark Bronze, DBXDK: Black, DBXDK: Natural Aluminum, DBXDK: White

Accessories:

- DBXDK: Dark Bronze
- DBXDK: Black
- DBXDK: Natural Aluminum
- DBXDK: White

Streetstone Design Studio
LIGHTING DESIGN

SHEETZ 2.0

RCNY LED Recessed LED Canopy/Ceiling Luminaire

Specifications

RCNY LED

Width: 11.5"

Height: 5.9"

Length: 11.5"

Weight: 14lbs

Introduction

The RCNY LED recessed luminaire is designed to provide exceptional energy savings and versatility in one fixture. With a compact design the RCNY provides superior illumination while blending into the canopy space seamlessly. The RCNY is unique in that as a recessed canopy it can be installed in both 12" and 16" canopy decks. Features such as Adjustable Lumen Output Dial with 8 Settings and forward throw distribution (ASYC) option make the RCNY one of the most versatile gas station canopies in the market. Lumen dial and driver are both accessible from below. With up to 17V LPW the RCNY LED delivers up to 90% in energy savings when replacing metal halide luminaires. With over 100,000 hour life expectancy (12+ years of 24/7 continuous operation), the RCNY LED luminaire provides significant maintenance savings over traditional luminaires. Class 1 Division 2 also available. Wet location junction box available upon request.

Ordering Information

Series	Package	Color Temperature	CR	Lens
RCNY LED	AL02	30K/300K	300K	DBXDK
	AL02	40K/400K	400K	DBXDK
	AL02	50K/500K	500K	DBXDK

Shipped installed: DBXDK: Dark Bronze, DBXDK: Black, DBXDK: Natural Aluminum, DBXDK: White

Shipped separately: DBXDK: Dark Bronze, DBXDK: Black, DBXDK: Natural Aluminum, DBXDK: White

Accessories:

- DBXDK: Dark Bronze
- DBXDK: Black
- DBXDK: Natural Aluminum
- DBXDK: White

QTY	LABEL	DESCRIPTION
7	XT	SEW12146 5FT L20W AN08 120-277V 4000K PSE SMA OAP (6.00 IN.) FIXT. HEAD FACTORY SET TO 50 DEGREE TILT AWAY FROM BLDG.



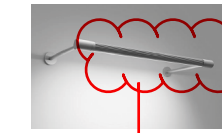
Should be tilted down so as to create 0 Uplight

QTY	LABEL	DESCRIPTION
3	XT1	SEW12146 4FT L16W AN08 120-277V 4000K PSE SMA OAP (6.00 IN.) FIXT. HEAD FACTORY SET TO 50 DEGREE TILT AWAY FROM BLDG.



Should be tilted down to create 0 Uplight

QTY	LABEL	DESCRIPTION
1	XT2	SEW12146 6FT L24W AN08 120-277V 4000K SMA OAP12



Should be tilted down to create 0 Uplight

QTY	LABEL	DESCRIPTION
7	XX	VWM-H-L17/840-TL-DBZ-CGL-DIM-UNV (BRONZE COLOR BR01)



Should be tilted down to create 0 Uplight

Styk Exterior Wall - Stem

SEW12146 2.20 in

JOB NAME: SHEETZ
TYPE: 5' Aiming Light
NOTES: Fixture head to be fixed at a 50 degree angle down away from the building.



ORDERING CODE: SEW12146 | 5FT-L20W(AN08) | 120-277V | 4000K | PSE | SMA | (DAP)6.00m |
Please Note: Default specifications *DF_XXX* will not appear in the ordering code.

DESCRIPTION

Styk was built to provide the most lumens with the smallest luminaire package, while optimizing life. Not limited to decorative lighting, effective functional lighting is provided through the optimal light control in this minimal package. It's three optical offerings enable tremendous flexibility including wall washing, wall grazing, and asymmetric lighting solutions. It is excellent at lighting signs and facades. The family features wall, ceiling, and pendant models in a variety of stylish, clean mounting options. Available in 1' to 8' lengths, Styk can be mounted individually or configured in runs.



FEATURES & BENEFITS

- Sleek 1.5" diameter housing
- A forward throw optic, for even wall wash illumination, is standard
- Symmetrical optic options are available for more volumetric illumination
- Lamp body can be rotated up to 330° and locks into position for precise fixture alignment
- Up to 1,450 lumens per foot delivered (with Forward Throw distribution)
- Up to 18" projection with no vertical support needed
- Anodized finish provides durable corrosion protection
- All visible fasteners are flush mounted, providing a clean design
- Handcrafted in USA, BABA Compliant

SPECIFICATIONS

- LIGHT SOURCE: IP66 white LED light engine
- CRF: 80+ or 90+
- LUMEN MAINTENANCE: L70 >= 50,000 Hrs.
- EFFICACY: 102 lm/W delivered (with Forward Throw distribution)
- CCT: 2700K, 3000K, 3500K, 4000K, 5000K
- VOLTAGE: 120-277V standard
- DRIVER: Includes one remote damp listed Class 2 power supply and either a damp location or wet location enclosure, except

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- 8FT-L113W which has two. Black power cord standard unless otherwise specified. Max distance to the driver (including DAP) is: #18 AWG = 50', #16 AWG = 75', #14 AWG = 100'. For extended distances, contact factory.
- DIMMING: 0-10V controls standard to 1%
- CONTROLS: Contact factory for control options (e.g., Lutron Athena, Current NX, nLight, Enlighted, Enceium, Wattstopper, WaveLinx, or Casambi).
- EMERGENCY: Remote battery option is provided with 10W Constant Power Battery back-up, providing 90 Minutes of Emergency operation. Contact factory for other Emergency options.
- INTEGRATED SURGE PROTECTION: LED components are protected against minor surge events
- CONSTRUCTION: Extruded aluminum construction provides durable protection for internal components and is recyclable
- FINISH: Housing available in anodized finishes only. End caps and mounting components painted to match.
- MODIFICATIONS: Consult factory for all modification requests, including RGB and static LED colors
- APPROVALS: ETL listed to UL standards (US and Canada) for use in wet locations

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Styk Exterior Wall - Stem

SEW12146 2.20 in

JOB NAME: SHEETZ
TYPE: 4' Aiming Light
NOTES: Fixture head to be fixed at a 50 degree angle down away from the building.



ORDERING CODE: SEW12146 | 4FT-L16W(AN08) | 120-277V | 4000K | PSE | SMA | (DAP)6.00m |
Please Note: Default specifications *DF_XXX* will not appear in the ordering code.

DESCRIPTION

Styk was built to provide the most lumens with the smallest luminaire package, while optimizing life. Not limited to decorative lighting, effective functional lighting is provided through the optimal light control in this minimal package. It's three optical offerings enable tremendous flexibility including wall washing, wall grazing, and asymmetric lighting solutions. It is excellent at lighting signs and facades. The family features wall, ceiling, and pendant models in a variety of stylish, clean mounting options. Available in 1' to 8' lengths, Styk can be mounted individually or configured in runs.



FEATURES & BENEFITS

- Sleek 1.5" diameter housing
- A forward throw optic, for even wall wash illumination, is standard
- Symmetrical optic options are available for more volumetric illumination
- Lamp body can be rotated up to 330° and locks into position for precise fixture alignment
- Up to 1,450 lumens per foot delivered (with Forward Throw distribution)
- Up to 18" projection with no vertical support needed
- Anodized finish provides durable corrosion protection
- All visible fasteners are flush mounted, providing a clean design
- Handcrafted in USA, BABA Compliant

SPECIFICATIONS

- LIGHT SOURCE: IP66 white LED light engine
- CRF: 80+ or 90+
- LUMEN MAINTENANCE: L70 >= 50,000 Hrs.
- EFFICACY: 102 lm/W delivered (with Forward Throw distribution)
- CCT: 2700K, 3000K, 3500K, 4000K, 5000K
- VOLTAGE: 120-277V standard
- DRIVER: Includes one remote damp listed Class 2 power supply and either a damp location or wet location enclosure, except

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- 8FT-L113W which has two. Black power cord standard unless otherwise specified. Max distance to the driver (including DAP) is: #18 AWG = 50', #16 AWG = 75', #14 AWG = 100'. For extended distances, contact factory.
- DIMMING: 0-10V controls standard to 1%
- CONTROLS: Contact factory for control options (e.g., Lutron Athena, Current NX, nLight, Enlighted, Enceium, Wattstopper, WaveLinx, or Casambi).
- EMERGENCY: Remote battery option is provided with 10W Constant Power Battery back-up, providing 90 Minutes of Emergency operation. Contact factory for other Emergency options.
- INTEGRATED SURGE PROTECTION: LED components are protected against minor surge events
- CONSTRUCTION: Extruded aluminum construction provides durable protection for internal components and is recyclable
- FINISH: Housing available in anodized finishes only. End caps and mounting components painted to match.
- MODIFICATIONS: Consult factory for all modification requests, including RGB and static LED colors
- APPROVALS: ETL listed to UL standards (US and Canada) for use in wet locations

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Styk Exterior Wall - Stem

SEW12146 2 in

JOB NAME: SHEETZ
TYPE:
NOTES:



ORDERING CODE: SEW12146 | 5FT-L20W(AN08) | 120-277V | 4000K | SMA |
Please Note: Default specifications *DF_XXX* will not appear in the ordering code.

DESCRIPTION

Styk was built to provide the most lumens with the smallest luminaire package, while optimizing life. Not limited to decorative lighting, effective functional lighting is provided through the optimal light control in this minimal package. It's three optical offerings enable tremendous flexibility including wall washing, wall grazing, and asymmetric lighting solutions. It is excellent at lighting signs and facades. The family features wall, ceiling, and pendant models in a variety of stylish, clean mounting options. Available in 1' to 8' lengths, Styk can be mounted individually or configured in runs.



FEATURES & BENEFITS

- Sleek 1.5" diameter housing
- A forward throw optic, for even wall wash illumination, is standard
- Symmetrical optic options are available for more volumetric illumination
- Lamp body can be rotated up to 330° and locks into position for precise fixture alignment
- Up to 1,450 lumens per foot delivered (with Forward Throw distribution)
- Up to 18" projection with no vertical support needed
- Anodized finish provides durable corrosion protection
- All visible fasteners are flush mounted, providing a clean design
- Handcrafted in USA

SPECIFICATIONS

- LIGHT SOURCE: IP66 white LED light engine
- CRF: 80+ (contact factory for 90+)
- LUMEN MAINTENANCE: L70 >= 50,000 Hrs.
- EFFICACY: 102 lm/W delivered (with Forward Throw distribution)
- CCT: 3000K, 3500K, or 4000K
- VOLTAGE: 120-277V standard
- DRIVER: Includes one remote damp listed Class 2 power supply and wet location enclosure, except 8FT-L113W which has two.

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- Black power cord standard unless otherwise specified. Max distance to the driver (including DAP) is: #18 AWG = 50', #16 AWG = 75', #14 AWG = 100'. For extended distances, contact factory.
- DIMMING: 0-10V controls standard to 1%
- EMERGENCY: Emergency battery remote optional
- INTEGRATED SURGE PROTECTION: LED components are protected against minor surge events
- CONSTRUCTION: Extruded aluminum construction provides durable protection for internal components and is recyclable
- FINISH: Housing available in anodized finishes only. End caps and mounting components painted to match.
- MODIFICATIONS: Consult factory for all modification requests, including RGB and static LED colors
- APPROVALS: ETL listed to UL standards (US and Canada) for use in wet locations

DIMENSIONS

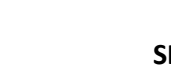
UNIT	L	H	W	OAP	MC
FT	17.5 in	2.2 in	12 in	16.2 in	
MM	444.0 mm	5.6 mm	305.0 mm	411.1 mm	
Mounting Weight: Approximately 5.9 lb (3 kg)					
FT	7.9 in	2.2 in	12 in	17.1 in	
MM	200.0 mm	5.6 mm	305.0 mm	433.0 mm	
Mounting Weight: Approximately 5.9 lb (3 kg)					
FT	11.8 in	2.2 in	12 in	17.9 in	
MM	300.0 mm	5.6 mm	305.0 mm	454.0 mm	
Mounting Weight: Approximately 5.9 lb (3 kg)					
FT	15.9 in	2.2 in	12 in	18.2 in	
MM	403.0 mm	5.6 mm	305.0 mm	463.0 mm	
Mounting Weight: Approximately 5.9 lb (3 kg)					
FT	19.9 in	2.2 in	12 in	18.5 in	
MM	506.0 mm	5.6 mm	305.0 mm	470.0 mm	
Mounting Weight: Approximately 10.9 lb (5 kg)					
FT	23.9 in	2.2 in	12 in	18.8 in	
MM	607.0 mm	5.6 mm	305.0 mm	478.0 mm	
Mounting Weight: Approximately 10.9 lb (5 kg)					
FT	27.9 in	2.2 in	12 in	19.1 in	
MM	713.0 mm	5.6 mm	305.0 mm	486.0 mm	
Mounting Weight: Approximately 10.9 lb (5 kg)					

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Shimstone Design Studio

SEW12146 2.20 in

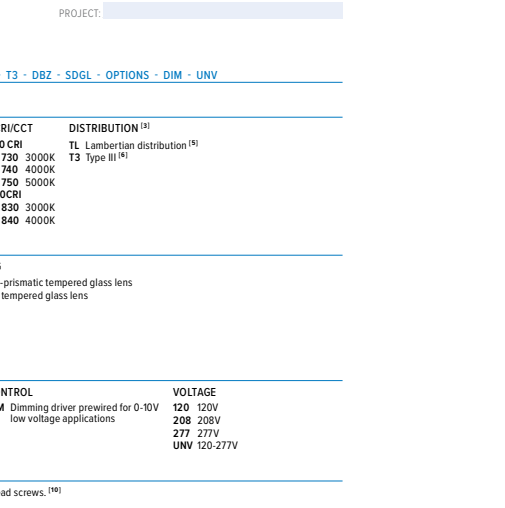
JOB NAME: SHEETZ
TYPE:
NOTES:



ORDERING CODE: SEW12146 | 5FT-L20W(AN08) | 120-277V | 4000K | SMA |
Please Note: Default specifications *DF_XXX* will not appear in the ordering code.

DESCRIPTION

Styk was built to provide the most lumens with the smallest luminaire package, while optimizing life. Not limited to decorative lighting, effective functional lighting is provided through the optimal light control in this minimal package. It's three optical offerings enable tremendous flexibility including wall washing, wall grazing, and asymmetric lighting solutions. It is excellent at lighting signs and facades. The family features wall, ceiling, and pendant models in a variety of stylish, clean mounting options. Available in 1' to 8' lengths, Styk can be mounted individually or configured in runs.



FEATURES & BENEFITS

- Sleek 1.5" diameter housing
- A forward throw optic, for even wall wash illumination, is standard
- Symmetrical optic options are available for more volumetric illumination
- Lamp body can be rotated up to 330° and locks into position for precise fixture alignment
- Up to 1,450 lumens per foot delivered (with Forward Throw distribution)
- Up to 18" projection with no vertical support needed
- Anodized finish provides durable corrosion protection
- All visible fasteners are flush mounted, providing a clean design
- Handcrafted in USA, BABA Compliant

SPECIFICATIONS

- LIGHT SOURCE: IP66 white LED light engine
- CRF: 80+ or 90+
- LUMEN MAINTENANCE: L70 >= 50,000 Hrs.
- EFFICACY: 102 lm/W delivered (with Forward Throw distribution)
- CCT: 2700K, 3000K, 3500K, 4000K, 5000K
- VOLTAGE: 120-277V standard
- DRIVER: Includes one remote damp listed Class 2 power supply and either a damp location or wet location enclosure, except

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- No uplight permitted

UNIT	L	H	W	OAP	MC
FT	17.5 in	2.2 in	12 in	16.2 in	
MM	444.0 mm	5.6 mm	305.0 mm	411.1 mm	
Mounting Weight: Approximately 5.9 lb (3 kg)					
FT	7.9 in	2.2 in	12 in	17.1 in	
MM	200.0 mm	5.6 mm	305.0 mm	433.0 mm	
Mounting Weight: Approximately 5.9 lb (3 kg)					
FT	11.8 in	2.2 in	12 in	17.9 in	
MM	300.0 mm	5.6 mm	305.0 mm	454.0 mm	
Mounting Weight: Approximately 5.9 lb (3 kg)					
FT	15.9 in	2.2 in	12 in	18.2 in	
MM	403.0 mm	5.6 mm	305.0 mm	463.0 mm	
Mounting Weight: Approximately 5.9 lb (3 kg)					
FT	19.9 in	2.2 in	12 in	18.5 in	
MM	506.0 mm	5.6 mm	305.0 mm	470.0 mm	
Mounting Weight: Approximately 10.9 lb (5 kg)					
FT	23.9 in	2.2 in	12 in	18.8 in	
MM	607.0 mm	5.6 mm	305.0 mm	478.0 mm	
Mounting Weight: Approximately 10.9 lb (5 kg)					
FT	27.9 in	2.2 in	12 in	19.1 in	
MM	713.0 mm	5.6 mm	305.0 mm	486.0 mm	
Mounting Weight: Approximately 10.9 lb (5 kg)					

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DOWNLIGHT




STREET WRAP



AREA



FLOOD



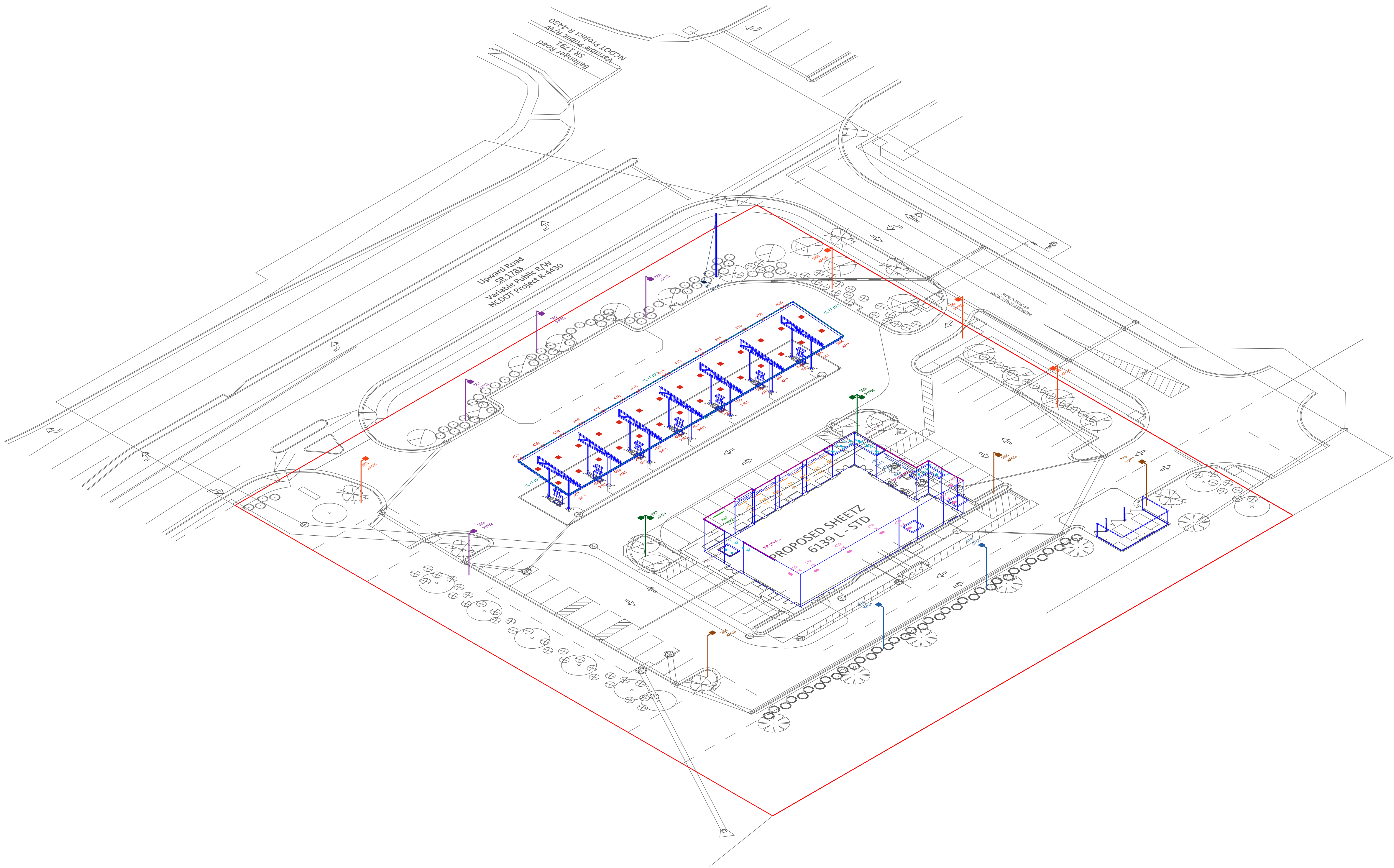
CANOPY

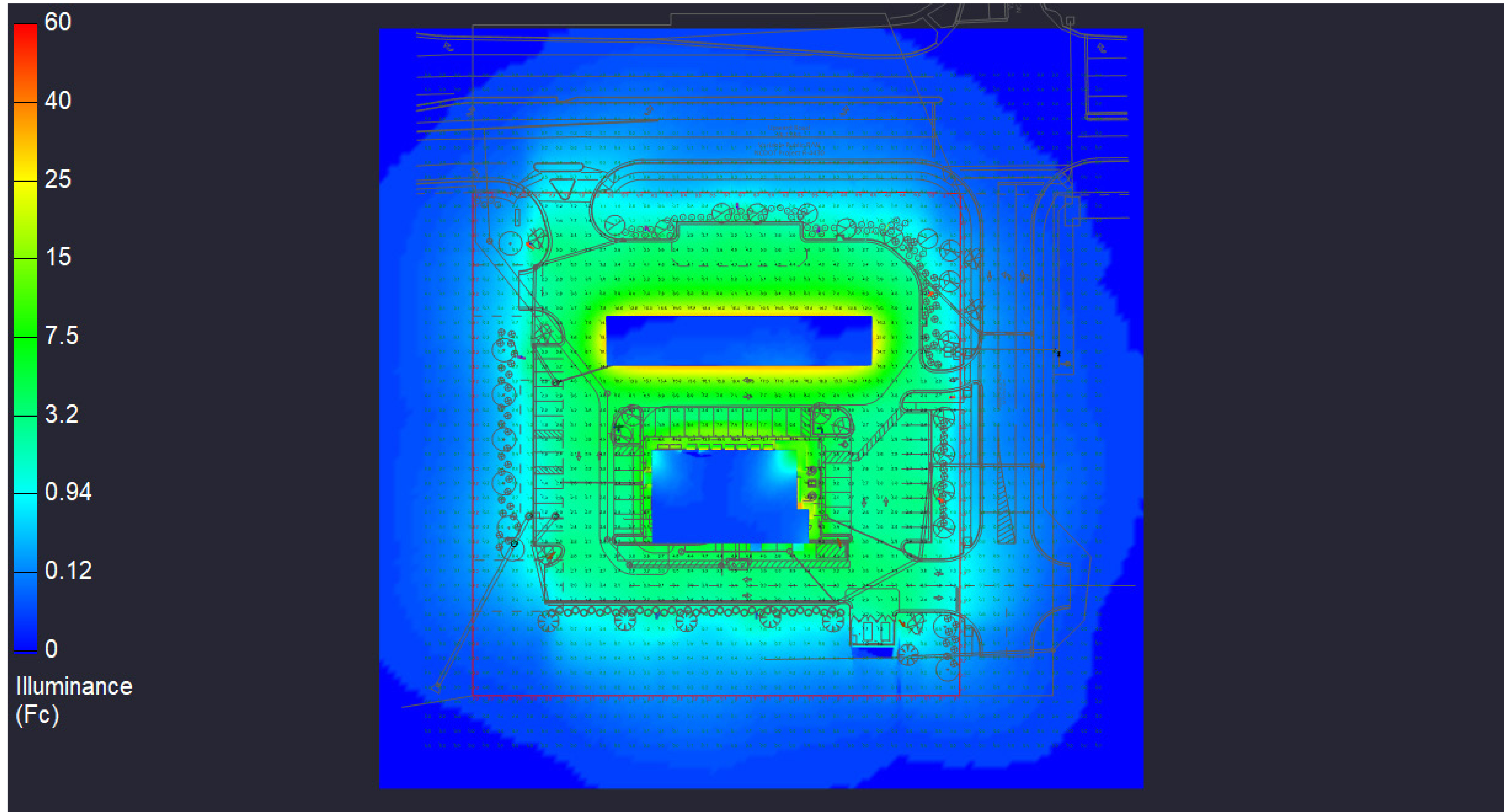


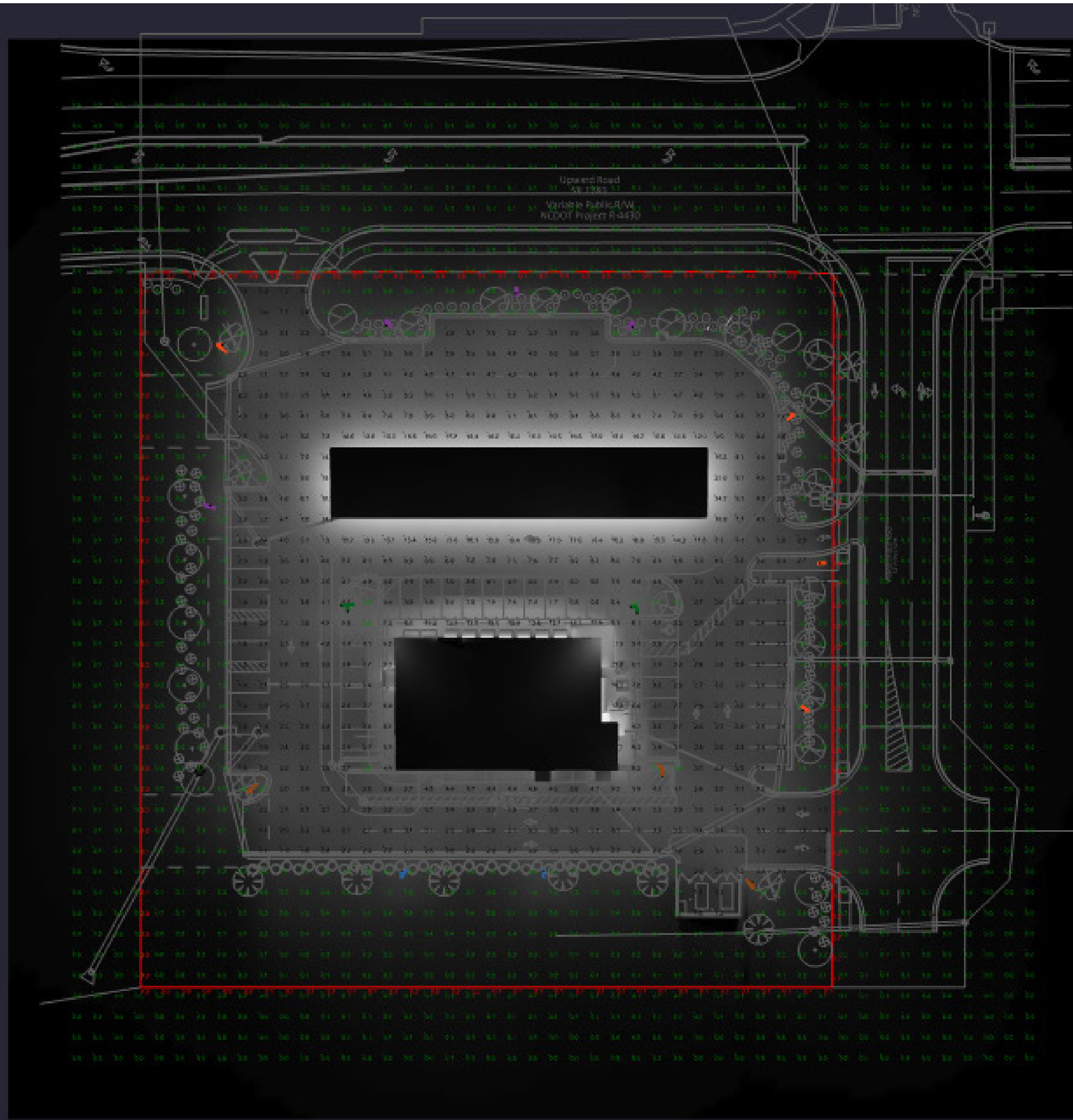
WALL MOUNTED



WALL MOUNTED







STATE OF NORTH CAROLINA
DEPARTMENT OF TRANSPORTATION

JOSH STEIN
GOVERNOR

J.R. "JOEY" HOPKINS
SECRETARY

DATE: July 1, 2025
TO: Andrew Aiezza, P.E.
Transportation Design Project Manager
DRMP, Inc.
FROM: R.H. Darnell
Assistant District Engineer
SUBJECT: Upward Road Sheetz – Traffic Impact Analysis Addendum

The Upward Road Sheetz – Traffic Impact Analysis Addendum, dated May 19, 2025, has been reviewed and discussed with NCDOT’s Congestion Management Section as well as the Blue Ridge Mountains Regional Traffic Office of the NCDOT Mobility and Safety Field Operations Section.

The NCDOT Policy On Street And Driveway Access to North Carolina Highways states: “All analyses shall include level of service determination for the entire network and **individual intersections** and roadway segments, as appropriate. Intersection analyses shall **include level of service determinations for all approaches and movements**. Intersection analyses shall include queue analysis.” (emphasis added)

An evaluation of the data provided in the Addendum, specifically Page 2, Table 2, shows that signalizing the intersection provides an Overall Level of Service (LOS) B for AM Peak Hour with a 19-second delay. This places the AM Peak Hour LOS just one second of delay out of the LOS C range. The PM Peak Hour is shown as an Overall LOS B with a 16-second delay, which is mid-range LOS B.

In the signalized scenario, looking at the individual approaches, three of the four operate at a LOS C or better in both the AM and PM Peak Hour. The Northbound (NB) approach operates at a solid mid-range LOS D in both Peak Hours, with 45 and 41 seconds of delay respectively. It should be noted that the NB approach is the approach to be utilized by customers visiting the proposed business on opening day.

The overall intersection LOS improves to LOS A with the roundabout scenario in both Peak Hours. It also includes a decrease in delay of more than 50% in both Peak Hours.

Table 2 shows all approaches operate at a LOS A in both Peak Hours except for the Southbound (SB) approach in the AM Peak Hour, which is shown as a LOS B with a 10-second delay. It is

Mailing Address:
NC DEPARTMENT OF TRANSPORTATION
DIVISION 14 / DISTRICT I FIELD OFFICE
4142 HAYWOOD ROAD
MILLS RIVER, NC 28759

Telephone: (828) 891-7911
Fax: (828) 891-5026
Customer Service: 1-877-368-4968

Location:
4142 HAYWOOD ROAD
MILLS RIVER, NC 28759

Website: ncdot.gov

Andrew Aiezza, P.E.
 Upward Road Sheetz – Traffic Impact Analysis Addendum
 July 1, 2025
 Page 2

noted that Addendum Table 1 on page 2 indicates that, in the Roundabout Scenario, a 10-second delay can be shown as either a low LOS A or a high LOS B. Table 2 also shows the Westbound approach at a LOS A with a 10-second delay.

Henderson County SR 1783, Upward Road, was constructed in its current configuration by STIP Project R-4430 and LET to contract in March 2010. That project was a widening project and provided access management along the corridor. With knowledge of the operations of NCDOT in the time that R-4430 was being developed and constructed, it is the opinion of this office that there was not enough traffic volume on SR 1791, Ballenger Rd., to justify spending the funds for improvements on that intersection at that time. All other intersections along that corridor were modified with the intent of providing access management for a safer and more efficient corridor. There is an existing bulb-out available approximately 750' east of this site.

This office must evaluate each proposed access for safety of all roadway users as well as the Governor's Executive Order for Carbon Reduction and the NC Vision Zero initiative.

As a result of NC Executive Orders 80, 246, and 271, NCDOT developed the NCDOT Carbon Reduction Strategy. One aspect of this strategy is to reduce carbon emissions by improving infrastructure to limit the idle time (delay) of motorists. This must be taken into consideration for any proposed development desiring access to the NCDOT state-maintained system.

Consideration must also be given for the NC Vision Zero initiative while evaluating any proposed development desiring access the NCDOT state-maintained system. The NC Vision Zero initiative is a traffic safety approach focused on eliminating traffic deaths and serious injuries, while improving access to safe, sustainable, and equitable mobility for everyone.

Published data shows the safety improvements achieved after roundabouts replaced traditional intersections across North Carolina:

- 46% reduction in Total crashes
- 76% reduction in Fatal and Injury crashes
- 75% reduction in Frontal Impact crashes

Additionally, a driver generally enters the roundabout more quickly than if waiting at a traffic signal. This movement improves travel time and decreases delay for less carbon impacts. The modern roundabout is much smaller than older traffic circles and requires vehicles to travel at lower speeds (15–20mph), making them safer than older traffic circles with a smaller footprint. These lower speeds generate less impactful crashes with greatly decreased fatal or serious injuries.

The Addendum makes reference to the full-access drive QT has existing on SR 1791, closest to the intersection with SR 1783. That drive will be converted to right-in/right-out, as it has become problematic to the intersection and the left-turn lane on SR 1791. QT was notified during the permit process it would have to be adjusted if it became a problem.

This office opposes the installation of the proposed signal at the intersection of Henderson County SR 1783, Upward Rd., and SR 1791, Ballenger Rd., as part of the Sheetz project. A new signalized intersection does not comply with the intent of R-4430, nor does it conform to the current climate toward safety and carbon reduction.

Andrew Aiezza, P.E.
Upward Road Sheetz – Traffic Impact Analysis Addendum
July 1, 2025
Page 3

A 2x1 Roundabout is the preferred method for modification of the intersection. It will ultimately provide full access to all users of the corridor in a safer and more efficient manner. A roundabout does stay with the intent of R-4430.

If the developer chooses not to pursue the preferred roundabout, NCDOT is willing to consider other types of Reduced Conflict Intersections (RCIs) for potential use at this location.

We look forward to continuing this conversation to find an equitable solution that works.

September 18, 2025

**Troy Wilson
NCDOT Division 14, District 1
4142 Haywood Road
Mills River, NC 28759**

Reference: Upward Road Sheetz – Hendersonville, North Carolina
Subject: Traffic Impact Analysis Supplement

Dear Mr. Wilson,

This letter serves as a supplement to the Traffic Impact Analysis (TIA) and its addendum for the proposed Sheetz development on Upward Road, located opposite Ballenger Road in Hendersonville, North Carolina. Prepared in response to follow-up discussions with NCDOT and Town of Hendersonville staff, this supplement provides additional clarity on the recommended intersection configurations and control strategies, both in the near term and long term, at the Upward Road and Ballenger Road intersection. In addition to the previously presented capacity analysis results, this letter also evaluates the anticipated impacts on safety and carbon emissions under the various intersection layout alternatives.

Background Information

After further review of the TIA, NCDOT requested additional analysis to ensure acceptable operations along the southern leg of the intersection of Upward Road and Ballenger Road as well as to make sure the improvements associated with the proposed development adhere to NC Executive Orders 80, 246, and 271 and the NC Vision Zero initiative that focuses on eliminating traffic related death and injuries.

Improvement considerations have been made to ensure that with construction of our development, capacity impacts are mitigated while protecting the safety and the future plan for development of this corridor. Capacity at the intersection is largely considered in the immediate future as well as the distant future with potential development generating significant traffic.

Traffic Analysis Comparison

Traffic capacity analysis for the intersection of Upward Road and Ballenger was performed using Synchro 11.1 and SIDRA, which are comprehensive software packages that allow the user to model signalized intersections, unsignalized intersections, and roundabouts to determine levels-of-service based on the thresholds specified in the Highway Capacity Manual (HCM).

Refer to the table below for a summary of the capacity analysis of the subject intersection during the analysis scenarios under both signalized and roundabout control.

ANALYSIS SCENARIO	APPROACH	LANE CONFIGURATIONS	WEEKDAY AM PEAK HOUR LEVEL OF SERVICE		WEEKDAY PM PEAK HOUR LEVEL OF SERVICE	
			Approach	Overall (seconds)	Approach	Overall (seconds)
2024 Existing	EB WB SB	1 LT, 2 TH 2 TH, 1 RT 1 LT, 1 RT	B (14) ¹ -- C (23) ²	N/A	B (10) ¹ -- C (17) ²	N/A
2026 No-Build	EB WB SB	1 LT, 2 TH 2 TH, 1 RT 1 LT, 1 RT	C (16) ¹ -- D (29) ²	N/A	B (11) ¹ -- C (19) ²	N/A
2026 Build - Signal	EB WB NB SB	1 LT, 1 TH, 1 TH-RT 1 LT, 2 TH, 1 RT 1 LT, 1 TH-RT 1 LT, 1 TH-RT	A (10) B (17) D (45) C (35)	B (19)	A (8) B (16) D (41) C (34)	B (16)
2026 Build - Signal (RTOR)	EB WB NB SB	1 LT, 1 TH, 1 TH-RT 1 LT, 2 TH, 1 RT 1 LT, 1 TH-RT 1 LT, 1 TH-RT	A (10) B (16) D (40) B (12)	B (16)	A (8) B (15) C (34) B (16)	B (14)
2026 Build - Roundabout	EB WB NB SB	1 LT-TH, 1 TH-RT 1 LT-TH, 1 TH-RT 1 LT-TH-RT 1 LT-TH-RT	A (6) A (10) A (9) B (10)	A (8)	A (5) A (7) A (8) A (7)	A (6)

Analysis of the intersection constructed as a roundabout was requested in addition to analysis of the intersection under full-movement signalized control. As shown in table, the intersection is expected to operate at overall LOS B or better during the AM and PM peak hour when operating as a signalized intersection. Additionally, under field conditions, with right-turn-on-red (RTOR) allowed, even better operations are expected along the northbound leg of the intersection. It should be noted that higher delays along the minor-street are not uncommon as the majority of green time is given to the major-street. With the amount of capacity at the intersection, more green time could be given to the side

streets if desired to improve operations. Additionally, the intersection under roundabout configuration is expected to operate at overall LOS A during the AM and PM peak hours. The proposed improvements would allow for safe vehicular movement until a roundabout becomes more appropriate with future development that will trigger a need to upgrade the intersection. Based on our analysis, no significant queuing is expected with either option.

Intersection Configuration Safety Comparison

A signalized intersection at Upward Road and Ballenger Road represents a meaningful step forward from a safety perspective when compared to the unsignalized condition that exists today. Signalization improves predictability, reduces conflict points for side-street traffic, and safer crossings for pedestrians aligning with the NC Vision Zero initiative. While a roundabout remains the long-term preferred solution, the signalized intersections provides a safer alternative versus the existing condition.

In accordance with the NC Vision Zero initiative, the intersection configurations were evaluated from a high-level safety standpoint. According to published research information from the Federal Highway Administration (FHWA) and National Cooperative Highway Research Program (NCHRP), roundabouts are generally safer than signalized intersections because they reduce both the frequency and severity of crashes. Research shows that converting a signalized intersection to a roundabout can lower overall crashes by about 35–40% with injury crashes dropping by 70–80%.

This crash reduction is largely due to the elimination of high-speed right-angle and head-on collisions, since vehicles in roundabouts travel at lower speeds and in the same direction. Most crashes at roundabouts are minor sideswipes or entry collisions, whereas signals tend to experience more severe angle and rear-end crashes, oftentimes linked to red-light running. While roundabouts often provide substantial safety advantages, particularly in reducing severe crashes, signals are still beneficial in high-volume or pedestrian-heavy contexts.

Intersection Configuration Carbon Emissions Comparison

Vehicles are a major contributor to poor air quality because they emit a variety of pollutants into the atmosphere. Traffic congestion contributes to poor air quality in populated areas through increased emissions of harmful pollutants known as criteria pollutants, including Carbon Monoxide (CO), Nitrogen Oxides (NO_x), Particulate Matter (PM), and Ozone (O₃). Criteria pollutants are federally regulated due to their adverse impacts to human health and the environment. Certain transportation improvements,

such as the use of roundabouts in place of traditional intersections, reduce criteria pollutant emissions from motor vehicles and improve air quality, even along high-volume arterials.

It is noted that the increase in emissions from the existing unsignalized to signalized condition would be minor in nature given the lengthy amount of time that vehicles on the side-street would idle while waiting for gaps in traffic to exit.

Proposed Intersection Configuration Phasing

The intersection of Upward Road and Ballenger Road is proposed to be signalized by the Sheetz development. Signalization of the intersection is expected to greatly improve operations at the intersection compared to its current condition providing safe protected left-turn movements. In order to adhere to North Carolina initiatives and executive orders, the intersection in the long term is planned to be converted to a 2-circulating lane roundabout when traffic conditions trigger the need with future development. In the meantime, the proposed signalization of the intersection ensures that an acceptable level of service can be achieved at the intersection. The Sheetz development proposes to dedicate the appropriate right-of-way that the roundabout intersection improvement can be made in the future.

Sincerely,

A handwritten signature in black ink, appearing to read "C. Seger", is displayed within a white rectangular box.

Cameron Seger, EI
Traffic Analysis Project Engineer

DRMP, Inc.

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DRAWN BY: MDS
PROJECT #: 24039
ORIGINAL ISSUE DATE: 09/12/2025

PRELIMINARY
NOT FOR CONSTRUCTION

Blue Ridge Engineering PLLC
924 Main Street - Suite 200
North Wilkesboro, NC 28659
336.838.2500
www.ridge.blue
Firm #P-0205



Proposed Roundabout Exhibit
Sheetz
Upward Road
Hendersonville, NC

SHEET
Exhibit
ver. 1.2

PRELIMINARY - NOT FOR CONSTRUCTION

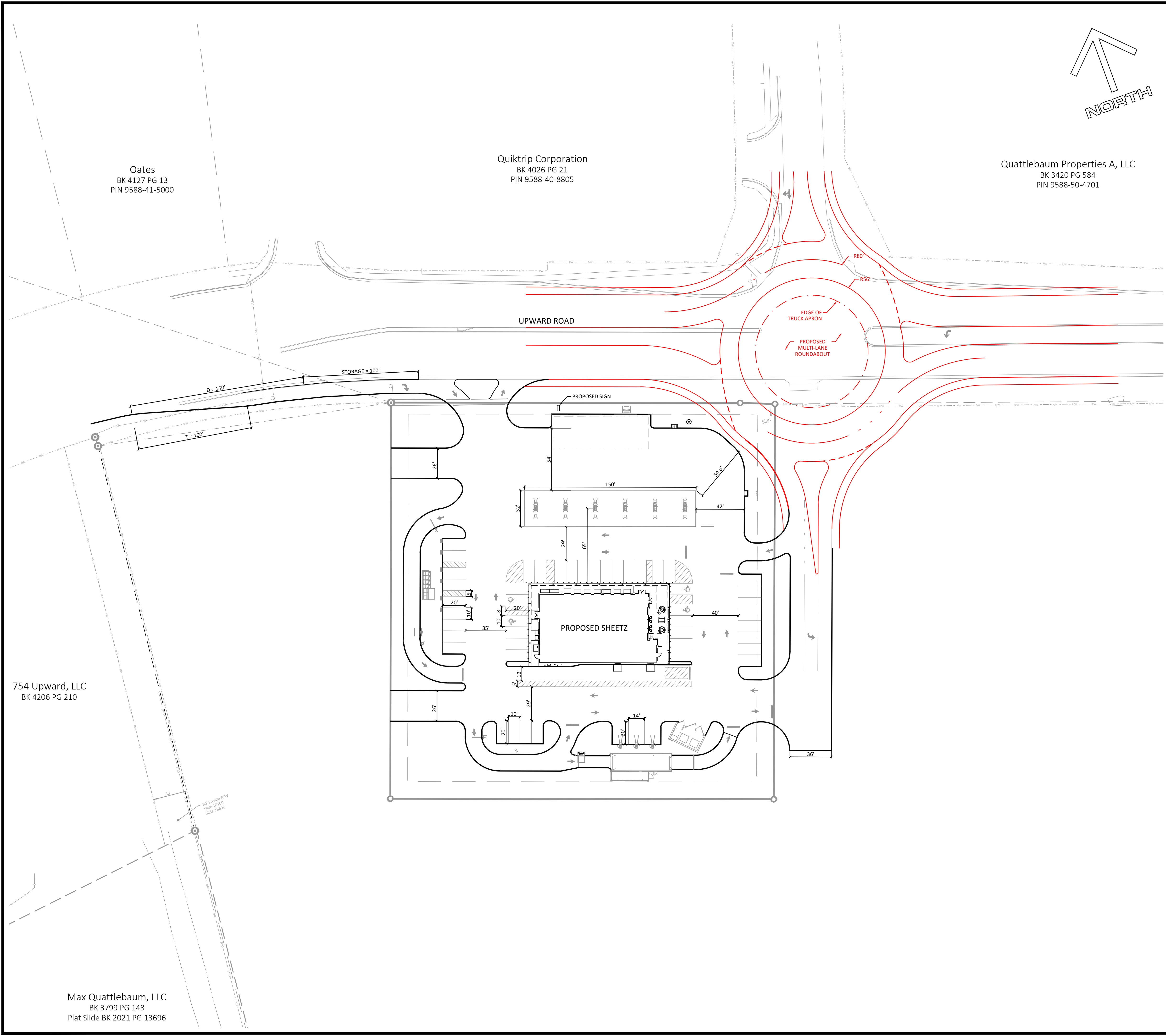
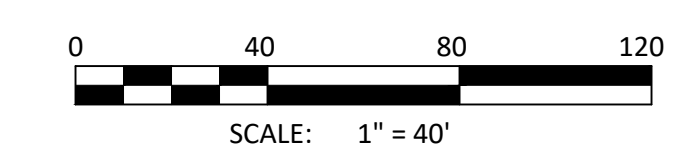
SITE LAYOUT NOTES

1. THIS IS NOT A PROPERTY SURVEY. SEE SURVEY FOR BOUNDARY INFORMATION.
2. ALL WORK SHALL COMPLY WITH LOCAL ORDINANCES, NCDOT STANDARDS AND OWNER SPECIFICATIONS. ALL SIGNS AND PAVEMENT MARKINGS SHALL COMPLY WITH MUTCD STANDARD.
3. CONTRACTOR IS RESPONSIBLE FOR ALL SAFETY, METHODS, PROCEDURES AND SEQUENCES OF ITS OPERATIONS. CONTRACTOR SHALL PROVIDE ADEQUATE TRAFFIC CONTROL FOR ITS OPERATIONS IN COMPLIANCE WITH MUTCD AND NCDOT STANDARDS. CONTRACTOR SHALL NOTIFY NCDOT, OWNER, AND ENGINEER AT LEAST 72 HOURS IN ADVANCE OF ANY LANE CLOSURE OR TRAFFIC SHIFT.
4. ALL WORK IS SUBJECT TO INSPECTION OF THE OWNER, ENGINEER AND AUTHORITIES HAVING JURISDICTION. THE OWNER SHALL PAY FOR ALL QUALITY ASSURANCE TESTING REQUIRED. CONTRACTOR SHALL PROTECT THE WORK UNTIL SUCH TIME THAT THE PROJECT IS APPROVED AND OPENED TO TRAFFIC.
5. SUBGRADE SHALL BE ACCEPTED BY THE ENGINEER PRIOR TO PLACEMENT OF STONE BASE. CONTRACTOR SHALL PROVIDE A LOADED TRUCK FOR ALL PROOF-ROLLS REQUIRED.
6. STONE BASE SHALL BE ACCEPTED BY THE ENGINEER PRIOR TO ASPHALT PLACEMENT. CONTRACTOR SHALL PROVIDE A LOADED TRUCK FOR ALL PROOF-ROLLS REQUIRED.
7. PAVING CONTRACTOR SHALL PROVIDE A CERTIFIED ROADWAY QUALITY CONTROL TECHNICIAN WITH A NUCLEAR GAGE AT ALL TIMES ASPHALT IS BEING PLACED. IF REQUESTED, THE CONTRACTOR SHALL BE RESPONSIBLE FOR TAKING ASPHALT CORE SAMPLES FOR VERIFICATION OF COMPACTION AND THICKNESS.
8. SLOPES IN HANDICAP PARKING SPACES AND ACCESS AISLE AREAS SHALL NOT EXCEED 2% IN ANY DIRECTION INCLUDING THE DIAGONAL.
9. CONTRACTOR SHALL MAINTAIN AN APPROVED SET OF PLANS ON THE JOB SITE AT ALL TIMES.

SITE DATA

GENERAL	
LATITUDE	35°17'48" N
LONGITUDE	82°24'13" W
LOT 1	2.65 AC.
PROPOSED ZONING	CHMU (COMMERCIAL HIGHWAY MIXED USE) [CITY OF HENDERSONVILLE]
PROPOSED USE	C-STORE
	6,139 SF
PARKING:	
REQUIRED	
WASH BAY	0.5 SPACE PER WASH BAY PLUS 2 (3 SPACES)
GAS STATION	1 PER 200 SQ. FT. (31 SPACES)
PROVIDED	48 SPACES
SETBACKS	
FRONT	10'
SIDE	15'
REAR	15'

PRELIMINARY - CONCEPT PLAN
THIS IS A PRELIMINARY CONCEPT PLAN INTENDED FOR PLANNING AND DESIGN. THIS IS NOT A CONSTRUCTION PLAN. THIS PLAN HAS NOT BEEN APPROVED BY GOVERNING AUTHORITIES. ZONING COMPLIANCE DATA SHOWN HEREON HAS NOT BEEN CONFIRMED BY GOVERNING AUTHORITIES.



Max Quattlebaum, LLC
BK 3799 PG 143
Plat Slide BK 2021 PG 13696

Agent Authorization Letter

Blue Ridge Engineering PLLC
924 Main Street, Suite 200
North Wilkesboro, NC 28659

RE: Sheetz – Flat Rock, NC

To Whom It May Concern,

This letter gives Blue Ridge Engineering and/or Jesse Hanlin, P.E. the authority to pursue site development, plan approvals, and for all site development permits for the Sheetz project, located on Upward Road in Hendersonville, Henderson County, North Carolina, per plans prepared by Blue Ridge Engineering PLLC.

Please contact me at (828) 674-1953 or [REDACTED] with any questions.

Sincerely,

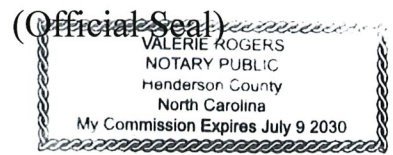
Quattlebaum Properties F LLC
By: 
Name: Robert Quattlebaum
Title: Member/Manager

[see notary on following page]

North Carolina
Henderson County

I, Valerie Rogers, a Notary Public for said County and State, do hereby certify that Robert Quattlebaum personally appeared before me this day and acknowledged the due execution of the foregoing instrument.

Witness my hand and official seal, this the 26th day of November, 2025.



Valerie Rogers, Notary Public

Valerie Rogers, Notary Public (printed name)

My Commission Expires: July 9, 2030



For use by Principal Authority / Para uso de la Autoridad Principal
Cloudpermit application number / Número de solicitud de Cloudpermit US-NC30720-P-2026-3
PIN / Número de rollo 9588407325
Application submitted to / Solicitud presentada a Hendersonville, NC, North Carolina / Hendersonville, NC, Carolina del Norte

Description of Subject Property
Address / Dirección 0 NO ADDRESS ASSIGNED
Municipality / Municipio Hendersonville, NC, North Carolina / Hendersonville, NC, Carolina del Norte
PIN / Número de rollo 9588407325



Purpose of Application
Application type / Tipo de solicitud Site Plan Review — Final Site Plan Review

Applicant, Engineer		
Last name / Apellido Hanlin	First name / Nombre de pila Jesse	Corporation or partnership / Corporación o sociedad Blue Ridge Engineering, PLLC
Street address / Dirección de la calle 924 Main Street	Unit number / Número de unidad Suite 200	Lot / Con.
Municipality / Municipio North Wilkesboro	State / Provincia NC	ZIP code / Código postal 28659
Other phone / Otro teléfono	Mobile phone / Teléfono móvil +1 3368382500	
Fax	Email / Correo electrónico [REDACTED]	

Property owner		
Last name / Apellido QUATTLEBAUM PROPERTIES F LLC	First name / Nombre de pila	Corporation or partnership / Corporación o sociedad
Street address / Dirección de la calle PO BOX 5007 HENDERSONVILLE NC 28793	Unit number / Número de unidad	Lot / Con.
Municipality / Municipio	State / Provincia	ZIP code / Código postal
Other phone / Otro teléfono	Mobile phone / Teléfono móvil	
Fax	Email / Correo electrónico	

Applicant-Company Information		
Applicant Name: Wes Hall	Company Name: Sheetz	Authorized Representative (if applicable) Section 5, Item A.

Property Owner-Company Information		
Property Owner Name: Max Quattlebaum	Company Name (if applicable, check corresponding box below) Quattlebaum Properties F, LLC	Authorized Representative Title (if applicable)

Declaration and Signatures
<p>Applicant</p> <p>I, Jesse Hanlin (The Applicant), do hereby declare that the information contained in this application, the attached schedules and forms, the attached plans and specifications, and other attached documentation is true to the best of my knowledge. If a permit is granted, I agree to comply with Local Ordinances and the conditions of the permit. If the Applicant is a corporation or partnership, I have the authority to bind the corporation or partnership by signing off, I understand that it constitutes a legal signature confirming that I acknowledge and agree to the above declaration.</p> <p>Signature of the applicant acknowledges that if the property is rezoned, the property involved in this request is bound to the use(s) authorized, the approved site plan and any conditions imposed, unless subsequently changed or amended as provided for in the Zoning Ordinance.</p> <p> Digitally signed on 01/06/2026, 10:42:23 AM EST by Jesse Hanlin. / Firmado digitalmente el 6/1/26 10:42:23 EST por Jesse Hanlin.</p> <p>Property owner</p> <p>I, QUATTLEBAUM PROPERTIES F LLC (The Property owner), do hereby declare that the information contained in this application, the attached schedules and forms, the attached plans and specifications, and other attached documentation is true to the best of my knowledge. If a permit is granted, I agree to comply with Local Ordinances and the conditions of the permit. If the Property owner is a corporation or partnership, I have the authority to bind the corporation or partnership by signing off, I understand that it constitutes a legal signature confirming that I acknowledge and agree to the above declaration.</p> <p>Property owner hereby grants permission to the City of Hendersonville personnel to enter the subject property for any purpose required in processing this application. If signed by an agent on behalf of the Owner, this petition MUST be accompanied by a Limited Power of Attorney signed by the property owner (s) and notarized, specifically authorizing the agent to act on the owner (s) behalf in signing this application. Failure of each owner, or their duly authorized agent, to sign, or failure to include the authority of the agent signed by the property owner, will result in an INVALID APPLICATION.</p> <p>Signature of the property owner acknowledges that if the property is rezoned, the property involved in this request is bound to the use(s) authorized, the approved site plan and any conditions imposed, unless subsequently changed or amended as provided for in the Zoning Ordinance.</p> <p> Digitally signed on 01/06/2026, 10:42:01 AM EST by Jesse Hanlin with an authorization letter from QUATTLEBAUM PROPERTIES F LLC. / Firmado digitalmente el 6/1/26 10:42:01 EST por Jesse Hanlin con una carta de autorización de QUATTLEBAUM PROPERTIES F LLC.</p>

City of Hendersonville Final Site Plan Review Process

Section 5, Item A.

This Outline of Understanding is established to define the prerequisites and conditions for the approval of the final site plan Hendersonville.

Section 1: Approvals by the Planning Division:

The City of Hendersonville Planning Division shall not grant final site plan approval until the site plans and all supporting documents are approved. This includes, but is not limited to:

- 1) Compliance with any applicable Zoning site requirements including but not limited to:
 - a. 7-3-4.3 Contents of final site plan.
 - b. Architectural and Design Review (if applicable).
 - c. Conditions associated with a Conditional Zoning District (if applicable).

Section 2: Approvals by Other Departments or Outside Entities (if applicable):

- 1) Soil and Erosion Control: Approval from Henderson County Site Development Department for Soil and Erosion Control Measures. <https://www.hendersoncountync.gov/waterresources/page/soil-erosion-and-sediment-control-plan-requirements>
- 2) Stormwater: Approval of the stormwater management plan from the City of Hendersonville Stormwater Division ensuring compliance with applicable regulations. <https://www.hendersonvillenc.gov/stormwater/forms-documents>
- 3) NCDOT Driveway and Encroachment Approvals: Permission and permits from the North Carolina Department of Transportation for driveways and any encroachments onto state-controlled roadways. (828) 891-5367 <https://connect.ncdot.gov/municipalities/Utilities/UtilitiesDocuments/NCDOT%20Driveway%20Permit%20Submissions%20User%20Guide.pdf>
- 4) City of Hendersonville Public Works Encroachment Permits: Permits from the City of Hendersonville for any encroachments onto city-controlled properties or rights-of-way. <https://www.cognitofirms.com/CityOfHendersonville4/CityOfHendersonvilleEncroachmentApplication>
- 5) Water and Sewer Approval: Approval from the relevant utility departments for water and sewer connections, services or extensions. <https://www.hendersonvillenc.gov/water-sewer/development-new-connections>
- 6) Floodplain Development Approval: Compliance with the City of Hendersonville Flood Damage Prevention Ordinance and any other necessary approvals. <https://www.hendersonvillenc.gov/stormwater/floodplain-management>

Section 3: Review Process:

- 1) The developer shall submit a completed final site plan review application signed by all required parties with appropriate fee.
- 2) The Planning Department will adhere to a 12-business day review timeline for the Planning Review. Outside departments and entities may have a longer review period.
- 3) The developer shall also submit all required documents and applications to the respective outside departments and entities for review and approval.
- 4) Final site plan approval and subsequent Zoning Compliance Permit shall not be granted until all required documents and approvals are provided to the Planning Division.

Section 4: Revisions and Resubmission:

- 1) If any discrepancies or issues are identified during the review process, the developer will be required to make necessary revisions and resubmit the documents for further review.
- 2) Site Plan revisions shall be bubble or otherwise identified on the site plans. Responses to the City's comments shall also be provide in a separate document.
- 3) Any resubmittal will start a new 12-business day review timeline for the Planning review.

Section 5: Contact Information:


- 1) For inquiries or concerns related to this Outline of Understanding, please contact the City of Hendersonville Planning Division at 828-697-3010.

Acknowledgement:

Section 5, Item A.

This Outline of Understanding of the final site plan review process is established and agreed to by the developer and property owner.

Date
01/06/2026

 Signed by / Firmado por **Jesse Hanlin** 01/06/2026,
10:36:36 AM EST / 6/1/26 10:36:36 EST

Preliminary Site Plan Review Triggers:

Projects proposing any of the following must submit a complete application at least 32 days prior to any Planning

Board meeting.

- Any commercial, industrial or institutional development consisting of 20,000 or more square feet of floor area.
- Addition of more than 30 parking spaces.
- Any minor planned residential development consisting of nine or more dwelling units.

Information

Type of Plan Review
Final

List 10 digit PIN or 7 digit PID number for each property
9588407325

CHECK TYPE OF DEVELOPMENT

Commercial

Current Zoning
CHMU

Total Acreage
2.65

Proposed Building Sq.ft.
7788.0 sq.ft.

of Dwelling Units
0

Applicant (Developer) Company Information

Authorized Representative Name:
Wes Hall

Company Name (if applicable, check corresponding box below)
Sheetz

Company Type:

- Corporation: Limited Liability Company: Trust:
 Partnership: Other:

If other:

Authorized Representative Title (if applicable - i.e. Member/Manager, President, etc.)

Property Owner Company Information (if different from Applicant)

Authorized Representative Name:
Max Quattlebaum

Company Name (if applicable, check corresponding box below)
Quattlebaum Properties F, LLC

Company Type:

- Corporation: Limited Liability Company: Trust:
 Partnership: Other:

If other:

Authorized Representative Title (if applicable - i.e. Member/Manager, President, etc.)