



CITY COUNCIL & URBAN RENEWAL AGENCY

Community Center – 415 S. Hwy 395

Where Life is Sweet

September 26, 2022 at 7:00 PM

AGENDA

*Other ways of viewing or participating in live meetings are available through:
YouTube at: <https://bit.ly/HermistonYoutube>*

*Zoom with Meeting ID: 862 026 140 4112 Passcode: 203345 Telephone number to join is: 1
253 215 8782; or submitting comments to meetings@hermiston.or.us*

1. CALL REGULAR MEETING TO ORDER – 7:00 PM

2. DECLARATION OF QUORUM

3. FLAG SALUTE

4. CITIZEN INPUT ON NON-AGENDA ITEMS

Anyone wishing to bring anything before the council that is not on the agenda is asked to please do the following: 1. Please limit comments to not more than FIVE minutes; 2. State your name and address; 3. Direct your comments to the Chair.

5. CONSENT AGENDA

A. Committee Vacancy Announcements

B. Confirmation of Appointment to the Airport Advisory Committee: Ron Linn to position #5, expiring October 31, 2025

C. Final Plat- Legacy Phases 5 & 6 4N2802BA Tax Lot 400 – NE 5th St/NE 6th ST/E Bella Vista Ave

D. Liquor License "Greater Privilege" Application for Ye Olde Pizza Shoppe

E. Minutes of the September 12 Work Session and Regular Meetings

6. ITEMS REMOVED FROM CONSENT AGENDA

7. ORDINANCES AND RESOLUTIONS

A. Resolution 2239- Long Term Rural Enterprise Zone Agreement: PDX245

B. Resolution 2240- Long Term Rural Enterprise Zone Agreement- PDX146

8. OTHER

- A.** Umatilla River Bridge Report Acceptance
- B.** August 2022 Financial Report
- C.** Approval of July 1, 2022 to June 30, 2025 Collective Bargaining Agreement (CBA) with the Hermiston Police Association (HPA)

9. COMMITTEE REPORTS

A. City Committee and Liaison:

Airport Advisory, Budget, Hispanic Advisory, Library Board, Parks and Recreation, Planning Commission, Recreation Projects Fund, Faith-Based Advisory, Community Enhancement, Community Accountability, Public Safety, Public Infrastructure, Transit Planning, EOTEC

B. Mayor's Report

C. Council Report

D. Youth Advisory Report

E. Manager's Report

10. ADJOURN CITY COUNCIL MEETING AND CONVENE URBAN RENEWAL AGENCY MEETING

11. CALL TO ORDER

12. NEW BUSINESS

- A.** Facade Grant- Stanton Smith 4N2811CB Tax Lot 6900- 395 E Main St

13. ADJOURN URBAN RENEWAL AGENCY MEETING

**** AMERICANS WITH DISABILITIES ACT NOTICE ****

Please contact Hermiston City Hall, 235 E Gladys Ave, Hermiston, OR 97838 (Phone No. 541-567-5521) at least 48 hours prior to the scheduled meeting time if you need an accommodation. TTY and TDD users please call Oregon Telecommunications Relay Service at 1-800-735-2900 or 711.



PUBLIC ANNOUNCEMENT

The City is accepting applications for the following Committees:

1) Airport Advisory Committee

- Position 3: remaining 3-year term ending October 31, 2023 (Advertised as of 09/13/2022)

2) Budget Committee

- Position 5: remaining 3-year term ending December 31, 2023 (Vacant as of 10/25/2021)

3) Hispanic Advisory Committee

- Position 3: remaining 3-year term ending June 30, 2024 (Vacant as of 01/19/2022)
- Position 5: 3-year term ending June 30, 2025 (Advertised as of 04/19/2022)

4) Library Board

- Position 3: remaining 4-year term ending June 30, 2025 (Vacant as of 04/05/2021)

Deadline to apply for Committee(s) in Section 1: October 10, 2022

Deadline to apply for Committee(s) in Section(s) 2-4: Open until filled

Interested persons are asked to submit an application to City Hall, 235 E Gladys Ave, Hermiston, or at lalarcon-strong@hermiston.or.us . Application forms are available at City Hall or on the City's website at <https://hermiston.or.us/volunteer> . If you have questions, please call Lilly Alarcon-Strong at 541-567-5521.

Proposed appointment and confirmation of these positions are made by the City Council. All appointments to city boards and commissions shall be made in accordance with the ordinances and city charter. Appointees shall not be full-time employees of the city, shall not be elected officials of the city, shall not be appointed to more than two boards or commissions at a time, and shall not sell to the city or its boards and commissions over which the council has appointive powers and budget control either directly as a prime contractor or supplier, or indirectly as a first-tier subcontractor or supplier. Sales shall be construed to mean sales, services or fees aggregating \$7,500 or more in any one calendar year. Preference for appointees shall be given to city residents.



Where Life is Sweet

Mayor and Members of the City Council
STAFF REPORT
 For the Meeting of September 26, 2022

Title/Subject

Final Plat- Legacy Phases 5 & 6 4N2802BA Tax Lot 400 – NE 5th St/NE 6th ST/E Bella Vista Ave

Summary and Background

PBS Engineering has submitted a final plat for Phases 5 & 6 of Legacy Subdivision. The property is described as 4N2802BA Tax lot 400 and is zoned Medium-High Density Residential (R-3). The plat creates 42 lots ranging in size from 5,036 square feet to 43,256 square feet.

The planning commission's approval of the preliminary plat through the planned unit development process in February of 2020 subject to 19 conditions of approval.

The criteria that are applicable to the decision to accept the final plat are contained in 154.46 of the Hermiston Code of Ordinances which governs the information presented on the final plat.

The preliminary plat conditions and the final plat requirements are attached to this report as Exhibit A. The map showing the property boundaries and adjacent streets and parcels is attached as Exhibit B. An aerial photo is attached as Exhibit C. The final plat as prepared by the surveyor is attached as Exhibit D.

Tie-In to Council Goals

Approval of plats is a matter of administration of city ordinances.

Fiscal Information

Forty-two R-3 lots will result in forty-one new housing units priced between \$300,000 and \$400,000 and a city park one acre in size. At full build-out the development will generate approximately \$98,000 in revenue to the city each year.

Alternatives and Recommendation

Alternatives

The city council may choose to approve or deny the final plat.

Recommended Action/Motion

Section 5, Item C.

Staff recommends the city council approve the final plat.

Submitted By:

Clinton Spencer, Planning Director

EXHIBIT A**Conditions of Approval & Findings of Fact****Legacy Subdivision – Planned Unit Development****September 26, 2022***Conditions of Preliminary Plat Approval- February 12, 2020*

1. Applicant shall work with and receive certification from the Hermiston Irrigation District prior to final plat approval. Applicant should be aware that the City of Hermiston will not sign the final plat until the irrigation district has been satisfied and signs the final plat.

Finding: The applicant shall meet this condition of approval. The necessary signature block for the Irrigation District is shown as required.

2. Applicant must sign an improvement agreement and shall install grading, storm drainage, curb and gutter, sidewalks, street paving, and all service utilities for this development. All improvements for each phase shall comply with city standards and specifications and shall receive final approval from the city engineer.

Finding: This condition has been satisfied.

3. Streetlights shall be installed at the applicant's cost. Once installed, the City will assume the monthly service charges.

Finding: Construction of improvements is underway. All construction conditions shall be satisfied prior to the city signing the final plat.

4. Applicant shall comply with all provisions of 92.12 of the Hermiston Code of Ordinances (relating to the control of blowing dust) during all phases of construction.

Finding: This condition shall remain in effect through the duration of construction.

5. All streets shall be designated in accordance with §94.22 of the Hermiston Code of Ordinances as follows:

- "A" Street shall be designated as NE 5th Street
- "B," "C," "D," and "E" Streets shall be designated as a NE Avenue
- The southernmost portion of "A" Street, where the street curves to an east-west shall be designated as a NE Avenue
- E Bella Ave shall be designated as E Bella Vista Ave

Finding: NE 5th Street, E Legacy Avenue and E Templeton Avenue, E Heather Ave, E Rosa Ave and E Bella Vista Ave are correctly designated on the plat. The southernmost portion of "A" Street shall be designated as a NE Avenue on the final plat prior to city signature.

6. The applicant shall commission a geotechnical report for E Punkin Center Road which is a county road with an unknown condition of asphalt and base rock. If the condition of the existing street is determined to be sufficient to meet city standards for a major collector, infill paving, curb, gutter, and sidewalk shall be required. If the condition of the existing street is determined to be insufficient to meet city standards for a major collector, the developer shall

rebuild the street to city standards to the centerline. City standards shall include sufficient aggregate base and paving, curb, gutter, sidewalk, and drainage improvements.

Finding: This condition has been satisfied.

7. The applicant shall be responsible for the extension of a new sanitary sewer main from the intersection of NE 5th Street to the west boundary of Lot 6 and the east boundary of Lot 1 in the E Punkin Center right of way at the time E Punkin Center improvements in condition #6 are installed.

Finding: This condition has been satisfied.

8. Comprehensive Plan Figure 12 identifies this site as an area subject to development hazards due to excessively well drained soils. Therefore, the City will prohibit the outdoor storage of hazardous chemicals and underground storage of gasoline and diesel fuels.

Finding: This condition shall remain in effect.

9. The developer shall improve and dedicate to the city a paved, multi-use trail of at least eight feet in width along the east side of NE 5th Street. The Developer shall construct the trail in phases and install each portion of the multi-use trail, associated landscape and irrigation at the developer's expense and concurrent with the public improvements within the underlying phase of the development. The Parks Department shall approve the landscape plan, and irrigation plan during the civil engineering process. At such a time when each phase of the trail has been constructed, the City shall assume all maintenance responsibilities for the trail, landscaping and irrigation.

Finding: This condition has been satisfied.

10. The developer shall improve and dedicate a one-acre park as shown on the approved PUD plan. Improvement of the park shall be concurrent with the construction of improvements for the phase containing the park. As shown on the approved development plan, the park is located in phase 6. Should the developer amend the phasing plan, the park shall be improved simultaneously with whichever phase contains the improvement of E Bella Vista Ave.

Finding: The city and developer have an agreement in place for improvement of the park. The dedication of the park is shown on the plat.

11. The city shall reimburse the developer for that specific portion of the trail which encircles the park by way of SDC credits. The city shall collect SDCs for each dwelling constructed in each phase of Legacy Park. At such time that the park trail is completed and approved by the city, the city shall issue a reimbursement of the actual cost of construction for the trail based upon contractor invoices for construction.

Finding: This condition shall remain in effect. The city and developer have an agreement in place for reimbursement of development credits.

12. The city and developer shall enter into an improvement agreement, which shall run with the land, detailing the responsibility of each party in the development of the park. The

improvement agreement shall conform to the provisions of the letter of understanding regarding park improvements and shall specify at a minimum:

- a. Scope of landscaping to be installed
- b. Developer's responsibility to provide utility stubs to the site for future use
- c. City's responsibility to install grass and irrigation
- d. City's responsibility to install a playground within two years of park dedication

Finding: This condition has been satisfied.

- 13. The developer shall install sufficient sewer mainline in the future SE 5th Street right of way to provide sanitary sewer service for phase 1 connecting to existing sanitary sewer service in E Bella Vista Ave or NE 6th Street. Design of the sewer extension shall be designed in accordance with city public works standards and obtain approval from the city engineer.

Finding: This condition has been satisfied.

- 14. Developer shall obtain an access permit from the Umatilla County Road Department for the new street approach of NE 5th Street to E Punkin Center Road.

Finding: This condition has been satisfied.

- 15. Lots 1 through 6 shall not have driveway access to E Punkin Center Road. The developer shall place a one-foot wide no vehicular access easement on the plat for phase 1 for Lots 1 through 6 along E Punkin Center Road.

Finding: This condition has been satisfied.

- 16. Lots 1, 15, 16, and 64-68 shall be single story houses and shall be fenced on the east property line.

Finding: This condition has been satisfied.

- 17. The C Street extension shall be removed and the lots adjusted to the north.

Finding: This condition has been satisfied.

- 18. D Street shall be extended to the east.

Finding: This condition has been satisfied.

- 19. A two-foot retaining wall with wooden fence on top shall be installed on E Punkin Center Road.

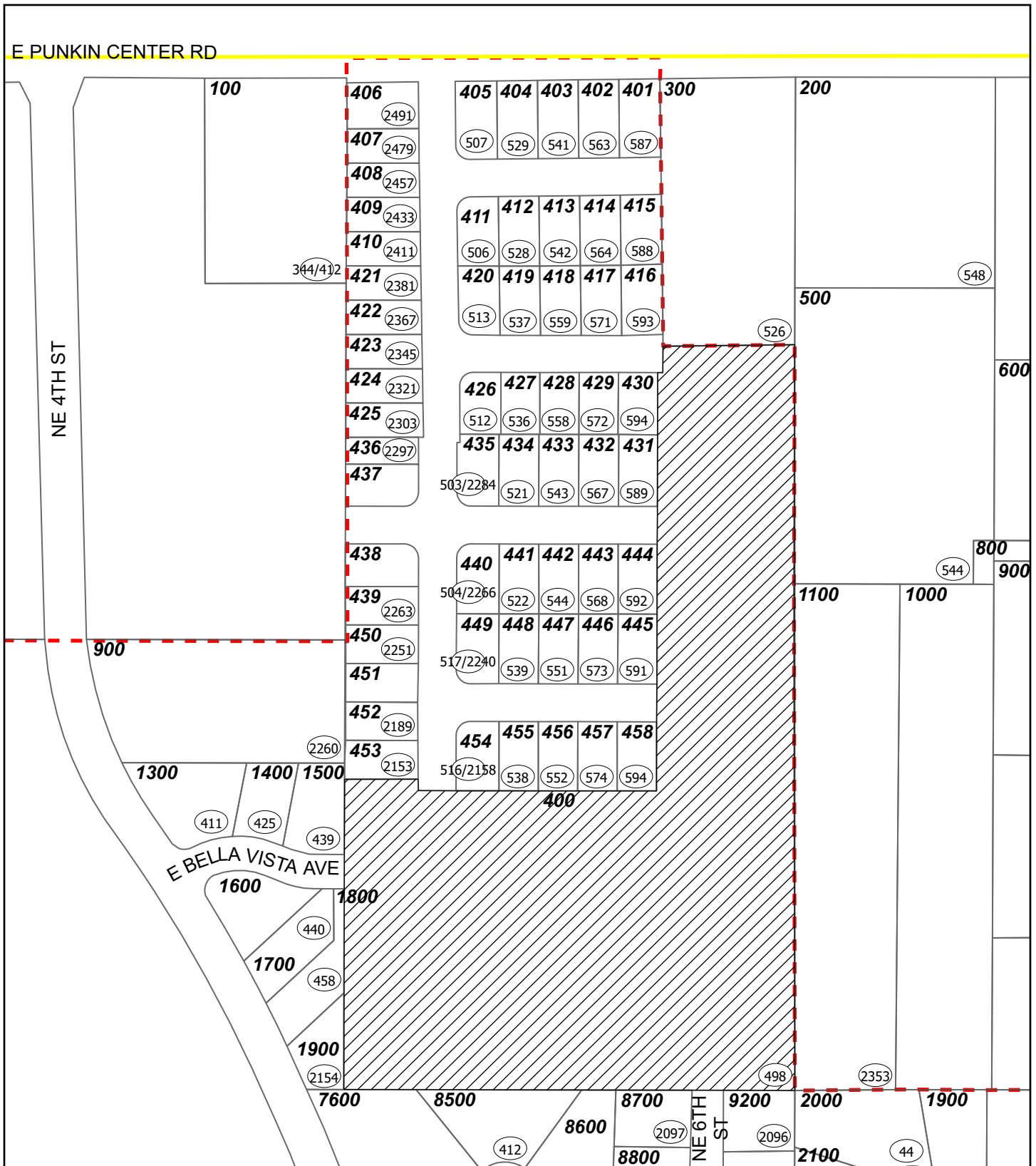
Finding: This condition is satisfied.

§154.46 Final Plat Requirements

Per §154.46 of the Hermiston Code of Ordinances, the final plat shall show:

- (A) The boundary lines of the area being subdivided, with accurate distances and bearings;
- (B) The lines of all proposed streets and alleys with their width and names;
- (C) The accurate outline of any portions of the property intended to be dedicated or granted for public use;
- (D) The line of departure of one street from another;
- (E) The lines of all adjoining property and the lines of adjoining streets and alleys with their widths and names;
- (F) All lot lines together with an identification system for all lots and blocks;
- (G) The location of all building lines and easements provided for public use, services or utilities;
- (H) All dimensions, both linear and angular, necessary for locating the boundaries of the subdivision, lots, streets, alleys, easements, and other areas for public or private use. Linear dimensions are to be given to the nearest 1/10 of a foot;
- (I) All necessary curve data;
- (J) The location of all survey monuments and bench marks together with their descriptions;
- (K) The name of the subdivision, the scale of the plat, points of the compass, and the name of owners or subdivider;
- (L) The certificate of the surveyor attesting to the accuracy of the survey and the correct location of all monuments shown;
- (M) Private restrictions and trusteeships and their periods of existence. Should these restrictions or trusteeships be of such length as to make their lettering on the plat impracticable and thus necessitate the preparation of a separate instrument, reference to such instrument shall be made on the plat;
- (N) Acknowledgment of the owner or owners to the plat and restrictions, including dedication to public use of all streets, alleys, parks or other open spaces shown thereon, and the granting of easements required; and
- (O) Certificates of approval for endorsement by the City Council and certificate indicating its submission to the Planning Commission, together with approval for endorsement by other local, county and/or state authority as required by Oregon statutes.

Finding: All items required for the final plat are shown on the final plat.



NOTE:
SEE SHEET 3 FOR SURVEYOR'S
CERTIFICATE, RECORD DOCUMENTS,
VICINITY MAP, AND SIGNATURES

- LEGEND
- PROPERTY LINE
 - SECTION LINE
 - CENTERLINE
 - RIGHT OF WAY
 - EASEMENT
 - EXISTING LOT LINE
 - FENCE LINE

P.U.E. PUBLIC UTILITY EASEMENT

() INDICATES RECORD DIMENSION

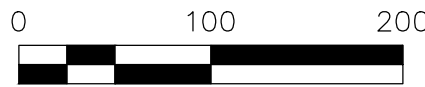
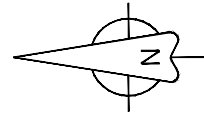
● MONUMENT FOUND AS DESCRIBED

⊗ FOUND 5/8" IRON REBAR WITH CAP PER
PLAT BK. 17 PG. 40

● SET 5/8" X 30" IRON REBAR WITH YELLOW
PLASTIC CAP STAMPED "WA19619 OR02820"

⊕ SET 5/8" X 30" IRON REBAR WITH 1 1/2"
ALUMINUM CAP STAMPED "OR02820"

LEGACY SUBDIVISION PHASE 5 AND 6
OF PROPERTY LOCATED IN THE NORTHEAST QUARTER OF THE
NORTHWEST QUARTER OF SECTION 2, TOWNSHIP 4 NORTH, RANGE 28
EAST, WILLAMETTE MERIDIAN, CITY OF HERMISTON, COUNTY OF UMATILLA,
STATE OF OREGON



BASIS OF BEARINGS

OREGON NORTH GRID BEARING OF S89°28'36"W BETWEEN 2.5" BRASS CAP
FOUND AT THE INTERSECTION OF SAGEBRUSH ROAD AND EAST PUNKIN CENTER
ROAD, BEING THE SOUTHWEST CORNER OF SECTION 35, TOWNSHIP 5 NORTH,
RANGE 28 EAST AND THE 2.5" BRASS CAP FOUND AT THE INTERSECTION OF
NORTH 7TH STREET AND EAST PUNKIN CENTER ROAD, BEING THE NORTH
QUARTER CORNER OF SECTION 2, TOWNSHIP 4 NORTH, RANGE 28 EAST.
DISTANCES SHOWN ARE GROUND DISTANCES DERIVED FROM GRID DISTANCES
USING A COMBINED SCALE FACTOR OF 1.0000532262.

EQUIPMENT USED

THIS SURVEY WAS CONDUCTED USING A TRIMBLE R-6
ROBOTIC TOTAL STATION HAVING AN ACCURACY OF
0"00'03" AND 2MM AND A SPECTRA PRECISION SP80
HAVING AN ACCURACY OF 8MM+1PPM.

| LINE | BEARING | DISTANCE |
|------|-------------|----------|
| L1 | N89°34'36"E | 7.47' |
| L2 | S00°28'24"E | 17.07' |

FOUND 2.5" BRASS CAP AT THE INTERSECTION
OF 7TH STREET AND PUNKIN CENTER ROAD,
MARKING THE NORTH 1/4 CORNER OF SECTION
2, STAMPED "T5N R28E 1/4 S2 PLS 1980"
PER COUNTY SURVEY NO. S-156-C

NORTHEAST CORNER
OF LOT 8 PER SURVEY
N-22-A

TAX ACCOUNT #118940
BRANDI K SINNER

1" IRON PIPE FOUND
(UNRECORDED)

TAX ACCOUNT #118944
REYMUENDO CORREA

5/8" REBAR W/
CAP 2817 FOUND
(UNRECORDED)

TAX ACCOUNT #118631
VICTORIANO MENDOZA

TAX ACCOUNT #160364
NICHOLAS CHOREY

5/8" REBAR W/ CAP 2817
FOUND PER (R6)

TAX ACCOUNT #160359
KRISTEN JOY DUNTEN, ET AL.

5/8" REBAR FOUND PER (R6)

TAX ACCOUNT #160358
TERRY W CUMMINGS, ET AL.

FENCE LINE IS CONTIGUOUS
WITH PROPERTY BOUNDARY

TAX ACCOUNT #160357
JEREMY E HARRIS, ET AL.

5/8" REBAR FOUND PER (R6)

TAX ACCOUNT #158643
ANA LAURA DE LA PAZ, ET AL.

5/8" REBAR
FOUND PER (R7)

5/8" REBAR W/
CAP 2817 FOUND
PER (R6)

5/8" REBAR W/
CAP 43406
FOUND PER (R7)

TAX ACCOUNT #163252
DIANE W BROWN

TAX ACCOUNT #163253
ROBERT N MAGID

MAG NAIL W/
WASHER FOUND ON
CENTERLINE OF BELLA

FOUND 5/8" X 30" IRON REBAR WITH
1 1/2" ALUMINUM CAP STAMPED
"OR02820" PER PLAT, BK. 17 PG. 40

TAX ACCOUNT #158242
KINGDOM HALL OF
JEHOVAH'S WITNESSES

TAX ACCOUNT
#163249
JERALD L
BRANDT, ET AL.

TAX ACCOUNT #118952
RHONDA SUE TEMPLETON

5/8" REBAR W/
CAP 48509 FOUND
PER (R8)

5/8" REBAR W/
CAP 48509 FOUND
PER (R8)

FOUND 2.5" BRASS CAP AT THE
INTERSECTION OF SAGEBRUSH ROAD
AND PUNKIN CENTER ROAD STAMPED
"T5N R28E SS C S34 S35 S2 1980"
PER SURVEY NO. S-156-C

FOUND 2.5" BRASS CAP IN PUNKIN CENTER
ROAD ±200' EAST OF THE CENTERLINE OF
HWY 395 PER COUNTY SURVEY NO. S-156-C

I CERTIFY THAT THIS IS A
TRUE AND EXACT COPY OF
THE ORIGINAL PLAT

GREG E. FLOWERS PLS

REGISTERED
PROFESSIONAL
LAND SURVEYOR

DRAFT

OREGON
JUNE 30, 1997
GREG E. FLOWERS
02820LS

RENEWS 12/31/2023

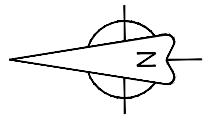


**PBS Engineering and
Environmental Inc.**
5 N Colville St, Ste 200
Walla Walla, WA 99362
509.956.3026

pbsusa.com

| | | |
|-------------------------------|-------------------|------------------------|
| CLIENT: LEGACY HOME GROUP LLC | | PROJECT NO.: 67573-002 |
| SURVEYOR: GREG E. FLOWERS | | DATE: 6/3/2022 |
| CALC BY: MFW | DRAWN BY: AA | SCALE: 1" = 100' |
| SECTION: 2 | TOWNSHIP: 4 NORTH | RANGE: 28 EAST |
| CITY: HERMISTON | COUNTY: UMATILLA | SHEET 1 OF 3 |

LEGACY SUBDIVISION PHASE 5 AND 6
OF PROPERTY LOCATED IN THE NORTHEAST QUARTER OF THE
NORTHWEST QUARTER OF SECTION 2, TOWNSHIP 4 NORTH, RANGE 28
EAST, WILLAMETTE MERIDIAN, CITY OF HERMISTON, COUNTY OF UMATILLA,
STATE OF OREGON

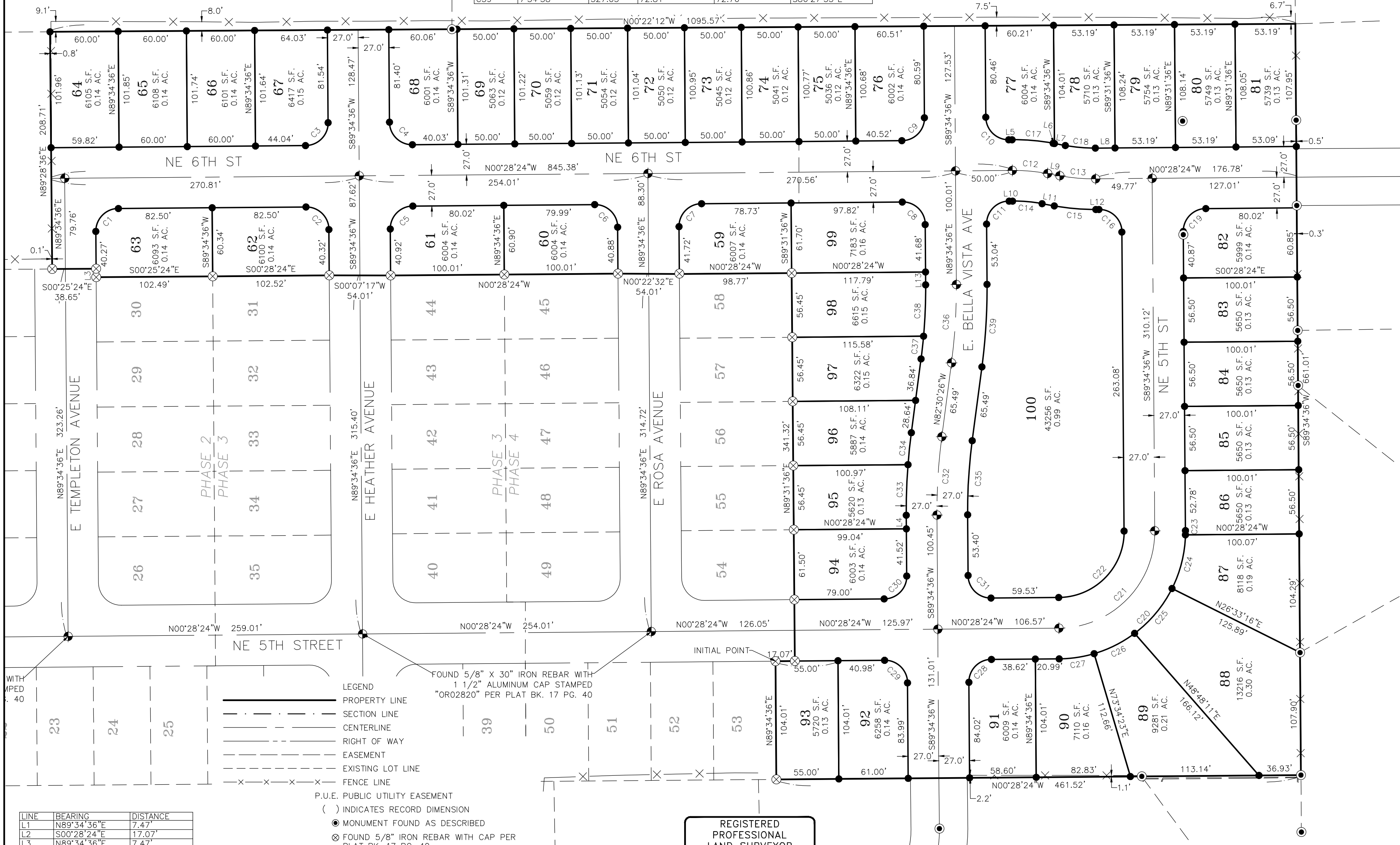


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QUARTER CORNER OF SECTION 2, TOWNSHIP 4 NORTH, RANGE 28 EAST.
DISTANCES SHOWN ARE GROUND DISTANCES DERIVED FROM GRID DISTANCES
USING A COMBINED SCALE FACTOR OF 1.0000532262.

| CURVE | DELTA ANGLE | RADIUS | ARC LENGTH | CHORD LENGTH | CHORD BEARING |
|-------|-------------|---------|------------|--------------|---------------|
| C1 | 89°57'00" | 20.00' | 31.40' | 28.27' | S45°26'54"E |
| C2 | 90°03'00" | 20.00' | 31.44' | 28.30' | N44°33'06"E |
| C3 | 89°57'00" | 20.00' | 31.40' | 28.27' | N45°26'54"W |
| C4 | 90°03'00" | 20.00' | 31.44' | 28.30' | N44°33'06"E |
| C5 | 89°57'00" | 20.00' | 31.40' | 28.27' | S45°26'54"E |
| C6 | 90°03'00" | 20.00' | 31.44' | 28.30' | N44°33'06"E |
| C7 | 89°57'00" | 20.00' | 31.40' | 28.27' | S45°26'54"E |
| C8 | 90°03'00" | 20.00' | 31.44' | 28.30' | N44°33'06"E |
| C9 | 89°57'00" | 20.00' | 31.40' | 28.27' | N45°26'54"W |
| C10 | 90°03'00" | 20.00' | 31.44' | 28.30' | S44°33'06"W |
| C11 | 89°57'00" | 20.00' | 31.40' | 28.27' | S45°26'54"E |
| C12 | 10°49'47" | 165.01' | 31.19' | 31.14' | N04°56'30"E |
| C13 | 10°49'47" | 167.46' | 31.65' | 31.61' | S04°56'30"W |
| C14 | 10°49'47" | 138.01' | 26.09' | 26.05' | S04°56'30"W |
| C15 | 10°49'47" | 194.46' | 36.76' | 36.70' | N04°56'30"E |
| C16 | 90°03'00" | 20.00' | 31.44' | 28.30' | N44°33'06"E |
| C17 | 10°49'47" | 192.01' | 36.29' | 36.24' | S04°56'30"W |
| C18 | 10°49'47" | 140.46' | 26.55' | 26.51' | N04°56'30"E |
| C19 | 89°57'00" | 20.00' | 31.40' | 28.27' | S45°26'54"E |

| CURVE | DELTA ANGLE | RADIUS | ARC LENGTH | CHORD LENGTH | CHORD BEARING |
|-------|-------------|---------|------------|--------------|---------------|
| C20 | 89°57'00" | 112.01' | 175.84' | 158.33' | S45°26'54"E |
| C21 | 89°57'00" | 85.00' | 133.45' | 120.16' | S45°26'54"E |
| C22 | 89°57'00" | 58.00' | 91.06' | 81.99' | S45°26'54"E |
| C23 | 1°54'15" | 112.01' | 3.72' | 3.72' | N89°28'17"W |
| C24 | 25°04'26" | 112.01' | 49.02' | 48.63' | N75°58'56"W |
| C25 | 26°32'28" | 112.01' | 51.88' | 51.42' | N50°10'30"W |
| C26 | 20°28'39" | 112.01' | 40.03' | 39.82' | N26°39'57"W |
| C27 | 15°57'13" | 112.01' | 31.19' | 31.09' | N08°27'01"W |
| C28 | 89°57'00" | 20.00' | 31.40' | 28.27' | S45°26'54"E |
| C29 | 90°03'00" | 20.00' | 31.44' | 28.30' | N44°33'06"E |
| C30 | 89°57'00" | 20.00' | 31.40' | 28.27' | S45°26'54"E |
| C31 | 90°03'00" | 20.00' | 31.44' | 28.30' | N44°33'06"E |
| C32 | 7°54'58" | 499.99' | 69.08' | 69.02' | N86°27'55"W |
| C33 | 4°50'34" | 526.99' | 44.54' | 44.53' | S88°00'07"E |
| C34 | 3°04'23" | 526.99' | 28.27' | 28.26' | S84°02'38"E |
| C35 | 7°54'58" | 472.99' | 65.35' | 65.30' | S86°27'55"E |
| C36 | 7°54'58" | 500.03' | 69.08' | 69.03' | N86°27'55"W |
| C37 | 2°26'07" | 473.03' | 20.11' | 20.10' | S83°43'30"E |
| C38 | 5°28'50" | 473.03' | 45.25' | 45.23' | S87°40'59"E |
| C39 | 7°54'58" | 527.03' | 72.81' | 72.76' | S86°27'55"E |



| LINE | BEARING | DISTANCE |
|------|-------------|----------|
| L1 | N89°34'36"E | 7.47' |
| L2 | S00°28'24"E | 17.07' |
| L3 | N89°34'36"E | 7.47' |
| L4 | N89°34'36"E | 11.96' |
| L5 | S00°28'24"E | 2.96' |
| L6 | S10°21'23"W | 1.17' |
| L7 | S10°21'23"W | 9.73' |
| L8 | S00°28'24"E | 17.33' |
| L9 | S10°21'23"W | 10.90' |
| L10 | S00°28'24"E | 3.04' |
| L11 | S10°21'23"W | 10.90' |
| L12 | N00°28'24"W | 2.73' |
| L13 | N89°34'36"E | 11.28' |

- LEGEND
- PROPERTY LINE
 - SECTION LINE
 - CENTERLINE
 - RIGHT OF WAY
 - EASEMENT
 - EXISTING LOT LINE
 - FENCE LINE
 - P.U.E. PUBLIC UTILITY EASEMENT
 - () INDICATES RECORD DIMENSION
 - MONUMENT FOUND AS DESCRIBED
 - ⊗ FOUND 5/8" IRON REBAR WITH CAP PER PLAT BK. 17 PG. 40
 - SET 5/8" X 30" IRON REBAR WITH YELLOW PLASTIC CAP STAMPED "WA19619 OR02820"
 - ◆ SET 5/8" X 30" IRON REBAR WITH 1 1/2" ALUMINUM CAP STAMPED "OR02820"

I CERTIFY THAT THIS IS A
TRUE AND EXACT COPY OF
THE ORIGINAL PLAT

GREG E. FLOWERS PLS

REGISTERED
PROFESSIONAL
LAND SURVEYOR

DRAFT

OREGON
JUNE 30, 1997
GREG E. FLOWERS
02820LS

RENEWS 12/31/2023



**PBS Engineering and
Environmental Inc.**
5 N Colville St, Ste 200
Walla Walla, WA 99362
509.956.3026
pbsusa.com

| | | | |
|-------------------------------|-------------------|----------------------------|--|
| CLIENT: LEGACY HOME GROUP LLC | | PROJECT NO.: 67573-002 | |
| SURVEYOR: GREG E. FLOWERS | | DATE: 6/3/2022 | |
| CALC BY: MFW | DRAWN BY: AA | SCALE: 1" = 60' | |
| SECTION: 2 | TOWNSHIP: 4 NORTH | RANGE: 28 EAST | |
| CITY: HERMISTON | COUNTY: UMATILLA | SHEET <u>2</u> OF <u>3</u> | |

NARRATIVE

REFERENCE DOCUMENTS

SURVEYOR'S CERTIFICATE

HAVING AN AREA OF 436,179 SQUARE FEET, 10.01 ACRES, MORE OR LESS.

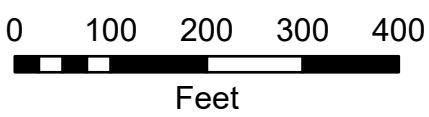
VICINITY MAP - NOT TO SCALE

HERMISTON IRRIGATION DISTRICT

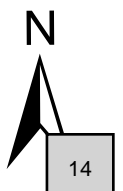
GREG E. FLOWERS PLS

RENEWS 12/31/2023

13



- Legend
- Property Line
 - City Limits
 - Urban Growth Boundary
 - Phases 5 and 6





HERMISTON POLICE DEPARTMENT

Section 5, Item D.



330 S. First Street
Hermiston, Oregon 97838
www.hermiston.or.us/police/home

Sine Metu Sine Gratia

Phone: 541-567-5519
Fax: 541-567-8469
Email: records@hermiston.or.us

Without Fear Without Favor

TO: City Manager Byron Smith
FROM: Chief Jason Edmiston 
DATE: September 12th, 2022
SUBJECT: Liquor License Application – Ye Olde Pizza Shoppe

After review of the liquor license application for “Greater Privilege” for Ye Olde Pizza Shoppe located at 845 N 1st Street, Hermiston, I find nothing of substance after performing a criminal history check consistent with established parameters utilized by the city, to deny the application submitted by Megan and Jasper Sabatino.

It is my recommendation this information/request be presented to the City Council.

CC: City Recorder Lilly Alarcon-Strong

LIQUOR LICENSE APPLICATION

Section 5, Item D.

Page 1 of 3

Check the appropriate license request option:

☐ [New Outlet](#) | ☐ [Change of Ownership](#) | ☒ [Greater Privilege](#) | ☐ [Lesser Privilege](#)

Select the license type you are applying for.

More information about all license types is available [online](#).

Full On-Premises

- ☒ Commercial
- ☐ Caterer
- ☐ Public Passenger Carrier
- ☐ Other Public Location
- ☐ For Profit Private Club
- ☐ Nonprofit Private Club

Winery

- ☐ Primary location
- Additional locations: ☐ 2nd ☐ 3rd ☐ 4th ☐ 5th

Brewery

- ☐ Primary location
- Additional locations: ☐ 2nd ☐ 3rd

Brewery-Public House

- ☐ Primary location
- Additional locations: ☐ 2nd ☐ 3rd

Grower Sales Privilege

- ☐ Primary location
- Additional locations: ☐ 2nd ☐ 3rd

Distillery

- ☐ Primary location
- Additional tasting locations: ☐ 2nd ☐ 3rd ☐ 4th ☐ 5th ☐ 6th

☐ Limited On-Premises

☐ Off Premises

☐ Warehouse

☐ Wholesale Malt Beverage and Wine

INTERNAL USE ONLY

Application received:

05/27/22

Minimum documents acquired:

05/27/22

LOCAL GOVERNING BODY USE ONLY

City/County name:

City of Hermiston

Date application received:

Optional: Date Stamp

09.12.2022

☐ Recommend this license be granted

☐ Recommend this license be denied

Printed Name

Date

Return this form to:

Investigator name:

Eric.Hildebrand

Email:

Eric.Hildebrand@oregon.gov

LIQUOR LICENSE APPLICATION

Section 5, Item D.

Page 2 of 3

APPLICANT INFORMATION

Identify the applicants applying for the license. This is the entity (example: corporation or LLC) or individual(s) applying for the license. Please add an additional page if more space is needed.

Name of entity or individual applicant #1:

Ye Olde Pizza Shoppe, LLC

Name of entity or individual applicant #2:

Name of entity or individual applicant #3:

Name of entity or individual applicant #4:

BUSINESS INFORMATION

Trade Name of the Business (name customers will see):

Ye Olde Pizza Shoppe

Business phone number:

5415679066

Business email:

yeoldepizzashoppe21@gmail.com

Premises street address (The physical location of the business and where the liquor license will be posted):

845 north 1st street

City:

hermiston

Zip Code:

oregon

County:

umatilla

Business mailing address (where we will send any items by mail as described in [OAR 845-004-0065\[11\]](#)):

P.O Box 23

City:

Stanfield

State:

Oregon

Zip Code:

97875

Does the business address currently have an OLCC liquor license? ☒ Yes ☐ No

Does the business address currently have an OLCC marijuana license? ☐ Yes ☒ No

APPLICATION CONTACT INFORMATION

Contact Name:

Megan Sabatino

Phone number:

5415719627

Email:

yeoldepizzashoppe21@gmail.com

Mailing address:

P.O Box 23

City:

Stanfield

Zip Code:

Oregon

County:

Umatilla

Please note: liquor license applications are public records.



OREGON LIQUOR CONTROL COMMISSION BUSINESS INFORMATION

Section 5, Item D.

Please Print or Type

Applicant Name: Megan Sabatino Phone: 5415719627

Trade Name (dba): Ye Olde Pizza Shoppe

Business Location Address: 845 North 1st Street

City: Hermiston ZIP Code: 97838

DAYS AND HOURS OF OPERATION

Business Hours:

| | | | |
|-----------|----------------|----|-----------------|
| Sunday | <u>12:00pm</u> | to | <u>9PM</u> |
| Monday | <u>11am</u> | to | <u>9PM</u> |
| Tuesday | <u>11am</u> | to | <u>05/27/22</u> |
| Wednesday | <u>11am</u> | to | <u>9PM</u> |
| Thursday | <u>11am</u> | to | <u>9PM</u> |
| Friday | <u>11am</u> | to | <u>10pm</u> |
| Saturday | <u>11am</u> | to | <u>10pm</u> |

Outdoor Area Hours:

| | | | |
|-----------|-------------|----|-------------|
| Sunday | <u>12pm</u> | to | <u>9pm</u> |
| Monday | <u>5pm</u> | to | <u>9pm</u> |
| Tuesday | <u>5pm</u> | to | <u>9pm</u> |
| Wednesday | <u>5pm</u> | to | <u>9pm</u> |
| Thursday | <u>5pm</u> | to | <u>9pm</u> |
| Friday | <u>5pm</u> | to | <u>10pm</u> |
| Saturday | <u>11am</u> | to | <u>10pm</u> |

The outdoor area is used for:

☐ Food service Hours: 11am to 10pm
☐ Alcohol service Hours: 11am to 10pm
☐ Enclosed, how Porch Railing

The exterior area is adequately viewed and/or supervised by Service Permittees.

____ (Investigator's Initials)

Seasonal Variations: ☒ Yes ☐ No If yes, explain: Shorter hours in the winter

ENTERTAINMENT

Check all that apply:

- ☒ Live Music *No live* ☐ Karaoke
☐ Recorded Music ☐ Coin-operated Games
☐ DJ Music ☐ Video Lottery Machines
☐ Dancing ☐ Social Gaming
☐ Nude Entertainers ☐ Pool Tables
☒ Other: juke box

DAYS & HOURS OF LIVE OR DJ MUSIC

| | | | |
|-----------|-------|----|-------|
| Sunday | _____ | to | _____ |
| Monday | _____ | to | _____ |
| Tuesday | _____ | to | _____ |
| Wednesday | _____ | to | _____ |
| Thursday | _____ | to | _____ |
| Friday | _____ | to | _____ |
| Saturday | _____ | to | _____ |

SEATING COUNT

Restaurant: 84 Outdoor: 15
Lounge: _____ Other (explain): _____
Banquet: _____ Total Seating: 99

OLCC USE ONLY

Investigator Verified Seating: _____ (Y) _____ (N)
Investigator Initials: _____
Date: _____

I understand if my answers are not true and complete, the OLCC may deny my license application.

Applicant Signature: Megan Sabatino Date: 05/27/2022

1-800-452-OLCC (6522)
www.oregon.gov/olcc



CITY COUNCIL

Work Session Meeting Minutes September 12, 2022

Mayor Drotzmann called the work session meeting to order at 6:00pm. Present were Councilors Hardin, Spicerkuhn, Peterson, Duron, Myers, Barron, and Myers. Councilors Davis and Primmer were excused. Staff members in attendance were: City Manager Byron Smith, Assistant City Manager Mark Morgan, City Attorney Richard Tovey, Chief Edmiston, Finance Director Mark Krawczyk, Planning Director Clint Spencer, Hermiston Energy Services (HES) General Manager (GM) Nate Rivera, Parks and Recreation Director Brandon Artz, Court Administrator Jillian Viles, and City Recorder Lilly Alarcon-Strong. Youth Advisory Members were not in attendance.

Broadband Survey Results

HES GM Nate Rivera presented the Council (PowerPoint presentation attached) with information regarding the Broadband Survey Results, to include: broadband community goals, connectivity and speed, reliability, price points, demand, current providers, community demographics, broadband differences between households and businesses, and more.

After some discussion the Council instructed City staff to move forward with continued research and information to connect city facilities to broadband.

Adjournment

Mayor Drotzmann adjourned the work session meeting at 6:57pm and stated the Council will take a short break before starting the regular City Council meeting at 7:03pm.

Hermiston Broadband eCheckup Data Collection

Preliminary Findings

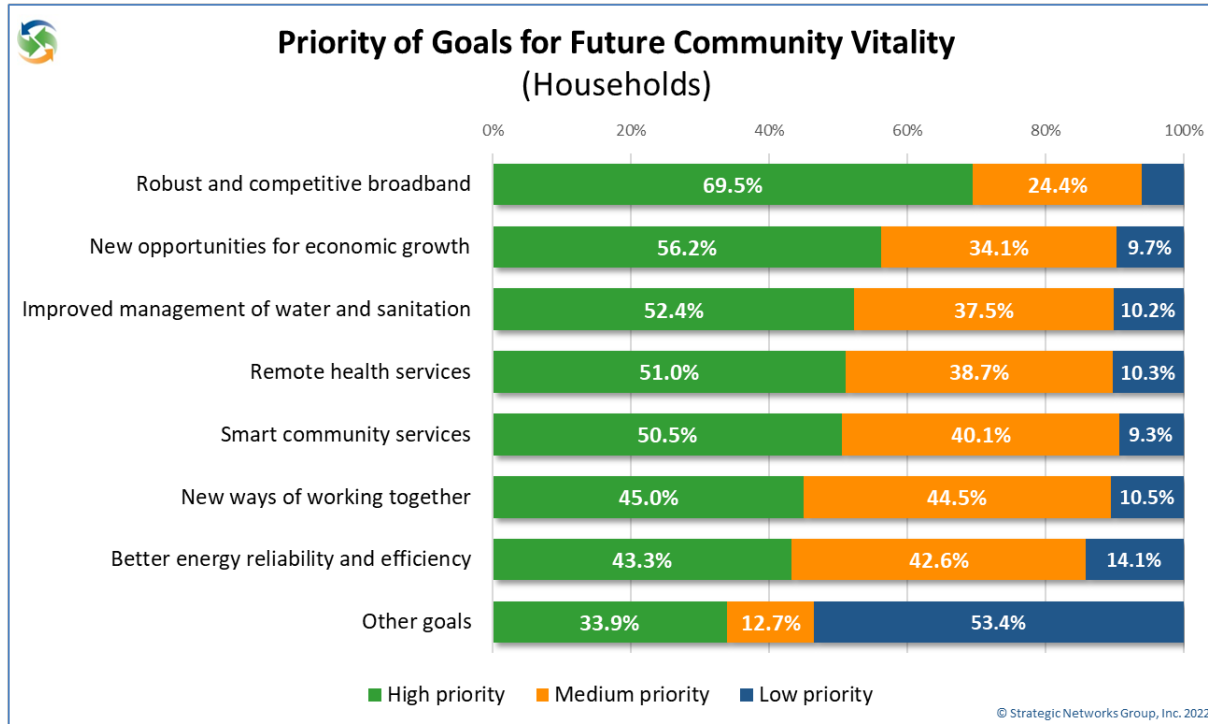
September 12, 2022

Introduction

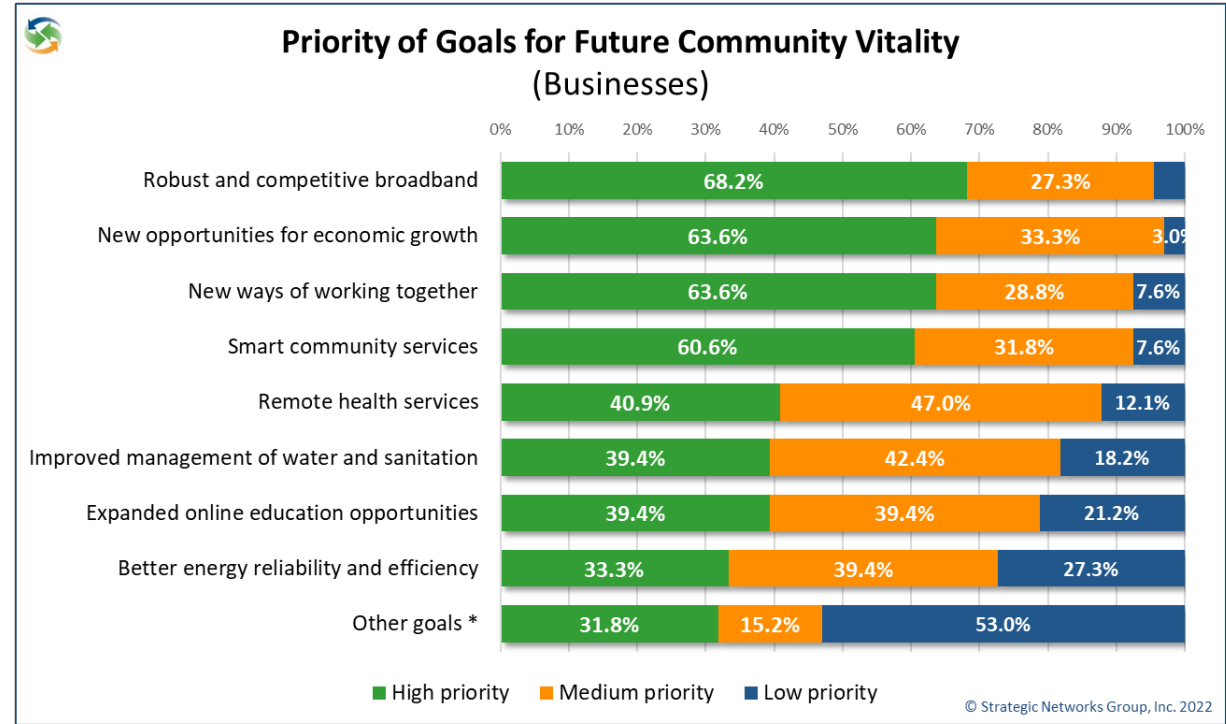
- Start with understanding digital goals, needs, and readiness (DNRA)
- Community planning to raise awareness and drive engagement
- Outreach to community through stakeholders and mailing lists
- Residents and businesses invited to take an online eCheckup that takes 20-30 minutes to complete
- Data is analyzed for user needs and service demand and benchmarked against SNG's Digital Economy Database
- Data collection ran from January 26th through May 15th, 2022
- Responses received from 827 households and 66 businesses

Community Goals

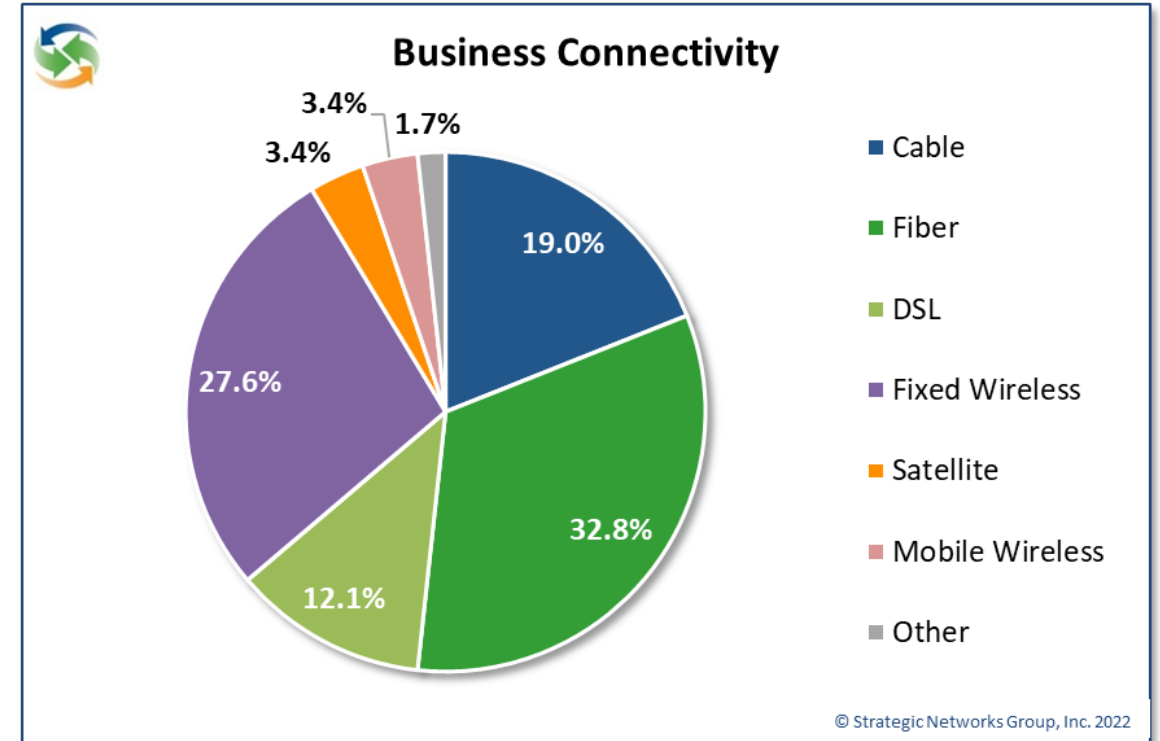
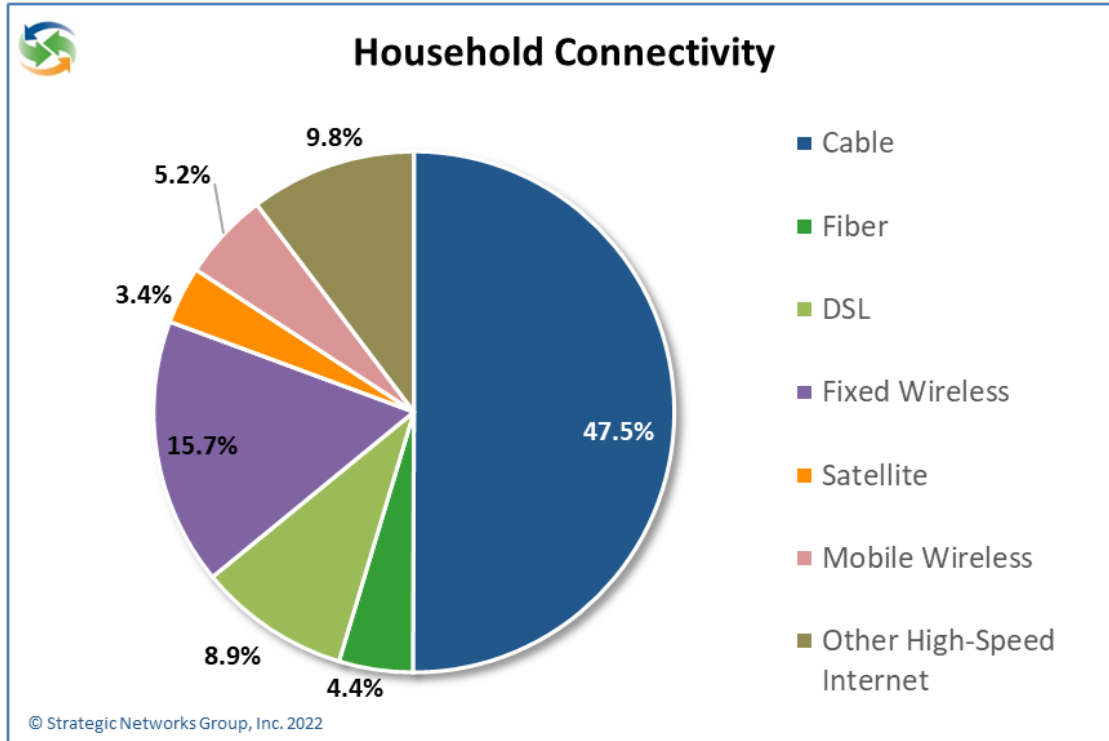
Households



Businesses



Robust, competitive broadband and new opportunities for economic growth are the top priorities for Hermiston residents and businesses



**The majority of Hermiston residents use Cable technology (47.5%)
Businesses primarily use Fiber for internet access (32.8%)**



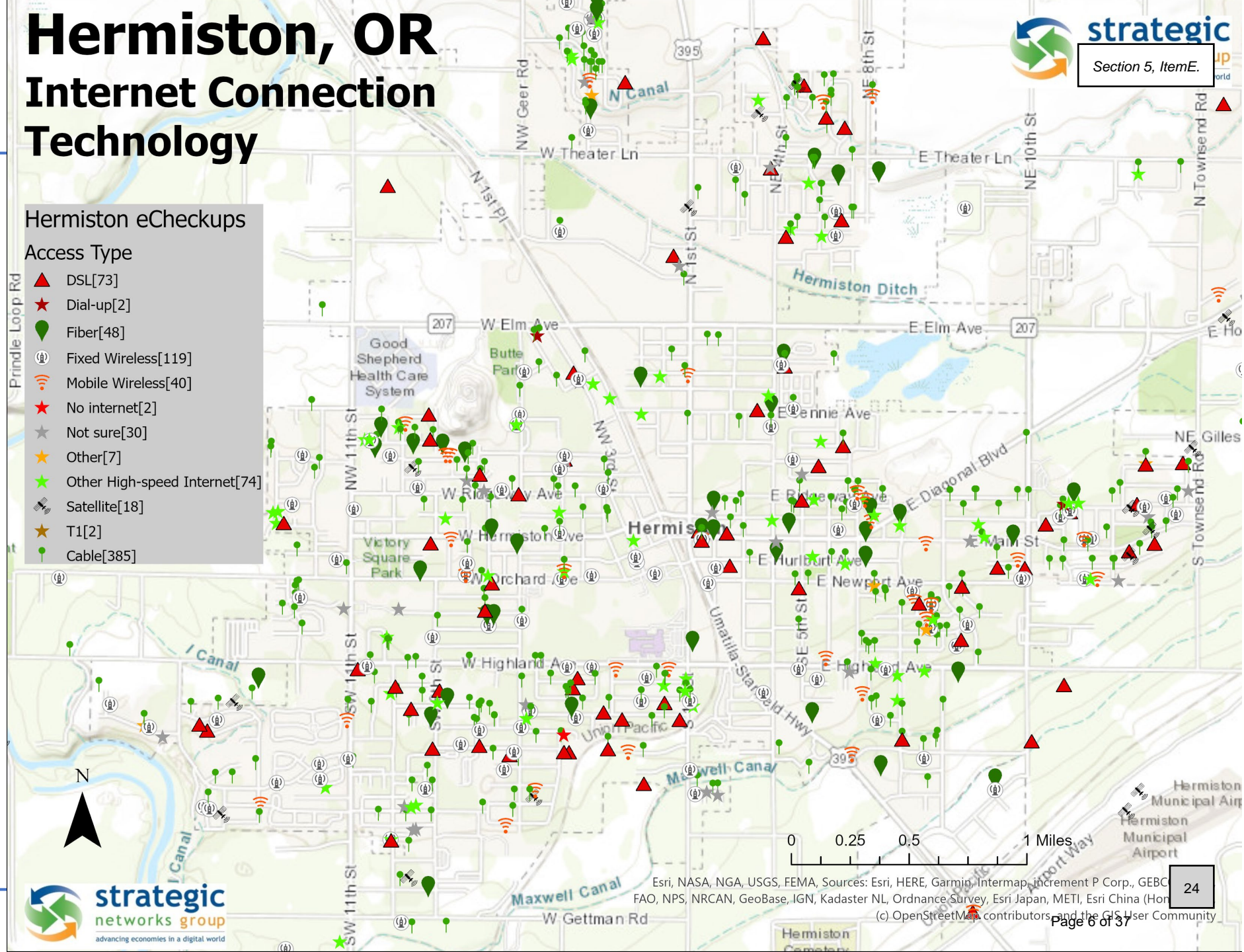
Where Life is Sweet

Hermiston, OR Internet Connection Technology

Hermiston eCheckups

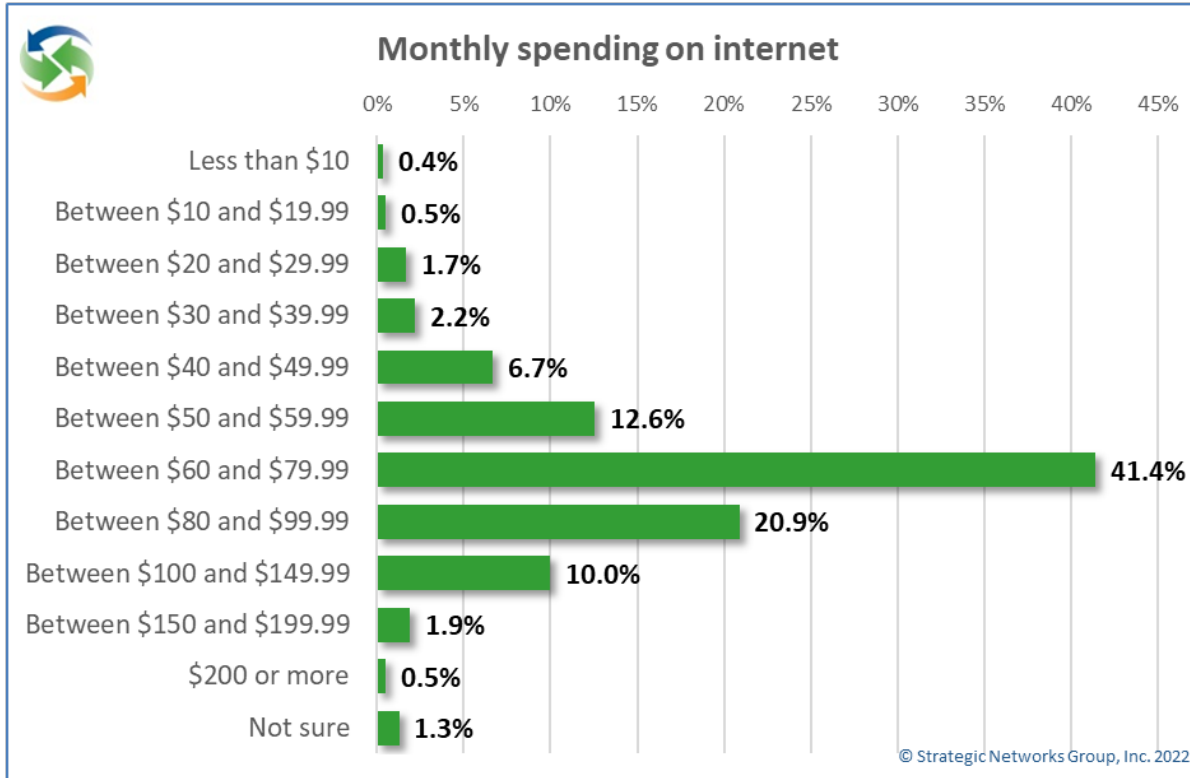
Access Type

- ▲ DSL[73]
- ★ Dial-up[2]
- Fiber[48]
- ④ Fixed Wireless[119]
- 📶 Mobile Wireless[40]
- ★ No internet[2]
- ★ Not sure[30]
- ★ Other[7]
- ★ Other High-speed Internet[74]
- 📶 Satellite[18]
- ★ T1[2]
- Cable[385]



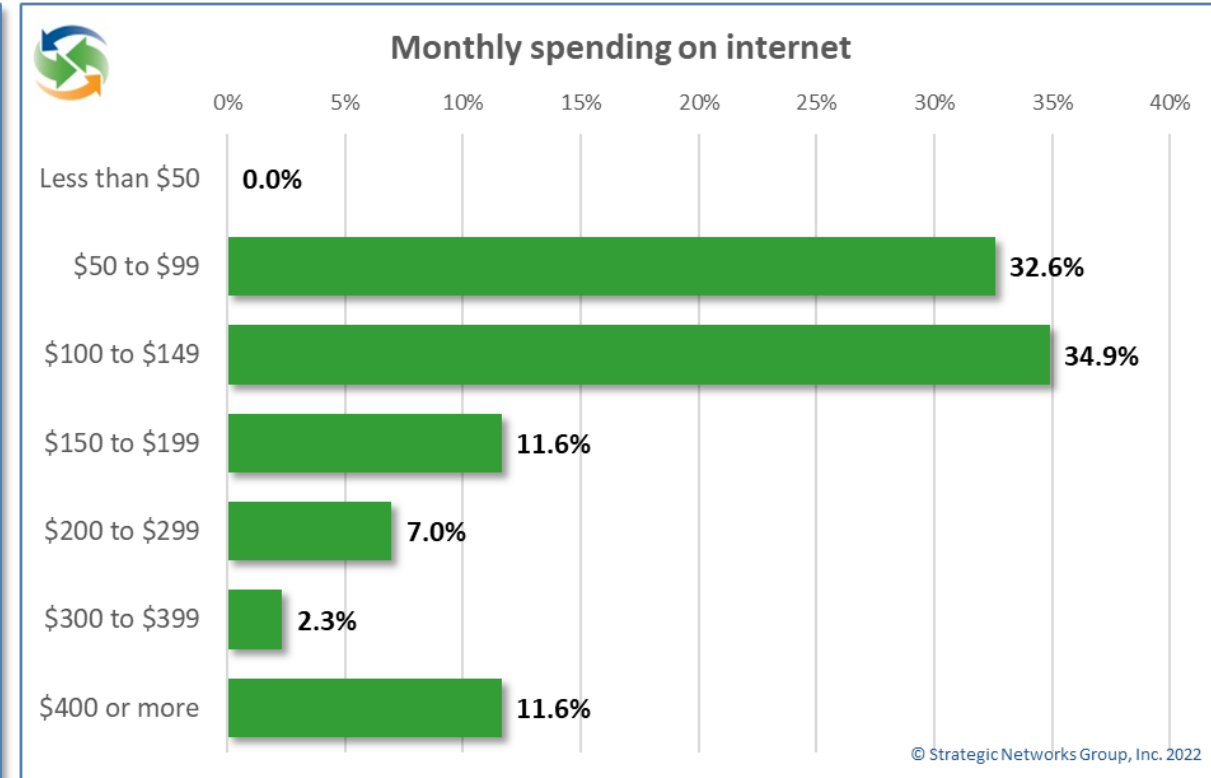
Monthly Spending on Internet

Households



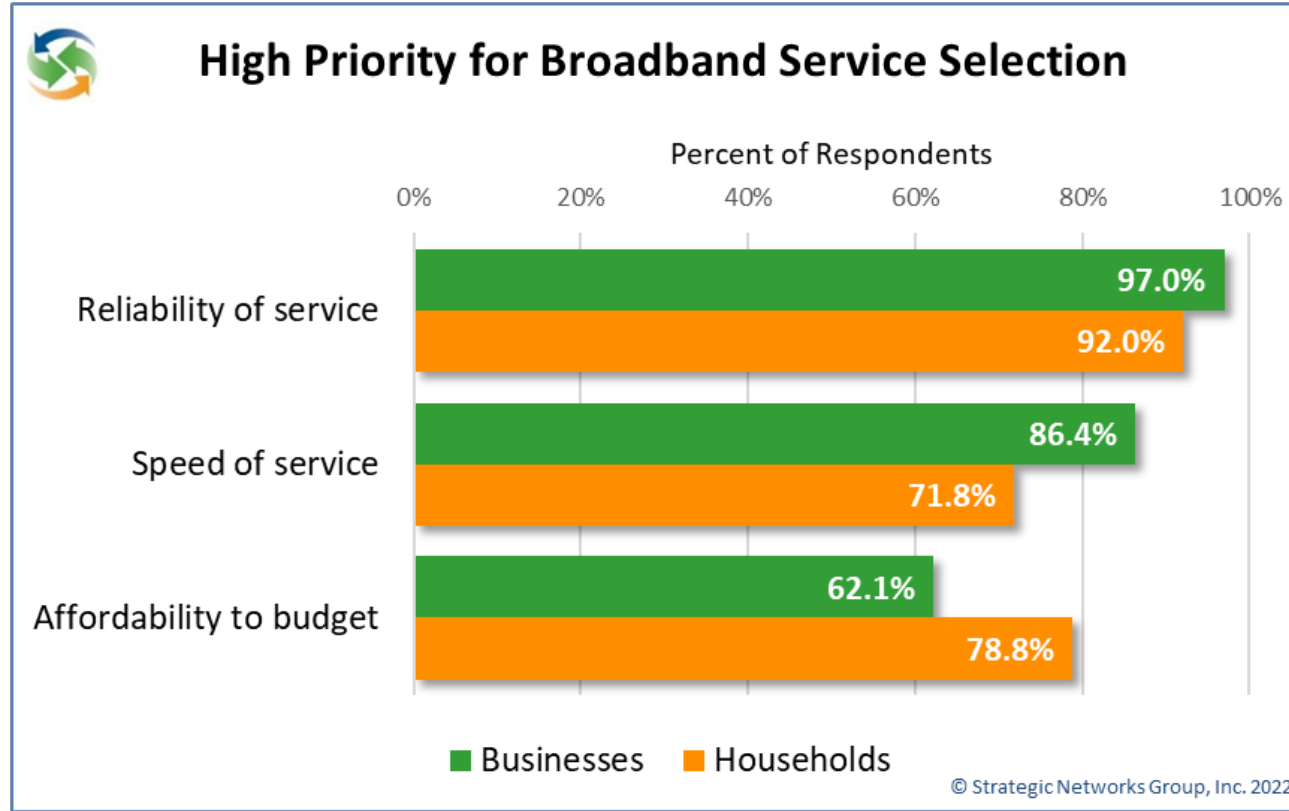
Most households spend between \$60 and \$79.99

Businesses



Over 20% of businesses spend more than \$200 per month

Priorities for Broadband Service



Reliability is the most important factor for broadband service selection for both households and businesses

Hermiston, OR

Satisfaction with Internet Reliability

Section 5, Item E.

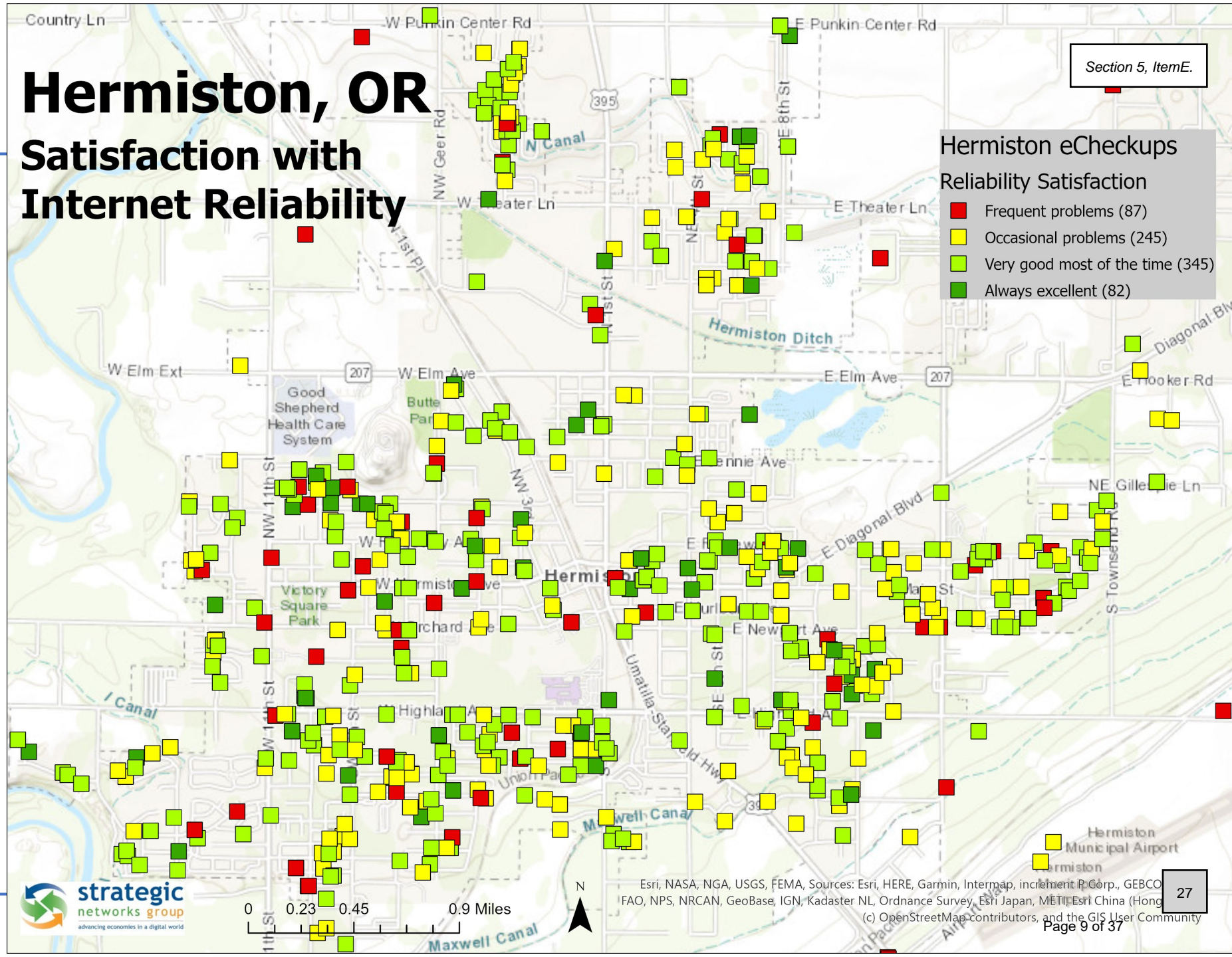
Hermiston eCheckups

Reliability Satisfaction

- Frequent problems (87)
- Occasional problems (245)
- Very good most of the time (345)
- Always excellent (82)

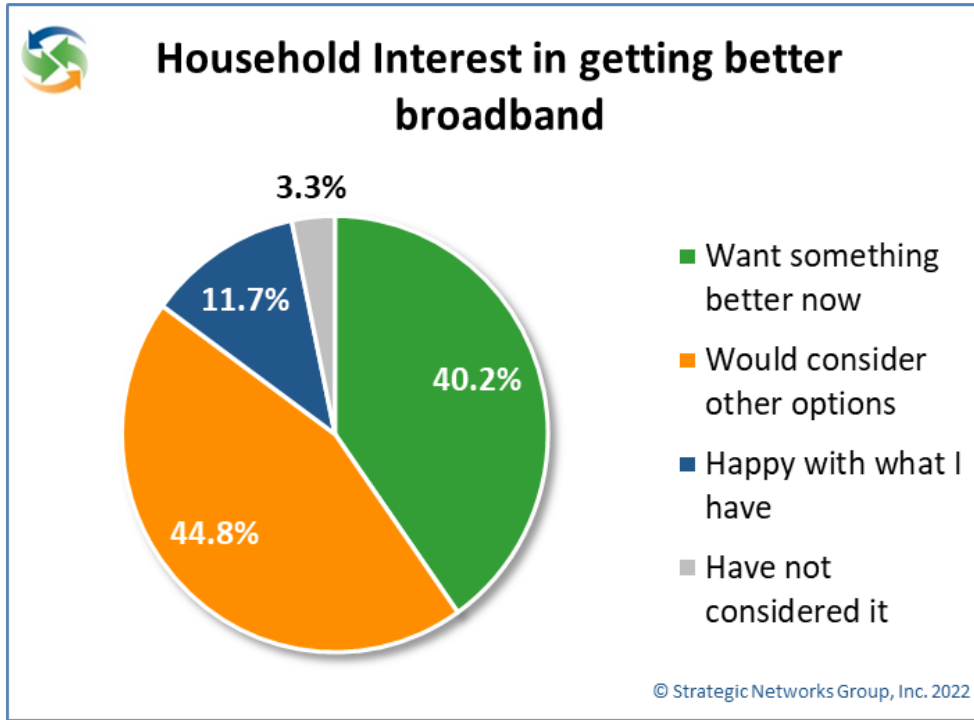


Where Life is Sweet

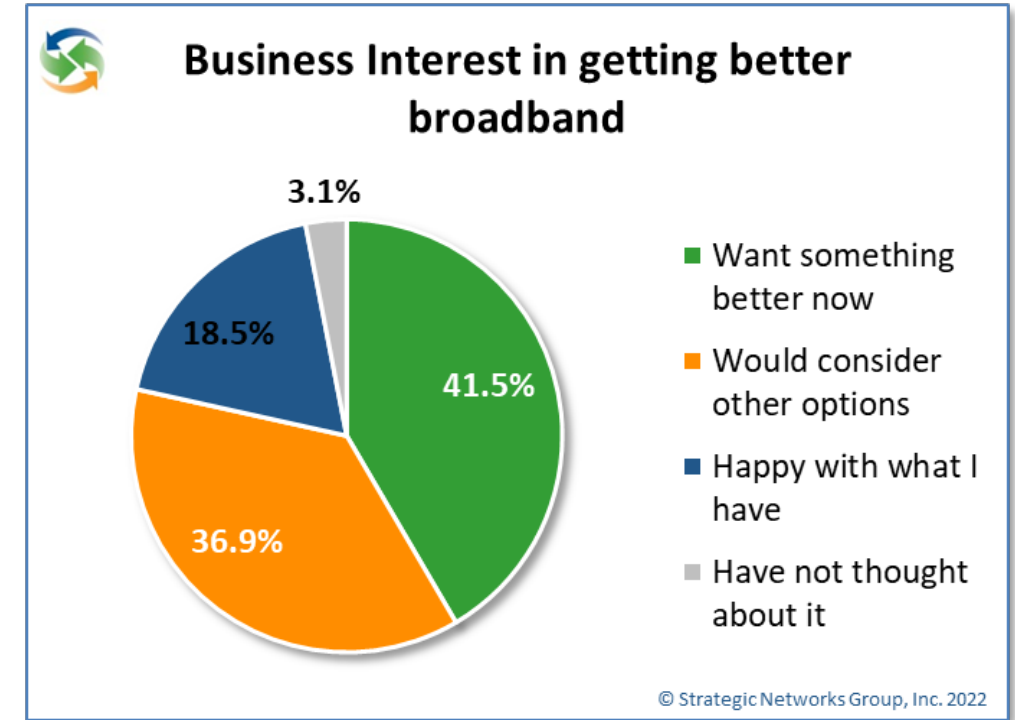


Demand for Better Broadband

Households



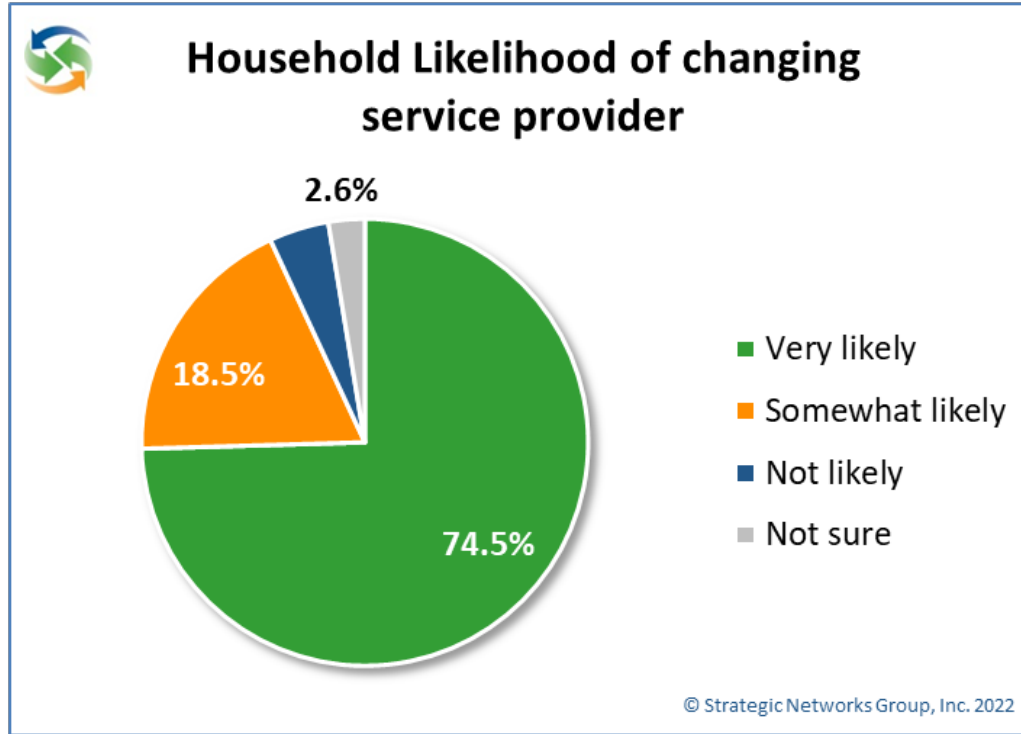
Businesses



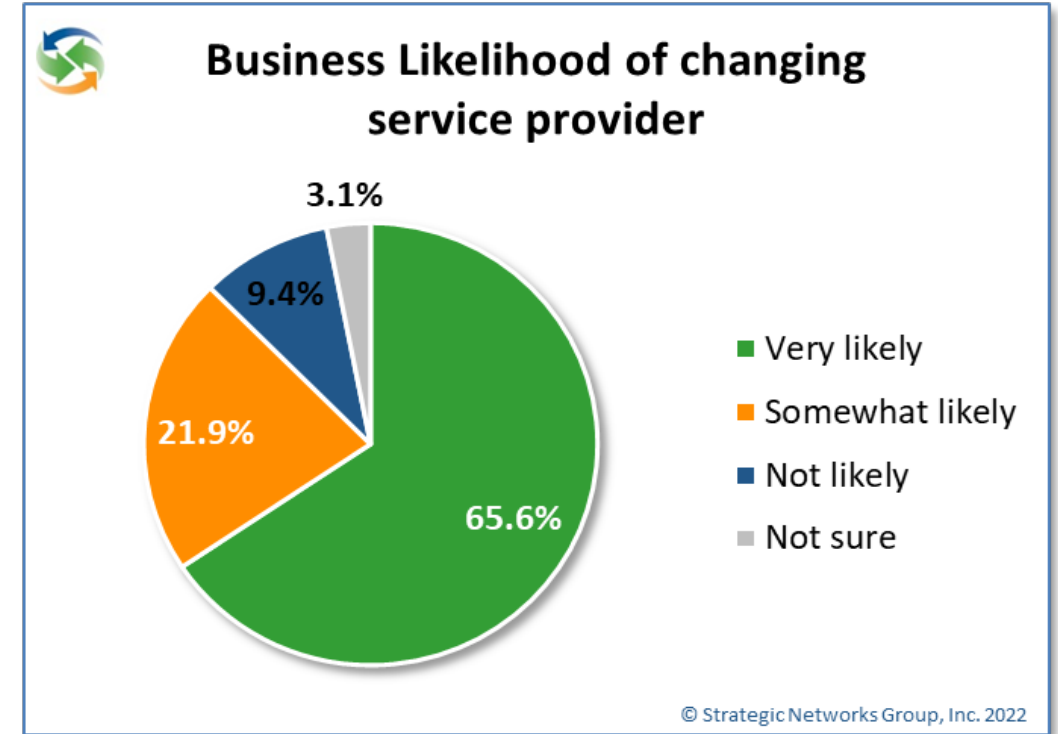
A majority of households would consider other options for broadband, while most businesses want better broadband *immediately*

Likelihood of Changing Service Provider

Households



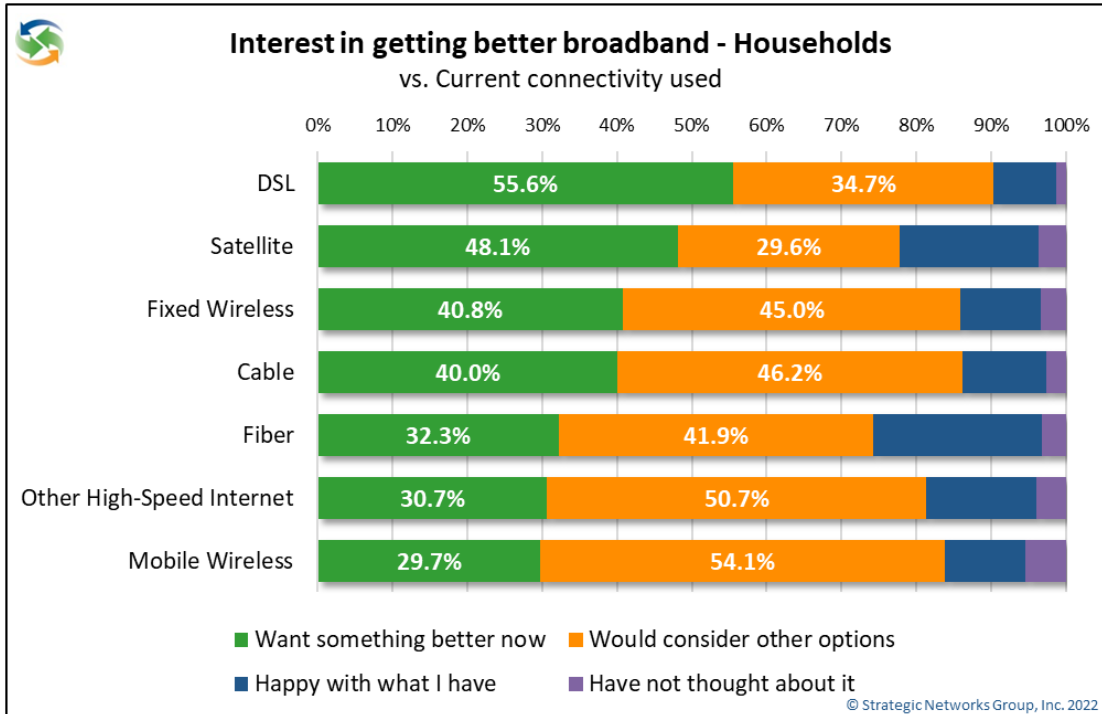
Businesses



Very high likelihood (willingness) for residents and businesses to change current service provider

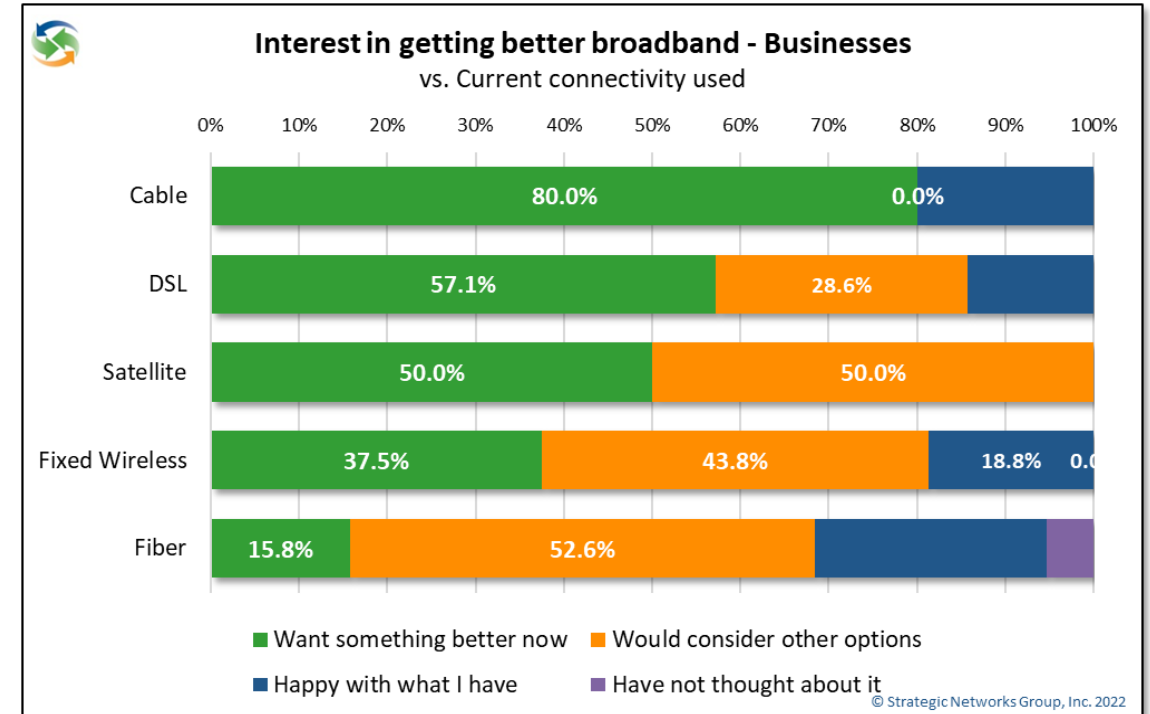
Interest in Better than Current Connectivity

Households



- Households with fiber broadband report the largest percentage of “Happy with what I have”
- DSL and Satellite households show most want something better now

Businesses

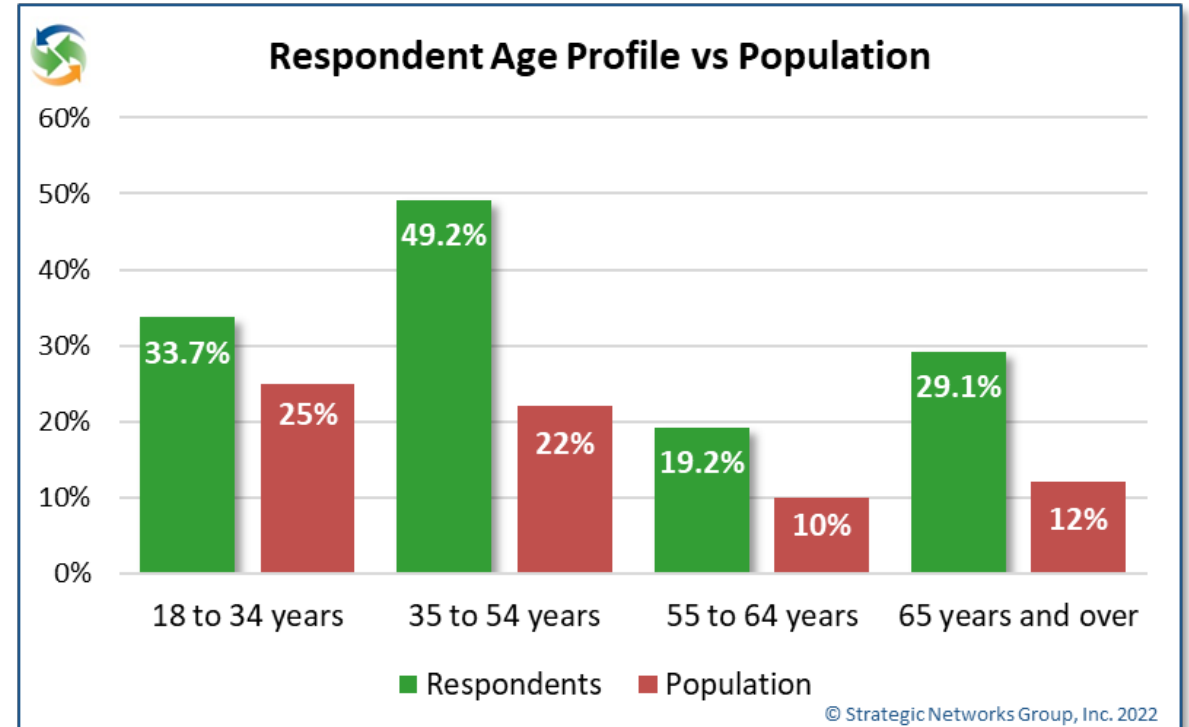
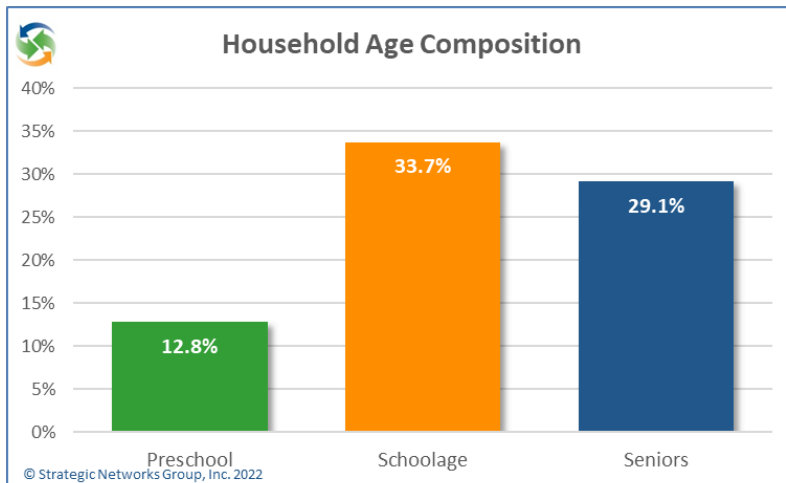
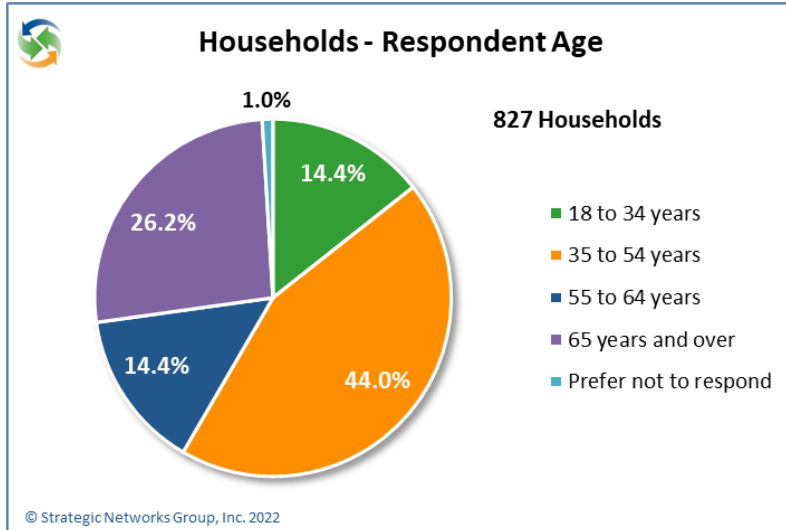


- 80% of businesses using cable technologies want something better now
- Many DSL users also want something better

Hermiston Broadband Data Collection Findings

Household Results

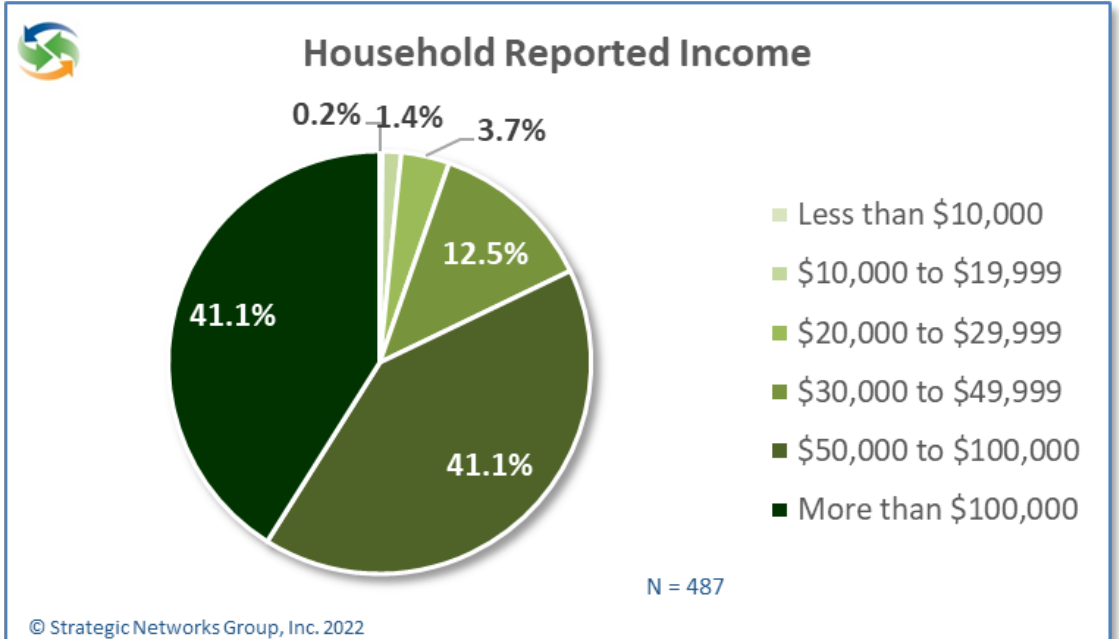
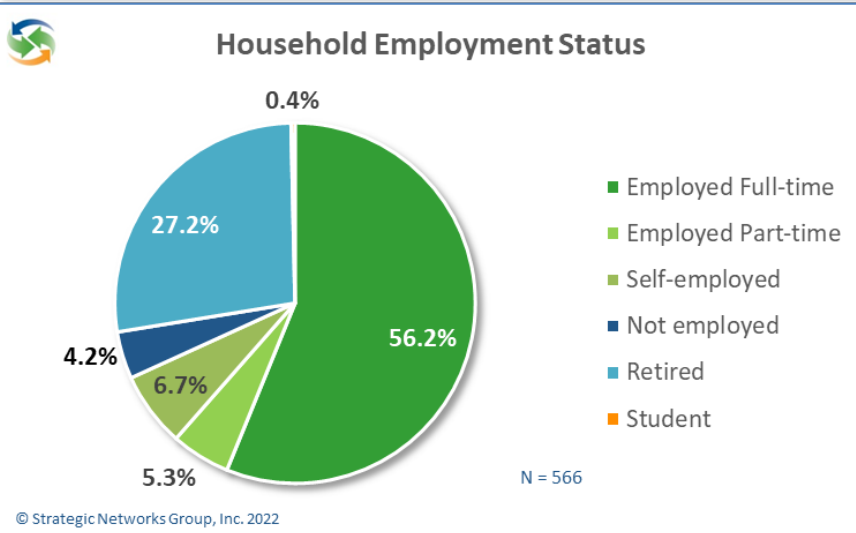
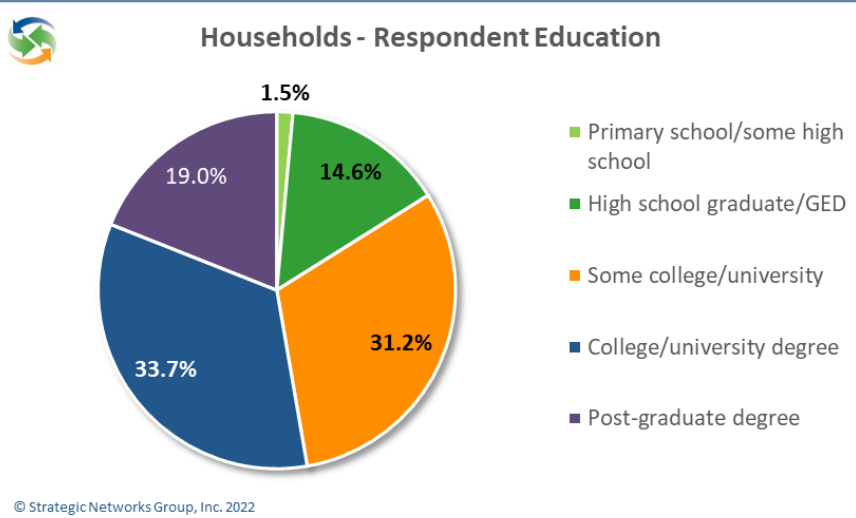
Household Respondent Age



- Respondent profile slightly older than overall population
- Almost one in four households have school age children

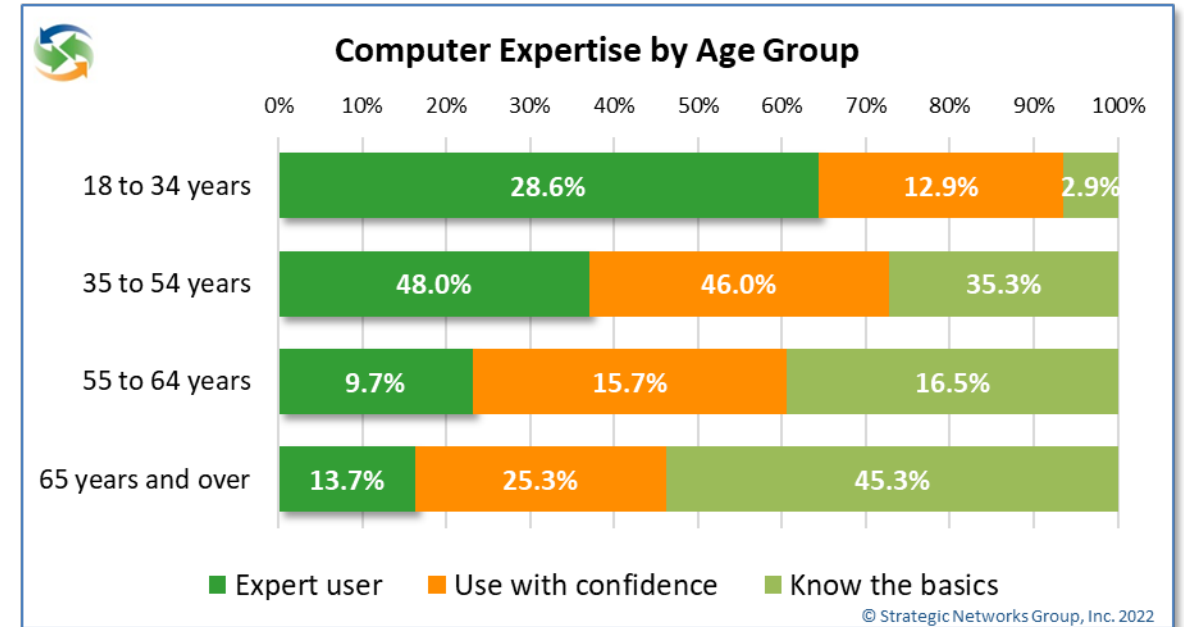
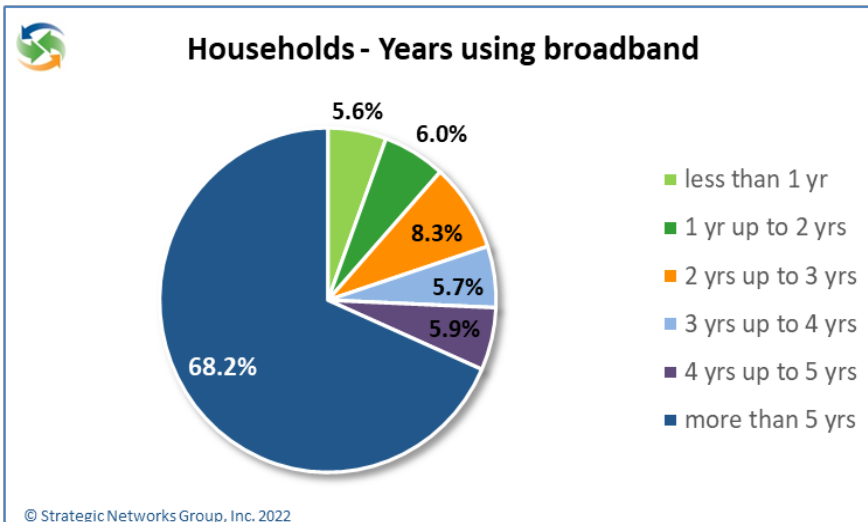
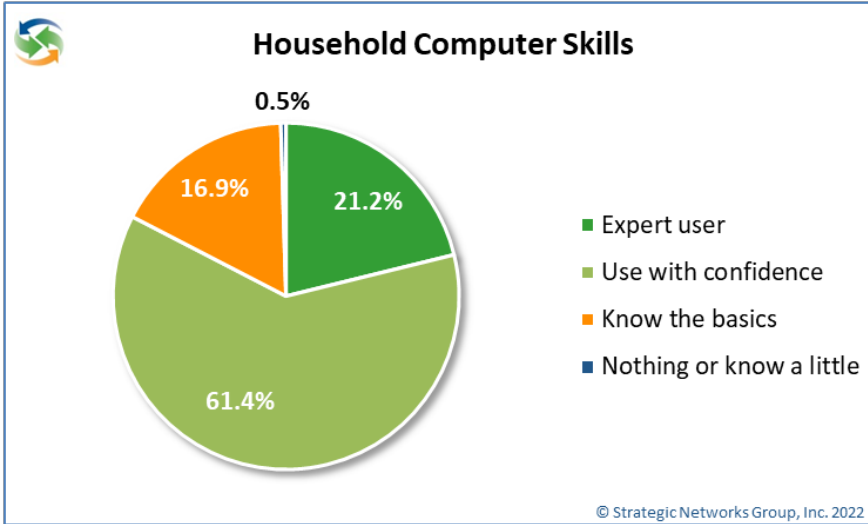
Population Data from Data USA, sourced from Census ACS 2020 – Age (<https://datausa.io/profile/geo/hermiston-or/>)

Household Income, Education, and Employment



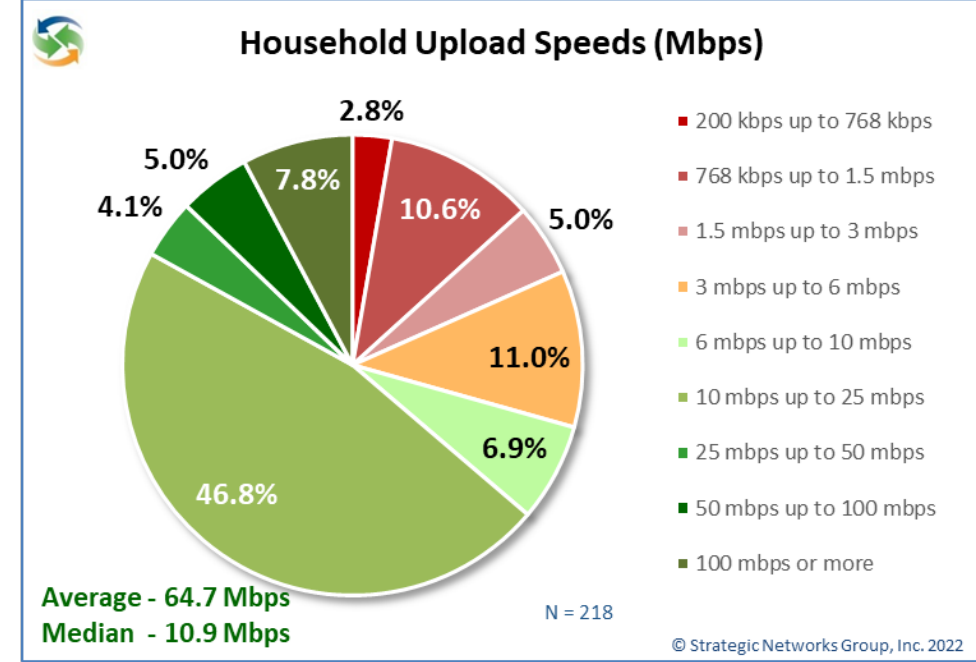
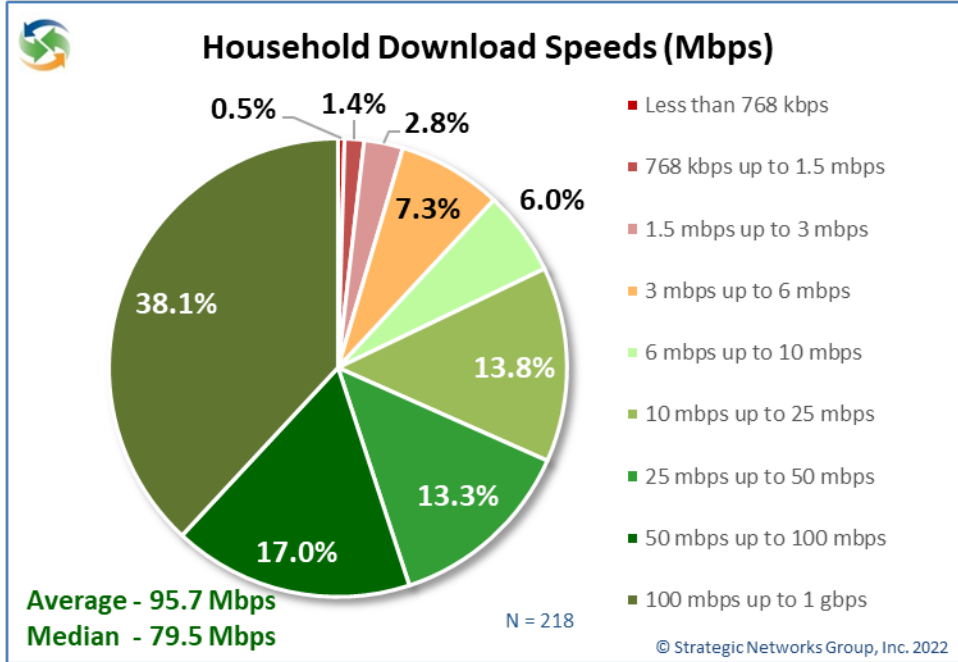
- **82% of households report earning more than \$50K annually**
- **52.7% have a college/university degree**
- **68% are employed**

Internet and Computer Experience



- Generally high level of computer skill across younger age groups
- More than 88% have been using broadband internet for at least 2 years

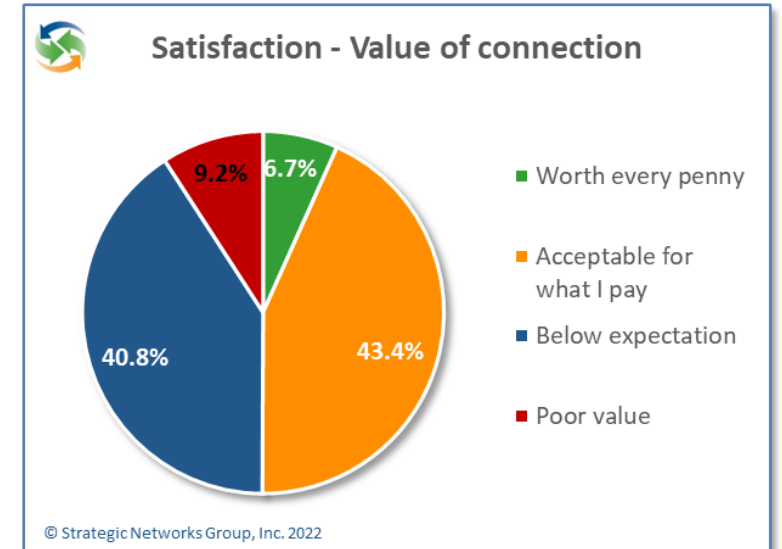
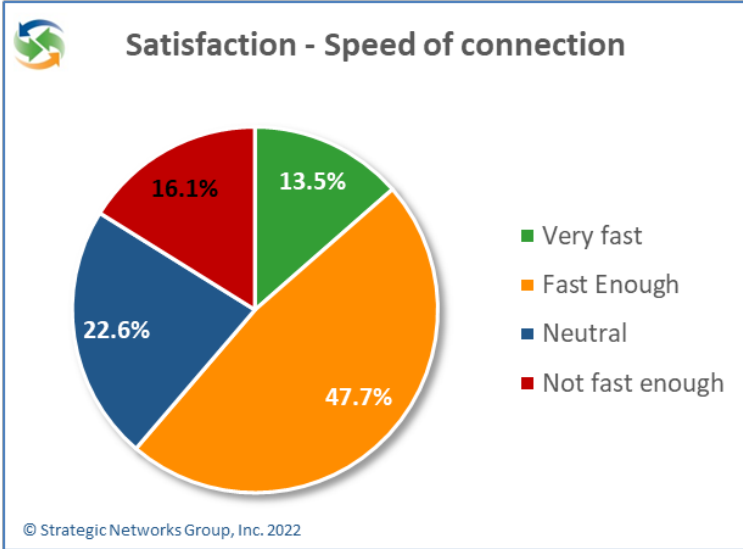
Measured Speeds – Households



- 55% of households have more than 50 Mbps download speed – this is good
- 31.8% have less than 25 Mbps download speed – not good
- Average speeds 95.7 Mbps down and 64.7 Mbps up – this is good overall, but there are a significant % of underserved

Almost one third of households do not meet FCC 25/3 Mbps standards

Satisfaction with Connectivity – Households



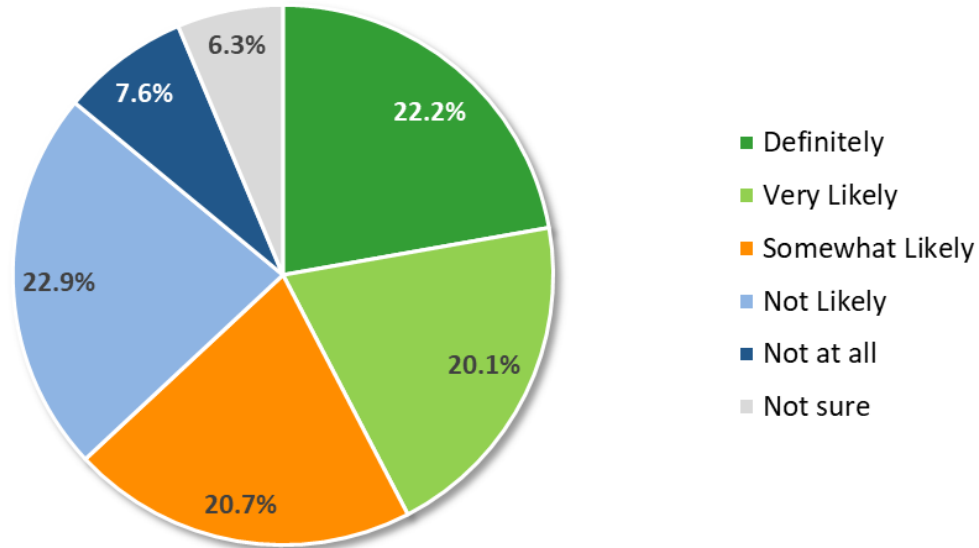
Households are least satisfied with the value of their internet connection.

Reliability is an issue as over 40% of residents report occasional or frequent problems with internet.

Broadband Impact on Living Location



Household Likelihood of Relocation for Broadband



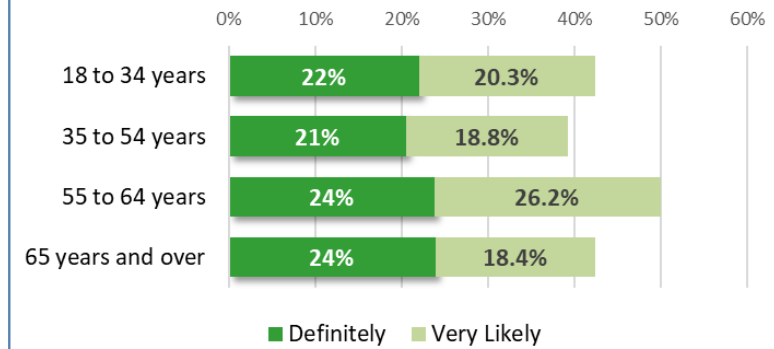
© Strategic Networks Group, Inc. 2022

Despite overall happiness in the community, over *one third* of residents would consider relocating for broadband

- High income earners are slightly more likely to relocate



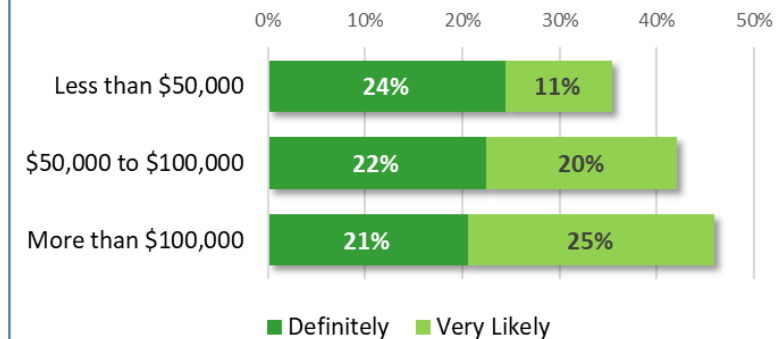
Likelihood of Relocation for Broadband by age group



© Strategic Networks Group, Inc. 2022



Likelihood of Relocation for Broadband by household income

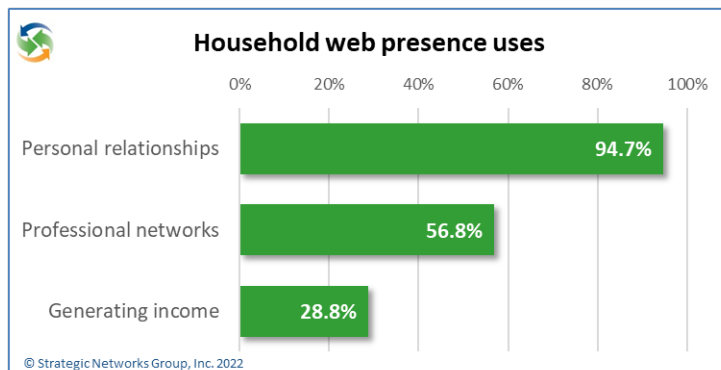
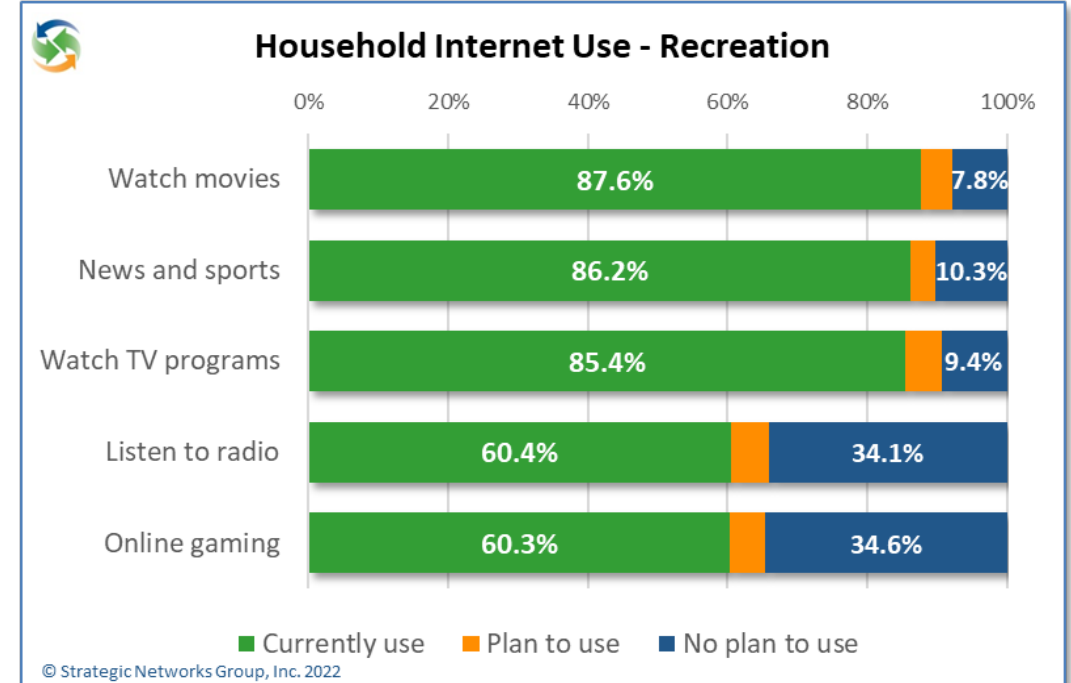
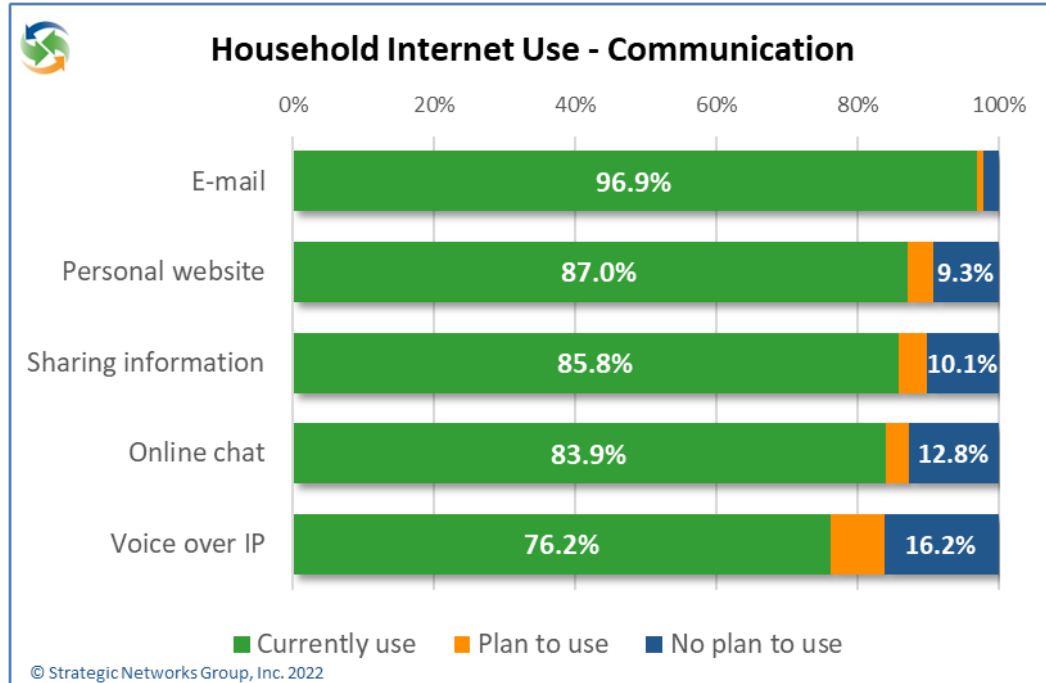


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Household Utilization – Communication

Section 5, Item E.

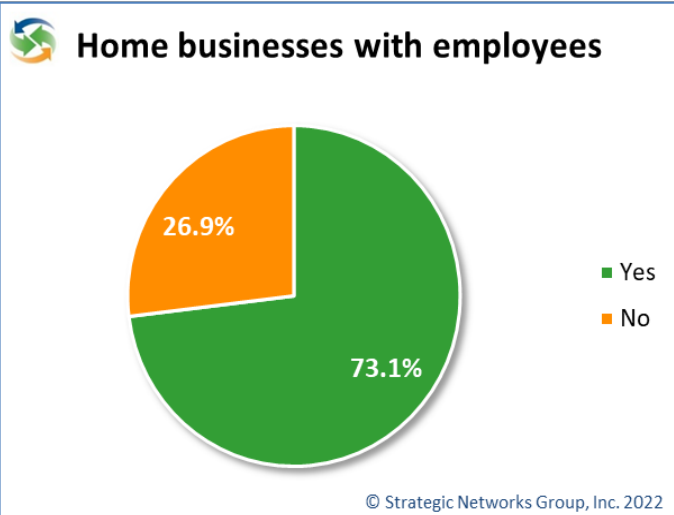
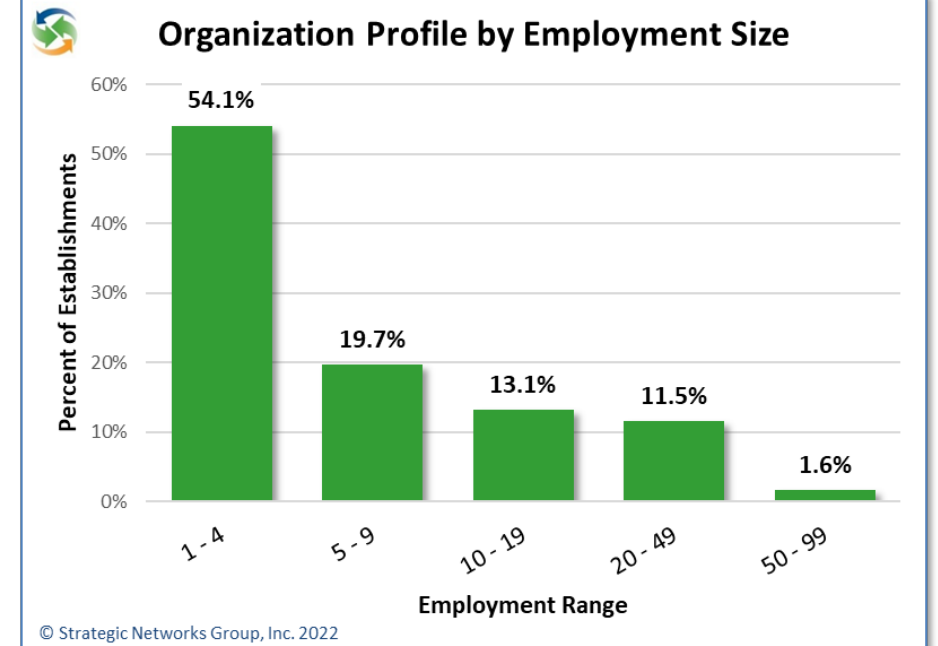
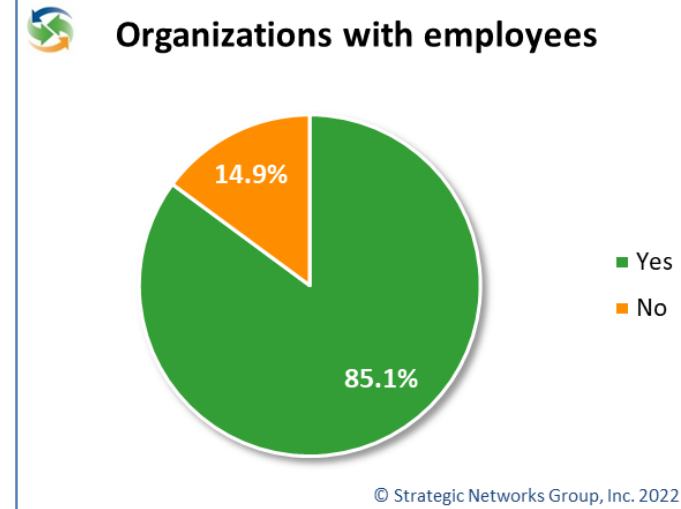
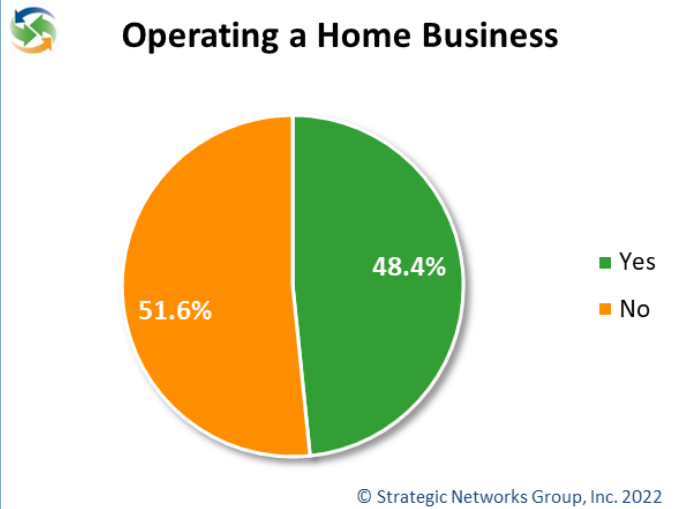
Recreation



Hermiston Broadband Data Collection Findings

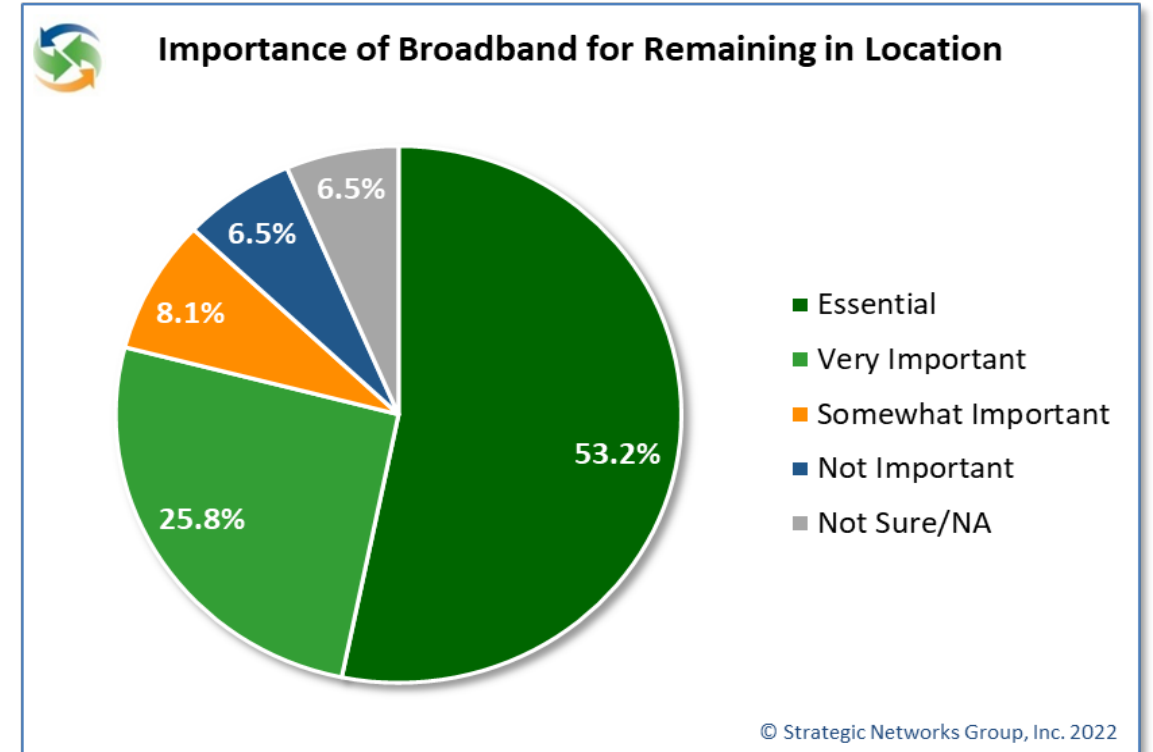
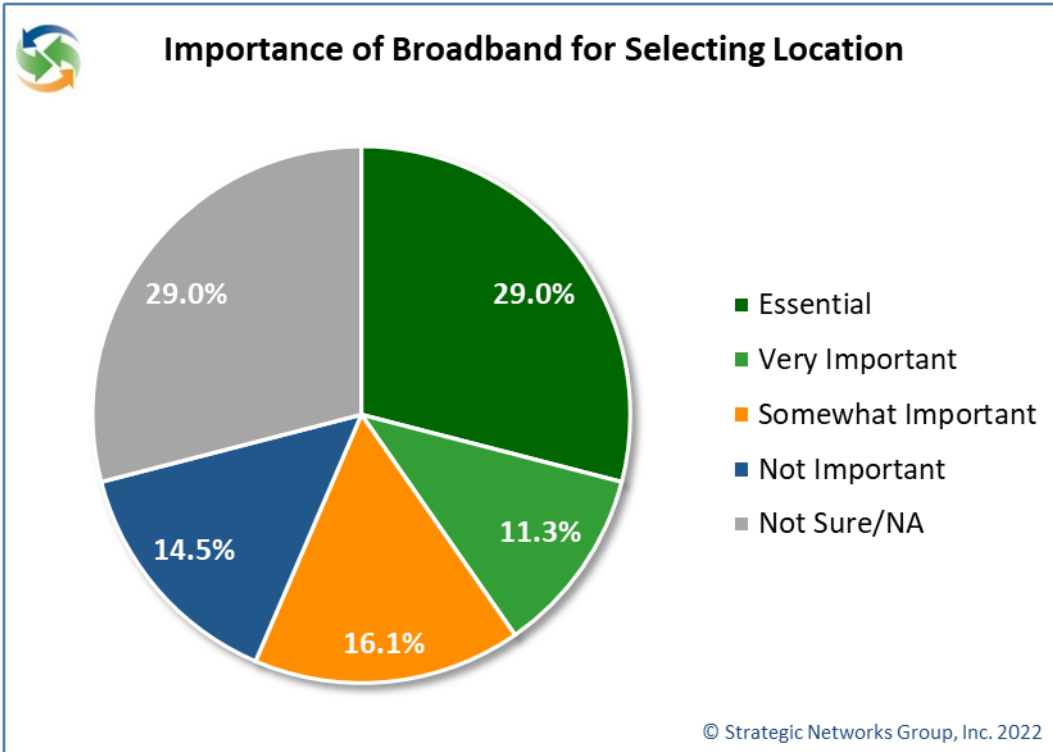
Business Results

Business Profile



- 54% of organizations have fewer than 5 employees
 - 14% of organizations do not have employees (sole-proprietorship)
- 48% of respondents operate a home business
 - 73% of those home businesses have employees

Importance of Broadband for Business Location

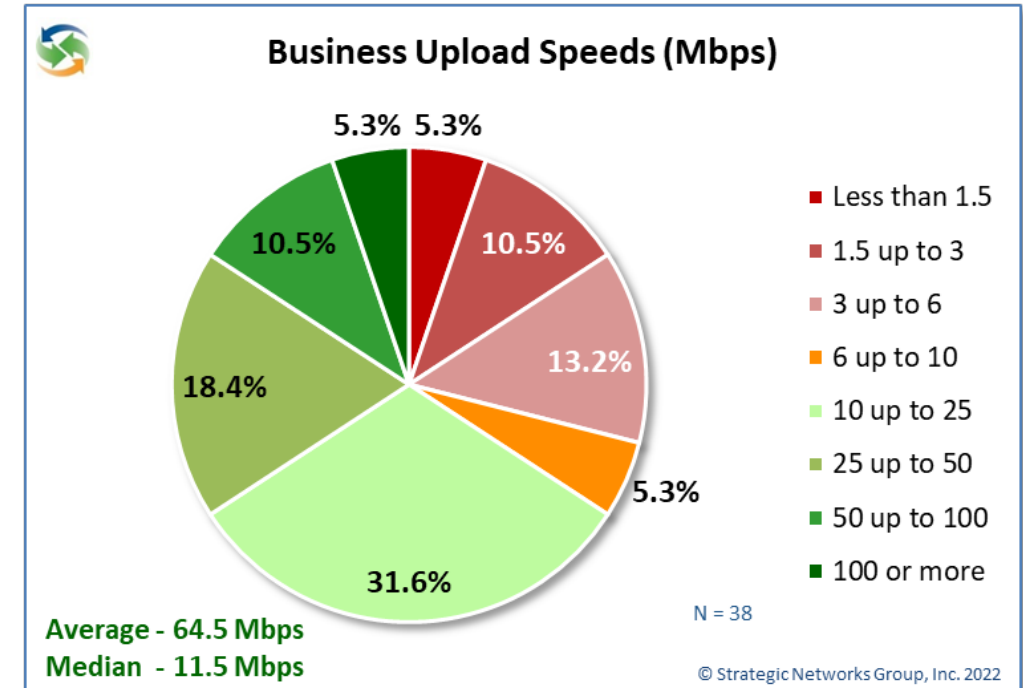
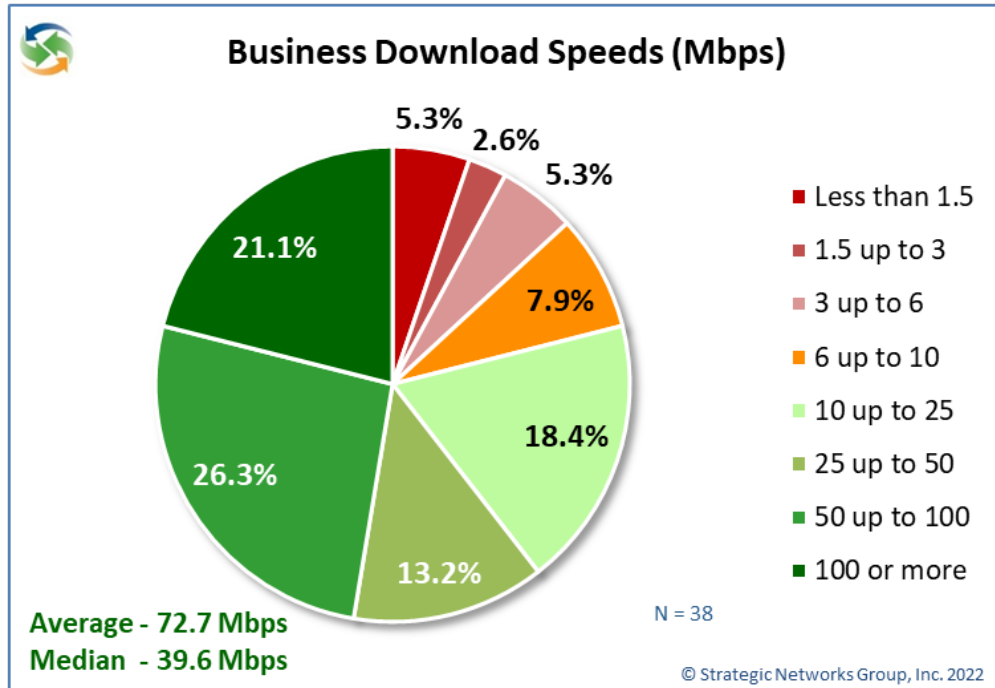


- Over one-third of businesses consider broadband a very important/essential factor in selecting business location

- More than *two-thirds* of businesses consider broadband very important/essential to remaining in their business location

Risk of business relocation out of Hermiston without better broadband

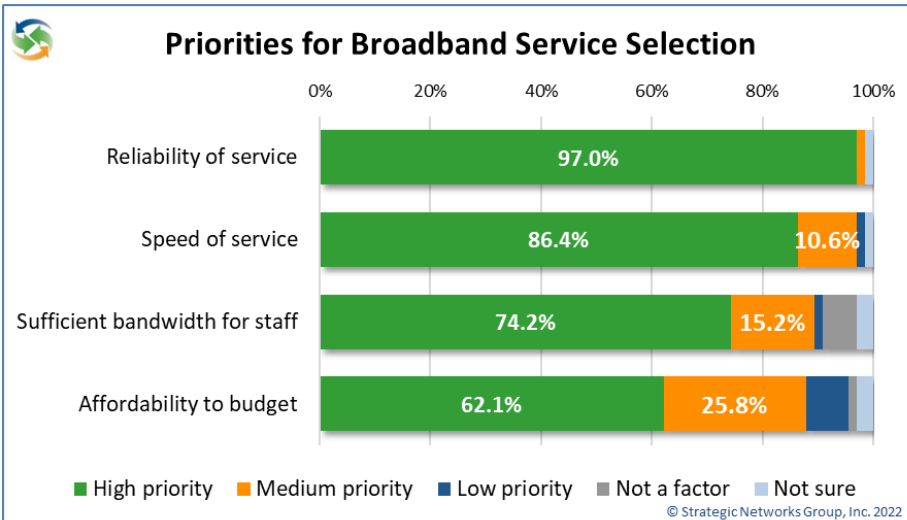
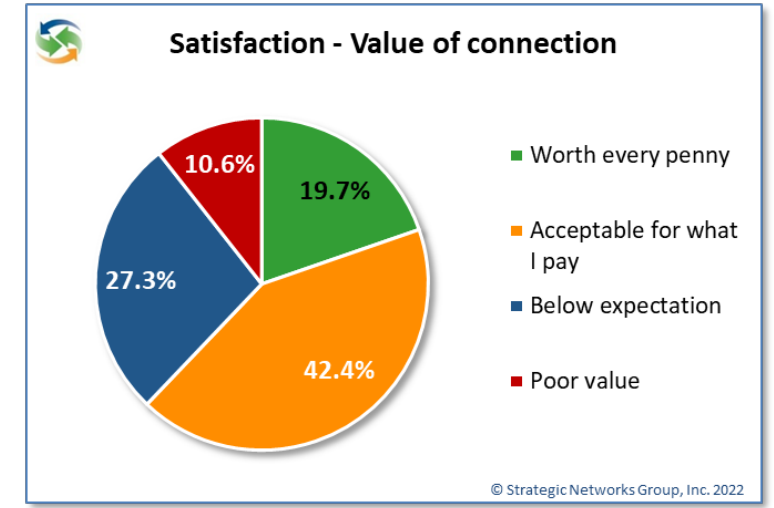
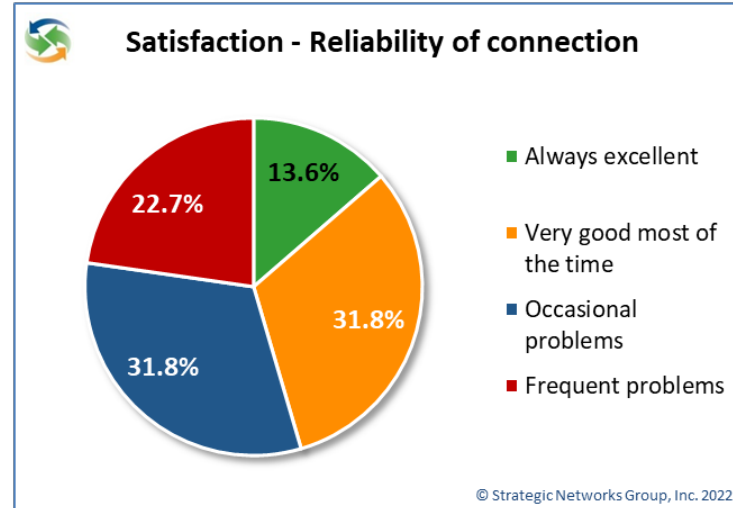
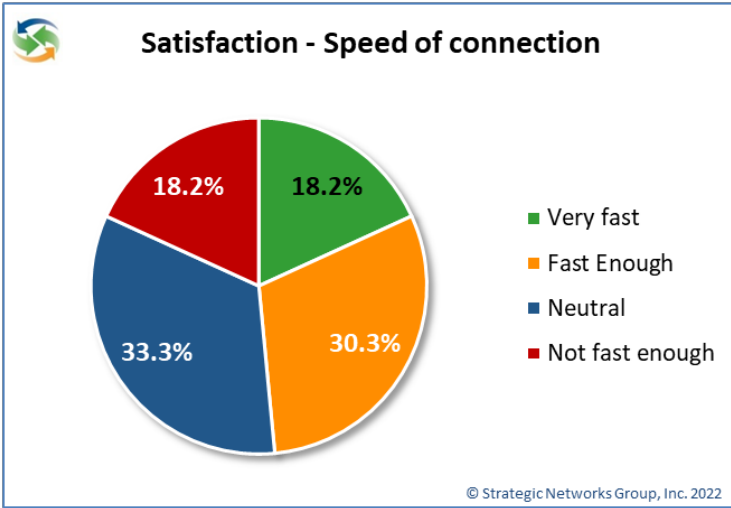
Measured Speeds – Businesses



- 47% of businesses have more than 50 Mbps download speed – this is good
- 39.5% of businesses have less than 25 Mbps download speed – not good
- Average speeds 72.7 Mbps down and 64.5 Mbps up – this is good overall, but there are a significant % of underserved

Over one third of businesses do not meet FCC 25/3 Mbps standards

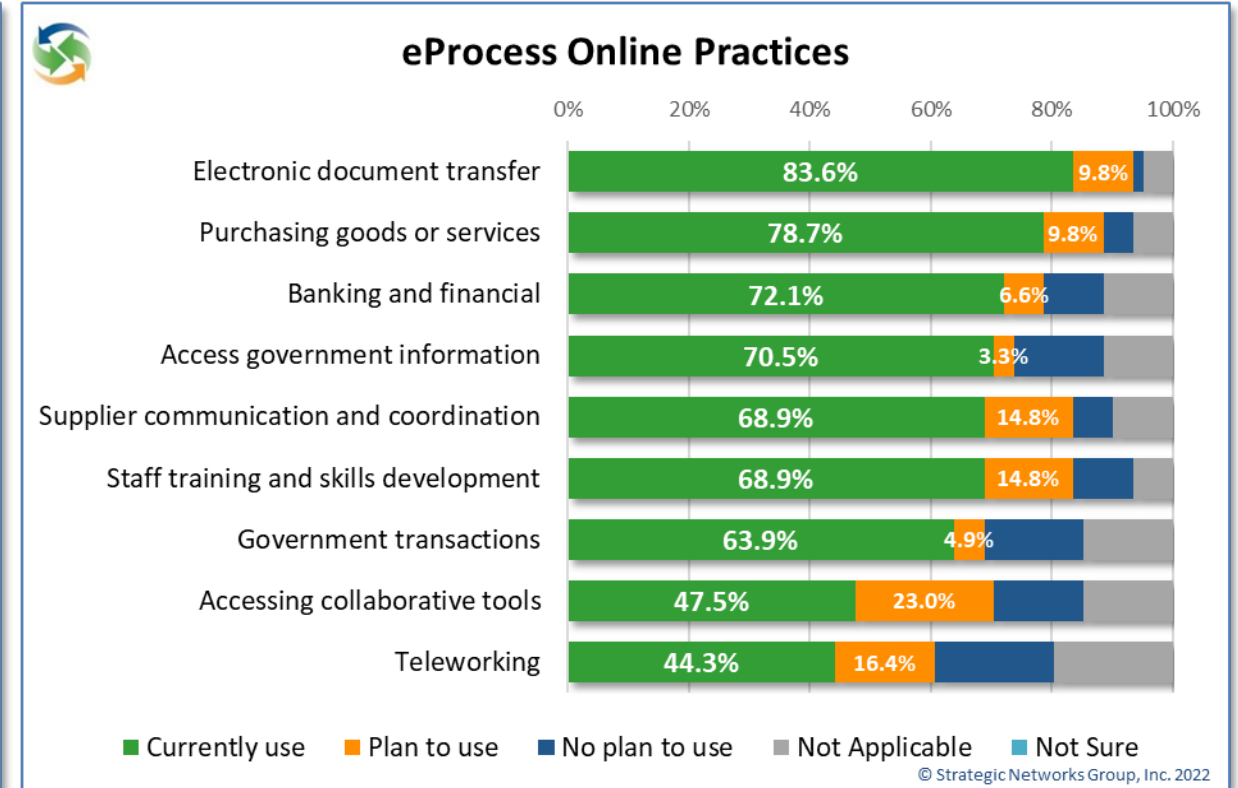
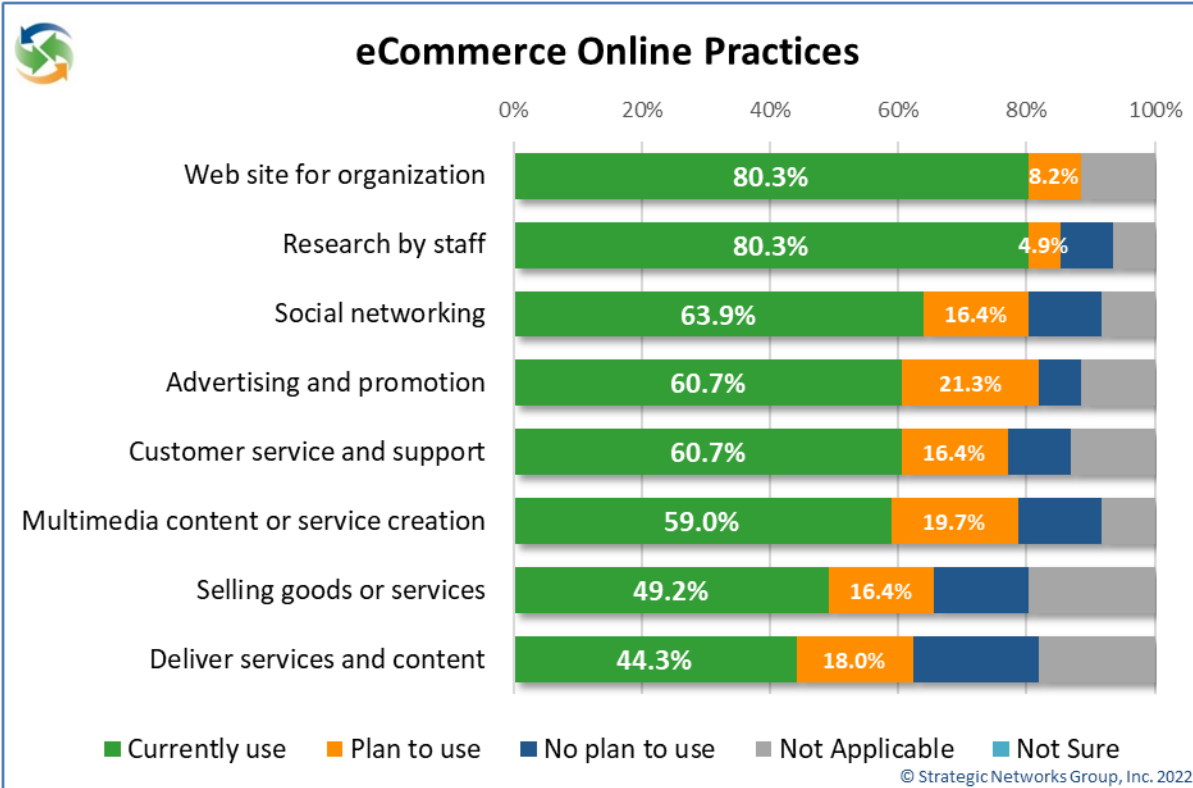
Business Satisfaction with Connectivity



- Reliability is the highest priority for service selection for most businesses
- 22% of businesses have frequent reliability problems with their connections
- 18% of businesses say their connection is not fast enough

Most businesses are less than satisfied with their connectivity

Online Business Practices

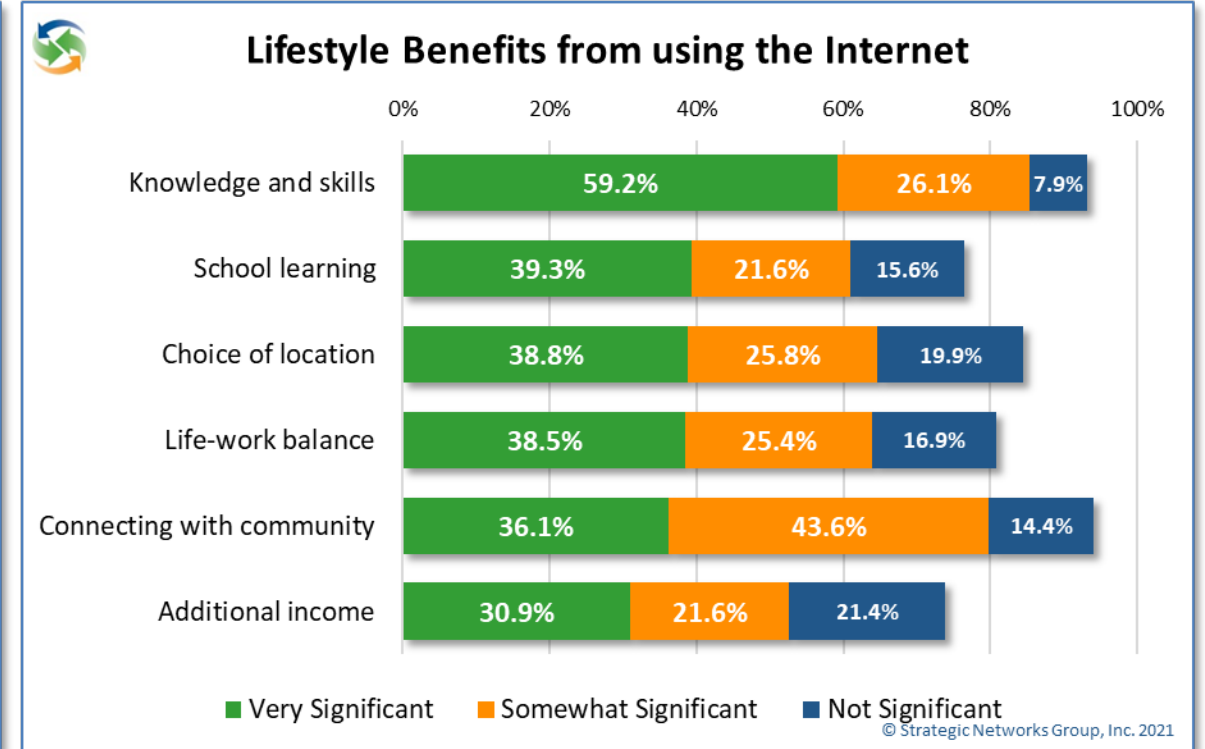
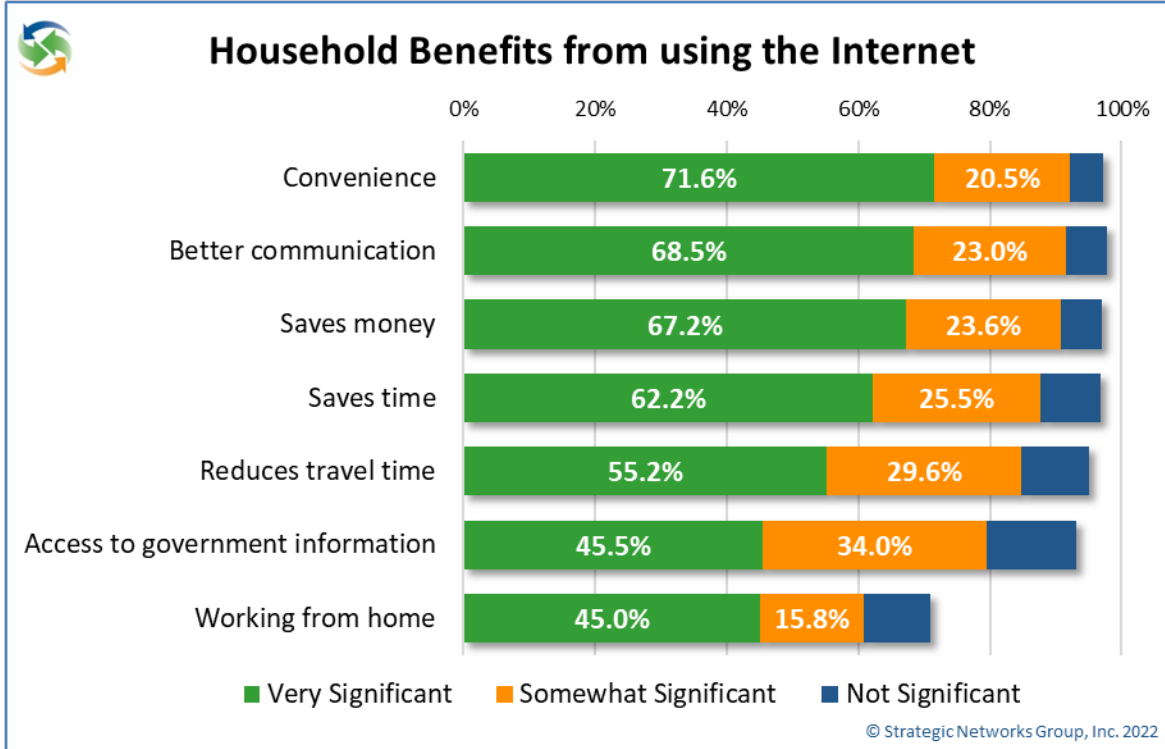


- Over 20% of Hermiston businesses plan to use the internet for advertising and promotion
- 80% of businesses report currently having a web site for their organization

Hermiston Broadband Additional Findings

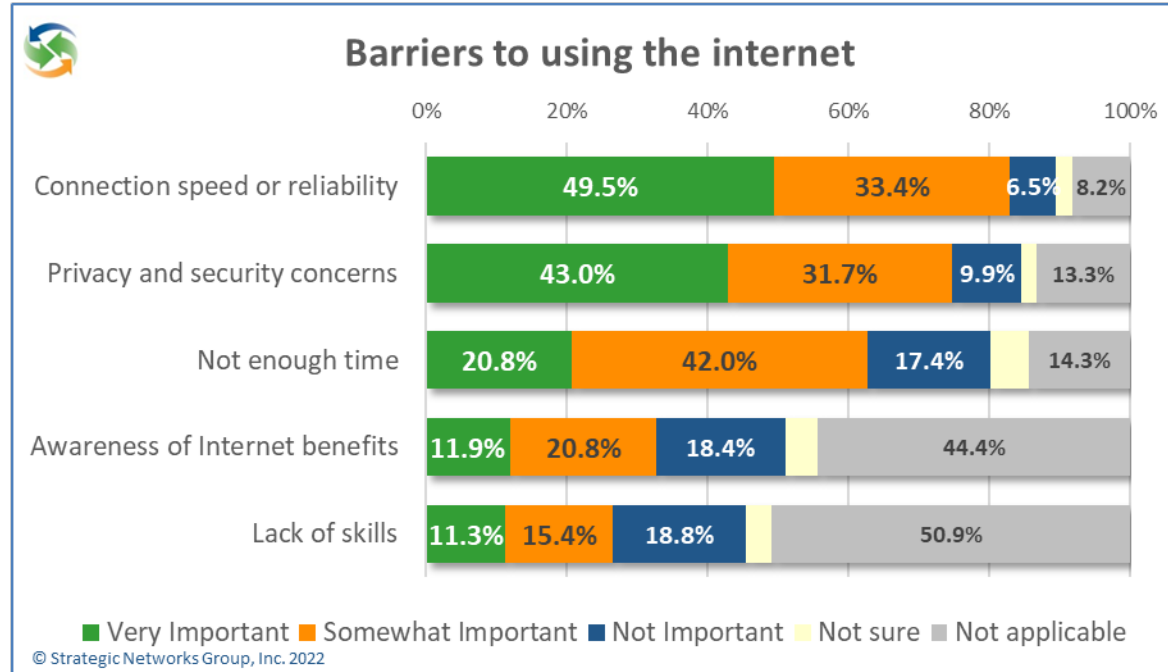
Household Results

Households Benefits from Online Usage



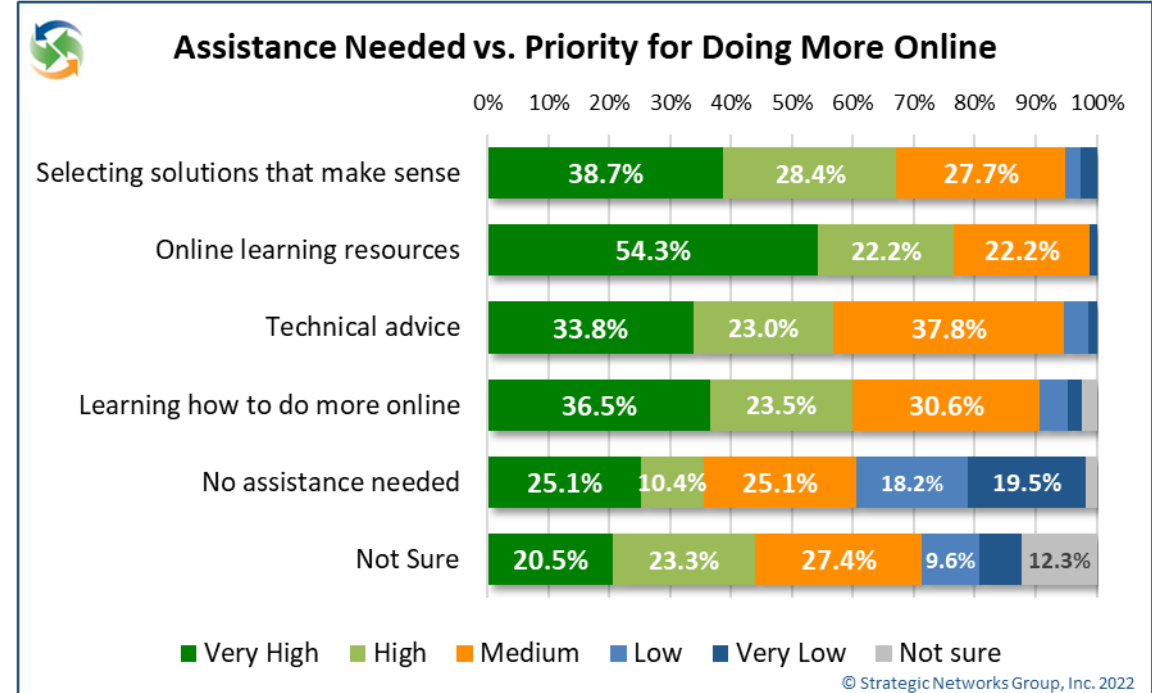
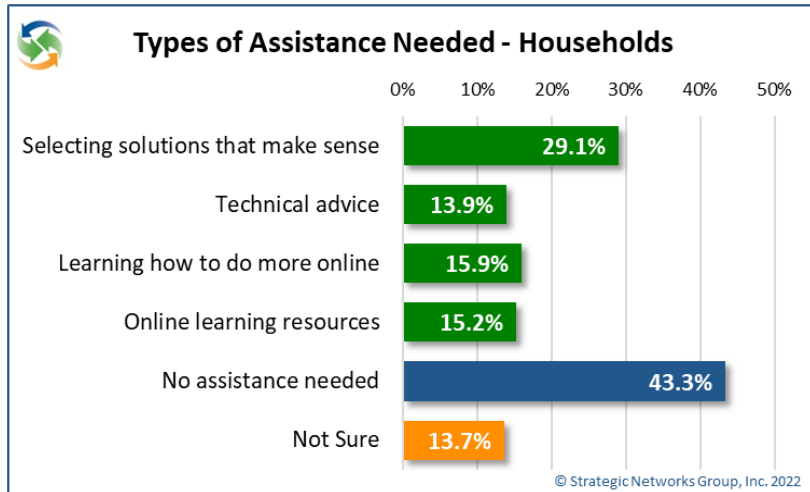
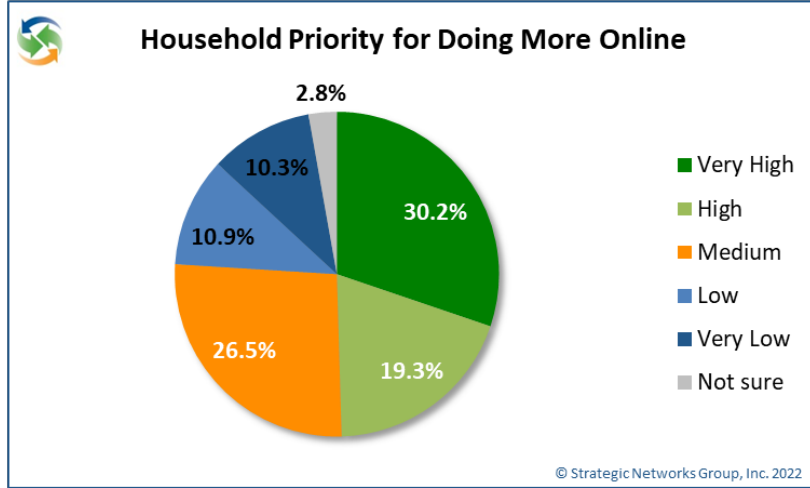
- Convenience, Communication, and Cost Savings were the main benefits reported from Households
- Many respondents credit online utilization to increasing their knowledge and skills

Barriers to Households Internet Usage

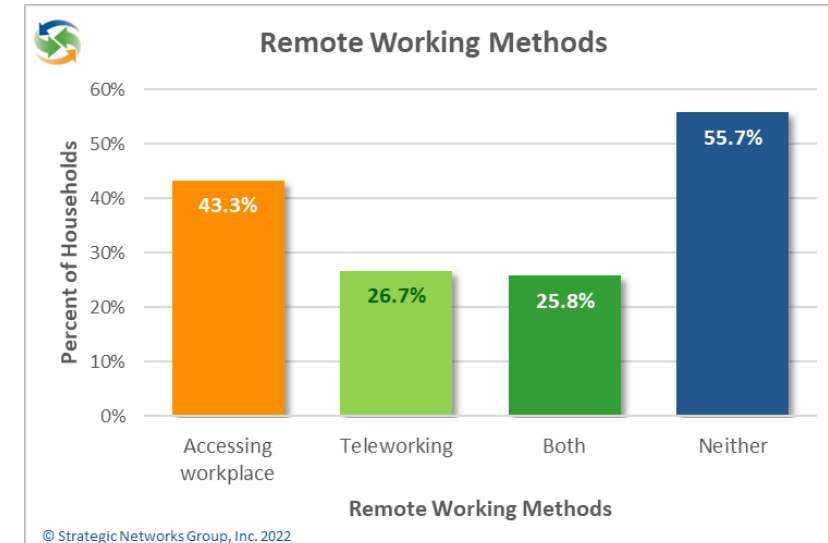
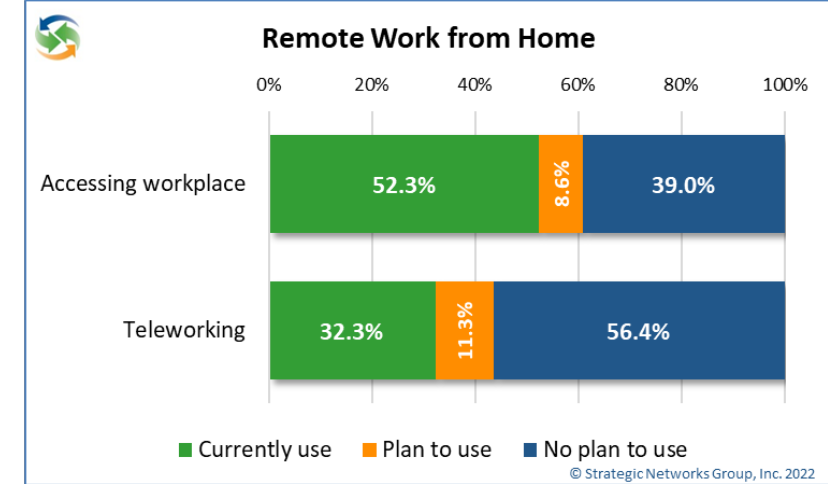
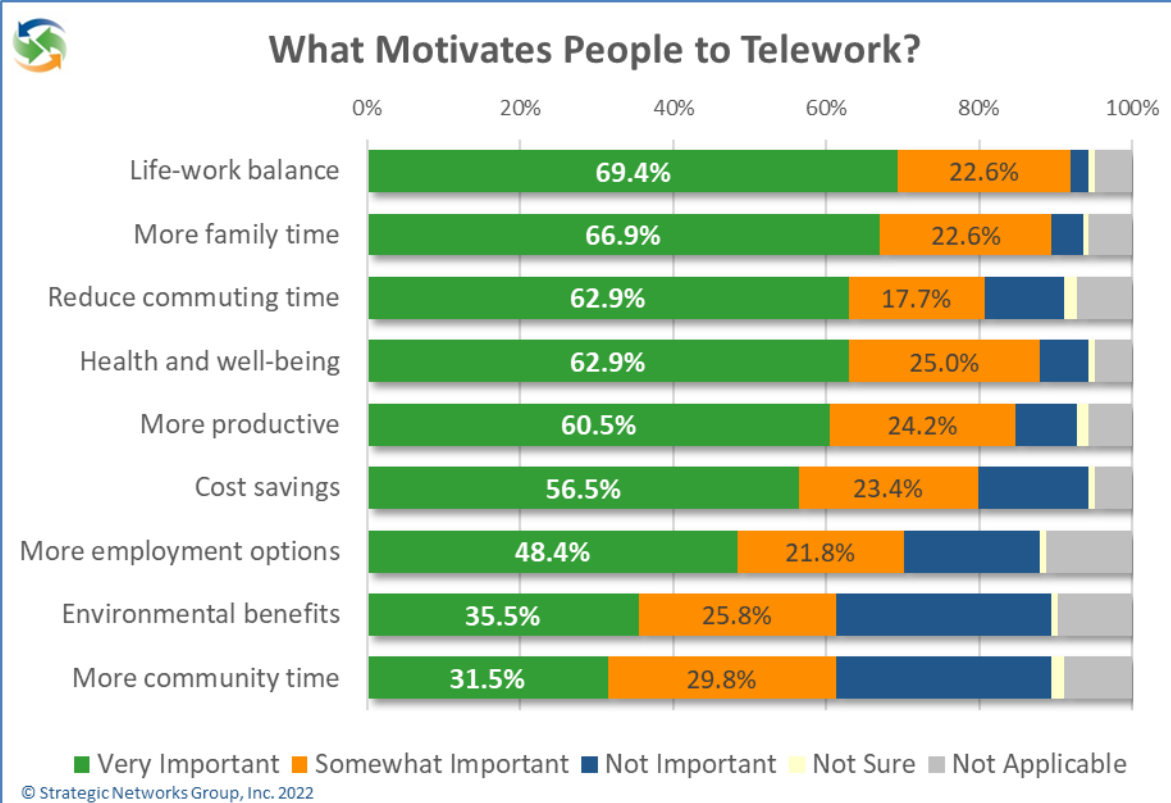


Connection Speed or Reliability is the most inhibiting factor to increasing online adoption

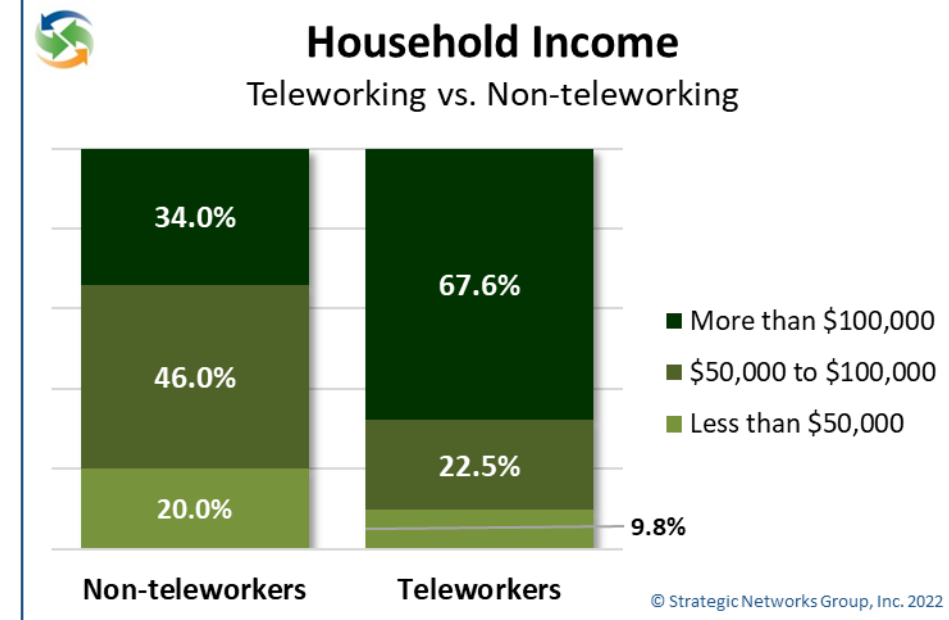
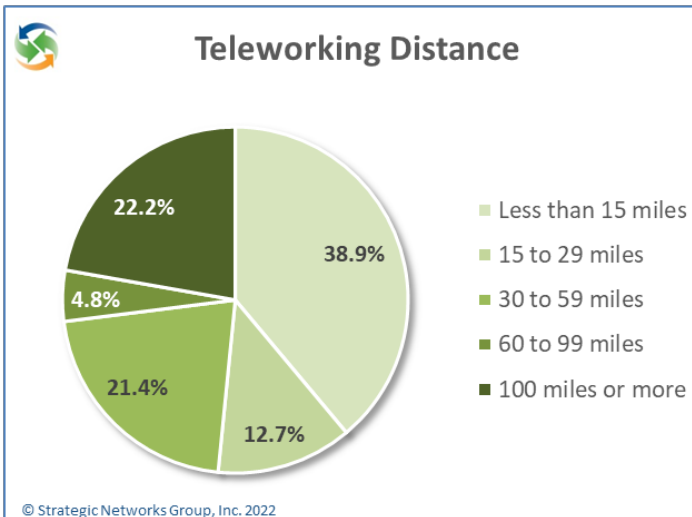
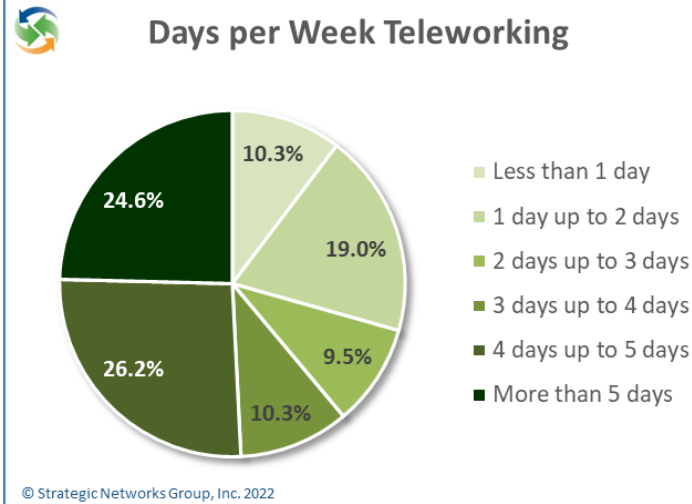
Getting Households Online



- Of those interested in doing more online- Online Learning Resources is the most viable way to increase adoption
- Respondents need help selecting online solutions that make sense

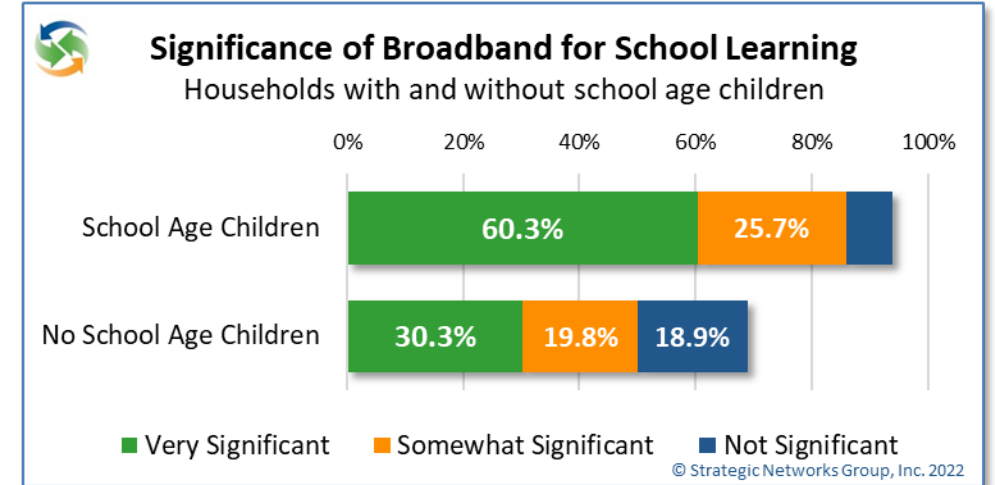
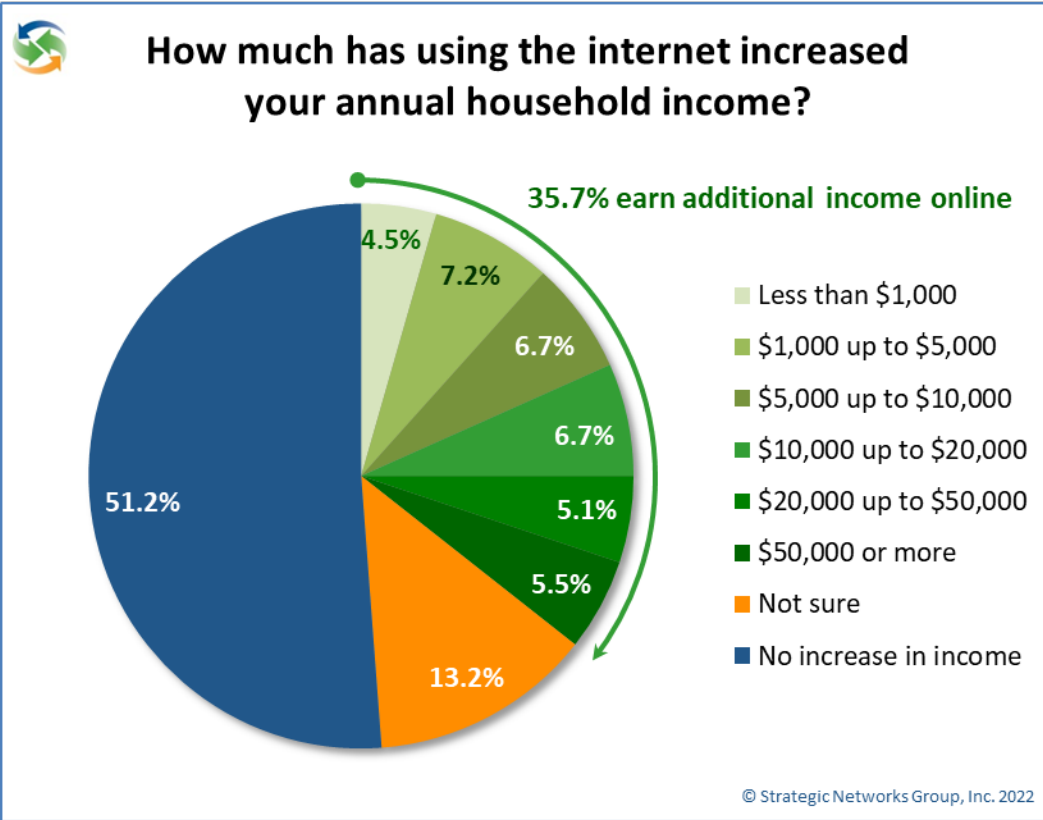


- **126 total respondents reported using the internet for Teleworking (15%)**
- **Lifestyle Benefits and More Family time are largest motivations**



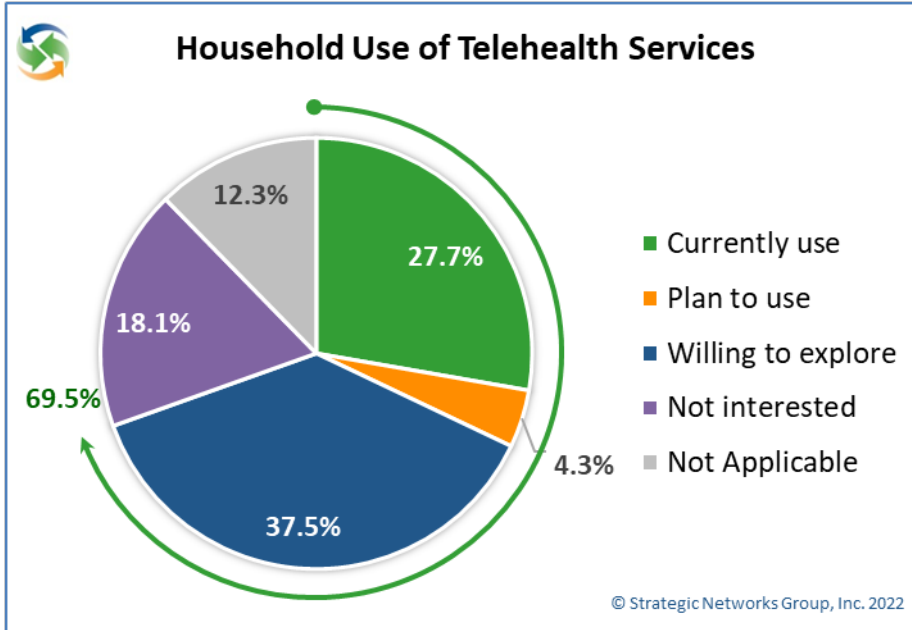
- About one fourth of Teleworkers do so for 5 days a week
- Teleworkers are averaging significantly higher incomes

Households Income and Education

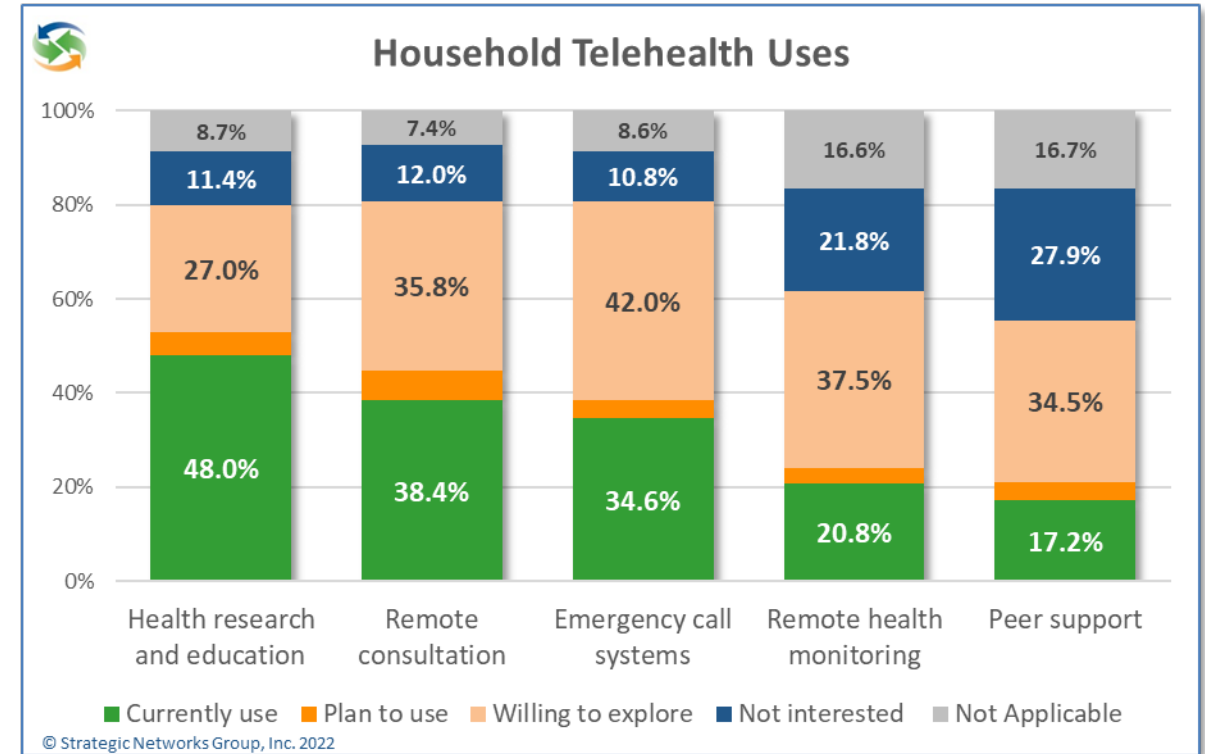


- **35.7% of respondents are earning additional income online**
- **Families with school age children (179) consider broadband highly significant for learning**

Telehealth Utilization



- A majority of respondents are willing to explore Telehealth services
- There is most interest in Emergency Call Systems



Thank You

Section 5, Item E.

Questions



CITY COUNCIL

Regular Meeting Minutes September 12, 2022

Mayor Drotzmann called the regular meeting to order at 7:00pm. Present were Councilors Hardin, Spicerkuhn, Peterson, Davis, Myers, Barron, and Duron. Council President Primmer was excused. Judge Creasing was in attendance, as well as staff to include: City Manager Byron Smith, Assistant City Manager Mark Morgan, City Attorney Rich Tovey, Chief Edmiston, Finance Director Mark Krawczyk, Parks and Recreation Director Brandon Artz, and City Recorder Lilly Alarcon-Strong. Youth Advisory Members were not in attendance. The pledge of allegiance was given.

Presentation- Hermiston School District Updates

Assistant Superintendent Jake Bacon gave HSD updates, to include: Elementary school ribbon cutting ceremonies, High School construction and parking, monitoring air quality for outside activities and events (due to smoke), student enrollment numbers, currently over 300 students are involved in extracurricular activities, 7th and 8th grade sport will be incorporating soccer, softball and baseball this year, student dress code, Amazon's Think Big Space Program partnership with BMCC and HSD, and the recent elementary school boundary changes.

Presentation- Community Counseling Solutions Update

Executive Director Kimberly Lindsay gave CCS information (attached) regarding: Their one-year anniversary providing addiction services in Hermiston, December will mark a one-year anniversary for providing mental health services in Hermiston; staff shortages, including bilingual staff needs, and the financial opportunities given to those who want a career in Counseling; the high demand and lengthy waiting list time frames for all services due to the lack of providers; Suicide statistics; CCS has partnered with Oxford House to provide it's first Home in Hermiston for recovering addicts; homelessness and their partnership with Stepping Stones; their work to purchase a building in Boardman specific to housing children 12 and under in foster care so they won't be transferred out of state due to the lack of foster care homes in the area, and more.

After some discussion, the Council thanked CCS and all their staff for all of their work providing these services to those who need it the most in our community.

Citizen Input on Non-Agenda Items

Umatilla County Commissioner Candidate Susan Bower spoke regarding her candidacy, business and leadership skillset, the County's growth, enhancement of Latino community, and homelessness resources, and stated she is hopeful to serve the community as County Commissioner (business card attached).

L-APS, Inc. President and Founder Amy Young gave information (attached) regarding the non-profit established by her and her husband that helps individuals going into, coming out of, or deciding to try other services to help with addiction, health or other behavior recovery as they noticed there was a lapse in care in for these services in the community. Having gone through recovery themselves, they knew what care was available and the help they could provide to those in need.

Consent Agenda Items

Councilor Spicerkuhn asked to remove item E as it was a conflict of interest. Councilor Spicerkuhn moved and Councilor Barron seconded to approve Consent Agenda items A-D to include:

- A. Committee Vacancy Announcements



CITY COUNCIL

Regular Meeting Minutes September 12, 2022

- B. Recommendation of Appointment to the Airport Advisory Committee: Ron Linn to position #5, expiring October 31, 2025
- C. Granting a non-exclusive utility easement across property at the Recycled Water Treatment Plant to MetroDuct Systems VA LLC.
- D. Minutes of the August 22 Work Session and Regular Meeting

Motion carried unanimously.

Items Removed from Consent Agenda

E. Initiate annexation proceedings 4N2804D Tax Lots 1100, 1101, 1102, & 1103- Nobles NW Sjoren Lane Councilor Spicerkuhn removed himself from the dais as this item was a conflict of interest.

Councilor Duron moved and Councilor Hardin seconded to approve Consent Agenda Item E. Motion carried unanimously.

Resolution No. 2237 – Adoption of Safety Action Grant Application

City Manager Smith stated this resolution will be used to support the City's efforts to secure grant funding for future transportation related projects as adoption of a Safety Action Plan is required by the funding program.

After some discussion, Councilor Duron moved and Councilor Spicerkuhn seconded to adopt Resolution No. 2237 and lay upon the record. Motion carried unanimously.

Committee Reports

Parks and Recreation Advisory Committee- Councilor Myers gave updates regarding: the Aquatic Center, Riverfront Park, Splash Pad repairs, 4th of July Fireworks, and Teen Adventure Park

EOTEC Advisory Committee- Mayor Drotzmann spoke regarding this year's Fair statistics, including possibly moving Latino Night from Friday to Saturday as it has the most attendance and is the most financially generating day of the week, barns are running out of space and how to address it, the continued record-breaking attendance, and more. The Rodeo did not present statistics. Field expansions for HEROS, and the upcoming Fall Festival and Faith & Blue events.

Stepping Stones (not a City Committee)- Councilor Spicerkuhn gave updates regarding: Funding, donations, September 24th work party in Umatilla to build huts, seeking employees, and trying to open the facility by winter as the warming station has officially closed.

Faith-Based Advisory Committee- Councilor Hardin stated the Committee welcomed Kaitlyn Wadkins the youth advisor member, the Fall Festival and Faith & Blue events, Soul Care provided by Good Shephard, and adoption of the new Elementary School.

Hispanic Advisory Committee- Councilor Barron stated the Committee will meet next week to discuss the possibly creation of a Hispanic Resource Center. There was also some discussion regarding if the Cinco de Mayo Committee (not a City Committee) would convene to continue this event.

Mayor's Report

Mayor Drotzmann spoke regarding:

- The County's Mayors and City Managers recent meeting
- Encouraged participation in the LOC Homelessness Session on Sept 19th at the Community Center



CITY COUNCIL

Regular Meeting Minutes September 12, 2022

Council Reports

Councilor Peterson spoke regarding her reappointment to the Woman's Caucus and encouraged the participation of other in other committees.

Councilor Myers stated she attended the elementary schools ribbon cuttings and they were well attended.

Youth Advisory Report

None given.

City Manager's Report

City Manager Smith spoke regarding:

- The possible rebranding of EOTEC
- City Hall furniture delivery and setup is scheduled for the week of September 26th
- Working with Sara Singer-Wilson on Goal Setting dates
- ICMA Annual Conference next week

Adjournment

Mayor Drotzmann adjourned the City Council meeting at 8:27pm as there was no other City business.

SIGNED:

Dr. David Drotzmann, Mayor

ATTEST:

Lilly Alarcon-Strong, CMC, City Recorder

| | <u>Feb 2021 – November 2021 (Lifeways)</u> | <u>December 2021 – August 2022 (CCS)</u> |
|-------------------------------------------|--------------------------------------------|------------------------------------------|
| Total number of mental health assessments | 600 | 1094 |

Average number of assessments completed per month for 2/21 – 11/21 period: 60

Average number of assessments completed per month for 12/21 – 8/22 period: 122

The average number of completed assessments per month has doubled.

There has been roughly an **82% increase** in completed assessments completed since December 1, 2021. This presumably would mean an 82% increase in demand for services. The staff have been doing a lot of work, much more work, with fewer clinicians to do the work. I say roughly as the first number is for a 10 month period, and the second is 9 months.

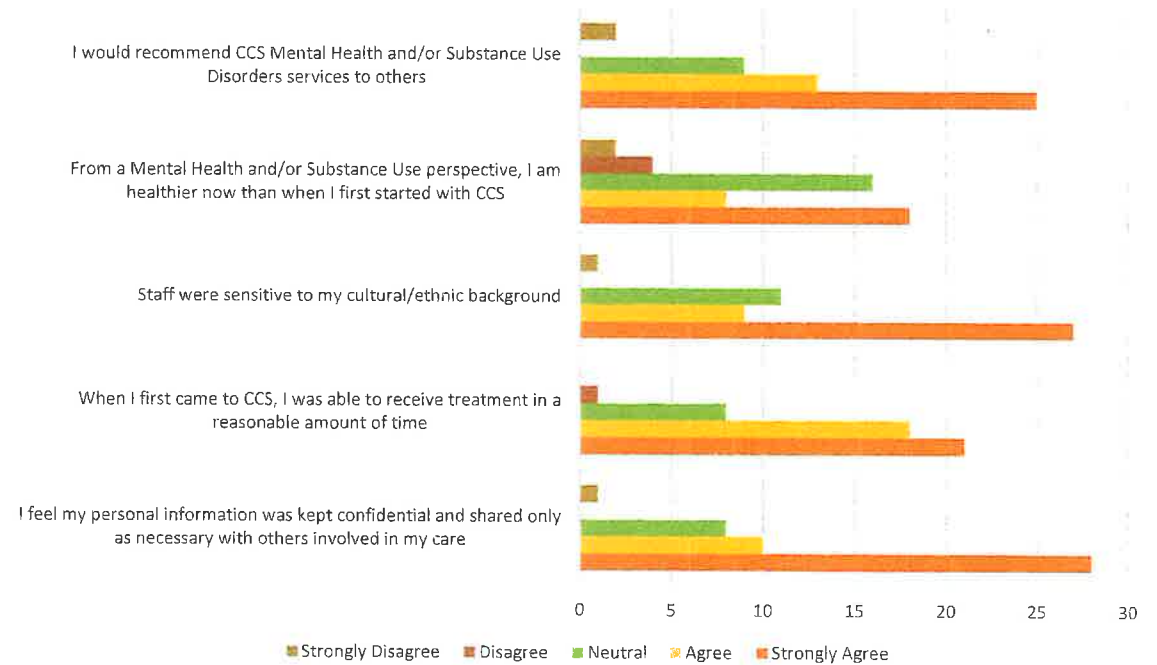
Community & Counseling Solutions



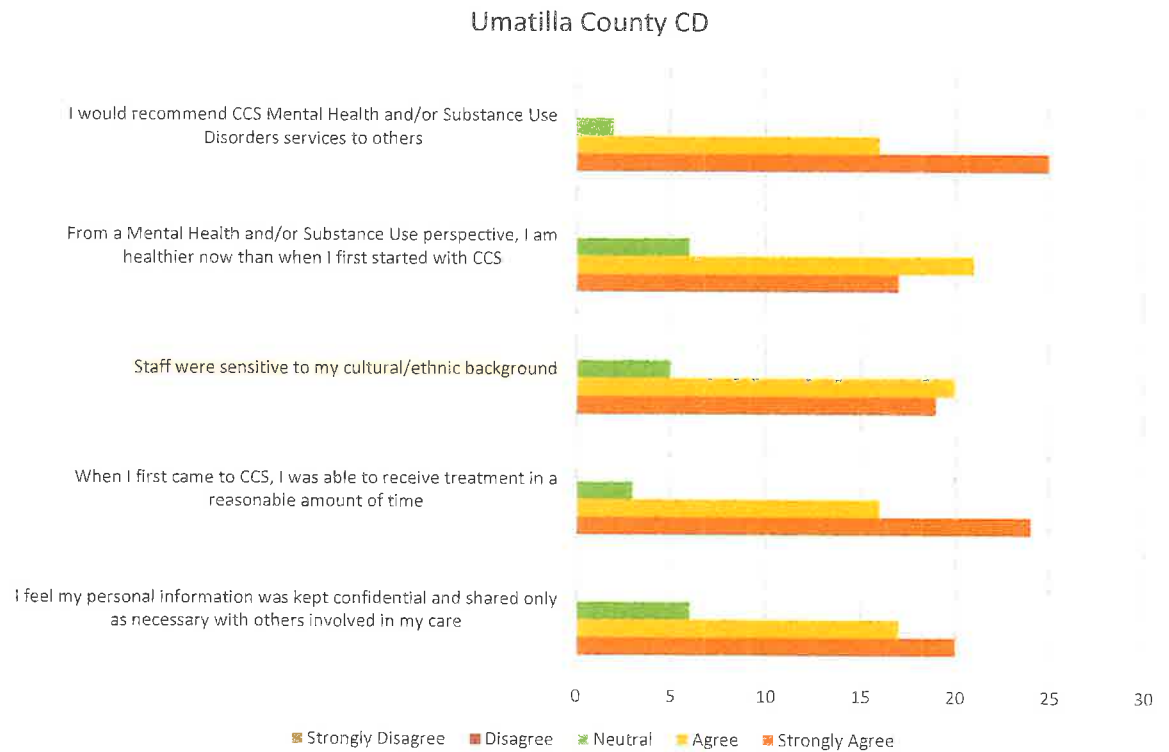
Changing Lives

Umatilla County MH Consumer Survey Results

Umatilla County MH



Umatilla County CD Consumer Survey Results



People At Risk For Suicide Are Falling Through the Cracks in Our Health Care System

In the month before their death by suicide:

- Half saw a general practitioner
- 30% saw a mental health professional

In the 60 days before their death by suicide:

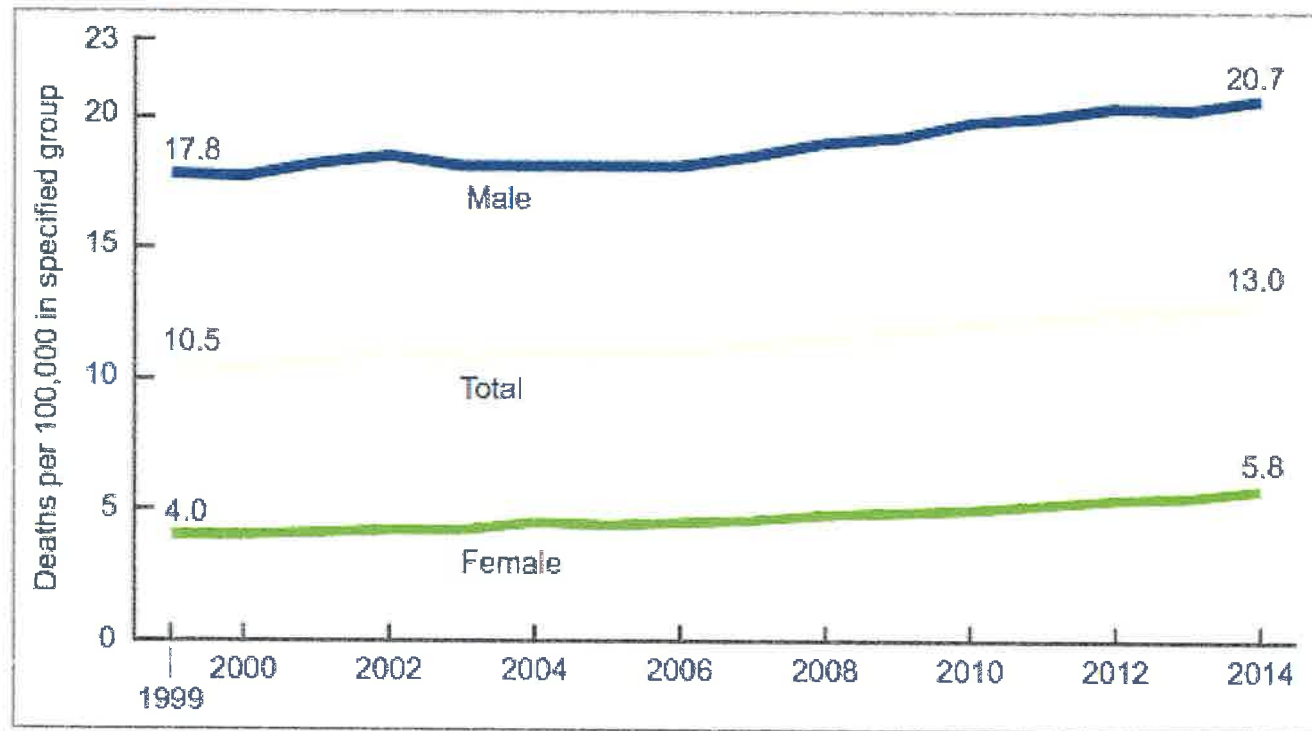
- 10% were seen in an emergency department



Suicide rates increased from 1999 through 2014, with greater annual percent increases after 2006.

- The age-adjusted suicide rate in 2014, 13.0 per 100,000 population, was

Figure 1. Age-adjusted suicide rates, by sex: United States, 1999–2014



NOTES: Suicide deaths are identified with codes U03, X60–X84, and Y87.0 from the *International Statistical Classification of Diseases and Related Health Problems, Tenth Revision*. Access data for Figure 1 at: http://www.cdc.gov/nchs/data/data-briefs/db241_table.pdf#1.

SOURCE: NCHS, National Vital Statistics System, Mortality.



More statistics...

- ❖ Oregon has seen a 28.2% increase from 1999 to 2016.
- ❖ CDC says that more than half of the people who die by suicide do not have a known mental health condition.
- ❖ Over 70% of older adults who died by suicide saw a primary care provider within 1 month of death.



Suicide Facts & Oregon 2020

Section 5, Item E.



On average, one person died by suicide every 10 hours in the state.

More than five times as many people died by suicide in Oregon in 2018 than in alcohol related motor vehicle accidents.

The total deaths to suicide reflected a total of 16,408 years of potential life lost (YPLL) before age 65.



Suicide cost Oregon a total of **\$740,356,000** combined lifetime medical and work loss cost in 2010, or an average of **\$1,080,811** per suicide death.



8th leading cause of death in Oregon

1st leading
cause of death for ages 10-24

2nd leading
cause of death for ages 25-34

3rd leading
cause of death for ages 35-44

5th leading
cause of death for ages 45-54

8th leading
cause of death for ages 55-64

13th leading
cause of death for ages 65+

Suicide Death Rates

| | Number of Deaths by Suicide | Rate per 100,000 Population | State Rank |
|---------------|-----------------------------|-----------------------------|------------|
| Oregon | 844 | 19.02 | 16 |
| Nationally | 48,344 | 14.21 | |

CDC, 2018 Fatal Injury Reports (accessed from www.cdc.gov/injury/wisqars/fatal.html on 3/1/2020).



American
Foundation
for Suicide
Prevention

Suicide Care in Behavioral Health Care Settings

- Suicide prevention is a core responsibility for behavioral health care systems
- Many licensed clinicians are not prepared
 - 39% report they don't have the skills to engage and assist those at risk for suicide
 - 44% report they don't have the training



What is Zero Suicide?

- A *priority* of the National Action Alliance for Suicide Prevention
- A *goal* of the National Strategy for Suicide Prevention
- A *project* of the Suicide Prevention Resource Center
- A *framework* for systematic, clinical suicide prevention in behavioral health and health care systems
- A *focus* on safety and error reduction in healthcare
- A *set of best practices* and tools for health systems and providers



ZEROSuicide

IN HEALTH AND BEHAVIORAL HEALTH CARE

Element #1: Lead

Create a leadership-driven, safety-oriented culture committed to dramatically reducing suicide among people under care. Include suicide attempt and loss survivors in leadership and planning roles.

Element #2: Train

Develop a competent, confident and caring workforce.

Element #3: Identify

Systematically identify and assess suicide risk among people receiving care.

Element #4: Engage

Ensure every person has a suicide care management plan, or pathway to care, that is both timely and adequate to meet patient needs.

Element #5: Treat

Use effective, evidence-based treatments that directly target suicidality.

Element #6: Transition

Provide continuous contact and support, especially after acute care.

Element #7: Improve

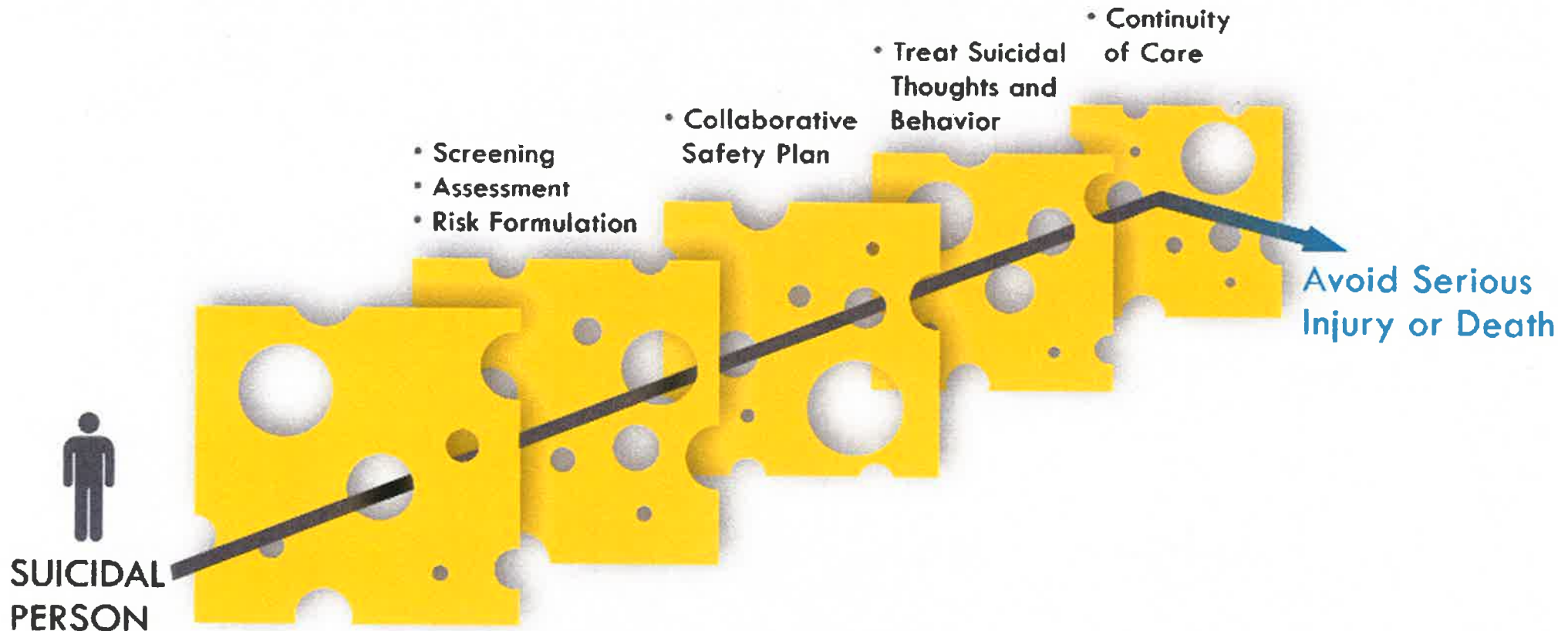
Apply a data-driven quality improvement approach to inform system changes that will lead to improved patient outcomes and better care for those at risk.

“It is critically important to design for zero even when it may not be theoretically possible...It’s about purposefully aiming for a higher level of performance.”

Thomas Priselac
President and CEO of Cedars-Sinai Medical Center



The Swiss Cheese take...



Adapted from James Reason's "Swiss Cheese" Model Of Accidents

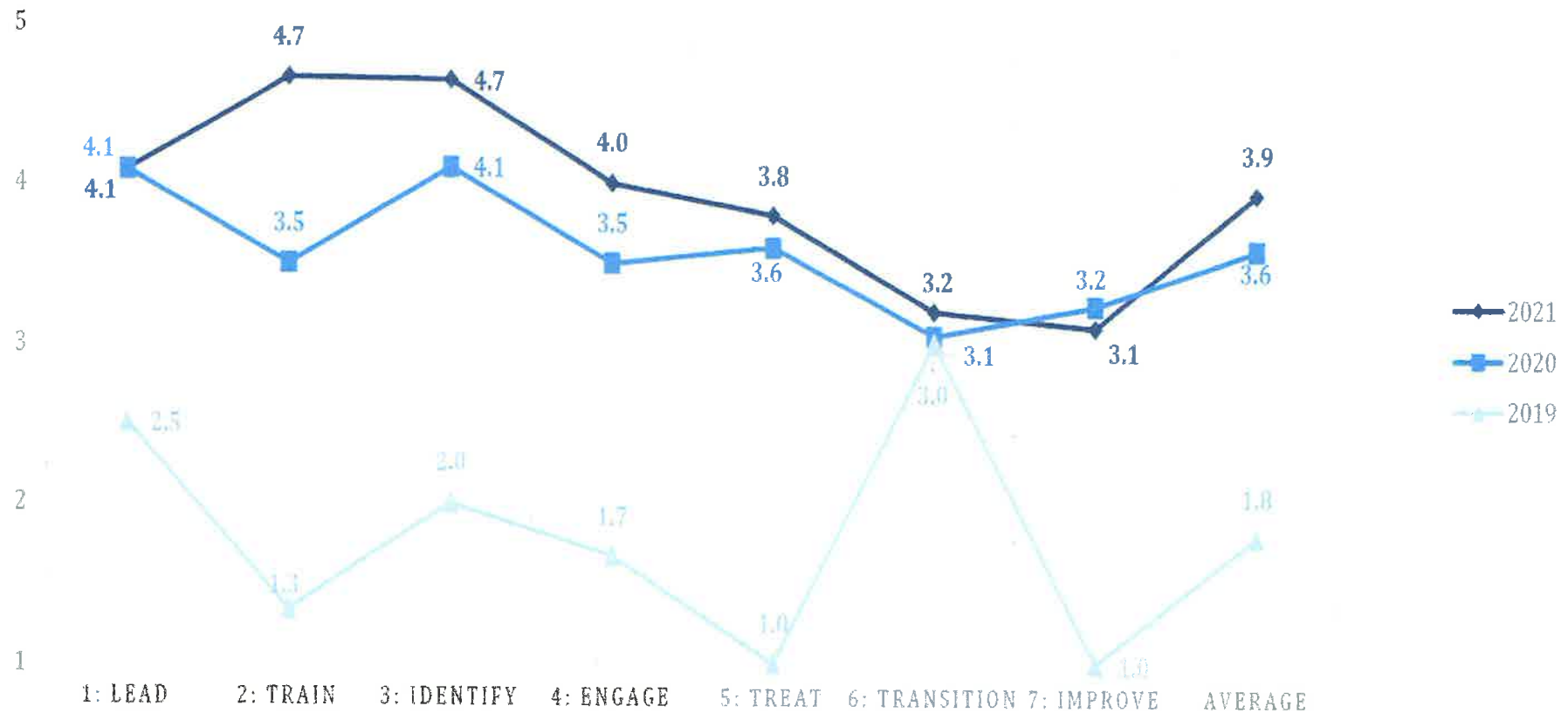
Whats Different About Zero Suicide

- » Suicide prevention is accepted as a core responsibility of health care
- » Patient deaths by suicides are not treated as inevitable
- » Emphasizes data, best practices, and continuous quality improvement
- » A systematic clinical approach in health systems, not “the heroic efforts of crisis staff and individual clinicians.”

Community Counseling Solutions Zero Suicide Implementation Change in Scores by Element from 2019 to 2020 to 2021

(Data sources: Organizational Self Study, Zero Suicide Metrics, Conversations with Staff)

Note: Scores from 2019 and 2020 are using the Zero Suicide Institute's Assessment. Scores from 2021 are using the Oregon Zero Suicide Assessment.



We got this...

- “It isn’t reasonable to ask that we achieve perfection. What is reasonable is to ask that we never cease to aim for it.”

Dr. Atul Gawande





L - APS, INC.

Learning, Living & Loving

Section 5, Item E.

"Fall seven times, Stand up eight" — Japanese Proverb

President/Founder- Amy Jo Young-BS Vice-President/Co-Founder- William "Joshua" Young

Secretary/Treasurer- Tanya Hunt Board Member Tami S. Jackson-Rochelle

From the Desk of the President:

Hello; My name is Amy Jo Young-BS. It has been a journey since May 14th, 2021, when my husband and I decided to establish a non-profit, L- aPs, Inc. (Learning, Living, and Loving after/along with Programs), that will serve and benefit Morrow and Umatilla County by providing a bridge from lapse support when people are coming out of, going into, in-between, and deciding to try other services for their addiction, life, and/or behavioral health recovery.

This is a non-traditional organization that is enthusiastic about developing program/s that will allow folks to "Belong" while also focusing on building abilities in a new self-directed recovery atmosphere.

With a history of numerous Peer Support Certifications (Family Support, Adult Addiction Support, Adult Mental Health Support, and Employment Support) since October 2014, employed at Lifeways, Inc., Eastern Oregon Recovery Center (EORC), and currently at Maple Star Oregon, as well as my own lived experiences living in recovery for the past 15 years, I am uniquely qualified to support, encourage, empathize, and advocate for others in their self-directed recovery. I am certified through Oregon Health Authority (OHA) as a Traditional Health Worker (THW) and hold a Qualified Mental Health Assistant 1 Certification through the Mental Health & Addiction Certification Board of Oregon (MHACCBO).

My husband and I have both lived experiences with houselessness, addiction, and life recovery from the justice system. We are creating an organization to encourage individuals and community partners to collaborate for the betterment of all those who require the services being explored to support their self-directed recovery. Developing a person-first approach that meets people where they are at and what they need, rather than what one assumes they should work on first. Recognizing that an individual is more knowledgeable about their circumstances than anyone else. Providing options that aid in the progression of the circumstances and knowing that by supporting is powerful. Developing our Mission so that people can "Belong" (noun) to be seen, understood, and be loved without having to change themselves."

We have a vision for this organization for the next ten years. That vision includes the following:

2022-2023 Free Mobile Thrift/Community Fridge

Our organization decided to develop a project that would help the people of Umatilla/Morrow County, who are now experiencing difficulties even getting what they need, let alone something they want.

We all have wanted to buy at yard sales and thrift stores, and visit local fruit stands and farmer markets, yet due to financial limitations at this moment in our economy, those excursions might not be happening.

80542 Paterson Ferry Rd. #44-45 / P.O. Box 673 Irrigon, OR 97844

Phone: (541)-571-2417 **Email:** l-aps@outlook.com

Website: <http://www.l-aps.org> **Facebook:** <http://www.facebook.com/LaPsBelongingIsKey/>

As a brand-new peer-driven organization, we want to engage with the communities, we want to serve and gather goods that can be distributed to those who need what they need for themselves and their homes, whether it be furniture, food, clothing, or assistance finding and comprehending the numerous community resources that can be accessible for supportive services.

2023-2024 Online Peer Support Platform

Online platform for connecting to supportive listening and connecting with other humans who have lived through similar experiences. Our organization has the chance to collaborate with an already established and recognized online platform Hey Peers! which believes peer support should be simple, safe, personalized, and affordable. Hey Peers! is on a mission to assist more people in bettering their lives by combining innovative technologies and evidence-based peer support programs. L -aPs, Inc., wishes to provide this incredible online capability that will be created to be supportive and tailored to Umatilla and Morrow County needs, allowing users to connect to personalized peer support and access groups to join, being convenient and experiencing the ability to connect from your own home, being anonymous or known and sharing what is comfortable to share, and ultimately offering flexible access for those who have difficulty attending site-based support sessions.

2024-2025 Community Peer Drop-In Center

A friendly setting that provides peer support, case management, a community resource center for social activities, job support, and referrals to other agencies. Games, drinks, and the ability to do laundry, take a shower, relax, and revitalize oneself, among other things, are available. Creating that sense of "Belonging" that everyone needs and desires.

2025-2027 Working Recovery Farm (1) and Halfway House (2)

(1) Offering a short-term to long-term living environment for individuals who are recovering from alcohol and addiction. Working and providing a safe environment for people to self-direct their recovery. This will be a non-traditional program where people can find a place to belong and focus on acquiring new skills in their new recovery. Individuals will garden, care for animals, participate in meetings and workshops, build work skills, explore career prospects and community service connections, and participate in self-evaluations to guide their recovery. With both programs, the person is getting time and support to strengthen their recovery choices from the beginning thoughts to the achievement of time within recovery.

(2) Offering a place to rest, renew, and re-integrate self to obtain skills and support to go forward.

Within ten to fifteen years the hope of having our organization within other rural communities tailored to their needs for their communities would be priceless for all that self-direct their recovery from addiction, life, and behavioral health.

Respectfully yours,

Amy Jo Young-BS

Amy Jo Young-BS
PSS/THW/ QMHA1
President/Founder L -aPs, Inc.





"Fall seven times, Stand up eight"—Japanese Proverb

Our Mission:

***Belonging* (noun)**

"To be seen, understood, and valued without needing to change yourself"

Values:

Believing in another person and oneself to have the strength to self-direct recovery

Empathy has the ability to understand and share the feelings of another

Listening to the story and experiences, meeting where another person is at, and supporting the needs to self-direct recovery.

Opportunities become available when growing in hope, health, and wellness

Nature is offered to be embraced and inspiring to give refreshing insight to develop strong relationships, and community and be the first steps in recovery

Grateful to have an experience offered to retreat, relax, & recharge the physical and mental space

Inspire recovery and to be able to be filled with desire, hope, optimism, and motivation

Novice to recovery, a learner to the subject, and new to the work of a new beginning

Gain support, and encouragement, receive practical assistance from others who have shared knowledge and experiences, be taught skills, connect to resources, opportunities, and communities of support & other people

President/Secretary- Amy Jo Young
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Vice-President- William "Joshua" Young
Board Member- Tami S. Jackson-Rochelle



Where Life is Sweet

Mayor and Members of the City Council

STAFF REPORT

For the Meeting of **September 26, 2022**

Title/Subject

Resolution 2239- Long Term Rural Enterprise Zone Agreement: PDX245

Summary and Background

Amazon Data Services has submitted an application for a Long-Term Rural Enterprise Zone (LTREZ) abatement for two planned new campuses within the Greater Hermiston Enterprise Zone (GHEZ). Both applications meet the minimum criteria to qualify for an LTREZ abatement which requires at least \$200 million in new investment, creation of at least 10 new permanent jobs, and average compensation that is at least 130% of the average wage in Umatilla County.

The terms of the agreement are very similar to the existing agreement that is in place with ADS within the GHEZ. As part of this agreement, ADS will pay to the zone:

1. Annual Improvement Payments (AIP) equal to \$5 per square foot of data center construction. The AIP shall not be less than \$2 million per year, and is expected to reach approximately \$4 million per year at full build-out.
2. Additional Annual Fee (AAF) equal to what the property tax would be on a valuation of \$27,318,175 beginning in 2023. That assumed valuation will index up annually to offset inflation.
3. Public Safety Impact Fee (PSIF) of \$54,636 beginning in 2023. The PSIF will index up annually to offset inflation.
4. Student Success Fee (SSF) of \$54,636 beginning in 2023. The SSF will index up annually to offset inflation.

Additional stipulations within these agreements, which were not included in the previous agreement are:

1. ADS shall, within 60 days of beginning construction, pay \$5 million to the GHEZ as a Community Development Contribution.
2. ADS shall, within 14 days of execution of the agreement, pay an application processing fee of \$120,000.

3. The value of the facilities shall be made available for taxation on bonds issued financing public utilities, public educational services, fire services, or other public safety services. ADS will then make a contribution toward the bond payments equal to what it's tax obligation would have been. This will have the effect of making bond obligations more affordable for other taxpayers in the community.

At full build-out, amortizing the Community Development Contribution over the 15 year life of the agreement, it is projected that total annual fees paid to the GHEZ per campus will be approximately \$4,975,000. This would make each campus the single largest financial contributor to local government services by far. Each one would pay nearly 7 times more than the current highest property tax payer within City Limits.

Additionally, the amount paid for local government services through this agreement is even more disproportionately high when considering the relatively low impact on local government services. Although each site is anticipated to employ 120 permanent full time employees, that is significantly lower than some of our other large industrial employers; who's employees often require educational services for their kids, make calls for service to emergency response agencies, etc.

Here is a table showing the amount of either fees or taxes paid to local governments by the top commercial/industrial employers in the Hermiston area. You can see that each site will be paying more than 10 times more per employee than the next highest per-employee payer.

| Top Commercial/Industrial Taxpayers in Hermiston Area 2020 | | | |
|-------------------------------------------------------------------|-------------|---------------------|-----------------------------|
| | Jobs | Tax/Fee Paid | Tax/Fee Per Employee |
| AWS (Proposed PDX245 LTREZ)* | 120 | \$ 4,975,606 | \$ 41,463.38 |
| AWS (Proposed PDX146 LTREZ)* | 120 | \$ 4,975,606 | \$ 41,463.38 |
| Pioneer Seed | 227 | \$ 728,832 | \$ 3,210.71 |
| Columbia Basin Bioscience | 60 | \$ 157,217 | \$ 2,620.28 |
| Lamb Weston- Hermiston** | 625 | \$ 1,211,176 | \$ 1,937.88 |
| Wal-Mart Store | 356 | \$ 415,331 | \$ 1,166.66 |
| Marlette Homes | 250 | \$ 170,616 | \$ 682.46 |
| Wal-Mart DC | 1050 | \$ 632,895 | \$ 602.76 |
| Shearer's | 400 | \$ 157,313 | \$ 393.28 |
| Good Shepherd | 767 | \$ 138,386 | \$ 180.43 |
| Two Rivers Correctional Institution | 440 | \$ - | \$ - |
| *Scheduled at Full Build-Out | | | |
| **Includes 2017 LTREZ | | | |

Tie-In to Council Goals

Fiscal Prudence.

Fiscal Information

See above. It is important to note that the figures outlined here for each of the various fee requirements apply to both agreements individually. I am simply covering them together here for expedience.

Alternatives and Recommendation

Alternatives

1. Approve Resolution 2239
2. Reject Resolution 2239
3. Direct Staff to request changes

It is important to keep in mind that the City of Hermiston is co-sponsors with Umatilla County on the GHEZ. Therefore, any recommended changes must be agreed to by Umatilla County.

Recommended Action/Motion

Motion to approve Resolution 2239.

Submitted By:

Mark Morgan

AGREEMENT FOR LONG-TERM RURAL ENTERPRISE ZONE TAX ABATEMENT

This agreement for Long-Term Rural Enterprise Zone Tax Abatement ("Agreement") is executed by GREATER HERMISTON ENTERPRISE ZONE BOARD, representing the sponsor entities UMATILLA COUNTY, OREGON ("Umatilla County"), the CITY OF HERMISTON, OREGON ("City of Hermiston"), and Amazon Data Services, Inc. (the "Company"). Umatilla County and City of Hermiston are referenced collectively in this Agreement as "Sponsors." In this Agreement, each of the Sponsors and the Company are sometimes individually referenced as a "Party" and collectively referenced as the "Parties."

RECITALS

The Sponsors have established the Greater Hermiston Enterprise Zone (the "Zone") pursuant to Oregon Revised Statute (ORS) 285C.045, *et seq.*

With the aid of tax incentives from the Sponsors, the Company, either directly or through an affiliate, intends to make investments in the Zone by building and operating one or more new data centers located in the Zone.

[The Company has previously made significant investments in the Zone that have contributed to long-term investment and employment in the Zone.] [NTD – Confirm this is correct.]

The Company and the Sponsors desire to enter into this Agreement to encourage the Company to continue to make investments in the Zone and to streamline and simplify the reporting and administration of tax abatements made available to the Company.

The Company has timely and properly filed the Oregon Form OR-AP-CERT, Oregon Enterprise Zone Certificate Application – Long-Term Rural Tax Incentive, 150-310-073 ("Application") with respect to the Facility (as defined below).

The Company has executed a first-source hiring agreement, pursuant to ORS 285C.215, regarding the new permanent jobs to be created in Umatilla County upon completion and operation of the Facility.

NOW, THEREFORE, in recognition of the foregoing recitals, in consideration of the covenants contained herein, and in accordance with ORS 285C.400 through 285C.420, the Parties hereby agree as follows:

AGREEMENT

- I. **The Facility:** The "Facility" will consist of investments in data centers to be developed by the Company or its affiliates and located in the Zone, which will include the land, real property improvements, machinery and equipment, related personal property, and related support facilities and buildings, which may be installed, constructed, added, or placed in service in the Zone during the term of this Agreement. The Facility site is located in the Zone on Lot 1 as shown on the STAHL REPLAT - A REPLAT OF PARCEL 1 OF PARTITION PLAT 1997-06 & PARCEL 2 OF PARTITION PLAT 2019-08 (Document No. 2019-6940037, Office of Umatilla County Records) located in the Southeast Quarter of Section 23 and the Southeast Quarter of the Northeast Quarter and the South Half of Section 24, Township 4 North, Range 28, East of the Willamette Meridian, City of

Hermiston, Umatilla County, Oregon (consisting of approximately 204.89 acres) (the "Partition Plat").. No addresses are currently attached to this location. [NTD – Confirm this is correct.] Additional addresses may be attached to the Partition Plat for future data centers developed in the Zone during the term of this Agreement.

- II. **Exemption for the Facility:** The Sponsors jointly acknowledge and agree that, upon approval of the Company's Application, and upon certification by the Sponsors and the Umatilla County Assessor, and conditioned upon the satisfaction of other requirements under ORS 285C.400 through 285C.420 and this Agreement, the Facility will be exempt from ad valorem property tax on the Facility as provided in ORS 285C.409(1). The property tax exemption granted by this Agreement will to the maximum extent permitted by law exempt from all ad valorem property taxation the real property improvements, personal property improvements, and tangible and intangible personal property (excluding land, as set forth in ORS 285C.409(5)(a)) comprising or installed, constructed, added or otherwise placed at the Facility site, all as set forth in ORS 285C.409 and Oregon Administrative Rule (OAR) 123-690-0100(2) and 123-690-6200.
- III. **Term of Exemption:** Pursuant to ORS 285C.409(1), and conditioned upon the satisfaction of other requirements under ORS 285C.400 through 285C.420 and this Agreement, the Facility will be exempt from *ad valorem* property tax as follows:
 - A. For the first tax year following the calendar year in which the Company is certified or after which construction of the Facility commences, whichever event occurs later;
 - B. For each subsequent tax year in which the Facility is not yet in service as of the assessment date; and
 - C. For a period of 15 consecutive property tax years commencing as of the first tax year in which the Facility is in service as of the assessment date (the "Exemption Period"). As used in this Agreement, "in service" has the same meaning as "in service" as defined in OAR 123-690-0100(3).
 1. The Company shall notify the Umatilla County Assessor upon the Facility's receipt of the permit to occupy and use the Facility for its intended purpose within 90 days of such receipt.
- IV. **Scope of Exemption:** Pursuant to ORS 285C.409(1) and subject to Section II, the Company and its affiliates are entitled to exemption from ad valorem property tax with respect to all qualified property, whether leased or owned and whether tangible or intangible, of the Company and its affiliates used in connection with the development, construction, and operation of the Facility, including without limitation all subsequent Facility investments as described in OAR 123-690-6200.
- V. **Obligations of the Company:** The Company will comply with the following conditions as authorized under ORS 285C.403(3)(c):
 - A. **Statutory and Administrative Requirements:** The Company agrees to comply with the requirements of ORS 285C.409 and 285C.412([5]) as well as those provided in OAR Chapter 123, Division 690.

B. Additional Obligations: As established solely under this Agreement, the Parties agree to the additional obligations set forth below:

1. **Annual Improvement Payment ("AIP"):** For each year of the Exemption Period, the Company shall pay to the Sponsors an AIP for each data center within the Facility that is issued, and maintains, a permit to occupy and use such data center (a "Qualifying DC"). The AIP for each Qualifying DC is equal to \$5 per square foot thereof; provided, however, that the AIP for each of the first 4 Qualifying DCs that is at least 180,000 square feet is equal to a minimum of \$1 million regardless of their square footage; provided further, that the minimum total AIPs for each year of the Exemption Period will be \$2,000,000. The references to data center square footage in this paragraph refer to the gross square footage within the exterior walls of each data center building.
2. **Additional Annual Fee ("AAF"):** For each year of the Exemption Period, the Company shall pay to the Sponsors an AAF equal to the amount calculated by applying the tax code area ad valorem property tax rate to the AAF Base Amount set forth in Appendix 1.
3. **Public Safety Impact Fee ("PSIF"):** For each year of the Exemption Period, the Company shall pay to the Sponsors a PSIF in the amount set forth in Appendix 1.
4. **Student Success Fee ("SSF"):** For each year of the Exemption Period, the Company shall pay to the Sponsors an SSF in the amount set forth in Appendix 1.
5. **Community Development Contribution ("CDC"):** Within 60 days after the Company has commenced construction, within the meaning of ORS 285C.409(1)(a), of the first new Qualifying DC within the Facility, the Company shall pay to the Sponsors a CDC in the amount of \$5,000,000. The Parties acknowledge that the CDC is intended to assist with costs of capital improvement projects within the tax districts included in the consolidated tax code area in which the Facility is located, which may include payment or defeasance of debt service on, or redemption of, outstanding bonded indebtedness issued for such purposes under applicable law. Nothing in this Section V.B.5 shall modify, extend, waive, or otherwise affect the exemption provided under this Agreement, or constitute an obligation of the Company to support the payment of or satisfy any such bonded indebtedness, or compel the Sponsors to use the CDC for this purpose.
6. **Fee Payment Instructions:** The Company shall make the AIP, AAF, PSIF, SSF, and CDC payable to the "Greater Hermiston Enterprise Zone Board." The Company shall deposit the AIP, AAF, PSIF and SSF to the Umatilla County Assessor on or before November 15 of each year of the Exemption Period.
 - a. The mailing address for all payments is: Greater Hermiston Enterprise Zone Administrator, 235 E Gladys Ave., Hermiston, OR 97838.

7. **Application Processing Fee:** Within 14 days after the Sponsors and the Umatilla County Assessor have approved the Application with respect to the Facility pursuant to Section VI.B. of this Agreement, the Company shall pay to the Sponsors an application processing fee equal to \$120,000. Payment of the application processing fee shall be made to the Umatilla County Assessor, 216 SE 4th St., Pendleton, OR 97801.
8. **Payment Reduction for Charitable Payment:** The Company may make payments to or for the benefit of charities or non-profit entities that benefit the City of Hermiston, or residents of the City of Hermiston, and that are reasonably approved by the Sponsors (a "Charitable Payment"). If the Company makes a Charitable Payment, the Company may reduce its payments due under this Section V.B on a dollar-for-dollar basis in an amount equal to the Charitable Payment made during each year, up to a total reduction each year in the amount of \$150,000.
9. **Permit Fees:** The Company will pay all standard building permit and other permit fees related to the Facility. The Company further agrees that if it has a choice between obtaining an equivalent permit, license, or similar approval for the Facility from either a local governmental entity or a state entity, the Company will obtain the permit, license, or similar approval from the local governmental entity.
10. **Payments Generally:** With respect to the Facility, and to the fullest extent permitted by applicable law, the Company will not be required to pay sums to any local taxing authority in which the Facility is located other than the AIP, AAF, PSIF, SSF, and CDC due under this Section V.
11. **Future Bonding.**
 - a. During each year of the Exemption Period, the Company will contribute an amount to the Sponsors (the "Bond Contribution") to be used solely for the payment or prepayment of debt service on newly-issued general obligation bond indebtedness incurred to finance public utilities, public educational services, fire services, or public safety services and issued pursuant to voter approval of a measure passed on or after [September 1], 2022 by any taxing district included in the consolidated tax area in which the Facility is located (a "Qualifying Bond"). The annual Bond Contribution will be equal to 100% of the aggregate amount the Company would have been assessed with respect to such Qualifying Bonds for all property owned by the Company within the taxing district subject to the Qualifying Bond but for the exemption pursuant to this Agreement and any other exemption for property owned by the Company within such taxing district. The Company will not unreasonably object to any local measures for Qualifying Bonds.
 - b. Following voter approval of a Qualifying Bond and prior to the first due date of any related Bond Contribution, the Company, the Sponsors and the Umatilla County Assessor will collaborate to ensure that (a) Bond Contributions are expended solely to prepay or to make debt service payments on Qualifying Bonds, (b) tax levy

rates imposed on taxpayers are adjusted to take into account any Bond Contributions, and (c) Bond Contributions do not violate any tax-exemption requirements for Qualifying Bonds. The Parties may enter into additional agreements to implement these provisions.

VI. Obligations of Sponsors: Sponsors will comply with the following conditions:

- A. Resolutions:** Within 30 days of the Effective Date (as defined below), the governing body of each of Umatilla County and City of Hermiston will adopt resolutions approving and authorizing the execution of this Agreement. Such resolutions will serve to approve the Facility for a property tax exemption under ORS 285C.409 and constitute the resolutions described in ORS 285C.403(3)(a).
- B. Certification:** Within 5 days of satisfaction of the criteria set forth under ORS 285C.403(3), Sponsors and the Umatilla County Assessor will request confirmation from the Oregon Business Development Department of administrative sufficiency and confirmation of the County's conformity with ORS 285C.400(3) and the County's status as a "qualified rural county" within the meaning of ORS 285C.400 and 285C.412. Within 30 days of receipt of confirmation from the Oregon Business Development Department, Sponsors and the Umatilla County Assessor will approve and countersign the Application submitted by the Company, thereby certifying the Company as eligible for the property tax exemption contemplated by this Agreement.
- C. Tax Exemption:** Sponsors hereby set the period of the property tax exemption for the Facility for purposes of ORS 285C.409(1)(c) to be 15 consecutive tax years, beginning with the first tax year in which the Facility is in service as of the assessment date.
- D. Allocation of Payments:** Sponsors are solely responsible for the allocation, budgeting, division, and disposition of any payment due under this Agreement, including any portions that may be due or payable to any other jurisdiction. In no event shall the Company have any liability in connection with any disagreement, error, or conflict related to the division, allocation, or distribution of such amounts. In no event will the Company have any liability or obligation to any other person with respect to any payment due under this Agreement after the Company has discharged its duty to pay as set forth in Section V above, and Sponsors shall hold the Company harmless with respect to any claims to the contrary.
- E. Sponsors' Support:** Sponsors will support the Company in the Company's efforts to qualify for and obtain other tax incentives in connection with the Facility, including by promptly executing such letters or other documentation of support as may be reasonably requested by the Company, but Sponsors make no warranty with respect to its ability to affect any outcome in such regards.

VII. Termination Rights: Each Party may terminate this Agreement as follows:

- A. The Company's Termination Right:** The Company may elect to terminate this Agreement for any reason or no reason by delivery of written notice thereof to Sponsors. Upon delivery of any such notice, this Agreement will be of no further force or effect and no party will have any further rights or obligations hereunder, except for obligations owing and unpaid on the date of termination.

- B. Sponsors' Termination Right:** If the Company fails to make any payment required under this Agreement within 30 days of the due date of such payment, or fails to fulfill any other obligation of this Agreement within 30 days of written notice specifying the obligation which is in default, Sponsors may terminate the Agreement upon written notice to the Company. Such notice must provide an additional 60 days in which the Company may cure any such payment deficiency or default. Notwithstanding the foregoing, Sponsors may not terminate the Agreement if the Company's failure to pay is due to the error of any Sponsor.
- C. Mutual Termination Right:** The Parties may terminate this Agreement at any time upon mutual written agreement of termination.

VIII. Confidentiality:

- A.** The Parties acknowledge that this Agreement is a public record subject to Oregon's public records laws. The Sponsors agree that their staff, representatives and agents will exercise the utmost discretion in oral and written communications regarding the Facility and will provide information internally only to those individuals who need the information to facilitate the Parties' performance under this Agreement. If any person or entity requests any data, documents, or notes about the Company or its business practices (other than this Agreement) that are related to this Agreement or its tax exemption ("Confidential Information"), whether by court order, subpoena, Oregon Public Records Request, or other reason, the Sponsors shall not release any such information except as consistent with the following provisions:
1. The Sponsors shall notify the Company within 3 business days of the receipt of such request;
 2. if the Company elects to challenge or appeal the release of such information, the Company shall notify the Sponsors within 9 business days of receipt of the request, and the Company shall assume all responsibilities, costs, and expenses for such defense; and
 3. if the Company does not notify the Sponsors within 9 business days of receipt of such request, the Sponsors shall be authorized to release the requested information to the requester to the extent required by law, and the Sponsors shall have no liability to the Company for such release of such Confidential Information.

Notwithstanding the above, the Sponsors agree to cooperate with the Company in any challenge or appeal to a court order, subpoena, public records request, or other applicable law requiring the release of Confidential Information. The Company shall indemnify and hold the Sponsors harmless for all costs and expenses incurred in the challenge or appeal to the release of such Confidential Information, including court and appeal costs and the Sponsors' attorney's fees and expenses. Nothing in this section requires the Parties to refuse to disclose such Confidential Information after a final order, including any appeal, by a competent judicial authority. If Oregon law is amended to require responses to public records requests be made less than 12 business days from the request, the number of business days specified in Paragraphs (1) and (2) of this Section VIII.A shall be reduced to 2 business days, and the number of business days to respond

to a public records request as mandated by Oregon law less 2 business days, respectively.

IX. Miscellaneous Provisions:

- A. **Effective Date:** This Agreement becomes effective on the date at which this Agreement has been signed by all of the Parties.
- B. **Assignment:** None of the Parties may assign this Agreement without the prior written consent of the other Parties, except that the Company may assign this Agreement to any of its affiliates, or to a new entity without the prior written consent of the other Parties. Subject to the foregoing, this Agreement will be binding upon, inure to the benefit of, and be enforceable by the Parties and their respective successors and assigns. Nothing in this Agreement shall be deemed to constitute a restriction or condition on the ability of the Facility owner to freely sell, transfer or assign ownership of the improvements or the underlying land or any portion thereof or any ownership interest in the Facility owner at any time.
- C. **Amendment:** This Agreement may be amended only by a written document signed by the Party against whom enforcement is sought.
- D. **Waiver:** No waiver will be binding on a Party unless it is in writing and signed by the Party making the waiver. A Party's waiver of a breach of a provision of this Agreement will not be a waiver of any other provision or a waiver of a subsequent breach of the same provision.
- E. **Notices:** All notices and communications relating to this Agreement shall be in writing and shall be personally delivered or sent by registered or certified mail, return receipt requested, or delivered by nationally recognized express courier service. Such notices and other communications shall be effective upon receipt if hand delivered, 3 business days after mailing if sent by mail, and 1 business day after dispatch if sent by express courier, to the following addresses:

| | |
|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| <u>If to the Company:</u> Amazon.com, Inc. 410 Terry Ave. N Seattle, WA 98109 Attn: Infrastructure Global Expansion, Economic Development <u>With copies to:</u> Amazon.com, Inc. Attn: General Counsel P.O. Box 81226 Seattle, WA 98108 | <u>If to Sponsors:</u> Greater Hermiston Enterprise Zone Administrator 235 E Gladys Ave. Hermiston, OR 97838 <u>With copies to:</u> Umatilla County Assessor 216 SE 4 th St. Pendleton, OR 97801 |
|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|

A copy of all notices and communications relating to this Agreement shall also be sent, on or before the date in which they become effective, by email to:

| | |
|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|------------------------|
| <u>If to the Company:</u> Aws-econ-dev@amazon.com & contracts-legal@amazon.com | <u>If to Sponsors:</u> |
|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|------------------------|

- F. Limitation of Liability:** Notwithstanding anything to the contrary in this Agreement, neither the Company, nor any of its affiliates, shall be liable for: (a) any indirect, reliance, exemplary, incidental, speculative, punitive, special, consequential or similar damages that may arise in connection with this Agreement, (b) any lost or foregone tax revenues, or (c) any damages, liabilities, fees, costs, expenses, penalties, diminishments in value, losses or payments (including any lost or foregone tax revenues) that exceed, in the aggregate, the lesser of (i) the financial benefit realized by the Company under this Agreement and (ii) \$3,000,000.
- G. Force Majeure:** A Party will not be deemed to be in breach, material breach, default or otherwise in violation of any term of this Agreement to the extent such Party's action, inaction or omission is the result of a Force Majeure Event. The Company and Sponsors agree to use commercially reasonable efforts to promptly resolve any Force Majeure Event that adversely and materially impacts their performance under this Agreement. A Force Majeure Event pauses a Party's performance obligation for the duration of the event, but does not excuse it. "Force Majeure Event" means any event or occurrence that is not within the control of such Party or its affiliates and prevents a Party from performing its obligations under this Agreement, including without limitation, any act of God; act of a public enemy; war; riot; sabotage; blockage; embargo; failure or inability to secure materials, supplies or labor through ordinary sources by reason of shortages or priority; labor strike, lockout or other labor or industrial disturbance (whether or not on the part of agents or employees of any Party); civil disturbance; terrorist act; power outage; fire, flood, windstorm, hurricane, earthquake or other casualty; any law, order, regulation or other action of any governing authority; any action, inaction, order, ruling moratorium, regulation, statute, condition or other decision of any governmental agency having jurisdiction over a Party hereto, over the Facility or over a Party's operations.
- H. Severability:** The Parties agree that if any term or provision of this Agreement is declared by a court of competent jurisdiction to be illegal or in conflict with any law, the validity of the remaining terms and provision shall not be affected, and the rights and obligations of the Parties shall be construed and enforced as if the Agreement did not contain the particular term or provision held to be invalid. It is the intent of the Parties that, in the event a clause or provision is stricken, that there be added as part of this Agreement a clause or provision as similar in terms as may be possible, legal, and enforceable so as to provide a comparable property tax exemption and comparable payments as provided for in this Agreement.
- I. Governing Law:** This Agreement is governed by the laws of the State of Oregon, without giving effect to any conflict-of-law principle that would result in the laws of any other jurisdiction governing this Agreement.

- J. Venue:** Any action, suit, or proceeding arising out of the subject matter of this Agreement will be litigated in courts located in Umatilla County, Oregon. Each Party consents and submits to the jurisdiction of any local, state, or federal court located in Umatilla County, Oregon.
- K. Attorney's Fees:** If any arbitration, action, suit, or proceeding is instituted to interpret, enforce, or rescind this Agreement, or otherwise in connection with the subject matter of this Agreement, including but not limited to any proceeding brought under the United States Bankruptcy Code, the prevailing party on a claim will be entitled to recover with respect to the claim, in addition to any other relief awarded, the prevailing party's reasonable attorney's fees and other fees, costs, and expenses of every kind, including but not limited to the costs and disbursements specified in ORCP 68 A(2), incurred in connection with the arbitration, action, suit, or proceeding, any appeal or petition for review, the collection of any award, or the enforcement of any order, as determined by the arbitrator or court. Notwithstanding the foregoing, each Party's maximum liability under this Section IX(K) is \$75,000 and in no event will any Party be obligated by this Section IX(K) to pay an amount in excess of \$75,000.
- L. Entire Agreement:** This Agreement contains the entire understanding of the Parties regarding the subject matter of this Agreement and supersedes all prior and contemporaneous negotiations and agreements, whether written or oral, between the Parties with respect to the subject matter of this Agreement.
- M. Signatures:** This Agreement may be signed in one or more counterparts, each of which is an original, and all of which together constitute only one agreement between the Parties. A signature page delivered by fax or electronic mail will be considered an original signature page. At the request of a Party, the other Parties will confirm delivery of a signature page (whether by fax or electronic mail) by delivering an original signature page to the requesting Party.
- N. Waiver of Jury Trial:** WITH RESPECT TO ANY PROCEEDING OR ACTION ARISING OUT OF OR IN ANY WAY RELATING TO THIS AGREEMENT (WHETHER IN CONTRACT, TORT, EQUITY OR OTHERWISE), THE PARTIES KNOWINGLY, INTENTIONALLY, IRREVOCABLY AND UNCONDITIONALLY WAIVE THEIR RIGHT TO TRIAL BY JURY. EACH PARTY CERTIFIES AND ACKNOWLEDGES THAT IT HAS CONSIDERED THE IMPLICATIONS OF THIS WAIVER, IT MAKES THIS WAIVER KNOWINGLY AND VOLUNTARILY, AND IT HAS DECIDED TO ENTER INTO THIS AGREEMENT IN CONSIDERATION OF, AMONG OTHER THINGS, THE MUTUAL WAIVERS AND CERTIFICATIONS IN THIS SECTION.

IN WITNESS WHEREOF, the Parties, by their respective duly authorized representatives, have executed this Agreement on the date shown below.

UMATILLA COUNTY

Dated this [] day of [], 2022

DRAFT

Dan Dorran, Commissioner

DRAFT

George L. Murdock, Commissioner

DRAFT

John M. Shafer, Commissioner

CITY OF HERMISTON

Dated this [] day of [], 2022

DRAFT

Dr. David A. Drotzmann, Mayor

Attest:

DRAFT

Mark I. Morgan, Greater Hermiston Enterprise Zone
Administrator

Amazon Data Services, Inc.

Dated this _____ day of [], 2022

DRAFT

Appendix 1

| Tax Year | AAF Base Amount* | PSIF Payment Amount | SSF Payment Amount |
|-----------------|-------------------------|----------------------------|---------------------------|
| 2023 | 27,318,175 | 54,636 | 54,636 |
| 2024 | 28,137,720 | 56,275 | 56,275 |
| 2025 | 28,981,852 | 57,964 | 57,964 |
| 2026 | 29,851,307 | 59,703 | 59,703 |
| 2027 | 30,746,847 | 61,494 | 61,494 |
| 2028 | 31,669,252 | 63,339 | 63,339 |
| 2029 | 32,619,330 | 65,239 | 65,239 |
| 2030 | 33,597,909 | 67,196 | 67,196 |
| 2031 | 34,605,847 | 69,212 | 69,212 |
| 2032 | 35,644,022 | 71,288 | 71,288 |
| 2033 | 36,713,343 | 73,427 | 73,427 |
| 2034 | 37,814,743 | 75,629 | 75,629 |
| 2035 | 38,949,185 | 77,898 | 77,898 |
| 2036 | 40,117,661 | 80,235 | 80,235 |
| 2037 | 41,321,191 | 82,642 | 82,642 |
| 2038 | 42,560,827 | 85,122 | 85,122 |
| 2039 | 43,837,651 | 87,675 | 87,675 |
| 2040 | 45,152,781 | 90,306 | 90,306 |
| 2041 | 46,507,364 | 93,015 | 93,015 |
| 2042 | 47,902,585 | 95,805 | 95,805 |
| 2043 | 49,339,663 | 98,679 | 98,679 |
| 2044 | 50,819,853 | 101,640 | 101,640 |
| 2045 | 52,344,448 | 104,689 | 104,689 |
| 2046 | 53,914,782 | 107,830 | 107,830 |
| 2047 | 55,532,225 | 111,064 | 111,064 |
| 2048 | 57,198,192 | 114,396 | 114,396 |
| 2049 | 58,914,138 | 117,828 | 117,828 |
| 2050 | 60,681,562 | 121,363 | 121,363 |
| 2051 | 62,502,009 | 125,004 | 125,004 |
| 2052 | 64,377,069 | 128,754 | 128,754 |
| 2053 | 66,308,381 | 132,617 | 132,617 |
| 2054 | 68,297,632 | 136,595 | 136,595 |
| 2055 | 70,346,561 | 140,693 | 140,693 |
| 2056 | 72,456,958 | 144,914 | 144,914 |
| 2057 | 74,630,667 | 149,261 | 149,261 |

* Amount of payment will be calculated by applying the applicable tax code area ad valorem property tax rate to the AAF Base Amount.

RESOLUTION NO. 2239**A RESOLUTION APPROVING THE EXECUTION OF A LONG-TERM RURAL ENTERPRISE ZONE ABATEMENT AGREEMENT WITH AMAZON DATA SERVICES, INC.**

WHEREAS, the Board of County Commissioners of Umatilla County and the City Council of the City of Hermiston are equal co-sponsors of the Greater Hermiston Enterprise Zone; and

WHEREAS, Amazon Data Services, Inc. has requested a Long-term Rural Enterprise Zone Abatement pursuant to ORS 285C.409(1); and

WHEREAS, the proposed facility site is located in the Greater Hermiston Enterprise Zone on Lot 1 as shown on the STAHL REPLAT - A REPLAT OF PARCEL 1 OF PARTITION PLAT 1997-06 & PARCEL 2 OF PARTITION PLAT 2019-08 (Document No. 2019-6940037, Office of Umatilla County Records) located in the Southeast Quarter of Section 23 and the Southeast Quarter of the Northeast Quarter and the South Half of Section 24, Township 4 North, Range 28, East of the Willamette Meridian, City of Hermiston, Umatilla County, Oregon (consisting of approximately 204.89 acres) (the "Partition Plat"); and

WHEREAS, Amazon Data Services, Inc. agrees to adhere to all requirements of an agreement for Long-Term Rural Enterprise Zone Abatement with Umatilla County and the City of Hermiston as co-sponsors of the Greater Hermiston Enterprise Zone, as well as any applicable laws of the State of Oregon relating to Long-Term Rural Enterprise Zone Abatement; and

WHEREAS, the Agreement for Long-Term Rural Enterprise Zone Abatement requires Amazon Data Services, for the term of the agreement, to invest more than \$200 million, hire at least 10 full-time employees, provide employee compensation greater than 130 percent of the Umatilla County average wage, and pay the required payments and fees to the zone over the term of the agreement.

NOW, THEREFORE, THE CITY OF HERMISTON RESOLVES AS FOLLOWS:

1. That the form, terms, and provisions of the draft of the Long-Term Rural Enterprise Zone Abatement Agreement with Amazon Data Services, entered by Amazon Data Services, and the City of Hermiston, and the transactions contemplated thereby, providing for, among other things, establishing Amazon Data Services' investment in a new data center facility and related employment, and the Long-Term Rural Enterprise Zone Abatement pursuant to Oregon Revised Statute, copies of which have been presented to and reviewed by the Common Council be, and they are, in all respects, hereby approved and adopted; and

2. That the City Manager of the City of Hermiston is hereby authorized to execute and deliver the Long-Term Rural Enterprise Zone Abatement Agreement with Amazon Data Services, Inc., substantially in the form heretofore approved and adopted with such changes, additions, deletions and modifications as such executing officer or officers may approve, such execution to be conclusive evidence of such approval and of the authorization thereof by the Common Council; and
3. That City Manager of the City of Hermiston is hereby authorized and directed to prepare, execute, deliver, acknowledge and file such additional documents, agreements, certificates, forms, receipts and other instruments, in the name of and on behalf of the City of Hermiston and under its corporate seal, if so desired, and to take all such other actions as such officer or officers shall, in his or her or their sole discretion, approve in order to carry out the transactions heretofore approved and perform and discharge the City of Hermiston's obligations under or in connection with the Long-Term Rural Enterprise Zone Abatement Agreement with Amazon Data Services, Inc., such actions and execution to be conclusive evidence of such approval and of the authorization thereof by the Common Council.
4. That this resolution is effective immediately upon its passage.

PASSED by the Common Council this 26th day of September, 2022.
SIGNED by the Mayor this 26th day of September, 2022.

Dr. David Drotzmann, MAYOR

ATTEST:

Lilly Alarcon-Strong, CMC, CITY RECORDER



Where Life is Sweet

Mayor and Members of the City Council

STAFF REPORT

For the Meeting of **September 26, 2022**

Title/Subject

Resolution 2240- Long Term Rural Enterprise Zone Agreement

Summary and Background

Amazon Data Services has submitted an application for a Long-Term Rural Enterprise Zone (LTREZ) abatement for two planned new campuses within the Greater Hermiston Enterprise Zone (GHEZ). Both applications meet the minimum criteria to qualify for an LTREZ abatement which requires at least \$200 million in new investment, creation of at least 10 new permanent jobs, and average compensation that is at least 130% of the average wage in Umatilla County.

The terms of the agreement are very similar to the existing agreement that is in place with ADS within the GHEZ. As part of this agreement, ADS will pay to the zone:

1. Annual Improvement Payments (AIP) equal to \$5 per square foot of data center construction. The AIP shall not be less than \$2 million per year, and is expected to reach approximately \$4 million per year at full build-out.
2. Additional Annual Fee (AAF) equal to what the property tax would be on a valuation of \$27,318,175 beginning in 2023. That assumed valuation will index up annually to offset inflation.
3. Public Safety Impact Fee (PSIF) of \$54,636 beginning in 2023. The PSIF will index up annually to offset inflation.
4. Student Success Fee (SSF) of \$54,636 beginning in 2023. The SSF will index up annually to offset inflation.

Additional stipulations within these agreements, which were not included in the previous agreement are:

1. ADS shall, within 60 days of beginning construction, pay \$5 million to the GHEZ as a Community Development Contribution.
2. ADS shall, within 14 days of execution of the agreement, pay an application processing fee of \$120,000.

3. The value of the facilities shall be made available for taxation on bonds issued financing public utilities, public educational services, fire services, or other public safety services. ADS will then make a contribution toward the bond payments equal to what it's tax obligation would have been. This will have the effect of making bond obligations more affordable for other taxpayers in the community.

At full build-out, amortizing the Community Development Contribution over the 15 year life of the agreement, it is projected that total annual fees paid to the GHEZ per campus will be approximately \$4,975,000. This would make each campus the single largest financial contributor to local government services by far. Each one would pay nearly 7 times more than the current highest property tax payer within City Limits.

Additionally, the amount paid for local government services through this agreement is even more disproportionately high when considering the relatively low impact on local government services. Although each site is anticipated to employ 120 permanent full time employees, that is significantly lower than some of our other large industrial employers; who's employees often require educational services for their kids, make calls for service to emergency response agencies, etc.

Here is a table showing the amount of either fees or taxes paid to local governments by the top commercial/industrial employers in the Hermiston area. You can see that each site will be paying more than 10 times more per employee than the next highest per-employee payer.

| Top Commercial/Industrial Taxpayers in Hermiston Area 2020 | | | |
|-------------------------------------------------------------------|-------------|---------------------|-----------------------------|
| | Jobs | Tax/Fee Paid | Tax/Fee Per Employee |
| AWS (Proposed PDX245 LTREZ)* | 120 | \$ 4,975,606 | \$ 41,463.38 |
| AWS (Proposed PDX146 LTREZ)* | 120 | \$ 4,975,606 | \$ 41,463.38 |
| Pioneer Seed | 227 | \$ 728,832 | \$ 3,210.71 |
| Columbia Basin Bioscience | 60 | \$ 157,217 | \$ 2,620.28 |
| Lamb Weston- Hermiston** | 625 | \$ 1,211,176 | \$ 1,937.88 |
| Wal-Mart Store | 356 | \$ 415,331 | \$ 1,166.66 |
| Marlette Homes | 250 | \$ 170,616 | \$ 682.46 |
| Wal-Mart DC | 1050 | \$ 632,895 | \$ 602.76 |
| Shearer's | 400 | \$ 157,313 | \$ 393.28 |
| Good Shepherd | 767 | \$ 138,386 | \$ 180.43 |
| Two Rivers Correctional Institution | 440 | \$ - | \$ - |
| *Scheduled at Full Build-Out | | | |
| **Includes 2017 LTREZ | | | |

Tie-In to Council Goals

Fiscal Prudence.

Fiscal Information

See above. It is important to note that the figures outlined here for each of the various fee requirements apply to both agreements individually. I am simply covering them together here for expedience.

Alternatives and Recommendation

Alternatives

1. Approve Resolution 2240
2. Reject Resolution 2240
3. Direct Staff to request changes

It is important to keep in mind that the City of Hermiston is co-sponsors with Umatilla County on the GHEZ. Therefore, any recommended changes must be agreed to by Umatilla County.

Recommended Action/Motion

Motion to approve Resolution 2240.

Submitted By:

Mark Morgan

AGREEMENT FOR LONG-TERM RURAL ENTERPRISE ZONE TAX ABATEMENT

This agreement for Long-Term Rural Enterprise Zone Tax Abatement ("Agreement") is executed by GREATER HERMISTON ENTERPRISE ZONE BOARD, representing the sponsor entities UMATILLA COUNTY, OREGON ("Umatilla County"), the CITY OF HERMISTON, OREGON ("City of Hermiston"), and Amazon Data Services, Inc. (the "Company"). Umatilla County and City of Hermiston are referenced collectively in this Agreement as "Sponsors." In this Agreement, each of the Sponsors and the Company are sometimes individually referenced as a "Party" and collectively referenced as the "Parties."

RECITALS

The Sponsors have established the Greater Hermiston Enterprise Zone (the "Zone") pursuant to Oregon Revised Statute (ORS) 285C.045, *et seq.*

With the aid of tax incentives from the Sponsors, the Company, either directly or through an affiliate, intends to make investments in the Zone by building and operating one or more new data centers located in the Zone.

[The Company has previously made significant investments in the Zone that have contributed to long-term investment and employment in the Zone.] [NTD – Confirm this is correct.]

The Company and the Sponsors desire to enter into this Agreement to encourage the Company to continue to make investments in the Zone and to streamline and simplify the reporting and administration of tax abatements made available to the Company.

The Company has timely and properly filed the Oregon Form OR-AP-CERT, Oregon Enterprise Zone Certificate Application – Long-Term Rural Tax Incentive, 150-310-073 ("Application") with respect to the Facility (as defined below).

The Company has executed a first-source hiring agreement, pursuant to ORS 285C.215, regarding the new permanent jobs to be created in Umatilla County upon completion and operation of the Facility.

NOW, THEREFORE, in recognition of the foregoing recitals, in consideration of the covenants contained herein, and in accordance with ORS 285C.400 through 285C.420, the Parties hereby agree as follows:

AGREEMENT

- I. **The Facility:** The "Facility" will consist of investments in data centers to be developed by the Company or its affiliates and located in the Zone, which will include the land, real property improvements, machinery and equipment, related personal property, and related support facilities and buildings, which may be installed, constructed, added, or placed in service in the Zone during the term of this Agreement. The Facility site is located in the Zone on **Parcels 1 and 2** [NTD – Confirm this is correct.] of UMATILLA COUNTY PARTITION PLAT NO. 2018-23 (Instrument No. 2018-6800729, Office of Umatilla County Records) located in the Northeast Quarter of Section 27, Township 4 North, Range 28, East of the Willamette Meridian, Umatilla County, Oregon (consisting of approximately 9.97 acres with respect to Parcel 1 and 120.83 acres with respect to Parcel 2) (the

"Partition Plat").. No addresses are currently attached to this location. [NTD – Confirm this is correct.] Additional addresses may be attached to the Partition Plat for future data centers developed in the Zone during the term of this Agreement.

- II. Exemption for the Facility:** The Sponsors jointly acknowledge and agree that, upon approval of the Company's Application, and upon certification by the Sponsors and the Umatilla County Assessor, and conditioned upon the satisfaction of other requirements under ORS 285C.400 through 285C.420 and this Agreement, the Facility will be exempt from ad valorem property tax on the Facility as provided in ORS 285C.409(1). The property tax exemption granted by this Agreement will to the maximum extent permitted by law exempt from all ad valorem property taxation the real property improvements, personal property improvements, and tangible and intangible personal property (excluding land, as set forth in ORS 285C.409(5)(a)) comprising or installed, constructed, added or otherwise placed at the Facility site, all as set forth in ORS 285C.409 and Oregon Administrative Rule (OAR) 123-690-0100(2) and 123-690-6200.
- III. Term of Exemption:** Pursuant to ORS 285C.409(1), and conditioned upon the satisfaction of other requirements under ORS 285C.400 through 285C.420 and this Agreement, the Facility will be exempt from *ad valorem* property tax as follows:
- A.** For the first tax year following the calendar year in which the Company is certified or after which construction of the Facility commences, whichever event occurs later;
 - B.** For each subsequent tax year in which the Facility is not yet in service as of the assessment date; and
 - C.** For a period of 15 consecutive property tax years commencing as of the first tax year in which the Facility is in service as of the assessment date (the "Exemption Period"). As used in this Agreement, "in service" has the same meaning as "in service" as defined in OAR 123-690-0100(3).
 - 1.** The Company shall notify the Umatilla County Assessor upon the Facility's receipt of the permit to occupy and use the Facility for its intended purpose within 90 days of such receipt.
- IV. Scope of Exemption:** Pursuant to ORS 285C.409(1) and subject to Section II, the Company and its affiliates are entitled to exemption from ad valorem property tax with respect to all qualified property, whether leased or owned and whether tangible or intangible, of the Company and its affiliates used in connection with the development, construction, and operation of the Facility, including without limitation all subsequent Facility investments as described in OAR 123-690-6200.
- V. Obligations of the Company:** The Company will comply with the following conditions as authorized under ORS 285C.403(3)(c):
- A. Statutory and Administrative Requirements:** The Company agrees to comply with the requirements of ORS 285C.409 and 285C.412([5]) as well as those provided in OAR Chapter 123, Division 690.
 - B. Additional Obligations:** As established solely under this Agreement, the Parties agree to the additional obligations set forth below:

1. **Annual Improvement Payment ("AIP"):** For each year of the Exemption Period, the Company shall pay to the Sponsors an AIP for each data center within the Facility that is issued, and maintains, a permit to occupy and use such data center (a "Qualifying DC"). The AIP for each Qualifying DC is equal to \$5 per square foot thereof; provided, however, that the AIP for each of the first 4 Qualifying DCs that is at least 180,000 square feet is equal to a minimum of \$1 million regardless of their square footage; provided further, that the minimum total AIPs for each year of the Exemption Period will be \$2,000,000. The references to data center square footage in this paragraph refer to the gross square footage within the exterior walls of each data center building.
2. **Additional Annual Fee ("AAF"):** For each year of the Exemption Period, the Company shall pay to the Sponsors an AAF equal to the amount calculated by applying the tax code area ad valorem property tax rate to the AAF Base Amount set forth in Appendix 1.
3. **Public Safety Impact Fee ("PSIF"):** For each year of the Exemption Period, the Company shall pay to the Sponsors a PSIF in the amount set forth in Appendix 1.
4. **Student Success Fee ("SSF"):** For each year of the Exemption Period, the Company shall pay to the Sponsors an SSF in the amount set forth in Appendix 1.
5. **Community Development Contribution ("CDC"):** Within 60 days after the Company has commenced construction, within the meaning of ORS 285C.409(1)(a), of the first new Qualifying DC within the Facility, the Company shall pay to the Sponsors a CDC in the amount of \$5,000,000. The Parties acknowledge that the CDC is intended to assist with costs of capital improvement projects within the tax districts included in the consolidated tax code area in which the Facility is located, which may include payment or defeasance of debt service on, or redemption of, outstanding bonded indebtedness issued for such purposes under applicable law. Nothing in this Section V.B.5 shall modify, extend, waive, or otherwise affect the exemption provided under this Agreement, or constitute an obligation of the Company to support the payment of or satisfy any such bonded indebtedness, or compel the Sponsors to use the CDC for this purpose.
6. **Fee Payment Instructions:** The Company shall make the AIP, AAF, PSIF, SSF, and CDC payable to the "Greater Hermiston Enterprise Zone Board." The Company shall deposit the AIP, AAF, PSIF and SSF to the Umatilla County Assessor on or before November 15 of each year of the Exemption Period.
 - a. The mailing address for all payments is: Greater Hermiston Enterprise Zone Administrator, 235 E Gladys Ave., Hermiston, OR 97838.
7. **Application Processing Fee:** Within 14 days after the Sponsors and the Umatilla County Assessor have approved the Application with respect to

the Facility pursuant to Section VI.B. of this Agreement, the Company shall pay to the Sponsors an application processing fee equal to \$120,000. Payment of the application processing fee shall be made to the Umatilla County Assessor, 216 SE 4th St., Pendleton, OR 97801.

8. **Payment Reduction for Charitable Payment:** The Company may make payments to or for the benefit of charities or non-profit entities that benefit the City of Hermiston, or residents of the City of Hermiston, and that are reasonably approved by the Sponsors (a "Charitable Payment"). If the Company makes a Charitable Payment, the Company may reduce its payments due under this Section V.B on a dollar-for-dollar basis in an amount equal to the Charitable Payment made during each year, up to a total reduction each year in the amount of \$150,000.
9. **Permit Fees:** The Company will pay all standard building permit and other permit fees related to the Facility. The Company further agrees that if it has a choice between obtaining an equivalent permit, license, or similar approval for the Facility from either a local governmental entity or a state entity, the Company will obtain the permit, license, or similar approval from the local governmental entity.
10. **Payments Generally:** With respect to the Facility, and to the fullest extent permitted by applicable law, the Company will not be required to pay sums to any local taxing authority in which the Facility is located other than the AIP, AAF, PSIF, SSF, and CDC due under this Section V.
11. **Future Bonding.**
 - a. During each year of the Exemption Period, the Company will contribute an amount to the Sponsors (the "Bond Contribution") to be used solely for the payment or prepayment of debt service on newly-issued general obligation bond indebtedness incurred to finance public utilities, public educational services, fire services, or public safety services and issued pursuant to voter approval of a measure passed on or after [September 1], 2022 by any taxing district included in the consolidated tax area in which the Facility is located (a "Qualifying Bond"). The annual Bond Contribution will be equal to 100% of the aggregate amount the Company would have been assessed with respect to such Qualifying Bonds for all property owned by the Company within the taxing district subject to the Qualifying Bond but for the exemption pursuant to this Agreement and any other exemption for property owned by the Company within such taxing district. The Company will not unreasonably object to any local measures for Qualifying Bonds.
 - b. Following voter approval of a Qualifying Bond and prior to the first due date of any related Bond Contribution, the Company, the Sponsors and the Umatilla County Assessor will collaborate to ensure that (a) Bond Contributions are expended solely to prepay or to make debt service payments on Qualifying Bonds, (b) tax levy rates imposed on taxpayers are adjusted to take into account any Bond Contributions, and (c) Bond Contributions do not violate any

tax-exemption requirements for Qualifying Bonds. The Parties may enter into additional agreements to implement these provisions.

VI. Obligations of Sponsors: Sponsors will comply with the following conditions:

- A. Resolutions:** Within 30 days of the Effective Date (as defined below), the governing body of each of Umatilla County and City of Hermiston will adopt resolutions approving and authorizing the execution of this Agreement. Such resolutions will serve to approve the Facility for a property tax exemption under ORS 285C.409 and constitute the resolutions described in ORS 285C.403(3)(a).
- B. Certification:** Within 5 days of satisfaction of the criteria set forth under ORS 285C.403(3), Sponsors and the Umatilla County Assessor will request confirmation from the Oregon Business Development Department of administrative sufficiency and confirmation of the County's conformity with ORS 285C.400(3) and the County's status as a "qualified rural county" within the meaning of ORS 285C.400 and 285C.412. Within 30 days of receipt of confirmation from the Oregon Business Development Department, Sponsors and the Umatilla County Assessor will approve and countersign the Application submitted by the Company, thereby certifying the Company as eligible for the property tax exemption contemplated by this Agreement.
- C. Tax Exemption:** Sponsors hereby set the period of the property tax exemption for the Facility for purposes of ORS 285C.409(1)(c) to be 15 consecutive tax years, beginning with the first tax year in which the Facility is in service as of the assessment date.
- D. Allocation of Payments:** Sponsors are solely responsible for the allocation, budgeting, division, and disposition of any payment due under this Agreement, including any portions that may be due or payable to any other jurisdiction. In no event shall the Company have any liability in connection with any disagreement, error, or conflict related to the division, allocation, or distribution of such amounts. In no event will the Company have any liability or obligation to any other person with respect to any payment due under this Agreement after the Company has discharged its duty to pay as set forth in Section V above, and Sponsors shall hold the Company harmless with respect to any claims to the contrary.
- E. Sponsors' Support:** Sponsors will support the Company in the Company's efforts to qualify for and obtain other tax incentives in connection with the Facility, including by promptly executing such letters or other documentation of support as may be reasonably requested by the Company, but Sponsors make no warranty with respect to its ability to affect any outcome in such regards.

VII. Termination Rights: Each Party may terminate this Agreement as follows:

- A. The Company's Termination Right:** The Company may elect to terminate this Agreement for any reason or no reason by delivery of written notice thereof to Sponsors. Upon delivery of any such notice, this Agreement will be of no further force or effect and no party will have any further rights or obligations hereunder, except for obligations owing and unpaid on the date of termination.

- B. Sponsors' Termination Right:** If the Company fails to make any payment required under this Agreement within 30 days of the due date of such payment, or fails to fulfill any other obligation of this Agreement within 30 days of written notice specifying the obligation which is in default, Sponsors may terminate the Agreement upon written notice to the Company. Such notice must provide an additional 60 days in which the Company may cure any such payment deficiency or default. Notwithstanding the foregoing, Sponsors may not terminate the Agreement if the Company's failure to pay is due to the error of any Sponsor.
- C. Mutual Termination Right:** The Parties may terminate this Agreement at any time upon mutual written agreement of termination.

VIII. Confidentiality:

- A.** The Parties acknowledge that this Agreement is a public record subject to Oregon's public records laws. The Sponsors agree that their staff, representatives and agents will exercise the utmost discretion in oral and written communications regarding the Facility and will provide information internally only to those individuals who need the information to facilitate the Parties' performance under this Agreement. If any person or entity requests any data, documents, or notes about the Company or its business practices (other than this Agreement) that are related to this Agreement or its tax exemption ("Confidential Information"), whether by court order, subpoena, Oregon Public Records Request, or other reason, the Sponsors shall not release any such information except as consistent with the following provisions:
1. The Sponsors shall notify the Company within 3 business days of the receipt of such request;
 2. if the Company elects to challenge or appeal the release of such information, the Company shall notify the Sponsors within 9 business days of receipt of the request, and the Company shall assume all responsibilities, costs, and expenses for such defense; and
 3. if the Company does not notify the Sponsors within 9 business days of receipt of such request, the Sponsors shall be authorized to release the requested information to the requester to the extent required by law, and the Sponsors shall have no liability to the Company for such release of such Confidential Information.

Notwithstanding the above, the Sponsors agree to cooperate with the Company in any challenge or appeal to a court order, subpoena, public records request, or other applicable law requiring the release of Confidential Information. The Company shall indemnify and hold the Sponsors harmless for all costs and expenses incurred in the challenge or appeal to the release of such Confidential Information, including court and appeal costs and the Sponsors' attorney's fees and expenses. Nothing in this section requires the Parties to refuse to disclose such Confidential Information after a final order, including any appeal, by a competent judicial authority. If Oregon law is amended to require responses to public records requests be made less than 12 business days from the request, the number of business days specified in Paragraphs (1) and (2) of this Section VIII.A shall be reduced to 2 business days, and the number of business days to respond

to a public records request as mandated by Oregon law less 2 business days, respectively.

IX. Miscellaneous Provisions:

- A. **Effective Date:** This Agreement becomes effective on the date at which this Agreement has been signed by all of the Parties.
- B. **Assignment:** None of the Parties may assign this Agreement without the prior written consent of the other Parties, except that the Company may assign this Agreement to any of its affiliates, or to a new entity without the prior written consent of the other Parties. Subject to the foregoing, this Agreement will be binding upon, inure to the benefit of, and be enforceable by the Parties and their respective successors and assigns. Nothing in this Agreement shall be deemed to constitute a restriction or condition on the ability of the Facility owner to freely sell, transfer or assign ownership of the improvements or the underlying land or any portion thereof or any ownership interest in the Facility owner at any time.
- C. **Amendment:** This Agreement may be amended only by a written document signed by the Party against whom enforcement is sought.
- D. **Waiver:** No waiver will be binding on a Party unless it is in writing and signed by the Party making the waiver. A Party's waiver of a breach of a provision of this Agreement will not be a waiver of any other provision or a waiver of a subsequent breach of the same provision.
- E. **Notices:** All notices and communications relating to this Agreement shall be in writing and shall be personally delivered or sent by registered or certified mail, return receipt requested, or delivered by nationally recognized express courier service. Such notices and other communications shall be effective upon receipt if hand delivered, 3 business days after mailing if sent by mail, and 1 business day after dispatch if sent by express courier, to the following addresses:

| | |
|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| <u>If to the Company:</u> Amazon.com, Inc. 410 Terry Ave. N Seattle, WA 98109 Attn: Infrastructure Global Expansion, Economic Development <u>With copies to:</u> Amazon.com, Inc. Attn: General Counsel P.O. Box 81226 Seattle, WA 98108 | <u>If to Sponsors:</u> Greater Hermiston Enterprise Zone Administrator 235 E Gladys Ave. Hermiston, OR 97838 <u>With copies to:</u> Umatilla County Assessor 216 SE 4 th St. Pendleton, OR 97801 |
|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|

A copy of all notices and communications relating to this Agreement shall also be sent, on or before the date in which they become effective, by email to:

| | |
|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|------------------------|
| <u>If to the Company:</u> Aws-econ-dev@amazon.com & contracts-legal@amazon.com | <u>If to Sponsors:</u> |
|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|------------------------|

- F. Limitation of Liability:** Notwithstanding anything to the contrary in this Agreement, neither the Company, nor any of its affiliates, shall be liable for: (a) any indirect, reliance, exemplary, incidental, speculative, punitive, special, consequential or similar damages that may arise in connection with this Agreement, (b) any lost or foregone tax revenues, or (c) any damages, liabilities, fees, costs, expenses, penalties, diminishments in value, losses or payments (including any lost or foregone tax revenues) that exceed, in the aggregate, the lesser of (i) the financial benefit realized by the Company under this Agreement and (ii) \$3,000,000.
- G. Force Majeure:** A Party will not be deemed to be in breach, material breach, default or otherwise in violation of any term of this Agreement to the extent such Party's action, inaction or omission is the result of a Force Majeure Event. The Company and Sponsors agree to use commercially reasonable efforts to promptly resolve any Force Majeure Event that adversely and materially impacts their performance under this Agreement. A Force Majeure Event pauses a Party's performance obligation for the duration of the event, but does not excuse it. "Force Majeure Event" means any event or occurrence that is not within the control of such Party or its affiliates and prevents a Party from performing its obligations under this Agreement, including without limitation, any act of God; act of a public enemy; war; riot; sabotage; blockage; embargo; failure or inability to secure materials, supplies or labor through ordinary sources by reason of shortages or priority; labor strike, lockout or other labor or industrial disturbance (whether or not on the part of agents or employees of any Party); civil disturbance; terrorist act; power outage; fire, flood, windstorm, hurricane, earthquake or other casualty; any law, order, regulation or other action of any governing authority; any action, inaction, order, ruling moratorium, regulation, statute, condition or other decision of any governmental agency having jurisdiction over a Party hereto, over the Facility or over a Party's operations.
- H. Severability:** The Parties agree that if any term or provision of this Agreement is declared by a court of competent jurisdiction to be illegal or in conflict with any law, the validity of the remaining terms and provision shall not be affected, and the rights and obligations of the Parties shall be construed and enforced as if the Agreement did not contain the particular term or provision held to be invalid. It is the intent of the Parties that, in the event a clause or provision is stricken, that there be added as part of this Agreement a clause or provision as similar in terms as may be possible, legal, and enforceable so as to provide a comparable property tax exemption and comparable payments as provided for in this Agreement.
- I. Governing Law:** This Agreement is governed by the laws of the State of Oregon, without giving effect to any conflict-of-law principle that would result in the laws of any other jurisdiction governing this Agreement.

- J. Venue:** Any action, suit, or proceeding arising out of the subject matter of this Agreement will be litigated in courts located in Umatilla County, Oregon. Each Party consents and submits to the jurisdiction of any local, state, or federal court located in Umatilla County, Oregon.
- K. Attorney's Fees:** If any arbitration, action, suit, or proceeding is instituted to interpret, enforce, or rescind this Agreement, or otherwise in connection with the subject matter of this Agreement, including but not limited to any proceeding brought under the United States Bankruptcy Code, the prevailing party on a claim will be entitled to recover with respect to the claim, in addition to any other relief awarded, the prevailing party's reasonable attorney's fees and other fees, costs, and expenses of every kind, including but not limited to the costs and disbursements specified in ORCP 68 A(2), incurred in connection with the arbitration, action, suit, or proceeding, any appeal or petition for review, the collection of any award, or the enforcement of any order, as determined by the arbitrator or court. Notwithstanding the foregoing, each Party's maximum liability under this Section IX(K) is \$75,000 and in no event will any Party be obligated by this Section IX(K) to pay an amount in excess of \$75,000.
- L. Entire Agreement:** This Agreement contains the entire understanding of the Parties regarding the subject matter of this Agreement and supersedes all prior and contemporaneous negotiations and agreements, whether written or oral, between the Parties with respect to the subject matter of this Agreement.
- M. Signatures:** This Agreement may be signed in one or more counterparts, each of which is an original, and all of which together constitute only one agreement between the Parties. A signature page delivered by fax or electronic mail will be considered an original signature page. At the request of a Party, the other Parties will confirm delivery of a signature page (whether by fax or electronic mail) by delivering an original signature page to the requesting Party.
- N. Waiver of Jury Trial:** WITH RESPECT TO ANY PROCEEDING OR ACTION ARISING OUT OF OR IN ANY WAY RELATING TO THIS AGREEMENT (WHETHER IN CONTRACT, TORT, EQUITY OR OTHERWISE), THE PARTIES KNOWINGLY, INTENTIONALLY, IRREVOCABLY AND UNCONDITIONALLY WAIVE THEIR RIGHT TO TRIAL BY JURY. EACH PARTY CERTIFIES AND ACKNOWLEDGES THAT IT HAS CONSIDERED THE IMPLICATIONS OF THIS WAIVER, IT MAKES THIS WAIVER KNOWINGLY AND VOLUNTARILY, AND IT HAS DECIDED TO ENTER INTO THIS AGREEMENT IN CONSIDERATION OF, AMONG OTHER THINGS, THE MUTUAL WAIVERS AND CERTIFICATIONS IN THIS SECTION.

IN WITNESS WHEREOF, the Parties, by their respective duly authorized representatives, have executed this Agreement on the date shown below.

UMATILLA COUNTY

Dated this [] day of [], 2022

DRAFT

Dan Dorran, Commissioner

DRAFT

George L. Murdock, Commissioner

DRAFT

John M. Shafer, Commissioner

CITY OF HERMISTON

Dated this [] day of [], 2022

DRAFT

Dr. David A. Drotzmann, Mayor

Attest:

DRAFT

Mark I. Morgan, Greater Hermiston Enterprise Zone
Administrator

Amazon Data Services, Inc.

Dated this _____ day of [], 2022

DRAFT

Appendix 1

| Tax Year | AAF Base Amount* | PSIF Payment Amount | SSF Payment Amount |
|-----------------|-------------------------|----------------------------|---------------------------|
| 2023 | 27,318,175 | 54,636 | 54,636 |
| 2024 | 28,137,720 | 56,275 | 56,275 |
| 2025 | 28,981,852 | 57,964 | 57,964 |
| 2026 | 29,851,307 | 59,703 | 59,703 |
| 2027 | 30,746,847 | 61,494 | 61,494 |
| 2028 | 31,669,252 | 63,339 | 63,339 |
| 2029 | 32,619,330 | 65,239 | 65,239 |
| 2030 | 33,597,909 | 67,196 | 67,196 |
| 2031 | 34,605,847 | 69,212 | 69,212 |
| 2032 | 35,644,022 | 71,288 | 71,288 |
| 2033 | 36,713,343 | 73,427 | 73,427 |
| 2034 | 37,814,743 | 75,629 | 75,629 |
| 2035 | 38,949,185 | 77,898 | 77,898 |
| 2036 | 40,117,661 | 80,235 | 80,235 |
| 2037 | 41,321,191 | 82,642 | 82,642 |
| 2038 | 42,560,827 | 85,122 | 85,122 |
| 2039 | 43,837,651 | 87,675 | 87,675 |
| 2040 | 45,152,781 | 90,306 | 90,306 |
| 2041 | 46,507,364 | 93,015 | 93,015 |
| 2042 | 47,902,585 | 95,805 | 95,805 |
| 2043 | 49,339,663 | 98,679 | 98,679 |
| 2044 | 50,819,853 | 101,640 | 101,640 |
| 2045 | 52,344,448 | 104,689 | 104,689 |
| 2046 | 53,914,782 | 107,830 | 107,830 |
| 2047 | 55,532,225 | 111,064 | 111,064 |
| 2048 | 57,198,192 | 114,396 | 114,396 |
| 2049 | 58,914,138 | 117,828 | 117,828 |
| 2050 | 60,681,562 | 121,363 | 121,363 |
| 2051 | 62,502,009 | 125,004 | 125,004 |
| 2052 | 64,377,069 | 128,754 | 128,754 |
| 2053 | 66,308,381 | 132,617 | 132,617 |
| 2054 | 68,297,632 | 136,595 | 136,595 |
| 2055 | 70,346,561 | 140,693 | 140,693 |
| 2056 | 72,456,958 | 144,914 | 144,914 |
| 2057 | 74,630,667 | 149,261 | 149,261 |

* Amount of payment will be calculated by applying the applicable tax code area ad valorem property tax rate to the AAF Base Amount.

RESOLUTION NO. 2240**A RESOLUTION APPROVING THE EXECUTION OF A LONG-TERM RURAL ENTERPRISE ZONE ABATEMENT AGREEMENT WITH AMAZON DATA SERVICES, INC.**

WHEREAS, the Board of County Commissioners of Umatilla County and the City Council of the City of Hermiston are equal co-sponsors of the Greater Hermiston Enterprise Zone; and

WHEREAS, Amazon Data Services, Inc. has requested a Long-term Rural Enterprise Zone Abatement pursuant to ORS 285C.409(1); and

WHEREAS, the proposed facility site is located in the Greater Hermiston Enterprise Zone on Parcels 1 and 2 of UMATILLA COUNTY PARTITION PLAT NO. 2018-23 (Instrument No. 2018-6800729, Office of Umatilla County Records) located in the Northeast Quarter of Section 27, Township 4 North, Range 28, East of the Willamette Meridian, Umatilla County, Oregon (consisting of approximately 9.97 acres with respect to Parcel 1 and 120.83 acres with respect to Parcel 2) (the "Partition Plat"); and

WHEREAS, Amazon Data Services, Inc. agrees to adhere to all requirements of an agreement for Long-Term Rural Enterprise Zone Abatement with Umatilla County and the City of Hermiston as co-sponsors of the Greater Hermiston Enterprise Zone, as well as any applicable laws of the State of Oregon relating to Long-Term Rural Enterprise Zone Abatement; and

WHEREAS, the Agreement for Long-Term Rural Enterprise Zone Abatement requires Amazon Data Services, for the term of the agreement, to invest more than \$200 million, hire at least 10 full-time employees, provide employee compensation greater than 130 percent of the Umatilla County average wage, and pay the required payments and fees to the zone over the term of the agreement.

NOW, THEREFORE, THE CITY OF HERMISTON RESOLVES AS FOLLOWS:

1. That the form, terms, and provisions of the draft of the Long-Term Rural Enterprise Zone Abatement Agreement with Amazon Data Services, entered by Amazon Data Services, and the City of Hermiston, and the transactions contemplated thereby, providing for, among other things, establishing Amazon Data Services' investment in a new data center facility and related employment, and the Long-Term Rural Enterprise Zone Abatement pursuant to Oregon Revised Statute, copies of which have been presented to and reviewed by the Common Council be, and they are, in all respects, hereby approved and adopted; and

2. That the City Manager of the City of Hermiston is hereby authorized to execute and deliver the Long-Term Rural Enterprise Zone Abatement Agreement with Amazon Data Services, Inc., substantially in the form heretofore approved and adopted with such changes, additions, deletions and modifications as such executing officer or officers may approve, such execution to be conclusive evidence of such approval and of the authorization thereof by the Common Council; and

3. That the City Manager of the City of Hermiston is hereby authorized and directed to prepare, execute, deliver, acknowledge and file such additional documents, agreements, certificates, forms, receipts and other instruments, in the name of and on behalf of the City of Hermiston and under its corporate seal, if so desired, and to take all such other actions as such officer or officers shall, in his or her or their sole discretion, approve in order to carry out the transactions heretofore approved and perform and discharge the City of Hermiston's obligations under or in connection with the Long-Term Rural Enterprise Zone Abatement Agreement with Amazon Data Services, Inc., such actions and execution to be conclusive evidence of such approval and of the authorization thereof by the Common Council.

4. That this resolution is effective immediately upon its passage.

PASSED by the Common Council this 26th day of September, 2022.
 SIGNED by the Mayor this 26th day of September, 2022.

Dr. David Drotzmann, MAYOR

ATTEST:

Lilly Alarcon-Strong, CMC, CITY RECORDER



Where Life is Sweet

Mayor and Members of the City Council

STAFF REPORT

For the Meeting of **September 26, 2022**

Title/Subject

Umatilla River Bridge Report Acceptance

Summary and Background

The City's Transportation System Plan has shown two potential future bridges over the Umatilla River for the past 20 years. Those bridge locations would line up either with Elm Avenue, or Punkin Center Road. Due to the high cost of bridge construction and maintenance, it has never been assumed that both bridges would be built, but both options have been shown as potential locations.

In analyzing potential funding options for a future bridge, it was determined that the first step toward actually making a case for funding had to be selecting one preferred option. The City therefore engaged Anderson Perry & Associates, along with Kittleson & Associates, to develop high-level designs of potential bridges at both locations and analyze their impacts to the broader transportation system. Due to the needs for connection to Umatilla's South Hill, and broader impact to the County, Umatilla County contributed \$50,000 toward this analysis, and the City of Umatilla contributed \$30,000.

Anderson Perry will present a full explanation of their analysis to the Public Infrastructure Committee on September 26. A final component to the analysis is including consensus from the Cities of Hermiston and Umatilla and Umatilla County about which is the preferred bridge location.

There are many pros and cons to both potential locations, but ultimately I recommend going with the Punkin Center option because:

1. It is less expensive overall.
2. It is more scalable. If funds don't allow, this project could just tie in to River Road to start with and then extend up the hill to connect to Punkin Center in the future.
3. It is a more direct connection to I-82.
4. The usage and parcel sizes of land along this route west of the river lend themselves much more readily for development in the future.

5. It is twice as far north from the existing bridge at Highland than the Elm option; truly creates a new connection point, rather than simply a redundant one.

Tie-In to Council Goals

Goal #5- Complete Alternative Route Transportation Study

Fiscal Information

This report will have no fiscal impact, but simply directs staff which option to focus future efforts on to obtain funding.

Alternatives and Recommendation

Alternatives

1. Adopt MOU with City of Umatilla and Umatilla County
2. Amend MOU
3. Reject MOU

Recommended Action/Motion

Motion to authorize the Mayor to execute the attached MOU.

Submitted By:

Mark Morgan

Memorandum of Understanding
City of Hermiston, City of Umatilla, and Umatilla County
Umatilla River Bridge Preliminary Engineering Report
August 10, 2022

The purpose of this Memorandum of Understanding (MOU) is to provide a general consensus among the City of Hermiston, the City of Umatilla, and Umatilla County, on which of the two bridge options presented in the draft Umatilla River Bridge Preliminary Engineering Report (PER) by Anderson Perry & Associates, Inc., dated August 2022, is preferred, and to agree on the next steps to begin construction of a new bridge over the Umatilla River. It is understood that this is not a final decision on bridge location, but a general consensus on which option appears to be preferred based on information provided in the PER and other currently available information.

All parties generally agree that both the Punkin Center Road and Elm Avenue options are viable and that the Punkin Center Road option, as presented in the PER, is the preferred option. Regardless of which option is ultimately constructed, all parties agree to incorporate and reference the PER in each of their respective Transportation System Plans at their next scheduled revision. This will allow all parties to be united and enable educated planning decisions to be made going forward.

These general terms as outlined herein are agreed to by each party. This MOU provides each party with assurance that all parties agree to and intend to proceed as outlined herein.

City of Hermiston, Oregon

By _____

Print Name _____

Title _____

City of Umatilla, Oregon

By _____

Print Name _____

Title _____

Umatilla County, Oregon

By  _____

Print Name John M. Shafer

Title Chair, Board of Commissioners



UMATILLA RIVER BRIDGE
PRELIMINARY ENGINEERING REPORT

JULY 2022



Prepared for
City of Hermiston, Oregon
City of Umatilla, Oregon
Umatilla County, Oregon

DRAFT

UMATILLA RIVER BRIDGE
PRELIMINARY ENGINEERING REPORT
FOR
CITY OF HERMISTON, OREGON
CITY OF UMATILLA, OREGON
UMATILLA COUNTY, OREGON

2022



ANDERSON PERRY & ASSOCIATES, INC.

La Grande, Redmond, and Hermiston, Oregon
Walla Walla, Washington

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Executive Summary

A. Introduction

This Executive Summary briefly presents the results of the Umatilla River Bridge Preliminary Engineering Report (PER) prepared by Anderson Perry & Associates, Inc., for the City of Hermiston, Oregon, in partnership with the City of Umatilla and Umatilla County. The findings outlined herein have been developed in cooperation with each of these three benefitting jurisdictions. The focus of this PER is to compare two potential locations for a bridge crossing the Umatilla River. The two locations considered are at W. Punkin Center Road and W. Elm Avenue. This PER includes a high-level analysis of the existing transportation system and how the construction of a bridge would affect the system. This PER discusses recommended improvements to the transportation system based on the traffic analysis and bridge location and provides high-level project cost estimates for each project necessitated by bridge construction for comparison purposes. Additionally, it includes a discussion of the impacts to land use and acquisition and explores environmental, permitting, and cultural resource requirements and recommendations. This Executive Summary includes a brief discussion of the transportation system analysis; recommended improvements to the transportation system; and permitting, environmental, and cultural resource requirements. This PER also includes a summary of the bridge location selected by the benefitting jurisdictions as a result of this PER. For a more detailed discussion of the information presented in this Executive Summary, refer to the individual chapters of this PER.

The benefitting jurisdictions recognize the need for this PER, as installation of an additional bridge over the Umatilla River has been included in the City of Hermiston's Transportation System Plan (TSP) since 1997. Hermiston's TSP has never identified which of the two proposed locations is the best option. This PER will act as a guide to enable the benefitting jurisdictions to ultimately select the most appropriate location for the Umatilla River Bridge.

B. Transportation System

Because constructing a new bridge across the Umatilla River will significantly impact traffic flow conditions, a transportation system analysis was conducted by Kittelson & Associates, Inc. Four scenarios were analyzed, including existing traffic conditions, projected traffic conditions in the future with no bridge constructed, projected traffic conditions in the future with a bridge constructed in line with W. Elm Avenue, and projected traffic conditions in the future with a bridge constructed in line with W. Punkin Center Road. The travel corridors are depicted on Figures 4-1A through 4-1D and 4-2A through 4-2C in Chapter 4. The Interstate 82 interchange on Powerline Road and U.S. Highway 395 is at the end of the travel corridors and is generally excluded from the scope of this PER. The analysis found that future conditions would exceed the capacity of much of the existing infrastructure. Improvements recommended in this PER include turning lanes, traffic signals, new sections of roadway, and the bridge structure itself. Furthermore, infrastructure improvements necessitate the acquisition of additional right-of-way (ROW). Total estimated project costs, including ROW acquisition, construction, engineering fees, contingencies, etc., for the various improvements discussed in this PER total approximately \$48.82 million for the Elm Avenue option and approximately \$43.6 million for the Punkin Center Road option. These costs do not include costs associated with permitting, environmental, and cultural resource requirements.

C. Land Use and Acquisition

A new bridge across the Umatilla River would fundamentally change traffic patterns and future land use. ROW acquisition would be needed to enable construction of the recommended improvements. Estimated costs discussed above include ROW acquisition and are based on properties' real market values. The recommended ROW width of the travel corridor was assumed to be 90 feet. Figures 4-1A through 4-1D and 4-2A through 4-2C in Chapter 4 show ROW acquisition locations for properties adjacent to potential future travel corridors.

Properties adjacent to each potential travel corridor have various zoning designations. Zoning designations encompass the zones identified by the City of Hermiston, City of Hermiston Comprehensive Use Plan, and Umatilla County. In Chapter 4, Figure 4-5A, City of Hermiston Zoning, shows the zoning designations of the land within city limits; Figure 4-5B, Umatilla County Zoning, shows the zoning designations of the land outside city limits and the urban growth boundary (UGB); and Figure 4-5C, Comprehensive Land Use Map, shows the zoning designations of the land outside of city limits but within the UGB. Each potential bridge location would provide access to the west side of the river and has substantial potential to encourage development along the new travel corridor. Much of the new travel corridor would pass through what is now mostly rural farmland. To further encourage development, land use and zoning designations would need to be modified to allow for more urbanized land use types such as commercial, industrial, and residential. The Elm Avenue option would provide a more direct path to the heart of the City of Hermiston, which could be advantageous for connectivity, but would come at the cost of significantly increased traffic. Although connectivity to the city center would not be directly achieved with the Punkin Center Road option, this option would generally provide more favorable traffic conditions than the Elm Avenue option.

D. Permitting, Environmental, and Cultural Resources

New bridge construction, to some extent, will impact local natural resources as defined and discussed in Goal 5 of Umatilla County's Comprehensive Land Use Plan. Goal 5 resources include riparian corridors, wetlands, wildlife habitat, and cultural areas. A new bridge is anticipated to impact wetlands, waterways, floodplains, and farmland. Endangered Species Act-listed species occur within the project corridor options. A new bridge would result in new impervious surfaces. Two parks, Butte Park, which is funded by the Land and Water Conservation Fund, and the disc golf course, are located adjacent to the Elm Avenue option. Several Oregon Department of Environmental Quality permitted sites are located adjacent to the project corridor options, resulting in a chance of contaminated soils being encountered during construction. A Bureau of Land Management (BLM)-owned natural area is located adjacent to the Punkin Center Road option.

Estimated costs for potential permits and environmental clearances that could be required for the new bridge total \$574,000 for the Elm Avenue option and \$624,000 for the Punkin Center Road option. The funding agency will determine which environmental permits and consultation will be required for the proposed project. If federal funding is utilized, an Environmental Assessment will be required. Since a portion of the Punkin Center Road option corridor occurs on BLM-owned land, the project may be subject to BLM National Environmental Policy Act requirements and an extensive environmental review process.

E. Selected Improvements and Project Implementation

Forthcoming.

Chapter 1 - Introduction and Background

A. Introduction

The purpose of this Preliminary Engineering Report is to identify and compare the strengths, weaknesses, requirements, high-level planning costs, and implications for constructing a bridge across the Umatilla River in one of two locations. Because inflation is currently unpredictable, all estimated costs are given for the year 2022. Inflation will need to be accounted for depending on the year the bridge is constructed. The two bridge crossing locations considered are at W. Elm Avenue and W. Punkin Center Road. See Figure 1-1 for the location and vicinity maps and Figure 1-2 for an aerial view of the potential project locations. The two bridge options were chosen based on the City of Hermiston's Transportation System Plan (TSP), which identified these locations for potential bridge options. A specific option was not selected in the TSP likely because there was not enough information readily available at the time to make an informed decision. This document will aid all benefitting jurisdictions involved in making an informed decision when selecting a bridge option.

B. Historical Background

In 1985 the U.S. Congress passed a law that created the Chemical Stockpile Emergency Preparedness Program (CSEPP) and directed the Army to dispose of stored aging chemical weapons. This was done with the protection of the public and environment as its primary objective. Federal funding through the CSEPP was available for projects related to stored chemical weapons at the Umatilla Army Depot, approximately 4 miles west of Hermiston. A bridge across the Umatilla River was originally proposed as an emergency evacuation route sometime during the 1990s. The proposal was ultimately not supported by the CSEPP, and a bridge was not constructed at that time. Although construction did not occur during that period, the two bridge options were adopted as part of Hermiston's original TSP in 1997. Although low on the priority list in the 1997 TSP, a bridge across the Umatilla River has become increasingly important as time has passed and urban growth has continued.

C. Demand for Umatilla River Bridge

Although not immediately necessary, the forecasted urban growth within Umatilla County has made apparent the need to plan for an additional bridge across the Umatilla River in the future. As part of this planning, it is prudent that one of the two options for the bridge location be selected well in advance of actual project initiation. Although a time frame for bridge construction is difficult to determine, local officials estimate that bridge construction will not occur until at least the year 2032. Selecting the location this far in advance will enable local officials to make informed decisions about growth and development in Umatilla County.

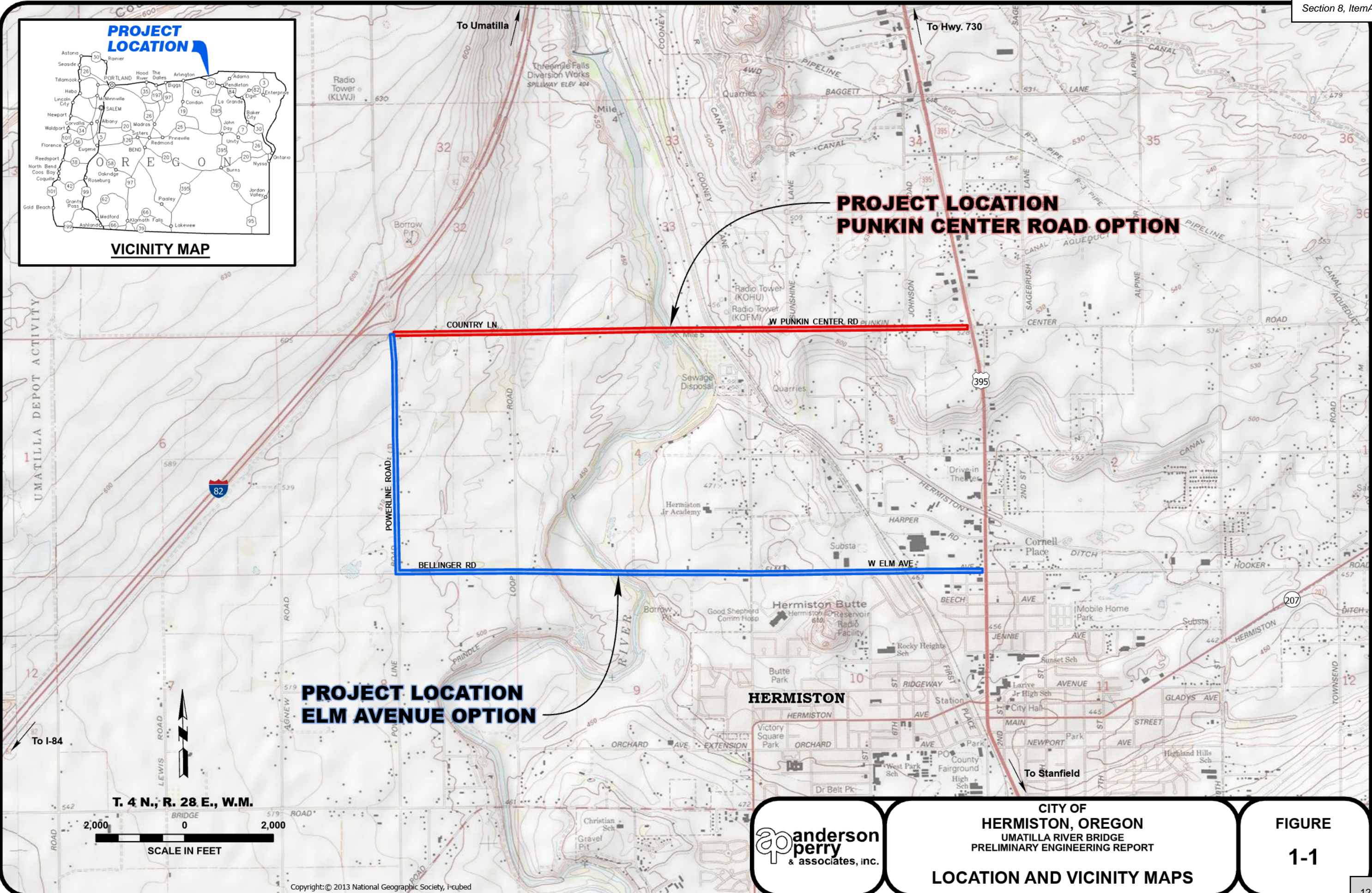
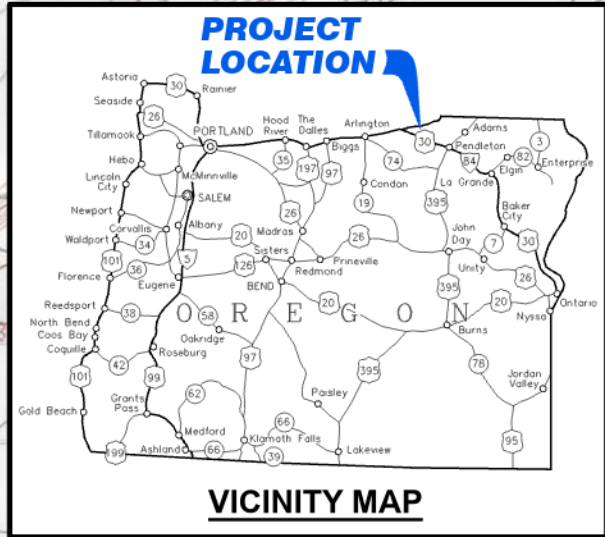
The City of Hermiston anticipates that their urban growth boundary will eventually include sections of land west of the Umatilla River. A second bridge spanning the Umatilla River would reduce the likelihood of bottlenecks at the current bridge location where Bridge Road becomes West Highland Avenue and would favorably alter local traffic conditions in the City of Hermiston and Umatilla County. The second bridge would also encourage growth on the west side of the Umatilla River. Because growth, especially urban growth, is usually associated with strong economic conditions, growth on the west side of the Umatilla River would benefit both Umatilla County and the City of Hermiston.

Adding a bridge crossing over the Umatilla River would be beneficial to the City of Umatilla because it would encourage further growth, especially at the south end of the City. Many City of Umatilla residents work, shop, recreate, and generally use the amenities available in Hermiston. A bridge would provide another travel route between the two cities that would likely be more direct and lead to decreased travel time. In addition to enabling and promoting growth on the west side of the Umatilla River and in the City of Umatilla, the bridge would also help alleviate current traffic congestion on U.S. 395 and N. 1st Place. Mitigating traffic congestion on U.S. 395 and N. 1st Place is in the best interest of Umatilla County, the City of Umatilla, and the City of Hermiston because it would increase safety and allow better traffic conditions for local residents.

D. Summary

This planning document presents information needed to make a more informed decision on which of the two proposed bridge locations is most appropriate. A bridge crossing the Umatilla River has been discussed since the CSEPP began funding projects in the area, and the need for a bridge has become increasingly important. Selecting a bridge location will help local jurisdictions plan for growth and development. Benefits of the bridge include, but are not limited to, promoting growth in the City of Hermiston, the City of Umatilla, and Umatilla County; decreasing traffic congestion; and increasing traffic safety.

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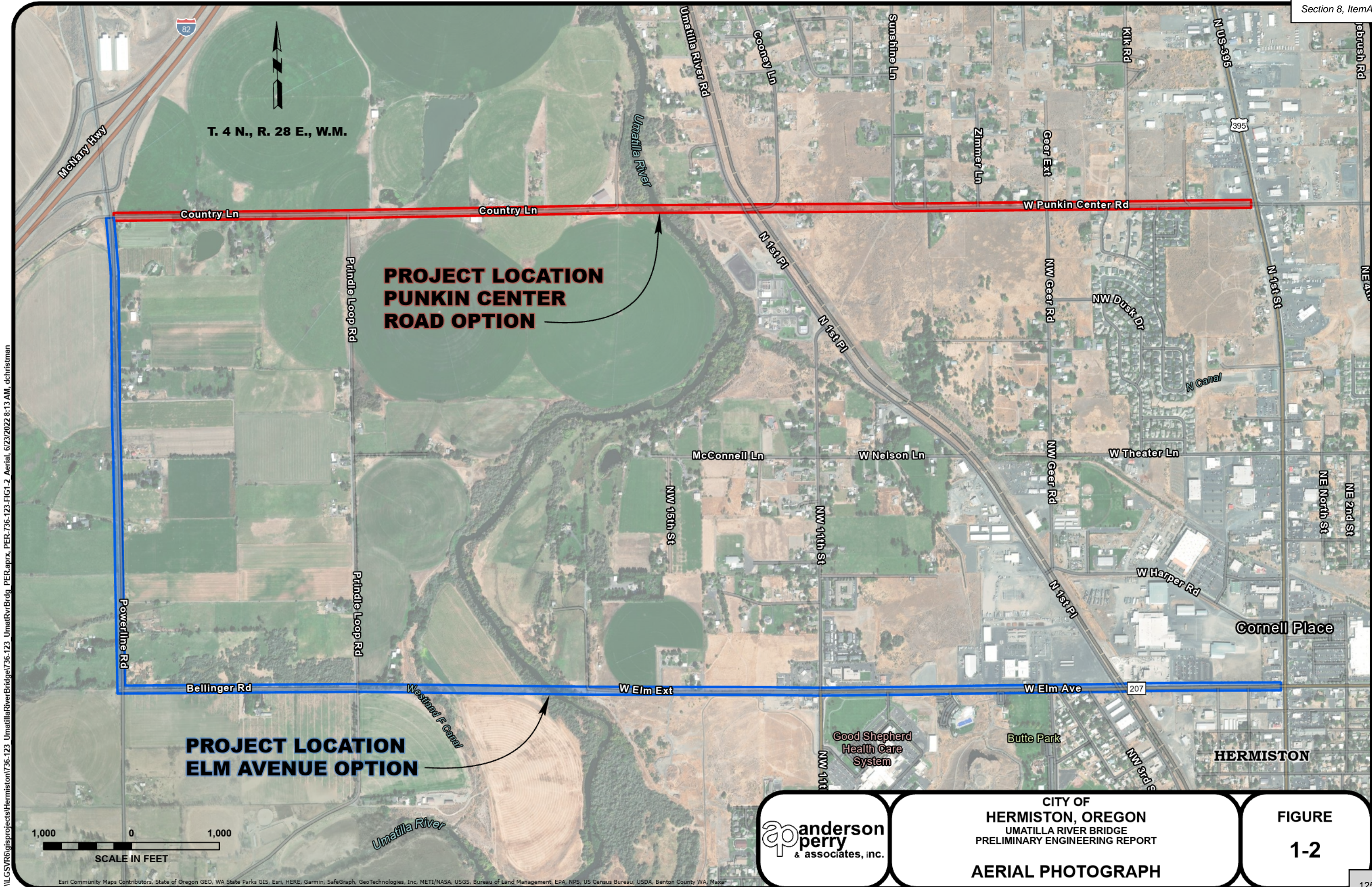


anderson
perry
& associates, inc.

**CITY OF
HERMISTON, OREGON**
UMATILLA RIVER BRIDGE
PRELIMINARY ENGINEERING REPORT

**FIGURE
1-1**

LOCATION AND VICINITY MAPS



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Chapter 2 - Transportation Impact Analysis

A. Introduction

Constructing a new bridge across the Umatilla River will have a significant impact on traffic flow conditions. As such, Kittelson & Associates, Inc., conducted an analysis of the transportation system. This chapter summarizes their analysis as described in a technical memo dated March 4, 2022, included in Appendix A of this Preliminary Engineering Report (PER), and hereinafter referred to as the Kittelson technical memo. This analysis is intended as a high-level analysis only, and future traffic patterns, traffic volumes, required infrastructure improvements, etc., may differ from those identified herein.

As discussed in Chapter 1, two options exist for the construction of a new bridge. The impacts to the transportation system likely reach far beyond the area of proposed improvements. To maintain cost effectiveness, the analysis area was confined to the major travel corridors that the project will directly affect, with the assumption that impacts on other areas will be minor. The Elm Avenue option consists of a travel corridor beginning at the intersection of Elm Avenue and U.S. 395. The corridor would proceed west of the intersection, span the Umatilla River with a new bridge, extend across what is currently private property, connect to Bellinger Road, turn north at Powerline Road, and terminate at the interchange of Powerline Road and Interstate 82 (I-82). The Punkin Center Road option consists of a travel corridor beginning at the intersection of Punkin Center Road and U.S. 395. The corridor would proceed west of the intersection until intersecting with Sunshine Lane, where it would continue west through what is currently private property. The corridor would continue west, crossing the Union Pacific Railroad and Umatilla River Road, then proceed west, spanning the Umatilla River and connecting with Country Lane, then turn north at Powerline Road and terminate at the same interchange as the Elm Avenue option.

These two options were analyzed under four scenarios, as follows:

- 2021 existing traffic conditions
- 2032 with neither option being constructed (No-Build Condition)
- 2032 with the Punkin Center Road option constructed
- 2032 with the Elm Avenue option constructed

These analyses were used to provide insight on resulting traffic conditions and to determine what improvements may be necessary as part of the bridge construction project. This chapter summarizes the analysis and the insights derived therefrom. Chapter 3 discusses recommended improvements based on the results of the traffic analysis.

B. Data Collection Summary

To analyze existing and future traffic conditions, data were collected to form a basis for the analysis. Manual turning/movement counts were collected at key intersections in October 2021. See Appendix A of the Kittelson technical memo for count data at various key intersections. Counts were collected on a typical weekday (while schools were in session) from 7:00 to 9:00 a.m. and 4:00 to 6:00 p.m. These

times were selected to capture anticipated weekday morning and afternoon peak hour operation conditions.

This information was utilized to assess existing intersection adequacy based on applicable Oregon Department of Transportation (ODOT) mobility targets. After projecting growth and using reasonable traffic redistribution assumptions, this information was used to assess intersection adequacy in the year 2032 under the various conditions previously described.

C. Existing Traffic Conditions

After collecting needed data and accounting for seasonal adjustments using industry standard methods, existing intersections were evaluated. Appendix C of the Kittelson technical memo contains the operations analysis of existing intersections. Four intersections currently experience substantial traffic movements: U.S. 395 and Punkin Center Road, N.W. 11th Street and W. Elm Avenue, N. 1st Place and W. Elm Avenue, and U.S. 395 and W. Elm Avenue, with volume to capacity ratios of 0.55, 0.66, 0.66, and 0.64, respectively. While these intersections do not exceed their ODOT mobility target of 0.9, they will be critical intersections to analyze under future conditions. ODOT has indicated they are aware of and are anticipating necessary upgrades to the intersections of U.S. 395 and W. Elm Avenue and N. 1st Place and W. Elm Avenue. All other existing intersection studies had volume to capacity ratios of less than or equal to 0.26. All study intersections meet applicable ODOT mobility targets during the weekday morning and afternoon study hours.

D. Projected Traffic Conditions

To determine needed improvements to the travel corridors for each option, an understanding of future traffic conditions was needed. To accomplish this task, practicable growth assumptions were used in conjunction with assumed traffic redistribution to project existing traffic counts into the year 2032. This year was chosen based on conversations with the City of Hermiston, the City of Umatilla, and Umatilla County and represents a reasonable near-term horizon year for this project. Each scenario was investigated as follows.

No-Build Condition (2032 Projection)

Future traffic was first analyzed assuming no bridge is constructed. Only traffic growth was accounted for, and redistribution of traffic does not occur since the fundamental path of travel does not change in this scenario. Annual traffic growth rates between 1.5 and 3 percent were conservatively applied to account for anticipated local and regional growth. Projected traffic demands associated with several approved development projects listed below were also incorporated into the analysis.

- Ambience Homes Traffic Impact Analysis in Umatilla
- Umatilla Residential Development Traffic Impact Analysis for Vandelay Meadows, Cheryl's Place, and Ballard Property
- McClannahan Summit subdivision

Under projected traffic flow conditions in the year 2032, all intersections except one are anticipated to meet applicable ODOT mobility targets during the weekday morning and afternoon study hours. The single exception is the intersection of U.S. 395 and W. Elm Avenue, which exceeds the required ODOT mobility target by approximately 3.3 percent.

Punkin Center Road Option (2032 Projection)

Using the No-Build Condition scenario as a baseline for projected traffic growth, redistribution assumptions were then made assuming a new bridge is constructed spanning the Umatilla River in line with Punkin Center Road. See pages 17 through 19 of the Kittelson technical memo for traffic redistribution assumptions.

Only one intersection was found to exceed its ODOT mobility target, the intersection of U.S. 395 and W. Elm Avenue. Since the intersection does not meet the required ODOT mobility target in the No-Build Condition, it follows that construction of the Punkin Center Road option would increase intersection traffic movements, lead to a higher volume to capacity ratio, and further exceed the ODOT mobility target. The intersection is projected to exceed the required ODOT mobility target by approximately 6.7 percent, which represents a slight degradation compared to the No-Build Condition.

The intersection of Powerline Road and Country Lane is approximately 275 feet south of the I-82/Powerline Road interchange. Although not exceeding its ODOT mobility target, this intersection is problematic because the existing spacing of 275 feet does not meet ODOT requirements. ODOT spacing standards require public street intersections to be a minimum of 0.25 mile (1,320 feet) from the interchange ramp terminal. Relocating the intersection farther away from the interchange would have a substantial impact on private landowners and would also need to be coordinated with ODOT and local jurisdictions.

A new intersection would be necessitated if Punkin Center Road is extended to span the Umatilla River. It is anticipated that the new intersection of Umatilla River Road and Punkin Center Road would be located at the existing intersection of Umatilla River Road and Cooney Lane. Since this would be a new intersection, it would have to be constructed to meet an applicable mobility target and operating standards.

While not anticipated to exceed its ODOT mobility target, the intersection of U.S. 395 and Punkin Center Road is projected to have significantly increased turning movements, resulting in long vehicle queues, especially for right-turn movements on the eastbound approach. It would be prudent, but not required, to consider upgrades to this intersection as set forth in Chapter 3.

Although the I-82/Powerline Road interchange is not considered within the scope of this PER, Kittelson & Associates, Inc., noted that, under this option, the applicable ODOT mobility target would be exceeded by approximately 11.8 percent.

Elm Avenue Option (2032 Projection)

Using the No-Build Condition scenario as a baseline for projected traffic growth, redistribution assumptions were then made assuming a new bridge is constructed spanning the Umatilla River in

line with W. Elm Avenue. See pages 27 through 33 of the Kittelson technical memo for traffic redistribution assumptions.

Under this scenario, several intersections were found to exceed applicable ODOT mobility targets. First, the intersection of U.S. 395 and W. Elm Avenue is projected to have significantly increased traffic movements and is anticipated to exceed its ODOT mobility target by approximately 13.3 percent. This is a substantial increase when compared to the No-Build Condition of 3.3 percent.

Another key intersection that exceeds its mobility target is the intersection of W. Elm Avenue and N.W. 11th Street. Significantly increased traffic, especially on the currently underutilized eastbound approach, is predicted. The intersection is projected to exceed its ODOT mobility target by approximately 4.4 percent.

Analysis indicates that the intersection of W. Elm Avenue and N. 1st Place will operate at 99 percent of its ODOT mobility target with a volume to capacity ratio of 0.89. Because this intersection will only narrowly operate within the acceptable ODOT mobility target, the intersection has limited long-term flexibility to accommodate additional traffic growth.

The intersection at Powerline Road and Bellinger Road, while not expected to exceed its ODOT mobility target, is expected to see significantly increased turning movements. The Kittelson technical memo indicates that to accommodate increased turning movements and create a safe and efficient intersection, the intersection will need to be upgraded to urban travel standards as set forth in Chapter 3.

Although the I-82/Powerline Road interchange is not considered within the scope of this PER, Kittelson & Associates, Inc., noted that its estimated volume to capacity ratio of 0.71 would not exceed the applicable ODOT mobility target of 0.85.

E. Summary

Traffic counts were collected at key intersections to serve as a basis for analyzing existing and projected traffic conditions. After analyzing the current transportation system, reasonable growth rates were applied to project traffic conditions in the year 2032 assuming no bridge is constructed. Reasonable traffic redistribution was then applied to the No-Build Condition to analyze the transportation system under each bridge option.

Projected traffic for the Punkin Center Road option is expected to exceed the applicable ODOT mobility target at the intersection of U.S. 395 and W. Elm Avenue; however, the volume to capacity ratio is only marginally higher than the No-Build Condition scenario. The I-82 interchange is also expected to exceed its ODOT mobility target. The intersection of Country Lane and Powerline Road will have to be reconfigured to meet ODOT spacing requirements. Although meeting its ODOT mobility target, significant queue lengths and delay times are expected for the west leg of the intersection at U.S. 395 and Punkin Center Road unless improvements to the intersection are constructed. A new intersection must be constructed to meet applicable standards at the extension of Punkin Center Road and Umatilla River Road.

Projected traffic for the Elm Avenue option is expected to significantly exceed the ODOT mobility target at the intersection of U.S. 395 and W. Elm Avenue. Traffic at the N.W. 11th Street and W. Elm Avenue

intersection is also expected to exceed its ODOT mobility target. Although not actually exceeding its ODOT mobility target, the intersection of W. Elm Avenue and N. 1st Place is expected to operate at 99 percent of capacity and will not have much flexibility to accommodate increased traffic demands as growth occurs in the surrounding area. In addition, operating standards would necessitate the upgrade of the intersection of Bellinger Road and Powerline Road.

**TABLE 2-1
TRANSPORTATION ANALYSIS SUMMARY**

| | |
|-----------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| Existing (2021) | <ul style="list-style-type: none"> No intersections exceed ODOT mobility targets. |
| No-Build (2032) | <ul style="list-style-type: none"> All intersections meet applicable ODOT mobility targets except one. The U.S. 395/Elm Avenue intersection exceeds the required ODOT mobility target by 3.3 percent. |
| Punkin Center Bridge (2032) | <ul style="list-style-type: none"> The U.S. 395/Elm Avenue intersection exceeds the ODOT mobility target by 6.7 percent. The Powerline Road/Country Lane intersection violates the ODOT spacing requirement of being farther than 0.25 mile from the nearest interchange ramp terminal. A new Umatilla River Road/Punkin Center Road intersection is needed. U.S. 395/Punkin Center Road does not exceed its ODOT mobility target but will likely experience long vehicles queues for right-turn movements on the east-bound approach. The I-82/Powerline Road interchange exceeds the ODOT mobility target by 11.8 percent. |
| Elm Avenue Bridge (2032) | <ul style="list-style-type: none"> The U.S. 395/W. Elm Avenue intersection exceeds the ODOT mobility target by 13.3 percent. The W. Elm Avenue/N.W. 11th Street intersection exceeds the ODOT mobility target by 4.4 percent. The W. Elm Avenue/N. 1st Place intersection will operate at 99 percent of its ODOT mobility target. Powerline Road/Bellinger Road will not exceed its ODOT mobility target but will require improvements to create a safe and efficient intersection. The I-82/Powerline Road interchange does not exceed the ODOT mobility target. |

Chapter 3 - Transportation System Improvements

A. Overview

The intent of this chapter is to identify required improvements to the transportation system for each bridge option. The travel corridor, intersections, bridge structure, grading, and other improvements are considered. Project costs for each improvement are estimated to provide a comparison between bridge options and to aid in the selection process. Estimated project costs in this chapter are high level and are intended for comparison purposes only. They are inclusive of construction costs, engineering fees, land acquisition costs, and other legal and administrative costs. Estimates in this chapter include only right-of-way (ROW) acquisition for intersection improvements when the additional ROW to be acquired is not parallel with the travel corridors. All other ROW acquisition costs are discussed in Chapter 4. Because the year of construction is unknown, inflation is difficult to predict in the current market, and cost estimates are for comparison purposes only, all costs are given in 2022 dollars and are not projected into the future. High-level cost estimates are located in Appendix B of this Preliminary Engineering Report (PER). Detailed cost estimates should be completed as part of the future bridge project.

Ordinary High Water

The ordinary high water (OHW) of a river is the typical or ordinary level that the water surface achieves during a typical year. It is generally determined by locating an OHW mark on the shore. OHW marks are typically identified in the field based on observations such as a clear, natural line impressed on the bank, silt stain lines on trees or leaves, the presence of litter or debris, the lowest extent of woody vegetation, etc. The ordinary high water elevation (OHWE) is the measured elevation at the surface of the OHW. The OHW width is the width of the river measured perpendicular to the banks at the OHWE. The Oregon Department of Fish and Wildlife typically requires a clear span equal to or greater than the OHW width with no bridge element or riprap within this span. If consultation under Section 7 of the Endangered Species Act (ESA) is required for the proposed project, the bridge span, as projected along the OHW width, would need to be 1.5 times the OHW width for a single span structure or 2.2 times the OHW width for a multi-span structure. Additional discussion regarding consultation under Section 7 of the ESA is included in Chapter 5. It is important to note that, for both options, the bridge is assumed to span the entire floodway utilizing a multi-span structure with intermediate supports. Spanning the floodway is a more stringent requirement than spanning 2.2 times the OHW width as discussed in the bridge section of each option below.

Federal Emergency Management Agency Floodplain and Floodway

The Federal Emergency Management Agency (FEMA) is currently in the process of analyzing the Umatilla River reach to revise their Flood Insurance Study (FIS) and corresponding Flood Insurance Rate Map (FIRM). The new FIS and FIRM will adjust flood boundaries, floodway widths, and other characteristics to reflect current topography, hydraulic data, and other items that may be in different conditions than existed at the time the last FIS and FIRM were completed. According to FEMA, the updated FIS is not anticipated to have a significant impact on current boundaries within

this section of the Umatilla River. However, minor changes are likely and large changes are possible. Although not expected, the results of the new FIS could alter some of the recommendations and findings contained in this PER. The most current FIS and corresponding FIRM should be utilized when the bridge design project is initiated.

According to FEMA FIRM Panel No. 41059C0577G, the Elm Avenue and Punkin Center Road bridge options would include infrastructure located within the 500- and 100-year floodplains and within the floodway. Therefore, FEMA development standards apply, and a Floodplain Development Permit would be required. Because typical concrete and steel bridges would not be able to span the large floodway widths at both locations without intermediate supports, bridge piers and associated footings would need to be constructed within the floodway. Any fill, bridge pier, or footing placed within the floodway triggers the requirement for a no-rise certification that must show a 0.00 foot change in the effective 100-year water surface elevation based on a hydraulic analysis. Based on discussions with FEMA representatives, it is unlikely that a no-rise condition can be demonstrated. Because bridge construction will preclude demonstration of a 0.00 foot change in the effective 100-year flood water surface elevation, the project will require a Conditional Letter of Map Revision (CLOMR) and, following construction of the bridge, a Letter of Map Revision (LOMR). Each of these requires a hydraulic/stream analysis that determines where the new flood boundaries will be located based on the increase in the surface water elevation of the 100-year flood. This process, including engineering documentation and agency fees, will be required for both options and is estimated to cost approximately \$150,000. It is important to note that the process of obtaining a CLOMR and a LOMR will likely be lengthy. It would be prudent to budget at least two years before construction to complete the CLOMR process and another two years after construction to complete the LOMR process.

B. Elm Avenue Option

Bridge Structure

Based on a desktop review, the OHW width for the river at the Elm Avenue Bridge crossing is approximately 120 feet. The bridge is anticipated to cross the river at an approximate 45-degree skew. The floodway measured along the line of travel is approximately 600 feet wide. The spans for this bridge were chosen based on several criteria. First, the spans had to be symmetrical, which usually lends to a more economical design. Second, the bridge had to cross the entire floodway to minimize environmental impacts and ensure that travel ways would not be damaged during a typical flood event. Third, a single segment of the bridge had to completely span across the OHW width portion of the river (no piers or abutments were assumed to be located in the OHW area). The bridge must also span 2.2 times the OHW width as discussed at the beginning of this chapter, and 2.2 times the OHW width of 120 feet projected onto the bridge travel corridor is approximately 375 feet. Since the total floodway width of approximately 600 feet is much larger than the required span of 375 feet, both span requirements are easily met by spanning the entire floodway. Three equal 250-foot spans were selected to meet these criteria, as shown on Figure 3-1. The total span of this bridge option is 750 feet.

The approximate bridge profile as shown on Figure 3-1 was determined based on two criteria. First, the bottom (low chord) of the bridge had to be a minimum of 3 feet above the 100-year flood elevation. This is an Oregon Department of Transportation (ODOT) requirement that is typically

deferred to when the local jurisdiction does not have a specific separation requirement. The 3 feet of separation provides sufficient space to enable floating debris to pass under the bridge during the 100-year flood event. Second, the approximate bridge profile was selected to effectively transition into the existing topography to provide smooth passage for vehicular traffic and to minimize the amount of cut/fill that will be required.

Bridge width was determined assuming two 12-foot travel lanes, two 6-foot wide bike/shoulder lanes, and two 6-foot wide sidewalks. This totals a width of 48 feet. Note that the bike/shoulder lane is one foot wider on the bridge than the rest of the travel corridor. This increased width is intended to create an increased buffer space between pedestrians and vehicular traffic so neither feel constricted as they navigate across the bridge.

Although bridge type was not specifically selected as part of this PER, the bridge is anticipated to be either prestressed concrete or steel. Bridge type should be selected when a preliminary design is completed as part of the actual design process. A planning-level cost of \$490 per square foot was used to conservatively estimate bridge project costs. The total area of the bridge, given a span of 750 feet and a width of 48 feet, is 36,000 square feet. At \$490 per square foot, the total project cost for the bridge structure is estimated to be \$17.7 million. Adding approximately \$1.7 million for the earthwork required for the roadway on either side of the river brings the total estimated project cost to \$19.4 million.

Although this PER assumes that the bridge crosses the river at a 45-degree skew to keep the travel corridor straight, other options exist where the bridge could cross at a much smaller skew provided that the travel corridor geometry is permitted to deviate from a straight line. Decreasing the skew angle means that the floodway width would also be lessened, which would lead to a shorter total bridge span and a reduced cost for the bridge structure.

Roadway Improvements

Chapter 2 discusses impacts to the transportation system from an analysis perspective. Based on those analyses, recommendations to improve the travel corridor were made. Many of the recommended improvements occur at intersections. Additional improvements are needed where no ROW currently exists. Improvements to the driving surface will also be required to increase roadway durability to account for increased traffic.

Discussions with ODOT revealed that this travel corridor is unlikely to ever become a state highway. They indicated that the only way the travel corridor could ever become a state route would be if Umatilla County took over other existing state routes as a trade for ODOT taking over the Elm Avenue option travel corridor. Because this is highly unlikely to occur, City of Hermiston standards were used as a basis for recommended travel corridor improvements.

Intersections

U.S. 395/W. Elm Avenue (OR 207)

Although this intersection was found to exceed capacity, the intersection is anticipated to exceed capacity regardless of a bridge being constructed. However, unlike the Punkin Center

Road option, construction of a bridge under the Elm Avenue option is estimated to significantly worsen capacity deficiencies. Regardless of whether a bridge is constructed, this intersection would need several improvements to the northbound, southbound, and westbound approaches. A northbound right-turn lane, a southbound right-turn lane, and a westbound right-turn lane would result in satisfactory traffic conditions. It is important to note that these improvements would produce an intersection that would meet the ODOT mobility target, but the intersection would be operating at full capacity. This means that any further growth would push the mobility target above acceptable values. All properties abutting the intersection are currently developed, which means acquiring additional ROW for turning lanes would be costly. The estimated project cost for upgrading this intersection is \$11.1 million. This includes ROW acquisition; see Chapter 4 for a discussion of ROW acquisition. Further improvements may be prudent to plan for further growth; however, they are not considered to contribute to the cost of this project since the improvements, as stated, create an acceptable intersection that will meet the ODOT mobility target.

W. Elm Avenue (OR 207)/N.W. 11th Street

Because this intersection was found to exceed its ODOT mobility target, improvements to the intersection would be needed. Since analysis found that the eastbound approach movements would be significantly increased, an eastbound right-turn lane would be required to enable the intersection to operate within acceptable capacity limits. The estimated project cost for this improvement is \$810,000.

W. Elm Avenue (OR 207)/N. 1st Street

With a projected volume to capacity ratio of 99 percent of the ODOT mobility target, this intersection would need enhanced to accommodate further traffic growth. The Kittelson technical memo indicates that this intersection would benefit from the construction of a southbound left-turn lane on N. 1st Place and construction of an eastbound right-turn lane on W. Elm Avenue. ODOT is aware that this intersection will need upgraded regardless of whether a new bridge is constructed. They have investigated upgrade options and completed preliminary designs. Based on those preliminary designs, in 2021 ODOT estimated the total project cost for the intersection improvements to be \$6,000,000.

Powerline Road/Bellinger Road

This intersection will change from a rural intersection to a much more heavily used intersection. Since southbound left-turn movements and westbound right-turn movements are expected to substantially increase, construction of a dedicated southbound left-turn lane and a dedicated westbound right-turn lane would be required. The estimated project cost for these upgrades is \$295,000.

I-82/Powerline Road Interchange

Construction of a bridge on W. Elm Avenue is anticipated to generate increased traffic on the I-82 interchange. While improvements to the interchange are beyond the scope of this project, Kittelson & Associates, Inc., recommends a study effort called an Interchange Area

Management Plan (IAMP) be completed. The IAMP would formally identify a preferred interchange improvement plan and recommend improvements to the I-82/Powerline Road interchange. The estimated fee for developing an IAMP is \$140,000.

Travel Corridor

The Kittelson technical memo states that a two-lane travel corridor (one lane each way) would be sufficient to accommodate projected traffic demands. Exceptions would occur at intersections requiring additional lanes for turning movements, which were previously discussed. Although a full buildout to an urban major arterial standard per City of Hermiston Standard Drawing ST07, as shown in Appendix C, is not required to meet projected traffic demands, it is prudent to plan for future roadway expansion since this travel corridor would essentially operate like a major arterial. As such, it is recommended that sufficient ROW be acquired as part of this project to plan for future road expansion. ROW acquisition for this purpose is discussed in Chapter 4, and anticipated cultural and environmental requirements are discussed in Chapter 5.

W. Elm Avenue Extension and Bellinger Road are currently gravel roads with no curb, gutter, or sidewalk. Powerline Road is currently a chip-sealed road and also has no curb, gutter, or sidewalk. The rest of the travel corridor along W. Elm Avenue is paved in asphalt concrete pavement (ACP) and has a mixture of areas with and without curb, gutter, and sidewalk. For the purposes of this PER, it was assumed that the travel corridor would be improved to two 12-foot travel lanes and two 5-foot bike lanes with curb/gutter and a 5-foot sidewalk. Full street improvements would be needed beginning just west of the intersection of N.W. 11th Street and W. Elm Avenue. From there, improvements would be needed west to Powerline Road and then north to the I-82/Powerline Road interchange. Furthermore, infill of curb, gutter, and sidewalk would be needed in some areas along W. Elm Avenue east of N.W. 11th Street. The estimated project cost for these road improvements, including ROW acquisition, is \$11.1 million.

C. Punkin Center Road Option

Bridge Structure

Based on a desktop review, the OHW width for the river at the Punkin Center Road Bridge crossing is approximately 100 feet. The bridge is anticipated to cross the river at an approximate 35-degree skew. The floodway measured along the line of travel is approximately 350 feet wide. The spans for this bridge were chosen based on several criteria. First, the spans had to be symmetrical, which usually lends to a more economical design. Second, the bridge had to cross the entire floodway to minimize environmental impacts and ensure that travel ways would not be damaged during a typical flood event. Third, a single segment of the bridge had to completely span across the OHW width portion of the river (no piers or abutments were assumed to be located in the OHW area). The bridge must also span 2.2 times the OHW width as discussed at the beginning of this chapter, and 2.2 times the OHW width of 100 feet projected onto the skewed bridge travel corridor is approximately 270 feet. Since the total floodway width of approximately 350 feet is much larger than the required span of 270 feet, both span requirements are easily met by spanning the entire floodway. Three equal 200-foot spans were selected to meet these criteria, as shown on Figure 3-2. The total span of this bridge option is estimated to be 600 feet.

The approximate bridge profile as shown on Figure 3-2 was determined based on two criteria. First, the bottom (low chord) of the bridge had to be a minimum of 3 feet above the 100-year flood elevation. This is an ODOT requirement that is typically deferred to when the local jurisdiction does not have a specific separation requirement. The 3 feet of separation provides sufficient space to enable floating debris to pass under the bridge in the 100-year flood event. Second, the approximate bridge profile was selected to effectively daylight into the existing topography to provide smooth passage for vehicular traffic and to minimize the amount of cut/fill that will be required.

Bridge width was determined assuming two 12-foot travel lanes, two 6-foot wide bike/shoulder lanes, and two 6-foot wide sidewalks. This totals a width of 48 feet. Note that the bike/shoulder lane is one foot wider on the bridge than the rest of the travel corridor for the same reasons discussed previously under the Elm Avenue Option section above. Although a bridge type was not specifically selected as part of this PER, the bridge is anticipated to be either prestressed concrete or steel. Bridge type should be selected when a preliminary design is completed as part of the actual design process. A planning-level cost of \$490 per square foot was used to conservatively estimate bridge project costs. The total area of the bridge, given a span of 600 feet and a width of 48 feet, is 28,800 square feet. At \$490 per square foot, the total cost for the bridge structure is estimated to be approximately \$14.1 million. Adding approximately \$450,000 for the earthwork required for the roadway on either side of the river brings the total estimated project cost to \$14.5 million.

Although this PER assumes that the bridge crosses the river at a 35-degree skew to keep the travel corridor straight, other options exist where the bridge could cross at a smaller skew provided that the travel corridor geometry is permitted to deviate from a straight line. Decreasing the skew angle would lead to a shorter total bridge span and a reduced cost for the bridge structure. Although this could generate some cost savings, the floodway width would be decreased by a much smaller amount than is the case with the Elm Avenue option.

Roadway Improvements

Chapter 2 discusses the impacts to the transportation system from an analysis perspective. Based on those analyses, recommendations to improve the travel corridor were made. Many of the recommended improvements occur at intersections. Additional improvements are needed where no ROW currently exists. Improvements to the driving surface will also be required to increase roadway durability to account for increased traffic.

Discussions with ODOT revealed that this travel corridor is unlikely to ever become a state highway. They indicated that the only way the travel corridor could ever become a state route would be if Umatilla County took over other existing state routes as a trade for ODOT taking over the Punkin Center Road option travel corridor. Because this is highly unlikely to occur, City of Hermiston standards were used as a basis for recommended travel corridor improvements.

Intersections

U.S. 395/Punkin Center Road

Although analysis of this intersection did not project it to exceed capacity, it was found that significant delays were expected for eastbound right-turn movements. To better manage this issue, an eastbound right-turn lane is recommended. The property on the southwest corner of the intersection is currently vacant. Utility pedestals would likely need to be relocated as part of this project. Depending on the design, the new turn lane could pass behind the traffic signals to preclude their relocation and save cost. The estimated project cost for these improvements is \$260,000, not including ROW acquisition.

U.S. 395/W. Elm Avenue (OR 207)

Similar to the Elm Avenue option, this intersection was found to exceed capacity regardless of a bridge being constructed in line with W. Punkin Center Road. Although construction of a bridge under the Punkin Center Road option is estimated to worsen capacity deficiencies, when compared to the Elm Avenue option, capacity deficiencies are only slightly affected. Regardless of whether a bridge is constructed, this intersection would need the same improvements as if the bridge for the Elm Avenue option were constructed. These improvements were discussed in the Elm Avenue Option section above and, similar to the Elm Avenue option, they would result in satisfactory traffic conditions. However, unlike the Elm Avenue option, the intersection would not operate at full capacity after the completion of these improvements, which means that some amount of future growth could be accommodated. ROW acquisition would be the same as for the Elm Avenue option and would be costly. Because the intersection would need upgrading regardless of whether the Punkin Center Bridge is constructed, and because the bridge would have only a slight effect on traffic congestion, it is anticipated that ODOT would need to upgrade this intersection. The estimated project cost for upgrading this intersection is \$11.0 million. This includes ROW acquisition; see Chapter 4 for a discussion of ROW acquisition.

Umatilla River Road/W. Punkin Center Road

Extending the existing W. Punkin Center Road west to the bridge construction site would form a new intersection at Umatilla River Road and would replace the existing intersection of Cooney Lane and Umatilla River Road. A conceptual illustration of this intersection is shown in Exhibit A (page 22) of the Kittelson technical memo. This illustration depicts dedicated left-turn lanes and shared through/right-turn lanes on all approaches. The illustration also shows an S-curve travel corridor between the bridge and the current end of W. Punkin Center Road. Based on projected traffic movements, volumes, and a planning-level signal warrant analysis, this intersection would need traffic control measure beyond two-way stop control. Installing a traffic signal would be the most likely option. A roundabout could be considered, but the proximity to the railroad would make this difficult without additional ROW acquisition and the demolition of some adjacent residential structures. Regardless of which traffic control measures are constructed, the Union Pacific Railroad (UPRR) crossing will need to be upgraded and Cooney Lane will need to be modified to intersect the new extension of W. Punkin Center Road. Exhibit A in the Kittelson technical

memo shows the railroad crossing remaining in the same location. If alternative alignments are considered, a new railroad crossing may be needed at a different location than the existing crossing. For each new railroad crossing constructed, the UPRR requires that three other existing crossings be closed. For this reason, it would be disadvantageous to construct a new crossing. The alignment of the proposed Punkin Road extension between Sunshine Lane and the railroad is discussed later in this chapter, in the Travel Corridor section, as is the modification to Cooney Lane. The estimated project cost of a traffic signal-controlled intersection with turn lanes as described and an upgrade to the railroad crossing is \$4.8 million. This cost may vary depending on the final design configuration of this intersection.

Powerline Road/Country Lane

As previously stated, ODOT requires intersections to be located at least 0.25 mile away from any highway interchange. Currently, the intersection of Powerline Road and Country Lane is approximately 275 feet south of the I-82/Powerline Road interchange. The intersection would need to be moved farther south to meet ODOT's minimum spacing requirements, which would entail realigning a section of Country Lane to deflect traffic farther south before intersecting Powerline Road. Because of the complexity resulting from proximity to the highway interchange and because this would involve many different stakeholders (potentially including the City of Hermiston, ODOT, private landowners, Umatilla County, and the City of Umatilla), Kittelson & Associates, Inc., recommends a separate IAMP study effort. The IAMP would formally identify a preferred interchange improvement plan including the design, timing, and ROW needs for a relocated intersection. Estimated fees for developing an IAMP are discussed in the I-82/Powerline Road Interchange section for the Elm Avenue option.

Although many options exist, one potential concept for the realignment of Country Lane to create a new intersection is illustrated in Exhibit B (page 24) of the Kittelson technical memo. This configuration includes closing off the existing Country Lane/Powerline Road intersection and realigning Country Lane to create a new intersection at Powerline Road approximately 1,500 feet south of the I-82 interchange. Regardless of where the new intersection along Powerline Road is created, a southbound left-turn lane and westbound right- and left-turn lanes would be needed. Although an IAMP would be needed to formally assess cost, a planning-level project cost estimate is \$290,000.

I-82/Powerline Road Interchange

Similar to the Elm Avenue option, construction of a bridge on W. Punkin Center Road is anticipated to generate increased traffic on the I-82 interchange. While improvements to the interchange are beyond the scope of this project, Kittelson & Associates, Inc., recommends an IAMP study effort be completed. The IAMP would include recommended improvements to the interchange and formally identify a preferred interchange improvement plan. The main difference from the Elm Avenue option is that this IAMP would provide recommendations for improvements to the Powerline Road/Country Lane intersection. The estimated fee for developing an IAMP is \$140,000.

Travel Corridor

The Kittelson technical memo states that a two-lane travel corridor (one lane each way) is sufficient to accommodate projected traffic demands. Exceptions would occur at intersections requiring additional lanes for turning movements, which were discussed previously. Although a full buildout to an urban major arterial standard per City of Hermiston Standard Drawing ST07 (see Appendix C of this PER) is not required to meet projected traffic demands, it would be prudent to plan for future roadway expansion since this travel corridor would essentially operate like a major arterial. As such, it is recommended that sufficient ROW be acquired as part of this project to plan for future road expansion. ROW acquisition for this purpose is discussed in Chapter 4.

W. Punkin Center Road from U.S. 395 to Sunshine Lane is currently a chip seal road. An approximately 1,130-foot long half street improvement was completed on the southern half of W. Punkin Center Road as part of the Overlook Ridge subdivision. The half street improvement included ACP with curb, gutter, and sidewalk. This is the only section along the entire corridor that does not require improvements.

From the intersection of Sunshine Lane and W. Punkin Center Road, W. Punkin Center Road would need to be extended west through what is currently private property. The extension of W. Punkin Center Road would continue until it crosses the railroad intersection near Umatilla River Road, where the intersection of Cooney Lane and Umatilla River Road currently exists. Requirements for this intersection and the railroad crossing were previously discussed. Cooney Lane would need to be reconfigured to connect to the new extension of W. Punkin Center Road. The road would then cross the Umatilla River Bridge and continue west until it connects to Country Lane. This entire section of new road would be built on what is now almost exclusively privately owned property. Since no road currently exists in these locations, full improvements would be required as previously discussed.

Once connected to Country Lane, the travel corridor would continue west along Country Lane until it intersects Powerline Road. From that intersection it would turn north and continue until it connects to the I-82/Powerline Road interchange. Country Lane is currently a gravel road, and Powerline Road is a chip seal road. Both roads would need to be improved to the standards discussed above. The estimated project cost for upgrading the travel corridor to two asphalt paved travel lanes with paved shoulders, curb, gutter, and sidewalk, is \$12.6 million, including ROW acquisition.

D. Summary

This chapter discusses needed improvements to the transportation system. The bridge structure is the critical improvement that drives all other necessary system improvements. The OHWE and the OHW width were defined and their effect on bridge span was discussed. The floodplain and floodway were characterized and their impacts on bridge span were examined. For both bridge options, it was determined that spanning the floodway utilizing a multi-span structure with one single bridge section clear-spanning the OHW width is recommended.

It was noted that FEMA is currently in the process of analyzing the Umatilla River reach to revise their FIS and corresponding FIRM. Changes to floodplains in the potential project areas are not likely to be

significant. Because the potential project areas are located within the floodplain and floodway, FEMA development standards will apply. Because intermediate bridge supports will need to be located within the floodway, a no-rise certification would typically be required; however, it is unlikely that a no-rise situation can be demonstrated for either bridge option. Thus, a hydraulic analysis will need to be completed along with a CLOMR and a LOMR, regardless of which bridge option is selected.

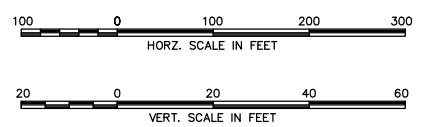
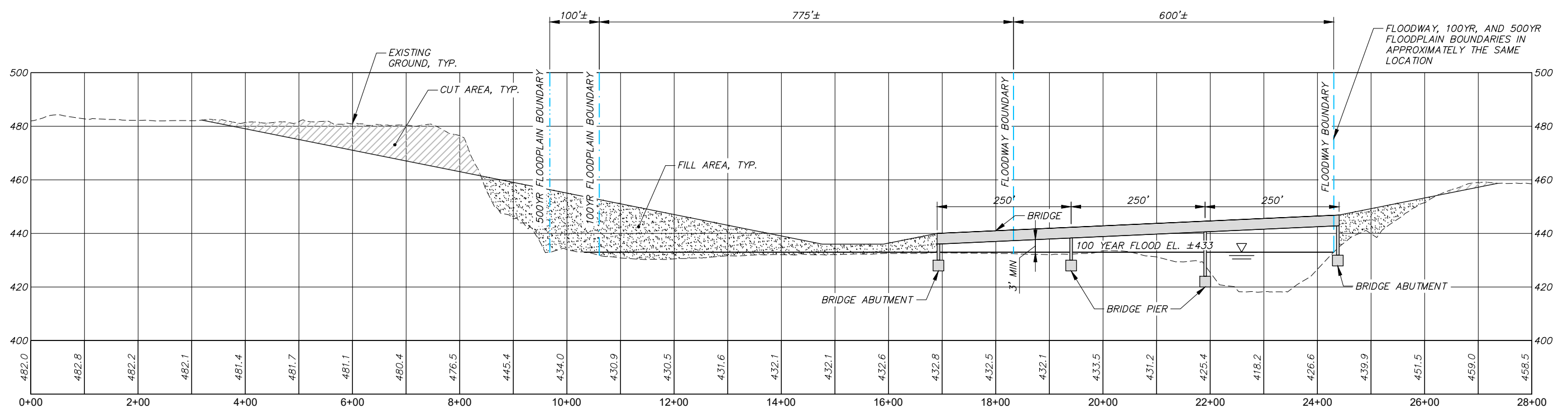
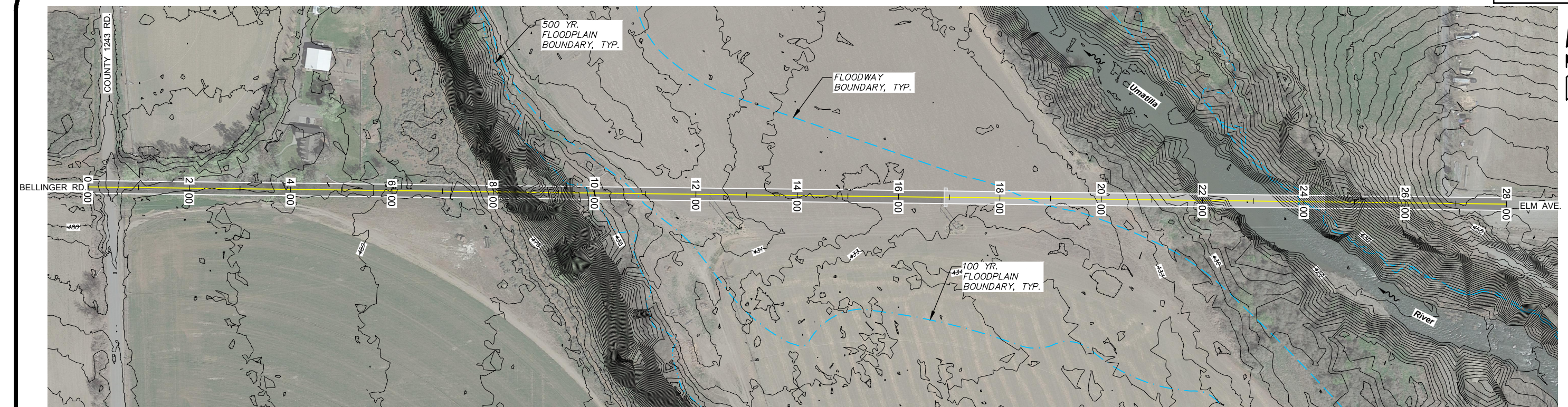
For the Elm Avenue option, the bridge is expected to be approximately 750 feet long. Upgrades to many of the intersections along the travel corridor are required. The majority of the travel corridor itself would need to be upgraded to current standards with two travel lanes, paved shoulders, and curb, gutter, and sidewalk on each side of the corridor. Specific upgrades and associated costs are summarized on the table below.

For the Punkin Center Road option, the bridge is expected to be approximately 600 feet long. Upgrades to many of the intersections along the travel corridor are required, and several intersections would need to be relocated or newly constructed. The existing railroad crossing would need to be upgraded. Most of the travel corridor itself would need to be upgraded to current standards with two travel lanes, paved shoulders, and curb, gutter, and sidewalk on each side of the corridor. Specific upgrades and associated costs are summarized on the table below.

TABLE 3-1
TRANSPORTATION SYSTEM IMPROVEMENTS SUMMARY

| Elm Avenue Option | | Punkin Center Road Option | |
|-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|--------------|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|--------------|
| Improvement | Cost | Improvement | Cost |
| <ul style="list-style-type: none"> Bridge (750 feet long) | \$19,400,000 | <ul style="list-style-type: none"> Bridge (600 feet long) | \$14,500,000 |
| U.S. 395/W. Elm Avenue (OR 207) <ul style="list-style-type: none"> Northbound right-turn lane, a southbound right-turn lane, and a westbound right-turn lane Improvements would meet mobility target but not provide room for future traffic growth ROW acquisition is costly (see Chapter 4) ODOT is aware this intersection will need upgraded regardless of bridge construction | \$11,000,000 | U.S. 395/W. Elm Avenue (OR 207) <ul style="list-style-type: none"> Northbound right-turn lane, a southbound right-turn lane, and a westbound right-turn lane Improvements would meet mobility target and allow for some future traffic growth ROW acquisition is costly (see Chapter 4) ODOT is aware that this intersection will need upgraded regardless of bridge construction | \$11,000,000 |
| W. Elm Avenue (OR 207)/N.W. 11th Street <ul style="list-style-type: none"> Eastbound right-turn lane | \$810,000 | U.S. 395/Punkin Center Road <ul style="list-style-type: none"> Eastbound right-turn lane recommended | \$260,000 |
| W. Elm Avenue (OR 207)/N. 1st Street <ul style="list-style-type: none"> Southbound left-turn lane and an eastbound right-turn lane ODOT is aware that this intersection needs upgraded regardless of bridge construction | \$6,000,000 | Umatilla River Road/W. Punkin Center Road <ul style="list-style-type: none"> Traffic-signal Dedicated left-turn lane and shared through/right-turn lane on all approaches Upgrade to railroad crossing | \$4,800,000 |

| Elm Avenue Option | | Punkin Center Road Option | |
|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|--------------------------|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|--------------------------|
| Improvement | Cost | Improvement | Cost |
| Powerline Road/Bellinger Road <ul style="list-style-type: none"> Southbound left-turn lane and westbound right-turn lane | \$295,000 | Powerline Road/Country Lane <ul style="list-style-type: none"> Intersection needs relocated per ODOT requirements | \$290,000 |
| I-82/Powerline Road Interchange <ul style="list-style-type: none"> Improvements are beyond the scope of this PER An IAMP would be required | \$140,000 (IAMP only) | I-82/Powerline Road Interchange <ul style="list-style-type: none"> Improvements are beyond the scope of this PER An IAMP would be required | \$140,000 (IAMP only) |
| Travel Corridor <ul style="list-style-type: none"> Corridor from I-82 to U.S. 395 upgraded to two 12-foot travel lanes, two 5-foot bike lanes, curb, gutter, and 5-foot sidewalks | \$11,100,000 | Travel Corridor <ul style="list-style-type: none"> Corridor from I-82 to U.S. 395 upgraded or newly constructed to two 12-foot travel lanes, two 5-foot bike lanes, curb, gutter, and 5-foot sidewalks | \$12,600,000 |
| Total: | \$48,745,000 | Total: | \$43,590,000 |

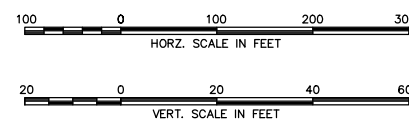
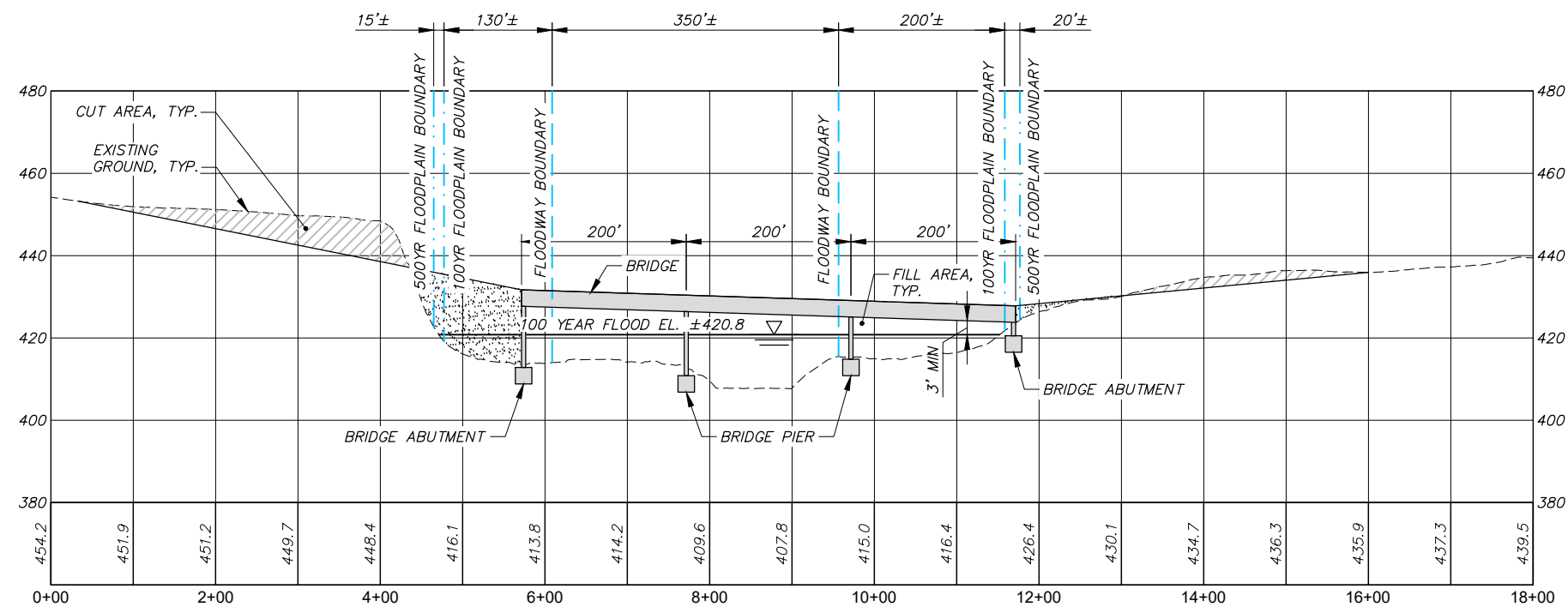
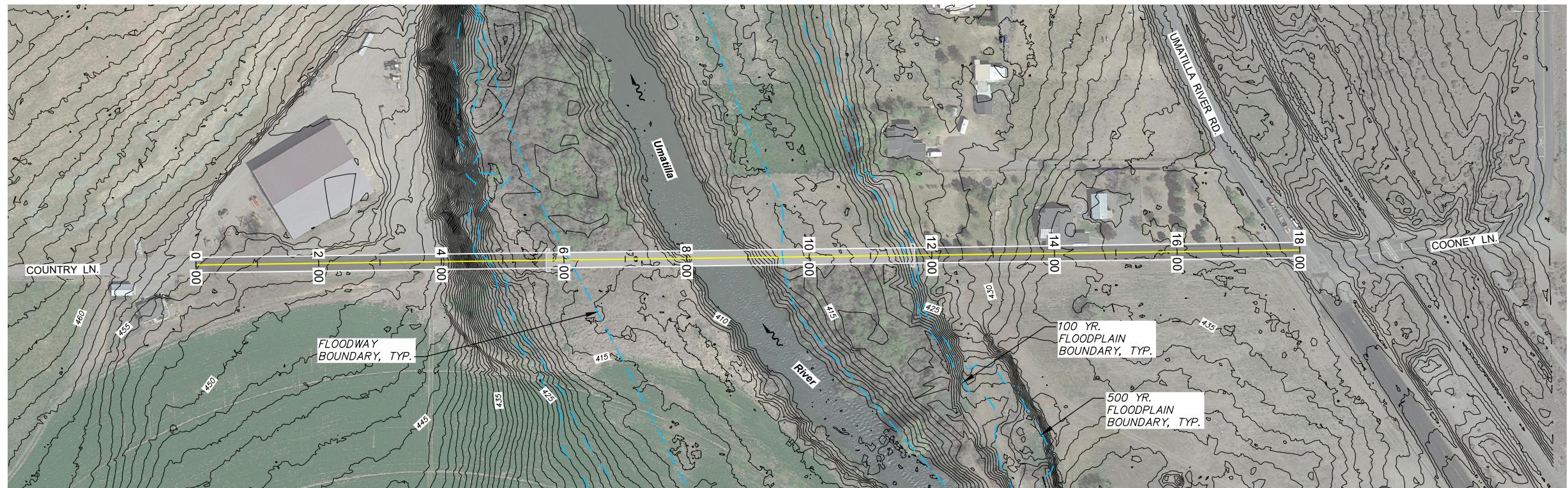


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RIVER CROSSING PLAN AND PROFILE
ELM AVENUE OPTION

FIGURE
3-1

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PRELIMINARY ENGINEERING REPORT
**RIVER CROSSING PLAN AND PROFILE
PUNKIN CENTER ROAD OPTION**

FIGURE
3-2

Chapter 4 - Land Use and Acquisition

A. Overview

Because a new bridge across the Umatilla River would fundamentally change traffic patterns and future land use, it is imperative that additional right-of-way (ROW) be acquired. ROW acquisition and careful planning are essential to successfully develop a new travel corridor across the Umatilla River that would accommodate future growth, increased traffic flows, and changes in land use. This chapter discusses needed ROW acquisition and existing land use. ROW acquisition required for intersection improvements is included in the estimated costs for improvements in Chapter 3.

The size and locations of properties discussed in this chapter were determined from the Umatilla County Assessor's maps. The information from the Umatilla County Assessor's website was assumed to be correct for the purposes of this Preliminary Engineering Report (PER). Figures 4-1A through 4-1D and 4-2A through 4-2C show ROW acquisition locations for properties adjacent to potential future travel corridors. Figures 4-3 and 4-4 summarize the cost of ROW acquisition from each property in tabular form.

B. Elm Avenue Option

Land and Right-of-Way Acquisition

The Elm Avenue option consists of approximately 3.5 miles of roadway stretching from Interstate 82 (I-82) to U.S. 395. According to the Umatilla County tax lot maps, the existing ROW within the City of Hermiston on Elm Avenue is 66 feet wide. The ROW in the County along Powerline Road and Bellinger Road is 40 feet wide. East of N.W. 7th Street, Elm Avenue is currently classified as an urban minor arterial. West of N.W. 7th Street, Elm Avenue is classified as a rural arterial. Because land use and development would be drastically impacted by construction of a new bridge, it is prudent to assume that the travel corridor would eventually become an urban major arterial and to plan for future road expansion.

The City of Hermiston's Transportation System Plan (TSP) and City of Hermiston Standard Drawing ST07 (see Appendix C) indicate that ROW widths for an urban major arterial are 86 feet to 98 feet. Assuming that future sidewalk will abut the back of curb, a ROW width of approximately 90 feet would be needed. Because the proposed ROW is larger than the existing ROW, additional ROW would need to be acquired. Furthermore, there are areas where no ROW currently exists, and completely new ROW would need to be acquired. Figures 4-1A through 4-1D show the Elm Avenue travel corridor from I-82 to U.S. 395. Additional ROW would need to be acquired from most tax lots adjacent to this travel corridor as shown on these figures.

The tax lot numbers shown on Figures 4-1A through 4-1D correspond with the tax lots listed on Figure 4-3. Figure 4-3 documents how much area would need to be acquired from each tax lot. Approximate costs for ROW acquisition from each lot are also calculated. For the purposes of estimating costs, current real market values for each property were used. Real market values were divided by total tax lot area to estimate a price per square foot. That price per square foot was then multiplied by the area of ROW that would be acquired from that tax lot. A multiplier of 1.5 was used

to increase the cost estimates for acquiring the ROW. The multiplier is not intended to increase the estimate of the actual sale price of the land; rather, it is intended to account for other factors in the land acquisitions process including, but not limited to, legal fees, administrative costs, coordination efforts, property functionality conflicts, and other contingency items. The estimated total cost, in 2022 dollars, for ROW acquisition for the Elm Avenue option is \$2.27 million.

In addition to road widening, the Oregon Department of Transportation (ODOT) has indicated that if the Elm Avenue option is used, the intersection at U.S. 395 and W. Elm Avenue would need to be upgraded significantly. This is further discussed in Chapter 3. Additional space would be needed in an already tight intersection. Likely, the only viable way to acquire the space necessary to upgrade the intersection would be to acquire the properties on all four corners of the intersection. McDonald's is located on Tax Lot 4N2802C001402, which has a real market value of \$1,677,470. Shiki Hibachi Sushi is located on Tax Lot 4N2811BB00600, which is currently owned by Wadekamper Investments, LLC, and has a real market value of \$531,700. Starbucks is located on Tax Lot 4N2810AA00300, which is currently owned by Legacy Pacific, LLC, and has a real market value of \$794,720. Jack in the Box is located on Tax Lot 4N2803D002502, which is also currently owned by Legacy Pacific, LLC, and has a real market value of \$753,640. Applying the same 1.5 multiplier as previously mentioned, the cost of acquiring these four properties amounts to approximately \$5.64 million.

Chapter 3 indicates that right-turn lanes would be needed for the northbound, southbound, and westbound approaches at the intersection of U.S. 395 and W. Elm Avenue. It is possible that the required design length for the northbound and southbound right-turn lanes would exceed the existing dimensions of the properties discussed in the previous paragraph. This PER assumes turn lane lengths are capped by the existing corner property dimensions. The cost estimate in Chapter 3 reflects this assumption.

Land Use (Zoning)

Land within the project corridor has various zoning designations, as the corridor encompasses the City of Hermiston, the Hermiston urban growth boundary (UGB), and Umatilla County. Figure 4-5A, City of Hermiston Zoning, shows the zoning designations of the land within city limits; Figure 4-5B, Umatilla County Zoning, shows the zoning designations of the land outside city limits and the UGB; and Figure 4-5C, Comprehensive Land Use Map, shows the zoning designations of the land within the UGB but outside the city limits. The zoning designations within the project corridor includes Exclusive Farm Use (EFU)-20, EFU-40, Open Space (OS), City Future Residential (F-R), City Multi-Structure Residential (R-4), City Outlying Commercial (C-2), City Low Density Residential (R-1), and City Light Industrial (M-1).

One advantage of the Elm Avenue option is that it would provide for increasing development along a travel corridor that is more directly centrally connected to the City of Hermiston. The City of Hermiston and Umatilla County would need to carefully plan for increased development on the west side of the Umatilla River. Permitting, environmental, and cultural resource requirements are discussed in Chapter 5.

C. Punkin Center Road Option

Land and Right-of-Way Acquisition

The Punkin Center Road option consists of approximately 2.5 miles of roadway stretching from I-82 to U.S. 395. According to the Umatilla County tax lot maps, the existing ROW on Punkin Center Road is 66 feet wide. The existing ROW in the County along Cooney Lane is 60 feet wide, and Country Lane is 40 feet wide. Punkin Center Road is currently classified as an urban major collector. Country Lane is classified as a rural arterial. Because land use and development would be drastically impacted by the construction of a new bridge, it is prudent to assume that the travel corridor would eventually become an urban major arterial and plan for future road expansion.

The City of Hermiston's TSP and City of Hermiston Standard Drawing ST07 (see Appendix C) indicate that ROW widths for an urban major arterial are 86 feet to 98 feet. Assuming that future sidewalk would abut the back of curb, a ROW width of approximately 90 feet would be needed. Because the proposed ROW is larger than the existing ROW, additional ROW would need to be acquired. Furthermore, there are areas where no ROW currently exists, and completely new ROW would need to be acquired. Figures 4-2A through 4-2C show the Punkin Center Road option travel corridor. Additional ROW would need to be acquired from most tax lots that are adjacent to this travel corridor, as shown on these figures.

The tax lot numbers shown on Figures 4-2A through 4-2C correspond with the tax lots listed on Figure 4-4. Figure 4-4 documents how much area would need to be acquired from each tax lot. Approximate costs for ROW acquisition from each lot are also calculated. The same process used to estimate ROW acquisition costs for the Elm Avenue option was used for the Punkin Center Road option. The estimated total cost, in 2022 dollars, for ROW acquisition for the Punkin Center Road option is \$3.65 million.

Although ODOT did not explicitly indicate the intersection at U.S. 395 and W. Elm Avenue would need to be upgraded as a result of constructing a bridge in line with W. Punkin Center Road, the Kittelson technical memo shows that the intersection would need to be upgraded as discussed in Chapter 3. Property acquisition at this intersection would cost the same as it would for the Elm Avenue option (approximately \$5.64 million). The main difference from the Elm Avenue option is that the bridge for the Punkin Center Road option would not be a significant catalyst for increased traffic congestion. Forecasted population growth and thus an increase in the number of vehicles on public roadways contribute significantly more to traffic congestion at this intersection than does bridge construction for the Punkin Center Road option. See Chapters 2 and 3 for further discussion on this topic. As such, there is potential for these intersection upgrades to be excluded from the overall bridge project costs. However, costs for upgrades to this intersection are still included with the Punkin Center Road option for planning purposes.

Land Use (Zoning)

Land within the project corridor has various zoning designations, as the corridor encompasses the City of Hermiston, the Hermiston UGB, and Umatilla County. Figure 4-5A, City of Hermiston Zoning, shows the zoning designations of the land within city limits; Figure 4-5B, Umatilla County Zoning, shows the zoning designations of the land outside of the city limits and the UGB; and Figure 4-5C,

Comprehensive Land Use Map, shows the zoning designations of the land within the UGB but outside the city limits. The zoning designations within the project corridor includes EFU-20, EFU-40, Rural Residential-2 (RR-2), Retail/Service Commercial (RSC), Light Industrial (LI), Low Density Residential (L), Medium Density Residential/Mobile Home (MH), City Multi-Structure Residential (R-4), Commercial (C), City Low Density Residential (R-1), and City Future Industrial (F-I).

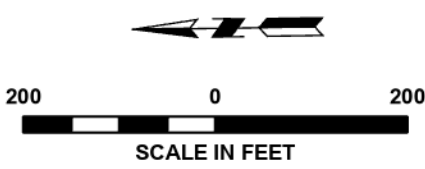
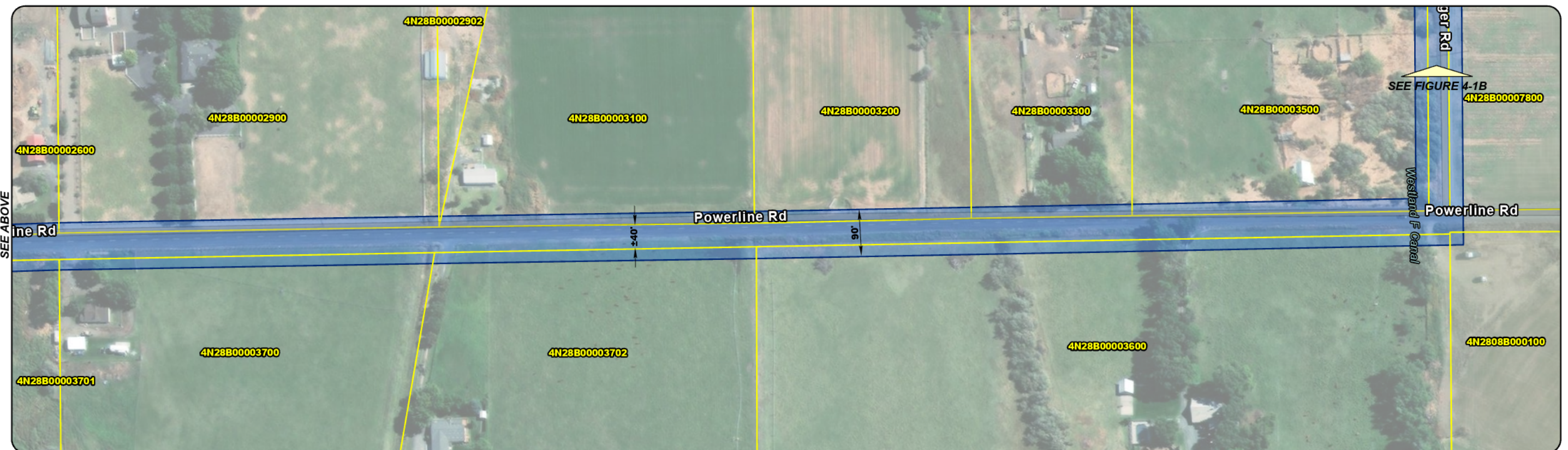
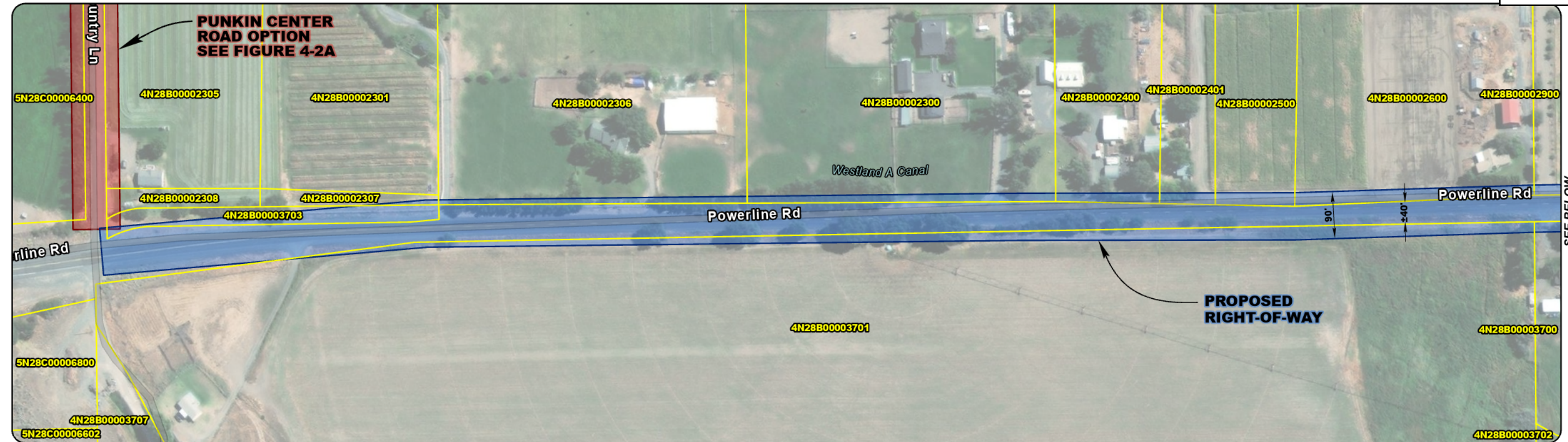
A disadvantage of the Punkin Center Road option is that it provides for increasing development along a travel corridor that is not centrally connected to the City of Hermiston, although this would become less of an issue as the northwestern corner of Hermiston within the UGB continues to develop. The City of Hermiston and Umatilla County would need to carefully plan for increased development along the Punkin Center Road option travel corridor if this option is selected. Permitting, environmental, and cultural resource requirements are discussed in Chapter 5.


D. Summary

TABLE 4-1
LAND USE AND ACQUISITION SUMMARY¹

| Elm Avenue Option | Punkin Center Road Option |
|-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| <ul style="list-style-type: none"> Travel corridor is linked more closely to the city center. More ROW acquisition (3.5-mile travel corridor) than the Punkin Center Road option High ROW acquisition costs around the OR 207/ U.S. 395 intersection (\$5.64 million), which includes several businesses | <ul style="list-style-type: none"> Less ROW acquisition (2.5-mile travel corridor) than the Elm Avenue option High ROW acquisition costs around the OR 207/ U.S. 395 intersection (\$5.64 million), which includes several businesses, could be excluded from the bridge project Encourages development in an area not closely linked to the city center |

¹ See Table 3-1 for a summary of estimated costs for both land acquisition and transportation system improvements.



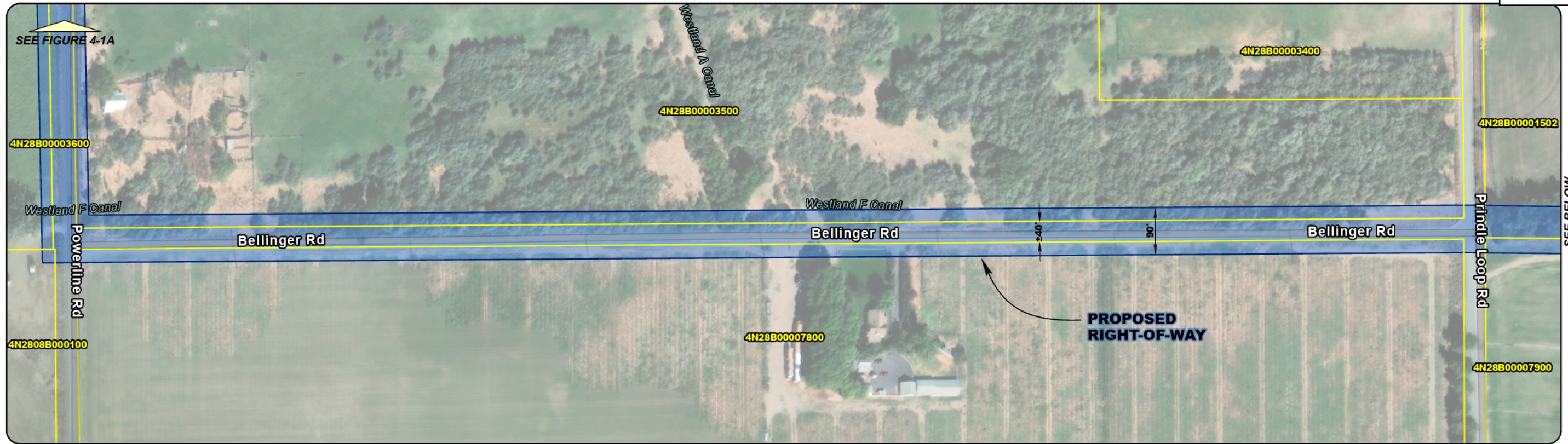


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TAX LOT MAP
ELM AVENUE OPTION

FIGURE
4-1A

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ELM AVENUE OPTION

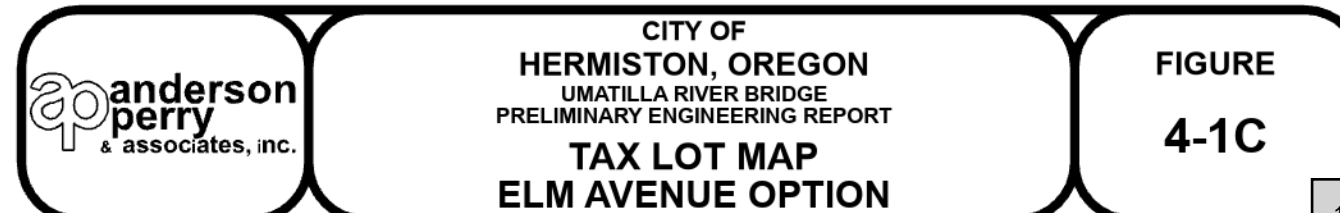
FIGURE
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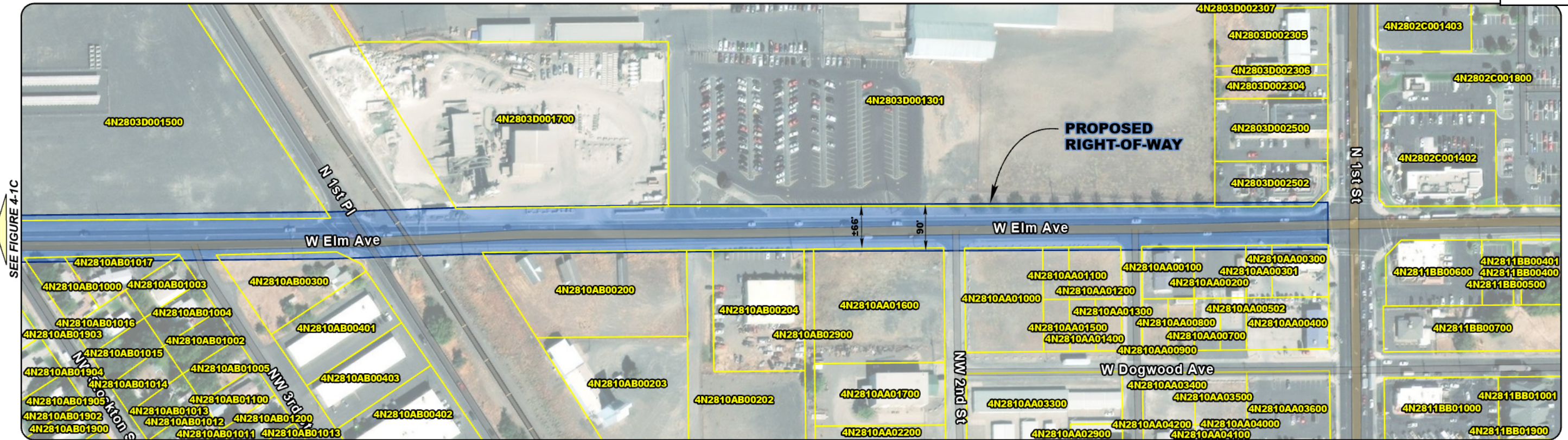
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SEE FIGURE 1.10



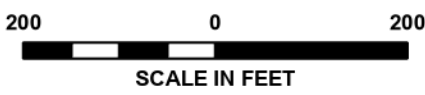
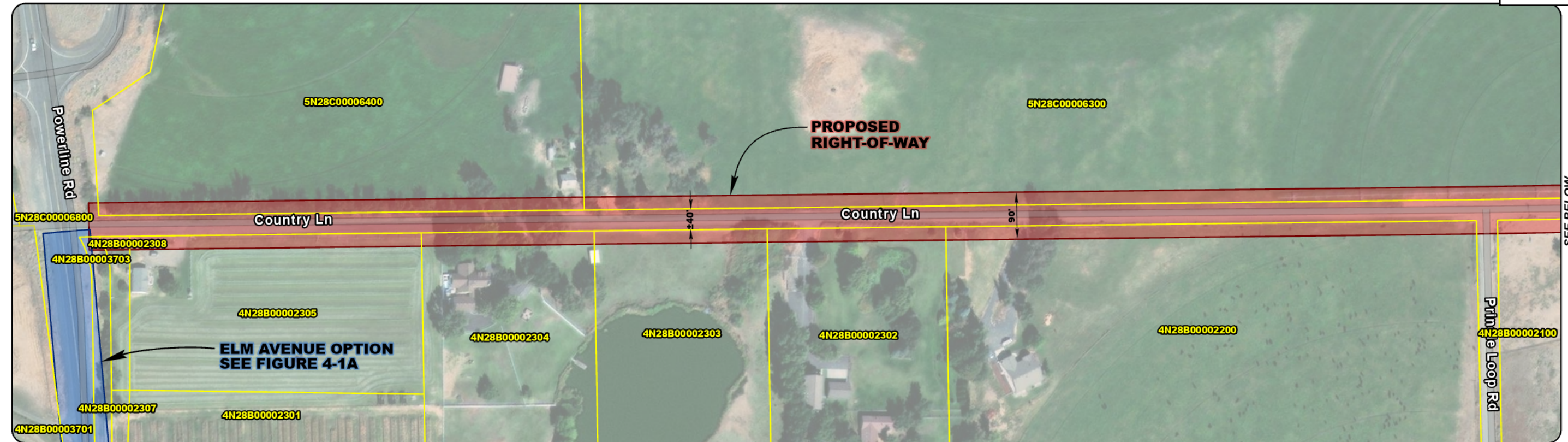
\\GIS\B6\gis\projects\Hermiston\736-123 UmatillaRiverBridge\736-123 UmatillaRiverBridge\PER.aprx, PER-736-123-FIG4-1D TaxlotElm4, 6/23/2022 1:58 PM, dchristman





**CITY OF
HERMISTON, OREGON**
UMATILLA RIVER BRIDGE
PRELIMINARY ENGINEERING REPORT
**TAX LOT MAP
ELM AVENUE OPTION**

**FIGURE
4-1D**



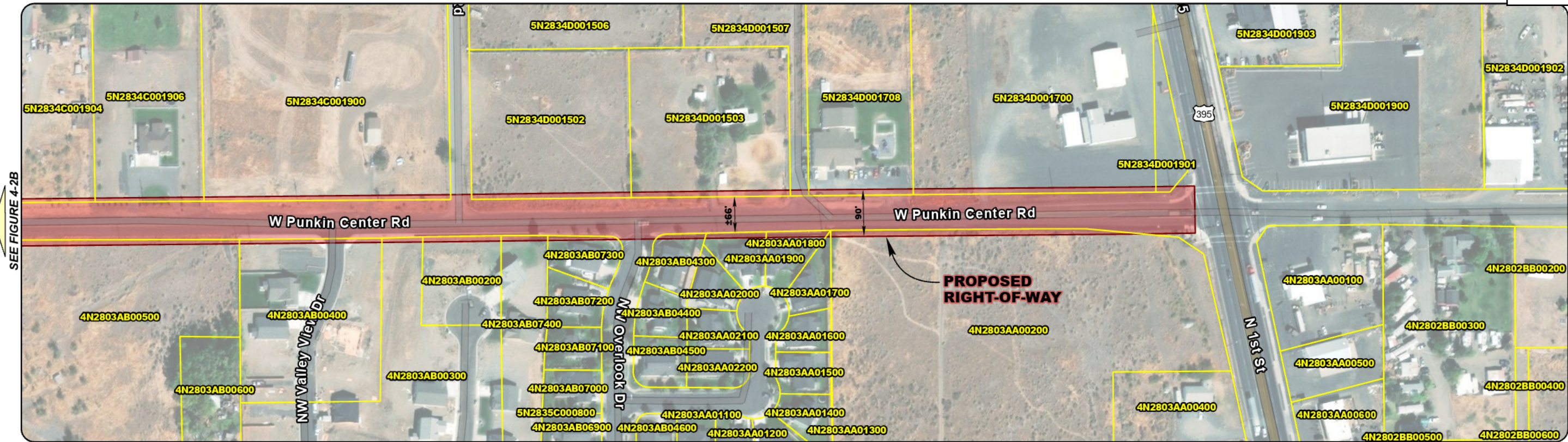
CITY OF
HERMISTON, OREGON
UMATILLA RIVER BRIDGE
PRELIMINARY ENGINEERING REPORT
TAX LOT MAP
PUNKIN CENTER ROAD OPTION

FIGURE
4-2A

\\GIS\GIS\projects\Hermiston\736-123 UmatillaRiverBridge\736-123 UmatillaRiverBridge\736-123 FIG-4-2A TaxlotPC1, 6/23/2022 2:08 PM, dchristman

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\\GIS\Projects\Hermiston\736-123 UmatillaRiverBridge\736-123 UmatillaRiverBridge\PER.aprx, PER-736-123-FIG-2C TaxLotPC3, 6/23/2022 2:07 PM, dchristman

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anderson
perry
& associates, inc.

CITY OF
HERMISTON, OREGON
UMATILLA RIVER BRIDGE
PRELIMINARY ENGINEERING REPORT

TAX LOT MAP
PUNKIN CENTER ROAD OPTION

FIGURE
4-2C

154

CITY OF HERMISTON, OREGON
UMATILLA RIVER BRIDGE
PRELIMINARY ENGINEERING REPORT
ELM AVENUE OPTION RIGHT-OF-WAY ACQUISITION COST

| Tax Lot Map # | Property | | Property Width Required (ft) ² | Area Required (SF) ³ | Real Market Value ⁴ | Tax Lot Size ⁵ | | Real Market Value per SF ⁶ | Estimated Cost ⁷ |
|---------------|---------------------|--------------------------------------|----------------------------------------------|------------------------------------|-----------------------------------|---------------------------|-----------|------------------------------------------|--------------------------------|
| | Account # | Frontage Length (ft) ¹ | | | | (Acres) | (SF) | | |
| 4N28B00001500 | 117258 | 300 | 45 | 13,500 | \$ 263,610 | 19.27 | 839,401 | \$ 0.31 | \$ 6,400 |
| 4N28B00002300 | 117269 | 540 | 25 | 13,500 | 422,370 | 20.39 | 888,188 | 0.48 | 9,700 |
| 4N28B00002400 | 117278 ⁸ | 240 | 25 | 6,000 | | | | 3.17 | 28,600 |
| 4N28B00002401 | 117280 | 100 | 25 | 2,500 | 165,650 | 1.20 | 52,272 | 3.17 | 11,900 |
| 4N28B00002500 | 117281 | 300 | 25 | 7,500 | 26,620 | 4.93 | 214,751 | 0.12 | 1,400 |
| 4N28B00002600 | 117282 | 360 | 25 | 9,000 | 237,540 | 14.78 | 643,817 | 0.37 | 5,000 |
| 4N28B00002900 | 117286 | 690 | 25 | 17,250 | 604,960 | 20.37 | 887,317 | 0.68 | 17,700 |
| 4N28B00003100 | 117289 | 635 | 25 | 15,875 | 198,300 | 16.21 | 706,108 | 0.28 | 6,700 |
| 4N28B00003200 | 117290 | 326 | 25 | 8,150 | 162,270 | 29.55 | 1,287,198 | 0.13 | 1,600 |
| 4N28B00003300 | 117292 | 340 | 25 | 8,500 | 181,230 | 14.85 | 646,866 | 0.28 | 3,600 |
| 4N28B00003500 | 117294 | 3,314 | 25 | 82,850 | 238,560 | 33.36 | 1,453,162 | 0.16 | 20,500 |
| 4N28B00003600 | 117295 | 1,320 | 25 | 33,000 | 381,930 | 39.39 | 1,715,828 | 0.22 | 11,100 |
| 4N28B00003700 | 117297 | 720 | 25 | 18,000 | 287,220 | 8.78 | 382,457 | 0.75 | 20,300 |
| 4N28B00003701 | 117298 | 2,500 | 25 | 62,500 | 472,380 | 78.31 | 3,411,184 | 0.14 | 13,000 |
| 4N2808B000100 | 117352 | 25 | 25 | 625 | 358,780 | 39.39 | 1,715,828 | 0.21 | 200 |
| 4N28B00007800 | 117389 | 2,670 | 25 | 66,750 | 491,510 | 57.71 | 2,513,848 | 0.20 | 19,600 |
| 4N28B00007900 | 117391 | 800 | 25 | 20,000 | 132,720 | 30.49 | 1,328,144 | 0.10 | 3,000 |
| 4N2804D002000 | 117423 | 222 | 12 | 2,664 | 754,840 | 0.83 | 35,969 | 20.99 | 83,900 |
| 4N2803C000900 | 118727 | 277 | 12 | 3,324 | 1,760,560 | 5.33 | 232,175 | 7.58 | 37,900 |
| 4N2803D001301 | 118739 | 1,049 | 12 | 12,588 | 7,213,820 | 40.32 | 1,756,339 | 4.11 | 77,600 |
| 4N2803D001500 | 118747 | 942 | 12 | 11,304 | 305,320 | 5.33 | 232,175 | 1.32 | 22,300 |
| 4N2803D001700 | 118751 | 379 | 12 | 4,548 | 315,180 | 4.12 | 179,467 | 1.76 | 12,000 |
| 4N2810AA00200 | 119476 | 100 | 12 | 1,200 | 168,660 | 0.23 | 10,000 | 16.87 | 30,400 |
| 4N2810AA00300 | 119478 | 108 | 12 | 1,296 | 794,720 | 0.25 | 10,836 | 73.34 | 142,600 |
| 4N2810AA00301 | 119480 | 50 | 12 | 600 | 66,120 | 0.11 | 5,000 | 13.22 | 12,000 |
| 4N2810AA01000 | 119495 | 150 | 12 | 1,800 | 221,800 | 0.69 | 30,000 | 7.39 | 20,000 |
| 4N2810AA01100 | 119496 | 50 | 12 | 600 | 41,200 | 0.11 | 5,000 | 8.24 | 7,500 |
| 4N2810AA01200 | 119498 | 100 | 12 | 1,200 | 82,400 | 0.23 | 10,000 | 8.24 | 14,900 |
| 4N2810AA01600 | 119504 | 250 | 12 | 3,000 | 157,550 | 1.26 | 55,000 | 2.86 | 12,900 |
| 4N2804D001600 | 119812 | 176 | 12 | 2,112 | 423,630 | 3.88 | 169,013 | 2.51 | 8,000 |
| 4N2804D001700 | 119813 | 273 | 12 | 3,276 | 58,640 | 1.65 | 71,874 | 0.82 | 4,100 |
| 4N2804D001701 | 119816 | 208 | 12 | 2,496 | 260,930 | 0.91 | 39,640 | 6.58 | 24,700 |
| 4N2810B000200 | 120050 | 1,275 | 12 | 15,300 | 2,006,350 | 5.06 | 220,415 | 9.10 | 209,000 |
| 4N2810AB00200 | 120250 | 402 | 12 | 4,824 | 76,430 | 1.29 | 56,192 | 1.36 | 9,900 |
| 4N2810AB00202 | 120263 | 25 | 12 | 300 | 196,820 | 2.29 | 99,752 | 1.97 | 900 |
| 4N2810AB00204 | 120270 | 225 | 12 | 2,700 | 438,020 | 0.94 | 40,946 | 10.70 | 43,400 |
| 4N2810AB00300 | 120273 | 260 | 12 | 3,120 | 117,940 | 0.59 | 25,530 | 4.62 | 21,700 |
| 4N2810AB01000 | 120304 | 90 | 12 | 1,080 | 78,830 | 0.21 | 9,148 | 8.62 | 14,000 |
| 4N2810AB01017 | 120352 | 441 | 12 | 5,292 | 101,530 | 0.25 | 10,890 | 9.32 | 74,100 |
| 4N2810AB01903 | 120398 | 20 | 12 | 240 | 282,710 | 0.27 | 11,976 | 23.61 | 8,500 |
| 4N2810AB02900 | 120440 | 18 | 12 | 216 | 5,380 | 0.42 | 18,295 | 0.29 | 100 |
| 4N2810AA00100 | 135304 | 100 | 12 | 1,200 | 202,720 | 0.23 | 10,000 | 20.27 | 36,500 |
| 4N2810B000100 | 135318 | 275 | 12 | 3,300 | 452,650 | 9.20 | 400,752 | 1.13 | 5,600 |
| 4N2810B000101 | 135320 | 1,000 | 12 | 12,000 | 5,001,500 | 25.79 | 1,123,412 | 4.45 | 80,200 |
| 4N28B00009000 | 135648 | 700 | 12 | 8,400 | 224,380 | 20.17 | 878,605 | 0.26 | 3,300 |
| 4N28B00009090 | 135650 | 1,300 | 12 | 15,600 | 200,140 | 19.19 | 835,916 | 0.24 | 5,700 |
| 4N28B00002306 | 137608 | 540 | 25 | 13,500 | 397,580 | 20.40 | 888,624 | 0.45 | 9,100 |
| 4N2803C001000 | 139652 | 812 | 12 | 9,744 | 2,465,500 | 14.54 | 633,362 | 3.89 | 56,900 |
| 4N28B00003702 | 142520 | 600 | 25 | 15,000 | 387,350 | 23.50 | 1,023,660 | 0.38 | 8,600 |
| 4N28B00001502 | 143077 | 495 | 65 | 32,175 | 786,990 | 31.75 | 1,383,030 | 0.57 | 27,500 |
| 4N2803C000801 | 144018 | 215 | 12 | 2,580 | 483,880 | 1.48 | 64,469 | 7.51 | 29,100 |
| 4N28B00001502 | 144264 | 1,620 | 45 | 72,900 | 390,900 | 150.67 | 6,563,185 | 0.06 | 6,600 |

| Tax Lot Map # | Account # | Property | | Area Required (SF) ³ | Real Market Value ⁴ | Tax Lot Size ⁵ | | Real Market Value per SF ⁶ | Estimated Cost ⁷ |
|-----------------------------|-----------|-----------------------------------|-------------------------------------------|---------------------------------|--------------------------------|---------------------------|-----------|---------------------------------------|-----------------------------|
| | | Frontage Length (ft) ¹ | Property Width Required (ft) ² | | | (Acres) | (SF) | | |
| 4N28B00002902 | 149975 | 25 | 25 | 625 | 14,210 | 2.82 | 122,839 | 0.12 | 200 |
| 4N2804D001802 | 154630 | 560 | 12 | 6,720 | 173,750 | 12.40 | 540,144 | 0.32 | 3,300 |
| 4N2803C000802 | 156146 | 704 | 12 | 8,448 | 2,735,620 | 30.50 | 1,328,580 | 2.06 | 26,100 |
| 4N2804D001601 | 156899 | 645 | 12 | 7,740 | 153,800 | 9.35 | 407,286 | 0.38 | 4,400 |
| 4N2803D002502 | 157407 | 188 | 12 | 2,256 | 753,640 | 0.42 | 18,294 | 41.20 | 139,500 |
| 4N2803C000806 | 157476 | 202 | 12 | 2,424 | 6,588,490 | 1.39 | 60,548 | 108.81 | 395,700 |
| 4N2803C000807 | 157477 | 195 | 12 | 2,340 | 313,840 | 1.33 | 57,935 | 5.42 | 19,100 |
| 4N2803C000810 | 159016 | 156 | 12 | 1,872 | 906,760 | 0.59 | 25,594 | 35.43 | 99,500 |
| 4N2804D002001 | 161108 | 222 | 12 | 2,664 | 257,980 | 0.85 | 37,026 | 6.97 | 27,900 |
| 4N2804D002002 | 161109 | 185 | 12 | 2,220 | 226,580 | 0.71 | 30,928 | 7.33 | 24,400 |
| 4N2804D001602 | 162114 | 295 | 12 | 3,540 | 2,099,090 | 1.95 | 84,917 | 24.72 | 131,300 |
| 4N2804D001603 | 162115 | 175 | 12 | 2,100 | 889,980 | 1.18 | 51,518 | 17.28 | 54,500 |
| 4N2804D001802 | 166033 | 281 | 12 | 3,372 | 173,750 | 12.40 | 540,144 | 0.32 | 1,700 |
| 4N2810AB00205 | 167471 | 25 | 12 | 300 | 113,070 | 1.20 | 52,272 | 2.16 | 1,000 |
| Total Estimated Cost | | | | | | | | | \$ 2,271,000 |

¹ Property frontage length is based on tax maps when information is available and estimated when distances are not provided.

² Property width required is based on the current right-of-way width (ROW), location of property lines on tax maps, and the width of the proposed ROW.

³ Area required is the result of multiplying the property frontage length and the property width required.

⁴ The real market value of the tax lot is the real market value available from the Umatilla County Assessment and Taxation Web Query in January 2022.

⁵ The tax lot size is the tax lot size available from the Umatilla County Assessment and Taxation Web Query in January 2022.

⁶ The real market value per square foot is the result of dividing the real market value by the tax lot size in square feet.

⁷ The estimated cost is an approximate estimate of the cost of purchasing any required property. The estimated cost is the result of multiplying the real market value per square foot, the area required, and a 1.5 multiplier to account for legal, administrative, contingencies, and any other fees. Values have been rounded to the nearest 100.

⁸ Information for this tax lot is considered confidential and is unavailable. The real market value per square foot was established based on the neighboring property, Account #117280.

ft = feet

SF = square feet

CITY OF HERMISTON, OREGON
UMATILLA RIVER BRIDGE
PRELIMINARY ENGINEERING REPORT
PUNKIN CENTER ROAD OPTION RIGHT-OF-WAY ACQUISITION COST

| Tax Lot Map # | Account # | Property | | Area Required (SF) ³ | Real Market Value ⁴ | Tax Lot Size ⁵ | | Real Market Value per SF ⁶ | Estimated Cost ⁷ |
|---------------|-----------|-----------------------------------|-------------------------------------------|---------------------------------|--------------------------------|---------------------------|-----------|---------------------------------------|-----------------------------|
| | | Frontage Length (ft) ¹ | Property Width Required (ft) ² | | | (Acres) | (SF) | | |
| 4N28B00002100 | 117266 | 3,450 | 25 | 86,250 | \$ 918,780 | 131.24 | 5,716,814 | \$ 0.16 | \$ 20,800 |
| 4N28B00002200 | 117268 | 1,020 | 25 | 25,500 | 432,010 | 38.59 | 1,680,980 | 0.26 | 9,900 |
| 4N28B00002302 | 117272 | 341 | 25 | 8,525 | 582,300 | 5.00 | 217,800 | 2.67 | 34,200 |
| 4N28B00002303 | 117273 | 330 | 25 | 8,250 | 20,890 | 4.85 | 211,266 | 0.10 | 1,300 |
| 4N28B00002304 | 117275 | 330 | 25 | 8,250 | 595,630 | 4.85 | 211,266 | 2.82 | 34,900 |
| 4N28B00002305 | 117277 | 650 | 25 | 16,250 | 149,090 | 4.37 | 190,357 | 0.78 | 19,100 |
| 4N2803AA00200 | 119107 | 546 | 12 | 6,552 | 2,727,010 | 30.39 | 1,323,788 | 2.06 | 20,300 |
| 4N2803AB00500 | 119120 | 467 | 12 | 5,604 | 157,350 | 5.00 | 217,800 | 0.72 | 6,100 |
| 4N2803B000100 | 119155 | 410 | 12 | 4,920 | 931,670 | 20.45 | 890,802 | 1.05 | 7,800 |
| 5N28330000400 | 124650 | 1,130 | 25 | 28,250 | 518,860 | 80.75 | 3,517,470 | 0.15 | 6,300 |
| 5N28330001500 | 124651 | 1,000 | 25 | 25,000 | 410,930 | 30.25 | 1,317,690 | 0.31 | 11,700 |
| 5N28330001700 | 124655 | 198 | 25 | 4,950 | 312,510 | 2.04 | 88,862 | 3.52 | 26,200 |
| 5N28C00006300 | 124656 | 1,960 | 25 | 49,000 | 246,460 | 60.00 | 2,613,600 | 0.09 | 7,000 |
| 5N28C00006400 | 124657 | 950 | 25 | 23,750 | 189,440 | 22.52 | 980,971 | 0.19 | 6,900 |
| 5N2833D000300 | 124707 | 200 | 25 | 5,000 | 599,260 | 7.75 | 337,590 | 1.78 | 13,400 |
| 5N2833D000800 | 124716 | 500 | 45 | 22,500 | 455,880 | 3.75 | 163,350 | 2.79 | 94,200 |
| 5N2833DC00900 | 124726 | 819 | 45 | 36,855 | 413,580 | 2.78 | 121,282 | 3.41 | 188,600 |
| 5N2834C001200 | 124902 | 50 | 12 | 600 | 3,190 | 1.18 | 51,401 | 0.06 | 100 |
| 5N2834C001300 | 124912 | 273 | 12 | 3,276 | 440,930 | 2.02 | 87,991 | 5.01 | 24,700 |
| 5N2834C001400 | 124915 | 341 | 12 | 4,092 | 411,360 | 2.02 | 87,991 | 4.68 | 28,700 |
| 5N2834C001900 | 124926 | 470 | 12 | 5,640 | 429,260 | 16.65 | 725,274 | 0.59 | 5,100 |
| 5N2834C001901 | 124927 | 204 | 12 | 2,448 | 460,880 | 2.01 | 87,556 | 5.26 | 19,400 |
| 5N2834D001502 | 124975 | 305 | 12 | 3,660 | 125,630 | 2.00 | 87,120 | 1.44 | 8,000 |
| 5N2834D001503 | 124976 | 305 | 12 | 3,660 | 62,490 | 2.00 | 87,120 | 0.72 | 4,000 |
| 5N2834D001700 | 124980 | 540 | 12 | 6,480 | 7,165,840 | 4.20 | 182,952 | 39.17 | 380,800 |
| 4N2804A000300 | 135297 | 520 | 45 | 23,400 | 45,161,200 | 23.34 | 1,016,690 | 44.42 | 1,559,200 |
| 4N2804A000400 | 135298 | 350 | 45 | 15,750 | 89,730 | 4.00 | 174,240 | 0.51 | 12,200 |
| 4N2803B000200 | 135672 | 1,000 | 45 | 45,000 | 1,695,990 | 38.40 | 1,672,704 | 1.01 | 68,500 |
| 5N28330000100 | 135746 | 315 | 65 | 20,475 | 131,500 | 65.75 | 2,864,070 | 0.05 | 1,500 |
| 5N2834D001708 | 135773 | 200 | 12 | 2,400 | 924,730 | 1.69 | 73,616 | 12.56 | 45,300 |
| 4N2804A000100 | 142124 | 511 | 45 | 22,995 | 347,090 | 2.00 | 87,120 | 3.98 | 137,500 |
| 4N28B00003703 | 142968 | 25 | 25 | 625 | 1,100 | 0.55 | 23,958 | 0.05 | 100 |
| 4N28B00002308 | 143080 | 25 | 25 | 625 | 520 | 0.26 | 11,326 | 0.05 | 100 |
| 5N2834D001507 | 143787 | 30 | 12 | 360 | 134,780 | 2.08 | 90,605 | 1.49 | 900 |
| 5N2834C001904 | 146370 | 208 | 12 | 2,496 | 151,560 | 1.97 | 85,813 | 1.77 | 6,700 |
| 5N2833D000602 | 148638 | 205 | 45 | 9,243 | 329,390 | 10.66 | 464,350 | 0.71 | 9,900 |
| 4N2803B000109 | 148726 | 509 | 12 | 6,108 | 146,070 | 2.00 | 87,120 | 1.68 | 15,400 |
| 5N2833D000603 | 150118 | 592 | 45 | 26,640 | 193,720 | 2.87 | 125,017 | 1.55 | 62,000 |
| 4N2803B000111 | 150343 | 225 | 12 | 2,700 | 555,180 | 2.07 | 90,169 | 6.16 | 25,000 |
| 5N2834C001905 | 150385 | 204 | 12 | 2,448 | 231,120 | 2.13 | 92,783 | 2.49 | 9,200 |
| 5N2834C001109 | 152706 | 281 | 12 | 3,372 | 629,010 | 1.72 | 74,923 | 8.40 | 42,500 |
| 5N2834C001110 | 152707 | 291 | 12 | 3,492 | 610,960 | 1.72 | 74,923 | 8.15 | 42,800 |
| 5N2834C001906 | 158336 | 208 | 12 | 2,496 | 400,390 | 1.99 | 86,684 | 4.62 | 17,300 |
| 5N28C00006001 | 158439 | 650 | 25 | 16,250 | 645,450 | 20.00 | 871,200 | 0.74 | 18,100 |
| 4N2803AB04300 | 160024 | 72 | 12 | 864 | 335,770 | 0.21 | 9,148 | 36.71 | 47,600 |
| 4N2803AB07300 | 160086 | 118 | 12 | 1,416 | 327,190 | 0.25 | 10,890 | 30.04 | 63,900 |
| 4N2803B000112 | 161466 | 100 | 12 | 1,200 | 114,560 | 1.26 | 54,886 | 2.09 | 3,800 |
| 4N2803B000113 | 161467 | 175 | 12 | 2,100 | 187,810 | 2.98 | 129,809 | 1.45 | 4,600 |
| 4N2803AA01700 | 163665 | 10 | 12 | 120 | 275,620 | 0.20 | 8,528 | 32.32 | 5,900 |

| Tax Lot Map # | Tax Lot | Property | | Area Required (SF) ³ | Real Market Value ⁴ | Tax Lot Size ⁵ | | Real Market Value per SF ⁶ | Estimated Cost ⁷ |
|----------------------|---------|--------------------------------------|----------------------------------------------|------------------------------------|-----------------------------------|---------------------------|--------|------------------------------------------|-----------------------------|
| | | Frontage Length (ft) ¹ | Property Width Required (ft) ² | | | (Acres) | (SF) | | |
| 4N2803AA01700 | 163665 | 10 | 12 | 120 | 275,620 | 0.20 | 8,528 | 32.32 | 5,900 |
| 4N2803AA01800 | 163666 | 133 | 12 | 1,596 | 283,330 | 0.21 | 9,215 | 30.75 | 73,700 |
| 4N2803AA01900 | 163667 | 112 | 12 | 1,344 | 255,820 | 0.21 | 9,064 | 28.22 | 56,900 |
| 4N2803AB10400 | 167036 | 142 | 12 | 1,704 | 336,130 | 0.24 | 10,398 | 32.33 | 82,700 |
| 4N2803AB10500 | 167037 | 98 | 12 | 1,176 | 336,870 | 0.20 | 8,590 | 39.22 | 69,200 |
| 4N2803AB11700 | 167050 | 52 | 12 | 624 | 388,410 | 0.22 | 9,530 | 40.76 | 38,200 |
| 4N2803AB11800 | 167051 | 135 | 12 | 1,620 | 386,080 | 0.21 | 9,347 | 41.31 | 100,400 |
| 4N2803AB11900 | 167052 | 80 | 12 | 960 | 392,060 | 0.27 | 11,656 | 33.64 | 48,500 |
| Total Estimated Cost | | | | | | | | \$ | 3,650,000 |

¹ Property frontage length is based on tax maps when information is available and estimated when distances are not provided.

² Property width required is based on the current right-of-way (ROW) width, location of property lines on tax maps, and the width of the proposed ROW.

³ Area required is the result of multiplying the property frontage length and the property width required.

⁴ The real market value of the tax lot is the real market value available from the Umatilla County Assessment and Taxation Web Query in January 2022.

⁵ The tax lot size is the tax lot size available from the Umatilla County Assessment and Taxation Web Query in January 2022.

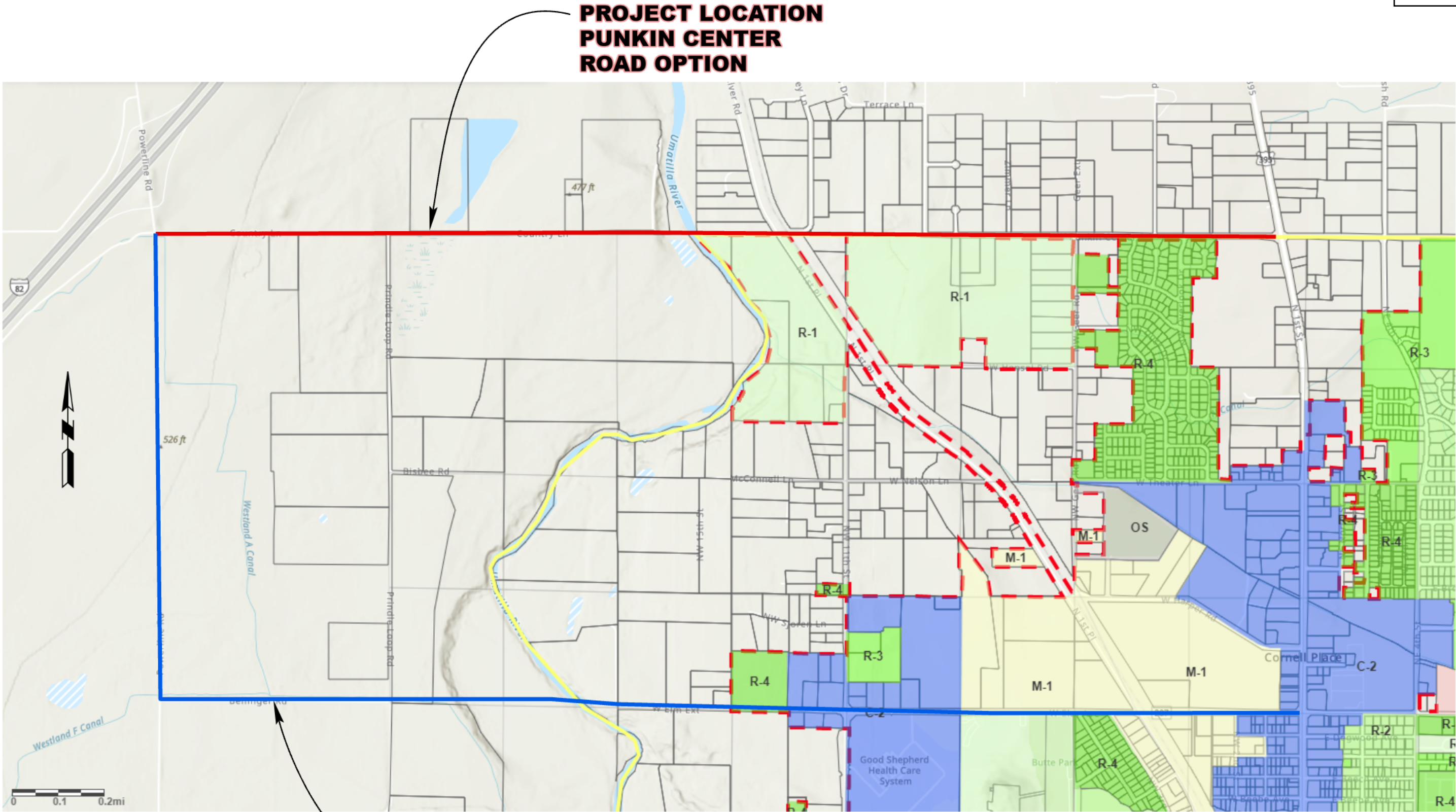
⁶ The real market value per square foot is the result of dividing the real market value by the tax lot size in square feet.

⁷ The estimated cost is an approximate estimate of the cost of purchasing any required property. The estimated cost is the result of multiplying the real market value per square foot, the area required, and a 1.5 multiplier to account for legal, administrative, contingencies, and any other fees. Values have been rounded to the nearest 100.

ft = feet

SF = square feet

\\GIS\GIS\projects\hermiston\736-123 UmatillaRiverBridge\736-123 UmatillaRiverBridge\PER.aprx, PER-736-123-FIG-4-5A ZoneCity, 4/15/2022 9:00 AM, dchristman



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& associates, inc.

CITY OF
HERMISTON, OREGON
UMATILLA RIVER BRIDGE
PRELIMINARY ENGINEERING REPORT
CITY OF HERMISTON ZONING

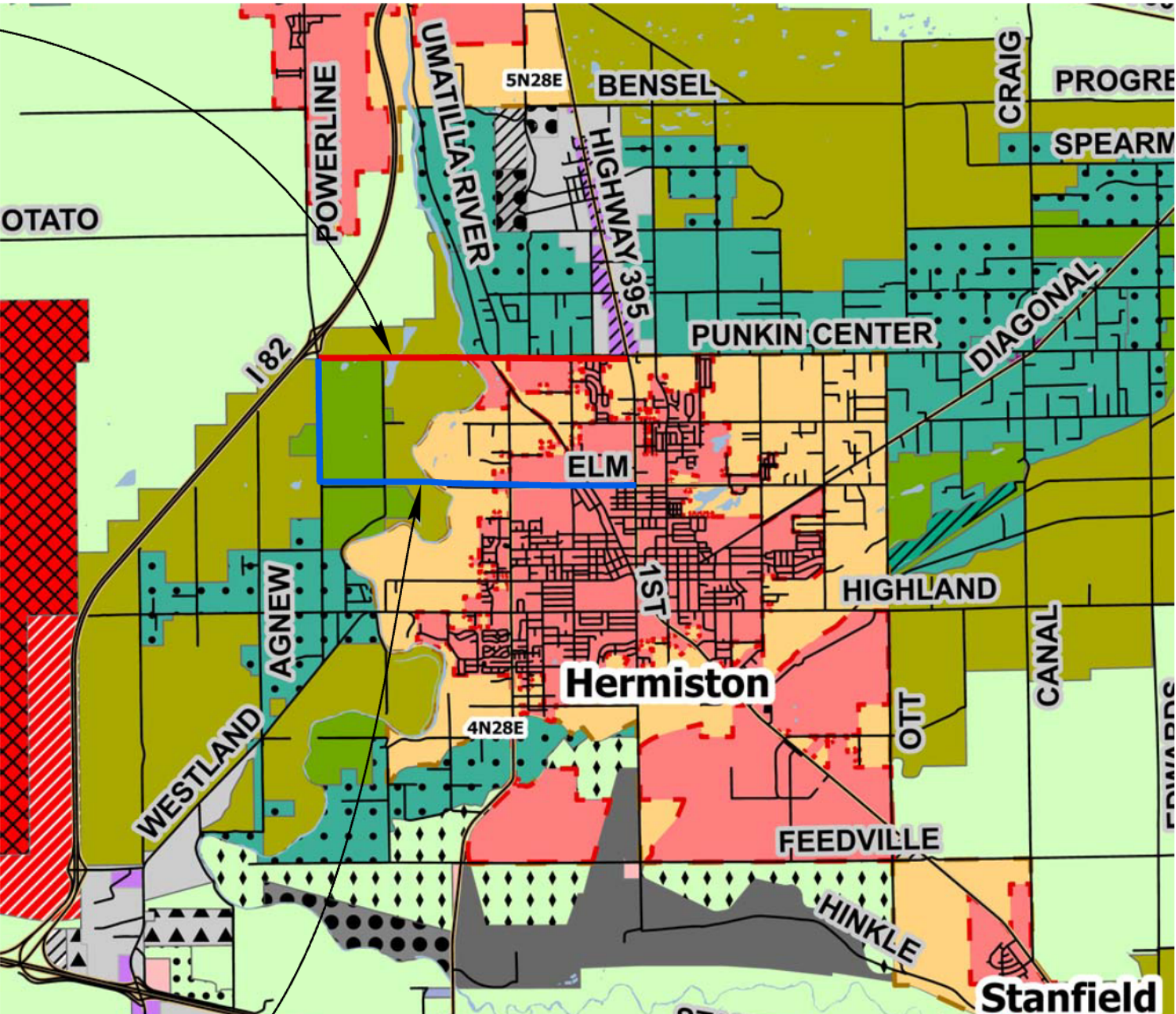
FIGURE
4-5A



**PROJECT LOCATION
PUNKIN CENTER
ROAD OPTION**

Legend

| | | | |
|----------------------|--------|--------|---------|
| State Highway | CRC | HI | RR-2 |
| Water Bodies | DI - R | HI/AR | RR-4 |
| Umatilla County | DI - U | LI | RRSC/LU |
| City Limits | EFU | LI/AR | RSC |
| City UGB | EFU-20 | LI/LU | RTC |
| County Zoning | EFU-40 | LRLI | UDM |
| AB | EFU/AR | RLI | UDR |
| | EFU/FI | RLI/AR | |
| | EFU/NA | RR-10 | |



**PROJECT LOCATION
ELM AVENUE OPTION**



CITY OF
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UMATILLA RIVER BRIDGE
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UMATILLA COUNTY ZONING

FIGURE
4-5B



LEGEND

- L = LOW DENSITY RESIDENTIAL
F-I = FUTURE INDUSTRIAL
F-R = FUTURE RESIDENTIAL
MH = MEDIUM DENSITY RESIDENTIAL/MOBILE HOME
C = COMMERCIAL
I = INDUSTRIAL
M = MEDIUM DENSITY RESIDENTIAL
OS = OPEN SPACE

**CITY OF
HERMISTON, OREGON
UMATILLA RIVER BRIDGE
PRELIMINARY ENGINEERING REPORT**

COMPREHENSIVE LAND USE MAP

FIGURE
4-5C

Chapter 5 - Permitting, Environmental, and Cultural Resources

A. Overview

The following environmental review is an evaluation of the natural and cultural resources within and near the proposed project corridors. This review is cursory in nature and identifies elements to consider when planning for each of the proposed project options. Potential impacts to each resource are described along with potential mitigation measures and potentially required permits. All analyses were conducted via a desktop review of resources; a site visit was not completed for this work.

B. Elm Avenue Option

Goal 5 Resource Mapping

Oregon's Statewide Planning Goal 5: Natural Resources, Scenic and Historic Areas, and Open Spaces requires review and consideration of important local resources to protect natural resources and conserve scenic and historic areas and open spaces. The Oregon Statewide Planning Goal 5 resources are addressed below.

- Riparian corridors, including water and riparian areas and fish habitat: The Elm Avenue option crosses the Umatilla River and its riparian corridor. According to a preliminary assessment by project engineers, the project would likely not require work below the ordinary high water elevation (OHWE) of the Umatilla River.
- Wetlands: Several wetlands occur near the Elm Avenue option, as discussed in the Wetlands and Waterways section below.
- Wildlife habitat: According to the City of Hermiston Comprehensive Plan, the Oregon Department of Fish and Wildlife (ODFW) has identified two critical habitats: the eastern bank of the Umatilla River and a three-acre pond in northeast Hermiston. These two areas and the Oregon State University (OSU) agricultural research station have been designated as Open Space, which protects them from incompatible development. The SageCon Landscape Planning Tool shows that there is no sage grouse habitat in Umatilla County (Oregon Explorer, 2021).
- Federal wild and scenic rivers: The National Wild and Scenic Rivers (NWSR) System map indicates that no designated Wild and Scenic Rivers are located within the Elm Avenue option (NWSR, 2016).
- State scenic waterways: The Oregon's Scenic Waterways list indicates that no designated State Scenic Waterways are located within the Elm Avenue option (Oregon Parks and Recreation Department [OPRD], 2021).
- Groundwater resources: According to the Oregon Water Resources Department (OWRD), the Elm Avenue option is located within the Stage Gulch, Butter Creek, and Ordinance Gravel critical groundwater areas (OWRD, 2016). Because the nature of the proposed work

does not include discharges to groundwater, the project is not anticipated to affect the Stage Gulch, Butter Creek, or Ordinance Gravel critical groundwater areas (see Figure 5-1, Critical Groundwater Areas).

- Approved Oregon recreation trails: According to Oregon State Parks, no designated scenic or regional trails are located near the Elm Avenue option (Oregon State Parks, 2021).
- Natural areas: The Cold Springs National Wildlife Refuge and the Umatilla National Wildlife Refuge are located near the City of Hermiston; however, neither occurs near the Elm Avenue option.
- Wilderness areas: The nearest wilderness area is the North Fork Umatilla Wilderness located approximately 50 miles east of the Elm Avenue option (Wilderness Connect, 2021).
- Mineral and aggregate resources: According to the City of Hermiston Comprehensive Plan, there are only two economically viable sand and gravel pits within the Hermiston urban growth boundary (UGB). Neither is located near the Elm Avenue option.
- Energy sources: According to the City of Hermiston Comprehensive Plan, the City is located near two major energy generators: the McNary Dam and the Portland General Electric (PGE) coal-fired electrical generating plant. Neither is located near the Elm Avenue option.
- Cultural Areas: Cultural resources and historic properties that occur within 1 mile of the project corridor are discussed in the Cultural Resources and Historic Properties section below.

Due to the nature of the proposed project, the Elm Avenue option could result in impacts to riparian corridors, wetlands, wildlife habitat, and cultural areas. Due to the distance of Goal 5 Resources, the Elm Avenue option is not anticipated to impact federal Wild and Scenic Rivers, State Scenic Waterways, groundwater resources, approved Oregon recreation trails, natural areas, wilderness areas, mineral and aggregate resources, and energy sources.

Wetlands and Waterways

According to the U.S. Fish and Wildlife Service (USFWS) National Wetlands Inventory (NWI) Mapper, there are several wetlands near the Elm Avenue option, consisting of freshwater emergent wetlands and freshwater forested/shrub wetlands (USFWS, 2021a) (see Figure 5-2, Wetlands and Waterways). A pending Local Wetlands Inventory exists for the area, but it has not been reflected on the Statewide Wetlands Inventory, as it has not been approved.

The Umatilla River, Westland F Canal, Westland A Canal, three unnamed ditches, and one freshwater pond are located within and adjacent to the Elm Avenue option (see Figure 5-2, Wetlands and Waterways).

A site visit will be necessary to verify the presence of wetlands and waterways. If jurisdictional wetlands or waterbodies will be impacted by the proposed project, they will be subject to the permit requirements of the state and federal Removal-Fill Law and may require wetland/water delineation and permitting from the Oregon Department of State Lands (DSL) and the U.S. Army Corps of Engineers (USACE).

Floodplains

According to the Federal Emergency Management Agency (FEMA) Map Service Center, FEMA Flood Insurance Rate Map (FIRM) Panel No. 41059C0577G, the Elm Avenue option is located within the 500- and 100-year floodplains and within the floodway (FEMA, 2021) (see Figure 3-1, River Crossing Plan and Profile Elm Avenue Option). Therefore, FEMA development standards may apply, and a Floodplain Development Permit may be required. Since structural elements of the bridge will likely be located within the floodway, it is unlikely that a no-rise condition can be demonstrated. Because bridge construction will likely preclude demonstration of a 0.00 foot change in the effective 100-year flood water surface elevation, the project will require a Conditional Letter of Map Revision (CLOMR) and, following construction of the bridge, a Letter of Map Revision (LOMR); see Chapter 3 for costs and a detailed discussion of these elements.

Soils and Farmland

The six soils mapped within the Elm Avenue option are shown on Table 5-1 along with the hydric status, drainage class, and farmland classification (Natural Resources Conservation Service [NRCS], 2021) (see Figure 5-3, Soils Map).

TABLE 5-1
SOILS FOUND WITHIN THE ELM AVENUE OPTION

| Map Unit | Soil Name | Hydric Status | Drainage Class | Farmland Classification |
|-----------------|--------------------------------------------------------------------|----------------------|-------------------------|----------------------------------|
| 14B | Burbank loamy fine sand, 0 to 5 percent slopes | No | Excessively drained | Not prime farmland |
| 119A | Wanser loamy fine sand, 0 to 3 percent slopes | Yes | Poorly drained | Farmland of statewide importance |
| 2B | Adkins fine sandy loam, gravelly substratum, 0 to 5 percent slopes | No | Well drained | Prime farmland if irrigated |
| 122B | Winchester sand, 0 to 5 percent slopes | No | Excessively drained | Not prime farmland |
| 124B | Winchester-Urban land complex, 0 to 5 percent slopes | No | Excessively drained | Not prime farmland |
| 126A | Xerofluvents, 0 to 3 percent slopes | No | Somewhat poorly drained | Not prime farmland |

The project will occur on previously disturbed farmland and on gravel and paved roads. If a federal nexus is identified (federal land, federal funding, federal permit, etc.), conversion of farmland to a non-farm use must comply with the guidelines of the Farmland Protection Policy Act, and farmland conversion impact consultation with the NRCS will be required. If improvements occur on Conservation Reserve Program (CRP) land, a waiver may be required.

Hydric soils have the potential to contain wetlands. A site visit will be necessary to verify the presence or absence of wetlands.

Protected Species

Listed species within the project corridor were obtained from the USFWS and National Marine Fisheries Service (NMFS) databases. The USFWS Information for Planning and Consultation (IPaC) list indicates that one threatened species (bull trout, *Salvelinus confluentus*) and bull trout critical habitat occur in the Umatilla River within the Elm Avenue option (USFWS, 2021b). The NMFS Protected Resources app indicates that one threatened species (Middle Columbia River [MCR] steelhead, *Oncorhynchus mykiss*) and steelhead critical habitat occur in the Umatilla River within the Elm Avenue option (NMFS, 2021a). The Umatilla River near the Elm Avenue option is also considered essential fish habitat (EFH) and is designated essential salmonid habitat (ESH) (NMFS, 2021b; DSL, 2021) (see Figure 5-4, Protected Species).

A search of the Oregon Biodiversity Information Center (ORBIC) database found 15 element occurrence records of rare, threatened, or endangered species within a 2-mile radius of the proposed project corridor, including federally listed steelhead and bull trout and state-listed Washington ground squirrel (*Urocitellus washingtoni*) (ORBIC, 2021). Depending on project funding or other considerations, a Washington ground squirrel survey may be required.

If no in-water work occurs, consultation under Section 7 of the Endangered Species Act (ESA) with the USFWS and NMFS would still be required if a federal nexus is identified for the project. To meet this requirement, a no effect memo may be required to document the lack of impacts to listed species. If in-water work is not required, the project will not be subject to an in-water work window.

If in-water work (or nearshore pile driving) occurs, formal consultation under Section 7 of the ESA with the USFWS and NMFS would be required. A USACE 404 Permit, DSL Removal-Fill Permit, and Oregon Department of Environmental Quality (DEQ) 401 Water Quality Certification Permit may be required. If the project requires in-water work, the project will be subject to the in-water work window for the Umatilla River of July 15 through September 30. If work occurs below the wetted edge of the Umatilla River, work area isolation, fish salvage, and ODFW fish passage approval may be required. The project is not anticipated to require in-water work. Construction of piers and abutments is anticipated to occur outside the OHWE. If work does occur within the OHWE, pile driving is anticipated to be required for the construction of piers and abutments. Due to the complexity of the project, it is unlikely that it would fit under the Standard Local Operating Procedures for Endangered Species (SLOPES) programmatic, and a Biological Assessment is anticipated to be required.

Migratory Birds

The USFWS IPaC list indicates bald eagle (*Haliaeetus leucocephalus*), Clark's grebe (*Aechmophorus clarkii*), Franklin's gull (*Leucophaeus pipixcan*), and rufous hummingbird (*Selasphorus rufus*) as birds of particular concern either because they occur on the USFWS Birds of Conservation Concern (BCC) list or warrant special attention in the project location. These species are highly mobile, and it is likely that they will simply leave the area during construction (USFWS, 2021b).

If tree removal occurs, trees should be surveyed for raptor nests prior to removal. Trees should be removed prior to the nesting season of March through August to ensure that no nesting raptors will

be affected by the proposed project. Construction should be completed outside the raptor nesting season, if possible, if any nesting raptors are observed near the project corridor.

Land Use/Zoning

The City of Hermiston Planning Department and the Umatilla County Planning Department should be consulted once the project design is complete to ensure all City and County permitting requirements are met.

If any work is located in an Exclusive Farm Use (EFU) area, a Conditional Land Use Permit and Goal 3 exception would be required along with strict County zoning requirements associated with the project. Goal 3 requires counties to identify farmland, designate it as such, and zone it EFU. An EFU zone places restrictions on developments that are unrelated to agriculture to minimize conflicts with farming.

If improvements occur on CRP land, a waiver may be required. Additionally, if a federal nexus is identified, conversion of farmland must comply with the guidelines of the Farmland Protection Policy Act. See Chapter 4 for additional land use and zoning discussion and costs.

Stormwater Discharge

Any expansion of impervious surfaces may require a DEQ Post-Construction Stormwater Management Plan. A National Pollutant Discharge Elimination System (NPDES) 1200-C Construction Stormwater Permit would be required if construction disturbs more than 1 acre of land and has the potential to discharge to the Umatilla River or other surface waters.

Parks, Green Spaces, and Natural Areas

Two parks, Butte Park and the disc golf course, are located adjacent to the Elm Avenue option (see Figure 5-5, Existing Park System).

Butte Park was funded by the Land and Water Conservation Fund (LWCF) in 1991 (LWCF, 2021). Section 6(f) of the Land and Water Conservation Fund Act (LWCFA) prohibits the conversion of property acquired or developed with LWCFA grants to uses other than public outdoor recreation. If work occurs on 6(f) land, consultation with the OPRD may be required.

Noise and Air Quality

The project may have the potential for noise impacts, and a noise analysis and mitigation plan may be required for construction noise, pile driving, and future traffic conditions.

According to the DEQ, the City of Hermiston is not in an air quality non-attainment or maintenance area (DEQ, 2021a). The proposed project is not anticipated to require an Air Quality Permit from the DEQ.

Hazardous Materials

Sixty-eight DEQ permitted sites occur within 0.5 mile of the Elm Avenue option; however, only hazardous materials adjacent to the project corridor will be discussed. Ten underground storage tanks (USTs), one hazardous waste generator, and four regulated leaking underground storage tanks (LUSTs) (cleanup completed) occur adjacent to the Elm Avenue option and are described below (DEQ, 2021b) (see Figure 5-6, DEQ Permitted Sites).

Underground Storage Tanks

- Western Express Mart; Facility ID 37420, UST Facility ID 12064 (1295 N.W. 11th Street). This site consists of three active USTs containing 30,000 gallons of gasoline and 20,000 gallons of diesel.
- Wondrack Distributing; Facility ID 17541, UST Facility ID 9477 (55 W. Elm Avenue). This site consists of four active USTs containing 12,000 gallons of gasoline and 12,000 gallons of diesel.
- Umatilla Electric Cooperative; Facility ID 879, UST ID 6477 (750 W. Elm Avenue). This site consists of three active USTs containing 20,000 gallons of gasoline and 40,000 gallons of diesel.

Leaking Underground Storage Tanks

- Umatilla Electric Cooperative; Facility ID 879, LUST ID 6477 (750 W. Elm Avenue). This site consists of two regulated LUSTs (cleanup completed). At the first tank, an unknown contaminant leaked, affecting soil and groundwater. The release was stopped on February 13, 1990, and cleanup was completed on September 21, 1990. At the second tank, an overfill caused diesel to release into soil. The release was stopped on January 23, 2002, and cleanup was completed on October 7, 2002. This site is located approximately 214 feet north of the Elm Avenue option. Due to the successful site cleanup, the project corridor is not anticipated to have been impacted by this release.
- McGregor Co. (Growers Fertilizer, Inc.); Facility ID 5559, LUST ID 10164 (445 W. Elm Avenue). This site consists of one regulated LUST (cleanup completed). An overfill caused unleaded gasoline/diesel to release into soil. The release was stopped on December 20, 1989, and cleanup was completed on August 13, 1999. This site is located approximately 115 feet north of the Elm Avenue option. Due to the successful site cleanup, the project corridor is not anticipated to have been impacted by this release.
- Netarts Bay, Inc.; Facility ID 22089, LUST ID 6132 (1235 N. 1st Street). This site consists of one regulated LUST (cleanup completed). An overfill caused unleaded gasoline to release into soil. The release was stopped on February 27, 1990, and cleanup was completed on November 1, 2005. This site is located approximately 131 feet south of the Elm Avenue option. Due to the successful site cleanup, the project corridor is not anticipated to have been impacted by this release.

Hazardous Waste Generators

- Umatilla Electric Cooperative; Conditionally Exempt Generator as of December 31, 2006; Facility ID 879, Site ID 1108 (750 W. Elm Avenue).

Because cleanup has been completed at all the LUSTs near the Elm Avenue option, the project is not anticipated to be impacted by these sites. A Phase 1 Environmental Site Assessment may be required for this project if property acquisition occurs. A Level 1 Hazardous Materials Corridor Study (HMCS) (and potentially a Level 2 Preliminary Site Investigation) may be required if Oregon Department of Transportation (ODOT) funding is utilized. If petroleum products are encountered during site excavation, work would be required to stop and the DEQ would be required to be contacted.

Cultural Resources and Historic Properties

The Oregon Archaeological Records Remote Access (OARRA) database was consulted for existing archaeological resources within 1 mile of the project corridor. While two previous surveys overlap the project corridor, no previously recorded archaeological sites or isolates were located within the proposed project corridor.

Eight cultural resources are mapped within 1 mile of the project corridor: site 35UM439 and seven isolates located on or near the Hermiston Butte. Site 35UM439 consists of two precontact burials that were 0.75 mile away from the proposed project and more than 1 mile away from the proposed bridge crossing. The graves were discovered on private land during construction of a road. After archaeological investigations were conducted by the Confederated Tribes of the Umatilla Indian Reservation (CTUIR), the individuals were reburied in their original grave site (Dickson, 2008). The seven isolates located on or near the Hermiston Butte consist of precontact flakes, cobbles, shards, and a projectile point fragment as well as one historical bullet, the historical City reservoir, and the historical Butte Irrigation Canal. Most isolates are clustered 350 yards away from the project corridor on the butte itself, while one isolate is located approximately 100 yards from the project corridor (Oman, 1998; Miller, 2003). Site 35UM439, the Butte Irrigation Canal, and the City reservoir are unevaluated while all other cultural resources have been found ineligible for inclusion to the National Register of Historic Places (NRHP).

Cultural resource surveys within 1 mile of the proposed project have been conducted for land consolidation, water system improvements (pumps, storage, piping, irrigation canal improvements), road improvements (widening, grading, drainage improvements, repaving, sign installation), trail improvements, community pool construction, communications tower installations, and transmission line installation. One survey overlapped the project corridor for 1 mile of the eastern extent of W. Elm Avenue, from N. 1st Street to N.W. 11th Street. This survey observed no cultural material in the project corridor (Baxter, 2005). Six surveys occurred on or near the Hermiston Butte and observed the seven isolates. All other surveys observed no cultural materials in the vicinity of the project corridor. It should be noted that early surveys within the search radius (conducted in the 1970s through 1990s) did not employ the use of shovel test probes (STPs) to identify potential subsurface cultural resources, whereas the use of STPs is standard practice in current cultural resource surveys when a project will include ground disturbance. Potential impacts to archaeological

resources as a result of construction include excavation, sediment disturbance, sediment compactions, and other ground-disturbing construction activities.

Construction of a new road and bridge have the potential to impact viewsheds in the surrounding area. It is anticipated that a historic property inventory may be required, in addition to a cultural resource inventory, depending on funding requirements.

Funding Requirements

The funding agency will determine which environmental permits and consultation will be required for the proposed project. If federal funding is utilized, an Environmental Assessment will be required.

Advantages

The Elm Avenue option is not anticipated to impact the following Goal 5 resources: federal Wild and Scenic Rivers, State Scenic Waterways, groundwater resources, approved Oregon recreation trails, natural areas, wilderness areas, mineral and aggregate resources, and energy sources.

Disadvantages

The Elm Avenue option could result in impacts to the following Goal 5 resources: riparian corridors, wetlands, wildlife habitat, and cultural areas. The Elm Avenue option is anticipated to have greater wetland and waterway impacts than the Punkin Center Road option. A portion of the project corridor will be located within the floodway, 100-year floodplain, and 500-year floodplain; this is anticipated to require detailed hydraulic modeling and a no-rise certificate. The project will require permanent conversion of farmland. A portion of the project corridor is zoned EFU, so it may be challenging to gain land use approval from the County for development. ESA-listed species occur within the project corridor. More vegetation removal would be required. Upon project completion, the Elm Avenue option would result in a greater amount of impervious surface than the Punkin Center Road option. Two parks, Butte Park (LWCF funded) and the disc golf course, are located adjacent to the Elm Avenue option. A greater number of DEQ-permitted sites are located adjacent to the project corridor, resulting in a greater chance of encountering contaminated soils than the Punkin Center Road option. The Elm Avenue option is located farther upstream on the Umatilla River than the Punkin Center Road option and, therefore, would result in a longer stretch of river that could potentially be impacted by project development.

Permits

Table 5-2 lists potential permits and environmental clearances that could be required for the Elm Avenue option.

TABLE 5-2
ELM AVENUE OPTION ENVIRONMENTAL PERMITS

| Permit | Approving Agency |
|-------------------------------------------------------|----------------------------------------------------------------------------------------------------------------------|
| County/City Permits/Other | |
| Conditional Use Permit | Umatilla County and the City of Hermiston |
| Goal 3 Exception Process | Umatilla County |
| Floodplain Development Permit and No-Rise Certificate | Umatilla County and the City of Hermiston |
| State Permits/Consultation | |
| DSL Removal-Fill Permit | DSL |
| Wetland Delineation Concurrence | DSL |
| Post-Construction Stormwater Management Plan | DEQ |
| 1200-C Construction Stormwater General Permit | DEQ |
| 401 Water Quality Certification | DEQ |
| ODFW Fish Passage Approval | ODFW |
| ODFW Consultation (Ground Squirrel Survey) | ODFW |
| Beneficial Use Determination | ODOT |
| Noise Analysis | ODOT |
| LWCF 6(f) Land Consultation | OPRD |
| Federal Permits/Consultation | |
| 404 Removal-Fill Permit | USACE |
| Section 7 ESA Consultation | USACE, USFWS, and NMFS |
| Section 106 Consultation | Oregon State Historic Preservation Office (SHPO)/Tribes as approved by the Legislative Commission on Indian Services |
| Environmental Assessment | Federal Funding Agency |
| CLOMR | FEMA |
| Farmland Conversion Impact Consultation | NRCS |

Cost

Table 5-3 lists the estimated costs for potential permits and environmental clearances that could be required for the Elm Avenue option (2022 dollars).

TABLE 5-3
ELM AVENUE OPTION ENVIRONMENTAL COSTS

| Task Description | Estimated Cost |
|----------------------------------------------------------------------------------------------------------|----------------|
| County/City Permits/Other | |
| Floodplain Development Permit Application and No-Rise Certificate (assumes hydraulic modeling completed) | \$5,000 |
| Conditional Use Permit Application (Goal 3 Exception) | \$30,000 |
| State Permits/Consultation | |
| Wetland Delineation | \$10,000 |
| Wetland/Waterway Mitigation Plan | \$20,000 |
| Stream Functional Assessment Method | \$6,000 |
| Oregon Rapid Wetland Assessment Protocol | \$4,000 |
| Ground Squirrel Survey | \$5,000 |
| Fish Salvage | \$10,000 |
| ODFW Fish Passage Approval | \$10,000 |

| | |
|--------------------------------------------------------------|----------------------------|
| Post-Construction Stormwater Management Plan | \$10,000 |
| 1200-C Construction Stormwater General Permit | \$8,000 |
| ODOT Noise Analysis | \$30,000 |
| Phase 1 Environmental Site Assessment/ODOT Level 1 HMCS | \$10,000 |
| ODOT Level 2 Preliminary Site Investigation | Depends on Level 1 Results |
| Federal Permits/Consultation | |
| CLOMR/LOMR | See Chapter 3 |
| Joint Permit Application | \$16,000 |
| Biological Assessment | \$20,000 |
| Environmental Assessment | \$25,000 |
| Historic Property Inventory | \$30,000 |
| Cultural Resource Phase I (STP and Pedestrian) | \$75,000 |
| Cultural Resource Phase II (Ten Sites) | \$150,000 |
| Cultural Resource Phase III | Depends on Phase II |
| Cultural Resource Monitoring (Assumes 100 Construction Days) | \$100,000 |
| Total | \$574,000 |

C. Punkin Center Road Option

Goal 5 Resource Mapping

Oregon's Statewide Planning Goal 5: Natural Resources, Scenic and Historic Areas, and Open Spaces requires review and consideration of important local resources to protect natural resources and conserve scenic and historic areas and open spaces. The Oregon Statewide Planning Goal 5 resources are addressed below.

- Riparian corridors, including water and riparian areas and fish habitat: The Punkin Center Road option crosses the Umatilla River and its riparian corridor. Based on a preliminary analysis by design engineers, the project is not anticipated to require work below the OHWE of the Umatilla River.
- Wetlands: Several wetlands occur near the Punkin Center Road option, as discussed in the Wetlands and Waterways section below.
- Wildlife habitat: According to the City of Hermiston Comprehensive Plan, ODFW has identified two critical habitats: the eastern bank of the Umatilla River and a 3-acre pond in northeast Hermiston. These two areas and the OSU agricultural research station have been designated as Open Space, which protects them from incompatible development. The SageCon Landscape Planning Tool shows that there is no sage grouse habitat in Umatilla County (Oregon Explorer, 2021).
- Federal Wild and Scenic Rivers: The NWSR System map indicates that no designated Wild and Scenic Rivers are located within the Punkin Center Road option (NWSR, 2016).
- State Scenic Waterways: The Oregon's Scenic Waterways list indicates that no designated State Scenic Waterways are located within the Punkin Center Road option (Oregon Parks and Recreation Department, 2021).
- Groundwater resources: According to the OWRD, the Punkin Center Road option is located within the Butter Creek critical groundwater area (OWRD, 2016). Because the nature of the

proposed work does not include discharges to groundwater, the project is not anticipated to affect the Butter Creek critical groundwater area (see Figure 5-1, Critical Groundwater Areas).

- Approved Oregon recreation trails: According to Oregon State Parks, no designated scenic or regional trails are located near the Punkin Center Road option (Oregon State Parks, 2021).
- Natural areas: The Cold Springs National Wildlife Refuge and the Umatilla National Wildlife Refuge are located near the City of Hermiston; however, neither occurs near the project corridor.
- Wilderness areas: The nearest wilderness area is the North Fork Umatilla Wilderness located approximately 50 miles east of the project corridor (Wilderness Connect, 2021).
- Mineral and aggregate resources: According to the City of Hermiston Comprehensive Plan, only two economically viable sand and gravel pits are located within the Hermiston UGB. Neither is located near the Punkin Center Road option.
- Energy sources: According to the City of Hermiston Comprehensive Plan, the City is located near two major energy generators, the McNary Dam and the PGE coal-fired electrical generating plant. Neither is located near the Punkin Center Road option.
- Cultural Areas: Cultural resources and historic properties that occur within 1 mile of the project corridor are discussed in the Cultural Resources and Historic Properties section below.

Due to the nature of the proposed project, the Punkin Center Road option could result in impacts to riparian corridors, wetlands, wildlife habitat, and cultural areas. Due to the distance of Goal 5 Resources, the Punkin Center Road option is not anticipated to impact federal Wild and Scenic Rivers, State Scenic Waterways, groundwater resources, approved Oregon recreation trails, natural areas, wilderness areas, mineral and aggregate resources, and energy sources.

Wetlands and Waterways

According to the USFWS NWI Mapper, several wetlands are located near the Punkin Center Road option, consisting of freshwater emergent wetlands and freshwater forested/shrub wetlands (USFWS, 2021a) (see Figure 5-2, Wetlands and Waterways). A pending Local Wetlands Inventory exists for the area, but it has not been reflected on the Statewide Wetlands Inventory, as it has not been approved.

The Umatilla River, Westland A Canal, two unnamed ditches, and several freshwater ponds are located within and adjacent to the Punkin Center Road option (see Figure 5-2, Wetlands and Waterways).

A site visit will be necessary to verify the presence of wetlands and waterways. If wetlands and waterbodies will be impacted by the proposed project, the wetlands may be subject to the permit requirements of the state and federal Removal-Fill Law and may require wetland/waters delineation requirements and permitting from the DSL and the USACE.

Floodplains

According to the FEMA Map Service Center, FEMA FIRM Panel No. 41059C0577G, the Punkin Center Road option is located within the 500- and 100-year floodplains and within the floodway (FEMA, 2021) (see Figure 3-2, River Crossing Plan and Profile Punkin Center Road Option). Therefore, FEMA development standards may apply, and a Floodplain Development Permit would be required. Since the bridge infrastructure will be located within the floodway, it is unlikely that a no-rise condition can be demonstrated. Because bridge construction will preclude demonstration of a 0.00 foot change in the effective 100-year flood water surface elevation, the project will require a CLOMR and, following construction of the bridge, a LOMR; see Chapter 3 for costs and a detailed discussion of these elements.

Soils and Farmland

The six soils mapped within the Punkin Center Road option are shown on Table 5-4 along with the hydric status, drainage class, and farmland classification (NRCS, 2021) (see Figure 5-3, Soils Map).

TABLE 5-4
SOILS FOUND WITHIN THE PUNKIN CENTER ROAD OPTION

| Map Unit | Soil Name | Hydric Status | Drainage Class | Farmland Classification |
|----------|--------------------------------------------------------------------|---------------|-------------------------|----------------------------------|
| 14B | Burbank loamy fine sand, 0 to 5 percent slopes | No | Excessively drained | Not prime farmland |
| 119A | Wanser loamy fine sand, 0 to 3 percent slopes | Yes | Poorly drained | Farmland of statewide importance |
| 122B | Winchester sand, 0 to 5 percent slopes | No | Excessively drained | Not prime farmland |
| 76B | Quincy loamy fine sand, gravelly substratum, 0 to 5 percent slopes | No | Excessively drained | Not prime farmland |
| 75E | Quincy loamy fine sand, 5 to 25 percent slopes | No | Excessively drained | Not prime farmland |
| 126A | Xerofluvents, 0 to 3 percent slopes | No | Somewhat poorly drained | Not prime farmland |

The project will occur on previously disturbed farmland and gravel roads. If a federal nexus is identified, conversion of farmland must comply with the guidelines of the Farmland Protection Policy Act, and farmland conversion impact consultation with the NRCS will be required. If improvements occur on CRP land, a waiver may be required.

Hydric soils have the potential to contain wetlands. A site visit will be necessary to verify the presence or absence of wetlands.

Protected Species

Listed species within the project corridor were obtained from the USFWS and NMFS databases. The USFWS IPaC list indicates that one threatened species (bull trout, *Salvelinus confluentus*) and bull trout critical habitat occur in the Umatilla River within the Punkin Center Road option (USFWS, 2021b). The NMFS Protected Resources app indicates that one threatened species (MCR steelhead,

Oncorhynchus mykiss) and steelhead critical habitat occur in the Umatilla River within the Punkin Center Road option (NMFS, 2021a). The Umatilla River near the Punkin Center Road option is also considered EFH and is designated ESH (NMFS, 2021b; DSL, 2021) (see Figure 5-4, Protected Species).

A search of the ORBIC database found 15 element occurrence records of rare, threatened, or endangered species within a 2-mile radius of the proposed project corridor, including federally listed steelhead and bull trout and state-listed Washington ground squirrel (*Urocitellus washingtoni*) (ORBIC, 2021). Depending on project funding or other considerations, a Washington ground squirrel survey may be required.

If no in-water work occurs, consultation under Section 7 of the ESA with the USFWS and NMFS would still be required if a federal nexus is identified. To meet this requirement, a no effect memo may be required to document the lack of impacts to listed species. If in-water work is not required, the project will not be subject to an in-water work window.

If in-water work (or nearshore pile driving) occurs, formal consultation under Section 7 of the ESA with the USFWS and NMFS would be required. A USACE 404 Permit, DSL Removal-Fill Permit, and DEQ 401 Water Quality Certification Permit may be required. If the project requires in-water work, the project will be subject to the in-water work window for the Umatilla River of July 15 through September 30. If work occurs below the wetted edge of the Umatilla River, work area isolation, fish salvage, and ODFW fish passage approval may be required. The project is not anticipated to require in-water work. The construction of piers and abutments is anticipated to occur outside the OHWE. If work does occur within the OHWE, pile driving is anticipated to be required for the construction of piers and abutments. Due to the complexity of the project, it is unlikely that it would fit under the SLOPES programmatic, and a Biological Assessment is anticipated to be required.

Migratory Birds

The USFWS IPaC list indicates bald eagle (*Haliaeetus leucocephalus*), Clark's grebe (*Aechmophorus clarkii*), Franklin's gull (*Leucophaeus pipixcan*), and rufous hummingbird (*Selasphorus rufus*) as birds of particular concern either because they occur on the USFWS BCC list or warrant special attention in the project corridor. These species are highly mobile, and it is likely that they will simply leave the area during construction (USFWS, 2021b).

If tree removal occurs, trees should be surveyed for raptor nests prior to removal. Trees should be removed prior to the nesting season of March through August to ensure that no nesting raptors will be affected by the proposed project. Construction should be completed outside the raptor nesting season, if possible, if any nesting raptors are observed near the project corridor.

Land Use/Zoning

The City of Hermiston Planning Department and the Umatilla County Planning Department should be consulted once the project design is complete to ensure that all City and County permitting requirements are met.

If any work is located in an EFU area, a Conditional Land Use Permit and Goal 3 exception would be required along with strict County zoning requirements associated with the project. Goal 3 requires

counties to identify farmland, designate it as such, and zone it EFU. An EFU zone places restrictions on developments that are unrelated to agriculture to minimize conflicts with farming.

If improvements occur on CRP land, a waiver may be required. Additionally, if a federal nexus is identified, conversion of farmland must comply with the guidelines of the Farmland Protection Policy Act.

Based on preliminary mapping, it appears a portion of the project may occur on Bureau of Land Management (BLM)-owned land. If the project occurs on BLM-owned land, the project may be subject to BLM National Environmental Policy Act (NEPA) requirements and an extensive environmental review process. The BLM should be consulted to ensure that all BLM permitting requirements are met. See Chapter 4 for additional land use and zoning discussion and cost.

Stormwater Discharge

Any expansion of impervious surfaces would require a DEQ Post-Construction Stormwater Management Plan. An NPDES 1200-C Construction Stormwater Permit would be required if construction disturbs more than 1 acre of land and has the potential to discharge to the Umatilla River or other surface waters.

Parks, Green Spaces, and Natural Areas

One natural area, owned by the BLM, is located adjacent to the Punkin Center Road option (see Figure 5-5, Existing Park System).

If the project occurs on BLM-owned land, the project may be subject to BLM NEPA requirements and an extensive environmental review process.

Noise and Air Quality

The project may have the potential for noise impacts, and a noise analysis and mitigation plan may be required for construction noise, pile driving, and future traffic conditions.

According to the DEQ, the City of Hermiston is not in an air quality non-attainment or maintenance area (DEQ, 2021a). The proposed project is not anticipated to require an Air Quality Permit from the DEQ.

Hazardous Materials

Six hazardous material sites occur within 0.5 mile of the project corridor; however, only hazardous materials adjacent to the project corridor will be discussed. One suspected contaminated site requiring further investigation and four USTs occur adjacent to the Punkin Center Road option and are described below (DEQ, 2021b) (see Figure 5-6, DEQ Permitted Sites).

Suspected Contaminated Site

- Swain Motors; Facility ID 141365, Site ID 6127 (80406 Highway 395). A floor drain located in the vehicle service area discharged to a former septic drainfield. The floor drain was closed in June 2016. The site has potential soil and groundwater contamination; soil and/or groundwater sampling is necessary to evaluate environmental impacts from the floor drain. This site is located approximately 615 feet north of the Punkin Center Road option. Due to the distance from the project corridor, the project is not anticipated to be impacted by this release.

Underground Storage Tank

- Rocket Mart; Facility ID 88807, UST Facility ID 2215 (2398 N. 1st Street). This site consists of four active USTs containing 50,000 gallons of gasoline and 15,000 gallons of diesel.

Based on the location of the permitted sites in relation to the project corridor, the project is not anticipated to be impacted by these sites. A Phase 1 Environmental Site Assessment may be required for this project if property acquisition occurs. A Level 1 HMCS (and potentially a Level 2 Preliminary Site Investigation) may be required if ODOT funding is utilized. If petroleum products are encountered during site excavation, work would be required to stop and the DEQ would be required to be contacted.

Cultural Resources and Historic Properties

The OARRA database was consulted for existing archaeological resources within 1 mile of the project corridor. While four previous surveys overlap the project corridor, no previously recorded archaeological sites or isolates were located within the proposed project corridor.

One cultural resource is mapped within 1 mile of the project corridor. Site 35UM439 consists of two precontact burials that were 0.25 mile away from the proposed project and more than 0.5 mile from the proposed bridge crossing. The graves were discovered on private land during the construction of a road. After archaeological investigations were conducted by the CTUIR, the two individuals were reburied in their original grave site (Dickson, 2008). Site 35UM439 is unevaluated for inclusion to the NRHP.

Cultural resource surveys within 1 mile of the proposed project have been conducted for wastewater treatment systems (construction of a plant, installation of piping and tanks), road improvements (widening, grading, drainage improvements, repaving, sign installation), land consolidation, and transmission line installation. None of these surveys observed cultural materials in the general vicinity of the proposed project. However, it should be noted that early surveys within the search radius (conducted in the 1970s through 1990s) did not employ the use of STPs to identify potential subsurface cultural resources, whereas the use of STPs is standard practice in current cultural resource surveys when a project will include ground disturbance. Potential impacts to archaeological resources as a result of construction include excavation, sediment disturbance, sediment compaction, and other ground-disturbing construction activities.

Construction of a new road and bridge have the potential to impact viewsheds in the surrounding area. It is anticipated that a historic property inventory may be required in addition to a cultural resource inventory, depending on funding requirements.

Funding Requirements

The funding agency will determine which environmental permits and consultation will be required for the proposed project. If federal funding is utilized, an Environmental Assessment will be required. Since a portion of the Punkin Center Road option corridor occurs on BLM-owned land, the project may be subject to BLM NEPA requirements and an extensive environmental review process.

Advantages

The Punkin Center Road option is not anticipated to impact the following Goal 5 resources: federal Wild and Scenic Rivers, State Scenic Waterways, groundwater resources, approved Oregon recreation trails, natural areas, wilderness areas, mineral and aggregate resources, and energy sources. The Punkin Center Road option is anticipated to have fewer wetland and waterway impacts than the Elm Avenue option. Less vegetation removal will be required than the Elm Avenue option. Upon project completion, the Punkin Center Road option would result in a smaller amount of impervious surface than the Elm Avenue option. Fewer DEQ-permitted sites are located adjacent to the project corridor than the Elm Avenue option, resulting in less chance of encountering contaminated soils. The Punkin Center Road option is located farther downstream on the Umatilla River and, therefore, would result in fewer downstream impacts than the Elm Avenue option.

Disadvantages

The Punkin Center Road option could result in impacts to the following Goal 5 resources: riparian corridors, wetlands, wildlife habitat, and cultural areas. A portion of the project corridor would be located within the floodway, 100-year floodplain, and 500-year floodplain; this is anticipated to require detailed hydraulic modeling and a no-rise certificate. The project would require permanent conversion of farmland. A portion of the project corridor is zoned EFU, so it may be challenging to gain land use approval from the County for development. ESA-listed species occur within the project corridor. A BLM-owned natural area is located adjacent to the Punkin Center Road option. A major disadvantage to the Punkin Center Road option is that a portion of the project corridor may occur on BLM-owned land and, therefore, may be subject to BLM NEPA requirements and an extensive environmental review process.

Permits

Table 5-5 lists potential permits and environmental clearances that could be required for the Punkin Center Road option.

**TABLE 5-5
PUNKIN CENTER ROAD OPTION ENVIRONMENTAL PERMITS**

| Permit | Approving Agency |
|-------------------------------------------------------|--------------------------------------------------------------------------|
| County/City Permits/Other | |
| Conditional Use Permit | Umatilla County and the City of Hermiston |
| Goal 3 Exception Process | Umatilla County |
| Floodplain Development Permit and No-Rise Certificate | Umatilla County and the City of Hermiston |
| State Permits/Consultation | |
| DSL Removal-Fill Permit | DSL |
| Wetland Delineation Concurrence | DSL |
| Post-Construction Stormwater Management Plan | DEQ |
| 1200-C Construction Stormwater General Permit | DEQ |
| 401 Water Quality Certification | DEQ |
| ODFW Fish Passage Approval | ODFW |
| ODFW Consultation (Ground Squirrel Survey) | ODFW |
| Beneficial Use Determination | ODOT |
| Noise Analysis | ODOT |
| 6(f) Land Consultation | OPRD |
| Federal Permits/Consultation | |
| 404 Removal-Fill Permit | USACE |
| Section 7 ESA Consultation | USACE, USFWS, and NMFS |
| BLM NEPA Process (Environmental Assessment) | BLM |
| Section 106 Consultation | SHPO/Tribes as approved by the Legislative Commission on Indian Services |
| CLOMR | FEMA |
| Farmland Conversion Impact Consultation | NRCS |

Cost

Table 5-6 lists the estimated costs for potential permits and environmental clearances that could be required for the Punkin Center Road option (2022 dollars).

**TABLE 5-6
PUNKIN CENTER ROAD OPTION ENVIRONMENTAL COSTS**

| Task Description | Estimated Cost |
|----------------------------------------------------------------------------------------------|----------------|
| County/City Permits/Other | |
| Floodplain Development Permit and No-Rise Certificate (assumes hydraulic modeling completed) | \$5,000 |
| Conditional Use Permit Application (Goal 3 Exception) | \$30,000 |
| State Permits/Consultation | |
| Wetland Delineation | \$10,000 |
| Wetland/Waterway Mitigation Plan | \$20,000 |
| Stream Functional Assessment Method | \$6,000 |
| Oregon Rapid Wetland Assessment Protocol | \$4,000 |
| Ground Squirrel Survey | \$5,000 |
| Fish Salvage | \$10,000 |
| ODFW Fish Passage Approval | \$10,000 |
| Post-Construction Stormwater Management Plan | \$10,000 |

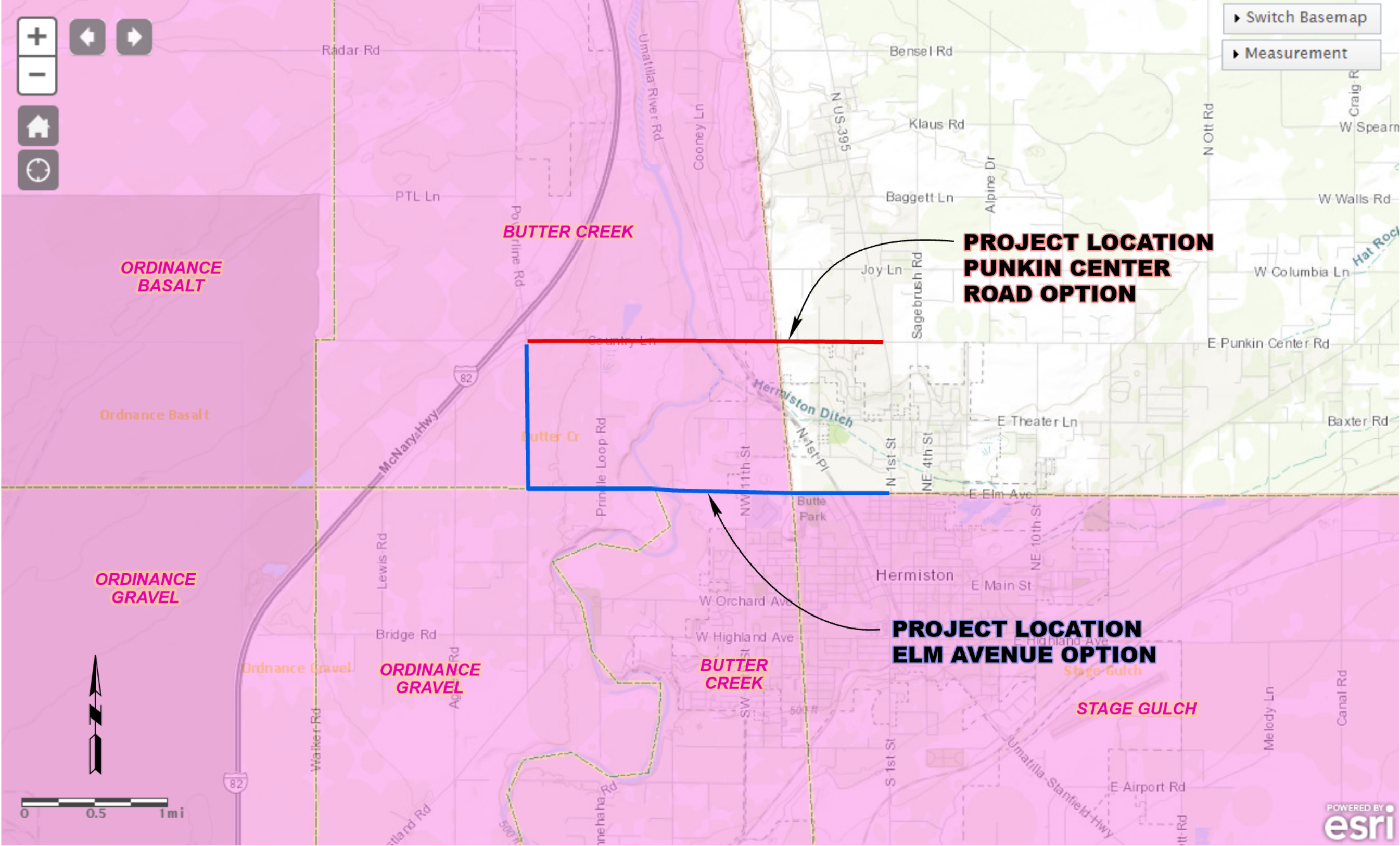
| | |
|--------------------------------------------------------------|----------------------------|
| 1200-C Construction Stormwater General Permit | \$8,000 |
| ODOT Noise Analysis | \$30,000 |
| Phase 1 Environmental Site Assessment/ODOT Level 1 HMCS | \$10,000 |
| ODOT Level 2 Preliminary Site Investigation | Depends on Level 1 Results |
| Federal Permits/Consultation | |
| CLOMR/LOMR | See Chapter 3 |
| Joint Permit Application | \$16,000 |
| Biological Assessment | \$20,000 |
| BLM NEPA Process (Environmental Assessment) | \$75,000 |
| Historic Property Inventory | \$30,000 |
| Cultural Resource Phase I (STP and Pedestrian) | \$75,000 |
| Cultural Resource Phase II (Ten Sites) | \$150,000 |
| Cultural Resource Phase III | Depends on Phase II |
| Cultural Resource Monitoring (Assumes 100 Construction Days) | \$100,000 |
| Total | \$624,000 |

Table 5-7 lists the advantages and disadvantages of the Elm Avenue option and the Punkin Center Road option.

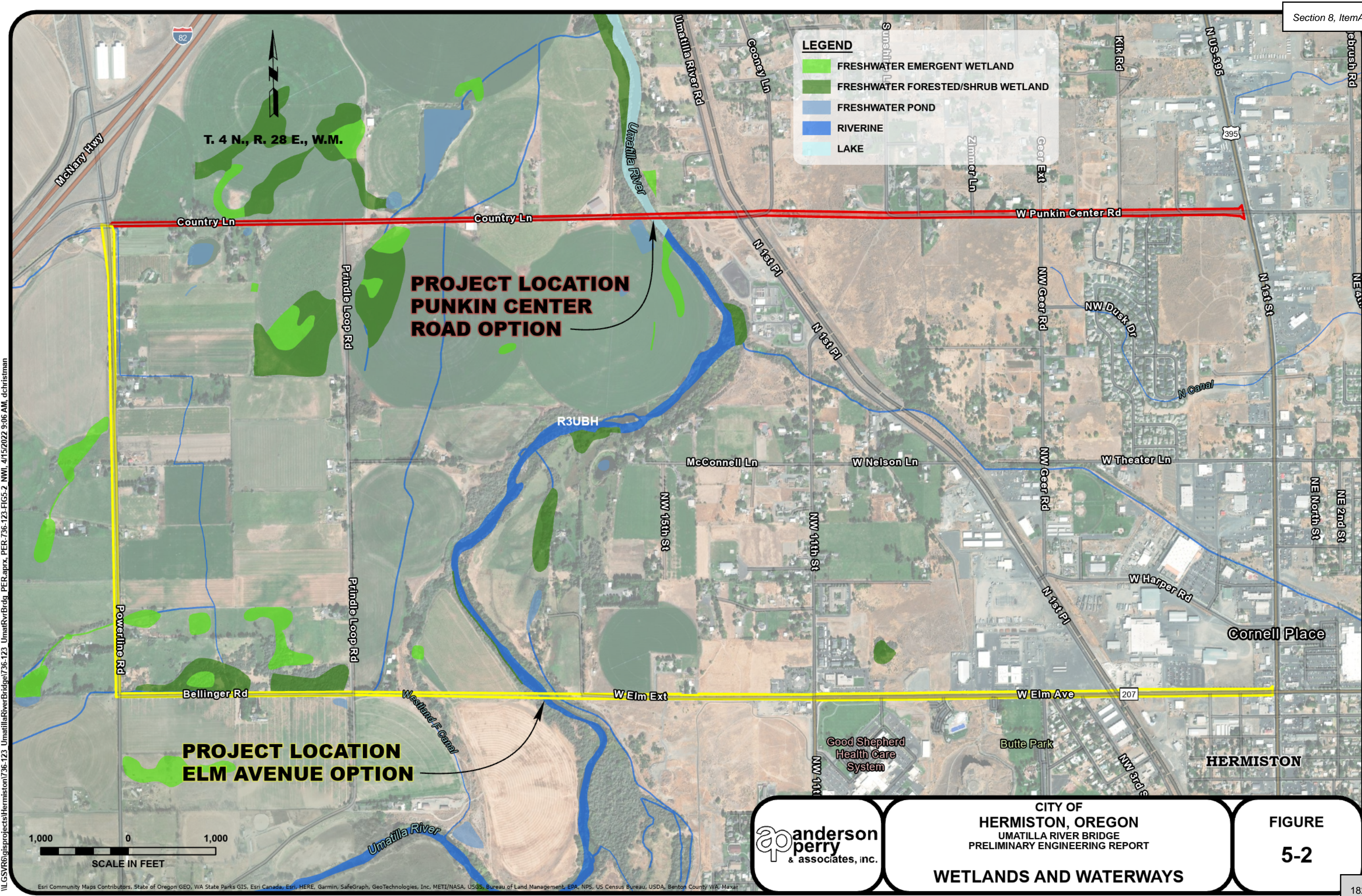
TABLE 5-7
ADVANTAGES AND DISADVANTAGES

| Elm Avenue Option | | Punkin Center Road Option | |
|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| Advantages | Disadvantages | Advantages | Disadvantages |
| <ul style="list-style-type: none"> • Likely will not affect the following Goal 5 resources: federal Wild and Scenic Rivers, State Scenic Waterways, groundwater resources, approved Oregon recreation trails, natural areas, wilderness areas, mineral and aggregate resources, and energy sources • Does not occur on federally owned land | <ul style="list-style-type: none"> • May affect the following Goal 5 resources: riparian corridors, wetlands, wildlife habitat, and cultural areas • May require demolition of houses or structures • More potential wetland and waterway impacts • Potential federally listed species impacts • Larger project corridor, more disturbance • Would require permanent conversion of farmland within EFU zoning designation • Would occur within the floodway, 100-year floodplain, and 500-year floodplain • Located farther upstream, resulting in more downstream impacts | <ul style="list-style-type: none"> • Likely will not affect the following Goal 5 resources: federal Wild and Scenic Rivers, State Scenic Waterways, groundwater resources, approved Oregon recreation trails, natural areas, wilderness areas, mineral and aggregate resources, and energy sources • Fewer potential wetland and waterway impacts • Less vegetation removal would be required • Smaller project corridor, reduced disturbance • Smaller amount of impervious surface created | <ul style="list-style-type: none"> • May affect the following Goal 5 resources: riparian corridors, wetlands, wildlife habitat, and cultural areas • May occur on BLM-owned land • May require demolition of houses or structures • Potential wetland and waterway impacts • Potential federally listed species impacts • Would require permanent conversion of farmland within EFU zoning designation • Would occur within the floodway, 100-year floodplain, and 500-year floodplain |


| | | | |
|--|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|--|
| | <ul style="list-style-type: none">• More vegetation removal would be required• Greater amount of impervious surface created• May impact Butte Park (LWCF funded)• Greater chance of encountering contaminated soils | <ul style="list-style-type: none">• Less chance of encountering contaminated soils• Located farther downstream, resulting in fewer downstream impacts | |
|--|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|--|



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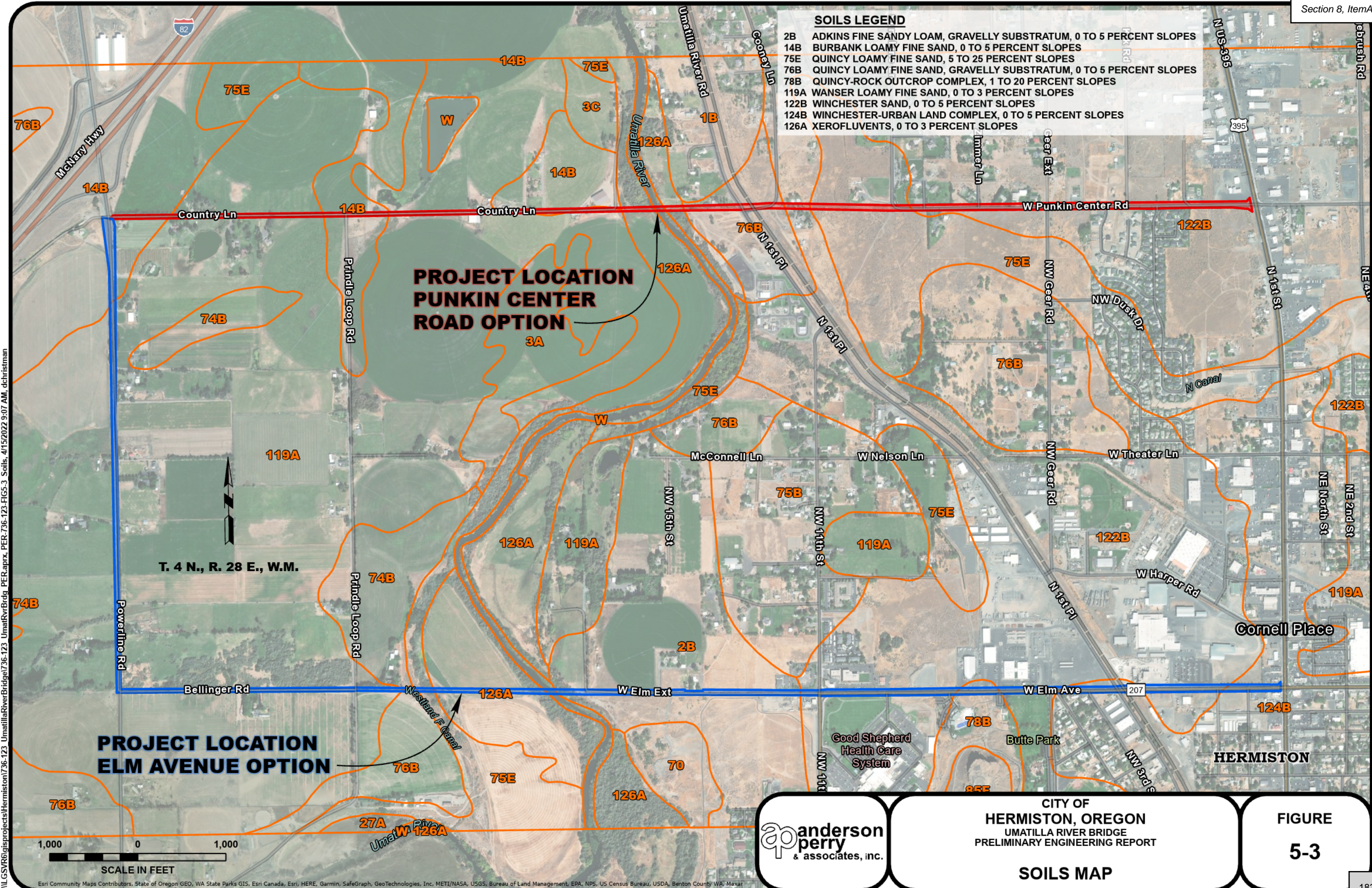
**CITY OF
HERMISTON, OREGON**
UMATILLA RIVER BRIDGE
PRELIMINARY ENGINEERING REPORT

**FIGURE
5-2**

WETLANDS AND WATERWAYS

SOILS LEGEND

- 2B ADKINS FINE SANDY LOAM, GRAVELLY SUBSTRATUM, 0 TO 5 PERCENT SLOPES
- 14B BURBANK LOAMY FINE SAND, 0 TO 5 PERCENT SLOPES
- 75E QUINCY LOAMY FINE SAND, 5 TO 25 PERCENT SLOPES
- 76B QUINCY LOAMY FINE SAND, GRAVELLY SUBSTRATUM, 0 TO 5 PERCENT SLOPES
- 78B QUINCY-ROCK OUTCROP COMPLEX, 1 TO 20 PERCENT SLOPES
- 119A WANSER LOAMY FINE SAND, 0 TO 3 PERCENT SLOPES
- 122B WINCHESTER SAND, 0 TO 5 PERCENT SLOPES
- 124B WINCHESTER-URBAN LAND COMPLEX, 0 TO 5 PERCENT SLOPES
- 126A XEROFLUVENTS, 0 TO 3 PERCENT SLOPES



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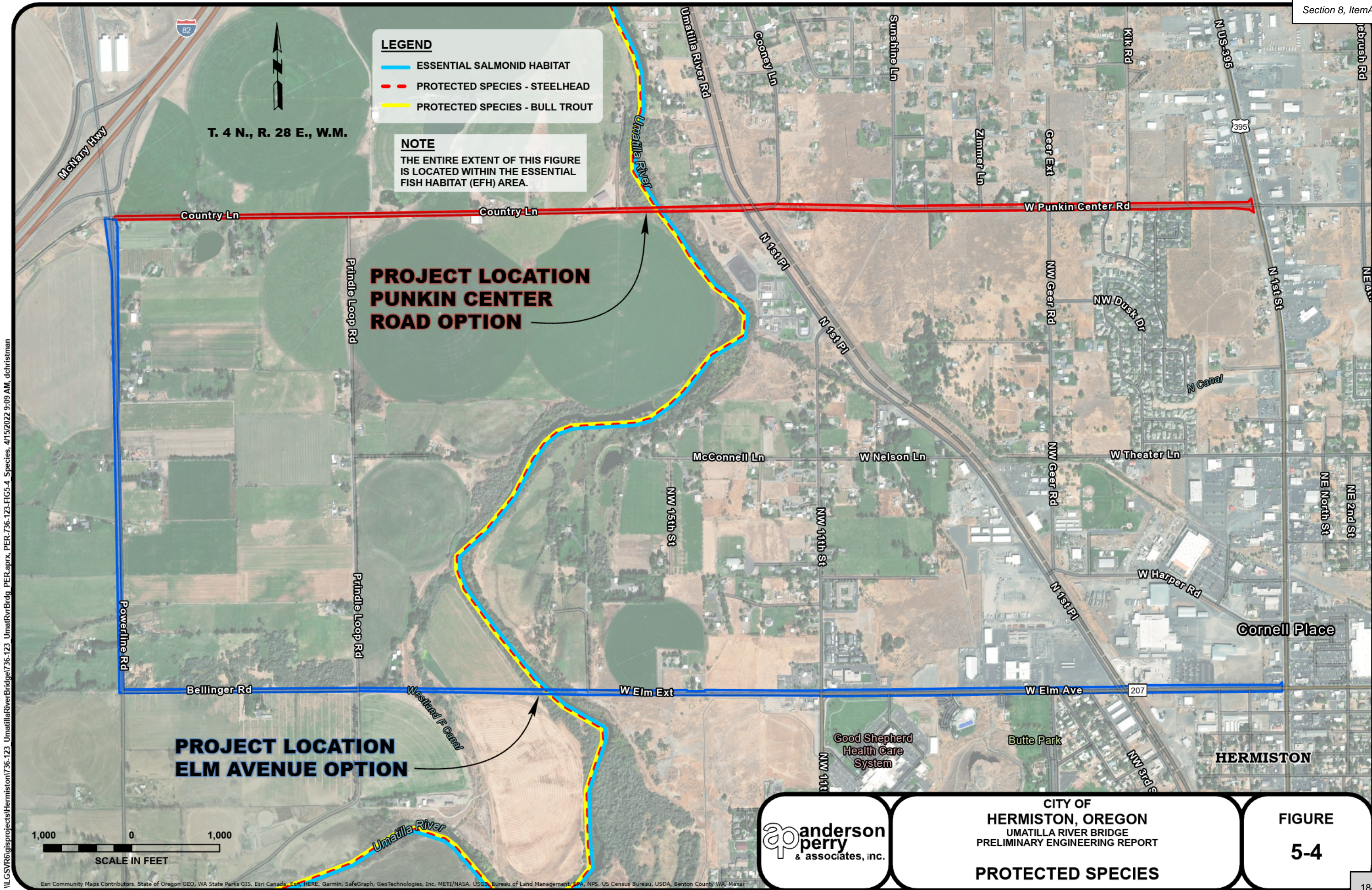
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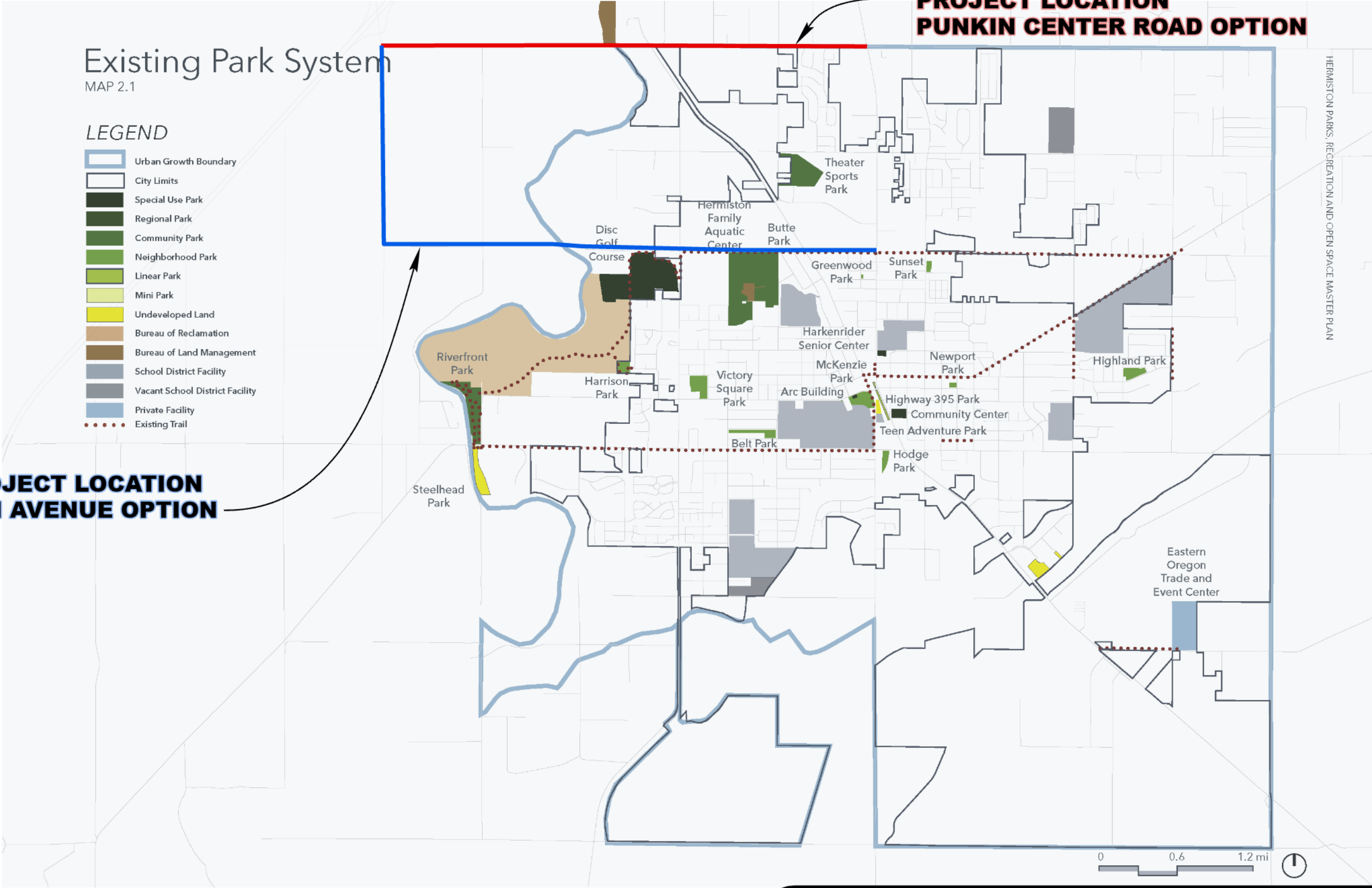
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PRELIMINARY ENGINEERING REPORT

SOILS MAP

FIGURE
5-3



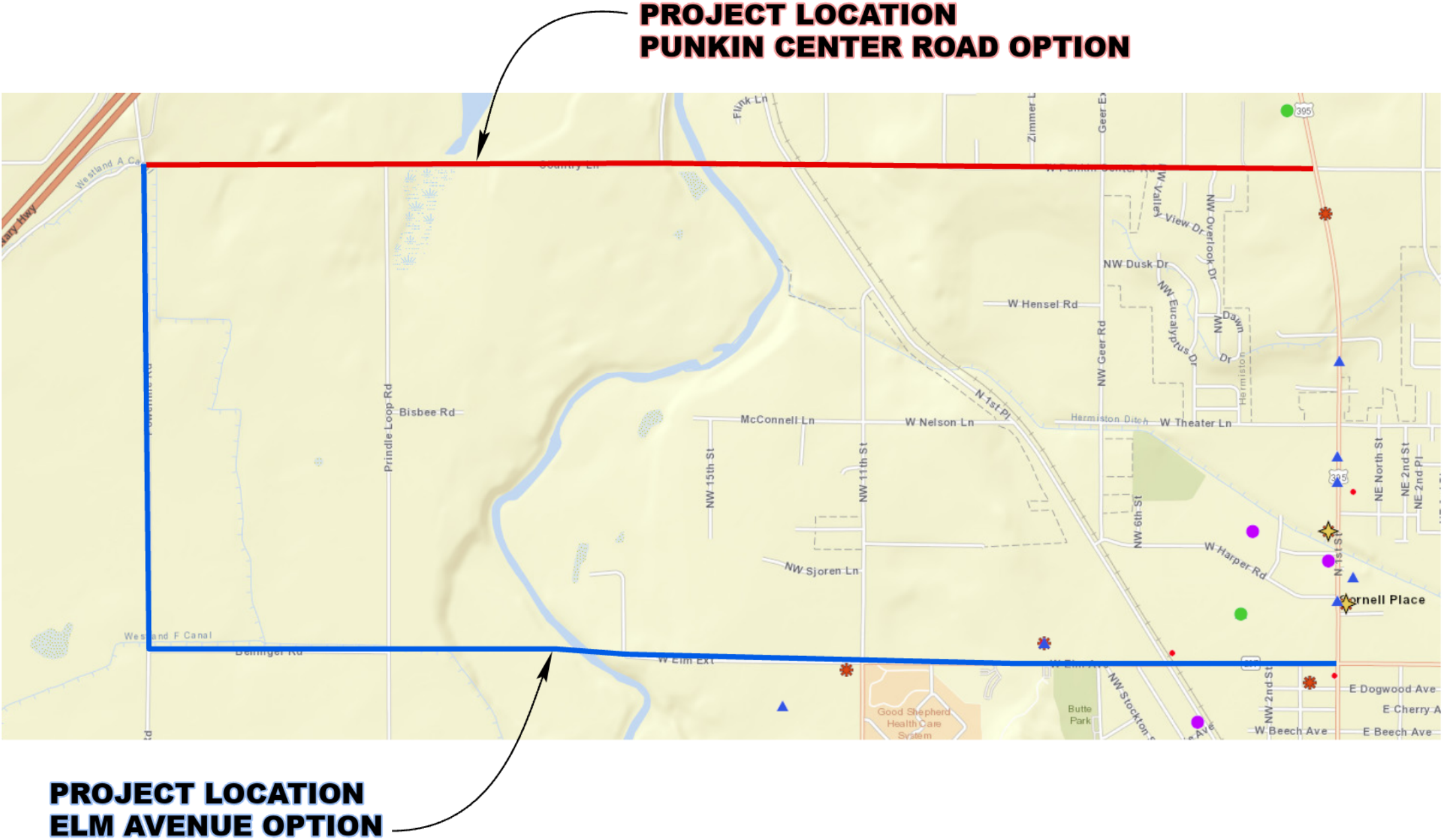
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- Environmental Cleanup Site Information (ECSI)
 - Contaminated Site, Listed on CRL or Inventory
 - Contaminated Site, No further action required
 - Contaminated Site, Suspect site requiring further investigation
 - Study Area, Listed on CRL or Inventory
 - Study Area, Suspect site requiring further investigation
- Hazardous Waste (HAZWASTE)
 - Generator
 - TSD
- Leaking Underground Storage Tanks (LUST)
 - Regulated LUST - Cleanup started
 - Regulated LUST - Reported
 - Regulated LUST - Cleanup completed
 - Non-regulated LUST - Cleanup started
 - Non-regulated LUST - Reported
 - Non-regulated LUST - Cleanup completed
 - Underground Storage Tanks (UST)



Chapter 6 - Selected Improvements and Project Implementation

A. Selected Option

B. Project Implementation

Potential Funding Sources

Action Items

Chapter 7 - References

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Appendices Table of Contents

Appendix A - New East-West Roadway Circulation Analysis Technical Memo

Appendix B - Cost Estimates for Infrastructure Improvements and Land Acquisition

Appendix C - City of Hermiston, Oregon, Major Arterial Standard Drawing

APPENDIX A

New East-West Roadway Circulation Analysis

Technical Memo by Kittelson & Associates, Inc.

Technical Memorandum

March 4, 2022

Project# 26970

To: Joshua Lott, PE; Chas Hutchins, PE
Anderson Perry & Associates, Inc.
243 E Main St, Suite C
Hermiston, OR 97838

From: Matt Hughart, AICP; Amy Griffiths, EIT; Chris Brehmer, P.E.
Kittelson & Associates, Inc.

RE: New East-West Roadway Circulation Analysis

EXECUTIVE SUMMARY

The City of Hermiston is exploring the possibility of a future travel corridor that would span the Umatilla River and provide an additional east-west connection to Powerline Road. The following two corridor options are being considered:

- A westerly extension of the W Punkin Center Road corridor
- A westerly extension of the W Elm Avenue corridor

This report documents the motorized circulation and operational impacts associated with these two corridor options for inclusion in the Umatilla River Bridge Preliminary Engineering Report (PER).

The following transportation conditions were analyzed for the study intersections shown in Figure 1:

- Existing Traffic Conditions
- 2032 No-Build Traffic Conditions (without either of the two corridor options but still considering local and regional transportation growth on the existing infrastructure network)
- 2032 Build Conditions with a W Punkin Center Road Extension
- 2032 Build Conditions with a W Elm Avenue Extension

As documented in greater detail herein, this analysis has generated the following findings and recommendations.

Findings

EXISTING TRAFFIC CONDITIONS

- All of the study intersections meet the applicable ODOT mobility targets and City of Hermiston operating standards during the AM and PM study hours.

2032 NO-BUILD CONDITIONS

- Annual traffic growth rates between 1.5% and 3.0% were conservatively applied to the study corridors and intersections to account for anticipated local and regional growth.

- Under 2032 No-Build conditions, all of the study intersections are forecast to meet the applicable ODOT mobility targets and City of Hermiston operating standards during the AM and PM study hours with the exception of US 395/W Elm Avenue (OR 207) intersection. During the weekday PM peak hour, this intersection is forecast to operate at a volume-to-capacity ratio (V/C) of 0.93 which exceeds ODOT's 0.90 mobility target.

2032 W PUNKIN CENTER ROAD EXTENSION

An extension of W Punkin Center Road is anticipated to result in operational impacts at the following intersections/study corridors:

- US 395/W Punkin Center Road - While projected to continue meeting the operational mobility target, the intersection is forecast to serve a significantly greater level of turning movement demand under the W Punkin Center Road extension option.
 - To better serve this projected demand and manage expected long-term vehicle queues, the intersection would benefit from the construction of a separate eastbound right-turn lane.
- US 395/W Elm Avenue (OR 207) - As with the 2032 No-Build conditions, the intersection is forecast to continue to exceed the 0.90 ODOT mobility target. During the weekday PM peak hour, the intersection is forecast to operate at a V/C of 0.96 which represents a slight degradation in operations compared to 2032 No-Build conditions.
 - To better serve the projected demand, the construction of a northbound right-turn lane, a southbound right-turn lane, and a westbound right-turn lane would improve operations of the intersection and result in an acceptable V/C of 0.76. While these improvements would benefit the long-term operations of the intersection, their accommodation would be difficult and costly considering the lack of public right-of-way and impacts to existing businesses.
- River Road/W Punkin Center Road - A westerly extension of W Punkin Center Road is assumed to form a new intersection with River Road in the approximate location of the existing River Road/Cooney Lane intersection. The following improvement would be needed to accommodate the long-term turning movement demands at this intersection:
 - Construction of separate left-turn and shared through/right-turn lanes on all four intersection approaches.
 - When warranted, construction of a traffic signal.
 - Upgrade and modernization of the adjacent Union Pacific Railroad crossing.
 - Realignment of Cooney Lane to intersect the westerly extension of W Punkin Center Road instead of directly connecting to River Road.
- Country Lane Corridor –
 - Realign Country Lane and form a new intersection with Powerline Road to increase spacing from the I-82 interchange. The alignment and spacing should be determined through a separate multi-agency Interchange Area Management Plan (IAMP).
 - Upgrade the Country Lane corridor to two paved twelve-foot travel lanes (widened to include left- or right-turn lanes at major crossroads), five-foot shoulders/bike lanes, curb and gutter, and five-foot sidewalks.
 - Upgrade the right-of-way width to 90 feet to allow for a future upgrade to an Urban Major Arterial per City of Hermiston Standard Drawing ST07.
- Powerline Road/Country Lane Intersection - With a Punkin Center Road extension to Country Lane, there will be an increase in travel demand at the Powerline Road/Country Lane intersection. This increase will necessitate the following improvements which are recommended to be more fully explored as part of a separate IAMP with ODOT and Umatilla County:
 - Construction of a southbound left-turn lane on Powerline Road.

- Construction of westbound right-turn and left-turn lanes on Country Lane.
- I-82 SB Ramp Terminal/Powerline Road – A Punkin Center Road extension is anticipated to generate additional travel demand through the I-82/Powerline Road interchange. As a result, the SB ramp terminal approach is forecast to operate at a V/C of 0.95 which exceeds the 0.85 mobility target. This forecast operating condition and potential traffic control improvements should be addressed as part of a more comprehensive Interchange Area Management Plan.
- Powerline Road Corridor –
 - Upgrade the Powerline Road corridor from the I-82 interchange to the realigned Country Lane intersection to two paved twelve-foot travel lanes (widened to include a southbound left-turn lane at Country Lane), five-foot shoulders/bike lanes, curb and gutter, and five-foot sidewalks.
 - Upgrade the right-of-way width to 90 feet to allow for a future upgrade to an Urban Major Arterial per City of Hermiston Standard Drawing ST07.

2032 W ELM AVENUE EXTENSION

An extension of W Elm Avenue is anticipated to result in operational impacts at the following intersections/study corridors:

- US 395/W Elm Avenue (OR 207) – A W Elm Avenue extension is anticipated to significantly increase turning movement volumes at the US 395/W Elm Avenue intersection resulting in over capacity conditions during the weekday PM peak hour.
 - To better serve the projected demand, the construction of a northbound right-turn lane, a southbound right-turn lane, and a westbound right-turn lane would significantly improve operations of the intersection and result in an acceptable V/C of 0.90. While these improvements would benefit the long-term operations of the intersection, their accommodation would be difficult and costly considering the lack of public right-of-way and impacts to existing businesses.
- W Elm Avenue (OR 207)/1st Place – A W Elm Avenue extension would increase traffic demands on W Elm Avenue. While the intersection is forecast to continue to meet the mobility target, the intersection will not have much long-term flexibility to accommodate additional traffic growth. To increase the flexibility, the following improvements are recommended:
 - Construction of a southbound right-turn lane on 1st Place.
 - Construction of an eastbound right-turn lane on W Elm Avenue.
- W Elm Avenue (OR 207)/NW 11th Street – A W Elm Avenue extension would add a significant amount of travel demand to the underutilized west leg of the intersection. During the weekday PM peak hour, the intersection is forecast to operate at a V/C of 0.94 which exceeds the 0.90 mobility target.
 - To better serve the projected demand, the construction of an eastbound right-turn lane would improve intersection operations and result in an acceptable V/C of 0.89.
- Powerline Road / Bellinger Road - With a W Elm Avenue extension, there will be a significant increase in travel demand at the Powerline Road/Bellinger Road intersection. This increase will necessitate the need for the following improvements:
 - Construction of a southbound left-turn lane on Powerline Road.
 - Construction of westbound right-turn and left-turn lanes on Bellinger Road.
- Bellinger Road Corridor –
 - Upgrade the Bellinger Road corridor to two paved twelve-foot travel lanes (widened to include left- or right-turn lanes at major crossroads), five-foot shoulders/bike lanes, curb and gutter, and five-foot sidewalks.

- Upgrade the right-of-way width to 90 feet to allow for a future upgrade to an Urban Major Arterial per City of Hermiston Standard Drawing ST07.
- Powerline Road Corridor –
 - Upgrade the Powerline Road corridor from the I-82 interchange to Bellinger Road to two paved twelve-foot travel lanes (widened to include left- or right-turn lanes at major crossroads), five-foot shoulders/bike lanes, curb and gutter, and five-foot sidewalks.
- I-82 SB Ramp Terminal/Powerline Road – A W Elm Avenue extension is anticipated to generate additional travel demand through the I-82/Powerline Road interchange. As a result, the increase in travel demand will likely necessitate an upgrade and modernization of the interchange ramp terminals. The specific improvements should be addressed as part of a more comprehensive Interchange Area Management Plan.

Additional details of the methodology, findings, and recommendations are provided herein.

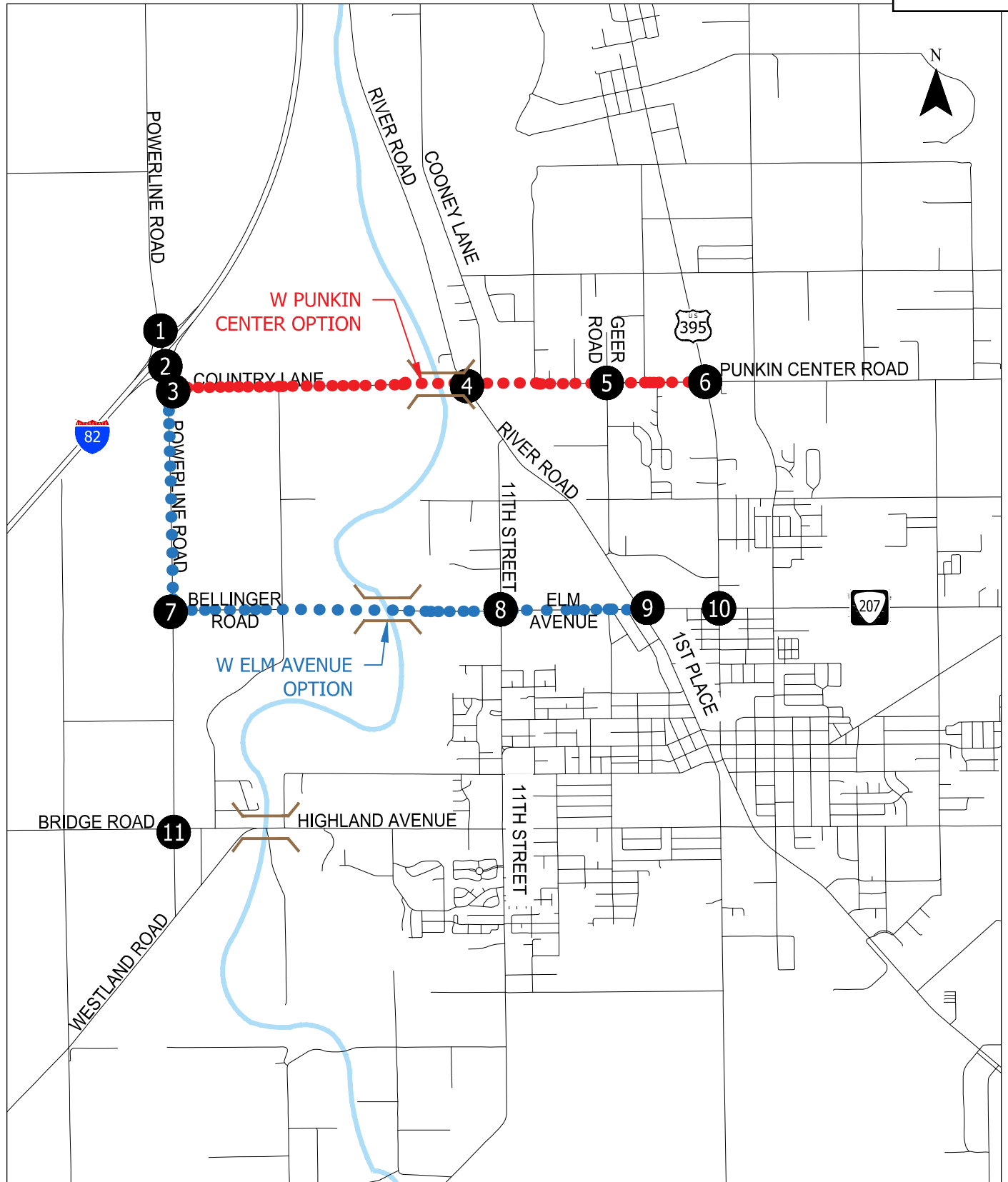
INTRODUCTION

The City of Hermiston is exploring the feasibility of extending one of two existing roadway corridors across the Umatilla River and connecting to the existing Powerline Road corridor to improve local and regional east-west travel connectivity.

W Punkin Center Option would involve the westerly extension of W Punkin Center Road from its current terminus at Sunshine Lane, forming a new intersection at River Road, spanning the Umatilla River on a new bridge, merging with the existing Country Lane corridor, and ultimately connecting to Powerline Road at an appropriate location south of the I-82/Powerline Road interchange.

W Elm Avenue Option would involve the westerly extension of W Elm Avenue from its current terminus west of the NW 11th Street intersection, spanning the Umatilla River on a new bridge, merging with the existing Bellinger Road corridor, and connecting with Powerline Road.

Figure 1 illustrates a study area map and conceptual alignments for the two study corridors. Additional details regarding these corridors are documented later in this technical memorandum.



LEGEND

- STUDY INTERSECTION

Study Area
Hermiston, Oregon

Figure
1

SCOPE OF THE REPORT

This analysis determines the high-level transportation-related impacts associated with the two corridor extension options. The study intersections were selected to assess the anticipated local and regional transportation impacts associated with each option. They include:

- I-82 Northbound Ramp Terminal/Powerline Road
- I-82 Southbound Ramp Terminal/Powerline Road
- Powerline Road/Country Lane
- Umatilla River Road/Cooney Lane
- Punkin Center Road/Geer Road
- US 395/Punkin Center Road
- Powerline Road/Bellinger Road
- Elm Avenue (OR 207)/11th Street
- Elm Avenue (OR 207)/1st Place
- US 395/Elm Avenue (OR 207)
- Powerline Road/Bridge Road

This report evaluates the following transportation scenarios at each study intersection:

- 2021 Existing Traffic Conditions
- 2032 No-Build Traffic Conditions (without either of the two corridor options but still considering transportation growth on the existing infrastructure network)
- 2032 Build Conditions with a W Punkin Center Road Extension
- 2032 Build Conditions with a W Elm Avenue Extension

Analysis Methodology

The signalized and stop-controlled intersection operational analyses presented in this report were prepared following Highway Capacity Manual (HCM) 6th Edition analysis procedures using PTV Vistro 2022 software in accordance with the ODOT Analysis Procedures Manual (APM). The observed peak hour factor was used for the existing traffic analyses.

Applicable Performance Measures & Operating Standards/Targets

Intersection performance measures reported in this study include, but are not limited to, level of service (LOS), volume to capacity (V/C), and delay. Intersection operating targets adopted by the Oregon Department of Transportation (ODOT) and the City of Hermiston are summarized below.

ODOT MOBILITY TARGETS

ODOT uses V/C to assess intersection operations. Table 6 of the *Oregon Highway Plan* (OHP) provides maximum volume-to-capacity ratio targets for all signalized and unsignalized intersections located outside the Portland metropolitan area. Based on the OHP, Table 1 summarizes the mobility target (V/C) used to assess intersection operations at the ODOT owned/maintained study intersections.

Table 1: ODOT Mobility Targets

| Intersection | OHP Mobility Target |
|--------------------------------------------------|-----------------------------------------------------------------|
| I-82 Northbound Ramp Terminal/ Powerline Road | V/C: 0.85 Powerline Road Approach V/C: 0.85 Offramp Approach |
| I-82 Southbound Ramp Terminal/ Powerline Road | V/C: 0.85 Powerline Road Approach V/C: 0.85 Offramp Approach |
| US 396 / Punkin Center Road | V/C \leq 0.80 |
| US 395 / Elm Avenue (OR 207) | V/C \leq 0.90 |
| Elm Avenue (OR 207) / 11 th Street | V/C \leq 0.90 |
| Elm Avenue (OR 207) / 1 st Place | V/C \leq 0.90 |

CITY OF HERMISTON OPERATING STANDARDS

Traffic operations at City intersections are described using a measure known as level of service (LOS). LOS represents ranges in the average amount of delay that motorists experience when passing through the intersection. The City of Hermiston has adopted LOS D or better for all signalized and unsignalized intersections.

EXISTING CONDITIONS ANALYSIS

This section summarizes the existing characteristics of the transportation system at key intersections that are most likely to be impacted by the two W Elm Avenue or W Punkin Center Road corridor options.

Transportation Facilities

Table 2 provides a summary of the transportation facilities in the site vicinity. Figure 2 illustrates the existing lane configurations and traffic control devices at the study intersections.

Table 2: Existing Transportation Facilities and Roadway Designations

| Roadway | Classification (bold indicates jurisdictional ownership) | Cross Section | Posted Speed (mph) | Sidewalks Present | Bike Lanes Present? |
|----------------------------------------|-------------------------------------------------------------------------------|---------------|--------------------|-------------------|---------------------|
| I-82 | Interstate Highway – ODOT | 4 lanes | 70 | None | None |
| Powerline Road | Local Road – Umatilla County Rural Collector - Hermiston | 2 lanes | 40 | None | None |
| Country Lane | Local Road – Umatilla County | 2 lanes | Not Posted | None | None |
| Bellinger Road | Local Road – Umatilla County | 2 lanes | Not Posted | None | None |
| Bridge Road | Local Road – Umatilla County Rural Collector Street - Hermiston | 2 lanes | 40 | None | None |
| River Road | Major Collector – Umatilla County Urban Major Collector - Hermiston | 2 lanes | 45 | None | None |
| W Punkin Center Road | Urban Major Collector - Hermiston | 2 lanes | 40 | Partial | None |
| US 395 | Statewide Highway – ODOT Urban Major Arterial - Hermiston | 5 lanes | 30-45 | Yes | None |
| W Elm Avenue (OR207) | Regional Highway - ODOT | 3 lanes | 30 | Partial | Partial |
| 11 th Street (north of Elm) | Regional Highway - ODOT Urban Minor Arterial - Hermiston | 2 lanes | 35 | Partial | Partial |
| 1 st Place | Urban Major Collector - Hermiston | 2 lanes | 35 | None | None |

Traffic Volumes and Peak Hour Operations

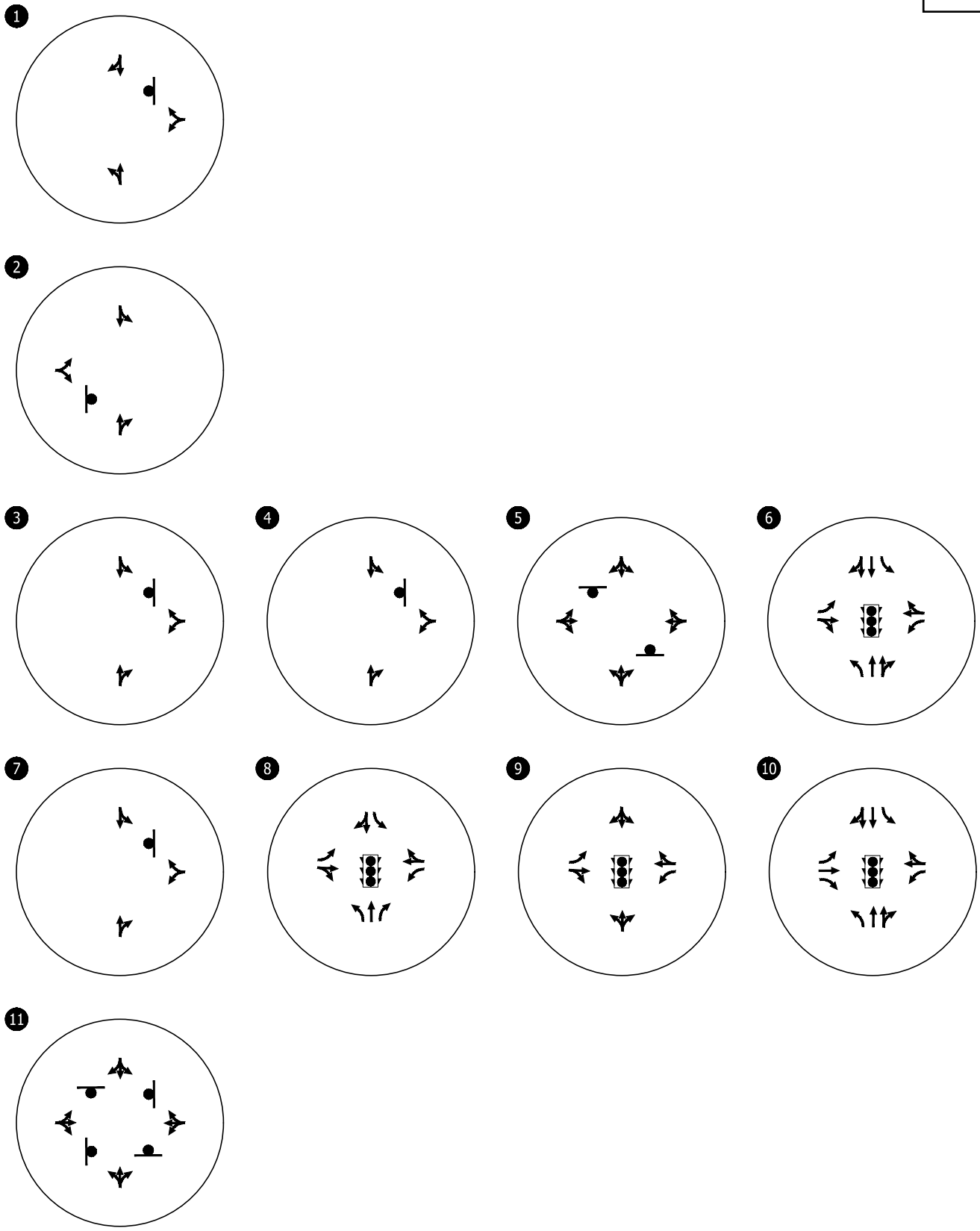
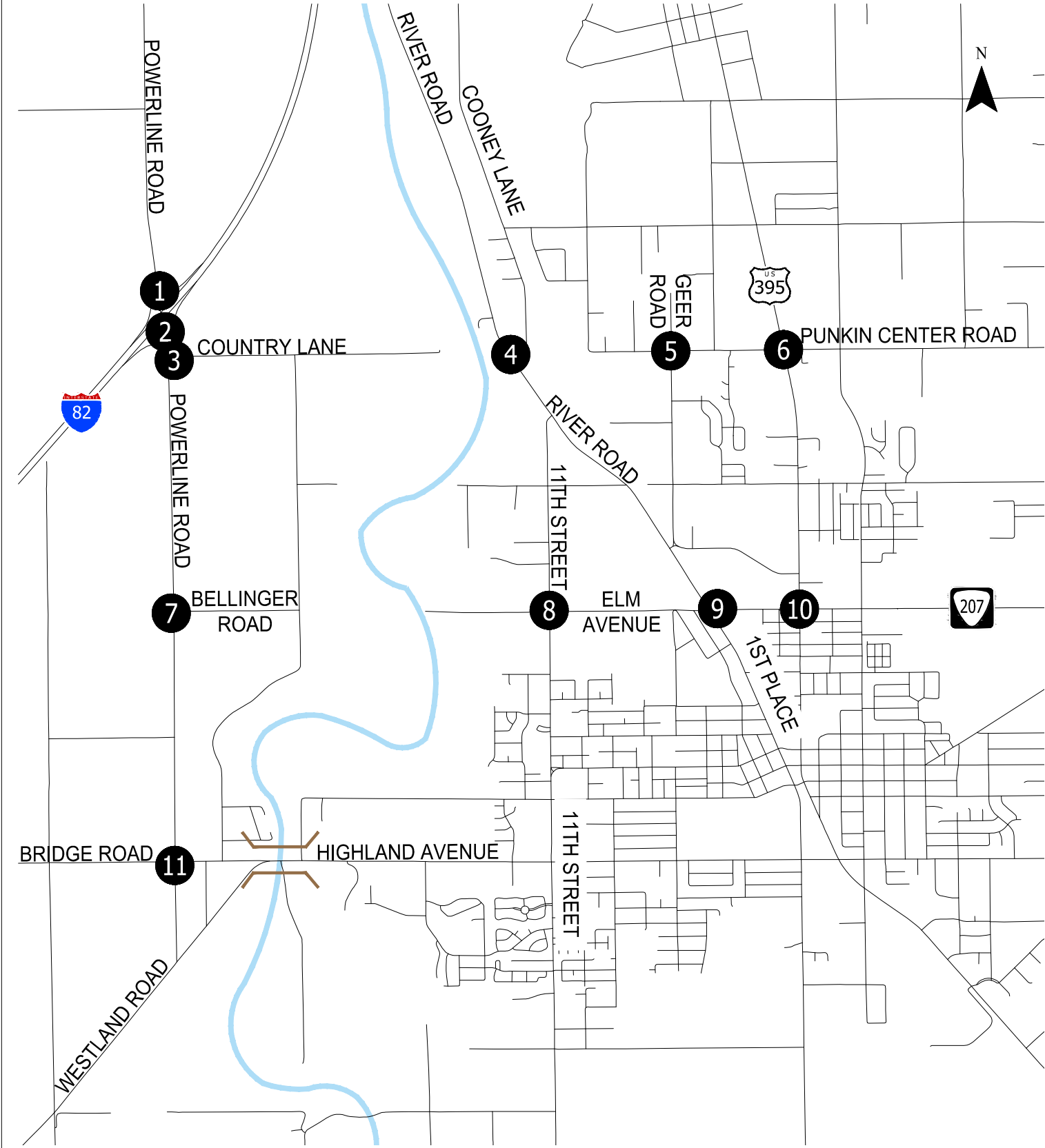
Manual turning-movement counts were collected at the study intersections in October 2021. The traffic counts were conducted on a typical mid-week day (while local schools were in session) from 7:00 to 9:00 AM and 4:00 to 6:00 PM in order to capture the anticipated weekday AM and PM peak hour operation conditions. *Appendix A contains the traffic count worksheets used in this study.* The following sections summarize how the volumes were adjusted to account for seasonality.

SEASONAL ADJUSTMENTS

Per the ODOT *Analysis Procedures Manual (APM)* (Chapter 5.2), the existing traffic volumes at intersection movements on state facilities were seasonally adjusted to represent the 30th highest hour volumes. The On-site Automatic Traffic Recorder (ATR) method was used for two different ATR locations within the site vicinity and averaged to arrive at a seasonal adjustment factor of 1.09. *Appendix B includes the detailed methodology and calculations for the 30th highest hour adjustments.*

EXISTING INTERSECTION OPERATIONS

Figure 3 and Figure 4 illustrate the adjusted existing turning movement counts and operational conditions for the weekday AM and PM peak hours. *Appendix C contains the existing conditions operations analysis worksheets.* As shown, all of the study intersection operations meet the applicable ODOT mobility targets and City of Hermiston operating standards during the weekday AM and PM study hours.



LEGEND

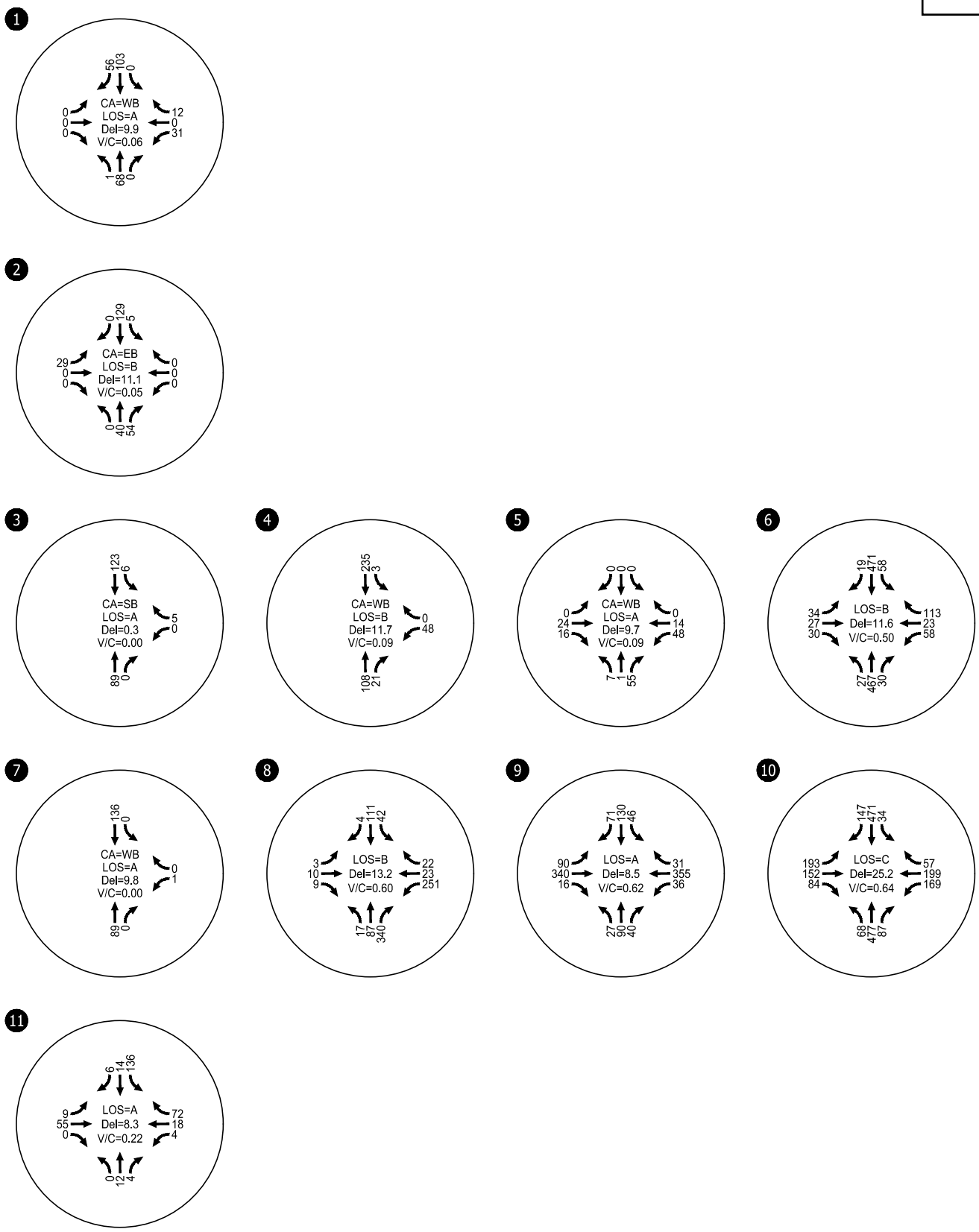
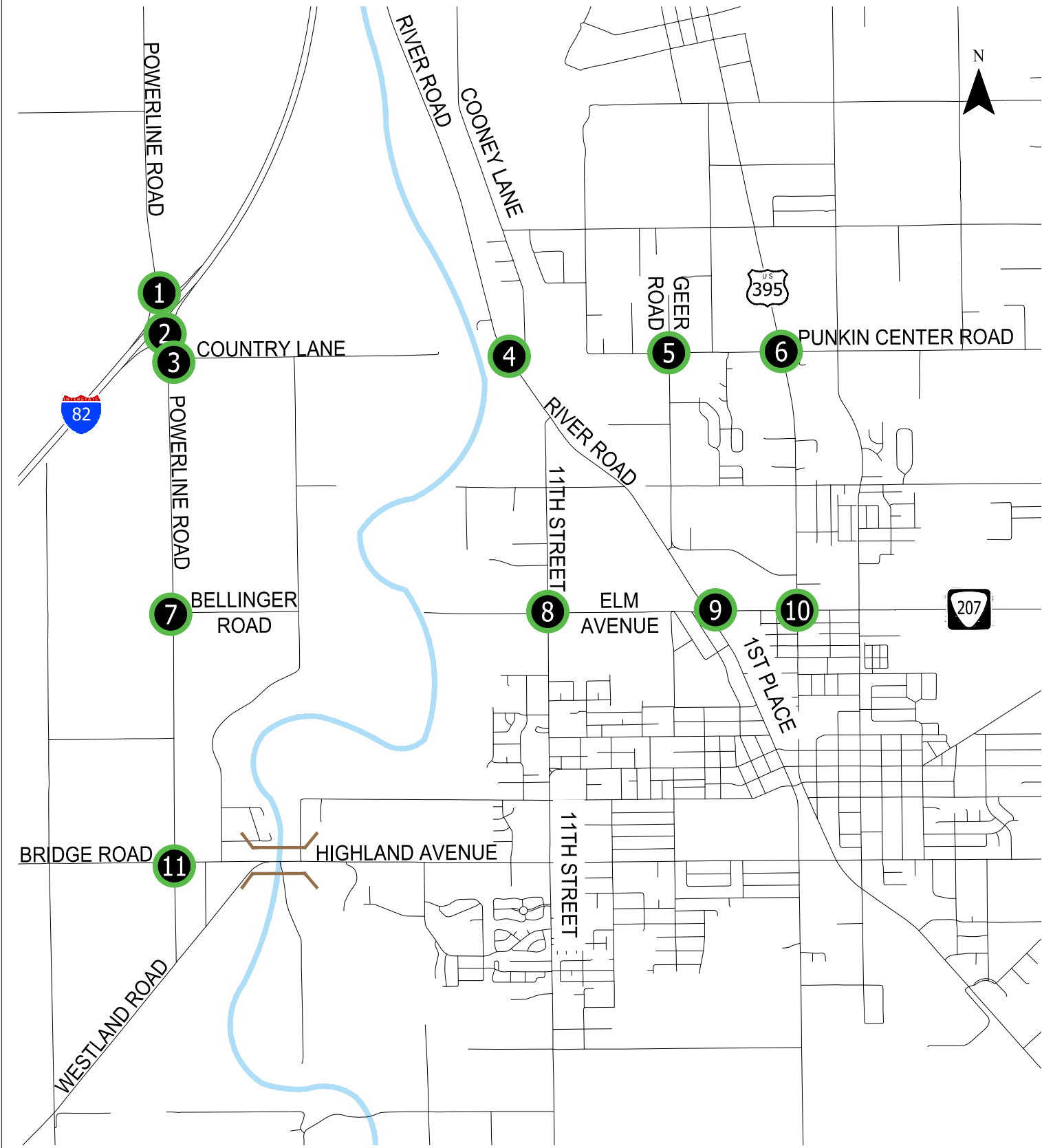
↑ - EXISTING LANE MOVEMENT # - STUDY INTERSECTION

● - STOP SIGN

🚦 - SIGNAL

Existing Lane Conditions
& Traffic Control Devices
Hermiston, OR

Figure
2



LEGEND

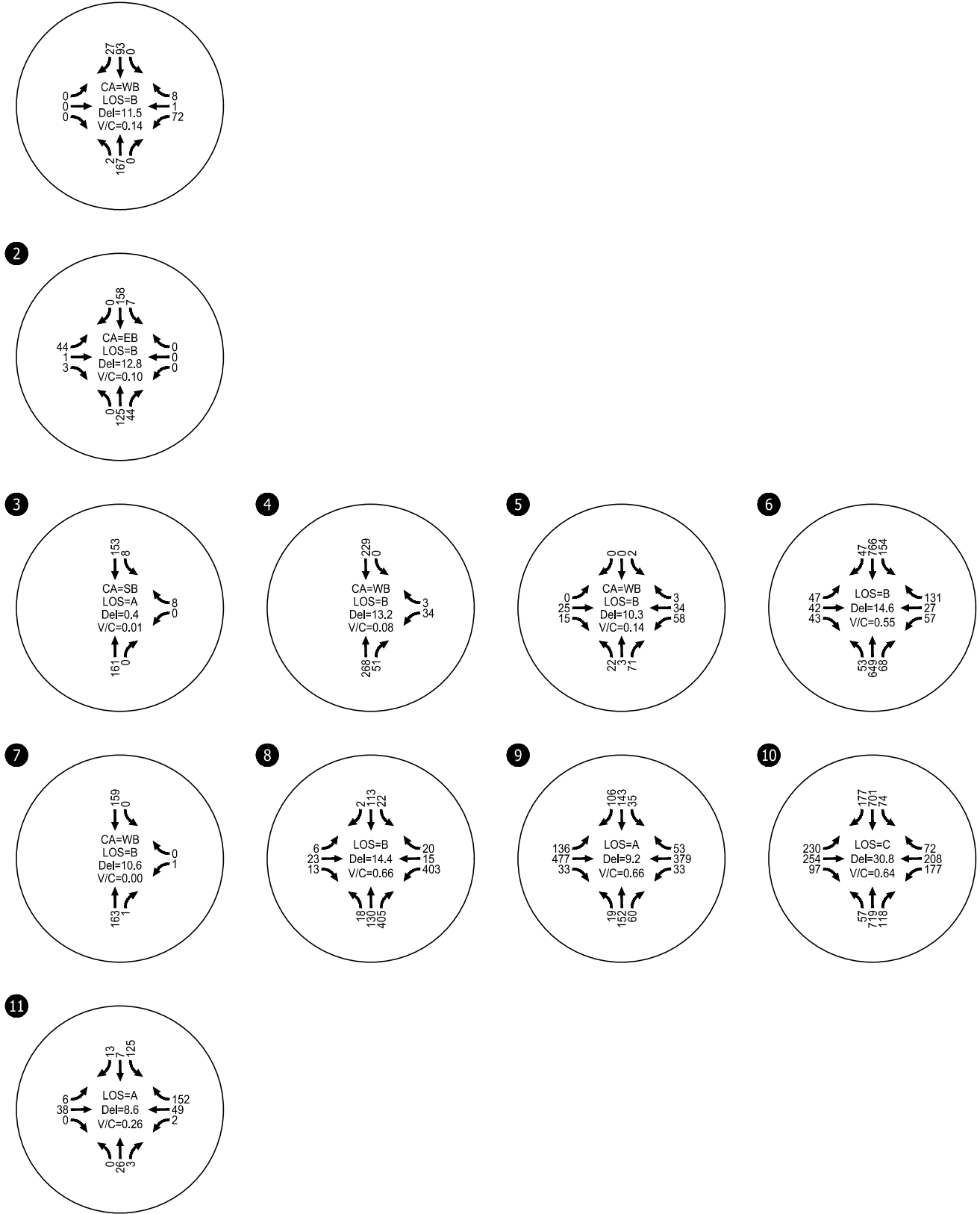
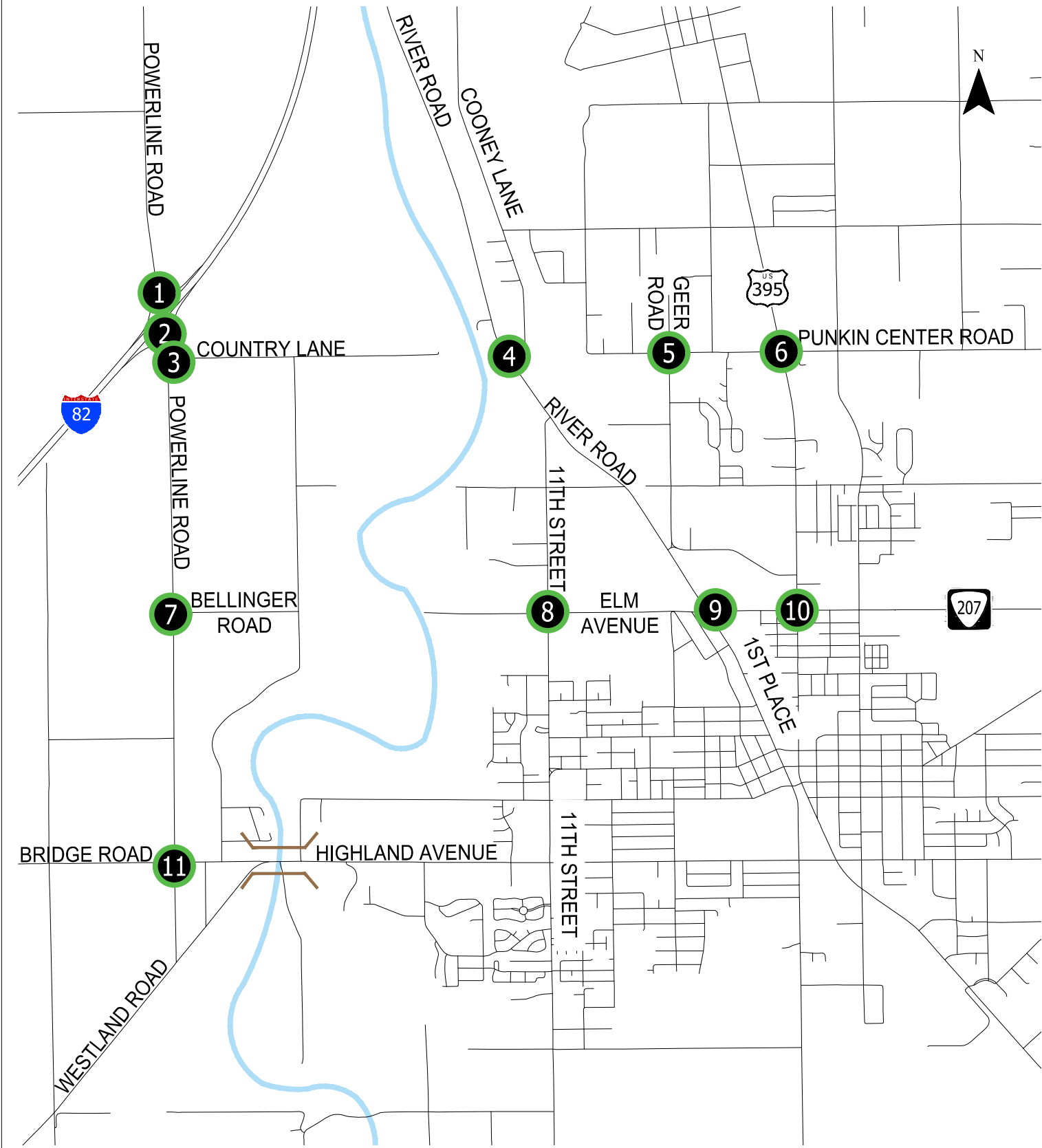
CA = CONTROL APPROACH
LOS = LEVEL OF SERVICE
Del = DELAY
V/C = VOLUME-TO-CAPACITY RATIO

- STUDY INTERSECTION HAS NO OPERATIONAL ISSUES OR CAPACITY CONSTRAINTS

- STUDY INTERSECTION HAS OPERATIONAL ISSUES THAT WILL REQUIRE CAPACITY OR GEOMETRIC IMPROVEMENTS

Existing Traffic Volumes (2021)
Weekday AM Peak Hour
Hermiston, OR

Figure 3



LEGEND

CA = CONTROL APPROACH
LOS = LEVEL OF SERVICE
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V/C = VOLUME-TO-CAPACITY RATIO

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Existing Traffic Volumes (2021)
Weekday PM Peak Hour
Hermiston, OR

Figure 4

FUTURE YEAR NO-BUILD ANALYSIS

This section of the report identifies how the study area's transportation system is forecast to operate in the future assuming continued local and regional traffic growth but without either of the two corridor extension options. Based on conversations with City of Hermiston staff, the year 2032 was chosen as the most reasonable near-term horizon year for this project.

2032 No-Build Traffic Volumes and Peak Hour Operations

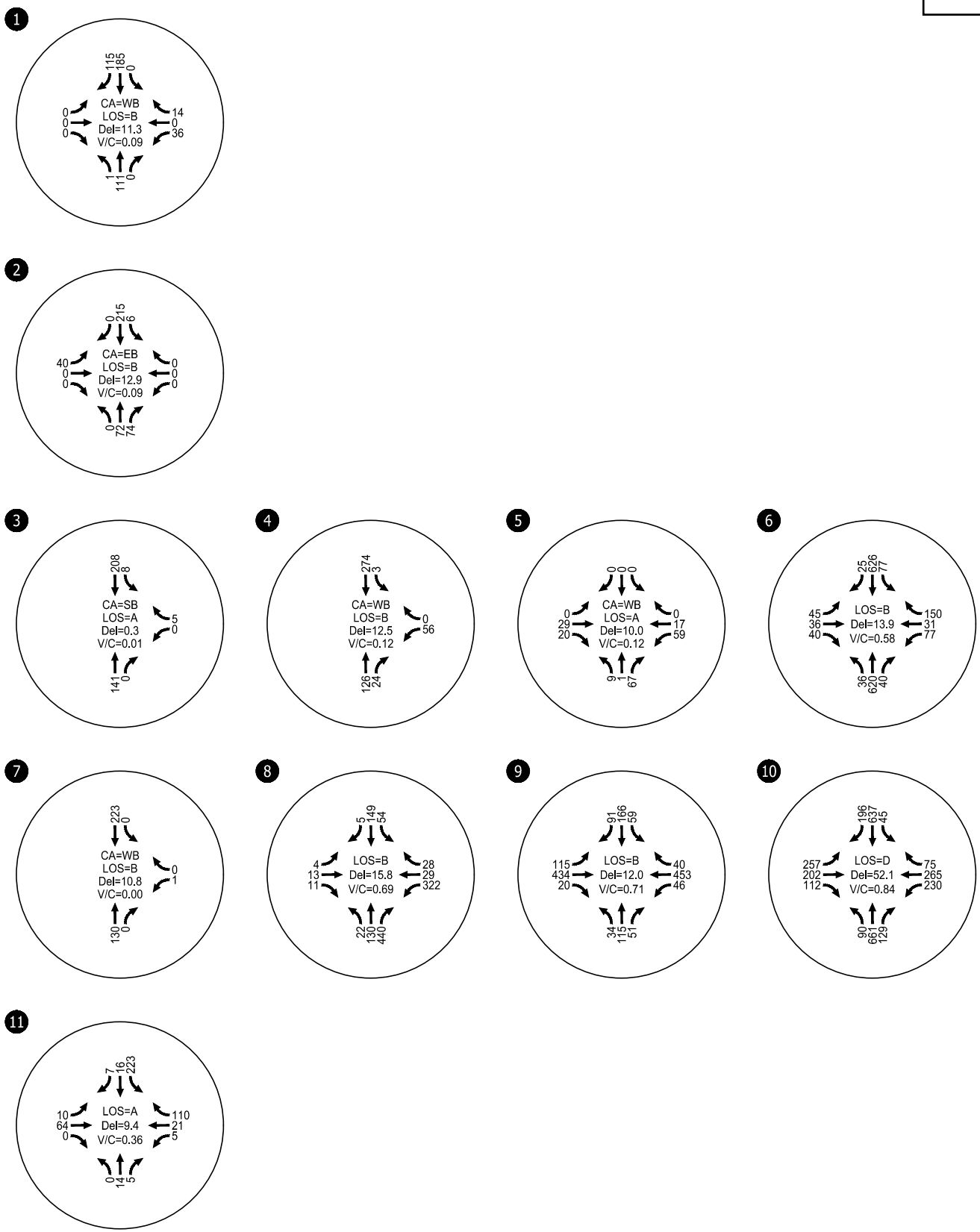
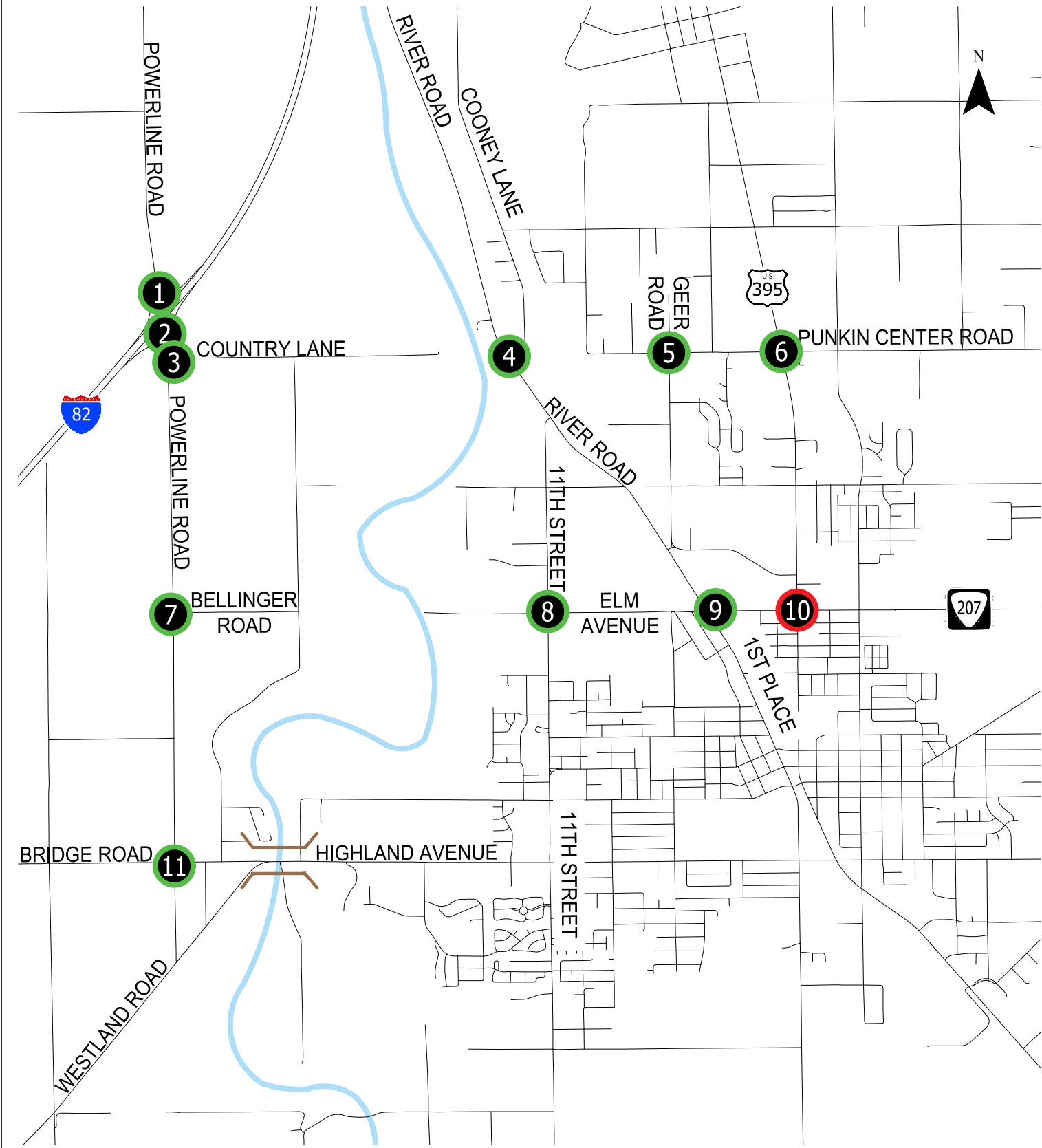
To estimate future year 2032 traffic conditions, a compound annual growth rate between 1.5% and 3% was applied to the seasonally adjusted 2021 traffic volumes in order to reflect anticipated local and regional growth. Specific growth rate applications include:

- Intersections along Powerline Road and Umatilla River Road were grown at a compound rate of 1.5% per year to be consistent with other recent traffic impact analyses performed in the study area.
- The intersections of 11th Street and 1st Place with Elm Avenue were grown at a compound rate of 2.5% per year to reflect a slightly higher projected growth within Hermiston and the significance of Elm Avenue as a major east-west travel corridor.
- The study intersections along US 395 were grown at a compound growth rate of 3% per year to account for greater projected growth in northeast Hermiston and the local and regional significance of the US 395 corridor.

In addition to these growth assumptions, the projected traffic demands associated with several approved development projects were incorporated at applicable study intersections. These include:

- Ambience Homes Traffic Impact Analysis in Umatilla. This project is anticipated to be completed by 2024, therefore the trips associated with this project were added to the 2032 background growth traffic volumes.
- Umatilla Residential Development Traffic Impact Analysis for the residential and commercial development of Vandelay Meadows, Cheryl's Place, and Ballard Property. This project is anticipated to be completed by 2030, therefore the trips associated with this project were added to the 2032 background growth traffic volumes.
- McClannahan Summit subdivision; this project has a planning horizon year of 2040. The phasing of the project is unclear in the study, therefore a proportional number of trips associated with this project between 2021 were added to the 2032 background growth traffic volumes.

Figure 5 and Figure 6 illustrate the resulting 2032 no-build traffic volumes and operational conditions for the weekday AM and PM peak hours. As shown, all of the study intersection operations are forecast to meet the applicable ODOT mobility targets and City of Hermiston operating standards during the AM and PM study hours with the exception of US 395/W Elm Avenue (OR 207). During the weekday PM peak hour, this intersection is forecast to operate at a V/C of 0.93 which exceeds the 0.90 mobility target. Additional discussion regarding future year operations of this intersection will be provided in following sections of this report. *Appendix D includes the 2032 no-build conditions operations analysis worksheets.*



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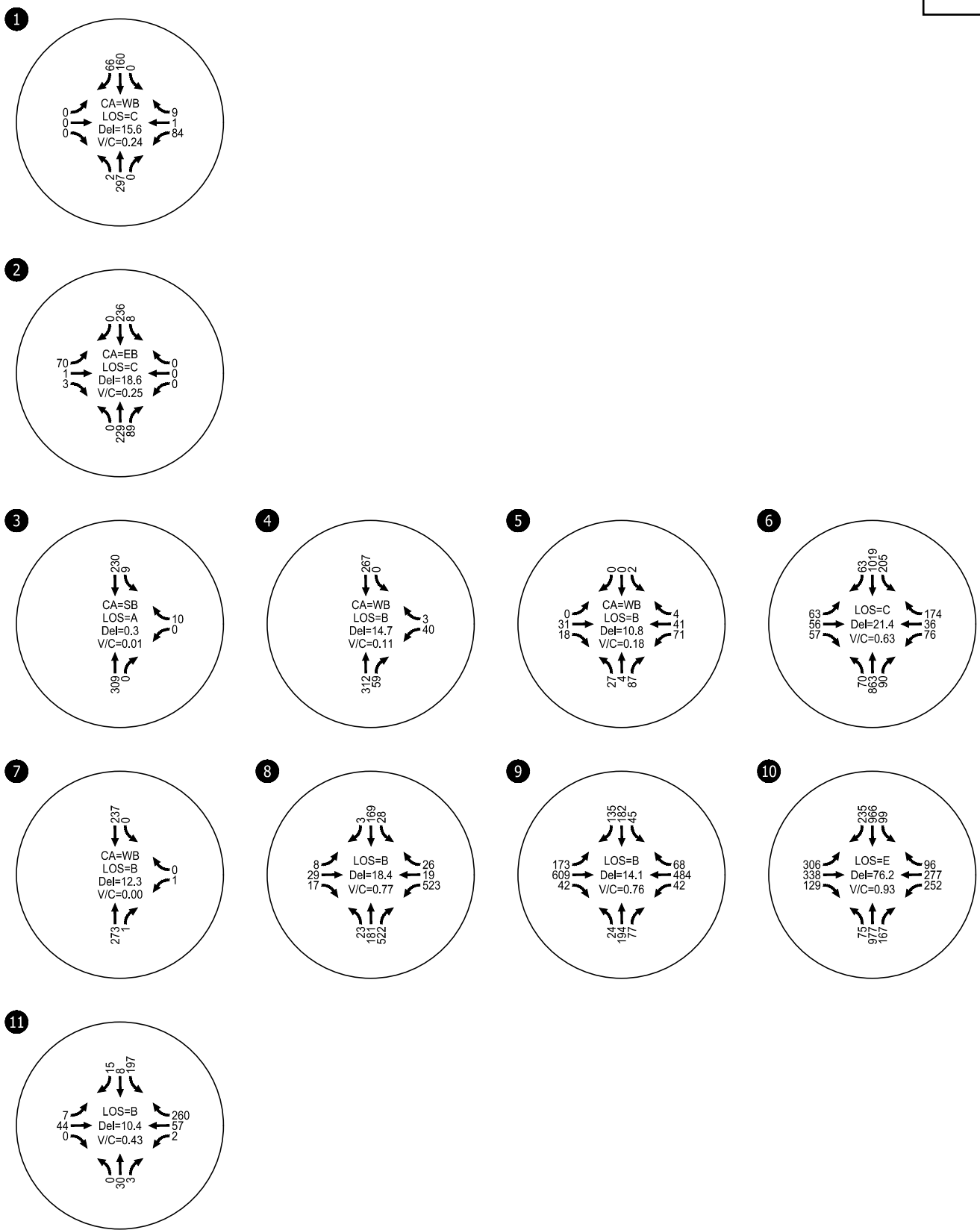
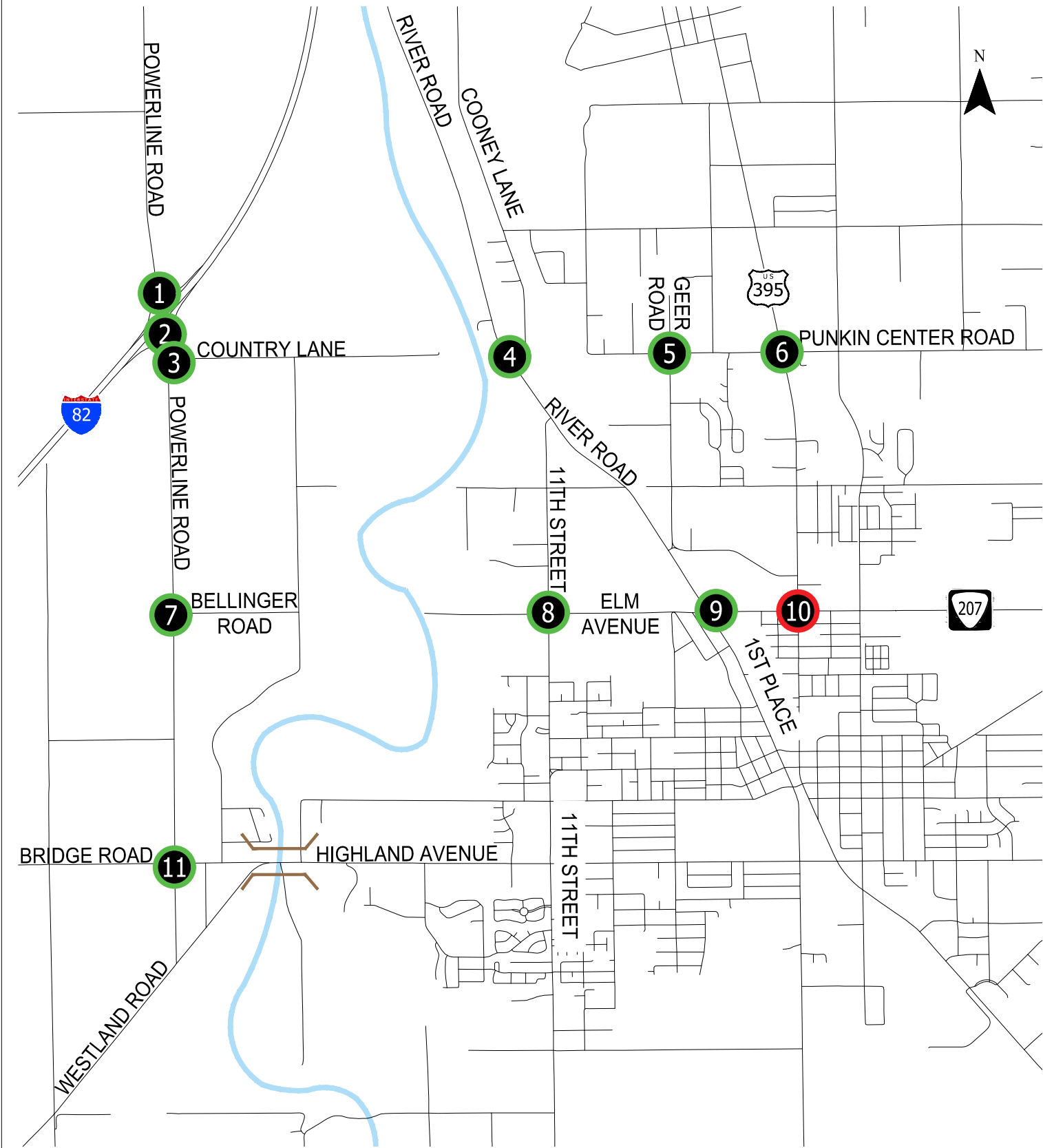
CA = CONTROL APPROACH
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- STUDY INTERSECTION HAS NO OPERATIONAL ISSUES OR CAPACITY CONSTRAINTS

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2032 No-Build Background Traffic Volumes
Weekday AM Peak Hour
Hermiston, OR

Figure 5



LEGEND

CA = CONTROL APPROACH
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V/C = VOLUME-TO-CAPACITY RATIO

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2032 No-Build Background Traffic Volumes
Weekday PM Peak Hour
Hermiston, OR

Figure
6

W PUNKIN CENTER ROAD EXTENSION

As conceptually visualized in Figure 1, the W Punkin Center Option would involve the following:

- A westerly extension of W Punkin Center Road from its current terminus at Sunshine Lane to River Road. While there are multiple alignment options for this extension, it is assumed that the extension would minimize impacts to existing properties and cross the Union Pacific rail line in the approximate location as the existing Cooney Lane crossing.
- A new four-legged intersection with River Road that replaces the existing River Road/Cooney Lane¹ intersection. Given the significance of this intersection and the traffic volumes that it would likely accommodate, it is preliminarily assumed that the intersection would need to be fully improved with left-turn and shared through/right-turn lanes on all four approaches. Potential traffic control improvements will be discussed in the following sections.
- A continued westerly extension from River Road that would span the Umatilla River on a new bridge and merge with the existing Country Lane corridor. It is preliminarily assumed that Country Lane would be upgraded to a fully improved Major Arterial cross section.
- A realignment of Country Lane at Powerline Road to improve spacing with the adjacent I-82/Powerline Road interchange. Potential alignment scenarios will be discussed in the following sections. Given the levels of new travel demand that it would accommodate, a new Powerline Road/Country Lane intersection was assumed that the intersection would be fully improved with left-turn and shared through/right-turn lanes on the southbound and westbound approaches.

Beyond these noted infrastructure changes, no other major regional or local infrastructure improvements were assumed to the study area roadways and intersections.

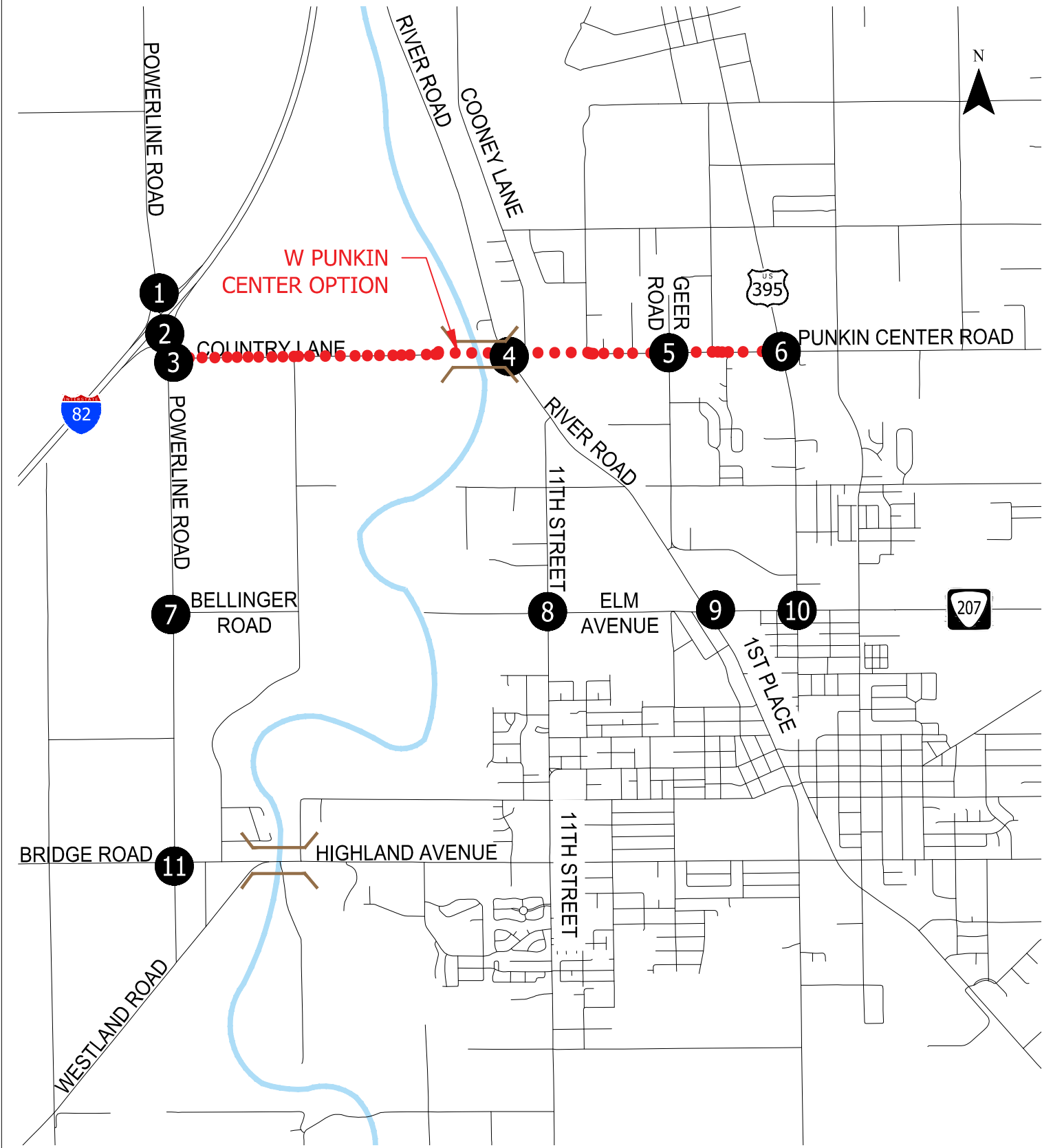
Assumed Redistribution of Trips

With the circulation components of the W Punkin Center Road extension described above, it is anticipated that there will be a natural redistribution of projected traffic volumes at the local and regional levels. Key elements of this assumed redistribution include:

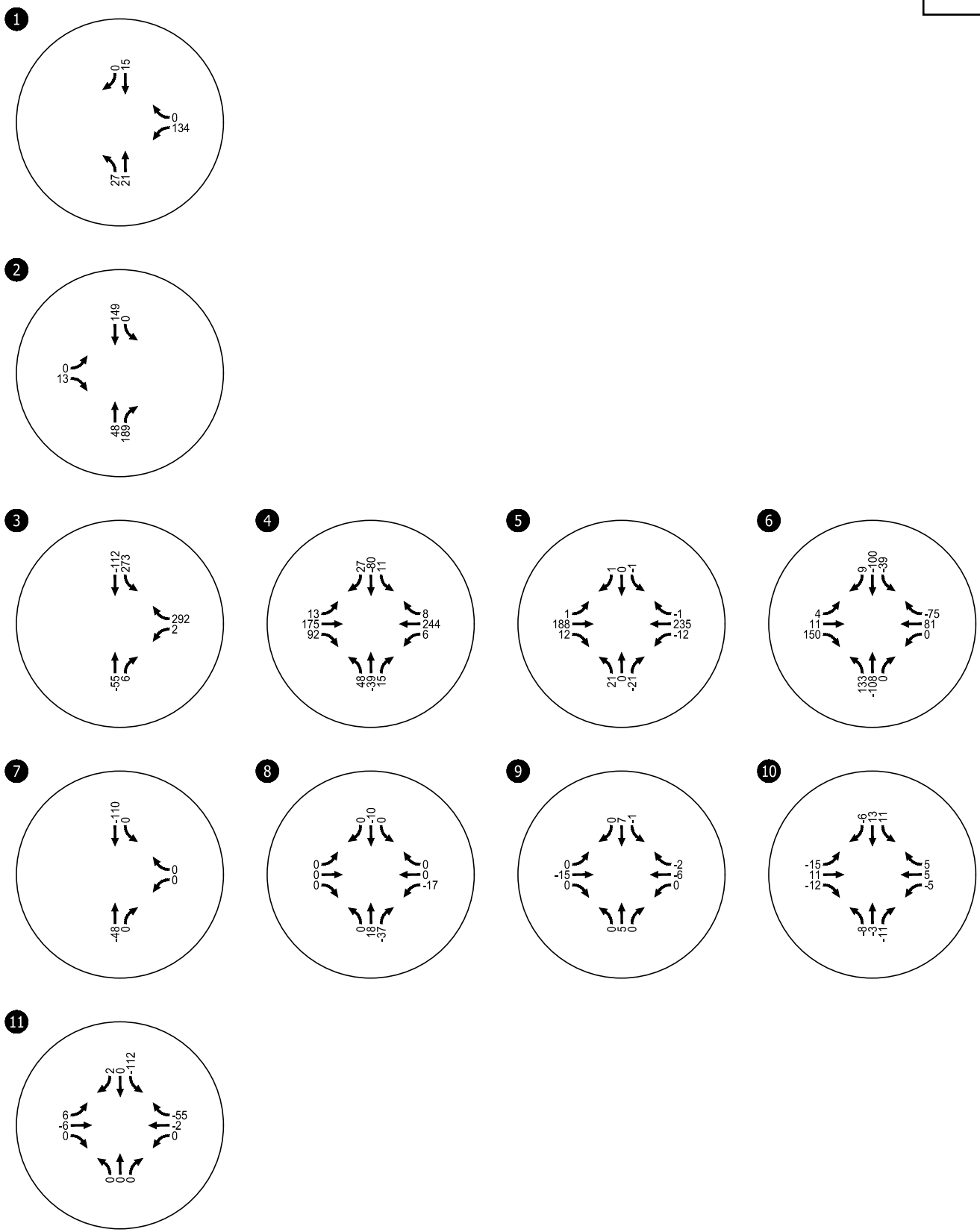
- A rerouting of a portion of the travel demand along the Powerline Road/Bridge Road/Highland Avenue corridors to the new W Punkin Center Road corridor.
- A rerouting of a portion of the north-south US 395 travel demand onto the new W Punkin Center Road corridor in recognition that it would provide a more direct connection to the I-82 corridor.
- A rerouting of a portion of the north-south River Road travel demand onto the new W Punkin Center Road corridor in recognition that it would provide new connections to Powerline Road, I-82, and US 395.
- A rerouting of a small portion of Westland Road demand to the I-82/Powerline Road interchange and the new east-west W Punkin Center Road corridor.

Figure 7 and Figure 8 illustrate the assumed travel demand redistribution at the study intersections during the 2032 weekday AM and PM peak study hours.

¹ It is assumed that Cooney Lane would be realigned and would connect to the new W Punkin Center Road extension.

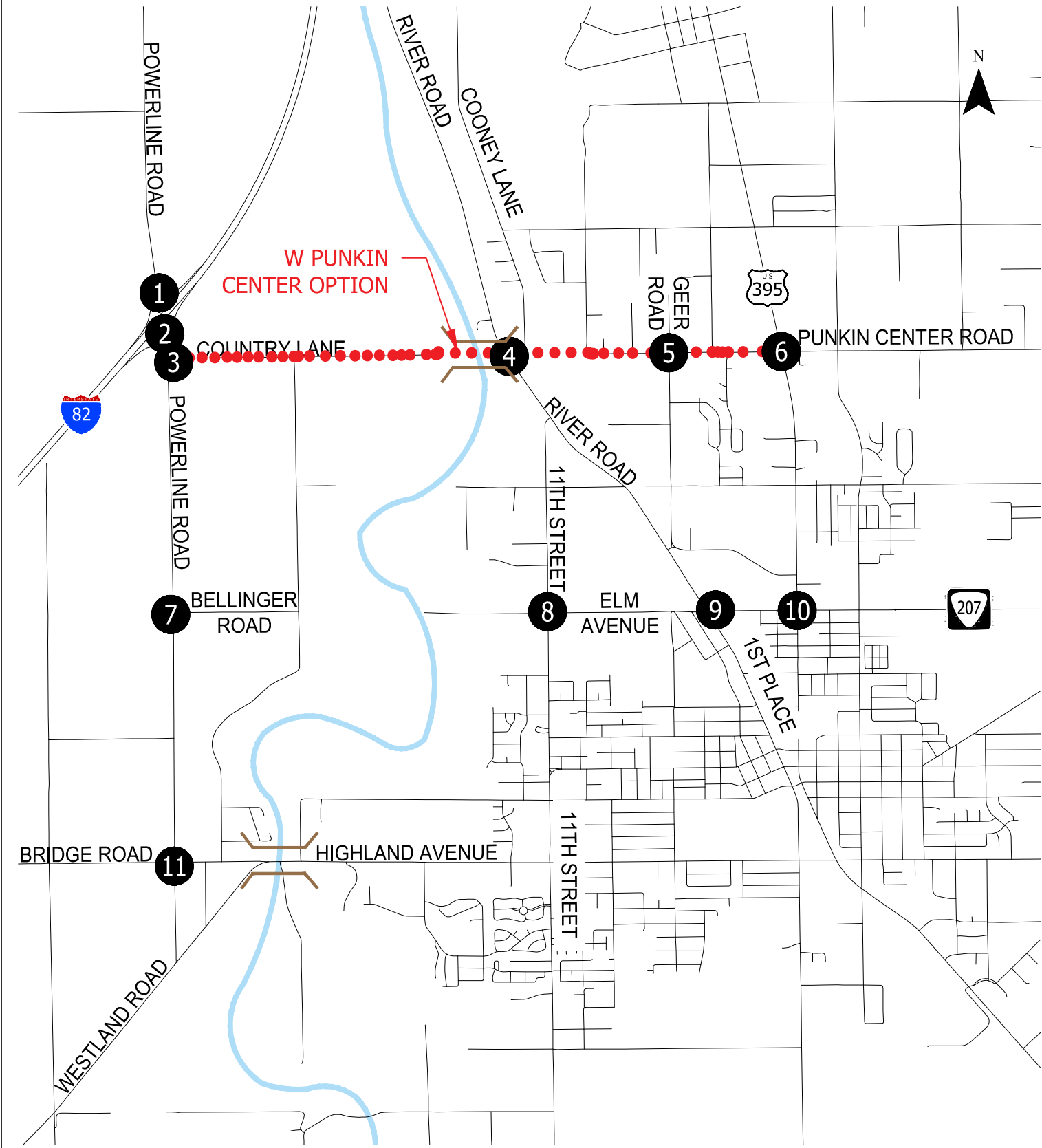


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- STUDY INTERSECTION

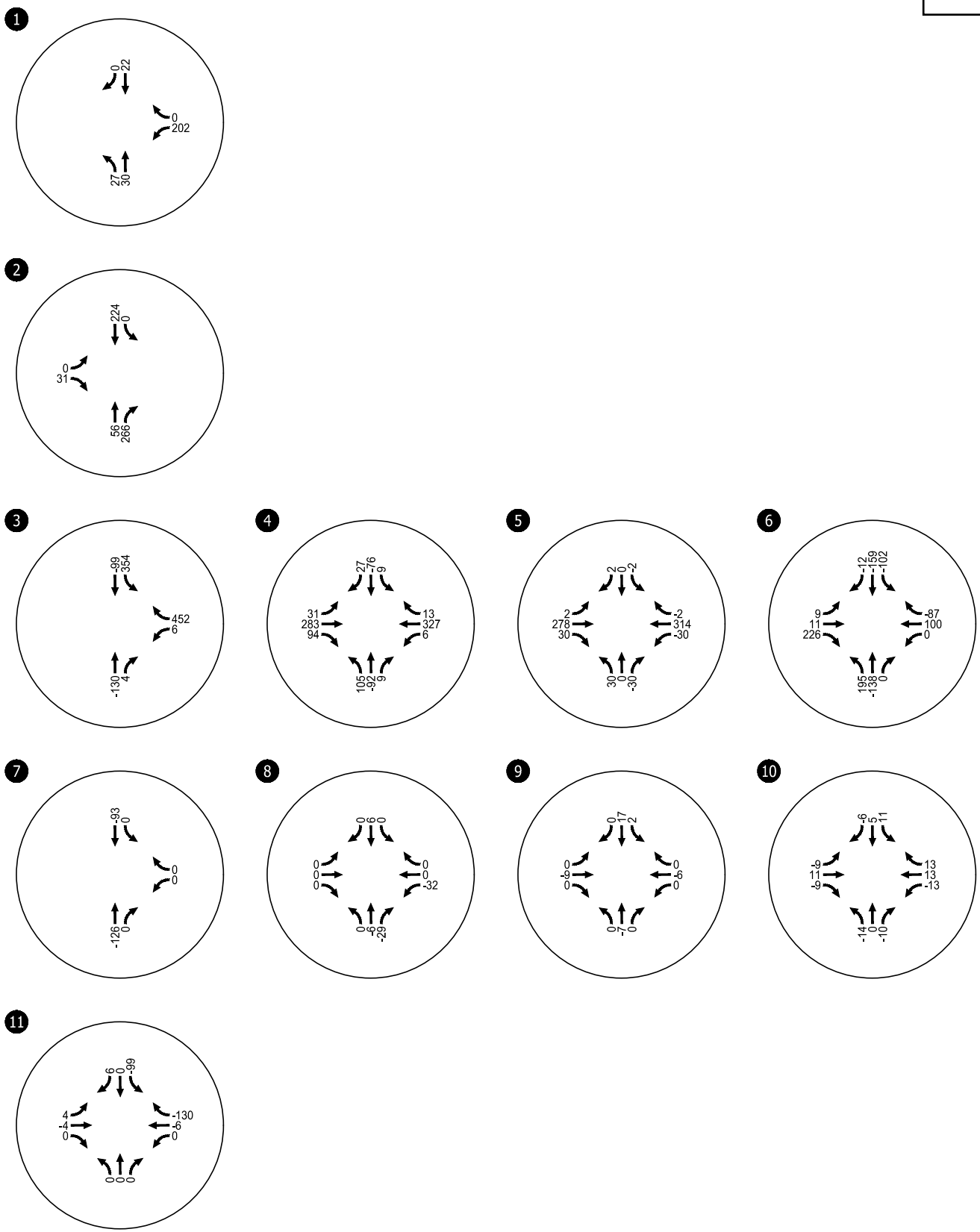


Redistributed Trips Under W Punkin Center Option
Weekday AM Peak Hour (2032)
Hermiston, OR

Figure
7



LEGEND
- STUDY INTERSECTION



Redistributed Trips Under W Punkin Center Option
Weekday PM Peak Hour (2032)
Hermiston, OR

Figure
8

2032 W Punkin Center Option Traffic Volumes and Peak Hour Operations

The 2032 traffic conditions with a W Punkin Center Road corridor were determined by applying the redistributed volumes shown in Figure 7 and Figure 8 to the 2032 No-Build volumes shown in Figure 5 and Figure 6. Figure 9 and Figure 10 illustrate the resulting and anticipated 2032 W Punkin Center Road corridor traffic volumes and peak hour operations. *Appendix E includes the 2032 total traffic volumes under Punkin Center Road extension operations analysis worksheets.*

As shown in these figures, the combination of local/regional traffic growth and the anticipated redistribution of area travel demand is projected to result in the following impacts:

US 395/W PUNKIN CENTER ROAD

While the US 395/Punkin Center Road intersection is forecast to continue to meet the ODOT mobility target during the weekday AM and PM peak hours, forecast traffic demand on the eastbound approach is projected to be high enough to require additional turn lane improvements. Specifically, the eastbound approach is projected to have a high right-turn demand during the peak study hours which would result in long vehicle queues in the existing shared through/right-turn lane.

In order to better manage this demand and address long vehicle queues, the following improvements are recommended at the US 395/W Punkin Center Road intersection:

- Construction of an eastbound right-turn lane with 200 feet of right-turn storage on the eastbound W Punkin Center Road approach.
- Relocation of the US 395/W Punkin Center Road signal controller infrastructure currently located in the southwest quadrant of the intersection in order to accommodate a new eastbound right-turn lane.

Given that the property in the southwest quadrant of the US 395/W Punkin Center Road intersection is currently undeveloped, the right-of-way for this improvement could be obtained with fewer private property impacts.

US 395/W ELM AVENUE (OR 207)

As with the 2032 No-Build conditions, the US 395/W Elm Avenue (OR 207) intersection is forecast to continue to exceed the 0.90 ODOT mobility target. Specifically, the intersection is forecast to operate near capacity at a V/C of 0.96 during the weekday PM peak hour. This represents a slight degradation in operations compared to the 2032 No-Build conditions (V/C 0.93). A review of forecast traffic demands (generated primarily as a result of local/regional growth) indicates that the intersection would benefit from a number of capacity enhancing improvements to the critical northbound, southbound and westbound approaches. Specifically, these include:

- Construction of a northbound right-turn lane, a southbound right-turn lane, and a westbound right-turn lane. These capacity enhancements would significantly improve operations of the intersection and result in an acceptable V/C of 0.76.
- Reconstruction of the US 395/W Elm Avenue traffic signal infrastructure.

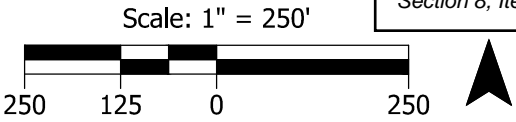
While these improvements would benefit the long-term operations of the intersection, their accommodation would be difficult and costly considering the lack of available public right-of-way and impacts to existing businesses.

RIVER ROAD/W PUNKIN CENTER ROAD

A westerly extension of W Punkin Center Road is assumed to form a new intersection with River Road in the approximate location of the existing River Road/Cooney Lane intersection. A conceptual illustration of this connection and the assumed intersection geometry (left-turn and shared through/right-turn lanes on all four approaches) is provided in Exhibit A. As shown in Figures 9 and 10, the projected future year traffic demands through this intersection will require the consideration of traffic control measures beyond two-way stop-control². A planning level signal warrant analysis indicates that the intersection would meet volume-based warrants for a traffic signal. With signalization and the conceptually illustrated geometric improvements, the intersection would operate at acceptable level of service D during both the weekday AM and PM peak hours. In addition to the geometric and long-term traffic control needs, a new River Road/W Punkin Center Road intersection would need to consider the following:

- An alignment between Sunshine Lane and River Road that minimizes impacts to existing land uses. The conceptual alignment shown in Exhibit A is one potential option.
- A realignment of Cooney Lane to intersect the W Punkin Center Road extension. The conceptual alignment shown in Exhibit A is one potential option.
- An upgrade and modernization of the adjacent Union Pacific Railroad crossing gates and signal infrastructure.

² The consideration of traffic control measures typically includes the consideration of a roundabout. However, a roundabout was not assessed at this location due to the typically large footprint of a roundabout, lack of existing right-of-way, and the intersection's close proximity to the adjacent Union Pacific rail line.



✕ - Road to be removed/relocated
* Alignments and geometry shown are for visualization purposes only and should be considered conceptual.

Conceptual Alignment
Hermiston, OR

Exhibit
A

POWERLINE ROAD/COUNTRY LANE

The existing Powerline Road/Country Lane intersection is located approximately 275 feet south of the I-82/Powerline Road interchange ramp terminal. This spacing does not meet ODOT's desired ¼-mile access spacing standard for public street intersections within the vicinity of freeway ramp terminals. While not meeting the spacing standard, existing traffic volumes on Country Lane are relatively minimal. However, with a Punkin Center Road extension to Country Lane, there will be a significant increase in travel demand necessitating a closer review of the intersection spacing issue, physical improvements such as intersection modernization, and travel lane improvements such as a southbound left-turn lane on Powerline Road and westbound right-turn and left-turn lanes on Country Lane.

Given the complexity and private property impacts associated with improving and relocating the Country Lane intersection with Powerline Road, a separate study effort called an Interchange Area Management Plan (IAMP) is typically recommended. An IAMP is a public planning effort that would involve ODOT, Umatilla County, City of Hermiston, nearby private property owners, and other interest groups. The IAMP would formally identify a preferred interchange improvement plan including the design, timing, and right of way needs for a relocated Country Lane intersection. While there are any number of potential Country Lane relocation scenarios that could be studied in an IAMP, one potential concept is illustrated in Exhibit B. This concept (included for illustrative purposes only but also inclusive of identified geometry and capacity needs) would:

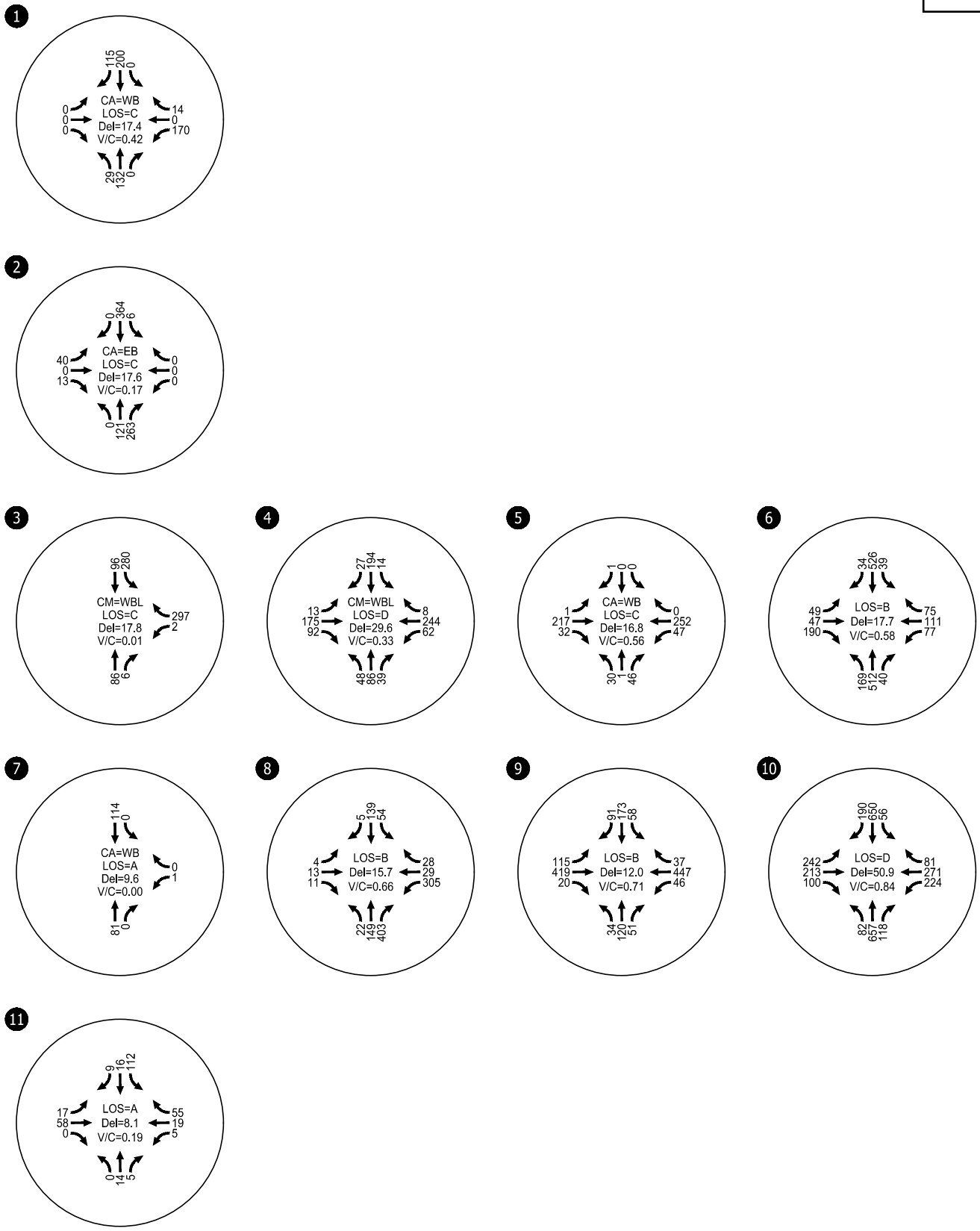
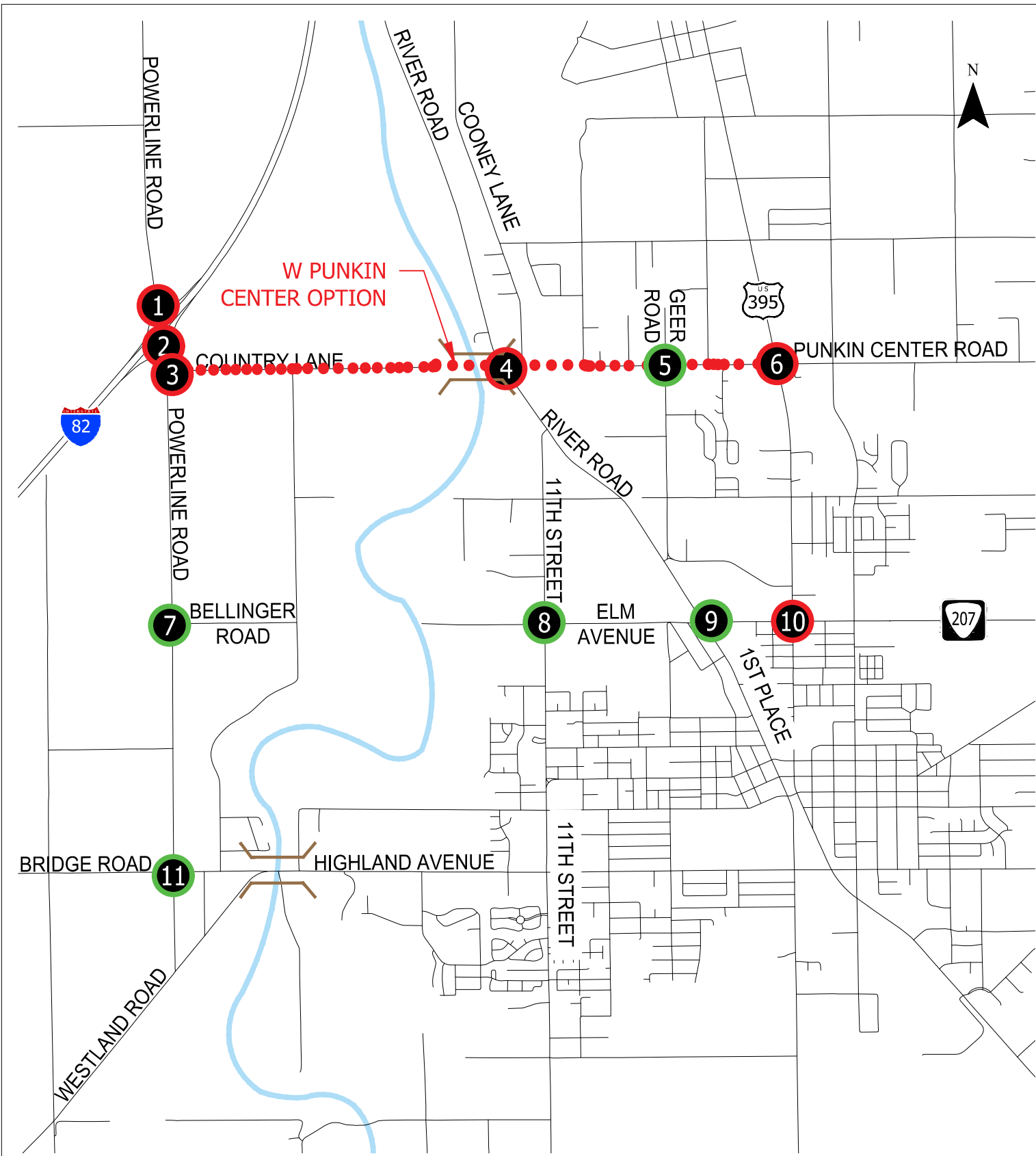
- Close off the existing Country Lane connection to Powerline Road and develop a separate alignment that would intersection Powerline Road along property lines approximately 1,500 feet to the south. An IAMP would formally study alignments like this as well as others to find the least impactful and cost-effective solution.
- Widen Powerline Road to provide a southbound left-turn lane at the realigned Country Lane intersection.
- Upgrade the Country Lane corridor to two paved twelve-foot travel lanes (widened to include left- or right-turn lanes at major crossroads), five-foot shoulders/bike lanes, curb and gutter, and five-foot sidewalks.
- Upgrade the right-of-way width to 90 feet to allow for a future upgrade to an Urban Major Arterial per City of Hermiston Standard Drawing ST07.



✕ - Road to be removed/relocated
* Alignments and geometry shown are for visualization purposes only and should be considered conceptual.

Conceptual Alignment
Hermiston, OR

Exhibit
B



LEGEND

CA = CONTROL APPROACH

LOS = LEVEL OF SERVICE

Del = DELAY

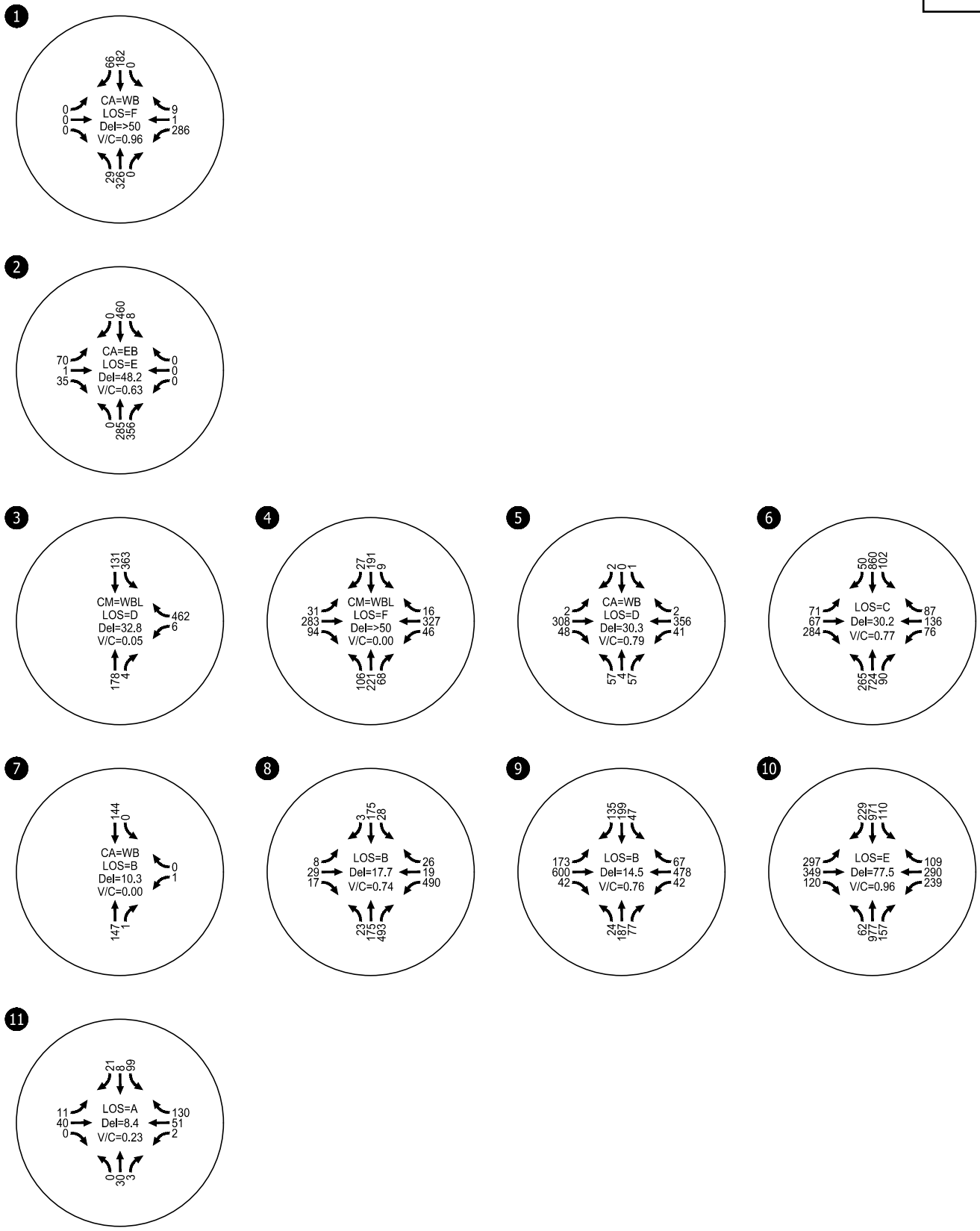
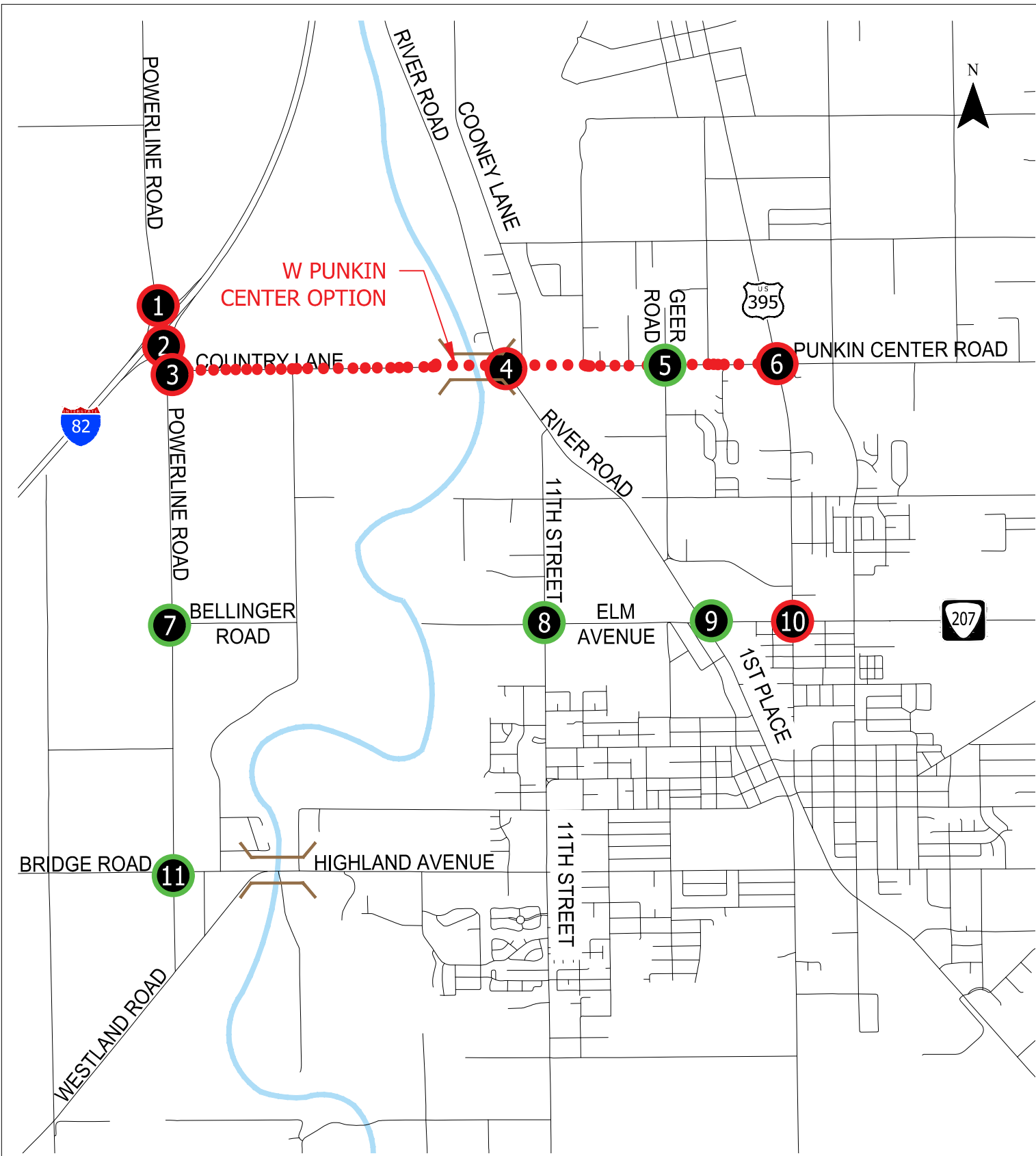
V/C = VOLUME-TO-CAPACITY RATIO

- STUDY INTERSECTION HAS NO OPERATIONAL ISSUES OR CAPACITY CONSTRAINTS

- STUDY INTERSECTION HAS OPERATIONAL ISSUES THAT WILL REQUIRE CAPACITY OR GEOMETRIC IMPROVEMENTS

Total Traffic Volumes Under W Punkin Center Option
Weekday AM Peak Hour (2032)
Hermiston, OR

Figure
9



LEGEND

CA = CONTROL APPROACH
LOS = LEVEL OF SERVICE
Del = DELAY
V/C = VOLUME-TO-CAPACITY RATIO

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Total Traffic Volumes Under W Punkin Center Option
Weekday PM Peak Hour (2032)
Hermiston, OR

Figure
10

W ELM AVENUE EXTENSION

As conceptually visualized in Figure 1, the W Elm Avenue Option would involve the following:

- A westerly extension of W Elm Avenue from its current terminus at NW 11th Street that would span the Umatilla River on a new bridge and merge with the existing Bellinger Road corridor.
- An upgraded Bellinger Road (to Major Arterial level standards).
- An upgraded intersection of Bellinger Road at Powerline Road.

Beyond these noted infrastructure changes, no other major regional or local infrastructure improvements to the study area roadways and intersections were assumed.

Assumed Redistribution of Trips

With the circulation components of the W Elm Avenue extension described above, it is anticipated that there will be a natural redistribution of projected traffic volumes at the local and regional levels. Key elements of this assumed redistribution include:

- A rerouting of a portion of the travel demand along the Powerline Road/Bridge Road/Highland Avenue corridors to the new W Elm Avenue corridor.
- A rerouting of a portion of the north-south US 395 travel demand onto the new W Elm Avenue corridor in recognition that it would provide an alternative connection to the I-82 corridor.
- A rerouting of a portion of the north-south 11th Avenue travel demand onto the new W Elm Avenue corridor in recognition that it would provide new connections to Powerline Road and I-82.

Figure 11 and Figure 12 illustrate the assumed travel demand redistribution at the study intersections during the 2032 weekday AM and PM peak study hours.

2032 W Elm Avenue Option Traffic Volumes and Peak Hour Operations

The 2032 traffic conditions with a W Elm Avenue corridor were determined by applying the redistributed volumes shown in Figure 11 and Figure 12 to the 2032 No-Build volumes shown in Figure 5 and Figure 6. Figure 13 and Figure 14 illustrate the resulting and anticipated 2032 W Elm Avenue Option traffic volumes and peak hour operations. *Appendix F includes the 2032 total traffic volumes under Elm Avenue extension operations analysis worksheets.*

As shown in these figures, the combination of local/regional traffic growth and the anticipated redistribution of area travel demand is projected to result in the following impacts:

US 395/W ELM AVENUE (OR 207)

Under the W Elm Avenue Option, the US 395/W Elm Avenue (OR 207) intersection is forecast to operate either over the 0.90 mobility target or over capacity depending upon the study period. Specifically, the intersection is forecast to operate at a V/C of 0.92 during the weekday AM peak hour and over capacity with a V/C of 1.02 during the weekday PM peak hour. With an increase in turning movement demand generated by the W Elm Avenue Option, this represents a much more significant degradation in operations compared to the 2032 No-Build conditions (V/C 0.93). A review of forecast traffic demands indicates the

intersection would benefit from the same previously identified capacity enhancements to the critical northbound, southbound and westbound approaches. These include:

- Construction of a northbound right-turn lane, a southbound right-turn lane, and a westbound right-turn lane would significantly improve operations of the intersection and result in an acceptable V/C of 0.90.
- Reconstruction of the US 395/W Elm Avenue traffic signal infrastructure to accommodate the above noted turn lanes.

While these improvements would benefit the long-term operations of the intersection, their accommodation would be difficult and costly considering the lack of right-of-way and impacts to existing businesses. It should also be noted that while these improvements would restore capacity to the intersection, they would only restore enough capacity in order to meet the 0.90 mobility target.

W ELM AVENUE (OR 207)/1ST PLACE

Under the W Elm Avenue Option, the W Elm Avenue (OR 207)/1st Place intersection is forecast to operate at a V/C ratio of 0.89 during the weekday PM peak hour. While this forecast operation does not exceed the 0.90 mobility target, it does indicate the intersection has limited long-term operational flexibility to accommodate additional traffic growth. To help address this long-term limitation and accommodate increased traffic demand anticipated under the W Elm Avenue Option, the intersection would benefit from the following modernization and capacity enhancing improvements:

- Construction of a southbound left-turn lane on 1st Place
- Construction of an eastbound right-turn lane on W Elm Avenue

While not a function of the increased traffic generated by the W Elm Avenue Option, the north and south legs of 1st Place are unimproved and would benefit from an upgrade to a Major Collector per option 2 of the City of Hermiston Standard Drawing ST09.

W ELM AVENUE (OR 207)/NW 11TH STREET

Under the W Elm Avenue Option, new traffic demand on the underutilized west leg of the intersection will result in a degradation of capacity during the weekday PM peak hour. Specifically, the intersection is forecast to operate at a V/C of 0.94 which will exceed the 0.90 mobility target. A review of forecast traffic demands indicates the intersection would benefit from capacity enhancements to the critical eastbound approach. This includes:

- Construction of an eastbound right-turn lane which would improve operations of the intersection and result in an acceptable V/C of 0.89.

POWERLINE ROAD/BELLINGER ROAD

Under the W Elm Avenue Option, the Powerline Road/Bellinger Road intersection will need to be upgraded to urban travel standards in order to safely accommodate anticipated increases in travel demand. This includes

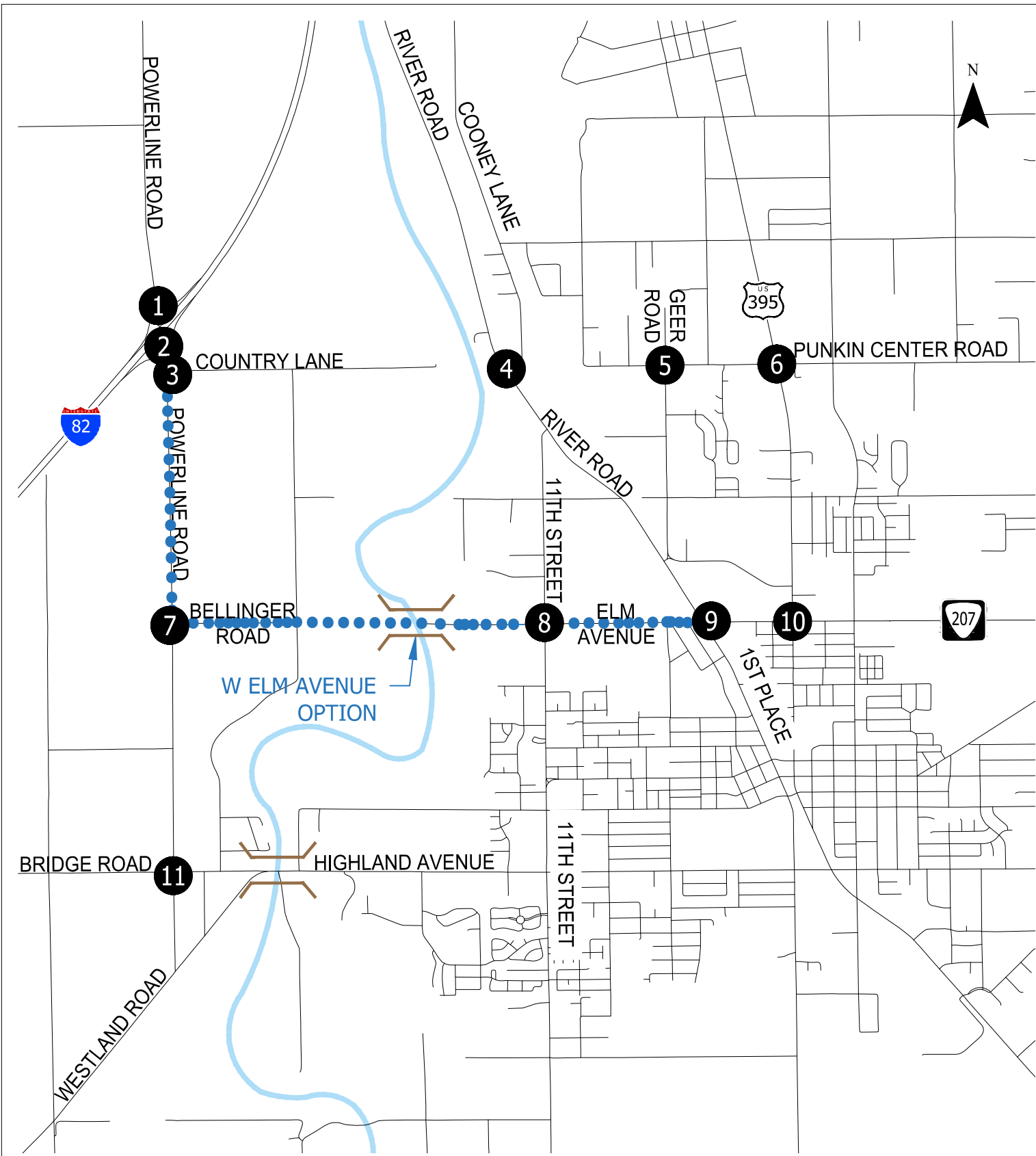
- Construction of a southbound left-turn lane on Powerline Road. The forecast left-turn demand will be high enough to warrant a separate turn lane allowing southbound through traffic to progress through the intersection unimpeded by the high left-turn demand.
- Construction of westbound right-turn and left-turn lanes on Bellinger Road.

POWERLINE ROAD AND BELLINGER ROAD CORRIDORS

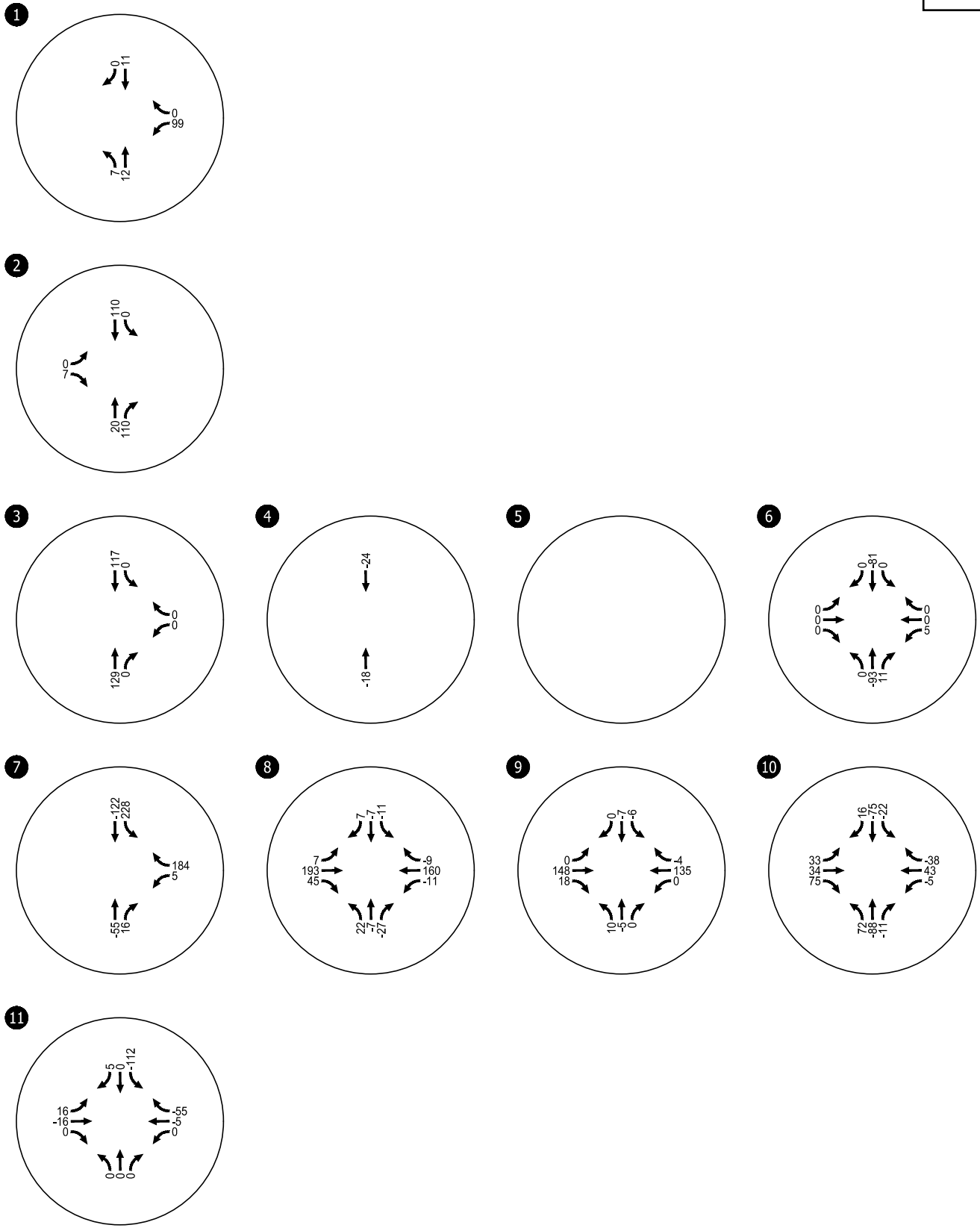
Under the W Elm Avenue Option, Power Line Road (from the I-82 interchange to Bellinger Road) and Bellinger Road (from Powerline Road to the Umatilla River Bridge) would need to be upgraded to two paved twelve-foot travel lanes (widened to include left- or right-turn lanes at major crossroads), five-foot shoulders/bike lanes, curb and gutter, and five-foot sidewalks. In addition, upgrade the right-of-way width to 90 feet to allow for a future upgrade to an Urban Major Arterial per City of Hermiston Standard Drawing ST07.

I-82 SB RAMP TERMINAL/POWERLINE ROAD

A W Elm Avenue extension is anticipated to generate additional travel demand through the I-82/Powerline Road interchange. As a result, the increase in travel demand will likely necessitate an upgrade and modernization of the interchange ramp terminals. The specific improvements should be addressed as part of a more comprehensive Interchange Area Management Plan.

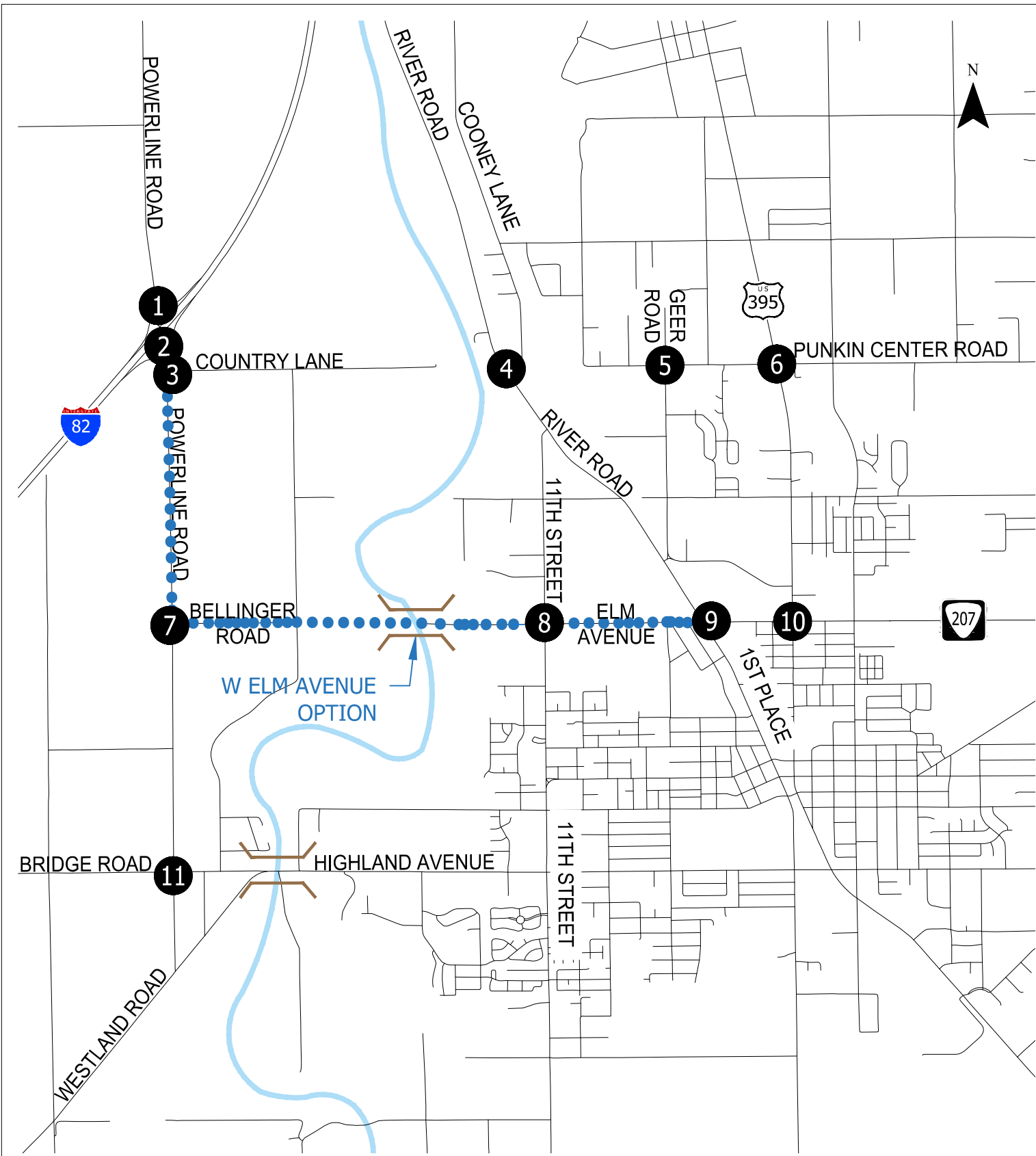


LEGEND
- STUDY INTERSECTION

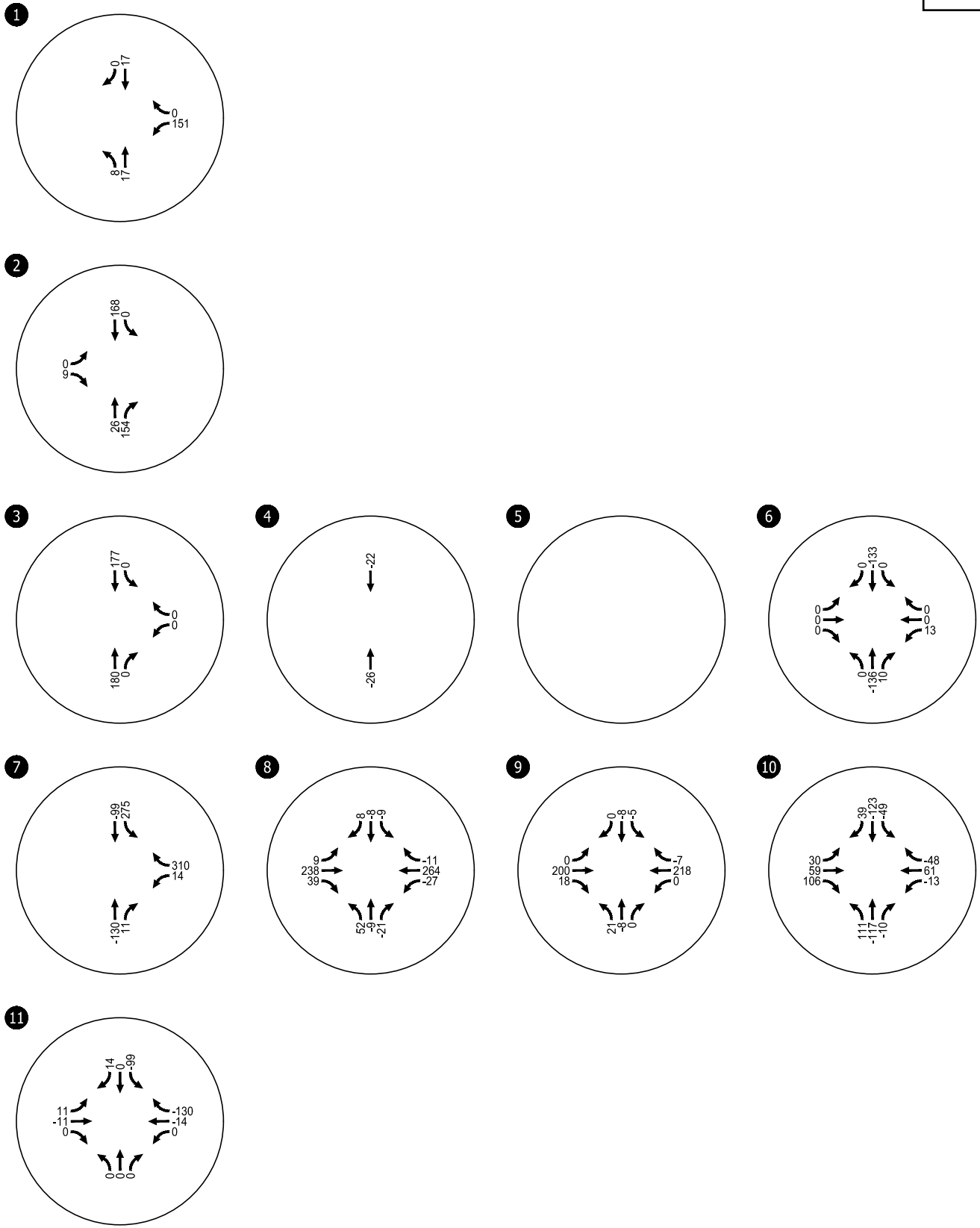


Redistributed Trips Under W Elm Avenue Option
Weekday AM Peak Hour (2032)
Hermiston, OR

Figure
11

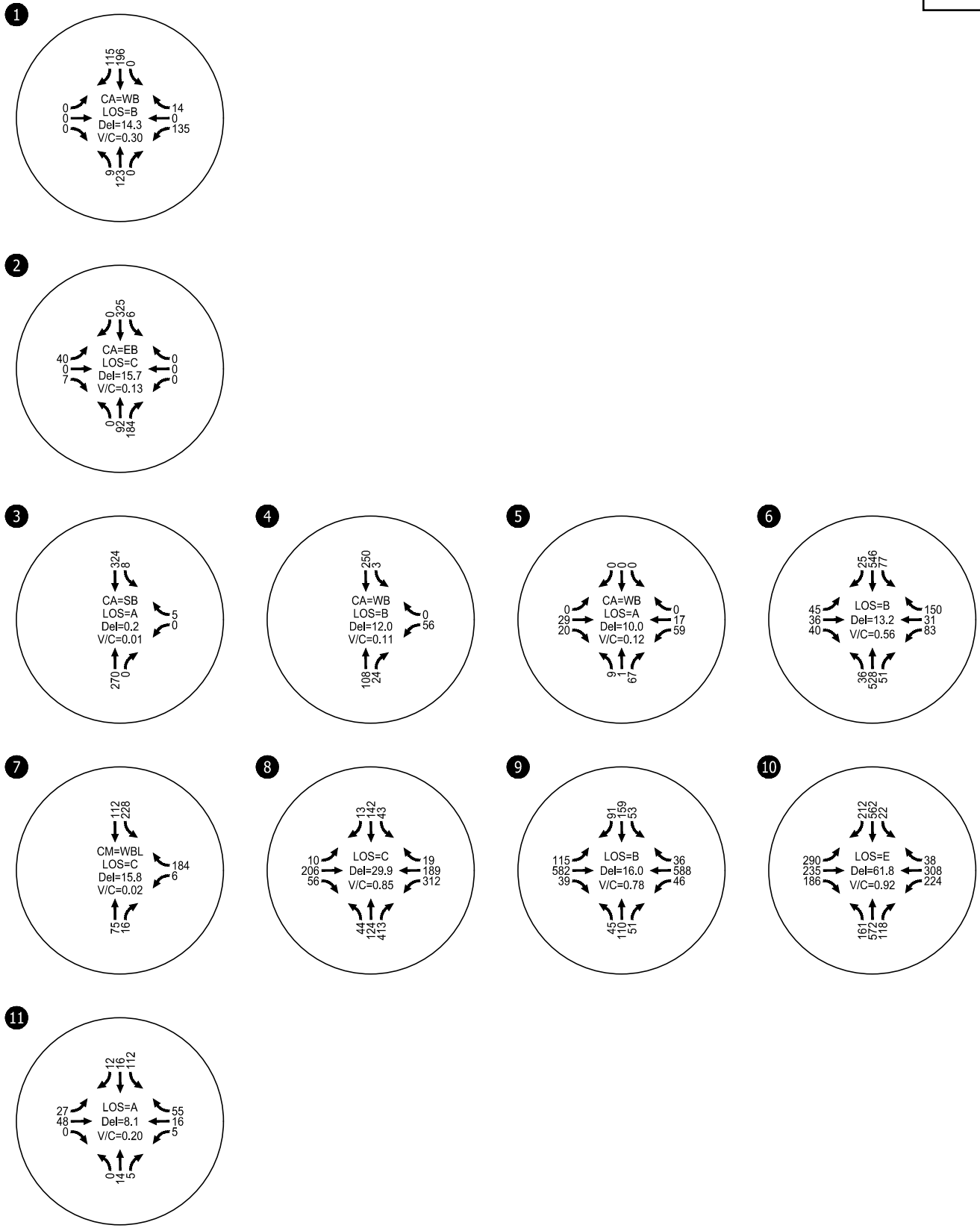
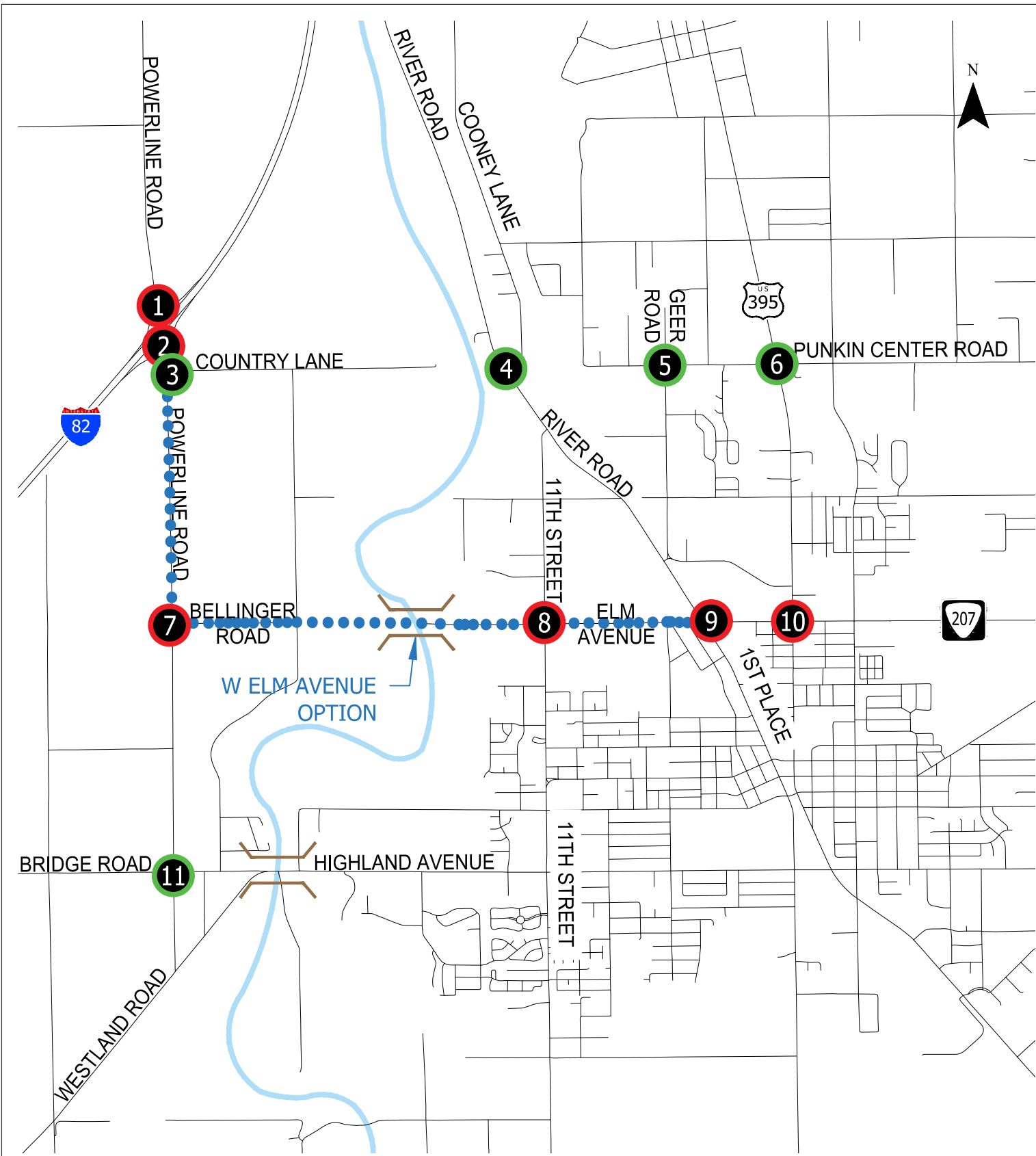


LEGEND
- STUDY INTERSECTION



Redistributed Trips Under W Elm Avenue Option
Weekday PM Peak Hour (2032)
Hermiston, OR

Figure
12



LEGEND

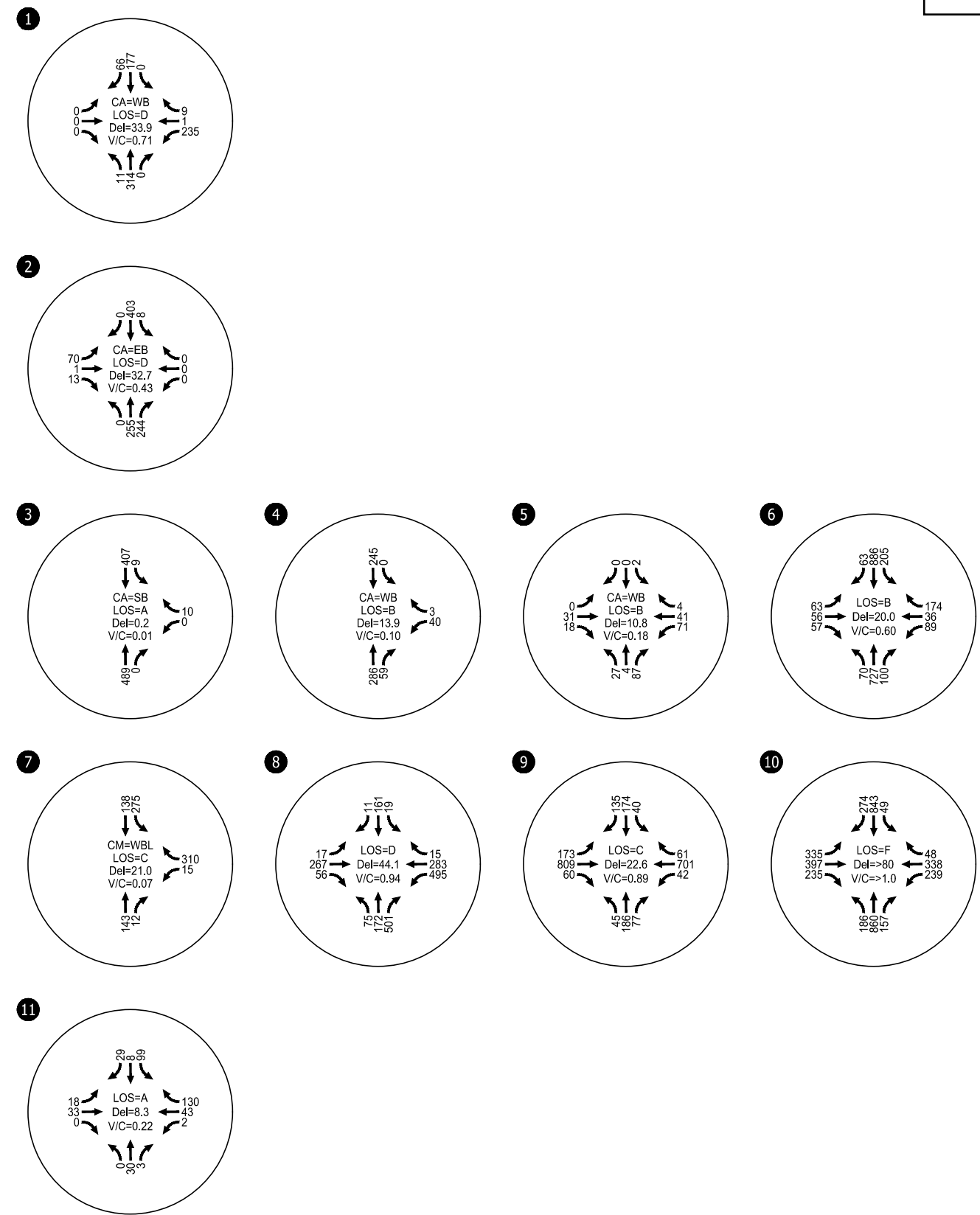
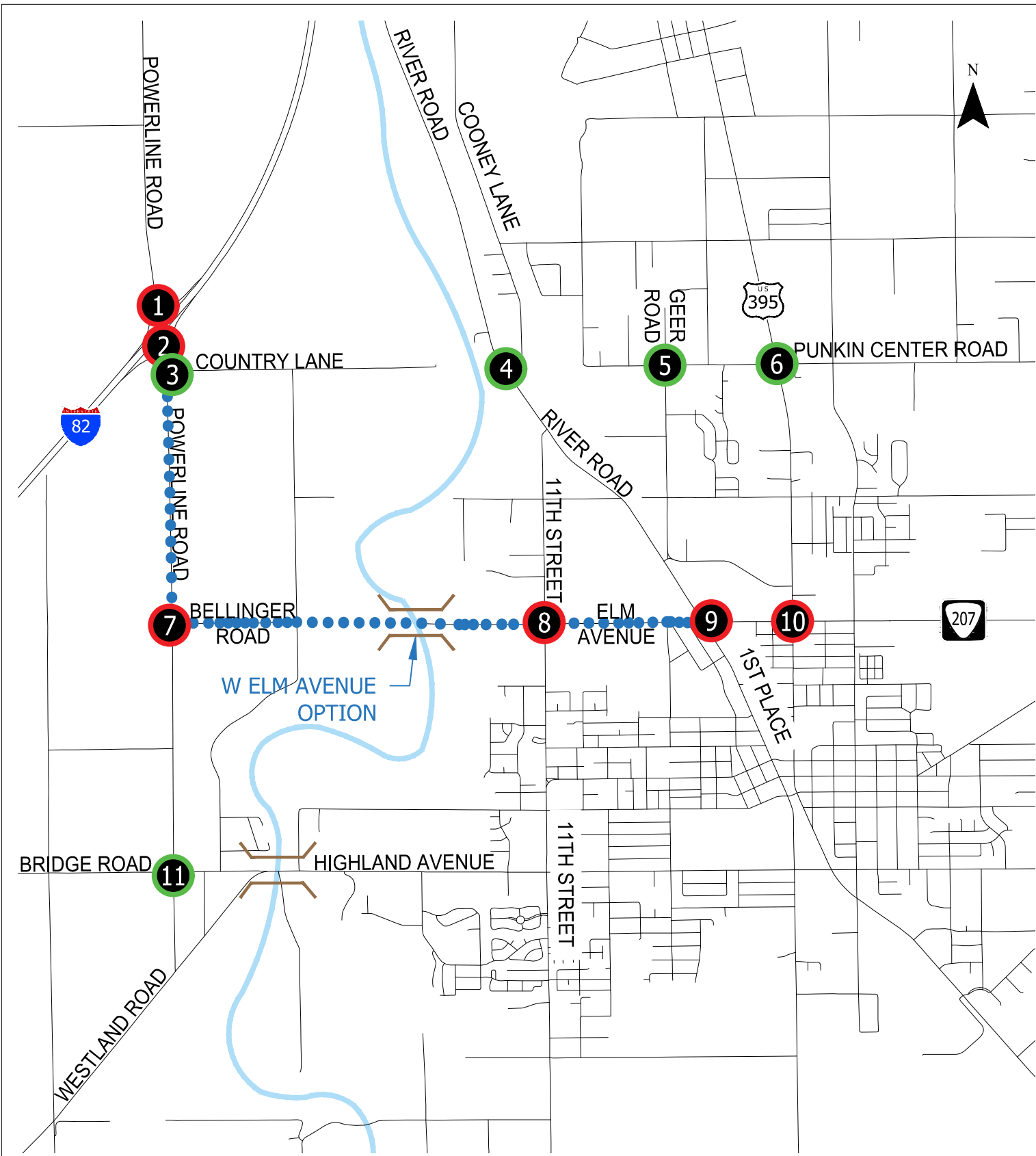
CA = CONTROL APPROACH
LOS = LEVEL OF SERVICE
Del = DELAY
V/C = VOLUME-TO-CAPACITY RATIO

- STUDY INTERSECTION HAS NO OPERATIONAL ISSUES OR CAPACITY CONSTRAINTS

- STUDY INTERSECTION HAS OPERATIONAL ISSUES THAT WILL REQUIRE CAPACITY OR GEOMETRIC IMPROVEMENTS

Total Traffic Volumes Under W Elm Avenue Option
Weekday AM Peak Hour (2032)
Hermiston, OR

Figure
13



LEGEND

CA = CONTROL APPROACH
LOS = LEVEL OF SERVICE
Del = DELAY
V/C = VOLUME-TO-CAPACITY RATIO

- STUDY INTERSECTION HAS NO OPERATIONAL ISSUES OR CAPACITY CONSTRAINTS

- STUDY INTERSECTION HAS OPERATIONAL ISSUES THAT WILL REQUIRE CAPACITY OR GEOMETRIC IMPROVEMENTS

Total Traffic Volumes Under W Elm Avenue Option
Weekday PM Peak Hour (2032)
Hermiston, OR

Figure
14

Table 3 summarizes the future operational needs associated with alternative roadway extension options at key study intersections.

Table 3: Future Operational/Geometric Improvement Needs Associated with Roadway Extension Options

| Intersection/ Roadway Corridor | W Punkin Center Option Improvement Needs | W Elm Avenue Option Improvement Needs |
|-----------------------------------------------------|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| US 395 / Punkin Center Road | <ul style="list-style-type: none"> Construct separate eastbound right-turn lane to manage expected long-term vehicle queues | <ul style="list-style-type: none"> None |
| River Road / Punkin Center Road / Cooney Lane | <ul style="list-style-type: none"> Construct separate left-turn and combined through and right-turn lanes on all approaches When warranted, construct a traffic signal Upgrade and modernize adjacent Union Pacific Railroad Crossing Realign Cooney Lane to intersect the new east-west extension of W Punkin Center Road | <ul style="list-style-type: none"> None |
| Country Lane Corridor | <ul style="list-style-type: none"> Realign Country Lane and form a new intersection with Powerline Road to increase spacing from the I-82 interchange. The alignment and spacing should be determined through a separate multi-agency Interchange Area Management Plan (IAMP) Widen and modernize the remaining segment of Country Lane to the Umatilla River Bridge | <ul style="list-style-type: none"> None |
| Powerline Road Corridor | <ul style="list-style-type: none"> Widen and modernize¹ Powerline Road from the I-82 interchange to a relocated Country Lane intersection | <ul style="list-style-type: none"> Widen and modernize¹ Powerline Road from the I-82 interchange to Bellinger Road |
| Powerline Road / Country Lane | <ul style="list-style-type: none"> Construct a southbound left-turn lane on Powerline Road at the relocated Country Lane intersection Construct separate westbound right-turn and left-turn lanes on Country Lane at Powerline Road | <ul style="list-style-type: none"> None |
| I-82 Ramp Terminals | <ul style="list-style-type: none"> Long-term widening/modernization needed. Specific improvements to be identified within the IAMP planning effort | <ul style="list-style-type: none"> Long-term widening/modernization needed. Specific improvements to be identified within the IAMP planning effort |
| US 395 / Elm Avenue (OR 207) | <ul style="list-style-type: none"> Construct northbound right-turn lane, southbound right-turn lane, and westbound right-turn lane | <ul style="list-style-type: none"> Construct northbound right-turn lane, southbound right-turn lane, and westbound right-turn lane |
| 1 st Place / Elm Avenue (OR 207) | <ul style="list-style-type: none"> None | <ul style="list-style-type: none"> Construct a southbound right-turn lane Construct an eastbound right-turn lane |
| 11th Avenue / Elm Avenue (OR 207) | <ul style="list-style-type: none"> None | <ul style="list-style-type: none"> Construct eastbound right-turn lane |

| Intersection/ Roadway Corridor | W Punkin Center Option Improvement Needs | W Elm Avenue Option Improvement Needs |
|------------------------------------|--------------------------------------------------------|-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| Powerline Road / Bellinger Road | <ul style="list-style-type: none"> None | <ul style="list-style-type: none"> Construct southbound left-turn lane on Powerline Road at Bellinger Road Upgrade of Bellinger Road corridor to the Urban Major Arterial roadway standard Construct separate westbound right-turn and left-turn lanes on Bellinger Road at Powerline Road |
| Elm Avenue Corridor | <ul style="list-style-type: none"> None | <ul style="list-style-type: none"> Widen and modernize W Elm Avenue from the new bridge over the Umatilla River to NW 11th Avenue. East of NW 11th Avenue to US 395, infill sidewalk curb, and gutter. |
| Bellinger Road Corridor | <ul style="list-style-type: none"> None | <ul style="list-style-type: none"> Widen and modernize² Bellinger Road from Powerline Road to the new bridge over the Umatilla River bridge |

¹ Upgrade to modern roadway standards including two twelve-foot travel lanes (widened to include a southbound left-turn lane at major intersections such as Country Lane under W Punkin Center Option and Bellinger Road under the W Elm Avenue Option), five-foot shoulders/bike lanes, curb and gutter, and five-foot sidewalks.

² Upgrade to modern roadway standards including two twelve-foot travel lanes, five-foot shoulders/bike lanes, curb and gutter, and five-foot sidewalks.

Appendix A

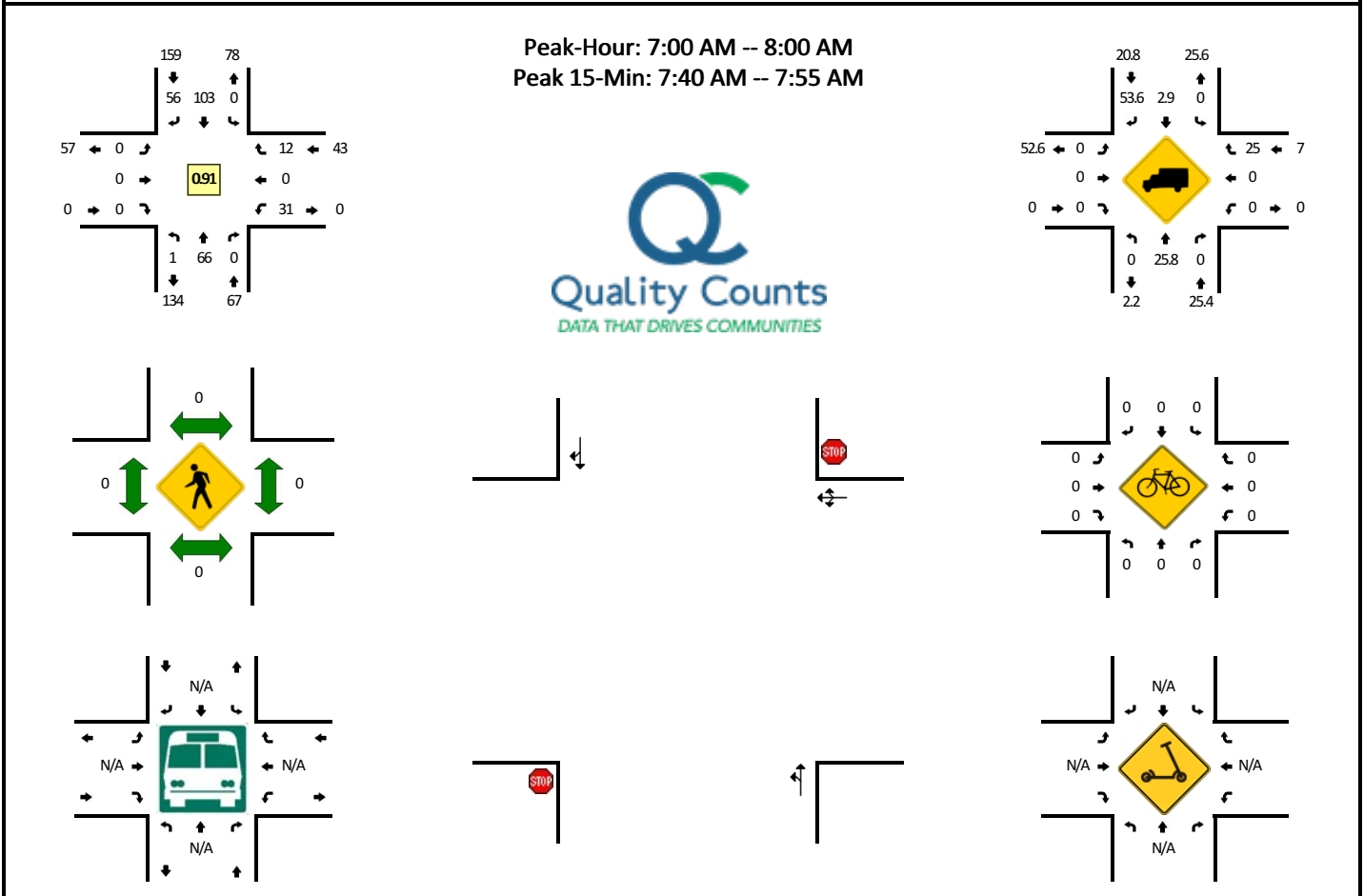
Traffic Count Worksheets

LOCATION: Powerline Rd -- I-82 WB Ramps

CITY/STATE: Umatilla, OR

QC JOB #: 15570919

DATE: Thu, Oct 7 2021

Peak-Hour: 7:00 AM -- 8:00 AM
Peak 15-Min: 7:40 AM -- 7:55 AM

| 5-Min Count Period Beginning At | Powerline Rd (Northbound) | | | | Powerline Rd (Southbound) | | | | I-82 WB Ramps (Eastbound) | | | | I-82 WB Ramps (Westbound) | | | | Total | Hourly Totals |
|---------------------------------|---------------------------|------|-------|---|---------------------------|------|-------|---|---------------------------|------|-------|---|---------------------------|------|-------|---|-------|---------------|
| | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | | |
| 7:00 AM | 0 | 6 | 0 | 0 | 0 | 3 | 3 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 0 | 0 | 14 | |
| 7:05 AM | 0 | 6 | 0 | 0 | 0 | 6 | 4 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 3 | 0 | 22 | |
| 7:10 AM | 0 | 7 | 0 | 0 | 0 | 5 | 6 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 2 | 0 | 22 | |
| 7:15 AM | 0 | 12 | 0 | 0 | 0 | 7 | 6 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 1 | 0 | 28 | |
| 7:20 AM | 0 | 3 | 0 | 0 | 0 | 7 | 5 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 2 | 0 | 21 | |
| 7:25 AM | 0 | 5 | 0 | 0 | 0 | 10 | 3 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 2 | 0 | 24 | |
| 7:30 AM | 0 | 4 | 0 | 0 | 0 | 10 | 3 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 0 | 0 | 19 | |
| 7:35 AM | 0 | 5 | 0 | 0 | 0 | 9 | 8 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 0 | 0 | 24 | |
| 7:40 AM | 0 | 4 | 0 | 0 | 0 | 13 | 5 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 0 | 0 | 26 | |
| 7:45 AM | 1 | 4 | 0 | 0 | 0 | 11 | 2 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 0 | 20 | |
| 7:50 AM | 0 | 5 | 0 | 0 | 0 | 12 | 5 | 0 | 0 | 0 | 0 | 0 | 5 | 0 | 1 | 0 | 28 | |
| 7:55 AM | 0 | 5 | 0 | 0 | 0 | 10 | 6 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 21 | 269 |
| 8:00 AM | 0 | 5 | 0 | 0 | 0 | 5 | 1 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 0 | 0 | 13 | 268 |
| 8:05 AM | 0 | 3 | 0 | 0 | 0 | 8 | 4 | 0 | 0 | 0 | 0 | 0 | 2 | 1 | 0 | 0 | 18 | 264 |
| 8:10 AM | 0 | 6 | 0 | 0 | 0 | 11 | 5 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 1 | 0 | 25 | 267 |
| 8:15 AM | 0 | 7 | 0 | 0 | 0 | 3 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 0 | 12 | 251 |
| 8:20 AM | 0 | 6 | 0 | 0 | 0 | 5 | 4 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 0 | 0 | 18 | 248 |
| 8:25 AM | 0 | 12 | 0 | 0 | 0 | 4 | 4 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 0 | 0 | 23 | 247 |
| 8:30 AM | 0 | 9 | 0 | 0 | 0 | 5 | 4 | 0 | 0 | 0 | 0 | 0 | 1 | 1 | 0 | 0 | 20 | 248 |
| 8:35 AM | 0 | 8 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 1 | 0 | 12 | 236 |
| 8:40 AM | 1 | 5 | 0 | 0 | 0 | 4 | 6 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 16 | 226 |
| 8:45 AM | 0 | 5 | 0 | 0 | 0 | 7 | 3 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 0 | 17 | 223 |
| 8:50 AM | 0 | 8 | 0 | 0 | 0 | 1 | 3 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 14 | 209 |
| 8:55 AM | 0 | 8 | 0 | 0 | 0 | 3 | 4 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 16 | 204 |
| Peak 15-Min Flowrates | Northbound | | | | Southbound | | | | Eastbound | | | | Westbound | | | | Total | |
| | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | | |
| All Vehicles | 4 | 52 | 0 | 0 | 0 | 144 | 48 | 0 | 0 | 0 | 0 | 0 | 40 | 0 | 8 | 0 | 296 | |
| Heavy Trucks | 0 | 20 | 0 | 0 | 0 | 0 | 32 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 52 | |
| Buses | | | | | | | | | | | | | | | | | | |
| Pedestrians | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Bicycles | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Scooters | | | | | | | | | | | | | | | | | | |

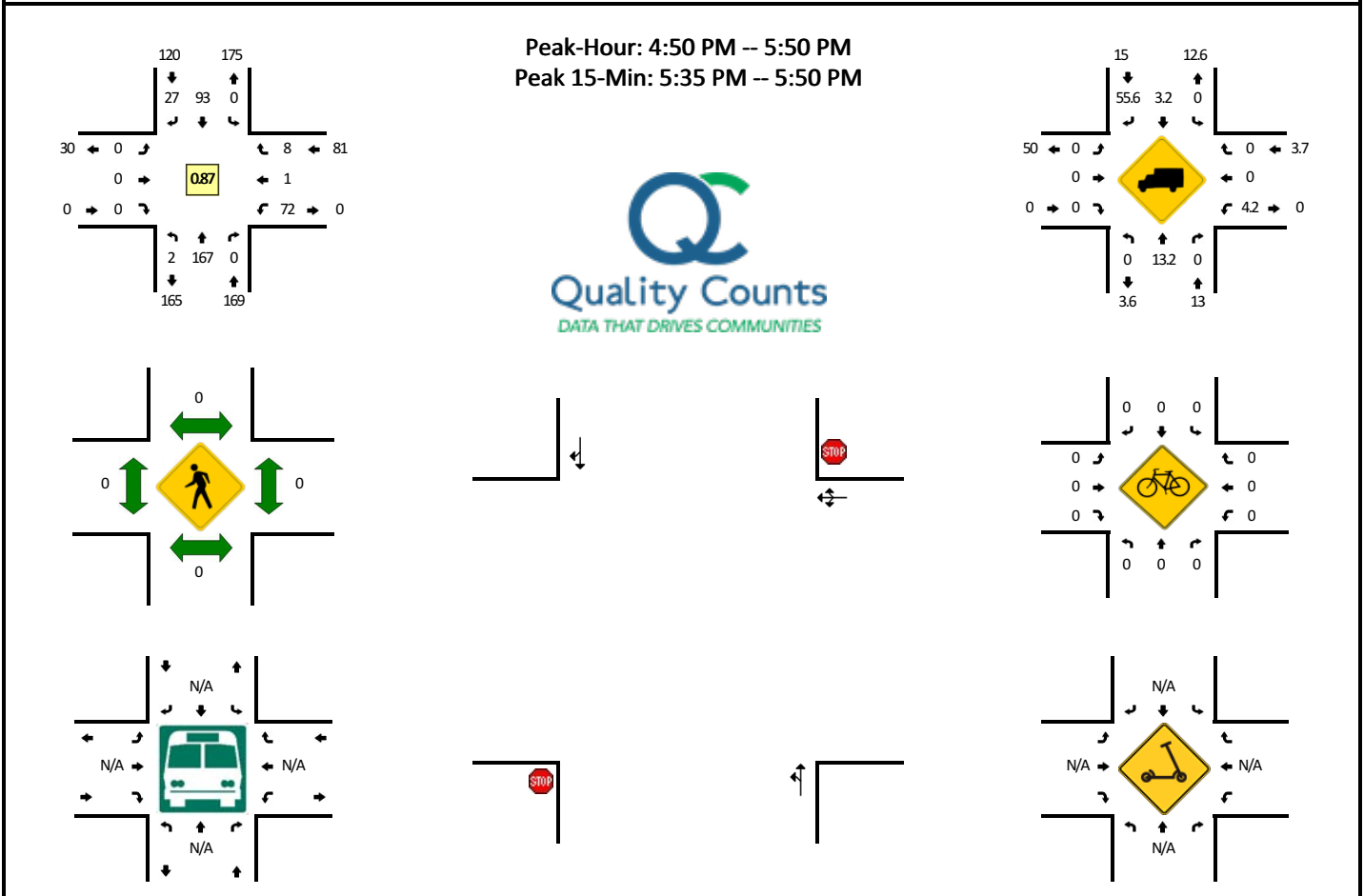
Comments:

LOCATION: Powerline Rd -- I-82 WB Ramps

CITY/STATE: Umatilla, OR

QC JOB #: 15570920

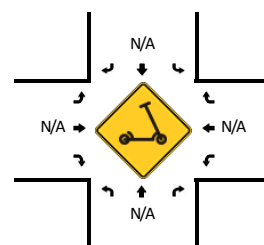
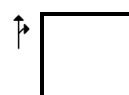
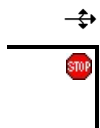
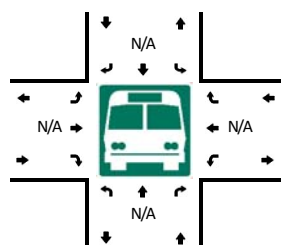
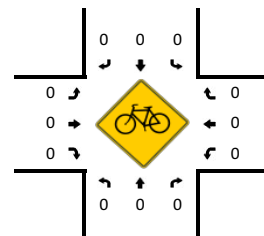
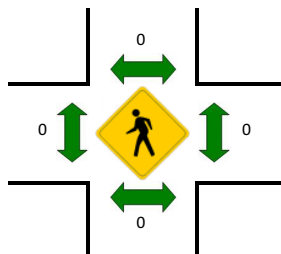
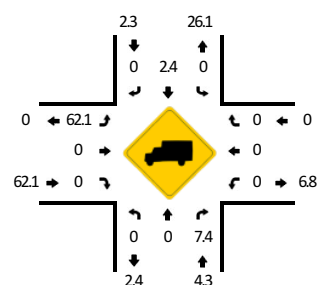
DATE: Wed, Oct 6 2021

Peak-Hour: 4:50 PM -- 5:50 PM
Peak 15-Min: 5:35 PM -- 5:50 PM

| 5-Min Count Period Beginning At | Powerline Rd (Northbound) | | | | Powerline Rd (Southbound) | | | | I-82 WB Ramps (Eastbound) | | | | I-82 WB Ramps (Westbound) | | | | Total | Hourly Totals |
|---------------------------------|---------------------------|------|-------|---|---------------------------|------|-------|---|---------------------------|------|-------|---|---------------------------|------|-------|---|-------|---------------|
| | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | | |
| 4:00 PM | 0 | 12 | 0 | 0 | 0 | 10 | 3 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 0 | 0 | 29 | |
| 4:05 PM | 0 | 13 | 0 | 0 | 0 | 8 | 2 | 0 | 0 | 0 | 0 | 0 | 5 | 0 | 1 | 0 | 29 | |
| 4:10 PM | 0 | 17 | 0 | 0 | 0 | 9 | 3 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 2 | 0 | 34 | |
| 4:15 PM | 0 | 10 | 0 | 0 | 0 | 11 | 3 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 0 | 0 | 27 | |
| 4:20 PM | 0 | 16 | 0 | 0 | 0 | 2 | 4 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 1 | 0 | 24 | |
| 4:25 PM | 0 | 17 | 0 | 0 | 0 | 5 | 4 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 1 | 0 | 31 | |
| 4:30 PM | 0 | 14 | 0 | 0 | 0 | 5 | 1 | 0 | 0 | 0 | 0 | 0 | 6 | 0 | 0 | 0 | 26 | |
| 4:35 PM | 0 | 17 | 0 | 0 | 0 | 7 | 3 | 0 | 0 | 0 | 0 | 0 | 5 | 0 | 0 | 0 | 32 | |
| 4:40 PM | 0 | 20 | 0 | 0 | 0 | 5 | 4 | 0 | 0 | 0 | 0 | 0 | 6 | 0 | 0 | 0 | 35 | |
| 4:45 PM | 0 | 11 | 0 | 0 | 0 | 8 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 0 | 0 | 21 | |
| 4:50 PM | 0 | 9 | 0 | 0 | 0 | 6 | 3 | 0 | 0 | 0 | 0 | 0 | 6 | 0 | 2 | 0 | 26 | |
| 4:55 PM | 1 | 8 | 0 | 0 | 0 | 6 | 4 | 0 | 0 | 0 | 0 | 0 | 8 | 1 | 1 | 0 | 29 | 343 |
| 5:00 PM | 0 | 18 | 0 | 0 | 0 | 4 | 4 | 0 | 0 | 0 | 0 | 0 | 7 | 0 | 0 | 0 | 33 | 347 |
| 5:05 PM | 0 | 10 | 0 | 0 | 0 | 9 | 1 | 0 | 0 | 0 | 0 | 0 | 5 | 0 | 0 | 0 | 25 | 343 |
| 5:10 PM | 0 | 14 | 0 | 0 | 0 | 6 | 3 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 1 | 0 | 27 | 336 |
| 5:15 PM | 0 | 15 | 0 | 0 | 0 | 8 | 2 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 0 | 0 | 29 | 338 |
| 5:20 PM | 0 | 15 | 0 | 0 | 0 | 9 | 2 | 0 | 0 | 0 | 0 | 0 | 5 | 0 | 1 | 0 | 32 | 346 |
| 5:25 PM | 0 | 14 | 0 | 0 | 0 | 9 | 1 | 0 | 0 | 0 | 0 | 0 | 7 | 0 | 1 | 0 | 32 | 347 |
| 5:30 PM | 0 | 12 | 0 | 0 | 0 | 11 | 1 | 0 | 0 | 0 | 0 | 0 | 6 | 0 | 1 | 0 | 31 | 352 |
| 5:35 PM | 1 | 17 | 0 | 0 | 0 | 6 | 0 | 0 | 0 | 0 | 0 | 0 | 9 | 0 | 1 | 0 | 34 | 354 |
| 5:40 PM | 0 | 16 | 0 | 0 | 0 | 12 | 3 | 0 | 0 | 0 | 0 | 0 | 5 | 0 | 0 | 0 | 36 | 355 |
| 5:45 PM | 0 | 19 | 0 | 0 | 0 | 7 | 3 | 0 | 0 | 0 | 0 | 0 | 7 | 0 | 0 | 0 | 36 | 370 |
| 5:50 PM | 0 | 6 | 0 | 0 | 0 | 4 | 6 | 0 | 0 | 0 | 0 | 0 | 6 | 0 | 0 | 0 | 22 | 366 |
| 5:55 PM | 0 | 11 | 0 | 0 | 0 | 7 | 3 | 0 | 0 | 0 | 0 | 0 | 7 | 0 | 1 | 0 | 29 | 366 |
| Peak 15-Min Flowrates | Northbound | | | | Southbound | | | | Eastbound | | | | Westbound | | | | Total | |
| | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | | |
| All Vehicles | 4 | 208 | 0 | 0 | 0 | 100 | 24 | 0 | 0 | 0 | 0 | 0 | 84 | 0 | 4 | 0 | 424 | |
| Heavy Trucks | 0 | 20 | 0 | 0 | 0 | 8 | 20 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 0 | 0 | 52 | |
| Buses | | | | | | | | | | | | | | | | | | |
| Pedestrians | | 0 | | | | 0 | | | | 0 | | | | 0 | | | 0 | |
| Bicycles | 0 | 0 | 0 | | 0 | 0 | 0 | | 0 | 0 | 0 | | 0 | 0 | 0 | | 0 | |
| Scoters | | | | | | | | | | | | | | | | | | |

Comments:

QC JOB #: 15570917
DATE: Thu, Oct 7 2021



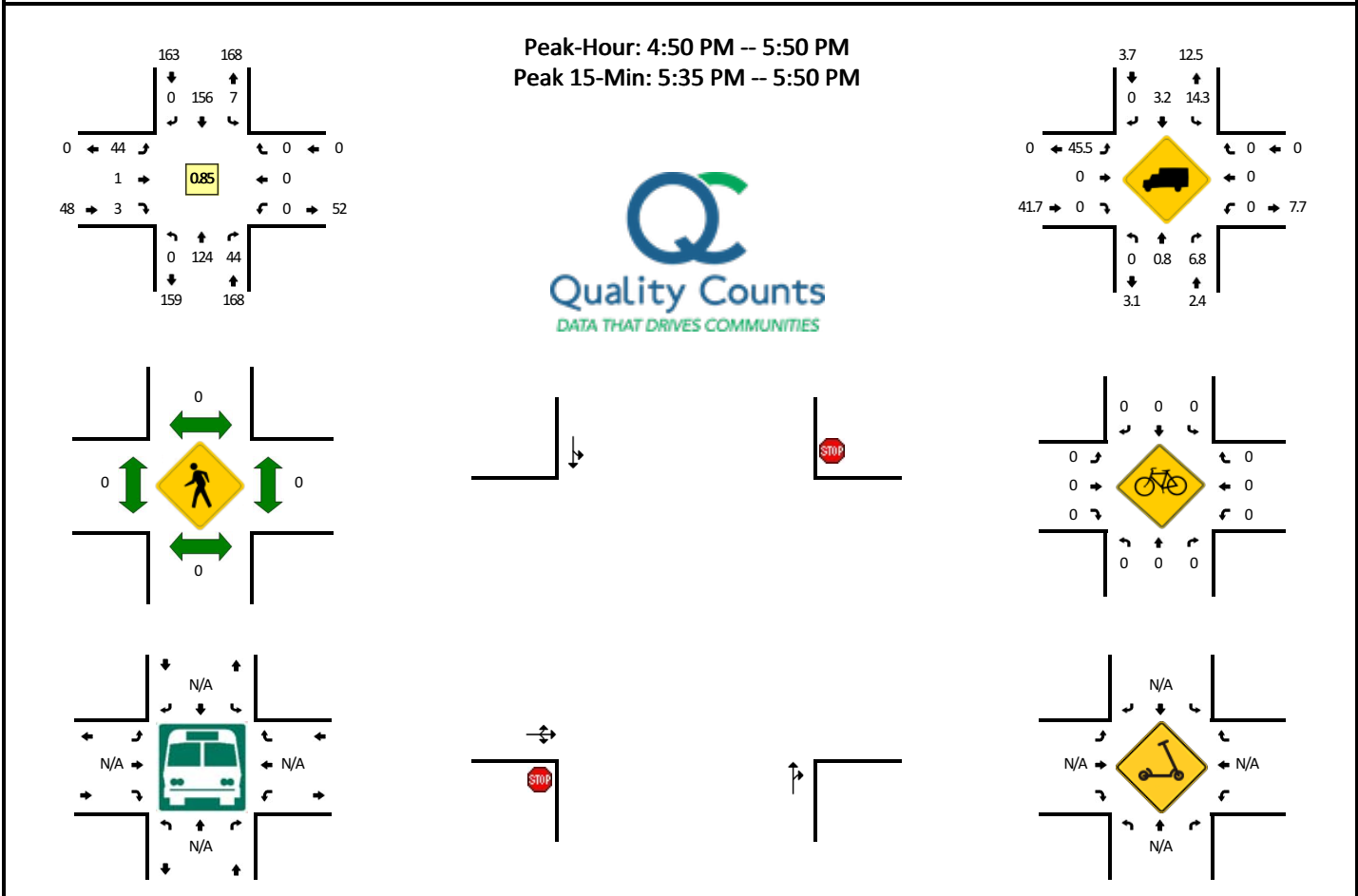
Comments:

LOCATION: Powerline Rd -- I-82 EB Ramps

CITY/STATE: Umatilla, OR

QC JOB #: 15570918

DATE: Wed, Oct 6 2021

Peak-Hour: 4:50 PM -- 5:50 PM
Peak 15-Min: 5:35 PM -- 5:50 PM

| 5-Min Count Period Beginning At | Powerline Rd (Northbound) | | | | Powerline Rd (Southbound) | | | | I-82 EB Ramps (Eastbound) | | | | I-82 EB Ramps (Westbound) | | | | Total | Hourly Totals |
|---------------------------------|---------------------------|------|-------|---|---------------------------|------|-------|---|---------------------------|------|-------|---|---------------------------|------|-------|---|-------|---------------|
| | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | | |
| 4:00 PM | 0 | 10 | 6 | 0 | 3 | 12 | 0 | 0 | 5 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 36 | |
| 4:05 PM | 0 | 6 | 4 | 0 | 2 | 11 | 0 | 0 | 4 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 27 | |
| 4:10 PM | 0 | 11 | 7 | 0 | 1 | 11 | 0 | 0 | 7 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 37 | |
| 4:15 PM | 0 | 8 | 5 | 0 | 1 | 13 | 0 | 0 | 1 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 29 | |
| 4:20 PM | 0 | 12 | 2 | 0 | 0 | 1 | 0 | 0 | 5 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 21 | |
| 4:25 PM | 0 | 13 | 7 | 0 | 1 | 9 | 0 | 0 | 5 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 35 | |
| 4:30 PM | 0 | 8 | 11 | 0 | 1 | 9 | 0 | 0 | 4 | 1 | 1 | 0 | 0 | 0 | 0 | 0 | 35 | |
| 4:35 PM | 0 | 13 | 6 | 0 | 0 | 12 | 0 | 0 | 4 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 35 | |
| 4:40 PM | 0 | 14 | 6 | 0 | 2 | 10 | 0 | 0 | 7 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 40 | |
| 4:45 PM | 0 | 8 | 4 | 0 | 2 | 8 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 23 | |
| 4:50 PM | 0 | 9 | 5 | 0 | 2 | 9 | 0 | 0 | 1 | 1 | 1 | 0 | 0 | 0 | 0 | 0 | 28 | |
| 4:55 PM | 0 | 6 | 3 | 0 | 1 | 13 | 0 | 0 | 5 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 28 | 374 |
| 5:00 PM | 0 | 12 | 6 | 0 | 0 | 11 | 0 | 0 | 4 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 33 | 371 |
| 5:05 PM | 0 | 6 | 1 | 0 | 0 | 15 | 0 | 0 | 5 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 28 | 372 |
| 5:10 PM | 0 | 13 | 3 | 0 | 0 | 8 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 25 | 360 |
| 5:15 PM | 0 | 9 | 3 | 0 | 0 | 13 | 0 | 0 | 4 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 29 | 360 |
| 5:20 PM | 0 | 10 | 3 | 0 | 0 | 12 | 0 | 0 | 4 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 29 | 368 |
| 5:25 PM | 0 | 9 | 5 | 0 | 0 | 15 | 0 | 0 | 6 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 35 | 368 |
| 5:30 PM | 0 | 10 | 4 | 0 | 0 | 15 | 0 | 0 | 4 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 33 | 366 |
| 5:35 PM | 0 | 11 | 3 | 0 | 0 | 18 | 0 | 0 | 5 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 37 | 368 |
| 5:40 PM | 0 | 14 | 2 | 0 | 2 | 14 | 0 | 0 | 3 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 36 | 364 |
| 5:45 PM | 0 | 15 | 6 | 0 | 2 | 13 | 0 | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 38 | 379 |
| 5:50 PM | 0 | 7 | 3 | 0 | 0 | 10 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 20 | 371 |
| 5:55 PM | 0 | 7 | 3 | 0 | 0 | 15 | 0 | 0 | 3 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 28 | 371 |
| Peak 15-Min Flowrates | Northbound | | | | Southbound | | | | Eastbound | | | | Westbound | | | | Total | |
| | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | | |
| All Vehicles | 0 | 160 | 44 | 0 | 16 | 180 | 0 | 0 | 40 | 0 | 4 | 0 | 0 | 0 | 0 | 0 | 444 | |
| Heavy Trucks | 0 | 0 | 4 | 0 | 4 | 8 | 0 | 0 | 16 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 32 | |
| Buses | | | | | | | | | | | | | | | | | | |
| Pedestrians | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Bicycles | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Scooters | | | | | | | | | | | | | | | | | | |

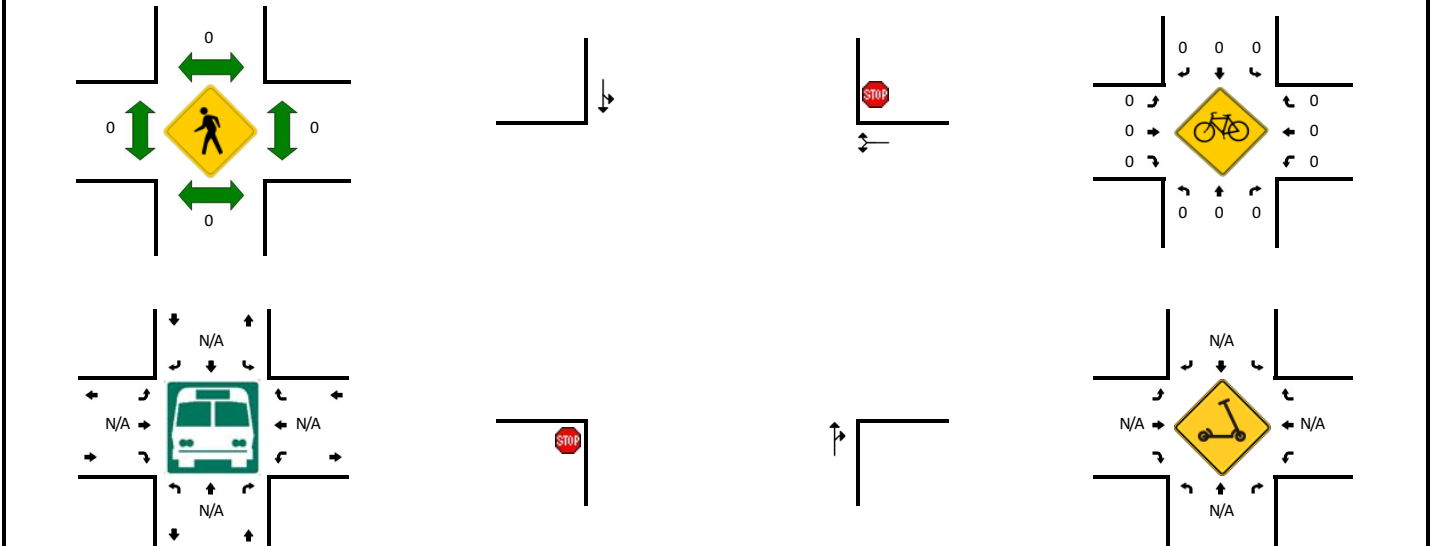
Comments:

LOCATION: Powerline Rd -- Bellinger Rd

CITY/STATE: Umatilla, OR

QC JOB #: 15570901

DATE: Thu, Oct 7 2021

Peak-Hour: 7:05 AM -- 8:05 AM
Peak 15-Min: 7:45 AM -- 8:00 AM

| 5-Min Count Period Beginning At | Powerline Rd (Northbound) | | | | Powerline Rd (Southbound) | | | | Bellinger Rd (Eastbound) | | | | Bellinger Rd (Westbound) | | | | Total | Hourly Totals |
|------------------------------------|---------------------------|------|-------|---|---------------------------|------|-------|---|--------------------------|------|-------|---|--------------------------|------|-------|---|-------|---------------|
| | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | | |
| 7:00 AM | 0 | 9 | 0 | 0 | 0 | 5 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 14 | |
| 7:05 AM | 0 | 12 | 0 | 0 | 0 | 5 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 17 | |
| 7:10 AM | 0 | 10 | 0 | 0 | 0 | 10 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 20 | |
| 7:15 AM | 0 | 13 | 0 | 0 | 0 | 8 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 21 | |
| 7:20 AM | 0 | 8 | 0 | 0 | 0 | 8 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 16 | |
| 7:25 AM | 0 | 6 | 0 | 0 | 0 | 15 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 21 | |
| 7:30 AM | 0 | 7 | 0 | 0 | 0 | 15 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 22 | |
| 7:35 AM | 0 | 5 | 0 | 0 | 0 | 8 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 13 | |
| 7:40 AM | 0 | 4 | 0 | 0 | 0 | 14 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 18 | |
| 7:45 AM | 0 | 3 | 0 | 0 | 0 | 16 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 19 | |
| 7:50 AM | 0 | 8 | 0 | 0 | 0 | 15 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 23 | |
| 7:55 AM | 0 | 6 | 0 | 0 | 0 | 12 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 19 | 223 |
| 8:00 AM | 0 | 7 | 0 | 0 | 0 | 10 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 17 | 226 |
| 8:05 AM | 0 | 8 | 1 | 0 | 0 | 3 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 12 | 221 |
| 8:10 AM | 0 | 7 | 0 | 0 | 0 | 16 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 23 | 224 |
| 8:15 AM | 0 | 11 | 0 | 0 | 0 | 4 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 15 | 218 |
| 8:20 AM | 0 | 11 | 0 | 0 | 0 | 7 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 18 | 220 |
| 8:25 AM | 0 | 15 | 0 | 0 | 0 | 8 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 23 | 222 |
| 8:30 AM | 0 | 6 | 0 | 0 | 0 | 5 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 11 | 211 |
| 8:35 AM | 0 | 8 | 0 | 0 | 0 | 4 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 12 | 210 |
| 8:40 AM | 0 | 6 | 0 | 0 | 0 | 5 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 11 | 203 |
| 8:45 AM | 0 | 5 | 0 | 0 | 0 | 7 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 12 | 196 |
| 8:50 AM | 0 | 6 | 0 | 0 | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 8 | 181 |
| 8:55 AM | 0 | 6 | 0 | 0 | 0 | 4 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 10 | 172 |
| Peak 15-Min Flowrates | Northbound | | | | Southbound | | | | Eastbound | | | | Westbound | | | | Total | |
| | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | | |
| All Vehicles | 0 | 68 | 0 | 0 | 0 | 172 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 0 | 0 | 244 | |
| Heavy Trucks | 0 | 0 | 0 | 0 | 0 | 8 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 8 | |
| Buses | | | | | | | | | | | | | | | | | | |
| Pedestrians | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Bicycles | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Scooters | | | | | | | | | | | | | | | | | | |

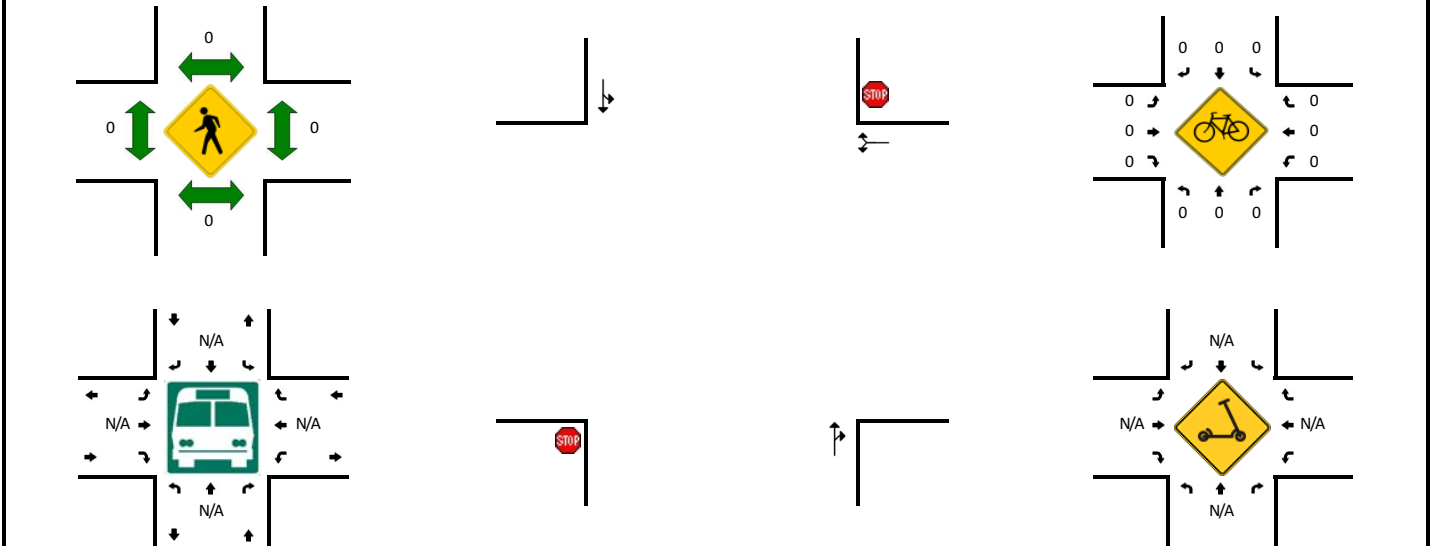
Comments:

LOCATION: Powerline Rd -- Bellinger Rd

CITY/STATE: Umatilla, OR

QC JOB #: 15570902

DATE: Wed, Oct 6 2021

Peak-Hour: 4:55 PM -- 5:55 PM
Peak 15-Min: 5:25 PM -- 5:40 PM

| 5-Min Count Period Beginning At | Powerline Rd (Northbound) | | | | Powerline Rd (Southbound) | | | | Bellinger Rd (Eastbound) | | | | Bellinger Rd (Westbound) | | | | Total | Hourly Totals |
|---------------------------------|---------------------------|------|-------|---|---------------------------|------|-------|---|--------------------------|------|-------|---|--------------------------|------|-------|---|-------|---------------|
| | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | | |
| 4:00 PM | 0 | 14 | 0 | 0 | 0 | 15 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 29 | |
| 4:05 PM | 0 | 15 | 0 | 0 | 0 | 11 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 26 | |
| 4:10 PM | 0 | 15 | 0 | 0 | 0 | 12 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 27 | |
| 4:15 PM | 0 | 17 | 0 | 0 | 0 | 13 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 30 | |
| 4:20 PM | 0 | 15 | 0 | 0 | 0 | 3 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 18 | |
| 4:25 PM | 0 | 20 | 0 | 0 | 0 | 5 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 25 | |
| 4:30 PM | 0 | 19 | 0 | 0 | 0 | 9 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 28 | |
| 4:35 PM | 0 | 19 | 0 | 0 | 0 | 9 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 28 | |
| 4:40 PM | 0 | 19 | 0 | 0 | 0 | 13 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 33 | |
| 4:45 PM | 0 | 14 | 0 | 0 | 0 | 9 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 23 | |
| 4:50 PM | 0 | 9 | 0 | 0 | 0 | 7 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 16 | |
| 4:55 PM | 0 | 14 | 1 | 0 | 0 | 15 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 30 | 313 |
| 5:00 PM | 0 | 12 | 0 | 0 | 0 | 11 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 23 | 307 |
| 5:05 PM | 0 | 11 | 0 | 0 | 0 | 19 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 30 | 311 |
| 5:10 PM | 0 | 20 | 0 | 0 | 0 | 9 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 29 | 313 |
| 5:15 PM | 0 | 12 | 0 | 0 | 0 | 12 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 24 | 307 |
| 5:20 PM | 0 | 11 | 0 | 0 | 0 | 13 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 24 | 313 |
| 5:25 PM | 0 | 13 | 0 | 0 | 0 | 13 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 26 | 314 |
| 5:30 PM | 0 | 15 | 0 | 0 | 0 | 18 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 33 | 319 |
| 5:35 PM | 0 | 14 | 0 | 0 | 0 | 15 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 29 | 320 |
| 5:40 PM | 0 | 14 | 0 | 0 | 0 | 11 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 25 | 312 |
| 5:45 PM | 0 | 19 | 0 | 0 | 0 | 12 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 31 | 320 |
| 5:50 PM | 0 | 8 | 0 | 0 | 0 | 11 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 19 | 323 |
| 5:55 PM | 0 | 11 | 0 | 0 | 0 | 16 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 27 | 320 |
| Peak 15-Min Flowrates | Northbound | | | | Southbound | | | | Eastbound | | | | Westbound | | | | Total | |
| | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | | |
| All Vehicles | 0 | 168 | 0 | 0 | 0 | 184 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 352 | |
| Heavy Trucks | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | |
| Buses | | | | | | | | | | | | | | | | | | |
| Pedestrians | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Bicycles | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Scoters | | | | | | | | | | | | | | | | | | |

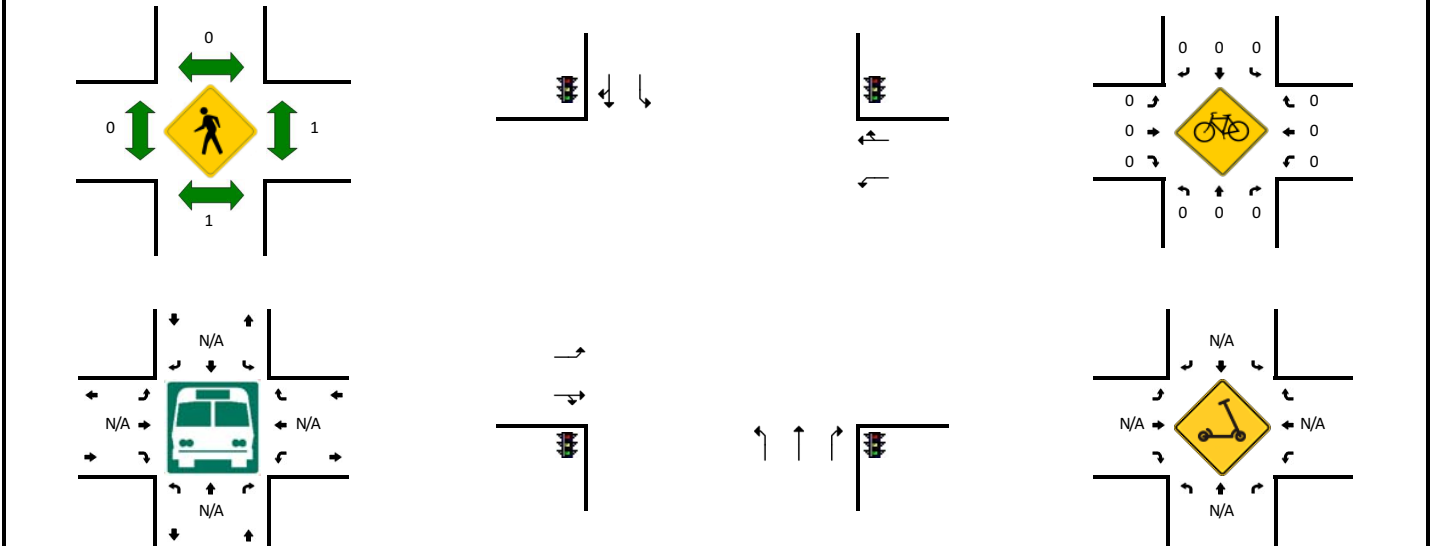
Comments:

LOCATION: NW 11th St -- W Elm Ave

CITY/STATE: Hermiston, OR

QC JOB #: 15570903

DATE: Thu, Oct 7 2021

Peak-Hour: 7:40 AM -- 8:40 AM
Peak 15-Min: 7:50 AM -- 8:05 AM

| 5-Min Count Period Beginning At | NW 11th St (Northbound) | | | | NW 11th St (Southbound) | | | | W Elm Ave (Eastbound) | | | | W Elm Ave (Westbound) | | | | Total | Hourly Totals |
|---------------------------------|-------------------------|------|-------|---|-------------------------|------|-------|---|-----------------------|------|-------|---|-----------------------|------|-------|---|-------|---------------|
| | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | | |
| 7:00 AM | 0 | 2 | 23 | 0 | 2 | 6 | 0 | 0 | 0 | 0 | 1 | 0 | 12 | 4 | 1 | 0 | 51 | |
| 7:05 AM | 1 | 9 | 18 | 0 | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 16 | 4 | 0 | 0 | 50 | |
| 7:10 AM | 1 | 6 | 19 | 0 | 0 | 6 | 0 | 0 | 3 | 0 | 0 | 0 | 20 | 2 | 2 | 0 | 59 | |
| 7:15 AM | 1 | 4 | 20 | 0 | 3 | 11 | 1 | 0 | 1 | 0 | 0 | 0 | 15 | 0 | 0 | 0 | 56 | |
| 7:20 AM | 2 | 12 | 13 | 0 | 1 | 5 | 0 | 0 | 0 | 3 | 1 | 0 | 17 | 0 | 0 | 0 | 54 | |
| 7:25 AM | 1 | 5 | 18 | 0 | 4 | 13 | 0 | 0 | 2 | 0 | 1 | 0 | 16 | 3 | 1 | 0 | 64 | |
| 7:30 AM | 0 | 9 | 23 | 0 | 4 | 5 | 2 | 0 | 2 | 0 | 1 | 0 | 21 | 3 | 3 | 0 | 73 | |
| 7:35 AM | 0 | 4 | 19 | 0 | 2 | 8 | 0 | 0 | 1 | 1 | 2 | 0 | 12 | 0 | 1 | 0 | 50 | |
| 7:40 AM | 0 | 3 | 32 | 0 | 8 | 14 | 0 | 0 | 0 | 0 | 0 | 0 | 20 | 1 | 3 | 0 | 81 | |
| 7:45 AM | 1 | 4 | 26 | 0 | 6 | 9 | 0 | 0 | 0 | 1 | 1 | 0 | 10 | 4 | 2 | 0 | 64 | |
| 7:50 AM | 4 | 15 | 21 | 0 | 6 | 16 | 1 | 0 | 0 | 1 | 1 | 0 | 17 | 2 | 2 | 0 | 86 | |
| 7:55 AM | 3 | 7 | 30 | 0 | 4 | 16 | 0 | 0 | 1 | 2 | 1 | 0 | 27 | 5 | 4 | 0 | 100 | 788 |
| 8:00 AM | 3 | 7 | 22 | 0 | 2 | 12 | 1 | 0 | 1 | 0 | 2 | 0 | 18 | 4 | 0 | 0 | 72 | 809 |
| 8:05 AM | 4 | 7 | 27 | 0 | 3 | 11 | 0 | 0 | 0 | 1 | 0 | 0 | 19 | 0 | 4 | 0 | 76 | 835 |
| 8:10 AM | 0 | 16 | 26 | 0 | 1 | 7 | 0 | 0 | 0 | 2 | 0 | 0 | 19 | 2 | 1 | 0 | 74 | 850 |
| 8:15 AM | 0 | 3 | 31 | 0 | 3 | 9 | 1 | 0 | 0 | 0 | 2 | 0 | 21 | 1 | 1 | 0 | 72 | 866 |
| 8:20 AM | 0 | 5 | 22 | 0 | 3 | 5 | 0 | 0 | 0 | 0 | 0 | 0 | 19 | 0 | 1 | 0 | 55 | 867 |
| 8:25 AM | 0 | 5 | 28 | 0 | 2 | 5 | 0 | 0 | 0 | 1 | 1 | 0 | 13 | 0 | 2 | 0 | 57 | 860 |
| 8:30 AM | 1 | 8 | 27 | 0 | 1 | 3 | 1 | 0 | 1 | 1 | 0 | 0 | 25 | 1 | 2 | 0 | 71 | 858 |
| 8:35 AM | 1 | 7 | 20 | 0 | 3 | 4 | 0 | 0 | 0 | 1 | 1 | 0 | 22 | 3 | 0 | 0 | 62 | 870 |
| 8:40 AM | 1 | 6 | 27 | 0 | 1 | 7 | 0 | 0 | 0 | 2 | 0 | 0 | 22 | 3 | 2 | 0 | 71 | 860 |
| 8:45 AM | 2 | 2 | 23 | 0 | 3 | 4 | 1 | 0 | 0 | 2 | 2 | 0 | 20 | 3 | 0 | 0 | 62 | 858 |
| 8:50 AM | 3 | 3 | 29 | 0 | 1 | 3 | 0 | 0 | 0 | 0 | 0 | 0 | 30 | 2 | 3 | 0 | 74 | 846 |
| 8:55 AM | 2 | 5 | 19 | 0 | 1 | 4 | 0 | 0 | 0 | 1 | 2 | 0 | 17 | 2 | 0 | 0 | 53 | 799 |
| Peak 15-Min Flowrates | Northbound | | | | Southbound | | | | Eastbound | | | | Westbound | | | | Total | |
| | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | | |
| All Vehicles | 40 | 116 | 292 | 0 | 48 | 176 | 8 | 0 | 8 | 12 | 16 | 0 | 248 | 44 | 24 | 0 | 1032 | |
| Heavy Trucks | 4 | 4 | 12 | | 0 | 0 | 0 | | 0 | 0 | 0 | | 20 | 8 | 0 | | 48 | |
| Buses | | | | | | | | | | | | | | | | | | |
| Pedestrians | | 4 | | | | 0 | | | | 0 | | | | 0 | | | 4 | |
| Bicycles | 0 | 0 | 0 | | 0 | 0 | 0 | | 0 | 0 | 0 | | 0 | 0 | 0 | | 0 | |
| Scooters | | | | | | | | | | | | | | | | | | |

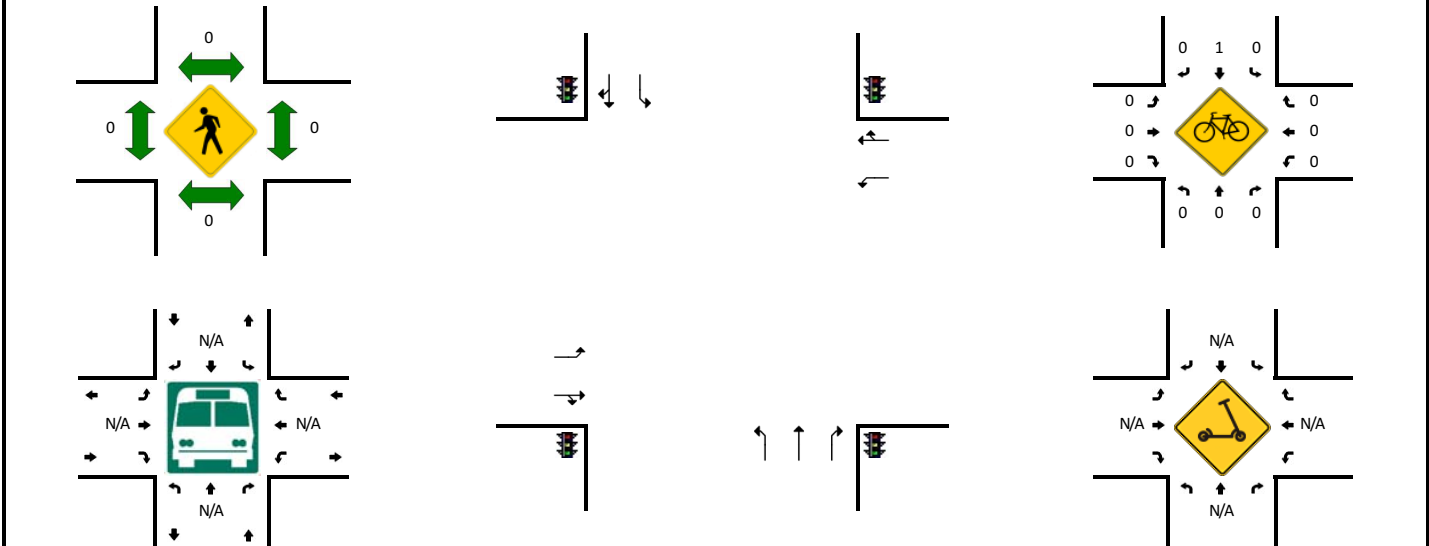
Comments:

LOCATION: NW 11th St -- W Elm Ave

CITY/STATE: Hermiston, OR

QC JOB #: 15570904

DATE: Wed, Oct 6 2021

Peak-Hour: 4:15 PM -- 5:15 PM
Peak 15-Min: 4:55 PM -- 5:10 PM

| 5-Min Count Period Beginning At | NW 11th St (Northbound) | | | | NW 11th St (Southbound) | | | | W Elm Ave (Eastbound) | | | | W Elm Ave (Westbound) | | | | Total | Hourly Totals |
|---------------------------------|-------------------------|------|-------|---|-------------------------|------|-------|---|-----------------------|------|-------|---|-----------------------|------|-------|---|-------|---------------|
| | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | | |
| 4:00 PM | 2 | 7 | 38 | 0 | 5 | 7 | 0 | 0 | 1 | 1 | 2 | 0 | 24 | 1 | 6 | 0 | 94 | |
| 4:05 PM | 2 | 10 | 37 | 0 | 6 | 16 | 0 | 0 | 1 | 1 | 1 | 0 | 20 | 2 | 2 | 0 | 98 | |
| 4:10 PM | 1 | 9 | 36 | 0 | 1 | 8 | 1 | 0 | 0 | 2 | 1 | 0 | 23 | 3 | 2 | 0 | 87 | |
| 4:15 PM | 3 | 8 | 31 | 0 | 0 | 10 | 0 | 0 | 0 | 2 | 0 | 0 | 35 | 3 | 0 | 0 | 92 | |
| 4:20 PM | 2 | 10 | 30 | 0 | 2 | 6 | 0 | 0 | 0 | 1 | 1 | 0 | 27 | 1 | 2 | 0 | 82 | |
| 4:25 PM | 1 | 14 | 32 | 0 | 0 | 6 | 0 | 0 | 1 | 2 | 0 | 0 | 31 | 1 | 0 | 0 | 88 | |
| 4:30 PM | 3 | 13 | 39 | 0 | 5 | 15 | 1 | 0 | 0 | 2 | 0 | 0 | 34 | 3 | 1 | 0 | 116 | |
| 4:35 PM | 2 | 15 | 20 | 0 | 1 | 8 | 0 | 0 | 0 | 3 | 1 | 0 | 26 | 1 | 4 | 0 | 81 | |
| 4:40 PM | 2 | 7 | 19 | 0 | 1 | 10 | 0 | 0 | 1 | 1 | 2 | 0 | 33 | 2 | 4 | 0 | 82 | |
| 4:45 PM | 0 | 11 | 43 | 0 | 2 | 19 | 0 | 0 | 1 | 1 | 2 | 0 | 25 | 0 | 0 | 0 | 104 | |
| 4:50 PM | 2 | 5 | 26 | 0 | 3 | 2 | 0 | 0 | 1 | 1 | 2 | 0 | 25 | 1 | 0 | 0 | 68 | |
| 4:55 PM | 1 | 12 | 33 | 0 | 4 | 11 | 0 | 0 | 0 | 2 | 0 | 0 | 32 | 1 | 3 | 0 | 99 | 1091 |
| 5:00 PM | 0 | 16 | 30 | 0 | 1 | 8 | 0 | 0 | 1 | 2 | 1 | 0 | 34 | 1 | 2 | 0 | 96 | 1093 |
| 5:05 PM | 1 | 9 | 40 | 0 | 0 | 10 | 0 | 0 | 0 | 3 | 2 | 0 | 35 | 0 | 2 | 0 | 102 | 1097 |
| 5:10 PM | 1 | 10 | 29 | 0 | 3 | 8 | 1 | 0 | 1 | 3 | 2 | 0 | 33 | 1 | 2 | 0 | 94 | 1104 |
| 5:15 PM | 0 | 5 | 27 | 0 | 3 | 10 | 0 | 0 | 1 | 1 | 0 | 0 | 37 | 0 | 3 | 0 | 87 | 1099 |
| 5:20 PM | 1 | 3 | 34 | 0 | 0 | 8 | 1 | 0 | 0 | 0 | 0 | 0 | 29 | 2 | 3 | 0 | 81 | 1098 |
| 5:25 PM | 0 | 11 | 26 | 0 | 4 | 14 | 0 | 0 | 0 | 0 | 0 | 0 | 22 | 0 | 2 | 0 | 79 | 1089 |
| 5:30 PM | 2 | 12 | 26 | 0 | 3 | 9 | 1 | 0 | 0 | 0 | 1 | 0 | 27 | 4 | 4 | 0 | 89 | 1062 |
| 5:35 PM | 2 | 11 | 24 | 0 | 2 | 13 | 0 | 0 | 4 | 2 | 0 | 0 | 24 | 1 | 3 | 0 | 86 | 1067 |
| 5:40 PM | 2 | 8 | 22 | 0 | 0 | 5 | 0 | 0 | 0 | 1 | 0 | 0 | 24 | 0 | 3 | 0 | 65 | 1050 |
| 5:45 PM | 0 | 8 | 22 | 0 | 1 | 6 | 1 | 0 | 0 | 1 | 1 | 0 | 18 | 1 | 2 | 0 | 61 | 1007 |
| 5:50 PM | 1 | 5 | 25 | 0 | 2 | 9 | 0 | 0 | 0 | 0 | 0 | 0 | 30 | 1 | 2 | 0 | 75 | 1014 |
| 5:55 PM | 1 | 9 | 27 | 0 | 0 | 9 | 0 | 0 | 1 | 0 | 1 | 0 | 28 | 1 | 2 | 0 | 79 | 994 |
| Peak 15-Min Flowrates | Northbound | | | | Southbound | | | | Eastbound | | | | Westbound | | | | Total | |
| | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | | |
| All Vehicles | 8 | 148 | 412 | 0 | 20 | 116 | 0 | 0 | 4 | 28 | 12 | 0 | 404 | 8 | 28 | 0 | 1188 | |
| Heavy Trucks | 0 | 4 | 16 | | 0 | 0 | 0 | | 0 | 0 | 0 | | 0 | 0 | 0 | | 20 | |
| Buses | | | | | | | | | | | | | | | | | | |
| Pedestrians | | 0 | | | | 0 | | | | 0 | | | | 0 | | | 0 | |
| Bicycles | 0 | 0 | 0 | | 0 | 0 | 0 | | 0 | 0 | 0 | | 0 | 0 | 0 | | 0 | |
| Scooters | | | | | | | | | | | | | | | | | | |

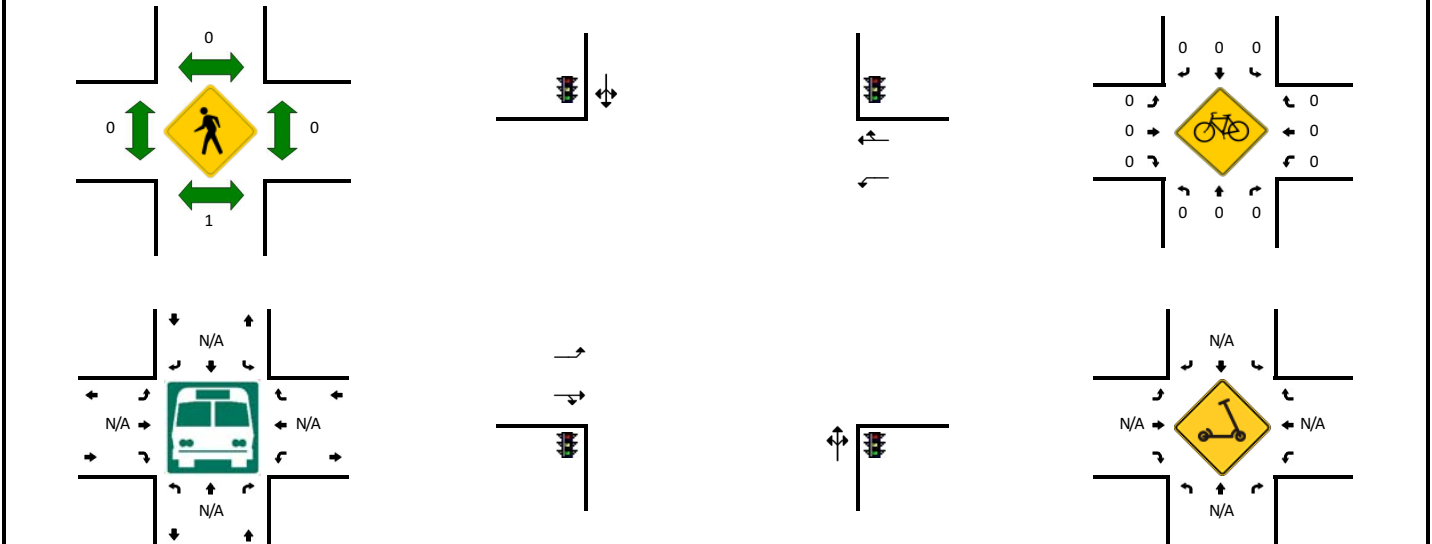
Comments:

LOCATION: N 1st Pl -- Hermiston Hwy

CITY/STATE: Hermiston, OR

QC JOB #: 15570905

DATE: Thu, Oct 7 2021

Peak-Hour: 7:30 AM -- 8:30 AM
Peak 15-Min: 7:50 AM -- 8:05 AM

| 5-Min Count Period Beginning At | N 1st Pl (Northbound) | | | | N 1st Pl (Southbound) | | | | Hermiston Hwy (Eastbound) | | | | Hermiston Hwy (Westbound) | | | | Total | Hourly Totals |
|---------------------------------|-----------------------|------|-------|---|-----------------------|------|-------|---|---------------------------|------|-------|---|---------------------------|------|-------|---|-------|---------------|
| | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | | |
| 7:00 AM | 2 | 3 | 3 | 0 | 2 | 6 | 8 | 0 | 5 | 14 | 1 | 0 | 1 | 21 | 2 | 0 | 68 | |
| 7:05 AM | 2 | 2 | 3 | 0 | 2 | 5 | 9 | 0 | 0 | 15 | 0 | 0 | 1 | 23 | 2 | 0 | 64 | |
| 7:10 AM | 3 | 6 | 4 | 0 | 3 | 7 | 9 | 0 | 1 | 22 | 2 | 0 | 0 | 22 | 2 | 0 | 81 | |
| 7:15 AM | 1 | 5 | 1 | 0 | 3 | 11 | 5 | 0 | 4 | 22 | 0 | 0 | 1 | 21 | 3 | 0 | 77 | |
| 7:20 AM | 0 | 2 | 5 | 0 | 2 | 4 | 5 | 0 | 3 | 22 | 1 | 0 | 3 | 19 | 2 | 0 | 68 | |
| 7:25 AM | 0 | 6 | 3 | 0 | 2 | 14 | 2 | 0 | 4 | 20 | 1 | 0 | 0 | 31 | 1 | 0 | 84 | |
| 7:30 AM | 3 | 2 | 3 | 0 | 7 | 11 | 2 | 0 | 6 | 24 | 1 | 0 | 5 | 34 | 0 | 0 | 98 | |
| 7:35 AM | 0 | 5 | 5 | 0 | 1 | 14 | 2 | 0 | 3 | 30 | 1 | 0 | 3 | 23 | 2 | 0 | 89 | |
| 7:40 AM | 2 | 7 | 9 | 0 | 3 | 17 | 8 | 0 | 7 | 16 | 3 | 0 | 5 | 26 | 2 | 0 | 105 | |
| 7:45 AM | 2 | 8 | 1 | 0 | 7 | 13 | 8 | 0 | 13 | 32 | 1 | 0 | 3 | 26 | 3 | 0 | 117 | |
| 7:50 AM | 3 | 13 | 6 | 0 | 3 | 13 | 6 | 0 | 12 | 26 | 3 | 0 | 4 | 33 | 4 | 0 | 126 | |
| 7:55 AM | 2 | 4 | 2 | 0 | 4 | 12 | 10 | 0 | 7 | 34 | 3 | 0 | 4 | 33 | 3 | 0 | 118 | 1095 |
| 8:00 AM | 2 | 13 | 4 | 0 | 4 | 12 | 5 | 0 | 8 | 32 | 1 | 0 | 3 | 30 | 4 | 0 | 118 | 1145 |
| 8:05 AM | 3 | 5 | 2 | 0 | 4 | 8 | 2 | 0 | 10 | 22 | 0 | 0 | 2 | 32 | 4 | 0 | 94 | 1175 |
| 8:10 AM | 3 | 8 | 2 | 0 | 0 | 8 | 6 | 0 | 7 | 18 | 1 | 0 | 3 | 21 | 4 | 0 | 81 | 1175 |
| 8:15 AM | 1 | 7 | 4 | 0 | 5 | 7 | 8 | 0 | 8 | 26 | 0 | 0 | 1 | 20 | 2 | 0 | 89 | 1187 |
| 8:20 AM | 1 | 8 | 2 | 0 | 4 | 9 | 7 | 0 | 4 | 26 | 1 | 0 | 1 | 25 | 3 | 0 | 91 | 1210 |
| 8:25 AM | 5 | 10 | 0 | 0 | 4 | 6 | 7 | 0 | 5 | 26 | 1 | 0 | 2 | 23 | 0 | 0 | 89 | 1215 |
| 8:30 AM | 2 | 7 | 4 | 0 | 2 | 3 | 3 | 0 | 13 | 22 | 2 | 0 | 2 | 25 | 0 | 0 | 85 | 1202 |
| 8:35 AM | 4 | 7 | 4 | 0 | 3 | 12 | 7 | 0 | 3 | 21 | 0 | 0 | 3 | 24 | 1 | 0 | 89 | 1202 |
| 8:40 AM | 2 | 5 | 4 | 0 | 4 | 8 | 7 | 0 | 8 | 27 | 0 | 0 | 1 | 32 | 1 | 0 | 99 | 1196 |
| 8:45 AM | 1 | 3 | 4 | 0 | 5 | 7 | 2 | 0 | 6 | 24 | 2 | 0 | 5 | 31 | 2 | 0 | 92 | 1171 |
| 8:50 AM | 3 | 8 | 6 | 0 | 2 | 6 | 10 | 0 | 11 | 25 | 1 | 0 | 2 | 23 | 1 | 0 | 98 | 1143 |
| 8:55 AM | 0 | 8 | 1 | 0 | 1 | 8 | 4 | 0 | 6 | 18 | 1 | 0 | 4 | 16 | 2 | 0 | 69 | 1094 |
| Peak 15-Min Flowrates | Northbound | | | | Southbound | | | | Eastbound | | | | Westbound | | | | Total | |
| | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | | |
| All Vehicles | 28 | 120 | 48 | 0 | 44 | 148 | 84 | 0 | 108 | 368 | 28 | 0 | 44 | 384 | 44 | 0 | 1448 | |
| Heavy Trucks | 4 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 16 | 0 | 0 | 4 | 28 | 0 | 0 | 56 | |
| Buses | | | | | | | | | | | | | | | | | | |
| Pedestrians | | 0 | | | | 0 | | | | 0 | | | | 0 | | | 0 | |
| Bicycles | 0 | 0 | 0 | | 0 | 0 | 0 | | 0 | 0 | 0 | | 0 | 0 | 0 | | 0 | |
| Scooters | | | | | | | | | | | | | | | | | | |

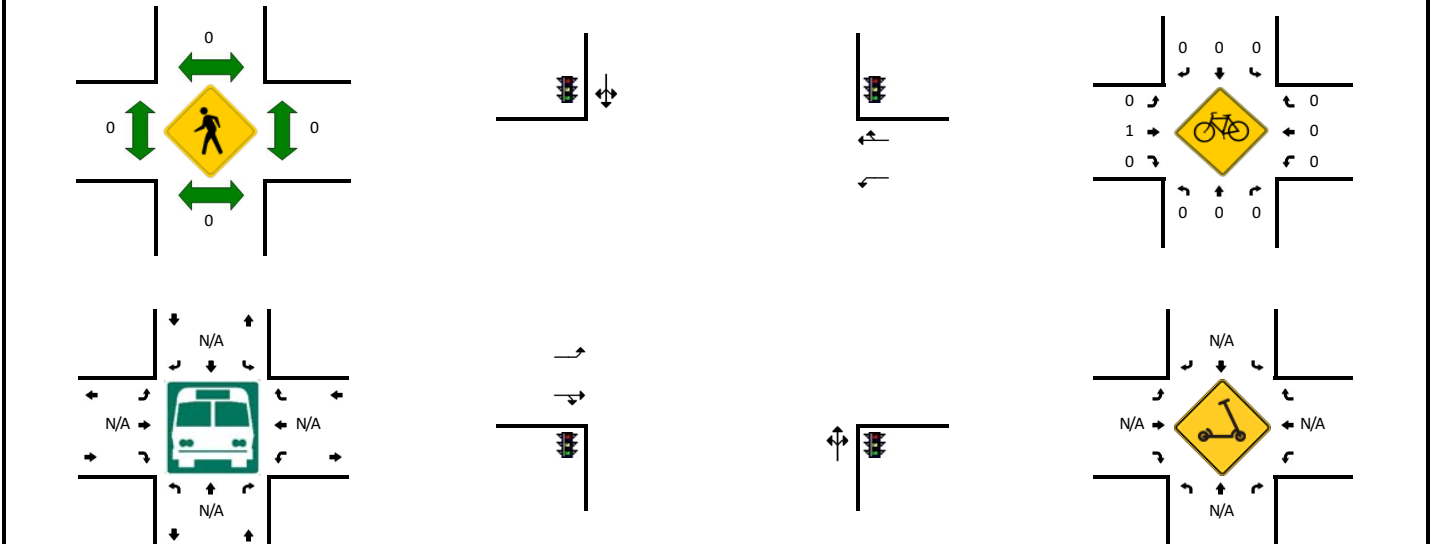
Comments:

LOCATION: N 1st Pl -- Hermiston Hwy

CITY/STATE: Hermiston, OR

QC JOB #: 15570906

DATE: Wed, Oct 6 2021

Peak-Hour: 4:25 PM -- 5:25 PM
Peak 15-Min: 4:25 PM -- 4:40 PM

| 5-Min Count Period Beginning At | N 1st Pl (Northbound) | | | | N 1st Pl (Southbound) | | | | Hermiston Hwy (Eastbound) | | | | Hermiston Hwy (Westbound) | | | | Total | Hourly Totals |
|---------------------------------|-----------------------|------|-------|---|-----------------------|------|-------|---|---------------------------|------|-------|---|---------------------------|------|-------|---|-------|---------------|
| | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | | |
| 4:00 PM | 1 | 18 | 7 | 0 | 3 | 10 | 10 | 0 | 10 | 28 | 6 | 0 | 1 | 18 | 2 | 0 | 114 | |
| 4:05 PM | 2 | 16 | 5 | 0 | 3 | 12 | 5 | 0 | 14 | 44 | 4 | 0 | 2 | 27 | 5 | 0 | 139 | |
| 4:10 PM | 2 | 11 | 8 | 0 | 5 | 12 | 7 | 0 | 7 | 40 | 1 | 0 | 6 | 30 | 5 | 0 | 134 | |
| 4:15 PM | 0 | 15 | 3 | 0 | 6 | 12 | 6 | 0 | 6 | 34 | 3 | 0 | 6 | 30 | 4 | 0 | 125 | |
| 4:20 PM | 6 | 11 | 4 | 0 | 3 | 12 | 12 | 0 | 7 | 22 | 3 | 0 | 3 | 24 | 6 | 0 | 113 | |
| 4:25 PM | 2 | 14 | 8 | 0 | 3 | 11 | 10 | 0 | 14 | 46 | 5 | 0 | 4 | 27 | 4 | 0 | 148 | |
| 4:30 PM | 4 | 15 | 10 | 0 | 1 | 7 | 8 | 0 | 10 | 38 | 4 | 0 | 2 | 29 | 6 | 0 | 134 | |
| 4:35 PM | 3 | 12 | 4 | 0 | 1 | 11 | 4 | 0 | 11 | 38 | 3 | 0 | 3 | 33 | 3 | 0 | 126 | |
| 4:40 PM | 4 | 11 | 4 | 0 | 3 | 8 | 9 | 0 | 10 | 33 | 3 | 0 | 5 | 26 | 5 | 0 | 121 | |
| 4:45 PM | 0 | 9 | 2 | 0 | 3 | 11 | 4 | 0 | 9 | 46 | 3 | 0 | 2 | 31 | 9 | 0 | 129 | |
| 4:50 PM | 0 | 20 | 5 | 0 | 5 | 16 | 17 | 0 | 12 | 29 | 0 | 0 | 1 | 28 | 3 | 0 | 136 | |
| 4:55 PM | 0 | 8 | 4 | 0 | 3 | 10 | 6 | 0 | 13 | 31 | 3 | 0 | 4 | 33 | 3 | 0 | 118 | 1537 |
| 5:00 PM | 1 | 6 | 6 | 0 | 2 | 15 | 8 | 0 | 13 | 33 | 3 | 0 | 0 | 29 | 2 | 0 | 118 | 1541 |
| 5:05 PM | 2 | 20 | 7 | 0 | 6 | 14 | 11 | 0 | 18 | 35 | 2 | 0 | 2 | 22 | 5 | 0 | 144 | 1546 |
| 5:10 PM | 0 | 14 | 3 | 0 | 1 | 15 | 10 | 0 | 7 | 47 | 2 | 0 | 3 | 30 | 5 | 0 | 137 | 1549 |
| 5:15 PM | 1 | 12 | 3 | 0 | 5 | 13 | 15 | 0 | 10 | 31 | 0 | 0 | 1 | 28 | 2 | 0 | 121 | 1545 |
| 5:20 PM | 2 | 11 | 4 | 0 | 2 | 12 | 4 | 0 | 9 | 31 | 5 | 0 | 6 | 32 | 6 | 0 | 124 | 1556 |
| 5:25 PM | 2 | 14 | 2 | 0 | 3 | 12 | 9 | 0 | 11 | 31 | 2 | 0 | 4 | 32 | 6 | 0 | 128 | 1536 |
| 5:30 PM | 0 | 13 | 1 | 0 | 6 | 7 | 10 | 0 | 12 | 35 | 4 | 0 | 7 | 22 | 4 | 0 | 121 | 1523 |
| 5:35 PM | 1 | 6 | 8 | 0 | 0 | 11 | 6 | 0 | 13 | 30 | 2 | 0 | 1 | 24 | 6 | 0 | 108 | 1505 |
| 5:40 PM | 0 | 14 | 5 | 0 | 5 | 6 | 4 | 0 | 9 | 26 | 1 | 0 | 3 | 24 | 2 | 0 | 99 | 1483 |
| 5:45 PM | 0 | 13 | 5 | 0 | 1 | 11 | 7 | 0 | 8 | 25 | 5 | 0 | 2 | 26 | 5 | 0 | 108 | 1462 |
| 5:50 PM | 0 | 10 | 3 | 0 | 2 | 11 | 16 | 0 | 9 | 25 | 2 | 0 | 4 | 26 | 6 | 0 | 114 | 1440 |
| 5:55 PM | 1 | 13 | 4 | 0 | 3 | 13 | 11 | 0 | 8 | 24 | 0 | 0 | 4 | 25 | 7 | 0 | 113 | 1435 |
| Peak 15-Min Flowrates | Northbound | | | | Southbound | | | | Eastbound | | | | Westbound | | | | Total | |
| | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | | |
| All Vehicles | 36 | 164 | 88 | 0 | 20 | 116 | 88 | 0 | 140 | 488 | 48 | 0 | 36 | 356 | 52 | 0 | 1632 | |
| Heavy Trucks | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 16 | 0 | 0 | 0 | 40 | 0 | 0 | 60 | |
| Buses | | | | | | | | | | | | | | | | | | |
| Pedestrians | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Bicycles | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Scooters | | | | | | | | | | | | | | | | | | |

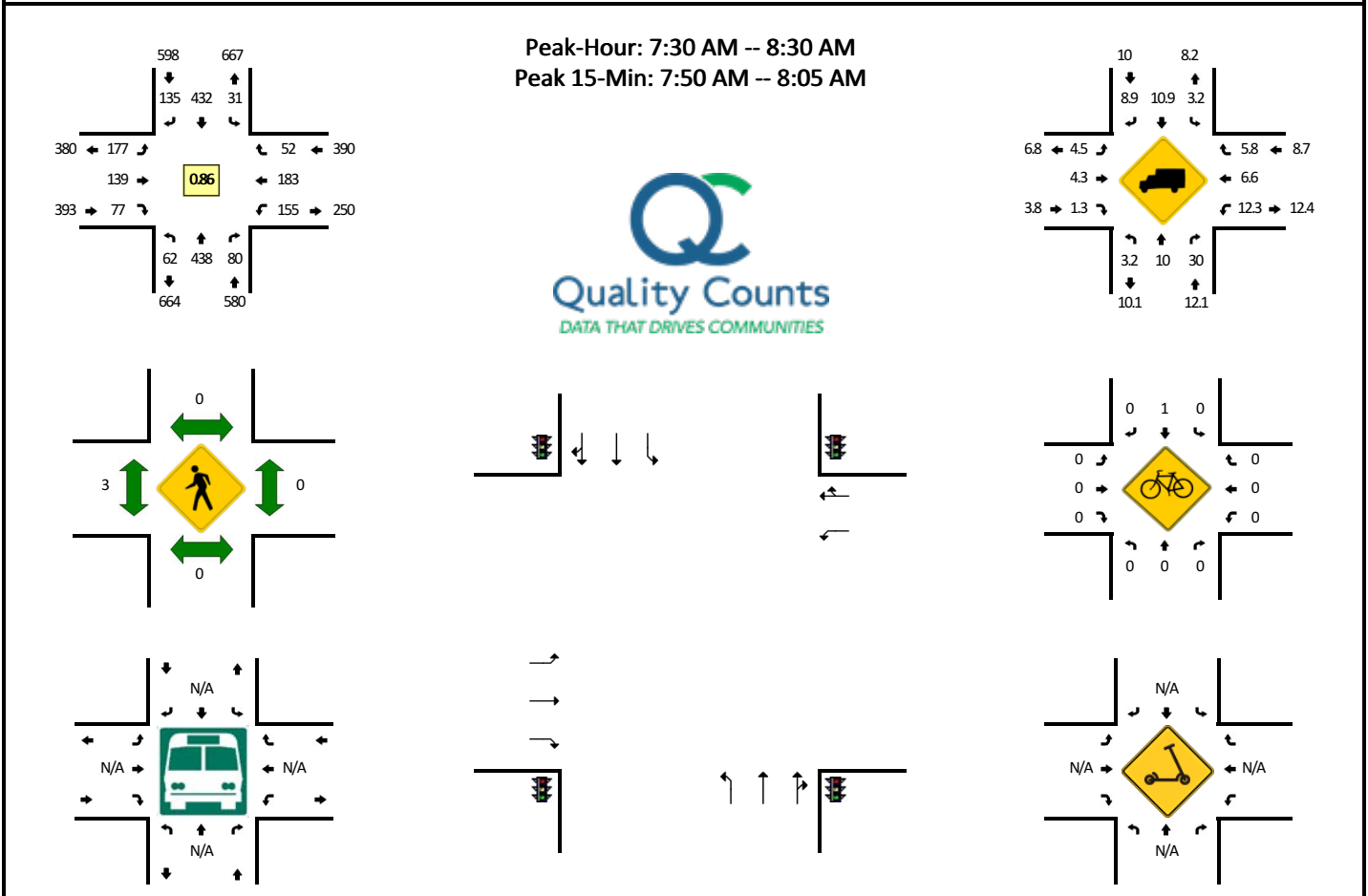
Comments:

LOCATION: Hwy 395 -- Hermiston Hwy

CITY/STATE: Hermiston, OR

QC JOB #: 15570907

DATE: Thu, Oct 7 2021

Peak-Hour: 7:30 AM -- 8:30 AM
Peak 15-Min: 7:50 AM -- 8:05 AM

| 5-Min Count Period Beginning At | Hwy 395 (Northbound) | | | | Hwy 395 (Southbound) | | | | Hermiston Hwy (Eastbound) | | | | Hermiston Hwy (Westbound) | | | | Total | Hourly Totals |
|---------------------------------|----------------------|------|-------|---|----------------------|------|-------|---|---------------------------|------|-------|---|---------------------------|------|-------|---|-------|---------------|
| | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | | |
| 7:00 AM | 1 | 18 | 1 | 0 | 2 | 22 | 4 | 0 | 11 | 3 | 2 | 0 | 4 | 14 | 1 | 0 | 83 | |
| 7:05 AM | 3 | 25 | 1 | 0 | 4 | 31 | 8 | 0 | 10 | 14 | 3 | 0 | 8 | 21 | 5 | 0 | 133 | |
| 7:10 AM | 4 | 25 | 5 | 0 | 0 | 22 | 9 | 0 | 10 | 12 | 4 | 0 | 7 | 15 | 5 | 0 | 118 | |
| 7:15 AM | 3 | 25 | 3 | 0 | 1 | 26 | 8 | 0 | 12 | 10 | 0 | 0 | 10 | 13 | 3 | 0 | 114 | |
| 7:20 AM | 6 | 25 | 7 | 0 | 2 | 20 | 5 | 0 | 12 | 13 | 4 | 0 | 11 | 17 | 7 | 0 | 129 | |
| 7:25 AM | 2 | 27 | 5 | 0 | 2 | 24 | 13 | 0 | 8 | 4 | 2 | 0 | 7 | 19 | 4 | 0 | 117 | |
| 7:30 AM | 12 | 30 | 8 | 0 | 3 | 20 | 15 | 0 | 23 | 7 | 4 | 0 | 19 | 10 | 3 | 0 | 154 | |
| 7:35 AM | 2 | 29 | 9 | 0 | 6 | 42 | 12 | 0 | 9 | 13 | 5 | 0 | 11 | 7 | 2 | 0 | 147 | |
| 7:40 AM | 5 | 47 | 4 | 0 | 2 | 34 | 9 | 0 | 14 | 16 | 2 | 0 | 12 | 22 | 2 | 0 | 169 | |
| 7:45 AM | 6 | 36 | 6 | 0 | 2 | 33 | 11 | 0 | 15 | 11 | 13 | 0 | 7 | 20 | 5 | 0 | 165 | |
| 7:50 AM | 10 | 39 | 9 | 0 | 3 | 41 | 9 | 0 | 14 | 12 | 7 | 0 | 11 | 19 | 2 | 0 | 176 | |
| 7:55 AM | 2 | 44 | 10 | 0 | 1 | 51 | 14 | 0 | 19 | 10 | 5 | 0 | 14 | 19 | 5 | 0 | 194 | 1699 |
| 8:00 AM | 4 | 56 | 5 | 0 | 0 | 44 | 13 | 0 | 22 | 12 | 8 | 0 | 13 | 17 | 9 | 0 | 203 | 1819 |
| 8:05 AM | 6 | 23 | 5 | 0 | 2 | 37 | 13 | 0 | 15 | 12 | 6 | 0 | 19 | 19 | 5 | 0 | 162 | 1848 |
| 8:10 AM | 2 | 45 | 9 | 0 | 2 | 30 | 12 | 0 | 12 | 11 | 6 | 0 | 9 | 15 | 4 | 0 | 157 | 1887 |
| 8:15 AM | 2 | 33 | 8 | 0 | 4 | 35 | 9 | 0 | 8 | 12 | 4 | 0 | 12 | 15 | 3 | 0 | 145 | 1918 |
| 8:20 AM | 8 | 32 | 2 | 0 | 2 | 34 | 9 | 0 | 17 | 12 | 10 | 0 | 11 | 11 | 7 | 0 | 155 | 1944 |
| 8:25 AM | 3 | 24 | 5 | 0 | 4 | 31 | 9 | 0 | 9 | 11 | 7 | 0 | 17 | 9 | 5 | 0 | 134 | 1961 |
| 8:30 AM | 10 | 46 | 3 | 0 | 5 | 22 | 5 | 0 | 8 | 13 | 8 | 0 | 6 | 17 | 3 | 0 | 146 | 1953 |
| 8:35 AM | 0 | 27 | 3 | 0 | 1 | 29 | 12 | 0 | 10 | 19 | 2 | 0 | 7 | 17 | 4 | 0 | 131 | 1937 |
| 8:40 AM | 7 | 39 | 4 | 0 | 2 | 31 | 13 | 0 | 7 | 13 | 12 | 0 | 9 | 11 | 8 | 0 | 156 | 1924 |
| 8:45 AM | 5 | 43 | 10 | 0 | 5 | 37 | 17 | 0 | 7 | 16 | 4 | 0 | 7 | 16 | 4 | 0 | 171 | 1930 |
| 8:50 AM | 8 | 30 | 8 | 0 | 3 | 26 | 11 | 0 | 16 | 16 | 7 | 0 | 11 | 11 | 5 | 0 | 152 | 1906 |
| 8:55 AM | 4 | 30 | 5 | 0 | 4 | 32 | 9 | 0 | 15 | 9 | 4 | 0 | 8 | 11 | 4 | 0 | 135 | 1847 |
| Peak 15-Min Flowrates | Northbound | | | | Southbound | | | | Eastbound | | | | Westbound | | | | Total | |
| | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | | |
| All Vehicles | 64 | 556 | 96 | 0 | 16 | 544 | 144 | 0 | 220 | 136 | 80 | 0 | 152 | 220 | 64 | 0 | 2292 | |
| Heavy Trucks | 4 | 44 | 28 | | 0 | 28 | 16 | | 12 | 0 | 0 | | 0 | 8 | 0 | | 140 | |
| Buses | | | | | | | | | | | | | | | | | | |
| Pedestrians | | 0 | | | | 0 | | | | 4 | | | | 0 | | | 4 | |
| Bicycles | 0 | 0 | 0 | | 0 | 4 | 0 | | 0 | 0 | 0 | | 0 | 0 | 0 | | 4 | |
| Scooters | | | | | | | | | | | | | | | | | | |

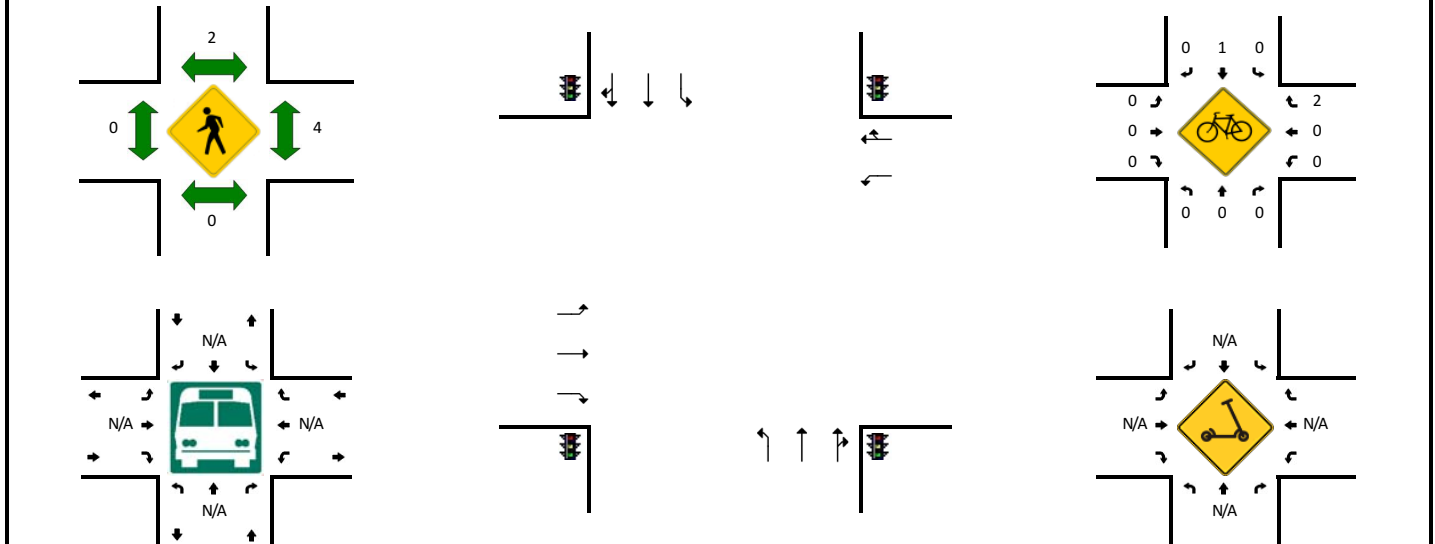
Comments:

LOCATION: Hwy 395 -- Hermiston Hwy

CITY/STATE: Hermiston, OR

QC JOB #: 15570908

DATE: Wed, Oct 6 2021

Peak-Hour: 4:25 PM -- 5:25 PM
Peak 15-Min: 5:00 PM -- 5:15 PM

| 5-Min Count Period Beginning At | Hwy 395 (Northbound) | | | | Hwy 395 (Southbound) | | | | Hermiston Hwy (Eastbound) | | | | Hermiston Hwy (Westbound) | | | | Total | Hourly Totals |
|---------------------------------|----------------------|------|-------|---|----------------------|------|-------|---|---------------------------|------|-------|---|---------------------------|------|-------|---|-------|---------------|
| | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | | |
| 4:00 PM | 7 | 42 | 6 | 0 | 5 | 41 | 6 | 0 | 14 | 14 | 5 | 0 | 16 | 14 | 3 | 0 | 173 | |
| 4:05 PM | 5 | 57 | 7 | 0 | 4 | 47 | 7 | 0 | 18 | 21 | 10 | 0 | 11 | 19 | 5 | 0 | 211 | |
| 4:10 PM | 5 | 66 | 6 | 0 | 1 | 46 | 13 | 0 | 22 | 27 | 6 | 0 | 9 | 10 | 4 | 0 | 215 | |
| 4:15 PM | 12 | 55 | 10 | 0 | 6 | 53 | 14 | 0 | 15 | 22 | 4 | 0 | 6 | 20 | 3 | 0 | 220 | |
| 4:20 PM | 2 | 39 | 6 | 0 | 5 | 69 | 9 | 0 | 10 | 20 | 6 | 0 | 13 | 20 | 8 | 0 | 207 | |
| 4:25 PM | 4 | 61 | 10 | 0 | 6 | 62 | 15 | 0 | 15 | 19 | 8 | 0 | 12 | 9 | 3 | 0 | 224 | |
| 4:30 PM | 4 | 37 | 8 | 0 | 3 | 51 | 16 | 0 | 15 | 31 | 11 | 0 | 7 | 23 | 7 | 0 | 213 | |
| 4:35 PM | 3 | 61 | 7 | 0 | 2 | 43 | 21 | 0 | 19 | 18 | 13 | 0 | 15 | 13 | 7 | 0 | 222 | |
| 4:40 PM | 7 | 52 | 7 | 0 | 3 | 47 | 7 | 0 | 20 | 13 | 6 | 0 | 12 | 20 | 3 | 0 | 197 | |
| 4:45 PM | 5 | 69 | 12 | 0 | 4 | 53 | 9 | 0 | 17 | 25 | 7 | 0 | 7 | 17 | 10 | 0 | 235 | |
| 4:50 PM | 4 | 54 | 11 | 0 | 6 | 41 | 14 | 0 | 17 | 20 | 9 | 0 | 19 | 17 | 7 | 0 | 219 | |
| 4:55 PM | 7 | 59 | 14 | 0 | 10 | 57 | 17 | 0 | 12 | 9 | 5 | 0 | 11 | 14 | 3 | 0 | 218 | 2554 |
| 5:00 PM | 4 | 58 | 5 | 0 | 5 | 54 | 14 | 0 | 17 | 20 | 8 | 0 | 12 | 17 | 7 | 0 | 221 | 2602 |
| 5:05 PM | 4 | 51 | 7 | 0 | 9 | 62 | 10 | 0 | 20 | 10 | 7 | 0 | 20 | 13 | 9 | 0 | 222 | 2613 |
| 5:10 PM | 2 | 58 | 10 | 0 | 11 | 60 | 13 | 0 | 24 | 27 | 7 | 0 | 14 | 14 | 3 | 0 | 243 | 2641 |
| 5:15 PM | 3 | 50 | 7 | 0 | 7 | 56 | 17 | 0 | 18 | 19 | 5 | 0 | 14 | 13 | 4 | 0 | 213 | 2634 |
| 5:20 PM | 5 | 50 | 10 | 0 | 2 | 57 | 9 | 0 | 17 | 22 | 3 | 0 | 19 | 21 | 3 | 0 | 218 | 2645 |
| 5:25 PM | 4 | 51 | 9 | 0 | 6 | 49 | 11 | 0 | 13 | 20 | 4 | 0 | 20 | 24 | 2 | 0 | 213 | 2634 |
| 5:30 PM | 6 | 49 | 6 | 0 | 2 | 34 | 10 | 0 | 18 | 16 | 10 | 0 | 16 | 16 | 10 | 0 | 193 | 2614 |
| 5:35 PM | 5 | 46 | 3 | 0 | 11 | 63 | 7 | 0 | 12 | 21 | 4 | 0 | 11 | 15 | 0 | 0 | 198 | 2590 |
| 5:40 PM | 6 | 54 | 16 | 0 | 5 | 53 | 13 | 0 | 10 | 14 | 8 | 0 | 11 | 9 | 3 | 0 | 202 | 2595 |
| 5:45 PM | 3 | 45 | 7 | 0 | 9 | 46 | 9 | 0 | 13 | 11 | 6 | 0 | 11 | 20 | 9 | 0 | 189 | 2549 |
| 5:50 PM | 5 | 59 | 10 | 0 | 9 | 44 | 8 | 0 | 12 | 20 | 6 | 0 | 10 | 21 | 10 | 0 | 214 | 2544 |
| 5:55 PM | 7 | 51 | 3 | 0 | 3 | 43 | 10 | 0 | 15 | 16 | 3 | 0 | 7 | 17 | 8 | 0 | 183 | 2509 |
| Peak 15-Min Flowrates | Northbound | | | | Southbound | | | | Eastbound | | | | Westbound | | | | Total | |
| | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | | |
| All Vehicles | 40 | 668 | 88 | 0 | 100 | 704 | 148 | 0 | 244 | 228 | 88 | 0 | 184 | 176 | 76 | 0 | 2744 | |
| Heavy Trucks | 4 | 24 | 16 | | 4 | 20 | 4 | | 12 | 16 | 0 | | 28 | 4 | 0 | | 132 | |
| Buses | | | | | | | | | | | | | | | | | | |
| Pedestrians | | 0 | | | | 4 | | | | 0 | | | | 8 | | | 12 | |
| Bicycles | 0 | 0 | 0 | | 0 | 0 | 0 | | 0 | 0 | 0 | | 0 | 0 | 0 | | 0 | |
| Scooters | | | | | | | | | | | | | | | | | | |

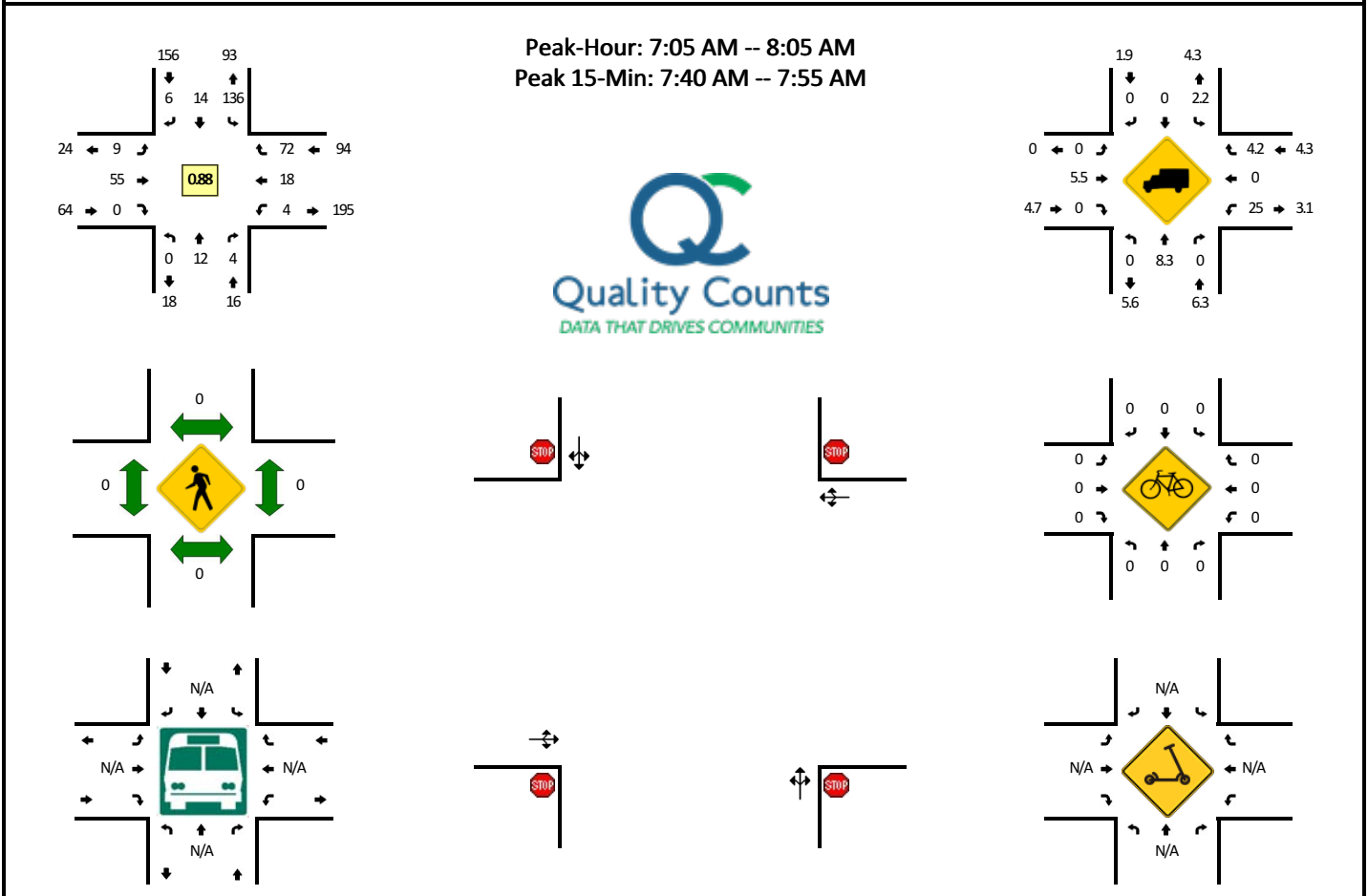
Comments:

LOCATION: Powerline Rd -- Bridge Rd

CITY/STATE: Umatilla, OR

QC JOB #: 15570909

DATE: Thu, Oct 7 2021

Peak-Hour: 7:05 AM -- 8:05 AM
Peak 15-Min: 7:40 AM -- 7:55 AM

| 5-Min Count Period Beginning At | Powerline Rd (Northbound) | | | | Powerline Rd (Southbound) | | | | Bridge Rd (Eastbound) | | | | Bridge Rd (Westbound) | | | | Total | Hourly Totals |
|---------------------------------|---------------------------|------|-------|---|---------------------------|------|-------|---|-----------------------|------|-------|---|-----------------------|------|-------|---|-------|---------------|
| | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | | |
| 7:00 AM | 0 | 2 | 0 | 0 | 7 | 4 | 0 | 0 | 0 | 5 | 0 | 0 | 0 | 3 | 4 | 0 | 25 | |
| 7:05 AM | 0 | 4 | 0 | 0 | 6 | 1 | 0 | 0 | 2 | 5 | 0 | 0 | 0 | 4 | 10 | 0 | 32 | |
| 7:10 AM | 0 | 2 | 0 | 0 | 8 | 1 | 1 | 0 | 0 | 4 | 0 | 0 | 0 | 0 | 8 | 0 | 24 | |
| 7:15 AM | 0 | 2 | 0 | 0 | 7 | 2 | 0 | 0 | 0 | 4 | 0 | 0 | 0 | 0 | 8 | 0 | 23 | |
| 7:20 AM | 0 | 1 | 0 | 0 | 9 | 2 | 1 | 0 | 1 | 2 | 0 | 0 | 0 | 0 | 6 | 0 | 22 | |
| 7:25 AM | 0 | 1 | 0 | 0 | 14 | 3 | 0 | 0 | 2 | 5 | 0 | 0 | 0 | 0 | 4 | 0 | 29 | |
| 7:30 AM | 0 | 0 | 0 | 0 | 11 | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 4 | 0 | 21 | |
| 7:35 AM | 0 | 1 | 0 | 0 | 10 | 1 | 1 | 0 | 1 | 4 | 0 | 0 | 1 | 1 | 5 | 0 | 25 | |
| 7:40 AM | 0 | 0 | 1 | 0 | 16 | 0 | 0 | 0 | 1 | 8 | 0 | 0 | 1 | 2 | 4 | 0 | 33 | |
| 7:45 AM | 0 | 1 | 2 | 0 | 13 | 2 | 0 | 0 | 0 | 8 | 0 | 0 | 1 | 0 | 3 | 0 | 30 | |
| 7:50 AM | 0 | 0 | 0 | 0 | 16 | 0 | 0 | 0 | 0 | 4 | 0 | 0 | 1 | 3 | 7 | 0 | 31 | |
| 7:55 AM | 0 | 0 | 0 | 0 | 15 | 0 | 0 | 0 | 0 | 6 | 0 | 0 | 0 | 2 | 6 | 0 | 29 | 324 |
| 8:00 AM | 0 | 0 | 1 | 0 | 11 | 2 | 1 | 0 | 2 | 5 | 0 | 0 | 0 | 2 | 7 | 0 | 31 | 330 |
| 8:05 AM | 0 | 0 | 0 | 0 | 2 | 0 | 0 | 0 | 1 | 2 | 0 | 0 | 0 | 3 | 8 | 0 | 16 | 314 |
| 8:10 AM | 0 | 3 | 0 | 0 | 10 | 1 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 2 | 7 | 0 | 24 | 314 |
| 8:15 AM | 0 | 0 | 0 | 0 | 9 | 1 | 0 | 0 | 0 | 3 | 0 | 0 | 0 | 1 | 8 | 0 | 22 | 313 |
| 8:20 AM | 0 | 0 | 0 | 0 | 4 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 5 | 13 | 0 | 23 | 314 |
| 8:25 AM | 0 | 1 | 0 | 0 | 7 | 2 | 0 | 0 | 0 | 3 | 0 | 0 | 0 | 0 | 10 | 0 | 23 | 308 |
| 8:30 AM | 0 | 1 | 0 | 0 | 6 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 1 | 7 | 0 | 17 | 304 |
| 8:35 AM | 0 | 3 | 0 | 0 | 6 | 2 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 6 | 0 | 18 | 297 |
| 8:40 AM | 0 | 0 | 0 | 0 | 2 | 0 | 0 | 0 | 0 | 4 | 0 | 0 | 1 | 0 | 4 | 0 | 11 | 275 |
| 8:45 AM | 0 | 0 | 0 | 0 | 10 | 1 | 0 | 0 | 0 | 4 | 0 | 0 | 0 | 1 | 6 | 0 | 22 | 267 |
| 8:50 AM | 0 | 0 | 0 | 0 | 4 | 0 | 0 | 0 | 1 | 2 | 0 | 0 | 0 | 2 | 8 | 0 | 17 | 253 |
| 8:55 AM | 0 | 0 | 1 | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 3 | 0 | 8 | 232 |
| Peak 15-Min Flowrates | Northbound | | | | Southbound | | | | Eastbound | | | | Westbound | | | | Total | |
| | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | | |
| All Vehicles | 0 | 4 | 12 | 0 | 180 | 8 | 0 | 0 | 4 | 80 | 0 | 0 | 12 | 20 | 56 | 0 | 376 | |
| Heavy Trucks | 0 | 0 | 0 | 0 | 4 | 0 | 0 | 0 | 0 | 4 | 0 | 0 | 0 | 0 | 0 | 0 | 8 | |
| Buses | | | | | | | | | | | | | | | | | | |
| Pedestrians | | 0 | | | | 0 | | | | 0 | | | | 0 | | | 0 | |
| Bicycles | 0 | 0 | 0 | | 0 | 0 | 0 | | 0 | 0 | 0 | | 0 | 0 | 0 | | 0 | |
| Scooters | | | | | | | | | | | | | | | | | | |

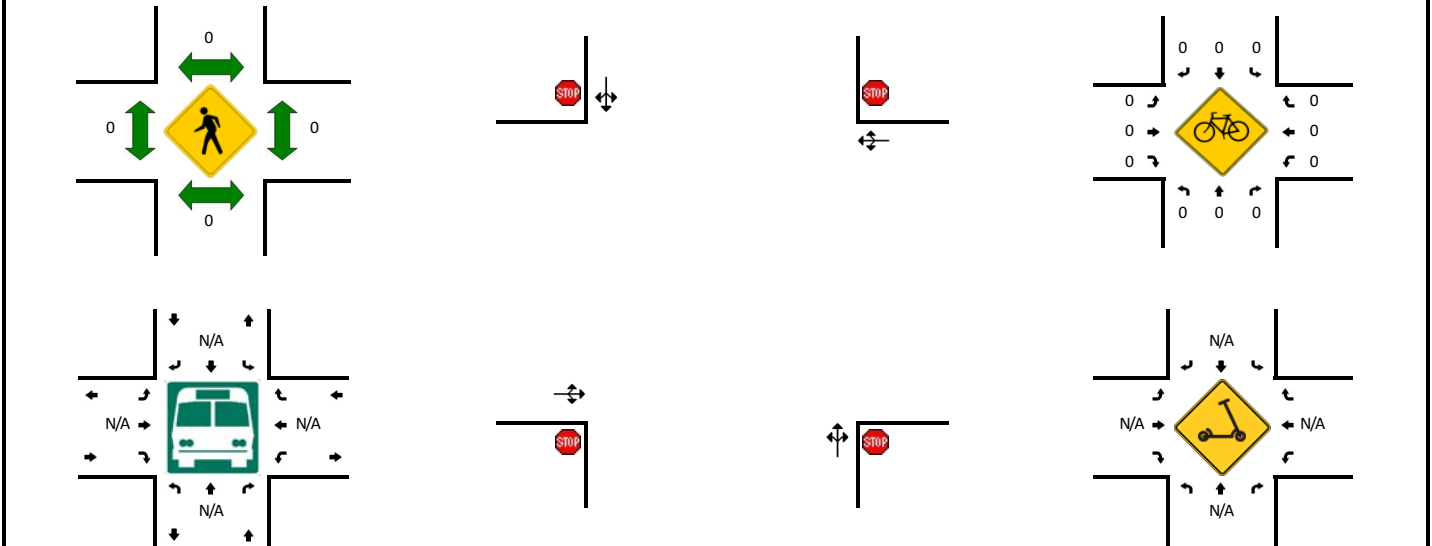
Comments:

LOCATION: Powerline Rd -- Bridge Rd

CITY/STATE: Umatilla, OR

QC JOB #: 15570910

DATE: Wed, Oct 6 2021

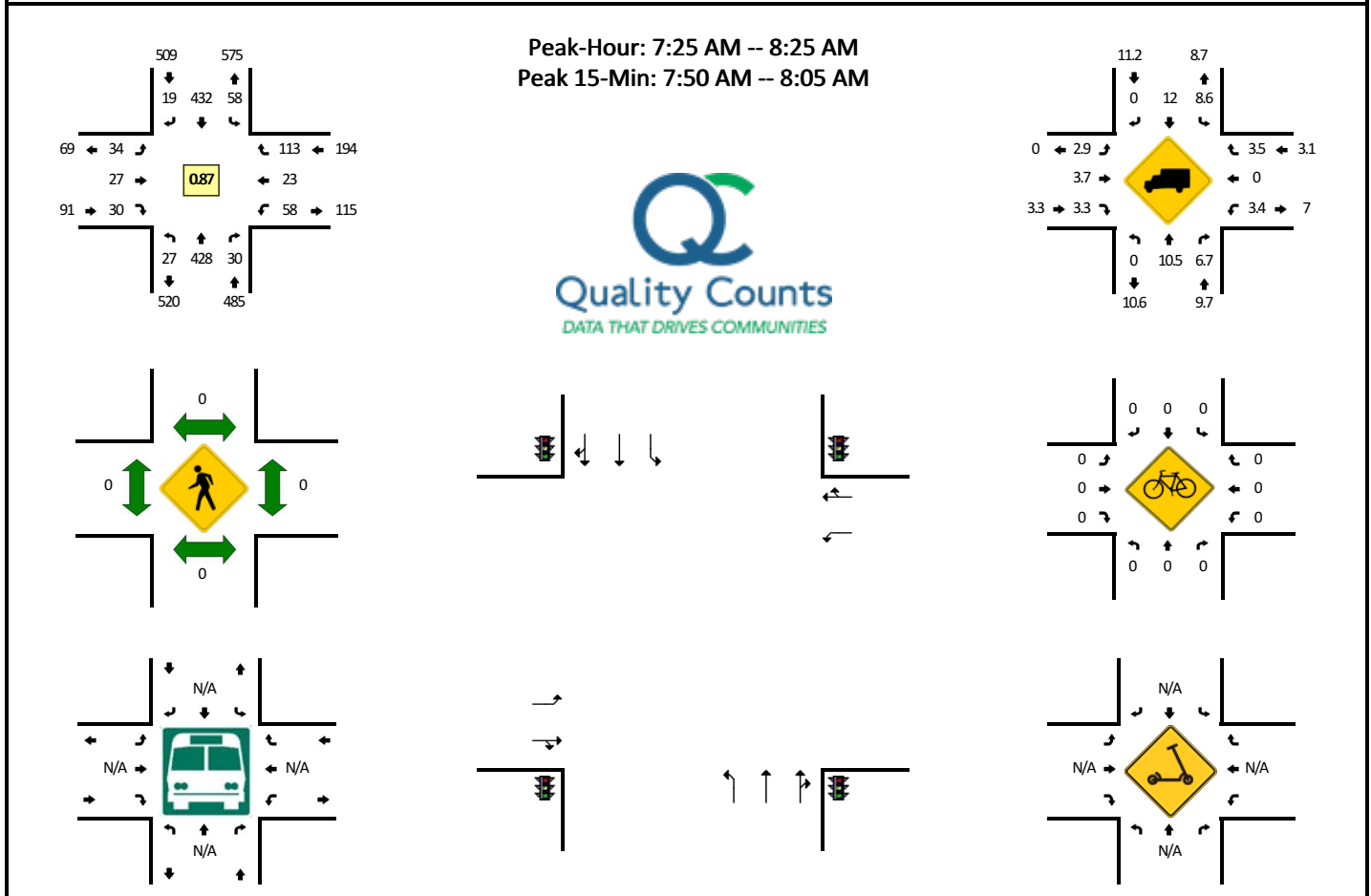
Peak-Hour: 4:30 PM -- 5:30 PM
Peak 15-Min: 4:30 PM -- 4:45 PM

| 5-Min Count Period Beginning At | Powerline Rd (Northbound) | | | | Powerline Rd (Southbound) | | | | Bridge Rd (Eastbound) | | | | Bridge Rd (Westbound) | | | | Total | Hourly Totals |
|---------------------------------|---------------------------|------|-------|---|---------------------------|------|-------|---|-----------------------|------|-------|---|-----------------------|------|-------|---|-------|---------------|
| | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | | |
| 4:00 PM | 1 | 3 | 0 | 0 | 15 | 1 | 1 | 0 | 0 | 3 | 0 | 0 | 1 | 2 | 12 | 0 | 39 | |
| 4:05 PM | 0 | 1 | 0 | 0 | 12 | 2 | 0 | 0 | 0 | 2 | 0 | 0 | 1 | 8 | 14 | 0 | 40 | |
| 4:10 PM | 0 | 3 | 0 | 0 | 10 | 0 | 0 | 0 | 1 | 3 | 0 | 0 | 0 | 1 | 14 | 0 | 32 | |
| 4:15 PM | 0 | 3 | 0 | 0 | 11 | 1 | 2 | 0 | 1 | 2 | 0 | 0 | 0 | 2 | 13 | 0 | 35 | |
| 4:20 PM | 0 | 1 | 0 | 0 | 6 | 1 | 0 | 0 | 0 | 2 | 0 | 0 | 0 | 3 | 16 | 0 | 29 | |
| 4:25 PM | 0 | 4 | 1 | 0 | 3 | 0 | 0 | 0 | 0 | 2 | 0 | 0 | 0 | 2 | 15 | 0 | 27 | |
| 4:30 PM | 0 | 7 | 0 | 0 | 11 | 0 | 0 | 0 | 1 | 1 | 0 | 0 | 1 | 5 | 18 | 0 | 44 | |
| 4:35 PM | 0 | 4 | 0 | 0 | 5 | 0 | 1 | 0 | 0 | 5 | 0 | 0 | 0 | 1 | 13 | 0 | 29 | |
| 4:40 PM | 0 | 2 | 1 | 0 | 14 | 1 | 1 | 0 | 0 | 2 | 0 | 0 | 0 | 4 | 21 | 0 | 46 | |
| 4:45 PM | 0 | 1 | 1 | 0 | 4 | 2 | 3 | 0 | 1 | 3 | 0 | 0 | 0 | 3 | 12 | 0 | 30 | |
| 4:50 PM | 0 | 1 | 0 | 0 | 8 | 0 | 2 | 0 | 1 | 3 | 0 | 0 | 0 | 3 | 8 | 0 | 26 | |
| 4:55 PM | 0 | 2 | 0 | 0 | 13 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 7 | 12 | 0 | 35 | 412 |
| 5:00 PM | 0 | 2 | 1 | 0 | 9 | 0 | 4 | 0 | 0 | 2 | 0 | 0 | 0 | 2 | 11 | 0 | 31 | 404 |
| 5:05 PM | 0 | 3 | 0 | 0 | 11 | 1 | 0 | 0 | 1 | 3 | 0 | 0 | 1 | 6 | 8 | 0 | 34 | 398 |
| 5:10 PM | 0 | 3 | 0 | 0 | 13 | 2 | 0 | 0 | 0 | 4 | 0 | 0 | 0 | 2 | 15 | 0 | 39 | 405 |
| 5:15 PM | 0 | 0 | 0 | 0 | 7 | 0 | 1 | 0 | 1 | 6 | 0 | 0 | 0 | 6 | 12 | 0 | 33 | 403 |
| 5:20 PM | 0 | 1 | 0 | 0 | 17 | 0 | 0 | 0 | 0 | 4 | 0 | 0 | 0 | 6 | 13 | 0 | 41 | 415 |
| 5:25 PM | 0 | 0 | 0 | 0 | 13 | 1 | 1 | 0 | 1 | 4 | 0 | 0 | 0 | 4 | 9 | 0 | 33 | 421 |
| 5:30 PM | 0 | 1 | 1 | 0 | 16 | 1 | 0 | 0 | 1 | 4 | 0 | 0 | 0 | 2 | 15 | 0 | 41 | 418 |
| 5:35 PM | 0 | 2 | 0 | 0 | 12 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 13 | 0 | 29 | 418 |
| 5:40 PM | 0 | 3 | 1 | 0 | 12 | 0 | 1 | 0 | 0 | 2 | 0 | 0 | 0 | 2 | 17 | 0 | 38 | 410 |
| 5:45 PM | 0 | 1 | 0 | 0 | 15 | 0 | 0 | 0 | 1 | 1 | 0 | 0 | 0 | 1 | 10 | 0 | 29 | 409 |
| 5:50 PM | 0 | 1 | 0 | 0 | 9 | 0 | 0 | 0 | 0 | 5 | 0 | 0 | 0 | 5 | 10 | 0 | 30 | 413 |
| 5:55 PM | 0 | 1 | 0 | 0 | 10 | 1 | 0 | 0 | 0 | 5 | 0 | 0 | 0 | 3 | 12 | 0 | 32 | 410 |
| Peak 15-Min Flowrates | Northbound | | | | Southbound | | | | Eastbound | | | | Westbound | | | | Total | |
| | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | | |
| All Vehicles | 0 | 52 | 4 | 0 | 120 | 4 | 8 | 0 | 4 | 32 | 0 | 0 | 4 | 40 | 208 | 0 | 476 | |
| Heavy Trucks | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 0 | 0 | 8 | 0 | 0 | 12 | |
| Buses | | | | | | | | | | | | | | | | | | |
| Pedestrians | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Bicycles | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Scooters | | | | | | | | | | | | | | | | | | |

Comments:

LOCATION: Hwy 395 -- W Punkin Center Rd
CITY/STATE: Umatilla, OR

QC JOB #: 15570911
DATE: Thu, Oct 7 2021

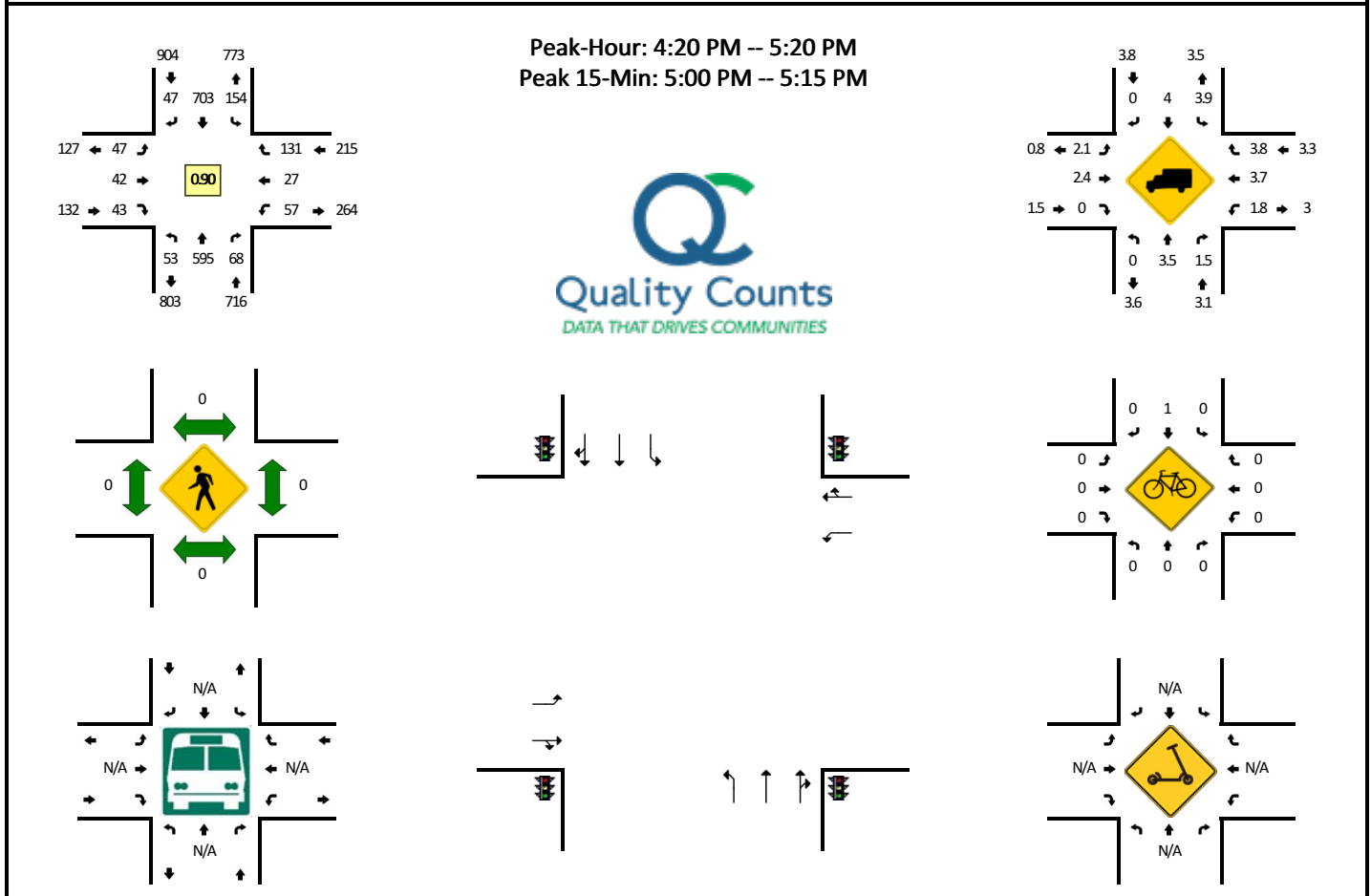


| 5-Min Count Period Beginning At | Hwy 395 (Northbound) | | | | Hwy 395 (Southbound) | | | | W Punkin Center Rd (Eastbound) | | | | W Punkin Center Rd (Westbound) | | | | Total | Hourly Totals |
|---------------------------------|----------------------|------|-------|---|----------------------|------|-------|---|--------------------------------|------|-------|---|--------------------------------|------|-------|---|-------|---------------|
| | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | | |
| 7:00 AM | 0 | 24 | 2 | 0 | 5 | 29 | 1 | 0 | 0 | 0 | 2 | 0 | 4 | 4 | 6 | 0 | 77 | |
| 7:05 AM | 1 | 24 | 2 | 0 | 1 | 36 | 1 | 0 | 1 | 2 | 3 | 0 | 2 | 0 | 15 | 0 | 88 | |
| 7:10 AM | 2 | 29 | 3 | 0 | 6 | 27 | 1 | 0 | 0 | 1 | 2 | 0 | 9 | 3 | 10 | 0 | 93 | |
| 7:15 AM | 0 | 31 | 1 | 0 | 6 | 23 | 2 | 0 | 2 | 1 | 3 | 0 | 8 | 2 | 5 | 0 | 84 | |
| 7:20 AM | 3 | 36 | 3 | 0 | 3 | 20 | 3 | 0 | 3 | 2 | 2 | 0 | 3 | 2 | 14 | 0 | 94 | |
| 7:25 AM | 1 | 28 | 0 | 0 | 4 | 35 | 2 | 0 | 2 | 0 | 3 | 0 | 4 | 2 | 7 | 0 | 88 | |
| 7:30 AM | 4 | 34 | 2 | 0 | 6 | 35 | 1 | 0 | 3 | 1 | 1 | 0 | 5 | 4 | 10 | 0 | 106 | |
| 7:35 AM | 2 | 27 | 2 | 0 | 4 | 36 | 2 | 0 | 3 | 4 | 1 | 0 | 0 | 0 | 9 | 0 | 90 | |
| 7:40 AM | 1 | 45 | 0 | 0 | 4 | 36 | 2 | 0 | 2 | 2 | 7 | 0 | 6 | 3 | 9 | 0 | 117 | |
| 7:45 AM | 3 | 34 | 2 | 0 | 2 | 23 | 3 | 0 | 2 | 5 | 7 | 0 | 5 | 1 | 8 | 0 | 95 | |
| 7:50 AM | 3 | 37 | 3 | 0 | 9 | 37 | 0 | 0 | 4 | 5 | 1 | 0 | 6 | 4 | 22 | 0 | 131 | |
| 7:55 AM | 1 | 44 | 7 | 0 | 4 | 37 | 0 | 0 | 5 | 1 | 1 | 0 | 8 | 2 | 8 | 0 | 118 | 1181 |
| 8:00 AM | 3 | 31 | 3 | 0 | 5 | 45 | 4 | 0 | 4 | 1 | 3 | 0 | 2 | 3 | 13 | 0 | 117 | 1221 |
| 8:05 AM | 0 | 43 | 2 | 0 | 5 | 39 | 2 | 0 | 3 | 3 | 2 | 0 | 6 | 1 | 14 | 0 | 120 | 1253 |
| 8:10 AM | 2 | 34 | 4 | 0 | 8 | 39 | 0 | 0 | 2 | 0 | 1 | 0 | 5 | 1 | 4 | 0 | 100 | 1260 |
| 8:15 AM | 4 | 33 | 3 | 0 | 2 | 41 | 1 | 0 | 1 | 1 | 2 | 0 | 8 | 0 | 5 | 0 | 101 | 1277 |
| 8:20 AM | 3 | 38 | 2 | 0 | 5 | 29 | 2 | 0 | 3 | 4 | 1 | 0 | 3 | 2 | 4 | 0 | 96 | 1279 |
| 8:25 AM | 6 | 20 | 1 | 0 | 4 | 31 | 2 | 0 | 2 | 0 | 3 | 0 | 2 | 2 | 13 | 0 | 86 | 1277 |
| 8:30 AM | 5 | 25 | 2 | 0 | 3 | 32 | 1 | 0 | 2 | 1 | 3 | 0 | 1 | 2 | 4 | 0 | 81 | 1252 |
| 8:35 AM | 4 | 27 | 4 | 0 | 4 | 31 | 0 | 0 | 2 | 4 | 5 | 0 | 2 | 3 | 9 | 0 | 95 | 1257 |
| 8:40 AM | 2 | 27 | 3 | 0 | 4 | 27 | 1 | 0 | 2 | 1 | 6 | 0 | 4 | 4 | 5 | 0 | 86 | 1226 |
| 8:45 AM | 7 | 33 | 1 | 0 | 3 | 43 | 0 | 0 | 6 | 1 | 5 | 0 | 5 | 0 | 7 | 0 | 111 | 1242 |
| 8:50 AM | 3 | 36 | 3 | 0 | 5 | 36 | 4 | 0 | 1 | 1 | 1 | 0 | 0 | 2 | 7 | 0 | 99 | 1210 |
| 8:55 AM | 3 | 26 | 0 | 0 | 4 | 39 | 1 | 0 | 1 | 2 | 2 | 0 | 4 | 5 | 8 | 0 | 95 | 1187 |
| Peak 15-Min Flowrates | Northbound | | | | Southbound | | | | Eastbound | | | | Westbound | | | | Total | |
| | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | | |
| All Vehicles | 28 | 448 | 52 | 0 | 72 | 476 | 16 | 0 | 52 | 28 | 20 | 0 | 64 | 36 | 172 | 0 | 1464 | |
| Heavy Trucks | 0 | 44 | 4 | 0 | 4 | 40 | 0 | 0 | 0 | 4 | 0 | 0 | 4 | 0 | 0 | 0 | 100 | |
| Buses | | | | | | | | | | | | | | | | | | |
| Pedestrians | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Bicycles | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Scooters | | | | | | | | | | | | | | | | | | |

Comments:

LOCATION: Hwy 395 -- W Punkin Center Rd
CITY/STATE: Umatilla, OR

QC JOB #: 15570912
DATE: Wed, Oct 6 2021



| 5-Min Count Period Beginning At | Hwy 395 (Northbound) | | | | Hwy 395 (Southbound) | | | | W Punkin Center Rd (Eastbound) | | | | W Punkin Center Rd (Westbound) | | | | Total | Hourly Totals |
|---------------------------------|----------------------|------|-------|---|----------------------|------|-------|---|--------------------------------|------|-------|---|--------------------------------|------|-------|---|-------|---------------|
| | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | | |
| 4:00 PM | 1 | 47 | 5 | 0 | 12 | 50 | 6 | 0 | 11 | 1 | 4 | 0 | 6 | 4 | 13 | 0 | 160 | |
| 4:05 PM | 1 | 47 | 8 | 0 | 13 | 56 | 3 | 0 | 9 | 2 | 4 | 0 | 7 | 2 | 8 | 0 | 160 | |
| 4:10 PM | 6 | 69 | 6 | 0 | 15 | 56 | 4 | 0 | 3 | 1 | 6 | 0 | 4 | 3 | 6 | 0 | 179 | |
| 4:15 PM | 2 | 45 | 3 | 0 | 9 | 45 | 5 | 0 | 5 | 1 | 6 | 0 | 9 | 1 | 15 | 0 | 146 | |
| 4:20 PM | 6 | 44 | 2 | 0 | 10 | 78 | 5 | 0 | 3 | 2 | 4 | 0 | 7 | 1 | 4 | 0 | 166 | |
| 4:25 PM | 1 | 35 | 3 | 0 | 14 | 53 | 3 | 0 | 8 | 6 | 5 | 0 | 7 | 3 | 13 | 0 | 151 | |
| 4:30 PM | 2 | 50 | 4 | 0 | 9 | 53 | 1 | 0 | 1 | 3 | 3 | 0 | 7 | 1 | 11 | 0 | 145 | |
| 4:35 PM | 10 | 49 | 6 | 0 | 18 | 55 | 3 | 0 | 2 | 1 | 5 | 0 | 2 | 2 | 5 | 0 | 158 | |
| 4:40 PM | 3 | 63 | 5 | 0 | 9 | 56 | 2 | 0 | 5 | 5 | 4 | 0 | 2 | 4 | 9 | 0 | 167 | |
| 4:45 PM | 8 | 50 | 9 | 0 | 14 | 46 | 2 | 0 | 4 | 5 | 2 | 0 | 1 | 2 | 11 | 0 | 154 | |
| 4:50 PM | 3 | 43 | 11 | 0 | 14 | 56 | 4 | 0 | 2 | 1 | 4 | 0 | 5 | 2 | 7 | 0 | 152 | |
| 4:55 PM | 2 | 44 | 5 | 0 | 11 | 52 | 6 | 0 | 7 | 5 | 4 | 0 | 7 | 2 | 25 | 0 | 170 | 1908 |
| 5:00 PM | 3 | 48 | 4 | 0 | 8 | 60 | 7 | 0 | 3 | 2 | 3 | 0 | 5 | 3 | 17 | 0 | 163 | 1911 |
| 5:05 PM | 3 | 60 | 8 | 0 | 16 | 81 | 5 | 0 | 3 | 4 | 3 | 0 | 3 | 3 | 16 | 0 | 205 | 1956 |
| 5:10 PM | 7 | 58 | 7 | 0 | 18 | 57 | 5 | 0 | 5 | 4 | 3 | 0 | 6 | 3 | 6 | 0 | 179 | 1956 |
| 5:15 PM | 5 | 51 | 4 | 0 | 13 | 56 | 4 | 0 | 4 | 4 | 3 | 0 | 5 | 1 | 7 | 0 | 157 | 1967 |
| 5:20 PM | 4 | 45 | 4 | 0 | 13 | 40 | 4 | 0 | 5 | 4 | 0 | 0 | 10 | 4 | 16 | 0 | 149 | 1950 |
| 5:25 PM | 2 | 49 | 3 | 0 | 9 | 53 | 7 | 0 | 8 | 1 | 3 | 0 | 5 | 3 | 9 | 0 | 152 | 1951 |
| 5:30 PM | 3 | 39 | 7 | 0 | 16 | 46 | 3 | 0 | 5 | 1 | 0 | 0 | 4 | 2 | 8 | 0 | 134 | 1940 |
| 5:35 PM | 4 | 35 | 4 | 0 | 8 | 55 | 2 | 0 | 4 | 4 | 7 | 0 | 3 | 1 | 6 | 0 | 133 | 1915 |
| 5:40 PM | 2 | 37 | 4 | 0 | 3 | 34 | 1 | 0 | 4 | 7 | 3 | 0 | 8 | 3 | 4 | 0 | 110 | 1858 |
| 5:45 PM | 1 | 38 | 4 | 0 | 8 | 60 | 2 | 0 | 1 | 3 | 1 | 0 | 4 | 2 | 12 | 0 | 136 | 1840 |
| 5:50 PM | 5 | 52 | 4 | 0 | 6 | 35 | 3 | 0 | 5 | 4 | 3 | 0 | 8 | 4 | 7 | 0 | 136 | 1824 |
| 5:55 PM | 4 | 35 | 6 | 0 | 15 | 49 | 1 | 0 | 6 | 2 | 5 | 0 | 6 | 4 | 5 | 0 | 138 | 1792 |
| Peak 15-Min Flowrates | Northbound | | | | Southbound | | | | Eastbound | | | | Westbound | | | | Total | |
| | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | | |
| All Vehicles | 52 | 664 | 76 | 0 | 168 | 792 | 68 | 0 | 44 | 40 | 36 | 0 | 56 | 36 | 156 | 0 | 2188 | |
| Heavy Trucks | 0 | 16 | 4 | 0 | 0 | 40 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 8 | 0 | 72 | |
| Buses | | | | | | | | | | | | | | | | | | |
| Pedestrians | | 0 | | | | 0 | | | | 0 | | | | 0 | | | 0 | |
| Bicycles | 0 | 0 | 0 | | 0 | 4 | 0 | | 0 | 0 | 0 | | 0 | 0 | 0 | | 4 | |
| Scooters | | | | | | | | | | | | | | | | | | |

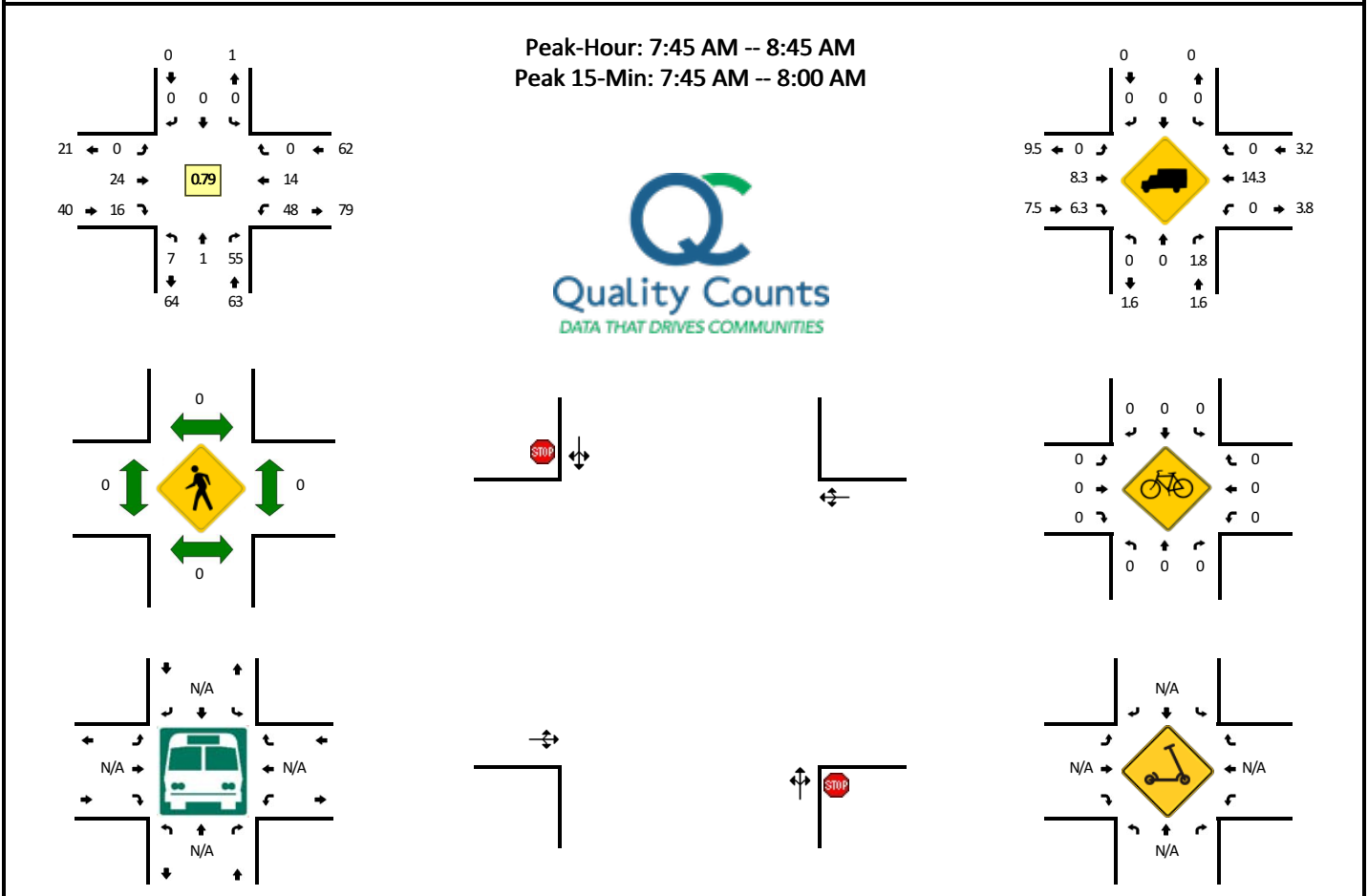
Comments:

LOCATION: NW Geer Rd -- W Punkin Center Rd

CITY/STATE: Hermiston, OR

QC JOB #: 15570913

DATE: Thu, Oct 7 2021

Peak-Hour: 7:45 AM -- 8:45 AM
Peak 15-Min: 7:45 AM -- 8:00 AM

| 5-Min Count Period Beginning At | NW Geer Rd (Northbound) | | | | NW Geer Rd (Southbound) | | | | W Punkin Center Rd (Eastbound) | | | | W Punkin Center Rd (Westbound) | | | | Total | Hourly Totals |
|---------------------------------|-------------------------|------|-------|---|-------------------------|------|-------|---|--------------------------------|------|-------|---|--------------------------------|------|-------|---|-------|---------------|
| | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | | |
| 7:00 AM | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 2 | 0 | 0 | 0 | 3 | |
| 7:05 AM | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 0 | 4 | 2 | 0 | 0 | 10 | |
| 7:10 AM | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 2 | 0 | 1 | 1 | 0 | 0 | 8 | |
| 7:15 AM | 0 | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 0 | 3 | 1 | 0 | 0 | 9 | |
| 7:20 AM | 2 | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 1 | 0 | 2 | 0 | 1 | 0 | 9 | |
| 7:25 AM | 0 | 1 | 3 | 0 | 0 | 2 | 0 | 0 | 0 | 3 | 0 | 0 | 2 | 2 | 0 | 0 | 13 | |
| 7:30 AM | 1 | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 1 | 1 | 0 | 0 | 6 | |
| 7:35 AM | 1 | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 2 | 0 | 5 | 0 | 1 | 0 | 14 | |
| 7:40 AM | 0 | 0 | 4 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 3 | 0 | 3 | 0 | 0 | 0 | 12 | |
| 7:45 AM | 0 | 0 | 6 | 0 | 0 | 0 | 0 | 0 | 0 | 5 | 2 | 0 | 4 | 1 | 0 | 0 | 18 | |
| 7:50 AM | 0 | 0 | 9 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 5 | 0 | 5 | 0 | 0 | 0 | 23 | |
| 7:55 AM | 1 | 0 | 5 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 1 | 0 | 3 | 0 | 0 | 0 | 11 | 136 |
| 8:00 AM | 1 | 0 | 5 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 1 | 0 | 4 | 0 | 0 | 0 | 12 | 145 |
| 8:05 AM | 2 | 0 | 3 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 1 | 0 | 1 | 1 | 0 | 0 | 9 | 144 |
| 8:10 AM | 0 | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 1 | 0 | 6 | 0 | 0 | 0 | 11 | 147 |
| 8:15 AM | 1 | 0 | 3 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 4 | 2 | 0 | 0 | 13 | 151 |
| 8:20 AM | 0 | 0 | 6 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 1 | 0 | 5 | 1 | 0 | 0 | 15 | 157 |
| 8:25 AM | 1 | 0 | 3 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 3 | 4 | 0 | 0 | 12 | 156 |
| 8:30 AM | 0 | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 1 | 0 | 6 | 1 | 0 | 0 | 13 | 163 |
| 8:35 AM | 0 | 0 | 8 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 1 | 0 | 0 | 13 | 162 |
| 8:40 AM | 1 | 1 | 3 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 0 | 3 | 3 | 0 | 0 | 15 | 165 |
| 8:45 AM | 0 | 0 | 7 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 1 | 0 | 2 | 2 | 0 | 0 | 15 | 162 |
| 8:50 AM | 0 | 1 | 4 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 0 | 5 | 1 | 0 | 0 | 13 | 152 |
| 8:55 AM | 0 | 0 | 4 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 5 | 2 | 0 | 0 | 12 | 153 |
| Peak 15-Min Flowrates | Northbound | | | | Southbound | | | | Eastbound | | | | Westbound | | | | Total | |
| | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | | |
| All Vehicles | 4 | 0 | 80 | 0 | 0 | 0 | 0 | 0 | 0 | 40 | 32 | 0 | 48 | 4 | 0 | 0 | 208 | |
| Heavy Trucks | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 4 | 0 | 0 | 4 | 0 | 0 | 12 | |
| Buses | | | | | | | | | | | | | | | | | | |
| Pedestrians | | 0 | | | | 0 | | | | 0 | | | | 0 | | | 0 | |
| Bicycles | 0 | 0 | 0 | | 0 | 0 | 0 | | 0 | 0 | 0 | | 0 | 0 | 0 | | 0 | |
| Scooters | | | | | | | | | | | | | | | | | | |

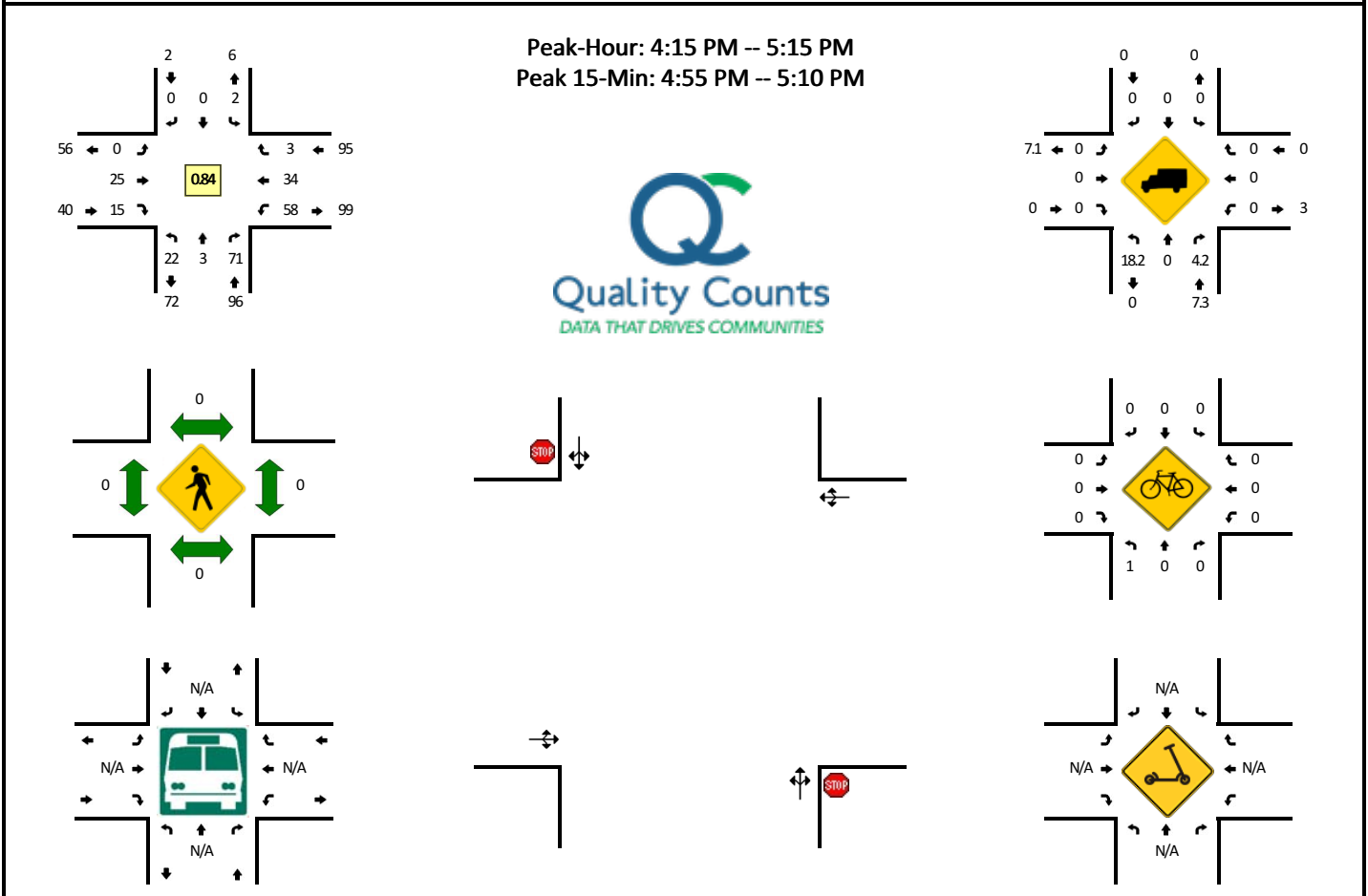
Comments:

LOCATION: NW Geer Rd -- W Punkin Center Rd

CITY/STATE: Hermiston, OR

QC JOB #: 15570914

DATE: Wed, Oct 6 2021

Peak-Hour: 4:15 PM -- 5:15 PM
Peak 15-Min: 4:55 PM -- 5:10 PM

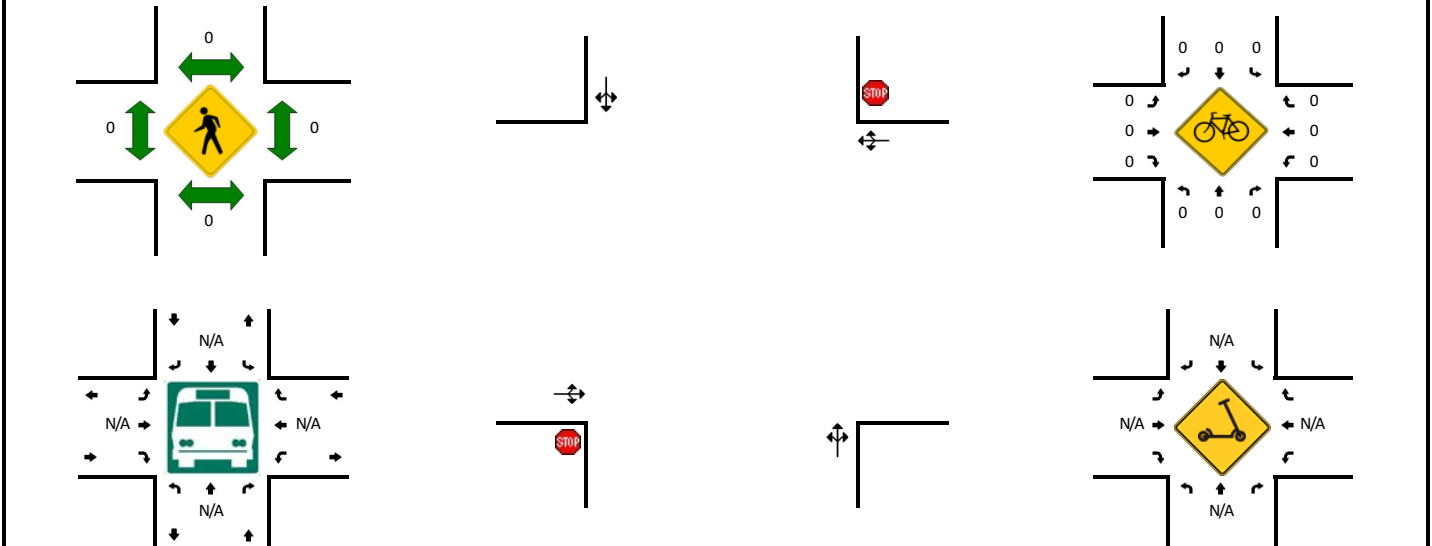
| 5-Min Count Period Beginning At | NW Geer Rd (Northbound) | | | | NW Geer Rd (Southbound) | | | | W Punkin Center Rd (Eastbound) | | | | W Punkin Center Rd (Westbound) | | | | Total | Hourly Totals |
|---------------------------------|-------------------------|------|-------|---|-------------------------|------|-------|---|--------------------------------|------|-------|---|--------------------------------|------|-------|---|-------|---------------|
| | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | | |
| 4:00 PM | 3 | 0 | 10 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 1 | 0 | 4 | 3 | 0 | 0 | 23 | |
| 4:05 PM | 3 | 0 | 8 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 1 | 0 | 2 | 3 | 0 | 0 | 19 | |
| 4:10 PM | 1 | 0 | 7 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 2 | 0 | 4 | 5 | 0 | 0 | 20 | |
| 4:15 PM | 3 | 0 | 6 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 2 | 0 | 4 | 2 | 0 | 0 | 19 | |
| 4:20 PM | 2 | 0 | 5 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 3 | 0 | 1 | 3 | 0 | 0 | 16 | |
| 4:25 PM | 4 | 0 | 8 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 1 | 0 | 5 | 2 | 0 | 0 | 21 | |
| 4:30 PM | 2 | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 3 | 0 | 0 | 0 | 8 | |
| 4:35 PM | 1 | 0 | 4 | 0 | 0 | 0 | 0 | 0 | 0 | 5 | 2 | 0 | 7 | 2 | 0 | 0 | 21 | |
| 4:40 PM | 1 | 2 | 7 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 3 | 0 | 3 | 2 | 0 | 0 | 19 | |
| 4:45 PM | 3 | 0 | 7 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 0 | 4 | 2 | 0 | 0 | 18 | |
| 4:50 PM | 0 | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 2 | 0 | 8 | 3 | 0 | 0 | 17 | |
| 4:55 PM | 3 | 0 | 9 | 0 | 1 | 0 | 0 | 0 | 0 | 3 | 0 | 0 | 7 | 2 | 1 | 0 | 26 | 227 |
| 5:00 PM | 0 | 0 | 3 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 1 | 0 | 4 | 5 | 0 | 0 | 14 | 218 |
| 5:05 PM | 3 | 0 | 12 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 6 | 7 | 0 | 1 | 29 | 228 |
| 5:10 PM | 0 | 1 | 6 | 0 | 1 | 0 | 0 | 0 | 0 | 5 | 1 | 0 | 5 | 4 | 2 | 0 | 25 | 233 |
| 5:15 PM | 1 | 0 | 5 | 0 | 2 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 3 | 1 | 0 | 0 | 13 | 227 |
| 5:20 PM | 0 | 0 | 5 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 8 | 5 | 0 | 0 | 18 | 229 |
| 5:25 PM | 1 | 0 | 6 | 0 | 2 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 7 | 1 | 0 | 0 | 18 | 226 |
| 5:30 PM | 1 | 0 | 8 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 3 | 1 | 0 | 0 | 14 | 232 |
| 5:35 PM | 0 | 1 | 6 | 0 | 1 | 0 | 0 | 0 | 0 | 4 | 1 | 0 | 2 | 1 | 0 | 0 | 16 | 227 |
| 5:40 PM | 3 | 0 | 5 | 0 | 0 | 1 | 0 | 0 | 0 | 3 | 1 | 0 | 3 | 1 | 0 | 0 | 17 | 225 |
| 5:45 PM | 4 | 0 | 4 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 1 | 0 | 2 | 1 | 0 | 0 | 14 | 221 |
| 5:50 PM | 1 | 0 | 6 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 0 | 6 | 2 | 0 | 0 | 18 | 222 |
| 5:55 PM | 1 | 1 | 8 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 2 | 0 | 7 | 3 | 0 | 0 | 23 | 219 |
| Peak 15-Min Flowrates | Northbound | | | | Southbound | | | | Eastbound | | | | Westbound | | | | Total | |
| | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | | |
| All Vehicles | 24 | 0 | 96 | 0 | 4 | 0 | 0 | 0 | 0 | 16 | 4 | 0 | 68 | 56 | 4 | 4 | 276 | |
| Heavy Trucks | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Buses | | | | | | | | | | | | | | | | | | |
| Pedestrians | | 0 | | | | 0 | | | | 0 | | | | 0 | | | 0 | |
| Bicycles | 0 | 0 | 0 | | 0 | 0 | 0 | | 0 | 0 | 0 | | 0 | 0 | 0 | | 0 | |
| Scooters | | | | | | | | | | | | | | | | | | |

Comments:

LOCATION: Umatilla River Rd -- Cooney Ln
CITY/STATE: Hermiston, OR

QC JOB #: 15570915
DATE: Thu, Oct 7 2021

Peak-Hour: 7:15 AM -- 8:15 AM
 Peak 15-Min: 7:40 AM -- 7:55 AM

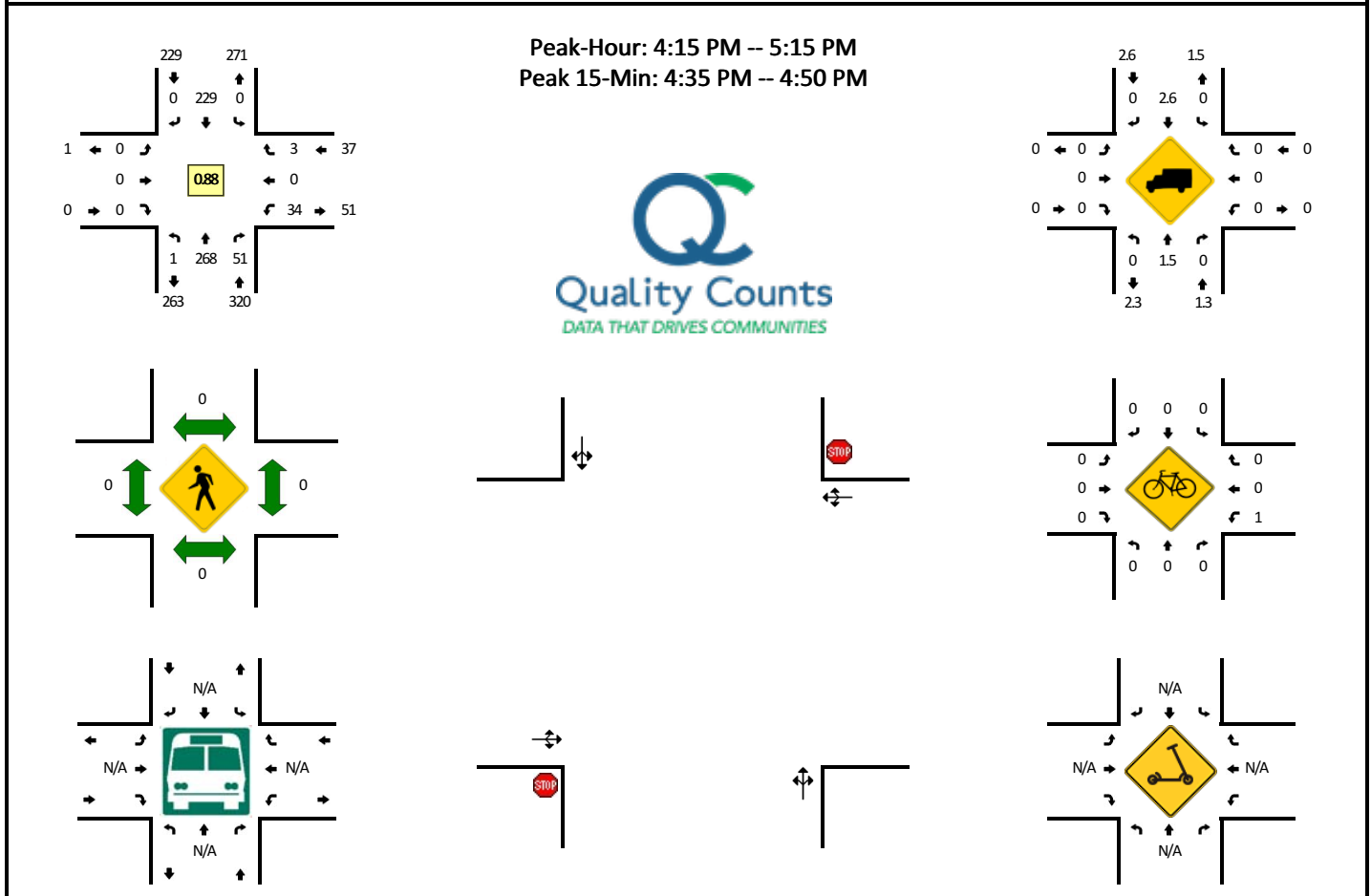


| 5-Min Count Period Beginning At | Umatilla River Rd (Northbound) | | | | Umatilla River Rd (Southbound) | | | | Cooney Ln (Eastbound) | | | | Cooney Ln (Westbound) | | | | Total | Hourly Totals |
|---------------------------------|--------------------------------|------|-------|---|--------------------------------|------|-------|---|-----------------------|------|-------|---|-----------------------|------|-------|---|-------|---------------|
| | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | | |
| 7:00 AM | 0 | 10 | 2 | 0 | 0 | 8 | 0 | 0 | 0 | 1 | 0 | 0 | 1 | 0 | 0 | 0 | 22 | |
| 7:05 AM | 0 | 9 | 1 | 0 | 0 | 13 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 23 | |
| 7:10 AM | 0 | 8 | 1 | 0 | 0 | 10 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 0 | 0 | 23 | |
| 7:15 AM | 0 | 13 | 1 | 0 | 0 | 19 | 0 | 0 | 0 | 0 | 0 | 0 | 5 | 0 | 0 | 0 | 38 | |
| 7:20 AM | 0 | 8 | 0 | 0 | 0 | 11 | 0 | 0 | 0 | 0 | 0 | 0 | 8 | 0 | 0 | 0 | 27 | |
| 7:25 AM | 0 | 6 | 2 | 0 | 0 | 18 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 0 | 0 | 28 | |
| 7:30 AM | 0 | 7 | 2 | 0 | 0 | 27 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 37 | |
| 7:35 AM | 0 | 11 | 2 | 0 | 2 | 15 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 0 | 0 | 33 | |
| 7:40 AM | 0 | 7 | 3 | 0 | 0 | 18 | 0 | 0 | 0 | 0 | 0 | 0 | 7 | 0 | 0 | 0 | 35 | |
| 7:45 AM | 0 | 7 | 2 | 0 | 0 | 23 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 0 | 0 | 36 | |
| 7:50 AM | 0 | 11 | 1 | 0 | 0 | 30 | 0 | 0 | 0 | 0 | 0 | 0 | 6 | 0 | 0 | 0 | 48 | |
| 7:55 AM | 0 | 7 | 2 | 0 | 0 | 17 | 0 | 0 | 0 | 0 | 0 | 0 | 5 | 0 | 0 | 0 | 31 | 381 |
| 8:00 AM | 0 | 6 | 1 | 0 | 0 | 21 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 0 | 0 | 32 | 391 |
| 8:05 AM | 0 | 11 | 2 | 0 | 1 | 20 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 0 | 0 | 36 | 404 |
| 8:10 AM | 0 | 14 | 3 | 0 | 0 | 16 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 34 | 415 |
| 8:15 AM | 0 | 12 | 2 | 0 | 0 | 13 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 0 | 0 | 30 | 407 |
| 8:20 AM | 0 | 11 | 2 | 0 | 0 | 12 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 25 | 405 |
| 8:25 AM | 0 | 5 | 7 | 0 | 0 | 18 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 0 | 0 | 32 | 409 |
| 8:30 AM | 0 | 11 | 2 | 0 | 0 | 10 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 0 | 0 | 25 | 397 |
| 8:35 AM | 0 | 5 | 0 | 0 | 0 | 14 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 0 | 0 | 21 | 385 |
| 8:40 AM | 0 | 9 | 2 | 0 | 0 | 10 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 22 | 372 |
| 8:45 AM | 0 | 4 | 0 | 0 | 0 | 12 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 0 | 0 | 19 | 355 |
| 8:50 AM | 0 | 9 | 2 | 0 | 0 | 10 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 0 | 0 | 25 | 332 |
| 8:55 AM | 0 | 9 | 1 | 0 | 0 | 13 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 23 | 324 |
| Peak 15-Min Flowrates | Northbound | | | | Southbound | | | | Eastbound | | | | Westbound | | | | Total | |
| | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | | |
| All Vehicles | 0 | 100 | 24 | 0 | 0 | 284 | 0 | 0 | 0 | 0 | 0 | 0 | 68 | 0 | 0 | 0 | 476 | |
| Heavy Trucks | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Buses | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Pedestrians | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Bicycles | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Scoters | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |

Comments:

LOCATION: Umatilla River Rd -- Cooney Ln
CITY/STATE: Hermiston, OR

QC JOB #: 15570916
DATE: Wed, Oct 6 2021



| 5-Min Count Period Beginning At | Umatilla River Rd (Northbound) | | | | Umatilla River Rd (Southbound) | | | | Cooney Ln (Eastbound) | | | | Cooney Ln (Westbound) | | | | Total | Hourly Totals |
|---------------------------------|--------------------------------|------|-------|---|--------------------------------|------|-------|---|-----------------------|------|-------|---|-----------------------|------|-------|---|-------|---------------|
| | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | | |
| 4:00 PM | 0 | 18 | 3 | 0 | 1 | 19 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 42 | |
| 4:05 PM | 0 | 20 | 3 | 0 | 0 | 15 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 0 | 0 | 42 | |
| 4:10 PM | 0 | 25 | 2 | 0 | 0 | 18 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 46 | |
| 4:15 PM | 0 | 19 | 4 | 0 | 0 | 24 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 0 | 0 | 49 | |
| 4:20 PM | 0 | 22 | 3 | 0 | 0 | 23 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 48 | |
| 4:25 PM | 0 | 24 | 2 | 0 | 0 | 15 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 0 | 0 | 44 | |
| 4:30 PM | 0 | 18 | 3 | 0 | 0 | 15 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 1 | 0 | 39 | |
| 4:35 PM | 0 | 21 | 9 | 0 | 0 | 18 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 49 | |
| 4:40 PM | 0 | 33 | 4 | 0 | 0 | 23 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 0 | 0 | 63 | |
| 4:45 PM | 0 | 18 | 5 | 0 | 0 | 30 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 0 | 55 | |
| 4:50 PM | 0 | 23 | 5 | 0 | 0 | 16 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 0 | 0 | 47 | |
| 4:55 PM | 1 | 17 | 4 | 0 | 0 | 18 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 0 | 0 | 43 | 567 |
| 5:00 PM | 0 | 24 | 2 | 0 | 0 | 17 | 0 | 0 | 0 | 0 | 0 | 0 | 7 | 0 | 0 | 0 | 50 | 575 |
| 5:05 PM | 0 | 25 | 5 | 0 | 0 | 13 | 0 | 0 | 0 | 0 | 0 | 0 | 5 | 0 | 0 | 0 | 48 | 581 |
| 5:10 PM | 0 | 24 | 5 | 0 | 0 | 17 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 1 | 0 | 51 | 586 |
| 5:15 PM | 0 | 12 | 1 | 0 | 0 | 13 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 27 | 564 |
| 5:20 PM | 0 | 16 | 1 | 0 | 1 | 20 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 0 | 0 | 41 | 557 |
| 5:25 PM | 0 | 16 | 4 | 0 | 0 | 13 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 0 | 0 | 36 | 549 |
| 5:30 PM | 0 | 14 | 5 | 0 | 0 | 18 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 1 | 0 | 42 | 552 |
| 5:35 PM | 0 | 27 | 4 | 0 | 0 | 14 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 0 | 0 | 48 | 551 |
| 5:40 PM | 0 | 15 | 7 | 0 | 0 | 12 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 35 | 523 |
| 5:45 PM | 0 | 18 | 3 | 0 | 0 | 15 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 1 | 0 | 39 | 507 |
| 5:50 PM | 0 | 13 | 2 | 0 | 0 | 21 | 1 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 0 | 0 | 39 | 499 |
| 5:55 PM | 0 | 20 | 5 | 0 | 0 | 19 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 0 | 46 | 502 |
| Peak 15-Min Flowrates | Northbound | | | | Southbound | | | | Eastbound | | | | Westbound | | | | Total | |
| | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | Left | Thru | Right | U | | |
| All Vehicles | 0 | 288 | 72 | 0 | 0 | 284 | 0 | 0 | 0 | 0 | 0 | 0 | 20 | 0 | 4 | 0 | 668 | |
| Heavy Trucks | 0 | 8 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 8 | |
| Buses | | | | | | | | | | | | | | | | | | |
| Pedestrians | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Bicycles | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | |
| Scooters | | | | | | | | | | | | | | | | | | |

Comments:

Appendix B

Seasonal Adjustment Calculations

SEASONAL ADJUSTMENT CALCULATIONS

Version 2 of the APM identifies three methods for identifying seasonal adjustment factors for highway traffic volumes. All three methods utilize information provided by Automatic Traffic Recorders (ATR) located in select locations throughout the State Highway System that collect traffic data 24-hours a day/365 days a year. Within the study area, ATR #30-019 is located on US 395 south of Hermiston while ATR #30-002 is located on US 730 east of Hermiston. Using the average of these two ATRs, the On-Site ATR Method was used to adjust the intersection turning movement counts to 30th highest hour conditions. The proposed seasonal adjustment factor calculations are summarized in the Table below.

Table 4: Seasonal Adjustment Factor Calculations

| | 2019 | 2018 | 2017 | 2016 | 2015 | Avg |
|-----------------------|------|------|------|------|------|----------------|
| ATR #30-002 | | | | | | |
| Peak Month (August) | 124 | 126 | 157 | 123 | 129 | 126.33% |
| Count Month (October) | 114 | 101 | 123 | 106 | 109 | 109.67% |
| ATR #30-019 | | | | | | |
| Peak Month (August) | 114 | 110 | 116 | 110 | 111 | 111.67% |
| Count Month (October) | 109 | 107 | 111 | 107 | 109 | 108.33% |

Source: ODOT 2020 Transportation Volume Tables. September 2021.

For ATR 30-002:

- The average peak month (August) is: $(124\% + 126\% + 129\%) / 3 = 126.33\%$
- The average count month (October) is: $(114\% + 106\% + 109\%) / 3 = 109.67\%$
- The seasonal adjustment factor is $126.33\% / 109.67\% = \mathbf{1.15}$

For ATR 30-019

- The average peak month (August) is: $(114\% + 110\% + 111\%) / 3 = 111.67\%$
- The average count month (October) is: $(109\% + 107\% + 109\%) / 3 = 108.33\%$
- The seasonal adjustment factor is $111.67\% / 108.33\% = \mathbf{1.03}$

Combined

- The season adjustment factor is $(1.15 + 1.03) / 2 = \mathbf{1.09}$

APPENDIX B

Cost Estimates for Infrastructure Improvements and Land Acquisition

**CITY OF HERMISTON, OREGON
UMATILLA RIVER BRIDGE PRELIMINARY ENGINEERING REPORT
ELM AVENUE TRAVEL CORRIDOR
PRELIMINARY COST ESTIMATE
YEAR 2022 COSTS**

| NO. | DESCRIPTION | UNIT | UNIT PRICE | ESTIMATED QUANTITY | TOTAL PRICE |
|--------------------------------------------|--------------------------------------------------------------|------|------------|--------------------|----------------------|
| 1 | Mobilization/Demobilization | LS | \$ 348,500 | All Req'd | \$ 348,500 |
| 2 | Temporary Protection and Direction of Traffic/Project Safety | LS | 40,000 | All Req'd | 40,000 |
| 3 | Land Acquisition (90-foot right-of-way) ¹ | LS | 2,271,000 | All Req'd | 2,271,000 |
| 4 | Concrete Curb and Gutter | LF | 30 | 28,900 | 867,000 |
| 5 | 5-foot Concrete Sidewalk | LF | 35 | 31,000 | 1,085,000 |
| 6 | Asphalt Concrete Pavement | TON | 130 | 11,900 | 1,547,000 |
| 7 | Aggregate Base | TON | 35 | 34,900 | 1,221,500 |
| 8 | Pavement Striping and Marking | LS | 20,000 | All Req'd | 20,000 |
| Total Estimated Construction Cost | | | | | \$ 7,400,000 |
| Design and Construction Engineering @ 20% | | | | | 1,480,000 |
| Contingencies @ 30% | | | | | 2,220,000 |
| TOTAL ESTIMATED PROJECT COST (2022) | | | | | \$ 11,100,000 |

¹ See Figures 4-1 and 4-3 in Chapter 4 for right-of-way acquisition information.

**CITY OF HERMISTON, OREGON
UMATILLA RIVER BRIDGE PRELIMINARY ENGINEERING REPORT
PUNKIN CENTER TRAVEL CORRIDOR
PRELIMINARY COST ESTIMATE
YEAR 2022 COSTS**

| NO. | DESCRIPTION | UNIT | UNIT PRICE | ESTIMATED QUANTITY | TOTAL PRICE |
|--------------------------------------------|--------------------------------------------------------------|------|------------|--------------------|----------------------|
| 1 | Mobilization/Demobilization | LS | \$ 406,500 | All Req'd | \$ 406,500 |
| 2 | Temporary Protection and Direction of Traffic/Project Safety | LS | 30,000 | All Req'd | 30,000 |
| 3 | Land Acquisition (90-foot right-of-way) ¹ | LS | 3,650,000 | All Req'd | 3,650,000 |
| 4 | Concrete Curb and Gutter | LF | 30 | 25,400 | 762,000 |
| 5 | 5-foot Concrete Sidewalk | LF | 35 | 25,400 | 889,000 |
| 6 | Asphalt Concrete Pavement | TON | 130 | 11,400 | 1,482,000 |
| 7 | Aggregate Base | TON | 35 | 33,300 | 1,165,500 |
| 8 | Pavement Striping and Marking | LS | 15,000 | All Req'd | 15,000 |
| Total Estimated Construction Cost | | | | | \$ 8,400,000 |
| Design and Construction Engineering @ 20% | | | | | 1,680,000 |
| Contingencies @ 30% | | | | | 2,520,000 |
| TOTAL ESTIMATED PROJECT COST (2022) | | | | | \$ 12,600,000 |

¹ See Figures 4-2 and 4-4 in Chapter 4 for right-of-way acquisition information.

**CITY OF HERMISTON, OREGON
UMATILLA RIVER BRIDGE PRELIMINARY ENGINEERING REPORT
ELM AVENUE BRIDGE¹
PRELIMINARY COST ESTIMATE
YEAR 2022 COSTS**

| NO. | DESCRIPTION | UNIT | UNIT PRICE | ESTIMATED QUANTITY | TOTAL PRICE |
|--------------------------------------------|-----------------------------------|------|------------|--------------------|----------------------|
| 1 | Mobilization/Demobilization | LS | \$ 610,000 | All Req'd | \$ 610,000 |
| 2 | Bridge Structure | LS | 11,150,000 | All Req'd | 11,150,000 |
| 3 | Earthwork (Cut/Fill) ² | CY | 30 | 38,000 | 1,140,000 |
| Total Estimated Construction Cost | | | | | \$ 12,900,000 |
| Design and Construction Engineering @ 20% | | | | | 2,600,000 |
| Contingencies @ 30% | | | | | 3,900,000 |
| TOTAL ESTIMATED PROJECT COST (2022) | | | | | \$ 19,400,000 |

¹ Estimate does not include sidewalk, asphalt, or aggregate base as estimated costs for these items are included in the Elm Avenue Travel Corridor cost estimate.

² Earthwork quantities were estimated using Figure 3-1 in Chapter 3. Assumes a cut/fill slope of 3:1.

**CITY OF HERMISTON, OREGON
UMATILLA RIVER BRIDGE PRELIMINARY ENGINEERING REPORT
PUNKIN CENTER BRIDGE¹
PRELIMINARY COST ESTIMATE
YEAR 2022 COSTS**

| NO. | DESCRIPTION | UNIT | UNIT PRICE | ESTIMATED QUANTITY | TOTAL PRICE |
|--------------------------------------------|-----------------------------------|------|------------|--------------------|----------------------|
| 1 | Mobilization/Demobilization | LS | \$ 450,000 | All Req'd | \$ 450,000 |
| 2 | Bridge Structure | LS | 8,950,000 | All Req'd | 8,950,000 |
| 3 | Earthwork (Cut/Fill) ² | CY | 30 | 10,000 | 300,000 |
| Total Estimated Construction Cost | | | | | \$ 9,700,000 |
| Design and Construction Engineering @ 20% | | | | | 1,900,000 |
| Contingencies @ 30% | | | | | 2,900,000 |
| TOTAL ESTIMATED PROJECT COST (2022) | | | | | \$ 14,500,000 |

¹ Estimate does not include sidewalk, asphalt, or aggregate base as estimated costs for these items are included in the Punkin Center Travel Corridor cost estimate.

² Earthwork quantities were estimated using Figure 3-2 in Chapter 3. Assumes a cut/fill slope of 3:1.

CITY OF HERMISTON, OREGON
UMATILLA RIVER BRIDGE PRELIMINARY ENGINEERING REPORT
U.S. 395/W. ELM AVENUE INTERSECTION IMPROVEMENTS
PRELIMINARY COST ESTIMATE
YEAR 2022 COSTS

| NO. | DESCRIPTION | UNIT | UNIT PRICE | ESTIMATED QUANTITY | TOTAL PRICE |
|---------------------------------------------------------------------------|--------------------------------------------------------------|------|------------|--------------------|----------------------|
| 1 | Mobilization/Demobilization | LS | \$ 345,800 | All Req'd | \$ 345,800 |
| 2 | Temporary Protection and Direction of Traffic/Project Safety | LS | 100,000 | All Req'd | 100,000 |
| 3 | Signal Modifications | LS | 800,000 | All Req'd | 800,000 |
| 4 | Americans with Disabilities Act Ramps | EA | 10,000 | 8 | 80,000 |
| 5 | Land Acquisition ¹ | LS | 5,636,000 | All Req'd | 5,636,000 |
| 6 | Structure Demolition | LS | 300,000 | All Req'd | 300,000 |
| Northbound, Southbound, and Westbound Right-Turn Lanes² | | | | | |
| 7 | Concrete Curb and Gutter | LF | 50 | 430 | 21,500 |
| 8 | 5-foot Concrete Sidewalk | LF | 40 | 430 | 17,200 |
| 9 | Asphalt Concrete Pavement | TON | 150 | 130 | 19,500 |
| 10 | Aggregate Base (12 inches) | TON | 40 | 250 | 10,000 |
| Total Estimated Construction Cost | | | | | \$ 7,330,000 |
| Design and Construction Engineering @ 20% | | | | | 1,470,000 |
| Contingencies @ 30% | | | | | 2,200,000 |
| TOTAL ESTIMATED PROJECT COST (2022) | | | | | \$ 11,000,000 |

¹ See Chapter 4 for discussion on land acquisition.

² Turn lane geometry for estimation purposes was derived using general design guidelines in the 2012 Highway Design Manual. A minimum storage length of 100 feet was assumed for comparison purposes, unless otherwise noted.

CITY OF HERMISTON, OREGON
UMATILLA RIVER BRIDGE PRELIMINARY ENGINEERING REPORT
N.W. 11TH STREET/W. ELM AVENUE INTERSECTION IMPROVEMENTS¹
PRELIMINARY COST ESTIMATE
YEAR 2022 COSTS

| NO. | DESCRIPTION | UNIT | UNIT PRICE | ESTIMATED QUANTITY | TOTAL PRICE |
|----------------------------------------------|--------------------------------------------------------------|------|------------|--------------------|-------------------|
| 1 | Mobilization/Demobilization | LS | \$ 28,100 | All Req'd | \$ 28,100 |
| 2 | Temporary Protection and Direction of Traffic/Project Safety | LS | 50,000 | All Req'd | 50,000 |
| 3 | Signal Modifications | LS | 400,000 | All Req'd | 400,000 |
| 4 | Americans with Disabilities Act Ramps | EA | 10,000 | 2 | 20,000 |
| Eastbound Right-Turn Lane² | | | | | |
| 5 | Concrete Curb and Gutter | LF | 50 | 260 | 13,000 |
| 6 | 5-foot Concrete Sidewalk | LF | 40 | 260 | 10,400 |
| 7 | Asphalt Concrete Pavement | TON | 150 | 70 | 10,500 |
| 8 | Aggregate Base (12 inches) | TON | 40 | 200 | 8,000 |
| Total Estimated Construction Cost | | | | | \$ 540,000 |
| Design and Construction Engineering @ 20% | | | | | 108,000 |
| Contingencies @ 30% | | | | | 162,000 |
| TOTAL ESTIMATED PROJECT COST (2022) | | | | | \$ 810,000 |

¹ Land acquisition was not accounted for in this cost estimate as it is already included in the Elm Avenue Travel Corridor cost estimate.

² Turn lane geometry for estimation purposes was derived using general design guidelines in the 2012 Highway Design Manual. A minimum storage length of 100 feet was assumed for comparison purposes, unless otherwise noted.

**CITY OF HERMISTON, OREGON
UMATILLA RIVER BRIDGE PRELIMINARY ENGINEERING REPORT
POWERLINE ROAD/BELLINGER ROAD¹
PRELIMINARY COST ESTIMATE
YEAR 2022 COSTS**

| NO. | DESCRIPTION | UNIT | UNIT PRICE | ESTIMATED QUANTITY | TOTAL PRICE |
|------------------------------------------------------------------------------|--------------------------------------------------------------|------|------------|--------------------|-------------------|
| 1 | Mobilization/Demobilization | LS | \$ 9,800 | All Req'd | \$ 9,800 |
| 2 | Temporary Protection and Direction of Traffic/Project Safety | LS | 30,000 | All Req'd | 30,000 |
| 3 | Americans with Disabilities Act Ramps | EA | 10,000 | 6 | 60,000 |
| Southbound Left-Turn Lane, and Westbound Right-Turn Lanes² | | | | | |
| 4 | Concrete Curb and Gutter | LF | 30 | 800 | 24,000 |
| 5 | 5-foot Concrete Sidewalk | LF | 35 | 800 | 28,000 |
| 6 | Asphalt Concrete Pavement | TON | 150 | 180 | 27,000 |
| 7 | Aggregate Base (12 inches) | TON | 35 | 520 | 18,200 |
| Total Estimated Construction Cost | | | | | \$ 197,000 |
| Design and Construction Engineering @ 20% | | | | | 39,000 |
| Contingencies @ 30% | | | | | 59,000 |
| TOTAL ESTIMATED PROJECT COST (2022) | | | | | \$ 295,000 |

¹ Land acquisition was not accounted for in this cost estimate as it is already included in the Elm Avenue Travel Corridor cost estimate.

² Turn lane geometry for estimation purposes was derived using general design guidelines in the 2012 Highway Design Manual. A minimum storage length of 100 feet was assumed for comparison purposes, unless otherwise noted.

CITY OF HERMISTON, OREGON
UMATILLA RIVER BRIDGE PRELIMINARY ENGINEERING REPORT
UMATILLA RIVER ROAD/W. PUNKIN CENTER ROAD
INTERSECTION IMPROVEMENTS¹
PRELIMINARY COST ESTIMATE
YEAR 2022 COSTS

| NO. | DESCRIPTION | UNIT | UNIT PRICE | ESTIMATED QUANTITY | TOTAL PRICE |
|----------------------------------------------------------|--------------------------------------------------------------|------|------------|--------------------|---------------------|
| 1 | Mobilization/Demobilization | LS | \$ 155,000 | All Req'd | \$ 155,000 |
| 2 | Temporary Protection and Direction of Traffic/Project Safety | LS | 50,000 | All Req'd | 50,000 |
| 3 | Traffic Signal | LS | 1,500,000 | All Req'd | 1,500,000 |
| 4 | Americans with Disabilities Act Ramps | EA | 10,000 | 4 | 40,000 |
| 5 | Land Acquisition ² | LS | 48,000 | All Req'd | 48,000 |
| 6 | Modernize/Upgrade Railroad Crossing | LS | 800,000 | All Req'd | 800,000 |
| Left-Turn Lane on All Four Approaches³ | | | | | |
| 7 | Concrete Curb and Gutter | LF | 30 | 3,300 | 99,000 |
| 8 | 5-foot Concrete Sidewalk | LF | 35 | 3,300 | 115,500 |
| 9 | Asphalt Concrete Pavement | TON | 130 | 1,700 | 221,000 |
| 10 | Aggregate Base (12 inches) | TON | 35 | 4,900 | 171,500 |
| Total Estimated Construction Cost | | | | | \$ 3,200,000 |
| Design and Construction Engineering @ 20% | | | | | 640,000 |
| Contingencies @ 30% | | | | | 960,000 |
| TOTAL ESTIMATED PROJECT COST (2022) | | | | | \$ 4,800,000 |

¹ Asphalt through the travel corridor is not included in this estimate as it is already included in the Punkin Center Travel Corridor cost estimate.

² Land acquisition was accounted for only on the southbound approach since land acquisition for the eastbound and westbound approaches is already included in the Punkin Center Travel Corridor cost estimate. Land acquisition for the northbound approach was not included as the land is owned by the City of Hermiston.

³ Turn lane geometry for estimation purposes was derived using general design guidelines in the 2012 Highway Design Manual. A minimum storage length of 100 feet was assumed for comparison purposes, unless otherwise noted.

CITY OF HERMISTON, OREGON
UMATILLA RIVER BRIDGE PRELIMINARY ENGINEERING REPORT
POWERLINE ROAD/COUNTRY LANE INTERSECTION IMPROVEMENTS¹
PRELIMINARY COST ESTIMATE
YEAR 2022 COSTS

| NO. | DESCRIPTION | UNIT | UNIT PRICE | ESTIMATED QUANTITY | TOTAL PRICE |
|-----------------------------------------------------------------------------|--------------------------------------------------------------|------|------------|--------------------|-------------------|
| 1 | Mobilization/Demobilization | LS | \$ 9,700 | All Req'd | \$ 9,700 |
| 2 | Temporary Protection and Direction of Traffic/Project Safety | LS | 20,000 | All Req'd | 20,000 |
| 3 | Americans with Disabilities Act Ramps | EA | 10,000 | 3 | 30,000 |
| 4 | Land Acquisition ² | LS | 45,000 | All Req'd | 45,000 |
| Southbound Turn-Lane Westbound Left and Right-Turn Lanes³ | | | | | |
| 5 | Asphalt Concrete Pavement | TON | 150 | 330 | 49,500 |
| 6 | Aggregate Base (12 inches) | TON | 40 | 970 | 38,800 |
| Total Estimated Construction Cost | | | | | \$ 193,000 |
| Design and Construction Engineering @ 20% | | | | | 39,000 |
| Contingencies @ 30% | | | | | 58,000 |
| TOTAL ESTIMATED PROJECT COST (2022) | | | | | \$ 290,000 |

¹ Asphalt through the travel corridor is not included in this estimate as it is already included in the Punkin Center Travel Corridor cost estimate.

² Land acquisition in addition to what was accounted for in the Punkin Center Travel Corridor cost estimate, is included to account for re-routing Country Lane through private property.

³ Turn lane geometry for estimation purposes was derived using general design guidelines in the 2012 Highway Design Manual. A minimum storage length of 100 feet was assumed for comparison purposes, unless otherwise noted. Curb, gutter, and sidewalk is already accounted for in the Punkin Center Travel Corridor cost estimate.

CITY OF HERMISTON, OREGON
UMATILLA RIVER BRIDGE PRELIMINARY ENGINEERING REPORT
U.S. 395/PUNKIN CENTER ROAD INTERSECTION IMPROVEMENTS¹
PRELIMINARY COST ESTIMATE
YEAR 2022 COSTS

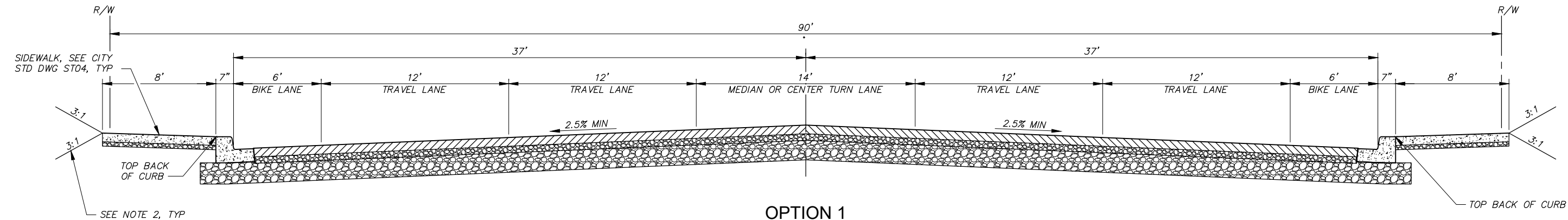
| NO. | DESCRIPTION | UNIT | UNIT PRICE | ESTIMATED QUANTITY | TOTAL PRICE |
|----------------------------------------------|--------------------------------------------------------------|------|------------|--------------------|-------------------|
| 1 | Mobilization/Demobilization | LS | \$ 8,900 | All Req'd | \$ 8,900 |
| 2 | Temporary Protection and Direction of Traffic/Project Safety | LS | 40,000 | All Req'd | 40,000 |
| 3 | Americans with Disabilities Act Ramps | EA | 10,000 | 2 | 20,000 |
| 4 | Concrete Island | LS | 30,000 | All Req'd | 30,000 |
| 5 | Utility Relocation | LS | 30,000 | All Req'd | 30,000 |
| Eastbound Right-Turn Lane² | | | | | |
| 6 | Concrete Curb and Gutter | LF | 50 | 260 | 13,000 |
| 7 | 5-foot Concrete Sidewalk | LF | 35 | 260 | 9,100 |
| 8 | Asphalt Concrete Pavement | TON | 200 | 70 | 14,000 |
| 9 | Aggregate Base (12 inches) | TON | 40 | 200 | 8,000 |
| Total Estimated Construction Cost | | | | | \$ 173,000 |
| Design and Construction Engineering @ 20% | | | | | 35,000 |
| Contingencies @ 30% | | | | | 52,000 |
| TOTAL ESTIMATED PROJECT COST (2022) | | | | | \$ 260,000 |

¹ Land acquisition was not included in this cost estimate as it is already included in the Punkin Center Travel Corridor cost estimate.

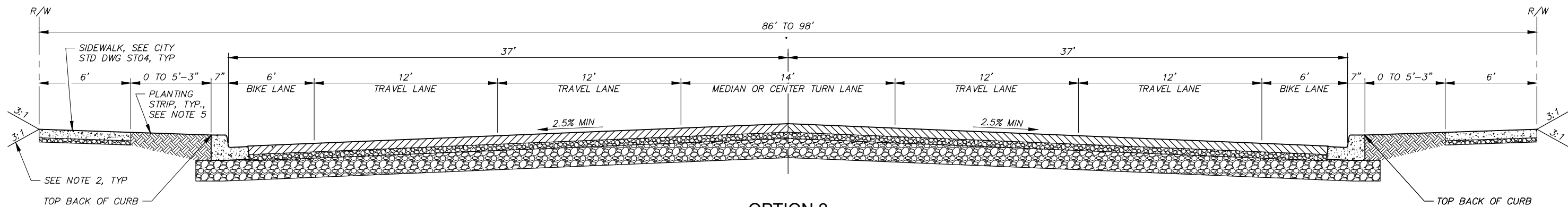
² Turn lane geometry for estimation purposes was derived using general design guidelines in the 2012 Highway Design Manual. A minimum storage length of 100 feet was assumed for comparison purposes, unless otherwise noted.

APPENDIX C

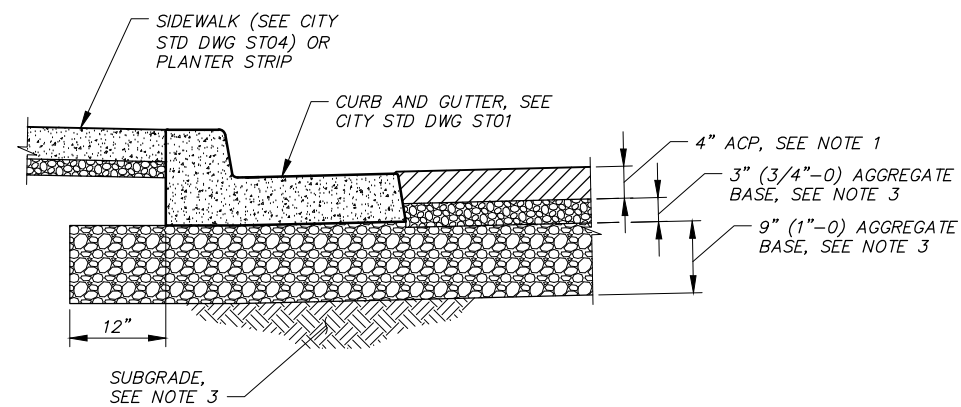
City of Hermiston, Oregon, Major Arterial Standard Drawing



OPTION 1
(IN CENTRAL BUSINESS DISTRICT)
NTS



OPTION 2
(OUTSIDE CENTRAL BUSINESS DISTRICT)
NTS



PAVEMENT SECTION
NTS

- NOTES
- 1. ASPHALT CONCRETE PAVEMENT SHALL BE CONSTRUCTED IN 2 EQUAL 2" LIFTS.
 - 2. SLOPES SHALL NOT EXCEED 3:1 UNLESS APPROVED BY THE CITY.
 - 3. COMPACT AGGREGATE BASE AND SUBGRADE TO 95% MAXIMUM DRY DENSITY PER ASTM D1557.
 - 4. THE DISTANCE BETWEEN TOP BACK OF CURBS IS A CRITICAL DIMENSION. CURBS POURED IN AN INCORRECT LOCATION SHALL BE REMOVED AND REPLACED BY THE CONTRACTOR.
 - 5. TREES ARE PROHIBITED IN THE PLANTING STRIP.

| REVISION | DATE |
|----------|------|
| | |
| | |
| | |
| | |



CITY OF
HERMISTON, OREGON
STANDARD DRAWINGS

URBAN MAJOR ARTERIAL
CROSS SECTIONS

FIGURE
ST07

Q:\HERMISTON\736-94_CityStandardDwgs\736-94_CityStandardDwgs UPDATED\Drafting\736-94-010ST-MAJ_ARTR-CROSS-SEC.dwg, FIG ST07, 10/22/2021 10:58 AM, GSaurbier



Where Life is Sweet

Mayor and Members of the City Council
STAFF REPORT
For the Meeting of September 26, 2022

Title/Subject

August 2022 Financial Report

Summary and Background

This is the monthly overview of the previous month's financial position of the city.

Tie-In to Council Goals

Fiscal Prudence

Fiscal Information

None

Alternatives and Recommendation

Alternatives

NA

Recommended Action/Motion

Request a motion to approve/accept the August 2022 Financial Report as presented.

Submitted By:

Mark Krawczyk

August 2022 Financial Report



Department of Finance
August 2022
(Unaudited)

FY2022-2023 Monthly Financial Report

Section 8, ItemB.

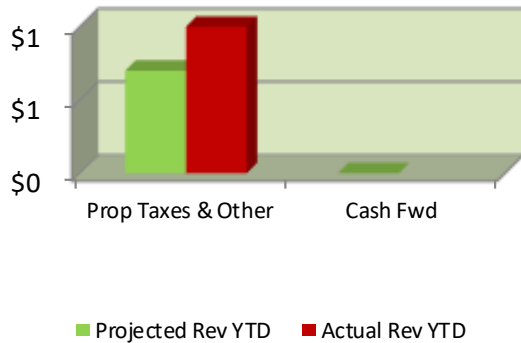
Hermiston Urban Renewal Agency (HURA)

For the Month Ending August 31, 2022

Resources

Through August 31, 2022

by Category



| | Annual Proj Rev | Actual Rev (YTD | % Var |
|--------------------|--------------------|---------------------|-----------|
| Prop Taxes & Other | \$ 271 | 1 | 0% |
| Cash Fwd | \$ 375 | | 0% |
| Total | \$ 646 | \$ 1 | 0% |

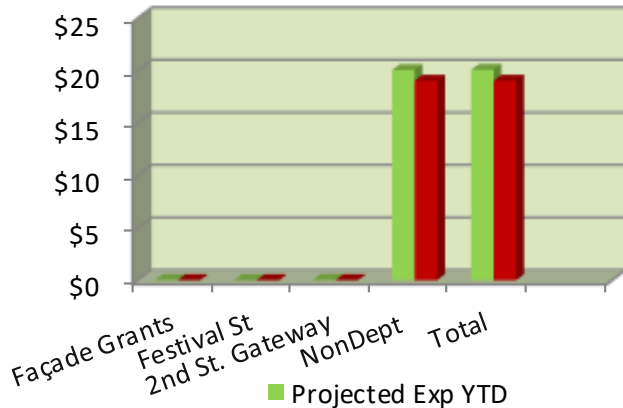
Note: Variance is calculated as % of revenue YTD

Expenditures

Through August 31, 2022

by Character

(in \$1,000)



| | Annual Proj Exp | Actual Exp (YTD | % Var |
|-----------------|--------------------|---------------------|-----------|
| Façade Grants | \$ 50 | 0 | 0% |
| Festival St | \$ 5 | 0 | 0% |
| 2nd St. Gateway | \$ 400 | 0 | 0% |
| NonDept | \$ 191 | 19 | 10% |
| Total | \$ 646 | \$ 19 | 3% |

Note: variance is calculated as % of expenses YTD.

The FY2022-23 budget for the Urban Renewal Agency is \$ 645,662. This includes \$50,000 for Façade Grants, \$5,000 for the Festival Street, \$400,000 for construction of the 2nd Street Gateway project, and \$190,662 for Non-Departmental expenses.

Property tax revenues of \$883.00 were collected in July and sent the first week of August, 2022. The semi-annual debt service (interest payment of \$18,781) was paid this month.

FY2022-2023 Monthly Financial Report

Section 8, ItemB.

City of Hermiston, Oregon

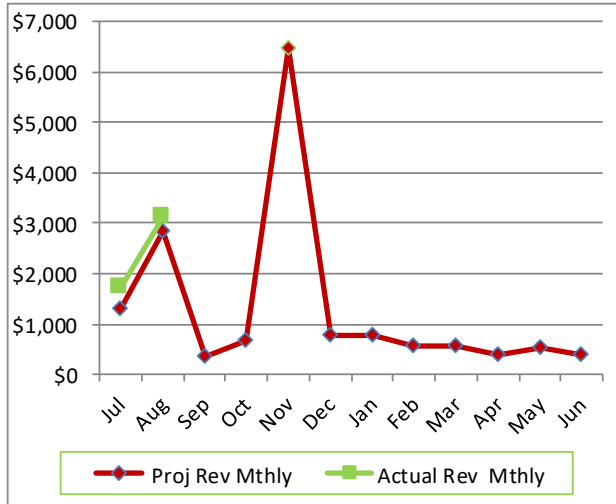
General Fund Resources

For the Month Ending August 31, 2022

General Fund Resources Summary

Through August 31, 2022

(in \$1,000)



| | Proj Rev Mthly | Rev Proj | Actual Rev Mthly | Var Fav/ (Unfav) | % Var |
|------------------|-------------------|------------------|---------------------|---------------------|-------------|
| Jul | \$ 1,298 | \$ 1,298 | \$ 1,731 | \$ 433 | 33% |
| Aug | \$ 2,834 | \$ 2,834 | \$ 3,133 | \$ 299 | 11% |
| Sep | \$ 342 | \$ 342 | | \$ - | 0% |
| Oct | \$ 675 | \$ 675 | | \$ - | 0% |
| Nov | \$ 6,472 | \$ 6,472 | | \$ - | 0% |
| Dec | \$ 781 | \$ 781 | | \$ - | 0% |
| Jan | \$ 762 | \$ 762 | | \$ - | 0% |
| Feb | \$ 557 | \$ 557 | | \$ - | 0% |
| Mar | \$ 573 | \$ 573 | | \$ - | 0% |
| Apr | \$ 379 | \$ 379 | | \$ - | 0% |
| May | \$ 544 | \$ 544 | | \$ - | 0% |
| Jun | \$ 384 | \$ 384 | | \$ - | 0% |
| Total YTD | 15,604 | 15,604 | 4,864 | 732 | 4.7% |
| Cash Fwd | 1,102 | - | - | - | 0% |
| Total | \$ 16,706 | \$ 15,604 | \$ 4,864 | 732 | 4.7% |

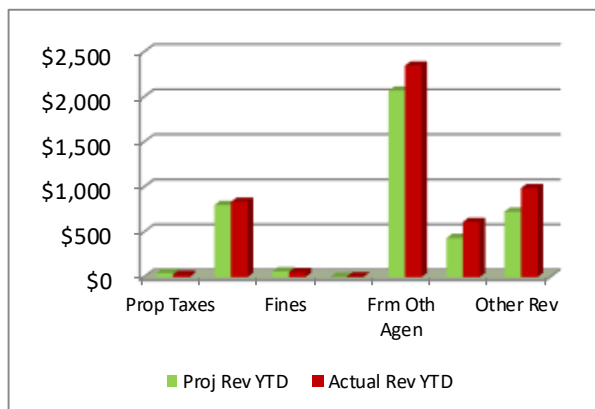
Estimated General Fund revenues for the 2022-23 fiscal year are **\$15,603,889**. Projected revenues for August were **\$2,834,253**, compared to actual revenues of **\$3,132,754**, a positive variance of **\$298,501**. The fund is currently **\$730,759** more than projected through August, or, **10.53% more** than projections for the month; and **4.7% more** for the fiscal year after two months.

General Fund - All Resources

Through August 31, 2022

by Category

(in \$1,000)



| | Annual Proj Rev | Proj Rev YTD | Actual Rev YTD | Var Fav/ (Unfav) | % Var |
|---------------|--------------------|-----------------|-------------------|---------------------|--------------|
| Prop Taxes | \$ 6,787 | \$ 44 | \$ 25 | \$ (19) | -43% |
| Lic & Fran | \$ 1,435 | \$ 798 | 835 | 37 | 5% |
| Fines | \$ 400 | \$ 63 | 53 | (9) | -15% |
| Interest Rev | \$ 15 | \$ 2 | 6 | 3 | 157% |
| Frmm Oth Agen | \$ 3,013 | \$ 2,068 | 2,347 | 279 | 13% |
| Svc Chgs | \$ 1,904 | \$ 433 | 611 | 178 | 41% |
| Other Rev | \$ 2,051 | \$ 724 | 987 | 262 | 36% |
| Cash Fwd | \$ 1,102 | \$ - | - | - | 0% |
| Total | \$16,706 | \$ 4,133 | \$ 4,864 | \$ 731 | 17.7% |

Note: variance is calculated as a percent of the projected revenue YTD.

FY2022-2023 Monthly Financial Report

Section 8, ItemB.

City of Hermiston, Oregon

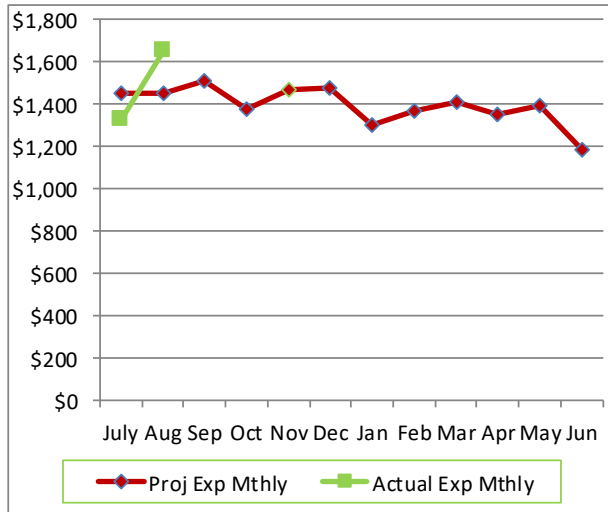
General Fund Expenditures

For the Month Ending August 31, 2022

General Fund Expenditure Summary

Through August 31, 2022

(in \$1,000)



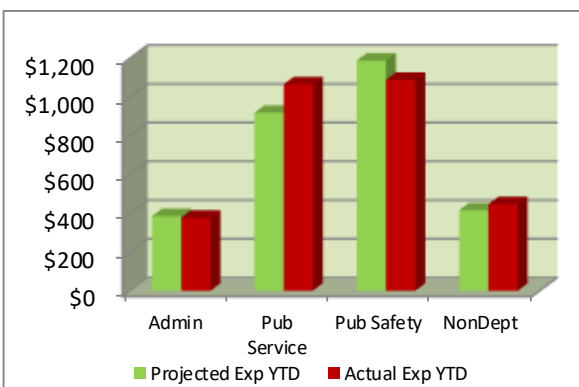
| | Proj Exp Mthly | Proj Exp | Actual Exp Mthly | Var Fav/ (Unfav) | % Var |
|------------------|------------------|------------------|------------------|------------------|--------------|
| July | \$ 1,454 | \$ 1,454 | \$ 1,324 | \$ 130 | 9% |
| Aug | \$ 1,453 | \$ 1,453 | \$ 1,653 | \$ (200) | -14% |
| Sep | \$ 1,512 | \$ 1,512 | | \$ - | 0% |
| Oct | \$ 1,377 | \$ 1,377 | | \$ - | 0% |
| Nov | \$ 1,469 | \$ 1,469 | | \$ - | 0% |
| Dec | \$ 1,477 | \$ 1,477 | | \$ - | 0% |
| Jan | \$ 1,304 | \$ 1,304 | | \$ - | 0% |
| Feb | \$ 1,367 | \$ 1,367 | | \$ - | 0% |
| Mar | \$ 1,409 | \$ 1,409 | | \$ - | 0% |
| Apr | \$ 1,355 | \$ 1,355 | | \$ - | 0% |
| May | \$ 1,398 | \$ 1,398 | | \$ - | 0% |
| Jun | \$ 1,183 | \$ 1,183 | | \$ - | 0% |
| Total YTD | 16,758 | 16,758 | 2,977 | (70) | -0.4% |
| Contngcy | | - | - | - | 0% |
| Total | \$ 16,758 | \$ 16,758 | \$ 2,977 | \$ (70) | -0.4% |

Projected General Fund monthly expenditures for August were **\$1,452,775**. Actual expenditures were **\$1,653,099**, which is **\$200,324** more than projected for a **negative variance of 13.7%** for the month, but only a **negative 2.4%** year-to-date.

General Fund Expenditures by Consolidated Department

Through August 31, 2022

(in \$1,000)



| | Annual Proj Exp | Projected Exp YTD | Actual Exp YTD | Var Fav/ (Unfav) | % Var |
|--------------|-----------------|-------------------|-----------------|------------------|--------------|
| Admin | \$ 2,164 | 385 | 377 | 8 | 2% |
| Pub Service | 4,985 | 918 | 1,065 | (147) | -16% |
| Pub Safety | 7,066 | 1,188 | 1,088 | 100 | 8% |
| NonDept | 2,492 | 415 | 447 | (31) | -8% |
| Unapp | - | - | | - | 0% |
| Total | \$16,706 | \$ 2,907 | \$ 2,977 | \$ (70) | -2.4% |

Note: variance is calculated as a percent of the projected expenditures YTD.

FY2022-2023 Monthly Financial Report

Section 8, ItemB.

General Fund Expenditure Detail For the Month Ending August 31, 2022

General Fund Expenditures by Department

| | Annual Projected Exp | Projected Exp YTD | Actual Exp YTD | Var Fav/ (Unfav) | % Var Fav/ (Unfav) |
|------------------------------|-------------------------|-------------------|------------------|---------------------|-----------------------|
| City Council | 67,027 | 35,020 | 23,381 | 11,639 | 33% |
| City Manager/Legal | 1,010,363 | 168,394 | 165,771 | 2,623 | 2% |
| City Planning | 431,372 | 71,895 | 69,604 | 2,291 | 3% |
| Finance | 654,929 | 109,976 | 118,119 | (8,143) | -7% |
| Total Administration | 2,163,691 | 385,285 | 376,875 | 8,410 | 2% |
| Transportation | 297,000 | 35,462 | 68,366 | (32,904) | -93% |
| Airport | 339,000 | 40,589 | 70,272 | (29,683) | -73% |
| Bldg Inspection | 600,157 | 98,618 | 89,663 | 8,955 | 9% |
| Parks | 731,981 | 135,382 | 154,421 | (19,039) | -14% |
| Parks/Utility Landscaping | 66,861 | 12,449 | 8,843 | 3,606 | 29% |
| Pool | 585,615 | 206,620 | 275,851 | (69,231) | -34% |
| Municipal Buildings | 145,173 | 24,196 | 24,418 | (223) | -1% |
| Library | 1,024,415 | 163,400 | 147,246 | 16,154 | 10% |
| Recreation | 874,159 | 150,116 | 197,315 | (47,199) | -31% |
| Community Center | 256,122 | 43,255 | 20,844 | 22,411 | 52% |
| Harkenrider Center | 64,064 | 8,017 | 7,915 | 102 | 1% |
| Total Public Services | 4,984,547 | 918,104 | 1,065,154 | (147,050) | -16% |
| Court | 833,378 | 160,969 | 101,038 | 59,931 | 37% |
| Public Safety Center | 62,000 | 7,453 | 7,599 | (146) | -2% |
| Police Operations | 6,169,885 | 1,019,513 | 979,533 | 39,980 | 4% |
| Total Public Safety | 7,065,263 | 1,187,935 | 1,088,170 | 99,765 | 8% |
| Non-Departmental | 2,492,087 | 415,348 | 446,660 | (31,312) | -8% |
| Unappropriated | 0 | 0 | 0 | 0 | 0% |
| Total Non-Dept | 2,492,087 | 415,348 | 446,660 | (31,312) | -8% |
| Total | 16,705,588 | 2,906,671 | 2,976,859 | (70,188) | -2.4% |

For August of FY2023, **Total Administration** is \$, \$8,410 less than projected for the month. **Total Public Services** is \$147,050 more than projected this month. **Public Safety** is \$99,765 less than projected for August. **Non-Departmental** is \$31,312 more than projected for the month. The total **General Fund expenses** are **\$70,188 more than projected (2.4%)** for the first two months of FY2023.

FY2022-2023 Monthly Financial Report

Section 8, ItemB.

Fund Balance - General Fund

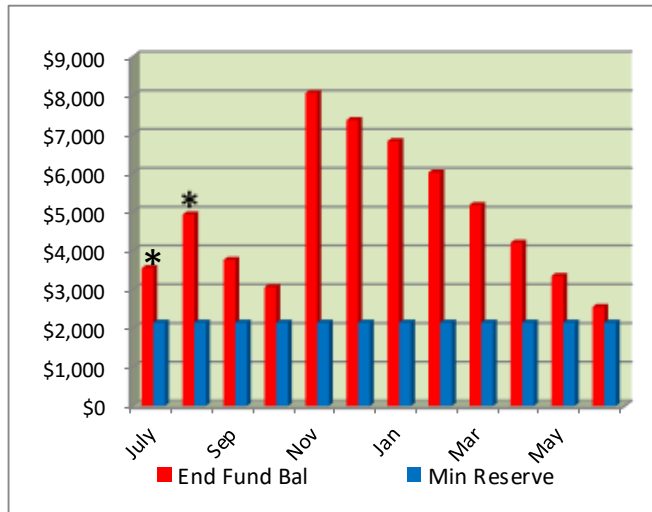
For the Month Ending August 31, 2022

General Fund

Through August 31, 2022

Ending Fund Balance

(in \$1,000)



Minimum Reserve = \$2,264,700

| | Begin Fund Bal | Revenue | Expense | End Fund Bal | ACT/PROJ Fund Bal |
|-------|----------------|-----------|-------------|--------------|-------------------|
| July | \$ 3,144 | \$ 1,731 | \$ (1,324) | \$ 3,551 | ACT* |
| Aug | \$ 3,551 | \$ 2,834 | \$ (1,453) | \$ 4,933 | ACT* |
| Sep | \$ 4,933 | \$ 342 | \$ (1,512) | \$ 3,763 | PROJ |
| Oct | \$ 3,763 | \$ 675 | \$ (1,377) | \$ 3,061 | PROJ |
| Nov | \$ 3,061 | \$ 6,472 | \$ (1,469) | \$ 8,065 | PROJ |
| Dec | \$ 8,065 | \$ 781 | \$ (1,477) | \$ 7,368 | PROJ |
| Jan | \$ 7,368 | \$ 762 | \$ (1,304) | \$ 6,826 | PROJ |
| Feb | \$ 6,826 | \$ 557 | \$ (1,367) | \$ 6,016 | PROJ |
| Mar | \$ 6,016 | \$ 573 | \$ (1,409) | \$ 5,179 | PROJ |
| Apr | \$ 5,179 | \$ 379 | \$ (1,355) | \$ 4,204 | PROJ |
| May | \$ 4,204 | \$ 544 | \$ (1,398) | \$ 3,351 | PROJ |
| June | \$ 3,351 | \$ 384 | \$ (1,183) | \$ 2,552 | PROJ |
| Total | \$ 3,144 | \$ 16,035 | \$ (16,627) | \$ 2,552 | |

The General Fund balance at the end of August for FY2023 is approximately **\$4,933,000**, which is **\$2,668,300** more than the current Minimum Reserve requirement of **\$2,264,700** (or **2.18 times** the Minimum Reserve requirement.)

The General Fund reserve policy is to maintain 15% fund balance of expenditures based on the prior fiscal year activity.

FY2022-2023 Monthly Financial Report

Section 8, ItemB.

Special Revenue Funds Report

For the Month Ending August 31, 2022

Special Revenue Funds

Resources & Requirements

| | 2022-23 Annual Budget | Actual YTD | Remaining Budget |
|---------------------------------------------|--------------------------|------------|---------------------|
| 02 Bonded Debt Fund | | | |
| Resources | 634,018 | 53,420 | 580,598 |
| Expenditures | 531,720 | 309,500 | 222,220 |
| Unappropriated Balance | 102,298 | N/A | N/A |
| 05 Transient Room Tax (TRT) | | | |
| Resources | 998,800 | 301,523 | 697,277 |
| Expenditures | 998,800 | 82,648 | 916,152 |
| Unappropriated Balance | - | N/A | N/A |
| 08 Reserve Fund | | | |
| Resources | 15,468,683 | 2,181,726 | 13,286,957 |
| Expenditures | 14,374,289 | 183,662 | 14,190,627 |
| Unappropriated Balance | 1,094,394 | N/A | N/A |
| 11 Miscellaneous Special Revenue | | | |
| Resources | 141,000 | 13,839 | 127,161 |
| Expenditures | 141,000 | 25,828 | 115,172 |
| Unappropriated Balance | - | N/A | N/A |
| 19 Christmas Express Special Revenue | | | |
| Resources | 35,000 | - | 35,000 |
| Expenditures | 30,000 | - | 30,000 |
| Unappropriated Balance | 5,000 | N/A | N/A |
| 20 Law Enforcement Special Revenue | | | |
| Resources | 84,014 | 4,295 | 79,719 |
| Expenditures | 64,014 | 1,650 | 62,364 |
| Unappropriated Balance | 20,000 | N/A | N/A |
| 21 Library Special Revenue | | | |
| Resources | 33,000 | 50 | 32,950 |
| Expenditures | 3,300 | 500 | 2,800 |
| Unappropriated Balance | 29,700 | N/A | N/A |
| 23 Enterprise Zone Project Fund | | | |
| Resources | 1,648,699 | 500,000 | 1,148,699 |
| Expenditures | 885,325 | 885,325 | - |
| Unappropriated Balance | 763,374 | N/A | N/A |
| 25 EOTEC Operations | | | |
| Resources | 627,793 | 11,680 | 616,113 |
| Expenditures | 627,793 | 118,485 | 509,308 |
| Unappropriated Balance | | N/A | N/A |
| 26 IT Services | | | |
| Resources | 670,800 | - | 670,800 |
| Expenditures | 670,800 | 100,745 | 570,055 |
| Unappropriated Balance | | N/A | N/A |

Beginning with the 2016-17 fiscal year the City began distinguishing within the funds some part of ending fund balance as contingency and some as reserved for future expenditure. The contingency is included in appropriations while the reserve for future expenditures is unappropriated.

The City uses multiple Special Revenue funds to account for revenues that are restricted to expenditure for particular purposes. They include funds for debt service, economic development, parks and recreation, capital projects, and grants. Since these funds are not operational in nature and used for specific purposes from year-to-year, their expenditures do not typically follow a predictable pattern so budget variances are not calculated for them.

FY2022-2023 Monthly Financial Report

Utility and Street Funds Report

For the Month Ending August 31, 2022

Utility and Street Funds Report

Resources & Expenditures

| | 2021-22 | | Variance | | |
|-------------------------------|---------------|---------------|------------|-------------|------------|
| | Annual Budget | Projected YTD | Actual YTD | Fav/(Unfav) | % Variance |
| 04 Street Fund | | | | | |
| Resources | 1,843,007 | 300,861 | 238,882 | (61,979) | -21% |
| Expenditures | 1,813,007 | 302,168 | 250,701 | 51,467 | 17% |
| Contingency | 30,000 | N/A | N/A | N/A | N/A |
| 06 Utility Fund | | | | | |
| Resources | 11,806,391 | 1,795,917 | 2,570,128 | 774,211 | 43% |
| Expenditures | 9,650,089 | 1,608,348 | 1,529,206 | 79,142 | 5% |
| Contingency | 2,156,302 | N/A | N/A | N/A | N/A |
| 13 HES Fund | | | | | |
| Resources | 12,495,134 | 1,635,000 | 1,797,211 | 162,211 | 10% |
| Expenditures | 9,945,430 | 1,657,572 | 1,786,510 | (128,938) | -8% |
| Contingency | 2,549,704 | N/A | N/A | N/A | N/A |
| 15 Regional Water Fund | | | | | |
| Resources | 1,874,653 | 225,167 | 120,507 | (104,660) | -46% |
| Expenditures | 1,206,031 | 201,005 | 187,290 | 13,715 | 7% |
| Contingency | 668,622 | N/A | N/A | N/A | N/A |

All four of these funds are projected on a straight line, 1/12th of budgeted expense or revenue per month.

Revenues for the **Street Fund** are **\$61,979** less than projected. Expenditures are **\$51,467** below projected.

Revenues in the **Utility Funds** are **\$774,211** above projected. Expenditures are **\$79,412** below projected.

The **HES Fund** revenue is **\$162,211** above projected. Expenditures are **\$128,938** more than projected.

The **Regional Water Fund** revenues are **\$104,660** below projected. Expenditures-for July are **\$13,715** below projected.

FY2021-2022 Monthly Financial Report

Section 8, ItemB.

City of Hermiston, Oregon Utilities/Streets Capital Projects Report For the Month Ending August 31, 2022

| | 2022-2023 Budget | YTD Expenditures | Project Budget | Project To-Date Expenditures | % Complete |
|-----------------------------------------|---------------------|---------------------|---------------------|---------------------------------|---------------|
| Geer & Harper Re-alignment | \$ 125,000 | \$ - | \$ 1,500,000 | \$ 11,062 | 0.74% |
| N. 1st Place Reconstruction | \$ 4,500,000 | \$ 21,956 | \$ 4,500,000 | \$ 103,956 | 2.31% |
| Well #6 Backup Generator | \$ 320,000 | \$ - | | \$ 41,929 | |
| N. 1st Place Water Line Replacement | \$ 100,000 | \$ 19,078 | \$ 800,000 | \$ 45,086 | 5.64% |
| Gladys & Main Waterline Replacement | \$ 100,000 | \$ - | \$ 1,180,000 | \$ - | 0.00% |
| Lift Station #6 Reconstruction | \$ 620,000 | \$ 5,143 | \$ 620,000 | \$ 36,731 | 5.92% |
| McDonald's & 395 Sewer Main Replacement | \$ 330,000 | \$ - | \$ 330,000 | \$ 22,000 | 6.67% |
| Lift Station #3 Upgrade | \$ 550,000 | \$ 6,691 | \$ 900,000 | \$ 365,765 | 40.64% |
| Total | \$ 6,645,000 | \$ 52,868 | \$ 9,830,000 | \$ 626,529 | 6.37% |

Geer & Harper Re-alignment

This intersection needs to be reconstructed to improve traffic/pedestrian access and ensure connectivity to property east of N. 1st Place. The current configuration makes access difficult for large trucks and traffic backs up on Harper Road during busy times during the day.

Current Update: No change in the status of this project. Still waiting for additional right-of-way acquisition.

North 1st Place Reconstruction

North 1st Place is a critical secondary north/south arterial in Hermiston's transportation system. The existing road is cracking, there are no sidewalks and there is unrestricted access to the roadway from parking and undeveloped areas.

Current Update: *Contractors began work to pave the gravel portion of NW 3rd to be used as a detour during the main project. It is still anticipated that bidding for the main project will take place in October/November, with construction expected early Spring '23*

Well #6 Backup Generator

Well No. 6 is the primary well and the only well available that could provide minimal water supply to lower pressure zones in the city during a power outage. The current pump was last repaired in 1994, and the backup pump lacks the capacity to service the pressure zone. The pump covers service area including Walmart Distribution Center, Eastern Oregon Higher Education Center, Pioneer Seed, and EOTEC. Preventative maintenance will avoid significant inconvenience and loss of water to area businesses.

Current Update: Delivery & Installation still on-track for the winter due to supply-chain issues.

North 1st Place Water Line Replacement

This project will eliminate failing, deteriorating lines which will minimize future repair work. It will also eliminate steel pipes from the 1920's and will improve public safety by ensuring no lead pipes.

Current Update: Design work continues in conjunction with the N 1st Place Roadway Project.

FY2021-2022 Monthly Financial Report

Section 8, Item B.

City of Hermiston, Oregon Utilities/Streets Capital Projects Report For the Month Ending August 31, 2022

| | 2022-2023 Budget | YTD Expenditures | Project Budget | Project To-Date Expenditures | % Complete |
|-----------------------------------------|---------------------|---------------------|---------------------|---------------------------------|---------------|
| Geer & Harper Re-alignment | \$ 125,000 | \$ - | \$ 1,500,000 | \$ 11,062 | 0.74% |
| N. 1st Place Reconstruction | \$ 4,500,000 | \$ 21,956 | \$ 4,500,000 | \$ 103,956 | 2.31% |
| Well #6 Backup Generator | \$ 320,000 | \$ - | | \$ 41,929 | |
| N. 1st Place Water Line Replacement | \$ 100,000 | \$ 19,078 | \$ 800,000 | \$ 45,086 | 5.64% |
| Gladys & Main Waterline Replacement | \$ 100,000 | \$ - | \$ 1,180,000 | \$ - | 0.00% |
| Lift Station #6 Reconstruction | \$ 620,000 | \$ 5,143 | \$ 620,000 | \$ 36,731 | 5.92% |
| McDonald's & 395 Sewer Main Replacement | \$ 330,000 | \$ - | \$ 330,000 | \$ 22,000 | 6.67% |
| Lift Station #3 Upgrade | \$ 550,000 | \$ 6,691 | \$ 900,000 | \$ 365,765 | 40.64% |
| Total | \$ 6,645,000 | \$ 52,868 | \$ 9,830,000 | \$ 626,529 | 6.37% |

Gladys & Main Waterline Replacement

Sections of this water distribution system were installed in the 1920s with steel pipe – some of the oldest pipe in the city water system. This project will eliminate failing, deteriorating and undersized line, reduce repair work, and improve public safety.

Current Update: Nothing to report this month.

Lift Station #6 Reconstruction

Lift station #6 was built in 1981. The submersible duplex pump station has experienced corrosion of both metal and brick. Replacement of the wetwell will increase capacity, eliminate porous brick, and safeguard the environment. Ventilation of the structure is inadequate and electrical systems are nearing the end of their useful life.

Current Update: The contractor mobilized on-site the first week of August and has begun work, with closures to N 1st Place occurring for approximately 2 months.

McDonald's & 395 Sewer Main Replacement

Segments of this piping have been found to have structural failures in multiple locations and failing joints. These line failures cause pipe blockage and requires monthly cleaning. In addition, pipe failures may be allowing sewage to exfiltrate the surrounding soil. A new sewer line would eliminate current monthly maintenance costs and provide watertight lines to protect existing groundwater resources from contamination.

Current Update: Work continues to secure the necessary easement through the property owner's attorney, George Anderson.

Lift Station #3 Upgrade

Lift Station No. 3 was built in approximately 1981 and is located underneath Highway 395. The pump and electrical systems need to be replaced. This project calls for the install of duplex pumps and upgraded electrical and control systems. It also will include evaluation of extending a gravity line to Lift Station No. 6, which is also in poor shape. Extending the line under Union Pacific Railroad tracks may be more cost effective than replacing both lift stations. Traffic control will be necessary during design phase

Current Update: The Contractor has remobilized on-site, begun digging pits to place boring equipment, and preparing to bore.

FY2022-23 Monthly Financial Report

Section 8, ItemB.

City of Hermiston, Oregon Other City Capital Projects Report For the Month Ending August 31, 2022

| | 2022-2023 Budget | YTD Expenditures | Project Budget | Project To-Date Expenditures | % Complete |
|----------------------------|---------------------|---------------------|-------------------|---------------------------------|---------------|
| New City Hall Construction | \$ 7,348,498 | \$ 806,396 | \$ 8,690,000 | \$ 8,933,956 | 102.81% |

New City Hall

This project encompasses the demolition of the old City Hall; the demolition of the Lanham building (to the east) for additional parking for City Hall; the construction of a new City Hall and parking lot; the complete rebuild of the "Banner Bank" parking lot across 2nd street from City Hall to the west; and the replacement of water and sewer lines in the alley from 1st street to 3rd street.

Current Update: *Punchlist items are being worked on. Furniture installation has been scheduled for the week of September 26th. Things are moving along very well.*



Where Life is Sweet

Mayor and Members of the City Council
STAFF REPORT
 For the Meeting of September 26, 2022

Title/Subject

Approval of July 1, 2022 to June 30, 2025 Collective Bargaining Agreement (CBA) with the Hermiston Police Association (HPA)

Summary and Background

Negotiation of the July 1, 2022 to June 30, 2025 CBA with the HPA was completed in late August. Ratification of the proposed CBA by the membership of the HPA took place on September 20th. This year's session of bargaining was unique in that both parties, the City and the HPA had hired new representatives. This created some additional time in the negotiation timeline as each side got to know the other side. This situation also led to a number of language changes in the contract. Below is a summary of the changes to the CBA that were agreed to by both parties:

- The following articles had only language clarification/legal update changes made:
 - Article 1, Article 2, Article 3, Article 4, Article 9, Article 11, Article 12, Article 13, Article 14, Article 15, Article 17, Article 18, Article 19, Article 21, Article 23, Article 26, Article 27, Article 28, Article 29, Article 31, and Article 33.
 - Article 5 – A number of clarification changes were made. However, it also had one area of substance that was changed. Current practice for covering the Umatilla County Fair and Fair Parade was written into the contract to make sure everyone knew what was taking place.
 - Article 6 – One substantive change was made in addition to a number of clarifications. That substantive change involved establishing a process to examine changes made to Health Insurance benefits and how that impacts the ability for either party to “reopen” the contract in the middle of the agreement’s term.
 - Article 7 – The City agreed to increase the match for HPA members participating in one of our deferred compensation programs. The match went from 2% of \$5,000 to 4% of \$7,500.
 - Article 10 – The one substantive change made was to express a preference for an internal candidate as long as their qualifications are equal. There is also more explicit language in the agreement related to who is the appointing authority for jobs within the police department.
 - Article 16 – This article is the pay section.
 - The City agreed to pay increases of 4%/3%/3% for the three years of the agreement.
 - The City also agreed to pay \$1,500 per association member in 2022.

- The City also agreed to change a number of the specialty pays from dollar amounts to percentages and set a cap for the most a member could receive based on these incentives.
- Article 22 – Many clarifying changes. One of the most important was to clearly establish the probation.
- Article 25 – Most of this article was just cleaning up clarifying changes. In the marked up version you can see we had brought Juneteenth into the contract. We ended up leaving it out as a way to have some more time to work on the overall issue of paid time off. The HPA has expressed willingness to sit down and think of a more comprehensive solution for this issue.
- Article 30 – Added the frames of corrective glasses and hearing aids to things the City would replace if damaged at work. There are caps of \$400 for eyewear and \$2000 for hearing aids.

Tie-In to Council Goals

N/A

Fiscal Information

The individual financial impact is outlined above.

Recommendation

Recommended Action/Motion

Adopt Proposed changes to the CBA with the HPA.

Submitted By: Byron D. Smith

COLLECTIVE BARGAINING AGREEMENT
BETWEEN
CITY OF HERMISTON
AND
HERMISTON POLICE ASSOCIATION

July 1, 2022 – June 30, 2025
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COLLECTIVE BARGAINING AGREEMENT

This Agreement is made and entered into by the Hermiston Police Association, hereinafter referred to as “Association,” and the City of Hermiston, hereinafter referred to as the “City.” Now, therefore, it is hereby agreed by and between the parties as follows:

ARTICLE 1 – RECOGNITION

Section 1. Recognition. The City recognizes the Association as the exclusive bargaining agent for all police officers, corporals, and clerks within the police department, excluding supervisors and confidential employees. Corporals are not supervisors under the PECBA but are responsible to provide leadership, mentorship and assistance to other officers commensurate with their prior service and experience, including directing officers as necessary and appropriate.

Section 2. Exclusive Representation. The City shall not enter into any agreements regarding employment relations with any other organization or individual claiming to represent any group of employees in the bargaining unit or engage in any conduct which would recognize anyone other than the Association as a representative or employees in the unit, unless specifically authorized to do so by the Association.

ARTICLE 2 – ASSOCIATION RIGHTS

Section 1. Employee Rights. Employees shall have the right to form, join and participate in the activities of employee organization of their own choosing for the purpose of representation on matters of employee relations. Employees shall also have the right to refuse to join or participate in the activities of any employee organization. No employee shall be interfered with, intimidated or restrained, coerced or discriminated against by the City or any employee organization because of his/her exercise of these rights.

Section 2. Non-discrimination. This Agreement shall be applied equally to all employees in the bargaining unit without discrimination as to age, marital status, race, color, sex, sexual preference, religion, national origin, disability subject to reasonable accommodation, Association or political affiliation, or other classification protected by Oregon or federal law. The Association shall share equally with the City the responsibility for applying the provisions of this Agreement.

Section 3. Association Dues Check-Off. Upon written consent of an employee within the bargaining unit documented in the City payroll or personnel record, the City will deduct Association dues from employee pay in such amount as the Association may establish for Association members, and continue to do so until such time as the Association notifies the City in writing that the employee has withdrawn his/her membership, or the employee cancels and rescinds the consent previously given in writing delivered to and notifying both the City and the Association. The aggregate deductions of all employees shall be remitted together with an itemized statement to the Treasurer of the Association no later than the 10th of the month following the month for which the deductions were withheld.

Section 4. Notification of Association Coverage. When a person is hired in any classification represented by the Association, the City shall notify him/her that the Association is his/her recognized bargaining representative.

Section 5. Leaves of Absence. Upon return from leaves of absence, the City shall reinstate the payroll deduction of Association dues for those workers who were on dues check-off immediately prior to taking leave.

Section 6. Representational Activity. Members of the bargaining unit elected to serve as authorized representatives of the Association shall be expected to perform their duties as a representative of the Association on their own time, except as otherwise provided in this Agreement.

Section 7. Bargaining Committee Participation at Negotiations. Three (3) employees appointed by the Association as members of the Association's Collective Bargaining Committee shall be granted time off with pay to negotiate with the City.

Section 8. Bulletin Board. The Association shall be allowed to use a designated bulletin board for appropriate and Association related business.

ARTICLE 3 – MANAGEMENT RIGHTS

The right of the employees in the bargaining unit and the Association are limited to the terms set forth in this Agreement. The City retains all the customary, usual and exclusive rights, decision-making, prerogatives, functions and authority connected with or in any way incident to its responsibility to manage the affairs of the City or any part of it. The City shall have no obligation to bargain with the Association with respect to any subjects or the exercise of its discretion and decision-making which are reserved as City prerogatives under this Management Rights Article 3. Without limitation, but by way of illustration, the exclusive prerogatives, functions, and rights of the City shall include the following:

1. To direct and supervise all operations, functions and policies for the City in which the employees in the bargaining unit are employed and operations, functions and policies in the remainder of the City as they may affect employees in the bargaining unit.
2. To close or liquidate an office, branch operation or facility, or combination of facilities, or to relocate, reorganize or combine the work of divisions, offices, branches, operation or facilities for budgetary or other reason.
3. To determine the need for a reduction or an increase in the work force and the implementation of any decision with regard thereto.
4. To establish, revise, and implement standards of hiring, classification, promotion, quality of work, safety, materials equipment, uniforms, appearance, methods and procedures.
5. To implement new, and to revise or discard, in whole or in part, old methods and procedures.
6. To assign and distribute work.

7. To contract or subcontract work as determined by the City, provided that as to work covered by the bargaining unit the City agrees to afford an opportunity to negotiate with the Association as to the effect of such action on wages and condition of employees in the bargaining unit before finalizing or implementing any decision concerning such subcontracting, utilizing the expedited bargaining process set out in ORS 243.698.
8. To assign shifts, workdays, hours of work, and work location.
9. To designate and to assign all work duties.
10. To introduce new duties and to revise job classifications and duties within the unit.
11. To determine the need for the qualifications of new employees, transfers, and promotions.
12. To discipline, suspend, demote, or discharge an employee so long as such action is not arbitrary, in bad faith, or without just cause.
13. To determine the need for additional educational courses, training programs, on-the-job training and cross training, and to assign employees to such duties for periods as to be determined by the City.

ARTICLE 4 – SICK LEAVE

Section 1. Accrual. All employees accrue sick leave benefits as an insurance against the impact of illness or injury. Sick leave shall accrue at the rate of .0577 hours for regular hours worked, or a total of one hundred twenty (120) hours for an employee who works a regular working year of two thousand eighty (2,080) hours.

For purposes of this calculation, holidays, compensated sick leave and vacations shall be considered hours worked. The approximate equivalent accrual rate is fifteen (15) days per year, or ten (10) hours per month of service, however, the actual rate will depend on the hours worked by the employee. Accrual shall begin on the employee's date of employment. Current sick leave balance will be unchanged by this Agreement and will be credited to each employee's accumulated sick leave. Unused sick leave accrual shall be limited to two thousand eight (2,080) hours.

Section 2. Utilization. An employee can use accrued sick leave when unable to perform work duties by reason of illness or injury, dental or medical appointment, exposure to a contagious disease, or illness of an immediate family member. Employees are encouraged to make appointments outside of regularly scheduled working hours when possible.

Sick leave may be used for the employee's own illness, injury or health condition (including diagnosis, care, treatment and preventive medical care); and to care for a family member (Spouse, Same-Sex Domestic Partner, Son/Daughter/Stepchild, Son-in-law/Daughter-in-law, Mother/Father/Stepparent, Mother-in-law/Father-in-law, Brother/Sister, Brother-in-law/Sister-in-law, Children of Same-Sex Domestic Partner, Grandparent/grandchild) with an illness, injury or

health condition (including diagnosis, care, treatment and preventive medical care), and for reasons required by Oregon or federal law.

Section 3. Valuation and Payments Upon Retirement or Qualified Separation. Upon retirement employees will be compensated for accumulated, unused sick leave in the form of increased retirement benefits as allowed under provisions of the Oregon Retirement System, ORS 238.350. Sick leave benefits are not paid upon termination of employment except as PERS fold-in. However, for employees who leave the employment of the City (for reasons other than discharge) after fifteen (15) years of full-time employment, the City deposits (at the employee's current rate of compensation) one-eighth (1/8) the employee's accumulated sick leave, up to a cap of two thousand eighty (2,080) hours, into the Employee's HRA VEBA Medical Reimbursement Plan Account provided such deposit is permitted under the terms of the City's applicable benefit plan documents, which are controlling. For employees who leave the employment of the City (for reasons other than discharge) after twenty-five (25) years of full-time employment with the City, the City Deposits (at the employee's current rate of compensation) one-fourth (1/4) the employee's accumulated sick leave deposited into the Employee's HRA VEBA Medical Reimbursement Plan provided such deposit is permitted under the terms of the City's applicable benefit plan documents, which are controlling.

ARTICLE 5 – VACATIONS

Section 1. Accrual. Vacation shall be earned and accrued at the hourly accrual rates specified for the employee's years of City service up to the "hours per year" maximum and the "maximum accrual" limitations stated below in the following chart:

| Years of Service | Hours Per Year | Accrual Rate Per Hour | Maximum Accrual (Hours) |
|-------------------------|-----------------------|------------------------------|--------------------------------|
| 1-2 | 80 | .03847 | 120 |
| 3-5 | 96 | .04616 | 144 |
| 6-10 | 120 | .05769 | 168 |
| 11-15 | 160 | .07692 | 192 |
| 16-20 | 200 | .09615 | 216 |
| 21 or more | 224 | .10769 | 240 |

Vacation accrual shall apply to all scheduled hours worked, all sick leave, holiday or other time off scheduled benefit. Vacation will not accrue on overtime hours worked

Section 2. Valuation and Payment upon Separation. Upon termination of a permanent employee, he/she shall be paid for all earned but unused vacation time at the employee's current rate of pay. In case of death, compensation for accrued vacation leave shall be paid in the same manner to the employee's spouse or heirs.

Section 3. Anniversary Dates and Continuous Service. Anniversary dates shall begin with the date of employment and will be the same date each year thereafter. Employees hired between the first (1st) and fifteenth (15th) of the month shall have an anniversary date of the first (1st) of the month. Employees hired on the fifteenth (15th) of the month and later shall have an anniversary date of the first (1st) of the following month. Continuous service shall be service unbroken by separation from the City other than approved military leave, vacation or sick leave, or other qualified leaves.

Section 4. Utilization. Employees are encouraged to take vacation in forty (40) hour blocks. Requests for vacation normally shall be submitted a minimum of fourteen (14) calendar days in advance of time requested. Requests for vacation shall not be unreasonably denied by the City and are subject to staffing and operational needs as necessarily determined by the Chief of Police consistent with current policies. Vacation shall not be forfeited if denied based on the needs of the City.

The parties recognize the importance of full staffing during Umatilla County Fair Parade and Fair Week during August. If the City determines that Umatilla County Fair Parade and Fair Week require additional staffing and either overtime or a denial of vacation or vacation bids, the City will attempt to staff these events with volunteers. If an insufficient number of employees volunteer to cover the need, the Police Chief, or designee, will assign overtime or deny the vacation request or bid giving due regard to special circumstances of employees who will be affected and to seniority.

Section 5. Comp Time Coordination. Employees may be allowed compensatory time in conjunction with vacation time only with prior consent of the City.

Section 6. Bidding. Bidding for forty (40) hour blocks shall be by seniority for the first two rounds of bidding, and any vacation less than forty (40) hours shall be considered on a first-come-first-served basis. Requests for vacation time off less than a forty (40) hour block may be requested after bidding is completed and will be considered first come, first serve. Nothing in this Section prevents units or teams from establishing, changing or discontinuing an alternate vacation bidding process with the approval of the Chief of Police and the Association.

Section 7. Cashing Out Vacation. Employees may, once a year, cash out up to forty (40) hours of vacation accrued time.

ARTICLE 6 – HEALTH AND ACCIDENT INSURANCE

The City or Association may reopen wages and insurance for negotiations if benefit levels, plan coverage, or plan availability is changed by City-County Insurances Services or the Plan underwriter unless the Parties agree the changes are inconsequential.

The City shall provide a comprehensive medical and dental plan, BCBS Copay Plan B PPP with Alternative Care (to include Hearing Aid Benefit) as well as vision coverage supplied by VSP. The dental plan is provided by ODS or Willamette Dental. Employees shall contribute ten percent (10%) of current health insurance premiums and the City will contribute the remaining ninety percent (90%).

The employee may enroll in a premium conversion payroll deduction plan, which provides the employee the option to pay for, on a pre-tax basis, their share of the premium.

The City shall continue to provide a VEBA benefit, commencing January 1, 2012, at the following levels:

| | |
|----------------------------------|------------|
| Single employee | \$1,000.00 |
| One employee plus spouse/partner | \$1,500.00 |

Employee plus family \$2,000.00

ARTICLE 7 – RETIREMENT

The City shall continue to provide the present employee retirement plan provided through the Oregon Public Employees Retirement System.

The City shall match four percent (4%) of up to the first seven thousand five hundred dollars (\$7,500.00) contributed by an employee to one of the City-approved deferred compensation programs.

ARTICLE 8 – DISABILITY INSURANCE

The City shall provide long-term disability insurance comparable to the current City policy.

ARTICLE 9 – INDEMNIFICATION AGAINST LIABILITY

The City shall indemnify and hold harmless employees from liability in accordance with the Oregon Tort Claims Act by maintaining current insurance coverage through City County Insurance Services in order to provide, in accordance with the terms of the CIS insurance policy, liability insurance protection for possible claims arising out of acts committed by employees in the discharge of their duties and in the course of their employment. Such insurance shall cover all costs, including attorney’s fees, connected with proposed or threatened suits and negotiated settlements.

ARTICLE 10 – SENIORITY

Section 1. Seniority Defined. Seniority as used in this Agreement is determined by the length of an employee’s continuous service with the Police Department since his/her date of hire by classification, provided however that in the case of any employee hired and appointed as a police officer after serving in a non-sworn bargaining unit position, such employee’s seniority date shall be adjusted to the date of employment as a sworn officer.

Section 2. Part-Time Seniority. All part-time service shall be credited on a prorated basis.

Section 3. Hire/Promotion Seniority Service Credit. In matters of promotion to a higher position, selection of an employee shall be made based on qualifications and ability, and where qualification and ability are equal, as determined by the City, seniority shall be the determining factor. The City will fill positions by promoting present Police Department employees whenever qualifications are deemed equal to those of an outside applicant. However, if in the judgment of the Chief of Police or the City Manager, , an external candidate possessing superior qualifications and ability to the internal Police Department candidates, the City may hire such outside person.

ARTICLE 11 – PERSONNEL RECORDS

Section 1. Inspection of Personnel File. In accordance with Oregon law each employee may review the contents of his/her own personnel file and obtain a copy. Employees may request to be accompanied by an Association representative.

Section 2. Confidentiality of Employment Records. Access to a staff member's personnel file shall be limited to the individual employee, those with a need in the interest of the City, and those whose access to the information is granted by the City in accordance with law, legal process, or a release and authorization of the employee.

Section 3. Derogatory Entries. Materials which reflect negatively upon an employee shall bear either the signature of the employee or written confirmation that the employee was shown the material and refused to sign it.

Section 4. Retention and Removal. Corrective action documentation, such as a letter of caution, oral reprimands and similar instructional and supervisory communications may be retained in supervisory files and, if appropriate, referred to or relied upon in yearly evaluation documents or, if relevant, in progressive action. Such corrective documentation (less than a written reprimand) shall not be relied upon in evaluation or progressive discipline which arises after three (3) years has elapsed without further corrective or disciplinary action for comparable reason(s).

Corrective or disciplinary records up to a written reprimand will be removed from a personnel file at an employee's request after thirty-six (36) months has elapsed following the events described if the employee has met performance expectations during that period without recurrence of similar issues documented in the record. After removal such records will not be used for purposes of progressive discipline except to demonstrate training or forewarning. Whenever documentation of corrective or disciplinary action is removed from an employee's personnel file or supervisory files maintained by the City, the records shall be retained in a separate system of records not identifiable by the name of any employee and kept in order to show training provided, demonstrate City policymaker decision-making, and illustrate forewarning in appropriate cases.

Material placed in the personnel record of an employee without conforming with the provisions of this Article shall not be permitted to be used by the City in any disciplinary proceeding involving the employee.

Nothing in this Agreement shall be interpreted or applied to require the City to purge any record which has been reported to a public database in accordance with Oregon law (HB 3145 and HB 4207) or which relates to a violation of standards of conduct identified by the Commission on Statewide Law Enforcement Standards of Conduct and Discipline (HB 2930).

Section 5. Public Records Requests for Employee Records. If employee records are requested under the Oregon Public Records Law while the employee is employed by the City or during the pendency of grievance or arbitration proceedings challenging a discharge from employment, the employee shall be provided a copy of the request to inspect or obtain a copy of the employment record or any portion thereof and notice of the City's intention to release records in advance of doing so.

Section 6. Written commendations shall become a permanent part of the employee's personnel file and the employee shall be furnished a copy of all such material at the time it is placed in the employee's personnel file.

ARTICLE 12 – GRIEVANCE PROCEDURE AND ARBITRATION

Section 1. Grievance Procedures. The purpose of the grievance procedure shall be to settle differences between the City and employees as quickly as possible to ensure efficiency and to promote employees' morale. A grievance is defined as a complaint arising out of alleged violations concerning the application of interpretation or compliance with the provisions of this Agreement.

Step 1. In cases involving a complaint by an employee or employees, the representative of the Association or the aggrieved employee or employees, with or without the presence of the representative of the Association, shall present the grievance, in writing, within ten (10) business days after it arises or is discovered, to the immediate supervisor and to the Association President. In cases involving imposition of discipline, the grievance shall be presented to the supervisor who imposed the discipline. A grievance shall identify the specific sections of this Agreement which the City is alleged to have violated, state the facts and actions that give rise to the grievance, and state the remedy sought. The supervisor shall respond in writing within ten (10) business days.

Step 2. If the grievance is not satisfactorily resolved at Step 1, the Association or the aggrieved employee may present the grievance to the Chief of Police or designee within ten (10) business days of the denial at Step 1. The Chief will issue a written decision within ten (10) business days of receipt unless the Chief requests a meeting with the employee and the Association concerning the grievance, in which case the Chief's written decision shall be issued within ten (10) business days after the meeting occurs.

Step 3. If the grievance is not satisfactorily resolved at Step 2, the Association may submit the grievance to the City Manager or designee. The Association and the City Manager or designee shall meet to discuss the matters in an earnest effort to resolve the grievance commencing within ten (10) business days of the Chief's Step 2 decision. The City Manager or designee will issue a written decision within ten (10) business days following the Parties' efforts to resolve the grievance.

Step 4. If the Parties are unable to resolve a grievance at Step 3, upon the written request of the Association within ten (10) business days of the Step 3 response, the Association may elect to submit the grievance to binding arbitration in the appropriate manner, as follows:

1. **Police Misconduct.** For police misconduct cases, the parties will comply with HB 2930 and Employment Relations Board published rules related to discipline and the selection of an arbitrator.
2. **Other cases.** For cases that do not involve police misconduct, the Association shall submit a written request to the Oregon Employment Relations Board for a

list of thirteen (13) arbitrators who are also on the FMCS list. A copy of the Association's request shall be provided to the City's labor counsel. The party requesting and initiating arbitration shall strike first from the list. Thereafter, the parties shall strike alternatively until only one (1) name remains. The remaining name shall be the arbitrator.

A. Arbitrator Authority. The arbitrator shall have no power to modify, add to or subtract from the terms of this Agreement and shall be confined to the interpretation and enforcement of this Agreement. The arbitrator's decision shall be in writing and shall be submitted to the parties within thirty (30) days following the close of the hearing. The arbitrator's decision shall be final and binding on the affected employee(s), the Association and the City.

B. Subpoenas. Either party may request the arbitrator to issue subpoenas. If subpoenaed to an arbitration, City employees/Association members shall not receive fees and mileage associated with an enforceable subpoena. Each party shall be responsible for compensating its own witnesses and representatives during the arbitration hearing. The costs, fees and expenses of the arbitrator shall be borne by the non-prevailing party or prorated as determined by the arbitrator.

C. Court Reporter/Record. If either party desires a verbatim recording of the proceedings, it may cause such a record to be made, provided it pays for the appearance fee, record, and makes a copy available without charge to the arbitrator. If the other party desires a copy, both parties shall jointly share in all costs of producing three (3) copies of the transcript.

D. Time Limitations and Extensions of Time. Any of the time limitations specified in the above-prescribed procedure may be extended by mutual consent of the parties in writing. In the absence of such mutual consent, non-compliance with any of the time limitations (1) by the Association shall constitute waiver of the grievance, and (2) by the City shall result in advancing the grievance to the next step. For the purposes of this article, days refers to business days, being Monday through Friday.

Section 2. Grievance Resolution Meetings. Meetings arranged by the parties hereto for the purpose of processing grievances or dealing with other matters relative to the administration of the Agreement shall not result in loss of pay for employees who attend such meetings while on duty.

ARTICLE 13 – UNIFORMS

Section 1. Uniform Issue and Cleaning. Uniforms required to be worn by employees in the Police Department shall be furnished and dry cleaned by the City. The dry cleaning will only be covered as long as a dry-cleaning business is located in Hermiston.

Section 2. Uniform Maintenance. Alteration and repairs of all uniforms will be paid by the City but only after pre-approval through the established chain of command. No employee shall be required to wear any part of a uniform that is deteriorated or has been mended in a manner that is obvious to the public eye.

Section 3. Detective Clothing Allowance. Any member assigned to the detective unit, shall be provided a three hundred dollars (\$300.00) per year clothing allowance, to be paid in January of each year.

Section 4. Boot Allowance. The City shall provide a boot allowance of two hundred dollars (\$200.00) per calendar year to all sworn members. If a member is hired mid-year, the amount will be prorated.

ARTICLE 14 – MILEAGE AND LODGING

Section 1. Personal Vehicle Mileage. When employees use a personal vehicle in the performance of official duties, they shall be compensated at the current IRS rate.

Section 2. Per Diem/Reimbursement. When an employee is traveling as authorized by the Chief of Police, employees are reimbursed for meals (breakfast, lunch and dinner) and lodging. Meals are only reimbursed if your trip includes an overnight stay. Alcoholic beverages do not qualify for reimbursement. For current meal reimbursement rates please see the per diem rates on the U S General Services Administration website: <http://www.gsa.gov/portal/content/104877>

Section 3. Receipts. Employees will furnish receipts for lodging, and in the event a City issued credit card is provided, all receipts for its use shall be maintained and furnished.

ARTICLE 15 – EDUCATION INCENTIVE PROGRAM

To encourage employees to pursue appropriate formal education, the education incentive program will be administered to reimburse approved job-related educational training costs for courses approved in advance by the Chief of Police subject to the following conditions:

- A. Employees must have completed at least twelve (12) months of City service to participate.
- B. Reimbursement shall be restricted to tuition, course fees and required textbooks, up to two hundred seventy dollars (\$270.00) per credit hour, for not in excess of ten (10) credit hours per school quarter.
- C. Reimbursement may be restricted to courses offered by a recognized and accredited school.
- D. Upon obtaining preapproval of the Chief of Police, the employee shall pay tuition, course fees, textbook and other costs, if any, upon enrollment. The employee must present evidence to substantiate expenses and obtain a grade of “C” or better to be eligible for reimbursement for an approved course. Classes taken on a “pass/fail” basis must be “passed.”
- E. The City’s educational incentive program shall not be utilized by employees who obtain training or education funding from any other source (such as a grant or scholarship) if the payments in the aggregate exceed the costs incurred by the employee for the course.

- F. In cases of hardship or other exceptional circumstances, the Chief of Police may, in the City’s sole discretion which may be exercised case-by-case, accommodate the needs of a particular employee by making exceptions to the requirements of this Article. However, if the employee fails to fulfill the requirements and conditions associated with reimbursement, the employee shall refund to the City the expenses the City has incurred on behalf of the employee including by payroll deduction.

ARTICLE 16 – WAGES AND SALARIES

Section 1. Wages. Employees covered by this Agreement shall be paid according to classification and rates of pay established on Schedule A. Salary schedules are attached hereto and made a part of this Agreement.

Effective 7/1/22, wages will be increased across the board by four percent (4%).

Association members employed on July 1, 2022, will be paid a lump sum signing bonus of one thousand five hundred dollars (\$1,500.00).

Effective July 1, 2023, wages will be increased across the board by three percent (3%).

Effective July 1, 2024, wages will be increased across the board by three percent (3%).

Section 2. Incentives and Premiums. In addition to the salaries set forth in Section 1 and Appendix A, employees are eligible to receive incentives as follows:

- A. Education Incentive. The City will pay officers who have earned an AA/AS degree one hundred fifty dollars (\$150.00) per month. Officers who have earned a BA/BS degree will be paid two hundred fifty dollars (\$250.00) per month. No employee may receive both education incentives.
- B. DPSST Incentive. The City will pay police officers who hold an Intermediate DPSST Certificate a premium of four percent (4%) computed on the monthly base wage. The City will pay police officers who hold an Advanced DPSST Certificate an incentive premium of an additional four percent (e.g.: a combined total of eight percent (8%)) computed on the monthly base wage.
- C. Bi-Lingual Incentive. Eligible employees shall receive bilingual pay of five percent (5%). To be eligible for bilingual pay, employees must pass an approved aptitude test for the specific (non-English) language. The (non-English) language must be consistent with demographics for the City of Hermiston and will only account for those (non-English) languages that comprise at least ten percent (10%) of the population within the City.
- D. Premium Pay. The City will compensate employees approved and assigned by the Chief of Police as follows, provided however that no employee shall receive incentive pay in excess of twelve percent (12%) for any combination of the following within these categories:

| | |
|-----------|----|
| Detective | 5% |
| Corporal | 5% |

| | |
|---------------------|-----------------------------------|
| SWAT | 3% |
| Instructor | 3% |
| DRE | 2% |
| DME | 2% |
| FTO | 5% while performing duties as FTO |
| Reserve Coordinator | 2% |

Officers who receive incentive as a corporal, FTO or detective shall be ineligible for out-of-class pay. Incentive pay shall discontinue if the qualifying certificate, if any, lapses. Each of the incentive percentages shall be computed based on base pay stated in the Wage Appendix A or pay classification plan, the sum of which shall be the regular rate of pay for computing the FLSA overtime rate.

Employees assigned as School Resource Officer will receive a premium of two percent (2%) and, for the duration of the assignment shall flex hours of work subject to approval of the Chief of Police or designee to meet the unique needs of the schools and students served during weeks when school is in session. When school is not in session, the SRO’s schedule and hours of work may be determined by the Chief or designee based on the City’s needs at the time.

The Chief of Police shall retain discretion to assign to and remove from each assignment compensated by an incentive, and no employee may be regarded as having an entitlement or property interest to retain or remain in any assignment.

Section 3. Rate of Pay for New Positions. If the City creates a new position which falls within the scope of the bargaining unit, the parties shall enter into wage negotiations for the purpose of establishing a rate of pay upon request; provided however that such bargaining shall not delay City implementation provided the City agrees that whatever rate is agreed upon shall be paid retroactive to the date of appointment.

Section 4. PERS Pick-up. In addition to the City’s employer contributions to PERS, the City shall, pay what would otherwise be the employee’s six percent (6%) contribution to PERS (“PERS Pick-Up”). Although the PERS Pick-Up contribution is an employee contribution for purposes of PERS, it shall be treated as an employer contribution for federal income tax purposes in accordance with relevant sections of the IRC.

NOTE IN BARGAINING --- AND AGREED: Upon conclusion of bargaining, the Chief of Police will establish a roster of those who are recognized to receive specialty pay noted above in this TA. Incentives and/or premiums paid under the existing CBA will continue to be paid in the same manner through August 2022 and retroactive to July 1, 2022. Effective September 1, 2022, incentives in Section D will switch to a percentage of base pay as opposed to the flat rate/point system. The new SRO incentive is effective September 1, 2022.

The Reserve Coordinator position and program are under evaluation, however the current assignment will be maintained until the completion of the August pay periods. Designation and assignment of the Reserve Coordinator position after the August pay periods will be made if the City determines to continue the program.

ARTICLE 17 – SALARY ADMINISTRATION

Section 1. Twelve-Month Increases: Each new employee shall receive a one-step salary increase on their twelve (12) month anniversary consistent with the established pay scale for steps A through I.

Section 2. Advanced Step Placement. On recommendation of the Police Chief and approval of the City Manager, a starting employee may be placed at a higher level if the experience of the applicant dictates the extra salary.

Section 3. Anniversary Date. Employees who begin work prior to the fifteenth (15th) day of any month shall have an anniversary date of the first of that month. Employees who begin work on or after the fifteenth (15th) day of the month shall have an anniversary date of the first of the following month.

Section 4. Step Advancement. Subject to annual review showing satisfactory performance, all employees will be raised annually to the next step of the salary schedule on their anniversary date of hire, as defined in Section 3, until the top step of the schedule is reached.

Employees receiving an unsatisfactory performance review will receive a notice of deficiency identifying the areas of deficient performance and be provided a period of ninety (90) days in which to remedy the deficiency or forfeit the step increase.

Performance review conducted at the close of the ninety (90) day period will determine eligibility for the step consideration. Any employee denied a step increase in accordance with this provision will be eligible for consideration for the same step level, subject to satisfactory performance review, on their next anniversary date. A subsequent satisfactory performance review will allow the employee a one (1) step move.

ARTICLE 18 – WORK SCHEDULES

Section 1. Regular Work Shifts. The work week shall consist of five (5) consecutive eight (8) hour days or four (4) consecutive ten (10) hour days or a variation of nine (9) hour or twelve (12) hour days. An employee's work week will start the first day worked on the designated shift or rotation. The parties elect to follow FLSA 7(k) rules and comply with FLSA requirements.

Section 2. Regular Workday and Breaks. The workday shall include a paid one-half (1/2) hour meal period and two (2) paid fifteen (15) minute rest breaks, one (1) rest break during each half of an employee's shift.

ARTICLE 19 – OVERTIME

Section 1. Overtime. All overtime shall be compensated at the rate of one and one-half (1 ½) times his/her regular rate. However, no overtime shall be paid where an employee voluntarily exchanges shifts of work or days of work.

For Patrol employees who work twelve (12) hour shifts, work in excess of the regularly assigned work shift and work in excess of one hundred seventy-one (171) hours in a twenty-eight (28) day FLSA 7k period shall be paid at the overtime rate of one-and-one-half (1 ½) times his/her regular

rate in accordance with the FLSA. All other overtime shall be compensated at the rate of one-and-one-half (1 ½) times his/her regular rate in accordance with the FLSA.

Section 2. Call-back. When recalled to work one (1) hour or more after completion of work and departure at end of the last completed shift and call-back more than one (1) hour prior to the start of the next shift, the employee shall receive not less than three (3) hours overtime pay at one and one-half (1 ½) times his/her regular rate.

Section 3. Compensatory time. This Agreement constitutes the parties' agreement regarding the use of compensatory time pursuant to 29 CFR § 553.23 et seq. Compensatory time will be accrued at the employee's regular overtime rate and paid at the employee's regular straight time rate. Compensatory time may be earned and taken off with the mutual agreement of the City and the employee. Employees shall have the option to elect compensatory time off subject to the one hundred twenty (120) hour limit on compensatory time bank in lieu of being paid at the overtime rate. Employees may accumulate a compensatory bank not to exceed one hundred twenty (120) hours.

Compensatory time will be taken off by the employee at times which do not disrupt operations or cause the City undue hardship or otherwise avoidable overtime expense. An employee who desires compensatory time off shall arrange compensatory time off with the consent of his/her supervisor. Such consent shall not be unreasonably denied. Compensatory time may be taken upon mutual agreement between the employee and his/her supervisor. The City may substitute cash and purchase the requested compensatory time off if an agreement to schedule requested compensatory time off cannot be reached. Compensatory time requests may be submitted not more than thirty (30) days in advance. A supervisor shall respond within ninety-six (96) hours of receipt of a request for compensatory time utilization. Compensatory time shall be taken off in blocks no greater than forty (40) hours.

There is no cash out of compensatory time except that the value of a compensatory time off bank shall be computed and paid to the employee upon retirement or other separation from City employment.

Section 4. Shift Trades. Employees in the same classification who have successfully completed FTEP and/or who have been released as qualified to work independently without direct supervision in a solo status, and who are working the same shift (i.e., day/afternoon/grave) may trade shifts with written approval prior to the trade (on a HPD shift trade form) from the affected shift supervisor(s). Shift trades will not occur as a matter of entitlement and are intended to accommodate unique and personal employee situations. The City shall not record hours worked on a trade in the time to payroll records of the City; both employees' records of hours of work shall be maintained as if each employee worked the regular hours assigned and shall be paid accordingly. In the event an employee who trades and works a shift for another employee in a holdover or call back situation, thereby working contractual overtime, such overtime, call back or other appropriate compensation shall be paid to the employee who works the hours and shall not be reciprocated as part of the trade agreement. Each employee involved in a shift trade is responsible for maintaining and being able to produce a record of shift trades (the HPD shift trade forms) they have participated in.

ARTICLE 20 – BEREAVEMENT LEAVE

In the event of death in the employee's immediate family, the employee shall be granted a paid leave of absence not to exceed five (5) calendar days. For the purpose of this Article, immediate family is defined to include the employee's present spouse or registered domestic partner, children, step-children, parents, step-parents, brothers, sister, grandparents, in-laws, and dependent.

ARTICLE 21 – LAYOFF

Section 1. Layoff. In the event of layoff (as opposed to discharge for just cause), employees shall be laid off in the inverse order of their seniority, provided the senior employee is qualified for the remaining position, and further provided that the senior employee is currently serving in that position or has previously served in that position for the City.

Section 2. Recall. Recall rights shall exist for a period of twelve (12) months from the date of layoff. Employees shall be recalled from layoff according to seniority, provided the senior employee is qualified for the position and has worked in that position for the City. No new employees shall be hired in any classification until employees are recalled pursuant to this section. It shall be each employee's responsibility to keep the City apprised of his/her current mailing address during the recall period.

ARTICLE 22 – DISCIPLINE AND DISCHARGE

Section 1. Purpose. This Article is designed to establish department policy and procedure for the disciplinary process. It is responsive to and is closely related with the investigation and disposition of complaints as well as performance issues. In order to protect the rights of citizens and department members, it is the policy of the police department to thoroughly investigate complaints alleging misconduct, lack of performance, or improper performance of any department employee. At the conclusion of an inquiry or complaint investigation, appropriate action will be taken. No employee shall be discharged or disciplined except for just cause.

Section 2. Disciplinary Actions
Discipline has as its immediate purpose the channeling of individual effort into effective and productive action. Corrective actions may include such activities as encouragement, counseling, and training. Formal discipline may be imposed when deemed appropriate for just cause. Corrective action may be instruction or counseling intended to modify or reinforce the behavior of an individual and/or correct an employee's behavior or performance.

Oral counseling and warnings for the purpose of resolving performance or work related issues may be documented in writing and retained in a supervision and/or performance and/or personnel file as the City may determine, and are not subject to the grievance procedure. Such actions may be relevant to progressive discipline consideration for a period of one year.

Discipline may include written reprimand, suspension without pay, demotion, and termination, imposed due to the seriousness of behavior or conduct in a particular instance or when corrective actions taken previously has been ineffective. Disciplinary action will normally be progressive in nature, however the form of discipline may be determined based upon the seriousness of the offense or behavior.

Section 3. Probationary Period.

Sworn officers shall be required to complete an eighteen (18) month probationary period. Non-sworn bargaining unit employees shall be required to complete a twelve (12) month probationary period. If the City hires a lateral with six (6) or more years sworn Oregon experience for a sworn position, the lateral probation shall be twelve (12) months. Probation begins from date of hire. Probationary employees are represented by the Association and entitled to all the benefits of this Agreement, except however that probation is an extension of the background and selection process and during the probation period the City may determine in its discretion that the probationary employee performance is not satisfactory and warrants termination of employment. In such event, the probationary employee shall not be entitled to recourse under the grievance article of this Agreement.

Section 4. Notice of Investigatory Interview or Internal Affairs InvestigationA. Notice:

Prior to formal investigatory interviews for the purpose of potential discipline, which could reasonably lead to disciplinary action, the employee and the Association shall receive confidential written notification of the complaint at least six (6) hours before the interview.

This notice shall include:

- a. a copy of the original complaint, or a summary listing the relevant facts.
- b. notice of potential policy or criminal violations.
- c. notice of opportunity to consult an Association or legal representative prior to the interview and have Association representation during such interview in accordance with PECBA rights.

B. Investigatory Interviews:

- a. Should an employee be ordered to testify in any investigatory hearing, the employee shall be given written "Garrity" warnings.
- b. Interviews ordinarily will be conducted during the officer's regular hours of duty, unless of exigent circumstances and shall be conducted in a professional manner with reasonable breaks as necessary.
- c. Interviews can be recorded by either party, and if so, either party will provide timely copies upon request, including transcripts if done.

Section 5. Notice of Proposed Discipline and Pre-Disciplinary Hearing.

A. Notice to Employee. Prior to management reaching any final conclusion to impose discipline, the employee and the Association shall be provided the following information prior to the pre-disciplinary or "Loudermill" hearing:

- a. A copy of the investigation and witness statements supporting the allegations.
- b. The policy, work rules or other basis for the discipline.
- c. The disciplinary action being considered or proposed.

B. Pre-disciplinary/Loudermill Hearing. Prior to imposition of discipline, the employee and Association shall be afforded opportunity to respond in writing to the charges, and/or to meet with the Chief of Police or individual with the authority to impose the discipline for the alleged conduct, normally within seventy-two (72) hours of receipt as delineated in the notice. This opportunity shall include the right to provide information including extenuation, mitigation and defense.

Section 6. Imposition of Discipline.

Imposition of discipline requires written notice to the employee to include: the specific allegations, the supporting evidence and the sanction imposed. The employee will be provided with a full copy of the investigation unless this documentation was provided to the employee and the Association during procedures described above in this Article. When the City provides disciplinary documentation to an employee the City shall copy the Association.

ARTICLE 23 – OUT OF CLASSIFICATION

When an employee is assigned to perform substantially all the duties of a classification with a higher rate of pay than his/her own regular classification, while so assigned the employee will be paid at the entry pay step of that classification, or five (5%) percent out-of-class pay, whichever is the greater.

ARTICLE 24 – ASSOCIATION MANAGEMENT COOPERATION

Each employee covered by this Agreement recognizes that employment at a fair wage can continue only as long as a high level of productivity is maintained. The City, the Association and all employees jointly enter into this Agreement pursuant to that recognition. Each employee pledges his/her cooperation in eliminating waste, in conserving equipment and in preventing excessive absenteeism.

ARTICLE 25 – HOLIDAYS

Section 1. Designated Holidays. The following holidays shall be designated as official holidays and shall be observed in accordance with this contract:

| | |
|-------------------------|------------------|
| New Year’s Day | Independence Day |
| Martin Luther King, Jr. | Labor Day |
| Presidents’ Day | Veteran’s Day |
| | Thanksgiving Day |
| Memorial Day | Christmas Day |

Section 2. Personal Days. Employees will also accrue one employee day off on January 1, and July 1, of each calendar year which must be used or forfeited within the calendar year earned.

Section 3. Holiday Accrual. Employees who work eight (8) or nine (9) hour shifts shall receive forty-four (44) hours of holiday leave on January 1 and again on July 1 (hereinafter, the holiday “bank”). Employees that work ten (10) or twelve (12) hour shifts shall receive sixty-six

(66) hours of holiday leave on January 1, and again on July 1. This time will be banked and can be used in the same manner as vacation accrual. All holiday and personal days must be used in the calendar year or will be forfeited. Effort should be made for timely use of holiday time, to avoid demands at the end of the year which creates situations below minimal staffing levels. Holidays will be recognized on the actual day of the holiday.

Section 4. Pay In Lieu of Holiday Accrual. Employees who work a holiday will receive overtime pay (rate of double time) for the hours worked. Upon payment, the holiday benefit for that holiday is received and charged to the bank. Employees may elect to work the holiday for straight time and bank the eight hours of leave.

Section 5. Holiday Accrual Cash-out. If an employee wishes to “cash out” holiday time, they can one time per calendar year, cash out up to forty (40) hours of holiday time. The employee must have the requested amount in their holiday balance at the time of request.

Section 6. Holiday Accrual Payment at Separation. If an employee separates from the City, tabulation of holiday leave will be based on the actual holiday occurrence and use.

ARTICLE 26 – WORKER’S COMPENSATION

The City shall provide workers’ compensation insurance or self-insure in accordance with law. During the period of workers’ compensation related time loss, the injured employee will continue to accrue seniority and shall be eligible for other benefits of this Agreement. In case of accidental occupational injury to any employee, such injury shall be reported immediately to the employee’s supervisor. The employee shall cooperate with the City and complete and submit all required reports and claim forms as promptly as the injury permits.

An employee who is entitled to receive Workers’ Compensation time loss payments is compensated an amount computed by the Workers’ Compensation insurer as provided by law based upon annualized earnings (base wage plus premiums, incentives and overtime). The employee shall receive these payments during the time loss eligibility period as provided by law.

In addition, during the first ninety (90) days of a Workers’ Compensation time loss due to an on the job injury or illness, if the time loss payment from the Workers’ Compensation insurer is determined by the City to be less than the employee’s annualized earnings (base wage plus premiums, incentives and overtime net of taxes), the City will pay to the employee an amount equal to the difference between the Workers’ Compensation payment received and the employee’s net regular salary.

During the first ninety (90) days, if the employee returns to light duty and the employee’s compensation received for light duty and time loss payments is less than the employee’s net regular salary, the City will pay to the employee an amount equal to the difference between the compensation currently received and the employee’s net regular salary.

Beginning on the ninety-first (91st) day after the date of injury, the employee may use any form of paid time off including accrued comp time, holiday credits, and vacation credits and any accumulated sick leave if the employee desires to supplement Workers’ Compensation payments under this Agreement so that the injured employee receives the employee’s net regular salary. Under no circumstance may an employee use accrued paid leave to exceed the employee’s net

regular salary. An employee may notify the City that the employee does not want the leave charged against accrued leave, and the employee will remain in time loss and/or other appropriate protected leave status.

Health care practitioner (HCP) progress reports may be required prior to approval of payments under this Article.

For the purposes of this Section, "net regular salary" is calculated based upon the formula used by the Workers' Compensation insurer as provided by law.

While an employee is out on Workers' Compensation the employee shall be entitled to accrue full holiday, vacation, sick leave and other leave accruals they would otherwise earn and be eligible for if the employee was working the full month.

ARTICLE 27 – IMMUNIZATION

When recommended by a designated physician advisor, the City may require immunization for employees as protection from occupational diseases such as tetanus and hepatitis. Employees shall seek such immunization through their preferred health care practitioner. If a vaccine is mandated by the State of Oregon or the Federal government, the City may require employees to provide immunization records from their health care provider.

Employees will not be required to submit to vaccinations that are not required by the State of Oregon or the Federal government.

ARTICLE 28 – TOBACCO USE

No employee is allowed to use tobacco products while on duty. "Duty" as defined for this policy shall include all time compensated by the City, other than vacation, holiday, sick leave and compensation time off.

ARTICLE 29 – DRUG AND ALCOHOL PROGRAM

Section 1. Safety and Productivity. The City of Hermiston and the Association recognize a responsibility to the citizens of Hermiston and the City's employees to maintain safe and productive working conditions. The parties also recognize the special duties and responsibilities that public safety employees have, including the need to maintain the respect and confidence of the citizenry.

Section 2. Drug Free Workplace. To aid in the accomplishment of the goals noted in Section 1, the City forbids the possession, distribution, dispensation, manufacture, sale or use of illegal drugs, or the abusive use of legal drugs by any bargaining unit employee at any time. The City also forbids the abusive use of alcohol in the work place.

For the purposes of this Article, "use" of illegal drugs, or alcohol is indicated by the presence of any detectable amount of an illegal drug use, or alcohol an employee's system during work time.

Section 3. Assistance and Treatment. Any employee with a substance abuse problem is strongly encouraged to voluntarily seek professional assistance, counseling, and/or treatment. Such help is available through the City's Employee Assistance Program or a list of local sources for such help is available from the City. The parties stand ready to assist in that process wherever appropriate.

Section 4. Reasonable Suspicion Testing. Where there is reasonable suspicion that an employee is using illegal drugs or abusing legal drugs or alcohol in violation of this Article, or where an employee is involved in an accident or other incident, while on duty, in which reasonable safety precautions appear to have been ignored, an employee may be required to submit to testing for presence of drugs or alcohol.

Employee testing will be at City expense. Analysis of such tests will be done at a licensed clinical laboratory which is US DOT certified. Employees may be subject to breath, urine or blood testing, which shall be conducted only upon reasonable suspicion.

Section 5. Accommodation. In appropriate cases where substance abuse is determined to be subject to the ADA and reasonable accommodation, accommodation may be considered as an alternative to discharge. In the event of a positive test, an employee who seeks professional help for addiction or alcoholism may be suspended without pay during a time determined necessary to successfully complete in-patient or other professionally administered treatment or rehabilitation program acceptable to the employee and the City. In the absence of other aggravating and disqualifying circumstances, upon successful completion, an employee will be reinstated subject to periodic retesting at any time for a three (3) year period and subject to the terms of a last chance agreement acceptable to the City.

The confidential nature of these matters will be respected in the administration of these procedure to the extent possible consistent with their effective use.

ARTICLE 30 – REPLACEMENT OF CORRECTIVE LENSES/FRAMES AND HEARING AIDS

If an employee loses or damages his/her corrective lenses and/or frames in the line of duty and not due to the officer's negligence, the City will reimburse the costs for replacement or repair up to four hundred dollars (\$400.00) in total for eyewear. If an officer loses or damages hearing aids not due to the officer's negligence, the City will contribute up to the first two thousand dollars (\$2,000.00) of expense unless replacement or repair is within a warranty or insurance benefit.

ARTICLE 31 – RESIDENCY

Each July, employees who have maintained a personal residence within the Hermiston City Limits for the prior twelve (12) months will receive a payment of five hundred dollars (\$500.00).

ARTICLE 32 – SAVINGS CLAUSE

Should any provision of this Agreement be found by a court of competent jurisdiction to be in violation of any federal, state or city law, that provision will be submitted to negotiations to bring it into compliance with the law. The remaining provisions of this Agreement shall remain in full force and effect.

ARTICLE 33 – DURATION OF AGREEMENT

Section 1. Term. This Agreement shall become effective as of July 1, 2022 and shall remain in full force and effect through June 30, 2025, or until a successor agreement is reached. Notice to bargain a new contract shall be provided at least ninety (90) days prior to termination.

Section 2. Amendments. The Agreement may be amended at any time by mutual agreement of the Association and the City; such amendments shall be in writing and signed by both parties.

Dated this **th day of *****, 2022.

CITY OF HERMISTON

HERMISTON POLICE ASSOCIATION

By _____
Byron D. Smith, City Manager

By _____
Tim Miears, Association President

Date: _____

Date: _____

APPENDIX A

| | | | | | | | | | |
|----------------------------------------------------------------------|--------|--------|--------|--------|--------|--------|--------|--------|--------|
| This salary schedule is effective July 1, 2022 through June 30, 2023 | | | | | | | | | |
| (Reflects salary for no certificate or incentives) | | | | | | | | | |
| | Step A | Step B | Step C | Step D | Step E | Step F | Step G | Step H | Step I |
| Records Specialist | | | 4238 | 4408 | 4582 | 4764 | 4957 | 5156 | 5365 |
| Patrol Officer | 5134 | 5309 | 5505 | 5707 | 5922 | 6160 | 6406 | 6665 | 6931 |
| | | | | | | | | | |
| | | | | | | | | | |
| | | | | | | | | | |
| This salary schedule is effective July 1, 2023 through June 30, 2024 | | | | | | | | | |
| (Reflects salary for no certificate or incentives) | | | | | | | | | |
| | Step A | Step B | Step C | Step D | Step E | Step F | Step G | Step H | Step I |
| Records Specialist | | | 4365 | 4540 | 4719 | 4907 | 5106 | 5311 | 5526 |
| Patrol Officer | 5288 | 5468 | 5670 | 5878 | 6100 | 6345 | 6598 | 6865 | 7139 |
| | | | | | | | | | |
| | | | | | | | | | |
| | | | | | | | | | |
| This salary schedule is effective July 1, 2024 through June 30, 2025 | | | | | | | | | |
| (Reflects salary for no certificate or incentives) | | | | | | | | | |
| | Step A | Step B | Step C | Step D | Step E | Step F | Step G | Step H | Step I |
| Records Specialist | | | 4496 | 4676 | 4861 | 5054 | 5259 | 5470 | 5692 |
| Patrol Officer | 5447 | 5632 | 5840 | 6055 | 6283 | 6535 | 6796 | 7071 | 7353 |



Where Life is Sweet

Mayor and Members of the City Council
STAFF REPORT
 For the Meeting of September 26, 2022

Title/Subject

Facade Grant- Stanton Smith 4N2811CB Tax Lot 6900- 395 E Main St

Summary and Background

The Hermiston Urban Renewal Agency has received a façade grant application for exterior improvements to commercial property located at 395 E Main St. A façade grant was awarded to the property owners for the adjacent property in January of 2022 and similar improvements are proposed for this property. The eligible improvements proposed include new storefront, windows, façade, stucco, parapet, and light fixtures to the E Main St frontage of the building. Exterior improvements to the NE 4th St and alley frontages are proposed in future phases. Additional remodeling is proposed for the interior of the building. The estimated budget for the eligible improvements is \$81,323. The applicant has requested an award of \$40,000. Under the terms of the grant program, the district may provide a match of up to 50% of the project budget up to a maximum award of \$10,000. Projects which have private investment in excess of \$75,000 that benefit the city as a whole and not just the district, may increase the total match at the discretion of the committee. The planning commission considered and scored the grant request at the September 14 regular meeting. The planning commission's scoring (attached) reached a recommended match of 50%. The planning commission discussed the improvements with the applicant. The planning commission considered inflationary pressure on construction costs, overall impact to the downtown, lack of applications for grants, and other factors, and agreed with the applicant to recommend going over the \$10,000 cap. Although the planning commission recommended granting \$40,000 once improvements are completed, the city council acting as the urban renewal agency board may choose to approve the grant as recommended or change the grant amount.

Tie-In to Council Goals

Urban renewal and downtown revitalization are council goals.

Fiscal Information

There is sufficient funding in the façade grant budget for 2022-2023 to accommodate the recommended grant award. Funds which are not expended at the end of each fiscal year roll into the next fiscal year.

Alternatives and Recommendation**Alternatives**

The committee may choose to accept the planning commission recommendation of a \$40,000 grant award or modify the grant amount.

Recommended Action/Motion

The proposed façade improvements will improve the appearance of the building and downtown. When considering the amount of the grant award the committee should determine if the project's improvements provide a benefit to the entire city or only the district.

Motion to approve the grant award for the exterior improvements to 395 E Main St in the amount of \$40,000 or as may be modified by the committee.

Submitted By:

Clinton Spencer

Hermiston Urban Renewal Agency

Application for Façade Improvement Grant

Name of Applicant

SA Smith LLC- Amy Stanton/Slade Smith

Property Address

395 East Main Street

Property Owner
(if different)Owner Address
(if different)

490 Christiansen Loop- Hermiston, OR 97838

Business Name

Simply Divine Suites (And another future tenant in this building)

Phone Number

541-720-5372

E-Mail

slade4623@gmail.com

Project Description

*Please address the scoring
criteria if possible. Use
additional pages if necessary.*

We are proposing to do a complete exterior facelift of this property consistant with the one done earlier this year to the neighboring property. New storefront, new windows, new stucco, new wood grain aluminum parapet, and new exterior lighting. Please reference attached market of current condition photo.

Grant Request

\$40,000

Project Budget

\$81,323 plus additional
\$50,000 during next phase

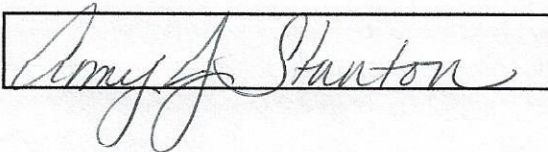
Start Date

9/1/2022

End Date

11/15/2022

Applicant Signature



Date

8/23/2022

Please attach to this form copies of the project budget, construction drawings, property owner consent, an itemized list of improvements, and other items that are pertinent to the project.

Hermiston Urban Renewal Agency

Application for Façade Improvement Grant

Colors proposed

White stucco, Dark Grey stucco, wood grain aluminum parapet, Black store front, black grid commercial windows.

Describe building materials proposed

As described above

Explain how the proposed improvements are consistent with the type of architectural features found in the existing historical buildings in the district.

Proposed improvements are intended to extend to appearance of the adjacent improved building at 379 E Main which have been received very well and have generated increased foot traffic at the east end of the historic district and to current tenant of that property (Sassafras Flowers)

Does the proposed improvement contain features designed to contribute to aesthetic enhancement?

We feel that a face-lift with proposed material and colors will substantially improve aesthetics to the building which has extremely strong structure and was originally built as the Hermiston Post Office in 1956.

Will the proposed improvements contribute not only to the enhancement of the commercial use, but the downtown as a whole?

These improvements will substantially increase the appearance and attraction at the East end of the Main Street historical district and will encourage our community members to travel the full length of the district.

Hermiston Urban Renewal Agency

Application for Façade Improvement Grant

Are the proposed improvements compatible with the overall downtown character?

These improvements are compatible with where we believe the future of our community is headed and they stay consistent with the improvements we did to the neighboring building earlier this year. We are receiving enormous amounts of positive feedback about the previous project and this proposed current project.

Are the proposed improvements designed to contribute to the long-term health of the district? For example, landscape improvements are not as durable as masonry.

Proposed improvements are long term. We have chosen all long term finishes including stucco, wood grain aluminum vs. real wood which can deteriorate, commercial grade storefronts and windows vs. residential grade.

Will the proposed improvements enhance the value of the property and provide a good return on investment for the district?

These improvements will add substantial value to the property. There will also be another phase to the improvements of this property early next year which will include a complete, matching face-lift of the 4th street side of this building as well as the backside of the building which contains parking and borders the alley.

Façade Grant Application ReviewApplicant SA Smith-Amy Stanton/Slade Smith

Review Date _____

Scoring 0 pts to 10 pts

0 points = Does not meet the criteria

5 points = Meets most of the criteria

10 points = Fully meets the criteria

| Criteria | Score |
|------------------------------------------------------------------------------------------------------------------------------------------------------------------|-------|
| Are the proposed improvements consistent with type of architectural features found in the existing historical buildings in the district | |
| Does the proposed improvement contain features designed to contribute to aesthetic enhancement? | |
| Will the proposed improvements contribute not only to the enhancement of the commercial use, but the downtown as a whole? | |
| Are the proposed improvements compatible with the overall downtown character? | |
| Are the proposed improvements designed to contribute to the long-term health of the district? For example, landscape improvements are not as durable as masonry. | |
| Will the proposed improvements enhance the value of the property and provide a good return on investment for the district? | |
| Total | |

The maximum score possible for a grant is 60 points. An application meeting all of the review criteria would be eligible for a full 50% match. An application meeting a portion of the criteria would be eligible for a lesser match percentage. Grants are considered for up to 50% of the project budget up to a maximum award of \$10,000. For example, a \$20,000 project would be eligible for a maximum of \$10,000 match. A \$15,000 project would be eligible for a maximum of \$7,500 match.

Based on the scoring by the committee, grants are awarded using the following percentages:

48-60 points: 50% match

36-47 points: 40% match

24-35 points: 30% match

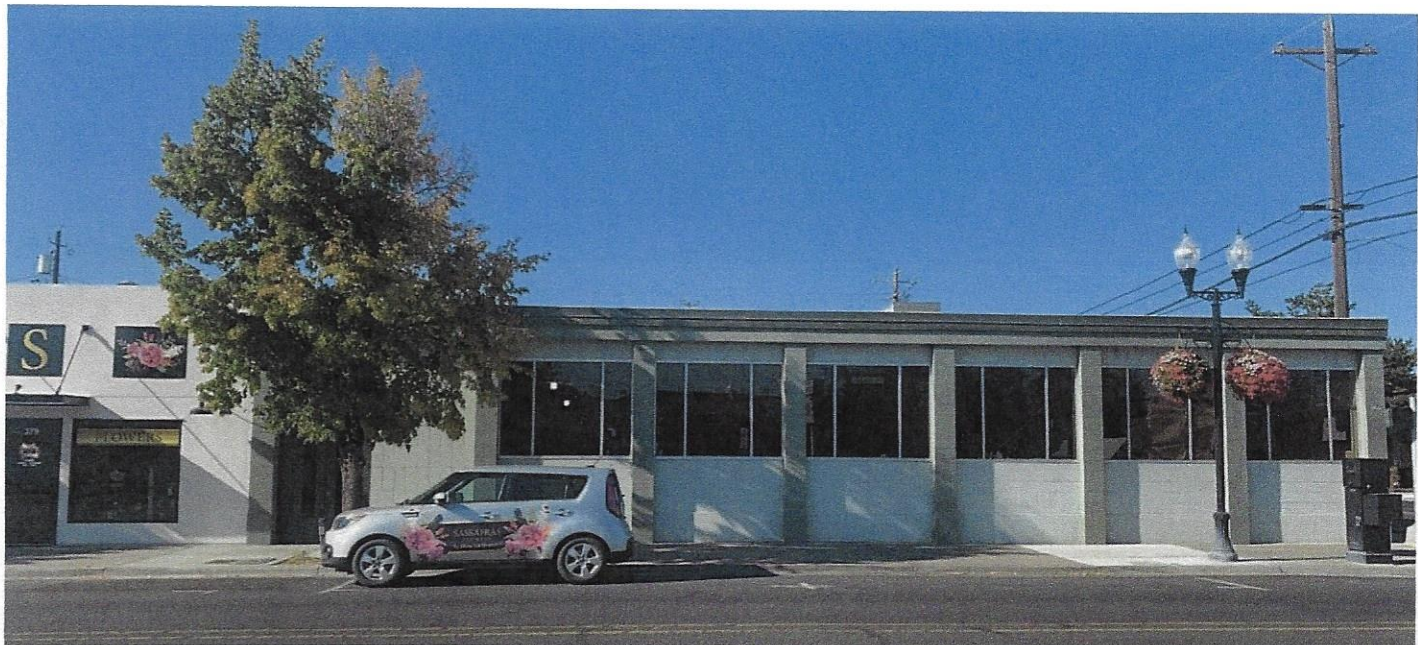
12-23 points: 20% match

1-11 points: 10% match

Schedule of Values

| Job Title: | Salon Studios | | Bid Date: Apr, 14th 2022 |
|------------|--------------------------------------------|----------|--------------------------|
| | | | 4:00 PM |
| Owner: | SA Smith LLC-Amy Stanton/Slade Smith | | 186.08 |
| | | | |
| Phase No | Description | Company | Total |
| 01017 | Permits & Fees | Owner | |
| 01715 | General Conditions | | |
| 02050 | Wall Demo | Est | |
| 02051 | Ceiling Demo | Est | |
| 02052 | CMU Demo | Est | |
| 02053 | Exterior Door(s) Demo | Est | |
| 02054 | Saw Cutting | A-Core | |
| 02055 | Concrete Slab Demo | Est | |
| 02200 | Excavation | WW | |
| 03120 | Slabs | Est | |
| 03210 | Rebar/Dowels | Est | |
| 04220 | CMU | Est | |
| 05510 | Metal Lintel | Ross | |
| 05511 | Metal Brackets | Est | |
| 06100 | Interior Framing | Est | |
| 06101 | Exterior Framing | Est | 6,178.00 |
| 06102 | Exterior Sheeting | Est | 2,790.00 |
| 06103 | Wood Façade | Est | 15,000.00 |
| 07210 | Wall & Ceiling Inso | Mr Inso | |
| 07920 | Sealants & Caulking | Est | |
| 07921 | Stucco | Ideal | 13,000.00 |
| 08110 | Timely Doors/Frames | Est | |
| 08410 | Aluminum Storefront | Superior | 21,705.00 |
| 08411 | Aluminum Ext Windows | Superior | 11,700.00 |
| 08520 | Salon Sliding Doors | Superior | |
| 08710 | Finish Hardware | Superior | |
| 08800 | Glass & Glazing | Superior | |
| 09260 | GWB | Foley | |
| 09510 | Acoustical Ceiling | ACI | |
| 09665 | LVP | Standard | |
| 09685 | Carpet | Standard | |
| 09686 | Rubber Base | Standard | |
| 09900 | Painting | A-Sharp | |
| 10200 | Roof Patch Allowance | Est | |
| 10201 | Parapet Cap Metal | Est | 2,950.00 |
| 10810 | Toilet Accessories | Dupree | |
| 15400 | Plumbing | WW | |
| 15401 | Plumbing Condensate | WW | |
| 15402 | Meter Allowance | Est | |
| 15800 | HVAC | EO Heat | |
| 15801 | Jumper Ducts | EO Heat | |
| 15801 | Gas Piping | EO Heat | |
| 16050 | Security | Est | |
| 16100 | Exterior light fixtures | Est | 8,000.00 |
| | Subtotal: | | |
| | Cost for Exterior at Main Street side only | | 81,323.00 |

Estimated Cost for 4th street exterior improvements during next phase- \$50,000



Façade Grant Application Review

Applicant Stanton/Smith 395 E. Main St
 Review Date Sept 14 2022

Scoring 0 pts to 10 pts

0 points = Does not meet the criteria

5 points = Meets most of the criteria

10 points = Fully meets the criteria

| Criteria | Score |
|------------------------------------------------------------------------------------------------------------------------------------------------------------------|-----------|
| Are the proposed improvements consistent with type of architectural features found in the existing historical buildings in the district | 8 |
| Does the proposed improvement contain features designed to contribute to aesthetic enhancement? | 10 |
| Will the proposed improvements contribute not only to the enhancement of the commercial use, but the downtown as a whole? | 10 |
| Are the proposed improvements compatible with the overall downtown character? | 10 |
| Are the proposed improvements designed to contribute to the long-term health of the district? For example, landscape improvements are not as durable as masonry. | 10 |
| Will the proposed improvements enhance the value of the property and provide a good return on investment for the district? | 10 |
| Total | 58 |

The maximum score possible for a grant is 60 points. An application meeting all of the review criteria would be eligible for a full 50% match. An application meeting a portion of the criteria would be eligible for a lesser match percentage. Grants are considered for up to 50% of the project budget up to a maximum award of \$10,000. For example, a \$20,000 project would be eligible for a maximum of \$10,000 match. A \$15,000 project would be eligible for a maximum of \$7,500 match.

Based on the scoring by the committee, grants are awarded using the following percentages:

48-60 points: 50% match
 36-47 points: 40% match
 24-35 points: 30% match
 12-23 points: 20% match
 1-11 points: 10% match

Commissioner Burns

Façade Grant Application ReviewApplicant Stanton Smith 395 E Main StReview Date Sept 14, 2022

Scoring 0 pts to 10 pts

0 points = Does not meet the criteria

5 points = Meets most of the criteria

10 points = Fully meets the criteria

| Criteria | Score |
|------------------------------------------------------------------------------------------------------------------------------------------------------------------|-----------|
| Are the proposed improvements consistent with type of architectural features found in the existing historical buildings in the district | 4 |
| Does the proposed improvement contain features designed to contribute to aesthetic enhancement? | 10 |
| Will the proposed improvements contribute not only to the enhancement of the commercial use, but the downtown as a whole? | 10 |
| Are the proposed improvements compatible with the overall downtown character? | 5 |
| Are the proposed improvements designed to contribute to the long-term health of the district? For example, landscape improvements are not as durable as masonry. | 10 |
| Will the proposed improvements enhance the value of the property and provide a good return on investment for the district? | 10 |
| Total | 49 |

The maximum score possible for a grant is 60 points. An application meeting all of the review criteria would be eligible for a full 50% match. An application meeting a portion of the criteria would be eligible for a lesser match percentage. Grants are considered for up to 50% of the project budget up to a maximum award of \$10,000. For example, a \$20,000 project would be eligible for a maximum of \$10,000 match. A \$15,000 project would be eligible for a maximum of \$7,500 match.

Based on the scoring by the committee, grants are awarded using the following percentages:

48-60 points: 50% match

36-47 points: 40% match

24-35 points: 30% match

12-23 points: 20% match

1-11 points: 10% match

Commissioner Caplinger

Façade Grant Application Review

Applicant Stanton / Smith 395 E Main St
 Review Date Sept 14, 2022

Scoring 0 pts to 10 pts

0 points = Does not meet the criteria

5 points = Meets most of the criteria

10 points = Fully meets the criteria

| Criteria | Score |
|------------------------------------------------------------------------------------------------------------------------------------------------------------------|-----------|
| Are the proposed improvements consistent with type of architectural features found in the existing historical buildings in the district | 9 |
| Does the proposed improvement contain features designed to contribute to aesthetic enhancement? | 10 |
| Will the proposed improvements contribute not only to the enhancement of the commercial use, but the downtown as a whole? | 9 |
| Are the proposed improvements compatible with the overall downtown character? | 8 |
| Are the proposed improvements designed to contribute to the long-term health of the district? For example, landscape improvements are not as durable as masonry. | 9 |
| Will the proposed improvements enhance the value of the property and provide a good return on investment for the district? | 9 |
| Total | 54 |

The maximum score possible for a grant is 60 points. An application meeting all of the review criteria would be eligible for a full 50% match. An application meeting a portion of the criteria would be eligible for a lesser match percentage. Grants are considered for up to 50% of the project budget up to a maximum award of \$10,000. For example, a \$20,000 project would be eligible for a maximum of \$10,000 match. A \$15,000 project would be eligible for a maximum of \$7,500 match.

Based on the scoring by the committee, grants are awarded using the following percentages:

48-60 points: 50% match

36-47 points: 40% match

24-35 points: 30% match

12-23 points: 20% match

1-11 points: 10% match

Commissioner Collins

Façade Grant Application Review

Applicant

Stanton/Smith 395 E Main St

Review Date

Sept 14 2022

Scoring 0 pts to 10 pts

0 points = Does not meet the criteria

5 points = Meets most of the criteria

10 points = Fully meets the criteria

| Criteria | Score |
|------------------------------------------------------------------------------------------------------------------------------------------------------------------|-----------|
| Are the proposed improvements consistent with type of architectural features found in the existing historical buildings in the district | 5 |
| Does the proposed improvement contain features designed to contribute to aesthetic enhancement? | 10 |
| Will the proposed improvements contribute not only to the enhancement of the commercial use, but the downtown as a whole? | 8 |
| Are the proposed improvements compatible with the overall downtown character? | 8 |
| Are the proposed improvements designed to contribute to the long-term health of the district? For example, landscape improvements are not as durable as masonry. | 10 |
| Will the proposed improvements enhance the value of the property and provide a good return on investment for the district? | 8 |
| Total | 49 |

The maximum score possible for a grant is 60 points. An application meeting all of the review criteria would be eligible for a full 50% match. An application meeting a portion of the criteria would be eligible for a lesser match percentage. Grants are considered for up to 50% of the project budget up to a maximum award of \$10,000. For example, a \$20,000 project would be eligible for a maximum of \$10,000 match. A \$15,000 project would be eligible for a maximum of \$7,500 match.

Based on the scoring by the committee, grants are awarded using the following percentages:

48-60 points: 50% match

36-47 points: 40% match

24-35 points: 30% match

12-23 points: 20% match

1-11 points: 10% match

Commissioner Doherty

Façade Grant Application Review

Applicant Stanton/Smith 395 E Main St.
 Review Date Sept 14 2012

Scoring 0 pts to 10 pts

0 points = Does not meet the criteria

5 points = Meets most of the criteria

10 points = Fully meets the criteria

| Criteria | Score |
|------------------------------------------------------------------------------------------------------------------------------------------------------------------|-----------|
| Are the proposed improvements consistent with type of architectural features found in the existing historical buildings in the district | 8 |
| Does the proposed improvement contain features designed to contribute to aesthetic enhancement? | 10 |
| Will the proposed improvements contribute not only to the enhancement of the commercial use, but the downtown as a whole? | 10 |
| Are the proposed improvements compatible with the overall downtown character? | 9 |
| Are the proposed improvements designed to contribute to the long-term health of the district? For example, landscape improvements are not as durable as masonry. | 8 |
| Will the proposed improvements enhance the value of the property and provide a good return on investment for the district? | 10 |
| Total | 55 |

The maximum score possible for a grant is 60 points. An application meeting all of the review criteria would be eligible for a full 50% match. An application meeting a portion of the criteria would be eligible for a lesser match percentage. Grants are considered for up to 50% of the project budget up to a maximum award of \$10,000. For example, a \$20,000 project would be eligible for a maximum of \$10,000 match. A \$15,000 project would be eligible for a maximum of \$7,500 match.

Based on the scoring by the committee, grants are awarded using the following percentages:

48-60 points: 50% match
 36-47 points: 40% match
 24-35 points: 30% match
 12-23 points: 20% match
 1-11 points: 10% match

Commissioner Fialka

Facade Grant Application Review

Applicant

Stanton/Smith 395 E Main St

Review Date

Sept 14 2022

Scoring 0 pts to 10 pts

0 points = Does not meet the criteria

5 points = Meets most of the criteria

10 points = Fully meets the criteria

| Criteria | Score |
|------------------------------------------------------------------------------------------------------------------------------------------------------------------|-----------|
| Are the proposed improvements consistent with type of architectural features found in the existing historical buildings in the district | 8 |
| Does the proposed improvement contain features designed to contribute to aesthetic enhancement? | 9 |
| Will the proposed improvements contribute not only to the enhancement of the commercial use, but the downtown as a whole? | 10 |
| Are the proposed improvements compatible with the overall downtown character? | 7 |
| Are the proposed improvements designed to contribute to the long-term health of the district? For example, landscape improvements are not as durable as masonry. | 10 |
| Will the proposed improvements enhance the value of the property and provide a good return on investment for the district? | 10 |
| Total | 54 |

The maximum score possible for a grant is 60 points. An application meeting all of the review criteria would be eligible for a full 50% match. An application meeting a portion of the criteria would be eligible for a lesser match percentage. Grants are considered for up to 50% of the project budget up to a maximum award of \$10,000. For example, a \$20,000 project would be eligible for a maximum of \$10,000 match. A \$15,000 project would be eligible for a maximum of \$7,500 match.

Based on the scoring by the committee, grants are awarded using the following percentages:

48-60 points: 50% match

36-47 points: 40% match

24-35 points: 30% match

12-23 points: 20% match

1-11 points: 10% match

Commissioner Hamm

Façade Grant Application ReviewApplicant Stanton/Smith 395 E Main StReview Date Sept 14 2022

Scoring 0 pts to 10 pts

0 points = Does not meet the criteria

5 points = Meets most of the criteria

10 points = Fully meets the criteria

| Criteria | Score |
|------------------------------------------------------------------------------------------------------------------------------------------------------------------|-----------|
| Are the proposed improvements consistent with type of architectural features found in the existing historical buildings in the district | 7 |
| Does the proposed improvement contain features designed to contribute to aesthetic enhancement? | 9 |
| Will the proposed improvements contribute not only to the enhancement of the commercial use, but the downtown as a whole? | 9 |
| Are the proposed improvements compatible with the overall downtown character? | 9 |
| Are the proposed improvements designed to contribute to the long-term health of the district? For example, landscape improvements are not as durable as masonry. | 10 |
| Will the proposed improvements enhance the value of the property and provide a good return on investment for the district? | 10 |
| Total | 54 |

The maximum score possible for a grant is 60 points. An application meeting all of the review criteria would be eligible for a full 50% match. An application meeting a portion of the criteria would be eligible for a lesser match percentage. Grants are considered for up to 50% of the project budget up to a maximum award of \$10,000. For example, a \$20,000 project would be eligible for a maximum of \$10,000 match. A \$15,000 project would be eligible for a maximum of \$7,500 match.

Based on the scoring by the committee, grants are awarded using the following percentages:

48-60 points: 50% match

36-47 points: 40% match

24-35 points: 30% match

12-23 points: 20% match

1-11 points: 10% match

Commissioner Kirkpatrick

Façade Grant Application Review

Applicant Stanton / Smith 395 E Main St
 Review Date Sept 14, 2022

Scoring 0 pts to 10 pts

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10 points = Fully meets the criteria

| Criteria | Score |
|------------------------------------------------------------------------------------------------------------------------------------------------------------------|-----------|
| Are the proposed improvements consistent with type of architectural features found in the existing historical buildings in the district | 7 |
| Does the proposed improvement contain features designed to contribute to aesthetic enhancement? | 10 |
| Will the proposed improvements contribute not only to the enhancement of the commercial use, but the downtown as a whole? | 10 |
| Are the proposed improvements compatible with the overall downtown character? | 9 |
| Are the proposed improvements designed to contribute to the long-term health of the district? For example, landscape improvements are not as durable as masonry. | 10 |
| Will the proposed improvements enhance the value of the property and provide a good return on investment for the district? | 10 |
| Total | 56 |

The maximum score possible for a grant is 60 points. An application meeting all of the review criteria would be eligible for a full 50% match. An application meeting a portion of the criteria would be eligible for a lesser match percentage. Grants are considered for up to 50% of the project budget up to a maximum award of \$10,000. For example, a \$20,000 project would be eligible for a maximum of \$10,000 match. A \$15,000 project would be eligible for a maximum of \$7,500 match.

Based on the scoring by the committee, grants are awarded using the following percentages:

48-60 points: 50% match

36-47 points: 40% match

24-35 points: 30% match

12-23 points: 20% match

1-11 points: 10% match

Commissioner Sargent

Façade Grant Application Review

Applicant

Stanton Smith 395 E Main St

Review Date

Sept 14 2022

Scoring 0 pts to 10 pts

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5 points = Meets most of the criteria

10 points = Fully meets the criteria

| Criteria | Score |
|------------------------------------------------------------------------------------------------------------------------------------------------------------------|-----------|
| Are the proposed improvements consistent with type of architectural features found in the existing historical buildings in the district | 9 |
| Does the proposed improvement contain features designed to contribute to aesthetic enhancement? | 10 |
| Will the proposed improvements contribute not only to the enhancement of the commercial use, but the downtown as a whole? | 8 |
| Are the proposed improvements compatible with the overall downtown character? | 10 |
| Are the proposed improvements designed to contribute to the long-term health of the district? For example, landscape improvements are not as durable as masonry. | 9 |
| Will the proposed improvements enhance the value of the property and provide a good return on investment for the district? | 8 |
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The maximum score possible for a grant is 60 points. An application meeting all of the review criteria would be eligible for a full 50% match. An application meeting a portion of the criteria would be eligible for a lesser match percentage. Grants are considered for up to 50% of the project budget up to a maximum award of \$10,000. For example, a \$20,000 project would be eligible for a maximum of \$10,000 match. A \$15,000 project would be eligible for a maximum of \$7,500 match.

Based on the scoring by the committee, grants are awarded using the following percentages:

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36-47 points: 40% match

24-35 points: 30% match

12-23 points: 20% match

1-11 points: 10% match

Chairman Saylor