

****PUBLIC NOTICE****



**CITY COUNCIL WORKSHOP & REGULAR SESSION,
CRIME CONTROL & PREVENTION DISTRICT, FIRE
CONTROL, PREVENTION AND EMERGENCY
MEDICAL SERVICES DISTRICT**

Thursday, July 15, 2021 at 5:30 PM

City Hall | 3300 Corinth Parkway

Pursuant to section 551.127, Texas Government Code, one or more council members or employees may attend this meeting remotely using videoconferencing technology. The videoconferencing technology can be accessed at www.cityofcorinth.com/remotesession. The video and audio feed of the videoconferencing equipment can be viewed and heard by the public at the address posted above as the location of the meeting.

A. NOTICE IS HEREBY GIVEN of a Workshop Session of the Corinth City Council, located at Corinth City Hall, 3300 Corinth Parkway, Corinth, Texas.

B. CALL TO ORDER

C. WORKSHOP AGENDA

1. Receive a report and hold discussions on the Public Works Department.
2. Receive a presentation on the activity for SPAN for 2020.
3. Discuss Regular Meeting Items on Regular Session Agenda, including the consideration of closed session items as set forth in the Closed Session agenda items below.

D. CLOSED SESSION

The City Council will convene in such executive or closed session to consider any matters regarding any of the above agenda items as well as the following matters pursuant to Chapter 551 of the Texas Government Code. After discussion of any matters in closed session, any final action or vote taken will be public by the City Council. City Council shall have the right at any time to seek legal advice in Closed Session from its Attorney on any agenda item, whether posted for Closed Session or not.

Section 551.071. (1) Private consultation with its attorney to seek advice about pending or contemplated litigation; and/or settlement offer; and/or (2) a matter in which the duty of the attorney to the government body under the Texas Disciplinary Rules of Professional Conduct of the State of Texas clearly conflict with Chapter 551.

a. Agenda Item S7.

E. RECONVENE IN OPEN SESSION TO TAKE ACTION, IF NECESSARY, ON CLOSED SESSION ITEMS

F. NOTICE IS HEREBY GIVEN of a meeting of the City of Corinth Crime Control and Prevention District.

G. CALL TO ORDER

H. BUSINESS AGENDA

1. Consider and act on minutes from January 28, 2021.

2. Hold a public hearing on the fiscal year 2021-2022 Proposed Budget for the City of Corinth Crime Control and Prevention District.
3. Consider and act on the fiscal year 2021-2022 budget for the City of Corinth Crime Control and Prevention District.
4. Receive a report and hold discussions on the Police Department operational activity.

I. ADJOURN

J. NOTICE IS HEREBY GIVEN of a meeting of the Fire Control, Prevention, and Emergency Management District.

K. CALL TO ORDER

L. BUSINESS AGENDA

1. Consider and act on minutes from January 28, 2021.
2. Hold a public hearing on an order of the Board of the Fire Control, Prevention, and EMS District establishing rules for the adoption of the Fire Control, Prevention, and EMS District Budget and establishing the fiscal year.
3. Consider and act on an Order of the Board of the Corinth Fire Control, Prevention, and EMS District establishing rules for the adoption of the Fire Control, Prevention, and EMS District Budget and establishing the fiscal year.
4. Hold a public hearing on the fiscal year 2021-2022 Proposed Budget for the Fire Control, Prevention & EMS District.
5. Consider and act on the fiscal year 2021-2022 budget for the Fire Control, Prevention, and Emergency Management Services District.
6. Receive a report and hold discussions on the impact, efficiency, and effectiveness of the fire control, prevention, and emergency medical services programs.

M. ADJOURN

N. NOTICE IS HEREBY GIVEN of a Regular Session meeting of the City Council.

O. CALL TO ORDER, INVOCATION, PLEDGE OF ALLEGIANCE & TEXAS PLEDGE

P. CITIZENS COMMENTS

Please limit your comments to three minutes. Comments about any of the Council agenda items are appreciated by the Council and may be taken into consideration at this time or during that agenda item. Council is prohibited from acting on or discussing items brought before them at this time.

Q. CONSENT AGENDA

All matters listed under the consent agenda are considered to be routine and will be enacted in one motion. Should the Mayor or a Councilmember desire discussion of any item, that item will be removed from the Consent Agenda and will be considered separately.

1. Consider and act on minutes from July 1, 2021.

2. Consider and act on the SPAN Transportation contract for transportation services for senior citizen and Denton County Veterans for fiscal year 2021-2022, authorizing the City Manager to execute the necessary documents.
3. Consider and act on the Fiscal Year 2022 Annual Budget for the City of Corinth Crime Control and Prevention District.
4. Consider and act on the fiscal year 2021-2022 budget for the Fire Control, Prevention, and Emergency Management Services District.
5. Consider approval of an Interlocal Agreement with Denton County for the collection of property taxes for the City of Corinth.

R. BUSINESS AGENDA

S. PUBLIC HEARING

6. Consider and take action to rename Dobbs Road between Quail Run Drive and I-35E Frontage Road to Lake Sharon Drive, to be known as the 3600 to 4000 blocks of Lake Sharon Drive.
7. Conduct a Public Hearing to consider testimony and take action on a rezoning request to amend the zoning classification from PD-6 Planned Development District, Ordinance No. 87-12-17-24, for Two Family Garden Homes, Townhomes, and Neighborhood Shopping and PD 24 Planned Development District, Ordinance No. 99-12-16-45 for Two Family Garden Homes to PD Planned Development District with a base zoning district of MF-1 Multi-Family Residential, on approximately 24.595 acres of land within the A.H. Serren Survey, Abstract No. 1198 and the B. Merchant Survey, Abstract No. 800, City of Corinth, Denton County, Texas. The property is generally located at the northwest corner of Lake Sharon Drive and Oakmont Drive and east of FM 2499. (Avilla Fairways PD ZAPD20-0004)

1. Staff Presentation
2. Applicant Presentation
3. Public Hearing
4. Take Action

T. COUNCIL COMMENTS & FUTURE AGENDA ITEMS

The purpose of this section is to allow each Council Member the opportunity to provide general updates and/or comments to fellow Council Members, the public, and/or staff on any issues or future events. Also, in accordance with Section 30.085 of the Code of Ordinances, at this time, any Council Member may direct that an item be added as a business item to any future agenda.

U. ADJOURN

Posted on this 12th day of July 2021, at 5:30 P.M. on the bulletin board at Corinth City Hall.



Lana Wylie, City Secretary
City of Corinth, Texas

CITY OF CORINTH
Staff Report

Meeting Date:	7/15/2021	Title:	Department Overview
Strategic Goals:	<input checked="" type="checkbox"/> Citizen Engagement <input checked="" type="checkbox"/> Proactive Government <input checked="" type="checkbox"/> Organizational Development		
Governance Focus:	<i>Sub-Ends:</i>		
	<input type="checkbox"/> Growing Community <input type="checkbox"/> Conveniently located <input checked="" type="checkbox"/> Delivers Outstanding Service <input type="checkbox"/> High-Quality Retail <input type="checkbox"/> High-Quality Restaurants <input type="checkbox"/> High-Quality Entertainment		
	<i>Focus:</i> <input checked="" type="checkbox"/> Owner <input type="checkbox"/> Customer <input type="checkbox"/> Stakeholder		
	<i>Decision:</i> <input checked="" type="checkbox"/> Governance Policy <input checked="" type="checkbox"/> Ministerial Function		
Owner Support:	<input type="checkbox"/> Planning & Zoning Commission <input type="checkbox"/> Economic Development Corporation <input type="checkbox"/> Parks & Recreation Board <input type="checkbox"/> TIRZ Board #2 <input type="checkbox"/> Finance Audit Committee <input type="checkbox"/> TIRZ Board #3 <input type="checkbox"/> Keep Corinth Beautiful <input type="checkbox"/> Ethics Commission n/a		

Item/Caption

Receive a report and hold discussions on the Public Works Department.

Item Summary/Background/Prior Action

The City of Corinth’s Public Works Department has 31 employees dedicated to maintaining and developing Corinth’s infrastructure. The diverse department is made of five operating divisions who work in close coordination to deliver services to over 22,000 residents every day. The department is responsible for the planning, design, construction, and maintenance of the city’s network of roads, water, wastewater, and stormwater pipelines, as well as sixteen parks.

The Director of Public Works will provide a presentation on the accomplishments, workload, and challenges for each department.

Staff Recommendation/Motion

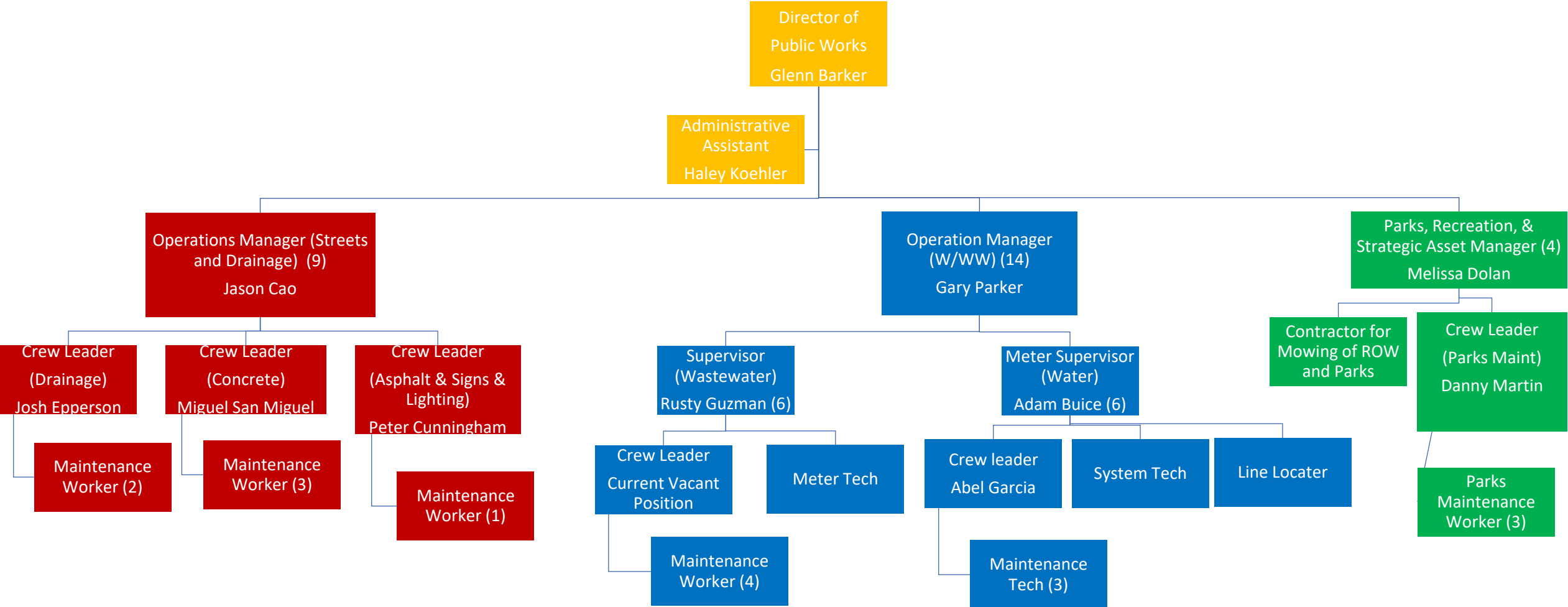
N/A

PUBLIC WORKS DEPARTMENT OVERVIEW

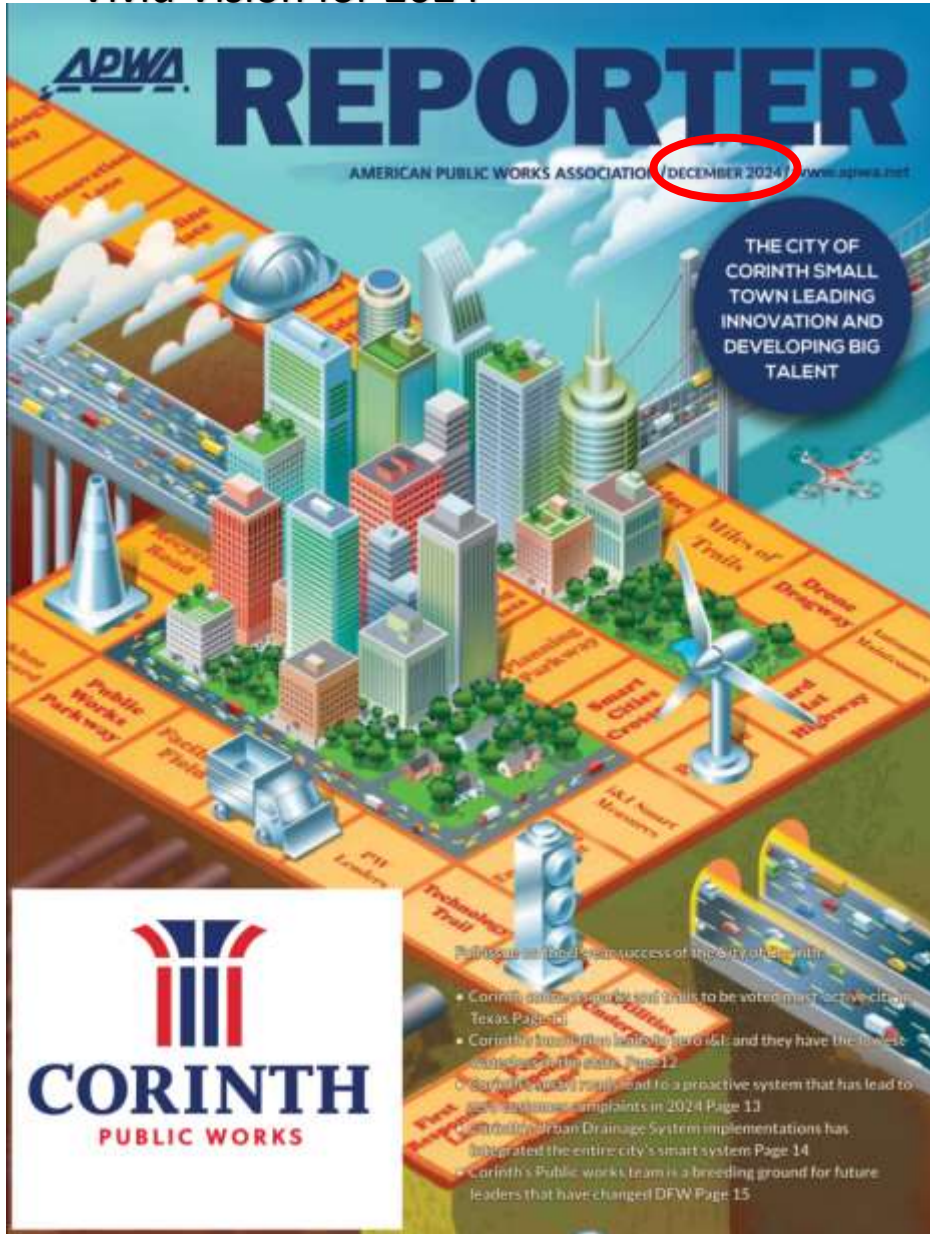
GLENN BARKER, DIRECTOR

The Department has multi-faceted services comprised of the following: Maintain the City Street System through proactive maintenance; preserve and enhance the City's quality of life through landscaping and park amenities in the community's public open spaces; maintain the City's storm and wastewater collection system to provide a reliable system that protects the residents and the environment; and to provide potable water at adequate pressure and in sufficient quantities to the residents of Corinth.

PUBLIC WORKS ORGANIZATION CHART



Vivid Vision for 2024



Mission Statement for i³

Public Works Initiative to use Technology to make a positive impact towards the future of the City of Corinth by focusing on the residents, improving services, and maintaining the budget as the City continues to grow.

- Develop goals and strategies that use technology to answer current and future problems.
- Customer Service.
- Employee training that develops the i³ program from within and becomes a culture.
- Technology to reduce the need for added manpower
- Optimizing budget.
- Access to information
- Improving performance and accountability
 - Water loss
 - I&I
 - Street Maintenance
- Asset Management
 - Replacement of equipment based on risk assessment vs equipment age.
- Use the existing technology to develop logistical planning.

i³ INTELLIGENT INFRASTRUCTURE INITIATIVE

Section C, Item 1.



City's 2018 AMP

Resident's Work Orders

- Public Works' Projects
- Daily Workload Measures
- Preventative Maint Tasks

Tracking Maintenance
Cityworks
Empowering GIS



Inspections - Storm and wastewater pipelines

City's Water & Wastewater Model
Innovyze
Empowering water experts
wastewater velocity,
water age, system pressures

ARCGIS Collector Mapping



Parks and Trails Maintenance and Growth

- i³ Targets**
- Technology monitoring systems
 - Budget Planning
 - Asset Replacement Schedule
 - 3D Modeling

Document control
Laserfiche



i³ BIG Ideas

- Intelligent lighting systems for all fields
- LED lighting (Grant Research)
- Smart irrigation controllers
- Scheduling software that manages fields
- Interactive Trail (Grant Research)
- Autonomous Mowers & Stripers
- Drone fertilizing & herbicide spraying
- Smart Trashcans (Grant Research)

Accomplishments for FY2020-21

- Implemented Community Park Preventive Maintenance Plan
- Renewed Sports Association (soccer, baseball/softball) Co-Sponsorship agreements.
- Improved Field Maintenance (Fences, Field prep, New soccer goals)

Goals & Objectives for FY2021-22

- Implement City of Corinth maintenance program for Katy trail
- Connect Katy trail to Community Park Trail with trail head and way finder
- Start 3 year plan to add fillable water stations at all parks that currently have fountains
- Implement neighborhood Preventive Maintenance Plan using Cityworks to track
- Complete APWA re-accreditation
- Add ROW and new parks to contract
- Become fully staffed
- Update Asset Inventory into Asset Tiger
- Develop Key Performance Indicators (KPI's) for Public Works with dashboard transparency



- **Trails**
 - 10.3 miles
 - Dorba Bike Trails
- **Parks**
 - 175 acres of public parkland
 - 15 neighborhood parks
 - 13 playgrounds
 - 10 miles of ROW maintained
 - Athletic Fields Prepped for 875 Games
- **Recreation**
 - 64* Class Participants
 - *Down from 272 due to COVID-19
 - 0* Summer Camp Participants due to COVID-19
 - *Down from 2,874 from Summer Camp 2019
 - 16,318* Association Athletic Participants due to COVID-19
 - *Down from 21,012 FY20, FY19-9770

70 Work Orders- Average Resolution Time - 6 Days



i³ BIG Ideas

- Smart Street Crossings at Trails
- Smart irrigation controllers for ROWs
- Signs that communicate with cars
- Report Card for Sidewalk

Accomplishments for FY2020-21

- Purchased and implemented Cityworks, a work order and asset management software system.
- Completed year 2 of road striping program
- Awarded second-year compliance for Tree City USA
- 550 LF of sidewalks replaced & 385 LF of new sidewalks added
- 21,500 square yards of street reconstruction (Fugro Assessment Year 1)

Goals & Objectives for FY2021-22

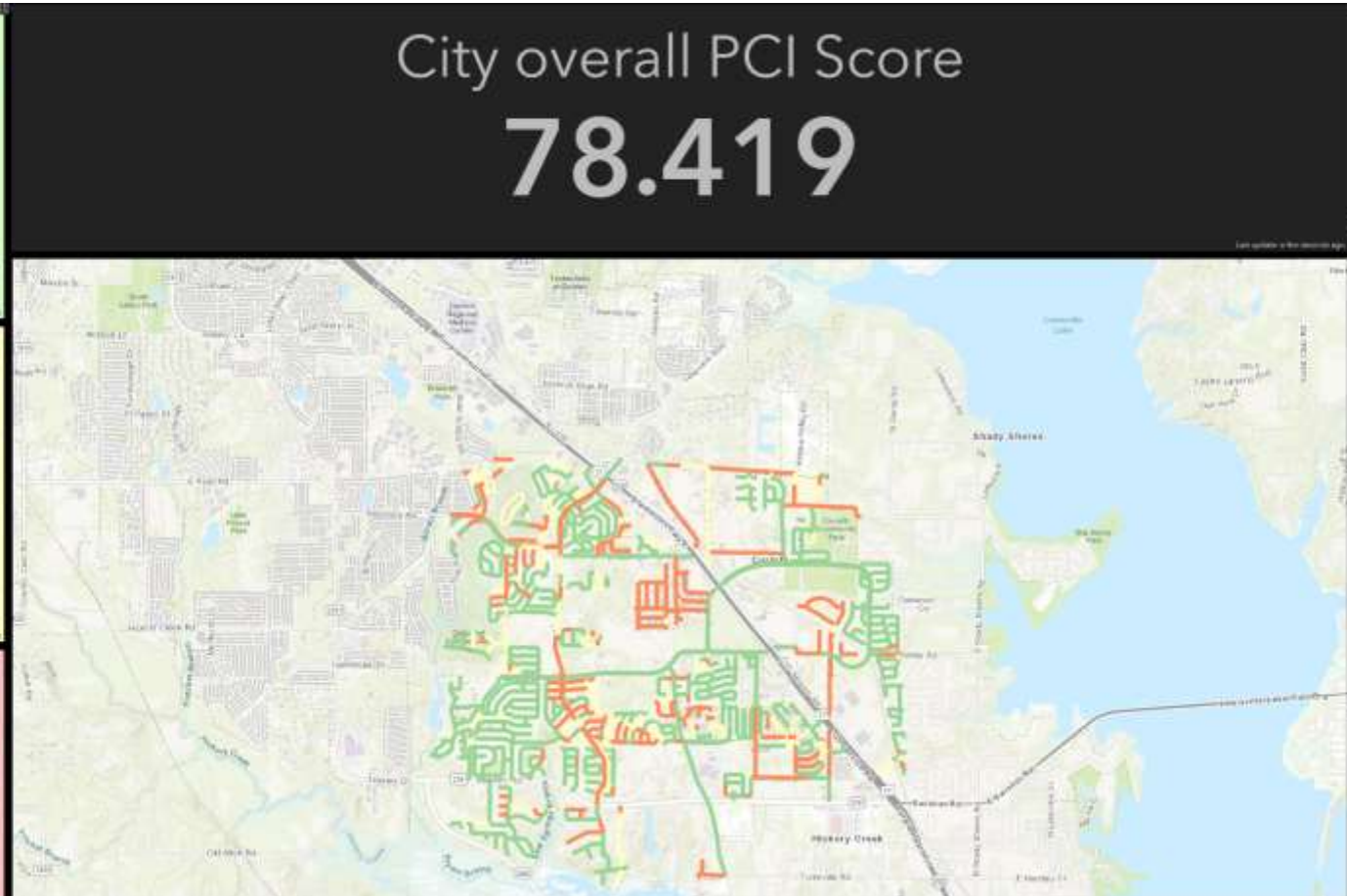
- Fog seal/Crack Seal 100% of ACP
- Install new sidewalk from Community Park to future Agora site
- Complete year 3 of striping program using Cityworks to develop mapping and plan
- Complete/update tree inventory
- Complete APWA re-accreditation
- Increase mowing contract to include; 2499, Lake Sharon extension, and Amherst Park
- Identify and repair sidewalks that pose a risk (trip hazard) to pedestrians, track using Cityworks or Collector.
- Update Asset Inventory into Asset Tiger
- Develop Key Performance Indicators (KPI's) for Public Works with dashboard transparency

2021 Streets Report Card

Street condition Good (LF)
283,784.2

Street condition Fair (LF)
74,971.3

Street condition Poor (LF)
117,316.3



Concrete Streets (LF)
405,192.8

Concrete Streets AVG PCI Rating
82.826

Asphalt Streets (LF)
70,879

Asphalt Streets AVG PCI Rating
51.208

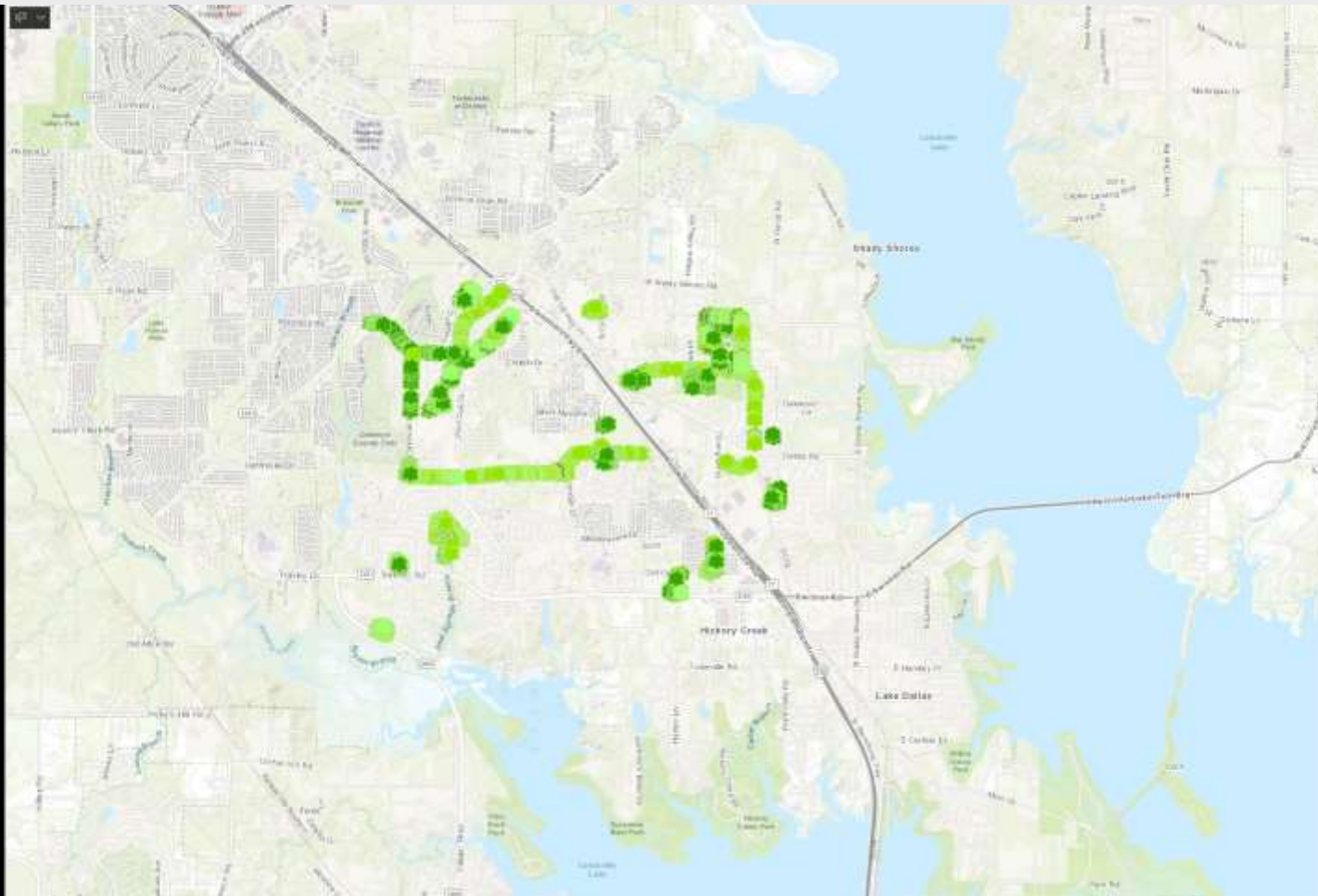
- Customer Calls - 150 (2024 Goal <30)
- 2022 Goal to develop Report Card for Sidewalks

FY21-22 Goal is to increase this by 15 points

2021 Tree Report Card

Dead or Poor Trees
360

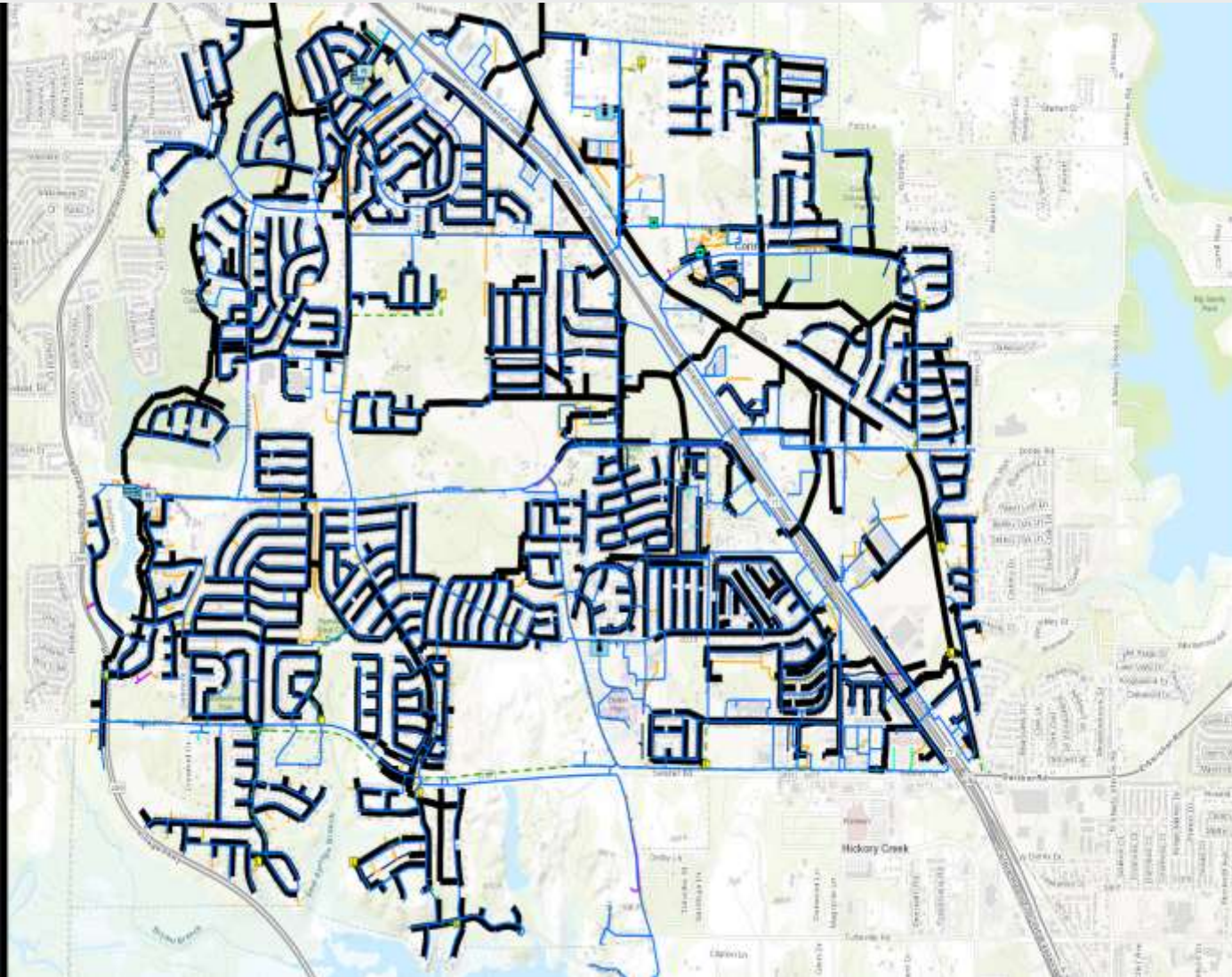
City of Corinth's Tree Count
7,482



- Most of these “dead” trees are in heavy wooded areas
 - FY2022 we will focus only on inventory of ROW’s and Parks
- Crews update the system as needed

Drainage, Water, and Wastewater Systems Overview

Fire hydrant 951	Valve Count 2,938	Pipeline FT 636,284.438
Sewer Valves 5	LF of Force Mains 47,818.929	WW Piping 552,290.421
Manhole 1,562	Storm Inlets 1,502	Stormwater 234,355.596
Detention ponds 30	Retention ponds 4	LF of Open Channels 10,735.281



10 Year Maintenance Plan List
H2O Linear FT >50 Years Old 636,284.438
WW Piping >8 89,825.289
Stormwater >45 37,235.61

- **TBD- Benchmarking report to compare the City with other like Cities; informational highlights will go here.**
- Corinth has 20 employees to manage 1.4 Million LF of pipeline (Avg is 27 employees per 1 Million LF)



i³ BIG Ideas

- Remote monitors at Outfalls
- Autonomous Mowers
- Drainage Report Card
- Prof. Drainage Cert for Lead
- Smart Ponds

Accomplishments for FY2019-20

- Extended Mosquito abatement contract
- Continue public education campaign on preventing storm water pollution
- Met or exceed TCEQ Stormwater Compliance and regulations and BMP's

Goals & Objectives for FY2020-21

- Develop a dry weather screening program within Cityworks
- Update the SWMP online
- Continue to meet or exceed TCEQ Storm water Compliance and regulations and BMP's
- Secure new mosquito abatement 5-year contract
- Continue public education campaign on preventing storm water pollution
- CCTV 8000 LF of drainage pipeline
- Update Asset Inventory into Asset Tiger
- Use Collector or Cityworks to track mowing, grading and inspections.



1000 Storm Inlet Inspections



65 Storm Inlets Cleaned



16 Work Orders Performed

23,000 linear feet of channel mowing

1,598 linear feet of channel cleaning & grading



16 Work Orders- Average Resolution Time - 4 Days



i³ BIG Ideas

- Pressure monitoring for early leak detection
- Smart Flushers
- Manhole monitoring for smart I&I measures
- Smart Valve Trailer for tracking valve maintenance directly to Cityworks
- Water & Wastewater Report Card

Accomplishments for FY2020-21

- Maintained a superior/outstanding water rating with TCEQ
- Phase 3 of the upgrade to the City's utility meter reading system to a single point meter reading system
- Continue to update GIS for new water/wastewater lines and older lines that have been repaired.
- Performed Inflow & Infiltration testing within the 3A basin

Goals & Objectives for FY2021-22

- Develop fire hydrant/valve maintenance program using Cityworks software
- Integrate a leak detection plan with Cityworks for water mains per AMP.
- Complete interconnect with LCUMA
- Inspect and clean 12,000 LF of the wastewater collection system
- Increase Maintenance hole inspections to 60 per year, use Cityworks and collector to track inspections.
- Upgrade meters that are 15 years or older. Use Cityworks to track age and replacement requirements.
- Integrate pump replacement in lift stations that have pumps older than 20 years. Use Cityworks to track age and replacement requirements.
- Maintained a superior water rating with TCEQ.
- Utility Network Software with GIS
- Complete APWA re-accreditation
- Non-revenue water loss shall be less than 15% of purchase water

Water /Wastewater



4 Water Main Repairs (82 service line repairs)



Water Loss Control Program
10% Non-revenue water loss (<15%)



645 MG of Wastewater Discharged
.329 MGD to Denton



4,490 Water Quality Sample Tests



Subscription usage
Water – 39% annually
2.94 MGD (one day was above 90% of 7.5MGD)
Wastewater – 80% annually 1.4 MGD (4.8MGD)



Flushed dead end lines
475 times (4.8 MG)



1.071 Billion Gallons of Water Purchased



7,347 Water Accounts



7081 Meters are AMI
266 Meters are AMR (<100)

6,213 Work Orders-Average Resolution Time - 4 Days

CITY OF CORINTH
Staff Report



Meeting Date:	7/15/2021	Title:	Review Span
Strategic Goals:	<input type="checkbox"/> Citizen Engagement <input checked="" type="checkbox"/> Proactive Government <input type="checkbox"/> Organizational Development		
Governance Focus:	<i>Sub-Ends:</i>		
	<input type="checkbox"/> Growing Community <input type="checkbox"/> Conveniently located <input type="checkbox"/> Delivers Outstanding Service <input type="checkbox"/> High-Quality Retail <input type="checkbox"/> High-Quality Restaurants <input type="checkbox"/> High-Quality Entertainment		
	<i>Focus:</i> <input checked="" type="checkbox"/> Owner <input type="checkbox"/> Customer <input type="checkbox"/> Stakeholder		
	<i>Decision:</i> <input type="checkbox"/> Governance Policy <input checked="" type="checkbox"/> Ministerial Function		
Owner Support:	<input type="checkbox"/> Planning & Zoning Commission <input type="checkbox"/> Economic Development Corporation <input type="checkbox"/> Parks & Recreation Board <input type="checkbox"/> TIRZ Board #2 <input type="checkbox"/> Finance Audit Committee <input type="checkbox"/> TIRZ Board #3 <input type="checkbox"/> Keep Corinth Beautiful <input type="checkbox"/> Ethics Commission <u>N/A</u>		

Item/Caption

Receive a presentation on the activity for SPAN for 2020.

Item Summary/Background/Prior Action

The City of Corinth contracts with SPAN Transportation; they provide transportation services to senior citizens and Denton County Veterans. Diane Beck, Mobility Manager, will be present to provide an overview of the service and projected usage.

Financial Impact

There is no direct cost to the City as SPAN utilizes grant funding to pay for the service.

Applicable Owner/Stakeholder Policy

Staff Recommendation/Motion

To authorize the city manager to execute the contract on behalf of the City.

STATE OF TEXAS §

COUNTY OF DENTON §

INTERLOCAL AGREEMENT FOR SERVICES

THIS SERVICE AGREEMENT ("Agreement") is entered into by and between Corinth, Texas, acting by and through its duly authorized City Manager (hereinafter referred to as "CITY") and SPAN, Inc., (hereinafter referred to as "SPAN"), a Texas non-profit corporation operating in Denton County, Texas as an organization described in Section 501(c)(3) of the Internal Revenue Code, acting by and through its duly authorized Executive Director.

WHEREAS, SPAN enables people to live as fully and independently as possible by providing nutrition, transportation and social services to older persons, persons with disabilities, veterans, and the general public; and

WHEREAS, the success of or failure of the SPAN's purposes and objectives has a direct impact on the health and welfare of the citizens of the City; and

WHEREAS, the City is charged with the responsibility of promoting and preserving the health, safety, peace, good government, and welfare of its citizens; and

WHEREAS, SPAN transportation services were developed to provide safe and efficient transportation to seniors, persons with special needs, veterans and as otherwise defined by agreements into which SPAN may enter from time to time; and

WHEREAS, The CITY and SPAN desire to enter into this Agreement whereby SPAN will provide demand response transit service for CITY residents that are seniors (age 65 or older), and people with documented disabilities (hereafter referred to collectively as "Riders"); and

WHEREAS, Riders in CITY may be taken anywhere in SPAN's demand response transit service area in Denton County at a cost to the Riders of \$3.00 for seniors (age 65 and older) and also for people with documented disabilities for the purposes of medical treatments, doctor's and dentist's appointments, trips to get prescriptions filled, shopping for necessities, travel to and from the Lake Dallas Public Library, Employment, Education, Nutrition, Recreation, and Workshop trips; and

WHEREAS, Riders may call in at least one (1) day in advance, but no more than two (2) weeks in advance, to set up appointments for pick-up and drop off by calling SPAN'S Transportation Office at 940-382-1900 weekdays between the hours of 8:00 a.m. and 2:00 p.m.; and

WHEREAS, Demand response transit service is available between the hours of 7:00 a.m. and 6:00 p.m. Monday through Friday excluding major holidays and subject to availability constraints.

NOW, THEREFORE, THE CITY AND SPAN DO HEREBY COVENANT AND AGREE AS FOLLOWS:

1. Recitals

The foregoing recitals are found to be true and correct, are fully incorporated into the body of this Agreement and made a part hereof by reference just as though they are set out in their entirety.

2. Scope of Transportation Services

SPAN shall provide door-to-door demand response transit services to CITY citizens residing in Denton County who are Riders in accordance with this Agreement and SPAN's "Transportation Policy and Procedures" which is attached hereto as Exhibit "A" and incorporated herein by reference as though it were set out in its entirety ("Policy"). In the event of conflict between this Agreement and the Policy, this Agreement shall control. In performing services under this Agreement, the relationship between the CITY and SPAN is that of an independent contractor. No term or provision of this Agreement or act of SPAN in the performance of this Agreement shall be construed as making SPAN the agent, servant, or employee of the CITY.

3. SPAN Transportation Operations

- a. SPAN shall provide all equipment, facilities, qualified employees, training, and insurance necessary to establish a demand response transit service for the CITY's Riders. SPAN shall further establish, operate, and maintain an accounting system for this program that will allow for a tracking of services provided to Riders and a review of the financial status of the program. SPAN shall also track and break down the information regarding the number of one-way trips it provides to Riders.
- b. The CITY shall have the right to review the activities and financial records kept incident to the services provided to the CITY's Riders by SPAN. In addition, SPAN shall provide monthly ridership information to the City Manager or his/her designee specifically identifying the number of Rider trips including rider origination, destination, and purpose.
- c. SPAN will be responsible for verifying and documenting the eligibility of Riders. SPAN reserves the right to determine on an individual basis whether SPAN has the capability to safely transport a passenger. If safety is compromised, SPAN may decline transportation for this person and must document the reason why service was declined.
- d. Span will inform riders that their trips to the doctor or dentist's office, hospital, drug store or other location may qualify as a Medicaid eligible trip.
- e. Span reserves the right to immediately terminate services without warning if a passenger poses a safety risk to himself/herself or any other person. Span also reserves the right to suspend or terminate riders who violate Span's cancellation policy.

4. Payment

- a. SPAN shall provide one-way trips to the Town riders of Shady Shores, Hickory Creek, Lake Dallas and Corinth using 5310 grant funds.

5. Indemnification

SPAN assumes all liability and responsibility for and agrees to fully indemnify, hold harmless and defend the CITY, and its officials, officers, agents, servants and employees from and against any and all claims, damages, losses and expenses, including but not limited to attorney's fees, for injury to or death of a person or damage to property, arising out of or in connection with, directly or indirectly, the performance, attempted performance or nonperformance of the services described hereunder or in any way resulting from or arising out of the management, supervision, and operation of the program and activities of SPAN. In the event of joint and concurring responsibility of SPAN and the CITY, responsibility and indemnity, if any, shall be apportioned comparatively in accordance with Texas Law, without waiving any defense of either party under Texas Law. The provisions of this paragraph are solely for the benefit of the parties hereto and are not intended to create or grant any rights, contractual or otherwise, to any other person or entity.

6. Insurance

SPAN shall obtain public liability insurance of the types and in the amounts set forth below from an insurance carrier or underwriter licensed to do business in the State of Texas and acceptable to the CITY. SPAN shall furnish CITY with certificates of insurance or copies of the policies, evidencing the required insurance on or before the beginning date of this Agreement. SPAN agrees to submit new certificates or policies to CITY on before the expiration date of the previous certificates or policies. The insurance shall be the following types in amounts not less than indicated:

- a. Comprehensive General (Public) Liability Insurance or its equivalent including minimum coverage limits of \$1,000,000 per occurrence combined single limit for bodily injury and property damage.
- b. Automobile Liability Insurance including minimum coverage limits of \$1,000,000 per combined single limit for bodily injury and property damage.
- c. On all insurance required, SPAN shall require insurance providers to:

Name the CITY, and its officials, officers and employees, as additional insureds and provide thirty (30) days written notice to CITY of any material change to or cancellation of the insurance.

7. Assignment and Delegation

Neither party shall assign or delegate the rights or obligations under this Agreement without the prior written consent of the other party.

8. Severability

In the event any provision of this Agreement shall be determined by any court of competent jurisdiction to be invalid or unenforceable, the Agreement shall, to the extent reasonably possible, remain in full force and effect as to the balance of its provisions and shall be construed as if such invalid provision were not a part hereof.

9. Mediation

In the event of any dispute regarding this Agreement or the terms contained herein, the parties hereto agree that they shall submit such dispute to non-binding mediation.

10. Term of Agreement

The term of this Agreement shall be from October 1, 2021 through September 30, 2022, subject to renewal by the parties. Either party may modify this Agreement by submitting, in writing, the proposed amendment to be considered and executed by both parties. This Agreement may be terminated with or without cause by either party by giving thirty (30) days written notice to the other party of their intent to terminate the Agreement. In the event the CITY terminates without cause, SPAN shall be entitled to receive just and equitable compensation for any satisfactory work completed in accordance with this Agreement prior to the termination.

11. Applicable Law Venue

This Agreement shall be governed by, construed, and enforced in accordance with the laws of the State of Texas, and venue for any claim or cause of action shall lie exclusively in Denton County, Texas or the Federal courts having jurisdiction over claims arising in Denton County, Texas.

12. Attorney's Fees and Costs

In the event it becomes necessary to take legal action to enforce the terms of this Agreement, the prevailing party in such action shall be entitled to recover attorney's fees and costs of court from the non-prevailing party.

IN WITNESS WHEREOF the CITY of Corinth and Span, Inc. have executed this Agreement on this the

_____ day of _____, 2021.

SPAN, INC:

Michelle McMahon, Executive Director

City of Corinth

Bob Hart, City Manager

ATTEST:

Lana Wylie, City Secretary

**STATE OF TEXAS
COUNTY OF DENTON
CITY OF CORINTH**

On this, the 28th day of January 2021, the Corinth Crime Control & Prevention District of the City of Corinth, Texas met in a Special Session at the Corinth City Hall at 5:35 P.M., located at 3300 Corinth Parkway, Corinth, Texas. The meeting date, time, place and purpose as required by Title 5, Subtitle A, Chapter 551, Subchapter C, Section 551.041, Government Code, with the following members to wit:

Members Present:

Bill Heidemann, Mayor
Sam Burke, Mayor Pro-Tem
Scott Garber, Council Member
Steve Holzwarth, Council Member
Tina Henderson, Council Member
Kelly Pickens, Council Member

Staff Members Present:

Bob Hart, City Manager
Lana Wylie, City Secretary
Patricia Adams, Messer – Fort – McDonald
Lee Ann Bunselmeyer, Finance, Administration, Communications & Marketing Director
Helen-Eve Beadle, Planning and Development Director
Jason Alexander, Economic Development Corporation Director
Jerry Garner, Police Chief
Melissa Dolan, Interim Public Works Director
Gary Parker, Water/Wastewater Operations Manager
Michelle Mixell, Planning & Development Manager
Miguel Inclan, Planner
George Marshall, City Engineer
Shea Rodgers, Technology Services Manager
James Trussell, Multi-Media Production Intern
Lindsey O'Brien, Police Officer

CALL TO ORDER:

Mayor Heidemann called the meeting to order at 5:35 p.m.

City Manager Hart explained the items were not included as a consent agenda; however, the City Council could act on the minutes.

1. Consider and act on minutes from the January 9, 2020, special session.
2. Consider and act on minutes from the August 20, 2020, special session.
3. Consider and act on minutes from the September 3, 2020, special session.

MOTION made by Council Member Garber to approve as presented. Seconded by Council Member Holzwarth.

AYES: Burke, Garber, Holzwarth, Henderson, Pickens
NOES: None
ABSENT: None

MOTION CARRIED

BUSINESS AGENDA

1. Consider and act on the Investment Policy for funds for the Crime Control & Prevention District; and providing an effective date.

Lee Ann Bunselmeyer, Finance, Administration, Communications & Marketing Director, outlined the Investment Policy for funds for the Crime Control District. The City is required to have the governing bodies review the policy per the Public Funds Investment Act. It is reviewed to make any changes found within the state statute, and policies that might help with investment strategies. The recommendation is to move the level of our investment from 50% to 75%. The purpose of request is due to the current economic environment with the low interest rates. The struggle is getting a good interest rate in some of the long-term investments. The bank contract rate is much higher than options on an open market. She confirmed the desire to continue investing with Independent Bank, and to put more funds in the investment pools.

MOTION made by Council Member Garber to approve as presented. Seconded by Council Member Pickens.

AYES: Burke, Garber, Holzwarth, Henderson, Pickens
NOES: None
ABSENT: None

MOTION CARRIED

ADJOURN:

Mayor Heidemann adjourned the meeting at 5:39 P.M.

AYES: All

Meeting adjourned.

Approved by Council on the _____ day of _____ 2021.

Lana Wylie, City Secretary
City of Corinth, Texas

CITY OF CORINTH

Staff Report



Meeting Date:	7/15/2021	Title:	Public Hearing CCD Budget
Strategic Goals:	<input type="checkbox"/> Citizen Engagement <input checked="" type="checkbox"/> Proactive Government <input type="checkbox"/> Organizational Development		
Governance Focus:	<i>Sub-Ends:</i>		
	<input type="checkbox"/> Growing Community <input checked="" type="checkbox"/> Delivers Outstanding Service <input type="checkbox"/> High-Quality Restaurants	<input type="checkbox"/> Conveniently located <input type="checkbox"/> High-Quality Retail <input type="checkbox"/> High-Quality Entertainment	
	<i>Focus:</i> <input checked="" type="checkbox"/> Owner <input type="checkbox"/> Customer <input checked="" type="checkbox"/> Stakeholder		
<i>Decision:</i> <input type="checkbox"/> Governance Policy <input checked="" type="checkbox"/> Ministerial Function			

Item/Caption

Hold a public hearing on the fiscal year 2021-2022 Proposed Budget for the City of Corinth Crime Control and Prevention District.

Item Summary/Background/Prior Action

The Board of the City of Corinth Crime Control & Prevention District will hold a public hearing on the Fiscal Year 2021-2022 Crime Control & Prevention District Budget, on Thursday, July 15, 2021, at 6:40 p.m. in the City Council Chambers at City Hall, located at 3300 Corinth Parkway, Corinth Texas 76208. The meeting will be held for the purpose of receiving community input on the Crime Control & Prevention District Budget.

Applicable Owner/Stakeholder Policy

The Texas Local Government Code Sections 363.204 and 363.205 prescribe procedures for adoption of the crime control and prevention budget. Texas Local Government Code 363.204(f) also permits the Board to develop and adopt procedures for adopting a budget different from the procedures as outlined in the statute. In 2009, the Board adopted an order requiring a public hearing on the proposed budget for the District, providing that any resident of the district be entitled to be present and participate in the hearing, and require that a notice of the public hearing be published in a newspaper with general circulation in the district not later than the fifth (5th) day before the date of the hearing. The notice above was published on Saturday, July 10, 2021 in the Denton Record Chronicle.

Staff Recommendation/Motion

N/A

CITY OF CORINTH

Staff Report



Meeting Date:	7/15/2021	Title:	CCD Budget
Strategic Goals:	<input type="checkbox"/> Citizen Engagement <input checked="" type="checkbox"/> Proactive Government <input type="checkbox"/> Organizational Development		
Governance Focus:	<i>Sub-Ends:</i> <input type="checkbox"/> Growing Community <input type="checkbox"/> Conveniently located <input checked="" type="checkbox"/> Delivers Outstanding Service <input type="checkbox"/> High-Quality Retail <input type="checkbox"/> High-Quality Restaurants <input type="checkbox"/> High-Quality Entertainment		
	<i>Focus:</i> <input checked="" type="checkbox"/> Owner <input type="checkbox"/> Customer <input type="checkbox"/> Stakeholder		
	<i>Decision:</i> <input checked="" type="checkbox"/> Governance Policy <input type="checkbox"/> Ministerial Function		

Item/Caption

Consider and act on the fiscal year 2021-2022 budget for the City of Corinth Crime Control and Prevention District.

Item Summary/Background/Prior Action

Budgeted expenditures total \$489,913 which includes \$202,954 for the retention of two Police Officers, \$218,009 for Enterprise vehicle lease payments for patrol vehicles, \$11,250 for aftermarket patrol vehicle equipment through the Enterprise Lease program, ongoing equipment replacement of \$9,700 for tasers, \$14,000 for radars, \$6,500 for body cams, and \$7,000 for thermal cameras.

New Program funding includes one-time expenditures of \$20,500.

\$13,000 for a UAV Drone. The use of a drone in law enforcement has become commonplace. The Drone can be used for a myriad of tasks not only for the police department and other city departments. The Drone will be used as a force multiplier on major incident, for accident reconstruction, Search and Rescue, Crime Scene analysis, surveillance, crowd monitoring, suspect apprehension as well as other tasks throughout the city in other departments such as Fire and Public Works.

\$7,500 for Accident Reconstruction Equipment. The department has teamed up with the Little Elm Police Department to create a Traffic Accident Reconstruction Team. The training for each officer is very time consuming and costly. By creating this team, the Department has consolidated resources and has the ability to reconstruct accidents and crime scenes with Little Elm.

Financial Impact

The budget projects the sales tax revenue will generate \$409,190. The ending fund balance is estimated at \$587,846 for the 2021-2022 fiscal year.

Applicable Owner/Stakeholder Policy

The Texas Local Government Code Sections 363.204 and 363.205 prescribe procedures for adoption of the crime control budget. Texas Local Government Code 363.204(f) permits the Board to develop and adopt procedures for adopting a budget different from the procedures as outlined in the statute. In 2009, the Board adopted an order establishing the following procedures.

- a) Board shall hold a public hearing on the proposed budget for the District. Any resident of the district is entitled to be present and participate in the hearing.
- b) Board shall publish a notice of the public hearing in a newspaper with general circulation in the district not later than the fifth (5th) day before the date of the hearing.
- c) The proposed budget shall be made available in the Office of the City Secretary for public inspection at least five (5) days prior to the public hearing
- d) After the public hearing, the Board may make any changes in the proposed budget that in its judgment, is in the interest of the taxpayers of the District. The Board may adopt the budget immediately following the public hearing or at any time within ten (10) days following the public hearing.
- e) The Secretary of the Board shall submit the adopted budget to the City Council of the City of Corinth not later than the 10th day after the date the budget is adopted.

Staff Recommendation/Motion

Staff recommends approval of the FY 2021-2022 Crime Control and Prevention Annual Budget.



SALES TAX FUND - CRIME CONTROL & PREVENTION

The Corinth Police Department is committed to excellence in service through innovative and progressive policing methods. We value the trust of our citizens and are committed to carrying out our duties with honor, integrity and pride. Through partnerships and collaborative efforts, we will strive to enhance the safety and security in our community.

ACCOMPLISHMENTS FY2020-2021

- Supported the Police Enterprise Fleet Program.
- Replaced the Police Department WatchGuard Server and aging bodycams.

GOALS & OBJECTIVES FY2021-2022

- Support the Police Enterprise Fleet Program.
- Replace the Police Department aging Tasers.
- Purchase a drone for Police Department.
- Purchase Accident Reconstruction Equipment and Software.



CRIME CONTROL & PREVENTION (2203)

DESCRIPTION

The Corinth Crime Control & Prevention tax is a special tax levied for crime control and prevention that allows the City to provide the citizens with professional and efficient police services. The Crime Control & Prevention District Fund was established on January 1, 2005. A special election was held on September 11, 2004 for the purpose of adopting a .25% local sales and use tax for crime control & prevention. An election on May 2019 reauthorized the dedicated sales tax for another ten years.

RESOURCE SUMMARY	2019-20 ACTUAL	2020-21 BUDGET	2020-21 ESTIMATE	2021-22 BUDGET
Sales Tax	\$ 406,616	\$ 371,991	\$ 371,991	\$ 409,190
Investment Income	-	-	-	-
Interest Income	5,608	4,500	2,202	2,000
Miscellaneous	-	-	-	-
Gain on Sales	-	-	-	-
Transfers	-	-	-	-
TOTAL REVENUES	412,224	\$ 376,491	\$ 374,193	\$ 411,190
Use of Fund Balance	-	-	-	78,723
TOTAL RESOURCES	\$ 412,224	\$ 376,491	\$ 374,193	\$ 489,913

EXPENDITURE SUMMARY	2019-20 ACTUAL	2020-21 BUDGET	2020-21 ESTIMATE	2021-22 BUDGET
Wages & Benefits	\$ 145,021	\$ 194,529	\$ 194,529	\$ 202,954
Professional Fees	-	-	-	-
Maintenance & Operations	6,470	13,760	13,760	-
Supplies	-	72,930	72,930	16,200
Capital Outlay	37,387	18,980	18,980	52,750
Capital Lease	163,261	-	-	218,009
Transfers	-	-	-	-
TOTAL EXPENDITURES	\$ 352,139	\$ 300,199	\$ 300,199	\$ 489,913

PROJECTED FUND BALANCE REVIEW	2019-20 ACTUAL	2020-21 BUDGET	2020-21 ESTIMATE	2021-22 BUDGET
Beginning Fund Balance	\$ 532,490	\$ 592,575	\$ 592,575	\$ 666,569
Net Income	60,085	76,292	73,994	(78,723)
ENDING FUND BALANCE	592,575	\$ 668,867	\$ 666,569	\$ 587,846

PERSONNEL Full-Time Equivalents	2019-20 ACTUAL	2020-21 BUDGET	2020-21 ESTIMATE	2021-22 BUDGET
Sworn/Civil Service	2.00	2.00	2.00	2.00
TOTAL	2.00	2.00	2.00	2.00

NEW PROGRAM FUNDING

Drone - \$13,000; Taser Replacement - \$9,700; Accident Reconstruction - \$7,500; Body Camera Replacement - \$6,500



CITY OF CORINTH

Staff Report

Meeting Date:	7/15/2021	Title:	Police Annual Review
Strategic Goals:	<input type="checkbox"/> Citizen Engagement <input checked="" type="checkbox"/> Proactive Government <input type="checkbox"/> Organizational Development		
Governance Focus:	<i>Sub-Ends:</i>		
	<input type="checkbox"/> Growing Community	<input type="checkbox"/> Conveniently located	
	<input checked="" type="checkbox"/> Delivers Outstanding Service	<input type="checkbox"/> High-Quality Retail	
<input type="checkbox"/> High-Quality Restaurants		<input type="checkbox"/> High-Quality Entertainment	
<i>Focus:</i>	<input checked="" type="checkbox"/> Owner	<input type="checkbox"/> Customer	<input type="checkbox"/> Stakeholder
<i>Decision:</i>	<input checked="" type="checkbox"/> Governance Policy	<input type="checkbox"/> Ministerial Function	

Item/Caption

Receive a report and hold discussions on the Police Department operational activity.

Item Summary/Background/Prior Action

The Corinth Police Department is committed to excellence in service through innovative and progressive policing methods. The department has a total of 41 employees, of which two are funded through the Crime Control, and Prevention District. The department values the trust of its citizens and are committed to carrying out their duties with honor, integrity, and pride. Through partnerships and collaborative efforts, the department strives to enhance the safety and security of our community.

The police department provides services to the Town of Shady Shores through an Interlocal Agreement. The agreement is a five-year contract that expires on September 30, 2022. The city will work with the City of Lake Dallas to provide police services in Lake Dallas. Chief Garner will also serve as the interim police chief for Lake Dallas.

The police department also provides Animal Services that promote the health, safety, and welfare of animals within Corinth. In July 2018, the City partnered with the City of Lake Dallas for Animal Shelter Services.

The Police Chief will provide a presentation on the accomplishments, workload, and challenges for each department.

Staff Recommendation/Motion

N/A



POLICE SERVICES

JERRY GARNER, POLICE CHIEF

Our mission is to preserve the quality of life in our community by providing protection and exceptional customer service with honor, integrity, and pride.



Our Vision:

A police department and community working in partnership for a safer, better Corinth and Shady Shores.

Our Mission:

Preserve the quality of life in our community by providing protection and exceptional customer service with honor, integrity, and pride.

OUR GOALS:

- WORK SAFELY
- FIND A WAY TO HELP (EXCEPTIONAL CUSTOMER SERVICE)
- DO THE RIGHT THING, ON-DUTY AND OFF

THE CORINTH POLICE
DEPARTMENT HAS BEEN
RECOGNIZED THREE
TIMES BY THE TEXAS
POLICE CHIEFS
ASSOCIATION AS A BEST
PRACTICES RECOGNIZED
AGENCY



Staffing

Command Level: 4

Sergeants: 6

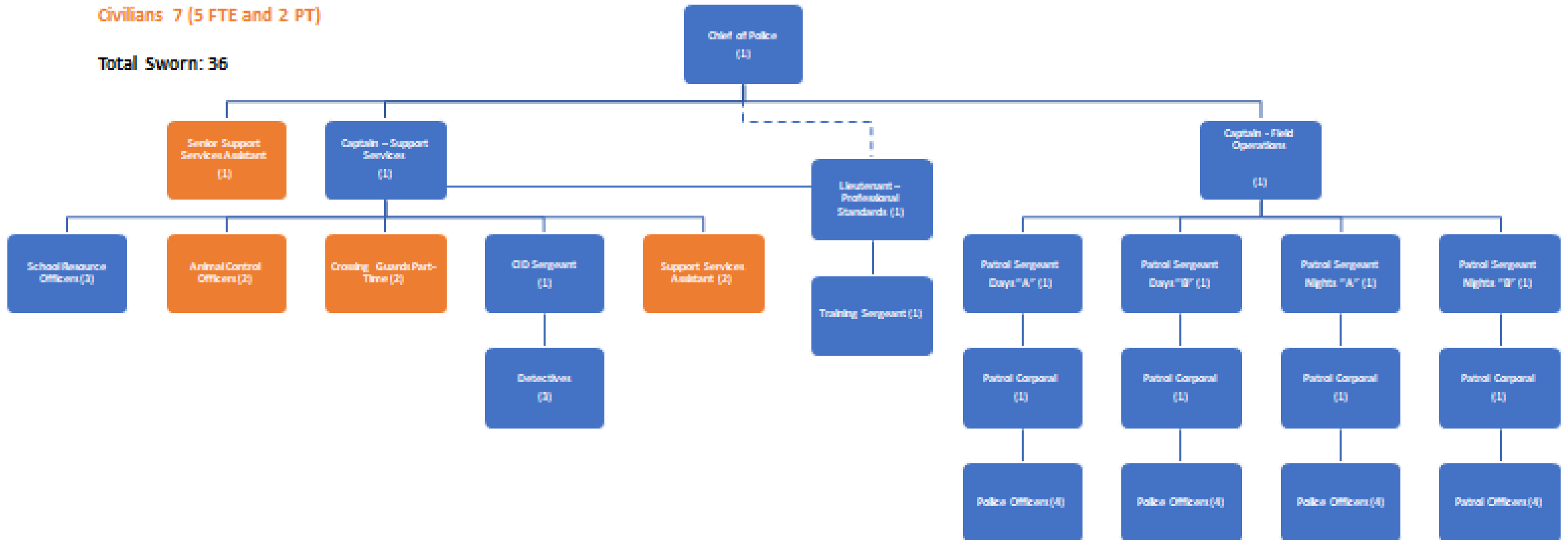
Corporals: 4

Police Officers: 22

Civilians 7 (5 FTE and 2 PT)

Total Sworn: 36

Corinth PD Organizational Chart 2021





- Recruited, trained, and hired exceptional candidates to fill two additional officer positions authorized by City Council.
- Began work on an Interlocal Agreement for continuing to provide police services to the Town of Shady Shores.
- Reached an authorized staffing level of 36 sworn by filling vacancies.

School Resource Officers

3 F/T Officers Assigned

Directed Patrols/Park and Walk

28,240

2018-2019:15,100

+87%

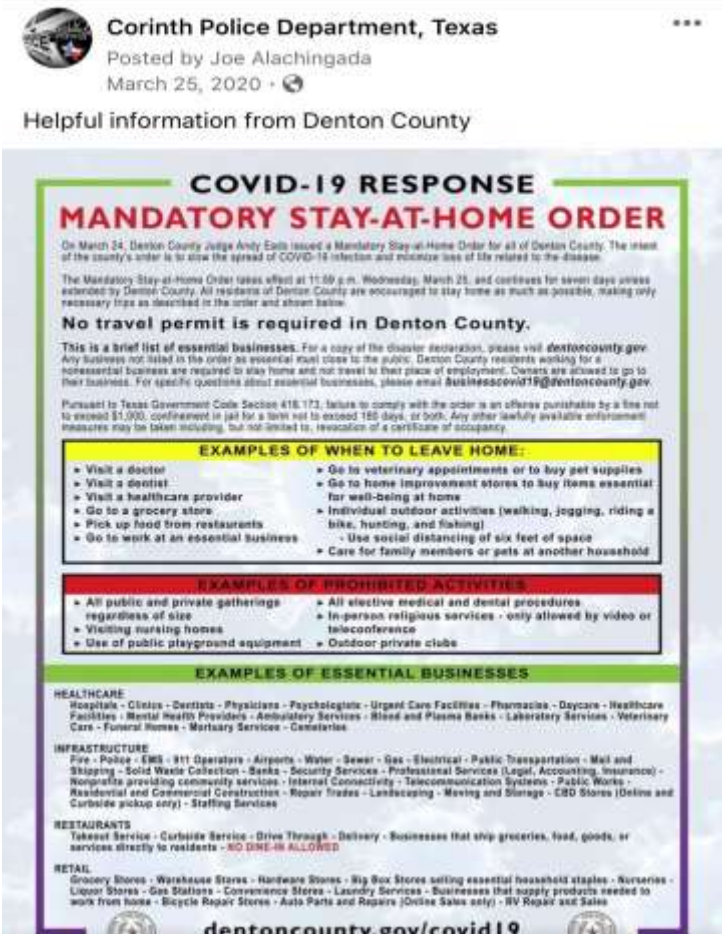


Cancelled due to Covid
Citizens Public Safety Academy

National Night Out

CSI Youth Camp

During the Covid pandemic, the Police Department's highly-popular social media accounts served as the community engagement face and voice of the Department.



PATROL DIVISION OPERATIONS 2019-2020



CALLS FOR SERVICE
Answered 11,502 calls
15% Increase



TRAFFIC CITATIONS
Issued 4,144 Citations
35% Decrease



WARNINGS
Issued 3,676 Warnings
27% Decrease



**PARK & WALKS/
DIRECTED PATROLS**
Conducted 28,240 Patrols
87% Increase



VEHICLE ACCIDENTS
Investigated 517 Accidents
3% Decrease



TRAFFIC COMPLAINTS
Investigated 598 Complaints
24% Decrease



ARRESTS
Conducted 429 Arrests
.005% Decrease



AVERAGE RESPONSE TIMES, PRIORITY ONE CALLS



From time call received in County Dispatch until the first officer arrives on-scene.



**INVESTIGATED 712
CRIMINAL CASES**

31% Increase



**RECOVERED
\$142,096 IN STOLEN
PROPERTY**

101% Increase



**FILED 560
CRIMINAL CASES**

20% Increase

ANIMAL SHELTER

Sheltered 112 Animals
40% Increase

CITATIONS

Issued 24 citations
8% Decrease

CALLS FOR SERVICE

Answered 951 Calls
3% Increase

ANIMALS RETURNED TO OWNER

Returned 86 Animals
35% Increase



PET REGISTRATIONS

Conducted 115 Pet
Registrations
38% Decrease

ANIMAL BITES

Responded to 31 Bite Calls
41% Increase

QUARANTINES

Quarantined 29 Animals
38% Increase

WILDLIFE RELEASED

Released 111 Animals
85% Increase

TOTAL HOURS

1,354 Hours Volunteered
45% Decrease



VACATION WATCHES

Conducted 327 vacation
watches
82% Decrease

The Corinth P.D. Volunteers In Police Service (VIPS) program started in 2012. Volunteers receive formal training on tasks involving Vacation House Watch, Abandoned Vehicle Notification, Special Patrols and clerical duties. We currently have approximately 10 active volunteers.

PART ONE (INDEX CRIME) STATS

Section H, Item 4.

Classification of Offense	2016	2017	2018	2019	2020
Murder	0	1	0	0	0
Rape	3	1	2	3	6
Robbery	1	4	2	2	4
Assault	125	123	96	78	103
Burglary	32	33	32	33	37
Theft	154	218	160	125	162
Motor Vehicle Theft	11	21	13	10	22
<i>Grand Total</i>	<i>326</i>	<i>401</i>	<i>305</i>	<i>251</i>	<i>334</i>

Based on Grand Totals, 33% increase from 2019 to 2020

GOALS/ OBJECTIVES FOR 2021-2022

Section H, Item 4.



- Conduct a study for merging the police services with Lake Dallas
- Complete an Interlocal Agreement for continuing police service for the Town of Shady Shores.
- Implement a Peer Support program to aid police personnel suffering from PTS or other mental/emotional trauma.
- Re-vitalize the Department's community outreach efforts sidelined by the Covid pandemic.
- Obtain Regional Training Provider status.

**CITY OF CORINTH
Staff Report**

Meeting Date:	7/15/2021	Title: Minutes Approval of Meeting Minutes (FCP&EMS)
Strategic Goals:	<input type="checkbox"/> Citizen Engagement <input checked="" type="checkbox"/> Proactive Government <input type="checkbox"/> Organizational Development	
Governance Focus:	<i>Sub-Ends:</i> <input type="checkbox"/> Growing Community <input type="checkbox"/> Conveniently located <input checked="" type="checkbox"/> Delivers Outstanding Service <input type="checkbox"/> High-Quality Retail <input type="checkbox"/> High-Quality Restaurants <input type="checkbox"/> High-Quality Entertainment	
	<i>Focus:</i> <input checked="" type="checkbox"/> Owner <input type="checkbox"/> Customer <input type="checkbox"/> Stakeholder	
	<i>Decision:</i> <input checked="" type="checkbox"/> Governance Policy <input type="checkbox"/> Ministerial Function	
Owner Support:	<input type="checkbox"/> Planning & Zoning Commission <input type="checkbox"/> Economic Development Corporation <input type="checkbox"/> Parks & Recreation Board <input type="checkbox"/> TIRZ Board #2 <input type="checkbox"/> Finance Audit Committee <input type="checkbox"/> TIRZ Board #3 <input type="checkbox"/> Keep Corinth Beautiful <input type="checkbox"/> Ethics Commission <u>N/A</u>	

Item/Caption

Consider and act on minutes from January 28, 2021.

Item Summary/Background/Prior Action

Attached are the minutes, in draft form, and are not considered official until formally approved by the City Council.

Staff Recommendation/Motion

Staff recommends approval of the minutes.

**STATE OF TEXAS
COUNTY OF DENTON
CITY OF CORINTH**

On this, the 28th day of January 2021, the Corinth Fire Control, Prevention & Emergency Medical Services District Board of Directors of the City of Corinth, Texas met in Special Session at the Corinth City Hall at 5:40 P.M., located at 3300 Corinth Parkway, Corinth, Texas. The meeting date, time, place and purpose as required by Title 5, Subtitle A, Chapter 551, Subchapter C, Section 551.041, Government Code, with the following members to wit:

Members Present:

- Bill Heidemann, Mayor
- Sam Burke, Mayor Pro-Tem
- Scott Garber, Council Member
- Steve Holzwarth, Council Member
- Tina Henderson, Council Member
- Kelly Pickens, Council Member

Staff Members Present:

- Bob Hart, City Manager
- Lana Wylie, City Secretary
- Patricia Adams, Messer – Fort – McDonald
- Lee Ann Bunselmeyer, Finance, Administration, Communications & Marketing Director
- Helen-Eve Beadle, Planning and Development Director
- Jason Alexander, Economic Development Corporation Director
- Jerry Garner, Police Chief
- Melissa Dolan, Interim Public Works Director
- Gary Parker, Water/Wastewater Operations Manager
- Michelle Mixell, Planning & Development Manager
- Miguel Inclan, Planner
- George Marshall, City Engineer
- Shea Rodgers, Technology Services Manager
- James Trussell, Multi-Media Production Intern
- Lindsey O’Brien, Police Officer

CALL TO ORDER: Mayor Heidemann called the meeting to order at 5:40 P.M.

City Manager Hart explained the items were not included as a consent agenda; however, the City Council could act on the minutes.

1. Consider and act on minutes from the February 13, 2020, special session.
2. Consider and act on minutes from the April 2, 2020, special session.
3. Consider and act on minutes from the August 6, 2020, special session.
4. Consider and act on minutes from the November 12, 2020, special session.
5. Consider and act on minutes from the November 17, 2020, special session.

MOTION made by Council Member Pickens to approve as presented. Seconded by Council Member Garber.

AYES: Burke, Garber, Holzwarth, Henderson, Pickens
NOES: None
ABSENT: None

MOTION CARRIED

BUSINESS AGENDA:

1. Consider and act on the Investment Policy for funds for the Fire Control, Prevention, and Emergency Services District; and providing an effective date.

Lee Ann Bunselmeyer, Finance, Administration, Communications & Marketing Director, outlined the Investment Policy for funds for the Fire Control, Prevention, & EMS District. The voters of Corinth passed a resolution in November approving the creation of the fire Control, Prevention, and Emergency Services District. The state statutes require the district have their own individual investment policy. This item creates the investment policy for the district, it has the same terms, and is consistent with what was approved for the Crime Control District. It does require a change the investment policy from 50% to 75%.

MOTION made by Council Member Henderson to approve as presented. Seconded by Council Member Garber.

AYES: Burke, Garber, Holzwarth, Henderson, Pickens
NOES: None
ABSENT: None

MOTION CARRIED

ADJOURN:

Mayor Heidemann adjourned the meeting at 5:43 p.m.

AYES: All

Meeting adjourned.

Approved by Council on the _____ day of _____ 2021.

Lana Wylie, City Secretary
City of Corinth, Texas



CITY OF CORINTH

Staff Report

Meeting Date:	7/15/2021	Title:	Public Hearing Budget Procedures	
Strategic Goals:	<input type="checkbox"/> Citizen Engagement <input checked="" type="checkbox"/> Proactive Government <input type="checkbox"/> Organizational Development			
Governance Focus:	<i>Sub-Ends:</i>			
	<input type="checkbox"/> Growing Community		<input type="checkbox"/> Conveniently located	
	<input checked="" type="checkbox"/> Delivers Outstanding Service		<input type="checkbox"/> High-Quality Retail	
	<input type="checkbox"/> High-Quality Restaurants		<input type="checkbox"/> High-Quality Entertainment	
	<i>Focus:</i>	<input checked="" type="checkbox"/> Owner	<input type="checkbox"/> Customer	<input type="checkbox"/> Stakeholder
	<i>Decision:</i>	<input checked="" type="checkbox"/> Governance Policy	<input type="checkbox"/> Ministerial Function	

Item/Caption

Hold a public hearing on an order of the Board of the Fire Control, Prevention, and EMS District establishing rules for the adoption of the Fire Control, Prevention, and EMS District Budget and establishing the fiscal year.

Item Summary/Background/Prior Action

The City of Corinth, Texas will hold a public hearing on an order of the Board of the Fire Control, Prevention, and EMS District establishing rules for the adoption of the Fire Control, Prevention, and EMS District Budget and establishing the fiscal year, on Thursday, July 15, 2021, at 6:50 p.m. in the City Council Chambers at City Hall, located at 3300 Corinth Parkway, Corinth Texas 76208.

Applicable Owner/Stakeholder Policy

Texas Local Government Code Chapter 344 prescribes procedures for the Fire Control, Prevention, and EMS District. Section 344.204(f) permits the Board to develop and adopt procedures for adopting a budget different from the procedures as outlined in the statute.

Local Government Code Sec. 344.204. Adoption of Budget.

- (a) Not later than the 100th day before the date each fiscal year begins, the board shall hold a public hearing to consider the proposed annual budget.
- (b) The board shall publish notice of the hearing in a newspaper with general circulation in the district not later than the 10th day before the date of the hearing.
- (c) A resident of the district is entitled to participate in the hearing.
- (d) Not later than the 80th day before the date each fiscal year begins, the board shall adopt a budget. The board may make any changes in the proposed budget that the interests of the taxpayers demand.
- (e) Not later than the 10th day after the date the budget is adopted, the board shall submit the budget to the governing body of the municipality that created the district.
- (f) The board by rule may adopt alternative procedures for adopting a budget that differ from the procedures outlined in this subchapter. The board must hold at least one public hearing related to the alternative procedures before their adoption.

Local Government Code Sec. 344.205. Approval of Budget.

- (a) Not later than the 45th day before the date each fiscal year begins, the governing body of the municipality that created the district shall hold a public hearing to consider the budget adopted by the board and submitted to the governing body.
- (b) The governing body must publish notice of the hearing in a newspaper with general circulation in the district not later than the 10th day before the date of the hearing.
- (c) A resident of the district is entitled to participate in the hearing.
- (d) Not later than the 30th day before the date the fiscal year begins, the governing body shall approve or reject the budget submitted by the board. The governing body may not amend the budget.
- (e) If the governing body rejects the budget submitted by the board, the governing body and the board shall meet and together amend and approve the budget before the beginning of the fiscal year.
- (f) The budget may be amended after the beginning of the fiscal year on approval by the board and the governing body.

Staff Recommendation/Motion

N/A

Texas Local Government Code Chapter 344 prescribes procedures for the Fire Control, Prevention, and EMS District. Section 344.204(f) permits the Board to develop and adopt procedures for adopting a budget different from the procedures as outlined in the statute.

Local Government Code Sec. 344.204. Adoption of Budget.

- (a) Not later than the 100th day before the date each fiscal year begins, the board shall hold a public hearing to consider the proposed annual budget.
- (b) The board shall publish notice of the hearing in a newspaper with general circulation in the district not later than the 10th day before the date of the hearing.
- (c) A resident of the district is entitled to participate in the hearing.
- (d) Not later than the 80th day before the date each fiscal year begins, the board shall adopt a budget. The board may make any changes in the proposed budget that the interests of the taxpayers demand.
- (e) Not later than the 10th day after the date the budget is adopted, the board shall submit the budget to the governing body of the municipality that created the district.
- (f) The board by rule may adopt alternative procedures for adopting a budget that differ from the procedures outlined in this subchapter. The board must hold at least one public hearing related to the alternative procedures before their adoption.

Local Government Code Sec. 344.205. Approval of Budget.

- (a) Not later than the 45th day before the date each fiscal year begins, the governing body of the municipality that created the district shall hold a public hearing to consider the budget adopted by the board and submitted to the governing body.
- (b) The governing body must publish notice of the hearing in a newspaper with general circulation in the district not later than the 10th day before the date of the hearing.
- (c) A resident of the district is entitled to participate in the hearing.
- (d) Not later than the 30th day before the date the fiscal year begins, the governing body shall approve or reject the budget submitted by the board. The governing body may not amend the budget.
- (e) If the governing body rejects the budget submitted by the board, the governing body and the board shall meet and together amend and approve the budget before the beginning of the fiscal year.
- (f) The budget may be amended after the beginning of the fiscal year on approval by the board and the governing body.

Staff Recommendation/Motion

Staff recommends approval of the order establishing rules for the adoption of the Fire Control, Prevention, and EMS District Budget and establishing the fiscal year.

ORDER NO. 21-01

AN ORDER OF THE BOARD OF THE CORINTH FIRE CONTROL, PREVENTION AND EMS DISTRICT ESTABLISHING RULES FOR THE ADOPTION OF THE FIRE CONTROL, PREVENTION AND EMS DISTRICT BUDGET AND ESTABLISHING THE FISCAL YEAR.

WHEREAS, Texas Local Government Code §344.204(f) permits the Board of the Fire Control, Prevention and EMS District to develop and adopt procedures for adopting a budget different from the procedures outlined in the statute;

WHEREAS, the board finds that it is in the best interest of the taxpayers of the District to do so;

NOW THEREFORE, BE IT ORDERED BY THE BOARD OF DIRECTORS OF THE CORINTH FIRE CONTROL, PREVENTION AND EMS DISTRICT.

SECTION 1.

The following rules are hereby established for the adoption of the budget for the Corinth Fire Control, Prevention and EMS District, (the "District"):

- a. **Public Hearing.** The Board of Directors of the Fire Control, Prevention and EMS District (the "Board"), shall hold a public hearing on the proposed annual budget for the District for the fiscal year beginning October 1 of each year. Any resident of the district is entitled to be present and participate at the hearing.
- b. **Notice of Public Hearing.** The Board shall publish notice of the public hearing in a newspaper with general circulation in the district not later than the fifth (5th) day before the date of the hearing.
- c. **Inspection of Proposed Budget.** The proposed budget shall be made available in the Office of the City Secretary for public inspection at least five (5) days prior to the public hearing.
- d. **Adoption of Budget.** After the public hearing, the Board may make any changes in the proposed budget that in its judgment, is in the interest of the taxpayers of the District. The Board may adopt the budget immediately following the public hearing or at any time within ten (10) days following the public hearing.
- e. **Submission of Budget to City Council.** The Secretary of the Board shall submit the adopted budget to the City Council of the City of

Corinth not later than the 10th day after the date the budget is adopted.

SECTION 2.

This order shall take effect immediately upon its passage and approval.

PASSED AND APPROVED THIS THE _____ DAY OF _____, 2021.

President, Board of Directors of the Corinth
Fire Control, Prevention and EMS District

ATTEST:

Secretary to the Board

CITY OF CORINTH
Staff Report



Meeting Date:	7/15/2021	Title:	Public Hearing Fire District Budget
Strategic Goals:	<input type="checkbox"/> Citizen Engagement <input checked="" type="checkbox"/> Proactive Government <input type="checkbox"/> Organizational Development		
Governance Focus:	<i>Sub-Ends:</i> <input type="checkbox"/> Growing Community <input type="checkbox"/> Conveniently located <input checked="" type="checkbox"/> Delivers Outstanding Service <input type="checkbox"/> High-Quality Retail <input type="checkbox"/> High-Quality Restaurants <input type="checkbox"/> High-Quality Entertainment		
	<i>Focus:</i> <input checked="" type="checkbox"/> Owner <input type="checkbox"/> Customer <input checked="" type="checkbox"/> Stakeholder		
	<i>Decision:</i> <input type="checkbox"/> Governance Policy <input checked="" type="checkbox"/> Ministerial Function		

Item/Caption

Hold a public hearing on the fiscal year 2021-2022 Proposed Budget for the Fire Control, Prevention & EMS District.

Item Summary/Background/Prior Action

The Board for the City of Corinth Fire Control, Prevention & EMS District will hold a public hearing on the Fiscal Year 2021-2022 Fire Control, Prevention & EMS District Budget, on Thursday, July 15, 2021, at 6:50 p.m. in the City Council Chambers at City Hall, located at 3300 Corinth Parkway, Corinth Texas 76208. The meeting will be held for the purpose of receiving community input on the Fire Control, Prevention & EMS District Budget.

Applicable Owner/Stakeholder Policy

The Texas Local Government Code Sections 344.204 prescribe procedures for adoption of the Fire Control, Prevention & EMS District. The board by rule may adopt alternative procedures for adopting a budget that differ from the procedures outlined in this subchapter. The board must hold at least one public hearing related to the alternative procedures before their adoption.

In July 2021, the Board adopted an order requiring a public hearing on the proposed budget for the District, providing that any resident of the district be entitled to be present and participate in the hearing, and require that a notice of the public hearing be published in a newspaper with general circulation in the district not later than the fifth (5th) day before the date of the hearing. The notice above was published on Saturday, July 10, 2021 in the Denton Record Chronicle.

Staff Recommendation/Motion

N/A

CITY OF CORINTH

Staff Report



Meeting Date:	7/15/2021	Title:	Fire District Budget Approval
Strategic Goals:	<input type="checkbox"/> Citizen Engagement <input checked="" type="checkbox"/> Proactive Government <input checked="" type="checkbox"/> Organizational Development		
Governance Focus:	<i>Sub-Ends:</i> <input type="checkbox"/> Growing Community <input type="checkbox"/> Conveniently located <input type="checkbox"/> Delivers Outstanding Service <input type="checkbox"/> High-Quality Retail <input type="checkbox"/> High-Quality Restaurants <input type="checkbox"/> High-Quality Entertainment		
	<i>Focus:</i> <input checked="" type="checkbox"/> Owner <input type="checkbox"/> Customer <input type="checkbox"/> Stakeholder		
	<i>Decision:</i> <input checked="" type="checkbox"/> Governance Policy <input type="checkbox"/> Ministerial Function		

Item/Caption

Consider and act on the fiscal year 2021-2022 budget for the Fire Control, Prevention, and Emergency Management Services District.

Item Summary/Background/Prior Action

In 2020, the Fire Control, Prevention, and Emergency Management Sales Tax District was created under Local Government Code Chapter 344. The district was created to support all costs of fire control, prevention, and emergency services, including costs for personnel, administration, expansion, enhancement, and capital expenditures.

The budget transfers operational expenditures from the general fund of \$442,176 and does not include any new program funding. The budget includes: Fire operations \$251,513, EMS \$171,998, Emergency Management \$9,855, Public Education \$4,934, Fire Prevention Bureau \$2,030 and Citizens Academy \$1,846.

Financial Impact

The budget projects the sales tax revenue will generate \$441,760. The ending fund balance is estimated at \$112,584 for the 2021-2022 fiscal year.

Applicable Owner/Stakeholder Policy

The Texas Local Government Code Sections 344.204 prescribe procedures for adoption of the Fire Control, Prevention & EMS District. The board by rule may adopt alternative procedures for adopting a budget that differ from the procedures outlined in this subchapter. The board must hold at least one public hearing related to the alternative procedures before their adoption.

In July 2021, the Board adopted an order requiring a public hearing on the proposed budget for the District, providing that any resident of the district be entitled to be present and participate in the hearing, and require that a notice of the public hearing be published in a newspaper with general circulation in the district not later than the fifth (5th) day before the date of the hearing. The notice above was published on Saturday, July 10, 2021 in the Denton Record Chronicle.

Staff Recommendation/Motion

Staff recommends approval of the FY 2021-2022 Fire Control, Prevention, and Emergency Management District Annual Budget.



SALES TAX FUND – FIRE CONTROL, PREVENTION, EMS DISTRICT

The Fire Control, Prevention and Emergency Medical Services District was approved by the voters on November 3, 2020, and it will support operations for fire prevention, fire operations, fire inspections, and fire training.

ACCOMPLISHMENTS FY2020-2021

- Passed an election in November 2020 for the creation of the Fire Control, Prevention, and Emergency Medical Services District.
- Developed the strategies approved by the Fire District Board: Fire Prevention, Fire Operations, Fire Inspection, and Fire Training.

GOALS & OBJECTIVES FY2021-2022

- Support the Operations of the Fire Department in the Delivery of exceptional service.
- Provide resources for improving the educational impact of fire and life safety on the community.

FIRE CONTROL, PREVENTION, EMS DISTRICT (133)

DESCRIPTION

The Fire District is funded by a sales tax that allows the City to provide the citizens with professional and efficient fire services. The creation of the Fire District Fund is was approved by the voters in a Special Election on November 3, 2020, for the purpose of adopting a .25% local sales and use tax for Fire services.

RESOURCE SUMMARY	2019-20 ACTUAL	2020-21 BUDGET	2020-21 ESTIMATE	2021-22 BUDGET
Sales Tax	\$ -	\$ 230,244	\$ 230,244	\$ 441,760
Investment Income	-	-	-	-
Interest Income	-	-	-	1,000
Miscellaneous	-	-	-	-
Transfers	-	-	-	-
TOTAL REVENUES	\$ -	\$ 230,244	\$ 230,244	\$ 442,760
Use of Fund Balance	-	-	-	-
TOTAL RESOURCES	\$ -	\$ 230,244	\$ 230,244	\$ 442,760

EXPENDITURE SUMMARY	2019-20 ACTUAL	2020-21 BUDGET	2020-21 ESTIMATE	2021-22 BUDGET
Wages & Benefits	\$ -	\$ -	\$ -	\$ -
Professional Fees	-	-	-	48,749
Maintenance & Operations	-	-	-	92,493
Supplies	-	-	-	253,246
Utilities	-	-	-	-
Communications	-	-	-	-
Vehicle & Fuel	-	-	-	6,655
Training	-	-	-	32,533
Capital Outlay	-	88,244	88,244	8,500
Capital Lease	-	-	-	-
Transfers	-	142,000	30,000	-
TOTAL EXPENDITURES	\$ -	\$ 230,244	\$ 118,244	\$ 442,176

PROJECTED FUND BALANCE REVIEW	2019-20 ACTUAL	2020-21 BUDGET	2020-21 ESTIMATE	2021-22 BUDGET
Beginning Fund Balance	\$ -	\$ -	\$ -	\$ 112,000
Net Income	-	0	112,000	584
ENDING FUND BALANCE	\$ -	\$ 0	\$ 112,000	\$ 112,584

NEW PROGRAM FUNDING

There is no new program funding.

CITY OF CORINTH
Staff Report



Meeting Date:	7/15/2021	Title:	Fire Annual Review
Strategic Goals:	<input type="checkbox"/> Citizen Engagement <input checked="" type="checkbox"/> Proactive Government <input type="checkbox"/> Organizational Development		
Governance Focus:	<i>Sub-Ends:</i>		
	<input type="checkbox"/> Growing Community	<input type="checkbox"/> Conveniently located	
	<input checked="" type="checkbox"/> Delivers Outstanding Service	<input type="checkbox"/> High-Quality Retail	
	<input type="checkbox"/> High-Quality Restaurants	<input type="checkbox"/> High-Quality Entertainment	
	<i>Focus:</i>	<input checked="" type="checkbox"/> Owner	<input type="checkbox"/> Customer <input type="checkbox"/> Stakeholder
	<i>Decision:</i>	<input checked="" type="checkbox"/> Governance Policy	<input type="checkbox"/> Ministerial Function

Item/Caption

Receive a report and hold discussions on the impact, efficiency, and effectiveness of the fire control, prevention, and emergency medical services programs.

Item Summary/Background/Prior Action

The Lake Cities Fire Department is a progressive organization located along the Interstate 35 corridor in Denton County. The Fire District protects the communities of Corinth, Hickory Creek, Lake Dallas, and Shady Shores. The fire district is approximately 30 square miles with a population of about 30,000. Lewisville Lake surrounds the South border, and Denton surrounds the North border. The Fire Department currently operate out of three fire houses and a headquarters building.

The Fire Department provides fire suppression and related investigatory activities, fire prevention and public education services, emergency rescue and extrication services, hazardous materials services, and emergency medical services.

The Fire Prevention Division is responsible for Fire Safety Inspections, Investigations, Construction Plan Reviews, Code Enforcement, Public Education and Information. This division is an integral part of the city government, responsible for developing and adopting Fire and Life Safety Codes.

The Operations Division supervises response to emergency and non-emergency calls for service, pre-planning of commercial occupancies and public education. This division is also responsible for obtaining and maintaining the department fleet, equipment and facilities.

The Fire Department is committed to providing progressive, innovative, and high quality training to meet the current and future demands of the fire service. The function of the Training Division is to plan, coordinate, and deliver training in an effort to ensure that the members of the Fire Department operate as safely, effectively, and efficiently as possible. The goal of the Training Division is to meet the 20 hour per member continuing education requirement from the Texas Commission on Fire Protection and the 240 hour per member requirement of the Insurance Service Office (ISO) for Class 1 Fire Departments annually.

Applicable Owner/Stakeholder Policy

Per Sec.344.152 of the Texas Local Government Code, the district shall conduct an annual evaluation program to study the impact, efficiency, and effectiveness of new or expanded fire control, prevention, and emergency medical services programs.

Staff Recommendation/Motion

N/A

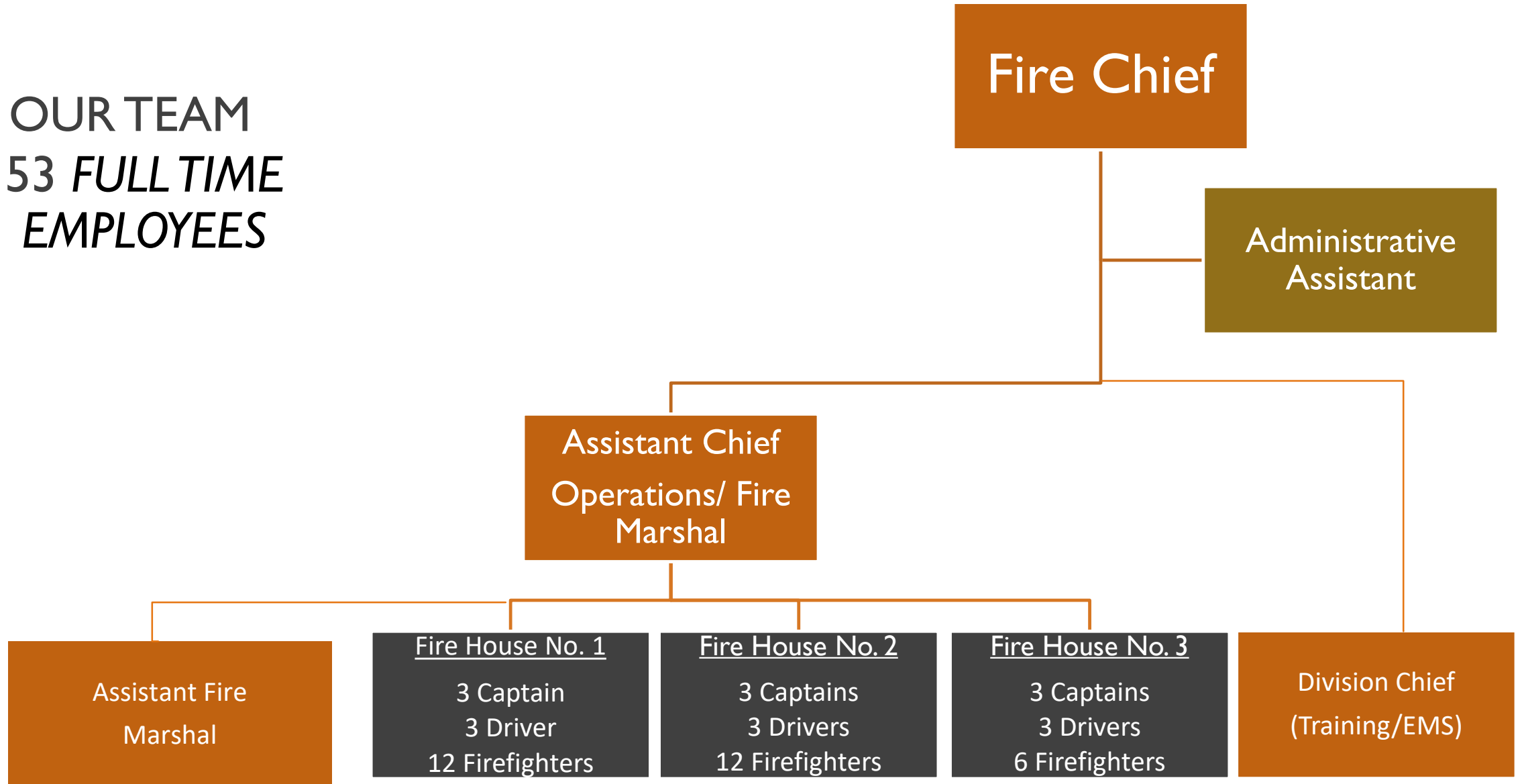


FIRE SERVICES

MICHAEL ROSS, FIRE CHIEF

The Fire Department is a recognized leader in delivering professional and innovative emergency and life-safety services. We'll be there – Ready to respond, compassionate in our care, and safe in our work.

OUR TEAM
53 *FULL TIME*
EMPLOYEES



ACCOMPLISHMENTS

- Sought Recognition -TX Fire Chiefs Association Best Practice.
- Revamped fire prevention program for senior citizens.
- Developed a preliminary Master Plan for fire training field.
- Renewed fire service agreements with the Lake Cities.
- Began final phases of training to implement step-up Shift Commanders.
- Created/updated fire prevention brochure for businesses.
- Leadership training for Captains, Driver Engineers and Field Training Paramedics.
- Adapted responses to Global Pandemic





Section L, Item 6.

Firehouse No.1 275 W. Main, Lake Dallas

Firehouse No.2 2700 W. Shady Shores, Corinth

Firehouse No.3 3750 Cliff Oaks, Corinth

- | | |
|---|---|
| <u>Station Staffing:</u> | <u>Equipment</u> |
| <ul style="list-style-type: none"> ▪ 1 Captain ▪ 1 Driver ▪ 4 Firefighters | <ul style="list-style-type: none"> ▪ Engine 591 ▪ Medic 591 |

- | | |
|---|---|
| <u>Station Staffing:</u> | <u>Equipment:</u> |
| <ul style="list-style-type: none"> ▪ 1 Captain ▪ 1 Driver ▪ 4 Firefighters | <ul style="list-style-type: none"> ▪ Engine 592 ▪ Medic 592 ▪ Reserve Engine/Medic ▪ Brush, Fleet Trucks, ATV |

- | | |
|---|--|
| <u>Station Staffing:</u> | <u>Equipment:</u> |
| <ul style="list-style-type: none"> ▪ 1 Captain ▪ 1 Driver ▪ 2 Firefighters | <ul style="list-style-type: none"> ▪ Quint 593, ▪ Heavy Rescue 593 |

Minimum Firefighters on duty daily

5

Minimum Firefighters on duty daily

5

Minimum Firefighters on duty daily

3

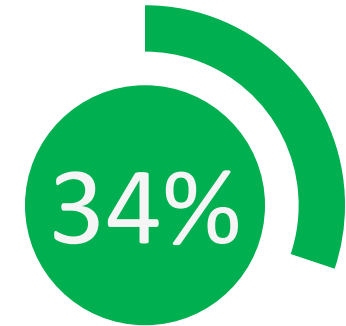
Provided Service to Corinth, Lake Dallas, Hickory Creek and Shady Shores



Calls that required action on scene



Response time within 8 minutes

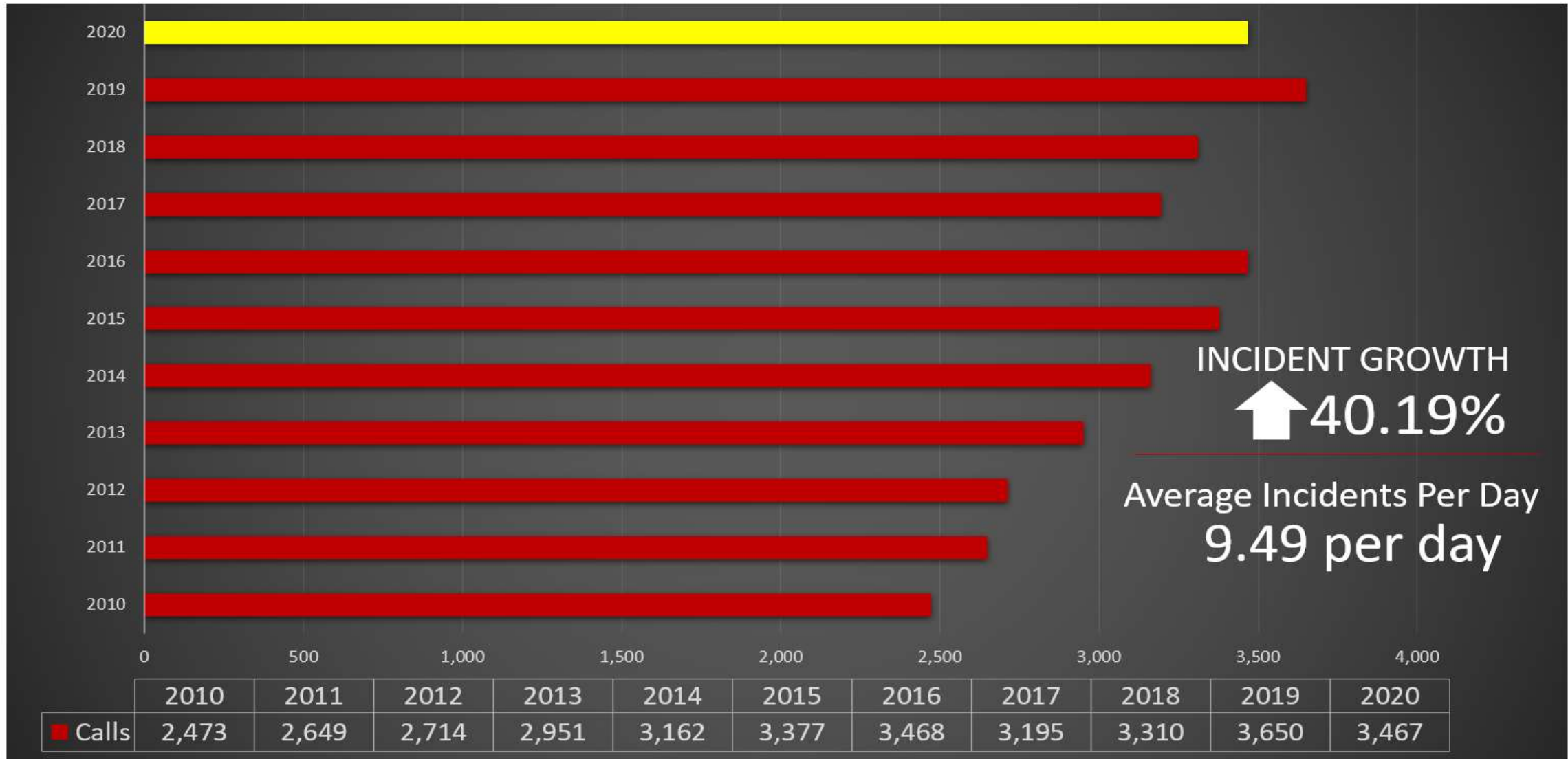


Response time within 4 minutes

3,467 Total Calls for Service District Response Time 6:06

1	EMS	1,895	2	Rescue/MVA	411	3	Service Calls	405
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Call Volume Trends





Fire & Life Safety Education

- Reached approx. 8000 children, pre-K to 3rd grade + 5th grade.
- Included LDISD, Charter School, and Daycare Facilities
- High School Fire Academy
- Citizens Public Safety Academy



Commercial Occupancy Inspections

- Inspected 544 businesses
- 188 Annual Inspections and 89 plan reviews.
- 93 Finals, CO's and Alarm Inspections.



Community Support

- 275 public education, public appearances, ride outs, and station tours.



Lake Cities Area Outdoor Siren System

Section L, Item 6.

- All sirens are on a single point of activation
- Web based- can set off remotely
- Sirens are encrypted
- Polygon based siren activation
- Maintenance program with automatic alerts program with automatic alerts

9 outdoor siren systems location:

300 East Hundley, Lake Dallas

700 North Shady Shores Road,
Lake Dallas

Intersection of S. Hooks and
Lakewood, Hickory Creek

Intersection of Oak Tree and Strait,
Hickory Creek

3101 South Garrison, Corinth

3200 Post Oak, Corinth

1701 Ford Street, Corinth

Shady Shores Road, Corinth

101 Shady Shores Road, Shady
Shores

OBJECTIVES FOR FY2021-22

- Revamp the wellness initiatives to include mental resilience and PTS prevention.
- Fully implement fire prevention program for senior citizens.
- Implement the master plan for the fire training field.
- Amend the Fire Department's Strategic plan.
- Reinstate the Public Safety Citizens Academy.
- Develop formal succession plan.
- Long term assessment of station locations.



CITY OF CORINTH
Staff Report

Meeting Date:	7/15/2021	Title: Minutes Approval of Meeting Minutes
Strategic Goals:	<input type="checkbox"/> Citizen Engagement <input checked="" type="checkbox"/> Proactive Government <input type="checkbox"/> Organizational Development	
Governance Focus:	<i>Sub-Ends:</i> <input type="checkbox"/> Growing Community <input type="checkbox"/> Conveniently located <input checked="" type="checkbox"/> Delivers Outstanding Service <input type="checkbox"/> High-Quality Retail <input type="checkbox"/> High-Quality Restaurants <input type="checkbox"/> High-Quality Entertainment	
	<i>Focus:</i> <input checked="" type="checkbox"/> Owner <input type="checkbox"/> Customer <input type="checkbox"/> Stakeholder	
	<i>Decision:</i> <input checked="" type="checkbox"/> Governance Policy <input type="checkbox"/> Ministerial Function	
Owner Support:	<input type="checkbox"/> Planning & Zoning Commission <input type="checkbox"/> Economic Development Corporation <input type="checkbox"/> Parks & Recreation Board <input type="checkbox"/> TIRZ Board #2 <input type="checkbox"/> Finance Audit Committee <input type="checkbox"/> TIRZ Board #3 <input type="checkbox"/> Keep Corinth Beautiful <input type="checkbox"/> Ethics Commission <u>N/A</u>	

Item/Caption

Consider and act on minutes from July 1, 2021.

Item Summary/Background/Prior Action

Attached are the minutes, in draft form, and are not considered official until formally approved by the City Council.

Staff Recommendation/Motion

Staff recommends approval of the minutes.



CITY COUNCIL WORKSHOP & REGULAR SESSION - MINUTES

Thursday, July 01, 2021 at 5:45 PM

City Hall | 3300 Corinth Parkway

STATE OF TEXAS COUNTY OF DENTON CITY OF CORINTH

On this, the 1st day of July 2021, the City Council of the City of Corinth, Texas, met in Workshop & Regular Session at the Corinth City Hall at 5:45 P.M., located at 3300 Corinth Parkway, Corinth, Texas. The meeting date, time, place, and purpose as required by Title 5, Subtitle A, Chapter 551, Subchapter C, Section 551.041, Government Code, with the following members to wit:

Council Members Present:

Bill Heidemann, Mayor
Sam Burke, Mayor Pro Tem
Scott Garber, Council Member
Tina Henderson, Council Member
Kelly Pickens, Council Member

Council Members Absent:

Steve Holzwarth, Council Member

Staff Members Present:

Bob Hart, City Manager
Lana Wylie, City Secretary
Patricia Adams, Messer – Fort – McDonald
Jerry Garner, Police Chief
Michael Ross, Fire Chief
Lee Ann Bunselmeyer, Finance, Communications & Strategic Services Director
Helen-Eve Beadle, Planning & Development Director
Glenn Barker, Public Works Director
Guadalupe Ruiz, Human Resources Director
George Marshall, City Engineer
Miguel Inclan, Planner
Shea Rodgers, Chief Technology Officer
James Trussell, Multi-Media Video Production Intern
Lance Stacy, City Marshal

CALL TO ORDER

Mayor Heidemann called the Workshop Session to order at 5:45 P.M.

WORKSHOP AGENDA

1. Hold a discussion on the ownership and maintenance of streetlights.

The item was presented and discussed.

- 2. Hold a discussion on the financing of *The Commons at Agora*, including the issuance of certificates of obligation.

The item was presented and discussed.

- 3. Hold a discussion on property values and the calculation of disputed values through the Denton County Central Appraisal District.

The item was presented and discussed.

- 4. Discuss Regular Meeting Items on Regular Session Agenda, including the consideration of closed session items as set forth in the Closed Session agenda items below.

The City Council inquired about the differences of architectural design between Items 6 and 8.

Mayor Heidemann adjourned the Workshop Session and immediately convened into Closed Session at 6:15 P.M.

CLOSED SESSION

The City Council will convene in such executive or closed session to consider any matters regarding any of the above agenda items as well as the following matters pursuant to Chapter 551 of the Texas Government Code. After discussion of any matters in closed session, any final action or vote taken will be public by the City Council. City Council shall have the right at any time to seek legal advice in Closed Session from its Attorney on any agenda item, whether posted for Closed Session or not.

Section 551.071. (1) Private consultation with its attorney to seek advice about pending or contemplated litigation; and/or settlement offer; and/or (2) a matter in which the duty of the attorney to the government body under the Texas Disciplinary Rules of Professional Conduct of the State of Texas clearly conflict with Chapter 551.

Interlocal Agreements; Mutual Aid Agreements; Governmental Services

Robert B. Palmer and Sherry L. Palmer v. Derek William Kirkwood, et al, Civil Action No. 4:20-cv-688, United States District Court, Eastern District of Texas

Martinez v. City of Corinth Police Department, et al, Case 4:21-cv-00146-ALM (U.S. District Court - Eastern District)

Public Information Requests: Attorney General ruling OR2021-15299 issued June 10, 2021

C&JJ Investments, LLC v. City of Corinth, Cause No. 21-5053-431, (431st Judicial District, Denton County, Texas)

Mayor Heidemann recessed the Closed Session at 6:52 P.M.

Mayor Heidemann called the Regular Session meeting to order at 7:00 P.M.

PROCLAMATIONS AND PRESENTATIONS

- 1. Proclamation recognizing 100th birthday of former Mayor Johnny Johnson.

Mayor Heidemann read and presented the Proclamation to Johnny Johnson.

CITIZENS COMMENTS

Please limit your comments to three minutes. Comments about any of the Council agenda items are appreciated by the Council and may be taken into consideration at this time or during that agenda item. Council is prohibited from acting on or discussing items brought before them at this time.

No comments were made.

CONSENT AGENDA

All matters listed under the consent agenda are considered to be routine and will be enacted in one motion. Should the Mayor or a Councilmember desire discussion of any item, that item will be removed from the Consent Agenda and will be considered separately.

- 2. Consider and act on minutes from June 9, 2021, and June 17, 2021.

There was no discussion on the Consent Agenda.
Motion made by Henderson to approve the consent agenda as presented. Seconded by Burke.
Voting Yea: Burke, Garber, Henderson, Pickens

BUSINESS AGENDA

- 3. Consider and act upon an Alternative Compliance Application for Tree Preservation for a Single-Family Residential Lot on ±.587 acres, located at 2502 Oak Bluff Drive in The Bluffs at Pinnell Pointe Subdivision. (Oak Bluff Drive AC21-0011)

Motion made by Burke authorizing approval as presented and authorizing removal of protected trees, granting the recommended exemptions and fee-in-lieu of replacement of mitigation to be paid as outlined. Seconded by Henderson.
Voting Yea: Burke, Garber, Henderson, Pickens

- 4. Consider and act on a Resolution for the appointment of one member to the Board of Managers of the Denco Area 9-1-1 District.

Motion made by Henderson to approve the resolution appointing Mark Klingele to the Board of Managers of the Denco Area 9-1-1 District. Seconded by Burke.
Voting Yea: Burke, Garber, Henderson, Pickens

- 5. Consider approval of a contract with Architexas for the architectural design of the Commons at Agora in the amount of \$346,960.00 and authorize the City Manager to execute any necessary documents.

Motion made by Burke to approve the contract with Architexas for the architectural design of the Commons at Agora in the amount of \$346,960.00 and authorize the City Manager to execute any necessary documents. Seconded by Henderson.
Voting Yea: Burke, Garber, Henderson, Pickens

- 6. Consider approval of Contract Amendment No. 1 with Jones|Carter to add the engineering site design of the Commons at Agora in the amount of \$393,240.00 for a total contract price of \$921,690 for design of the streets and park civil infrastructure and authorize the City Manager to execute any necessary documents.

Motion made by Burke to approve contract Amendment No. 1 with Jones|Carter to add the engineering site design of the Commons at Agora in the amount of \$393,240.00 for a total contract price of \$921,690 for design of the streets and park civil infrastructure and authorize the City Manager to execute any necessary documents.
Voting Yea: Burke, Garber, Henderson, Pickens

- 7. Consider approval of a contract with TBG Partners for the architectural design of the Commons at Agora in the amount of \$394,400 and authorize the City Manager to execute any necessary documents.

Motion made by Henderson to approve the contract with TBG Partners for the architectural design of the Commons at Agora in the amount of \$394,400 and authorize the City Manager to execute any necessary documents. Seconded by Burke.

Voting Yea: Burke, Garber, Henderson, Pickens

- 8. Consider approval of a contract with Byrne Construction Services for the Construction Manager at Risk for the Commons at Agora for a preconstruction services amount of \$7,500, a Construction Services Fee of 3.50%., General Conditions amount of \$428,431.00 for a total proposal of \$610,931.00 and authorize the City Manager to execute any necessary documents.

Motion made by Burke to approve the contract with Byrne Construction Services for the Construction Manager at Risk for the Commons at Agora for a preconstruction services amount of \$7,500, a Construction Services Fee of 3.50%., General Conditions amount of \$428,431.00 for a total proposal of \$610,931.00 and authorize the City Manager to execute any necessary documents., Seconded by Henderson.

Voting Yea: Burke, Garber, Henderson, Pickens

COUNCIL COMMENTS & FUTURE AGENDA ITEMS

The purpose of this section is to allow each Council Member the opportunity to provide general updates and/or comments to fellow Council Members, the public, and/or staff on any issues or future events. Also, in accordance with Section 30.085 of the Code of Ordinances, at this time, any Council Member may direct that an item be added as a business item to any future agenda.

City Manager Hart will provide Amanda Ripley's High Conflict to Council Member's Garber and Pickens next week. There is one employee with covid at this time. The Lake Cities Parade is Saturday. Mayor and City Council will ride in a convertible provided by Bill Utter Ford. City Manager Hart will support Lake Cities Focus.

No other comments were made.

Mayor Heidemann recessed the Regular Meeting at 7:28 P.M. and reconvened into Closed Session at 7:30 P.M.

Section 551.072. To deliberate the purchase, exchange, lease, or value of real property if deliberations in an open meeting would have a detrimental effect on the position of the governmental body in negotiations with a third person.

.7 acres, more or less, of land located at 6801 S. I-35E, Corinth TX, 76210, H. Garrison Survey, Abstract No. 507, Tract 8 (F)

.787 acres, more or less, of land located at 3404 Dobbs Road, Corinth TX, 76210, H. Garrison Survey, Abstract No. 507, Tract 7 (F)

Right-of-way consisting of 1.56 acres located at 6881 I-35E and 3404 Dobbs Road along Dobbs and within the H. Garrison Survey, Abstract No. 507, within the City of Corinth, Denton County, Texas (M/B)

Section 551.087. To deliberate or discuss regarding commercial or financial information that the governmental body has received from a business prospect that the governmental body seeks to have locate, stay, or expand in or near the territory of the governmental body and with which the governmental body

is conducting economic development negotiations; or to deliberate the offer of a financial or other incentive to a business project.

Project Agora

RECONVENE IN OPEN SESSION TO TAKE ACTION, IF NECESSARY, ON CLOSED SESSION ITEMS

Mayor Heidemann adjourned the Closed Session and immediately reconvened into the Regular Session at 7:55 P.M.

Motion made by Burke to authorize the City Attorney’s office to move forward with the process of filing against the Attorney General’s ruling on OR2121-15299 from June 10, 2021. Henderson seconded.

Voting Yea: Burke, Garber, Henderson, Pickens

Motion made by Burke authorizing the purchase of .7 acres, more or less, of land located at 6801 S. I-35E, Corinth TX, 76210, H. Garrison Survey, Abstract No. 507, Tract 8 (F) and .787 acres, more or less, of land located at 3404 Dobbs Road, Corinth TX, 76210, H. Garrison Survey, Abstract No. 507, Tract 7 (F) in the amount of \$1,650,000 and authorizing the City Manager to execute the necessary documents. Seconded by Henderson.

Voting Yea: Burke, Garber, Henderson, Pickens

ADJOURN

Mayor Heidemann adjourned the meeting at 7:59 P.M.

AYES: All

Meeting adjourned.

Approved by Council on the _____ day of _____ 2021.

Lana Wylie, City Secretary
City of Corinth, Texas

CITY OF CORINTH
Staff Report



Meeting Date:	7/15/2021	Title: Contract Span
Strategic Goals:	<input type="checkbox"/> Citizen Engagement <input checked="" type="checkbox"/> Proactive Government <input type="checkbox"/> Organizational Development	
Governance Focus:	<i>Sub-Ends:</i>	
	<input type="checkbox"/> Growing Community <input type="checkbox"/> Conveniently located <input type="checkbox"/> Delivers Outstanding Service <input type="checkbox"/> High-Quality Retail <input type="checkbox"/> High-Quality Restaurants <input type="checkbox"/> High-Quality Entertainment	
	<i>Focus:</i> <input checked="" type="checkbox"/> Owner <input type="checkbox"/> Customer <input type="checkbox"/> Stakeholder	
	<i>Decision:</i> <input type="checkbox"/> Governance Policy <input checked="" type="checkbox"/> Ministerial Function	
Owner Support:	<input type="checkbox"/> Planning & Zoning Commission <input type="checkbox"/> Economic Development Corporation <input type="checkbox"/> Parks & Recreation Board <input type="checkbox"/> TIRZ Board #2 <input type="checkbox"/> Finance Audit Committee <input type="checkbox"/> TIRZ Board #3 <input type="checkbox"/> Keep Corinth Beautiful <input type="checkbox"/> Ethics Commission <u>N/A</u>	

Item/Caption

Consider and act on the SPAN Transportation contract for transportation services for senior citizen and Denton County Veterans for fiscal year 2021-2022, authorizing the City Manager to execute the necessary documents.

Item Summary/Background/Prior Action

The city has participated in a grant program offered through NCTCOG for transportation services providing transportation services for seniors. The grant covers trips for medical purposes and to/from the senior center in Lake Dallas. All four Lake Cities are parties to the program. The grant covers 100% of the costs in providing the service.

Financial Impact

Applicable Owner/Stakeholder Policy

Staff Recommendation/Motion

Approve as presented.

STATE OF TEXAS §

COUNTY OF DENTON §

INTERLOCAL AGREEMENT FOR SERVICES

THIS SERVICE AGREEMENT ("Agreement") is entered into by and between Corinth, Texas, acting by and through its duly authorized City Manager (hereinafter referred to as "CITY") and SPAN, Inc., (hereinafter referred to as "SPAN"), a Texas non-profit corporation operating in Denton County, Texas as an organization described in Section 501(c)(3) of the Internal Revenue Code, acting by and through its duly authorized Executive Director.

WHEREAS, SPAN enables people to live as fully and independently as possible by providing nutrition, transportation and social services to older persons, persons with disabilities, veterans, and the general public; and

WHEREAS, the success of or failure of the SPAN's purposes and objectives has a direct impact on the health and welfare of the citizens of the City; and

WHEREAS, the City is charged with the responsibility of promoting and preserving the health, safety, peace, good government, and welfare of its citizens; and

WHEREAS, SPAN transportation services were developed to provide safe and efficient transportation to seniors, persons with special needs, veterans and as otherwise defined by agreements into which SPAN may enter from time to time; and

WHEREAS, The CITY and SPAN desire to enter into this Agreement whereby SPAN will provide demand response transit service for CITY residents that are seniors (age 65 or older), and people with documented disabilities (hereafter referred to collectively as "Riders"); and

WHEREAS, Riders in CITY may be taken anywhere in SPAN's demand response transit service area in Denton County at a cost to the Riders of \$3.00 for seniors (age 65 and older) and also for people with documented disabilities for the purposes of medical treatments, doctor's and dentist's appointments, trips to get prescriptions filled, shopping for necessities, travel to and from the Lake Dallas Public Library, Employment, Education, Nutrition, Recreation, and Workshop trips; and

WHEREAS, Riders may call in at least one (1) day in advance, but no more than two (2) weeks in advance, to set up appointments for pick-up and drop off by calling SPAN'S Transportation Office at 940-382-1900 weekdays between the hours of 8:00 a.m. and 2:00 p.m.; and

WHEREAS, Demand response transit service is available between the hours of 7:00 a.m. and 6:00 p.m. Monday through Friday excluding major holidays and subject to availability constraints.

NOW, THEREFORE, THE CITY AND SPAN DO HEREBY COVENANT AND AGREE AS FOLLOWS:

1. Recitals

The foregoing recitals are found to be true and correct, are fully incorporated into the body of this Agreement and made a part hereof by reference just as though they are set out in their entirety.

2. Scope of Transportation Services

SPAN shall provide door-to-door demand response transit services to CITY citizens residing in Denton County who are Riders in accordance with this Agreement and SPAN's "Transportation Policy and Procedures" which is attached hereto as Exhibit "A" and incorporated herein by reference as though it were set out in its entirety ("Policy"). In the event of conflict between this Agreement and the Policy, this Agreement shall control. In performing services under this Agreement, the relationship between the CITY and SPAN is that of an independent contractor. No term or provision of this Agreement or act of SPAN in the performance of this Agreement shall be construed as making SPAN the agent, servant, or employee of the CITY.

3. SPAN Transportation Operations

- a. SPAN shall provide all equipment, facilities, qualified employees, training, and insurance necessary to establish a demand response transit service for the CITY's Riders. SPAN shall further establish, operate, and maintain an accounting system for this program that will allow for a tracking of services provided to Riders and a review of the financial status of the program. SPAN shall also track and break down the information regarding the number of one-way trips it provides to Riders.
- b. The CITY shall have the right to review the activities and financial records kept incident to the services provided to the CITY's Riders by SPAN. In addition, SPAN shall provide monthly ridership information to the City Manager or his/her designee specifically identifying the number of Rider trips including rider origination, destination, and purpose.
- c. SPAN will be responsible for verifying and documenting the eligibility of Riders. SPAN reserves the right to determine on an individual basis whether SPAN has the capability to safely transport a passenger. If safety is compromised, SPAN may decline transportation for this person and must document the reason why service was declined.
- d. Span will inform riders that their trips to the doctor or dentist's office, hospital, drug store or other location may qualify as a Medicaid eligible trip.
- e. Span reserves the right to immediately terminate services without warning if a passenger poses a safety risk to himself/herself or any other person. Span also reserves the right to suspend or terminate riders who violate Span's cancellation policy.

4. Payment

- a. SPAN shall provide one-way trips to the Town riders of Shady Shores, Hickory Creek, Lake Dallas and Corinth using 5310 grant funds.

5. Indemnification

SPAN assumes all liability and responsibility for and agrees to fully indemnify, hold harmless and defend the CITY, and its officials, officers, agents, servants and employees from and against any and all claims, damages, losses and expenses, including but not limited to attorney's fees, for injury to or death of a person or damage to property, arising out of or in connection with, directly or indirectly, the performance, attempted performance or nonperformance of the services described hereunder or in any way resulting from or arising out of the management, supervision, and operation of the program and activities of SPAN. In the event of joint and concurring responsibility of SPAN and the CITY, responsibility and indemnity, if any, shall be apportioned comparatively in accordance with Texas Law, without waiving any defense of either party under Texas Law. The provisions of this paragraph are solely for the benefit of the parties hereto and are not intended to create or grant any rights, contractual or otherwise, to any other person or entity.

6. Insurance

SPAN shall obtain public liability insurance of the types and in the amounts set forth below from an insurance carrier or underwriter licensed to do business in the State of Texas and acceptable to the CITY. SPAN shall furnish CITY with certificates of insurance or copies of the policies, evidencing the required insurance on or before the beginning date of this Agreement. SPAN agrees to submit new certificates or policies to CITY on before the expiration date of the previous certificates or policies. The insurance shall be the following types in amounts not less than indicated:

- a. Comprehensive General (Public) Liability Insurance or its equivalent including minimum coverage limits of \$1,000,000 per occurrence combined single limit for bodily injury and property damage.
- b. Automobile Liability Insurance including minimum coverage limits of \$1,000,000 per combined single limit for bodily injury and property damage.
- c. On all insurance required, SPAN shall require insurance providers to:

Name the CITY, and its officials, officers and employees, as additional insureds and provide thirty (30) days written notice to CITY of any material change to or cancellation of the insurance.

7. Assignment and Delegation

Neither party shall assign or delegate the rights or obligations under this Agreement without the prior written consent of the other party.

8. Severability

In the event any provision of this Agreement shall be determined by any court of competent jurisdiction to be invalid or unenforceable, the Agreement shall, to the extent reasonably possible, remain in full force and effect as to the balance of its provisions and shall be construed as if such invalid provision were not a part hereof.

9. Mediation

In the event of any dispute regarding this Agreement or the terms contained herein, the parties hereto agree that they shall submit such dispute to non-binding mediation.

10. Term of Agreement

The term of this Agreement shall be from October 1, 2021 through September 30, 2022, subject to renewal by the parties. Either party may modify this Agreement by submitting, in writing, the proposed amendment to be considered and executed by both parties. This Agreement may be terminated with or without cause by either party by giving thirty (30) days written notice to the other party of their intent to terminate the Agreement. In the event the CITY terminates without cause, SPAN shall be entitled to receive just and equitable compensation for any satisfactory work completed in accordance with this Agreement prior to the termination.

11. Applicable Law Venue

This Agreement shall be governed by, construed, and enforced in accordance with the laws of the State of Texas, and venue for any claim or cause of action shall lie exclusively in Denton County, Texas or the Federal courts having jurisdiction over claims arising in Denton County, Texas.

12. Attorney's Fees and Costs

In the event it becomes necessary to take legal action to enforce the terms of this Agreement, the prevailing party in such action shall be entitled to recover attorney's fees and costs of court from the non-prevailing party.

IN WITNESS WHEREOF the CITY of Corinth and Span, Inc. have executed this Agreement on this the

_____ day of _____, 2021.

SPAN, INC:

Michelle McMahon, Executive Director

City of Corinth

Bob Hart, City Manager

ATTEST:

Lana Wylie, City Secretary

CITY OF CORINTH
Staff Report

Meeting Date:	7/15/2021	Title:	Crime Control & Prevention District Budget
Strategic Goals:	<input checked="" type="checkbox"/> Citizen Engagement <input checked="" type="checkbox"/> Proactive Government <input checked="" type="checkbox"/> Organizational Development		
Governance Focus:	<i>Sub-Ends:</i> <input type="checkbox"/> Growing Community <input type="checkbox"/> Conveniently located <input checked="" type="checkbox"/> Delivers Outstanding Service <input type="checkbox"/> High-Quality Retail <input type="checkbox"/> High-Quality Restaurants <input type="checkbox"/> High-Quality Entertainment		
	<i>Focus:</i> <input checked="" type="checkbox"/> Owner <input type="checkbox"/> Customer <input type="checkbox"/> Stakeholder		
	<i>Decision:</i> <input checked="" type="checkbox"/> Governance Policy <input type="checkbox"/> Ministerial Function		
Owner Support:	<input type="checkbox"/> Planning & Zoning Commission <input type="checkbox"/> Economic Development Corporation <input type="checkbox"/> Parks & Recreation Board <input type="checkbox"/> TIRZ Board #2 <input type="checkbox"/> Finance Audit Committee <input type="checkbox"/> TIRZ Board #3 <input type="checkbox"/> Keep Corinth Beautiful <input type="checkbox"/> Ethics Commission <input checked="" type="checkbox"/> Crime Control & Prevention Board <input type="checkbox"/> Fire Control, Prevention & EMS Board <i>The Crime Control & Prevention Board will consider this item on July 15, 2021.</i>		

Item/Caption

Consider and act on the Fiscal Year 2022 Annual Budget for the City of Corinth Crime Control and Prevention District.

Item Summary/Background/Prior Action

Budgeted expenditures total \$489,913 which includes \$202,954 for the retention of two Police Officers, \$218,009 for Enterprise vehicle lease payments for patrol vehicles, \$11,250 for aftermarket patrol vehicle equipment through the Enterprise Lease program, ongoing equipment replacement of \$9,700 for tasers, \$14,000 for radars, \$6,500 for body cams, and \$7,000 for thermal cameras.

New Program funding includes one-time expenditures of \$20,500.

\$13,000 for a UAV Drone. The use of a drone in law enforcement has become commonplace. The Drone can be used for a myriad of tasks not only for the police department and other city departments. The Drone will be used as a force multiplier on major incident, for accident reconstruction, Search and Rescue, Crime Scene analysis, surveillance, crowd monitoring, suspect apprehension as well as other tasks throughout the city in other departments such as Fire and Public Works.

\$7,500 for Accident Reconstruction Equipment. The department has teamed up with the Little Elm Police Department to create a Traffic Accident Reconstruction Team. The training for each officer is very time consuming and costly. By creating this team, the Department has consolidated resources and has the ability to reconstruct accidents and crime scenes with Little Elm.

Financial Impact

The budget projects the sales tax revenue will generate \$409,190. The ending fund balance is estimated at \$587,846 for the 2021-2022 fiscal year.

Applicable Owner/Stakeholder Policy

The Texas Local Government Code Sections 363.204 and 363.205 prescribe procedures for adoption of the crime control budget. Texas Local Government Code 363.204(f) permits the Board to develop and adopt procedures for adopting a budget different from the procedures as outlined in the statute. In 2009, the Board adopted an order establishing the following procedures.

- a) Board shall hold a public hearing on the proposed budget for the District. Any resident of the district is entitled to be present and participate in the hearing.
- b) Board shall publish a notice of the public hearing in a newspaper with general circulation in the district not later than the fifth (5th) day before the date of the hearing.
- c) The proposed budget shall be made available in the Office of the City Secretary for public inspection at least five (5) days prior to the public hearing
- d) After the public hearing, the Board may make any changes in the proposed budget that in its judgment, is in the interest of the taxpayers of the District. The Board may adopt the budget immediately following the public hearing or at any time within ten (10) days following the public hearing.
- e) The Secretary of the Board shall submit the adopted budget to the City Council of the City of Corinth not later than the 10th day after the date the budget is adopted.

Staff Recommendation/Motion

Staff recommends approval of the FY2022 Crime Control and Prevention Annual Budget.



SALES TAX FUND - CRIME CONTROL & PREVENTION

The Corinth Police Department is committed to excellence in service through innovative and progressive policing methods. We value the trust of our citizens and are committed to carrying out our duties with honor, integrity and pride. Through partnerships and collaborative efforts, we will strive to enhance the safety and security in our community.

ACCOMPLISHMENTS FY2020-2021

- Supported the Police Enterprise Fleet Program.
- Replaced the Police Department WatchGuard Server and aging bodycams.

GOALS & OBJECTIVES FY2021-2022

- Support the Police Enterprise Fleet Program.
- Replace the Police Department aging Tasers.
- Purchase a drone for Police Department.
- Purchase Accident Reconstruction Equipment and Software.



CRIME CONTROL & PREVENTION (2203)

DESCRIPTION

The Corinth Crime Control & Prevention tax is a special tax levied for crime control and prevention that allows the City to provide the citizens with professional and efficient police services. The Crime Control & Prevention District Fund was established on January 1, 2005. A special election was held on September 11, 2004 for the purpose of adopting a .25% local sales and use tax for crime control & prevention. An election on May 2019 reauthorized the dedicated sales tax for another ten years.

RESOURCE SUMMARY	2019-20 ACTUAL	2020-21 BUDGET	2020-21 ESTIMATE	2021-22 BUDGET
Sales Tax	\$ 406,616	\$ 371,991	\$ 371,991	\$ 409,190
Investment Income	-	-	-	-
Interest Income	5,608	4,500	2,202	2,000
Miscellaneous	-	-	-	-
Gain on Sales	-	-	-	-
Transfers	-	-	-	-
TOTAL REVENUES	412,224	\$ 376,491	\$ 374,193	\$ 411,190
Use of Fund Balance	-	-	-	78,723
TOTAL RESOURCES	\$ 412,224	\$ 376,491	\$ 374,193	\$ 489,913

EXPENDITURE SUMMARY	2019-20 ACTUAL	2020-21 BUDGET	2020-21 ESTIMATE	2021-22 BUDGET
Wages & Benefits	\$ 145,021	\$ 194,529	\$ 194,529	\$ 202,954
Professional Fees	-	-	-	-
Maintenance & Operations	6,470	13,760	13,760	-
Supplies	-	72,930	72,930	16,200
Capital Outlay	37,387	18,980	18,980	52,750
Capital Lease	163,261	-	-	218,009
Transfers	-	-	-	-
TOTAL EXPENDITURES	\$ 352,139	\$ 300,199	\$ 300,199	\$ 489,913

PROJECTED FUND BALANCE REVIEW	2019-20 ACTUAL	2020-21 BUDGET	2020-21 ESTIMATE	2021-22 BUDGET
Beginning Fund Balance	\$ 532,490	\$ 592,575	\$ 592,575	\$ 666,569
Net Income	60,085	76,292	73,994	(78,723)
ENDING FUND BALANCE	592,575	\$ 668,867	\$ 666,569	\$ 587,846

PERSONNEL Full-Time Equivalents	2019-20 ACTUAL	2020-21 BUDGET	2020-21 ESTIMATE	2021-22 BUDGET
Sworn/Civil Service	2.00	2.00	2.00	2.00
TOTAL	2.00	2.00	2.00	2.00

NEW PROGRAM FUNDING

Drone - \$13,000; Taser Replacement - \$9,700; Accident Reconstruction - \$7,500; Body Camera Replacement - \$6,500

CITY OF CORINTH

Staff Report



Meeting Date:	7/15/2021	Title:	Fire District Budget Approval
Strategic Goals:	<input type="checkbox"/> Citizen Engagement <input checked="" type="checkbox"/> Proactive Government <input checked="" type="checkbox"/> Organizational Development		
Governance Focus:	<i>Sub-Ends:</i> <input type="checkbox"/> Growing Community <input type="checkbox"/> Conveniently located <input type="checkbox"/> Delivers Outstanding Service <input type="checkbox"/> High-Quality Retail <input type="checkbox"/> High-Quality Restaurants <input type="checkbox"/> High-Quality Entertainment		
	<i>Focus:</i> <input checked="" type="checkbox"/> Owner <input type="checkbox"/> Customer <input type="checkbox"/> Stakeholder		
	<i>Decision:</i> <input checked="" type="checkbox"/> Governance Policy <input type="checkbox"/> Ministerial Function		

Item/Caption

Consider and act on the fiscal year 2021-2022 budget for the Fire Control, Prevention, and Emergency Management Services District.

Item Summary/Background/Prior Action

In 2020, the Fire Control, Prevention, and Emergency Management Sales Tax District was created under Local Government Code Chapter 344. The district was created to support all costs of fire control, prevention, and emergency services, including costs for personnel, administration, expansion, enhancement, and capital expenditures.

The budget transfers operational expenditures from the general fund of \$442,176 and does not include any new program funding. The budget includes: Fire operations \$251,513, EMS \$171,998, Emergency Management \$9,855, Public Education \$4,934, Fire Prevention Bureau \$2,030 and Citizens Academy \$1,846.

Financial Impact

The budget projects the sales tax revenue will generate \$441,760. The ending fund balance is estimated at \$112,584 for the 2021-2022 fiscal year.

Applicable Owner/Stakeholder Policy

The Texas Local Government Code Sections 344.204 prescribe procedures for adoption of the Fire Control, Prevention & EMS District. The board by rule may adopt alternative procedures for adopting a budget that differ from the procedures outlined in this subchapter. The board must hold at least one public hearing related to the alternative procedures before their adoption.

In July 2021, the Board adopted an order requiring a public hearing on the proposed budget for the District, providing that any resident of the district be entitled to be present and participate in the hearing, and require that a notice of the public hearing be published in a newspaper with general circulation in the district not later than the fifth (5th) day before the date of the hearing. The notice above was published on Saturday, July 10, 2021 in the Denton Record Chronicle.

Staff Recommendation/Motion

Staff recommends approval of the FY 2021-2022 Fire Control, Prevention, and Emergency Management District Annual Budget.



SALES TAX FUND – FIRE CONTROL, PREVENTION, EMS DISTRICT

The Fire Control, Prevention and Emergency Medical Services District was approved by the voters on November 3, 2020, and it will support operations for fire prevention, fire operations, fire inspections, and fire training.

ACCOMPLISHMENTS FY2020-2021

- Passed an election in November 2020 for the creation of the Fire Control, Prevention, and Emergency Medical Services District.
- Developed the strategies approved by the Fire District Board: Fire Prevention, Fire Operations, Fire Inspection, and Fire Training.

GOALS & OBJECTIVES FY2021-2022

- Support the Operations of the Fire Department in the Delivery of exceptional service.
- Provide resources for improving the educational impact of fire and life safety on the community.

FIRE CONTROL, PREVENTION, EMS DISTRICT (133)

DESCRIPTION

The Fire District is funded by a sales tax that allows the City to provide the citizens with professional and efficient fire services. The creation of the Fire District Fund is was approved by the voters in a Special Election on November 3, 2020, for the purpose of adopting a .25% local sales and use tax for Fire services.

RESOURCE SUMMARY	2019-20 ACTUAL	2020-21 BUDGET	2020-21 ESTIMATE	2021-22 BUDGET
Sales Tax	\$ -	\$ 230,244	\$ 230,244	\$ 441,760
Investment Income	-	-	-	-
Interest Income	-	-	-	1,000
Miscellaneous	-	-	-	-
Transfers	-	-	-	-
TOTAL REVENUES	\$ -	\$ 230,244	\$ 230,244	\$ 442,760
Use of Fund Balance	-	-	-	-
TOTAL RESOURCES	\$ -	\$ 230,244	\$ 230,244	\$ 442,760

EXPENDITURE SUMMARY	2019-20 ACTUAL	2020-21 BUDGET	2020-21 ESTIMATE	2021-22 BUDGET
Wages & Benefits	\$ -	\$ -	\$ -	\$ -
Professional Fees	-	-	-	48,749
Maintenance & Operations	-	-	-	92,493
Supplies	-	-	-	253,246
Utilities	-	-	-	-
Communications	-	-	-	-
Vehicle & Fuel	-	-	-	6,655
Training	-	-	-	32,533
Capital Outlay	-	88,244	88,244	8,500
Capital Lease	-	-	-	-
Transfers	-	142,000	30,000	-
TOTAL EXPENDITURES	\$ -	\$ 230,244	\$ 118,244	\$ 442,176

PROJECTED FUND BALANCE REVIEW	2019-20 ACTUAL	2020-21 BUDGET	2020-21 ESTIMATE	2021-22 BUDGET
Beginning Fund Balance	\$ -	\$ -	\$ -	\$ 112,000
Net Income	-	0	112,000	584
ENDING FUND BALANCE	\$ -	\$ 0	\$ 112,000	\$ 112,584

NEW PROGRAM FUNDING

There is no new program funding.

CITY OF CORINTH
Staff Report

Meeting Date:	7/15/2021 Title: ILA Denton County Tax Collections
Strategic Goals:	<input type="checkbox"/> Citizen Engagement <input checked="" type="checkbox"/> Proactive Government <input type="checkbox"/> Organizational Development
Governance Focus:	<i>Sub-Ends:</i> <input type="checkbox"/> Growing Community <input type="checkbox"/> Conveniently located <input checked="" type="checkbox"/> Delivers Outstanding Service <input type="checkbox"/> High-Quality Retail <input type="checkbox"/> High-Quality Restaurants <input type="checkbox"/> High-Quality Entertainment
	<i>Focus:</i> <input checked="" type="checkbox"/> Owner <input type="checkbox"/> Customer <input type="checkbox"/> Stakeholder
	<i>Decision:</i> <input type="checkbox"/> Governance Policy <input checked="" type="checkbox"/> Ministerial Function
Owner Support:	<input type="checkbox"/> Planning & Zoning Commission <input type="checkbox"/> Economic Development Corporation <input type="checkbox"/> Parks & Recreation Board <input type="checkbox"/> TIRZ Board #2 <input type="checkbox"/> Finance Audit Committee <input type="checkbox"/> TIRZ Board #3 <input type="checkbox"/> Keep Corinth Beautiful <input type="checkbox"/> Ethics Commission <input type="checkbox"/> Crime Control & Prevention Board <input type="checkbox"/> Fire Control, Prevention & EMS Board

Item/Caption

Consider approval of an Interlocal Agreement with Denton County for the collection of property taxes for the City of Corinth.

Item Summary/Background/Prior Action

Property tax collection services are currently provided to the City of Corinth by Denton County through an Interlocal Agreement. The agreement includes, but is not limited to, the calculation of the effective and rollback tax rates, preparation of all truth-in-taxation notices, and the mailing of current and delinquent tax statements itemizing all taxes due per property account.

Financial Impact

The proposed agreement is for the period from October 1, 2021 through September 30, 2022 and will renew automatically unless terminated by either party. The per statement cost of \$1.00 remains unchanged (estimated total cost of \$8,300) for the October, February and May statement printing for each jurisdiction. The parcel fee will be analyzed annually and submitted by separate notice

Applicable Owner/Stakeholder Policy

N/A

Staff Recommendation/Motion

Staff recommends approval of the Interlocal Agreement with Denton County.



THE STATE OF TEXAS §

COUNTY OF DENTON §

INTERLOCAL COOPERATION AGREEMENT FOR PROPERTY TAX

ASSESSMENT AND COLLECTION BETWEEN

DENTON COUNTY, TEXAS AND CITY/TOWN OF _____, TEXAS

INTERLOCAL COOPERATION AGREEMENT –TAX COLLECTION

THIS AGREEMENT is made and entered into by and between **DENTON COUNTY**, a political subdivision of the State of Texas, hereinafter referred to as "**COUNTY**," and _____, Denton County, Texas, also a political subdivision of the State of Texas, hereinafter referred to as "**MUNICIPALITY**."

WHEREAS, COUNTY and **MUNICIPALITY** mutually desire to be subject to the provisions of Texas Government Code, Chapter 791 (the Interlocal Cooperation Act), and Section 6.24 of the Texas Tax Code; and;

WHEREAS, MUNICIPALITY has the authority to contract with the **COUNTY** for the **COUNTY** to act as tax assessor and collector for **MUNICIPALITY** and **COUNTY** has the authority to so act.

NOW THEREFORE, COUNTY and **MUNICIPALITY**, for and in consideration of the mutual promises, covenants, and agreements herein contained, do agree as follows:

Throughout this Agreement, the term "Property Tax Code" means Title 1 of the Texas Tax Code. Throughout this Agreement, the term "tax year" means the calendar year in which the applicable tax lien attaches to the taxable property. The term "collection year" refers to the period commencing on October 1st of the applicable tax year and continuing through the end of the applicable term (September 30th of the following year), in which collection and billing services are to be performed under this Agreement.

I.

The effective date of this Agreement shall be October 1, 2021. The initial term of this Agreement shall be for a period of one year beginning on the effective date and ending on, September 30, 2022. The initial term of the Agreement is for tax year 2021 property tax rate calculation, billing and collection services. Following the initial term, this Agreement shall automatically renew for subsequent one-year terms, unless written notice of termination is provided by **COUNTY** or **MUNICIPALITY** no later than one hundred-eighty (180) days prior to the expiration date of the then-current term of the Agreement. If said notice is provided, this Agreement shall terminate at the end of the then-current term. Each renewal term shall be for property tax rate calculation, billing and collection services for the applicable tax year (the first renewal term will be for tax year 2022, the second renewal terms for tax year 2023, etc.).

II.

For the purposes and consideration herein stated and contemplated, **COUNTY** shall provide the following necessary and appropriate services for **MUNICIPALITY** to

the maximum extent authorized by this Agreement, without regard to race, sex, religion, color, age, disability, or national origin:

1. COUNTY, by and through its duly qualified tax assessor/collector, shall serve as tax assessor/collector for MUNICIPALITY for ad valorem tax collection for the tax year. COUNTY agrees to perform all necessary ad valorem assessing and collecting duties for MUNICIPALITY and MUNICIPALITY does hereby expressly authorize COUNTY to do and perform all acts necessary and proper to assess and collect taxes for MUNICIPALITY. COUNTY agrees to collect base taxes, penalties, interest, and attorney's fees.

2. COUNTY agrees to prepare and mail all current and delinquent tax statements required by statute, supplemental changes for applicable property accounts, as well as prepare and mail any other mailing as deemed necessary and appropriate by COUNTY; provide daily, monthly and annual collection reports to MUNICIPALITY; prepare tax certificates; develop and maintain both current and delinquent tax rolls, disburse tax monies to MUNICIPALITY daily (business day) based on prior day tax postings, approve and refund overpayment or erroneous payment of taxes for MUNICIPALITY pursuant to Property Tax Code Chapter 31 from available current tax collections of MUNICIPALITY; and to meet the requirements of Section 26.04 and Chapter 42, Subchapter C and develop and maintain such other records and forms as are necessary or required by State law, rules, or regulations. If daily disbursal is to be delayed, COUNTY will notify MUNICIPALITY in the secured web entity folder the reason for the delay.

3. COUNTY further agrees that it will make for MUNICIPALITY the property tax rate calculations required by Property Code Section 26.04 (currently identified in the Section by the terms "no new revenue tax rate" and "voter-approval tax rate"), and will do so in accordance with all requirements therein. All such rate calculations will be performed using only the Texas State Comptroller's "Truth In Taxation" formulas, and at no additional cost to MUNICIPALITY. The information concerning the rate calculations described in this Article II.3 and publications will be provided to MUNICIPALITY in the form prescribed by the Comptroller of Public Accounts of the State of Texas, and as required by Property Tax Code Chapter 26. MUNICIPALITY shall be responsible for all publications as required by Chapter 26. In the event MUNICIPALITY requires early calculation based on certified estimate values, COUNTY will perform the tax rate calculations described in this Article II.3. and provide the required publications to MUNICIPALITY in the same manner as performing the tax rate calculations pursuant to the annual appraisal district reports required to be Certified on July 25 of each tax year.

4. COUNTY agrees, upon request, to offer guidance and the necessary forms for posting notices as required by Chapter 26 of the Property Tax Code if MUNICIPALITY requests such no less than 7 days in advance of the intended publication date. MUNICIPALITY must approve all calculations and notices, in the format required by COUNTY and Property Tax Code Chapter 26. The accuracy and timeliness of all required notices are the responsibility of MUNICIPALITY. COUNTY will update tax transparency databases, as required in Property Tax Code Sections 26.17(b),(5A,B),(7),(12),(13) and 26.17(e)(2) with applicable Truth In Taxation

worksheets and Notices. **MUNICIPALITY** is responsible for any other required information posted on a tax transparency database. This Agreement is subject to and the parties herein shall comply with all applicable provisions of the Property Tax Code and all other applicable Texas statutes. **COUNTY** will submit to **MUNICIPALITY** approval forms of the tax rate calculation and required notices. **MUNICIPALITY** must return executed approval forms to tax assessor/collector as required by law and this agreement.

5. Should **MUNICIPALITY** vote to increase its tax rate above the statutory voter approval limit (also known as the "rollback" or the "voter approval" rate), the required publication of notices shall be the responsibility of the **MUNICIPALITY**. Should **MUNICIPALITY** roll back the tax rate as a result of Tax Rate Election, the required publication of notices shall be the responsibility of **MUNICIPALITY**.

6. **COUNTY** agrees to develop and maintain written policies and procedures of its operation. **COUNTY** further agrees to make available full information about the operation of the County Tax Office to **MUNICIPALITY**, and to promptly furnish written reports to keep **MUNICIPALITY** informed of all financial information affecting it.

7. **MUNICIPALITY** agrees to promptly deliver to **COUNTY** all records that it has accumulated and developed in the assessment and collection of taxes, and to cooperate in furnishing or locating any other information and records needed by **COUNTY** to perform its duties under the terms and conditions of this Agreement.

8. **COUNTY** agrees to allow an audit of the tax records of **MUNICIPALITY** in **COUNTY'S** possession during normal working hours with at least 72 hours advance, written notice to **COUNTY**. The expense of any and all such audits shall be paid by **MUNICIPALITY**. A copy of any and all such audits shall be furnished to **COUNTY**.

9. If required by **MUNICIPALITY**, **COUNTY** agrees to obtain a surety bond for the County Tax Assessor/Collector. Such bond will be conditioned upon the faithful performance of the tax assessor/collector’s lawful duties, will be made payable to **MUNICIPALITY** and in an amount determined by the governing body of **MUNICIPALITY**. The premium for any such bond shall be borne solely by **MUNICIPALITY**.

10. **COUNTY** agrees that it will post a notice on its website, as a reminder that delinquent tax penalties will apply to all assessed taxes that are not paid by January 31st of the collection year.

11. **COUNTY** agrees that it will post to a secure website collection reports for **MUNICIPALITY** listing current taxes, delinquent taxes, penalties and interest on a daily basis through September 30th of the collection year. **COUNTY** will provide monthly Maintenance and Operation (hereinafter referred to as “MO”), and Interest and Sinking (hereinafter referred to as “IS”) collection reports; provide monthly recap reports; and provide monthly attorney fee collection reports.

12. **MUNICIPALITY** retains its right to select its own delinquent tax collection attorney and **COUNTY** agrees to reasonably cooperate with the attorney selected by **MUNICIPALITY** in the collection of delinquent taxes and related activities.

13. **MUNICIPALITY** will provide **COUNTY** with notice of any change in collection attorney at least 7 days before the effective date of the new collection attorney contract.

III.

COUNTY hereby designates the Denton County Tax Assessor/ Collector to act on behalf of the County Tax Office and to serve as Liaison for **COUNTY** with **MUNICIPALITY**. The County Tax Assessor/Collector, and/or his/her designated substitute, shall ensure the performance of all duties and obligations of **COUNTY**; shall devote sufficient time and attention to the execution of said duties on behalf of **COUNTY** in full compliance with the terms and conditions of this Agreement; and shall provide immediate and direct supervision of the County Tax Office employees, agents, contractors, subcontractors, and/or laborers, if any, in the furtherance of the purposes, terms and conditions of this Agreement for the mutual benefit of **COUNTY** and **MUNICIPALITY**.

IV.

COUNTY accepts responsibility for the acts, negligence, and/or omissions related to property tax service of all **COUNTY** employees and agents, sub-contractors and/or contract laborers, and for those actions of other persons doing work under a contract or agreement with **COUNTY** to the extent allowed by law.

V.

MUNICIPALITY accepts responsibility for the acts, negligence, and/or omissions of all **MUNICIPALITY** employees and agents, sub-contractors and/or contract laborers, and for those of all other persons doing work under a contract or agreement with **MUNICIPALITY** to the extent allowed by law.

VI.

MUNICIPALITY understands and agrees that **MUNICIPALITY**, its employees, servants, agents, and representatives shall at no time represent themselves to be employees, servants, agents, and/or representatives of **COUNTY**. **COUNTY** understands and agrees that **COUNTY**, its employees, servants, agents, and representatives shall at no time represent themselves to be employees, servants, agents, and/or representatives of **MUNICIPALITY**.

VII.

For the services rendered during the tax year, **MUNICIPALITY** agrees to pay **COUNTY** for the receipting, bookkeeping, issuing, and mailing of tax statements as follows:

1. The current tax statements will be mailed by October 10th of the tax year or as soon thereafter as practical. The **MUNICIPALITY** must adopt its tax year tax rate on or before September 30th of the applicable tax year, if that rate does not exceed the voter-approval tax rate. **MUNICIPALITY** must adopt a tax rate that exceeds the voter-approval tax rate not later than the deadline set forth in Property Tax Code Section 26.05(a) and Election Code 3.005 and 41.001. In order to expedite mailing of tax statements, **MUNICIPALITY** shall adopt and then deliver its adopted tax rate to **COUNTY** no later than the applicable adoption deadline described herein. Failure by **MUNICIPALITY** to adopt and then deliver the adopted tax rate to **COUNTY** by said applicable adoption deadline may result in delay of processing and mailing **MUNICIPALITY** tax statements. **MUNICIPALITY** agrees to assume the costs for additional delayed tax statements, processing and mailing as determined by **COUNTY**. An additional notice will be sent

during the month of March following the initial mailing provided that **MUNICIPALITY** has requested such a notice on or before February 28th of the collection year. During the initial term of this Agreement, the fee for this service will be **\$1.00** per statement. During the first and second renewal terms of this Agreement, the fee for this service will be the per statement rate approved by Commissioners Court for the applicable tax year, provided notice of that rate is provided to **MUNICIPALITY** as described in Section 8 of this Article VII. In the event **COUNTY** does not provide **MUNICIPALITY** with said notice, the rate charged during the preceding term will apply.

2. At least 30 days, but no more than 60 days prior to April 1st of the collection year and following the initial mailing, a delinquent tax statement meeting the requirements of Section 33.11 of the Property Tax Code will be mailed to the owner of each parcel having delinquent taxes.

3. At least 30 days, but no more than 60 days prior to July 1st of the collection year and following the initial mailing, a delinquent tax statement meeting the requirements of Section 33.07 of the Property Tax Code will be mailed to the owner of each parcel having delinquent taxes.

4. For accounts that become delinquent on or after June 1st of the collection year, **COUNTY** shall mail a delinquent tax statement meeting the requirements of Section 33.08 of the Property Tax Code to the owner of each parcel having delinquent taxes.

5. For accounts that become delinquent on February 1st of the tax year, **COUNTY**, in its sole discretion, may mail a reminder notice to the owner of each parcel having delinquent taxes not including February 33.11 notices.

6. In event of a tax rate change resulting from a rollback or tax approval election that takes place after tax bills for **MUNICIPALITY** have been mailed, **MUNICIPALITY** agrees to pay **COUNTY** a programming charge of \$5,000.00. **COUNTY**, pursuant to Property Tax Code Section 26.07(f) or 26.075(j) will mail corrected statements to the owner of each property. The fee for this service will be the same per statement rate described in Section 1 of this Article VII. When a refund is required per Property Tax Code Section 26.07(g) or 26.075(k) **COUNTY** will charge a \$.25 processing fee per check, in addition to the corrected statement mailing costs. Issuance of refunds, in the event of a successful rollback election, will be the responsibility of the **COUNTY**. **MUNICIPALITY** will be billed for the refunds, postage and processing fees.

7. **MUNICIPALITY** understands and agrees that **COUNTY** will, no later than January 31st of the tax year, deduct from current collections of **MUNICIPALITY** the "Total Cost" of providing all services described in Sections 1-5 above. This "Total Cost" includes any such services that have not yet been performed at the time of deduction. During the initial term of this Agreement, the "Total Cost" of providing all services described in Sections 1-5 above shall be the total of: **\$1.00** (the "per parcel rate") x the total number of parcels listed on **MUNICIPALITY's** preceding tax year Tax Roll on September 30th of the tax year. During the first and second renewal terms of this agreement, the "per parcel rate" will be the per parcel rate approved by Commissioners Court for the applicable tax year, provided notice of that rate is provided to **MUNICIPALITY** as described in Sections 1 and 8 of this Article VII. In the event **COUNTY** does not provide **MUNICIPALITY** with said notice, the per parcel rate charged during the preceding term will apply.

In the event that a rollback or tax rate approval election as described in Section 6 of this Article VII takes place, **COUNTY** shall bill **MUNICIPALITY** for the applicable programming charge, check processing fees, refunds paid, and refund postage costs. **MUNICIPALITY** shall pay **COUNTY** all billed amounts within 30 days of its receipt of said bill. In the event costs for additional delayed tax statements, processing and mailing are incurred as described in Section 1 of this Article VII, **COUNTY** shall bill **MUNICIPALITY** for such amounts. **MUNICIPALITY** shall pay **COUNTY** all such billed amounts within 30 days of its receipt of said bill.

8. The County Budget Office establishes collection rates annually based on a survey of actual annual costs incurred by the County in performing tax collection services. The collection rate for each tax year is approved by County Commissioners' Court, and all entities are assessed the same per parcel collection rate. Following approval of the collection rate for each tax year, **COUNTY** will, at least sixty (60) days prior to the expiration date of the then-current term of this Agreement, provide **MUNICIPALITY** with written notice of that rate.

VIII.

COUNTY agrees to remit all taxes, penalties, and interest collected on **MUNICIPALITY's** behalf and to deposit such funds into the **MUNICIPALITY's** depositories, as designated:

1. For deposits of tax, penalties, and interest, payment shall be by wire transfer or ACH to **MUNICIPALITY's** depository accounts only, and segregated into the appropriate MO and IS accounts, as applicable, specified on the Direct Deposit Authorization executed between the **MUNICIPALITY** and **COUNTY**. Only in the event

of failure of electronic transfer protocol will a check for deposits of tax, penalty and interest be sent by mail to **MUNICIPALITY**.

2. In anticipation of renewal of this Agreement, **COUNTY** further agrees that deposits will be made daily through September 30th of the collection year. It is expressly understood, however, that this obligation of **COUNTY** shall not survive termination of this Agreement, whether by termination by either party or by failure of the parties to renew this Agreement.

3. In event that **COUNTY** experiences shortage in collections as a result of an outstanding tax debt of **MUNICIPALITY**, the **MUNICIPALITY** agrees a payment in the amount of shortage shall be made by check or ACH to **COUNTY** within 15 days after notification of such shortage. Failure to remit payment of shortage restricts release of collected taxes until such time as payment is remitted

IX.

In the event of termination, the terminating party shall be obligated to make such payments as are required by this Agreement through the balance of the tax year in which notice is given. **COUNTY** shall be obligated to provide services pursuant to this Agreement during such period.

X.

This Agreement represents the entire agreement between **MUNICIPALITY** and **COUNTY** and supersedes all prior negotiations, representations, and/or agreements, either written or oral. This Agreement may be amended only by written instrument signed by the

governing bodies of both **MUNICIPALITY** and **COUNTY** or those authorized to sign on behalf of those governing bodies.

XI.

Any and all written notices required to be given under this Agreement shall be delivered or mailed to the listed addresses:

COUNTY:

County Judge of Denton County
110 West Hickory
Denton, Texas 76201
Telephone: 940-349-2820

MUNICIPALITY:

XII.

MUNICIPALITY hereby designates _____ to act on behalf of **MUNICIPALITY**, and to serve as Liaison for **MUNICIPALITY** to ensure the performance of all duties and obligations of **MUNICIPALITY** as stated in this Agreement. **MUNICIPALITY**'s designee shall devote sufficient time and attention to the execution of said duties on behalf of **MUNICIPALITY** in full compliance with the terms and conditions of this Agreement; shall provide immediate and direct supervision of the **MUNICIPALITY** employees, agents, contractors, subcontractors, and/or laborers, if any,

in the furtherance of the purposes, terms and conditions of this Agreement for the mutual benefit of **MUNICIPALITY** and **COUNTY**.

XIII.

In the event that any portion of this Agreement shall be found to be contrary to law, it is the intent of the parties that the remaining portions shall remain valid and in full force and effect to the extent possible.

XIV.

The undersigned officers and/or agents of the parties are the properly authorized officials and have the necessary authority to execute this agreement on behalf of the parties. Each party hereby certifies to the other that any resolutions necessary for this Agreement have been duly passed and are now in full force and effect.

Executed in triplicate originals this, _____ day of _____
20____.

COUNTY

Denton County Texas
110 West Hickory
Denton, Texas 76201

MUNICIPALITY

City/Town:
Street address:
City, state, zip:
Email:
Phone:

BY: _____
Honorable Andy Eads
County Judge

BY: _____
Name: _____
Title: _____

ATTEST:

ATTEST:

BY: _____
Juli Luke
Denton County Clerk

BY: _____
Name _____
Title _____

APPROVED FORM AND CONTENT:

APPROVED AS TO FORM:

Michelle French

Tax Assessor/Collector

Attorney Denton County

**CITY OF CORINTH
Staff Report**



Meeting Date:	7/15/2021	Title: Lake Sharon Drive / Dobbs Road Street Name Change
Strategic Goals:	<input type="checkbox"/> Citizen Engagement <input checked="" type="checkbox"/> Proactive Government <input type="checkbox"/> Organizational Development	
Governance Focus:	<i>Sub-Ends:</i> <input type="checkbox"/> Growing Community <input type="checkbox"/> Conveniently located <input checked="" type="checkbox"/> Delivers Outstanding Service <input type="checkbox"/> High-Quality Retail <input type="checkbox"/> High-Quality Restaurants <input type="checkbox"/> High-Quality Entertainment	
	<i>Focus:</i> <input checked="" type="checkbox"/> Owner <input checked="" type="checkbox"/> Customer <input type="checkbox"/> Stakeholder	
	<i>Decision:</i> <input type="checkbox"/> Governance Policy <input checked="" type="checkbox"/> Ministerial Function	
Owner Support:	<input type="checkbox"/> Planning & Zoning Commission <input type="checkbox"/> Economic Development Corporation <input type="checkbox"/> Parks & Recreation Board <input type="checkbox"/> TIRZ Board #2 <input type="checkbox"/> Finance Audit Committee <input type="checkbox"/> TIRZ Board #3 <input type="checkbox"/> Keep Corinth Beautiful <input type="checkbox"/> Ethics Commission Click to enter recommendation/decision of supporting group.	

Item/Caption

Consider and take action to rename Dobbs Road between Quail Run Drive and I-35E Frontage Road to Lake Sharon Drive, to be known as the 3600 to 4000 blocks of Lake Sharon Drive.

Item Summary/Background/Prior Action

In consideration of the impending Capital Improvement Project for the realignment and reconstruction of Dobbs Road from I-35E Frontage Road to Quail Run Drive, it in the best interests of the public to rename Dobbs Road between Quail Run Drive and I-35E Frontage Road to Lake Sharon Drive, to be known as the 3600 to 4000 blocks of Lake Sharon Drive. This will also be consistent with the Texas Department of Transportation Overpass design at Lake Sharon Drive and I-35E.

Initially, the timing of this name change was going to be during the CIP project, however, there is an application for a lot within the Lake Vista Business Park and therefore the City needs to provide proper addressing. As opposed to assigning Dobbs Road addressing and having the owner’s change their stationary once we realign the road.

Financial Impact

\$150/sign.

Applicable Owner/Stakeholder Policy

Staff Recommendation/Motion

Recommend approval of an ordinance to rename Dobbs Road between Quail Run Drive and I-35E Frontage Road to Lake Sharon Drive, to be known as the 3600 to 4000 blocks of Lake Sharon Drive.

ORDINANCE NO. _____

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF CORINTH, TEXAS, RENAMING PORTIONS OF DOBBS ROAD BETWEEN QUAIL RUN DRIVE AND I-35E FRONTAGE ROAD TO LAKE SHARON DRIVE; PROVIDING FOR THE INCORPORATION OF PREMISES; PROVIDING A SEVERABILITY CLAUSE; PROVIDING A CUMULATIVE REPEALER CLAUSE; PROVIDING FOR NOTICES; AND PROVIDING FOR PUBLICATION AND AN EFFECTIVE DATE.

WHEREAS, the City of Corinth is a home rule city acting under its charter adopted by the electorate pursuant to Article XI, Section 5 of the Texas Constitution and Chapter 9 of the Local Government Code; and

WHEREAS, City staff has recommended that portions of Dobbs Road be renamed to Lake Sharon Drive to facilitate development in the respective area; and

WHEREAS, the City Council has notified each owner of property adjacent to, abutting and/or having an address on the street being considered for a name change; and

WHEREAS, the City Council held a public hearing on July 15, 2021, to allow persons to speak in favor or against the proposed renaming; and

WHEREAS, after considering the public comment, the City Council deems it necessary and in the best interests of the public to rename Dobbs Road between Quail Run Drive and I-35E Frontage Road to Lake Sharon Drive, to be known as the 3600 to 4000 blocks of Lake Sharon Drive; and

WHEREAS, the Mayor or designee shall direct the replacement of street signs reflecting the street name change; and

WHEREAS, the Denton County Central Appraisal District, Post Office and other service providers shall be notified at the direction of the Mayor or designee of the street name change.

NOW, THEREFORE, BE IT ORDAINED BY THE COUNCIL OF THE CITY OF CORINTH:

SECTION 1

All of the above premises are hereby found to be true and correct legislative and factual determinations of the City of Corinth and are hereby approved and incorporated into the body of this Ordinance as if copied in their entirety.

SECTION 2

That the portion of Dobbs Road between Quail Run Drive and I-35E Frontage Road is hereby renamed Lake Sharon Drive, to be known as the 3600 to 4000 blocks of Lake Sharon Drive.

SECTION 3

This Ordinance shall be cumulative of all provisions of ordinances of the City of Corinth, Texas, except where the provisions of this Ordinance are in direct conflict with the provisions of such ordinances, in which event the conflicting provisions of such ordinances are hereby repealed.

SECTION 4

It is hereby declared to be the intention of the City Council that the phrases, clauses, sentences, paragraphs, and sections of this Ordinance are severable, and if any phrase, clause, sentence, paragraph, or section of this Ordinance shall be declared unconstitutional by the valid judgment or decree of any court of competent jurisdiction, such unconstitutionality shall not affect any of the remaining phrases, clauses, sentences, paragraphs, and sections of this Ordinance, since same would have been enacted by the City Council without the incorporation in this Ordinance of any such unconstitutional phrase, clause, sentence, paragraph, or section.

SECTION 5

That the Mayor or designee is hereby authorized and directed to change the City's maps, and records to reflect the name change approved with this Ordinance and is directed to notify the Lake Cities Fire Department, Corinth Police Department, United States Postal Service, Denton County and Denco 911 District of such changes; and further, the Mayor or designee is authorized to erect signs reflecting the name change.

SECTION 6

This Ordinance shall take effect and be in full force from and after its passage and publication, as required by law, and it is so ordained.

DULY PASSED AND APPROVED BY THE CITY COUNCIL OF THE CITY OF CORINTH, TEXAS on this 15th day of July, 2021.

Bill Heidemann, Mayor

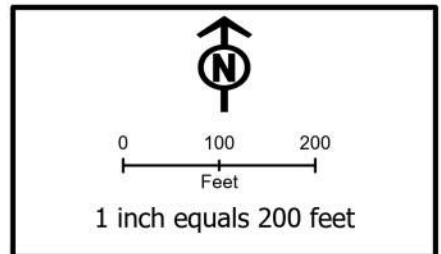
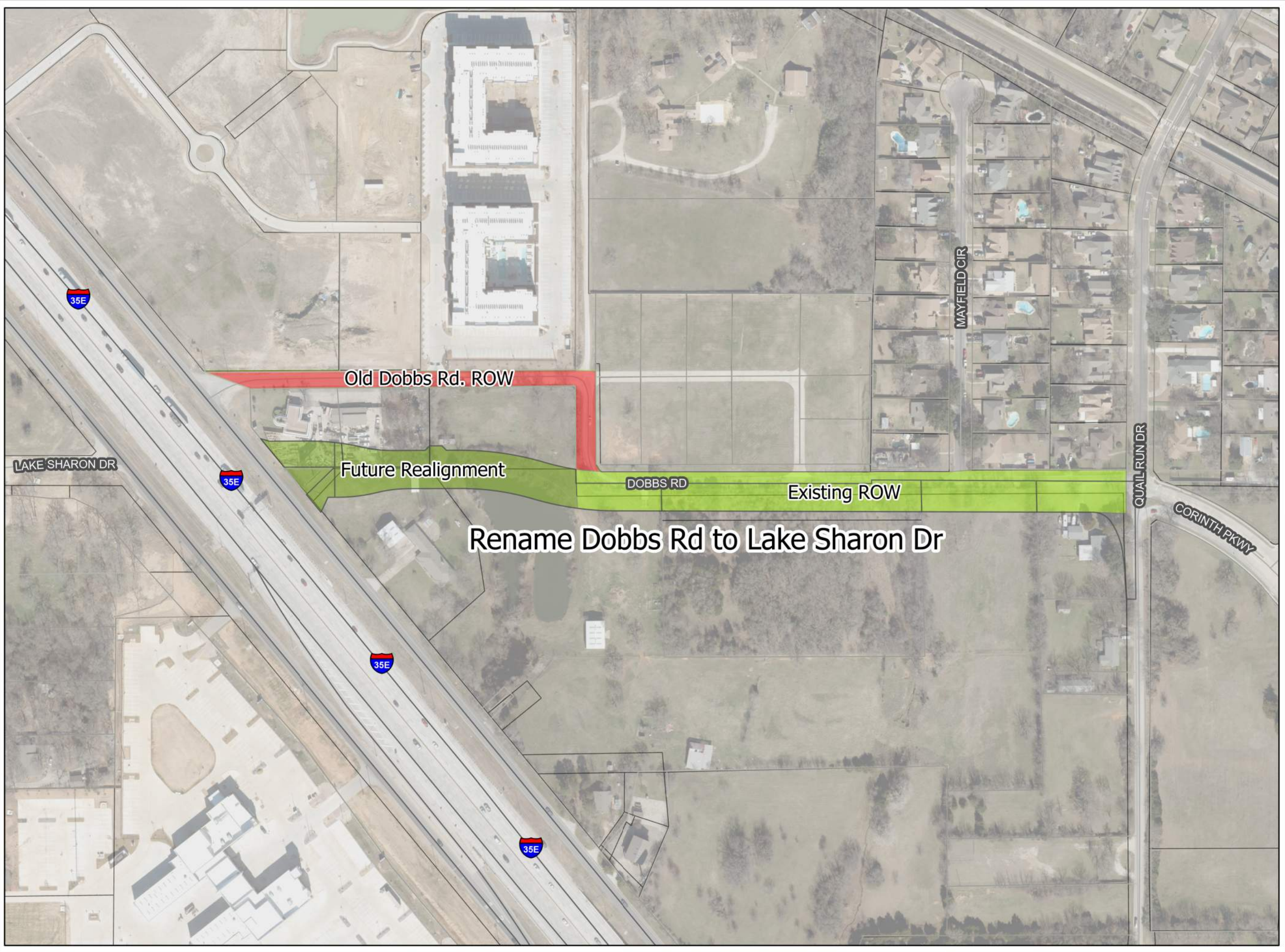
Attest:

Lana Wylie, City Secretary

Approved as to Legal Form:

Patricia A. Adams, City Attorney

***Rename Dobbs Rd.
to Lake Sharon Dr***



This map is the property of the City of Corinth, and is not to be reproduced by any means, mechanical or digital, without written consent of the City. This product is for informational purposes and may not have been prepared for or be suitable for legal, engineering, or surveying purposes. It does not represent an on-the-ground survey and represents only the approximate relative location of boundaries.

**CITY OF CORINTH
Staff Report**



Meeting Date:	7/15/2021	Title:	Avilla Fairways Planned Development
Strategic Goals:	<input checked="" type="checkbox"/> Citizen Engagement <input checked="" type="checkbox"/> Proactive Government <input type="checkbox"/> Organizational Development		
Governance Focus:	<i>Sub-Ends:</i>		
	<input checked="" type="checkbox"/> Growing Community <input checked="" type="checkbox"/> Conveniently located <input type="checkbox"/> Delivers Outstanding Service <input checked="" type="checkbox"/> High-Quality Retail <input type="checkbox"/> High-Quality Restaurants <input type="checkbox"/> High-Quality Entertainment		
	<i>Focus:</i> <input type="checkbox"/> Owner <input type="checkbox"/> Customer <input type="checkbox"/> Stakeholder		
	<i>Decision:</i> <input checked="" type="checkbox"/> Governance Policy <input type="checkbox"/> Ministerial Function		
Owner Support:	<input checked="" type="checkbox"/> Planning & Zoning Commission <input type="checkbox"/> Economic Development Corporation <input type="checkbox"/> Parks & Recreation Board <input type="checkbox"/> TIRZ Board #2 <input type="checkbox"/> Finance Audit Committee <input type="checkbox"/> TIRZ Board #3 <input type="checkbox"/> Keep Corinth Beautiful <input type="checkbox"/> Ethics Commission		
	<p><u>Planning and Zoning Commission recommended the request for approval to City Council with the condition that masonry exterior building finishes be enhanced to include 100% brick and/or stone. Staff proposes an alternative to the Planning and Zoning Commission’s recommendation as outlined in Section II.E.14., Building Materials as contained herein.</u></p>		

Item/Caption

Conduct a Public Hearing to consider testimony and take action on a rezoning request to amend the zoning classification from PD-6 Planned Development District, Ordinance No. 87-12-17-24, for Two Family Garden Homes, Townhomes, and Neighborhood Shopping and PD 24 Planned Development District, Ordinance No. 99-12-16-45 for Two Family Garden Homes to PD Planned Development District with a base zoning district of MF-1 Multi-Family Residential, on approximately 24.595 acres of land within the A.H. Serren Survey, Abstract No. 1198 and the B. Merchant Survey, Abstract No. 800, City of Corinth, Denton County, Texas. The property is generally located at the northwest corner of Lake Sharon Drive and Oakmont Drive and east of FM 2499. (Avilla Fairways PD ZAPD20-0004)

Item Summary/Background/Prior Action

I. Item Summary:

Avilla Fairways project (*a 215 unit residential cottage community*) was first reviewed publicly at the February 22, 2021, Planning and Zoning Commission Meeting. The item was not recommended for City Council approval by the Commission. Rezoning applications are forwarded to City Council to hold the public hearing as published and act on the item. The applicant requested the rezoning application be returned to the Planning & Zoning Commission for consideration with revisions and new supporting information/studies in response to the speakers’ concerns during the public hearing and written comments.

On March 18, 2021 the City Council unanimously voted to remand the zoning request to the Planning and Zoning Commission and for Staff to work with the applicant on outstanding issues. There were subsequent meetings with the neighbors and City Staff to review the existing and proposed zoning and an additional neighborhood meeting held with the developer and residents.

On June 28, 2021, the Planning and Zoning Commission conducted a second public hearing on the topic providing input both via Zoom and in person. The public comment echoed the concerns raised at the initial February 22, 2021, Planning and Zoning Commission meeting as follows:

- Multi-Family (rental) use and density at the proposed location
- Transportation, street connectivity, and pedestrian safety
- Drainage Management (Stormwater/Floodplain/Wetlands)
- School attendance
- Location of Dog Park (near to Oakmont Golf Course)
- Heavily Treed Site (Tree Preservation)

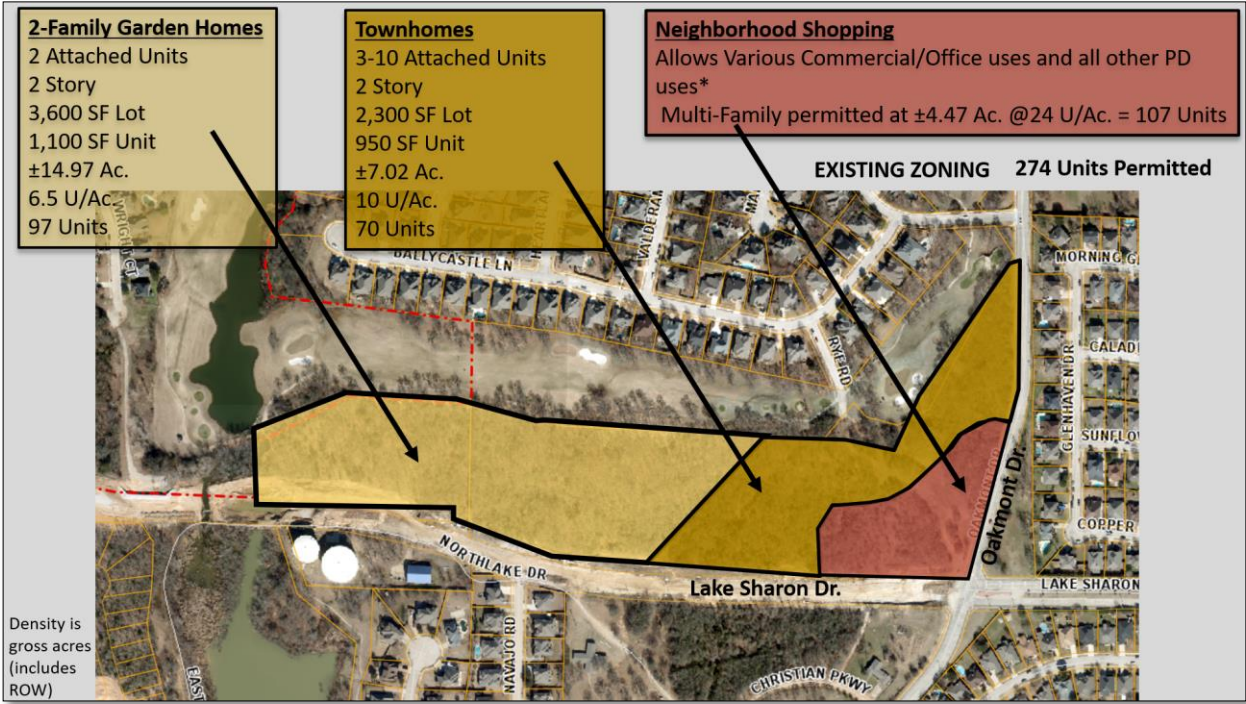
To address these concerns, Staff has the following to report:

A. Multi-Family use at the proposed location:

The existing zoning for the Oakmont Planned Development (PD) District was established in 1987 on approximately 566.9 acres. In 1999 another ±5.709 acres immediately west of the subject tract was zoned for Two Family Garden Homes. The Oakmont PD has slowly developed since 1987 and there are a few remaining undeveloped parcels. This particular tract had not been strongly marketed for sale and had not been as attractive for development since Lake Sharon Drive improvements had not been completed. The roadway completion has provided additional access for the site.

The tracts within the overall subject parcel were assigned the following subdistricts:

1. Neighborhood Shopping
2. Townhomes
3. Two Family Garden Homes



The Oakmont PD provides for “cumulative zoning” in subdistricts. This means that uses permitted in a subdistrict are allowed in other subdistricts. For example:

- Neighborhood Shopping also permits all the Garden Office uses
- Garden Office also permits all uses in the Apartment/Condominiums areas
- Apartment/Condominiums also permits Multi-Family units and all uses in the Villas areas
- Villas also permits Multi-Family units and all uses in the Townhomes areas
- Townhomes also permit Single Family Attached units and all uses permitted in the Two Family Garden Homes areas
- Two Family Garden Homes also permit Single Family Attached units and all uses permitted in the Patio Home areas
- Patio Homes also permit single family detached units and all uses permitted in the Cluster Home areas
- Cluster Homes also permit Single Family dwellings, and all uses permitted in Single Family areas, etc.

This is also true for the nonresidential subdistricts where higher intensity subdistricts permit uses in lesser intensity subdistricts. For example: Neighborhood Shopping permits such uses as convenience stores (with gas service), retail stores, day cares, banks, etc. The Neighborhood Shopping subdistrict permits all uses in the Garden Office subdistrict that includes offices, labs, restaurants, etc.

Therefore, Multi-Family is permitted in the Neighborhood Shopping tract subdistrict at a density of 24 units per acres (± 4.47 acres x 24 units/acre equals ± 107 Multi-Family units). Townhomes are permitted on ± 7.02 acres at 10 units/acre totaling ± 70 units. Two Family Garden Homes (Duplex) are permitted on ± 14.97 acres at 6.5 units/acre totaling ± 97 units. The existing zoning can permit up to 274 units.

The Comprehensive Plan, adopted in July of 2020 (*Envision Corinth 2040 Comprehensive Plan linked [here](#)*) identified the subject property as Land Use Place Type - “Mixed Residential.” Mixed Residential provides for a range of residential uses including single family, patio homes, townhomes, and multi-family at an allowable density of six to ten units per acre.

The NexMetro product is categorized as a multi-family use due to multiple units on a lot and the proposed 9/units per acre density is comparable to a townhouse community rather than a multi-family development density.

The existing zoning could be built with more dwelling units than proposed or more intensive nonresidential uses (convenience store with gas pumps, restaurants, etc.). The proposed density of nine units per acre fits within the Mixed Residential Place Type density of six to ten units per acre.

Additionally, the proposed dwellings are one-story rather than two-story.

MIXED RESIDENTIAL

Purpose and intent

- › New residential development that capitalizes on existing natural amenities of the land/property
- › Network of trails with access to the creeks, parks, schools, shopping, and civic destinations
- › Preserve majority of creek/floodplain or open space frontage for public access

Land use types and density

- › Range of single family from large lot, to patio homes, to townhomes and multi-family transitions to neighborhood commercial
- › Recreation related uses with access to parks and open space
- › Overall residential density of 6 to 10 units per acre

Design priorities

- › **Connected streets** (grid-like blocks under 600-ft average block face; max. block face to be 1,000 ft.)
- › Streets should maintain view corridors that focus on open space (which allows for both visual and physical access)

› **Network of connected parks and trails**

- › Architectural design recommendations to ensure quality neighborhoods
- › Transparent fencing alongside parks and open space
- › Access to creeks, open space, and parks
- › Single loaded roads in strategic locations to maximize public access and frontage to creeks and open space

Sustainability priorities

- › Regional or neighborhood scale detention/drainage facilities that serve as amenities with trails, street and development frontages
- › Incorporate trails and sidewalks to maximize walking and biking to parks, schools, and neighborhood retail
- › Design of new streets and infrastructure to incorporate appropriate LID elements
- › Allow roof-top solar panels



Further, it is important to note that “rental options” are a need in any community and the single family for rent product can provide a housing option. A recent Wall Street Journal article dated June 11, 2021, discusses the “economic forces and generational preferences leading to a new kind of housing . . .” such as the single-family for rent product.

B. Transportation, street connectivity, and pedestrian safety:

To address traffic impact concerns identified by the community, the City of Corinth commissioned a Traffic Impact Analysis (TIA) by Lee Engineering, dated June 11, 2021 (see Appendix B). A TIA is not required by the UDC at the time of zoning and one may be required at the time of site plan/platting (subdivision of land). Staff made the decision to order a TIA for this zoning change due to the residents’ concerns.

The collected traffic volumes were adjusted by a factor of 1.09 during the AM peak hour and 1.17 during the PM peak hour to account for the COVID-19 pandemic. Background (non-site) traffic volumes (2019 North Central Texas Council of Governments) for the study area intersections and roadways were estimated by growing the adjusted existing traffic volumes at an annual rate of four percent (4%) until the Build-Out Year (2023). In short, the TIA accounted for growth, COVID-19 pandemic, and growth.

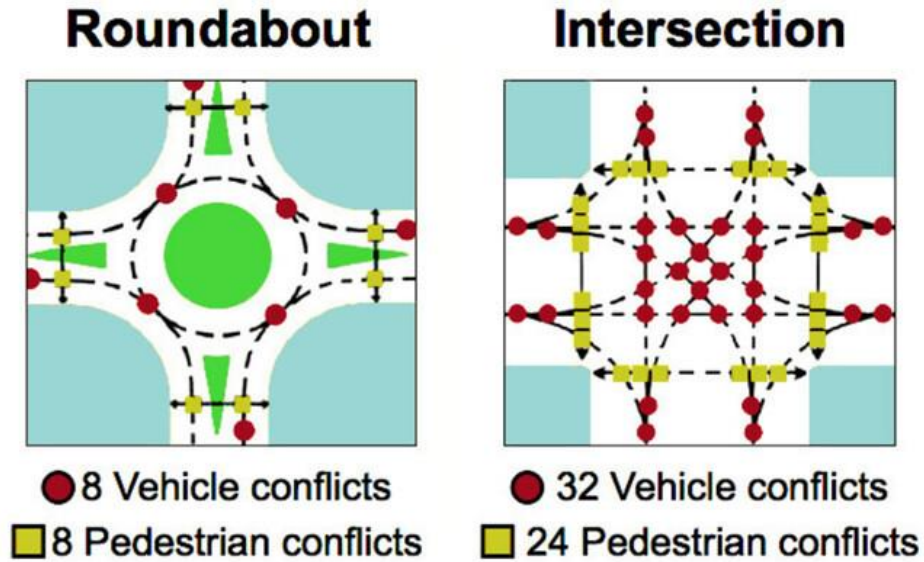
Key conclusions of the Traffic Impact Analysis include the following:

1. The proposed PD Concept Plan provides the best access to the development and existing neighborhoods. Additionally, it provides the least travel delays at all intersections when compared to restrictive access options offered by residents. Resident ideas included:
 - a. Restricting the proposed development to sole access via Lake Sharon Drive and not have a driveway off Oakmont Drive, and
 - b. Restricting traffic to and from Rye Road as either closed off (emergency access only) or only allow for southbound traffic from the northern neighborhood.
2. The existing roadway cross sections are sufficient to handle the proposed development. Lake Sharon Drive and Oakmont Drive were constructed per the City’s Master Thoroughfare Plan. The

Master Plan was developed based on future land use, anticipated development, and traffic flows. The proposed development does not create concern for traffic along either of these two streets.

3. The existing intersection at Oakmont Drive at Lake Sharon Drive will experience an increase in delay in the next few years bringing the overall intersection Level of Service (LOS) D (with a northbound LOS E) if no intersection improvements are made. This level of service condition is with or without the proposed development.
4. The proposed development will not require any right turn deceleration lanes off westbound Lake Sharon Drive or southbound Oakmont Drive.
5. The eastbound direction will require left turn bays to both Rye Road and the western driveway. Median modifications are anticipated and are at the developer's expense.
6. Because Oakmont Drive is a wide 36' curb-to-curb paving section with a centerline stripe, there is sufficient space for the queuing from the school drop-off and pickup to stage on the sides of the road and the through traffic is able to pass by without major issues or delay.
7. When analyzing the nearby intersections, it should be noted that the only intersection that is of major concern as to additional delays would be Lake Sharon Drive at Oakmont Drive.
 - a. Currently the intersection is configured as a single northbound lane– east/west/north, eastbound and westbound three lanes – through/right through and left, and southbound is two lanes - through/right and left.
 - b. Traffic volumes today (2021): Appendix B, Figure 7,
 - c. Traffic volumes buildout (2023): Appendix B, Figure 8
 - d. Traffic volumes proposed site: Appendix B, Figure 13
 - e. Traffic volumes buildout + proposed site: Appendix B, Figure 18
 - i. Buildout was estimated at a 4% increase over 2 years based on historical traffic growth.
 - f. Level of Service (LOS): Appendix B, Table 10 indicates there is a 28.5 second delay (LOS D) at Oakmont Drive & Lake Sharon Drive in the NB direction for today conditions. When you add in buildout that delay increases to 40 seconds (LOS E) and when include the proposed site development that delay increases to 44.7 seconds (LOS E).
 - i. If we were to consider limiting or restricting the access for the proposed development as proposed by others, this delay would increase due to more traffic focusing on this intersection.
 - g. Mitigation measures that would improve this additional delay due to buildout would be to add turn lanes or inclusion of a roundabout. Considering there is sufficient land available to the east of the intersection, a roundabout makes the most sense. A roundabout would shift all directions of the intersection to a LOS A under 10 seconds of delay.
8. The recommendation of a roundabout at Lake Sharon Drive and Oakmont Drive is consistent with the City's Master Throughfare plan. In fact, a roundabout was added to the Throughfare Plan back in 2020 prior to the proposed project application. Roundabouts for Lake Sharon Drive at Parkridge Drive as well as Quail Run Drive at Corinth Parkway/Dobbs Road are already in the design phase. While there are many personal opinions on roundabouts and their efficacy, staff must focus on facts and traffic engineers have proved time and time again that roundabouts are able to flow more traffic efficiently and safer through an intersection than many other types of intersection control.

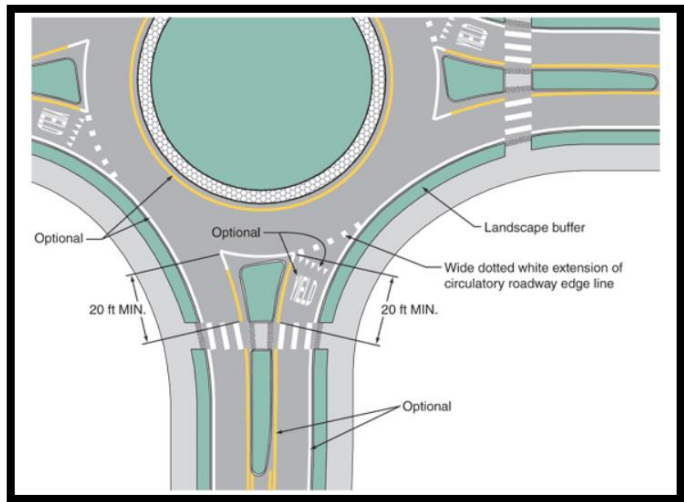
- a. Roundabouts reduce vehicle speeds, minimize vehicle weaving, automatically establish right of way, reduce conflict points from 32 to 8 according to the FHWA Roundabout Guide. The circulatory vehicle movements at roundabouts eliminate or drastically reduce the critical conflicts resulting from red light running, left turns against opposing traffic, right angle conflicts at corners and rear end collisions. As a result, roundabouts significantly reduce vehicular crashes.
 - i. Based on studies, roundabouts reduce vehicular crashes by 39% and injury crashes by about 90%



- b. Pedestrian safety at roundabouts:
 - i. Speed Reduction – entry speeds are reduced due to anticipating curves. Slower speeds at or below 20-mph are safer and enable pedestrians to find gaps in traffic to safely cross and encourage vehicles to yield to them as they step up to the crosswalk.
 - ii. Central Island – a raised central island prevents drivers from seeing all the way through to the other side of the roundabout and encourages drivers to slow down to negotiate the turns. Bringing the driver’s focus back to the near side of the crosswalk.
 - iii. Splitter Islands – Provide pedestrian refuge islands. Therefore, only having to gain clearance from one direction at a time.
 - iv. By including a truck apron in the center of the circular space, a roundabout can accommodate emergency vehicles and large busses and trucks.



Example of a modern roundabout approach



Example of Marking for Approach and Circulatory Roadways at a Roundabout

9. Concern for pedestrian safety as children walk to school. Residents have raised concern for pedestrian safety and if there should be a crossing guard at the intersection of Oakmont Drive at Lake Sharon Drive. Traditionally, a detailed pedestrian count is performed to verify the volume of pedestrians walking during the peak times on their way to and from school. Due to Covid 19, the pedestrian volumes would be much less than historical. Staff can work with the Police Department and Denton ISD to determine if thresholds are met to warrant addition of crossing guards. Due to the nature of the intersection staff anticipates two crossings if the merit study demonstrated the need (one for east/west and one for North/South).

Access Options:

In terms of access, Item 1) above references suggestions made by residents to:

1. Limit access to southbound trips only for Rye Road from the southern boundary of the golf course;
2. Limit access to Rye Road for emergency use only, provide gates/knox lock, provide pedestrian connection; and

- 3. Limit access onto Oakmont Drive for emergency use only.

Staff is unable to support any of the options as they conflict with our Unified Development Code, Subdivision Regulations, and Comprehensive Plan. Additionally, the Traffic Impact Analysis prepared for this application clearly identifies additional stress on street capacity when access options are eliminated.

Also, engineering and planning best practices include the vehicular and pedestrian connectivity in a community. Closing off access or prohibiting connections creates separation, contradicts the goals of the Comprehensive Plan, and opposes the policy of a connected and walkable community.

The following are the Subdivision Regulations from the UDC and general requirements guiding access decision-making in Corinth:

3.05.13.- Streets

E. General Requirements

- 1. Streets must be designed in relation to the Comprehensive Plan/Transportation Plan, existing and proposed streets, the terrain, streams, and other physical conditions.
a. The arrangement of streets must provide for the continuation of streets between adjacent properties when the continuation is necessary for the safe and efficient movement of traffic and for utility efficiency.

F. Cul-de-Sacs and Dead-End Streets

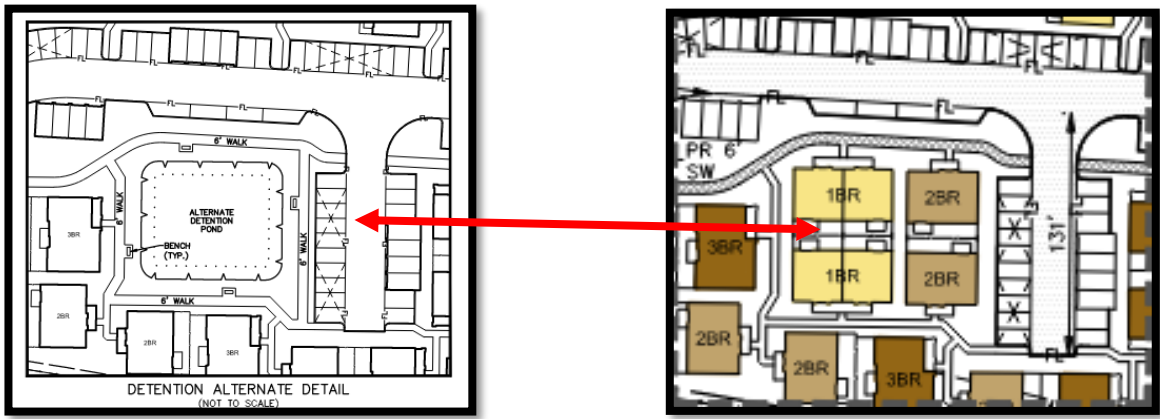
- 1. Cul-de-sacs.
a. A cul-de-sac or dead-end street shall not exceed six hundred (600) feet in length.
b. A cul-de-sac street shall be platted and constructed with a concrete paved cul-de-sac at the closed end having a turnaround with a minimum outside paving diameter of at least one hundred (100) feet and a minimum street Right-of-Way diameter of at least one hundred-twenty (120) feet. (See 3.05.07. B.4 for supplemental information.)
2. Dead-End Streets.
a. Dead-end streets are prohibited unless the street design meets the requirements of paragraph (1) above or unless the street is intended to be extended in the future and the dead-end design is only temporary in nature.
b. If a temporary dead-end street is permitted, turnaround pavement meeting the dimensions listed for cul-de-sacs in Paragraph 3.05.13. F.1 Cul-de-sacs (above) and a temporary turnaround easement meeting the dimensions listed for the Right-of-Way in Paragraph 3.05.13. F.1 shall be provided on the Plat.
i. The portion of the temporary turnaround easement lying outside of the street Right-of-Way shall be shown as a dotted line on the Final Plat which shall denote a temporary easement.

C. Stormwater, Floodplain, and Wetland Management:

The Avilla Fairways development will be required to provide a detailed engineering design of the drainage system prior to release for construction. The Applicant is currently illustrating on the PD Concept Plan an option to detain on site however, if they can prove no negative impact then no detention will be necessary.

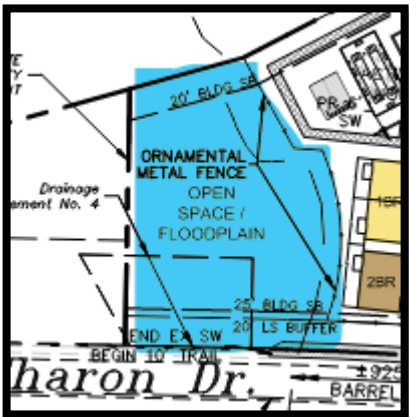
The developer and their engineer will be required to provide a drainage assessment to prove that they have no negative impact upstream, adjacent, and downstream of their development. This assessment will identify the possibility of an increased flowrate, increased water surface elevation, and erosive water velocity. This is where the City will require measures to mitigate impact.

Development drainage issues are usually handled in the form of a detention basin whereby the release of water is limited to pre-existing flowrates. For example, if the site is 10 acres and pre-existing flow is around 27 cubic feet per second (cfs) and the proposed developed rate is 81 cfs then the engineer will need to design a detention basin that would only release 27 cfs and detain the difference during a calculated duration. Thus, limiting the discharge rate to the preexisting flowrate of 27 cfs. Occasionally, this form of detention will possibly be worse than just allowing the site to discharge directly without detention. That will need to be evaluated during detailed engineering design.



Excerpts from PD Concept Plan, 6/22/21 – Location of proposed detention basin (left) in place of four buildings if determined necessary (right)

The latest PD Concept Plan dated June 22, 2021 shows the location of a detention basin option (above left) if it is determined that on-site detention is required. Should a detention basin be required (based on detailed design and associated calculations as required at a later stage in the process), the four buildings shown (right) will be eliminated to accommodate the improvement.



The site does have existing floodplain on it. This floodplain has been modified due to the recent construction of Lake Sharon Drive and the final paperwork with FEMA is still in the process. The proposed development appears to be outside of the floodplain limits (Appendix D-FEMA Floodplain Map). During detailed engineering design, the developers engineer will determine if any modifications are necessary to the area of land in the floodplain. If so, the City will require a detailed flood study and improvements that would create no negative impact to the adjacent landowners. Specifically, the City has a process and procedure for developing in a floodplain and those standards are used on all developments.

The City through the Federal Emergency Management Agency (FEMA) has specific regulations that restrict and allow under specific circumstances development within the floodplain. These regulations

are detailed in the [City's Code of Ordinances, Section 152 Floodplain Damage Prevention](#). The City is the delegated responsibility to adopt and enforce regulations designed to minimize flood losses.

The developer and their engineer have laid out the site to limit the disturbances within the floodplain. Based on the preliminary information provided, the City does not anticipate the development having any physical impact on the floodplain and therefore not require a Floodplain Development Permit. The concept plan clearly shows the floodplain limits and indicates the proposed development, including fences, which are outside those limits.

Where there is a waterbody there is usually wetlands and/or waters of the US (WOTUS). The City is not the regulatory authority for development/construction within wetlands or WOTUS. However, when development is anticipated near such an area, the City requires the design team to confirm and provide proof that there are not any wetlands or WOTUS via an environmental study. The study will be required to be prepared by a professional that uses industry standards for providing a site assessment that meets the federal guidelines. The City is aware of this sensitive area due to the recent construction of Lake Sharon Drive. During that project wetlands or WOTUS were identified and a permit for development was obtained through the US Army Corps of Engineers (USACE). Should this development have any impact on wetlands or WOTUS, the City will require necessary proof through USACE that they have meet federal regulations. Wetlands are usually identified at or below the normal high water mark. Because this development does not show any impact to the floodplain, we do not anticipate any wetland impact as the floodplain elevation is always above the normal high water mark.

As a recap, prior to detailed design approval, the engineering team will require the developer and their engineer to confirm/prove that they are in conformance with all of the codes listed in the Unified Development Code Section 3.05.03.A., as follows (*and linked [here](#) for reference*).

- The Transportation Plan;
- The Drainage Design Manual of the department of public works;
- The Standard Construction Details of the department of public works;
- The Texas Uniform Traffic Control Device Manual;
- North Central Texas Council of Governments Standard Specifications for Construction of Public Works;
- American Association of State Highway Transportation Officials Design Manual;
- Texas Health Code;
- City of Corinth Engineering Standards Manual (ESM);
- Texas Water Code;
- Master Drainage Plans;
- Floodplain Ordinance;
- Erosion Control Ordinance;
- Stormwater Management Plan; and
- All other codes and ordinances of the City.

D. School Attendance

As previously stated, the property has been zoned for high density housing and non-residential development since 1987. School districts regularly contract with demographers to estimate and plan for future growth and attendance within their districts. Denton Independent School District was notified of the zoning change by US Mail in February and again in early June of the upcoming public hearings. On July 1, 2021, a housing analyst consultant for Denton Independent School District

reached out to the Planning and Development Department Staff asking when the construction is anticipated to begin on the development and when the first units will be available. Anticipating rezoning approval, the applicant provided an approximate date of February 2022 for construction to begin and the first home being occupied within seven to eight months after construction begins.

E. Location of Dog Park (near to Oakmont Golf Course)

At the request of residents in Larkspur subdivision, the developer has moved the proposed dog park south of the east/west access drive to provide more separation (APPENDIX A, Concept Plan).

F. Heavily Treed Site

The site is subject to a prior settlement agreement (*Endeavor Agreement 2017*) that specifically controls the Tree Preservation regulations as applied to any development of the ±24.595-acre project site. Specifically, the agreement notes that the site is considered “heavily wooded lot” allowing a 50% reduction in the amount of replacement trees required by Section 2.09.02.B3 of UDC in areas other than building pads plus 5’ from edge of building pad, street-right-of-way, utility easements, driveways, and are considered exempted from Protected Tree regulations.

II. Background:

A. **Project Overview.** The Applicant is requesting approval of a PD, Planned Development rezoning for the future development of ±24.59 acres located on the northwest corner of Lake Sharon Drive and Oakmont Drive. The proposal is to construct a 215-unit multi-family residential cottage community that consists of the following unit types (also *see Attachment 1 – PD Concept Plan*):

- 209 individual one-story cottage buildings and 23 detached garages
- Internal pedestrian sidewalk/path network
- Private recreation amenities including event lawns, a pool and spa, and dog park

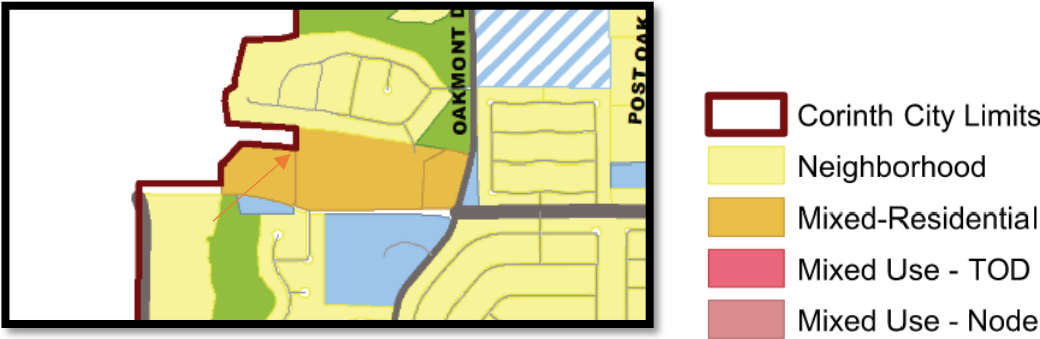
B. **Existing Site Conditions.** The existing site is currently undeveloped, wooded, and contains a small area of floodplain along Bryant Branch on its western boundary with the City of Denton.

- The site is bounded by Lake Sharon Drive to the south, Oakmont Drive to the east, and the Oakmont golf course to the north and west.



Source: [Corinth GIS](#):

- A recorded utility easement (Denton Municipal Electric) traverses the site in a general north south direction permitting the area to be used for drainage, streets, walks, gardens, parking, and other similar uses.
 - The site is subject to a prior settlement agreement (*Endeavor Agreement 2017*) that specifically controls the Tree Preservation regulations as applied to any development of the ±24.595-acre project site. Specifically, the agreement notes that the site is considered “heavily wooded lot” allowing a 50% reduction in the amount of replacement trees required by Section 2.09.02.B3 of UDC in areas other than building pads plus 5’ from edge of building pad (includes gas well pad site), street-right-of-way, utility easement, or driveways and are considered under the agreement exempted from Protected Tree regulations.
- C. **Existing Site Zoning.** The project site is currently controlled by two zoning designations: PD-24 (±5.7 acres located on the western end of the site (*approved 12-16-1999*)) and PD-6 (± 18.895 acres located on the remaining portion of the tract (*approved 12-17-1987*)). See Exhibit A, below.
- PD-6 zoning provides for the development a mix of residential uses including townhomes, single-family attached garden homes (*ranging in density from 6.5 to 10 dwelling units per acre*), and neighborhood shopping on ±5 acres at the northwest corner of Lake Sharon and Oakmont Drive as uses “by right.”
 - PD-24 provides for two family garden homes (*attached single family dwellings and patio homes as uses “by right” with density of 6.5 dwellings per acre*).
- D. **Future Land Use.** The Comprehensive Plan identifies this area as Mixed Residential which provides for a variety of dwelling types ranging in density from 6-10 dwelling units which may include single family, townhouse, multifamily, and neighborhood commercial uses.



Source: *Envision Corinth 2040 Comprehensive Plan (Adopted July 2020)*



Source: Envision Corinth 2040 Comprehensive Plan (Adopted July 2020)

In terms of transportation, the site is served according to the City’s adopted Thoroughfare Plan with a Minor Arterial (Lake Sharon Drive) and a Collector (Oakmont Drive). The Thoroughfare Plan also provides for a future Roundabout at the intersection of Lake Sharon Drive and Oakmont Drive.

Additionally, the Comprehensive Plan identifies a six (6’) – ten (10’) foot “Sidepath Trail” along Lake Sharon Drive. See excerpt from the Active Transportation Plan below:



Sidepath on Lake Sharon Drive
Source: Envision Corinth 2040 Comprehensive Plan (Adopted July 2020)

E. **Project Design Features for Consideration.** The following key points represent specific departures or design modifications from the regulations outlined in Unified Development Code (UDC) to permit the unique design as presented in *Appendix A- 1 - PD Concept Plan* and other associated attachments as presented in “Supporting Documents” contained in Appendix A. Additionally, each modification standard presented contains a “**justification**” statement for the departure.

1. **Project Proposal/Density.** NexMetro is proposing to rezone the ±24.595 acres using a Planned Development process to construct a unique product type consisting of 215 dwelling units within 209 one-story cottage buildings which equates to approximately 9.0 dwelling units per acre broken down as follows:
 - 30 Duplex Cottages (1-bedroom units (*totaling 60 1-bedroom units*))
 - 87 Cottages (2-bedroom units)
 - 68 Cottages (3-bedroom units)

2. **Specific Uses.** UDC Section 2.07.05.A shall be modified to permit all proposed structures (leasing center, dwelling units, garages, and recreational amenities (pool, spa, dog park, grills, fire pits,

pergolas, event lawns, etc. (as further detailed on Attachment 3-PD Landscape Plan) to be permitted on one (1) or two (2) lot/s. Further, Section 2.07.07, shall be modified where necessary to meet the intent of the layout of accessory buildings and uses with respect to location, size, and number of detached garages based on the concept presented in Appendix A-1-PD Concept Plan.

Justification: To permit flexibility and innovation of design and allow for individual one-story primarily detached cottage style buildings to be arranged (maintaining a “single-family detached” type appearance) on a single lot.

3. **Dimensional Regulations/Site Data Table.** UDC Section 2.08.04 shall apply, except as modified from the base zoning district of MF-1 to allow for a **reduction in floor area** from a minimum floor area of 1,050 sf per dwelling unit to a minimum floor area of 680 sf per dwelling unit for the proposed one-bedroom units (attached cottage (duplex) buildings) and a minimum floor area of 1,022 sf per dwelling unit for the two-bedroom units as indicated on Attachment 1-PD Concept Plan (floor area values represent “slab size” floor area). Further, all units throughout the complex shall have private backyard areas (*which is not reflected in the minimum floor area*) as depicted in Appendix A-1 - PD Concept Plan. Table 2 presents the “Site Date Summary” for the overall project.

Table 1 – Dimensional Requirements (Base Zoning and Proposed PD requirements)

ZONING COMPARISON TABLE		
	BASE MF-1	PROP. PD
MINIMUM SETBACKS		
FRONT YARD	30'	25'
SIDE YARD		
INTERIOR LOT	30'	N/A
CORNER LOT	30'	N/A
REAR YARD	30'	20'
GARAGE	N/A	N/A
MINIMUM LOT DIMENSIONS		
LOT AREA (SF)	40,000	40,000
LOT WIDTH	200'	200'
LOT DEPTH	200'	200'
DENSITY (MAX, DU/A)	12	9
MINIMUM FLOOR AREA (SF PER DU)	1,050	680
MAX. BUILDING HT. (FT/STORIES)	35' / 2	30' / 1
MAX. BUILDING AREA (TOTAL)	40%	23%

Table 2 – Site Data Summary

SITE DATA SUMMARY TABLE				
EXISTING ZONING/USE:	PD-6, PD-24 / VACANT			
PROPOSED ZONING:	PD W/MF-1 BASE ZONING			
PROPOSED USE:	RESIDENTIAL			
	COTTAGE COMMUNITY			
NUMBER OF LOTS:	2 (EXISTING); 2 (PROPOSED)			
LOT COVERAGE:	22.8%			
LOT AREA:	AC.	SQ. FT.		
GROSS AREA	24.595	1,071,370		
ROW DEDICATION	0.698	30,389		
NET AREA	23.898	1,040,981		
DENSITY:	9.00 UNITS / ACRE (w/o ROW Ded.)			
PROPOSED BUILDINGS (1 STORY):	AREA*	NO. BLDGS	NO. UNITS	TOTAL
1-BEDROOM (DUPEX)	1,342	30	60	40,260
2-BEDROOM	1,022	87	87	88,914
3-BEDROOM	1,307	68	68	88,876
STANDARD GARAGE, DETACHED*	831	20	-	16,620
STANDARD GARAGE W/HANDICAP, DETACHED	682	3	-	2,046
LEASING CENTER	1,131	1	-	1,131
TOTALS:		209	215	237,847
PARKING REQUIRED:	UNITS	REQUIRED	PROPOSED	
1 BEDROOM (2 SPACES REQ'D; 2 SPACES PROP.)	60	120	120	
2 BEDROOM (3 SPACES REQ'D; 2 SPACES PROP.)	87	261	174	
3 BEDROOM (4 SPACES REQ'D; 2 SPACES PROP.)	68	272	136	
TOTALS:	215	653	430	
PARKING PROVIDED:	PROVIDED			
STANDARD SPACES	234			
CARPORT SPACES	144			
HANDICAP SPACES	3			
STANDARD GARAGE SPACES	87			
HANDICAP GARAGE SPACES	3			
TOTALS:	471			
GARAGE SPACES PER UNIT:	41.9%			
OPEN SPACE AREA REQUIRED:	83,278	8%	(SF, %)	
OPEN SPACE AREA PROVIDED:				
EVENT LAWN	2,773	0.3%		
PASSIVE NODES	7,024	0.7%		
ACTIVITY NODES	16,802	1.6%		
DOG PARK	11,968	1.1%		
MISC. OPEN SPACE	142,338	13.7%		
TOTALS:	180,905	17.4%	(SF, %)	
LANDSCAPE AREA:	500,032	48.0%	(SF, %)	
TRAIL DEDICATION:	WIDTH, FT	LENGTH, FT	AREA, SF	
LAKE SHARON DRIVE	10	2,189	21,890	
OAKMONT DRIVE	6	1,017	6,102	
TOTALS:	-	3,206	27,992	
IMPERVIOUS AREA:	540,949	52.0%	(SF, %)	
LOADING SPACE:	NOT REQUIRED			
OUTSIDE STORAGE:	NONE PROPOSED			
CONSTRUCTION DATES:	Q1 2021 - Q3 2021			

* THESE VALUES REPRESENT "SLAB SIZE" FLOOR AREA
 + ONE STANDARD GARAGE WILL USE TWO OF FOUR BAYS FOR ON-SITE MAINTENANCE PURPOSES

Justification: The Applicant indicates that the reduction in minimum floor area is necessary to align more with industry standards. At the time that the UDC Ordinance was written, it did not contemplate this style of development or product type and specifically states that the Planned Development process found in UDC Section 2.06.03 is to provide the City a method to consider new and innovative concepts that may require flexibility in regulations.

- 4. Parking Regulations.** UDC Section 2.09.03. Vehicular Parking Regulations shall apply, except that the space per unit as applied to the apartments may be reduced to a minimum of 2.0 spaces per unit.

Justification: Documentation provided by NexMetro (*see Appendix A-5 - Parking Demand Study*), suggests that the City’s existing parking requirements are higher than data derived from an independent parking study of existing NexMetro developments and reflect actual parking demands on site.

Specifically, the study indicates that the product yields a parking space demand of 0.75 spaces per bedroom, which equates to 1.85 spaces per unit in a 1, 2, and 3 bedroom unit mix. However, the Applicant has revised this initial figure to provide 2.0 space per unit and afford a greater amount of parking on site. Further, the UDC was adopted in 2013 and since that time the multi-family regulations (including associated parking requirements) have not been updated to address trends and market needs. Recently constructed Corinth multi-family developments have provided for parking ratios less than the proposed ratio for this PD. The PD process is the appropriate tool to achieve zoning to meet current market requirements for multifamily parking and the implement the City’s adopted vision in the Comprehensive Plan.

Finally, the *Institute of Traffic Engineer’s Parking Generation Manual, 5th Edition* recommends a standard of 1.7 spaces per unit and/or 1 space per bedroom. Based on the ITE parking standards, the Avilla Fairways’ parking calculation to serve the development would be 438 spaces. The proposed Concept Plan provides 471 spaces.

- 5. **Garages.** UDC Section 2.04.07.C.5 requirement that “a minimum of seventy-five (75) percent of all apartments shall have a one-car enclosed garage, two hundred forty (240) square feet minimum, attached or detached, per dwelling unit”, shall be modified as follows: Garages and covered parking to be allowed per the Attachment 1 – PD Concept Plan and will provide for an overall minimum of two (2.0) parking spaces per unit, including surface, covered, and garage parking spaces. NexMetro will assign either a covered space or garage space with every unit, ensuring a 100% covered parking per unit ratio across the site. These parking numbers are exclusive of any parking that may occur on Rye Road.

Justification: The Applicant’s indicates that the ratio proposed (when referencing other NexMetro developments across the Dallas-Fort Worth metroplex), ranks near the top of all projects to date in terms of parking ratios.

- 6. **Residential Adjacency Standards.** UDC Section 2.09.06. shall not apply to the project site as the Golf Course property located adjacent to the northern property boundary of the subject site is not residentially zoned. PD-6 indicates that area as specific sub-area for Golf Course (nonresidential use) and the adjacency standards are not required. Additionally, a twenty-five (25’) foot front yard setback is being proposed along the major roadways Lake Sharon Drive and Oakmont Drive as well as a twenty (20’) foot building setback along all other boundaries.

Justification: The Applicant is offering a design that goes beyond the UDC requirements to be more in line with Corinth’s single family zoning regulations and are more applicable to a single-story product.

- 7. **Landscaping Regulations.** UDC Section 2.09.01 Residential landscaping requirements shall apply and to be subject to the following modifications:
 - a. Landscape shrub plantings shall be used to soften the view of wood fencing around backyard areas when viewed from Lake Sharon Drive and/or Oakmont Drive.
 - b. Where wainscotting (with cementitious fiberboard) is proposed on exterior side walls only that are visible from a public street, this feature shall be either continuous and/or supplemented with continuous foundation plantings. See Attachment 3- PD Landscape Plan.

Previous standards (above) where hardie board (cementitious fiberboard) was contemplated has been replaced with the following:

Exterior side walls that are visible from a public street shall provide for a continuous row of foundation plantings/shrubs.

- (a) Lake Sharon Drive: refer to Avilla Fairways Conceptual Landscape Plan, sheet LS2 (detail 2, section B), and (b) Oakmont Drive: refer to Avilla Fairways Conceptual Landscape Plan, sheet LS2 (detail 1, section A).
- c. Opaque fencing for rear yards shall be allowed for privacy where rear yards are visible from Lake Sharon Rive or Oakmont Drive. All other perimeter fencing shall be ornamental metal with adaptive screening. Double fencing concepts are not desirable, and efforts will be made to reduce conditions that would require such. Additionally, significant interior landscaping shall be required to achieve a level of opacity that sufficiently affords a living screen and privacy. Specific treatments will be further evaluated and defined at time of Site Plan based on best design practices.
 - d. Streetscapes for Lake Sharon and Oakmont drive shall refer to Avilla Fairways Conceptual Landscape Plan as follows: (a) Lake Sharon Drive: refer to sheets LS2 (detail B) and LS3 (detail 5), and Oakmont Drive: refer to sheets LS2 (detail A) and LS3 (details 4,5,&7).
 - e. Landscape edge buffers along Lake Sharon Drive and Oakmont Drive shall be planted per Avilla Fairways Conceptual Plan (refer to sheets LS1 and LS2) as shown on Attachment 3-PD Landscape Plan and be according to the following conditions:
 - i. Shade trees shall be planted at a rate of one (1) per 30 linear of feet of landscaped edge and include at least one (1) ornamental tree provided at a rate of one (1) per every two required shade trees.
 - ii. Trees may be clustered or located to accommodate driveway spacing, utilities, drainage facilities, trails, and similar site features, provided that a visual rhythm is maintained.
 - iii. Further, evergreen shrubs shall be included along the fencing and planted at varying intervals (which includes changes in height) to provide vistas into the development and buffered edges (e.g., variations of four feet minimum and 6-8 feet in height).
 - f. A continuous evergreen hedgerow a minimum of 4 feet in height shall be provided (along drives, driveways, and perimeter parking areas) where necessary to reduce impact from vehicle headlights.
 - g. Along the north and west property line, a barrier will be established, and no disturbance shall occur within the drip line and/or critical root line of any tree located adjacent to the property line that extends into Avilla Fairways site. Any tree that dies along the adjacent property line within 2 years of site disturbance shall be replaced/fee-in-lieu-of applied at a rate of 3:1 caliper inch lost.
8. **Private Recreational Areas.** UDC Section 2.04.09.C.8 shall apply, where a minimum of 8% of the gross complex is required to be in the form of private recreation. Note that the requirements of this section, are in addition to the park dedication requirements within 3.05.10. Park and Trail Dedications for Residentially Zoned Property. To meet the Private Recreation requirements, the applicant is providing 2.29 acres or 9.6 % of the Net Acreage in the form of Private Recreation areas which includes all activity nodes, large open space areas, pool/event lawn, and the dog park as shown and detail in Attachment 3-PD Landscape Plan.
9. **Park and Trail Land Dedication.** UDC Section 3.05.10 requires that Park and Trail dedication for Residentially Zoned Property to be provided at a rate of 1 acre per/50 DU and/or fees-in-lieu-of (or combination). Because the PD Concept Plan identifies the construction of a “Sidepath

Trail” as shown on the Active Transportation Plan in the Envision Corinth 2040: Comprehensive Plan, the area required for the “Sidepath Trail” construction may be used to “satisfy” 0.5 acres of the required by UDC Section 3.05.10 Park and Trail Dedications for Residentially Zoned Property for this project site provided the following conditions are met:

- a. The developer shall construct a ten (10’) foot concrete trail along Lake Sharon Drive in accordance with ADA standards. A pedestrian public access easement shall be provided should the final design of the trail may meander outside of the public right-of-way and into the required 20’ landscaped buffer edge.

Justification: Trails are required as part implementing the Comprehensive Plan, Mobility Plan objectives.

- 10. **Rye Road (extension).** The Applicant is dedicating right-of-way and constructing the Rye Road (extension) from its current terminus at the northern property boundary through the subject site to connect with Lake Sharon Drive. Rye Road construction will include five (5’) foot sidewalks on both the east and west sides of the street.

Justification: This connection provides an important second point of access to the Larkspur at Oakmont neighborhood (north of the subject site) for fire and safety purposes. The exiting stubbed out section of Rye Road was installed to provide for future connection when property to the subject site eventually developed to provide for additional access and interconnection of street system.

- 11. **Roundabout at Lake Sharon Drive/Oakmont Drive.** The Applicant shows a detail of the City’s proposed future roundabout at the intersection of Lake Sharon and Oakmont Drive on the PD Concept Plan as identified on Master Thoroughfare Plan (*Envision Corinth 2040 Comprehensive Plan*). The roundabout is not a part of the Avilla Fairways project.

Justification: These roadway improvements are shown on the “Master Thoroughfare Plan” as part of the Envision Corinth 2040 Comprehensive Plan (*adopted July 2020*).

- 12. **Mechanical Equipment and Screening of Outdoor Waste Storage.** UDC Sections 2.04.07, C.6 and 4.02.13 shall apply, with the additional stipulation that dumpster enclosures will be masonry (or similarly acceptable material) and match the materials of the adjacent dwellings. Further, enclosures shall contain landscape foundation plantings to soften the view and enclosed on all four sides as depicted on the Attachment 1-PD Landscape Concept Plan. Additionally, mechanicals shall be screened with evergreen plant material to create an opaque boarder screen. Screening standards may be adjusted at time of site plan review based on best practices.

- 13. **Tree Preservation.** UDC Section 2.09.02 Tree Preservation regulations shall apply, except as provided for in the Endeavor Settlement Agreement (2017).

- 14. **Building Materials.** UDC Section 2.09.04 Building Façade Material Standards shall apply as defined in the outlined in below.

- a. Exterior finishes of buildings shall consist of 80% masonry where masonry is considered brick and/or stone and in no case shall any exterior wall contain less than seventy-five (75) percent of the brick and/or stone materials.
- b. All facades that face the northern property, along the golf course boundary shall consist of one hundred (100) percent brick and/or stone materials.
- c. A minimum of three distinct elevations will be provided per residential home floor plan with differing roof pitches.

Justification: Per the June 28, 2021, Planning and Zoning Commission Meeting, a motion to recommend approval to City Council, with the condition that exterior finishes of buildings shall consist of 100% masonry and masonry is considered brick and/or stone. Prior to the June 28, 2021, Planning and Zoning Commission motion, the design standard proposed was for a minimum of 80% of exterior finishes of buildings to include masonry where masonry included, brick, stone, hardie board, and stucco).

- d. Staff recommends the exterior masonry requirement originally proposed at 80% and excluding hardie board/cementitious fiberboard from the minimum percentage. Additionally, exterior facades that face the northern property, along the golf course boundary shall consist of one hundred (100) percent brick and/or stone materials. The ability to provide other materials will allow for more character and variety on the elevations as well as provide for alternative materials where brick and stone are structurally challenging or not feasible. The use of metal for awnings, hardie board on gable dormers, shed dormers, columns, porch rails, and other architectural elements is common and recommended. Additionally, no exterior wall shall contain less than seventy-five (75) percent brick and/or stone materials. Garage doors are exempt from the brick and/or stone masonry requirement.

For comparison, the PD to the north, Larkspur, requires a 75% masonry, stucco, or glass building materials for exterior walls and each individual exterior elevation may be no less than 50% masonry as described masonry materials.

- e. Roof Line. Where visible along the exterior corridors (Lake Sharon Drive and Oakmont Drive), roof pitches shall have a minimum of 8/12 pitch with a mixture ranging from 8/12, 10/12 and 12/12 to provide visual interest. Specifically, no more than two adjacent dwellings with the same roof pitch may be permitted along the corridors. Additionally, dormers, moldings, and other architectural features are required to provide character break up monotony of unit density.
- f. Garage doors shall be designed with architectural elements such as dentil moldings, windows, raised panels, etc., while materials shall match dwellings. Garage rooflines may have variation in roofline and pitch. This design detail will be determined at time of Site Plan.

15. **Lighting and Glare Regulations.** UDC Section 2.09.07. shall apply, and as determined at time of Site Plan, pedestrian scale lighting fixtures shall be considered where practical.

16. **Sign Regulations.** UDC Subsection 4.01 sign regulations shall apply, however, if necessary, standards may be modified as shown on the PD Concept Plan to achieve the “signage and monumentation concepts” provided that visibility and setbacks requirements are met to ensure safety. Further the portal signs will be located during the site plan/construction plan submission.

- a. Portal signs to be located within landscape islands designating groupings of units as shown on the Avilla Fairways Conceptual Landscape Plan, sheet LS3.
- b. Three (3) total monument signs proposed for this development, one (1) sign at each location:
 - i. Lake Sharon Drive driveway connection
 - ii. Oakmont Drive driveway connection
 - iii. Rye Road and Lake Sharon intersection

17. **Fence and Screening Regulations.** UDC Section 4.02. shall apply, and include the installation of fencing and screening as further outlined below to meet intent of the PD Concept Plan and PD Conceptual Landscape Plan (*see Attachments 1 and 3*):

- a. Lake Sharon Drive: Six (6') foot ornamental fencing with masonry columns every 30 linear feet, along with interior landscaping shall be provided; refer to sheets LS1, LS2, & LS3 in Attachment 3 – Landscape Plan.
- i. Oakmont Drive: Six (6') foot Board-on-Board Wood fence with exterior landscape adjacent to Oakmont Drive Ornamental located along the Golf Course is not intended to have masonry columns.
- b. Board-on-Board fence per sheets LS1, LS2, & LS3 shall be provided for a dwelling unit backyard adjacent to street frontages, otherwise six (6') Ornamental fence per sheets LS1 & LS2 shall be provided.
- c. Golf Course: Six (6') foot ornamental fence per sheets LS1, LS2 & LS3 shall be provided.
- d. Other Misc. Fence and Screening Standards:
 - ii. have masonry columns every 30 linear feet.
 - iii. In instances where rear or side yard wood fencing is visible or fronts Lake Sharon Drive and Oakmont Drive corridors, such fencing shall be provided with a “cap” to ensure finished appearance along corridors.
 - iv. Dumpster enclosures shall be screened with landscape foundation plantings and be of masonry material which shall match the material of adjacent dwelling units.

18. **UDC Section 2.10.09.D. Planned Development (PD) Modifications and Amendments** shall apply, however, d. (density is capped at 215 dwelling units), f. (minimum floor area may not be decreased from what is represented in **Table 1 – Dimensional Requirements (Base Zoning and Proposed PD requirements) and Table 2 – Site Data Summary**), and i. (building materials) will not be varied from the regulations outlined in this ordinance through a modification and/or amendment.

19. **Other.**

- a. Cottage community building separation minimum to be 8-feet (foundation to foundation). All resident units to be fire sprinkled per NFPA-13D requirements.
- b. Cottage community to be 1 story max height residential buildings.

III. Prior Action:

- A. **June 28, 28, 2021** – Planning and Zoning Commission held a public hearing and recommended an action of approval with condition that masonry exterior building finishes be enhanced to include 100% brick and/or stone.
- B. **February 22, 2021** – Planning and Zoning Commission held a public hearing and recommended an action of denial to City Council.
- C. **March 18, 2021** – City Council remanded Avilla Fairways Planned Development back to the Planning and Zoning Commission for further consideration.

IV. Supporting Documents:

APPENDIX A – ATTACHMENTS (*Applicant Documents*):

Attachment 1 - PD Concept Plan (*dated 6/22/21*)

Attachment 2 – PD Illustrative Plan (*dated 6/23/21*)

Attachment 3 – PD Conceptual Landscape Plan (*dated 6/23/21*)

Attachment 4 – Elevation Exhibits (*dated 7/8/21 – revised per 6/28/21 Planning and Zoning Commission Comments*)

Attachment 5 – Parking Demand Study

APPENDIX B – TRAFFIC IMPACT ANALYSIS (*Traffic Impact Analysis Commissioned by the City of Corinth, dated June 11, 2021*):

APPENDIX C – LETTERS

Attachment C1 – Letters from Property Owners within 200 feet of Proposed Rezoning
Attachment C2 – Letters from General Public

APPENDIX D – FLOODPLAIN MAP

APPENDIX E – 200-FOOT NOTIFICATION BOUNDARY EXHIBIT

APPENDIX F – Draft PD-59 Ordinance (dated 7-11-21)

V. Neighborhood Meetings:

As provided for in Section 2.10.09., of the UDC, the Applicant of a Planned Development rezoning is strongly encouraged (*though not required by City ordinance*) to conduct a neighborhood meeting with homeowners within the vicinity of the rezoning request.

To understand resident concerns, the developer, NexMetro conducted a series of three neighborhood meetings over the past several months to explain the proposed project, seek input, and seek resolve concerns where practicable.

Neighborhood meetings were held on January 27, 2021, April 6, 2021, and June 23, 2021. The initial meeting was held at City Hall with remote access also available via Zoom with approximately 25 attendees.

The April 6, 2021, meeting was held with the Larkspur neighborhood with approximately 10 attendees, and the June 23, 2021, Zoom Meeting included approximately 50 attendees from Lake Sharon Estates, Larkspur, and greater Oakmont area.

Additionally, on March 23, 2021, the Oakmont area residents held a meeting at the Hawk Elementary parking lot. City Staff was in attendance. The Applicant was not present.

VI. Compliance with the Comprehensive Plan:

The rezoning request for the subject property, as presented, is in accordance with the “Land Use and Development Strategy” designations, Mixed-Residential, and the concepts outlined in the Master Thoroughfare Plan and Active Transportation Plan as set forth in the "Envision Corinth 2040" Comprehensive Plan.

VII. Public Notices:

Notice of the public hearing was published in the June 12, 2021, edition of the Denton Record-Chronicle. Written public notices were mailed to all property owners located within 200’ of the subject property proposed for the zoning change on June 11, 2021.

At the time of packet preparation (*July 9, 2021, at 11:00 AM*), we have received eight emails (seven property owners) in opposition to the request from property owners located within the 200’ written notice boundary.

Emails to Planning & Development staff from residents located outside of the 200' written boundary from the general public as follows:

- 85 General Public Emails for February 22, 2021 P&Z Commission meeting
- 29 General Public Emails for June 28, 2021 P&Z Commission meeting
- 10 General Public Emails for July 15, 2021 City Council meeting

Reference Appendix C – Letters and Appendix E - 200' Notification Boundary area exhibit.

VIII. Planning Commission Recommendation:

At the June 28, 2021, Planning and Zoning Commission Meeting, the Commission voted 4-1 to recommend approval as presented with the condition that the building elevation materials be enhanced to include 100% stone and brick materials and excluding hardie board/cementitious fiber board from the required percentage.

Staff Recommendation/Motion

The concerns raised at the initial February 22, 2021, Planning and Zoning Commission Public Hearing, the subsequent resident and homeowner meetings, and at the June 28, 2021, Planning and Zoning Commission Public Hearing, have been addressed in detail in this staff report. The application remains generally the same with supporting documentation and questions being addressed. The proposed use is suitable at this location.

The application as presented complies with the Comprehensive Plan, the proposed uses are less intense than what existing zoning would allow by right, and the existing transportation infrastructure is satisfactory. Further, the development will be required to comply with the City's development regulations.

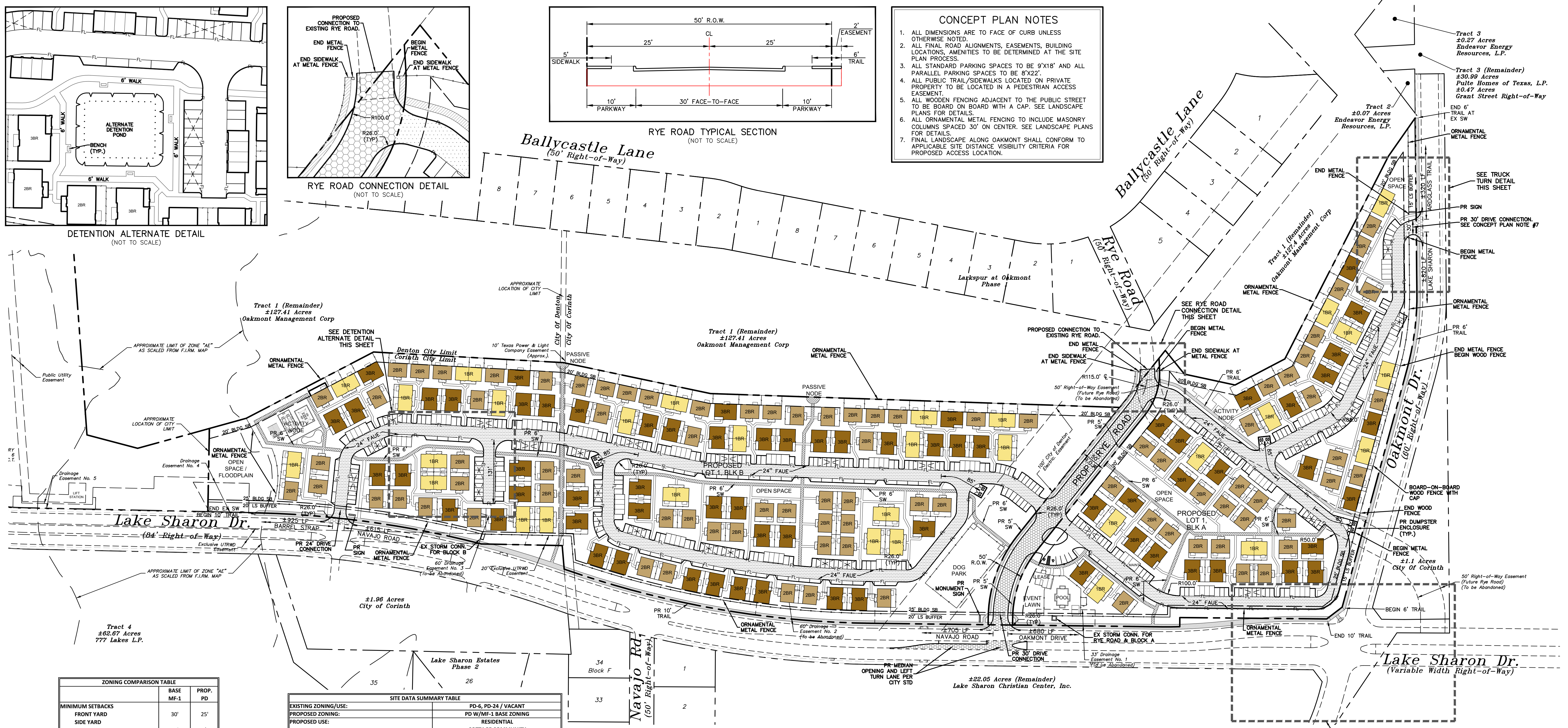
Staff recommends approval as presented.

APPENDIX A

SUPPORTING DOCUMENTS

ATTACHMENTS:

1. PD Concept Plan (*revised date 6/22/21*)
2. PD Illustrative Plan (*revised date 6/23/21*)
3. PD Conceptual Landscape Plan (*revised date 6/23/21*)
4. Elevation Exhibits
5. Parking Demand Study

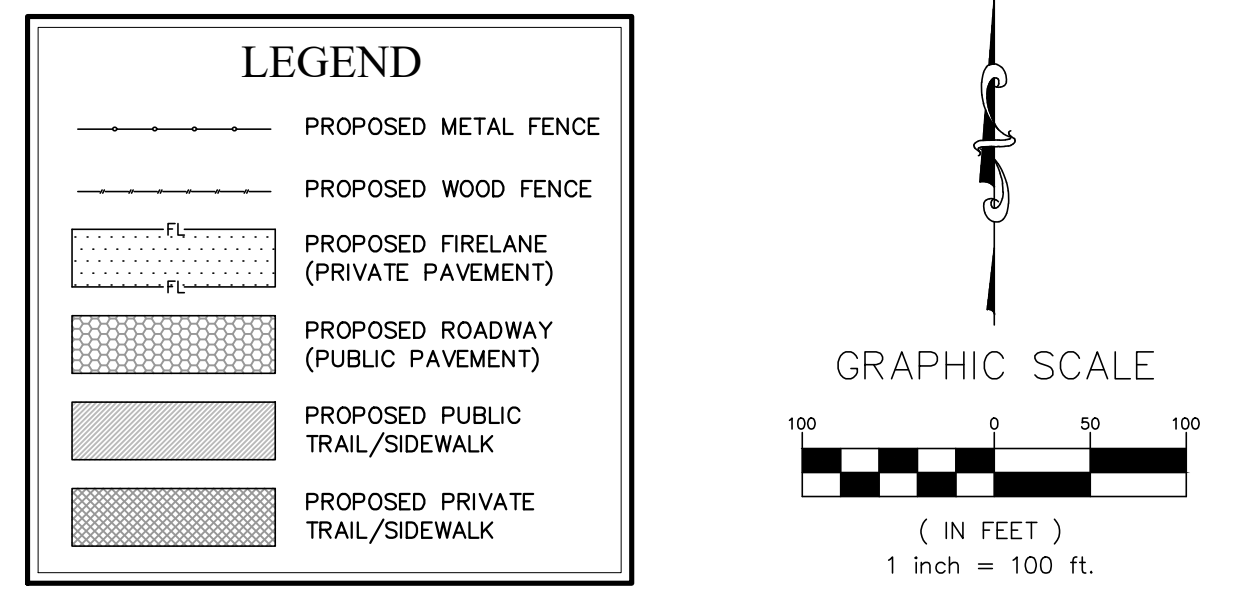
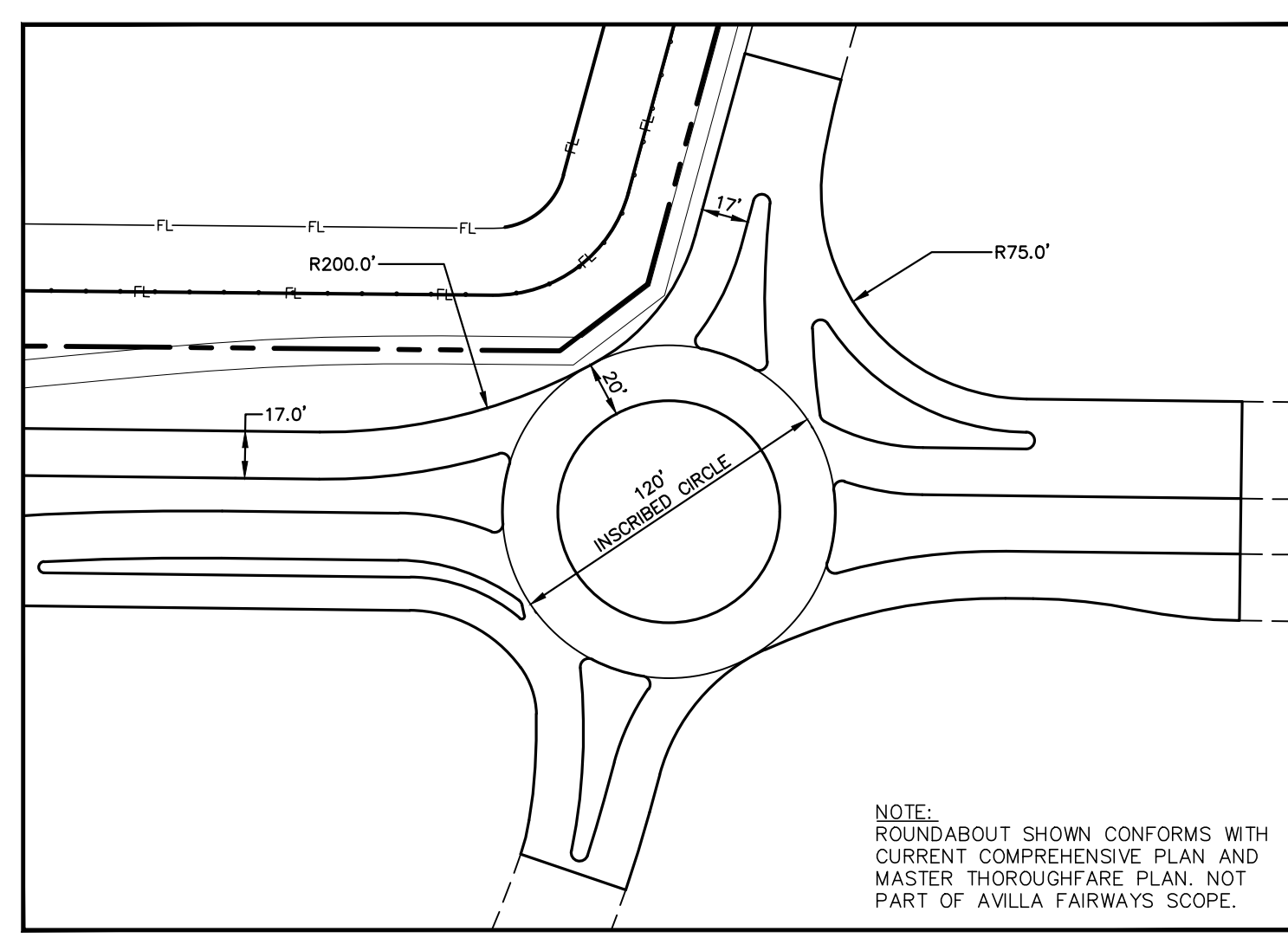
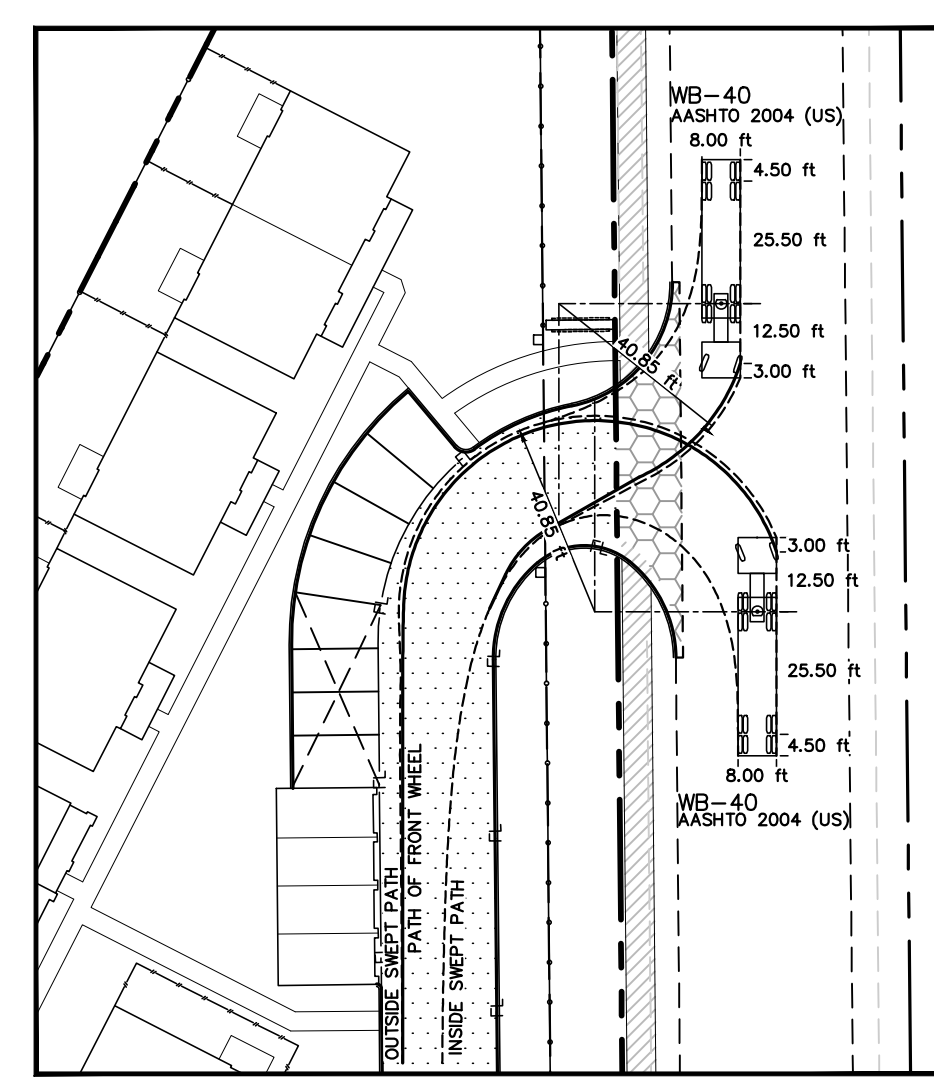
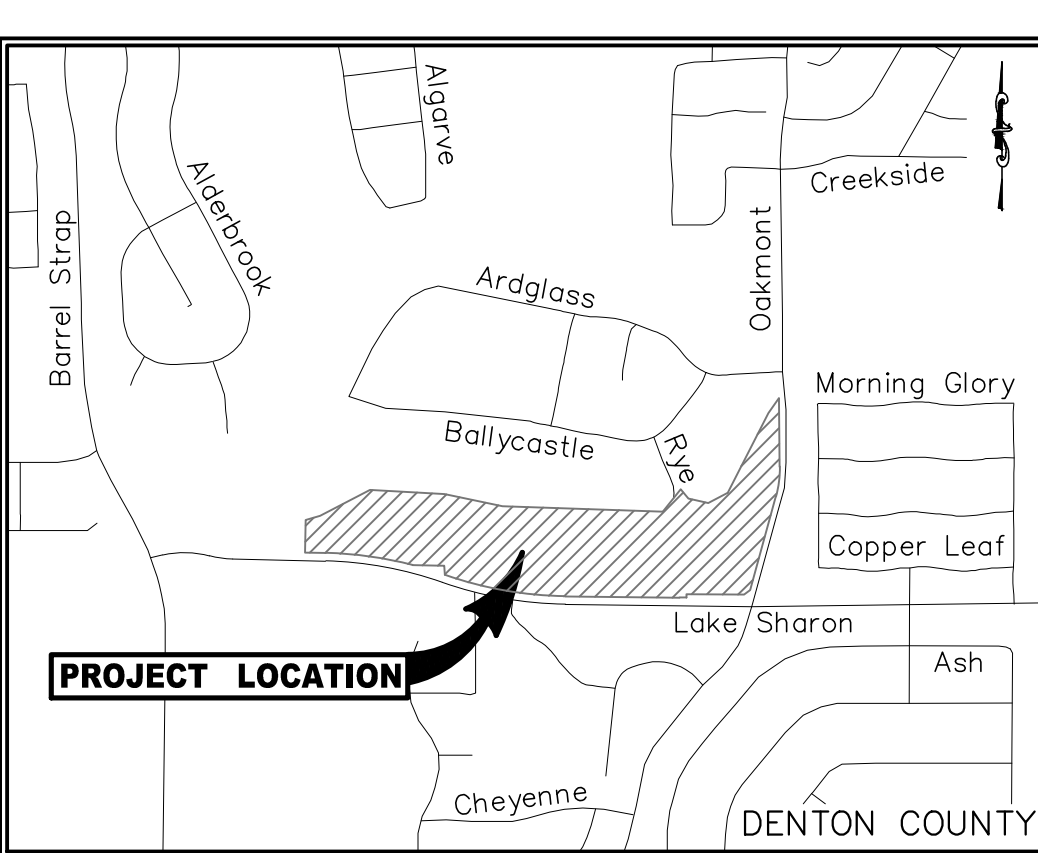


ZONING COMPARISON TABLE

	BASE MF-1	PROP. PD
MINIMUM SETBACKS		
FRONT YARD	30'	25'
SIDE YARD		
INTERIOR LOT	30'	N/A
CORNER LOT	30'	N/A
REAR YARD	30'	20'
GARAGE	N/A	N/A
MINIMUM LOT DIMENSIONS		
LOT AREA (SF)	40,000	40,000
LOT WIDTH	200'	200'
LOT DEPTH	200'	200'
DENSITY (MAX, DU/A)	12	9
MINIMUM FLOOR AREA (SF PER DU)	1,050	680
MAX. BUILDING HT. (FT/STORIES)	35' / 2	30' / 1
MAX. BUILDING AREA (TOTAL)	40%	23%

SITE DATA SUMMARY TABLE

EXISTING ZONING/USE:	PD-6, PD-24 / VACANT			
PROPOSED ZONING:	PD W/MF-1 BASE ZONING			
PROPOSED USE:	COTTAGE COMMUNITY			
NUMBER OF LOTS:	2 (EXISTING); 2 (PROPOSED)			
LOT COVERAGE:	22.8%			
LOT AREA:	AC	SQ. FT.		
GROSS AREA	24,595	1,071,370		
ROW DEDICATION	0.698	30,389		
NET AREA	23,898	1,040,981		
DENSITY:	9.00 UNITS / ACRE (w/o ROW Ded.)			
PROPOSED BUILDINGS (1 STORY):	AREA*	NO. BLDGS	NO. UNITS	TOTAL
1-BEDROOM (DUPLX)	1,342	30	60	40,260
2-BEDROOM	1,022	87	87	88,914
3-BEDROOM	1,307	68	68	88,876
STANDARD GARAGE, DETACHED*	831	20	-	16,620
STANDARD GARAGE W/HANDICAP, DETACHED	682	3	-	2,046
LEASING CENTER	1,131	1	-	1,131
TOTALS:	209	215	215	237,847
PARKING REQUIRED:	UNITS	REQUIRED	PROPOSED	
1 BEDROOM (2 SPACES REQ'D; 2 SPACES PROP.)	60	120	60	
2 BEDROOM (3 SPACES REQ'D; 2 SPACES PROP.)	87	261	174	
3 BEDROOM (4 SPACES REQ'D; 2 SPACES PROP.)	68	272	136	
TOTALS:	215	653	430	
PARKING PROVIDED:	STANDARD SPACES	234		
CARPPOOL SPACES	144			
HANDICAP SPACES	3			
STANDARD GARAGE SPACES	87			
HANDICAP GARAGE SPACES	3			
TOTALS:	471			
GARAGE SPACES PER UNIT:	41.9%			
OPEN SPACE AREA REQUIRED:	83,278	8%	(SF, %)	
OPEN SPACE AREA PROVIDED:	2,773	0.3%	(SF, %)	
EVENT LAWN	7,024	0.7%	(SF, %)	
PASSIVE NODES	16,802	1.6%	(SF, %)	
ACTIVITY NODES	11,968	1.1%	(SF, %)	
DOG PARK	142,338	13.7%	(SF, %)	
MISC. OPEN SPACE	180,905	17.4%	(SF, %)	
TOTALS:	500,032	48.0%	(SF, %)	
LANDSCAPE AREA:	500,032	48.0%	(SF, %)	
TRAIL DEDICATION:	WIDTH, FT	LENGTH, FT	AREA, SF	
LAKE SHARON DRIVE	10	2,189	21,890	
OAKMONT DRIVE	6	1,017	6,102	
TOTALS:	-	3,206	27,992	
IMPERVIOUS AREA:	\$40,949	52.0%	(SF, %)	
LOADING SPACE:	NOT REQUIRED			
OUTSIDE STORAGE:	NONE PROPOSED			
CONSTRUCTION DATES:	Q1 2021 - Q3 2021			



OWNER/DEVELOPER:
NexMetro Communities
 2221 Lakeside Boulevard, Suite 120
 Richardson, Texas 75082
 Tel. No. (972) 795-0039
 Contact: Josh Eadie

ENGINEER:
Lincoln Consulting & Engineering, Inc.
 P.O. Box 1176
 Wylie, Texas 75298
 Tel. No. (214) 215-5066
 Contact: Joshua A. Lincoln, P.E.

PD CONCEPT PLAN
AVILLA FAIRWAYS
LAKE SHARON @ OAKMONT
24.595 ACRE TRACT OF LAND
 BERRY MERCHANT SURVEY, ABSTRACT NO. 800
 A.H. SERREN SURVEY, ABSTRACT NO. 1198
CITY OF CORINTH, DENTON COUNTY, TEXAS
 June 22, 2021

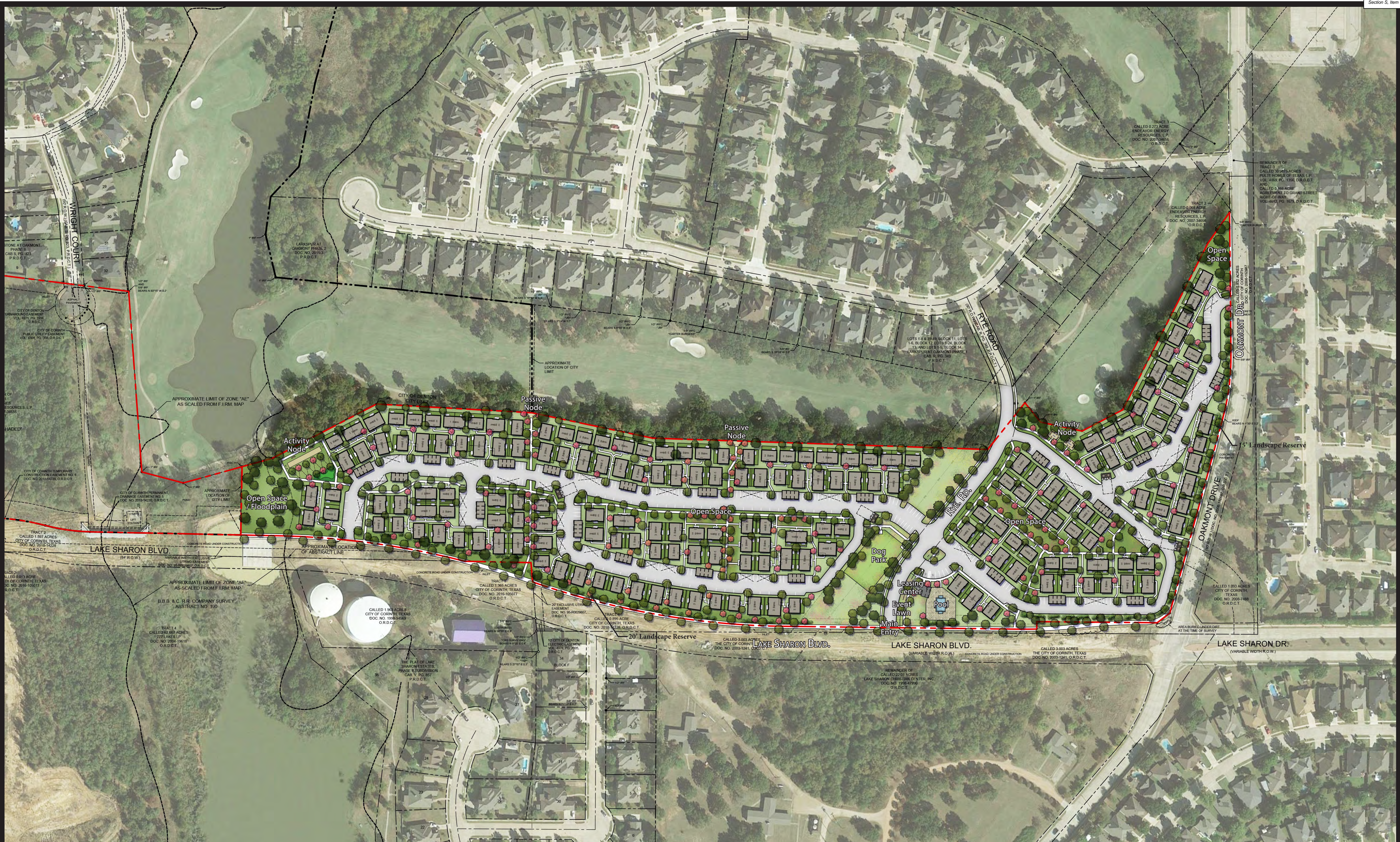
PRELIMINARY
 FOR REVIEW ONLY
 NOT FOR CONSTRUCTION
 LINCOLN CONSULTING & ENGINEERING
 Firm No. 7123
 Engineer: JOSHUA A. LINCOLN, P.E.
 P.E. No. 114389 Date: 6/22/2021

AVILLA FAIRWAYS
LAKE SHARON @ OAKMONT
24.595 ACRE TRACT OF LAND
 City of Corinth, Denton County, Texas

CP

Scale: 1" = 100'
 Designed by: JAL
 Drawn by: JAL
 Checked by: JAL
 Date: June 22, 2021
 Project No. 1015-003

LC&E LINCOLN CONSULTING & ENGINEERING
 phone (214) 215-5066
 Josh@lincolncpe.com
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 P.O. Box 1176
 Wylie, Texas 75298
 AVILLA FAIRWAYS - City of Corinth, Texas - LC&E PROJECT NO. 1015-003



KLS
KNAPP LAND SOLUTIONS

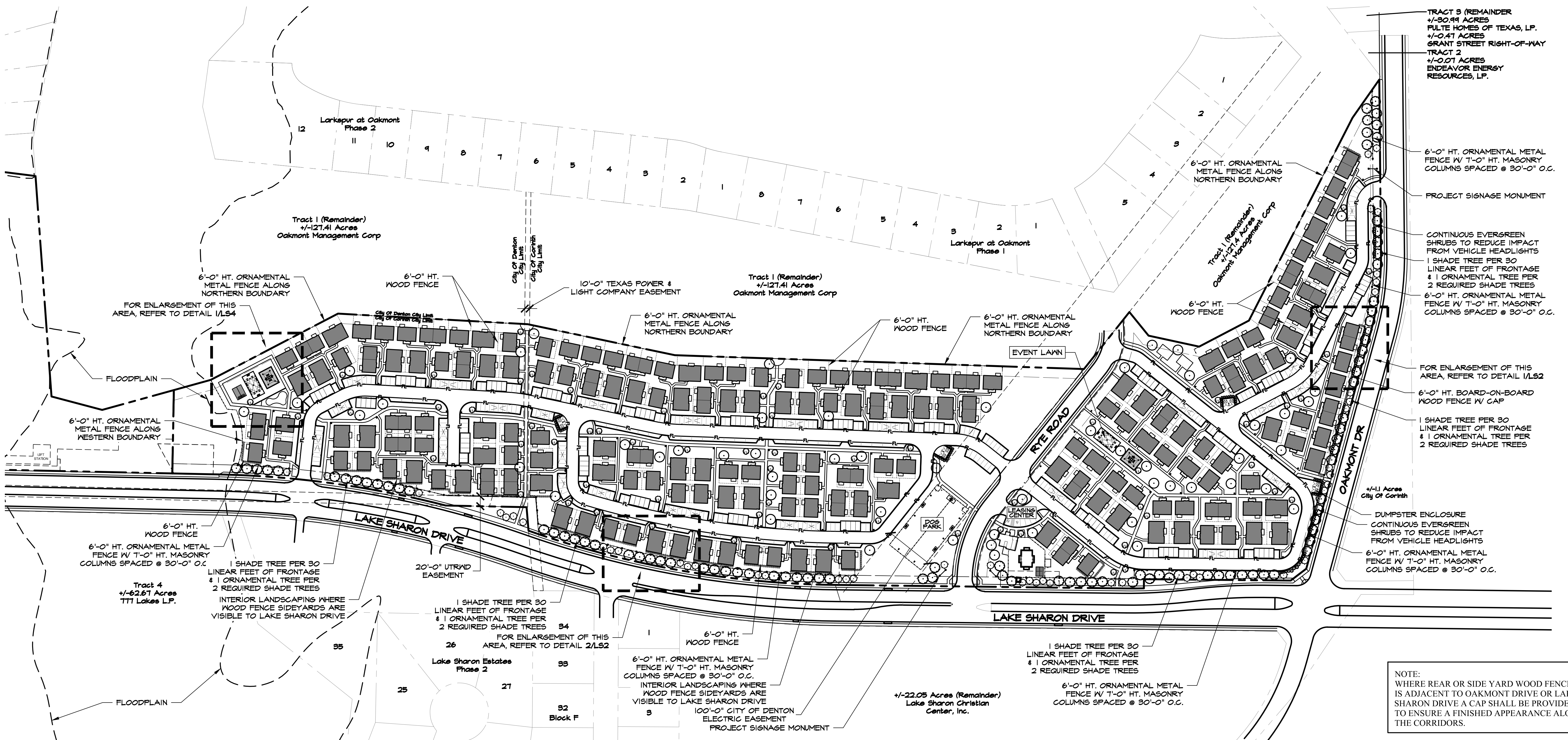
June 23, 2021

Scale 1"=100'

This drawing is a pictorial representation for presentation purposes only and is subject to change without notice. Additionally, no warranty is made to the accuracy, completeness, or of the information contained herein.

AVILLA FAIRWAYS - CORINTH, TX

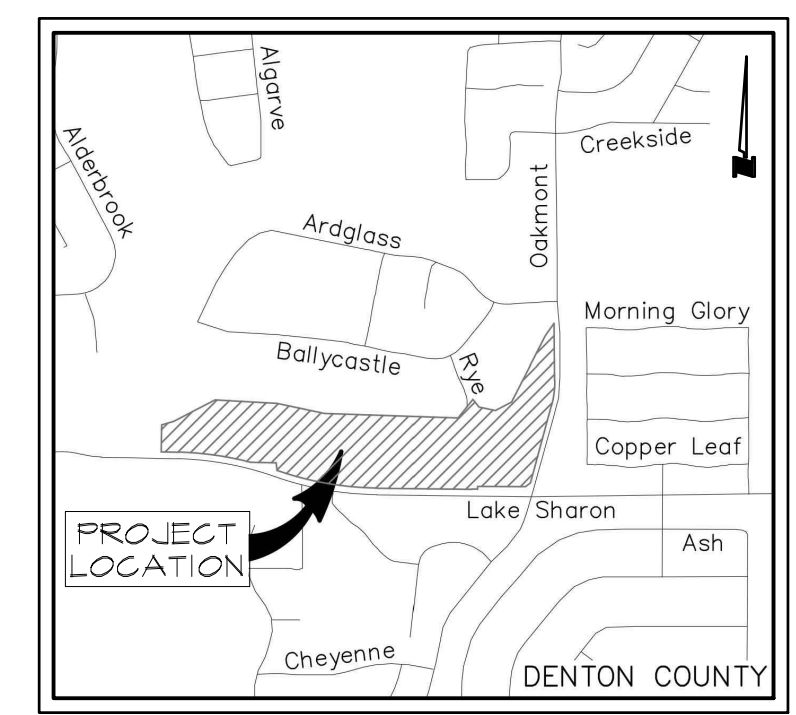
ILLUSTRATIVE PLAN



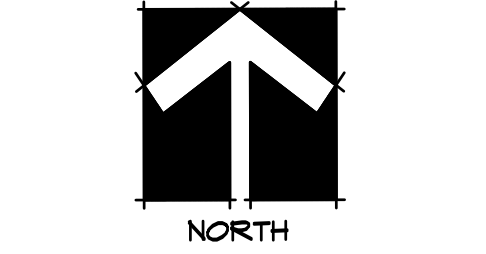
NOTE:
WHERE REAR OR SIDE YARD WOOD FENCING IS ADJACENT TO OAKMONT DRIVE OR LAKE SHARON DRIVE A CAP SHALL BE PROVIDED TO ENSURE A FINISHED APPEARANCE ALONG THE CORRIDORS.

LANDSCAPE REQUIREMENTS	
OAKMONT DRIVE	
A.	1-3" CAL. CANOPY TREE / 30 LF OF LANDSCAPE BUFFER 982 LF OF LS BUFFER / 30 LF = 32 - 3" CAL. CANOPY TREES REQUIRED PROVIDED: 32 - 3" CALIFER CANOPY TREES PROVIDED
B.	1-2" CAL. ORNAMENTAL TREE FOR EVERY 2 CANOPY TREES REQUIRED 32 - 3" CAL. CANOPY TREES / 2 = 16 - 2" CAL. ORNAMENTAL TREES REQUIRED PROVIDED: 16 - 2" CALIFER ORNAMENTAL TREES PROVIDED
LAKE SHARON DRIVE	
A.	1-3" CAL. CANOPY TREE / 30 LF OF LANDSCAPE BUFFER 2,000 LF OF LS BUFFER / 30 LF = 66 - 3" CAL. CANOPY TREES REQUIRED PROVIDED: 66 - 3" CALIFER CANOPY TREES PROVIDED
B.	1-2" CAL. ORNAMENTAL TREE FOR EVERY 2 CANOPY TREES REQUIRED 66 - 3" CAL. CANOPY TREES / 2 = 33 - 2" CAL. ORNAMENTAL TREES REQUIRED PROVIDED: 33 - 2" CALIFER ORNAMENTAL TREES PROVIDED

AVILLA FAIRWAYS
~CONCEPTUAL LANDSCAPE PLAN~



LOCATION MAP
NOT TO SCALE



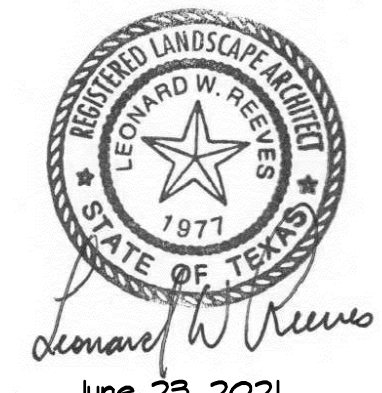
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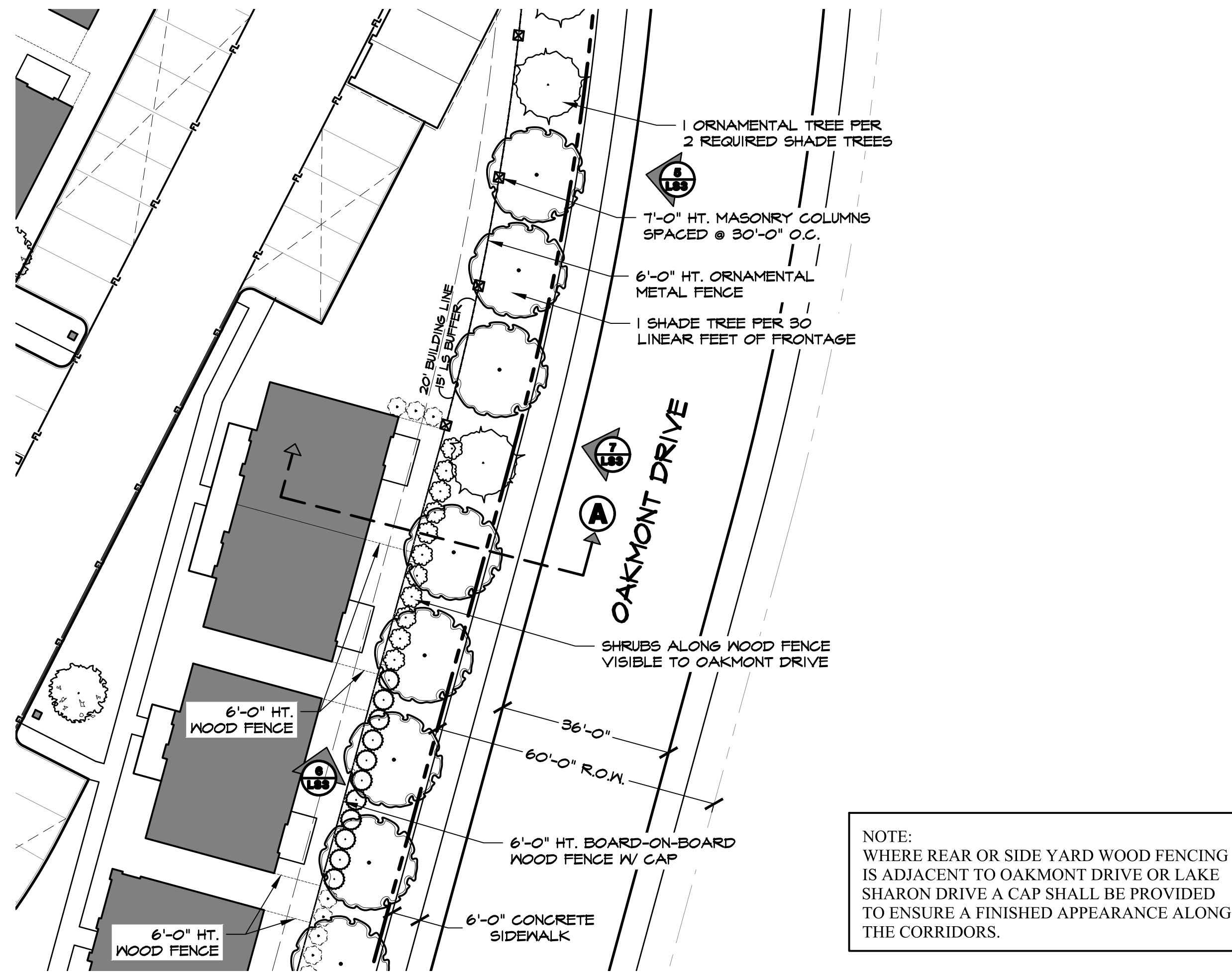
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NOTE:
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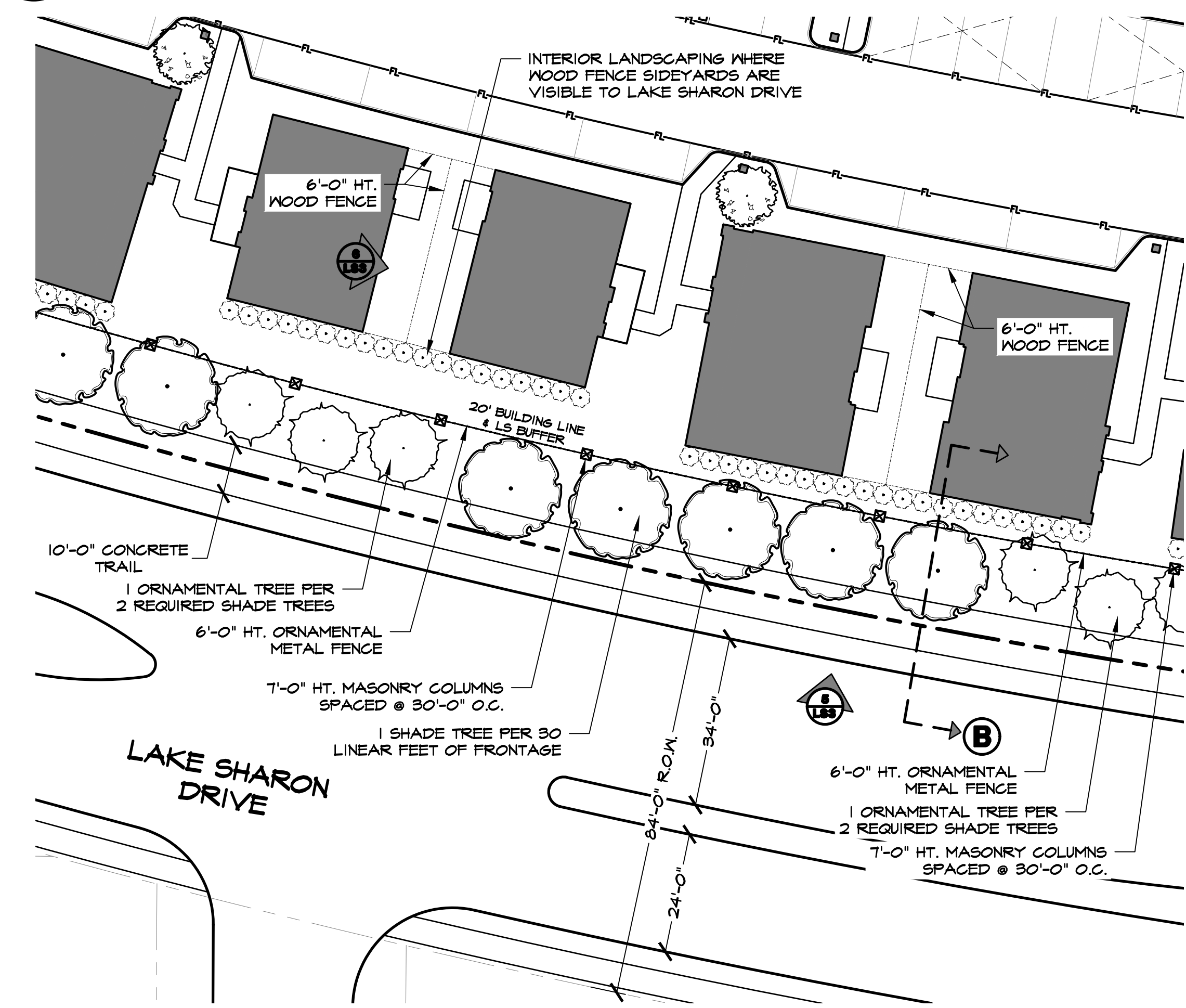


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Lewisville, Texas 75057
469-635-1900
TBAE Firm #BR643



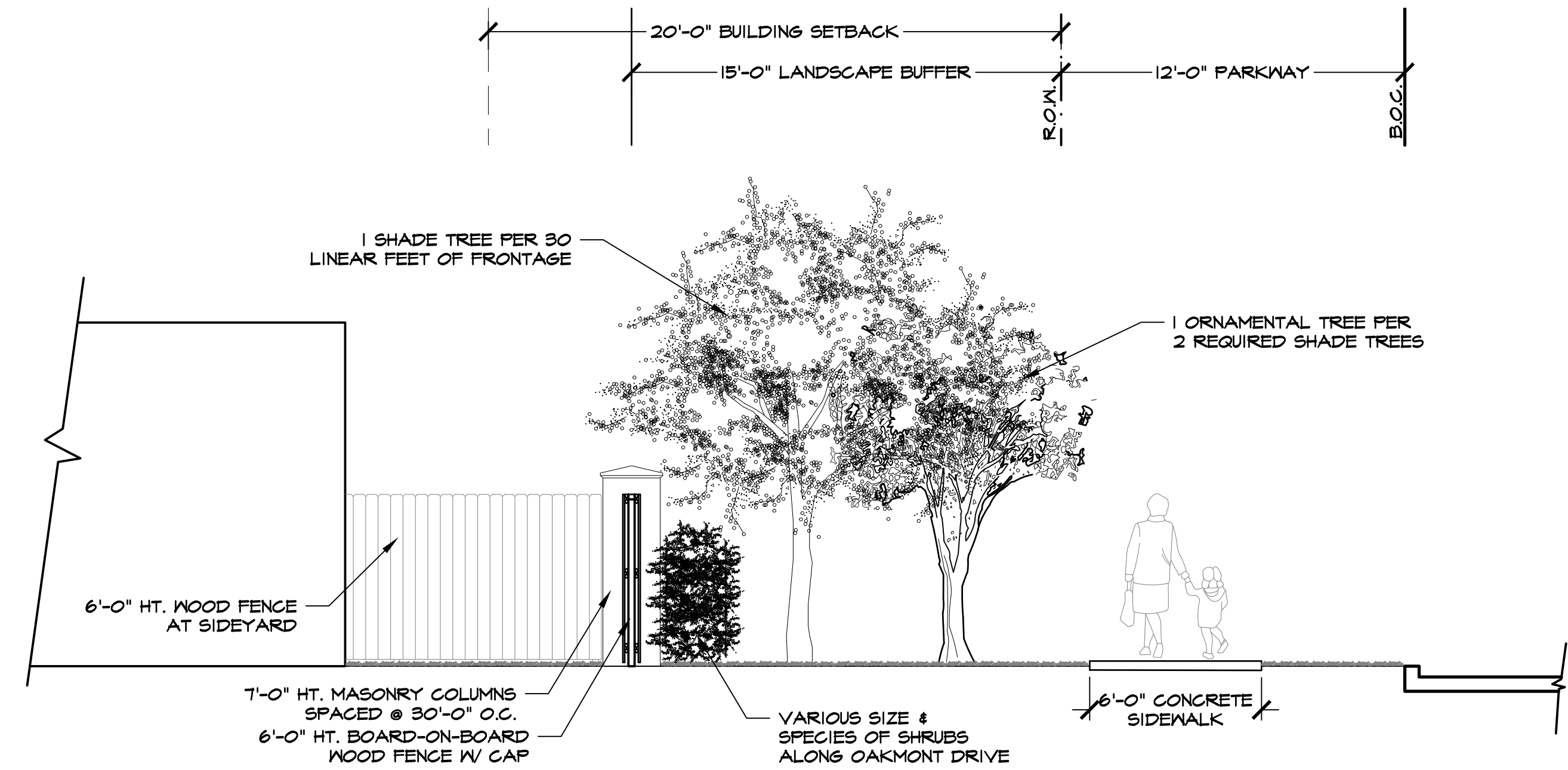


1 OAKMONT DRIVE FRONTAGE PLAN
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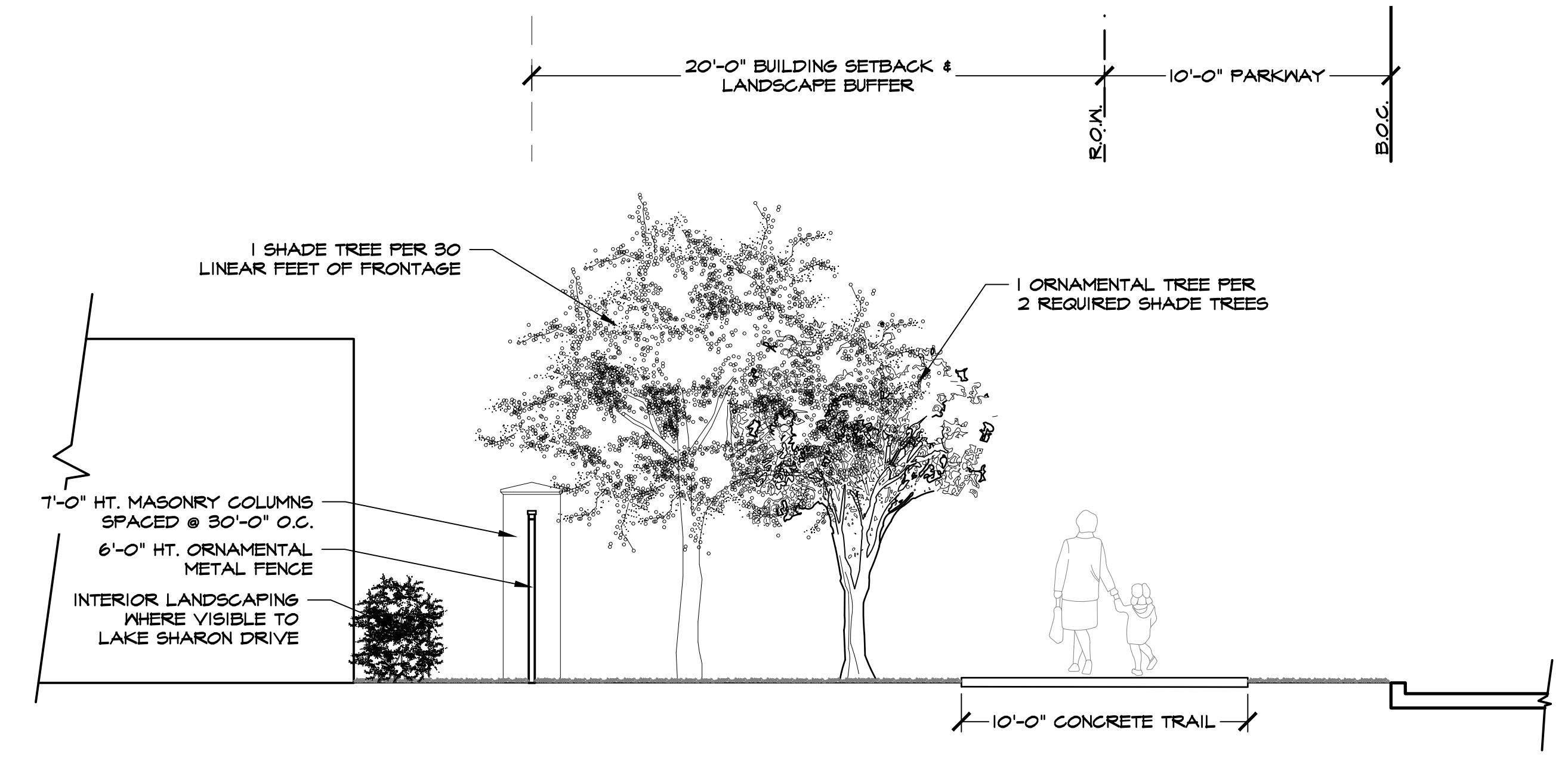


2 LAKE SHARON DRIVE FRONTAGE PLAN
 SCALE: 1"=20'-0"

NOTE:
 WHERE REAR OR SIDE YARD WOOD FENCING IS ADJACENT TO OAKMONT DRIVE OR LAKE SHARON DRIVE A CAP SHALL BE PROVIDED TO ENSURE A FINISHED APPEARANCE ALONG THE CORRIDORS.

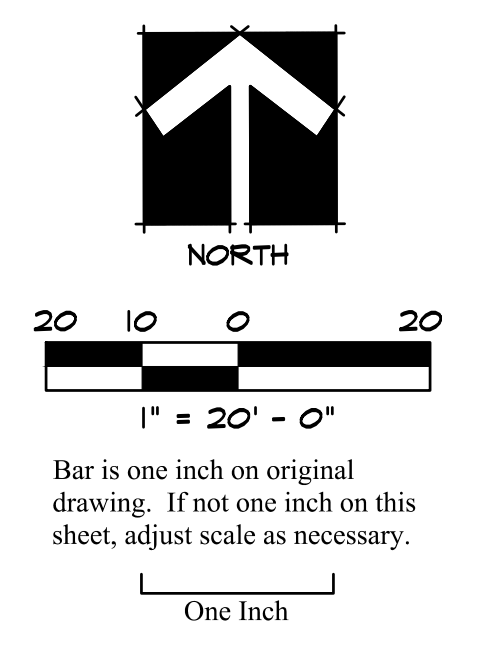
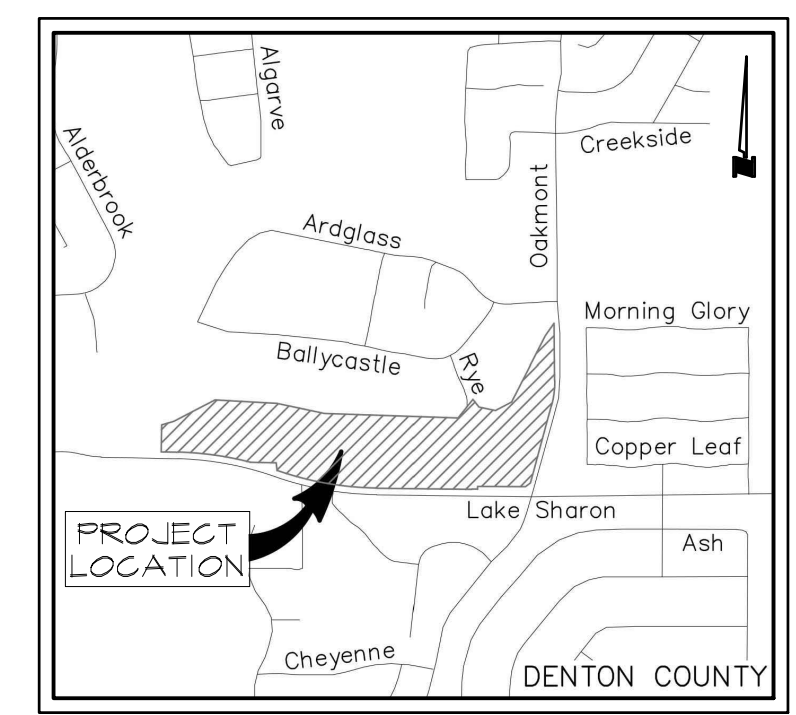


A LANDSCAPE SECTION - OAKMONT DRIVE FRONTAGE SECTION
 SCALE: 1/4"=1'-0"



B LANDSCAPE SECTION - LAKE SHARON DRIVE FRONTAGE SECTION
 SCALE: 1/4"=1'-0"

AVILLA FAIRWAYS
 ~CONCEPTUAL LANDSCAPE PLAN~

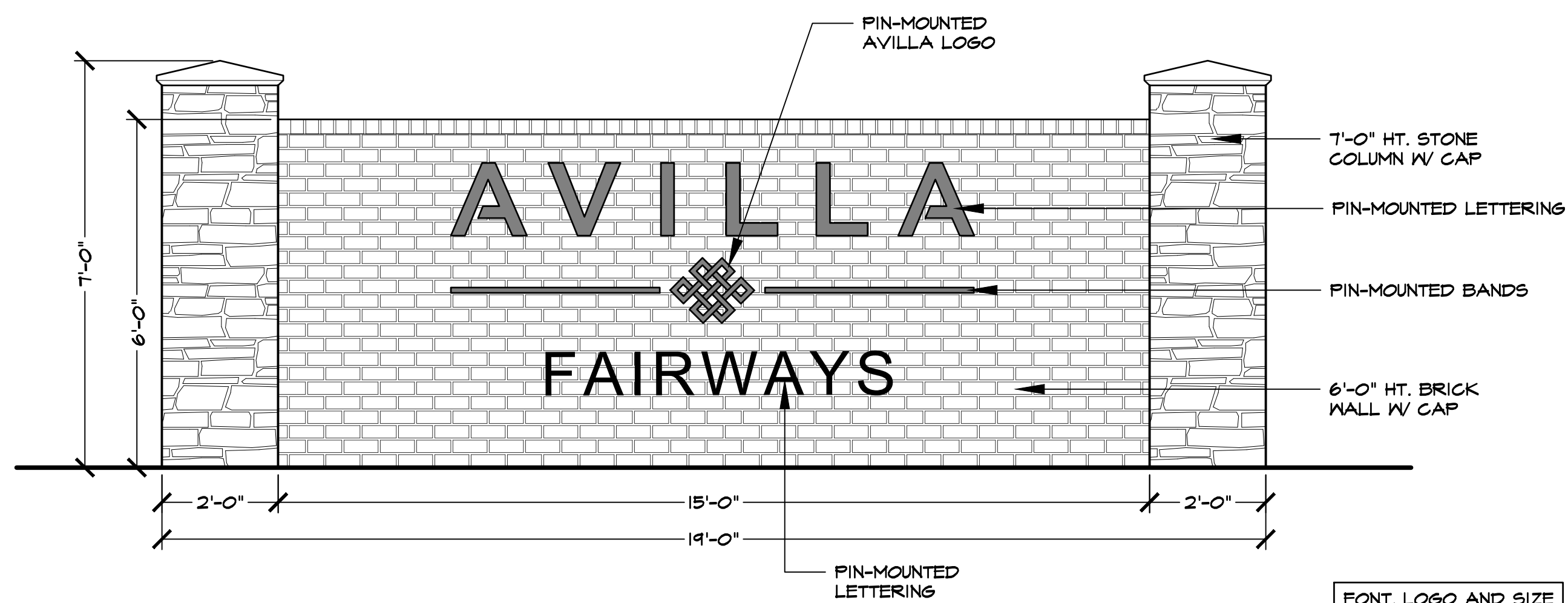


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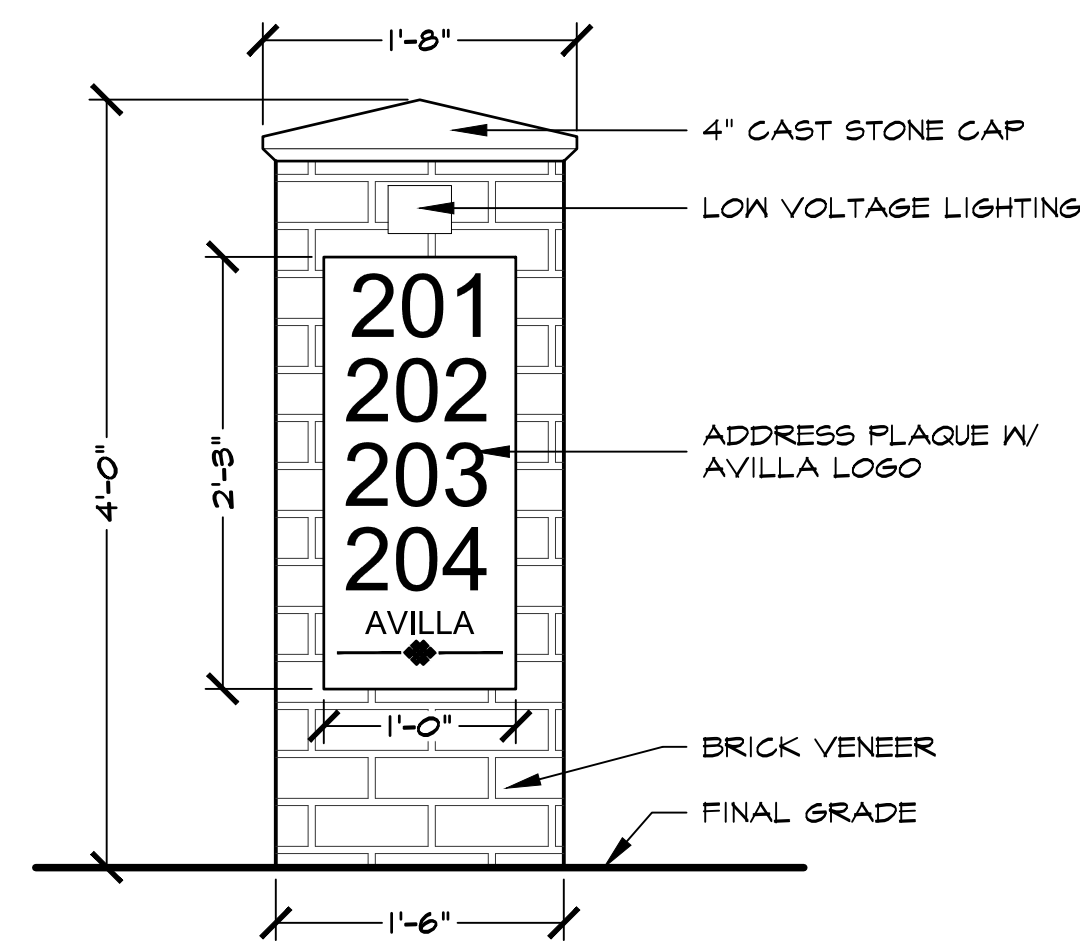




1 PROJECT SIGNAGE MONUMENT ELEVATION

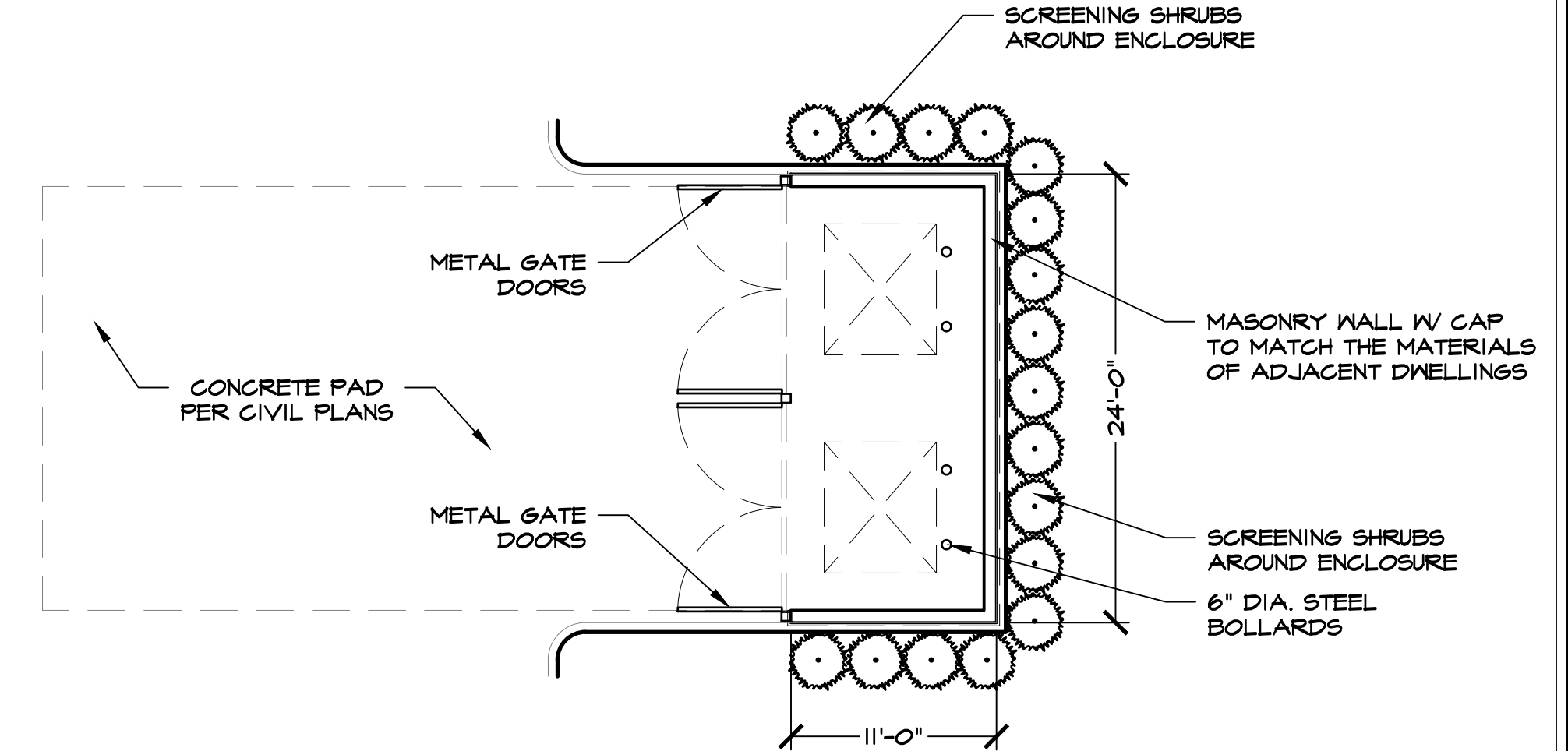
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FONT, LOGO AND SIZE ARE CONCEPTUAL & SUBJECT TO CHANGE



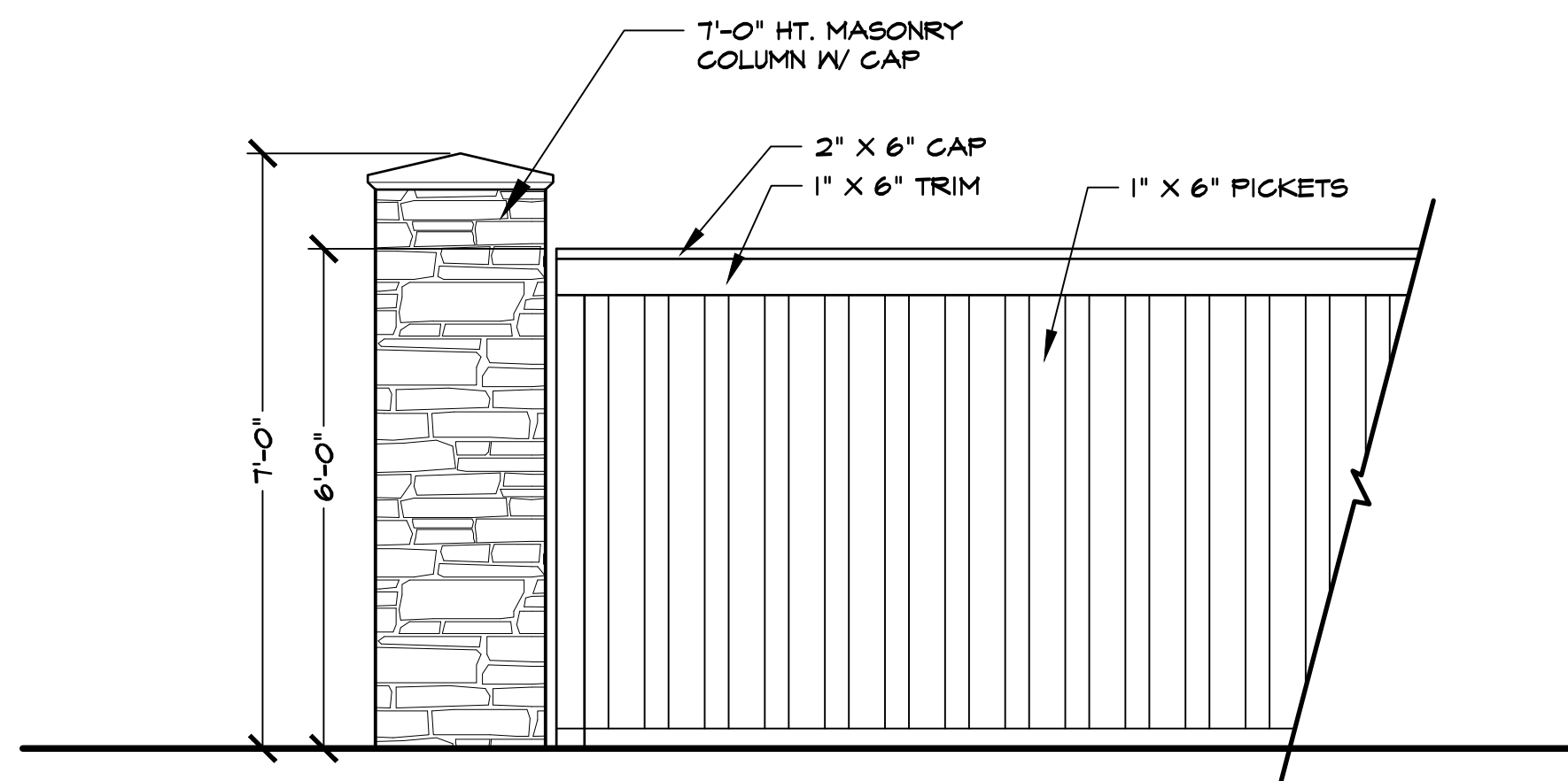
2 UNIT PORTAL SIGN ELEVATION

SCALE: 1"=1'-0"



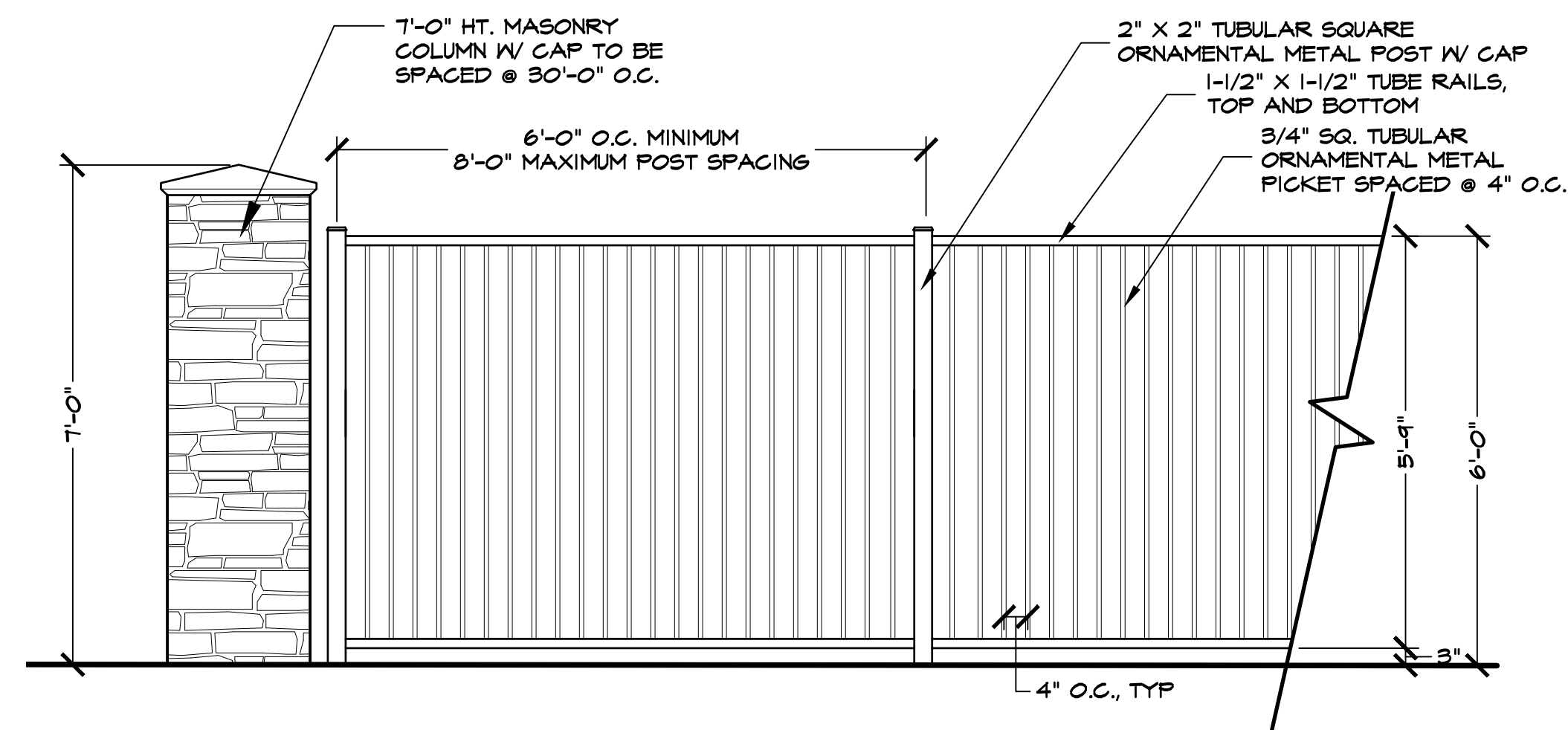
3 DUMPSTER ENCLOSURE PLAN

SCALE: 1/8"=1'-0"



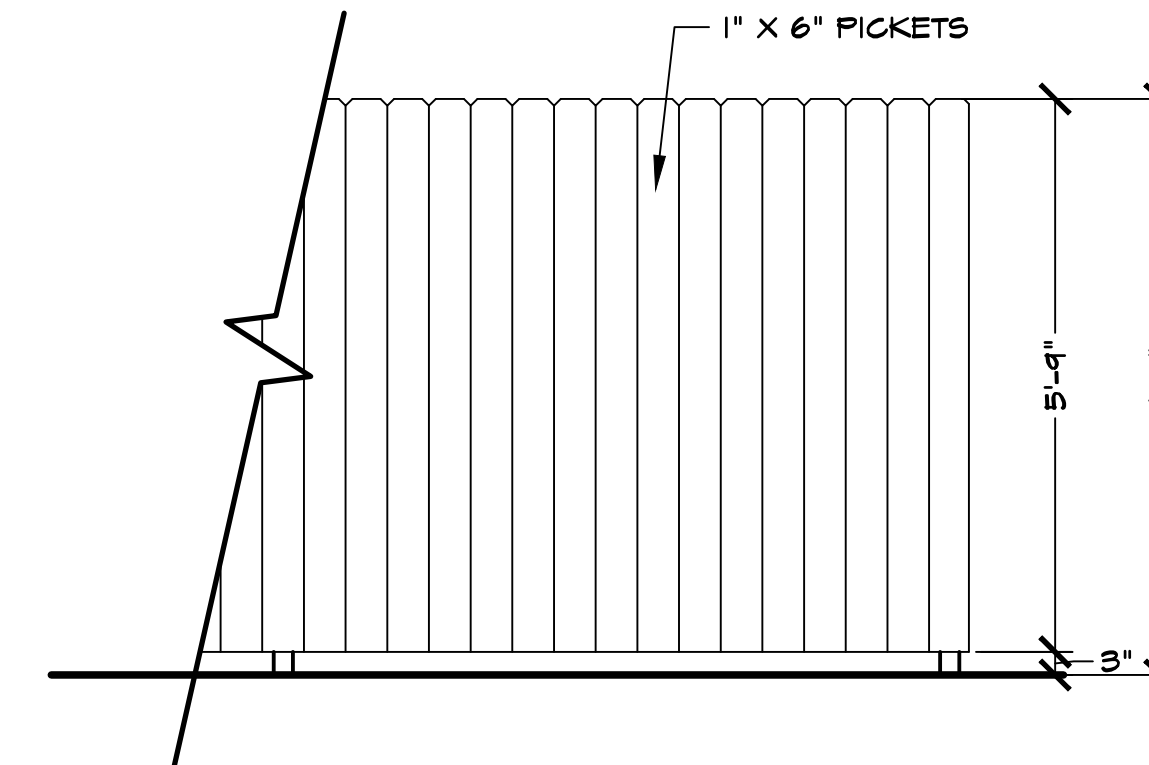
4 6'-0" HT. BOARD-ON-BOARD WOOD FENCE ALONG FRONTAGE ELEVATION

SCALE: 1/2"=1'-0"



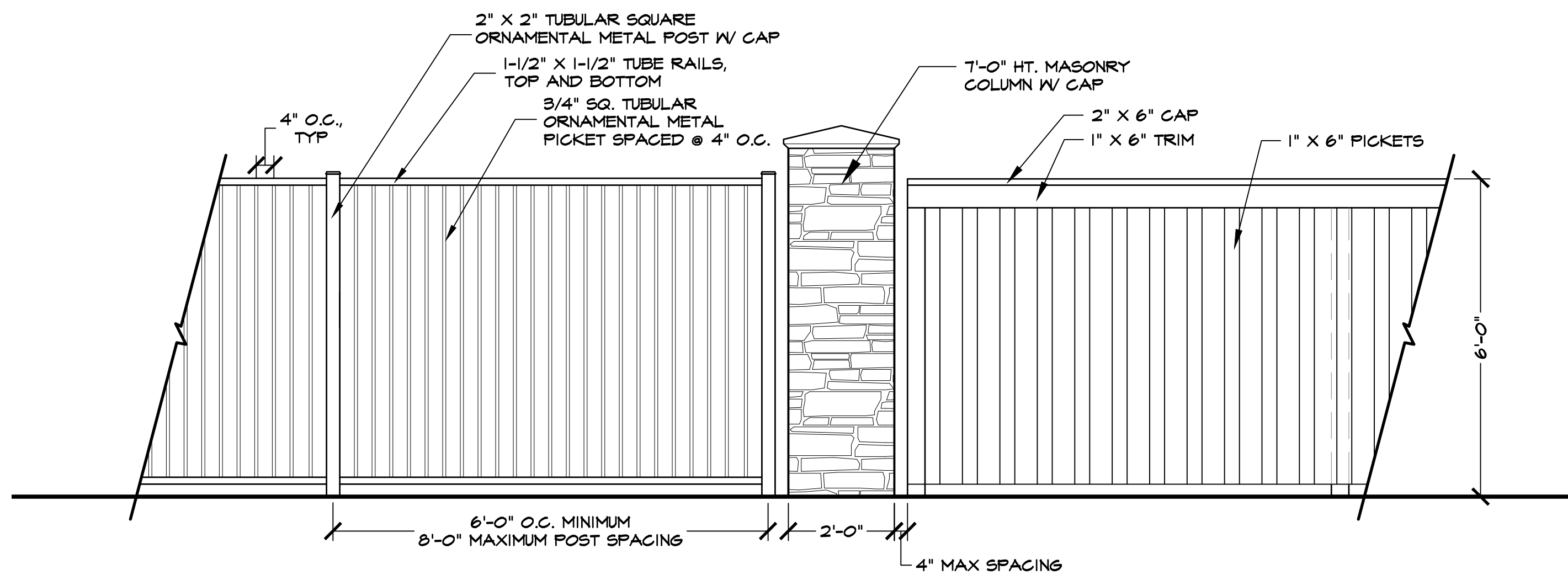
5 6'-0" HT. ORNAMENTAL METAL FENCE ALONG FRONTAGE ELEVATION

SCALE: 1/2"=1'-0"



6 6'-0" HT. WOOD FENCE (INTERNAL YARDS) ELEVATION

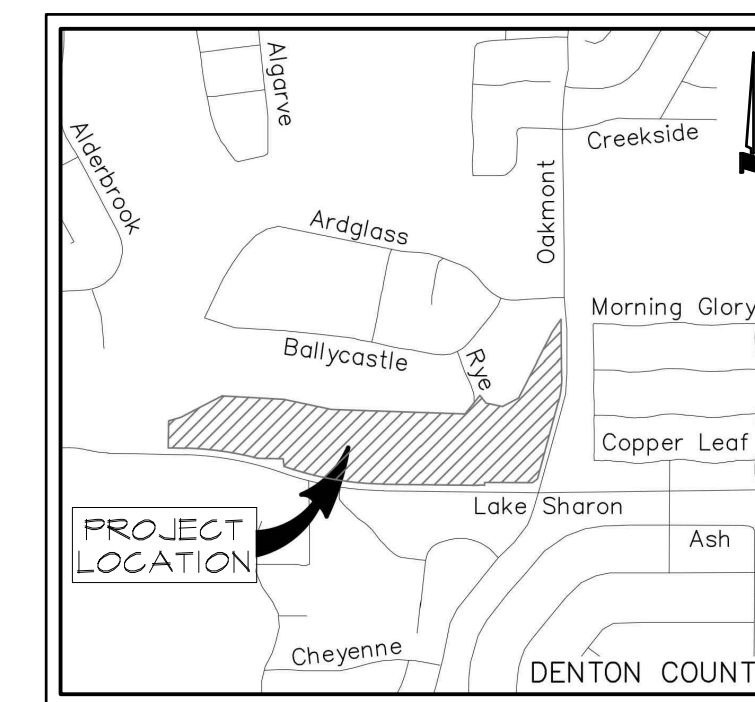
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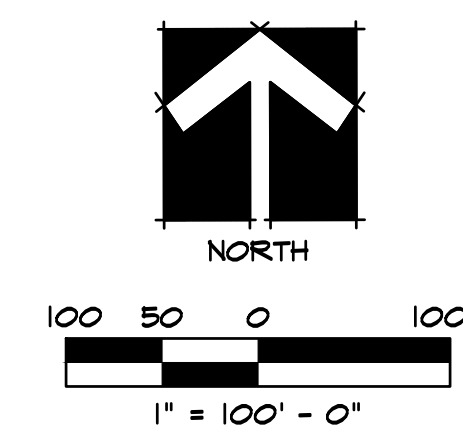
7 PARTIAL ELEVATION, WOOD FENCING TRANSITION TO METAL FENCING ELEVATION

SCALE: 1/2"=1'-0"

AVILLA FAIRWAYS
~CONCEPTUAL FENCING & MONUMENTATION~



LOCATION MAP
NOT TO SCALE



Bar is one inch on original drawing. If not one inch on this sheet, adjust scale as necessary.
One Inch

NOTE: ALL IMPROVEMENTS SHOWN ON THIS PLAN ARE CONCEPTUAL IN NATURE & SUBJECT TO CHANGE.



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386 W. Main Street
Lewisville, Texas 75057
469-635-1900

TBAE Firm #BR643



June 23, 2021

<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>0% FIBER CEMENT SIDING 70% BRICK 30% STONE</p>	<p>LEFT ELEVATION</p> <p>0% FIBER CEMENT SIDING 70% BRICK 30% STONE</p>	<p>FRONT ELEVATION</p> <p>5% FIBER CEMENT SIDING 49% BRICK 46% STONE</p>
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ALTERNATE ROOF PITCH



02 FRONT ELEVATION (ALTERNATE)

<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>0% FIBER CEMENT SIDING 70% BRICK 30% STONE</p>	<p>LEFT ELEVATION</p> <p>0% FIBER CEMENT SIDING 70% BRICK 30% STONE</p>	<p>FRONT ELEVATION</p> <p>3% FIBER CEMENT SIDING 50% BRICK 47% STONE</p>
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STANDARD ROOF PITCH



01 FRONT ELEVATION (STANDARD)

SCALE: 1/4" = 1'-0"

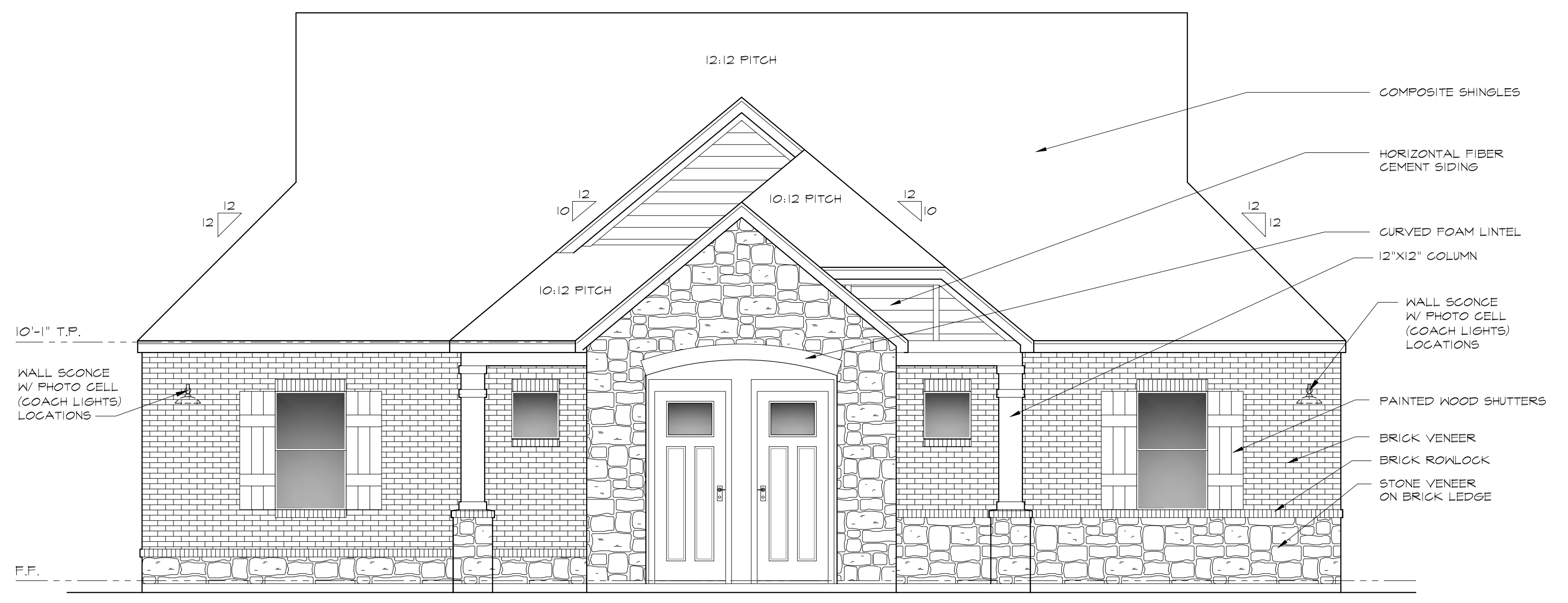
BUILDING TYPE IA ELEVATIONS

SCALE: 1/4" = 1'-0"

<p>ARCHITECT IKEMIRE ARCHITECTS 16660 Dallas Parkway, Suite 2900 Dallas TX 75001 PH 972-248-2486 FX 972-248-1557 CONTACT: JAMIE HULSE</p>	<p>DEVELOPER NEXMETRO COMMUNITIES THREE ENERGY SQUARE 6688 N. CENTRAL EXPRESSWAY, SUITE 275 Addison TX 75206 PH 972-745-0039 CONTACT: JASON FLORY</p>	<p>NEXMETRO AVILLA FAIRWAYS BUILDING ELEVATIONS</p>
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<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>5% FIBER CEMENT SIDING 85% BRICK 10% STONE</p>	<p>RIGHT ELEVATION</p> <p>5% FIBER CEMENT SIDING 85% BRICK 10% STONE</p>	<p>FRONT ELEVATION</p> <p>10% FIBER CEMENT SIDING 60% BRICK 40% STONE</p>
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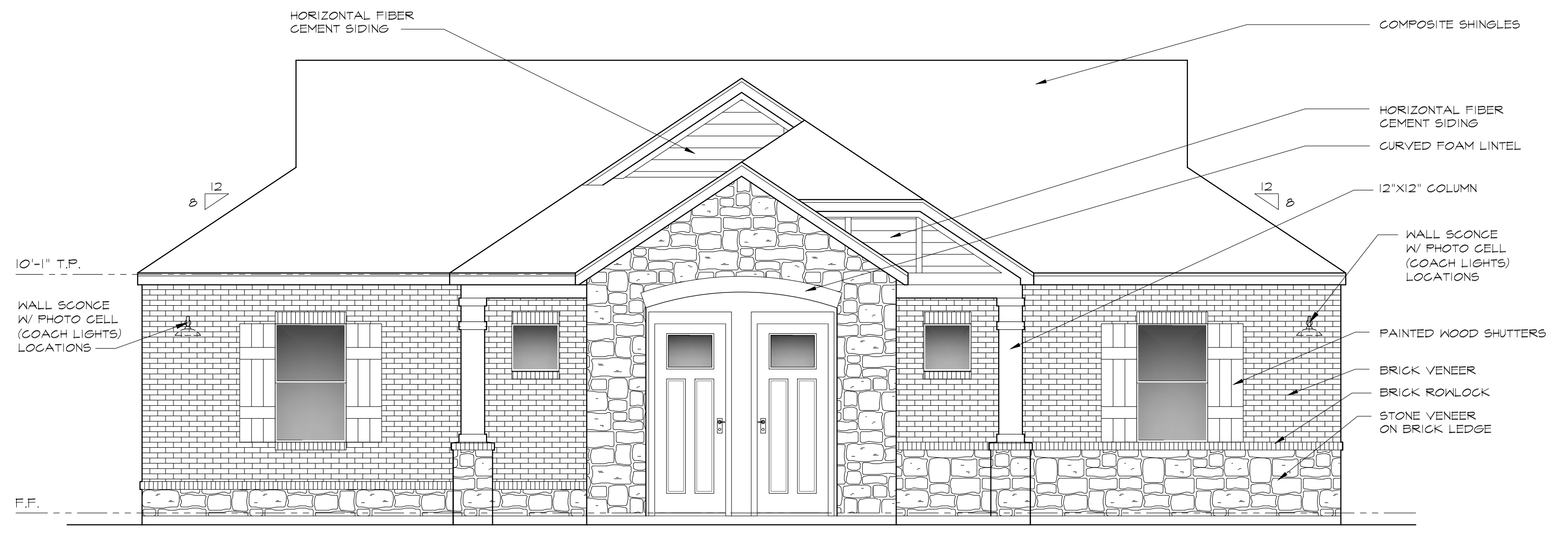
ALTERNATE ROOF PITCH



02 FRONT ELEVATION (ALTERNATE)
SCALE: 1/4" = 1'-0"

<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>3% FIBER CEMENT SIDING 86% BRICK 11% STONE</p>	<p>RIGHT ELEVATION</p> <p>3% FIBER CEMENT SIDING 86% BRICK 11% STONE</p>	<p>FRONT ELEVATION</p> <p>10% FIBER CEMENT SIDING 60% BRICK 40% STONE</p>
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STANDARD ROOF PITCH



01 FRONT ELEVATION (STANDARD)
SCALE: 1/4" = 1'-0"

BUILDING TYPE IB ELEVATIONS

SCALE: 1/4" = 1'-0"

<p>ARCHITECT IKEMIRE ARCHITECTS 16660 Dallas Parkway, Suite 2900 Dallas TX 75001 PH 972-248-2486 FX 972-248-1557 CONTACT: JAMIE HULSE</p>	<p>DEVELOPER NEXMETRO COMMUNITIES THREE ENERGY SQUARE 6688 N. CENTRAL EXPRESSWAY, SUITE 275 Addison TX 75206 PH 972-745-0039 CONTACT: JASON FLORY</p>	<p>NEXMETRO AVILLA FAIRWAYS BUILDING ELEVATIONS</p>
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<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>21% FIBER CEMENT SIDING 76% BRICK 3% STONE</p>	<p>RIGHT ELEVATION</p> <p>21% FIBER CEMENT SIDING 76% BRICK 3% STONE</p>	<p>FRONT ELEVATION</p> <p>14% FIBER CEMENT SIDING 82% BRICK 4% STONE</p>
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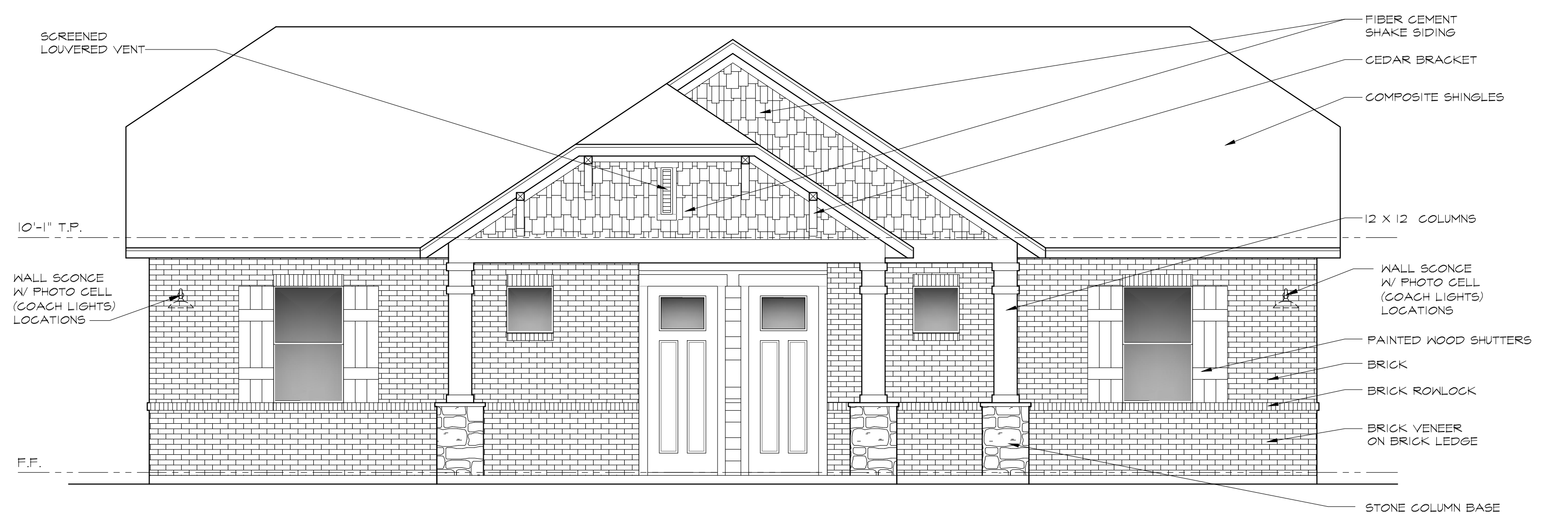
ALTERNATE ROOF PITCH



02 FRONT ELEVATION (ALTERNATE)
SCALE: 1/4"=1'-0"

<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>20% FIBER CEMENT SIDING 77% BRICK 3% STONE</p>	<p>RIGHT ELEVATION</p> <p>20% FIBER CEMENT SIDING 77% BRICK 3% STONE</p>	<p>FRONT ELEVATION</p> <p>12% FIBER CEMENT SIDING 83% BRICK 5% STONE</p>
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STANDARD ROOF PITCH



01 FRONT ELEVATION (STANDARD)
SCALE: 1/4"=1'-0"

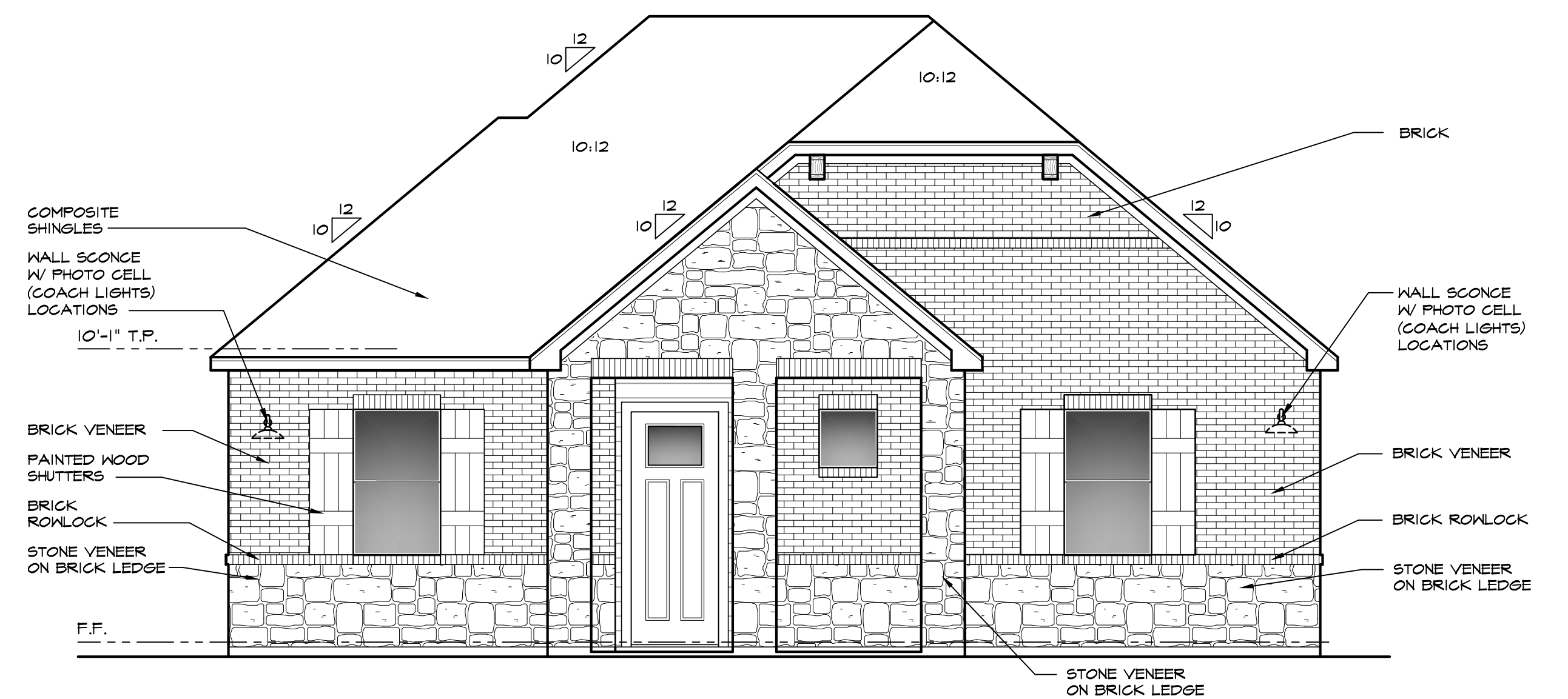
BUILDING TYPE 1C ELEVATIONS

SCALE: 1/4" = 1'-0"

<p>ARCHITECT</p> <p>IKEMIRE ARCHITECTS 16660 Dallas Parkway, Suite 2900 Dallas TX 75001 PH 972-248-2486 FX 972-248-1557 CONTACT: JAMIE HULSE</p>	<p>DEVELOPER</p> <p>NEXMETRO COMMUNITIES THREE ENERGY SQUARE 6688 N. CENTRAL EXPRESSWAY, SUITE 275 Addison TX 75206 PH 972-745-0039 CONTACT: JASON FLORY</p>	<p>NEXMETRO</p> <p>AVILLA FAIRWAYS BUILDING ELEVATIONS</p>
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<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK AND STONE 94% BRICK AND 6% STONE</p>	<p>RIGHT ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK AND STONE 95% BRICK AND 5% STONE</p>	<p>FRONT ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK AND STONE (58% BRICK, 42% STONE)</p>
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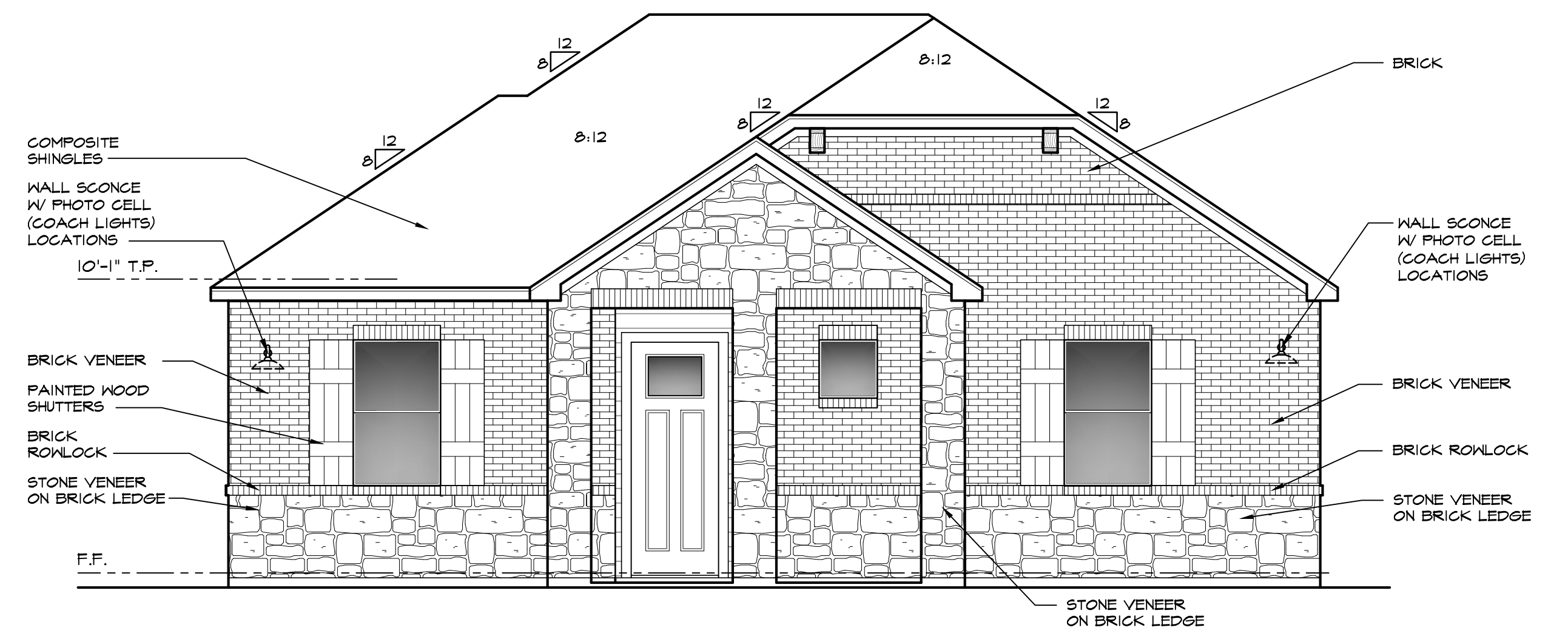
ALTERNATE ROOF PITCH



02 FRONT ELEVATION (ALTERNATE)

<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK AND STONE 94% BRICK AND 6% STONE</p>	<p>RIGHT ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK AND STONE 94% BRICK AND 6% STONE</p>	<p>FRONT ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK AND STONE (60% BRICK, 40% STONE)</p>
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STANDARD ROOF PITCH



01 FRONT ELEVATION (STANDARD)

BUILDING TYPE 2A ELEVATIONS

SCALE: 1/4" = 1'-0"

<p>ARCHITECT IKEMIRE ARCHITECTS 16660 Dallas Parkway, Suite 2900 Dallas TX 75001 PH 972-248-2486 FX 972-248-1557 CONTACT: JAMIE HULSE</p>	<p>DEVELOPER NEXMETRO COMMUNITIES THREE ENERGY SQUARE 6688 N. CENTRAL EXPRESSWAY, SUITE 275 Addison TX 75206 PH 972-795-0039 CONTACT: JASON FLORY</p>	<p>NEXMETRO AVILLA FAIRWAYS BUILDING ELEVATIONS</p>
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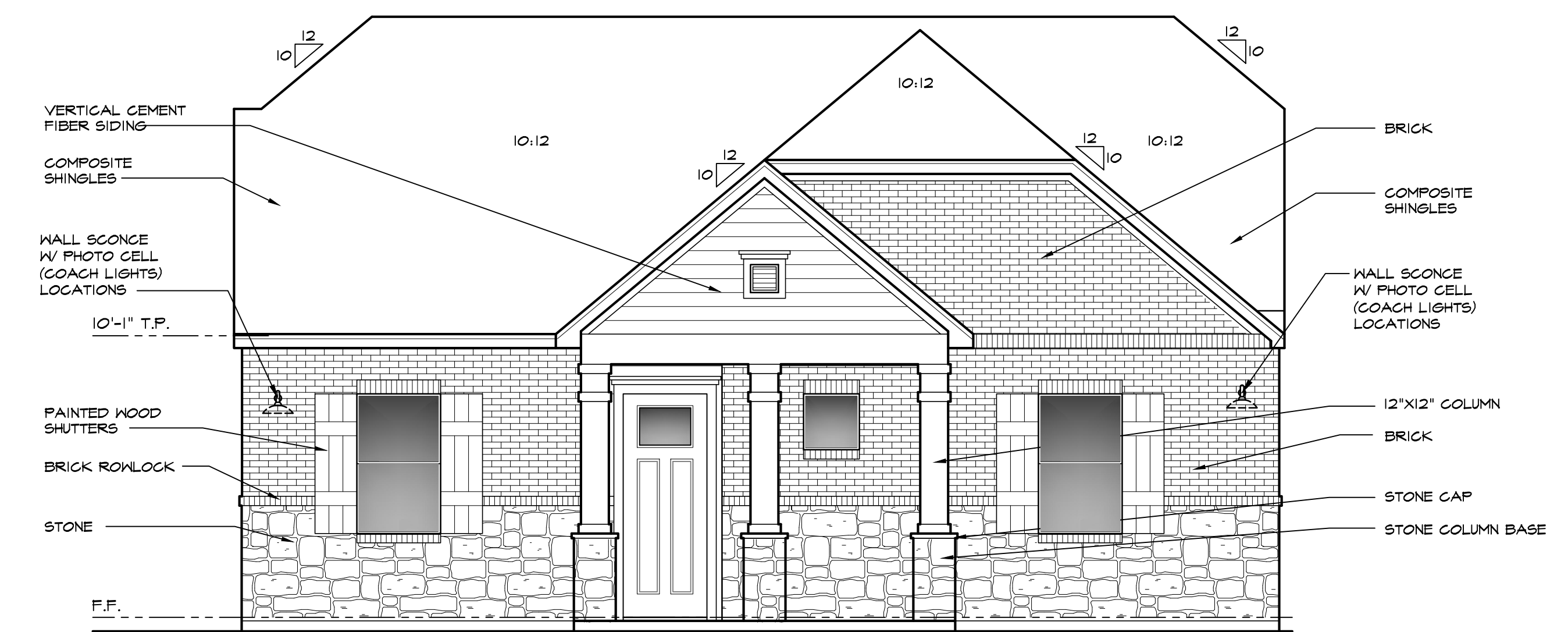
REAR ELEVATION
 0% FIBER CEMENT SIDING
 100% BRICK
 0% STONE

LEFT ELEVATION
 25% FIBER CEMENT SIDING
 75% BRICK AND STONE
 71% BRICK AND 4% STONE

RIGHT ELEVATION
 24% FIBER CEMENT SIDING
 76% BRICK AND STONE
 72% BRICK AND 4% STONE

FRONT ELEVATION
 8% FIBER CEMENT SIDING
 92% BRICK AND STONE
 (66% BRICK, 26% STONE)

ALTERNATE ROOF PITCH



02 FRONT ELEVATION (ALTERNATE)
 SCALE: 1/4"=1'-0"

FRONT PORCH 78 S.F.

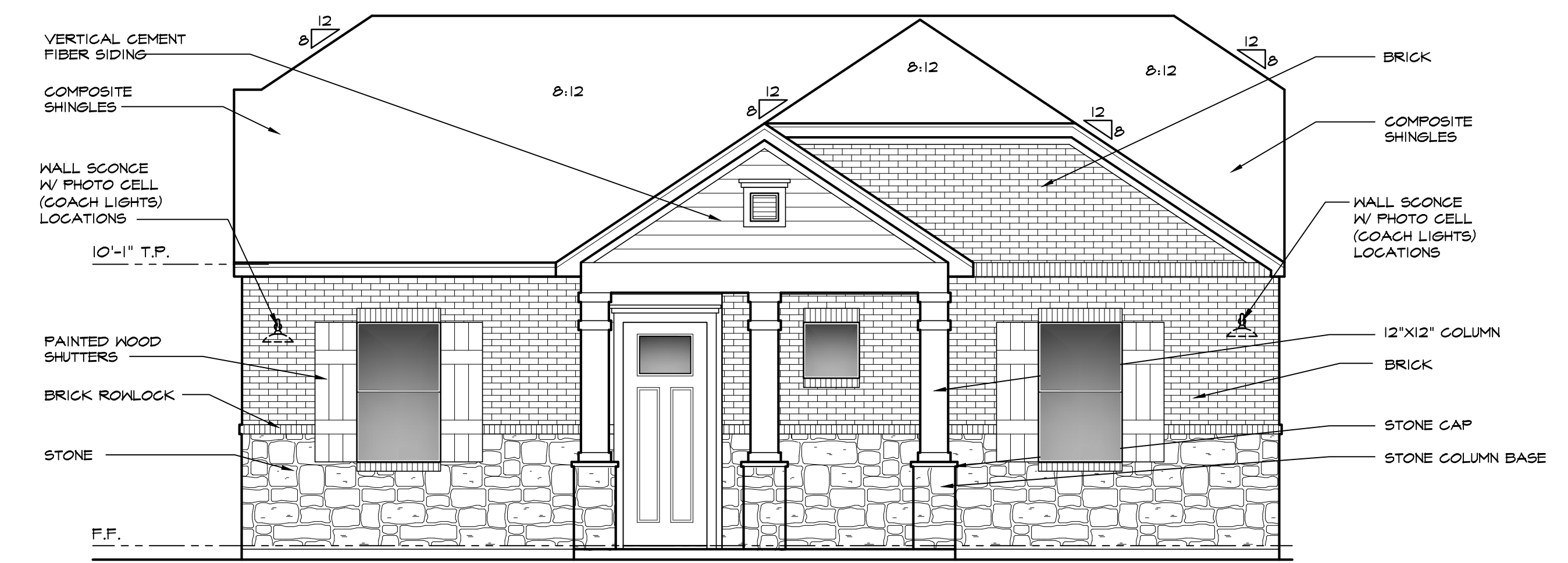
REAR ELEVATION
 0% FIBER CEMENT SIDING
 100% BRICK
 0% STONE

LEFT ELEVATION
 25% FIBER CEMENT SIDING
 75% BRICK AND STONE
 71% BRICK AND 4% STONE

RIGHT ELEVATION
 23% FIBER CEMENT SIDING
 77% BRICK AND STONE
 73% BRICK AND 4% STONE

FRONT ELEVATION
 6% FIBER CEMENT SIDING
 94% BRICK AND STONE
 (61% BRICK, 33% STONE)

STANDARD ROOF PITCH



01 FRONT ELEVATION (STANDARD)
 SCALE: 1/4"=1'-0"

FRONT PORCH 78 S.F.

BUILDING TYPE 2B ELEVATIONS

SCALE: 1/4" = 1'-0"

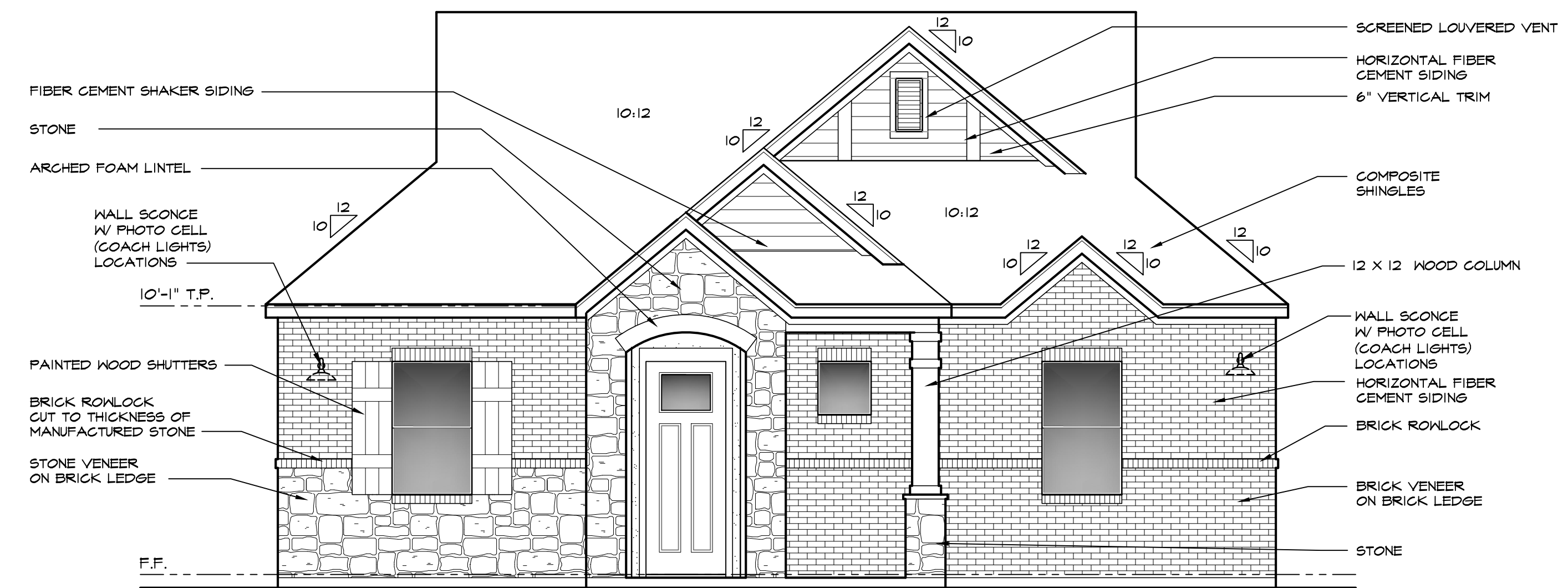
ARCHITECT
 IKEMIRE ARCHITECTS
 16660 Dallas Parkway, Suite 2900
 Dallas TX 75001
 PH 972-248-2486 FX 972-248-1557
 CONTACT: JAMIE HULSE

DEVELOPER
 NEXMETRO COMMUNITIES
 THREE ENERGY SQUARE
 6688 N. CENTRAL EXPRESSWAY, SUITE 275
 Addison TX 75206
 PH 972-795-0039
 CONTACT: JASON FLORY

NEXMETRO
 AVILLA FAIRWAYS
 BUILDING ELEVATIONS

<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>9% FIBER CEMENT SIDING 91% BRICK AND STONE 83% BRICK AND 8% STONE</p>	<p>RIGHT ELEVATION</p> <p>10% FIBER CEMENT SIDING 90% BRICK AND STONE 88% BRICK AND 2% STONE</p>	<p>FRONT ELEVATION</p> <p>5% FIBER CEMENT SIDING 95% BRICK AND STONE (84% BRICK, 11% STONE)</p>
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ALTERNATE ROOF PITCH



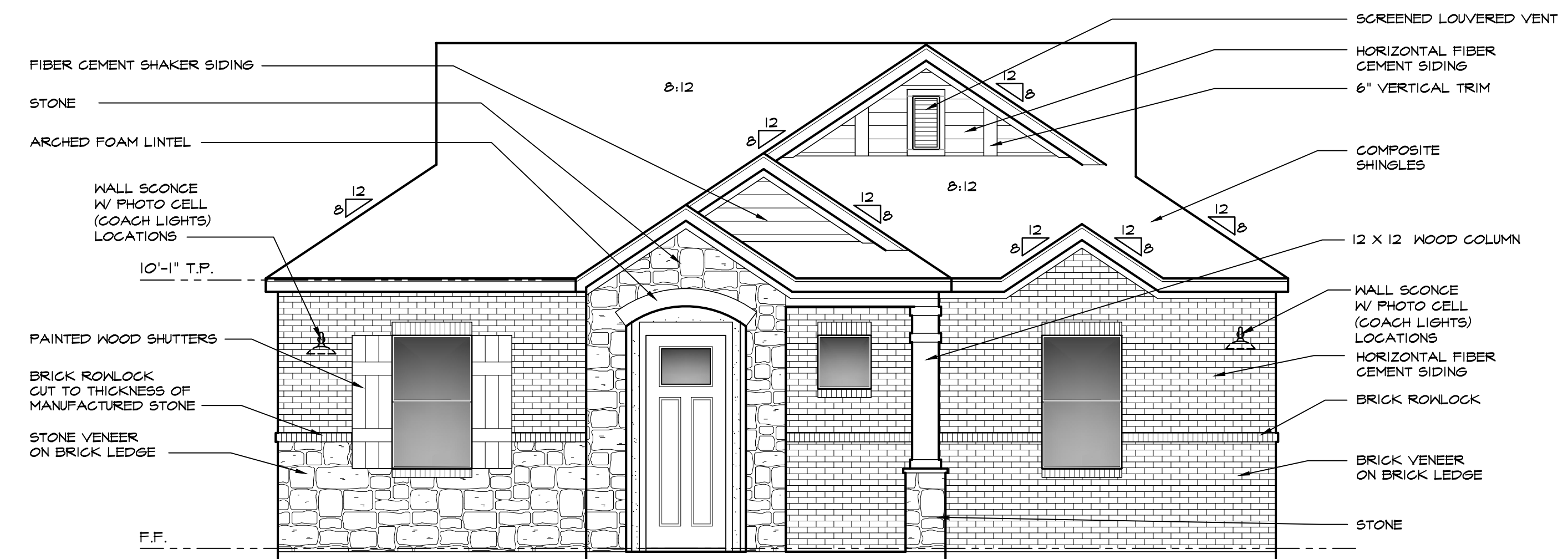
02 FRONT ELEVATION (ALTERNATE)

FRONT PORCH 74 S.F.

SCALE: 1/4"=1'-0"

<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>9% FIBER CEMENT SIDING 91% BRICK AND STONE 83% BRICK AND 8% STONE</p>	<p>RIGHT ELEVATION</p> <p>9% FIBER CEMENT SIDING 91% BRICK AND STONE 89% BRICK AND 2% STONE</p>	<p>FRONT ELEVATION</p> <p>4% FIBER CEMENT SIDING 96% BRICK AND STONE (65% BRICK, 35% STONE)</p>
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STANDARD ROOF PITCH



01 FRONT ELEVATION (STANDARD)

FRONT PORCH 74 S.F.

SCALE: 1/4"=1'-0"

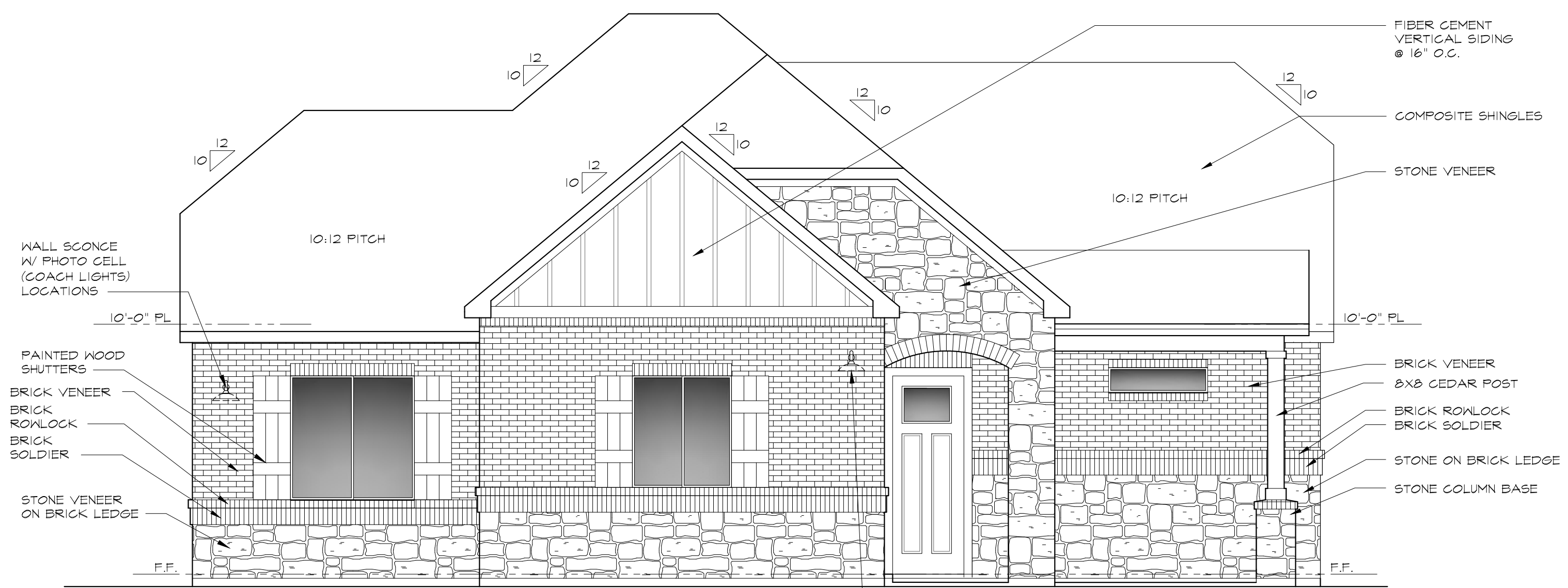
BUILDING TYPE 2C ELEVATIONS

SCALE: 1/4" = 1'-0"

<p>ARCHITECT IKEMIRE ARCHITECTS 16660 Dallas Parkway, Suite 2900 Dallas TX 75001 PH 972-248-2486 FX 972-248-1557 CONTACT: JAMIE HULSE</p>	<p>DEVELOPER NEXMETRO COMMUNITIES THREE ENERGY SQUARE 6688 N. CENTRAL EXPRESSWAY, SUITE 275 Addison TX 75206 PH 972-795-0039 CONTACT: JASON FLORY</p>	<p>NEXMETRO AVILLA FAIRWAYS BUILDING ELEVATIONS</p>
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<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>11% FIBER CEMENT SIDING 49% BRICK 40% STONE</p>	<p>RIGHT ELEVATION</p> <p>15% FIBER CEMENT SIDING 45% BRICK 40% STONE</p>	<p>FRONT ELEVATION</p> <p>12% FIBER CEMENT SIDING 40% BRICK 48% STONE</p>
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ALTERNATE ROOF PITCH

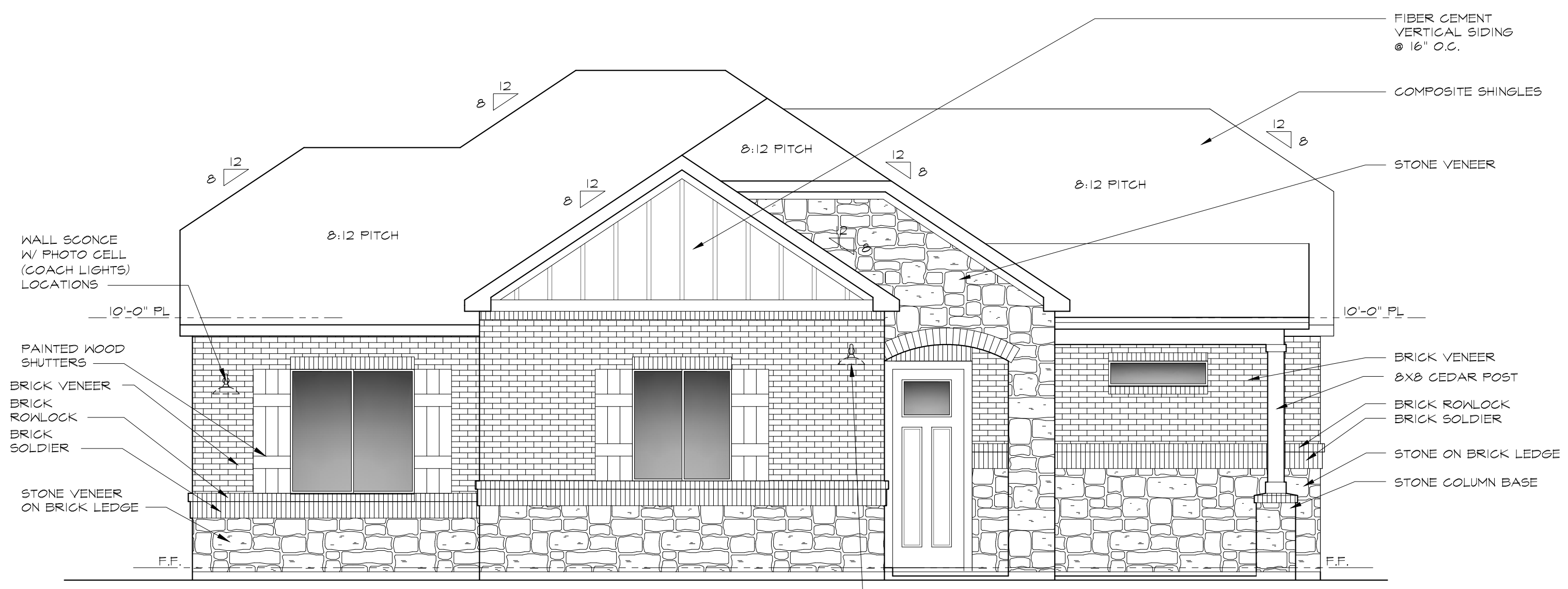


02 FRONT ELEVATION (ALTERNATE)

SCALE: 1/4"=1'-0"

<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>10% FIBER CEMENT SIDING 47% BRICK 43% STONE</p>	<p>RIGHT ELEVATION</p> <p>20% FIBER CEMENT SIDING 45% BRICK 35% STONE</p>	<p>FRONT ELEVATION</p> <p>09% FIBER CEMENT SIDING 47% BRICK 44% STONE</p>
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STANDARD ROOF PITCH



01 FRONT ELEVATION (STANDARD)

SCALE: 1/4"=1'-0"

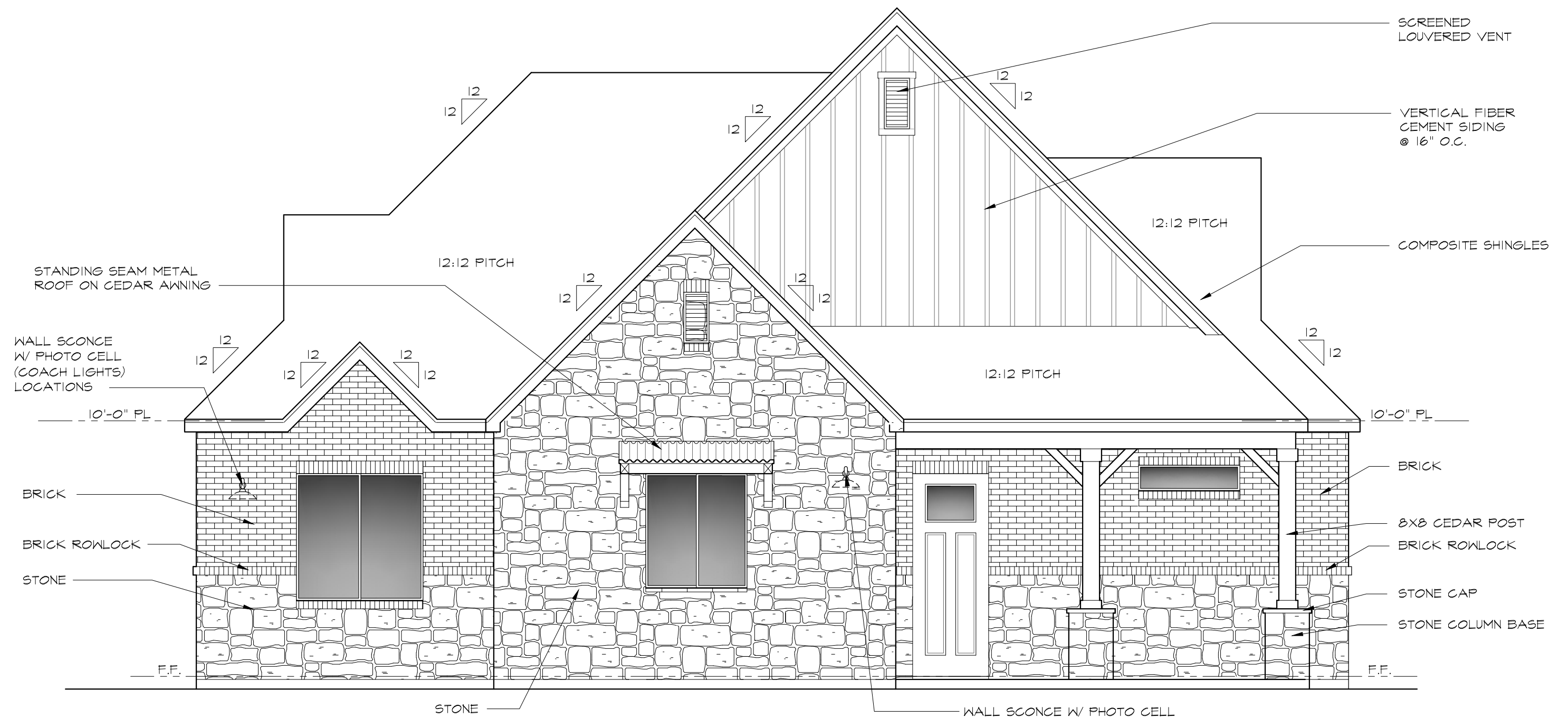
BUILDING TYPE 3A ELEVATIONS

SCALE: 1/4" = 1'-0"

<p>ARCHITECT</p> <p>IKEMIRE ARCHITECTS 16660 Dallas Parkway, Suite 2900 Dallas TX 75001 PH 972-248-2486 FX 972-248-1557 CONTACT: JAMIE HULSE</p>	<p>DEVELOPER</p> <p>NEXMETRO COMMUNITIES THREE ENERGY SQUARE 6688 N. CENTRAL EXPRESSWAY, SUITE 275 Addison TX 75206 PH 972-745-0039 CONTACT: JASON FLORY</p>	<p>NEXMETRO</p> <p>AVILLA FAIRWAYS</p> <p>BUILDING ELEVATIONS</p>
--	--	---

<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>7% FIBER CEMENT SIDING 42% BRICK 45% STONE</p>	<p>RIGHT ELEVATION</p> <p>7% FIBER CEMENT SIDING 42% BRICK 45% STONE</p>	<p>FRONT ELEVATION</p> <p>11% FIBER CEMENT SIDING 49% BRICK 40% STONE</p>
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ALTERNATE ROOF PITCH

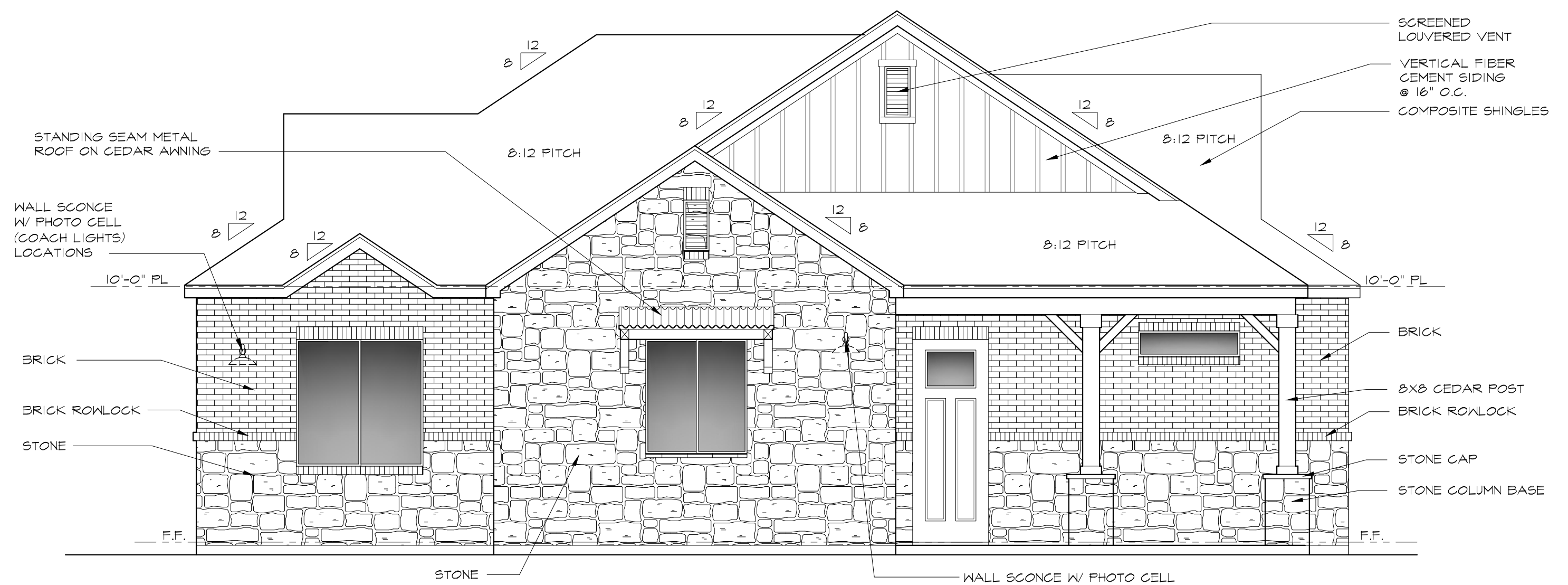


02 FRONT ELEVATION (ALTERNATE)

SCALE: 1/4"=1'-0"

<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>5% FIBER CEMENT SIDING 55% BRICK 40% STONE</p>	<p>RIGHT ELEVATION</p> <p>5% FIBER CEMENT SIDING 50% BRICK 45% STONE</p>	<p>FRONT ELEVATION</p> <p>10% FIBER CEMENT SIDING 43% BRICK 47% STONE</p>
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STANDARD ROOF PITCH



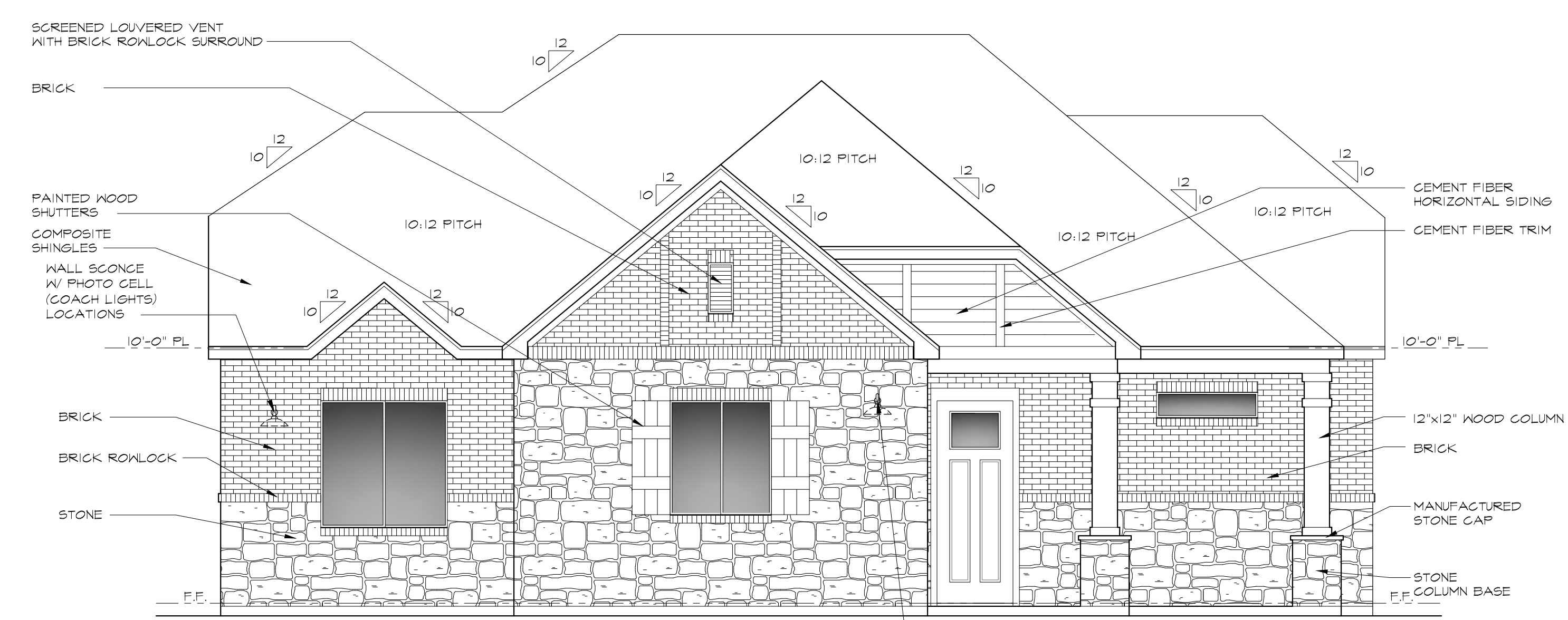
01 FRONT ELEVATION (STANDARD)

SCALE: 1/4"=1'-0"

BUILDING TYPE 3B ELEVATIONS

SCALE: 1/4" = 1'-0"

<p>ARCHITECT IKEMIRE ARCHITECTS 16660 Dallas Parkway, Suite 2900 Dallas TX 75001 PH 972-248-2486 FX 972-248-1557 CONTACT: JAMIE HULSE</p>	<p>DEVELOPER NEXMETRO COMMUNITIES THREE ENERGY SQUARE 6688 N. CENTRAL EXPRESSWAY, SUITE 275 Addison TX 75206 PH 972-745-0039 CONTACT: JASON FLORY</p>	<p>NEXMETRO AVILLA FAIRWAYS BUILDING ELEVATIONS</p>
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02 FRONT ELEVATION (ALTERNATE)

SCALE: 1/4"=1'-0"

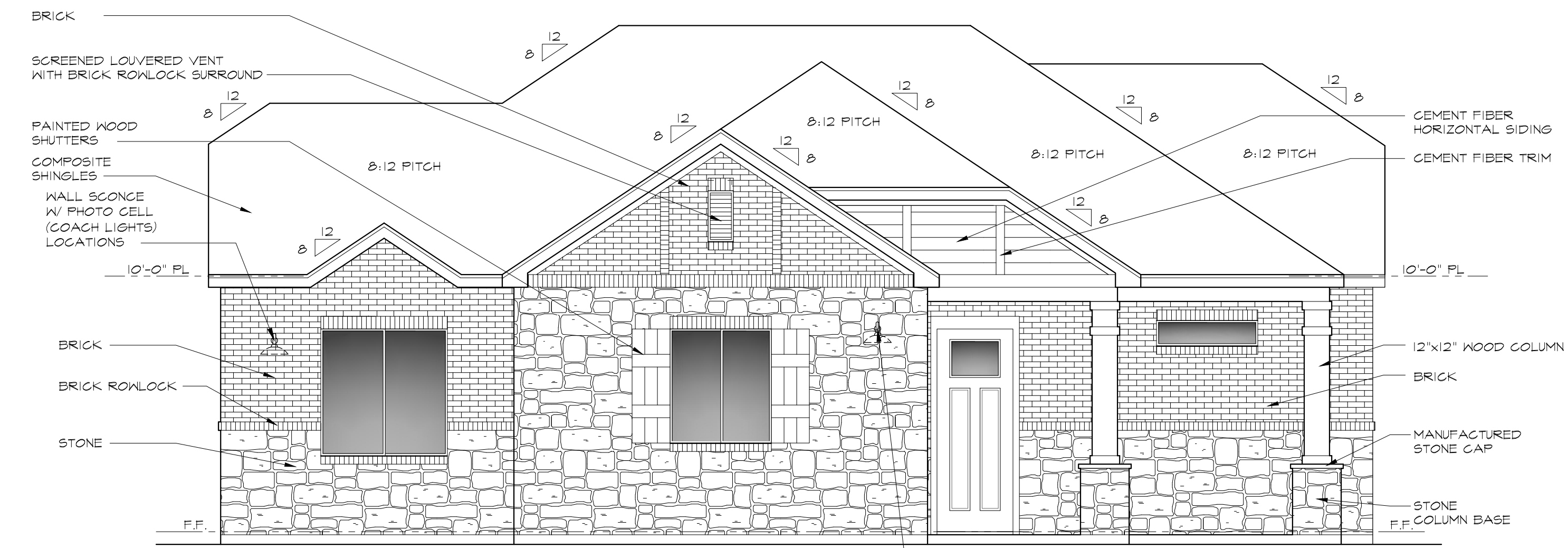
REAR ELEVATION
0% FIBER CEMENT SIDING
100% BRICK
0% BRICK

LEFT ELEVATION
7% FIBER CEMENT SIDING
44% BRICK
44% STONE

RIGHT ELEVATION
7% FIBER CEMENT SIDING
44% BRICK
44% STONE

FRONT ELEVATION
9% FIBER CEMENT SIDING
41% BRICK
50% STONE

ALTERNATE ROOF PITCH



01 FRONT ELEVATION (STANDARD)

SCALE: 1/4"=1'-0"

REAR ELEVATION
0% FIBER CEMENT SIDING
100% BRICK
0% STONE

LEFT ELEVATION
5% FIBER CEMENT SIDING
52% BRICK
43% STONE

RIGHT ELEVATION
5% FIBER CEMENT SIDING
52% BRICK
43% STONE

FRONT ELEVATION
7% FIBER CEMENT SIDING
57% BRICK
36% STONE

STANDARD ROOF PITCH

BUILDING TYPE 3C ELEVATIONS

SCALE: 1/4" = 1'-0"

<p>ARCHITECT IKEMIRE ARCHITECTS 16660 Dallas Parkway, Suite 2900 Dallas TX 75001 PH 972-248-2486 FX 972-248-1557 CONTACT: JAMIE HULSE</p>	<p>DEVELOPER NEXMETRO COMMUNITIES THREE ENERGY SQUARE 6688 N. CENTRAL EXPRESSWAY, SUITE 275 Addison TX 75206 PH 972-745-0039 CONTACT: JASON FLORY</p>	<p>NEXMETRO AVILLA FAIRWAYS BUILDING ELEVATIONS</p>
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APPENDIX B

TRAFFIC IMPACT ANALYSIS FOR

AVILLA FAIRWAYS MULTIFAMILY

DEVELOPMENT



Technical Memorandum

To: Mr. Josh Hartmann
NEXmetro Communities

From: Kimley-Horn and Associates, Inc.

Date: February 18, 2014

Re: NEXmetro Neighborhoods - Residential Parking Demand Study

Introduction

The NEXmetro neighborhood concept is a hybrid housing option of high-end, single-story, detached and duplex rental residences in gated neighborhoods. Compared to traditional multifamily rental developments, the NEXmetro neighborhoods tend to attract an older, higher-income population. The traffic and parking demands of the NEXmetro residents may be closer to that of a mix of traditional single-family detached housing and age-restricted senior housing.

Municipalities have a variety of standards for parking supply minimums or maximums for land uses. Most of the standards would not contain an exact match for the NEXmetro residential neighborhood, so they risk providing too much or too little parking for efficient use of the site. This parking demand study will identify the actual parking demand in established NEXmetro neighborhoods in order to provide guidance for development of future neighborhoods.

NEXmetro Neighborhood Data Collection

The parking occupancy was recorded at the following NEXmetro neighborhoods by site staff or KHA data collectors:

- Avilla Marana One (4050 W. Aerie Drive, Marana, AZ)
- Avilla River (1000 W. River Road, Tucson, AZ)
- Avilla Preserve (2501 W. Orange Grove Road, Tucson, AZ)

Table 1 shows the units and bedroom counts for the neighborhoods at the times of the highest parking demand. The number of occupied units varied slightly through the data collection period, so all parking calculations are performed using the conditions during the highest observed parking demand.

The site staff made observations at 10 AM, 6 PM, 9 PM, and 4 AM on a weekday and a Saturday/Sunday. KHA observations were overnight occupancy counts for a weekday. The counts included visitor parking.



Table 1. NEXmetro Neighborhood Unit Makeup

NEXmetro Neighborhood	Occupied Units	1BR Units	2BR Units	3BR Units	Total Occupied Bedrooms	Garage Spaces Leased	Observation Dates
Marana One	157	31	73	53	336	26	1/15, 1/18, 2/5
River	50	16	17	17	101	6	1/15, 1/18, 2/5, 2/10
Preserve	43	11	17	15	90	12	1/15, 1/18, 2/5, 2/10
Totals:	250	58	107	85	527	44	

Each NEXmetro neighborhood includes covered garages which are available for rent separately from the basic unit rate. As reserved spaces which are not available to any user, these garage spaces are assumed to be occupied at all times. The number of leased garage spaces was provided for each site by the site staff, with a weighted average of 0.18 garage leases per unit, and 0.08 garage leases per bedroom. When garage spaces are not included in occupancy numbers, the resulting occupancy of the general spaces is called “surface” parking.

NEXmetro Neighborhood Time-of-Day Parking Observations

Exhibit 1 shows the surface parking occupancy counts at each location, and notes the number of additional garage spaces which are also considered occupied. This data is from the weekday and weekend counts with four observations per day.

The daytime data shows that the neighborhoods exhibit a typical residential parking occupancy trend, with low occupancy during the weekday daytime, rising occupancy throughout the evening, and an overnight peak between midnight and 6 AM. The weekend data is similar, with the Marana and River neighborhoods showing the expected higher occupancy during the Saturday morning count.

NEXmetro Neighborhood Parking Demand Calculations

Comparing the unit and bedroom count for each neighborhood with the parking occupancy data results in the demand calculations shown in **Table 2**. The maximum surface parking spaces occupied for each neighborhood is the highest individual observation from the data set.

The demand is calculated to find the surface space demand per unit and per bedroom. A second set of calculations shows the total demand (surface plus garage spaces) per unit and per bedroom.

A weighted average of parking demands across all the neighborhoods was also calculated.

Table 2. NEXmetro Neighborhood Parking Demand

NEXmetro Neighborhood	Occupied Units	Occupied Bedrooms	Maximum Observed Surface Parking Spaces Occupied <i>(Date)</i>	Garage Spaces Occupied	Surface + Garage Spaces Occupied	Surface Parking Space Demand Per Unit	Surface Parking Space Demand Per Bedroom	Surface + Garage Parking Space Demand Per Unit	Surface + Garage Parking Space Demand Per Bedroom
Marana One	157	336	229 <i>(1/15)</i>	26	255	1.46	0.68	1.62	0.76
River	50	101	63 <i>(2/5)</i>	6	69	1.26	0.62	1.38	0.68
Preserve	43	90	59 <i>(2/10)</i>	12	71	1.37	0.66	1.65	0.79
Totals / Weighted Avg:	250	527	351	44	395	1.40	0.67	1.58	0.75

The parking demand per bedroom is relatively consistent between the neighborhoods, with a weighted average demand of 0.67 surface spaces per bedroom, and 0.75 total spaces per bedroom.

The parking demand per unit is less consistent across the neighborhoods, probably due to the difference in unit mix for each neighborhood. The River neighborhood has a noticeably lower parking demand per unit than the others, which seems to correspond with its higher percentage of 1-bedroom units.

Parking Demand Comparisons to Traditional Multifamily

Two published sources of national research on parking demand rates are *Shared Parking, 2nd Edition* by the Urban Land Institute (ULI) and *Parking Generation, 4th Edition* by the Institute of Transportation Engineers (ITE). **Table 3** shows the typical parking demand from each resource. For the ITE parking demand, the 85th-percentile of observed demands is often taken as a design value.

Table 3. Parking Demand Comparison

Land Use	Unit of Measure	Peak Parking Demand
ULI <i>Shared Parking</i> Residential, Rental	Unit	1.65
ULI <i>Shared Parking</i> Residential, Owned	Unit	1.85
ITE <i>Parking Generation</i> Low/Mid-Rise Apartment Suburban, Weekday	Unit	1.23 Average 1.94 85th-Percentile
NEXmetro Observed Weighted Average	Unit	1.40 Surface Spaces 1.58 Total Spaces
NEXmetro Observed Maximums at any site	Unit	1.46 Surface Spaces 1.65 Total Spaces

A comparison of the NEXmetro observations using per-unit demands shows that the NEXmetro neighborhoods have parking demands similar to or slightly less than the national reference data for multifamily uses.

NEXmetro Neighborhood Parking Supply Recommendation

Based on a conservative analysis of the parking demand data collected at established neighborhoods, future NEXmetro neighborhoods should have a minimum parking supply set using the following rates:

- **0.90 total parking spaces per bedroom** (surface spaces plus garage spaces, including visitors)

A typical division between surface and garage spaces would be the following minimums:

- **0.75 surface parking spaces per bedroom**
- **0.15 garage parking spaces per bedroom**

The recommended rates include an approximately 10% vacancy rate to improve perceived parking efficiency and quality of life factors within the site. The number of surface spaces needed per bedroom has very little variance between neighborhoods in the observation, showing it is the preferred accounting method for the parking supply. The garage leasing behavior may vary more significantly between sites based on climate and other factors.

If the parking supply is to be calculated per unit, the following minimum rates should be used:

- **1.85 total parking spaces per unit** (surface spaces plus garage spaces, including visitors), with the typical division being:
 - **1.55 surface parking spaces per unit**
 - **0.30 garage parking spaces per unit**

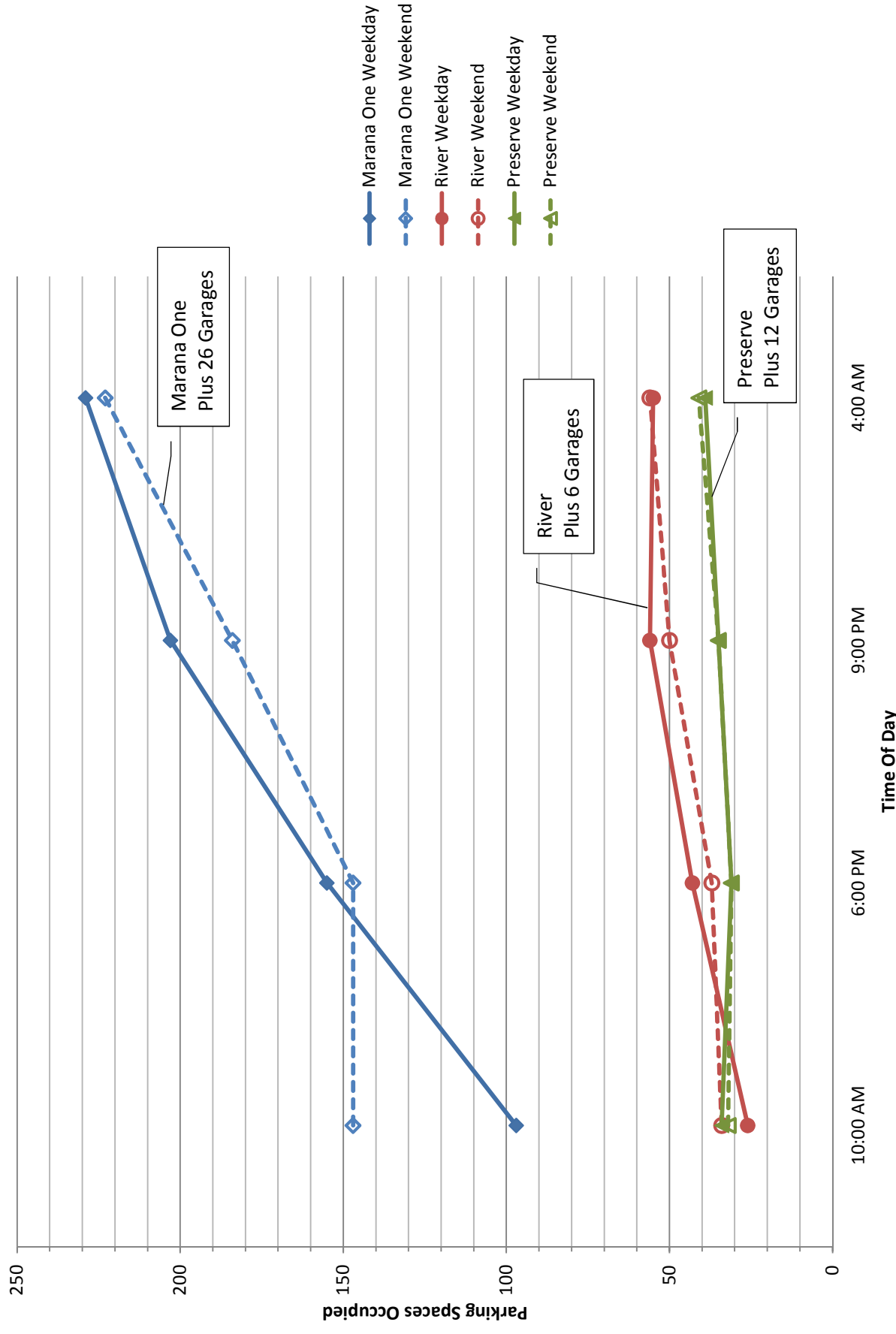
Due to the variability in unit mix at each site, the parking demand per unit is not as certain as using the per bedroom rates. However, the recommended rates per bedroom and per unit are internally consistent for the average 2.1 bedrooms per unit mix at the observed neighborhoods.

As other NEXmetro neighborhoods are completed, continued parking occupancy observations should be made in order to broaden the data set and refine the parking supply recommendations.

END

Attachments: Exhibit 1 - NEXmetro Surface Parking Time-of-Day Observations
NEXmetro Parking Occupancy Observations

Exhibit 1 - NEXmetro Surface Parking Time-of-Day Observations



Parking Occupancy Observations

Site Staff Observations:

Marana One

Date	Day	Time	# of cars
1/15/2014	Wed	10:00 AM	97
1/15/2014	Wed	6:00 PM	155
1/15/2014	Wed	9:00 PM	203
1/16/2014	Thurs	4:00 AM	229
1/18/2014	Sat	10:00 AM	147
1/18/2014	Sat	6:00 PM	147
1/18/2014	Sat	9:00 PM	184
1/19/2014	Sun	4:00 AM	223

Total of 166 units- 157 occupied
26 garages leased

KHA Observations:

Marana One

Date	Day	Time	# of cars
2/5/2014	Wed	Overnight	224

Total of 166 units- 157 occupied

River

Date	Day	Time	# of cars
1/15/2014	Wed	10:00 AM	26
1/15/2014	Wed	6:00 PM	43
1/15/2014	Wed	9:00 PM	56
1/16/2014	Thurs	4:00 AM	55
1/18/2014	Sat	10:00 AM	34
1/18/2014	Sat	6:00 PM	37
1/18/2014	Sat	9:00 PM	50
1/19/2014	Sun	4:00 AM	56

Total of 76 units- 50 occupied
6 garages leased

River

Date	Day	Time	# of cars
2/5/2014	Wed	Overnight	63

Total of 76 units- 50 occupied

Preserve

Date	Day	Time	# of cars
1/15/2014	Wed	10:00 AM	34
1/15/2014	Wed	6:00 PM	31
1/15/2014	Wed	9:00 PM	35
1/16/2014	Thurs	4:00 AM	39
1/18/2014	Sat	10:00 AM	32
1/18/2014	Sat	6:00 PM	31
1/18/2014	Sat	9:00 PM	35
1/19/2014	Sun	4:00 AM	41

Total of 184 units (96 released)- 33 occupied
12 garages leased

Preserve

Date	Day	Time	# of cars
2/5/2014	Wed	Overnight	57

Total of 184 units - 41 occupied

2/10/2014	Mon	Overnight	59
-----------	-----	-----------	----

Total of 184 units - 43 occupied

2/10/2014	Mon	Overnight	61
-----------	-----	-----------	----

Total of 76 units- 50 occupied

TRAFFIC IMPACT ANALYSIS FOR AVILLA FAIRWAYS MULTIFAMILY DEVELOPMENT

OAKMONT DRIVE & LAKE SHARON DRIVE CORINTH, TEXAS

Prepared for:

The City of Corinth
3300 Corinth Parkway
Corinth, Texas 76208

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June 2021



John P. Denholm III
Digitally signed
on 6/11/2021

T1458.06

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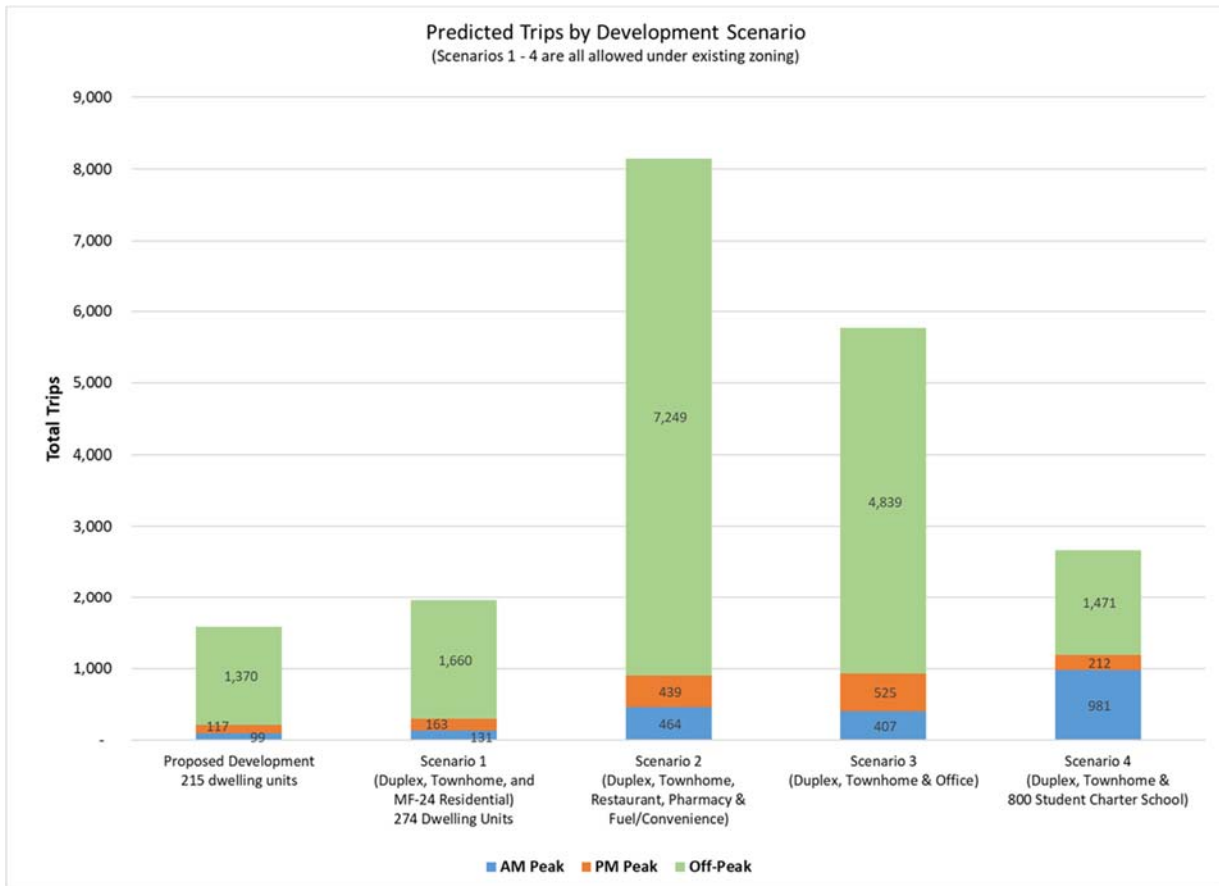
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EXECUTIVE SUMMARY

The proposed Avilla Fairways development will be located on the northwest corner of the intersection of Lake Sharon Drive and Oakmont Drive in Corinth, Texas. Based on the preliminary site plan, site access points will include two (2) full-access driveways on Lake Sharon Drive and one (1) full-access driveway on Oakmont Drive. One of the access points on Lake Sharon Drive is the proposed extension of Rye Road, which will provide the neighborhood to the north with a second access point. Two additional access scenarios were analyzed, including only providing access on Lake Sharon Drive (Alternative 1) or requiring Rye Road to operate as a one-way southbound street (Alternative 2).

The proposed development is predicted to generate approximately 1,586 daily trips, 99 trips during the AM peak hour and 117 trips during the PM peak hour. Estimates of other allowable land use possibilities under existing zoning indicate that the site could generate as many as 8,195 daily trips, 979 trips in the AM peak hour, and 517 trips in the PM peak hour. The number of trips generated by the Avilla Fairways development is significantly lower than the number of trips that could be generated under existing zoning, including the maximum residential density allowed under existing zoning. Thus, the proposed development is a significantly less intense traffic generator than others allowed under existing zoning.



While all three site access options are predicted to operate with minimal impact on the surrounding roadway network, the access shown in the proposed site plan results in the least amount of average delay and the shortest queue lengths for most movements. Providing three full-access driveways will result in the least amount of new traffic at the intersection of Lake Sharon Drive and Oakmont Drive. Providing three full-access points as proposed in the site plan will provide the flexibility for traffic circulation, avoids concentrating all development traffic at a single location, and provides greater ingress /egress for better emergency response access to the site. The neighborhood to the north also gains improved emergency access and overall traffic circulation via the new access via Rye Road. It is recommended to consider providing the greatest amount of site access, as shown in the proposed site plan.

Based on the preliminary site plan (Figure 2), an eastbound left turn lane is proposed on Lake Sharon Drive at the Rye Road extension and is anticipated to include adequate vehicle storage. If feasible, it is recommended to also install an eastbound left turn lane on Lake Sharon Drive at the West Driveway, which will be located at an existing median opening on a divided roadway. However, if the culvert crossing precludes construction of an eastbound left-turn lane, consideration should be given to making the western driveway right-in/right-out and lengthening the storage at the Rye Road access point. No other auxiliary lanes were recommended. It should also be noted that sight distance from the East Driveway on Oakmont Drive is less than desired looking to the right due to the horizontal curvature of Oakmont Drive and vegetation. With development of the site, is recommended to remove all vegetation along the west edge of Oakmont Drive within the sight triangle south of East Driveway.

Overall, the existing and planned roadway network is anticipated to fully accommodate the site traffic volumes generated by the proposed Avilla Fairways development.

INTRODUCTION

This traffic study was conducted to analyze the traffic impacts associated with the proposed Avilla Fairways multifamily development in Corinth, Texas located on the northwest corner of the intersection of Lake Sharon Drive and Oakmont Drive. Additionally, this study was conducted to analyze existing traffic operations in the study area along with the impacts to the adjacent residential neighborhood with provision of a second access point. An aerial image showing the location of the site and the count locations is shown in **Figure 1**.

The proposed Avilla Fairways multifamily development includes 215 dwelling units and was assumed to be built-out by 2023. The preliminary site plan for the development is provided in **Figure 2**, with a larger version in the Appendix. The site proposes the following access points:

- Two (2) full-access site driveways on Lake Sharon Drive, including a proposed extension of Rye Road from the adjacent neighborhood; and
- One (1) full-access site driveway on Oakmont Drive.

Two additional scenarios were also analyzed in addition to the proposed site plan:

- Alternative 1 – Only the two access points on Lake Sharon Drive
- Alternative 2 – Rye Road operates as a one-way (outbound) roadway

The following existing study intersections are included:

- Lake Sharon Drive and Oakmont Drive;
- Oakmont Drive and Ardglass Trail; and
- Oakmont Drive and Creekside Drive.

The following elements were included in this study, based on discussion with the City of Corinth:

Data Collection

- Collected existing AM and PM peak hour traffic volumes at the three (3) existing intersections identified above.
- Obtained historical traffic volumes in the vicinity of the proposed development.

Traffic Analysis

- Assessed the general accessibility of the site.
- Estimated the number of trips that will be generated by the proposed new development and by potential developments allowed by existing zoning.
- Estimated the directional distribution of traffic approaching / departing the proposed development.
- Assigned the estimated traffic to the street network.
- Performed capacity analysis for the critical intersections.
- Performed capacity analysis for roadways adjacent to the site.

- Analyzed the impact of the development on the area roadways for the proposed site plan and the alternate scenarios.

Recommendations

- Determined if any roadway improvements are needed to accommodate existing traffic, projected background traffic in the build-out year, and projected site traffic generated by the proposed development.
- Determine preferred alternative between the three site access options.

Figure 1. Vicinity Map of the Study Area



LEGEND

- PROPOSED METAL FENCE
- PROPOSED WOOD FENCE
- PROPOSED FIRELANE (PRIVATE PAVEMENT)
- PROPOSED ROADWAY (PUBLIC PAVEMENT)
- PROPOSED PUBLIC TRAIL/SIDEWALK
- PROPOSED PRIVATE TRAIL/SIDEWALK

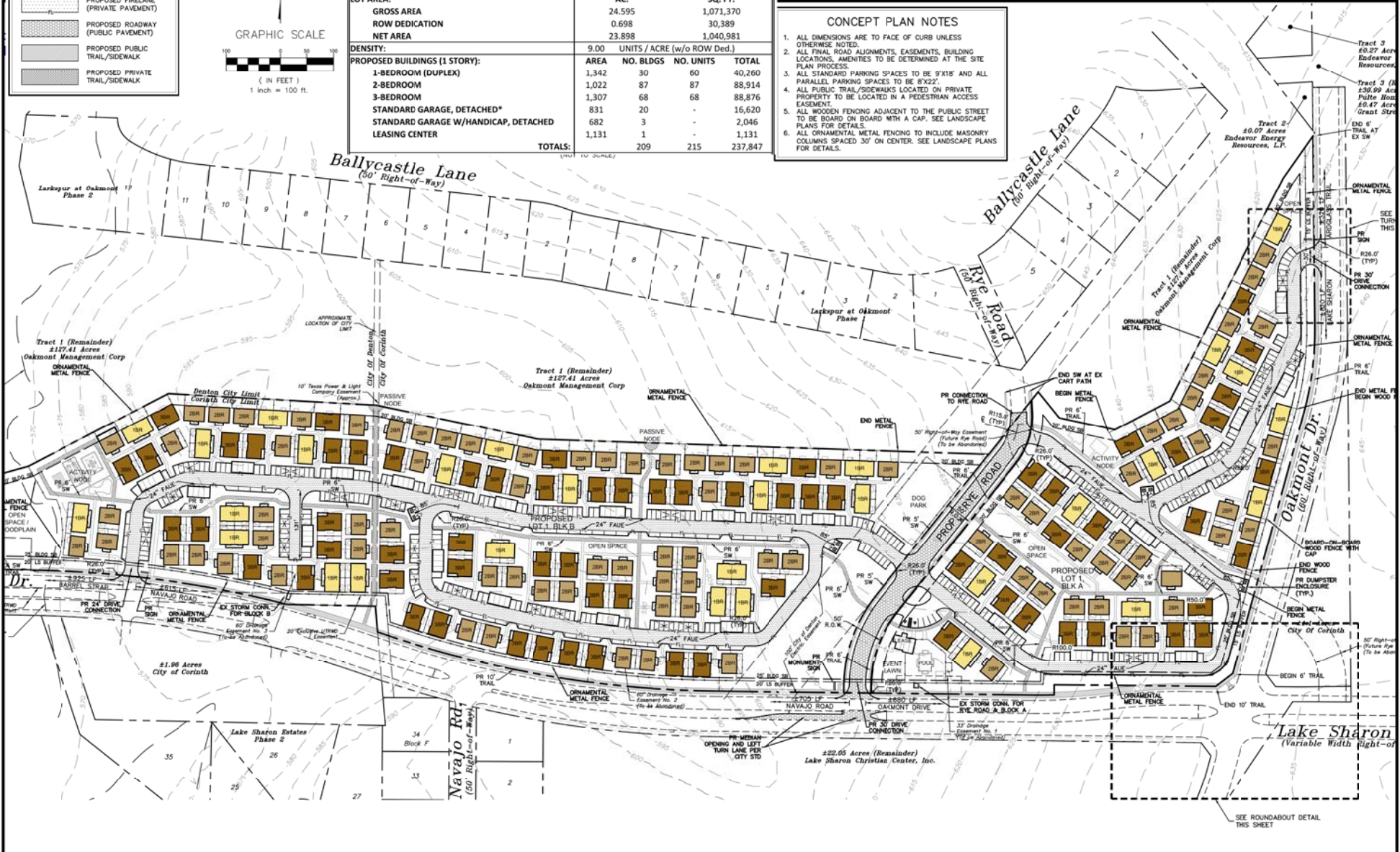
GRAPHIC SCALE

(IN FEET)

1 inch = 100 ft.

SITE DATA SUMMARY TABLE			
EXISTING ZONING/USE:	PD-6, PD-24 / VACANT		
PROPOSED ZONING:	PD W/MF-1 BASE ZONING		
PROPOSED USE:	RESIDENTIAL COTTAGE COMMUNITY		
NUMBER OF LOTS:	2 (EXISTING); 2 (PROPOSED)		
LOT COVERAGE:	22.8%		
LOT AREA:	AC.	SQ. FT.	
GROSS AREA	24.595	1,071,370	
ROW DEDICATION	0.698	30,389	
NET AREA	23.898	1,040,981	
DENSITY:	9.00	UNITS / ACRE (w/o ROW Ded.)	
PROPOSED BUILDINGS (1 STORY):	AREA	NO. BLDGS	NO. UNITS
1-BEDROOM (DUPEX)	1,342	30	60
2-BEDROOM	1,022	87	87
3-BEDROOM	1,307	68	68
STANDARD GARAGE, DETACHED*	831	20	-
STANDARD GARAGE W/HANDICAP, DETACHED	682	3	-
LEASING CENTER	1,131	1	-
TOTALS:	209	215	237,847

- CONCEPT PLAN NOTES**
1. ALL DIMENSIONS ARE TO FACE OF CURB UNLESS OTHERWISE NOTED.
 2. ALL FINAL ROAD ALIGNMENTS, EASEMENTS, BUILDING LOCATIONS, AMENITIES TO BE DETERMINED AT THE SITE PLAN PROCESS.
 3. ALL STANDARD PARKING SPACES TO BE 9'X18' AND ALL PARALLEL PARKING SPACES TO BE 8'X22'.
 4. ALL PUBLIC TRAIL/SIDEWALKS LOCATED ON PRIVATE PROPERTY TO BE LOCATED IN A PEDESTRIAN ACCESS EASEMENT.
 5. ALL WOODEN FENCING ADJACENT TO THE PUBLIC STREET TO BE BOARD ON BOARD WITH A CAP. SEE LANDSCAPE PLANS FOR DETAILS.
 6. ALL ORNAMENTAL METAL FENCING TO INCLUDE MASONRY COLUMNS SPACED 30' ON CENTER. SEE LANDSCAPE PLANS FOR DETAILS.



3030 LBJ FREEWAY
SUITE 1660
DALLAS, TEXAS 75234
972-248-3006

Preliminary Site Plan

Figure 2

SITE ACCESSIBILITY

Site accessibility describes the ease with which vehicles can get to and from a development. A site's accessibility is affected by the geographical location of the development with respect to other activity areas, the roadway system, and physical restraints such as rivers or lakes.

The proposed Avilla Fairways will be located on the northwest corner of the intersection of Lake Sharon Drive and Oakmont Drive in Corinth, Texas. A brief description of the major area roadways is provided below:

Lake Sharon Drive – Lake Sharon Drive is a four-lane divided eastbound-westbound roadway with a 32-foot median and a posted speed limit of 40 miles per hour (mph) in the vicinity of the site. Lake Sharon Drive is classified as a Minor Arterial in the City of Corinth *Master Thoroughfare Plan* (dated March 30, 2021). Lake Sharon Drive has recently been extended west of Oakmont Drive to FM 2499.

Lake Sharon Drive borders the southern edge of the development. The proposed development will have one full-access driveway on Lake Sharon Drive at an existing median opening near the western edge of the site. Additionally, Rye Road is proposed to be extended through the development to provide an additional full-access point on Lake Sharon Drive.

Oakmont Drive – Oakmont Drive is a two-lane undivided northbound-southbound roadway approximately 36-feet wide and with a posted speed limit of 30 mph in the vicinity of the site. Oakmont Drive is classified as a Collector in the City of Corinth *Master Thoroughfare Plan* (dated March 30, 2021).

Oakmont Drive borders the eastern edge of the development. The proposed development will have one full-access driveway on Oakmont Drive, based on the currently proposed site plan. One of the alternative access scenarios will remove this driveway.

The existing intersection of Oakmont Drive and Lake Sharon Drive operates with multiway stop control, with stop signs on all four approaches.

Creekside Drive – Creekside Drive is a two-lane undivided eastbound-westbound roadway with a posted speed limit of 25 mph. Creekside Drive is approximately 24 feet wide west of Oakmont Drive and approximately 40 feet wide east of Oakmont Drive adjacent to the schools. Creekside Drive is classified as a Collector in the City of Corinth *Master Thoroughfare Plan* (dated March 30, 2021). Creekside Drive currently terminates at Post Oak Drive but may be extended to the east in the future, based on the *Master Thoroughfare Plan*.

The existing intersection of Oakmont Drive and Creekside Drive operates with multiway stop control, with stop signs on all four approaches.

Ardglass Trail – Ardglass Trail is a two-lane undivided eastbound-westbound roadway approximately 24-feet wide. There is no posted speed limit. Ardglass Trail is classified as a local roadway in the City of Corinth *Master Thoroughfare Plan* (dated March 30, 2021). Ardglass Trail is currently the only access point for the neighborhood north of the proposed site, but the neighborhood will have an additional access point if Rye Road is extended.

The existing intersection of Oakmont Drive and Ardglass Trail operates with two-way stop control, with a stop sign on the eastbound Ardglass Trail approach. There is no westbound approach at this intersection.

Rye Road – Rye Road is a two-lane undivided northbound-southbound roadway approximately 24-feet wide. There is no posted speed limit. Rye Road is classified as a local roadway in the City of Corinth *Master Thoroughfare Plan* (dated March 30, 2021). Currently, Rye Road terminates approximately 300 feet south of Ballycastle Lane, but is proposed to be extended to Lake Sharon Drive with the development.

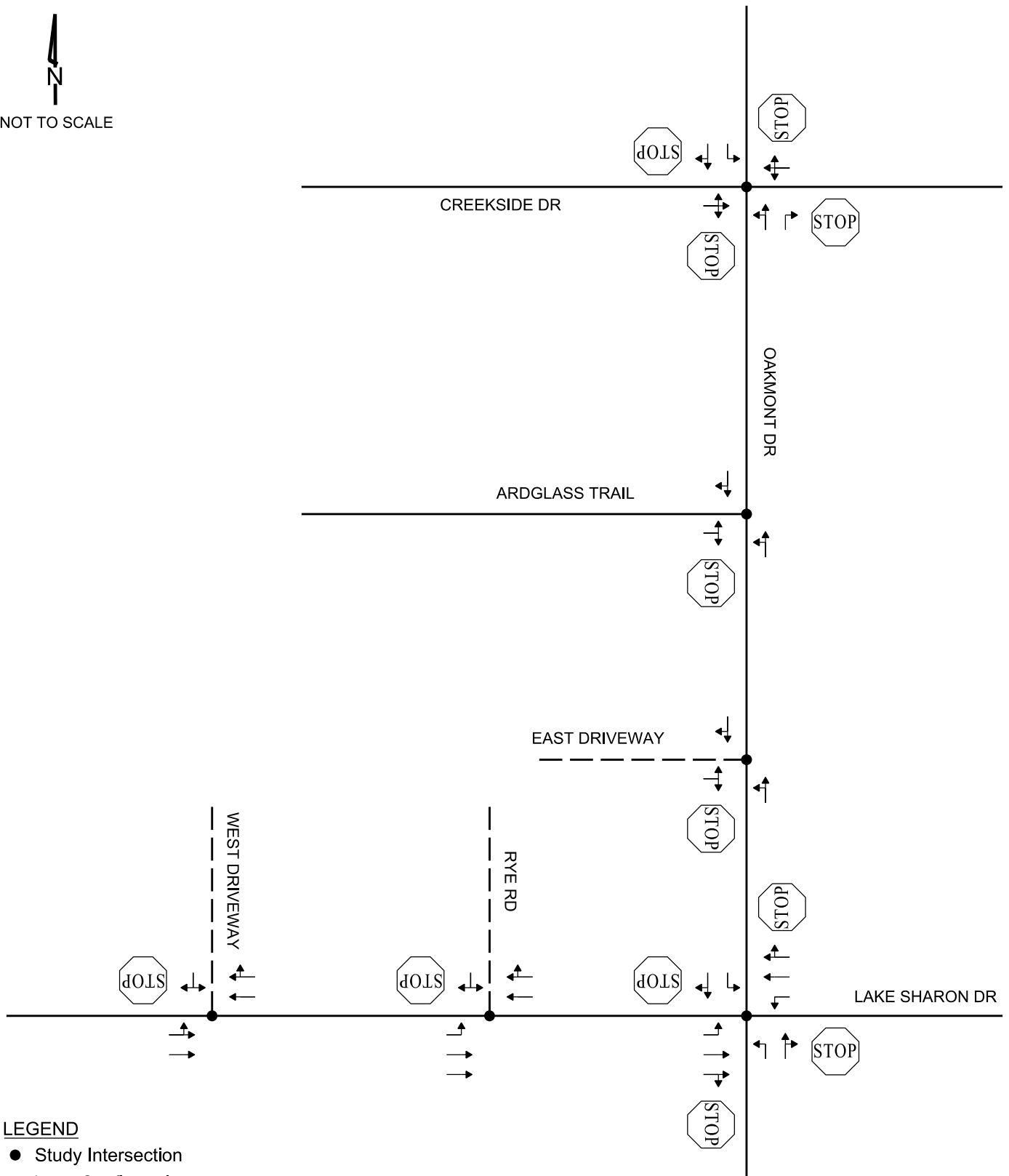
The existing intersection lane configurations in the study area are shown in **Figure 3**, along with the existing traffic control at the study intersections. Additionally, the assumed driveway lane configurations based on the site plan (Figure 2) are shown.

Existing transportation modes in the study area are primarily vehicular traffic. There are existing sidewalk facilities along all of the study roadways. There are also marked crosswalks on all approaches at the two multiway stop intersections and along the stop-controlled approach of Ardglass Trail.

As shown in Figure 1, Hawk Elementary School is located approximately ¼-mile north of the site on the southeast corner of the intersection of Oakmont Drive and Creekside Drive. Additionally, Crownover Middle School is located on the east side of the elementary school south of Creekside Drive.



NOT TO SCALE



LEGEND

- Study Intersection
- Lane Configuration
- - - Proposed Driveway / Roadway
- STOP Stop-Controlled Approach

PROPOSED DEVELOPMENT

The site is on the northwest corner of Lake Sharon Drive and Oakmont Drive in Corinth, Texas. The site is undeveloped at this time and is zoned as a Planned Development (PD-6) in the City of Corinth *Zoning Map* dated April 2021. The proposed Avilla Fairways development will include 215 multifamily dwelling units and is predicted to be built by 2023.

The site proposes the following access points:

- Two (2) full-access site driveways on Lake Sharon Drive (including the proposed Rye Road extension); and
- One (1) full-access driveway on Oakmont Drive.

Proposed Trip Generation

The number of trips generated by the development is a function of the type and quantity of land use. The number of vehicle trips generated by the development was estimated based on the trip generation rates and equations provided in the publication entitled *Trip Generation Manual, Tenth Edition*, by the Institute of Transportation Engineers (ITE). Estimates of the number of trips generated by the site were made for the weekday AM and PM peak hours, as well as on a daily basis. The trip generation rates/equations utilized are provided in **Table 1**. The directional splits are shown in **Table 2**. The rates and splits for a general urban/suburban area were utilized. Finally, the predicted trip generation results for the proposed multifamily development are shown in **Table 3**.

Table 1. ITE Trip Generation Rates/Equations for Proposed Development

Land Use	ITE Code	Average Weekday	AM Peak Hour	PM Peak Hour
Multifamily Housing (Low-Rise)	220	$T = 7.56X - 40.86^1$	$\ln(T) = 0.95\ln(X) - 0.51$	$\ln(T) = 0.89\ln(X) - 0.02$

¹T = Trips Ends; X = Dwelling Units

Table 2. ITE Directional Splits for Proposed Development

Land Use	ITE Code	Average Weekday	AM Peak Hour	PM Peak Hour
Multifamily Housing (Low-Rise)	220	50 / 50 ¹	23 / 77	63 / 37

¹XX / YY = % entering vehicles / % exiting vehicles for General Urban/Suburban Area

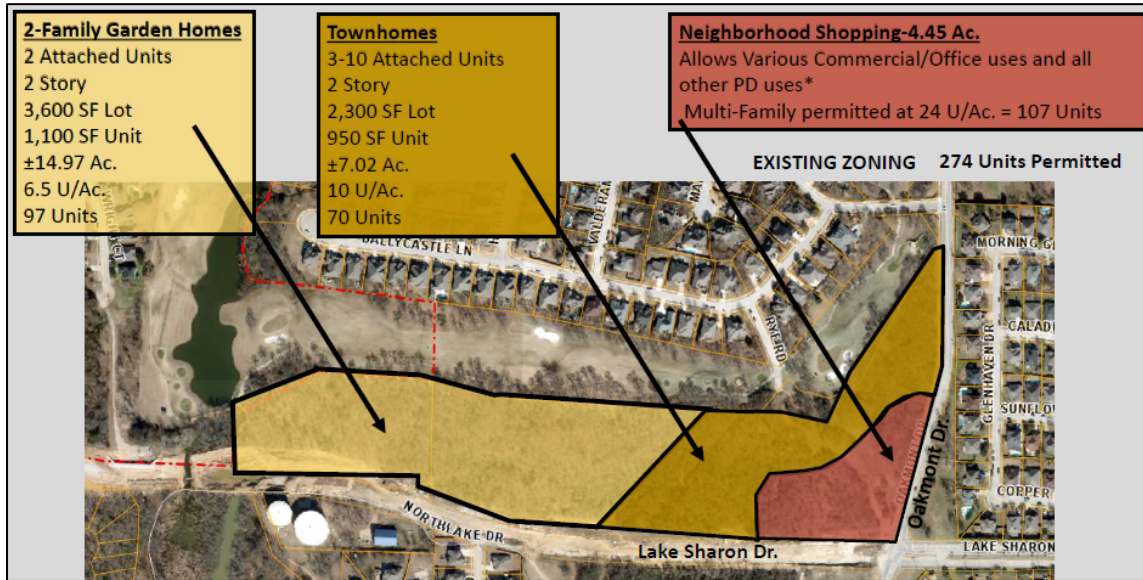
Table 3. Trip Generation Calculations for Avilla Fairways Development

Amount	Units	ITE Land Use (ITE Code)	Daily Trips	AM Peak Hour			PM Peak Hour		
				In	Out	Total	In	Out	Total
215	Dwellings	Multi-Family Housing (Low-Rise) (220)	1,586	23	76	99	74	43	117

Allowable Trip Generation for Existing Zoning

Additional trip generation was performed to identify site traffic which could be generated by the site under existing zoning. The majority of the site would include residential land uses under existing zoning, as shown in **Figure 4**. This 'Existing Zoning Density Exhibit' was provided by the City of Corinth. Approximately 4.5 acres on the southeast corner of the site is currently zoned as Neighborhood Shopping, which could include additional residential land use or various commercial/office land uses.

Figure 4. City of Corinth Existing Zoning Density Exhibit for Site



Additional information was provided by the City of Corinth regarding zoning information such as permitted land uses, building heights, lot coverage, maximum floor to area ratio, and parking requirements. Based on this information, four potential scenarios were identified for trip generation purposes, as identified in **Table 4**.

Table 4 also shows the resulting site traffic predicted to be generated under existing zoning for the four scenarios. For reference, the ITE trip generation equations/rates and directional splits utilized for these land uses are included in the Appendix. Additionally, internal capture reductions were applied to applicable scenarios. This methodology is further described in the Appendix.

Table 4. Assumed Land Use Scenarios and Trip Generation Results for Existing Zoning

Amount	Units	ITE Land Use (ITE Code)	Daily Trips	AM Peak Hour			PM Peak Hour		
				In	Out	Total	In	Out	Total
ALTERNATE SCENARIO 1 - Existing Zoning (274 Total Duplex, Townhome, and MF-24 Residential)									
97	dwellings	Multifamily Housing (Low-Rise) (220) ¹	694	11	35	46	36	21	57
70	dwellings	Multifamily Housing (Low-Rise) (220) ¹	490	8	26	34	27	16	43
107	dwellings	Multifamily Housing (Low-Rise) (220)	770	12	39	51	40	23	63
TOTAL			1,954	31	100	131	103	60	163
ALTERNATE SCENARIO 2 - Existing Zoning (167 Duplex/Townhome Dwelling Units, Restaurant, Retail/Pharmacy, and Fuel/Convenience)									
97	dwellings	Multifamily Housing (Low-Rise) (220) ¹	694	11	35	46	36	21	57
70	dwellings	Multifamily Housing (Low-Rise) (220) ¹	490	8	26	34	27	16	43
13,000	ft ²	Pharmacy/Drugstore with Drive-Through Window (881)	1,420	27	23	50	67	67	134
5,000	ft ²	Fast-Food Restaurant without Drive-Through Window (933)	1,732	76	50	126	71	71	142
20	vfp	Gasoline/Service Station with Convenience Market (945)	4,108	144	139	283	143	137	280
SUBTOTAL			8,444	266	273	539	344	312	656
Internal Capture Trips			292	34	41	75	112	105	217
TOTAL NET EXTERNAL TRIPS			8,152	232	232	464	232	207	439
ALTERNATE SCENARIO 3 - Existing Zoning (167 Duplex/Townhome Dwelling Units + Office)									
97	dwellings	Multifamily Housing (Low-Rise) (220) ¹	694	11	35	46	36	21	57
70	dwellings	Multifamily Housing (Low-Rise) (220) ¹	490	8	26	34	27	16	43
95,564	ft ²	General Office Building (710)	1,016	100	16	116	17	92	109
95,564	ft ²	Medical-Dental Office (720)	3,584	167	47	214	91	235	326
SUBTOTAL			5,784	286	124	410	171	364	535
Internal Capture Trips			13	2	1	3	5	5	10
TOTAL NET EXTERNAL TRIPS			5,771	284	123	407	166	359	525
ALTERNATE SCENARIO 4 - Existing Zoning (167 Duplex/Townhome Dwelling Units + 800 Student Charter School)									
97	dwellings	Multifamily Housing (Low-Rise) (220) ¹	694	11	35	46	36	21	57
70	dwellings	Multifamily Housing (Low-Rise) (220) ¹	490	8	26	34	27	16	43
800	students	Charter Elementary School (537)	1,480	478	423	901	39	73	112
TOTAL			2,664	497	484	981	102	110	212

¹ Both duplexes and townhomes are considered part of the 'Multifamily Housing (Low-Rise) land use in the 10th edition of the ITE Trip Generation Manual

Table 5 compares the resulting site traffic which could be generated under existing zoning for the four scenarios and the site traffic predicted to be generated by the proposed Avilla Fairways development.

Table 5. Trip Generation Comparison of Proposed Site and Existing Zoning Options

Description	Daily Trips	AM Peak Hour			PM Peak Hour		
		In	Out	Total	In	Out	Total
PROPOSED							
Avilla Fairways Site	1,586	23	76	99	74	43	117
PERMITTED UNDER EXISTING ZONING							
1. Residential Only	1,954	31	100	131	103	60	163
2. Residential and Retail Mix	8,152	232	232	464	232	207	439
3. Residential and Office	5,771	284	123	407	166	359	525
4. Residential and Charter School	2,664	497	484	981	102	110	212

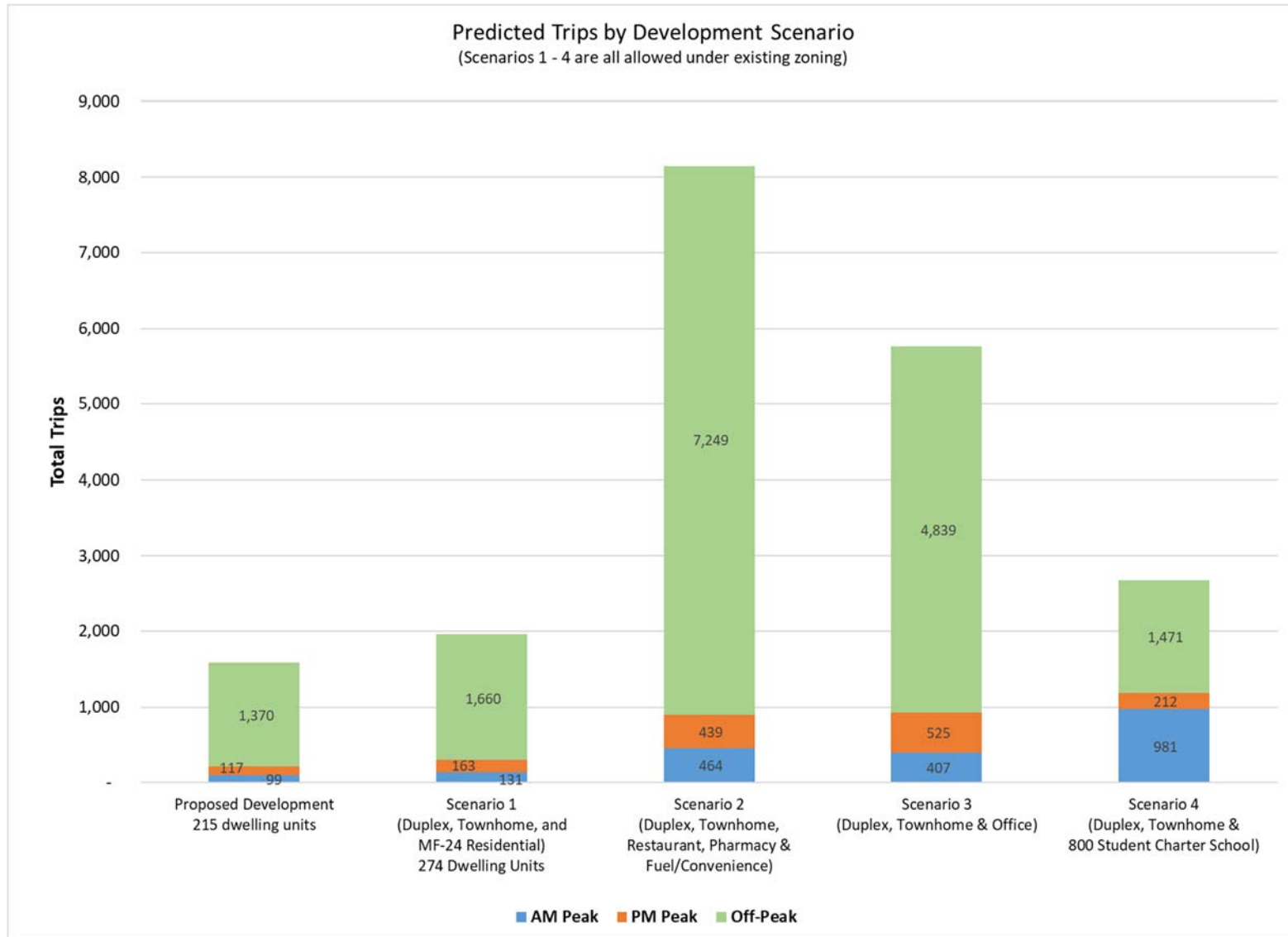
As shown, the proposed Avilla Fairways development is predicted to generate fewer trips than the site could produce under existing zoning. The site would generate a higher number of daily trips, AM peak hour trips, and PM peak hour trips for the scenarios investigated under existing zoning requirements, including the residential-only option. The proposed Avilla Fairways development includes 59 fewer dwelling units than could be allowed under existing zoning.

A visual comparison of predicted trip generation for the proposed site and existing zoning options is shown in **Figure 5**. Figure 5 shows the overall daily trips calculated for each scenario and a breakdown of AM peak hour site traffic, PM peak hour site traffic, and off-peak site traffic.

As shown, the site would produce significantly more traffic if developed with retail uses on the southeast corner, including approximately 4.7 times more AM peak hour trips and approximately 3.7 times more PM peak hour traffic compared to the Avilla Fairways development. Development of office on the southeast corner would also result in significantly more traffic, including approximately 4.1 times more AM peak hour trips and approximately 4.4 times more PM peak hour traffic compared to the Avilla Fairways development. Finally, while including a charter school in the southeast corner would not generate nearly as much traffic on a daily basis, in the AM peak hour nearly 10 times the number of trips would be generated compared to the proposed Avilla Fairways development.

Based on the results, the proposed Avilla Fairways development is predicted to be a less intense traffic generator compared to development which could be allowed under existing zoning.

Figure 5. Comparison of Trip Generation Results for Proposed Development and Existing Zoning Scenarios



EXISTING AND BACKGROUND TRAFFIC VOLUMES

Collected Traffic Volumes

Existing AM and PM peak hour turning movement volumes were collected on Tuesday, April 13, 2021, at the following intersections:

- Oakmont Drive at Lake Sharon Drive;
- Oakmont Drive at Ardglass Trail; and
- Oakmont Drive at Creekside Drive.

Volumes were collected between 6:30 AM and 9:00 AM and between 3:00 PM and 6:30 PM. The overall peak hours for each study intersection were utilized to present a conservative analysis. The collected AM peak hour and PM peak hour volumes are shown in **Figure 6**. Raw data sheets are included in the Appendix.

Adjusted Existing (2021) Traffic Volumes

Due to the ongoing COVID-19 pandemic, the collected traffic volumes on Oakmont Drive north of Lake Sharon Drive were compared to historic 2019 NCTCOG volumes on Oakmont Drive in the same location. The AM and PM peak hour volumes from 2019 were grown by an annual growth rate of four percent per year to represent expected volumes in 2021. The resulting peak hour volumes were compared to the collected traffic volumes (Figure 6), as shown in **Table 6**. Based on these results, a COVID adjustment factor of 1.09 was applied to the collected volumes during the AM peak hour and a factor of 1.17 was applied to the collected volumes during the PM peak hour. The adjusted Existing (2021) peak hour traffic volumes are shown in **Figure 7**.

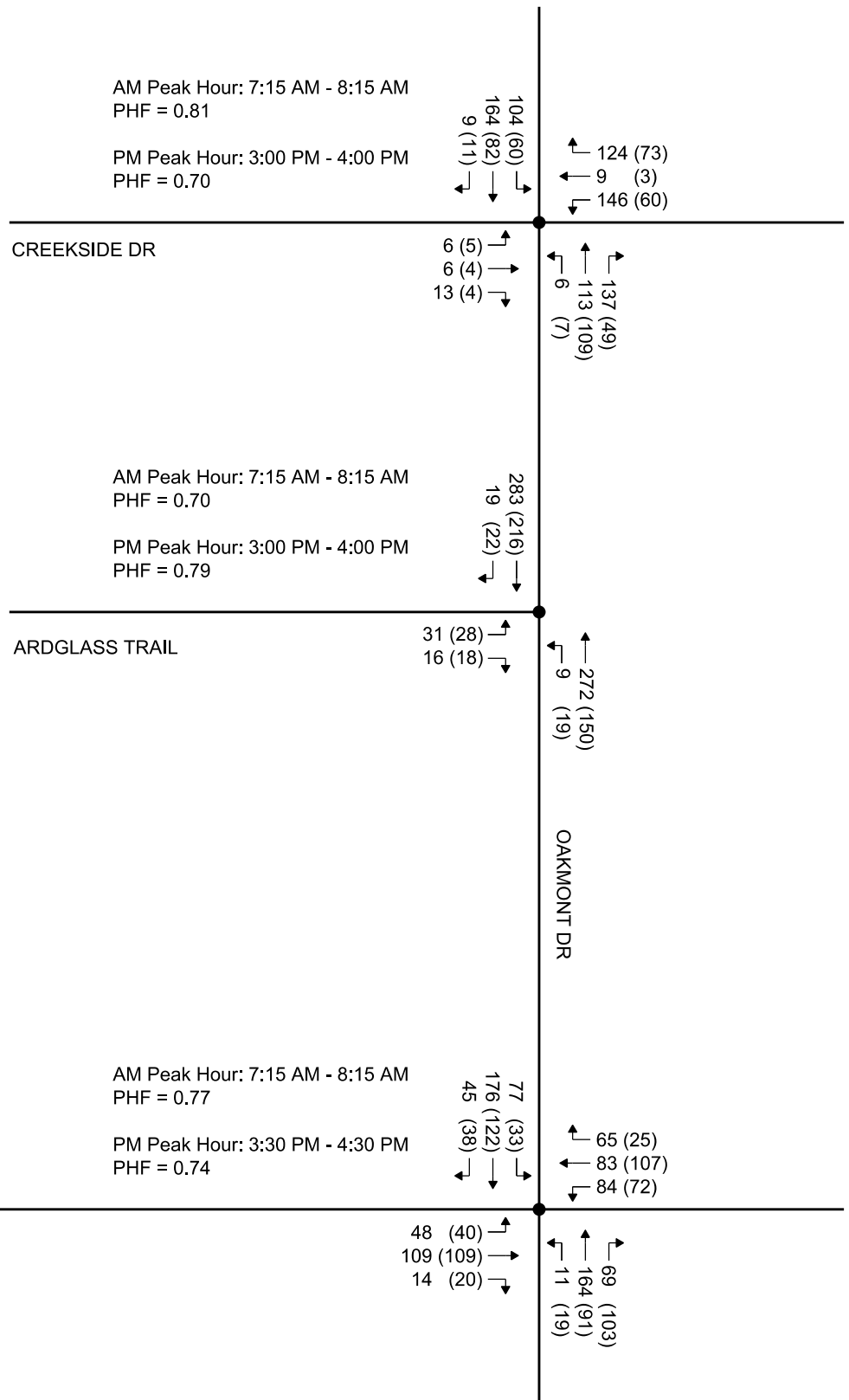
Table 6. Comparison of Collected Peak Hour Volumes to Historic NCTCOG Volumes

2019 NCTCOG DATA ¹		2021 TMC DATA	COMPARISON	
AM Peak Hour – Oakmont Dr North of Lake Sharon Dr				
Date	Grown to 2021 (4% Annually)	Date	Previous Counts (Grown to 2021) : TMC TIA Data	
12/3/2019		4/13/2021		
579	626	577	1.09	
PM Peak Hour – Oakmont Dr North of Lake Sharon Dr				
Date	Grown to 2021 (4% Annually)	Date	Previous Counts (Grown to 2021) : TMC TIA Data	
12/3/2019		4/13/2021		
378	409	349	1.17	

¹ Source: <https://trafficcounts.nctcog.org/trafficcount/>



NOT TO SCALE

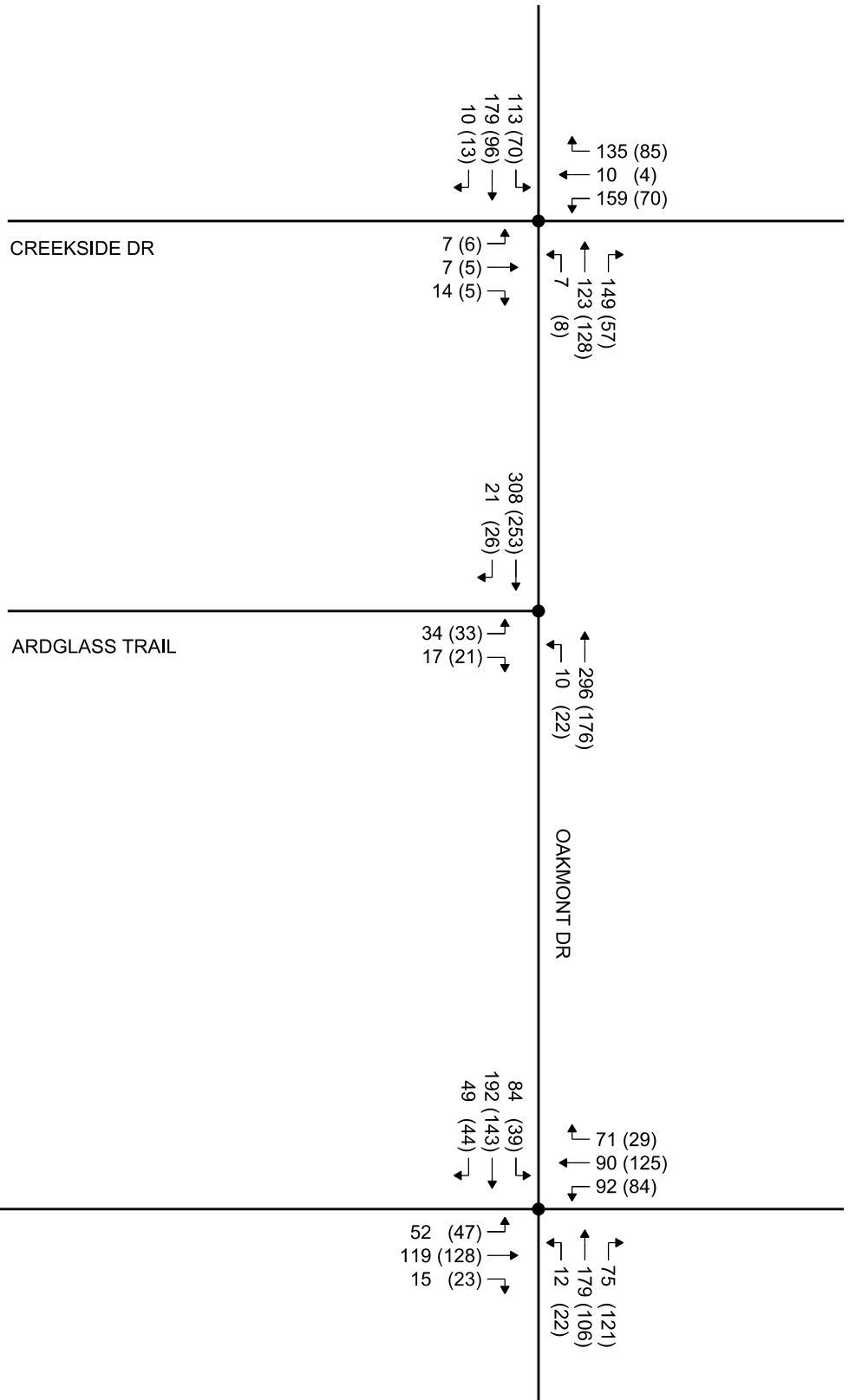


LEGEND

- Study Intersection
- ← AM Peak Hour (PM Peak Hour)



NOT TO SCALE



LEGEND

- Study Intersection
- ← AM Peak Hour (PM Peak Hour)

Development of Background Traffic Volumes

Historical daily traffic volumes in the study vicinity were gathered from available online TxDOT and NCTCOG traffic counts in the vicinity of the site, which are summarized in **Table 7**.

Table 7. Historical Daily Traffic Volumes

Year	Oakmont North of FM 2181 ¹	Corinth St West of IH-35 ¹	FM 2181 West of Village Pkwy ²	FM 2181 West of IH-35 ²
2009	3,139	3,996	13,500	19,300
2010	-	-	11,500	19,500
2011	-	-	15,900	18,000
2012	-	-	16,700	19,100
2013	-	-	15,678	16,893
2014	4,474	5,780	12,456	10,933
2015	-	-	15,382	14,255
2016	-	-	19,877	20,848
2017	-	-	22,340	21,661
2018	-	-	17,574	21,661
2019	4,968	6,001	16,835	23,715
Average Growth Rate	5%	4%	2%	2%

¹ Source: <https://trafficcounts.nctcog.org/trafficcount/>

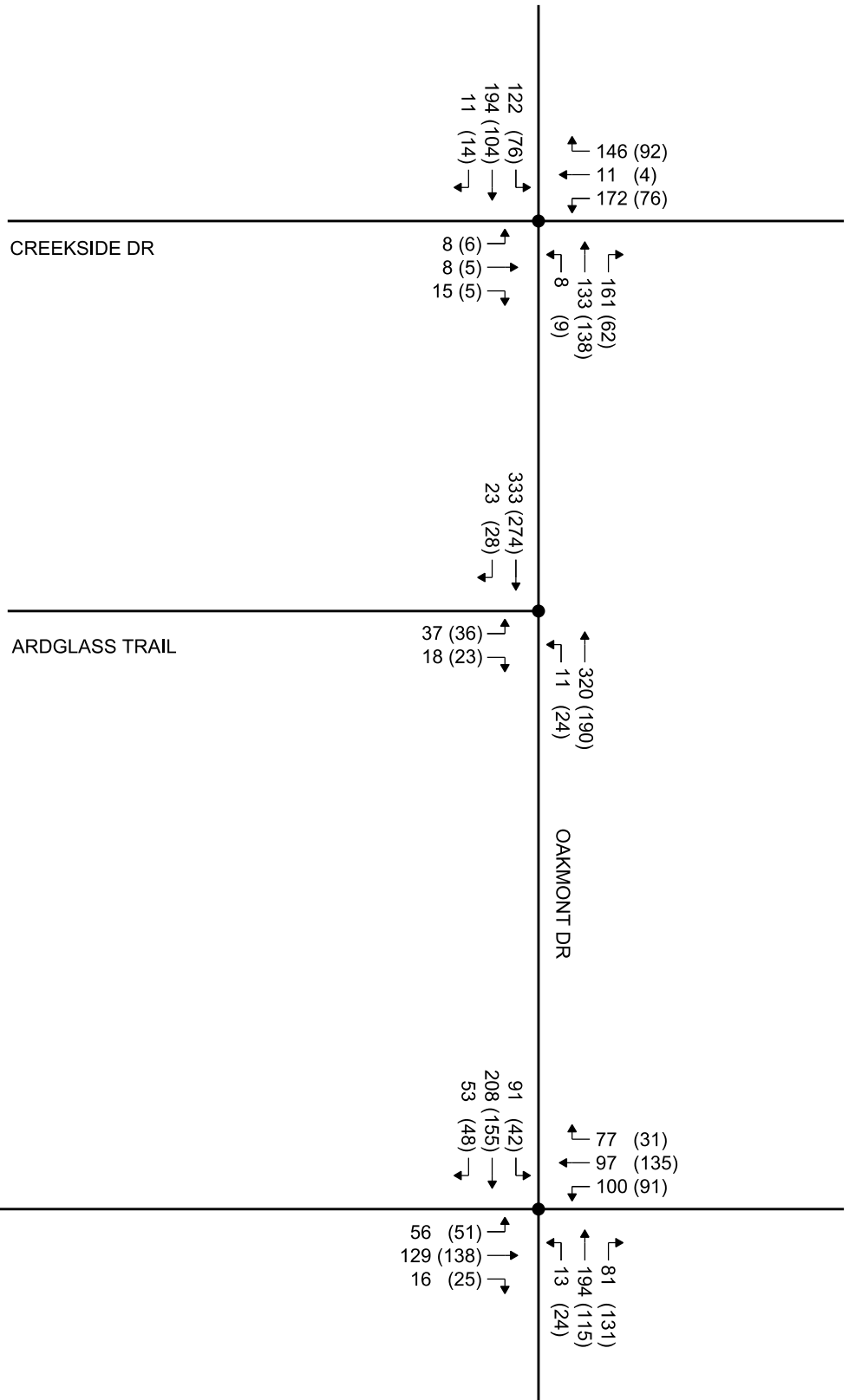
² Source: http://www.txdot.gov/apps/statewide_mapping/StatewidePlanningMap.html

Based on the TxDOT data, traffic volumes in the vicinity of the site have varied, but have generally grown by approximately two to five percent per year over a ten-year period (2009 to 2019).

Based on these results, background (non-site) traffic volumes for the study area intersections and roadways were estimated by growing the existing (adjusted) traffic volumes at an annual rate of four percent (4%) for two years to obtain the Build-Out Year (2023) Background traffic volumes, as shown in **Figure 8**.



NOT TO SCALE



LEGEND

- Study Intersection
- ← AM Peak Hour (PM Peak Hour)

TOTAL TRAFFIC VOLUMES

Directional Distribution

The existing traffic volumes in the area, knowledge of the study area, and the proposed site layout were used to determine the directions from which site traffic would approach and depart the development. The assumed directional distribution used for site traffic for the Avilla Fairways development is shown in the following figures:

- **Figure 9** – Proposed site plan (3 driveways)
- **Figure 10** – Alternative 1 (2 driveways)
- **Figure 11** – Alternative 2 (Rye Road as one-way outbound)

For reference, the assumed lane configurations for the site access points for the two alternative scenarios are shown in **Figure 12**. Lane configurations based on the currently proposed site plan (Figure 2) were previously shown in Figure 3. Lane configurations at the existing three intersections along Oakmont Drive were the same for all scenarios.

Site Traffic Volumes

Traffic volumes expected to be generated by the proposed development were assigned to the area roadways and site access points based on the assumed directional distributions identified in Figures 9, 10, and 11. The estimated site generated traffic volumes for the proposed development for the weekday AM and PM hours are shown in the following figures:

- **Figure 13** – Proposed site plan (3 driveways)
- **Figure 14** – Alternative 1 (2 driveways)
- **Figure 15** – Alternative 2 (Rye Road as one-way outbound)

Redistributed Neighborhood Traffic Volumes

With the extension of Rye Road to Lake Sharon Drive, it is anticipated that some of the existing traffic from the neighborhood to the north would utilize Rye Road rather than Ardglass Trail, which is currently the only access point for that neighborhood. The overall directional distribution was utilized for this traffic. The estimated redistributed peak hour traffic volumes for the neighborhood to the north are shown in the following figures:

- **Figure 16** – Proposed site plan and Alternative 1 (Rye Road as two-way street)
- **Figure 17** – Alternative 2 (Rye Road as one-way outbound)

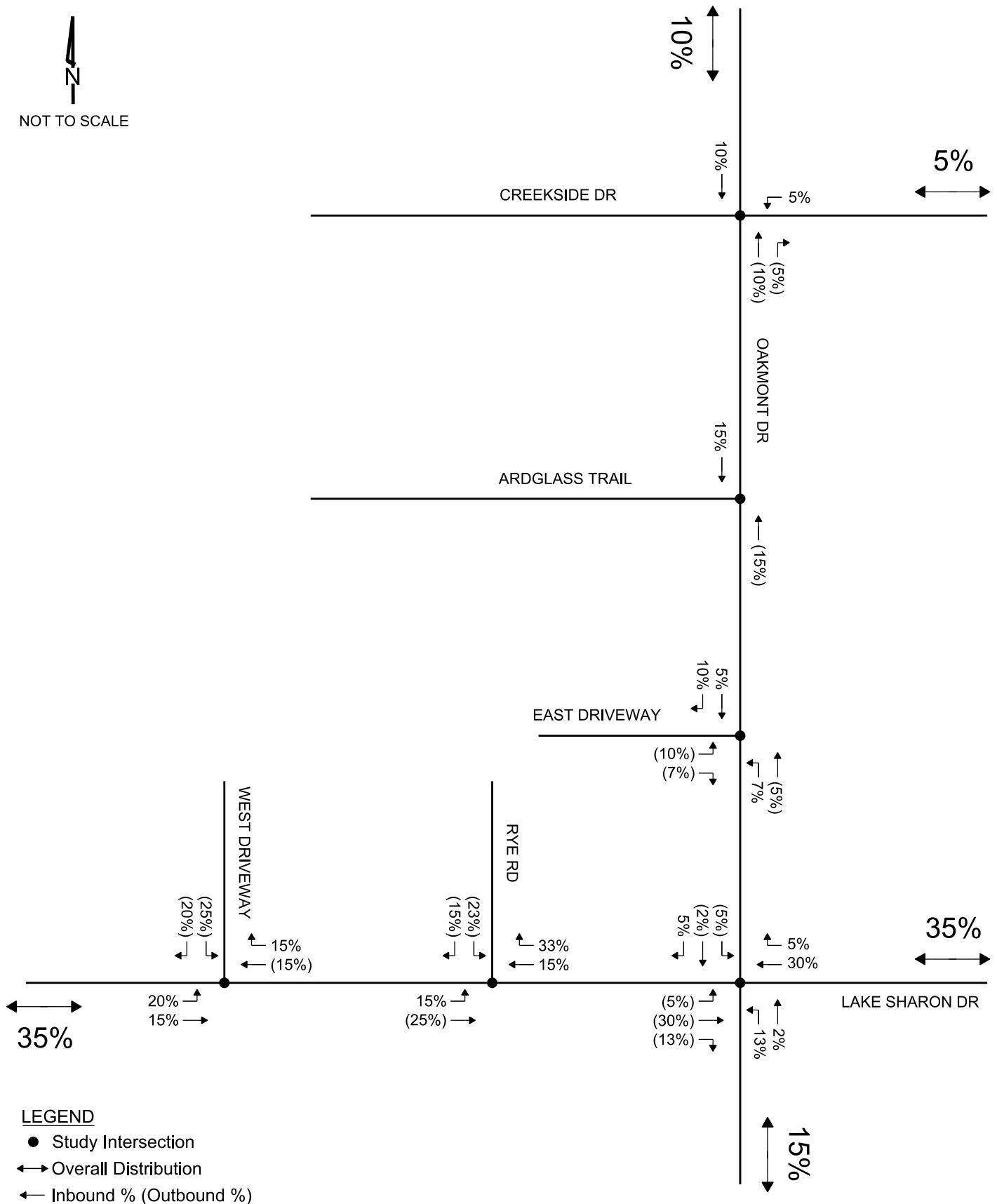
Projected Total Traffic Volumes

To obtain the projected total traffic volumes at site Build-Out (2023), the estimated site generated traffic volumes at build-out (Figures 13-15) and the redistributed neighborhood traffic volumes (Figures 16-17) were added to the 2023 background traffic volumes (Figure 8). The projected Build-Out Year (2023) Total traffic volumes are shown in the following figures:

- **Figure 18** – Proposed site plan (3 driveways)
- **Figure 19** – Alternative 1 (2 driveways)
- **Figure 20** – Alternative 2 (Rye Road as one-way outbound)

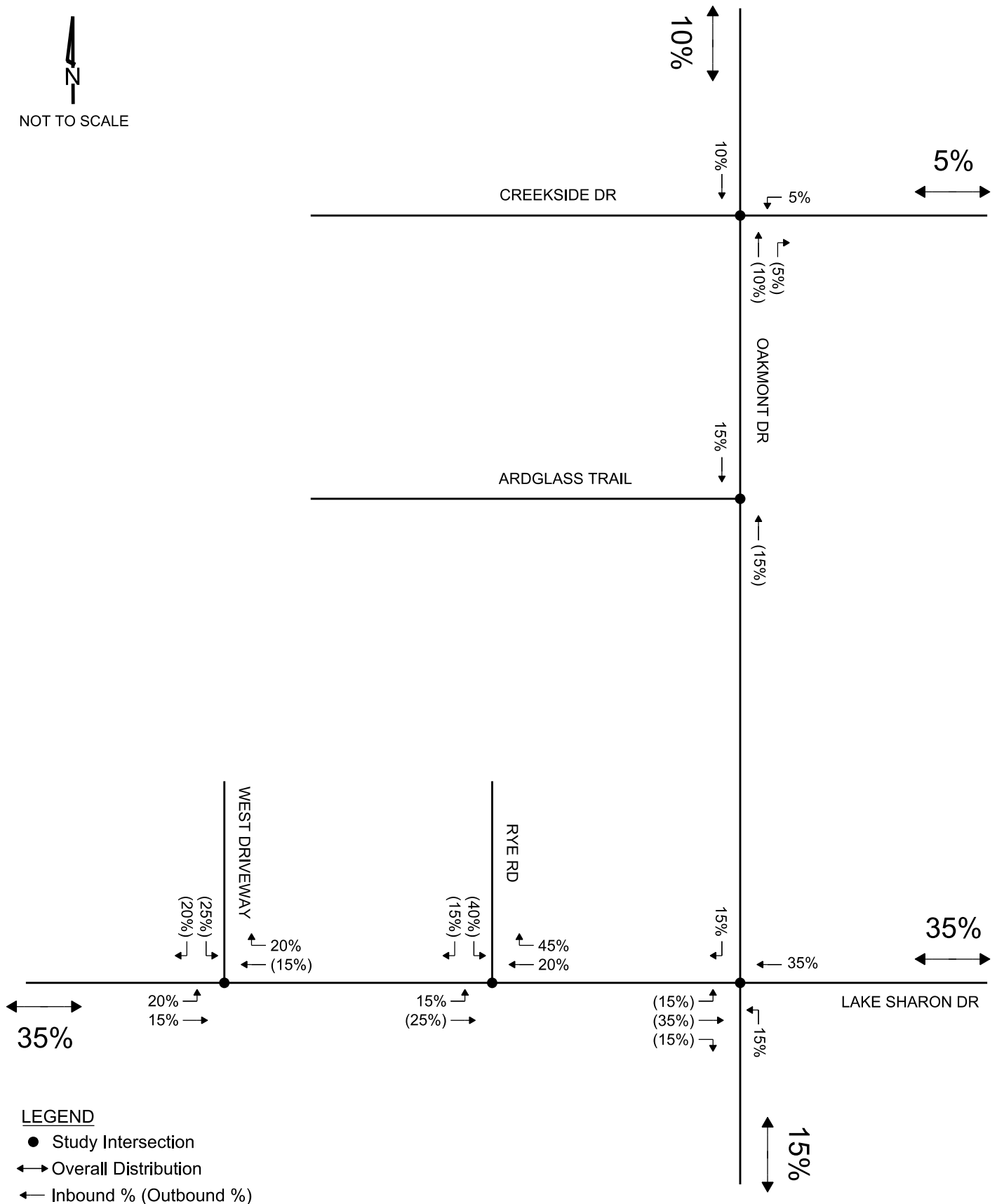


NOT TO SCALE



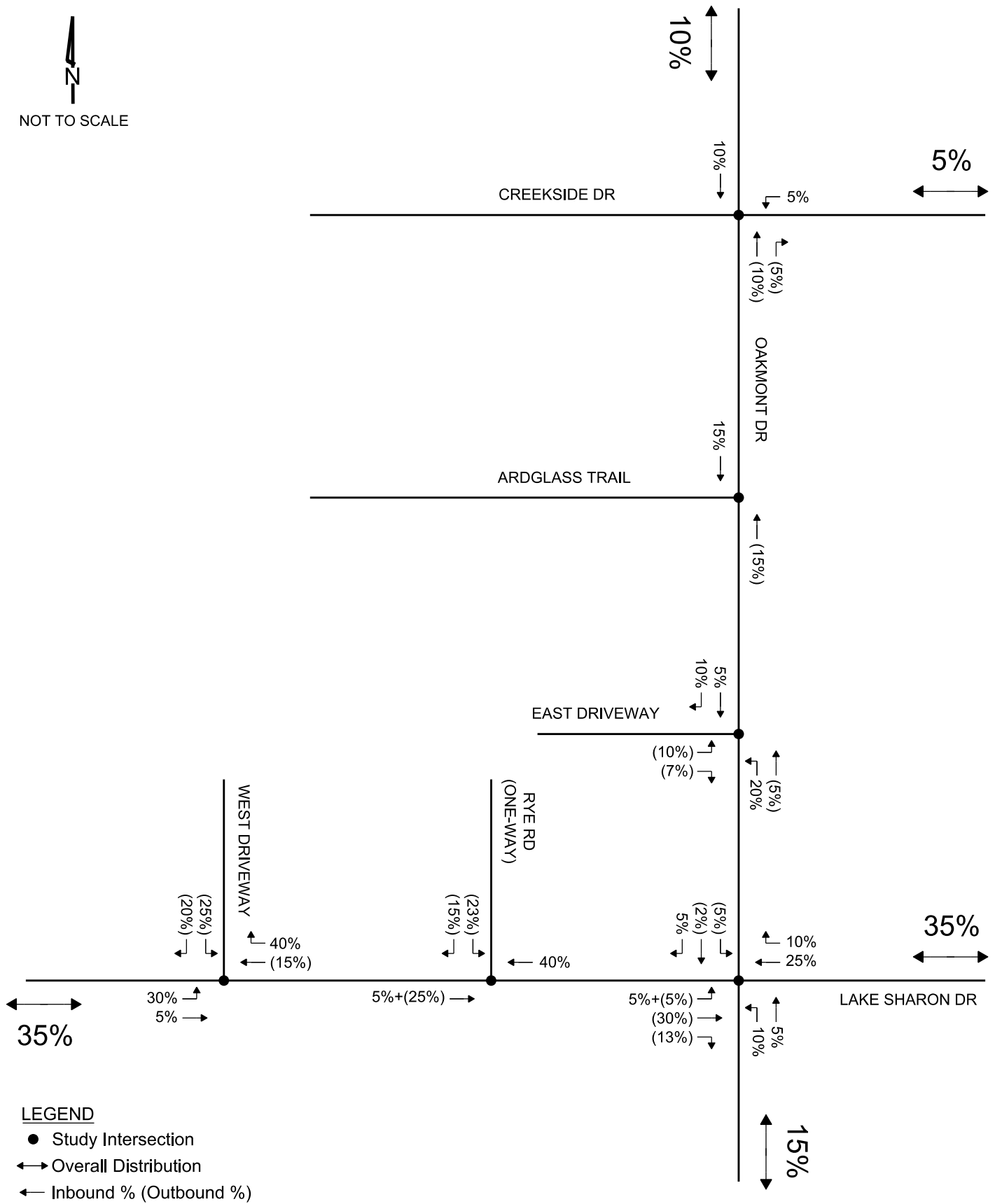


NOT TO SCALE



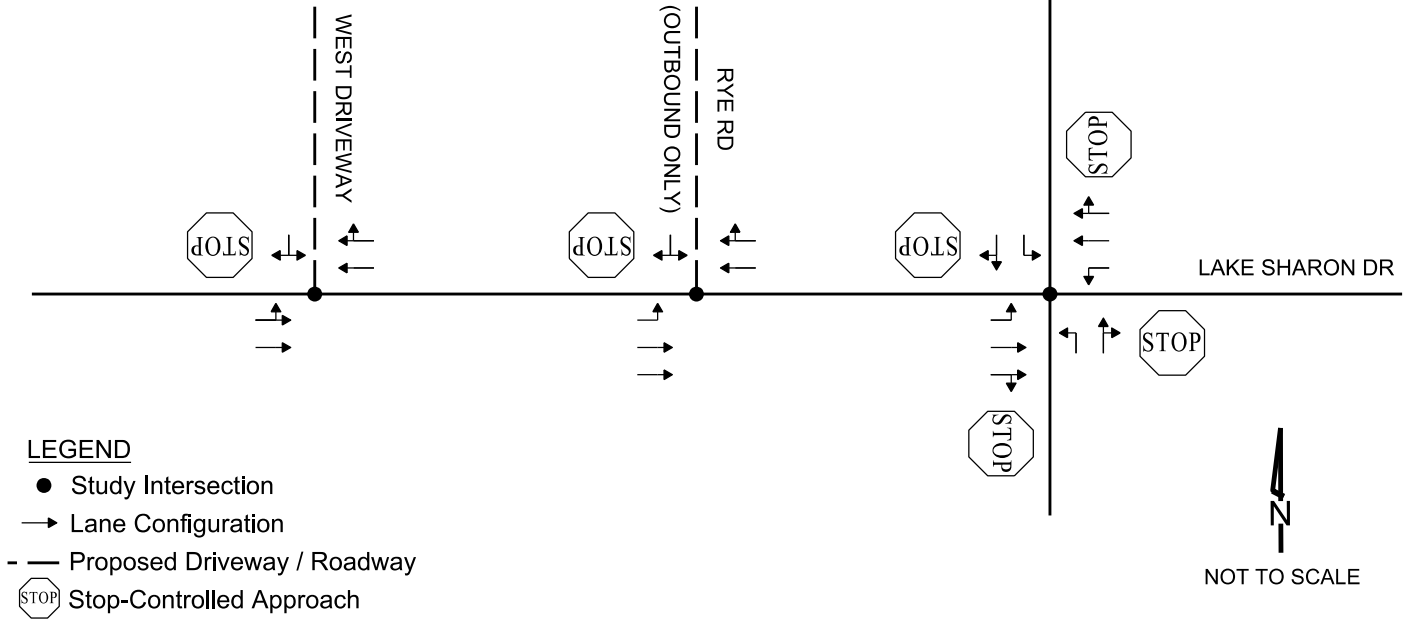


NOT TO SCALE



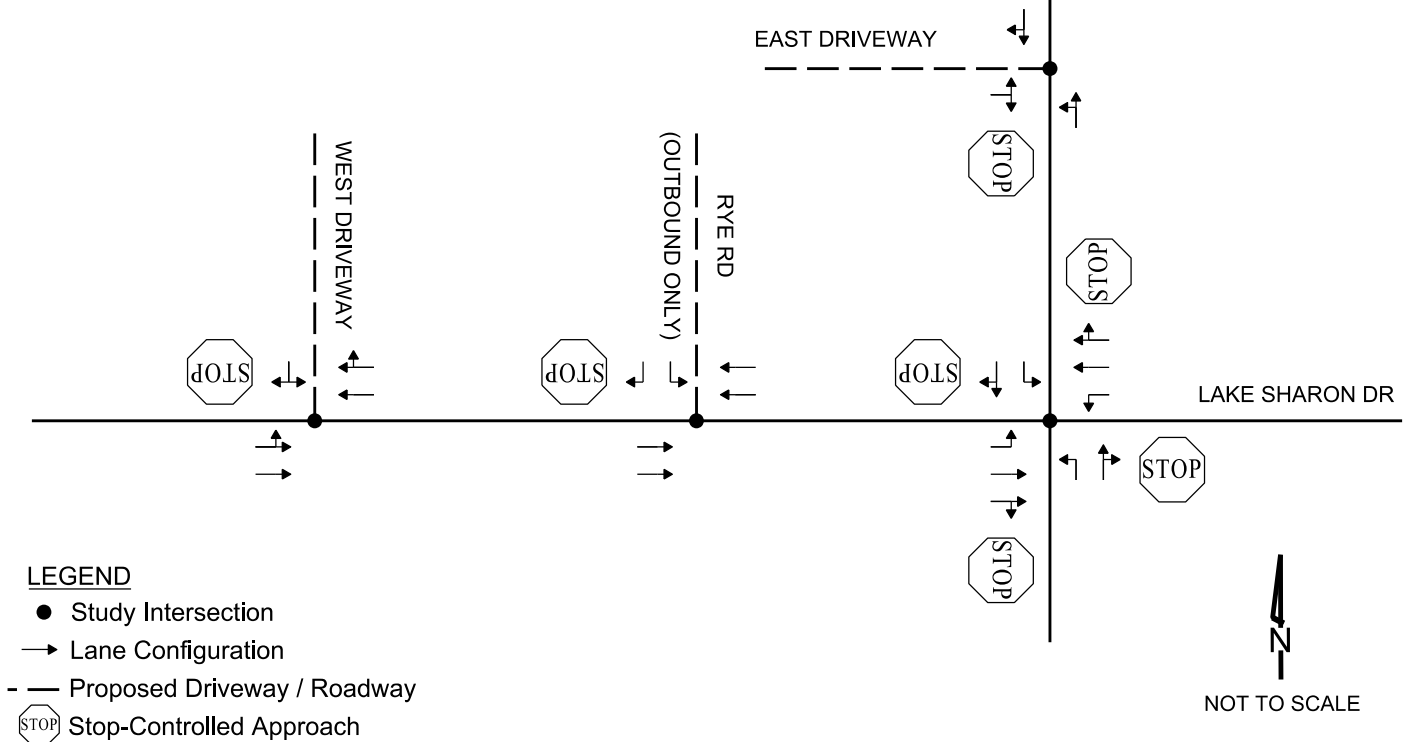
ALTERNATIVE 1

(Direct Access to Oakmont Drive Eliminated)



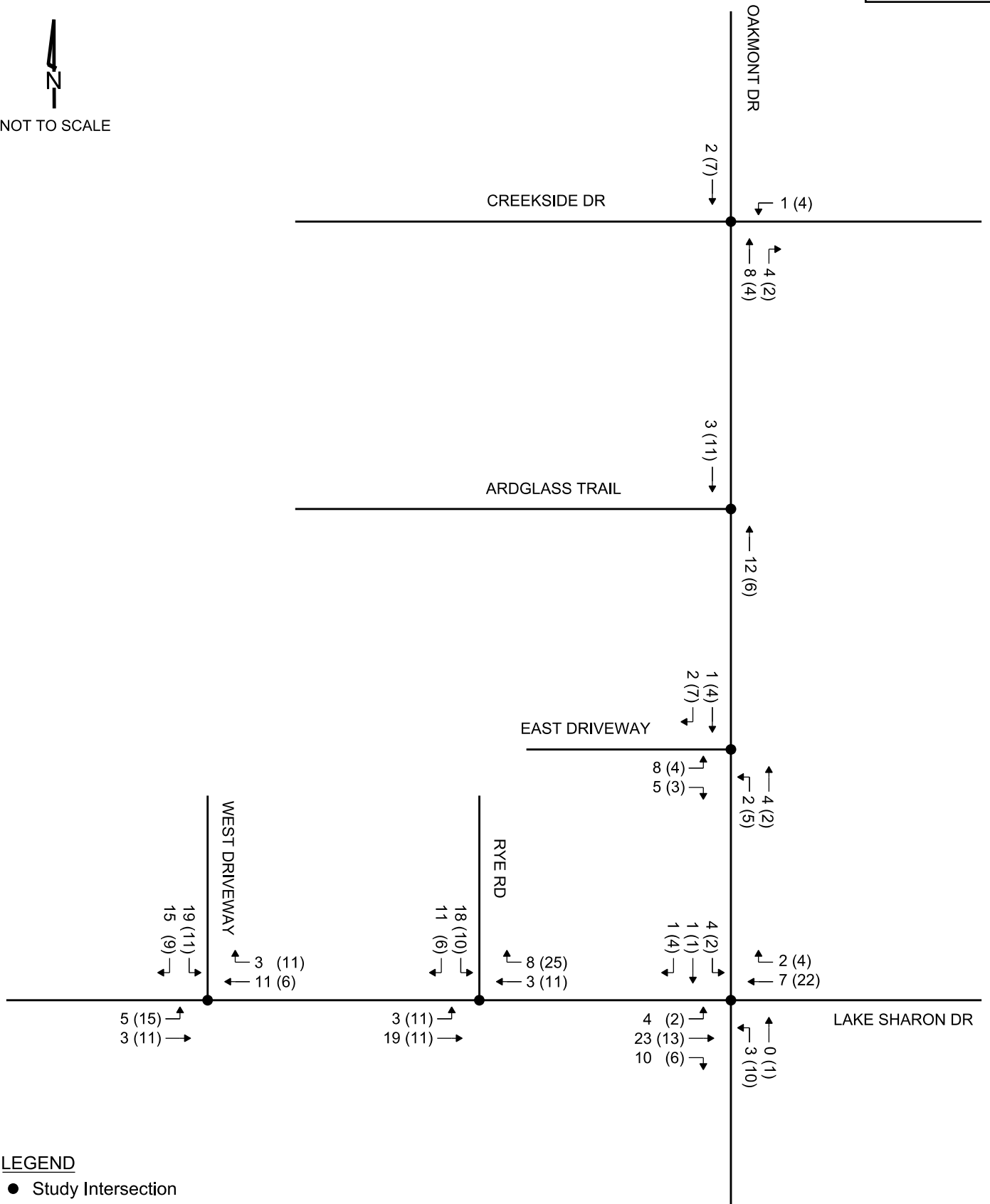
ALTERNATIVE 2

(Rye Road One-Way Southbound)





NOT TO SCALE

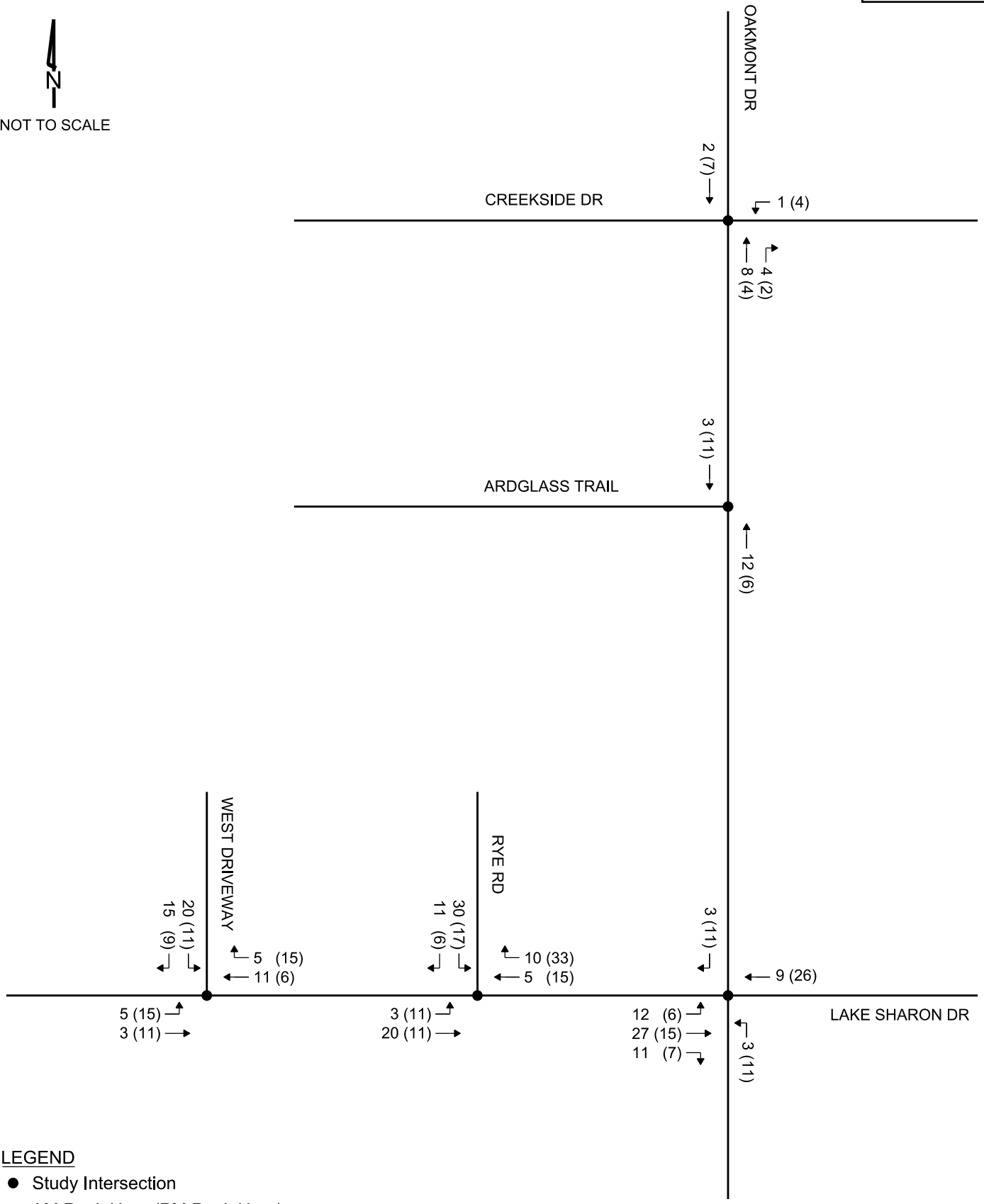


LEGEND

- Study Intersection
- ← AM Peak Hour (PM Peak Hour)



NOT TO SCALE

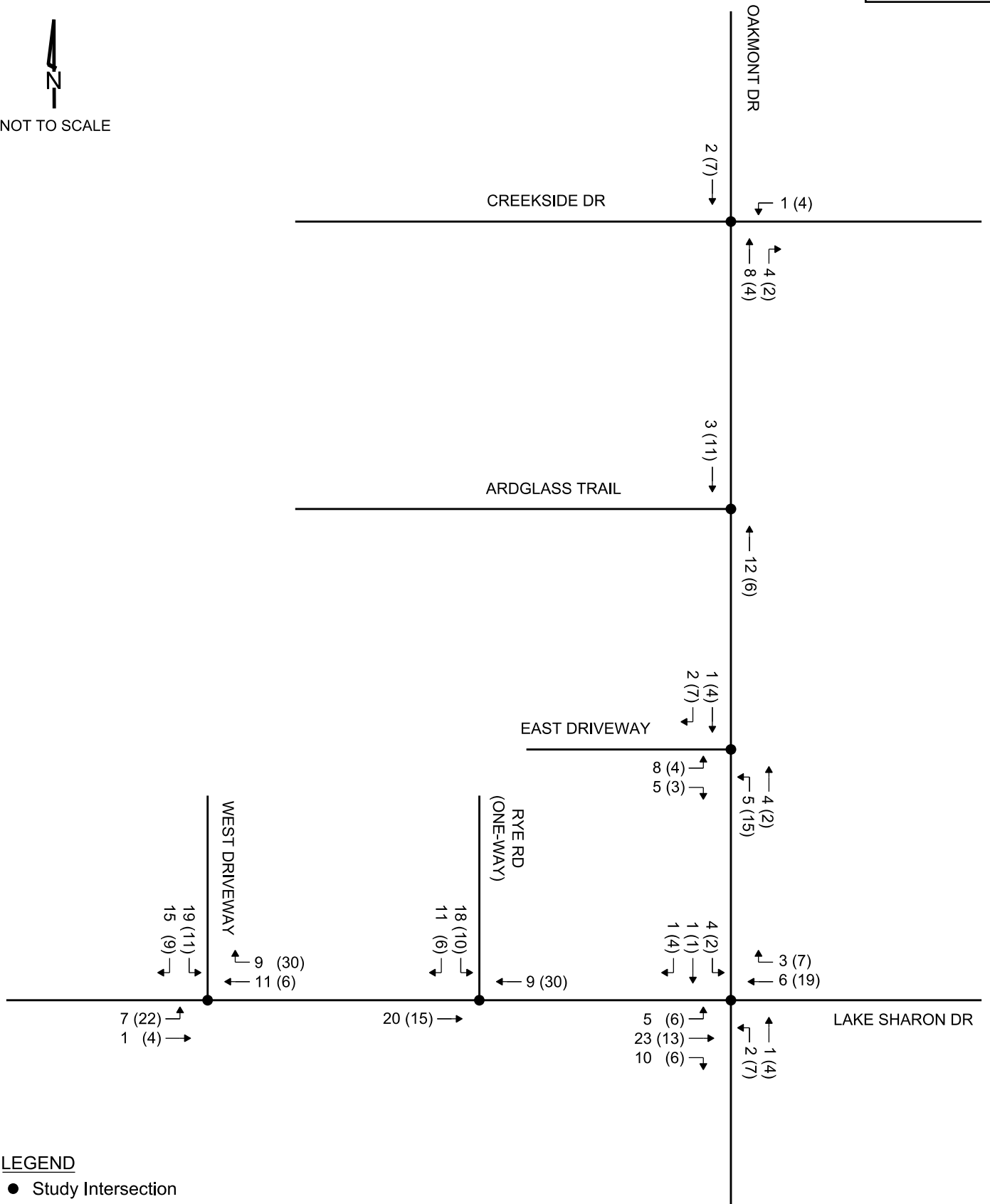


LEGEND

- Study Intersection
- ← AM Peak Hour (PM Peak Hour)

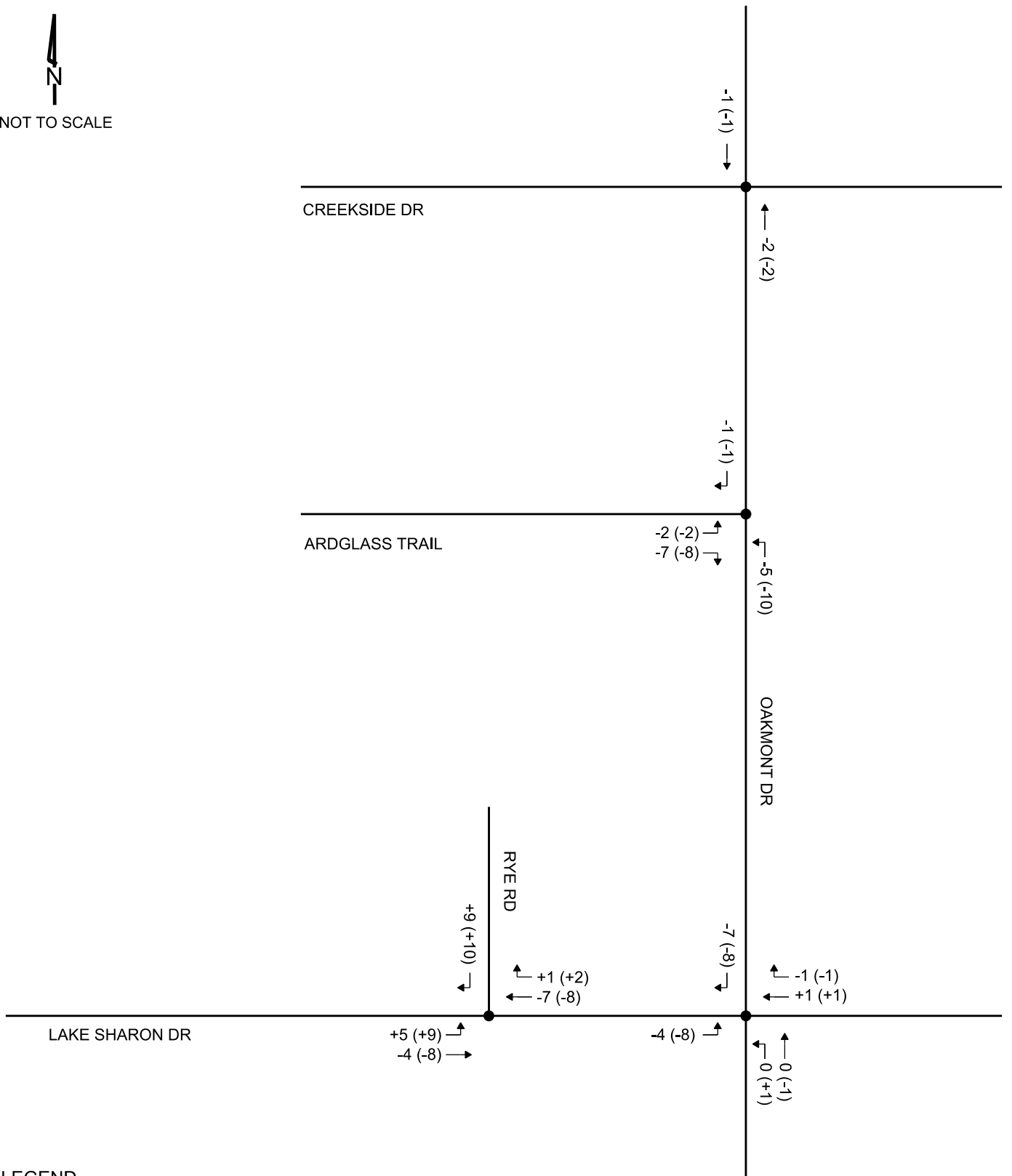


NOT TO SCALE





NOT TO SCALE



LEGEND

- Study Intersection
- ← AM Peak Hour (PM Peak Hour)



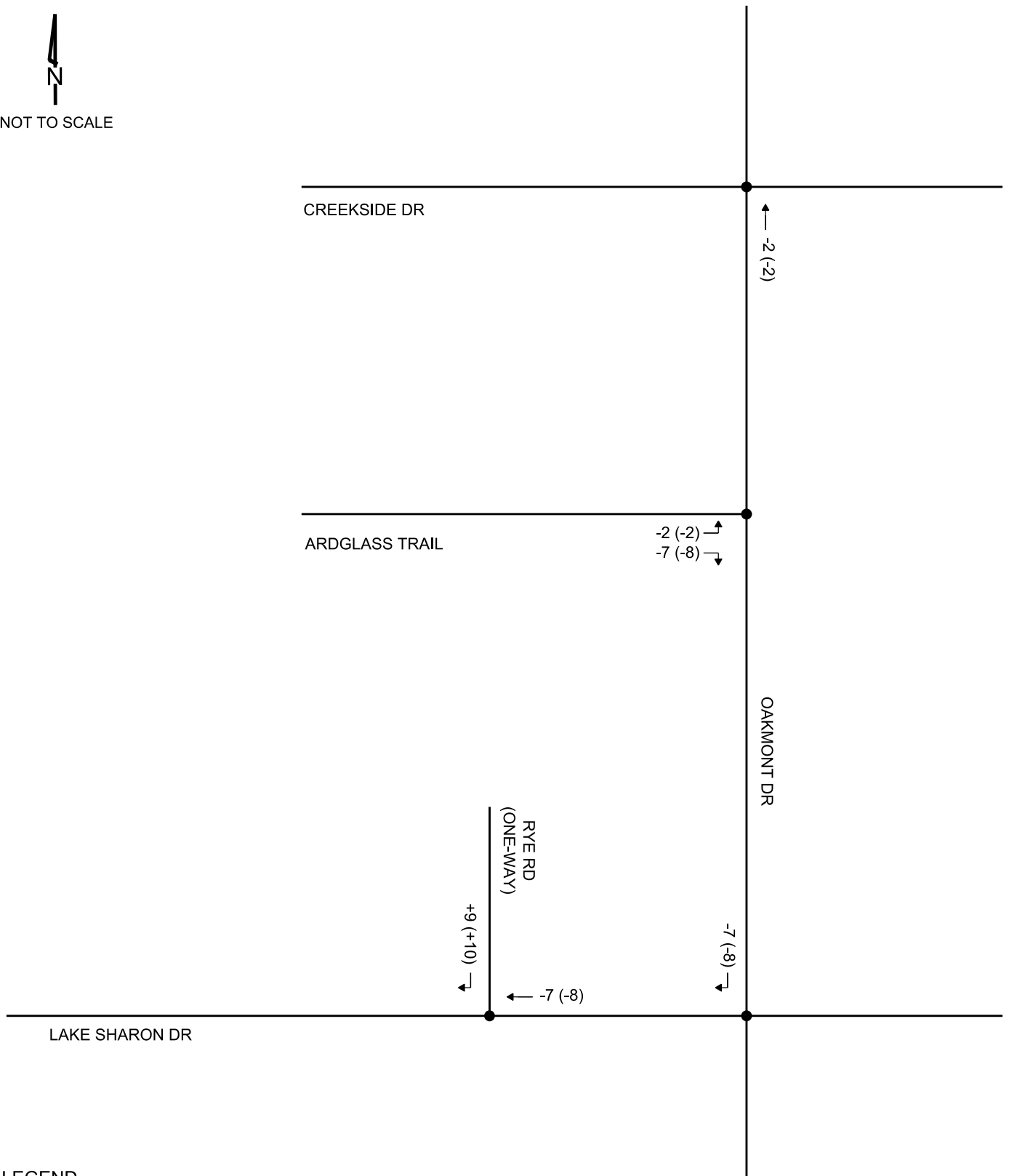
3030 LBJ FREEWAY
SUITE 1660
DALLAS, TEXAS 75234
972-248-3006

Redistributed Neighborhood Traffic -
Proposed Site Plan and Alternative 1

Figure 16



NOT TO SCALE



LEGEND

- Study Intersection
- ← AM Peak Hour (PM Peak Hour)

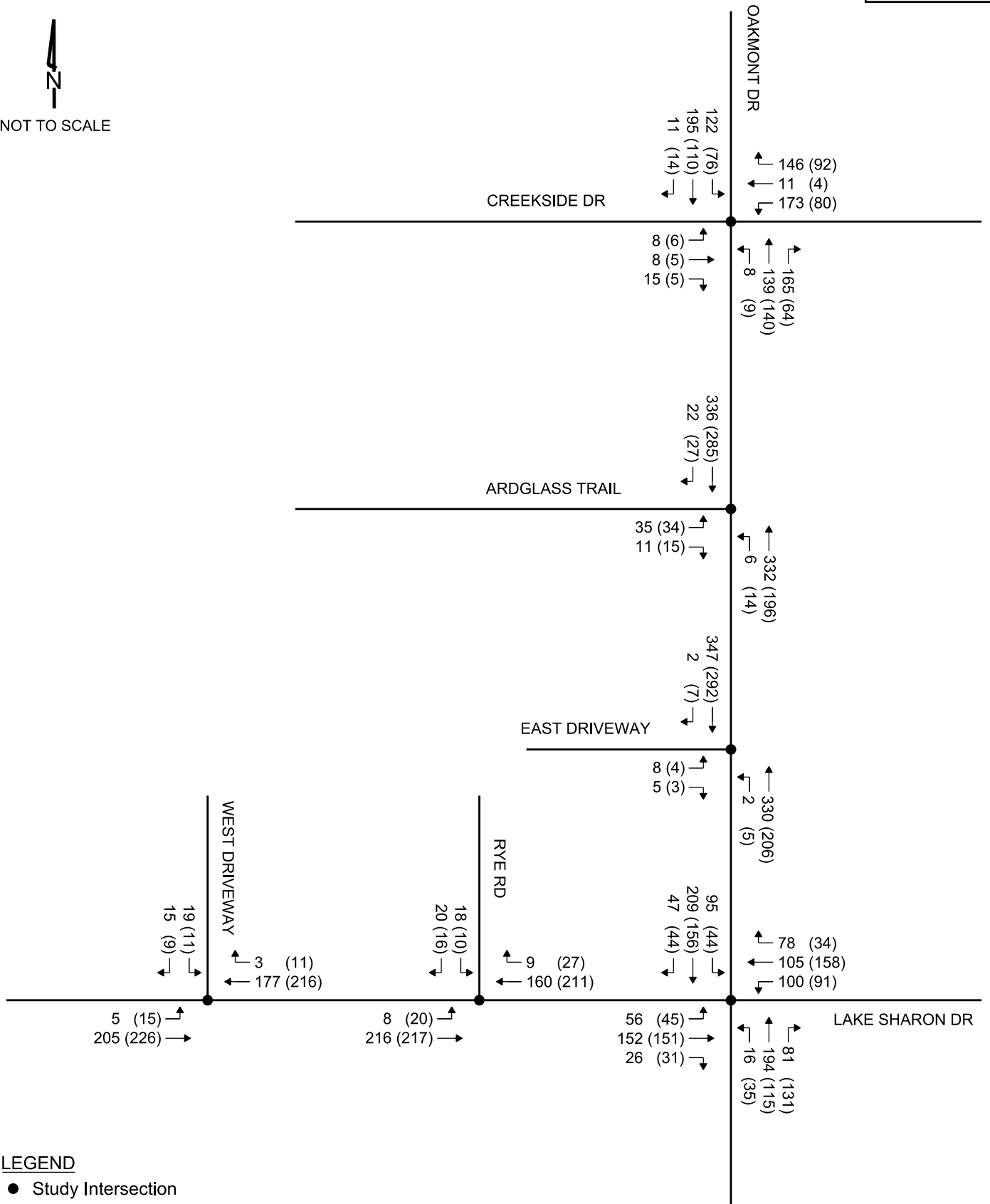


3030 LBJ FREEWAY
 SUITE 1660
 DALLAS, TEXAS 75234
 972-248-3006

LEE ENGINEERING



NOT TO SCALE

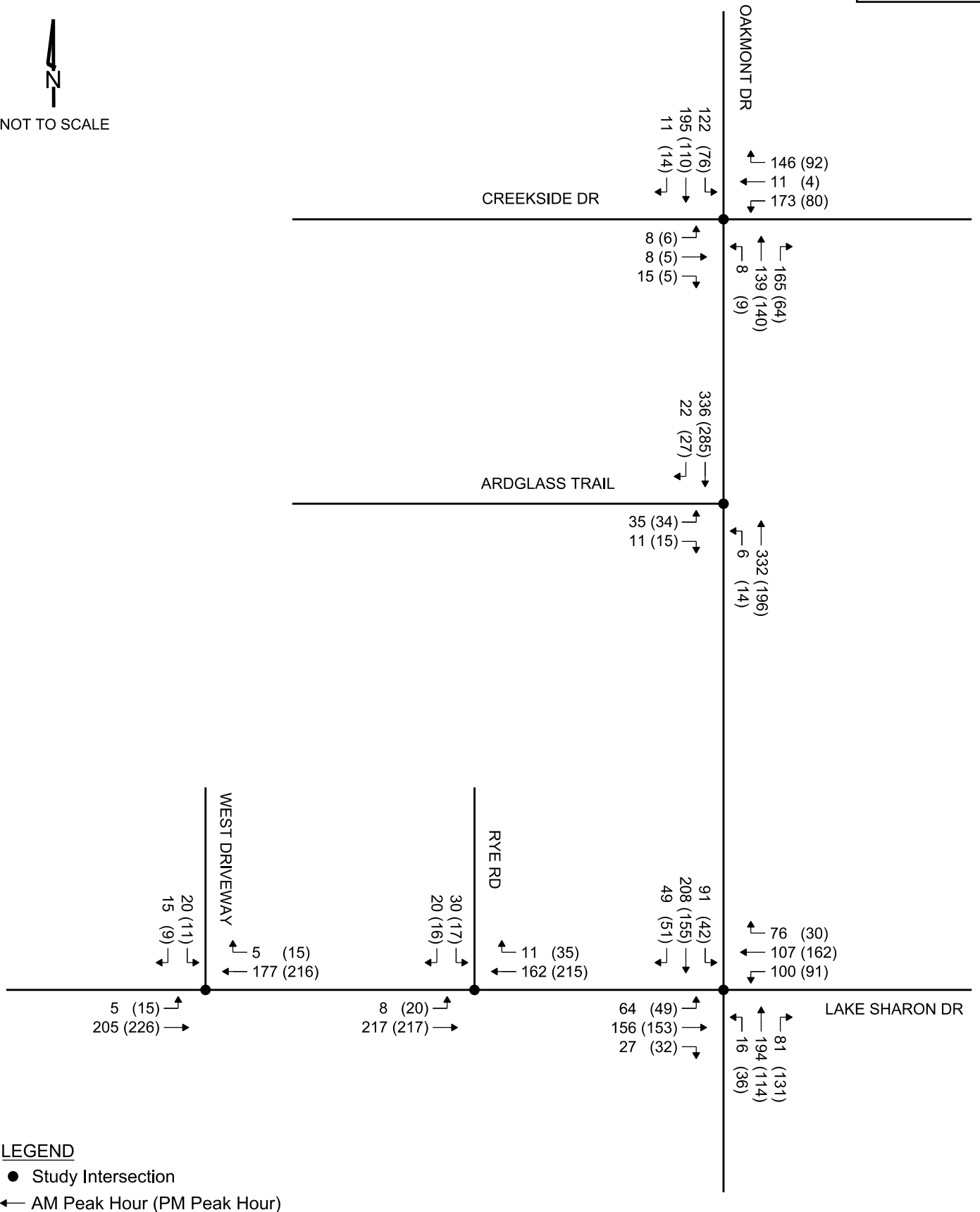


LEGEND

- Study Intersection
- ← AM Peak Hour (PM Peak Hour)



NOT TO SCALE



LEGEND

- Study Intersection
- ← AM Peak Hour (PM Peak Hour)

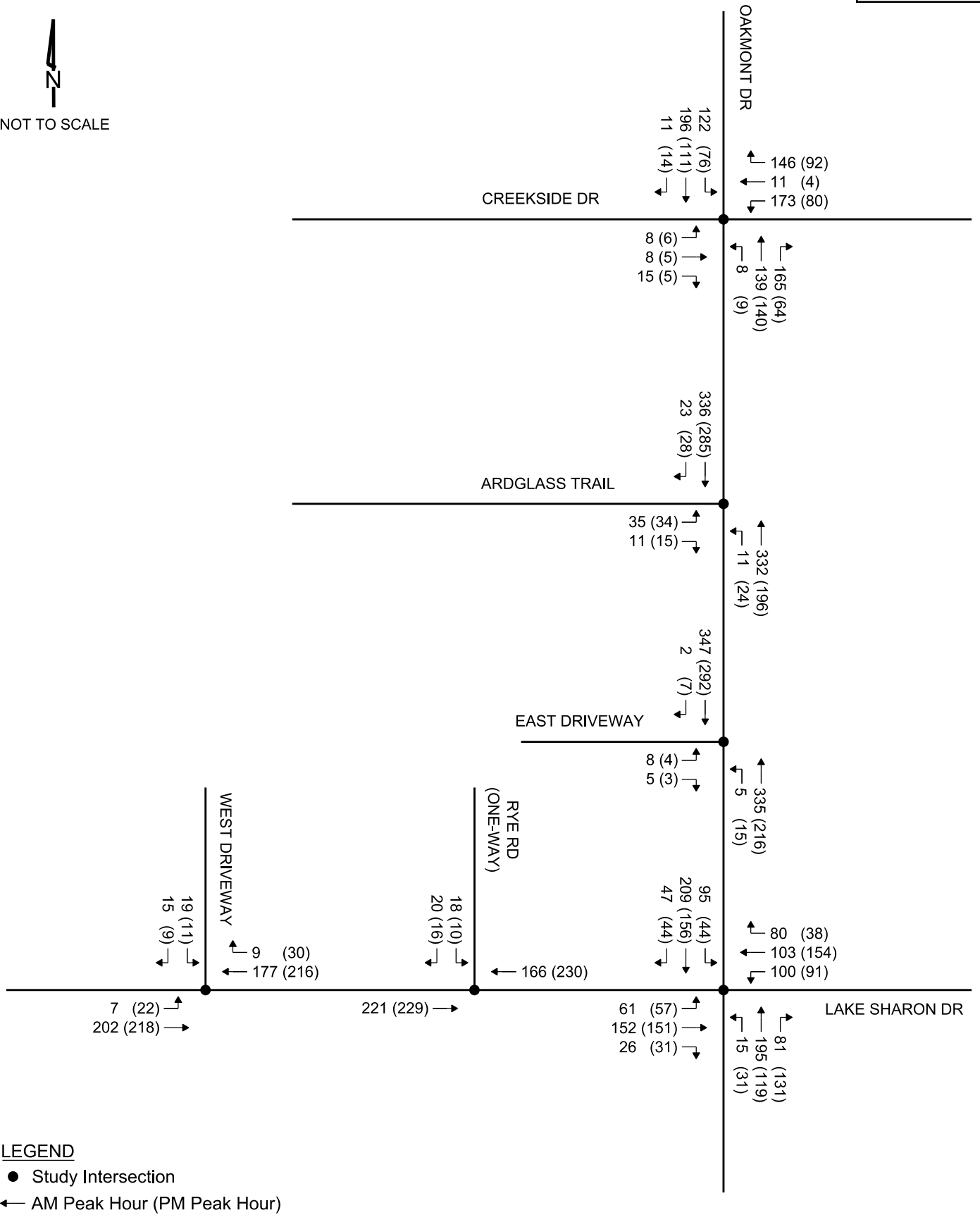


Build Out (2023) Total Traffic Volumes - Alternative 1
(Direct Access to Oakmont Drive Eliminated)

Figure 19



NOT TO SCALE



TRAFFIC ANALYSIS

Intersection Capacity and Level of Service

The Level of Service (LOS) of an intersection is a qualitative measure of capacity and operating conditions and is directly related to vehicle delay. The LOS criteria for an unsignalized intersection are shown in **Table 8**. LOS is given a letter designation from A to F, with LOS A representing very short delays (less than 10 seconds of average control delay per vehicle) and LOS F representing very long delays (more than 50 seconds of average control delay per vehicle). LOS D, ranging from 25.1 to 35.0 seconds of average control delay per vehicle, is typically considered the minimum acceptable condition in an urban environment.

The LOS criteria for a signalized intersection are shown in **Table 9** for reference.

Capacity analyses were conducted for the study area intersections under the following analysis scenarios:

- Existing (2021) traffic conditions (Figure 7)
- Build-Out Year (2023) Background traffic conditions (Figure 8)
- Build-Out (2023) Total traffic conditions for the proposed site plan (Figure 18)
- Build-Out (2023) Total traffic conditions for Alternative 1 (Figure 19)
- Build-Out (2023) Total traffic conditions for Alternative 2 (Figure 20)

Results were obtained using the macroscopic traffic analysis software package *Synchro 10*. Software output sheets are included in the Appendix. It should be noted that HCM methodology does not provide intersection-wide delay or level of service for intersections operating under two-way stop control.

Additional performance measures such as volume to capacity (v/c) ratios and queue lengths also provide an indication of operations. For example, at two-way stop-controlled intersections, main street traffic volumes may impose longer average delays for a small number of side-street vehicles, thus creating vehicle delays which correspond to a poor level of service.

Table 8. Level of Service Criteria for Unsignalized Intersections

Level-of-Service (LOS)	Average Control Delay (seconds/vehicle)	Description
A	≤ 10.0	No delays at intersections with continuous flow of traffic. Uncongested operations: high frequency of long gaps available for all left and right turning traffic. No observable queues.
B	10.1 to 15.0	No delays at intersections with continuous flow of traffic. Uncongested operations: high frequency of long gaps available for all left and right turning traffic. No observable queues.
C	15.1 to 25.0	Moderate delays at intersections with satisfactory to good traffic flow. Light congestion; infrequent backups on critical approaches.
D	25.1 to 35.0	Increased probability of delays along every approach. Significant congestion on critical approaches, but intersection functional. No standing long lines formed.
E	35.1 to 50.0	Heavy traffic flow condition. Heavy delays probable. No available gaps for cross-street traffic or main street turning traffic. Limit of stable flow.
F	> 50.0 or v/c>1.0	Unstable traffic flow. Heavy congestion. Traffic moves in forced flow condition. Average delays greater than one minute highly probable. Total breakdown.

SOURCE: *Highway Capacity Manual (HCM), 6th Edition*, Transportation Research Board, 2016

Table 9. Level of Service Criteria for Signalized Intersections

Level-of-Service (LOS)	Average Control Delay (seconds/vehicle)	Description
A	≤ 10.0	Very low vehicle delays, free flow, signal progression extremely favorable, most vehicles arrive during given signal phase.
B	10.1 to 20.0	Good signal progression, more vehicles stop and experience higher delays than for LOS A.
C	20.1 to 35.0	Stable flow, fair signal progression, significant number of vehicles stop at signals.
D	35.1 to 55.0	Congestion noticeable, longer delays and unfavorable signal progression, many vehicles stop at signals.
E	55.1 to 80.0	Limit of acceptable delay, unstable flow, poor signal progression, traffic near roadway capacity, frequent cycle failures.
F	> 80.0	Unacceptable delays, extremely unstable flow and congestion, traffic exceeds roadway capacity, stop-and-go conditions.

SOURCE: *Highway Capacity Manual (HCM), 6th Edition*, Transportation Research Board, 2016

Existing (2021) Traffic Conditions

The existing lane configurations shown in Figure 3 and the Adjusted Existing (2021) traffic volumes shown in Figure 7 were used for this analysis. **Table 10** presents the analysis results for the existing study intersection under Existing (2021) traffic conditions.

Table 10. Intersection Capacity Analysis Results – Existing (2021) Traffic Conditions

Oakmont Drive at Lake Sharon Drive (Multiway Stop-Control)					
Peak Hour	Intersection	EB	WB	NB	SB
AM Peak	20.7 (C) ¹	14.0 (B)	14.5 (B)	28.5 (D)	23.0 (C)
PM Peak	16.3 (C)	13.1 (B)	13.4 (B)	20.4 (C)	17.6 (C)
Oakmont Drive at Ardglass Trail (Two-Way Stop-Control)					
Peak Hour	Intersection ²	EB	WB	NB Left	SB
AM Peak	---	17.5 (C)	---	8.3 (A)	0.0 (A)
PM Peak	---	13.1 (B)	---	8.2 (A)	0.0 (A)
Oakmont Drive at Creekside Drive (Multiway Stop-Control)					
Peak Hour	Intersection	EB	WB	NB	SB
AM Peak	13.5 (B)	9.9 (A)	16.7 (C)	11.2 (B)	12.8 (B)
PM Peak	10.1 (B)	8.8 (A)	10.6 (B)	9.9 (A)	9.9 (A)

¹ Delay in seconds/vehicle (Level of Service)

² HCM methodology does not provide intersection-wide delay/level of service for TWSC analysis

As shown in Table 10, the study intersections are predicted to operate at acceptable levels of service for existing conditions.

Build-Out Year (2023) Background Traffic Conditions

The existing lane configurations shown in Figure 3 and the Build-Out Year (2023) Background traffic volumes shown in Figure 8 were used for this analysis. **Table 11** presents the analysis results for the study intersection under Build-Out Year (2023) Background traffic conditions. The shaded cells indicate movements which are predicted to operate below acceptable levels of service (LOS D). This scenario does *not* include site generated traffic.

Table 11. Intersection Capacity Analysis Results – Build-Out Year (2023) Background Traffic Conditions

Oakmont Drive at Lake Sharon Drive (Multiway Stop-Control)					
Peak Hour	Intersection	EB	WB	NB	SB
AM Peak	26.5 (D) ¹	15.2 (C)	15.9 (C)	40.0 (E), 0.83	30.2 (D)
PM Peak	19.4 (C)	14.2 (B)	14.6 (B)	26.6 (D)	21.2 (C)
Oakmont Drive at Ardglass Trail (Two-Way Stop-Control)					
Peak Hour	Intersection ²	EB	WB	NB Left	SB
AM Peak	---	19.5 (C)	---	8.5 (A)	0.0 (A)
PM Peak	---	13.9 (B)	---	8.3 (A)	0.0 (A)
Oakmont Drive at Creekside Drive (Multiway Stop-Control)					
Peak Hour	Intersection	EB	WB	NB	SB
AM Peak	15.1 (C)	10.3 (B)	19.6 (C)	12.0 (B)	13.9 (B)
PM Peak	10.6 (B)	9.0 (A)	11.2 (B)	10.4 (B)	10.3 (B)

¹ Delay in seconds/vehicle (Level of Service), *v/c ratio for LOS E or F*

² HCM methodology does not provide intersection-wide delay/level of service for TWSC analysis

As shown in Table 11, the intersection of Oakmont Drive and Lake Sharon Drive is predicted to begin to operate at LOS D during the AM peak hour, with the northbound approach operating at LOS E. It should be noted that the predicted volume to capacity ratio is for the shared through/right movement specifically. The northbound approach is also predicted to begin to operate at LOS D during the PM peak hour. Additionally, the southbound approach is predicted to begin to operate at LOS D during the AM peak hour.

All other study intersections are predicted to continue to operate at acceptable levels of service for projected 2023 background traffic volumes.

Build-Out (2023) Total Traffic Conditions – Proposed Site Plan

The proposed lane configurations shown in Figure 3 and the Build-Out Year (2023) Total traffic volumes for the proposed site plan (Figure 18) were used for this analysis. Note that all three proposed driveways were assumed to be in place for this scenario. **Table 12** presents the analysis results for the study intersections under Build-Out Year (2023) Total traffic conditions with the proposed site plan configuration. The shaded cells indicate movements which are predicted to operate below acceptable levels of service (LOS D).

Table 12. Intersection Capacity Analysis Results – Build-Out (2023) Total Traffic Conditions - Proposed Site Plan

Oakmont Drive at Lake Sharon Drive (Multiway Stop-Control)					
Peak Hour	Intersection	EB	WB	NB	SB
AM Peak	28.3 (D) ¹	16.3 (C)	16.7 (C)	44.7 (E), 0.86	32.1 (D)
PM Peak	20.4 (C)	15.1 (C)	15.4 (C)	28.1 (D)	22.4 (C)
Oakmont Drive at Ardglass Trail (Two-Way Stop-Control)					
Peak Hour	Intersection ²	EB	WB	NB Left	SB
AM Peak	---	19.8 (C)	---	8.4 (A)	0.0 (A)
PM Peak	---	13.9 (B)	---	8.3 (A)	0.0 (A)
Oakmont Drive at Creekside Drive (Multiway Stop-Control)					
Peak Hour	Intersection	EB	WB	NB	SB
AM Peak	15.3 (C)	10.4 (B)	20.0 (C)	12.2 (B)	14.1 (B)
PM Peak	10.8 (B)	9.0 (A)	11.5 (B)	10.5 (B)	10.5 (B)
Lake Sharon Drive at West Driveway (Two-Way Stop-Control)					
Peak Hour	Intersection ²	EB Left	WB	NB	SB
AM Peak	---	7.7 (A)	0.0 (A)	---	10.5 (B)
PM Peak	---	7.9 (A)	0.0 (A)	---	11.1 (B)
Lake Sharon Drive at Rye Road (Two-Way Stop-Control)					
Peak Hour	Intersection ²	EB Left	WB	NB	SB
AM Peak	---	7.7 (A)	0.0 (A)	---	10.3 (B)
PM Peak	---	8.0 (A)	0.0 (A)	---	10.7 (B)
Oakmont Drive at East Driveway (Two-Way Stop-Control)					
Peak Hour	Intersection ²	EB	WB	NB Left	SB
AM Peak	---	14.9 (B)	---	8.3 (A)	0.0 (A)
PM Peak	---	12.6 (B)	---	8.1 (A)	0.0 (A)

¹ Delay in seconds/vehicle (Level of Service), v/c ratio for LOS E or F

² HCM methodology does not provide intersection-wide delay/level of service for TWSC analysis

As shown in Table 12, the existing study intersections are predicted to operate similar to 2023 background conditions with the addition of site traffic volumes for Build-Out of the development, with some minimal delay added to most approaches.

With the lane and driveway configuration shown in the preliminary site plan (Figure 2), the proposed site driveways are predicted to operate at acceptable levels of service.

Build-Out (2023) Total Traffic Conditions – Alternative 1

The proposed lane configurations shown in Figure 12 and the Build-Out Year (2023) Total traffic volumes for Alternative 1 (Figure 19) were used for this analysis. Note that this scenario assumes only two driveways for the site, with both located on Lake Sharon Drive. **Table 13** presents the

analysis results for the study intersections under Build-Out Year (2023) Total traffic conditions with the Alternative 1 driveway configuration. The shaded cells indicate movements which are predicted to operate below acceptable levels of service (LOS D).

Table 13. Intersection Capacity Analysis Results – Build-Out (2023) Total Traffic Conditions - Alternative 1

Oakmont Drive at Lake Sharon Drive (Multiway Stop-Control)					
Peak Hour	Intersection	EB	WB	NB	SB
AM Peak	29.1 (D) ¹	16.6 (C)	16.9 (C)	46.3 (E), 0.87	33.5 (D)
PM Peak	20.9 (C)	15.3 (C)	15.7 (C)	28.6 (D)	23.6 (C)
Oakmont Drive at Ardglass Trail (Two-Way Stop-Control)					
Peak Hour	Intersection ²	EB	WB	NB Left	SB
AM Peak	---	19.8 (C)	---	8.4 (A)	0.0 (A)
PM Peak	---	13.9 (B)	---	8.3 (A)	0.0 (A)
Oakmont Drive at Creekside Drive (Multiway Stop-Control)					
Peak Hour	Intersection	EB	WB	NB	SB
AM Peak	15.3 (C)	10.4 (B)	20.0 (C)	12.2 (B)	14.1 (B)
PM Peak	10.8 (B)	9.0 (A)	11.5 (B)	10.5 (B)	10.5 (B)
Lake Sharon Drive at West Driveway (Two-Way Stop-Control)					
Peak Hour	Intersection ²	EB Left	WB	NB	SB
AM Peak	---	7.7 (A)	0.0 (A)	---	10.6 (B)
PM Peak	---	7.9 (A)	0.0 (A)	---	11.2 (B)
Lake Sharon Drive at Rye Road (Two-Way Stop-Control)					
Peak Hour	Intersection ²	EB Left	WB	NB	SB
AM Peak	---	7.7 (A)	0.0 (A)	---	10.8 (B)
PM Peak	---	8.0 (A)	0.0 (A)	---	11.4 (B)

¹ Delay in seconds/vehicle (Level of Service), v/c ratio for LOS E or F

² HCM methodology does not provide intersection-wide delay/level of service for TWSC analysis

As shown in Table 13, this driveway configuration is predicted to slightly increase delay for all movements at the intersection of Oakmont Drive and Lake Sharon Drive compared to the currently proposed site plan. Left turn movements entering the site are predicted to operate similar to the proposed site plan. Exiting movements are predicted to experience slight increases in delay with only two driveways.

No change was predicted at the intersections of Oakmont Drive at Ardglass Trail and at Creekside Drive compared to the proposed site plan.

Build-Out (2023) Total Traffic Conditions – Alternative 2

The proposed lane configurations shown in Figure 12 and the Build-Out Year (2023) Total traffic volumes for Alternative 2 (Figure 20) were used for this analysis. Note that this scenario assumes

all three driveways for the site, but with Rye Road operating as a one-way roadway (outbound only). **Table 14** presents the analysis results for the study intersections under Build-Out Year (2023) Total traffic conditions with the Alternative 2 driveway configuration. The shaded cells indicate movements which are predicted to operate below acceptable levels of service (LOS D).

Table 14. Intersection Capacity Analysis Results – Build-Out (2023) Total Traffic Conditions - Alternative 2

Oakmont Drive at Lake Sharon Drive (Multiway Stop-Control)					
Peak Hour	Intersection	EB	WB	NB	SB
AM Peak	28.8 (D) ¹	16.3 (C)	16.8 (C)	46.1 (E), 0.87	32.6 (D)
PM Peak	21.2 (C)	15.3 (C)	15.6 (C)	30.2 (D)	22.9 (C)
Oakmont Drive at Ardglass Trail (Two-Way Stop-Control)					
Peak Hour	Intersection ²	EB	WB	NB Left	SB
AM Peak	---	20.3 (C)	---	8.5 (A)	0.0 (A)
PM Peak	---	14.3 (B)	---	8.3 (A)	0.0 (A)
Oakmont Drive at Creekside Drive (Multiway Stop-Control)					
Peak Hour	Intersection	EB	WB	NB	SB
AM Peak	15.3 (C)	10.4 (B)	20.0 (C)	12.2 (B)	14.1 (B)
PM Peak	10.8 (B)	9.1 (A)	11.5 (B)	10.5 (B)	10.5 (B)
Lake Sharon Drive at West Driveway (Two-Way Stop-Control)					
Peak Hour	Intersection ²	EB Left	WB	NB	SB
AM Peak	---	7.8 (A)	0.0 (A)	---	10.6 (B)
PM Peak	---	8.0 (A)	0.0 (A)	---	11.4 (B)
Lake Sharon Drive at Rye Road (Two-Way Stop-Control)					
Peak Hour	Intersection ²	EB	WB	NB	SB
AM Peak	---	0.0 (A)	0.0 (A)	---	10.0 (B)
PM Peak	---	0.0 (A)	0.0 (A)	---	10.3 (B)
Oakmont Drive at East Driveway (Two-Way Stop-Control)					
Peak Hour	Intersection ²	EB	WB	NB Left	SB
AM Peak	---	15.1 (C)	---	8.3 (A)	0.0 (A)
PM Peak	---	13.0 (B)	---	8.2 (A)	0.0 (A)

¹ Delay in seconds/vehicle (Level of Service), v/c ratio for LOS E or F

² HCM methodology does not provide intersection-wide delay/level of service for TWSC analysis

As shown in Table 14, this driveway configuration is predicted to slightly increase delay for all movements at the intersection of Oakmont Drive and Lake Sharon Drive compared to the currently proposed site plan. Similar to Alternative 1, Oakmont Drive at Creekside is predicted to operate the same as with the proposed site plan driveway configuration. On the other hand, the intersection of Oakmont Drive at Ardglass Trail is predicted to experience some slight increase in delay compared to both the proposed site plan and Alternative 1.

Exiting movements from Rye Road are predicted to improve slightly, with two outbound lanes assumed when the roadway operates as one-way. Movements at the other two site driveways are predicted to experience slight increases in delay.

Comparison of Proposed Site Plan, Alternative 1, and Alternative 2

For further comparison of the three options for site access, predicted average performance measures by movement were compared side-by-side for each option. **Table 15** shows the predicted average delay by movement for each scenario.

As shown, minimal differences were predicted in vehicle delay between the three site access options. In general, the access shown in the proposed site plan results in the least amount of average delay for most movements.

In addition, **Table 16** shows the predicted 95th percentile queue lengths identified by *Synchro* for each movement at the above study intersections. Queue lengths were rounded up to the nearest whole number.

As shown, predicted 95th percentile queue lengths are similar between the three site access options. The two alternative options are predicted to add approximately one vehicle to the northbound approach of Oakmont Drive at Lake Sharon Drive during the PM peak hour compared to existing zoning.

The highest 95th percentile queue lengths are predicted to occur during the AM peak hour on Oakmont Drive at Lake Sharon Drive, with approximately nine vehicles in the northbound direction and approximately eight vehicles in the southbound direction. An eight-vehicle queue on the southbound approach would be approximately 200 feet. The proposed East Driveway will be located approximately 800 feet north of Lake Sharon Drive. Thus, the predicted queue length is not anticipated to extend to the site driveway on Oakmont Drive. It should also be noted that the 95th percentile queue only occurs for approximately two to three minutes during the peak 60 minute period.

Overall, all three site access options are predicted to operate at acceptable levels of service with minimal impact on the surrounding roadway network.

Lee Engineering recommends providing all three proposed access points for the Avilla Fairways site. Providing three full-access points, as shown in the proposed site plan, will result in the least amount of new traffic at the intersection of Lake Sharon Drive and Oakmont Drive. The new Rye Road extension will also result in improved ingress and egress access for the existing neighborhood to the north, particularly with both inbound and outbound movements allowed. Finally, providing three full-access points will provide the greatest safety benefit in terms of fire and emergency services access to the site. The Rye Road extension also provide additional fire and emergency access to the adjacent neighborhood to the north.

Table 15. Average Delay (Seconds per Vehicle) by Movement

Intersection	Movement	AM Peak Hour					PM Peak Hour				
		Existing	Background	Proposed Site	Alt 1	Alt 2	Existing	Background	Proposed Site	Alt 1	Alt 2
Oakmont Drive at Lake Sharon Drive (Multiway Stop-Control)	Intersection	20.7	26.5	28.3	29.1	28.8	16.3	19.4	20.4	20.9	21.2
	NB LT	11.7	12.3	12.7	12.8	12.7	11.5	12.1	12.8	13.0	12.8
	NB Thru/RT	29.3	41.3	46.6	48.2	47.9	21.3	28.0	30.3	30.9	32.4
	SB LT	14.2	15.4	16.3	16.3	16.4	12.2	12.9	13.4	13.4	13.5
	SB Thru/RT	26.1	35.3	38.0	39.6	38.6	18.7	22.9	24.4	25.7	25.0
	EB LT	14.1	15.2	15.5	16.0	15.8	12.9	13.8	13.9	14.2	14.6
	EB Thru/RT	14.5	15.9	17.4	17.7	17.4	13.7	15.0	16.1	16.3	16.2
	WB LT	15.7	17.3	18.0	18.3	18.1	14.3	15.7	16.1	16.3	16.4
WB Thru/RT	14.1	15.6	16.5	16.7	16.7	13.3	14.5	15.9	16.2	15.9	
Oakmont Drive at Ardglass Trail (Two-Way Stop Control)	Intersection	-	-	-	-	-	-	-	-	-	-
	NB LT	8.3	8.5	8.4	8.4	8.5	8.2	8.3	8.3	8.3	8.3
	NB Thru	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
	SB Thru/RT	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
	EB	17.5	19.5	19.8	19.8	20.3	13.1	13.9	13.9	13.9	14.3
Oakmont Drive at Creekside Drive (Multiway Stop-Control)	Intersection	13.5	15.1	15.3	15.3	15.3	10.1	10.6	10.8	10.8	10.8
	NB LT/Thru	11.7	12.6	12.8	12.8	12.8	10.6	11.2	11.4	11.4	11.4
	NB RT	10.8	11.5	11.7	11.7	11.7	8.1	8.4	8.4	8.4	8.4
	SB LT	12.0	12.7	12.8	12.8	12.8	10.0	10.3	10.4	10.4	10.4
	SB Thru/RT	13.3	14.6	14.8	14.8	14.8	9.8	10.3	10.5	10.5	10.5
	EB	9.9	10.3	10.4	10.4	10.4	8.8	9.0	9.0	9.0	9.1
	WB	16.7	19.6	20.0	20.0	20.0	10.6	11.2	11.5	11.5	11.5
Lake Sharon Drive at West Driveway (Two-Way Stop Control)	Intersection			-	-	-			-	-	-
	EB LT			7.7	7.7	7.8			7.9	7.9	8.0
	EB Thru			0.0	0.0	0.0			0.0	0.0	0.0
	WB Thru/RT			0.0	0.0	0.0			0.0	0.0	0.0
	SB			10.5	10.6	10.6			11.1	11.2	11.4
Lake Sharon Drive at Rye Road (Two-Way Stop Control)	Intersection			-	-	-			-	-	-
	EB LT			7.7	7.7				8.0	8.0	
	EB Thru			0.0	0.0	0.0			0.0	0.0	0.0
	WB Thru/RT			0.0	0.0	0.0			0.0	0.0	0.0
	SB			10.3	10.8	10.0			10.7	11.4	10.3
Oakmont Drive at East Driveway (Two-Way Stop Control)	Intersection			-		-			-		-
	NB LT			8.3		8.3			8.1		8.2
	NB Thru			0.0		0.0			0.0		0.0
	SB Thru/RT			0.0		0.0			0.0		0.0
	EB			14.9		15.1			12.6		13.0

Table 16. 95th Percentile Queue in Vehicles by Movement – Build-Out (2023) Total Traffic Scenarios

Intersection	Movement	AM Peak Hour					PM Peak Hour				
		Existing	Background	Proposed Site	Alt 1	Alt 2	Existing	Background	Proposed Site	Alt 1	Alt 2
Oakmont Drive at Lake Sharon Drive (Multiway Stop-Control)	NB LT	1	1	1	1	1	1	1	1	1	1
	NB Thru/RT	6	8	9	9	9	5	6	6	7	7
	SB LT	1	2	2	2	2	1	1	1	1	1
	SB Thru/RT	6	7	8	8	8	4	4	5	5	5
	EB LT	1	1	1	1	1	1	1	1	1	1
	EB Thru/RT	1	2	2	2	2	1	2	2	2	2
	WB LT	2	2	2	2	2	2	2	2	2	2
	WB Thru/RT	2	2	2	2	2	1	2	2	2	2
Oakmont Drive at Ardglass Trail (Two-Way Stop Control)	NB LT	0	0	0	0	0	1	1	0	0	1
	NB Thru	-	-	-	-	-	-	-	-	-	-
	SB Thru/RT	-	-	-	-	-	-	-	-	-	-
	EB	1	1	1	1	1	1	1	1	1	1
Oakmont Drive at Creekside Drive (Multiway Stop-Control)	NB LT/Thru	2	2	2	2	2	2	2	2	2	2
	NB RT	2	2	2	2	2	1	1	1	1	1
	SB LT	2	2	2	2	2	1	1	1	1	1
	SB Thru/RT	2	3	3	3	3	1	1	2	2	2
	EB	1	1	1	1	1	1	1	1	1	1
	WB	4	5	5	5	5	2	2	2	2	2
Lake Sharon Drive at West Driveway (Two-Way Stop Control)	EB LT			0	0	0			0	1	1
	EB Thru			-	-	-			-	-	-
	WB Thru/RT			-	-	-			-	-	-
	SB			1	1	1			1	1	1
Lake Sharon Drive at Rye Road (Two-Way Stop Control)	EB LT			0	0	-			1	1	-
	EB Thru			-	-	-			-	-	-
	WB Thru/RT			-	-	-			-	-	-
	SB			1	1	1			1	1	1
Oakmont Drive at East Driveway (Two-Way Stop Control)	NB LT			0		0			0		1
	NB Thru			-		-			-		-
	SB Thru/RT			-		-			-		-
	EB			1		1			1		1

Potential Mitigation Measures

As identified in the previous tables, the northbound approach of Oakmont Drive at Lake Sharon Drive is predicted to operate at LOS E by 2023 for both background and total traffic conditions. In order to mitigate this poor level of service, the City could consider installation of a northbound right turn lane on Oakmont Drive at Lake Sharon Drive. **Table 17** presents the resulting operation for both Build-Out Year (2023) Background conditions and Build-Out (2023) Total conditions with this mitigation measure. For the purposes of this analysis, only the total scenario based on the proposed site plan is shown. Similar improvements would also occur for the Alternative 1 and Alternative 2 scenarios.

**Table 17. Intersection Capacity Analysis Results –
Installation of Right Turn Lane on Oakmont Drive at Lake Sharon Drive**

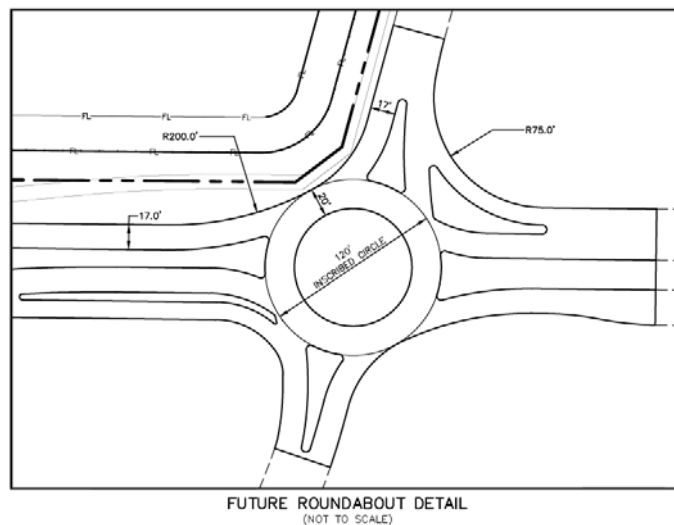
Oakmont Drive at Lake Sharon Drive (Multiway Stop-Control)						
Scenario	Peak Hour	Intersection	EB	WB	NB	SB
2023 Background	AM Peak	19.7 (C) ¹	14.4 (B)	15.0 (B)	19.4 (C)	26.5 (D)
	PM Peak	15.1 (C)	13.5 (B)	13.8 (B)	14.1 (B)	19.1 (C)
2023 Total (Proposed Site Plan)	AM Peak	20.5 (C)	15.3 (C)	15.7 (C)	20.6 (C)	27.6 (D)
	PM Peak	15.8 (C)	14.2 (B)	14.5 (B)	14.6 (B)	20.0 (C)

¹ Delay in seconds/vehicle (Level of Service), v/c ratio for LOS E or F

As shown, with the installation of a northbound right turn lane on Oakmont Drive at Lake Sharon Drive, all approaches are predicted to operate at acceptable levels of service.

On the other hand, a roundabout is being considered for evaluation for construction at the intersection of Oakmont Drive and Lake Sharon Drive in the future, based on the City of Corinth *Master Thoroughfare Plan* (dated March 30, 2021). A preliminary roundabout design is also shown on the preliminary site plan, as shown below for reference in **Figure 21**.

Figure 21. Preliminary Roundabout Design in Site Plan – Oakmont Drive at Lake Sharon Drive



As shown, the preliminary roundabout design is a single-lane roundabout with right-turn bypass lanes on the eastbound and westbound approaches. **Table 18** presents the resulting operation for both Build-Out Year (2023) Background conditions and Build-Out (2023) Total conditions with this preliminary roundabout design. For the purposes of this analysis, only the total scenario based on the proposed site plan is shown. Similar operation would also occur for the Alternative 1 and Alternative 2 scenarios. This analysis was performed with *SIDRA* roundabout software, with output sheets included in the Appendix.

**Table 18. Intersection Capacity Analysis Results –
Installation of Roundabout at Oakmont Drive at Lake Sharon Drive**

Oakmont Drive at Lake Sharon Drive (Roundabout)						
Scenario	Peak Hour	Intersection	EB	WB	NB	SB
2023 Background	AM Peak	7.7 (A) ¹	7.1 (A)	5.6 (A)	8.6 (A)	8.9 (A)
	PM Peak	6.9 (A)	6.2 (A)	5.8 (A)	7.9 (A)	7.8 (A)
2023 Total (Proposed Site Plan)	AM Peak	8.0 (A)	7.5 (A)	5.8 (A)	9.2 (A)	9.1 (A)
	PM Peak	7.2 (A)	6.3 (A)	6.1 (A)	8.3 (A)	8.3 (A)

¹ Delay in seconds/vehicle (Level of Service), *v/c ratio for LOS E or F*

As shown, with the installation of a roundabout at the intersection of Oakmont Drive at Lake Sharon Drive, all approaches are predicted to operate at LOS A for both background and total traffic conditions.

ACCESS MANAGEMENT ANALYSIS

Right Turn Lane Analysis

The proposed site access connections to the development were analyzed to determine if right turn lanes would be required.

Based on guidelines presented in TxDOT’s *Access Management Manual*, right turn deceleration lanes are considered under the following conditions:

- Right turn volumes greater than 50 vph (if posted speed limit greater than 45 mph)
- Right turn volumes greater than 60 vph (if posted speed limit less than/equal to 45 mph)

Table 17 summarizes the predicted right turn volumes at the proposed site access driveways for Build-Out (2023) Total traffic conditions for each site access option (Figures 18-20).

Table 19. Right Turn Deceleration Lane Analysis Results

Intersection	Scenario	Approach	Speed Limit (mph)	Volume (vph) AM (PM)	Threshold (vph)	Exceed Threshold? AM (PM)
West Driveway at Lake Sharon Drive	Proposed Site Plan	WB	40	3 (11)	60	No (No)
	Alternative 1			5 (15)		No (No)
	Alternative 2			9 (30)		No (No)
Rye Road at Lake Sharon Drive	Proposed Site Plan	WB	40	9 (27)	60	No (No)
	Alternative 1			11 (35)		No (No)
East Driveway at Oakmont Drive	Proposed Site Plan	SB	30	2 (7)	60	No (No)
	Alternative 2			2 (7)		No (No)

Based on the projected site traffic volumes, the predicted right turn volumes at the proposed site access driveways are not predicted to exceed the TxDOT threshold for the consideration of a right turn deceleration lane, and right turn lanes are not required at these locations for any of the site access scenarios.

Left Turn Lane Analysis

Based on the preliminary site plan (Figure 2), an eastbound left turn lane is proposed on Lake Sharon Drive at the Rye Road extension. The proposed left turn lane appears to include approximately 100 feet of storage, which should fully accommodate anticipated queues.

No eastbound left-turn lane is shown at the West Driveway. However, this driveway is proposed to be located at an existing median opening on a divided roadway and was included in all three site access scenarios. If feasible, it is recommended that an eastbound left-turn lane should be constructed on Lake Sharon Drive at West Driveway with development of the site. If the culvert crossing precludes construction of an eastbound left-turn lane, consideration should be given to making the western driveway right-in/right-out and lengthening the storage at the Rye Road access point.

East Driveway is located on the undivided Oakmont Drive. Based on the results shown in the previous section, minimal delay and queuing is anticipated for this movement. However, this location was further evaluated based on TxDOT’s procedure for determining whether left turn lanes should be considered on two-lane highways, as presented in TxDOT’s *Roadway Design Manual*. **Table 18** summarizes the information presented in Table 3-11 of this manual for a design speed of 40 mph. Note that design values for a 30-mph roadway are not provided in the manual, but the thresholds would be higher for a lower speed.

Table 20: Guide for Left Turn Lanes on Two-Lane Highways (TxDOT)

Opposing Volume (vph)	Advancing Volume (vph)			
	5% Left Turns	10% Left Turns	20% Left Turns	30% Left Turns
40 mph Design Speed				
800	330	240	180	160
600	410	305	225	200
400	510	380	275	245
200	640	470	350	305
100	720	515	390	340

*SOURCE: TxDOT Roadway Design Manual (Table 3-11)

Table 19 presents the evaluation results for a northbound left-turn deceleration lane on Oakmont Drive at East Driveway under Build-Out (2023) Total conditions. Analysis was performed for the proposed site plan and for Alternative 2 site access, as this driveway was removed for Alternative 1.

Table 21: Left Turn Deceleration Lane Analysis Results on Oakmont Drive

Peak Hour	Opposing (SB) Volume (vph)	Advancing (NB) Volume (vph)				Volume > Guideline?
		Percent Left Turns	Left Turns	Volume	Guideline ¹	
Build-Out (2023) Total Conditions – Proposed Site Plan						
AM Peak	349	1%	2	332	~650	No
PM Peak	299	2%	5	211	~660	No
Build-Out (2023) Total Conditions – Alternative 2						
AM Peak	349	1%	5	340	~650	No
PM Peak	299	6%	15	231	~550	No

¹For a 40-mph roadway

As shown, predicted left turn volumes at this driveway are low and guidelines are not met. Therefore, installation of a left-turn lane on Oakmont Drive at East Driveway is not recommended. In addition, striping a left-turn lane along Oakmont Drive may create undesirable lane use during school peak periods.

Sight Distance Evaluation

As part of this traffic analysis, the required intersection sight distance for motorists accessing the adjacent roadway from the proposed site driveways was calculated. The desired sight distance was estimated using the procedures developed by the American Association of State Highway and Transportation Officials (AASHTO) and published in the 2018 edition of *A Policy on Geometric Design of Highways and Streets*. At a stop-controlled location, the motorist should be able to see to the left and to the right to determine if and when adequate gaps exist to perform a left or right turn maneuver. **Table 20** presents the desirable and available intersection sight distance for vehicles exiting the proposed driveways.

Table 22. Intersection Sight Distance for Site Driveways

Major Roadway	Lake Sharon Drive		Oakmont Drive
Posted Speed Limit	40 mph		30 mph
Design Vehicle	Passenger Car		
Driveway	West Driveway	Rye Road	East Driveway
Approach	SB	SB	EB
Desired Intersection Sight Distance	510 feet	510 feet	335 feet
Available Sight Distance to the Left	730 feet	530 feet	680 feet
Available Sight Distance to the Right	>1,000 feet	650 feet	300 feet
Sight Distance Available > Required:			
To the Left	Yes	Yes	Yes
To the Right	Yes	Yes	No

As shown in Table 20, comparison of the field measurements of the available sight distance and the recommended sight distance indicates that adequate sight distance is provided for passenger cars at the proposed site access points on Lake Sharon Drive, based on conditions that existed at the time of the site visit and the posted speed limits.

However, sight distance from the East Driveway on Oakmont Drive is less than desired in looking to the right. Sight distance is obstructed due to the horizontal curvature on Oakmont Drive and due to vegetation.

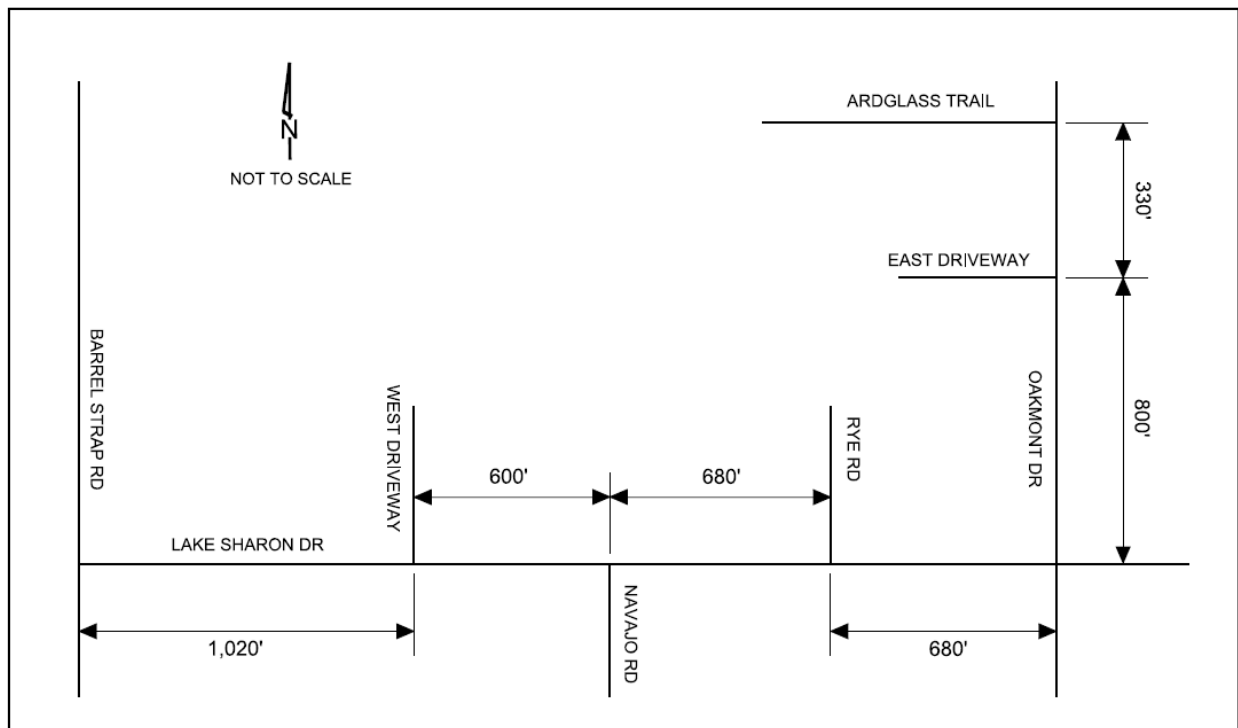
With development of the site, is recommended to remove all vegetation along the west edge of Oakmont Drive within the sight triangle south of East Driveway.

Driveway Spacing Evaluation

According to TxDOT's *Access Management Manual*, required access point spacing is determined based on the posted speed limit of the roadway. For a roadway with a posted speed limit of 30 mph or less, the required minimum access point spacing is 200 feet (Table 2-2, *Access Management Manual*). For a roadway with a posted speed limit of 40 mph, the required minimum access point spacing is 305 feet.

Approximate driveway spacing for the site is shown in **Figure 22**. As shown, proposed driveway spacing exceeds TxDOT requirements for all three site driveways.

Figure 22. Approximate Driveway Spacing for Avilla Fairways Site



ADDITIONAL CONSIDERATIONS

Several additional factors are anticipated to impact the transportation operation in the vicinity of the proposed site.

School Operation

As previously identified, an elementary school and a middle school are both located on the south side of Creekside Drive east of Oakmont Drive, approximately 1,000 feet north of the site. The proposed Avilla Fairways development is located within the attendance boundary for both schools. Both schools are part of Denton ISD. School hours for Hawk Elementary school are 7:40 AM to 3:05 PM. School hours for Crownover Middle School are 8:15 AM to 3:40 PM.

Sidewalks are currently available along both sides of Oakmont Drive for pedestrians walking to the schools. There are also marked crosswalks at the multiway stop-controlled intersection of Oakmont Drive and Creekside Drive.

Field observations were completed during both the morning peak and the afternoon school peak on April 22, 2021. Weather was overcast. Vehicles were observed queueing along the right-hand side of Oakmont Drive in both the northbound and southbound directions to enter the elementary school pick-up line. The northbound queue was observed to extend for approximately 1,500 feet to the south, past both Ardglass Trail and the proposed East Driveway location, prior to the end of the school day. By 3:06 PM, the queue was shortened to approximately 800 feet once pick-up operations began.

Oakmont Drive is approximately 36 feet wide, which allows enough room for through vehicles to bypass the vehicle queue.

A delay study was conducted for the eastbound approach of Ardglass Trail at Oakmont Drive to observe the actual delay in the field, which is likely impacted by the school operations. Delay study sheets are included in the Appendix. In the morning peak between 7:30 AM and 7:45 AM, average vehicle delay was observed to be approximately 19 seconds per vehicle. In the afternoon peak between 3:45 PM and 4:00 PM, average vehicle delay was observed to be approximately 24 seconds per vehicle. This observed delay is similar to the *Synchro* results for the AM peak but higher than the *Synchro* results for the PM peak. However, it should be noted that only the peak 15-minute period was observed rather than the entire peak hour.

City Planning Discussion

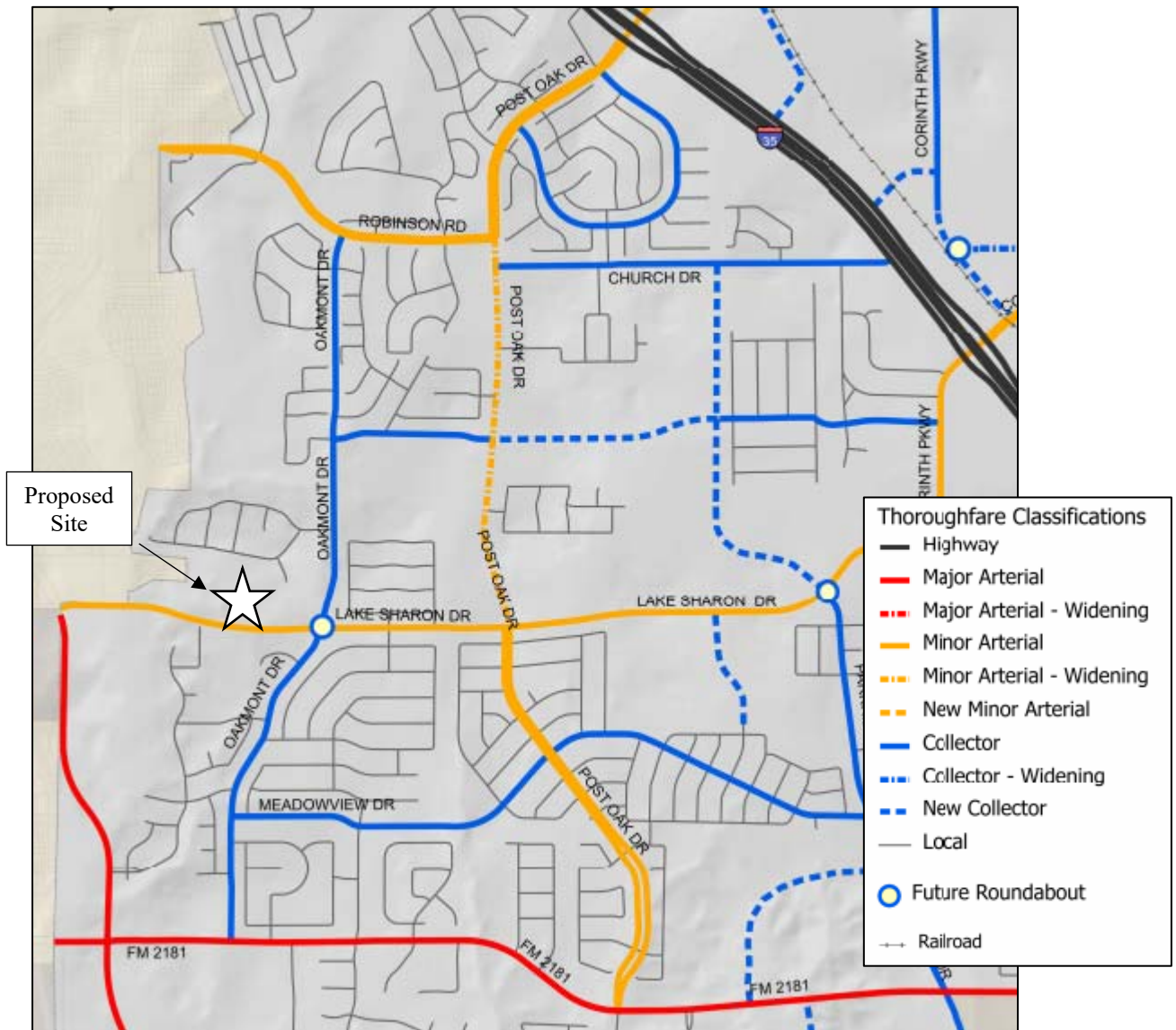
Several City planning documents were reviewed to identify any plans or impacts in the vicinity of the site. As previously stated, the City of Corinth *Master Thoroughfare Plan* (dated March 30, 2021) classifies the study roadways in the following manner:

- Lake Sharon Drive is classified as a Minor Arterial
- Oakmont Drive is classified as a Collector

- Creekside Drive is classified as a Collector.
- The intersection of Lake Sharon Drive and Oakmont Drive is planned as a future roundabout.

A portion of the City thoroughfare plan is shown in **Figure 23** for the area in the vicinity of the proposed site.

Figure 23. Thoroughfare Plan Clip



Source:

https://www.cityofcorinth.com/sites/default/files/fileattachments/planning_amp_development/page/2281/master_thoroughfare_plan_layout_3_30_2021.pdf

As shown, several traffic improvements are planned in the vicinity of the site, in addition to the future roundabout. Post Oak Drive is an existing parallel route to Oakmont Drive and is classified as a Minor Arterial. Post Oak Drive is planned to be widened in the future, which will provide additional north-south capacity. The portion of Post Oak Drive between Robinson Road and Lake Sharon Drive is currently a two-lane undivided roadway. Post Oak Drive is a four-lane divided roadway with a 36-foot wide median both south of Lake Sharon Drive and north of Robinson Road.

In addition, Creekside Drive is planned to be extended to the east in the future to tie in to Silver Meadow Lane. This extension will provide additional east-west capacity, especially for school traffic. Finally, Parkridge Drive is planned to be extended north to Church Drive, which will provide additional north-south capacity. Several potential roadway options for this area are illustrated in the City of Corinth *2040 Comprehensive Plan*. Based on the City of Corinth online capital improvement project list, a preliminary alignment for the extension of Parkridge Drive is being reviewed.

Based on the online list of capital improvement projects for the City, Lake Sharon Drive and Dobbs Road are under design to be realigned and provide an underpass at IH-35, which will allow easier access between Lake Sharon Drive and IH-35. This will also provide additional east-west connectivity across the freeway.

Relatively recent improvements in the study area include the extension of Lake Sharon Drive from Oakmont Drive to FM 2499/Barrel Strap Road, which has just recently opened. In addition, FM 2499/Barrel Strap Road was extended from FM 2181 to IH-35 within the previous five years, providing a significant north-south route just west of the proposed site. Barrel Strap Road is a six-lane divided Major Arterial.

Based on the City of Corinth *Future Land Use* map (dated January 20, 2021), much of the undeveloped land in the vicinity of the site is anticipated to be developed as 'Mixed-Residential'. Based on the *2040 Comprehensive Plan*, this land use is anticipated to include a range of single-family lots, multifamily lots, and neighborhood commercial, with an overall residential density of 6 to 10 units per acre. However, based on the thoroughfare plan, it appears that plans are already in place within the City to accommodate traffic generated by this future development.

Based on the location of undeveloped parcels, close vicinity of FM 2499/Barrel Strap Road, and planned widening of Post Oak Road, it does not appear likely that significantly higher traffic would occur on Oakmont Drive.

Higher volumes are likely to occur on Lake Sharon Drive; however, many of these trips will be oriented to IH-35 and not impact the proposed site. In addition, the extension of Creekside Drive to the east will provide additional capacity for future developments.

Overall, the existing and planned roadway network is anticipated to fully accommodate the site traffic volumes generated by the proposed Avilla Fairways development.

CONCLUSIONS AND RECOMMENDATIONS

The proposed Avilla Fairways site will be located on the northwest corner of the intersection of Lake Sharon Drive and Oakmont Drive in Corinth, Texas. Based on the analysis of the site plan and proposed characteristics of the proposed Avilla Fairways, the following conclusions and recommendations can be made:

- The proposed multifamily is estimated to be built-out by 2023. Based on the preliminary site plan, site access points will include two (2) full-access driveways on Lake Sharon Drive and one (1) full-access driveway on Oakmont Drive. One of the access points on Lake Sharon Drive is the proposed extension of Rye Road from the neighborhood to the north, which will provide that neighborhood with a second access point.
- Additional scenarios analyzed included:
 - Alternative 1 – only the two driveways on Lake Sharon Drive, with no access to Oakmont Drive.
 - Alternative 2 – all three access points included, but Rye Road is assumed to operate as a one-way (outbound) roadway.
- Based on ITE trip generation information, the Avilla Fairways is predicted to generate approximately 1,586 trips in a daily basis, including approximately 99 trips during the AM peak hour and approximately 117 trips during the PM peak hour.
- Development allowable under the existing zoning for the site could generate significantly more traffic than the proposed Avilla Fairways development. Estimates of different land use possibilities indicate that the site could generate up to 8,195 trips on a daily basis, up to 979 trips in the AM peak hour, and up to 517 trips in the PM peak hour, depending on land use. Thus, the proposed development is a significantly less intense traffic generator than could be allowed under existing zoning.
- The collected traffic volumes were adjusted by a factor of 1.09 during the AM peak hour and 1.17 during the PM peak hour to account for the COVID-19 pandemic. Background (non-site) traffic volumes for the study area intersections and roadways were estimated by growing the adjusted existing traffic volumes at an annual rate of four percent (4%) until the Build-Out Year (2023).

Intersection Capacity Analysis

The existing study intersections analyzed include:

- Oakmont Drive at Lake Sharon Drive;
- Oakmont Drive at Ardglass Trail; and
- Oakmont Drive at Creekside Drive.

The results of the intersection capacity analysis are described in the following section.

Existing (2021) Conditions

- All three study intersections are predicted to operate at acceptable levels of service for existing conditions.

Build-Out Year (2023) Background Conditions

- The intersection of Oakmont Drive and Lake Sharon Drive is predicted to begin to operate at LOS D overall during the AM peak hour, with the northbound approach operating at LOS E. The predicted volume to capacity ratio for the northbound approach is 0.83 during the AM peak hour.
- If a northbound right-turn lane were installed on Oakmont Drive at Lake Sharon Drive, all approaches would be predicted to operate at acceptable levels of service.
- Furthermore, installation of a roundabout is anticipated at this intersection in the future, based on the City of Corinth *Master Thoroughfare Plan*. Based on the preliminary roundabout design shown in the site plan (Figure 2), this intersection is predicted to operate at LOS A as a single-lane roundabout.

Build-Out Year (2023) Total Conditions

- The existing study intersections are predicted to operate similar to 2023 background conditions with the addition of site traffic volumes for Build-Out of the development, with some minimal delay added to most approaches. This is true for all three site access scenarios, with relatively minor differences between scenarios.
- The proposed site driveways are predicted to operate at acceptable levels of service for all three access scenarios, with relatively minor differences between scenarios as well.
- In general, the access shown in the proposed site plan results in the least amount of average delay and the shortest queue lengths for most movements. However, all three site access options are predicted to operate at acceptable levels of service with minimal impact on the surrounding roadway network.
- It is recommended to consider providing the greatest amount of site access, as shown in the proposed site plan. With three full-access site driveways, predicted volumes at the intersection of Lake Sharon Drive and Oakmont Drive will be minimized. Three access points also provides the greatest safety benefit in terms of fire access.

Access Management

- Based on the projected traffic volumes, installation of a right-turn deceleration lane is not recommended at any of the site driveways for any of the access options.
- Based on the preliminary site plan (Figure 2), an eastbound left turn lane is proposed on Lake Sharon Drive at the Rye Road extension. The proposed left turn lane appears to include approximately 100 feet of storage, which should fully accommodate anticipated queues.
- If feasible, it is recommended to install an eastbound left turn lane on Lake Sharon Drive at the West Driveway as well, as this driveway is proposed to be located at an existing

median opening on a divided roadway and was included in all three site access scenarios.

- If the culvert crossing precludes construction of an eastbound left-turn lane, consideration should be given to making the western driveway right-in/right-out and lengthening the storage at the Rye Road access point.
- A northbound left turn lane is not recommended on Oakmont Drive at East Driveway based on both traffic volumes and lane usage during school peak periods.
- Comparison of the field measurements of the available sight distance and the recommended sight distance indicates that adequate sight distance is provided for passenger cars at proposed site access points on Lake Sharon Drive based on conditions at the time of the site visit and the posted speed limits.
- However, sight distance from the East Driveway on Oakmont Drive is less than desired looking to the right due to the horizontal curvature of Oakmont Drive and vegetation.
 - With development of the site, is recommended to remove all vegetation along the west edge of Oakmont Drive within the sight triangle south of East Driveway.
- The proposed driveway spacing for the site exceeds TxDOT requirements.

School Operation

- Hawk Elementary School is located approximately 1,000 feet north of the site on the southeast corner of the intersection of Oakmont Drive and Creekside Drive. Additionally, Crownover Middle School is located on the east side of the elementary school south of Creekside Drive. The proposed site is located within the attendance boundary for both schools.
- Sidewalks are currently available along both sides of Oakmont Drive for pedestrians walking to the schools. There are also marked crosswalks at the multiway stop-controlled intersection of Oakmont Drive and Creekside Drive.
- Field observations indicate that northbound vehicle queues on Oakmont Drive extend approximately 1,500 feet south of the entrance to the school pick-up line during the afternoon pick-up period. This queue length extends beyond both Ardglass Trail and the proposed East Driveway.
 - Field observations indicate that the queue length is reduced by approximately fifty percent shortly after pick-up operations begin.
 - Oakmont Drive is approximately 36 feet wide, which appears to allow enough room for through vehicles to bypass the vehicle queue. Vehicles appear to queue along the right-hand curb.
- A delay study conducted for the eastbound approach of Ardglass Trail showed similar delay results compared to the *Synchro* intersection analysis during the AM peak. PM peak delay was observed to be higher than shown in *Synchro*. However, only the peak 15-minute period was observed.

City Planning Discussion

- Relatively recent roadway improvements in the vicinity of the site include the extension of Lake Sharon Drive to FM 2499 and the extension of FM 2499/Barrel Strap Road from FM 281 to IH-35.
- Based on the City *Master Thoroughfare Plan, 2040 Comprehensive Plan*, and online list of capital improvement projects, several roadway improvements are anticipated in the vicinity of the site in the future:
 - Installation of a roundabout at the intersection of Oakmont Drive and Lake Sharon Drive.
 - Widening of Post Oak Drive between Lake Sharon Drive and Robinson Road to a four-lane divided roadway, providing additional north-south capacity.
 - Extension of Creekside Drive from Post Oak Road to tie into Silver Meadow Lane, providing additional east-west capacity.
 - Extension of Parkridge Drive from Lake Sharon Drive to Church Drive, providing additional north-south capacity.
 - Realignment of Lake Sharon Drive and Dobbs Road with installation of an underpass at IH-35, providing additional east-west connectivity and access to IH-35.
- Based on these planned improvements, it appears that additional infrastructure is being planned for to accommodate future development of vacant parcels.
- Based on the location of undeveloped parcels and adjacent north-south roadway capacity, it is likely traffic volumes on Oakmont Drive will not significantly increase in the future.
- While traffic will likely increase on Lake Sharon Drive, additional capacity will be provided by the extension of Creekside Drive and much of the future traffic will be oriented towards IH-35.
- Overall, the existing and planned roadway network is anticipated to fully accommodate the site traffic volumes generated by the proposed Avilla Fairways development.

APPENDIX

Appendix A: Preliminary Site Plan

Appendix B: Raw Traffic Count Data

Appendix C: Trip Generation Calculations for Existing Zoning

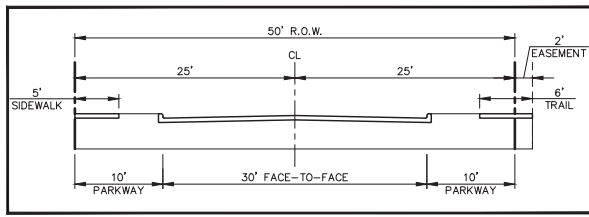
Appendix D: *Synchro* Output Sheets

Appendix E: *SIDRA* Output Sheets

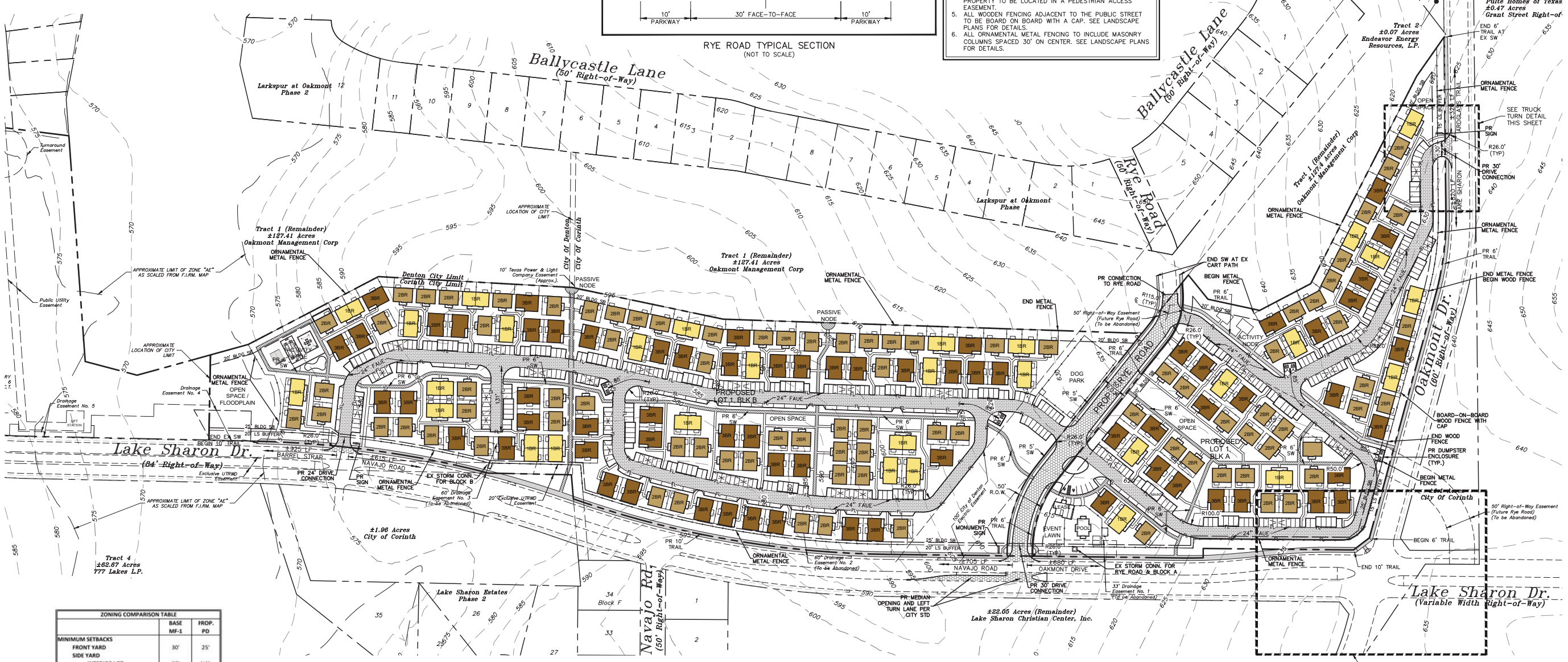
Appendix F: Delay Study

Appendix A: Preliminary Site Plan

2-17-21 (REV) PD CONCEPT PLAN to show increased 25' setbacks instead of 20'
Also see 2-10-21 PD CONCEPT PLAN with Staff Comments

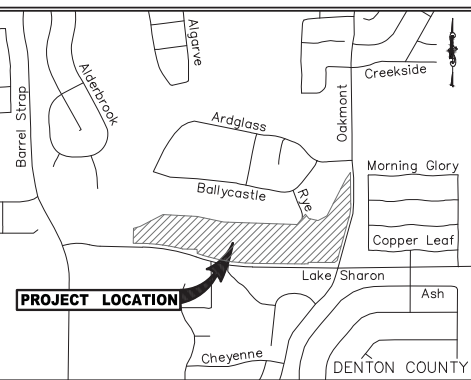


- CONCEPT PLAN NOTES**
1. ALL DIMENSIONS ARE TO FACE OF CURB UNLESS OTHERWISE NOTED.
 2. ALL FINAL ROAD ALIGNMENTS, EASEMENTS, BUILDING LOCATIONS, AMENITIES TO BE DETERMINED AT THE SITE PLAN PROCESS.
 3. ALL STANDARD PARKING SPACES TO BE 9'X18' AND ALL PARALLEL PARKING SPACES TO BE 8'X22'.
 4. ALL PUBLIC TRAIL/SIDEWALKS LOCATED ON PRIVATE PROPERTY TO BE LOCATED IN A PEDESTRIAN ACCESS EASEMENT.
 5. ALL WOODEN FENCING ADJACENT TO THE PUBLIC STREET TO BE BOARD ON BOARD WITH A CAP. SEE LANDSCAPE PLANS FOR DETAILS.
 6. ALL ORNAMENTAL METAL FENCING TO INCLUDE MASONRY COLUMNS SPACED 30' ON CENTER. SEE LANDSCAPE PLANS FOR DETAILS.



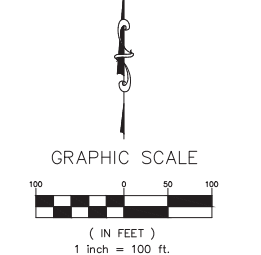
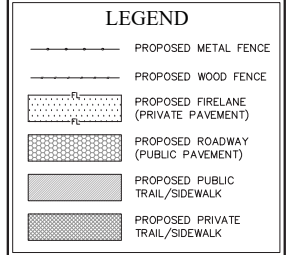
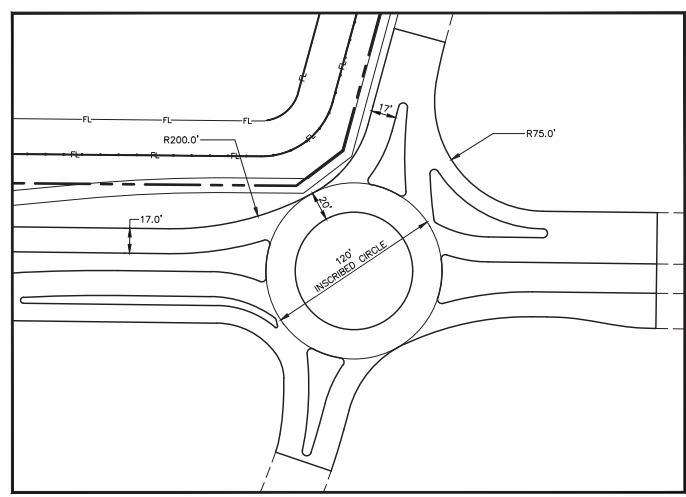
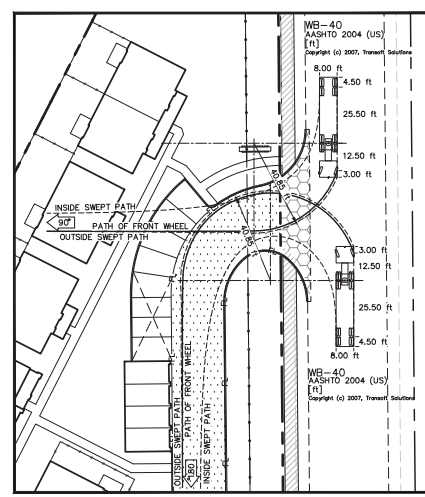
ZONING COMPARISON TABLE

	BASE MF-1	PROP. PD
MINIMUM SETBACKS		
FRONT YARD	30'	25'
SIDE YARD	30'	N/A
INTERIOR LOT CORNER LOT	30'	N/A
REAR YARD	30'	20'
GARAGE	N/A	N/A
MINIMUM LOT DIMENSIONS		
LOT AREA (SF)	40,000	40,000
LOT WIDTH	200'	200'
LOT DEPTH	200'	200'
DENSITY (MAX. DU/A)	12	9
MINIMUM FLOOR AREA (SF PER DU)	1,050	680
MAX. BUILDING HT. (FT/STORIES)	35' / 2	30' / 1
MAX. BUILDING AREA (TOTAL)	40%	23%



SITE DATA SUMMARY TABLE

EXISTING ZONING/USE:	PD-6, PD-34 / VACANT
PROPOSED ZONING:	PD W/MF-1 BASE ZONING
PROPOSED USE:	RESIDENTIAL COTTAGE COMMUNITY
NUMBER OF LOTS:	2 (EXISTING); 2 (PROPOSED)
LOT COVERAGE:	22.8%
LOT AREA:	AC. 24.595 SQ. FT. 1071,370
GROSS AREA:	0.698 30,389
ROW DEDICATION:	23.898 1040,981
NET AREA:	9.00 UNITS / ACRE (w/o ROW Ded.)
DENSITY:	
PROPOSED BUILDINGS (1 STORY):	AREA NO. BLDGS NO. UNITS TOTAL
1-BEDROOM (DUPLX)	1,342 30 60 40,200
2-BEDROOM	1,022 87 87 88,914
3-BEDROOM	1,307 68 68 88,876
STANDARD GARAGE, DETACHED*	831 20 - 16,620
STANDARD GARAGE W/HANDICAP, DETACHED	682 3 - 2,046
LEASING CENTER	1,131 1 - 1,131
TOTALS:	209 215 257,847
PARKING REQUIRED:	UNITS REQUIRED PROPOSED
1 BEDROOM (2 SPACES REQ'D; 2 SPACES PROP.)	60 120 120
2 BEDROOM (3 SPACES REQ'D; 2 SPACES PROP.)	87 261 174
3 BEDROOM (4 SPACES REQ'D; 2 SPACES PROP.)	68 272 136
TOTALS:	215 653 430
PARKING PROVIDED:	STANDARD SPACES 144
STANDARD SPACES	CARPPOOL SPACES 3
HANDICAP SPACES	STANDARD GARAGE SPACES 87
HANDICAP GARAGE SPACES	HANDICAP GARAGE SPACES 3
TOTALS:	471
GARAGE SPACES PER UNIT:	2.19
OPEN SPACE AREA:	1,800 SQ. FT. (5% %)
LANDSCAPE AREA:	5,000 SQ. FT. (17.4% %)
IMPERVIOUS AREA:	540,349 SQ. FT. (52.0% %)
LOADING SPACE:	NOT REQUIRED
OUTSIDE STORAGE:	NONE PROPOSED
CONSTRUCTION DATES:	Q3 2021 - Q3 2021



OWNER/DEVELOPER:
NexMetro Communities
 6688 N. Central Expressway, Suite 275
 Dallas, Texas 75206
 Tel. No. (602) 559-9388
 Contact: Jason Flory, P.E.

ENGINEER:
Lincoln Consulting & Engineering, Inc.
 P.O. Box 1176
 Wylie, Texas 75298
 Tel. No. (214) 215-5066
 Contact: Joshua A. Lincoln, P.E.

PD CONCEPT PLAN
AVILLA FAIRWAYS
LAKE SHARON @ OAKMONT
24.595 ACRE TRACT OF LAND
 BERRY MERCHANT SURVEY, ABSTRACT NO. 800
 A.H. SERREN SURVEY, ABSTRACT NO. 1198
CITY OF CORINTH,
DENTON COUNTY, TEXAS
 FEBRUARY 10, 2021

Scale: 1" = 100'
 Designed by: JAL
 Drawn by: JAL
 Checked by: JAL
 Date: February 17, 2021
 Project No.: 1015-003

Section S, Item 7.

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PRELIMINARY
 FOR REVIEW ONLY
 NOT FOR CONSTRUCTION
LINCOLN CONSULTING & ENGINEERING
 ENGINEER: JOSHUA A. LINCOLN, P.E.
 FIRM NO. 2227
 P.E. NO. 114389 Date: 2/17/2021

AVILLA FAIRWAYS
LAKE SHARON @ OAKMONT
24.595 ACRE TRACT OF LAND
 City of Corinth, Denton County, Texas

PD CONCEPT PLAN

CP

**Appendix B:
Raw Traffic Count Data**

GRAM Traffic NTX Inc.
1120 W. Lovers Lane
Arlington, Texas, United States 76013
817.265.8968

Count Name: OAKMONT DR @ LAKE SHARON DR
Site Code:
Start Date: 04/13/2021
Page No: 1

Turning Movement Data

Start Time	OAKMONT DR Southbound					LAKE SHARON DR Westbound					OAKMONT DR Northbound					LAKE SHARON DR Eastbound					Int. Total
	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	
6:30 AM	0	5	0	0	5	8	11	2	0	21	3	4	7	0	14	0	8	2	0	10	50
6:45 AM	5	6	2	0	13	16	2	1	0	19	5	6	10	0	21	2	10	1	0	13	66
Hourly Total	5	11	2	0	18	24	13	3	0	40	8	10	17	0	35	2	18	3	0	23	116
7:00 AM	7	9	4	0	20	12	15	5	0	32	5	13	6	0	24	7	14	0	0	21	97
7:15 AM	11	17	4	0	32	12	12	14	0	38	3	21	16	0	40	8	19	1	0	28	138
7:30 AM	28	45	11	0	84	21	15	32	0	68	5	65	13	1	84	17	27	2	0	46	282
7:45 AM	30	64	20	1	115	26	20	13	1	60	2	42	29	1	74	13	44	4	0	61	310
Hourly Total	76	135	39	1	251	71	62	64	1	198	15	141	64	2	222	45	104	7	0	156	827
8:00 AM	8	50	10	0	68	25	36	6	0	67	1	36	11	0	48	10	19	7	0	36	219
8:15 AM	6	24	8	0	38	19	8	5	0	32	6	10	8	0	24	4	30	0	0	34	128
8:30 AM	6	13	2	0	21	28	21	5	0	54	3	10	19	0	32	5	15	3	0	23	130
8:45 AM	1	11	2	0	14	15	11	0	0	26	3	10	11	0	24	4	15	5	0	24	88
Hourly Total	21	98	22	0	141	87	76	16	0	179	13	66	49	0	128	23	79	15	0	117	565
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
3:00 PM	24	54	12	0	90	12	13	7	0	32	4	20	19	0	43	3	17	4	0	24	189
3:15 PM	5	14	6	0	25	13	19	7	0	39	2	23	24	0	49	12	19	6	0	37	150
3:30 PM	6	16	7	0	29	12	18	6	1	37	9	28	25	0	62	12	30	4	0	46	174
3:45 PM	12	62	18	0	92	28	43	8	0	79	4	23	28	0	55	6	25	8	0	39	265
Hourly Total	47	146	43	0	236	65	93	28	1	187	19	94	96	0	209	33	91	22	0	146	778
4:00 PM	6	25	9	0	40	15	25	4	0	44	4	21	26	0	51	10	25	5	0	40	175
4:15 PM	9	19	4	0	32	17	21	7	0	45	2	19	24	0	45	12	29	3	0	44	166
4:30 PM	4	15	6	0	25	19	20	2	0	41	4	27	23	0	54	13	28	7	0	48	168
4:45 PM	7	10	4	0	21	9	21	8	0	38	2	17	33	0	52	7	38	8	0	53	164
Hourly Total	26	69	23	0	118	60	87	21	0	168	12	84	106	0	202	42	120	23	0	185	673
5:00 PM	6	17	8	0	31	19	21	5	0	45	5	15	30	0	50	9	18	7	0	34	160
5:15 PM	8	14	1	0	23	17	22	4	0	43	8	20	23	0	51	11	36	5	0	52	169
5:30 PM	5	17	5	0	27	15	27	5	0	47	11	10	36	0	57	7	24	4	0	35	166
5:45 PM	3	17	3	0	23	13	25	3	0	41	6	11	24	0	41	5	18	7	0	30	135
Hourly Total	22	65	17	0	104	64	95	17	0	176	30	56	113	0	199	32	96	23	0	151	630
6:00 PM	6	9	2	0	17	7	29	3	0	39	4	8	12	0	24	3	23	2	0	28	108
6:15 PM	4	9	1	0	14	13	15	2	0	30	7	11	10	0	28	4	18	4	0	26	98
6:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Grand Total	207	542	149	1	899	391	470	154	2	1017	108	470	467	2	1047	184	549	99	0	832	3795
Approach %	23.0	60.3	16.6	0.1	-	38.4	46.2	15.1	0.2	-	10.3	44.9	44.6	0.2	-	22.1	66.0	11.9	0.0	-	-
Total %	5.5	14.3	3.9	0.0	23.7	10.3	12.4	4.1	0.1	26.8	2.8	12.4	12.3	0.1	27.6	4.8	14.5	2.6	0.0	21.9	-
Lights	205	531	145	1	882	386	468	150	2	1006	108	459	463	2	1032	181	547	98	0	826	3746
% Lights	99.0	98.0	97.3	100.0	98.1	98.7	99.6	97.4	100.0	98.9	100.0	97.7	99.1	100.0	98.6	98.4	99.6	99.0	-	99.3	98.7
Mediums	2	11	4	0	17	5	2	3	0	10	0	11	4	0	15	3	2	1	0	6	48
% Mediums	1.0	2.0	2.7	0.0	1.9	1.3	0.4	1.9	0.0	1.0	0.0	2.3	0.9	0.0	1.4	1.6	0.4	1.0	-	0.7	1.3

Articulated Trucks	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	1	
% Articulated Trucks	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.6	0.0	0.1	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0

GRAM Traffic NTX Inc.
 1120 W. Lovers Lane
 Arlington, Texas, United States 76013
 817.265.8968

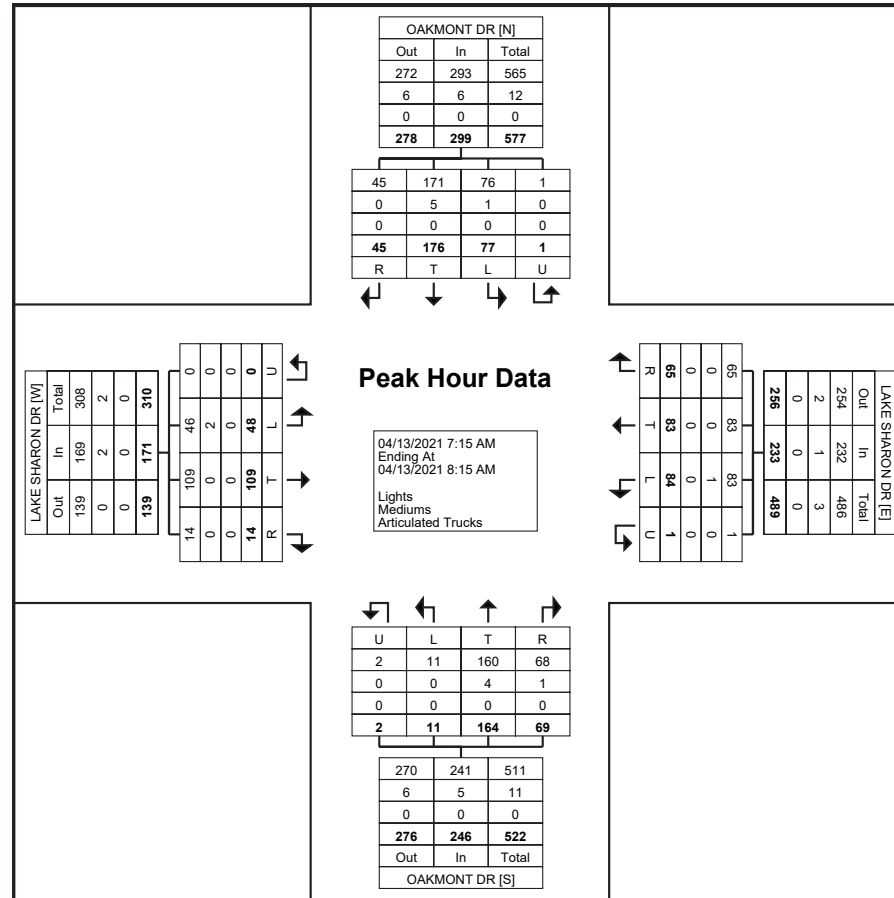
Count Name: OAKMONT DR @ LAKE SHARON
 DR
 Site Code:
 Start Date: 04/13/2021
 Page No: 4

Turning Movement Peak Hour Data (7:15 AM)

Start Time	OAKMONT DR Southbound					LAKE SHARON DR Westbound					OAKMONT DR Northbound					LAKE SHARON DR Eastbound					Int. Total
	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	
7:15 AM	11	17	4	0	32	12	12	14	0	38	3	21	16	0	40	8	19	1	0	28	138
7:30 AM	28	45	11	0	84	21	15	32	0	68	5	65	13	1	84	17	27	2	0	46	282
7:45 AM	30	64	20	1	115	26	20	13	1	60	2	42	29	1	74	13	44	4	0	61	310
8:00 AM	8	50	10	0	68	25	36	6	0	67	1	36	11	0	48	10	19	7	0	36	219
Total	77	176	45	1	299	84	83	65	1	233	11	164	69	2	246	48	109	14	0	171	949
Approach %	25.8	58.9	15.1	0.3	-	36.1	35.6	27.9	0.4	-	4.5	66.7	28.0	0.8	-	28.1	63.7	8.2	0.0	-	-
Total %	8.1	18.5	4.7	0.1	31.5	8.9	8.7	6.8	0.1	24.6	1.2	17.3	7.3	0.2	25.9	5.1	11.5	1.5	0.0	18.0	-
PHF	0.642	0.688	0.563	0.250	0.650	0.808	0.576	0.508	0.250	0.857	0.550	0.631	0.595	0.500	0.732	0.706	0.619	0.500	0.000	0.701	0.765
Lights	76	171	45	1	293	83	83	65	1	232	11	160	68	2	241	46	109	14	0	169	935
% Lights	98.7	97.2	100.0	100.0	98.0	98.8	100.0	100.0	100.0	99.6	100.0	97.6	98.6	100.0	98.0	95.8	100.0	100.0	-	98.8	98.5
Mediums	1	5	0	0	6	1	0	0	0	1	0	4	1	0	5	2	0	0	0	2	14
% Mediums	1.3	2.8	0.0	0.0	2.0	1.2	0.0	0.0	0.0	0.4	0.0	2.4	1.4	0.0	2.0	4.2	0.0	0.0	-	1.2	1.5
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Articulated Trucks	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0

GRAM Traffic NTX Inc.
 1120 W. Lovers Lane
 Arlington, Texas, United States 76013
 817.265.8968

Count Name: OAKMONT DR @ LAKE SHARON
 DR
 Site Code:
 Start Date: 04/13/2021
 Page No: 5



Turning Movement Peak Hour Data Plot (7:15 AM)

GRAM Traffic NTX Inc.
 1120 W. Lovers Lane
 Arlington, Texas, United States 76013
 817.265.8968

Count Name: OAKMONT DR @ LAKE SHARON
 DR
 Site Code:
 Start Date: 04/13/2021
 Page No: 6

Turning Movement Peak Hour Data (3:30 PM)

Start Time	OAKMONT DR Southbound					LAKE SHARON DR Westbound					OAKMONT DR Northbound					LAKE SHARON DR Eastbound					Int. Total
	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	
3:30 PM	6	16	7	0	29	12	18	6	1	37	9	28	25	0	62	12	30	4	0	46	174
3:45 PM	12	62	18	0	92	28	43	8	0	79	4	23	28	0	55	6	25	8	0	39	265
4:00 PM	6	25	9	0	40	15	25	4	0	44	4	21	26	0	51	10	25	5	0	40	175
4:15 PM	9	19	4	0	32	17	21	7	0	45	2	19	24	0	45	12	29	3	0	44	166
Total	33	122	38	0	193	72	107	25	1	205	19	91	103	0	213	40	109	20	0	169	780
Approach %	17.1	63.2	19.7	0.0	-	35.1	52.2	12.2	0.5	-	8.9	42.7	48.4	0.0	-	23.7	64.5	11.8	0.0	-	-
Total %	4.2	15.6	4.9	0.0	24.7	9.2	13.7	3.2	0.1	26.3	2.4	11.7	13.2	0.0	27.3	5.1	14.0	2.6	0.0	21.7	-
PHF	0.688	0.492	0.528	0.000	0.524	0.643	0.622	0.781	0.250	0.649	0.528	0.813	0.920	0.000	0.859	0.833	0.908	0.625	0.000	0.918	0.736
Lights	32	118	35	0	185	69	107	24	1	201	19	89	103	0	211	40	107	19	0	166	763
% Lights	97.0	96.7	92.1	-	95.9	95.8	100.0	96.0	100.0	98.0	100.0	97.8	100.0	-	99.1	100.0	98.2	95.0	-	98.2	97.8
Mediums	1	4	3	0	8	3	0	0	0	3	0	2	0	2	2	0	2	1	0	3	16
% Mediums	3.0	3.3	7.9	-	4.1	4.2	0.0	0.0	0.0	1.5	0.0	2.2	0.0	-	0.9	0.0	1.8	5.0	-	1.8	2.1
Articulated Trucks	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	0	0	0	0	0	1
% Articulated Trucks	0.0	0.0	0.0	-	0.0	0.0	0.0	4.0	0.0	0.5	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	-	0.0	0.1

GRAM Traffic NTX Inc.
1120 W. Lovers Lane
Arlington, Texas, United States 76013
817.265.8968

Count Name: OAKMONT DR @ ARDGLASS
TRL
Site Code:
Start Date: 04/13/2021
Page No: 1

Turning Movement Data

Start Time	OAKMONT DR Southbound				Eastbound St. Westbound				OAKMONT DR Northbound				ARDGLASS TRL Eastbound				Int. Total				
	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	Left		Thru	Right	U-Turn	App. Total
6:30 AM	0	6	0	0	6	0	0	0	0	0	0	6	0	0	6	3	0	1	0	4	16
6:45 AM	0	9	1	0	10	0	0	0	0	0	1	10	0	0	11	1	0	5	0	6	27
Hourly Total	0	15	1	0	16	0	0	0	0	0	1	16	0	0	17	4	0	6	0	10	43
7:00 AM	0	15	0	0	15	0	0	0	0	0	0	23	0	0	23	8	0	6	0	14	52
7:15 AM	0	29	2	0	31	0	0	0	0	0	3	43	0	0	46	9	0	2	0	11	88
7:30 AM	0	81	12	0	93	0	0	0	0	0	1	115	0	0	116	10	0	5	0	15	224
7:45 AM	0	106	2	0	108	0	0	0	0	0	0	69	0	0	69	6	0	8	0	14	191
Hourly Total	0	231	16	0	247	0	0	0	0	0	4	250	0	0	254	33	0	21	0	54	555
8:00 AM	0	67	3	0	70	0	0	0	0	0	5	45	0	0	50	6	0	1	0	7	127
8:15 AM	0	28	1	0	29	0	0	0	0	0	6	12	0	0	18	2	0	5	0	7	54
8:30 AM	0	17	1	0	18	0	0	0	0	0	4	15	0	0	19	2	0	2	0	4	41
8:45 AM	0	11	1	0	12	0	0	0	0	0	1	13	0	0	14	5	0	3	0	8	34
Hourly Total	0	123	6	0	129	0	0	0	0	0	16	85	0	0	101	15	0	11	0	26	256
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
3:00 PM	0	82	5	0	87	0	0	0	0	0	4	39	0	0	43	6	0	3	0	9	139
3:15 PM	0	22	0	0	22	0	0	0	0	0	7	35	0	0	42	9	0	5	0	14	78
3:30 PM	0	31	8	0	39	0	0	0	0	0	0	44	0	0	44	7	0	3	0	10	93
3:45 PM	0	81	9	0	90	0	0	0	0	0	8	32	0	0	40	6	0	7	0	13	143
Hourly Total	0	216	22	0	238	0	0	0	0	0	19	150	0	0	169	28	0	18	0	46	453
4:00 PM	0	33	8	0	41	0	0	0	0	0	6	28	0	0	34	2	0	4	0	6	81
4:15 PM	0	29	6	0	35	0	0	0	0	0	8	30	0	0	38	2	0	2	0	4	77
4:30 PM	0	22	5	0	27	0	0	0	0	0	6	36	0	0	42	3	0	3	0	6	75
4:45 PM	0	18	1	0	19	0	0	0	0	0	4	28	0	0	32	2	0	5	0	7	58
Hourly Total	0	102	20	0	122	0	0	0	0	0	24	122	0	0	146	9	0	14	0	23	291
5:00 PM	0	24	8	0	32	0	0	0	0	0	4	25	0	0	29	7	0	5	0	12	73
5:15 PM	0	23	5	0	28	0	0	0	0	0	3	31	0	0	34	0	0	2	0	2	64
5:30 PM	0	23	3	0	26	0	0	0	0	0	7	16	0	0	23	3	0	4	0	7	56
5:45 PM	0	20	5	0	25	0	0	0	0	0	4	15	0	0	19	4	0	3	0	7	51
Hourly Total	0	90	21	0	111	0	0	0	0	0	18	87	0	0	105	14	0	14	0	28	244
6:00 PM	0	14	1	0	15	0	0	0	0	0	1	12	0	0	13	2	0	0	0	2	30
6:15 PM	0	15	5	0	20	0	0	0	0	0	1	16	0	0	17	4	0	1	0	5	42
6:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
Grand Total	0	806	92	0	898	0	0	0	0	0	84	738	0	0	822	109	0	85	0	194	1914
Approach %	0.0	89.8	10.2	0.0	-	0.0	0.0	0.0	0.0	-	10.2	89.8	0.0	0.0	-	56.2	0.0	43.8	0.0	-	-
Total %	0.0	42.1	4.8	0.0	46.9	0.0	0.0	0.0	0.0	0.0	4.4	38.6	0.0	0.0	42.9	5.7	0.0	4.4	0.0	10.1	-
Lights	0	792	88	0	880	0	0	0	0	0	82	724	0	0	806	106	0	83	0	189	1875
% Lights	-	98.3	95.7	-	98.0	-	-	-	-	-	97.6	98.1	-	-	98.1	97.2	-	97.6	-	97.4	98.0
Mediums	0	14	4	0	18	0	0	0	0	0	2	14	0	0	16	3	0	2	0	5	39
% Mediums	-	1.7	4.3	-	2.0	-	-	-	-	-	2.4	1.9	-	-	1.9	2.8	-	2.4	-	2.6	2.0

Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
% Articulated Trucks	-	0.0	0.0	-	0.0	-	-	-	-	-	0.0	0.0	-	-	0.0	0.0	-	0.0	-	0.0

GRAM Traffic NTX Inc.
 1120 W. Lovers Lane
 Arlington, Texas, United States 76013
 817.265.8968

Count Name: OAKMONT DR @ ARDGLASS
 TRL
 Site Code:
 Start Date: 04/13/2021
 Page No: 4

Turning Movement Peak Hour Data (7:15 AM)

Start Time	OAKMONT DR Southbound					Eastbound St. Westbound					OAKMONT DR Northbound					ARDGLASS TRL Eastbound					Int. Total
	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	
7:15 AM	0	29	2	0	31	0	0	0	0	0	3	43	0	0	46	9	0	2	0	11	88
7:30 AM	0	81	12	0	93	0	0	0	0	0	1	115	0	0	116	10	0	5	0	15	224
7:45 AM	0	106	2	0	108	0	0	0	0	0	0	69	0	0	69	6	0	8	0	14	191
8:00 AM	0	67	3	0	70	0	0	0	0	0	5	45	0	0	50	6	0	1	0	7	127
Total	0	283	19	0	302	0	0	0	0	0	9	272	0	0	281	31	0	16	0	47	630
Approach %	0.0	93.7	6.3	0.0	-	0.0	0.0	0.0	0.0	-	3.2	96.8	0.0	0.0	-	66.0	0.0	34.0	0.0	-	-
Total %	0.0	44.9	3.0	0.0	47.9	0.0	0.0	0.0	0.0	0.0	1.4	43.2	0.0	0.0	44.6	4.9	0.0	2.5	0.0	7.5	-
PHF	0.000	0.667	0.396	0.000	0.699	0.000	0.000	0.000	0.000	0.000	0.450	0.591	0.000	0.000	0.606	0.775	0.000	0.500	0.000	0.783	0.703
Lights	0	277	18	0	295	0	0	0	0	0	9	267	0	0	276	30	0	16	0	46	617
% Lights	-	97.9	94.7	-	97.7	-	-	-	-	-	100.0	98.2	-	-	98.2	96.8	-	100.0	-	97.9	97.9
Mediums	0	6	1	0	7	0	0	0	0	0	0	5	0	0	5	1	0	0	0	1	13
% Mediums	-	2.1	5.3	-	2.3	-	-	-	-	-	0.0	1.8	-	-	1.8	3.2	-	0.0	-	2.1	2.1
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Articulated Trucks	-	0.0	0.0	-	0.0	-	-	-	-	-	0.0	0.0	-	-	0.0	0.0	-	0.0	-	0.0	0.0

GRAM Traffic NTX Inc.
 1120 W. Lovers Lane
 Arlington, Texas, United States 76013
 817.265.8968

Count Name: OAKMONT DR @ ARDGLASS
 TRL
 Site Code:
 Start Date: 04/13/2021
 Page No: 6

Turning Movement Peak Hour Data (3:00 PM)

Start Time	OAKMONT DR Southbound					Eastbound St. Westbound					OAKMONT DR Northbound					ARDGLASS TRL Eastbound					Int. Total
	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	
3:00 PM	0	82	5	0	87	0	0	0	0	0	4	39	0	0	43	6	0	3	0	9	139
3:15 PM	0	22	0	0	22	0	0	0	0	0	7	35	0	0	42	9	0	5	0	14	78
3:30 PM	0	31	8	0	39	0	0	0	0	0	0	44	0	0	44	7	0	3	0	10	93
3:45 PM	0	81	9	0	90	0	0	0	0	0	8	32	0	0	40	6	0	7	0	13	143
Total	0	216	22	0	238	0	0	0	0	0	19	150	0	0	169	28	0	18	0	46	453
Approach %	0.0	90.8	9.2	0.0	-	0.0	0.0	0.0	0.0	-	11.2	88.8	0.0	0.0	-	60.9	0.0	39.1	0.0	-	-
Total %	0.0	47.7	4.9	0.0	52.5	0.0	0.0	0.0	0.0	0.0	4.2	33.1	0.0	0.0	37.3	6.2	0.0	4.0	0.0	10.2	-
PHF	0.000	0.659	0.611	0.000	0.661	0.000	0.000	0.000	0.000	0.000	0.594	0.852	0.000	0.000	0.960	0.778	0.000	0.643	0.000	0.821	0.792
Lights	0	212	20	0	232	0	0	0	0	0	17	150	0	0	167	27	0	16	0	43	442
% Lights	-	98.1	90.9	-	97.5	-	-	-	-	-	89.5	100.0	-	-	98.8	96.4	-	88.9	-	93.5	97.6
Mediums	0	4	2	0	6	0	0	0	0	0	2	0	0	0	2	1	0	2	0	3	11
% Mediums	-	1.9	9.1	-	2.5	-	-	-	-	-	10.5	0.0	-	-	1.2	3.6	-	11.1	-	6.5	2.4
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Articulated Trucks	-	0.0	0.0	-	0.0	-	-	-	-	-	0.0	0.0	-	-	0.0	0.0	-	0.0	-	0.0	0.0

GRAM Traffic NTX Inc.
1120 W. Lovers Lane
Arlington, Texas, United States 76013
817.265.8968

Count Name: OAKMONT DR @ CREEKSIDE DR
Site Code:
Start Date: 04/13/2021
Page No: 1

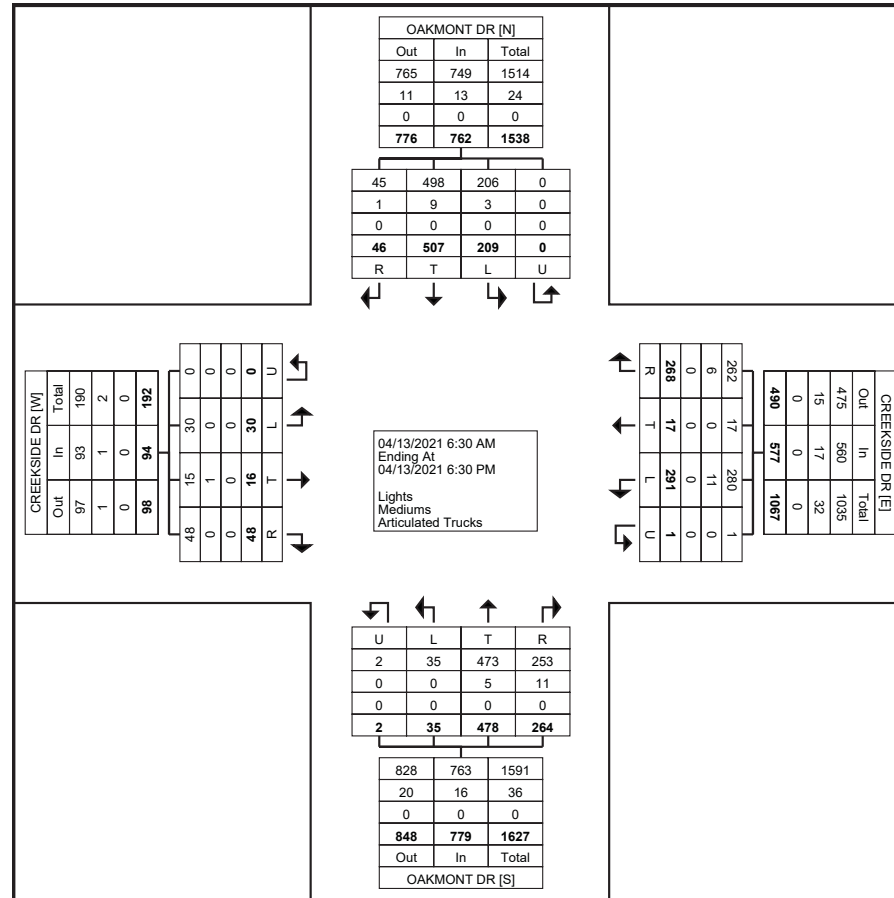
Turning Movement Data

Start Time	OAKMONT DR Southbound				CREEKSIDE DR Westbound				OAKMONT DR Northbound				CREEKSIDE DR Eastbound				Int. Total				
	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	Left		Thru	Right	U-Turn	App. Total
6:30 AM	1	6	0	0	7	1	0	1	0	2	0	8	0	0	8	1	1	0	0	2	19
6:45 AM	3	15	0	0	18	2	0	0	0	2	1	5	3	0	9	2	0	2	0	4	33
Hourly Total	4	21	0	0	25	3	0	1	0	4	1	13	3	0	17	3	1	2	0	6	52
7:00 AM	2	20	0	0	22	4	1	2	0	7	1	12	6	0	19	3	0	1	0	4	52
7:15 AM	6	42	0	0	48	7	1	5	0	13	0	17	11	0	28	1	0	1	0	2	91
7:30 AM	24	77	6	0	107	31	1	11	0	43	3	52	47	0	102	3	1	4	0	8	260
7:45 AM	29	26	2	0	57	56	2	50	0	108	2	32	44	0	78	2	0	6	0	8	251
Hourly Total	61	165	8	0	234	98	5	68	0	171	6	113	108	0	227	9	1	12	0	22	654
8:00 AM	45	19	1	0	65	52	5	58	1	116	1	12	35	1	49	0	5	2	0	7	237
8:15 AM	4	17	3	0	24	9	1	19	0	29	0	13	1	0	14	0	0	3	0	3	70
8:30 AM	0	17	1	0	18	4	0	2	0	6	0	11	3	0	14	1	1	3	0	5	43
8:45 AM	2	8	0	0	10	4	1	0	0	5	0	15	2	0	17	1	0	1	0	2	34
Hourly Total	51	61	5	0	117	69	7	79	1	156	1	51	41	1	94	2	6	9	0	17	384
*** BREAK ***	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
3:00 PM	3	25	3	0	31	9	0	5	0	14	0	35	16	0	51	0	1	2	0	3	99
3:15 PM	23	16	3	0	42	2	0	4	0	6	0	24	11	0	35	2	1	1	0	4	87
3:30 PM	28	21	4	0	53	14	0	15	0	29	1	19	10	0	30	0	1	0	0	1	113
3:45 PM	6	20	1	0	27	35	3	49	0	87	6	31	12	0	49	3	1	1	0	5	168
Hourly Total	60	82	11	0	153	60	3	73	0	136	7	109	49	0	165	5	4	4	0	13	467
4:00 PM	3	20	3	0	26	17	0	10	0	27	3	22	2	0	27	1	0	2	0	3	83
4:15 PM	4	20	1	0	25	7	0	7	0	14	3	25	9	0	37	3	0	4	0	7	83
4:30 PM	5	18	2	0	25	7	0	8	0	15	5	24	8	0	37	2	0	3	0	5	82
4:45 PM	4	14	1	0	19	4	0	6	0	10	1	28	5	0	34	1	1	2	0	4	67
Hourly Total	16	72	7	0	95	35	0	31	0	66	12	99	24	0	135	7	1	11	0	19	315
5:00 PM	4	20	3	0	27	9	0	5	0	14	2	20	11	1	34	0	0	3	0	3	78
5:15 PM	3	19	2	0	24	6	0	5	0	11	1	18	9	0	28	0	0	1	0	1	64
5:30 PM	1	20	3	0	24	4	0	0	0	4	4	9	4	0	17	3	1	2	0	6	51
5:45 PM	2	19	1	0	22	3	1	0	0	4	1	16	4	0	21	0	0	2	0	2	49
Hourly Total	10	78	9	0	97	22	1	10	0	33	8	63	28	1	100	3	1	8	0	12	242
6:00 PM	2	9	5	0	16	4	0	3	0	7	0	13	8	0	21	0	0	2	0	2	46
6:15 PM	5	19	1	0	25	0	1	3	0	4	0	17	3	0	20	1	2	0	0	3	52
Grand Total	209	507	46	0	762	291	17	268	1	577	35	478	264	2	779	30	16	48	0	94	2212
Approach %	27.4	66.5	6.0	0.0	-	50.4	2.9	46.4	0.2	-	4.5	61.4	33.9	0.3	-	31.9	17.0	51.1	0.0	-	-
Total %	9.4	22.9	2.1	0.0	34.4	13.2	0.8	12.1	0.0	26.1	1.6	21.6	11.9	0.1	35.2	1.4	0.7	2.2	0.0	4.2	-
Lights	206	498	45	0	749	280	17	262	1	560	35	473	253	2	763	30	15	48	0	93	2165
% Lights	98.6	98.2	97.8	-	98.3	96.2	100.0	97.8	100.0	97.1	100.0	99.0	95.8	100.0	97.9	100.0	93.8	100.0	-	98.9	97.9
Mediums	3	9	1	0	13	11	0	6	0	17	0	5	11	0	16	0	1	0	0	1	47
% Mediums	1.4	1.8	2.2	-	1.7	3.8	0.0	2.2	0.0	2.9	0.0	1.0	4.2	0.0	2.1	0.0	6.3	0.0	-	1.1	2.1
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0

% Articulated Trucks	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0
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GRAM Traffic NTX Inc.
 1120 W. Lovers Lane
 Arlington, Texas, United States 76013
 817.265.8968

Count Name: OAKMONT DR @ CREEKSIDE
 DR
 Site Code:
 Start Date: 04/13/2021
 Page No: 3



Turning Movement Data Plot

GRAM Traffic NTX Inc.
 1120 W. Lovers Lane
 Arlington, Texas, United States 76013
 817.265.8968

Count Name: OAKMONT DR @ CREEKSIDE
 DR
 Site Code:
 Start Date: 04/13/2021
 Page No: 4

Turning Movement Peak Hour Data (7:15 AM)

Start Time	OAKMONT DR Southbound					CREEKSIDE DR Westbound					OAKMONT DR Northbound					CREEKSIDE DR Eastbound					Int. Total
	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	
7:15 AM	6	42	0	0	48	7	1	5	0	13	0	17	11	0	28	1	0	1	0	2	91
7:30 AM	24	77	6	0	107	31	1	11	0	43	3	52	47	0	102	3	1	4	0	8	260
7:45 AM	29	26	2	0	57	56	2	50	0	108	2	32	44	0	78	2	0	6	0	8	251
8:00 AM	45	19	1	0	65	52	5	58	1	116	1	12	35	1	49	0	5	2	0	7	237
Total	104	164	9	0	277	146	9	124	1	280	6	113	137	1	257	6	6	13	0	25	839
Approach %	37.5	59.2	3.2	0.0	-	52.1	3.2	44.3	0.4	-	2.3	44.0	53.3	0.4	-	24.0	24.0	52.0	0.0	-	-
Total %	12.4	19.5	1.1	0.0	33.0	17.4	1.1	14.8	0.1	33.4	0.7	13.5	16.3	0.1	30.6	0.7	0.7	1.5	0.0	3.0	-
PHF	0.578	0.532	0.375	0.000	0.647	0.652	0.450	0.534	0.250	0.603	0.500	0.543	0.729	0.250	0.630	0.500	0.300	0.542	0.000	0.781	0.807
Lights	103	161	8	0	272	141	9	122	1	273	6	113	130	1	250	6	6	13	0	25	820
% Lights	99.0	98.2	88.9	-	98.2	96.6	100.0	98.4	100.0	97.5	100.0	100.0	94.9	100.0	97.3	100.0	100.0	100.0	-	100.0	97.7
Mediums	1	3	1	0	5	5	0	2	0	7	0	0	7	0	7	0	0	0	0	0	19
% Mediums	1.0	1.8	11.1	-	1.8	3.4	0.0	1.6	0.0	2.5	0.0	0.0	5.1	0.0	2.7	0.0	0.0	0.0	-	0.0	2.3
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Articulated Trucks	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	-	0.0	0.0

GRAM Traffic NTX Inc.
 1120 W. Lovers Lane
 Arlington, Texas, United States 76013
 817.265.8968

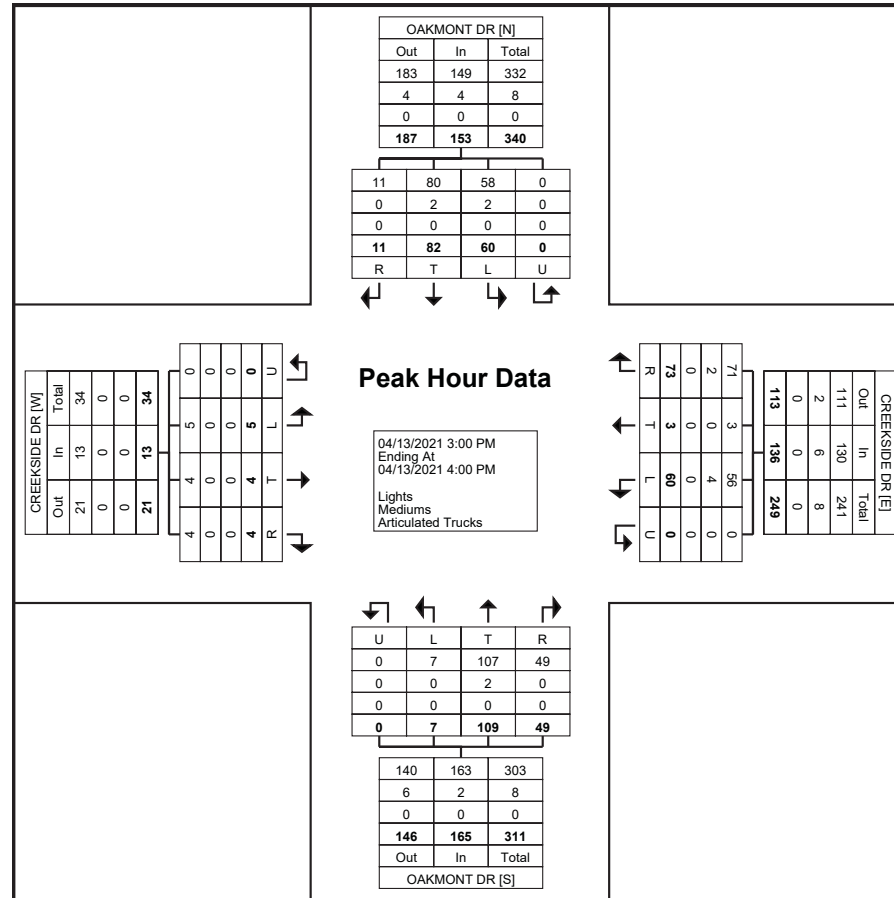
Count Name: OAKMONT DR @ CREEKSIDE
 DR
 Site Code:
 Start Date: 04/13/2021
 Page No: 6

Turning Movement Peak Hour Data (3:00 PM)

Start Time	OAKMONT DR Southbound					CREEKSIDE DR Westbound					OAKMONT DR Northbound					CREEKSIDE DR Eastbound					Int. Total
	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	Left	Thru	Right	U-Turn	App. Total	
3:00 PM	3	25	3	0	31	9	0	5	0	14	0	35	16	0	51	0	1	2	0	3	99
3:15 PM	23	16	3	0	42	2	0	4	0	6	0	24	11	0	35	2	1	1	0	4	87
3:30 PM	28	21	4	0	53	14	0	15	0	29	1	19	10	0	30	0	1	0	0	1	113
3:45 PM	6	20	1	0	27	35	3	49	0	87	6	31	12	0	49	3	1	1	0	5	168
Total	60	82	11	0	153	60	3	73	0	136	7	109	49	0	165	5	4	4	0	13	467
Approach %	39.2	53.6	7.2	0.0	-	44.1	2.2	53.7	0.0	-	4.2	66.1	29.7	0.0	-	38.5	30.8	30.8	0.0	-	-
Total %	12.8	17.6	2.4	0.0	32.8	12.8	0.6	15.6	0.0	29.1	1.5	23.3	10.5	0.0	35.3	1.1	0.9	0.9	0.0	2.8	-
PHF	0.536	0.820	0.688	0.000	0.722	0.429	0.250	0.372	0.000	0.391	0.292	0.779	0.766	0.000	0.809	0.417	1.000	0.500	0.000	0.650	0.695
Lights	58	80	11	0	149	56	3	71	0	130	7	107	49	0	163	5	4	4	0	13	455
% Lights	96.7	97.6	100.0	-	97.4	93.3	100.0	97.3	-	95.6	100.0	98.2	100.0	-	98.8	100.0	100.0	100.0	-	100.0	97.4
Mediums	2	2	0	0	4	4	0	2	0	6	0	2	0	0	2	0	0	0	0	0	12
% Mediums	3.3	2.4	0.0	-	2.6	6.7	0.0	2.7	-	4.4	0.0	1.8	0.0	-	1.2	0.0	0.0	0.0	-	0.0	2.6
Articulated Trucks	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
% Articulated Trucks	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	-	0.0	0.0	0.0	0.0	-	0.0	0.0

GRAM Traffic NTX Inc.
 1120 W. Lovers Lane
 Arlington, Texas, United States 76013
 817.265.8968

Count Name: OAKMONT DR @ CREEKSIDE
 DR
 Site Code:
 Start Date: 04/13/2021
 Page No: 7



Turning Movement Peak Hour Data Plot (3:00 PM)

**Appendix C:
Trip Generation Calculations for Existing Zoning**

Alternate Trip Generation Calculations

The number of trips generated by the development is a function of the type and quantity of land use. The number of vehicle trips generated by the development was estimated based on the trip generation rates and equations provided in the publication entitled *Trip Generation Manual, Tenth Edition*, by the Institute of Transportation Engineers (ITE). Estimates of the number of trips generated by the site were made for the weekday AM and PM peak hours, as well as on a daily basis, for the assumed land uses possible under existing zoning, as previously identified in Table 4 of the report. The trip generation rates/equations utilized are provided in **Table 1A**. The directional splits are shown in **Table 2A**. The rates and splits for a general urban/suburban area were utilized.

Table 1A. ITE Trip Generation Rates/Equations

Land Use	ITE Code	Average Weekday	AM Peak Hour	PM Peak Hour
Multifamily Housing (Low-Rise)	220	$T = 7.56X - 40.86^1$	$\ln(T) = 0.95\ln(X) - 0.51$	$\ln(T) = 0.89\ln(X) - 0.02$
Charter Elementary School (537)	537	$T = 1.85Y$	$T = 1.17Y - 34.68$	$T = 0.14Y$
General Office Building	710	$\ln(T) = 0.97\ln(Z) + 2.50$	$T = 0.94Z + 26.49$	$\ln(T) = 0.95\ln(Z) + 0.36$
Medical-Dental Office	720	$T = 38.42Z - 87.62$	$\ln(T) = 0.89\ln(Z) + 1.31$	$T = 3.39Z + 2.02$
Pharmacy/Drugstore with Drive-Through Window	881	$T = 109.16Z$	$T = 3.84Z$	$T = 10.29Z$
Fast-Food Restaurant without Drive-Through Window	933	$T = 346.23Z$	$T = 25.10Z$	$T = 28.34Z$
Gasoline/Service Station with Convenience Market	945	$T = 205.36V$	$T = 19.00V - 96.53$	$T = 13.99V$

¹T = Trips Ends; X = Dwelling Units; Y = Students; Z = 1,000 Square Feet; V = Vehicle Fueling Positions

Table 2A. ITE Directional Splits

Land Use	ITE Code	Average Weekday	AM Peak Hour	PM Peak Hour
Multifamily Housing (Low-Rise)	220	50 / 50 ¹	23 / 77	63 / 37
Charter Elementary School (537)	537	50 / 50	53 / 47	35 / 65
General Office Building	710	50 / 50	86 / 14	16 / 84
Medical-Dental Office	720	50 / 50	78 / 22	28 / 72
Pharmacy/Drugstore with Drive-Through Window	881	50 / 50	53 / 47	50 / 50
Fast-Food Restaurant without Drive-Through Window	933	50 / 50	60 / 40	50 / 50
Gasoline/Service Station with Convenience Market	945	50 / 50	51 / 49	51 / 49

¹XX / YY = % entering vehicles / % exiting vehicles for General Urban/Suburban Area

Internal Capture

In a mixed-use development, land uses tend to interact and thus attract a portion of each other's trip generation. This phenomenon is known as "internal capture" and results in a lesser percentage of trips assumed to use the external roadway system. Internal capture adjustments were applied, where applicable, to the trip generation estimates.

Criteria set forth in the ITE's *Trip Generation Handbook, Third Edition* were used to estimate the appropriate internal capture adjustment. A spreadsheet tool was developed to calculate internal capture as part of the National Cooperative Highway Research Program (NCHRP) project 8-51. Spreadsheets calculating internal capture for the proposed development are included at the end of this Appendix section.

Inputs to the internal capture method include the base trip generation, assumed mode split for external trips, vehicle occupancy estimates, and average land use proximity. The base trip generation was developed from the information shown in Tables 1A and 2A above. Transit mode split was based on the prospect of future transit service in the vicinity, assumed to be 0% for each type of land use on the site.

Vehicle occupancy (i.e. the number of passengers per vehicle) was assumed based on queries to a local Dallas-Forth Worth area subset of the Federal Highway Administration's 2009 National Household Travel Survey (NHTS) database at <http://nhts.ornl.gov>. The database was queried separately for each land use on the site. For example, DFW travelers reported an average rate of 1.11 persons per vehicle for going to work but 2.32 persons per vehicle for going out to eat.

The internal capture effect of the site was estimated by measuring the average land use proximity (i.e. walking distances) between the estimated centroids of each land use type on the site. For the purposes of this study, a general walking distance of 1,000 feet was estimated based on the size of the site.

Site Generated Traffic Volumes

The trip generation calculations for the four existing zoning scenarios are shown in **Table 3A**. The table includes overall trip generation, internal capture trips, external trips. As previously stated, the four scenarios represent possible development land uses and intensities allowed under existing zoning. For comparison purposes, the anticipated trip generation for the proposed Avilla Fairways development is also shown.

Table 3A. Trip Generation Calculations for Proposed Site and for Site with Existing Zoning

Amount	Units	ITE Land Use (ITE Code)	Daily Trips	AM Peak Hour			PM Peak Hour		
				In	Out	Total	In	Out	Total
PROPOSED SITE - 215 Dwelling Units									
215	dwellings	Multifamily Housing (Low-Rise) (220)	1,586	23	76	99	74	43	117
ALTERNATE SCENARIO 1 - Existing Zoning (274 Total Duplex, Townhome, and MF-24 Residential)									
97	dwellings	Multifamily Housing (Low-Rise) (220) ¹	694	11	35	46	36	21	57
70	dwellings	Multifamily Housing (Low-Rise) (220) ¹	490	8	26	34	27	16	43
107	dwellings	Multifamily Housing (Low-Rise) (220)	770	12	39	51	40	23	63
TOTAL			1,954	31	100	131	103	60	163
ALTERNATE SCENARIO 2 - Existing Zoning (167 Duplex/Townhome Dwelling Units, Restaurant, Retail/Pharmacy, and Fuel/Convenience)									
97	dwellings	Multifamily Housing (Low-Rise) (220) ¹	694	11	35	46	36	21	57
70	dwellings	Multifamily Housing (Low-Rise) (220) ¹	490	8	26	34	27	16	43
13,000	ft ²	Pharmacy/Drugstore with Drive-Through Window (881)	1,420	27	23	50	67	67	134
5,000	ft ²	Fast-Food Restaurant without Drive-Through Window (933)	1,732	76	50	126	71	71	142
20	vfp	Gasoline/Service Station with Convenience Market (945)	4,108	144	139	283	143	137	280
SUBTOTAL			8,444	266	273	539	344	312	656
Internal Capture Trips			292	34	41	75	112	105	217
TOTAL NET EXTERNAL TRIPS			8,152	232	232	464	232	207	439
ALTERNATE SCENARIO 3 - Existing Zoning (167 Duplex/Townhome Dwelling Units + Office)									
97	dwellings	Multifamily Housing (Low-Rise) (220) ¹	694	11	35	46	36	21	57
70	dwellings	Multifamily Housing (Low-Rise) (220) ¹	490	8	26	34	27	16	43
95,564	ft ²	General Office Building (710)	1,016	100	16	116	17	92	109
95,564	ft ²	Medical-Dental Office (720)	3,584	167	47	214	91	235	326
SUBTOTAL			5,784	286	124	410	171	364	535
Internal Capture Trips			13	2	1	3	5	5	10
TOTAL NET EXTERNAL TRIPS			5,771	284	123	407	166	359	525
ALTERNATE SCENARIO 4 - Existing Zoning (167 Duplex/Townhome Dwelling Units + 800 Student Charter School)									
97	dwellings	Multifamily Housing (Low-Rise) (220) ¹	694	11	35	46	36	21	57
70	dwellings	Multifamily Housing (Low-Rise) (220) ¹	490	8	26	34	27	16	43
800	students	Charter Elementary School (537)	1,480	478	423	901	39	73	112
TOTAL			2,664	497	484	981	102	110	212

¹ Both duplexes and townhomes are considered part of the 'Multifamily Housing (Low-Rise) land use in the 10th edition of the ITE Trip Generation Manual

NCHRP 8-51 Internal Trip Capture Estimation Tool			
Project Name:	Lake Sharon at Oakmont	Organization:	Lee Engineering
Project Location:	Corinth, TX	Performed By:	KWN
Scenario Description:	Zoning with Retail	Date:	4/16/2021
Analysis Year:		Checked By:	
Analysis Period:	AM Street Peak Hour	Date:	

Table 1-A: Base Vehicle-Trip Generation Estimates (Single-Use Site Estimate)						
Land Use	Development Data (For Information Only)			Estimated Vehicle-Trips		
	ITE LUCs ¹	Quantity	Units	Total	Entering	Exiting
Office				0		
Retail	881,945			333	171	162
Restaurant	933			126	76	50
Cinema/Entertainment				0		
Residential	220			80	19	61
Hotel				0		
All Other Land Uses ²				0		
Total				539	266	273

Table 2-A: Mode Split and Vehicle Occupancy Estimates						
Land Use	Entering Trips			Exiting Trips		
	Veh. Occ.	% Transit	% Non-Motorized	Veh. Occ.	% Transit	% Non-Motorized
Office	1.11			1.11		
Retail	1.77			1.77		
Restaurant	2.32			2.32		
Cinema/Entertainment	2.18			2.18		
Residential	1.54			1.54		
Hotel	1.93			1.93		
All Other Land Uses ²	1.71			1.71		

Table 3-A: Average Land Use Interchange Distances (Feet Walking Distance)						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office						
Retail						
Restaurant						
Cinema/Entertainment						
Residential						
Hotel						

Table 4-A: Internal Person-Trip Origin-Destination Matrix*						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office		0	0	0	0	0
Retail	0		37	0	1	0
Restaurant	0	16		0	1	0
Cinema/Entertainment	0	0	0		0	0
Residential	0	1	19	0		0
Hotel	0	0	0	0	0	

Table 5-A: Computations Summary			
	Total	Entering	Exiting
All Person-Trips	1,005	508	497
Internal Capture Percentage	15%	15%	15%
External Vehicle-Trips ³	464	232	232
External Transit-Trips ⁴	0	0	0
External Non-Motorized Trips ⁴	0	0	0

Table 6-A: Internal Trip Capture Percentages by Land Use		
Land Use	Entering Trips	Exiting Trips
Office	N/A	N/A
Retail	6%	13%
Restaurant	32%	15%
Cinema/Entertainment	N/A	N/A
Residential	7%	21%
Hotel	N/A	N/A

¹Land Use Codes (LUCs) from *Trip Generation Informational Report*, published by the Institute of Transportation Engineers.

²Total estimate for all other land uses at mixed-use development site-not subject to internal trip capture computations in this estimator

³Vehicle-trips computed using the mode split and vehicle occupancy values provided in Table 2-A

⁴Person-Trips

*Indicates computation that has been rounded to the nearest whole number.

Estimation Tool Developed by the Texas Transportation Institute

Project Name:	Lake Sharon at Oakmont
Analysis Period:	AM Street Peak Hour

Land Use	Table 7-A (D): Entering Trips			Table 7-A (O): Exiting Trips		
	Veh. Occ.	Vehicle-Trips	Person-Trips*	Veh. Occ.	Vehicle-Trips	Person-Trips*
Office	1.11	0	0	1.11	0	0
Retail	1.77	171	303	1.77	162	287
Restaurant	2.32	76	176	2.32	50	116
Cinema/Entertainment	2.18	0	0	2.18	0	0
Residential	1.54	19	29	1.54	61	94
Hotel	1.93	0	0	1.93	0	0

Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office		0	0	0	0	0
Retail	83		37	0	40	0
Restaurant	36	16		0	5	3
Cinema/Entertainment	0	0	0		0	0
Residential	2	1	19	0		0
Hotel	0	0	0	0	0	

Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office		97	40	0	0	0
Retail	0		88	0	1	0
Restaurant	0	24		0	1	0
Cinema/Entertainment	0	0	0		0	0
Residential	0	52	35	0		0
Hotel	0	12	11	0	0	

Destination Land Use	Person-Trip Estimates			External Trips by Mode*		
	Internal	External	Total	Vehicles ¹	Transit ²	Non-Motorized ²
Office	0	0	0	0	0	0
Retail	17	286	303	162	0	0
Restaurant	56	120	176	52	0	0
Cinema/Entertainment	0	0	0	0	0	0
Residential	2	27	29	18	0	0
Hotel	0	0	0	0	0	0
All Other Land Uses ³	0	0	0	0	0	0

Origin Land Use	Person-Trip Estimates			External Trips by Mode*		
	Internal	External	Total	Vehicles ¹	Transit ²	Non-Motorized ²
Office	0	0	0	0	0	0
Retail	38	249	287	141	0	0
Restaurant	17	99	116	43	0	0
Cinema/Entertainment	0	0	0	0	0	0
Residential	20	74	94	48	0	0
Hotel	0	0	0	0	0	0
All Other Land Uses ³	0	0	0	0	0	0

¹Vehicle-trips computed using the mode split and vehicle occupancy values provided in Table 2-A
²Person-Trips
³Total estimate for all other land uses at mixed-use development site-not subject to internal trip capture computations in this estimator
*Indicates computation that has been rounded to the nearest whole number.

NCHRP 8-51 Internal Trip Capture Estimation Tool					
Project Name:	Lake Sharon at Oakmont	Organization:	Lee Engineering		
Project Location:	Corinth, TX	Performed By:	KWN		
Scenario Description:	Zoning with Retail	Date:	4/16/2021		
Analysis Year:		Checked By:			
Analysis Period:	PM Street Peak Hour	Date:			

Table 1-P: Base Vehicle-Trip Generation Estimates (Single-Use Site Estimate)						
Land Use	Development Data (For Information Only)			Estimated Vehicle-Trips		
	ITE LUCs ¹	Quantity	Units	Total	Entering	Exiting
Office				0		
Retail				414	210	204
Restaurant				142	71	71
Cinema/Entertainment				0		
Residential				100	63	37
Hotel				0		
All Other Land Uses ²				0		
Total				656	344	312

Table 2-P: Mode Split and Vehicle Occupancy Estimates						
Land Use	Entering Trips			Exiting Trips		
	Veh. Occ.	% Transit	% Non-Motorized	Veh. Occ.	% Transit	% Non-Motorized
Office	1.11			1.11		
Retail	1.77			1.77		
Restaurant	2.32			2.32		
Cinema/Entertainment	2.18			2.18		
Residential	1.54			1.54		
Hotel	1.93			1.93		
All Other Land Uses ²	1.71			1.71		

Table 3-P: Average Land Use Interchange Distances (Feet Walking Distance)						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office						
Retail					1000	
Restaurant					1000	
Cinema/Entertainment						
Residential		1000	1000			
Hotel						

Table 4-P: Internal Person-Trip Origin-Destination Matrix*						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office		0	0	0	0	0
Retail	0		48	0	45	0
Restaurant	0	68		0	16	0
Cinema/Entertainment	0	0	0		0	0
Residential	0	18	9	0		0
Hotel	0	0	0	0	0	

Table 5-P: Computations Summary			
	Total	Entering	Exiting
All Person-Trips	1,217	634	583
Internal Capture Percentage	34%	32%	35%
External Vehicle-Trips ³	437	232	205
External Transit-Trips ⁴	0	0	0
External Non-Motorized Trips ⁴	0	0	0

Table 6-P: Internal Trip Capture Percentages by Land Use		
Land Use	Entering Trips	Exiting Trips
Office	N/A	N/A
Retail	23%	26%
Restaurant	35%	51%
Cinema/Entertainment	N/A	N/A
Residential	63%	47%
Hotel	N/A	N/A

¹Land Use Codes (LUCs) from *Trip Generation Informational Report*, published by the Institute of Transportation Engineers.

²Total estimate for all other land uses at mixed-use development site-not subject to internal trip capture computations in this estimator

³Vehicle-trips computed using the mode split and vehicle occupancy values provided in Table 2-P

⁴Person-Trips

*Indicates computation that has been rounded to the nearest whole number.

Estimation Tool Developed by the Texas Transportation Institute

Project Name:	Lake Sharon at Oakmont
Analysis Period:	PM Street Peak Hour

Table 7-P: Conversion of Vehicle-Trip Ends to Person-Trip Ends						
Land Use	Table 7-P (D): Entering Trips			Table 7-P (O): Exiting Trips		
	Veh. Occ.	Vehicle-Trips	Person-Trips*	Veh. Occ.	Vehicle-Trips	Person-Trips*
Office	1.11	0	0	1.11	0	0
Retail	1.77	210	372	1.77	204	361
Restaurant	2.32	71	165	2.32	71	165
Cinema/Entertainment	2.18	0	0	2.18	0	0
Residential	1.54	63	97	1.54	37	57
Hotel	1.93	0	0	1.93	0	0

Table 8-P (O): Internal Person-Trip Origin-Destination Matrix (Computed at Origin)						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office		0	0	0	0	0
Retail	7		105	14	87	18
Restaurant	5	68		13	28	12
Cinema/Entertainment	0	0	0		0	0
Residential	2	18	9	0		2
Hotel	0	0	0	0	0	

Table 8-P (D): Internal Person-Trip Origin-Destination Matrix (Computed at Destination)						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office		30	3	0	4	0
Retail	0		48	0	45	0
Restaurant	0	186		0	16	0
Cinema/Entertainment	0	15	5		4	0
Residential	0	28	18	0		0
Hotel	0	7	8	0	0	

Table 9-P (D): Internal and External Trips Summary (Entering Trips)						
Destination Land Use	Person-Trip Estimates			External Trips by Mode*		
	Internal	External	Total	Vehicles ¹	Transit ²	Non-Motorized ²
Office	0	0	0	0	0	0
Retail	86	286	372	162	0	0
Restaurant	57	108	165	47	0	0
Cinema/Entertainment	0	0	0	0	0	0
Residential	61	36	97	23	0	0
Hotel	0	0	0	0	0	0
All Other Land Uses ³	0	0	0	0	0	0

Table 9-P (O): Internal and External Trips Summary (Exiting Trips)						
Origin Land Use	Person-Trip Estimates			External Trips by Mode*		
	Internal	External	Total	Vehicles ¹	Transit ²	Non-Motorized ²
Office	0	0	0	0	0	0
Retail	93	268	361	151	0	0
Restaurant	84	81	165	35	0	0
Cinema/Entertainment	0	0	0	0	0	0
Residential	27	30	57	19	0	0
Hotel	0	0	0	0	0	0
All Other Land Uses ³	0	0	0	0	0	0

¹Vehicle-trips computed using the mode split and vehicle occupancy values provided in Table 2-P
²Person-Trips
³Total estimate for all other land uses at mixed-use development site-not subject to internal trip capture computations in this estimator
*Indicates computation that has been rounded to the nearest whole number.

NCHRP 8-51 Internal Trip Capture Estimation Tool			
Project Name:	Lake Sharon at Oakmont	Organization:	Lee Engineering
Project Location:	Corinth, TX	Performed By:	KWN
Scenario Description:	Zoning with Office	Date:	4/16/2021
Analysis Year:		Checked By:	
Analysis Period:	AM Street Peak Hour	Date:	

Land Use	Development Data (For Information Only)			Estimated Vehicle-Trips		
	ITE LUCs ¹	Quantity	Units	Total	Entering	Exiting
Office	710,720			330	267	63
Retail				0		
Restaurant				0		
Cinema/Entertainment				0		
Residential	220			80	19	61
Hotel				0		
All Other Land Uses ²				0		
Total				410	286	124

Land Use	Entering Trips			Exiting Trips		
	Veh. Occ.	% Transit	% Non-Motorized	Veh. Occ.	% Transit	% Non-Motorized
Office	1.11			1.11		
Retail	1.77			1.77		
Restaurant	2.32			2.32		
Cinema/Entertainment	2.18			2.18		
Residential	1.54			1.54		
Hotel	1.93			1.93		
All Other Land Uses ²	1.71			1.71		

Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office						
Retail						
Restaurant						
Cinema/Entertainment						
Residential						
Hotel						

Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office		0	0	0	0	0
Retail	0		0	0	0	0
Restaurant	0	0		0	0	0
Cinema/Entertainment	0	0	0		0	0
Residential	2	0	0	0		0
Hotel	0	0	0	0	0	

	Total	Entering	Exiting
All Person-Trips	489	325	164
Internal Capture Percentage	1%	1%	1%
External Vehicle-Trips ³	407	284	123
External Transit-Trips ⁴	0	0	0
External Non-Motorized Trips ⁴	0	0	0

Land Use	Entering Trips	Exiting Trips
Office	1%	0%
Retail	N/A	N/A
Restaurant	N/A	N/A
Cinema/Entertainment	N/A	N/A
Residential	0%	2%
Hotel	N/A	N/A

¹Land Use Codes (LUCs) from *Trip Generation Informational Report*, published by the Institute of Transportation Engineers.

²Total estimate for all other land uses at mixed-use development site-not subject to internal trip capture computations in this estimator

³Vehicle-trips computed using the mode split and vehicle occupancy values provided in Table 2-A

⁴Person-Trips

*Indicates computation that has been rounded to the nearest whole number.

Estimation Tool Developed by the Texas Transportation Institute

Project Name:	Lake Sharon at Oakmont
Analysis Period:	AM Street Peak Hour

Land Use	Table 7-A (D): Entering Trips			Table 7-A (O): Exiting Trips		
	Veh. Occ.	Vehicle-Trips	Person-Trips*	Veh. Occ.	Vehicle-Trips	Person-Trips*
Office	1.11	267	296	1.11	63	70
Retail	1.77	0	0	1.77	0	0
Restaurant	2.32	0	0	2.32	0	0
Cinema/Entertainment	2.18	0	0	2.18	0	0
Residential	1.54	19	29	1.54	61	94
Hotel	1.93	0	0	1.93	0	0

Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office		20	44	0	1	0
Retail	0		0	0	0	0
Restaurant	0	0		0	0	0
Cinema/Entertainment	0	0	0		0	0
Residential	2	1	19	0		0
Hotel	0	0	0	0	0	

Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office		0	0	0	0	0
Retail	12		0	0	1	0
Restaurant	41	0		0	1	0
Cinema/Entertainment	0	0	0		0	0
Residential	9	0	0	0		0
Hotel	9	0	0	0	0	

Destination Land Use	Person-Trip Estimates			External Trips by Mode*		
	Internal	External	Total	Vehicles ¹	Transit ²	Non-Motorized ²
Office	2	294	296	265	0	0
Retail	0	0	0	0	0	0
Restaurant	0	0	0	0	0	0
Cinema/Entertainment	0	0	0	0	0	0
Residential	0	29	29	19	0	0
Hotel	0	0	0	0	0	0
All Other Land Uses ³	0	0	0	0	0	0

Origin Land Use	Person-Trip Estimates			External Trips by Mode*		
	Internal	External	Total	Vehicles ¹	Transit ²	Non-Motorized ²
Office	0	70	70	63	0	0
Retail	0	0	0	0	0	0
Restaurant	0	0	0	0	0	0
Cinema/Entertainment	0	0	0	0	0	0
Residential	2	92	94	60	0	0
Hotel	0	0	0	0	0	0
All Other Land Uses ³	0	0	0	0	0	0

¹Vehicle-trips computed using the mode split and vehicle occupancy values provided in Table 2-A
²Person-Trips
³Total estimate for all other land uses at mixed-use development site-not subject to internal trip capture computations in this estimator
*Indicates computation that has been rounded to the nearest whole number.

NCHRP 8-51 Internal Trip Capture Estimation Tool			
Project Name:	Lake Sharon at Oakmont	Organization:	Lee Engineering
Project Location:	Corinth, TX	Performed By:	KWN
Scenario Description:	Zoning with Office	Date:	4/16/2021
Analysis Year:		Checked By:	
Analysis Period:	PM Street Peak Hour	Date:	

Table 1-P: Base Vehicle-Trip Generation Estimates (Single-Use Site Estimate)						
Land Use	Development Data (For Information Only)			Estimated Vehicle-Trips		
	ITE LUCs ¹	Quantity	Units	Total	Entering	Exiting
Office				435	108	327
Retail				0		
Restaurant				0		
Cinema/Entertainment				0		
Residential				100	63	37
Hotel				0		
All Other Land Uses ²				0		
Total				535	171	364

Table 2-P: Mode Split and Vehicle Occupancy Estimates						
Land Use	Entering Trips			Exiting Trips		
	Veh. Occ.	% Transit	% Non-Motorized	Veh. Occ.	% Transit	% Non-Motorized
Office	1.11			1.11		
Retail	1.77			1.77		
Restaurant	2.32			2.32		
Cinema/Entertainment	2.18			2.18		
Residential	1.54			1.54		
Hotel	1.93			1.93		
All Other Land Uses ²	1.71			1.71		

Table 3-P: Average Land Use Interchange Distances (Feet Walking Distance)						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office					1000	
Retail						
Restaurant						
Cinema/Entertainment						
Residential						
Hotel						

Table 4-P: Internal Person-Trip Origin-Destination Matrix*						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office		0	0	0	4	0
Retail	0		0	0	0	0
Restaurant	0	0		0	0	0
Cinema/Entertainment	0	0	0		0	0
Residential	2	0	0	0		0
Hotel	0	0	0	0	0	

Table 5-P: Computations Summary			
	Total	Entering	Exiting
All Person-Trips	637	217	420
Internal Capture Percentage	2%	3%	1%
External Vehicle-Trips ³	525	166	359
External Transit-Trips ⁴	0	0	0
External Non-Motorized Trips ⁴	0	0	0

Table 6-P: Internal Trip Capture Percentages by Land Use		
Land Use	Entering Trips	Exiting Trips
Office	2%	1%
Retail	N/A	N/A
Restaurant	N/A	N/A
Cinema/Entertainment	N/A	N/A
Residential	4%	4%
Hotel	N/A	N/A

¹Land Use Codes (LUCs) from *Trip Generation Informational Report*, published by the Institute of Transportation Engineers.

²Total estimate for all other land uses at mixed-use development site-not subject to internal trip capture computations in this estimator

³Vehicle-trips computed using the mode split and vehicle occupancy values provided in Table 2-P

⁴Person-Trips

*Indicates computation that has been rounded to the nearest whole number.

Estimation Tool Developed by the Texas Transportation Institute

Project Name:	Lake Sharon at Oakmont
Analysis Period:	PM Street Peak Hour

Table 7-P: Conversion of Vehicle-Trip Ends to Person-Trip Ends						
Land Use	Table 7-P (D): Entering Trips			Table 7-P (O): Exiting Trips		
	Veh. Occ.	Vehicle-Trips	Person-Trips*	Veh. Occ.	Vehicle-Trips	Person-Trips*
Office	1.11	108	120	1.11	327	363
Retail	1.77	0	0	1.77	0	0
Restaurant	2.32	0	0	2.32	0	0
Cinema/Entertainment	2.18	0	0	2.18	0	0
Residential	1.54	63	97	1.54	37	57
Hotel	1.93	0	0	1.93	0	0

Table 8-P (O): Internal Person-Trip Origin-Destination Matrix (Computed at Origin)						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office		73	15	0	7	0
Retail	0		0	0	0	0
Restaurant	0	0		0	0	0
Cinema/Entertainment	0	0	0		0	0
Residential	2	24	12	0		2
Hotel	0	0	0	0	0	

Table 8-P (D): Internal Person-Trip Origin-Destination Matrix (Computed at Destination)						
Origin (From)	Destination (To)					
	Office	Retail	Restaurant	Cinema/Entertainment	Residential	Hotel
Office		0	0	0	4	0
Retail	37		0	0	45	0
Restaurant	36	0		0	16	0
Cinema/Entertainment	7	0	0		4	0
Residential	68	0	0	0		0
Hotel	0	0	0	0	0	

Table 9-P (D): Internal and External Trips Summary (Entering Trips)						
Destination Land Use	Person-Trip Estimates			External Trips by Mode*		
	Internal	External	Total	Vehicles ¹	Transit ²	Non-Motorized ²
Office	2	118	120	106	0	0
Retail	0	0	0	0	0	0
Restaurant	0	0	0	0	0	0
Cinema/Entertainment	0	0	0	0	0	0
Residential	4	93	97	60	0	0
Hotel	0	0	0	0	0	0
All Other Land Uses ³	0	0	0	0	0	0

Table 9-P (O): Internal and External Trips Summary (Exiting Trips)						
Origin Land Use	Person-Trip Estimates			External Trips by Mode*		
	Internal	External	Total	Vehicles ¹	Transit ²	Non-Motorized ²
Office	4	359	363	323	0	0
Retail	0	0	0	0	0	0
Restaurant	0	0	0	0	0	0
Cinema/Entertainment	0	0	0	0	0	0
Residential	2	55	57	36	0	0
Hotel	0	0	0	0	0	0
All Other Land Uses ³	0	0	0	0	0	0

¹Vehicle-trips computed using the mode split and vehicle occupancy values provided in Table 2-P
²Person-Trips
³Total estimate for all other land uses at mixed-use development site-not subject to internal trip capture computations in this estimator
*Indicates computation that has been rounded to the nearest whole number.

Appendix D:
***Synchro* Output Sheets**

Existing (2021) AM Peak
1: Oakmont Dr & Lake Sharon Dr

Intersection	
Intersection Delay, s/veh	20.7
Intersection LOS	C

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↵	↕↗		↵	↕↗		↵	↗		↵	↗	
Traffic Vol, veh/h	52	119	15	92	90	71	12	179	75	84	192	49
Future Vol, veh/h	52	119	15	92	90	71	12	179	75	84	192	49
Peak Hour Factor	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77
Heavy Vehicles, %	4	2	2	2	2	2	2	2	2	2	3	2
Mvmt Flow	68	155	19	119	117	92	16	232	97	109	249	64
Number of Lanes	1	2	0	1	2	0	1	1	0	1	1	0

Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	3	3	2	2
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	2	2	3	3
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	2	2	3	3
HCM Control Delay	14	14.5	28.5	23
HCM LOS	B	B	D	C

Lane	NBLn1	NBLn2	EBLn1	EBLn2	EBLn3	WBLn1	WBLn2	WBLn3	SBLn1	SBLn2
Vol Left, %	100%	0%	100%	0%	0%	100%	0%	0%	100%	0%
Vol Thru, %	0%	70%	0%	100%	73%	0%	100%	30%	0%	80%
Vol Right, %	0%	30%	0%	0%	27%	0%	0%	70%	0%	20%
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop
Traffic Vol by Lane	12	254	52	79	55	92	60	101	84	241
LT Vol	12	0	52	0	0	92	0	0	84	0
Through Vol	0	179	0	79	40	0	60	30	0	192
RT Vol	0	75	0	0	15	0	0	71	0	49
Lane Flow Rate	16	330	68	103	71	119	78	131	109	313
Geometry Grp	8	8	8	8	8	8	8	8	8	8
Degree of Util (X)	0.038	0.729	0.175	0.252	0.169	0.3	0.185	0.292	0.258	0.685
Departure Headway (Hd)	8.671	7.955	9.342	8.788	8.588	9.045	8.527	8.017	8.51	7.877
Convergence, Y/N	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
Cap	414	455	384	408	417	398	421	447	423	460
Service Time	6.391	5.675	7.102	6.547	6.348	6.801	6.283	5.773	6.229	5.596
HCM Lane V/C Ratio	0.039	0.725	0.177	0.252	0.17	0.299	0.185	0.293	0.258	0.68
HCM Control Delay	11.7	29.3	14.1	14.5	13.1	15.7	13.2	14.1	14.2	26.1
HCM Lane LOS	B	D	B	B	B	C	B	B	B	D
HCM 95th-tile Q	0.1	5.8	0.6	1	0.6	1.2	0.7	1.2	1	5.1

Existing (2021) AM Peak
2: Oakmont Dr & Ardglass Trail

Intersection						
Int Delay, s/veh	1.4					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T		T
Traffic Vol, veh/h	34	17	10	296	308	21
Future Vol, veh/h	34	17	10	296	308	21
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	70	70	70	70	70	70
Heavy Vehicles, %	3	2	2	2	2	5
Mvmt Flow	49	24	14	423	440	30

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	906	455	470	0	-	0
Stage 1	455	-	-	-	-	-
Stage 2	451	-	-	-	-	-
Critical Hdwy	6.43	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.43	-	-	-	-	-
Critical Hdwy Stg 2	5.43	-	-	-	-	-
Follow-up Hdwy	3.527	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	305	605	1092	-	-	-
Stage 1	637	-	-	-	-	-
Stage 2	640	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	300	605	1092	-	-	-
Mov Cap-2 Maneuver	300	-	-	-	-	-
Stage 1	626	-	-	-	-	-
Stage 2	640	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	17.5	0.3	0
HCM LOS	C		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1092	-	361	-	-
HCM Lane V/C Ratio	0.013	-	0.202	-	-
HCM Control Delay (s)	8.3	0	17.5	-	-
HCM Lane LOS	A	A	C	-	-
HCM 95th %tile Q(veh)	0	-	0.7	-	-

Existing (2021) AM Peak
3: Oakmont Dr & Creekside Dr

Intersection	
Intersection Delay, s/veh	13.5
Intersection LOS	B

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕	↕	↕	↕	
Traffic Vol, veh/h	7	7	14	159	10	135	7	123	149	113	179	10
Future Vol, veh/h	7	7	14	159	10	135	7	123	149	113	179	10
Peak Hour Factor	0.81	0.81	0.81	0.81	0.81	0.81	0.81	0.81	0.81	0.81	0.81	0.81
Heavy Vehicles, %	2	2	2	3	2	2	2	2	5	2	2	11
Mvmt Flow	9	9	17	196	12	167	9	152	184	140	221	12
Number of Lanes	0	1	0	0	1	0	0	1	1	1	1	0

Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	1	1	2	2
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	2	2	1	1
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	2	2	1	1
HCM Control Delay	9.9	16.7	11.2	12.8
HCM LOS	A	C	B	B

Lane	NBLn1	NBLn2	EBLn1	WBLn1	SBLn1	SBLn2
Vol Left, %	5%	0%	25%	52%	100%	0%
Vol Thru, %	95%	0%	25%	3%	0%	95%
Vol Right, %	0%	100%	50%	44%	0%	5%
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop
Traffic Vol by Lane	130	149	28	304	113	189
LT Vol	7	0	7	159	113	0
Through Vol	123	0	7	10	0	179
RT Vol	0	149	14	135	0	10
Lane Flow Rate	160	184	35	375	140	233
Geometry Grp	7	7	2	2	7	7
Degree of Util (X)	0.286	0.29	0.061	0.592	0.264	0.406
Departure Headway (Hd)	6.407	5.666	6.372	5.677	6.805	6.258
Convergence, Y/N	Yes	Yes	Yes	Yes	Yes	Yes
Cap	559	631	557	634	526	574
Service Time	4.172	3.43	4.465	3.732	4.568	4.021
HCM Lane V/C Ratio	0.286	0.292	0.063	0.591	0.266	0.406
HCM Control Delay	11.7	10.8	9.9	16.7	12	13.3
HCM Lane LOS	B	B	A	C	B	B
HCM 95th-tile Q	1.2	1.2	0.2	3.9	1.1	2

Existing (2021) PM Peak
1: Oakmont Dr & Lake Sharon Dr

Intersection	
Intersection Delay, s/veh	16.3
Intersection LOS	C

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↵	↕↗		↵	↕↗		↵	↗		↵	↗	
Traffic Vol, veh/h	47	128	23	84	125	29	22	106	121	39	143	44
Future Vol, veh/h	47	128	23	84	125	29	22	106	121	39	143	44
Peak Hour Factor	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74
Heavy Vehicles, %	2	2	5	4	2	2	2	2	2	3	3	8
Mvmt Flow	64	173	31	114	169	39	30	143	164	53	193	59
Number of Lanes	1	2	0	1	2	0	1	1	0	1	1	0

Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	3	3	2	2
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	2	2	3	3
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	2	2	3	3
HCM Control Delay	13.1	13.4	20.4	17.6
HCM LOS	B	B	C	C

Lane	NBLn1	NBLn2	EBLn1	EBLn2	EBLn3	WBLn1	WBLn2	WBLn3	SBLn1	SBLn2
Vol Left, %	100%	0%	100%	0%	0%	100%	0%	0%	100%	0%
Vol Thru, %	0%	47%	0%	100%	65%	0%	100%	59%	0%	76%
Vol Right, %	0%	53%	0%	0%	35%	0%	0%	41%	0%	24%
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop
Traffic Vol by Lane	22	227	47	85	66	84	83	71	39	187
LT Vol	22	0	47	0	0	84	0	0	39	0
Through Vol	0	106	0	85	43	0	83	42	0	143
RT Vol	0	121	0	0	23	0	0	29	0	44
Lane Flow Rate	30	307	64	115	89	114	113	95	53	253
Geometry Grp	8	8	8	8	8	8	8	8	8	8
Degree of Util (X)	0.067	0.62	0.151	0.258	0.194	0.267	0.248	0.202	0.121	0.532
Departure Headway (Hd)	8.157	7.276	8.579	8.063	7.863	8.463	7.913	7.618	8.25	7.578
Convergence, Y/N	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
Cap	438	494	417	444	454	423	453	469	433	475
Service Time	5.928	5.046	6.362	5.846	5.645	6.241	5.691	5.395	6.023	5.351
HCM Lane V/C Ratio	0.068	0.621	0.153	0.259	0.196	0.27	0.249	0.203	0.122	0.533
HCM Control Delay	11.5	21.3	12.9	13.7	12.5	14.3	13.3	12.3	12.2	18.7
HCM Lane LOS	B	C	B	B	B	B	B	B	B	C
HCM 95th-tile Q	0.2	4.2	0.5	1	0.7	1.1	1	0.7	0.4	3.1

Existing (2021) PM Peak
2: Oakmont Dr & Ardglass Trail

Intersection						
Int Delay, s/veh	1.7					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T		T		T	
Traffic Vol, veh/h	33	21	22	176	253	26
Future Vol, veh/h	33	21	22	176	253	26
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	79	79	79	79	79	79
Heavy Vehicles, %	4	11	10	2	2	9
Mvmt Flow	42	27	28	223	320	33

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	616	337	353	0	-	0
Stage 1	337	-	-	-	-	-
Stage 2	279	-	-	-	-	-
Critical Hdwy	6.44	6.31	4.2	-	-	-
Critical Hdwy Stg 1	5.44	-	-	-	-	-
Critical Hdwy Stg 2	5.44	-	-	-	-	-
Follow-up Hdwy	3.536	3.399	2.29	-	-	-
Pot Cap-1 Maneuver	451	685	1163	-	-	-
Stage 1	719	-	-	-	-	-
Stage 2	764	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	439	685	1163	-	-	-
Mov Cap-2 Maneuver	439	-	-	-	-	-
Stage 1	700	-	-	-	-	-
Stage 2	764	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	13.1	0.9	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1163	-	510	-	-
HCM Lane V/C Ratio	0.024	-	0.134	-	-
HCM Control Delay (s)	8.2	0	13.1	-	-
HCM Lane LOS	A	A	B	-	-
HCM 95th %tile Q(veh)	0.1	-	0.5	-	-

Existing (2021) PM Peak
3: Oakmont Dr & Creekside Dr

Intersection	
Intersection Delay, s/veh	10.1
Intersection LOS	B

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕	↕	↕	↕	
Traffic Vol, veh/h	6	5	5	70	4	85	8	128	57	70	96	13
Future Vol, veh/h	6	5	5	70	4	85	8	128	57	70	96	13
Peak Hour Factor	0.70	0.70	0.70	0.70	0.70	0.70	0.70	0.70	0.70	0.70	0.70	0.70
Heavy Vehicles, %	2	2	2	7	2	3	2	2	2	3	2	2
Mvmt Flow	9	7	7	100	6	121	11	183	81	100	137	19
Number of Lanes	0	1	0	0	1	0	0	1	1	1	1	0

Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	1	1	2	2
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	2	2	1	1
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	2	2	1	1
HCM Control Delay	8.8	10.6	9.9	9.9
HCM LOS	A	B	A	A

Lane	NBLn1	NBLn2	EBLn1	WBLn1	SBLn1	SBLn2
Vol Left, %	6%	0%	38%	44%	100%	0%
Vol Thru, %	94%	0%	31%	3%	0%	88%
Vol Right, %	0%	100%	31%	53%	0%	12%
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop
Traffic Vol by Lane	136	57	16	159	70	109
LT Vol	8	0	6	70	70	0
Through Vol	128	0	5	4	0	96
RT Vol	0	57	5	85	0	13
Lane Flow Rate	194	81	23	227	100	156
Geometry Grp	7	7	2	2	7	7
Degree of Util (X)	0.296	0.108	0.035	0.32	0.166	0.233
Departure Headway (Hd)	5.492	4.756	5.554	5.075	5.986	5.38
Convergence, Y/N	Yes	Yes	Yes	Yes	Yes	Yes
Cap	649	744	648	702	594	660
Service Time	3.28	2.543	3.554	3.148	3.777	3.17
HCM Lane V/C Ratio	0.299	0.109	0.035	0.323	0.168	0.236
HCM Control Delay	10.6	8.1	8.8	10.6	10	9.8
HCM Lane LOS	B	A	A	B	A	A
HCM 95th-tile Q	1.2	0.4	0.1	1.4	0.6	0.9

Build-Out Year (2023) Background AM Peak
1: Oakmont Dr & Lake Sharon Dr

Intersection	
Intersection Delay, s/veh	26.5
Intersection LOS	D

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↵	↕↗		↵	↕↗		↵	↗		↵	↗	
Traffic Vol, veh/h	56	129	16	100	97	77	13	194	81	91	208	53
Future Vol, veh/h	56	129	16	100	97	77	13	194	81	91	208	53
Peak Hour Factor	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77
Heavy Vehicles, %	4	2	2	2	2	2	2	2	2	2	3	2
Mvmt Flow	73	168	21	130	126	100	17	252	105	118	270	69
Number of Lanes	1	2	0	1	2	0	1	1	0	1	1	0

Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	3	3	2	2
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	2	2	3	3
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	2	2	3	3
HCM Control Delay	15.2	15.9	40	30.2
HCM LOS	C	C	E	D

Lane	NBLn1	NBLn2	EBLn1	EBLn2	EBLn3	WBLn1	WBLn2	WBLn3	SBLn1	SBLn2
Vol Left, %	100%	0%	100%	0%	0%	100%	0%	0%	100%	0%
Vol Thru, %	0%	71%	0%	100%	73%	0%	100%	30%	0%	80%
Vol Right, %	0%	29%	0%	0%	27%	0%	0%	70%	0%	20%
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop
Traffic Vol by Lane	13	275	56	86	59	100	65	109	91	261
LT Vol	13	0	56	0	0	100	0	0	91	0
Through Vol	0	194	0	86	43	0	65	32	0	208
RT Vol	0	81	0	0	16	0	0	77	0	53
Lane Flow Rate	17	357	73	112	77	130	84	142	118	339
Geometry Grp	8	8	8	8	8	8	8	8	8	8
Degree of Util (X)	0.043	0.833	0.2	0.29	0.195	0.345	0.211	0.337	0.294	0.783
Departure Headway (Hd)	9.112	8.394	9.907	9.349	9.151	9.567	9.046	8.533	8.947	8.312
Convergence, Y/N	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
Cap	393	432	361	384	392	375	396	420	402	434
Service Time	6.874	6.156	7.68	7.122	6.923	7.337	6.816	6.302	6.707	6.072
HCM Lane V/C Ratio	0.043	0.826	0.202	0.292	0.196	0.347	0.212	0.338	0.294	0.781
HCM Control Delay	12.3	41.3	15.2	15.9	14.1	17.3	14.2	15.6	15.4	35.3
HCM Lane LOS	B	E	C	C	B	C	B	C	C	E
HCM 95th-tile Q	0.1	7.9	0.7	1.2	0.7	1.5	0.8	1.5	1.2	6.8

Build-Out Year (2023) Background AM Peak
2: Oakmont Dr & Ardglass Trail

Intersection						
Int Delay, s/veh	1.6					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T		
Traffic Vol, veh/h	37	18	11	320	333	23
Future Vol, veh/h	37	18	11	320	333	23
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	70	70	70	70	70	70
Heavy Vehicles, %	3	2	2	2	2	5
Mvmt Flow	53	26	16	457	476	33

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	982	493	509	0	-	0
Stage 1	493	-	-	-	-	-
Stage 2	489	-	-	-	-	-
Critical Hdwy	6.43	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.43	-	-	-	-	-
Critical Hdwy Stg 2	5.43	-	-	-	-	-
Follow-up Hdwy	3.527	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	275	576	1056	-	-	-
Stage 1	612	-	-	-	-	-
Stage 2	614	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	270	576	1056	-	-	-
Mov Cap-2 Maneuver	270	-	-	-	-	-
Stage 1	600	-	-	-	-	-
Stage 2	614	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	19.5	0.3	0
HCM LOS	C		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1056	-	327	-	-
HCM Lane V/C Ratio	0.015	-	0.24	-	-
HCM Control Delay (s)	8.5	0	19.5	-	-
HCM Lane LOS	A	A	C	-	-
HCM 95th %tile Q(veh)	0	-	0.9	-	-

Build-Out Year (2023) Background AM Peak
3: Oakmont Dr & Creekside Dr

Intersection	
Intersection Delay, s/veh	15.1
Intersection LOS	C

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕	↕	↕	↕	
Traffic Vol, veh/h	8	8	15	172	11	146	8	133	161	122	194	11
Future Vol, veh/h	8	8	15	172	11	146	8	133	161	122	194	11
Peak Hour Factor	0.81	0.81	0.81	0.81	0.81	0.81	0.81	0.81	0.81	0.81	0.81	0.81
Heavy Vehicles, %	2	2	2	3	2	2	2	2	5	2	2	11
Mvmt Flow	10	10	19	212	14	180	10	164	199	151	240	14
Number of Lanes	0	1	0	0	1	0	0	1	1	1	1	0

Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	1	1	2	2
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	2	2	1	1
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	2	2	1	1
HCM Control Delay	10.3	19.6	12	13.9
HCM LOS	B	C	B	B

Lane	NBLn1	NBLn2	EBLn1	WBLn1	SBLn1	SBLn2
Vol Left, %	6%	0%	26%	52%	100%	0%
Vol Thru, %	94%	0%	26%	3%	0%	95%
Vol Right, %	0%	100%	48%	44%	0%	5%
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop
Traffic Vol by Lane	141	161	31	329	122	205
LT Vol	8	0	8	172	122	0
Through Vol	133	0	8	11	0	194
RT Vol	0	161	15	146	0	11
Lane Flow Rate	174	199	38	406	151	253
Geometry Grp	7	7	2	2	7	7
Degree of Util (X)	0.321	0.325	0.072	0.66	0.294	0.455
Departure Headway (Hd)	6.634	5.89	6.808	5.847	7.023	6.474
Convergence, Y/N	Yes	Yes	Yes	Yes	Yes	Yes
Cap	538	606	529	613	509	552
Service Time	4.42	3.675	4.808	3.916	4.807	4.258
HCM Lane V/C Ratio	0.323	0.328	0.072	0.662	0.297	0.458
HCM Control Delay	12.6	11.5	10.3	19.6	12.7	14.6
HCM Lane LOS	B	B	B	C	B	B
HCM 95th-tile Q	1.4	1.4	0.2	4.9	1.2	2.4

Build-Out Year (2023) Background PM Peak
1: Oakmont Dr & Lake Sharon Dr

Intersection	
Intersection Delay, s/veh	19.4
Intersection LOS	C

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↵	↕↗		↵	↕↗		↵	↗		↵	↗	
Traffic Vol, veh/h	51	138	25	91	135	31	24	115	131	42	155	48
Future Vol, veh/h	51	138	25	91	135	31	24	115	131	42	155	48
Peak Hour Factor	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74
Heavy Vehicles, %	2	2	5	4	2	2	2	2	2	3	3	8
Mvmt Flow	69	186	34	123	182	42	32	155	177	57	209	65
Number of Lanes	1	2	0	1	2	0	1	1	0	1	1	0

Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	3	3	2	2
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	2	2	3	3
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	2	2	3	3
HCM Control Delay	14.2	14.6	26.6	21.2
HCM LOS	B	B	D	C

Lane	NBLn1	NBLn2	EBLn1	EBLn2	EBLn3	WBLn1	WBLn2	WBLn3	SBLn1	SBLn2
Vol Left, %	100%	0%	100%	0%	0%	100%	0%	0%	100%	0%
Vol Thru, %	0%	47%	0%	100%	65%	0%	100%	59%	0%	76%
Vol Right, %	0%	53%	0%	0%	35%	0%	0%	41%	0%	24%
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop
Traffic Vol by Lane	24	246	51	92	71	91	90	76	42	203
LT Vol	24	0	51	0	0	91	0	0	42	0
Through Vol	0	115	0	92	46	0	90	45	0	155
RT Vol	0	131	0	0	25	0	0	31	0	48
Lane Flow Rate	32	332	69	124	96	123	122	103	57	274
Geometry Grp	8	8	8	8	8	8	8	8	8	8
Degree of Util (X)	0.078	0.719	0.175	0.297	0.224	0.307	0.285	0.232	0.138	0.614
Departure Headway (Hd)	8.667	7.783	9.128	8.61	8.407	8.991	8.439	8.143	8.737	8.062
Convergence, Y/N	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
Cap	416	469	393	418	427	400	426	441	410	447
Service Time	6.367	5.483	6.885	6.366	6.164	6.745	6.192	5.896	6.488	5.813
HCM Lane V/C Ratio	0.077	0.708	0.176	0.297	0.225	0.307	0.286	0.234	0.139	0.613
HCM Control Delay	12.1	28	13.8	15	13.6	15.7	14.5	13.4	12.9	22.9
HCM Lane LOS	B	D	B	B	B	C	B	B	B	C
HCM 95th-tile Q	0.3	5.7	0.6	1.2	0.8	1.3	1.2	0.9	0.5	4

Build-Out Year (2023) Background PM Peak
2: Oakmont Dr & Ardglass Trail

Intersection						
Int Delay, s/veh	1.8					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T			T		T
Traffic Vol, veh/h	36	23	24	190	274	28
Future Vol, veh/h	36	23	24	190	274	28
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	79	79	79	79	79	79
Heavy Vehicles, %	4	11	10	2	2	9
Mvmt Flow	46	29	30	241	347	35

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	666	365	382	0	-	0
Stage 1	365	-	-	-	-	-
Stage 2	301	-	-	-	-	-
Critical Hdwy	6.44	6.31	4.2	-	-	-
Critical Hdwy Stg 1	5.44	-	-	-	-	-
Critical Hdwy Stg 2	5.44	-	-	-	-	-
Follow-up Hdwy	3.536	3.399	2.29	-	-	-
Pot Cap-1 Maneuver	421	660	1134	-	-	-
Stage 1	698	-	-	-	-	-
Stage 2	746	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	408	660	1134	-	-	-
Mov Cap-2 Maneuver	408	-	-	-	-	-
Stage 1	676	-	-	-	-	-
Stage 2	746	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	13.9	0.9	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1134	-	479	-	-
HCM Lane V/C Ratio	0.027	-	0.156	-	-
HCM Control Delay (s)	8.3	0	13.9	-	-
HCM Lane LOS	A	A	B	-	-
HCM 95th %tile Q(veh)	0.1	-	0.5	-	-

Build-Out Year (2023) Background PM Peak
3: Oakmont Dr & Creekside Dr

Intersection	
Intersection Delay, s/veh	10.6
Intersection LOS	B

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕	↕	↕	↕	
Traffic Vol, veh/h	6	5	5	76	4	92	9	138	62	76	104	14
Future Vol, veh/h	6	5	5	76	4	92	9	138	62	76	104	14
Peak Hour Factor	0.70	0.70	0.70	0.70	0.70	0.70	0.70	0.70	0.70	0.70	0.70	0.70
Heavy Vehicles, %	2	2	2	7	2	3	2	2	2	3	2	2
Mvmt Flow	9	7	7	109	6	131	13	197	89	109	149	20
Number of Lanes	0	1	0	0	1	0	0	1	1	1	1	0

Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	1	1	2	2
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	2	2	1	1
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	2	2	1	1
HCM Control Delay	9	11.2	10.4	10.3
HCM LOS	A	B	B	B

Lane	NBLn1	NBLn2	EBLn1	WBLn1	SBLn1	SBLn2
Vol Left, %	6%	0%	38%	44%	100%	0%
Vol Thru, %	94%	0%	31%	2%	0%	88%
Vol Right, %	0%	100%	31%	53%	0%	12%
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop
Traffic Vol by Lane	147	62	16	172	76	118
LT Vol	9	0	6	76	76	0
Through Vol	138	0	5	4	0	104
RT Vol	0	62	5	92	0	14
Lane Flow Rate	210	89	23	246	109	169
Geometry Grp	7	7	2	2	7	7
Degree of Util (X)	0.332	0.122	0.036	0.361	0.187	0.261
Departure Headway (Hd)	5.687	4.948	5.727	5.287	6.184	5.577
Convergence, Y/N	Yes	Yes	Yes	Yes	Yes	Yes
Cap	634	725	625	686	581	646
Service Time	3.414	2.675	3.766	3.287	3.911	3.304
HCM Lane V/C Ratio	0.331	0.123	0.037	0.359	0.188	0.262
HCM Control Delay	11.2	8.4	9	11.2	10.3	10.3
HCM Lane LOS	B	A	A	B	B	B
HCM 95th-tile Q	1.5	0.4	0.1	1.6	0.7	1

Build-Out (2023) Total AM Peak - Existing Site Plan
 1: Oakmont Dr & Lake Sharon Dr

Intersection	
Intersection Delay, s/veh	28.3
Intersection LOS	D

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↵	↕↗		↵	↕↗		↵	↗		↵	↗	
Traffic Vol, veh/h	56	152	26	100	105	78	16	194	81	95	209	47
Future Vol, veh/h	56	152	26	100	105	78	16	194	81	95	209	47
Peak Hour Factor	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77
Heavy Vehicles, %	4	2	2	2	2	2	2	2	2	2	3	2
Mvmt Flow	73	197	34	130	136	101	21	252	105	123	271	61
Number of Lanes	1	2	0	1	2	0	1	1	0	1	1	0

Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	3	3	2	2
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	2	2	3	3
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	2	2	3	3
HCM Control Delay	16.3	16.7	44.7	32.1
HCM LOS	C	C	E	D

Lane	NBLn1	NBLn2	EBLn1	EBLn2	EBLn3	WBLn1	WBLn2	WBLn3	SBLn1	SBLn2
Vol Left, %	100%	0%	100%	0%	0%	100%	0%	0%	100%	0%
Vol Thru, %	0%	71%	0%	100%	66%	0%	100%	31%	0%	82%
Vol Right, %	0%	29%	0%	0%	34%	0%	0%	69%	0%	18%
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop
Traffic Vol by Lane	16	275	56	101	77	100	70	113	95	256
LT Vol	16	0	56	0	0	100	0	0	95	0
Through Vol	0	194	0	101	51	0	70	35	0	209
RT Vol	0	81	0	0	26	0	0	78	0	47
Lane Flow Rate	21	357	73	132	100	130	91	147	123	332
Geometry Grp	8	8	8	8	8	8	8	8	8	8
Degree of Util (X)	0.054	0.863	0.204	0.348	0.257	0.356	0.236	0.36	0.317	0.798
Departure Headway (Hd)	9.413	8.694	10.092	9.533	9.285	9.858	9.336	8.831	9.259	8.636
Convergence, Y/N	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
Cap	380	414	355	376	385	364	384	407	387	419
Service Time	7.188	6.469	7.881	7.322	7.073	7.641	7.118	6.613	7.035	6.412
HCM Lane V/C Ratio	0.055	0.862	0.206	0.351	0.26	0.357	0.237	0.361	0.318	0.792
HCM Control Delay	12.7	46.6	15.5	17.4	15.3	18	15	16.5	16.3	38
HCM Lane LOS	B	E	C	C	C	C	B	C	C	E
HCM 95th-tile Q	0.2	8.5	0.8	1.5	1	1.6	0.9	1.6	1.3	7.1

Build-Out (2023) Total AM Peak - Existing Site Plan
2: Oakmont Dr & Ardglass Trail

Intersection						
Int Delay, s/veh	1.3					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T		T		T	
Traffic Vol, veh/h	35	11	6	332	336	22
Future Vol, veh/h	35	11	6	332	336	22
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	70	70	70	70	70	70
Heavy Vehicles, %	3	2	2	2	2	5
Mvmt Flow	50	16	9	474	480	31

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	988	496	511	0	-	0
Stage 1	496	-	-	-	-	-
Stage 2	492	-	-	-	-	-
Critical Hdwy	6.43	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.43	-	-	-	-	-
Critical Hdwy Stg 2	5.43	-	-	-	-	-
Follow-up Hdwy	3.527	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	273	574	1054	-	-	-
Stage 1	610	-	-	-	-	-
Stage 2	612	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	270	574	1054	-	-	-
Mov Cap-2 Maneuver	270	-	-	-	-	-
Stage 1	603	-	-	-	-	-
Stage 2	612	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	19.8	0.1	0
HCM LOS	C		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1054	-	309	-	-
HCM Lane V/C Ratio	0.008	-	0.213	-	-
HCM Control Delay (s)	8.4	0	19.8	-	-
HCM Lane LOS	A	A	C	-	-
HCM 95th %tile Q(veh)	0	-	0.8	-	-

Build-Out (2023) Total AM Peak - Existing Site Plan
3: Oakmont Dr & Creekside Dr

Intersection	
Intersection Delay, s/veh	15.3
Intersection LOS	C

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕	↕	↕	↕	
Traffic Vol, veh/h	8	8	15	173	11	146	8	139	165	122	195	11
Future Vol, veh/h	8	8	15	173	11	146	8	139	165	122	195	11
Peak Hour Factor	0.81	0.81	0.81	0.81	0.81	0.81	0.81	0.81	0.81	0.81	0.81	0.81
Heavy Vehicles, %	2	2	2	3	2	2	2	2	5	2	2	11
Mvmt Flow	10	10	19	214	14	180	10	172	204	151	241	14
Number of Lanes	0	1	0	0	1	0	0	1	1	1	1	0

Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	1	1	2	2
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	2	2	1	1
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	2	2	1	1
HCM Control Delay	10.4	20	12.2	14.1
HCM LOS	B	C	B	B

Lane	NBLn1	NBLn2	EBLn1	WBLn1	SBLn1	SBLn2
Vol Left, %	5%	0%	26%	52%	100%	0%
Vol Thru, %	95%	0%	26%	3%	0%	95%
Vol Right, %	0%	100%	48%	44%	0%	5%
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop
Traffic Vol by Lane	147	165	31	330	122	206
LT Vol	8	0	8	173	122	0
Through Vol	139	0	8	11	0	195
RT Vol	0	165	15	146	0	11
Lane Flow Rate	181	204	38	407	151	254
Geometry Grp	7	7	2	2	7	7
Degree of Util (X)	0.335	0.334	0.073	0.665	0.295	0.46
Departure Headway (Hd)	6.651	5.907	6.86	5.879	7.056	6.507
Convergence, Y/N	Yes	Yes	Yes	Yes	Yes	Yes
Cap	537	603	525	611	506	549
Service Time	4.437	3.693	4.86	3.95	4.841	4.292
HCM Lane V/C Ratio	0.337	0.338	0.072	0.666	0.298	0.463
HCM Control Delay	12.8	11.7	10.4	20	12.8	14.8
HCM Lane LOS	B	B	B	C	B	B
HCM 95th-tile Q	1.5	1.5	0.2	5	1.2	2.4

Build-Out (2023) Total AM Peak - Existing Site Plan
 4: Lake Sharon Dr & West Driveway

Intersection						
Int Delay, s/veh	0.9					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↑↑	↑↑		↑↑	
Traffic Vol, veh/h	5	205	177	3	19	15
Future Vol, veh/h	5	205	177	3	19	15
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	76	76	76	76	76	76
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	7	270	233	4	25	20

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	237	0	0	384	119
Stage 1	-	-	-	235	-
Stage 2	-	-	-	149	-
Critical Hdwy	4.14	-	-	6.84	6.94
Critical Hdwy Stg 1	-	-	-	5.84	-
Critical Hdwy Stg 2	-	-	-	5.84	-
Follow-up Hdwy	2.22	-	-	3.52	3.32
Pot Cap-1 Maneuver	1327	-	-	591	910
Stage 1	-	-	-	782	-
Stage 2	-	-	-	863	-
Platoon blocked, %		-	-		
Mov Cap-1 Maneuver	1327	-	-	587	910
Mov Cap-2 Maneuver	-	-	-	587	-
Stage 1	-	-	-	777	-
Stage 2	-	-	-	863	-

Approach	EB	WB	SB
HCM Control Delay, s	0.2	0	10.5
HCM LOS			B

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1327	-	-	-	696
HCM Lane V/C Ratio	0.005	-	-	-	0.064
HCM Control Delay (s)	7.7	0	-	-	10.5
HCM Lane LOS	A	A	-	-	B
HCM 95th %tile Q(veh)	0	-	-	-	0.2

Build-Out (2023) Total AM Peak - Existing Site Plan
5: Lake Sharon Dr & Rye Rd

Intersection						
Int Delay, s/veh	1.1					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations	↘	↑↑	↑↑		↘	
Traffic Vol, veh/h	8	216	160	9	18	20
Future Vol, veh/h	8	216	160	9	18	20
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	100	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	76	76	76	76	76	76
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	11	284	211	12	24	26

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	223	0	-	0	381 112
Stage 1	-	-	-	-	217 -
Stage 2	-	-	-	-	164 -
Critical Hdwy	4.14	-	-	-	6.84 6.94
Critical Hdwy Stg 1	-	-	-	-	5.84 -
Critical Hdwy Stg 2	-	-	-	-	5.84 -
Follow-up Hdwy	2.22	-	-	-	3.52 3.32
Pot Cap-1 Maneuver	1343	-	-	-	594 920
Stage 1	-	-	-	-	798 -
Stage 2	-	-	-	-	848 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1343	-	-	-	589 920
Mov Cap-2 Maneuver	-	-	-	-	589 -
Stage 1	-	-	-	-	792 -
Stage 2	-	-	-	-	848 -

Approach	EB	WB	SB
HCM Control Delay, s	0.3	0	10.3
HCM LOS			B

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1343	-	-	-	727
HCM Lane V/C Ratio	0.008	-	-	-	0.069
HCM Control Delay (s)	7.7	-	-	-	10.3
HCM Lane LOS	A	-	-	-	B
HCM 95th %tile Q(veh)	0	-	-	-	0.2

Build-Out (2023) Total AM Peak - Existing Site Plan
6: Oakmont Dr & East Driveway

Intersection						
Int Delay, s/veh	0.3					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Vol, veh/h	8	5	2	330	347	2
Future Vol, veh/h	8	5	2	330	347	2
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	76	76	76	76	76	76
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	11	7	3	434	457	3

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	899	459	460	0	-	0
Stage 1	459	-	-	-	-	-
Stage 2	440	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	309	602	1101	-	-	-
Stage 1	636	-	-	-	-	-
Stage 2	649	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	308	602	1101	-	-	-
Mov Cap-2 Maneuver	308	-	-	-	-	-
Stage 1	633	-	-	-	-	-
Stage 2	649	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	14.9	0	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1101	-	379	-	-
HCM Lane V/C Ratio	0.002	-	0.045	-	-
HCM Control Delay (s)	8.3	0	14.9	-	-
HCM Lane LOS	A	A	B	-	-
HCM 95th %tile Q(veh)	0	-	0.1	-	-

Build-Out (2023) Total PM Peak - Existing Site Plan
1: Oakmont Dr & Lake Sharon Dr

Intersection	
Intersection Delay, s/veh	20.4
Intersection LOS	C

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↵	↕↗		↵	↕↗		↵	↗		↵	↗	
Traffic Vol, veh/h	45	151	31	91	158	34	35	115	131	44	156	44
Future Vol, veh/h	45	151	31	91	158	34	35	115	131	44	156	44
Peak Hour Factor	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74
Heavy Vehicles, %	2	2	5	4	2	2	2	2	2	3	3	8
Mvmt Flow	61	204	42	123	214	46	47	155	177	59	211	59
Number of Lanes	1	2	0	1	2	0	1	1	0	1	1	0

Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	3	3	2	2
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	2	2	3	3
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	2	2	3	3
HCM Control Delay	15.1	15.4	28.1	22.4
HCM LOS	C	C	D	C

Lane	NBLn1	NBLn2	EBLn1	EBLn2	EBLn3	WBLn1	WBLn2	WBLn3	SBLn1	SBLn2
Vol Left, %	100%	0%	100%	0%	0%	100%	0%	0%	100%	0%
Vol Thru, %	0%	47%	0%	100%	62%	0%	100%	61%	0%	78%
Vol Right, %	0%	53%	0%	0%	38%	0%	0%	39%	0%	22%
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop
Traffic Vol by Lane	35	246	45	101	81	91	105	87	44	200
LT Vol	35	0	45	0	0	91	0	0	44	0
Through Vol	0	115	0	101	50	0	105	53	0	156
RT Vol	0	131	0	0	31	0	0	34	0	44
Lane Flow Rate	47	332	61	136	110	123	142	117	59	270
Geometry Grp	8	8	8	8	8	8	8	8	8	8
Degree of Util (X)	0.117	0.739	0.159	0.335	0.264	0.314	0.342	0.272	0.149	0.629
Departure Headway (Hd)	8.888	8.002	9.388	8.868	8.644	9.198	8.645	8.36	9.044	8.38
Convergence, Y/N	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
Cap	403	452	382	405	415	391	416	430	397	430
Service Time	6.639	5.754	7.147	6.627	6.403	6.957	6.403	6.118	6.8	6.135
HCM Lane V/C Ratio	0.117	0.735	0.16	0.336	0.265	0.315	0.341	0.272	0.149	0.628
HCM Control Delay	12.8	30.3	13.9	16.1	14.5	16.1	15.9	14.2	13.4	24.4
HCM Lane LOS	B	D	B	C	B	C	C	B	B	C
HCM 95th-tile Q	0.4	6	0.6	1.4	1	1.3	1.5	1.1	0.5	4.2

Build-Out (2023) Total PM Peak - Existing Site Plan
2: Oakmont Dr & Ardglass Trail

Intersection						
Int Delay, s/veh	1.4					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T		T		T	
Traffic Vol, veh/h	34	15	14	196	285	27
Future Vol, veh/h	34	15	14	196	285	27
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	79	79	79	79	79	79
Heavy Vehicles, %	4	11	10	2	2	9
Mvmt Flow	43	19	18	248	361	34

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	662	378	395	0	-	0
Stage 1	378	-	-	-	-	-
Stage 2	284	-	-	-	-	-
Critical Hdwy	6.44	6.31	4.2	-	-	-
Critical Hdwy Stg 1	5.44	-	-	-	-	-
Critical Hdwy Stg 2	5.44	-	-	-	-	-
Follow-up Hdwy	3.536	3.399	2.29	-	-	-
Pot Cap-1 Maneuver	424	649	1121	-	-	-
Stage 1	688	-	-	-	-	-
Stage 2	760	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	416	649	1121	-	-	-
Mov Cap-2 Maneuver	416	-	-	-	-	-
Stage 1	675	-	-	-	-	-
Stage 2	760	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	13.9	0.6	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1121	-	467	-	-
HCM Lane V/C Ratio	0.016	-	0.133	-	-
HCM Control Delay (s)	8.3	0	13.9	-	-
HCM Lane LOS	A	A	B	-	-
HCM 95th %tile Q(veh)	0	-	0.5	-	-

Build-Out (2023) Total PM Peak - Existing Site Plan
3: Oakmont Dr & Creekside Dr

Intersection	
Intersection Delay, s/veh	10.8
Intersection LOS	B

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕	↕	↕	↕	
Traffic Vol, veh/h	6	5	5	80	4	92	9	140	64	76	110	14
Future Vol, veh/h	6	5	5	80	4	92	9	140	64	76	110	14
Peak Hour Factor	0.70	0.70	0.70	0.70	0.70	0.70	0.70	0.70	0.70	0.70	0.70	0.70
Heavy Vehicles, %	2	2	2	7	2	3	2	2	2	3	2	2
Mvmt Flow	9	7	7	114	6	131	13	200	91	109	157	20
Number of Lanes	0	1	0	0	1	0	0	1	1	1	1	0

Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	1	1	2	2
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	2	2	1	1
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	2	2	1	1
HCM Control Delay	9	11.5	10.5	10.5
HCM LOS	A	B	B	B

Lane	NBLn1	NBLn2	EBLn1	WBLn1	SBLn1	SBLn2
Vol Left, %	6%	0%	38%	45%	100%	0%
Vol Thru, %	94%	0%	31%	2%	0%	89%
Vol Right, %	0%	100%	31%	52%	0%	11%
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop
Traffic Vol by Lane	149	64	16	176	76	124
LT Vol	9	0	6	80	76	0
Through Vol	140	0	5	4	0	110
RT Vol	0	64	5	92	0	14
Lane Flow Rate	213	91	23	251	109	177
Geometry Grp	7	7	2	2	7	7
Degree of Util (X)	0.339	0.127	0.037	0.373	0.188	0.276
Departure Headway (Hd)	5.726	4.987	5.784	5.336	6.22	5.616
Convergence, Y/N	Yes	Yes	Yes	Yes	Yes	Yes
Cap	628	719	618	679	578	639
Service Time	3.456	2.716	3.826	3.336	3.95	3.347
HCM Lane V/C Ratio	0.339	0.127	0.037	0.37	0.189	0.277
HCM Control Delay	11.4	8.4	9	11.5	10.4	10.5
HCM Lane LOS	B	A	A	B	B	B
HCM 95th-tile Q	1.5	0.4	0.1	1.7	0.7	1.1

Build-Out (2023) Total PM Peak - Existing Site Plan
4: Lake Sharon Dr & West Driveway

Intersection						
Int Delay, s/veh	0.8					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↑↑	↑↑		↑↑	
Traffic Vol, veh/h	15	226	216	11	11	9
Future Vol, veh/h	15	226	216	11	11	9
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	74	74	74	74	74	74
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	20	305	292	15	15	12

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	307	0	-	0	493
Stage 1	-	-	-	-	300
Stage 2	-	-	-	-	193
Critical Hdwy	4.14	-	-	-	6.84
Critical Hdwy Stg 1	-	-	-	-	5.84
Critical Hdwy Stg 2	-	-	-	-	5.84
Follow-up Hdwy	2.22	-	-	-	3.52
Pot Cap-1 Maneuver	1250	-	-	-	505
Stage 1	-	-	-	-	725
Stage 2	-	-	-	-	821
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1250	-	-	-	495
Mov Cap-2 Maneuver	-	-	-	-	495
Stage 1	-	-	-	-	711
Stage 2	-	-	-	-	821

Approach	EB	WB	SB
HCM Control Delay, s	0.6	0	11.1
HCM LOS			B

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1250	-	-	-	613
HCM Lane V/C Ratio	0.016	-	-	-	0.044
HCM Control Delay (s)	7.9	0.1	-	-	11.1
HCM Lane LOS	A	A	-	-	B
HCM 95th %tile Q(veh)	0	-	-	-	0.1

Build-Out (2023) Total PM Peak - Existing Site Plan
5: Lake Sharon Dr & Rye Rd

Intersection						
Int Delay, s/veh	0.9					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Vol, veh/h	20	217	211	27	10	16
Future Vol, veh/h	20	217	211	27	10	16
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	100	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	74	74	74	74	74	74
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	27	293	285	36	14	22

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	321	0	-	0	504 161
Stage 1	-	-	-	-	303 -
Stage 2	-	-	-	-	201 -
Critical Hdwy	4.14	-	-	-	6.84 6.94
Critical Hdwy Stg 1	-	-	-	-	5.84 -
Critical Hdwy Stg 2	-	-	-	-	5.84 -
Follow-up Hdwy	2.22	-	-	-	3.52 3.32
Pot Cap-1 Maneuver	1236	-	-	-	497 855
Stage 1	-	-	-	-	723 -
Stage 2	-	-	-	-	813 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1236	-	-	-	486 855
Mov Cap-2 Maneuver	-	-	-	-	486 -
Stage 1	-	-	-	-	707 -
Stage 2	-	-	-	-	813 -

Approach	EB	WB	SB
HCM Control Delay, s	0.7	0	10.7
HCM LOS			B

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1236	-	-	-	662
HCM Lane V/C Ratio	0.022	-	-	-	0.053
HCM Control Delay (s)	8	-	-	-	10.7
HCM Lane LOS	A	-	-	-	B
HCM 95th %tile Q(veh)	0.1	-	-	-	0.2

Build-Out (2023) Total PM Peak - Existing Site Plan
6: Oakmont Dr & East Driveway

Intersection						
Int Delay, s/veh	0.3					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Vol, veh/h	4	3	5	206	292	7
Future Vol, veh/h	4	3	5	206	292	7
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	74	74	74	74	74	74
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	5	4	7	278	395	9

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	692	400	404	0	-	0
Stage 1	400	-	-	-	-	-
Stage 2	292	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	410	650	1155	-	-	-
Stage 1	677	-	-	-	-	-
Stage 2	758	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	407	650	1155	-	-	-
Mov Cap-2 Maneuver	407	-	-	-	-	-
Stage 1	672	-	-	-	-	-
Stage 2	758	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	12.6	0.2	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1155	-	485	-	-
HCM Lane V/C Ratio	0.006	-	0.02	-	-
HCM Control Delay (s)	8.1	0	12.6	-	-
HCM Lane LOS	A	A	B	-	-
HCM 95th %tile Q(veh)	0	-	0.1	-	-

Build-Out (2023) Total AM Peak - Alternate 1
1: Oakmont Dr & Lake Sharon Dr

Intersection	
Intersection Delay, s/veh	29.1
Intersection LOS	D

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↵	↕↗		↵	↕↗		↵	↗		↵	↗	
Traffic Vol, veh/h	64	156	27	100	107	76	16	194	81	91	208	49
Future Vol, veh/h	64	156	27	100	107	76	16	194	81	91	208	49
Peak Hour Factor	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77
Heavy Vehicles, %	4	2	2	2	2	2	2	2	2	2	3	2
Mvmt Flow	83	203	35	130	139	99	21	252	105	118	270	64
Number of Lanes	1	2	0	1	2	0	1	1	0	1	1	0

Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	3	3	2	2
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	2	2	3	3
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	2	2	3	3
HCM Control Delay	16.6	16.9	46.3	33.5
HCM LOS	C	C	E	D

Lane	NBLn1	NBLn2	EBLn1	EBLn2	EBLn3	WBLn1	WBLn2	WBLn3	SBLn1	SBLn2
Vol Left, %	100%	0%	100%	0%	0%	100%	0%	0%	100%	0%
Vol Thru, %	0%	71%	0%	100%	66%	0%	100%	32%	0%	81%
Vol Right, %	0%	29%	0%	0%	34%	0%	0%	68%	0%	19%
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop
Traffic Vol by Lane	16	275	64	104	79	100	71	112	91	257
LT Vol	16	0	64	0	0	100	0	0	91	0
Through Vol	0	194	0	104	52	0	71	36	0	208
RT Vol	0	81	0	0	27	0	0	76	0	49
Lane Flow Rate	21	357	83	135	103	130	93	145	118	334
Geometry Grp	8	8	8	8	8	8	8	8	8	8
Degree of Util (X)	0.055	0.871	0.234	0.359	0.266	0.359	0.243	0.36	0.307	0.809
Departure Headway (Hd)	9.499	8.779	10.129	9.57	9.319	9.949	9.427	8.929	9.352	8.724
Convergence, Y/N	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
Cap	376	411	353	374	384	361	380	402	383	414
Service Time	7.28	6.56	7.919	7.36	7.109	7.737	7.214	6.716	7.134	6.506
HCM Lane V/C Ratio	0.056	0.869	0.235	0.361	0.268	0.36	0.245	0.361	0.308	0.807
HCM Control Delay	12.8	48.2	16	17.7	15.5	18.3	15.2	16.7	16.3	39.6
HCM Lane LOS	B	E	C	C	C	C	C	C	C	E
HCM 95th-tile Q	0.2	8.7	0.9	1.6	1.1	1.6	0.9	1.6	1.3	7.3

Build-Out (2023) Total AM Peak - Alternate 1
2: Oakmont Dr & Ardglass Trail

Intersection						
Int Delay, s/veh	1.3					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Vol, veh/h	35	11	6	332	336	22
Future Vol, veh/h	35	11	6	332	336	22
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	70	70	70	70	70	70
Heavy Vehicles, %	3	2	2	2	2	5
Mvmt Flow	50	16	9	474	480	31
Major/Minor	Minor2	Major1		Major2		
Conflicting Flow All	988	496	511	0	-	0
Stage 1	496	-	-	-	-	-
Stage 2	492	-	-	-	-	-
Critical Hdwy	6.43	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.43	-	-	-	-	-
Critical Hdwy Stg 2	5.43	-	-	-	-	-
Follow-up Hdwy	3.527	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	273	574	1054	-	-	-
Stage 1	610	-	-	-	-	-
Stage 2	612	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	270	574	1054	-	-	-
Mov Cap-2 Maneuver	270	-	-	-	-	-
Stage 1	603	-	-	-	-	-
Stage 2	612	-	-	-	-	-
Approach	EB		NB		SB	
HCM Control Delay, s	19.8		0.1		0	
HCM LOS	C					
Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR	
Capacity (veh/h)	1054	-	309	-	-	
HCM Lane V/C Ratio	0.008	-	0.213	-	-	
HCM Control Delay (s)	8.4	0	19.8	-	-	
HCM Lane LOS	A	A	C	-	-	
HCM 95th %tile Q(veh)	0	-	0.8	-	-	

Build-Out (2023) Total AM Peak - Alternate 1
3: Oakmont Dr & Creekside Dr

Intersection	
Intersection Delay, s/veh	15.3
Intersection LOS	C

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕	↕	↕	↕	
Traffic Vol, veh/h	8	8	15	173	11	146	8	139	165	122	195	11
Future Vol, veh/h	8	8	15	173	11	146	8	139	165	122	195	11
Peak Hour Factor	0.81	0.81	0.81	0.81	0.81	0.81	0.81	0.81	0.81	0.81	0.81	0.81
Heavy Vehicles, %	2	2	2	3	2	2	2	2	5	2	2	11
Mvmt Flow	10	10	19	214	14	180	10	172	204	151	241	14
Number of Lanes	0	1	0	0	1	0	0	1	1	1	1	0

Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	1	1	2	2
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	2	2	1	1
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	2	2	1	1
HCM Control Delay	10.4	20	12.2	14.1
HCM LOS	B	C	B	B

Lane	NBLn1	NBLn2	EBLn1	WBLn1	SBLn1	SBLn2
Vol Left, %	5%	0%	26%	52%	100%	0%
Vol Thru, %	95%	0%	26%	3%	0%	95%
Vol Right, %	0%	100%	48%	44%	0%	5%
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop
Traffic Vol by Lane	147	165	31	330	122	206
LT Vol	8	0	8	173	122	0
Through Vol	139	0	8	11	0	195
RT Vol	0	165	15	146	0	11
Lane Flow Rate	181	204	38	407	151	254
Geometry Grp	7	7	2	2	7	7
Degree of Util (X)	0.335	0.334	0.073	0.665	0.295	0.46
Departure Headway (Hd)	6.651	5.907	6.86	5.879	7.056	6.507
Convergence, Y/N	Yes	Yes	Yes	Yes	Yes	Yes
Cap	537	603	525	611	506	549
Service Time	4.437	3.693	4.86	3.95	4.841	4.292
HCM Lane V/C Ratio	0.337	0.338	0.072	0.666	0.298	0.463
HCM Control Delay	12.8	11.7	10.4	20	12.8	14.8
HCM Lane LOS	B	B	B	C	B	B
HCM 95th-tile Q	1.5	1.5	0.2	5	1.2	2.4

Build-Out (2023) Total AM Peak - Alternate 1
4: Lake Sharon Dr & West Driveway

Intersection						
Int Delay, s/veh	1					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↑↑	↑↑		↑↑	
Traffic Vol, veh/h	5	205	177	5	20	15
Future Vol, veh/h	5	205	177	5	20	15
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	76	76	76	76	76	76
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	7	270	233	7	26	20

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	240	0	-	0	386 120
Stage 1	-	-	-	-	237 -
Stage 2	-	-	-	-	149 -
Critical Hdwy	4.14	-	-	-	6.84 6.94
Critical Hdwy Stg 1	-	-	-	-	5.84 -
Critical Hdwy Stg 2	-	-	-	-	5.84 -
Follow-up Hdwy	2.22	-	-	-	3.52 3.32
Pot Cap-1 Maneuver	1324	-	-	-	590 909
Stage 1	-	-	-	-	780 -
Stage 2	-	-	-	-	863 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1324	-	-	-	586 909
Mov Cap-2 Maneuver	-	-	-	-	586 -
Stage 1	-	-	-	-	775 -
Stage 2	-	-	-	-	863 -

Approach	EB	WB	SB
HCM Control Delay, s	0.2	0	10.6
HCM LOS			B

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1324	-	-	-	691
HCM Lane V/C Ratio	0.005	-	-	-	0.067
HCM Control Delay (s)	7.7	0	-	-	10.6
HCM Lane LOS	A	A	-	-	B
HCM 95th %tile Q(veh)	0	-	-	-	0.2

Build-Out (2023) Total AM Peak - Alternate 1
5: Lake Sharon Dr & Rye Rd

Intersection						
Int Delay, s/veh	1.4					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations	↘	↑↑	↑↑		↘	
Traffic Vol, veh/h	8	217	162	11	30	20
Future Vol, veh/h	8	217	162	11	30	20
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	100	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	76	76	76	76	76	76
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	11	286	213	14	39	26

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	227	0	-	0	385 114
Stage 1	-	-	-	-	220 -
Stage 2	-	-	-	-	165 -
Critical Hdwy	4.14	-	-	-	6.84 6.94
Critical Hdwy Stg 1	-	-	-	-	5.84 -
Critical Hdwy Stg 2	-	-	-	-	5.84 -
Follow-up Hdwy	2.22	-	-	-	3.52 3.32
Pot Cap-1 Maneuver	1339	-	-	-	591 917
Stage 1	-	-	-	-	795 -
Stage 2	-	-	-	-	847 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1339	-	-	-	586 917
Mov Cap-2 Maneuver	-	-	-	-	586 -
Stage 1	-	-	-	-	789 -
Stage 2	-	-	-	-	847 -

Approach	EB	WB	SB
HCM Control Delay, s	0.3	0	10.8
HCM LOS			B

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1339	-	-	-	685
HCM Lane V/C Ratio	0.008	-	-	-	0.096
HCM Control Delay (s)	7.7	-	-	-	10.8
HCM Lane LOS	A	-	-	-	B
HCM 95th %tile Q(veh)	0	-	-	-	0.3

Build-Out (2023) Total PM Peak - Alternate 1
1: Oakmont Dr & Lake Sharon Dr

Intersection	
Intersection Delay, s/veh	20.9
Intersection LOS	C

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↵	↕↗		↵	↕↗		↵	↗		↵	↗	
Traffic Vol, veh/h	49	153	32	91	162	30	36	114	131	42	155	51
Future Vol, veh/h	49	153	32	91	162	30	36	114	131	42	155	51
Peak Hour Factor	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74
Heavy Vehicles, %	2	2	5	4	2	2	2	2	2	3	3	8
Mvmt Flow	66	207	43	123	219	41	49	154	177	57	209	69
Number of Lanes	1	2	0	1	2	0	1	1	0	1	1	0

Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	3	3	2	2
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	2	2	3	3
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	2	2	3	3
HCM Control Delay	15.3	15.7	28.6	23.6
HCM LOS	C	C	D	C

Lane	NBLn1	NBLn2	EBLn1	EBLn2	EBLn3	WBLn1	WBLn2	WBLn3	SBLn1	SBLn2
Vol Left, %	100%	0%	100%	0%	0%	100%	0%	0%	100%	0%
Vol Thru, %	0%	47%	0%	100%	61%	0%	100%	64%	0%	75%
Vol Right, %	0%	53%	0%	0%	39%	0%	0%	36%	0%	25%
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop
Traffic Vol by Lane	36	245	49	102	83	91	108	84	42	206
LT Vol	36	0	49	0	0	91	0	0	42	0
Through Vol	0	114	0	102	51	0	108	54	0	155
RT Vol	0	131	0	0	32	0	0	30	0	51
Lane Flow Rate	49	331	66	138	112	123	146	114	57	278
Geometry Grp	8	8	8	8	8	8	8	8	8	8
Degree of Util (X)	0.121	0.743	0.174	0.342	0.271	0.317	0.354	0.267	0.144	0.651
Departure Headway (Hd)	8.968	8.081	9.446	8.927	8.699	9.284	8.729	8.47	9.108	8.424
Convergence, Y/N	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
Cap	400	447	379	402	413	387	412	423	393	429
Service Time	6.724	5.837	7.208	6.687	6.46	7.042	6.488	6.228	6.866	6.182
HCM Lane V/C Ratio	0.122	0.74	0.174	0.343	0.271	0.318	0.354	0.27	0.145	0.648
HCM Control Delay	13	30.9	14.2	16.3	14.7	16.3	16.2	14.3	13.4	25.7
HCM Lane LOS	B	D	B	C	B	C	C	B	B	D
HCM 95th-tile Q	0.4	6.1	0.6	1.5	1.1	1.3	1.6	1.1	0.5	4.5

Build-Out (2023) Total PM Peak - Alternate 1
2: Oakmont Dr & Ardglass Trail

Intersection						
Int Delay, s/veh	1.4					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T		T		T	
Traffic Vol, veh/h	34	15	14	196	285	27
Future Vol, veh/h	34	15	14	196	285	27
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	79	79	79	79	79	79
Heavy Vehicles, %	4	11	10	2	2	9
Mvmt Flow	43	19	18	248	361	34

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	662	378	395	0	-	0
Stage 1	378	-	-	-	-	-
Stage 2	284	-	-	-	-	-
Critical Hdwy	6.44	6.31	4.2	-	-	-
Critical Hdwy Stg 1	5.44	-	-	-	-	-
Critical Hdwy Stg 2	5.44	-	-	-	-	-
Follow-up Hdwy	3.536	3.399	2.29	-	-	-
Pot Cap-1 Maneuver	424	649	1121	-	-	-
Stage 1	688	-	-	-	-	-
Stage 2	760	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	416	649	1121	-	-	-
Mov Cap-2 Maneuver	416	-	-	-	-	-
Stage 1	675	-	-	-	-	-
Stage 2	760	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	13.9	0.6	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1121	-	467	-	-
HCM Lane V/C Ratio	0.016	-	0.133	-	-
HCM Control Delay (s)	8.3	0	13.9	-	-
HCM Lane LOS	A	A	B	-	-
HCM 95th %tile Q(veh)	0	-	0.5	-	-

Build-Out (2023) Total PM Peak - Alternate 1
3: Oakmont Dr & Creekside Dr

Intersection	
Intersection Delay, s/veh	10.8
Intersection LOS	B

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕	↕	↕	↕	
Traffic Vol, veh/h	6	5	5	80	4	92	9	140	64	76	110	14
Future Vol, veh/h	6	5	5	80	4	92	9	140	64	76	110	14
Peak Hour Factor	0.70	0.70	0.70	0.70	0.70	0.70	0.70	0.70	0.70	0.70	0.70	0.70
Heavy Vehicles, %	2	2	2	7	2	3	2	2	2	3	2	2
Mvmt Flow	9	7	7	114	6	131	13	200	91	109	157	20
Number of Lanes	0	1	0	0	1	0	0	1	1	1	1	0

Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	1	1	2	2
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	2	2	1	1
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	2	2	1	1
HCM Control Delay	9	11.5	10.5	10.5
HCM LOS	A	B	B	B

Lane	NBLn1	NBLn2	EBLn1	WBLn1	SBLn1	SBLn2
Vol Left, %	6%	0%	38%	45%	100%	0%
Vol Thru, %	94%	0%	31%	2%	0%	89%
Vol Right, %	0%	100%	31%	52%	0%	11%
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop
Traffic Vol by Lane	149	64	16	176	76	124
LT Vol	9	0	6	80	76	0
Through Vol	140	0	5	4	0	110
RT Vol	0	64	5	92	0	14
Lane Flow Rate	213	91	23	251	109	177
Geometry Grp	7	7	2	2	7	7
Degree of Util (X)	0.339	0.127	0.037	0.373	0.188	0.276
Departure Headway (Hd)	5.726	4.987	5.784	5.336	6.22	5.616
Convergence, Y/N	Yes	Yes	Yes	Yes	Yes	Yes
Cap	628	719	618	679	578	639
Service Time	3.456	2.716	3.826	3.336	3.95	3.347
HCM Lane V/C Ratio	0.339	0.127	0.037	0.37	0.189	0.277
HCM Control Delay	11.4	8.4	9	11.5	10.4	10.5
HCM Lane LOS	B	A	A	B	B	B
HCM 95th-tile Q	1.5	0.4	0.1	1.7	0.7	1.1

Build-Out (2023) Total PM Peak - Alternate 1
4: Lake Sharon Dr & West Driveway

Intersection						
Int Delay, s/veh	0.7					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↑↑	↑↑		↑↑	
Traffic Vol, veh/h	15	226	216	15	11	9
Future Vol, veh/h	15	226	216	15	11	9
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	74	74	74	74	74	74
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	20	305	292	20	15	12

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	312	0	-	0	495 156
Stage 1	-	-	-	-	302 -
Stage 2	-	-	-	-	193 -
Critical Hdwy	4.14	-	-	-	6.84 6.94
Critical Hdwy Stg 1	-	-	-	-	5.84 -
Critical Hdwy Stg 2	-	-	-	-	5.84 -
Follow-up Hdwy	2.22	-	-	-	3.52 3.32
Pot Cap-1 Maneuver	1245	-	-	-	504 862
Stage 1	-	-	-	-	724 -
Stage 2	-	-	-	-	821 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1245	-	-	-	494 862
Mov Cap-2 Maneuver	-	-	-	-	494 -
Stage 1	-	-	-	-	710 -
Stage 2	-	-	-	-	821 -

Approach	EB	WB	SB
HCM Control Delay, s	0.6	0	11.2
HCM LOS			B

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1245	-	-	-	611
HCM Lane V/C Ratio	0.016	-	-	-	0.044
HCM Control Delay (s)	7.9	0.1	-	-	11.2
HCM Lane LOS	A	A	-	-	B
HCM 95th %tile Q(veh)	0.1	-	-	-	0.1

Build-Out (2023) Total PM Peak - Alternate 1
5: Lake Sharon Dr & Rye Rd

Intersection						
Int Delay, s/veh	1					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations						
Traffic Vol, veh/h	20	217	215	35	17	16
Future Vol, veh/h	20	217	215	35	17	16
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	100	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	74	74	74	74	74	74
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	27	293	291	47	23	22

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	338	0	-	0	516
Stage 1	-	-	-	-	315
Stage 2	-	-	-	-	201
Critical Hdwy	4.14	-	-	-	6.84
Critical Hdwy Stg 1	-	-	-	-	5.84
Critical Hdwy Stg 2	-	-	-	-	5.84
Follow-up Hdwy	2.22	-	-	-	3.52
Pot Cap-1 Maneuver	1218	-	-	-	489
Stage 1	-	-	-	-	713
Stage 2	-	-	-	-	813
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1218	-	-	-	478
Mov Cap-2 Maneuver	-	-	-	-	478
Stage 1	-	-	-	-	697
Stage 2	-	-	-	-	813

Approach	EB	WB	SB
HCM Control Delay, s	0.7	0	11.4
HCM LOS			B

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1218	-	-	-	606
HCM Lane V/C Ratio	0.022	-	-	-	0.074
HCM Control Delay (s)	8	-	-	-	11.4
HCM Lane LOS	A	-	-	-	B
HCM 95th %tile Q(veh)	0.1	-	-	-	0.2

Build-Out (2023) Total AM Peak - Alternate 2
1: Oakmont Dr & Lake Sharon Dr

Intersection	
Intersection Delay, s/veh	28.8
Intersection LOS	D

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↵	↕↗		↵	↕↗		↵	↗		↵	↗	
Traffic Vol, veh/h	61	152	26	100	103	80	15	195	81	95	209	47
Future Vol, veh/h	61	152	26	100	103	80	15	195	81	95	209	47
Peak Hour Factor	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77
Heavy Vehicles, %	4	2	2	2	2	2	2	2	2	2	3	2
Mvmt Flow	79	197	34	130	134	104	19	253	105	123	271	61
Number of Lanes	1	2	0	1	2	0	1	1	0	1	1	0

Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	3	3	2	2
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	2	2	3	3
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	2	2	3	3
HCM Control Delay	16.3	16.8	46.1	32.6
HCM LOS	C	C	E	D

Lane	NBLn1	NBLn2	EBLn1	EBLn2	EBLn3	WBLn1	WBLn2	WBLn3	SBLn1	SBLn2
Vol Left, %	100%	0%	100%	0%	0%	100%	0%	0%	100%	0%
Vol Thru, %	0%	71%	0%	100%	66%	0%	100%	30%	0%	82%
Vol Right, %	0%	29%	0%	0%	34%	0%	0%	70%	0%	18%
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop
Traffic Vol by Lane	15	276	61	101	77	100	69	114	95	256
LT Vol	15	0	61	0	0	100	0	0	95	0
Through Vol	0	195	0	101	51	0	69	34	0	209
RT Vol	0	81	0	0	26	0	0	80	0	47
Lane Flow Rate	19	358	79	132	100	130	89	148	123	332
Geometry Grp	8	8	8	8	8	8	8	8	8	8
Degree of Util (X)	0.051	0.87	0.223	0.349	0.258	0.357	0.232	0.366	0.319	0.802
Departure Headway (Hd)	9.456	8.737	10.118	9.559	9.311	9.904	9.382	8.87	9.303	8.681
Convergence, Y/N	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
Cap	378	412	354	375	384	362	382	404	386	416
Service Time	7.231	6.512	7.905	7.346	7.097	7.69	7.167	6.655	7.078	6.455
HCM Lane V/C Ratio	0.05	0.869	0.223	0.352	0.26	0.359	0.233	0.366	0.319	0.798
HCM Control Delay	12.7	47.9	15.8	17.4	15.3	18.1	15	16.7	16.4	38.6
HCM Lane LOS	B	E	C	C	C	C	B	C	C	E
HCM 95th-tile Q	0.2	8.7	0.8	1.5	1	1.6	0.9	1.6	1.3	7.1

Build-Out (2023) Total AM Peak - Alternate 2
2: Oakmont Dr & Ardglass Trail

Intersection						
Int Delay, s/veh	1.4					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T		T		T	
Traffic Vol, veh/h	35	11	11	332	336	23
Future Vol, veh/h	35	11	11	332	336	23
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	70	70	70	70	70	70
Heavy Vehicles, %	3	2	2	2	2	5
Mvmt Flow	50	16	16	474	480	33

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	1003	497	513	0	-	0
Stage 1	497	-	-	-	-	-
Stage 2	506	-	-	-	-	-
Critical Hdwy	6.43	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.43	-	-	-	-	-
Critical Hdwy Stg 2	5.43	-	-	-	-	-
Follow-up Hdwy	3.527	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	267	573	1052	-	-	-
Stage 1	609	-	-	-	-	-
Stage 2	603	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	261	573	1052	-	-	-
Mov Cap-2 Maneuver	261	-	-	-	-	-
Stage 1	596	-	-	-	-	-
Stage 2	603	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	20.3	0.3	0
HCM LOS	C		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1052	-	300	-	-
HCM Lane V/C Ratio	0.015	-	0.219	-	-
HCM Control Delay (s)	8.5	0	20.3	-	-
HCM Lane LOS	A	A	C	-	-
HCM 95th %tile Q(veh)	0	-	0.8	-	-

Build-Out (2023) Total AM Peak - Alternate 2
3: Oakmont Dr & Creekside Dr

Intersection	
Intersection Delay, s/veh	15.3
Intersection LOS	C

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕	↕	↕	↕	
Traffic Vol, veh/h	8	8	15	173	11	146	8	139	165	122	196	11
Future Vol, veh/h	8	8	15	173	11	146	8	139	165	122	196	11
Peak Hour Factor	0.81	0.81	0.81	0.81	0.81	0.81	0.81	0.81	0.81	0.81	0.81	0.81
Heavy Vehicles, %	2	2	2	3	2	2	2	2	5	2	2	11
Mvmt Flow	10	10	19	214	14	180	10	172	204	151	242	14
Number of Lanes	0	1	0	0	1	0	0	1	1	1	1	0

Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	1	1	2	2
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	2	2	1	1
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	2	2	1	1
HCM Control Delay	10.4	20	12.2	14.1
HCM LOS	B	C	B	B

Lane	NBLn1	NBLn2	EBLn1	WBLn1	SBLn1	SBLn2
Vol Left, %	5%	0%	26%	52%	100%	0%
Vol Thru, %	95%	0%	26%	3%	0%	95%
Vol Right, %	0%	100%	48%	44%	0%	5%
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop
Traffic Vol by Lane	147	165	31	330	122	207
LT Vol	8	0	8	173	122	0
Through Vol	139	0	8	11	0	196
RT Vol	0	165	15	146	0	11
Lane Flow Rate	181	204	38	407	151	256
Geometry Grp	7	7	2	2	7	7
Degree of Util (X)	0.335	0.334	0.073	0.666	0.295	0.462
Departure Headway (Hd)	6.653	5.909	6.865	5.882	7.056	6.507
Convergence, Y/N	Yes	Yes	Yes	Yes	Yes	Yes
Cap	537	603	525	611	506	549
Service Time	4.442	3.698	4.865	3.953	4.843	4.294
HCM Lane V/C Ratio	0.337	0.338	0.072	0.666	0.298	0.466
HCM Control Delay	12.8	11.7	10.4	20	12.8	14.8
HCM Lane LOS	B	B	B	C	B	B
HCM 95th-tile Q	1.5	1.5	0.2	5	1.2	2.4

Build-Out (2023) Total AM Peak - Alternate 2
4: Lake Sharon Dr & West Driveway

Intersection						
Int Delay, s/veh	1					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↑↑	↑↑		↑↑	
Traffic Vol, veh/h	7	202	177	9	19	15
Future Vol, veh/h	7	202	177	9	19	15
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	76	76	76	76	76	76
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	9	266	233	12	25	20

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	245	0	0	390	123
Stage 1	-	-	-	239	-
Stage 2	-	-	-	151	-
Critical Hdwy	4.14	-	-	6.84	6.94
Critical Hdwy Stg 1	-	-	-	5.84	-
Critical Hdwy Stg 2	-	-	-	5.84	-
Follow-up Hdwy	2.22	-	-	3.52	3.32
Pot Cap-1 Maneuver	1318	-	-	586	905
Stage 1	-	-	-	778	-
Stage 2	-	-	-	861	-
Platoon blocked, %		-	-		
Mov Cap-1 Maneuver	1318	-	-	581	905
Mov Cap-2 Maneuver	-	-	-	581	-
Stage 1	-	-	-	772	-
Stage 2	-	-	-	861	-

Approach	EB	WB	SB
HCM Control Delay, s	0.3	0	10.6
HCM LOS			B

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1318	-	-	-	690
HCM Lane V/C Ratio	0.007	-	-	-	0.065
HCM Control Delay (s)	7.8	0	-	-	10.6
HCM Lane LOS	A	A	-	-	B
HCM 95th %tile Q(veh)	0	-	-	-	0.2

Build-Out (2023) Total AM Peak - Alternate 2
5: Lake Sharon Dr & Rye Rd

Intersection						
Int Delay, s/veh	0.9					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↑↑	↑↑		↑	↑
Traffic Vol, veh/h	0	221	166	0	18	20
Future Vol, veh/h	0	221	166	0	18	20
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	0
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	76	76	76	76	76	76
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	291	218	0	24	26

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	-	0	-	0	364 109
Stage 1	-	-	-	-	218 -
Stage 2	-	-	-	-	146 -
Critical Hdwy	-	-	-	-	6.84 6.94
Critical Hdwy Stg 1	-	-	-	-	5.84 -
Critical Hdwy Stg 2	-	-	-	-	5.84 -
Follow-up Hdwy	-	-	-	-	3.52 3.32
Pot Cap-1 Maneuver	0	-	-	0	609 924
Stage 1	0	-	-	0	797 -
Stage 2	0	-	-	0	866 -
Platoon blocked, %		-	-		
Mov Cap-1 Maneuver	-	-	-	-	609 924
Mov Cap-2 Maneuver	-	-	-	-	609 -
Stage 1	-	-	-	-	797 -
Stage 2	-	-	-	-	866 -

Approach	EB	WB	SB
HCM Control Delay, s	0	0	10
HCM LOS			B

Minor Lane/Major Mvmt	EBT	WBT	SBLn1	SBLn2
Capacity (veh/h)	-	-	609	924
HCM Lane V/C Ratio	-	-	0.039	0.028
HCM Control Delay (s)	-	-	11.2	9
HCM Lane LOS	-	-	B	A
HCM 95th %tile Q(veh)	-	-	0.1	0.1

Build-Out (2023) Total AM Peak - Alternate 2
6: Oakmont Dr & East Driveway

Intersection						
Int Delay, s/veh	0.3					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Vol, veh/h	8	5	5	335	347	2
Future Vol, veh/h	8	5	5	335	347	2
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	76	76	76	76	76	76
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	11	7	7	441	457	3

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	914	459	460	0	-	0
Stage 1	459	-	-	-	-	-
Stage 2	455	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	303	602	1101	-	-	-
Stage 1	636	-	-	-	-	-
Stage 2	639	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	301	602	1101	-	-	-
Mov Cap-2 Maneuver	301	-	-	-	-	-
Stage 1	631	-	-	-	-	-
Stage 2	639	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	15.1	0.1	0
HCM LOS	C		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1101	-	373	-	-
HCM Lane V/C Ratio	0.006	-	0.046	-	-
HCM Control Delay (s)	8.3	0	15.1	-	-
HCM Lane LOS	A	A	C	-	-
HCM 95th %tile Q(veh)	0	-	0.1	-	-

Build-Out (2023) Total PM Peak - Alternate 2
1: Oakmont Dr & Lake Sharon Dr

Intersection	
Intersection Delay, s/veh	21.2
Intersection LOS	C

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↵	↕↗		↵	↕↗		↵	↗		↵	↗	
Traffic Vol, veh/h	57	151	31	91	154	38	31	119	131	44	156	44
Future Vol, veh/h	57	151	31	91	154	38	31	119	131	44	156	44
Peak Hour Factor	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74
Heavy Vehicles, %	2	2	5	4	2	2	2	2	2	3	3	8
Mvmt Flow	77	204	42	123	208	51	42	161	177	59	211	59
Number of Lanes	1	2	0	1	2	0	1	1	0	1	1	0

Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	3	3	2	2
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	2	2	3	3
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	2	2	3	3
HCM Control Delay	15.3	15.6	30.2	22.9
HCM LOS	C	C	D	C

Lane	NBLn1	NBLn2	EBLn1	EBLn2	EBLn3	WBLn1	WBLn2	WBLn3	SBLn1	SBLn2
Vol Left, %	100%	0%	100%	0%	0%	100%	0%	0%	100%	0%
Vol Thru, %	0%	48%	0%	100%	62%	0%	100%	57%	0%	78%
Vol Right, %	0%	52%	0%	0%	38%	0%	0%	43%	0%	22%
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop
Traffic Vol by Lane	31	250	57	101	81	91	103	89	44	200
LT Vol	31	0	57	0	0	91	0	0	44	0
Through Vol	0	119	0	101	50	0	103	51	0	156
RT Vol	0	131	0	0	31	0	0	38	0	44
Lane Flow Rate	42	338	77	136	110	123	139	121	59	270
Geometry Grp	8	8	8	8	8	8	8	8	8	8
Degree of Util (X)	0.104	0.76	0.202	0.337	0.265	0.318	0.337	0.283	0.151	0.636
Departure Headway (Hd)	8.975	8.096	9.438	8.918	8.694	9.3	8.745	8.436	9.139	8.475
Convergence, Y/N	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
Cap	399	446	380	403	413	386	411	426	392	427
Service Time	6.729	5.849	7.201	6.681	6.457	7.059	6.504	6.195	6.898	6.233
HCM Lane V/C Ratio	0.105	0.758	0.203	0.337	0.266	0.319	0.338	0.284	0.151	0.632
HCM Control Delay	12.8	32.4	14.6	16.2	14.6	16.4	15.9	14.5	13.5	25
HCM Lane LOS	B	D	B	C	B	C	C	B	B	C
HCM 95th-tile Q	0.3	6.4	0.7	1.5	1.1	1.3	1.5	1.1	0.5	4.3

Build-Out (2023) Total PM Peak - Alternate 2
2: Oakmont Dr & Ardglass Trail

Intersection						
Int Delay, s/veh	1.5					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T		T		T	
Traffic Vol, veh/h	34	15	24	196	285	28
Future Vol, veh/h	34	15	24	196	285	28
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	79	79	79	79	79	79
Heavy Vehicles, %	4	11	10	2	2	9
Mvmt Flow	43	19	30	248	361	35

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	687	379	396	0	-	0
Stage 1	379	-	-	-	-	-
Stage 2	308	-	-	-	-	-
Critical Hdwy	6.44	6.31	4.2	-	-	-
Critical Hdwy Stg 1	5.44	-	-	-	-	-
Critical Hdwy Stg 2	5.44	-	-	-	-	-
Follow-up Hdwy	3.536	3.399	2.29	-	-	-
Pot Cap-1 Maneuver	410	648	1120	-	-	-
Stage 1	688	-	-	-	-	-
Stage 2	741	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	397	648	1120	-	-	-
Mov Cap-2 Maneuver	397	-	-	-	-	-
Stage 1	667	-	-	-	-	-
Stage 2	741	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	14.3	0.9	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1120	-	450	-	-
HCM Lane V/C Ratio	0.027	-	0.138	-	-
HCM Control Delay (s)	8.3	0	14.3	-	-
HCM Lane LOS	A	A	B	-	-
HCM 95th %tile Q(veh)	0.1	-	0.5	-	-

Build-Out (2023) Total PM Peak - Alternate 2
 3: Oakmont Dr & Creekside Dr

Intersection	
Intersection Delay, s/veh	10.8
Intersection LOS	B

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕	↕	↕	↕	
Traffic Vol, veh/h	6	5	5	80	4	92	9	140	64	76	111	14
Future Vol, veh/h	6	5	5	80	4	92	9	140	64	76	111	14
Peak Hour Factor	0.70	0.70	0.70	0.70	0.70	0.70	0.70	0.70	0.70	0.70	0.70	0.70
Heavy Vehicles, %	2	2	2	7	2	3	2	2	2	3	2	2
Mvmt Flow	9	7	7	114	6	131	13	200	91	109	159	20
Number of Lanes	0	1	0	0	1	0	0	1	1	1	1	0

Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	1	1	2	2
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	2	2	1	1
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	2	2	1	1
HCM Control Delay	9.1	11.5	10.5	10.5
HCM LOS	A	B	B	B

Lane	NBLn1	NBLn2	EBLn1	WBLn1	SBLn1	SBLn2
Vol Left, %	6%	0%	38%	45%	100%	0%
Vol Thru, %	94%	0%	31%	2%	0%	89%
Vol Right, %	0%	100%	31%	52%	0%	11%
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop
Traffic Vol by Lane	149	64	16	176	76	125
LT Vol	9	0	6	80	76	0
Through Vol	140	0	5	4	0	111
RT Vol	0	64	5	92	0	14
Lane Flow Rate	213	91	23	251	109	179
Geometry Grp	7	7	2	2	7	7
Degree of Util (X)	0.339	0.127	0.037	0.373	0.188	0.279
Departure Headway (Hd)	5.73	4.991	5.789	5.34	6.222	5.619
Convergence, Y/N	Yes	Yes	Yes	Yes	Yes	Yes
Cap	628	719	618	679	578	639
Service Time	3.458	2.719	3.831	3.34	3.95	3.348
HCM Lane V/C Ratio	0.339	0.127	0.037	0.37	0.189	0.28
HCM Control Delay	11.4	8.4	9.1	11.5	10.4	10.5
HCM Lane LOS	B	A	A	B	B	B
HCM 95th-tile Q	1.5	0.4	0.1	1.7	0.7	1.1

Build-Out (2023) Total PM Peak - Alternate 2
4: Lake Sharon Dr & West Driveway

Intersection						
Int Delay, s/veh	0.8					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↑↑	↑↑		↑↑	
Traffic Vol, veh/h	22	218	216	30	11	9
Future Vol, veh/h	22	218	216	30	11	9
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	74	74	74	74	74	74
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	30	295	292	41	15	12

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	333	0	-	0	521 167
Stage 1	-	-	-	-	313 -
Stage 2	-	-	-	-	208 -
Critical Hdwy	4.14	-	-	-	6.84 6.94
Critical Hdwy Stg 1	-	-	-	-	5.84 -
Critical Hdwy Stg 2	-	-	-	-	5.84 -
Follow-up Hdwy	2.22	-	-	-	3.52 3.32
Pot Cap-1 Maneuver	1223	-	-	-	485 848
Stage 1	-	-	-	-	715 -
Stage 2	-	-	-	-	807 -
Platoon blocked, %		-	-	-	
Mov Cap-1 Maneuver	1223	-	-	-	471 848
Mov Cap-2 Maneuver	-	-	-	-	471 -
Stage 1	-	-	-	-	694 -
Stage 2	-	-	-	-	807 -

Approach	EB	WB	SB
HCM Control Delay, s	0.8	0	11.4
HCM LOS			B

Minor Lane/Major Mvmt	EBL	EBT	WBT	WBR	SBLn1
Capacity (veh/h)	1223	-	-	-	589
HCM Lane V/C Ratio	0.024	-	-	-	0.046
HCM Control Delay (s)	8	0.1	-	-	11.4
HCM Lane LOS	A	A	-	-	B
HCM 95th %tile Q(veh)	0.1	-	-	-	0.1

Build-Out (2023) Total PM Peak - Alternate 2
5: Lake Sharon Dr & Rye Rd

Intersection						
Int Delay, s/veh	0.6					
Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations		↑↑	↑↑		↑	↑
Traffic Vol, veh/h	0	229	230	0	10	16
Future Vol, veh/h	0	229	230	0	10	16
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	0
Veh in Median Storage, #	-	0	0	-	0	-
Grade, %	-	0	0	-	0	-
Peak Hour Factor	74	74	74	74	74	74
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	309	311	0	14	22

Major/Minor	Major1	Major2	Minor2		
Conflicting Flow All	-	0	-	0	466 156
Stage 1	-	-	-	-	311 -
Stage 2	-	-	-	-	155 -
Critical Hdwy	-	-	-	-	6.84 6.94
Critical Hdwy Stg 1	-	-	-	-	5.84 -
Critical Hdwy Stg 2	-	-	-	-	5.84 -
Follow-up Hdwy	-	-	-	-	3.52 3.32
Pot Cap-1 Maneuver	0	-	-	0	525 862
Stage 1	0	-	-	0	716 -
Stage 2	0	-	-	0	857 -
Platoon blocked, %		-	-		
Mov Cap-1 Maneuver	-	-	-	-	525 862
Mov Cap-2 Maneuver	-	-	-	-	525 -
Stage 1	-	-	-	-	716 -
Stage 2	-	-	-	-	857 -

Approach	EB	WB	SB
HCM Control Delay, s	0	0	10.3
HCM LOS			B

Minor Lane/Major Mvmt	EBT	WBT	SBLn1	SBLn2
Capacity (veh/h)	-	-	525	862
HCM Lane V/C Ratio	-	-	0.026	0.025
HCM Control Delay (s)	-	-	12	9.3
HCM Lane LOS	-	-	B	A
HCM 95th %tile Q(veh)	-	-	0.1	0.1

Build-Out (2023) Total PM Peak - Alternate 2
6: Oakmont Dr & East Driveway

Intersection						
Int Delay, s/veh	0.4					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations	T		T		T	
Traffic Vol, veh/h	4	3	15	216	292	7
Future Vol, veh/h	4	3	15	216	292	7
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	74	74	74	74	74	74
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	5	4	20	292	395	9

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	732	400	404	0	-	0
Stage 1	400	-	-	-	-	-
Stage 2	332	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	388	650	1155	-	-	-
Stage 1	677	-	-	-	-	-
Stage 2	727	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	380	650	1155	-	-	-
Mov Cap-2 Maneuver	380	-	-	-	-	-
Stage 1	663	-	-	-	-	-
Stage 2	727	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	13	0.5	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1155	-	462	-	-
HCM Lane V/C Ratio	0.018	-	0.02	-	-
HCM Control Delay (s)	8.2	0	13	-	-
HCM Lane LOS	A	A	B	-	-
HCM 95th %tile Q(veh)	0.1	-	0.1	-	-

Build-Out Year (2023) Background AM Peak with RT Lane
1: Oakmont Dr & Lake Sharon Dr

Intersection	
Intersection Delay, s/veh	19.7
Intersection LOS	C

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↵	↕↗		↵	↕↗		↵	↕	↗	↵	↗	
Traffic Vol, veh/h	56	129	16	100	97	77	13	194	81	91	208	53
Future Vol, veh/h	56	129	16	100	97	77	13	194	81	91	208	53
Peak Hour Factor	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77
Heavy Vehicles, %	4	2	2	2	2	2	2	2	2	2	3	2
Mvmt Flow	73	168	21	130	126	100	17	252	105	118	270	69
Number of Lanes	1	2	0	1	2	0	1	1	1	1	1	0

Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	3	3	2	3
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	2	3	3	3
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	3	2	3	3
HCM Control Delay	14.4	15	19.4	26.5
HCM LOS	B	B	C	D

Lane	NBLn1	NBLn2	NBLn3	EBLn1	EBLn2	EBLn3	WBLn1	WBLn2	WBLn3	SBLn1	SBLn2
Vol Left, %	100%	0%	0%	100%	0%	0%	100%	0%	0%	100%	0%
Vol Thru, %	0%	100%	0%	0%	100%	73%	0%	100%	30%	0%	80%
Vol Right, %	0%	0%	100%	0%	0%	27%	0%	0%	70%	0%	20%
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop
Traffic Vol by Lane	13	194	81	56	86	59	100	65	109	91	261
LT Vol	13	0	0	56	0	0	100	0	0	91	0
Through Vol	0	194	0	0	86	43	0	65	32	0	208
RT Vol	0	0	81	0	0	16	0	0	77	0	53
Lane Flow Rate	17	252	105	73	112	77	130	84	142	118	339
Geometry Grp	8	8	8	8	8	8	8	8	8	8	8
Degree of Util (X)	0.042	0.591	0.226	0.191	0.276	0.185	0.33	0.201	0.32	0.281	0.747
Departure Headway (Hd)	8.952	8.444	7.734	9.447	8.899	8.704	9.137	8.624	8.119	8.558	7.933
Convergence, Y/N	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
Cap	400	428	464	380	404	412	394	417	443	422	458
Service Time	6.699	6.191	5.48	7.201	6.652	6.457	6.886	6.373	5.868	6.258	5.633
HCM Lane V/C Ratio	0.043	0.589	0.226	0.192	0.277	0.187	0.33	0.201	0.321	0.28	0.74
HCM Control Delay	12.1	22.7	12.7	14.4	15	13.4	16.3	13.5	14.7	14.6	30.6
HCM Lane LOS	B	C	B	B	B	B	C	B	B	B	D
HCM 95th-tile Q	0.1	3.7	0.9	0.7	1.1	0.7	1.4	0.7	1.4	1.1	6.2

Build-Out Year (2023) Background PM Peak with RT Lane
1: Oakmont Dr & Lake Sharon Dr

Intersection	
Intersection Delay, s/veh	15.1
Intersection LOS	C

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↵	↕↗		↵	↕↗		↵	↕	↗	↵	↗	
Traffic Vol, veh/h	51	138	25	91	135	31	24	115	131	42	155	48
Future Vol, veh/h	51	138	25	91	135	31	24	115	131	42	155	48
Peak Hour Factor	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74
Heavy Vehicles, %	2	2	5	4	2	2	2	2	2	3	3	8
Mvmt Flow	69	186	34	123	182	42	32	155	177	57	209	65
Number of Lanes	1	2	0	1	2	0	1	1	1	1	1	0

Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	3	3	2	3
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	2	3	3	3
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	3	2	3	3
HCM Control Delay	13.5	13.8	14.1	19.1
HCM LOS	B	B	B	C

Lane	NBLn1	NBLn2	NBLn3	EBLn1	EBLn2	EBLn3	WBLn1	WBLn2	WBLn3	SBLn1	SBLn2
Vol Left, %	100%	0%	0%	100%	0%	0%	100%	0%	0%	100%	0%
Vol Thru, %	0%	100%	0%	0%	100%	65%	0%	100%	59%	0%	76%
Vol Right, %	0%	0%	100%	0%	0%	35%	0%	0%	41%	0%	24%
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop
Traffic Vol by Lane	24	115	131	51	92	71	91	90	76	42	203
LT Vol	24	0	0	51	0	0	91	0	0	42	0
Through Vol	0	115	0	0	92	46	0	90	45	0	155
RT Vol	0	0	131	0	0	25	0	0	31	0	48
Lane Flow Rate	32	155	177	69	124	96	123	122	103	57	274
Geometry Grp	8	8	8	8	8	8	8	8	8	8	8
Degree of Util (X)	0.076	0.341	0.354	0.166	0.281	0.212	0.292	0.27	0.22	0.131	0.581
Departure Headway (Hd)	8.416	7.91	7.202	8.661	8.15	7.951	8.537	7.993	7.702	8.292	7.627
Convergence, Y/N	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
Cap	424	454	497	413	439	450	419	448	464	431	472
Service Time	6.193	5.687	4.979	6.442	5.931	5.731	6.314	5.77	5.478	6.064	5.398
HCM Lane V/C Ratio	0.075	0.341	0.356	0.167	0.282	0.213	0.294	0.272	0.222	0.132	0.581
HCM Control Delay	11.9	14.8	13.9	13.2	14.1	12.9	14.8	13.7	12.7	12.3	20.5
HCM Lane LOS	B	B	B	B	B	B	B	B	B	B	C
HCM 95th-tile Q	0.2	1.5	1.6	0.6	1.1	0.8	1.2	1.1	0.8	0.4	3.6

Build-Out (2023) Total AM Peak - Existing Site Plan - with RT Lane
 1: Oakmont Dr & Lake Sharon Dr

Intersection	
Intersection Delay, s/veh	20.5
Intersection LOS	C

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↵	↕↔		↵	↕↔		↵	↕	↗	↵	↔	
Traffic Vol, veh/h	56	152	26	100	105	78	16	194	81	95	209	47
Future Vol, veh/h	56	152	26	100	105	78	16	194	81	95	209	47
Peak Hour Factor	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77	0.77
Heavy Vehicles, %	4	2	2	2	2	2	2	2	2	2	3	2
Mvmt Flow	73	197	34	130	136	101	21	252	105	123	271	61
Number of Lanes	1	2	0	1	2	0	1	1	1	1	1	0

Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	3	3	2	3
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	2	3	3	3
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	3	2	3	3
HCM Control Delay	15.3	15.7	20.6	27.6
HCM LOS	C	C	C	D

Lane	NBLn1	NBLn2	NBLn3	EBLn1	EBLn2	EBLn3	WBLn1	WBLn2	WBLn3	SBLn1	SBLn2
Vol Left, %	100%	0%	0%	100%	0%	0%	100%	0%	0%	100%	0%
Vol Thru, %	0%	100%	0%	0%	100%	66%	0%	100%	31%	0%	82%
Vol Right, %	0%	0%	100%	0%	0%	34%	0%	0%	69%	0%	18%
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop
Traffic Vol by Lane	16	194	81	56	101	77	100	70	113	95	256
LT Vol	16	0	0	56	0	0	100	0	0	95	0
Through Vol	0	194	0	0	101	51	0	70	35	0	209
RT Vol	0	0	81	0	0	26	0	0	78	0	47
Lane Flow Rate	21	252	105	73	132	100	130	91	147	123	332
Geometry Grp	8	8	8	8	8	8	8	8	8	8	8
Degree of Util (X)	0.053	0.61	0.234	0.194	0.331	0.244	0.339	0.224	0.342	0.301	0.755
Departure Headway (Hd)	9.228	8.72	8.008	9.605	9.056	8.812	9.387	8.874	8.378	8.781	8.17
Convergence, Y/N	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
Cap	388	413	448	374	397	407	383	405	429	410	442
Service Time	6.984	6.476	5.764	7.361	6.812	6.568	7.14	6.627	6.131	6.529	5.917
HCM Lane V/C Ratio	0.054	0.61	0.234	0.195	0.332	0.246	0.339	0.225	0.343	0.3	0.751
HCM Control Delay	12.5	24.3	13.2	14.7	16.3	14.4	16.9	14.2	15.5	15.3	32.1
HCM Lane LOS	B	C	B	B	C	B	C	B	C	C	D
HCM 95th-tile Q	0.2	3.9	0.9	0.7	1.4	0.9	1.5	0.8	1.5	1.2	6.3

Build-Out (2023) Total PM Peak - Existing Site Plan - with RT Lane
 1: Oakmont Dr & Lake Sharon Dr

Intersection	
Intersection Delay, s/veh	15.8
Intersection LOS	C

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↵	↕↗		↵	↕↗		↵	↕	↗	↵	↗	
Traffic Vol, veh/h	45	151	31	91	158	34	35	115	131	44	156	44
Future Vol, veh/h	45	151	31	91	158	34	35	115	131	44	156	44
Peak Hour Factor	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74	0.74
Heavy Vehicles, %	2	2	5	4	2	2	2	2	2	3	3	8
Mvmt Flow	61	204	42	123	214	46	47	155	177	59	211	59
Number of Lanes	1	2	0	1	2	0	1	1	1	1	1	0

Approach	EB	WB	NB	SB
Opposing Approach	WB	EB	SB	NB
Opposing Lanes	3	3	2	3
Conflicting Approach Left	SB	NB	EB	WB
Conflicting Lanes Left	2	3	3	3
Conflicting Approach Right	NB	SB	WB	EB
Conflicting Lanes Right	3	2	3	3
HCM Control Delay	14.2	14.5	14.6	20
HCM LOS	B	B	B	C

Lane	NBLn1	NBLn2	NBLn3	EBLn1	EBLn2	EBLn3	WBLn1	WBLn2	WBLn3	SBLn1	SBLn2
Vol Left, %	100%	0%	0%	100%	0%	0%	100%	0%	0%	100%	0%
Vol Thru, %	0%	100%	0%	0%	100%	62%	0%	100%	61%	0%	78%
Vol Right, %	0%	0%	100%	0%	0%	38%	0%	0%	39%	0%	22%
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop	Stop
Traffic Vol by Lane	35	115	131	45	101	81	91	105	87	44	200
LT Vol	35	0	0	45	0	0	91	0	0	44	0
Through Vol	0	115	0	0	101	50	0	105	53	0	156
RT Vol	0	0	131	0	0	31	0	0	34	0	44
Lane Flow Rate	47	155	177	61	136	110	123	142	117	59	270
Geometry Grp	8	8	8	8	8	8	8	8	8	8	8
Degree of Util (X)	0.114	0.352	0.366	0.15	0.316	0.249	0.298	0.323	0.257	0.141	0.593
Departure Headway (Hd)	8.65	8.144	7.435	8.883	8.372	8.151	8.715	8.17	7.89	8.548	7.894
Convergence, Y/N	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes	Yes
Cap	412	440	482	402	427	439	411	438	453	418	454
Service Time	6.442	5.935	5.226	6.682	6.171	5.95	6.507	5.962	5.681	6.334	5.68
HCM Lane V/C Ratio	0.114	0.352	0.367	0.152	0.319	0.251	0.299	0.324	0.258	0.141	0.595
HCM Control Delay	12.6	15.3	14.5	13.3	15	13.7	15.2	14.9	13.4	12.7	21.6
HCM Lane LOS	B	C	B	B	B	B	C	B	B	B	C
HCM 95th-tile Q	0.4	1.6	1.7	0.5	1.3	1	1.2	1.4	1	0.5	3.8

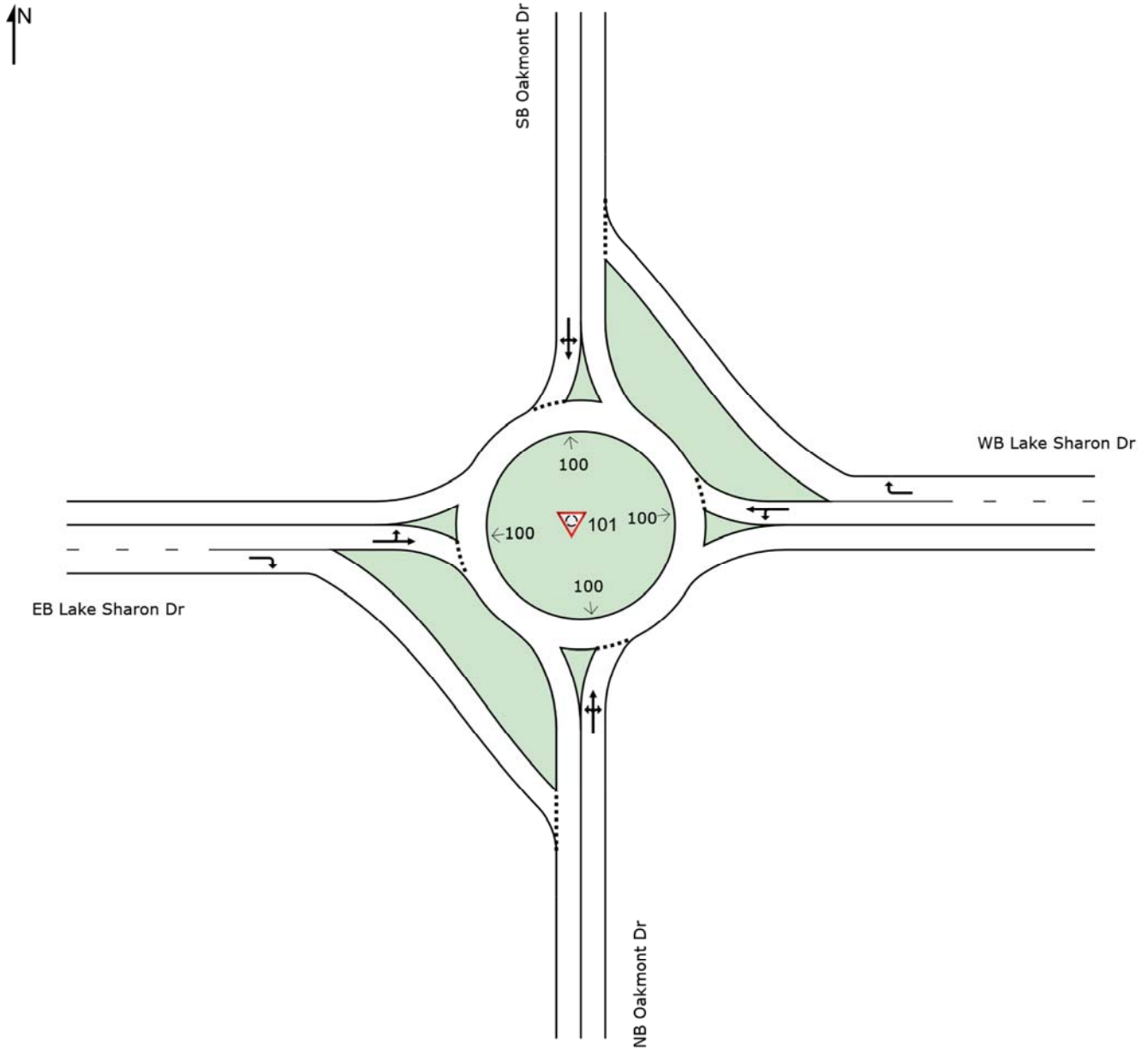
Appendix E:
***SIDRA* Output Sheets**

SITE LAYOUT

Site: 101 [2023 Background AM Peak (Site Folder: General)]

Build-Out (2023) Background AM Peak
Single-Lane Roundabout
Site Category: (None)
Roundabout

Layout pictures are schematic functional drawings reflecting input data. They are not design drawings.



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Project: H:\T1458.06 - Traffic Study, Oakmont at Lake Sharon\Sidra\oakmont at lake sharon.sip9

MOVEMENT SUMMARY

Site: 101 [2023 Background AM Peak (Site Folder: General)]

Build-Out (2023) Background AM Peak
 Single-Lane Roundabout
 Site Category: (None)
 Roundabout

Vehicle Movement Performance														
Mov ID	Turn	INPUT VOLUMES		DEMAND FLOWS		Deg. Satn	Aver. Delay	Level of Service	95% BACK OF QUEUE		Prop. Que	Effective Stop Rate	Aver. No. Cycles	Aver. Speed
		[Total veh/h	HV %	[Total veh/h	HV %				[Veh. veh	Dist] ft				
South: NB Oakmont Dr														
3	L2	13	3.0	17	3.0	0.407	8.6	LOS A	2.2	55.0	0.60	0.53	0.60	33.3
8	T1	194	3.0	252	3.0	0.407	8.6	LOS A	2.2	55.0	0.60	0.53	0.60	33.2
18	R2	81	3.0	105	3.0	0.407	8.6	LOS A	2.2	55.0	0.60	0.53	0.60	32.3
Approach		288	3.0	374	3.0	0.407	8.6	LOS A	2.2	55.0	0.60	0.53	0.60	33.0
East: WB Lake Sharon Dr														
1	L2	100	3.0	130	3.0	0.256	6.1	LOS A	1.1	28.7	0.48	0.39	0.48	33.3
6	T1	97	3.0	126	3.0	0.256	6.1	LOS A	1.1	28.7	0.48	0.39	0.48	33.2
16	R2	77	3.0	100	3.0	0.098	4.4	LOS A	0.4	9.8	0.41	0.30	0.41	34.4
Approach		274	3.0	356	3.0	0.256	5.6	LOS A	1.1	28.7	0.46	0.37	0.46	33.6
North: SB Oakmont Dr														
7	L2	91	3.0	118	3.0	0.458	8.9	LOS A	2.7	68.3	0.57	0.46	0.57	32.6
4	T1	208	3.0	270	3.0	0.458	8.9	LOS A	2.7	68.3	0.57	0.46	0.57	32.5
14	R2	53	8.0	69	8.0	0.458	9.0	LOS A	2.7	68.3	0.57	0.46	0.57	31.5
Approach		352	3.8	457	3.8	0.458	8.9	LOS A	2.7	68.3	0.57	0.46	0.57	32.4
West: EB Lake Sharon Dr														
5	L2	56	4.0	73	4.0	0.284	7.4	LOS A	1.2	30.8	0.57	0.55	0.57	33.2
2	T1	129	3.0	168	3.0	0.284	7.3	LOS A	1.2	30.8	0.57	0.55	0.57	33.2
12	R2	16	3.0	21	3.0	0.022	4.0	LOS A	0.1	2.0	0.43	0.29	0.43	34.7
Approach		201	3.3	261	3.3	0.284	7.1	LOS A	1.2	30.8	0.56	0.53	0.56	33.3
All Vehicles		1115	3.3	1448	3.3	0.458	7.7	LOS A	2.7	68.3	0.55	0.47	0.55	33.0

Site Level of Service (LOS) Method: Delay & v/c (HCM 6). Site LOS Method is specified in the Parameter Settings dialog (Site tab).

Roundabout LOS Method: Same as Sign Control.

Vehicle movement LOS values are based on average delay and v/c ratio (degree of saturation) per movement.

LOS F will result if v/c > 1 irrespective of movement delay value (does not apply for approaches and intersection).

Intersection and Approach LOS values are based on average delay for all movements (v/c not used as specified in HCM 6).

Roundabout Capacity Model: US HCM 6.

Delay Model: HCM Delay Formula (Geometric Delay is not included).

Queue Model: HCM Queue Formula.

Gap-Acceptance Capacity: Traditional M1.

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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MOVEMENT SUMMARY

Site: 101 [2023 Background PM Peak (Site Folder: General)]

Build-Out (2023) Background PM Peak
 Single-Lane Roundabout
 Site Category: (None)
 Roundabout

Vehicle Movement Performance														
Mov ID	Turn	INPUT VOLUMES		DEMAND FLOWS		Deg. Satn	Aver. Delay	Level of Service	95% BACK OF QUEUE		Prop. Que	Effective Stop Rate	Aver. No. Cycles	Aver. Speed
		[Total veh/h	HV %	[Total veh/h	HV %				[Veh. veh	Dist] ft				
South: NB Oakmont Dr														
3	L2	24	3.0	32	3.0	0.378	7.9	LOS A	2.0	51.0	0.55	0.46	0.55	33.5
8	T1	115	3.0	155	3.0	0.378	7.9	LOS A	2.0	51.0	0.55	0.46	0.55	33.4
18	R2	131	3.0	177	3.0	0.378	7.9	LOS A	2.0	51.0	0.55	0.46	0.55	32.5
Approach		270	3.0	365	3.0	0.378	7.9	LOS A	2.0	51.0	0.55	0.46	0.55	33.0
East: WB Lake Sharon Dr														
1	L2	91	4.0	123	4.0	0.283	6.1	LOS A	1.3	33.5	0.43	0.32	0.43	33.6
6	T1	135	3.0	182	3.0	0.283	6.0	LOS A	1.3	33.5	0.43	0.32	0.43	33.5
16	R2	31	3.0	42	3.0	0.037	3.5	LOS A	0.1	3.6	0.33	0.19	0.33	34.9
Approach		257	3.4	347	3.4	0.283	5.8	LOS A	1.3	33.5	0.42	0.31	0.42	33.7
North: SB Oakmont Dr														
7	L2	42	3.0	57	3.0	0.356	7.7	LOS A	1.8	46.3	0.56	0.48	0.56	33.3
4	T1	155	3.0	209	3.0	0.356	7.7	LOS A	1.8	46.3	0.56	0.48	0.56	33.3
14	R2	48	8.0	65	8.0	0.356	7.9	LOS A	1.8	46.3	0.56	0.48	0.56	32.2
Approach		245	4.0	331	4.0	0.356	7.8	LOS A	1.8	46.3	0.56	0.48	0.56	33.1
West: EB Lake Sharon Dr														
5	L2	51	3.0	69	3.0	0.267	6.5	LOS A	1.2	29.8	0.51	0.44	0.51	33.8
2	T1	138	3.0	186	3.0	0.267	6.5	LOS A	1.2	29.8	0.51	0.44	0.51	33.7
12	R2	25	5.0	34	5.0	0.034	3.9	LOS A	0.1	3.2	0.40	0.26	0.40	34.6
Approach		214	3.2	289	3.2	0.267	6.2	LOS A	1.2	29.8	0.50	0.42	0.50	33.8
All Vehicles		986	3.4	1332	3.4	0.378	6.9	LOS A	2.0	51.0	0.51	0.42	0.51	33.4

Site Level of Service (LOS) Method: Delay & v/c (HCM 6). Site LOS Method is specified in the Parameter Settings dialog (Site tab).

Roundabout LOS Method: Same as Sign Control.

Vehicle movement LOS values are based on average delay and v/c ratio (degree of saturation) per movement.

LOS F will result if $v/c > 1$ irrespective of movement delay value (does not apply for approaches and intersection).

Intersection and Approach LOS values are based on average delay for all movements (v/c not used as specified in HCM 6).

Roundabout Capacity Model: US HCM 6.

Delay Model: HCM Delay Formula (Geometric Delay is not included).

Queue Model: HCM Queue Formula.

Gap-Acceptance Capacity: Traditional M1.

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

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MOVEMENT SUMMARY

Site: 101 [2023 Total AM Peak (Site Folder: General)]

Build-Out (2023) Total AM Peak
 Existing Site Plan Volumes
 Single-Lane Roundabout
 Site Category: (None)
 Roundabout

Vehicle Movement Performance														
Mov ID	Turn	INPUT VOLUMES		DEMAND FLOWS		Deg. Satn v/c	Aver. Delay sec	Level of Service	95% BACK OF QUEUE		Prop. Que	Effective Stop Rate	Aver. No. Cycles	Aver. Speed mph
		[Total veh/h]	[HV %]	[Total veh/h]	[HV %]				[Veh. veh]	[Dist ft]				
South: NB Oakmont Dr														
3	L2	16	3.0	21	3.0	0.427	9.2	LOS A	2.4	61.1	0.63	0.59	0.67	33.0
8	T1	194	3.0	252	3.0	0.427	9.2	LOS A	2.4	61.1	0.63	0.59	0.67	32.9
18	R2	81	3.0	105	3.0	0.427	9.2	LOS A	2.4	61.1	0.63	0.59	0.67	32.0
Approach		291	3.0	378	3.0	0.427	9.2	LOS A	2.4	61.1	0.63	0.59	0.67	32.7
East: WB Lake Sharon Dr														
1	L2	100	3.0	130	3.0	0.267	6.3	LOS A	1.2	30.3	0.48	0.40	0.48	33.3
6	T1	105	3.0	136	3.0	0.267	6.3	LOS A	1.2	30.3	0.48	0.40	0.48	33.2
16	R2	78	3.0	101	3.0	0.100	4.4	LOS A	0.4	9.9	0.41	0.30	0.41	34.4
Approach		283	3.0	368	3.0	0.267	5.8	LOS A	1.2	30.3	0.46	0.37	0.46	33.5
North: SB Oakmont Dr														
7	L2	95	3.0	123	3.0	0.463	9.1	LOS A	2.7	68.9	0.58	0.48	0.58	32.5
4	T1	209	3.0	271	3.0	0.463	9.1	LOS A	2.7	68.9	0.58	0.48	0.58	32.4
14	R2	47	8.0	61	8.0	0.463	9.2	LOS A	2.7	68.9	0.58	0.48	0.58	31.4
Approach		351	3.7	456	3.7	0.463	9.1	LOS A	2.7	68.9	0.58	0.48	0.58	32.3
West: EB Lake Sharon Dr														
5	L2	56	4.0	73	4.0	0.321	7.9	LOS A	1.4	35.7	0.59	0.57	0.59	33.0
2	T1	152	3.0	197	3.0	0.321	7.9	LOS A	1.4	35.7	0.59	0.57	0.59	33.0
12	R2	26	3.0	34	3.0	0.036	4.1	LOS A	0.1	3.3	0.43	0.31	0.43	34.6
Approach		234	3.2	304	3.2	0.321	7.5	LOS A	1.4	35.7	0.57	0.54	0.57	33.2
All Vehicles		1159	3.3	1505	3.3	0.463	8.0	LOS A	2.7	68.9	0.56	0.50	0.57	32.9

Site Level of Service (LOS) Method: Delay & v/c (HCM 6). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
 Roundabout LOS Method: Same as Sign Control.
 Vehicle movement LOS values are based on average delay and v/c ratio (degree of saturation) per movement.
 LOS F will result if v/c > 1 irrespective of movement delay value (does not apply for approaches and intersection).
 Intersection and Approach LOS values are based on average delay for all movements (v/c not used as specified in HCM 6).
 Roundabout Capacity Model: US HCM 6.
 Delay Model: HCM Delay Formula (Geometric Delay is not included).
 Queue Model: HCM Queue Formula.
 Gap-Acceptance Capacity: Traditional M1.
 HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

MOVEMENT SUMMARY

Site: 101 [2023 Total PM Peak (Site Folder: General)]

Build-Out (2023) Total PM Peak
 Existing Site Plan Volumes
 Single-Lane Roundabout
 Site Category: (None)
 Roundabout

Vehicle Movement Performance														
Mov ID	Turn	INPUT VOLUMES		DEMAND FLOWS		Deg. Satn v/c	Aver. Delay sec	Level of Service	95% BACK OF QUEUE		Prop. Que	Effective Stop Rate	Aver. No. Cycles	Aver. Speed mph
		[Total veh/h]	[HV %]	[Total veh/h]	[HV %]				[Veh. veh]	[Dist ft]				
South: NB Oakmont Dr														
3	L2	35	3.0	47	3.0	0.398	8.3	LOS A	2.1	54.5	0.57	0.49	0.57	33.2
8	T1	115	3.0	155	3.0	0.398	8.3	LOS A	2.1	54.5	0.57	0.49	0.57	33.2
18	R2	131	3.0	177	3.0	0.398	8.3	LOS A	2.1	54.5	0.57	0.49	0.57	32.2
Approach		281	3.0	380	3.0	0.398	8.3	LOS A	2.1	54.5	0.57	0.49	0.57	32.7
East: WB Lake Sharon Dr														
1	L2	91	4.0	123	4.0	0.314	6.5	LOS A	1.5	38.2	0.45	0.34	0.45	33.5
6	T1	158	3.0	214	3.0	0.314	6.4	LOS A	1.5	38.2	0.45	0.34	0.45	33.4
16	R2	34	3.0	46	3.0	0.041	3.5	LOS A	0.2	3.9	0.32	0.18	0.32	34.9
Approach		283	3.3	382	3.3	0.314	6.1	LOS A	1.5	38.2	0.43	0.32	0.43	33.6
North: SB Oakmont Dr														
7	L2	44	3.0	59	3.0	0.372	8.3	LOS A	1.9	47.9	0.59	0.53	0.59	33.1
4	T1	156	3.0	211	3.0	0.372	8.3	LOS A	1.9	47.9	0.59	0.53	0.59	33.0
14	R2	44	8.0	59	8.0	0.372	8.5	LOS A	1.9	47.9	0.59	0.53	0.59	32.0
Approach		244	3.9	330	3.9	0.372	8.3	LOS A	1.9	47.9	0.59	0.53	0.59	32.8
West: EB Lake Sharon Dr														
5	L2	45	3.0	61	3.0	0.278	6.6	LOS A	1.2	31.3	0.52	0.45	0.52	33.8
2	T1	151	3.0	204	3.0	0.278	6.6	LOS A	1.2	31.3	0.52	0.45	0.52	33.7
12	R2	31	5.0	42	5.0	0.042	4.0	LOS A	0.2	4.0	0.40	0.27	0.40	34.6
Approach		227	3.3	307	3.3	0.278	6.3	LOS A	1.2	31.3	0.50	0.42	0.50	33.9
All Vehicles		1035	3.4	1399	3.4	0.398	7.2	LOS A	2.1	54.5	0.52	0.44	0.52	33.2

Site Level of Service (LOS) Method: Delay & v/c (HCM 6). Site LOS Method is specified in the Parameter Settings dialog (Site tab).
 Roundabout LOS Method: Same as Sign Control.
 Vehicle movement LOS values are based on average delay and v/c ratio (degree of saturation) per movement.
 LOS F will result if v/c > 1 irrespective of movement delay value (does not apply for approaches and intersection).
 Intersection and Approach LOS values are based on average delay for all movements (v/c not used as specified in HCM 6).
 Roundabout Capacity Model: US HCM 6.
 Delay Model: HCM Delay Formula (Geometric Delay is not included).
 Queue Model: HCM Queue Formula.
 Gap-Acceptance Capacity: Traditional M1.
 HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Appendix F: Delay Study



Intersection Delay Study

Field Sheet

(arranged for 15-second time intervals)

Location: Corinth, TX - Oakmont Drive at Ardglass Trail Approach: Eastbound Movement: EBL & EBR
 Date: 04/22/21 Weather: Overcast Study No.: _____ Observer: Curtis Hefner

Time (minute starting at)	Total Number of Vehicles Stopped in the Approach at Time:				Approach Volume	
	+0 sec.	+ 15 sec.	+ 30 sec.	+ 45 sec.	Number Stopped	Number Not Stopping
7:30 AM	0	0	0	0	0	
7:31	0	0	0	0	3	
7:32	1	1	0	0	1	
7:33	1	1	0	0	2	
7:34	1	0	0	0	1	
7:35	0	0	1	0	1	
7:36	0	2	1	0	2	
7:37	0	0	0	0	0	
7:38	0	0	0	0	1	
7:39	1	0	1	0	1	
7:40	0	1	1	0	1	
7:41	0	2	3	3	3	
7:42	2	0	0	0	1	
7:43	0	0	0	0	1	
7:44	0	0	1	0	1	
Subtotal:	6	7	8	3	19	
Total:	24				19	

Total Delay = Total Number Stopped x Sampling Interval

$$= \underline{24} \times 15 = \underline{360} \text{ veh-sec}$$

Average Delay per Stopped Vehicle = $\frac{\text{Total Delay}}{\text{Number of Stopped Vehicles}}$

$$= \underline{360 / 19} = \underline{18.95} \text{ sec.}$$

Average Delay per Approach Vehicle = $\frac{\text{Total Delay}}{\text{Approach Volume}}$

$$= \underline{360 / 19} = \underline{18.95} \text{ sec.}$$

Percent of Vehicles Stopped = $\frac{\text{Number of Stopped Vehicles}}{\text{Approach Volume}} = \underline{19 / 19} = \underline{100} \text{ percent}$



Intersection Delay Study

Field Sheet

(arranged for 15-second time intervals)

Location: Corinth, TX - Oakmont Drive at Ardglass Trail Approach: Eastbound Movement: EBL & EBR
 Date: 04/22/21 Weather: Overcast Study No.: _____ Observer: Curtis Hefner

Time (minute starting at)	Total Number of Vehicles Stopped in the Approach at Time:				Approach Volume	
	+0 sec.	+ 15 sec.	+ 30 sec.	+ 45 sec.	Number Stopped	Number Not Stopping
3:45 pm	0	0	0	1	1	
3:46	1	2	0	1	2	
3:47	0	0	0	0	1	
3:48	1	1	1	2	2	
3:49	2	0	0	1	1	
3:50	0	0	1	0	1	
3:51	0	0	0	0	0	
3:52	0	0	0	0	0	
3:53	0	0	0	0	0	
3:54	0	0	0	1	1	
3:55	0	0	1	0	1	
3:56	0	0	0	0	0	
3:57	0	0	0	0	0	
3:58	0	0	0	0	0	
3:59	0	0	0	0	0	
Subtotal:	4	3	3	6	10	
Total:	16				10	

Total Delay = Total Number Stopped x Sampling Interval

= 16 x 15 = 240 veh-sec

Average Delay per Stopped Vehicle = $\frac{\text{Total Delay}}{\text{Number of Stopped Vehicles}}$

= $\frac{240}{10}$ = 24 sec.

Average Delay per Approach Vehicle = $\frac{\text{Total Delay}}{\text{Approach Volume}}$

= $\frac{240}{10}$ = 24 sec.

Percent of Vehicles Stopped = $\frac{\text{Number of Stopped Vehicles}}{\text{Approach Volume}} = \frac{10}{10} \times 100 = \underline{100}$ percent

APPENDIX C

LETTERS

Miguel Inclan

From: Cheryl Small
Sent: Tuesday, June 22, 2021 7:34 PM
To: Miguel Inclan
Subject: Casting a vote for the rezoning at Oakmont & Lake Sharon

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

My name is Cheryl Small and my husband is Shawn Small. We live at 2702 Navajo Road Corinth, TX 76210.

We want to vote NO for the planned rezoning at Lake Sharon and Oakmont Drive.

It seems, by what we've read, this is not a multi-family addition, but it's a mixed residential area.

The plans for the roundabout at Oakmont Drive will be dangerous to the amount of students that walk home, especially the elementary students from Hawk Elementary School. Roundabouts are constant moving cars and we feel extremely dangerous for students.

What about the inconsistent overall existing PD zoning ordinances for the Oakmont Country Club? I know there were issues when Lake Sharon was going in concerning this.

Lastly, the amount of traffic and how it's being directed is ridiculous in the new plans. You will be setting us up for immense congestion and back up. The influx of traffic and noise that we are dealing with just on Lake Sharon with the current neighborhoods has been a huge adjustment.

The speed limit isn't followed well, people fishing there at the lake and parking on our street and the foot traffic have all increased greatly without the new zoning development.

There has got to be a better option than squeezing in a bunch of homes in such a small area that is not conducive to the area.

Thank you for considering our vote.

Shawn & Cheryl Small

Cheryl Small
469.569.1079

Miguel Inclan

From: Totiro Clark
Sent: Tuesday, June 22, 2021 9:00 PM
To: Miguel Inclan
Cc: Nancy Gegbe
Subject: Objection to Proposed Rezoning - Avilla Fairways Proposal

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

To Mr. Miguel Inclan, City Planner,

My wife and I would like to voice our unequivocal objection to the rezoning of the parcel of land that is currently planned for development by NextMetro on the corner of Oakmont Drive and Lake Sharon Drive. Our house is within the 200 feet limit, as we are the first house in the Larkspur subdivision along Ardglass. We chose to move to this neighborhood because we believed it to be a good place to lay down roots and to continue to grow and nurture our family. The area in the adjoining tract of land is currently zoned to allow for townhomes and two-family garden homes. Changing the Zoning requirements is absolutely unnecessary.

It is my understanding that with the implementation of Avilla Fairways and the additional population it will bring, the area traffic rating will be an F according to a recent analysis. We have two schools within a mile of our location and the proposed new neighborhood. The schools have a combined student population of over 1,400 students and could not withstand the additional influx of students, nor could the area deal with the additional traffic that the Avilla Fairways development would bring. This is not even mentioning the danger posed to the students who walk to BOTH schools daily.

The proposed rezoning to identify the land as mixed residential, not multi-family, is inconsistent with the overall existing PD zoning ordinances for Oakmont Country Club Estates. The proposal would also allow/encourage a potentially dangerous and completely unnecessary addition to what my wife and I felt was an idyllic location to settle down.

The increased traffic, population density that would result from the change, and type of property proposed 200 feet from ours would also make our property much less desirable and valuable. My wife and I have done research on the other neighborhoods developed by this company and have identified that these specific types of developments have an extremely negative impact on the areas in which they are placed. If all sides of the proposed development are single family homes, then why would this type of development be placed here and not additional single family homes? This would be an unacceptable addition to the neighborhood for all the above mentioned reasons, and my wife and I OBJECT to the proposal.

--
Very Respectfully,

Totiro and Nancy Clark
321.323.9683



Planning and Zoning Commission **City Hall and Video Conference Meeting**

Date: **MONDAY, JUNE 28, 2021 AT 6:30 P.M.**

City Council Regular **City Hall and Video Conference Meeting**

Date: **THURSDAY, JULY 15, 2021 AT 7:00 P.M.**

Hearings Location: City Hall, 3300 Corinth Parkway, Corinth, TX 76208 and remotely at <https://www.cityofcorinth.com/remotesession>

Dear Property Owner:

On **Monday, June 28, 2021, at 6:30 PM**, the City of Corinth Planning and Zoning Commission will hold a public hearing to consider testimony and make a recommendation to the Corinth City Council on the item listed below, and on **Thursday, July 15, 2021, at 7:00 PM**, the Corinth City Council will hold a public hearing to consider testimony and consider the approval of an Ordinance regarding the item listed below:

A rezoning request to amend the zoning classification from PD-6 Planned Development District, Ordinance No. 87-12-17-24, for Two Family Garden Homes, Townhomes, and Neighborhood Shopping and PD 24 Planned Development District, Ordinance No. 99-12-16-45 for Two Family Garden Homes to a Planned Development District with a base zoning district of MF-1 Multi-Family Residential, on approximately ±24.595 acres of land within the A.H. Serren Survey, Abstract No. 1198 and the B. Merchant Survey, Abstract No. 800, City of Corinth, Denton County, Texas. **The property is generally located at the northwest corner of Lake Sharon Drive and Oakmont Drive and east of FM 2499. (Avilla Fairways PD ZAPD20-0004)**

As a property owner within two hundred (200) feet of this property, you are invited to attend this meeting, either in person or through video conference, and voice your opinion at the public hearing. You are not required to be present, but all interested parties wanting to be heard should participate through the time and methods stated above.

For your information, the latest version of the Planned Development Concept Plan for the subject property is enclosed with this letter. Please note that the concept plan is subject to change.

Additionally, your opinion regarding the request on the property described above may be expressed by notation on this form or by letter. You may support or oppose this request; your opposition will be considered a protest. Written comments must be received by the **City of Corinth Planning and Development Department at 3300 Corinth Parkway, Corinth, Texas 76208 (protests must be received 3 days prior to public hearings)**. Comments may also be sent by email to **Miguel Inclan, Planner, at miguel.inclan@cityofcorinth.com**. Additionally, if you have any questions regarding the proposed item you may call **940-498-3263** for assistance.

I am writing in (Check as applicable) Support: Opposition: of the proposal. **OPPOSED**

Name/Address/City: (Please Print) (Required)
DOOTY, CHRISTOPHER & KIMBERLEY
2405 GLENHAVEN DR
CORINTH, TX 76210
2405 Glenhaven Dr Corinth, TX, 76210

REC'D JUN 24 2021

In accordance with Section 418.016 of the Texas Government Code, the Corinth Planning and Zoning Commission and the City Council may participate in this meeting remotely in compliance with the Texas Open Meetings Act and under the provisions provided by the Governor of Texas in conjunction with the Declaration of Disaster enacted March 13, 2020 and as amended on March 24, 2020.

Miguel Inclan

From: Chip Lucas
Sent: Tuesday, June 22, 2021 5:19 PM
To: Miguel Inclan
Cc: Larkspur
Subject: Objection to Proposed Rezoning - Endeavor Tract

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

To: Miguel Inclan, City Planner

Dear Mr. Inclan:

We would like to voice our **STRONG** objection to the rezoning of the Endeavor tract at the NW corner of Lake Sharon Drive and Oakmont Drive. Our house directly backs up to the fairway that borders this tract and is only one house down from Rye Rd., so is well within the 200 feet limit area most affected.

When my wife and I moved to the Oakmont Country Club Estates Larkspur neighborhood, this adjoining tract was included in the existing PD zoning ordinances, and so changing it now would be inconsistent at the very least. The tract shows as "Mixed Residential", so changing it to multi-family does not meet the city's own definition of Mixed Residential, nor does it comply with the city's Comprehensive Plan. The existing zoning already allows for townhomes and two-family garden homes, which are not much different than the smaller units being proposed, so there is **NO NEED** to change the zoning.

The current infrastructure (schools, streets, utilities) is **NOT** designed to support the increased population density and parking requirements, and the proposed roundabout at Oakmont Dr. and Lake Sharon Dr. will make the area **EXTREMELY** dangerous for the many school-aged children who walk to Hawk Elementary and Crownover Middle Schools. Oakmont Dr. is already problematic during school drop-off and pick up hours, and adding this many more students will make the situation untenable. Also, we can see **NO USEFUL PURPOSE** for extending Rye Rd. from Larkspur into the proposed subdivision, even if it were for 'emergency access only'. In addition to the fact that we do not believe that this emergency access can or will be enforced, it will make it that much more dangerous for golfers, as now they will have to cross a street for two consecutive holes. Certainly we do not need the tremendous amount of overflow traffic that will result from this development spilling into Larkspur, which is not designed for it at all. We also understand that the current traffic study is incomplete, so any decision to change the zoning would be made with incomplete information at best.

The increased population density, increased parking requirements, increased traffic, and the type of property proposed **DIRECTLY** across from us will serve to make our own property much less desirable as well as less valuable. I have seen many examples of developments such as the one proposed, and as the target renters have much less investment in the property, it will soon have very negative impact on the entire area. We really do not understand why similar type housing to those located on **BOTH SIDES** of Lake Sharon Dr. could not be an option, at least where it directly adjoins Larkspur.

Sincerely,

Edgar C. (Chip) and Suzanne Lucas
1308 Ballycastle Ln.
Corinth, TX 76210



Planning and Zoning Commission **City Hall and Video Conference Meeting**

Date: **MONDAY, JUNE 28, 2021 AT 6:30 P.M.**

City Council Regular **City Hall and Video Conference Meeting**

Date: **THURSDAY, JULY 15, 2021 AT 7:00 P.M.**

Hearings Location: City Hall, 3300 Corinth Parkway, Corinth, TX 76208 and remotely at <https://www.cityofcorinth.com/remotesession>

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For your information, the latest version of the Planned Development Concept Plan for the subject property is enclosed with this letter. Please note that the concept plan is subject to change.

Additionally, your opinion regarding the request on the property described above may be expressed by notation on this form or by letter. You may support or oppose this request; your opposition will be considered a protest. Written comments must be received by the City of Corinth Planning and Development Department at 3300 Corinth Parkway, Corinth, Texas 76208 (protests must be received 3 days prior to public hearings). Comments may also be sent by email to Miguel Inclan, Planner, at miguel.inclan@cityofcorinth.com. Additionally, if you have any questions regarding the proposed item you may call 940-498-3263 for assistance.

I am writing in (Check as applicable) Support: Opposition: of the proposal.

We oppose for the following reasons: ① traffic safety issues and related risk to school children ② Quality and type of development is inconsistent with current neighborhoods ③ increased noise and pedestrian activity creating unwanted disturbance due to high density of development and proposed dog park ④ inconsistent with overall existing zoning ordinances for Oakmont Country Club Estates.

Name/Address/City: (Please Print) (Required)

Alfred + Teresa Goughan 1400 Ballycastle Ln CORINTH

In accordance with Section 418.016 of the Texas Government Code, the Corinth Planning and Zoning Commission and the City Council may participate in this meeting remotely in compliance with the Texas Open Meetings Act and under the provisions provided by the Governor of Texas in conjunction with the Declaration of Disaster enacted March 13, 2020 and as amended on March 24, 2020.

June 22, 2021

OBJECTION to Proposed Rezoning

City of Corinth Planning and Development Department,

As shared in February 2021, we would like to reiterate our STRONG opposition to the proposed rezoning on the corner of Lake Sharon Drive and Oakmont Drive. We just purchased our home in December 2020, which is located on the corner of Ballycastle Lane and Rye Road. This rezoning will have a significant negative impact directly on our property and livelihood, as well as that of our community.

Over the past several months, our community has expressed various concerns with this proposed development. In February 2021, the P&Z Commission voted against this proposal due to our community's widespread opposition and concerns.

The proposal does not comply with the City's Comprehensive Plan, which designates this land as mixed residential, not multi-family. Changing it to multi-family does not meet the city's own definition of mixed residential. The existing zoning already allows for townhomes and two-family garden homes, which are not much different than the smaller units proposed by the developer, so there is no need to change the zoning.

The city's current infrastructure is not designed to support the large increase in population in an already dense area, further congesting our schools, roadways, parking, and traffic. Adding 471+ additional daily travelers in this highly populated community creates significant concerns for children who walk to and from school unsupervised. Oakmont Drive is already overly congested during drop-off and pick-up hours and adding more students/traffic to this mix will make it even more problematic. Hawk, Crownover, and Guyer are highly desired schools and adding additional students from a high-density community will cause further capacity constraints.

The connection of Rye Road is also a huge concern, as there is no valid reason to do so. The proposed property already has three different entryways/exits, so justifying it as an 'emergency access only' does not make sense and cannot be reasonably enforced without residential burden. The Larkspur community has only had one entryway/exit since initial development and was maintained as such during expansion, so why would this community need four? Connecting two separately owned developments will cause excessive traffic overflow within Larkspur (which is ultimately shifting the problem from a city street to a residential street) and create further safety concerns for our children.

The proposal to build rental housing on an island surrounded by middle to upper income properties will obviously have a significant negative impact on our property value and others within our community. Since target renters have little to no investment in their property, this will quickly have a widespread negative impact on surrounding neighborhoods. We have also seen many examples where property management turnover is inevitable, which leads to diminished maintenance, lighter restrictions on renters, and concern for increased crime. This is a large concern for us, especially due to the proximity to Hawk Elementary and Crownover Middle School, which our children attend.

We have been residents of Oakmont/Corinth for over 15 years and have watched this area rapidly flourish, especially the neighborhoods surrounding Hawk Elementary and Crownover Middle School. Our

previous Oakmont residence was more heavily trafficked as time went on, so we recently chose this home for its secluded, peaceful nature. One thing that we have always loved about Corinth is its proximity to larger cities, but it has also maintained the rural, small-town atmosphere with a healthy balance regarding development. Over-developing this area will make it less desirable to live, especially with long-standing residents who have invested their livelihood here.

We ask that you hear and respect the concerns of your community. We believe there are more suitable areas for a development of this nature.

Respectfully,

David and Brittani Graham
1310 Ballycastle Lane
Corinth, TX 76210

Miguel Inclan

From: Tina Zamora
Sent: Thursday, June 24, 2021 1:40 PM
To: Miguel Inclan
Cc: Jennifer Olive; Cody Gober; brian.rush@boards.cityofcorinth; Wade May; Rodney Thornton; Billy Roussel; Rob zamora; Helen-Eve Beadle
Subject: Avila Fairways PD ZAPD20-0004

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Roberto & Tina Zamora
2700 Navajo Rd

We are in Opposition of the Avila Fairways, Lake Sharon @ Oakmont proposal.

Aside from our home being in the buffer zone (unacceptable) we have issues with the proximity to schools and the students safety with the increase of traffic, and the density of this proposal is not conducive to this area.

Regards
Roberto & Tina

Sent from my iPad



Planning and Zoning Commission **City Hall and Video Conference** Meeting
Date: **MONDAY, JUNE 28, 2021 AT 6:30 P.M.**
City Council Regular **City Hall and Video Conference** Meeting
Date: **THURSDAY, JULY 15, 2021 AT 7:00 P.M.**

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I am writing in (Check as applicable) Support: Opposition: of the proposal.

I have voiced my strong opposition to this proposal previously. Current residents moved to this area aware of the current zoning laws and we see NO NEED to change the zoning. The current infrastructure will not support the increased population density and tearing up an intersection which was just completed is wasteful (Oakmont Dr. and Lake Sharon)

Name/Address/City: (Please Print) **(Required)**

Edgar C. Lucas, Jr. 1308 Ballycastle Ln. Corinth, TX 76210

In accordance with Section 418.016 of the Texas Government Code, the Corinth Planning and Zoning Commission and the City Council may participate in this meeting remotely in compliance with the Texas Open Meetings Act and under the provisions provided by the Governor of Texas in conjunction with the Declaration of Disaster enacted March 13, 2020 and as amended on March 24, 2020.

Miguel Inclan

From: Alan Nelson < >
Sent: Wednesday, July 7, 2021 8:53 AM
To: Bill Heidemann; Sam Burke; Scott Garber; Steve Holzwarth; Tina Henderson; Kelly Pickens
Cc: Bob Hart; Helen-Eve Beadle
Subject: NexMetro development

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Members of the City Council,

You have a very serious issue to consider regarding the referenced development proposed along the Oakmont/Lake Sharon tract of which I am confident you are well aware. I am very concerned about the development that NexMetro is proposing to build along the south edge of the Country Club and Oakmont POA boundary and the negative impact it will have on the many fine features of the Oakmont community.

As a resident in Corinth, a member of Oakmont Country Club, and president of the Oakmont Property Owners Association I take great pride in the quality of life and the strong property values we have in Oakmont. I understand the need for growth as the population increases, and the demand for additional housing pressure becomes greater. However, this proposed development may appear on the surface to be a very good fit for this tract of land, but as a retired architect I have the experience and skill to view a proposed site plan such as this one and understand both the pro's and con's.

I'm sure that each of you have been inundated with emails from Oakmont and Lake Sharon home owners and residents objecting to this development. I do understand that the zoning of this particular tract by right allows multi-family type of construction, however I would ask that any consideration for approval be done so with conditions. Should this development move forward as proposed, sadly this will become a black eye in the community that cannot be corrected. It will forever create a neighborhood that is out of character with the high standards that are found in the Oakmont area, and will certainly lower the quality of life and have a negative impact on property values in the Oakmont/Lake Sharon area.

As I mentioned above, there are features of the overall site plan and the layout of the individual rental units that should be seriously reconsidered. Hopefully some adjustments to what is proposed can be negotiated. Below is a brief list of items and suggestions for change that would help improve the overall development, assuming that it is going to be allowed to go forward in some form or another.

1. Rye Road connection: if this road extension is necessary it should be with a "crash gate" to avoid increased traffic into the Larkspur subdivision.
2. Oakmont east side street approach: Terminating a street into the middle of Oakmont Drive is a poor solution. This is too close to the school, and access will create safety issues with children and parents queuing during pick up and drop off periods> Solution: Convert this to a cul de sac with a crash gate. Keep the internal circulation of this development exiting onto Lake Sharon only.
3. Buffering: Require a landscape buffer of at least 15' between the adjoining property (Country Club) along the fairways, and require that the natural trees be maintained to help secure this buffer.
4. Terracing: This site will be very difficult to grade due to the amount of overall fall (grades) and drainage considerations. The developer will be forced to clear cut the entire site to allow terracing of the lots. This will

require many grades changes and retaining walls. Many of these retaining walls will have to be placed between the rental units. The proposed separation between each unit will require very narrow yards with difficult grade features. Solution: reduce the density and allow for more gradual and less severe terracing.

5. Detention ponds: This development will probably gross out to at least 60% or greater lot coverage. The terracing and narrow spacing between the rental units will not allow much ground absorption. Collecting storm water into the streets will accelerate the storm runoff velocity and the west end of the site will be very prone to flooding in high rainfall periods. We were told in the neighborhood meeting that they did not anticipate installing any in-ground storm drainage system. Solution: have the Corinth City Engineer carefully review all engineering calculations and drainage designs.
6. On street, surface parking: Their concept of primarily providing only surface parking with only a few covered spaces will render this development to look more like a modular home (trailer park) community; not an upscale residential community as they want us to believe. The overall quantity of parking proposed is inadequate for the typical lifestyle and community. Solution: require a higher parking to unit ratio.
7. Overall density: the average density of approx. 9 units per acre is more than twice the average density of any of the adjoining neighborhoods. The developer uses this as an explanation and justification for profitability. Suggestion: require the layout to provide for less density or consider multi-story units along the Lake Sharon (southern) buffer to achieve a lower overall lot coverage. (consider the tight spacing of the houses in The Greens; the spacing between each of these are much greater than the proposed spacing of the rental units.
8. Lack of common space: The only significant common spaces are in areas that were essentially unusable for building. Side yards appear to be approx. 4' wide and rear yards (back yards) are only 8' deep, surrounded by a 6' wood privacy fence. These small yards will have very little ability to receive adequate light or ventilation to sustain any landscape growth and will be inhabitable in hot weather. Suggestion: Provide greater side yard and rear yard minimum dimensions.
9. Lack of creativity; adjacency (redundancy) of layouts: the basic layout of the rental units in long rows in the back of the site (along the golf course primarily) and in small groups will resemble high density row housing and modular home communities. Suggestion: break up the long rows with more common spaces.
10. Hardi-siding and building materials: All of these rental units will be 100% hardi-siding (cementitious board). This over use of material will render these rental units to be completely out of character with the adjacent neighborhoods. This would be in direct conflict with the CCR's of the subdivisions in the Oakmont POA and Lake Sharon. Cement board is a preferred exterior materials in harsh environmental conditions, such as along the coast, but in Texas this material is primarily used in lower income, modular style housing communities and sparingly on rear elevations of some residences. Solution: mandate that the developer provide a minimum of 75% or higher masonry on all units and 100% on units that are exposed to the golf course.
11. Too traditional appearance: The overall designs of these units do not meet the general design standards of the residences in either Larkspur or Lake Sharon sub divisions.

(As a side bar, that this tract has a multi-family zoning is a very unfortunate. I cannot understand the thought process that lead the P&Z to believe that this Oakmont/Lake Sharon tract would in anyway complement the high value single family neighborhoods that surround it. This zoning from the beginning has been a mistake and is a dis-service to Corinth. It's unfortunate that it has never been revised or updated prior to this time.)

I understand the City's position of not being able to give a blanket denial to this project for legal reasons. I can also appreciate the economic impact that this could have for the City. I do however strongly encourage each of you to consider the impact of this development as proposed and leverage as many of these above comments and suggested solutions where possible.

I appreciate your time, and would be more than happy to meet and discuss this with any of you if you have any questions or just want to brain-storm some of these options.

Respectfully,

Alan Nelson

Nelson + Morgan Architects - retired

1910 Vintage Drive, Corinth Texas 76210

c 940.390.3925

This email and any files transmitted with it may contain confidential information that is intended only for the addressee(s). Unless otherwise indicated, please do not share or forward this information without the sender's approval as it may not be intended for review, dissemination or use by other persons or unauthorized employees. I doubt anyone ever reads this, but if you do let me know. If you have received this communication in error, please contact the sender by telephone at (940) 566-0266 or respond via email and permanently delete the original email and any copies.

Miguel Inclan

From: Carmen "Ga Ga" Crooks <
Sent: Tuesday, June 29, 2021 10:38 AM
To: Helen-Eve Beadle
Subject: Community growing

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

I have lived in Corinth for 15 years. When will our city planning put Building restrictions in place to protect nature and keep our area beautiful. Growth is important but more can be done to protect our beautiful trees. And protect nature. U don't have to take them all down. We have coyotes eating all our pets because of this poor planning. Follow Flower Mound planning they did it right. Not money driven. Make developers show how they can save the aesthetics of our unique community.

Example: the CVS in argyle is built with trees and aesthetics in mind. Our CVS in Corinth is the sad version and obviously had no restrictions.

7-11 this is a major intersection yet the building is the wrong aesthetics.

Now we are being pushed into a new development of renters where you have an excellent golf course and nice neighborhoods. Why isn't our city protecting how our city grows or is it all about the money.

Nicer communities bring more money to the city in the long run and support the city to prosper. If u make our property values go down then we will leave.

Sent from my iPhone

Miguel Inclan

From: David Goodwin <[redacted]>
Sent: Wednesday, July 7, 2021 3:36 PM
To: Miguel Inclan
Subject: Propose rezoning - against

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

I am a citizen of Corinth reaching out to you regarding the proposed rezoning concern.

I am against this addition of hundreds of cars and people as proposed in a high density residential rental community. There are no retail establishments within walking distance - this will be a driving community which will add hundreds of cars in an already dangerous area.

- Safety concerns for area children walking and biking to school - it can be difficult already without extra cars
- Infrastructure concerns - our Internet service is fairly low already and adding these people will make the connections even slower.
- How does this align with the Master plan? I thought neighborhood retail was needed instead of high density residential?
- Cars at the beginning and end of the school day are already traffic problems - How can hundreds of additional cars make things better? Where is the traffic study that says this should be a good thing?

I see several other areas in my side of town where this development would have better chances of success.

David Goodwin
1646 Ash Lane

Miguel Inclan

From: Jeffrey Francisco <[redacted]>
Sent: Thursday, July 8, 2021 8:40 AM
To: Bill Heidemann; Sam Burke; Scott Garber; Steve Holzwarth; Tina Henderson; Kelly Pickens; Helen-Eve Beadle; Jennifer Olive; Cody Gober; Brian Rush; Wade May; Rodney Thornton; Billy Roussel; Francisco, Stephanie M
Subject: NexMetro Mistake

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

On February 22, 2021 the residents of the neighborhoods around the proposed development site of Oakmont and Lake Sharon answered the question of building 215 rental units with a resounding "NO". We didn't misspeak, we haven't forgotten, and we didn't change our minds. As a resident of the community, I stand to gain absolutely nothing from this area being covered with asphalt and apartments. Do You? What is being proposed will not be any kind of an upgrade to the neighborhood. As a matter of opinion, it will forever scar the intersection with an unnecessary apartment unit.

Does any one reading these emails actually believe that developing our little green belt will do anything for the golf course? Seems illogical to consider removing the natural boundary that shields our community country club and installing what will end up being. "just another group of housing units that will complain about golf balls on their porches". Horns sounding during a player's back swing will be heard by more residents than just those looking at a foursome on the tee boxes. It's not that funny.

Instead of finding ways to put more cars on the streets surrounding our homes and taking away some of the natural beauty, I have a different suggestion for the group.

Leave it alone! We said "NO" and we meant it. Asking the same question a different way won't garner a different result.

Also, maybe the group should make some proposals that we may appreciate. For example, a backup generator for the lift stations. We pay an extraordinary amount for water each month and our city doesn't have a back up plan. Or a back up plan to keep the power on when it gets cold and freezes outside. I mentioned earlier that we didn't forget, because sitting in a really cold house with no ability to heat it, or use the toilet without having a bathtub full of water ,is still very fresh on our minds. The excuses that were given were just that, excuses. Now get out of our neighborhoods with any plan to make money off of more taxes.

Without regard to your feelings

Jeffrey K. Francisco

Miguel Inclan

From: K Guidry
Sent: Tuesday, June 29, 2021 9:55 AM
To: Helen-Eve Beadle; Jennifer Olive; Cody Gober; Brian Rush; Wade May; Rodney Thornton; Billy Roussel; Bill Heidemann; Sam Burke; Scott Garber; Steve Holzwarth; Tina Henderson; Kelly Pickens
Subject: To whom it may concern - regarding my objection to the approved rezoning

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

This is in regards to the approval of the rezoning of the property is generally located at the northwest corner of Lake Sharon Drive and Oakmont Drive and east of FM 2499. (Avilla Fairways PD ZAPD20-0004)

Please add this email to the stack of emails and letters you have already received regarding the objections to the rezoning of this property. I will give you another perspective to add though.

I am originally from Louisiana. I have lived in Corinth, Texas for about 11 years now. My husband and I specifically moved here to be away from the hustle and bustle of the city - to be able to see squirrels, trees, birds and lakes - which I am very used to. I am slowly seeing the area being destroyed by additional developments. Animals have nowhere to go. Just this week a cat in Lake Sharon was eaten up by a coyote which i feel in part are due to the dwindling woods and nature in which they live. I realize this is inevitable, but we do not have to add to the progression. I am absolutely disgusted that the beautiful land remaining in Corinth is being sold so someone somewhere can line their pockets. I, like other residents, are paying very close attention to the progression of the potential Avilla neighborhood. It will definitely determine where I cast my vote in the next election or whether or not we decide to continue to live in this beautiful town. I am disheartened that this vote passed. I will be stepping up as well as many others to pay even closer attention to this. I implore you to listen to your constituents. If you support them, they will support you. I would also like to request the details regarding who voted for this rezoning as I do not see it in the meeting minutes - with my intention to not vote for them.. ever. You can be sure they lost any potential future vote for myself for any possible Corinth City appointments.

Thank you,
Kala Guidry
1410 Apache Trail
Corinth, Texas 76210
214-533-3460

Miguel Inclan

From: Karen Field
Sent: Wednesday, July 7, 2021 9:10 AM
To: Karen Field
Cc: Bill Heidemann; Sam Burke; Scott Garber; Kelly Pickens; Steve Holzwarth; Tina Henderson; Helen-Eve Beadle
Subject: Oakmont/Lake Sharon Rezoning

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear City Council Members,

I can not voice my objections loud enough for the proposed rezoning of the land at the corner of Oakmont Drive and Lake Sharon.

This is an already overly dense traffic area. The impact of an additional 430 cars is mind boggling. I worry about the 1,500+ students attending the nearby schools, not to mention their parents trying to navigate the overcrowded road. Horns are already honking and there is a simmering of road rage. I saw it every day, 4x a day. Add rain and fog ... it's frightening. The proposed roundabout is only going to add to the confusion and without a crossing guard at this intersection, I pray for the unsupervised children who must make this trek alone. This is a very pedestrian neighborhood. Show me a roundabout near two schools in a residential neighborhood. Will there be a crossing guard to help these school age children cross this busy intersection? We're talking kids as young as nine years old holding their little sister's hand. Most of the children who attend these schools do not meet the district requirement for riding a bus. Will an exemption be made for this? Has the traffic study been completed? I remember seeing a traffic camera installed on April 13, 2021. Here are a few pics from around that day, ten weeks ago. The proposed roundabout will be just past this line of waiting cars. Just another day in the neighborhood.







I would also think that our traffic rating of an 'F', with the proposed development and no roundabout, would make sense to most, that maybe this is not the right area. The extension of Rye Road is also not the answer. Helen, the city planner, who I think lives in Frisco, says Corinth is all for connectivity. And Larkspur needs another entrance/exit in case of emergency. When asked about Larkspur getting a gate, this too was shot down too. Meanwhile Fairways Estates is larger than Larkspur and has only one entrance/exit. How are they 'exempt' from connectivity? Then Villas on the Fairway is a gated community right in the heart of Oakmont. Again, how is this neighborhood exempt. Not to mention, Northwood on Swisher. Or Provence off of Post Oak. Or Corinth Forest with its gates. The rules apply to some and not others.

As previously stated, I am also worried about our schools. NexMetro may say they plan to target the retired professional wanting to golf. I honestly think these folks would prefer Robson Ranch, a community built for them with all the amenities. Not a city leaching off a private country club and golf course for apartment use. And as far as I know, there are no restrictions prohibiting renting to college students. I believe it was asked if the proposed renters needed to be related, i.e. a family unit, and the answer was no. Based on the most recent published data, Hawk has a student population of 676 students and is already the second largest school population wise, just after Pecan Creek with 678 students. Meanwhile, Hawk is the only 5-star elementary school in the area. Crownover has 917 students. Hawk is rated 344 out of elementary schools in Texas. Corinth Elementary, in Lake Dallas, is 2,041. Hawk's average test score on the STAAR test was 89.91 in 2019. Corinth Elementary was 50.1. (2021 test scores haven't been made public yet to compare schools.) You can't tell me that folks moving into the Avilla Fairways aren't interested in our schools.

And the timing of everything is suspect. In February, the P&Z Commission unanimously voted to reject the proposed development. So many residents voiced their opinions. For that moment, we thought you represented us. Fast forward to May, and the city elections. Everyone on City Council is re-elected. Miraculously, come June, this is on the agenda for the P&Z Commission once again. Really, nothing had changed and yet, the commission voted in favor of the

development. And here we are... the proposal before City Council. I also know more than 25 people email rezoning, as was stated by Helen on June 28, 2021. Maybe only 25 emails were received within a certain time frame. Will Helen present the real numbers before City Council? I hope so. I believe this is my fourth or fifth email in objection, but the 260+ page document captured one.

I've heard there are so many developers interested in this land. But Endeavor is in contract with NexMetro. I wish that we could hold out hope that at least one other developer would perhaps be interested in a Larkspur Phase III. I've also heard the land is valued between \$4-8 million. Didn't the city receive a nice check as part of the U.S. governments Covid relief fund. And yet, Corinth was minimally impacted by Covid, as a whole. In addition to updating the telephone system, maybe spend some of this money to buy back this land from Endeavor. Better yet, reject NexMetro! It's a win for Endeavor, as the land has probably been deemed more valuable in recent months, and work with them to find a good fit. Maybe someone is interested in building single family homes and Endeavor can make an extra \$2 million on the land. There are other options. It's a win win. To me, NexMetro is a glass half empty. Let's look at this as the glass half full ... and find the right fit for our community. Now is our chance!

Thank you for your time,
Karen Steger
1307 Ardglass Trail

Miguel Inclan

From: Keith Macy
Sent: Thursday, July 8, 2021 4:58 PM
To: Bill Heidemann; Sam Burke; Scott Garber; Tina Henderson; Helen-Eve Beadle; Kelly Pickens
Subject: vote no on zoning change for lake sharon and oakmont

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The city council are supposed to be representatives of the people of Corinth. Everyone I have spoken to say no this zoning change for various reasons. Too much traffic,safety of children, over crowding of schools.

[Keith Macy](#)

Miguel Inclan

From: Stacy Greaber <
Sent: Sunday, July 4, 2021 3:15 PM
To: Helen-Eve Beadle
Subject: Zoning change for Lake Sharon/Oakmont Drive

Importance: High

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Please do not approve the multi-housing zoning change for Oakmont and Lake Sharon. I watched the last meeting and I have a great concern for the parking, property values and safety. I'm about to build across the street and I currently live off of Post Oak. This will be bad for the community.

Feel free to forward this to whomever needs to see it. I appreciate the time.

Take care,

Stacy Edward Greaber
1901 Durance Ct
Corinth, TX 76210

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Miguel Inclan

From: Stephanie Francisco
Sent: Wednesday, July 7, 2021 9:21 AM
To: Bill Heidemann; Sam Burke; Scott Garber; Steve Holzwarth; Tina Henderson; Kelly Pickens
Cc: Helen-Eve Beadle; Jennifer Olive; Cody Gober; Brian Rush; Wade May; Rodney Thornton; Billy Roussel; Jeffrey Francisco
Subject: NexMetro Development

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Mr. Mayor, City Council, and Zoning Commission,

I wanted to send all of you an extremely clear message to inform you that we are against the zoning change for the land at the corner of Lake Sharon and Oakmont. We are against NexMetro developing that land to include 215 rental units.

I felt that the residents were very clear the first time this proposed zoning change was brought before the zoning board on February 22, 2021. I was in attendance at that meeting and there were no residents who were in favor of these changes. I find it appalling that there was ANOTHER zoning meeting on June 28, 2021 without any notice to the residents. Yes, we have seen the signs posted, however, there was no new information included on those signs about a new zoning meeting.

I'm not sure how or if the city council members are benefitting from this development, but the residents will be the ones who suffer. It was apparent back in February that our group of elected officials cannot support the residents who already live here. If we add this many more, it would be disastrous.

City council needs to focus on the infrastructure issues that are currently plaguing Corinth rather than trying to jam yet another apartment development into our neighborhood that we do not want.

Respectfully,

--

Stephanie M. Francisco

Miguel Inclan

From: Helen-Eve Beadle
Sent: Tuesday, July 6, 2021 8:20 AM
To: Helen-Eve Beadle
Subject: FW: Rezoning at Lake Sharon and Oakmont.

NO,NO,NO. we are long time residents in the area. Please don't allow all that construction to but so much traffic in this residential area. We strongly object to this change.

From: vicaug
Sent: Saturday, July 3, 2021 6:46 PM
To: Helen-Eve Beadle <Helen-Eve.Beadle@cityofcorinth.com>
Subject: Rezoning at Lake Sharon and Oakmont.

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Vic&Carol Augustine

1629 Cedar Elm Dr. Corinth

Sent via the Samsung Galaxy S9+, an AT&T 5G Evolution capable smartphone

Miguel Inclan

From: Bob Novinsky
Sent: Tuesday, June 22, 2021 9:31 PM
To: Helen-Eve Beadle
Cc: Home - Bob
Subject: Opposition to NexMetro rezoning case

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Ms. Olive,

My name is Bob Novinsky and I have resided for over 15 years in the Oakmont development on Redrock Drive. I strongly oppose the rezoning proposed by NexMetro/ Avila at the corner of Lake Sharon and Oakmont Drive.

Here are my concerns:

1. it does not comply with the City's Comprehensive Plan, which shows that tract as "Mixed Residential"
2. it is inconsistent with the overall existing PD zoning ordinances for Oakmont Country Club Estates (which this tract is included in),
3. increased density which will create significant traffic flow issues,
4. decreased or insufficient parking requirements,
5. increased traffic during school drop-off & pick-up hours because of new street onto Oakmont Dr. and increased traffic & parking within Larkspur subdivision (abutting this tract/across from Hawk) if Rye Street is connected for anything other than emergency access only,
6. dangerous for our school kids who walk to and from Hawk Elementary and Crownover Middle Schools because of the proposed roundabout at Oakmont Dr. & Lake Sharon Dr., and
7. the existing zoning already allows for townhomes and two-family garden homes (not much different than the smaller units proposed by the developer) so there is NO NEED to change the zoning.

I appreciate your attention and would hope that you would vote against and reject this rezoning proposal.

Thank you,
Bob Novinsky
2212 Redrock Drive
Corinth TX
214-316-8175

Miguel Inclan

From: Michelle Mixell
Sent: Friday, June 25, 2021 9:32 AM
To: Miguel Inclan
Subject: FW: Zoning change at Oakmont Dr. & Lake Sharon Drive

From: John & Cherie Holt >
Sent: Friday, June 25, 2021 9:30 AM
To: Michelle Mixell <Michelle.Mixell@cityofcorinth.com>
Subject: Zoning change at Oakmont Dr. & Lake Sharon Drive

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hello Michelle,
I would like to express to following concerns regarding the above mentioned Zoning change:

1. It will be dangerous for our school kids who walk to and from Hawk Elementary and Crownover Middle Schools because of the proposed roundabout at Oakmont Dr. & Lake Sharon Dr.
2. The existing zoning already allows for townhomes and two-family garden homes (not much different than the smaller units proposed by the developer) so there is NO NEED to change the zoning to be multi-family.

As fairly new residents of Corinth, we are a little surprised and disappointed that this is being considered. Corinth is a great place to live but increased street parking, traffic, and congestion don't seem appealing.

Thank you for your consideration and what you do for the City of Corinth.

Best,
Cherie Holt

Miguel Inclan

From: Amy Conine <aconine@avillafossilcreek.com>
Sent: Wednesday, June 23, 2021 8:51 PM
To: Helen-Eve Beadle; Jennifer Olive; Cody Gober; Brian Rush; Wade May; Rodney Thornton; Billy Roussel
Subject: Lake Sharon/Oakmont Zoning Request

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hello,

My name is Amy Conine, and I live at 1705 Goshawk Lane in the Eagle Pass Community of Corinth. This email is in reference to the zoning changes requested by Avilla Fairways. I would like officially state that I am against the zoning changes.

I attended the initial informational meeting on January 27th, the Planning & Zoning meeting on February 22nd, and the second informational meeting via Zoom on June 23rd. After seeing their initial proposal, I was disappointed to see that Avilla had made minimal changes based on discussion points and input of the community. One of these points was the maximum bedroom occupancy and car spaces per dwelling. As a realtor and property manager, I can say that all of Denton/Corinth rental properties are appealing to student tenants. I frequently receive applications for properties in residential areas that will be seven people for a four bedroom, or six for a three bedroom. In many instances, the neighborhood HOAs have By-Laws that prevent this type of occupancy, or the Landlord limits the number of vehicles allowed to prevent this type of occupancy. Considering the representative for Avilla referred to his concern over Fair Housing laws, it shows he does not have a grasp of what the Fair Housing laws apply to, nor does he understand the rental market in this area. His lack of concern for community input reveals an overall dismissal of resident concern. Therefore, I reach out to you, as the representatives of our community, to not dismiss our concerns.

Avilla mentioned that they do not typically appeal to applicants with children or college students. However, none of their other locations are located within walking distance of two schools, specifically schools with the ratings that Hawk and Crowover have. I have visited their Avilla Fossil Creek location, and while I would say it is not aesthetically pleasing, I will say, one of their talking points, is its convenience to TCU. Source: https://www.avillafossilcreek.com/mapsanddirections?gadid=515092478582&device=c&network=g&keyword=avilla%20fossil%20creek&adgroup=120446769505&campaign=12766325031&gclid=CjwKC_Ajw8uGBhBAEiwAayu_9XO50dfomEABB84pA-QxsrnMMdOJpkYsh76cvLaIIRrsMijYQcLSGhoCADUQAvD_BwE

Furthermore, I am against this zoning change request due to the increased traffic on Oakmont. Oakmont has a heavy pedestrian presence not just during at the beginning and end of the school day, but also after Crowover's athletic practices end between 5:30-6:00 PM. With students walking home in both directions on Oakmont, they are already competing with the traffic of commuters returning home, but would now have to contend with an additional access point on Oakmont, as well as a potential traffic circle, that as one city employee described, would have a "landing pad" for students to stand on as traffic swirled around them. There is nothing that sounds safe about having a landing pad. At the initial informational meeting, we were told by a city employee that a traffic study would be completed. At the Planning & Zoning meeting, the city attorney stated that it was

not required. These conflicting statements seem to suggest that the city does not have any plans to potential impact on its citizens. This is a dangerous oversight and a concern.

I could go on, and I will in future emails if needed, but I believe the current zoning that allows for townhomes and two-family garden homes is satisfactory for the area. I appreciate the time and dedication that you provide for our city on behalf of its citizens, and I implore to be our voice in regards to this matter.

Thank you,

Amy Conine
REALTOR

Mobile: 940-368-2160
Serving the DFW area
Web: <https://www.crownretx.com/>



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Miguel Inclan

From: Michelle del Carpio
Sent: Wednesday, June 23, 2021 1:54 PM
To: Helen-Eve Beadle
Subject: Rezoning of NW corner of Lake Sharon and Oakmont

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Helen-Eve Beadle,

I am writing to you today to let you know that I oppose the proposed zoning change for the Endeavor tract at the NW corner of Lake Sharon Drive and Oakmont Drive.

I would like to start off by making it clear that I object to anything that will enable a developer to build multiple rental properties in my neighborhood. This is not the kind of neighborhood I moved into 17 years ago, and not the kind of neighborhood I want to live in now. This developer is well known for building a rental neighborhood then selling it off a few years later and all original agreements about upkeep, etc. are then gone and the area rapidly declines. The City of Corinth does not need nor does its citizens want this type of development in our area.

Additionally, this development is very close to a school. This will greatly increase traffic in this area and add the need for a crossing guard to aid elementary school students walking home. The intersection at Lake Sharon and Oakmont will have so much traffic, that a stop light and additional turn lanes will be needed at the very least. The speed limit on Lake Sharon was just increased to 40 mph and it will be hard enough for children to get to and from school safely with the intersection and area as it is. Adding a roundabout is a terrible idea and it is too dangerous for 5 and 6 year old children to have to navigate through on their way to and from school. As an adult, I don't even want to think about having to navigate crossing a roundabout, let alone small children!

This doesn't even address the added crime that will come to the area due to renters. Renters have little to no loyalty to their neighbors or city. They do not care about the area they are renting in. It is just another temporary place for them to live before they move on to the next place. Statistics show that crime is higher near rental properties. The residents in the golf course area did not buy half a million dollar homes to have a rental property built right next door. This will drive property values down and will result in many of your long time residents to lose money on their property.

After the issues with the water and electricity in February, I think city infrastructure needs to be improved before focusing on adding additional rental properties in the area. The area along Lake Sharon is a water shed and there are already flooding and drainage problems on Blue Holly. Adding more concrete to this area is not the answer. Please focus on improving the lives of those long time residents that have hung in through thick and thin with the city before adding new "rental homes".

Please do not enable or allow this type of development in our city. Your job is not to work for the developers, but to work for the citizens.

Sincerely,

Michelle del Carpio
2506 Blue Holly Drive
Cypress Point Estates

Miguel Inclan

From: Wendy Dixon
Sent: Tuesday, June 22, 2021 11:17 AM
To: Helen-Eve Beadle
Subject: Protesting the zoning change

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Ms. Beadle,

When my family moved back to Texas 3 years ago, we happily chose Corinth (specifically across from Hawk and Crownover) for the excellent schools, safe neighborhoods and the small town feeling community.

We love taking family walks and bike rides through the beautiful tree-lined streets and waving to friends and neighbors along the way.

My children walk to school daily and feel safe when crossing the street thanks to our thoughtful crossing guard.

When I learned about the plans for a small townhouse area being built in the Lake Sharon and Oakmont area a few months ago, I joined the zoom calls and was one of many who shared my concerns. My husband and I were relieved when the plans were voted down so we could continue to enjoy our safe community with lots of beautiful trees.

I understand there is another proposal so I would like to share my concerns:

1. Increased traffic during school drop off and pick up hours because of new streets and more traffic. The safety of our children should be a TOP priority for you as part of the Planning and Zoning Commission.
2. The zoning change does not comply with the city's comprehensive plan which shows that tract as mixed residential. Can this zoning be changed to leave the land as is which is a beautiful area of trees?
3. Increased population density. The schools are great but adding a large number of new students isn't feasible.
4. A roundabout at Oakmont Drive and Lake Sharon is extremely dangerous. The traffic is designed to not stop flowing at a round-about. What about all of the children who walk to school? How will they navigate crossing the street safely when a large percentage of drivers don't even understand how to drive in a round-about?
5. Smaller properties including 600 square foot homes will decrease property value in the area. We all paid a high price to live in this community.
6. The existing zoning already allows for townhomes and two-family garden homes so there is no need to change the zoning unless it changes it to stay as a beautiful park area with trees.

As a concerned citizen of Corinth, I greatly appreciate your time and attention to this matter.

I ask you to please reconsider any proposals of building anything in that area unless it is a beautification project including parks and walking trails for the community.

Thank you,

Wendy

Section S, Item 7.

Miguel Inclan

From: Joan Dudley < >
Sent: Wednesday, June 23, 2021 8:30 PM
To: Helen-Eve Beadle; Jennifer Olive; Cody Gober; Brian Rush; Wade May; Rodney Thornton; Billy Roussel
Subject: Oakmont Drive and Lake Sharon Drive Proposed Rezoning

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

We strongly object to the rezoning of the Endeavor tract at the NW corner of Lake Sharon Drive and Oakmont Drive. There are multiple reasons we are protesting this change:

- . It does not comply with the City's Comprehensive Plan, which shows that tract as "Mixed Residential" (so changing it to multi-family does not meet the City's own definition of Mixed Residential.f**
- . It is inconsistent with the overall existing PD zoning ordinances for Oakmont Country Club Estates (which this tract is included in).**
- . Increased density (more dwelling units per acre) than current zoning.**
- . Decreased parking requirements than current zoning.**

Miguel Inclan

From: Joan Dudley <joan.dudley@gmail.com>
Sent: Wednesday, June 23, 2021 8:58 PM
To: Helen-Eve Beadle; Jennifer Olive; Cody Gober; Brian Rush; Wade May; Rodney Thornton; Billy Roussel
Subject: Re: Oakmont Drive and Lake Sharon Drive Proposed Rezoning

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Continuation of Joan Dudley e-mail

- . Increased traffic during school drop-off & pick-up hours because of new street onto Oakmont Drive and increased traffic & parking within Larkspur subdivision(abutting this tract/across from Hawk)
- . Dangerous for our school kids who walk to and from Hawk Elementary and Crownover Middle Schools because of the proposed roundabout at Oakmont Dr. & Lake Sharon Dr.
- . The existing zoning already allows for townhomes and two-family garden homes (not much different than the smaller units proposed by the developer) so there is NO NEED to change the zoning to be multi-family.

We believe the Best & Highest use of said Property would definitely be single family homes that would fit in much better with adjacent homes & properties.

As taxpayers, voters & residents in this City, WE ARE DEFINITELY AGAINST THIS PROPOSAL!

Thomas & Joni Dudley
1002 Balleycastle Lane
Corinth, TX 76210

p.s. WE ARE ALSO VERY CONCERNED THAT WE WILL BE LOSING ALL OR SOME OF THE TREES ON HOLE #13!!!!

On Wed, Jun 23, 2021 at 8:29 PM Joan Dudley <joan.dudley@gmail.com> wrote:

We strongly object to the rezoning of the Endeavor tract at the NW corner of Lake Sharon Drive and Oakmont Drive. There are multiple reasons we are protesting this change:

- . It does not comply with the City's Comprehensive Plan, which shows that tract as "Mixed Residential" (so changing it to multi-family does not meet the City's own definition of Mixed Residential.)
- . It is inconsistent with the overall existing PD zoning ordinances for Oakmont Country Club Estates (which this tract is included in).
- . Increased density (more dwelling units per acre) than current zoning.

. **Decreased parking requirements than current zoning.**

Section S, Item 7.

Miguel Inclan

From: Karen Field
Sent: Monday, June 21, 2021 9:09 AM
To: Helen-Eve Beadle; Jennifer Olive; Cody Gober; Brian Rush; Wade May; Rodney Thornton; Billy Roussel
Subject: Proposed rezoning at Oakmont and Lake Sharon

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

As a Corinth resident, I am disheartened that the proposed rezoning, specifically the Avilla Fairways project, is being represented to the Corinth Planning and Zoning commission.

I am worried about our schools and the increase in traffic. More than 1,400 kids attend Hawk Elementary and Crownover Middle School. So many of these kids walk to and from school; this is a very pedestrian neighborhood. The proposed roundabout is a horrible idea. Now cars don't even have to stop at Oakmont and Lake Sharon? And the new housing developments on Lake Sharon are only going to compound the mess. Show me a roundabout near two schools in a residential neighborhood. Will there be a crossing guard to help these school age children cross this busy intersection? We're talking kids as young as nine years old holding their little sister's hand. Most of the children who attend these schools do not meet the district requirement for riding a bus. Will an exemption be made for this? Has the traffic study been completed? I remember seeing a traffic camera installed on April 13, 2021. Here are a few pics from around that day, eight weeks ago. The proposed roundabout will be just past this line of waiting cars. This is a pretty typical day, maybe the line a little longer, due to the rain and less foot traffic. I know, as a Larkspur resident, I'm often one of multiple cars waiting to turn left into this mess every school morning.







The Rye Road access connecting Larkspur to Avilla should be for emergency access only. We do not need more cars in Larkspur! The developer has decreased their parking requirements, and thus underestimated the number of spots truly needed. Rye Road and Ballycastle could potentially be an alternate route to avoid the current traffic situation or worse, additional parking. I read somewhere that without a roundabout and without extending Rye Road a preliminary traffic study concluded this area would be rated an F. Makes me wonder if maybe this isn't the right location for another housing project.

Let's talk about the schools, and neighboring schools. It's been stated that this community hopes to attract an older, retired age resident who possibly is interested in golf. I beg to differ. Hawk Elementary is in the top 10 percent of elementary schools in Texas, ranked 344 out of 4479. Crownover is ranked 261 out of 2193. Neighboring schools: McNair Elementary is 783, Nelson Elementary is 1077, Pecan Creek Elementary is 1115 and Corinth Elementary is 2041 out of 4479 elementary schools in Texas. Hawk is the only one in the top 10 percent. There is a reason why Oakmont residents and surrounding neighborhoods purchase their homes in this area. We want the best for our children. The Avilla housing project is going to also attract families that want to attend highly successful schools. Will Hawk and Crownover be able to handle this influx and still keep their high rankings? Sadly, I don't think so. Generally folks living in apartments don't tend to stay in one place for years. They are not truly invested in their communities. Unfortunately the result is a revolving door and the schools are going to have to fill the gap. What first attracted us to the area, exemplary schools, is going to decline.

Isn't the proposed rezoning inconsistent with the overall existing PD zoning ordinances for Oakmont Country Club Estates. Plus it's my understanding that the city of Corinth's comprehensive plan shows this sleeper property as mixed

residential, not multi-family. I'm confused as to why the city is entertaining a potential development that with their comprehensive plan.

Section S, Item 7.

Aren't several developers interested in this land? Why not entertain some of the other proposals that maybe don't require rezoning and better yet, keep with the city's comprehensive plan.

Miguel Inclan

From: Karen Field
Sent: Monday, June 21, 2021 7:01 PM
To: Helen-Eve Beadle; Jennifer Olive; Cody Gober; Brian Rush; Wade May; Rodney Thornton; Billy Roussel
Subject: Proposed rezoning at Oakmont and Lake Sharon

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

I realized I forgot to sign, so I am resending my initial email.

Dear City Leaders,

As a Corinth resident, I am disheartened that the proposed rezoning, specifically the Avilla Fairways project, is being represented to the Corinth Planning and Zoning commission.

I am worried about our schools and the increase in traffic. More than 1,400 kids attend Hawk Elementary and Crownover Middle School. So many of these kids walk to and from school; this is a very pedestrian neighborhood. The proposed roundabout is a horrible idea. Now cars don't even have to stop at Oakmont and Lake Sharon? And the new housing developments on Lake Sharon are only going to compound the mess. Show me a roundabout near two schools in a residential neighborhood. Will there be a crossing guard to help these school age children cross this busy intersection? We're talking kids as young as nine years old holding their little sister's hand. Most of the children who attend these schools do not meet the district requirement for riding a bus. Will an exemption be made for this? Has the traffic study been completed? I remember seeing a traffic camera installed on April 13, 2021. Here are a few pics from around that day, eight weeks ago. The proposed roundabout will be just past this line of waiting cars. This is a pretty typical day, maybe the line a little longer, due to the rain and less foot traffic. I know, as a Larkspur resident, I'm often one of multiple cars waiting to turn left into this mess every school morning.







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Let's talk about the schools, and neighboring schools. It's been stated that this community hopes to attract an older, retired age resident who possibly is interested in golf. I beg to differ. Hawk Elementary is in the top 10 percent of elementary schools in Texas, ranked 344 out of 4479. Crownover is ranked 261 out of 2193. Neighboring schools: McNair Elementary is 783, Nelson Elementary is 1077, Pecan Creek Elementary is 1115 and Corinth Elementary is 2041 out of 4479 elementary schools in Texas. Hawk is the only one in the top 10 percent. There is a reason why Oakmont residents and surrounding neighborhoods purchase their homes in this area. We want the best for our children. The Avilla housing project is going to also attract families that want to attend highly successful schools. Will Hawk and Crownover be able to handle this influx and still keep their high rankings? Sadly, I don't think so. Generally folks living in apartments don't tend to stay in one place for years. They are not truly invested in their communities. Unfortunately the result is a revolving door and the schools are going to have to fill the gap. What first attracted us to the area, exemplary schools, is going to decline.

Isn't the proposed rezoning inconsistent with the overall existing PD zoning ordinances for Oakmont Country Club Estates. Plus it's my understanding that the city of Corinth's comprehensive plan shows this sleeper property as mixed

residential, not multi-family. I'm confused as to why the city is entertaining a potential development that with their comprehensive plan.

Aren't several developers interested in this land? Why not entertain some of the other proposals that maybe don't require rezoning and better yet, keep with the city's comprehensive plan.

Thank you for your time,
Karen Steger
1307 Ardglass Trail

Miguel Inclan

From:
Sent: Monday, June 21, 2021 12:34 PM
To: Helen-Eve Beadle; Bill Heidemann; Sam Burke; Sam Burke; Steve Holzwarth; Tina Henderson; Kelly Pickens
Subject: Rezoning of NW corner of Lake Sharon Drive and Oakmont Drive

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Once again, we would like to express our concerns regarding the rezoning of the NW corner of Lake Sharon Drive and Oakmont Drive. We have done additional research and find the following issues to be of concern to us:

1. This tract is included in the Oakmont Country Club Estates and the changes are inconsistent with the overall existing PD zoning ordinances.
2. What is proposed has more residences per acre than current zoning allows.
3. It does not appear to comply with the city's comprehensive plan – that plan shows the tract as “Mixed Residential”. If it is changed to multi-family, then it does not meet the city's definition of “Mixed Residential”.
4. Reduced parking requirements from the current zoning.
5. Then there is the concern regarding increased traffic around a multi-school area and adding a roundabout through which young children would have to walk to get to school. Add to that, the parents who already line up for blocks in order to drop off children could cause additional problems for morning and afternoon traffic in a roundabout.
6. Another street (Rye) funneling traffic onto Oakmont Drive would cause increased congestion in that area. Or increased congestion into the Larkspur subdivision onto Ballycastle Lane and Ardglass Trail.

Please reconsider and keep this area consistent with the other areas in Oakmont Country Club Estates.

Thank you for reading this email and considering the concern we as citizens of Corinth have regarding this rezoning.

Toni and Don Finn

Miguel Inclan

From: Bob Foster
Sent: Wednesday, June 23, 2021 9:59 AM
To: Helen-Eve Beadle; Jennifer Olive; Cody Gober; Brian Rush; Wade May; Rodney Thornton; Billy Roussel
Subject: AGAINST zoning change for Avilla Fairways proposal

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Ladies and Gentlemen,

I am writing to voice my opposition to the Avilla Fairways proposal by NexMetro development.

It is inconsistent with Corinth's comprehensive plan to to change the area from as mixed residential to multi-family zoning. I purchased my home and have remained in this area based upon the protections of existing zoning within Oakmont Country Club Estates and Corinth's comprehensive plan.

Please DO NOT move forward with the zoning change and help us maintain our community as has been intended.

I love to see development and growth, however it much be a controlled growth in order to remain consistent with the City of Corinth's Comprehensive plan. Thank you for your consideration.

Regards,

Robert Foster
2712 Navajo Road
Corinth, TX 76210
940-453-4369

Miguel Inclan

From: Tiffany Gough < >
Sent: Tuesday, June 22, 2021 4:12 PM
To: Helen-Eve Beadle; Cody Gober; Jennifer Olive; Brian Rush; Wade May; Rodney Thornton; Billy Roussel
Subject: Objection to proposed rezoning for Avilla Fairways

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear P&Z Committee Members,

I am a resident of Larkspur at Oakmont with two young children who will be walking to and from school beginning in the fall. I am already concerned about the flow of traffic on Oakmont near our home and the presence of only one crossing guard in that area and I have even greater concerns about the effects of the rezoning proposed under the Avilla Fairways proposal. We bought our home here so our children could safely walk to and from school. This area must remain mixed residential or single family only. Please note my strong objections to this proposal in your deliberations. Thank you.

Best regards,
Tiffany Gough
(2207 Makena Ct, Corinth)

Miguel Inclan

From: 1
Sent: Monday, June 21, 2021 3:04 PM
To: Helen-Eve Beadle
Subject: Zoning change at Oakmont Dr. & Lake Sharon Drive

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

I am objecting to the rezoning of the Endeavor tract at the NW corner of Lake Sharon Drive and Oakmont Drive

1. This does not comply with the City's Comprehensive Plan, which shows that tract as "Mixed Residential" (so changing it to multi-family does not meet the City's own definition of Mixed Residential),
2. It is inconsistent with the overall existing PD zoning ordinances for Oakmont Country Club Estates (which this tract is included in),
3. This will increase density (more dwelling units per acre) than current zoning,
4. It decreased parking requirements than current zoning,
5. It will increased traffic during school drop-off & pick-up hours because of new street onto Oakmont Dr. and increased traffic & parking within Larkspur subdivision (abutting this tract/across from Hawk) if Rye Street is connected for anything other than emergency access only [by the way, I asked the City earlier this week for a copy of the traffic study and was told it is not yet complete],
6. This will be more dangerous for our school kids who walk to and from Hawk Elementary and Crownover Middle Schools because of the proposed roundabout at Oakmont Dr. & Lake Sharon Dr., and
7. AND, the existing zoning already allows for townhomes and two-family garden homes (not much different than the smaller units proposed by the developer) so there is NO NEED to change the zoning to be multi-family.

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[Texas Real Estate Commission Consumer Protection Notice](#)



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Miguel Inclan

From: Mindy Jameson <[REDACTED]>
Sent: Wednesday, June 23, 2021 12:52 PM
To: Miguel Inclan; Helen-Eve Beadle; Jennifer Olive; Cody Gober; Brian Rush; Wade May; Rodney Thornton; BillyRoussel@boards.cityofcorinth.com
Subject: Oakmont Dr. and Lake Sharon Dr. Proposed Rezoning / Avilla Fairways Proposal
Importance: High

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Good Afternoon,

I am writing in regards to the Oakmont Drive and Lake Sharon Drive Proposed rezoning and the Avilla Fairways proposal.

We live in Larkspur, which will be directly impacted by this development. We will be able to see the development from our yard, which is currently beautiful green space - golf course and trees. The entire feel of our neighborhood will diminish with this development, as will the value of our homes. Oakmont is a beautiful community, and the City of Corinth should be proud of that and fight to conserve what we all have and love. This zoning variance request is not appropriate. I would hate to see Oakmont diminish...we have worked very hard to purchase our home and live in this wonderful community.

We have children that attend Hawk Elementary AND Crownover Middle School...this development is literally STEPS from these schools, where our children, as well as approximately 1400+ other children attend school. There is a large number of children who walk/bike home from these schools on a daily basis unsupervised. The proposed traffic changes at Rye Road and Oakmont/Lake Sharon would put these children (OUR children) in danger. The traffic is already bad before and after school...adding more traffic to this area would be a HUGE mistake for the City of Corinth.

Please consider how this would impact the community. Oakmont is SAFE right now – why would you want to change that? These apartment homes would bring in HUNDREDS of people to a very small area. Apartment homes produce more crime by default...they just do...we moved here because it is SAFE for our family. Also, renters do not show pride in their homes, as they do not own them. There would be an increase NOISE as well as SAFETY and CRIME concerns. We would soon see the entire area go downhill. The development company does not care about Oakmont or Corinth. They also do not care about Avilla Fairway or their other communities (which they seem to just turn around a sale anyway). I have seen many houses go up for sale in our community this year (more than ever before)...if this rezoning is approved, I guarantee you will see many more people leave the area. Oakmont is now a very nice, family oriented, golf course community. Bringing in a development such as this will diminish that family oriented feel. WHY would you want that for Corinth? The golf course will suffer (no one wants to stare at apartment homes), wildlife and trees will be plowed over, and our schools will experience overcrowding and safety concerns. Why would you not want to protect and preserve the characteristics that make Oakmont and Corinth such a great place to live?

After the winter storms, there is solid proof that the infrastructure in Corinth is not ready for another 200+ homes on a 24 acre lot. This is too much, and the city is not prepared to handle it at this time. You need to protect Corinth and make improvements (water/internet/roads/etc.) before moving forward with this or any other large development!

We moved to Corinth to get away from school over crowding. We wanted our children at Hawk and Crownover. We wanted to be near the golf course and experience the family environment and community feel of Oakmont. If this

happens, we will have to sell our house and move out of Oakmont. This directly impacts our CHILDREN. what we want in our community. This is NOT what we want our children to grow up next to. This is NOT what we want to see when we look out our window.

Please, PLEASE do not fail this community. Please stop this development from happening.

Thank you for your time,

Mindy Jameson
1103 Ballycastle Lane
Larkspur @ Oakmont Resident

Sent from [Mail](#) for Windows 10

Miguel Inclan

From: Jenn Kirkley
Sent: Tuesday, June 22, 2021 8:49 AM
To: Helen-Eve Beadle
Subject: Fwd: Opposition to zoning change to Endeavor Tract (lake Sharon & Oakmont)

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Sent from my iPad

Begin forwarded message:

From: Jenn Kirkley <jkirkley15@me.com>
Date: June 22, 2021 at 8:43:29 AM CDT
To: eve.Beadle@cityofcorinth.com, Jennifer.Olive@boards.cityofcorinth.com,
Cody.Gober@boards.cityofcorinth.com, Brian.Rush@boards.cityofcorinth.com,
Wade.May@boards.cityofcorinth.com, Rodney.thornton@boards.cityofcorinth.com,
Billy.Roussel@boards.cityofcorinth.com
Subject: Opposition to zoning change to Endeavor Tract (lake Sharon & Oakmont)

All

My name is Jennifer Kirkley , I reside at 1107 Ballycastle Lane. I oppose this zoning change for the following reasons and you should too!

1. The increased density of THIS plan is over the top ridiculous.
- 2 It does not even meet the zoning requirements of Oakmont Country Club Estates
We bought our homes believing these zoning requirements would be upheld
3. The increase in traffic will be mind boggling and so detrimental to our neighborhood it is not fathomable.
4. This increase in traffic is also so dangerous to the numerous school children walking back and forth to children each day, not to mention the transient clients that this development is going to attract.
- 5 The existing zoning allows for townhomes and two family garden homes, so there Is NO need to change the zoning!

Please do not destroy the golf course, our beautiful way of life here in Corinth, endanger the children, for developer, who will be gone in 3 years!

Respectfully
Jennifer Kirkley

Sent from my iPad

Miguel Inclan

From: delores knowles
Sent: Tuesday, June 22, 2021 3:20 PM
To: Helen-Eve Beadle; Jennifer Olive; Cody Gober; Brian Rush; Wade May; Rodney Thornton; Billy Roussel; Tina Henderson; Bill Heidemann; Steve Holzwarth
Subject: Protest of Zoning Change at Oakmont Dr & Lake Sharon Rd Corinth Tx

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Mayor Heidemann, Council Member Ms Henderson, Council Member Mr Holzworth and Zoning Committee Members:

I wish to inform you of my formal protest of the proposed rezoning of the Endeavor tract at the NW corner of Lake Sharon Drive and Oakmont Drive. My protests are based on the following

1. this proposal does not comply with the City's Comprehensive Plan, which shows that tract as "Mixed Residential" (so changing it to multi-family does not meet the City's own definition of Mixed Residential),
2. it is inconsistent with the overall existing PD zoning ordinances for Oakmont Country Club Estates (which this tract is included in),
3. increased density (more dwelling units per acre) than current zoning,
4. decreased parking requirements than current zoning,
5. increased traffic during school drop-off & pick-up hours because of new street onto Oakmont Dr. and increased traffic & parking within Larkspur subdivision (abutting this tract/across from Hawk) if Rye Street is connected for anything other than emergency access only
6. dangerous for our school children who walk to and from Hawk Elementary and Crownover Middle Schools because of the proposed roundabout at Oakmont Dr. & Lake Sharon Dr.
7. the existing zoning already allows for townhomes and two-family garden homes (not much different than the smaller units proposed by the developer) so there is NO NEED to change the zoning to be multi-family.

Thank you for your consideration.

Sincerely

Delores Knowles
1107 Oakhollow Dr
Corinth Tx

I live at 1401 Ballycastle Lane; Rye Road is in front of my mailbox and driveway. There is already a problem because coming up from the end of the street it is difficult to see the cars from the other end. What about the golf carts trying to cross? Will golfers want to hear the traffic and try to cross a one-way street? What if golfers stop coming to our course; then what? No greens to look at, just cheap rentals, car ports, cars and walls.

When school is in session the traffic in the morning and after school is already a problem. Parents park on Ardglass because the traffic is so bad on Oakmont. I know about the morning traffic due to the fact I leave early for health reasons. Having a roundabout is not safe for the children. The students leave at different times in the afternoon; due to school activities. No crossing guard(s) after a certain time. Having lived here for 15 years; we have had to fight the City Council when we needed the three way signs at Oakmont and Robinson, which they didn't want to do. Safety is not a major concern to them.

I saw the housing they build and they are CHEAP looking. When I built that was not what I was told would be built there. Corinth does not need any more apartments and that's what they are - one story apartments. Why mess up our neighbor hood? We paid to live in a quite golf club area not a rental community. Rye Road needs to have an "emergency gate" if they must open the road.

Miguel Inclan

From: James Leverett
Sent: Wednesday, June 23, 2021 6:24 PM
To: Miguel Inclan
Cc: Gabe & Lupe Silva Friends; Chip Lucas; Teresa; Carol Leverett Family
Subject: Objection to the Proposed Rezoning - Endeavor Tract

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

To: Miguel Inclan, City Planner

Dear Sir,

I have written before but I feel it needs to be said again, I Strongly object to any effort on the part of the City of Corinth to Rezone the Endeavor tract of land bordering Larkspur, hole 12, and hole 13 of Oakmont CC. I live on the 13th hole, right across the fairway from this tract of land.

I have multiple reasons to object:

1. The current infrastructure of roads, schools, streets, utilities are NOT sufficient to support an increased building density and population density increase. The P&Z has already ruled against this once. Sending it back just telegraphs the counsels intentions regardless of their constituents desire.
2. The school children’s safety is a huge concern. Having to try and cross at a round about is an accident waiting to happen. If you put personnel there to stop cars, you have defeated the purpose of the round about.
3. The traffic backup waiting to drop kids off in the morning and pick them up in the afternoon, already causes huge lines of cars waiting on Oakmont drive. Add to that the additional traffic from increased population due to this multi family development, and you have a bigger mess.
4. I am not against what zoning currently exists and has existed for 30+ years. We did not move in unwittingly hoping that no one would move in across the fairway, but I am completely against changing the zoning to accommodate multi-family housing. Build what it is zoned for.
5. The examples of past developments like this one proposed, are a disgrace. They may look good when they are built. They promise the world, but then they sell it to another company to run. In very short order, they are run down, low rent units. Maybe the city counsel should go look at some of the past projects that are more than a couple of years old.
6. The access into Larkspur via Rye road will only increase traffic in an area not designed for high traffic load. That traffic still has to dump onto or come off of Oakmont which further exacerbates the traffic issues on Oakmont.
7. Last, but not least in my mind, is the lost revenue/value of our homes. We paid a premium for our lots on the golf course and built high end homes. Had we known that there would be increased population density, increased traffic, and multi-family units across from us, we would have built somewhere else. You might say that there will not be a loss of value. I say you are already wrong. Homes in this subdivision were selling for \$200+ / sq. ft. as recently as April 2021. Since it has become known that there could potentially be multi-family units across the fairway, 3 homes on the market currently are either not receiving offers or they are so low, it’s laughable.

Please reconsider this carefully and maintain the current zoning, which would be very similar to the existing housing on both side of this development.

Thank You,

Jim and Carol Leverett
1220 Ballycastle Ln
Corinth, TX 76210

Miguel Inclan

From: John Malloy <John.Malloy@jcdelivery.com>
Sent: Monday, June 28, 2021 4:13 PM
To: Miguel Inclan
Cc: Kelly Pickens
Subject: BUSINESS AGENDA ITEM 4

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Miguel,

I hope this finds you safe and well.

I am unable to attend tonight's P&Z Meeting. I am emailing you so that my feelings on the single story rental project and purposed roundabout at Lake Sharon, can be written in the record.

I am strongly against both these project. I feel the way the land is zone today it should stay. The owners knew the zoning when they purchased it. I have no problem with Garden Homes, Condo's or any other of the current land zoning use. I've heard that some people feel that the current owner may file a lawsuit. On what grounds? AGAIN, the owners knew the zoning when they purchased it. The purposed project will be devastating to our property values as well the over all look of our Oakmont Country Club Community.

As far as a turnabout at the intersection of Oakmont and Lake Sharon Road, terrible idea!! Children have to cross that intersection everyday. I can't tell you how many driver's have a difficult time with the roundabout in Unicorn Lake. I personally have witness this MANY TIMES. Please consider our children's safety!

Thank you for your time.

John E. Malloy
Chief Executive Officer
J.C. Delivery Inc.
1645 Wallace Dr. Suite 120
Carrollton, TX 75006
Phone:972.434.7767(pros)
Fax:972-434-7769
Cell:214-869-0788

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Miguel Inclan

From: Aaron Petty <[redacted]@nextmetro.net>
Sent: Monday, June 21, 2021 8:49 PM
To: Helen-Eve Beadle
Subject: Protest - NextMetro Zoning Change

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Helen-Eve Beadle,

I live at 1402 Ballycastle Lane and I am protesting the zoning change for the following reasons:

- 1) It does not comply with the City's Comprehensive Plan, which shows that tract as "Mixed Residential" (changing it to multi-family does not meet the City's own definition of Mixed Residential)
- 2) It is inconsistent with the overall existing PD zoning ordinances for Oakmont Country Club Estates (which this tract is included in)
- 3) Increased density in a compressed area (already congested)
- 4) Decreased parking requirements
- 5) Increased traffic during school drop-off & pick-up hours because of new street onto Oakmont Dr. and increased traffic & parking within Larkspur subdivision (abutting this tract/across from Hawk) if Rye Street is connected for anything other than emergency access only
- 6) Dangerous for our school kids who walk to and from Hawk Elementary and Crownover Middle Schools because of the proposed roundabout at Oakmont Dr. & Lake Sharon Dr.
- 7) The existing zoning already allows for townhomes and two-family garden homes (not much different than the smaller units proposed by NextMetro) so there is NO NEED to change the zoning.

* I never received any formal notification of any kind relative to the NextMetro development.

Thanks,
Aaron & Bridget Petty
1402 Ballycastle Lane

Miguel Inclan

From: Justin Reed <justin.reed@il.com>
Sent: Monday, June 28, 2021 4:30 PM
To: Miguel Inclan
Subject: Avilla Fairways Comments

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Mr. Inclan:

Please review my comments below regarding the proposed zoning change for NexMetro's proposed community, Avilla Fairways. These are similar to what I had sent in for the previous hearing however some items were modified.

I have been apprised that there are plans to develop approximately 24 acres of undeveloped green space adjoining Oakmont golf course & several communities with densely-packed rental units. I have reviewed the websites and photos of NexMetro/Avilla communities and I can see that they have a unique appeal and do a fine job in developing their properties. Unfortunately, I do not share the vision nor see the attractiveness of a complex such as this in our community.

In reviewing the guiding principles for *Envision Corinth 2040*, the vision of this Avilla community does not seem to align with what was stated for Corinth's vision:

--"A Dynamic and Aesthetically Pleasing Community": while the Avilla community may appeal to multiple generations, it will negatively-change the landscape of the environment along the Oakmont golf course and surrounding communities. A packed set of rentals among single-family owned properties will not be appreciated by citizens who have significantly invested in their homes and property. Additionally, golfers may not appreciate the appeal of 30+ tiny backyards with pets and potentially less than ideal regulations (compared to the Oakmont POA) along the course. Given that individuals will generally value a rental property less than their personally-owned homes; the long-term effect of leased properties may not be as aesthetically-pleasing nearby relative to individually-owned properties.

--"Complete, Connected, and Safe Neighborhoods": our home is located in the adjoining Larkspur community (via Rye Rd). I love the safety of our neighborhood as there is minimal traffic at this time as there is no through traffic; this connection and community will compromise that safety. Additionally, my understanding is the nearby elementary/middle schools are already at capacity and may not support or be able to provide a place for children in a new community (are there plans to add more schools to compensate for the new growth?). I feel that this zoning change compromises the ability to 'provide quality goods & services', including education, in this portion of the community, thereby, making it less complete/connected.

--"Future Infill Development": this development/zoning change is not context-appropriate and it is not compatible with existing adjacent development. It also seemingly doesn't take "... into consideration the concerns of the current residents..." and "...retain long-term value in Corinth".

My family has lived in Oakmont since 2015 and I have lived in Denton County for the majority of my life. I grew up going to church camp at Lake Sharon when there was almost nothing in the immediate area. I am not opposed to progress but I believe our community and officials should be mindful of the existing citizens' values and the beauty of the green spaces, waterways, and landscape. Clear-cutting a majority of trees along a golf course and building packed-in garden homes amongst an area with single-family homes doesn't seem compatible or aligned with what makes the Oakmont community appealing to so many families. Changing this existing environment is a one-time event and cannot be undone afterwards.

In the past year, I have evaluated at least two businesses to purchase and even considered starting one but all of these opportunities would require a significant compromise in what I value in my career. I remind myself that opportunities are like trains; there will be another one that's coming shortly. I hope that the City of Corinth officials will keep this in mind and not compromise their guiding principles to making our community valued for years to come.

Thank you for the opportunity to share my comments on this matter. It is fortunate that we are discussing promising growth of our community rather than a less-fortunate alternative. I look forward to seeing how Corinth proceeds into the future.

Justin Reed

Miguel Inclan

From: Ronald Ribman
Sent: Tuesday, June 22, 2021 7:39 PM
To: Helen-Eve Beadle
Cc: Jennifer.Olive@boardsofcorinth.com; cody.gober@boardsofcorinth.com; Brian.Rush@boardsofcorinth.com; Wade.May@boardsofcorinth.com; Rodney.Thornton@boardsofcorinth.com; Billy.Roussel@boardsofcorinth.com
Subject: Avila Fairways

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Ms. Beadle, Jennifer Olive, Cody Gober, Brian Rush, Wade May, Rodney Thornton, William Roussel III,

I strongly object to The Avila Fairways housing project. I can't think of a single benefit this project will bring to the residents of Larkspur, unless you consider increased crowding, greater pollution, crime, school traffic accidents, transient rental housing and the general diminishment of property values making this area a less pleasant place to live a benefit.

Sincerely,

Ronald Ribman, Ph.D

To Whom it May Concern:

I would like to express my concerns regarding the zone change at Oakmont Dr. and Lake Sharon Dr. While I think the proposed rental community would be a welcome addition to Corinth I do NOT believe that this particular location is suitable for what they are wanting to build. The surrounding area is a very quiet and quant community where our kids are able to walk to school and people run and walk the area all the time. We also have multiple members of Oakmont CC who live in the surrounding area that drive golf carts to and from the CC. All of this would possibly have to change if we had more traffic in the area. This would change the feel of the community that is the Oakmont area. As I understand it that tract of land was zoned, years ago before any of the surrounding neighborhoods were there, as a mixed residential use. This was before there were two schools in the picture both of which are at capacity with just the surrounding neighborhoods already. Bringing more people who will just rent into the area will increase the capacity at our children's schools along with increased traffic on Lake Sharon Dr and Oakmont Dr. During school times drop off and pick-up times, special events at the schools etc we already have traffic issues at the schools. This is a community where most people own their own home and you have residents that have lived here over 20 years. This tract is already zoned for mixed residential which means that townhomes or garden homes would fit and blend in with the surrounding neighborhoods.

I personally have nothing against renters and would actually love it if my son and wife could live there while they are looking for their forever home; however, the short-term occupancy doesn't fit in with people who have made this area their forever homes. Renters do come and go and unless there is something in the contract with Avilla that says they can never sell to another management company we have no assurances that the property will be kept up to match the standards of the communities that surround it. This coupled with the fact that Oakmont CC does bring a sense of a golf course community

and ruining the natural trees and foliage that make the golf course so beautiful will likely decrease people wanting to join.

As a board member for the Lake Sharon Estates I'm also concerned with what removing all the trees and vegetation will affect the dam that we are getting possibly ready to take ownership of. We've seen nothing showing what the plans would be in order to keep Lake Sharon from being impacted by the development, if that exists we would love to see it.

Thanks for your time,

Christi Sessions

Lake Sharon Estates, HOA VP

817-988-4245

Christi.sessions@yahoo.com

Miguel Inclan

From: Christi Sessions <[redacted]>
Sent: Monday, June 21, 2021 1:11 PM
To: Helen-Eve Beadle
Subject: P&Z meeting letter
Attachments: PZ letter.docx

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Helen,

Please see attached my letter of protest against the development that is being proposed for the tract of land at Oakmont Dr and Lake Sharon Dr. Please let me know if you have any questions. I am planning on being on the call on Wed night.

Thanks

Christi Sessions

Miguel Inclan

From: Gabriel Silva
Sent: Monday, June 21, 2021 9:17 AM
To: Helen-Eve Beadle; Jennifer Olive; Cody Gober; Brian Rush; Wade May; Rodney Thornton; Billy Roussel
Subject: Objection to Rezoning - No to Avila Homes!

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hello all Planning and Zoning Commission Members and Staff, hope you are doing well.

Writing (again) to express my complete disagreement and objecting to the rezoning of the Endeavor tract at the NW corner of Lake Sharon Drive and Oakmont Drive.

We've been through this before and we have expressed our multiple concerns about this before. Allowing the rezoning and giving green light to a monster project like Avila Homes will change our city forever.

The proposed rezoning will only serve the developer and, once they make a profit, they will sell the property and leave the city and it's residents with a big problem.

This project doesn't comply with the City's Comprehensive Plan, which shows that tract as "Mixed Residential" (so changing it to multi-family does not meet the City's own definition of Mixed Residential). It is also inconsistent with the overall existing PD zoning ordinances for Oakmont Country Club Estates (which this tract is included in),

If built, the increase in density will increased traffic during school drop-off & pick-up hours because of new street onto Oakmont Dr. and increased traffic & parking within Larkspur subdivision (abutting this tract/across from Hawk) if Rye Street is connected for anything other than emergency access only [by the way... where's the traffic study results?] It will become very dangerous for our school kids who walk to and from Hawk Elementary and Crownover Middle Schools because of the proposed roundabout at Oakmont Dr. & Lake Sharon Dr., and the existing zoning already allows for townhomes and two-family garden homes (not much different than the smaller units proposed by the developer) so there is NO NEED to change the zoning.

Also, the property value for those homes adjacent to the project will be greatly affected... not to mention the area will show and increase in violence and crime (big density projects like this ALWAYS bring crime and violence and they are multiple studies made about this).

Please listen to your citizens. This rezoning has been denied by P&Z before for this same project. We've been through this before... please deny the rezoning request.

YOUR CITY. YOUR VOICE.... right?

Yours,

Gabriel E Silva
1222 Ballycastle Ln
Corinth, TX 76210

Miguel Inclan

From: Smith,Fred < >
Sent: Thursday, June 24, 2021 3:39 PM
To: Miguel Inclan
Subject: Avilla Fairways

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Miguel,

I am a homeowner in Larkspur. I live at 1317 Ardglass Trail which is the first house in the subdivision. Below are some comments on this proposed development:

1. Corinth is a very nice city. It takes work to achieve that status.
2. The greater Oakmont area is the core of our community. It is composed to single family housing.
3. Rental communities distract from the community because of their transitory nature. Once approved this can never be undone even when it is realized that it was a mistake.
4. In society there is a great feeling that the people's representatives are not listening to their constituents and so it is with the case of the Avilla Fairways proposal. There have been several meetings where severe opposition was expressed only to find that the city is still trying to ram this through
5. I and others oppose this been a rental community.
6. There should be no connection to Rye Road, not even one way.
7. There should be no access to Oakmont Drive. All access should be off of Lake Sharon
8. It is absurd to think about putting a roundabout at Lake Sharon and Oakmont Drive. There are large numbers of children that walk through this intersection daily. A roundabout creates a major hazard. This obviously means this rental community is going to create too much traffic.
9. Surely everyone should direct their abilities to creative use of this property in the center of single family homes instead of trying to push this through. Please do not destroy our community by short sighted decisions. Try to make a positive decision that will enhance our community instead of a decision that will start the area into decline.
10. Promises from the owners of a rental community will never be kept. They will hold for 5 or so years and then sell to a new owner.

Fred

Fred Smith, CFP | Financial Advisor

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Miguel Inclan

From: Jeff Wickstrom < >
Sent: Tuesday, June 22, 2021 3:17 PM
To: Michelle.Mixel@cityofcorinth.com; Miguel Inclan
Subject: RE: Zoning Change - Avilla location on Lake Sharon/Oakmont

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I heard there's another meeting coming up on this topic.
See Email below

From: Jeff Wickstrom
Sent: Wednesday, April 14, 2021 11:37 AM
To: Michelle.Mixel@cityofcorinth.com; Miguel.Inclan@cityofcorinth.com
Subject: Zoning Change - Avilla location on Lake Sharon/Oakmont

I am opposed to the zoning change and building of apartments near the Oakmont golf course. We moved to Oakmont for the rolling hills and wooded area and have been very happy here. **Apartments are not what this community needs.**

Please consider the following concerns:

- Rental property is inconsistent with the current owner properties, near a private golf course community
- Negative impact to Oakmont safety/family community, especially Larkspur
- Reduction in property value with rental property and high turn around
- Increased traffic congestion; Rye Road does not need to open into Larkspur, creating more traffic and a place for people to cut through (safety)
- Reduction in trees and impact to area wildlife
- Unsafe for children walking to local schools

Thank you for your time and support in this matter, sincerely
Jeff Wickstrom
2200 Valderamma Lane
Corinth, Texas

From: Helen-Eve Beadle <Helen-Eve.Beadle@cityofcorinth.com>
Sent: Wednesday, April 14, 2021 11:32 AM
To: Jeff Wickstrom <Jeff.Wickstrom@PACCAR.com>
Subject: Automatic reply: Zoning Change - Avilla location on Lake Sharon/Oakmont

I will be out of the office Wednesday, April 14 through Friday April 16, 2021. I will respond to your email on Monday, April 19, 2021.

If you need immediate assistance please contact Michelle Mixell or Miguel Inclan
Thank you.

Miguel Inclan

From: Sue Wood < >
Sent: Wednesday, June 23, 2021 11:04 AM
To: Helen-Eve Beadle; Jennifer Olive; Cody Gober; Brian Rush; Wade May; Rodney Thornton; Billy.Rouseel@boards.cityofcorinth.com
Cc: Tony Alfano
Subject: Opposition to Oakmont Drive and Lake Sharon Drive Proposed Rezoning

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Hello,

My name is Susan Wood and I am co-owner of property at 1006 Ballycastle Ln, Corinth, TX 76210 with Tony Alfano who is copied on this email.

We are opposed to the Avilla Fairways proposal to rezone property at the corner of Oakmont Drive and Lake Sharon Drive. Numerous reasons have been expressed by members of our community. You should have these on file. Please make note of our opposition to the proposed rezoning.

Thank you for your time and consideration.

Sincerely,
Susan Wood
1006 Ballycastle Lane
Corinth, TX 76210

Miguel Inclan

From: Katherine Woodward <Katherine.Woodward@cityofcorinth.com>
Sent: Wednesday, June 23, 2021 2:33 PM
To: Helen-Eve Beadle
Subject: Objection to Proposed Rezoning - Endeavor Tract

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Ms. Beadle,

I would like to voice my very strong objection to the rezoning of the Endeavor tract at the NW corner of Lake Sharon Drive and Oakmont Drive. Although my home is not within the 200 foot limit because the fairway is wide at that point, it does back up to the fairway directly adjoining the proposed development.

When I purchased my home in Larkspur in 2012, this adjoining tract was included in the existing PD zoning ordinances. It was zoned "Mixed Residential" and allows for townhomes and two-family garden homes. The size of those entities is not much different than the proposed units, but the density is much greater.

The greatly increased density of the current proposal is the primary reason I so strongly object to the change in zoning. As it currently is, Oakmont Drive is a very busy street and route to FM 2181. During school arrival and departure times, Oakmont Drive, a wide two-lane road, becomes a very congested four lane road that comes to a standstill. Entry to and exits from Larkspur are very difficult under those conditions now. With increased population density in the immediate area, Oakmont Drive will become even more dangerous for both vehicles and the school-age children walking to and from Hawk Elementary and Crownover Middle School. The proposed roundabout at Lake Sharon and Oakmont Drive will do nothing to alleviate that danger or congestion. Students who must pass through that intersection will face a gauntlet of vehicles every day. I also see no rational reason to open Rye Road into the proposed development. It will do nothing but increase traffic trying to enter and exit Larkspur and increase the danger to golfers as they try to cross to the next hole.

Finally, the increased density will inevitably make the properties that all of us in Larkspur purchased for the character and peacefulness of the neighborhood less desirable and less valuable. Homes that are comparable to those that exist on both sides of Lake Sharon would be more consistent with the surrounding neighborhoods and all of us would maintain the value of our investments.

Sincerely,
Katherine Woodward
1306 Ballycastle Ln.
Corinth, TX 76210

Miguel Inclan

From: Aaron Petty
Sent: Saturday, February 20, 2021 8:21 PM
To: Miguel Inclan
Subject: Proposed P6 Development

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hi Miguel,

Spoke with you before about this rental development behind my house.

I wanted to strongly oppose the development for the following reasons:

Clearing the land of most of the trees and wildlife (there won't be left even if they leave some trees) Increased traffic for our kids from a safety perspective Destroy the view for all of us who moved to our quiet neighborhood.
215 rentals with just one car would mean, at minimum, 215 more cars (congestion in this area with safety concerns)
215 rentals with 4 person family would mean 860 more ppl right behind us (congestion, noise, safety concerns etc. in this area) Long term home value concerns

Thanks,
Aaron Petty
1402 Ballycastle Lane

Miguel Inclan

From: A.J. Azzarello < >
Sent: Monday, February 22, 2021 1:54 PM
To: Miguel Inclan
Subject: Rental townhomes Oakmont & Lake Sharon

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Because I am unable to attend the meeting in person this evening, I wanted to send a note saying how incredibly against me and neighbors are about the possible rezoning of the area on Oakmont & Lake Sharon to allow for 215 rental townhomes.

This will greatly increase traffic on Oakmont, which many kids play around and utilize for walking to school at Hawk and Crownover. That one of the major benefits to our community, walking to school thru middle school.

The level of education our children receive at Hawk and Crownover will be diminished, as this will overwhelm the schools and allow for lower income families to move into the area just for schooling.

Multifamily complexes will lower our home values, along with increase of traffic this is terrible for all surrounding subdivisions.

The city financial gain from this in no way can overcome the negatives – this will force more people out, turning more houses in subdivisions to be rental properties also.

Please hear the community complaints, and react accordingly by not allowing this re-zoning to occur.

Thank you for the consideration.

A.J. Azzarello | Director, eCommerce & Customer Engagement | The Container Store
972-538-6709 phone | 972-538-7609 fax

[containerstore.com](https://www.containerstore.com) |    

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Miguel Inclan

From: ali kohandani
Sent: Sunday, February 21, 2021 3:07 PM
To: Miguel Inclan
Subject: New developments in Corinth

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Hi Miguel

My name is Ali kohandani and I am a resident on 1227 Ardglass trl in Corinth. I heard there is a new development coming close to my house. 215 rental town homes is a lot for our Neighborhood. I moved to my house 4 years ago and one of my reason why I choose Corinth was because of the school for my 2 children. PLEAE DO NOT LET THEM TO TAKE THAT TOWN HOMES. If you have any questions please call me at 940-595-0700 Thank you

Sent from my iPhone

Miguel Inclan

From: Alice Ribman
Sent: Monday, February 22, 2021 6:07 PM
To: Miguel Inclan
Subject: Rental Development and zoning change

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

My husband and I are deeply upset over the proposed zoning change being considered for this development. We moved into the Larkspur community 15 years ago, choosing this community because of it's open uncrowded space, lack of traffic congestion, safety, and quality of the community. Turning this lovely residential neighborhood into a high density, high traffic commercial rental environment is a total violation of everybody's quality of life.

Changing the zoning right next door to our community is a violation of what was promised to us when we purchased our home. It's unfair for the Town of Corinth to so disregard this pledge. Zoning is a contract that the city makes with its residents inducing them to settle here only to betray that trust.

I have no confidence that the Town Of Corinth will do anything but betray us and sell us out for financial rewards.

Alice and Ron Ribman

Sent from my iPad

February 11, 2021

Miguel Inclan, Planner
3300 Corinth Parkway
Corinth, Texas 76208

Dear Mr. Inclan:

The following represents my opposition to the proposed zoning change for Lake Sharon and Oakmont Drive. I will be sending this to the mayor and city council members as well as posting it to Oakmont Neighborhood.

How shocked I was to discover the City of Corinth is considering allowing project at Lake Sharon and Oakmont Drive that was 215 multi family dwellings. I was even more dismayed when I watched the presentation and realized how totally destroyed this corner would be. Though we are told there is a tree preservation clause, there is no way any trees can be preserved there except for those running along the golf course. Surely our city planners never envisioned this type of project and all the concrete it will involve in place of a wooded area.

What was the original plan for this corner? It was a plan which included duplex, townhome, and neighborhood shopping for the corner. That still means the destruction of this property; loss of trees, birds, small ground animals, etc. But if we have to pick, then I would accept this over the other.

In addition to ravaging the land, there is also the problem with all the additional traffic that will be added to Oakmont Drive. Currently this street has a great deal of traffic. Even with the addition of 2499 and Lake Sharon Drive, the amount of cars is still high. What is worse is that the speed limit is 30 mph and never enforced. There is also a great deal of foot traffic as children go to and from school that this will affect.

The units will start at the Larkspur entrance and wrap around to the flood plain area near 2499. They also have to put in a road that will cut across the golf course.

215 residences X 2 cars per resident equals 430 more cars for that area. If there is an average of 3 people per unit, that is 645 more people in that little space.

How many will be school children? There are more houses planned for 2499 and Lake Sharon. Do you think DISD will be excited about this? Is there sufficient room in the two schools to accommodate this influx. And do not use the excuse that many of these will be older residents with no children. Rentals=more children.

I do not live in Larkspur nor do I play I golf, but if I did my level of outrage would be even greater. A road has to be built across the golf course to connect Lake Sharon Drive and the Larkspur addition. WOW!

A city council member told me he is tired of going to Denton, Highland Village and Flower Mound for “nice” dining. According to him, the way to attract more restaurants is with more people. So we sacrifice trees, small animals, birds and decimate a golf course?

I know this property belongs to Endeavor Oil Company who really want to sell it. I have been here long enough to remember all the promises we were made about the natural gas pipeline. And then how things seem to dissolve with that. Like many of those companies who were sure they could make a fortune, this did not work out. Of course, they want to sell and could care less who gets the land or how it will be used.

There are so many more reasons to NOT change this zoning. Hopefully, our voices will be heard.

Sincerely,

Anna (Kitty) Hudnall, Ed.D.
1618 Fairway Vista
Corinth

Miguel Inclan

From: Tony Alfano
Sent: Sunday, February 21, 2021 3:52 PM
To: Miguel Inclan
Subject: Rezoning hearing Avilla Fairways Planned Development, STRONGLY OPPOSED TO REZONING

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Mr. Inclan,

I have been a property owner in Corinth for 23 years. I currently reside at 1006 Ballycastle Ln, Corinth, TX 76210, directly across the fairway from the proposed Avilla Fairways Planned Development.

I am in strong opposition to the rezoning of the proposed area.

I think it would serve the community better to keep this area zoned for single-family homes.

Best regards,

Anthony P. Alfano
1006 Ballycastle Ln, Corinth, TX 76210
Mobile: 469-951-0264

Miguel Inclan

From: Bill Cramer
Sent: Friday, February 12, 2021 8:03 PM
To: Miguel Inclan

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

I oppose the apartment development on Lake Sharon between Oakmont and 2499 on Corinth. I am a home owner in Oakmont CC and want to preserve the quality of the neighborhood and not increase the traffic near the 2 schools in the area.

William and Patricia Cramer

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Miguel Inclan

From: MW
Sent: Monday, February 22, 2021 3:20 PM
To: Miguel Inclan
Subject: Planning and Zoning Meeting 2/22/2021

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

My name is Bob Wiman. I reside on Oak Ridge Drive. Corinth 76210. I am against this proposed project because it will adversely affect my property value. This type of development will also create increased traffic congestion, which will create safety concerns for children attending Hawk Elementary.
Please note my strong opposition to this project.

Bob Wiman
Sent from my iPad

Miguel Inclan

From: Bridget Petty (US)
Sent: Monday, February 22, 2021 1:53 PM
To: Miguel Inclan
Subject: Avilla Fairways

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hi Miguel, sharing the concerns around the proposed Avila development at Lake Sharon and Oakmont.

- As we all know from living in a rental at one time or another, it is transient and not something thought of as “putting down roots”
 - We could be looking at over 800 people and at minimum 215 cars into this intersection – very congested!
- With the proximity to the elementary and junior high we have lots of kids walking
 - More traffic and a potential roundabout would be very dangerous
 - In addition, opening Rye Road will generate more traffic through Larkspur at Oakmont for those that do NOT reside there
 - Many of our residents use this area to exercise (running/walking) and the increased traffic & congestion will be dangerous, especially with a roundabout
- Continued loss of trees and green belts/space in our community
- Long term impacts to our home values

This is very concerning for our safety and quality of living in addition to the long-term questions about value for our community.

Bridget Petty

Director Operations | BCD Travel
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Miguel Inclan

Subject: RE: Zoning Change

From: David Burnett
Sent: Friday, February 5, 2021 8:36 PM
To: Helen-Eve Beadle <Helen-Eve.Beadle@cityofcorinth.com>
Subject: Zoning Change

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Ms Beadle

I want to register my opposition to the proposed zoning change for the Oakmont/Lake Sharon rental units. I really hate to see anything develop that property....but rental housing is definitely a NO from me.

Thanks for listening,
Dave Burnett
1703 Copper Leaf Dr, Corinth, TX 76210

--

Sent from Gmail Mobile

Miguel Inclan

From: Carol Leverett
Sent: Sunday, February 21, 2021 2:45 PM
To: Miguel Inclan
Subject: AGAINST

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

I am against the building of apartments next to the oakmont golf course! Please make my voice heard on this matter!

Sent from my iPhone

Miguel Inclan

From: Cayce Nelson
Sent: Monday, February 22, 2021 12:42 PM
To: Miguel Inclan
Subject: Lake Sharon new development

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Good afternoon,

My husband and I live in Cyprus Pointe Estates. I know the hearing is tonight about the new zoning for the development on Lake Sharon and Oakmont. We are extremely opposed to this future development and feel it is going to lower the value of our community. We are also concerned about the safety as we know rental properties tend to attract more crime. We feel this development would be a downgrade for our community. With the demand for new homes in our area I feel this land would be better utilized for more (for purchase) home communities which would increase the value of our community.

Thank you for your time and consideration.
Cayce Freedman

Miguel Inclan

From: [REDACTED]
Sent: Sunday, February 21, 2021 8:19 PM
To: Miguel Inclan
Subject: Avilla Fairways

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Good Evening Mr Inclan,

We have recently been made aware of plans to develop rental properties in our quiet neighborhood. One of the reasons we decided to live in and invest in a home in the Oakmont area was its quiet charm, low crime, and little vehicle traffic. Also, we appreciated the unlikelihood of a high density commercial property like this being erected near the golf course, elementary and middle schools. Allowing a rental development to be built in our backyard puts our home value, quality of the neighborhood and more abundantly the safety of the children in the area at risk.

I ask that you take a look at the big picture and listen to the concerns of the residents in the neighborhood. While this may seem like a chance to increase tax revenue it will likely only hurt the city with the drop in home values. Commercial rentals exist to make money primarily for the real estate developers, and brings down the overall value of a quiet livable community. Corners are cut, homes are not kept up with and the value of the area declines. We have lived in commercial rental properties many times.

There have always been issues with these types of properties, no matter what price point, location, or socio-economic status they cater to. The best commercial real estate is still high density housing and brings great concern when so close to an Elementary school. High density parking and the transient nature of commercial real estate is a common component in increased crime, including theft, vandalism and drugs. Not only that, it will be an eyesore blotting the current view of our neighborhood. Likely many current home owners will leave for other cities and neighborhoods as home values decline, impacting the beauty, safety and needed tax revenue to run the city going forward.

This area would better serve the community used as a park. It allows the natural elements around us to thrive and drives up the desirability to live in the area.

Regards,

Chris and Megan Kent

Miguel Inclan

From: Cindy Mackey
Sent: Monday, February 22, 2021 12:40 PM
To: Miguel Inclan
Subject: Lake Sharon and Oakmont

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hi Miguel,

I am writing in opposition to the multi-family complex proposed for the corner of Lake Sharon and Oakmont. Traffic on Oakmont is already heavy and if this is approved, will negatively impact us in that area. It will also take our property value down. No matter how nice the complex is, and no matter how you try to sell it, we all know this is true. Please do not allow this.

Thank you,
Cindy Mackey
1608 Cedar Elm Dr, Corinth, TX 76210

Miguel Inclan

From: Richardson, Cindy
Sent: Monday, February 22, 2021 8:57 AM
To: Miguel Inclan
Cc: Richardson, Cindy
Subject: Avila Fairways

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Good morning,

I wanted to reach out and share my concerns over this planned development. We moved to Corinth 5 years ago because it still felt like a small, quiet town that had some wild, untouched areas. Looking across the town today, there are a number of zone change requests for multi-family developments, and it appears that we are getting ready to grow the community significantly. This growth comes at the expense of the local wildlife, as well as the overall small town feeling that I have come to love.

I want to specifically share my concerns about the Avila Fairways project. Adding that many (215) houses in such a small area creates many issues for your existing citizens including:

- Overcrowding existing elementary and middle schools
- Increased traffic in an area that has few major roads and/or secondary roads
- Impact to the existing floodplain resulting from the work needed to drain/reroute the existing creek at the West end of Lake Sharon
- Loss of habitat for local wildlife, including birds-the outflow from lake Sharon is a prime feeding area for both Egrets and Herons
- Loss of the small town, quiet ambiance that I have come to love
- Reduced property values for homeowners
- Potential for increased crime-there are clear statistics related to an increase in crime as an increase in multi-family developments

I am sure you will hear similar things from other residents, this development is universally opposed by the impacted homeowners. I know it is unrealistic to expect this property will not be developed at all, but I would urge you to consider less damaging alternatives such as traditional single family homes. Honestly, even a development like the proposed Avila Fairways would be acceptable at a much lower density than the current plan. This may be the best win-win as it would allow Corinth to generate additional tax revenue while having a smaller negative impact on the community we all love.

Thanks for your support

CINDY RICHARDSON
SENIOR DIRECTOR
GLOBAL FUNCTIONAL LICENSEE SUPPORT

7-ELEVEN INC
OFFICE # 972-828-5558 CELL PHONE 775-771-5520

Certainty comes from believing we have learned all there is to know. Confidence comes from the effort to learn all we can. " - Madeline Albright

Miguel Inclan

From: Clif Clay
Sent: Monday, February 22, 2021 12:27 PM
To: Miguel Inclan
Subject: Avilla Fairways

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

City of Corinth,

I am writing to voice my opposition to the new development Avilla Fairways. My family lives across from the development on Ballycastle Ln. We moved here about 3 years ago because of the quiet feel of my neighborhood, the proximity to the good schools, and the close community feel of Corinth. We love the green space behind our house and we have been told, and made to feel like the is something that the city of Corinth takes pride in as well. I believe that putting in 200+ rental properties across from here will destroy the feel of the community completely. I don't believe that the community has the infrastructure nor does it comply with what the city has stated is important with their growth plans. 200+ more homes and over 400 new parking spaces will crowd our streets and make it unsafe for the schools across Oakmont Drive. The traffic this will create will not only be a nuisance but will be unsafe as well. It will also kill the quiet neighborhood we bought into which will hurt not only the feel of our community, but a big part of the investment we made 3 years ago. On the city of Corinth website it talks about its commitment to keeping Corinth Beautiful. One of those projects is tree planting. This project will wipe out a huge population of old trees. This will take a big part of nature with it. Again one of the things we love about this neighborhood is our backyard green space and the nature that comes with it. The birds, a lot of which you don't see much in surrounding areas. Also from what I have read about this community they like to talk about the average family staying for 2-3 years at these homes. That is not the type of community that will take pride in Corinth and the property. I am not opposed to new homes on that property. I am opposed to 200+ apartments that will slowly take away from the feel of our neighborhood and the since of community. On the Corinth Website it talks about the sense of listening to the community and how they feel about the growth of Corinth in a responsible way. If this passes it shows that those words have no meaning. Please take time to think about if this is the type of growth that we are looking for in Corinth and listen to all of our objections and act on our behalf.

Thank you,
Clif Clay

Miguel Inclan

From: cynthia geis
Sent: Monday, February 22, 2021 1:56 PM
To: Miguel Inclan
Subject: Agenda item B Lake Sharon/Oakmont rezoning proposal

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

To the Corinth City Council:

My name is Cynthia Geis I'm a resident of Cypress Pointe Estates. I'm am contacting you to ask you to please, please, NOT approve this rezoning proposal at the corner of Lake Sharon and Oakmont. This small area can not sustain or support a multi-family complex. Our property values will fall, congestion will be overwhelming and our two schools Hawk and Crownover absolutely do not have the capacity for that many additional families.

Thank you for your consideration
Cynthia Geis

Dale L. Walker

1105 Ballycastle Lane, Corinth, TX 76210
214-725-5310 dwsquareranch@yahoo.com

February 22, 2021

Mr. Miquel Inclan
Planner
City of Corinth
Via Email miquelinclan@cityofcorinth.com

RE: Avilla Fairways Planned Development Opposition

Dear Mr. Miquel Inclan,

I'm writing you today as a concerned resident in the "Larkspur II at Oakmont" subdivision immediately adjacent to the "planned development" noted above, to express my Strong Opposition. Please share with Town Council and Planning and Zoning Commission prior to tonight's 6 pm meeting.

The current zoning in place for the tract is appropriate, and should remain "as is", or "only ever" be revised/amended sensibly to insure neighborhood quality, integrity and continuity.

There are many reasons that will be shared by adjacent residents as to why this project is not a good idea. Very simply... this tract of land under consideration should emulate current surrounding land use to maintain a beautiful, and classy golf course community.

The Oakmont Property Owners Association (POA) is an appropriate moniker for the general area, a rental property development doesn't fit...Renters "are not" property owners!

My, and surrounding neighborhoods are completely opposed to the addition of rental properties that will cause even more traffic and safety problems already evident, create even more problems with schools that are near capacity, and destroy local wildlife habitat.

Traffic and safety of pedestrians are major areas of concern.

Traffic jams are already a problem on Oakmont Drive with school start and end times daily.

The safety of our children that currently walk to school cannot be minimized!

To members of the "Planning and Zoning Commission":

Don't lose your heads on this variance request, take a drive around..this project should not be jammed in a place it just doesn't belong!

While millions of dollars have likely been spent on the "Envision Corinth Comprehensive Plan" don't think for one second that you have to make "bad decisions" to satisfy the guiding principles of the plan!

Zoning Variances should be the Exception, Not the Rule....

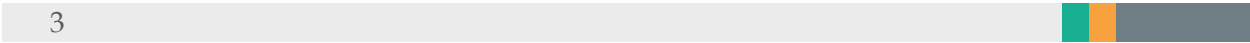
While **DENIAL of the proposed zoning variance is the only RIGHT decision...** should you consider approving this proposal you must strongly consider these adjacent homeowner necessary concessions to the plan:

- 1) Do not permit a planned development entrance off of Oakmont Drive
- 2) If Rye Road must be extended to Lake Sharon Drive, then make it an "emergency vehicle only" use with locked swing pole gate. Deny planned development resident access to Oakmont Country Club cart paths as a walking trail.
- 3) Mandate "high quality" Masonry 8 foot sound-proofing fence bordering golf course property preventing new planned development to be seen in full view as much as possible by affected homeowners.
- 4) Insure planned development parking lot lighting be at street level.

Finally, this zoning variance request is not appropriate, and it is not the right project for the parcel of land under consideration.

Sincerely,

Dale L. Walker



Miguel Inclan

From: Amanda Baxter
Sent: Monday, February 22, 2021 10:16 PM
To: Miguel Inclan
Subject: Proposed Apartments in Oakmont

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Mr. Inclan,

I am writing to voice my opinion about the proposed apartment complex to be built in Oakmont. I believe this would be a huge mistake and negatively impact our community and neighborhood. Oakmont is a community of beautiful homes and families. Property values in Oakmont will suffer with the addition of apartments. I ask that you consider the negative impact to our neighborhood and not allow apartments to be built. In neighboring communities the addition of apartments has dramatically lowered the value of homes and caused the neighborhood to be less desirable. Please keep Oakmont beautiful as a community of homes not rental property.

Sincerely,
Dan and Amanda Baxter

Miguel Inclan

From: Danielle Reillo
Sent: Monday, February 22, 2021 12:43 PM
To: Miguel Inclan
Subject: Oppose proposed zoning change of Lake Sharon and Oakmont

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hello Mr. Inclan,

I hope this email finds you well. I wanted to send a quick note stating my opposition of the proposed rental properties on Lake Sharon and Oakmont. Quite frankly, this is not the neighborhood for that type of development. This development would not only detract from the golf course, but also the surrounding neighborhoods. To be blunt, if these rentals were there when we were buying our house, we would NOT have bought in this area.

I think this past storm also proved the City has no business adding 200+ more residences when they can't even sustain the people who live here now.

Please don't detract from the beauty of our neighborhoods!

Sincerely,
Danielle

Miguel Inclan

From: David Burnett
Sent: Monday, February 22, 2021 1:44 PM
To: Miguel Inclan
Subject: Lake Sharon - Oakmont Hearing

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Miguel,

I come to you as a resident of 1703 Copper Leaf Drive. I speak as a former member of the Corinth Board of Adjustment and Economic Development Committee, as the former Executive Director of a Regional Planning Commission and 15 years as a Certified Economic Developer.

As a property owner and concerned citizen of the City of Corinth...I am in opposition to the development of rental units at the proposed site. The demand on all forms of infrastructure that will result from approval of this project will only exacerbate situations such as last week's winter emergency.

Further, I challenge the idea that the proposed project is the highest and best use of this property. Regardless of the tax revenue generated by this development, I contend that the residents of our neighborhood cannot afford the long-term impacts of a large rental project.

I respectfully request that your department exercise every lawful and proper measure to prevent the development of this project.

Sincerely,
David Burnett

--

Sent from Gmail Mobile

Miguel Inclan

From: Don Pajda
Sent: Wednesday, February 24, 2021 10:40 AM
To: Miguel Inclan
Subject: Avilla Fairways

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Though the Meeting has ended, I wish to give my opinion.

I attended the meeting on Monday, 2/22/21 regarding the planned development by NexMetro. However, I did not speak; but I do want to affirm that my wife and I are against changing the zoning and the building of 215 rental town homes.

Thank you,
D. Pajda and J. Pajda
1228 Ballycastle Ln, Corinth

Miguel Inclan

From: Doug Kirkley
Sent: Monday, February 22, 2021 1:02 PM
To: Miguel Inclan
Subject: Avilla Fairways

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Miguel,

My name is Doug Kirkley. I currently live at 1107 Ballycastle Lane, Corinth, Texas 76210.

My primary question to you and the people making this decision to allow these rental homes to be built across from a school and ruining property values to those that own home on the Oakmont CC holes # 12 and 13 is the following.

What does the City of Corinth getting from this deal and how will it benefit the existing citizens for these homes to be built.

How are you going to handle the additional infrastructure needs when we've just survived a horrible failure of that same infrastructure for the current citizens.

I can't believe you are even considering this!!!!!!!!!!!!!!

Again, what is the City receiving from this deal, what do the current citizens of Corinth get from this deal and what do the City Council members and City Administration get from this deal?

I'm quite sure the developer makes out like a bandit, literally!!!

Doug Kirkley
Property Owner
1107 Ballycastle Lane

Miguel Inclan

From: Sheila Jiang
Sent: Monday, February 22, 2021 1:34 PM
To: Miguel Inclan
Subject: Avilla Fairways

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Mr. Inclan,

We own two properties in Oakmont community. We just got a flyer yesterday regarding the rental development next to Hark Elementary. Just want to take the moment to express our concerns for the safety, quality of our community. We are going to tonight's meeting to voice our concerns as well. Please advise what else we can do to make sure the city managers are aware of many residents like us are totally against this proposal.

We would like to see a senior center or recreational center things of that nature to be built on the property.

Thank you for listening.

Kind Regards,

Doyle Hunt & Xiaojie Sheila Jiang

Miguel Inclan

From: E F
Sent: Monday, February 22, 2021 7:19 PM
To: Miguel Inclan
Subject: Opposed: Proposed zoning change PD-6 and PD-24

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

I am opposed to the proposed zoning changes for these sections for the following reasons:

- Inadequate infrastructure to support additional residential needs (recent events should highlight this shortfall.) This includes utilities, roadways, city services, and school enrollment/capacities.
- Lack of confidence in City to adequately plan, develop, and fund infrastructure to support these changes.
- Estimated increased tax revenues offset by increased need of infrastructure projects and City-provided services.
- Long-standing and dismal lack of effort by City and CEDC to fully explore commercial development in accordance with current zoning and within the City in general. Commercial development would not only provide tax revenue but also provide employment opportunities.
- Development inconsistent with existing residential properties and out of character within the immediate neighborhood.
- Proposed development will encroach upon and negatively impact natural features and waterways.
- Negative impact of concentrated rental properties within a single-family neighborhood on property values.
- Historical evidence of the impact of higher-density housing on criminal activity and other behaviors that strain city services (concentrations of exposed vehicles attract crimes of opportunity, etc)
- Turnover of residents in proposed development prevents 'community equity'.
- Developer's resident profile unlikely to be sustainable long-term.

Eric Fehrenbacher
1832 Vintage Dr
Coronh, TX

Miguel Inclan

From: ERIC JOHNSON
Sent: Sunday, February 21, 2021 8:55 PM
To: Miguel Inclan
Subject: Avilla Fairways

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Miguel,

I have reviewed the proposal and strongly oppose the implementation of the rentals due to the vicinity of the elementary and middle school.

Crime rates rise in high density housing areas. I've been a witness and victim in several crimes living in apartment complexes. I know these are townhomes, but they have the same characteristics. (Parking lots, high number of various types of people)

Criminals are attracted to these characteristics. They prefer parking lots over driveways and prefer victims who are less likely to pursue a crime in court.

Small properties draw in non-family oriented tenants. Why put them next to a school?

Rentals bring temporary tenants and high turnover. This becomes a challenge for police, teachers, and ESD workers to identify possible threats living and wandering near the school.

The school has minimal security. No fence, no cameras, and no trained security officers. Plus 100s of children pass through this area unattended as they walk to school.

I'm very proud of what you and the development staff have accomplished with the layout of Corinth so far and I moved here specifically because there were no rentals in the Hawk district. I know the completion of the neighborhood was likely interrupted during the downturn in the housing market. This is when the home builder sold it to Endeavor. The housing market is hot now, especially for high end homes, so Endeavor and Corinth should have plenty opportunity to upgrade rather than downgrade.

I would like you to continue with the original plan of Corinth and allow for a neighborhood of single family homes that match or exceed the homes in the neighborhood it connects to. This would complete the circle of neighborhoods around Hawk/Crownover school system. This creates a barrier of safety around the school. It is safe because it is stable, familiar, family oriented, and long term.

Endeavor and NexMetro just want money, but I hope you will do what's right for the community.

I've talked to several people in the surrounding neighborhood and they all share the same opinion. If the city isn't yet convinced, I would ask that you extend the approval of this proposal for at least a few weeks to receive more input. The people have been heavily burdened with school closings, power outages, water loss, and damaged houses.

I look forward to your response.

Section S, Item 7.

**Thank you,
Eric Johnson
940-395-8239**

Miguel Inclan

From: Frank slovacek
Sent: Monday, February 22, 2021 1:14 PM
To: Miguel Inclan
Subject: Avilla Fairway Planned Development

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Good afternoon Mr. Inclan,

My name is Frank Slovacek. My family and I are residents of Corinth, my home located in the Larkspur addition on Ardglass Trail.

I have seen the plans for the development of property located on the corner of Oakmont Dr and Lake Sharon, specifically the Avilla Fairway Development.

I am quite shocked that the city would even consider allowing this type of development in this immediate area due to the high density of the plan being proposed.

The developer is proposing to put (i made a rough guess) 200 dwellings on 25 acres, which i guess is at least twice, maybe 3 times the other households in the area.

This development is going to increase the traffic at an intersection that is used heavily by children going to school and will present a significant traffic danger in this area.

I really hope the city considers the negative impact this development will have on the safety of the neighborhood and schools in the area and economic impact this will have on existing residents.

I plan on attending the hearing this evening.

Best regards

Frank J. Slovacek
214-385-0196

Miguel Inclan

From: Frankie
Sent: Sunday, February 21, 2021 4:16 PM
To: Miguel Inclan
Subject: Avilla Fairways

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Mr. Inclan,

I write to you to inform you that I oppose changing the zoning for the property at Lake Sharon and Oakmont. The addition of 215 high density rental homes is not what we need at this time. The events of this past week have proven that in times of crisis, The City of Corinth and Oncor could not support the current residence, much less an additional 215 families. These rental homes bring no value to our city, the people renting have no stake in the community and it will bring property values down. Oakmont Country Club and the residence of Larkspur should be up in arms over this development since it affects them the most. If taxes are the driving force then Corinth needs to pursue another avenue that can bring in the tax revenue they are seeking, because this development is not the answer.

Thanks,
Frankie Sanford

Miguel Inclan

From: Smith,Fred
Sent: Monday, February 22, 2021 3:49 PM
To: Miguel Inclan
Subject: Avilla Fairways proposal

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Miguel,

I am a homeowner in Larkspur. I want to express dismay at the proposed zoning changes for Avilla Fairways. Corinth has always been know for its quality of life focused around single family housing.

At a time when there is a dramatic movement away from high density housing it makes no sense for Corinth to be moving in the wrong direction. Maybe a lower density townhome development with garages where there will be no curbside parking. Do not allow traffic to flow into Larkspur by connecting with the dead end Rye road. Our single family home neighborhood has only one entrance. Flooding traffic from this development into our neighborhood makes no logical sense. In fact because off the close proximity to our entrance and the Lake Sharon Drive and Oakmont Drive intersection there should be no allowed entrance from this proposed development flowing into Oakmont Drive. All access to the potential development should be off of the new Lake Sharon Drive extension. A roundabout at this intersection is absurd. Remember that within a couple of hundred yards we have an elementary school and a middle school with a large number of students walking to school. A roundabout would be a disaster from a safety point of view. A traffic light here makes more sense.



Fred Smith, CFP | Financial Advisor

Edward**Jones**
MAKING SENSE OF INVESTING

**1205 Bent Oaks Court
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Text the word "Connect" to **31268** to
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Miguel Inclan

From: Gabriel Silva
Sent: Sunday, February 21, 2021 7:56 PM
To: Miguel Inclan
Subject: Against Avilla Fairways Development!!

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

I'm writing to express our extreme disagreement with NexMetro proposal of building Avilla Fairways project. This is a high density commercial real estate development that will disrupt many thing in our beloved neighborhood which doesn't have the infrastructure to support a monstrosity like this.

We live in Larkspur at Oakmont Country Club, which is a residential, single-family, peaceful and quiet neighborhood. Avilla Fairways development will end all that and bring renters in-and-out that probably don't care about settling or being part of a long-term community.

Just take a look at the traffic in Oakmont Drive in the mornings and afternoons. Oakmont Drive is a two lane road and we already have traffic problems with parents dropping and picking up kids from Hawk Elementary... can you imagine what the traffic will be with at least 200-350 more cars that Avilla Fairways will bring?

Now, schools... can you imagine how overcrowded class rooms at Hawk Elementary and Crownover Middle School will be?

We built our forever home in Larkspur 5 years ago and paid a high-end residential property price tag for our lot because is adjacent to a private golf course. That said, and leaving aside how visually disrupting Avilla Fairways development will be, and the impact it will have on our home value... how exactly NexMetro/Avilla Fairways pretend to stop or control their tenants to roam freely on a private golf course or be a danger for the homeowners across the fairway with open backyards?? Do you think a "no trespass" sign will do?

Allso... Have you ran an environmental impact study yet? We have a range of wildlife living in that area that will be severely affected too.

You asked what we rather see being built there... single-family residential homes. Places for families to settle and be part of a community... not rentals where people come and go not interested on keeping things safe and beautiful.

Please stop NExtMetro and rule against Avilla Fairways project being built.

Gabriel Silva

Miguel Inclan

From: James Gilliland
Sent: Sunday, February 21, 2021 10:31 PM
To: Miguel Inclan
Cc: Evite
Subject: Avilla Fairway Project Corinth Texas

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hi Miguel,

I am writing you concerning the new development referred to as Avilla Fairways. I absolutely do not believe this is good for the city of Corinth.

1. As a new resident on Ballycastle I was not told of this potential development and would have been concerned and pause had I known it at the time of my decision to purchase the property. Rental development will have an impact on the property value of our home.
2. Rental property will bring in people who are not concerned about surrounding neighbors, if ordinances allow for College students to rent these properties the problems will be many. Rental property traditionally has a higher crime rate than owner property.
3. I have a copy of the survey and we are property 6 on Ballycastle and we are at 605 ft elevation and the end of the survey and the tee box of 14 are at 570. This makes the property a watershed for Lake Sharon, I would like to know about the environmental impact on Lake Sharon and the Marsh on Lake Sharon road.
4. On the survey it is apparent that the developer leaves an easy tie into the Lark Spur sub division via Rye Road. This is totally unacceptable as there is no need for any access for non-residents into the sub division and would dramatically increase crime, also driving down our property values.
5. The new development would put back yards of homes right on the 13th fairway on the Oakmont Golf Course. Our Home Owners Association has strict rules on the appearance and function of what our back yard looks like and there is no way renters will obey any mandates, why would they? There would be no enforceable consequences you could put on them.
6. There is no way the property owners of Hole 13 will not be able to not see the backyards and homes of the new development and vice versa which also dramatically reduces my privacy which is already somewhat compromised by golfers except on cold days and Mondays.
7. 214 Homes is a lot of people crowded into 127 acres. This has the potential to increase the population of the area by over 1,000 people in a ¼ mile area. This is urban blight and I'm not sure that has some Federal Implications I will explore. You are also asking the school districts to potentially absorb another 400+ students into the system over a 2 year period.

These are just a few concerns me and my fellow home owners are concerned about with this new development. If anything needs to be built there, which I don't think there should be, it should only be a handful of homes that are in the range of homes already facing the Golf course.

Thank you for your time in reading my concerns,

James Gilliland
 1300 BalleyCastle
 Corinth TX

Miguel Inclan

From: James Leverett
Sent: Sunday, February 21, 2021 3:32 PM
To: Miguel Inclan
Subject: AGAINST High Density Rental Propertirs

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

I am writing to express my extreme displeasure with the proposed build of High density rental properties adjacent to the Larkspur subdivision and Oakmont CC. Larkspur homes are high value properties due to quality of build, proximity to other high end properties, and location on a private golf course. The property values for all homes in the area of this proposal will decline. I like where we currently live but if this goes on through, we will probably sell and leave Corinth. I don't believe I am in the minority in my thoughts.

Sent from my iPhone

Miguel Inclan

From: Jason Freeman
Sent: Monday, February 22, 2021 7:51 PM
To: Miguel Inclan
Subject: Zoning Change

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hello,

I am emailing to state that I am AGAINST the zoning change at Lake Sharon and Oakmont.

Thank you,

Jason Freeman
1705 Bradford Ct
Corinth, TX 76210

Sent from my iPhone

Miguel Inclan

From: Jaz Uk
Sent: Sunday, February 21, 2021 5:37 PM
To: Miguel Inclan
Subject: Avilla Fairways

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Mr. Inclan,

My name is Jaz Uk, and I am a resident of Larkspur at Oakmont.

Larkspur is a charming, secluded neighborhood, and my neighbors and I pay a premium for its privacy. Building the proposed development will decrease the value of our homes and quality of life drastically. Also, the proposal of the Rye Road cut through alarms me. It will expose our serene environment to hundreds of questionable tenants. This raises concerns for the safety and the standard-of living in our community.

Instead of building a commercial rental property that would decrease the value of the surrounding neighborhoods, I propose a development that would increase value over time such as a park or a local grocery store like Trader Joe's. I acknowledge that Corinth is a desirable residential destination, but the lot would be better served by small local businesses that could serve and enhance their community.

I look forward to meeting with you at Corinth City Hall tomorrow.

Thank you,
Jaz Uk

P.S. Is Avilla Fairways the best they could come up with?

--
Jaz Uk

Miguel Inclan

From: Kelli Smith
Sent: Monday, February 22, 2021 10:58 AM
To: Miguel Inclan
Subject: Zoning in Oakmont

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hello there,

I am writing to express my concern over the proposed zoning changes to adding townhomes along Oakmont. My family and I moved here last year specifically for the community that it is and the excellent school district. My family and I are opposed to this because of many reasons.

There are many kids, including my own, that walk home from school. In increase in traffic would put them more in harms way.

The schools (Hawk and Crownover) are excellent!!! And they can not add more children into their schools. This would decrease how excellent they are.

This is a community of unity and love. Many walkers, runners, and families. It is a close community that loves the culture it has created.

Townhomes would change the culture and the dynamic that drew so many families here in the first place.

We love this community. My parents who also live here, love their community.

Please, we urge you to not support the 215 townhomes and the changes that would come with it.

Very concerned mother, father and community members,

Jeff and Kelli Smith

Sent from my iPhone

Miguel Inclan

From: Jeff Goethe
Sent: Monday, February 22, 2021 12:52 PM
To: Miguel Inclan
Subject: Avilla Fairways

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Council Members,

As a citizen of the City of Corinth and an Oakmont member, I am writing this letter in protest of allowing Avilla Fairways to be zoned for commercial multi-residential property. This would be a misdeed on the part of city council to allow this zoning to proceed based upon the following reasons:

1. Lack of infrastructure support by bringing on several hundred non-tax paying citizens which is a burden to our already stretched city services of whom we pay taxes for.
2. This development would add several students from non-tax paying families to our school systems which can diminish overall ratings of both Hawk & Crownover thereby detracting potential homebuyers to our community.
3. Creating a traffic situation to a main street of Oakmont especially during school drop off and pick up times.
4. No individual homeowner tax base to benefit the city with tax base revenue and increase property appraisal values.
5. Decreasing property values of neighboring homes that have negative consequences on future city tax revenue.
6. Eyesore to current taxpaying residences by having renters that do not value their property and are not controlled by HOA governance.
7. Devalue to Oakmont golf course as other ClubCorp properties do not have apartments adjacent to their golf courses.

My suggestion is to zone for patio homes to be placed on Avilla Fairways like the Pulte development off holes 16, 17, & 18. This could be maintained by the developer and would allow for the city to increase several tax paying citizens without heavy burden to our city services. This would also increase property values for Oakmont; thereby, attracting potential buyers to join our community. This direction would be better for our community and would have the support of your constituents.

Regards,

Jeff Goethe
1806 Timber Ridge Cir

Miguel Inclan

From: Jenn Kirkley
Sent: Monday, February 22, 2021 11:29 AM
To: Miguel Inclan
Subject: Avilla Fairways

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Curious what does the City of Corinth get out of this deal?????

Taking into account that you are not far sighted enough to see that this will drag down all property values in the surrounding areas, probably resulting in most also becoming rentals, higher crime, traffic, cost of infrastructure upgrade???

I am clearly not seeing the benefit to anyone????? The Developers..... sure....

Sooooo What else is going on here???

Jennifer Kirkley
1107 Ballycastle Ln
Corinth Texas 76210

Sent from my iPhone

Miguel Inclan

From: Jennifer Compton < >
Sent: Monday, February 22, 2021 9:57 AM
To: Miguel Inclan
Subject: Avilla Fairways

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Good morning,

I would like to add my response to the proposal for adding town homes in Corinth off of Oakmont golf course. We moved to Corinth from Denton to escape the hustle of the many people coming in with their massive expansion of apartments and new neighborhoods. Corinth offered the small town feel in the middle of suburbia. We have amazing schools that I felt comfortable enough to stop homeschooling and send my three children to. Bringing in town homes will disrupt this quiet small town feel. I most definitely do not agree with these type housing communities (even if it is done in a classy upscale way) being built so close to where my children go to school and sometimes walk the main roads home. Lake Sharon has turned into a beautiful road since it's expansion. I would hate to see all of that beautiful nature be displaced all for expanding our community. No one wants to see Corinth turn into another Denton, Lewisville or Flower Mound. We are part of the quiet Lake Cities and as a resident here, I would like to ensure it will remain a quiet town with low crime rates and a tight knit community. If you would like to expand and build something at that lot, how about we discuss our own town library.

Thank you for you time.

Jennifer Compton
Northwood resident of 5 years.

Miguel Inclan

From: Jennifer Hall
Sent: Saturday, February 13, 2021 5:12 PM
To: Miguel Inclan
Subject: Proposed apartment complex

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hello,

I am writing to voice my concern about the proposed apartment complex on Lake Sharon and Oakmont. There are many animals that live there and around the area. It would be a shame to encroach upon their space. Not to mention the amount of trees and that will need to be cleared. The trees and natural areas around the neighborhood is what gives it a nice feeling here.

I'm also concerned about the amount of traffic this complex will bring - very near Hawk and Crownover. I wouldn't feel comfortable letting my daughter walk to school with that level of traffic around. The school is the reason we chose to move here to begin with. I also am worried about the class sizes increasing.

Which brings me to my next concern - property value. I believe the surrounding area's homes value will degrade with the addition of the apartment buildings. Regardless if they're "luxury" or limited to one story.

My husband and I plan on joining the meeting on 2/22 virtually. Thank you for taking the time to read my email.

Best wishes,
Jennifer Hall

Miguel Inclan

From: Jessica Hughes
Sent: Monday, February 22, 2021 4:05 PM
To: Miguel Inclan
Subject: Oakmont subdivision

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hi Miguel-

I am aware that there is a meeting tonight for Planning and Zoning of the proposed rental subdivision going in on Oakmont Drive. I live in the Larkspur section of Oakmont that is directly across the golf course from the proposed location of the new subdivision. I strongly oppose the approval of this zone change. I believe that it will drive down my property value.

Thank you for listening.

Jessica Hughes
hughesjr26@gmail.com

Miguel Inclan

From: Kate Bourns
Sent: Monday, February 22, 2021 8:22 PM
To: Miguel Inclan
Subject: Avilla Fairways Development Proposal

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Mr. Inclan,

We wanted to take the opportunity to express our extreme opposition for the proposed development on the corner of Lake Sharon and Oakmont Dr. We take so much pride living in the prestigious and beautiful Oakmont area. We have worked hard to afford to live here and feel that offering rental homes will be such a detriment to this area. Oakmont Road will be too congested, our schools are at capacity, safety will be a huge concern, there is no need for Rye road connecting to the Lark Spur neighborhood, it will lower our property values, and the list goes on.

Please consider all of the concerns voiced by the residents of this community who are very much against this proposal.

Thank you for your time and consideration,

Kate and Jon Bourns
1830 Vintage Dr.
Corinth

Miguel Inclan

From: Kelly Mears
Sent: Monday, February 22, 2021 5:45 PM
To: Miguel Inclan
Subject: Avilla Fairways

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Mr. Inclan,

I am writing to express our concern about the proposed development of rental townhomes on Lake Sharon and Oakmont Dr. We have concerns about how such a development would impact the safety of children walking to and from Hawk Elementary and Crownover Middle Schools. Parents and concerned citizens have been asking for a crossing guard at that intersection for quite some time, and with a new development, traffic would be much worse in that area. It would further compromise the quiet neighborhood atmosphere that we currently enjoy. In addition, we feel that a rental development would diminish the property values in our Oakmont neighborhood.

Sincerely,

John and Kelly Mears
1512 Shadow Crest Dr.
Corinth, TX 76210

Miguel Inclan

From: Justin Reed
Sent: Monday, February 22, 2021 6:10 PM
To: Miguel Inclan
Subject: Comments on Zoning Change (Avilla Fairways)

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Mr. Inclan:

Please review my comments below regarding the proposed zoning change for NexMetro's proposed community, Avilla Fairways.

I have been apprised that there are plans to develop approximately 24 acres of undeveloped green space adjoining Oakmont golf course & several communities with densely-packed rental units. I have reviewed the websites and photos of NexMetro/Avilla communities and I can see that they have a unique appeal and do a fine job in developing their properties. Unfortunately, I do not share the vision nor see the attractiveness of a complex such as this in our community.

In reviewing the guiding principles for Envision Corinth 2040, the vision of this Avilla community does not seem to align with what was stated.

--"A Dynamic and Aesthetically Pleasing Community": while the Avilla community may appeal to multiple generations, it will negatively-change the landscape of the environment along the Oakmont golf course and surrounding communities. A packed set of rentals among single-family owned properties will not be appreciated by citizens who have significantly invested in their homes and property. The Avilla design has dumpster locations along Oakmont Dr, the pool across from a church parking lot, and a dog park within a few hundred feet of the golf green. Additionally, individuals will generally value a rental property less than their personally-owned homes; the long-term effect may not be desirable relative to nearby valued and individual-owned properties.

--"Complete, Connected, and Safe Neighborhoods": our home is located in the adjoining Larkspur community (via Rye Rd). I love the safety of our neighborhood as there is minimal traffic at this time as there is no through traffic. Additionally, my understanding is the nearby elementary/middle schools are already at capacity and may not support or be able to provide a place for children in a new community (are there plans to add more schools to compensate for the new growth?). I feel that this compromises the ability to provide quality goods & services, including education, in this portion of the community, thereby, making it less complete/connected.

--"Future Infill Development": this development is not context appropriate and it is not compatible with existing adjacent development. It also seemingly doesn't take "... into consideration the concerns of the current residents..." and "...retain long-term value in Corinth".

My family has lived in Oakmont since 2015 and I have lived in Denton County for the majority of my life. I grew up going to church camp at Lake Sharon when there was almost nothing in the immediate area. I am not opposed to progress but I believe our community and officials should be mindful of the existing citizens' values and the beauty of the green spaces, waterways, and landscape. Clear-cutting a majority of trees along a golf course and building packed-in garden homes amongst an area with single-family homes doesn't seem compatible or aligned with what makes the Oakmont community appealing to so many families. Changing this existing environment is a one-time event and cannot be undone afterwards.

In the past year, I have evaluated at least two businesses to purchase and even considered starting one but opportunities would require a significant compromise what I value in my career. I remind myself that opportunities are like trains; there will another one that's coming shortly. I hope that the City of Corinth officials will keep this in mind and not compromise their guiding principles to making our community valued for years to come.

Thank you for the opportunity to share my comments on this matter. It is fortunate that we are discussing promising growth of our community rather than a less-fortunate alternative. I look forward to see how Corinth proceeds into the future.

Justin Reed

Miguel Inclan

From: Karen Field ·
Sent: Monday, February 22, 2021 10:54 AM
To: Miguel Inclan
Subject: Housing projects of Corinth

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

To whom it my concern,

Who in their right mind would consider a rental development in the heart of Corinth? Seriously. Near a highly regarded elementary school?

I am all for affordable housing but why this location? We pay more per square foot for our home than any other part of the city. And this is the thanks we get. We didn't pay for a golf course lot only to have the wooded areas disappear. Why wasn't this mentioned when Phase II of Larkspur was being developed? Maybe it was different city officials than who actually cared about our roads and schools? What not use the land to build nice homes worthy of being on a golf course. Not a 215 rental town community. A few nice homes ...

Have you taken into account the added traffic? The need for additional crossing guards for the increase in kids walking to schools? With the Lake Sharon extension completed, the Oakmont/Lake Sharon intersection is already dangerous for kids walking home from elementary and middle school. The other two crossing guards work intersections with one lane of traffic each way. Lake Sharon is a four lane road with a turn lane ... and no crossing guard. I pray no child is hit by a car because someone wanted to make more money. Heck, why did you turn down Bucees years ago? Maybe with the increase tax revenue you can hire more police to be crossing guards and also patrol this project that no one wants.

Has the study been completed to determine if there would be an entrance off of Oakmont? What were the findings? I see on the map you've included this entrance. We in Larkspur already have a hard enough time turning left onto Oakmont in the mornings between 7-8:15 a.m. But I guess you really just don't care.

And while I am in favor of Rye Road being extended, that just gives direct access of these future tenants to Larkspur. You've got to be kidding me.

If this goes through, I will lead charge to make sure no one on the current city council is re-elected. Unfortunately, the real damage will have already been done. Lantana is looking pretty good!

Karen Steger
1307 Ardglass Trail

Miguel Inclan

From: Karl Jobst
Sent: Sunday, February 21, 2021 3:54 PM
To: Miguel Inclan
Subject: Rental development next to hawk elementary.

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

I am opposed to this development. I live in the larkspur neighborhood of Oakmont country club. The traffic is already over congested in the mornings, and this development will have a negative impact on the current value of my home. I would be more open to single family homes but not rental properties.

I vote no. My wife also votes no.

Karl & Lisa Jobst
2203 Valderamma In Corinth tx.

Sent from [Mail](#) for Windows 10

Miguel Inclan

From: Kathleen M. Craven
Sent: Monday, February 22, 2021 11:56 AM
To: Miguel Inclan
Subject: Oakmont Proposed housing

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

I am a resident in the Oakmont area and would like to voice my opinion on the new proposal of rental housing on Oakmont Drive.

One of the many reasons we bought our home in Corinth was the value and look of the area. While we enjoy the beauty of Oakmont, the most important to us was safety and the quality of schools and at this point, all the reasons we chose Oakmont are at a risk. We walk our neighborhood a lot and the treed and open areas that are left are what keep our area beautiful and unique. I cannot think of a single reason that the new rental homes would be a good thing for our community.

Though we do not live on the golf course, I cannot imagine how the course will look over the years of added people that would have access to the greens with no invested interest in keeping it beautiful.

I am not foolish to think that this land would not ever be developed.

We all know that it's beautiful property. Is there no other option for us? For example, why not solicit a company to build zero lot property there instead? There would be pride of ownership which makes for a much better neighbor than people who rent for a year or two and then they are gone.

I am invested in our area. I own a home and appreciate all that comes with it. I enjoy the safety, I enjoy the look, I enjoy the golf course and as stated, I love the open fields and trees that are left.

I would like to leave it as it is but if I am putting a final opinion, my response is NO to RENTAL homes in Oakmont.

Kathleen

--
Kathleen M. Craven
940.368.2885

Miguel Inclan

From: Katie Beth Bruxvoort
Sent: Monday, February 22, 2021 10:54 AM
To: Miguel Inclan
Subject: Avilla Fairways

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Mr Inclan,

I am writing to express my concern over the new proposed development on Lake Sharon. As a resident of Oakmont living on Bally Castle Ln, this greatly impacts me. My children attend Hawk Elementary and we walk to and from school every day along with hundreds of other children. Many children from Crownover and Hawk walk home unattended every single day. In my opinion the proposed development will make this walk unsafe for the children at Hawk Elementary and Crownover middle school. I would encourage you and the city council to drive down Oakmont Dr while school is being let out. What you will see is hundreds of children walking home and hundreds of cars lined up along Oakmont. If you were to drive down Oakmont when children are being dropped off for school in the mornings you would see much the same scene but added to that, people trying to navigate the long lines of cars and children walking to school and get to work. Adding 215 more homes along this area will only exacerbate the traffic issue at pick up and drop off times, but also increase the risk that there is a traffic accident involving a child pedestrian. These are not risks as a homeowner and mother that I am comfortable with. Beyond the traffic issue for school children, there is also the issue of where the children living in these homes will go to school. Hawk and Crownover are already operating at or near capacity, and I do not see how they can serve this many new families.

Another concern I have is with the proposed cut through on Rye Rd. This will create high levels of through traffic on Bally Castle Ln. Our neighborhood is one where kids can play safely in front of their houses. There are several homes on Bally Castle that have small children living in them. If traffic is increased on our street, they will no longer be safe to play in their front yards with their friends, play basketball in the driveways, do sidewalk chalk on the sidewalks, all things children love to do and deserve to do safely. We specifically chose this neighborhood and this street because it was not a busy street and because the neighborhood had a single entrance. We feel like that is important for the safety of our children and this proposal completely changes that.

Furthermore, we purchased our home knowing there was undeveloped land adjacent to our neighborhood, but with comfort in the knowledge that it was zoned for a planned development of single family homes, not multi-family use. Real estate market data shows that multi-family zoning and busy streets can significantly negatively impact the value of nearby homes. Residents of this neighborhood have made significant financial investments into their homes. Oakmont is considered to be one of the best neighborhoods in Corinth to live in, particularly because of the beautiful trees and the quiet streets with minimal entrances. This proposed community of rental townhomes will completely change the this.

What drew us to Corinth when we moved here a few years ago was that Corinth had a reputation as a small town feel with big city access. We heard such great things about how the people of Corinth took great pride in their homes, and how the city council was wanting to make strategic changes that allowed for healthy growth while making Corinth feel even more like a community. (ie. projects such as The Commons at Agora). This planned zoning change and development is the complete opposite of that. I keep seeing the messaging "preserving the unique character of the community" and in my opinion the Avilla Fairways Planned Development will destroy the uniqueness of Oakmont and

Corinth, and moving forward with the zoning change would be a violation of the trust the current citizens have placed in the leadership of the City of Corinth.

Section S, Item 7.

Thank you for your time,
Katie Beth Bruxvoort

Sent from my iPhone

Miguel Inclan

From: Kristine Nader
Sent: Monday, February 22, 2021 2:31 PM
To: Miguel Inclan
Cc: Richard Nader
Subject: Against rezoning near Hawk Elem

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hello Miguel,

I'd like to lend my voice to those opposing the rezoning of land near Hawk to multi-family apartment housing.

I live in Larkspur in Corinth, and our property values, and also the children walking or being driven to Hawk or Crownover, will all be negatively impacted by such a development.

The City owes it to the existing citizens of Corinth to operate in our best interest. This development is not in the best interest of those of us who already call Corinth our home.

You need to do lists of impact studies on pedestrian safety, traffic safety, property value preservation, and once you do logic tells us this development will not be a plus to the citizens of Corinth.

Do the right thing and stop trying to shove something with so little support through at a time when Texans, including us in Corinth, are suffering with the deep freeze fallout on our properties. Also, to do it now when you can hide behind restrictions of gathering due to Covid, shows all of us your desire to slide this re-zoning under the radar and under our noses. I live probably half a mile from the proposed site and found out yesterday about the project.

Shame on the City of Corinth, once again ! Start working for the citizens you already have that chose to come here for the quality of life this neighborhood, and stop trying to destroy and cannibalize what's already in existence, for dollars or your bottom line or some misguided liberal sense of fairness that you are trying to shove on property owners who pay a premium to have the quality of life we bought here.

Kristine Nader
2209 Valderamma Lane
Corinth, Texas 76210
Sent from my iPhone

Miguel Inclan

From: Laura Trevino
Sent: Sunday, February 21, 2021 8:34 PM
To: Miguel Inclan
Subject: Lake Sharon new rental development

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Good evening,

Like many of my neighbors, I write to you tonight with the hope that you will listen to our concerns and not move forward with the new rental community on lake Sharon rd. We do not want to have a rental community near our school and our beautiful golf course. Our schools cannot handle more enrollment and the traffic would be dangerous to the small community. We're already been deeply impacted by the extension of the road and the new housing development across lake Sharon. Please do not approve, we want to keep the safe community feel to our home.

Thank you,
Laura Trevino
(Lake Sharon Estates Resident)

Sent from my iPhone

Miguel Inclan

From: Lesa Claycomb
Sent: Monday, February 22, 2021 4:54 PM
To: Miguel Inclan
Subject: PROTEST PD-6 Planned Development Concept Plan for Lake Sharon/Oakmont Drive

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Planning & Zoning Committee, Mr. Miguel Inclan, Corinth City Council & Citizens of Corinth,

I am writing this correspondence to state my Opposition to the proposed zoning change of the property generally located at the NW corner of Lake Sharon Dr. & Oakmont Dr. east of FM 2499.

Furthermore, I oppose the planned development concept plan to develop 24.595 acres of land by NexMetro Communities, on behalf of owner Endeavor Energy Resources, LP., to change the zoned usage to Multi-family residential.

This requested zoning change is not aligned with the original PD-6 planned development district usage, that allows 2-family garden homes, townhomes, & retail shopping.

Furthermore, allowing this zoning request and development by NexMetro Communities would have a significant negative impact on the existing community & wetlands.

Two areas of significant safety concern:

1. The City of Corinth's water treatment facility is located next to the proposed development, at a lower elevation to the proposed development. Lake Sharon, and its tributary, is a documented floodplain by FEMA. IN the event of flooding, this area would have a greater probability for soil, subsoil, or flooding conditions & create public health or safety hazards. Allowing development in this area could lead to urban flooding, affecting the homes and lives of residents of Corinth.

I urge you to read "The Affects of Urban Development on Floods", link attached. This could happen here. FEMA has already identified in Corinth an area very close to this proposed development where urban flooding could happen. FEMA states on the map(link),

The Lynchburg Creek watershed is experiencing both moderate residential and commercial growth as developments fill in areas that were previously used for agriculture. The Boulevard Apartment complex and the Meadow Oaks subdivision south of Lake Sharon Drive have grown exponentially since 2001. Flows have potentially increased due to increased impervious surfaces, which increase the likelihood and severity of flooding. Mitigation strategies put in place before development occurs could limit the potential impact of flooding."

Link to FEMA Lynchburg Creek Floodplain:

https://map1.msc.fema.gov/data/FRP/FRM_NCTCOG_FY15_2017_1031.pdf?LOC=edd7adc34a70836922b996a151ca397f

Article: "The Affects of Urban Development on Floods"

[Effects of Urban Development on Floods](#)



Effects of Urban Development on Floods

USGS Washington Water Science Center
Effects of Urban Development on Floods

If NexMetro was to proceed with development, the potential for watershed coming from the developed land, would firstly go to Corinth's water treatment facility, then to the neighborhood homes adjacent to Lake Sharon. Secondly, more watershed would empty into a small tributary at the base of the proposed development. This area is a historical nesting site for migratory egrets, as well as whooping cranes, and other yet identified wildlife that would be negatively impacted.

I urge the City of Corinth planners to first investigate the impact developing this land will have on our city's safety, health, infrastructure and wetlands.

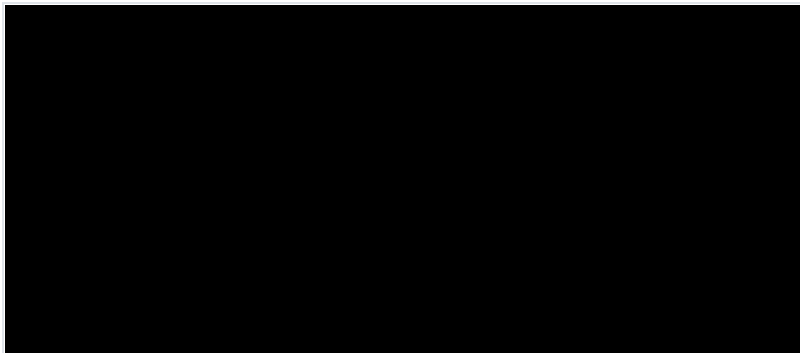
Fish and Wildlife Service, Congressional and Legislative Affairs

Fish and Wildlife Service, Congressional and Legislative Affairs

U.S. Fish and Wildlife Service

Web site of the U.S. Fish and Wildlife Service

"The Migratory Bird Treaty Act of 1918 (MBTA) prohibits taking, attempting to take, capturing, killing, selling/purchasing, possessing, transporting, and importing of migratory birds (including ground-nesting species), their eggs, parts and nests, except when specifically authorized by the Department of the Interior. This would include prohibiting harassment of nesting birds and young during the breeding season. In addition, the ESA and state law protect migratory birds that are listed as endangered or threatened. TPWD recommends that the bird species that use the project area be identified and best management practices for avoiding harassment and harm to migratory birds be implemented. In accordance with the MBTA, TPWD recommends that vegetation removal and ground disturbing activities be phased to occur outside of the nesting season (March 15 to September 15) and impacts to spring and fall migrants be avoided. Construction noise that could harass nesting birds should be phased to occur outside of the nesting season as well. Additional information regarding the MBTA may be obtained through the USFWS Region 2 Migratory Bird Permit Office at (505) 248-7882 or online at [U.S. Fish & Wildlife Service - Migratory Bird Program | Conserving America's Birds](#)"



U.S. Fish & Wildlife Service - Migratory Bird Program | Conserving Ameri...

2. The second and most important point I wish to bring to your attention is the safety of the children in our schools and neighborhood.

NexMetro's proposed development is within 400 ft. of an elementary school and next to that a middle school. Combined,

these schools house 1,442 students. Of which, mostly are walkers considered "walkers". Meaning they are too close to school to receive bus services. Additionally, these schools do not bus many students. Crownover has only 5 bus routes, and Hawk has 4. The impact of additional cars using our residential streets would potentially affect the safety of our student walkers.

When listening to NexMetro's representative speak at an earlier meeting in January, he stated, "We rent to whomever passes our background check". Most apartment complexes will rent to those who have past sexual offenses. Many do not discriminate, as the representative stated that night, whomever passed their test.

NexMetro wants us to believe them, offering that their residents are different than most apartment complexes. This isn't the case. Perhaps in Arizona or Frisco their residents are a certain demographic, i.e. empty nesters, professionals, etc. In reality, our demographics will include college students who pass the background check thanks to paying parents. We are bordered by two colleges, we will have an issue with college students renting these apartments. Although the NexMetro rep has all the faith in his property managers, I do not.

I don't trust their own demographics, their own background checks, and I especially don't expect them to do their due diligence in preserving our children's safety, our city's water treatment facility, our community's floodplains and wetlands.

Please pause and reflect on these concerns, I ask that you take time to consider my humble request to deny the zoning change.

Thank you so much,
Lesa Francis-Claycomb

Miguel Inclan

From: Lynda Bradley
Sent: Monday, February 22, 2021 2:10 PM
To: Miguel Inclan
Subject: Avilla Fairways Planned Development

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

To the Planning and Development Department

Subject: Avilla Fairways Planned Development

The anticipated occupancy of this development is very disconcerting. Oakmont Drive is very busy with the traffic of two large schools. Mildred Hawk Elementary and Crownover Middle School are directly in the traffic zone of this plan. These two schools create a lot of automobile, truck and very importantly, walkers, accessing Oakmont Drive-Lake Sharon routes. Now the traffic, entering and exiting the complex will add to the existing congestion at the most commonly used times of the day. Having an entrance on Oakmont drive will create additional hazardous conditions for its current users.

The fine print of the Avilla Fairways concept lists 1, 2 and 3 bedroom units. This does not lend itself to senior living accommodations since seniors mostly desire one story housing. With the easy access to two schools, plus Guyer High School, requiring additional motor traffic, this development is clearly designed for multi-family use. The parking, alone, will create a messy eyesore with the numerous slots needed for residents and commercial use. Most units will need to accommodate at least two cars, with a possibility of three to four cars.

Furthermore, there are more desirable uses for the land. Ideally, Corinth could consider additional beautification for the enjoyment of our city’s residents. Lighted walking and biking paths would be a much-desired attraction, with a safer use of the property. It would be an uplifting and enriching addition to Corinth, to have a green area adjacent to the commercial development which is coming along FM2499.

Hazardous conditions will worsen on Oakmont Drive and Lake Sharon Road with the increase that high density two to three bedroom and storied units that Avilla Fairways will bring. Access to the complex from Oakmont Drive, which is heavily used by motor and foot traffic and across from two large schools, creates additional hazards. Hopefully, the Corinth Zoning and Development commissioners will be giving some merit to a more positive use of this land. Please consider the enhancement of Corinth, rather than the opposite.

Lynda Bradley
1404 Ballycastle Lane
Corinth, TX 76210
940-765-2369

Miguel Inclan

From: Lindsey Anderson 1>
Sent: Sunday, February 21, 2021 1:42 PM
To: Miguel Inclan
Subject: Opposition to Avilla Fairways (NexMetro Communities) plan

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

To whom it may concern,

It has come to my attention there is a rezoning request from NexMetro Communities to build Two Family Garden Homes, Townhomes, and Neighborhood Shopping at the northwest corner of Lake Sharon Drive and Oakmont Drive.

As a homeowner in the Cypress Point Estates neighborhood, I would like formally submit my opposition to this plan.

Two family homes, townhomes, etc. are not conducive to this area. Oakmont drive has historically been overcrowded with traffic especially during peak commuting times. Hawk Elementary and Crownover Middle School are less than 1 mile away from the proposed community which will significantly increase the amount of traffic in the area creating potentially hazardous conditions for school aged children. Additionally, the current 2 lane road on Oakmont is not sufficient to support the increase in traffic, the current 4 way stop at Lake Sharon and Oakmont will become an unsafe intersection with such a high volume of cars using it daily, and the intersection of Lake Sharon and 2499 will be extremely dangerous without proper traffic control.

Additionally, a quick search on local apps such as Nextdoor, Ring, etc. will shed light on the number of car break ins that occur. The proposed community has plans for covered parking which by design increases the opportunity and risk of crime.

Thank you,
Lindsey Jensen

Miguel Inclan

From: Madison Uk
Sent: Sunday, February 21, 2021 4:44 PM
To: Miguel Inclan
Subject: Avilla Fairways

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hi Mr. Inclan,

My name is Madison Uk and I am a resident of Larkspur neighborhood. I'm contacting you to express my concerns about Avilla Fairways. I'm worried that the location of this rental property will decrease the value of my community and the quality of life that I enjoy. My neighborhood is small and quiet, free from cut-through traffic. If the zoning proposal and property layout is approved, my neighborhood would lose its quiet charm and seclusion from a busy street. I especially have concern for the proposed Rye Road. If I've understood the plans correctly, this street would serve as a way for people to cut through our neighborhood unnecessarily. It could result in possible safety concerns for families and residents with young kids and pets.

I also have some concerns about how the townhomes will impact the beauty of my neighborhood. We have a lovely golf course backed up to a mostly wooded area. It's much more pleasing to see woods and nature rather than townhomes in my backyard.

Thank you for taking my comments on this proposal and I hope this message will make clear that I and many of my neighbors are against Avilla Fairways.

Thank you,
Madison Uk

Miguel Inclan

From: Mary Bettes
Sent: Sunday, February 21, 2021 10:42 PM
To: Miguel Inclan
Subject: Avilla Fairway Apartment Project

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Miguel,

I am writing you to voice our deep concerns about the Avilla Fairways housing project. Succinctly, our concerns center on three areas:

1. Safety of school children with two schools less than 500 yards from the project.
2. Increased traffic hazards and congestion
3. Quality of life and security of nearby residents due to the inherent transient nature of apartment tenants

In 32 years of law enforcement and 28 years as an FBI Special Agent, I have lived in numerous cities where these type projects have been built. In almost every case, these types of complexes are **not** built in an existing residential neighborhood, but instead in a commercially zoned district. These complexes are by nature occupied by residents who sign short term leases, thus creating a transient culture in an otherwise stable community.

With 215 apartments being proposed by the developers, this would conservatively add an additional 300 vehicles to the immediate proximity of the schools.

We have been residents of the Oakmont area for almost 30 years. This is a family-based community that has a low percentage of resident turnover. In our opinion, to grant this zoning in this location would be reckless and send a message that our city leaders do not have the best interest of the Corinth family community at heart.

Sincerely,

Robert & Mary Bettes
1004 Ballycastle Lane
Corinth, Texas 76210
Cell: 214.558.6290
Email: RNB417@msn.com

Miguel Inclan

From:
Sent: Monday, February 22, 2021 10:23 AM
To: Miguel Inclan
Cc:
Subject: Avilla Fairways Planned Development

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Mr. Inclan,

As a resident of the neighboring houses to the planned Avilla Fairways development, I have several concerns I would like to communicate to you and the board.

I relocated in retirement to Corinth because of the rolling hills, heavily treed properties, parks, minimal traffic area and minimal high density housing. I also pay extremely high taxes for the luxury of these amenities. I am concerned those amenities are being jeopardized.

Upon review of the proposed plot of the Avilla Fairways development I have the following concerns:

1. The density of the proposed housing will eliminate much if not all of the natural and MATURE landscaping which is now beautiful and a natural sound barrier to the new and additional traffic on Lake Sharon Road. The removal of so many MATURE trees for buildings and parking lot feels unacceptable as it takes generations to regrow those resources.
2. With the opening of the extension of the Lake Sharon Road, there is already more traffic. On multiple occasions I have seen drivers fly through the stop signs. There is no doubt the impact of the planned develop will see additional traffic detrimental to the safety of the local drivers. One also has to presume there will be a light needed at 2499.
3. If you have ever tried to maneuver through the school drop off and pick up traffic on Oakmont Drive you certainly cannot be accepting of adding an ingress/egress on Oakmont let alone the increase traffic that will result. It is difficult enough to by pass the existing line of vehicles to try and get to work or to home if you are using Ardglass.

My recommendations;

1. Please do not rezone to multifamily housing.
2. If rezoning goes through, reduce the density of the units by 40%.
3. Increase the landscaping requirements to retain 60% of the existing trees.
4. Increase the set back from the golf course.
5. Delete the ingress/egress on Oakmont.
6. Require a traffic light at Oakmont/Lake Sharon.

Thank you for the time to read these comments.

Sincerely,

Maureen Underwood



Virus-free. www.avast.com

Miguel Inclan

From: Michael Hunt
Sent: Monday, February 22, 2021 3:01 PM
To: Miguel Inclan
Subject: I OPPOSE THE AVILLA FAIRWAYS DEVELOPMENT

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Mr. Inclan,

I have lived in Larkspur at Oakmont for 6 years now. I love my neighborhood and its secluded nature, it was the primary reason I bought the property and moved here from Highland Village.

I am a member of the Oakmont golf Club and walk/hike thru out the area on a regular basis. The area is a quiet, safe area where numerous families and children feel safe to walk and play.

I OPPOSE THE AVILLA FAIRWAYS DEVELOPMENT for a number of reasons:

1. The increased traffic it will generate in a low traffic residential area that houses 2 schools where most of the students walk to and from school.
2. It will introduce an additional entryway into our neighborhood making the neighborhood more susceptible to crime. (Like a number of the other subdivisions in Oakmont with multiple entryways and exits.)
3. It will change the "visible characteristics" of the surrounding area.
4. It would set a precedent for establishing a single area of rental properties surrounded by single family homes/homeowners. There are many other areas within the Corinth city limits where rental properties can be built without impacting an existing single family home neighborhood.

How would you feel about this type of residential area being put in your backyard?

Thank you and please do the right thing for the residents of Corinth,

Michael Hunt
Larkspur at Oakmont Subdivision
1309 Ballycastle Lane
Corinth, TX 76210

Miguel Inclan

From: Michelle del Carpio <Michelle.delCarpio@cityofcorinth.com>
Sent: Monday, February 22, 2021 12:58 PM
To: Miguel Inclan
Subject: Avilla Fairways - OPPOSED

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

To whom it may concern:

I am vehemently opposed to the proposed rental residences on Lake Sharon and Oakmont. The absence of this type of residence is exactly the reason we moved to Corinth 17 years ago and we will be moving out of Corinth if this is the type of development that is now being supported here. Corinth residents DO NOT WANT rental developments in our area. This has been addressed time and time again and every time, the residents have been opposed.

After this past week, it is very clear that the current infrastructure cannot handle this type of development in this area. Adding over 200 homes is simply irresponsible without fixing what is currently wrong first. We already have major drainage issues and now we have become aware of water supply issues.

Adding this type of development will also increase the need for additional police presence as it is clear that crime in the surrounding area (Denton) is now coming into our Corinth area. By adding these rental homes you will be inviting renters and people involved in crime that do not care about their city and have no loyalty to their neighbors to live amongst your long time citizens. Renters DO NOT CARE about the property they are renting. Statistics show that crime is higher in rental properties. The residents in the golf course area did not buy half a million dollar homes to have a rental property built right next door. This will drive property values down and will result in many of your long time residents to lose money on their property. I know many that have already contacted realtors and are ready to turn their back on the city that seems to be turning its back on us.

Additionally, putting this so close to a school will increase traffic along Oakmont and there are a lot of children walking to and from school in the morning and afternoon. Right now it is reduced due to the number of children on distance learning due to Covid, but the number is still very high. There will need to be a new traffic light at Oakmont and Lake Sharon to handle the amount of traffic from adding 200 residences AND several crossing guards to ensure the children arrive at school safely. The school is already at capacity, so adding this many homes will overwhelm the school and force children that live nearby, but further away from this development, to have to be bussed to other schools. Rental properties also have high turnover so the school will then have issues with continuity of education for the children. This is not acceptable and not what the citizens purchased when they bought in this area.

Please do not allow this type of development in our city.

Sincerely,
17-year Resident

Michelle del Carpio
2506 Blue Holly Drive
Cypress Point Estates

Section S, Item 7.

Miguel Inclan

From: Michelle Fernandez
Sent: Saturday, February 13, 2021 9:13 PM
To: Miguel Inclan
Subject: Lake Sharon and Oakmont Drive development

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Mr. Inclan,

As members of this community for over a decade, we are writing to you to state that we oppose the proposal for development of new housing in the area of Lake Sharon, Oakmont Drive and 2499.

Our town is becoming saturated with nothing more than less than desirable housing projects. Small homes on small lots, townhomes and more apartments is not what this community needs. This is old school, more of the same thinking, is what makes established families decide to move away from our lovely little town. You are slowly killing the way of life we have here, just to cram in more families in smaller living quarters that adds nothing more than excessive traffic, police enforcement and a strain on our resources.

Our town needs development, but housing is not the answer. We need to diversify and keep what we have worked so hard to have.

Sincerely,
Daniel and Diane Fernandez.

Miguel Inclan

From: Michelle Pittard
Sent: Monday, February 22, 2021 2:08 PM
To: Miguel Inclan
Subject: Lake Sharon/Oakmont Apts

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Mr. Inclan,

As a resident of Corinth for the past 20 years, I am opposed to any apartment or rental property being built near the Oakmont and Lake Sharon area. The city does not take care of the streets and landscaping well, if at all, in this area, and to place another 200+ families in this space will not be advantageous to existing residents.

Advantages

Best Regards
Michelle Pittard

Miguel Inclan

From: Michael Ramos
Sent: Monday, February 22, 2021 10:53 AM
To: Miguel Inclan
Cc:
Subject: 13-year Lake Sharon Estates resident that OPPOSES the Avilla Fairways Planned Development

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Mr. Inclan,

My wife and I have lived in Lake Sharon Estates for 13 years now and can't be happier with the amenities and perks that the area brings.

Those amenities include the following:

- * High tax and utility rates that help protect our residential investments
- * Lake Sharon area and all its included wildlife
- * Lots of trees in and around Oakmont golf course where I'm an avid member
- * Wide open and mostly non-congested streets where we can take walks, jog, bike, ride a golf cart, etc.
- * Safe area for school children to walk to and from school

WHY WE OPPOSE THE AVILLA FAIRWAYS DEVELOPMENT

We sincerely believe that with the approval and introduction of the Avilla Fairways development, the proposed area will negate so many of the amenities listed above. We know that development and progress in Corinth are always desired, especially when it increases the tax base. However, given the small amount of land and number of units proposed per acre (qty = 9), it just doesn't make sense given the current neighborhood and its average home size (3000 sq ft). I'm not a professional urban planner like yourself, but I am a degreed engineer and the development numbers just don't make sense given the proposed area, resultant increase in traffic, and decreased quality of life.

ALTERNATIVES THAT WE WOULD SUPPORT

In our opinion, it would make more sense to allow the development of homes in that same size range (3000 sq ft) in the proposed area much like what it is being done in the Lake Sharon section 3 subdivision. Or if commercial options are more size-appropriate and tax-beneficial to Corinth, then go ahead and allow for a few retail establishments like a respectable coffee shop and a few high-end restaurants that are so sorely needed in Corinth so we don't have to drive to Denton, Flower Mound, etc. for a decent meal. Just like with the Bucees situation a few years ago, we're asking for the right thing to be done that fits within Corinth's economic legacy.

OUR QUESTION TO YOU

If you lived in the Lake Sharon area as we do, how would you feel about this type of residential area being put in your backyard?

Thanks so much for listening,

Mike and Yvonne Ramos
Lake Sharon Subdivision
2907 Sioux Court
Corinth, TX 76210

Miguel Inclan

From: Mindy Jameson
Sent: Monday, February 22, 2021 11:33 AM
To: Miguel Inclan; Planners
Cc: Will Jameson
Subject: NexMetro Communities - Avilla Fairways - Planning & Zoning 2/22/21

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Good Morning,

I am writing in regards to the rental development at the corner of Oakmont Drive and Lake Sharon Drive.

We live in a neighborhood that would be directly impacted by this development. We will be able to see the development from our yard, which is currently green space - golf course and trees. The entire feel of our neighborhood will diminish. The development is literally STEPS from Hawk Elementary School, where our children attend school. We are very concerned with the amount of traffic this would add to the area, and believe it would be UNSAFE for our children, who walk to and from school. Also, apartment homes are KNOWN to bring in more crime – Oakmont is SAFE right now – why would you want to change that?

After last weeks winter storms, there is solid proof that the infrastructure in Corinth is not ready for another 200+ homes on a 24 acre lot. This is too much, and the city is not prepared to handle it at this time. You need to protect Corinth and make improvements (water) before moving forward with this rental property!

Additionally, being so close to the development will certainly bring down our home value and completely ruin Oakmont Estates. We live in Larkspur, which will be directly impacted by this decision, and many of our neighbors have expressed that they will have to sell and leave. Otherwise, we will ALL lose...our houses will not be worth what we paid for them. We specifically sought out this neighborhood for its location, but if this property is developed, we will have to leave. Oakmont is now a very nice, family oriented, golf course community. Bringing in a development such as this will diminish that family oriented feel. WHY would you want that for Corinth? The golf course will suffer (no one wants to stare at apartment homes), wildlife and trees will be plowed over, and our schools will experience overcrowding and safety concerns. Why would you not want to protect and preserve the characteristics that make Oakmont such a great place to live?

We moved to Corinth to get away from school over crowding. We wanted our children at Hawk Elementary. We wanted to be near the golf course and experience the family environment and community feel of Oakmont. If this happens, we will have to sell our house and move out of Oakmont. This directly impacts the safety of our CHILDREN. This is NOT what we want in our community. This is NOT what we want our children to grow up next to. This is NOT what we want to see when we look out our window.

Please, PLEASE do not fail this community. Please stop this development from happening.

Thank you,
Mindy Jameson
Larkspur @ Oakmont Resident

Miguel Inclan

From: Tricia Coon
Sent: Monday, February 22, 2021 5:09 PM
To: Miguel Inclan
Subject: Avilla Fairways

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Miguel and the Planning & Zoning Team:

I am writing to contest the rezoning request to multi family residential by Endeavor Energy Resources.

I live in the North section of Cypress Point off of Lake Sharon and Oakmont Dr. I am very disturbed by the idea of having 200+ rental homes in this area. When I drive to Oakmont Country Club down Oakmont Drive during school days, there are so many cars parked waiting to pick up kids. This development will bring additional traffic that our streets in this area can't handle. Being a member at Oakmont I am also concerned about the drainage issue this development will cause. I know you will say that it will be addressed but it always seems to be the last thing addressed AFTER building is done. It will also be taxing on our community as far as services are concerned. Will the rental tenants honor the no trespassing at the golf course? Will it require more of our Police resources to keep them off of private property? And last but not least, I feel this development will bring down our property values. I know you can find studies that say it won't but there are studies that show it does as well.

To summaries: Please DO NOT approve this request because:

- Traffic
- Taxing on City Services
- Safety for our kids at the school close by
- Property Values

Sincerely,
Patricia Coon

Miguel Inclan

From: .. - - - - -
Sent: Sunday, February 21, 2021 7:08 PM
To: Miguel Inclan
Subject: "Avilla Fairways"

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Miguel,

Miguel,

The concerns are as follows:

- This will bring the house property value down.
- Sexual offenders, will this be monitored? Due to the school being so close.
- Who will be monitoring to make sure there are no sex offenders to move in the rental units?
- Instead of rentals why not develop nice homes? This will help Corinth to establish strong property value.
- If the rental units are build will our property taxes decrease? Due to the rental property brining down our home values.

Please reconsider building rental units.

Sincerely,
Raymond and Teresa Roller

Miguel Inclan

From: Smith, Rebecca
Sent: Monday, February 22, 2021 4:38 PM
To: Miguel Inclan
Subject: Avilla Fairways

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Mr. Inclan,

My husband and I moved to Lake Sharon Estates last summer because of the wonderful amenities and perks that the area brings. We appreciate the high tax rate and utility rates that help keep our residential investment. The wildlife that Lake Sharon bring. The close proximity to the country club and golf course. We liked that the area is not very congested so we can take our small children on walks, jog, bikes rides ect. We also love how close we are to great schools.

I am writing to say that **I oppose the Avilla Fairways development.** We believe this development will have a negative affect on all the reasons listed above. I agree that development of the area is good for the city, especially when it increase the tax base. However given the small amount of land and the number of units proposed I don't believe this development is a good fit.

I would propose instead to build like homes on that property (avg 3000 sq ft) much like what is being done in phase 3 of the Lake Sharon Estates. I understand the property is zoned for mix use, so another option would be establish some high end retail shops, and restaurants that are so badly needed in Corinth. Currently we drive to Highland Village to meet these needs.

Thank you for listing and I hope you take these concerns to heart.

Rebecca Smith
(P) 972-969-5828
(C) 469-216-9392

Pioneer Natural Resources
777 Hidden Ridge
Irving TX 75038

Statement of Confidentiality:

This message may contain information that is privileged or confidential. If you receive this transmission in error, please notify the sender by reply e-mail and delete the message and any attachments.

Miguel Inclan

From: naderrick2020
Sent: Sunday, February 21, 2021 8:58 PM
To: Planners
Cc: Kris
Subject: need a full and independent study of Avilla Fairways

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Rentals bring DOWN property values when adjacent to single family homes. NOT Happy!!!!

Rick and Kristine Nader
2209 Valderrama lane

Sent via the Samsung Galaxy S10+, an AT&T 5G Evolution capable smartphone

Miguel Inclan

From: Robert Bettes
Sent: Sunday, February 21, 2021 11:37 PM
To: Miguel Inclan
Subject: Avilla Fairways Apartment Project

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Miguel,

I am writing you to voice our deep concerns about the Avilla Fairways housing project. Succinctly, our concerns center on three areas:

1. Safety of school children with two schools less than 500 yards from the project.
2. Increased traffic hazards and congestion
3. Quality of life and security of nearby residents due to the inherent transient nature of apartment tenants

In 32 years of law enforcement and 28 years as an FBI Special Agent, I have lived in numerous cities where these type projects have been built. In almost every case, these types of complexes are **not** built in an existing residential neighborhood, but instead in a commercially zoned district. These complexes are by nature occupied by residents who sign short term leases, thus creating a transient culture in an otherwise stable community.

With 215 apartments being proposed by the developers, this would conservatively add an additional 300 vehicles to the immediate proximity of the schools.

We have been residents of the Oakmont area for almost 30 years. This is a family-based community that has a low percentage of resident turnover. In our opinion, to grant this zoning in this location would be reckless and send a message that our city leaders do not have the best interest of the Corinth family community at heart.

Sincerely,

Robert & Mary Bettes
1004 Ballycastle Lane
Corinth, Texas 76210
Cell: 214.558.6290
Email: RNB417@msn.com

Miguel Inclan

From:
Sent: Monday, February 22, 2021 1:28 PM
To: Miguel Inclan
Subject: AGAINST Avilla Fairways!

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Mr. Inclan,
This email is to express my concern AGAINST Avilla Fairways planned development.

I live in the Larkspur II development on Ardglass Trail, adjacent to the area of this proposed development. My husband and I purchased our home here with hopes to raise our family in a stable and safe neighborhood, and live amongst like minded individuals who care for their property and bring value to the neighborhood.

Adding a large rental property, as proposed, will serve to devalue and destabilize our community. Rental communities are known for constant turnover of their tenets. The constant turnover will destabilize the student populations of the already crowded Hawk Elementary and Crownover Middle School.

The addition of at least 215 new families to the area is great cause for concern about the increased traffic to the area. Oakmont Dr. sees many children, both accompanied and unaccompanied, walking to and from school. I am very concerned for the safety of these children with a high increase of vehicle traffic to the area. The connection to Rye Road and then Ballycastle will put all of our families in danger whom use the area for play and exercise.

I am also VERY concerned about the negative effects this development can have on property values in the area. The owners in this area purchased our homes and property for a premium price with the expectation that those values would remain and potentially grow. As a resident of the Larkspur II development, I had expectations that this area of proposed development would mirror the rest of the Oakmont and Lake Sharon neighborhoods in prestige, aesthetics and value. Families are drawn to these neighborhoods for these qualities and Avilla Fairways stands to harm the culture of the area.

Please consider the hundreds of families already in the area that stand to be negatively impacted by this development before allowing a commercial rental property to be approved.

Thank you for your time.

Sincerely,

Robyn Reed
1103 Ardglass Trail
915-241-4132

Miguel Inclan

From: Ron Gajewski
Sent: Sunday, February 21, 2021 3:12 PM
To: Miguel Inclan
Subject: AVilla Fairways

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

THIS IS UNBELIEVABLE.

#1: Must the City of Corinth tear down EVERY tree within city limits? Who's low-class money-grubbing idea is this? They should go to jail for wanting to totally destroy such a beautiful greenbelt along the only golf course in the city. WHY TEAR DOWN EVERY TREE in a city that still has a tiny bit of character. Even old Lake Dallas and Shady Shores would never do such a thing. And those communities could use the money a lot more than Corinth.

All it would take is a SMIDGEN of conscience to keep the zoning for single-family homes, so the city can stay at LEAST PARTLY GREEN.

Corinth is slowly becoming just like Irving and Garland, and all the other ugly places that are nothing but concrete and apartment buildings.

#2: Having so many rental apartments so close to the elementary and middle schools poses a risk to the many kids who walk to school. Think drugs, offenders, and generally less responsible people than homeowners.

#3: If you zoning people lived in this neighborhood, you WOULD NEVER allow for such a change. Don't be money-grubbing idiots who are only interested in a few more tax dollars.

#4: This is terribly unfair to all who have invested in the Oakmont community – the golf course itself as well as the homeowners. BETRAYAL by our own city council is the worst. You should be ashamed of yourselves for even considering such a DRASTIC BAIT-AN-SWITCH.

#5: Let the developers go to Lewisville or somewhere else, who don't care about trees, safety, and civic beauty. This is NOT the only piece of land available for them.

#6: HERE'S the result of such a change...you'd get a few more tax dollars, only to need them for the EXTRA POLICE that will be required to police the new CORINTH HOUSING PROJECTS.

#7: If you people allow this change, it must be because you are a bunch of liberal Yankee-type democrats only interested in more taxes and more voters for your inane ideas.

THINK PEOPLE, THINK!!!! A LITTLE BIT OF TAX MONEY IS NOT EVERYTHING!!

NOT FAIR TO CURRENT HOMEOWNERS AND OAKMONT COUNTRY CLUB!!! NOT FAIR to the City of Corinth!!

Ron Gajewski
214-415-0310
1315 Ardglass Trail, a resident of 25 years.

Miguel Inclan

From: Ronnie Vanatta <
Sent: Monday, February 22, 2021 3:27 PM
To: Miguel Inclan
Subject: Avilla Fairways
Signed By:

Dear Miguel,

Wanted to let you know that bulldozing the trees for Avilla Fairways 215 rental townhomes is not a good idea for Corinth. Surely someone could build single family homes on that acreage without clear cutting it, etc. Please do not do this to your residents and taxpayers. Hundreds of cars and hundreds of parking spaces is not a good look for that gateway to Corinth from 2499.

Thanks,
Ronnie

Ronnie Vanatta
Technical Writer | Mainframe Software Division
Broadcom

office: 469.497.4893
5465 Legacy Drive Suite 700 | Plano, TX 75024-3106
Ronnie.Vanatta@broadcom.com | broadcom.com

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Miguel Inclan

From: Wickstrom, Shannon
Sent: Sunday, February 21, 2021 10:12 PM
To: Miguel Inclan
Subject: Avilla Fairways

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hello,,

I'm writing to voice my concerns on the proposed zoning change to approve rental property in and around Oakmont Golf Residential Community with neighborhood schools. As you are aware, rental property has many cons, including safety and value of the community. It has been found that zip codes with a higher-than-average concentration of renters have lower property values by 14% as well as a higher crime rate with inconsistency of renters. This data is from the [American Community Survey](#).

We are long time Oakmont residents who enjoy the area and ability to walk and bike safely in our neighborhood. We have many kids, a lot of them, elementary age who walk to and from school. This change will bring more traffic and unfamiliar faces, in and out of our community. This is clearly a safety concern on multiple levels for our residents. We never imagined we would ever be faced with the option of rental property in the area when we purchased here.

Again, this is a family residential area in a golf course community with many young children. Please consider these factors of safety, decrease in property value, and integrity of the Corinth, Oakmont community. Can we leave and enjoy the little greenery and walkable sidewalks that we do have in the area so that the neighborhood of ownership and quality remains.

Please do not move forward with this zoning change and allow rental or commercial property in the Oakmont, Lake Sharon area.

Thank you for listening, understanding, and using your position to stop this rental development.

Sincerely,

Shannon & Jeff Wickstrom

Miguel Inclan

From: staci freeman
Sent: Monday, February 22, 2021 7:43 PM
To: Miguel Inclan
Subject: Zoning Change

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

I am against the zoning change request at Lake Sharon and Oakmont.

Thank you,

Staci Freeman

Sent from my iPhone

Miguel Inclan

From: Stephanie Francisco < >
Sent: Friday, February 12, 2021 11:09 AM
To: Miguel Inclan
Subject: Avilla Fairways PD ZAPD20-0004

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hi Miguel,

I hope this email finds you well and warm!

I am a resident of Corinth and have seen the signs for a proposed zoning change to the corner of Oakmont and Lake Sharon and the extension of Lake Sharon. I reviewed the proposal on the City of Corinth's website and am very displeased at the proposed change.

I was pleasantly surprised that the extension of Lake Sharon included what my family is now calling the "estuary." My children and I love to look at the snowy egrets and blue herons who gather every day to fish beneath the bridge on Lake Sharon drive. If an apartment complex is built in this area, our estuary will be nonexistent.

I'm not sure how to officially protest this proposed change and would appreciate your guidance in this regard. I see that there is a public meeting on Feb. 22, and while attending this meeting would be an inconvenience to me (as I'm usually cooking dinner and getting kids ready for bed), I would take time to attend and let my feelings be known and recorded as this is something that I feel very strongly about. One of the reasons we chose to live in Corinth was due to all the green spaces and wonderful integration of nature.

Thank you for your time and consideration.

--

Stephanie M. Francisco

Miguel Inclan

From: Steven Geis
Sent: Monday, February 22, 2021 2:21 PM
To: Miguel Inclan
Subject: Agenda Item B rezoning site @ Lake Sharon & Oakmont

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Corinth City Council,

Regarding Agenda Item B rezoning of site @ corner of Lake Sharon & Oakmont;

I am a resident at Cypress Pointe Estates and I'm opposed to the rezoning of this site. Being a multifamily real estate broker for 18 years, Corinth does not need this type of rental product nor should it want this type of cottage product. They always look good on paper but never live up to the hype and end up being nothing but an eyesore after a few years. They almost always bring down property values which is the last thing the residents in this area need.

Sincerely,

Steven Geis | Broker Associate | Multi-Family Specialist

Irwin Realty Group
525 S. Carroll Blvd, #100, Denton, TX 76201
Mobile 940.368.5559



Miguel Inclan

From: Sue Delmark
Sent: Monday, February 22, 2021 3:04 PM
To: Miguel Inclan
Subject: Avilla Fairways

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

I am writing to contest the rezoning request to multi family residential by Endeavor Energy Resources. I am a member of Oakmont Country Club and have friends who live near this proposed property. I am concerned about the traffic and congestion that several hundred townhomes or apartments and a neighborhood shopping area will bring to the area. When I drive to Oakmont there are many young children walking home from the Elementary school on Oakmont Dr. and parent cars park on the road waiting to pick up their children. This blocks one lane of traffic, which is a problem, but is understandable. Adding more congestion will only create more problems and potential accidents.

The other issue is the drainage problem that taking down the trees that are so beautiful in this area. This will create an aesthetic problem as well. The drainage problem is already an issue at the Oakmont CC, causing many days of shutting the course down, but taking down the trees will only continue to increase the drainage problems. Hole number 15 completely goes under water due to back up and Oakmont and Club corp has contested this for several years with the City of Denton.

Please do not approve this request. I also have lived near Lantana for 20 years and protested at the City of Denton, with Lantana coming into the area. Canyon Oaks, where I live has suffered from many homes being built there with increased drainage problems. We reached out to Andy Eads several times for help.

Please do not approve this request.

Sue Delmark

Thank you,

Miguel Inclan

From: Sue Wood
Sent: Sunday, February 21, 2021 3:14 PM
To: Miguel Inclan
Subject: Avilla Fairways

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Miguel Inclan,

As a homeowner and city of Corinth resident, I am writing to communicate my concern regarding the new development Avilla Fairways that is being proposed by NexMetro on the south side of Oakmont golf course. A rental community is not in keeping with the current status of the neighborhood and would not be an asset to the city in this location. Of most concern is that this proposed development is very close to Hawk Elementary School. This raises questions of the City's duty to safeguard the welfare of our children.

My request is that you do not move forward with the proposed zoning change and uphold the safety, quality, integrity, and value of the community.

Thank you for your time and consideration,
Susan Wood
1006 Ballycastle Ln
Corinth, TX 76210

Miguel Inclan

From: Taryn Owen
Sent: Monday, February 22, 2021 10:34 PM
To: Miguel Inclan
Subject: NexMetro

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hi Miguel -

As a property owner, I would like to protest the request to rezone the northwest corner of Lake Sharon Drive and Oakmont Drive. We moved to this neighborhood because it was quiet and wasn't close to any rental communities. I run down Oakmont and Lake Sharon multiple times a week and that wooded area is one of my favorite places. My vote would be to leave it undeveloped, but if something must be built on it, it should be a neighborhood similar to the Lake Sharon and Oakmont communities that it would be sandwiched between. I am also worried about the additional traffic it would bring. Not only would it be frustrating driving in it, but I'm also concerned for us who walk/run and the kids from Hawk Elementary who walk home from school.

Thank you for taking the time to hear my thoughts!
Taryn

Miguel Inclan

From: Ted Wlazlowski
Sent: Monday, February 22, 2021 6:37 AM
To: Miguel Inclan
Subject: Avilla Fairways

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Miguel and to all city managers - **unequivocally, my wife and I are absolutely shocked with the prospect of having a high density rental project in the vicinity of the school, our home and the golf course.**

We moved to Corinth 5 years ago to be distant from such developments and to enjoy the type of community that Corinth currently represents. It represents the prospect and undoubted reality of bringing in residents that are inconsistent with the stable environment we now enjoy.

The issue will be devaluation of our property value and that of the golf course community in general. **WE VOTE "NOT" TO THIS PROPOSAL - and IF ADOPTED, we do all that we can politically to ensure that those that approve this will be adversely impacted on election day. THIS POSITION IS UNIVERSALLY HELD BY MY NEIGHBORS AND OAKMONT MEMBERS.**

DO NOT LET THIS HAPPEN!!

Ted and Linda Wlazlowski

1113 Ardglass Trail

Corinth, Tx 76210

214-914-3908

Miguel Inclan

From: Terry Wright
Sent: Monday, February 22, 2021 1:54 AM
To: Miguel Inclan
Subject: Rental units

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Mr. Inclan;

Corinth is not ready for 215 rental units that create more traffic congestion, energy consumption (which was handled despicably as recently as this weekend), et.al. & all that is included in a 215 rental property apartment complex, including transients.

Keep our communities for people who are able to "purchase" homes & pay higher taxes for better schools & education etc., not some shoddy rental units thrown up creating future crime areas, unsupervised children & absent parents. I'm totally opposed to any plan like this. You want to pave paradise & put up a parking lot..... or several. Ruin the peaceful country life we left the city to avoid. Go to Lewisville, that area is accustomed to planned rental communities like yours.

Sincerely,
Terry E. Sparks

Miguel Inclan

From: Thomas Dudley
Sent: Sunday, February 21, 2021 6:34 PM
To: Miguel Inclan
Subject: Avilla Fairways

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Unfortunately, we will be out of town for the meeting on 02-22-21, but would like to offer our opinion.

We would like to know how this plan enhances the development of the City of Corinth. Single family homes would bring more civility and value which could possibly increase more tax dollars for the City.

The number of homes this plan calls for is ridiculous for the amount of land available. Based on the drawings of the plan, it appears all the trees will disappear in order to accommodate the homes. Having single family homes, we on the 13th hole would at least have some well groomed back yards to look at.

We are concerned about the closeness of this development to the elementary school with regard to the traffic and safety of the children in the area.

As a taxpayer, voter and resident in this area, we are totally 'in disagreement' with this proposal to change the Zoning request to allow the Rental Development.

Sincerely,

Joni & Thomas Dudley
1002 Ballycastle Lane
Corinth, TX 76210

Tiffany and Frankie Sanford
1105 Ardglass Trail
Corinth, TX 76210

February 22, 2021

Via Email: miguel.inclan@cityofcorinth.com

Planning and Zoning Commission
Corinth City Hall
3300 Corinth Parkway
Corinth, TX 76208
Attn: Miguel Inclan

Re: Public Hearing for Proposed Rezoning of the Northwest Corner of Lake Sharon Drive and Oakmont Drive, Corinth, Texas (the "Endeavor Tract")

Dear Mr. Inclan:

My husband and I live in Larkspur Phase II, which is the closest residential neighborhood to the Endeavor Tract, and we oppose the rezoning that has been proposed for the Endeavor Tract by NexMetro Communities. We have lived in Corinth for 20 years in neighborhoods that abut Oakmont Drive and own units and work in the Robinson Road office condos located at Robinson Road and State School Road. We are committed to Corinth being a wonderful place to live and work.

We oppose increasing the density for the development of the site from the existing PD-24 zoning (allowing for density of 6.5 dwellings per acre) that affects approximately 5.7 acres of the Endeavor Tract and the existing PD-6 zoning (allowing for density ranging from 6.5 to 10 dwellings, except that approximately 5 acres is for neighborhood shopping) that affects approximately 18.895 acres. The proposed zoning change would provide for 9 dwellings per acre. A higher density, 219 unit development on the Endeavor Tract would increase traffic on Oakmont Drive such that it would make school drop off and pick up at Hawk Elementary School and Crownover Middle School more difficult (there already is a lot of traffic on Oakmont Drive during those periods each school day) and would require additional crossing guards to be stationed at Oakmont Drive and Lake Sharon Drive. We have children who attend both Hawk Elementary School and Crownover Middle School and are concerned about increased traffic on Oakmont Drive. A higher density development would also increase the burden on our utilities, such as water and electricity. As shown by our experiences last week with electricity and water outages, we do not have reliable infrastructure to serve our existing needs, much less an additional 219 residential units.

We oppose the reduction of required parking spaces within the Endeavor Tract, which is proposed to be 1.85 spaces per unit. If you drive through neighborhoods located on either side of Oakmont Drive, households typically have two or more vehicles per residence and, even with garages and driveways for those residences, there are always vehicles parked in the streets. The

proposed zoning change for the Endeavor Tract provides for 1.85 parking spaces for each unit **and that includes garages and covered spaces in that count.** So if a one-bedroom unit has one couple living in that unit and that couple has two vehicles (which is typical since we do not have public transportation in the area and people need to drive to work and shop), then there will not be enough parking for those two vehicles within the Endeavor Tract. If a family with two adults and two teenagers of driving age live in a 3-bedroom unit within the Endeavor Tract, they might have three or four vehicles and nowhere to park within the Endeavor Tract. So where would those additional vehicles be parked? There will not be parking around Lake Sharon Drive or along Oakmont Drive, so will those additional vehicles wind up in neighboring subdivisions? If the zoning change is going to be approved, it should not include the change to allow for reduced parking requirements. The parking data included by NexMetro in its application that was prepared by Kimley-Horn gives details about parking in three NexMetro neighborhoods in Arizona. Is public transportation available in those Arizona communities? North Texas residents strongly depend on vehicles and do not have public transportation readily available to them. Corinth also is mainly a bedroom community and people must drive to work or to shop.

Related to such parking matter, we additionally oppose the connection of the Endeavor Tract to the Larkspur subdivision by the proposed extension of Rye Street. Having residents and visitors from the Endeavor Tract use Rye Street would increase our traffic in the Larkspur subdivision, which currently has limited traffic since no one uses the subdivision as a pass-through to another neighborhood, and would allow residents or visitors to the Endeavor Tract to use Larkspur for overflow parking.

We also oppose changing the uses allowed by the current zoning. We would like to see the neighborhood shopping portion of the tract zoning PD-6 to be developed as such. We patronize Corinth businesses to the extent we can such as Oakmont Country Club, Albertson's, Classic Cleaners, Colour Bar Salon, Hallmark, Interstate Batteries, Chick-Fil-A, Joey O's, AT&T, Corinth Orthodontics, Walgreens, Discount Tire, and North Texas Skin and Laser. However, our shopping areas within Corinth's city limits are few and we often end up shopping in nearby businesses in Hickory Creek, Denton, and Highland Village (which means Corinth does not get taxes from that shopping).

Sincerely,

A handwritten signature in black ink that reads "Tiffany Sanford". The signature is written in a cursive, flowing style.

Tiffany Sanford

Miguel Inclan

From: Timothy Krampitz
Sent: Monday, February 22, 2021 6:52 PM
To: Miguel Inclan
Subject: Lake Sharon/Oakmont development

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hi

As a resident in Cypress Estates, I'm writing to express my disappointment with any development of this corridor. I feel as the school and infrastructure needs of this area are already at capacity. I would hope this plan could be rejected.

Tim Krampitz
1624 Nightingale Lane.

[Sent from Yahoo Mail for iPhone](#)

Miguel Inclan

From: Todd Steger
Sent: Sunday, February 21, 2021 10:43 PM
To: Miguel Inclan
Subject: AVilla Fairways

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

This project is nothing more than a money grab by the City of Corinth for more tax dollars. I'm against this project for a number of reasons.

1. Must we destroy the entire green belt along Oakmont Dr and Lake Sharon to build rental properties in an area of nothing but single family homes? It will destroy our property values and make the area a much less desirable place to live. Who in their right mind thinks this is a good idea for the current residents?
2. The increase in traffic will be a hazard with all the kids who walk to/from school everyday down Oakmont Dr to Hawk Elementary and Crownover. Have any traffic studies been done regarding the impact of this proposed project?
3. These type of rental developments degrade over time and as they age become an eyesore in the community. This leads to a less affluent group of people moving into the neighborhood. We don't need the corner of Oakmont Dr and Lake Sharon to become the Corinth Housing Project. We invested a lot of money to live in Oakmont and have no desire to live near a project like this.
4. Be assured, any elected city official or council member who votes "yes" for this project will be voted out in the next election. The people in Oakmont/Lake Sharon are very against it and will ban together to make sure only candidates are elected who represent our best interests.
5. The project if approved will ultimately backfire and lead to a lot of long term residents, including my family, to move. It's already started in my neighborhood. The day after the proposed zoning change signs went up, for sale signs started appearing. Our property values will decline and the neighborhood schools will be burdened with renters who otherwise couldn't afford to live in the neighborhood. This is NOT progress. This is nothing more than the city selling out to a greedy developer providing any lip service necessary to get their project approved. Listen to the tax paying residents and vote NO!

Todd Steger
940-595-6081
1307 Ardglass Tr

Sent from my iPhone

Miguel Inclan

From: Virginia Holt
Sent: Monday, February 22, 2021 3:59 PM
To: Miguel Inclan
Subject: Avilla. Some traffic concerns

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

A few more points I hope P&Z will consider.

IMO the traffic flow plus impermeable surface runoff need to be part of an acceptable design.

For traffic- with no retail inhouse, all residents need to hop in cars with in and outs maybe 4x a day per car assuming people work away from home. I would assume that 215 units would each have at least two cars. We've seen in communities like Denton, major congestion and parking issues arise because it's not unusual for four cars to be associated with an apartment. I'm assuming that these luxury rental homes would appeal to the sort of young working people who perhaps share a house in order to get a better lifestyle.

Routes out need to be capable of handling high volume traffic several times a day, safely. Typically luxury rental home demographic will work in more affluent areas as non exist on site, will either head north to Denton or south to FM and the airport via Highland Village for work and food and entertainment.

Poor Signal Safety - The no traffic light left turns out of the complex and into major roads will create backups and accidents in rush hours in the vicinity unless traffic signals are procured, very doubtful on FM roads.

Traffic could go left along Lake Sharon Drive east to I35 E, but that will increase volume at Oakmont and Lake Sharon which is full of walkers to all 3 schools 2x a day. Not safe. The site of a new Firehouse is on the NE corner could be a concern. How will they safely exit in a highly congested site into a 4 way stop?

Unlikely TxDot will approve signals- Corinth suffered multiple wrecks almost daily from 2013-2017 as FM 2181 was widened when TxDot refused the city's 3 requests to install 30mph speed limit signs in a construction zone with poor sightlines. On Monday Nov. 2, 2014 the City asked for signage. On Nov 4th I was the 6th wreck at the our neighborhood's only exit in 7 days. 4 other neighborhoods had the same. Only after all these wrecks were covered in 3 networks did they put up signs. Stopping wrecks.

Oakmont has never been given a traffic light, so not expect TxDot to grant requests for an additional traffic signal at Lake Sharon and FM 2499.

Areas with poor traffic flow are usually not seen as desirable for retail. So another negative.

Virginia Holt

Sent from my iPhone

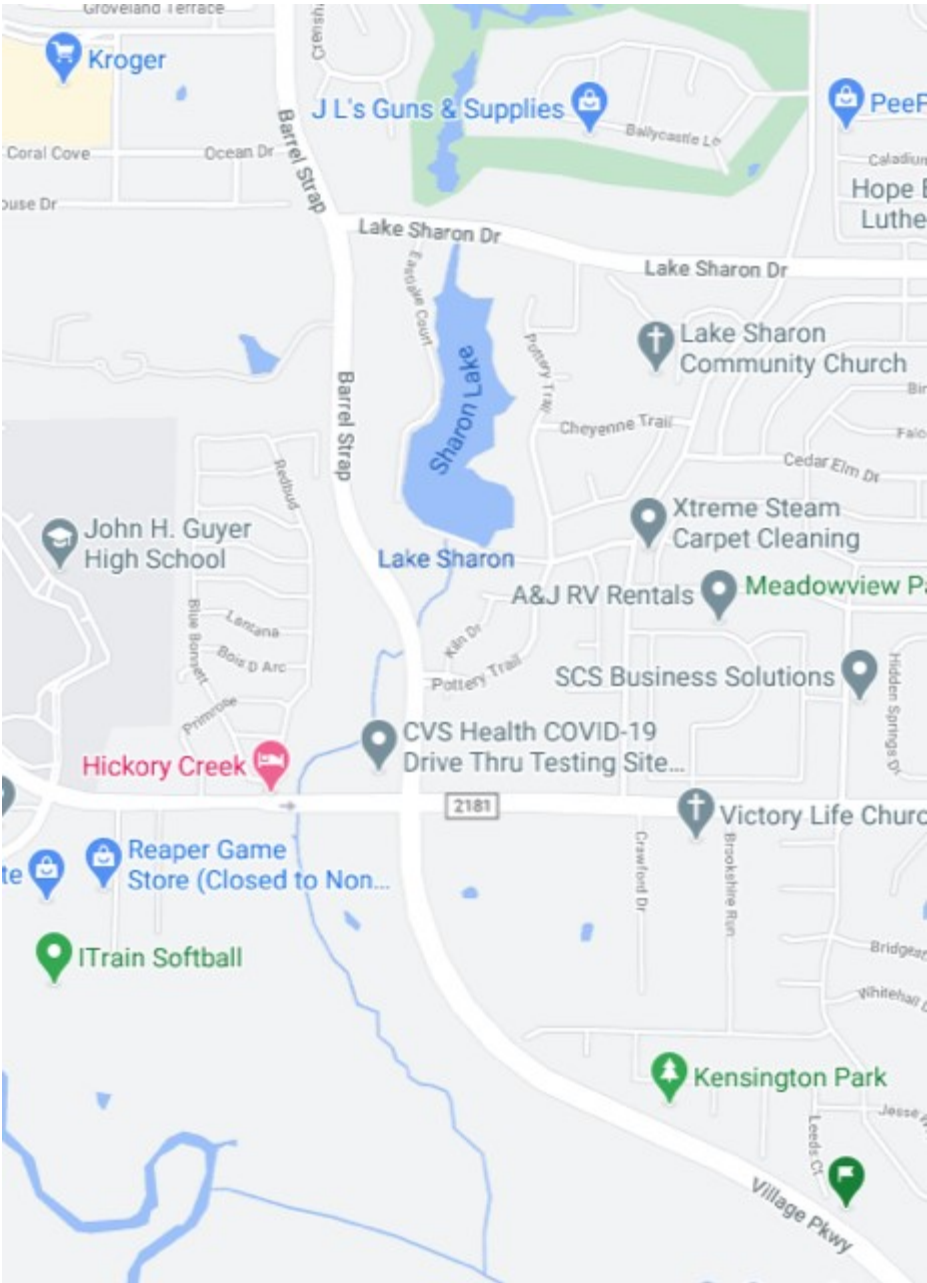
Miguel Inclan

From: Virginia Holt
Sent: Sunday, February 21, 2021 11:00 PM
To: Miguel Inclan
Subject: AVILLA FAIRWAYS - Please vote NO on Feb. 22

CAUTION: This email originated from outside of the organization. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Hope to “see” you at the meeting on Monday the 22nd at 6pm. I do not think that Avilla Fairways will contribute to the well being and community spirit of the Lake Sharon area in west Corinth. Folks here have worries that a large rental community with short term leases could create a transient atmosphere in the region near existing homes, lowering property values. Other neighbors have been concerned Endeavor could later slant drill and frack under the nearby site under the Christian center, with Endeavor’s leased properties guaranteeing little homeowner dissent. XTO Energy attempted to do this back in 2010, but was defeated after citizen protests, though this was later overturned by Governor Abbott who permitted suburban fracking. Neighbors have commented elsewhere that several Independent studies are needed to determine the long term impact of this 215 home rental complex. 1. Traffic Flow Study - the City should require a traffic flow study at max residence as the area will have walkers to 3 schools. 2. Accident study - southbound traffic at 4 lane FM 2499 and Lake Sharon Drive has no traffic signal. High volumes of traffic heading southbound could cause multiple rush hour wrecks without a signal. It is unlikely TxDot would permit a traffic signal in that location as they do not place signals close to existing signals at FM 2181 and near Kroger off FM 2499. 3. Downstream flooding - Corinth should have an independent hydrologic study as the old Lake Sharon dam could be undermined by flooding from the new subdivision upstream. A concern is this could cause downstream flooding of existing homes, rather like Meyerland experienced in Houston after new developments in the area were permitted. As neighbors in Kensington Estates might recall, cul de sacs at FM 2499 had much flooding after rains after the wall installation, with the waters threatening nearby existing homes. If you look at how the flow from Lake Sharon, a very shallow lake, heads south towards our Lake Lewisville watershed, you can see there is a danger that unanticipated water from a large subdivision could enter Lake Sharon, endangering the old dam, then possibly flooding the FM 2499 and FM 2181 intersection, which could impact on any large Retail establishments surrounding the intersection.

Virginia Holt
940-381-0616



APPENDIX C
LETTERS FROM PROPERTY OWNERS
WITHIN 200 FEET OF THE SUBJECT
PROPERTY

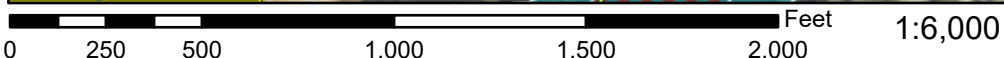
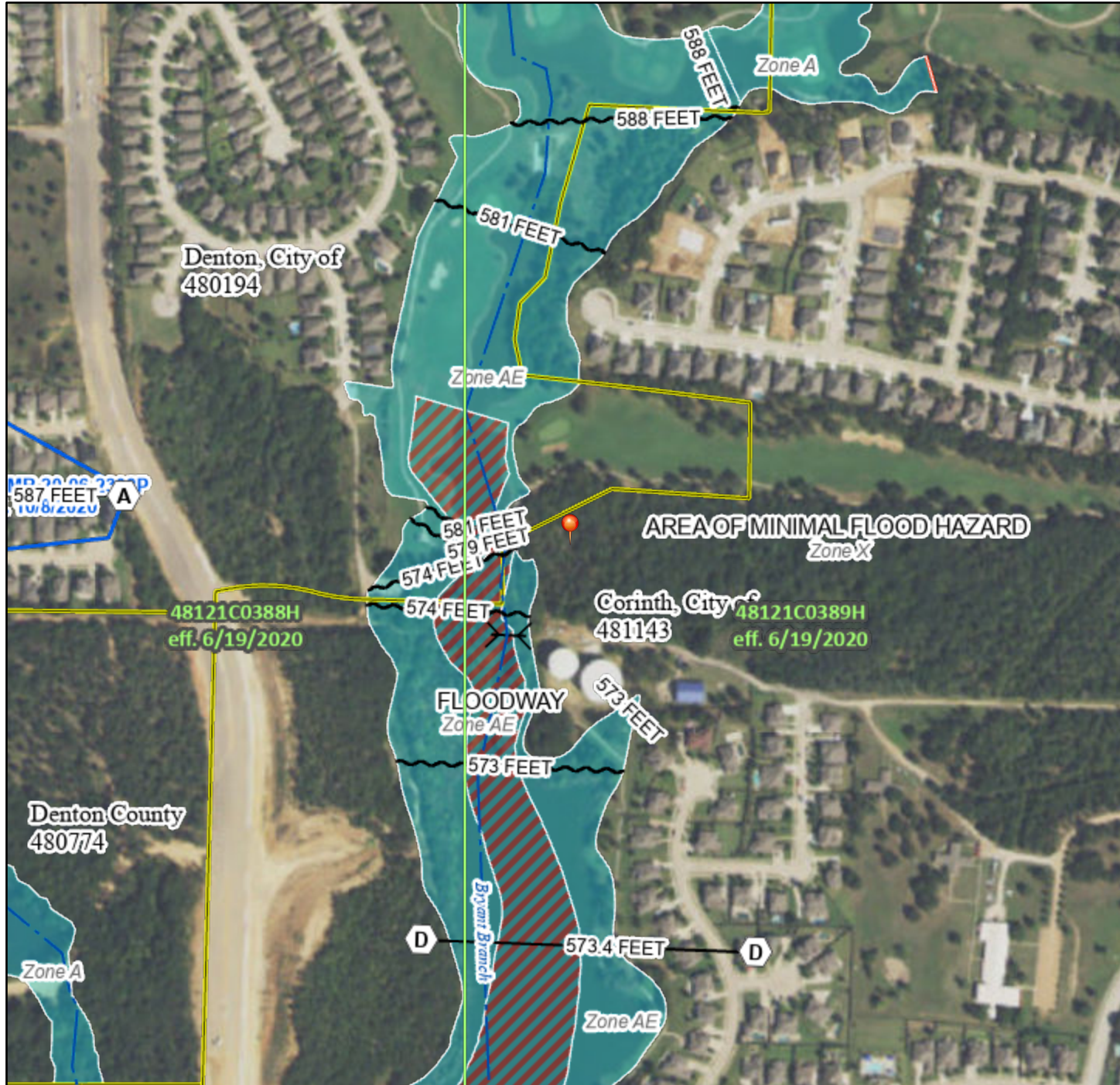
APPENDIX C

LETTERS FROM GENERAL PUBLIC

National Flood Hazard Layer FIRMMette



97°5'53"W 33°8'52"N



Basemap: USGS National Map: Orthoimagery: Data refreshed October, 2020

Legend

Section S, Item 7.

SEE FIS REPORT FOR DETAILED LEGEND AND INFORMATION

- SPECIAL FLOOD HAZARD AREAS**
 - Without Base Flood Elevation (BFE) Zone A, V, A99
 - With BFE or Depth Zone AE, AO, AH, VE, AR
 - Regulatory Floodway

- OTHER AREAS OF FLOOD HAZARD**
 - 0.2% Annual Chance Flood Hazard, Areas of 1% annual chance flood with average depth less than one foot or with drainage areas of less than one square mile Zone X
 - Future Conditions 1% Annual Chance Flood Hazard Zone X
 - Area with Reduced Flood Risk due to Levee. See Notes. Zone X
 - Area with Flood Risk due to Levee Zone D

- OTHER AREAS**
 - NO SCREEN Area of Minimal Flood Hazard Zone X
 - Effective LOMRs
 - Area of Undetermined Flood Hazard Zone D

- GENERAL STRUCTURES**
 - Channel, Culvert, or Storm Sewer
 - Levee, Dike, or Floodwall

- OTHER FEATURES**
 - Cross Sections with 1% Annual Chance Water Surface Elevation
 - Coastal Transect
 - Base Flood Elevation Line (BFE)
 - Limit of Study
 - Jurisdiction Boundary
 - Coastal Transect Baseline
 - Profile Baseline
 - Hydrographic Feature

- MAP PANELS**
 - Digital Data Available
 - No Digital Data Available
 - Unmapped



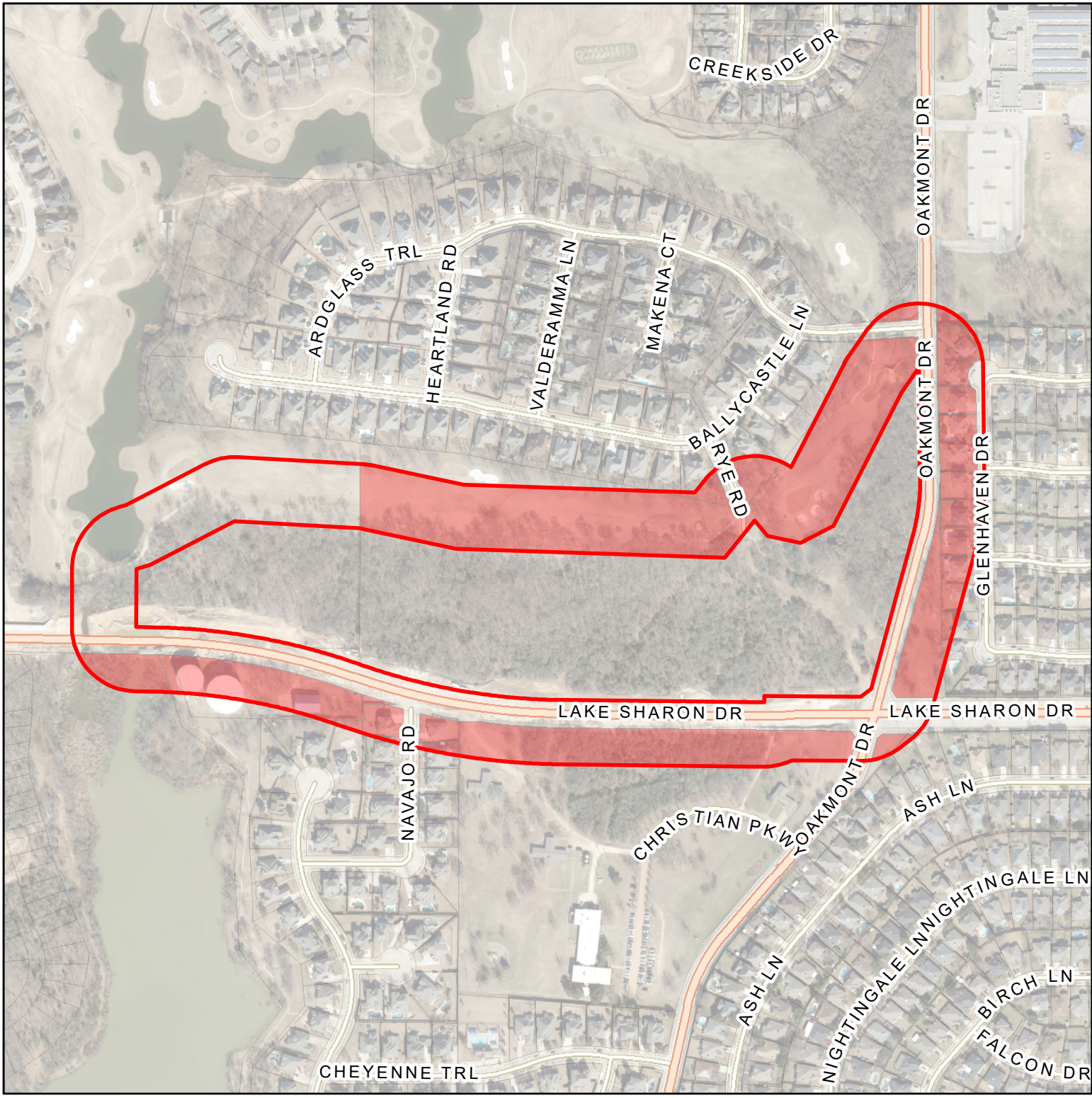
The pin displayed on the map is an approximate point selected by the user and does not represent an authoritative property location.

This map complies with FEMA's standards for the use of digital flood maps if it is not void as described below. The basemap shown complies with FEMA's basemap accuracy standards

The flood hazard information is derived directly from the authoritative NFHL web services provided by FEMA. This map was exported on 6/24/2021 at 8:39 AM and does not reflect changes or amendments subsequent to this date and time. The NFHL and effective information may change or become superseded by new data over time.

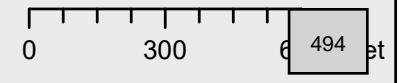
This map image is void if the one or more of the following map elements do not appear: basemap imagery, flood zone labels, legend, scale bar, map creation date, community id, FIRM panel number, and FIRM effective date. Map is unmapped and unmodernized areas cannot be used for regulatory purposes.

Section S, Item 7.
Parcels with a 200 ft.
of Property to be
Rezoned.
City of Corinth, TX



Legend

- 200 ft. Buffer
- Clipped Parcel



**CITY OF CORINTH, TEXAS
ORDINANCE NO. 21-07-15-19**

AVILLA FAIRWAYS PLANNED DEVELOPMENT DISTRICT #59

AN ORDINANCE OF THE CITY OF CORINTH, TEXAS, AMENDING THE CITY’S COMPREHENSIVE PLAN, SPECIFICALLY THE COMPREHENSIVE ZONING ORDINANCE AND THE “OFFICIAL ZONING DISTRICT MAP OF THE CITY OF CORINTH, TEXAS,” EACH BEING A PART OF THE UNIFIED DEVELOPMENT CODE OF THE CITY OF CORINTH, BY AMENDING ORDINANCE NO. 87-12-17-24, “PD-6 PLANNED DEVELOPMENT DISTRICT, FOR—TWO FAMILY GARDEN HOMES, TOWNHOMES, AND NEIGHBORHOOD SHOPPING”, BY REPEALING ORDINANCE NO. 99-12-16-45, “PD-24 PLANNED DEVELOPMENT DISTRICT, FOR TWO FAMILY GARDEN HOMES” PROVIDING THE CURRENT ZONING CLASSIFICATION FOR THE PROPERTY DESCRIBED AND DEPICTED IN EXHIBIT A AND EXHIBIT B HERETO, HEREIN, AND ADOPTING THIS ORDINANCE TO REZONE THE PROPERTY PD PLANNED DEVELOPMENT DISTRICT WITH A BASE ZONING DISTRICT OF MF-1 MULTI-FAMILY RESIDENTIAL ON APPROXIMATELY ±24.595 ACRES (±1,071,370 SQUARE FEET) OF LAND IN THE A.H. SERREN SURVEY, ABSTRACT NO. 1198 AND THE B. MERCHANT SURVEY, ABSTRACT NO. 800, CITY OF CORINTH, DENTON COUNTY, TEXAS; THE PROPERTY IS GENERALLY LOCATED AT THE NORTHWEST CORNER OF LAKE SHARON DRIVE AND OAKMONT DRIVE, EAST OF FM 2499, AND IDENTIFIED AS AVILLA FAIRWAYS PLANNED DEVELOPMENT DISTRICT NO. 59 (“PD-59”); PROVIDING FOR THE INCORPORATION OF PREMISES; PROVIDING A LEGAL PROPERTY DESCRIPTION (EXHIBIT “A”) PROVIDING AN ILLUSTRATIVE DEPICTION OF THE PROPERTY (EXHIBIT “B”); PROVIDING A PLANNED DEVELOPMENT CONCEPT PLAN (EXHIBIT “C”); PROVIDING A CONCEPTUAL LANDSCAPE PLAN (EXHIBIT “D”); PROVIDING CONCEPTUAL ELEVATIONS (EXHIBIT “E”); PROVIDING LAND USE REGULATIONS (EXHIBIT “F”); PROVIDING FOR A PENALTY NOT TO EXCEED \$2,000 A DAY FOR EACH VIOLATION OF THE ORDINANCE AND A SEPARATE OFFENSE SHALL OCCUR ON EACH DAY THAT A VIOLATION OCCURS ON CONTINUES; PROVIDING A CUMULATIVE REPEALER CLAUSE; PROVIDING A SAVINGS CLAUSE; PROVIDING FOR PUBLICATION; PROVIDING A SEVERABILITY CLAUSE; AND PROVIDING FOR AN EFFECTIVE DATE.

WHEREAS, the City of Corinth, Texas has adopted Ordinance 13-05-02-08, which adopts a Unified Development Code of the City, which includes the Comprehensive Zoning Ordinance and which, in accordance with the City’s Comprehensive Plan, establishes zoning districts and adopts a Zoning Map; and

WHEREAS, the property is comprised of multiple tracts of land, as described in **Exhibit “A”** and depicted in **Exhibit “B”** (collectively, the “Property”), and is currently zoned as PD-6 Planned Development District, Ordinance No. 87-12-17-24, Two Family Garden Homes, Townhomes, and Neighborhood Shopping and PD-24 Planned Development District, Ordinance No. 99-12-16-45, Two

Family Garden Homes, under the City’s Unified Development Code and as designated on the City’s Zoning Map; and,

WHEREAS, and an authorized person having a proprietary interest in the Property has requested a change in the zoning classification of said Property to PD-Planned Development zoning district with a base zoning of MF-1, Multi-Family Residential under the City’s Unified Development Code (“UDC”), more specifically identified as Avilla Fairways Planned Development District No. 59 (“PD-59”); and

WHEREAS, the City Council and the Planning and Zoning Commission of the City of Corinth, having given the requisite notices by publication and otherwise, and each, held due hearings and afforded a full and fair hearing to all the property owners generally, and to the persons interested and situated in the affected area and in the vicinity thereof; and

WHEREAS, the Planning and Zoning Commission has recommended approval of the requested change in zoning to the Property, and the City Council has determined that the Property has unique characteristics and that zoning through a planned development district is the most appropriate mechanism for zoning the Property, thus an amendment to the Comprehensive Zoning Ordinance and the Zoning Map of the City’s Unified Development Code, in accordance with the standards and specifications set forth herein, including without limitation the Land Use Regulations set forth in **Exhibit “F,”** should be approved; and

WHEREAS, the City Council considered the following factors in making a determination as to whether the requested change should be granted or denied: safety of the motoring public and pedestrians using the facilities in the area immediately surrounding the sites; safety from fire hazards and damages; noise producing elements and glare of the vehicular and stationary lights and effect of such lights on established character of neighborhoods; location, and types of signs and relation of signs to traffic control and adjacent property; street size and adequacy of width for traffic reasonably expected to be generated by the proposed use around the site and in the immediate neighborhood; adequacy of parking as determined by requirements of this ordinance for off-street parking facilities; location of ingress and egress points for parking and off-street loading spaces, and protection of public health by surfacing on all parking areas to control dust; effect on the promotion of health and the general welfare; effect on light and air; effect on the over-crowding of the land; effect on the concentration of population, and effect on transportation, water, sewerage, schools, parks and other public facilities; and

WHEREAS, the City Council further considered among other things the character of the proposed district and its peculiar suitability for particular use requested and the view to conserve the value of the buildings, and encourage the most appropriate use of the land throughout this City; and

WHEREAS, the City Council finds that the requested Amendment to the City’s Comprehensive Zoning Ordinance and Zoning Map to effect the change in zoning for the Property promotes the health and the general welfare, provides adequate light and air, prevents the over-crowding of land, avoids undue concentration of population, and facilitates the adequate provision of transportation, water, sewerage, schools, parks and other public requirements; and the general health, safety and welfare of the community;

NOW, THEREFORE, BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF CORINTH, TEXAS:

**SECTION 1.
INCORPORATION OF PREMISES**

The above and foregoing recitals are found to be true and correct and are incorporated into the body of this Ordinance for all purposes.

**SECTION 2.
LEGAL PROPERTY DESCRIPTION; AMENDMENT**

That Ordinance No. 87-12-17-24, “PD-6 Planned Development District for Two Family Garden Homes, Townhomes, and Neighborhood Shopping and Ordinance No. 99-12-16-45, “PD-24 Planned Development District for Two Family Garden Homes, which ordinances amended Ordinance 13-05-02-08, adopting the Unified Development Code of the City of Corinth (“UDC”), which UDC includes the Comprehensive Zoning Ordinance that establishes zoning districts in accordance with the City’s Comprehensive Plan, and adopts the Zoning Map of the City of Corinth (“Zoning Map”), are hereby repealed in their entirety, and this Ordinance is hereby adopted to amend Ordinance No. 13-05-02-08, the UDC, and the Zoning Map in order to change the zoning classifications on approximately ±24.595 acres of land, (±1,071,370 Square Feet) of land in the A.H. Serren Survey, Abstract No. 1198 and the B. Merchant Survey, Abstract No. 800, City of Corinth, Denton County, Texas, and generally located at the Northwest Corner of Lake Sharon Drive and Oakmont Drive, East of FM 2499, as more specifically described in **Exhibit “A,”** and **depicted in Exhibit “B,”** each of which are attached hereto and incorporated herein (the “Property”), **from** PD-6 Planned Development District, and PD-24 Planned Development District, Avilla Fairways Planned Development District No. 59, to PD Planned Development District with a base zoning district of MF-1 Multi-Family Residential and identified as Avilla Fairways Planned Development District No. 59 (“PD-59”) subject to the regulations contained in this Ordinance. The Zoning Map of the City is also hereby amended to reflect the new zoning classification for the Property.

**SECTION 3.
PLANNED DEVELOPMENT CONCEPT PLAN**

The Planned Development Concept Plan for the Property as set forth in **Exhibit “C,”** a copy of which is attached hereto and incorporated herein, is hereby adopted.

**SECTION 4.
ADDITIONAL ANCILLIARY CONCEPTUAL PLANS**

Additional ancillary conceptual plans identified as the Conceptual Landscape Plan, as set forth in **“Exhibit D,”** Conceptual Elevations, as set forth in **“Exhibit E,”** are attached hereto and incorporated herein, and are hereby adopted as part of this Ordinance. (**Exhibits “D,” and “E”** are collectively herein referred to as the **“Ancillary Conceptual Plans”**). **SECTION 5.**

LAND USE REGULATIONS

A. The Zoning and Land Use Regulations set forth in “**Exhibit F**,” attached hereto and made a part hereof for all purposes are hereby adopted and shall be adhered to in their entirety for the purposes of this PD-Planned Development zoning district with a base zoning of MF-1, Multi-Family Residential. In the event of conflict between the provisions of “**Exhibit F**” and provisions of any other City zoning regulations, including without limitation the regulations governing MF-1, Multi-Family Residential zoning district, “**Exhibit F**” shall control. Except in the event of a conflict as provided herein or as otherwise expressly provided herein, all UDC regulations shall apply to the Property and shall be cumulative of this Ordinance and all applicable regulations of the City.

B. That the zoning regulations and districts herein established have been adopted in accordance with the Comprehensive Plan for the purpose of promoting the health, safety, morals, and the general welfare of the community. They have been designed, with respect to both present conditions and the conditions reasonably anticipated to exist in the foreseeable future; to lessen congestion in the streets; to provide adequate light and air; to prevent over-crowding of land; to avoid undue concentration of population; and to facilitate the adequate provision of transportation, water, sewerage, drainage and surface water, parks and other commercial needs and development of the community. They have been made after a full and complete hearing with reasonable consideration among other things of the character of the district and its peculiar suitability for the particular uses and with a view of conserving the value of buildings and encouraging the most appropriate use of land throughout the community.

C. The Planned Development Concept Plan (“**Exhibit C**”), Conceptual Landscape Plan (“**Exhibit D**”) Conceptual Elevations (“**Exhibit E**”), and the Land Use Regulations (“**Exhibit F**”) shall control the use and development of the Property in accordance with the provisions of this Ordinance, and all building permits and development requests shall be in accordance with applicable City ordinances, the PD Concept Plan, Ancillary Conceptual Plans, and Land Use Regulations. The PD Concept Plan, Ancillary Concept Plans and Land Use Regulations shall remain in effect as set forth herein unless amended by the City Council.

If a change to the Concept Plan, and/or associated Ancillary Conceptual Plans, if any, is requested for the Property, the request shall be processed in accordance with the UDC and other development standards in effect at the time the change is requested for the proposed development and shall be subject to City Council approval.

**SECTION 6.
PENALTY FOR VIOLATIONS**

Any person, firm, or corporation who intentionally, knowingly or recklessly violates any provision of this Ordinance or the Code of Ordinances, as amended hereby, shall be subject to a fine not to exceed the sum of five hundred dollars (\$500.00) for each offense, and each and every day any such offense shall continue shall be deemed to constitute a separate offense, provided, however, that in all cases involving violation of any provision of this Ordinance or Code of Ordinances, as amended hereby, governing the fire safety, zoning, or public health and sanitation shall be subject to a fine not to exceed the sum of two thousand dollars (\$2,000.00) for each offense.

**SECTION 7.
SEVERABILITY**

The provisions of the Ordinance are severable. However, in the event this Ordinance or any procedure provided in this Ordinance becomes unlawful, or is declared or determined by a judicial, administrative or legislative authority exercising its jurisdiction to be excessive, unenforceable, void, illegal or otherwise inapplicable, in whole or in part, the remaining and lawful provisions shall be of full force and effect and the City shall promptly promulgate new revised provisions in compliance with the authority's decisions or enactment.

**SECTION 8.
CUMULATIVE REPEALER**

This Ordinance shall be cumulative of all other Ordinances and shall not repeal any of the provisions of such Ordinances except for those instances where there are direct conflicts with the provisions of this Ordinance. Ordinances, or parts thereof, in force at the time this Ordinance shall take effect and that are inconsistent with this Ordinance are hereby repealed to the extent that they are inconsistent with this Ordinance. Provided however, that any complaint, action, claim or lawsuit which has been initiated or has arisen under or pursuant to such other Ordinances on this date of adoption of this Ordinance shall continue to be governed by the provisions of such Ordinance and for that purpose the Ordinance shall remain in full force and effect.

**SECTION 9.
SAVINGS**

All rights and remedies of the City of Corinth, Texas, are expressly saved as to any and all violations of the provisions of any other ordinance affecting zoning for the Property which have occurred at the time of the effective date of this ordinance; and, as to such accrued violations and all pending litigation, both civil and criminal, whether pending in court or not, under such ordinances same shall not be affected by this Ordinance but may be prosecuted until final disposition by the court.

**SECTION 10.
EFFECTIVE DATE**

This ordinance shall become effective after approval and publication as provided by law. The City Secretary is directed to publish the caption and penalty of this ordinance two times.

**PASSED AND APPROVED BY THE CITY COUNCIL OF THE CITY OF CORINTH THIS ____
DAY OF _____, 2021.**

APPROVED:

Bill Heidemann, Mayor

ATTEST:

Lana Wylie, City Secretary

APPROVED AS TO FORM:

Patricia Adams, City Attorney

EXHIBIT "A"

LEGAL DESCRIPTION

BEING a tract of land situated in the Berry Merchant Survey, Abstract No. 800 and the A.H. Serren Survey, Abstract No. 1198, City of Corinth, Denton County, Texas, and being a portion of a called 41.272 acre tract of land described as Tract 1 in a Special Warranty Deed to Endeavor Energy Resources, L.P., as recorded in Document No. 2007-34656 of the Official Records of Denton County, Texas, and being more particularly described as follows:

BEGINNING at a 5/8 inch iron rod with plastic cap stamped "Carter Burgess" found for the northerly southeast corner of said 41.272 acre tract, common to the northerly end of a corner clip at the intersection of the westerly right-of-way line of Oakmont Drive, a 60 foot wide right-of-way, as created in the deed to City of Corinth recorded in Document No. 2006-152687 of the Official Records of Denton County, Texas, with the northerly right-of-way line of Lake Sharon Blvd., a variable width right-of-way as created in said deed to City of Corinth recorded in Document No. 2006-152687 of the Official Records of Denton County, Texas;

THENCE South 52°40'31" West, departing the westerly right-of-way line of said Oakmont Drive, continuing along the easterly line of said 41.272 acre tract and along said corner clip, a distance of 39.84 feet to a 5/8 inch iron rod with plastic cap stamped "KHA" set for the southerly southeast corner of said 41.272 acre tract, common to the southerly end of said corner clip, being on the northerly right-of-way line of said Lake Sharon Blvd.;

THENCE along the southerly line of said 41.272 acre tract and the northerly right-of-way line of said Lake Sharon Blvd., the following courses:

North 89°28'05" West, a distance of 300.09 feet (called 300.00 feet) to a 5/8 inch iron rod with plastic cap stamped "Carter Burgess" found for corner;

South 0°16'50" West, a distance of 18.73 feet to a 5/8 inch iron rod with plastic cap stamped "KHA" set for the northeast corner of a called 0.895 acre tract of land described in a deed to City of Corinth, Texas, as recorded in Document No. 2018-14228 of the Official Records of Denton County, Texas, and at the beginning of a non-tangent curve to the right having a central angle of 2°04'20", a radius of 1008.00 feet, a chord bearing and distance of South 89°39'39" West, 36.45 feet;

THENCE continuing along the northerly right-of-way line of said Lake Sharon Blvd., and along the northerly line of said 0.895 acre tract, the following courses:

In a southwesterly direction, with said curve to the right, an arc distance of 36.46 feet (called 36.48 feet) to a 5/8 inch iron rod with plastic cap stamped "KHA" set for corner;

North 89°18'07" West, a distance of 594.43 feet to a 5/8 inch iron rod with plastic cap stamped "KHA" set at the beginning of a tangent curve to the right having a central angle of 18°38'22", a radius of 1965.98 feet, a chord bearing and distance of North 79°58'59" West, 636.76 feet;

In a northwesterly direction, with said curve to the right, an arc distance of 639.57 feet to a 5/8 inch iron rod with plastic cap stamped "KHA" set for the northwest corner of said 0.895 acre tract, being on the easterly line of a called 1.365 acre tract of land described as Tract 1 in a deed to City of Corinth, Texas, as recorded in Document No. 2016-105077 of the Official Records of Denton County, Texas,

THENCE North 4°46'46" West, continuing along the northerly right-of-way line of said Lake Sharon Blvd., and the easterly line of said 1.365 acre tract, a distance of 48.27 feet to a 5/8 inch iron rod with plastic cap stamped "KHA" set for the northeast corner of said 1.365 acre tract;

THENCE North 88°49'28" West, continuing along the northerly right-of-way line of said Lake Sharon Blvd., and the northerly line of said 1.365 acre tract, a distance of 165.36 feet to a 5/8 inch iron rod with plastic cap stamped "KHA" set for the easterly corner of a called 1.597 acre tract of land described as Tract 2 in a deed to City of Corinth, Texas, as recorded in Document No. 2018-14228 of the Official Records of Denton County, Texas, and at the beginning of a non-tangent curve to the left having a central angle of 12°52'01", a radius of 2034.00 feet, a chord bearing and distance of North 82°00'53" West, 455.82 feet;

THENCE departing the northerly line of said 1.365 acre tract, continuing along the northerly right-of-way line of said Lake Sharon Blvd., and along the northerly line of said 1.597 acre tract, the following courses:

In a northwesterly direction, with said curve to the left, an arc distance of 456.78 feet to a 5/8 inch iron rod with plastic cap stamped "KHA" set for corner;

North 88°47'12" West, a distance of 107.42 feet to a point for corner;

THENCE North 1°03'54" East, departing the northerly right-of-way line of said Lake Sharon Blvd., and the northerly line of said 1.597 acre tract, and crossing said 41.272 acre tract, a distance of 170.35 feet to a 5/8 inch iron rod with plastic cap stamped "KHA" set for corner on the northerly line of said 41.272 acre tract and the southerly line of a called 127.4118 acre tract of land described as Tract 1 in a deed to Oakmont Management Corp, as recorded in Volume 3415, Page 839 of the Deed Records of Denton County, Texas;

THENCE along the northerly line of said 41.272 acre tract and the southerly line of said 127.4118 acre tract, the following courses:

North 74°32'55" East, a distance of 78.16 feet to a 5/8 inch iron rod with plastic cap stamped "KHA" set for corner;

North 62°29'17" East, a distance of 293.39 feet to a 1 inch iron rod found for corner;

South 86°47'50" East, a distance of 388.31 feet to a 1 inch iron rod found for corner;

South 78°07'06" East, a distance of 307.57 feet (called 306.97 feet) to a 1 inch iron rod found for corner;

South 88°03'33" East, a distance of 836.57 feet (called 837.21 feet) to a 1 inch iron rod found for corner;

North 38°32'46" East, a distance of 149.05 feet (called 149.01 feet) to a 1 inch iron rod found for corner;

South 39°00'09" East, a distance of 62.15 feet to a 1 inch iron rod found for corner;

South 77°48'03" East, a distance of 105.78 feet to a 1 inch iron rod found for corner;

North 62°23'27" East, a distance of 114.88 feet to 1 inch iron rod found for corner;

North 27°06'38" East, a distance of 465.25 feet to a 1 inch iron rod found for corner;

North 34°46'19" East, a distance of 95.49 feet to a 5/8 inch iron rod with plastic cap stamped "Carter Burgess" found for the easterly northeast corner of said 41.272 acre tract, being on the westerly right-of-way line of aforesaid Oakmont Drive;

THENCE along the easterly line of said 41.272 acre tract and the westerly right-of-way line of said Oakmont Drive, the following courses:

South 0°33'43" East, a distance of 357.41 feet to a 5/8 inch iron rod found at the beginning of a tangent curve to the right having a central angle of 15°48'38", a radius of 570.00 feet, a chord bearing and distance of South 7°20'35" West, 156.79 feet;

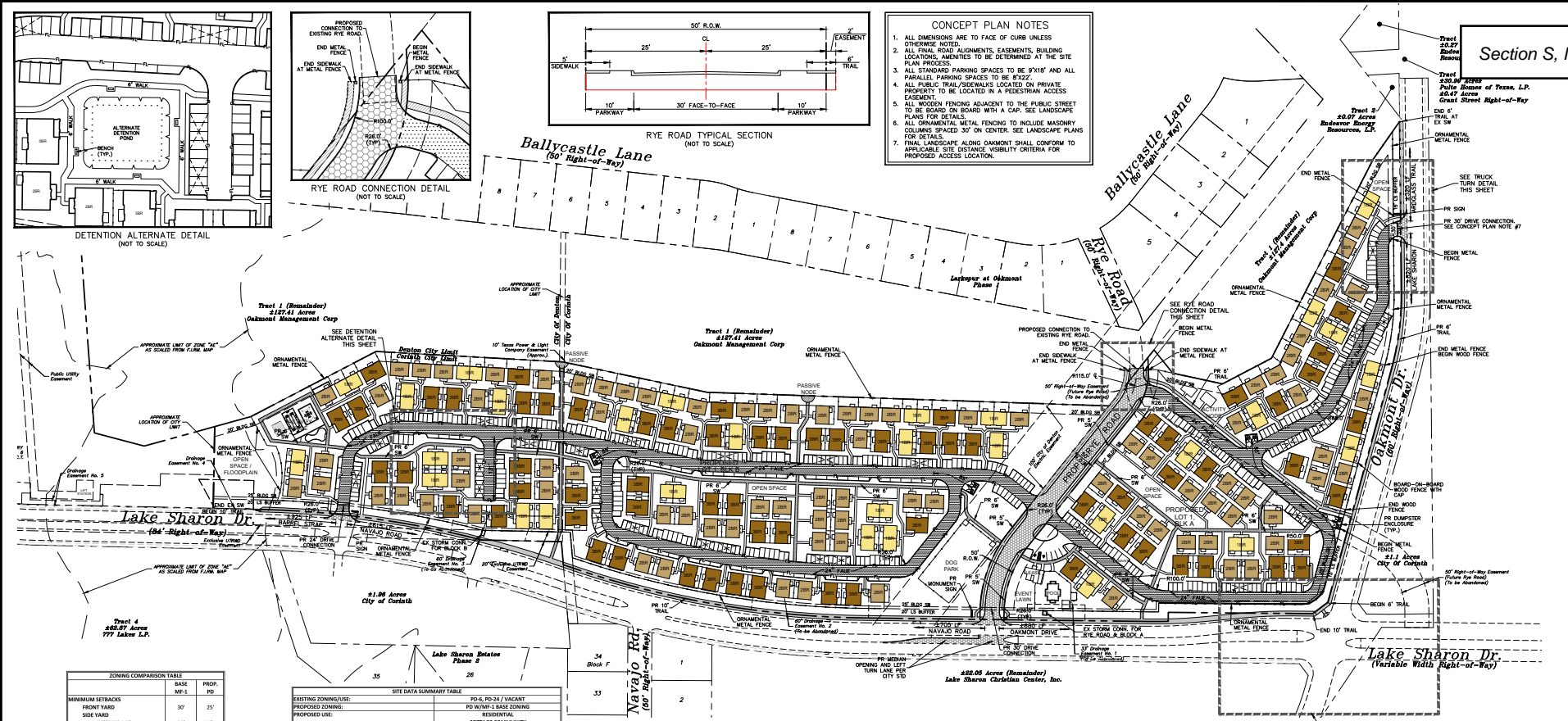
In a southwesterly direction, with said curve to the right, an arc distance of 157.29 feet to a point for corner, from which, a 60D nail found for witness bears North 1°30' East, 0.2 feet;

South 15°14'53" West, a distance of 504.68 feet to the POINT OF BEGINNING and containing 24.595 acres (1,071,370 square feet) of land, more or less.



AVILLA FAIRWAYS - CORINTH, TX

ILLUSTRATIVE PLAN



- CONCEPT PLAN NOTES**
1. ALL DIMENSIONS ARE TO FACE OF CURB UNLESS OTHERWISE NOTED.
 2. ALL FINAL ROAD ALIGNMENTS, EASEMENTS, BUILDING LOCATIONS, ADJACENCIES TO BE DETERMINED AT THE SITE PLAN PROCESS.
 3. ALL STANDARD PARKING SPACES TO BE 9'x18' AND ALL PARALLEL PARKING SPACES TO BE 8'x22'.
 4. ALL PUBLIC TRAIL/SIDEWALKS LOCATED ON PRIVATE PROPERTY TO BE LOCATED IN A PEDESTRIAN ACCESS EASEMENT.
 5. ALL WOODEN FENCING ADJACENT TO THE PUBLIC STREET TO BE BOARD ON BOARD WITH A CAP. SEE LANDSCAPE PLAN FOR DETAILS.
 6. ALL ORNAMENTAL METAL FENCING TO INCLUDE MASONRY COLUMNS SPACED 30' ON CENTER. SEE LANDSCAPE PLANS FOR DETAILS.
 7. FINAL LANDSCAPE ALONG DRAINAGE SHALL CONFORM TO APPLICABLE SITE DISTANCE VISIBILITY CRITERIA FOR PROPOSED ACCESS LOCATION.

Section S, Item 7.

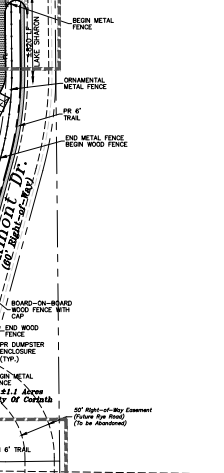
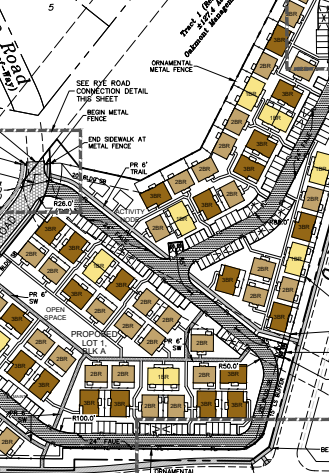
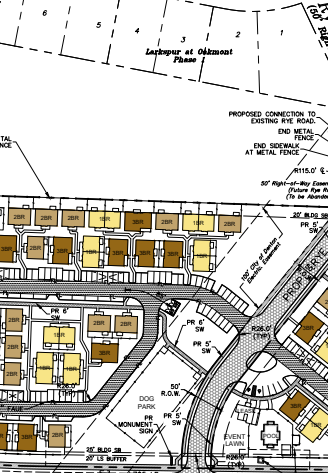
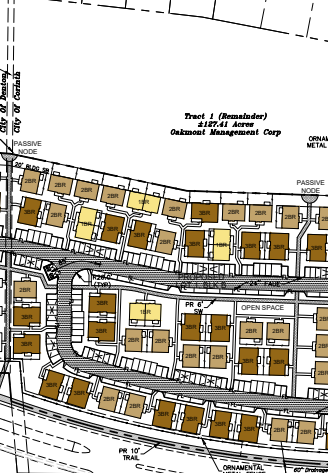
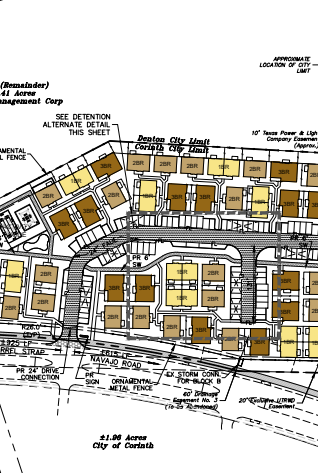
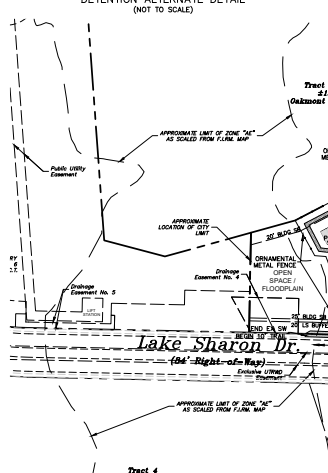
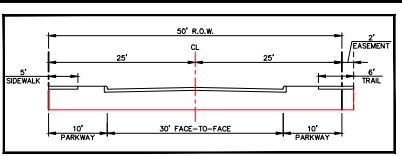
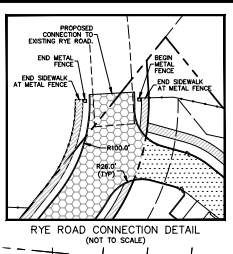
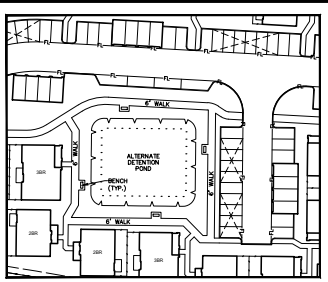
LCE LINCOLN CONSULTING & ENGINEERING
 PRELIMINARY
 FOR REVIEW ONLY
 NOT FOR CONSTRUCTION
 LINCOLN CONSULTING & ENGINEERING
 P.O. Box 1176
 Wylie, Texas 75298
 Phone: 972.422.2221
 Fax: 972.422.2221
 Email: info@lceinc.com
 Website: www.lceinc.com

AVILLA FAIRWAYS - City of Corinth, Texas - LCSE PROJECT NO. 1015-003

**AVILLA FAIRWAYS
 LAKE SHARON @ OAKMONT
 24.595 ACRE TRACT OF LAND**
 City of Corinth, Denton County, Texas

PD CONCEPT PLAN

Scale: 1" = 100'
 Designed by: JAL
 Checked by: JAL
 Date: June 22, 2021
 Project No.: 1015-003

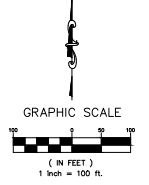
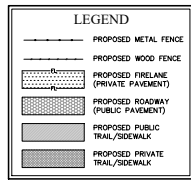
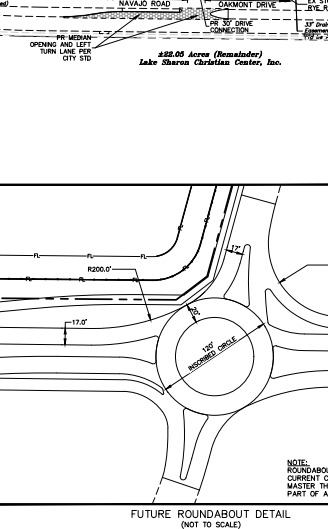
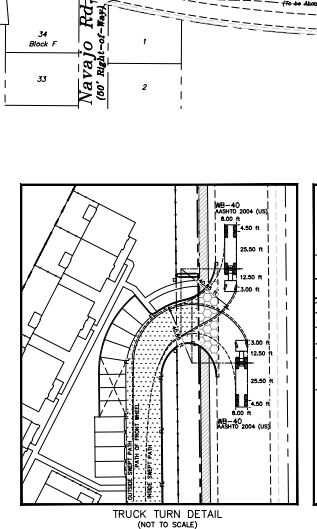


ZONING COMPARISON TABLE

	BASE MF-1	PROP. PD
MINIMUM SETBACKS		
FRONT YARD	30'	25'
REAR YARD	30'	25'
INTERIOR LOT	N/A	N/A
CORNER LOT	30'	N/A
REAR YARD	30'	20'
GARAGE	N/A	N/A
MINIMUM LOT DIMENSIONS		
LOT AREA (A)	40,000	40,000
LOT WIDTH	200'	200'
LOT DEPTH	200'	200'
DENSITY (MAX. D.U./A)	12	8
MINIMUM FLOOR AREA (SF PER DU)	1,550	1,800
MAX. BUILDING HT. (FT STOREYS)	35' / 2	30' / 1
MAX. BUILDING AREA (TOTAL)	40%	25%

SITE DATA SUMMARY TABLE

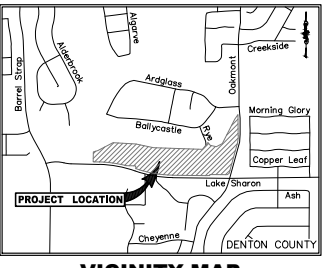
EXISTING ZONING/USE	PROPOSED ZONING	PD #	PD-24 / VACANT
RESIDENTIAL	RESIDENTIAL	PD W/WR	BASE ZONING
COTTAGE COMMUNITY			
2 (EXISTING) / 2 (PROPOSED)			
NUMBER OF LOTS	22		
LOT COVERAGE	22.8%		
GROSS AREA	24,595		1,071,370
NET AREA	23,888		1,040,981
DENSITY	9.00 UNITS/ACRE (w/ROW DEC)		
PROPOSED BUILDINGS (1 STORY)	1,342	30	60
2 BEDROOM (DUPLIX)	1,022	87	87
3 BEDROOM	1,307	68	68
STANDARD GARAGE, DETACHED	831	20	16,620
STANDARD GARAGE W/HANDBICAP, DETACHED	682	1	2,086
LEASING CENTER	1,131	1	1,131
TOTALS	209	215	237,867
PARKING REQUIRED:			
1 BEDROOM (2 SPACES REQ'D; 2 SPACES PROP.)	60	120	120
2 BEDROOM (4 SPACES REQ'D; 2 SPACES PROP.)	87	251	174
3 BEDROOM (4 SPACES REQ'D; 2 SPACES PROP.)	68	272	136
TOTALS	215	643	430
PARKING PROVIDED:			
STANDARD SPACES	234		
CARPPOOL SPACES	144		
HANDICAP SPACES	3		
STANDARD GARAGE SPACES	87		
HANDICAP GARAGE SPACES	47		
TOTALS	475		
GARAGE SPACES PER UNIT:	83,278	8%	(SF, %)
OPEN SPACE AREA REQUIRED:			
EVENT LAWN	2,773	0.3%	(SF, %)
PASSIVE NODES	7,024	0.7%	(SF, %)
ACTIVITY NODES	18,801	1.6%	(SF, %)
DOG PARK	11,969	1.1%	(SF, %)
MISC. OPEN SPACE	142,358	12.7%	(SF, %)
TOTALS	180,925	17.4%	(SF, %)
LANDSCAPE AREA	500,352	48.0%	(SF, %)
TRAIL DEDICATION:			
LAKE SHARON DRIVE	10	2,189	21,890
OAKMONT DRIVE	6	1,617	6,102
TOTALS	16	3,806	27,992
IMPERVIOUS AREA	540,949	52.0%	(SF, %)
LOADING SPACE		NOT REQUIRED	(SF, %)
OUTSIDE STORAGE		NONE PROPOSED	(SF, %)
CONSTRUCTION DATES	01/2021 - 03/2021		



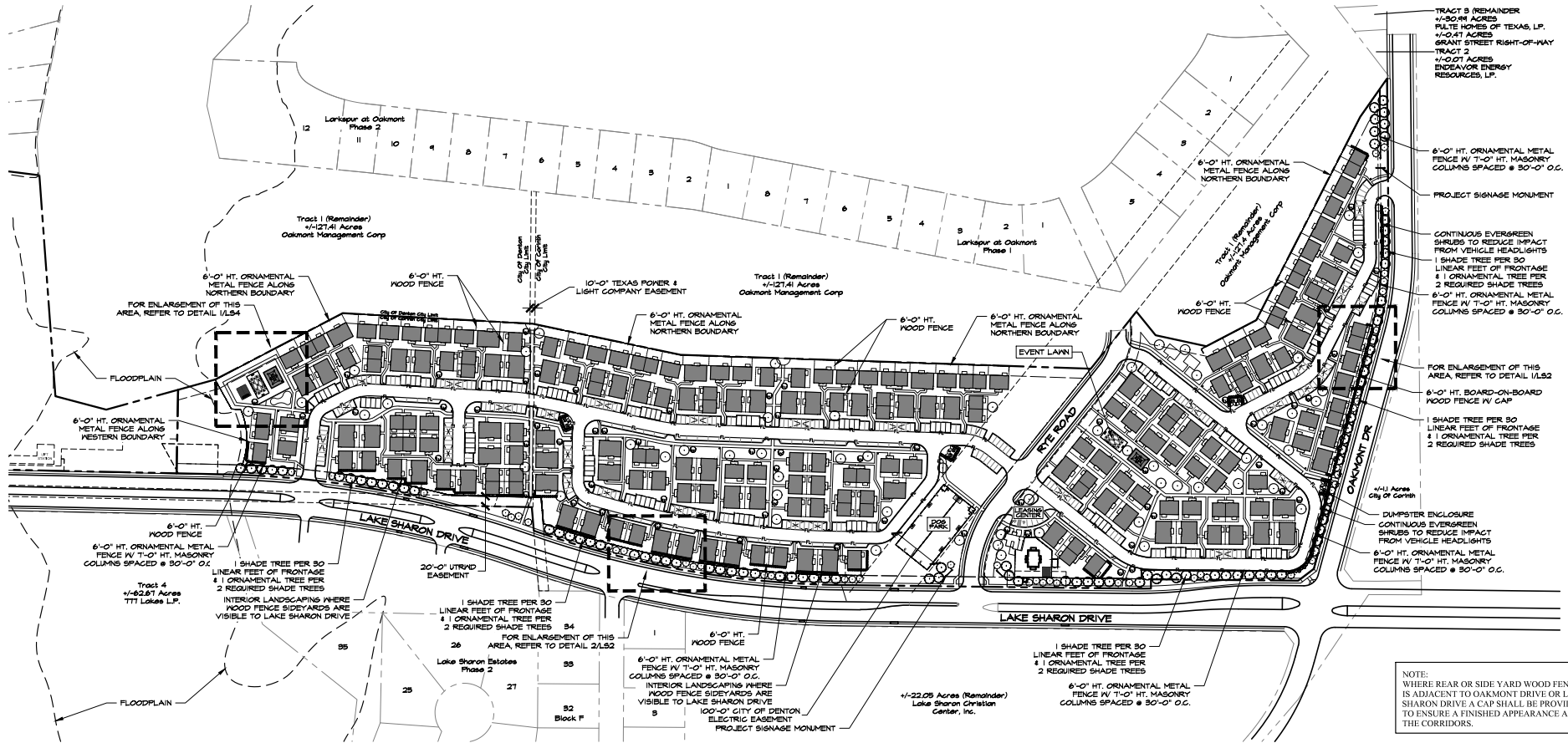
PD-59: EXHIBIT "C" - PD CONCEPT PLAN

OWNER/DEVELOPER:
NexMetro Communities
 2221 Lake Boulevard, Suite 120
 Richardson, Texas 75082
 Tel. No. (972) 795-0038
 Contact: Josh Eadie

**PD CONCEPT PLAN
 AVILLA FAIRWAYS
 LAKE SHARON @ OAKMONT
 24.595 ACRE TRACT OF LAND**
 BERRY MERCHANT SURVEY, ABSTRACT NO. 800
 A.H. SERREN SURVEY, ABSTRACT NO. 1198
**CITY OF CORINTH,
 DENTON COUNTY, TEXAS**
 June 22, 2021



FILE: C:\Users\jwalton\Desktop\Projects\1015003_Corinth\1015003_Concept_Plan.dwg
 User: JAL DATE: 06/22/2021 10:38 AM
 USBC: JAL DATE: 06/22/2021 10:38 AM



NOTE:
WHERE REAR OR SIDE YARD WOOD FENCING IS ADJACENT TO OAKMONT DRIVE OR LAKE SHARON DRIVE A CAP SHALL BE PROVIDED TO ENSURE A FINISHED APPEARANCE ALONG THE CORRIDORS.

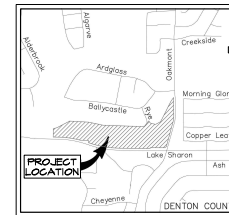
LANDSCAPE REQUIREMENTS

OAKMONT DRIVE

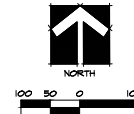
- A. 1-3' GAL. CANOPY TREE / 50 LF OF LANDSCAPE BUFFER
482 LF OF LS BUFFER / 50 LF = 92 - 3' GAL. CANOPY TREES REQUIRED PROVIDED: 32 - 5' CALIFER CANOPY TREES PROVIDED
- B. 1-2' GAL. ORNAMENTAL TREE FOR EVERY 2 CANOPY TREES REQUIRED
32 - 3' GAL. CANOPY TREES / 2 = 16 - 2' GAL. ORNAMENTAL TREES REQUIRED PROVIDED: 16 - 2' CALIFER ORNAMENTAL TREES PROVIDED

LAKE SHARON DRIVE

- A. 1-3' GAL. CANOPY TREE / 50 LF OF LANDSCAPE BUFFER
2,000 LF OF LS BUFFER / 50 LF = 66 - 3' GAL. CANOPY TREES REQUIRED PROVIDED: 66 - 3' CALIFER CANOPY TREES PROVIDED
- B. 1-2' GAL. ORNAMENTAL TREE FOR EVERY 2 CANOPY TREES REQUIRED
66 - 3' GAL. CANOPY TREES / 2 = 33 - 2' GAL. ORNAMENTAL TREES REQUIRED PROVIDED: 33 - 2' CALIFER ORNAMENTAL TREES PROVIDED



LOCATION MAP
NOT TO SCALE



Bar is one inch on original drawing. If not one inch on this sheet, adjust scale as necessary.

NOTE:
ALL IMPROVEMENTS SHOWN ON THIS PLAN ARE CONCEPTUAL IN NATURE & SUBJECT TO CHANGE.

STUDIO DESIGN GROUP

Studio 13 Design Group, PLLC
360 W. Main Street
Dentonville, Texas 76207
469-631-1900

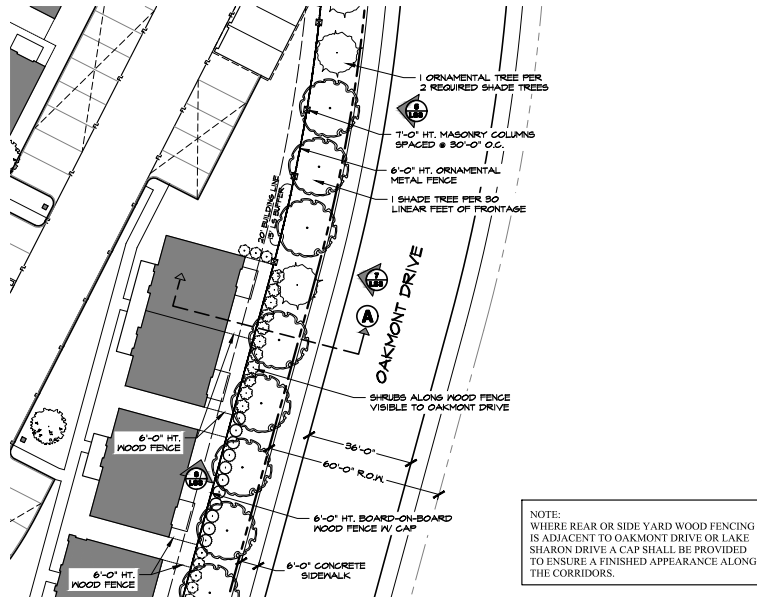
TRAE Firm #18643



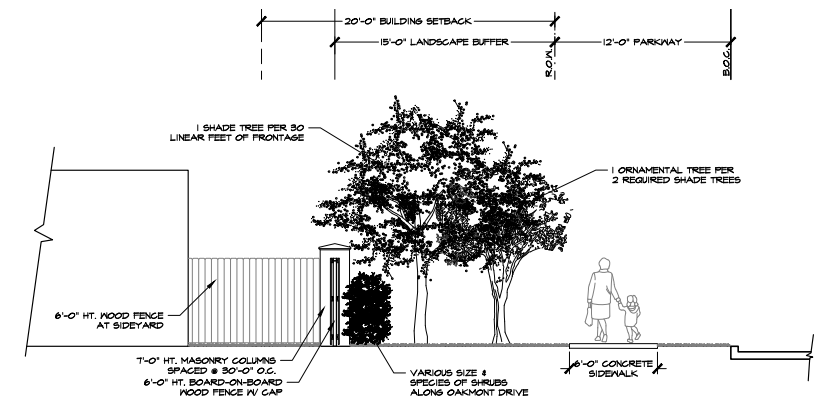
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June 29, 2021

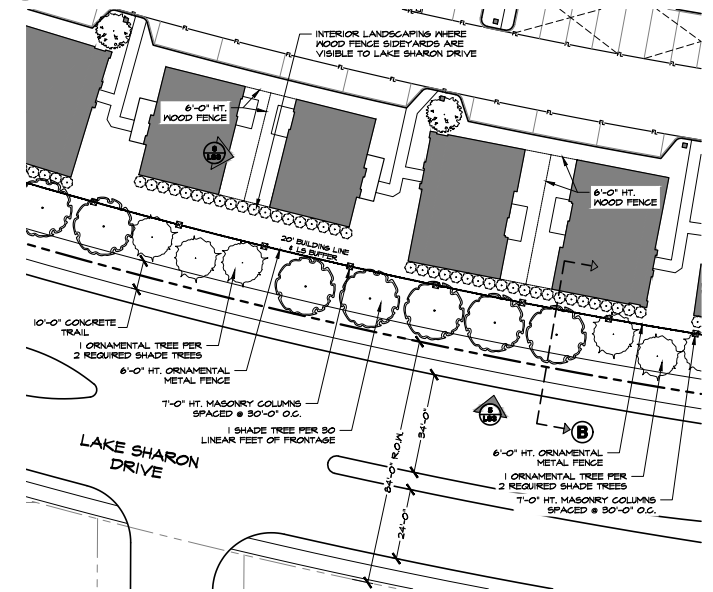
L.S1



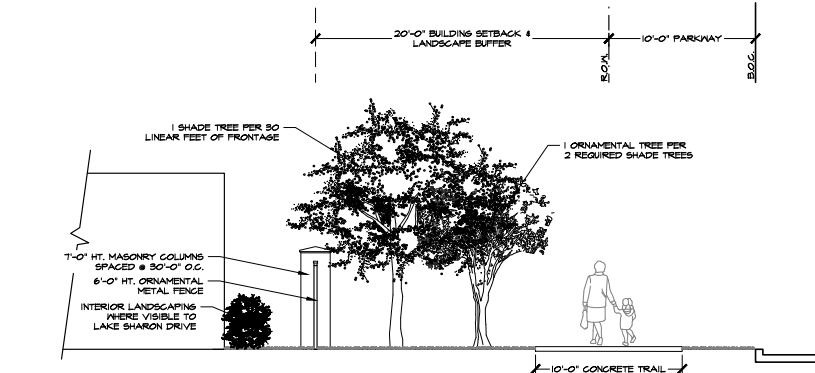
1 OAKMONT DRIVE FRONTAGE PLAN
SCALE: 1"=20'-0"



A LANDSCAPE SECTION - OAKMONT DRIVE FRONTAGE SECTION
SCALE: 1/4"=1'-0"

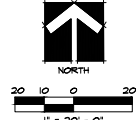
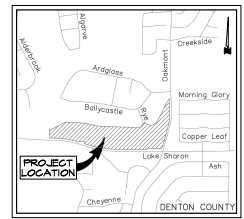


2 LAKE SHARON DRIVE FRONTAGE PLAN
SCALE: 1"=20'-0"



B LANDSCAPE SECTION - LAKE SHARON DRIVE FRONTAGE SECTION
SCALE: 1/4"=1'-0"

AVILLA FAIRWAYS
~CONCEPTUAL LANDSCAPE PLAN~



Bar is one inch on original drawing. If not one inch on this sheet, adjust scale as necessary.

NOTE: ALL IMPROVEMENTS SHOWN ON THIS PLAN ARE CONCEPTUAL IN NATURE & SUBJECT TO CHANGE.



Studio 13 Design Group, P.L.L.C.
386 W. Main Street
Lombard, Texas 75077
469-633-1900

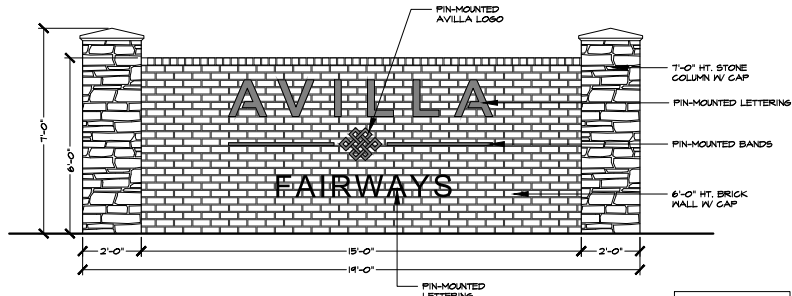
TRAE Firm #R8643



June 29, 2021

507

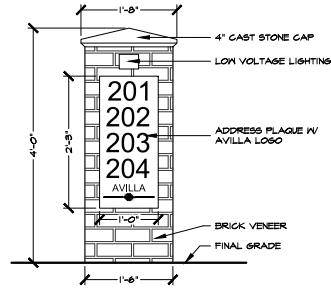
L.S.2



FONT, LOGO AND SIZE ARE CONCEPTUAL & SUBJECT TO CHANGE

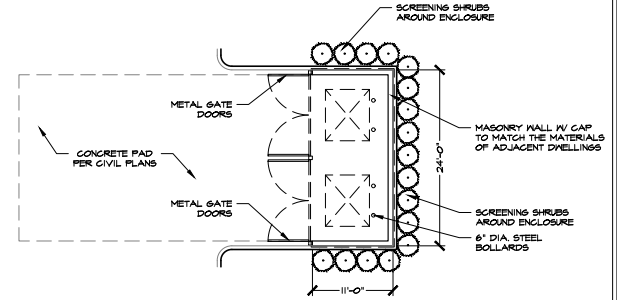
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1 PROJECT SIGNAGE MONUMENT ELEVATION



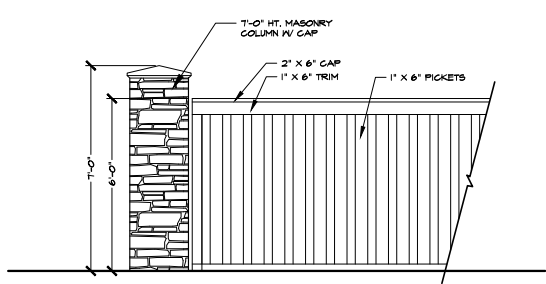
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2 UNIT PORTAL SIGN ELEVATION



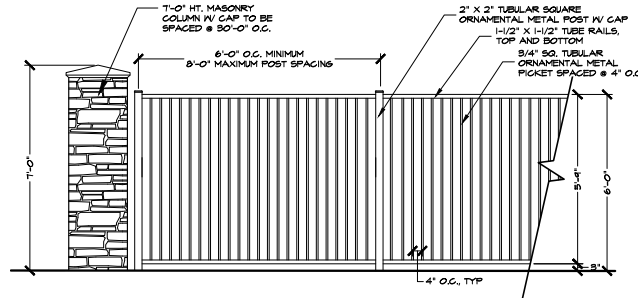
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3 DUMPSTER ENCLOSURE PLAN



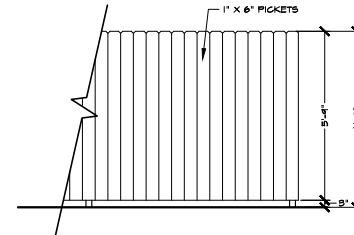
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4 6'-0" HT. BOARD-ON-BOARD WOOD FENCE ALONG FRONTAGE ELEVATION



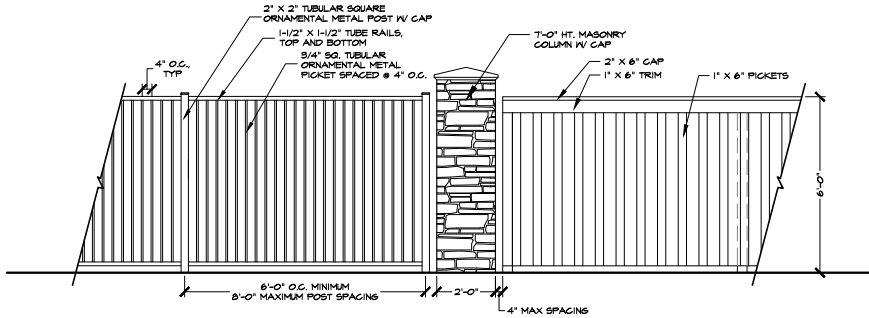
SCALE: 1/2"=1'-0"

5 6'-0" HT. ORNAMENTAL METAL FENCE ALONG FRONTAGE ELEVATION



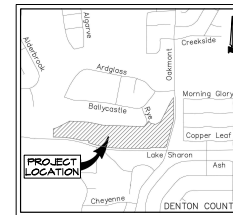
SCALE: 1/2"=1'-0"

6 6'-0" HT. WOOD FENCE (INTERNAL YARDS) ELEVATION



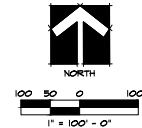
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7 PARTIAL ELEVATION: WOOD FENCING TRANSITION TO METAL FENCING ELEVATION



LOCATION MAP NOT TO SCALE

AVILLA FAIRWAYS
~CONCEPTUAL FENCING & MONUMENTATION~



Bar is one inch on original drawing. If not one inch on this sheet, adjust scale as necessary.

NOTE: ALL IMPROVEMENTS SHOWN ON THIS PLAN ARE CONCEPTUAL IN NATURE & SUBJECT TO CHANGE.

STUDIO DESIGN GROUP

Studio 13 Design Group, P.L.L.C.
366 W. Main Street
Lewisville, Texas 75057
469-653-1900

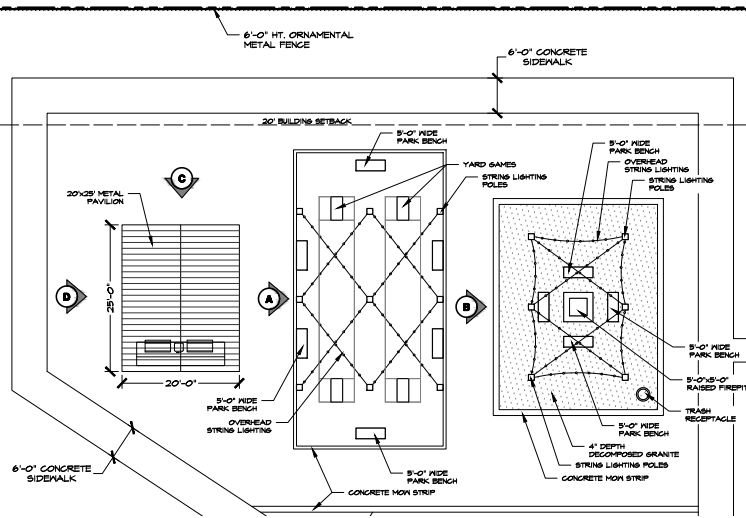
TRAE Firm #R8643



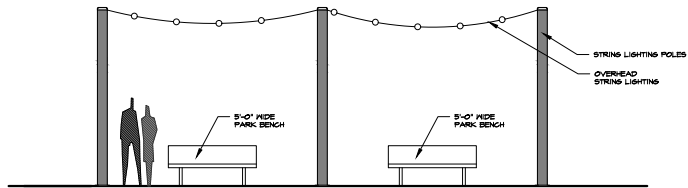
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June 29, 2021

L.S.3

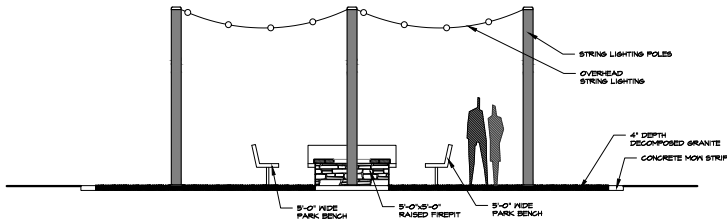


1 AMENITY AREA ENLARGEMENT PLAN



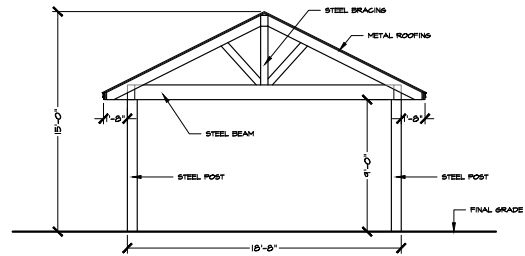
A LIGHTED LAWN AREA ELEVATION

SCALE: 1/4"=1'-0"



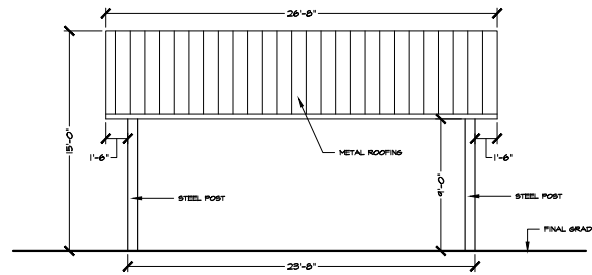
B FIRE PIT AREA ELEVATION

SCALE: 1" = 1'-0"



C METAL PAVILION - END VIEW ELEVATION

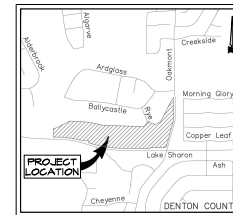
SCALE: 1/4"=1'-0"



D METAL PAVILION - SIDE VIEW ELEVATION

SCALE: 1/4"=1'-0"

AVILLA FAIRWAYS
~CONCEPTUAL AMENITIES~



LOCATION MAP
NOT TO SCALE

Bar is one inch on original drawing. If not one inch on this sheet, adjust scale as necessary.
One Inch

NOTE:
ALL IMPROVEMENTS SHOWN ON THIS PLAN ARE CONCEPTUAL IN NATURE & SUBJECT TO CHANGE.

STUDIO
DESIGN GROUP

Studio 13 Design Group, P.L.L.C.
386 W. Main Street
Lawville, Texas 75057
409-633-1900

TBAE Firm #R643



June 29, 2021

LS4

<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>0% FIBER CEMENT SIDING 70% BRICK 30% STONE</p>	<p>LEFT ELEVATION</p> <p>0% FIBER CEMENT SIDING 70% BRICK 30% STONE</p>	<p>FRONT ELEVATION</p> <p>5% FIBER CEMENT SIDING 49% BRICK 46% STONE</p>
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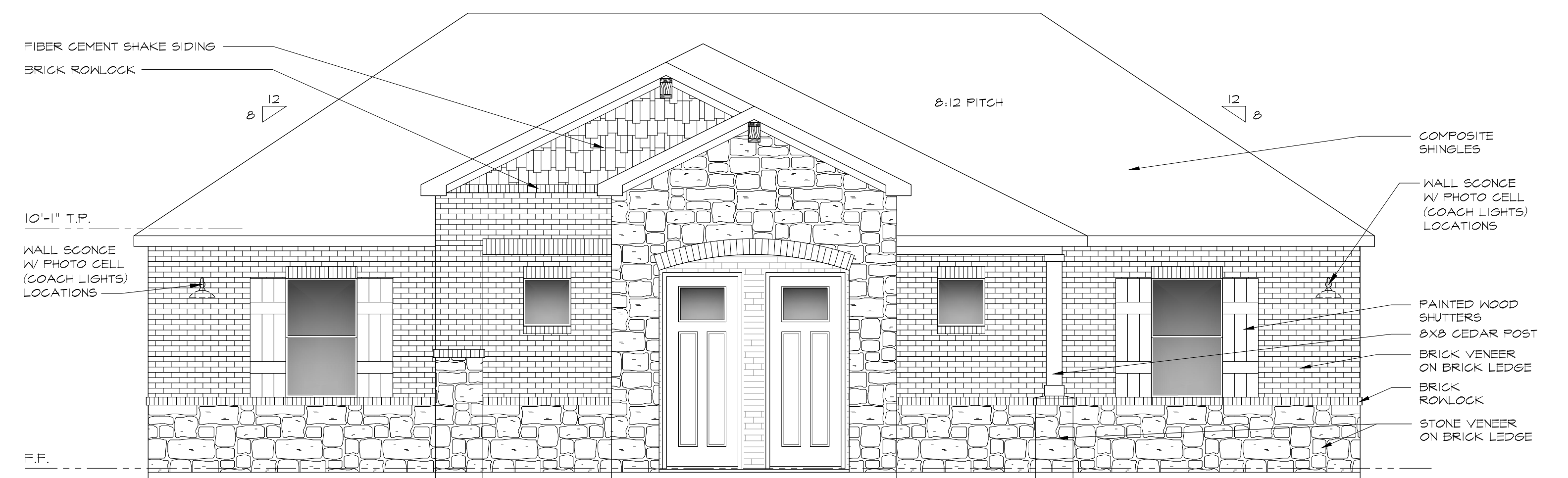
ALTERNATE ROOF PITCH



02 FRONT ELEVATION (ALTERNATE)

<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>0% FIBER CEMENT SIDING 70% BRICK 30% STONE</p>	<p>LEFT ELEVATION</p> <p>0% FIBER CEMENT SIDING 70% BRICK 30% STONE</p>	<p>FRONT ELEVATION</p> <p>3% FIBER CEMENT SIDING 50% BRICK 47% STONE</p>
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STANDARD ROOF PITCH



01 FRONT ELEVATION (STANDARD)

SCALE: 1/4" = 1'-0"

BUILDING TYPE IA ELEVATIONS

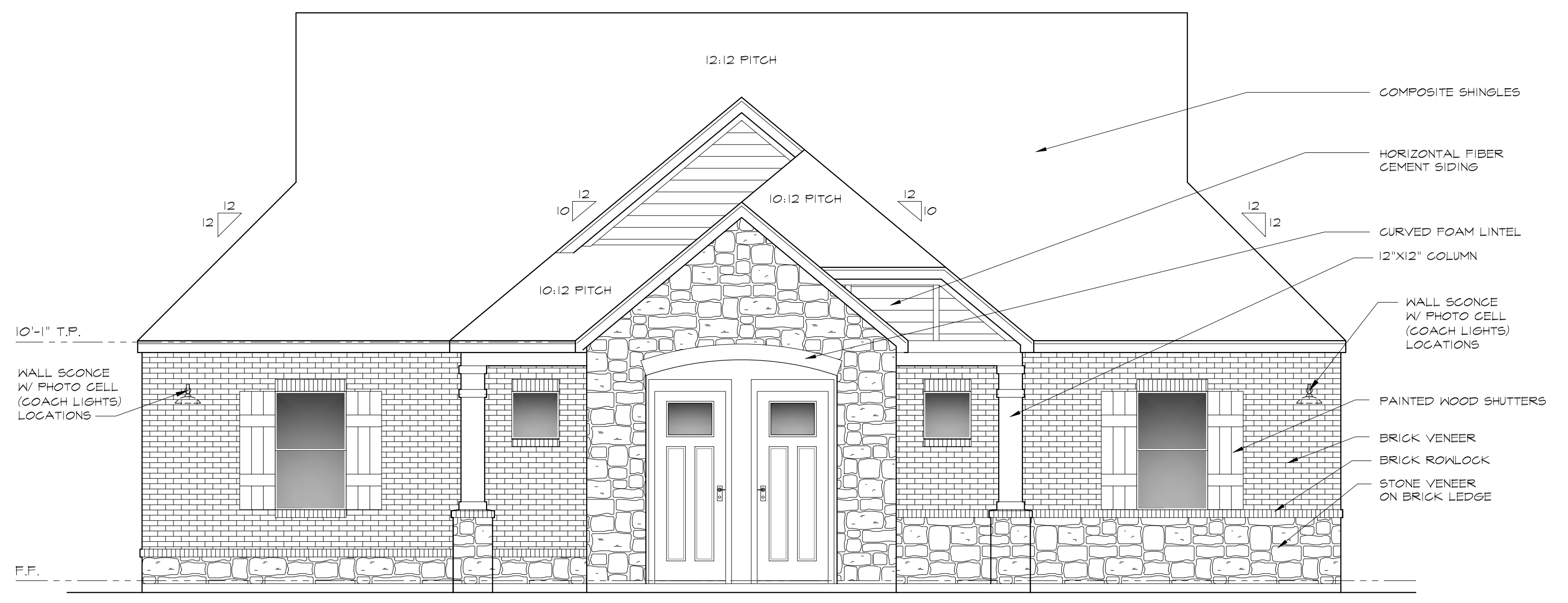
PD-59: EXHIBIT "E" -ELEVATIONS

SCALE: 1/4" = 1'-0"

<p>ARCHITECT IKEMIRE ARCHITECTS 16660 Dallas Parkway, Suite 2900 Dallas TX 75001 PH 972-248-2486 FX 972-248-1557 CONTACT: JAMIE HULSE</p>	<p>DEVELOPER NEXMETRO COMMUNITIES THREE ENERGY SQUARE 6688 N. CENTRAL EXPRESSWAY, SUITE 275 Addison TX 75206 PH 972-745-0039 CONTACT: JASON FLORY</p>	<p>NEXMETRO AVILLA FAIRWAYS BUILDING ELEVATIONS</p>
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<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>5% FIBER CEMENT SIDING 85% BRICK 10% STONE</p>	<p>RIGHT ELEVATION</p> <p>5% FIBER CEMENT SIDING 85% BRICK 10% STONE</p>	<p>FRONT ELEVATION</p> <p>10% FIBER CEMENT SIDING 60% BRICK 40% STONE</p>
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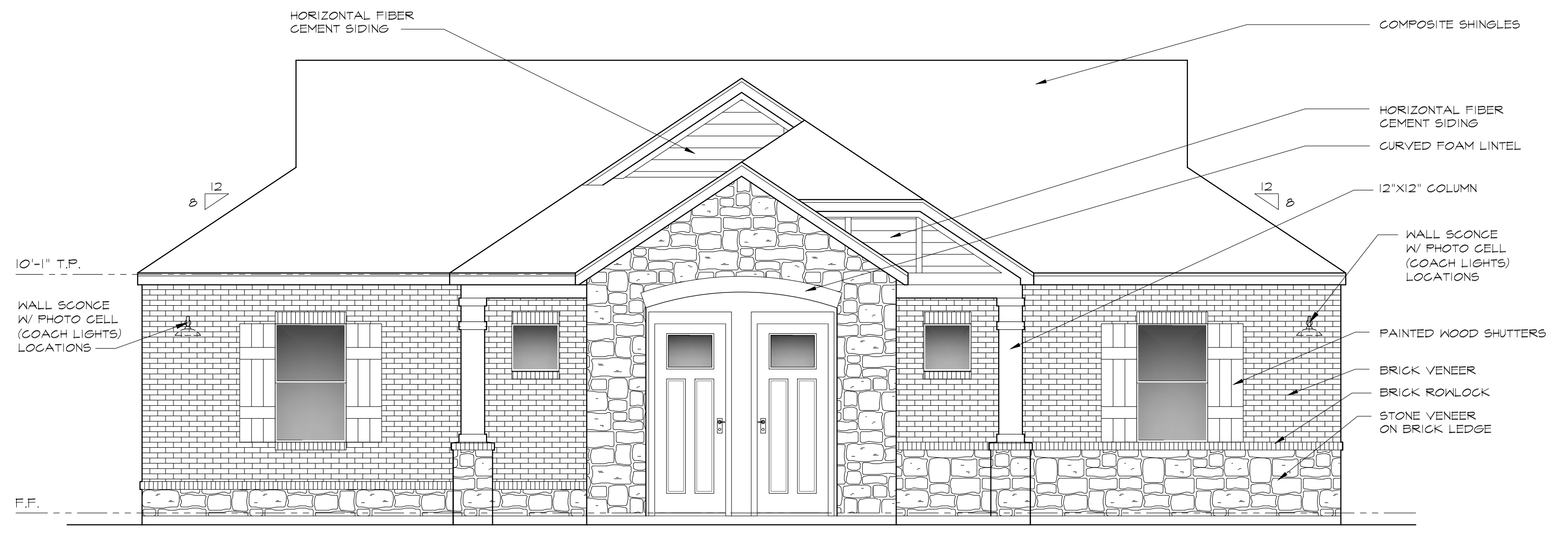
ALTERNATE ROOF PITCH



02 FRONT ELEVATION (ALTERNATE)
SCALE: 1/4" = 1'-0"

<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>3% FIBER CEMENT SIDING 86% BRICK 11% STONE</p>	<p>RIGHT ELEVATION</p> <p>3% FIBER CEMENT SIDING 86% BRICK 11% STONE</p>	<p>FRONT ELEVATION</p> <p>10% FIBER CEMENT SIDING 60% BRICK 40% STONE</p>
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STANDARD ROOF PITCH



01 FRONT ELEVATION (STANDARD)
SCALE: 1/4" = 1'-0"

BUILDING TYPE IB ELEVATIONS

SCALE: 1/4" = 1'-0"

PD-59: EXHIBIT "E" -ELEVATIONS

<p>ARCHITECT IKEMIRE ARCHITECTS 16660 Dallas Parkway, Suite 2900 Dallas TX 75001 PH 972-248-2486 FX 972-248-1557 CONTACT: JAMIE HULSE</p>	<p>DEVELOPER NEXMETRO COMMUNITIES THREE ENERGY SQUARE 6688 N. CENTRAL EXPRESSWAY, SUITE 275 Addison TX 75206 PH 972-745-0039 CONTACT: JASON FLORY</p>	<p>NEXMETRO AVILLA FAIRWAYS BUILDING ELEVATIONS</p>
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<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>21% FIBER CEMENT SIDING 76% BRICK 3% STONE</p>	<p>RIGHT ELEVATION</p> <p>21% FIBER CEMENT SIDING 76% BRICK 3% STONE</p>	<p>FRONT ELEVATION</p> <p>14% FIBER CEMENT SIDING 82% BRICK 4% STONE</p>
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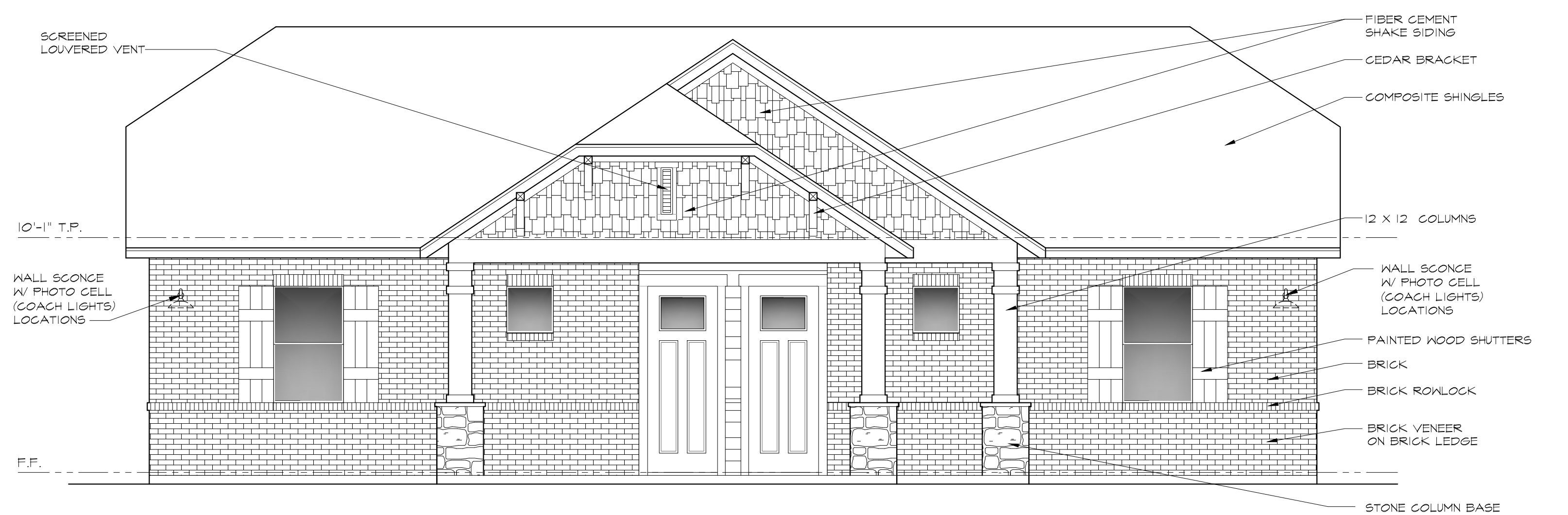
ALTERNATE ROOF PITCH



02 FRONT ELEVATION (ALTERNATE)
SCALE: 1/4"=1'-0"

<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>20% FIBER CEMENT SIDING 77% BRICK 3% STONE</p>	<p>RIGHT ELEVATION</p> <p>20% FIBER CEMENT SIDING 77% BRICK 3% STONE</p>	<p>FRONT ELEVATION</p> <p>12% FIBER CEMENT SIDING 83% BRICK 5% STONE</p>
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STANDARD ROOF PITCH



01 FRONT ELEVATION (STANDARD)
SCALE: 1/4"=1'-0"

BUILDING TYPE 1C ELEVATIONS

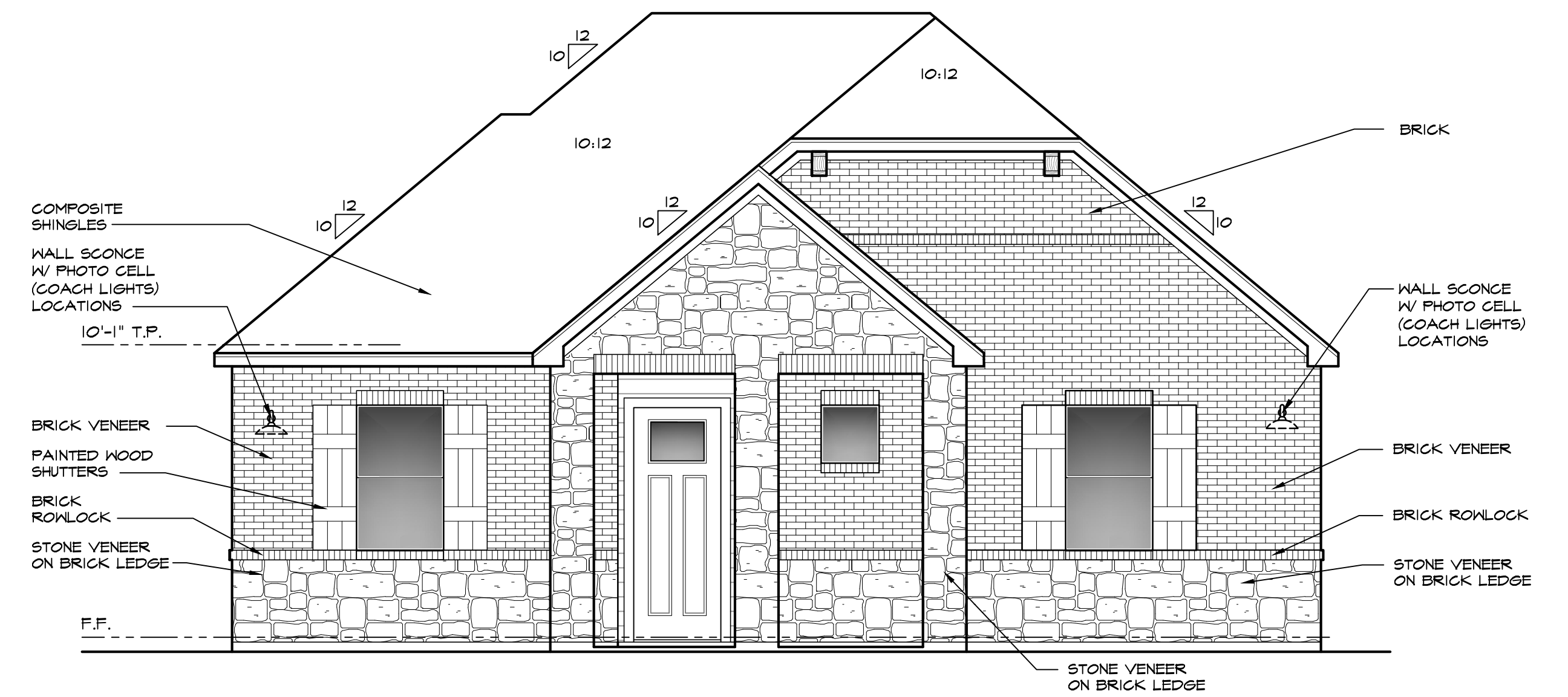
SCALE: 1/4" = 1'-0"

PD-59: EXHIBIT "E" -ELEVATIONS

<p>ARCHITECT IKEMIRE ARCHITECTS 16660 Dallas Parkway, Suite 2900 Dallas TX 75001 PH 972-248-2486 FX 972-248-1557 CONTACT: JAMIE HULSE</p>	<p>DEVELOPER NEXMETRO COMMUNITIES THREE ENERGY SQUARE 6688 N. CENTRAL EXPRESSWAY, SUITE 275 Addison TX 75206 PH 972-745-0039 CONTACT: JASON FLORY</p>	<p>NEXMETRO AVILLA FAIRWAYS BUILDING ELEVATIONS</p>
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<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK AND STONE 94% BRICK AND 6% STONE</p>	<p>RIGHT ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK AND STONE 95% BRICK AND 5% STONE</p>	<p>FRONT ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK AND STONE (58% BRICK, 42% STONE)</p>
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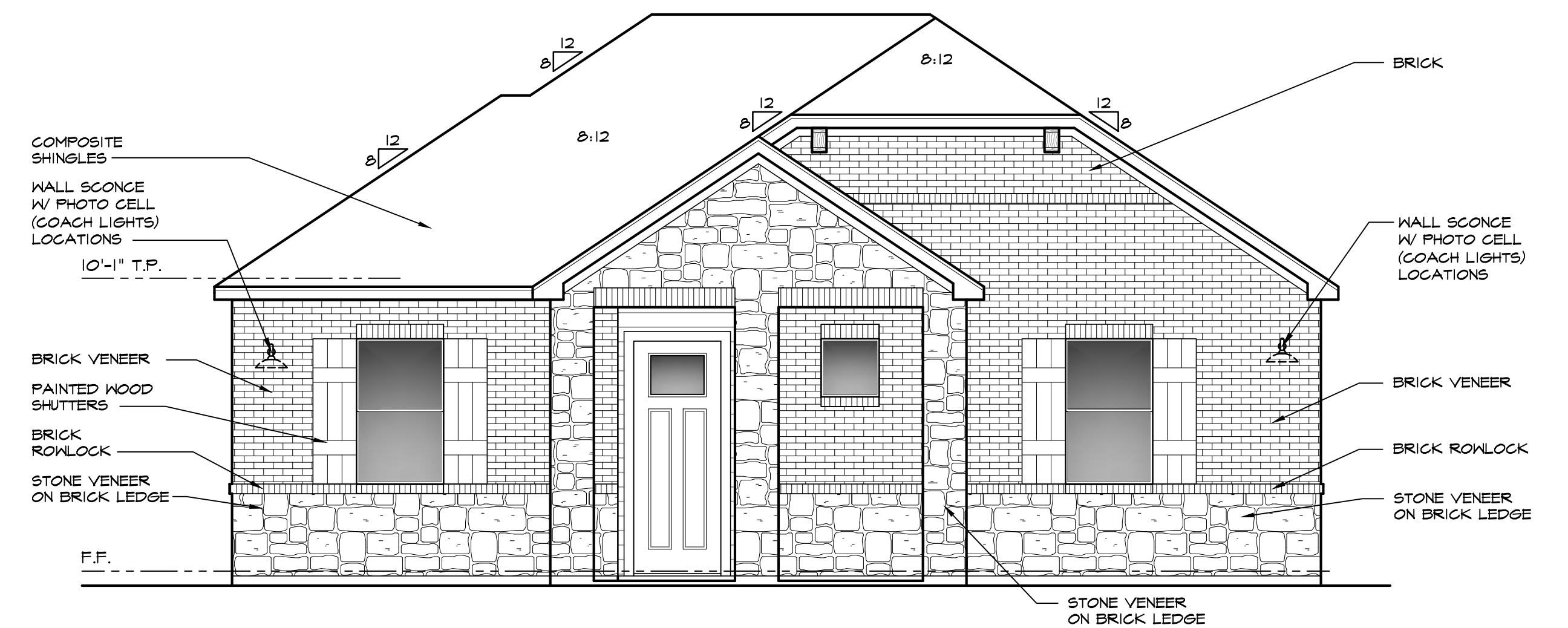
ALTERNATE ROOF PITCH



02 FRONT ELEVATION (ALTERNATE)

<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK AND STONE 94% BRICK AND 6% STONE</p>	<p>RIGHT ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK AND STONE 94% BRICK AND 6% STONE</p>	<p>FRONT ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK AND STONE (60% BRICK, 40% STONE)</p>
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STANDARD ROOF PITCH



01 FRONT ELEVATION (STANDARD)

BUILDING TYPE 2A ELEVATIONS

PD-59: EXHIBIT "E" -ELEVATIONS

SCALE: 1/4" = 1'-0"

<p>ARCHITECT IKEMIRE ARCHITECTS 16660 Dallas Parkway, Suite 2900 Dallas TX 75001 PH 972-248-2486 FX 972-248-1557 CONTACT: JAMIE HULSE</p>	<p>DEVELOPER NEXMETRO COMMUNITIES THREE ENERGY SQUARE 6688 N. CENTRAL EXPRESSWAY, SUITE 275 Addison TX 75206 PH 972-795-0039 CONTACT: JASON FLORY</p>	<p>NEXMETRO AVILLA FAIRWAYS BUILDING ELEVATIONS</p>
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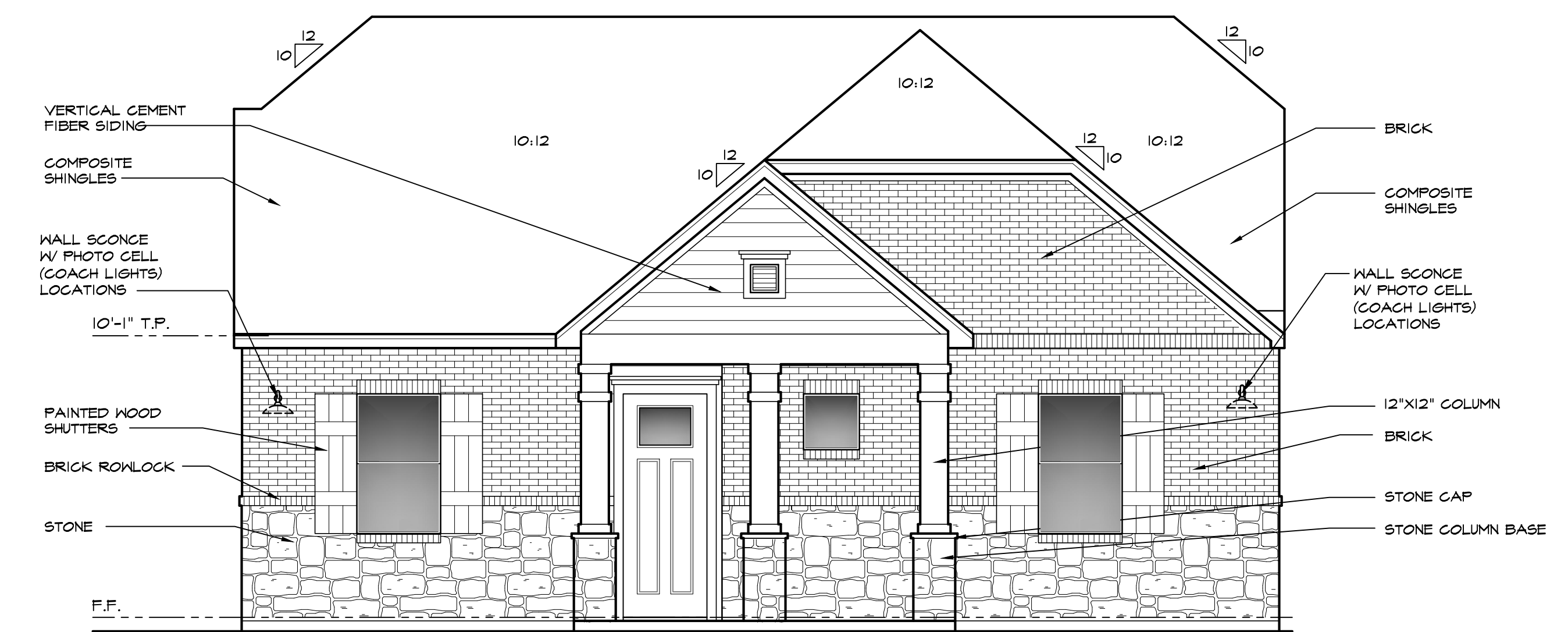
REAR ELEVATION
 0% FIBER CEMENT SIDING
 100% BRICK
 0% STONE

LEFT ELEVATION
 25% FIBER CEMENT SIDING
 75% BRICK AND STONE
 71% BRICK AND 4% STONE

RIGHT ELEVATION
 24% FIBER CEMENT SIDING
 76% BRICK AND STONE
 72% BRICK AND 4% STONE

FRONT ELEVATION
 8% FIBER CEMENT SIDING
 92% BRICK AND STONE
 (66% BRICK, 26% STONE)

ALTERNATE ROOF PITCH



02 FRONT ELEVATION (ALTERNATE)
 SCALE: 1/4"=1'-0"

FRONT PORCH 78 S.F.

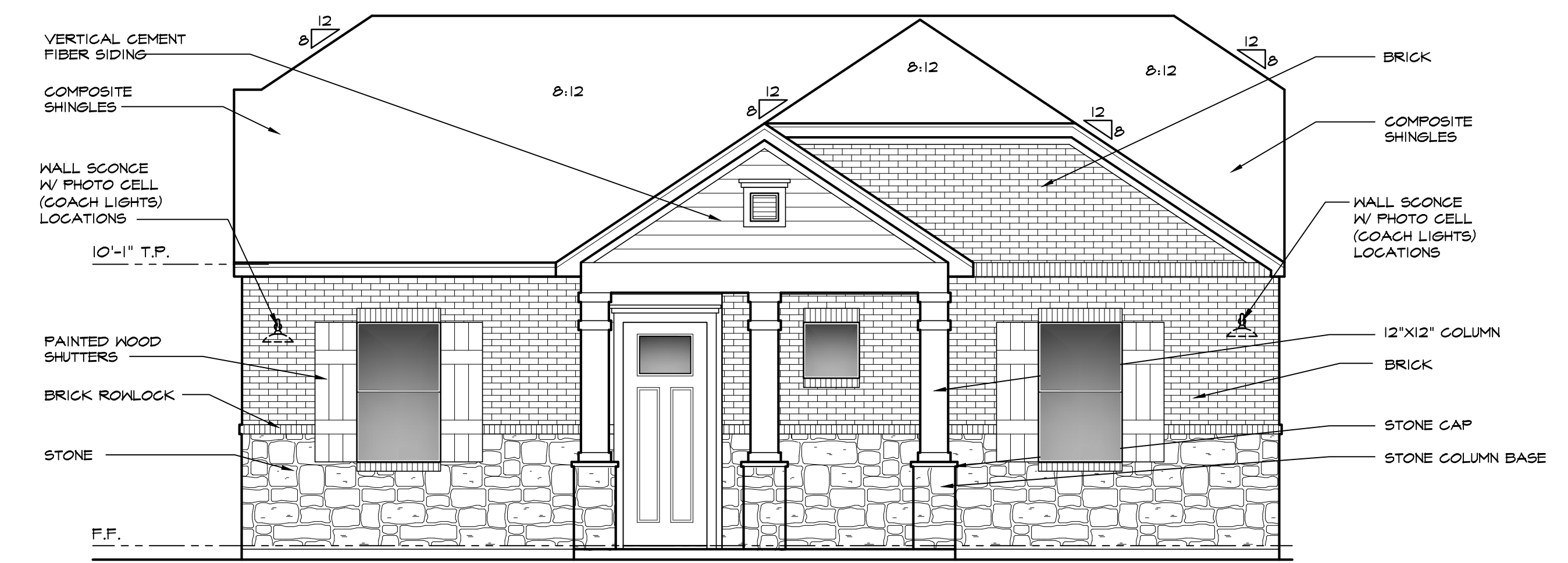
REAR ELEVATION
 0% FIBER CEMENT SIDING
 100% BRICK
 0% STONE

LEFT ELEVATION
 25% FIBER CEMENT SIDING
 75% BRICK AND STONE
 71% BRICK AND 4% STONE

RIGHT ELEVATION
 23% FIBER CEMENT SIDING
 77% BRICK AND STONE
 73% BRICK AND 4% STONE

FRONT ELEVATION
 6% FIBER CEMENT SIDING
 94% BRICK AND STONE
 (61% BRICK, 33% STONE)

STANDARD ROOF PITCH



01 FRONT ELEVATION (STANDARD)
 SCALE: 1/4"=1'-0"

FRONT PORCH 78 S.F.

BUILDING TYPE 2B ELEVATIONS

PD-59: EXHIBIT "E" -ELEVATIONS

SCALE: 1/4" = 1'-0"

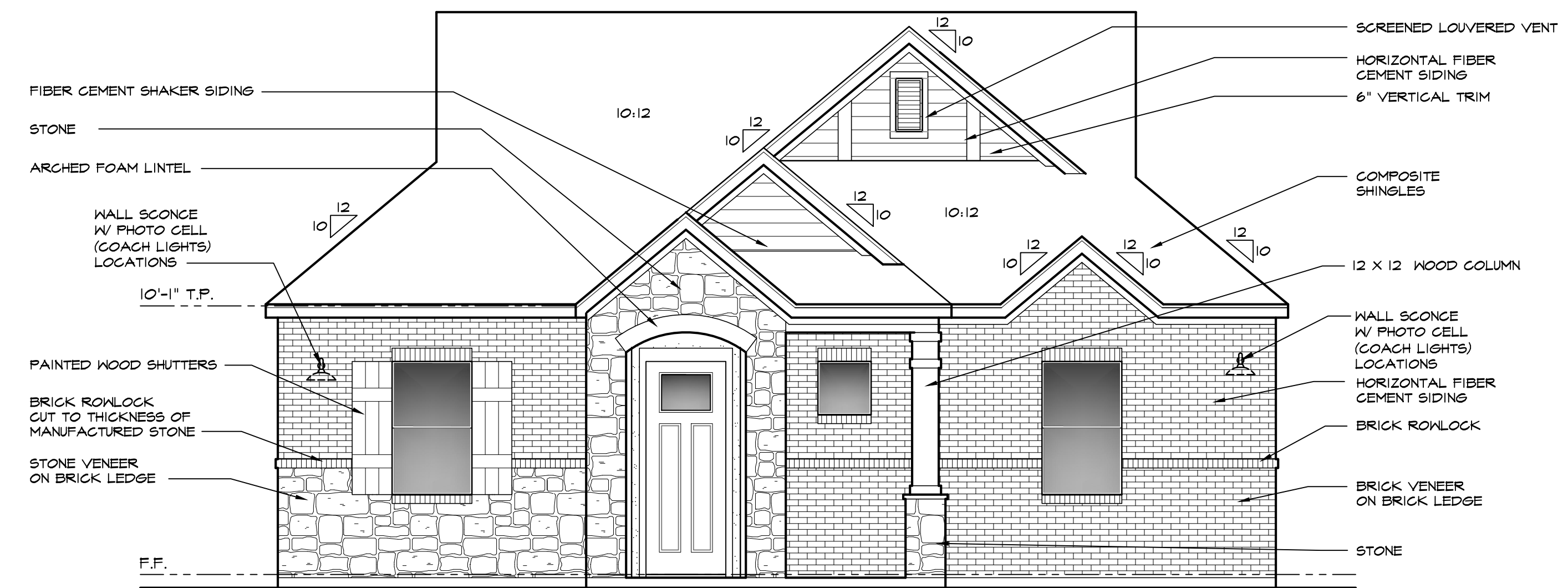
ARCHITECT
 IKEMIRE ARCHITECTS
 16660 Dallas Parkway, Suite 2900
 Dallas TX 75001
 PH 972-248-2486 FX 972-248-1557
 CONTACT: JAMIE HULSE

DEVELOPER
 NEXMETRO COMMUNITIES
 THREE ENERGY SQUARE
 6688 N. CENTRAL EXPRESSWAY, SUITE 275
 Addison TX 75206
 PH 972-795-0039
 CONTACT: JASON FLORY

NEXMETRO
 AVILLA FAIRWAYS
 BUILDING ELEVATIONS

<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>9% FIBER CEMENT SIDING 91% BRICK AND STONE 83% BRICK AND 8% STONE</p>	<p>RIGHT ELEVATION</p> <p>10% FIBER CEMENT SIDING 90% BRICK AND STONE 88% BRICK AND 2% STONE</p>	<p>FRONT ELEVATION</p> <p>5% FIBER CEMENT SIDING 95% BRICK AND STONE (84% BRICK, 11% STONE)</p>
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ALTERNATE ROOF PITCH



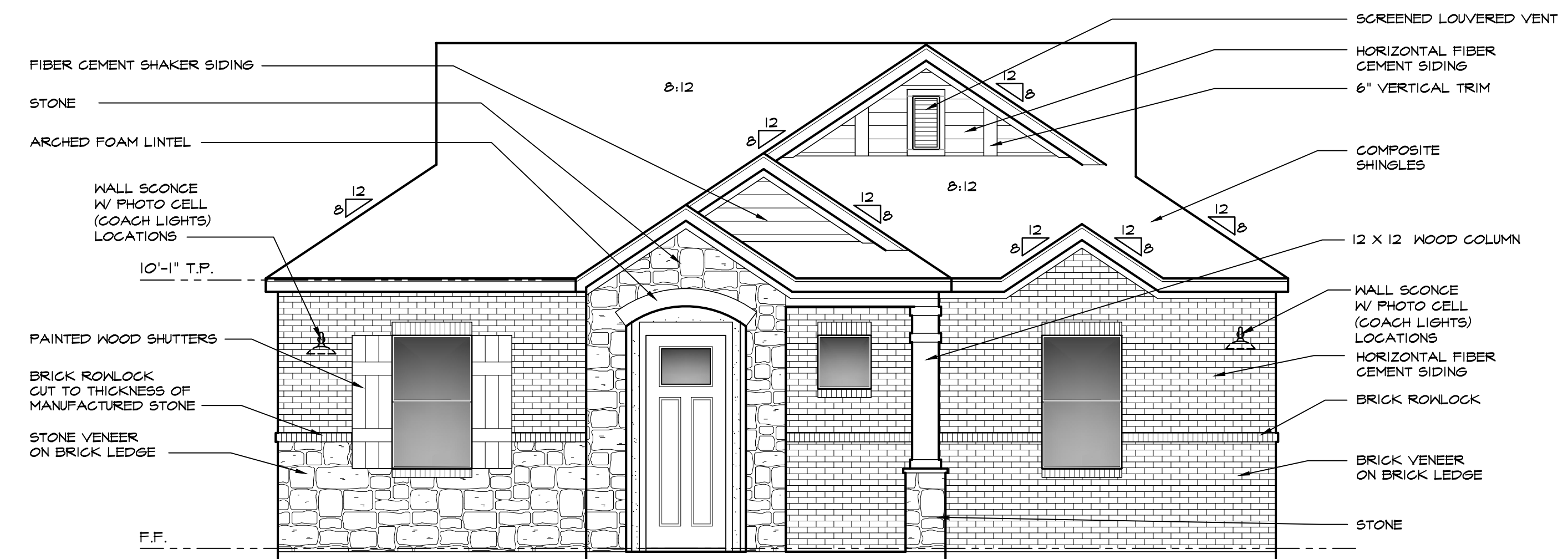
02 FRONT ELEVATION (ALTERNATE)

FRONT PORCH 74 S.F.

SCALE: 1/4"=1'-0"

<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>9% FIBER CEMENT SIDING 91% BRICK AND STONE 83% BRICK AND 8% STONE</p>	<p>RIGHT ELEVATION</p> <p>9% FIBER CEMENT SIDING 91% BRICK AND STONE 89% BRICK AND 2% STONE</p>	<p>FRONT ELEVATION</p> <p>4% FIBER CEMENT SIDING 96% BRICK AND STONE (65% BRICK, 35% STONE)</p>
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STANDARD ROOF PITCH



01 FRONT ELEVATION (STANDARD)

FRONT PORCH 74 S.F.

SCALE: 1/4"=1'-0"

BUILDING TYPE 2C ELEVATIONS

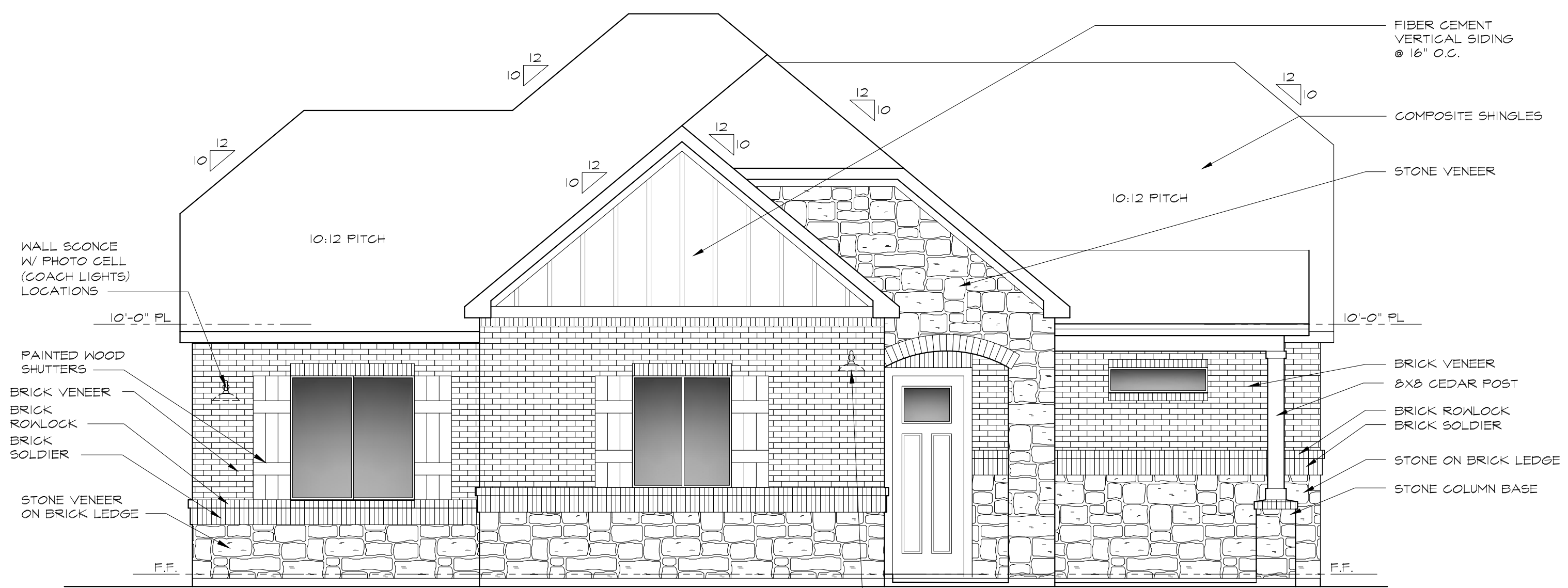
SCALE: 1/4" = 1'-0"

PD-59: EXHIBIT "E" -ELEVATIONS

<p>ARCHITECT IKEMIRE ARCHITECTS 16660 Dallas Parkway, Suite 2900 Dallas TX 75001 PH 972-248-2486 FX 972-248-1557 CONTACT: JAMIE HULSE</p>	<p>DEVELOPER NEXMETRO COMMUNITIES THREE ENERGY SQUARE 6688 N. CENTRAL EXPRESSWAY, SUITE 275 Addison TX 75206 PH 972-795-0039 CONTACT: JASON FLORY</p>	<p>NEXMETRO AVILLA FAIRWAYS BUILDING ELEVATIONS</p>
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<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>11% FIBER CEMENT SIDING 49% BRICK 40% STONE</p>	<p>RIGHT ELEVATION</p> <p>15% FIBER CEMENT SIDING 45% BRICK 40% STONE</p>	<p>FRONT ELEVATION</p> <p>12% FIBER CEMENT SIDING 40% BRICK 48% STONE</p>
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ALTERNATE ROOF PITCH

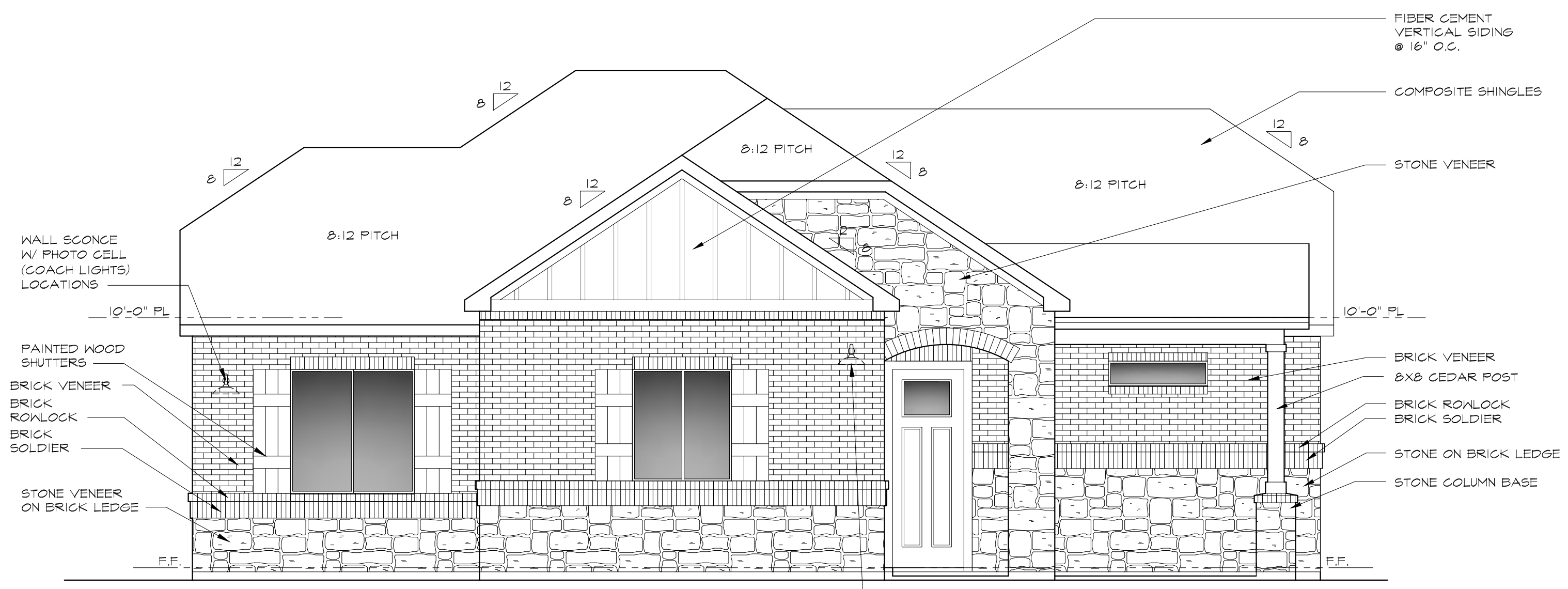


02 FRONT ELEVATION (ALTERNATE)

SCALE: 1/4"=1'-0"

<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>10% FIBER CEMENT SIDING 47% BRICK 43% STONE</p>	<p>RIGHT ELEVATION</p> <p>20% FIBER CEMENT SIDING 45% BRICK 35% STONE</p>	<p>FRONT ELEVATION</p> <p>09% FIBER CEMENT SIDING 47% BRICK 44% STONE</p>
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STANDARD ROOF PITCH



01 FRONT ELEVATION (STANDARD)

SCALE: 1/4"=1'-0"

BUILDING TYPE 3A ELEVATIONS

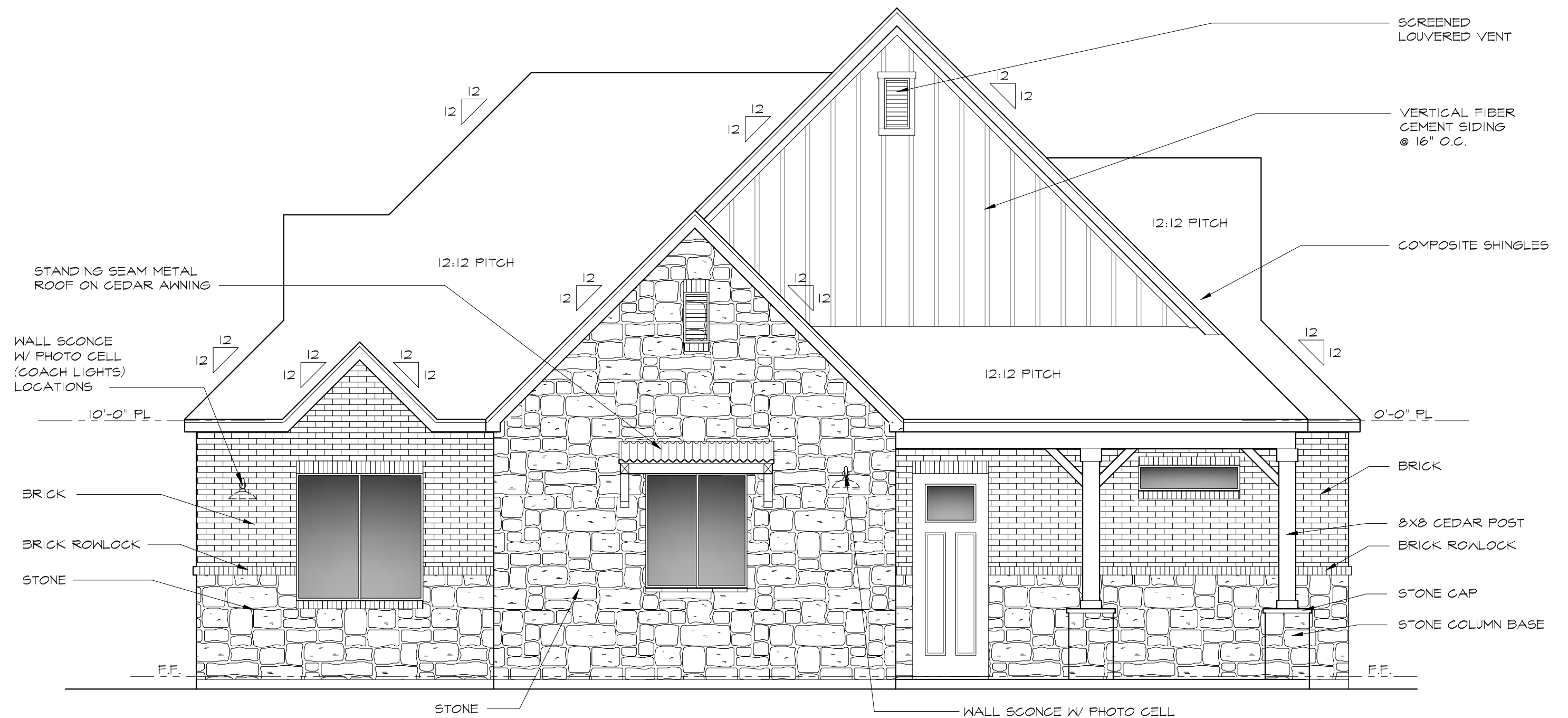
PD-59: EXHIBIT "E" -ELEVATIONS

SCALE: 1/4" = 1'-0"

<p>ARCHITECT IKEMIRE ARCHITECTS 16660 Dallas Parkway, Suite 2900 Dallas TX 75001 PH 972-248-2486 FX 972-248-1557 CONTACT: JAMIE HULSE</p>	<p>DEVELOPER NEXMETRO COMMUNITIES THREE ENERGY SQUARE 6688 N. CENTRAL EXPRESSWAY, SUITE 275 Addison TX 75206 PH 972-745-0039 CONTACT: JASON FLORY</p>	<p>NEXMETRO AVILLA FAIRWAYS BUILDING ELEVATIONS</p>
---	---	---

<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>7% FIBER CEMENT SIDING 42% BRICK 45% STONE</p>	<p>RIGHT ELEVATION</p> <p>7% FIBER CEMENT SIDING 42% BRICK 45% STONE</p>	<p>FRONT ELEVATION</p> <p>11% FIBER CEMENT SIDING 49% BRICK 40% STONE</p>
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ALTERNATE ROOF PITCH

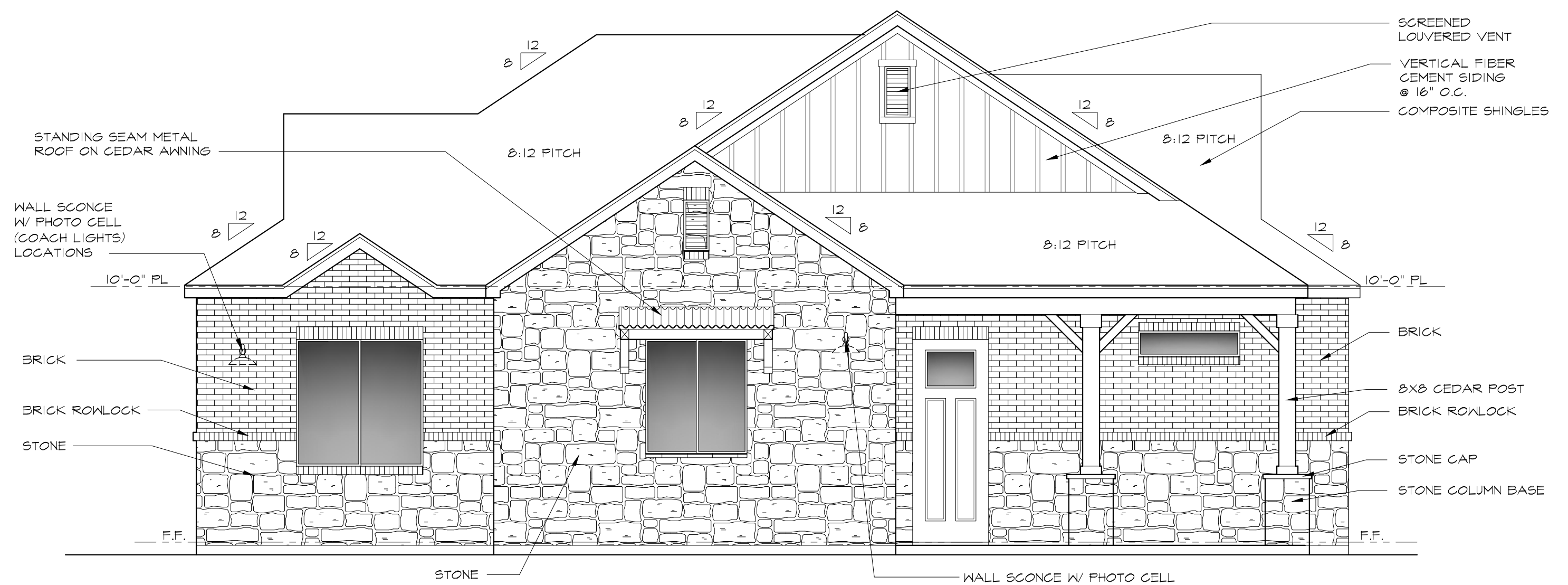


02 FRONT ELEVATION (ALTERNATE)

SCALE: 1/4"=1'-0"

<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>5% FIBER CEMENT SIDING 55% BRICK 40% STONE</p>	<p>RIGHT ELEVATION</p> <p>5% FIBER CEMENT SIDING 50% BRICK 45% STONE</p>	<p>FRONT ELEVATION</p> <p>10% FIBER CEMENT SIDING 43% BRICK 47% STONE</p>
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STANDARD ROOF PITCH



01 FRONT ELEVATION (STANDARD)

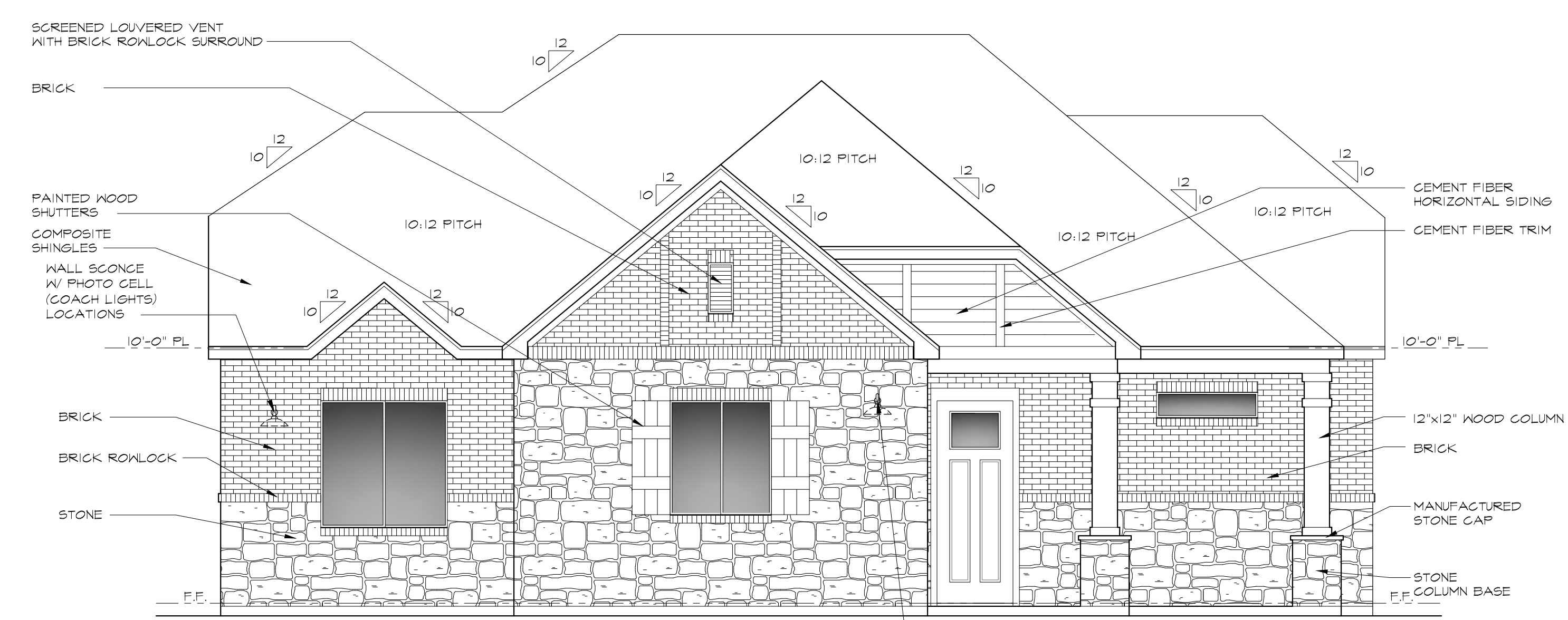
SCALE: 1/4"=1'-0"

BUILDING TYPE 3B ELEVATIONS

SCALE: 1/4" = 1'-0"

PD-59: EXHIBIT "E" -ELEVATIONS

<p>ARCHITECT IKEMIRE ARCHITECTS 16660 Dallas Parkway, Suite 2900 Dallas TX 75001 PH 972-248-2486 FX 972-248-1557 CONTACT: JAMIE HULSE</p>	<p>DEVELOPER NEXMETRO COMMUNITIES THREE ENERGY SQUARE 6688 N. CENTRAL EXPRESSWAY, SUITE 275 Addison TX 75206 PH 972-745-0039 CONTACT: JASON FLORY</p>	<p>NEXMETRO AVILLA FAIRWAYS BUILDING ELEVATIONS</p>
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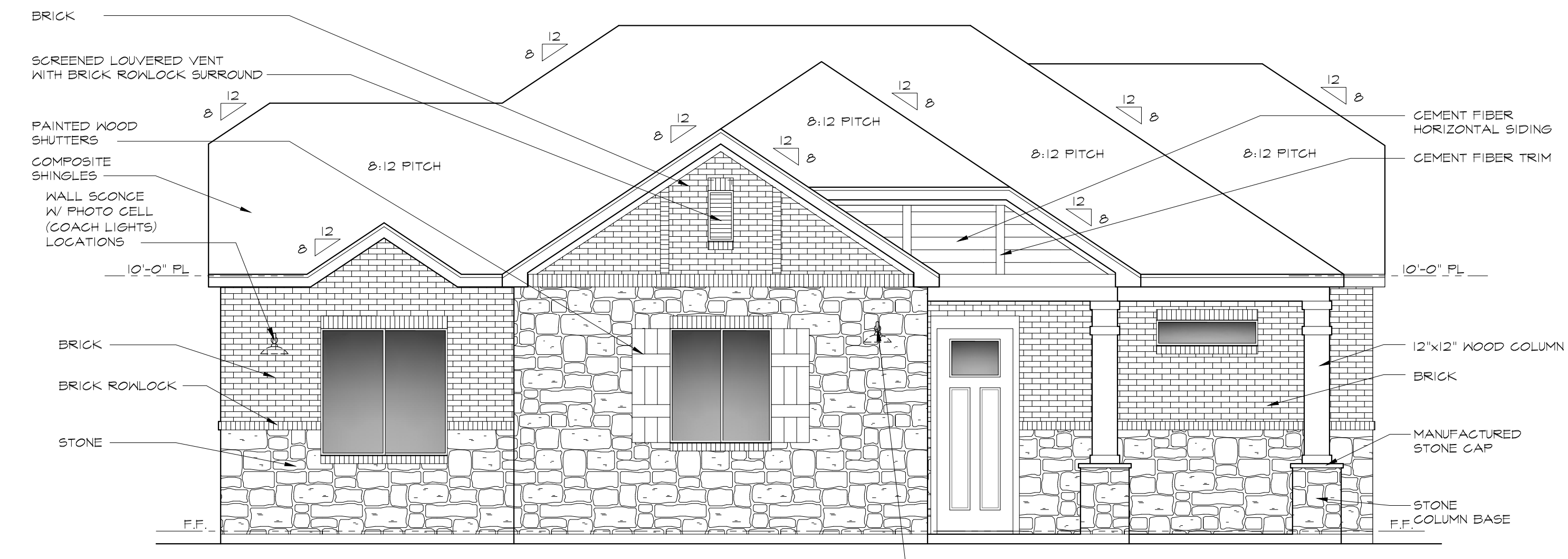


02 FRONT ELEVATION (ALTERNATE)

SCALE: 1/4"=1'-0"

<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% BRICK</p>	<p>LEFT ELEVATION</p> <p>7% FIBER CEMENT SIDING 44% BRICK 44% STONE</p>	<p>RIGHT ELEVATION</p> <p>7% FIBER CEMENT SIDING 44% BRICK 44% STONE</p>	<p>FRONT ELEVATION</p> <p>9% FIBER CEMENT SIDING 41% BRICK 50% STONE</p>
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ALTERNATE ROOF PITCH



01 FRONT ELEVATION (STANDARD)

SCALE: 1/4"=1'-0"

<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>5% FIBER CEMENT SIDING 52% BRICK 43% STONE</p>	<p>RIGHT ELEVATION</p> <p>5% FIBER CEMENT SIDING 52% BRICK 43% STONE</p>	<p>FRONT ELEVATION</p> <p>7% FIBER CEMENT SIDING 57% BRICK 36% STONE</p>
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STANDARD ROOF PITCH

BUILDING TYPE 3C ELEVATIONS

SCALE: 1/4" = 1'-0"

PD-59: EXHIBIT "E" -ELEVATIONS

<p>ARCHITECT IKEMIRE ARCHITECTS 16660 Dallas Parkway, Suite 2900 Dallas TX 75001 PH 972-248-2486 FX 972-248-1557 CONTACT: JAMIE HULSE</p>	<p>DEVELOPER NEXMETRO COMMUNITIES THREE ENERGY SQUARE 6688 N. CENTRAL EXPRESSWAY, SUITE 275 Addison TX 75206 PH 972-745-0039 CONTACT: JASON FLORY</p>	<p>NEXMETRO AVILLA FAIRWAYS BUILDING ELEVATIONS</p>
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EXHIBIT “F”
LAND USE REGULATIONS

SECTION 1: BASE DISTRICT

A. Purpose

The regulations set forth herein (**Exhibit “F”**) provide development standards for multi-family residential and ancillary uses within the Avilla Fairways Planned Development District No. 59 (“PD-57”). The boundaries of PD-59 are identified by metes and bounds on the Legal Description, **Exhibit “A”** and as depicted in **Exhibit “B”** to this Ordinance (“PD-59 or the “Property”), and the Property shall be developed in accordance with these regulations and the PD Concept Plan as conceptually depicted on **Exhibit “C”**, associated Ancillary Concept Plans as conceptually depicted in Exhibits **“D and E”** to this Ordinance and the UDC (defined below). A use that is not expressly authorized herein is expressly prohibited in this PD-59.

A. Base District

The “MF-1” Multi-Family Residential District regulations of the Corinth Unified Development Code, Ordinance No. 13-05-02-08, as amended, (“UDC”) shall apply to the Property except as modified herein. If a change to the PD Concept Plan, and/or associated Ancillary Concept Plans is requested, the request shall be processed in accordance with the UDC and development standards in effect at the time the change is requested for the proposed development per the Planned Development Amendment Process.

SECTION 2: USES AND AREA REGULATIONS

A. Purpose

PD-59 is intended to provide for a quality development of a multi-family residential cottage community taking advantage of the location and the concepts outlined in Envision Corinth 2040 Comprehensive Plan by promoting an alternative housing option and thereby adding to the overall mix of housing types within the community, the extension of Rye Road, and the provision of common open space activity nodes and parks and other recreational amenities including an interconnected internal trail system and Sidepath Trail along Lake Sharon Drive (See Exhibit “C” – PD Concept Plan and Exhibit “D” Conceptual Landscape Plan).

B. Permitted Uses and Use Regulations

In the PD-59 District, no building, or land shall be used, and no building shall be hereafter erected, reconstructed, enlarged, or converted unless otherwise provided for in the MF-1 Multi-Family Residential District regulations of the Unified Development Code or as otherwise permitted by this PD Ordinance. The Permitted Uses in the MF-1 Multi-Family District as listed in Subsection 2.07.03 of the Unified Development Code shall be permitted in the PD-59 District.

Additionally, to afford a focal point to the Avilla Fairways multifamily cottage community, a leasing center/club/pool, dog park, and open space activity nodes uses shall be permitted for the enjoyment of the community as presented in **Exhibits “C and D,”** to this PD-59 Ordinance in accordance with Site Plan review and approval. All building codes and other applicable regulations of the City shall apply to the leasing center/club/pool/dog park, and open spaces.

1. **Specific Uses.** UDC Section 2.07.05.A shall be modified to permit all proposed structures (leasing center, dwelling units, garages, and recreational amenities (pool, spa, dog park, grills, fire pits, pergolas, event lawns, etc. (as further detailed Exhibit “D” Conceptual Landscape Plan) to be permitted on one (1) lot. Further, Section 2.07.07, shall be modified where necessary to meet the intent of the layout of accessory buildings and uses with respect to location, size, and number of detached garages based on the concept presented in Exhibit “C” PD Concept Plan.

C. Dimensional Regulations/Site Data Table

The Dimensional Regulations described in Section 2.08.04 of the Unified Development Code, Ordinance No. 13-05-02-08, for the base zoning district MF-1 Multi-Family shall apply, except as modified below:

1. **UDC Section 2.08.04.** shall apply, except as modified from the base zoning district of MF-1 to allow for a **reduction in floor area** from a minimum floor area of 1,050 sf per dwelling unit to a minimum floor area of 680 sf per dwelling unit for the proposed one-bedroom units (attached cottage (duplex) buildings) and a minimum floor area of 1,022 sf per dwelling unit for the two-bedroom units as indicated on Exhibit “C” PD Concept Plan (*floor area values represent “slab size” floor area*). Further, all units throughout the complex shall have private backyard areas (*which is not reflected in the minimum floor area*) as depicted in Exhibit “C” PD Concept Plan. Table 1 – presents a zoning comparison from what is permitted within the base MF-1 zoning district and the proposed Avilla Fairways PD-59.

Table 2 presents the “Site Date Summary” for the overall project.

Table 1 – Dimensional Requirements (Base Zoning and Proposed PD requirements)

ZONING COMPARISON TABLE		
	BASE MF-1	PROP. PD
MINIMUM SETBACKS		
FRONT YARD	30'	25'
SIDE YARD		
INTERIOR LOT	30'	N/A
CORNER LOT	30'	N/A
REAR YARD	30'	20'
GARAGE	N/A	N/A
MINIMUM LOT DIMENSIONS		
LOT AREA (SF)	40,000	40,000
LOT WIDTH	200'	200'
LOT DEPTH	200'	200'
DENSITY (MAX, DU/A)	12	9
MINIMUM FLOOR AREA (SF PER DU)	1,050	680
MAX. BUILDING HT. (FT/STORIES)	35' / 2	30' / 1
MAX. BUILDING AREA (TOTAL)	40%	23%

Table 2 – Site Data Summary

SITE DATA SUMMARY TABLE			
EXISTING ZONING/USE:	PD-6, PD-24 / VACANT		
PROPOSED ZONING:	PD W/MF-1 BASE ZONING		
PROPOSED USE:	RESIDENTIAL COTTAGE COMMUNITY		
NUMBER OF LOTS:	2 (EXISTING); 2 (PROPOSED)		
LOT COVERAGE:	22.8%		
LOT AREA:	AC.	SQ. FT.	
GROSS AREA	24.595	1,071,370	
ROW DEDICATION	0.698	30,389	
NET AREA	23.898	1,040,981	
DENSITY:	9.00	UNITS / ACRE (w/o ROW Ded.)	
PROPOSED BUILDINGS (1 STORY):	AREA*	NO. BLDGS	NO. UNITS
1-BEDROOM (DUPLEX)	1,342	30	60
2-BEDROOM	1,022	87	87
3-BEDROOM	1,307	68	68
STANDARD GARAGE, DETACHED ⁺	831	20	-
STANDARD GARAGE W/HANDICAP, DETACHED	682	3	-
LEASING CENTER	1,131	1	-
TOTALS:		209	215
PARKING REQUIRED:	UNITS	REQUIRED	PROPOSED
1 BEDROOM (2 SPACES REQ'D; 2 SPACES PROP.)	60	120	120
2 BEDROOM (3 SPACES REQ'D; 2 SPACES PROP.)	87	261	174
3 BEDROOM (4 SPACES REQ'D; 2 SPACES PROP.)	68	272	136
TOTALS:	215	653	430
PARKING PROVIDED:	PROVIDED		
STANDARD SPACES	234		
CARPORT SPACES	144		
HANDICAP SPACES	3		
STANDARD GARAGE SPACES	87		
HANDICAP GARAGE SPACES	3		
TOTALS:	471		
GARAGE SPACES PER UNIT:	41.9%		
OPEN SPACE AREA REQUIRED:	83,278	8%	(SF, %)
OPEN SPACE AREA PROVIDED:			
EVENT LAWN	2,773	0.3%	
PASSIVE NODES	7,024	0.7%	
ACTIVITY NODES	16,802	1.6%	
DOG PARK	11,968	1.1%	
MISC. OPEN SPACE	142,338	13.7%	
TOTALS:	180,905	17.4%	(SF, %)
LANDSCAPE AREA:	500,032	48.0%	(SF, %)
TRAIL DEDICATION:	WIDTH, FT	LENGTH, FT	AREA, SF
LAKE SHARON DRIVE	10	2,189	21,890
OAKMONT DRIVE	6	1,017	6,102
TOTALS:	-	3,206	27,992
IMPERVIOUS AREA:	540,949	52.0%	(SF, %)
LOADING SPACE:	NOT REQUIRED		
OUTSIDE STORAGE:	NONE PROPOSED		
CONSTRUCTION DATES:	Q1 2021 - Q3 2021		

* THESE VALUES REPRESENT "SLAB SIZE" FLOOR AREA

+ ONE STANDARD GARAGE WILL USE TWO OF FOUR BAYS FOR ON-SITE MAINTENANCE PURPOSES

D. Development Standards

The Development Standards described in Section 2.04.04 of the Unified Development Code, Ordinance No. 13-05-02-08, for the MF-1 Multi-Family District shall apply to all development within PD-59, except as otherwise stated herein.

The following sections of the City of Corinth Unified Development Code (“UDC”), as modified below, shall serve as the development standards for PD-59:

Avilla Fairways Planned Development Requirements and Modified Standards

1. **UDC Section 2.09.03. Vehicular Parking Regulations** shall apply, except that the **space per unit** as applied to the apartments may be reduced to a minimum of **2.0 spaces per unit** on site as per Exhibit “C” PD Concept Plan and listed in Table 2 – Site Data Summary, herein.
2. **UDC Section 2.04.07.C.5.** requirement that “a minimum of seventy-five (75) percent of all apartments shall have a one-car enclosed garage, two hundred forty (240) square feet minimum, attached or detached, per dwelling unit”, shall **be modified as follows:** Garages and covered parking to be allowed per the Exhibit “C” – PD Concept Plan and will provide for an **overall minimum of two (2.0) parking spaces per unit**, including surface, covered, and garage parking spaces. Either a covered space or garage space will be assigned with every unit, ensuring a 100% covered parking per unit ratio across the site. These parking numbers are exclusive of any parking that may occur on Rye Road.
3. **UDC Section 2.09.06. Nonresidential Architectural Standards** shall **not** apply to the project site as the Golf Course property located adjacent to the northern property boundary of the subject site is not residentially zoned. PD-6 indicates that area as specific sub-area for Golf Course and does not require the proposed MF-1 base district to maintain a thirty (30’) foot buffer. However, a twenty-five (25’) foot front yard setback is being proposed along the major roadways Lake Sharon Drive and Oakmont Drive as well as a twenty (20’) foot building setback along all other boundaries.
4. **UDC Section 2.09.01 Residential Landscaping Requirements** shall apply and to be subject to the following modifications:
 - a. Landscape shrub plantings shall be used to soften the view of wood fencing around backyard areas when viewed from Lake Sharon Drive and/or Oakmont Drive.
 - b. Where wainscotting is proposed on exterior side walls only that are visible from a public street, this feature shall be either continuous and/or supplemented with continuous foundation plantings.
 - i. Lake Sharon Drive: refer to Exhibit “D” Conceptual Landscape Plan, sheet LS2 (detail 2, section B), and
 - ii. Oakmont Drive: refer to Exhibit “D” Conceptual Landscape Plan, sheet LS2 (detail 1, section A).
 - c. Opaque fencing for rear yards shall be allowed for privacy where rear yards are visible from Lake Sharon Rive or Oakmont Drive. All other perimeter fencing shall be ornamental metal with adaptive screening. Double fencing concepts are not desirable, and efforts will be made to reduce conditions that would require such. Additionally, significant interior landscaping shall be required to achieve a level of opacity that sufficiently affords a living screen and privacy. Specific treatments will be further evaluated and defined at time of Site Plan based on best design practices.
 - d. Streetscapes for Lake Sharon and Oakmont drive shall refer to Exhibit “D” Conceptual Landscape Plan as follows:
 - i. Lake Sharon Drive: refer to sheets LS2 (detail B) and LS3 (detail 5), and
 - ii. Oakmont Drive: refer to sheets LS2 (detail A) and LS3 (details 4,5, &7).

- e. Landscape edge buffers along Lake Sharon Drive and Oakmont Drive shall be planted per Avilla Fairways Conceptual Plan (refer to sheets LS1 and LS2) as shown on Exhibit “D” Conceptual Landscape Plan and be according to the following conditions:
 - i. Shade trees shall be planted at a rate of one (1) per 30 linear of feet of landscaped edge and include at least one (1) ornamental tree provided at a rate of one (1) per every two required shade trees.
 - ii. Trees may be clustered or located to accommodate driveway spacing, utilities, drainage facilities, trails, and similar site features, provided that a visual rhythm is maintained.
 - iii. Further, evergreen shrubs shall be included along the fencing and planted at varying intervals (which includes changes in height) to provide vistas into the development and buffered edges (e.g., variations of four feet minimum and 6-8 feet in height).
 - iv. A continuous evergreen hedgerow a minimum of 4 feet in height shall be provided (along drives, driveways, and perimeter parking areas) where necessary to reduce impact from vehicle headlights.

 - f. Along the north and west property line, a barrier will be established, and no disturbance shall occur within the drip line and/or critical root line of any tree located adjacent to the property line that extends into Avilla Fairways site. Any tree that dies along the adjacent property line within 2 years of site disturbance shall be replaced/fee-in-lieu-of applied at a rate of 3:1 caliper inch lost.
5. **UDC Section 2.04.09.C.8. Private Recreational Areas** shall apply, where a minimum of 8% of the gross complex is required to be in the form of private recreation. Note that the requirements of this section, are in addition to the park dedication requirements within 3.05.10. Park and Trail Dedications for Residentially Zoned Property. To meet the minimum 8% Private Recreation requirements, 2.29 acres or 9.6 % of the Net Acreage is designed in the form of Private Recreation areas which includes all activity nodes, large open space areas, pool/event lawn, and the dog park as shown and detail in Exhibits “C” and “D”, PD Concept Plan and Conceptual Landscape Plan, respectively.
6. **Park and Trail Land Dedication.** UDC Section 3.05.10 requires that **Park and Trail dedication for Residentially Zoned Property** to be provided at a rate of 1 acre per/50 DU and/or fees-in-lieu-of (or combination). Because the PD Concept Plan identifies the construction of a “Sidepath Trail” as shown on the Active Transportation Plan in the Envision Corinth 2040: Comprehensive Plan, the area required for the “Sidepath Trail” construction may be used to “satisfy” 0.5 acres of the overall required acreage and/or fees-in-lieu-of land dedication requirement as set forth by UDC Section 3.05.10 Park and Trail Dedications for Residentially Zoned Property for this project site provided the following conditions are met:
- a. The developer shall construct a ten (10’) foot concrete trail along Lake Sharon Drive in accordance with ADA standards.
 - b. A pedestrian public access easement shall be provided should the final design of the trail may meander outside of the public right-of-way and into the required 20’ landscaped buffer edge.
7. **Rye Road (extension).** Property Owner shall dedicate right-of-way and construct or cause the construction of the Rye Road (extension) from its current terminus at the northern Property boundary through the subject site to connect with Lake Sharon Drive. Rye Road construction will include five (5’) foot sidewalks on both the east and west sides of the street. All such construction shall be in accordance with this Ordinance and applicable City ordinances and regulations.

8. **UDC Sections 2.04.07. C.6 and 4.02.13 addressing Mechanical Equipment and Screening of Outdoor Waste Storage** shall apply, with the additional stipulation that dumpster enclosures will be masonry (or similarly acceptable material) and match the materials of the adjacent dwellings. Further, enclosures shall contain landscape foundation plantings to soften the view and enclosed on all four sides as depicted on the Exhibit “D” Conceptual Landscape Concept Plan. Additionally, mechanicals shall be screened with evergreen plant material to create an opaque boarder screen. Screening standards may be adjusted at time of site plan review based on best practices.
9. **UDC Section 2.09.02. Tree Preservation** regulations shall apply, except as provided for in the Endeavor Settlement Agreement (2017).
10. **UDC Section 2.09.04. Building Façade Material Standards** shall apply as defined below and compliance shall be determined by the City:
 - a. The exterior finishes of buildings shall be 80% masonry consisting of brick and/or stone and in no case shall any exterior wall contain less than seventy-five (75) percent of the brick and/or stone materials.
 - b. Exterior facades that face the northern property, along the golf course boundary shall consist of one hundred (100) percent brick and/or stone materials.
 - c. A minimum of 3 distinct elevations shall be provided per residential home floor plan with differing roof pitches.
 - d. **Roof Line.** Where visible along the exterior corridors (Lake Sharon Drive and Oakmont Drive), roof pitches shall have a minimum of 8/12 pitch with a mixture ranging from 8/12, 10/12 and 12/12 to provide visual interest. Specifically, no more than two (2) adjacent dwellings with the same roof pitch may be permitted along the corridors. Additionally, dormers, moldings, and other architectural features are required to provide character break up monotony of unit density.
Garage doors shall be designed with architectural elements such as dentil moldings, windows, raised panels, etc., while materials shall match dwellings. Garage doors are exempt from the brick and/or stone masonry requirement. Garage rooflines may have variation in roofline and pitch. This design detail will be determined at time of Site Plan and is subject to approval by the City.
11. **UDC Section 2.09.07. Lighting and Glare Regulations** shall apply, and as determined at time of Site Plan, pedestrian scale lighting fixtures shall be considered where practical, subject to the approval of the City.
12. **UDC Subsection 4.01 Sign Regulations** shall apply, however, if necessary, standards may be modified as shown on Exhibit “C” PD Concept Plan to achieve the “signage and monumentation concepts” provided that visibility and setbacks requirements are met to ensure safety. Further the portal signs will be located during the site plan/construction plan submission.
 - a. Portal signs to be located within landscape islands designating groupings of units as shown on Exhibit “D” Conceptual Landscape Plan, sheet LS3.
 - b. Three (3) total monument signs proposed for this development, one (1) sign at each location:
 - i. Lake Sharon Drive driveway connection
 - ii. Oakmont Drive driveway connection
 - iii. Rye Road and Lake Sharon intersection

13. **UDC Section 4.02. Fence and Screening Regulations** shall apply and include the installation of fencing and screening as further outlined below to meet intent of Exhibits “C” PD Concept Plan and “D”, Conceptual Landscape Plan.
- a. Lake Sharon Drive: Six (6’) foot ornamental fencing with masonry columns every 30 linear feet, along with interior landscaping shall be provided; refer to sheets LS1 & LS2 in Exhibit “D” Conceptual Landscape Plan.
 - b. Oakmont Drive: Six (6’) foot Board-on-Board Wood fence with exterior landscape adjacent to Oakmont Drive where dwelling unit backyard adjacent, otherwise six (6’) Ornamental fence per Exhibit “D” sheets LS1 & LS2 shall be provided.
 - c. Golf Course: Six (6’) foot ornamental fence per Exhibit “D” sheets LS1 & LS2 shall be provided.
 - d. Other Misc. Fence and Screening Standards:
 - i. Ornamental located along the Golf Course is not intended to have masonry columns.
 - ii. Board-on-Board fence shall have masonry columns every 30 linear feet.
 - iii. In instances where rear or side yard wood fencing is visible or fronts Lake Sharon Drive and Oakmont Drive corridors, such fencing shall be provided with a “cap” to ensure finished appearance along corridors.
Dumpster enclosures shall be screened with landscape foundation plantings and be of masonry material which shall match the material of adjacent dwelling units.
14. UDC Section 2.10.09.D. Planned Development (PD) Modifications and Amendments shall apply, however, d. (density is capped at 215 dwelling units), f. (minimum floor area may not be decreased from what is represented in Table 1 – Dimensional Requirements (Base Zoning and Proposed PD requirements) and Table 2 – Site Data Summary, and i. (building materials) will not be varied from the regulations outlined in this ordinance through a modification and/or amendment.
15. **Other.**
- a. Cottage community building separation minimum to be 8-feet (foundation to foundation). All residential units shall be fire sprinkled per NFPA-13D requirements.
 - b. Cottage community residential structures shall be 1 story maximum, not to exceed the maximum allowed height for residential buildings in the underlying zoning district.

<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>0% FIBER CEMENT SIDING 70% BRICK 30% STONE</p>	<p>LEFT ELEVATION</p> <p>0% FIBER CEMENT SIDING 70% BRICK 30% STONE</p>	<p>FRONT ELEVATION</p> <p>5% FIBER CEMENT SIDING 49% BRICK 46% STONE</p>
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ALTERNATE ROOF PITCH



02 FRONT ELEVATION (ALTERNATE)

<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>0% FIBER CEMENT SIDING 70% BRICK 30% STONE</p>	<p>LEFT ELEVATION</p> <p>0% FIBER CEMENT SIDING 70% BRICK 30% STONE</p>	<p>FRONT ELEVATION</p> <p>3% FIBER CEMENT SIDING 50% BRICK 47% STONE</p>
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STANDARD ROOF PITCH



01 FRONT ELEVATION (STANDARD)

SCALE: 1/4" = 1'-0"

BUILDING TYPE IA ELEVATIONS

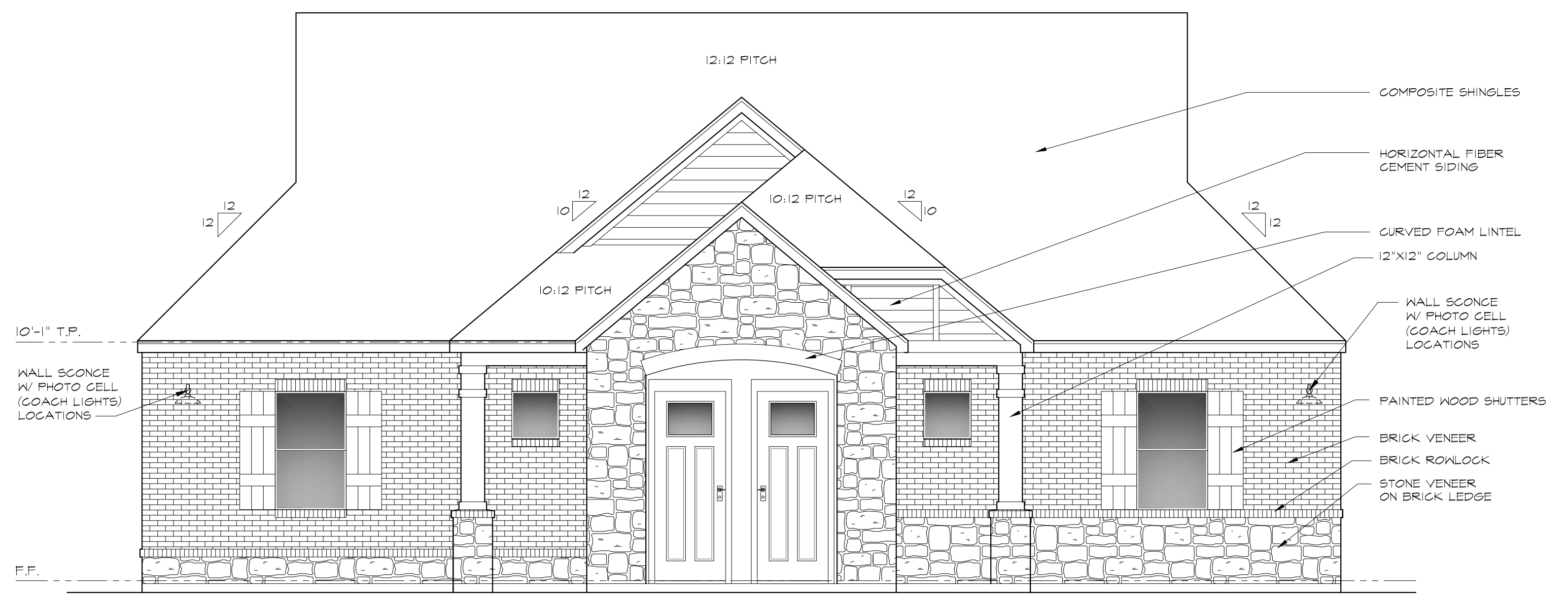
PD-59: EXHIBIT "E" -ELEVATIONS

SCALE: 1/4" = 1'-0"

<p>ARCHITECT IKEMIRE ARCHITECTS 16660 Dallas Parkway, Suite 2900 Dallas TX 75001 PH 972-248-2486 FX 972-248-1557 CONTACT: JAMIE HULSE</p>	<p>DEVELOPER NEXMETRO COMMUNITIES THREE ENERGY SQUARE 6688 N. CENTRAL EXPRESSWAY, SUITE 275 Addison TX 75206 PH 972-745-0039 CONTACT: JASON FLORY</p>	<p>NEXMETRO AVILLA FAIRWAYS BUILDING ELEVATIONS</p>
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<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>5% FIBER CEMENT SIDING 85% BRICK 10% STONE</p>	<p>RIGHT ELEVATION</p> <p>5% FIBER CEMENT SIDING 85% BRICK 10% STONE</p>	<p>FRONT ELEVATION</p> <p>10% FIBER CEMENT SIDING 60% BRICK 40% STONE</p>
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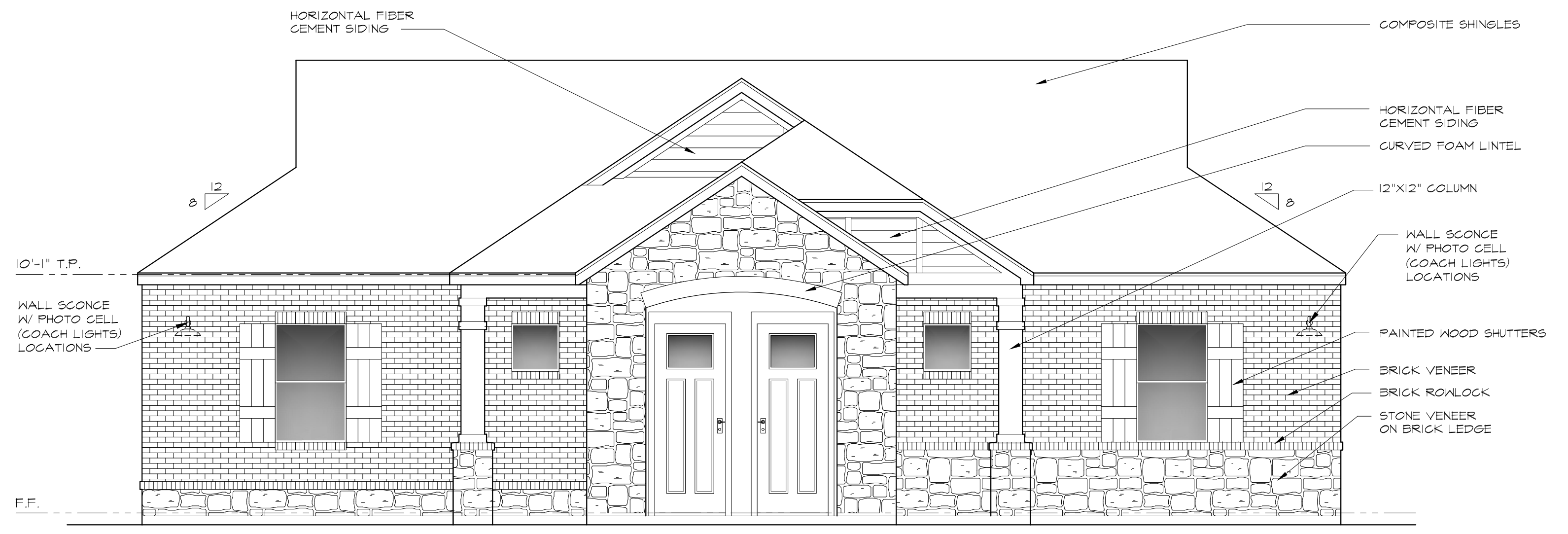
ALTERNATE ROOF PITCH



02 FRONT ELEVATION (ALTERNATE)
SCALE: 1/4" = 1'-0"

<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>3% FIBER CEMENT SIDING 86% BRICK 11% STONE</p>	<p>RIGHT ELEVATION</p> <p>3% FIBER CEMENT SIDING 86% BRICK 11% STONE</p>	<p>FRONT ELEVATION</p> <p>10% FIBER CEMENT SIDING 60% BRICK 40% STONE</p>
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STANDARD ROOF PITCH



01 FRONT ELEVATION (STANDARD)
SCALE: 1/4" = 1'-0"

BUILDING TYPE IB ELEVATIONS

SCALE: 1/4" = 1'-0"

PD-59: EXHIBIT "E" -ELEVATIONS

<p>ARCHITECT IKEMIRE ARCHITECTS 16660 Dallas Parkway, Suite 2900 Dallas TX 75001 PH 972-248-2486 FX 972-248-1557 CONTACT: JAMIE HULSE</p>	<p>DEVELOPER NEXMETRO COMMUNITIES THREE ENERGY SQUARE 6688 N. CENTRAL EXPRESSWAY, SUITE 275 Addison TX 75206 PH 972-745-0039 CONTACT: JASON FLORY</p>	<p>NEXMETRO AVILLA FAIRWAYS BUILDING ELEVATIONS</p>
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<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>21% FIBER CEMENT SIDING 76% BRICK 3% STONE</p>	<p>RIGHT ELEVATION</p> <p>21% FIBER CEMENT SIDING 76% BRICK 3% STONE</p>	<p>FRONT ELEVATION</p> <p>14% FIBER CEMENT SIDING 82% BRICK 4% STONE</p>
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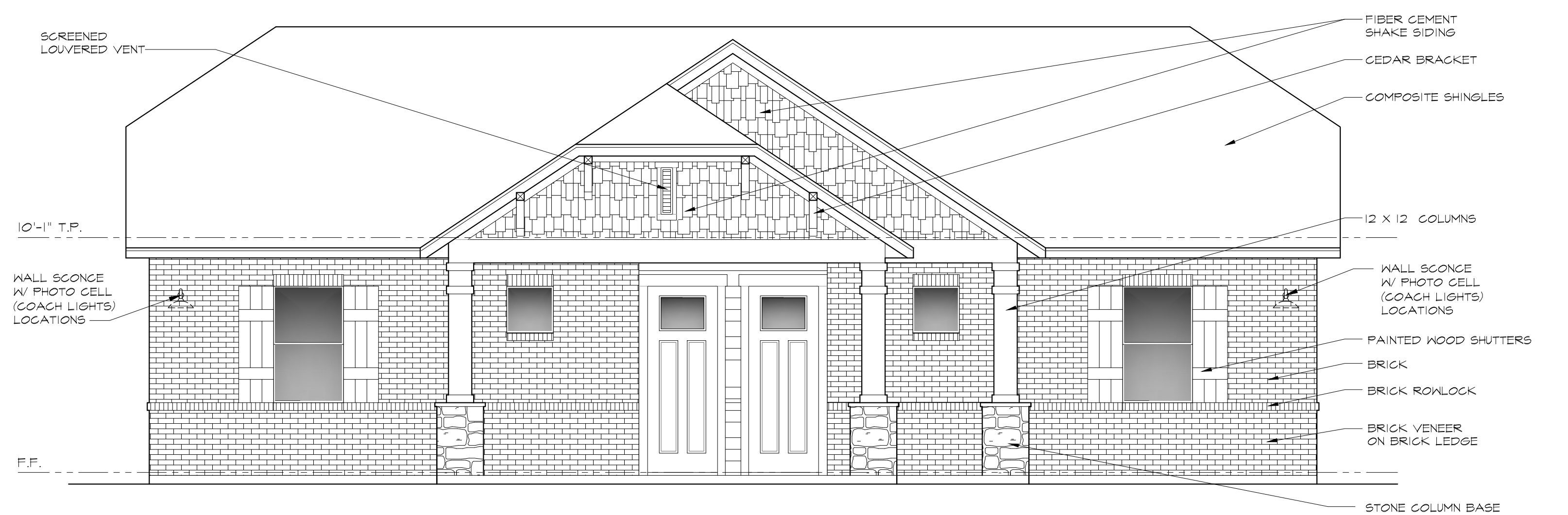
ALTERNATE ROOF PITCH



02 FRONT ELEVATION (ALTERNATE)
SCALE: 1/4"=1'-0"

<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>20% FIBER CEMENT SIDING 77% BRICK 3% STONE</p>	<p>RIGHT ELEVATION</p> <p>20% FIBER CEMENT SIDING 77% BRICK 3% STONE</p>	<p>FRONT ELEVATION</p> <p>12% FIBER CEMENT SIDING 83% BRICK 5% STONE</p>
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STANDARD ROOF PITCH



01 FRONT ELEVATION (STANDARD)
SCALE: 1/4"=1'-0"

BUILDING TYPE 1C ELEVATIONS

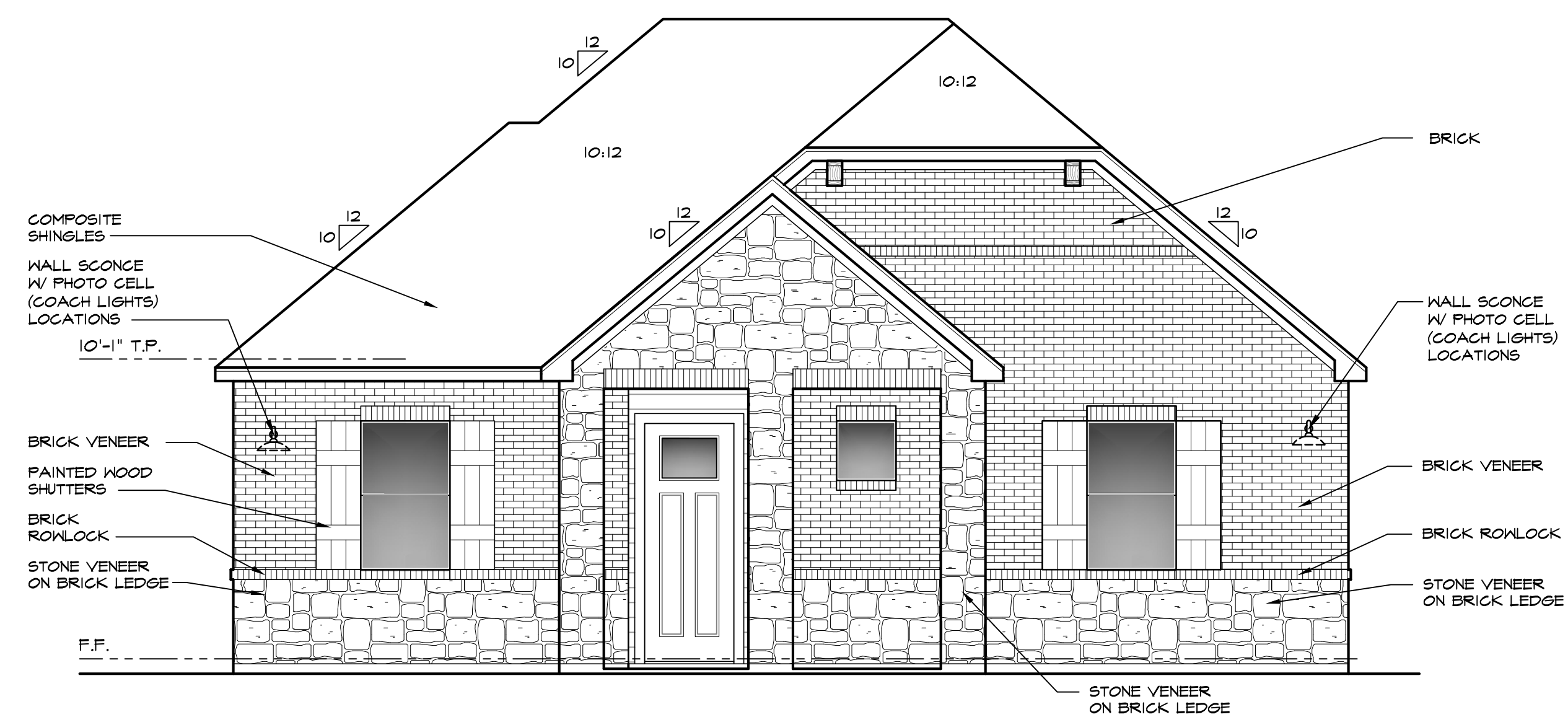
SCALE: 1/4" = 1'-0"

PD-59: EXHIBIT "E" -ELEVATIONS

<p>ARCHITECT IKEMIRE ARCHITECTS 16660 Dallas Parkway, Suite 2900 Dallas TX 75001 PH 972-248-2486 FX 972-248-1557 CONTACT: JAMIE HULSE</p>	<p>DEVELOPER NEXMETRO COMMUNITIES THREE ENERGY SQUARE 6688 N. CENTRAL EXPRESSWAY, SUITE 275 Addison TX 75206 PH 972-745-0039 CONTACT: JASON FLORY</p>	<p>NEXMETRO AVILLA FAIRWAYS BUILDING ELEVATIONS</p>
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<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK AND STONE 94% BRICK AND 6% STONE</p>	<p>RIGHT ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK AND STONE 95% BRICK AND 5% STONE</p>	<p>FRONT ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK AND STONE (58% BRICK, 42% STONE)</p>
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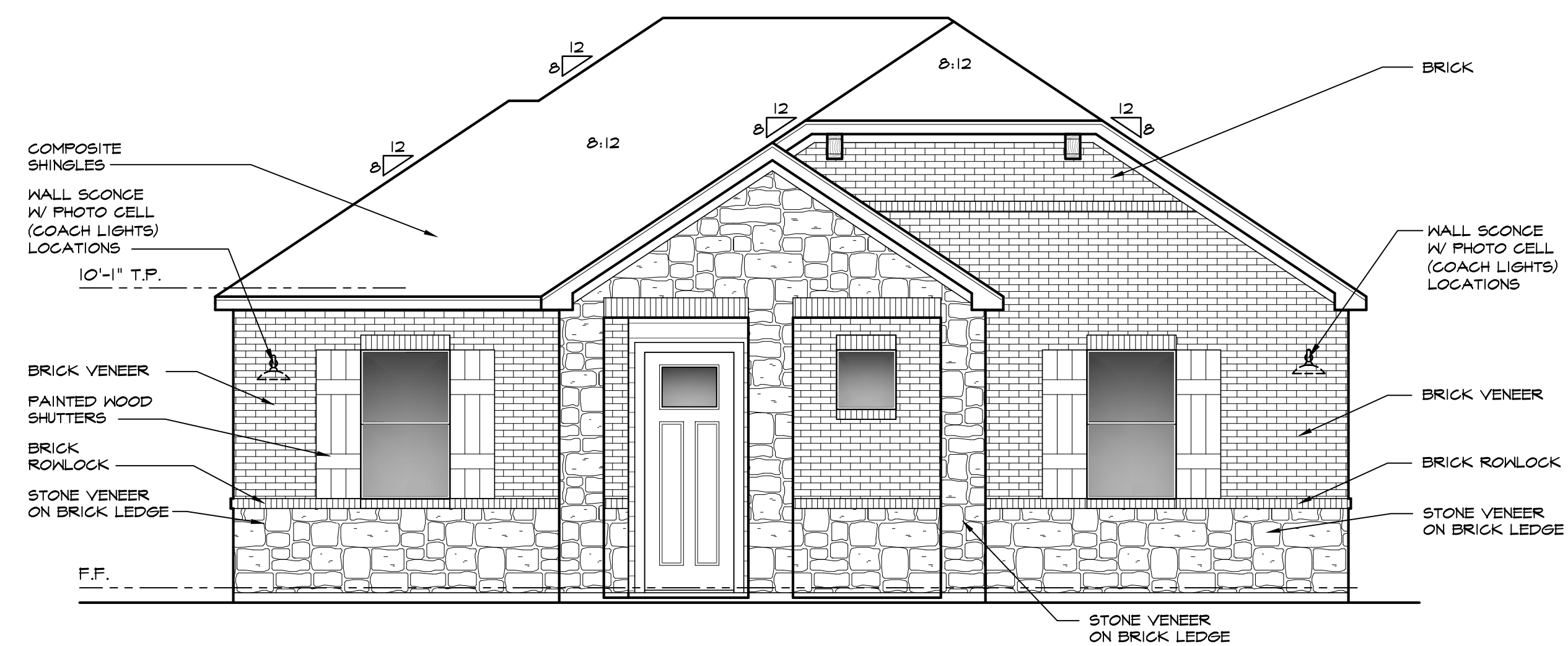
ALTERNATE ROOF PITCH



02 FRONT ELEVATION (ALTERNATE)

<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK AND STONE 94% BRICK AND 6% STONE</p>	<p>RIGHT ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK AND STONE 94% BRICK AND 6% STONE</p>	<p>FRONT ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK AND STONE (60% BRICK, 40% STONE)</p>
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STANDARD ROOF PITCH



01 FRONT ELEVATION (STANDARD)

BUILDING TYPE 2A ELEVATIONS

PD-59: EXHIBIT "E" -ELEVATIONS

SCALE: 1/4" = 1'-0"

<p>ARCHITECT IKEMIRE ARCHITECTS 16660 Dallas Parkway, Suite 2900 Dallas TX 75001 PH 972-248-2486 FX 972-248-1557 CONTACT: JAMIE HULSE</p>	<p>DEVELOPER NEXMETRO COMMUNITIES THREE ENERGY SQUARE 6688 N. CENTRAL EXPRESSWAY, SUITE 275 Addison TX 75206 PH 972-795-0039 CONTACT: JASON FLORY</p>	<p>NEXMETRO AVILLA FAIRWAYS BUILDING ELEVATIONS</p>
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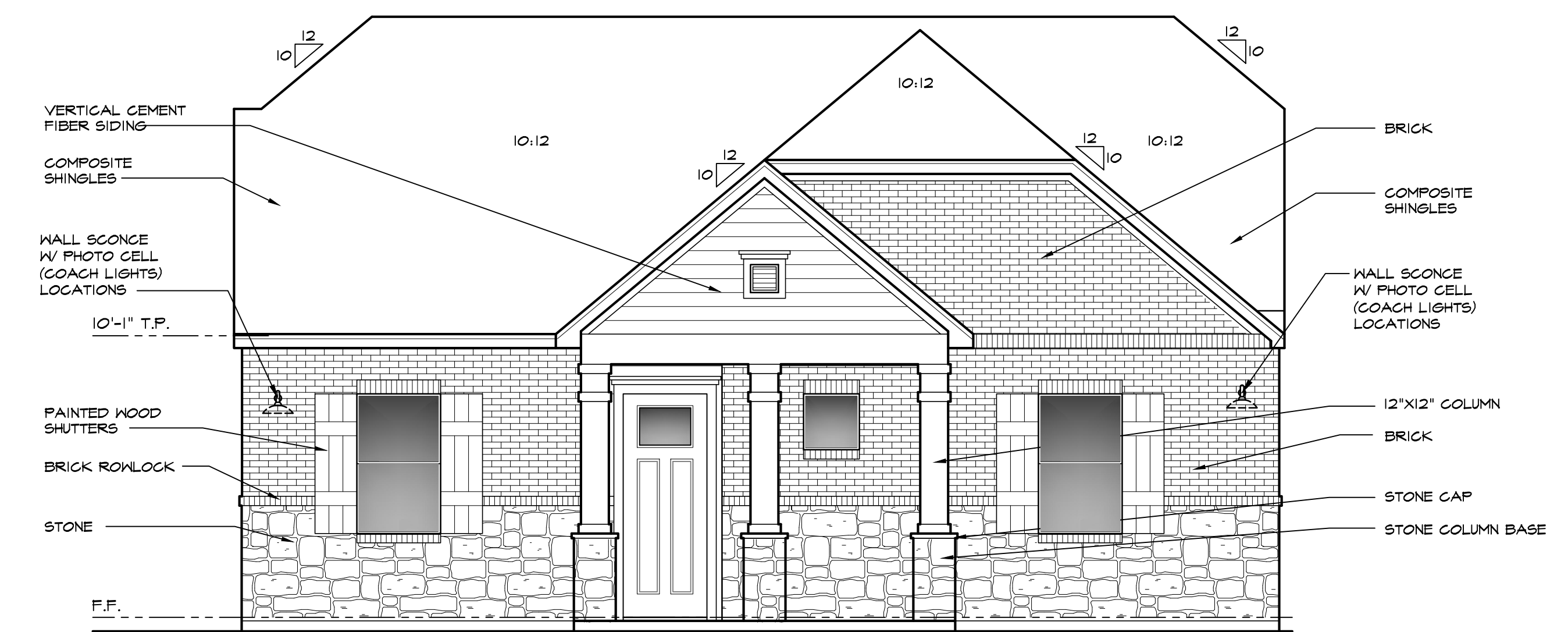
REAR ELEVATION
 0% FIBER CEMENT SIDING
 100% BRICK
 0% STONE

LEFT ELEVATION
 25% FIBER CEMENT SIDING
 75% BRICK AND STONE
 71% BRICK AND 4% STONE

RIGHT ELEVATION
 24% FIBER CEMENT SIDING
 76% BRICK AND STONE
 72% BRICK AND 4% STONE

FRONT ELEVATION
 8% FIBER CEMENT SIDING
 92% BRICK AND STONE
 (66% BRICK, 26% STONE)

ALTERNATE ROOF PITCH



02 FRONT ELEVATION (ALTERNATE)
 SCALE: 1/4"=1'-0"

FRONT PORCH 78 S.F.

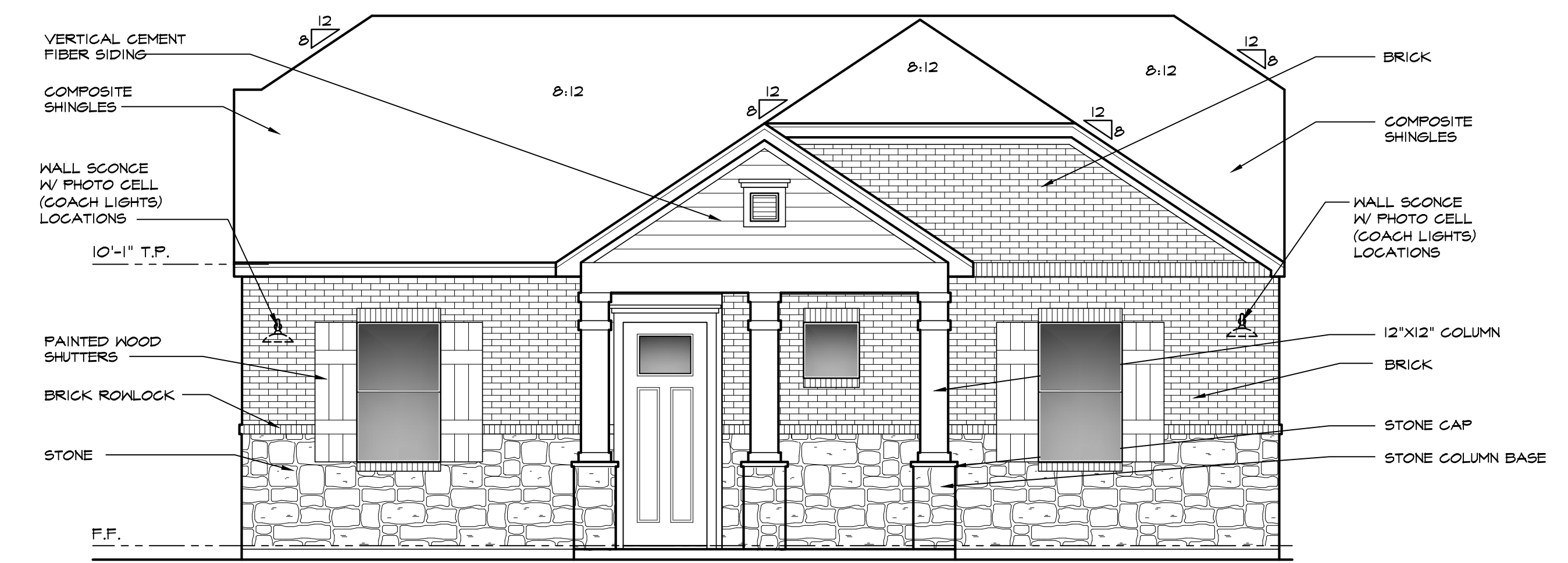
REAR ELEVATION
 0% FIBER CEMENT SIDING
 100% BRICK
 0% STONE

LEFT ELEVATION
 25% FIBER CEMENT SIDING
 75% BRICK AND STONE
 71% BRICK AND 4% STONE

RIGHT ELEVATION
 23% FIBER CEMENT SIDING
 77% BRICK AND STONE
 73% BRICK AND 4% STONE

FRONT ELEVATION
 6% FIBER CEMENT SIDING
 94% BRICK AND STONE
 (61% BRICK, 33% STONE)

STANDARD ROOF PITCH



01 FRONT ELEVATION (STANDARD)
 SCALE: 1/4"=1'-0"

FRONT PORCH 78 S.F.

BUILDING TYPE 2B ELEVATIONS

PD-59: EXHIBIT "E" -ELEVATIONS

SCALE: 1/4" = 1'-0"

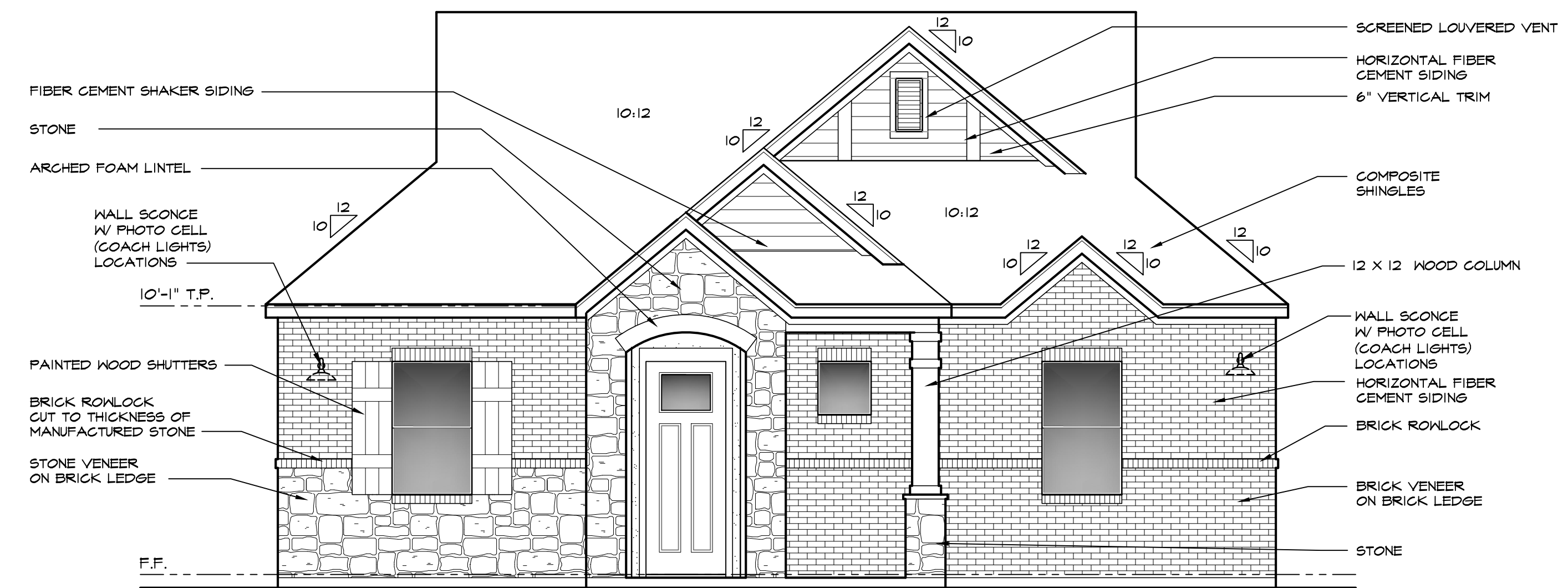
ARCHITECT
 IKEMIRE ARCHITECTS
 16660 Dallas Parkway, Suite 2900
 Dallas TX 75001
 PH 972-248-2486 FX 972-248-1557
 CONTACT: JAMIE HULSE

DEVELOPER
 NEXMETRO COMMUNITIES
 THREE ENERGY SQUARE
 6688 N. CENTRAL EXPRESSWAY, SUITE 275
 Addison TX 75206
 PH 972-795-0039
 CONTACT: JASON FLORY

NEXMETRO
 AVILLA FAIRWAYS
 BUILDING ELEVATIONS

<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>9% FIBER CEMENT SIDING 91% BRICK AND STONE 83% BRICK AND 8% STONE</p>	<p>RIGHT ELEVATION</p> <p>10% FIBER CEMENT SIDING 90% BRICK AND STONE 88% BRICK AND 2% STONE</p>	<p>FRONT ELEVATION</p> <p>5% FIBER CEMENT SIDING 95% BRICK AND STONE (84% BRICK, 11% STONE)</p>
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ALTERNATE ROOF PITCH



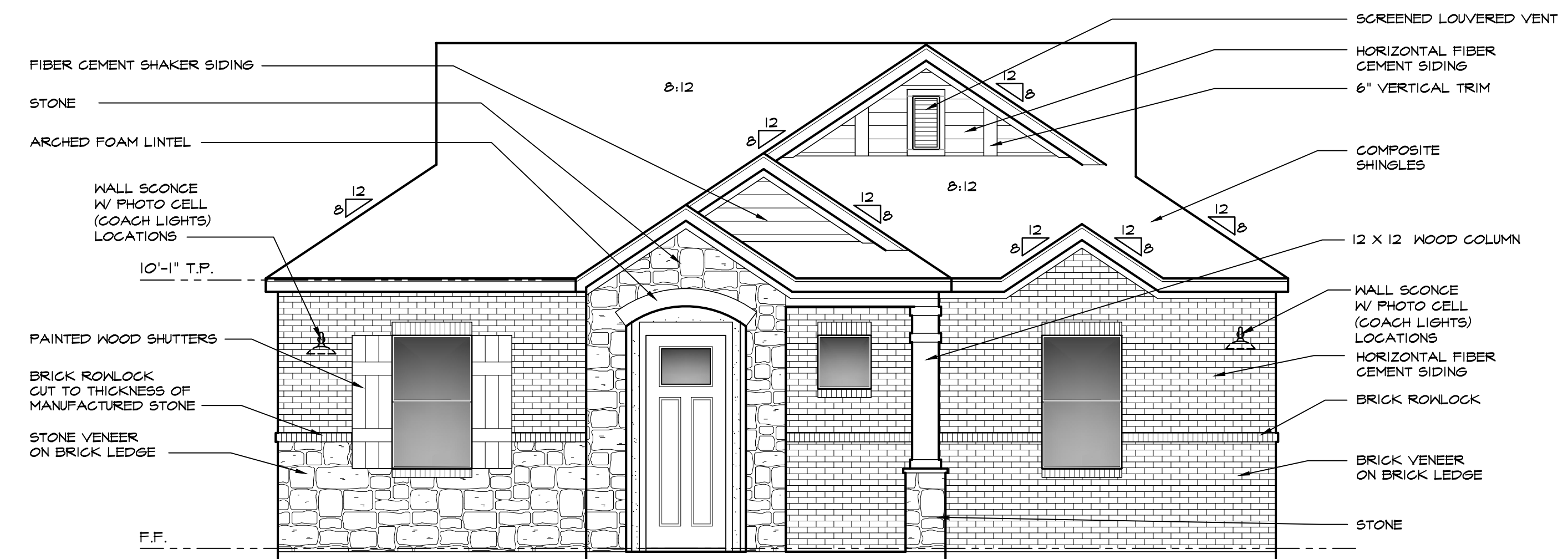
02 FRONT ELEVATION (ALTERNATE)

FRONT PORCH 74 S.F.

SCALE: 1/4"=1'-0"

<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>9% FIBER CEMENT SIDING 91% BRICK AND STONE 83% BRICK AND 8% STONE</p>	<p>RIGHT ELEVATION</p> <p>9% FIBER CEMENT SIDING 91% BRICK AND STONE 89% BRICK AND 2% STONE</p>	<p>FRONT ELEVATION</p> <p>4% FIBER CEMENT SIDING 96% BRICK AND STONE (65% BRICK, 35% STONE)</p>
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STANDARD ROOF PITCH



01 FRONT ELEVATION (STANDARD)

FRONT PORCH 74 S.F.

SCALE: 1/4"=1'-0"

BUILDING TYPE 2C ELEVATIONS

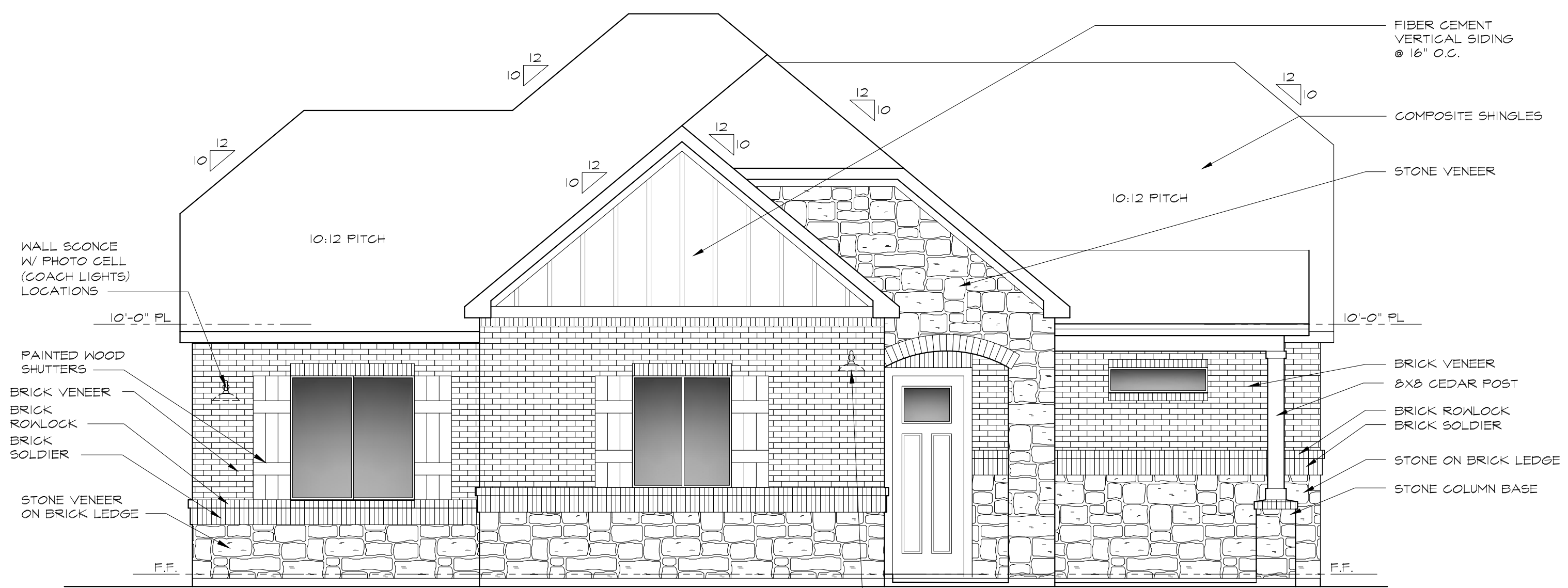
SCALE: 1/4" = 1'-0"

PD-59: EXHIBIT "E" -ELEVATIONS

<p>ARCHITECT IKEMIRE ARCHITECTS 16660 Dallas Parkway, Suite 2900 Dallas TX 75001 PH 972-248-2486 FX 972-248-1557 CONTACT: JAMIE HULSE</p>	<p>DEVELOPER NEXMETRO COMMUNITIES THREE ENERGY SQUARE 6688 N. CENTRAL EXPRESSWAY, SUITE 275 Addison TX 75206 PH 972-795-0039 CONTACT: JASON FLORY</p>	<p>NEXMETRO AVILLA FAIRWAYS BUILDING ELEVATIONS</p>
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<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>11% FIBER CEMENT SIDING 49% BRICK 40% STONE</p>	<p>RIGHT ELEVATION</p> <p>15% FIBER CEMENT SIDING 45% BRICK 40% STONE</p>	<p>FRONT ELEVATION</p> <p>12% FIBER CEMENT SIDING 40% BRICK 48% STONE</p>
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ALTERNATE ROOF PITCH

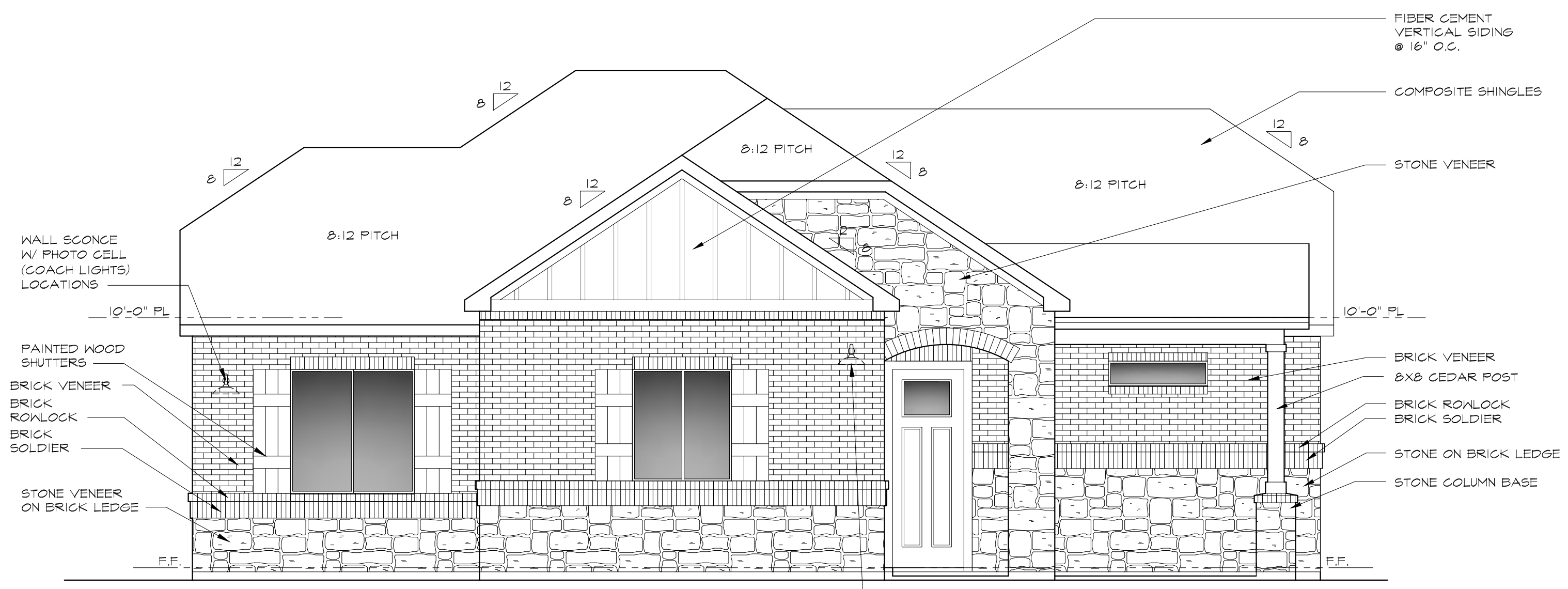


02 FRONT ELEVATION (ALTERNATE)

SCALE: 1/4"=1'-0"

<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>10% FIBER CEMENT SIDING 47% BRICK 43% STONE</p>	<p>RIGHT ELEVATION</p> <p>20% FIBER CEMENT SIDING 45% BRICK 35% STONE</p>	<p>FRONT ELEVATION</p> <p>09% FIBER CEMENT SIDING 47% BRICK 44% STONE</p>
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STANDARD ROOF PITCH



01 FRONT ELEVATION (STANDARD)

SCALE: 1/4"=1'-0"

BUILDING TYPE 3A ELEVATIONS

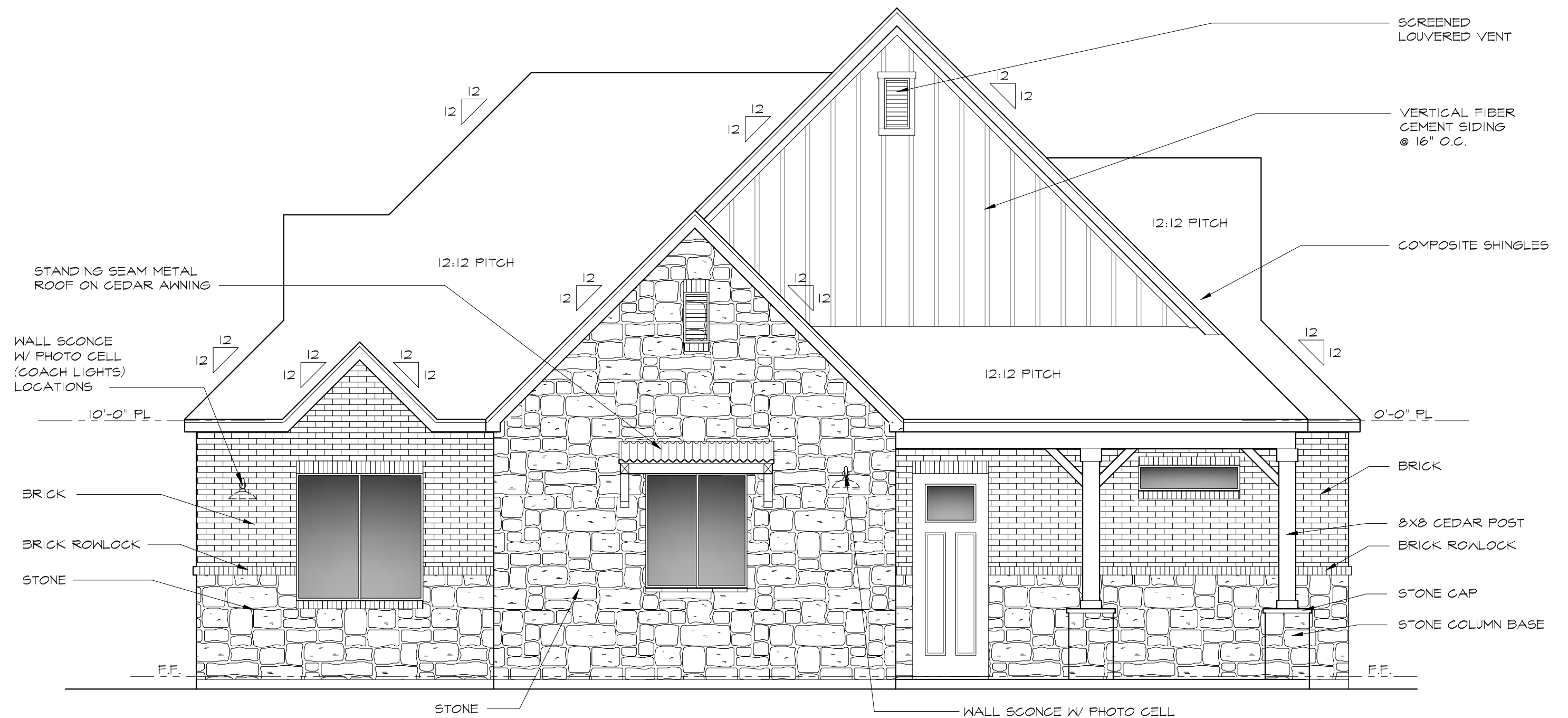
PD-59: EXHIBIT "E" -ELEVATIONS

SCALE: 1/4" = 1'-0"

<p>ARCHITECT IKEMIRE ARCHITECTS 16660 Dallas Parkway, Suite 2900 Dallas TX 75001 PH 972-248-2486 FX 972-248-1557 CONTACT: JAMIE HULSE</p>	<p>DEVELOPER NEXMETRO COMMUNITIES THREE ENERGY SQUARE 6688 N. CENTRAL EXPRESSWAY, SUITE 275 Addison TX 75206 PH 972-745-0039 CONTACT: JASON FLORY</p>	<p>NEXMETRO AVILLA FAIRWAYS BUILDING ELEVATIONS</p>
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<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>7% FIBER CEMENT SIDING 42% BRICK 45% STONE</p>	<p>RIGHT ELEVATION</p> <p>7% FIBER CEMENT SIDING 42% BRICK 45% STONE</p>	<p>FRONT ELEVATION</p> <p>11% FIBER CEMENT SIDING 49% BRICK 40% STONE</p>
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ALTERNATE ROOF PITCH

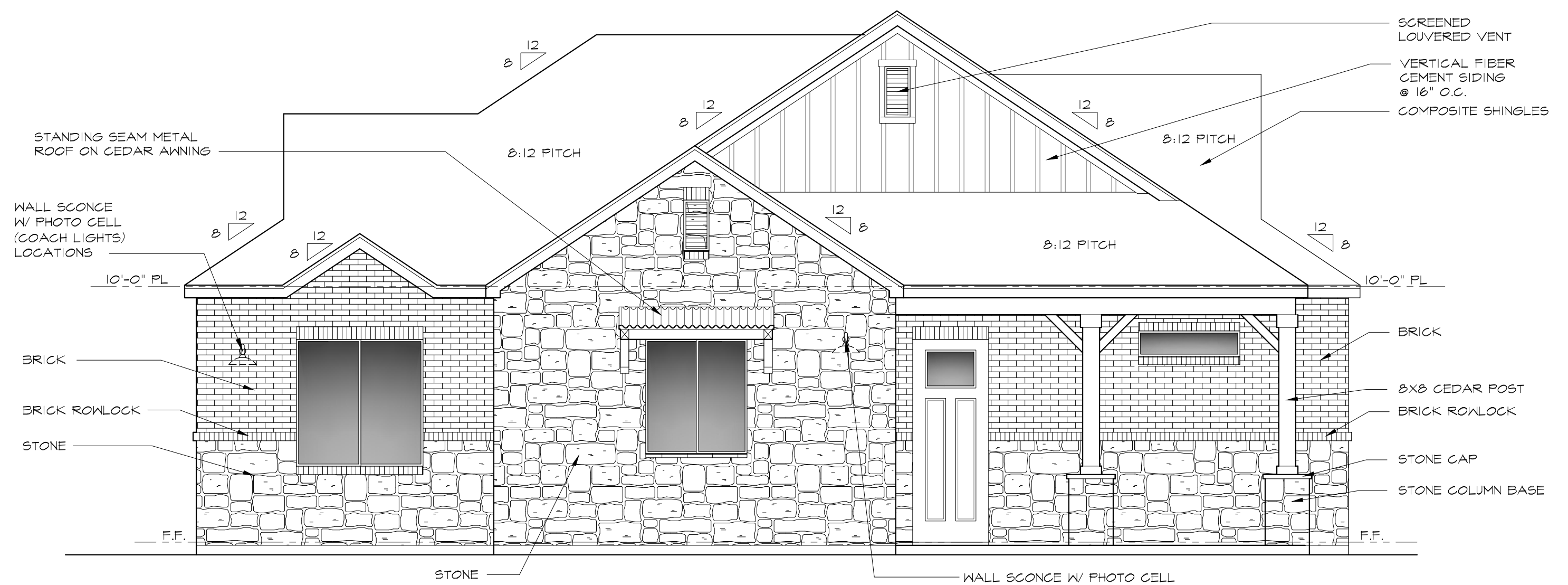


02 FRONT ELEVATION (ALTERNATE)

SCALE: 1/4"=1'-0"

<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>5% FIBER CEMENT SIDING 55% BRICK 40% STONE</p>	<p>RIGHT ELEVATION</p> <p>5% FIBER CEMENT SIDING 50% BRICK 45% STONE</p>	<p>FRONT ELEVATION</p> <p>10% FIBER CEMENT SIDING 43% BRICK 47% STONE</p>
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STANDARD ROOF PITCH



01 FRONT ELEVATION (STANDARD)

SCALE: 1/4"=1'-0"

BUILDING TYPE 3B ELEVATIONS

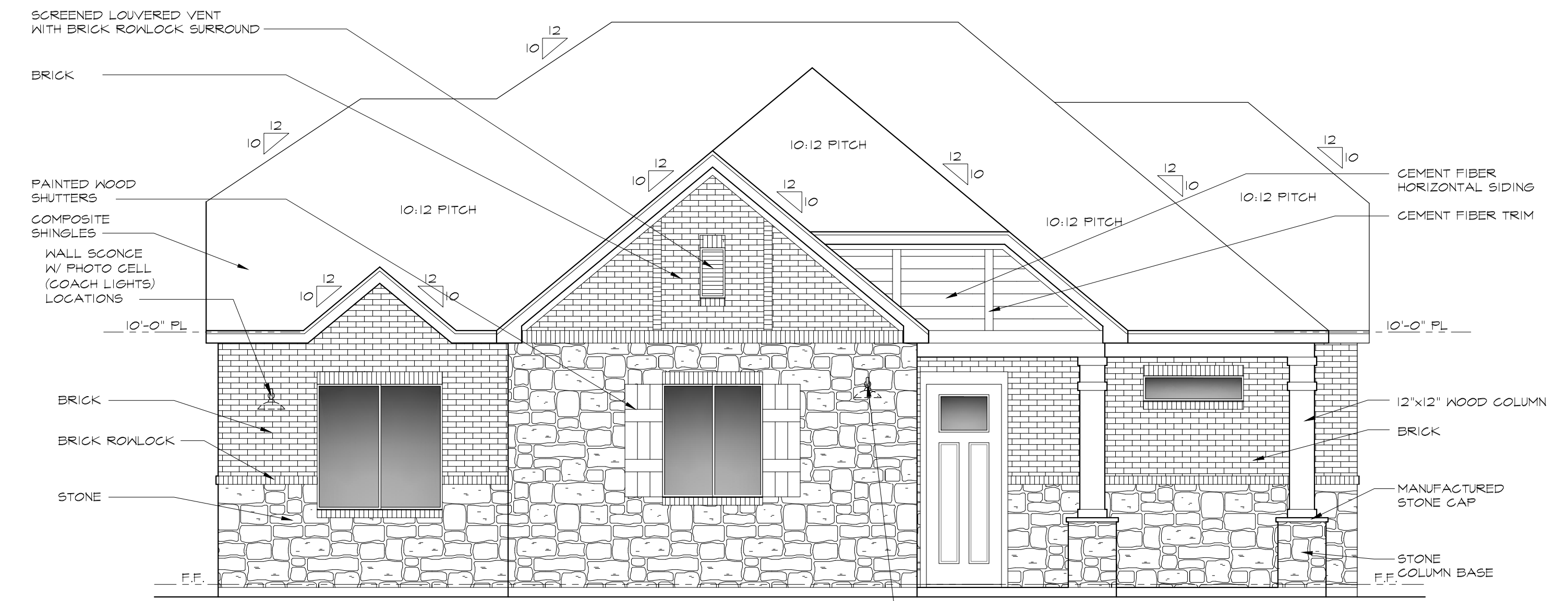
SCALE: 1/4" = 1'-0"

PD-59: EXHIBIT "E" -ELEVATIONS

<p>ARCHITECT IKEMIRE ARCHITECTS 16660 Dallas Parkway, Suite 2900 Dallas TX 75001 PH 972-248-2486 FX 972-248-1557 CONTACT: JAMIE HULSE</p>	<p>DEVELOPER NEXMETRO COMMUNITIES THREE ENERGY SQUARE 6688 N. CENTRAL EXPRESSWAY, SUITE 275 Addison TX 75206 PH 972-745-0039 CONTACT: JASON FLORY</p>	<p>NEXMETRO AVILLA FAIRWAYS BUILDING ELEVATIONS</p>
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<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% BRICK</p>	<p>LEFT ELEVATION</p> <p>7% FIBER CEMENT SIDING 44% BRICK 44% STONE</p>	<p>RIGHT ELEVATION</p> <p>7% FIBER CEMENT SIDING 44% BRICK 44% STONE</p>	<p>FRONT ELEVATION</p> <p>9% FIBER CEMENT SIDING 41% BRICK 50% STONE</p>
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ALTERNATE ROOF PITCH

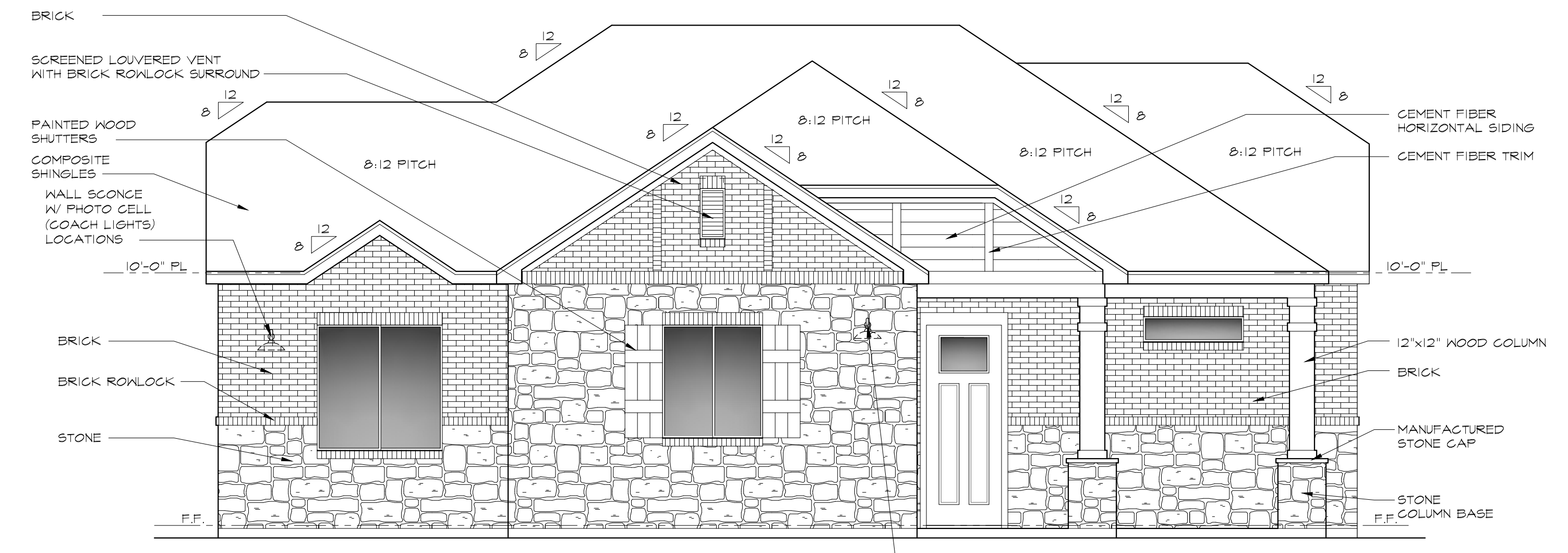


02 FRONT ELEVATION (ALTERNATE)

SCALE: 1/4"=1'-0"

<p>REAR ELEVATION</p> <p>0% FIBER CEMENT SIDING 100% BRICK 0% STONE</p>	<p>LEFT ELEVATION</p> <p>5% FIBER CEMENT SIDING 52% BRICK 43% STONE</p>	<p>RIGHT ELEVATION</p> <p>5% FIBER CEMENT SIDING 52% BRICK 43% STONE</p>	<p>FRONT ELEVATION</p> <p>7% FIBER CEMENT SIDING 57% BRICK 36% STONE</p>
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STANDARD ROOF PITCH



01 FRONT ELEVATION (STANDARD)

SCALE: 1/4"=1'-0"

BUILDING TYPE 3C ELEVATIONS

SCALE: 1/4" = 1'-0"

PD-59: EXHIBIT "E" -ELEVATIONS

<p>ARCHITECT IKEMIRE ARCHITECTS 16660 Dallas Parkway, Suite 2900 Dallas TX 75001 PH 972-248-2486 FX 972-248-1557 CONTACT: JAMIE HULSE</p>	<p>DEVELOPER NEXMETRO COMMUNITIES THREE ENERGY SQUARE 6688 N. CENTRAL EXPRESSWAY, SUITE 275 Addison TX 75206 PH 972-745-0039 CONTACT: JASON FLORY</p>	<p>NEXMETRO AVILLA FAIRWAYS BUILDING ELEVATIONS</p>
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