



**City Council Workshop Agenda**  
**Monday, April 06, 2026, 4:30 PM**  
**Council Chambers, 616 NE 4th AVE**

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*NOTE: The City welcomes public meeting citizen participation. TTY Relay Service: 711. In compliance with the ADA, if you need special assistance to participate in a meeting, contact the City Clerk's office at (360) 834-6864, 72 hours prior to the meeting so reasonable accommodations can be made (28 CFR 35.102-35.104 ADA Title 1)*

**To observe the meeting** (no public comment ability)

- go to <https://vimeo.com/event/5843423>

**To participate in the meeting** (able to public comment)

- go to <https://cityofcamas-us.zoom.us/j/84310812974>

(public comments may be submitted to [publiccomments@cityofcamas.us](mailto:publiccomments@cityofcamas.us))

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**CALL TO ORDER**

**ROLL CALL**

**PUBLIC COMMENTS**

**WORKSHOP TOPICS**

1. [ADA Transition Plan Update Presentation](#)  
[Presenter: James Carothers, Engineering Manager](#)  
[Time Estimate: 30 minutes](#)
2. [ADA Transition Plan Update Professional Services Agreement Amendment 2](#)  
[Presenter: James Carothers, Engineering Manager](#)  
[Time Estimate: 5 minutes](#)
3. [NW 28<sup>th</sup> Avenue and NW Fargo Street Pavement Preservation and Complete Street Reconfiguration](#)  
Presenter: James Carothers, Engineering Manager  
Time Estimate: 10 minutes
4. [NW Oregon Street Vacation Petition](#)  
[Presenter: James Carothers, Engineering Manager](#)  
[Time Estimate: 5 minutes](#)
5. [Waterline Easement Vacation Request](#)  
[Presenter: James Carothers, Engineering Manager](#)  
[Time Estimate: 5 minutes](#)
6. Staff Miscellaneous Updates  
Presenter: Doug Quinn, City Administrator  
Time Estimate: 10 minutes

**PUBLIC COMMENTS**

**COUNCIL COMMENTS AND REPORTS**

**CLOSE OF MEETING**



# Staff Report

April 6, 2026 Council Workshop Meeting

ADA Transition Plan Update Presentation  
Presenter: James Carothers, Engineering Manager  
Time Estimate: 30 minutes

Phone	Email
360.817.7230	jcarothers@cityofcamas.us

### BACKGROUND:

The City’s existing ADA Transition Plan was originally completed in 2015 and requires periodic updates to maintain compliance with the Americans with Disabilities Act (ADA) and remain competitive for federal and state grant funding.

In March 2025, Council approved a Professional Services Agreement with Apex Companies, LLC to update the City’s ADA Transition Plan. This update is focused on transportation-related facilities within the public right-of-way and includes an inventory and evaluation of curb ramps throughout the city, along with prioritization of improvements, cost estimates, and implementation strategies.

As part of the update process, staff and the Citizens Advisory Committee identified the downtown area as a high-priority location for ADA improvements due to its high pedestrian activity and importance as a central community destination.

Based on this input, additional analysis was required to further refine downtown curb ramp needs, including detailed inventory, prioritization, mapping, and cost estimating.

### SUMMARY: The main purposes of the plan are to:

- Identify physical obstacles in the City’s facilities that limit the accessibility of its programs or activities to individuals with disabilities.
- Describe how the facilities will be made accessible.
- Provide a timeframe and schedule for completing facility upgrades.
- Establish an attainable annual expenditure rate.

The draft ADA Transition Plan update provides a prioritized list of curb ramp improvements throughout the city, including both near-term, low-cost improvements and longer-term capital upgrades. The plan includes planning-level cost estimates and is intended to guide implementation through the City’s Transportation Improvement Program and future capital planning efforts.

The update incorporates current field data, engineering evaluation, and community input to establish a phased and achievable approach to improving accessibility within the public right-of-way.

Staff welcomes comments on the contents and recommends approval of the plan.

**BENEFITS TO THE COMMUNITY:** ADA improvements allow all community members to use pedestrian access routes within the public rights-of-way. This plan is in accordance with 4.4.2 Walking, Biking, and ADA Mobility Goals and Policies section of the Camas Comprehensive Plan.

**STRATEGIC PLAN:** By planning for the removal of mobility barriers within the public rights-of-way, under the *Safe and Accessible Community* priority, *Improve transportation and recreation safety and accessibility for all ages and abilities*.

**POTENTIAL CHALLENGES:** Funding for projects within this plan must be budgeted on an annual basis.

**BUDGET IMPACT:** An annual budget of \$75,000 has been allotted from the general fund for the past several years. Staff is recommending increasing the annual allotment to \$100,000 in future budget years in order to complete the listed improvements in a timely manner.

**RECOMMENDATION:** Staff recommends this item be placed on the April 20, 2026 Council Regular Meeting Consent Agenda for Council's consideration.

# Americans with Disabilities Act (ADA) Plan Update 2025-2026

## City Council Workshop

April 6, 2026



# Agenda



Introductions



Background



2015 ADA  
Transition Plan:  
Priority Locations



2025 Self-  
Evaluation Plan:  
What We Learned



2025 ADA  
Transition Plan:  
What We Recommend



Next Steps

# INTRODUCTIONS

# Background

- Includes two pieces:
  - Self-Evaluation Plan
  - ADA Transition Plan (ADA Plan Update)
- Increases accessibility and mobility for all in the public right-of-way (streets)
- Improves curb ramps, traffic-signal push buttons, sidewalks, policies, and practices but not a pedestrian plan
- Updates Camas's 2015 ADA Plan as required by Washington State Department of Transportation (WSDOT)

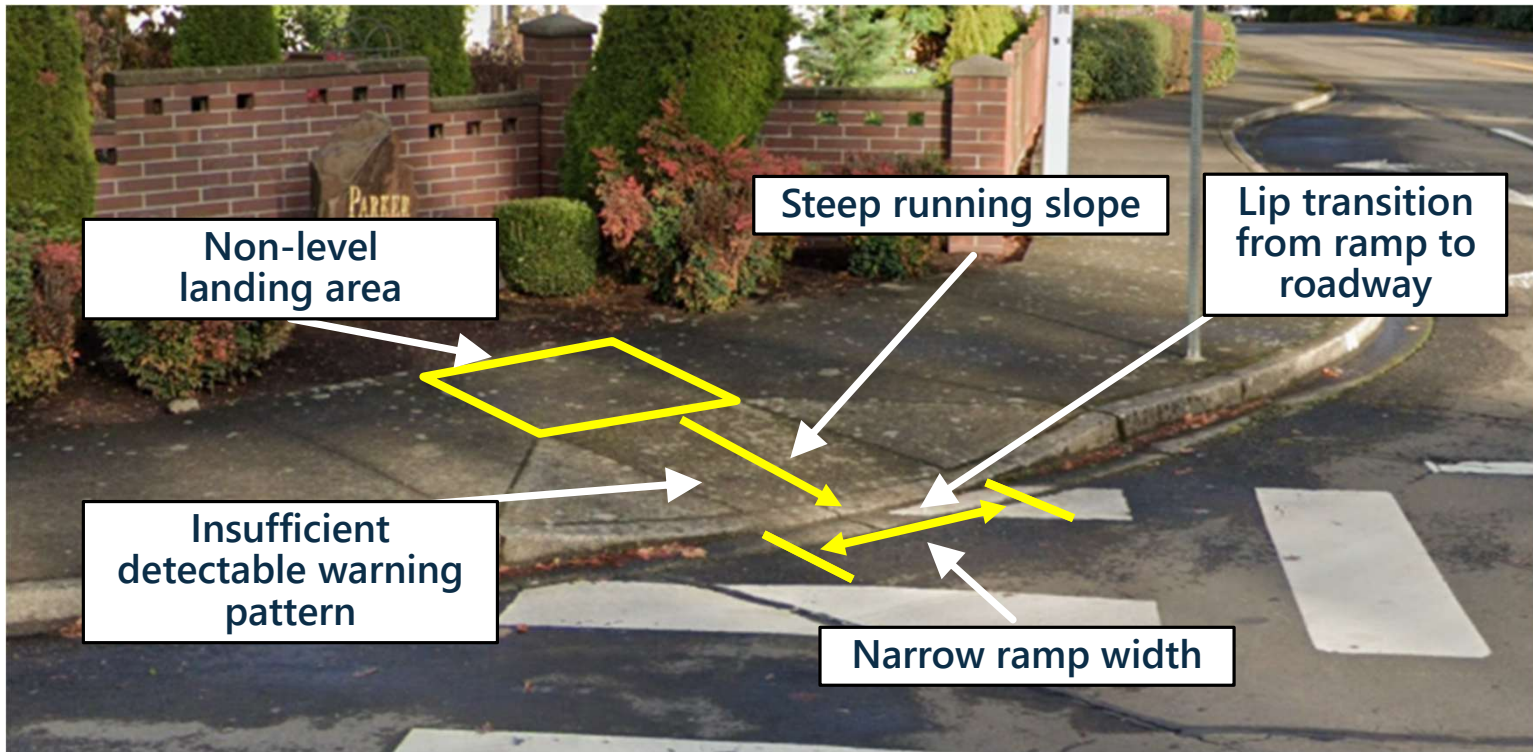
# 2015 High-Priority Areas (1)



# 2025 Self-Evaluation Plan

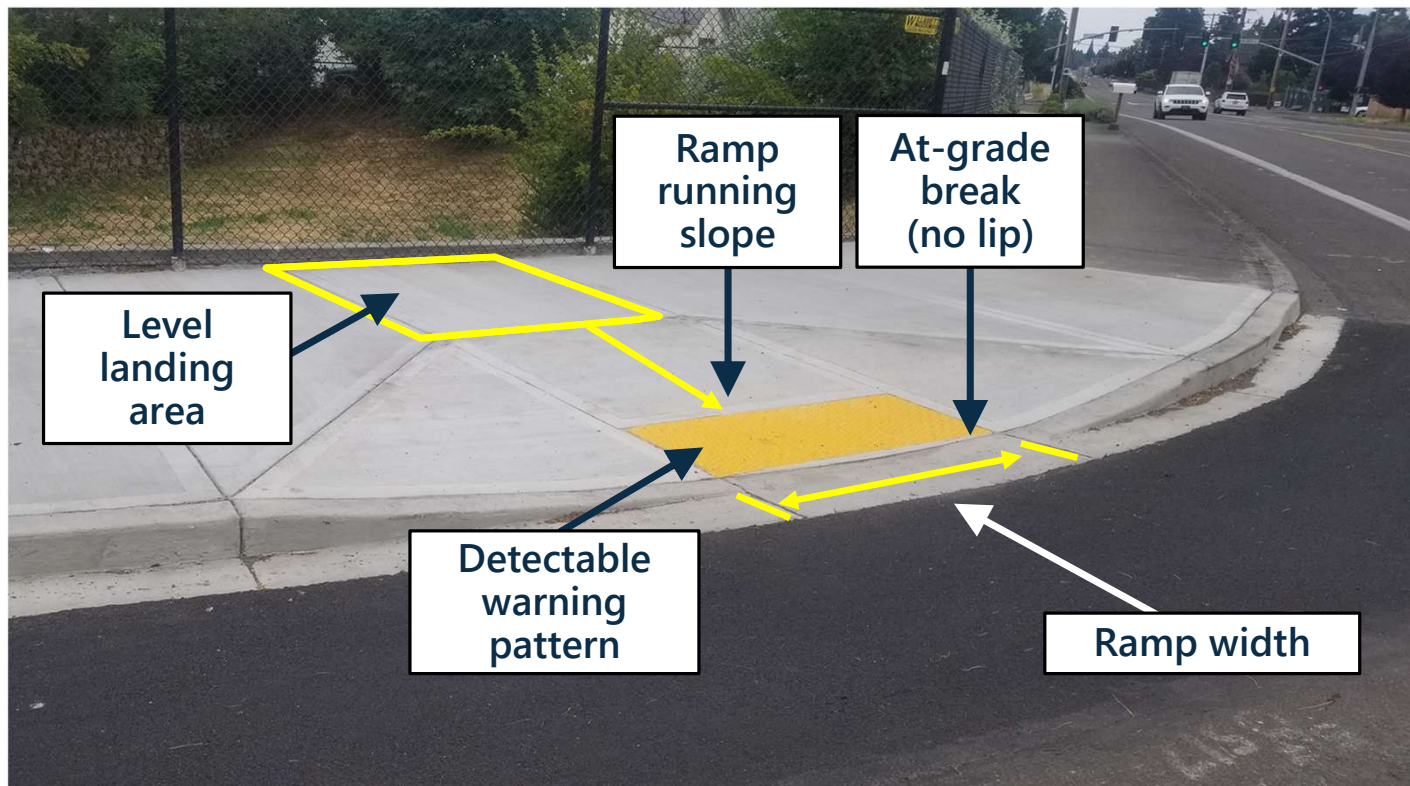
- 2020 Inventory: A private firm walked the streets and assessed ramps.
- GIS Mapping: Apex translated the assessment into a geographic information system (GIS) database and maps.
- Public Involvement: Critical component to reach out and seek input from the general population and the disabled community.

# Inventory Non-Compliant ADA Ramp

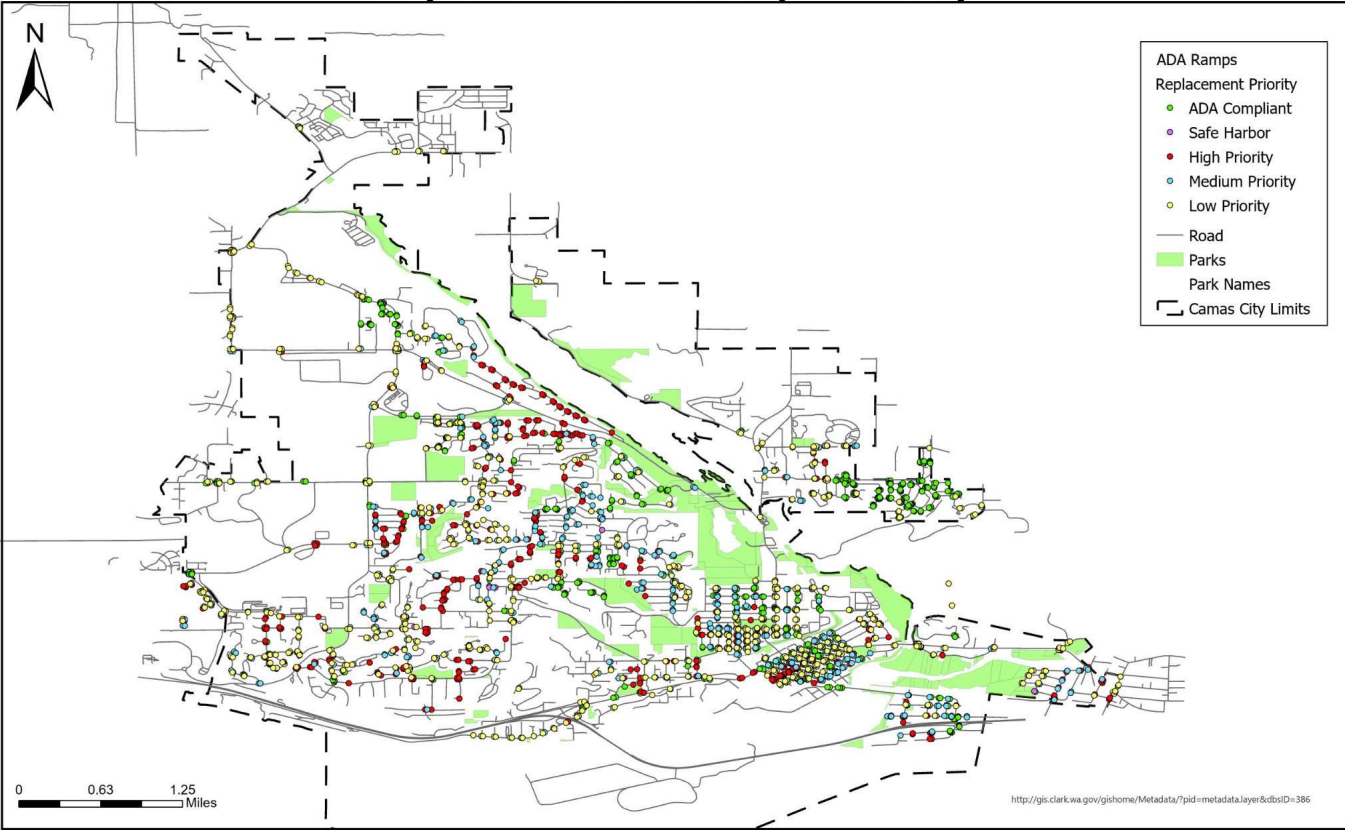


Google. (2025). Google Street View [Image of 45°36'00.3"N 122°27'01.2"W, Oregon, USA]. Retrieved June 10, 2025, from [NW Parker St / NW Pacific Rim Dr - Google](#).

# ADA-Compliant Curb Ramp

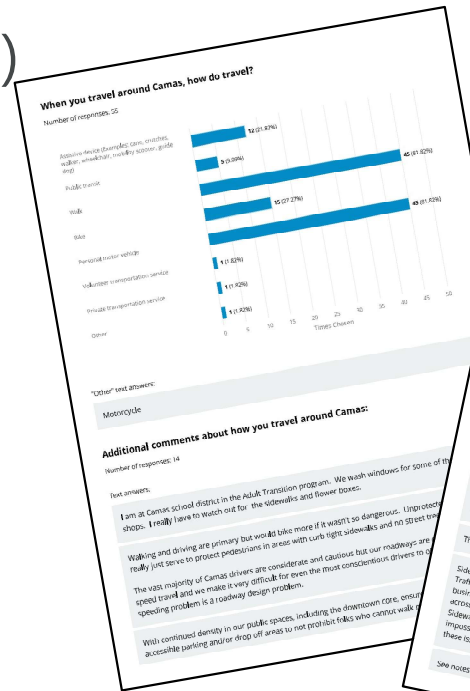


# GIS Mapping



# Public Involvement

- Citizen Advisory Committee (CAC)
- Community Survey
- Engage Camas
- Scribble Maps



# Who Did We Talk To?

- Community members with disabilities
- Community members of Camas
- C-TRAN
- Washington State Department of Transportation (WSDOT)
- Clark County Community Services, Developmental Disabilities Program
- Northwest ADA Center
- People First of Clark County
- Faith-based organizations
- Area Agency on Aging and Disabilities of Southwest Washington
- Vancouver Housing Authority
- Washington State Department of Social and Health Services, Division of Vocational Rehabilitation
- Washington State School for the Blind
- Washington School for the Deaf
- Camas School District
- Camas School District special education leaders
- Camas School District Special Education Parent/Teacher/Student Organization
- Downtown Camas Association

(Red text are active participants)

# Community Survey Overview

## Publicity:

- Community-wide mailer
- EngageCamas.com
- City social media
- Downtown Camas Association social media and e-newsletter
- Email to CAC members and organizations
- Flyers at City Hall and Camas Library

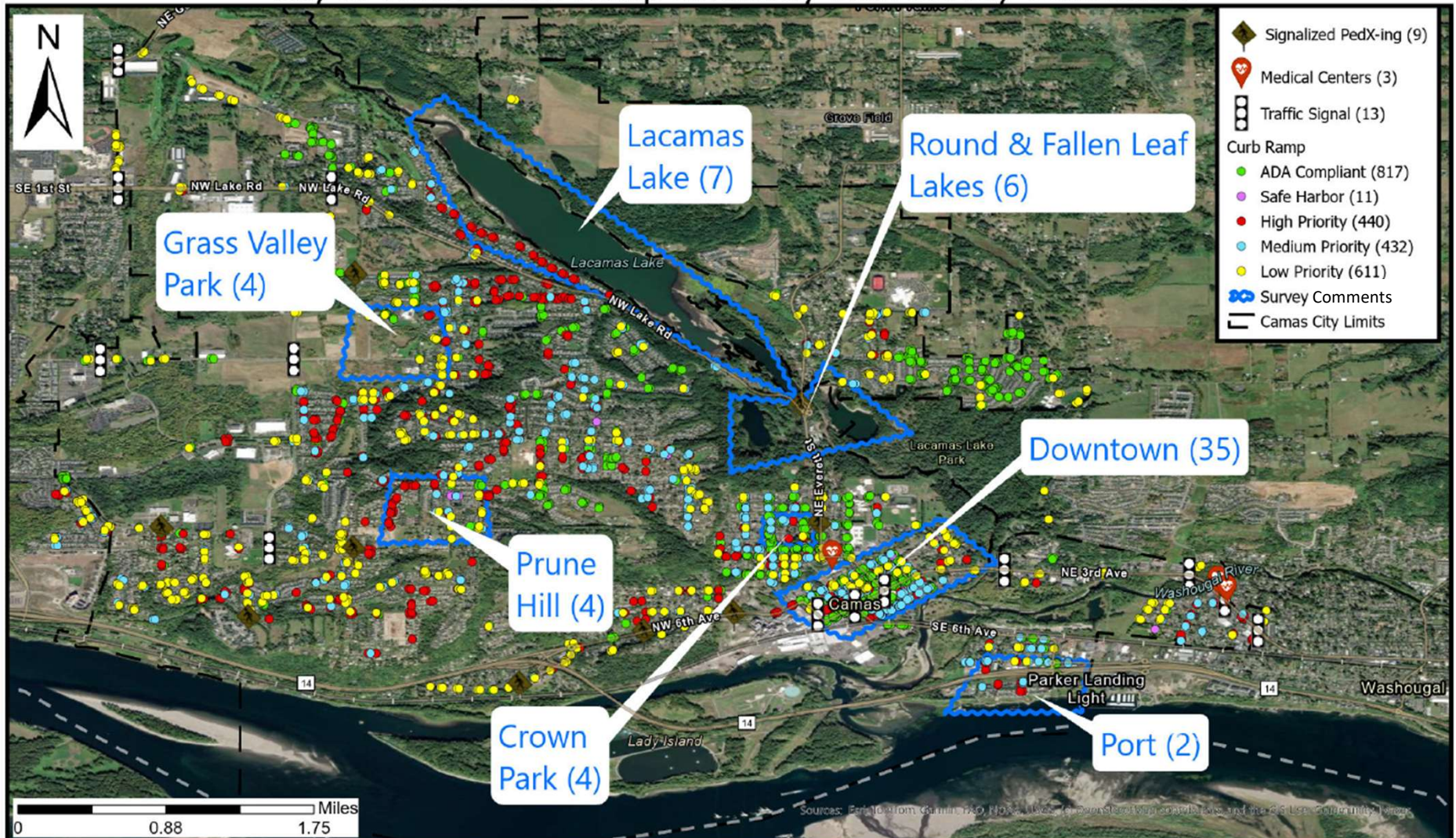
## Details:

- Open August 1 through September 4, 2025
- 12 questions
- 56 responses
- Option for photo uploads
- Online and printed versions
- Printouts at City Hall and Camas Library
- Additional accessibility support available

# What We Learned

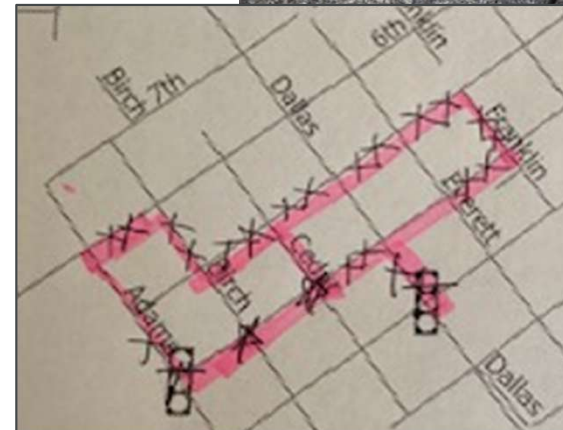
- The inventory evaluated ramps used strict criteria resulting in:
  - Calling for replacement of OK ramps
  - Additional inventory in the downtown
- Downtown access is the highest priority, especially:
  - Sidewalk uplift
  - ADA parking
- Access to parks—especially the trails near the lake—is the second-highest priority.
- Many comments were related to infill of sidewalks.
- ADA improvements related to trails are not within the public right-of-way.

### City of Camas Curb Ramp Inventory With Survey Comments



# Uplifted Sidewalks Downtown

- Create barriers for individuals who rely on mobility assistance devices.
- Adams, Birch, Cedar, Dallas, Everett, and Franklin between 4th & 5th are primary concerns
- 6th & Birch and Dallas & 3rd are also frequently traveled.



# What We Recommend

- Accessible contact and request/grievance process
- Low-cost downtown improvement recommendations
- Project list and cost estimate for the six-year Transportation Improvement Program (TIP)
- Project list and cost estimate for the 20-year Capital Facility Plan (CIP)

**City of Camas**  
WASHINGTON

**ADA Request - Grievance Form DRAFT**

COMPLAINANT NAME: \_\_\_\_\_

DESIGNEE NAME (if applicable): \_\_\_\_\_

Designee Relationship to Complainant (if applicable): \_\_\_\_\_

CONTACT INFORMATION (please indicate):  Complainant  Designee

Address: \_\_\_\_\_

Phone: \_\_\_\_\_ E-Mail: \_\_\_\_\_

**DETAILED DESCRIPTION OF SPECIFIC REQUEST / GRIEVANCE:** (Include all known details such as dates(s), location(s), circumstance(s), person(s) involved, witnesses), etc. Use additional paper if necessary. Attach any other information you believe is pertinent.)

Remedy Requested: (Use additional paper if necessary)

Complainant or Designee Signature: \_\_\_\_\_

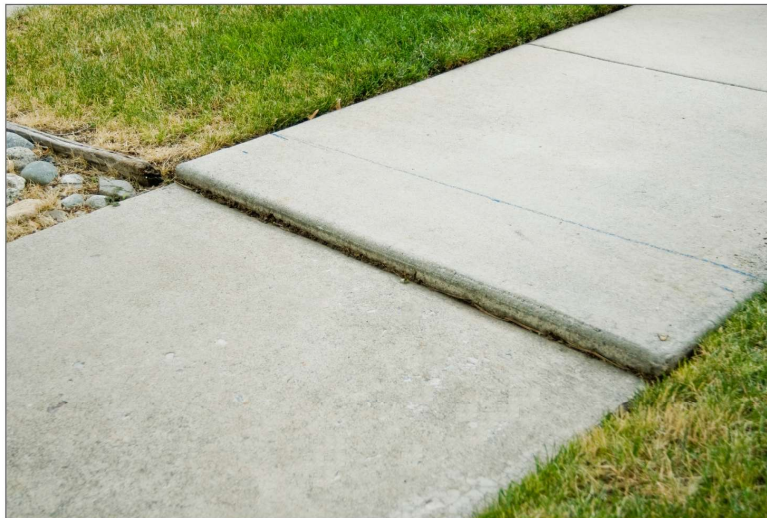
Date: \_\_\_\_\_

RETURN TO: James Carothers - City of Camas  
616 NE 4th Avenue, Camas, WA 98607  
jcarothers@cityofcamas.us

# DRAFT LIST OF PRIORITIZED PROJECTS

# Draft Prioritized Project List (1 of 3)

1. **Downtown:** Upgrade high- and medium-priority ramps with low-cost improvements (6-year TIP).
2. **Downtown:** Upgrade sidewalks by trimming to address tripping and wheelchair barriers (near-term).



## Draft Prioritized Project List (2 of 3)

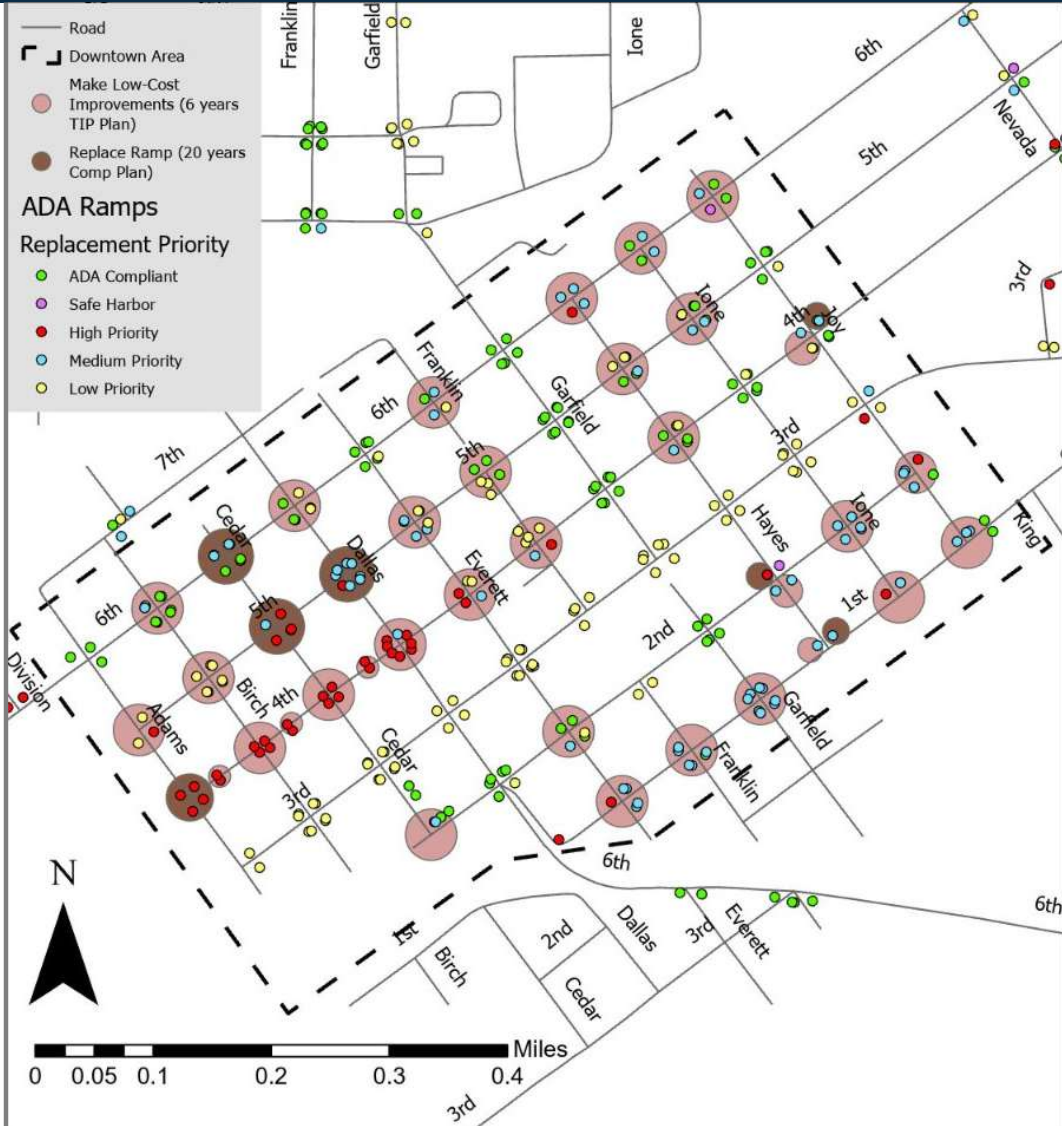
3. **Downtown:** Replace high- and medium-priority ramps (20-year CIP).
4. **Downtown:** Replace sidewalks that are barriers to people experiencing a disability (20-year CIP).
5. **Downtown:** Educate downtown businesses about keeping sidewalks open and barrier-free and encourage barrier removal (near-term).



## Draft Prioritized Project List (3 of 3)

6. **Downtown:** Review the City of Camas ADA Parking Policy and Parking Standards and implement new ADA parking spaces as needed.
7. **Lake Parks (Heritage Park, Fallen Leaf Lake Park, Lacamas Park):** ADA access upgrades on streets within 0.25 mile of entrances.
8. **Neighborhood Parks:** ADA access upgrades on streets within 0.25 mile of entrances.
9. **Traffic Signals:** All traffic signals without ADA-compliant push buttons.
10. **2015 ADA Transition Plan Project:** High-priority locations without completed improvements.

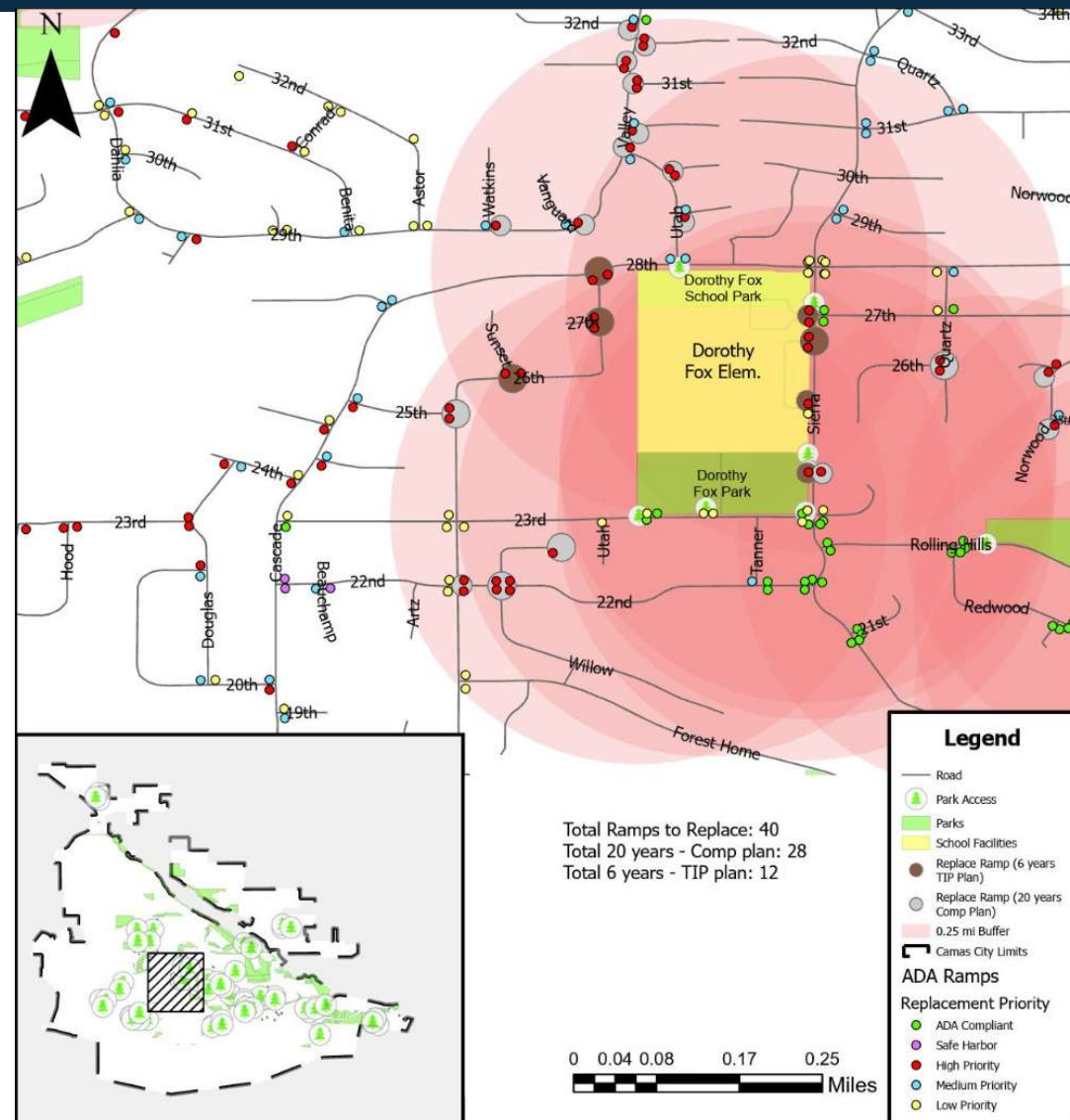
# Curb Ramp Improvement Plan: Downtown



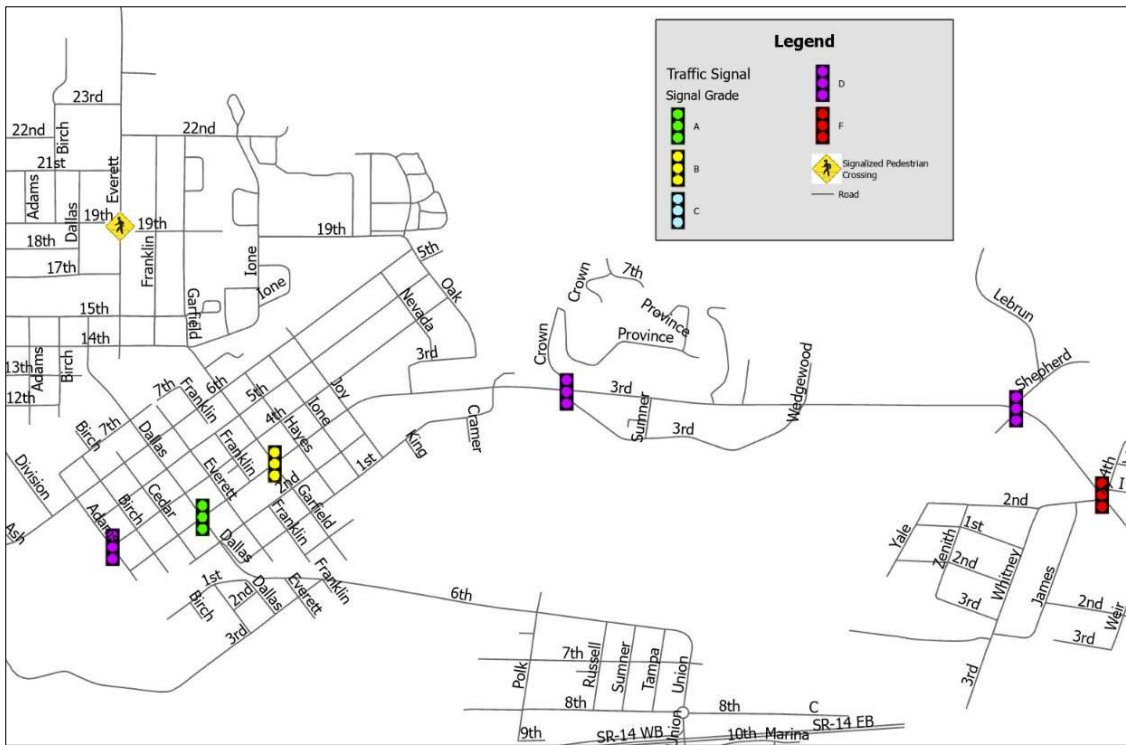
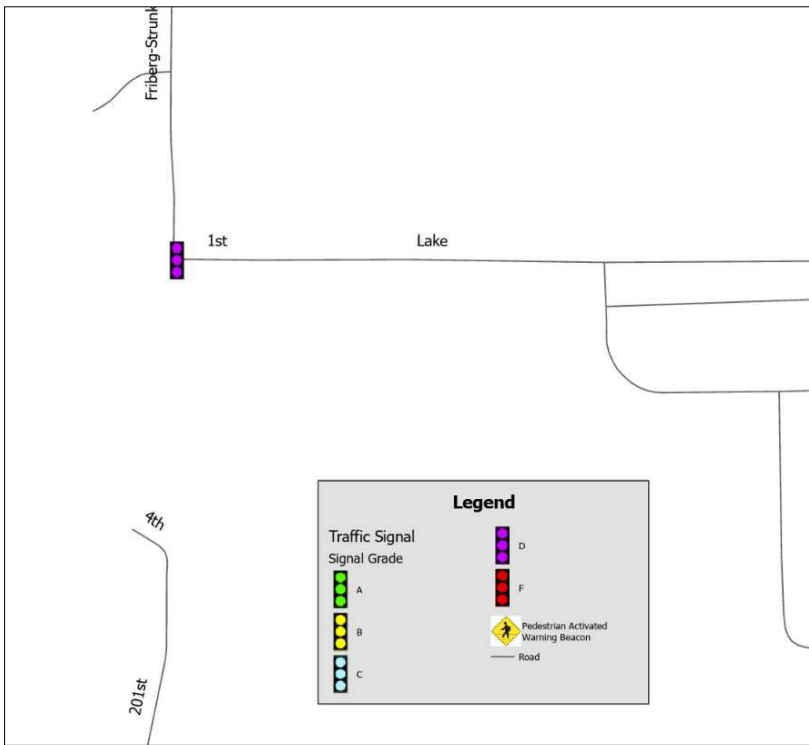
# Curb Ramp Improvement Plan: Dorothy Fox Park

## List of all parks:

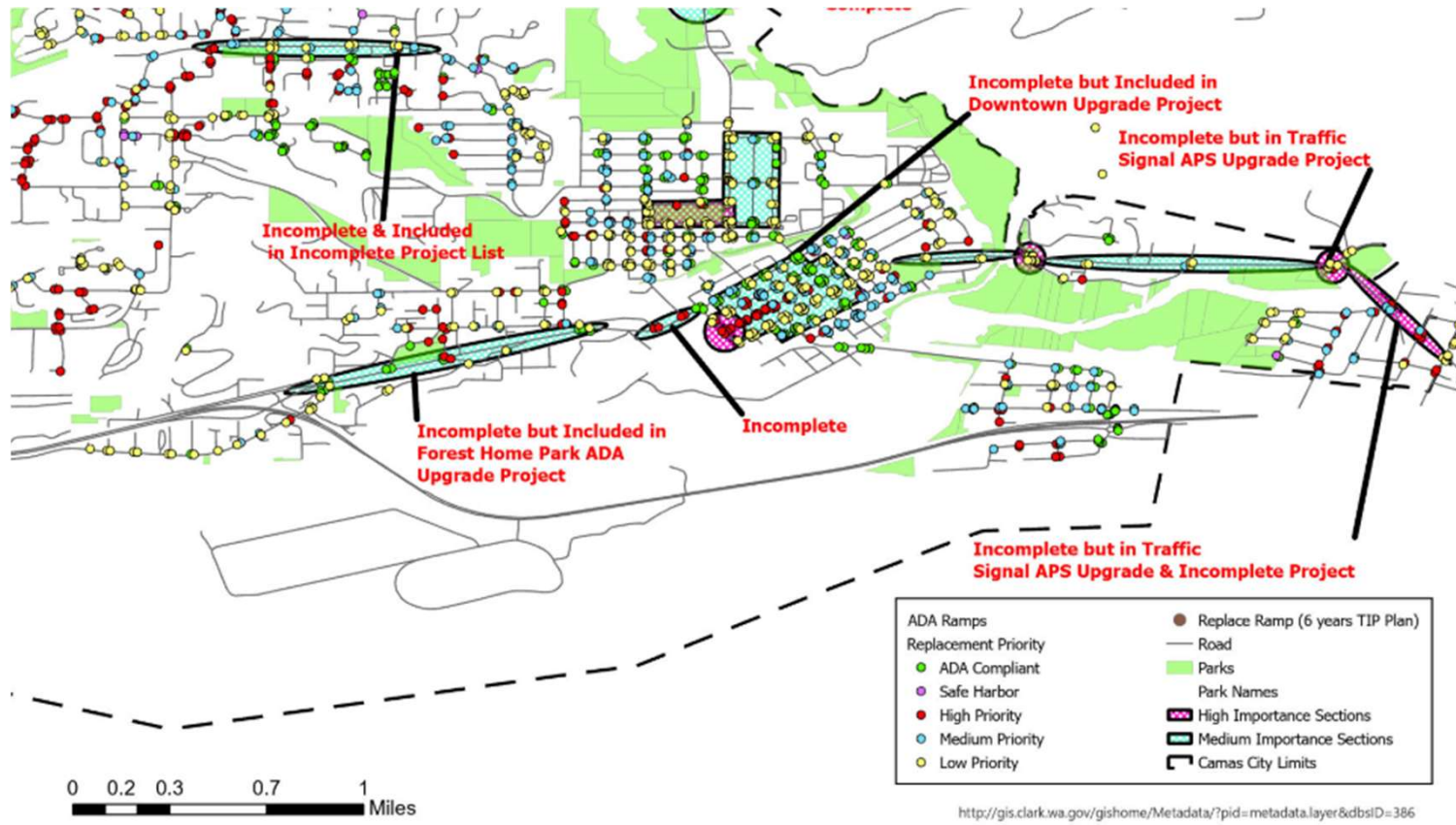
- Grass Valley Park
- Forest Home Park
- Crown Park
- Dorothy Fox Park



# Push Button Improvement Plan: Signals



# 2015 Incomplete Projects Plan



# 2025 ADA Transition Plan Schedule and Cost Estimate

## Planning Level Cost Estimate

Projects	6-Year TIP	20-Year Comp Plan
Downtown	\$68,000	\$356,000
Crown Park	\$170,000	\$139,000
Grass Valley	\$115,000	\$84,000
Forest Home	\$96,000	\$28,000
Dorothy Fox	\$166,000	\$382,000
Push Buttons	--	\$45,000
2015 Incomplete Projects	--	\$298,000
<b>Totals</b>	<b>\$615,000</b>	<b>\$1,332,000</b>

## Next Steps

- Host a virtual open house for the community. ✓
- Hold CAC Session 2 to gather more input. ✓
- Compile a list of high-priority areas and corridors. ✓
- Complete a ranking of existing ADA curb ramps and note missing or substandard curb ramps. ✓
- Hold CAC Session 3 to review a draft plan. ✓
- Prepare a cost estimate to fit the City's budget. ✓
- Seek City Council approval of the new plan. April 20 Council Meeting
- Include the projects in the City's short- and long-term improvement programs.

# Discussion

What are your **questions**?

What are your **comments**?



THANK YOU





February 26, 2026

Curleigh (Jim) Carothers  
City of Camas  
616 NE 4th Avenue  
Camas, Washington 98607

Via email: jcarothers@cityofcamas.us

Regarding: 2025 ADA Transition Plan  
City of Camas – ADA Transition Plan  
616 NE 4th Avenue  
Camas, Washington  
Apex Project 24011088

Dear Mr. Carothers:

This addendum letter serves as an update to the City of Camas' (City) original ADA Transition Plan prepared in 2015 (2015 ADA Transition Plan) and documents updates made since the previous adoption. See Appendix B for the 2015 ADA Transition Plan. The purpose of this addendum is to summarize key changes, including updated inventory data, revised prioritization, updated cost estimates, and adjustments to the implementation schedule. These updates reflect current conditions, completed improvements, and newly identified barriers within the public right-of-way.

This addendum ensures that the ADA Transition Plan remains a living document and continues to guide the City toward compliance with the Americans with Disabilities Act (ADA). By incorporating updated findings and aligning improvements with available funding programs, the City reaffirms its commitment to providing accessible facilities for all members of the community.

## **OVERVIEW**

The Americans with Disabilities Act requires public agencies with responsibility over public facilities and right-of-way to maintain an updated Transition Plan that identifies barriers and outlines a schedule for their removal. The City completed a comprehensive ADA Transition Plan in 2015 to evaluate accessibility within the public right-of-way and establish a framework for compliance. The 2025 ADA Transition Plan fulfills the ADA requirement to periodically review and update this plan by reassessing existing conditions, documenting improvements completed since 2015, incorporating current federal and state accessibility standards, and identifying remaining barriers.

The 2025 ADA Transition Plan is informed by the 2025 ADA Self Evaluation Plan. The findings from the 2025 Self Evaluation Plan are included the 2025 ADA Transition plan are used to provide the scope of work for the projects proposed in the 2025 ADA Transition Plan.

The guidance in this document is based on the Washington State Department of Transportation Local Agencies Guide (LAG) manual. This approach ensures that accessibility upgrades are systematically evaluated and scheduled

based on community feedback and funding availability, while continuing to guide ADA implementation through the City's capital planning and maintenance programs.

This document shall be updated periodically, approximately every 5-10 years, to update project lists and review projects that have been completed.

## **OBJECTIVES**

The objectives outlined below were originally established in the 2015 ADA Transition Plan and were carried forward and incorporated into the 2025 ADA Plan update. These objectives continue to guide the City's approach to identifying barriers, prioritizing improvements, and implementing accessible infrastructure in an inclusive manner.

1. Establish criteria to prioritize locations for ADA improvements.
2. Prioritize streets as high, medium, or low priority for removing barriers impacting the disabled community.
3. Establish a list of projects that will eliminate barriers within the public right-of-way at high-priority locations.
4. Establish a schedule for completing high-priority projects.
5. Provide City staff with policy and procedure recommendations to guide future ADA upgrades as opportunities arise.
6. Complete the plan with a minimum of three Citizens Advisory Committee (CAC) meetings.

Throughout the update process, the CAC played a key role in supporting these objectives by assisting with the development of prioritization criteria, particularly for curb ramp replacements, and by providing location-specific input on needed ADA upgrades. This collaborative effort helped ensure that the updated plan reflects both community experience and City implementation needs. The 2025 Self Evaluation plan documents the CAC process as well as the updated inventory. The following findings are from the 2025 Self Evaluation Plan.

## **SELF EVALUTION PLAN FINDINGS**

Through three Community Advisory Committee (CAC) meetings, survey responses, and a downtown scribble map exercise, community members identified downtown Camas and City parks as the highest-priority areas for ADA improvements. Key concerns included uneven or uplifted sidewalks creating tripping hazards, the need for curb ramp upgrades, improved pedestrian push-button accessibility, clearer accessible parking in downtown, and consistent maintenance of a minimum four-foot clear pedestrian path.

The downtown corridor, particularly Birch Street, Cedar Street, Dallas Street, and Everett Street between 3rd Avenue and 6th Avenue, was identified as a primary focus area. Community input, combined with field verification, refined the prioritization of improvements and confirmed the need for both near-term low-cost improvements and full ramp replacements.

Review of post-2020 improvements found that portions of the original inventory, particularly in the downtown area, no longer reflected existing conditions. Several previously high-priority ramps had already been upgraded, confirming the need to supplement GIS-based prioritization with field verification and engineering judgment. Community input also provided important context in identifying remaining accessibility needs.

Updated evaluations informed the identification of improvement strategies, including full ramp replacements, low-cost ramp improvements, and pedestrian push-button upgrades documented in the 2025 ADA Transition Plan. Most park and lake access points within the public right-of-way were found to be compliant; park-related

improvements remaining high priority were categorized into 6-Year TIP and 20-Year Comp Plan implementation horizons based on severity, connectivity, location, and feasibility.

### **SCOPE OF WORK OF ADA IMPROVEMENTS**

The scope of work for the 2025 ADA Transition Plan was developed based on the CAC and staff input as well as professional judgement. The scope of work for various priorities were summarized in memos, with a scope of work and a cost estimate. See Appendix A for all project scoping memos.

### **Location Prioritization**

Location prioritization is based on criteria identified in the updated Self Evaluation Report and the 2015 ADA Transition Plan. This ranking criterion was discussed at the final CAC meeting that allowed the community to provide feedback. The prioritized project list is as follows:

1. Downtown – Upgrade high- and medium priority ramps for upgrade with low-cost improvements. This is intended as an interim improvement until these ramps are altered.
2. Downtown – Upgrade sidewalks by grinding to address tripping and wheelchair barriers
3. Downtown – replace high- and medium-priority ramps that are not candidates for low-cost interim improvements
4. Downtown – Replace sidewalks that are barriers to people experiencing a disability
5. Downtown – Educate downtown businesses about keeping sidewalks open and barrier free and barrier removal
6. Downtown – Develop the City of Camas ADA Parking Policy and Parking Standards and implement new ADA parking spaces
7. Lake Parks (Heritage Park, Fallen Leaf Lake Park, Lacamas Park) – ADA access upgrades on streets within 0.25 mile of entrance.
8. Neighborhood Parks – ADA access upgrades on streets within 0.25 mile of entrance.
9. Traffic Signals – All traffic signal without ADA compliant push buttons.
10. 2015 ADA Transition Plan Project – High priority locations without completed improvements

A review of the community comments regarding trail systems within Lake Parks determined that these locations are not located within the public right-of-way. As a result, the updated ADA inventory and associated recommendations do not include improvements within the Lake Parks.

Downtown Camas was identified as the City’s highest priority area for ADA improvements based on community input, its high pedestrian activity, and its role as a central location for local businesses. As the community’s central gathering area, ensuring accessible routes and compliant curb ramps within the downtown core is critical to providing equitable access for all users.

To minimize disruption of downtown intersections and sidewalks, lower-cost improvements are proposed as an alternative to full replacements at locations with ramps that can be modified to significantly improve access — such as grinding curb ramp lips, adding detectable warning surfaces, or a combination of both—are recommended to be programmed into the City’s TIP to allow for timely and cost-effective barrier removal. Locations requiring full curb ramp reconstruction or more substantial infrastructure modifications will be incorporated into the City’s 20-Year Comp Plan to ensure long-term compliance and coordination with future capital improvement projects. This phased approach allows the City to make measurable near-term progress while strategically planning for larger investments that advance full ADA compliance over time.

The identified ADA improvements surrounding City parks used similar schedule methodology when suggesting which ramps to include in the TIP and Comp Plan. Ramp replacements located closer to park access points were included in the TIP and all other ramp replacements were categorized in the Comp Plan based on engineering judgment and geographic location.

**COST ESTIMATE**

Apex developed a cost estimating tool to provide planning-level estimates for full curb ramp replacements and low-cost improvements for the city. The spreadsheet is structured to be flexible and user-friendly, allowing City staff to update quantities, unit costs, and inflation assumptions over time. Users can manually adjust the number of full ramp replacements or improvements needed along specific corridors and at individual intersections throughout Camas. The ramp types included in the spreadsheet reflect the most common configurations identified during the inventory process and are consistent with the City of Camas Engineering Standard Details. Approximate quantities of four different ramp types were calculated to support accurate cost estimates.

As future ADA improvements are constructed, the City can incorporate completed work into the inventory database and update the cost tool accordingly to maintain an accurate and up-to-date implementation program for future Transition Plan updates.

Table 1 presents the estimated total project costs proposed for inclusion in the City’s 6-Year Transportation Improvement Program (TIP) and the 20-Year Comprehensive Plan (Comp Plan) from the cost estimating tool. The locations summarized in the table represent high-priority areas identified through input from members of the CAC, as well as field assessments conducted as part of this update. Detailed cost estimates included in the TIP and Comp Plan are provided in the project scoping memos located in Appendix C through Appendix F.

**Table 1. Planning Level Cost Estimates**

<b>Projects</b>	<b>6-Year TIP</b>	<b>20-Year Comp Plan</b>
Downtown	\$68,000	\$356,000
Crown Park	\$170,000	\$139,000
Grass Valley	\$115,000	\$84,000
Forest Home	\$96,000	\$28,000
Dorothy Fox	\$166,000	\$382,000
Push Buttons	--	\$45,000
Incomplete Projects	--	\$298,000
<b>Totals:</b>	<b>\$615,000</b>	<b>\$1,332,000</b>

### **Low-Cost Improvements**

Based on the City's GIS inventory, many curb ramps within the downtown area were initially identified as high priority for replacement. However, following a detailed field review, looking at aerial imagery, and using engineering judgment, it was determined that a significant number of these ramps are already close to ADA compliance and can be addressed with targeted, low-cost improvements rather than full reconstruction.

In many cases, compliance deficiencies are limited to minor vertical lips at the gutter line or the absence of detectable warning surfaces (DWS). These issues can be corrected by grinding the curb ramp lip to create a flush transition with the roadway, installing DWS panels, or implementing a combination of both improvements. By focusing on these low-cost improvements, the City can efficiently reduce barriers and bring a substantial number of downtown curb ramps into compliance without the higher cost and disruption associated with full ramp replacement. This approach allows available funding to be leveraged more effectively while still achieving accessibility goals in the downtown.

Precision Concrete Cutting (PCC), a company specializing in the removal of trip hazards and ADA compliance solutions nationwide, was consulted as part of this process. PCC provided technical input and helped identify curb ramps within the downtown area where lip grinding could be implemented as a low-cost improvement. Apex suggests this company to inventory the downtown curb ramps when proceeding with these updates.

Through coordination with PCC, specific locations were evaluated to determine whether vertical lips could be corrected through precision grinding rather than full ramp reconstruction. Their expertise helped confirm that many of the identified deficiencies could be effectively addressed using this method, providing a cost-efficient and minimally disruptive approach to improving accessibility while maintaining compliance with ADA standards. Appendix B also contains a preliminary inventory list and bid from PCC from their findings with Apex comments comparing low-cost improvement recommendations.

Through the public involvement process, the community expressed a priority to addressing uplifted sidewalks in the downtown. Roots from street trees often damage sidewalks, creating tripping hazards by lifting one side of a cracked panel. Typically, removal of the uplifted sidewalk panel and associated tree root removal is necessary to fully address the problem. However, this can be an expensive process and disruptive to pedestrian circulation. Grinding or trimming the uplifted sidewalk to create a flush surface between panels is a lower-cost alternative to full sidewalk and root removal. This approach can also be done with minimal sidewalk closures. While it may not be a permanent solution, some level of sidewalk cracking should be expected in areas with street trees. In locations with mature trees, growth rates are generally slower and future sidewalk displacement may occur gradually. Precision Concrete Cutting can provide an inspection of the downtown sidewalks and will provide a cost estimate for trimming sidewalk panels to eliminate uplifted sidewalks.

### **City Policy Recommendations**

During the public involvement process, community members expressed a priority for keeping downtown sidewalks free from obstructions that limit pedestrian accessibility. The Public Right-of-Way Accessibility Guidelines (PROWAG) requires sidewalks to maintain a minimum continuous clear width of 4-feet, with a minimum width of 5 feet at least every 200 feet to allow for passing. Restrictions were noted in the downtown area due to street furnishings encroaching into the pedestrian clear zone.

**Recommended Policy:** The City should inform downtown businesses of the 4-foot minimum clear width requirement and encourage compliance.

Community members also expressed a desire for additional ADA parking spaces in the downtown area. This can be frustrating during community events where closed streets restrict access to some of the accessible parking spaces.

Recommended Policy: Manage downtown events to maintain or provide additional accessible parking during events when road closures occur.

## **FUNDING**

The City of Camas will implement these ADA improvements in coordination with the funding programs identified in the City's adopted 6-Year TIP. Aligning ADA upgrades with this planning document ensure that improvements are funded and integrated with other capital improvement efforts. The 2026-2031 City TIP currently assumes an ADA Access Upgrades budget of \$675,000.

The projects identified in this ADA Transition Plan are intended to inform future updates to the City's 20-Year Comp Plan and the City will refine the Comp Plan project list and funding assumptions to reflect the prioritized ADA improvements outlined in this Transition Plan.

Refer to the 2015 ADA Transition Plan for grant funding opportunities not mentioned in this update.

Additional funding opportunities may include improvements completed in conjunction with development projects. As land is developed or redeveloped, frontage improvements are typically required. Corner lots are generally required to install ADA-compliant curb ramps and sidewalks. Whenever pedestrian routes are "altered" by construction, corresponding pedestrian improvements are required. Typically, this applies to the City pavement management program. When streets are repaved, ADA curb ramp upgrades are included. These requirements can also include utility improvements and other construction in the public right-of-way that alter existing substandard ramps.

## **ADA UPGRADE SCHEDULE**

A key component of the ADA Transition Plan is the development of a clear and actionable schedule of improvements that will guide the City toward full compliance with the ADA within the public right-of-way. This schedule identifies and prioritizes the improvements necessary to address barriers within the existing public right-of-way. By establishing a structured implementation framework, the City can systematically remove accessibility barriers and ensure that pedestrian facilities are usable by all members of the community.

The improvement priorities outlined in this section are informed by the findings of the Self-Evaluation Memo and the public involvement process. Community members, including the Citizen Advisory Committee (CAC), provided valuable input regarding locations where accessibility improvements are most needed. CAC feedback consistently identified downtown Camas and areas surrounding local parks as high-priority locations due to their importance as community destinations.

Based on these findings and input, Apex developed a prioritized ranking of projects to guide implementation within the 6-Year TIP and 20-Year Comp Plan. The improvement priorities outlined in this section are informed by the findings of the Self-Evaluation Memo and the public involvement process. Community members, including the CAC, provided valuable input regarding locations where accessibility improvements are most needed. Feedback consistently identified downtown Camas and areas surrounding local parks as high-priority locations due to their

importance as community destinations. Lower-cost improvements in the downtown and full ramp replacements closest to park accesses were listed under the 6-Year TIP to allow for near-term action. Full ramp replacements surrounding parks and downtown Camas were prioritized under the 20-Year Comp Plan, along with push-button upgrades, to support long range funding and implementation.

The 6-Year TIP priority ranking is as follows:

1. Downtown
2. Dorothy Fox
3. Crown Park
4. Grass Valley
5. Forest Home

The 20-Year Comp Plan priority ranking is as follows:

6. Signals
7. Downtown
8. Incomplete 2015 Projects
9. Crown Park
10. Dorothy Fox
11. Grass Valley
12. Forest Home

By integrating ADA improvements into both the 6-Year TIP and the 20-Year Comp Plan, the City establishes a financially responsible and transparent approach to achieving compliance. This integration supports coordinated funding, capital planning, and project delivery, while demonstrating the City's long-term commitment to improving accessibility throughout the community.

## **CLOSING**

Please feel free to contact me at 360.567.2117 or [John.Manix@apexcos.com](mailto:John.Manix@apexcos.com) with any questions or comments.

Sincerely,

John Manix, PE  
Senior Traffic Engineer

cc:

Attachment(s): Appendix A. Project Scoping Memos  
Appendix B. 2015 ADA Transition Plan

ERS:JAM

# **Appendix A**

## **Project Scoping Memos**



**Memorandum**

DATE: December 19, 2025  
TO: Curleigh (Jim) Carothers  
FROM: Apex Companies, LLC  
PROJECT: 24011088—Camas ADA Plan Update  
REGARDING: Camas ADA Plan: Downtown Project List

**DOWNTOWN CAMAS CURB RAMP INVENTORY**

A major component of the 2025 Camas ADA Self-Evaluation Plan update is the geographic information system (GIS) based inventory of all curb ramps within Camas, Washington. Apex Companies, LLC (Apex) developed a replacement priority-ranking methodology for this data based on existing ramp width, detectable warning surface type, and gutter-sidewalk lip and landing conditions. Using this methodology, all the curb ramp data supplied by the City of Camas (Camas) was categorized into three replacement priority levels: high, medium, and low. To support this effort, Apex conducted field investigations to verify the priority of curb ramp replacement and investigate if low-cost improvements can be implemented as an interim improvement to full curb ramp replacement. The field verification found many curb ramps ranked high priority for replacement could be brought up to near compliance with low-cost improvements. This included confirming locations where full curb ramp replacement is necessary.

The public involvement phase of the 2025 ADA Transition Plan Update identified downtown Camas as the most important location for Americans with Disabilities Act (ADA) ramp improvements. Based on the importance that residents placed on downtown Camas, Apex field inventoried all curb ramps within the downtown area and developed a list of projects that could be brought into near compliance through low-cost improvements where feasible. Curb ramps with no viable low-cost improvement options are recommended for full replacement to meet ADA standards. Low-cost improvements may include the installation of detectable warning surfaces (DWS) or grinding the lip of the curb ramp to achieve a flush transition with the roadway. To support this effort, Apex developed a cost-estimating tool for each curb ramp type commonly found throughout the city to estimate both low-cost improvement costs and full replacement costs.

Consistent with the Citizen Advisory Committee (CAC) feedback, downtown Camas is identified as the most important area for ADA upgrades. The CAC members provided field inventory feedback on any ramps or sidewalks that were especially important for upgrading. The CAC confirmed the approach to make interim low-cost improvements to bring the ramps up to near compliance.

The following sections present the project lists for low-cost improvements and full curb ramp replacements. These project lists represent planning-level estimates, and the number of projects within each category may increase or decrease as designs are refined and additional field verification is completed. See the attached cost estimate for additional details. These project lists are categorized as either low-cost improvement projects to be included in the City’s 6-Year Transportation Improvement Program (TIP) or full ramp replacement locations to be included in the 20-Year Comprehensive Plan (Comp Plan).

### **LOW-COST IMPROVEMENT PROJECT LIST**

The following intersections meet the criteria for a low-cost improvement within downtown Camas and are suggested in the 6-Year TIP:

- NE 6th Avenue / NE Birch Street
- NE 6th Avenue / NE Dallas Street
- NE 6th Avenue / NE Franklin Street
- NE 6th Avenue / NE Hayes Street
- NE 6th Avenue / NE Ione Street
- NE 6th Avenue / NE Joy Street
- NE 6th Avenue / NE Adams Street
- NE 6th Avenue / NE Birch Street
- NE 6th Avenue / NE Everett Street
- NE 5th Avenue / NE Franklin Street
- NE 5th Avenue / NE Hayes Street
- NE 5th Avenue / NE Ione Street
- Mid-block crossing on NE 4th Avenue between NE Adams Street and NE Birch Street
- NE 4th Avenue / NE Birch Street
- Mid-block crossing on NE 4th Avenue between NE Birch Street and NE Cedar Street
- NE 4th Avenue / NE Cedar Street
- Mid-block crossing on NE 4th Avenue between NE Cedar Street and NE Dallas Street
- NE 4th Avenue / NE Dallas Street
- NE 4th Avenue / NE Everett Street
- NE 4th Avenue / NE Franklin Street
- NE 4th Avenue / NE Hayes Street
- NE 4th Avenue / NE Joy Street
  - Southeast and southwest corners
- NE 2nd Avenue / NE Cedar Street
- NE 2nd Avenue / NE Everett Street
- NE 2nd Avenue / NE Hayes Street
  - Northeast and southeast corners
- NE 2nd Avenue / NE Ione Street
- NE 2nd Avenue / NE Joy Street
- NE 1st Avenue / NE Everett Street
- NE 1st Avenue / NE Franklin Street
- NE 1st Avenue / NE Garfield Street
- NE 1st Avenue / NE Hayes Street
  - Southwest corner
- NE 1st Avenue / NE Ione Street
- NE 1st Avenue / NE Joy Street

The total cost of these low-cost improvements within the downtown for inclusion in the 6-Year TIP is approximately \$68,000.

### **CURB RAMP REPLACEMENT PROJECT LIST**

The following intersections meet the criteria for full ramp replacements within downtown Camas and are suggested for inclusion in the 20-Year Comp Plan:

- NE 6th Avenue / NE Cedar Street
- NE 5th Avenue / NE Cedar Street
- NE 5th Avenue / NE Dallas Street
- NE 4th Avenue / NE Joy Street
  - Northwest corner
- NE 2nd Avenue / NE Hayes Street
  - Southwest corner
- NE 1st Avenue / Hayes Street
  - Northwest corner

The total cost of the ramp replacements within the downtown for inclusion in the 20-Year Comp Plan is approximately \$356,000.

Attachments: Camas ADA Transition Plan – Downtown Intersection Ramp Improvements Figure  
Downtown ADA Upgrade Cost Estimates

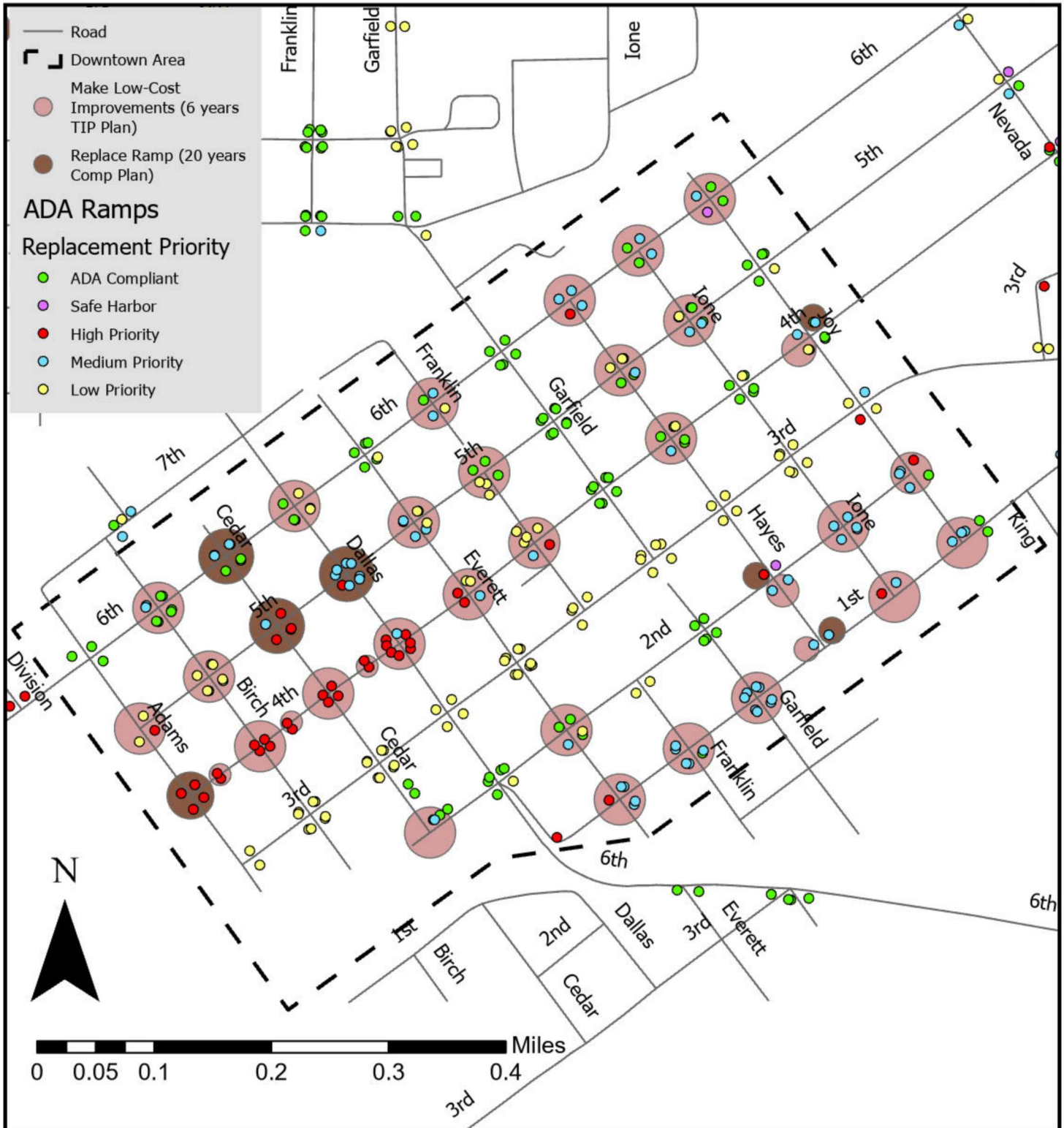
cc: Chris Lopez (City of Camas)

ESJAM:tl

# Camas ADA Transition Plan

Item 1.

## Downtown Intersection Ramp Improvements



Camas ADA Transition Plan - Transportation Improvement Plan Cost Estimate

Intersection / Corridor Name: 1st Ave, 2nd Ave, 4th Ave, 5th Ave, and 6th Ave corridors  
 Area within Camas: Downtown

Item No.	Approx. Qty.	Unit	Description	Unit Price	Total
<b>Street Construction</b>					
1		LS	Mobilization (10% of Street Construction Total)		\$5,360
2	1	EACH	Erosion Control per ramp	\$200	\$0
3	1	EACH	Temporary Traffic Control & Signing per ramp	\$500	\$0
4	1	EACH	Removal of Structures & Obstructions per ramp	\$300	\$0
5	0	EACH	ADA Corner - ST21 / ST24 (Single Parallel / Mid-Block)	\$8,100	\$0
6	0	EACH	ADA Corner - ST20 (Single Perpendicular)	\$8,700	\$0
7	0	EACH	ADA Corner - ST21A (Dual Parallel)	\$14,700	\$0
8	0	EACH	ADA Corner - ST20A (Dual Perpendicular)	\$18,200	\$0
9	52	EACH	Low cost - Grind lip	\$300	\$15,600
10	28	EACH	Low Cost - Detectable Warning Surface (DWS)	\$500	\$14,000
11	30	EACH	Low Cost - Grind lip & DWS	\$800	\$24,000
12		EACH	Minor Changes (20% of Ramp Total Cost)		\$0

Street Subtotal	\$53,600
Sales Tax (0%)	\$0
Street Construction Total without Mobilization	\$53,600
<b>Street Construction Total</b>	<b>\$59,000</b>

Item No.	Approx. Qty.	Unit	Description	Unit Price	Total
<b>Miscellaneous</b>					
10	0	LS	Materials Testing	\$125	\$0
11	1	LS	Construction Design (15% of Street Construction Cost)		\$8,040
12	1	LS	Engineering Design (15% of Ramp Total Cost)		\$0

Miscellaneous Subtotal	\$8,040
Sales Tax (0%)	\$0
<b>Miscellaneous Total</b>	<b>\$8,100</b>

TOTAL PROJECT COST WITHOUT MOBILIZATION \$61,700

**TOTAL PROJECT COST \$68,000**

**List of exclusions:**

Utility pole relocations  
 Right-of-way  
 Traffic signals to be determined

**Assumptions:**

Catch basin adjustments: \$1,500 per every 4 ramps  
 Low cost improvement assumes revising the existing ramp by adding either DWS, grinding lip, or both

**List of typical obstructions:**

Sign poles  
 Mailboxes

\*The number of recommended ramp replacements & low-cost improvements is an estimate and may increase or decrease as further work is conducted

Camas ADA Transition Plan - Comprehensive Plan Cost Estimate

Intersection / Corridor Name: 1st Ave, 2nd Ave, 4th Ave, 5th Ave, and 6th Ave corridors  
 Area within Camas: Downtown

Item No.	Approx. Qty.	Unit	Description	Unit Price	Total
<b>Street Construction</b>					
1		LS	Mobilization (10% of Street Construction Total)		\$25,880
2	1	EACH	Erosion Control per ramp	\$200	\$3,600
3	1	EACH	Temporary Traffic Control & Signing per ramp	\$500	\$9,000
4	1	EACH	Removal of Structures & Obstructions per ramp	\$300	\$5,400
5	0	EACH	ADA Corner - ST21 / ST24 (Single Parallel / Mid-Block)	\$8,100	\$0
6	13	EACH	ADA Corner - ST20 (Single Perpendicular)	\$8,700	\$113,100
7	1	EACH	ADA Corner - ST21A (Dual Parallel)	\$14,700	\$14,700
8	4	EACH	ADA Corner - ST20A (Dual Perpendicular)	\$18,200	\$72,800
9	0	EACH	Low cost - Grind lip	\$300	\$0
10	0	EACH	Low Cost - Detectable Warning Surface (DWS)	\$500	\$0
11	0	EACH	Low Cost - Grind lip & DWS	\$800	\$0
12		EACH	Minor Changes (20% of Ramp Total Cost)		\$40,120

Street Subtotal	\$258,720
Sales Tax (0%)	\$0
Street Construction Total without Mobilization	\$258,800
<b>Street Construction Total</b>	<b>\$284,600</b>

Item No.	Approx. Qty.	Unit	Description	Unit Price	Total
<b>Miscellaneous</b>					
10	18	LS	Materials Testing	\$125	\$2,250
11	1	LS	Construction Design (15% of Street Construction Cost)		\$38,820
12	1	LS	Engineering Design (15% of Ramp Total Cost)		\$30,090

Miscellaneous Subtotal	\$71,160
Sales Tax (0%)	\$0
<b>Miscellaneous Total</b>	<b>\$71,200</b>

TOTAL PROJECT COST WITHOUT MOBILIZATION	\$330,000
<b>TOTAL PROJECT COST</b>	<b>\$356,000</b>

**List of exclusions:**

Utility pole relocations  
 Right-of-way  
 Traffic signals to be determined

**Assumptions:**

Catch basin adjustments: \$1,500 per every 4 ramps  
 Low cost improvement assumes revising the existing ramp by adding either DWS, grinding lip, or both

**List of typical obstructions:**

Sign poles  
 Mailboxes

\*The number of recommended ramp replacements & low-cost improvements is an estimate and may increase or decrease as further work is conducted



## Memorandum

DATE: December 18, 2025

TO: Curleigh (Jim) Carothers

FROM: Apex Companies, LLC

PROJECT: 24011088—Camas ADA Plan Update

REGARDING: Camas ADA Plan: City Parks Project List

### PARKS CURB RAMP INVENTORY

A major component of the 2025 Camas ADA Self-Evaluation Plan update is the geographic information system (GIS) based inventory of all curb ramps within Camas, Washington. Apex Companies, LLC (Apex) developed a replacement priority-ranking methodology for this data based on existing ramp width, detectable warning surface type, and gutter-sidewalk lip and landing conditions. Using this methodology, all curb ramp data provided by the City of Camas (Camas) were categorized into three replacement priority levels: high, medium, and low. To support this effort, Apex conducted a GIS analysis to identify high-priority curb ramps by location. A quarter-mile buffer was created around each park access point, and only high-priority ramps located within these buffers were selected for replacement. Many curb ramps initially ranked as high priority could be brought to near compliance through low-cost improvements rather than full replacement but at this time full replacement is recommended.

The public involvement phase of the 2025 ADA Transition Plan Update identified these parks' locations in Camas as important location for Americans with Disabilities Act (ADA) improvements. Curb ramps near parks are recommended for full replacement to meet ADA standards. Low-cost improvements may be a viable alternative such as installation of detectable warning surfaces (DWS) or grinding the lip of the curb ramp to achieve a flush transition with the roadway but further work is necessary. To support this effort, Apex developed a cost-estimating tool for each curb ramp type commonly found throughout the city to estimate both low-cost improvement costs and full replacement costs. See the attached cost estimate for more details.

These project lists represent planning-level estimates, and the number of projects within each category may increase or decrease as designs are refined and additional field verification is completed. The projects were categorized as either Transportation Improvement Plan (TIP) projects or Comprehensive Plan projects (see associated maps and cost estimates). This classification was based on factors such as urgency, geographic location, engineering judgment, and familiarity with the area.

### GRASS VALLEY PARK CURB RAMP IMPROVEMENT PROJECT LIST

The following intersections meet the criteria for a high priority for replacement within Grass Valley Park surrounding area:

- NW JULIA ST / ADDRESS #3340
- NW EMILY WAY / NW 44TH AVE
- NW EMILY WAY / NW DAHLIA DR
- NW LEADBETTER PKWY/NW DAHLIA DR / NW DAHLIA DR/NW LEADBETTER PKWY
- NW DAHLIA LP / NW DAHLIA DR

- NW DAHLIA DR / NW 34TH CIR
- NW HUNSAKER CT / HOUSE #3336
- NW PACIFIC RIM DR / NW IRON HORSE CT

The total cost of the ramp replacements within the Grass Valley Park area is approximately \$199,000 with \$115,000 allocated to the Transportation Improvement Plan (TIP) and \$84,000 allocated to the Comprehensive Plan.

### **CROWN PARK CURB RAMP IMPROVEMENT PROJECT LIST**

The following intersections meet the criteria for a high priority for replacement within Crown Park surrounding area:

- NE 14TH AVE / NE DALLAS ST
- NE 14TH AVE / DIVISION ST
- NW COUCH ST / NW 14TH AVE
- NW 15TH AVE/NW BENTON ST
- NW 15TH AVE/NW ASH ST
- NE 13TH AVE/NE ADAMS ST
- NE 17TH AVE / NE 17TH AVE (PARK HANDICAP ACCESS)
- NE 17TH AVE / NE DALLAS ST (CROWN PARK ACCESS)
- NW 13TH AVE / DIVISION ST
- NE 21ST AVE / NE BIRCH ST
- NE BIRCH ST / NE 19TH AVE
- NW BENTON ST / NW 18TH AVE

The total cost of the ramp replacements within the Crown Park area is approximately \$309,000 with \$170,000 allocated to the Transportation Improvement Plan (TIP) and \$139,000 allocated to the Comprehensive Plan.

### **FOREST HOME PARK CURB RAMP IMPROVEMENT PROJECT LIST**

The following intersections meet the criteria for a high priority for replacement within Forest Home Park surrounding area

- NW LOGAN ST / FOREST HOME PARK (EAST PARKING LOT)
- NW 8TH AVE / CUL DE SAC / ADDRESS #1039
- NW LOGAN ST / NW 8TH AVE
- NW 7TH AVE / NW MEADOW RIDGE LN (PVT) (EAST INTERSECTION)
- NW 7TH AVE / NW IVY ST (WEST INTERSECTION)
- NW 7TH AVE / NW IVY ST / NW LOGAN ST
- NW IVY ST / NW 6TH AVE
- NW IVY ST / FOREST HOME PARK PARKING LOT
- NW IVY DR / NW 6TH AVE

The total cost of the ramp replacements within the Forest Home Park area is approximately \$124,000 with \$96,000 allocated to the Transportation Improvement Plan (TIP) and \$28,000 allocated to the Comprehensive Plan.

### **DOROTHY FOX PARK CURB RAMP IMPROVEMENT PROJECT LIST**

The following intersections meet the criteria for a high priority for replacement within Dorothy Fox Park surrounding area:

- NW VALLEY ST / NW 28TH AVE
- NW VALLEY ST / NW 27TH CIR
- NW 26TH AVE / NW SUNSET CT
- NW ASTOR ST / NW 25TH AVE
- NW ASTOR ST / NW 22ND AVE
- NW SIERRA ST / DOROTHY FOX ELEMENTARY (EXIT ONLY)
- NW SIERRA ST / PARK CROSSWALK
- NW 22ND AVE / NW 22ND CIR
- NW 22ND AVE / NW WILLOW DR
- NW 22ND CIR / END OF STREET / ADDRESS #2314
- NW 25TH AVE / NW NORWOOD PL
- NW 26TH CIR / NW NORWOOD PL
- NW 26TH AVE / NW QUARTZ ST
- NW 27TH AVE (DOROTHY FOX ELEMENTARY) / NW SIERRA ST
- DOROTHY FOX ELEMENTARY (STAFF PARKING) / NW SIERRA ST
- NW UTAH ST / NW VALLEY ST
- NW 29TH CIR (PVT) / NW UTAH ST
- NW 30TH CIR (PVT) / NW UTAH ST
- NW 31ST CIR (PVT) / NW VALLEY ST
- NW 31ST WAY (PVT) / NW VALLEY ST
- NW 32ND CIR (PVT) / NW VALLEY ST
- NW VANGUARD PL (PVT) / NW 29TH AVE
- NW WATKINS CT / NW 29TH AVE

The total cost of the ramp replacements within the Dorothy Fox Park area is approximately \$548,000 with \$166,000 allocated to the Transportation Improvement Plan (TIP) and \$382,000 allocated to the Comprehensive Plan.

Attachments: Camas ADA Transition Plan – Parks Intersection Ramp Improvements Figures  
Parks ADA Upgrade Cost Estimates

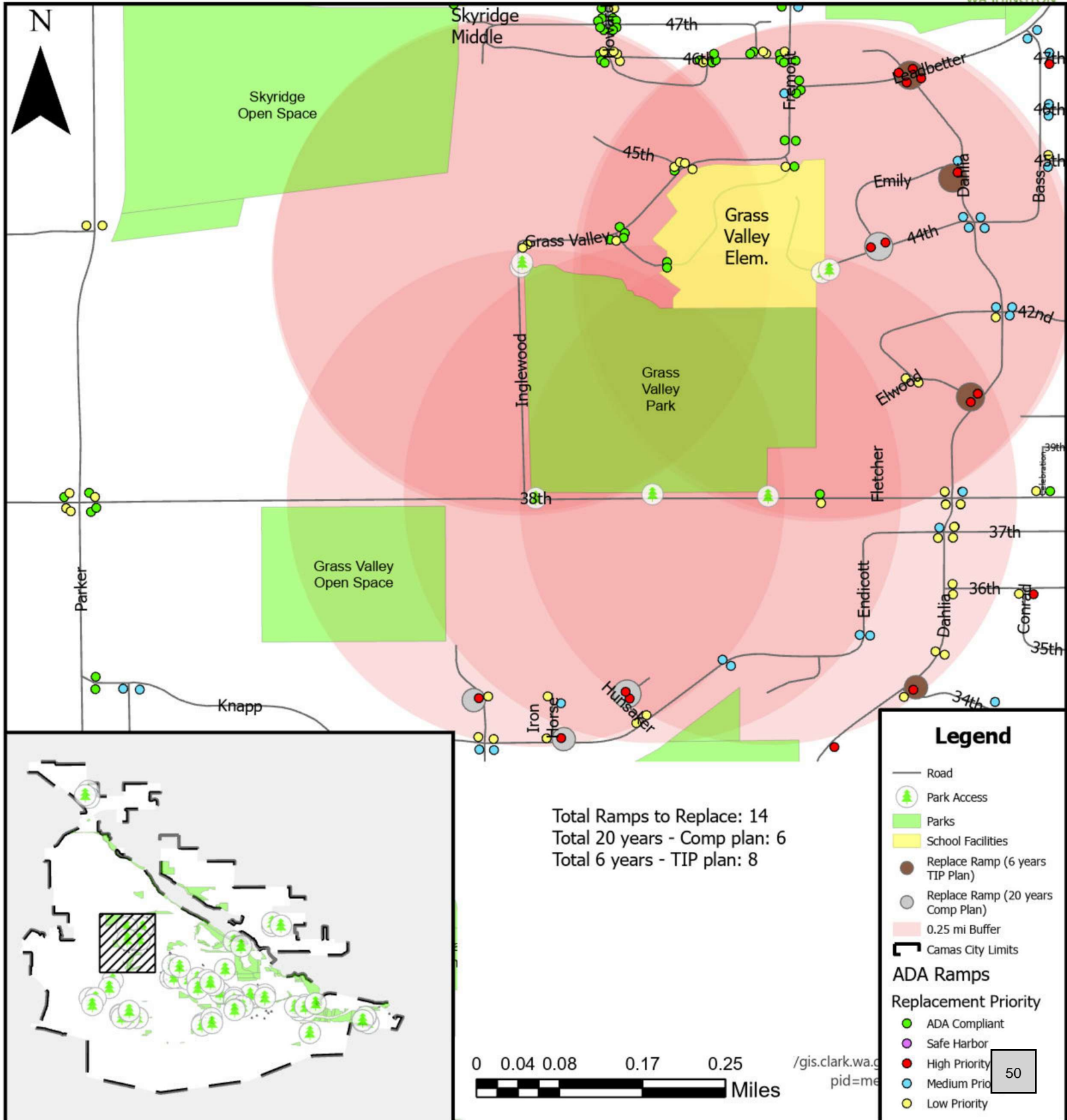
cc: Chris Lopez (City of Camas)

MT: JAM

# Camas ADA Transition Plan Update

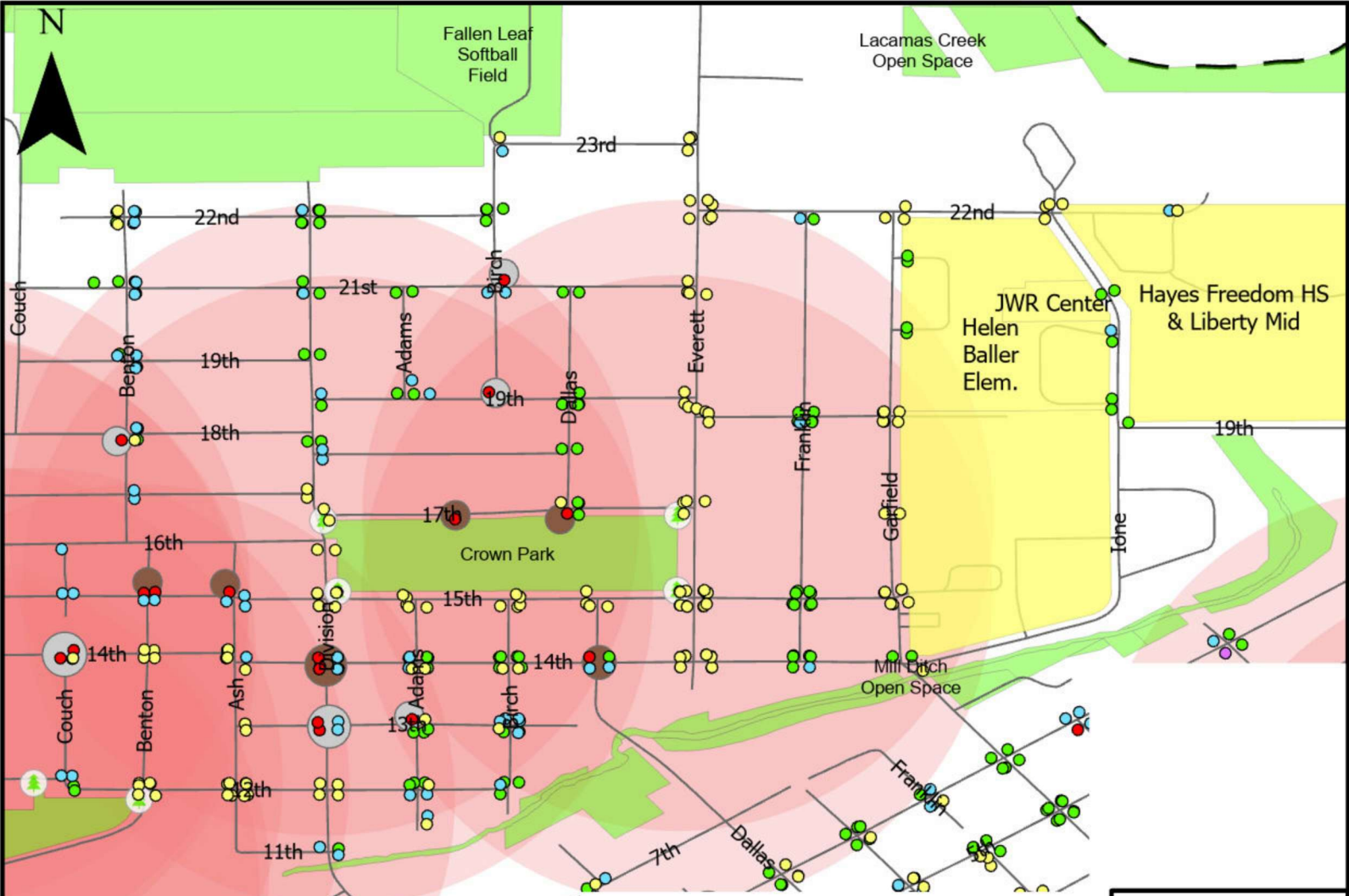
Item 1.

## Ramps Within 1/4 Mile of Grass Valley Park Access



# Camas ADA Transition Plan Update Item 1.

## Ramps Within 1/4 Mile of Crown Park Access



Total Ramps to Replace: 23  
 Total 20 years - Comp plan: 10  
 Total 6 years - TIP plan: 12



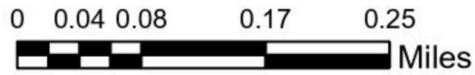
### Legend

- Road
- Park Access
- Parks
- School Facilities
- Replace Ramp (6 years TIP Plan)
- Replace Ramp (20 years Comp Plan)
- 0.25 mi Buffer
- Camas City Limits

### ADA Ramps

Replacement Priority

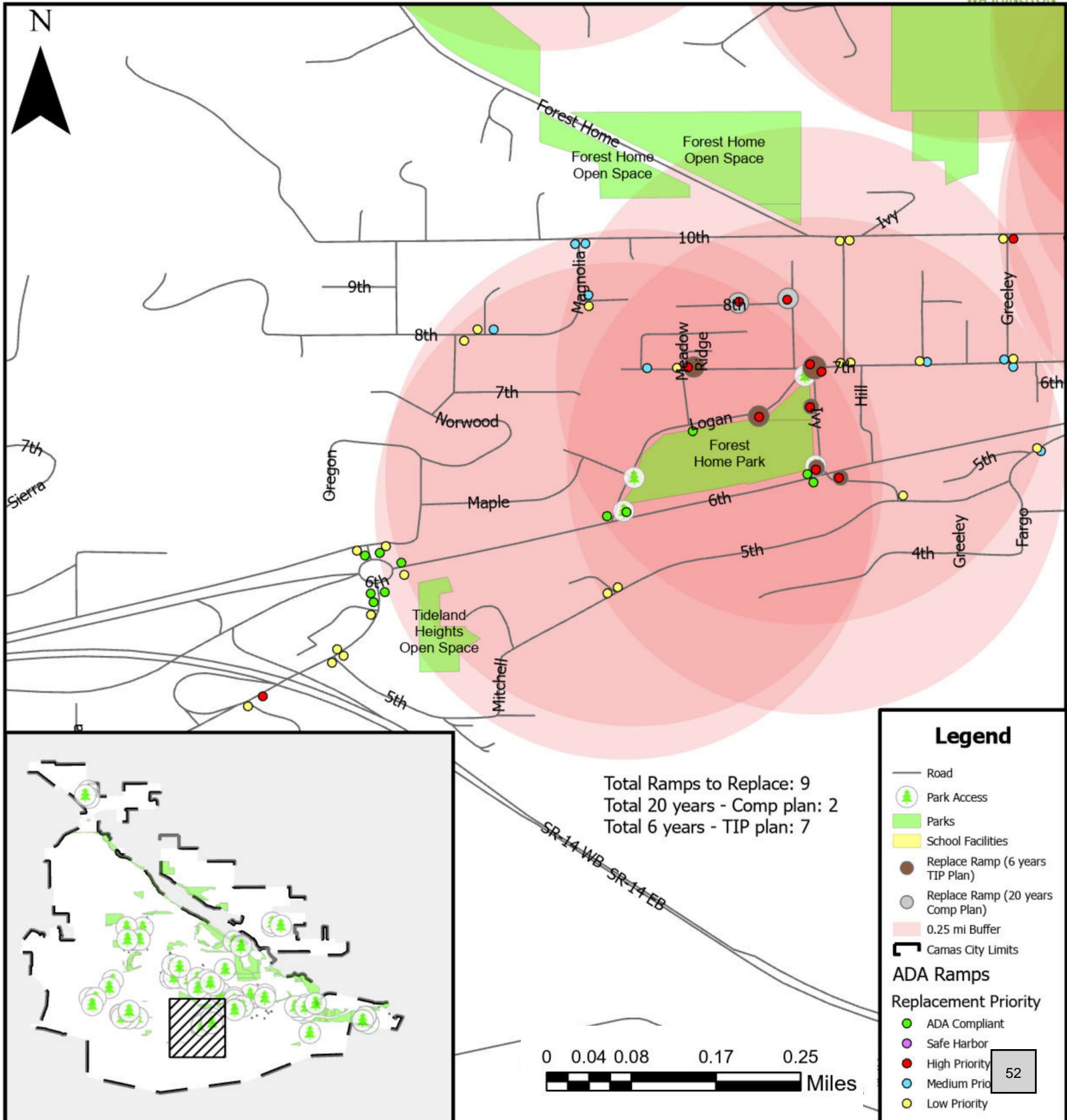
- ADA Compliant
- Safe Harbor
- High Priority
- Medium Priority
- Low Priority



# Camas ADA Transition Plan Update

Item 1.

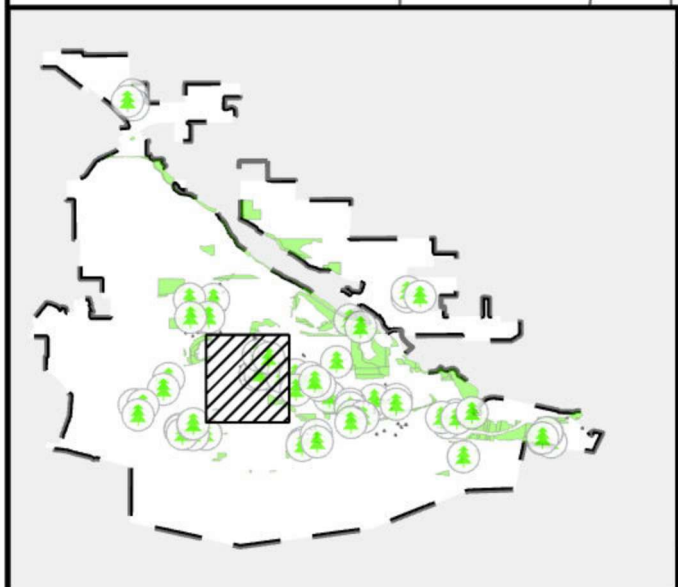
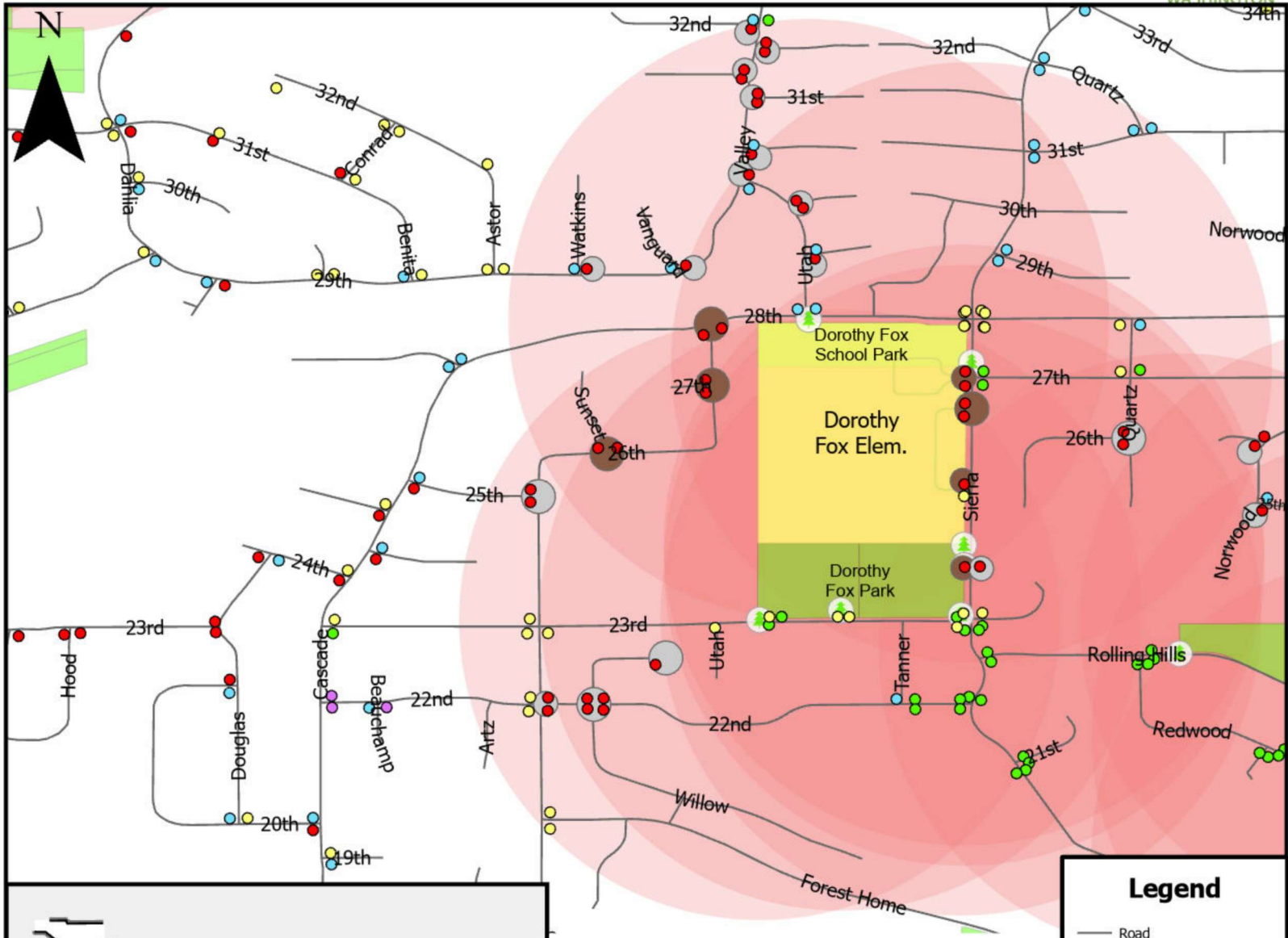
## Ramps Within 1/4 Mile of Forest Home Park Access



# Camas ADA Transition Plan Update

Item 1.

## Ramps Within 1/4 Mile of Dorothy Fox Park Access



Total Ramps to Replace: 40  
 Total 20 years - Comp plan: 28  
 Total 6 years - TIP plan: 12

**Legend**

- Road
- 🌲 Park Access
- 🌳 Parks
- 🏫 School Facilities
- 🕒 Replace Ramp (6 years TIP Plan)
- 🕒 Replace Ramp (20 years Comp Plan)
- 🟡 0.25 mi Buffer
- 📏 Camas City Limits

**ADA Ramps**

**Replacement Priority**

- 🟢 ADA Compliant
- 🟣 Safe Harbor
- 🔴 High Priority
- 🟡 Medium Priority
- 🟠 Low Priority



Camas ADA Transition Plan - Transportation Improvement Plan Cost Estimate

Intersection / Corridor Name: Not Applicable  
 Area within Camas: Grass Valley Park

Item No.	Approx. Qty.	Unit	Description	Unit Price	Total
<b>Street Construction</b>					
1		LS	Mobilization (10% of Street Construction Total)		\$9,010
2	1	EACH	Erosion Control per ramp	\$200	\$1,600
3	1	EACH	Temporary Traffic Control & Signing per ramp	\$500	\$4,000
4	1	EACH	Removal of Structures & Obstructions per ramp	\$300	\$2,400
5	2	EACH	ADA Corner - ST21 / ST24 (Single Parallel / Mid-Block)	\$8,100	\$16,200
6	6	EACH	ADA Corner - ST20 (Single Perpendicular)	\$8,700	\$52,200
7	0	EACH	ADA Corner - ST21A (Dual Parallel)	\$14,700	\$0
8	0	EACH	ADA Corner - ST20A (Dual Perpendicular)	\$18,200	\$0
9	0	EACH	Low Cost - Grind lip	\$500	\$0
10	0	EACH	Low Cost - Detectable Warning Surface (DWS)	\$500	\$0
11	0	EACH	Low Cost - Grind lip & DWS	\$1,000	\$0
12		EACH	Minor Changes (20% of Ramp Total Cost)		\$13,680

Street Subtotal	\$90,080
Sales Tax (0%)	\$0
Street Construction Total without Mobilization	\$90,100
<b>Street Construction Total</b>	<b>\$90,100</b>

Item No.	Approx. Qty.	Unit	Description	Unit Price	Total
<b>Miscellaneous</b>					
10	8	LS	Materials Testing	\$125	\$1,000
11	1	LS	Construction Design (15%) of Street Construction Cost		\$13,515
12	1	LS	Engineering Design (15%) of Ramp Total Cost		\$10,260

Miscellaneous Subtotal	\$24,775
Sales Tax (0%)	\$0
<b>Miscellaneous Total</b>	<b>\$24,800</b>

TOTAL PROJECT COST WITHOUT MOBILIZATION	\$114,900
<b>TOTAL PROJECT COST</b>	<b>\$115,000</b>

**List of exclusions:**

- Utility pole relocations
- Right-of-way
- Traffic signals to be determined

**Assumptions:**

- Catch basin adjustments: \$1,500 per every 4 ramps
- Low cost improvements assume revising the existing ramp by adding either DWS, grinding lip flush or both
- \* Near school

**List of typical obstructions:**

- Sign poles
- Mailboxes

**Grass Valley Park Ramps**

Street Name	Cross_Street	Corner	ST#
NW EMILY WAY	NW DAHLIA DR	SW	ST21
NW LEADBETTER PKWY/NW DAHLIA DR	NW DAHLIA DR/NW LEADBETTER PKWY	SE	ST20
NW LEADBETTER PKWY/NW DAHLIA DR	NW DAHLIA DR/NW LEADBETTER PKWY	SW	ST20
NW LEADBETTER PKWY/NW DAHLIA DR	NW DAHLIA DR/NW LEADBETTER PKWY	NW	ST20
NW LEADBETTER PKWY/NW DAHLIA DR	NW DAHLIA DR/NW LEADBETTER PKWY	NE	ST20
NW DAHLIA LP	NW DAHLIA DR	SW	ST20
NW DAHLIA LP	NW DAHLIA DR	NW	ST20
NW DAHLIA DR	NW 34TH CIR	NE	ST21

Camas ADA Transition Plan - Comprehensive Plan Cost Estimate

Intersection / Corridor Name: Not Applicable  
 Area within Camas: Grass Valley Park

Item No.	Approx. Qty.	Unit	Description	Unit Price	Total
<b>Street Construction</b>					
1		LS	Mobilization (10% of Street Construction Total)		\$6,580
2	1	EACH	Erosion Control per ramp	\$200	\$1,200
3	1	EACH	Temporary Traffic Control & Signing per ramp	\$500	\$3,000
4	1	EACH	Removal of Structures & Obstructions per ramp	\$300	\$1,800
5	4	EACH	ADA Corner - ST21 / ST24 (Single Parallel / Mid-Block)	\$8,100	\$32,400
6	2	EACH	ADA Corner - ST20 (Single Perpendicular)	\$8,700	\$17,400
7	0	EACH	ADA Corner - ST21A (Dual Parallel)	\$14,700	\$0
8	0	EACH	ADA Corner - ST20A (Dual Perpendicular)	\$18,200	\$0
9	0	EACH	Low Cost - Grind lip	\$500	\$0
10	0	EACH	Low Cost - Detectable Warning Surface (DWS)	\$500	\$0
11	0	EACH	Low Cost - Grind lip & DWS	\$1,000	\$0
12		EACH	Minor Changes (20% of Ramp Total Cost)		\$9,960

Street Subtotal	\$65,760
Sales Tax (0%)	\$0
Street Construction Total without Mobilization	\$65,800
<b>Street Construction Total</b>	<b>\$65,800</b>

Item No.	Approx. Qty.	Unit	Description	Unit Price	Total
<b>Miscellaneous</b>					
10	6	LS	Materials Testing	\$125	\$750
11	1	LS	Construction Design (15%) of Street Construction Cost		\$9,870
12	1	LS	Engineering Design (15%) of Ramp Total Cost		\$7,470

Miscellaneous Subtotal	\$18,090
Sales Tax (0%)	\$0
<b>Miscellaneous Total</b>	<b>\$18,100</b>

TOTAL PROJECT COST WITHOUT MOBILIZATION	\$83,900
<b>TOTAL PROJECT COST</b>	<b>\$84,000</b>

**List of exclusions:**

Utility pole relocations  
 Right-of-way  
 Traffic signals to be determined

**Assumptions:**

Catch basin adjustments: \$1,500 per every 4 ramps  
 Low cost improvements assume revising the existing ramp by adding either DWS, grinding lip flush or both  
 \* Near school

**List of typical obstructions:**

Sign poles  
 Mailboxes

**Grass Valley Park Ramps**

Street Name	Cross_Street ADDRESS #3340	Corner	ST#
NW JULIA ST		NW	ST24
NW EMILY WAY	NW 44TH AVE	NE	ST20
NW EMILY WAY	NW 44TH AVE	NW	ST20
NW HUNSAKER CT	HOUSE #3336	E	ST21
NW HUNSAKER CT	HOUSE #3336	W	ST21
NW PACIFIC RIM DR	NW IRON HORSE CT	E	ST21

Camas ADA Transition Plan - Transportation Improvement Plan Cost Estimate

Intersection / Corridor Name: Not Applicable  
 Area within Camas: Crown Park

Item No.	Approx. Qty.	Unit	Description	Unit Price	Total
<b>Street Construction</b>					
1		LS	Mobilization (10% of Street Construction Total)		\$13,060
2	1	EACH	Erosion Control per ramp	\$200	\$2,400
3	1	EACH	Temporary Traffic Control & Signing per ramp	\$500	\$6,000
4	1	EACH	Removal of Structures & Obstructions per ramp	\$300	\$3,600
5	6	EACH	ADA Corner - ST21 / ST24 (Single Parallel / Mid-Block)	\$8,100	\$48,600
6	6	EACH	ADA Corner - ST20 (Single Perpendicular)	\$8,700	\$52,200
7	0	EACH	ADA Corner - ST21A (Dual Parallel)	\$14,700	\$0
8	0	EACH	ADA Corner - ST20A (Dual Perpendicular)	\$18,200	\$0
9	0	EACH	Low Cost - Grind lip	\$500	\$0
10	0	EACH	Low Cost - Detectable Warning Surface (DWS)	\$500	\$0
11	0	EACH	Low Cost - Grind lip & DWS	\$1,000	\$0
12		EACH	Minor Changes (20% of Ramp Total Cost)		\$20,160

Street Subtotal	\$130,560
Sales Tax (0%)	\$0
Street Construction Total without Mobilization	\$130,600
<b>Street Construction Total</b>	<b>\$133,000</b>

Item No.	Approx. Qty.	Unit	Description	Unit Price	Total
<b>Miscellaneous</b>					
10	12	LS	Materials Testing	\$125	\$1,500
11	1	LS	Construction Design (15%) of Street Construction Cost		\$19,590
12	1	LS	Engineering Design (15%) of Ramp Total Cost		\$15,120

Miscellaneous Subtotal	\$36,210
Sales Tax (0%)	\$0
<b>Miscellaneous Total</b>	<b>\$36,300</b>

TOTAL PROJECT COST WITHOUT MOBILIZATION \$166,900

**TOTAL PROJECT COST \$170,000**

**List of exclusions:**

- Utility pole relocations
- Right-of-way
- Traffic signals to be determined

**Assumptions:**

- Catch basin adjustments: \$1,500 per every 4 ramps
- Low cost improvements assume revising the existing ramp by adding either DWS, grinding lip flush or both
- \* Near school

**List of typical obstructions:**

- Sign poles
- Mailboxes

**Crown Park Ramps**

Street Name	Cross Street	Corner	ST#
NE 13TH AVE/NE ADAMS ST	NE ADAMS ST/NE 13TH AVE	NW	ST20
NE 14TH AVE	NE DALLAS ST	NW	ST20
NE 14TH AVE	NE DALLAS ST	NE	ST20
NE 14TH AVE	NE DALLAS ST	SW	ST20
NE 14TH AVE	NE DALLAS ST	SE	ST20
NE 14TH AVE	DIVISION ST	SW	ST20
NW 14TH AVE	DIVISION ST	NE	ST21
NW 14TH AVE	DIVISION ST	NW	ST21
NW 14TH AVE	DIVISION ST	SE	ST21
NE 21ST AVE	NE BIRCH ST	NW	ST21
NE BIRCH ST	NE 19TH AVE	SW	ST21
NW BENTON ST	NW 18TH AVE	E	ST21

Camas ADA Transition Plan - Comprehensive Plan Cost Estimate

Intersection / Corridor Name: Not Applicable  
 Area within Camas: Crown Park

Item No.	Approx. Qty.	Unit	Description	Unit Price	Total
<b>Street Construction</b>					
1		LS	Mobilization (10% of Street Construction Total)		\$10,670
2	1	EACH	Erosion Control per ramp	\$200	\$2,000
3	1	EACH	Temporary Traffic Control & Signing per ramp	\$500	\$5,000
4	1	EACH	Removal of Structures & Obstructions per ramp	\$300	\$3,000
5	8	EACH	ADA Corner - ST21 / ST24 (Single Parallel / Mid-Block)	\$8,100	\$64,800
6	2	EACH	ADA Corner - ST20 (Single Perpendicular)	\$8,700	\$17,400
7	0	EACH	ADA Corner - ST21A (Dual Parallel)	\$14,700	\$0
8	0	EACH	ADA Corner - ST20A (Dual Perpendicular)	\$18,200	\$0
9	0	EACH	Low Cost - Grind lip	\$500	\$0
10	0	EACH	Low Cost - Detectable Warning Surface (DWS)	\$500	\$0
11	0	EACH	Low Cost - Grind lip & DWS	\$1,000	\$0
12		EACH	Minor Changes (20% of Ramp Total Cost)		\$16,440

Street Subtotal	\$106,640
Sales Tax (0%)	\$0
Street Construction Total without Mobilization	\$106,700
<b>Street Construction Total</b>	<b>\$108,700</b>

Item No.	Approx. Qty.	Unit	Description	Unit Price	Total
<b>Miscellaneous</b>					
10	10	LS	Materials Testing	\$125	\$1,250
11	1	LS	Construction Design (15%) of Street Construction Cost		\$16,005
12	1	LS	Engineering Design (15%) of Ramp Total Cost		\$12,330

Miscellaneous Subtotal	\$29,585
Sales Tax (0%)	\$0
<b>Miscellaneous Total</b>	<b>\$29,600</b>

TOTAL PROJECT COST WITHOUT MOBILIZATION	\$136,300
<b>TOTAL PROJECT COST</b>	<b>\$139,000</b>

**List of exclusions:**

Utility pole relocations  
 Right-of-way  
 Traffic signals to be determined

**Assumptions:**

Catch basin adjustments: \$1,500 per every 4 ramps  
 Low cost improvements assume revising the existing ramp by adding either DWS, grinding lip flush or both  
 \* Near school

**List of typical obstructions:**

Sign poles  
 Mailboxes

**Crown Park Ramps**

Street Name	Cross Street	Corner	ST#
NW COUCH ST	NW 14TH AVE	NE	ST21
NW COUCH ST	NW 14TH AVE	SE	ST21
NW COUCH ST	NW 14TH AVE	SW	ST21
NE 13TH AVE/NE ADAMS ST	NE ADAMS ST/NE 13TH AVE	NW	ST20
NW 13TH AVE	DIVISION ST	NW	ST21
NW 13TH AVE	DIVISION ST	NW	ST20
NW 14TH AVE	DIVISION ST	NE	ST21
NE 21ST AVE	NE BIRCH ST	NW	ST21
NE BIRCH ST	NE 19TH AVE	SW	ST21
NW BENTON ST	NW 18TH AVE	E	ST21

Camas ADA Transition Plan - Transportation Improvement Plan Cost Estimate

Intersection / Corridor Name: Not Applicable  
 Area within Camas: Forest Home Park

Item No.	Approx. Qty.	Unit	Description	Unit Price	Total
<b>Street Construction</b>					
1		LS	Mobilization (10% of Street Construction Total)		\$7,370
2	1	EACH	Erosion Control per ramp	\$200	\$1,400
3	1	EACH	Temporary Traffic Control & Signing per ramp	\$500	\$3,500
4	1	EACH	Removal of Structures & Obstructions per ramp	\$300	\$2,100
5	7	EACH	ADA Corner - ST21 / ST24 (Single Parallel / Mid-Block)	\$8,100	\$56,700
6	0	EACH	ADA Corner - ST20 (Single Perpendicular)	\$8,700	\$0
7	0	EACH	ADA Corner - ST21A (Dual Parallel)	\$14,700	\$0
8	0	EACH	ADA Corner - ST20A (Dual Perpendicular)	\$18,200	\$0
9	0	EACH	Low Cost - Grind lip	\$500	\$0
10	0	EACH	Low Cost - Detactable Warning Surface (DWS)	\$500	\$0
11	0	EACH	Low Cost - Grind lip & DWS	\$1,000	\$0
12		EACH	Minor Changes (20% of Ramp Total Cost)		\$11,340

Street Subtotal	\$73,640
Sales Tax (0%)	\$0
Street Construction Total without Mobilization	\$73,700
<b>Street Construction Total</b>	<b>\$75,100</b>

Item No.	Approx. Qty.	Unit	Description	Unit Price	Total
<b>Miscellaneous</b>					
10	7	LS	Materials Testing	\$125	\$875
11	1	LS	Construction Design (15%) of Street Construction Cost		\$11,055
12	1	LS	Engineering Design (15%) of Ramp Total Cost		\$8,505

Miscellaneous Subtotal	\$20,435
Sales Tax (0%)	\$0
<b>Miscellaneous Total</b>	<b>\$20,500</b>

TOTAL PROJECT COST WITHOUT MOBILIZATION \$94,200  
**TOTAL PROJECT COST \$96,000**

**List of exclusions:**

Utility pole relocations  
 Right-of-way  
 Traffic signals to be determined

**Assumptions:**

Catch basin adjustments: \$1,500 per every 4 ramps  
 Low cost improvements assume revising the existing ramp by adding either DWS, grinding lip flush or both  
 \* Near school

**List of typical obstructions:**

Sign poles  
 Mailboxes

**Forest Home Park Ramps**

Street Name	Cross Street	Corner	ST#
NW LOGAN ST	FOREST HOME PARK (EAST PARKING LOT)	S	ST21
NW 7TH AVE	/ MEADOW RIDGE LN (PVT) (EAST INTERSECTI	NE	ST21
NW 7TH AVE	NW IVY ST (WEST INTERSECTION)	SE	ST21
NW 7TH AVE	NW IVY ST / NW LOGAN ST	NW	ST21
NW IVY ST	NW 6TH AVE	NW	ST21
NW IVY ST	FOREST HOME PARK PARKING LOT	W	ST21
NW IVY DR	NW 6TH AVE	SE	ST21

Camas ADA Transition Plan - Comprehensive Plan Cost Estimate

Intersection / Corridor Name: Not Applicable  
 Area within Camas: Forest Home Park

Item No.	Approx. Qty.	Unit	Description	Unit Price	Total
<b>Street Construction</b>					
1		LS	Mobilization (10% of Street Construction Total)		\$2,110
2	1	EACH	Erosion Control per ramp	\$200	\$400
3	1	EACH	Temporary Traffic Control & Signing per ramp	\$500	\$1,000
4	1	EACH	Removal of Structures & Obstructions per ramp	\$300	\$600
5	2	EACH	ADA Corner - ST21 / ST24 (Single Parallel / Mid-Block)	\$8,100	\$16,200
6	0	EACH	ADA Corner - ST20 (Single Perpendicular)	\$8,700	\$0
7	0	EACH	ADA Corner - ST21A (Dual Parallel)	\$14,700	\$0
8	0	EACH	ADA Corner - ST20A (Dual Perpendicular)	\$18,200	\$0
9	0	EACH	Low Cost - Grind lip	\$500	\$0
10	0	EACH	Low Cost - Detactable Warning Surface (DWS)	\$500	\$0
11	0	EACH	Low Cost - Grind lip & DWS	\$1,000	\$0
12		EACH	Minor Changes (20% of Ramp Total Cost)		\$3,240

Street Subtotal	\$21,040
Sales Tax (0%)	\$0
Street Construction Total without Mobilization	\$21,100
<b>Street Construction Total</b>	<b>\$21,500</b>

Item No.	Approx. Qty.	Unit	Description	Unit Price	Total
<b>Miscellaneous</b>					
10	2	LS	Materials Testing	\$125	\$250
11	1	LS	Construction Design (15%) of Street Construction Cost		\$3,165
12	1	LS	Engineering Design (15%) of Ramp Total Cost		\$2,430

Miscellaneous Subtotal	\$5,845
Sales Tax (0%)	\$0
<b>Miscellaneous Total</b>	<b>\$5,900</b>

TOTAL PROJECT COST WITHOUT MOBILIZATION \$27,000  
**TOTAL PROJECT COST \$28,000**

**List of exclusions:**

Utility pole relocations  
 Right-of-way  
 Traffic signals to be determined

**Assumptions:**

Catch basin adjustments: \$1,500 per every 4 ramps  
 Low cost improvements assume revising the existing ramp by adding either DWS, grinding lip flush or both  
 \* Near school

**List of typical obstructions:**

Sign poles  
 Mailboxes

**Forest Home Park Ramps**

Street Name	Cross Street	Corner	ST#
NW 8TH AVE	CUL DE SAC / ADDRESS #1039	NE	ST21
NW LOGAN ST	NW 8TH AVE	NW	ST21

Camas ADA Transition Plan - Comprehensive Plan Cost Estimate

Intersection / Corridor Name: Not Applicable  
 Area within Camas: Dorothy Fox Park

Item No.	Approx. Qty.	Unit	Description	Unit Price	Total
<b>Street Construction</b>					
1		LS	Mobilization (10% of Street Construction Total)		\$29,460
2	1	EACH	Erosion Control per ramp	\$200	\$5,600
3	1	EACH	Temporary Traffic Control & Signing per ramp	\$500	\$14,000
4	1	EACH	Removal of Structures & Obstructions per ramp	\$300	\$8,400
5	28	EACH	ADA Corner - ST21 / ST24 (Single Parallel / Mid-Block)	\$8,100	\$226,800
6	0	EACH	ADA Corner - ST20 (Single Perpendicular)	\$8,700	\$0
7	0	EACH	ADA Corner - ST21A (Dual Parallel)	\$14,700	\$0
8	0	EACH	ADA Corner - ST20A (Dual Perpendicular)	\$18,200	\$0
9	0	EACH	Low Cost - Grind lip	\$500	\$0
10	0	EACH	Low Cost - Detachable Warning Surface (DWS)	\$500	\$0
11		EACH	Low Cost - Grind lip & DWS	\$1,000	\$0
12		EACH	Minor Changes (20% of Ramp Total Cost)		\$45,360

Street Subtotal	\$294,560
Sales Tax (0%)	\$0
Street Construction Total without Mobilization	\$294,600
<b>Street Construction Total</b>	<b>\$300,200</b>

Item No.	Approx. Qty.	Unit	Description	Unit Price	Total
<b>Miscellaneous</b>					
10	28	LS	Materials Testing	\$125	\$3,500
11	1	LS	Construction Design (15%) of Street Construction Cost		\$44,190
12	1	LS	Engineering Design (15%) of Ramp Total Cost		\$34,020

Miscellaneous Subtotal	\$81,710
Sales Tax (0%)	\$0
<b>Miscellaneous Total</b>	<b>\$81,800</b>

TOTAL PROJECT COST WITHOUT MOBILIZATION \$376,400

**TOTAL PROJECT COST \$382,000**

**List of exclusions:**

- Utility pole relocations
- Right-of-way
- Traffic signals to be determined

**Assumptions:**

- Catch basin adjustments: \$1,500 per every 4 ramps
- \* Near school

**List of typical obstructions:**

- Sign poles
- Mailboxes

**Dorothy Fox Park Ramps**

Street Name	Cross Street	Corner	ST#
NW ASTOR ST	NW 25TH AVE	SW	ST21
NW ASTOR ST	NW 25TH AVE	NW	ST21
NW ASTOR ST	NW 22ND AVE	SE	ST21
NW ASTOR ST	NW 22ND AVE	NE	ST21
NW SIERRA ST	PARK CROSSWALK	E	ST21
NW 22ND AVE	NW 22ND CIR	NE	ST21
NW 22ND AVE	NW 22ND CIR	NW	ST21
NW 22ND AVE	NW WILLOW DR	SW	ST21
NW 22ND AVE	NW WILLOW DR	SE	ST21
NW 22ND CIR	END OF STREET / ADDRESS #2314	SW	ST21
NW 25TH AVE	NW NORWOOD PL	SE	ST21
NW 26TH CIR	NW NORWOOD PL	SW	ST21
NW 26TH AVE	NW QUARTZ ST	SW	ST21
NW 26TH AVE	NW QUARTZ ST	NW	ST21
NW UTAH ST	NW VALLEY ST	NE	ST21
NW 29TH CIR (PVT)	NW UTAH ST	SE	ST21
NW 30TH CIR (PVT)	NW UTAH ST	SE	ST21
NW 30TH CIR (PVT)	NW UTAH ST	NE	ST21
NW 31ST CIR (PVT)	NW VALLEY ST	SE	ST21
NW 31ST WAY (PVT)	NW VALLEY ST	SE	ST21
NW 31ST WAY (PVT)	NW VALLEY ST	NE	ST21
NW 32ND CIR (PVT)	NW VALLEY ST	SE	ST21
NW 32ND CIR (PVT)	NW VALLEY ST	NE	ST21
NW 32ND CIR (PVT)	NW VALLEY ST	SW	ST21
NW 31ST WAY (PVT)	NW VALLEY ST	SW	ST21
NW 31ST WAY (PVT)	NW VALLEY ST	NW	ST21
NW VANGUARD PL (PVT)	NW 29TH AVE	NE	ST21
NW WATKINS CT	NW 29TH AVE	NE	ST21

Camas ADA Transition Plan - Transportation Improvement Plan Cost Estimate

Intersection / Corridor Name: Not Applicable  
 Area within Camas: Dorothy Fox Park

Item No.	Approx. Qty.	Unit	Description	Unit Price	Total
<b>Street Construction</b>					
1		LS	Mobilization (10% of Street Construction Total)		\$12,770
2	1	EACH	Erosion Control per ramp	\$200	\$2,400
3	1	EACH	Temporary Traffic Control & Signing per ramp	\$500	\$6,000
4	1	EACH	Removal of Structures & Obstructions per ramp	\$300	\$3,600
5	10	EACH	ADA Corner - ST21 / ST24 (Single Parallel / Mid-Block)	\$8,100	\$81,000
6	2	EACH	ADA Corner - ST20 (Single Perpendicular)	\$8,700	\$17,400
7	0	EACH	ADA Corner - ST21A (Dual Parallel)	\$14,700	\$0
8	0	EACH	ADA Corner - ST20A (Dual Perpendicular)	\$18,200	\$0
9	0	EACH	Low Cost - Grind lip	\$500	\$0
10	0	EACH	Low Cost - Detachable Warning Surface (DWS)	\$500	\$0
11		EACH	Low Cost - Grind lip & DWS	\$1,000	\$0
12		EACH	Minor Changes (20% of Ramp Total Cost)		\$19,680

Street Subtotal	\$127,680
Sales Tax (0%)	\$0
Street Construction Total without Mobilization	\$127,700
<b>Street Construction Total</b>	<b>\$130,100</b>

Item No.	Approx. Qty.	Unit	Description	Unit Price	Total
<b>Miscellaneous</b>					
10	12	LS	Materials Testing	\$125	\$1,500
11	1	LS	Construction Design (15%) of Street Construction Cost		\$19,155
12	1	LS	Engineering Design (15%) of Ramp Total Cost		\$14,760

Miscellaneous Subtotal	\$35,415
Sales Tax (0%)	\$0
<b>Miscellaneous Total</b>	<b>\$35,500</b>

TOTAL PROJECT COST WITHOUT MOBILIZATION \$163,200

**TOTAL PROJECT COST \$166,000**

**List of exclusions:**

Utility pole relocations  
 Right-of-way  
 Traffic signals to be determined

**Assumptions:**

Catch basin adjustments: \$1,500 per every 4 ramps  
 \* Near school

**List of typical obstructions:**

Sign poles  
 Mailboxes

**Dorothy Fox Park Ramps**

Street_Name	Cross_Street	Corner	ST#
NW VALLEY ST	NW 28TH AVE	SW	ST20
NW VALLEY ST	NW 28TH AVE	SE	ST20
NW VALLEY ST	NW 27TH CIR	SW	ST21
NW VALLEY ST	NW 27TH CIR	NW	ST21
NW 26TH AVE	NW SUNSET CT	NW	ST21
NW 26TH AVE	NW SUNSET CT	NE	ST21
NW SIERRA ST*	DOROTHY FOX ELEMENTARY (EXIT ONLY)	NW	ST21
NW SIERRA ST	PARK CROSSWALK	W	ST21
NW 27TH AVE (DOROTHY FOX ELEMENTARY)*	NW SIERRA ST	SW	ST21
NW 27TH AVE (DOROTHY FOX ELEMENTARY)*	NW SIERRA ST	NW	ST21
DOROTHY FOX ELEMENTARY (STAFF PARKING)*	NW SIERRA ST	SW	ST21
DOROTHY FOX ELEMENTARY (STAFF PARKING)*	NW SIERRA ST	NW	ST21



## Memorandum

DATE: December 23, 2025  
TO: Curleigh (Jim) Carothers  
FROM: Apex Companies, LLC  
PROJECT: 24011088—Camas ADA Plan Update  
REGARDING: Camas 2025 ADA Transition Plan Update: Status of 2015 ADA Transition Plan Project List

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### **CAMAS 2015 ADA TRANSITION PLAN CURB RAMP INVENTORY**

A major component of the 2025 Camas ADA Self-Evaluation Plan update is the geographic information system (GIS) based inventory of all curb ramps within Camas, Washington. Apex Companies, LLC (Apex) developed a replacement priority-ranking methodology for this data based on existing ramp width, detectable warning surface type, gutter-sidewalk lip and landing conditions. Using this methodology, all the curb ramp data supplied by the City of Camas (Camas) were categorized into three replacement priority levels: high, medium, and low. As part of the 2025 ADA Transition Plan (2025 Plan) Update, the original 2015 ADA Transition Plan (2015 Plan) was reviewed to verify if high and medium important locations had been addressed with ADA improvement upgrades. See Figures 1-4 from the 2015 Plan for High & Medium Locations. The current ramp inventory was compared with the 2015 projects to develop a list of projects for the 2025 update. It also mapped the work completed between when the 2015 Plan was completed and when the curb ramps and traffic signals were inventoried for the 2025 Plan. See Figure 5 and 6 – Status of 2015 ADA Transition Plan High and Medium Important Locations.

The 2015 Plan included an evaluation of roadway near destinations and services that may be utilized by members of the disabled community. For example, intersections along C-Tran bus service (route 92) run along E 3rd Avenue and NW 6th Avenue ranked important. All locations in the 2015 Plan that are ranked important in the 2025 Plan, such as in the downtown, parks and traffic signal were excluded in the evaluation. Only locations ranked important in the 2015 Plan and with curb ramps rated high-medium priority for ramp replacement were included. These project lists represent planning-level estimates for the 20-year City comprehensive plan. See the attached cost estimate for more details.

Three of the highlighted intersections below include cost estimates for pedestrian push button upgrades, which are incorporated into the total project cost. These signalized intersections are identified within the 2015 Incomplete Projects list; therefore, the push button upgrade costs have been included in this memo.

### **CURB RAMP REPLACEMENT PROJECT LIST**

The following intersections from the 2015 ADA Transition Plan recommended for ramp replacement:

- NW 6th Avenue / NW Ash Street
- NW 6th Avenue / Division Street
- NE 3rd Avenue / N Shepperd Road
- NE 3rd Avenue / NE 2nd Avenue / 4th Avenue
- NE 3rd Avenue / NE Weir Street
- NW 28th Avenue / NW Logan Street
- NE Fiberg-Strunk Street / SE 1st Street

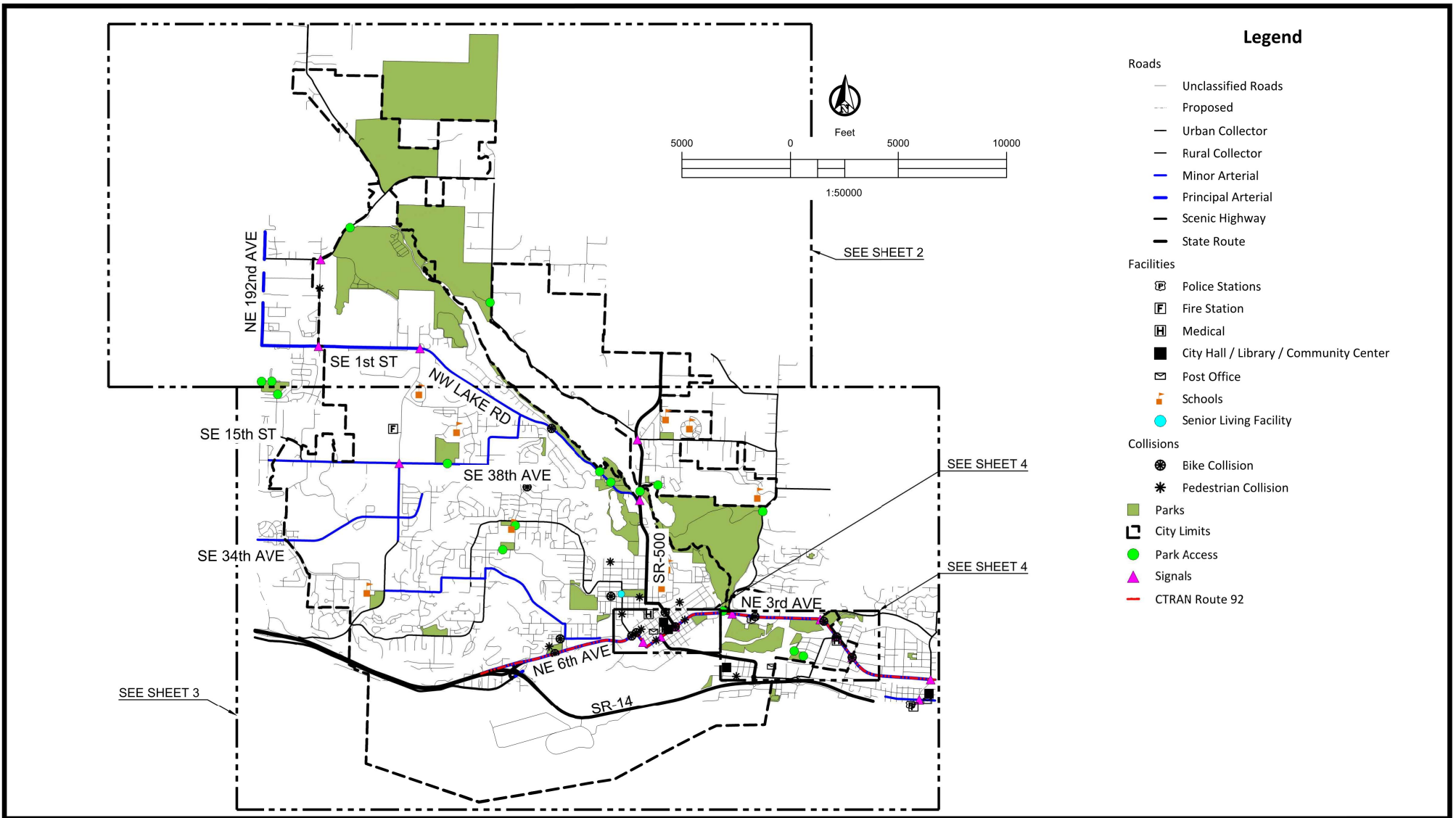
City of Camas  
Camas ADA Plan: Status of 2015 ADA Transition Plan Project List  
December 19, 2025  
Page 2

The total cost of the ramp replacements and push button upgrades is approximately \$298,000. See attached cost estimate for more details.

Attachments: Figures 1-4 from the 2015 Plan for High & Medium Locations.  
Figures 5-6 - 2015 ADA Transition Plan High and Medium Important Locations  
Cost Estimates - Incomplete 2015 ADA Transition Plan Projects

cc: Chris Lopez (City of Camas)

MT:JAM

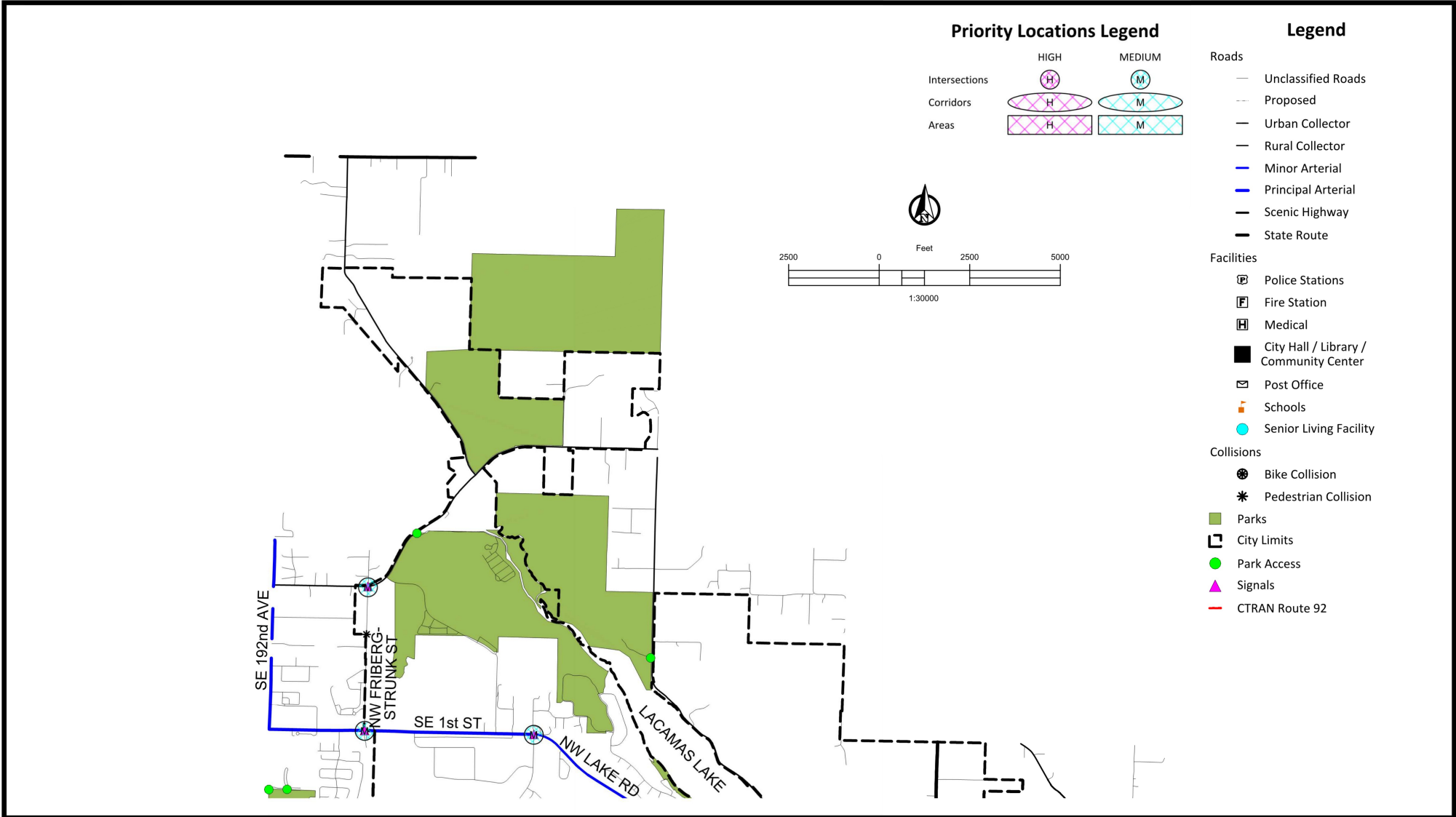


OVERALL MAP FOR:  
**CAMAS CITY-WIDE SELF EVALUTATION & ADA TRANSITION PLAN**  
 HIGH & MEDIUM PRIORITY LOCATIONS

**HDJ**  
 DESIGN GROUP  
 engineers | landscape architects | planners | surveyors

314 W 15th Street  
 Vancouver, WA 98660-2927  
 360/695-3488  
 503/924-4005  
 360/695-8767 fax  
 Internet: www.hdjg.com

DESIGNED: N/A	SCALE: H: N/A V: N/A	SHEET
DRAWN: MPA	DATE: DEC 2014	1
CHECKED: JAM	JOB NO.: 3804	4

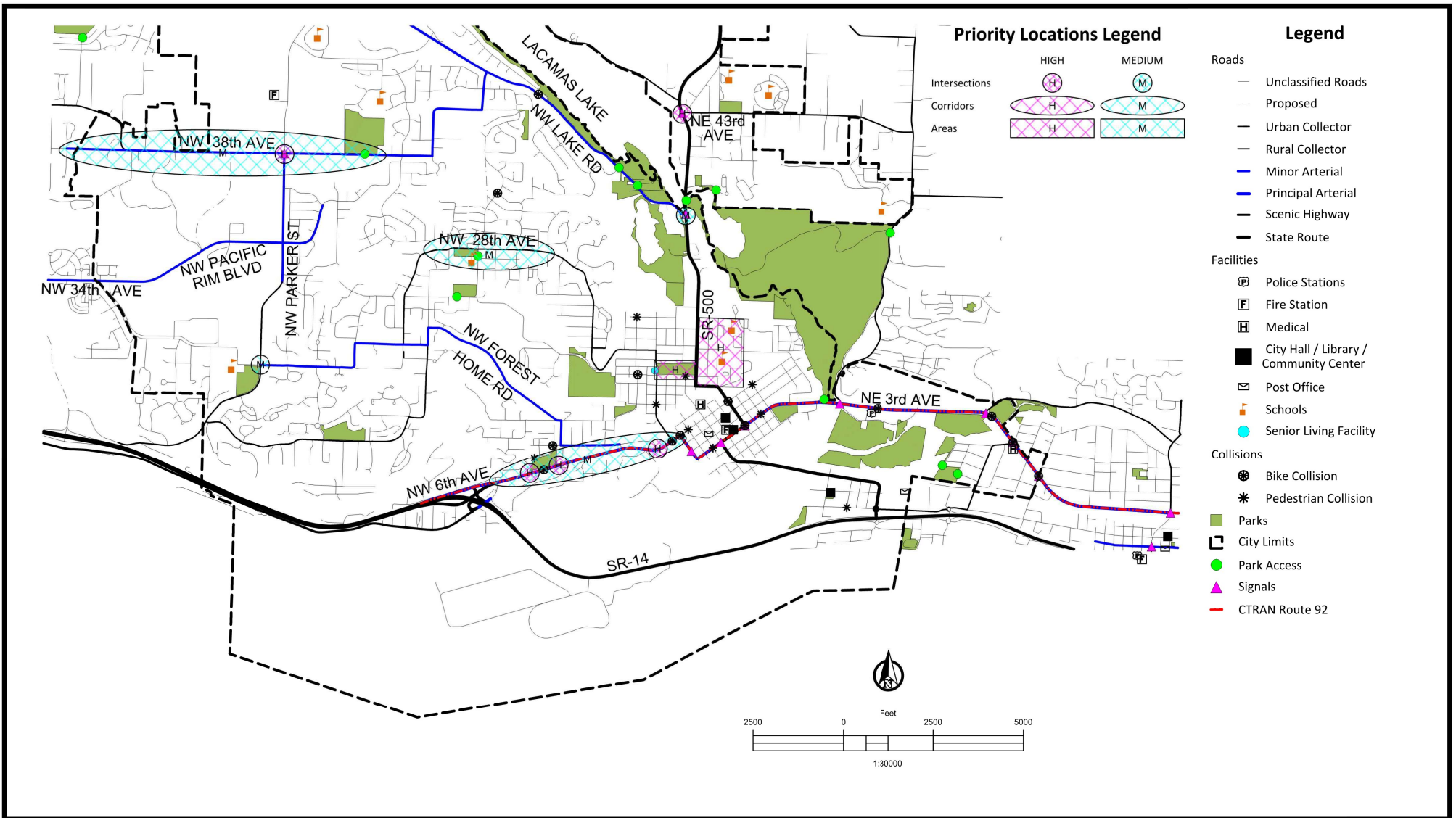


**NORTHEAST MAP FOR:  
CAMAS CITY-WIDE SELF EVALUTATION & ADA TRANSITION PLAN  
HIGH & MEDIUM PRIORITY LOCATIONS**

**HDJ**  
DESIGN GROUP  
engineers | landscape architects | planners | surveyors

314 W 15th Street  
Vancouver, WA 98660-2927  
360/695-3488  
503/924-4005  
360/695-8767 fax  
Internet: www.hdjg.com

DESIGNED: N/A	SCALE: H: N/A V: N/A	SHEET
DRAWN: MPA	DATE: DEC 2014	2
CHECKED: JAM	JOB NO.: 3804	4

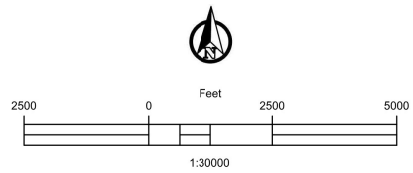


**Priority Locations Legend**

- Intersections: HIGH (H in circle), MEDIUM (M in circle)
- Corridors: HIGH (H in oval), MEDIUM (M in oval)
- Areas: HIGH (H in square), MEDIUM (M in square)

**Legend**

- Roads**
- Unclassified Roads
  - Proposed
  - Urban Collector
  - Rural Collector
  - Minor Arterial
  - Principal Arterial
  - Scenic Highway
  - State Route
- Facilities**
- Police Stations
  - Fire Station
  - Medical
  - City Hall / Library / Community Center
  - Post Office
  - Schools
  - Senior Living Facility
- Collisions**
- Bike Collision
  - Pedestrian Collision
- Other**
- Parks
  - City Limits
  - Park Access
  - Signals
  - CTRAN Route 92

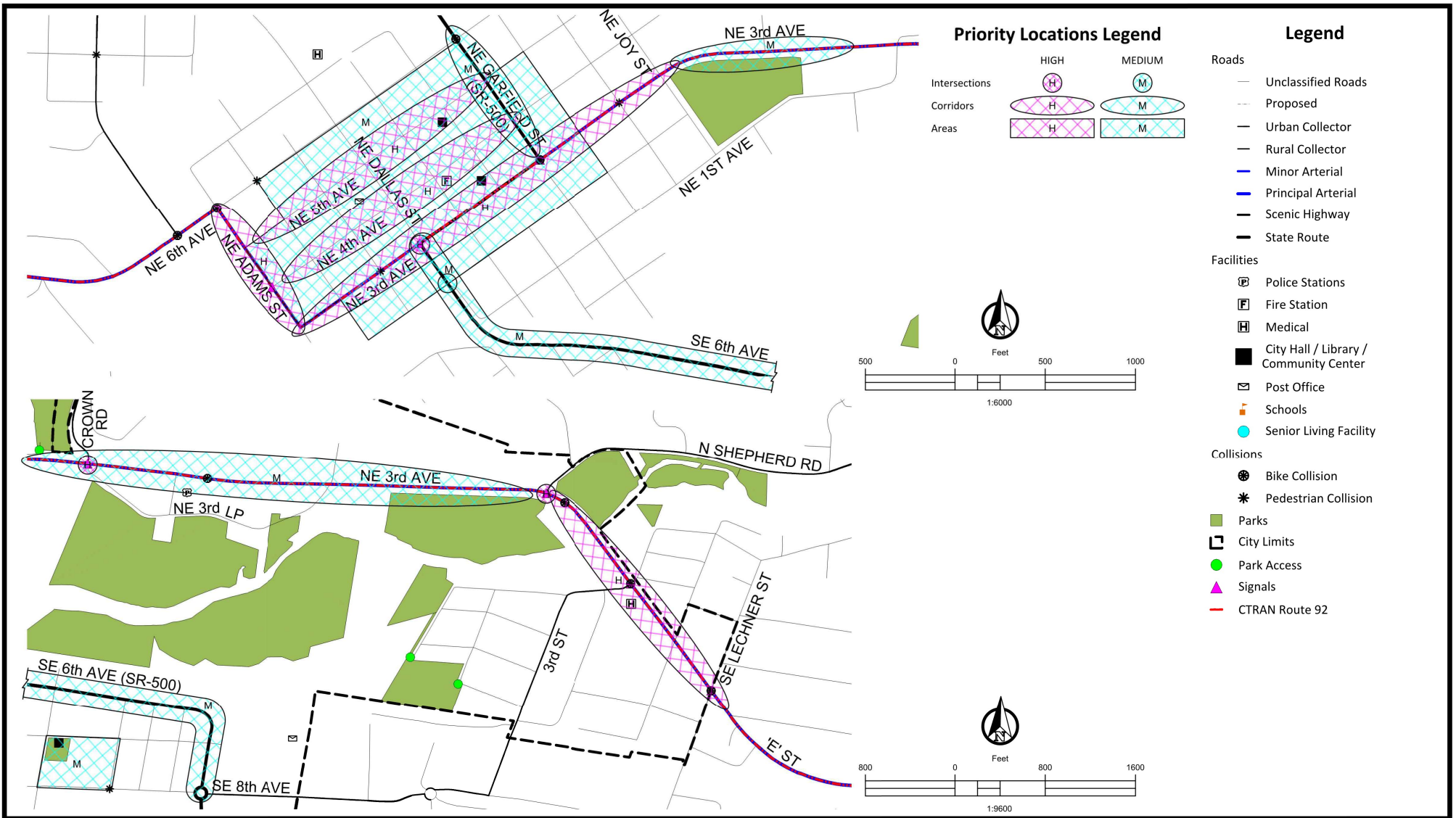


SOUTHEAST MAP FOR:  
**CAMAS CITY-WIDE SELF EVALUTATION & ADA TRANSITION PLAN**  
 HIGH & MEDIUM PRIORITY LOCATIONS

**HDJ**  
 DESIGN GROUP  
 engineers | landscape architects | planners | surveyors

314 W 15th Street  
 Vancouver, WA 98660-2927  
 360/695-3488  
 503/924-4005  
 360/695-8767 fax  
 Internet: www.hdjdg.com

DESIGNED: N/A	SCALE: H: N/A V: N/A	SHEET
DRAWN: MPA	DATE: DEC 2014	3
CHECKED: JAM	JOB NO.: 3804	4



**DOWNTOWN MAP & NE 3rd AVE CORRIDOR FOR:  
CAMAS CITY-WIDE SELF EVALUTATION & ADA TRANSITION PLAN  
HIGH & MEDIUM PRIORITY LOCATIONS**

**HDJ**  
DESIGN GROUP  
engineers | landscape architects | planners | surveyors

314 W 15th Street  
Vancouver, WA 98660-2927  
360/695-3488  
503/924-4005  
360/695-8767 fax  
Internet: www.hdjdg.com

DESIGNED: N/A	SCALE: H: N/A V: N/A	SHEET
DRAWN: MPA	DATE: DEC 2014	4
CHECKED: JAM	JOB NO.: 3804	4

### Figure 5 Status of 2015 ADA Transition Plan High and Medium Important Locations

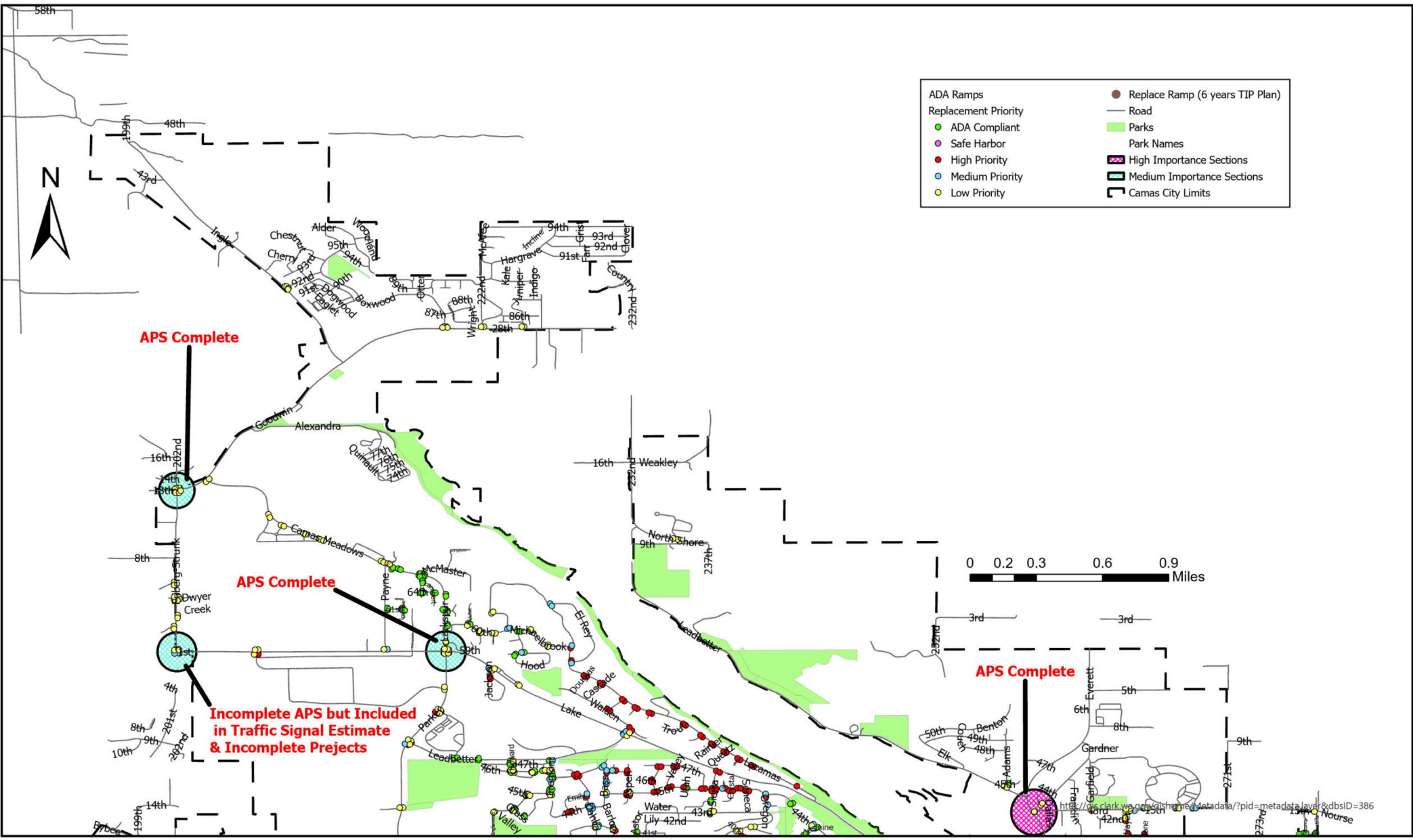
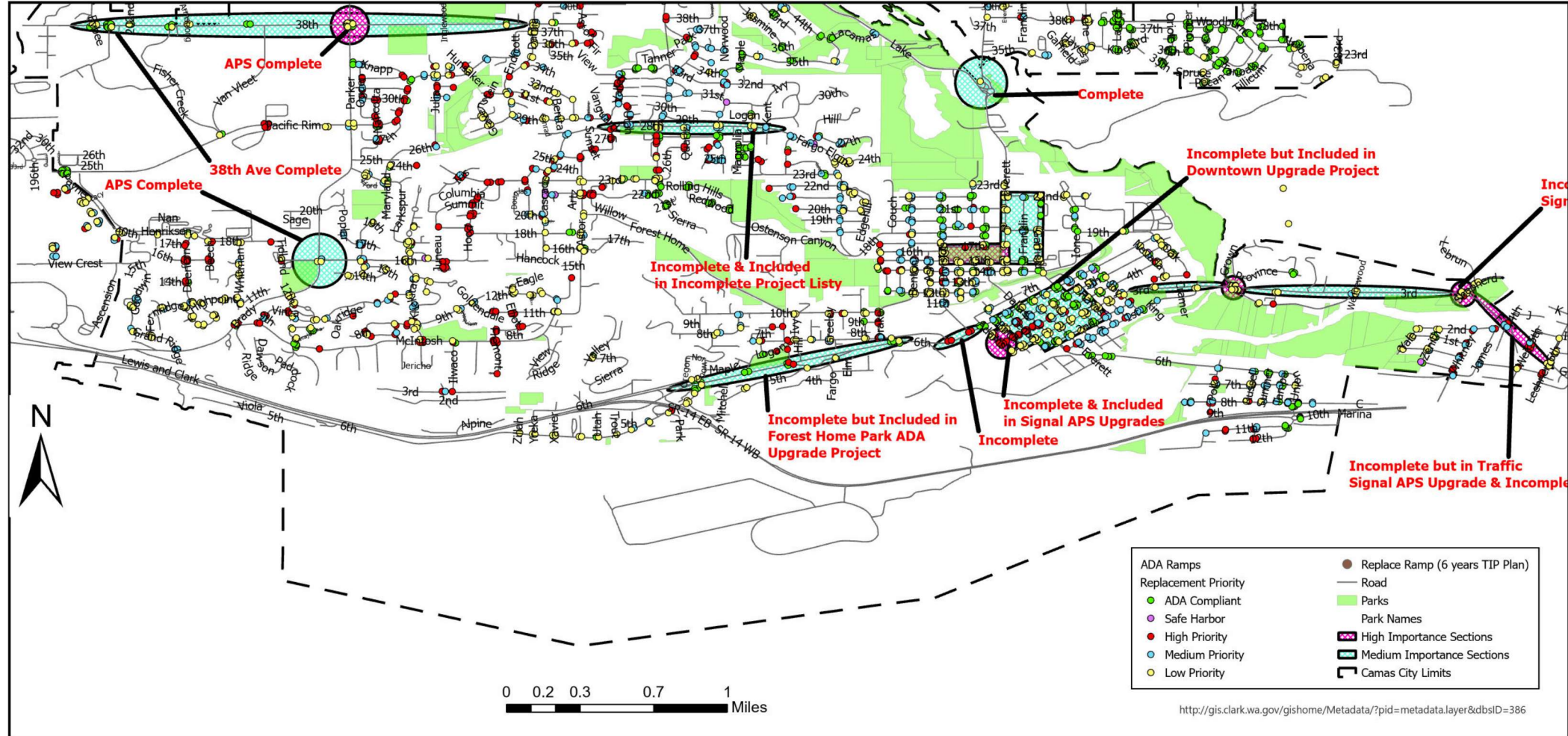


Figure 6 Status of 2015 ADA Transition Plan High and Medium Important Locations



Camas ADA Transition Plan - Comprehensive Plan Cost Estimate

Intersection / Corridor Name: Incomplete 2015 ADA Plan Projects  
 Area within Camas: NA

Item No.	Approx. Qty.	Unit	Description	Unit Price	Total
<b>Street Construction</b>					
1		LS	Mobilization (10% of Street Construction Total)		\$18,370
2	1	EACH	Erosion Control per ramp	\$200	\$3,400
3	1	EACH	Temporary Traffic Control & Signing per ramp	\$500	\$8,500
4	1	EACH	Removal of Structures & Obstructions per ramp	\$300	\$5,100
5	15	EACH	ADA Corner - ST21 / ST24 (Single Parallel / Mid-Block)	\$8,100	\$121,500
6	2	EACH	ADA Corner - ST20 (Single Perpendicular)	\$8,700	\$17,400
7	0	EACH	ADA Corner - ST21A (Dual Parallel)	\$14,700	\$0
8	0	EACH	ADA Corner - ST20A (Dual Perpendicular)	\$18,200	\$0
9	0	EACH	Low cost - Grind lip	\$300	\$0
10	0	EACH	Low Cost - Detectable Warning Surface (DWS)	\$500	\$0
11	0	EACH	Low Cost - Grind lip & DWS	\$800	\$0
12		EACH	Minor Changes (20% of Ramp Total Cost)		\$27,780

Street Subtotal	\$183,680
Sales Tax (0%)	\$0
Street Construction Total without Mobilization	\$183,700
<b>Street Construction Total</b>	<b>\$202,100</b>

Item No.	Approx. Qty.	Unit	Description	Unit Price	Total
<b>Miscellaneous</b>					
13	17	LS	Materials Testing	\$125	\$2,125
14	1	LS	Construction Design (15%) of Street Construction Cost		\$27,555
15	1	LS	Engineering Design (15%) of Ramp Total Cost		\$20,835

Miscellaneous Subtotal	\$50,515
Sales Tax (0%)	\$0
<b>Miscellaneous Total</b>	<b>\$50,600</b>

TOTAL PROJECT COST WITHOUT MOBILIZATION	\$234,300
Push Button Replacement Total Costs	\$44,900
<b>TOTAL PROJECT COST</b>	<b>\$298,000</b>

**List of exclusions:**

Utility pole relocations  
 Right-of-way  
 Traffic signals to be determined

**Assumptions:**

Catch basin adjustments: \$1,500 per every 4 ramps  
 Low cost improvement assumes revising the existing ramp by adding either DWS, grinding lip, or both

**List of typical obstructions:**

Sign poles  
 Mailboxes

**2015 ADA Plan Locations**

Street Name	Cross Street	Corner	Ramp Type	Push Button Improvements at Intersection
NW 6TH AVENUE	NE ASH STREET	E	ST21	No
NW 6TH AVENUE	NE ASH STREET	W	ST21	No
NW 6TH AVENUE	NE DIVISION STREET	E	ST21	No
NW 6TH AVENUE	NE DIVISION STREET	W	ST21	No
NW 6TH AVENUE	NE DIVISION STREET	SE (New)	ST21	No
NE 3RD AVENUE	N SHEPHERD ROAD	SE	ST21	Yes
NE 3RD AVENUE	NE 2ND AVENUE / 4th Avenue	NE	ST21	Yes
NE 3RD AVENUE	NE 2ND AVENUE / 4th Avenue	NW	ST21	Yes
NE 3RD AVENUE	NE 2ND AVENUE / 4th Avenue	SE	ST21	Yes
NE 3RD AVENUE	NE 2ND AVENUE / 4th Avenue	SW	ST21	Yes
NE 3RD AVENUE	NE Weir Street	NE	ST21	No
NE 3RD AVENUE	NE Weir Street	SW	ST21	No
NE 3RD AVENUE	NE Weir Street	SE	ST21	No
NE 3RD AVENUE	NE Weir Street	NW	ST21	No
NW 28TH AVENUE	NW Logan Street	SE	ST21	No
Fiberg-Strunk Street	1st Street	SE	ST20	Yes
Fiberg-Strunk Street	1st Street	SW	ST20	Yes

# Camas ADA Transition Plan Push Button Cost Estimate

Intersection: NW Lake Road / NW Frieberg-Strunk Street  
Area within Camas: NW Camas

Item No.	Quantity	Unit	Description	Unit Price	Total
1	0	EACH	Ped Pole	\$2,500	\$0
2	2	EACH	Push Button post	\$1,500	\$3,000
3	0	EACH	Ped heads	\$800	\$0
4	8	EACH	Push buttons	\$800	\$6,400
5	0	EACH	push button extenders	\$500	\$0
6	30	EACH	new conduit/wiring	\$80	\$2,400
7	100	EACH	wiring in existing conduit	\$10	\$1,000

Subtotal \$12,800  
20% Contingency \$2,600

**Total \$15,400**

**Assumptions:**  
This cost estimate does not include ramp replacements

# Camas ADA Transition Plan Push Button Cost Estimate

Intersection: NE 3rd Avenue / N Shepherd Road  
Area within Camas: East Camas

Item No.	Quantity	Unit	Description	Unit Price	Total
1	0	EACH	Ped Pole	\$2,500	\$0
2	4	EACH	Push Button post	\$1,500	\$6,000
3	0	EACH	Ped heads	\$800	\$0
4	6	EACH	Push buttons	\$800	\$4,800
5	1	EACH	push button extenders	\$500	\$500
6	30	EACH	new conduit/wiring	\$80	\$2,400
7	100	EACH	wiring in existing conduit	\$10	\$1,000

Subtotal \$14,700  
20% Contingency \$3,000  
**Total \$17,700**

**Assumptions:**  
This cost estimate does not include ramp replacements

# Camas ADA Transition Plan Push Button Cost Estimate

Intersection: NE 3rd Avenue / NE 2nd Avenue  
Area within Camas: East Camas

Item No.	Quantity	Unit	Description	Unit Price	Total
1	0	EACH	Ped Pole	\$2,500	\$0
2	0	EACH	Push Button post	\$1,500	\$0
3	0	EACH	Ped heads	\$800	\$0
4	8	EACH	Push buttons	\$800	\$6,400
5	0	EACH	push button extenders	\$500	\$0
6	30	EACH	new conduit/wiring	\$80	\$2,400
7	100	EACH	wiring in existing conduit	\$10	\$1,000

Subtotal \$9,800  
20% Contingency \$2,000

**Total \$11,800**

**Assumptions:**  
This cost estimate does not include ramp replacements



## Memorandum

DATE: December 19, 2025  
TO: Curleigh (Jim) Carothers  
FROM: Apex Companies, LLC  
PROJECT: 24011088 – Camas ADA Plan Update  
REGARDING: Camas ADA Plan: Signal & Push Button Project List

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### SIGNAL & PUSH BUTTON INVENTORY

As part of the City of Camas ADA Transition Plan update, an evaluation of pedestrian push buttons and traffic signal features was conducted to assess compliance with current ADA and PROWAG accessibility requirements. Apex inventoried all traffic signals within the City of Camas (City) and assigned each an A–F grade based on push button compliance and associated curb ramp conditions. The review focused on identifying deficiencies related to push button location, reach range, operability, and associated pedestrian signal features. Descriptions of the push button grading criteria are provided below.

- A – APS Compliant: Push buttons meet all MUTCD spacing requirements and APS compliance, and all associated curb ramps are ADA compliant.
- B – Partially Compliant: All curb ramps are ADA compliant or considered low priority. Pedestrian push buttons are APS compliant, but require upgrades to meet MUTCD spacing requirements.
- C – Push Button Partially Compliant: Push buttons are of APS compliant, but do not meet MUTCD placement requirements. Associated curb ramps meet low and medium priority for replacement.
- D – Push Button Outdated: Push buttons are not APS compliant and do not meet MUTCD spacing requirements. Associated curb ramps meet low and medium priority for replacement.
- F – Non-Compliant: Push buttons are of an older, non-compliant style and do not meet any MUTCD requirements. Associated curb ramps meet high priority for replacement.

For pedestrian push buttons to be ADA compliant, they must meet the requirements of the Americans with Disabilities Act (ADA) and the Public Rights-of-Way Accessibility Guidelines (PROWAG). Push buttons must be located within reach range, typically mounted between 42 and 48 inches above the sidewalk surface and positioned to provide adequate clear space for wheelchair access. Accessible pedestrian signals may also include audible indications and vibrating surfaces to assist individuals with vision impairments in locating the push button and determining when it is safe to cross.

The public involvement phase of the 2025 ADA Transition Plan Update identified downtown Camas as the most important location for Americans with Disabilities Act (ADA) improvements. These project lists represent planning-level estimates, and the number of projects may increase or decrease as designs are refined and additional field verification is completed. This memo lists the intersections where upgrades are needed to improve accessibility for

pedestrians with disabilities and to support safe, equitable pedestrian travel throughout the City. Signalized intersections with grades "D" and "F" are high priority locations for push button replacements.

Three of the intersections highlighted below were previously identified in the 2015 ADA Transition Plan Incomplete Projects Memorandum; therefore, cost estimates for these locations are included in that document attached to the 2025 Self Evaluation Plan. For the remaining intersections not included in the 2015 project list, updated cost estimates have been prepared and are attached to this memo. These cost estimates represent planning level estimates for the 20-Year City Comprehensive Plan. See the attached cost estimate for more details.

## **PUSH BUTTON PROJECT LIST**

### **High Priority Locations**

The following list of intersections are locations where it is recommended that the push buttons be replaced to meet ADA and MUTCD compliance requirements near high priority locations such as downtown Camas.

- NE 4th Avenue / NE Adams Street
- NE 3rd Avenue / SE Crown Road
- NE 3rd Avenue / N Shepherd Road
- NE 3rd Avenue / NE 2nd Avenue
- NE 3rd Avenue / SE Lechner Street

### **Low Priority Locations**

The following list of intersections are locations where it is recommended that the push buttons be replaced to meet ADA and MUTCD compliance requirements near low priority locations.

- NW Lake Road / NW Frieberg-Strunk Street

The total cost of these push button replacements at three locations within the City of Camas is approximately \$45,000.

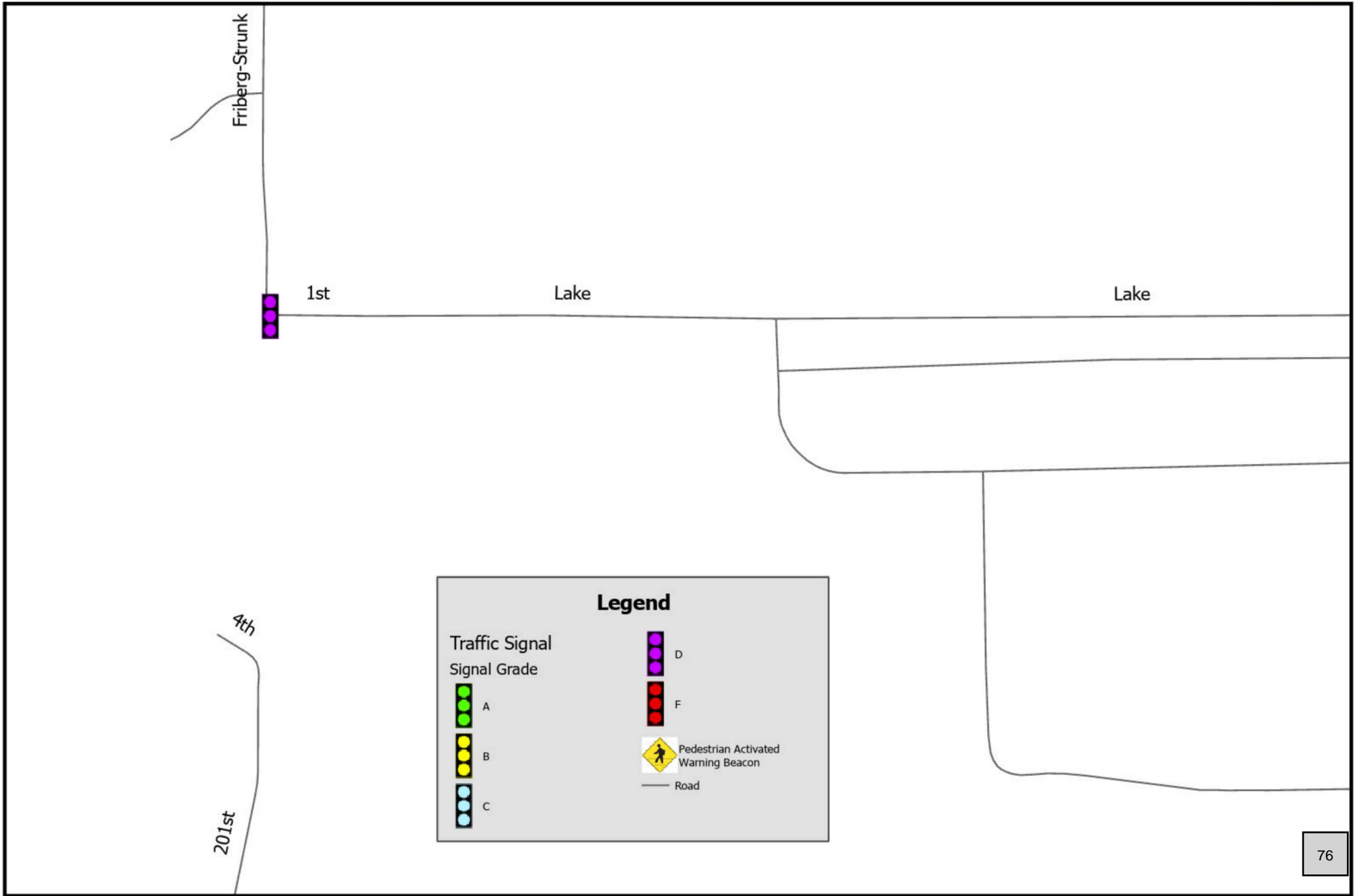
Attachment(s): Push Button Figure  
Push Button Cost Estimates

cc: Chris Lopez – City of Camas

ERS

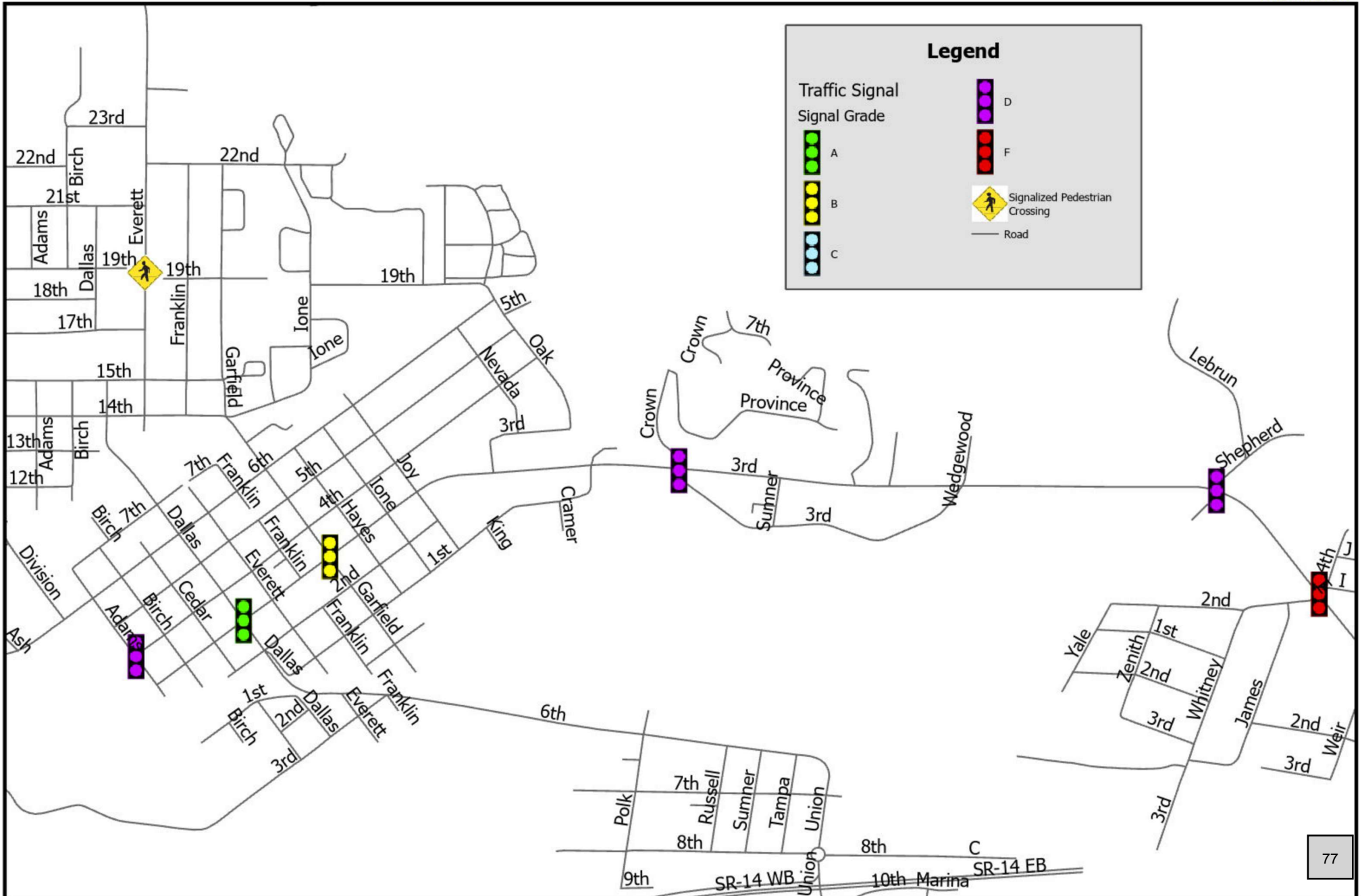
# Camas Downtown ADA Transition Plan

Item 1.



# Camas Downtown ADA Transition Plan

Item 1.



# Camas ADA Transition Plan Push Button Cost Estimate

Intersection: NE 4th Avenue / NE Adams Street  
Area within Camas: Downtown

Item No.	Quantity	Unit	Description	Unit Price	Total
1	0	EACH	Ped Pole	\$2,500	\$0
2	4	EACH	Push Button post	\$1,500	\$6,000
3	0	EACH	Ped heads	\$800	\$0
4	6	EACH	Push buttons	\$800	\$4,800
5	1	EACH	push button extenders	\$500	\$500
6	30	EACH	new conduit/wiring	\$80	\$2,400
7	100	EACH	wiring in existing conduit	\$10	\$1,000

Subtotal \$14,700  
20% Contingency \$3,000  
**Total \$17,700**

**Assumptions:**  
This cost estimate does not include ramp replacements

# Camas ADA Transition Plan Push Button Cost Estimate

Intersection: NE 3rd Avenue / SE Crown Road  
Area within Camas: East Camas

Item No.	Quantity	Unit	Description	Unit Price	Total
1	0	EACH	Ped Pole	\$2,500	\$0
2	2	EACH	Push Button post	\$1,500	\$3,000
3	0	EACH	Ped heads	\$800	\$0
4	8	EACH	Push buttons	\$800	\$6,400
5	0	EACH	push button extenders	\$500	\$0
6	30	EACH	new conduit/wiring	\$80	\$2,400
7	100	EACH	wiring in existing conduit	\$10	\$1,000

Subtotal	\$12,800
20% Contingency	\$2,600
<b>Total</b>	<b>\$15,400</b>

**Assumptions:**  
This cost estimate does not include ramp replacements

### Camas ADA Transition Plan Push Button Cost Estimate

Intersection: NE 3rd Avenue / SE Lechner Street  
Area within Camas: East Camas

Item No.	Quantity	Unit	Description	Unit Price	Total
1	0	EACH	Ped Pole	\$2,500	\$0
2	0	EACH	Push Button post	\$1,500	\$0
3	0	EACH	Ped heads	\$800	\$0
4	8	EACH	Push buttons	\$800	\$6,400
5	0	EACH	push button extenders	\$500	\$0
6	30	EACH	new conduit/wiring	\$80	\$2,400
7	100	EACH	wiring in existing conduit	\$10	\$1,000

Subtotal	\$9,800
20% Contingency	\$2,000
<b>Total</b>	<b>\$11,800</b>

**Assumptions:**  
This cost estimate does not include ramp replacements



January 29, 2026  
 Bid #: WA88631BC

Precision Concrete Cutting  
 3191 North Canyon Rd  
 Provo, Utah 84604  
 (801) 373-6060 - phone  
 (801) 855-7150 - fax

City of Camas  
 Jim Carothers  
 616 Northeast 4th ave  
 Camas, Washington 98607  
 (360) 817-1568  
[jcarothers@cityofcamas.us](mailto:jcarothers@cityofcamas.us)

City of Camas 2026

**Total Ln. Ft.**  
 278.0

**Total In. Ft.**  
 177.64

<b>PRECISION CONCRETE CUTTING</b>						
No.	High	Low	Linear Feet	Location		Inch Feet
1	0.750	0.625	4.0	ne walk of 6th ave. intersection on 6th ave and birch st. ✓		2.75
2	0.750	0.250	6.0	ne walk of 6th ave. intersection on 6th ave and Dallas st. asphalt lip cut ✓		3.00
4	0.500	0.375	4.0	ne walk of 6th ave. intersection on 6th ave and Hayes st. ←	we listed NE, SE, and NW corners to grind	1.75
7	0.375	0.000	2.0	ne walk of 6th ave. intersection on 6th ave and joy st. Ada. ←	we only listed SW corner to grind	0.38
8	0.375	0.000	6.0	ne walk of 6th ave. intersection on 6th ave and joy st. Ada. asphalt to Ada possible remeasure ←		1.13
9	0.375	0.000	4.0	ne walk of 6th ave. intersection on 6th ave and joy st. Ada. ←		0.75
11	0.250	0.000	4.3	ne walk of 6th ave. intersection on 6th ave and joy st. Ada. asphalt to concrete cut. possible remeasure ←	we did not list this corner to grind	0.53
12	0.625	0.500	4.5	sw walk of 5th ave. intersection on 5th ave and lone st. Ada.		2.53
13	0.875	0.000	4.5	sw walk of 5th ave. intersection on 5th ave and lone st. Ada. ✓		1.97
14	1.500	0.875	8.0	sw walk of 5th ave. intersection on 5th ave and lone st. Ada. possible drop ✓		9.50
15	0.500	0.000	4.0	sw walk of 5th ave. intersection on 5th ave and lone st. Ada. ✓		1.00
16	0.750	0.000	4.0	sw walk of 5th ave. intersection on 5th ave and Hayes st. Ada. asphalt to concrete cut ✓		1.50
17	0.375	0.000	4.0	sw walk of 5th ave. intersection on 5th ave and Hayes st. Ada. asphalt to concrete cut ✓		0.75

listed this corner to add DWS and not to grind

18	0.500	0.000	8.8	sw walk of 5th ave. intersection on 5th ave and franklin st. Ada. asphalt to concrete cut	2.19
19	0.875	0.250	7.0	sw walk of 5th ave. intersection on 5th ave and Everett st. Ada. ✓	3.94
20	0.875	0.500	6.0	sw walk of 5th ave. intersection on 5th ave and Everett st. Ada. ✓	4.13
21	0.625	0.000	4.3	sw walk of 5th ave. intersection on 5th ave and Everett st. Ada. ✓	1.33
22	0.750	0.250	7.0	sw walk of 5th ave. intersection on 5th ave and birch st. Ada. ✓	3.50
23	1.750	0.250	9.8	ne walk of 4 th ave. crosswalk by universal jujitsu.	9.75
24	0.500	0.000	6.0	ne walk of 4 th ave. crosswalk by universal jujitsu.	1.50
25	1.000	0.750	10.0	ne walk of 4 th ave. intersection on 4th ave and birch st. extended cut 1 of 3 ✓	8.75
26	1.000	0.750	10.0	ne walk of 4 th ave. intersection on 4th ave and birch st. extended cut 2 of 3	8.75
27	1.875	0.750	6.0	ne walk of 4 th ave. intersection on 4th ave and birch st. extended cut 3. of 3	7.88
28	0.500	0.500	10.0	ne walk of 4 th ave. intersection on 4th ave and birch st. extended cut 1 of 2 ✓	5.00
29	0.500	0.500	10.0	ne walk of 4 th ave. intersection on 4th ave and birch st. extended cut 2 of 2	5.00
30	0.500	0.250	8.0	ne walk of 4 th ave. intersection on 4th ave and cedar st. ✓	3.00
31	0.875	0.750	10.0	ne walk of 4 th ave. intersection on 4th ave and cedar st. extended cut 1 of 3	8.13
32	1.125	0.750	10.0	ne walk of 4 th ave. intersection on 4th ave and cedar st. extended cut 2 of 3 ✓	9.38
33	0.750	0.500	10.0	ne walk of 4 th ave. intersection on 4th ave and cedar st. extended cut 3 of 3	6.25
34	0.750	0.375	10.0	ne walk of 4 th ave. intersection on 4th ave and cedar st. extended cut 1 of 3	5.63
35	0.750	0.500	10.0	ne walk of 4 th ave. intersection on 4th ave and cedar st. extended cut 2 of 3 ✓	6.25
36	0.750	0.500	5.0	ne walk of 4 th ave. intersection on 4th ave and cedar st. extended cut 3 of 3	3.13
37	0.750	0.375	6.0	ne walk of 4 th ave. intersection on 4th ave and Dallas St. asphalt to cement cut.	3.38
38	0.500	0.250	5.0	ne walk of 4 th ave. intersection on 4th ave and Dallas St. asphalt to cement cut.	1.88

we also listed NW, NE, and SE corners to grind

we listed these as DWS only

we also listed the NE corner to grind

we also listed the NW and SE corners to grind

39	0.750	0.500	4.5	ne walk of 4 th ave. intersection on 4th ave and Everett st. ✓	2.81
40	0.500	0.250	4.5	ne walk of 4 th ave. intersection on 4th ave and Franklin st. Ada. ✓	1.69
41	0.875	0.000	4.0	ne walk of 4 th ave. intersection on 4th ave and Franklin st. Ada. ✓	1.75
42	0.625	0.000	3.0	ne walk of 4 th ave. intersection on 4th ave and joy st. Ada. ✓	0.94
43	1.250	0.250	4.5	ne walk of 4 th ave. intersection on 4th ave and joy st. Ada. ✓	3.38
44	0.875	0.000	3.5	ne walk of 4 th ave. intersection on 4th ave and joy st. Ada. ← we are listing this ramp to replace	1.53
45	1.000	0.750	7.0	ne walk of 2nd ave. intersection on 2nd ave and Everett st. Ada. ✓	6.13
59	1.250	0.750	6.0	sw walk of 1st ave. intersection on 1st ave and Hayes st. Ada. ← we are listing this ramp to replace	6.00
60	1.750	0.625	6.0	sw walk of 1st ave. intersection on 1st ave and Hayes st. Ada. storm grate possible cut to metal ✓	7.13
72	2.000	0.875	7.0	sw walk of 1st ave. intersection on 1st ave and Everett st. Ada. possible replace ← we are listing this ramp to replace	10.06
		<b>Totals:</b>	<b>278.0</b>		<b>177.64</b>

**Total Cost for Trip Hazard Repair: \$13,678.33**

*\*All Bids and Proposals from Precision Concrete Cutting are valid for 90 days. After 90 days the scope or pricing may need to be adjusted, please contact your sales rep for a new bid with current pricing.*

*Precision Concrete Cutting has a minimum mobilization fee of \$1,000.00*

*\*Bids are proprietary to Precision Concrete Cutting & should not be shared with other contractors without permission*

*\*Precision Concrete Cutting will identify panels that need replacement but we do not provide replacement services.*

*Precision Concrete Cutting (PCC) repairs only those uneven sidewalks specifically requested by the client and therefore makes no guarantee that the property is free of uneven sidewalk hazards (trip hazards). After the project is completed, sidewalks will continue to shift due to tree roots, water, settling, and other natural and man-made causes outside of PCC's control. PCC is not liable for any related claims, losses, or damages related to future trip hazards or hazards that were not addressed by this project.*

*At the time of completion, PCC warranties that the trip hazard repairs are ADA Compliant, specifically with regard to the ADA Change in Level standard. Upon completion you agree to inspect the work, payment of your invoice is indication that you have inspected the property and the work has been done to your satisfaction.*

*If any repair locations are inaccessible during our repair process, and an additional trip is needed, a \$250 mobilization fee will be added to the invoice. Invoice is due upon receipt, if not paid in full within 30 days of the invoice date a 5% late fee will be assessed every 15 days until it is paid.*

*\*If credit card payment is used, 3% service fee will apply.*





January 29, 2026  
Bid #: WA88631BC

Precision Concrete Cutting  
3191 North Canyon Rd  
Provo, Utah 84604  
(801) 373-6060 - phone  
(801) 855-7150 - fax

City of Camas  
Jim Carothers  
616 Northeast 4th ave  
Camas, Washington 98607  
(360) 817-1568  
[jcarothers@cityofcamas.us](mailto:jcarothers@cityofcamas.us)

City of Camas 2026  
DOME PLATE LOCATIONS

**Total Ln. Ft.**  
141.8

**Total In. Ft.**  
91.22

<b>PRECISION CONCRETE CUTTING</b>					
<b>No.</b>	<b>High</b>	<b>Low</b>	<b>Linear Feet</b>	<b>Location</b>	<b>Inch Feet</b>
3	0.750	0.500	4.0	ne walk of 6th ave. intersection on 6th ave and Hayes st. dome plate must be removed	2.50
5	0.750	0.500	4.0	ne walk of 6th ave. intersection on 6th ave and lone st. dome plate remove	2.50
6	0.375	0.250	4.0	ne walk of 6th ave. intersection on 6th ave and lone st. dome plate remove	1.25
10	0.750	0.250	4.5	ne walk of 6th ave. intersection on 6th ave and joy st. Ada. remove dome plate	2.25
46	0.625	0.375	5.0	ne walk of 2nd ave. intersection on 2nd ave and Everett st. Ada. dome plate	2.50
47	0.500	0.250	4.3	ne walk of 2nd ave. intersection on 2nd ave and Everett st. Ada. dome plate	1.59
48	1.000	0.125	5.5	ne walk of 2nd ave. intersection on 2nd ave and Hayes st. Ada. dome plate	3.09
49	0.375	0.375	5.0	ne walk of 2nd ave. intersection on 2nd ave and lone st. Ada. dome plate	1.88
50	0.625	0.250	5.0	ne walk of 2nd ave. intersection on 2nd ave and lone st. Ada. dome plate	2.19
51	0.500	0.500	5.0	ne walk of 2nd ave. intersection on 2nd ave and lone st. Ada. dome plate	2.50
52	0.875	0.750	5.0	ne walk of 2nd ave. intersection on 2nd ave and lone st. Ada. dome plate	4.06
53	0.875	0.375	5.3	ne walk of 2nd ave. intersection on 2nd ave and lone st. Ada. dome plate	3.28
54	1.125	0.500	6.0	ne walk of 2nd ave. intersection on 2nd ave and joy st. Ada. dome plate	4.88
55	0.875	0.750	6.0	ne walk of 2nd ave. intersection on 2nd ave and joy st. Ada. dome plate	4.88

56	1.875	0.000	5.0	sw walk of 1st ave. intersection on 1st ave and joy st. Ada. dome plate	4.69
57	0.750	0.750	5.5	sw walk of 1st ave. intersection on 1st ave and joy st. Ada. dome plate	4.13
58	0.875	0.875	6.0	sw walk of 1st ave. intersection on 1st ave and lone st. Ada. dome plate	5.25
61	0.750	0.750	5.5	sw walk of 1st ave. intersection on 1st ave and garfield st. Ada. dome plate	4.13
62	0.500	0.375	5.0	sw walk of 1st ave. intersection on 1st ave and Garfield st. Ada. dome plate	2.19
63	0.625	0.500	5.0	sw walk of 1st ave. intersection on 1st ave and Garfield st. Ada. dome plate	2.81
64	1.125	0.500	5.5	sw walk of 1st ave. intersection on 1st ave and Garfield st. Ada. dome plate	4.47
65	0.875	0.500	5.5	sw walk of 1st ave. intersection on 1st ave and Franklin st. Ada. dome plate	3.78
66	0.750	0.250	5.3	sw walk of 1st ave. intersection on 1st ave and Franklin st. Ada. dome plate	2.63
67	0.875	0.375	5.0	sw walk of 1st ave. intersection on 1st ave and Franklin st. Ada. dome plate	3.13
68	1.250	0.625	6.0	sw walk of 1st ave. intersection on 1st ave and Everett st. Ada. dome plate	5.63
69	1.125	0.875	6.0	sw walk of 1st ave. intersection on 1st ave and Everett st. Ada. dome plate	6.00
70	0.625	0.375	5.0	sw walk of 1st ave. intersection on 1st ave and Everett st. Ada. dome plate	2.50
71	0.375	0.000	3.0	sw walk of 1st ave. intersection on 1st ave and Everett st. Ada. dome plate	0.56
		<b>Totals:</b>	<b>141.8</b>		<b>91.22</b>

**Total Cost for Trip Hazard Repair: \$7,023.84**

*\*All Bids and Proposals from Precision Concrete Cutting are valid for 90 days. After 90 days the scope or pricing may need to be adjusted, please contact your sales rep for a new bid with current pricing.*

*Precision Concrete Cutting has a minimum mobilization fee of \$1,000.00*

*\*Bids are proprietary to Precision Concrete Cutting & should not be shared with other contractors without permission  
 \*Precision Concrete Cutting will identify panels that need replacement but we do not provide replacement services.*

*Precision Concrete Cutting (PCC) repairs only those uneven sidewalks specifically requested by the client and therefore makes no guarantee that the property is free of uneven sidewalk hazards (trip hazards). After the project is completed, sidewalks will continue to shift due to tree roots, water, settling, and other natural and man-made causes outside of PCC's control. PCC is not liable for any related claims, losses, or damages*

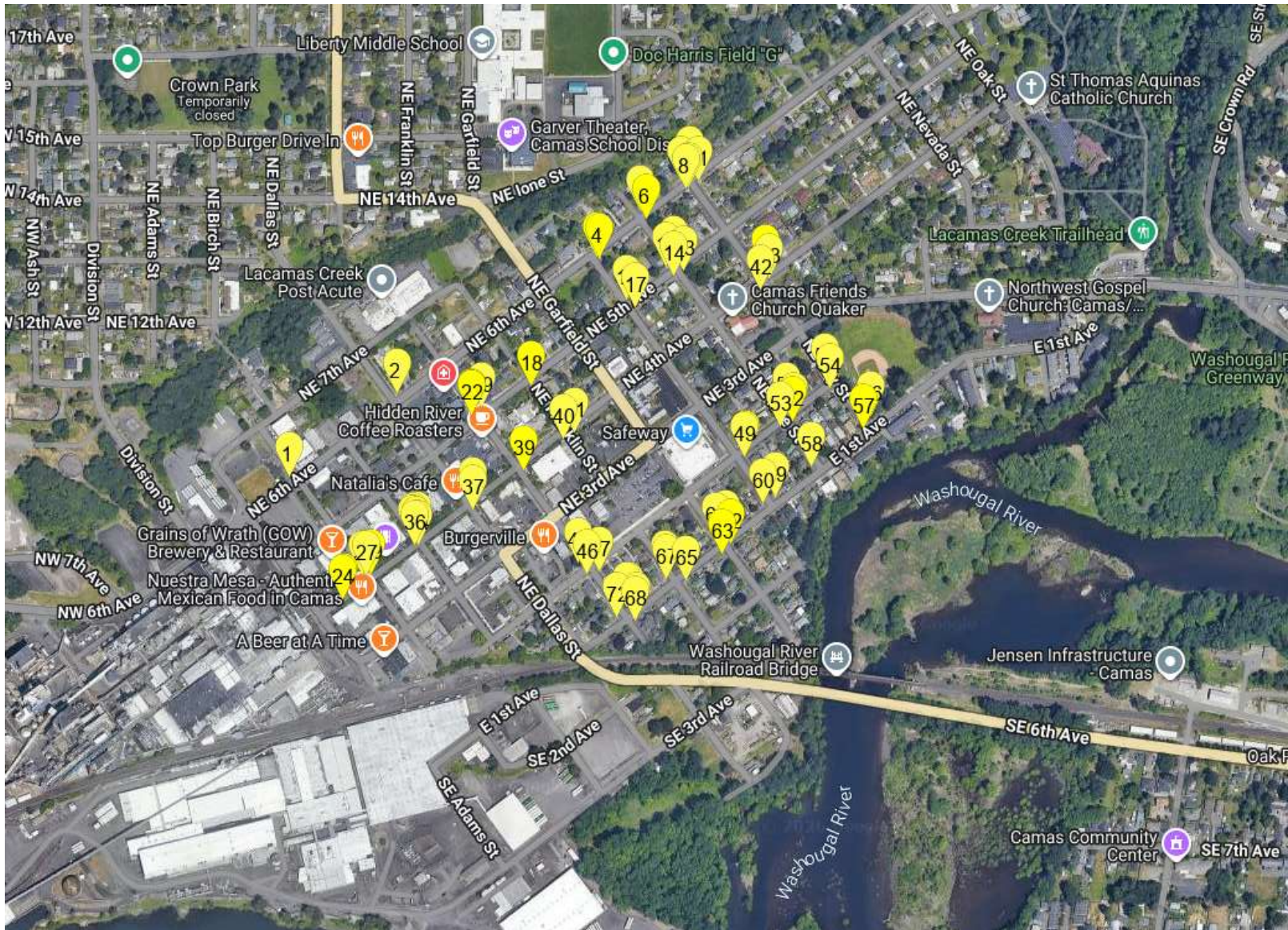
*related to future trip hazards or hazards that were not addressed by this project.*

*At the time of completion, PCC warranties that the trip hazard repairs are ADA Compliant, specifically with regard to the ADA Change in Level standard. Upon completion you agree to inspect the work, payment of your invoice is indication that you have inspected the property and the work has been done to your satisfaction.*

*If any repair locations are inaccessible during our repair process, and an additional trip is needed, a \$250 mobilization fee will be added to the invoice. Invoice is due upon receipt, if not paid in full within 30 days of the invoice date a 5% late fee will be assessed every 15 days until it is paid.*

*\*If credit card payment is used, 3% service fee will apply.*





# PRECISION CONCRETE CUTTING • AUTHORIZATION TO PROCEED

## Billing Information:

Business/Client Name: \_\_\_\_\_

Address: \_\_\_\_\_

City \_\_\_\_\_ State: \_\_\_\_\_ Zip \_\_\_\_\_

Phone # \_\_\_\_\_ Email: \_\_\_\_\_

Bid #: \_\_\_\_\_ PO # (if applicable): \_\_\_\_\_

Option Approved: \_\_\_\_\_ Amount: \_\_\_\_\_

Start Date: \_\_\_\_\_

Signature of Authorized Purchaser:

\_\_\_\_\_ Date: \_\_\_\_\_

## Project Details:

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_  
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All Bids and Proposals from Precision Concrete Cutting are valid for 90 days. After 90 days the scope or pricing may need to be adjusted, please contact your sales rep for a new bid with current pricing.

Precision Concrete Cutting has a minimum mobilization fee of \$1,000.00

Bids are proprietary to Precision Concrete Cutting & should not be shared with other contractors without permission. Precision Concrete Cutting will identify panels that need replacement but we do not do replacement.

Precision Concrete Cutting (PCC) repairs only those uneven sidewalks specifically requested by the client and therefore makes no guarantee that the property is free of uneven sidewalk hazards (trip hazards). After the project is completed, sidewalks will continue to shift due to tree roots, water, settling, and other natural and man-made causes outside of PCC's control. PCC is not liable for any related claims, losses, or damages related to future trip hazards or hazards that were not addressed by this project.

At the time of completion, PCC warrants that the trip hazard repairs are ADA Compliant, specifically with regard to the ADA Change in Level standard. Upon completion you agree to inspect the work, payment of your invoice is indication that you have inspected the property and the work has been done to your satisfaction.

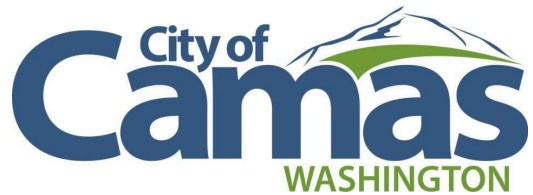
If any repair locations are inaccessible during our repair process, and an additional trip is needed, a \$250 mobilization fee will be added to the invoice. Invoice is due upon receipt, if not paid in full within 30 days of the invoice date a 5% late fee will be assessed every 15 days until it is paid.

\*If credit card payment is used, 3% service fee will apply.



# **Appendix B**

## **2015 ADA Transition Plan**



# ADA TRANSITION PLAN FOR THE PUBLIC RIGHT OF WAY

May 7, 2015



Prepared for:  
**The City of Camas Public Works Department**

Prepared by:  
**HDJ Design Group PLLC**  
in conjunction with:  
**Urbane Streets**



## **Mission Statement**

*The City of Camas commits to preserving its heritage, sustaining and enhancing a high quality of life for all its citizens, and developing the community to meet the challenges of the future. We take pride in preserving a healthful environment while promoting economic growth. We encourage citizens to participate in government and community, assisting the City in its efforts to provide quality services consistent with their desires and needs.*

## **Americans with Disabilities Act & Title VI information**

**Americans with Disabilities Act (ADA) Information:** Materials can be provided in alternative formats by contacting Jim Carothers, pending City Transportation ADA Coordinator at 360-817-1561 x4230 (voice) or [jcarothers@cityofcamas.us](mailto:jcarothers@cityofcamas.us) (email). Persons who are deaf or hard of hearing may make a request for alternative formats through the Washington Relay Service at 7-1-1.

**Title VI Notice to Public:** It is the City of Camas' policy to assure that no person shall, on the grounds of race, color, national origin or sex, as provided by Title VI of the Civil Rights Act of 1964, be excluded from participation in, be denied the benefits of, or be otherwise discriminated against under any of its federally funded programs and activities. Any person who believes his/her Title VI protection has been violated may file a complaint with the Title VI Coordinator. For Title VI complaint forms and advice, please contact the City's Administrative Services Director who is the Title VI Coordinator at 360-817-7013 (voice).

## Acknowledgements

### **The City of Camas City Council**

Scott Higgins, Mayor  
Greg Anderson  
Bonnie Carter  
Don Chaney  
Tim Hazen  
Steve Hogan  
Melissa Smith  
Shannon Turk

### **The City of Camas**

Pete Capell  
James Carothers  
Wes Heigh  
Jeff Englund  
Jim Hodges  
Ronda Syverson

### **ADA Community Advisory Committee for the Public Right of Way**

Tom Anderson  
Barbara Blair  
Michelle Cousins  
Sherri Dickerson  
Michael Frazer  
Aileen Gillespie  
Kristine Graham  
Christine Kamps  
Karl Martin  
Carol Popi

### **Submitted by**

HDJ: John Manix, PTOE

### **In Association with**

Urbane Streets: Todd Boulanger, MURP

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## Overview

The Americans with Disabilities Act (ADA) and Section 504 of the Rehabilitation Act of 1973 requires cities with more than fifty employees to prepare a Self-evaluation Report and ADA Transition Plan. The ADA Transition Plan is intended to build on the self-evaluation and provide a plan to eliminate barriers to the disabled.

The City of Camas has an existing ADA Transition Plan for its public buildings, hiring practices, and meeting procedures. The current effort is intended to address the built environment in the public right of way. The ADA and Section 504 emphasize that cities engage members of the disabled community in preparing their Self-evaluation Report and ADA Transition Plan.

The guidance in this document is based on the Washington State Department of Transportation Local Agencies Guide (LAG) manual. Using the results of the self-evaluation, the ADA Transition Plan shall include, as a minimum:

- Identify the physical obstacles that limit accessibility;
- Describe in detail what will done to make the facilities accessible;
- Specify the schedule for each facility and or obstacle to be retrofitted; and
- Identify the individual responsible for the implementation of the plan.

Many agencies throughout the US are behind in implementing all current ADA and Section 504 regulations. With the tremendous cost of ADA improvements, this is not surprising, but it is important to remember that ADA improvements benefit many users of the public right of way. Pedestrians are less like to trip on up-lifted sidewalk sections, children can easily access the sidewalk using ramps while they learn to ride a bike, and most all, as the citizens of Camas age, accessibility will be critical to maintaining quality of life, as they seek to age in place.

## Executive Summary

This ADA Transition Plan focuses on the transportation facilities in the public right of way. The City's Self-evaluation Report identified high priority locations and high priority improvements within those locations. It also provides recommendations on policy and procedures related to accessibility. The ADA Transition Plan will recommend a list of high priority projects to bring the City into compliance with the Americans with Disabilities Act.

### The City of Camas Self-evaluation Report Conclusions:

Based on the findings of the Self-Evaluation Report concluded that:

- Modifications are necessary to achieve ADA compliance and an ADA Transition Plan is required;
- The City of Camas's policy and practices should be modified per the recommendations of the self-evaluation. Appendix A includes the recommendations from the initial Self-evaluation Report related to Public Works Policies and Procedures;
- A public process was conducted to elicit comments from the disabled community on locations that are a concern and on proposed improvements;
- Locations with a full height curb should be addressed first, as they are major barriers to the disabled community;
- Sidewalks and pedestrian crossings in need of substantial repair should be addressed, as a second priority;
- Curb ramps without detectable warning patterns or a lip of over a half of an inch at the center, should be third priority for minor upgrades; and
- Traffic signals with pedestrian push buttons beyond the reach standards for ADA should be the highest priority for Accessible Pedestrian Signal upgrades.

Appendix C provides a list of high priority projects and a proposed schedule based on assumed funding sources.

### Vision, Goals and Objectives:

Good planning efforts rely on vision, goals, and objectives statements to assure successful outcomes. The following proposed statements are intended to provide the citizens, City Council, and the staff of Camas a clear direction that the plan will lead them forward.

#### Vision:

Camas is renowned as a healthy, vibrant community with high pedestrian activity. Citizens of all ability feel comfortable traveling within the community. The streets have sidewalks that are wide and smooth and have curb ramps at all the intersections. The Camas traffic signals are user friendly for pedestrians. It is acknowledged as a walk friendly community by the Federal Highway Administration based on the progress made to make it's streets accessible to all users. The Camas ADA Transition Plan has proven useful to assist staff to eliminate barriers to the disabled community.

#### Goals:

The following goals will help Camas achieve this vision.

1. Important locations, such as the downtown, are free of tripping hazards;
2. All arterial intersections have curb ramps that meet current standards;
3. All bus stops are accessible and connected to the sidewalk network; and
4. All traffic signals are upgraded with accessible pedestrian signal (APS) hardware.

### Objectives:

The following objectives will meet the goals and the vision of the ADA Transition Plan.

1. Complete ADA upgrades at all high priority location within 5 to 10 years;
2. Complete ADA upgrades at traffic signal within 10 years;
3. Complete ADA upgrades at all medium priority locations within 30 years; and
4. The ADA coordinator actively pursues ADA upgrades.

### Location Prioritization

Location prioritization is based on criteria identified in the Self-Evaluation Report. The following ranking criteria data were mapped for evaluation. It was then reviewed and discussed by the Citizen Advisory Committee (CAC) for use in the final location prioritization. See Appendix B for the maps of high priority criteria.

- **Collision History** for pedestrians and bicyclists, as a measure for risk exposure for persons with disabilities in the public right of way;
- **Roadway Classification** of arterials and not local streets, as a measure for more exposure to higher speed and volume roadways;
- **Government Buildings** that are important destinations for all citizens, as a measure for higher levels of pedestrian traffic generated when seeking public services, access to job sites or elected officials;
- **Land-use Zoning** of commercial property, a measure for higher levels of pedestrian traffic generated to travel along the public right of way;
- **Medical Facilities** important to persons who are disabled, as measure for higher frequency of such traffic demand;
- **Fixed Route Transit Service** as a measure of pedestrian traffic and also of desirable routes to bus stops for a barrier free pedestrian network; and
- **Committee Identified Priority Sites** these locations have been identified through the CAC public process and added to the priority list based on local knowledge of conditions and desirable destinations for persons with disabilities.

The high priority locations were inventoried for need for ADA upgrades that include partial retrofit or full replacement of curb ramps, with each upgrade ranked in the inventory. See Appendix B for maps and memo describing upgrades to the high and medium priority locations.

The inventory included an estimate of the scope of work to upgrade high priority locations to current ADA standards. The inventory was reviewed and a planning level construction cost estimated was prepared for each upgrade.

## Funding

The final project list and schedule are based on the funding available for ADA upgrades. Funding for ADA upgrades fall under the following categories:

- Local funding;
- The Community Development Block Grant (CDBG);
- State and Federal transportation grants; and
- The C-TRAN Streamline Program for improving access to bus stops.

### Local Funding Focus

- Respond to complaints within annual budget;
- Minor upgrades to existing curb ramps;
- Condition upgrades as part of the development review process;
- Upgrade sidewalk ramps as part of pavement preservation projects; and
- Notify property owner(s) to repair of sidewalk as required by the Camas Municipal Code (CMC Chapter 12.04.020).

It is important for the City to allocate an annual budget to effectively respond in a timely manner to a citizen request (aka the “Grievance Process”), so as to minimize any future risk of the US Department of Justice involvement in a complaint. A program that only responds to citizen requests and requires development to install ADA upgrades, as a condition of their approval, is important work, but will typically be focused at spot locations and thus the outcome may not address high priority locations during the initial years.

Assuming that during some years, if no citizen requests are submitted, the available budget could be used for minor upgrades to substantially compliant ADA curb ramps. This might include adding post-construction detectable warning panels and grinding down the lip of gutter at the base of the ramp to older facilities.



**Figure 1: Example of ramp lip of gutter that is a barrier to the disabled**

As property is redeveloped, the City will have the opportunity to require ADA upgrades to the sidewalk system. This provides another local funding source to reach the vision set forth in this plan.

The upgrade of pedestrian crossings and curb ramps altered by pavement preservation projects will have a higher correlation with high priority locations than projects only initiated only under the local funding focus, as mentioned previously. Often transit routes need higher frequency of pavement maintenance due to the heavy load associated with buses along higher frequency routes.

Requiring repair of uplifted sidewalk panels by the adjacent property owner is typically made based on citizen complaints. The City could focus on high priority

corridors and work in partnership with adjacent property owner to facilitate repairs that will both address ADA compliance but also minimize risk of tripping hazards or other property maintenance for the affected owner.



**Figure 2: Example of damaged sidewalk that is a barrier to the disabled**

### **Community Development Block Grant Focus**

The Community Development Block Grant (aka CDBG) program can provide funding at prioritized locations but will be limited to funding upgrades to new curb ramps at intersections without existing ramps. This is important because full height curbs are true barriers in the public right of way. The inventory of high priority locations found that most Camas intersections have ramps but a few projects as proposed are expected to be stand-alone CDBG projects due to the current funding guidance restrictions.



**Figure 3: Example of full height curb that is a barrier to the disabled**

### **Federal and State Transportation Grant Focus**

Transportation grants are competitive and on an annual or every other year schedule. These grants are intended to address safety, capacity or missing links in the transportation system. Technically all ADA upgrades qualify for use of these grants but typically they do not rank well as a stand-alone ADA project. The following grant programs should be combined with other transportation improvements to upgrade high priority locations.

- The Surface Transportation Program (STP);
- The State City Safety Program;
- The Transportation Alternatives Program (TAP);
- State Pedestrian & Bicycle Safety Grants;
- The Safe Routes to School (SRTS) Program; and
- The Transportation Improvement Board Sidewalk Program.

The Washington State City Safety Program has been used effectively in Pasco (Court Street) and in Vancouver (Fourth Plain Boulevard) to make ADA upgrades at high priority locations within larger corridor “safety” projects. Signal upgrades often rank among the highest of any improvement for safety grants. This is an excellent opportunity to make intersections built before 2010 ADA compliant by upgrading the same traffic signals with Accessible Pedestrian Signal (APS) hardware. The key

strategy is to propose ADA enhancements on locations with serious or fatal injury traffic collisions.



Figure 4: Examples of pedestrian push buttons, existing (left) and APS (right)

The Safe Routes to School Program or the TAP Program are great funding sources for upgrades to high priority locations that include sidewalk in-fill in the vicinity of schools or other pedestrian destinations. Safety and accessibility improvements made to pedestrian facilities closest to schools can allow more students to walk to school and assist school districts in providing transportation to more distant students, as these state funds become more limited each year.



Figure 5: Example of children walking to school on funded sidewalk

The same criteria used for ranking locations as high priority for ADA upgrades will tend to rank well for TAP funds too. The key is to extend the scope beyond just the facility expansion to include other ADA upgrades. The NE 3<sup>rd</sup> Ave corridor in Camas has transit service and some missing sections of sidewalk. A pedestrian improvement grant along this corridor to infill the missing sidewalk, add count down signal heads should rank very well for funding during any given grant year.



Figure 6: Examples of existing missing sidewalk section

**C-TRAN Streamline Program**

C-TRAN has a Streamlining Project program that systematically retrofits existing fixed route transit stops with ADA accessible concrete pads and other enhancements. This grant program has not been used for any bus stop enhancement work in Camas yet. Although no funding appears available until the 2016 grant cycle, this source looks like a very good opportunity to fund bus stop upgrades along the Route 92 corridor.



**Figure 6: Example of an improved bus stop**

## ADA Upgrade Schedule

The key element of an ADA Transition Plan is the schedule of improvements that will bring the community into compliance. The schedule is the culmination of the following tasks:

1. Provide a public process to identify high priority locations;
2. Inventory the locations;
3. Develop a scope of work for the improvements;
4. Estimate the construction cost for the improvements; and
5. Identify funding programs for implementing the improvements.

The preparation of the Self-evaluation Report included providing a public process that engaged members of the disabled community as well as provided public notice to the community at large. An inventory of high priority locations with a scope of work to upgrade them and construction cost estimate are included in the Self-evaluation Report. This ADA Transition Plan builds upon the initial self-evaluation to include proposed funding sources for the upgrades.

The schedule is based on local funding for minor upgrades or repairs, CDBG funding for replacing full height curb with new curb ramps, and State and Federal grants for larger corridor projects. See Appendix C for project schedules for the each funding source.

## **Appendix A – Public Works Department Policies and Procedures**

- Recommendations from the Self-evaluation Report

## THE CITY OF CAMAS

### ADA SELF-EVALUATION REPORT CONCLUSIONS

In accordance with the requirements of Title II of the Americans with Disabilities Act of 1990 (“ADA”), The City of Camas will not discriminate against qualified individuals with disabilities on the basis of disability in its services, programs, or activities.

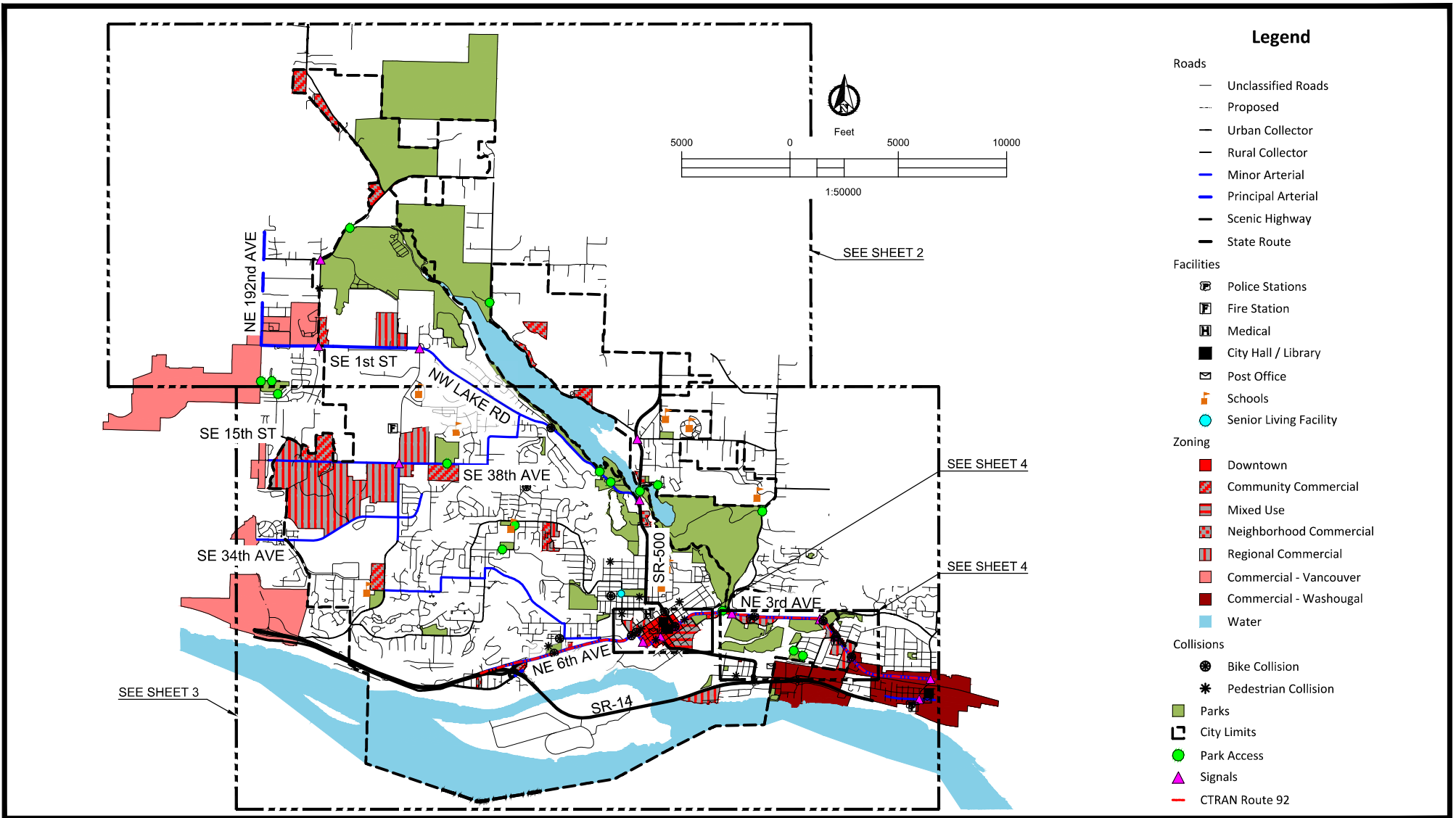
The ADA Transition Plan is intended to build on the ADA Self-evaluation Report and provide a formal assessment as to how to eliminate barriers to the disabled. The Self-evaluation activity is intended as an inventory of existing conditions of the physical conditions, as well as policies and procedures.

#### The City of Camas ADA Self-evaluation Report’s planning activity with its public process concluded:

- The City of Camas has reasonably good ADA access to transportation facilities in the public right of way;
- The City of Camas’s policy and practices should be modified per the recommendations of the Self-evaluation report;
- Modifications are necessary to achieve ADA compliance and an ADA Transition Plan is required;
- The inventory of high priority locations should be included in the ADA Transition Plan and ADA improvement scheduled at locations that are a barrier to persons with disabilities;
- Locations with a full height curb should be addressed first, as they are major barriers to the disabled community;
- Sidewalks and pedestrian crossings in need of substantial repair should be addressed, as a second priority;
- Curb ramps without detectable warning patters or a lip of over a half of an inch at the center, should be third priority for minor upgrades;
- Traffic signals with pedestrian push buttons beyond the reach standards for ADA should be the highest priority for Accessible Pedestrian Signal upgrades; and
- Adopt the US Department of Justice ADA Grievance Procedure Under the Americans with Disability Act.

## Appendix B – Prioritization Criteria for High and Medium Locations

- City of Camas City-wide Self-evaluation and ADA Transition Plan – Prioritization Criteria Map
- City of Camas City-wide Self-evaluation and ADA Transition Plan – High and Medium Priority Locations Map – September 2014
- Memo regarding High and Medium Priority Locations by Type
- Final High and Medium Priority Intersections, Corridors, Areas List
- City of Camas City-wide Self-evaluation and ADA Transition Plan – High and Medium Priority Locations Map – December 2014

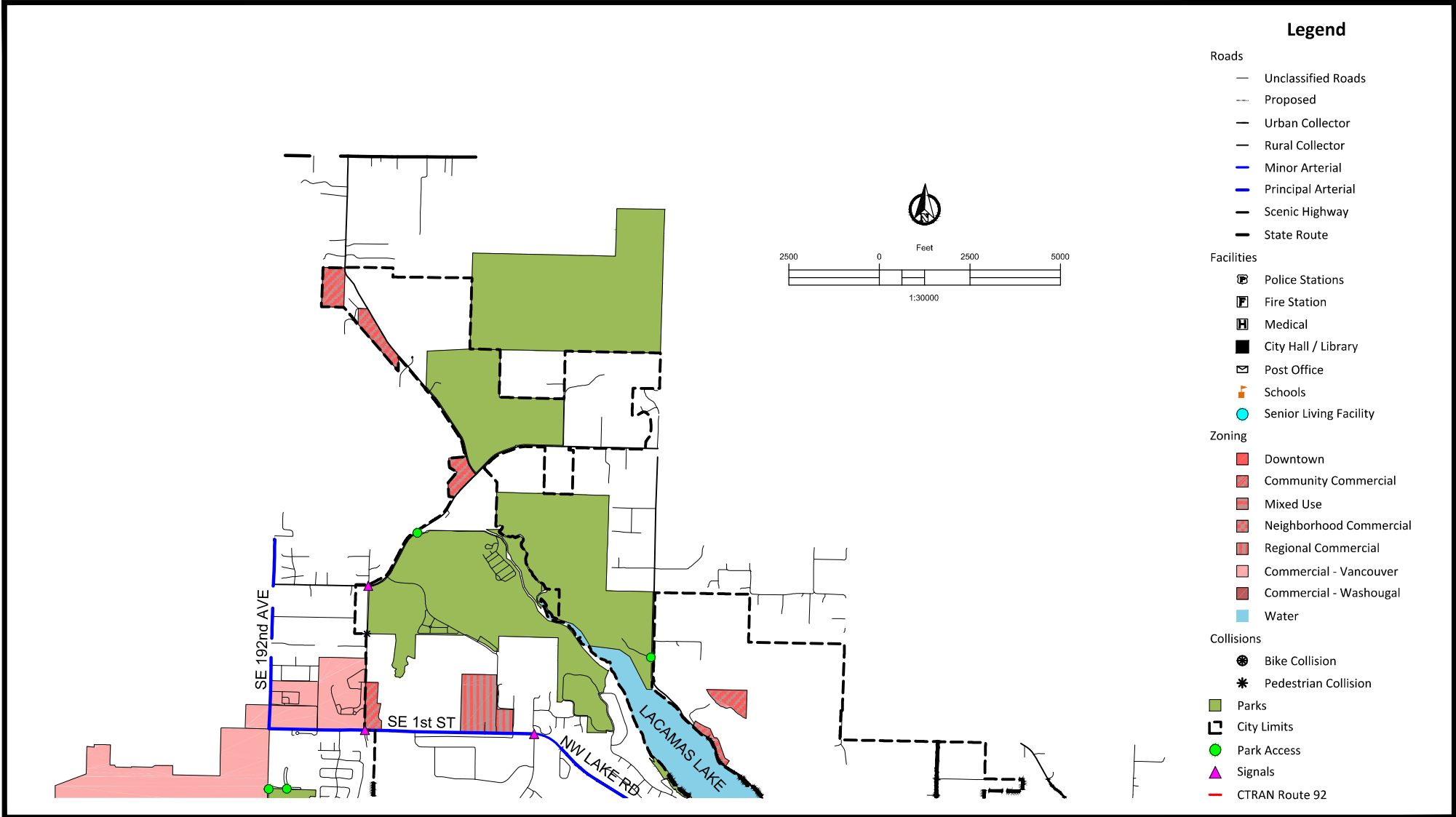


**OVERALL MAP FOR:  
CAMAS CITY-WIDE SELF EVALUTATION & ADA TRANSITION PLAN  
PRIORITIZATION CRITERIA**

**HDJ**  
DESIGN GROUP

314 W 15th Street  
Vancouver, WA 98660-2927  
360/695-3488  
503/824-4005  
360/695-8767 fax  
Internet: www.hdjg.com

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CHECKED: JAM	JOB NO.: 3804	4

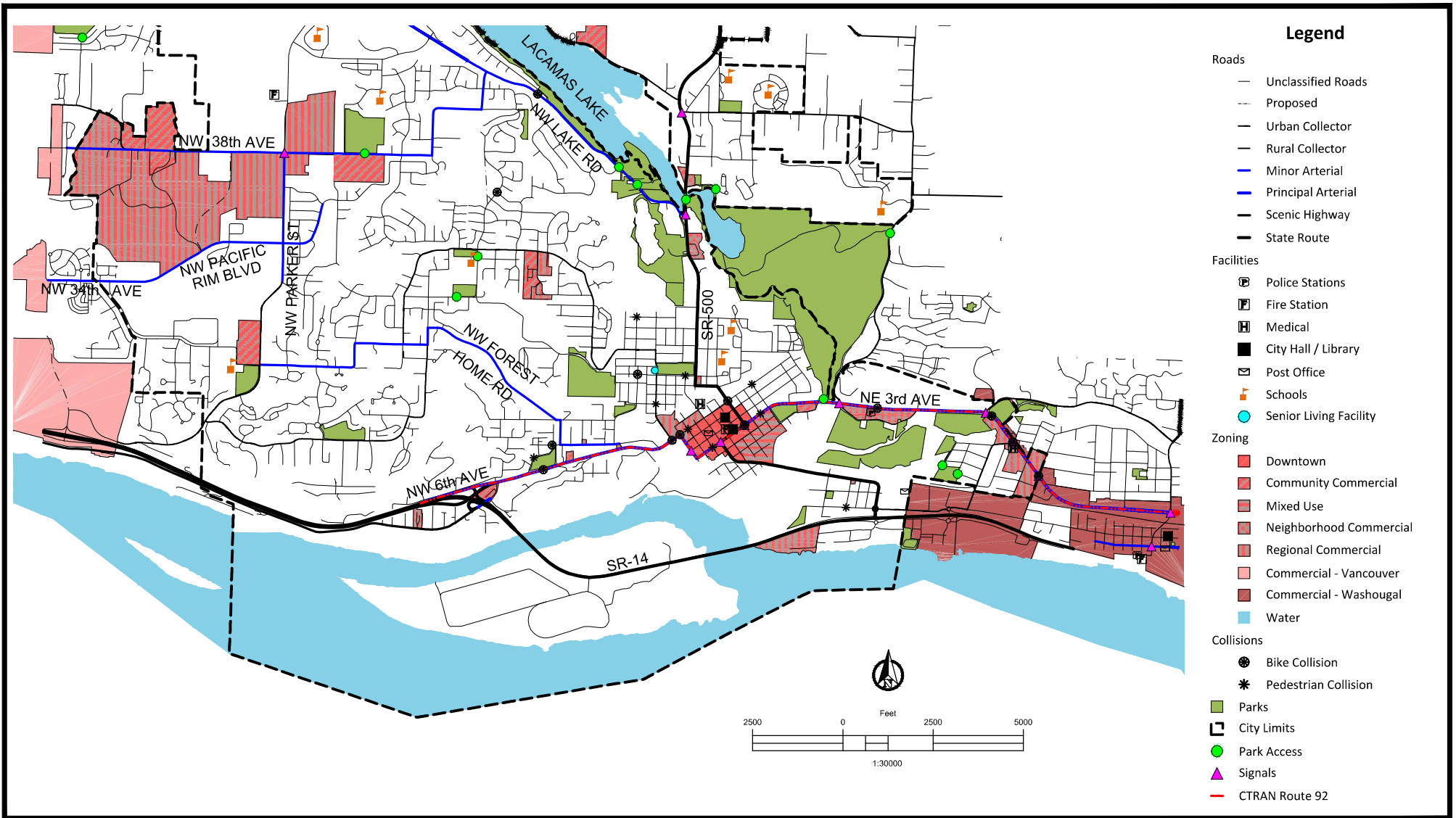


**NORTHEAST MAP FOR:  
CAMAS CITY-WIDE SELF EVALUTATION & ADA TRANSITION PLAN  
PRIORITIZATION CRITERIA**

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DESIGN GROUP

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360/695-3488  
503/824-4005  
360/695-8767 fax  
Internet: www.hdjg.com

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**SOUTHEAST MAP FOR:  
CAMAS CITY-WIDE SELF EVALUTATION & ADA TRANSITION PLAN  
PRIORITIZATION CRITERIA**

**HDJ**  
DESIGN GROUP

314 W 15th Street  
Vancouver, WA 98660-2927  
360/695-3488  
503/824-4005  
360/695-8767 fax  
Internet: www.hdjg.com

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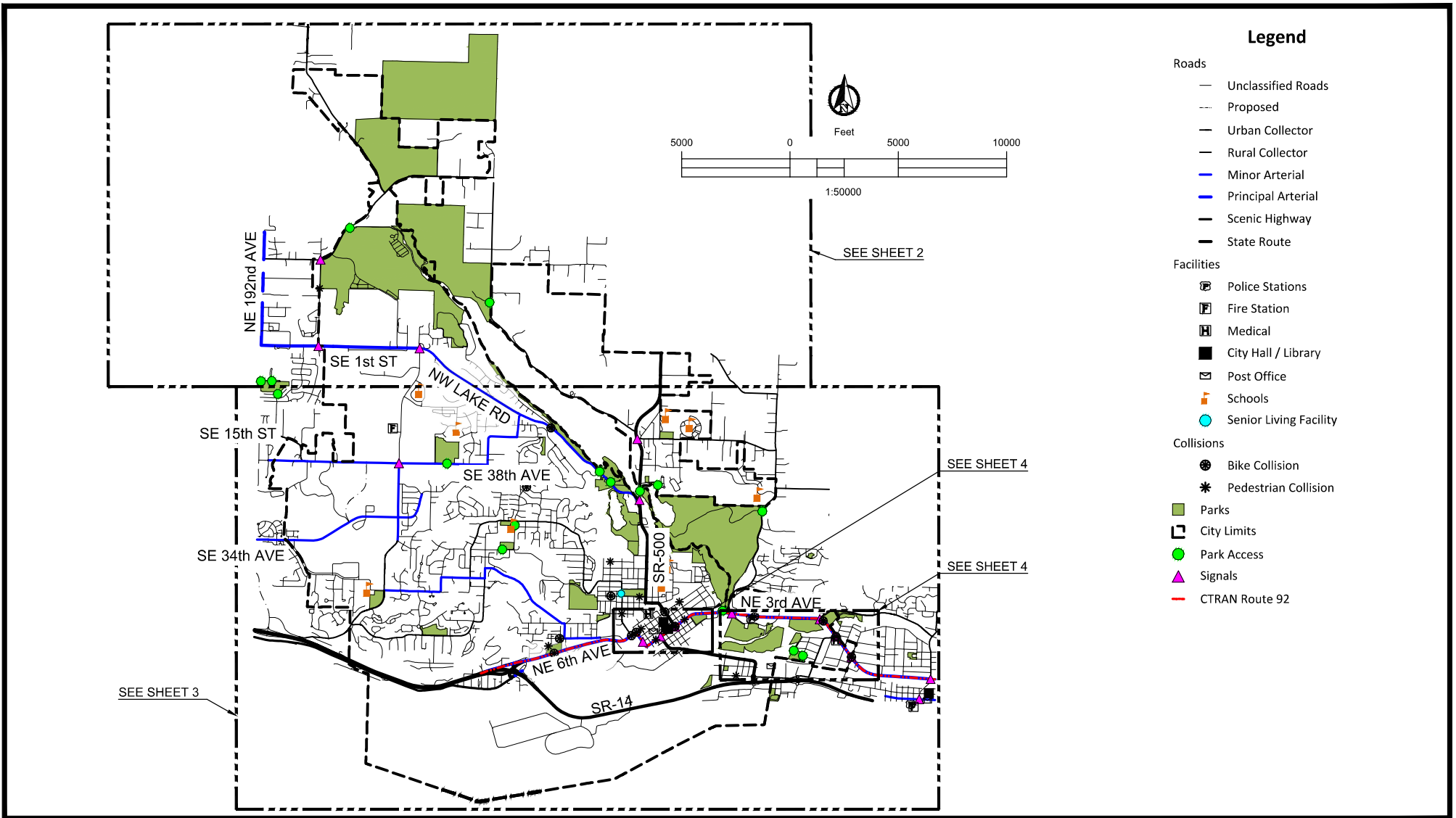


**DOWNTOWN MAP & NE 3rd AVE CORRIDOR FOR:  
CAMAS CITY-WIDE SELF EVALUTATION & ADA TRANSITION PLAN  
PRIORITIZATION CRITERIA**

**HDJ**  
DESIGN GROUP

314 W 15th Street  
Vancouver, WA 98660-2927  
360/695-3485  
503/824-4005  
360/695-8767 fax  
Internet: www.hdjg.com

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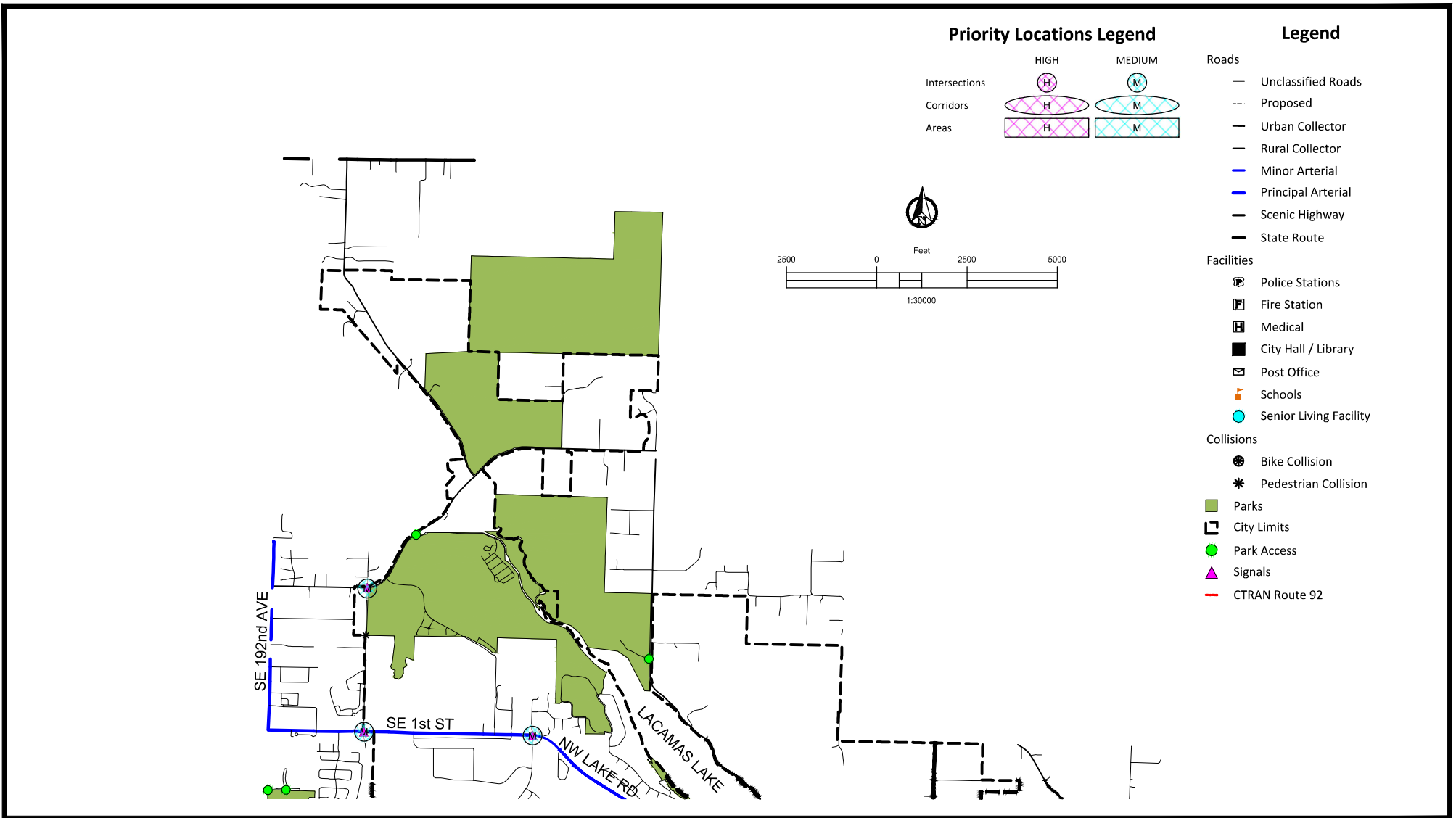
- Roads**
- Unclassified Roads
  - - - Proposed
  - Urban Collector
  - Rural Collector
  - Minor Arterial
  - Principal Arterial
  - Scenic Highway
  - State Route
- Facilities**
- ☎ Police Stations
  - 🚒 Fire Station
  - 🏥 Medical
  - 🏛 City Hall / Library
  - ✉ Post Office
  - 🎓 Schools
  - 👴 Senior Living Facility
- Collisions**
- 🚲 Bike Collision
  - ✳ Pedestrian Collision
- 🌳 Parks
  - 🏠 City Limits
  - 🟢 Park Access
  - 🚦 Signals
  - 🛣 CTRAN Route 92

**OVERALL MAP FOR:**  
**CAMAS CITY-WIDE SELF EVALUTATION & ADA TRANSITION PLAN**  
**HIGH & MEDIUM PRIORITY LOCATIONS**

**HDJ**  
**DESIGN GROUP**

314 W 15th Street  
 Vancouver, WA 98660-2927  
 360/695-3488  
 503/824-4005  
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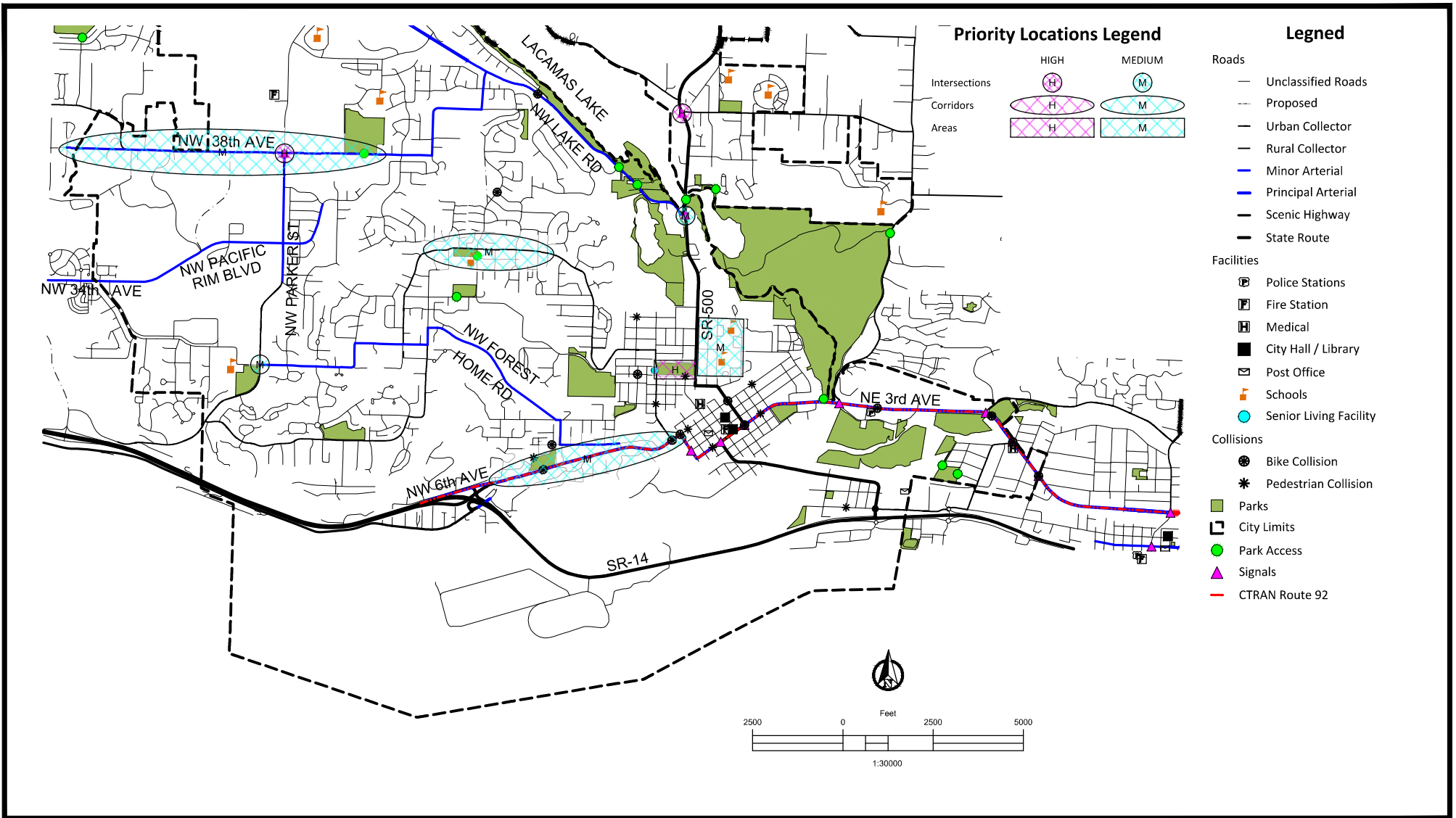


**NORTHEAST MAP FOR:  
CAMAS CITY-WIDE SELF EVALUTATION & ADA TRANSITION PLAN  
HIGH & MEDIUM PRIORITY LOCATIONS**

**HDJ**  
DESIGN GROUP

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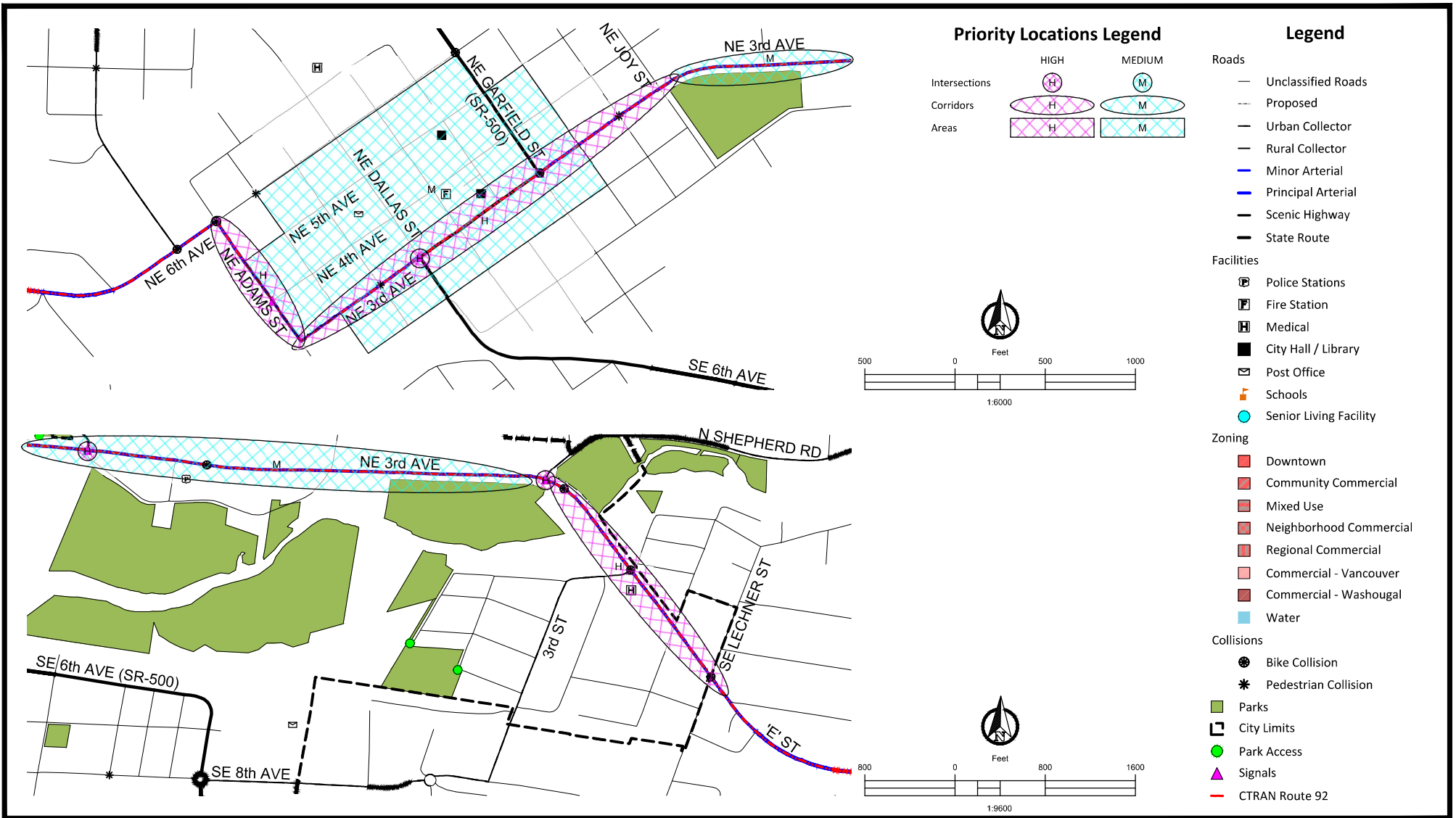


SOUTHEAST MAP FOR:  
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 HIGH & MEDIUM PRIORITY LOCATIONS

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**DOWNTOWN MAP & NE 3rd AVE CORRIDOR FOR:  
CAMAS CITY-WIDE SELF EVALUTATION & ADA TRANSITION PLAN  
HIGH & MEDIUM PRIORITY LOCATIONS**



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## Memo

**To:** Camas ADA Plan Community Advisory Committee  
**CC:** James Carothers, City of Camas  
**From:** John Manix, PE, HDJ; and Todd Boulanger, Urbane Streets  
**Dated:** 29 October 2014  
**Regarding:** CAMAS ADA PLAN: High & Medium Priority Locations by Type

### High and Medium Priority Locations for ADA Upgrades

Please review the attached list of locations and provide the project team with input on locations of high to moderate need that have not been included.

A major component of the ADA Plan is the Self-evaluation of the existing roadways. To facilitate this process, ranking criteria was proposed and discussed at our previous meeting. The next step is to focus the evaluation on locations that are the most important. The team has used the ranking criteria to propose locations that should be high to moderate need for ADA upgrades.

#### **SELF-EVALUATION PLAN RANKING CRITERIA**

The locations below are ranked based on criteria that are considered best practice by ADA planning experts but your input is critical to make sure the disabled community is best served in Camas. The action of ranking projects is a function of the following criteria:

- **Collision History** for pedestrians and bicyclists
- **Roadway Classification** arterials have higher traffic
- **Land-use Zoning** commercial has higher pedestrian traffic
- **Important Destinations** such as support or medical services and public offices for the disabled community.
- **Transit Service** is highly relied upon by the disabled community and
- **CAC Sites:** these locations have been identified through the CAC public process and added to the priority list based on local knowledge.

Pedestrian safety is an important consideration in ranking potential sites and can be done based on exposure to high volume and high-speed motor vehicle traffic. Typically the arterial roadway classification is a good starting point for judging the likely exposure above, as higher order arterials have more than collector or local streets. Thus high priority locations have most, if not all, criteria present while medium have less. And any public streets not listed below are considered low priority, as they generally have low traffic volumes in residential areas and often are without reported crash history. These street facilities also generally do not have transit service or public facilities serving persons with disabilities thus reducing their importance for this community wide planning process.

The priority locations are also organized by type (intersection, corridor, and area) for purposes of easier discussion in identification and treatment types. All facilities types are collectively equal within each priority tier. The type of location becomes important later when mitigations or treatments are proposed.

**HIGH PRIORITY PROJECT LOCATIONS – INTERSECTION:**

- NW 38<sup>th</sup> Ave. & NW Parker St. Intersection (Sheet 3)
- NE 43<sup>rd</sup> Ave. & SR-500/ Everett St. Intersection (Sheet 3)
- NE 3<sup>rd</sup> Ave. & NE Dallas St. Intersection (Sheet 4)
- NE 3<sup>rd</sup> Ave. & NE 3<sup>rd</sup> Loop / SE Crown Rd. Intersection (Sheet 4)
- NE 3<sup>rd</sup> Ave. & NW Shepherd Intersection (Sheet 4)

**HIGH PRIORITY PROJECT LOCATIONS – CORRIDOR:**

- NE Adams Corridor: NE 6<sup>th</sup> Ave. to NE 3<sup>rd</sup> Ave. (Sheet 4)
- NE 3<sup>rd</sup> Ave. Corridor: NE Adams St. to NE Joy St. (Sheet 4)
- NE 3<sup>rd</sup> Ave. Corridor: N Shepherd Rd. to SE Lechner St. (Sheet 4)

**HIGH PRIORITY PROJECT LOCATIONS – AREA:**

- Crown Park Neighborhood (Sheet 3)

**MEDIUM PRIORITY PROJECT LOCATIONS – INTERSECTION:**

- NE Goodwin Rd. & NW Friberg-Strunk St. Intersection (Sheet 2)
- SE 1<sup>st</sup> St & NW Friberg-Strunk St. Intersection (Sheet 2)
- NW Lake Rd. & NW Parker St. Intersection (Sheet 2)
- NW 16<sup>th</sup> Ave. & NW Brady Rd. Intersection (Sheet 3)
- NW Lake Rd. & SR-500/ Everett St. Intersection (Sheet 3)

**MEDIUM PRIORITY PROJECT LOCATIONS – CORRIDOR:**

- NW 38<sup>th</sup> Ave. Corridor: SE Bybee Rd. to NW Dahlia Dr. (Sheet 3)
- NE 28<sup>th</sup> Ave. Corridor: NW Valley St. to NW Kent St. (Sheet 3)
- NW 6<sup>th</sup> Ave. Corridor: NW Norwood St. to Division St. (Sheet 3)
- NE 3<sup>rd</sup> Ave. Corridor: NE Joy St. to E 1<sup>st</sup> Ave. (Sheet 4)
- NE 3<sup>rd</sup> Ave. Corridor: E 1<sup>st</sup> Ave. to NE Shepherd Rd. (Sheet 4)

**MEDIUM PRIORITY PROJECT LOCATIONS – AREA:**

- West Highland Neighborhood (Sheet 3)
- Downtown Core (Sheet 4)

**LOW PRIORITY PROJECT LOCATIONS – INTERSECTION / CORRIDOR / AREA:**

- All other public streets not listed due to either low demand and or low need.

## High and Medium Priority Intersections, Corridors, and Areas List

### HIGH PRIORITY PROJECT LOCATIONS - INTERSECTION:

- NE 43<sup>rd</sup> Ave. & SR-500/ Everett St. Intersection (Sheet 3)
- NE 3<sup>rd</sup> Ave. & NE Dallas St. Intersection (Sheet 4)
- NE 3<sup>rd</sup> Ave. & NE 3<sup>rd</sup> Loop / SE Crown Rd. Intersection (Sheet 4)
- NE 3<sup>rd</sup> Ave. & NW Shepherd Intersection (Sheet 4)
- NW 6<sup>th</sup> Ave. & NW Logan, Forest Home Park (Sheet 3)
- NW 6<sup>th</sup> Ave. & NW Ivy / NW 5<sup>th</sup>, Forest Home Park (Sheet 3)
- NW 6<sup>th</sup> Ave. & NW Ash (Sheet 3)

### HIGH PRIORITY PROJECT LOCATIONS - CORRIDOR:

- NE Adams Corridor: NE 6<sup>th</sup> Ave. to NE 3<sup>rd</sup> Ave. (Sheet 4)
- NE 3<sup>rd</sup> Ave. Corridor: NE Adams St. to NE Joy St. (Sheet 4)
- NE 5<sup>th</sup> Ave. Corridor: NE Adams St. to NE Garfield St. (Sheet 4)
- NE 4<sup>th</sup> Ave. Corridor: NE Adams St. to NE Garfield St. (Sheet 4)
- NE 3<sup>rd</sup> Ave. Corridor: N Shepherd Rd. to SE Lechner St. (Sheet 4)

### HIGH PRIORITY PROJECT LOCATIONS - AREA:

- Crown Park Neighborhood (Sheet 3)
- West Highland Neighborhood (Sheet 3)

### MEDIUM PRIORITY PROJECT LOCATIONS - INTERSECTION:

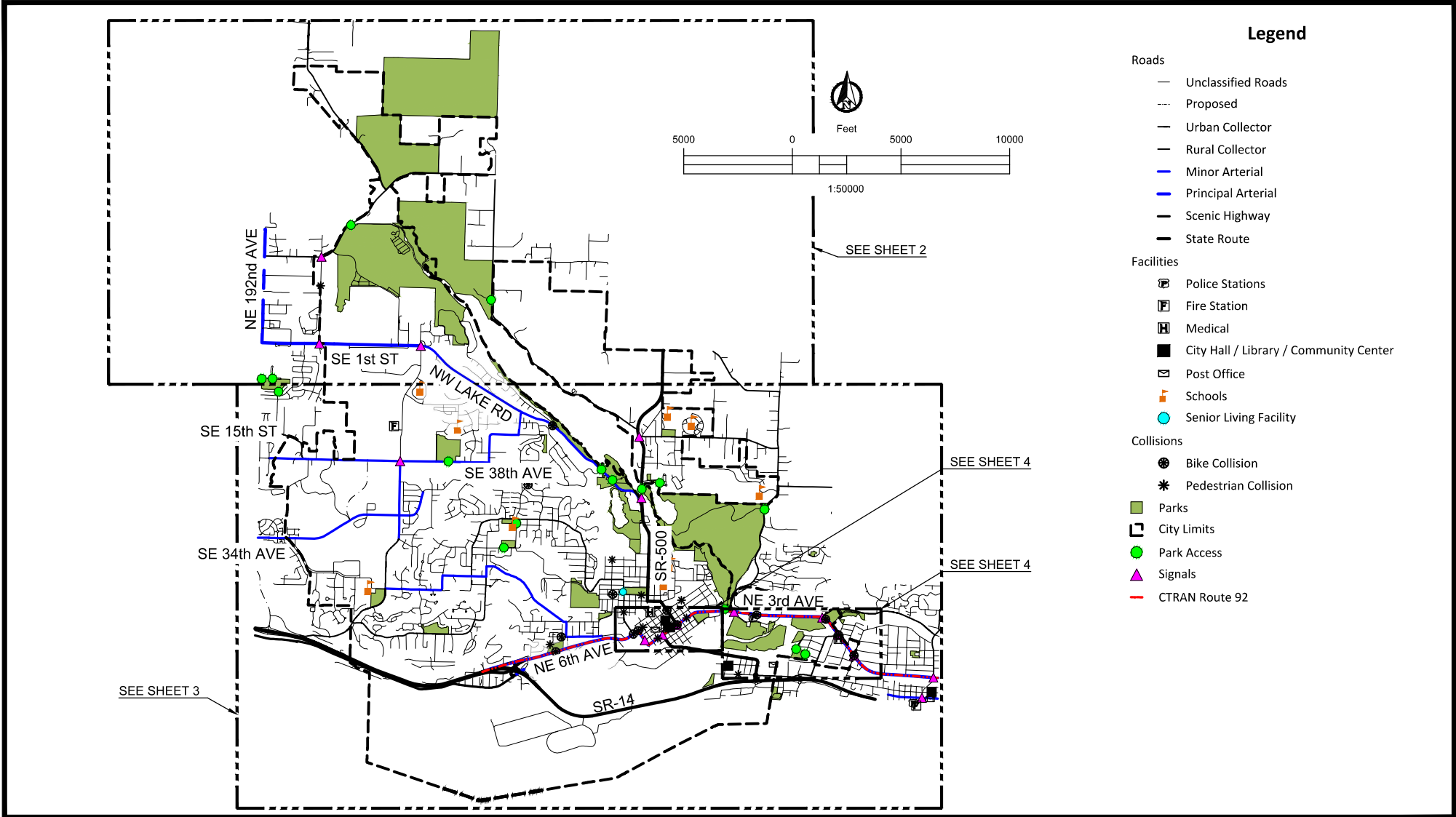
- SE 1<sup>st</sup> St & NW Friberg-Strunk St. Intersection (Sheet 2)
- NW Lake Rd. & SR-500/ Everett St. Intersection (Sheet 3)
- NE 2<sup>nd</sup> Ave. & Dallas St. Intersection (Sheet 4) – new [crossing enhancement]

### MEDIUM PRIORITY PROJECT LOCATIONS - CORRIDOR:

- NW 38<sup>th</sup> Ave. Corridor: SE Bybee Rd. to NW Dahlia Dr. (Sheet 3)
- NE 28<sup>th</sup> Ave. Corridor: NW Valley St. to NW Kent St. (Sheet 3)
- NW 6<sup>th</sup> Ave. Corridor: NW Norwood St. to NW Logan St. (Sheet 3)
- NW 6<sup>th</sup> Ave. Corridor: NW Logan St. to NW Ivy St. (Sheet 3)
- NW 6<sup>th</sup> Ave. Corridor: NW Ivy St. to NW Ash St. (Sheet 3)
- NW 6<sup>th</sup> Ave. Corridor: NW Ash St. to NW Adams St. (Sheet 3)
- NE 3<sup>rd</sup> Ave. Corridor: NE Joy St. to E 1<sup>st</sup> Ave. (Sheet 4)
- NE 3<sup>rd</sup> Ave. Corridor: E 1<sup>st</sup> Ave. to NE Shepherd Rd. (Sheet 4)
- Garfield St. / SR-500 Corridor: NE 6<sup>th</sup> Ave. to NE 3<sup>rd</sup> Ave. (Sheet 4)
- SR-500 Corridor South (NE Dallas St. / SE 6<sup>th</sup> St. / SE Union St.): NE 3<sup>rd</sup> Ave. to SE 8<sup>th</sup> Ave. (Sheet 4)

### MEDIUM PRIORITY PROJECT LOCATIONS - AREA:

- Downtown Core, areas not already included above (Sheet 4)
- Camas Community Center (Sheet 4)

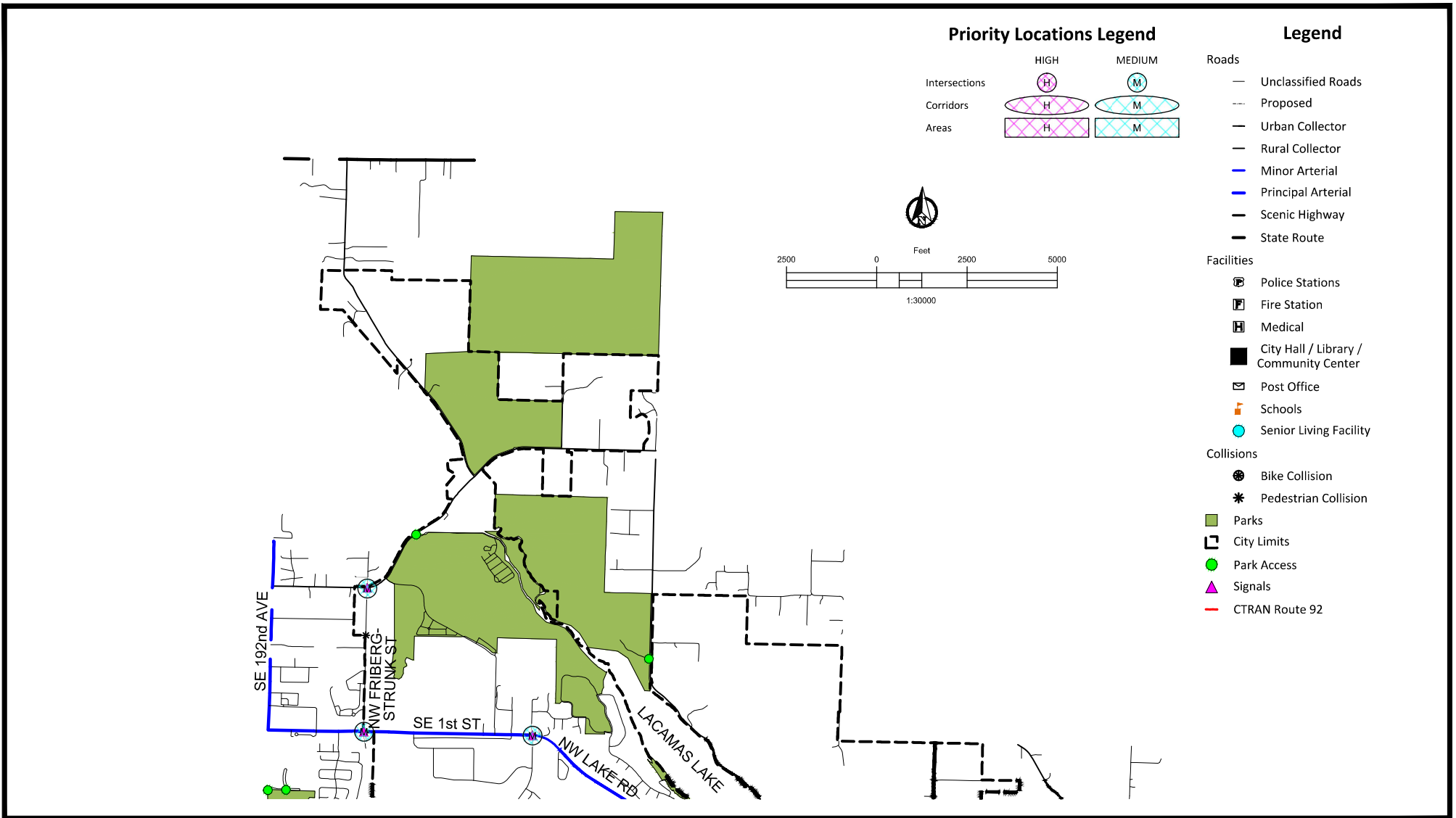


**OVERALL MAP FOR:**  
**CAMAS CITY-WIDE SELF EVALUTATION & ADA TRANSITION PLAN**  
**HIGH & MEDIUM PRIORITY LOCATIONS**

**HDJ**  
**DESIGN GROUP**

314 W 15th Street  
 Vancouver, WA 98660-2927  
 360/695-3485  
 503/824-4005  
 360/695-8767 fax  
 Internet: www.hdjg.com

DESIGNED: N/A	SCALE: H: N/A V: N/A	SHEET
DRAWN: MPA	DATE: DEC 2014	1
CHECKED: JAM	JOB NO.: 3804	4

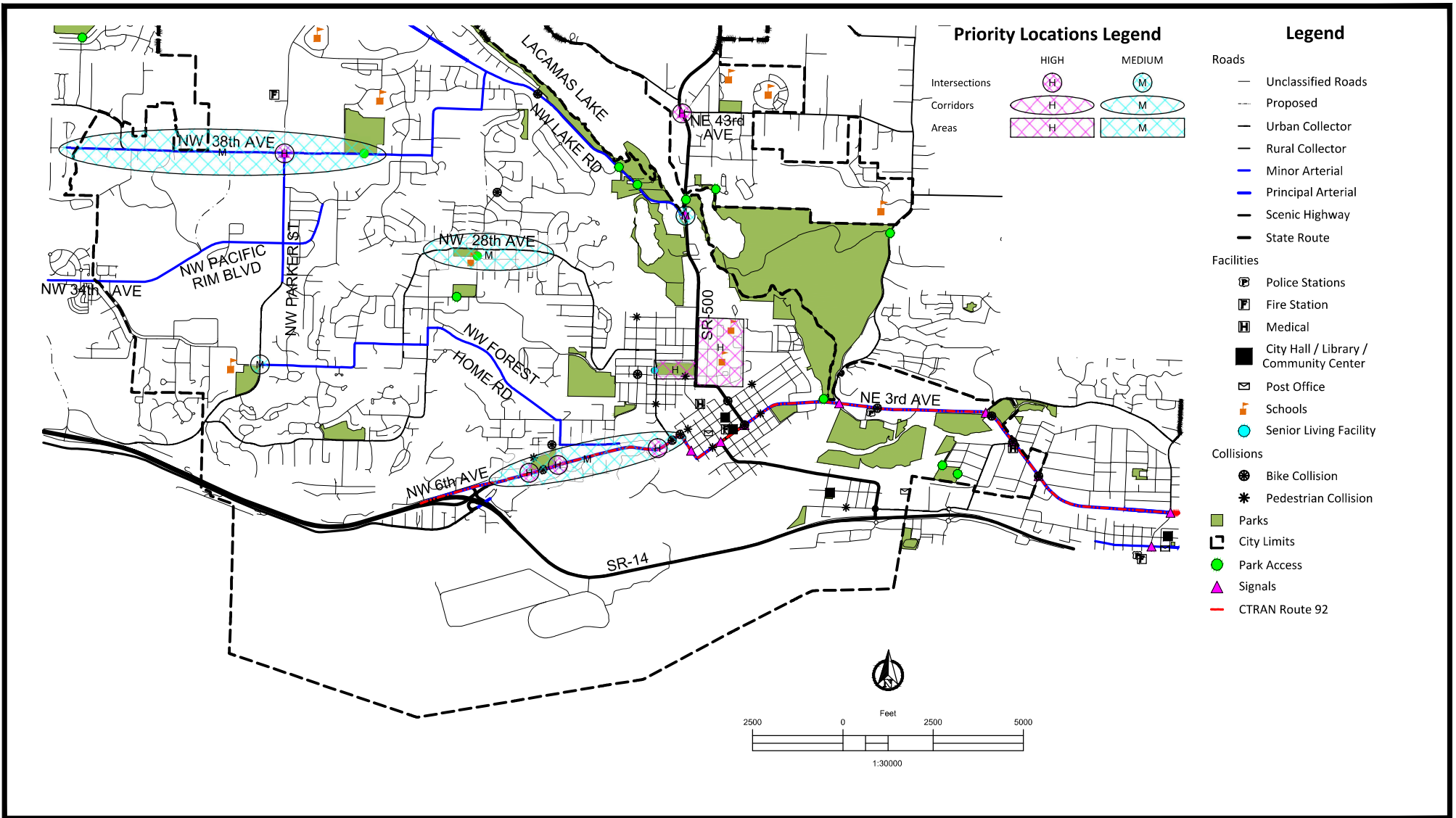


**NORTHEAST MAP FOR:  
CAMAS CITY-WIDE SELF EVALUTATION & ADA TRANSITION PLAN  
HIGH & MEDIUM PRIORITY LOCATIONS**

**HDJ**  
DESIGN GROUP

314 W 15th Street  
Vancouver, WA 98660-2927  
360/695-3485  
503/824-4005  
360/695-8767 fax  
Internet: www.hdjg.com

DESIGNED: N/A	SCALE: H: N/A V: N/A	SHEET
DRAWN: MPA	DATE: DEC 2014	2
CHECKED: JAM	JOB NO.: 3804	4

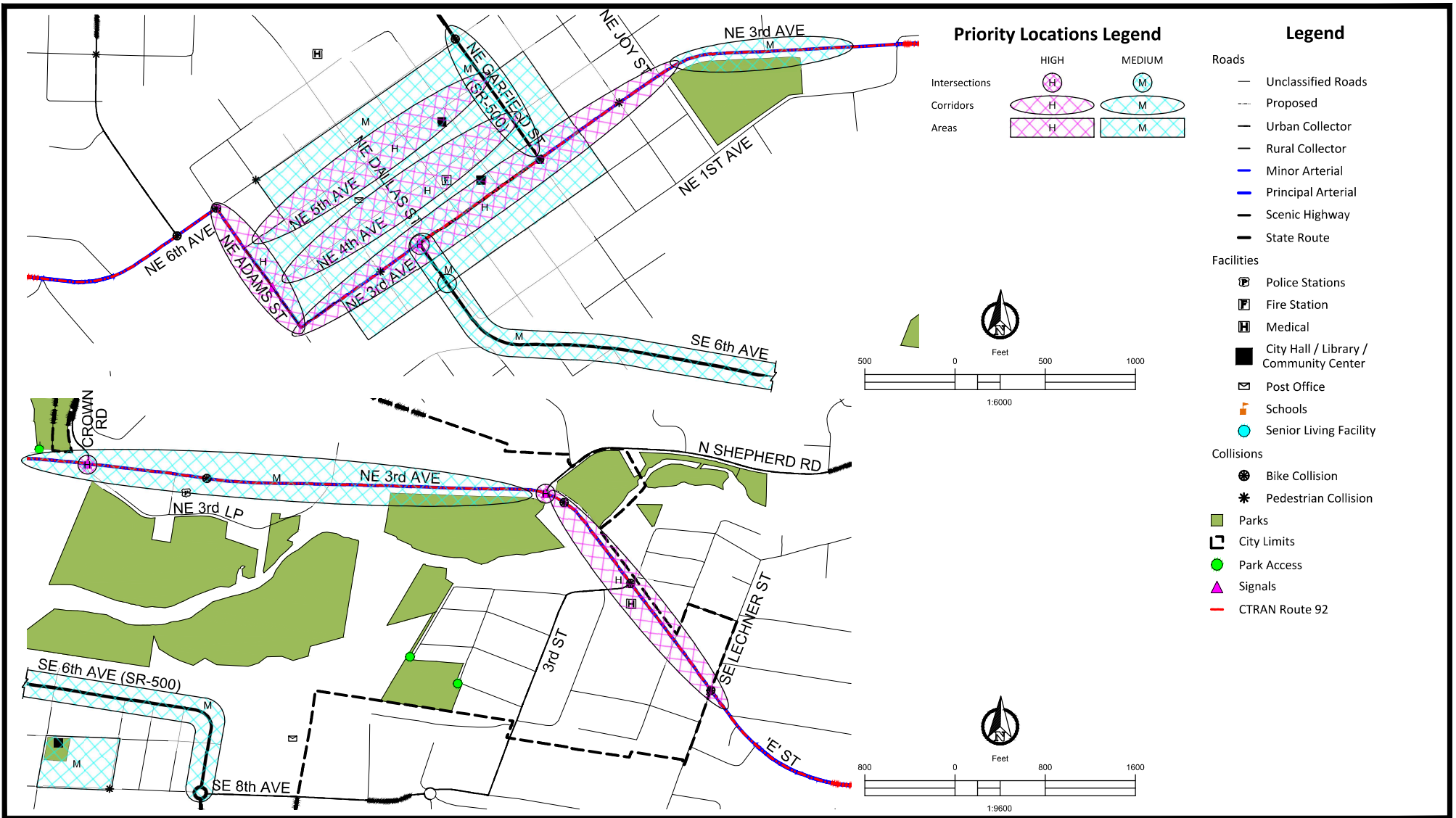


**SOUTHEAST MAP FOR:  
CAMAS CITY-WIDE SELF EVALUTATION & ADA TRANSITION PLAN  
HIGH & MEDIUM PRIORITY LOCATIONS**

**HDJ**  
DESIGN GROUP

314 W 15th Street  
Vancouver, WA 98660-2927  
360/695-9488  
503/824-4005  
360/695-8767 fax  
Internet: www.hdjg.com

DESIGNED: N/A	SCALE: H: N/A V: N/A	SHEET
DRAWN: MPA	DATE: DEC 2014	3
CHECKED: JAM	JOB NO.: 3804	4



**DOWNTOWN MAP & NE 3rd AVE CORRIDOR FOR:  
CAMAS CITY-WIDE SELF EVALUTATION & ADA TRANSITION PLAN  
HIGH & MEDIUM PRIORITY LOCATIONS**



314 W 15th Street  
Vancouver, WA 98660-2927  
360/695-9485  
503/824-4005  
360/695-8767 fax  
Internet: www.hdig.com

DESIGNED: N/A	SCALE: H: N/A V: N/A	SHEET: 4
DRAWN: MPA	DATE: DEC 2014	4
CHECKED: JAM	JOB NO.: 3804	4

## **Appendix C – ADA Project Schedule by Funding Source**

- Project Schedule with Local Funding
- Project Schedule with CDBG Grant Funding
- Project Schedule with State and Federal Grant Funding

Rank	Roadway	End Point 1	End point 2	Scope of Work	Construction Cost	Construction Year	Comments and funding source
1	Varies	NA	NA	Response to requests for ADA upgrades in the public	Varies	Annual	ADA Upgrades based on requests by City
4	Division St	NE 17th Ave	NE 15th Ave	Sidewalk repair on east side only	\$1,000	2015	Repair sidewalk on City park frontage by City
5	Everett St	NE 15th Ave	NE 17th Ave	Sidewalk repair on west side only	\$3,000	2016	Repair sidewalk on City park frontage by City
6	NE 15th Ave	Division St	Everett St	Sidewalk repair on north side only	\$5,000	2017	Repair sidewalk on City park frontage by City
7	NE 17th Ave	Everett St	Division St	Sidewalk repair on south side only	\$16,000	2018	Repair sidewalk on City park frontage by City
8	Division St	NE 17th Ave	NE 15th Ave	Sidewalk repair on west side only	\$9,000	2015	Repair sidewalk on by adjacent property owner
5	Everett St	NE 15th Ave	NE 17th Ave	Sidewalk repair on east side only	\$2,000	2016	Repair sidewalk by adjacent property owner
6	NE 15th Ave	Division St	Everett St	Sidewalk repair on south side only	\$13,200	2017	Repair sidewalk by adjacent property owner
7	NE 17th Ave	Everett St	Division St	Sidewalk repair on north side only	\$20,800	2018	Repair sidewalk by adjacent property owner
8	NE 3rd Ave	Adams St	Joy St	Sidewalk repair	\$8,500	2015	Repair sidewalk by adjacent property owner
10	Adams St	NE 3rd Ave	NE 6th Ave	Sidewalk repair	\$3,000	2017	Repair sidewalk by adjacent property owner
11	NE 4th Ave	Adams St	Joy St	Sidewalk repair	\$36,100	2018	Repair sidewalk by adjacent property owner
12	NE 5th Ave	Adams St	Joy St	Sidewalk repair	\$18,500	2019	Repair sidewalk by adjacent property owner
13	Garfield St	NE 15th Ave	19th St	Sidewalk repair	\$3,000	2019	Repair sidewalk by adjacent property owner
14	Division St	NE 17th Ave	NE 15th Ave	Minor ADA ramp upgrades	\$7,500	2020	Add warning pattern and or eliminate excesslive lip.

Rank	Roadway	End Point 1	End point 2	Scope of Work	Construction Cost	Construction Year	Comments and funding source
15	Everett St	NE 15th Ave	NE 17th Ave	Minor ADA ramp upgrades	\$5,000	2021	Add warning pattern and or eliminate excesslive lip.
16	NE 15th Ave	Adams St	Dallas St	Minor ADA ramp upgrades	\$7,000	2022	Add warning pattern and or eliminate excesslive lip.
16	NE 3rd Ave	Adams St	Joy St	Minor ADA ramp upgrades	\$13,000	2023	Add warning pattern and or eliminate excesslive lip.
17	Adams St	NE 3rd Ave	NE 6th Ave	Minor ADA ramp upgrades	\$4,000	2024	Add warning pattern and or eliminate excesslive lip.
18	NE 4th Ave	Adams St	Joy St	Minor ADA ramp upgrades	\$22,000	2025	Add warning pattern and or eliminate excesslive lip.
18	NE 5th Ave	Adams St	Joy St	Minor ADA ramp upgrades	\$16,500	2026	Add warning pattern and or eliminate excesslive lip.
19	Garfield St	NE 15th Ave	19th St	Minor ADA ramp upgrades	\$2,500	2027	Add warning pattern and or eliminate excesslive lip.
20	NE 22nd Ave	Garfield St	Ione St	Minor ADA ramp upgrades	\$1,500	2028	Add warning pattern and or eliminate excesslive lip.

Rank	Roadway	End Point 1	End point 2	Scope of Work	Construction Cost	Construction Year	Comments and funding source
1	NE 3rd Ave	Adams St	Joy St	New ADA Ramps only	\$62,000	2016	Replace full height curbs with ADA ramps that qualify for CDBG funding
5	Adams St	NE 3rd Ave	NE 6th Ave	New ADA Ramps only	\$10,000	2020	Replace full height curbs with ADA ramps that qualify for CDBG funding

Rank	Roadway	End Point 1	End point 2	Scope of Work	Construction Cost	Construction Year	Comments and funding source
1	NE 3rd Ave	Adams	Lechner	ADA work to include signal and ramp replacement. Does not include minor upgrades at substancially compliant ADA ramps	\$110,000	2019	Combine ADA upgrades with traffic, pedestrian and bicycle safety improvements. The Federal City Safety Grant progam is likely source of funding. <b>No minor upgrades to existing curb ramps.</b>
2	NA	NA	NA	City-wide traffic signal APS upgrades	\$100,000	2020	Combine with pedestrian and traffic safety improvements at traffic signals. The Federal City Safety Grant progam is likely source of funding. <b>No minor upgrades to existing curb ramps.</b>
3	NA	NA	NA	Crown Park Area improvements	\$120,000	2022	Sidewalk repaired. Combine with sidewalk infill as part of grant application. The Federal Transportation Aternative Progam (TAP) grant is likely source of funding. <b>No minor upgrades to existing curb ramps.</b>
4	NA	NA	NA	Highlands Area improvements	\$46,000	2024	Combine ADA upgrades with sidewalk infill, traffic, pedestrian and bicycle safety improvements. The Safe Route to School program that is part of the Federal Transportation Aternative Progam (TAP) grant is likely source of funding. <b>No minor upgrades to existing curb ramps.</b>
5	NW 6th St	Adams St	Norwood St	High Priority Intersections	\$28,000	2026	Combine ADA upgrades with traffic, pedestrian and bicycle safety improvements. The State pedestrian and bicycle grant program is a likely good source of funding. <b>No minor upgrades to existing curb ramps.</b>



## Memorandum

DATE: February 26, 2026  
TO: Curleigh (Jim) Carothers  
FROM: John Manix, PE  
PROJECT: 24011088  
REGARDING: 2025 Updated Self Evaluation Plan

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### INTRODUCTION

The 2025 Updated Self-Evaluation Plan updates the original Self-Evaluation Plan prepared in 2015 (see Appendix J). The 2025 Updated Self-Evaluation Plan informs the 2025 Updated ADA Transition Plan, which provides a list of public improvements needed to eliminate barriers within the public right-of-way, identifies the estimated costs of those improvements, and establishes a schedule for their implementation. Both the Self-Evaluation Plan and the ADA Transition Plan are required to be updated on a regular basis.

The Washington State Department of Transportation (WSDOT) Local Programs Office informed City staff that the City of Camas would not qualify for grant funding for transportation improvements without completing the 2025 updates to the Self-Evaluation and ADA Transition Plans. See Appendix A for the WSDOT-supplied Non-State Jurisdiction ADA Transition Plan Review Rubric and Rating Guide (WSDOT Rubrics).

### ADA Self Evaluation and ADA Transition Plan Requirements

The following categories are listed in the WSDOT Rubrics:

1. Official Responsible for Implementation of ADA Transition Plan – See 2015 Self Evaluation Plan
2. Inventory of Barriers – See 2025 Self Evaluation Plan
3. Modification Schedule – See 2025 Updated ADA Transition Plan
4. Accessibility Methodology
  - a. Self-Evaluation Plan Facilities – See 2025 Self Evaluation Plan for public right-of-way elements inventoried and assessed using the ADA Accessibility Guidelines (ADAAG) and the Public Rights-of-Way Access Guidelines (PROWAG) technical criteria. Barriers will be documented, mapped, and prioritized.
  - b. Updated Transition Plan Barriers – See 2025 Updated ADA Transition Plan for ranked improvements based on safety, public use, access to government services, and community input. Improvements will be phased and integrated into capital improvement and resurfacing projects.
  - c. Design and Construction Controls – See 2015 Self Evaluation Plan for standard plan compliance and policy for review during design, verified during construction, and confirmed prior to project closeout through field measurements and inspection.

- d. **Maximum Extent Feasible** – See 2015 Self Evaluation Plan where structural, right-of-way, or topographic constraints prevent full compliance, improvements will meet standards to the maximum extent feasible. Technical infeasibility will be documented and alternative accessible solutions provided where necessary.
5. **Public Involvement** – See 2025 Self Evaluation Plan
6. **ADA Policy Statement** – See 2015 Self Evaluation Plan
7. **Complaint/Request/Grievance Process** – See 2015 Self Evaluation Plan

This 2025 Updated Self Evaluation Plan focuses on two important components that inform the 2025 Updated ADA Transition Plan: public involvement and inventory of the public right-of-way.

## **PUBLIC INVOLVEMENT PROCESS**

Public involvement was a central component of the 2025 ADA Transition Plan update and played a key role in shaping the priorities and recommendations included in this document. Significant time and resources were dedicated to involving the public throughout the process to ensure that the plan reflects community members' current priorities and needs. Outreach activities provided multiple opportunities for people with disabilities, advocates, service providers, and community members to share their experiences, identify barriers, and help prioritize improvements. The input received through this process directly informed project recommendations and helps ensure the plan promotes inclusivity for all users throughout the city of Camas.

### **Public Outreach**

Public outreach for this effort began early in the process to support the citywide ADA Plan update. Outreach activities began with the launch of Engage Camas project page content (<https://engagecamas.com/ada-plan-updates-2025>) and social media posts inviting community members and advocates to serve on the Community Advisory Committee (CAC).

To engage the broader community, we sent a postcard mailer to all Camas residents. It informed them of the project, the project webpage, the chance to serve on the CAC, and the online virtual open house and community survey. Those interested in participating in the CAC were invited to three sessions, which allowed for more in-depth discussion and collaboration.

These engagement efforts provided multiple accessible opportunities for community members to learn about the project, share their experiences, ask questions, and provide meaningful feedback on accessibility needs and priorities throughout the City of Camas. Input received through three CAC sessions and the community survey informed the development of the Self-Evaluation Plan and contributed to shaping a more inclusive, connected, and accessible Camas for all users.

### **Agencies & Service Providers**

In addition to broad public outreach, the City conducted targeted outreach to organizations that represent or serve people with disabilities and other community stakeholders. Their involvement in the CAC provided valuable insight into accessibility needs, barriers, and priorities across the community, from students to seniors. The following agencies and organizations were contacted to participate in the project.

- Community members with disabilities
- Community members of Camas
- C-TRAN

- Washington State Department of Transportation
- Clark County Community Services – Developmental Disabilities Program
- Northwest ADA Center
- People First of Clark County
- Lacamas Creek Post Acute assisted living facility
- Faith-based organizations
- Vancouver Housing Authority
- Washington State Department of Social and Health Services – Division of Vocational Rehabilitation
- Washington State School for the Blind
- Washington School for the Deaf
- Camas School District
- Camas School District special education leaders
- Camas School District Special Education Parent/Teacher/Student Organization

These entities were all approached during the outreach process to participate in the CAC and provide actionable feedback on barriers within the public right-of-way and recommendations to enhance Camas' accessibility. The organizations were invited to share our efforts through their own communications channels, and the Downtown Camas Association, for one, utilized its community newsletter to publicize the community survey.

#### **Process Kick-Off Meeting with City of Camas Staff**

A project kick-off meeting was held virtually with City staff on March 17, 2025, to introduce the Self-Evaluation Plan and ADA Transition Plan update. The meeting focused on reviewing the project goals and objectives and presenting the proposed Public Involvement Plan. City staff were invited to provide input on the outreach approach, confirm project roles and responsibilities, and discuss the overall project schedule, helping to ensure a coordinated and effective planning process.

#### **ADA Community Advisory Committee for the Public Right of Way**

Three CAC meetings were held at Camas City Hall, which was selected as a centrally located and accessible meeting place for all participants. For those unable to attend in person, a virtual participation option was also provided. Together, these approaches helped reduce barriers for participation and ensured consistent involvement throughout the public involvement process.

To maximize participation, online surveys were distributed to CAC members several weeks in advance to identify preferred meeting dates and times, and a dedicated City of Camas Public Works representative managed invites, correspondence, and materials. Title VI forms were distributed at every session, and materials were made available to committee members after each session via Engage Camas, where an update for the broader community was also posted.

#### **Community Outreach**

Community outreach was a key element of the ADA Plan update and was designed to reach a broad and diverse audience using multiple communication methods. The City used a combination of direct mail, online engagement, social media, and partnerships with local organizations to raise awareness of this plan update and encourage participation. These outreach efforts provided accessible opportunities for community members to learn about the ADA Plan update and share input and included the following:

- Community-wide mailer
- EngageCamas.com webpage

- City social media
- Downtown Camas Association social media and e-newsletter
- Email to CAC members and organizations
- Flyers at City Hall and Camas Library
- Scribble maps
- Online surveys
- Virtual open house

These outreach efforts were highly successful and resulted in meaningful participation from the community. The input received helped clearly identify priority focus areas, including downtown Camas and City parks, and directly informed the development of project recommendations and implementation strategies. Participants then had the opportunity to attend in-person CAC meetings described below.

### **CAC Meeting #1 Findings – June 27, 2025**

The first CAC meeting was designed to introduce the project scope, including the types of data to be collected, potential barrier removal improvements, planned community outreach efforts, and examples of ADA-compliant curb ramps. CAC members were invited to share their interest in the project, identify Camas destinations that are most important to them, and discuss accessibility barriers they have experienced throughout the City of Camas. This initial discussion helped establish a shared understanding of community priorities and provided valuable context for the self-evaluation process. Feedback received during the meeting informed the identification of key focus areas and reinforced the importance of addressing both physical barriers and user experience to improve accessibility for all community members.

### **CAC Meeting #2 Findings – September 18, 2025**

Prior to the second CAC meeting, an online survey was distributed to all participants to gather background information and better understand accessibility needs within Camas. The survey asked participants to share information about themselves, whether they experience mobility or accessibility challenges, how they travel throughout the City, and how they would rank curb ramps, traffic signal push buttons, and sidewalks within Camas. One of the most critical components of the survey asked participants to identify the destinations and streets that are most important to them. Responses to these questions directly informed the prioritization of focus areas for the 2025 ADA Transition Plan update. See Appendix B for survey responses.

The second CAC meeting built upon these survey results and focused on reviewing the feedback and identifying high-priority locations for accessibility improvements. Survey responses consistently identified downtown Camas and City parks as top priorities. During the meeting, CAC members reinforced the importance of improving park access points and accessibility within downtown Camas. Additional discussion highlighted challenges related to locating accessible parking downtown and the need for clearer, more consistent accessibility policies for City-sponsored events. Input from this meeting helped refine priority locations and strengthened the City's understanding of community-identified accessibility needs.

### **CAC Meeting #3 Findings – December 4, 2025**

The final CAC meeting focused on confirming the City's high-priority accessibility areas and reviewing the results of the scribble map exercise. Prior to the meeting, CAC members were provided with scribble maps of downtown Camas and asked to use symbols to identify key travel routes, rank curb ramps, and highlight specific accessibility barriers. This interactive activity allowed members to visually document areas of concern and prioritize

improvements based on lived experience. The results identified Birch Street, Cedar Street, Dallas Street, and Everett Street—between 3rd Avenue and 6th Avenue—as primary areas of concern.

In addition to the meeting discussion, CAC members were invited to submit completed scribble maps to further capture curb ramp and sidewalk conditions. See Appendix C for completed scribble maps. The intent of this exercise was to provide members with a direct and accessible way to indicate where barriers should be addressed within the downtown sidewalk network. Three members participated and the following findings were derived:

- Sidewalk repair is a high priority, particularly in areas with uplifted or uneven panels that create tripping hazards.
- Support for implementing the proposed low-cost improvements at existing curb ramps.
- The need to maintain an accessible pedestrian path on sidewalks. This may be supported through outreach and education to downtown merchants to ensure a minimum of four feet of clear sidewalk width is consistently maintained.

A draft list of prioritized projects was also presented at the meeting. This list was developed using input gathered from previous CAC sessions, the community survey, and the scribble map exercise. Based on this combined feedback, downtown Camas was identified as the highest-priority area for accessibility improvements. Recommended actions include replacing high- and medium-priority curb ramps or implementing appropriate low-cost improvements, addressing sidewalk tripping hazards through targeted grinding, and upgrading park access points with ADA-compliant improvements on streets located within one-quarter mile of park entrances.

### **Conclusion**

The public involvement process for the 2025 ADA Plan update demonstrates the City of Camas' commitment to connectivity and inclusivity. Through early outreach, meaningful engagement with the CAC, coordination with agencies, and community participation, the City gathered valuable input that directly shaped project prioritization and implementation strategies. All CAC summary meeting notes can be seen in Appendix D.

The feedback received throughout this process reinforced the importance of improving accessibility in high priority destinations such as downtown and city parks. See Appendix E for a citywide community feedback map illustrating important destinations mentioned by the community throughout the public involvement process. By incorporating community-identified priorities into the updated ADA Transition Plan, the City is taking deliberate steps toward creating a more accessible and welcoming Camas for all users. In the downtown, CAC members endorsed the low-cost ramp upgrades in the downtown and cited the importance of midblock sidewalk repair of uplifted sidewalk.

There was a concern regarding the availability of accessible parking in the downtown area, particularly during busy community events during CAC meeting #2. Apex acknowledges the desire for additional accessible on-street parking spaces to better serve event-related demand. A City-provided downtown parking diagram can be seen in Appendix F to show existing accessible parking inventory and support future evaluation. Additional study and coordination will be necessary to assess existing supply, location, event conditions, and opportunities for expanded accessible parking spaces.

## INVENTORY

### Introduction

This inventory memorandum serves as an update to the 2015 Camas ADA Transition Plan. All other regulatory requirements, definitions of important locations, and applicable standards remain consistent with the 2015 ADA Plan.

The City of Camas provided a curb ramp inventory dataset documenting the location, physical characteristics, and ADA compliance status of curb ramps within the city limits. This inventory was analyzed using GIS-based prioritization criteria to identify the ramps most in need of enhancement or replacement. As the dataset was last updated in 2020, a combination of field verification, aerial imagery review, and community feedback was used to validate and refine the prioritization results.

### Summary of Inventory

The GIS inventory received identifies key characteristics of intersection ramps and indicates whether individual elements comply with Americans with Disabilities Act (ADA) guidelines. These attributes were evaluated using a weighted scoring approach to identify ramps with the greatest accessibility deficiencies and to support prioritization for enhancement or replacement. See Appendix G for a memo describing the coding logic. The attributes included in the City's spreadsheet and used in this evaluation are explained below.

- Ramp width (Ramp\_Width)
- Ramp location (Latitude/Longitude)
- Presence of warning surface (Ramp\_Warni)
- Warning surface type (Warning\_ty)
- Presence of compliant landing (Landing\_on)
- Ramp slope (Ramp\_Slope)
- Presence and height of a lip (Land\_Drop)
- Safe Harbor designation (SafeHarbor)

The GIS-based prioritization criteria provided a structured method for evaluating curb ramp conditions across the city. However, the analysis presented challenges in distinguishing the most critical locations, as multiple noncompliant ramps were recently updated after the data was collected. As a result, a relatively large number of ramps were initially categorized as high priority for replacement based solely on scoring outputs. To better differentiate the data, the results were redefined into five defined priority categories:

- **ADA Compliant:** Ramps that meet current ADA guidelines.
- **Safe Harbor:** Ramps that meet 1991 ADA standards and do not require replacement unless altered.
- **High Priority:** Ramps most in need of enhancement or replacement.
- **Medium Priority:** Ramps require improvements, but not in critical condition.
- **Low Priority:** Ramps meeting some ADA guidelines, but requiring minor enhancements.

A citywide map illustrating ramp priorities is provided in Appendix H.

### **Limitations**

The inventory data were collected in 2020. Since that time, several curb ramp improvement projects have been completed between 2020 and 2025, resulting in changes to ramp conditions that are not reflected in the original dataset. This created limitations for the prioritization process, as certain inventory attributes no longer represent current field conditions.

Community feedback surveys indicated a strong concentration of accessibility concerns in downtown Camas. To address these limitations, a field visit was conducted on November 24, 2025, focusing on downtown curb ramps. The field review relied on professional engineering judgment, familiarity with ADA accessibility guidelines, and comparison with the inventory data to assess existing conditions and confirm or revise priority rankings.

In addition to downtown locations, the Apex team reviewed sites identified as important through community surveys, including neighborhood parks, high schools, middle schools, regional parks, and lake access areas. Medical offices and signalized intersections were also mapped and considered for consistency with the City's 2015 ADA Transition Plan and based on familiarity with the study area.

This evaluation is intended to support planning-level prioritization and does not replace a comprehensive field ADA compliance audit.

### **Findings**

Review of post-2020 improvements confirmed that portions of the original inventory no longer reflected existing conditions, particularly in the downtown area. As a result:

- Several ramps previously classified as high priority were determined to have been upgraded and were reclassified accordingly.
- Inventory-based scoring alone was insufficient in some locations without field confirmation.
- Community input provided valuable context in identifying areas where improvements were still needed.

These findings demonstrate the importance of supplementing GIS-based prioritizing approach with field verification and engineering judgment to ensure that prioritization reflects current conditions.

### **Evaluation Process**

Following the downtown field audit, several curb ramps had their priority rankings adjusted to reflect recent upgrades, particularly along 3rd Avenue in downtown Camas. Based on these updates, Apex evaluated appropriate treatments to bring ramps into ADA compliance or reduce high-priority ramps to a lower priority category through interim improvements.

Signalized intersections were evaluated primarily through aerial review to assess the presence of ADA-compliant pedestrian pushbuttons. The prioritization methodology for pushbutton replacements, consistent with Manual on Uniform Traffic Control Devices (MUTCD) requirements, is documented in the Push-Button Inventory Memorandum attached to the 2025 ADA Transition Plan. Projects identified through this process are allocated to the 20-Year Comprehensive Plan horizon.

For other locations identified through community surveys, aerial imagery was used to assess and verify inventory data where field visits were not feasible. See the 2025 ADA Transition Plan Parks Memorandum for park maps

referenced below. Using engineering judgment, several parks and lake access areas were selected as case studies for more detailed evaluation:

- Grass Valley Park
- Crown Park
- Forest Home Park
- Dorothy Fox Park
- Lacamas Lake, Fallen Leaf Lake, and Round Lake

As shown in the park maps included in Appendix A in the 2025 ADA Transition Plan, a 0.25-mile buffer was established around each park access point to evaluate surrounding curb ramps that support pedestrian connectivity within the public right-of-way. Only ramps initially classified as high priority were selected for further review. These ramps were evaluated using aerial imagery and professional engineering judgment to determine whether they should be prioritized for full replacement within either the 6-Year TIP or the 20-Year Comprehensive Plan horizon, depending on project priority.

Appendix I includes maps specific to the lake park locations (Lacamas Lake, Fallen Leaf Lake, and Round Lake) to document that curb ramps within the public right-of-way surrounding these areas do not exhibit high-priority characteristics. Community survey comments referencing ramps in these areas likely pertain to trailhead facilities or ramps outside the public right-of-way maintained by the City, which are beyond the scope of this evaluation. All other park maps are included in Appendix A in the 2025 ADA Transition Plan.

Apex also reviewed the 2015 Plan project list to determine which projects in the high- and medium-priority locations were completed and which remain outstanding. Incomplete projects from the 2015 Plan have been incorporated into the 20-Year Comprehensive Plan horizon, with associated cost estimates provided in the “2015 Incomplete Projects” memorandum attached to the 2025 ADA Transition Plan.

### **Findings**

Following the evaluation process, the updates to downtown curb ramps, particularly along 3rd Avenue in downtown Camas, informed the identification of appropriate improvement actions. In the downtown area, low-cost interim improvements, such as lip grinding and installation of truncated domes, were assigned to the 6-Year Transportation Improvement Plan (TIP) horizon, while full ramp replacements were programmed within the 20-Year Comprehensive Plan (Comp Plan) horizon to achieve ADA compliance.

Signalized intersections were also reviewed for ADA-compliant pedestrian pushbuttons. Projects for pushbutton replacements are allocated to the 20-Year Comp Plan horizon, with the associated project list documented in the Push-Button Inventory Memorandum attached to the 2025 ADA Transition Plan.

For park and lake access points, all ramps selected for improvements were categorized for full replacement only, either within the 6-Year TIP or the 20-Year Comp Plan horizon, depending on project priority. Engineering judgment was applied to determine project scheduling based on ramp condition, connectivity importance, and proximity to key destinations.

Most park access points and surrounding ramps located within the public right-of-way were found to be compliant and did not require enhancements. Community survey comments referencing ramps near lake parks, trailheads, or other facilities outside the public right-of-way likely relate to locations beyond the City's maintenance responsibility and therefore were excluded from this evaluation.

Appendix I of this memorandum confirms that curb ramps surrounding lake park locations (Lacamas Lake, Fallen Leaf Lake, and Round Lake) do not exhibit high-priority characteristics.

Finally, Apex reviewed the 2015 Plan project list to identify completed and outstanding projects in high- and medium-priority locations. All incomplete projects from the 2015 Plan have been incorporated into the 20-Year Comprehensive Plan horizon, with the associated project list documented in the "2015 Incomplete Projects" memorandum and cost estimates provided in the 2025 ADA Transition Plan.

### **Conclusion**

The self-evaluation combined GIS-based prioritization, inventory review, field verification, aerial imagery analysis, and professional engineering judgment to update curb ramp priorities within the public right-of-way.

Findings related to inventory limitations confirmed that post-2020 improvements required adjustment of several priority rankings, particularly in the downtown area. Findings related to the evaluation process refined treatment recommendations and established a phased implementation strategy through the Transportation Improvement Plan and Comprehensive Plan horizons.

Together, these findings inform the project lists included in the 2025 updated ADA Transition Plan.

Attachment(s): Appendix A: WSDOT ADA Transition Plan Rubric  
 Appendix B: Survey Responses  
 Appendix C: Scribble Maps  
 Appendix D: CAC Summary Meeting Notes  
 Appendix E: Community City Map  
 Appendix F: City of Camas Downtown Parking Inventory  
 Appendix G: GIS Coding Logic Memo  
 Appendix H: Citywide Map  
 Appendix I: Park Maps  
 Appendix J: 2015 City of Camas Self Evaluation Plan

ERS/MT:JAM

# Appendix A

## WSDOT ADA Transition Plan Rubric

## Non-State Jurisdiction ADA Transition Plan Review Rubrics & Rating Guide

**Jurisdiction:**

**Reviewer:**

**Date:**

Category One - Official Responsible for Implementation of ADA Transition Plan		
Exceeds Minimum Criteria	Acceptable	Unacceptable
Primary manager, name, title and role included along with delegation by agency directory or equivalent; all or other key ADA contacts within the organization named, including titles and roles	Primary manager, name, title and role included in plan or website; may also include delegation by agency directory or equivalent	Not included in document or name or title listed, but not both; lacks identification of role

**Rating:**

**Notes:**

**Action Items  
& Dates:**

Category Two - Inventory of Barriers		
Exceeds Minimum Criteria	Acceptable	Unacceptable
Includes results of inventory, assessments and summary of inventory methodology for three or more ADA-related features (best practice minimum - curb ramps, pedestrian signals and sidewalks); or meets criteria for "Acceptable," but includes plan and/or schedule for expanding evaluation to other features or assets	Includes results of inventory and assessments for at least two ADA-related features (best practice minimum - sidewalks and curb ramps or curb ramps and pedestrian signals); may include summary of inventory methodology	Lacks sufficient inventory to evaluate barriers and/or lacks plan to build such data for self-evaluation

**Rating:**

**Notes:**

**Action Items  
& Dates:**

Category Three - Modification Schedule		
Exceeds Minimum Criteria	Acceptable	Unacceptable
Plan shows a strong commitment toward upgrading ADA elements identified in the inventory of barriers in the short term (planned capital improvement projects) and a strong commitment over time toward prioritizing curb ramps at walkways serving entities covered by the ADA. 28 CFR 35.150(d)(2) This would also include prioritization information, planning, and investments directed at eliminating other identified barriers over time. 28 CFR 35.150(d)(3). Resources dedicated to eliminate identified ADA deficiencies.	Plan shows some commitment toward upgrading ADA elements identified in the inventory of barriers in the short term (planned capital improvement projects) and a recognition of priority of curb ramps at walkways serving entities covered by the ADA. 28 CFR 35.150(d)(2) May include some planning for elimination of other barriers over time. 28 CFR 35.150(d)(3). Resources to eliminate identified ADA deficiencies may or may not be identified, but may not be dedicated.	Lacks sufficient commitment to eliminate barriers and/or lacks plan to build a schedule and committed resources

**Rating:**

**Notes:**

**Action Items  
& Dates:**

Category Four - Accessibility Methods		
Exceeds Minimum Criteria	Acceptable	Unacceptable
Describe in detail the <b>Methods</b> that will be used to make the facilities accessible. 28 CFR 35.150(d)(3)(ii); standards that will be applied and any modifications/refinements clearly defined (i.e., 2010 ADAAG, 2011 PROWAG)	Describe most of the <b>Methods</b> that will be used to make the facilities accessible. 28 CFR 35.150(d)(3)(ii); primary standards that will be applied clearly identified (i.e., 2010 ADAAG, 2011 PROWAG)	Incomplete description of the <b>Methods</b> that will be used to make the facilities accessible. 28 CFR 35.150(d)(3)(ii); and/or standards that will be applied not clearly identified and/or defined (i.e., 2010 ADAAG, 2011 PROWAG)

**Rating:**

**Notes:**

**Action Items  
& Dates:**

Category Five - Public Involvement		
Exceeds Minimum Criteria	Acceptable	Unacceptable
Description of process to allow public to readily access and submit comments for both self-evaluation and transition plan. 28 CFR 35.150(d)(1); 28 CFR 35.105(b). Best practices: a) detailed list of individuals consulted posted conspicuously on website, does not have to be in actual TP, but must be documented and available; b) have both electronic and hard copy notice. 28 CFR 35.105(c)	Description of process to allow public to readily access and submit comments for both self-evaluation and transition plan. 28 CFR 35.150(d)(1); 28 CFR 35.105(b).	Incomplete or unclear process to allow public to readily access and submit comments for both self-evaluation and transition plan.

**Rating:**

**Notes:**

**Action Items  
& Dates:**

Category Six - ADA Policy Statement		
Exceeds Minimum Criteria	Acceptable	Unacceptable
Not required of local agencies, but best practice if mentioned and link provided to policy posted conspicuously on website	N/A (Not required of local agencies)	N/A (Not required of local agencies)

**Rating:**

**Notes:**

**Action Items  
& Dates:**

Category Seven - Complaint/Request/Grievance Process		
Exceeds Minimum Criteria	Acceptable	Unacceptable
Description of process and detailed information included to help an individual know how to submit a request for accommodation or file a formal complaint; more than one option provided (including assisted) to allow individuals to submit issue	Basic information included to help an individual know how to submit a request for accommodation or file a formal complaint; link to form or other method included	Insufficient information included to help individuals know how to submit a request for accommodation or file a formal complaint

**Rating:**

**Notes:**

**Action Items  
& Dates:**

# Appendix B

## Survey Responses

# City of Camas ADA Plan Update Survey

## What Camas destinations and streets are important to you?

Number of responses: 33

Text answers:

6th Ave, 3rd Ave, Everett and downtown streets - they get the most traffic and visitors come to the downtown. The round-about at Hwy 14 and 6th is a problem - the asphalt keeps splitting and it started doing that about three months after the roundabout was completed. Can't the original contractor fix it or did they have zero guarantees on the quality of its asphalt product?

Third Avenue is very important to me. Live on that street walk it all the time to and from places like Safeway (Camas), U.S. bank, St. Thomas Aquinas church, Camas library. Love downtown Camas, and the stores, and the walk

Downtown/prune hill east

City hall

Walking near camas high school and north of the lake (for fishing and paddling)

I live near the camas Highschool there are no sidewalks that go west or east of the school . Are there plans to at least have side walks that will go from school to the new roundabout . Ne Everett from the roundabout to the lake park needs bike lanes and sidewalks

All streets and destinations. It's our city.

Downtown

The residential sidewalks surrounding downtown Camas (1st Ave to 3rd Ave). Visitors and residents use these to access businesses, parks and events downtown. If the sidewalks were replaced more people would safely stroll/roll around there.

Downtown and Lake trail

The area near Lacamas and Round Lake (Everett and Lake Rd) is unsafe for the many people who visit there. I live nearby and see people crossing Everett, it's like playing frogger, and the accessibility is terrible.

Downtown core area

The main downtown, 4th Ave and the side streets off of 4th Ave.

All of downtown Camas are important to my daughter that is in a power chair. She washes the windows of several business and some are hard to get in or she can't at all.

Prune Hill area

Downtown

NW 38th Ave, NW Astor St, NW 43rd Ave

Sidewalk on the south side of Safeway across 2nd street is very dangerous for all pedestrians.

Getting to school (Prune Hill Elementary) and the local park (Klickitat Park).

Camas Antiques, Liberty Theater, Library, Next Dough Neighbor, Tommy Os, Camas Hotel, Backpacker Pizza, Daily Paper, Lacamas Lake and trails, Camas Cemetery, Miller Ditch Trail

Downtown, parks, trails, everything

All of them, all streets should be accessible to all individuals no matter the circumstances of hardships they face.

Paper mill. Shops downtown.

Downtown Camas, the Oak Park neighborhood .

Shop entrances, sidewalks, and adding bike lanes

Main street and down town

I feel like the most used destinations and streets in camas is definitely main st

Main street all of it, and most surface streets.

NW 23rd Ave., 18th Loop, Fargo, 28th Ave. and NW Logan St. and every access to and from Prune Hill

Grains of Wrath, Cafe Picalo

Camas Library

4th street

4th, 5th, and 6th and downtown cross roads. Also the Lacamas trail and heritage trails

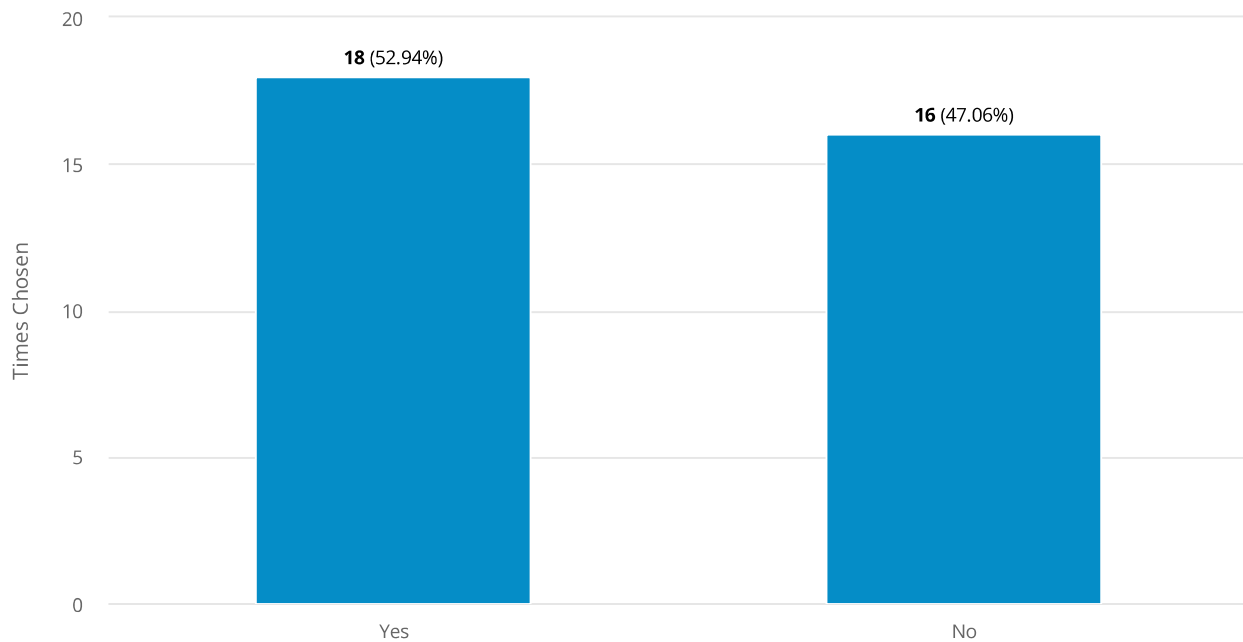
Downtown 4th street

We desperately a sidewalk and bike path from NW 16th, up around the deer farm, connecting to NW Cascade Street. There is no sidewalk or bike path for pedestrians and cyclists to navigate that section of Prune Hill safely. I cannot believe the city allowed the Senior care home on that corner to be developed without requiring a sidewalk be installed. Do Better

The main downtown area.

### Do you experience any mobility or accessibility challenges/barriers in incorporated Camas?

Number of responses: 34



### If yes, please describe. You may also include photos below.

Number of responses: 20

Text answers:

Mostly accessible parking and number of parking spots. Some businesses cannot be accessed because of step up ...

I don't have problems but very concerned with accessibility of others in the City

There are no sidewalks, or incomplete sidewalks on Everett and none on Leadbetter

My mother 83 uses a walker we have 1/2 a block of sidewalk from my house to Everett st . My kids had to walk in the street to get to school because no sidewalk from 43rd circle to camas Highschool  
Pick any part of camas outside the downtown bubble and there are sidewalks needed.  
I'm concerned with the west of the Highschool area down to the new area of housing on ne leadbetter rd and around the new roundabout planed.

Designated motorcycle parking would be a nice feature

Most of the residential sidewalks along 1st and 2nd Avenue including cross streets have old, uneven, broken sidewalks making it difficult to push a stroller or wheelchair along. Young kids are unable to ride a bicycle, scooter or skate on the sidewalk forcing them to use the road which is more unsafe. The sidewalks are also a tripping hazard (especially when trying to walk/roll in early more or late evening/night).

I do not use any assistive devices (yet), but I do have issues with walking due to ankle/ leg pain.

Every time you close off the streets for an event, you block all of the handicapped access parking. It is very difficult to find parking for a van. I also have issue with my daughters salon, Moonlight. There is no Handicap parking close by and we were told that we can't park at the business next to it which is the eye doctor.

No sidewalks and no shoulder on Astor Street from City shed at #4010 to 38th Avenue.

Huge areas of the sidewalk on the south side of 2nd street across from Safeway are crumbling away!

There is a missing section of sidewalk on NW McIntosh Road between NW Sacajawea Street and NW Klickitat Street. This missing section makes me have to go more than an extra mile our if my way to get to school and to the park.

Another dangerous and difficult spot is on NW Astor Street between NW 16th Avenue and the end of the walking path just south of NW 15th Circle. I am forced onto a busy street with no sidewalk or shoulder when going up to Dorothy Fox School because there is a missing segment of sidewalk.  
Finally, there is no easy nor safe way for me to get down the Camas from Prune Hill as a pedestrian. It would be wonderful to have a safe path and/or sidewalk along Forest Home Road (or allow access down NW Rolling Hills Drive and Ostenson Canyon Road that currently has a gate blocking the path).

There is a huge mobility safety issue crossing 6th Ave and 5th Ave on Birch Street. Folks like me who live in Clara Flats walk up and down the hill on Birch Street toward often. We have no pedestrian warning lights to warn drivers or mirrors to see traffic. It is scary because we often come face to face with an oncoming vehicle that we couldn't see because of parked cars lining 6th and 5th.

All of the Oak Park neighborhood is not accessible. Most times there are only sidewalks on one side of the street.

I have family and friends in wheel chairs. I also push a stroller a lot which needs accessible streets

I don't but that doesn't mean that it's the same for everyone. It's 2025, downtown camas should be accessible for everyone.

My father uses a walker or a wheelchair, as does my wife. The sidewalks that have steep ramps or are blocked by the vendors at the shows (car, camas days, food market) all block the sidewalks all the time. Also the lack of handicapable parking is truly sad. The parking spaces are too short for a full length wheelchair van.

Deteriorating sidewalks, neighbors blocking sidewalks with RVs, potted plants, etc.

While entering the office of Camas Vision Centre, or Poppy and Hawk, it's very difficult to access the office of Camas Vision Centre because there are two doors to enter the actual office area while in a wheelchair

It is very difficult to access the forest trails while using a stroller. The north side of 6th Ave has some sidewalk areas that are not accessible for those with wheels

There is currently no safe way for pedestrians or cyclists to go from NW 16th up to past the LDS church to connect with neighborhoods on the top of Prune Hill.

**Please upload your photos here:**

Number of responses: 3

IMG\_3674.jpeg (5 MB)

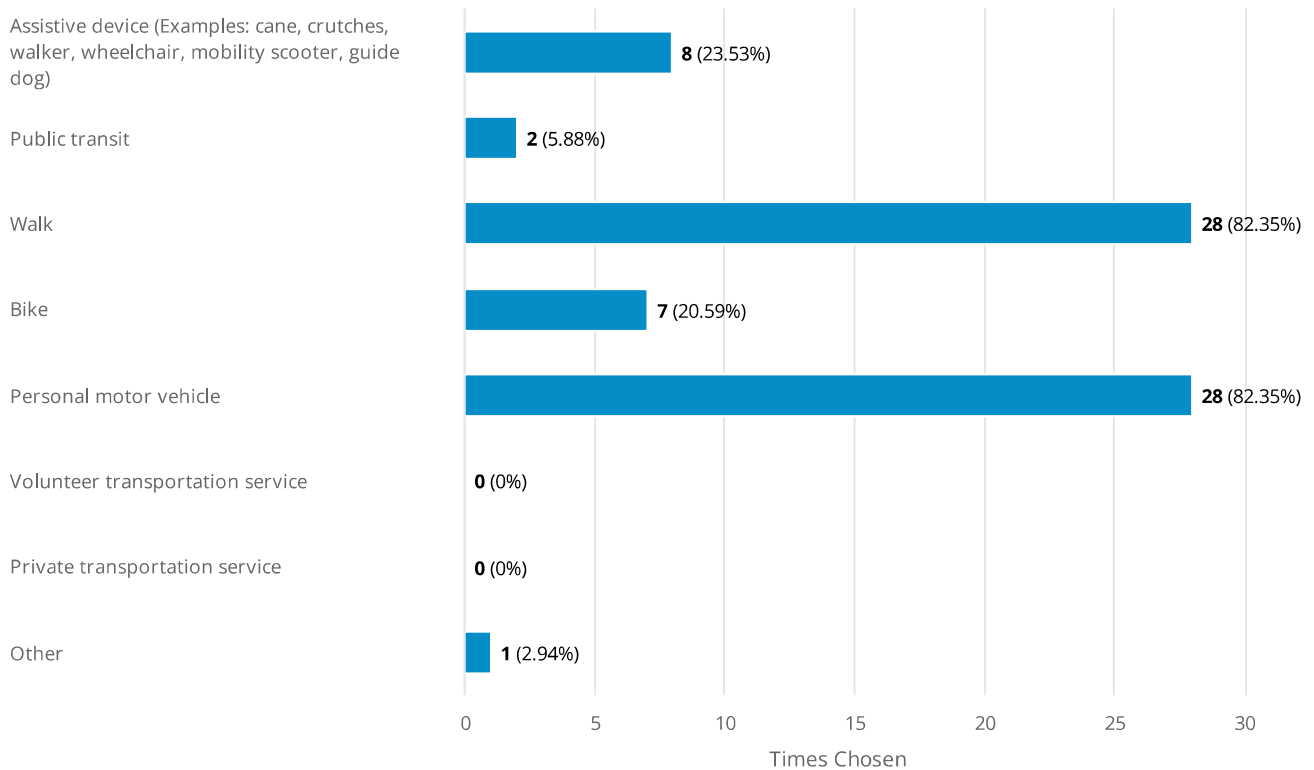
IMG\_3772.jpeg (565 KB)

IMG\_4714.jpeg (139 KB)

## When you travel around Camas, how do travel?

Item 1.

Number of responses: 34



"Other" text answers:

Motorcycle

## Additional comments about how you travel around Camas:

Number of responses: 9

Text answers:

ADA accessibility improvements make the city better for all users, not only the disabled.

Knee scooter or cane

We ride motorcycles for get together lunches

I really enjoy walking with my dog and kids around downtown Camas and not just along Main Street, but down the side streets, to Louis Bloch Park, LaCamas Regional Park, Thai Bloom/Dev's, Baz Riverfront Park, etc. Walking along 3rd I often feel unsafe because cars are passing by very close, so we prefer using First Avenue. I wish I didn't have to watch the ground all the time for tripping hazards. Replacement of the

sidewalks down there would really beautify the entire downtown area and make it more functional and safe for all.

I understand that Camas is an old city built before ADA requirements. My daughter can not get into some of the stores because of a single step. This is unfortunate because one of her favorite stores is Lily's and half of the store is not accessible. As a city you could probably have a few removable ramps that are temporary for a single threshold or step. I have one for the our back slider door.

Anticipate using mobility device (cane) and public bus in the next few years.

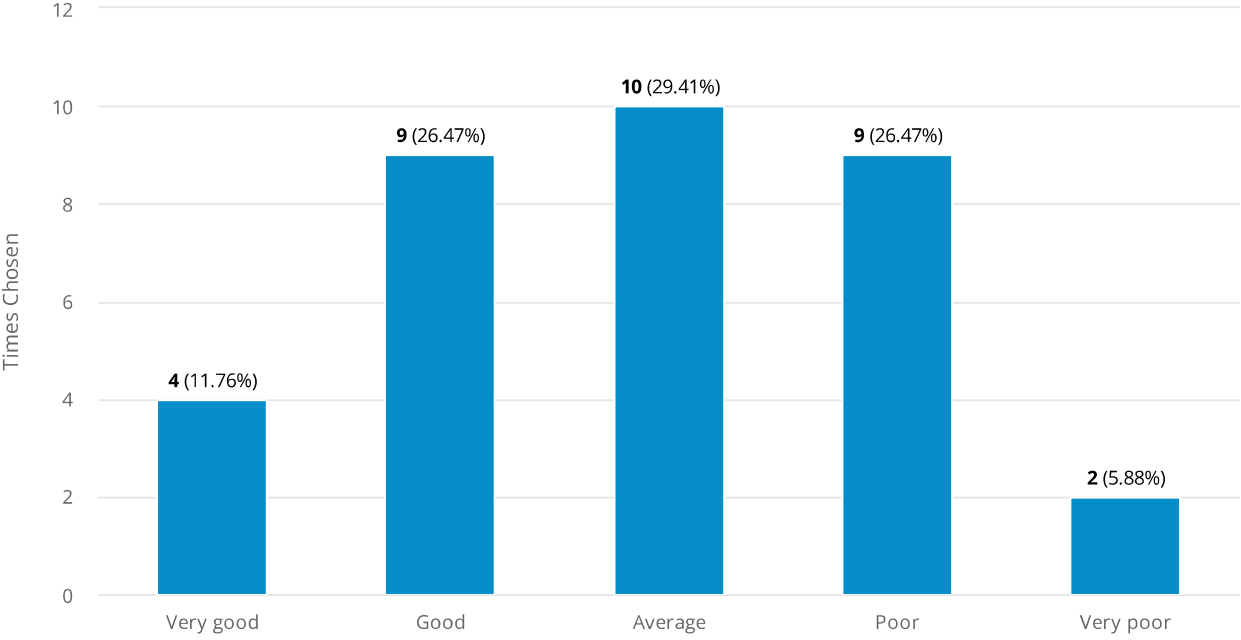
N/A

I try to get around a lot for my health, but there are too many small but meaningful gaps in the trail network that make it difficult. I have highlighted a few of those gaps above.

Never had an issue driving or walking around Camas

### How would you generally rate the curb ramps, traffic-signal push buttons, and sidewalks in Camas?

Number of responses: 34



### Additional comments about curb ramps, traffic-signal push buttons, and sidewalks in Camas:

Number of responses: 19

One of the traffic light push button on 3rd Ave, coming out of skate park driveway is broken. Sidewalks, are ok in Camas for the most part. Hard to push a cart (a small grocery cart) on the rough sidewalks that do not have a smooth paved area. I realized these "old" sidewalks were designed for cold, snow and wet sidewalks so no one slips and falls. Hard to work with in drier weather.

X Can a flashing light and a crosswalk be placed between the apartment complex 3rd Ave Apts, "Riverview Apts 3004 N.E. 3rd Ave, and Terrace at River Oaks 3009 N.E. 3rd Ave? So many people cross at the various apt complexes. We are far away from the light at 3rd Ave and Shepherd. Traffic is fast coming from Washougal, so there is not alot of time to cross 3rd Ave, at these apartment areas. Traffic needs to slow down coming from Washougal, way too fast., this might slow them down. A flashing light and NOT a regular traffic light, should not make for congestion, just slow down traffic, so we can cross the street without getting "smacked" by a bunch of fools.

Some access ramps have curbs too narrow for chair wheels to clear.

Most signals appear to be accessible with ADA pushbuttons. Most sidewalks are OK but tripping hazzards can be found near trees.

Very few new ramps and NO sidewalk. It is not at all safe, especially with increasing traffic.

Nw lacamas rd and Ne lake were the lake walking path by the docks needs a pedestrian crossing with lights . Traffic comes down the hill fast and it is a blind corner someone will get hurt without a crosswalk . I see moms with stroller's crossing at that intersection to walk at the lake park .

They are good at a particular corners along 3rd Avenue but aren't found anywhere else downtown. I would love to see a cross walk with buttons put in at 1st Avenue and 3rd by the Regional park entrance. Many people park along 1st Avenue when parking is full and cross the street over to the park. Downtown residents also walk up 1st Avenue and cross 3rd over to the park.

Many of the sidewalks are uneven or bumpy .

I love the charm of the downtown area, but the condition sidewalks makes it a challenge to walk.

Need more, keep putting them in where needed most.

There are too many missing segments in the trail network of sidewalks and paths in Camas to make it easy to get around. Too often, vehicles are promoted over pedestrian travel.

A lot of intersections lack push buttons or proper cross walk signals.

Everything is excellent

Many neighborhoods do not have these things, or if they do it is insufficient in quantity.

The sidewalk are uneven and I sometimes trip, Lacking bike lanes some sidewalk entrance to shop are not accessible

Their used to be lights at the crosswalk to warn drivers, then you went to the orange flags that no longer exist. Put the warning lights back. Make the curb ramps less steep.

Pedestrians must wait for all signals, it seems, before they are given a signal to cross. Also, are there no signals that givie them exclusive access?

Some sidewalk areas have large cracks/broken pieces.

The curb on the right side of the library near the bike rack should be marked to indicate there is a change in elevation (near the bee tree). I have seen multiple people fall off that area of curb where it increases in height.

When button is pushed especially at night, have the sidewalk light-up.

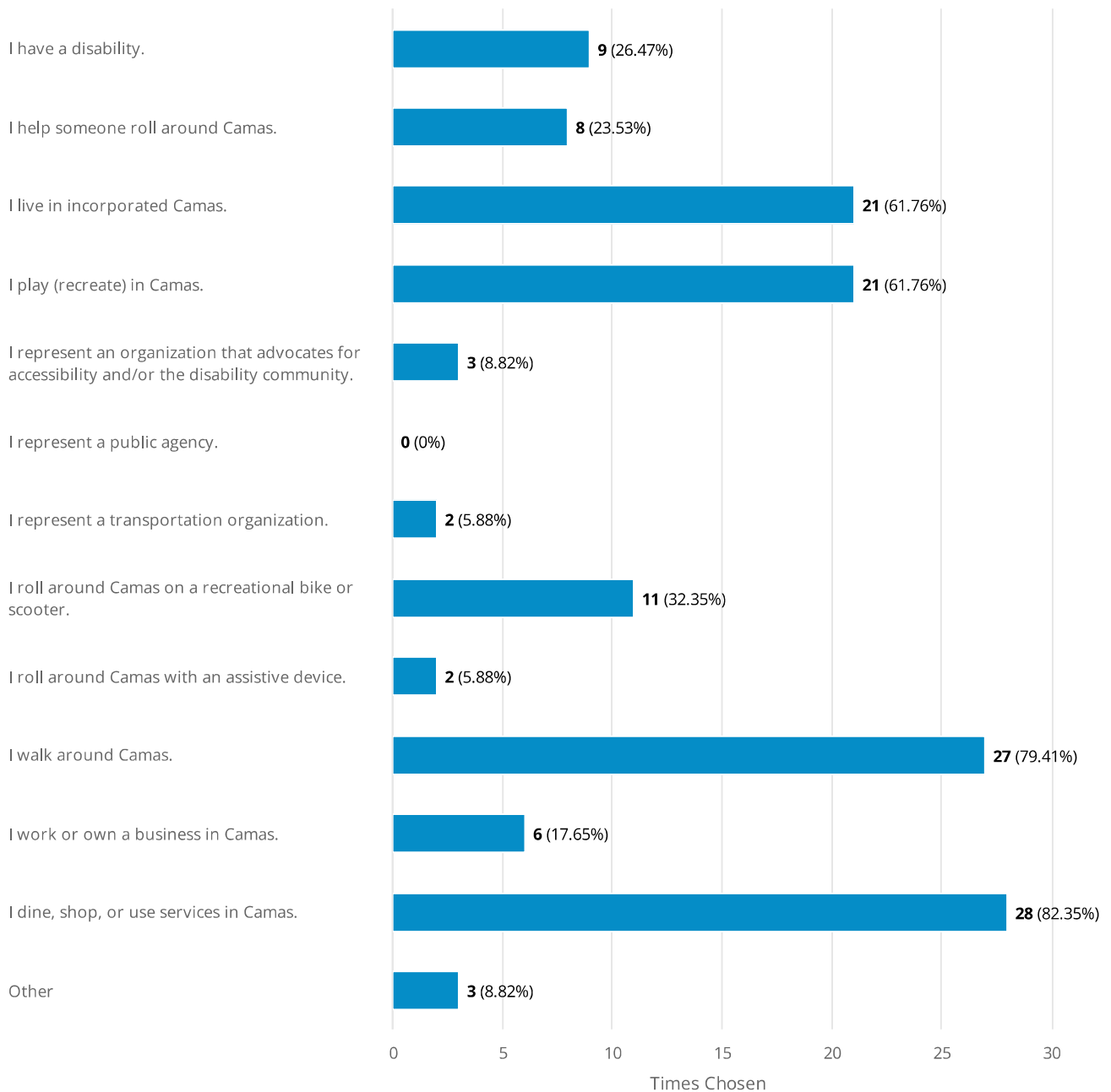
Improve lighting at crosswalk

There are not enough sidewalks installed to make Camas a walk friendly city.

Traffic signal push button at NW 16th and Klickitat has been broken since they built the McMansion on that corner.

# Please tell us a little bit about yourself.

Number of responses: 34



"Other" text answers:

- I am working the City's ADA plan update
- I also have a daughter that attends camas adult transition and they work in downtown camas.
- Mobility issues

## Additional comments about yourself:

Item 1.

Number of responses: 13

Text answers:

I will be 79 yrs old in September. So, am slowing down a bit, but I still get out and walk most of Camas. Am pretty much familiar with downtown Camas, main part of Camas( 3rd Ave), and the traffic leading in and out of Washougal.

My comments are are intended to test the system to see how well it is collecting comments.

Paddling helps reduce stress

I've had knee surgery last year we have to drive to the parks because there's no sidewalks that lead out of my cul-de-sac on 43rd circle

I was a Camas/Washougal volunteer Firefighter for 13 years before my knees got to bad to continue  
It would be nice to have more of this community connected with proper bike paths and sidewalks.

I work with Deaf, Hard of Hearing, Deaf-Blind, Blind, Low-Vision students as well as children with physical disabilities.

I love going downtown to shop or eat. I don't like taking my daughter down during an event. It's extremely hard to find parking. You should have a designated drop off area for handicap people or reserve spots for vans with the ramps.

Retired. but still busy.

My home is across from the loading dock of Safeway. Heavy foot traffic to the store. Sidewalk is very dangerous. Someone can possibly get injured...

I would love to see an accessibility map done for the downtown businesses. This could help people know ahead of time if a coffee shop or restaurant is fully accessible. Maybe a HS student could do this as a senior project?

As an older person, I think everything here in town is perfectly accessible already. I have no issues

I have Epliepsy and when im in a dizzy state it hard for me to tell if there a bump(Crack or uneven in sidewalk) so I tend to trip alot

I am over 75. My daughter is intellectually disabled and needs door-to-door transport but cannot get it where we live.

I'm a tax payer and I hate to see millions spent in areas of no need. Such as the lower Lacamas parking lot. It may look nice, but parking was reduced and the trails are not for handicapped people. Trail walkers didn't need paved parking and additional landscaping.

**Is there anyone else you think we should reach out to for input (person, advocate, organization, etc.)?**

Number of responses: 6

Text answers:

Rob Klug at WSDOT Local programs

Noise pollution

Orientation and mobility specialist that works in Camas School District, Evergreen School District or for the State School for the Blind.

Ask the residents in the senior homes and centers.

Putting things like this on the ballot. You should also ask local businesses.

Madeline Cooley  
(360) 356-4525

**If you would like to be contacted about your feedback, please provide your contact information here:**

Number of responses: 10

Text answers:

[Redacted]

[Redacted]

[Redacted]

[Redacted]

[Redacted]

Thanks for prioritizing accessibility

[Redacted]

[Redacted]

# Appendix C

## Scribble Maps



# Americans with Disabilities Act 2025 Plan Update




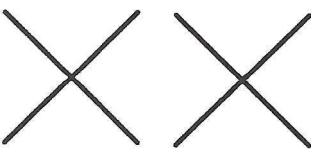
## Help us improve how you walk, ride, or roll around downtown Camas!

Please indicate how you travel around downtown Camas, then use the symbols below to mark your routes and access experience on the map on the back of this page. Return the completed map by Monday, December 1, 2025, to City Hall or Camas Library, or send a photo or digital image by email to MPhillips@cityofcamas.us.

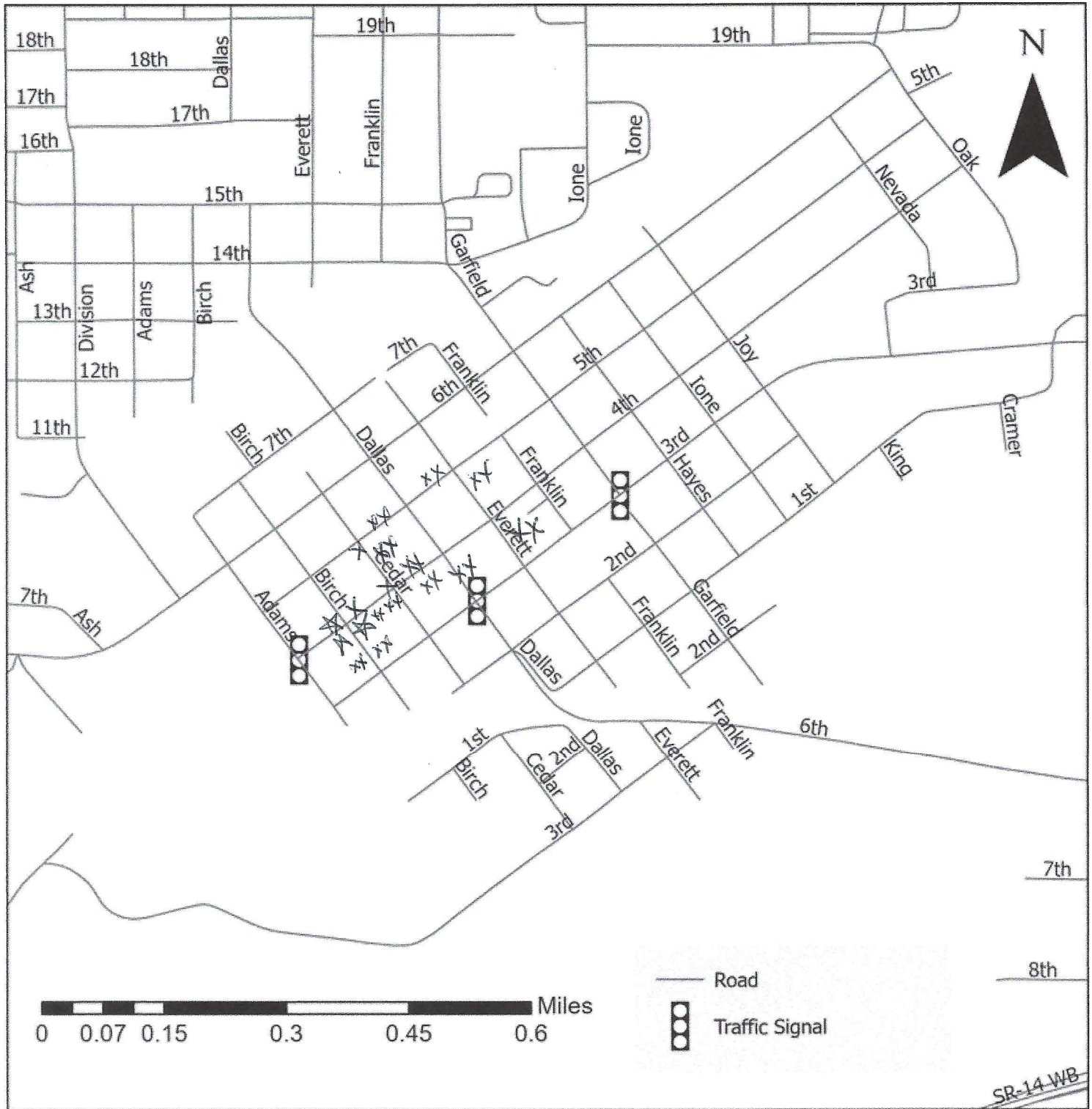
**How do you get around Downtown Camas? Select ALL the ways you travel.**

- Walk with an assistive device
- Roll in a wheelchair
- Walk
- Roll on a bike, e-bike, or scooter
- Ride in a car
- Ride in public transit
- Other:

**Use these symbols to mark the map on the back of this page.**

	Use a <b>line</b> to show the routes you use to walk, ride, or roll around Downtown Camas. <i>I walk all through town!</i>
	Draw a <b>star</b> on the corners with curb ramps (angled pavement) that allow you to go up and down safely and easily.
	Draw an <b>X</b> on the corners with curb ramps (angled pavement) that <i>do not</i> allow you to go up and down safely and easily.
	Draw an <b>XX</b> where a barrier blocks travel on a sidewalk. Barriers can be objects such as mailboxes or damage to a sidewalk or curb ramp.

Use the symbols on page one to show your routes, curb ramp rankings, and barriers in downtown Camas.



# **Appendix D**

## **CAC Summary Meeting Notes**



## EVENT OVERVIEW

PROJECT: City of Camas Americans with Disabilities (ADA) Plan Update 2025

EVENT: Community Advisory Committee, Session 1

DATE: June 27, 2025, 10:00 am

LOCATION: Camas City Hall, Council Chambers

## HOSTED BY:

- James Carothers, City of Camas Engineer Manager
- James Hodges, City of Camas Capital Projects Manager
- Chris Lopez, City of Camas Engineer
- Madison Phillips Sr. Administrative Support Assistant
- John Manix, Apex Companies Sr. Traffic Engineer
- Alicia Brazington, Apex Companies Public Involvement Director

## ATTENDEES:

*In-person*

- Carrie Schulstad, Downtown Camas Association
- Chelsey Ekstrom, community member
- Jeff and Micah Snell, community members/Micah's Miles founders
- Doreen McKercher, Camas School District

*Online*

- WSDOT- ADA/504 Compliance Coordinator, Allison Spector

## DISTRIBUTIONS:

- City of Camas ADA Transition Plan 2015
- PowerPoint presentation handout
- Enlarged maps from the handout
- Title VI Public Involvement form

## MATERIALS RECEIVED:

- Sign-in Sheets (2)
- Title VI Public Involvement forms (3)

## SUMMARY

The project team, led by James "Curleigh" Carothers, welcomed the group and thanked attendees for their participation, then had individuals introduce themselves and their interest in the project. John Manix of the consultant team presented the overall importance of an ADA Plan, explained the outcomes of the City's 2015 efforts, gave examples of the types of barriers to mobility and accessibility we are looking to address in the 2025 plan. Alicia touched on the people and organizations who would be included in additional outreach and communicated the schedule for doing so. Then, facilitated a discussion around three key questions:

Question	Answer
<b>1. What Camas destinations and streets are important to you?</b>	Routes/intersections near transit/transportation, areas near schools, Third and Sixth Avenues in the Downtown core, areas near community gathering spaces and accessible amenities
<b>2. What mobility and accessibility barriers do you experience in Camas?</b>	Inability to cross roadways safely, vegetation (e.g., blackberry brambles), poor curb design, power poles inhibiting sidewalks
<b>3. Anyone else we should talk to?</b>	Several (see below for details)

After, Curleigh clarified the next steps in the project, including forthcoming CAC sessions, and conveyed the team's contact information and availability to them throughout the process.

## AGENDA

- Welcome & Introductions
- What is an ADA Plan?
- Community Outreach
- Possible Improvements
- Compliant & Noncompliant Facilities
- Group Discussion
- Next Steps & Conclusion

## AREAS OF DISCUSSION

### What Camas destinations and streets are important to you?

- Camas Schools:
  - Safe Routes to Schools, especially Dorothy Fox Elementary (2623 NW Sierra Street), which has the fewest buses and most walkers.
  - West of Sixth, the roadway changes from one to two lanes, and there have been pedestrian conflicts caused by vehicles moving over early.
- DCA:
  - The Third Avenue Complete Streets Grant will allow for bulb-outs, street diets, and ramps. Make sure these are ADA-compliant.
  - We have tree root problems in downtown sidewalks. For example, from Journey Church (304 NE 4th Avenue) to Fuel Medical Group (314 NE Birch Street) on the east side of Birch, and in front of Painless Rick's Tattoo Parlor (315 NE Birch Street).
  - Downtown curbs lack truncated domes.
  - The Sixth and Adams intersection lacks a safe crossing.
- Snells:
  - Access to community gathering spaces, such as Doc Harris Stadium (1125 NE 22nd Avenue) and Garver Theatre (1500 NE Garfield Street).
  - Prioritize access to parks that already have accessible paths and features.
  - Prioritize and anticipate intersections, school routes, etc., leading to or near transit and transportation.
- Eskstrom:

- Walking from Tidland Heights to Sixth Avenue, despite crosswalk with light, is challenging.
- Uneven spots require her to stay on the street side of her son while walking to ensure his safety.
- Crosswalks without white striping make her son feel less confident crossing.

### What mobility and accessibility barriers do you experience in Camas?

- Access to parks – Some parks do not have accessible paths and features. Harper’s Playground in Vancouver, WA, is a great example of successful design.
- Wheelchair vehicle parking – There is often not enough room, so the back-end ramp of the Snells’ car extends into the travel lane and other cars drive over it.
- Hills – keep in mind that self-propelled and pushed wheelchairs are different experiences.
- Access and angles at intersections – the changes recently made at QFC (3505 SE 192nd Avenue) have made the area *less* accessible. It’s harder to pivot.
- Blackberries and vegetation need long-term maintenance, come into sidewalks.
- A power pole restricts the sidewalk at Helen Baller Elementary (1954 NE Garfield Street) on 22nd across from Zellerbach Administration Building.

### Anyone else we should talk to?

1. Greenen family, who owns Grains of Wrath restaurant in Camas – Parents: Colleen & Kent, son: Brendan (current GOW owner, former Caps n Taps owner), son: Ryan, has a disability. Brendan info: 360.624.5606 / [brendan@gowbeer.com](mailto:brendan@gowbeer.com)
2. Camas School District bus drivers – Doreen Mc Kercher will get back to us on the best way to reach them; this might end up being through the online survey we’ll be creating.
3. Amy Campbell, Camas School District special education teacher, national teacher of the year award winner, Micah’s former teacher. 360.910.8930 / [amy.campbell@camas.wednet.edu](mailto:amy.campbell@camas.wednet.edu) (might not use this over summer)
4. Faith-based organizations – Jeff Snell felt they might be able to recommend CAC participants. Grace Foursquare pastor, Keith Nelson, is one. See screenshot of contact info below\*
5. Nan Henriksen – Former mayor, current volunteer/leader, has a disability: 360.521.2655 / [nannow@alpinelane.org](mailto:nannow@alpinelane.org)
6. Christy Quinn – Spouse of Camas City Administrator Doug Quinn, has a disability. Please connect with Doug to get her contact info.

### Other:

- Be aware of the experiences of individuals with sight differences and other types of disabilities.
- John explained the concept of Maximum Extent Feasible (MEF) design and documentation.
- John assigned the homework of looking at existing ramps and taking pictures.

## ATTENDANCE

The City had sent eighteen invites by email and US mail to a variety of community members with a disability and representatives of local advocates, agencies, and service providers who had expressed interest in participating in the CAC during and who had indicated their preferred meeting dates during previous correspondence. Madison followed up by phone and/or email to confirm receipt and reiterate the meeting details.



## EVENT OVERVIEW

PROJECT: City of Camas Americans with Disabilities (ADA) Plan Update 2025

EVENT: Community Advisory Committee, Session 2

DATE: September 18, 2025, 10:00 am

LOCATION: Camas City Hall, Council Chambers

## HOSTED BY:

- James Carothers, City of Camas Engineer Manager
- James Hodges, City of Camas Capital Projects Manager
- Carrie Davis, Executive Assistant
- Alicia Stevens, Records Management Coordinator
- John Manix, Apex Companies Sr. Traffic Engineer
- Alicia Brazington, Apex Companies Public Involvement Director

## ATTENDEES:

*In-person*

- Chelsey Ekstrom, Community Member/Parent
- Christina Marneris, Community Services Manager, Area Agency on Aging and Disabilities of Southwest Washington

*Online*

- Norma Madden Page, Camas School District (CSD) Parent-Teacher-Student Organization
- Stephanie Eakins, CSD Adult Transition Training Center (ATTC) Teacher/Trainee Manager
- Madeleine Cooley, CSD ATTAC Student/Trainee

## DISTRIBUTIONS:

- PowerPoint presentation handout
- Enlarged maps from the handout
- Title VI Public Involvement form

## MATERIALS RECEIVED:

- Sign-in Sheets (1)
- Title VI Public Involvement forms (1)

## SUMMARY

The project team, led by James "Curleigh" Carothers, opened the session by welcoming attendees and inviting introductions. John Manix of the consultant team provided a recap of the ADA Plan update process for new CAC attendees, noting the focus on sidewalks, curb ramps, traffic-signal push buttons.

Alicia reviewed the list of stakeholders invited to take part in the online survey and the CAC. She then presented the survey results, which included 56 responses from community members, caregivers, advocates, and local organizations. Respondents reported a range of mobility and accessibility challenges, rated Camas' sidewalks and

ramps with mixed reviews, and highlighted destinations such as Downtown, parks, and key residential and connector streets. It was noted that respondents offered numerous additional comments on each question.

John then presented a detailed map of the locations and conditions of existing sidewalks and curb ramps, with callouts showing areas mentioned in the online survey. The group then responded to the questions, “Based on the survey results, for which streets should we prioritize improvements and invest funds? Why?”

Finally, Curleigh outlined next steps, including refining high-priority corridors, preparing draft recommendations for Session 3, and developing cost estimates to fit the City’s budget. Project team contact information was shared, and attendees were encouraged to stay engaged throughout the process.

## AGENDA

- Welcome & Introductions
- Session 1 Recap
- Survey Results
- Important Destinations & Streets
- Discussion
- Next Steps & Conclusion

## AREAS OF DISCUSSION

*Note: Parks and buildings, as well as properties not owned by the City of Camas, such as the Port of Camas-Washougal, are not part of this project. However, all comments were noted.*

### Session 1 Recap

- Current focus is on sidewalks and ramps in the public right-of-way (not buildings or parks).
- Emphasis on identifying what, where, how much, then creating a budget and schedule.
- Also looking at policies and practice, for instance, who do you call if sidewalk is unsafe.
- The final ADA Plan will also highlight progress since 2015.

### Who We’re Talking To

- Broad outreach includes community members with disabilities, public agencies, advocacy groups, C-TRAN, WSDOT, Clark County, Camas School District (transportation, special ed, Parent-Teacher-Student Organization), faith-based groups, and assisted living facilities.

### Survey Overview

- Open: Aug. 1 – Sept. 4, 2025.
- 56 responses collected through online and print versions (City Hall, Library).
- Publicized through mailers, EngageCamas.com, social media, flyers, Downtown Camas Association communications, and CAC network.
- Accessibility support provided.

### Respondent Profile

- Participants included residents, people experiencing disabilities, caregivers, public agency representatives, advocates, and local businesses.
- Over 20% of respondents expressed having a disability, and many reported mobility or accessibility challenges; photo uploads and comments illustrated barriers.

### Travel Modes

- Walking, biking, personal vehicles, transit, assistive devices, and volunteer/private transportation were all noted.

### Accessibility Ratings

- Mixed views: some rated ramps/signals/sidewalks “good” or “very good,” while others said “poor” or “very poor.”
- Frequent comments highlighted uneven sidewalks and difficulty with truncated domes/visibility.

### Important Destinations & Streets

- John shared the curb ramp inventory map, explaining the color-coded dots and clarifying the safe harbor rule: ramps built to code at the time don’t need updating unless altered.
- Explained we will get more specific in the next phase of the self-evaluation, when we’ll decide what ramps should be replaced.
- Top places mentioned in the online survey were Downtown Camas, parks, trails, and lakes. These areas were represented on the map too.
- Attendees were encouraged to challenge the “dot colors” (condition ratings) based on their personal knowledge.

**CAC Discussion:** Based on the survey results, for which streets should we prioritize improvements and invest funds? Why?

- **Stephanie, Norma, and Madeleine:**
  - Downtown accessibility concerns during events.
  - Steps up to downtown buildings and through stores are challenging for Madeleine, who provides window washing services through the ATTC.
  - Cord safety issues at Hometown Holidays.
  - Parks and trails (e.g., Greenway Trail) are sometimes accessible, but ramps could be more visible (truncated domes needed).
  - Suggested marking sidewalk breaks on 2nd Avenue at Everett (near 612 NE Everett) in bright yellow for visibility. Stephanie suggested policy to include student input in accessibility planning.
  - Want to make sure that students like Madeleine have a platform. Madeleine is available to serve as a spokesperson for accessibility campaigns.
  - Suggestion was made to investigate community partners such as Heidi Curley Art (respite care) and outreach via Camas School District’s PeachJar service for sending e-flyers and sharing accessibility-related information in the future.
- **Chelsea:**
  - Recreation barriers: accessible bathrooms often locked or for those with pavilion reservations.
  - Heritage Trail is better than others; enjoys Port views but noted lack of continuous connections.
  - Event challenges: difficulty finding accessible parking, then traversed extremely steep terrain from Garver lot down to the event, where she, her son, their friend (Micah, also a CAC member), and his caregiver then faced more obstructions from the crowd.
  - Requested event accessibility policies (maintain pathways, additional ADA parking).
  - Need for continuous safety planning for families with children who may be unaware of dangers.
- **Christina:**
  - Suggested finding existing fitness classes for older residents to share out resources.
  - Value of walking maps showing accessible routes and curbs, including older residents.
  - Emphasized “life enrichment” areas (parks, trails) as important to living full lives.

- Offered to help promote accessibility resources to older adults through Mountaintops and at her workplace (community lodge).

### **Transit & Mobility**

- Questions were raised about public transit and light rail opposition.
  - Curleigh and Carrie: CTRAN services will not be reduced; the discussion happening is about cost-sharing.
  - Possible additional 2–3 CTRAN routes in future.
  - Request was made to prioritize ADA accessibility on transit routes NW 6th Ave and 3rd Ave.
- Mention of Community in Motion services (Medicaid-related).
- Accessible bus services are available through CSD and CTRAN for large events; Carrie will share with the Chamber of Commerce that folks might not know about these services.

### **Next Steps**

- Compile list of high-priority corridors and curb ramps (missing, substandard, or needing replacement).
- Rank existing curb ramps and Public Rights-of-Way Access Guidelines (PROWAG) shortfalls.
- Prepare draft plan (review in Session 3).
- Develop cost estimates to align with City budget.
- Present draft for City Council adoption.
- Integrate projects into short- and long-term Transportation Improvement Plans.

### **Project Contacts**

- Jim Hodges, Capital Projects Manager – [jhodes@cityofcamas.us](mailto:jhodes@cityofcamas.us), 360.817.7234
- Chris Lopez, Engineer – [clopez@cityofcamas.us](mailto:clopez@cityofcamas.us), 360.817.1568
- James Carothers, Engineering Manager – [jcarothers@cityofcamas.us](mailto:jcarothers@cityofcamas.us), 360.817.7230
- John Manix, Apex Companies – [john.manix@apexcos.com](mailto:john.manix@apexcos.com), 360.567.2117
- Project page: [engagecamas.com/ada-plan-updates-2025](https://engagecamas.com/ada-plan-updates-2025)



## EVENT OVERVIEW

PROJECT: City of Camas Americans with Disabilities (ADA) Plan Update 2025

EVENT: Community Advisory Committee, Session 2=3

DATE: December 4, 2025, 12:00 pm

LOCATION: Camas City Hall, Council Chambers

## HOSTED BY:

- James Carothers, City of Camas Engineer Manager
- Chris Lopez, City of Camas Engineer
- Madison Phillips Sr. Administrative Support Assistant
- John Manix, Apex Companies Sr. Traffic Engineer
- Alicia Brazington, Apex Companies Public Involvement Director

## ATTENDEES:

*In-person*

- Carrie Schulstad, Downtown Camas Association
- Doreen McKercher, Camas School District
- Nan Henriksen, Camas Community Member & Former Camas Mayor
- Mike Sallas, Clark County TBD

*Online*

- Christina Marneris, Community Services Manager, Area Agency on Aging and Disabilities of Southwest Washington
- Moe Taha, Apex Companies Traffic Engineer
- Emmelie Schulteis, Apex Companies Traffic Engineer

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- Next Steps & Conclusion

## AREAS OF DISCUSSION

- Chelsey Ekstrom, Community Member/Parent
- Jeff and Micah Snell, community members/Micah's Miles founders
- Diane

### Session 1 Recap

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- Emphasis on identifying what, where, how much, then creating a budget and schedule.
- Also looking at policies and practice, for instance, who do you call if sidewalk is unsafe.
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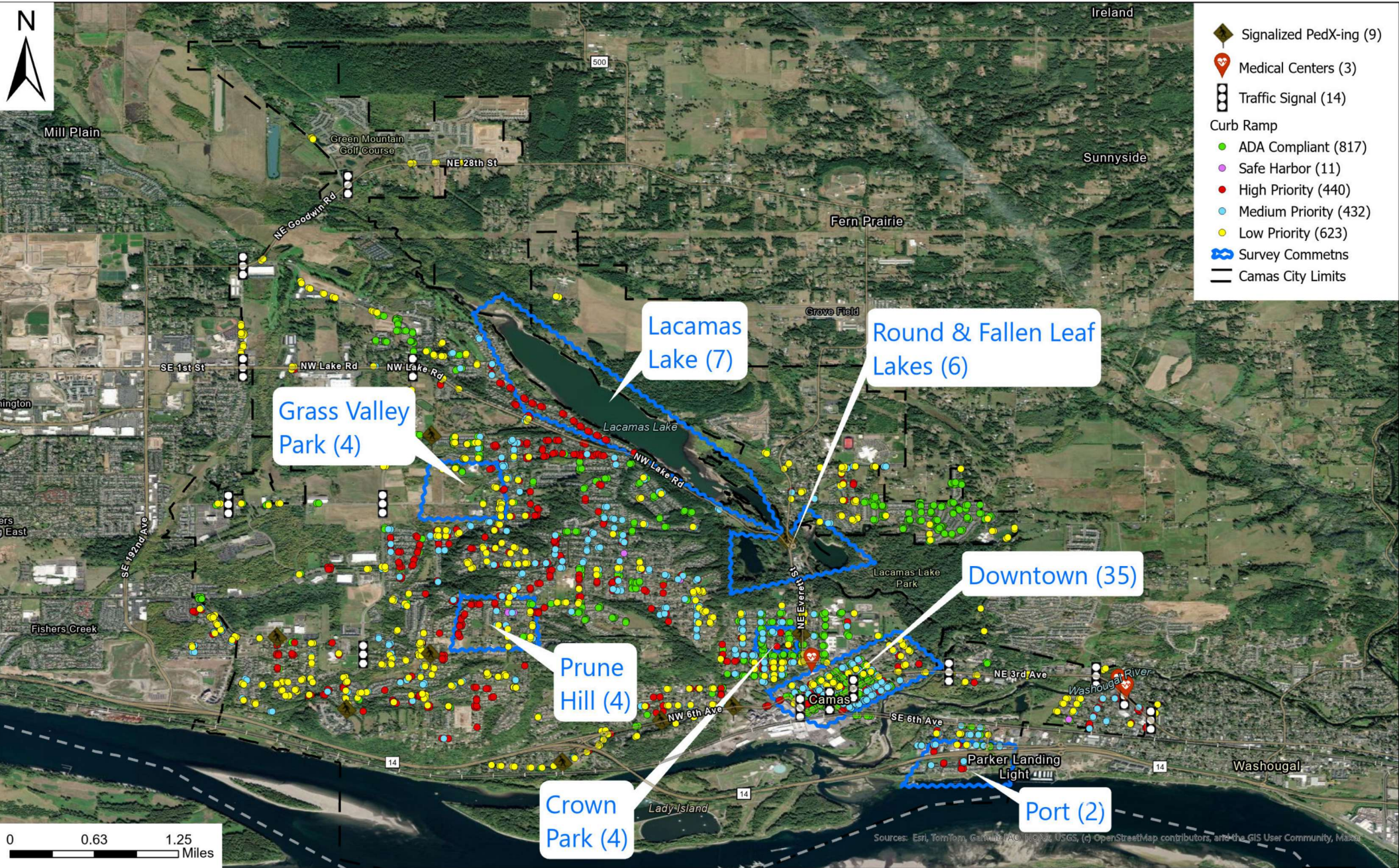
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- Chris Lopez, Engineer – [clopez@cityofcamas.us](mailto:clopez@cityofcamas.us), 360.817.1568
- James Carothers, Engineering Manager – [jcarothers@cityofcamas.us](mailto:jcarothers@cityofcamas.us), 360.817.7230
- John Manix, Apex Companies – [john.manix@apexcos.com](mailto:john.manix@apexcos.com), 360.567.2117
- Project page: [engagecamas.com/ada-plan-updates-2025](https://engagecamas.com/ada-plan-updates-2025)

# Appendix E

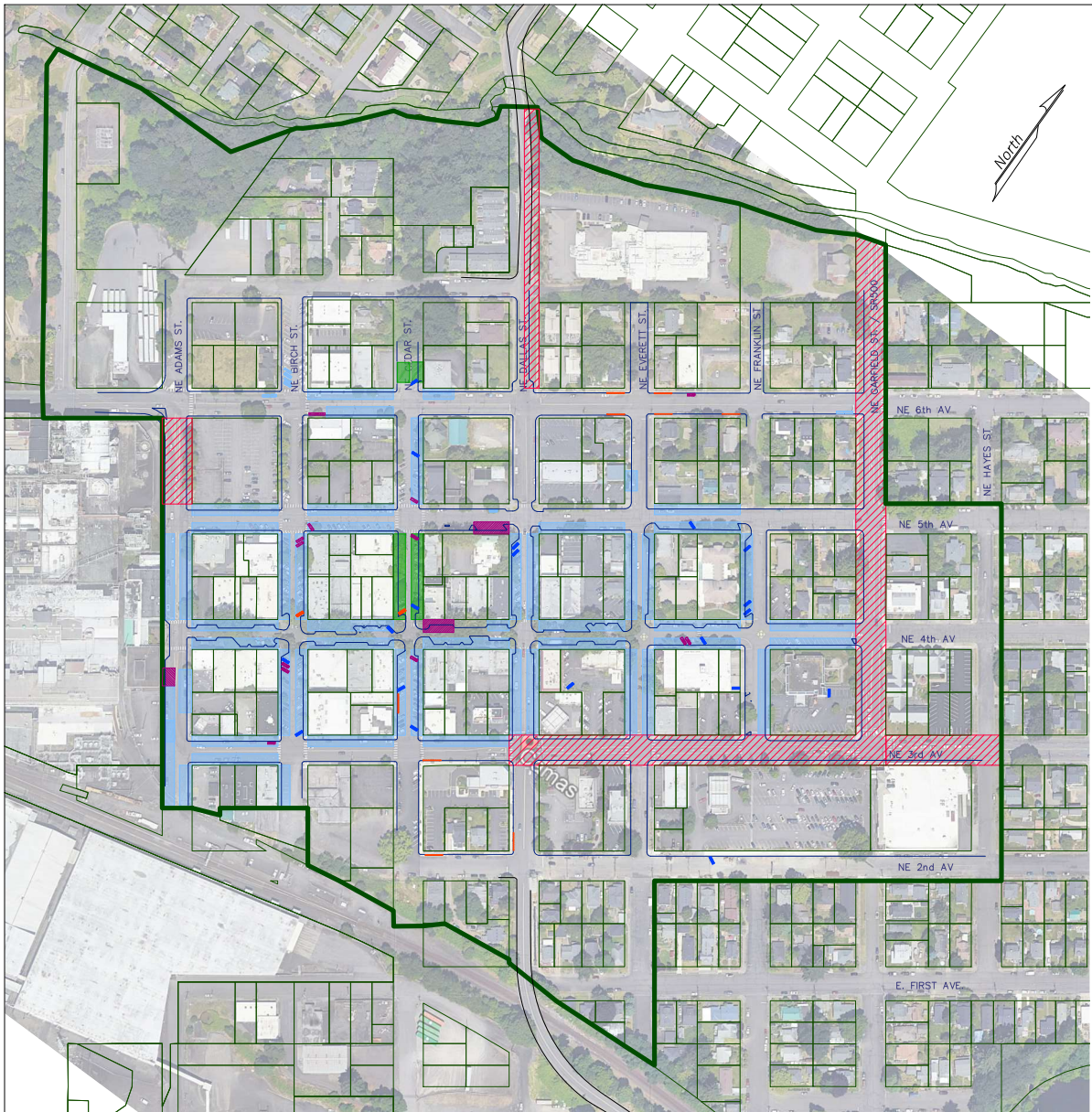
## Community City Map

# City of Camas Curb Ramp Inventory With Survey Comments



# **Appendix F**

## **City of Camas Downtown Parking Inventory**



# City of Camas

WASHINGTON

- PARKING ZONE  
COLOR KEY
-  HANDICAP PARKING ONLY
  -  3 HOUR PARKING
  -  2 HOUR PARKING
  -  1 HOUR PARKING
  -  30 MIN PARKING
  -  10 MIN PARKING
  -  NO PARKING
  -  LOADING ZONE
  -  NO SIGNS POSTED (72 HR)

CITY OF CAMAS  
PUBLIC WORKS DEPARTMENT  
DOWNTOWN COMMERCIAL  
PARKING MAP

# Appendix G

## GIS Coding Logic Memo

## ADA Camas – GIS Work

Project File is located here:

[L:\Projects\\\_DVP\C\CIT333\24011088\Traffic\Documents\GIS\2025-03-17\\_GIS\\_Map\Camas-ADA-Transition-Plan\Camas-ADA-Transition-Plan.aprx](L:\Projects\_DVP\C\CIT333\24011088\Traffic\Documents\GIS\2025-03-17_GIS_Map\Camas-ADA-Transition-Plan\Camas-ADA-Transition-Plan.aprx)

Ranking Criteria (In-Progress):

[L:\Projects\\\_DVP\C\CIT333\24011088\Traffic\Documents\Study\2025-04-16 Draft Replacement ranking criteria.docx](L:\Projects\_DVP\C\CIT333\24011088\Traffic\Documents\Study\2025-04-16 Draft Replacement ranking criteria.docx)

### Code in progress (Latest as of 7/17/2025)

```
myCalc(!Ramp_Width!, !Ramp_Slope!, !Warning_ty!, !Land_Drop!, !Notes!, !Landing_on!, !Ramp_Warni!,
!SafeHarbor!, !Improved!)
```

```
def myCalc(Ramp_Width, Ramp_Slope, Warning_ty, Land_Drop, Notes, Landing_on, Ramp_Warni,
SafeHarbor, Improved):
```

```
    if (Ramp_Width >= 48 and Ramp_Slope <= 8 and (Warning_ty == 'Yellow Bumped' or Warning_ty == 'Other')
and Notes != 'Horizontal Line Stamp' and Landing_on == 'Yes' and Land_Drop == '0') or Improved == 'Yes':
```

```
        return 'ADA Compliant'
```

```
    if SafeHarbor == 'Safe Harbor':
```

```
        return 'Safe Harbor'
```

```
    if ((Ramp_Width >= 42) and Ramp_Slope <= 10 and (Ramp_Warni == 'Yes') and (Landing_on == 'Yes' or
Landing_on == 'No') and Land_Drop == '0' and SafeHarbor == 'Not Protected'):
```

```
        return 'Low Priority'
```

```
    if (Ramp_Width >= 48 or Ramp_Slope <= 8) and (Ramp_Warni == 'Yes') and (Land_Drop == '0' or Land_Drop
== '>0 and <1') and SafeHarbor == 'Not Protected':
```

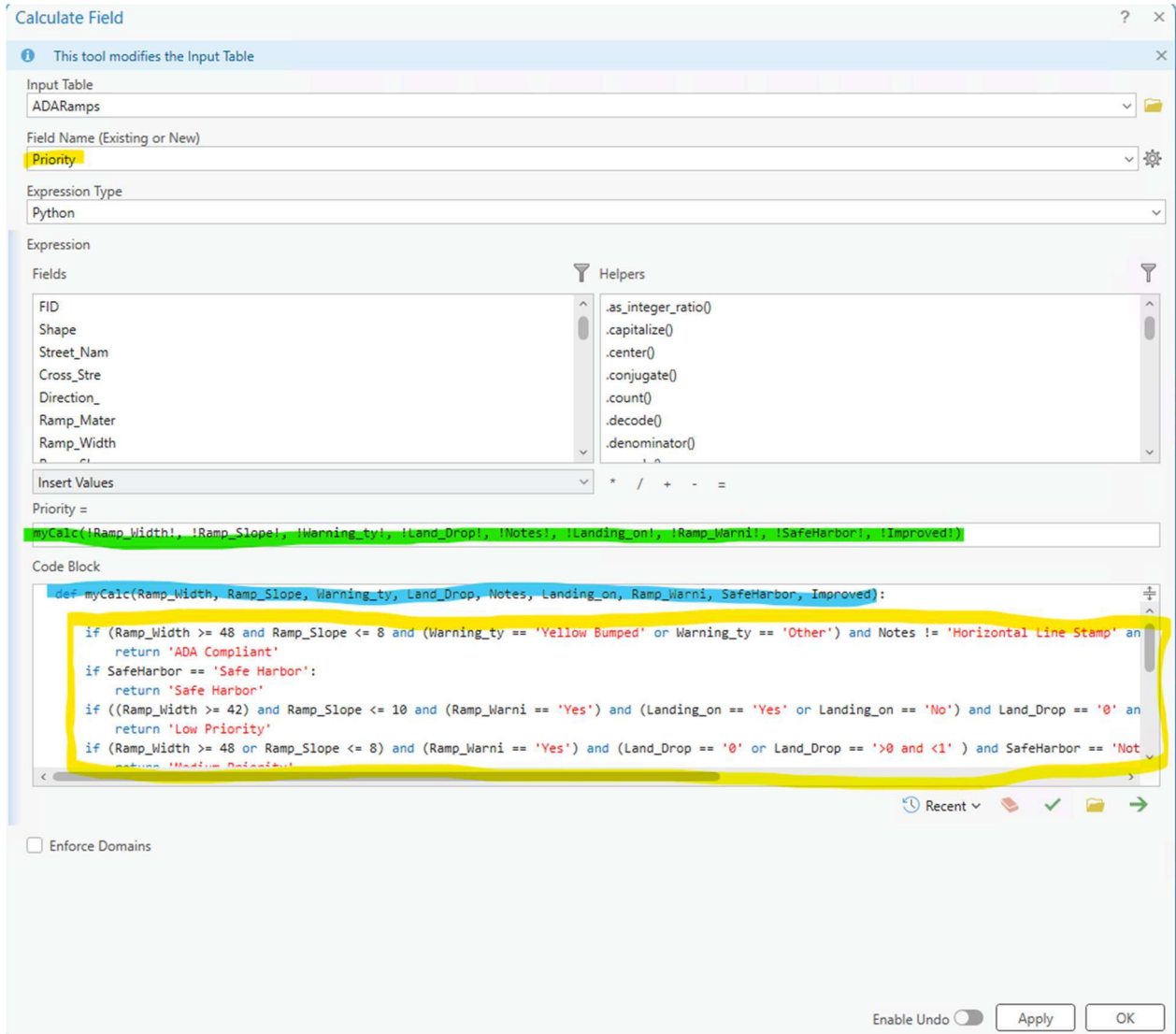
```
        return 'Medium Priority'
```

```
    if (Ramp_Width < 48 and Ramp_Slope > 8) or (Land_Drop == '1' or Land_Drop == '1.3' or Land_Drop == '1.5'
or Land_Drop == '2') or Ramp_Warni == 'No' and SafeHarbor == 'Not Protected':
```

```
        return 'High Priority'
```

```
    else:
```

```
        return 'Undetermined'
```



## Safe Harbor

04/09/2025

### Result: 11 ramps protected by Safe Harbor

```
def myCalc(Ramp_Width, Ramp_Slope, Warning_ty, Land_Drop, Notes, Landing_on):
```

```
    if (36 <= Ramp_Width < 48) and Ramp_Slope <= 8 and (Warning_ty == 'Yellow Bumped' or Warning_ty ==
'METAL BUMPED' or Warning_ty == 'METAL BUMPS') and Landing_on == 'Yes' and Land_Drop == '0':
```

```
        return 'Safe Harbor'
```

```
    else:
```

```
        return 'Not Protected'
```

**12/9/2025 Updates by Moe:**

```
myCalc(!Ramp_Width!, !Ramp_Slope!, !Warning_ty!, !Land_Drop!, !Notes!, !Landing_on!, !Ramp_Warni!,
!SafeHarbor!, !Improved!)
```

```
def myCalc(Ramp_Width, Ramp_Slope, Warning_ty, Land_Drop, Notes, Landing_on, Ramp_Warni,
SafeHarbor, Improved):
```

```
    Improved = str(Improved).strip()
```

```
    if Improved == 'Yes,C':
```

```
        return 'ADA Compliant'
```

```
    if Improved == 'Yes':
```

```
        return 'Low Priority'
```

```
    if Improved == 'No':
```

```
        return 'High Priority'
```

```
    if (Ramp_Width >= 48 and Ramp_Slope <= 8 and (Warning_ty == 'Yellow Bumped' or Warning_ty == 'Other')
and Notes != 'Horizontal Line Stamp' and Landing_on == 'Yes' and Land_Drop == '0'):
```

```
        return 'ADA Compliant'
```

```
    if SafeHarbor == 'Safe Harbor':
```

```
        return 'Safe Harbor'
```

```
    if (Ramp_Width >= 42 and Ramp_Slope <= 10 and Ramp_Warni == 'Yes' and Landing_on in ('Yes', 'No') and
Land_Drop == '0' and SafeHarbor == 'Not Protected'):
```

```
        return 'Low Priority'
```

```
    if (Ramp_Width >= 48 or Ramp_Slope <= 8) and Ramp_Warni == 'Yes' and (Land_Drop == '0' or Land_Drop ==
'>0 and <1') and SafeHarbor == 'Not Protected':
```

return 'Medium Priority'

if (Ramp\_Width < 48 and Ramp\_Slope > 8) or (Land\_Drop in ('1', '1.3', '1.5', '2')) or (Ramp\_Warni == 'No' and SafeHarbor == 'Not Protected'):

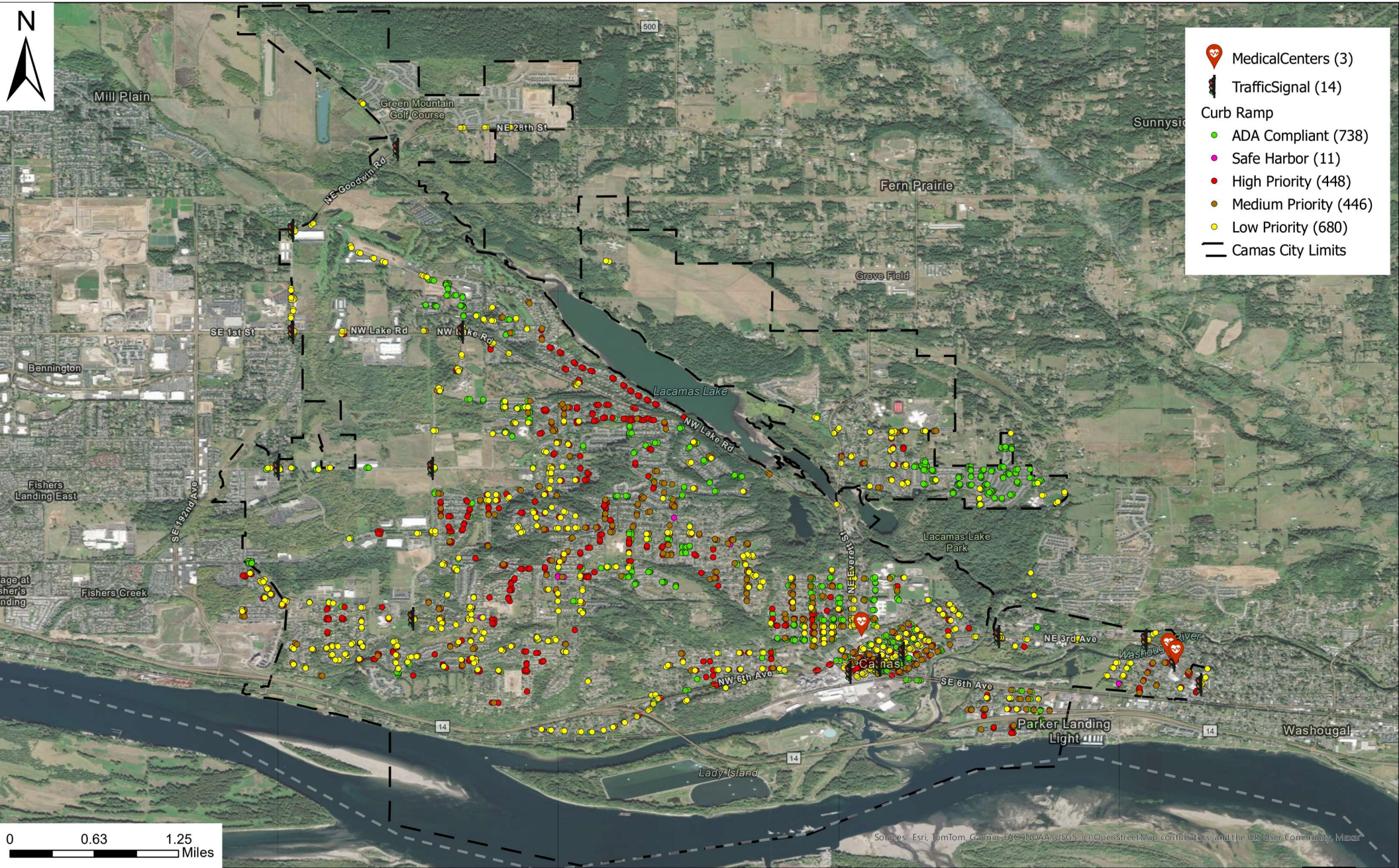
return 'High Priority'

return 'Undetermined'

# Appendix H

## Citywide Map

# City of Camas Curb Ramp Inventory by ADA Compliance

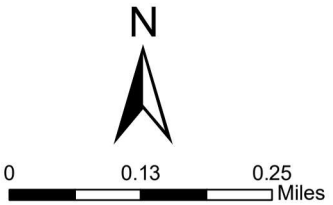


# High and Medium Priority Locations



- Curb Ramp**
- ADA Compliant
- Safe Harbor
- High Priority
- Medium Priority
- Low Priority
- Camas City Limits
- ▨ High Priority Sections
- ▨ Medium Priority Sections
- 🚦 TrafficSignal
- 📍 MedicalCenters

Maxar, Microsoft, Sources: Esri, TomTom, Garmin, FAO, NOAA, USGS, (c) OpenStreetMap contributors, and the GIS User Community





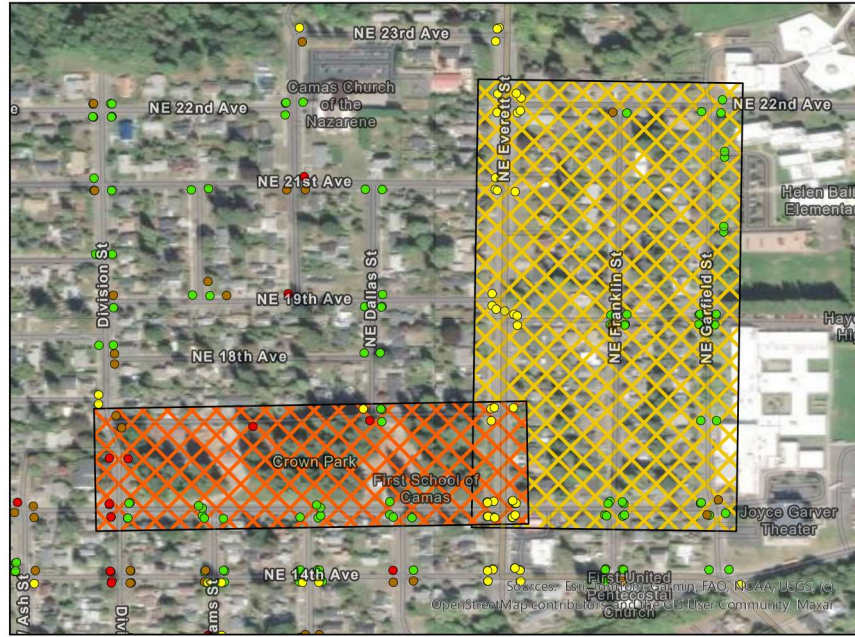
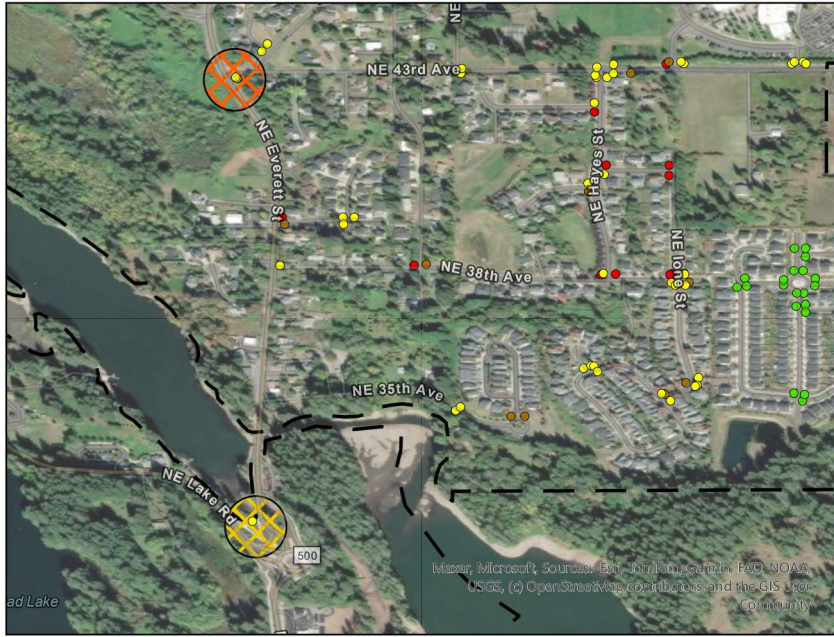
# High and Medium Priority Locations



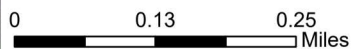
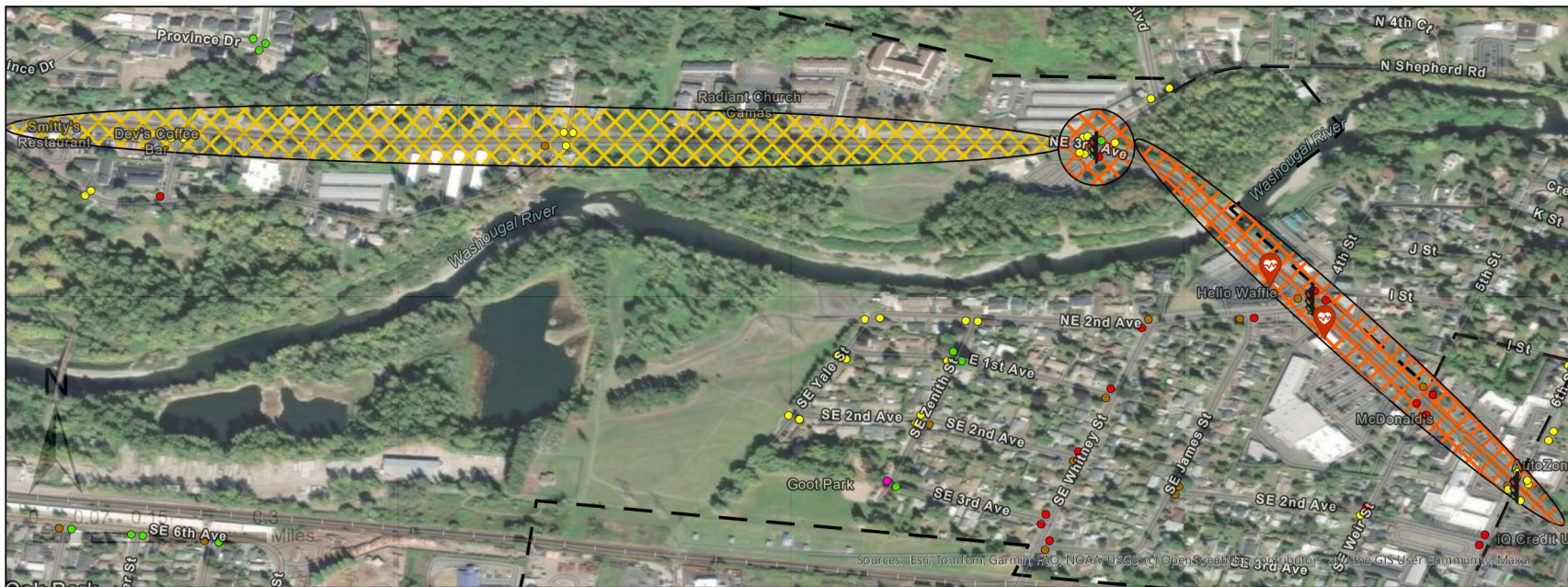
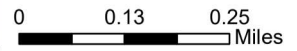
- Curb Ramp
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Maxar, Microsoft, Sources: Esri, TomTom, Garmin, FAO, NOAA, USGS, © OpenStreetMap contributors, and the GIS User Community

# High and Medium Priority Locations



- Curb Ramp**
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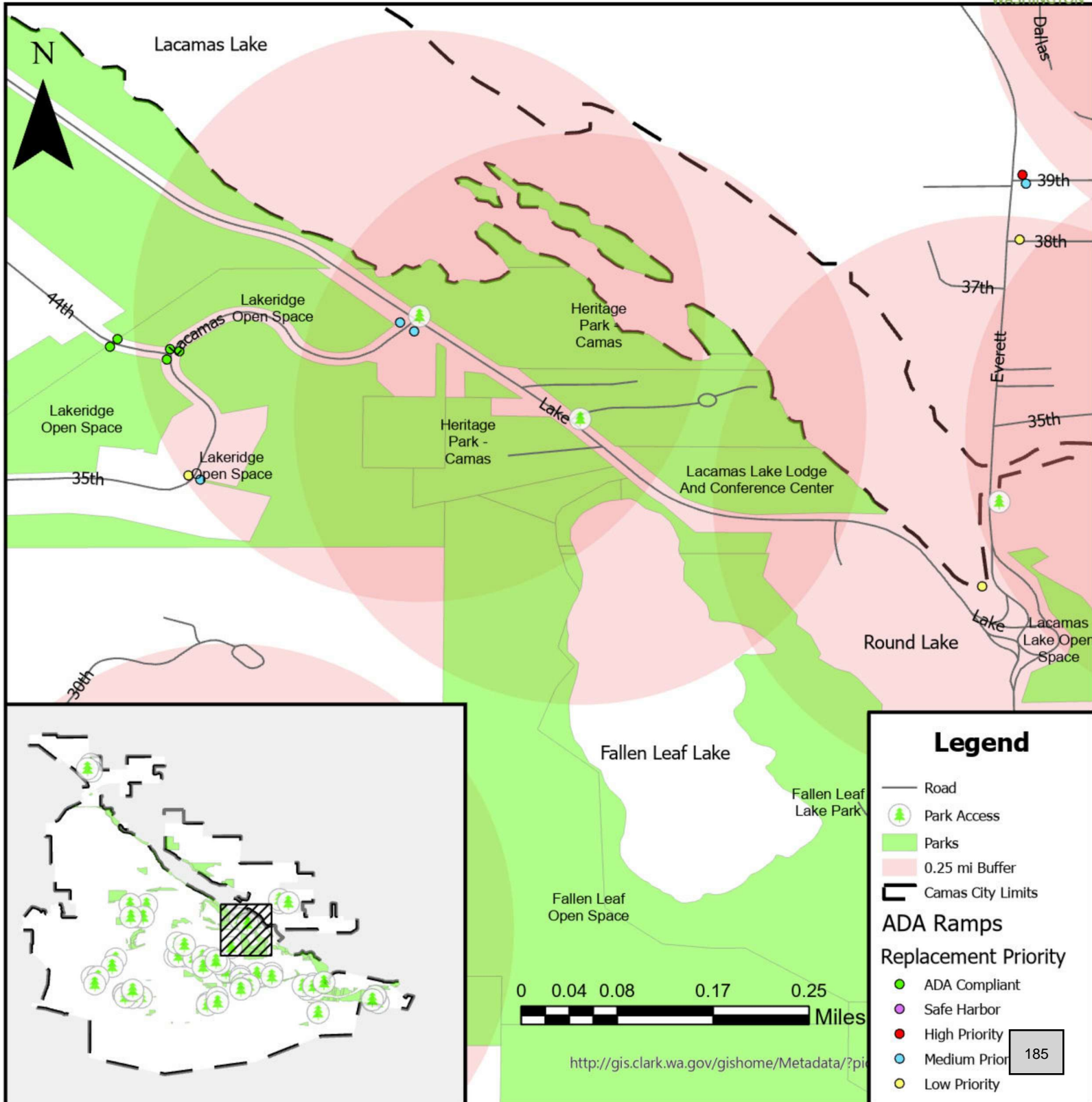
# Appendix I

## Lake Parks Maps

# Camas ADA Transition Plan Update

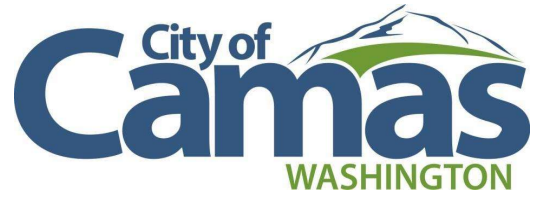
Item 1.

## Ramps Within 1/4 Mile of Lake Park Access



# **Appendix J**

## **2015 Self Evaluation Plan**



# SELF EVALUATION REPORT FOR THE PUBLIC RIGHT OF WAY

May 7, 2015



Prepared for:  
**The City of Camas Public Works Department**

Prepared by:  
**HDJ Design Group PLLC**  
in conjunction with:  
**Urbane Streets**



## **Mission Statement**

*The City of Camas commits to preserving its heritage, sustaining and enhancing a high quality of life for all its citizens, and developing the community to meet the challenges of the future. We take pride in preserving a healthful environment while promoting economic growth. We encourage citizens to participate in government and community, assisting the City in its efforts to provide quality services consistent with their desires and needs.*

## **Americans with Disabilities Act & Title VI information**

**Americans with Disabilities Act (ADA) Information:** Materials can be provided in alternative formats by contacting Jim Carothers, pending City Transportation ADA Coordinator at 360-817-1561 x4230 (voice) or [jcarothers@cityofcamas.us](mailto:jcarothers@cityofcamas.us) (email). Persons who are deaf or hard of hearing may make a request for alternative formats through the Washington Relay Service at 7-1-1.

**Title VI Notice to Public:** It is the City of Camas' policy to assure that no person shall, on the grounds of race, color, national origin or sex, as provided by Title VI of the Civil Rights Act of 1964, be excluded from participation in, be denied the benefits of, or be otherwise discriminated against under any of its federally funded programs and activities. Any person who believes his/her Title VI protection has been violated may file a complaint with the Title VI Coordinator. For Title VI complaint forms and advice, please contact the City's Administrative Services Director who is the Title VI Coordinator at 360-817-7013 (voice).

## Acknowledgements

### **The City of Camas City Council**

Scott Higgins, Mayor  
Greg Anderson  
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Shannon Turk

### **The City of Camas**

Pete Capell  
James Carothers  
Wes Heigh  
Jeff Englund  
Jim Hodges  
Ronda Syverson

### **ADA Community Advisory Committee for the Public Right of Way**

Tom Anderson  
Barbara Blair  
Michelle Cousins  
Sherri Dickerson  
Michael Frazer  
Aileen Gillespie  
Kristine Graham  
Christine Kamps  
Karl Martin  
Carol Popi

### **Submitted by**

HDJ: John Manix, PTOE

### **In Association with**

Urbane Streets: Todd Boulanger, MURP

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## Overview

The Americans with Disabilities Act (ADA) and Section 504 of the Rehabilitation Act of 1973 requires cities with more than fifty employees to prepare a Self-evaluation and ADA Transition Plan. The Self-evaluation activity is intended as an inventory of existing conditions of the physical conditions, as well as policies and procedures. The ADA Transition Plan is intended to build on the ADA Self-evaluation Report and provide a formal assessment as to how to eliminate barriers to the disabled.

The City of Camas has an existing ADA Transition Plan for its public buildings, hiring practices, and meeting procedures. The current effort is intended to address the built environment in the public right of way. The ADA and Section 504 emphasize that cities engage members of the disabled community in preparing their Self-evaluation Report and ADA Transition Plan.

The goal of this process is to complete a high quality ADA Transition Plan covering the near term for the City of Camas that reflects public input, especially from members of the disabled community.

## Public Right of Way Facilities Self-evaluation Report

The Americans with Disabilities Act requires public participation in the preparation of the Self-Evaluation Report. This self-evaluation report focuses on the public right of way with a primary emphasis on public streets. The City of Camas has invited community members to participate in a community advisory committee. Staff and their representatives prepared “findings” for the committee to consider that will be used to prepare the ADA Transition Plan, which will be a living document that will establish a list of high priority projects for approximately the next ten years.

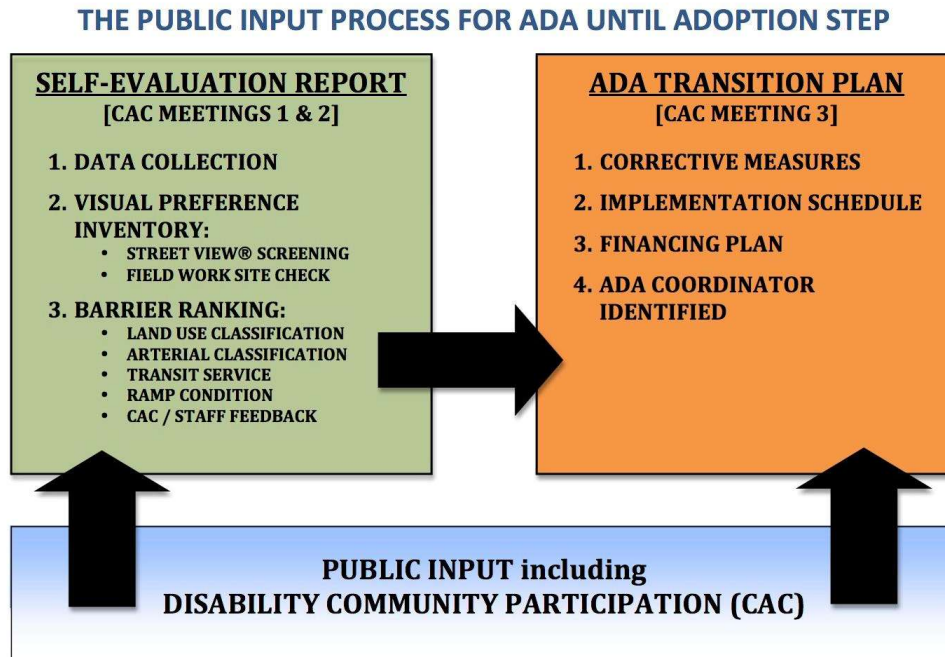
The Sidewalk Self-Evaluation Report and ADA Transition Plan will go before City Council for final approval.

### Self-evaluation Report for the Public Right of Way

The Self-evaluation Report should produce the minimum:

- A list of interested persons consulted;
- A description of the policy areas and procedures examined and any problems identified;
- A description of any modification made;
- A citizen comment / grievance procedure related to sidewalk accessibility; and
- A designated individual to oversee the ADA Transition Plan.

Figure 1: Flowchart of the public process



Urbane Streets 2014

### The ADA Transition Plan for the Public Right of Way

The Self-evaluation Report is then used to prepare an ADA Transition Plan that will be used to develop a schedule for correcting barriers to the disabled community.

## City of Camas Public Right of Way Policy and Practices Review

### Summary

The Department of Justice provides a Title II Checklist for local governments to use to evaluate their compliance under the Americans with Disabilities Act. See Appendix A for the Title II checklist. The findings from this process were then used to develop the recommendations for changes in policies and practices. These were prepared to assure the City of Camas is in overall compliance with Title II of the ADA and in doing so meeting the needs of the community. A full listing of each finding with its corresponding recommendation(s) can be found in Appendix B.

The City of Camas' Transportation policies and procedures were reviewed with respect to compliance with Title II requirements and to verify if such are creating barriers denying access to transportation facilities in the public right of way.

This included review of:

- The Transportation component of the City’s Comprehensive Plan;
- City of Camas Municipal Code, Title 12.04 020: Streets and Sidewalks;
- City of Camas Standard Plans and Street Standards; and
- Clark County Bicycle and Pedestrian Master Plan (2010).

### Findings

The recently updated City of Camas Design Standard Manual (October 2014) details included are in compliance with current ADA standards.

There are additional design issues and decisions that can be addressed, such as retrofitting urban streets often requires maneuvering around utility poles, adding ramps when sidewalk gaps are located midblock, and other obstructions, as well as complications not covered in the Camas Design Standards Manual. Many self-evaluations find that these physical gaps often occur at the edge of new developments or during half street improvements when one side of the road is compliant but not the other. This can be accomplished by supplementing the current street details with the current standards, such as the WSDOT Design Manual or the WSDOT Field Guide for Accessible Public Right of Way, 2012 Edition. Citizen committees will find the latter document is more user friendly.



Figure 2: Examples of accessibility barriers in a developing pedestrian network

### Recommendation 1:

Modify the Camas Design Standard Manual to include reference to the WSDOT Field Guide for Accessible Public Right of Way, 2012 Edition.

### Findings

The City of Camas Design Standard Manual does not provide staff specific direction about what alterations to an existing roadway facility triggers the need to upgrade a facility to meet ADA standards related to providing curb ramps at pedestrian crossings.

**Recommendation 2:**

Modify the Design Standard Manual to include specific policies and procedures to assure that altered pedestrian crossings are upgraded to full ADA standards.

**Findings**

In projects where this is necessary, take special care during the construction inspection to verify the ramp dimensions meet the design. Through the design, construction, and inspection process the resulting curb ramps are sometimes found out of full compliance in the final inspection. In some projects with federal funding, agencies have been made to replace newly constructed curb ramps that are out of compliance.

**Recommendation 3:**

Modify the City's Design Standard Manual to warn against designing to the maximum or minimum dimensions allowed.

**Findings**

It is important to note that the US Department of Justice does not consider cost as a valid consideration, but the federal accessibility standards only require compliance within the scope of the project.

If the original scope of work only included replacing the ramps, then reconstructing the whole intersection to meet grades is an example of major scope of work change that can be used to rationalize not needing to complete all ADA standards for a single intersection.

**Recommendation 4:**

Modify the City's Design Standard Manual to include documentation of criteria that prevent specific locations from meeting full ADA compliance, such as right of way purchase or utility relocation, when these are not required elsewhere in the project.

**Findings**

With alteration, ADA upgrades are required at traffic signals to meet the 2009 edition of the Manual on Uniform Traffic Control Devices (MUTCD) standards for accessible pedestrian signals (APS). Most traffic signals in Camas do not meet APS standards at this time.

**Recommendation 5:**

Upgrade traffic signals at high priority locations that do not meet current reach standards for either height or distance. As traffic signals are modified, they all should be upgraded to current APS standards.



**Figure 3: Examples of pedestrian push buttons, inaccessible versus accessible**

**Findings**

Washington State Law requires upgrade of sidewalk ramps across the street but only requires one (1) additional receiving ramp.

**Recommendation 6:**

Modify the City’s Design Standard Manual to include documentation that requires a matching sidewalk ramp at the other end of a crosswalk, if the sidewalk and curb exist, so as to comply with RCW 35.68.075(3).

**Findings**

The provision of accessibility for persons with disabilities through construction work zones and severe weather is an important issue as our communities become more accessible on a day-to-day basis. This has become an emerging topic of awareness for staff at local governments, as citizen expectations concerning mobility grow. All agencies are required to maintain accessibility that includes pedestrian facilities impacted by overgrown vegetation, snow/ice, severe heaving/cracking of surfaces, construction work zones, and so on, as called for in Chapter 29.3 of the WSDOT LAG Manual. For example, when streets are de-iced/plowed of snow or swept, Camas should consider also performing the same maintenance tasks on the adjoining sidewalks and curb ramps, especially along transit routes and stops. The same evolution of access is currently occurring within construction work zones by providing temporary ADA ramps or trench crossings; and pathway fencing is now more common, where once projects only used caution tape and did not include ADA ramps with a protected detour route.

**Recommendation 7:**

Modify the City’s maintenance procedures to assure the transit routes are barrier free due to damage to walkways, snow events on snow routes, and, update pedestrian detour plans in construction zones to maintain access to the disabled.



**Figure 4: Examples of work zone accessibility, inaccessible versus accessible**

### Findings

“Reasonable” access can be provided by many sub-standard curb ramps. ADA upgrades to existing sidewalks and curb ramps are expensive and funding is limited. In Clark County, Community Development Block Grant (CDBG) funds are programmatically restricted to the construction of new ramps and do not cover the upgrade of existing sub-standard ramps. The inspection of ramps throughout Camas and other small communities throughout Washington State finds a high percentage of ramps within the public right of way out of full compliance with the ADA.

While reasonable access can be provided by sub-standard curb ramps, direction is still needed by staff, as to which sub-standard curb ramps are a priority and should be replaced. To do this, a ranking of replacement ramps is recommended to make best use of scarce resources.

The following criteria were prepared to assist in the inventory and prioritizing locations for replacement of substandard ramps. For projects that “alter” existing pedestrian crossings with substandard curb ramps, particularly at an important destination, any facility defect may require replacement or upgrade.

### Recommendation 8:

To assist in the inventory process, the following criteria are recommended to rank ramps for replacement:

#### High need for replacement:

- Lack of level landing;
- Obstructions or damaged sidewalks;
- Steep grade on ramp throat or ramp wings; or
- A half of an inch or more lip at the curb gutter.

The above deficiencies in curb ramps create a barrier to mobility.

**Medium need for replacement:**

- Level landing near ramp;
- No obstructions or tripping hazards; and
- Less than one quarter of an inch lip at curb gutter.

These are the borderline sidewalk ramps that may be a barrier to accessibility. In some cases they may be upgraded with minor improvements such as a retrofit warning pattern or grinding the curb to eliminate too high of lip.

**Low need for replacement:**

- Ramps with detectable warning patterns;
- Level landing behind ramp;
- No obstructions such as utility poles or tripping hazards (one half of an inch high uplifted sidewalk panel);
- Less than a one quarter of an inch lip at curb; and
- The ramp throat is less than three feet wide.

**Findings**

The Department of Justice (DOJ) provides precedence with the “Safe Harbor” (§ 35.150(b)(2)(i)) provision that does not require upgrade of substandard ramps built before 2012 and meet the 1991 standards unless they are part of a planned alteration. Any street pavement restoration project or other physical alteration that affects a pedestrian crossing after 2012 are required to be upgraded by the US Department of Justice.

**Recommendation 9:**

Review pavement management program for streets altered after 2012. Program ADA improvements at these pedestrian crossings in the next phase of the ADA Transition Plan.

**Findings**

Fixed route transit provides persons with disabilities an important option for mobility; C-TRAN Route 92, though limited in service hours, proves riders with an important transportation option, and one that does not require making an advance reservation.

**Recommendation 10:**

Check that sidewalk work planned and standard details affecting arterials with fixed route transit service accommodate the eight-foot deep transit loading pads and bus shelter pads as needed. Coordinate with C-TRAN staff during the review process. Use C-TRAN’s 2007 bus stop design guidelines when appropriate.

**Citizen Request and Grievance Procedure**

The City of Camas is required to adopt and publish procedures for resolving requests and/or grievances arising under Title II of the ADA. The procedure is

intended to set out a formalized system for resolving complaints of disability discrimination within the time periods set forth. It is important that complaints are resolved in a timely manner and at the local agency before they are directed to the US Department of Justice. See Appendix A for more information on the content of the checklist.

### Findings

The City of Camas currently reacts to citizen requests concerning ADA barriers on existing public transportation facilities through a three-step approach:

1. All publicly reported comments are logged by the Senior Administrative Support Assistant;
2. The requests are then prescreened for responsibility by the Senior Engineering Technician; and
3. The Engineering Manager undertakes the resolution of the complaint.

### Recommendation 11:

The City of Camas should adopt a formal Citizen Request and Grievance Procedure as outlined in Appendix F, and have this document prepared in alternative formats. The City of Camas should give priority to requests for ADA upgrades, but if a request is reviewed and found that it is not appropriate to fulfill then a meeting should be set up to discuss such a response should occur within the fifteen day time period set forth by the US Department of Justice. Additional information about the appeal process to the City Administrator and its deadlines must be included in the follow up notification if no resolution is reached.

### ADA Coordinator for the Public Right of Way

The selection of the City's ADA Coordinator for the Public Right of Way was one of the first actions undertaken by this process. The City of Camas has designated James Carothers, Engineering Manager, as its first ADA Coordinator for the Public Right of Way per Title II. This action formalizes his current role in processing citizen requests at the City for ADA barriers in the public right of way.

The City Administrator will continue with their existing responsibilities for Civil Rights under the Title VI Program.

### Recommendation:

The City of Camas shall make available to all interested people the name, office address, and telephone number of the ADA Coordinator for the Public Right of Way. Additionally, the city should post on its website if individuals needing a (TTY) teletypewriter / text emulator to communicate should dial direct a city phone number or dial 711 at the state.

## Public Involvement Process

### Public Notice:

Public notice of the Self-Evaluation and the ADA Transition Plan is required throughout the process. It should include at a minimum:

- Providing opportunity to comment on the development of the Self-evaluation Report and ADA Transition Plan through the citizen committee and press releases for each committee meeting.
- Keeping record of comments received for 3 years.
- Keeping the Self-Evaluation Report and ADA Transition Plan available to the public.

### Public Outreach

The participation element for this report was conducted through repeated public advertisement of this planning activity in newspapers of public record, public bulletins, and word of mouth spread through local service providers. The message included the opportunity to serve on the future advisory committee when formed and that the City meetings were open to attendance for all. There was some initial difficulty at identifying potential local committee members, as Camas is a small city with many specialized resources located in the larger adjoining communities. But this was overcome through the dedicated assistance of the C-TRAN staff that was well acquainted with potential candidates.

### Agencies & Service Providers:

- C-TRAN;
- Clark County Community Services;
- Northwest ADA Center;
- People First of Clark County/ ARC of SW Washington;
- Prestige Care and Rehabilitation of Camas;
- Vancouver Housing Authority;
- Washington State Division of Vocational Rehabilitation;
- Washington State School for the Blind; and
- Washington School for the Deaf

These entities when approached were very interested in making their community a better place and eagerly supported the process. This committee once formed was composed of persons with disabilities, formal service providers, and informal caregivers. Additionally, an attempt was made to select participants with direct experience travelling independently through most districts of Camas and with a range of physical mobility such as: low vision, ambulatory with aid of a cane or walker, power wheel chair, etc. so that any feedback from the committee on barriers in the public right of way would be both comprehensive (as to aid) and broad (type or location of barrier).

The City Hall was chosen as the meeting location because of its central location and being fully accessible. Meeting dates were established to avoid summer and winter holidays. The meeting times and room access were organized, so as to allow paratransit service drop off and pick up before the 7:15 PM service end for the Camas area. Additionally, for our visually impaired committee member the prepared meeting agendas and other materials were provided in text format via email for their conversion into Braille, as requested. Other documents, such as maps were verbally described in detail.

### **Process Kick-Off Meeting with City of Camas Staff**

On August 28, the project self-evaluation process started off with a meeting with City staff from the public works department to discuss how ADA requests are managed within the City for the public right of way. There are three primary sources to fund the reconstruction of sidewalk and curb ramps to remove ADA barriers: annual CDBG grant awards for ramps, the City reconstruction activities when ad hoc requests/ complaints are made by citizens, and by property owners when properties are developed or complaints arise. The meeting also included discussion of which City staff position would be best to be the future ADA Coordinator for the Public Right of Way.

### **ADA Community Advisory Committee for the Public Right of Way**

The Community Advisory Committee (CAC) assisted the Public Works Department and consultant staff in the preparation of the City-wide Self-evaluation and ADA Transition Plan for the public right of way during three meetings held during the fall and winter of 2014.

### **Objectives**

1. Establish criteria to prioritize locations;
2. Prioritize the streets for high, medium and low need to remove barriers to the disabled;
3. Establish a list of projects that will eliminate barriers in the public right of way at high priority locations;
4. Establish a schedule for completing the high priority projects;
5. The ADA plan will provide City staff with policy and procedure recommendations to deal with ADA upgrades in the future as opportunities presents themselves; and
6. Complete the plan with three CAC meetings, as a minimum.

The committee assistance focused on setting criteria for prioritizing locations, providing feedback on the locations for ADA upgrades, on the scope of work for high ranked projects and the schedule for completion.

### **CAC Meeting One Findings – September 30**

The first CAC committee meeting established the project outcomes and meeting processes for the CAC members. The CAC members discussed their broad interests

and motivations for assisting the City of Camas with reducing barriers in the public right of way. They also discussed what geographic areas of the city and destinations they frequently used and which ones the CAC may want to be included in the high and medium priority list.



**Figure 5: CAC members at work, in meeting and in field accessibility tour**

### **Project Workshop with the Mayor and City Council – November 3**

The project added a workshop with City Council on November 3 to the public process scope of work. This change was an opportunity to raise their awareness of the public comments generated by the Self-evaluation process in regards to common barriers experienced by our CAC representatives and geographic areas that would likely be targeted in the upcoming ADA Transition Plan project list.

### **CAC Meeting Two Findings – November 4**

The second CAC meeting focused on the CAC members and staff review of the emerging high and medium priority locations generated out of the initial project screening process. The CAC members reiterated that the zone around the Crown Point Park should be a high priority area due to the importance of the City recreational facilities there for persons with disabilities and the proximity to the Vancouver Housing Authority's affordable housing units. Additional locations and corridors were added to the review process at the request of the CAC, such as private medical offices. A few proposed locations were removed due to recent or pending capital contraction work. This discussion often led to secondary but important issues of the range of design enhancements that each location could see in the future, such as the role of pedestrian refuges, signal timing, and traffic speeds play in accessibility; especially for the elderly in crossing accessible streets. The CAC discussed and accepted the consultant's recommendation to classify intersections among the "highest" priority locations due to a barrier there affecting access to two or more arterials. These locations also often provided access to fixed route transit stops.

### **CAC Meeting Three Findings – December 9**

The third CAC committee meeting finalized the Self-evaluation report findings and focused on the ADA Transition Planning. There was much discussion in the CAC of

the role of the ADA and a clarification that it does not directly fund projects even though the ADA is a federal legal mandate. This then led to defining what the CAC's vision of ADA was for Camas and how this would be reflected in the projects ranked highest versus lower priority locations. The cost of project treatments were reviewed individually and as a whole in light of what local and grant funding exists in the region on any given year.

### CAC Field Workshop on Physical Barriers in the Right of Way

The Community Advisory Committee members and City staff also participated in a field tour of common accessibility barriers affecting urban transportation facilities over their lifespan. The following design issues were identified and discussed during September 30:

- Marked crosswalks with missing sidewalk ramps;
- Ramp directionality (oblique versus parallel to a crosswalk);
- Out of compliance sidewalk ramps (slope and missing landing);
- Sidewalk problems (panel lift, gaps and curb lips);
- Traffic signal accessibility (walk cycle lengths, APS, push button locations);
- and
- Other barriers (vegetation, trash cans, street furniture, etc.).



**Figure 6: Examples of levels of accessibility experienced during the CAC field tour, such as a missing ramp and a ramp with an outdated design**

See Appendix C for more information on the CAC and materials from meetings.

## Inventory

### Public Facilities in the Right of Way Ranking Criteria Methodology

The outcome of ranking projects is to facilitate the management of the “need” with the resources available to complete the objective of making all public pedestrian facilities accessible.

The action of ranking projects is a function of the following criteria:

- **Collision History** for pedestrians and bicyclists (vulnerable roadway users) is used to quantify risk exposure for persons with disabilities;
- **Government Buildings** that are important destinations for all citizens.
- **Roadway Classification** for arterials, not local streets; is a measure of greater exposure (higher risk) to traffic injuries;
- **Land-use Zoning** of commercial, not residential land, is used to measure likelihood of pedestrian traffic generation;
- **Medical Services** and other facilities important to persons who are disabled;
- **Fixed Route Transit Service** is used to measure likelihood of pedestrian traffic; and
- **Committee Identified Priority Sites** these locations have been identified through the CAC public process and added to the priority list based on local knowledge for as facilities important to persons who are disabled.

Pedestrian safety is an important consideration in ranking potential sites and can be done based on exposure to high volume and high-speed motor vehicle traffic. Typically the arterial roadway classification is a good starting point for judging the likely exposure levels, as higher order arterials have more than collector or local streets. Thus high priority locations have most if not all criteria present while medium have fewer. And any public streets not listed below are considered low priority, as they generally have low traffic volumes in residential areas and often are without reported crash history. These street facilities also generally do not have transit service or public facilities serving persons with disabilities thus reducing their importance for this community wide planning process.

The priority locations are also organized by type (intersection, corridor, and area) for purposes of easier discussion in identification and treatment types. All facilities types are collectively equal within each priority tier. The type of location becomes important later when mitigations or treatments are proposed.

See Appendix D for high and medium priority location memo and maps.

#### **Summary of inventory:**

Based on field observations of the public right of way, the City of Camas provides reasonable access throughout the community. Most intersections with sidewalks have curb ramps. The older ramps are not compliant by today's standards but most provide reasonable access. The downtown intersection and sidewalk are good examples of sidewalks that do not meet all standards but are very accommodating. The sidewalks on newer streets, especially the newer arterials, where constructed under the ADA standards of the day. These ramps provide reasonable access and should be a low priority to be replaced due to their functionality for the interim period.

The two areas where curb ramps are frequently substandard are the lip where the sidewalk ramp meets the gutter at street level and the lack of the detectable warning pattern for the sight impaired. The lip at the gutter should be flush or no greater than one quarter of an inch in height. Older ramps do not have the warning pattern to provide a notice to the sight impaired they are about to enter a vehicle travel way. With modern concrete grinding equipment and after-market detectable warning panel, it is assumed these ramps can be quickly improved and at a reasonable cost overall.

The field inspection of existing sidewalks noted many locations with damage that exceeded ADA standards. These were listed in the inventory of the sidewalks. Most of these are along private property frontages and were considered to be the responsibility of the butting property to address, per city code. In some cases, sidewalk was missing but ADA does not require sidewalk construction just that new and existing sidewalk do not pose a barrier. In some case with short sections of missing sidewalk, installation was recommended.

Driveway approaches are a challenging consideration. Most sidewalks attached to the curb have driveways too steep to meet ADA standards. The field inventory considered that a driveway approach less than eight feet in sidewalk width was so steep in cross slope, so as to be a physical barrier and need to be replaced. And a driveway approach wider than eight feet provided reasonable access even though they may not meet ADA standards for cross slope.

As discussed in the City of Camas Public Right of Way Policy and Practice Review section, staff were supplied with inventory criteria to prioritize work at each location based on reasonable access at existing curb ramps, sidewalks and traffic signals. Each high priority location was further ranked for the need to replace the existing facility based on the level of barrier to the disabled. See Appendix E for details on the ranking of criteria for barriers.

To facilitate planning level estimates of ADA upgrades, the City of Camas' standard plan detail(s) was listed, when applicable, as a recommendation for the scope of the work for the upgrade. Each upgrade was assigned a planning level cost estimate that includes most construction cost such as construction cost, construction administration, traffic control and mobilization but does not include design engineering and right of way.

At traffic signals with one pedestrian push button substantially out of compliance (high priority for replacement), all APS upgrades were assumed at the intersection. See Appendix E for the inventory of high-ranking locations.

## The Self-evaluation Report Conclusions:

- The City of Camas has reasonably good ADA access to transportation facilities in the public right of way;
- The City of Camas' policy and practices should be modified per the recommendations of the Self-evaluation Report;
- Modifications are necessary to achieve ADA compliance and an ADA Transition Plan is required;
- The inventory of high priority locations should be included in the ADA Transition Plan and ADA improvement scheduled at locations that are a barrier to persons with disabilities;
- Locations with a full height curb should be addressed first, as they are major barriers to the disabled community;
- Sidewalks and pedestrian crossings in need of substantial repair should be addressed, as a second priority;
- Traffic signals with pedestrian push buttons beyond the reach standards for ADA should be the highest priority for Accessible Pedestrian Signal upgrades;
- Curb ramps without detectable warning patters or a lip of over a half of an inch at the center, should be third priority for minor upgrades; and
- Adopt the US Department of Justice ADA Grievance Procedure Under the Americans with Disabilities Act.



## **Appendix A – Title II Pre-assessment Checklists for Self-evaluation**

- Checklist for Curb Ramps and Pedestrian Crossings
- Checklist for the ADA Coordinator, Notice and Grievance Procedure

## City of Camas ADA Coordinator, Notice and Grievance Procedure Title II Pre-assessment Checklist for Self-Evaluation

### PURPOSE OF THIS CHECKLIST:

This checklist is designed for use as an assessment of the requirements and tasks of an ADA Coordinator, the government entity's provision of the ADA notice, and the government entity's ADA grievance procedures.

### ADA COORDINATOR:

1. Does the state or local government have an ADA Coordinator? **[Response: YES]**
2. Does the ADA Coordinator have the time and expertise necessary to coordinate the government's efforts to comply with and carry out its responsibilities under the ADA? **[Response: YES]**
3. Does the ADA coordinator actually carry out these duties? **[Response: YES]**
4. Does the ADA Coordinator investigate all complaints communicated to the
5. government alleging that the government does not comply with the ADA? **[Response: YES]**
6. Does the government make available to all interested people the name, office address, and telephone number of the ADA Coordinator? **[Response: YES]**

### NOTICE:

1. Does the local government make information available to the general public regarding the fact that the ADA applies to the services, programs, and activities of the government? **[Response: YES]**
2. Does the local government use the Department of Justice's model "Notice Under the Americans with Disabilities Act" or a similarly comprehensive notice? **[Response: YES]**
3. Does the local government post this information in public areas or make it available in other ways as deemed necessary by the head of the government entity to inform people of the protections of the ADA? **[Response: YES]**
4. Is the ADA notice available in alternate formats – i.e., large print, Braille, audio format, accessible electronic format (e.g., via email, in HTML format on its website)? **[Response: YES]**

### GRIEVANCE PROCEDURES:

1. Does the local government have a grievance procedure? **[Response: YES]**
2. Does the local government use the Department of Justice's model "Grievance Procedure under the Americans with Disabilities Act" or a similarly comprehensive grievance procedure? **[Response: NO]**
3. Is the grievance procedure available in alternate formats? **[Response: NO]**

*NOTE: This is an abridged version of the complete DOJ text for purposes of improving readability and accessibility within this report.*

## City of Camas Curb Ramps and Pedestrian Crossings Title II Pre-assessment Checklist for Self-Evaluation

### Evaluating Compliance with the Requirements for Curbs at Pedestrian Crossings

Review the policies, procedures, and contracts your entity has used relating to the construction, alteration, and repair of curb ramps. Responded for the period since **January 26, 1992\*** for the City of Camas WA:

- 1) Has your entity implemented\* policies and procedures to ensure that curb ramps or other sloped surfaces were provided wherever walkways intersected curbs whenever your entity constructed or altered highways, streets, roads, pedestrian crossings, and sidewalks? **[Response: NO]**
- 2) Has your entity implemented\* policies and procedures to ensure that curb ramps at pedestrian crossings were constructed and altered in compliance with either the ADA Standards for Accessible Design or the Uniform Federal Accessibility Standards? **[Response: YES]**
- 3) Review any standardized curb ramp designs and specifications that your entity has used.\* **[Response: YES]**
- 4) Survey a sample of the pedestrian crossings on portions of “streets” that were constructed\* by or on behalf of your entity and a sample of the curb ramps at pedestrian crossings on portions of “streets” that were altered.
  - a. Do all curbs have curb ramps that allow people with disabilities to go from the sidewalk on one side of the vehicular way across ...to the sidewalk on the opposite side? **[Response: NO]**
  - b. Are all of these curb ramps free of accessibility problems? **[Response: NO]**
- 5) Has your entity performed an evaluation of its pre-ADA pedestrian crossings to identify the locations where curb ramps need to be constructed to provide program access for people with disabilities? **[Response: NO]**
- 6) If the answer to #5 is “Yes,” has your entity been implementing those curb ramp installations as it implements its long-range plan for streets and sidewalks? **[Response: N/A]**
- 7) Does your entity seek input from people with disabilities with respect to its plans for the construction and alteration of highways, streets, roads, sidewalks, and pedestrian crossings? **[Response: NO]**
- 8) Does your entity have a mechanism that people with disabilities can use to request the installation or repair of a curb ramp? **[Response: YES]**
- 9) If your answer to Question #8 is “Yes,” does your entity also have procedures to ensure that such requests are given priority when your entity plans and implements the construction and alteration of streets, roads, highways, sidewalks, and pedestrian crossings? **[Response: NO]**

*Note: This is an abridged version of the complete DOJ text for purposes of improving readability and accessibility within this report.*

## **Appendix B – City of Camas Policies and Procedures**

- Camas ADA Plan – Policies and Procedures Review and Recommendations Memo (draft)

## Memo

**To:** Curleigh Carothers  
**From:** John Manix, PE  
**Dated:** December 2, 2014  
**Regarding:** Camas ADA Plan – Policies and procedures review and recommendations

The Department of Justice provides a Title II Checklist for local governments to use to evaluate their compliance under the Americans with Disabilities Act. See Appendix A for the Title II checklist. The following recommendations were prepared to assure the City of Camas is in compliance with Title II of the ADA and is meeting the needs of the community.

### **Alterations:**

The City of Camas Design Standard Manual provides no specific direction about what triggers the need to upgrade a facility to meet ADA standards related to providing curb ramps at pedestrian crossings. Typically, the upgrades are required with any physical modification or “alteration” to the street or sidewalk. For example, if a sidewalk has any work that touches the corner with a substandard curb ramp, upgrades to the ramp would be required as part of the project. It is important to note that paving, repaving, and resurfacing projects trigger the need for ADA upgrades at pedestrian crossings. Exceptions are made for typical maintenance such as filling pot holes. See Appendix B for BDOJ/DOT Joint Technical Assistance on Title II requirements. Another important exception is on streets without walkways. ADA does not require installation of curb ramps in the absence of a pedestrian walkway.

**Recommendation:** Modify the City’s Design Standard Manual to include specific policies and procedures to assure that altered pedestrian crossings are upgraded to full ADA standards.

Traffic signals are now required to be constructed to Accessible Pedestrian Signal (APS) standards as outlined in the most recent *Manual on Uniform Traffic Control Devices (MUTCD)*, Ninth Edition. WSDOT Local Programs provides guidance on what signal modifications constitutes the need for APS upgrades. This is an evolving policy but for the short term any modification to the pedestrian components of a traffic signal should trigger the need to upgrade traffic signals to meet APS standards.

**Recommendation:** Modify the City’s Design Standard Manual to include a policy that the modifications to pedestrian components such as pedestrian signal heads or push buttons will require APS upgrades.

### **Design Standards:**

The Street Details as revised 10/21/14 of the City of Camas Design Standard Manual for construction of sidewalk and curb ramp design were reviewed for compliance with proposed Public Right of Way ADA Guidelines (PROWAG) and WSDOT’s *Field Guide for Accessible Public Right of Way*, 2012 Edition. The details shown are in

compliance with current standards. Retrofitting urban streets often requires maneuvering around utility poles and other obstructions as well as complications not covered in the Camas Design Standards Manual. The WSDOT Field Guide provides additional information in a user friendly format on Pedestrian Access Routes, pedestrian push buttons and Accessible Pedestrian Signals (APS) to name a few.

**Recommendation:** Supplement the current Street Details with WSDOT's *Field Guide for Accessible Public Right of Way*.

Through the design, construction, and inspection process the resulting curb ramps are sometimes found out of full compliance in the final inspection. In some projects with federal funding, agencies have been made to replace new curb ramps that are out of compliance. This is often a result of designing to the maximum grade allowed by ADA standards and in the construction process the grades are exceeded.

**Recommendation:** Modify the City's Design Standard Manual to warn against designing to the maximum or minimum dimensions allow. In projects where this is necessary, take special care in the construction inspection to verify the ramp dimensions meet the design.

***Design documentation for less than full ADA compliance:***

*If full ADA compliance cannot be achieved in an alteration, compliance is required to the extent feasible within the scope of the project<sup>1</sup>.* The WSDOT *Local Agency Guidelines* (LAG) manual provides details on the criteria that often prevents agencies for building curb ramps and sidewalks that meet full ADA compliance. It is important to note that the Department of Justice does not consider cost as a valid consideration, but federal accessibility standards only requires compliance within the scope of the project. For example, building improvements to full ADA compliance that would require purchase of any right of way or relocation of utilities, when the original project did not include this in the original scope of work, is an example of a situation when an agency can use this "out of scope" as rational for deviating from full ADA compliance. WSDOT has an established documentation procedure for application of *maximum extent feasible* in alteration of state routes. Local agencies are not required to adopt this procedure but are encouraged to develop their own documentation protocol for such situations.

**Recommendation:** Modify the City's Design Standard Manual to include documentation of criteria that prevent specific locations from meeting full ADA compliance such as right of way purchase or utility relocation when these are not required elsewhere in the project.

***Matching ramps and the other end of a crosswalk:***

RCW 35.68.075(3) requires a matching ramp at the other end of a crosswalk (marking not required) unless there is no curb nor sidewalk. It also does not require a subsequent matching ramp. Thus, if a corner is upgraded, a

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<sup>1</sup> WSDOT *Local Agency Guideline*, April 2014

second ramp (if none is available and sidewalk and curb exist across the street) is required to match the first but a third ramp is not required to match the second ramp.

**Recommendation:** Modify the City’s Design Standard Manual to include documentation that requires a matching ramp at the other end of a crosswalk if the sidewalk and curb exist.

***Reasonable access provided by sub-standard ramps:***

ADA upgrades to sidewalks are expensive and funding is limited. In Clark County, CDBG funds are limited to construction of new ramps and do not cover upgrade of sub-standard ramps. Inspection of ramps throughout Camas and other small communities throughout the state, noted a high percentage out of full compliance. Thus, direction is needed on when to replace sub-standard curb ramps. To do this, a ranking of replacement ramps is recommended to make best use of scarce resources.

The Department of Justice (DOJ) provides precedence with the “Safe Harbor” (§ 35.150(b)(2)(i)) provision that does not require upgrade of substandard ramps built before 2012 and meet the 1991 standards unless they are part of a planned alteration. Any street overlaid or gone through other alteration that effects pedestrian crossing after 2012 are susceptible to upgrade by the DOJ, if they receive a complaint.

To simplify the decision, the following criteria are recommended to rank ramps for replacement:

**Low need for replacement:**

- Ramps with warning patterns
- Level landing behind ramp
- No obstructions such as utility poles or tripping hazards (½-inch-high uplifted sidewalk)
- Less than a ¼-inch lip at curb
- Throat that is three feet wide

Most modern ramps will meet these criteria even without meeting some grades to meet full ADA Compliance. In some important locations, such as at a medial office, these may still need to be replaced.

**Medium need for replacement:**

- Level landing near ramp
- No obstructions or tripping hazards
- Less than ¼ inch lip at curb

These are the borderline ramps that may be a barrier to accessibility. In some cases they may be upgraded with minor improvements such as a retrofit warning pattern or grinding the curb to eliminate too high of lip.

**High need for replacement:**

- Lack of landing
- Obstructions or damaged sidewalks
- Steep grade on ramp throat or wings
- ½ inch or more lip at the gutter.

These are ramps that are so out of compliance they create a barrier to mobility.

These criteria were prepared to assist in the inventory and prioritizing locations for replacement of substandard ramps. For projects that “alter” existing crossing, with substandard curb ramps, particularly at important destination, any defect may require replacement or upgrade.

## **Appendix C – Public Outreach**

- CAC Composition and Accessibility List Memo
- CAC Committee Charter
- CAC Meeting 1 Public Notice
- CAC Meeting 1 Agenda
- Background Presentation to City Council Agenda
- CAC Meeting 2 Public Notice
- CAC Meeting 2 Agenda
- CAC Meeting 3 Public Notice
- CAC Meeting 3 Agenda



23 September 2014

To: John Manix, HDJ Design Group

From: Todd Boulanger, Urbane Streets

Re: Camas ADA Transition Plan - Community Advisory Committee List v2

This is an update of the information for the individuals who have confirmed that they will join the Community Advisory Committee for the Camas ADA Transition Plan.

There are seven (7) confirmed members for our first meeting: four (4) persons with physical disabilities or past injuries affecting their mobility (temporary use of a walker) and three (3) professionals serving either the senior or disabled community. All invitees have some awareness of the built environment and accessibility barriers in Camas, as they either reside/ work in Camas or have clientele in Camas.

NAME	STAKEHOLDER	CAMAS	INVITED	NOTE
Kristine Graham	Citizen – CTRAN CCAC member	Visits	Accepted	Power chair or walker / *
Tom Anderson	Citizen	Works	Accepted	White cane use / * /AOL TXP format for email documents
Aileen Gillespie	Citizen (senior citizen)	Resides	Accepted	Used a walker when injured last year/ *
Carol Popi	Citizen	Resides	Accepted	Cane use / *
Sherri Dickerson	VHA - Crown Villa Apts	Service area	Accepted	Will give Aileen a ride to meetings
Michelle Cousins	ARC - People First of Clark County	Resides	Accepted	Juanita Williams: alternative
Michael Moses	Prestige Care	Works	Accepted	Diana: Alternative
Vera White	Citizen	Resides	Not responding**	White cane use / *

NOTE: \*C-VAN user, so must end meeting by 19:00, or plan for a ride home if ends later than 19:15.

There is an additional person with disabilities who is a potential committee member but has not confirmed our invitation to attend yet.\*\*



# City of Camas

## Self-evaluation and ADA Transition Plan

### Community Advisory Committee Charter

#### Introduction

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The Americans with Disabilities Act (ADA) and Section 504 of the Rehabilitation Act of 1973 requires cities with more than 50 employees to prepare a Self-evaluation and ADA Transition Plan. The Self-evaluation is intended as an inventory of existing conditions of both the physical conditions as well as policies and procedures. The ADA Transition Plan is intended to build on the Self-evaluation and provide a plan to eliminate barriers to the disabled. The City of Camas has prepared an ADA Transition Plan for its public buildings, hiring practices, and meeting procedures. This work is intended to address the built environment in the public right of way. The ADA and Section 504 emphasize that cities engage members of the disabled community in preparing their Self-evaluation and ADA Transition Plan.

#### Mission

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The Community Advisory Committee (CAC) will assist the Public Works Department in the preparation of the City-wide Self-evaluation and ADA Transition Plan (ADA Plan) for the public right of way. The committee assistance will focus on setting criteria for prioritizing locations, providing feedback on the locations for ADA upgrades, on the scope of work for high ranked projects and the schedule for completion.

#### Charter

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The committee will be referred to as the ADA Community Advisory Committee or CAC. Its purpose is described in the mission statement above. The CAC is authorized to provide input to City staff and their consultants on prioritization criteria, high priority locations, and on the scope and schedule.

#### Goals

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Complete a high quality ADA Plan for the City of Camas that reflects public input, especially from members of the disabled community.

#### Objectives

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1. Establish criteria to prioritize locations.
2. Prioritize the streets for high, medium and low need to remove barriers to the disabled.
3. Establish a list of projects that will eliminate barriers in the public right of way at high priority locations.
4. Establish a schedule for completing the high priority projects.

5. The ADA plan will provide City staff with policy and procedure recommendations to deal with ADA upgrades in the future as opportunities presents themselves.
6. Complete the plan in two to three months with three CAC meetings.

## Limitations

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The ADA Plan is only focusing on the public right of way with primary emphasis on City streets. The ADA Plan is a living document that will establish a list of high priority projects for approximately the next 10 years. Medium and low priority locations will be addressed in the next update of the plan.

Comments Needed to Improve  
Accessibility for the Camas  
Disabled

FOR IMMEDIATE RELEASE

Contact: Jim (Curleigh) Carothers,  
Engineering Manager  
City of Camas, WA  
(360) 817-1561 ext. 4230

Camas, Wash. – City officials invite the public, especially those with accessibility concerns, to attend the first Open Meeting on Tuesday, Sept. 30, 5:30 p.m. at the City Council Chambers, located on the ground floor level on NE 4th Avenue of the Camas Municipal Center (City Hall), 616 NE 4th Ave. in Camas. Officials advise anyone who needs assistance with seating to arrive by 5:15 p.m.

As part of the process of developing a Self-Evaluation and Transition Plan for Compliance with the Americans with Disabilities Act, the City invites comments and ideas from the disability community, as well as service providers.

The City of Camas is currently working on its Americans with Disabilities Act (ADA) Self-Evaluation and Transition Plan, and would appreciate receiving input from the public so the plan can best address accessibility concerns of the disabled in Camas. The plan will provide a method for establishing priorities for street improvements, what changes should be made, and schedule for completion.

There will be two more open meetings during the next few months that will give the community an additional opportunity to provide input on accessibility issues before the team completes its final report.

In addition to Tuesday's open meeting, the ADA Transition Planning Team welcomes comments sent to John Manix at HDJ Design Group, 314 W 15th St, Vancouver WA 98665, manix@hdjdg.com, 360-567-2117, or Jim (Curleigh) Carothers, Engineering Manager, City of Camas, WA, 616 NE 4th Avenue, Camas WA 98607, jcarothers@cityofcamas.us, 360-817-1561 x4230

Citizens are encouraged to go on record, and help the City make its streets accessible to all citizens.

The ADA Transition Planning Team consists of the participating citizens, City of Camas, HDJ Design Group, and Urbane Streets.

NOTE: The City of Camas welcomes and encourages the participation of all of its citizens in the public meeting process. A special effort will be made to ensure that persons with special needs have opportunities to participate. The City will provide translators for Non-English speaking persons who request assistance at least three (3) working days prior to a public hearing or other public meeting. Persons who need auxiliary aids or other assistance to be able to fully participate may request assistance at least three (3) working days prior to the hearing or meeting. Or, for further assistance you may contact the City of Camas by calling 360.834.6864.

No. 519514-Sept. 23

# AS-WASHOUGAL ST-REFURD

334158 09/18/2015  
FORT VANCOUVER LIBRARY  
901 C ST  
VANCOUVER WA 98660-3218

Camas-Washougal, Washington, Tuesday, September 23, 2014  
Haver Community Library  
901 C Street  
Vancouver, WA 98660

12 Sections, 82 Pages • 75 Cents

## ADA PLAN

### COMMUNITY ADVISORY COMMITTEE - MEETING 1

#### AGENDA:

- |  |               |
|--|---------------|
| 1. Welcome and Introductions                   | All           |
| 2. Committee Charter / Ground Rules            | Todd          |
| 3. Self Evaluation Status / ADA                | John          |
| 4. Priority / Criteria for Project Scheduling  | John / Todd   |
| 5. Transition Plan – Outline                   | John          |
| 6. Wrap-up / Next Meeting                      | Todd          |
| 7. 15 minute “walk” to see existing conditions | All available |

#### MEETING DETAILS:

- Date: September 30, 2014, Tuesday
- Time: Arrive by 5:15 PM Meeting starts: 5:30
- Location: Camas City Hall, City Council Chambers
- Address: 616 NE 4<sup>th</sup> Avenue, Camas WA
- ADA Access: Meeting room is on ground floor

#### CONTACTS:

##### City of Camas:

- Jim (Curleigh) Carothers, PE – Engineering Manager  
phone: (360) 817-7230 email: jcarothers@cityofcamas.us
- Ronda Syverson – Senior Administrative Support Assistant  
phone: (360) 817-7256 email: rsyverson@cityofcamas.us

##### Project Consultants:

- John Manix, PE, PTOE – Consulting Engineer, HDJ Design Group  
phone: (360) 567-2117 email: manixj@hdjdg.com
- Todd Boulanger – Transportation Planner, Urbane Streets  
phone: (360) 852-9177 email: urbanestreeets@gmail.com

# CAMAS ADA CAC #1 – MEETING ATTENDANCE SHEET

FILE COPY

<b>Project:</b> ADA Self-evaluation & Transition Plan	<b>Meeting Date:</b> Tuesday, 30 September 2014
<b>Facilitator:</b> Manix / Boulanger	<b>Place/Room:</b> Camas City Hall, Camas WA

Name	Attendance	Notes
Carol Popi	Invited and Attended ✓	
Aileen Gillespie	Invited and Attended ✓	
Tom Anderson	Invited and Attended ✓	
Kristine Graham	Invited and Attended ✓	
Michelle Cousins	Invited and Attended ✓	
Sherri Dickerson	Invited and Attended ✓	
Karl Martin	Attended (add to CAC) ✓	Caregiver to person (wife) with disabilities
Michael Frazer	Attended (add to CAC) ✓	C-TRAN
Barbara Bair	Attended (add to CAC) ✓	Person with disabilities
Christine Kamps	Attended (add to CAC) ✓	Caregiver to person with disabilities
Ronda Syverson	Attended ✓	City of Camas
Wes Heigh	Attended ✓	City of Camas
Jeff Englund	Attended ✓	City of Camas
Jim Hodges	Attended ✓	City of Camas
Curleigh Carothers	Attended ✓	City of Camas
Vera White	Invited, did not attend ✗	
Michael Moses	Invited, did not attend ✗	

Tuesday, October 21, 2014

C2 — Camas-Washougal, WA Post-Record

# PUBLIC NOTICE

## Citizen's Advisory Committee Meeting for Improved Accessibility for the Camas Disabled

Camas, Wash. – City officials invite the public, especially those with accessibility concerns, to attend the second Citizen's Advisory Committee Meeting on Tuesday, November 4, 2014, from 5:30 p.m. until 7:00 p.m. at the City Council Chambers, located on the ground floor level on NE 4th Avenue of the Camas Municipal Center (City Hall), 616 NE 4th Ave. in Camas. Officials advise anyone who needs assistance with seating to arrive by 5:20 p.m.

As part of the process of developing a Self-Evaluation and Transition Plan for Compliance with the Americans with Disabilities Act, the City invites comments and ideas from the disability community, as well as service providers.

The City of Camas is currently working on its Americans with Disabilities Act (ADA) Self-Evaluation and Transition Plan, and would appreciate receiving input from the public so the plan can best address accessibility concerns of the disabled in Camas. The plan will provide a method for establishing priorities for street improvements, what changes should be made, and schedule for completion.

There will be one more open meeting during the next few months that will give the community an additional opportunity to provide input on accessibility issues before the team completes its final report.

In addition to Tuesday's open meeting, the ADA Transition Planning Team welcomes comments sent to John Manix at HDJ Design Group, 314 W 15th St, Vancouver WA 98665, manixj@hdjdg.com, 360-567-2117, or Jim (Curleigh) Carothers, Engineering Manager, City of Camas, WA, 616 NE 4th Avenue, Camas

WA 98607, jcarothers@cityofcamas.us; 360-817-1561 x4230

Citizens are encouraged to go on record, and help the City make its streets accessible to all citizens.

The ADA Transition Planning Team consists of the participating citizens, City of Camas, HDJ Design Group, and Urbane Streets.

NOTE: The City of Camas welcomes and encourages the participation of all of its citizens in the public meeting process. A special effort will be made to ensure that persons with special needs have opportunities to participate. The City will provide translators for Non-English speaking persons who request assistance at least three (3) working days prior to a public hearing or other public meeting. Persons who need auxiliary aids or other assistance to be able to fully participate may request assistance at least three (3) working days prior to the hearing or meeting. Or, for further assistance you may contact the City of Camas by calling 360.834.6864.

No. 000000-Oct. 21



**CITY COUNCIL  
WORKSHOP AGENDA  
Monday, November 3,  
2014 at 04:30 p.m.  
Camas City Hall, 616 NE 4th  
Avenue**

**I. [CALL TO ORDER](#)**

**II. [ROLL CALL](#)**

**III. [PUBLIC COMMENTS](#)**

**IV. [SPECIAL PRESENTATIONS](#)**

**A. [Recognition of Community Development Employee](#)**

Details: Formal recognition of Community Development employee Bob Cunningham, who recently celebrated 25 years with the City.  
Department/Presenter: Phil Bourquin, Community Development Director

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**VI. [COMMUNITY DEVELOPMENT DEPARTMENT](#)**

**A. [Americans with Disabilities Act \(ADA\) Transition Plan Presentation](#)**

Details: This presentation provides information regarding the process for establishing an ADA self-evaluation and transition plan for ADA retrofit access in the public rights-of-way. Staff is working with John Manix, the presenter, from HDJ Design Group and sub consultant Todd Boulanger from Urbane Streets. The transition plan is a requirement for public agencies with 50 or more employees per Section 504 of the Rehabilitation Act of 1973 and the Americans with Disabilities Act of 1990.  
Department/Presenter: James Carothers, Engineering Manager/City Engineer  
Recommended Action: This presentation is informational. No action from Council is required.

[ADAPresentation](#)

-----edited for space-----

## ADA PLAN

## COMMUNITY ADVISORY COMMITTEE

### AGENDA:

- |                                |             |
|--------------------------------|-------------|
| 1. Welcome and Introductions   | All         |
| 2. Self Evaluation Status      | John        |
| 3. Prioritized locations       | John / Todd |
| 4. Ramp replacement Priorities | Todd        |
| 5. Policy and Procedure Review | John        |
| 6. Wrap-up / Next Meeting      | Todd        |

### MEETING DETAILS:

- Date: November 4, 2014, Tuesday
- Time: Arrive by 5:15 PM Meeting starts: 5:30
- Location: Camas City Hall, City Council Chambers
- Address: 616 NE 4<sup>th</sup> Avenue, Camas WA
- ADA Access: Meeting room is on ground floor

### CONTACTS:

#### City of Camas:

- Jim (Curleigh) Carothers, PE – Engineering Manager  
phone: (360) 817-7230 email: jcarothers@cityofcamas.us
- Ronda Syverson – Senior Administrative Support Assistant  
phone: (360) 817-7256 email: rsyverson@cityofcamas.us

#### Project Consultants:

- John Manix, PE, PTOE – Consulting Engineer, HDJ Design Group  
phone: (360) 567-2117 email: manixj@hdjdg.com
- Todd Boulanger – Transportation Planner, Urbane Streets  
phone: (360) 852-9177 email: urbanestreeets@gmail.com

## CAMAS ADA CAC #2 – MEETING ATTENDANCE SHEET

<b>Project:</b> ADA Self-evaluation & Transition Plan	<b>Meeting Date:</b> Tuesday, 04 November 2014
<b>Facilitator:</b> Manix / Boulanger	<b>Place/Room:</b> Camas City Hall, Camas WA

Name	Attendance		Notes
Carol Popi	Attended	✓	
Aileen Gillespie	Attended	✓	
Tom Anderson	Attended	✓	
Barbara Bair	Attended	✓	
Christine Kamps	Attended	✓	
Ronda Syverson	Attended	✓	City of Camas
Wes Heigh	Attended	✓	City of Camas
Jeff Englund	Attended	✓	City of Camas
Jim Hodges	Attended	✓	City of Camas
Michelle Cousins	Did not attend	✗	
Kristine Graham	Did not attend	✗	Called in sick
Karl Martin	Did not attend	✗	
Michael Frazer	Did not attend	✗	
Michael Moses	Did not attend	✗	
Sherri Dickerson	Did not attend	✗	Called in sick

**Comments Needed to Improve Accessibility for the Camas Disabled**

Camas, Wash. - City officials invite the public, especially those with accessibility concerns, to attend the third Open Meeting on Tuesday, December 09, 5:30 p.m. at the City Council Chambers, located on the ground floor level on NE 4th Avenue of the Camas Municipal Center (City Hall), 616 NE 4th Ave. in Camas. Officials advise anyone who needs assistance with seating to arrive by 5:15 p.m.

As part of the process of developing a Self-Evaluation and Transition Plan for Compliance with the Americans with Disabilities Act, the City invites comments and ideas from the disability community, as well as service providers.

The City of Camas is currently working on its Americans with Disabilities Act (ADA) Self-Evaluation and Transition Plan, and would appreciate receiving input from the public so the plan can best address accessibility concerns of the disabled in Camas. The plan will provide a method for establishing priorities for street improvements, what changes should be made, and schedule for completion.

This is the third and last scheduled committee meeting. The public will still have opportunity to comment on the draft ADA Transition Plan when it is published and circulated for comment. The draft ADA Transition Plan should be available by the end of December.

In addition to Tuesday's open meeting, the ADA Transition Planning Team welcomes comments sent to John Manix at HDJ Design Group, 314 W 15th St, Vancouver WA 98665, manixj@hdjdg.com, 360-567-2117, or Jim (Curleigh) Carothers, Engineering Manager, City of Camas, WA, 616 NE 4th Avenue, Camas WA 98607, jcarothers@cityofcamas.us, 360-817-1561 x4230

Citizens are encouraged to go on record, and help the City make its streets accessible to all citizens.

The ADA Transition Planning Team consists of the participating citizens, City of Camas, HDJ Design Group, and Urbane Streets.

NOTE: The City of Camas welcomes and encourages the participation of all of its citizens in the public meeting process. A special effort will be made to ensure that persons with special needs have opportunities to participate. The City will provide translators for Non-English speaking persons who request assistance at least three (3) working days prior to a public hearing or other public meeting. Persons who need auxiliary aids or other assistance to be able to fully participate may request assistance at least three (3) working days prior to the hearing or meeting.

Or, for further assistance you may contact the City of Camas by calling 360.834.6864.

No. 524439-Dec. 2

3 311 N.A. 7042 12631 Grants

Camas-Washougal, WA Post-Record

**PUBLIC NOTICE**

Tuesday, December 2, 2014

## ADA PLAN

### COMMUNITY ADVISORY COMMITTEE: Meeting 3

#### AGENDA:

- |   |      |
|---|------|
| 1. Welcome & Your “Vision” for ADA in Camas | All  |
| 2. Self Evaluation Status (flow chart)      | Todd |
| 3. Feedback on Ramp Replacement Criteria    | John |
| 4. Discussion of Conceptual Cost Estimates  | John |
| 5. Process of Project Scheduling            | John |
| 6. Transition Plan: Outline (flow chart)    | Todd |
| 7. Next Steps in Public Process             | Todd |
| 8. Wrap-up / Thanks                         | Todd |

#### MEETING DETAILS:

- Date: December 09, 2014, Tuesday
- Time: Arrive by 5:15 PM Meeting starts: 5:30
- Location: Camas City Hall, City Council Chambers
- Address: 616 NE 4<sup>th</sup> Avenue, Camas WA
- ADA Access: Meeting room is on ground floor

#### CONTACTS:

##### City of Camas:

- Jim (Curleigh) Carothers, PE – Engineering Manager  
phone: (360) 817-7230 email: jcarothers@cityofcamas.us
- Ronda Syverson – Senior Administrative Support Assistant  
phone: (360) 817-7256 email: rsyverson@cityofcamas.us

##### Project Consultants:

- John Manix, PE, PTOE – Consulting Engineer, HDJ Design Group  
phone: (360) 567-2117 email: manixj@hdjdg.com
- Todd Boulanger – Transportation Planner, Urbane Streets  
phone: (360) 852-9177 email: urbanestreeets@gmail.com

# CAMAS ADA CAC #3 – MEETING ATTENDANCE SHEET

<b>Project:</b> ADA Self-evaluation & Transition Plan	<b>Meeting Date:</b> Tuesday, 09 November 2014
<b>Facilitator:</b> Manix / Boulanger	<b>Place/Room:</b> Camas City Hall, Camas WA

Name	Attendance	Notes
✓ Carol Popi	CIRCLE: YES / NO	
Aileen Gillespie	CIRCLE: YES / NO	Called in Sick
✓ Tom Anderson	CIRCLE: YES / NO	
✓ Kristine Graham	CIRCLE: YES / NO	
Michelle Cousins	CIRCLE: YES / NO	
✓ Sherri Dickerson	CIRCLE: YES / NO	
Karl Martin	CIRCLE: YES / NO	
Michael Frazer	CIRCLE: YES / NO	
Barbara Bair	CIRCLE: YES / NO	Called in sick.
Christine Kamps	CIRCLE: YES / NO	
✓ Ronda Syverson	CIRCLE: YES / NO	City of Camas
Wes Heigh	CIRCLE: YES / NO	City of Camas
✓ Jeff Englund <del>Englund</del>	CIRCLE: YES / NO	City of Camas Engineering Tech
✓ Jim Hodges	CIRCLE: YES / NO	City of Camas
✓ Curleigh Carothers	CIRCLE: YES / NO	City of Camas
Vera White	CIRCLE: YES / NO	
Michael Moses	CIRCLE: YES / NO	

## **Appendix D – Public Right of Way ADA Prioritization**

- High and Medium Priority Locations Memo
- High and Medium Priority Locations Maps

## Memo

**To:** Camas ADA Plan Community Advisory Committee  
**CC:** James Carothers, City of Camas  
**From:** John Manix, PE, HDJ; and Todd Boulanger, Urbane Streets  
**Dated:** 29 October 2014  
**Regarding:** CAMAS ADA PLAN: High & Medium Priority Locations by Type

### High and Medium Priority Locations for ADA Upgrades

Please review the attached list of locations and provide the project team with input on locations of high to moderate need that have not been included.

A major component of the ADA Plan is the Self-evaluation of the existing roadways. To facilitate this process, ranking criteria was proposed and discussed at our previous meeting. The next step is to focus the evaluation on locations that are the most important. The team has used the ranking criteria to propose locations that should be high to moderate need for ADA upgrades.

#### SELF-EVALUATION PLAN RANKING CRITERIA

The locations below are ranked based on criteria that are considered best practice by ADA planning experts but your input is critical to make sure the disabled community is best served in Camas. The action of ranking projects is a function of the following criteria:

- **Collision History** for pedestrians and bicyclists
- **Roadway Classification** arterials have higher traffic
- **Land-use Zoning** commercial has higher pedestrian traffic
- **Important Destinations** such as support or medical services and public offices for the disabled community.
- **Transit Service** is highly relied upon by the disabled community and
- **CAC Sites:** these locations have been identified through the CAC public process and added to the priority list based on local knowledge.

Pedestrian safety is an important consideration in ranking potential sites and can be done based on exposure to high volume and high-speed motor vehicle traffic. Typically the arterial roadway classification is a good starting point for judging the likely exposure above, as higher order arterials have more than collector or local streets. Thus high priority locations have most, if not all, criteria present while medium have less. And any public streets not listed below are considered low priority, as they generally have low traffic volumes in residential areas and often are without reported crash history. These street facilities also generally do not have transit service or public facilities serving persons with disabilities thus reducing their importance for this community wide planning process.

The priority locations are also organized by type (intersection, corridor, and area) for purposes of easier discussion in identification and treatment types. All facilities types are collectively equal within each priority tier. The type of location becomes important later when mitigations or treatments are proposed.

**HIGH PRIORITY PROJECT LOCATIONS – INTERSECTION:**

- NW 38<sup>th</sup> Ave. & NW Parker St. Intersection (Sheet 3)
- NE 43<sup>rd</sup> Ave. & SR-500/ Everett St. Intersection (Sheet 3)
- NE 3<sup>rd</sup> Ave. & NE Dallas St. Intersection (Sheet 4)
- NE 3<sup>rd</sup> Ave. & NE 3<sup>rd</sup> Loop / SE Crown Rd. Intersection (Sheet 4)
- NE 3<sup>rd</sup> Ave. & NW Shepherd Intersection (Sheet 4)

**HIGH PRIORITY PROJECT LOCATIONS – CORRIDOR:**

- NE Adams Corridor: NE 6<sup>th</sup> Ave. to NE 3<sup>rd</sup> Ave. (Sheet 4)
- NE 3<sup>rd</sup> Ave. Corridor: NE Adams St. to NE Joy St. (Sheet 4)
- NE 3<sup>rd</sup> Ave. Corridor: N Shepherd Rd. to SE Lechner St. (Sheet 4)

**HIGH PRIORITY PROJECT LOCATIONS – AREA:**

- Crown Park Neighborhood (Sheet 3)

**MEDIUM PRIORITY PROJECT LOCATIONS – INTERSECTION:**

- NE Goodwin Rd. & NW Friberg-Strunk St. Intersection (Sheet 2)
- SE 1<sup>st</sup> St & NW Friberg-Strunk St. Intersection (Sheet 2)
- NW Lake Rd. & NW Parker St. Intersection (Sheet 2)
- NW 16<sup>th</sup> Ave. & NW Brady Rd. Intersection (Sheet 3)
- NW Lake Rd. & SR-500/ Everett St. Intersection (Sheet 3)

**MEDIUM PRIORITY PROJECT LOCATIONS – CORRIDOR:**

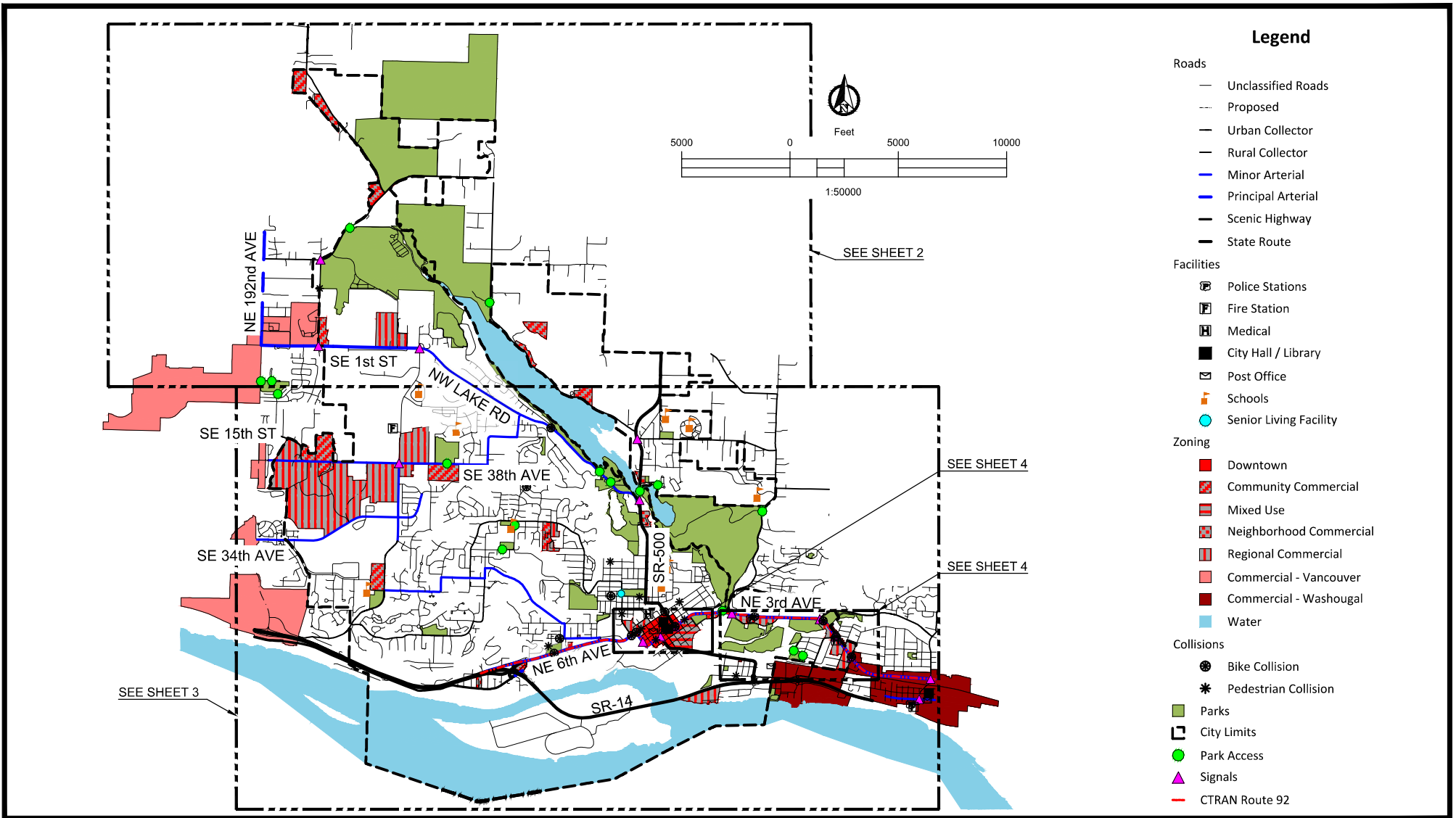
- NW 38<sup>th</sup> Ave. Corridor: SE Bybee Rd. to NW Dahlia Dr. (Sheet 3)
- NE 28<sup>th</sup> Ave. Corridor: NW Valley St. to NW Kent St. (Sheet 3)
- NW 6<sup>th</sup> Ave. Corridor: NW Norwood St. to Division St. (Sheet 3)
- NE 3<sup>rd</sup> Ave. Corridor: NE Joy St. to E 1<sup>st</sup> Ave. (Sheet 4)
- NE 3<sup>rd</sup> Ave. Corridor: E 1<sup>st</sup> Ave. to NE Shepherd Rd. (Sheet 4)

**MEDIUM PRIORITY PROJECT LOCATIONS – AREA:**

- West Highland Neighborhood (Sheet 3)
- Downtown Core (Sheet 4)

**LOW PRIORITY PROJECT LOCATIONS – INTERSECTION / CORRIDOR / AREA:**

- All other public streets not listed due to either low demand and or low need.



**Legend**

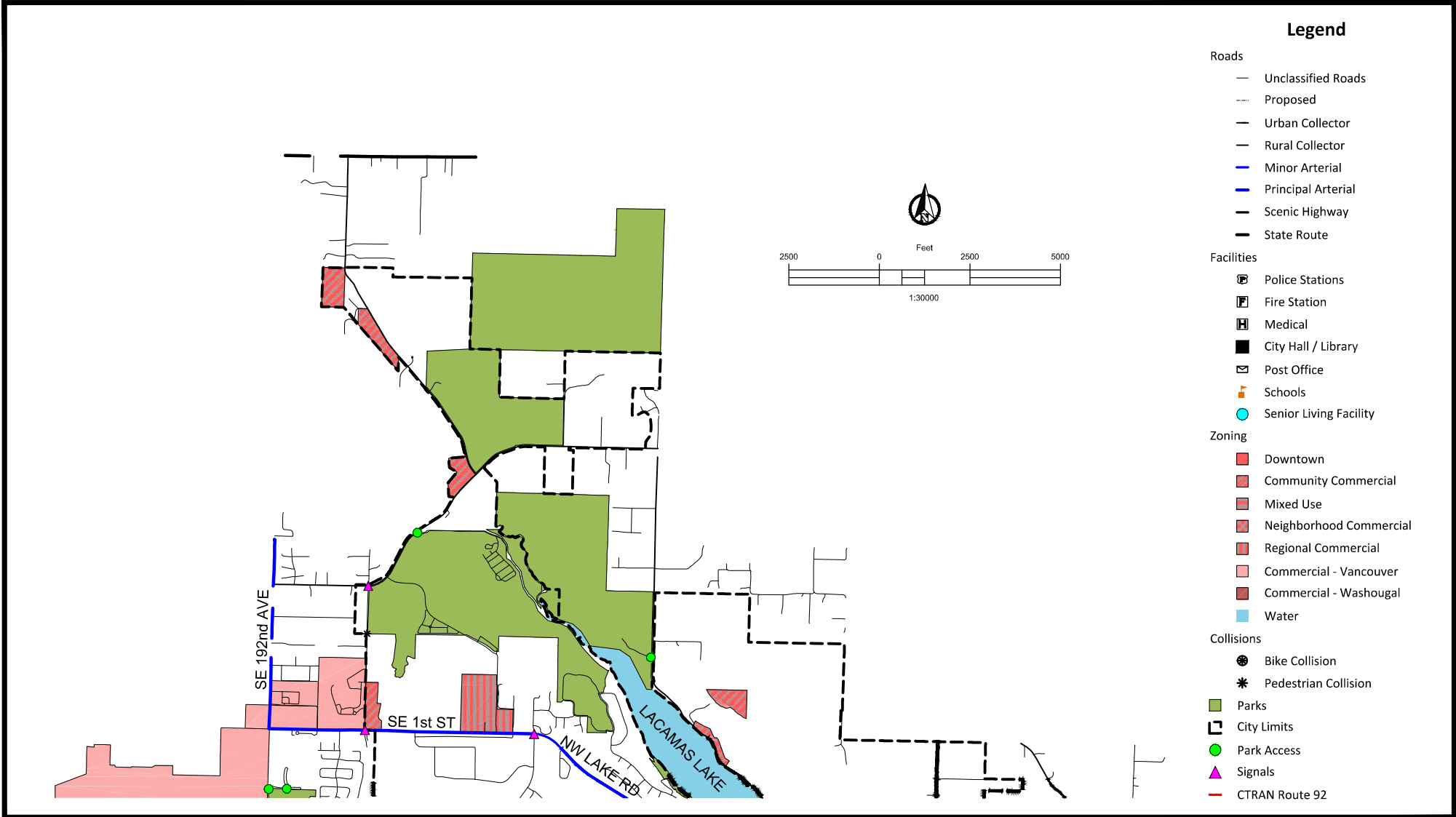
- Roads**
  - Unclassified Roads
  - - - Proposed
  - Urban Collector
  - Rural Collector
  - Minor Arterial
  - Principal Arterial
  - Scenic Highway
  - State Route
- Facilities**
  - 🚓 Police Stations
  - 🚒 Fire Station
  - 🏥 Medical
  - 🏛️ City Hall / Library
  - 📬 Post Office
  - 🎓 Schools
  - 👴 Senior Living Facility
- Zoning**
  - 🔴 Downtown
  - 🔲 Community Commercial
  - 🔲 Mixed Use
  - 🔲 Neighborhood Commercial
  - 🔲 Regional Commercial
  - 🔲 Commercial - Vancouver
  - 🔲 Commercial - Washougal
  - 🔵 Water
- Collisions**
  - ⊗ Bike Collision
  - \* Pedestrian Collision
  - 🌳 Parks
  - 🏠 City Limits
  - Park Access
  - ▲ Signals
  - CTRAN Route 92

**OVERALL MAP FOR:  
CAMAS CITY-WIDE SELF EVALUTATION & ADA TRANSITION PLAN  
PRIORITIZATION CRITERIA**

**HDJ**  
DESIGN GROUP

314 W 15th Street  
Vancouver, WA 98660-2927  
360/695-3488  
503/824-4005  
360/695-8767 fax  
Internet: www.hdjg.com

DESIGNED: N/A	SCALE: H: N/A V: N/A	SHEET 1 4
DRAWN: MPA	DATE: SEP 2014	
CHECKED: JAM	JOB NO.: 3804	



**Legend**

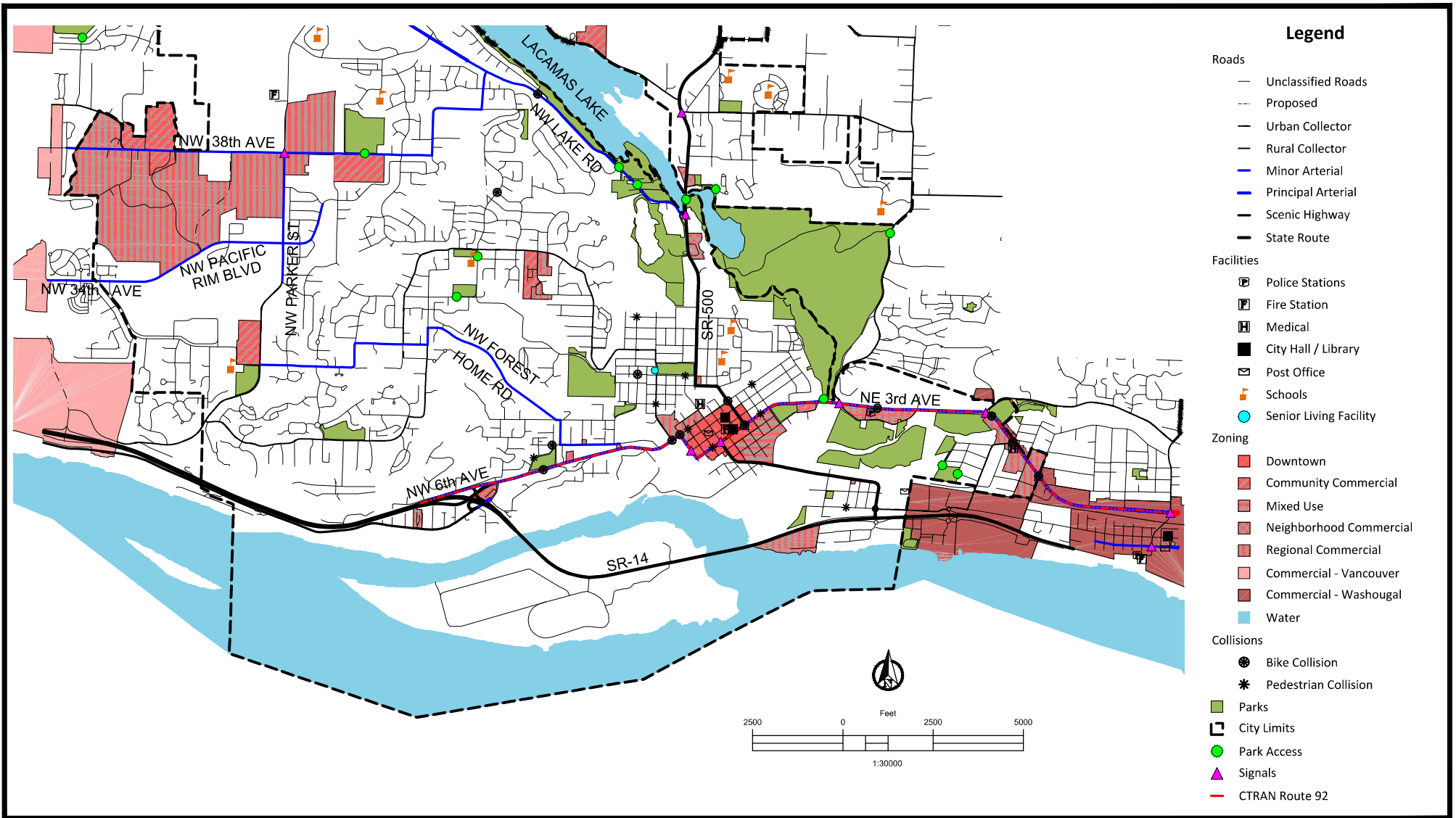
- Roads**
  - Unclassified Roads
  - Proposed
  - Urban Collector
  - Rural Collector
  - Minor Arterial
  - Principal Arterial
  - Scenic Highway
  - State Route
- Facilities**
  - ☎ Police Stations
  - 🚒 Fire Station
  - 🏥 Medical
  - 🏛 City Hall / Library
  - ✉ Post Office
  - 🎓 Schools
  - 👴 Senior Living Facility
- Zoning**
  - 🏠 Downtown
  - 🏠 Community Commercial
  - 🏠 Mixed Use
  - 🏠 Neighborhood Commercial
  - 🏠 Regional Commercial
  - 🏠 Commercial - Vancouver
  - 🏠 Commercial - Washougal
  - 💧 Water
- Collisions**
  - 🚲 Bike Collision
  - 🚶 Pedestrian Collision
- Parks**
  - 🌳 Parks
  - 📐 City Limits
  - 🟢 Park Access
  - 🚦 Signals
  - 🛣 CTRAN Route 92

**NORTHEAST MAP FOR:  
CAMAS CITY-WIDE SELF EVALUTATION & ADA TRANSITION PLAN  
PRIORITIZATION CRITERIA**

**HDJ**  
DESIGN GROUP

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Vancouver, WA 98660-2927  
360/695-3488  
503/824-4005  
360/695-8767 fax  
Internet: www.hdjg.com

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DRAWN: MPA	DATE: SEP 2014	
CHECKED: JAM	JOB NO.: 3804	



**SOUTHEAST MAP FOR:  
CAMAS CITY-WIDE SELF EVALUTATION & ADA TRANSITION PLAN  
PRIORITIZATION CRITERIA**

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360/695-3488  
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360/695-8767 fax  
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DESIGNED: N/A	SCALE: H: N/A V: N/A	SHEET
DRAWN: MPA	DATE: SEP 2014	3
CHECKED: JAM	JOB NO.: 3804	4

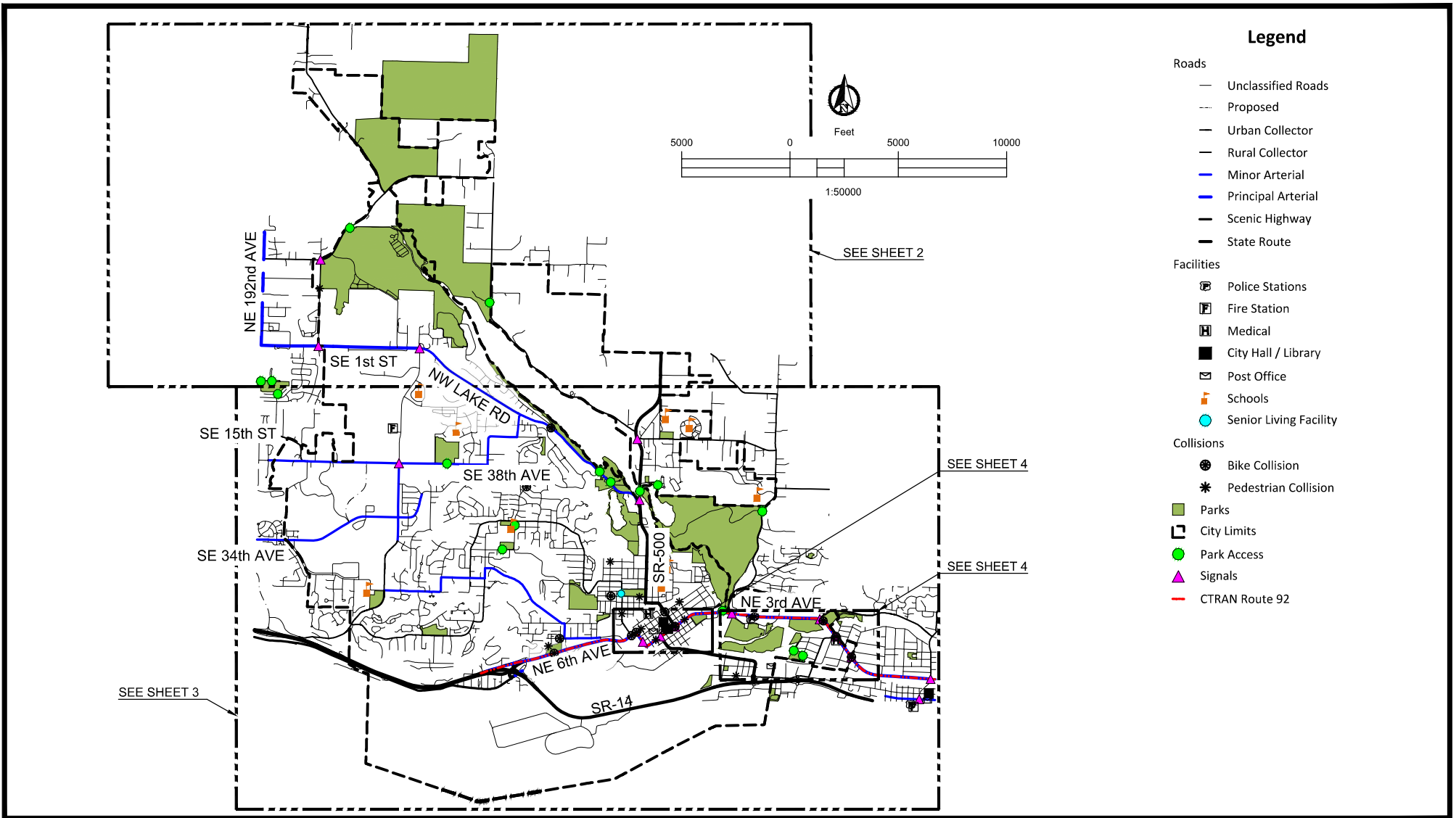


**DOWNTOWN MAP & NE 3rd AVE CORRIDOR FOR:  
CAMAS CITY-WIDE SELF EVALUTATION & ADA TRANSITION PLAN  
PRIORITIZATION CRITERIA**

**HDJ DESIGN GROUP**

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Internet: www.hdjg.com

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DRAWN: MPA	DATE: SEP 2014	4
CHECKED: JAM	JOB NO.: 3804	4

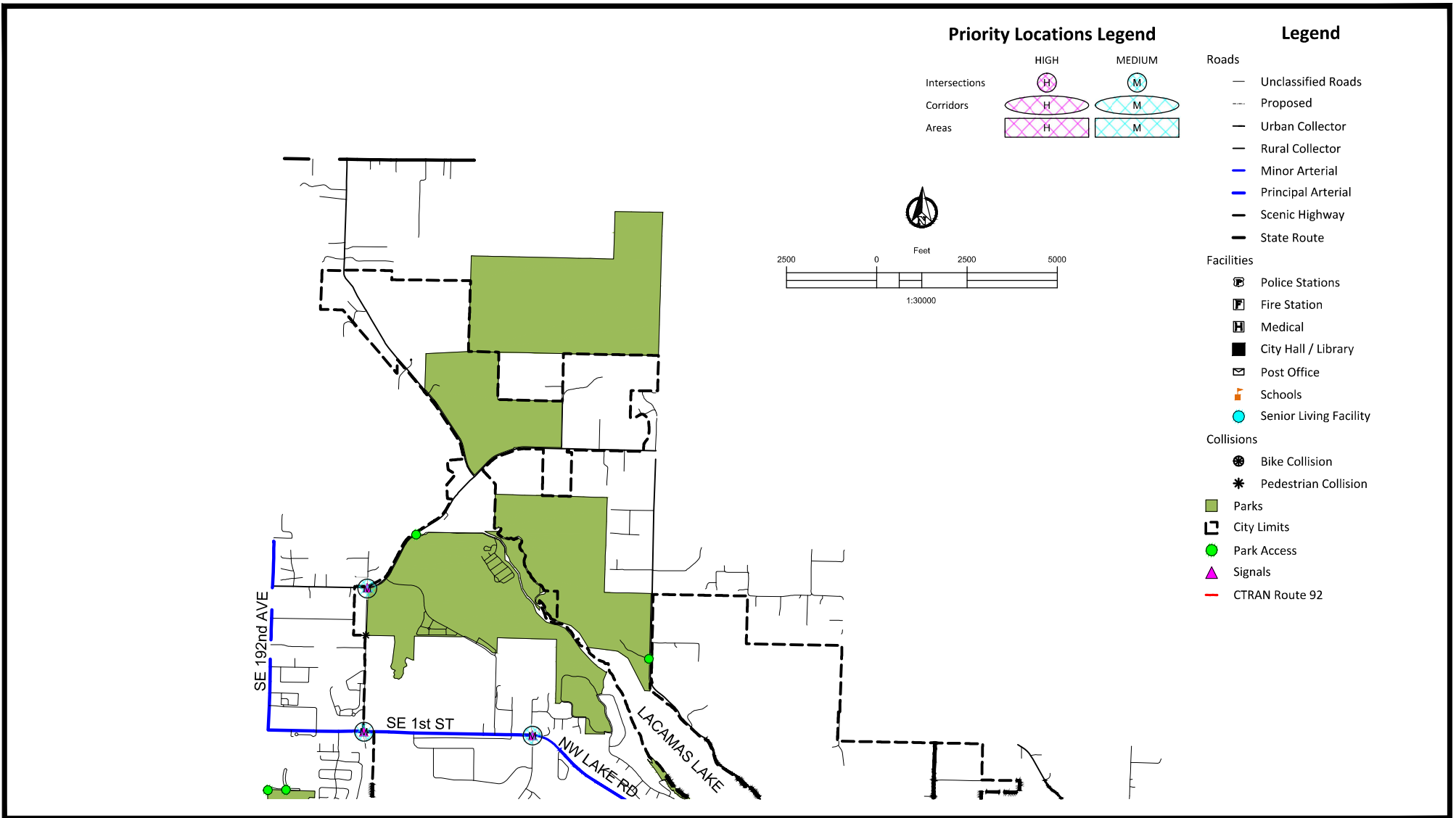


**OVERALL MAP FOR:  
CAMAS CITY-WIDE SELF EVALUTATION & ADA TRANSITION PLAN  
HIGH & MEDIUM PRIORITY LOCATIONS**

**HDJ**  
DESIGN GROUP

314 W 15th Street  
Vancouver, WA 98660-2927  
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503/824-4005  
360/695-8767 fax  
Internet: www.hdjg.com

DESIGNED: N/A	SCALE: H: N/A V: N/A	SHEET
DRAWN: MPA	DATE: SEP 2014	1
CHECKED: JAM	JOB NO.: 3804	4

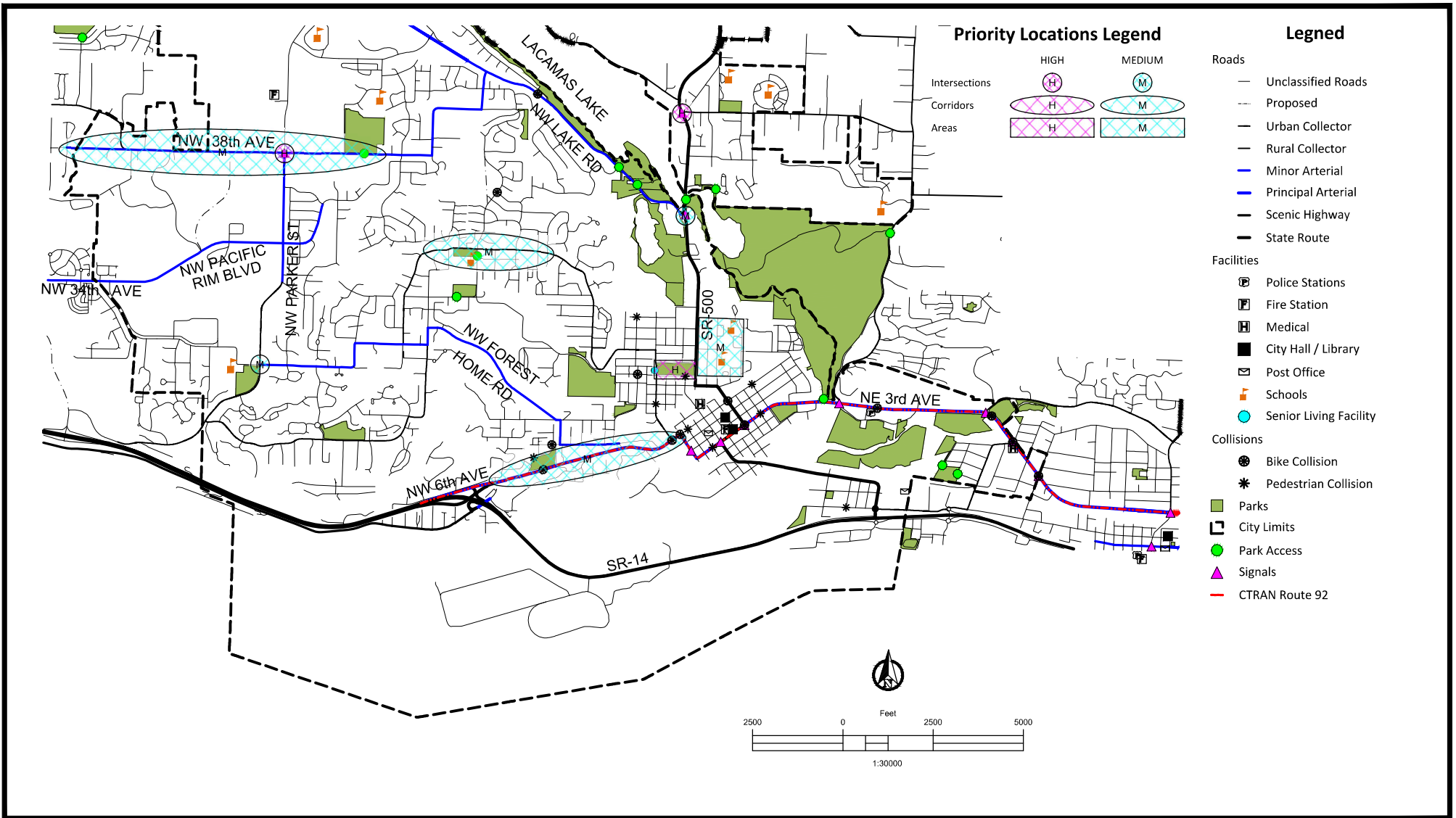


**NORTHEAST MAP FOR:  
CAMAS CITY-WIDE SELF EVALUTATION & ADA TRANSITION PLAN  
HIGH & MEDIUM PRIORITY LOCATIONS**

**HDJ**  
DESIGN GROUP

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Vancouver, WA 98660-2927  
360/695-3485  
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360/695-8767 fax  
Internet: www.hdjg.com

DESIGNED: N/A	SCALE: H: N/A V: N/A	SHEET <b>2</b>
DRAWN: MPA	DATE: SEP 2014	
CHECKED: JAM	JOB NO.: 3804	<b>4</b>

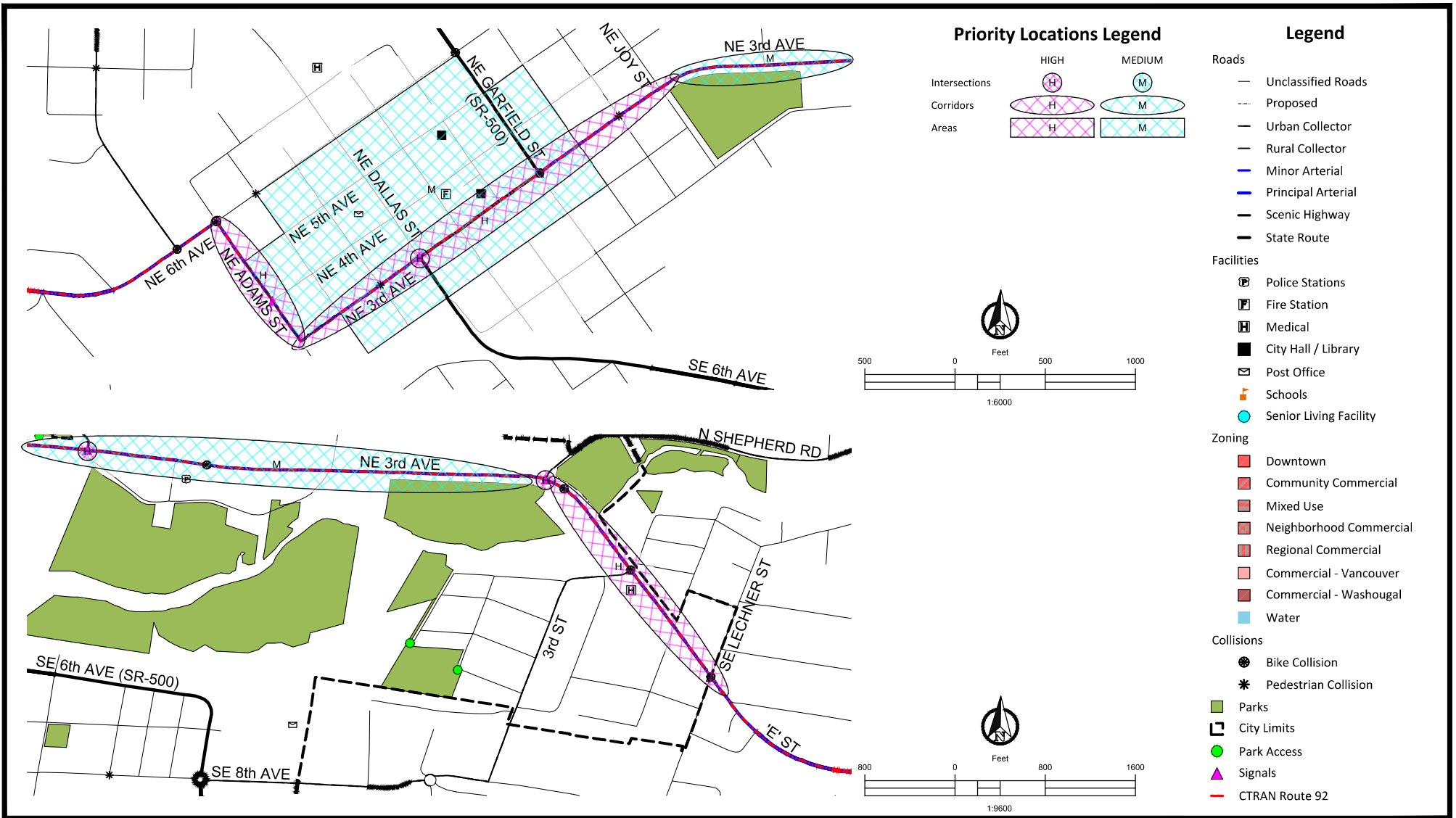


SOUTHEAST MAP FOR:  
**CAMAS CITY-WIDE SELF EVALUTATION & ADA TRANSITION PLAN**  
 HIGH & MEDIUM PRIORITY LOCATIONS

**HDJ**  
 DESIGN GROUP

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 Vancouver, WA 98660-2927  
 360/695-3485  
 503/824-4005  
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DRAWN: MPA	DATE: SEP 2014	3
CHECKED: JAM	JOB NO.: 3804	4



**DOWNTOWN MAP & NE 3rd AVE CORRIDOR FOR:  
CAMAS CITY-WIDE SELF EVALUTATION & ADA TRANSITION PLAN  
HIGH & MEDIUM PRIORITY LOCATIONS**



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Vancouver, WA 98660-2927  
360/695-3485  
503/824-4005  
360/695-8767 fax  
Internet: www.hdjg.com

DESIGNED: N/A	SCALE: H: N/A V: N/A	SHEET
DRAWN: MPA	DATE: SEP 2014	4
CHECKED: JAM	JOB NO.: 3804	4

## Appendix E – Inventory

- Definitions for Inventory Form
- Inventory of Intersections
- Inventory of Corridors
- Inventory of Areas

NW 6th Ave & Division Street						Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	No curb ramp	Substantially compliant ST20 ramp	Substantially compliant ST20 ramp	No curb ramp		
Replacement Priority	High	Medium	Medium	High		
Recommendation	ST21	Add Detectable Warning Pattern per ST29	Add Detectable Warning Pattern per ST29	ST21		
Cost Estimate	\$6,000	\$1,000	\$1,000			\$8,000
NW 6th Ave&NW Ash						Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	No sidewalk	Substantially compliant ST21 ramp	Substantially compliant ST23 ramp	No sidewalk		
Replacement Priority		Medium	Medium			
Recommendation		Correct excessive Lip and add Detectable Warning Pattern per ST29	Correct excessive Lip and Add Detectable Warning Pattern per ST29			
Cost Estimate		\$1,000	\$1,000			\$2,000

NW 6th Ave/NW Ivy/NW 5th, Forest Home Park						Subtotal
Quadrant	SW	NW	NE	SE	Pedestrian crossing	
Status of existing curb ramp*	No curb ramp - pedestrian crossing just west	Substantially compliant ST20 ramp	No curb ramp	No curb ramp no sidewalk	Substantially complaint ST117 ramps	
Replacement Priority	NA	Medium	High	NA	Medium	
Recommendation	No change	Add Detectable Warning Pattern per ST29	ST20, replace driveway	No change	Correct excessive lips and add detectable warning pattern per ST29	
Cost Estimate			\$10,000		\$2,000	\$12,000
<b>Signal - Status of existing PPB</b>	None	None	None	None	No APS	
Replacement Priority					Medium	
Recommendation					No change	
Cost Estimate						\$0
NW 6th Ave & NW Logan, Forest Home Park						Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	No curb ramp	Substantially compliant ST20 ramp	Substantially compliant ST20 ramp	No curb ramp		
Replacement Priority	High	Medium	Medium	High		
Recommendation	ST24	Add Detectable Warning Pattern per ST29	Add Detectable Warning Pattern per ST29	ST24		
Cost Estimate	\$6,000	\$500	\$500	\$6,000		\$13,000

NE 43rd Ave & SR-500/Everett St. Intersection						Subtotal
Quadrant	SW	NW	NE	SE	East	
Status of existing curb ramp*	None	None	None	None	Substantially compliant island	
Replacement Priority	NA	NA	NA	NA	Medium	
Recommendation	No change - no sidewalk	No change - no sidewalk	No change - no sidewalk	No change - no sidewalk	Add Detectable Warning Pattern per ST29	
Cost Estimate					\$500	\$500
<b>Signal - Status of existing PPB</b>	None	Excessive reach	Excessive reach	None	Excessive reach	
Replacement Priority		High	High		High	
Recommendation		Up grade to APS	Up grade to APS		Up grade to APS	
Cost Estimate		\$2,000	\$2,000		\$2,000	\$6,000
NE 3rd Ave & NE 3rd Loop/SE Crown Rd.						Subtotal
Quadrant	SW	NW	NE	SE	North	
Status of existing curb ramp*	Complaint ST20 ramp	Complaint ST20 ramp	Compliant ST21A ramp	Substantially compliant ST20A ramp	Compliant traffic island	
Replacement Priority	NA	NA	NA	NA	NA	
Recommendation	No change	No change	No change	No change	No change	
Cost Estimate						\$0
<b>Status of existing PPB</b>	No APS	No APS	No APS	No APS	No APS	
Replacement Priority	Medium	Medium	Medium	Medium	Medium	
Recommendation	No change	No change	No change	No change	No change	
Cost Estimate						\$0

**Total construction cost to upgrade intersections**

**\$41,500**

NE Adams Street - NE 3rd Ave to NE 6th Ave						
Intersection	Adams/NE 3rd Ave					Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	No curb ramp	No curb ramp	No curb ramp	No curb ramp		
Replacement Priority	High	High	High	High		
Recommendation	Special design needed to address multiple conflicts. Consider curb extensions					
Cost Estimate	\$8,000	\$8,000	\$8,000	\$8,000		\$32,000
Mid Block	NE Adams Street from NE 3rd Ave to NE 4th Ave					Subtotal
Distance from previous intersection	130 feet north					
Side of street	east					
Obstruction	Vertical lip					
Recommendation	Bevel lip					
Cost Estimate	\$1,000					\$1,000
Intersection	Adams/NE 4th Ave					Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	Steep grade substandard ramp	Steep grade substandard ramp	Substantially compliant ST20 ramp	Substantially compliant ST20 ramp		
Replacement Priority	High	High	Medium	Medium		
Recommendation	ST24	ST24	Add Detectable Warning Pattern per ST29	Correct Excessive Lip and add Detectable Warning Pattern ST 29		
Cost Estimate	\$6,000	\$6,000	\$1,000	\$1,000		\$14,000
<b>Signal - Status of existing PPB</b>	Substantially compliant	Substantially compliant	Substantially compliant	West -add PPB to move closer to crosswalk		
Replacement Priority	NA	NA	NA	low		
Recommendation	APS	APS	APS	APS		
Cost Estimate	\$2,000	\$2,000	\$2,000	\$3,000		\$9,000

NE Adams Street - NE 3rd Ave to NE 6th Ave						
Mid Block	NE Adams Street from NE 4th Ave to NE 5th Ave					Subtotal
Distance from previous intersection	20 feet north					
Side of street	East					
Obstruction	Vertical lip					
Recommendation	Bevel lip					
Cost Estimate	\$1,000					\$1,000
Intersection	Adams/NE 5th Ave					Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	Substantially compliant ST20A 1/2 ramp	No ramp due to driveway	Substantially compliant ST20 ramp	Substantially compliant ST20 ramp		
Replacement Priority	Medium	NA	Medium	Medium		
Recommendation	Correct Excessive Lip and add Detectable Warning Pattern per ST29	No change	Add Detectable Warning Pattern per ST29	Add Detectable Warning Pattern per ST29		
Cost Estimate	\$1,000		\$500	\$500		\$2,000
Mid Block	NE Adams Street from NE 5th Ave to NE 6th Ave					Subtotal
Distance from previous intersection	110 feet north					
Side of street	East					
Obstruction	Vertical lip					
Recommendation	Bevel					
Cost Estimate	\$1,000					\$1,000

NE Adams Street - NE 3rd Ave to NE 6th Ave						
Intersection	Adams/NE 6th Ave					Subtotal
Quadrant	SW	NW	NE	SE	SW	
Status of existing curb ramp*	No curb ramp	Substantially compliant ST20 ramp	Substandard ramp	Substantially compliant ST23 ramp	Inaccessible traffic island	
Replacement Priority	High	Medium	High	Medium	High	
Recommendation	ST21	Correct Excessive Lip and add Detectable Warning Pattern per ST29	ST23	Correct Excessive Lip and add Detectable Warning Pattern per ST29	Modify to make accessible	
Cost Estimate	\$6,000	\$1,000	\$6,000	\$1,000	\$4,000	\$18,000

**Total construction cost to upgrade corridor**

**\$78,000**

NE 3rd Avenue - Shepherd Road to NE 6th Street to Lechner Street						
NE 3rd Ave is assumed east - west						
Intersection	NE 3rd Avenue/Shepherd Rd					Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	Compliant ST23 ramp	Compliant ST23 ramp	Compliant ST21 ramp	Substantially compliant ST201ramp		
Replacement Priority	NA	NA	Medium	Medium		
Recommendation			Add Detectable Warning Pattern per ST29	Correct Excessive Lip and add Detectable Warning Pattern ST 29		
Cost Estimate	\$0	\$0	\$1,000	\$1,000		\$2,000
Signal - Location status of existing PPB	Substantially compliant	Substantially compliant	Substantially compliant	East - Substantially compliant West - add PPB to move closer to crosswalk		
Replacement Priority	high	High	High	High		
Recommendation	APS and relocation of button	APS and relocation of button	APS and relocation of button	APS and relocation of button		
Cost Estimate	\$3,000	\$3,000	\$3,000	\$3,000		\$12,000
Mid Block	NE 3rd Avenue from Shepherd Rd to 4th Street /NE 2nd Avenue					Subtotal
Distance from previous intersection						
Side of street						
Obstruction						
Recommendation						
Cost Estimate						\$0

NE 3rd Avenue - Shepherd Road to NE 6th Street to Lechner Street						
Intersection	NE 3rd Ave/4th Street/NE 2nd Avenue					Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	Substantially compliant ST20	Substantially compliant ST20	Substantially compliant ST20	Substantially compliant ST20		
Replacement Priority	Medium	Medium	Medium	Medium		
Recommendation	Correct excessive lip and add Detectable Warning Pattern per ST29	Add Detectable Warning Pattern per ST29	Correct excessive lip and add Detectable Warning Pattern per ST29	Correct excessive lip and add Detectable Warning Pattern per ST29		
Cost Estimate	\$1,000	\$1,000	\$1,000	\$1,000		\$4,000
<b>Signal - Location status of existing PPB</b>	Compliant	Compliant	Compliant	Compliant		
Replacement Priority	NA	NA	NA	NA		
Recommendation	No change	No change	No change	No change		
Cost Estimate	\$0	\$0	\$0	\$0		\$0
Mid Block	NE 3rd Avenue from 4th Street to NE 2nd Avenue to 5th Street/NE Weir Street					Subtotal
Distance from previous intersection	70 to 165 feet east	195 to 260 feet east	295 to 445 feet east	75 to 105 feet east	235 to 290 feet east	
Side of street	North	North	North	South	South	
Obstruction	Noncompliant surface	Noncompliant surface	Noncompliant surface	Noncompliant driveway slope	Noncompliant driveway slope	
Recommendation	Construct sidewalk	Construct sidewalk	Construct sidewalk and ST117	ST117	ST117	
Cost Estimate	\$4,300	\$2,900	\$6,700	\$1,600	\$2,500	\$18,000

NE 3rd Avenue - Shepherd Road to NE 6th Street to Lechner Street						
Intersection	NE 3rd Ave/5th Street/NE Weir Street					Subtotal
Quadrant	SW	NW	NE	SE		
Inventory Date: December 8, 2014	Substantially compliant ST20	Substantially compliant ST20	Substantially compliant ST20	Substantially compliant ST20		
Replacement Priority	Medium	Medium	Medium	Medium		
Recommendation	Correct excessive lip and add Detectable Warning Pattern per ST29	Correct excessive lip and add Detectable Warning Pattern per ST29	Correct excessive lip and add Detectable Warning Pattern per ST29	Correct excessive lip and add Detectable Warning Pattern per ST29		
Cost Estimate	\$1,000	\$1,000	\$1,000	\$1,000		\$4,000
Mid Block	NE 3rd Avenue from 5th Street/Weir Street to 6th Street/Lechner Street					Subtotal
Distance from previous intersection	210 to 440 feet	175 to 195 feet east				
Side of street	North	South				
Obstruction	No sidewalk	Noncompliant driveway				
Recommendation	Construct sidewalk and ST117	ST117				
Cost Estimate	\$10,300	\$1,000				\$11,300

NE 3rd Avenue - Shepherd Road to NE 6th Street to Lechner Street						
Intersection	NE 3rd Ave/6th Street/Lechner Street					Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	Compliant ST20 ramp	Compliant ST20 ramp	Compliant ST20 ramp	Compliant ST20A ramp		
Replacement Priority	NA	NA	NA	NA		
Recommendation	No change	No change	No change	No change		
Cost Estimate						
Signal - Location status of existing PPB	Compliant	Compliant	Compliant	Compliant		
Replacement Priority	NA	NA	NA	NA		
Recommendation	No change	No change	No change	No change		
Cost Estimate	\$0	\$0	\$0	\$0		\$0
<b>Total construction cost to upgrade corridor</b>						<b>\$51,300</b>

NE 3rd Avenue - NE Adams Street to NE Joy Street						
NE 3rd Ave is assumed east - west						
Intersection	NE 3rd Avenue/Adams Street - Also see NE Adams Street Corridor					Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	No curb ramp	No curb ramp	No curb ramp	No curb ramp		
Replacement Priority	High	High	High	High		
Recommendation	Special design needed to address multiple conflicts. Consider curb extensions					
Cost Estimate	\$8,000	\$8,000	\$8,000	\$8,000		\$32,000
Mid Block	NE 3rd Ave from NE Adams Street to NE Birch Street					Subtotal
Distance from previous intersection		42 feet east	65 feet east			
Side of street		North	South			
Obstruction		Vertical lip	Vertical lip			
Recommendation		Bevel lip	Bevel lip			
Cost Estimate		\$1,000	\$1,000			\$2,000
Intersection	NE 3rd Avenue/ Birch Street					Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	No curb ramp	Substantially compliant ST20 ramp	Substantially compliant ST20 ramp	Substantially compliant ST20 ramp	Does this corner have a ramp	
Replacement Priority	High	Medium	Medium	Medium		
Recommendation	ST23	Correct Lip and add Detectable Warning Pattern per ST29 x 2	Correct Lip and Add Detectable Warning Pattern per ST29 x 2	Add Detectable Warning Pattern per ST29		
Cost Estimate	\$6,000	\$1,000	\$1,000	\$500		\$8,500
Mid Block	NE 3rd Ave from NE Birch Street to NE Cedar Street					Subtotal
Distance from previous intersection		90 feet east	120 feet east			
Side of street		South	South			
Obstruction		Vertical lip	Noncompliant driveway			
Recommendation		Correct junction box	ST117			
Cost Estimate		\$1,000	\$1,500			\$2,500

NE 3rd Avenue - NE Adams Street to NE Joy Street						
Intersection	NE 3rd Avenue/ Cedar Street					Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	No curb ramp	Substantially compliant ST20 ramp	Substantially compliant ST20 ramp	No curb ramp		
Replacement Priority	High	Medium	Medium	High		
Recommendation	ST20	Correct excessive lip and add Detectable Warning Pattern per ST29	Correct excessive lip and add Detectable Warning Pattern per ST29	ST23		
Cost Estimate	\$6,000	\$1,000	\$1,000	\$6,000		\$14,000
Mid Block	NE 3rd Ave from NE Cedar Street to NE Dallas Street					Subtotal
Distance from previous intersection						
Side of street						
Obstruction						
Recommendation						
Cost Estimate						\$0

NE 3rd Avenue - NE Adams Street to NE Joy Street						
Singalized Intersection	NE 3rd Avenue/Dallas Street					Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	Substantially compliant ST21 ramp	Substantially complaint ST20 ramp	Substantially compliant ST20 ramp	Spalling and utility box substandard Ramp		
Replacement Priority	Medium	Medium	Medium	High		
Recommendation	Bevel lip	Correct excessive Lip and add Detectable Warning Pattern per ST29	Correct Excessive Lip and add Detectable Warning Pattern per ST29	ST20A		
Cost Estimate	500	1000	1000	6000		\$8,500
<b>Signal - Status of existing PPB</b>	Substantially compliant	Substantially compliant	Substantially compliant	East - Substantially compliant West - add PPB to move closer to crosswalk		
Replacement Priority	NA	NA	NA	low		
Recommendation	APS	APS	APS	APS		
Cost Estimate	\$2,000	\$2,000	\$2,000	\$2,000		\$8,000
Mid Block	NE 3rd Ave from NE Dallas Street to NE Everett Street					Subtotal
Distance from previous intersection						
Side of street						
Obstruction						
Recommendation						
Cost Estimate						\$0

NE 3rd Avenue - NE Adams Street to NE Joy Street						
Intersection	NE 3rd Avenue/ Everett Street					Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	Substantially complaint ST20 ramp	Substantially complaint ST20 ramp	Substantially compliant ST20 ramps	Substantially compliant ST23 ramp		
Replacement Priority	Medium	Medium	Medium	Medium		
Recommendation	Correct excessive Lip and add Detectable Warning Pattern per ST29	Correct excessive Lip and add Detectable Warning Pattern per ST29	Add Detectable Warning Pattern per ST29	Correct excessive Lip and add Detectable Warning Pattern per ST29		
Cost Estimate	\$1,000	\$1,000	\$500	\$1,000		\$3,500
Mid Block	NE 3rd Ave from NE Everett Street to NE Franklin Street					Subtotal
Distance from previous intersection						
Side of street						
Obstruction						
Recommendation						
Cost Estimate						\$0
Intersection	NE 3rd Avenue/ Franklin Street					Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	No curb ramp	Substantially complaint ST20 ramp	Noncompliant curb ramp	No curb ramp		
Replacement Priority	High	Medium	High	High		
Recommendation	ST24	ST23	ST23	ST24		
Cost Estimate	\$6,000	\$6,000	\$6,000	\$6,000		\$24,000
Mid Block	NE 3rd Ave from NE Franklin Street to NE Garfield Street					Subtotal
Distance from previous intersection						
Side of street						
Obstruction						
Recommendation						
Cost Estimate						\$0

NE 3rd Avenue - NE Adams Street to NE Joy Street						
Singalized Intersection	NE 3rd Avenue/Garfield Street					Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	Substantially compliant ST20 ramp	Substantially compliant ST20 ramp	Substantially compliant ST20 ramp	Substantially compliant ST20 ramp		
Replacement Priority	Medium	Medium	Medium	Medium		
Recommendation	Add Detectable Warning Pattern per ST29	Correct excessive lip and add detectable warning pattern per ST29	Correct excessive lip and add detectable warning pattern per ST29	Add detectable warning pattern per ST29		
Cost Estimate	500	500	500	500		\$2,000
<b>Signal - Status of existing PPB</b>	Compliant	Compliant	Compliant	Compliant		
Replacement Priority	NA	NA	NA	NA		
Recommendation	No change	No change	No change	No change		
Cost Estimate						\$0
<b>Mid Block</b>	<b>NE 3rd Ave from NE Garfield Street to NE Hayes Street</b>					<b>Subtotal</b>
Distance from previous intersection						
Side of street						
Obstruction						
Recommendation						
Cost Estimate						\$0
Intersection	NE 3rd Avenue/Hayes Street					Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	Complaint ST27 for east-west travel only	Complaint ST27 for east-west travel only	Complaint ST27 for east-west travel only	Complaint ST27 for east-west travel only		
Replacement Priority	NA	NA	NA	NA		
Recommendation	No change	No change	No change	No change		
Cost Estimate						\$0

NE 3rd Avenue - NE Adams Street to NE Joy Street						
<b>Mid Block</b>	<b>NE 3rd Ave from NE Hayes Street to NE Ione Street</b>					<b>Subtotal</b>
Distance from previous intersection						
Side of street						
Obstruction						
Recommendation						
Cost Estimate						\$0
<b>Intersection</b>	<b>NE 3rd Avenue/Ione Street</b>					<b>Subtotal</b>
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	Complaint ST26 ramps	Complaint ST27 for east-west travel only	Complaint ST27 for east-west travel only	Complaint ST27 ramp add ST25		
Replacement Priority	NA	NA	NA	NA		
Recommendation	No change	No change	No change	No change		
Cost Estimate						\$0
<b>Mid Block</b>	<b>NE 3rd Ave from Ione Street to NE Joy Street</b>					<b>Subtotal</b>
Distance from previous intersection						
Side of street						
Obstruction						
Recommendation						
Recommendation						
Cost Estimate						\$0
<b>Intersection</b>	<b>NE 3rd Avenue/Joy Street</b>					<b>Subtotal</b>
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	Complaint ST27 for east-west travel only	Complaint ST27 for east-west travel only	Substantially compliant ST20 ramp	Complaint ST21 ramp		
Replacement Priority	NA	NA	Medium	NA		
Recommendation	No change	No change	Add Detectable Warning Pattern per ST29	No change		
Cost Estimate			\$500			\$500

**Total construction cost to upgrade corridor**

\$105,500

NE 4th Avenue - NE Adams Street to NE Garfield Street						
NE 4th Ave is assumed east-west						
Intersection	NE 4th Ave/Adams Street - Also see NE Adams Street Corridor					Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	Steep grade substandard ramp	Steep grade substandard ramp	Substantially compliant ST20 ramp	Substantially compliant ST20 ramp		
Replacement Priority	High	High	Medium	Medium		
Recommendation	ST24	ST24	Add Detectable Warning Pattern per ST29	Correct excessive Lip and add Detectable Warning Pattern ST 29		
Cost Estimate	\$6,000	\$6,000	\$1,000	\$1,000		\$14,000
<b>Signal - Status of existing PPB</b>	Substantially compliant	Substantially compliant	Substantially compliant	East - Substantially compliant West - add PPB to move closer to crosswalk		
Replacement Priority	NA	NA	NA	low		
Recommendation	APS	APS	APS	APS		
Cost Estimate	\$2,000	\$2,000	\$2,000	\$3,000		\$9,000

NE 4th Avenue - NE Adams Street to NE Garfield Street						
Mid Block	NE 4th Ave from NE Adams Street to NE Birch Street					Subtotal
Distance from previous intersection	9 feet east	42 feet east	90 - 95 feet east	100 - 110 feet east	124 to 132 feet east	
Side of street	North	North	North	North	North	
Obstruction	Vertical lip	Sidewalk panel failure	Noncompliant sidewalk surface	Noncompliant sidewalk surface	Noncompliant sidewalk surface	
Recommendation	Correct excessive lip	Replace sidewalk panel	Replace sidewalk	Replace sidewalk	Replace sidewalk	
Cost Estimate	\$1,000	\$2,900	\$1,000	\$1,000	\$1,000	\$6,900
Distance from previous intersection	70 feet east - midblock crossing					
Side of street	North and south					
Status of existing curb ramp*	Substantially compliant flush ramps					
Recommendation	Add Detectable Warning Pattern per ST29 x 2					
Cost Estimate	\$1,000					\$1,000
Intersection	NE 4th Ave/Birch Street					Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	Substantially compliant flush ramps	Substantially compliant ramps	Substantially compliant ramps	Substantially compliant ST20 ramp		
Replacement Priority	Medium	Medium	Medium	Medium		
Recommendation	Add Detectable Warning Pattern per ST29 x 2	Correct Lip and add Detectable Warning Pattern per ST29 x 2	Correct Lip and Add Detectable Warning Pattern per ST29 x 2	Add Detectable Warning Pattern per ST29		
Cost Estimate	\$500	\$1,000	\$1,000	\$500		\$3,000

NE 4th Avenue - NE Adams Street to NE Garfield Street						
Mid Block	NE 4th Ave from NE Birch Street to NE Cedar Street					Subtotal
Distance from previous intersection	23 feet east	34 feet east				
Side of street	North	South				
Obstruction	Vertical lip	Vertical lip				
Recommendation	Correct sunken water meter lid	Correct unmarked utility lid				
Cost Estimate	\$1,000	\$2,000				\$3,000
Intersection	NE 4th Ave/Cedar Street					Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	Substantially compliant ramp	Substantially compliant ramp	Substantially compliant ramp	Substantially compliant ramp		
Replacement Priority	Medium	Medium	Medium	Medium		
Recommendation	Add Detectable Warning Pattern per ST29	Add Detectable Warning Pattern per ST29	Add Detectable Warning Pattern per ST29	Add Detectable Warning Pattern per ST29		
Cost Estimate	\$500	\$500	\$500	\$500		\$2,000

NE 4th Avenue - NE Adams Street to NE Garfield Street						
Mid Block	NE 4th Ave from NE Cedar Street to NE Dallas Street					Subtotal
Distance from previous intersection	12 to 26 feet east	43 feet east	65 feet east - midblock crossing	97 to 126 feet east	210 to 217 feet east	
Side of street	North	North	North and south	North	North	
Obstruction	Noncompliant sidewalk surface	Vertical lip	Substantially compliant flush ramps	Noncompliant sidewalk surface	Noncompliant sidewalk surface	
Recommendation	Replace sidewalk	Correct water meter	Add Detectable Warning Pattern per ST29 x 2	Replace sidewalk	Replace sidewalk	
Cost Estimate	\$1,000	\$1,000	\$500	\$1,300	\$1,000	\$4,800
Distance from previous intersection	18 to 37 feet east	56 to 65 feet east	88 feet east	91 - 110 feet east	116 to 127 feet	
Side of street	South	South	South	South	South	
Obstruction	Noncompliant sidewalk surface	Noncompliant sidewalk surface	Exposed drain pipe	Noncompliant sidewalk surface	Noncompliant sidewalk surface	
Recommendation	Replace sidewalk	Replace sidewalk	Repair section	Replace sidewalk	Replace sidewalk	
Cost Estimate	\$1,000	\$1,000	\$1,000	\$1,000	\$1,000	\$5,000
Distance from previous intersection	142 feet east					
Side of street	South					
Obstruction	Vertical lip					
Recommendation	Repair sunken water meter lid					
Cost Estimate	\$1,000					\$1,000

NE 4th Avenue - NE Adams Street to NE Garfield Street						
Intersection	NE 4th Ave/Dallas Street					Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	Substantially complaint ST20A ramp	Substantially complaint ST20A ramp	Substantially compliant ST20 ramp	OK substandard Ramp		
Replacement Priority	Medium	Medium	Medium	Medium		
Recommendation	Correct excessive Lip and add Detectable Warning Pattern per ST29	Correct excessive Lip and add Detectable Warning Pattern per ST29	Correct excessive Lip and add Detectable Warning Pattern per ST29	Correct excessive Lip and add Detectable Warning Pattern per ST29		
Cost Estimate	\$1,000	\$1,000	\$1,000	\$1,000		\$4,000
Mid Block	NE 4th Ave from NE Dallas Street to NE Everett Street					Subtotal
Distance from previous intersection	132 feet east	132 - 147 feet east	175 to 185 feet east	203 to 216 feet east	39 feet east	
Side of street	North	North	North	North	South	
Obstruction	Vertical lip	Noncompliant sidewalk surface	Noncompliant sidewalk surface	Noncompliant sidewalk surface	Vertical lip	
Recommendation	Correct excessive lip	Replace sidewalk	Replace sidewalk	Replace sidewalk	Correct sunken water meter	
Cost Estimate	\$1,000	\$1,000	\$1,000	\$1,000	\$1,000	\$5,000
Distance from previous intersection	78 feet east	95 feet east	110 to 113 feet east	176 feet east		
Side of street	South	South and north	South	South		
Obstruction	Vertical lip	Substantially compliant flush ramps	Vertical lip	Vertical lip		
Recommendation	Correct excessive lip	Add Detectable Warning Pattern per ST29 x 2	Correct excessive lip	Correct vault lid		
Cost Estimate	\$1,000	\$500	\$1,000	\$1,000		\$3,500

NE 4th Avenue - NE Adams Street to NE Garfield Street						
Intersection	NE 4th Ave/Everett Street					Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	Substantially complaint ST20 ramp	Substantially complaint ramps	Substantially compliant ST26 ramps	Substantially compliant ST20 ramp		
Replacement Priority	Medium	Medium	Medium	Medium		
Recommendation	Correct excessive Lip and add Detectable Warning Pattern per ST29	Correct excessive Lip and add Detectable Warning Pattern per ST29 x 2	Add Detectable Warning Pattern per ST29	Correct excessive Lip and add Detectable Warning Pattern per ST29		
Cost Estimate	\$1,000	\$1,000	\$500	\$1,000		\$3,500
Mid Block	NE 4th Ave from NE Everett Street to NE Franklin Street					Subtotal
Distance from previous intersection	110 feet east midblock crossing	Entire block				
Side of street	North and south	South				
Obstruction	Substantially complaint ramps	Noncompliant sidewalk surface				
Recommendation	Medium	High				
Recommendation	Add Detectable Warning Pattern per ST29 x 2	Replace sidewalk				
Cost Estimate	\$500	\$7,800				\$8,300

NE 4th Avenue - NE Adams Street to NE Garfield Street						
Intersection	NE 4th Ave/Franklin Street					Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	Substantially complaint ST20 ramp	Substantially complaint ST26 ramps	Noncompliant curb ramp	Substantially complaint ST20 ramp		
Replacement Priority	Medium	Medium	High	Medium		
Recommendation	Add Detectable Warning Pattern per ST29	Correct excessive Lip and add Detectable Warning Pattern per ST29	ST21 and address grate	Add Detectable Warning Pattern per ST29		
Cost Estimate	\$500	\$1,000	\$6,000	\$500		\$8,000
Mid Block	NE 4th Ave from NE Franklin Street to NE Garfield Street					Subtotal
Distance from previous intersection	70 to 86 feet east	113 - 147 feet east	179 to 186 feet east	82 to 92 feet east	101 feet east	
Side of street	North	North	North	South	South	
Obstruction	Noncompliant sidewalk surface	Noncompliant sidewalk surface	Noncompliant sidewalk surface	Noncompliant sidewalk surface	Vertical lip	
Recommendation	Correct sidewalk surface	Replace sidewalk	Replace sidewalk	Replace sidewalk	Correct excessive lip	
Cost Estimate	\$1,000	\$1,500	\$1,000	\$1,000	\$1,000	\$5,500

NE 4th Avenue - NE Adams Street to NE Garfield Street						
Intersection	NE 4th Ave/Garfield Street					Subtotal
Quadrant	SW	NW	NE	SE		
City of Camas	Substantially compliant ST20 ramp	Substantially compliant ST20 ramp	Substantially compliant ST20 ramp	Substantially compliant ST20 ramp		
ADA Self-evaluation Plan	Medium	Medium	Medium	Medium		
Recommendation	Add Detectable Warning Pattern per ST29	Correct excessive lip and add detectable warning pattern per ST29	Correct excessive lip and add detectable warning pattern per ST29	Correct excessive lip and add detectable warning pattern per ST29		
Cost Estimate	\$1,000	\$1,000	\$1,000	\$1,000		\$4,000

**Total construction cost to upgrade corridor**

\$91,500

NE 5th Avenue - NE Adams Street to NE Garfield Street						
NE 5th Ave is assumed east - west						
Intersection	NE Adams Street/NE 5th Ave - also see NE Adams Street Corridor					Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	Substantially compliant ST20A - 1/2 ramp	No ramp due to driveway	Substantially compliant ST20 ramp	Substantially compliant ST20 ramp		
Replacement Priority	Medium	NA	Medium	Medium		
Recommendation	Correct excessive Lip and add Detectable Warning Pattern per ST29	No change	Add Detectable Warning Pattern per ST29	Add Detectable Warning Pattern per ST29		
Cost Estimate	\$1,000		\$500	\$500		\$2,000
Mid Block	NE 5th Avenue - NE Adams Street to NE Birch Street					Subtotal
Distance from previous intersection	127 to 171 feet east	189 - 213 feet east	135 feet east	139 to 143 feet east	183 feet east	
Side of street	North	North	South	South	South	
Obstruction	Noncompliant sidewalk surface	Noncompliant sidewalk surface	Vertical lip	Noncompliant sidewalk surface	Vertical lip	
Recommendation	Replace sidewalk	Replace sidewalk	Correct excessive lip	Replace sidewalk	Correct sunken water meter	
Cost Estimate	\$2,000	\$1,100	\$1,000	\$1,000	\$1,000	\$6,100

NE 5th Avenue - NE Adams Street to NE Garfield Street						
Intersection	NE 5th Avenue/NE Birch Street					Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	Substantially compliant ST20 ramp	Substantially compliant ST20 amp	Substantially compliant ST23 ramps	Compliant ST23 ramps		
Replacement Priority	Medium	Medium	Medium	NA		
Recommendation	Correct excessive Lip and add Detectable Warning Pattern per ST29	Correct excessive Lip and add Detectable Warning Pattern per ST29	Add Detectable Warning Pattern per ST29	No change		
Cost Estimate	\$1,000	\$1,000	\$500			\$2,500
NOTE: Repair roadway in crosswalk between the SW and SE quadrant.						
Mid Block	NE 5th Avenue - NE Birch Street to NE Cedar Street					Subtotal
Distance from previous intersection	26 - 29 feet east	53 to 57 feet				
Side of street	South	South				
Obstruction	Noncompliant sidewalk surface	Noncompliant sidewalk surface				
Recommendation	Replace sidewalk	Replace sidewalk				
Cost Estimate	\$1,000	\$1,000				\$2,000

NE 5th Avenue - NE Adams Street to NE Garfield Street						
Intersection	NE 5th Avenue/NE Cedar Street					Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	Substantially compliant ST20 ramp	Substantially compliant ST20 ramp	Substantially compliant ST20 ramp	Substantially compliant ST22 ramp		
Replacement Priority	Medium	High	Medium	Medium		
Recommendation	Add detectable warning pattern per ST29	Add detectable warning pattern per ST29	Raise grate and add detectable warning pattern ST29 or replace ramp	Add detectable warning pattern per ST29		
Cost Estimate	\$500	\$500	\$1,000	\$500		\$2,500
Mid Block	NE 5th Avenue - NE Cedar Street to NE Dallas Street					Subtotal
Distance from previous intersection	58 feet east	62 feet east	148 to 151 feet east	157 to 169 feet east		
Side of street	South	South	North	North		
Obstruction	Vertical lip	Vertical lip	Noncompliant sidewalk surface	Noncompliant sidewalk surface		
Recommendation	Correct sunken water meter lid	Correct excessive Lip and add Detectable Warning Pattern per ST29	Replace sidewalk	Replace sidewalk		
Cost Estimate	\$1,000	\$1,000	\$1,000	\$1,000		\$4,000

NE 5th Avenue - NE Adams Street to NE Garfield Street						
Intersection	NE 5th Avenue/NE Dallas Street					Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	Substantially compliant ST20A ramp	Substantially compliant 20A ramp	Substantially compliant 20A ramp	Substantially compliant 20A ramp		
Replacement Priority	Medium	Medium	Medium	Medium		
Recommendation	Correct excessive lip and add detectable warning pattern per ST29	Correct excessive lip and add detectable warning pattern per ST29	Correct excessive lip and add detectable warning pattern per ST29	Correct excessive Lip and add Detectable Warning Pattern per ST29		
Cost Estimate	\$1,000	\$1,000	\$1,000	\$1,000		\$4,000
<b>Mid Block</b>	<b>NE 5th Avenue - NE Dallas Street to NE Everett Street</b>					<b>Subtotal</b>
	Midblock section substantially compliant.					
Intersection	NE 5th Avenue/NE Everett Street					Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	Substantially compliant ST20 ramp	Substantially compliant ST22 ramp	Substantially compliant ramp ST23 and ST25 ramps	Substantially compliant ST26 ramps		
Replacement Priority	Medium	Medium	Medium	Medium		
Recommendation	Correct excessive lip and add detectable warning pattern per ST29	Correct excessive lip and add detectable warning pattern per ST29 x 2	Correct excessive lip x 1 and add detectable warning pattern per ST29 x 2	Add detectable warning pattern per ST 29 X 2		
Cost Estimate	\$1,000	\$1,000	\$1,000	\$500		\$3,500

NE 5th Avenue - NE Adams Street to NE Garfield Street						
Mid Block	NE 5th Avenue - NE Everett Street to NE Franklin Street				Subtotal	
Distance from previous intersection	140 feet east	206 feet east	211 feet east			
Side of street	North	North	South			
Obstruction	Vertical lip	Vertical lip	Vertical lip			
Recommendation	Correct sunken water meter lid	Correct excessive lip	Correct excessive lip			
Cost Estimate	\$1,000	\$1,000	\$1,000			\$3,000
Intersection	NE 5th Avenue/NE Franklin Street				Subtotal	
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	Substantially compliant ST26 ramps	Substantially compliant ST20 ramp	Substantially compliant ST27 ramp	Substantially compliant ST20A 1/2 ramp		
Replacement Priority	Medium	NA	NA	NA		
Recommendation	Add detectable warning pattern per ST 29 X 2	No change	No change	No change		
Cost Estimate	\$500					\$500
NOTE: North side of "T" is missing sidewalk.						
						\$0
Mid Block	NE 5th Avenue - NE Franklin Street to NE Garfield Street				Subtotal	
Distance from previous intersection	172 to 202 feet east	100 feet east	177 - 184 feet east			
Side of street	North	South	South			
Obstruction	Noncompliant sidewalk surface	Noncompliant sidewalk surface	Noncompliant sidewalk surface			
Recommendation	Replace sidewalk	ST117	Replace depressed sidewalk			
Cost Estimate	\$1,300	\$1,000	\$1,000			\$3,300

NE 5th Avenue - NE Adams Street to NE Garfield Street						
Intersection	NE 5th Ave/NE Garfield Street					Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	Compliant ST25 - North-south only	Compliant ST25 - North-south only	Substantially compliant ST20 ramp	Compliant ST25 - North-south only		
Replacement Priority	Low	Low	Medium	High		
Recommendation	No change	No change	Add detectable warning pattern per ST29	No change		
Cost Estimate			\$500			\$500

**Total construction cost to upgrade corridor**

**\$33,900**

Crown Park Neighborhood						
Intersection	Division Street/NW 17th Ave (north intersection)					Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	Substantially compliant ST20 ramp	Substantially compliant ST20A 1/2 ramp	No curb ramp	No curb ramp		
Replacement Priority	Medium	Medium	High	High		
Recommendation	Add Detectable Warning Pattern per ST29	Add Detectable Warning Pattern per ST29	ST26	ST26		
Cost Estimate	\$500	\$500	\$6,000	\$6,000		\$13,000
Intersection	NE 17th Ave/Division Street (south intersection)					Subtotal
Quadrant	SW	NW	NE	SE	SE	
Status of existing curb ramp*	No curb ramp	No curb ramp	Compliant ramp	Compliant ramp		
Replacement Priority	NA	NA	Low	low		
Recommendation	pedestrian crossing on east leg	pedestrian crossing on east leg	No change	No change		
Cost Estimate	\$6,000	\$6,000				\$12,000
Mid Block	Division Street NW 17th Ave to NW 16th Ave (west side only)					Subtotal
Distance from previous intersection	11 feet south	26 feet south				
Side of street	West	West				
Obstruction	Encroachment	Vertical lip				
Recommendation	Trim foliage	Correct excessive lip				
Cost Estimate	\$1,000	\$1,000				\$2,000
Mid Block	Division Street NE 17th Ave to NE 15th Ave (east side only)					Subtotal
Distance from previous intersection	227 - 232 south					
Side of street	East					
Obstruction	Noncompliant sidewalk surface					
Recommendation	Replace sidewalk					
Cost Estimate	\$1,000					\$1,000

Crown Park Neighborhood						
Intersection	Division Street/NW 16th Ave					Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	Substantially compliant road approach	Substantially compliant road approach	No curb ramp	Substantially compliant road approach		
Replacement Priority	High	High	NA	High		
Recommendation	Add Detectable Warning Pattern per ST29	Add Detectable Warning Pattern per ST29	crosswalk on south leg	Add Detectable Warning Pattern per ST29		
Cost Estimate	\$500	\$500		\$500		\$1,500
Mid Block	Division Street NW 16th Ave to NW/NE 15th Ave (west side only)					Subtotal
Distance from previous intersection	1 foot south	10 feet south	19 feet south	27 feet south	60 feet south	
Side of street	West	West	West	West and east	West	
Obstruction	Vertical lip	Vertical lip	Vertical lip	Noncompliant midblock crossing ramps	Vertical lip	
Recommendation	Correct excessive ACP Patch	Correct excessive lip	Correct excessive lip	ST26 x 2	Correct excessive lip	
Cost Estimate	\$1,000	\$1,000	\$1,000	\$1,000	\$1,000	\$5,000
Distance from previous intersection	103 feet south	141 feet south				
Side of street	West	West				
Obstruction	Vertical lip	Noncompliant sidewalk surface				
Recommendation	Correct excessive	Replace panel				
Cost Estimate	\$1,000	\$1,000				\$2,000

Crown Park Neighborhood						
Intersection	NE 15th Ave/NW 15th Ave/Division Street					Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	Substantially compliant ST20 ramp	Substantially compliant ST20 ramp	Substantially compliant ST20 Ramp	Substantially compliant ST20 ramp		
Replacement Priority	Medium	Medium	Medium	Medium		
Recommendation	Correct excessive lip and add detectable warning pattern per ST29	Correct excessive lip and add detectable warning pattern per ST29	Correct excessive lip and add detectable warning pattern per ST29	Correct excessive lip and add detectable warning pattern per ST29	Repair AC on north street	
Cost Estimate	\$1,000	\$1,000	\$1,000	\$1,000	\$1,000	\$5,000
<b>Mid Block</b>	<b>NE 15th Ave from Division Street to Everett Street (north side only)</b>					<b>Subtotal</b>
Distance from previous intersection	168 feet east	235 feet east	324 - 400 feet east	678 - 712 feet east	925 - 929 feet east	
Side of street	North	North	North	North	North	
Obstruction	Vertical lip	Vertical lip	Noncompliant sidewalk surface	Noncompliant sidewalk surface	Noncompliant sidewalk surface	
Recommendation	Correct excessive lip	Correct excessive lip	Replace sidewalk	Replace sidewalk	Replace sidewalk	
Cost Estimate	\$1,000	\$1,000	\$1,000	\$1,000	\$1,000	\$5,000
<b>Mid Block</b>	<b>NE 15th Street from Division Street to NE Adams Street (south side only)</b>					<b>Subtotal</b>
Distance from previous intersection	114 feet east	126 feet east	166 feet east	186 feet east	202 feet east	
Side of street	South	South	South	South	South	
Obstruction	Noncompliant sidewalk surface	Vertical lip	Noncompliant sidewalk surface	Vertical lip	Vertical lip	
Recommendation	Replace sidewalk	Correct excessive lip	Replace sidewalk	Correct excessive lip	Correct excessive lip	
Cost Estimate	\$1,000	\$1,000	\$1,000	\$1,000	\$1,000	\$5,000

Crown Park Neighborhood						
Intersection	NE 15th Ave/Adams Street					Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	Substantially compliant ST23 ramps	Substantially compliant ST26 ramp	None - parking area	Substantially compliant ST20 ramp		
Replacement Priority	Medium	Medium	NA	Medium		
Recommendation	Bevel lips and add detectable warning pattern per ST29	Trim foliage, add detectable warning pattern per ST29	No change	Add detectable warning pattern per ST29		
Cost Estimate	\$1,000	\$1,000		\$1,000		\$3,000
Mid Block	NE 15th Street from Adams Street to Birch Street (south side only)					Subtotal
Distance from previous intersection	74 feet east	161 - 178 feet east	186 - 211 feet east			
Side of street	South	South	South			
Obstruction	Vertical lip	Noncompliant sidewalk surface	Noncompliant sidewalk surface			
Recommendation	Correct sunken water meter	Replace sidewalk	Replace sidewalk			
Cost Estimate	\$1,000	\$1,000	\$1,000			\$3,000
Intersection	NE 15th Ave/Birch Street					Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	Substantially compliant ST20 ramp	None - parking area	Substantially compliant ST26 amp	Substantially compliant ST23 ramps		
Replacement Priority	Medium		Medium	Medium		
Recommendation	Correct excessive lip and add detectable warning pattern per ST29		Correct excessive lip and add detectable warning pattern per ST29	Add detectable warning pattern per ST29 and repair roadway across Adams		
Cost Estimate	\$1,000		\$1,000	\$1,000		\$3,000

Crown Park Neighborhood						
<b>Mid Block</b>	<b>W 15th Ave from Birch Street to Dallas Street (south side only)</b>					<b>Subtotal</b>
Distance from previous intersection	28 - 205 feet east					
Side of street	South					
Obstruction	Noncompliant sidewalk surface					
Recommendation	Replace sidewalk					
Cost Estimate	\$4,900					\$4,900
<b>Intersection</b>	<b>NE 15th Ave/Dallas Street</b>					<b>Subtotal</b>
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	Substantially compliant ST23 ramps	Substantially compliant ST26 ramp	None - parking area	Poor condition substantially compliant ST20 ramp		
Replacement Priority	Medium	Medium	NA	High		
Recommendation	Correct excessive lip and add detectable warning pattern per ST29	Add detectable warning pattern per ST29	No change	Replace per ST26		
Cost Estimate	\$1,000	\$500		\$6,000	-	\$7,500

Crown Park Neighborhood						
Mid Block	NE 15th Ave from Dallas Street to Everett Street (south side only)					Subtotal
Distance from previous intersection	2 - 62 feet east	83 feet east	90 feet east	110 - 211 feet east		
Side of street	South	South	South	South		
Obstruction	Foliage encroaching	Vertical lip	Vertical lip	Noncompliant sidewalk surface and foliage encroaching		
Recommendation	Trim foliage	Correct excessive lip	Correct excessive lip	Replace sidewalk and trim foliage		
Cost Estimate	\$500	\$1,000	\$1,000	\$2,800		\$5,300
Intersection	NE 15th Ave/Everett Street					Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	Substantially compliant ST23 ramps	Substantially compliant ST21 and ST28 ramps	Substantially compliant ST20A ramp	Substantially compliant ST20A ramp		
Replacement Priority	Medium	Medium	Medium	High		
Recommendation	Correct excessive lip add detectable warning pattern per ST29	Add detectable warning pattern per ST29	Correct excessive lip and add detectable warning pattern per ST29	Add detectable warning pattern per ST29		
Cost Estimate	\$1,000	\$500	\$1,000	\$500		\$3,000
Mid Block	Everett Street from NE 15th Ave to NE 17th Ave (east side only)					Subtotal
Distance from previous intersection	50 feet north	95 feet north				
Side of street	East	East				
Obstruction	Noncompliant sidewalk/driveway surface	Noncompliant sidewalk/driveway surface				
Recommendation	Replace sidewalk/driveway	Replace sidewalk/driveway				
Cost Estimate	\$1,100	\$1,100				\$2,200

Crown Park Neighborhood						
<b>Mid Block</b>	<b>Everett Street from NE 15th Ave to NE 17th Ave (west side only)</b>					
Distance from previous intersection	8 feet north	249 feet north	267 feet north			
Side of street	West	West	West			
Obstruction	Vertical lip	Vertical lip	Vertical lip			
Recommendation	Correct excessive lip	Correct excessive lip	Correct excessive lip			
Cost Estimate	\$1,000	\$1,000	\$1,000			\$3,000
<b>Intersection</b>	<b>NE 17th Ave/Everett Street</b>					<b>Subtotal</b>
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	Substantially compliant ramp	Substantially compliant Ramp	No curb ramp	No ramp due to driveway		
Replacement Priority	Medium	Medium	High	NA		
Recommendation	Correct excessive lip and add detectable warning pattern per ST29	Correct excessive lip and add detectable warning pattern per ST29 for N-S travel, Add ramp for E-W travel ST28	ST28	No change		
Cost Estimate	\$1,000	\$7,000	\$6,000			\$14,000

Crown Park Neighborhood						
Mid Block	NE 17th Ave from Everett Street to Division Street (south side only)					Subtotal
Distance from previous intersection	96 to 115 feet west	137 - 143 feet west	164 feet west	207 to 211 feet west	465 feet West	
Side of street	South	South	South	South	South	
Obstruction	Noncompliant sidewalk surface	Noncompliant sidewalk surface	Vertical lip	Noncompliant sidewalk surface	Vertical lip	
Recommendation	Replace sidewalk	Replace sidewalk	Correct excessive lip	Replace sidewalk	Correct excessive lip	
Cost Estimate	\$1,000	\$1,000	\$1,000	\$1,000	\$1,000	\$5,000
Distance from previous intersection	282 feet west	651 feet west	657 feet west	684 feet west	698 - 717 feet west	
Side of street	South	South	South	South	South	
Obstruction	Vertical lip	Vertical lip	Vertical lip	Inaccessible handicap parking ramp	Noncompliant sidewalk surface	
Recommendation	Correct excessive lip	Correct excessive lip	Correct excessive lip	Replace - ST28	Replace sidewalk	
Cost Estimate	\$1,000	\$1,000	\$1,000	\$1,000	\$1,000	\$5,000
Distance from previous intersection	722 - 734 feet west	761 feet west	780 - 789	845 - 852 feet west	862 feet west	
Side of street	South	South	South	South	South	
Obstruction	Steep noncompliant driveway ramp in walking area	Noncompliant sidewalk surface	Noncompliant sidewalk surface along wall	Noncompliant Sidewalk	Vertical lip	
Recommendation	Replace - ST17	Replace sidewalk	Replace sidewalk	Replace sidewalk	Correct excessive lip	
Cost Estimate	\$1,000	\$1,000	\$1,000	\$1,000	\$1,000	\$5,000
Distance from previous intersection	987 feet west					
Side of street	South					
Obstruction	Vertical lip					
Recommendation	Correct excessive lip					
Cost Estimate	\$1,000					\$1,000

Crown Park Neighborhood						
Mid Block	NE 17th Ave from Everett Street to Dallas Street (north side only)					Subtotal
Distance from previous intersection	19 to 140 feet west	160 to 228 feet west	228 to 238 feet west	264 feet west		
Side of street	North	North	North	North		
Obstruction	No sidewalk	No sidewalk	Noncompliant sidewalk surface	Vertical lip		
Recommendation	Assess need for sidewalk	Assess need for sidewalk	Replace sidewalk	Correct excessive lip		
Cost Estimate	\$3,400	\$1,900	\$1,000	\$1,000		\$7,300
Intersection	NE 17th Ave/Dallas Street					Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	No curb ramp	East -Compliant ST26 ramp - South - No curb ramp	Compliant ST23 ramps	Compliant ST24 ramp		
Replacement Priority	High	High	NA	NA		
Recommendation	ST25	ST24	No change	No change		
Cost Estimate	\$6,000	\$6,000				\$12,000

Crown Park Neighborhood						
Mid Block	NE 17th Ave form Dallas Street to Division Street (north side only)					Subtotal
Distance from previous intersection	43 to 47 feet west	90 - 98 feet west	312 feet west	338 feet west	362 feet west	
Side of street	North	North	North	North	North	
Obstruction	Noncompliant sidewalk surface	Noncompliant sidewalk surface	Vertical lip	Vertical lip	Vertical lip	
Recommendation	Replace sidewalk	Replace sidewalk	Correct excessive lip	Correct excessive lip	Correct excessive lip	
Cost Estimate	\$1,000	\$1,000	\$1,000	\$1,000	\$1,000	\$5,000
Distance from previous intersection	381 to 385 feet west	409 feet west	469 to 704 feet west			
Side of street	North	North	North			
Obstruction	Noncompliant sidewalk surface	Vertical lip	No sidewalk			
Recommendation	Replace sidewalk	Correct excessive lip	Assess need for sidewalk			
Cost Estimate	\$1,000	\$1,000	\$6,500			\$8,500

**Total construction cost to upgrade area**

\$149,200

West Highland Neighborhood						
Intersection	NE Garfield Street/NE 14th Ave					Subtotal
Inventory	SW	NW	NE	SE		
Status of existing curb ramp*	No curb ramp	Substantially compliant ST27 Modified ramp	Substantially compliant ST20 ramp	Substantially compliant ST21 ramp		
Replacement Priority	High	Medium	Medium	Medium		
Recommendation	ST21	Add Detectable Warning Pattern per ST29	Add Detectable Warning Pattern per ST29	Add Detectable Warning Pattern per ST29		
Cost Estimate	\$6,000	\$500	\$500	\$500		\$7,500
Mid Block	NE Garfield Street from NW 14th Ave to NW 15th Ave (west side only)					Subtotal
Distance from previous intersection	No sidewalk entire block - 215 feet					
Side of street	West					
Obstruction	NA					
Recommendation	No ADA upgrade required					
Cost Estimate	\$8,000					\$8,000
Intersection	NE Garfield Street/15th Ave					Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	Compliant ST27 Type ramp	Compliant ST26 Type ramp	Compliant ST117 Type Sidewalk Ramp	Compliant ST117 Type Sidewalk Ramp		
Replacement Priority	NA	NA	Medium	NA		
Recommendation	No change	No change	Add Detectable Warning Pattern per ST29	No change		
Cost Estimate			\$500			\$500

West Highland Neighborhood						
<b>Mid Block</b>	<b>NE Garfield Street from NW 14th Ave to NW 15th Ave (east side only)</b>					<b>Subtotal</b>
Distance from previous intersection						
Side of street						
Obstruction						
Recommendation						
Cost Estimate						\$0
<b>Mid Block</b>	<b>NE Garfield Street from NE 15th Ave to NE 19th Ave (west side only)</b>					<b>Subtotal</b>
Distance from previous intersection	40 to 115 feet north	115 to 135 feet north				
Side of street	West	West				
Obstruction	Foliage encroaching	Noncompliant sidewalk				
Recommendation	Trim foliage	Replace sidewalk				
Cost Estimate	\$2,000	\$1,000				\$3,000
<b>Intersection</b>	<b>NE Garfield Street/NE 19th Ave</b>					<b>Subtotal</b>
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	Compliant ST26 Type ramp	Compliant ST26 Type ramp	Compliant ST117 Type ramp	Compliant ST117 Type ramp		
Replacement Priority	NA	NA	NA	NA		
Recommendation	No change	No change	No change	No change		
Cost Estimate						\$0
<b>Mid Block</b>	<b>NE Garfield Street from NE 19th Ave to 22nd Ave (east side only)</b>					<b>Subtotal</b>
Distance from previous intersection						
Side of street						
Obstruction						
Recommendation						
Cost Estimate						\$0

West Highland Neighborhood						
<b>Mid Block</b>	<b>NE Garfield Street from NE 19th Ave to 22nd Ave (west side only)</b>					<b>Subtotal</b>
Distance from previous intersection		285 to 290 feet north	310 to 530 feet north			
Side of street		West	West			
Obstruction		No sidewalk	No sidewalk			
Recommendation		Construct sidewalk	Construct sidewalk - minus two driveways			
Cost Estimate		\$1,000	\$7,000			\$8,000
<b>Intersection</b>	<b>NE Garfield/NE 22nd Ave</b>					<b>Subtotal</b>
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	Substantially compliant type ST21 ramp	None - Residential Driveway	Compliant ST 117 ramp	Substantially compliant type ST21 ramp		
Replacement Priority	Medium	NA	NA	NA		
Recommendation	Add detectable warning pattern per ST29	No change	No change	No change		
Cost Estimate	\$500					\$500
<b>Mid Block</b>	<b>NE 22nd Ave from Garfield Street to NE lone Street (north side only)</b>					<b>Subtotal</b>
Distance from previous intersection	No obstructions noted					
Side of street						
Obstruction						
Recommendation						
Cost Estimate						\$0
<b>Mid Block</b>	<b>NE 22nd Ave from Garfield Street to NE lone Street (south side only)</b>					<b>Subtotal</b>
Distance from previous intersection	No obstructions					
Side of street						
Obstruction						
Recommendation						
Cost Estimate						\$0

West Highland Neighborhood						
Intersection	NE lone Street/NE 22nd Ave					Subtotal
Quadrant	SW	NW	NE	SE	NE	
Status of existing curb ramp*	Substantially compliant ST20 ramp	Substantially compliant ST21A Type ramp	Substantially compliant ST20 ramp	None	No sidewalk across street	
Replacement Priority	NA	Medium	Medium	NA		
Recommendation		Add detectable warning pattern per ST29	Add Detectable Warning Pattern per ST29	No change - no sidewalk		
Cost Estimate		\$500	\$500			\$1,000
Mid Block	NE 22nd Avenue from NE lone to stadium					Subtotal
Distance from previous intersection						
Side of street	North	South				
Obstruction	none	No sidewalk				
Recommendation						
Cost Estimate						\$0
Mid Block	lone Street from NE 14th Ave to NE lone Loop (southern intersection)					Subtotal
Distance from previous intersection	320 feet east					
Side of street	North	South				
Obstruction	Noncompliant Driveway	No sidewalk				
Recommendation	ST 117					
Cost Estimate	\$3,000					\$3,000
Intersection	NE lone Street/NE lone Loop (southern intersection)					Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	None - Existing Driveway	No sidewalk on opposite side	None	None		
Replacement Priority	NA	NA	NA	NA		
Recommendation	No Change	No change	Add ramp when sidewalk is added to east side	Add ramp when sidewalk is added to east side		
Cost Estimate						\$0

West Highland Neighborhood						
<b>Mid Block</b>	<b>NE lone Street - west side only between curve and lone Loop (norther intersection)</b>					<b>Subtotal</b>
Distance from 1st driveway nearest curve	0 feet - starting point	180 feet north				
Side of street	West	West				
Obstruction	150' of noncompliant driveway	90' of noncompliant driveway				
Recommendation	ST117	ST117				
Cost Estimate	\$7,500	\$4,500				\$12,000
<b>Mid Block</b>	<b>NE lone Street - east side only between NE lone Loop intersections</b>					<b>Subtotal</b>
NA	Entire Block - 280 feet					
Side of street	East					
Obstruction	No sidewalk					
Recommendation	NA					
Cost Estimate						\$0
<b>Intersection</b>	<b>NE lone Street/NE lone Loop (northern intersection)</b>					<b>Subtotal</b>
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	None	None	None	None		
Replacement Priority	NA	NA	NA	NA		
Recommendation	Add ramp when sidewalk is added to east side	Add ramp when sidewalk is added to east side	Add ramp when sidewalk is added to east side	Add ramp when sidewalk is added to east side		
Cost Estimate						\$0
<b>Mid Block</b>	<b>NE lone Street - (east side only between NE lone Loop and NE 19th Ave intersections)</b>					<b>Subtotal</b>
Distance from previous intersection	Entire Block - 220 feet					
Side of street	East					
Obstruction	No sidewalk					
Recommendation	Construct sidewalk					
Cost Estimate						\$0

West Highland Neighborhood						
Intersection	NE lone Street/NE 19th Ave					Subtotal
Quadrant	SW	NW	NE	SE		
Status of existing curb ramp*	No curb ramp	No curb ramp	Compliant ramp	No sidewalk		
Replacement Priority	NA	High	NA	NA		
Recommendation	No sidewalk on opposite side of street	ST117	No change	Add ramp when sidewalk is added to east side		
Cost Estimate		\$6,000				\$6,000
Mid Block	NE lone Street - (east side only between NE 19th Ave and NE 22nd Ave)					Subtotal
Distance from previous intersection	1 foot north	5 and 9 feet north	35 feet north	600 feet north		
Side of street	East	East	East	East		
Obstruction	Utility Pole in sidewalk	Utility Pole Guywires in sidewalk	Speed limit sign in sidewalk	No sidewalk		
Recommendation	Construct sidewalk bump out	Construct sidewalk bump out	Construct sidewalk bump out			
Cost Estimate		\$8,000				\$8,000

**Total construction cost to upgrade area** \$57,500

Inventory Row Terms	Definitions
At intersections: Quadrant:	Corner of intersection including both direction of pedestrian travel For "tee" intersections, it includes the section of sidewalk at the location of corner if the intersection had 4 legs.
Status of existing curb ramp:	Brief description of existing ADA ramp. Often described by comparison with current Camas street standard plans for ADA ramp where it applies
Replacement Priority:	Professional judgement of need to replace an existing curb ramp categorized as High need, Medium need, and Low need, based on the condition of the ramp. The criteria used for the judging the replacement priority is as follows:
Low priority for replacement:	Substantial compliance with WSDOT Field Guide and <b>reasonable access</b>
Medium priority for replacement:	Replace without: Flat landing close to ramp at top or base Width of ramp throat 3 feet or more Lip of gutter less than 1/4"  Possible minor upgrades Add warning pattern Grind lip
High priority for replacement:	Replace if: No curb ramp at the corner of intersection Utility obstruction such as catch basin Steep ramp without landing Damaged ramp with tripping hazards
If intersection is signalized: Signal - Status of existing PPB	Does the signal have pedestrian push buttons that meet current standard for Accessible Pedestrian Signals (APS).
Replacement Priority	Professional judgement if non-compliant signal should be upgraded to current APS standards.
High Priority to upgrade traffic signals:	Height of ped push button above 48" Reach to ped push button with over 24" reach.
At mid-block locations: Distance from previous intersection	Offset in feet and direction from previous intersection listed in the inventory.
Side of street:	Side of street of the block inventoried.
Obstruction:	Description of sidewalk obstruction that included: Steep Driveways (less than 8 feet width of sidewalk) Note obstructions such as signs, poles and private property. Tripping Hazards (greater than 1/2" lift or width of crack) Narrow - continuous width less than 4 feet. Surface failure, such as spalling. Short missing sections
Recommendation:	Recommended improvements based on Camas standard plans and WSDOT APS standards
Cost Estimate:	Planning level estimate of ADA ramps installation, sidewalk repair and APS upgrade costs

## **Appendix F – Citizen Request Notice and Grievance Procedures**

- Notice Procedure Under The Americans with Disabilities Act (sample form)
- Grievance Procedure Under The Americans with Disabilities Act (sample form)

[Name of public entity]

## NOTICE UNDER THE AMERICANS WITH DISABILITIES ACT

In accordance with the requirements of Title II of the Americans with Disabilities Act of 1990 (“ADA”), **[name of public entity]** will not discriminate against qualified individuals with disabilities on the basis of disability in its services, programs, or activities.

**Employment:** **[Name of public entity]** does not discriminate on the basis of disability in its hiring or employment practices and complies with all regulations promulgated by the U.S. Equal Employment Opportunity Commission under Title I of the ADA.

**Effective Communication:** **[Name of public entity]** will generally, upon request, provide appropriate aids and services leading to effective communication for qualified persons with disabilities so they can participate equally in the **[name of public entity’s]** programs, services, and activities, including qualified sign language interpreters, documents in Braille, and other ways of making information and communications accessible to people who have speech, hearing, or vision impairments.

**Modifications to Policies and Procedures:** **[Name of public entity]** will make all reasonable modifications to policies and programs to ensure that people with disabilities have an equal opportunity to enjoy all of its programs, services, and activities. For example, individuals with service animals are welcomed in **[name of public entity]** offices, even where pets are generally prohibited.

Anyone who requires an auxiliary aid or service for effective communication, or a modification of policies or procedures to participate in a program, service, or activity of [name of public entity], should contact the office of **[name and contact information for ADA Coordinator]** as soon as possible but no later than 48 hours before the scheduled event.

The ADA does not require **[name of public entity]** to take any action that would fundamentally alter the nature of its programs or services, or impose an undue financial or administrative burden.

Complaints that a program, service, or activity of [name of public entity] is not accessible to persons with disabilities should be directed to **[name and contact information for ADA Coordinator]**.

**[Name of public entity]** will not place a surcharge on a particular individual with a disability or any group of individuals with disabilities to cover the cost of providing auxiliary aids/services or reasonable modifications of policy, such as retrieving items from locations that are open to the public but are not accessible to persons who use wheelchairs.

## [Name of public entity]

### Grievance Procedure Under The Americans with Disabilities Act

This Grievance Procedure is established to meet the requirements of the Americans with Disabilities Act of 1990 (“ADA”). It may be used by anyone who wishes to file a complaint alleging discrimination on the basis of disability in the provision of services, activities, programs, or benefits by the **[name of public entity]**. The **[e.g., State, City, County, Town]**’s Personnel Policy governs employment-related complaints of disability discrimination.

The complaint should be in writing and contain information about the alleged discrimination such as name, address, phone number of complainant and location, date, and description of the problem. Alternative means of filing complaints, such as personal interviews or a tape recording of the complaint, will be made available for persons with disabilities upon request.

The complaint should be submitted by the grievant and/or his/her designee as soon as possible but no later than 60 calendar days after the alleged violation to:

***[Insert ADA Coordinator’s name]***  
***ADA Coordinator [and other title if appropriate]***  
***[Insert ADA Coordinator’s mailing address]***

Within 15 calendar days after receipt of the complaint, **[ADA Coordinator’s name]** or **[his/her]** designee will meet with the complainant to discuss the complaint and the possible resolutions. Within 15 calendar days of the meeting, **[ADA Coordinator’s name]** or **[his/her]** designee will respond in writing, and where appropriate, in a format accessible to the complainant, such as large print, Braille, or audio tape. The response will explain the position of the **[name of public entity]** and offer options for substantive resolution of the complaint.

If the response by **[name of ADA coordinator]** or **[his/her]** designee does not satisfactorily resolve the issue, the complainant and/or his/her designee may appeal the decision within 15 calendar days after receipt of the response to **the [City Manager/ County Commissioner, or other appropriate high-level official]** or **[his/her]** designee.

Within 15 calendar days after receipt of the appeal, the **[City Manager/ County Commissioner/ other appropriate high-level official]** or **[his/her]** designee will meet with the complainant to discuss the complaint and possible resolutions. Within 15 calendar days after the meeting, the **[City Manager/ County Commissioner/ other appropriate high-level official]** or **[his/her]** designee will respond in writing, and, where appropriate, in a format accessible to the complainant, with a final resolution of the complaint.

All written complaints received by **[name of ADA coordinator]** or **[his/her]** designee, appeals to the **[City Manager/ County Commissioner/other appropriate high-level official]** or **[his/her]** designee, and responses from these two offices will be retained by the **[public entity]** for at least three years.



# Staff Report

April 6, 2026 Council Workshop Meeting

ADA Transition Plan Update Professional Services Agreement Amendment 2

Presenter: James Carothers, Engineering Manager

Time Estimate: 5 minutes

Phone	Email
360.817.7230	jcarothers@cityofcamas.us

**BACKGROUND:** The City’s initial and current Americans with Disabilities (ADA) Self Evaluation (SEP) and Transition Plan, commonly and collectively called the ADA Transition Plan, was created in 2015. This plan needed to be updated. The main purposes of the plan are to:

- Identify physical obstacles in the City’s facilities that limit the accessibility of its programs or activities to individuals with disabilities.
- Describe how the facilities will be made accessible.
- Provide a timeframe and schedule for completing facility upgrades.
- Continue efforts to fully comply with all aspects of the ADA.

Keeping an updated and active transition plan is required for local agencies to remain eligible for federal grant funding. Additionally, agencies in southwest Washington with a plan less than 10 years old score higher on federal grant applications filed with the Regional Transportation Council (RTC).

**SUMMARY:** In March 2025 Council approved a Professional Services Agreement (PSA) with Apex to complete the ADA Transition Plan Update. This agreement included interaction with community members and organizations that promote the mobility of the disabled population. These stakeholders made up an advisory committee that provided input on areas to emphasize upgrades to ADA facilities in the public rights-of-way.

Upon input from the committee and survey comments from the community, staff determined that the downtown area needed to be analyzed in more detail to determine the high priority needs and define potential low-cost interim mobility improvements for timely implementation. The additional tasks and related costs are as follows:

- |  |                 |
|--|-----------------|
| • Additional Downtown Inventory          | \$5,780         |
| • Additional Downtown Prioritizing       | \$3,300         |
| • Additional Mapping Using GIS           | \$2,130         |
| • Additional Scoping of ADA Improvements | \$1,349         |
| • Additional Cost Estimate Determination | \$1,309         |
| <b>Total Additional Cost</b>             | <b>\$13,868</b> |

Original ADA Transition Plan Update PSA	\$105,797
PSA Amendment 1 (Time extension only)	\$ 0
Proposed PSA Amendment 2 (downtown analysis)	<u>\$ 13,868</u>
Total Expenditures	\$119,665

**BENEFITS TO THE COMMUNITY:** ADA improvements allow all community members to use pedestrian access routes within the public rights-of-way. This plan is in accordance with 4.4.2 Walking, Biking, and ADA Mobility Goals and Policies section of the Camas Comprehensive Plan.

**STRATEGIC PLAN:** By planning for the removal of mobility barriers within the public rights-of-way, under the *Safe and Accessible Community* priority, *Improve transportation and recreation safety and accessibility for all ages and abilities.*

**POTENTIAL CHALLENGES:** There are no known challenges associated with this PSA Amendment.

**BUDGET IMPACT:** This amendment is funded by Engineering Operations Professional Services. There is ample budget available in this line item in the 2026 budget.

**RECOMMENDATION:** Staff recommends this item be placed on the April 20, 2026 Council Regular Meeting Consent Agenda for Council’s consideration.



**CITY OF CAMAS  
PROFESSIONAL SERVICES AGREEMENT  
Amendment No. 2**

616 NE 4th Avenue  
Camas, WA 98607

**Self-Evaluation Plan (SEP) and ADA Transition Plan Update**

THIS AMENDMENT (“Amendment”) to Professional Services Agreement is made as of the day of \_\_\_\_\_, 2026, by and between the **City of Camas**, a municipal corporation, hereinafter referred to as "the City", and **PBS Engineering and Environmental LLC** hereinafter referred to as the "Consultant", in consideration of the mutual benefits, terms, and conditions hereinafter specified. The City and Consultant may hereinafter be referred to collectively as the “Parties.”

The Parties entered into an Original Agreement dated March 4, 2025, by which Consultant provides professional services in support of the Project identified above. Except as amended herein, the Original Agreement shall remain in full force and effect.

1. **Scope of Services.** Consultant agrees to perform additional services as identified on **Exhibit “A”** (Amended Scope of Services) attached hereto, including the provision of all labor, materials, equipment, supplies and expenses, for an amount not-to-exceed \$13,868.00.

a.  Unchanged from Original/Previous Contract

2. **Time for Performance.** Consultant shall perform all services and provide all work product required pursuant to this Amendment by:

a.  Extended to \_\_\_\_\_.

b.  Unchanged from Original/Previous Contract date of June 30, 2026.

Unless an additional extension of such time is granted in writing by the City, or the Agreement is terminated by the City in accordance with Section 18 of the Original Agreement.

3. **Payment.** Based on the Scope of Services and assumptions noted in **Exhibit “A”**, Consultant proposes to be compensated on a time and material basis per **Exhibit “B”** (Costs for Scope of Services) with a total estimated not to exceed fee of:

a. Previous not to exceed fee: \$105,797.00

b. Amendment No. 2 \$13,868.00

c. **Total: \$119,665.00**

d. Consultant billing rates:

Modification to Consultant Billing Rates per **Exhibit “C”** attached herein

Unchanged from Original/Previous Contract

4. Counterparts. Each individual executing this Agreement on behalf of the City and Consultant represents and warrants that such individual is duly authorized to execute and deliver this Agreement. This Agreement may be executed in any number of counter-parts, which counterparts shall collectively constitute the entire Agreement.

DATED this day of \_\_\_\_\_, 2026.

CITY OF CAMAS:

PBS Engineering and Environmental LLC:  
*Authorized Representative*

By: \_\_\_\_\_

By: \_\_\_\_\_

Print Name: \_\_\_\_\_

Print Name: \_\_\_\_\_

Title: \_\_\_\_\_

Title: \_\_\_\_\_

Date: \_\_\_\_\_

**EXHIBIT "A"**  
**AMENDED SCOPE OF SERVICES**

Project Change Order 1

**Client:** City of Camas  
**Change Order Number:** 1  
**Project Name:** Self-Evaluation Plan (SEP) and ADA Transition Plan Update  
**Project Manager:** John Manix

**Date:** February 26, 2026  
**Date of Agreement:** February 19, 2025  
**Project Number:** 24011088  
**Project Location:** Camas, Washington

**Issue to Be Addressed:** The community expressed a high priority for improvements in downtown Camas. Of the 62 comments received by community members, 35 noted downtown locations as important locations for Americans with Disabilities Act (ADA) upgrades. Apex provided additional services in the Self-Evaluation Plan (SEP) and ADA Transition Plan regarding the downtown area.

Apex provided additional services to the City of Camas (City) preparing low-cost ADA improvements to upgrade the existing ramps. **This will substantially reduce costs and minimize disruption of existing streetscaping and pedestrian activities associated with construction of full ramp replacement.**

**Baseline Change Type:** The cost was increased and the schedule lengthed to address inventory data and additional services in the downtown area.

**Reason(s) for Change Selected:** In late November 2025, at a meeting at City Hall, I noticed that the ramps on 4th Avenue did not seem to be so out of compliance as to need to be fully replaced. I proposed low-cost upgrades as an alternative to full replacement of the ramps. Apex staff collected new inventory data at all downtown ramps, except for 3rd Avenue. The existing ramps were prioritized for some with low-cost upgrades and some with full replacement. All ramps were mapped with the new inventory data. A new scope of work for both low-cost and full replacement improvements was prepared, as was a cost estimate.

**Estimated Effect on Project:**

Scope of Work	Increased
Budgets	Increased
Schedule	Increased
Construction and O&M Costs	Not applicable

**Estimated Increase in Compensation and/or Costs Due to This Change: \$13,868**

Subtask 3.4: Additional downtown inventory	\$5,780
Subtask 4.2: Additional downtown prioritizing	\$3,300
Subtask 4.3: Additional mapping using GIS	\$2,130
Subtask 4.4: Additional scoping of ADA improvements	\$1,349
Subtask 4.5: Additional cost estimate	\$1,309

**Updated Scope of Work:** In the December 2025, we put in a lot of work into the downtown area and exhausted the project budget.


**Effect Upon Project if Change is Not Approved:** No change. The work was completed assuming it fit within the project budget, but we learned how much effort was put into the downtown area in December in the January billing.

This change order is subject to all the terms and conditions, including those related to compensation and payment, of the original agreement between the parties.

For agreement by the parties:

**Apex Companies, LLC**

**[Insert Name of Other Party]**

Signature: 

Signature:

Print Name: John Manix, PE

Print Name: James E Carothers, PE

Title: Senior Traffic Engineer

Title: Engineering Manager/City Engineer

Date: February 26, 2026

Date:

**EXHIBIT "B"**  
**AMENDED COSTS FOR SCOPE OF SERVICES**

See Exhibit "A"

PSA Ammendment 2 Detail Breakdown

Camas ADA Plan Update	PBS Engineering and Environmental Staff											Expense	TOTAL
	Senior Traffic Engineer VII	Engineer IV	Engineer	Engineer I	GIS Analyst (Eng 1)	Public Involvement Manger	Public Involvement III	Graphic Artist	Editor I	Administrator IV			
<b>Task 1: Project Management</b>													0.00
Subtask 1.1: Kick-Off Meeting and Team Coordination													0.00
Subtask 1.2: Progress Reports and Invoicing													0.00
Subtask 1.3: Administraion and Quality Control													0.00
<b>Task 2: Public Involvement (PI)</b>													0.00
Subtask 2.1: Public Involvement Plan and Summary													0.00
Subtask 2.2: Engage Camas Webpage & Virtual Open House													0.00
Subtask 2.3: Community Advisory Committee													0.00
Subtask 2.4: Communications and Promotion													0.00
Subtask 2.5: City Council Update													0.00
<b>Task 3: Update Self-Evaluation Plan</b>													5,780
Subtask 3.1: Review Existing SEP													0
Subtask 3.2: Review of Existing City Policies and Processes													0
Subtask 3.3: Review of Current Standard Plans													0
Subtask 3.4: Inventory of High Priority Locations	3			36									5,780
Subtask 3.5: Mapping Data Collected in the Field Inventory													0
Subtask 3.6: Update Self-Evaluation Report													0
<b>Task 4: ADA Plan Update</b>													8,088
Subtask 4.1: Review Existing ADA Plan													0
Subtask 4.2: Prioritize ADA Improvements to Curb Ramps, Traffic Signals, and Sidewalks	14												3,300
Subtask 4.3: Map Projects using GIS				15									2,130
Subtask 4.4: Update Scope of Work for ADA Improvements				10									1,349
Subtask 4.5: Update Planning Level Cost Estimates	2			6									1,309
Subtask 4.6: Update a Planning Level Schedule													0
Subtask 4.7: Prepare ADA Plan Update Documents													0
<i>Reimbursable Expenses</i>													0
<i>Copies</i>													0
<i>Expenses</i>													0
<i>Travel</i>													0
<b>TOTAL HOURS</b>	19	0	0	66	0	0	0	0	0	0	0		
<b>MAX HOURLY RATES</b>	235	195	185	142	142	170	135	120	120	135			
<b>TOTAL DOLLARS</b>	\$ 4,474.64	\$ -	\$ -	\$ 9,393.30	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ -	\$ 13,868



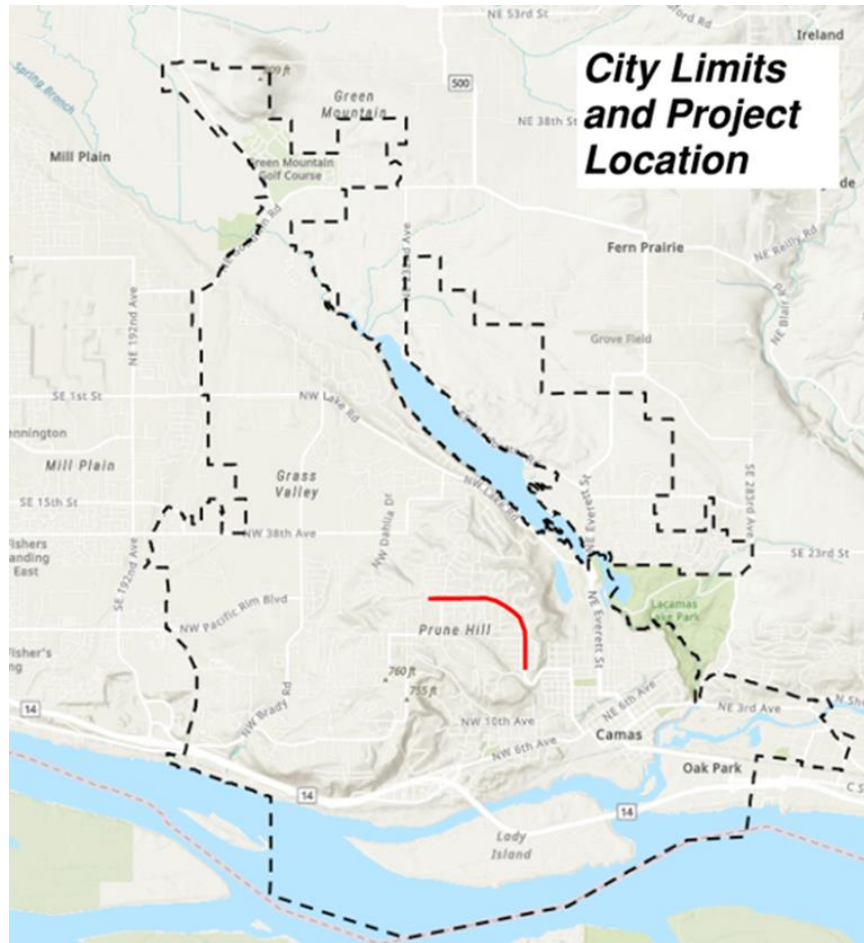
# Staff Report

April 6, 2026 Council Workshop Meeting

NW 28<sup>th</sup> Avenue and NW Fargo Street Pavement Preservation and Complete Street Reconfiguration (Submitted by James Carothers, Engineering Manager)

Phone	Email
360.817.7230	jcarothers@cityofcamas.us

**BACKGROUND:** The City’s pavement management program has identified NW 28<sup>th</sup> Avenue and NW Fargo Street for 2026 pavement preservation program. The existing asphalt surfacing has reached the end of its useful life, requiring a grind and inlay treatment to restore the travel lanes. The project limits extend from NW Utah Street to NW Kent Street along NW 28th Avenue, and from NW Kent Street to NW 18th Avenue along NW Fargo Street.



*Project Limits NW 28th Ave and NW Fargo St*

**SUMMARY:** In alignment with the City’s Complete Streets Ordinance No. 24-005, CMC 12.44, staff evaluated this segment for potential safety and mobility enhancements. This project presents an opportunity to advance the Strategic Plan goals and Active Transportation System goals by reconfiguring the street striping to provide Bicycle Lanes. By integrating these "Complete Street" elements into the scheduled maintenance, the city achieves significant cost efficiencies, utilizing the planned preservation work to implement safety improvements that would otherwise require a separate, more expensive capital project.

Staff have prepared a presentation to inform council and community on these changes and how they will impact all users.

**BENEFITS TO THE COMMUNITY:** Firstly, this project will preserve the street by avoiding costlier repairs in the future. Secondly, this item will incorporate Complete Streets policy better serving all transportation users mobility while enhancing safety.

**STRATEGIC PLAN:** This item is aligned with strategic plan priority for a safe and accessible community by expanding accessibility and improving public safety.

**POTENTIAL CHALLENGES:** This item will require reduction in street parking which can impact residents on corridor. Attached are comments from stakeholders regarding the proposed addition of bike lanes.

**BUDGET IMPACT:** Pavement Preservation program has an available fund balance of \$1,628,000 for 2026 preservation projects. This year’s program will include Hot Mix Asphalt (HMA) repairs, slurry seals, and crack sealing contracts.

**Preservation Breakdown**

NW Fargo/28 <sup>th</sup> HMA Replace	\$713,000
NW Fargo/28 <sup>th</sup> Striping and Signage	\$73,000
<b>NW Fargo and 28<sup>th</sup> Total</b>	<b>\$786,000</b>

There will be approximately \$842,000 reserved for Slurry Seals, Crack seal, and HMA replacement. The City plans to perform maintenance on NW 38<sup>th</sup> Ave from NW Dahlia Drive to NW 43<sup>rd</sup> Avenue as our second asphalt contract dependent on bid prices and available budget.

Cost efficiency: The Camas Transportation System Plan’s draft cost to add bike lanes to NW Fargo/28<sup>th</sup> Avenue is estimated at \$1,290,000. Staff’s current engineer’s estimate calculates to a savings of approximately \$504,000 by completing this work simultaneously with pavement preservation providing a 40% cost reduction on project delivery.

**RECOMMENDATION:** Staff is seeking consensus from Council to add bike lanes and incorporate complete street elements.

---

# NW 28<sup>th</sup> Ave and NW Fargo from NW Utah to NW 18<sup>th</sup> AVE

---

TRAVEL LANE RECONFIGURATION FOR COMPLETE  
STREET

COUNCIL WORKSHOP

APRIL 6, 2026

# Introduction : Why this project matters?

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- Pavement preservation will be re-surfacing travel lanes
- Timely preservation avoids more costly repairs
- Provides opportunity to align roadway configuration with Complete Street Ordinance, Transportation System Plan, and Strategic plan goals.
- Opportunity to improve safety for all users, all modes of travel, at a reduced cost to public

# Project Location

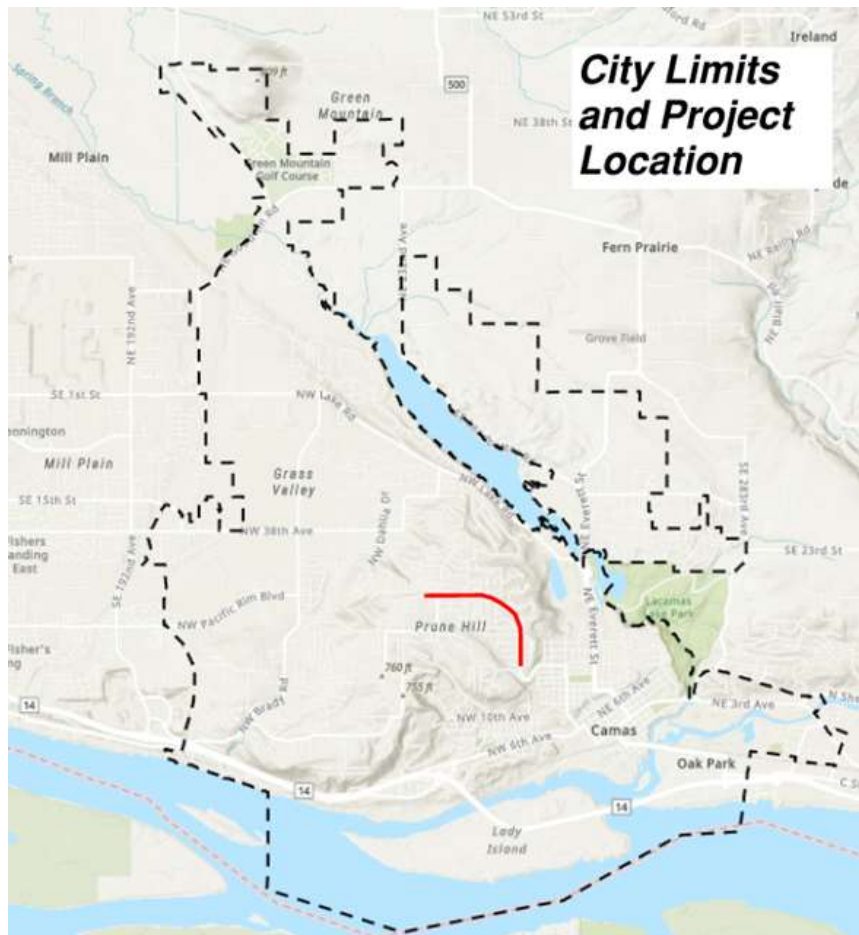
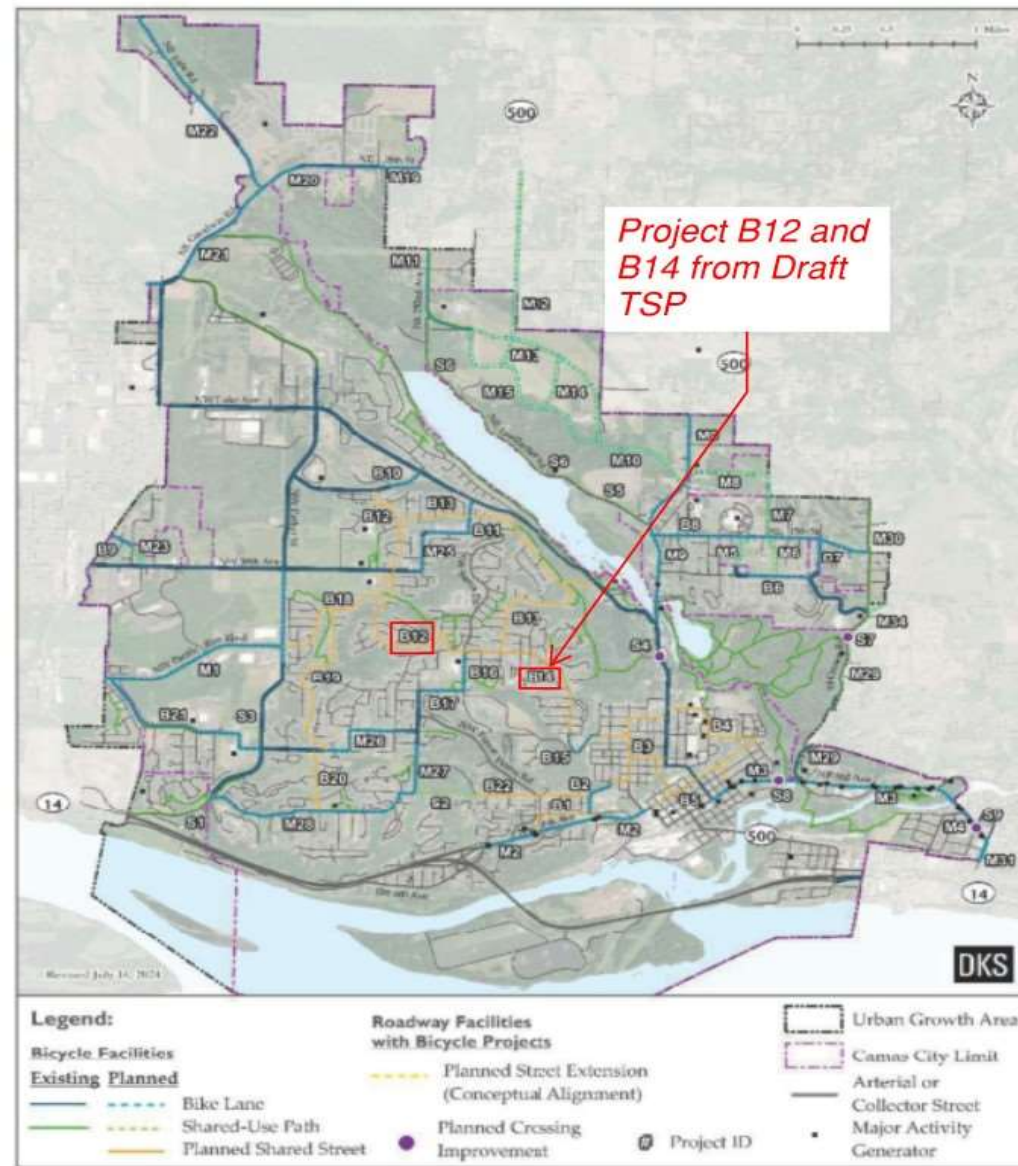


FIGURE 33: PLANNED BICYCLE PROJECTS



# Existing Conditions

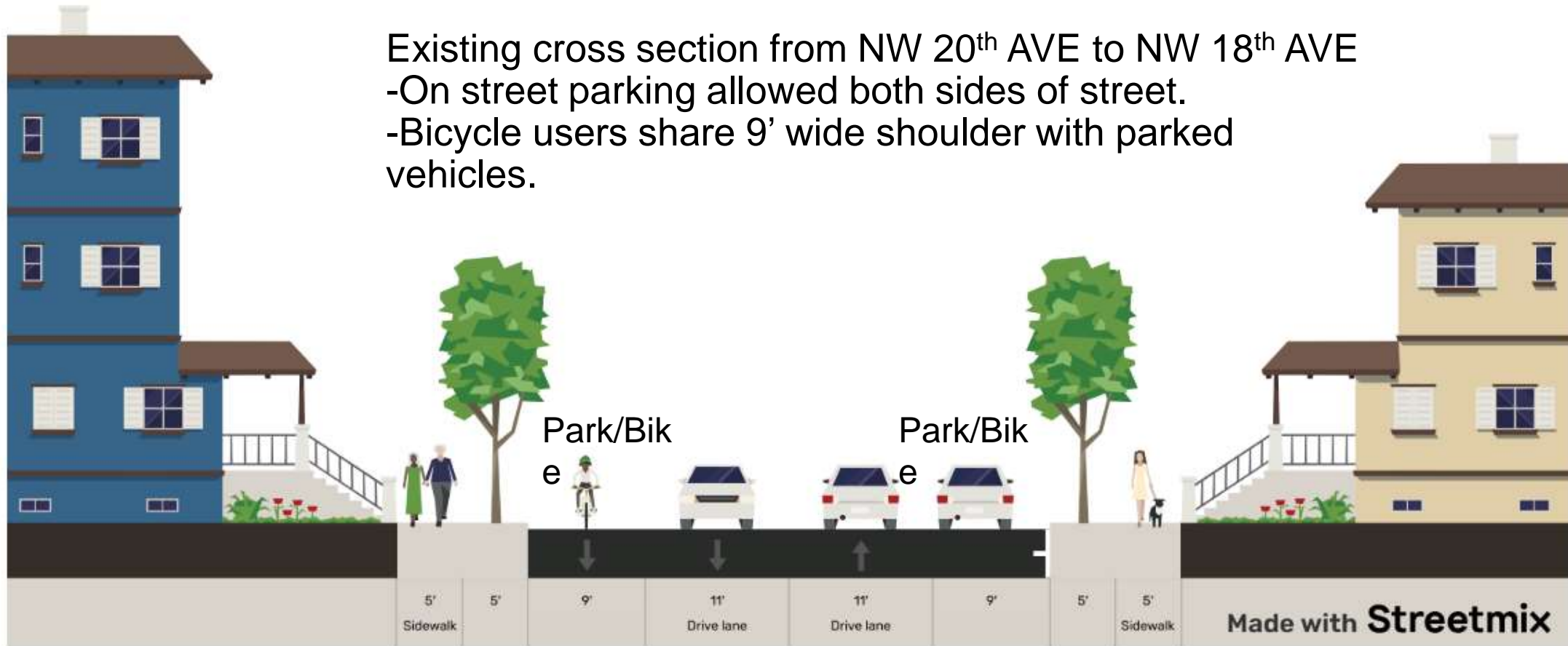
Existing cross section from NW Utah East to NW 20<sup>th</sup> AVE.

- On street parking allowed both sides of street
- Bicycle users share travel lane with vehicles






# Existing Conditions

Existing cross section from NW 20<sup>th</sup> AVE to NW 18<sup>th</sup> AVE  
-On street parking allowed both sides of street.  
-Bicycle users share 9' wide shoulder with parked vehicles.



# Proposed Changes

## NW 28<sup>th</sup> Ave and NW Fargo from NW Utah to NW 28<sup>th</sup> Ave

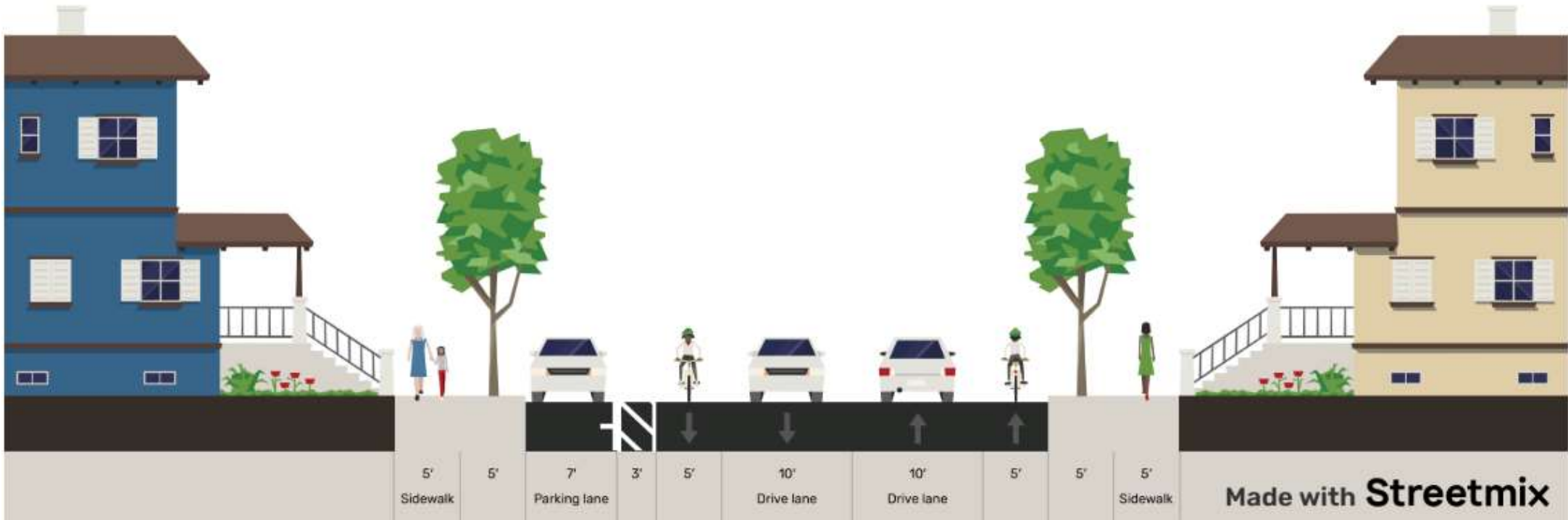
-  **Section 1: Traditional bike lanes with parking along south curb line only**
-  **Section 2: Separated bike lane eastbound, Traditional bike lane westbound with parking along south curb line only**
-  **Section 3: Separated bike lane northbound and southbound**



# Complete Street Proposed Change

Section 1: NW Utah to NW Sierra and NW Kent to NW 23<sup>rd</sup> AVE

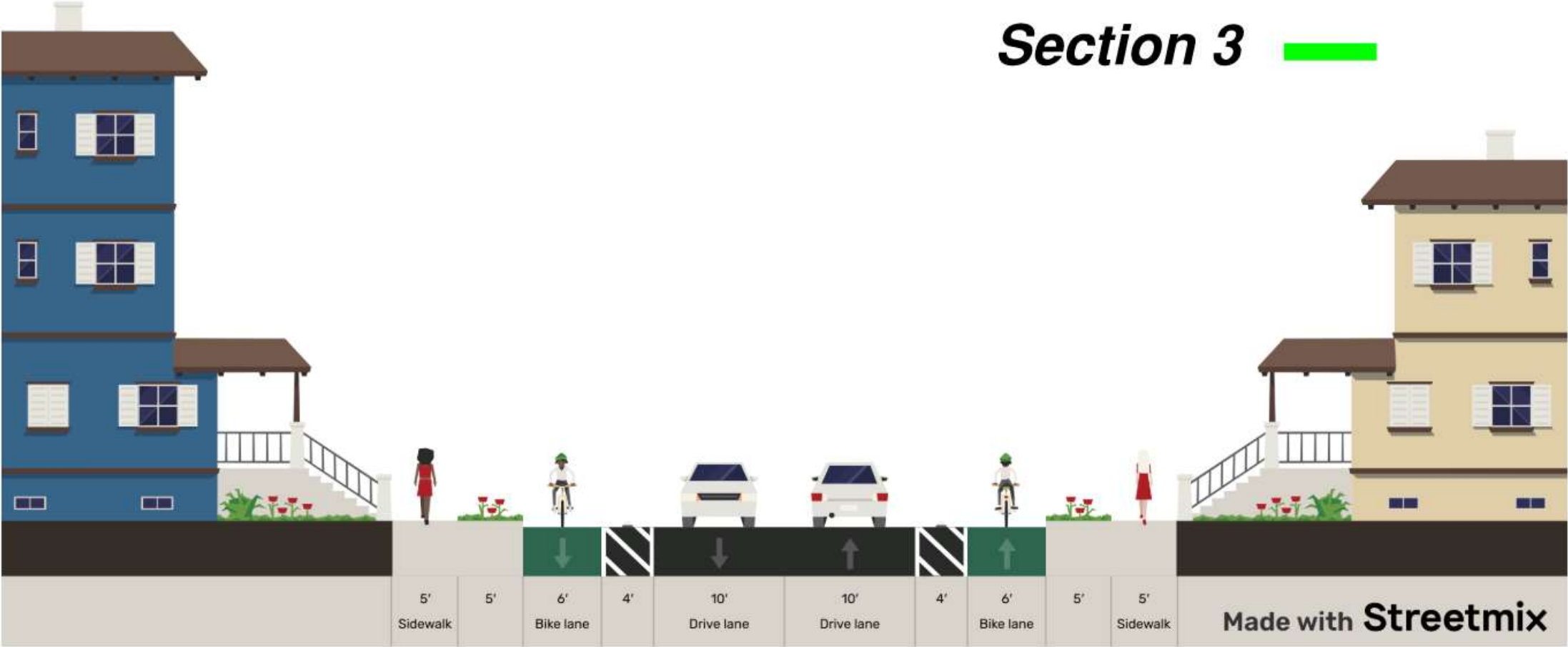
**Section 1**   
**Section 2** 



# Complete Street Proposed Change

## Section 3: NW 23<sup>rd</sup> Ave to NW 18<sup>th</sup> Ave

**Section 3** █



# Maintain Existing Conditions Option

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- No changes and retains parking on both sides of street
- No safety improvement for bicycle facilities or traffic calming
- Not aligned with City Complete Streets Ordinance, Transportation Masterplan, and Strategic Plan goals
- Missed opportunity for cost savings

# Add Bike Lanes Option

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- Aligned with Complete Streets Ordinance and Strategic Plan goals
- Improved safety for all users with traffic calming benefits
- Cost savings implementing with preservation project
- More predictable roadway use
- Impacts to on-street parking
  - no parking from 18<sup>th</sup> St to 23<sup>rd</sup> Ave
  - parking reduced NW 23<sup>rd</sup> Ave to NW Utah St

# Cost Efficiency

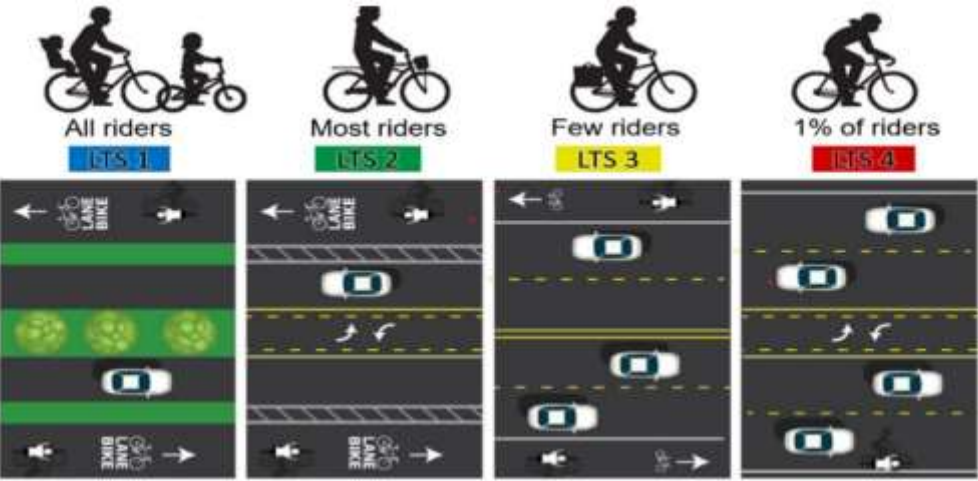
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- Striping changes removal and replacement will occur with pavement preservation
- Minimize disruption to residents while avoiding future standalone project costs
- Estimated Savings: \$500,000

# Level of Traffic Stress (LTS)

Level of Traffic Stress	Description
1	Suitable for all ages and abilities; children could walk or bike here independently. Separated and/or barrier-protected.
2	Comfortable for most adults, including most adults experiencing disabilities. Some separation, no barrier.
3	Tolerable for enthusiastic and/or confident adults. Little space, no separation.
4	Only used by highly confident people, or those with no alternative. No dedicated space, no separation

- LTS measures the stress someone walking, rolling, or biking on a road feels (comfort).
- Considerations include traffic speed and volume as well as space, separation, and visibility.
- Staff recommended standard LTS 2 or lower proposed in draft Transportation System Plan.



# Bicycle Stress

Traffic volume is 3,500 Veh/Day

Posted Speed 25 MPH

85th percentile speed is 28 MPH-33MPH

Travel lanes will be narrowed

Target speed is less than 30 MPH

Conventional Bike Lanes (5' or greater)								
Lane Configuration	AADT (total)	Target Speed						
		≤20	25	30	35	40	45	50+
1 thru lane per direction (or 1 lane one-way street)	0-750	1	1	2	3	4	4	4
	751-1500	1	1	2	3	4	4	4
	1501-3000	1	1	2	3	4	4	4
	3000+	2	2	2	3	4	4	4
2 thru lanes per direction	0-6000	2	2	2	3	4	4	4
	>6000	2	2	3	3	4	4	4
3+ thru lanes per direction	Any ADT	3	3	3	4	4	4	4

# Staff Recommendation

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- Add Bike Lanes to NW Fargo Street and NW 28<sup>th</sup> Avenue
- Implement striping reconfiguration with pavement preservation
- Advance safety and policy goals

## Questions?

## EMAIL COMMENTS

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**Sent:** Wednesday, March 25, 2026 10:52 AM

I support the proposed changes and hope they will move forward. As a walker, I find this stretch of road dangerous because of a lack of visibility especially when cars are parked on the road. There is one crosswalk in this area and only one side of the road has complete sidewalks (so to walk on a sidewalk, I have to cross!). The proposed changes would improve visibility and make the area much safer. As a driver, I'd like to see bikers have more space and a dedicated lane.

Thanks! Joan

---

**Sent:** Wednesday, March 25, 2026 2:04 PM

**Subject:** 2026 Pavement Preservation and Lane Restriping - NW 28th Ave and NW Fargo St from NW Utah St to NW 18th Ave

Hello !

I walk this stretch of road almost daily. Currently I have to scamper across the road a couple of times because the sidewalk disappears. I strongly support the proposed changes, making it a bit safer to walk along the street.

David

---

**Sent:** Saturday, March 28, 2026 4:17 PM

I've been living in Camas for 4 years, & I love it.

Thank you for taking the repaving opportunity to upgrade the safety of those on bike and on foot on this roadway.

Generally, we're better off with less cars & more bikes!

I suggest that on the road leading straight to Dorothy Fox Elementary, a higher standard of physical separation and protection should exist in order that parents can feel comfortable letting their small children walk or bike to school.

In fact, wherever possible, a physical barrier between cars & bikes is preferable.

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COMPILED FACEBOOK COMMENTS

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Check with Bryan R if I should include and if first name is ok...

**Comment 1:**

Seems like this will have a big impact to Dorothy Fox Elementary. Might want to include camas school district and Dorothy Fox elementary in your discussions.

User reply: this was my first thought. Whoever thought of this idea either doesn't have kids attending Dorothy or has never been to an event at the school.

**Comment 2:**

Go to 34th in Vancouver and this is what it will look like. Odd, never have seen a bicycle on the road. The residents are losing parking and will have to park around the corner on the side streets, same for their guests and any service vehicles. Maybe think about paving the roads that have a ton of potholes. Fix Forest Home so that people can walk safely instead of having to walk in the ditch and weeds.

User reply: I love the idea of fixing Forest Home as much as anyone, but that feels like a whole different \$\$\$ and engineering ballgame.

**Comment 3:**

Everyone complaining about lost car travel lanes and lost parking spots needs to drive this stretch of road.

First, there are only two car travel lanes currently, so it's not a 4-down-to-2 situation and the analogies to Washougal and Vancouver don't apply.

Second, there are already bike lanes (in actuality or in practice) for stretches here.

Third, you don't need car parking lanes on both stretches of this road because there are very/relatively few houses that face onto the street, especially when you consider both sides. For example, from the NW 18th end of the project to the Shell station (a stretch of almost 3/4 of a mile), there are only five residences on the east/north sides of the road

NW Fargo and 28<sup>th</sup> from NW Utah to NW 18<sup>th</sup>

Lane Reconfiguration

Public comments

with a driveway onto the street. Every other house faces away into a subdivision and is (or should be) parking in it.

It's similar but still manageable on the west/south side of the road (14 houses). It's hard to believe that people in a total of 19 houses, already with a driveway, can't each find a place to park an additional car (should they need to, occasionally) in nearly 4,000 feet of parking space.

**Comment 4:** People will use bike lanes a lot more when they actually connect and go places instead of dead ending. You don't see car traffic on dead end streets either, do you? In any event, every single house on that street has parking for multiple cars. There is a ridiculous amount of parking. We can put the streets to better use than for free car storage.

**Comment 5:** Is "pavement preservation" code for chip seal? If so, please advise bikes and motorcycles to avoid the area for several weeks.

**Comment 6:** Is "pavement preservation" code for chip seal? If so, please advise bikes and motorcycles to avoid the area for several weeks.

**Comment 7:** I am very skeptical of this project but will wait to hear more. So many of the duplexes/apartments park on this street, as well as the school traffic at pick-up, that it's hard to maneuver around now. I agree that school buses should be a high safety concern but the crossing guards have their hands full already and I can't understand how this plan will do anything other than exasperate the situation. Parents will not stop picking up their kids just because of bike lanes, it will just cram everyone in like sardines.

**Comment 8:** This is definitely a higher priority than fixing the road surface on the slough bridge.

**Comment 9:** Go figure .. reduce the number of parking space in hopes people ride bus mass transit

**Comment 10:** I understand the need for updates, but we need to consider the impact of reducing driving lanes on traffic flow and safety, especially during peak hours. Quit reducing our driving lanes seems like a step back. What's the plan to mitigate congestion?

**Comment 11:** Losing parking options for residence is never a good idea. Why make things harder for people?

**Comment 12:** If it's to better serve all users then leave the parking alone.

**Comment 13:** Good grief 🙄 who comes up with these ridiculous designs!?

**Comment 14:** There already isn't enough parking and now you plan on taking away the little that there is.. that would send bookish into the ground.. along with more illegal parking that already takes place in the summertime from lack of parking..

- User Reply: this project is nowhere near Bookish or the lake.

**Comment 15:** The stretch is really only used for parking during the school year when school is starting or getting out. That said ,we live on Utah St. just north of the intersection and the school. I have issues getting into and out of our driveway at times during those times already and the line of sight to the north, coming up from Valley towards NW 28th, is already limited and more cars probably parking on NW Utah to pick up kids, particularly when raining, even for a short period of time will decrease the line of sight from both northbound and southbound cars causing a heightened safety issue. I am wondering if the City had a licensed traffic engineer take those factors into consideration? As far as I am concerned the City is now on notice of a potential increase in safety issues from the proposed changes to NW 28th.

**Comment 16:** They want bike lanes. That's the whole purpose of these "updates". Do away with street parking so bike lanes, that will RARELY get used, are put in their place! Look what Washougal did to E st. 4 lanes to 2 and put bike lanes in that are RARELY used! And traffic still backs way up! It's a real joke!

- User Reply: agreed! It's been a nightmare in Washougal. It was so much more functional as 4 lanes. Back up at the lights was an instant issue.

4/1/26

Hello,

Plans for the 2026 Pavement Preservation and Lane Restriping – NW 28<sup>th</sup> Ave and NW Fargo St. from NW Utah St. to NW 18<sup>th</sup> Ave. appear to follow the City's "Complete Street" policy and federal and state guidelines. While the concepts are notable and more closely align with new development designs, they do not align with existing physical street layouts and, more importantly, common sense.

Complete Streets policy advocates ignore cost-benefit analysis, as the guidelines for action assume that someone, someday, will benefit from changing sidewalks and roadway structures, even though bicycle lanes and sidewalks are usually empty or used by very few people.

Camas is not an urban area with close-by shopping, employment, and often schools. Motorized vehicles, primarily automobiles and trucks, will be used by 90+% of people for their transportation needs. Others will use bicycles, scooters, and motorized wheelchairs to a much lower percentage.

Bicycles, cars, trucks, and people have coexisted on sidewalks and streets for over a century. Accident rates are very low. In 2023, WSDOT reported 154 pedestrians died, and 474 were seriously injured in traffic crashes. In 2023, WSDOT reported 18 Bicyclists died, and 157 bicyclists were seriously injured in traffic crashes. 2024 preliminary data from Washington's Traffic Safety Commission, reported by the Washington State Standard, showed 168 pedestrians or bicyclists were killed.

No doubt, anyone would wish these numbers were lower, or even zero. The reality is that people don't live in cocoons; they're active and take risks. Even walking on a sidewalk poses a risk of stepping off a curb, tripping on a crack, or just stumbling.

As tragic as accidental deaths and injuries are, is attempting to change all or nearly all streets in the state justified or even practical? What has been the City of Camas' experience with the number of bicyclists and pedestrian deaths? How have costs been analyzed?

The current plan presented on the 3 slides published on the website is questionable and more likely to create a false sense of security. Roadway striping also includes evaluating accessibility features. This proposed project will likely result in changes similar to those the City of Vancouver made to 34th Street between 192nd and 164th streets and on Tec Center, where accessibility features have turned the two streets into eyesores and likely hazards for drivers and into delays for emergency vehicles. The City of Vancouver example is intended as a question to City staff, asking them to detail exactly what alternative accessibility features they are evaluating during the workshop.

In my humble opinion, after reading information at the federal and state levels, the complete streets program is a mandate with very little leeway. The best approach for the City of Camas is to do only what is necessary to comply minimally with regulations. The restriping project should not include any additional striping other than replacing existing striping.

Future road improvement projects may include extending bike lanes and their markings. Projects could add sidewalks to existing streets where property setbacks allow. New construction projects could have wider sidewalks and potentially place sidewalks and bike lanes adjacent to one another, with parking strips separating the roadway from the sidewalk and bike lanes. The result of realigning new construction projects would leave the roadway for autos, buses, and trucks, with sidewalks and bike lanes for bikes, scooters, pedestrians, and motorized wheelchairs.

Respectively submitted,

Mark E. Swenson



# Staff Report

April 6, 2026 Council Workshop Meeting

NW Oregon Street Vacation Petition  
Presenter: James Carothers, Engineering Manager  
Time Estimate: 5 minutes

Phone	Email
360.817.7230	jcarothers@cityofcamas.us

**BACKGROUND:** Staff has received a right-of-way vacation petition from the abutting owner of the unimproved portion of NW Oregon Street north of NW 6<sup>th</sup> Place. There is one property owner for the entire right-of-way frontage for the subject right-of-way.

**SUMMARY:** Staff has reviewed this petition and analyzed the subject right-of-way. The NW Oregon Street roadway is a dead end to the north due to the steep terrain on the subject property that prohibits a practical connection to NW 6<sup>th</sup> Place. There are no utilities on site. Staff recommends the following schedule of events for this vacation process:

- **April 6, 2026 – Introduce Vacation Petition to Council**
- April 20, 2026 – Resolution to Set the Public Hearing Date
- May 18, 2026 – Public Hearing
- June 1, 2026 – Vacation Ordinance (Tentative based on Council direction)

**BENEFITS TO THE COMMUNITY:** This right-of-way area might be better used for private residential development.

**BUDGET IMPACT:** There is no budget impact. If vacated, the petitioner could be charged up to fair market value for the property.

**RECOMMENDATION:** Staff recommends a Resolution to set a Public Hearing date for May 18, 2026 be placed on the April 20, 2026 Council Regular Meeting Agenda for Council’s consideration.

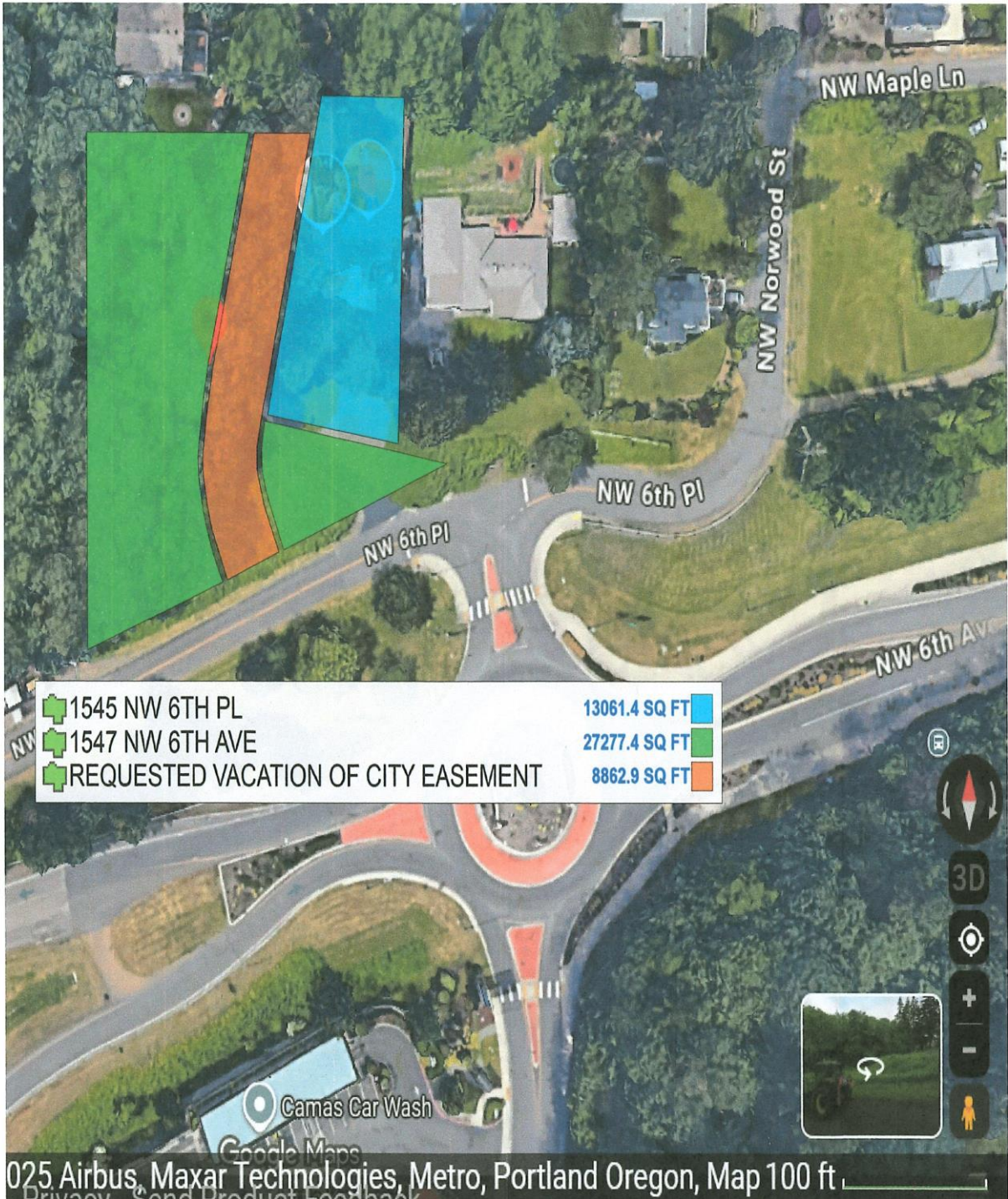


Figure 1: Vacation Area Aerial Map

**EXHIBIT "A"**

**PETITION FOR VACATION OF CITY RIGHT-OF-WAY AT: NW OREGON STREET, CAMAS WA 98607**

TO: City Council, City of Camas, State of Washington

We, the undersigned taxpayers and landowners within the City of Camas, Washington, present this Petition and request that the City right-of-way known as: **NW OREGON STREET** be vacated from the point commencing at **NW 6TH PLACE** and ending at **NORTH PROPERTY LINE OF TAX PARCEL # 84179000**. The area of the land requested to be vacated consists of approximately **8,860** square feet.

The Petition for Vacation of right-of-way/alley/street (circle one) based on the following:

- The land is no longer used or has not been used for some time by the public or the city.
- The maintenance of this property is a waste of city funds.
- The land would be better used under private ownership.



The names and addresses of the abutting property owners whose realty abuts the City property are:

<u>Name</u>	<u>Address</u>
<u>STEPHEN M SAFRAN AND AENOY SAFRAN</u>	<u>1545 NW 6TH PLACE, CAMAS WA 98607</u>
<u>STEPHEN M SAFRAN AND AENOY SAFRAN</u>	<u>1547 NW 6TH AVENUE, CAMAS WA 98607</u>
_____	_____
_____	_____

The effect of the vacation of this property to the owners thereof will be:

STEPHEN M SAFRAN AND AENOY SAFRAN

Respectfully submitted,

<u>Name</u>	<u>Address</u>	<u>Date</u>
 <u>STEPHEN M SAFRAN</u>	<u>1547 NW 6TH AVENUE</u> <u>CAMAS, WA 98607</u>	<u>11/10/2025</u>
 <u>AENOY SAFRAN</u>	_____	_____
_____	_____	_____

Attachment: Assessor's map of the requested City property and legal description.

**Figure 2: Signed Petition**

## EXHIBIT "A"

### ROAD VACATION LEGAL DESCRIPTION OF A PORTION OF NW OREGON STREET

Dated March 16, 2026

**A strip of land being a portion of NW O Street, now known as NW Oregon Street, being a 40-foot road fronting on a portion of Lot 11 and a portion of Lot 6, Bennett's Addition, recorded in Book D, Page 97, located in the Southwest quarter of Section 10, Township 1 North, Range 3 East of the Willamette Meridian, City of Camas, County of Clark, State of Washington, being more particularly described as follows:**

Commencing at the Northeast corner of Lot 11, Bennett's Addition; thence North  $87^{\circ}27'46''$  West, along the North line of said Lot 11, a distance of 99.12 feet, more or less, to a point on the East Right-of-Way of NW Oregon Street; thence South  $14^{\circ}38'43''$  West, along the East right-of-way of said NW Oregon Street, a distance of 29.08 feet, and the Point of Beginning of this description; thence continuing Southerly along the East right-of-way of said NW Oregon St. the following three courses;

- 1) thence South  $14^{\circ}38'43''$  West, 130.57 feet to the beginning of a tangent curve concaved Southwest, having a radius of 80.00 feet;
- 2) thence Southerly along said curve, an arc length of 51.61 feet, a central angle of  $36^{\circ}57'38''$ , a chord which bears South  $03^{\circ}49'58''$  East, a chord length of 50.72 feet;
- 3) thence South  $22^{\circ}18'47''$  East, 14.11 feet to a point on the Northerly right-of-way of NW 6<sup>th</sup> Avenue, also being (Evergreen Highway);

Thence South  $69^{\circ}09'10''$  West, along the Northerly right-of-way of NW 6<sup>th</sup> Avenue a distance of 40.01 feet to a point on the West right-of-way of NW Oregon Street;

thence Northerly along the West right-of-way of said NW Oregon Street, the following three courses;

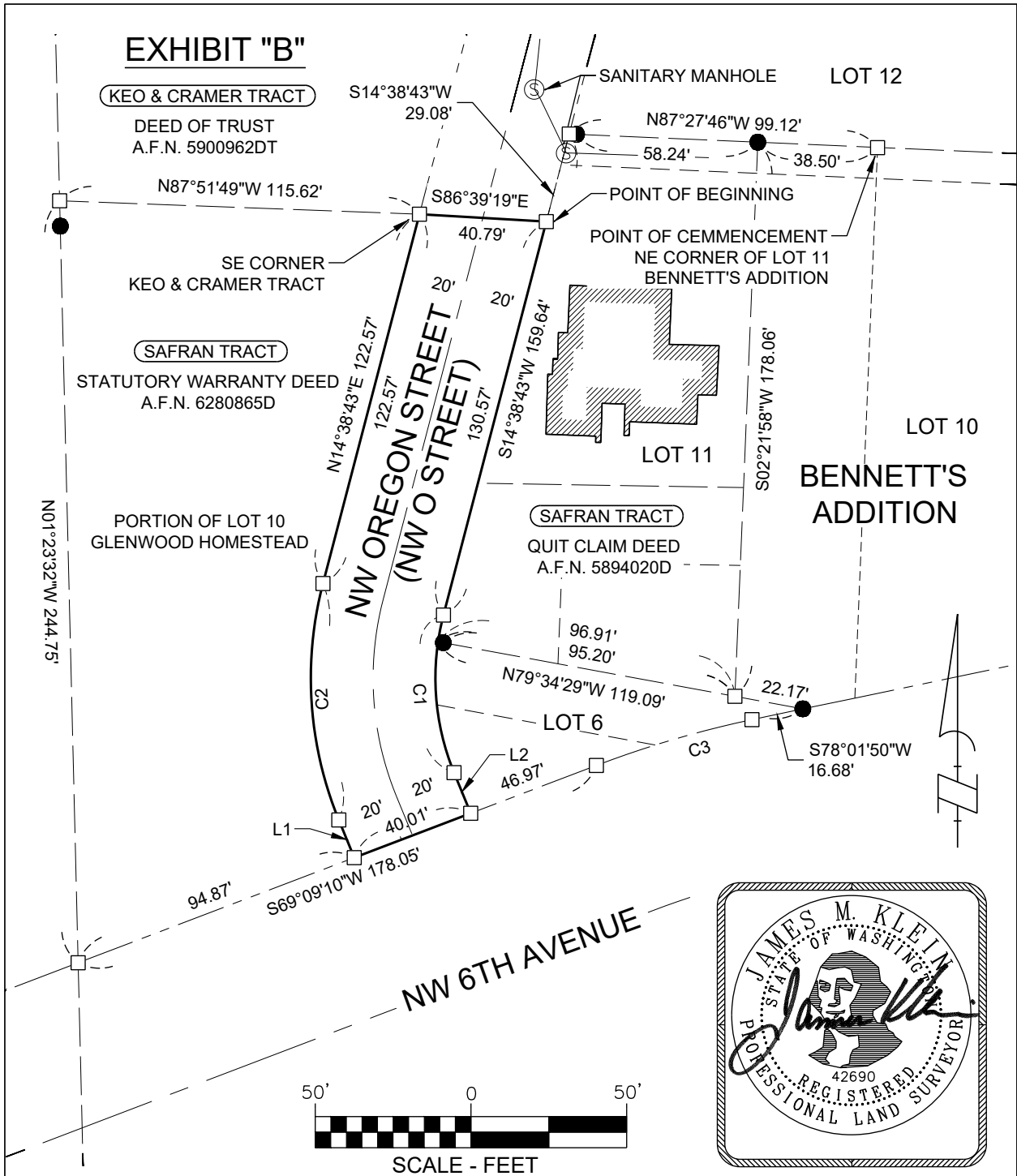
1) thence North  $22^{\circ}18'47''$  West, 13.09 feet to the beginning of a tangent curve concaved southwest, having a radius of 120.00 feet;

2) thence Northerly along said curve, an arc length of 77.41 feet, a central angle of  $36^{\circ}57'38''$ , a chord which bears North  $03^{\circ}49'58''$  West, a chord length of 76.07 feet;

3) thence North  $14^{\circ}38'43''$  East, 122.57 feet to the Southeast corner of that certain tract of land convey in Deed of Trust, recorded April 27, 2021, Auditor's File No. 5900962D, Clark County records;

thence South  $86^{\circ}39'19''$  East, 40.79 feet, back to the Point of Beginning.

Containing 8,192 square feet, more or less.



**BASIS OF BEARINGS**

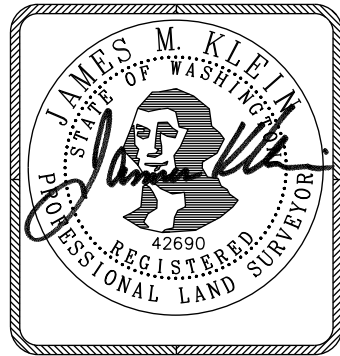
WASHINGTON STATE PLANE  
 COORDINATES SYSTEM,  
 NAD83(2011) EPOCH 2010.00,  
 NORTH ZONE, GRID NORTH,  
 GROUND DISTANCE

RIGHT OF WAY DEDICATION  
 LOCATED THE NE 1/4 OF THE SW 1/4 OF  
 SECTION 10, T. 1N., R. 3E., W.M. CITY OF CAMAS, CLARK COUNTY, WA



**Klein & Associates, Inc.**  
 ENGINEERING • SURVEYING • PLANNING  
 1411 13th Street • Hood River, OR 97031  
 TEL: 541-386-3322 • FAX: 541-386-2515

PROJECT: 260204  
 FILE No: 260204  
 FILE PATH  
 LAYOUT: KA8.5X11  
 SURVEYED: JK  
 DESIGN: JK  
 DRAFT: GD  
 APPROVE: JK  
 DATE: 3-10-26  
 SHEET 1 OF 1 SHEETS



### EXHIBIT "B"

LINE TABLE		
LINE #	DIRECTION	LENGTH
L1	N22°18'47"W	13.09'
L2	S22°18'47"E	14.11'

CURVE TABLE					
CURVE	LENGTH	RADIUS	DELTA	CHORD BEARING	CHORD LENGTH
C1	51.61'	80.00'	36°57'38"	S3°49'58"E	50.72'
C2	77.41'	120.00'	36°57'38"	N3°49'58"W	76.07'



RIGHT OF WAY DEDICATION  
 LOCATED THE NE 1/4 OF THE SW 1/4 OF  
 SECTION 10, T. 1N., R. 3E., W.M. CITY OF CAMAS, CLARK COUNTY, WA



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PROJECT: 260204  
 FILE No: 260204  
 FILE PATH  
 LAYOUT: KA8.5X11  
 SURVEYED: KA  
 DESIGN: JK  
 DRAFT: GD  
 APPROVE: JK  
 DATE: 3-10-26  
 SHEET 2 OF 2 SHEETS



# Staff Report

April 6, 2026 Council Workshop Meeting

Waterline Easement Vacation Request  
Presenter: James Carothers, Engineering Manager  
Time Estimate: 5 minutes

Phone	Email
360.817.7230	jcarothers@cityofcamas.us

**BACKGROUND:** Camas Woods 1 Subdivision (SUB 24-1002) applicant received preliminary plat approval on May 8, 2025. This subdivision is located adjacent north to Camas High School. In 1924, the City purchased a waterline easement across the subject project property for a nominal fee. A transmission main line was installed 100 years ago, and this pipe was abandoned and relocated off the subject property several decades ago. There are no plans to install any new utilities in this easement.

**SUMMARY:** The property owner and development applicant, HSR Capital, LLC, has formally requested the City’s vacation of the sections of easement across the Camas Woods 1 property. The total area of the easement is 37,541 square feet or 0.86 acre.

HSR hired an appraiser to provide valuation of the easement. The appraiser determined the fair market value of the easement to be \$40,000. In coordination with the development, HSR will be constructing and dedicating public infrastructure to the City with a value that significantly exceeds the appraised easement value. As such, HSR has requested that Council consider whether additional monetary compensation is necessary in connection with the easement vacation.

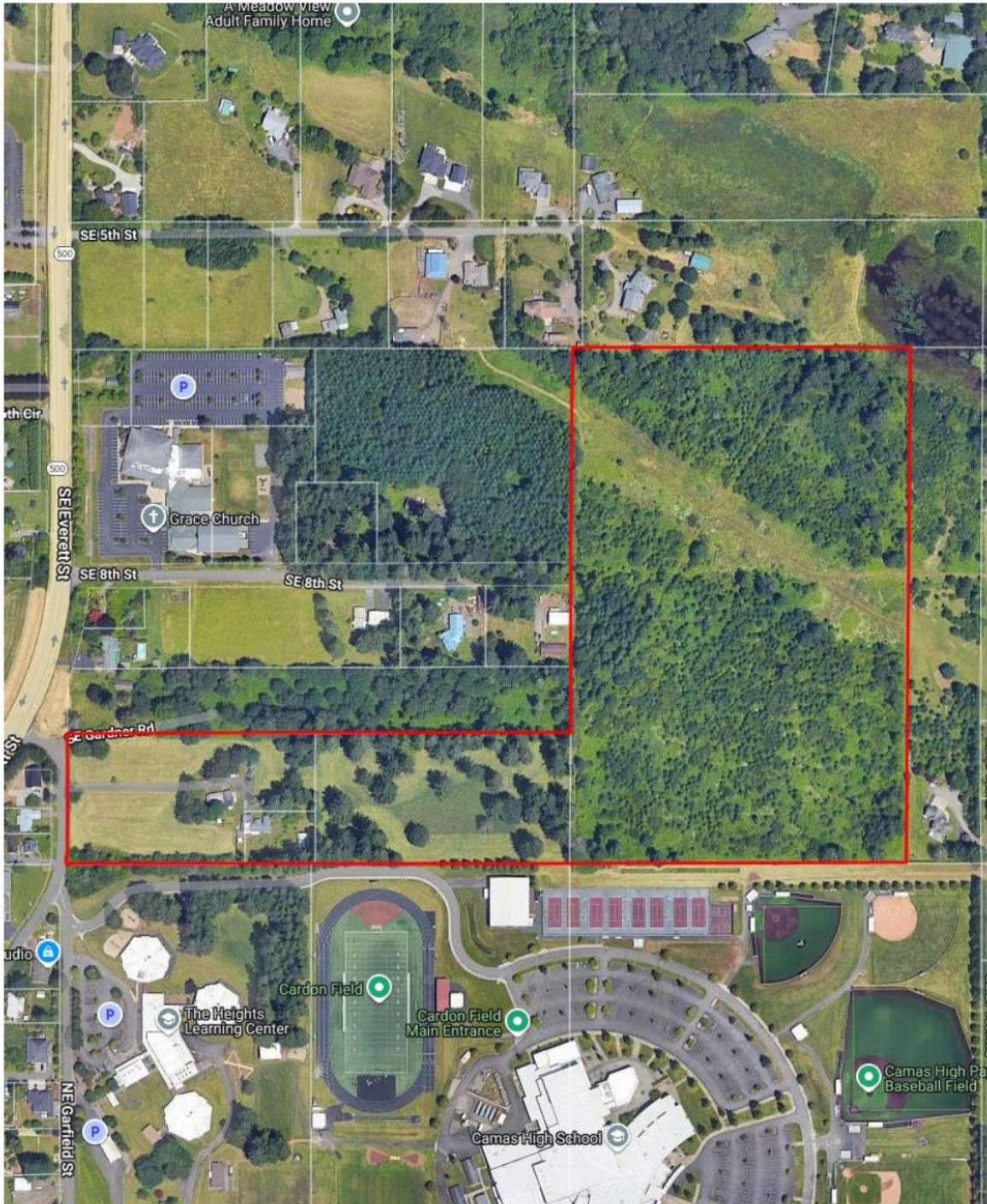
RCW 35.94.040 requires a public hearing for easement vacations. A resolution would need to follow the public hearing to allow the vacation, or release, of the easement to be recorded.

Staff is proposing the following schedule:

- **April 6, 2026 – Introduce at Council Workshop**
- May 4, 2026 – Hold Public Hearing
- May 18, 2026 – Adopt the Vacation Resolution

**BENEFITS TO THE COMMUNITY:** Development of this easement area would be a better use for the property.

# AERIAL MAP









WWW.AKS-ENG.COM

February 3, 2026

Jim (Curleigh) Carothers, PE  
City Engineer  
City of Camas  
616 NE 4th Ave, Camas, WA 98607

**RE: Waterline Easement Vacation (SUB24-1002, ENG-25-1042)**

Curleigh:

The purpose of this memorandum is to provide a narrative explanation pertaining to an easement vacation request as part of the Camas Woods development project (SUB24-1002) also associated with construction plan phase 1 (ENG-25-1042), located on Parcels 178108-000 and 178140-000.

The existing 25-foot-wide easement to the City of Camas (City) provides utility and maintenance access to an abandoned 10-inch steel water line originally serving the municipal headworks. The line is no longer in use and lies approximately three feet below grade. The property owner intends to construct over the easement area, as depicted in the approved land use documents, which necessitates formal vacation through City Council approval.

Per the original documents, (Book 158, Page 432 and Book 160, Page 381), recorded January 28<sup>th</sup>, 1924, the easement was granted with a total dollar value of \$1.00 from each respective parcel. As part of the standardized process, a valuation of the easement must be submitted to establish compensation for the City's relinquishment of rights. The Developer/Applicant/property owner has prepared a letter and commissioned an appraisal of the easement to satisfy the City's requirement. These documents are attached for review and consideration for presenting to City Council.

With construction of the Camas Woods project (Phase 1), the development will remove the steel pipe within the project extents and will cap the ends of the waterline.

Sincerely,

**AKS ENGINEERING & FORESTRY, LLC**

Bryce Hanson, PE

- Enclosures: HSR letter (dated 1/27/2026)  
Appraisal (dated 1/26/2026)  
Legal descriptions and exhibits (dated 2/2/2026)



Camas Woods, Waterline Vacation  
AKS Job #8397

2/3/2026  
Page 1 of 1



**REQUEST FOR CONSIDERATION OF NON-MONETARY OFFSET  
Camas Woods Phases 1 & 2 – Waterline Easement Vacation**

January 27, 2026

Dear Mayor and Members of the City Council,

On behalf of the Camas Woods development team, we respectfully submit the enclosed January 26, 2026 independent appraisal prepared by Appraisal & Consulting Group, LLC, estimating the fair market value of the City of Camas’ existing waterline easement interest at \$40,000. The appraisal is based on a lot-by-lot partial diminution analysis and assumes no loss of proposed lot count, no reduction in density, and continued buildability of all affected proposed lots.

In conjunction with the requested easement vacation, we, as the developer, are committed to constructing, at our sole cost, and dedicating substantial new public water system improvements as part of the Camas Woods subdivision. These improvements include removal of an obsolete City-owned steel waterline and installation of a new, looped public water system with an estimated combined value of approximately \$1.39 million. The proposed system will improve system redundancy and water pressure, enhance long-term reliability, and allow for future extension eastward, providing benefit to the broader City water service area.

While the appraisal appropriately concludes the fair market value of the easement interest in isolation, we respectfully request that the City Council consider the developer-funded water system improvements as non-monetary consideration that substantially exceeds the appraised value of the easement being vacated. In practical terms, the City will receive new, modern public infrastructure, eliminate future maintenance and replacement obligations associated with the obsolete line, and advance its long-term water system objectives — with no reimbursement, credit obligation, or future financial commitment by the City.

Considering the magnitude of these public benefits and their proportional relationship to the appraised easement value, we respectfully request that the City Council consider whether additional monetary compensation is necessary in connection with the easement vacation. We understand that this determination is a matter of Council discretion and is consistent with the City’s consideration of public benefit in prior utility and roadway easement vacation actions.

We appreciate the City’s thoughtful consideration of this request and look forward to continuing to work collaboratively with staff and Council toward an outcome that serves the best interests of both the City of Camas and the community.

Sincerely,

A handwritten signature in blue ink, appearing to read "Andy Swanson".

Camas Woods Development Team  
Andy Swanson  
President  
HSR Capital, LLC



Appraisal &  
Consulting Group, LLC

January 26, 2026

**HSR CAMAS WOODS P2, LLC**

Attn: Andy Swanson  
19120 SE 34<sup>th</sup> Street #103  
Vancouver, WA 98683

RE: City of Camas Waterline Easement Vacation  
Camas Woods Subdivision  
Camas, WA 98607

Dear Mr. Swanson:

Pursuant with our engagement, I have prepared an analysis of the value of the City of Camas's existing waterline easement across the Camas Woods Phases 1 & 2 property. The existing easement is not being used by the City and neither the easement area or the existing waterline infrastructure within the easement is planned for use as a part of the Camas Woods development. As a result, a proposed vacation of the existing waterline easement is proposed. This appraisal presents the value of the existing easement area and the resulting value of the property rights that would be relinquished by the City if the waterline easement is vacated. It is noted the primary analysis does not consider the value or benefits to the City of Camas from new waterlines and water system infrastructure that will be installed as a part of the Camas Woods development. These improvements will improve public water system to the area and allow the future orderly development of surrounding properties zoned for residential development as supported by market conditions. This appraisal report is intended to satisfy the scope of work and requirements agreed upon by the client and Appraisal & Consulting Group, LLC.

At the request of the client, this appraisal is presented in a Restricted Appraisal Report as defined by *USPAP* Standards Rule 2-2(b). Use of this report is limited to the client (HSR CAMAS WOODS P2, LLC) and the City of Camas for decision making regarding the potential vacation of the existing waterline easement. The rationale for how the appraiser arrived at the opinions and conclusions set forth in this report may not be understood properly without additional information that is retained in the appraiser's work file.

The subject property is zoned for a mix of residential densities. The majority of the subject is zoned HD-NS – North Shore Higher Density Residential and LD-NS North Shore Lower Density Residential. In addition, the northeast corner of Parcel 178140000 is zoned POS-NS, North Shore Park/Open Space and the west portion of Parcel 178159000 is zoned MX-NS, North Shore Mixed Use. The existing waterline easement does not encumber the Mixed Use zoned portion of the property with all but a small portion within the Higher Density and Lower Density residential zones.

13306 NW Cornell Road, Suite 201  
Portland, OR 97229  
503.740.8729 – Cell Phone  
971.277.6368 – Fax  
[www.acovaluation.com](http://www.acovaluation.com)

The Camas Woods Phases 1 & 2 property is proposed/approved for 206 single-family lots and two pads for future multi-family/mixed-use development. The 206 single-family lots include 88 townhomes, 80 small, detached lots, and 38 standard detached lots. Given the property type (residential development land), the market would determine the value of the easement area proposed for vacation based on its impact on development on the site including the number of potential lots as well as any impacts on the utility of planned lots. This, in effect, establishes the difference in value of the property “With” and “Without” the existing waterline easement. This is typically referred to as a Before and After analysis.

The proposed development was designed assuming the existing waterline easement will be vacated. Therefore, the appraiser has also relied on information from engineer Bryce Hanson, PE, LSIT, Certified Arborist with AKS Engineering & Forestry, LLC who considered alternative redesigns of some portions of the site if the easement is not vacated. The resulting impact on the number and utility of future residential lots establishes the value impact from the existing waterline easement and mirrors market behavior and the analysis a developer/buyer would consider in a purchase decision. The value impact is based on the current unimproved status of the property as vacant land approved for residential development.

While no extraordinary assumptions or hypothetical conditions are made in this appraisal, the appraiser has relied on exhibits and size estimates prepared by the property owner and their engineer in the preparation of this appraisal.

### **Appraisal Information**

**Valuation Date:** December 10, 2025

**Inspection Date:** The appraiser inspected the subject property with Steve Waugh, Chief Development Officer with HSR on December 10, 2025. This date is used as the valuation date for this appraisal assignment.

**Client:** HSR CAMAS WOODS P2, LLC

**Intended Use:** The intended use of this appraisal is to assist with decision making by the client and intended user regarding the potential vacation of an existing 25-foot wide waterline easement.

**Intended Users:** HSR CAMAS WOODS P2, LLC and City of Camas City Council and City Engineer.

**Purpose of Report:** To estimate the fair market value of the City of Camas’ existing waterline easement interest as a basis for determining just compensation in connection with a proposed easement vacation.

**Scope of Work:** The scope of work included a review of the proposed subdivision layout, engineering exhibits prepared by AKS Engineering, off-site cost estimates, and other relevant subject property information (zoning, utility availability, general market conditions). The appraiser then concludes the highest and best use of the property and, consistent with market behavior, uses residential and sales to conclude per proposed lot land values for the affected product types on the subject property (Attached Townhome Lots, Narrow Small Detached Lots, and Standard Detached Lots). The analysis evaluates the partial impact of the existing waterline easement as it traverses portions of twelve (12) proposed lots. All proposed lots remain buildable, with no loss of lot count or residential density (noting current proposed lots 16 & 17 would be relocated/switched with current Tract 502 to avoid a loss in development potential).

**Property Description**

**Subject Property:** The subject property consists of 36.12 acres of land zoned for a mix of residential development (primarily lower and higher density residential development with a small area of park/open space and mixed-use land). The Camas Woods Phases 1 & 2 property is proposed/approved for 206 single-family lots and two pads for future multi-family/mixed-use development. The 206 single-family lots include 88 townhomes, 80 small, detached lots, and 38 standard detached lots. The subject property consists of Parcels 178140000, 178108000, 178169000, and 178159000 as summarized in the table below.

SIZE	
PARCEL	ACRES
178140000	26.12
178108000	5.00
178169000	0.56
178159000	4.44
<b>TOTALS</b>	<b>36.12</b>

**Property Rights Valued:** Fee Simple subject to the City of Camas’s existing easement rights in the Before situation and fee simple without the City of Camas’s easement interest following the vacation of the easement in the After situation.

**Ownership & Sales History:** The subject property is under the ownership of Camas Woods LLC (Parcel 178140000) and HSR Camas Woods P2, LLC (Parcels 178159000, 178169000, and 178108000). Parcel 178140000 was originally purchased by HSR Capital, LLC on November 23, 2022 from the Webberly Family Living Trust et al. for \$3,650,000. On October 17, 2023, the ownership in this parcel was transferred to Camas Woods LLC. This was a related party transfer/change of name with no stated consideration.

The remaining parcels (Parcels 178159000, 178169000, and 178108000) were purchased from Jana L. Brandli and Linda K. Thomas, Co-Trustees of the Rekdahl Living Trust dated January 4, 2006 by HSR Camas Woods P2, LLC on September 18, 2024 for \$3,516,344.

Given the length of time from the negotiation and closing of the original 2022 purchase, it is not given significant weight in this analysis, but the more recent 2024 sale is used in the valuation analysis with an adjustment for required off-site costs.

**Assessed Values/Property Taxes:**

TAXES & ASSESSMENT (2025 Values for 2026 Taxes)					
				TAX RATE	0.9508%
PARCEL	LAND	IMPROVEMENTS	TOTAL	TAXABLE	TAXES
178140000	\$5,054,116	\$0	\$5,054,116	\$5,054,116	\$48,757.05
178108000	\$492,800	\$0	\$492,800	\$492,800	\$4,441.73
178169000	\$262,956	\$275,865	\$538,821	\$538,821	\$4,891.66
178159000	\$409,478	\$117,749	\$527,227	\$527,227	\$4,787.96
<b>TOTALS</b>	<b>\$6,219,350</b>	<b>\$393,614</b>	<b>\$6,612,964</b>	<b>\$6,612,964</b>	<b>\$62,878.40</b>

Source: Clark County Assessment & Taxation

**Definition of Market Value:** Given the scope and intended use of this assignment, the applicable value definition is “Market Value.” It is noted that a specific market value conclusion (as unencumbered) is not included in the scope of work for this assignment with the analysis focusing on a supportable diminution of value range (on a percentage basis) resulting from the easement to the State of Oregon. However, in concluding a supportable percentage loss in value, the appraiser has given consideration to the concept of “Market Value” in the concluded diminution of value percentage.

### **Market Value**

The most probable price which a property should bring in a competitive and open market under all conditions requisite to a fair sale, the buyer and seller each acting prudently, knowledgeably, and assuming that the price is not affected by undue stimulus. Implicit in this definition is the consummation of a sale as of a specified date and the passing of title from seller to buyer under conditions whereby:

1. Buyer and seller are typically motivated;
2. Both parties are well informed or well advised, and acting in what they consider their own best interests;
3. A reasonable time is allowed for exposure in the open market;
4. Payment is made in terms of cash in United States dollars or in terms of financial arrangements comparable thereto; and
5. The price represents the normal consideration for the property sold unaffected by special or creative financing or sales concessions granted by anyone associated with the sale.<sup>1</sup>

**Exposure Time:** The appraiser’s opinion of reasonable exposure period for the subject property is 1 to 2 years which is a typical closing period for a large, residential development site.

### **Definition of Easement**

Definition of Easement, as defined by the Dictionary of Real Estate Appraisal Practice, Sixth Edition (2015), is:

“The right to use another’s land for a stated purpose.”

### **Property Description**

**Site:** The Camas Woods Phases 1 & 2 Property consists of four parcels totaling 36.12-acres zoned for a mix of residential densities. The property is within the City of Camas and adjacent (to the north) of Camas High School. The property is proposed/approved for 206 single-family lots and two pads for future multi-family/mixed-use development. The 206 single-family lots include 88 townhomes, 80 small, detached lots, and 38 standard detached lots. The subject property consists of Parcels 178140000, 178108000, 178169000, and 178159000.

**Zoning:** The subject property is zoned for a mix of residential densities. The majority of the subject is zoned HD-NS – North Shore Higher Density Residential and LD-NS North Shore Lower Density Residential. In addition, the northeast corner of Parcel 178140000 is zoned POS-NS, North Shore Park/Open Space and the west portion of Parcel 178159000 is zoned MX-NS, North Shore Mixed Use. The existing waterline easement does not encumber the Mixed Use zoned portion of the property with all but a small portion within the Higher Density and Lower Density residential zones.

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<sup>1</sup> Office of Comptroller of the Currency (OCC), Title 12 of the Code of Federal Regulation, Part 34, Subpart C -Appraisals, 34.42 (g); Office of Thrift Supervision (OTS), 12 CFR 564.2 (g); This is also compatible with the FDIC, FRS and NCUA definitions of market value.

**Highest and Best Use:** The highest and best use of the subject property, both before and after the proposed easement vacation, is residential subdivision development consistent with the Camas Woods Phases 1 & 2 project design and applicable zoning regulations. The presence of the easement does not eliminate buildable lots or reduce permitted density. However, as will be discussed in the valuation analysis, there are some impacts to the utility of 10 lots with the current location of the waterline easement. This is based on the appraiser’s review of the development “as proposed” as well as a review of information provided by engineer Bryce Hanson, PE, LSIT, Certified Arborist with AKS Engineering & Forestry, LLC who considered alternative redesigns to some portions of the site if the easement is not vacated. In summary, if the easement is not vacated/relinquished, there is a reduction in utility to 12 proposed lots on the property.

**Valuation Analysis**

The first step in the valuation analysis is to establish baseline land values per proposed for the various lot types impacted by the City’s waterline easement. The lot types are summarized below.

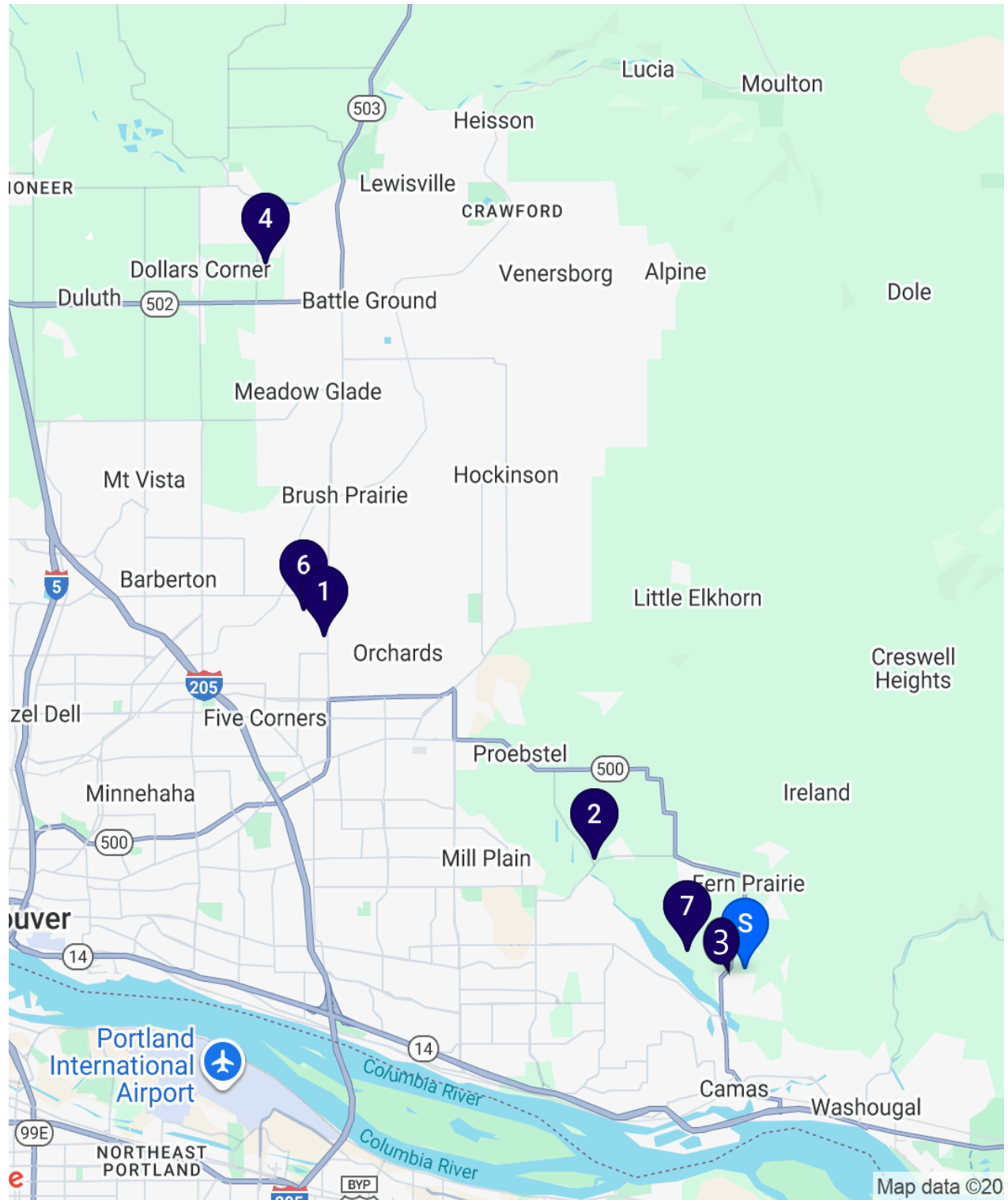
LOT TYPES IMPACTED BY EASEMENT
<b>Component</b>
Attached Townhome Lots
Narrow, Small, Detached Lots
Standard Detached Lots

The conclusion of these baseline land values by lot type allows for the upcoming conclusion of the diminution in value to the individual lots impacted by the existing waterline easement. The land sales summary chart on the following page summarizes the selected comparables along with appropriate adjustments for necessary off-site costs to indicate a “Land Sales Price Per Proposed Lot As Fully Served”.

Comparable Residential Land Sales

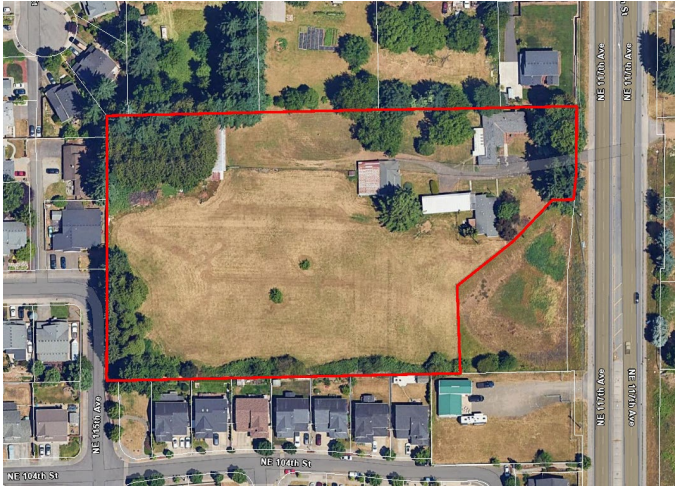
No.	Location / Address	Zoning	Size Acres	Sale Date	Sale Price (\$)	Sale Price Per Acre	Proposed	Sale Price	Comments/Notes	
							Lots or Units	Per Proposed Lot		
							Density/Acre	As Fully Served		
1	Ankenbauer Townhome site HSR 10502 NE 117th Avenue Vancouver, WA 98662 (County)	R1-6	4.14	5/25	\$1,500,000	\$362,319	53 Townhome Lots at 2,500 SF Avg. 12.80 Units/Acre	<b>\$28,302</b>	Irregular site abutting SR-503 propose for townhomes. Good soils with lower construction costs anticipated.	
2	LaCamas Village Subdivision NWC of NE Ingle Road/NE Goodwin Rd Camas, WA 98607	R-6 & MF-10	12.7	10/25	\$5,500,000	\$433,071	159 113 Detached & 46 Townhome 12.60 Units/Acre	<b>\$34,591</b>	Mix of detached and attached townhomes. Located in part of the Green Mountain Master Plan. The developer proposed plat in for approvals now with significant planning work done	
3	Camas Woods-West Portion 920 SE Gardner Road Camas, WA 178159, 178169, 178108	HD-NW & MX High Density Residential & MX Northshore	10.00	9/24	\$3,516,344 <u>Plus \$940,000 Off-sites</u> \$4,456,344	\$351,634	144 56 TH & 88 APT 144 Ttl. Res Units 14.44 Units/Acre	<b>\$30,947</b> Adjusted for Off-Sites as Fully Served	Western 10-acres of the overall Camas Woods Ph. 1 & 2 plat purchased 9/24. We have adjusted the price upward by \$16,800/unit applied to the 56 SFR lot portion of the total 206 SF Lots for its allocated off-site costs to be considered fully served with utilities/access for this analysis as part of Phase 1 off-site costs.	
4	Village at Cherry Grove NE 92nd Avenue, S of NW 15th Way Battle Ground, wA 228522000 & 228545-000	R10	20.01	3/25	\$5,000,000 <u>Plus \$1,500,000 Off-sites</u> \$6,500,000	\$324,838	180 Townhome lots at 2,500 SF 9.00 Units/Acre	<b>\$36,111</b> Adjusted for Off-sites as fully Served	Closed sales for a level site proposed for 180 townhome lots now under construction. The price was adjusted upward to reflect the off-site costs the buyer will incur to extend water and sewer to the site.	
5	Confidential Pending Sale Vancouver, WA (Clark County)	R1-6 Middle Housing Option	8.48	Confidential Pending Sale 12/25	\$3,875,000	\$456,958	98 Townhome Lots at 2807 SF Avg. 11.55 Units/Acre	<b>\$39,541</b>	Level, fully served site pending sale for development of 98 future lots averaging 2807 Sf. Site will have lower than typical development costs with level topo, no off-site cost, and well drained soils with most of the frontage improvements done.	
6	Falcon Pionte 10809 NE 119th Street Vancouver, WA Pcl# 199611-000 & 986063383	R1-5	39.74	8/24	\$11,712,000	\$294,716	247 Standard Detached at 5,000 SF Avg. 6.22 Units/Acre	<b>\$47,417</b>	Level, rectangular site with utilities in Brush Prairie area of Vancouver purchased by large local builder for development of larger standard lot at over 5,000 SF. Development costs will be low for this project given size, soils, shape, etc..and the lot sizes are	
7	Mills West Land N. of Leadbetter Road, West of Boat Ramp Camas, WA  Pcl#177884-000	HD-NS	35.6 <u>-9.25</u> 26.35	12/25 Pending Now Under Contract	\$6,000,000, <u>Plus \$1,000,000 Off-Sites</u> \$7,000,000	\$265,655	127 Standard Detached at 6,500 SF Avg. 4.82 units/acre	<b>\$55,118</b>	This represents the asking price for a 26.35 acre developable site located north of LaCamas Lake that is now reportedly under contract to large developer SunCal. The pending price was not disclosed. This site is proposed for large lots over 6,500 SF and will be served by the proposed abutting plat to the east.	

**RESIDENTIAL LAND SALE COMPARISON LOCATION MAP**



**Comparable 5 is a Confidential Pending**

**RESIDENTIAL LAND SALE COMPARISON AERIALS**



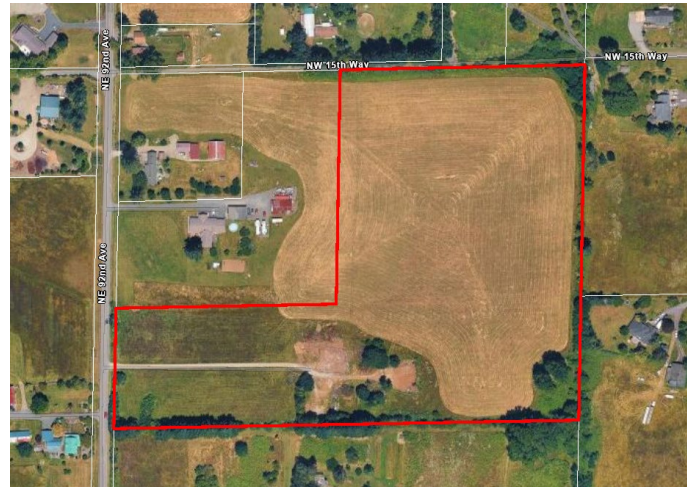
1. Ankenbauer Townhome site HSR



2. LaCamas Village Subdivision



3. Camas Woods-West Portion



4. Village at Cherry Grove



5. Falcon Pointe



7. Mills West Land

The comparables indicate a range of \$28,302 to \$55,118 per potential lot. The comparables include land sales proposed for a variety of lot types. Comparable 3 represents the 2024 sale of the west portion of the subject. The transaction price is adjusted upward for the allocated portion of the off-site costs required to serve the property (sewer extensions, roundabout, traffic mitigation).

**Townhome Lot Values:** Regarding the subject’s townhome lots, the low end is indicated by Comparable 1, which has an inferior location and site configuration. This sale sets the low end of the value range at \$28,032/lot. The high end of the range for the subject’s townhouse lots is indicated by Comparable 2 at \$34,591/lot which has a slightly lower density and a mix of townhouse and detached lots. Adjusted for off-site costs, the west portion of the subject (Comparable 3) was purchased for \$30,947/lot. As this includes some planned, higher density apartment units, it is a slightly low indicator for the subject’s townhome lots. Based on the information above, a value of **\$32,000/lot** is concluded for the subject’s townhome lots.

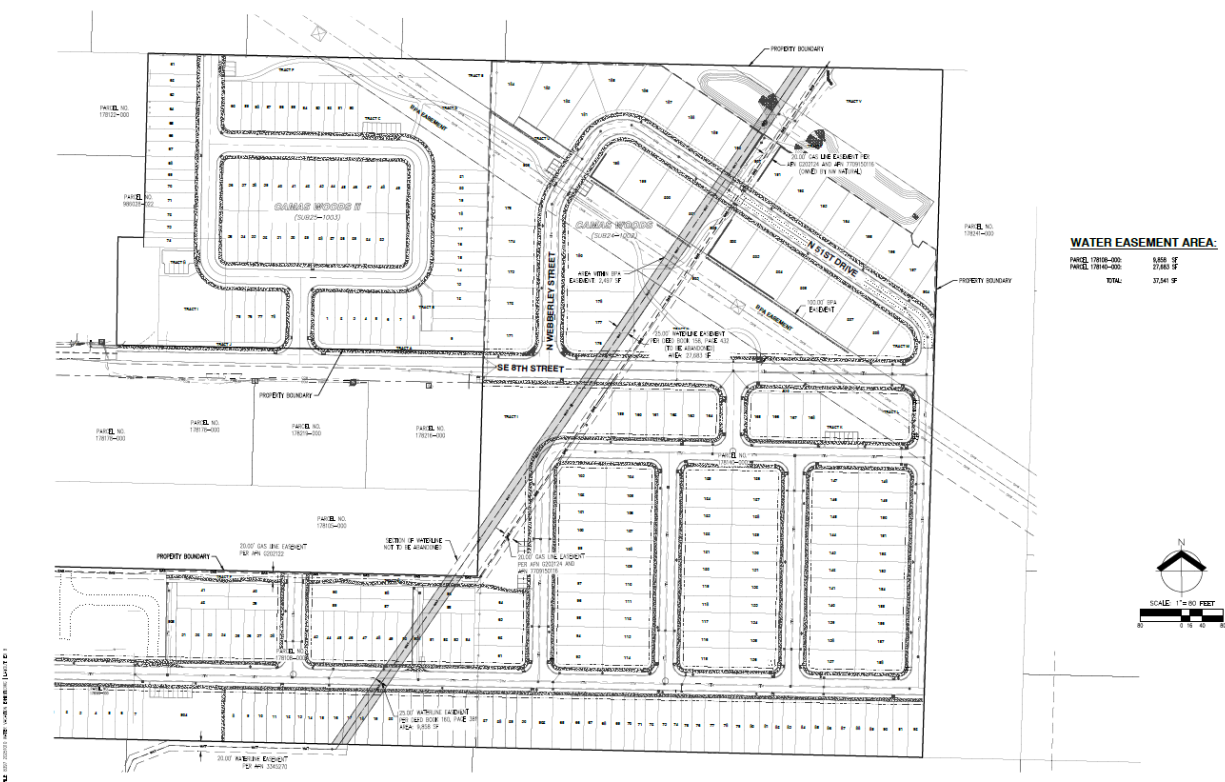
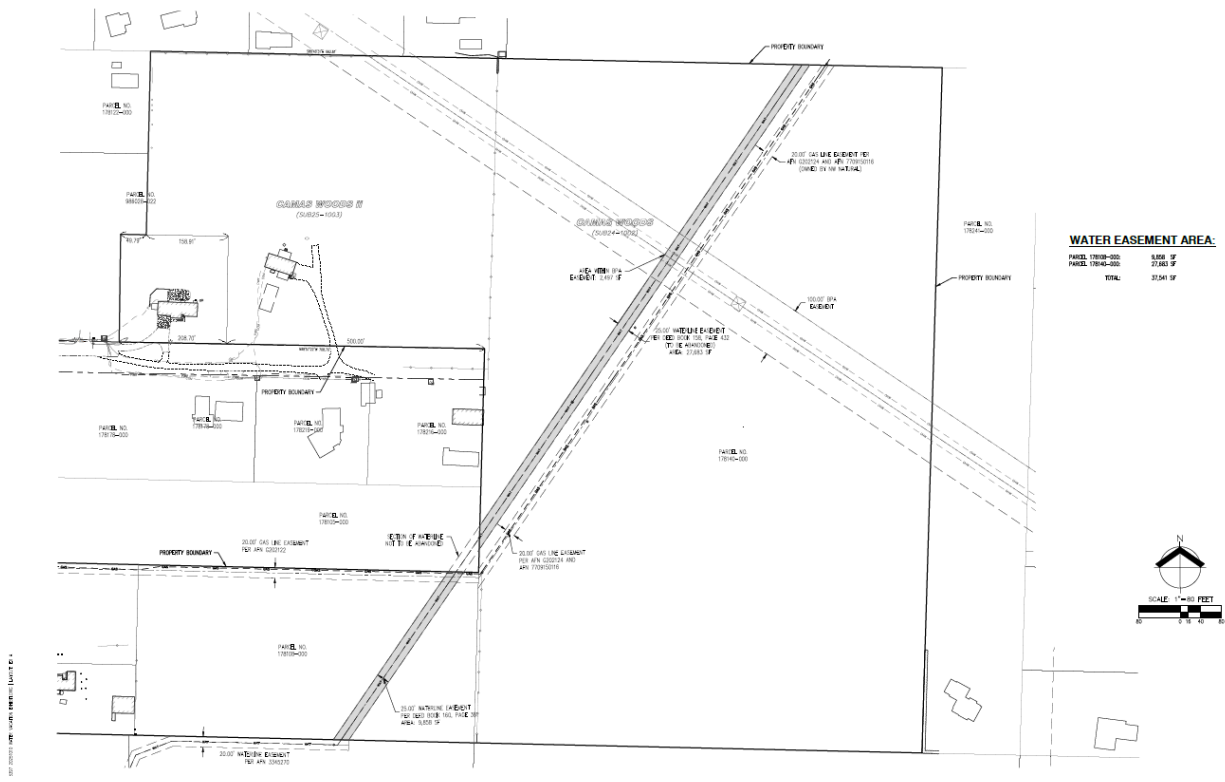
**Narrow Small Detached & Standard Detached Lot Values:** The subject’s Narrow Small Detached Lots and Standard Detached Lots are somewhat similar with the primary difference being the narrower width of the “Narrow Small Detached Lots”. Comparable 2 (\$34,591/lot) brackets the low end of value for these lots with a mix of detached and townhome lots. The most applicable comparables for these lot types are Comparables 6 (\$47,417/lot) and 7 (\$55,118/lot). Comparable 6 will have lower development costs, supporting a value just below this sale for the subject’s Narrow Small Detached Lots, with a value conclusion of **\$45,000/lot**. The subject’s Standard Detached Lots support a value between the two sales at the upper end of the range of **\$50,000/lot**.

The land value per proposed lot, on a fully served basis, for each lot type are summarized below. It is noted that these values do not reflect an adjustment for the subject’s extraordinary off-site costs.

Per Lot Land Values Based on Lot Type	
Component	Land Value Per Lot
Attached Townhome Lots	\$32,000
Narrow, Small, Detached Lots	\$45,000
Standard Detached Lots	\$50,000

**Easement Description**

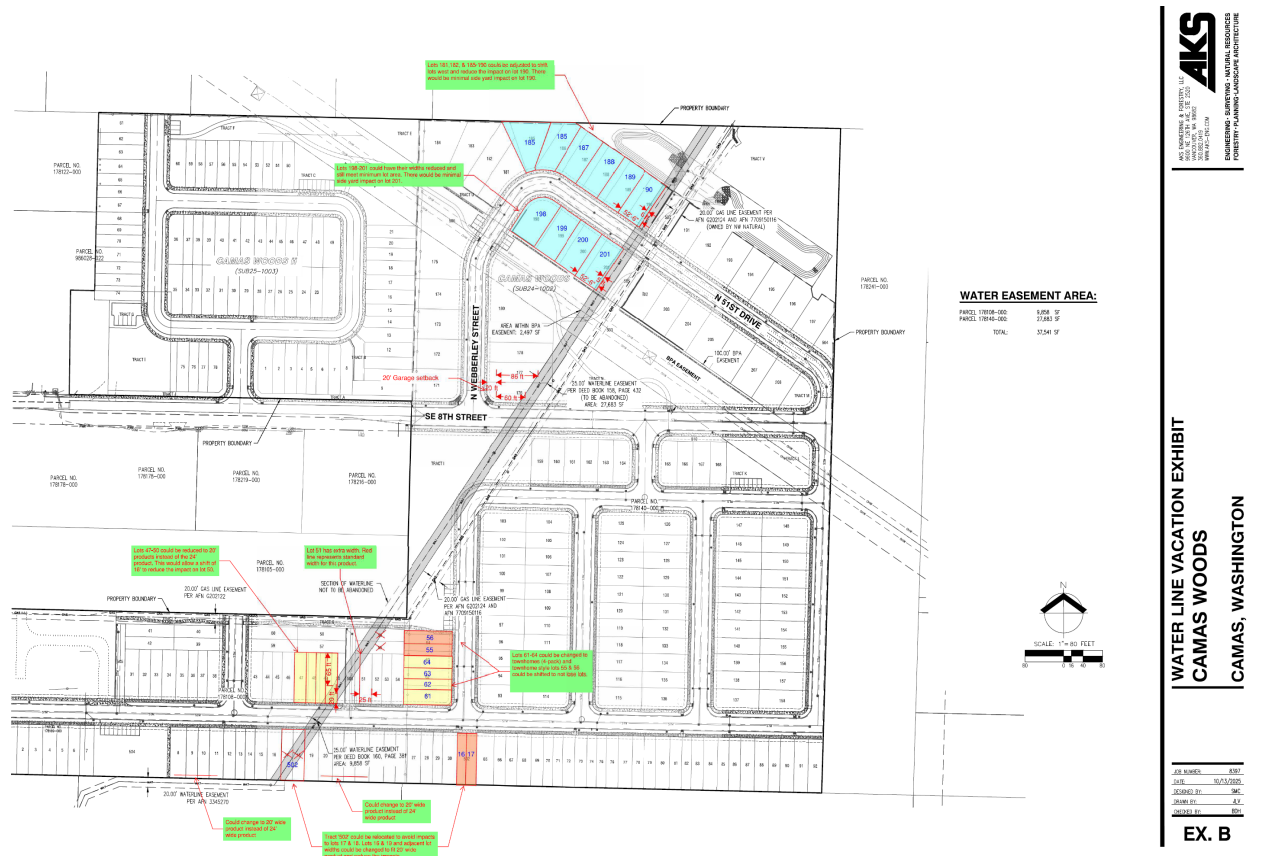
The waterline easement proposed to be vacated/relinquished is 25-foot side and runs diagonally across the subject property. The easement encumbers a total of 37,541 SF or 0.86 acres. While there is an existing older steel waterline within the easement area, the existing water main and easement are no longer needed by the City as a new water transmission mainline was reportedly installed in an alternative location. The two maps below show the existing easement.



**Diminution in Value Analysis**

The proposed development was designed assuming the existing waterline easement will be vacated. Therefore, the appraiser has also relied on information from engineer Bryce Hanson, PE, LSIT, Certified Arborist with AKS Engineering & Forestry, LLC who considered alternative redesigns of some portions of the site if the easement is not vacated. The resulting impact on the number and utility of future residential lots establishes the value impact from the existing waterline easement and mirrors market behavior and reflects the analysis a developer/buyer would consider in a purchase decision.

The exhibit below show the analysis completed by the engineer. This exhibit is also attached to the appraisal allowing a more detailed view.



In summary, by slightly shifting some lots, and some small adjustments to lot dimensions, no lots are lost as a result of the waterline easement. However, even with the reconfiguration described above, the existing waterline easement would encumber 12 lots. These 12 lots and their “lot type” are summarized in the table below.

Lot No.	Proposed Lot Type	Lot Size (SF)
16	Attached TH	2,967
17	Attached TH	2,458
18	Attached TH	2,461
19	Attached TH	2,977
50	Attached TH	2,900
51	Attached TH	3,500
55	Attached TH	2,500
56	Attached TH	2,500
176	Narrow Small Detached	5,294
177	Narrow Small Detached	5,400
190	Standard Detached	5,250
201	Standard Detached	6,000

For each of the impacted lots, the City’s easement interest was valued using a partial diminution in land value approach. This methodology measures the reduction in market value attributable solely to the presence of the waterline easement, where the easement results in partial functional limitations/reduced utility but does not cause a total loss of use.

Market-supported land values on a price per proposed lot basis were established on an as-if fully served basis for each proposed lot product type (see previous land value analysis). An extraordinary off-site cost allocation was deducted from these values to reflect normalized as-is market conditions because the subject project will incur substantial off-site utility and traffic improvements to be considered fully served.

Severity weighting factors ranging from 0.05 (light impact) to 0.40 (moderate impact) were applied on a lot-by-lot basis, reflecting the degree of encroachment, product sensitivity, and available design mitigation. While these categories are somewhat subjective, they are based on the actual impact to each lot and reflect a reasonable indicator of the impact on value.

The primary concern of a developer/builder is the ability to build on a lot. While, with some design mitigation, there is no loss in the potential number of lots, market participants also give consideration the utility and appeal of each lot in a development which is reflected in the analysis below. The first table summarizes each lot, the concluded impact and the reason for the conclusion of the impact to each of the impacted potential lots.

Lot No.	Proposed Lot Type	Severity Category (Weight)	Net Impact (Reason) for Severity Rating	Design Flexibility & Mitigation Considerations
16	Attached TH	Light (0.05)	Minor rear yard encroachment	Retains buildability, minor rear yard encroachment
17	Attached TH	Light (0.05)	Light-product type sensitivity	Light Slight Design Modification-Tract 502 could relocated in place of lots 17 & 18
18	Attached TH	Light (0.05)	Light-product type sensitivity	Light Slight Design Modification-Tract 502 could relocated in place of lots 17 & 18
19	Attached TH	Light (0.05)	Minor front yard encroachment	Retains buildability, minor front yard encroachment
50	Attached TH	Light-Moderate (0.20)	Light-Moderate front/footprint impacts	Design mod to change 47-50 from 24 ft product to 20, reducing impact on lot 50
51	Attached TH	Light (0.05)	Light-Minor back yard encroachment	Retains buildability, wider lot, minor rear yard encroachment
55	Attached TH	Moderate (0.40)	Moderate-product type sensitivity	Design Modification (No net loss of lots), revise lots 61-64 to 2 det & 4 att units
56	Attached TH	Moderate (0.40)	Moderate-product type sensitivity	Design Modification (No net loss of lots), revise lots 61-64 to 2 det & 4 att units
176	Narrow Small Detached	Moderate (0.40)	Moderate-footprint reduction	Deep lot mitigates impact; easement reduces building footprint and limits design flexibility
177	Narrow Small Detached	Light (0.05)	Light rear yard impact	Deep lot partially mitigates impact; minor rear yard encroachment reduces usable yard area.
190	Standard Detached	Light-Moderate (0.20)	Light-Moderate width impact	Could adjust 181, 182 & 185 & shift 186-190 westerly (moderate impact on lot width)
201	Standard Detached	Light (0.05)	Reduced side yard by a few feet, still buildable	Shift/narrow up lots 198-201 to 52.5', with 5 ft or less impact on building area

Lot No.	Proposed Lot Type	Lot Size (SF)	Land Value As-If Fully Served (No Easement)	Less Extraordinary Off-Site Cost Allocation/Unit	Normalized As-Is Market Value (Non-Easement Adjusted)	Severity Category (Weight)	Easement Diminution in Value
16	Attached TH	2,967	\$32,000	\$16,800	\$15,200	Light (0.05)	\$760
17	Attached TH	2,458	\$32,000	\$16,800	\$15,200	Light (0.05)	\$760
18	Attached TH	2,461	\$32,000	\$16,800	\$15,200	Light (0.05)	\$760
19	Attached TH	2,977	\$32,000	\$16,800	\$15,200	Light (0.05)	\$760
50	Attached TH	2,900	\$32,000	\$16,800	\$15,200	Light-Moderate (0.20)	\$3,040
51	Attached TH	3,500	\$32,000	\$16,800	\$15,200	Light (0.05)	\$760
55	Attached TH	2,500	\$32,000	\$16,800	\$15,200	Moderate (0.40)	\$6,080
56	Attached TH	2,500	\$32,000	\$16,800	\$15,200	Moderate (0.40)	\$6,080
176	Narrow Small Detached	5,294	\$45,000	\$16,800	\$28,200	Moderate (0.40)	\$11,280
177	Narrow Small Detached	5,400	\$45,000	\$16,800	\$28,200	Light (0.05)	\$1,410
190	Standard Detached	5,250	\$50,000	\$16,800	\$33,200	Light-Moderate (0.20)	\$6,640
201	Standard Detached	6,000	\$50,000	\$16,800	\$33,200	Light (0.05)	\$1,660
			\$446,000	\$201,600	\$244,400		\$39,990

The total value impact from the existing waterline easement is \$39,990, rounded to **\$40,000**. Based on the total value of the land, adjusted for applicable off-site costs, or \$244,400, this represents a loss in value of 16.4%. The \$40,000 value conclusion represents the incremental increase in the subject’s land value that is attributable to the removal of the existing waterline easement assuming no loss of lot count, no reduction in density, and continued buildability of all affected lots.

Valuation Summary	
Value Scenario	Value Conclusion
Market Value/Diminution in Value from Existing Easement	\$40,000

**Proposed Developer Water System Improvements**

The analysis above reflects the value impact of the subject’s existing waterline easement. In conjunction with the proposed easement vacation, the developer proposes to construct and dedicate new public water system improvements to the City of Camas during Phase 1. These improvements include removal of an obsolete steel waterline within the easement area at an estimated cost of \$16,764, and construction of new water system infrastructure at an estimated cost of \$1,373,017 for Phase 1. Contractor estimates for these improvements are retained in the appraiser’s file.

The proposed water system will serve the Camas Woods subdivision, allow for future extension to the east, provide system looping for redundancy, and improve water pressure and reliability for the subdivision and surrounding service areas. These improvements are not reflected in the concluded fair market value of the easement interest. Consideration of these improvements as an offset to just compensation is a policy determination that is beyond the scope of this appraisal assignment.

HSR CAPITAL CAMAS WOODS P2, LLC  
Attn: Andy Swanson

January 26, 2026  
Page 14

Item 5.

If you have questions regarding this appraisal, please contact me.

Sincerely,

APPRAISAL & CONSULTING GROUP, LLC

A handwritten signature in blue ink that reads "Matthew Call".

Matthew P. Call, MAI  
Certified General Real Estate Appraiser  
State of Washington License No. 1102167  
Email: matthew.call@acgvaluation.com  
Phone: 503.740.8729

MPC:ays

A250309

## ASSUMPTIONS & LIMITING CONDITIONS

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- ▶ Information presented in this report has been obtained from reliable sources, and it is assumed that the information is accurate.
- ▶ This analysis assumes that the information provided for this appraisal accurately reflect the current condition of the subject property.
- ▶ This report shall be used for its intended purpose only, and by the party to whom it is addressed. Possession of this report does not include the right of publication.
- ▶ The appraiser may not be required to give testimony or to appear in court by reason of this appraisal, with reference to the property in question, unless prior arrangements have been made.
- ▶ The statements of value and all conclusions shall apply as of the dates shown herein.
- ▶ There is no present or contemplated future interest in the property by the appraiser which is not specifically disclosed in this report.
- ▶ Without the written consent or approval of the author neither all, nor any part of, the contents of this report shall be conveyed to the public through advertising, public relations, news, sales, or other media. This applies particularly to value conclusions and to the identity of the appraiser and the company with which the appraiser is connected.
- ▶ This report must be used in its entirety. Reliance on any portion of the report independent of others, may lead the reader to erroneous conclusions regarding the property values. Unless approval is provided by the author no portion of the report stands alone.
- ▶ We assume no responsibility for matters legal in character, nor do we render any opinion as to title, which is assumed to be marketable. All existing liens, encumbrances, and assessments have been disregarded, unless otherwise noted, and the property is appraised as though free and clear, under responsible ownership, and competent management.
- ▶ The appraisal has provided exhibits to assist the client(s)/intended user(s) to understand from a graphical standpoint some of the salient issues which impact the subject property. We have made no survey of the property and if further verification is required, a survey by a registered surveyor is advised.
- ▶ The appraiser assumes no responsibility for determining if the property requires environmental approval by the appropriate governing agencies, nor if it is in violation thereof, unless otherwise noted herein. This analysis assumes that no asbestos or other hazardous materials are stored or found in or on the subject property. If evidence of hazardous materials of any kind occurs, the reader should seek qualified professional assistance. If hazardous materials are discovered and if future market conditions indicate an impact on value and increased perceived risk, a revision of the concluded values may be necessary.
- ▶ The valuation stated herein assumes professional management and operation of the buildings throughout the lifetime of the improvements, with an adequate maintenance and repair program.
- ▶ The liability of Appraisal & Consulting Group LLC, its principals, agents, and employees is limited to the client. Further, there is no accountability, obligation, or liability to any third party. If this report is placed in the hands of anyone other than the client, the client shall make such party aware of all limiting conditions and assumptions of the assignment and related discussions. The appraiser is in no way responsible for any costs incurred to discover or correct any deficiency in the property.
- ▶ The appraiser is not qualified to detect the presence of toxic or hazardous substances or materials which may influence or be associated with the property or any adjacent properties, has made no investigation or analysis as to the presence of such materials, and expressly disclaims any duty to note the degree of fault. Appraisal & Consulting Group LLC and its principals, agents, employees, shall not be liable for any costs, expenses, assessments, or penalties, or diminution in value, property damage, or personal injury (including death) resulting from or otherwise attributable to toxic or hazardous substances or materials, including without limitation hazardous waste, asbestos material, formaldehyde, or any smoke, vapors, soot, fumes, acids, alkalis, toxic chemicals, liquids, solids or gasses, waste materials or other irritants, contaminants or pollutants.
- ▶ Unless otherwise noted herein, a detailed soils study was not provided for this analysis. The subject's soils and sub-soil conditions are assumed to be suitable based upon a visual inspection of the subject property and surrounding properties, which did not indicate evidence of excessive settling or unstable soils. No certification is made regarding the stability or suitability of the soil or sub-soil conditions.

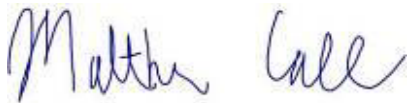
## CERTIFICATION

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I certify that, to the best of my knowledge and belief:

- ▶ The statements of fact contained in this report are true and correct.
- ▶ The reported analyses, opinions, and conclusions of the signer are limited only by the reported assumptions and limiting conditions, and are my personal, impartial, and unbiased professional analyses, opinions, and conclusions.
- ▶ The signer of this report has no present or prospective interest in the property that is the subject of this report, and no personal interest with respect to the parties involved.
- ▶ Matthew Call, MAI has performed no services, specifically as an appraiser or in any other capacity, regarding the property that is the subject of this report within the three-year period immediately preceding acceptance of this assignment.
- ▶ The signer is not biased with respect to the property that is the subject of this report or to the parties involved with this assignment.
- ▶ The engagement in this assignment was not contingent upon developing or reporting predetermined results.
- ▶ The compensation for completing this assignment is not contingent upon the development or reporting of a predetermined value or direction in value that favors the cause of the client, the amount of the value opinion, the attainment of a stipulated result, or the occurrence of a subsequent event directly related to the intended use of this appraisal.
- ▶ The reported analysis, opinions, and conclusions were developed, and this report has been prepared, in conformity with the requirements of the *Code of Professional Ethics* and *Standards of Professional Appraisal Practice* of the Appraisal Institute, and the *Uniform Standards of Professional Appraisal Practice*, as set forth by the Appraisal Standards Board of the Appraisal Foundation.
- ▶ Matthew Call, MAI has made an in person inspection of the subject property but has not personally inspected the comparable sales.
- ▶ No one provided significant real property appraisal assistance to the appraisers signing the certification.

The use of this report is subject to the requirements of the Appraisal Institute relating to review by its duly authorized representatives. As of the date of this report, Matthew Call, MAI has completed the continuing education program for Designated Members of the Appraisal Institute.



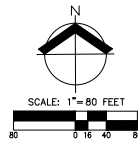
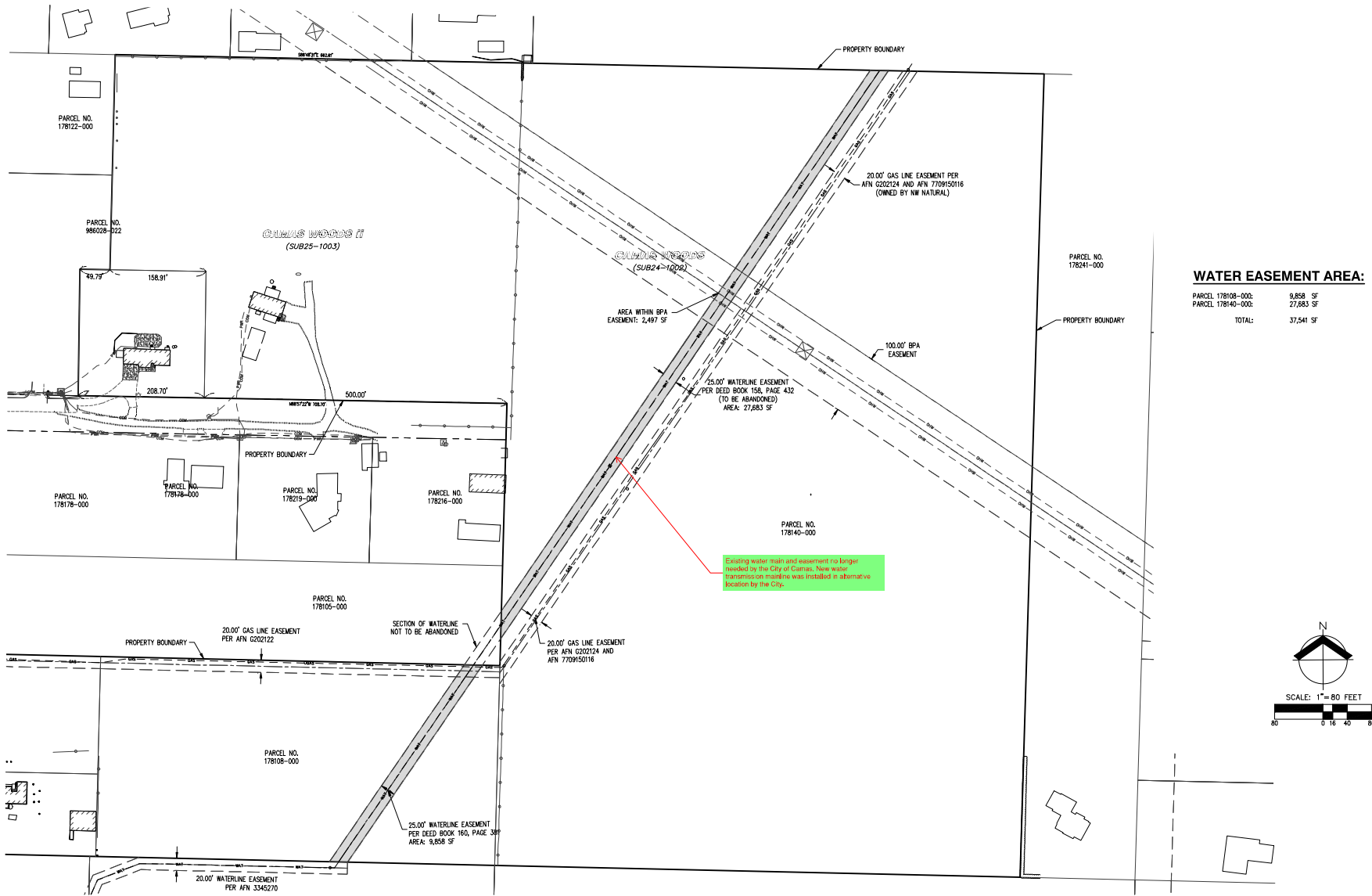

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Matthew Call, MAI  
 Certified General Real Estate Appraiser  
 State of Washington License No. 1102167

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January 26, 2026

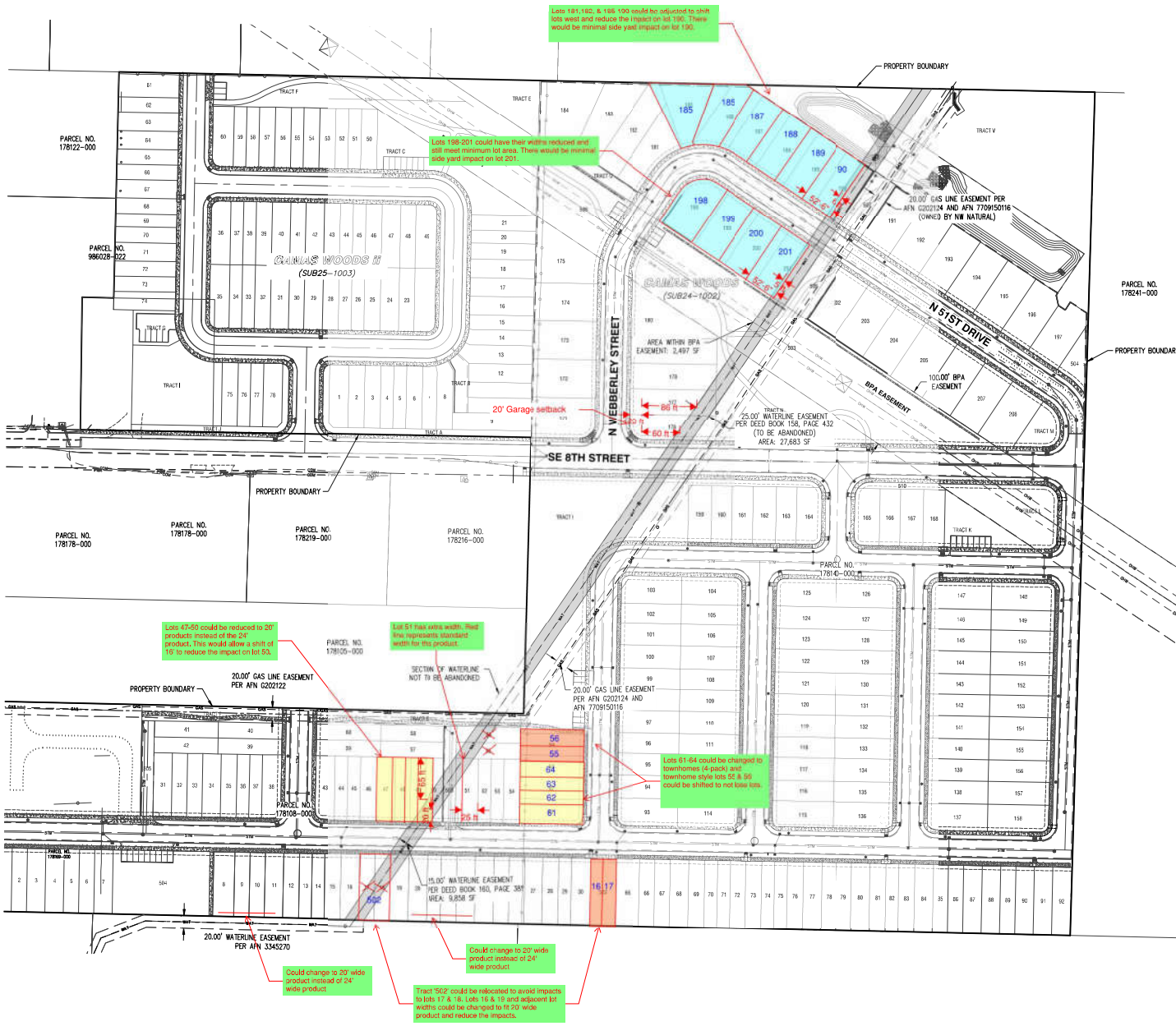
Date



**WATER LINE VACATION EXHIBIT  
 CAMAS WOODS  
 CAMAS, WASHINGTON**

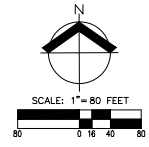
JOB NUMBER:	8397
DATE:	10/11/2025
DESIGNED BY:	SMC
DRAWN BY:	J.V.
CHECKED BY:	BDH

**EX. A**



**WATER EASEMENT AREA:**

PARCEL 178109-000:	9,858 SF
PARCEL 178140-000:	27,883 SF
<b>TOTAL:</b>	<b>37,541 SF</b>



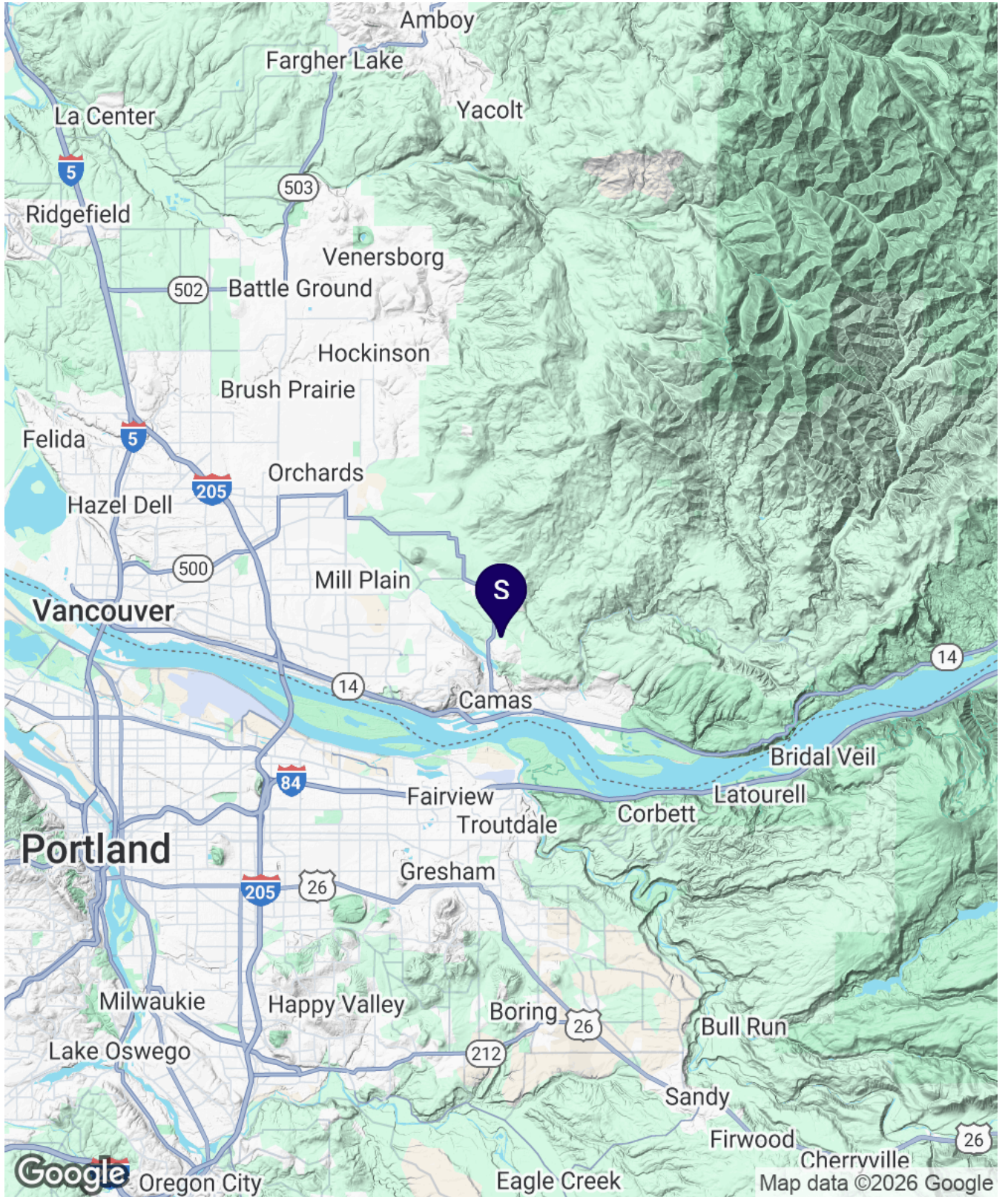
AKS DRAWING FILE: 20250301 WATER VACATION EXHIBIT - LAYOUT - EX B

**WATER LINE VACATION EXHIBIT  
 CAMAS WOODS  
 CAMAS, WASHINGTON**

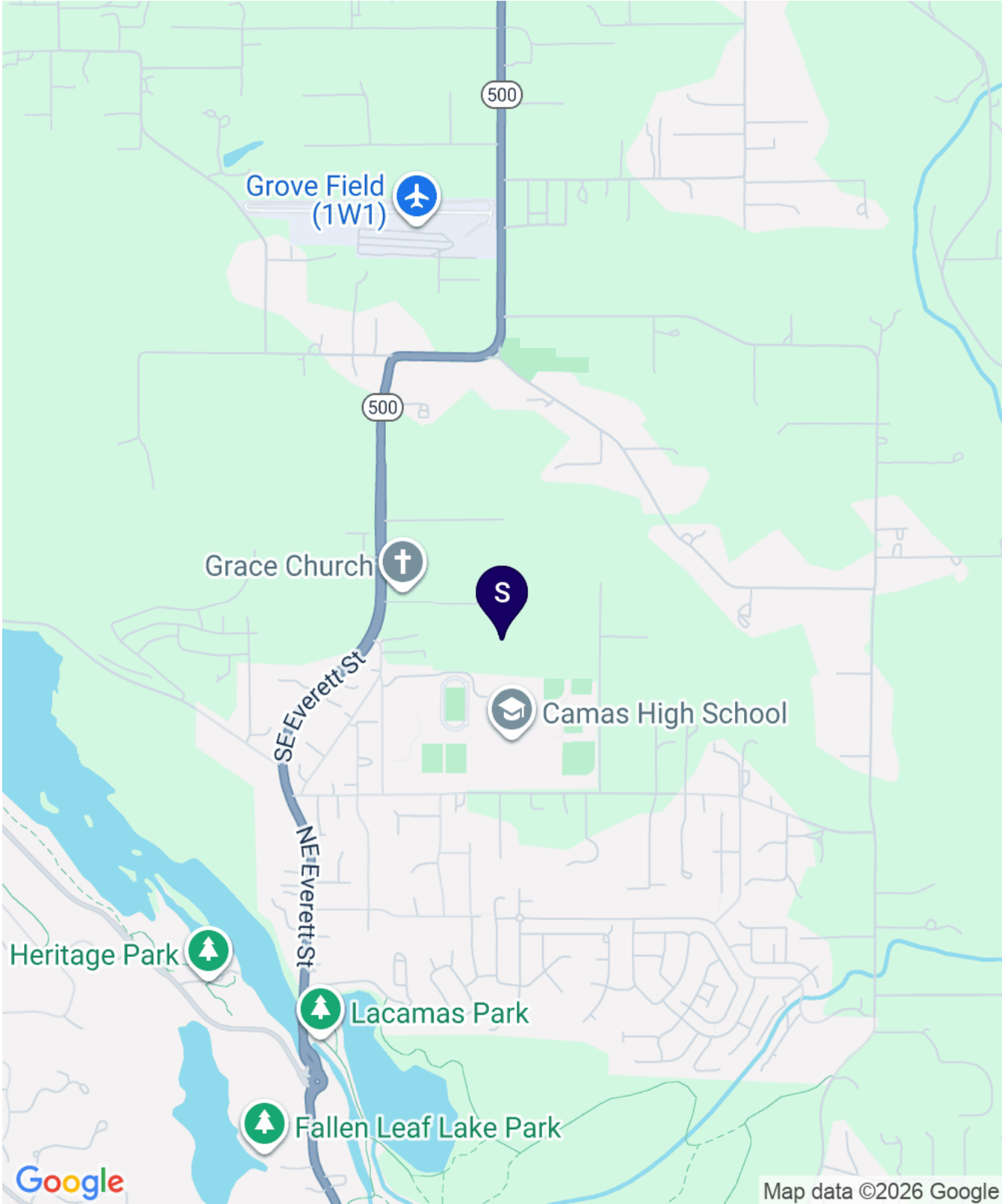
JOB NUMBER: 8397  
 DATE: 10/15/2025  
 DESIGNED BY: SMC  
 DRAWN BY: J.V.  
 CHECKED BY: BDM

**EX. B**

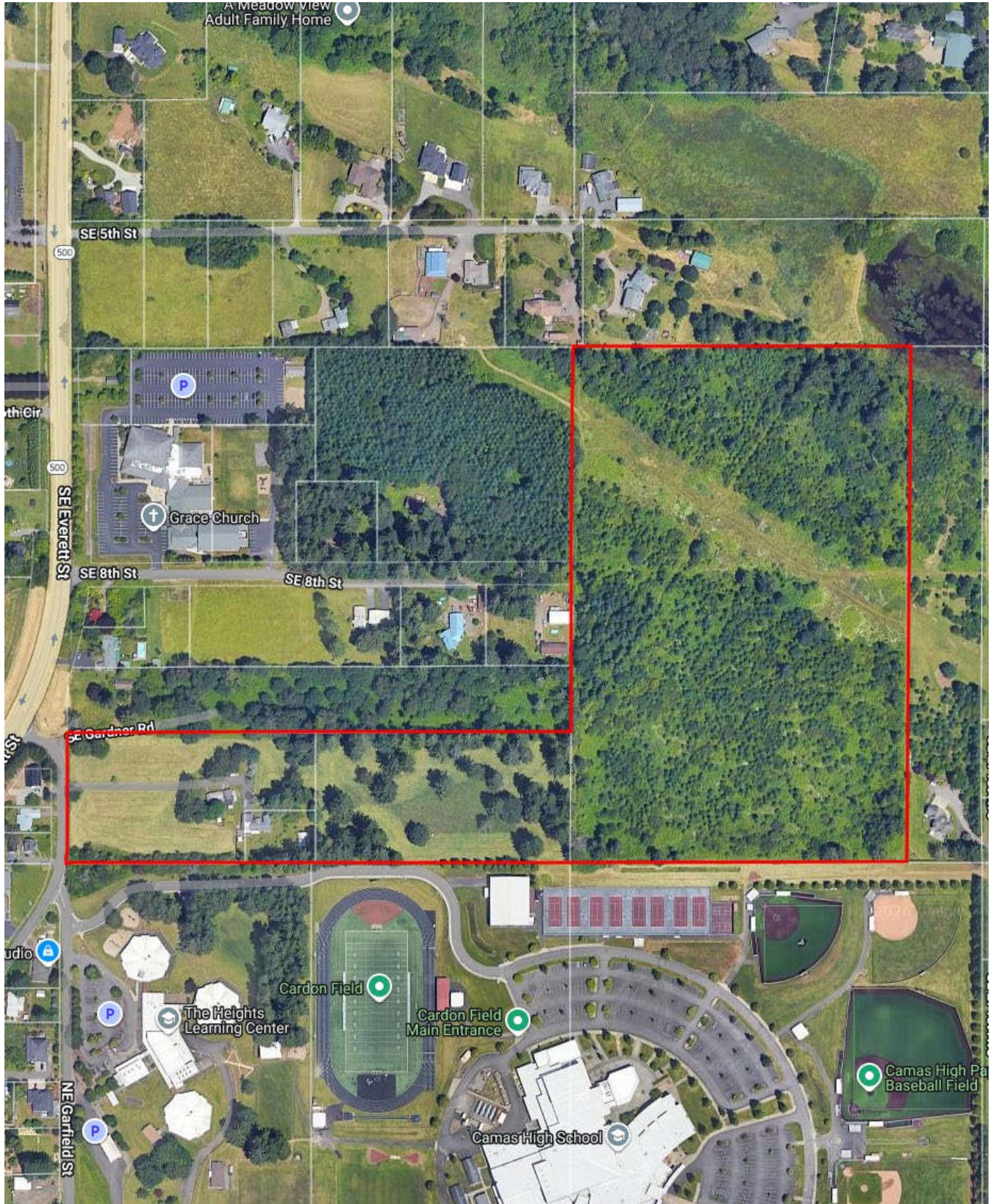
# REGIONAL MAP



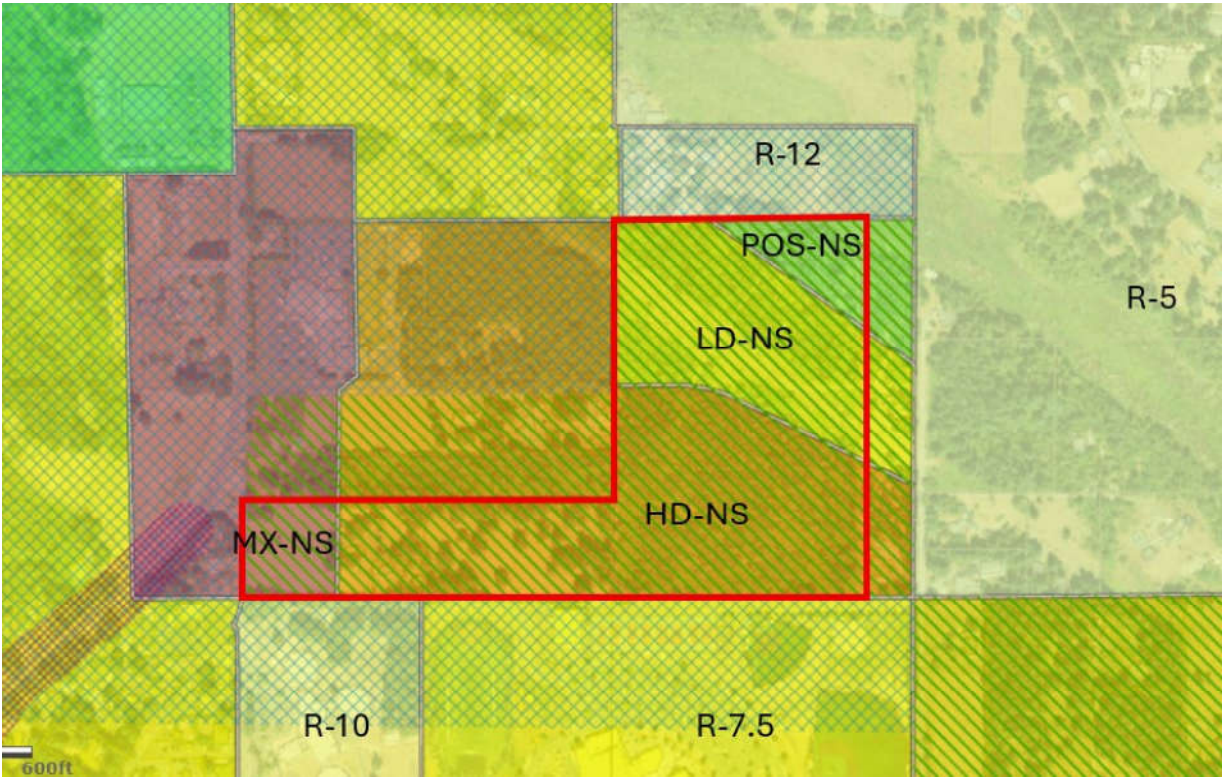
# LOCAL AREA MAP



# AERIAL MAP



# ZONING MAP



# Matthew P. Call, MAI

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## Appraisal & Consulting Group, LLC

After graduating from Gonzaga University in 1998, I was looking for something in the real estate field that would match my interests and skills. I was lucky enough to be put in touch with Don Palmer and began working at Palmer, Groth & Pietka in 1999. During my time at PGP I was able to learn from great mentors and eventually co-manage a group of appraisers in the office. My appraisal experience includes a wide range of property types with a focus on conservation/rural lands, including Yellow Book appraisals, and legal/estate work, litigation, and condemnation cases. I have experience on several multi-property condemnation assignments, in both urban and rural areas, including natural gas, power line, and light rail projects. I enjoy the challenge of unique appraisal assignments and pride myself on attention to detail and working with the client from the initial engagement to answering any questions after the delivery of the report. I obtained the MAI designation from the Appraisal Institute in 2011 and am committed to continue increasing my appraisal knowledge and focus on client service into the future.

### EDUCATION AND QUALIFICATIONS

Bachelor of Arts in Business Administration, Gonzaga University, 1998

### APPRAISAL INSTITUTE COURSES

Uniform Standards of Professional Appraisal Practice

Uniform Appraisal Standards for Federal Land Acquisitions

Valuation of Conservation Easements

Basic and Advanced Condemnation Appraisal

Business Practices & Ethics

Various other Appraisal Institute Courses and Seminars

### EXPERIENCE

Appraisal & Consulting Group – Commercial Appraiser / 2013 – Present

Multnomah Appraisal Group – Partner, 2012

Valuation Services Director, Colliers International Valuation & Advisory Services (formerly Palmer, Groth & Pietka, Inc.), 1999 to 2011

### ORGANIZATIONS

Greater Oregon Chapter of the Appraisal Institute (2024 Chapter Secretary)

La Salle Catholic College Prep Alumni Board (2004-2016)

### PRESENTATIONS

The Seminar Group – 2012 – Eminent Domain: Current Development in Condemnation, Valuation & Challenges  
– “Pipeline Easement Valuation”


The Seminar Group – 2011 – Eminent Domain: Current Development in Condemnation, Valuation & Challenges  
– “Severance Damages and Cost to Cure”

The Seminar Group – 2010 – Eminent Domain: Current Development in Condemnation, Valuation & Challenges  
– “Valuation of Easements”

### STATE CERTIFICATIONS

Oregon, State Certified General Appraiser, License No. C000759

Washington, State Certified General Real Estate Appraiser, License No. 1102167




**STATE OF WASHINGTON**  
 DEPARTMENT OF LICENSING - BUSINESS AND PROFESSIONS DIVISION  
 THIS CERTIFIES THE PERSON OR BUSINESS NAMED BELOW IS AUTHORIZED AS A

**CERTIFIED GENERAL REAL ESTATE APPRAISER**

**MATTHEW P CALL**

1102167 License Number      03/27/2012 Issue Date      08/26/2027 Expiration Date



Marcus J Glasper, Director

(R/4/23)



**Appraiser Certification and Licensure Board**  
 State Certified General Appraiser  
*28 hours of continuing education required*

**MATTHEW P CALL**  
**APPRAISAL & CONSULTING GROUP, LLC**  
**13306 NW CORNELL RD, STE 201**  
**PORTLAND, OR 97229**

License No.: C000759  
 Issue Date: September 01, 2024  
 Expiration Date: August 31, 2026



Chad Koch, Administrator



## Appraisal & Consulting Group, LLC

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Appraisal & Consulting Group, LLC, was formed in June 2012 to serve the appraisal needs of lenders, government agencies, investors, and property owners throughout the Pacific Northwest. With offices in Portland, Oregon, and Vancouver, Washington, our experienced appraisal team is:

*“Ready, willing and able to serve you in a professional and timely manner.”*

Don Palmer, David Groth, and David Pietka, each with 45+ years of experience, have brought together a group of highly experienced appraisers who will take full responsibility for inspections, gathering of information, analysis, and report preparation. Reports are prepared by experienced appraisers and all reports are reviewed for quality and accuracy by an MAI. The members have extensive appraising and real estate consulting experience in Oregon and Washington and average 24 years in the business. With fourteen members, our combined appraisal experience is over 353 years. Our appraisal experience covers all urban property types from single family to large multiple family complexes and from small retail properties to large motels, hotels, retail complexes, office buildings, and special purpose properties. In addition, we have appraisers specializing in rural and resource properties and commercial properties on the Oregon and Washington coast.

We look forward to serving you on a regular basis.

### **PORTLAND**

13306 NW Cornell Road, Suite 201  
Portland, OR 97229

### **VANCOUVER**

112 W 11<sup>th</sup> Street, Suite 250  
Vancouver, WA 98660

971.277.6368 fax

[www.acgvaluation.com](http://www.acgvaluation.com)



AKS ENGINEERING & FORESTRY  
9600 NE 126th Avenue, Suite 2520, Vancouver, WA 98682  
P: (360) 882-0419 F: (360) 882-0426

AKS Job #8397

OFFICES IN: BEND, OR | KEIZER, OR | THE DALLES, OR | TUALATIN, OR | VANCOUVER, WA | WHITE SALMON, WA

**EXHIBIT \_\_**  
**LEGAL DESCRIPTION**  
**WATER EASEMENT TERMINATION**

Located in the Northeast Quarter of Section 35, Township 2 North, Range 3 East of the Willamette Meridian, City of Camas, Clark County, Washington, described as follows:

**COMMENCING** at the southeast corner of the Northeast Quarter of Section 35;

thence along the south line of said Northeast Quarter North 88°42'17" West 1318.71 feet to the southwest corner of Auditor's File Number 6148731;

thence along the west line of said tract North 01°31'20" East 374.56 feet to the most southerly corner of the water pipeline easement per Book 158 at Page 432 (1/28/1924) and the **POINT OF BEGINNING**;

thence continuing along said west line North 01°31'20" East 46.02 feet;

thence along the northwest line of said easement North 34°25'46" East 299.11 feet;

thence continuing along the northwest line of said easement North 34°18'32" East 782.26 feet to the north line of said Auditor's File Number 6148731;

thence along said north line South 88°48'31" East 29.85 feet;

thence along the southeast line of said easement South 34°18'32" West 798.60 feet;

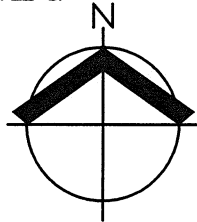
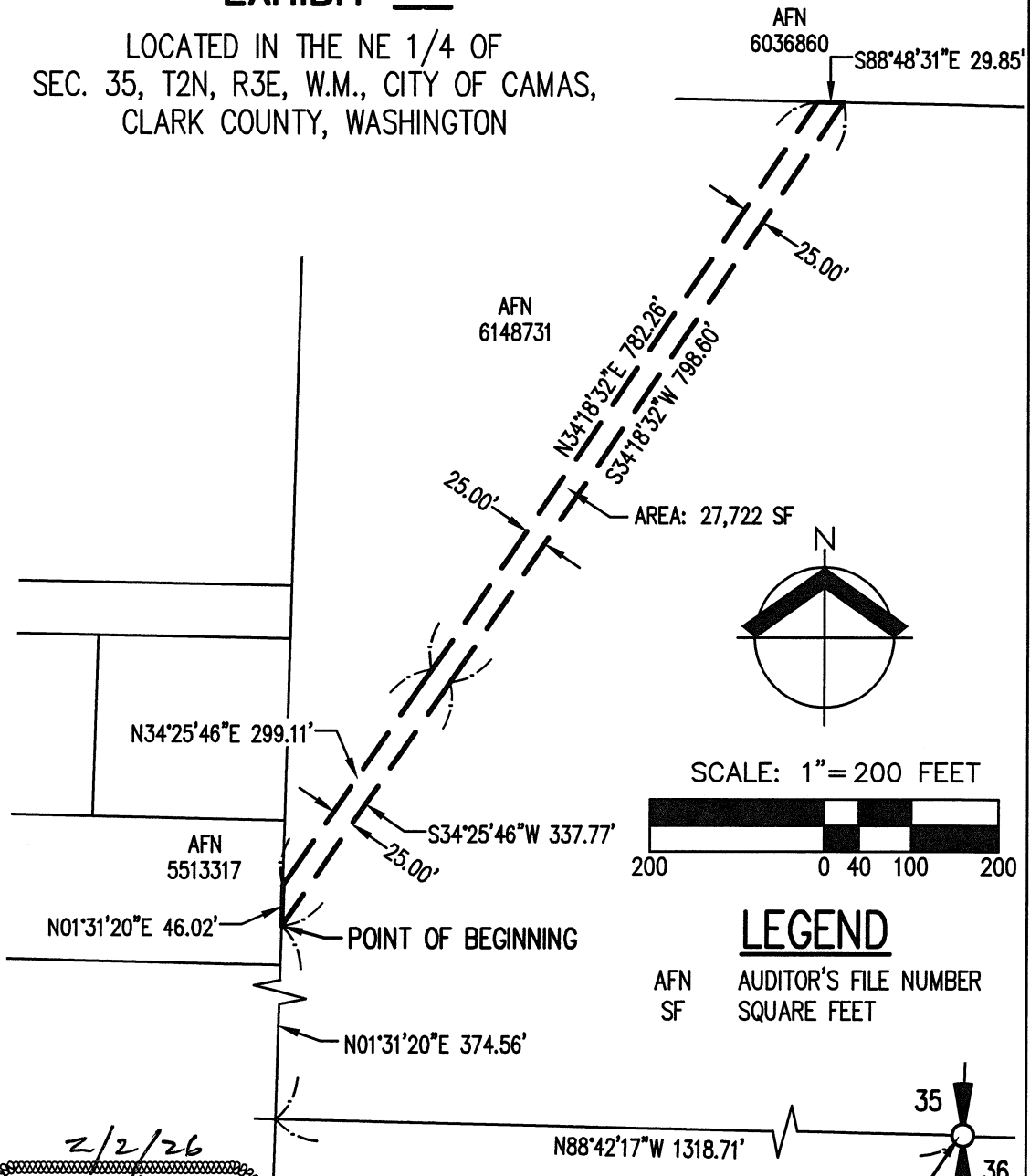
thence continuing along the southeast line of said easement South 34°25'46" West 337.77 feet to the **POINT OF BEGINNING**.

Contains approximately 27,722 square feet.

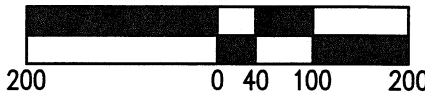


# EXHIBIT

LOCATED IN THE NE 1/4 OF  
SEC. 35, T2N, R3E, W.M., CITY OF CAMAS,  
CLARK COUNTY, WASHINGTON



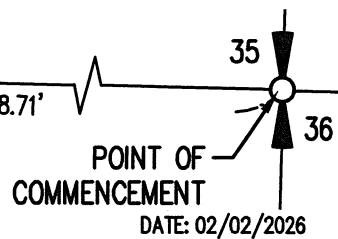
SCALE: 1" = 200 FEET



## LEGEND

AFN     AUDITOR'S FILE NUMBER  
SF     SQUARE FEET

2/2/26



WATER EASEMENT TERMINATION			
DRAWN BY: CJC	CHECKED BY: JOH	DWG: 8397 WAT TERMINATE	JOB: 8397
AKS ENGINEERING & FORESTRY, LLC 9600 NE 126TH AVE, SUITE 2520 VANCOUVER, WA 98682 360.882.0419     WWW.AKS-ENG.COM			





AKS ENGINEERING & FORESTRY  
9600 NE 126th Avenue, Suite 2520, Vancouver, WA 98682  
P: (360) 882-0419 F: (360) 882-0426

AKS Job #8397

OFFICES IN: BEND, OR | KEIZER, OR | THE DALLES, OR | TUALATIN, OR | VANCOUVER, WA | WHITE SALMON, WA

**EXHIBIT \_\_**  
**LEGAL DESCRIPTION**  
**WATER EASEMENT TERMINATION**

Located in the Northeast Quarter of Section 35, Township 2 North, Range 3 East of the Willamette Meridian, City of Camas, Clark County, Washington, described as follows:

**COMMENCING** at the southeast corner of the Northeast Quarter of Section 35;

thence along the south line of said Northeast Quarter North 88°42'17" West 1561.72 feet to the southeast corner of the water pipeline easement per Book 160 at Page 381 (5/15/1924) and the **POINT OF BEGINNING**;

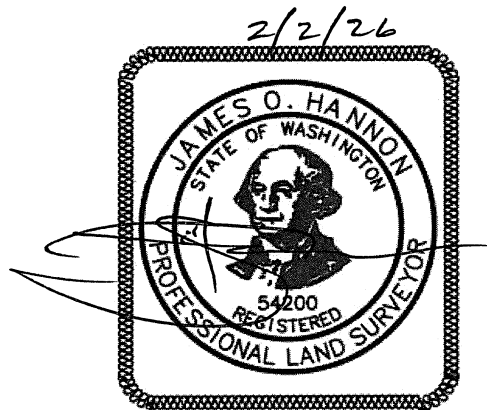
thence continuing along said south line North 88°42'17" West 29.85 feet to the southwest corner of said easement;

thence along the northwest line of said easement North 34°25'46" East 394.31 feet to the north line of Auditor's File Number 6205801;

thence along said north line South 88°42'17" East 29.85 feet to the southeast line of said easement;

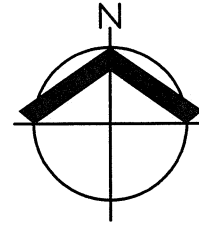
thence along said southeast line South 34°25'46" West 394.31 feet to the **POINT OF BEGINNING**.

Contains approximately 9,858 square feet.

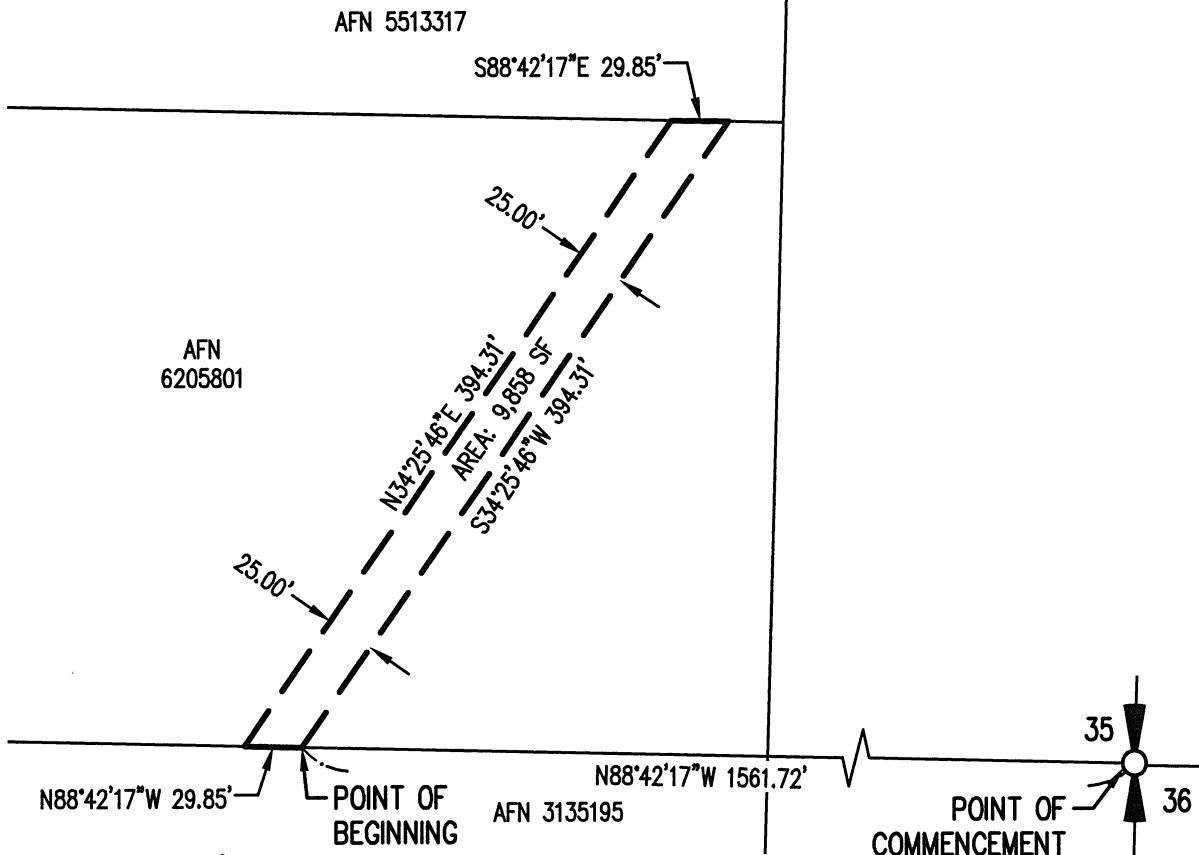
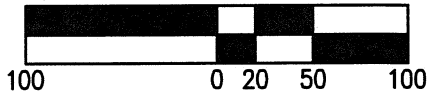


# EXHIBIT \_\_\_

LOCATED IN THE NE 1/4 OF SEC. 35, T2N,  
R3E, W.M., CITY OF CAMAS,  
CLARK COUNTY, WASHINGTON



SCALE: 1" = 100 FEET



AFN  
6205801

AFN 5513317

S88°42'17"E 29.85'

25.00'

25.00'

N34°25'46"E 394.31'  
AREA: 9,858 SF  
S34°25'46"W 394.31'

N88°42'17"W 29.85'

POINT OF BEGINNING

AFN 3135195

N88°42'17"W 1561.72'

POINT OF COMMENCEMENT

35

36

2/2/26

## LEGEND

AFN AUDITOR'S FILE NUMBER  
SF SQUARE FEET

DATE: 02/02/2026



### WATER EASEMENT TERMINATION

DRAWN BY: CJC | CHECKED BY: JOH | DWG: 8397 WAT TERMINATE | JOB: 8397

AKS ENGINEERING & FORESTRY, LLC  
9600 NE 126TH AVE, SUITE 2520  
VANCOUVER, WA 98682  
360.882.0419 WWW.AKS-ENG.COM

