



CITY COUNCIL

AGENDA

MONDAY, AUGUST 11, 2025

Where Life is Sweet

COUNCIL CHAMBERS - 180 NE 2ND ST.

*Other ways of viewing or participating in live meetings are available through:
YouTube at: <https://bit.ly/HermistonYoutube>*

Zoom with Meeting ID: 829 0043 5622 Passcode: 417097 Telephone number to join is: 1 253 215 8782; or submitting comments to meetings@hermiston.gov

For written electronic public comments to be part of the official record, sender must provide their full name and place of residence and comments must be received within the time frame given for the item under discussion. The City Recorder will respond/confirm to sender that their electronic comment was received and will be made part of the record; or, if their electronic comment is not able to be made part of the record, the City Recorder will respond to the sender and state the reason(s) why.

1. CALL REGULAR MEETING TO ORDER – 7:00 PM

2. DECLARATION OF QUORUM

3. FLAG SALUTE

4. PROCLAMATIONS, PRESENTATIONS AND RECOGNITIONS

A. PATH Quarterly Report

5. CITIZEN INPUT ON NON-AGENDA ITEMS

Anyone wishing to bring anything before the council that is not on the agenda is asked to please do the following: 1. Please limit comments to not more than FIVE minutes; 2. State your name and address; 3. Direct your comments to the Chair.

6. CONSENT AGENDA

A. Committee Vacancy Announcement

B. Minutes of the July 28th City Council Regular Meeting

7. ITEMS REMOVED FROM CONSENT AGENDA

8. PUBLIC HEARINGS

- [A.](#) Annexation 4N2802BA Tax Lot 100 Jaber Investment, LLC 2455 NE 7th St

9. ORDINANCES AND RESOLUTIONS

- [A.](#) Ordinance No. 2376 Annexation Jaber Investment LLC 4N2802BA Tax Lot 100 - 2455 NE 7th St (See Public Hearing Section Above)
- [B.](#) Resolution 2383- Initiate LID for W Dogwood
- [C.](#) Resolution 2384- Initiate LID for W Dogwood
- [D.](#) Resolution 2385- Initiate LID for NW 2nd St
- [E.](#) Resolution 2386- Initiate LID for NW 2nd St
- [F.](#) Resolution 2387- Declare Intent to Form LID for W Dogwood
- [G.](#) Resolution 2388- Declare Intent to Form LID for W Dogwood
- [H.](#) Resolution 2389- Declare Intent to Form LID for NW 2nd
- [I.](#) Resolution 2390- Declare Intent to Form LID for NW 2nd
- [J.](#) Resolution 2391- SE 10th Street Jurisdictional Transfer

10. OTHER

- [A.](#) Report on and Discussion of City Chats FY24-25

11. COMMITTEE REPORTS

- A.** City Committee and Liaison:

Airport Advisory, Budget, Hispanic Advisory, Library Board, Parks and Recreation, Planning Commission, Recreation Projects Fund, Faith-Based Advisory, Community Accountability, Public Safety, Public Infrastructure, Transit Planning, EOTEC, Stepping Stones Alliance (not a City Committee)

- B.** Mayor's Report
- C.** Council President Report
- D.** Council Report
- E.** Youth Advisory Report
- F.** Manager's Report

12. ADJOURN

**** AMERICANS WITH DISABILITIES ACT NOTICE****

Please contact Hermiston City Hall, 180 NE 2nd Street, Hermiston, OR 97838 (Phone No. 541-567-5521) at least 48 hours prior to the scheduled meeting time if you need an accommodation. TTY and TDD users please call Oregon Telecommunications Relay Service at 1-800-735-2900 or 711.

PROJECT PATH

4th Quarter Report, FY 24-25



Stepping Stones
ALLIANCE

Introduction

EXECUTIVE SUMMARY

This report is provided in accordance with the Service Agreement between Stepping Stones Alliance and the City of Umatilla as part of the West Umatilla County partnership to provide services to our community's homeless residents through Project PATH (Practical Assistance through Transitional Housing). This report provides an update on site preparations, community outreach, strategic plan, budget, and general services. More detailed materials are available upon re-request which might include draft/adopted operational policies and procedures, meeting minutes of Stepping Stones Alliance, or general member policies and code of conduct. This report is intended to serve as a high level update of the services provided and the general progress we have made thus far.

Pilot Program Contacts

County Commissioner
Dan Dorran

City Managers
Dave Stockdale, City of Umatilla
Byron Smith, City of Hermiston
Darla Linker, City of Stanfield
Dave Slaght, City of Echo

Project Contact
Marisela Morales

Stepping Stones Contact
Jesalyn Cole

PERSONNEL UPDATES

During this reporting period, we implemented key updates to enhance staff coverage and member support. These updates include adjustments to staff schedules, expanded security coverage, and the addition of a new hire to improve accessibility through shuttle services.

Staffing Overview

During this quarter, we maintained five (5) hourly staff members providing support in both staff assistant and site support roles. Additionally, our mid-day shuttle service continues to operate at 12 hours per week, ensuring reliable transportation during peak hours.

Staffing Adjustment

To address a temporary staffing gap resulting from a medical leave of absence, we hired an additional hourly employee. This individual initially served in a volunteer capacity and has now transitioned into an on-call role. They will remain available to provide coverage for staff absences due to illness or vacation, as needed.

Phoenix Overnight Operations

The Phoenix overnight program remained active throughout the quarter, operating nightly from 8:00 PM to 6:00 AM, seven days a week. Staffing levels have been maintained with one (1) officer on-site during each overnight shift.

PARTNERSHIP HIGHLIGHTS

Stepping Stones Alliance remains committed to cultivating strong community and regional partnerships that support our mission and enhance the services we provide. This quarter, we continued to collaborate with a wide range of organizations, both new and existing, to share our resources, exchange ideas, and create meaningful opportunities for our guests and members. The following highlights reflect the growth and strength of these partnership and the collective impact we are building together.

Donation and Sponsorships

Financial Donations

- We received between \$25-\$200 from Community Donors.

Groceries and Meal Donations

- Hermiston Altrusa Club.
- Oasis Vineyard Church.
- Ye Olde Pizza Shoppe.
- Veg Out.
- Community Members.

Gala Sponsors

- **Bedrock Sponsors:** Atkinson Staffing and AWS donated \$5,000 each.
- **Hearth Sponsors:** Kiwanis donated \$1,000.
- **Cobblestone Sponsors:** ABR, Wheatland, Sanitary Disposal, ITC, and UEC donated \$500 each.
- **Riprap Sponsors:** Hermiston Generating, Kirby Nagelhout, Rogers Toyota, and AWS donated different dollar amounts.

COMMUNITY OUTREACH AND ENGAGEMENT

Our media outreach has remained consistent this quarter, with regular “Thankful Thursday” posts, weekly grocery list updates each Friday, and periodic targeted messages such as special shout-outs and specific calls to action. We have also continued to distribute monthly newsletter updates to subscribers via our website, ensuring steady communication and engagement with our community.

Community Outreach Updates

Social Media Posts

- 30 total posts on Facebook and Instagram.
- **Facebook:** 46K views, 9K reach, 402 content interactions, and 901 followers.
- **Instagram:** 1K views, 146K reach, 64 content interactions and 179 followers.

Community Engagement

Attended the following meetings and events

- Hermiston Faith-Based Advisory Council.
- PATH Advisory.
- Local Planning Group.
- Meetings with Ducote, AWS, CAPECO, Umatilla City Staff, Marlette Staff, and Phoenix Protective.
- Schnitzer Care grant award ceremony.
- Proud River Leadership dinner.

Hosted the following organizations and representatives:

- Crossroads Community Church Members.
- Umatilla County Fellowship of Christian Athletes.
- Brooke Matthews from the ROCC/OC3.
- Robbin from CAPECO.
- Judy from Hui-Pasquini from Oregon Housing & Community Services.
- AgWest representatives.
- AWS representatives.
- CCS Supported Employment Division.

Presentations given to

- Crossroad Community Church in Stanfield.

NAVIGATION CENTER & MEMBER PROGRAMMING

Health and Community Engagement

- Partnered with Umatilla Health to host an STD testing day in April.
- Participated in the ***I love my City event*** by hosting free haircuts on site and coordinating transportation for homeless individuals.
- Continued monthly outreach in collaboration with OWhN to provide resources and information at local homeless camps.
- Staff completed QPR (Question, Persuade, Refer) suicide prevention training through CCS.

Member Services and Activities

- Monthly member birthday celebrations are held to help foster a sense of community and recognition.
- Bi-weekly member meetings are held to provide updates, share community news, and address ongoing challenges.
- Daily wellness and ISU cleanliness checks are conducted for each member.
- Ongoing support is provided in employment, housing, health, and essential needs.
- One-on-one check-ins with members have been introduced to strengthen engagement and support.
- Classes are now offered regularly, focusing on both educational topics and recreational activities.



FUNDRAISING AND GRANTS

Significant efforts have been made to secure funding to support and sustain our initiatives. This section highlights the progress made in identifying and pursuing funding opportunities, including the status of awarded grants, pending applications, and targeted fundraising activities. These efforts are critical to ensuring the continued impact and growth of our programs.

Awarded Grants

- Roundhouse – \$2,500 for transportation staffing.
- AgWest Farm Credit – \$2,500 for parking lot improvements.

Pending Applications

- Wildhorse Foundation \$30,000 for outdoor coverings, awning on Sleep Center and outdoor common space.

Annual Gala Fundraising Event

“Enchanted Garden”

- Date: Saturday, October 11.
- Location: EOTEC.
- Time: 6:00–10:00 PM.
- Sponsorships and ticket sales launching soon.



CAPITAL PROJECTS AND PURCHASES

Completed Projects

- Installed bathroom fans in the Sleep Center.
- Added three new outdoor water spigots.
- Improved the area between asphalt parking lot and Lind Road.
- Raised garden beds were built by Hermiston High School's construction class.

Pending Projects

- Adding an awning to the Sleep Center.
- Ordering an outdoor common space shelter.
- The construction of a new four-unit shower/bathroom building, including an ADA-compliant unit.

Appliance Upgrades

- Schnitzer Care funds were used to purchase new commercial-grade washers and dryers for both the Sleep Center and Navigation Center.

Technology Investments

- Upgraded the Sleep Center's computer and monitor used for security camera monitoring.

Inventory and Resource Replenishment

- Purchased mattresses, bedding, and storage items to improve comfort for members.

Garden Purchases

- Purchased plants, planters, and gardening materials to support the raised beds constructed by Hermiston High School's Construction class.

GUEST NUMBERS AND VOLUNTEERS

ISU Occupancy

- **April:** 411 guest stays, 13 new guests, 4 ISU transitions, with all 21 ISUs fully occupied.
- **May:** 438 guest stays, 16 new guests, 3 ISU transitions, with all 21 ISUs fully occupied.
- **June:** 414 guest stays, 13 new guests, 6 ISU transitions, with all 21 ISUs fully occupied.

Meals Served This Quarter

- 91 total meals.
- 54 meals donated by volunteers/community.
- 37 meals prepared by staff.

Volunteer Trends

Sleep Center Volunteer Shifts

- **April:** 22 shifts.
- **May:** 24 shifts.
- **June:** 21 shifts.



Additional Photos



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What is SS Looking forward to this next quarter?

Stepping Stones Alliance is excited about the continued growth and progress across our program and facilities. Upcoming projects and developments include:

- Offering more classes for members, especially outdoors, to enjoy the weather and use new recreational items.
- Moving forward with the next phase of ISUs in partnership with Marlette/Clayton Homes, as our numbers continue to grow.
- Continuing participation in community events to share information and support public education about homelessness.
- Supporting members as they make steady progress toward their personal goals.
- Improving the Navigation Center kitchen and outdoor common space through CDBG funding.
- Adding an awning at the Sleep Center to help protect guests from extreme weather during evening check-in.
- Installing an outdoor common space shelter before winter.
- Hosting a screening of No Address at the Hermiston movie theater, sponsored by AWS (details coming soon).
- Creating a donor recognition wall and planning a memorial garden to honor members we have served who have passed away.
- Installing three new visitor parking signs in the parking lot, which have already been ordered.





PUBLIC ANNOUNCEMENT

The City is accepting applications for the following Committees:

1. Charter Review Committee

- The City Council approved formation of the Ad-Hoc Charter Review Committee on July 28, 2025 to review the current City Charter and may make recommended changes, amendments, or determine that no changes are necessary.
- **Deadline to apply: August 26th** (Advertised 07/29/2025)

2. Parks and Recreation Committee

- **Position 4, 5, & 6: 3-year term ending October 31, 2028** (Advertised 06/18/2025)

3. Library Board

- **Position 3: 4-year term ending June 30, 2029** (Advertised 04/29/2025)

4. Airport Advisory Committee

- **Position 2: Remaining 3-year term ending October 31, 2026** (Advertised 11/08/2024)
- **Position 5: 3-year term ending October 31, 2028** (Advertised 06/18/2025)

5. Eastern Oregon Trade & Event Center (EOTEC) Committee

- **Positions 7: 3-year term ending June 30, 2028** (Advertised 04/29/2025)

Deadline to apply for Committees 2-5: Open Until Filled

Interested persons are asked to submit an application to City Hall, 180 NE 2nd Street, Hermiston, or at lalarcon-strong@hermiston.gov. Application forms are available at City Hall or on the City's website at <https://hermiston.or.us/volunteer>. If you have questions, please call Lilly Alarcon-Strong at 541-567-5521.

Proposed appointment and confirmation of these positions are made by the City Council. All appointments to city boards and commissions shall be made in accordance with the ordinances and city charter. Appointees shall not be full-time employees of the city, shall not be elected officials of the city, shall not be appointed to more than two boards or commissions at a time, and shall not sell to the city or its boards and commissions over which the council has appointive powers and budget control either directly as a prime contractor or supplier, or indirectly as a first-tier subcontractor or supplier. Sales shall be construed to mean sales, services or fees aggregating \$20,000 or more in any one calendar year. Preference for appointees shall be given to city residents.



CITY COUNCIL

Regular Meeting Minutes July 28, 2025

Mayor Primmer called the regular meeting to order at 7:00pm. Present were Councilors Hayward, Roberts, Myers, McCarthy, Linton, and Kelso. Councilors Duron and Barron were excused. City Staff in attendance included: City Manager Byron Smith, Assistant City Manager Mark Morgan, Finance Director Ignacio Palacios, Chief Jason Edmiston, Planning Director Clint Spencer, Court Administrator Jillian Viles, Parks and Recreation Director Brandon Artz, and City Recorder Lilly Alarcon-Strong. The pledge of allegiance was given.

Farm-City Pro Rodeo Presentation

Farm-City Pro Rodeo (FCPR) Board Members Jon Gill and David Bothum presented the Council with information and updates to include: 1988 origins with FCPR being created by cowboys for cowboys with \$1,000 per event and hosting 3 performances with a total purse of \$18,000 while the NFR total payout was \$2.2 million, to this years \$25,000 per event, many performances with a total purse of \$300,00 and NFR total payout expected to top over \$6 million; location challenges and changes from the old congested rodeo grounds downtown to the new grounds at EOTEC; multiple awards received throughout the years for top quality stock and the rodeo grounds dirt; equal payout in all events, which is rare; thanked local sponsors and the community for all their support; and announced that this year, FCPR is ranked the 15th Best Rodeo out of 700 Rodeo's in the US and Canada, climbing from last year's ranking of 28th.

Mayor Primmer thanked the FCPR for the information and congratulated them on the incredible accomplishments they have achieved.

Citizen Input on Non-Agenda Items

Wayne Welle, 745 E Quince Ave- Thanked the Council, Chief Edmiston, and the entire Police Department for the noticeable difference they have made in his neighborhood by their continued efforts, stating the neighborhoods call for help two weeks ago was heard and has renewed their sense of community with a unified purpose, but stated more work will need to continue to be done.

Manuel Gutierrez, 750 E Quince Ave- Echoed Mr. Welle's comments thanking the City Council, Chief Edmiston and the Police Department as a whole.

Mayor Primmer thanked Mr. Welle's and Mr. Gutierrez for returning to the City Council meeting to share the positive feedback.

Consent Agenda Items

Councilor McCarthy moved, and Councilor Hayward seconded to approve Consent Agenda item A-D, to include:

- A. Committee Vacancy Announcement
- B. Confirmation to reappoint Luke Swanson to Position #6 of the Eastern Oregon Trade & Event Center Committee for a 3-year term ending June 30, 2028
- C. Confirmation to reappoint Alberto Munoz to Position #5 of the Hispanic Advisory Committee for a 3-year term ending June 30, 2028
- D. Minutes of the July 14th City Council Work Session and Regular Meetings

Motion carried unanimously.



CITY COUNCIL

Regular Meeting Minutes July 28, 2025

Resolution No. 2382- Establishing a Charter Review Committee and Process

City Manager Smith gave information regarding the formation of a Charter Review Committee to engage the community and assist the Council in determining potential amendments as per the Council's 2025 Goals. Councilor Hayward moved, and Councilor McCarthy seconded to approve Resolution No. 2382 and lay upon the record. Motion carried unanimously.

Resolution No. 2383- Supporting an application by the City for a Housing Planning Assistance Grant from the State of Oregon- Planning Director Clint Spencer gave information regarding the proposal to apply for the Housing Planning Assistance Grant to allow the City to perform a code audit and draft new code to be in compliance with current and recent changes made in legislation.

After answering questions from the Council, Councilor Linton moved, and Councilor Hayward seconded to approve Resolution No. 2382 and lay upon the record. Motion carried unanimously.

June 2025 Monthly Financial Report

After answering questions from the Council, Councilor Myers moved, and Councilor Roberts seconded to accept the financial report as presented (PowerPoint Presentation attached) by City Manager Smith, Assistant City Manager Morgan, and Finance Director Ignacio Palacios. Motion carried unanimously.

July 2025 Quarterly Investment Report

Councilor Kelso moved, and Councilor McCarthy seconded to accept the Quarterly Investment Report as presented by Finance Director Palacios (PowerPoint Presentation attached). Motion carried unanimously.

Mayor Primmer and Councilor Kelso thanked Finance Director Palacios for the great explanations he gave, the wise ways in which the City is investing, and the transparency the report and explanations were given.

Police Collective Bargaining Agreement for FY26 through FY28

City Manager Smith stated City staff met with the Hermiston Police Association for the bargaining process of a new union contract where a tentative agreement has been reached. City Manager Smith outlined proposed changes to the current agreement as outlined in the agenda packet staff report and answered Council questions to include: proposed financial changes make Hermiston more competitive and place the City where it needs to be when compared to similar sized communities, the City contributing 1% of retirement benefits for each union member regardless if they also contribute, and more.

Councilor Linton moved, and Councilor Myers seconded to approve the FY26-FY28 Collective Bargaining Agreement with the Hermiston Police Association as presented. Motion carried unanimously.

Committee Reports

Library Board- Councilor Roberts shared information regarding construction updates, hiring new individuals to replace those who have retired, and circulation and card issuance statistics.

Public Safety Committee- Mayor Primmer stated the Committee met prior to the City Council meeting and heard updates from the Police Department and Municipal Court, to include: crime statistics, calls for service updates, new Police Department contract with Good Shephard Hospital, Court adjudication time frames, and court staff member Jennifer Herrera will be moving to the Finance Department.



CITY COUNCIL

Regular Meeting Minutes

July 28, 2025

Mayors Report

Mayor Primmer spoke regarding:

- Thanked Council President McCarthy for convening the last City Council meeting while he was on vacation
- Encouraged the community to partake in Hermiston's delicious watermelons
- Read aloud a Thank you letter from the Sherman's (attached)
- Announced the following events:
 - National Night Out and National Night Out Scavenger Hunt where the winner will receive a free Cuppa Yo Block Party
 - Umatilla County Fair Kick-Off Parade
 - Umatilla County Fair and Farm-City Pro Rodeo
 - MelonFest

Council Presidents Report

Councilor McCarthy thanked Assistant City Manager Morgan for agreeing to be a judge for the Cutest Baby Contest during the Umatilla County Fair.

Council Reports

Councilor Kelso stated the Hermiston and Pendleton Kiwanis Clubs helped provide and serve food to Foster Kids and their Foster Families as they enjoyed a fun day at the Aquatic Center and thanked the great Aquatic Center staff for their incredible professionalism, stating each interaction he had with these staff members, most all teenagers, was always followed with "sir". Councilor Kelso encouraged the community to tune into the Umatilla County Fair YouTube Channel for Livestock Shows as the Hermiston School District IT interns, all teenagers, lead on this effort.

Councilor Linton echoed Councilor Kelso's comments regarding the professionalism of Aquatic Center staff stating she has witnessed, on two occasions, lifeguards in action as they saved small children from drowning.

City Manager's Report

City Manager Smith spoke regarding:

- Asked Council members to meet at the Community Center at 5:45pm on National Night Out to travel together for the Cuppa Yo Block Party
- Asked Council members to reach out to City Recorder Alarcon-Strong if they wish to attend the upcoming LOC training in Boardman

Adjourn

There was no other business and Mayor Primmer adjourned the regular City Council meeting at 8:14pm.

SIGNED:

Doug Primmer, Mayor

ATTEST:

Lilly Alarcon-Strong, CMC, City Recorder

To: Mayor Primavera and Hermiston
City Council —

We would like to thank you
for the honor you have
bestowed on us, by naming the
new park, "Sherman Park".

We are very humbled and
grateful to live in such
an inclusive community.
We are especially pleased
that in the future so
many children will be
able to enjoy themselves
in a beautiful and safe
place.

Thank you so very much
for such a great honor.

Ric + Karen Sherman



Hermiston Urban Renewal Agency (HURA)

- North Hermiston Urban Renewal Area – ROW appraisals complete with anticipated bidding this winter.



General Fund

- Twelfth month (or 100.0%) of 2024-25 fiscal year
- Monthly Revenues ended over projection by ~\$1,002k
 - When compared to budget, revenues met expectation at 105.3%



General Fund

- Monthly Expenses were under projection by ~\$1,617k
 - Building department's permit activity resulted in increased turnover for electrical inspections.



Special Revenue Funds

- Observations:
 - Transient Room Tax fund increased revenues (5% over budget) with corresponding expenses, difficult to estimate
 - Included is a EOTEC fund summary for June 2025 Financial Reports
 - It is presented compared to projected
 - Actual revenues are \$1,370k or 81% of budget
 - Actual expenditures \$2.6k and \$4.8k over in materials & services and capital, respectively



Utility and Street Funds

- Observations:
 - Utility fund ~\$947k over projected revenues
 - This corresponds to one time 'septic service' in recycled water



Capital Projects

- A number of projects in design:
 - Geer/Harper Realignment- full design expected by late Spring
 - N. 1st Sidewalk (additional ROW with UPRR has begun)
 - RWS Backup Generators
 - Well #6 Chlorination Structure
 - Aquifer Storage/Recovery – upper borehole complete
 - Well #4 Controls – contract awarded, project to 'commence' after peak season



Capital Projects

- Hangar Replacement – 'kit' delivered and set up to begin in August
- Library – Cabinet installation with furniture delivery in late August; elevator updates underway
- The Arc – PD moved in. Sand and finish interior upon PD vacation
- Public Safety Center
 - Interior framing nearly complete
 - Outside 'hardscape' in construction





Discussion Questions?

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Quarterly Investment Report Changes

- Investment report summary presented
 - The schedule summarizes investments by type and is presented in a 'new' format
 - The summary includes beginning and ending balances
 - The 'Change' column summarizes investment maturity and/or redemption
 - The 'Net Gain (Loss) or Interest' reports earnings for the period reported
 - The 'Fees' column presents associated fees (transaction, advisory, etc.)
 - The '% of Portfolio' column reports the ratio of each investment type in the city's investment holdings



Investment Summary

- Total investments ending June 30, 2025, were \$26.6 million
- For the quarter ended June 30, 2025, \$66k in municipal bonds
- \$9.5 million redeemed from the LGIP account to fund ongoing capital projects
- Interest earnings for the quarter ending June 30, 2025, totaled \$380k
- The average interest earned totaled 2.6%



Discussion

Questions?

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Where Life is Sweet

Mayor and Members of the City Council
STAFF REPORT
 For the Meeting of August 11, 2025

Title/Subject

Annexation 4N2802BA Tax Lot 100 Jaber Investment, LLC 2455 NE 7th St

Summary and Background

Ordinance No. 2376 annexing approximately 1.5 acres of land located at 2455 NE 7th St is ready for adoption.

Ibrahim Jaber of Jaber Investment, LLC has submitted an application to annex approximately 3.69 acres of land located at 2455 NE 7th St. The property lies on the south side of E Punkin Center Road approximately 150 feet west of NE 8th St. The property is within the urban portion of the urban growth boundary and is adjacent to city limits along the east property line. The applicant wishes to annex the property to connect to city services enabling residential development.

The property is described as 4N2802BA Tax Lot 100. The property has a comprehensive plan map designation of Medium Density Residential (M) which corresponds to the Medium-High Density Residential (R-3) zoning designation. The applicant requests annexation with an R-3 zoning designation. The property abuts R-4 land to the east that is developed with single family dwellings. Land to the south and west is zoned M on the comprehensive plan map and contain rural residential dwellings. Land to the north is not within the city's urban growth boundary and is a mix of rural residential and agricultural uses. The land has been located within the city's urban growth boundary since the adoption of the city comprehensive plan in 1984. The original 1984 comprehensive plan designated this property as M for residential development and no amendment to the comprehensive plan map designation affecting this parcel has been identified since that date.

The property is adjacent to E Punkin Center Road. E Punkin Center Road is a county road with paving adjacent to the site. Improvements to E Punkin Center Road are required at time of development of the property. The applicant has submitted a site plan application and proposes to construct a 76 unit apartment complex.

The property contains a 40' private easement along the east property line. This easement is locally known as NE 7th Street and provides private access for four dwellings and out buildings south of the site. Development staff have researched the easement and determined that the

easement is not suited for conversion to a public street in the future. The distance from the easement to NE 8th Street to the west is 150 feet and cannot provide safe access to Punkin Center and meet access spacing standards. Therefore, no dedication of this easement is required as a condition of annexation.

Utilities are available in the vicinity of the site. A 16" water main extends along the entire E Punkin Center Road frontage of the site, extending from the municipal water tower to the east to NE 4th Street to the west. Sufficient capacity exists in this water line to serve any potential development on this parcel and is designed to accommodate significant additional development in the northeast quadrant. Sewer is extended to E Punkin Center Road at NE 8th Street approximately 150 feet to the east. The sewer is of sufficient depth to extend to the west property line of the site and has adequate capacity to accommodate development.

155.05 of the Hermiston Code of Ordinances provides the requirements for annexations. The requirements for annexation are as follows:

1. The proposal is in conformance with all applicable state annexation requirements.
2. The property is contained within the urban portion of the urban growth boundary as identified on the comprehensive plan.
3. The proposed zoning is consistent with the underlying comprehensive plan designation.
4. Findings of fact are developed in support or denial of the annexation.
5. All city services can be readily extended, and the property owner is willing to bear costs associated with sewer, water, and roads.

The site development itself is not a criteria for approval or denial of annexation. Subsequent site development is a limited land use decision subject to additional notice and comment. As noted above, the city has received an application for construction of multi-family housing on the property. This is a permitted use under the current zoning and subject to administrative review and approval.

Public notice requirements have been satisfied through the following actions:

1. Notice was provided by direct mail to all property owners within 100 feet on June 18, 2025.
2. Notice was published in the East Oregonian on June 18 and 25, 2025.
3. A sign displaying a notice of public hearing was placed on the property on June 18, 2025.

The planning commission held a public hearing on the proposed annexation on July 9, 2025. Written testimony submitted in opposition to the annexation is attached to this report. Following the closure of the public hearing, the planning commission made a recommendation to the city council to annex the property with the proposed R-3 zoning designation.

Tie-In to Council Goals

Annexation is a matter of administration of city ordinances.

Fiscal Information

The property has an assessed value of \$96,440. Following annexation, an additional \$659 will be paid in property taxes to the city annually. Development of the property will increase the amount of property taxes paid.

Alternatives and Recommendation

Alternatives

The city council may choose to

- Approve the annexation
- Deny the annexation

Recommended Action/Motion

Staff recommends that the city council approve annexation of the property.

- Motion to approve the findings of fact
- Motion to impose conditions of approval
- Motion to adopt Ordinance No. 2376

Submitted By:

C.F. Spencer, Planning Director

ORDINANCE NO. 2376

AN ORDINANCE ANNEXING CERTAIN REAL PROPERTY ON E THEATER LANE, DESCRIBING SAID REAL PROPERTY, WITHDRAWING SAID REAL PROPERTY FROM SPECIAL DISTRICTS AND DESIGNATING ZONING.

THE CITY OF HERMISTON ORDAINS AS FOLLOWS:

SECTION 1. The following described real property is annexed to the City of Hermiston and is withdrawn from the Umatilla County Library District and the Umatilla County Sheriff's Office Law Enforcement District due to annexation:

THAT PORTION OF THE EAST HALF OF NORTHEAST QUARTER OF NORTHWEST QUARTER OF SECTION 2, TOWNSHIP 4 NORTH, RANGE 28 EAST, WM.; EXCEPTING THEREFROM THE WEST 292 FEET THEREOF; ALSO EXCEPTING THEREFROM THE SOUTH 1069 FEET THEREOF; MORE PARTICULARLY DESCRIBED AS FOLLOWS:

COMMENCING AT THE NORTHWEST CORNER OF SAID SECTION 02 THENCE NORTH 89°32'51" EAST ALONG THE NORTHERLY LINE OF SAID SECTION 2284.40 FEET TO THE TRUE POINT OF BEGINNING;

THENCE CONTINUING NORTH 89°32'51" EAST ALONG SAID LINE 370.45 FEET; THENCE SOUTH 00°17'51" EAST ALONG THE EASTERLY LINE THEREOF 444.10 FEET; THENCE SOUTH 89°29'12" WEST ALONG THE SOUTHERLY LINE THEREOF 370.45 FEET; THENCE NORTH 00°17'51" WEST ALONG THE WESTERLY LINE THEREOF 444.50 FEET TO THE SAID TRUE POINT OF BEGINNING

CONTAINS 3.78 ACRES

All being East of the Willamette Meridian, Umatilla County, Oregon;

SECTION 2. The City Zoning Map shall include the real property described in Section 1 above and shall be designated as Medium-High Density Residential (R-3) on said map.

SECTION 3. The findings of fact as adopted by the Council on August 11, 2025, are incorporated herein by this reference.

SECTION 4. The City Recorder shall promptly transmit a record of annexation proceedings to the Secretary of State and notify the County Assessor of the change in boundary.

SECTION 5. The effective date of this ordinance shall be the thirtieth day after its adoption.

SECTION 6. This annexation shall be complete when all necessary documents have been accepted and

filed by the Secretary of State.

ADOPTED by the Council this 11th day of August 2025.

SIGNED by the Mayor this 11th day of August 2025.

Doug Primmer, MAYOR

ATTEST:

Heather KP La Beau, ASSISTANT CITY RECORDER

DRAFT

Exhibit A

Findings of Fact for Jaber Investment, LLC Annexation

August 11, 2025

2455 NE 7th St

The city council shall make a decision upon determination that the annexation complies with the applicable criteria in §150.05 of the Hermiston Code of Ordinances relating to annexation.

- A.** §150.05(1) *The proposal is in conformance with all applicable state annexation requirements.*

Response:

1. The proposed annexation of the subject property is aligned with the City of Hermiston Comprehensive Plan, acknowledged as compliant by the State of Oregon, and codified in Code Section 156.02 of Title XV.
2. The City has received consent to annexation from the property owner for approximately 3.7 acres of land and an election was deemed not necessary by the city council on June 9, 2025 (*ORS 222.120(1)*)
3. Notice of public hearing was published in the local newspaper for two consecutive weeks prior to the planning commission hearing on June 18 and 25, 2025. Notices were also posted in four public places in the city for a like period. Comments or remonstrances received have been incorporated into the record. (*ORS 222.120(3)*)
4. Notice of public hearing was physically posted on the property on June 18, 2025. (*HZO §157.229(B)*)
5. Affected agencies were notified. (*ORS 222.005*)
6. A public hearing of the planning commission was held on July 9, 2025. Comments received at the hearing are incorporated into the planning commission record. (*ORS 222.120(2)*)
7. Notice of public hearing of the city council was published in the local newspaper for two consecutive weeks prior to the city council hearing on July 16 and 23, 2025. Notices were also posted in four public places in the city for a like period. Comments or remonstrances received have been incorporated into the record. (*ORS 222.120(3)*)
8. A public hearing of the city council was held on August 11, 2025. Comments received at the hearing are incorporated into the record. (*ORS 222.120(2)*)

The city council finds the proposal is consistent with all applicable state annexation requirements in ORS 222:

- a. The city has received consent from the property owners within the affected area
- b. An election has been deemed not necessary since consent from more than half the owners has been received
- c. The property is contiguous with the existing city limits
- d. All statutorily required notices have been published and posted

B. §150.05(2) *The property is contained within the urban portion of the urban growth boundary as identified on the comprehensive plan.*

Response:

- 9. Since the property is contiguous to the existing city limits, the annexation is in accord with Comprehensive Plan Policy 4 which promotes compact urban development within and adjacent to existing urban areas to ensure efficient utilization of land resources and facilitates economic provision of urban facilities and services.
- 10. Annexation is consistent with Policy 5 which requires the city to establish a program for annexation and efficient and orderly provision of public services.
 - a. Property is contained within the urban portion of the UGB (See Finding 11 below)
 - b. Proposed development is consistent with applicable comprehensive plan policies and map designations (See Finding 11 below)
 - c. All city services can be extended readily (See Findings 15-18 below)
 - d. Property owner(s) is willing to bear costs associated with extension of sewer, water and roads except for major facilities -- e.g. sewer pump station or major water main -- necessary to facilitate later growth. (See Findings 15-18 below)
 - e. Proposal is consistent with all applicable state requirements including ORS Chapter 222 governing annexations and Chapter 225 governing utility extensions. (See Findings 1-8 above)
- 11. The property is located within the urban portion of the urban growth boundary (UGB) as identified on the comprehensive plan map. The property is designated as "M" on the comprehensive plan. The M designation is a medium density residential comprehensive planning designation corresponding to the R-3 zoning designation on the city zoning map.

The city council finds that the property is contained within the urban portion of the urban growth boundary.

C. §150.05(3) *The proposed zoning is consistent with the underlying comprehensive plan designation*

Response:

12. The applicant has submitted an application to annex the property with an R-3 zoning designation.
13. The proposed Medium-High Density Residential (R-3) zoning designation is an implementing zoning designation for the M comprehensive plan map designation.

The city council finds that the proposed zoning is consistent with the underlying comprehensive plan map designation.

D. §150.05(4) *Findings of fact are developed in support or denial of the annexation.*

Response:

14. This document, consisting of three pages of findings adopted by the city council on August 11, 2025 serves as findings of fact in support of annexation.

E. §150.05(5) *All city services can be readily extended, and the property owner is willing to bear costs associated with sewer, water, and roads.*

Response:

15. Sewer is available to service this property at NE 8th St and E Punkin Center Road. At the time of connection, the applicant is responsible for all connection fees, and extension of the sewer line to the western most property line.
16. A 16" water main extends along the entire E Punkin Center Road frontage of the site, extending from the municipal water tower to the east to NE 4th Street to the west. Sufficient capacity exists in this water line to serve any potential development on this parcel.
17. A traffic impact analysis has been prepared considering the traffic impacts generated by a medium-high residential development at the potential maximum density for the parcel size. All intersections studied continue to operate at mobility targets following development. No additional off-site mitigation measures are necessary as a result of development of the parcel.
18. Half-street improvement to E Punkin Center Road along the entire frontage of the subject parcel is necessary as a condition of subsequent development. E Punkin Center Road is classified as a major collector street and major collector improvements, including rebuilding of the road to centerline, curb, gutter, sidewalk, and drainage improvements will be required as a condition of development.

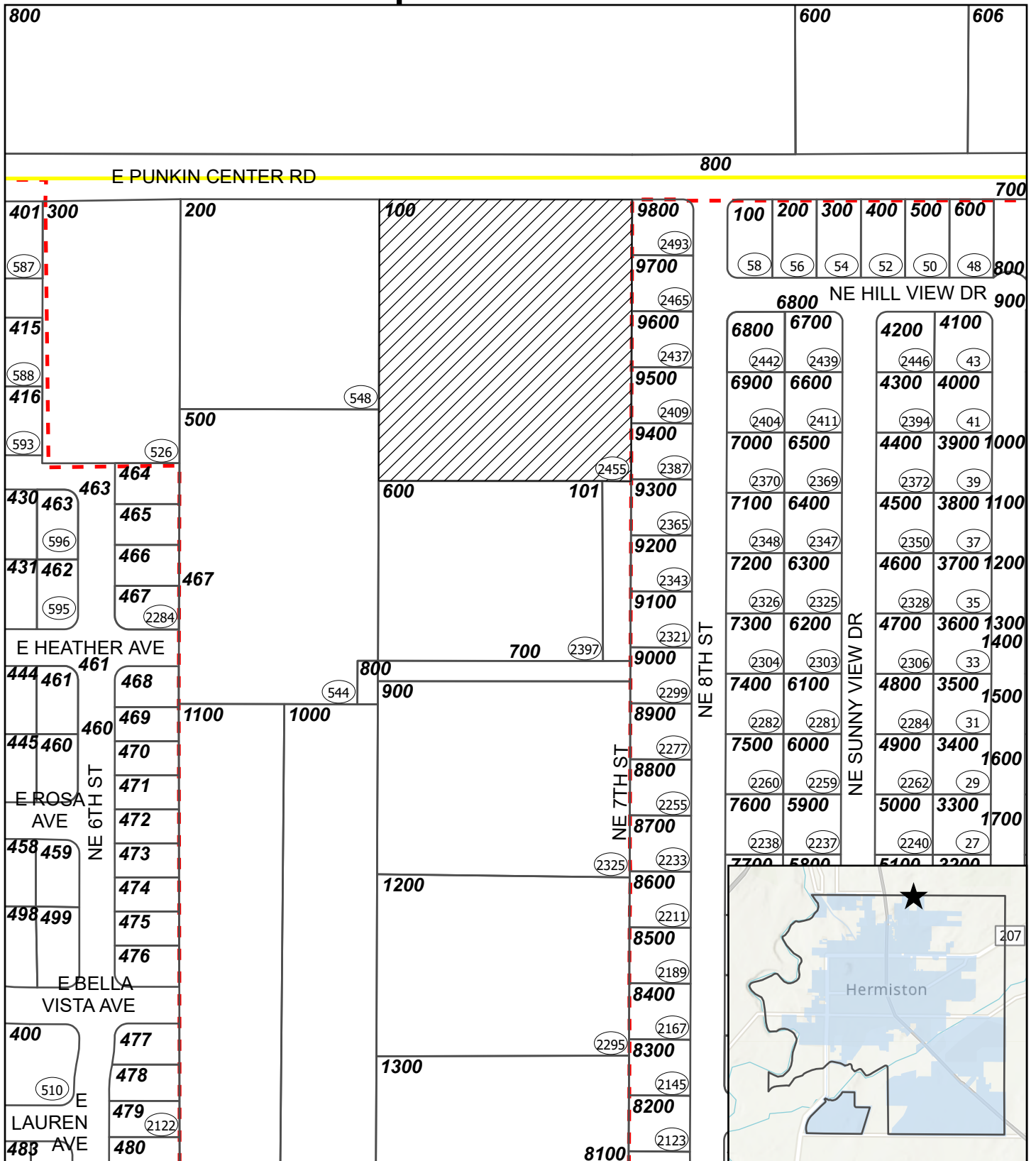
The city council finds that all city services can be readily extended and the property owner is will to bear costs associated with sewer, water, and roads.

Exhibit B**Conditions of Approval for Jaber Investment, LLC Annexation****August 11, 2025****2455 NE 7th St**

Subject to the deliberations of the planning commission following the public hearing, the following draft conditions of approval are presented:

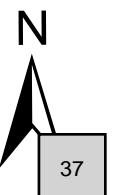
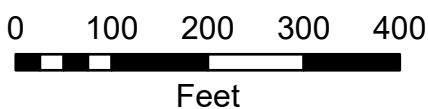
1. The property lies within an area subject to potential groundwater pollution hazards due to excessively well-drained soils. Therefore, the outdoor storage of hazardous chemicals and the underground storage of gasoline and diesel fuels are prohibited per §157.101(B) of the Hermiston Code of Ordinances. Per §157.101(D) of the Hermiston Code of Ordinances, a developer may receive an exemption from this requirement upon submission of evidence from a registered engineer that the storage will not contribute to groundwater pollution.
2. At such time that construction occurs on the property which triggers the development standards of 157.163 of the Hermiston Code of Ordinances, street improvements are required along that property's frontage on E Punkin Center Road. Street improvements shall be consistent with major collector improvements in ST-09 of the public works standards.

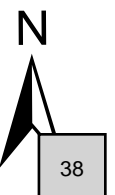
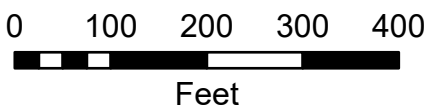
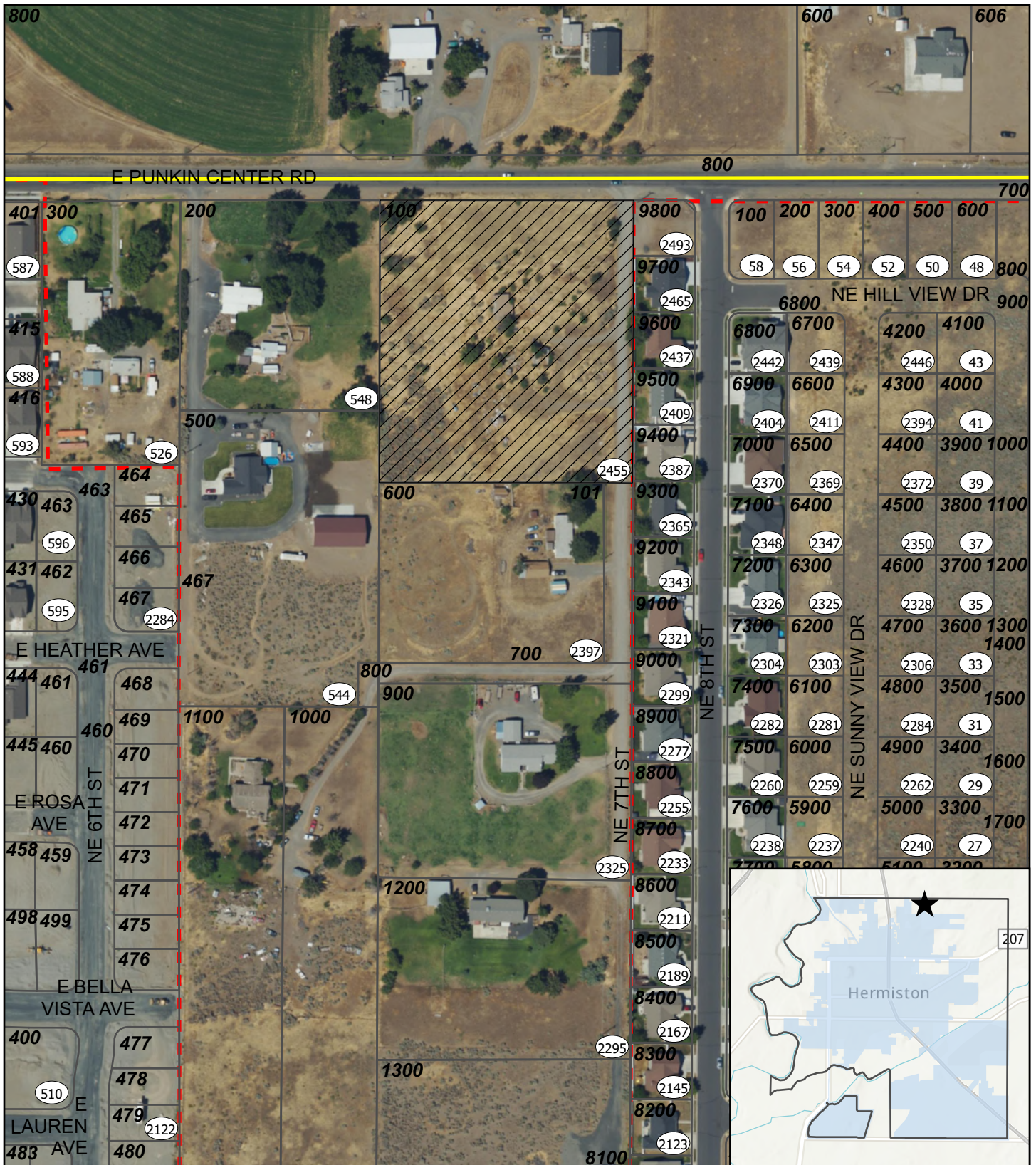
Notice of Proposed Land Use Action



Legend

- Area of Proposed Annexation
- Property Line
- City Limits
- Urban Growth Boundary





CITY OF HERMISTON

APPLICATION FOR ANNEXATION

Pursuant to the provisions of ORS Chapter 222 and Chapter 150 of the Hermiston Code of Ordinances, application is hereby made to annex the following described property:

Name of Applicant: Ibrahim Jaber / Jaber Investment LLC Phone: 509-420-0420

Mailing Address: 2318 S 1st St, Yakima, WA 98903

Contact Person: Christine Batayola Phone: 509-308-0805

Mailing Address: 1632 W Sylvester St, Pasco, WA 99301

Name of Owner (If Different): Jaber Investment LLC / Ibrahim Jaber Phone: 509-420-0420

Mailing Address: 2318 S 1st St, Yakima, WA 98903

Legal Description: Assessor's Map No: 4N2802BA Tax Lot No: 100

Subdivision (If Applicable): _____

Street Address: 2455 NE 7th St, Hermiston, OR

Current Comprehensive Plan Designation: M Proposed Zoning Designation: R-3

Land Area (In Acres): 3.5

Existing Use of Property:

Number of Single-Family Units: 0 Number of Multi-Family Units: 0

Number of Commercial Units: 0 Number of Industrial Units: 0

Public Facilities or Other Uses: _____

Population: Owners: _____ Tenants: _____ Voters: _____

Please Include the Names and Ages of All Residents: _____

No current residencies.

Surrounding Use of Property:

North: Residential

South: Residential

East: Residential

West: Residential

Current Year Taxes: 1,418.67 Previous Year Taxes: 1,459.95

Total Assessed Valuation: 220,100

Please provide a general description of the property including topography, vegetation, drainage basins, flood plain areas, etc.:

The property is characterized by a predominantly level terrain with gentle inclines. Sparse grassland, few large trees. Natural drainage, property not located in a known high-risk flood zone. Water well located within the interior of the lot.

APPLICATION FOR ANNEXATION - PAGE 2

Please explain why the annexation has been proposed:

The annexation of 2455 NE 7th St. is proposed to facilitate the development of apartment units.

Project will provide residential living for community in Hermiston. Annexing the property will ensure access to city services, infrastructure, and utilities, for efficient development.

If the property is undeveloped, please describe plans for future development. This description should describe whether the development will be residential, commercial or industrial and include building types, public facilities, number of units, etc.:

The property is currently undeveloped, with plans for future residential development. Proposed development will consist of 76 apartment units in 6 3-story buildings, 1 4-unit building, and a building with an office and community room. Site improvements include parking, landscaping, storm water facilities and connections to public utilities, including water, sewer, power, and communications.

Does the proposed development conform to the uses allowed under the proposed zoning designation? Yes.

Please provide the following information regarding services and utilities:

Location and size of the nearest water line:

12 feet from North property line, 16" ductile iron water line.

Location and size of the nearest sewer line:

130 feet from East property line, 8" PVC sewer line

Proximity of other facilities (storm drains, gas lines, irrigation lines, etc.):

No storm drain lines within 500 feet.

The time at which services can be reasonably provided by the city or other district:

At time of connection.

The estimated cost of extending such facilities and/or services and the method of financing:

Only utility that will need to be extended is the sewer main. That is estimated to cost \$60k and will be paid with private funds.

Availability of the desired service from any other unit of local government (list service and government):

None.

Please indicate the roads that adjoin the parcel or will provide service to the parcel:

E Punkin Center Rd adjoins the north side and a private access easement, designated as NE 7th St, is on the east side of the property.

APPLICATION FOR ANNEXATION - PAGE 3

Please indicate the condition of the roads and any improvements that are projected:

E Punkin Center Rd is a paved public road in good condition. Development will include widening, curb, gutter, sidewalks, and storm drainage across the frontage. NE 7th St is a private gravel road, the only proposed improvement is a driveway out E Punkin Center Rd.

Please indicate if any new roads will be created or extended through the property:

No new roads will be created or extended through the property.

If the property is presently included within the boundaries of any of the following types of governmental units, please indicate by stating the name or names of the governmental units involved.

Rural Fire District: Umatilla County Fire District #1 Irrigation District: Urban Growth Boundary
 School District: Hermiston School District Drainage District: None
 Library District: None Parks and Recreation District: None
 Special Road District: None

Please indicate which services are presently being received in the territory (For example, are residents receiving municipal water or sewer services?):

No municipal services are being received.


ADDITIONAL INFORMATION TO BE FURNISHED AND ATTACHED TO APPLICATION:

1. Evidence that applicant is owner or purchaser of the property or has written permission of such owner to make an application for the proposed use.
2. Two copies of a site plan (11" x 17") drawn to scale, showing the location of the property concerned, the location of all proposed building(s), highways, streets and alleys.
3. A metes and bounds legal description of the entire property proposed for annexation. A metes and bounds legal description of each individual parcel is not sufficient.

The above statements are true to the best of my beliefs and knowledge. As applicant, I understand that the city council, planning commission and annexation advisory committee request the attendance of me, or my representative, at the meeting(s) where this request is scheduled for consideration and that the city council, planning commission and annexation advisory committee retain the right to approve or deny this request and impose those conditions as may be necessary to lessen probable adverse impacts based upon the testimony provided at the hearing.

I am the ☒ owner/ ☐ owner(s) authorized representative.

(If authorized representative, attach letter signed by owner or owners.)



Signature of Applicant



Date

OUT OF POCKET EXPENSES FOR PUBLICATION COSTS AND RECORDING FEES WILL BE BILLED LATER

For further information, please feel free to contact the planning department at the Hermiston City Hall, 180 N.E. 2nd Street, Hermiston, Oregon 97838, or telephone (541) 667-5025. The City's fax number is (541) 567-5530.

OFFICE USE ONLY

Date Filed: _____

Fee: \$900.00

Received By: _____

Date Paid: _____

Meeting Date: _____

Receipt No: _____



Christopher
M Clemow



RENEWES 31 DECEMBER 2025

TRANSPORTATION IMPACT ANALYSIS

To

City of Hermiston

For

Punkin Center Residential
Development

Prepared

April 7, 2025

C&A Project Number

20241205.00

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I. EXECUTIVE SUMMARY

The following summarizes the analysis and findings contained in this Transportation Impact Analysis (TIA).

1. The subject property is proposed to be annexed into the City of Hermiston and zoned *Medium-High Density Residential* (R-3) consistent with the Hermiston Comprehensive Plan. The proposed specific development includes 76 apartments, which are consistent with the current zoning.
2. The proposed land use actions are consistent with the Hermiston TSP assumptions and do not significantly affect an existing or planned transportation facility. As such, the Transportation Planning Rule (TPR) criteria outlined in Oregon Administrative Rule (OAR) 660-012-0060 are satisfied without additional analysis.
3. The property currently has access to the public roadway system via a 40-foot-wide access easement along the east side of the property, identified as NE 7th Street. However, this is not a public roadway and will not be designated as one. As such, it is assumed that future property/development access will be direct to E Punkin Center Road, further west.
4. The observed crash rates at the study intersections are less than the 1.0 CMEV threshold and the 90th percentile of the reference population. As such, the intersections are considered relatively safe, and no further evaluation of safety deficiencies is necessary.
5. The proposed 76-unit apartment development generates 512 daily, 30 AM peak hour, and 39 PM peak hour trips.
6. All study intersections are anticipated to operate within agency mobility targets, and all approach movements are anticipated to have adequate queue storage in all analysis scenarios. Overall, no mitigation is necessary to provide adequate operations or queue storage for the proposed development.
7. It is further noted that the east- and westbound left-turn lanes at the US 395/Punkin Center Road intersection are nearing storage capacity, and additional striped storage may need to be considered in the future.

II. INTRODUCTION

Property Description and Proposed Land Use Actions

The subject 3.50-acre property is at 2455 NE 7th Street, Hermiston, Oregon, and is described as tax lot 100 on Umatilla County Assessor's Map 4N2802BA. The property currently has access to the public roadway system via a 40-foot-wide access easement along the east side of the property, identified as NE 7th Street. However, this is not a public roadway and will not be designated as one. As such, it is assumed that future property/development access will be direct to E Punkin Center Road, further west. The study area is illustrated in Figure 1 in Appendix A.

The property is proposed to be annexed into the City of Hermiston and zoned *Medium-High Density Residential* (R-3) consistent with the Hermiston Comprehensive Plan. The proposed specific development includes 76 apartments, which are consistent with the current zoning. The Design Layout is attached in Appendix A.

Transportation Analysis Description

The proposed zoning is consistent with the existing Comprehensive Plan map designation, the amendment does not change the Comprehensive Plan, and the proposed zoning is consistent with the Hermiston Transportation System Plan (TSP). As such, the proposed land use actions are consistent with the Hermiston TSP assumptions and do not significantly affect an existing or planned transportation facility. As such, the Transportation Planning Rule (TPR) criteria outlined in Oregon Administrative Rule (OAR) 660-012-0060 are satisfied without additional analysis.

The proposed land use actions do include a specific development application. As such, a transportation impact analysis (TIA) is necessary to address the criteria identified in the following:

- City of Hermiston Municipal Code requirements outlined in Chapters 156.09 and 157.150, and
- Oregon Department of Transportation (ODOT) TIS requirements.

Study Area

Based on the development trip generation and distribution described later in this analysis, as well as correspondence with City of Hermiston staff, the following project area intersections and development access are evaluated and illustrated in Figure 2 in Appendix A.

- US 395 / Punkin Center Road
- E Punkin Center Road / NE 4th Street
- E Punkin Center Road / Site Access
- E Punkin Center Road / NE 8th Street
- E Punkin Center Road / NE 10th Street

Analysis Scenarios

The proposed 76-unit apartment development is anticipated to be constructed in two phases and fully occupied by 2028. As such, analysis scenarios include:

- 2025 Existing Conditions
- 2028 Pre-Development Conditions
- 2028 Post-Development Conditions

III. EXISTING CONDITIONS

Existing Site Conditions

The subject 3.50-acre property is at 2455 NE 7th Street, Hermiston, Oregon. The property currently has access to the public roadway system via a 40-foot-wide access easement along the east side of the property, identified as NE 7th Street. However, this access is not a public roadway, nor will it be designated as one. As such, it is assumed that future property/development access will be direct to E Punkin Center Road, further west.

Roadway Facilities

The following table summarizes existing roadway classifications and characteristics in the study area.

TABLE 1 – EXISTING ROADWAY CHARACTERISTICS						
Roadway	Functional Classification	Lanes	Posted Speed (MPH)	Sidewalks	Bicycle Lanes	On-Street Parking
US 395	Urban Major Arterial – Hermiston Statewide Highway - ODOT	5	45	Partial	No	No
Punkin Center Road	Urban Major Collector	2	45	No	No	No
NE 4 th Street	Urban Minor Arterial	2	35	No	Yes	No
Sagebrush Road	Local	2	Not Posted	No	No	No
NE 5 th Street	Local	2	25	East Side	No	Yes
NE 8 th Street	Local	2	25	Yes	No	Yes
Alpine Drive	Local	2	Not Posted	No	No	No
Bode Lane	Local	2	Not Posted	No	No	No
NE 10 th Street	Urban Major Collector	2	45	No	No	No

Safety Analysis

When evaluating roadway and intersection safety, consideration is given to the number and types of crashes occurring and the number of vehicles traveling on a roadway segment or entering the intersection. This leads to the concept known as the “crash rate.” Specific to intersections, it is typically expressed in terms of the number of crashes occurring per one million vehicles entering the intersection (CMEV). A critical crash rate analysis is then performed by comparing the subject intersection to the published statewide 90th percentile intersection crash rates at comparable/reference intersections. Crash rates close to or exceeding 1.0 CMEV or the 90th percentile rates require further analysis.

Crash data for the study area intersections were obtained from the Oregon Department of Transportation (ODOT) for five years from January 1, 2019 through December 31, 2023. The following table presents the study intersection crash rates and critical crash analysis. All crash data and crash rate calculations are provided in Appendix B.

TABLE 2 – INTERSECTION CRASH RATES										
Intersection	2019	2020	2021	2022	2023	Total	Crash Rate (crashes/mev)	Reference Population		Over or Under Crash Rate?
								Description ¹	90 th ile Crash Rate	
US 395 / Punkin Center Road	3	2	4	6	3	18	0.457	Urban 4SG	0.860	Under
Punkin Center Road / NE 4 th Street	1	0	1	2	2	6	0.519	Rural 4ST	1.080	Under
Punkin Center Road / NE 8 th Street	1	0	0	0	0	1	0.122	Rural 3ST	0.475	Under
Punkin Center Road / NE 10 th Street	0	0	0	0	0	0	0.000	Rural 3ST	0.475	Under

¹ 3ST is defined as a three-leg minor stop-control intersection, 4ST is a four-leg minor stop-control intersection, and 4SG is a four-leg signalized intersection.

The observed crash rates at the study intersections are less than the 1.0 CMEV threshold and the 90th percentile of the reference population. As such, the intersections are considered relatively safe, and no further evaluation of safety deficiencies is necessary.

Existing Traffic Counts

Existing traffic counts were obtained on February 27, 2025, during the AM and PM peak hours. Existing (base) traffic counts are included in Appendix C and are illustrated in Figures 2 and 3 in Appendix A.

Seasonal Adjustment and 30th Highest Hour Volumes

Seasonal adjustments account for the variation in traffic volumes during the year. As required for intersections under ODOT jurisdiction, the February 2025 traffic counts were adjusted to the 30th highest hour volume (30HV) consistent with procedures identified in the ODOT Analysis Procedures Manual (APM) Version 2, Chapter 5.5.1 – On-Site Automatic Traffic Recorder (ATR) Method. This method is used when an ATR is within or near the project area.

In the study area, ATR 30-019 – Stanfield is located on US395; Umatilla-Stanfield Highway NO. 54; 0.12 miles NW of Feedville Road (NW Stanfield). Using this ATR data, a seasonal adjustment of 1.06 was applied to the February 27, 2025 Base traffic counts to obtain 2025 30HV volumes. Seasonal adjustment assumptions are included in Appendix C, and 2025 30HV volumes are illustrated in Figures 2 and 3 in Appendix A.

IV. DEVELOPMENT TRIP GENERATION

The subject property is currently undeveloped. The proposed specific development includes 76 apartments.

Trip generation for the proposed development is estimated using the Institute of Transportation Engineers (ITE) *Trip Generation Manual*, 11th Edition, and practices from the ITE *Trip Generation Handbook*, 3rd Edition, and is presented in the following table.

TABLE 3 – DEVELOPMENT TRIP GENERATION									
Land Use	ITE Code	Size	Daily Trips	AM Peak Hour			PM Peak Hour		
				Enter	Exit	Total	Enter	Exit	Total
Multifamily Housing (Low-Rise) ¹	220	76 DU	512	7	23	30	24	15	39

¹ Trip generation is estimated using the *Average Rate* per recommended practice in the ITE *Trip Generation Handbook*, 3rd Edition.

As identified in the table above, the proposed 76-unit apartment development generates 512 daily, 30 AM peak hour, and 39 PM peak hour trips.

V. DEVELOPMENT YEAR CONDITIONS

Background Growth

Based on a review of the Hermiston Transportation System Plan (TSP) and discussions with City staff, background traffic growth for the study area is assumed to be 1.5% per year on all roadways.

Development Year Traffic Volumes

The proposed 76-unit apartment development is anticipated to be constructed in two phases and fully occupied by 2028.

Accordingly, the 2025 30HV volumes were adjusted to the 2028 development year using a 1.5% annual compounded growth rate. Detailed background growth rate calculations are included in Appendix C and are illustrated in Figures 4 and 5 in Appendix A.

Trip Distribution and Traffic Assignment

Development trip generation, as identified in the previous section of this TIA, was distributed onto the roadway system based on existing intersection volumes, surrounding land uses, and engineering judgment. The resulting trip distribution and development traffic assignment are illustrated in Figures 4 and 5 in Appendix A.

VI. INTERSECTION ANALYSIS

Analysis Scope

The following project area intersections and development accesses are evaluated:

- US 395 / Punkin Center Road
- E Punkin Center Road / NE 4th Street
- E Punkin Center Road / Site Access
- E Punkin Center Road / NE 8th Street
- E Punkin Center Road / NE 10th Street

Analysis Description

Existing intersection AM and PM peak hour factors (PHFs) are used for all intersections in all analysis scenarios.

Intersection operation characteristics are generally defined by two key metrics: the volume-to-capacity (V/C) ratio and the level-of-service (LOS). At signalized intersections, the v/c ratio is a measurement of an intersection's ability to accommodate critical movements, while LOS is based on the average control delay per vehicle for the entire intersection. At unsignalized intersections, the v/c ratio and LOS are calculated for intersection approach movements yielding right-of-way.

The City of Hermiston mobility standard for signalized and unsignalized intersections is LOS D or better.

Table 6 of Policy 1F in the *Oregon Highway Plan* (OHP), as updated through January 2023, provides ODOT mobility targets for state roadways. In the study area, US 395 is classified as a *Freight Route on a Statewide Highway*, inside the urban growth boundary of a non-metropolitan planning organization (MPO).

At the Punkin Center Road intersection, US 395 has a posted speed of 45 MPH, and the intersection mobility target is a v/c ratio ≤ 0.80 .

Intersection Operations Analysis

Unsignalized intersection operations analyses were performed using the Transportation Research Board's *Highway Capacity Manual* 6th Edition methodologies using Trafficware's *Synchro* software (Version 11). Signalized intersection operations analyses were performed per the Transportation Research Board's *Highway Capacity Manual* 6th Edition, 2000, and 2010 methodologies using Trafficware's *Synchro* software (Version 11) and practices outlined in the ODOT Analysis Procedures Manual V2 necessary to calculate the intersection v/c ratio.

The proposed 76-unit apartment development is anticipated to be constructed in two phases and fully occupied by 2028. As such, analysis scenarios include:

- 2025 Existing Conditions
- 2028 Pre-Development Conditions
- 2028 Post-Development Conditions

The following table summarizes the analysis results of weekday AM and PM peak hour operations. Data output sheets from all operations calculations are attached in Appendix D.

TABLE 4 – INTERSECTION OPERATIONS ANALYSIS								
Intersection	Critical Movement Lane Group	Mobility Target	AM Peak Hour			PM Peak Hour		
			2025 Existing	2028 Pre-Development	2028 Post-Development	2025 Existing	2028 Pre-Development	2028 Post-Development
US 395 / Punkin Center Road	Intersection	v/c ≤ 0.80	0.31	0.35	0.35	0.53	0.59	0.60
E Punkin Center Road / NE 4 th Street / Sagebrush Road	NB L NB T/R SB L/T/R	LOS D	B B B	B B B	B B B	C B B	C B C	C B C
E Punkin Center Road / Site Access	NB L/R	LOS D	—	—	B	—	—	B
E Punkin Center Road / NE 8 th Street	NB L/R	LOS D	B	B	B	B	B	B
E Punkin Center Road / NE 10 th Street	NB L/R	LOS D	B	B	B	B	B	B

Operations Analysis Discussion

As identified in the table above, all study intersections are anticipated to operate within agency mobility targets in all analysis scenarios. Overall, no mitigation is necessary to provide adequate operations for the proposed development.

Intersection Queuing Analysis

Queuing analysis was performed to evaluate queue storage adequacy. 95th percentile queues were estimated using Trafficware's *SimTraffic* software (Version 11) and ODOT *Analysis Procedure Manual* methodologies. Available storage is rounded to the nearest five feet, and queue demand is rounded to the nearest 25 feet, which corresponds to the average length of a queued vehicle.

The following table summarizes the analysis results of weekday AM and PM peak hour queuing. Data output sheets from all queuing calculations are contained in Appendix D.

TABLE 5 – INTERSECTION QUEUING ANALYSIS								
Intersection	Critical Movement Lane Group	Queue Storage Available (Feet) ¹	AM Peak Hour			PM Peak Hour		
			2025 Existing	2028 Pre-Development	2028 Post-Development	2025 Existing	2028 Pre-Development	2028 Post-Development
US 395 / Punkin Center Road	NB L	225+	50	50	50	50	50	50
	NB T/R	1,000+	100	100	100	175	200	200
	SB L	250+	50	50	50	125	150	150
	SB T/R	1,000+	100	100	100	125	150	150
	EB L	85	50	75	75	50	75	75
	EB T/R	1,000	50	50	50	100	75	100
	WB L	90	75	75	75	75	75	75
	WB T/R	1,000	100	100	100	100	125	150
E Punkin Center Road / NE 4 th Street / Sagebrush Road	NB L	75	50	50	50	50	75	75
	NB T/R	1,000+	50	50	50	75	50	50
	SB L/T/R	1,000+	50	50	50	50	50	50
	EB L/T/R	1,000	25	25	25	25	25	25
E Punkin Center Road / Site Access	WB L/T/R	500	25	25	25	25	50	25
	NB L/R	75	—	—	50	—	—	50
E Punkin Center Road / NE 8 th Street	WB L/T	350	—	—	25	—	—	25
	NB L/R	1,000+	50	50	50	50	50	50
E Punkin Center Road / NE 10 th Street	WB L/T0	750	25	25	25	25	25	25
	NB L/R	1,000+	75	75	75	75	75	75
	WB L/T	900	25	50	25	25	25	25

¹ Available queue storage is measured to the nearest upstream intersection for continuous lanes between intersections and to the end of full-width storage for turn lanes.

Queuing Analysis Discussion

As identified in the table above, all study intersection approach movements are anticipated to have adequate queue storage in all analysis scenarios to accommodate the 95th percentile vehicle queues. It is further noted that the east- and westbound left-turn lanes at the US 395/Punkin Center Road intersection are nearing storage capacity, and additional striped storage may need to be considered in the future. Overall, no mitigation is necessary to provide adequate queue storage for the proposed development.

VII. CONCLUSION

The following summary and recommendations are based on materials contained in this analysis.

1. The subject 3.50-acre property is at 2455 NE 7th Street, Hermiston, Oregon, and is described as tax lot 100 on Umatilla County Assessor's Map 4N2802BA. The property currently has access to the public roadway system via a 40-foot-wide access easement along the east side of the property, identified as NE 7th Street.
2. The property is proposed to be annexed into the City of Hermiston and zoned *Medium-High Density Residential* (R-3) consistent with the Hermiston Comprehensive Plan. The proposed specific development includes 76 apartments, which are consistent with the current zoning.
3. The proposed land use actions are consistent with the Hermiston TSP assumptions and do not significantly affect an existing or planned transportation facility. As such, the Transportation Planning Rule (TPR) criteria outlined in Oregon Administrative Rule (OAR) 660-012-0060 are satisfied without additional analysis.
4. The 40-foot-wide access easement along the east side of the property, identified as NE 7th Street, is not a public roadway and will not be designated as one. As such, it is assumed that future property/development access will be direct to E Punkin Center Road, further west.
5. The observed crash rates at the study intersections are less than the 1.0 CMEV threshold and the 90th percentile of the reference population. As such, the intersections are considered relatively safe, and no further evaluation of safety deficiencies is necessary.
6. The proposed 76-unit apartment development generates 512 daily, 30 AM peak hour, and 39 PM peak hour trips.
7. All study intersections are anticipated to operate within agency mobility targets in all analysis scenarios. Overall, no mitigation is necessary to provide adequate operations for the proposed development.
8. All study intersection approach movements are anticipated to have adequate queue storage in all analysis scenarios to accommodate the 95th percentile vehicle queues. It is further noted that the east- and westbound left-turn lanes at the US 395/Punkin Center Road intersection are nearing storage capacity, and additional striped storage may need to be considered in the future. Overall, no mitigation is necessary to provide adequate queue storage for the proposed development.

HERMISTON IRRIGATION DISTRICT



366 East Hurlburt Avenue
Hermiston, OR 97838-2445
Office: 541-567-3024
Fax: 541-564-1069
Mobile: 541-571-7698
E-mail: Manager@hermistonid.org

June 23, 2025

City of Hermiston
Clint Spencer, City Planner
180 NE 2nd St
Hermiston OR 97838

**Re: Jaber Investment, LLC.
Annexation of Property – 4N2802BA 100**

Mr. Spencer,

I have reviewed the information regarding the request for annexation submitted by Jaber Investment, LLC for the map and tax lot listed above. My research has shown that this property is within the District boundary, however, there are no water rights on this property, nor is there any Federal easement or District facilities.

HID has no objection to the annexation. Thank you for the opportunity to review and comment on this application.

Respectfully,

Karra

Karra Van Fossen
Water Right Specialist

July 9, 2025

Planning Commission
City of Hermiston
180 NE 2nd St
Hermiston, OR 97838

RE: Annexation 4N2702BA Tax Lot 100 Jaber Investment, LLC

Commissioners,

This is our response to the notice of land use action we recently received regarding the property south of E Punkin Center Rd to the west of NE 7th Street. **We strongly oppose the proposed annexation.**

It is our understanding that this annexation would bring currently zoned R-3 land within the UGB into the city limits, allowing city infrastructure to be built that would in turn enable medium-high density housing to be built on this lot. The lot in question is currently a bare, overgrown field, surrounded by:

- North: RR-2 farmland, in the form of our 40 acre family farm. We have owned this land for 95 years, and the driveway to and front lawn of our 1931 farmhouse are directly opposite the proposed accessway on the tentative site plan.
- West/South: County land zoned R-3 but currently built in a manner characteristic of RR-2 - single family homes on 2+ acre plots. Some of this land is currently used for grazing a few head of cattle.
- East: City land, zoned R-4 but partially built out as R-1 style single family homes on decently sized (7500 sqf / 0.17 ac) lots. The remainder of the unbuilt land is currently the subject of another planning decision to change the platting to allow a significantly higher density, but still lower than that planned for the parcel under consideration here.

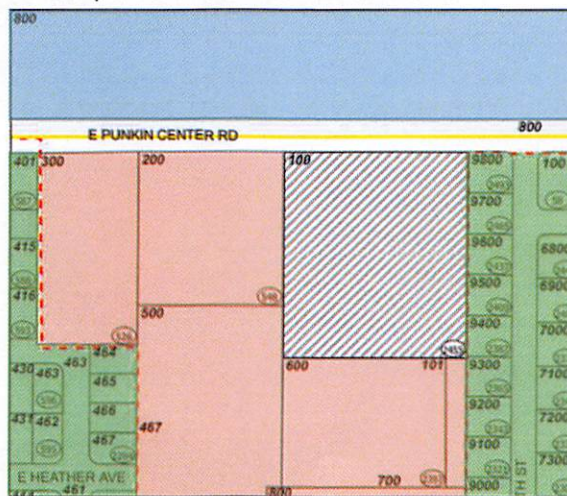


Figure A: Actual land use at present. **Blue:** rural residential/farming in RR-2 zone. **Red:** rural residential in R-3 zone. **Green:** Single family homes in R-3/R-4 zones.

We feel that the proposed use of this plot after annexation is contrary to the character of the surrounding neighborhood - it doesn't fit in with the revealed preferences of landowners in the area for single family housing, and it is far denser than anything short of Theater Lane or even further south. It creates an "island" of urban density at the edge of a sea of suburban/rural tranquility. Many who live in

this area would not have moved in had there been that kind of density nearby; our family has been here long enough that the choice was made by prior generations, but we have always enjoyed the quiet, spacious rural life (and expansive, uncluttered views) of this area and we do not want to see that change.

The Hermiston Comprehensive Plan's Policy 6 discusses conversion of urbanizable to urban land, which this annexation would seem to fall under¹:

If the property being considered for conversion contains less than 10 acres, the city will add sufficient additional surrounding property to equal at least 10 acres. The 10 acre area to designated should consider major natural or manmade features in setting boundaries. This will assure that the planning of newly converted areas will not occur in a piecemeal fashion. In designating residential uses, the city will strive to maintain the following acreage balance: 71% low density residential (R1 and R2), 16% medium density residential (R3), and 13% medium density residential/mobile home (R4).

The property within city limits surrounding the proposed annexation, going far beyond 10 acres, is on the order of 75% R-3 and R-4, though even within that area there is nothing built or proposed that approaches the density in this plot's tentative site plan. Enabling the construction of the highest density permissible by right in R-3 on this plot makes a situation that is already failing the acreage balance specified in Policy 6 worse, which is itself a clear (and potentially reversible²) error in application of this Policy.

Another significant concern is the ability of infrastructure in the area to support medium-high density residential use. E Punkin Center Rd is signed for 45mph, but as a major artery of US 395 is frequently driven at speeds 10mph over that or worse. It is already congested - just today we observed a backup of cars waiting to turn left onto US 395 that was at least 100% past the allotted turn lane, at 6:30PM on a Tuesday. The traffic analysis submitted to the commission suggests that 512 trips per day will be added by the planned use, and that this will have no substantial impact. It even explicitly states that in the worst likely case, with the new development and other growth by 2028, that exact left turn lane will rarely if ever overflow. Based on what we currently see on this road daily, we believe this traffic analysis is highly optimistic.

Worse, this is not the only proposed addition of density on this small section of Punkin Center. Only 500 feet east, there is another review in process to build medium density row housing on the remainder of the R-4 parcels zoned past 8th street, which would allow for 210 more units of housing to be built, all of which would use Punkin Center as its primary entry and exit. Oregon DOT submitted testimony to the commission regarding that application and requested a traffic generation report, which was done and which shows 1,573 additional trips per day, meaning that we now have proposals to add over 2000 trips per day³ to a road which is arguably inadequate for the usage it currently sees. Moreover, the north half of Punkin Center is in county land and under Umatilla County jurisdiction, meaning any improvements the City compels developers to build will only apply to half of the roadway (while the increased traffic uses both halves and the north half continues to deteriorate absent any action from County).

Certainly these two traffic analyses should not be considered in isolation from each other, and potentially a joint full analysis should be done that accounts for the total density to be added by both projects within substantially the same small area.

¹ Hermiston Comprehensive Plan Policies, page 9:

https://www.hermiston.gov/sites/default/files/fileattachments/community_development/page/2781/comp_plan_policies_sept_2024.pdf

² Kine v. City of Bend, 72 Or LUBA 423 (2015). <https://www.oregon.gov/luba/Docs/Opinions/2015/12-15/15068.pdf>

³ Realistically, this constitutes >2 additional cars per minute on Punkin Center during the hours most people are awake. Anecdotally, this could easily double the road's traffic at most hours of the day.

The same concerns can be raised regarding the city water pressure issues that residents on 8th have discussed with the commission. Adding another 76 + 210 units of housing in this area, connected to city water, seems likely to exacerbate these issues.

It is worth noting here that there is no need for additional density in this area - many housing units nearby are vacant and more are being built already. This is the outer edge of the City; density matching the highest existing anywhere in town should be built centrally, not on the rural outskirts of our agricultural town. Policy 23 of the Comprehensive Plan⁴ states:

THE CITY OF HERMISTON WILL PLAN FOR THE TIMELY AND EFFICIENT PROVISION OF A FULL COMPLEMENT OF URBAN SERVICES AND FACILITIES IN ALL DEVELOPED AND DEVELOPING AREAS WITHIN THE COMMUNITY. TIMELY MEANS A POINT WITHIN THE 20-YEAR TIMEFRAME WHEN THE CITY DEEMS DEVELOPMENT APPROPRIATE FOR A GIVEN PROPERTY BASED ON FACTORS INCLUDING BUT NOT LIMITED TO THE NEED FOR ADDITIONAL URBAN DEVELOPMENT WITHIN THE URBAN GROWTH BOUNDARY AND THE EXTENT OF UNDEVELOPED OR UNDERDEVELOPED LAND BETWEEN THE EXISTING DEVELOPMENT AND THE SUBJECT PROPERTY.

To a reasonable observer, this proposed development, at this time, does not meet the definition of "timely" given here. In the absence of need and the presence of much lower density land surrounding this plot, allowing this plan to proceed would be an error.

Ultimately, we do not see a set of requirements that the City could impose which alleviate our concerns with this land usage. It is our impression that if the annexation is approved, development is allowable by right under the R-3 zoning without public input or comment. Thus, while proposed use is not a criteria for annexation approval, the annexation represents our only opportunity, as nearby residents with standing, to express our concerns to the city government. The proposed use, without which the annexation serves no purpose, would destroy the rural character of the area, deeply affect our quality of life, tax the existing infrastructure without adequate remedy, and create an incongruous and undesirable-to-all arrangement of housing densities. **We urge the commission to deny this annexation, and would support a reconsideration of the zoning of outside-of-City urbanizable UGB land in this entire area to conform with the land use balance specified in Policy 6 of the Comprehensive Plan.**

Sincerely,



TJ Knight
Brenda Knight
Peter K Cawley

535 E Punkin Center Rd
Hermiston, OR
97838

⁴ Hermiston Comprehensive Plan Policies, Page 34 (see footnote 1).

Ami Little
2409 NE 8th St
Hermiston, OR 97838

7/9/2025

Hermiston Planning Commission
180 NE 2nd Street
Hermiston, OR 97838

RE: Public Hearing Comment – Proposed Annexation & Multi-Family Development at 2455 NE 7th St (Tax Lot 100)

Dear Planning Commission Members,

I am writing as a homeowner whose property directly borders the proposed annexation site at 2455 NE 7th Street. I recently received notice of the proposal to annex this property into the City of Hermiston, with the intent of designating it as Medium High Density Residential (R-3) and developing a multi-family apartment complex. I would like to formally submit my concerns.

While I understand the importance of housing development, I have serious concerns regarding the safety and livability impacts this project may have on our neighborhood.

1. Safety and Traffic Concerns – High-Crash Corridor, New School Zone, and Incomplete Road Infrastructure

The proposed development would add approximately seventy new apartment units, directly increasing daily traffic along East Punkin Center Road and through the intersection at Highway 395 and Punkin Center, one of the most dangerous intersections in Hermiston.

- According to the City of Hermiston's own Safety Action Plan, this intersection ranks as one of the highest in crash severity across the city.
- Multiple fatal crashes have occurred at this location.
- The Oregon Department of Transportation (ODOT) has recognized this danger and has approved plans to install a roundabout, but construction is not expected until 2028.
- In the meantime, traffic from the proposed apartments, including residents, guests, and delivery drivers, will be funneled through this high-risk intersection daily.

Even more concerning is the fact that a new elementary school is now located at East Punkin Center and NE 10th Street, just blocks from the proposed development. This school brings increased pedestrian activity, school bus traffic, and family vehicles into a zone that is already struggling with safety.

It should also be noted that East Punkin Center Road is not fully developed for safe pedestrian use:

- There are no sidewalks, bike lanes, or paved shoulders.
- The posted speed limit is 45 mph, which is unusually high for a residential and school-adjacent streets.

These conditions raise real concerns about whether the neighborhood can safely handle the increased traffic, school zone activity, and pedestrian use that would come with this development. Without thoughtful planning and improvements, the risk to children, families, and other residents could grow significantly. I would request a formal Traffic Impact and Safety Study that includes crash history, projected traffic volume, and pedestrian use before any decision is made.

2. Loss of Privacy

As a direct neighbor to the property, I am concerned about how the height and layout of the apartment buildings may affect privacy. Multi-story buildings with windows or balconies facing residential backyards will significantly diminish the sense of privacy we currently enjoy.

3. Noise and Increased Activity

Apartment complexes naturally generate more foot traffic, noise, and vehicle movement than single-family neighborhoods. This would mark a major shift for those of us living on quiet residential streets and could affect both quality of life and safety.

4. Impact on Property Values

The close proximity of a high-density development may negatively affect the resale value of nearby single-family homes, especially if the development is not designed with thoughtful integration into the surrounding neighborhood.

I also want to highlight concerns specific to my own street. I live on NE 8th Street, which has increasingly become a cut-through route for drivers trying to avoid delays on Punkin Center Road. We already see vehicles speeding through our residential area, and with the addition of seventy new apartments, that problem is likely to worsen. This creates real safety concerns, especially for families with children or pets. I respectfully request that NE 8th Street be included in any traffic impact study and that traffic-calming options be seriously considered.

I want to be clear: **I do not support rezoning this property for multi-family use.** While I understand the city's housing needs, I believe this location is simply not suited for high-density development. It directly borders single-family homes, contributes traffic to a high-crash intersection, and sits within an area that lacks the infrastructure to safely absorb this scale of growth.

I respectfully urge the Planning Commission to deny the rezoning request and preserve the land's current designation. Any future development should reflect the character, safety, and capacity of the surrounding neighborhood.

Thank you for considering the voices of nearby residents. I truly appreciate the work you do for our city.

Sincerely,

A handwritten signature in black ink, appearing to read 'Ami Little', with a stylized flourish at the end.

Ami Little



HERMISTON SAFETY ACTION PLAN

Appendix



Intersections

Intersection crashes were matched to the nearest intersection within 250 feet of the crash. The annual crash severity score was calculated for each intersection. The number of FSI crashes and other injury crashes that occurred at the intersections with the highest number of crashes are summarized in Table 4. Intersections are ordered by the annual crash severity score. ODOT's Social Equity Index is recorded for each intersection in Table 4. The Social Equity Index aggregates demographic data and indicates the level of disadvantage in each census block group, a 'High' Social Equity Index indicates greater disadvantage. The Social Equity Index is described in greater detail in the Equity Assessment section of the memo.

Intersections where there was at least one crash resulting in an injury between 2018 and 2022 are shown in Figure 14 by the annual crash severity score. There were more injury crashes between 2018 and 2022 at intersections with a higher annual crash severity score. The 11 intersections with the highest number of crashes (shown in Table 4) are shown in Figure 14 as a red circle.

Table 4: Intersections with Highest Crash Severity Scores

Rank	Intersection	Traffic Control	Jurisdiction	Social Equity Index	Annual Crash Severity Score	FSI Crashes	Other Injury Crashes
1	US 395 & E Punkin Center Rd	Signal	ODOT	High	64	2	12
2	US 395 & E Gladys Ave	Signal	ODOT	Med./High	58	2	9
3	US 395 & E Elm Ave	Signal	ODOT	Med./High	56	1	18
4	US 395 & E Main St	Signal	ODOT	Med./High	40	1	10
5	OR 207 (W Elm Ave) & N 1st Pl	Signal	ODOT	Med./High	40	1	10
6	US 395 & E Theater Ln	Signal	ODOT	Med./High	32	1	6
7	OR 207 (11th St) & W Orchard Ave	Signal	ODOT	High	30	1	5
8	US 395 & Kelli Blvd	Stop	ODOT	Med./High	28	1	4
9	N 1st Pl & W Harper Rd	Stop	City	High	28	1	4
10	SW 17th St & W Highland Ave	Stop	City	High	28	1	4
11	US 395 & W Harper Rd	Stop	ODOT	Med./High	26	1	3

KEY FINDINGS

Based on the crash conditions analysis, the location specific analysis, and the systemic safety analysis, the following key findings have been identified, and are depicted geographically in Figure 21. The Emphasis Areas for the Hermiston SAP are defined based upon these key findings.

- **Crash Characteristics:** Crashes with several characteristics tend to be more common or more severe in Hermiston.
 - **Crashes at Intersections** occur more often than crashes along segments. 60% of injury crashes in Hermiston between 2018 and 2022 occurred at intersections.
 - **Turning Movement and Rear End Crashes** make up most of the crashes in Hermiston. Turning movement related crashes (including angle crashes) and rear end crashes account for 79% of injury crashes in Hermiston between 2018 and 2022.
 - **Seatbelt Usage** in Hermiston is a notable characteristic of crashes resulting in deaths and serious injuries. 21% of crashes resulting in a death or serious injury in Hermiston between 2018 and 2022 involved an unrestrained occupant.
 - **Crashes Involving Pedestrians and Bicyclists** tend to result in more serious injuries and deaths than crashes involving only vehicles. Considering all injury crashes:
 - 20% of crashes involving a pedestrian resulted in a fatality or serious injury.
 - 27% of crashes involving a bicyclist resulted in a fatality or serious injury.
 - **An Impaired Person** was involved in 23% of the crashes that resulted in a fatality or serious injury.
- **Crash Locations:** Several roadways and intersections have historically had a greater number of crashes resulting in an injury or fatality. These roadways and intersections are listed below and shown in Figure 21 using orange lines (for segments) and orange circles (for intersections).
 - US 395, north of Hermiston Avenue to the urban growth boundary, including intersections at:
 - US 395 & Punkin Center Road
 - US 395 & Hermiston Avenue/Gladys Avenue
 - US 395 & OR 207 (Elm Avenue)
 - OR 207 (11th Street), between Joseph Avenue and Elm Avenue
 - Orchard Avenue, between OR 207 (11th Street) and US 395
 - W Highland Avenue, west of OR 207 (11th Street) to the urban growth boundary
- **Roadway Characteristics:** Following methodologies from Oregon Department of Transportation (ODOT) certain characteristics of roadways and intersections are correlated with more intersection, pedestrian, or bicyclist crashes. Intersections and segments with characteristics correlated with the greatest risk for intersection, pedestrian, or bicyclist crashes are shown in Figure 21 using teal lines.

July 9th, 2025

Jeff Stroben & Brandi Sinner

544 E Punkin Center Rd

Hermiston OR. 97838

509-948-6736

Brandi and I strongly object to the Annexation 4N2802BA Tax Lot 100 Jaber Investment, LLC
2455 NE 7th St.

We always knew that this property would be developed some day and expected it to be similar to the homes recently built on 8th & 5th streets. Single family dwellings.

We were unaware of the M or R3 zoning designation. Had we known this we would have lobbied to have it changed to R2 or R1 for future development.

This annexation will severely diminish our quality of life and the rural nature of our property and the surrounding properties. The proposed development of a 3 story apartment complex will have a detrimental impact on our property and way of life.

The increased noise, light & air pollution from vehicles so close and densely packed only feet from our property will negatively impact our quality of life and take away our privacy that we enjoy now. Would you want everyone in the 2nd and 3rd story to be able to watch everything you now do in your yard and observe all you have in your buildings and property?

I have a great concern with the storm water that will now run off the entire property and is to be directed to swales and catch basins that butt up against my property. I believe this threatens the safety of my well which is only 99 ft away. Should these overflow onto my property there is a great chance it could contaminate my well with chemicals, gas, and oil from the asphalt and cement of the complex.

Traffic safety is of great concern and after reading the Transportation Impact Analysis I believe some of the data is outdated and does not take into account the recent added traffic from 100 new homes on 5th and new homes built to the East of 7th. Example – Traffic safety study using data from 1-1-2019 to 12-31-2023.

Punkin Center has seen a substantial increase in traffic and noise in the last 3 years and with the addition of 512 projected daily trips from this development there will be even more of a safety hazard on an already dangerous road.

“The proposed 76-unit apartment development generates 512 daily, 30 AM peak hour, and 39 PM peak hour trips.”

We are not in opposition to the development of this property, but are opposed to the annexation with the intention of building the proposed 3 story apartment complex. This is not in character with the rest of this rural community and reduces our privacy, increases traffic unreasonably without providing for adequate road safety improvements, increases noise air and water pollution.

Jeff Stroben & Brandi Sinner

PROCLAMATION

IT IS HEREBY PROCLAIMED that at the regular meeting of August 11, 2025, the City Council of the City of Hermiston, Umatilla County, Oregon, did by unanimous vote annex the following described property, to-wit:

THAT PORTION OF THE EAST HALF OF NORTHEAST QUARTER OF NORTHWEST QUARTER OF SECTION 2, TOWNSHIP 4 NORTH, RANGE 28 EAST, WM.; EXCEPTING THEREFROM THE WEST 292 FEET THEREOF; ALSO EXCEPTING THEREFROM THE SOUTH 1069 FEET THEREOF; MORE PARTICULARLY DESCRIBED AS FOLLOWS:

COMMENCING AT THE NORTHWEST CORNER OF SAID SECTION 02 THENCE NORTH 89°32'51" EAST ALONG THE NORTHERLY LINE OF SAID SECTION 2284.40 FEET TO THE TRUE POINT OF BEGINNING;

THENCE CONTINUING NORTH 89°32'51" EAST ALONG SAID LINE 370.45 FEET; THENCE SOUTH 00°17'51" EAST ALONG THE EASTERLY LINE THEREOF 444.10 FEET; THENCE SOUTH 89°29'12" WEST ALONG THE SOUTHERLY LINE THEREOF 370.45 FEET; THENCE NORTH 00°17'51" WEST ALONG THE WESTERLY LINE THEREOF 444.50 FEET TO THE SAID TRUE POINT OF BEGINNING

CONTAINS 3.78 ACRES

All being East of the Willamette Meridian, Umatilla County, Oregon;

IT IS FURTHER PROCLAIMED that copies of this Proclamation be posted in four places in the City of Hermiston for two weeks.

DATED AT HERMISTON, OREGON, this 11th day of August 2025.

Doug Primmer, MAYOR

ATTEST:

Heather La Beau, ASSISTANT CITY RECORDER



Where Life is Sweet

Mayor and Members of the City Council
STAFF REPORT
 For the Meeting of August 11, 2025

Title/Subject

Resolution 2383- Initiate LID for W Dogwood

Summary and Background

West Dogwood is a gravel unimproved street between NW 2nd and NW which provides direct access to an active industrial business, and one industrial lot which regularly gets used for construction laydown activities, these are known as 4N2810AA Tax Lots 1000 and 3300.

The Oregon Department of Transportation is scheduled to erect traffic barricades on W Elm Avenue to better manage the efficient and safe flow of traffic through the Elm/395 intersection. These barricades will eliminate the ability for left-hand turning movements in to and out of NW Spruce. Therefore, the only remaining access on Elm Ave to the active businesses on NW 2nd for vehicles coming from the Northeast will be on NW 2nd. Similarly, the only Elm Avenue access for vehicles coming from these businesses looking to go West will be on NW 2nd. This will significantly increase the amount of traffic on this sub-standard street.

W Dogwood has been an unimproved gravel roadway due to two main factors:

- Non-Remonstrance Agreements (NRA)
 - The City of Hermiston's standard requires that when a tax lot develops (structures are built on it), then the roadway in front of it must be built.
 - Developers can avoid this obligation through signing an NRA. This generally states that they will not have to develop the street in order to build on their property, but if/when a Local Improvement District is proposed, then they waive their ability to formally oppose it's formation.
 - One of the two tax lots involved in this section of Dogwood has an executed NRA
- Undeveloped
 - Undeveloped lots within the City are generally only required to pay to improve their street frontage at the time that they develop.
 - One of the two tax lots involved in this section of Dogwood has not formally developed

Due to the impending increase in traffic on Dogwood triggered by the shift in traffic pattern accessing these businesses, it is recommended that the roadway be constructed to city standards.

Formation of a Local Improvement District (LID) is a way to finance public streets by levying the cost of the roadway on to the abutting properties which benefit from the street in amounts proportionate to their frontage.

The City of Hermiston has successfully executed 322 LID's, primarily to develop roadways.

There are several steps required by City Code, and State Law, in order to form an LID. The first step is that the City Council must direct the City Manager to have a report developed which actually scopes out the project, estimates costs, establishes methodology for assigning costs, etc. That is what Resolution 2383 does.

Tie-In to Council Goals

Transportation: Improve Mobility and Transportation

Fiscal Information

This resolution does not actually form an LID, or create any obligation whatsoever on to property owners. This resolution simply directs the City Manager to develop a report which can be used by the Council in determining whether or not to move forward to the next steps.

Alternatives and Recommendation

Alternatives

1. Approve resolution 2383
2. Reject resolution 2383

Recommended Action/Motion

Motion to approve Resolution 2383

Submitted By:

Mark Morgan

RESOLUTION NO. 2383

A RESOLUTION INITIATING THE FORMATION OF A LOCAL IMPROVEMENT DISTRICT IN THE VICINITY OF W DOGWOOD BETWEEN TAX LOT 1000 AND TAX LOT 3300 AND DIRECTING THE CITY MANAGER TO PREPARE A FEASIBILITY REPORT FOR THE IMPROVEMENTS IN ACCORDANCE WITH THE ORDINANCES OF THE CITY OF HERMISTON.

WHEREAS, approximately 150 linear feet of City Street right of way for W Dogwood Avenue exists in an underdeveloped nature consisting of a graveled surface between Tax Lot 1000 and Tax Lot 3300.

NOW, THEREFORE, THE CITY OF HERMISTON RESOLVES AS FOLLOWS:

1. That it is expedient and necessary to improve, and it is hereby proposed to curb, gutter, sidewalk and pave with asphalt, in accordance with standard city specifications, the following parts of streets hereinafter named and at the stated width:

Between Tax Lot 1000 and Tax Lot 3300, a distance of 150 linear feet of W Dogwood Avenue between NW Spruce Street and NW 1st Street.

2. That the city manager shall make, and is hereby instructed to prepare a report including a description of the local improvement project for the above described improvement and its boundaries based on preliminary project plans and specifications; a preliminary determination of the feasibility of making the proposed improvement, including an estimate of the actual cost of the proposed local improvement; a map of the proposed local improvement district with the address and the map and tax lot number or other sufficient description and ownership of each specially benefited property with a brief explanation of why the properties benefit; the proposed methodology for allocating the improvement project costs among and between the specially benefited properties, together with a description of other funds, if any, proposed to be used; the assessed valuation of each property and an estimate of the assessment amount for each lot or portion thereof, with a statement of the amount of outstanding assessments against any lot proposed to be assessed by the improvement.
3. That this resolution is effective immediately upon its passage.

PASSED by the Common Council this 11th day of August 2025.

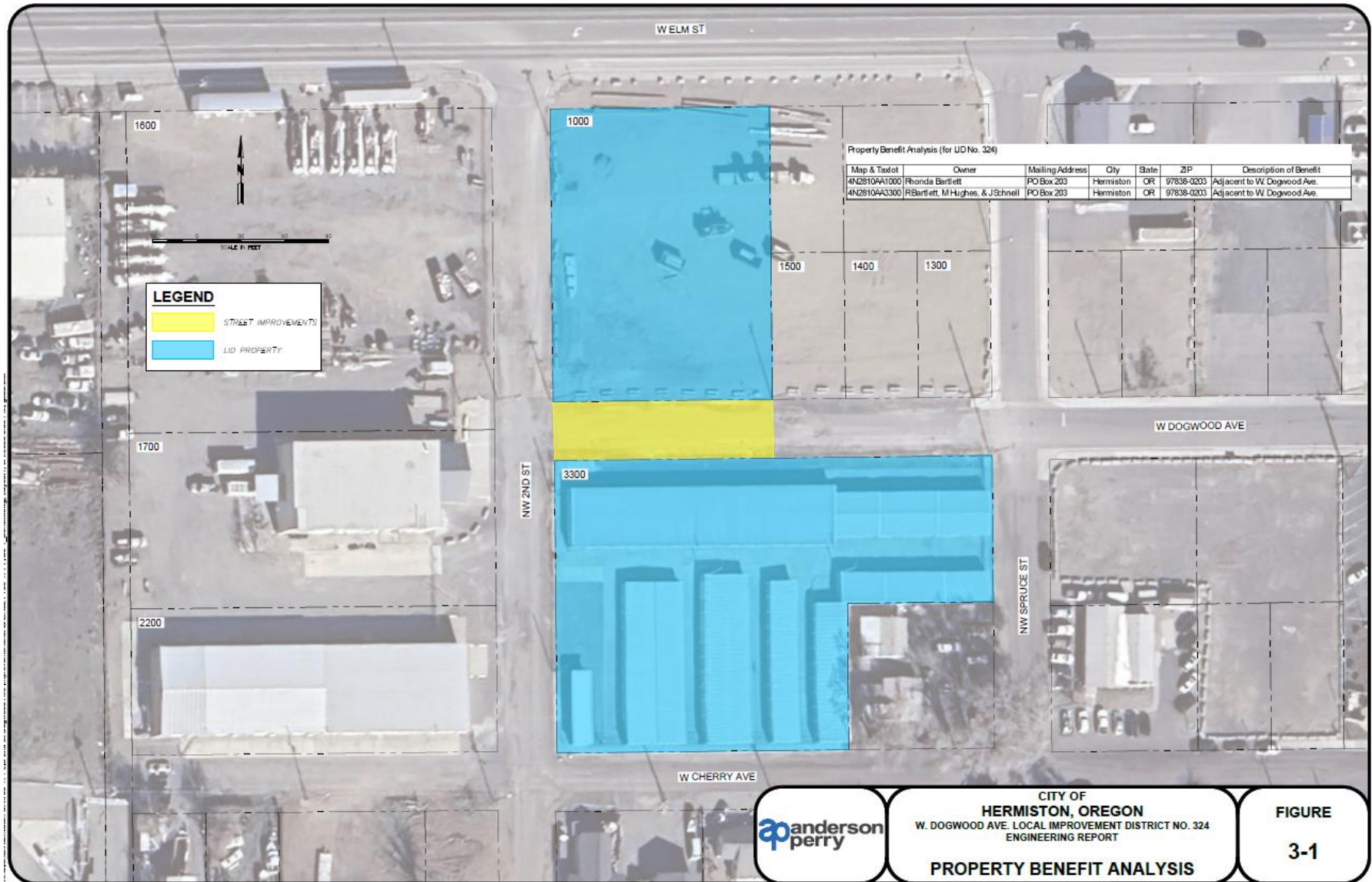
SIGNED by the Mayor this 11th day of August 2025.

Doug Primmer, MAYOR

ATTEST:

Heather KP La Beau, ASSISTANT CITY RECORDER

Resolution 2383 Appendix A





Where Life is Sweet

Mayor and Members of the City Council
STAFF REPORT
 For the Meeting of August 11, 2025

Title/Subject

Resolution 2384- Initiate LID for W Dogwood

Summary and Background

West Dogwood is a gravel unimproved street between NW 2nd and NW Spruce which provides direct access to an active industrial business, and one industrial lot which regularly gets used for construction laydown activities, these are known as 4N2810AA Tax Lots 1500 and 3300.

The Oregon Department of Transportation is scheduled to erect traffic barricades on W Elm Avenue to better manage the efficient and safe flow of traffic through the Elm/395 intersection. These barricades will eliminate the ability for left-hand turning movements in to and out of NW Spruce. Therefore, the only remaining access on Elm Ave to the active businesses on NW 2nd for vehicles coming from the Northeast will be on NW 2nd. Similarly, the only Elm Avenue access for vehicles coming from these businesses looking to go West will be on NW 2nd. This will significantly increase the amount of traffic on this sub-standard street.

W Dogwood has been an unimproved gravel roadway due to two main factors:

- Non-Remonstrance Agreements (NRA)
 - The City of Hermiston's standard requires that when a tax lot develops (structures are built on it), then the roadway in front of it must be built.
 - Developers can avoid this obligation through signing an NRA. This generally states that they will not have to develop the street in order to build on their property, but if/when a Local Improvement District is proposed, then they waive their ability to formally oppose it's formation.
 - One of the two tax lots involved in this section of Dogwood has an executed NRA
- Undeveloped
 - Undeveloped lots within the City are generally only required to pay to improve their street frontage at the time that they develop.
 - One of the two tax lots involved in this section of Dogwood has not formally developed

Due to the impending increase in traffic on Dogwood triggered by the shift in traffic pattern accessing these businesses, it is recommended that the roadway be constructed to city standards.

Formation of a Local Improvement District (LID) is a way to finance public streets by levying the cost of the roadway on to the abutting properties which benefit from the street in amounts proportionate to their frontage.

The City of Hermiston has successfully executed 322 LID's, primarily to develop roadways.

There are several steps required by City Code, and State Law, in order to form an LID. The first step is that the City Council must direct the City Manager to have a report developed which actually scopes out the project, estimates costs, establishes methodology for assigning costs, etc. That is what Resolution 2384 does.

Tie-In to Council Goals

Transportation: Improve Mobility and Transportation

Fiscal Information

This resolution does not actually form an LID, or create any obligation whatsoever on to property owners. This resolution simply directs the City Manager to develop a report which can be used by the Council in determining whether or not to move forward to the next steps.

Alternatives and Recommendation

Alternatives

1. Approve resolution 2384
2. Reject resolution 2384

Recommended Action/Motion

Motion to approve Resolution 2384

Submitted By:

Mark Morgan

RESOLUTION NO. 2384

A RESOLUTION INITIATING THE FORMATION OF A LOCAL IMPROVEMENT DISTRICT IN THE VICINITY OF W DOGWOOD BETWEEN TAX LOT 1500 AND TAX LOT 3300 AND DIRECTING THE CITY MANAGER TO PREPARE A FEASIBILITY REPORT FOR THE IMPROVEMENTS IN ACCORDANCE WITH THE ORDINANCES OF THE CITY OF HERMISTON.

WHEREAS, approximately 50 linear feet of City Street right of way for W Dogwood Avenue exists in an underdeveloped nature consisting of a graveled surface between Tax Lot 1500 and Tax Lot 3300.

NOW, THEREFORE, THE CITY OF HERMISTON RESOLVES AS FOLLOWS:

1. That it is expedient and necessary to improve, and it is hereby proposed to curb, gutter, sidewalk and pave with asphalt, in accordance with standard city specifications, the following parts of streets hereinafter named and at the stated width:

Between Tax Lot 1500 and Tax Lot 3300, a distance of 50 linear feet of W Dogwood Avenue between NW Spruce Street and NW 1st Street.

2. That the city manager shall make, and is hereby instructed to prepare a report including a description of the local improvement project for the above described improvement and its boundaries based on preliminary project plans and specifications; a preliminary determination of the feasibility of making the proposed improvement, including an estimate of the actual cost of the proposed local improvement; a map of the proposed local improvement district with the address and the map and tax lot number or other sufficient description and ownership of each specially benefited property with a brief explanation of why the properties benefit; the proposed methodology for allocating the improvement project costs among and between the specially benefited properties, together with a description of other funds, if any, proposed to be used; the assessed valuation of each property and an estimate of the assessment amount for each lot or portion thereof, with a statement of the amount of outstanding assessments against any lot proposed to be assessed by the improvement.
3. That this resolution is effective immediately upon its passage.

PASSED by the Common Council this 11th day of August 2025.

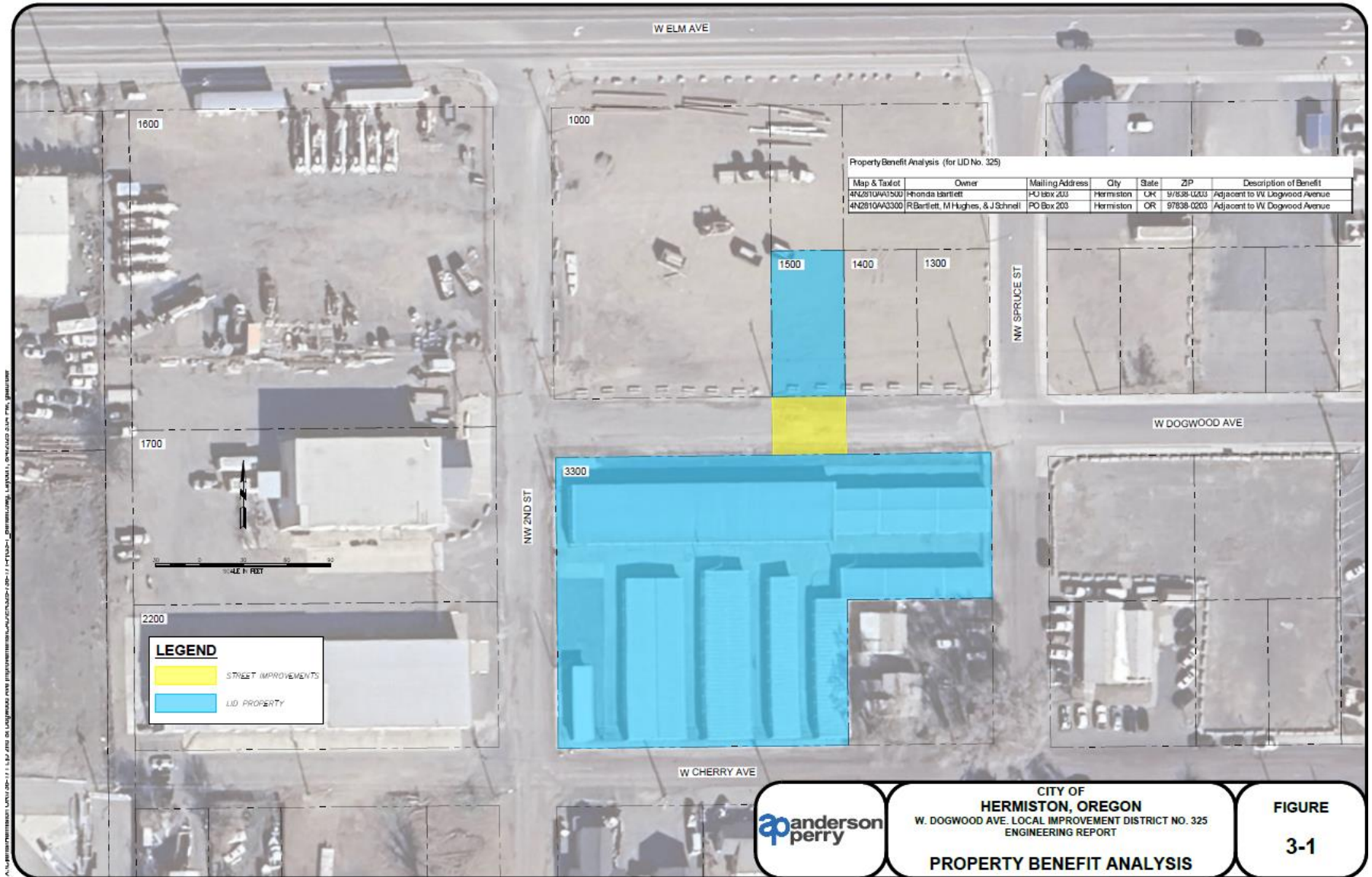
SIGNED by the Mayor this 11th day of August 2025.

Doug Primmer, MAYOR

ATTEST:

Heather KP La Beau, ASSISTANT CITY RECORDER

Resolution 2384 Appendix A





Where Life is Sweet

Mayor and Members of the City Council
STAFF REPORT
 For the Meeting of August 11, 2025

Title/Subject

Resolution 2385- Initiate LID for NW 2nd St

Summary and Background

NW 2nd is a gravel unimproved street between W Elm and W Cherry which provides direct access to two active industrial businesses. These are known as 4N2810AA Tax Lots 1700 and 3300.

The Oregon Department of Transportation is scheduled to erect traffic barricades on W Elm Avenue to better manage the efficient and safe flow of traffic through the Elm/395 intersection. These barricades will eliminate the ability for left-hand turning movements in to and out of NW Spruce. Therefore, the only remaining access on Elm Ave to the active businesses on NW 2nd for vehicles coming from the Northeast will be on NW 2nd. Similarly, the only Elm Avenue access for vehicles coming from these businesses looking to go West will be on NW 2nd. This will significantly increase the amount of traffic on this sub-standard street.

NW 2nd has been an unimproved gravel roadway due to two main factors:

- Non-Remonstrance Agreements (NRA)
 - o The City of Hermiston's standard requires that when a tax lot develops (structures are built on it), then the roadway in front of it must be built.
 - o Developers can avoid this obligation through signing an NRA. This generally states that they will not have to develop the street in order to build on their property, but if/when a Local Improvement District is proposed, then they waive their ability to formally oppose it's formation.
 - o One of the two tax lots involved in this section of Dogwood has an executed NRA
- Undeveloped
 - o Undeveloped lots within the City are generally only required to pay to improve their street frontage at the time that they develop.
 - o One of the two tax lots involved in this section of Dogwood has not formally developed

Due to the impending increase in traffic on NW 2nd triggered by the shift in traffic pattern accessing these businesses, it is recommended that the roadway be constructed to city standards.

Formation of a Local Improvement District (LID) is a way to finance public streets by levying the cost of the roadway on to the abutting properties which benefit from the street in amounts proportionate to their frontage.

The City of Hermiston has successfully executed 322 LID's, primarily to develop roadways.

There are several steps required by City Code, and State Law, in order to form an LID. The first step is that the City Council must direct the City Manager to have a report developed which actually scopes out the project, estimates costs, establishes methodology for assigning costs, etc. That is what Resolution 2385 does.

Tie-In to Council Goals

Transportation: Improve Mobility and Transportation

Fiscal Information

This resolution does not actually form an LID, or create any obligation whatsoever on to property owners. This resolution simply directs the City Manager to develop a report which can be used by the Council in determining whether or not to move forward to the next steps.

Alternatives and Recommendation

Alternatives

1. Approve resolution 2385
2. Reject resolution 2385

Recommended Action/Motion

Motion to approve Resolution 2385

Submitted By:

Mark Morgan

RESOLUTION NO. 2385

A RESOLUTION INITIATING THE FORMATION OF A LOCAL IMPROVEMENT DISTRICT IN THE VICINITY OF NW 2ND STREET BETWEEN TAX LOT 1700 AND TAX LOT 3300 AND DIRECTING THE CITY MANAGER TO PREPARE A FEASIBILITY REPORT FOR THE IMPROVEMENTS IN ACCORDANCE WITH THE ORDINANCES OF THE CITY OF HERMISTON.

WHEREAS, approximately 120 linear feet of City Street right of way for NW 2nd Street exists in an underdeveloped nature consisting of a graveled surface between Tax Lot 1700 and Tax Lot 3300.

NOW, THEREFORE, THE CITY OF HERMISTON RESOLVES AS FOLLOWS:

1. That it is expedient and necessary to improve, and it is hereby proposed to curb, gutter, sidewalk and pave with asphalt, in accordance with standard city specifications, the following parts of streets hereinafter named and at the stated width:

Between Tax Lot 1700 and Tax Lot 3300, a distance of 120 linear feet of NW 2nd Street between W Cherry Avenue and W Elm Avenue.

2. That the city manager shall make, and is hereby instructed to prepare a report including a description of the local improvement project for the above described improvement and its boundaries based on preliminary project plans and specifications; a preliminary determination of the feasibility of making the proposed improvement, including an estimate of the actual cost of the proposed local improvement; a map of the proposed local improvement district with the address and the map and tax lot number or other sufficient description and ownership of each specially benefited property with a brief explanation of why the properties benefit; the proposed methodology for allocating the improvement project costs among and between the specially benefited properties, together with a description of other funds, if any, proposed to be used; the assessed valuation of each property and an estimate of the assessment amount for each lot or portion thereof, with a statement of the amount of outstanding assessments against any lot proposed to be assessed by the improvement.
3. That this resolution is effective immediately upon its passage.

PASSED by the Common Council this 11th day of August 2025.

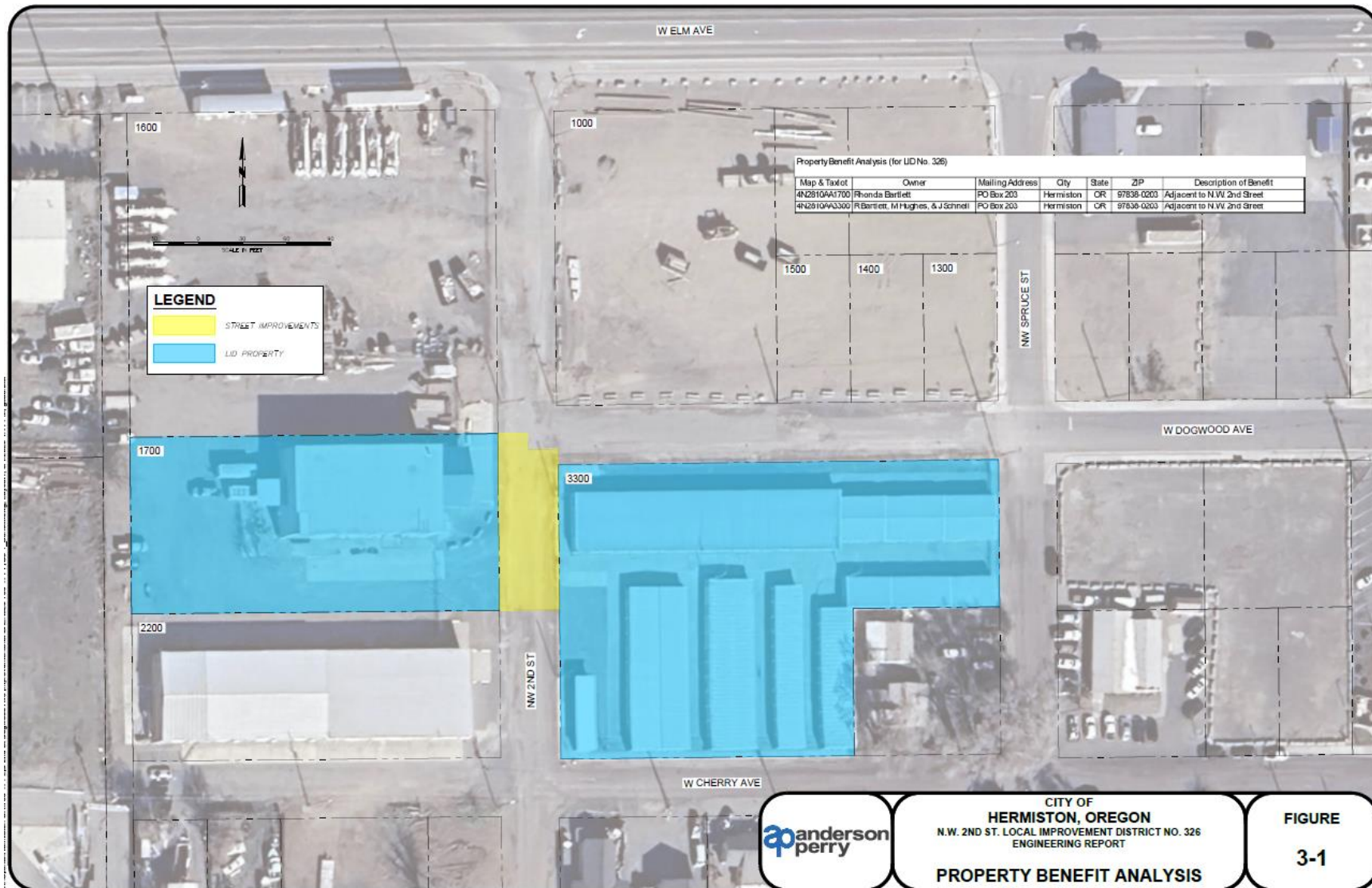
SIGNED by the Mayor this 11th day of August 2025.

Doug Primmer, MAYOR

ATTEST:

Heather KP La Beau, ASSISTANT CITY RECORDER

Resolution 2385 Appendix A





Where Life is Sweet

Mayor and Members of the City Council
STAFF REPORT
 For the Meeting of August 11, 2025

Title/Subject

Resolution 2386- Initiate LID for NW 2nd St

Summary and Background

NW 2nd is a gravel unimproved street between W Elm and W Cherry which provides direct access to two active industrial businesses. These are known as 4N2810AA Tax Lots 2200 and 3300.

The Oregon Department of Transportation is scheduled to erect traffic barricades on W Elm Avenue to better manage the efficient and safe flow of traffic through the Elm/395 intersection. These barricades will eliminate the ability for left-hand turning movements in to and out of NW Spruce. Therefore, the only remaining access on Elm Ave to the active businesses on NW 2nd for vehicles coming from the Northeast will be on NW 2nd. Similarly, the only Elm Avenue access for vehicles coming from these businesses looking to go West will be on NW 2nd. This will significantly increase the amount of traffic on this sub-standard street.

NW 2nd has been an unimproved gravel roadway due to two main factors:

- Non-Remonstrance Agreements (NRA)
 - o The City of Hermiston's standard requires that when a tax lot develops (structures are built on it), then the roadway in front of it must be built.
 - o Developers can avoid this obligation through signing an NRA. This generally states that they will not have to develop the street in order to build on their property, but if/when a Local Improvement District is proposed, then they waive their ability to formally oppose it's formation.
 - o One of the two tax lots involved in this section of Dogwood has an executed NRA
- Undeveloped
 - o Undeveloped lots within the City are generally only required to pay to improve their street frontage at the time that they develop.
 - o One of the two tax lots involved in this section of Dogwood has not formally developed

Due to the impending increase in traffic on NW 2nd triggered by the shift in traffic pattern accessing these businesses, it is recommended that the roadway be constructed to city standards.

Formation of a Local Improvement District (LID) is a way to finance public streets by levying the cost of the roadway on to the abutting properties which benefit from the street in amounts proportionate to their frontage.

The City of Hermiston has successfully executed 322 LID's, primarily to develop roadways.

There are several steps required by City Code, and State Law, in order to form an LID. The first step is that the City Council must direct the City Manager to have a report developed which actually scopes out the project, estimates costs, establishes methodology for assigning costs, etc. That is what Resolution 2386 does.

Tie-In to Council Goals

Transportation: Improve Mobility and Transportation

Fiscal Information

This resolution does not actually form an LID, or create any obligation whatsoever on to property owners. This resolution simply directs the City Manager to develop a report which can be used by the Council in determining whether or not to move forward to the next steps.

Alternatives and Recommendation

Alternatives

1. Approve resolution 2386
2. Reject resolution 2386

Recommended Action/Motion

Motion to approve Resolution 2386

Submitted By:

Mark Morgan

RESOLUTION NO. 2386

A RESOLUTION INITIATING THE FORMATION OF A LOCAL IMPROVEMENT DISTRICT IN THE VICINITY OF NW 2ND STREET BETWEEN TAX LOT 2200 AND TAX LOT 3300 AND DIRECTING THE CITY MANAGER TO PREPARE A FEASIBILITY REPORT FOR THE IMPROVEMENTS IN ACCORDANCE WITH THE ORDINANCES OF THE CITY OF HERMISTON.

WHEREAS, approximately 100 linear feet of City Street right of way for NW 2nd Street exists in an underdeveloped nature consisting of a graveled surface between Tax Lot 2200 and Tax Lot 3300.

NOW, THEREFORE, THE CITY OF HERMISTON RESOLVES AS FOLLOWS:

1. That it is expedient and necessary to improve, and it is hereby proposed to curb, gutter, sidewalk and pave with asphalt, in accordance with standard city specifications, the following parts of streets hereinafter named and at the stated width:

Between Tax Lot 2200 and Tax Lot 3300, a distance of 100 linear feet of NW 2nd Street between W Cherry Avenue and W Elm Avenue.

2. That the city manager shall make, and is hereby instructed to prepare a report including a description of the local improvement project for the above described improvement and its boundaries based on preliminary project plans and specifications; a preliminary determination of the feasibility of making the proposed improvement, including an estimate of the actual cost of the proposed local improvement; a map of the proposed local improvement district with the address and the map and tax lot number or other sufficient description and ownership of each specially benefited property with a brief explanation of why the properties benefit; the proposed methodology for allocating the improvement project costs among and between the specially benefited properties, together with a description of other funds, if any, proposed to be used; the assessed valuation of each property and an estimate of the assessment amount for each lot or portion thereof, with a statement of the amount of outstanding assessments against any lot proposed to be assessed by the improvement.
3. That this resolution is effective immediately upon its passage.

PASSED by the Common Council this 11th day of August 2025.

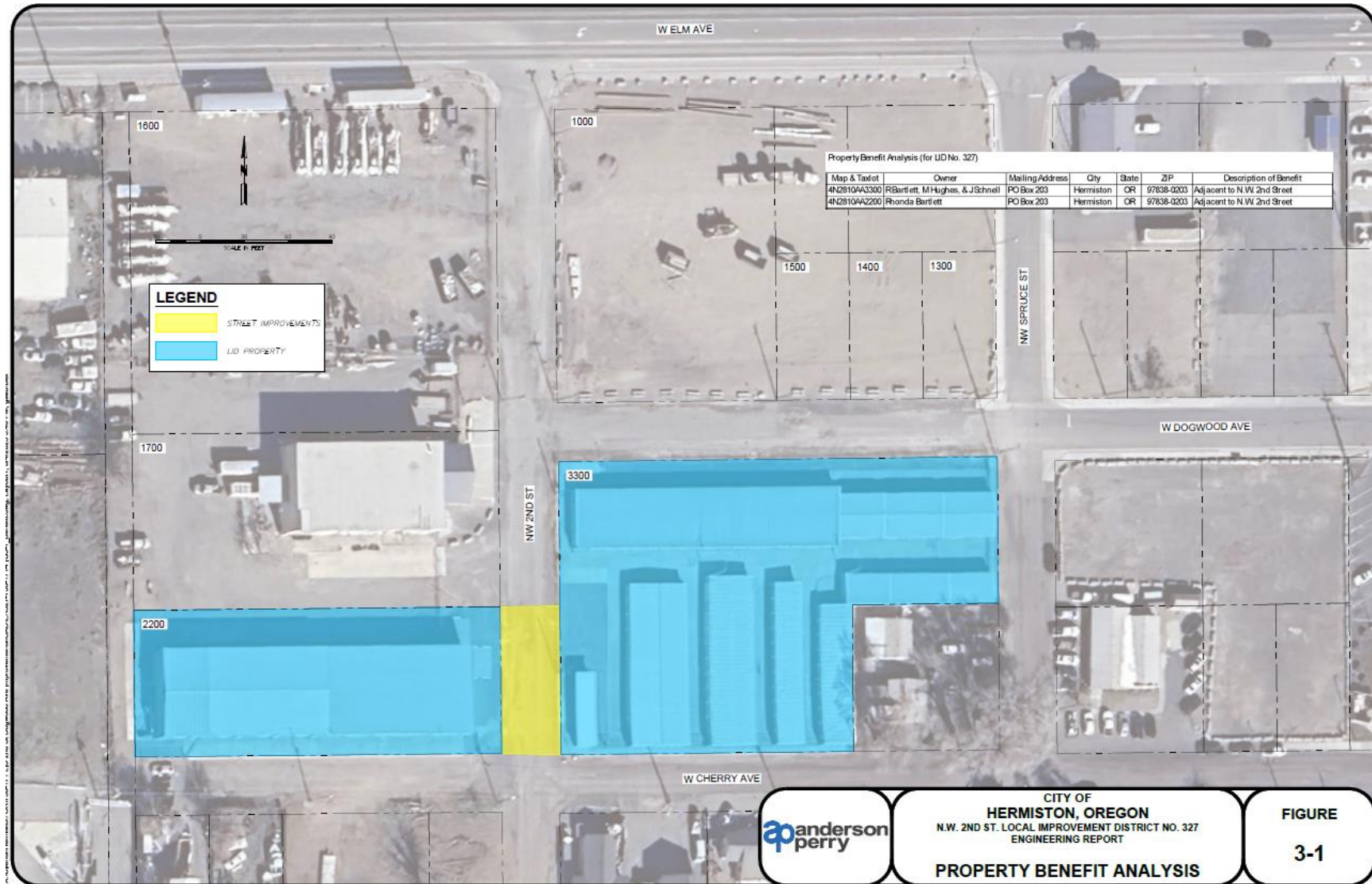
SIGNED by the Mayor this 11th day of August 2025.

Doug Primmer, MAYOR

ATTEST:

Heather KP La Beau, ASSISTANT CITY RECORDER

Resolution 2386 Appendix A





Where Life is Sweet

Mayor and Members of the City Council
STAFF REPORT
 For the Meeting of August 11, 2025

Title/Subject

Resolution 2387- Declare Intent to Form LID for W Dogwood

Summary and Background

Resolution 2383 discusses the need for improvements to W Dogwood to accommodate increased traffic accessing businesses between 2nd & Spruce.

A report has been developed which contains the relevant information regarding scope, cost, impacted properties, methodology and estimated cost to each property, and it is included with this staff report.

Resolution 2387 declares the City Council's intent to form the LID as outlined, and sets a date for a public hearing for the September 8, 2025 City Council meeting. If Resolution 2387 is approved, then the two impacted properties shall immediately be mailed notice of the hearing date, as well as informing them of the project scope, estimated cost, methodology for assigning costs, estimated amount assigned to their property, and rights available to submit written comment and formal remonstrance.

Although Resolution 2387 is technically declaring "intent to form," it does not form the LID. Upon receipt of public comment and/or remonstrances at the September 8 meeting, City Council may choose to cease LID formation.

Tie-In to Council Goals

Transportation: Improve Mobility and Transportation

Fiscal Information

The LID as presented allocates all costs to the benefitting properties. No taxpayer contribution is considered.

This resolution does not actually form an LID, or create any obligation whatsoever on to property owners. This resolution simply sets a public hearing date at which point Council will consider whether or not to actually form the LID.

Alternatives and Recommendation

Alternatives

1. Approve resolution 2387
2. Reject resolution 2387

Recommended Action/Motion

Motion to approve Resolution 2387

Submitted By:

Mark Morgan

RESOLUTION NO. 2387

A RESOLUTION ESTABLISHING THE INTENT TO FORM LOCAL IMPROVEMENT DISTRICT IN THE VICINITY OF W DOGWOOD AVENUE BETWEEN TAX LOT 1000 AND TAX LOT 3300 AND SETTING A DATE FOR PUBLIC HEARING

WHEREAS, on August 11, 2025, the City Manager of the City of Hermiston, Oregon, under the direction and by requirement of the council, did prepare and have on file in his office plans and specifications for the formation of a Local Improvement District, a copy of which is attached as Exhibit A for the installation of curb, gutter, sidewalk and paving of:

W Dogwood Avenue between Tax Lot 1000 and Tax Lot 3300, a distance of approximately 150 linear feet.

WHEREAS, Exhibit A also includes: a map of the proposed local improvement district boundary, including the address, map and tax lot and ownership information for each property within the boundary; an estimate of the actual cost of the improvements; and the proposed methodology for establishing special benefit and levying assessments, the estimated assessment to be levied against each property and a description of funds other than assessments that will be used to fund the local improvement; and

WHEREAS, the council has examined Exhibit A and finds the same satisfactory and the estimate therefore to be in accordance with the probable cost of such work; and

WHEREAS, the property recommended by the City Manager to be included within the boundaries of the district benefitted is in the judgement of the council proper to be included therein and no property is excluded therefrom which should properly be included therein; and

WHEREAS, the installation of the above described paving and utilities is at this time necessary.

NOW, THEREFORE, THE CITY OF HERMISTON RESOLVES AS FOLLOWS:

That it is expedient to improve, and it is hereby proposed, to curb, gutter, sidewalk, and pave W Dogwood Avenue between Tax Lot 1000 and Tax Lot 3300, a distance of approximately 150 linear feet in accordance with Exhibit A, which is hereby adopted and approved.

1. That the total estimated cost identified in Exhibit A as \$206,000 shall be assessed as a local improvement district.
2. That the proposed improvement district is to be known as "LOCAL IMPROVEMENT DISTRICT NO. 324"; embracing the property benefitted and assessed for the payment of such improvements; which improvement district shall include all lots, parts of lots and parcels of land lying and being within the district described in Exhibit A.

3. That at least ten days prior to the public hearing on formation, the City Manager shall mail or personally deliver notice to the owners of each property proposed to be located within the local improvement district. In addition to the date, time and location of the hearing, the notice shall include:
 - a. A copy of Exhibit A, which includes a general description of the improvements, the boundary of the district, the addresses of each property included, the estimated cost of the proposed local improvement, the methodology for levying assessments and the estimated assessment for each property.
 - b. A statement that these are estimates only, and that the final assessment may be greater.
 - c. A statement that any interested person may testify or submit written comments on the proposed local improvement district and that any property owner may formally object by submitting a written remonstrance stating the reason for the objection and signed by each owner of the property and that the remonstrance must be received by the City Manager no later than 5:00 p.m. on the business day before the public hearing.
 - d. A form for remonstrance.
4. That a public hearing is set for the 8th day of September 2025 at or after 7:00 p.m. at the City Hall (180 NE 2nd St.) in Hermiston, Oregon at which time the Council shall provide a reasonable opportunity for persons to testify and shall consider any written comments received with any remonstrances on such proposed improvement.
5. That this resolution is effective immediately upon its passage.

PASSED by the Common Council this 11th day of August 2025.

SIGNED by the Mayor this 11th day of August 2025.

Doug Primmer, MAYOR

ATTEST:

Heather KP La Beau, ASSISTANT CITY RECORDER

**W. DOGWOOD AVENUE
LOCAL IMPROVEMENT DISTRICT NO. 324
ENGINEERING REPORT**

AUGUST 2025



Prepared for the
City of Hermiston, Oregon

**W. DOGWOOD AVENUE
LOCAL IMPROVEMENT DISTRICT NO. 324
ENGINEERING REPORT**

FOR

CITY OF HERMISTON, OREGON

2025



ANDERSON PERRY & ASSOCIATES, INC.

**La Grande, Redmond, Hermiston, and Enterprise, Oregon
Walla Walla, Washington**

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Section 1 - Project Description

General

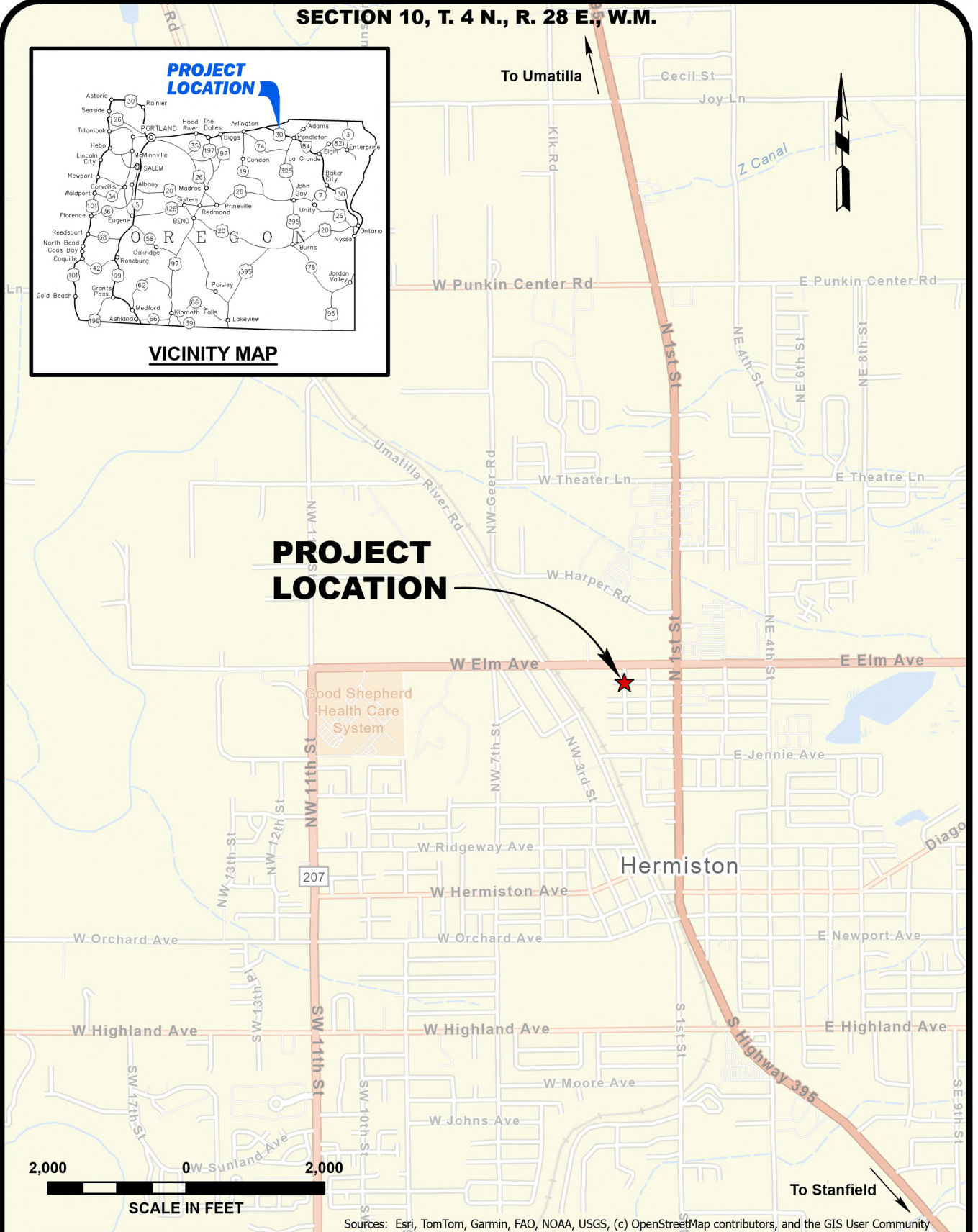
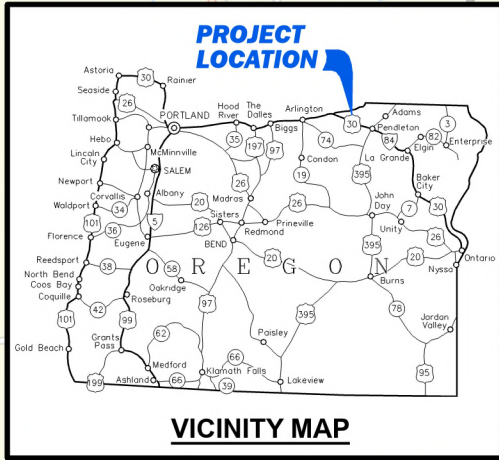
The City of Hermiston, Oregon, is considering the formation of a Local Improvement District (LID) No. 324 for W. Dogwood Avenue to provide a portion of the street improvements in anticipation of increased traffic due to upcoming improvements to Highway 207 (W. Elm Avenue). Two local improvement districts, including this LID and LID No. 325, will be used to construct improvements to W. Dogwood Avenue. These improvements would generally include replacing existing gravel streets with urban local residential streets.

Project Description

The proposed LID location is shown on Figure 1-1, Location and Vicinity Maps, and an aerial of the proposed LID boundary is shown on Figure 1-2, Aerial Photograph. The proposed LID consists of two properties, comprising a total of 300 feet of frontage. A 40-foot wide existing right-of-way is available for the construction of the street improvements. The W. Dogwood Avenue LID tax map with lot lines and parcel sizes is included in Appendix A.

The improvements for the proposed LID are shown in Figure 1-3, Proposed Improvements. The improvements would consist of approximately 150 linear feet of street improvements, including 5-foot sidewalks, pedestrian ramps, driveways, curb and gutter, two 14-foot travel lanes, and stormwater facilities. “No Parking” signs would be located on both sides of the street. The pavement section would consist of 3 inches of asphalt concrete pavement on 3 inches of 3/4”-0 aggregate base and 6 inches of 1”-0 aggregate base. These proposed improvements are based on a planning-level design and are preliminary in nature. A typical street cross section of the proposed street improvements, based on City Standard Drawing ST11 Local Residential - Option 1, is shown on Figure 1-4. Curb and sidewalk standard drawings are included in Appendix B. A property benefit analysis is included in Chapter 3, with property boundaries current as of July 2025.

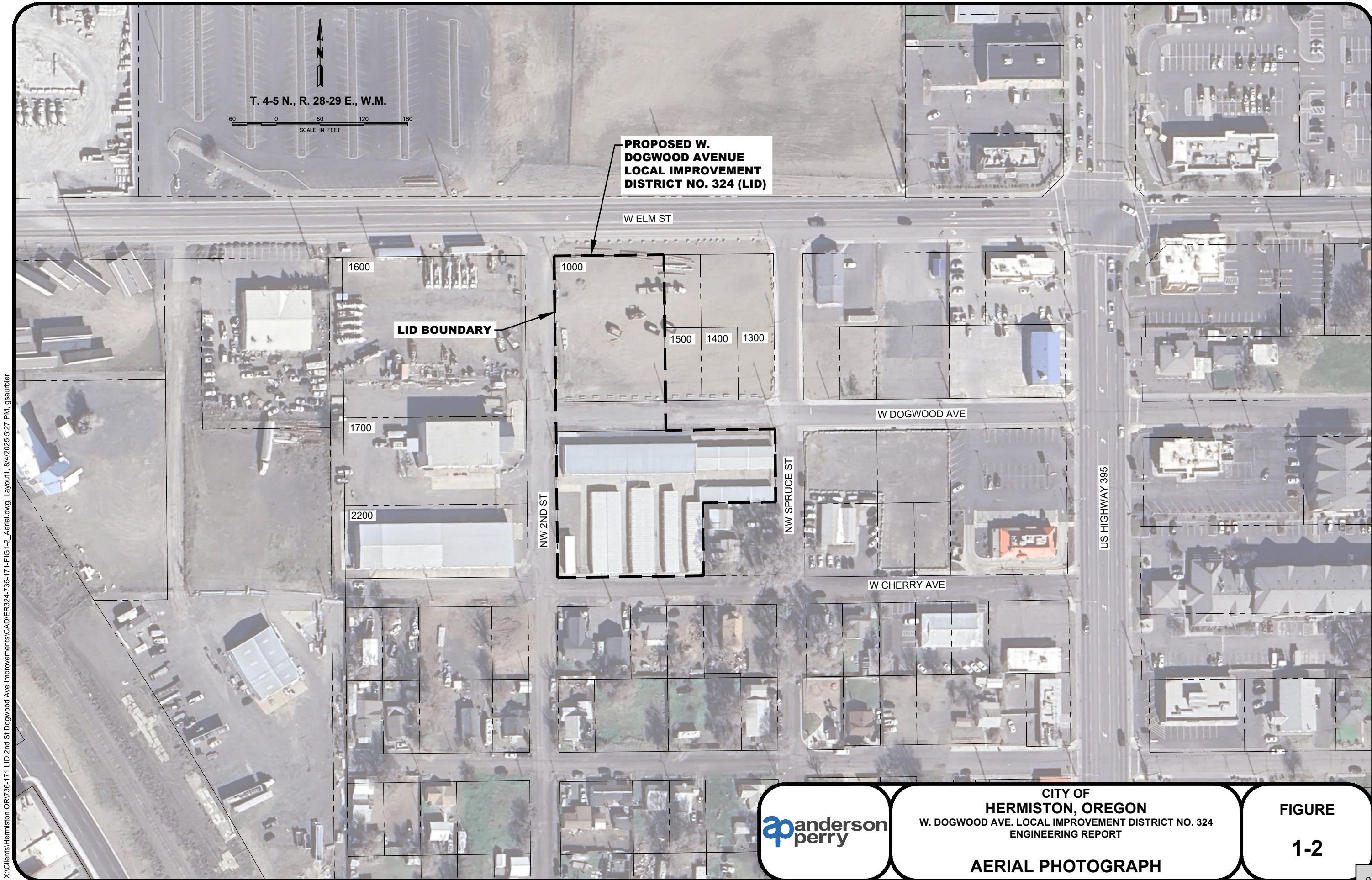
SECTION 10, T. 4 N., R. 28 E., W.M.



CITY OF
HERMISTON, OREGON
W. DOGWOOD AVE. LOCAL IMPROVEMENT DISTRICT NO. 324
ENGINEERING REPORT

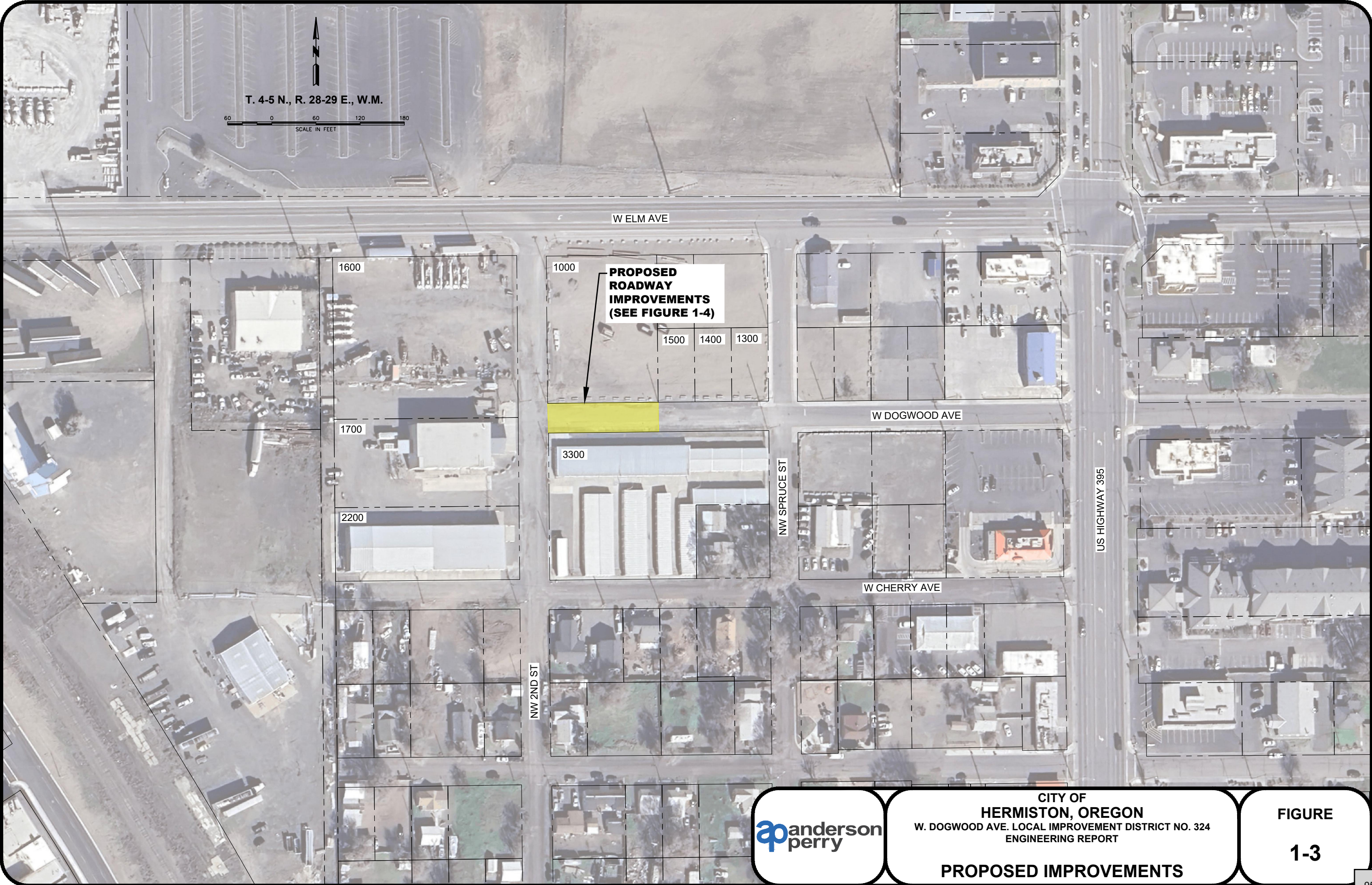
FIGURE
1-1

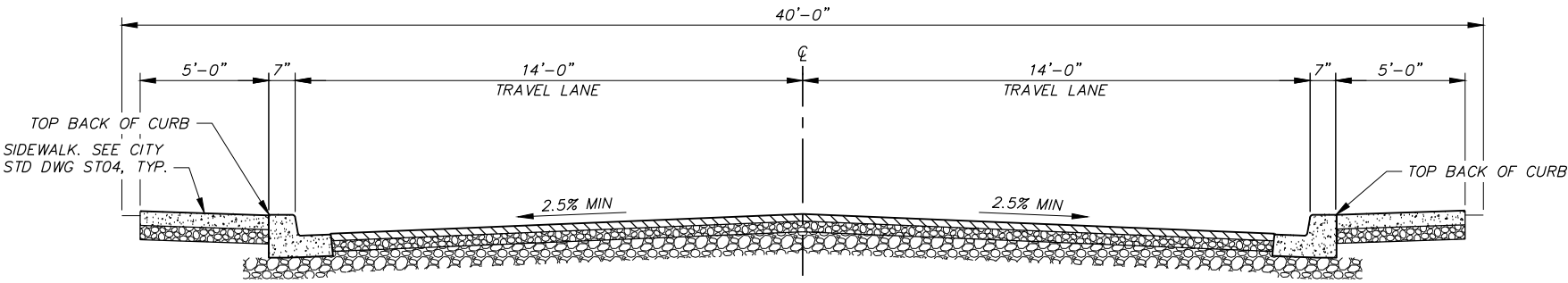
LOCATION AND VICINITY MAPS



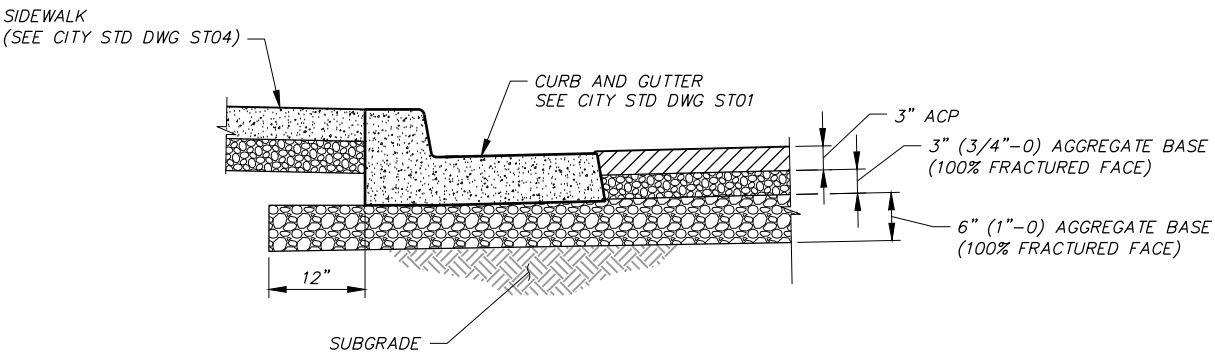
X:\Clients\Hermiston OR\736-171 LID 2nd St Dogwood Ave Improvements\CAD\ER324-736-171-FIG1-2_Aerial.dwg, Layout1, 8/4/2025 5:27 PM, gsaubier

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STREET CROSS SECTION
NTS



PAVEMENT SECTION
NTS



CITY OF
HERMISTON, OREGON
W. DOGWOOD AVE. LOCAL IMPROVEMENT DISTRICT NO. 324
ENGINEERING REPORT

URBAN LOCAL STREET SECTIONS

FIGURE
1-4

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Section 2 - Preliminary Feasibility Analysis

Street Improvement

The proposed street improvements required for the W. Dogwood Avenue Local Improvement District (LID) No. 324 shown in Figures 1-3 and 1-4 are feasible; however, they present certain challenges. The streets have been graded and graveled within the existing 40-foot right-of-way (ROW). The ROW width is narrower than the city standard ROW width of 50 feet. The street section shown on Figure 1-4 will be used to fit within the existing ROW. Additionally, the power poles near the edge of the ROW would need to be considered during engineering design. If the power poles are confirmed to be in the proposed sidewalk, they would need to be relocated during construction to provide a minimum 4-foot wide accessible path. Nevertheless, no significant obstacles are preventing the proposed street improvements.

Other Utilities

Water, sewer, communications, and electricity are currently available to the properties in the proposed LID.

Project Cost

A preliminary cost estimate was prepared as part of the feasibility analysis and is shown on Figure 2-1, Preliminary Cost Estimate. Due to the preliminary nature of the cost estimate, a 20 percent construction contingency has been included to account for unforeseen issues and potential variability in the bidding environment when the improvements are to be constructed. As shown, the estimate includes anticipated engineering as a percentage of construction costs. The preliminary cost estimate assumes construction of the improvements would occur in 2026; an anticipated annual inflation of 5 percent was assumed and is shown on the cost estimate. The cost estimate would need to be reviewed and potentially adjusted should the street improvements project move to the design and construction phases.

CITY OF HERMISTON, OREGON
W. DOGWOOD AVENUE
LOCAL IMPROVEMENT DISTRICTS NO. 324
PRELIMINARY COST ESTIMATE
August 6, 2025

NO.	DESCRIPTION	UNIT	UNIT PRICE	ESTIMATED QUANTITY	TOTAL PRICE
1	Mobilization/Demobilization	LS	\$ 15,100	All Req'd	\$ 15,100
2	Temporary Protection and Direction of Traffic/Project Safety	LS	3,050	All Req'd	3,050
3	Erosion and Sediment Control	LS	3,300	All Req'd	3,300
4	Removal of Structures and Obstructions*	LS	3,000	All Req'd	3,000
5	General Earthwork	LS	2,000	All Req'd	2,000
6	3/4"-0 Aggregate Base (3 inches)	CY	85	60	5,100
7	1"-0 Aggregate Base (6 inches)	CY	85	110	9,350
8	3-inch Asphalt Concrete Pavement	TON	175	80	14,000
9	Concrete Curb and Gutter	LF	50	300	15,000
10	Americans with Disabilities Act Ramp	EA	3,750	2	7,500
11	Concrete Driveway Approaches	SF	18	150	2,700
12	Concrete Sidewalk	SF	14	1,350	18,900
13	Stormwater**	LS	25,000	All Req'd	25,000
14	Surface Restoration	LS	1,000	All Req'd	1,000
15	Permanent Striping and Signing	LS	1,000	All Req'd	1,000
Estimated Construction Cost					\$ 126,000
Construction Contingency					30,000
Design Engineering					20,000
Construction Engineering					20,000
Total Year 2025 Estimated Project Cost					\$ 196,000
Inflation at Time of Construction*** (Assumed Construction Year 2026)					10,000
TOTAL YEAR 2026 ESTIMATED IMPROVEMENTS COST					\$ 206,000

*Power pole relocation is not included in this cost estimate since its location relative to the right-of-way has not been determined.

**Stormwater is assumed to include one drywell manhole, one sedimentation chamber, and two catch basins.

***Construction cost inflation is assumed to be 5 percent annually until construction occurs.

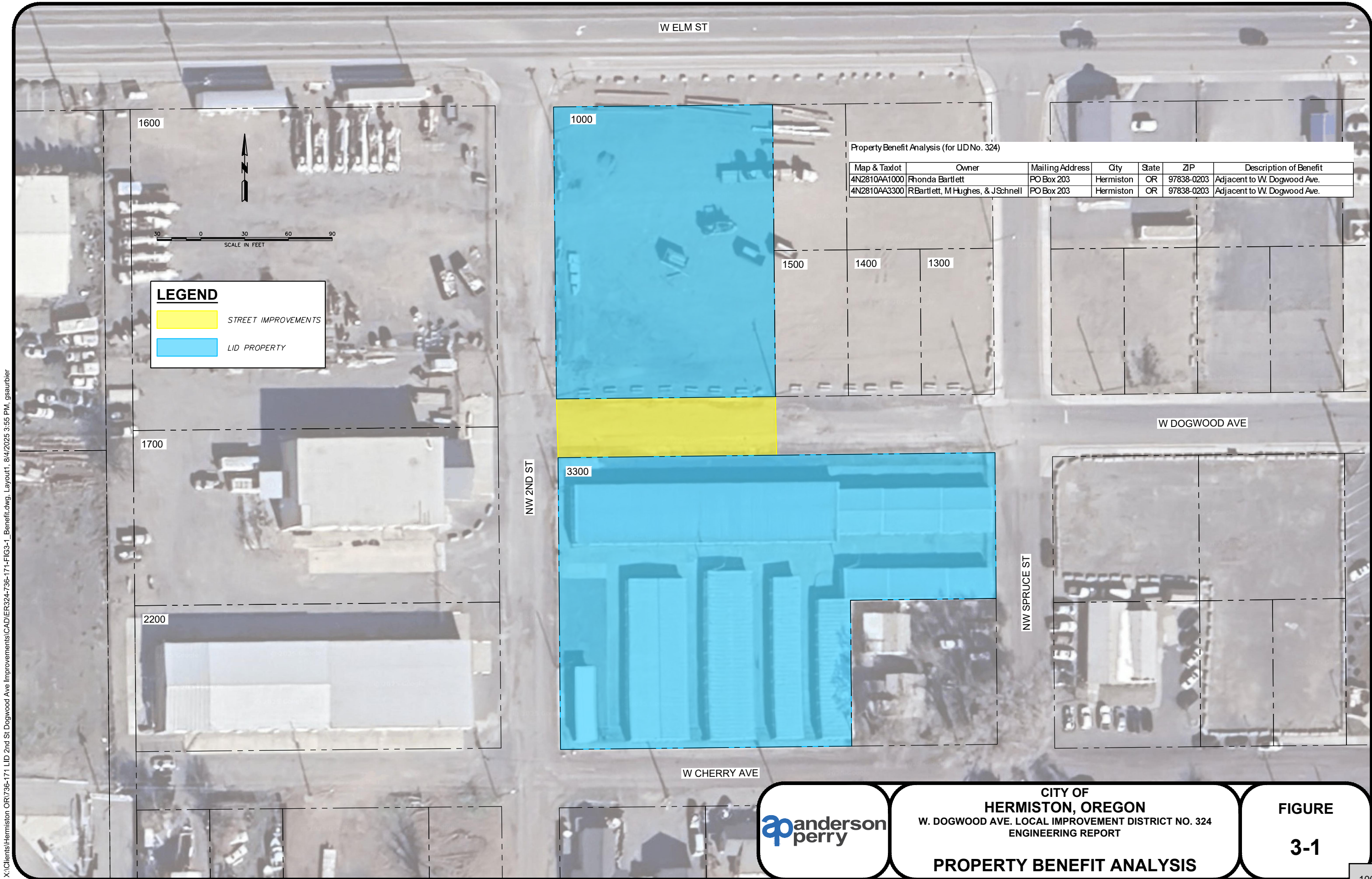
Section 3 - Property Benefit Analysis

Hermiston Municipal Code 157.164

Figure 3-1, Property Benefit Analysis, includes property identification information, ownership information, and a description of the benefit that each property would receive from the proposed improvements. For all properties in this W. Dogwood Avenue Local Improvement District (LID) No. 324, the benefit to each property is that the street infrastructure that would be installed for the proposed development area would be in conformance with Hermiston Municipal Code 157.164. The proposed street improvements would ensure each tax lot has paved access to and from the remainder of the street system.

Section 4 of this LID Engineering Report outlines the methodology for assigning costs, which would be based on linear feet (LF) of frontage adjacent to the street improvements. Since no off-site improvements are included in this LID Engineering Report, the benefit that each property would receive is directly proportional to the LF of improvements installed.

X:\Clients\Hermiston OR\736-171 LID 2nd St Dogwood Ave Improvements\CAD\ER324-736-171-FIG3-1_Benefit.dwg, Layout1, 8/4/2025 3:55 PM, gsaubier



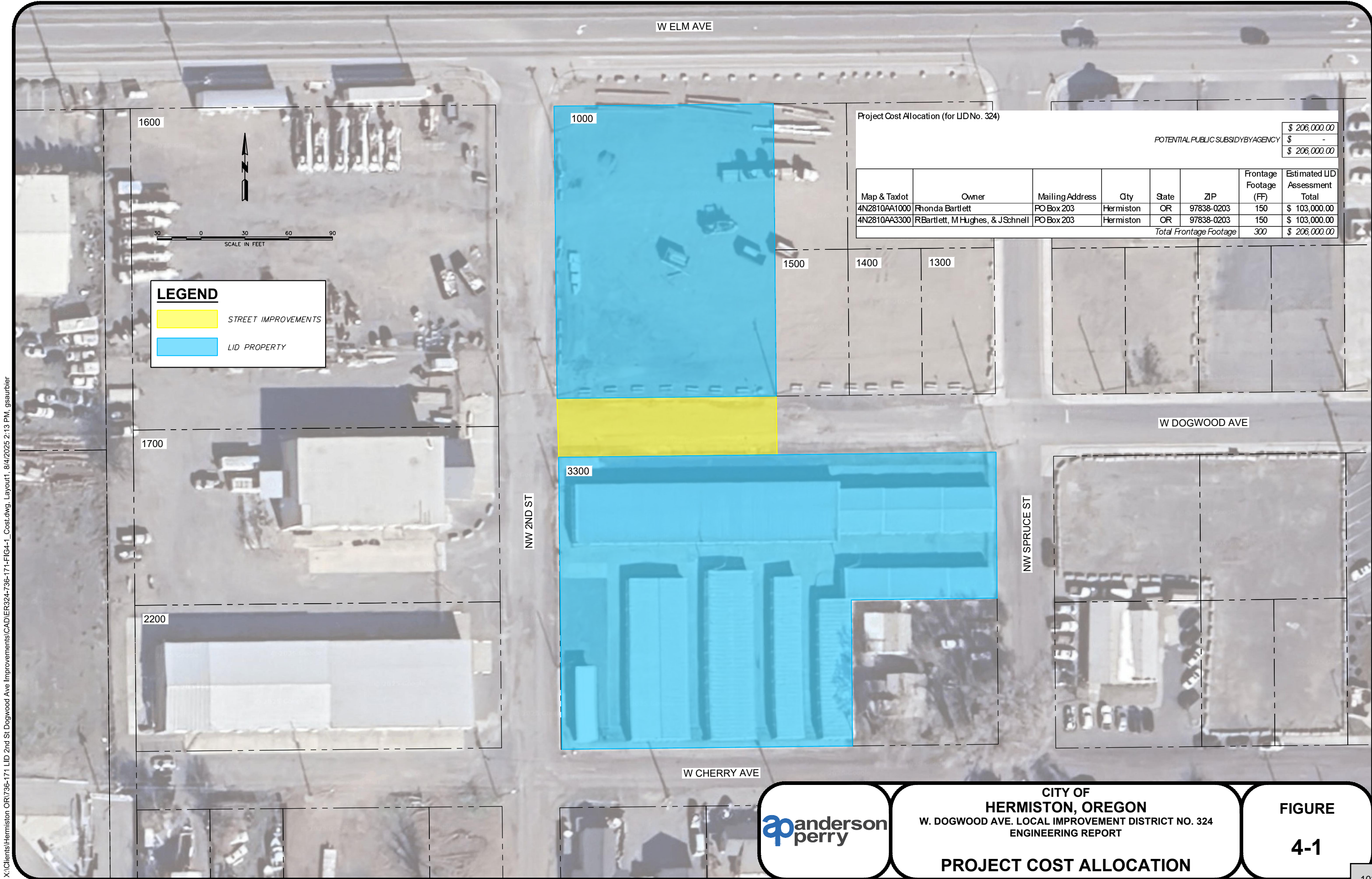
Section 4 - Project Cost Allocation

Methodology

The street improvement project included in the proposed W. Dogwood Avenue Local Improvement District (LID) No. 324 lends itself to allocating costs based on each property's frontage along the street improvements. The property frontage length along the proposed street improvements relative to total property frontage on the proposed street improvements for all properties in the LID is directly proportional to the assessment of each property. This method would distribute the costs of the LID equitably. Figure 4-1, Project Cost Allocation, shows each affected property, its associated frontage length, and its associated estimated LID assessment.

Description of Other Funds

The project cost is anticipated to be completely covered by the owners of the properties within the LID, so no additional funds would be required.



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Section 5 - Property Valuation and Assessment Estimate

General

The valuations for each property shown on Table 5-1 below represent the latest figures available from the Umatilla County Assessor's office. Table 5-1 shows an estimate of the amount to be assessed for each property in the W. Dogwood Avenue Local Improvement District (LID) No. 324. These amounts represent the total current value of the assessments without regard for potential long-term repayment plans and associated interest fees. An explanation of options for long-term payment plans is available below. A full accounting of how the assessment was calculated, using the methodology outlined in Section 4, is shown on Figure 4-1, Project Cost Allocation. Actual costs would be determined after construction is completed and final project costs are known.

TABLE 5-1
UMATILLA COUNTY ASSESSOR'S PROPERTY VALUATIONS

Map and Tax Lot	Assessed Value	Real Market Value	Outstanding Tax Assessments	Estimated LID Assessment Total
4N2810AA1000	\$112,740.00	\$283,500.00	\$0.00	\$103,000.00
4N2810AA3300	\$662,380.00	\$1,502,500.00	\$0.00	\$103,000.00

Long-term Repayment Plan

Construction of the proposed improvements considered in the LID would be coordinated by the City of Hermiston on behalf of each of the properties. Each property owner would have the option to pay their LID assessment in its entirety, or they may choose to make installment payments over ten years. The City intends to finance the improvements upfront through the most cost-effective means available. Any costs associated with carrying costs of financing the proposed improvements would be passed on directly to the benefiting properties; however, the City would not add on any administrative or overhead fees associated with obtaining and managing the financing instrument.

Appendices Table of Contents

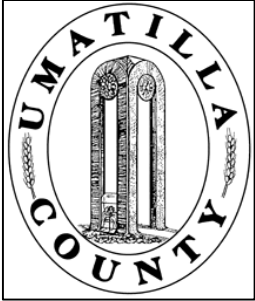
Appendix A - Tax Lot Map

Appendix B - Standard Drawings

APPENDIX A

Tax Lot Maps

This map was prepared for Assessment & Taxation purposes only and was NOT prepared nor is it suitable for legal, engineering or surveying purposes.

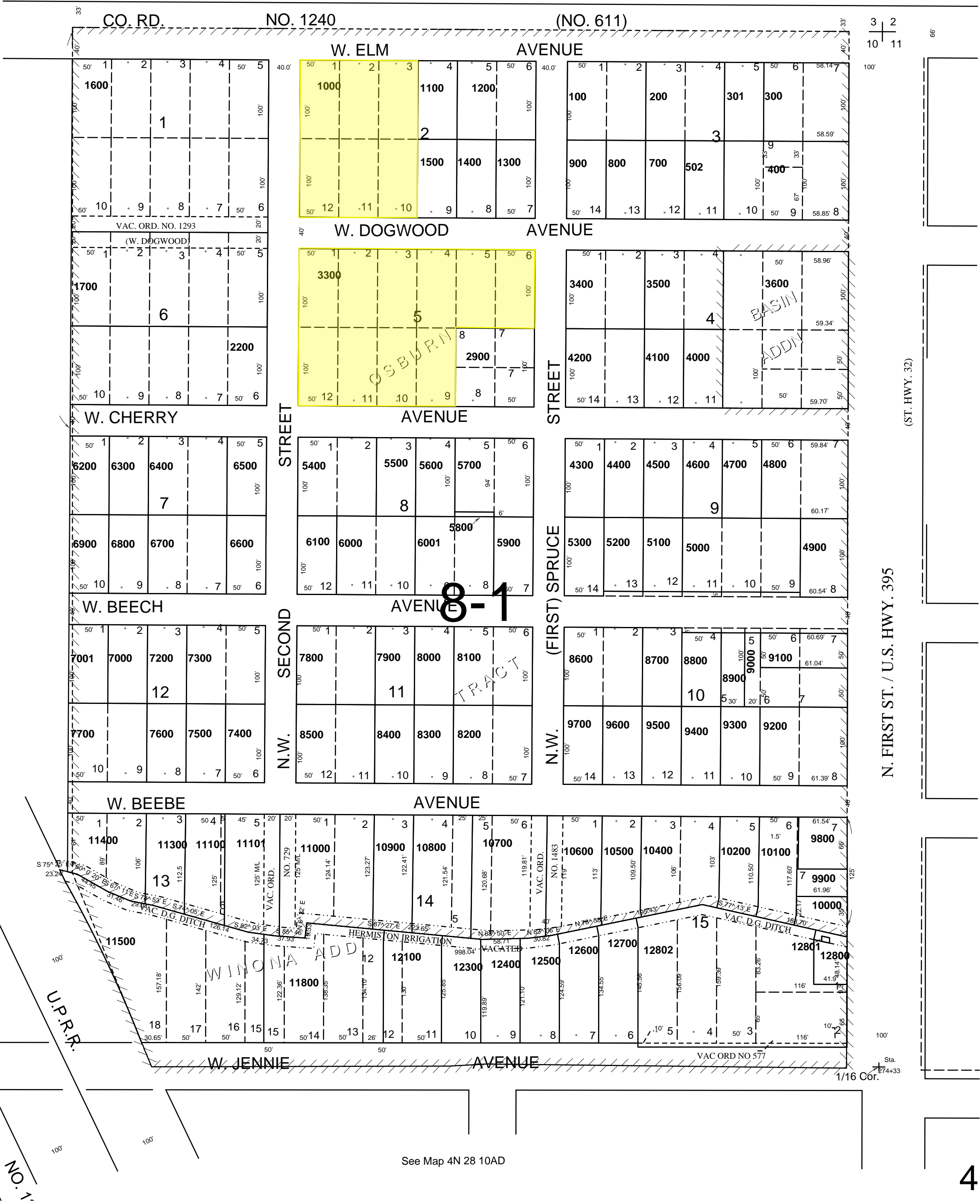


NE1/4 NE1/4 SEC 10 T4N R28E WM
UMATILLA COUNTY, OR

SCALE: 1" = 100'

4N2810AA

See Map 4N 03D



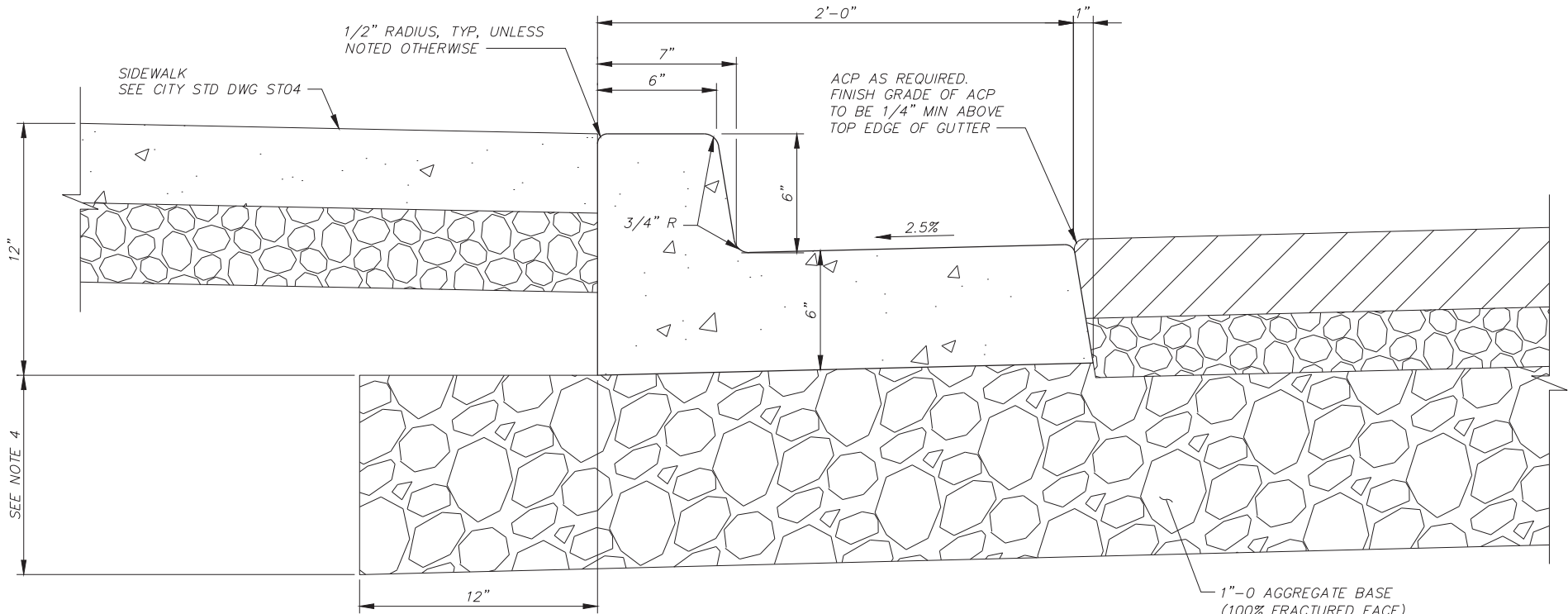
LID Boundary

APPENDIX B

Standard Drawings

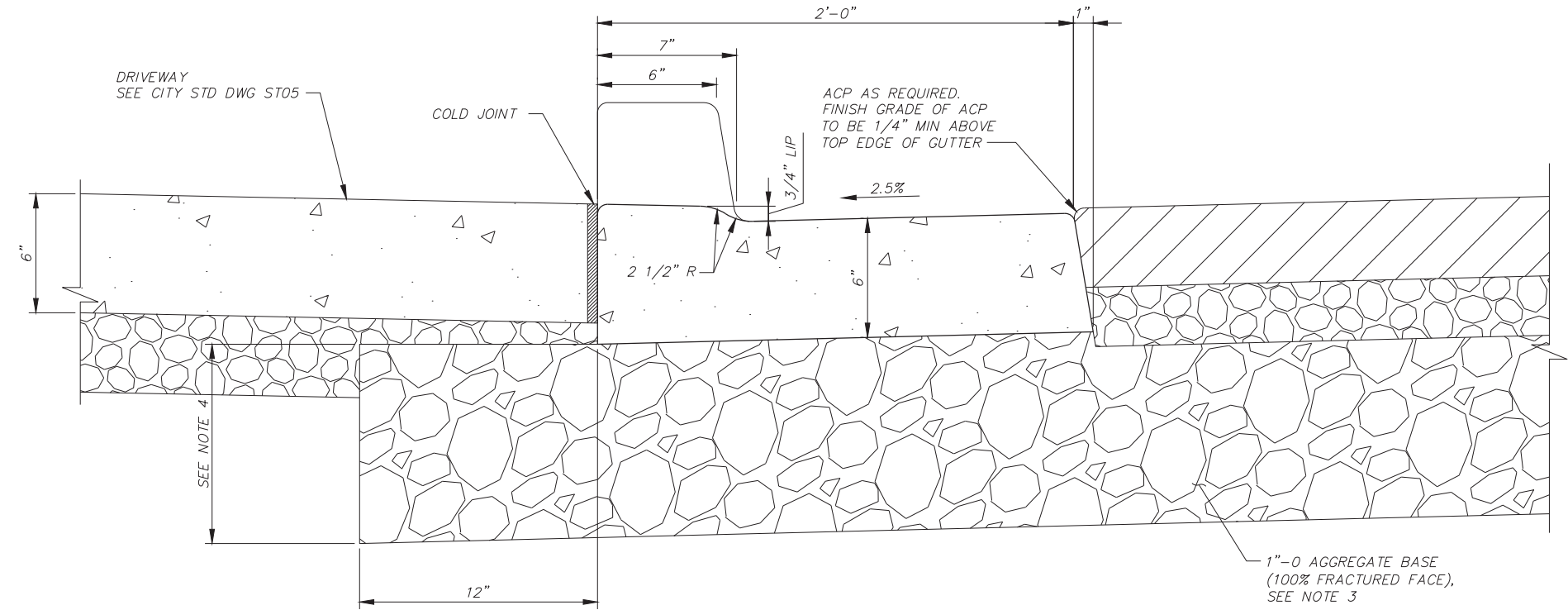
NOTES

1. GUTTER CROSS SLOPE SHALL BE MINIMUM 2% AND MAXIMUM 3% UNLESS OTHERWISE APPROVED BY THE CITY.
2. ALL CONCRETE SHALL BE COMMERCIAL GRADE 4,000 PSI CONCRETE.
3. COMPACT AGGREGATE BASE TO 95% MAXIMUM DRY DENSITY PER ASTM D1557.
4. AGGREGATE BASE THICKNESS BELOW CURB AND GUTTER SHALL BE 6" THICK OR CONFORM TO CITY STD DWG ST07-ST11, WHICHEVER IS GREATER.
5. SEE CITY STD DWG ST04 FOR CURB AND GUTTER JOINT REQUIREMENTS.
6. CONCRETE SHALL BE TROWELED SMOOTH PRIOR TO APPLYING BROOM FINISH.
7. INSPECTION CRITERIA:
 - 7.1. COMPLETED CURB AND GUTTER SHALL NOT CAUSE PUDDLING ISSUES. PUDDLE WIDTH SHALL NOT EXCEED 75% OF THE GUTTER WIDTH AND PUDDLE LENGTH SHALL NOT EXCEED 2'.
 - 7.2. CROSS SLOPE OF THE GUTTER SHALL BE A MINIMUM OF 1.5% AND A MAXIMUM OF 3.5% AS MEASURED BY A 2' ELECTRONIC LEVEL.
 - 7.3. THERE SHALL BE NO BREAK POINTS OR REVERSE SLOPE IN THE GUTTER CROSS SLOPE AS MEASURED BY A 2' ELECTRONIC LEVEL. THE MAXIMUM ALLOWABLE GAP BETWEEN THE LEVEL AND THE GUTTER SURFACE SHALL BE 1/8".
 - 7.4. WHEN A 6 FOOT LONG STRAIGHTEDGE IS LAID ON THE TOP FACE OF THE CURB OR ON THE SURFACE OF THE GUTTER, THE SURFACE SHALL NOT VARY MORE THAN 0.02 FEET FROM THE EDGE OF THE STRAIGHTEDGE EXCEPT AT GRADE CHANGES OR VERTICAL CURVES.
 - 7.5. THE CONTRACTOR SHALL CONSTRUCT ALL CURB AND GUTTER WITHIN 0.02 FEET OF TRUE LINE, WITHIN 0.02 FEET OF ESTABLISHED SURFACE GRADE, CROSS SECTION, AND SLOPE, AND WITHIN 0.02 FEET OF SPECIFIED THICKNESS.
8. WHEN INSTALLING NEW CURB AND GUTTER ON AN EXISTING PAVED STREET, THE ASPHALT SHALL BE CUT A MINIMUM OF 2 FEET FROM THE EDGE OF THE GUTTER OR AS NEEDED TO ACHIEVE COMPACTION OF THE AGGREGATE BASE BY MECHANICAL MEANS.



STANDARD CURB AND GUTTER

NTS



CURB AND GUTTER AT DRIVEWAY

NTS

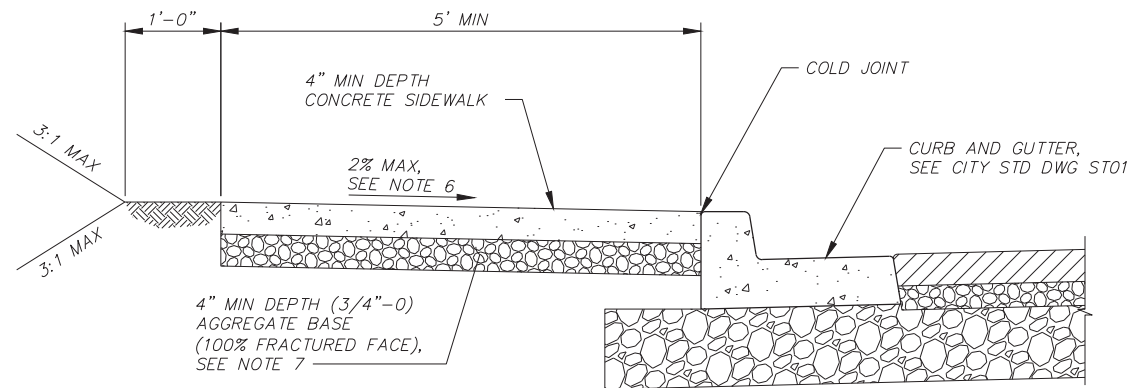


Where Life is Sweet

CITY OF
HERMISTON, OREGON
STANDARD DRAWINGS

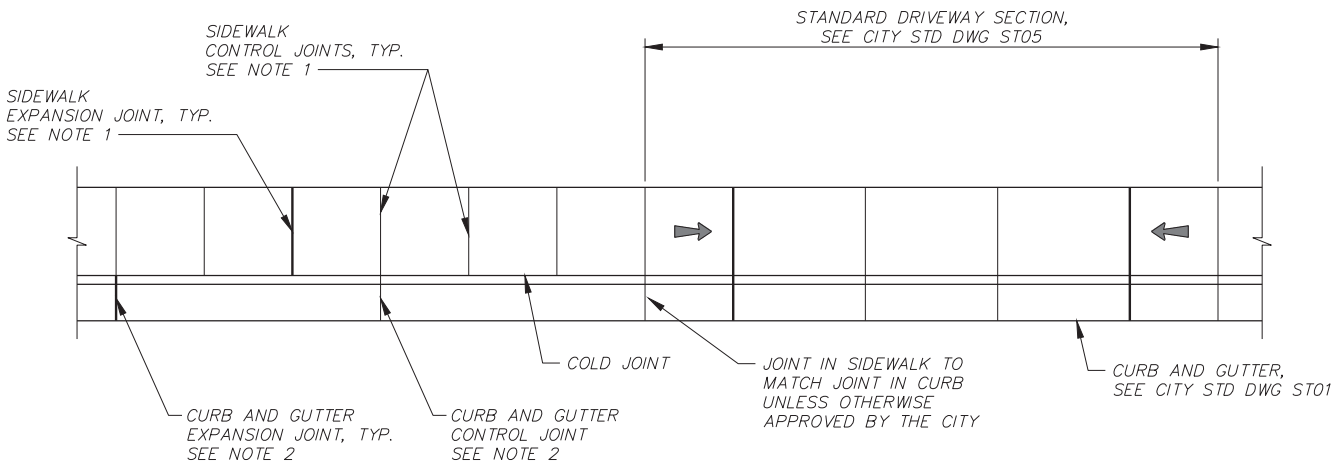
CURB AND GUTTER

FIGURE
ST01



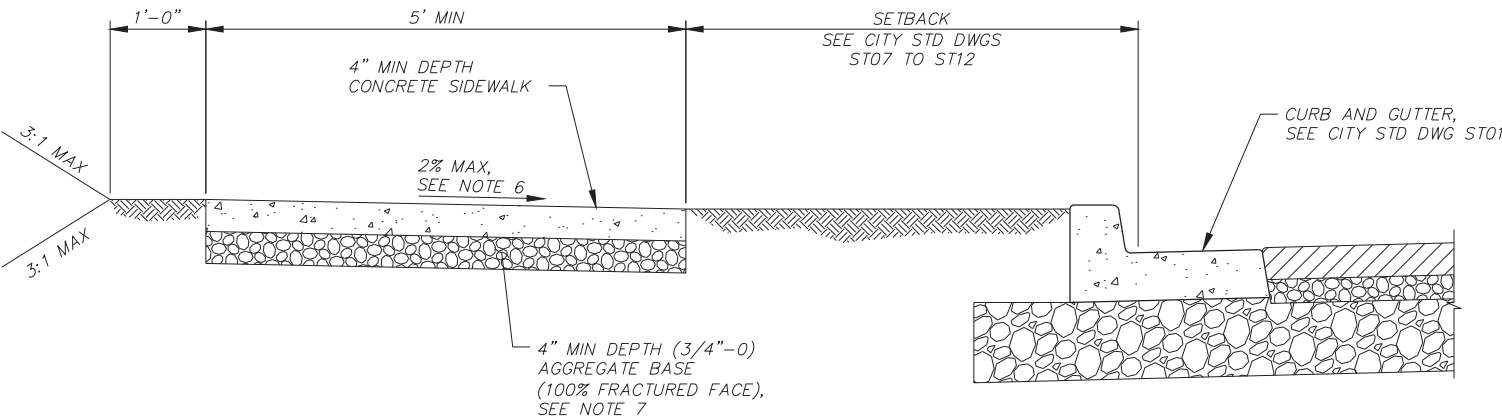
SIDEWALK SECTION

NTS



TYPICAL PLAN VIEW

NTS



SIDEWALK SECTION WITH SETBACK

NTS

NOTES

- SIDEWALK JOINT REQUIREMENTS:
 - EXPANSION JOINT SPACING SHALL BE 45' ON CENTER MAXIMUM, AND SHALL BE PROVIDED AROUND POLES, BOXES, AND ANY FIXTURES WHICH PROTRUDE THROUGH THE SIDEWALK, BETWEEN ANY STRUCTURE IMMEDIATELY ADJACENT TO THE SIDEWALK, AND AT THE ENDS OF EACH DRIVEWAY (AS DETERMINED BY DRIVEWAY WIDTH).
 - FOR SIDEWALKS LESS THAN 8' WIDE, TRANSVERSE CONTROL JOINT SPACING SHALL BE EQUAL TO THE WIDTH OF THE SIDEWALK EXCLUDING THE CURB. FOR SIDEWALKS 8' OR GREATER IN WIDTH TRANSVERSE CONTROL JOINT SPACING SHALL BE EQUAL TO HALF THE WIDTH OF THE SIDEWALK AND A LONGITUDINAL CONTROL JOINT SHALL BE CONSTRUCTED AT THE CENTER OF THE SIDEWALK.
- CURB AND GUTTER JOINT REQUIREMENTS:
 - EXPANSION JOINTS ARE OPTIONAL WHEN USED. SPACING SHALL BE 200' ON CENTER MAXIMUM AND SHALL BE PLACED AT POINTS OF TANGENCY, AT BOTH ENDS OF EACH DRIVEWAY, AT CONNECTION TO EXISTING CURB/GUTTER, AND ADJACENT TO ANY STRUCTURE.
 - CONTROL JOINT SPACING SHALL BE 15' ON CENTER MAXIMUM, AND SHALL BE PLACED AT THE ENDS OF EACH STORMWATER INLET AND CURB RAMP.
- CONCRETE DEPTH FOR SIDEWALKS SHALL BE 4" MIN, SEE ST05 FOR DRIVEWAY THICKNESS.
- BROOM FINISH ALL SIDEWALKS AND DRIVEWAYS.
- WHERE NEW SIDEWALK CONSTRUCTION ABUTS AN EXISTING SIDEWALK NOT MEETING CITY STANDARDS, NEW SIDEWALK CONSTRUCTION MUST TRANSITION TO MEET CITY STANDARDS WITHIN A MAXIMUM DISTANCE EQUAL TO THE WIDTH OF THE SIDEWALK EXCLUDING THE CURB.
- TO ENSURE SLOPES DO NOT EXCEED MAXIMUM ALLOWABLE SLOPES, IT IS RECOMMENDED TO SET SLOPES LOWER THAN MAXIMUM ALLOWABLE SLOPES AS FOLLOWS:
 - FOR 2% MAX, SET SLOPE AT 1.5%
 - FOR 8.33% MAX, SET SLOPE AT 7.5%SIDEWALK WITH SLOPES EXCEEDING MAXIMUM ALLOWABLE VALUES SHALL BE REMOVED AND REPLACED AT THE CONTRACTORS EXPENSE.
- COMPACT AGGREGATE BASE (100% FRACTURED FACE) TO 95% MAXIMUM DRY DENSITY PER ASTM D1557.
- ALL CONCRETE SHALL BE COMMERCIAL GRADE 4,000 PSI CONCRETE.



CITY OF
HERMISTON, OREGON
STANDARD DRAWINGS

SIDEWALK

FIGURE
ST04



Where Life is Sweet

Mayor and Members of the City Council
STAFF REPORT
 For the Meeting of August 11, 2025

Title/Subject

Resolution 2388- Declare Intent to Form LID for W Dogwood

Summary and Background

Resolution 2384 discusses the need for improvements to W Dogwood to accommodate increased traffic accessing businesses between 2nd & Spruce.

A report has been developed which contains the relevant information regarding scope, cost, impacted properties, methodology and estimated cost to each property, and it is included with this staff report.

Resolution 2388 declares the City Council's intent to form the LID as outlined, and sets a date for a public hearing for the September 8, 2025 City Council meeting. If Resolution 2388 is approved, then the two impacted properties shall immediately be mailed notice of the hearing date, as well as informing them of the project scope, estimated cost, methodology for assigning costs, estimated amount assigned to their property, and rights available to submit written comment and formal remonstrance.

Although Resolution 2388 is technically declaring "intent to form," it does not form the LID. Upon receipt of public comment and/or remonstrances at the September 8 meeting, City Council may choose to cease LID formation.

Tie-In to Council Goals

Transportation: Improve Mobility and Transportation

Fiscal Information

The LID as presented allocates all costs to the benefitting properties. No taxpayer contribution is considered.

This resolution does not actually form an LID, or create any obligation whatsoever on to property owners. This resolution simply sets a public hearing date at which point Council will consider whether or not to actually form the LID.

Alternatives and Recommendation

Alternatives

1. Approve resolution 2388
2. Reject resolution 2388

Recommended Action/Motion

Motion to approve Resolution 2388

Submitted By:

Mark Morgan

RESOLUTION NO. 2388

A RESOLUTION ESTABLISHING THE INTENT TO FORM LOCAL IMPROVEMENT DISTRICT IN THE VICINITY OF W DOGWOOD AVENUE BETWEEN TAX LOT 1500 AND TAX LOT 3300 AND SETTING A DATE FOR PUBLIC HEARING

WHEREAS, on August 11, 2025, the City Manager of the City of Hermiston, Oregon, under the direction and by requirement of the council, did prepare and have on file in his office plans and specifications for the formation of a Local Improvement District, a copy of which is attached as Exhibit A for the installation of curb, gutter, sidewalk and paving of:

W Dogwood Avenue between Tax Lot 1500 and Tax Lot 3300, a distance of approximately 50 linear feet.

WHEREAS, Exhibit A also includes: a map of the proposed local improvement district boundary, including the address, map and tax lot and ownership information for each property within the boundary; an estimate of the actual cost of the improvements; and the proposed methodology for establishing special benefit and levying assessments, the estimated assessment to be levied against each property and a description of funds other than assessments that will be used to fund the local improvement; and

WHEREAS, the council has examined Exhibit A and finds the same satisfactory and the estimate therefore to be in accordance with the probable cost of such work; and

WHEREAS, the property recommended by the City Manager to be included within the boundaries of the district benefitted is in the judgement of the council proper to be included therein and no property is excluded therefrom which should properly be included therein; and

WHEREAS, the installation of the above described paving and utilities is at this time necessary.

NOW, THEREFORE, THE CITY OF HERMISTON RESOLVES AS FOLLOWS:

That it is expedient to improve, and it is hereby proposed, to curb, gutter, sidewalk, and pave W Dogwood Avenue between Tax Lot 1500 and Tax Lot 3300, a distance of approximately 50 linear feet in accordance with Exhibit A, which is hereby adopted and approved.

1. That the total estimated cost identified in Exhibit A as \$72,000 shall be assessed as a local improvement district.
2. That the proposed improvement district is to be known as "LOCAL IMPROVEMENT DISTRICT NO. 325"; embracing the property benefitted and assessed for the payment of such improvements; which improvement district shall include all lots, parts of lots and parcels of land lying and being within the district described in Exhibit A.

3. That at least ten days prior to the public hearing on formation, the City Manager shall mail or personally deliver notice to the owners of each property proposed to be located within the local improvement district. In addition to the date, time and location of the hearing, the notice shall include:
 - a. A copy of Exhibit A, which includes a general description of the improvements, the boundary of the district, the addresses of each property included, the estimated cost of the proposed local improvement, the methodology for levying assessments and the estimated assessment for each property.
 - b. A statement that these are estimates only, and that the final assessment may be greater.
 - c. A statement that any interested person may testify or submit written comments on the proposed local improvement district and that any property owner may formally object by submitting a written remonstrance stating the reason for the objection and signed by each owner of the property and that the remonstrance must be received by the City Manager no later than 5:00 p.m. on the business day before the public hearing.
 - d. A form for remonstrance.
4. That a public hearing is set for the 8th day of September 2025 at or after 7:00 p.m. at the City Hall (180 NE 2nd St.) in Hermiston, Oregon at which time the Council shall provide a reasonable opportunity for persons to testify and shall consider any written comments received with any remonstrances on such proposed improvement.
5. That this resolution is effective immediately upon its passage.

PASSED by the Common Council this 11th day of August 2025.

SIGNED by the Mayor this 11th day of August 2025.

Doug Primmer, MAYOR

ATTEST:

Heather KP La Beau, ASSISTANT CITY RECORDER

**W. DOGWOOD AVENUE
LOCAL IMPROVEMENT DISTRICT NO. 325
ENGINEERING REPORT**

AUGUST 2025



Prepared for the
City of Hermiston, Oregon

**W. DOGWOOD AVENUE
LOCAL IMPROVEMENT DISTRICT NO. 325
ENGINEERING REPORT**

FOR

CITY OF HERMISTON, OREGON

2025



ANDERSON PERRY & ASSOCIATES, INC.

**La Grande, Redmond, Hermiston, and Enterprise, Oregon
Walla Walla, Washington**

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Section 1 - Project Description

General

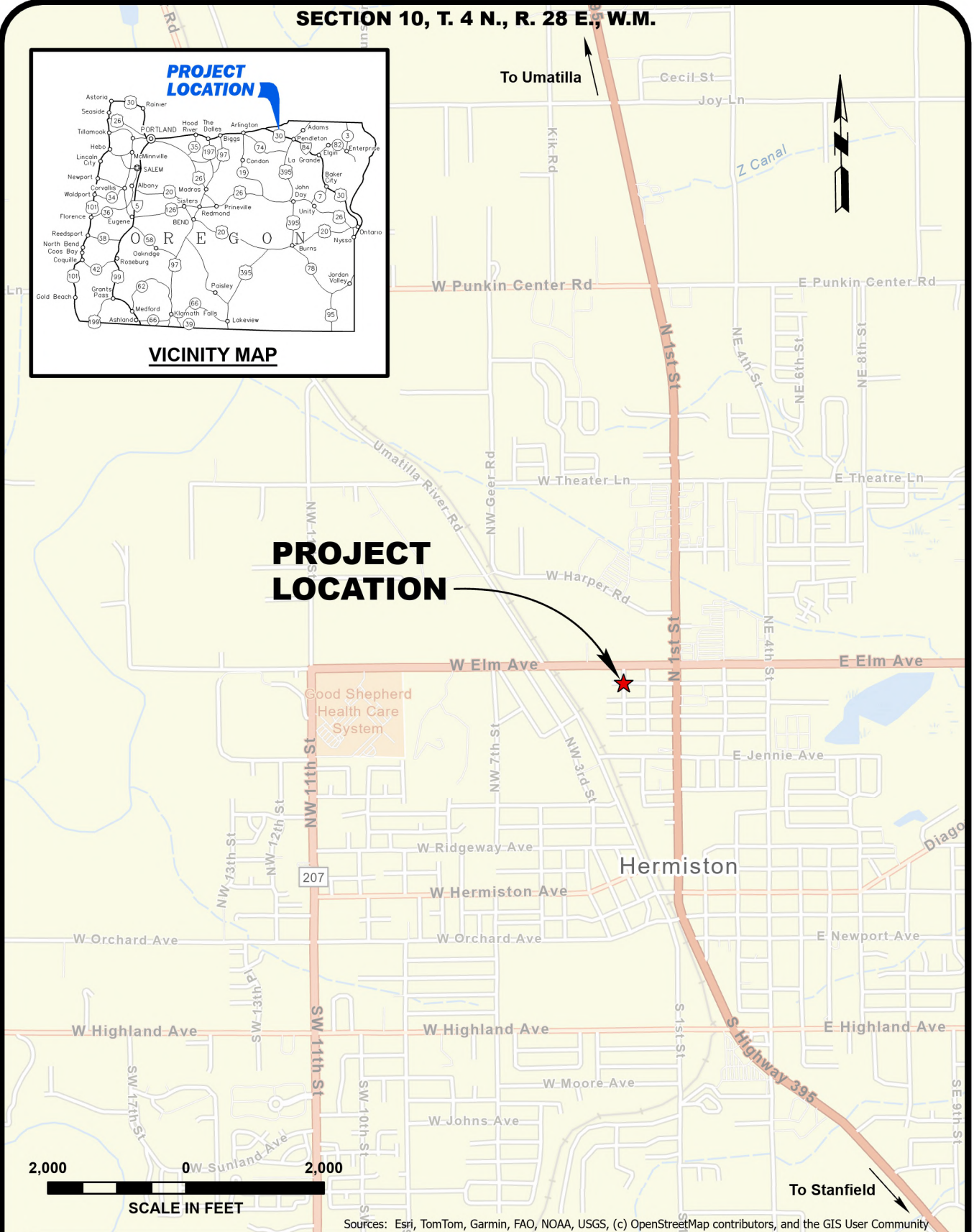
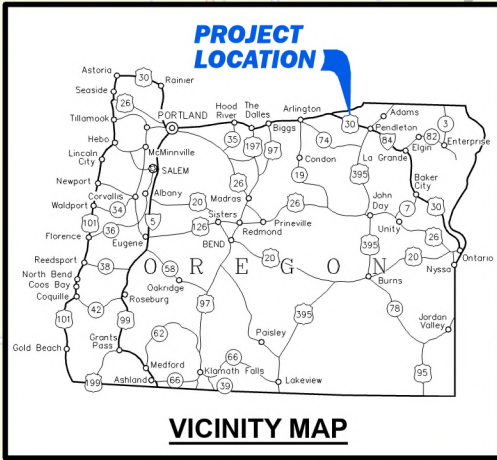
The City of Hermiston, Oregon, is considering the formation of a Local Improvement District (LID) No. 325 for W. Dogwood Avenue to provide a portion of the street improvements in anticipation of increased traffic due to upcoming improvements to Highway 207 (W. Elm Avenue). Two local improvement districts, including this LID and LID No. 324, will be used to construct improvements to W. Dogwood Avenue. These improvements would generally include replacing existing gravel streets with urban local residential streets.

Project Description

The proposed LID location is shown on Figure 1-1, Location and Vicinity Maps, and an aerial of the proposed LID boundary is shown on Figure 1-2, Aerial Photograph. The proposed LID consists of two properties, comprising a total of 100 feet of frontage. A 40-foot wide existing right-of-way is available for the construction of the street improvements. The W. Dogwood Avenue LID tax map with lot lines and parcel sizes is included in Appendix A.

The improvements for the proposed LID are shown in Figure 1-3, Proposed Improvements. The improvements would consist of approximately 50 linear feet of street improvement, including 5-foot sidewalks, curb and gutter, two 14-foot travel lanes, and stormwater facilities. “No Parking” signs would be located on both sides of the street. The pavement section would consist of 3 inches of asphalt concrete pavement on 3 inches of 3/4”-0 aggregate base and 6 inches of 1”-0 aggregate base. These proposed improvements are based on a planning-level design and are preliminary in nature. A typical street cross section of the proposed street improvements, based on City Standard Drawing ST11 Local Residential - Option 1, is shown on Figure 1-4. Curb and sidewalk standard drawings are included in Appendix B. A property benefit analysis is included in Chapter 3, with property boundaries current as of July 2025.

SECTION 10, T. 4 N., R. 28 E., W.M.



**CITY OF
HERMISTON, OREGON**
W. DOGWOOD AVE. LOCAL IMPROVEMENT DISTRICT NO. 325
ENGINEERING REPORT

**FIGURE
1-1**

LOCATION AND VICINITY MAPS

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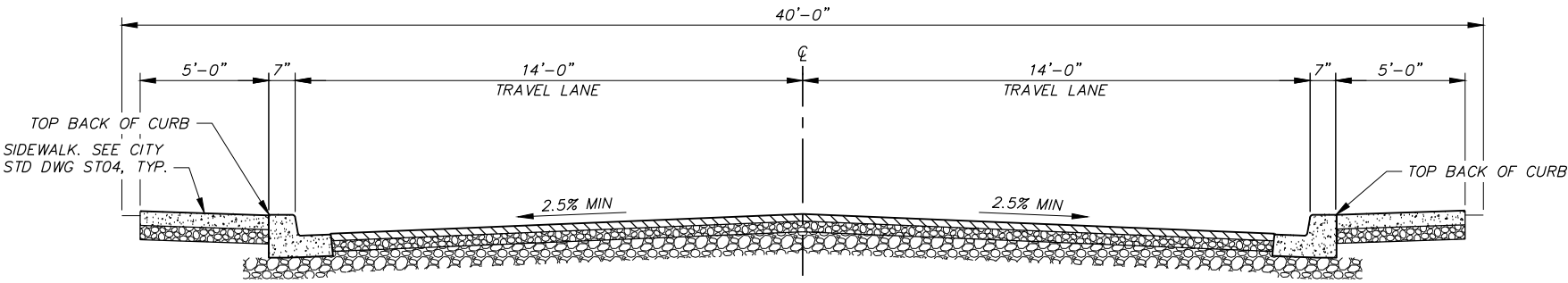


CITY OF
HERMISTON, OREGON
W. DOGWOOD AVE. LOCAL IMPROVEMENT DISTRICT NO. 325
ENGINEERING REPORT
AERIAL PHOTOGRAPH

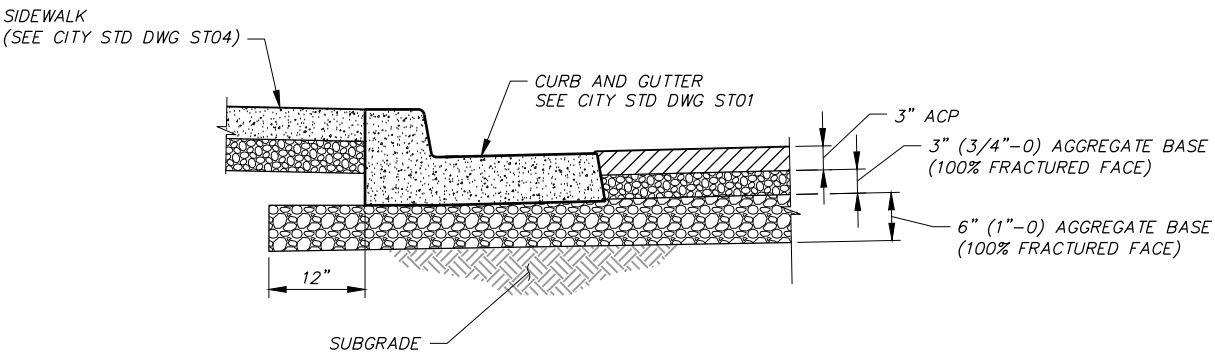
FIGURE
1-2

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STREET CROSS SECTION
NTS



PAVEMENT SECTION
NTS

Section 2 - Preliminary Feasibility Analysis

Street Improvement

The proposed street improvements required for the W. Dogwood Avenue Local Improvement District (LID) No. 325 shown in Figures 1-3 and 1-4 are feasible; however, they present certain challenges. The streets have been graded and graveled within the existing 40-foot right-of-way (ROW). The ROW width is narrower than the city standard ROW width of 50 feet. The street section shown on Figure 1-4 will be used to fit within the existing ROW. Additionally, the power poles near the edge of the ROW would need to be considered during engineering design. If the power poles are confirmed to be in the proposed sidewalk, they would need to be relocated during construction to provide a minimum 4-foot wide accessible path. Nevertheless, no significant obstacles are preventing the proposed street improvements.

Other Utilities

Water, sewer, communications, and electricity are currently available to the properties in the proposed LID.

Project Cost

A preliminary cost estimate was prepared as part of the feasibility analysis and is shown on Figure 2-1, Preliminary Cost Estimate. Due to the preliminary nature of the cost estimate, a 20 percent construction contingency has been included to account for unforeseen issues and potential variability in the bidding environment when the improvements are to be constructed. As shown, the estimate includes anticipated engineering as a percentage of construction costs. The preliminary cost estimate assumes construction of the improvements would occur in 2026; an anticipated annual inflation of 5 percent was assumed and is shown on the cost estimate. The cost estimate would need to be reviewed and potentially adjusted should the street improvements project move to the design and construction phases.

CITY OF HERMISTON, OREGON
W. DOGWOOD AVENUE
LOCAL IMPROVEMENT DISTRICTS NO. 325
PRELIMINARY COST ESTIMATE
August 6, 2025

NO.	DESCRIPTION	UNIT	UNIT PRICE	ESTIMATED QUANTITY	TOTAL PRICE
1	Mobilization/Demobilization	LS	\$ 4,250	All Req'd	\$ 4,250
2	Temporary Protection and Direction of Traffic/Project Safety	LS	1,300	All Req'd	1,300
3	Erosion and Sediment Control	LS	600	All Req'd	600
4	Removal of Structures and Obstructions*	LS	2,000	All Req'd	2,000
5	General Earthwork	LS	1,000	All Req'd	1,000
6	3/4"-0 Aggregate Base (3 inches)	CY	85	20	1,700
7	1"-0 Aggregate Base (6 inches)	CY	85	40	3,400
8	3-inch Asphalt Concrete Pavement	TON	175	30	5,250
9	Concrete Curb and Gutter	LF	50	100	5,000
10	Americans with Disabilities Act Ramp	EA	3,750	-	-
11	Concrete Driveway Approaches	SF	18	-	-
12	Concrete Sidewalk	SF	14	500	7,000
13	Surface Restoration	LS	250	All Req'd	250
14	Permanent Striping and Signing	LS	250	All Req'd	250
Estimated Construction Cost					\$ 32,000
Construction Contingency					10,000
Design Engineering					10,000
Construction Engineering					10,000
Total Year 2025 Estimated Project Cost					\$ 62,000
Inflation at Time of Construction** (Assumed Construction Year 2026)					10,000
TOTAL YEAR 2026 ESTIMATED IMPROVEMENTS COST					\$ 72,000

*Power pole relocation is not included in this cost estimate since its location relative to the right-of-way has not been determined.

**Construction cost inflation is assumed to be 5 percent annually until construction occurs.

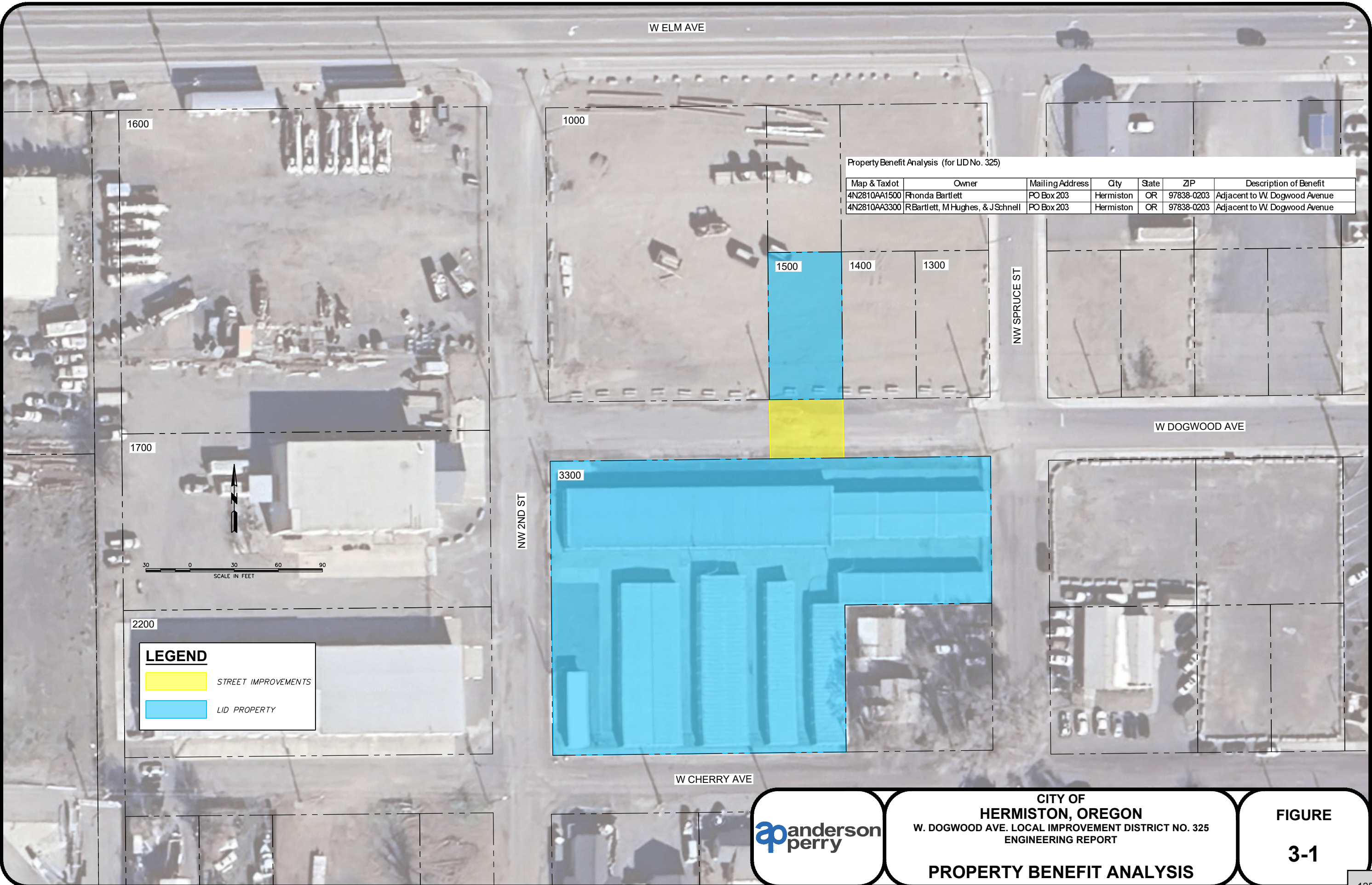
Section 3 - Property Benefit Analysis

Hermiston Municipal Code 157.164

Figure 3-1, Property Benefit Analysis, includes property identification information, ownership information, and a description of the benefit that each property would receive from the proposed improvements. For all properties in this W. Dogwood Avenue Local Improvement District (LID) No. 325, the benefit to each property is that the street infrastructure that would be installed for the proposed development area would be in conformance with Hermiston Municipal Code 157.164. The proposed street improvements would ensure each tax lot has paved access to and from the remainder of the street system.

Section 4 of this LID Engineering Report outlines the methodology for assigning costs, which would be based on linear feet (LF) of frontage adjacent to the street improvements. Since no off-site improvements are included in this LID Engineering Report, the benefit that each property would receive is directly proportional to the LF of improvements installed.

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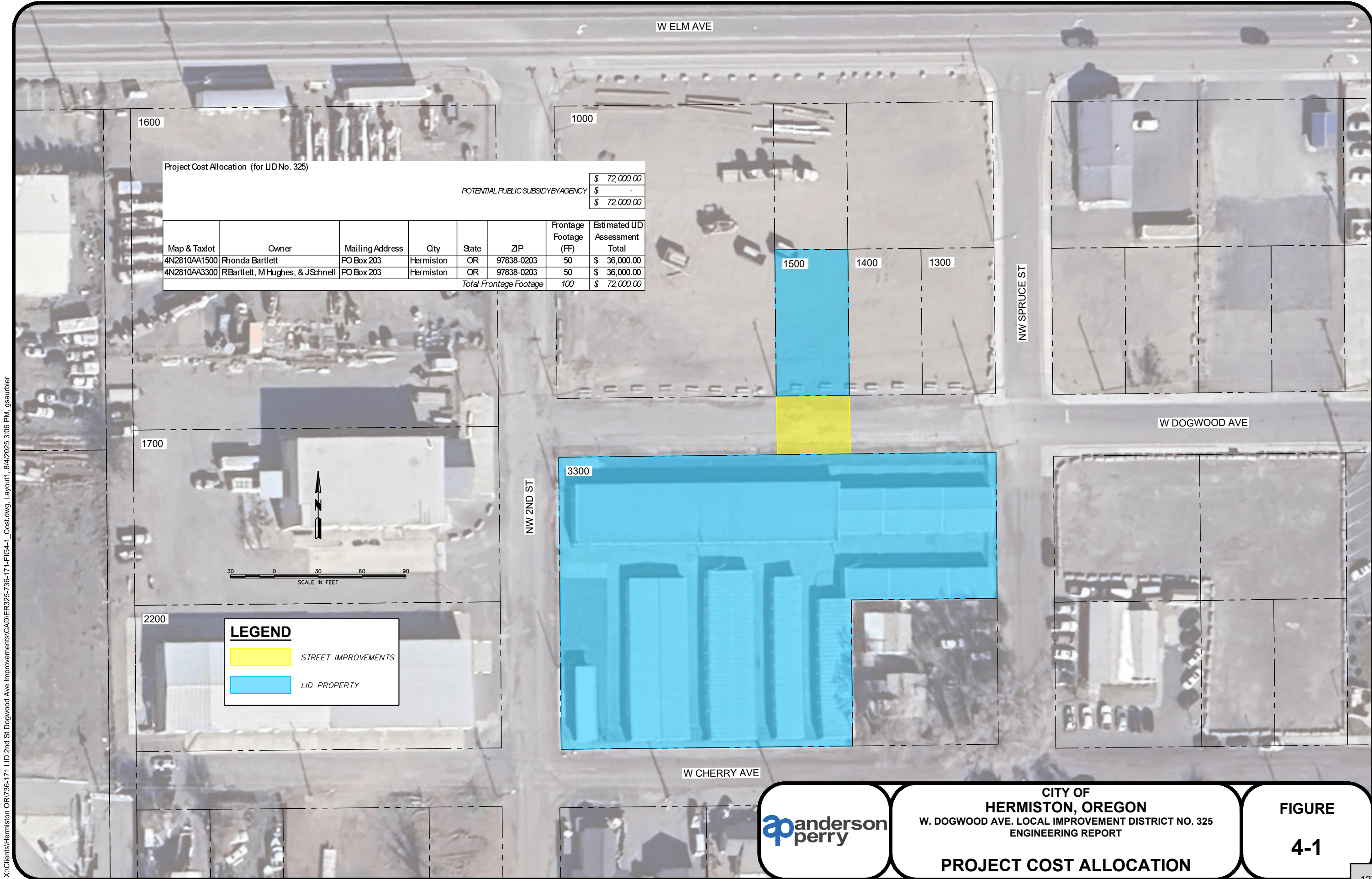
Section 4 - Project Cost Allocation

Methodology

The street improvement project included in the proposed W. Dogwood Avenue Local Improvement District (LID) No. 325 lends itself to allocating costs based on each property's frontage along the street improvements. The property frontage length along the proposed street improvements relative to total property frontage on the proposed street improvements for all properties in the LID is directly proportional to the assessment of each property. This method would distribute the costs of the LID equitably. Figure 4-1, Project Cost Allocation, shows each affected property, its associated frontage length, and its associated estimated LID assessment.

Description of Other Funds

The project cost is anticipated to be completely covered by the owners of the properties within the LID, so no additional funds would be required.



X:\Clients\Hermiston OR\736-171 LID 2nd St Dogwood Ave Improvements\CAD\ER325-736-171-FIG4-1_Cost.dwg, Layout1, 8/4/2025 3:06 PM, gsaubier

Section 5 - Property Valuation and Assessment Estimate

General

The valuations for each property shown in Table 5-1 below represent the latest figures available from the Umatilla County Assessor's office. Table 5-1 shows an estimate of the amount to be assessed for each property in the W. Dogwood Avenue Local Improvement District (LID) No. 325. These amounts represent the total current value of the assessments without regard for potential long-term repayment plans and associated interest fees. An explanation of options for long-term payment plans is available below. A full accounting of how the assessment was calculated, using the methodology outlined in Section 4, is shown on Figure 4-1, Project Cost Allocation. Actual costs would be determined after construction is completed and final project costs are known.

TABLE 5-1
UMATILLA COUNTY ASSESSOR'S PROPERTY VALUATIONS

Map and Tax Lot	Assessed Value	Real Market Value	Outstanding Tax Assessments	Estimated LID Assessment Total
4N2810AA1500	\$10,560.00	\$42,530.00	\$0.00	\$36,000.00
4N2810AA3300	\$662,380.00	\$1,502,500.00	\$0.00	\$36,000.00

Long-term Repayment Plan

Construction of the proposed improvements considered in the LID would be coordinated by the City of Hermiston on behalf of each property. Each property owner would have the option to pay their LID assessment in its entirety, or they may choose to make installment payments over ten years. The City intends to finance the improvements upfront through the most cost-effective means available. Any costs associated with carrying costs of financing the proposed improvements would be passed on directly to the benefiting properties; however, the City would not add on any administrative or overhead fees associated with obtaining and managing the financing instrument.

Appendices Table of Contents

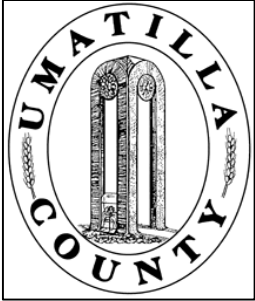
Appendix A - Tax Lot Map

Appendix B - Standard Drawings

APPENDIX A

Tax Lot Map

This map was prepared for Assessment & Taxation purposes only and was NOT prepared nor is it suitable for legal, engineering or surveying purposes.

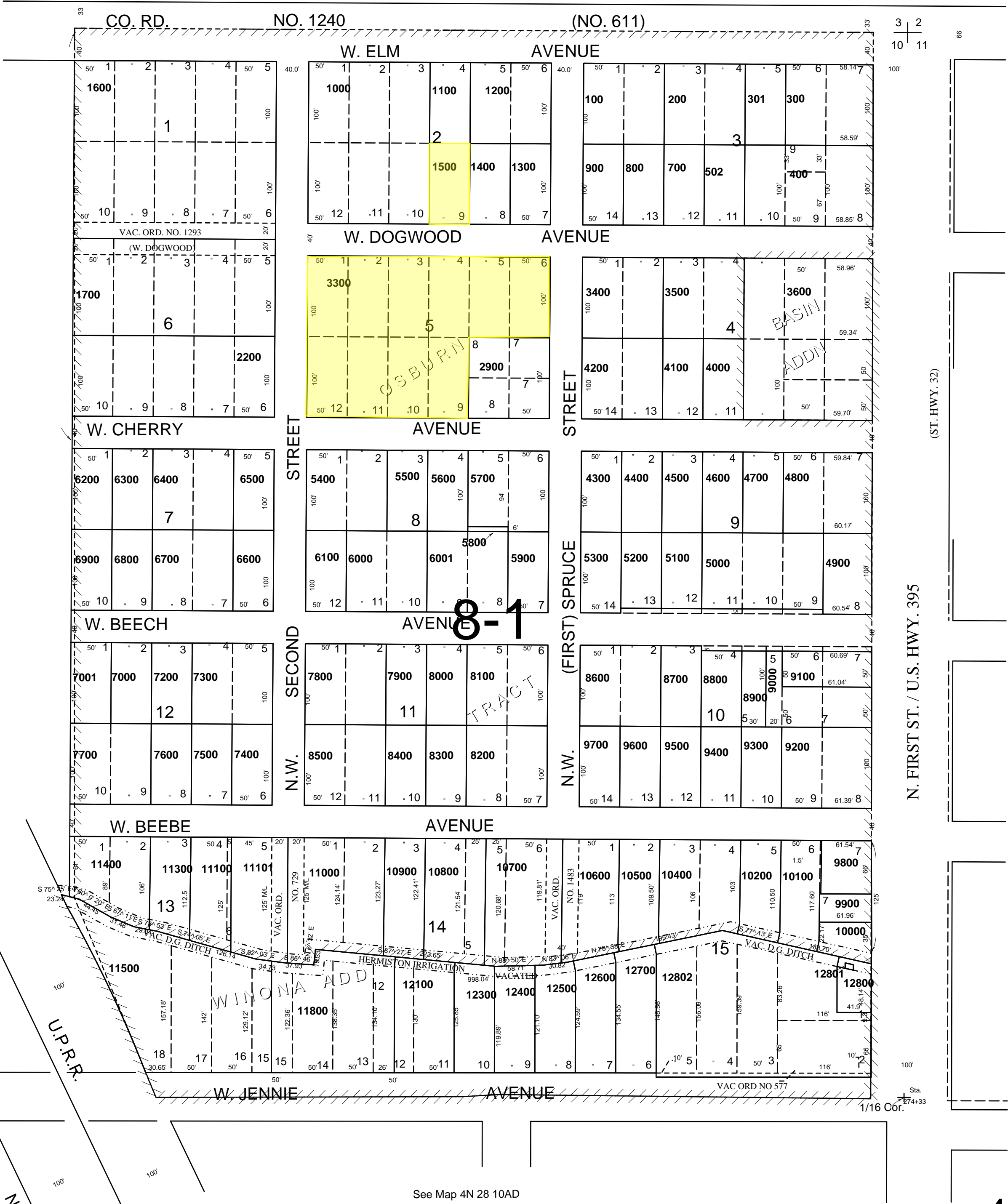


NE1/4 NE1/4 SEC 10 T4N R28E WM
UMATILLA COUNTY, OR

SCALE: 1" = 100'

4N2810AA

See Map 4N 03D



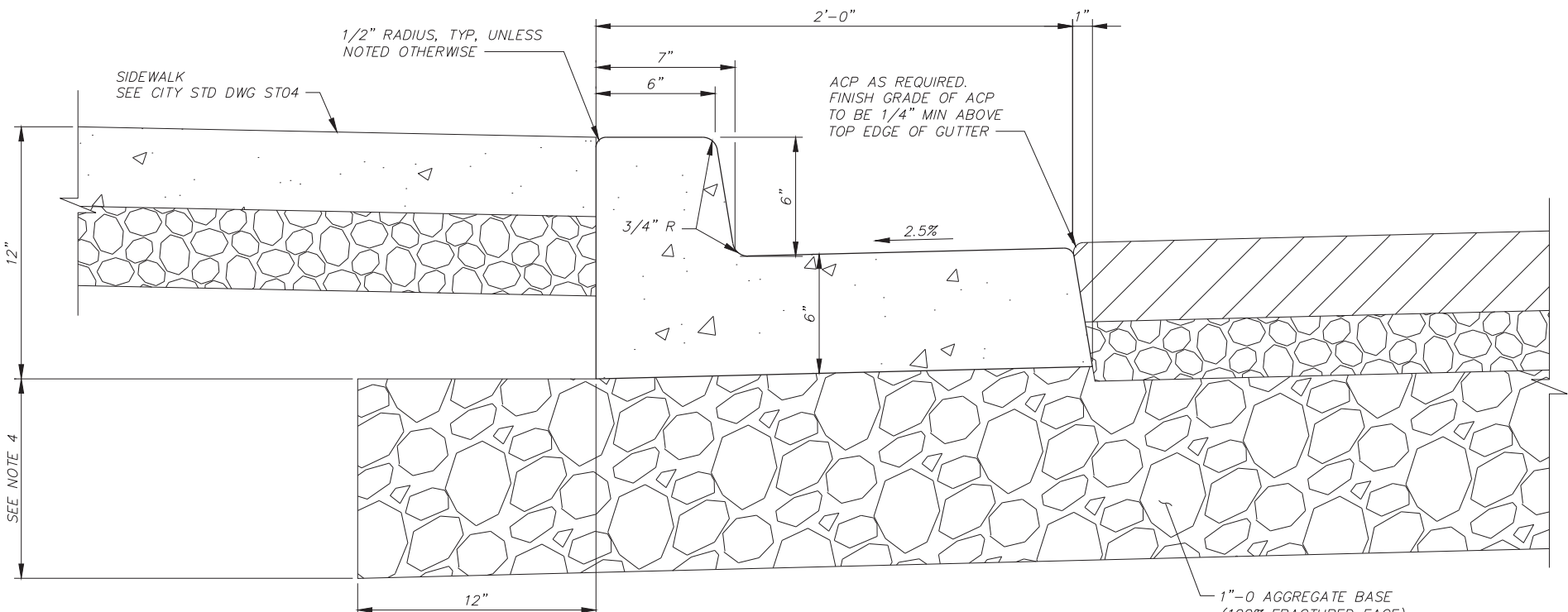
LID Boundary

APPENDIX B

Standard Drawings

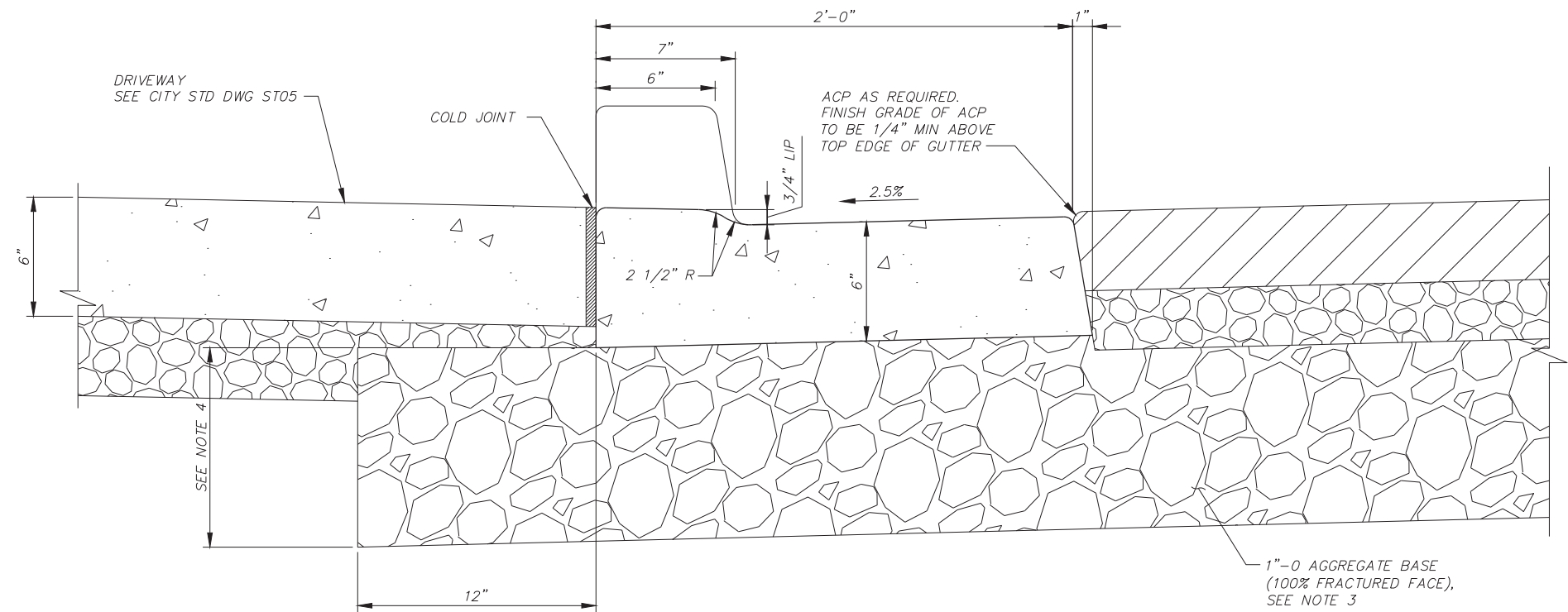
NOTES

1. GUTTER CROSS SLOPE SHALL BE MINIMUM 2% AND MAXIMUM 3% UNLESS OTHERWISE APPROVED BY THE CITY.
2. ALL CONCRETE SHALL BE COMMERCIAL GRADE 4,000 PSI CONCRETE.
3. COMPACT AGGREGATE BASE TO 95% MAXIMUM DRY DENSITY PER ASTM D1557.
4. AGGREGATE BASE THICKNESS BELOW CURB AND GUTTER SHALL BE 6" THICK OR CONFORM TO CITY STD DWG ST07-ST11, WHICHEVER IS GREATER.
5. SEE CITY STD DWG ST04 FOR CURB AND GUTTER JOINT REQUIREMENTS.
6. CONCRETE SHALL BE TROWELED SMOOTH PRIOR TO APPLYING BROOM FINISH.
7. INSPECTION CRITERIA:
 - 7.1. COMPLETED CURB AND GUTTER SHALL NOT CAUSE PUDDLING ISSUES. PUDDLE WIDTH SHALL NOT EXCEED 75% OF THE GUTTER WIDTH AND PUDDLE LENGTH SHALL NOT EXCEED 2'.
 - 7.2. CROSS SLOPE OF THE GUTTER SHALL BE A MINIMUM OF 1.5% AND A MAXIMUM OF 3.5% AS MEASURED BY A 2' ELECTRONIC LEVEL.
 - 7.3. THERE SHALL BE NO BREAK POINTS OR REVERSE SLOPE IN THE GUTTER CROSS SLOPE AS MEASURED BY A 2' ELECTRONIC LEVEL. THE MAXIMUM ALLOWABLE GAP BETWEEN THE LEVEL AND THE GUTTER SURFACE SHALL BE 1/8".
 - 7.4. WHEN A 6 FOOT LONG STRAIGHTEDGE IS LAID ON THE TOP FACE OF THE CURB OR ON THE SURFACE OF THE GUTTER, THE SURFACE SHALL NOT VARY MORE THAN 0.02 FEET FROM THE EDGE OF THE STRAIGHTEDGE EXCEPT AT GRADE CHANGES OR VERTICAL CURVES.
 - 7.5. THE CONTRACTOR SHALL CONSTRUCT ALL CURB AND GUTTER WITHIN 0.02 FEET OF TRUE LINE, WITHIN 0.02 FEET OF ESTABLISHED SURFACE GRADE, CROSS SECTION, AND SLOPE, AND WITHIN 0.02 FEET OF SPECIFIED THICKNESS.
8. WHEN INSTALLING NEW CURB AND GUTTER ON AN EXISTING PAVED STREET, THE ASPHALT SHALL BE CUT A MINIMUM OF 2 FEET FROM THE EDGE OF THE GUTTER OR AS NEEDED TO ACHIEVE COMPACTION OF THE AGGREGATE BASE BY MECHANICAL MEANS.



STANDARD CURB AND GUTTER

NTS



CURB AND GUTTER AT DRIVEWAY

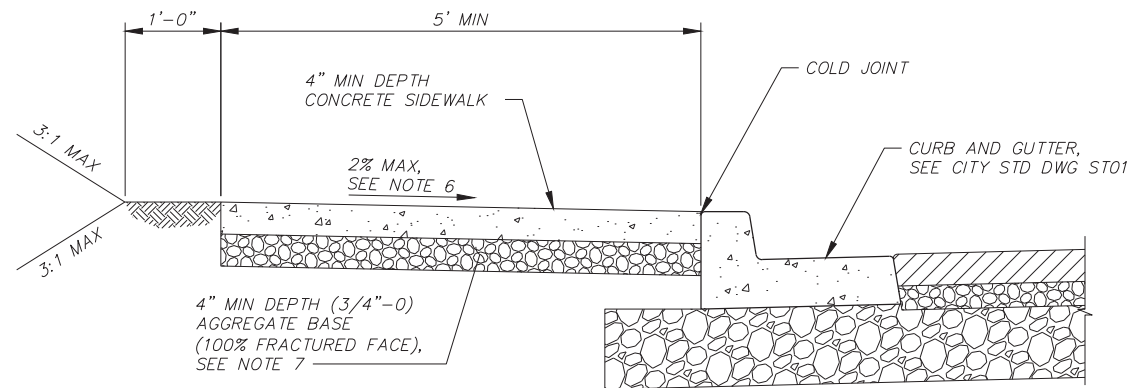
NTS



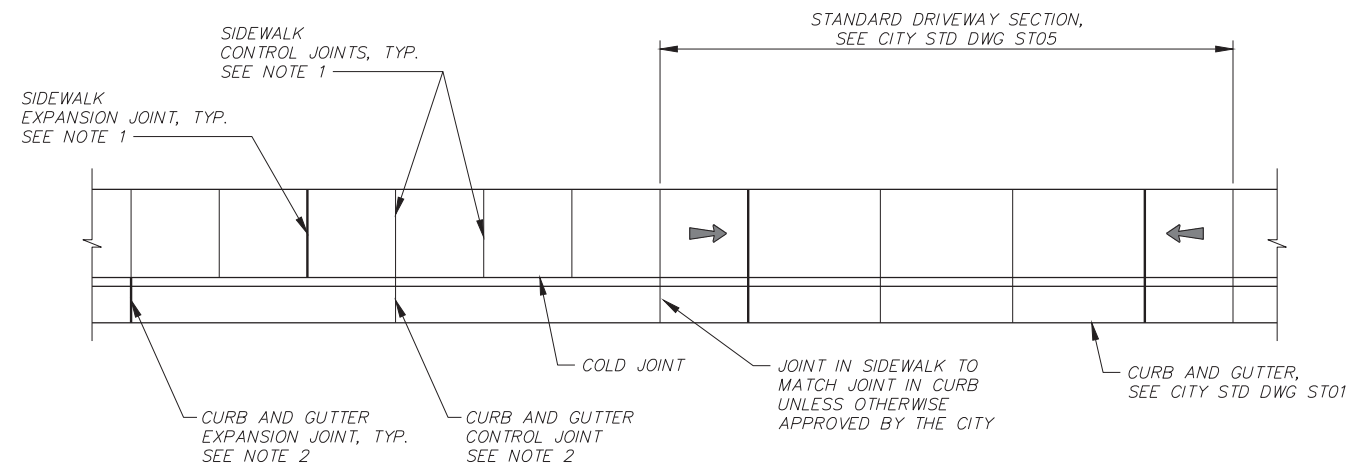
CITY OF
HERMISTON, OREGON
STANDARD DRAWINGS

CURB AND GUTTER

FIGURE
ST01



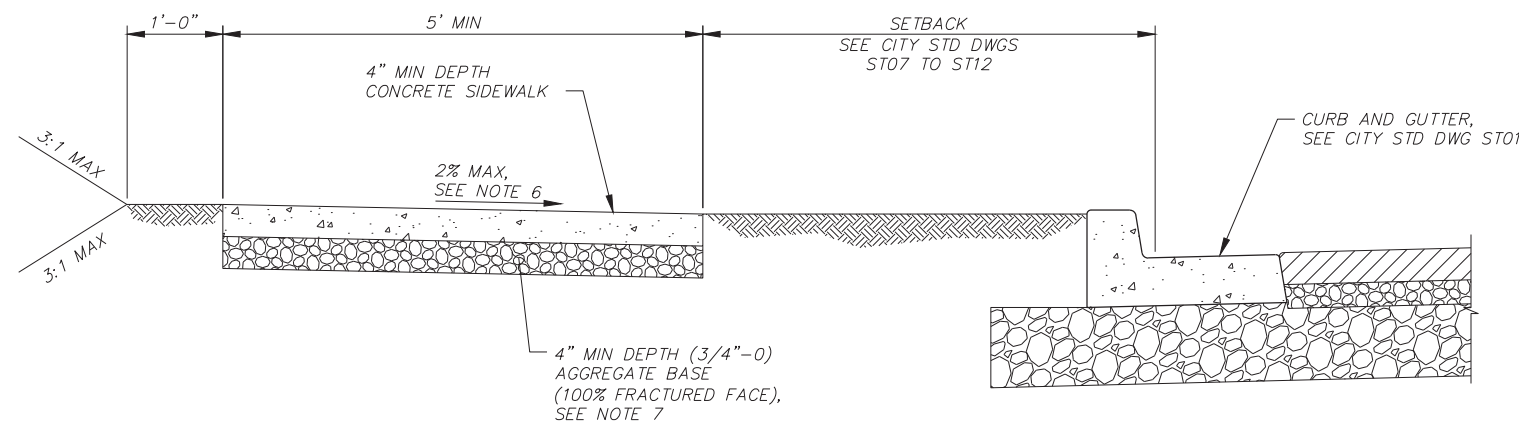
SIDEWALK SECTION
NTS



TYPICAL PLAN VIEW
NTS

NOTES

- SIDEWALK JOINT REQUIREMENTS:
 - EXPANSION JOINT SPACING SHALL BE 45' ON CENTER MAXIMUM, AND SHALL BE PROVIDED AROUND POLES, BOXES, AND ANY FIXTURES WHICH PROTRUDE THROUGH THE SIDEWALK, BETWEEN ANY STRUCTURE IMMEDIATELY ADJACENT TO THE SIDEWALK, AND AT THE ENDS OF EACH DRIVEWAY (AS DETERMINED BY DRIVEWAY WIDTH).
 - FOR SIDEWALKS LESS THAN 8' WIDE, TRANSVERSE CONTROL JOINT SPACING SHALL BE EQUAL TO THE WIDTH OF THE SIDEWALK EXCLUDING THE CURB. FOR SIDEWALKS 8' OR GREATER IN WIDTH TRANSVERSE CONTROL JOINT SPACING SHALL BE EQUAL TO HALF THE WIDTH OF THE SIDEWALK AND A LONGITUDINAL CONTROL JOINT SHALL BE CONSTRUCTED AT THE CENTER OF THE SIDEWALK.
- CURB AND GUTTER JOINT REQUIREMENTS:
 - EXPANSION JOINTS ARE OPTIONAL WHEN USED. SPACING SHALL BE 200' ON CENTER MAXIMUM AND SHALL BE PLACED AT POINTS OF TANGENCY, AT BOTH ENDS OF EACH DRIVEWAY, AT CONNECTION TO EXISTING CURB/GUTTER, AND ADJACENT TO ANY STRUCTURE.
 - CONTROL JOINT SPACING SHALL BE 15' ON CENTER MAXIMUM, AND SHALL BE PLACED AT THE ENDS OF EACH STORMWATER INLET AND CURB RAMP.
- CONCRETE DEPTH FOR SIDEWALKS SHALL BE 4" MIN, SEE ST05 FOR DRIVEWAY THICKNESS.
- BROOM FINISH ALL SIDEWALKS AND DRIVEWAYS.
- WHERE NEW SIDEWALK CONSTRUCTION ABUTS AN EXISTING SIDEWALK NOT MEETING CITY STANDARDS, NEW SIDEWALK CONSTRUCTION MUST TRANSITION TO MEET CITY STANDARDS WITHIN A MAXIMUM DISTANCE EQUAL TO THE WIDTH OF THE SIDEWALK EXCLUDING THE CURB.
- TO ENSURE SLOPES DO NOT EXCEED MAXIMUM ALLOWABLE SLOPES, IT IS RECOMMENDED TO SET SLOPES LOWER THAN MAXIMUM ALLOWABLE SLOPES AS FOLLOWS:
 - FOR 2% MAX, SET SLOPE AT 1.5%
 - FOR 8.33% MAX, SET SLOPE AT 7.5%SIDEWALK WITH SLOPES EXCEEDING MAXIMUM ALLOWABLE VALUES SHALL BE REMOVED AND REPLACED AT THE CONTRACTORS EXPENSE.
- COMPACT AGGREGATE BASE (100% FRACTURED FACE) TO 95% MAXIMUM DRY DENSITY PER ASTM D1557.
- ALL CONCRETE SHALL BE COMMERCIAL GRADE 4,000 PSI CONCRETE.



SIDEWALK SECTION WITH SETBACK
NTS



CITY OF
HERMISTON, OREGON
STANDARD DRAWINGS

SIDEWALK

FIGURE
ST04



Where Life is Sweet

Mayor and Members of the City Council
STAFF REPORT
 For the Meeting of August 11, 2025

Title/Subject

Resolution 2389- Declare Intent to Form LID for NW 2nd

Summary and Background

Resolution 2385 discusses the need for improvements to NW 2nd to accommodate increased traffic accessing businesses between Elm & Cherry.

A report has been developed which contains the relevant information regarding scope, cost, impacted properties, methodology and estimated cost to each property, and it is included with this staff report.

Resolution 2389 declares the City Council's intent to form the LID as outlined, and sets a date for a public hearing for the September 8, 2025 City Council meeting. If Resolution 2389 is approved, then the two impacted properties shall immediately be mailed notice of the hearing date, as well as informing them of the project scope, estimated cost, methodology for assigning costs, estimated amount assigned to their property, and rights available to submit written comment and formal remonstrance.

Although Resolution 2389 is technically declaring "intent to form," it does not form the LID. Upon receipt of public comment and/or remonstrances at the September 8 meeting, City Council may choose to cease LID formation.

Tie-In to Council Goals

Transportation: Improve Mobility and Transportation

Fiscal Information

The LID as presented allocates all costs to the benefitting properties. No taxpayer contribution is considered.

This resolution does not actually form an LID, or create any obligation whatsoever on to property owners. This resolution simply sets a public hearing date at which point Council will consider whether or not to actually form the LID.

Alternatives and Recommendation

Alternatives

1. Approve resolution 2389
2. Reject resolution 2389

Recommended Action/Motion

Motion to approve Resolution 2389

Submitted By:

Mark Morgan

RESOLUTION NO. 2389

A RESOLUTION ESTABLISHING THE INTENT TO FORM LOCAL IMPROVEMENT DISTRICT IN THE VICINITY OF NW 2ND STREET BETWEEN TAX LOT 1700 AND TAX LOT 3300 AND SETTING A DATE FOR PUBLIC HEARING

WHEREAS, on August 11, 2025, the City Manager of the City of Hermiston, Oregon, under the direction and by requirement of the council, did prepare and have on file in his office plans and specifications for the formation of a Local Improvement District, a copy of which is attached as Exhibit A for the installation of curb, gutter, sidewalk and paving of:

NW 2nd Street between Tax Lot 1700 and Tax Lot 3300, a distance of approximately 120 linear feet.

WHEREAS, Exhibit A also includes: a map of the proposed local improvement district boundary, including the address, map and tax lot and ownership information for each property within the boundary; an estimate of the actual cost of the improvements; and the proposed methodology for establishing special benefit and levying assessments, the estimated assessment to be levied against each property and a description of funds other than assessments that will be used to fund the local improvement; and

WHEREAS, the council has examined Exhibit A and finds the same satisfactory and the estimate therefore to be in accordance with the probable cost of such work; and

WHEREAS, the property recommended by the City Manager to be included within the boundaries of the district benefitted is in the judgement of the council proper to be included therein and no property is excluded therefrom which should properly be included therein; and

WHEREAS, the installation of the above described paving and utilities is at this time necessary.

NOW, THEREFORE, THE CITY OF HERMISTON RESOLVES AS FOLLOWS:

That it is expedient to improve, and it is hereby proposed, to curb, gutter, sidewalk, and pave NW 2nd Street between Tax Lot 1700 and Tax Lot 3300, a distance of approximately 120 linear feet in accordance with Exhibit A, which is hereby adopted and approved.

1. That the total estimated cost identified in Exhibit A as \$196,000 shall be assessed as a local improvement district.
2. That the proposed improvement district is to be known as "LOCAL IMPROVEMENT DISTRICT NO. 326"; embracing the property benefitted and assessed for the payment of such improvements; which improvement district shall include all lots, parts of lots and parcels of land lying and being within the district described in Exhibit A.

3. That at least ten days prior to the public hearing on formation, the City Manager shall mail or personally deliver notice to the owners of each property proposed to be located within the local improvement district. In addition to the date, time and location of the hearing, the notice shall include:
 - a. A copy of Exhibit A, which includes a general description of the improvements, the boundary of the district, the addresses of each property included, the estimated cost of the proposed local improvement, the methodology for levying assessments and the estimated assessment for each property.
 - b. A statement that these are estimates only, and that the final assessment may be greater.
 - c. A statement that any interested person may testify or submit written comments on the proposed local improvement district and that any property owner may formally object by submitting a written remonstrance stating the reason for the objection and signed by each owner of the property and that the remonstrance must be received by the City Manager no later than 5:00 p.m. on the business day before the public hearing.
 - d. A form for remonstrance.
4. That a public hearing is set for the 8th day of September 2025 at or after 7:00 p.m. at the City Hall (180 NE 2nd St.) in Hermiston, Oregon at which time the Council shall provide a reasonable opportunity for persons to testify and shall consider any written comments received with any remonstrances on such proposed improvement.
5. That this resolution is effective immediately upon its passage.

PASSED by the Common Council this 11th day of August 2025.

SIGNED by the Mayor this 11th day of August 2025.

Doug Primmer, MAYOR

ATTEST:

Heather KP La Beau, ASSISTANT CITY RECORDER

**N.W. 2ND STREET
LOCAL IMPROVEMENT DISTRICT NO. 326
ENGINEERING REPORT**

AUGUST 2025



Prepared for the
City of Hermiston, Oregon

**N.W. 2ND STREET
LOCAL IMPROVEMENT DISTRICT NO. 326
ENGINEERING REPORT**

FOR

CITY OF HERMISTON, OREGON

2025



ANDERSON PERRY & ASSOCIATES, INC.

**La Grande, Redmond, Hermiston, and Enterprise, Oregon
Walla Walla, Washington**

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Section 1 - Project Description

General

The City of Hermiston, Oregon, is considering the formation of a Local Improvement District (LID) No. 326 for N.W. 2nd Street to provide a portion of the street improvements in anticipation of increased traffic due to upcoming improvements to Highway 207 (W. Elm Avenue). Two local improvement districts, including this LID and LID No. 327, will be used to construct improvements to N.W. 2nd Street. These improvements would generally include replacing existing gravel streets with urban local residential streets.

Project Description

The proposed LID location is shown on Figure 1-1, Location and Vicinity Maps, and an aerial of the proposed LID boundary is shown on Figure 1-2, Aerial Photograph. The proposed LID consists of two properties, comprising a total of 200 feet of frontage. In addition to the 200 feet of private property frontage, approximately 20 feet along the LID boundary fronts the W. Dogwood Avenue right-of-way (ROW) (see Figure 1-3). A 40-foot wide existing ROW is available for the construction of the street improvements. The N.W. 2nd Street LID tax map with lot lines and parcel sizes is included in Appendix A.

The improvements for the proposed LID are shown on Figure 1-3, Proposed Improvements. The improvements would consist of approximately 120 linear feet of street improvement, including 5-foot sidewalks, pedestrian ramps, driveways, curb and gutter, two 14-foot travel lanes, and stormwater facilities. "No Parking" signs would be located on both sides of the street. The pavement section would consist of 3 inches of asphalt concrete pavement on 3 inches of 3/4"-0 aggregate base and 6 inches of 1"-0 aggregate base. These proposed improvements are based on a planning-level design and are preliminary in nature. A typical street cross section of the proposed street improvements, based on City Standard Drawing ST11 Local Residential - Option 1, is shown on Figure 1-4. Curb and sidewalk standard drawings are included in Appendix B. A property benefit analysis is included in Chapter 3, with property boundaries current as of July 2025.

SECTION 10, T. 4 N., R. 28 E., W.M.

PROJECT LOCATION

VICINITY MAP

PROJECT LOCATION

SCALE IN FEET

Sources: Esri, TomTom, Garmin, FAO, NOAA, USGS, (c) OpenStreetMap contributors, and the GIS User Community

To Stanfield

Sources: Esri, TomTom, Garmin, FAO, NOAA, USGS, (c) OpenStreetMap contributors, and the GIS User Community

**CITY OF
HERMISTON, OREGON
N.W. 2ND ST. LOCAL IMPROVEMENT DISTRICT NO. 326
ENGINEERING REPORT**

FIGURE
1-1

LOCATION AND VICINITY MAPS

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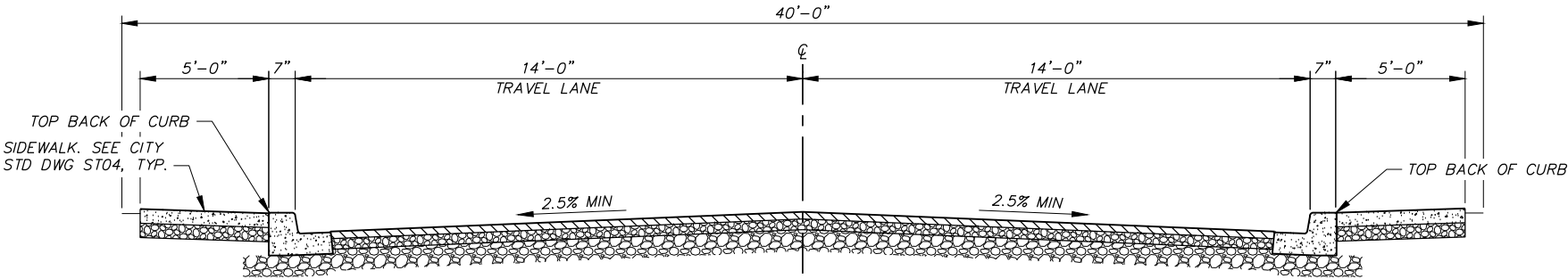


CITY OF
HERMISTON, OREGON
N.W. 2ND ST. LOCAL IMPROVEMENT DISTRICT NO. 326
ENGINEERING REPORT
AERIAL PHOTOGRAPH

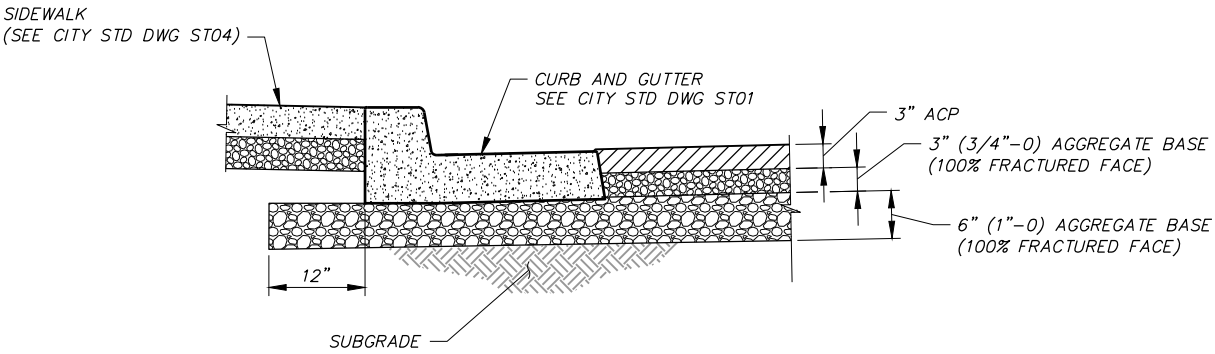
FIGURE
1-2



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STREET CROSS SECTION
NTS



PAVEMENT SECTION
NTS



CITY OF
HERMISTON, OREGON
N.W. 2ND ST. LOCAL IMPROVEMENT DISTRICT NO. 326
ENGINEERING REPORT

URBAN LOCAL STREET SECTIONS

FIGURE
1-4

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Section 2 - Preliminary Feasibility Analysis

Street Improvement

The proposed street improvements required for the N.W. 2nd Street Local Improvement District (LID) No. 326 shown on Figures 1-3 and 1-4 are feasible; however, they present certain challenges. The streets have been graded and graveled within the existing 40-foot right-of-way (ROW). The ROW width is narrower than the city standard ROW width of 50 feet. The street section shown on Figure 1-4 will be used to fit within the existing ROW. Additionally, the power poles near the edge of the ROW would need to be considered during engineering design. If the power poles are confirmed to be in the proposed sidewalk, they would need to be relocated during construction to provide a minimum 4-foot wide accessible path. Nevertheless, no significant obstacles are preventing the proposed street improvements.

Other Utilities

Water, sewer, communications, and electricity are currently available to the properties in the proposed LID.

Project Cost

A preliminary cost estimate was prepared as part of the feasibility analysis and is shown on Figure 2-1, Preliminary Cost Estimate. Due to the preliminary nature of the cost estimate, a 20 percent construction contingency has been included to account for unforeseen issues and potential variability in the bidding environment when the improvements are to be constructed. As shown, the estimate includes anticipated engineering as a percentage of construction costs. The preliminary cost estimate assumes construction of the improvements would occur in 2026; an anticipated annual inflation of 5 percent was assumed and is shown on the cost estimate. The cost estimate would need to be reviewed and potentially adjusted should the street improvements project move to the design and construction phases.

CITY OF HERMISTON, OREGON
N.W. 2ND STREET
LOCAL IMPROVEMENT DISTRICTS NO. 326
PRELIMINARY COST ESTIMATE
August 6, 2025

NO.	DESCRIPTION	UNIT	UNIT PRICE	ESTIMATED QUANTITY	TOTAL PRICE
1	Mobilization/Demobilization	LS	\$ 14,050	All Req'd	\$ 14,050
2	Temporary Protection and Direction of Traffic/Project Safety	LS	5,000	All Req'd	5,000
3	Erosion and Sediment Control	LS	3,300	All Req'd	3,300
4	Removal of Structures and Obstructions*	LS	3,000	All Req'd	3,000
5	General Earthwork	LS	2,000	All Req'd	2,000
6	3/4"-0 Aggregate Base (3 inches)	CY	85	50	4,250
7	1"-0 Aggregate Base (6 inches)	CY	85	90	7,650
8	3-inch Asphalt Concrete Pavement	TON	175	70	12,250
9	Concrete Curb and Gutter	LF	50	240	12,000
10	Americans with Disabilities Act Ramp	EA	3,750	2	7,500
11	Concrete Driveway Approaches	SF	18	300	5,400
12	Concrete Sidewalk	SF	14	900	12,600
13	Stormwater**	LS	25,000	All Req'd	25,000
14	Surface Restoration	LS	1,000	All Req'd	1,000
15	Permanent Striping and Signing	LS	1,000	All Req'd	1,000
Estimated Construction Cost					\$ 116,000
Construction Contingency					30,000
Design Engineering					20,000
Construction Engineering					20,000
Total Year 2025 Estimated Project Cost					\$ 186,000
Inflation at Time of Construction*** (Assumed Construction Year 2026)					10,000
TOTAL YEAR 2026 ESTIMATED IMPROVEMENTS COST					\$ 196,000

*Power pole relocation is not included in this cost estimate since its location relative to the right-of-way has not been determined.

**Stormwater is assumed to include one drywell manhole, one sedimentation chamber, and two catch basins.

***Construction cost inflation is assumed to be 5 percent annually until construction occurs.



CITY OF
HERMISTON, OREGON
 N.W. 2ND STREET LOCAL IMPROVEMENT DISTRICT NO. 326
 ENGINEERING REPORT

PRELIMINARY COST ESTIMATE

FIGURE
2-1

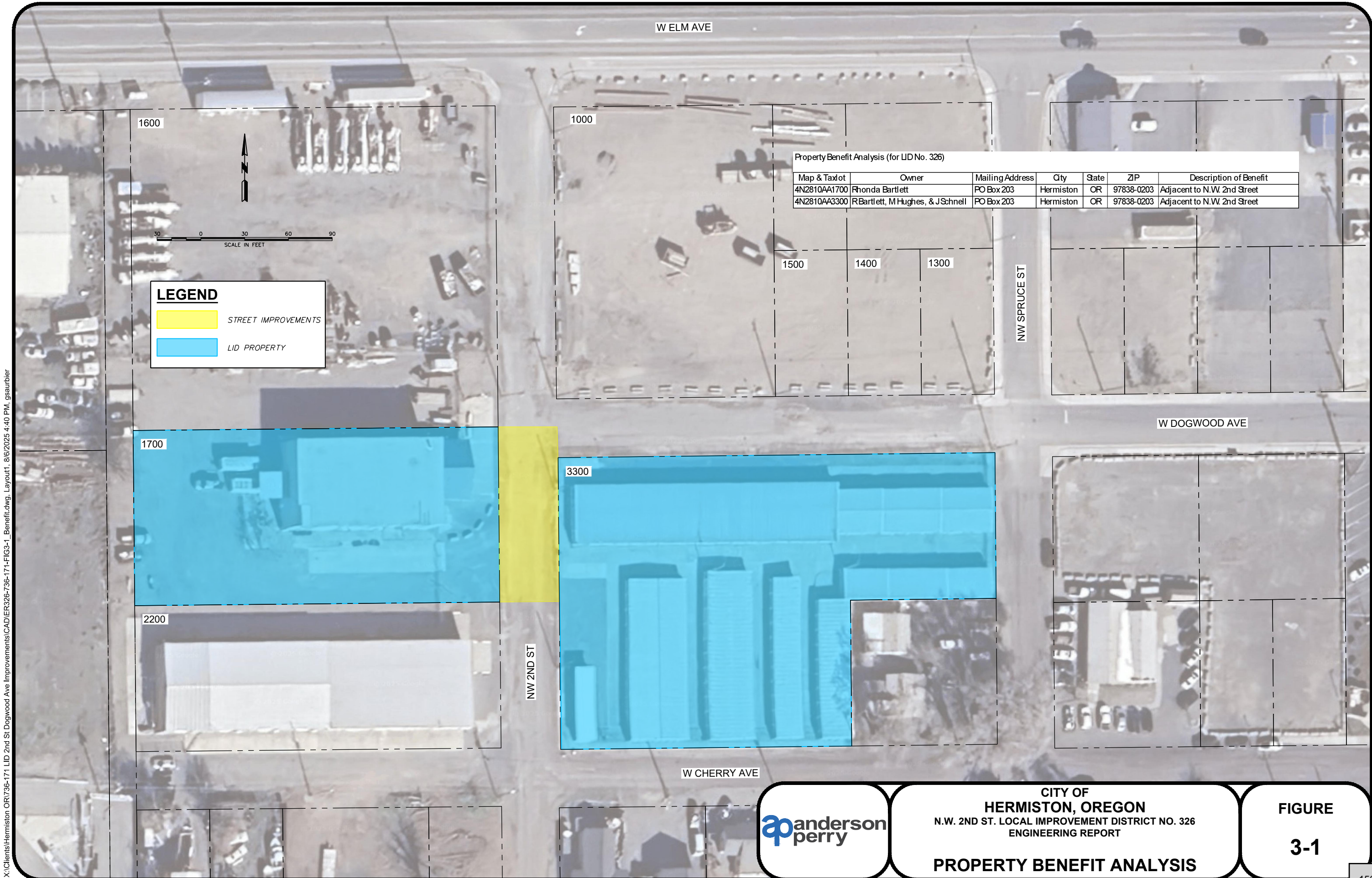
Section 3 - Property Benefit Analysis

Hermiston Municipal Code 157.164

Figure 3-1, Property Benefit Analysis, includes property identification information, ownership information, and a description of the benefit that each property would receive from the proposed improvements. For all properties in this N.W. 2nd Street Local Improvement District (LID) No. 326, the benefit to each property is that the street infrastructure that would be installed for the proposed development area would be in conformance with Hermiston Municipal Code 157.164. The proposed street improvements would ensure each tax lot has paved access to and from the remainder of the street system.

Section 4 of this LID Engineering Report outlines the methodology for assigning costs, which would be based on linear feet (LF) of frontage adjacent to the street improvements. Since no off-site improvements are included in this LID Engineering Report, the benefit that each property would receive is directly proportional to the LF of improvements installed.

X:\Clients\Hermiston OR\736-171 LID 2nd St Dogwood Ave Improvements\CAD\ER326-736-171-FIG3-1_Benefit.dwg, Layout1, 8/6/2025 4:40 PM, gsaubier





CITY OF
HERMISTON, OREGON
N.W. 2ND ST. LOCAL IMPROVEMENT DISTRICT NO. 326
ENGINEERING REPORT

PROPERTY BENEFIT ANALYSIS

FIGURE
3-1

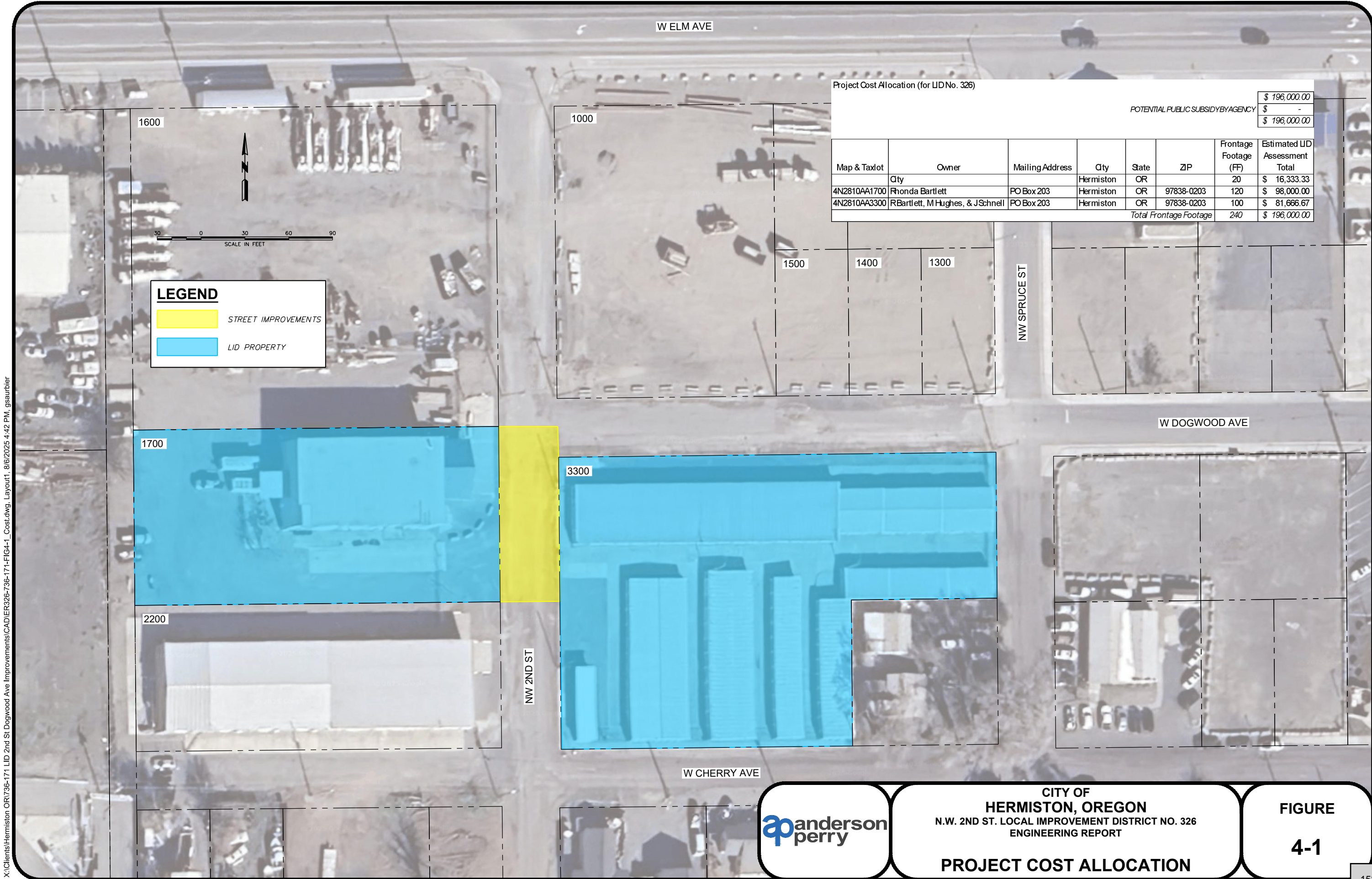
Section 4 - Project Cost Allocation

Methodology

The street improvement project included in the proposed N.W. 2nd Street Local Improvement District (LID) No. 326 lends itself to allocating costs based on each property's frontage along the street improvements. The property frontage length along the proposed street improvements relative to total property frontage on the proposed street improvements for all properties in the LID is directly proportional to the assessment of each property. This method would distribute the costs of the LID equitably. Figure 4-1, Project Cost Allocation, shows each affected property, its associated frontage length, and its associated estimated LID assessment. The City will be covering the 20 linear feet of frontage for the improvements of the eastern half of N.W. 2nd Street across the W. Dogwood Avenue intersection.

Description of Other Funds

The project cost is anticipated to be completely covered by the owners of the properties within the LID and by the City as described above. No additional funds would be required.



X:\Clients\Hermiston OR\736-171 LID 2nd St Dogwood Ave Improvements\CAD\ER326-736-171-FIG4-1_Cost.dwg, Layout1, 8/6/2025 4:42 PM, gsaubier



CITY OF
HERMISTON, OREGON
N.W. 2ND ST. LOCAL IMPROVEMENT DISTRICT NO. 326
ENGINEERING REPORT

PROJECT COST ALLOCATION

FIGURE

4-1

Section 5 - Property Valuation and Assessment Estimate

General

The valuations for each property shown on Table 5-1 below represent the latest figures available from the Umatilla County Assessor's office. Table 5-1 shows an estimate of the amount to be assessed for each property in the N.W. 2nd Street Local Improvement District (LID) No. 326. These amounts represent the total current value of the assessments without regard for potential long-term repayment plans and associated interest fees. An explanation of options for long-term payment plans is available below. A full accounting of how the assessment was calculated, using the methodology outlined in Section 4, is shown on Figure 4-1, Project Cost Allocation. Actual costs would be determined after construction is completed and final project costs are known.

TABLE 5-1
UMATILLA COUNTY ASSESSOR'S PROPERTY VALUATIONS

Map and Tax Lot	Assessed Value	Real Market Value	Outstanding Tax Assessments	Estimated LID Assessment Total
4N2810AA1700	\$238,220.00	\$678,790.00	\$0.00	\$98,000.00
4N2810AA3300	\$662,380.00	\$1,502,500.00	\$0.00	\$81,666.67

Long-term Repayment Plan

Construction of the proposed improvements considered in the LID would be coordinated by the City of Hermiston on behalf of each property. Each property owner would have the option to pay their LID assessment in its entirety, or they may choose to make installment payments over ten years. The City intends to finance the improvements upfront through the most cost-effective means available. Any costs associated with carrying costs of financing the proposed improvements would be passed on directly to the benefiting properties; however, the City would not add on any administrative or overhead fees associated with obtaining and managing the financing instrument.

Appendices Table of Contents

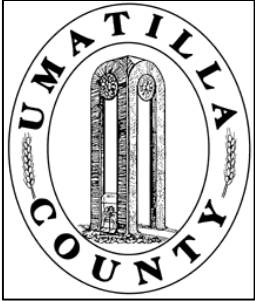
Appendix A - Tax Lot Map

Appendix B - Standard Drawings

APPENDIX A

Tax Lot Map

This map was prepared for Assessment & Taxation purposes only and was NOT prepared nor is it suitable for legal, engineering or surveying purposes.

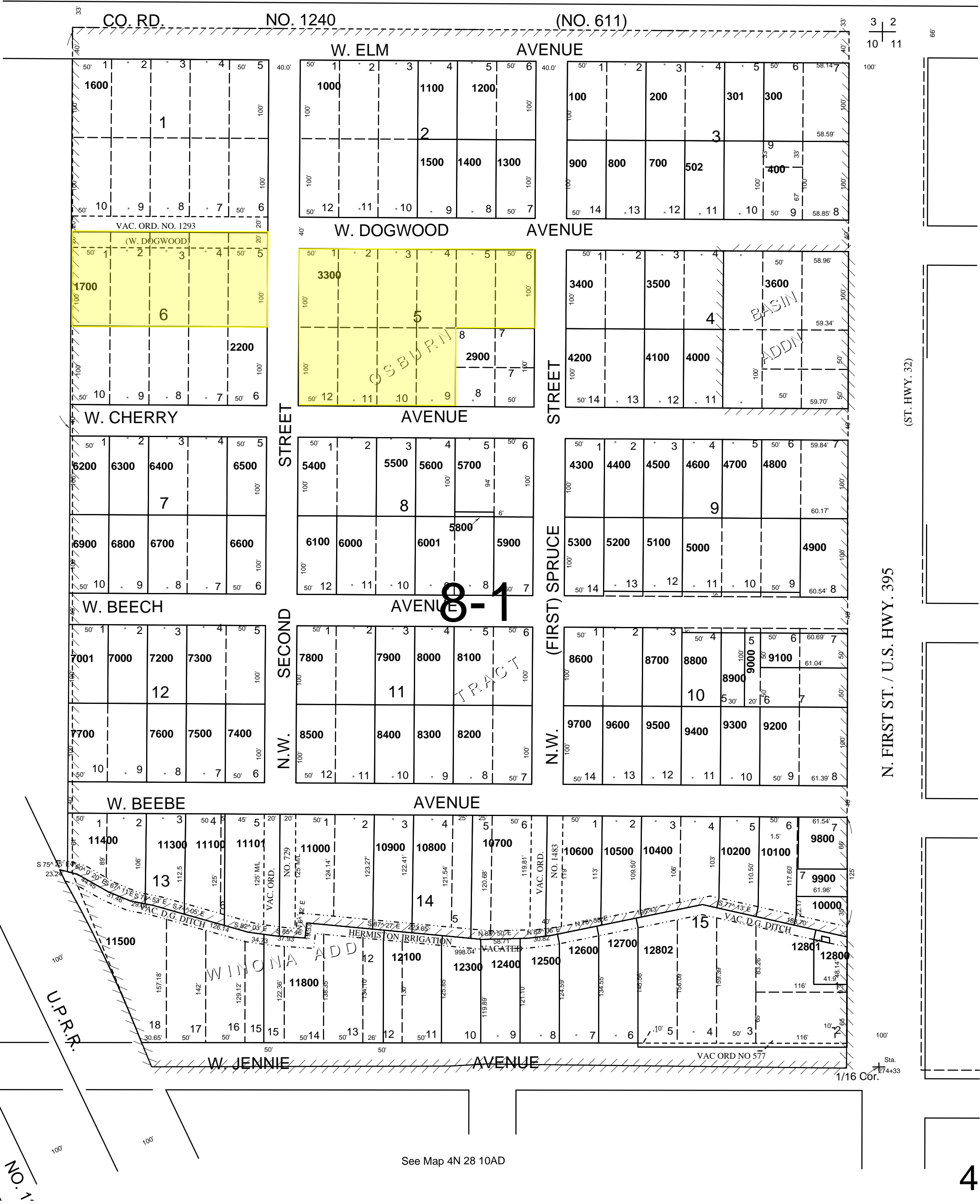


NE1/4 NE1/4 SEC 10 T4N R28E WM
UMATILLA COUNTY, OR

SCALE: 1" = 100'

4N2810AA

See Map 4N 03D



CANCELLED
TL 12900
13000
13100
13200
11600
11700
11801
13300
10300
490
501
600
3100
11900
12000
12101
11200
7100
12200
2700
3000
3200
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5201
8801
8901
9001
9101
1800
1900
2000
2100
2300
2400
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2902
3501
3700
3800
3900

See Map 4N 28 11BB

Revised: 6/16/09

4N2810AA

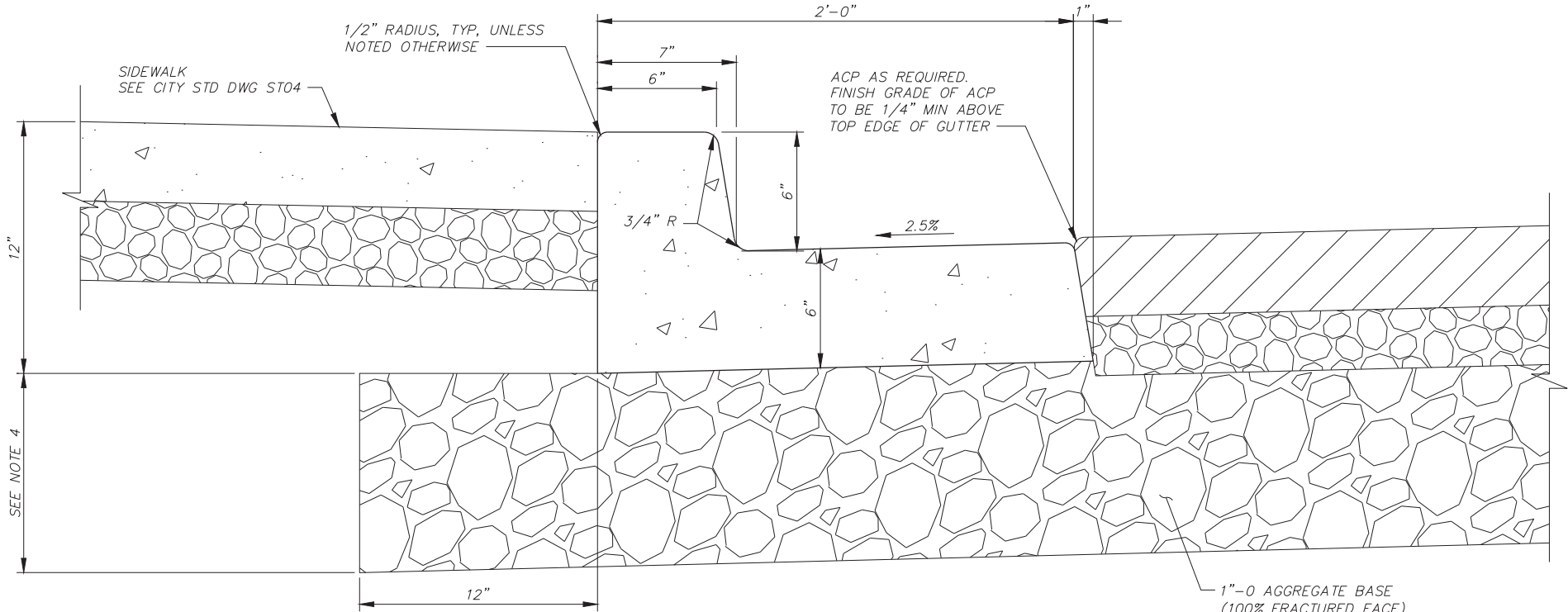
LID Boundary

APPENDIX B

Standard Drawings

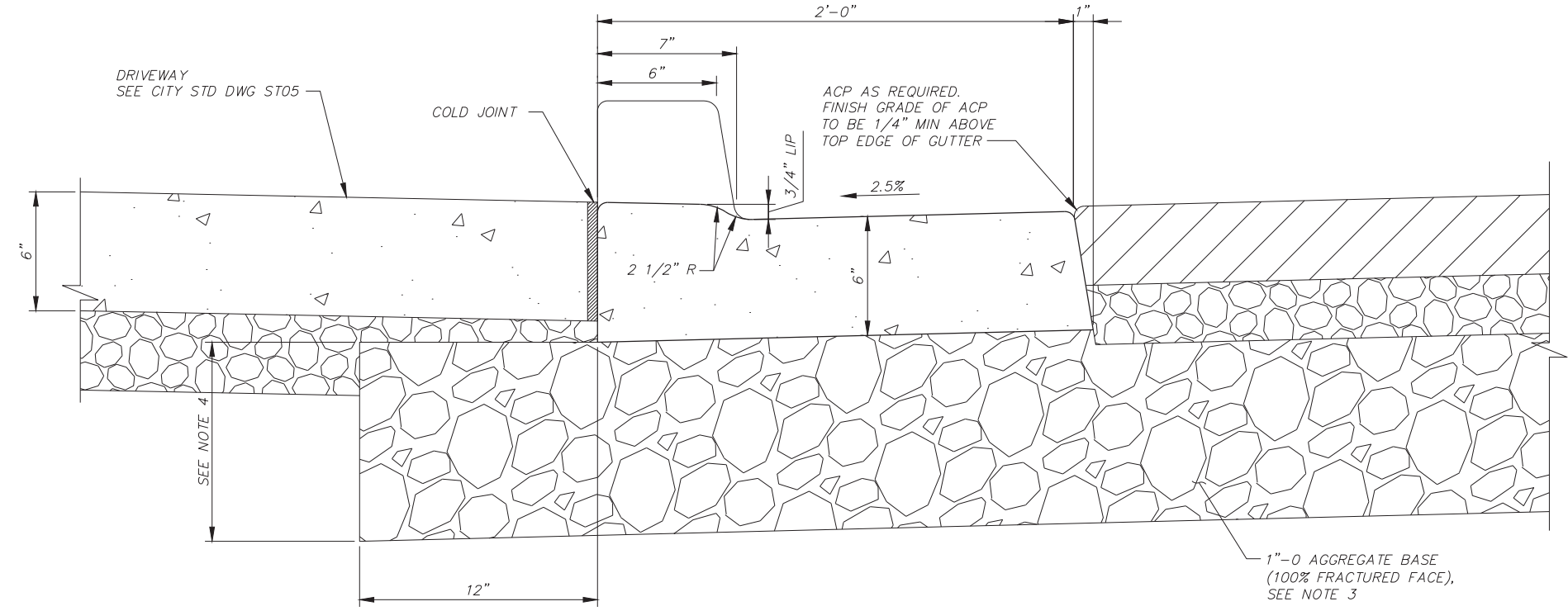
NOTES

1. GUTTER CROSS SLOPE SHALL BE MINIMUM 2% AND MAXIMUM 3% UNLESS OTHERWISE APPROVED BY THE CITY.
2. ALL CONCRETE SHALL BE COMMERCIAL GRADE 4,000 PSI CONCRETE.
3. COMPACT AGGREGATE BASE TO 95% MAXIMUM DRY DENSITY PER ASTM D1557.
4. AGGREGATE BASE THICKNESS BELOW CURB AND GUTTER SHALL BE 6" THICK OR CONFORM TO CITY STD DWG ST07-ST11, WHICHEVER IS GREATER.
5. SEE CITY STD DWG ST04 FOR CURB AND GUTTER JOINT REQUIREMENTS.
6. CONCRETE SHALL BE TROWELED SMOOTH PRIOR TO APPLYING BROOM FINISH.
7. INSPECTION CRITERIA:
 - 7.1. COMPLETED CURB AND GUTTER SHALL NOT CAUSE PUDDLING ISSUES. PUDDLE WIDTH SHALL NOT EXCEED 75% OF THE GUTTER WIDTH AND PUDDLE LENGTH SHALL NOT EXCEED 2'.
 - 7.2. CROSS SLOPE OF THE GUTTER SHALL BE A MINIMUM OF 1.5% AND A MAXIMUM OF 3.5% AS MEASURED BY A 2' ELECTRONIC LEVEL.
 - 7.3. THERE SHALL BE NO BREAK POINTS OR REVERSE SLOPE IN THE GUTTER CROSS SLOPE AS MEASURED BY A 2' ELECTRONIC LEVEL. THE MAXIMUM ALLOWABLE GAP BETWEEN THE LEVEL AND THE GUTTER SURFACE SHALL BE 1/8".
 - 7.4. WHEN A 6 FOOT LONG STRAIGHTEDGE IS LAID ON THE TOP FACE OF THE CURB OR ON THE SURFACE OF THE GUTTER, THE SURFACE SHALL NOT VARY MORE THAN 0.02 FEET FROM THE EDGE OF THE STRAIGHTEDGE EXCEPT AT GRADE CHANGES OR VERTICAL CURVES.
 - 7.5. THE CONTRACTOR SHALL CONSTRUCT ALL CURB AND GUTTER WITHIN 0.02 FEET OF TRUE LINE, WITHIN 0.02 FEET OF ESTABLISHED SURFACE GRADE, CROSS SECTION, AND SLOPE, AND WITHIN 0.02 FEET OF SPECIFIED THICKNESS.
8. WHEN INSTALLING NEW CURB AND GUTTER ON AN EXISTING PAVED STREET, THE ASPHALT SHALL BE CUT A MINIMUM OF 2 FEET FROM THE EDGE OF THE GUTTER OR AS NEEDED TO ACHIEVE COMPACTION OF THE AGGREGATE BASE BY MECHANICAL MEANS.



STANDARD CURB AND GUTTER

NTS



CURB AND GUTTER AT DRIVEWAY

NTS



Where Life is Sweet

CITY OF
HERMISTON, OREGON
STANDARD DRAWINGS

CURB AND GUTTER

FIGURE
ST01



Where Life is Sweet

Mayor and Members of the City Council
STAFF REPORT
 For the Meeting of August 11, 2025

Title/Subject

Resolution 2390- Declare Intent to Form LID for NW 2nd

Summary and Background

Resolution 2386 discusses the need for improvements to NW 2nd to accommodate increased traffic accessing businesses between Elm & Cherry.

A report has been developed which contains the relevant information regarding scope, cost, impacted properties, methodology and estimated cost to each property, and it is included with this staff report.

Resolution 2390 declares the City Council's intent to form the LID as outlined, and sets a date for a public hearing for the September 8, 2025 City Council meeting. If Resolution 2390 is approved, then the two impacted properties shall immediately be mailed notice of the hearing date, as well as informing them of the project scope, estimated cost, methodology for assigning costs, estimated amount assigned to their property, and rights available to submit written comment and formal remonstrance.

Although Resolution 2390 is technically declaring "intent to form," it does not form the LID. Upon receipt of public comment and/or remonstrances at the September 8 meeting, City Council may choose to cease LID formation.

Tie-In to Council Goals

Transportation: Improve Mobility and Transportation

Fiscal Information

The LID as presented allocates all costs to the benefitting properties. No taxpayer contribution is considered.

This resolution does not actually form an LID, or create any obligation whatsoever on to property owners. This resolution simply sets a public hearing date at which point Council will consider whether or not to actually form the LID.

Alternatives and Recommendation

Alternatives

1. Approve resolution 2390
2. Reject resolution 2390

Recommended Action/Motion

Motion to approve Resolution 2390

Submitted By:

Mark Morgan

RESOLUTION NO. 2390

A RESOLUTION ESTABLISHING THE INTENT TO FORM LOCAL IMPROVEMENT DISTRICT IN THE VICINITY OF NW 2ND STREET BETWEEN TAX LOT 2200 AND TAX LOT 3300 AND SETTING A DATE FOR PUBLIC HEARING

WHEREAS, on August 11, 2025, the City Manager of the City of Hermiston, Oregon, under the direction and by requirement of the council, did prepare and have on file in his office plans and specifications for the formation of a Local Improvement District, a copy of which is attached as Exhibit A for the installation of curb, gutter, sidewalk and paving of:

NW 2nd Street between Tax Lot 2200 and Tax Lot 3300, a distance of approximately 100 linear feet.

WHEREAS, Exhibit A also includes: a map of the proposed local improvement district boundary, including the address, map and tax lot and ownership information for each property within the boundary; an estimate of the actual cost of the improvements; and the proposed methodology for establishing special benefit and levying assessments, the estimated assessment to be levied against each property and a description of funds other than assessments that will be used to fund the local improvement; and

WHEREAS, the council has examined Exhibit A and finds the same satisfactory and the estimate therefore to be in accordance with the probable cost of such work; and

WHEREAS, the property recommended by the City Manager to be included within the boundaries of the district benefitted is in the judgement of the council proper to be included therein and no property is excluded therefrom which should properly be included therein; and

WHEREAS, the installation of the above described paving and utilities is at this time necessary.

NOW, THEREFORE, THE CITY OF HERMISTON RESOLVES AS FOLLOWS:

That it is expedient to improve, and it is hereby proposed, to curb, gutter, sidewalk, and pave NW 2nd Street between Tax Lot 2200 and Tax Lot 3300, a distance of approximately 100 linear feet in accordance with Exhibit A, which is hereby adopted and approved.

1. That the total estimated cost identified in Exhibit A as \$150,000 shall be assessed as a local improvement district.
2. That the proposed improvement district is to be known as "LOCAL IMPROVEMENT DISTRICT NO. 327"; embracing the property benefitted and assessed for the payment of such improvements; which improvement district shall include all lots, parts of lots and parcels of land lying and being within the district described in Exhibit A.
3. That at least ten days prior to the public hearing on formation, the City Manager

shall mail or personally deliver notice to the owners of each property proposed to be located within the local improvement district. In addition to the date, time and location of the hearing, the notice shall include:

- a. A copy of Exhibit A, which includes a general description of the improvements, the boundary of the district, the addresses of each property included, the estimated cost of the proposed local improvement, the methodology for levying assessments and the estimated assessment for each property.
 - b. A statement that these are estimates only, and that the final assessment may be greater.
 - c. A statement that any interested person may testify or submit written comments on the proposed local improvement district and that any property owner may formally object by submitting a written remonstrance stating the reason for the objection and signed by each owner of the property and that the remonstrance must be received by the City Manager no later than 5:00 p.m. on the business day before the public hearing.
 - d. A form for remonstrance.
4. That a public hearing is set for the 8th day of September 2025 at or after 7:00 p.m. at the City Hall (180 NE 2nd St.) in Hermiston, Oregon at which time the Council shall provide a reasonable opportunity for persons to testify and shall consider any written comments received with any remonstrances on such proposed improvement.
 5. That this resolution is effective immediately upon its passage.

PASSED by the Common Council this 11th day of August 2025.
SIGNED by the Mayor this 11th day of August 2025.

Doug Primmer, MAYOR

ATTEST:

Heather KP La Beau, ASSISTANT CITY RECORDER

**N.W. 2ND STREET
LOCAL IMPROVEMENT DISTRICT NO. 327
ENGINEERING REPORT**

AUGUST 2025



Prepared for the
City of Hermiston, Oregon

**N.W. 2ND STREET
LOCAL IMPROVEMENT DISTRICT NO. 327
ENGINEERING REPORT**

FOR

CITY OF HERMISTON, OREGON

2025



ANDERSON PERRY & ASSOCIATES, INC.

**La Grande, Redmond, Hermiston, and Enterprise, Oregon
Walla Walla, Washington**

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Section 1 - Project Description

General

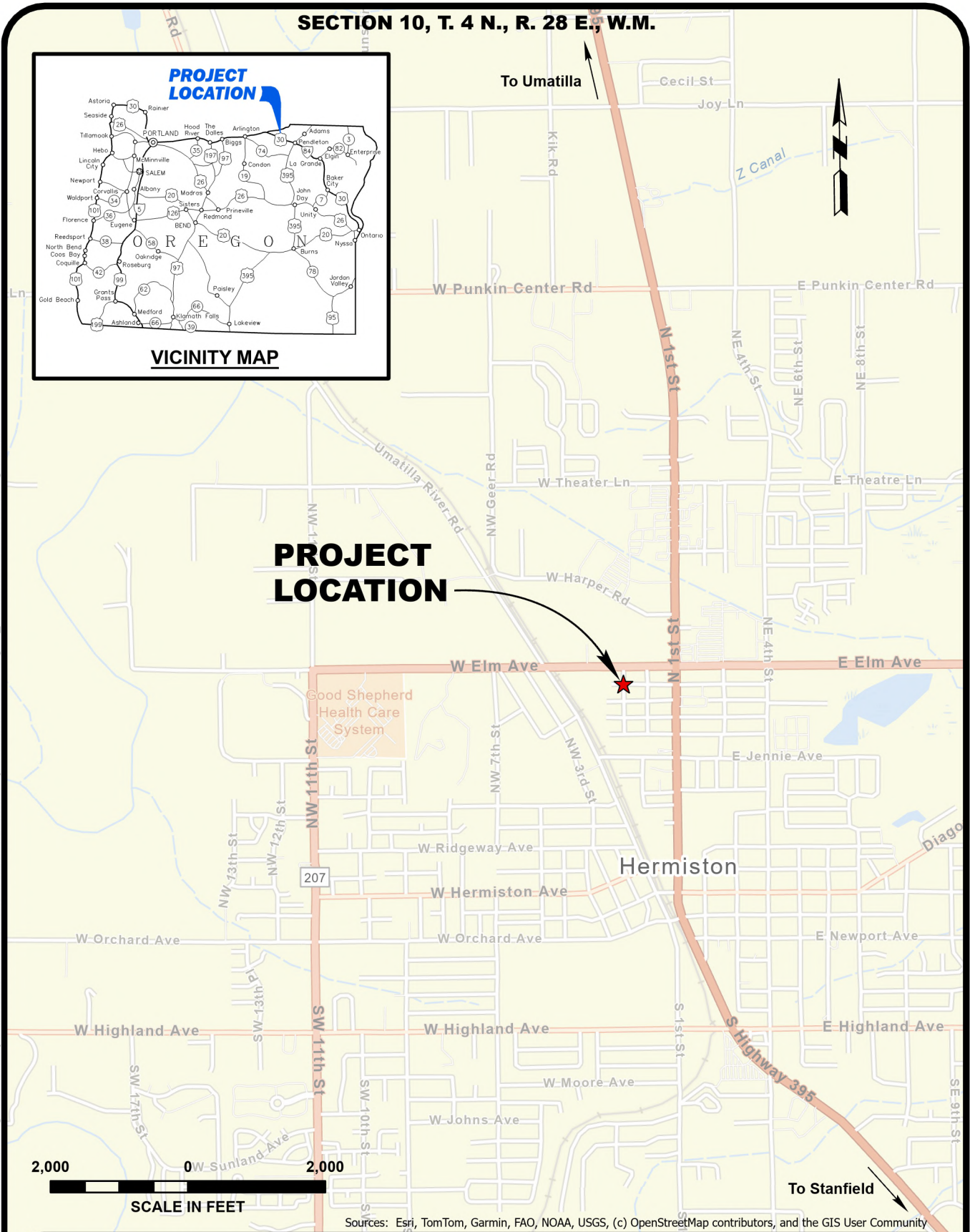
The City of Hermiston, Oregon, is considering the formation of a Local Improvement District (LID) No. 327 for N.W. 2nd Street to provide a portion of the street improvements in anticipation of increased traffic due to upcoming improvements to Highway 207 (W. Elm Avenue). Two local improvement districts, including this LID and LID No. 326, will be used to construct improvements to N.W. 2nd Street. These improvements would generally include replacing existing gravel streets with urban local residential streets.

Project Description

The proposed LID location is shown on Figure 1-1, Location and Vicinity Maps, and an aerial of the proposed LID boundary is shown on Figure 1-2, Aerial Photograph. The proposed LID consists of two properties with a total of 200 feet of frontage. A 40-foot wide existing right-of-way is available for the construction of the street improvements. The N.W. 2nd Street LID tax map with lot lines and parcel sizes is included in Appendix A.

The improvements for the proposed LID are shown in Figure 1-3, Proposed Improvements. The improvements would consist of approximately 100 linear feet of street improvement, including 5-foot sidewalks, pedestrian ramps, driveways, curb and gutter, two 14-foot travel lanes, and stormwater facilities. “No Parking” signs would be located on both sides of the street. The pavement section would consist of 3 inches of asphalt concrete pavement on 3 inches of 3/4”-0 aggregate base and 6 inches of 1”-0 aggregate base. These proposed improvements are based on a planning-level design and are preliminary in nature. A typical street cross section of the proposed street improvements, based on City Standard Drawing ST11 Local Residential - Option 1, is shown on Figure 1-4. Curb and sidewalk standard drawings are included in Appendix B. A property benefit analysis is included in Chapter 3, with property boundaries current as of July 2025.

X:\Clients\Hermiston OR\736-171 LID 2nd St Dogwood Ave Improvements\GIS\LID-736-171-2NDST-DOGWOOD.aprx, ER327-736-171-Fig1-1_LocVic, 7/14/2025 9:01 AM, gsaubier



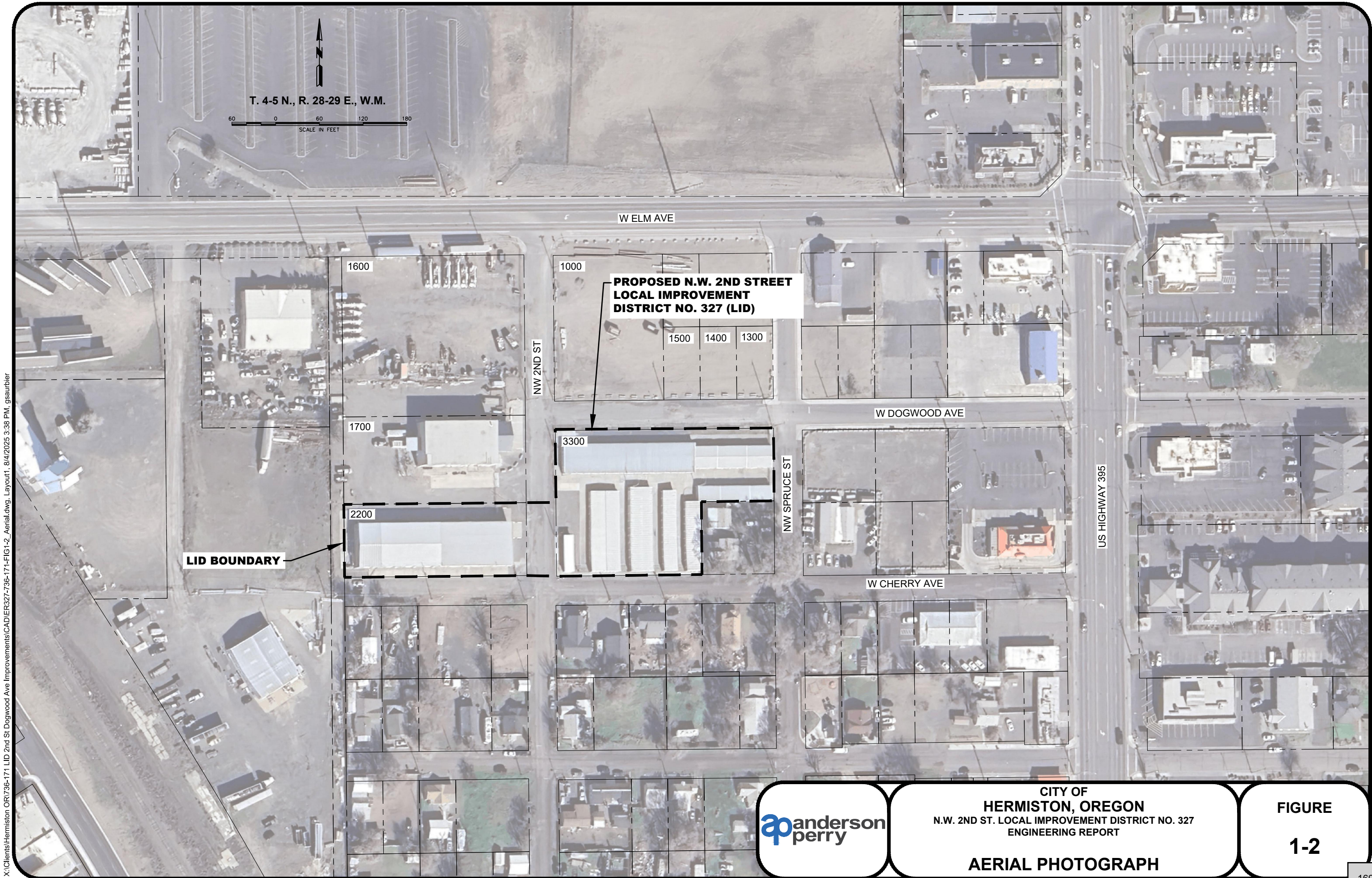


**CITY OF
HERMISTON, OREGON**
N.W. 2ND ST. LOCAL IMPROVEMENT DISTRICT NO. 327
ENGINEERING REPORT

LOCATION AND VICINITY MAPS

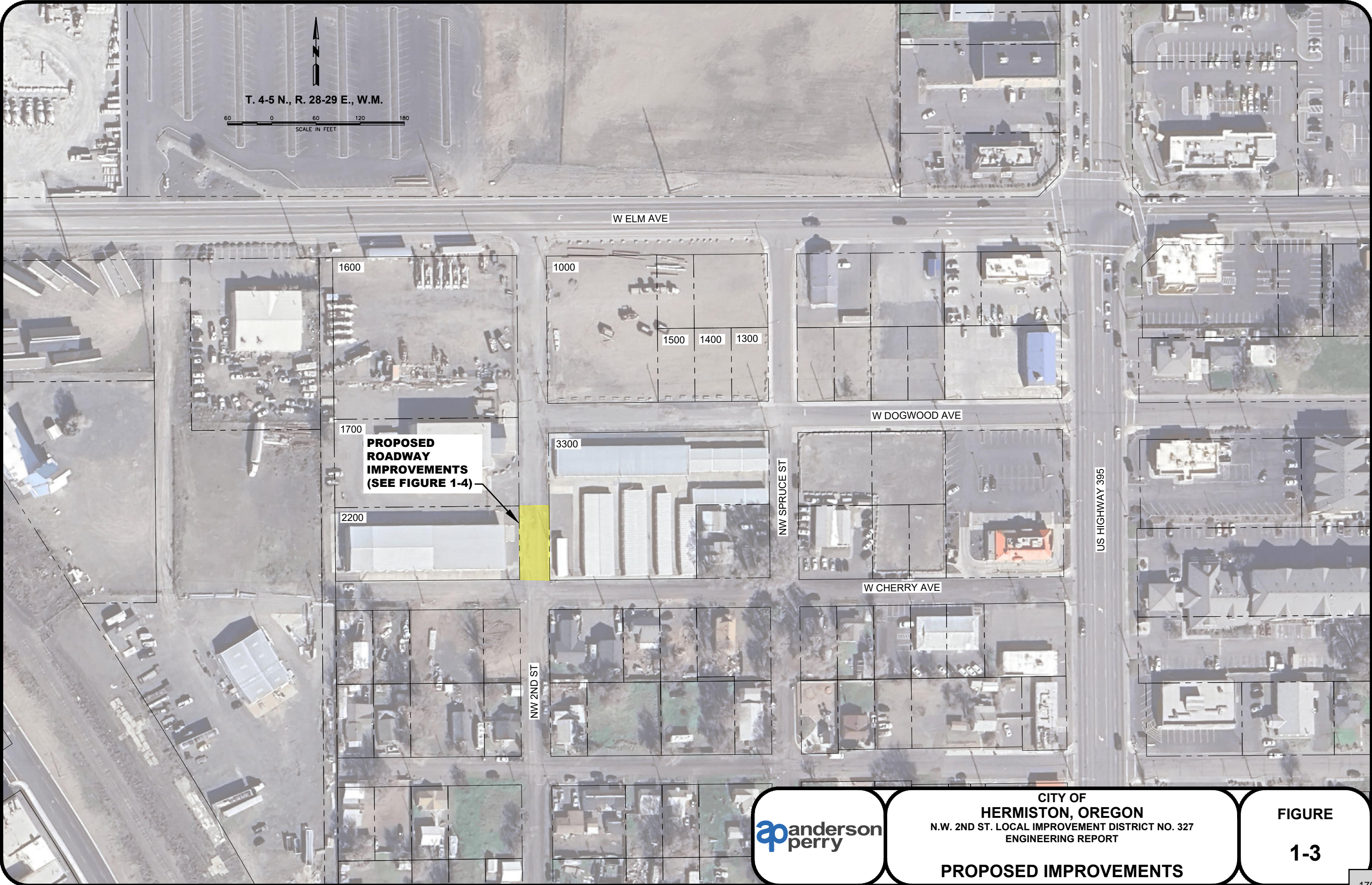
**FIGURE
1-1**

168



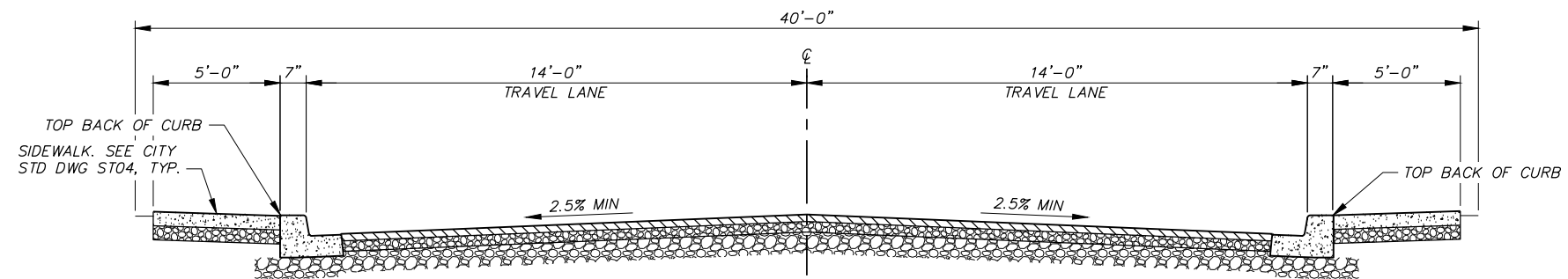
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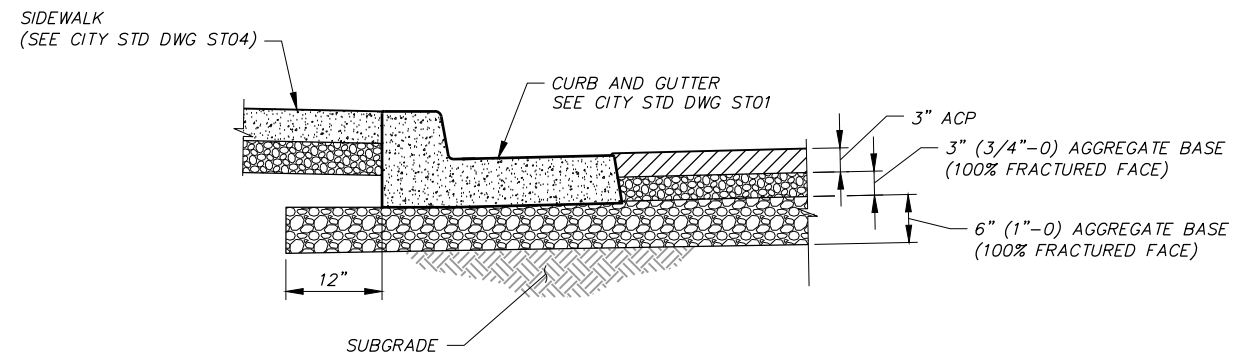
CITY OF
HERMISTON, OREGON
N.W. 2ND ST. LOCAL IMPROVEMENT DISTRICT NO. 327
ENGINEERING REPORT
PROPOSED IMPROVEMENTS

FIGURE
1-3



STREET CROSS SECTION

NTS



PAVEMENT SECTION

NTS

Section 2 - Preliminary Feasibility Analysis

Street Improvement

The proposed street improvements required for the N.W. 2nd Street Local Improvement District (LID) No. 327 shown in Figures 1-3 and 1-4 are feasible; however, they present certain challenges. The streets have been graded and graveled within the existing 40-foot right-of-way (ROW). The ROW width is narrower than the city standard ROW width of 50 feet. The street section shown on Figure 1-4 will be used to fit within the existing ROW. Additionally, the power poles near the edge of the ROW would need to be considered during engineering design. If the power poles are confirmed to be in the proposed sidewalk, they would need to be relocated during construction to provide a minimum 4-foot wide accessible path. Nevertheless, no significant obstacles are preventing the proposed street improvements.

Other Utilities

Water, sewer, communications, and electricity are currently available to the properties in the proposed LID.

Project Cost

A preliminary cost estimate was prepared as part of the feasibility analysis and is shown on Figure 2-1, Preliminary Cost Estimate. Due to the preliminary nature of the cost estimate, a 20 percent construction contingency has been included to account for unforeseen issues and potential variability in the bidding environment when the improvements are to be constructed. As shown, the estimate includes anticipated engineering as a percentage of construction costs. The preliminary cost estimate assumes construction of the improvements would occur in 2026; an anticipated annual inflation of 5 percent was assumed and is shown on the cost estimate. The cost estimate would need to be reviewed and potentially adjusted should the street improvements project move to the design and construction phases.

CITY OF HERMISTON, OREGON
N.W. 2ND STREET
LOCAL IMPROVEMENT DISTRICTS NO. 327
PRELIMINARY COST ESTIMATE
August 6, 2025

NO.	DESCRIPTION	UNIT	UNIT PRICE	ESTIMATED QUANTITY	TOTAL PRICE
1	Mobilization/Demobilization	LS	\$ 10,350	All Req'd	\$ 10,350
2	Temporary Protection and Direction of Traffic/Project Safety	LS	3,100	All Req'd	3,100
3	Erosion and Sediment Control	LS	3,000	All Req'd	3,000
4	Removal of Structures and Obstructions*	LS	2,000	All Req'd	2,000
5	General Earthwork	LS	2,000	All Req'd	2,000
6	3/4"-0 Aggregate Base (3 inches)	CY	85	40	3,400
7	1"-0 Aggregate Base (6 inches)	CY	85	70	5,950
8	3-inch Asphalt Concrete Pavement	TON	175	60	10,500
9	Concrete Curb and Gutter	LF	50	200	10,000
10	Americans with Disabilities Act Ramp	EA	3,750	2	7,500
11	Concrete Driveway Approaches	SF	18	300	5,400
12	Concrete Sidewalk	SF	14	700	9,800
13	Stormwater**	LS	25,000	All Req'd	25,000
14	Surface Restoration	LS	1,000	All Req'd	1,000
15	Permanent Striping and Signing	LS	1,000	All Req'd	1,000
Estimated Construction Cost					\$ 100,000
Construction Contingency					20,000
Design Engineering					10,000
Construction Engineering					10,000
Total Year 2025 Estimated Project Cost					\$ 140,000
Inflation at Time of Construction*** (Assumed Construction Year 2026)					10,000
TOTAL YEAR 2026 ESTIMATED IMPROVEMENTS COST					\$ 150,000

*Power pole relocation is not included in this cost estimate since its location relative to the right-of-way has not been determined.

**Stormwater is assumed to include one drywell manhole, one sedimentation chamber, and two catch basins.

***Construction cost inflation is assumed to be 5 percent annually until construction occurs.

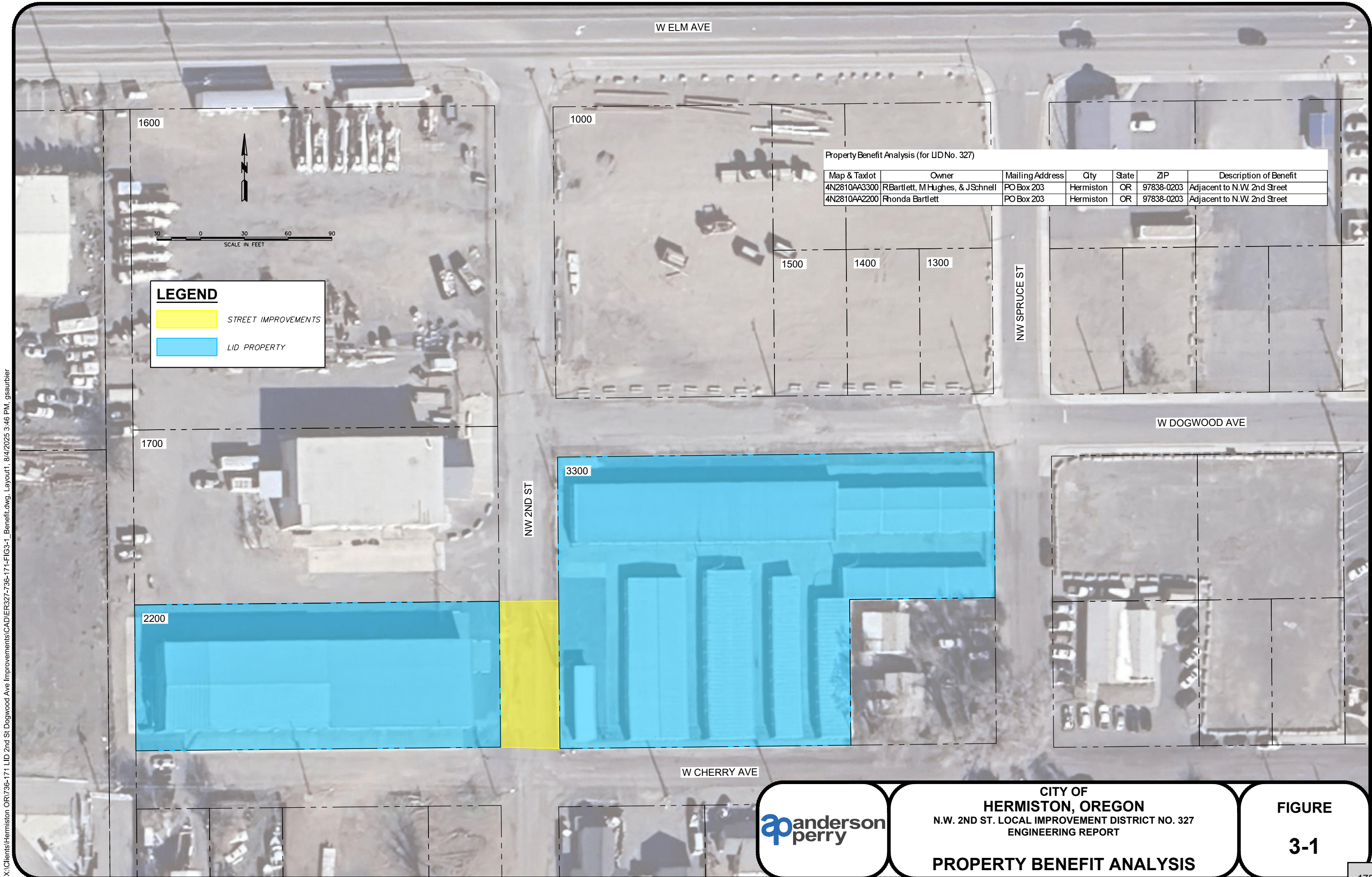
Section 3 - Property Benefit Analysis

Hermiston Municipal Code 157.164

Figure 3-1, Property Benefit Analysis, includes property identification information, ownership information, and a description of the benefit that each property would receive from the proposed improvements. For all properties in this N.W. 2nd Street Local Improvement District (LID) No. 327, the benefit to each property is that the street infrastructure that would be installed for the proposed development area would be in conformance with Hermiston Municipal Code 157.164. The proposed street improvements would ensure each tax lot has paved access to and from the remainder of the street system.

Section 4 of this LID Engineering Report outlines the methodology for assigning costs, which would be based on linear feet (LF) of frontage adjacent to the street improvements. Since no off-site improvements are included in this LID Engineering Report, the benefit that each property would receive is directly proportional to the LF of improvements installed.

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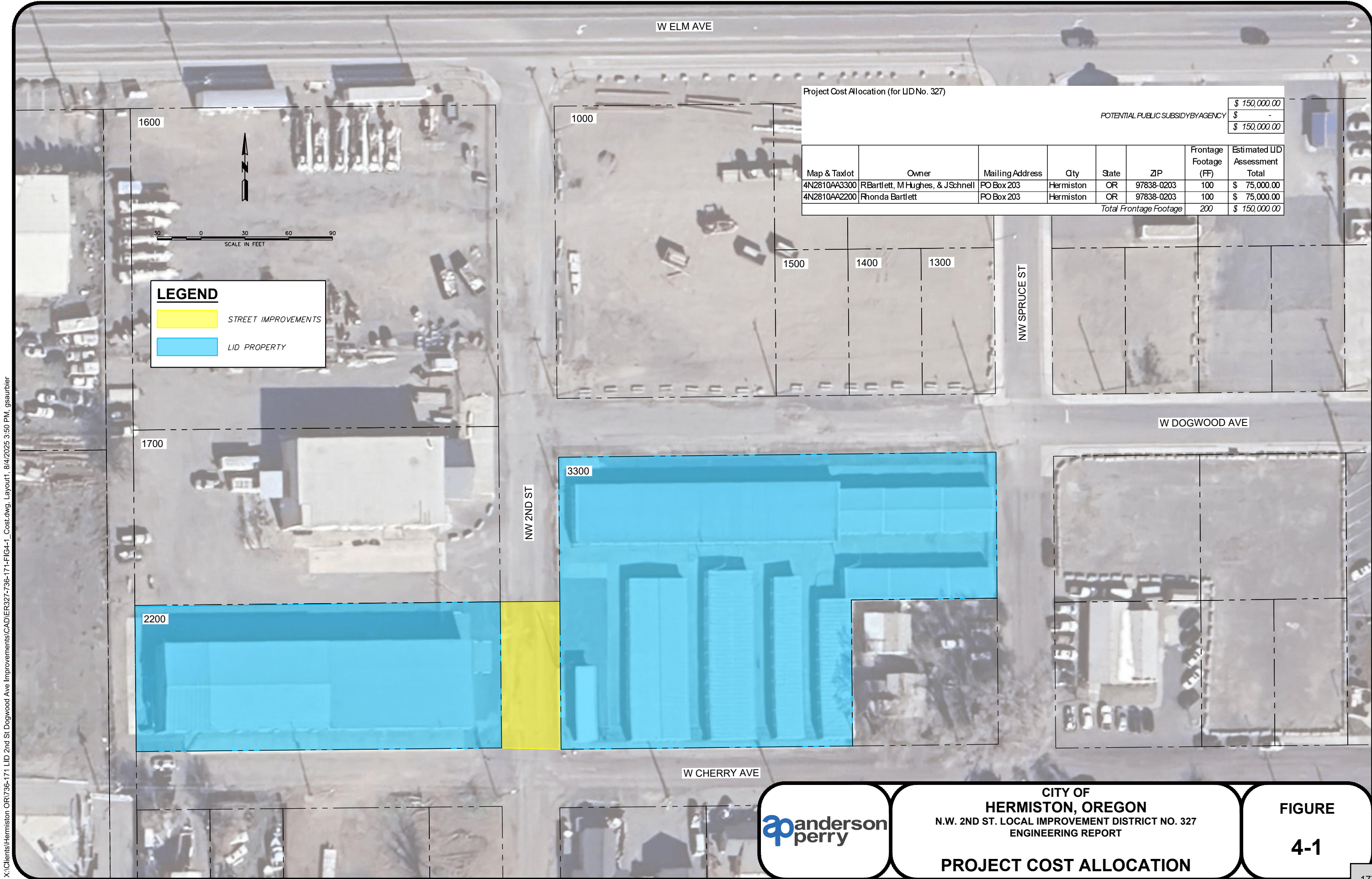
Section 4 - Project Cost Allocation

Methodology

The street improvement project included in the proposed N.W. 2nd Street Local Improvement District (LID) No. 327 lends itself to allocating costs based on each property's frontage along the street improvements. The property frontage length along the proposed street improvements relative to total property frontage on the proposed street improvements for all properties in the LID is directly proportional to the assessment of each property. This method would distribute the costs of the LID equitably. Figure 4-1, Project Cost Allocation, shows each affected property, its associated frontage length, and its associated estimated LID assessment.

Description of Other Funds

The project cost is anticipated to be completely covered by the owners of the properties within the LID, so no additional funds would be required.



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CITY OF
HERMISTON, OREGON
N.W. 2ND ST. LOCAL IMPROVEMENT DISTRICT NO. 327
ENGINEERING REPORT

PROJECT COST ALLOCATION

FIGURE
4-1

Section 5 - Property Valuation and Assessment Estimate

General

The valuations for each property shown in Table 5-1 below represent the latest figures available from the Umatilla County Assessor's office. Table 5-1 shows an estimate of the amount to be assessed for each property in the N.W. 2nd Street Local Improvement District (LID) No. 327. These amounts represent the total current value of the assessments without regard for potential long-term repayment plans and associated interest fees. An explanation of options for long-term payment plans is available below. A full accounting of how the assessment was calculated, using the methodology outlined in Section 4, is shown on Figure 4-1, Project Cost Allocation. Actual costs would be determined after construction is completed and final project costs are known.

TABLE 5-1
UMATILLA COUNTY ASSESSOR'S PROPERTY VALUATIONS

Map and Tax Lot	Assessed Value	Real Market Value	Outstanding Tax Assessments	Estimated LID Assessment Total
4N2810AA3300	\$662,380.00	\$1,502,500.00	\$0.00	\$75,000.00
4N2810AA2200	\$436,610.00	\$927,070.00	\$0.00	\$75,000.00

Long-term Repayment Plan

Construction of the proposed improvements considered in the LID would be coordinated by the City of Hermiston on behalf of each property. Each property owner would have the option to pay their LID assessment in its entirety, or they may choose to make installment payments over ten years. The City intends to finance the improvements upfront through the most cost-effective means available. Any costs associated with carrying costs of financing the proposed improvements would be passed on directly to the benefiting properties; however, the City would not add on any administrative or overhead fees associated with obtaining and managing the financing instrument.

Appendices Table of Contents

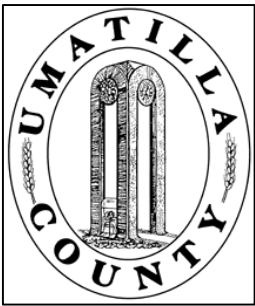
Appendix A - Tax Lot Map

Appendix B - Standard Drawings

APPENDIX A

Tax Lot Map

This map was prepared for Assessment & Taxation purposes only and was NOT prepared nor is it suitable for legal, engineering or surveying purposes.

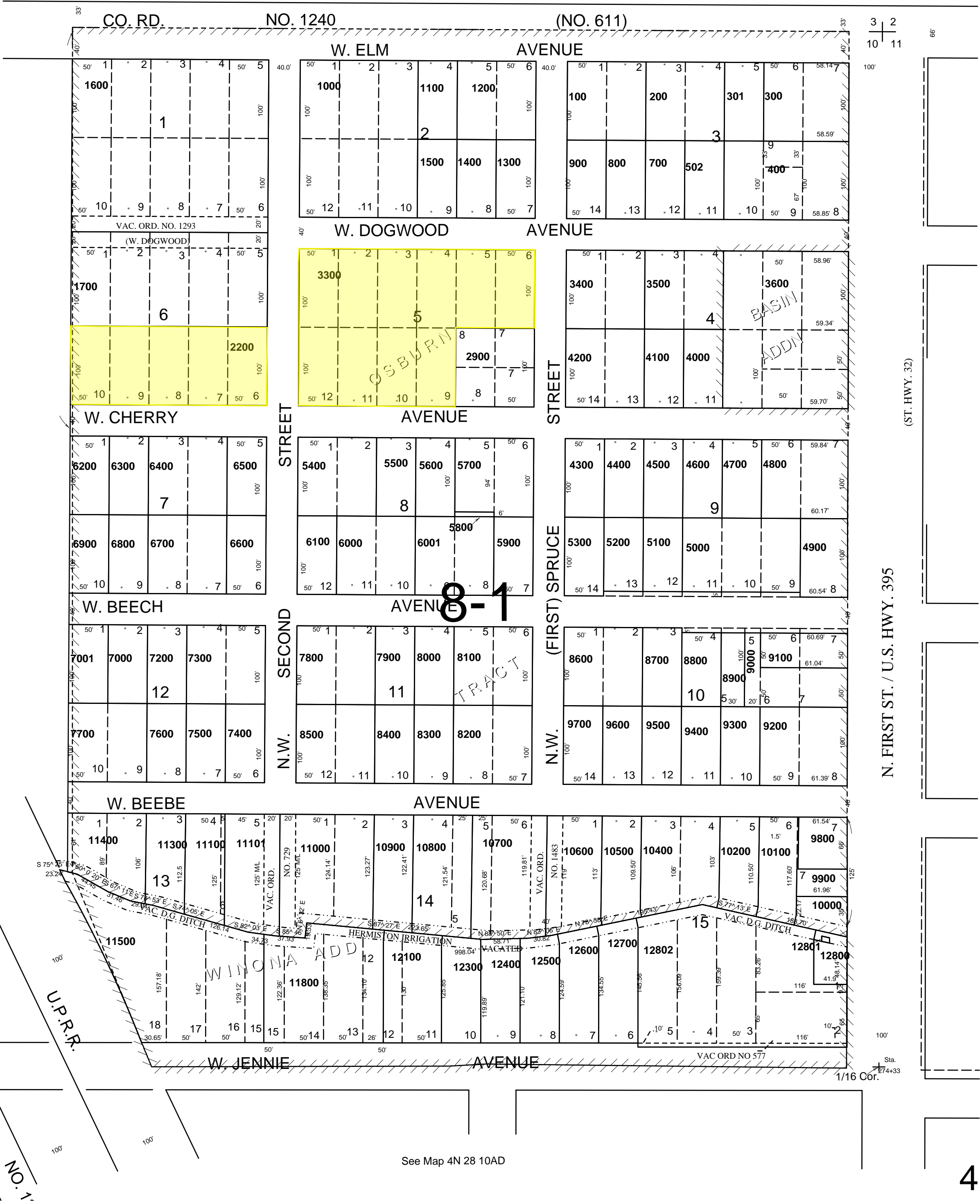


NE1/4 NE1/4 SEC 10 T4N R28E WM
UMATILLA COUNTY, OR

SCALE: 1" = 100'

4N2810AA

See Map 4N 03D



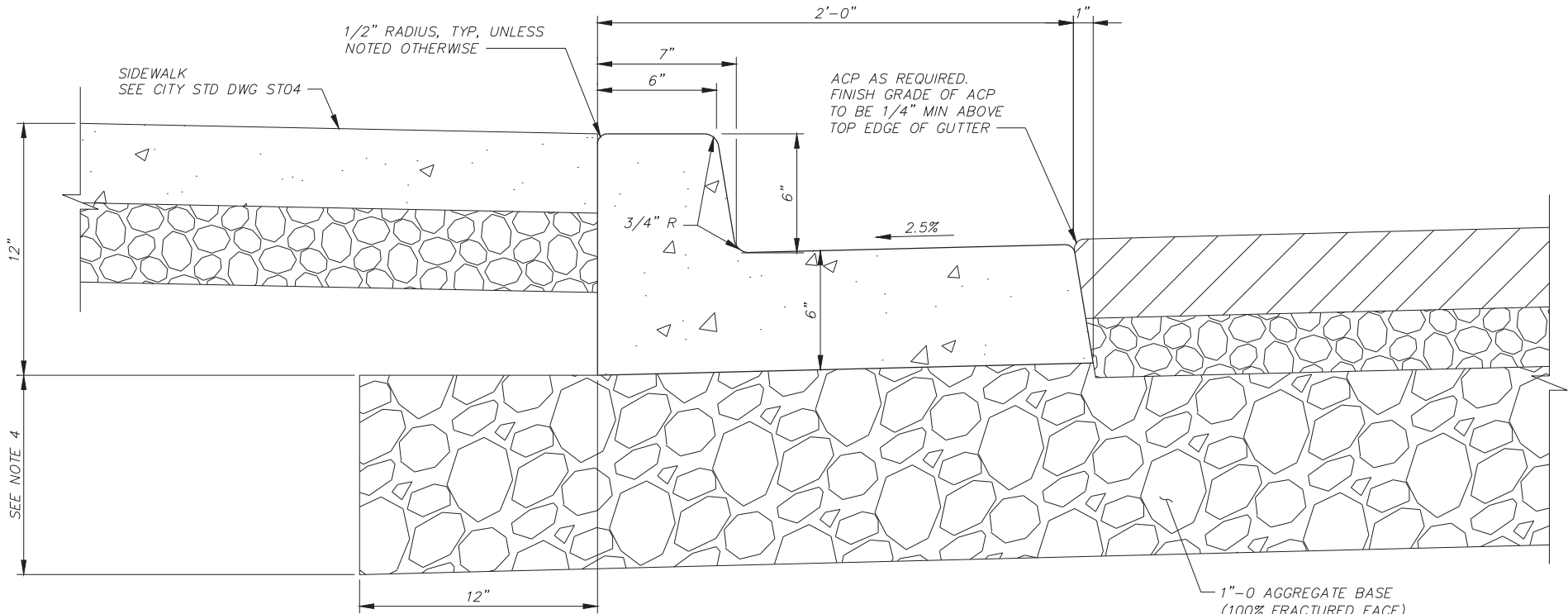
LID Boundary

APPENDIX B

Standard Drawings

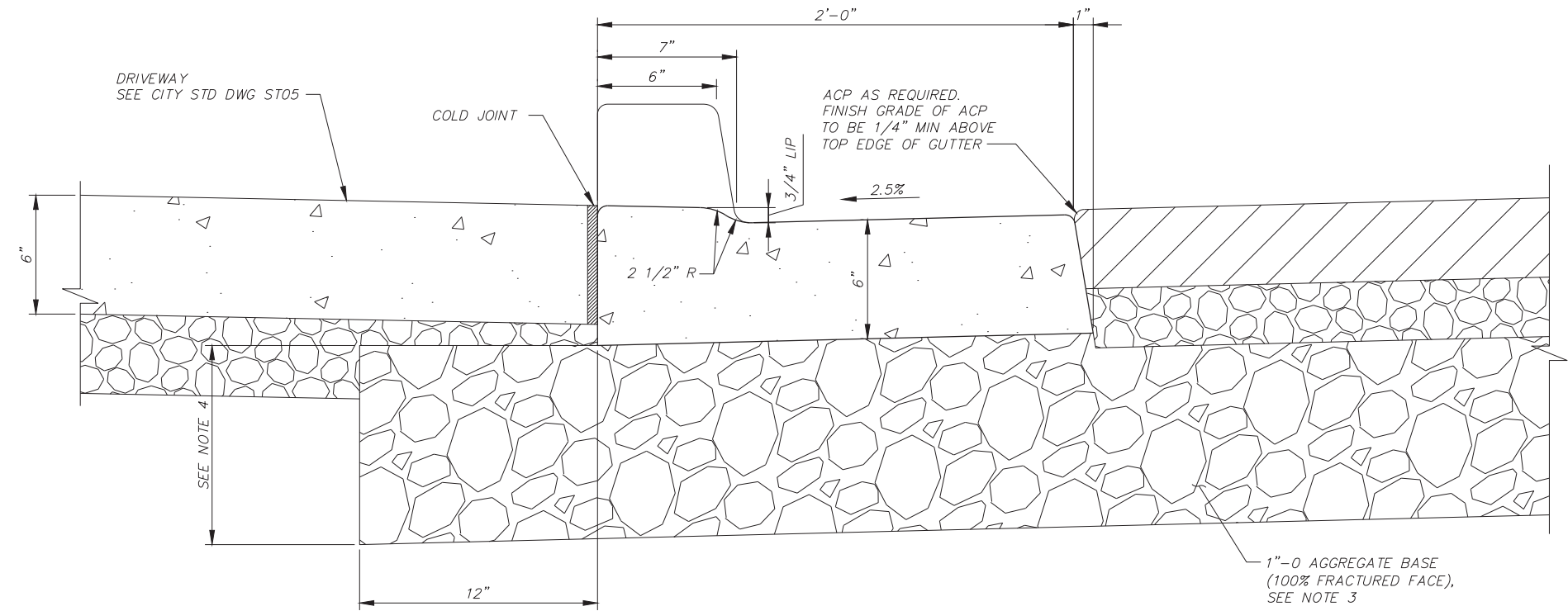
NOTES

1. GUTTER CROSS SLOPE SHALL BE MINIMUM 2% AND MAXIMUM 3% UNLESS OTHERWISE APPROVED BY THE CITY.
2. ALL CONCRETE SHALL BE COMMERCIAL GRADE 4,000 PSI CONCRETE.
3. COMPACT AGGREGATE BASE TO 95% MAXIMUM DRY DENSITY PER ASTM D1557.
4. AGGREGATE BASE THICKNESS BELOW CURB AND GUTTER SHALL BE 6" THICK OR CONFORM TO CITY STD DWG ST07-ST11, WHICHEVER IS GREATER.
5. SEE CITY STD DWG ST04 FOR CURB AND GUTTER JOINT REQUIREMENTS.
6. CONCRETE SHALL BE TROWELED SMOOTH PRIOR TO APPLYING BROOM FINISH.
7. INSPECTION CRITERIA:
 - 7.1. COMPLETED CURB AND GUTTER SHALL NOT CAUSE PUDDLING ISSUES. PUDDLE WIDTH SHALL NOT EXCEED 75% OF THE GUTTER WIDTH AND PUDDLE LENGTH SHALL NOT EXCEED 2'.
 - 7.2. CROSS SLOPE OF THE GUTTER SHALL BE A MINIMUM OF 1.5% AND A MAXIMUM OF 3.5% AS MEASURED BY A 2' ELECTRONIC LEVEL.
 - 7.3. THERE SHALL BE NO BREAK POINTS OR REVERSE SLOPE IN THE GUTTER CROSS SLOPE AS MEASURED BY A 2' ELECTRONIC LEVEL. THE MAXIMUM ALLOWABLE GAP BETWEEN THE LEVEL AND THE GUTTER SURFACE SHALL BE 1/8".
 - 7.4. WHEN A 6 FOOT LONG STRAIGHTEDGE IS LAID ON THE TOP FACE OF THE CURB OR ON THE SURFACE OF THE GUTTER, THE SURFACE SHALL NOT VARY MORE THAN 0.02 FEET FROM THE EDGE OF THE STRAIGHTEDGE EXCEPT AT GRADE CHANGES OR VERTICAL CURVES.
 - 7.5. THE CONTRACTOR SHALL CONSTRUCT ALL CURB AND GUTTER WITHIN 0.02 FEET OF TRUE LINE, WITHIN 0.02 FEET OF ESTABLISHED SURFACE GRADE, CROSS SECTION, AND SLOPE, AND WITHIN 0.02 FEET OF SPECIFIED THICKNESS.
8. WHEN INSTALLING NEW CURB AND GUTTER ON AN EXISTING PAVED STREET, THE ASPHALT SHALL BE CUT A MINIMUM OF 2 FEET FROM THE EDGE OF THE GUTTER OR AS NEEDED TO ACHIEVE COMPACTION OF THE AGGREGATE BASE BY MECHANICAL MEANS.



STANDARD CURB AND GUTTER

NTS



CURB AND GUTTER AT DRIVEWAY

NTS

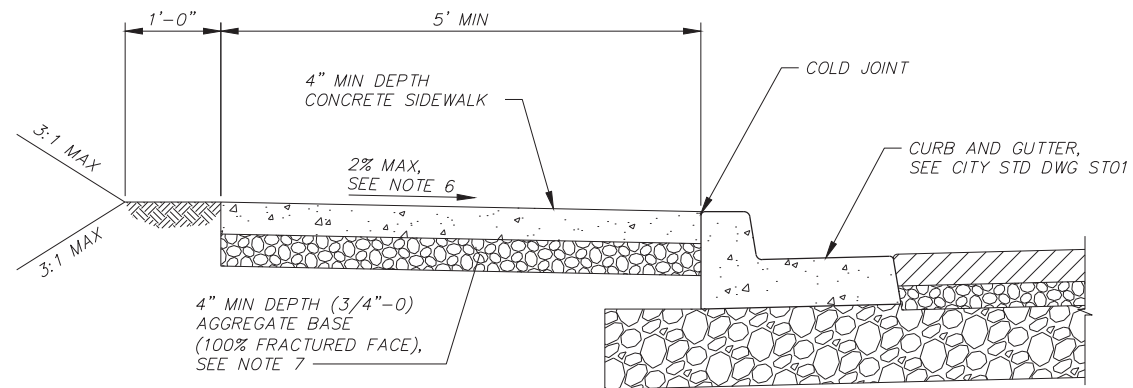


Where Life is Sweet

CITY OF
HERMISTON, OREGON
STANDARD DRAWINGS

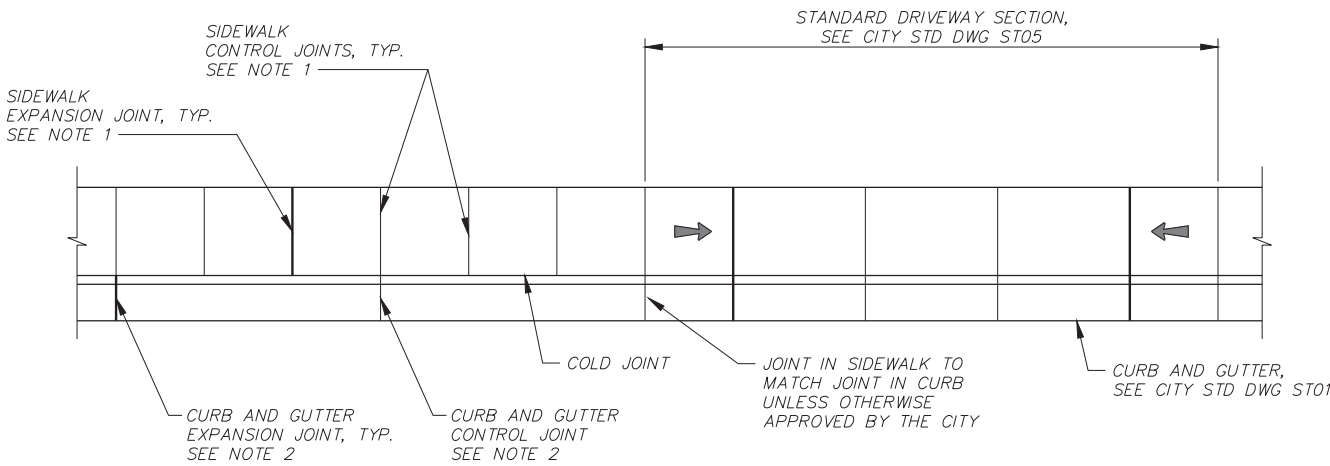
CURB AND GUTTER

FIGURE
ST01



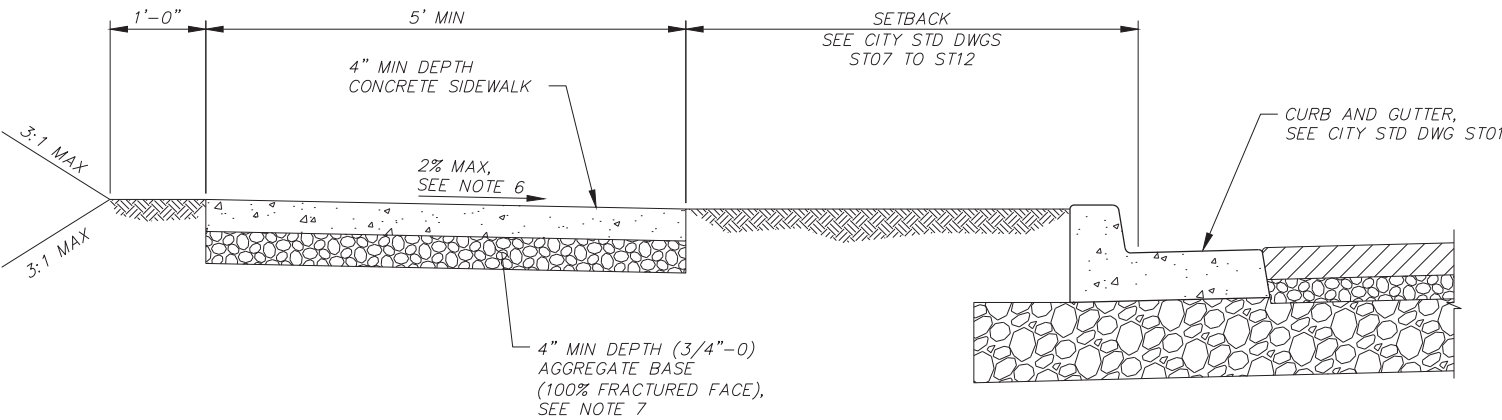
SIDEWALK SECTION

NTS



TYPICAL PLAN VIEW

NTS



SIDEWALK SECTION WITH SETBACK

NTS

NOTES

- SIDEWALK JOINT REQUIREMENTS:
 - EXPANSION JOINT SPACING SHALL BE 45' ON CENTER MAXIMUM, AND SHALL BE PROVIDED AROUND POLES, BOXES, AND ANY FIXTURES WHICH PROTRUDE THROUGH THE SIDEWALK, BETWEEN ANY STRUCTURE IMMEDIATELY ADJACENT TO THE SIDEWALK, AND AT THE ENDS OF EACH DRIVEWAY (AS DETERMINED BY DRIVEWAY WIDTH).
 - FOR SIDEWALKS LESS THAN 8' WIDE, TRANSVERSE CONTROL JOINT SPACING SHALL BE EQUAL TO THE WIDTH OF THE SIDEWALK EXCLUDING THE CURB. FOR SIDEWALKS 8' OR GREATER IN WIDTH TRANSVERSE CONTROL JOINT SPACING SHALL BE EQUAL TO HALF THE WIDTH OF THE SIDEWALK AND A LONGITUDINAL CONTROL JOINT SHALL BE CONSTRUCTED AT THE CENTER OF THE SIDEWALK.
- CURB AND GUTTER JOINT REQUIREMENTS:
 - EXPANSION JOINTS ARE OPTIONAL WHEN USED. SPACING SHALL BE 200' ON CENTER MAXIMUM AND SHALL BE PLACED AT POINTS OF TANGENCY, AT BOTH ENDS OF EACH DRIVEWAY, AT CONNECTION TO EXISTING CURB/GUTTER, AND ADJACENT TO ANY STRUCTURE.
 - CONTROL JOINT SPACING SHALL BE 15' ON CENTER MAXIMUM, AND SHALL BE PLACED AT THE ENDS OF EACH STORMWATER INLET AND CURB RAMP.
- CONCRETE DEPTH FOR SIDEWALKS SHALL BE 4" MIN, SEE ST05 FOR DRIVEWAY THICKNESS.
- BROOM FINISH ALL SIDEWALKS AND DRIVEWAYS.
- WHERE NEW SIDEWALK CONSTRUCTION ABUTS AN EXISTING SIDEWALK NOT MEETING CITY STANDARDS, NEW SIDEWALK CONSTRUCTION MUST TRANSITION TO MEET CITY STANDARDS WITHIN A MAXIMUM DISTANCE EQUAL TO THE WIDTH OF THE SIDEWALK EXCLUDING THE CURB.
- TO ENSURE SLOPES DO NOT EXCEED MAXIMUM ALLOWABLE SLOPES, IT IS RECOMMENDED TO SET SLOPES LOWER THAN MAXIMUM ALLOWABLE SLOPES AS FOLLOWS:
 - FOR 2% MAX, SET SLOPE AT 1.5%
 - FOR 8.33% MAX, SET SLOPE AT 7.5%SIDEWALK WITH SLOPES EXCEEDING MAXIMUM ALLOWABLE VALUES SHALL BE REMOVED AND REPLACED AT THE CONTRACTORS EXPENSE.
- COMPACT AGGREGATE BASE (100% FRACTURED FACE) TO 95% MAXIMUM DRY DENSITY PER ASTM D1557.
- ALL CONCRETE SHALL BE COMMERCIAL GRADE 4,000 PSI CONCRETE.



CITY OF
HERMISTON, OREGON
STANDARD DRAWINGS

SIDEWALK

FIGURE
ST04



Where Life is Sweet

Mayor and Members of the City Council
STAFF REPORT
 For the Meeting of August 11, 2025

Title/Subject

Resolution 2391- SE 10th Street Jurisdictional Transfer

Summary and Background

This resolution initiates the transfer of ownership of the portion of SE 10th Street between Highland Avenue and Columbia Drive from Umatilla County to the City of Hermiston.

Tie-In to Council Goals

City Facilities: public facilities that meet the community needs of today and the future

Fiscal Information

This transfer includes a one-time lump sum compensation from Umatilla County to the City of Hermiston of \$200,000. This transfer is in recognition of the fact that the street section is still the primary access to a number of properties outside of City Limits, and accommodates a significant amount of traffic which either originates or ends (or both) outside of City Limits. Meanwhile, the City of Hermiston will not receive any additional State Gas Tax revenue share by virtue of the fact that cities receive Gas Tax based on population within city limits (which will not be affected by this transfer of ownership responsibilities). Long-term, however, it is anticipated that the unincorporated properties surrounding this section will, over time, annex in to City Limits as they develop in to urban-level housing. Meanwhile, there is also a significant amount of existing traffic which either originates or ends (or both) inside of City Limits which uses this section of roadway. Therefore, this agreement simply helps facilitate an orderly transition of this roadway section to an urban city standard.

Aside from long term maintenance costs, a significant looming cost associated with this section of street is replacement of the failing 1-lane culvert over HID's A-Line canal near the BMCC campus. This replacement is identified as ST12.0 in the adopted Street Capital Improvement Plan, and is currently estimated to cost \$800,000.

In recognition of the increased traffic taking the 1-lane culvert associated with the new Columbia Ridge subdivision, each home in that development is being assessed a \$1,557 fee for a portion of rebuilding the culvert capable of accommodating two lanes with sidewalks. Additionally, each new home will pay a generalized \$423 Transportation System Development

Charge. With approximately 200+ homes planned for Columbia Ridge, it is anticipated that the development will pay approximately \$396,000 associated with off-site transportation improvements.

With an \$800,000 culvert replacement cost offset by \$596,000 in County jurisdictional transfer revenue and development fees from Columbia Ridge, that leaves approximately \$204,000 unaccounted for. This margin will be covered by the City through a combination of anticipated future SDC revenue from future developments along SE 10th between Highland and Columbia as well as recognition of the fact that the roadway already carries a significant volume of “city traffic.” The volume of “city traffic” is only anticipated to increase as the 10th street corridor is anticipated to function as a more and more important alternative North/South route for traffic on the growing eastern side of the community.

Alternatives and Recommendation

Alternatives

1. Approve resolution 2391
2. Reject resolution 2391

Recommended Action/Motion

Motion to approve Resolution 2391

Submitted By:

Mark Morgan

RESOLUTION NO. 2391**A RESOLUTION AUTHORIZING THE CITY OF HERMISTON TO ENTER INTO AN INTERGOVERNMENTAL AGREEMENT WITH UMATILLA COUNTY FOR THE INITIATION OF A TRANSFER OF JURISDICTION OVER A PORTION OF COUNTY ROAD NO. 1219 (SE 10TH STREET) AND TO AUTHORIZE THE CITY MANAGER TO SIGN ALL NECESSARY DOCUMENTS ON BEHALF OF THE CITY**

WHEREAS, Umatilla County (County) currently has jurisdiction over County Road No. 1219 (SE 10th Street) and desires to transfer jurisdiction of a portion of County Road No. 1219 from its intersection with E. Highland Avenue to its intersection with E. Columbia Drive to the City of Hermiston (City); and

WHEREAS, County will agree to pay to the City the sum of \$200,000 as part of the transfer of jurisdiction; and

WHEREAS, current and future development connected to this portion of County Road No. 1219 from its intersection with E. Highland Avenue to its intersection with E. Columbia Drive create the need for an orderly transition of this roadway section to an urban city standard; and

WHEREAS, City staff believe it is in the best interest of the City to approve and execute the Intergovernmental Agreement.

NOW, THEREFORE, THE CITY OF HERMISTON RESOLVES AS FOLLOWS:

1. That the Intergovernmental Agreement attached to this resolution between the City of Hermiston and Umatilla County to initiate the transfer of jurisdiction of the portion of County Road No. 1219 from its intersection with E. Highland Avenue to its intersection with E. Columbia Drive.
2. That the City Manager is authorized to sign the Intergovernmental Agreement.
3. That this resolution is effective immediately upon its passage.

PASSED by the Common Council this 11th day of August 2025.

SIGNED by the Mayor this 11th day of August 2025.

Doug Primmer, MAYOR

ATTEST:

Lilly Alarcon-Strong, CMC, CITY RECORDER

Agreement
City of Hermiston
County of Umatilla
SE 10th Street Road Jurisdiction

This agreement is executed this _____ day of July, 2025, between the City of Hermiston, an Oregon Municipal Corporation ("City") and Umatilla County, a political subdivision of the State of Oregon ("County"), for the transfer of a portion of County Road No. 1219 (SE 10th Street), from the County to the City.

Now, therefore, the City of Hermiston and Umatilla County agree as follows:

1. The County will transfer and the City will accept, jurisdiction of a portion of County Road No. 1219 from its intersection with E. Highland Avenue to its intersection with E. Columbia Drive.

2. Transfer of jurisdiction will be full and absolute for all purposes of repair, construction, improvement, and the levying and collection of assessments.

3. The County will pay to the City the sum of \$200,000 from bicycle/footpath funds.

4. Each party agrees to prepare and to execute any documents necessary to complete the transfer of jurisdiction.

Approved as authorized by action of the Hermiston City Council _____, 2025.

Attest:

City Recorder

Approved on behalf of Umatilla County

Umatilla County

Celinda A. Timmons, Chair

Date

Daniel N. Dorran, Commissioner

Date

John M. Shafer, Commissioner

Date

ATTEST:

Office of County Records

Records Officer



Where Life is Sweet

Mayor and Members of the City Council
STAFF REPORT
 For the Meeting of August 11, 2025

Title/Subject

Report on and Discussion of City Chats FY24-25

Summary and Background

One of the ideas generated by the Hermiston Leadership Academy Class of 2024 was to hold something like “coffee with a councilor” around the community. We named our effort “City Chats” and held four at different locations, different times of day and throughout the year. Below is a summary of each chat:

- Monday, November 18, 2024 – Maple Moose Coffee
 The turnout was very light. We had one person other than city councilors and city staff and he was the new chair of the City’s Hispanic Advisory Committee. We had a few hiccups on getting the word out.
- Thursday, January 16, 2025 – Java Junkies-Hermiston
 There were six residents along with city councilors and staff. The topics that came up were finances, animal control, homelessness and communication/engagement.
- Wednesday, April 9, 2025 – Next Chapter Bookstore
 There were eleven residents in attendance with a full complement of elected officials and two city staff members. Discussion topics were quite wide ranging as follows: Main Street tree maintenance, Geer and Harper intersection, Budget, Grant writing, truck traffic, Tariffs and city projects, Code enforcement, burning, pedestrian crossings, speed bumps in neighborhoods, street lighting, neighborhood watch and crime, citizen communication/engagement.
- Thursday, June 26, 2025 – Cuppa Yo-Hermiston
 There were four city residents in addition to the councilors and myself. The topics that came up were a variety: vehicle noise from vehicles (car stereos and exhaust systems as well as semi-trucks and their engine brakes), truck routes (signage) and truck noise, handouts at committee meetings, specific code enforcement concerns.

I think we got much better over the year making sure we advertised the city chat times and locations. For the most part, participation and interest in the program grew over the year. We even received some media notice of the program in November from Apple Valley News.

Tie-In to Council Goals

One complete subarea of our goals and actions is titled “Community Engagement: Improve Engagement and Connection with the Community”

Fiscal Information

There are very few direct costs. Most of the cost is staff time.

Alternatives and Recommendation**Alternatives**

The city council may choose to:

1. Discontinue City Chats
2. Continue Quarterly City Chats in FY25-26
 - a. If this is your preferred path, please be ready with suggestions for locations.

Recommended Action/Motion

Recommend continuation of City Chat program with the following four general dates:

- Mid-September 2025
- Mid-January 2026
- Late March 2026
- Late May 2026

Submitted By:

Byron D. Smith, City Manager