



# city council agenda

**Updated Agenda**  
**August 06, 2019 \* 6:30 PM**  
**City Council Meeting**  
**City Hall Chambers 1600 Nela Avenue**

Nicholas Fouraker Mayor	Kurt Ardaman City Attorney	Bob Francis City Manager	Ed Gold District 1	Anthony Carugno District 2	Karl Shuck District 3	Mike Sims District 4	Harv Readey District 5	Jim Partin District 6	Sue Nielsen District 7
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## Welcome

Welcome to the City of Belle Isle City Council meeting. Agendas and all backup material supporting each agenda item are available in the City Clerk's office or on the city's website at [cityofbelleislefl.org](http://cityofbelleislefl.org).

1. Call to Order and Confirmation of Quorum
2. Invocation and Pledge to Flag - Commissioner Mike Sims - District 4
3. Presentation of the FY2019-2020 Budget (10 minutes)
4. Consent Items (10 minutes)
  - a. Proclamation Announcing the 2020 Election for District 1 and District 7
  - b. Approval of the City Council meeting minutes for July 16, 2019

### 5. Citizen's Comments

**Persons desiring to address the Council MUST complete and provide to the City Clerk a yellow "Request to Speak" form located by the door.** After being recognized by the Mayor, persons are asked to come forward and speak from the lectern, state their name and address, and direct all remarks to the Council as a body and not to individual members of the Council, staff or audience. **Citizen comments and each section of the agenda where public comment is allowed are limited to three (3) minutes.** Questions will be referred to staff and should be answered by staff within a reasonable period of time following the date of the meeting. Order and decorum will be preserved at all meetings. Personal, impertinent or slanderous remarks are not permitted. Thank you.

### 6. Unfinished Business

- a. ORD 19-05 First Reading and Consideration - AN ORDINANCE OF THE CITY OF BELLE ISLE, FLORIDA, AMENDING CHAPTER 34: WATERWAYS, ARTICLE II BOAT EQUIPMENT AND OPERATION, BY REPLACING SECTION 34-37 WAKE REGULATIONS WITH NEW SECTION ENTITLED ESTABLISHMENT OF SPECIAL SLOW SPEED MINIMUM WAKE ZONES; PROVIDING FOR REPEAL OF ORDINANCES OR PARTS OF ORDINANCES IN CONFLICT HEREWITH; PROVIDING FOR INCLUSION IN THE CODE OF ORDINANCES OF THE CITY OF BELLE ISLE, FLORIDA; PROVIDING FOR SEVERABILITY; PROVIDING FOR ENFORCEMENT AND PENALTIES; AND PROVIDING FOR THE EFFECTIVE DATE OF THIS ORDINANCE (30 minutes)
- b. Appointment of District 6 Planning & Zoning member - Andrew Thompson (5 minutes)
- c. Approval of Solid Waste and Recycling Proposal (15 minutes)
- d. Approval of Transportation Master Plan (30 minutes)
- e. Approval of Property & Casualty Insurance Coverage and Services Proposal (15 minutes)

### 7. New Business

- a. Proposed Agreement with Regal Boats to Use Venetian Ramp (10 minutes)
- b. Appointment of Special Events members - Betty DeJarnette and Mayor Nicholas Fouraker ex-officio (5 minutes)

### 8. Attorney's Report

### 9. City Manager's Report

- a. Issues Log (10 minutes)
- b. Chief's Report

### 10. Mayor's Report

### 11. Council Report

### 12. Adjournment

"If a person decides to appeal any decision made by the Council with respect to any matter considered at such meeting or hearing, he/she will need a record of the proceedings, and that, for such purpose, he/she may need to ensure that a verbatim record of the proceedings is made, which record includes the testimony and evidence upon which the appeal is to be based." (F. S. 286.0105). "Persons with disabilities needing assistance to participate in any of these proceedings should contact the City Clerk's Office (407-851-7730) at least 48 hours in advance of the meeting." --Page 1 of 82

# City of Belle Isle

## 2020 Election Proclamation



By the authority vested in me, Nicholas Fouraker, as Mayor of the City of Belle Isle, Florida, do hereby proclaim and pronounce, as required by Ordinance No. 13-07, that a Municipal Election of the City of Belle Isle, Florida will be held Tuesday, March 17, 2020; said election to be held between the hours of 7:00 a.m. and 7:00 p.m., for the purpose of electing individual Commissioners to represent Districts 1 and 7, each to serve a term of three years beginning April 7, 2020.

Each candidate for such office shall file qualification papers and pay a qualifying fee to the City Clerk of the City of Belle Isle at City Hall, located at 1600 Nela Avenue, Belle Isle, Florida, at any time after 12:00 noon on Monday, November 11th, 2019 and before 12:00 noon on Friday, November 15th, 2019.

Candidates wishing to qualify for any office of Council must do so in compliance with the City Charter.

This election will be held at the following locations, respectively:

- Polling Place #9213 located at Belle Isle City Hall, 1600 Nela Avenue, Belle Isle, FL 32809
- Polling Place #9113 located at Pine Castle Lodge #368 F&M, 1216 Hoffner Avenue, Belle Isle, FL 32809.

So be it proclaimed this 6th day of August, 2019.

\_\_\_\_\_  
Mayor Nicholas Fouraker

ATTEST: \_\_\_\_\_  
Yolanda Quiceno, CMC-City Clerk





## city council minutes

b.

### MINUTES July 16, 2019 Regular Session 6:30 pm

The Belle Isle City Council met in a regular session on July 16, 2019, at 6:30 p.m. at the City Hall Chambers located at 1600 Nela Avenue, Belle Isle, FL 32809.

Present was:

Commissioner Ed Gold  
Commissioner Anthony Carugno  
Commissioner Karl Shuck  
Commissioner Jim Partin  
Commissioner Harv Readey  
Commissioner Mike Sims  
Commissioner Sue Nielsen

Absent was:

Mayor Nicholas Fouraker

Also present were City Manager Bob Francis, Attorney Kurt Ardaman, Chief Houston and City Clerk Yolanda Quiceno.

#### CALL TO ORDER

Vice Mayor Readey called the City Council Regular Session to order at 6:30 pm and confirmed quorum. Commissioner Shuck gave the invocation and led the Pledge to the flag.

#### PRESENTATION

Chief Houston congratulated Sergeant Ruiz on a successful apprehension of a suspect on the run in Orange County. She announced that the Orange County

Vice Mayor Readey announced that Mayor Fouraker had a prior engagement and will be in attendance at 7:30 pm. He requested Item #3-Presentation of CCA Refunding of Existing Debt be moved later on the agenda until his arrival.

#### CONSENT ITEMS

- a. Approval of the City Council meeting minutes for May 21, 2019
- b. Approval of the City Council meeting minutes for July 2, 2019
- c. Resolution 19-08 – A Resolution of the City of Belle Isle, Florida, amending the budget for the fiscal year beginning October 1, 2018, and ending September 30, 2109, providing an effective date.
- d. Monthly Reports

**Comm Sims motioned to approve the Consent Agenda as presented.**

**Comm Nielsen seconded the motion.**

**Comm Gold asked to remove item c Resolution 19-08 from the consent agenda.**

**Comm Sims amended the motion to remove item c Resolution 19-08 for discussion.**

**The motion was unanimously approved 7:0.**

**Acting Mayor Reading called for public comment on Resolution 19-08.**

**Clayton Van Kamp residing at 5452 Ming Drive shared his concern with the proposed budget amendment. He said the fund balance last year was projected less than reported. He further shared his concern with the FEMA distribution and asked if the funds will be received this year. He recommended creating a budget committee and ways to look for a reduction in cost moving forward.**

**Mr. Francis said last year's fund balance included the money due from FEMA from three debris reimbursements have not been received totaling \$900,000.**



Comm Gold asked why the fund balance carryover is contingent upon accepting the Resolution. Mr. Francis said approval of the Resolution is to amend the budget to show the deficit of revenue.

Comm Carugno asked about the increase in legal fees.

Mr. Francis said the fees are attributed to research on Council direction such as lot splits, reimbursement of attorney's fee, cornerstone use agreement. Also, the total cost includes fees for the Planning & Zoning Attorney.

After discussion, Comm Nielsen motioned to approve item c – Resolution 19-08.

Comm Sims seconded the motion.

Comm Gold clarified his interpretation and said the resolution is to show what is spent and not contingent upon approval.

The motion was unanimously approved 7:0.

#### EXCUSE ABSENCE

Acting Mayor Readey called for a motion to excuse Mayor Fouraker, Vice Mayor Readey and Comm Carugno from the July 2, 2019, Council meeting.

Comm Gold motioned to excuse Mayor Fouraker, Vice Mayor Readey and Comm Carugno from the July 2, 2019, Council meeting.

Comm Nielsen seconded the motion which passed 6:1 with Comm Carugno-nay.

#### CITIZEN COMMENT

Acting Mayor Readey opened for citizen comment.

There being no comments Acting Mayor Readey closed citizen comment.

#### UNFINISHED BUSINESS

##### Donation Policy

City Manager Francis said the proposed policy covers restricted and unrestricted donations; procedures for accepting donations; and an agreement between the donor and City as to what conditions may be placed on the donation. Also, the policy has been reviewed by the City Attorney. Mr. Francis asked for a motion to approve.

Comm Nielsen motioned to approve the Donation Policy.

Comm Gold seconded the motion which passed unanimously 7:0.

#### NEW BUSINESS

##### a. Discussion of Urban Chickens

Acting Mayor Readey opened for public comment.

Emily Wakley, residing at 3019 Indian Drive, spoke in favor of allowing chickens within the City limits and exploring a Pilot program similar to the City Orlando. She encourages the Council to educate them by attending Back Yard Chicken classes before moving forward.

City Manager Francis gave a brief overview of previous Council meetings that were met with little success. The staff discussed this topic, and although other municipalities allow chickens, the staff determined that allowing chickens from the City may be staff intensive, especially in the beginning. He introduced JK Yarborough, a representative from the Extension Center, to talk about chickens.

Mr. Yarborough works for Orange and Seminole Counties educating residents by providing monthly classes on Back Yard Chickens. He noted that once completed; the attendee will receive a certificate of completion. He covers all aspects of having chickens from cost, purchase, housing, predators, and upkeep.

After discussion, the Council discussed the personal benefits and code enforcement issues that may occur.

Comm Carugno motioned to not allow chickens within the City limits.

Comm Shuck seconded the motion which passed 6:1 with Comm Nielsen-nay.

b. Discussion on Renaming Certain City Recreational Areas

Comm Shuck opened the discussion and said he had received calls by some residents complaining about the use of the beach areas throughout the day and night. He proposed changing the names of the two areas in the city Delia Beach and Swann Beach because these areas are not technically beaches and are drawing more of a nuisance crowd. Mr. Francis said without changing the Comp Plan; he does not believe the name can be changed. Also, these two areas are public parks and open to the public. In the interim, the City can refrain from using “beach” for those parks until they are officially changed in the Comp Plan. If the nuisance happens more at night, the City can add more patrol and ensure that the gates are closed in the evening. Council agreed with the recommendation offered by the City Manager.

c. Planning and Zoning Appointment

Mr. Francis reported, after further research, the applicant who applied for the Planning and Zoning Board open position does not live in District 6 and will not be able to be appointed. There will be no appointment made and the City will re-advertise the vacancy.

d. Discussion on Proposed Ordinance – Slow Speed/Minimum Wake Zones

City Manager Francis said over the past years, traffic on the Conway Chain of Lakes has significantly increased, especially personal watercraft. He spoke of a recent incident on the Lake and the safety concerns that are a common occurrence but never reported. There are several residents on the north lake who have shown interests in annexing into the City. Since the City limit borders the entire lake, he has drafted an ordinance to slow the speed through the narrows. In addition to controlling the speed on the lake and safety on the water, the staff is looking into limiting the sale of alcohol and fuel at the Sunoco station on Daetwyler.

The ordinances that the City and County have in place cannot be enforced because the Florida Wildlife Commission (FWC) has not recognized the ordinances. Comm Uribe informed him that the Sheriff's Office takes the lead on starting the process of getting a Bill in place before presenting to FWC. The staff recommends the City continue to work in partnership with the County and our State Legislators but continue to move forward toward adopting this ordinance as a backup plan.

Comm Carugno asked if the City Attorney has started the process on the preliminary research. Attorney Ardaman said there might be some hurdles before bringing it forward to FWC. Mr. Ardaman said if the City engages the County and maybe the City of Edgewood, it may create positive media recognition, public perception, and government entities participation. He will be meeting with the Chief and Marine Patrol to review our Code and Orange County Code for similarities and differences before bringing it back to Council for review. For the record, Comm Carugno said he and the City Manager have met with many of the residents on the North Lake. He asked the Mayor if he plans to attend any future meetings on the North Lake he should let staff know so that they do not violate any Sunshine Law and be present at the same meeting.

Mayor Fouraker said he is coordinating with House Representative Eskamani on legislation changes to establish lake speed limits and clarifying the definition between reckless and careless. He said because all of the lakes are not in our jurisdiction, we need to continue working in partnership with the County to move things forward.

Acting Mayor Readey opened for public comment.

- August Stanton resident for 70 years on the North Shore thanked Council for laboring support over the current issues.
- Mr. Gene residing at 1700 Lake Grove Lane said he was the person involved in the recent incident with a jet skier and would welcome the Mayor and Council to all meetings held on the Northside of the Lake to make our lakes safer.
- Alexander Klein, a north lake resident, spoke on two fundamental problems: safety on the lake and environmental. He addressed the MSTU tax and said he had been told the lake is “Switzerland” and if nothing is done it will get out of control and someone will get hurt.

Mayor Fouraker asked moving forward the City should research how much Belle Isle residents other Orange County residents contribute to MSTU tax dollars. Bobby Lance residing at 6615 Matchett Road said Belle Isle resident contributions are about 2/3. Discussion ensued.

After discussion, Comm Nielsen moved to forward the draft Ordinance to the City Attorney for review and first reading at the August 6, 2019, City Council meeting.

Comm Carugno seconded the motion.

Comm Partin asked if the Council can be notified if the Attorney cost exceeds \$15,000 before further in the process.

The motion was unanimously approved 7:0.

Mayor Fouraker called for a motion to excuse Comm Readey from the remainder of the meeting.

Comm Nielsen motioned to excused Vice Mayor Readey from the remainder of the meeting.

Comm Partin seconded the motion which passed unanimously 6:0.

**PRESENTATION: CCA Refunding of Existing Debt – Taylor Smith, Cornerstone Charter Academy (CCA) Consultant**

City Manager Francis introduced Taylor Smith, CCA consultant and said the information outlined in the packet is a third option to refinance CCA debt. The Council was presented two other options (1) buy the property and (2) purchase the building, and both were declined. The consultant will provide option three to determine Councils direction and willingness to participate in this option.

Taylor Smith, CCA Consultant, said he had been a part of the journey from the very beginning. Mr. Smith gave a recap of the previous presentation at CCA and the current structure of the lease. He said the school's excellent performance is allowing for this option to be offered. He said the proposed option if approved, will take the City out of the obligation of the maintenance game, will not change the ownership, will not extend or change the term of the lease. Also, the remaining cost of the Wallace Field repayment will be forgiven adding to the City's General Fund. He presented the following third alternative as follows,

1. CCA Acquisition of Property from COBI - declined
2. CCA Ground Lease from COBI - declined
3. CCA-COBI lease modification to allow CCA to refund existing debt, fund expansion and assume maintenance of the Property – proposed

Wesley Bradish, Ziegler Managing Director, said they did the original financing in 2012. This significant opportunity can be achieved because the school is viewed very favorably. The immediate savings from this Ziegler 2019 Refunding will give an annual savings of 100,000 per year. He reported the following,

- Current annual debt \$708,000
- New annual debt \$608,000
- Annual Savings \$100,000 per year
- Total Current Savings \$2,400,000

**Refunding & Cornerstone Masterplan 2.0 Annual Debt Payments**

- •\$1,375,000 per year for 23 years
- •3.8% interest rate fixed for the expansion
- •\$859 annual per student station

**Benefits to the City of Belle Isle**

- •School assumes maintenance obligations of campus (deferred current and future)
- •General Revenue Fund savings from Wallace Park annual repayments to Public Funds School Rental Account
- •, The city, retains ownership of School facilities
- •100% of existing City debt repaid

Council asked the following questions,

1. Q-The documents speak to the Security on the 1<sup>st</sup> mortgage on its facility - Are the buildings on the property to be used as security for the new loan as referenced in section 2 of the June 26, 2019 letter.
  - a. Mr. Smith said there would not be a mortgage on the property. He said that applies to the Texaco station and will need to be clarified.
2. The CCA Governing Board has not made any attempts to meet with the City Council, and some of the benefits need to be discussed and clarified as a group – there needs to be a cooperative effort before moving forward.
3. Q-What is the City's recourse to ensure the completion of the required inspections and repairs.
  - a. The City can add the language to the lease to request a monthly/yearly maintenance report.
4. Q-What is the administrative costs and property insurance fees to the City.

- a. Mr. Francis said he does not believe the loss is substantial. He will provide a report to the Council for review.
5. Q-Does this change any of the relationship with the City and the school remaining a Municipal City Charter School or a Chartered Municipality.
  - a. The question can be answered once a joint session scheduled with CCA and Orange County.
6. Q-Does this change any of the relationship with the City and the school remaining a Municipal City Charter School or a Chartered Municipality.
7. Q-Should the City obtain a Bond Counsel to be our advocate and decipher the proposed plan.
  - a. Attorney Ardaman stated that he had not had the opportunity to review the document in its entirety.
8. Q-Does the City have a seat at the table to oversee to construction and future planning.
  - a. Mr. Smith said the City add it as a condition to the lease modification.
9. Q-What happens if nothing happens tonight, there are a few dates on the proposal; how long is this process going to take to complete and what happens if the September 9<sup>th</sup> deadline is not met.
  - a. Mr. Smith said the dates were set in the contract during previous discussions with CCA to lock the rate. The risk is that the rates may go up and miss the 60-month loan opportunity on interest rates. The process takes approximately 60-90 days.
10. Q-Can the City refund/payoff the bonds at this time without going through this process.

**After further discussion, Comm Carugno motioned not to consider the refinancing debt proposal. Comm Nielsen seconded the motion which passed 6:1 with Comm Shuck – nay.**

#### **EXTENSION OF MEETING TIME**

**Comm Gold motioned to extend the meeting to 9:15 pm.**

**Comm Partin seconded the motion which passed 5:1 with Comm Sims – nay.**

**ATTORNEY REPORT** – No report.

#### **CITY MANAGER'S REPORT**

##### Issues Log update

Budget Workshops – Aug 13 and Aug 27 Budget Workshops, Sept 3-First Budget Hearing and Sept 17 Budget Approval  
Mayor Fouraker asked if Council is open to creating a Budget Advisory Committee.

**Comm Gold motioned to approve the creation of a Budget Committee.**

**The motion dies for lack of a second.**

After discussion, Mayor Fouraker acknowledged that there is not enough time to create a committee, due to the prescribed schedule for budget approval; however, he recommended putting a process in place to start the creation of a budget committee after the CAFR has been presented.

Fountain – The fountain will be up and running, and dedication of the fountain will be scheduled at a later time.

Parking - The City will start sending information on the Parking changes per the Ordinance before issuing citations.

**CHIEF'S REPORT** – No report.

**MAYOR'S REPORT**– No report.

#### **COUNCIL REPORT**

Comm Carugno reported on the ANAC meeting discussions.

#### **ADJOURNMENT**

There being no further business Comm Sims called for a motion to adjourn. The motion was passed unanimously at 9:10 p.m.

Yolanda Quiceno, CMC, City Clerk



## ORDINANCE 2019-

AN ORDINANCE OF THE CITY OF BELLE ISLE, FLORIDA, AMENDING CHAPTER 34: WATERWAYS, ARTICLE II BOAT EQUIPMENT AND OPERATION, BY REPLACING SECTION 34-37 WAKE REGULATIONS WITH NEW SECTION ENTITLED ESTABLISHMENT OF SPECIAL SLOW SPEED MINIMUM WAKE ZONES; PROVIDING FOR REPEAL OF ORDINANCES OR PARTS OF ORDINANCES IN CONFLICT HEREWITH; PROVIDING FOR INCLUSION IN THE CODE OF ORDINANCES OF THE CITY OF BELLE ISLE, FLORIDA; PROVIDING FOR SEVERABILITY; PROVIDING FOR ENFORCEMENT AND PENALTIES; AND PROVIDING FOR THE EFFECTIVE DATE OF THIS ORDINANCE.

WHEREAS, the City Council of the City of Belle Isle desires to provide for the health, safety, and welfare of its citizens, residents and visitors; and

WHEREAS, boating safety concerns around the "Narrows" on the North Lake of the Conway Chain of Lakes (Little Lake Conway) and the Canoe/Kayak Trail at the Canoe/Kayak Launch Locations within the Conway Chain of Lakes have been brought to the attention of the City Council of the City of Belle Isle; and

WHEREAS, there exists extreme safety hazards on portions of the Conway Chain of Lakes which are used for public bathing, pleasure boating and fishing by children and adults; and

WHEREAS, the Conway Chain of Lakes is the location of two Sea Plane Bases; one on the South Lake and One on the North Lake; and

WHEREAS, the Little Lake Conway Sea Plane Base lands within this narrow "gap" on the Little Lake Conway measuring less than 400 feet wide; and

WHEREAS, the hazards that result from the operation of boats and personal watercraft (PWC), whether driven by outboard or inboard motors, at high rates of speed, or in such a fashion as to create a substantial wake, or in a careless fashion; and

WHEREAS, the health, safety and welfare of the citizens of the City of Belle Isle and Orange County and of all persons using or living along the Conway Chain of Lakes is jeopardized unless the use of said boats and PWCs is regulated; and

WHEREAS, no boats shall be operated in the navigable waters of the Conway Chain of Lakes at such a speed so as to cause a wake of such magnitude which will endanger the safety of any swimmer or diver or which will endanger any other boat, whether anchored, secured to a dock or underway; and

WHEREAS, no boats shall be operated in the navigable waters of the Conway Chain of Lakes at such a speed so as to cause a wake of such magnitude which will endanger the safety of sea planes landing or taking off, and

WHEREAS, no boats shall be operated at such a speed so as to cause a wake of such magnitude which will cause danger to any dock, or any other public or private property, or cause appreciable erosion to



the banks or shallow areas of any navigable waterway in a way that will cause irreparable damage to the lake's vegetation; and

WHEREAS, Florida Statutes § 327.46 (1) (b) authorizes the City Council of City of Belle Isle to establish Slow Speed Minimum Wake boating safety zones by ordinance without additional agency approval, within 300 feet of a confluence of water bodies presenting a blind corner, a bend in a narrow channel or fairway, or such other area if an intervening obstruction to visibility may obscure other vessels or other users of the waterway, and hazardous water levels or currents, or containing other navigational hazards on the Conway Chain of Lakes after the Florida Fish and Wildlife Conservation Commission (FWC) has reviewed the ordinance and determined by substantial competent evidence that the ordinance is necessary to protect public safety; and

WHEREAS, the City wishes establish Slow Speed Minimum Wake boating safety zones around specified blind corners within the Conway Chain of Lakes, especially on Little Lake Conway as it approaches the "Narrows" on the Little Lake Conway measuring an average of less than 400 feet at its widest point; and

WHEREAS, the City Council of the City of Belle Isle has identified areas within the Conway Chain of Lakes that require regulation in order to protect the boating public and by allowing boaters to operate at speeds greater than Slow Speed Minimum Wake within this "gap" would create dangerous navigational hazards;

WHEREAS, Florida Statutes § 327.46(1)(c) grants the Board of Commissioners of the City of Belle Isle the authority to implement an Slow Speed Minimum Wake boating safety zone within the "Narrows" after the Florida Fish and Wildlife Conservation Commission has reviewed the ordinance and determined by substantial competent evidence that the ordinance is necessary to protect public safety; and

WHEREAS, the City wishes to establish Slow Speed Minimum Wake boating safety zones within the Conway Chain of Lakes, pursuant to Florida Statutes § 327.46 (1) (b) and within the "Narrows" with the Conway Chain of Lakes boating safety zones that is less than 400 feet in width, pursuant to Florida Statutes § 327.46 (1) (c); and

WHEREAS, an intent of this ordinance is to regulate, from shoreline to shoreline, the entire contiguous zone that is the blind zone area as an Slow Speed Minimum Wake zone for the safety and welfare of the City's citizens, residents, and visitors; and

WHEREAS, nothing in the ordinance shall limit or nullify any areas on the Conway Chain of Lakes where higher speeds and wakes for the recreational enjoyment of City's citizens, residents, and visitors already existing with the waterways and boundaries of the City of Belle Isle.

**NOW, THEREFORE, BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF BELLE ISLE THAT:**

**SECTION 1.**

Chapter 34: - WATERWAYS, shall hereinafter be amended to read as follows:

**ARTICLE III – BOAT EQUIPMENT AND OPERATION**



Section 34-37 - ESTABLISHMENT OF SPECIAL SLOW SPEED MINIMUM WAKE ZONES AND SPECIAL IDLE SPEED NO WAKE ZONES ON THE CONWAY CHAIN OF LAKES.

(a) Slow Speed and Slow Speed Minimum Wake Zones.

(1) Pursuant to Florida Administrative Code Chapter 68D-23.103, "Slow Speed" and "Slow Speed Minimum Wake" may be used interchangeably, and means that a vessel must be fully off plane and completely settled into the water and the vessel must then proceed at a speed which is reasonable and prudent under the prevailing circumstances so as to avoid the creation of an excessive wake or other hazardous condition which endangers or is likely to endanger other vessels or other persons using the waterway. At no time is any vessel required to proceed so slowly that the operator is unable to maintain control over the vessel or any other vessel or object that it has under tow. A vessel that is:

- a. Operating on plane is not proceeding at this speed;
- b. In the process of coming off plane and settling into the water or coming up onto plane is not proceeding at this speed;
- c. Operating at a speed that creates a wake which unreasonably or unnecessarily endangers other vessels or other persons using the waterway, or is likely to do so, is not proceeding at this speed;
- d. Completely off plane and which has fully settled into the water and is proceeding at a reasonable and prudent speed with little or no wake is proceeding at this speed

(2) The following described waterways or portions of waterways are hereby established as Slow Speed Minimum Wake zones within 300 feet of a confluence of water bodies presenting a blind corner and a bend in a narrow channel which causes an intervening obstruction to visibility and can obscure other vessels or other users of the "Narrows". The "Narrows" is subject to unsafe levels of vessel traffic congestion and this area has demonstrated through boating citations and other creditable data presents a significant risk of collision or a significant threat to boating safety:

- a. A boating safety zone from shoreline to shoreline, at the "Narrows" on Little Lake Conway, that extends 370 feet from a point at the dock located at 2042 Gatlin Avenue, Orland, FL (which is at 28.49159837,-81.35309159,26.83308085); 353 feet from a point of land located at 2054 Gatlin Ave., Orlando, FL (which is at 28.49177057,-81.35270102,23.6994152); 355 feet from a dock located at 2062 Gatlin Ave., Orlando, FL (which is at 28.49207672,-81.35238923); and 360 feet from a dock located at 4875 Murray Lee Lane, Orlando, FL (which is at 28.49221826,-81.35217949) to a line drawn perpendicular to the waterway and to points of land located at 4908 Oak Island Road, Belle Isle, FL (which is at 28.49123343,-81.35176158), as depicted in the attached Exhibit A.



b. A boating safety zone, along the shoreline on the Conway Chain of Lakes, incorporating one, continuous zone with no gaps, that begins at Venetian Boat Ramp (28.48156013,-81.3521391), continuing, with no gaps, incorporating the Canoe/Kayak Trail to the Warren Park Ramp (28.46175653,-81.34170848) to Perkins Ramp (28.45856914,-81.35330564) to Swann Beach (28.46017067,-81.35494271) to Delia Beach (28.46854215,-81.36023079) to La Belle Beach (28.47387299,-81.35906624) to Peninsular Beach (28.47078844,-81.35555146) back to Venetian Ramp, located within the City of Belle and the Sea Plane Bases (Lake Conway North 80-30; 28.47831446,-81.3659345 and Lake Conway South 80-20: 28.45473677,-81.34056525) as depicted in the attached Exhibit B and Exhibit C, respectively.

SECTION 2. All Ordinances or parts of Ordinances in conflict herewith are hereby repealed to the extent of such conflict.

SECTION 3. In the event a court of competent jurisdiction finds any part or provision of this Ordinance unconstitutional or unenforceable as a matter of law, the same shall be stricken and the remainder of the Ordinance shall continue in full force and effect.

SECTION 4. The keeper of Code of Ordinances for the City of Belle Isle is directed to include this Ordinance in the Code of Ordinances and may renumber and reclassify the same as may be required for inclusion in the Code of Ordinances of the City of Belle Isle.

SECTION 5. EFFECTIVE DATE. This ordinance shall take effect immediately upon its passage and adoption in the manner provided by law.

PASSED AND ADOPTED BY THE CITY COUNCIL OF THE CITY OF BELLE ISLE



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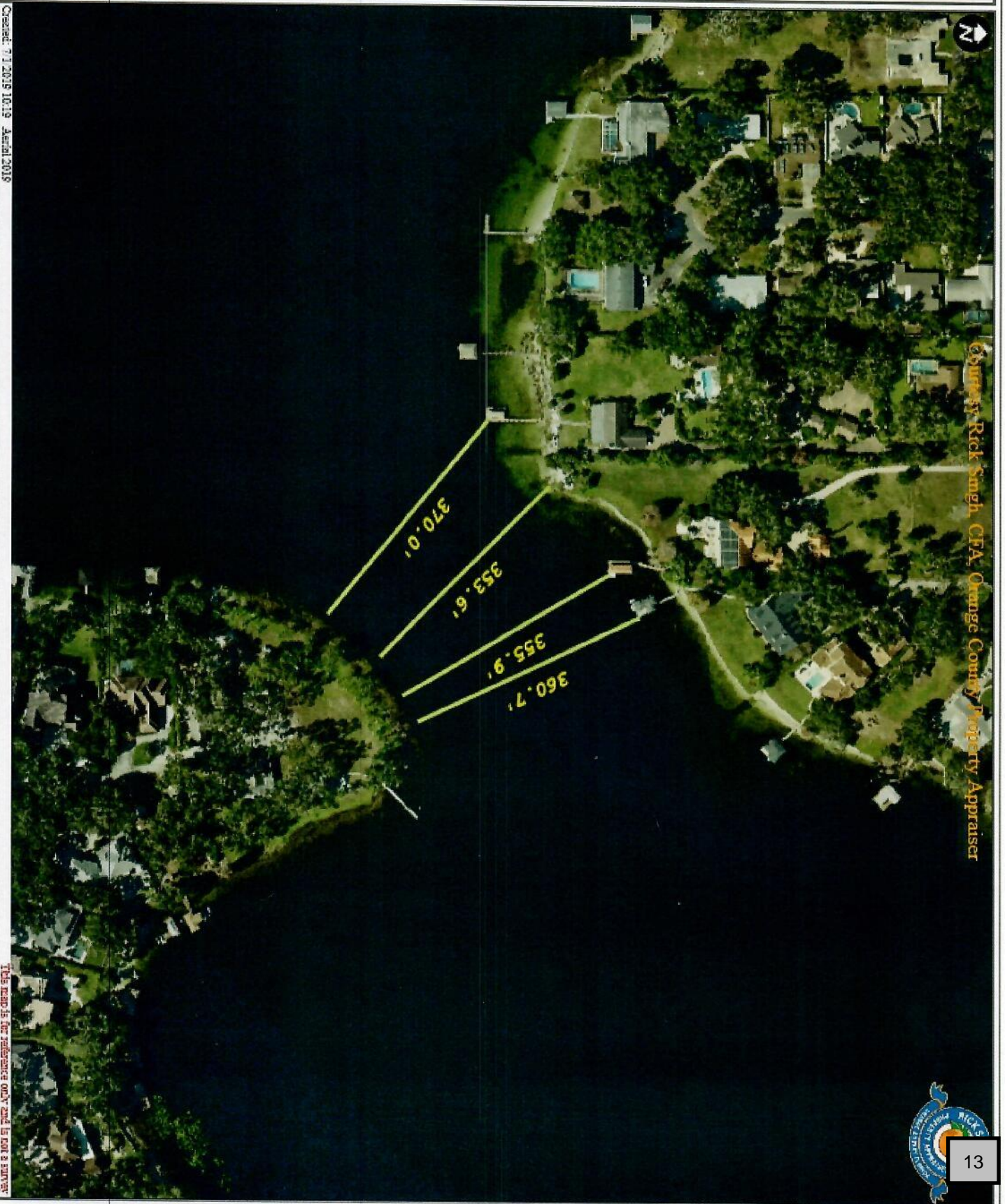
	Florida turnpike		Interstate 4		Toll Road		Major Road		Public Roads		Gated Roads		Road Under Construction		Proposed Road		US Road		State Road		County Road		Toll Ramp		Interstate Ramp		One Way		Brick Road		Rail Road		Proposed SunRail		Block Line		Lot Line		Residential		Agriculture		Commercial		Institutional		Governmental		Institutional/Religious		Commercial		Industrial/Vacant Land		Hydro		Waste Land		Agricultural/Cutlidge		County Boundary		Parks		Golf Course		Lakes and Rivers		Building		Power Plant
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Courtesy: Rick Smith, CFA, Orange County Property Appraiser



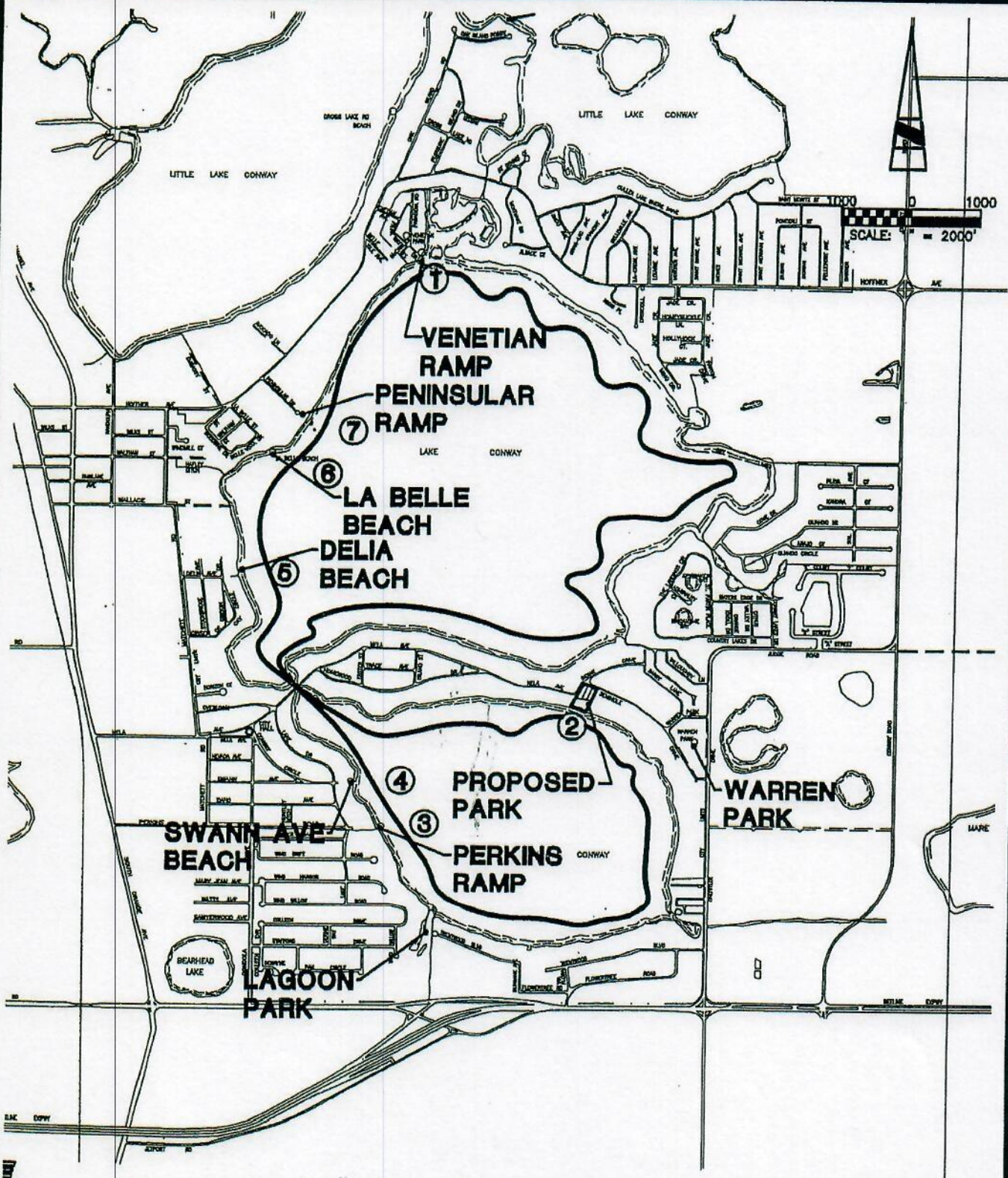
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This map is for reference only and is not a survey.





CITY OF BELLE ISLE  
CANOE TRAIL



**BOYLE**

FE-B52-001-02

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a.



July 22, 2019

Commissioner Jim Partin  
City of Belle Isle  
1600 Nela Avenue  
Belle Isle, FL 32809

Yolanda Quiceno  
City Clerk  
City of Belle Isle  
1600 Nela Avenue  
Belle Isle, FL 32809

Dear Commissioner Partin and Ms. Quiceno,

In accordance with the City of Belle Isle's Public Notice regarding the open seat on the City of Belle Isle's Planning and Zoning Board (District 6), I formally submit this Letter of Interest.

I am married, the father of a handsome four-year-old son and have lived in Belle Isle since 2010. I own my own small business, a law firm, and practice as a lawyer in the field of commercial and construction litigation. In my practice, we routinely handle matters that come into the purview of city government and city management, such as zoning disputes, construction-related problems such as traffic impacts, surveying and other property-related matters. I defend architects and engineers in disputes regarding their professional work, and frequently represent small businesses in all types of matters. I believe my experience in these matters make my potential inclusion on the Planning and Zoning Board a net positive for the issues that are currently before the board.

I can provide more information, including a CV, references, etc., upon request.

Thank you for your consideration.

Very Truly Yours,



Andrew P. Thompson



## CITY OF BELLE ISLE, FLORIDA CITY COUNCIL AGENDA ITEM COVER SHEET

**Meeting Date:** August 6, 2019

**To:** Honorable Mayor and City Council Members

**From:** B. Francis, City Manager

**Subject:** Final Draft of Transportation Master Plan

**Background:** The City Council held a workshop on April 9, 2019, to discuss a draft of the Transportation Master Plan. The council requested some changes to the plan which were made by the consultant. Those changes were as follows:

- Addressing limitations of right of way and city-county boundaries (as on pages iii-iv)
- Adjustments to the Hoffner widening discussion on page 1-8
- Adjustments to the Annexation map (Figure 4) on page 2-6
- Mention of Hoffner sidewalk ownership on page 2-7
- Clarification of sidewalk ownership on Daetwyler/Judge on page 2-7
- Addition of some discussion of McCoy signal timings and golf carts (low speed vehicles) on page 5-5
- Clarification of jurisdictional coordination at Gondola/Perkins on page 5-6
- Revised Recommendations Figures 19 & 20 (pages 5-7 & 5-8)
- Addition of textured pavement traffic calming discussion at Seminole/Nela on page 5-9
- Addition of raised crosswalk discussion on Nela Avenue on page 5-11
- Rewording of Pleasure Island Drive crosswalk (should FDOT keep the crosswalk) recommendation on page 5-12
- Notation of ROW and utilities impacts with Hoffner Path on page 5-13
- Notation of ROW and utilities impacts with Daetwyler/Judge path on page 5-15
- Discussion of possible Nela/Seminole path on page 5-15
  - Not recommended at this time, but discussed.
- Addition of language for signing and striping of shared use of bicycles on roadways noted for shared use on page 5-15
- Addition of footnote and clarification of Cost Estimate table (Figure 28) noting ROW, utilities, and other factors may fluctuate costs.

**Staff Recommendation:** Approve the final changes and adopt the Plan.

**Suggested Motion:** I move we approve the changes proposed and adopt the Transportation Master Plan.

**Alternatives:** Do not adopt the plan and request additional changes.

**Fiscal Impact:** \$75,000 for Plan. See attached plan for Project costs (page 6-1).

**Attachments:** Draft Transportation Master Plan





# CITY OF BELLE ISLE, FLORIDA TRANSPORTATION MASTER PLAN

# DRAFT

May 2019



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## EXECUTIVE SUMMARY

Belle Isle is a bucolic community set among the Lake Conway chain of lakes in the Orlando region. The City was first developed in the mid-1920s in order to protect the Lake Conway chain of lakes in resistance to Orange County's plan, at the time, to drain the lakes for farming. Although the City was annexed back into the County in the late 1920s, it was reconstituted in the mid-1950s as a part of another effort to save the Lake Conway chain of lakes. With a charter recognized by the state in 1972, the City has flourished ever since as a residential oasis in the flurry of metropolitan Orlando.

The lakes themselves, while providing a beautiful natural setting for the City, also create some barriers for getting around Belle Isle. East-west connectivity is limited to the Hoffner Avenue, Nela Avenue/Seminole Road, and McCoy Road corridors.

North-south connectivity is further limited by residential development, pushing most north-south movements along the edge of, or beyond the city limits. The contrast of the beautiful natural setting and quiet residential streets with the limited east-west and north-south connectivity provides a challenging setting for Belle Isle residents to retain the bucolic environment they enjoy, while allowing the mobility and growth that the City needs to advance.

Overall, Belle Isle's transportation network presents several key challenges focused around the limitations presented by existing constraints and city limit boundaries. Particular challenges include:

- Limited connectivity in both the east-west and north-south directions minimizes redundancy in the network and makes some trips indirect. This limits low-stress routes for bicyclists, increases pedestrian travel distances, and constrains route choice for drivers including flexibility to adjust to network disruptions or changes in circulation patterns.
- Limited access and egress points to a number of communities.
- Natural and built environments limit opportunities to increase access across and within many communities.



- Limitations of the existing right-of-way available for transportation infrastructure expansion.

Continued success of the City requires the management and appropriate accommodation of regional travel demands while retaining local access within the City for its residents. As a part of this, Belle Isle must have a well-integrated multimodal transportation plan for the city. With only a handful of streets that provide continuous connectivity through the City and to destinations beyond, the City is challenged in providing an equally high level of accommodation for all the modes and demands that can fit within the right-of-way.

**Multimodal** refers to the availability of multiple transportation options within a system or corridor.

**Mode** refers to the different means of travel such as automobile, bicycle, transit or walking.

The plan ensures that, to the extent feasible, most streets are safe for all users and that, taken as a whole, the City provides attractive and efficient travel corridors for each mode. In this way, the City is able to provide streets for everyone and a network for all. The plan is comprised of separate modal networks for pedestrians, bicycle travel, and principal vehicle corridors. When reassembled into a composite system, the multimodal network identifies modal emphasis on some corridors in order to provide a quality system for each mode of travel and complete streets for every mode.

Within Belle Isle there are four corridors of citywide or regional significance: Hoffner Avenue, Nela Avenue/Seminole Drive, Daetwyler Drive/Judge Road, and McCoy Road. While there are many details to the corridor recommendations, at a high level, each corridor should provide for safe and continuous pedestrian travel while some corridors are recommended to provide enhanced accommodation for certain modes:

- Hoffner Avenue is a principal vehicular corridor that must efficiently provide for private autos, local bicycle and pedestrian traffic. The key objective is a smooth flow of vehicles through metering (managing the volume of vehicles entering the corridor) and speed management. Pedestrians will be accommodated at safe crossings and an enhanced multi-use path is designed to accommodate pedestrians as well as bicyclists.
- Nela Avenue/Seminole Drive are neighborhood streets that, together, provide the only east/west connectivity through the City between Hoffner Avenue and McCoy Road. Vehicular traffic should be smooth and even, but maintained at lower speeds within the City. Strategically added curb extensions manage traffic speeds, improve safe operations, and enhance street character.
- Daetwyler Drive/Judge Road provide a principal vehicular corridor on the east side of the City. The plan will emphasize pedestrian and bicycle use providing enhanced bicycle and pedestrian facilities in the form of a multi-use path. Additionally, right-of-way newly acquired by the City will allow for the extension of turn lanes at the Judge Road/Conway Road intersection for improved traffic flows.
- McCoy Road is also a principal vehicular corridor providing bicycle and transit connections and high capacity vehicle flow along the southern edge of the City. While regional traffic flow will continue to be accommodated along McCoy Road, improvements to the pedestrian and bicycle facilities will allow for safer

connectivity for non-auto modes along the corridor within the City. Additionally, continued coordination with FDOT for improved signal timings will allow for more regular traffic flows.

This plan proposes a complete, connected network for pedestrians, low stress/"family-friendly" travel corridors, commuter bicycle networks, transit, and vehicles. Together, these networks provide a composite multimodal network plan while supporting the goals of the plan itself, as follows:

- *Transportation is safe for all residents and visitors whether driving, walking, or bicycling*
- *Residents can drive between destinations with minimal traffic congestion*
- *Residents can comfortably walk and bicycle to parks, schools, transit, and shopping areas.*
- *Belle Isle's streets are attractive and contribute to the beauty of the community.*



Many actions that will improve transportation in Belle Isle result from changes in policies and management rather than from capital improvement projects. The following recommendations will help guide predictable and sustainable growth and development in Belle Isle:

- **Adopt a Complete Streets Policy** – Every street within Belle Isle must safely accommodate all users. While this does not mean that every street will have a bicycle lane or sidewalks on both sides of the street, it does mean that every



street project – whether significant maintenance, retrofit, or new construction – must consider and provide for the needs of all users. Adequate accommodation ensures users may access and travel on the street, regardless of age or ability, with a reasonable assumption of safety and protection. The policy provides a clear process for seeking and documenting any necessary exceptions.

- **Establish Sidewalk and Pedestrian Accessibility Policies** – Walking is the most fundamental mode of transportation and the basis for nearly all others. In general, pedestrians are only safe to mix with motor vehicle traffic when streets are explicitly designed and/or actively managed with this intent. In all other cases, pedestrians, and especially vulnerable pedestrians such as children, seniors, and persons with disabilities, require and deserve the protection and accessibility provided by a complete sidewalk network and associated curb ramps and crosswalks. New streets, regardless of volume, should provide continuous, connected sidewalks on both sides of the street. For existing streets, sidewalks should be considered on at least one side for streets carrying daily vehicle volumes below 5,000. On moderate volume streets (5,000 to 10,000 vehicles per day), sidewalks are recommended on both sides of the street whenever possible and required on high volume streets – those with daily vehicle volumes in excess of 10,000 vehicles per day. Sidewalks should be provided with any significant street maintenance, rehabilitation, or reconstruction project and may be constructed independent of a street project.
- **Adopt Bicycle Supportive Policies and Services** – A number of different factors support bicycling as a viable mode choice: adequate provision of bicycle parking, bike share, bicycle facilities, and building amenities for bicyclists. Transportation demand management policies further support bicycle mode share. It is recommended that Belle Isle support bicycling through the adoption of the proposed complete streets policy, expanded bicycle education, improved bicycle facilities, and increased awareness of the bicycle network. In addition, the City will continue to consider the addition of more bicycle accommodations into the Zoning code to include secure bicycle parking, repair facilities, and user amenities (i.e., showers and lockers) in nonresidential uses.
- **Regional Coordination of Multimodal Facilities** – Providing connectivity to regional transportation facilities requires coordination with other government entities. Potential coordination opportunities include sidewalk connections to the SunRail station and other destinations outside the city limits on Orange Avenue and coordination with other jurisdictions on trail and bicycle planning including the Orlando Bicycle Plan update occurring in 2019 and any future updates of the Orange County Trails Master Plan. Regular participation and cooperation with MetroPlan Orlando, Orange County, FDOT, the Cities of Orlando and Edgewater, and other nearby governmental entities can further the visibility of Belle Isle's priorities.



# 1 BACKGROUND OVERVIEW

## RELEVANT STUDIES AND PLANS

Current transportation planning efforts in Belle Isle exist within a broader planning context that has evolved over time. This chapter reviews seven planning documents that provide regional context and impact transportation planning efforts in and around Belle Isle:

1. City of Belle Isle Comprehensive Plan
2. Orange County Multimodal Corridor Plan
3. Orange Avenue Corridor Study
4. Orange Avenue Corridor Study: Preliminary Engineering Concept
5. Orange County Infill Master Plan
6. Orange County Pine Castle Urban Center
7. Sand Lake Road SunRail Station Area Bicycle & Pedestrian Connectivity Study

## Key Findings

### Plans & Studies Review

The following are the key themes from the review of previous plans and studies:

- **Multimodal Networks.** Orange County is developing several projects and initiatives to create a more complete multimodal transportation network. The most relevant of these projects to Belle Isle stakeholders include the Orange Avenue Corridor Study and the Hoffner Avenue (SR-15) roadway widening project just east of the municipal border. The Hoffner Avenue road widening will add bike lanes and sidewalks to the multimodal network and is under construction as of the date of this Master Plan. Orange Avenue is undergoing redesign, between Sand Lake Road and Hoffner Avenue, from a primarily suburban, auto-oriented roadway to a more urban multimodal arterial that meets the needs of people walking and biking.
- **Safety for People Walking and Biking.** Walking and biking in the areas immediately surrounding Belle Isle are generally perceived as inconvenient and unsafe, and a more complete network of safe walking and biking routes is desired surrounding the City. There is strong stakeholder interest in traffic calming and streetscape improvements on Orange Avenue between Lancaster Road and Hoffner Avenue, where commercial development is more prominent. The Orange Avenue redesign project is expected to remedy key barriers to

multimodal mobility in the area, such as improved sidewalks, traffic calming, on-street bike lanes, and safer pedestrian crossings. These improvements are expected to have significant impacts to residents of Belle Isle, particularly should the City opt to annex property in this area.

- **Transit-oriented Development.** Should the City annex the area around the Sand Lake Road SunRail station, significant real estate value for the City of Belle Isle could be created along with associated property tax revenue. However, land values near the station have underperformed in the past decade. This is partly due to restrictive nearby County zoning codes that limit the density of development, and partly due to poor pedestrian and bike access to the station. To overcome these barriers to development, Orange County has proposed changes in the development approach to the Orange Avenue corridor with higher-intensity development near Sand Lake Road SunRail station as part of Pine Castle Urban Center enabled by updated future land use and zoning.
- **Connections to SunRail.** The Sand Lake Road SunRail station has limited connectivity with the surrounding street network and features low population/employment density and land use diversity. As a result, few destinations are accessible within walking distance of the station, and first/last-mile gaps between the station and user's origins/destinations are a major barrier to transit ridership.

## PLANS & STUDIES REVIEW

This section describes the findings from eight planning documents that have implications for multimodal transportation in and around Belle Isle. These documents include a mix of plans from Orange County, Florida Department of Transportation (FDOT), and the City of Belle Isle. Each plan relates to different components of the overall transportation network, in different planning jurisdictions, and in different planning horizon timeframes. Collectively, the backgrounds and key findings from these plans create the regional and local context of transportation in Belle Isle. Included below are descriptions of each study's purpose, a brief summary, and key recommendations.

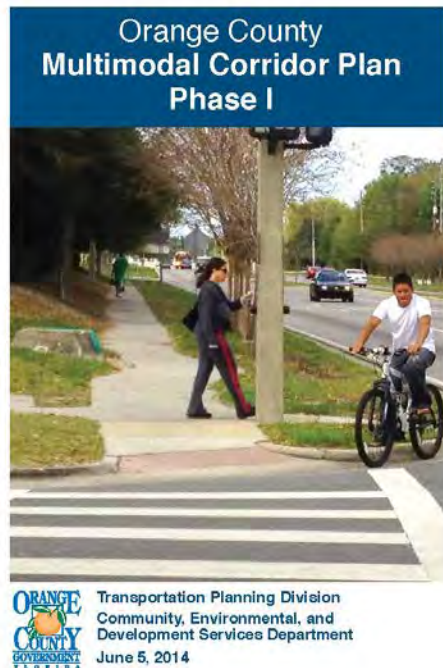
### City of Belle Isle Comprehensive Plan

The City of Belle Isle published their Comprehensive Plan in 2010, succeeding the 1991 Comprehensive Plan, and is due for review. The Transportation Element of the Comprehensive Plan was intended to establish guidelines, policies, and performance metrics for the transportation system in Belle Isle. One aim of components within the Element was to integrate land use and transportation planning processes in a coordinated manner.

The Comprehensive Plan used the Level of Service (LOS) metric to ensure that land use and transportation policies are coordinated. The document advised against approving any development that would cause roadways to function with greater vehicle delay than the LOS standards the City set for them. Objective 1.3 of the Comprehensive Plan established a minimum standard of operations for all City streets at LOS "C." County and State roads must operate at LOS "E" or better. McCoy Road was an exception, noting that it must operate at traffic volumes of less than 3,530 vehicles per peak hour.

The Comprehensive Plan called for improvements to infrastructure for people walking, biking, and taking transit. In Objective 1.4, the City required all new developments to include sidewalks and all new roads to include on-street bike facilities. Bus pullouts were also required on major arterials to prevent traffic congestion from bus stop activity.

## Orange County Multimodal Corridor Plan: Phase I



Orange County developed the Orange County Multimodal Corridor Plan: Phase I and adopted the plan in 2014. The document has an overall focus on the County's current and future multimodal network needs from land use, transportation, and capital planning perspectives. The Multimodal Corridor Plan builds on initiatives such as the interconnected regional trail network and SunRail commuter rail line, with the goal of creating a transportation network for Orange County consistent with the MetroPlan Orlando 2040 Long Range Transportation Plan.

The nearest major project to Belle Isle is the FDOT-funded reconstruction of Hoffner Avenue/SR 15, east of Conway Road, which is currently under construction. According to FDOT, the project involves "widening and reconstruction of SR 15 (Hoffner Avenue)

from a two-lane roadway to a four-lane divided roadway. The project will create a curb and gutter roadway with two 11-foot travel lanes, a 4-foot bike lane, a 5-foot sidewalk in each direction separated by a 22-foot grassed median."

## Recommendations

- Some recommendations focused on developing policies that make streets accommodate more users, such as potential adoption of an Orange County Complete Streets Policy and further studying existing on-street parking system and implementation measures in County roadways (e.g. traffic calming).
- Evaluate municipal zoning ordinances to recommend sidewalk widths and pedestrian connectivity improvements
- Consider re-evaluating posted speed limits on County roadways with posted speeds above 45 miles per hour, based on crash data analysis
- Compile and collect data to assist pedestrian and bicycle planning. For example, install pedestrian counters in key locations, create GIS layers of available current pedestrian and bicycle counts and turning movement counts and compile GIS inventory of multi-purpose paths to identify further path connections

- Review recommendations of MetroPlan Orlando's Bike Sharing Working Group for potential bike share and car share programs

## Orange Avenue Corridor Study

The Orange Avenue Corridor Study (2014) was developed for FDOT and focused on identifying a range of multi-modal solutions to improve walkability, safety, quality of life, and mobility. The study also sought to advance the long-term vision for the Orange Avenue corridor and encourage outcomes supportive of long-term transit-oriented development plans because the corridor is the primary access route to the Sand Lake Road SunRail station. The study focused on two segments of Orange Avenue: Sand Lake Road to Lancaster Road and Lancaster Avenue to Hofner Avenue.



The study noted the following transportation challenges and plans for the corridor:

- There were gaps in multimodal network along Orange Avenue and connecting streets with missing sidewalks, overgrown vegetation on paved shoulders, bus stops without shelters and not setback from the sidewalk, and a lack of bike facilities.
- The most common crash locations on the corridor were Orange Avenue/Sand Lake Road, Orange Avenue/Lancaster Road, and Hansel Avenue/Hoffner Avenue. The segment between Sand Lake Road and the SunRail station entrance driveway had a particularly high crash frequency.
- Orange Avenue is one of LYNX's (transit agency) priority corridors, putting it in the running for transit signal priority and queue jumps along the corridor to strengthen bus service and the connection between bus and rail (San Lake Road SunRail Station).
- Walking and biking in the area were generally perceived as inconvenient and unsafe. A more complete network of safe walking and biking routes was desired. There was strong stakeholder interest in traffic calming and streetscape improvements along the northern segment of Orange Avenue between Lancaster Road and Hoffner Avenue, in an area known as Pine Castle's "Main Street."
- This area was also home to several educational institutions like CCA, Pine Castle Christian Academy, and Pine Castle Elementary School, which generated higher levels of walking and biking.



Multimodal improvements the study recommended include:

- Pedestrian connectivity with the installation of more crosswalks at places such as Office Court & Orange Avenue to improve pedestrian crossing at Sand Lake Road SunRail station.
- Add pedestrian bulb-bouts, “green lanes” for bikes, raised medians, and sidewalk enhancements along Orange Avenue/Hansel Avenue north of Lancaster Road
- Remove right-turn channelization at Orange Avenue & Sand Lake Road to reduce pedestrian crossing distances
- Add raised median, east side sidewalks, and on-street bike lanes between Lancaster Road and Sand Lake Road to provide safer spaces for people biking and walking
- Refine bus stop locations so they provide better access to entrances of major destinations and provide enhanced bus stop amenities like seating, shelters

## Orange Avenue Preliminary Engineering Concept Plan



The Orange Avenue Preliminary Engineering Concept Plan was published in 2015. The focus of this study was to develop a preliminary engineering concept plan for the segment of the corridor from Parkline Boulevard to Lancaster Road, based on urban cross-sections in the FDOT Orange Avenue Corridor Study. The preliminary engineering concept was intended to transform Orange Avenue from a more rural, auto-oriented arterial to a more

urban multimodal arterial. The concept also prepared the project for the funding of the next phases of the project, design and construction. The design incorporated Transportation Design for Livable Communities (TDLC) standards.

Orange Avenue runs parallel to the SunRail line and is the primary arterial in the area. The corridor services large trucks and high-volume traffic, making its transition to a walkable, bike-friendly corridor challenging. The challenges for people walking and biking along the corridor include:

- limited places to cross the corridor
- incomplete sidewalk network
- bus stops at midblock.

The engineering concept contained the design recommendations along the Orange Avenue corridor to make walking and biking along Orange Avenue safer, more

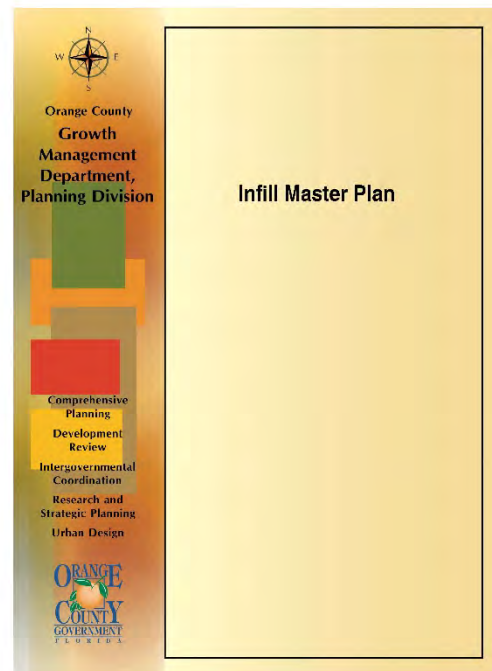
comfortable and more convenient by adding bike lanes and making shorter pedestrians crossings (among other recommendations). The plan also suggested creating landscaped pedestrian plazas at each corner of Sand Lake Road/Orange Avenue to transform this intersection into a “gateway” to the communities of Belle Isle and Pine Castle.

## Orange County Infill Master Plan

The Infill Master Plan (2008) by Orange County made recommendations to update the County’s Comprehensive Policy Plan and local zoning ordinances to facilitate infill development. Orange County recognizes infill development as a potential strategy to make more efficient use of existing public infrastructure, accommodate growth in population and employment, provide affordable housing, facilitate safe walking and biking conditions, and mitigate traffic congestion.

The County proposed infill development on various underutilized sites and corridors based on their location within designated urbanized areas and within ¼ mile of major streets and LYNX bus routes. One of the County’s five priority corridors for infill development was in Pine Castle, around a section of Orange Avenue, denoting properties in and around the intersections of Sand Lake Road/Orange Avenue and Orange Avenue/Lancaster Road. The Orange Avenue corridor received high scores for its infill potential due to its proximity to the Sand Lake Road SunRail station, which could promote transit-oriented development. The corridor received poor scores for the quality of existing pedestrian infrastructure and its ability to serve non-driving trips. Some of the recommendations the County made include:

- Prioritize infill based on the age, capacity of existing infrastructure (e.g. roadways)
- Adopt infill development guidelines – increased density, reduced parking ratios, pedestrian-oriented frontages
- Administer a survey of business owners of underutilized parcels
- Amend Comprehensive Policy Plan and zoning ordinances to incentivize infill
- Create infill incentives and certification programs – e.g. tax credits, reduced impact fees, brownfield development program, density bonuses, workforce housing tax credits/density bonuses.



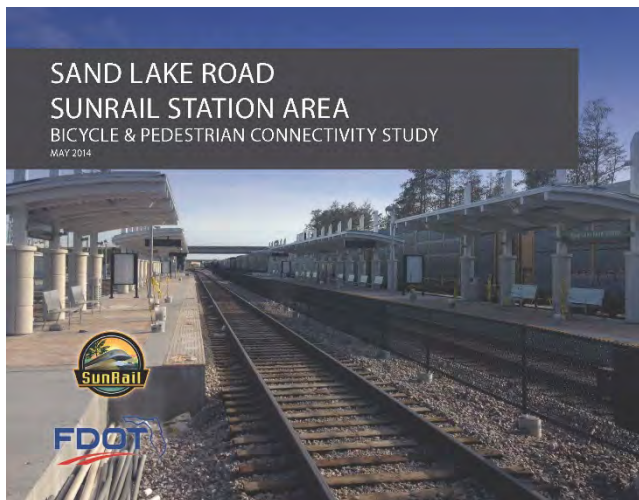
## Orange County Pine Castle Urban Center

In 2017, Orange County transmitted updates to the Orange County comprehensive plan that would promote transit-oriented development near the Sand Lake Road SunRail

station. The new land use are Urban Core, Urban Center Place Type, which allowed for transit-oriented development within ½ mile of a transit station (new or existing).

The Urban Center future land use is further divided into subdistrict. Areas along the Orange Avenue Corridor fall into the “Core” transect, allowing residential densities between 21 and 150 dwelling units per acre, depending on the site’s proximity to the Sand Lake Road SunRail station. The previous future land use for the Orange Avenue corridor was the Low-Medium Density Residential (LMDR) zoning designation, permitting up to 10 dwelling units per acre. The Urban Center Place Type overlay is accompanied by proposed form-based code standards divided into “transects” or zones of varying density. Orange County staff recommended the adoption of the revised form-based code for the Pine Castle area, and in particular the designation of the Orange Avenue corridor as an Urban Center.

### **Sand Lake Road SunRail Station Area Bicycle & Pedestrian Connectivity Study**



The Sand Lake Road SunRail Station Area Bicycle & Pedestrian Connectivity Study (2014) was developed for FDOT and SunRail. FDOT initiated this study to coordinate efforts to improve bike and pedestrian connectivity to SunRail stations. The study’s focus was on short-term improvements that could be advanced through FDOT as well as municipal/local capital improvements programs. The study evaluated bike and pedestrian infrastructure needs and identified key projects for implementation

through planning and concept development. Findings from previous studies and audits were considered of transit, bike, and pedestrian accessibility as well.

General objectives of these projects included:

- Filling crucial first/last-mile gaps between SunRail stations and area employers, schools, and other destinations;
- Addressing pedestrian and bike safety issues (e.g. lighting, accessibility issues, or crossing distances);
- Other low-cost, high-value improvements such as signal re-timing, bus stop relocation, or lane striping.

The study assessed each SunRail station area’s transportation and land use characteristics according to metrics such as street connectivity, residential and employment density, and land use diversity. The Sand Lake Road SunRail station scored poorest of all 12 stations in the system on all metrics except employment and land use diversity, for which only two other stations performed worse.



## Recommendations

The study identified several high-priority bike and pedestrian improvements to enhance access to Sand Lake Road SunRail station, including:

- Fill in gaps of the sidewalk network along Orange and Nela Avenues and Office Court. Also, create bike and pedestrian connections along Sand Lake Road on the bridge over the SunRail tracks by installing dedicated space for people to walk or bike over the bridge or constructing a bike/ped bridge over the tracks).
- Improve pedestrian safety at intersections. In interim, add signage “Turning Vehicles Yield to Pedestrian” and yield line markings. Install detectable warning surfaces on all corners of the intersection of Orange Avenue/Nela Avenue to comply with ADA requirements.
- Support bike riding along local, low-speed roads intended as bike boulevards, such as Nela Avenue, Perkins Road, and Gondola Drive.

## OTHER MISCELLANEOUS PLANS & STUDIES

### Orange County District 3 Future Roadway Projects

The Orange County Future Roadway Map, updated July 2018, shows the progress of several pending roadway redesigns in and around Belle Isle. The Orange Avenue redesign is scheduled to begin construction in October 2018 between Pineloch Avenue and Grant Street, north of the City. The segments between Pineloch Avenue and Hoffner Avenue, as well as Hoffner Avenue and Parkline Drive, are shown as having completed the planning stages. However, no date of construction for these segments has been scheduled.

### FDOT Hoffner Widening Plan at Conway

Hoffner Avenue (SR-15) has been recently widened from Conway Road to Mauna Loa Lane, from a two-lane collector to a five-lane arterial with two lanes in each direction and a center turning lane. The design also includes accommodations for an on-street bike lane in each direction. The project area lies immediately to the east of Belle Isle’s jurisdictional border.

### SandLake Station Development Plans

A local developer, Sandlake Station Partners LLC, has proposed a mixed-use development at 7803 Orange Avenue, including 38,000 square of commercial space and 196 rental units. If approved by the Orange County Development Review Committee, the project would constitute the most significant transit-oriented development in the area, just ¼ mile from the Sand Lake Road SunRail station.

## 2 GETTING AROUND BELLE ISLE TODAY

Belle Isle is a bucolic community set among the Lake Conway chain of lakes in the suburban ring of metropolitan Orlando. The City was first developed in the mid-1920s in order to protect the Lake Conway chain of lakes in resistance to Orange County's plan, at the time, to drain the lakes for farming. Although the City was annexed back into the County in the late 1920s, it was reconstituted in the mid-1950s as a part of another effort to save the Lake Conway chain of lakes. With a charter recognized by the state in 1972, the City has flourished ever since as a residential oasis in the flurry of metropolitan Orlando.

Belle Isle is bordered by the City of Orlando and the unincorporated community of Conway on the east, the City of Edgewood and the unincorporated community of Pine Castle on the west, and unincorporated areas of Orange County in the North and South.



The lakes themselves, while providing a beautiful natural setting for the City, also create some barriers for getting around Belle Isle. East-west connectivity is limited to the Hoffner Avenue, Nela Avenue/Seminole Road, and McCoy Road corridors while north-south connectivity is further limited by residential

development, pushing most north-south movements along the edge of, or beyond the city limits. The lakes themselves also provide an additional mode of transportation for those that own private boats, however, the privatized nature of the lakeshores restricts much of the ability for those living off of the lakes to use this as a mode of transportation without using a privately owned dock. The contrast of the beautiful natural setting and

quiet residential streets with the limited east-west and north-south connectivity provides a challenging setting for Belle Isle residents to retain the bucolic environment they enjoy, while allowing the mobility and growth that the City needs to advance.

Overall, Belle Isle's transportation network presents several key challenges:

- Limited connectivity in both the east-west and north-south directions minimizes redundancy in the network, constraining travel choice in terms of routes available, and flexibility to adjust to network disruptions or changes in circulation patterns.
- Limited access and egress points to a number of communities.
- Natural and built environments limit opportunities to increase access across and within many communities.
- Limitations of the existing right-of-way available for transportation infrastructure expansion.

In order to assess the challenges faced in traveling around Belle Isle, the Transportation Master Plan is informed by an understanding of local population and the existing transportation setting. The following sections define Belle Isle's demographics, roadways, and transit and multimodal networks serving local residents, employees, and visitors.

## DEMOGRAPHICS

The demographics of Belle Isle paint a picture of the suburban community that it is, as noted below on Figure 1. Incomes are over 60% higher than Orange County on average, which is reflected in not only lower rates of residents below the poverty line and without insurance, but also in higher number of residents with two or more vehicles available. Single occupant vehicle commuting patterns, however, are similar to those noted on average in Orange County.

## Commute Patterns

Approximately one-third of residents of Belle Isle work in Orlando, with the most prominent commute travel flows to the southwest Orlando tourist areas and Downtown Orlando, as shown on Figure 3. As most residents are employed outside of the City of Belle Isle itself, few residents commute within the City.

## Annexation Plan

The City has conducted a study around possible annexing certain parcels of land directly adjacent to its current borders. As shown on Figure 4, the three different areas lay on the east and southwestern borders, the southeastern corner, and a section along the northeastern border. Most notable among these annexation areas is the SunRail station located at the corner of Orange Avenue and Sand Lake Road, and section of Orange Avenue due north. Both the station and corridor are prime candidates and have been studies for transit-oriented development and multi-modal improvements.



Figure 1 Belle Isle Demographics<sup>1</sup>

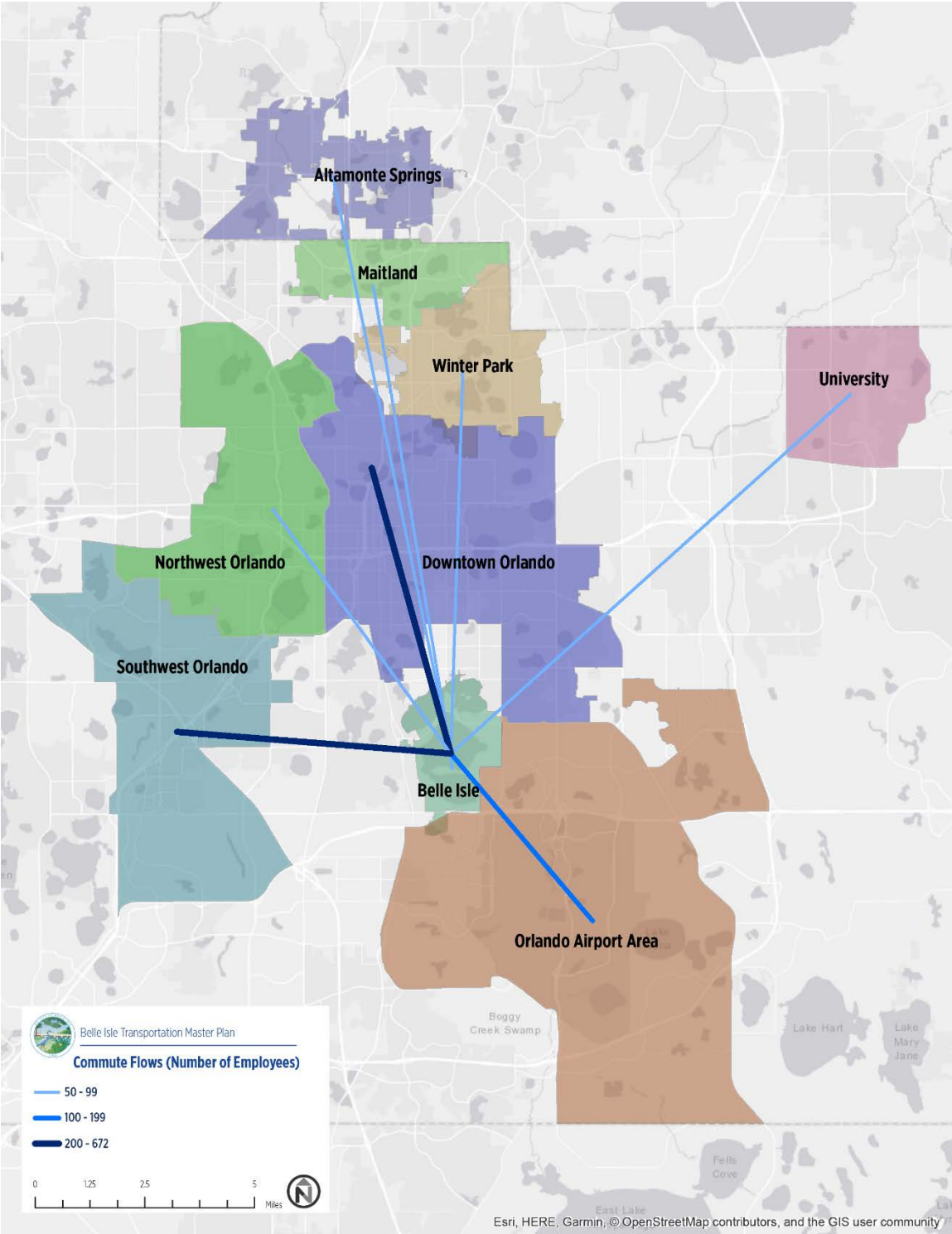
Demographic	Belle Isle	Orange County
Population	6,686 residents	1,290,216 residents
Households	2,750 units	517,631 units
Median Household Income	\$84,145/year	\$51,586/year
Median Age	49	35
Commuting		
Drive Alone	81%	79%
Carpool	5%	10%
Transit	0%	3%
Walk	1%	1%
Work from Home	10%	5%
Other Means	3%	2%
Auto Ownership		
No Vehicles	5%	6%
1 Vehicle	22%	39%
2 Vehicles	44%	40%
3+ Vehicles	29%	15%

<sup>1</sup> US Census Data

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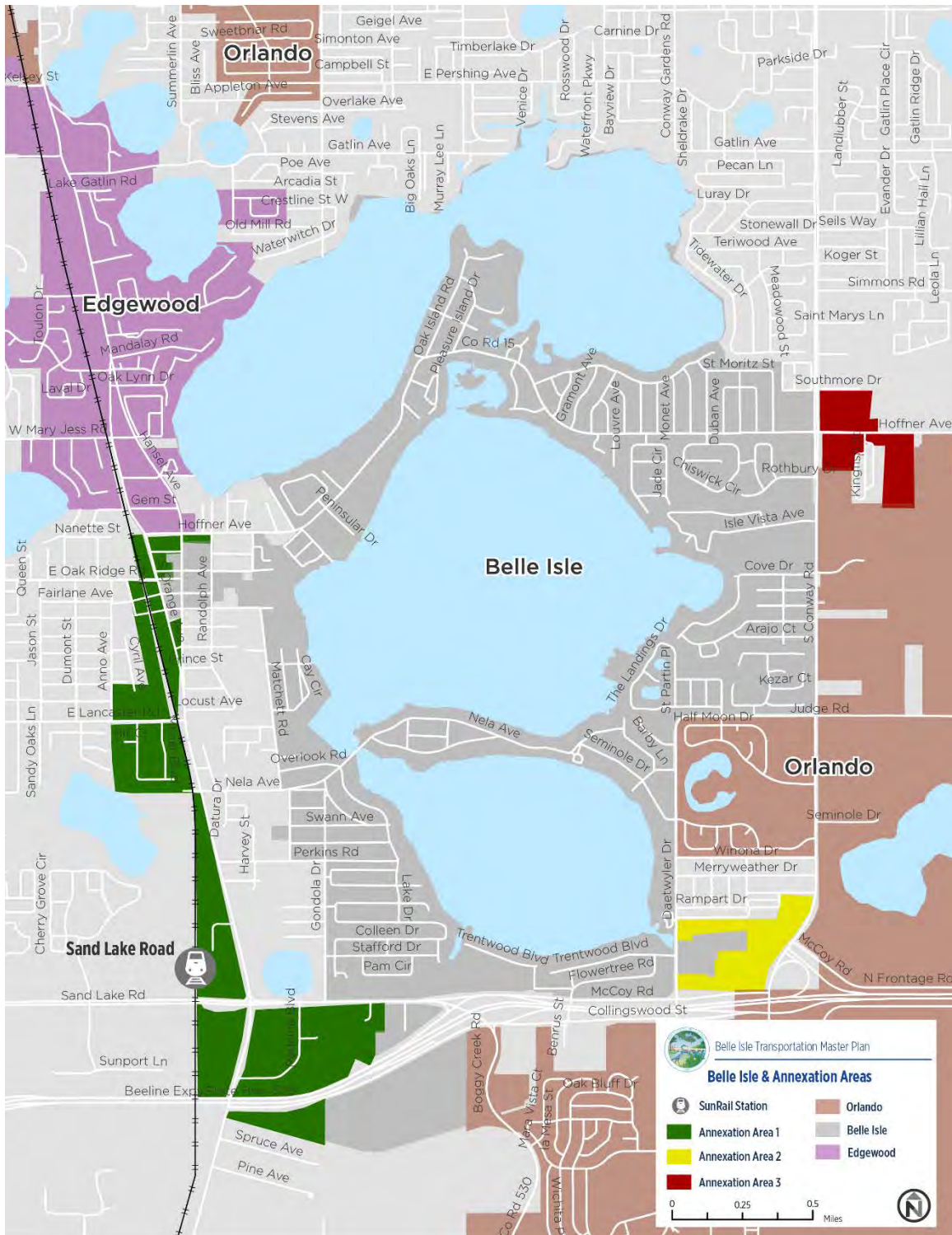


Figure 3 Belle Isle Commute Travel Flows





**Figure 4 Belle Isle Proposed Annexations**



## ROADWAY NETWORK

While Belle Isle contains an intricate network of neighborhood streets, within Belle Isle there are four corridors of citywide or regional significance: Hoffner Avenue, Nela Avenue/Seminole Drive, Daetwyler Drive/Judge Road, and McCoy Road. Other streets within Belle Isle provide some additional connectivity within the City and Orange Avenue/Hansel Avenue and Conway Road provide north-south connectivity immediately adjacent to the western and eastern boundaries of the City, respectively. Below is a discussion of the four primary corridors within the City itself.

- **Hoffner Avenue** is classified by FDOT as a minor rural arterial, which as a principal vehicular corridor not only for local Belle Isle traffic, but also east-west traffic in the immediate vicinity. The paved width itself is owned and maintained by Orange County, however, the remaining right of way is owned and maintained by the City of Belle Isle. Average Daily Traffic (ADT) counts reflect this, with the corridor accommodating 16,959-18,008 vehicles per day<sup>2</sup> within 2.3 mile section of Hoffner Avenue within the City of Belle Isle (from Conway Road to LaBelle Street). The two-lane roadway (with some intermittent turn lanes) varies from 22 to 23 feet in paved width with sidewalks varying from 6 to 8 feet in width for the length of the south side and portions of the north side of the roadway. While most of the sidewalks along Hoffner Avenue are within the public right of way, it is unclear whether the sidewalks along the central portion of the corridor (between Venetian Drive and Avocado Lane) are on private property with or without easements. There are no stop controlled or signalized intersections between Conway Road and Orange Avenue and there are marked crosswalks at two locations within the City of Belle Isle. The roadway's speed limit is primarily signed at 35 miles per hour (mph), with a signed short 25 mph segment as the roadway curves through the isthmus separating Lake Conway from Little Lake Conway. Rights of way are very wide for the eastern segment of Hoffner Avenue at approximately 100 feet in width, but narrow considerably along the western segment of Hoffner Avenue to the 22-23 foot width of pavement<sup>3</sup>.
- **Daetwyler Drive/Judge Road** are together classified by FDOT as a major rural collector, providing a principal vehicular corridor on the east side of the City. While similarly classified by FDOT as Nela Avenue/Seminole Drive, these roadways provide a bit more connectivity for Belle Isle and surrounding residents, many times functioning as an alternate route for traffic travelling between Semoran Boulevard and points further east and McCoy Road. The paved width of the roadway has been recently acquired by the City of Belle Isle from Orange County, extending the City's boundary to the southern/eastern edge of pavement of the corridor. ADT counts reveal that the corridor accommodates 13,116-15,151 vehicles per day<sup>4</sup>. As two-lane roadways (with some intermittent turn lanes) that vary from 22 to 24 feet in paved width within 65 to 75 feet of right of way, both roadways have 5 to 6 foot wide sidewalks for most of the length of both sides of the roadway. However, only the sidewalks along the northern side of Judge Road and eastern side of Daetwyler Drive fall within the Belle Isle city limits.

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<sup>2</sup> 2017 Annual Count Report, Orange County Traffic Engineering Department

<sup>3</sup> It is unclear whether sidewalks in the segment between Venetian Drive and just south of Avocado Lane are within the public right of way or located on private property.

<sup>4</sup> 2017 Annual Count Report, Orange County Traffic Engineering Department



The roadway's speed limit is signed at 35 mph with no stop controlled intersections between Conway Road and McCoy Road.

- **Nela Avenue/Seminole Drive** are together classified by FDOT as a major rural collector. Functionally, these are neighborhood streets that, together, provide the only east/west connectivity through the City between Hoffner Avenue and McCoy Road. ADT counts reveal that the corridor accommodates 2,731-2,799 vehicles per day<sup>5</sup>. As



Figure 5 Nela Avenue/Homewood Drive Intersection Treatment



Figure 6 Nela Avenue/Overlook Road/Lake Drive Intersection Treatment

Nela Avenue in three locations between the eastern and western Homewood Drive intersections.

two-lane roadways that vary from 18 to 20 feet in paved width in 35 feet of right of way, both roadways have 5 foot wide sidewalks for most of the length of the south side and small segments of sidewalk on the north side of the roadway. The roadway's speed limit is signed at 25 mph and features stopped controlled intersections throughout its length. Intersection treatments are present at Lake Drive/Overlook Road, which is designed as a hybrid roundabout, and at the western Homewood Drive intersection, which has raised pavement diverters in place. Additionally, Nela Avenue crosses a historic bridge over Lake Conway that provides limited pavement width and a narrow sidewalk. Finally, speed humps are installed along

<sup>5</sup> 2017 Annual Count Report, Orange County Traffic Engineering Department



- **McCoy Road** is classified as a major rural collector east of Boggy Creek Road and a minor rural arterial west of Boggy Creek Road, providing a principal vehicular corridor with high capacity vehicle flow along the southern edge of the City. Between Conway Road and via Flora, McCoy Road is an Orange County roadway, functioning similar to an access roadway for the Beachline Expressway and providing two-way traffic in a three lane, approximately 36 foot paved width cross section with sidewalks along most of the northern side of the road. Between Via Flora and Boggy Creek Road, McCoy Road is an FDOT roadway, becoming one-way westbound as a ramp from the westbound Beachline Expressway merges into it and providing one westbound vehicular lane and bicycle lane within approximately 22 feet of paved width and no sidewalks. West of Boggy Creek Road, McCoy Road continues as an FDOT roadway, becoming a two-way roadway again and providing two vehicular lanes and bicycle lanes in either direction with a center turn lane within approximately 66 feet of paved width and sidewalks on either side of the roadway. McCoy Road is signed with a speed limit of 35 mph between Conway Road and Via Flora, 40 mph between Via Flora and Boggy Creek Road, 35 mph between Boggy Creek Road and Lindos Drive, and 45 mph between Lindos Drive and Orange Avenue.

Other roadways within and abutting the City that provide key circulation include Gondola Drive, Perkins Road, and Matchett Road, all of which are not classified by FDOT, but function as neighborhood streets that accommodate vehicular circulation for both Belle Isle residents as well as those bypassing congestion on more regional roadways such as Orange Avenue and McCoy Road. These roadways are all two-way, two lane, approximately 20 foot wide streets that include stop controlled intersections and sidewalks on one or both sides, and are controlled by either the City or Orange County.

Regional north-south corridors such as Orange Avenue/Hansel Avenue and Conway Road provide connectivity along the periphery of the City on roadways controlled by FDOT and Orange County, respectively.

## Safety

Understanding where high concentrations of vehicular crashes have historically occurred can help target infrastructure improvements to make these roads and intersections safer going forward. As noted on Figure 8, very few crashes occur on the local and neighborhood streets within the City of Belle Isle itself, but intersections of major roads, and Orange Avenue on the City's eastern border, McCoy Road to the south, and Conway Road to the northeast note safety concerns along the periphery of the City.

**Figure 7 Average Daily Traffic Volumes**





**Belle Isle Transportation Master Plan**  
**Crash Hot Spots (2012 - 2018)**

Legend: High (Red), Low (Green)

Scale: 0 to 0.5 Miles

North Arrow

SunRail Station

Map Labels: Orlando, Edgewood, Sand Lake Road, Belle Isle, Orlando.



## WALKING AND BIKING

### Pedestrian Infrastructure

Sidewalks are present on one or both sides of most streets within the City of Belle Isle, as shown on Figure 9. Those that are provided are typically 5 to 6 feet in width and in most locations, include a grass buffer between the sidewalk and the edge of the adjacent traveled roadway.

While the presence of sidewalks on one side of the street, or even the absence of sidewalks, is acceptable for many neighborhood streets, higher traveled roadways such as Hoffner Avenue and McCoy Road present areas where pedestrian accessibility is difficult. For instance, restricted right-of-way along Hoffner Avenue presents challenges to providing a complete sidewalk infrastructure in areas where sidewalks are present only along one side of the street.

Some crosswalks are present, most notably in areas adjacent to schools, City Hall, and some parks, as well along select portions of Hoffner Avenue and at signalized intersections within the portions of McCoy Road that are in the city limits. However, some critical crosswalks near Cornerstone Charter Academy are missing, particularly in locations such as at the Randolph Avenue/Fairlane Avenue intersection as well as along long stretches of Hoffner Avenue that do not provide safe locations for pedestrians to cross.

### Bicycle Infrastructure

The City of Belle Isle currently does not have designated bicycle infrastructure, however almost all roads within the City have speed limits of 35 miles per hour or less, and all neighborhood roads have speed limits of 25 miles per hour. Low-speed roads, such as those that are signed for 25 miles per hour speed limits or less, are good potential candidates for cycling because cyclists feel more comfortable traveling near vehicles traveling at lower speeds and these roads are safer to cycle on than high-speed roads. However, Hoffner Avenue can prove to be discouraging to some lesser experienced cyclists given the lack of nearby or on-street east-west bicycle infrastructure and the existing narrow sidewalk. Studies in other parts of the country have noted that 60 percent of potential cyclists are interested in cycling, but concerned, preferring complete separation from motor vehicle traffic, or routes with very low traffic volumes and speeds.<sup>6</sup>

While no bike lanes or off-road paths exist within the City, Belle Isle is surrounded by existing or proposed bicycle infrastructure in all directions (as shown on Figure 10), creating a potential to link into the existing regional network. However, conventional bicycle lanes that exist along roadways such as Conway Road, McCoy Road, and Orange Avenue/Hansel Avenue, can provide for daunting experiences for cyclists along these routes, putting them in close proximity to high speed, high volume motor vehicle traffic.

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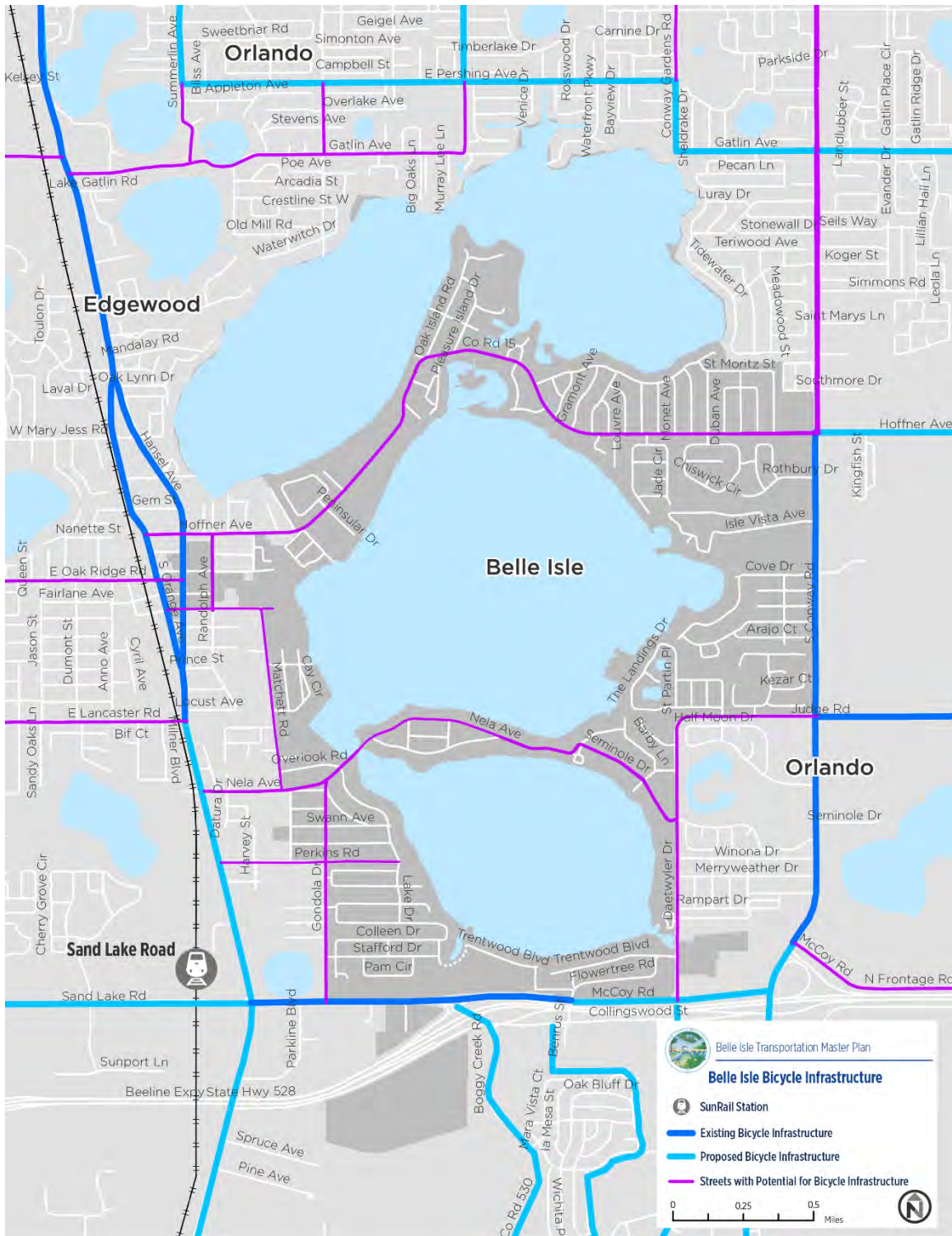
<sup>6</sup> <https://www.portlandoregon.gov/transportation/article/264746>

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**Figure 10 Belle Isle Bicycle Infrastructure**





## TRANSIT

Currently, there is limited public transit operating within the City, as shown on Figure 12. However, local LYNX bus service and commuter rail service operate adjacent and just beyond the City's borders.

### LYNX Transit

LYNX Transit is the bus system serving the greater Orlando area. Minimal residents in the south and northeast of the City are within a five-minute walk of a LYNX bus stop, which is generally how far riders are willing to walk to access bus service. Routes that are walkable to City residents operate along Orange Avenue/Hansel Avenue, McCoy Road, and Conway Road. Twelve LYNX routes serve the Sand Lake Road SunRail Station, however this important regional transit connection is outside reasonable reach of residents who may not own a car or want to travel with a private automobile.

### SunRail

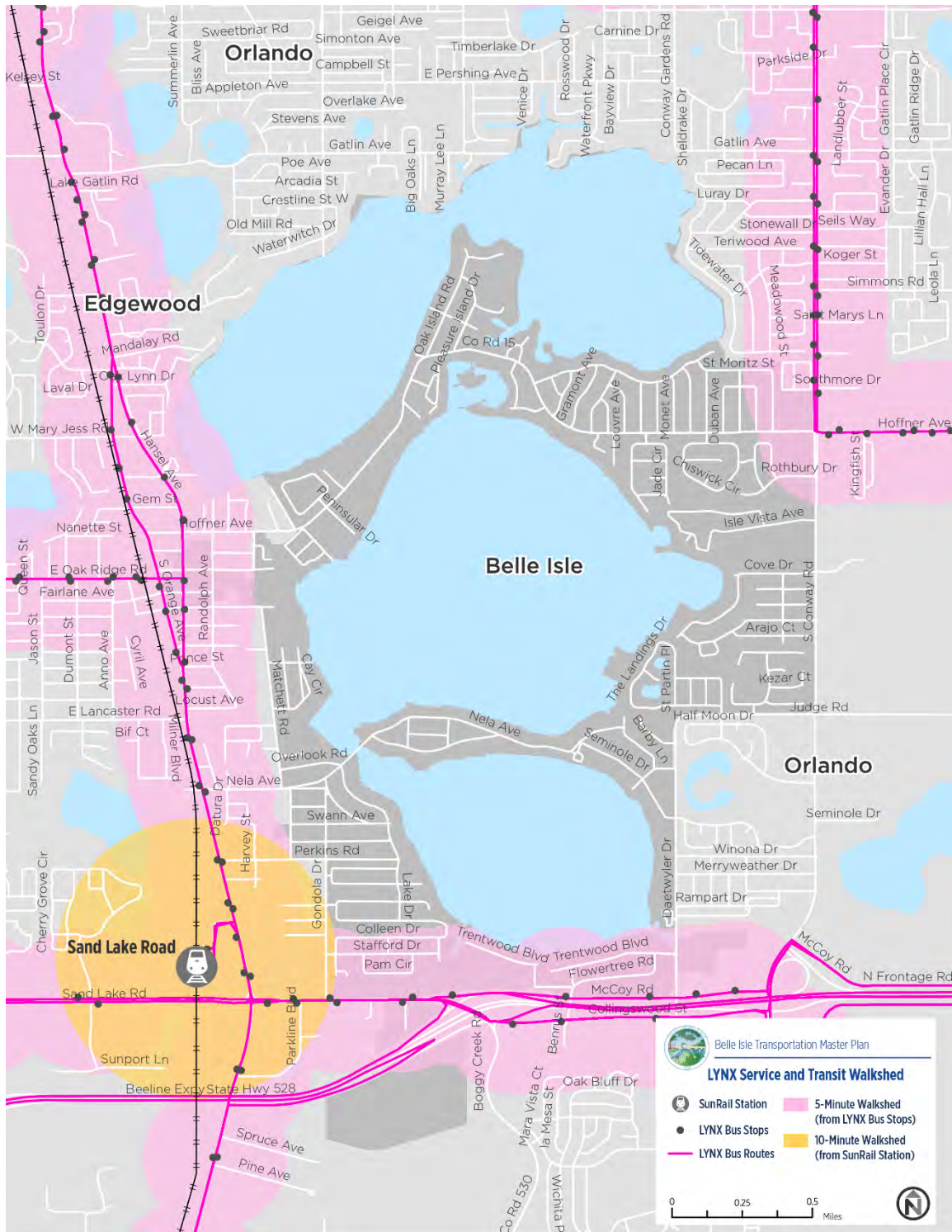
SunRail is a commuter rail service that serves the greater Orlando area. Sand Lake Road lies approximately one-half mile outside of Belle Isle's southwest border along the service's main line, which extends from Poinciana, south of Belle Isle, through downtown Orlando, and to DeBary further to the north.

While a large amount of parking is available at the station, the station is not within a ten-minute walk of almost any residents of the City. Ten-minutes (one-half mile) is usually considered a reasonable distance to walk to access rail transit. However, using a network of lower-speed roads, the southwest portion of Belle Isle is accessible to the SunRail station within a five-minute bicycle ride with much of the western portion of the City within a ten-minute bicycle ride.



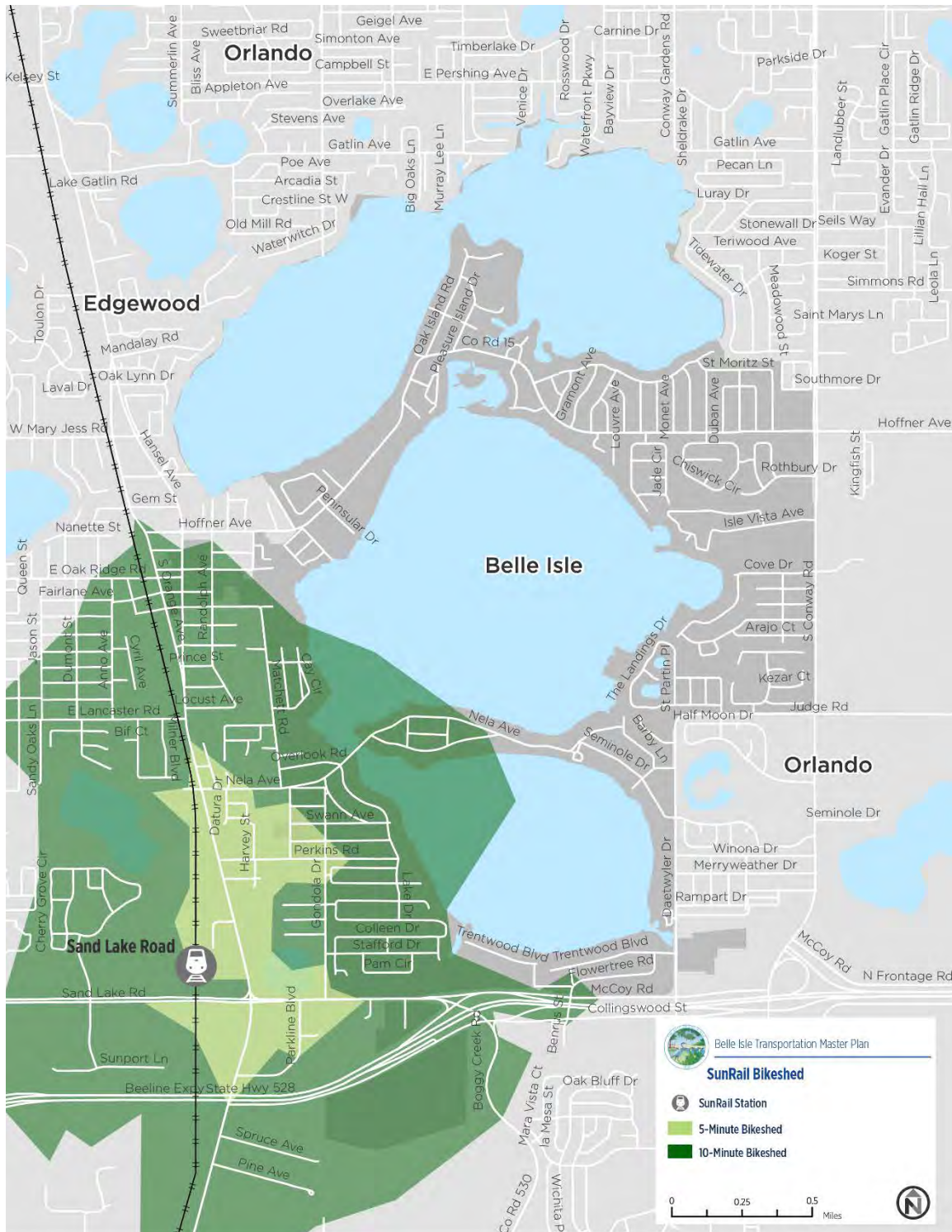
Figure 11 Sand Lake Road SunRail Station

**Figure 12 Transit Pedestrian Access**





**Figure 13 SunRail Bicycle Access**





## 3 PUBLIC OUTREACH

The Transportation Master Plan was informed by a number of stakeholder and public outreach efforts. These efforts were intended to provide the project team with valuable insight and guidance from various local sources, including key municipal staff, local residents and employees, and business owners and managers with a stake in the City. The Project Team that oversaw this process consisted of staff from the City.

The stakeholder outreach activities conducted for this plan Outreach efforts included:

- Project Team Meetings
- A Public Workshop
- Stakeholder Interviews

### PROJECT TEAM MEETINGS

The Project Team met for roughly bi-weekly phone calls. They also had three physical meetings - one as a kickoff to the project, another in conjunction with the Public Workshop, and a final in conjunction with the stakeholder interviews (see below). In addition to general project management tasks, the Project Team was responsible for confirming the goals of the study.

### PUBLIC WORKSHOP

The Project Team held a public workshop, on June 21, 2018, that provided participants and team members with an opportunity for a more hands-on engagement with the plan. The public workshop gave the project team the opportunity to inform the public on the scope and progress of the plan and provided citizens with a forum to express issues and concerns, and share local knowledge, insight, and ideas with the Project Team.

### Goal Setting

Through discussions with City staff and stakeholders, a set of goals were set for the project that were affirmed through the June 2018 public meeting. Of these, the primary goals identified by residents were as follows:

- *Transportation is safe for all residents and visitors whether driving, walking, or bicycling*
- *Residents can drive between destinations with minimal traffic congestion*
- *Residents can comfortably walk and bicycle to parks, schools, transit, and shopping areas.*

- *Belle Isle's streets are attractive and contribute to the beauty of the community.*

## Public Comments

Public meeting attendees shared the common concern of the City being used by commuters and outside residents as a “cut through” town, specifically by way of Hoffner Avenue and Nela Avenue. Residents expressed that Nela Avenue and Hoffner Avenue are small, quiet roads by design, but “cut through” traffic makes both streets dangerous and congested. In the morning hours, as well as the hours of school drop-off/pick-up, traffic conditions intensify.

Residents expressed frustration with the difficulty of making turning movements onto or from Hoffner Avenue, citing the lack of proper markings, high speeds and traffic as issues. It was also noted that Hoffner Avenue is not bicycle/pedestrian friendly. Sidewalks are uneven, cracked, and narrow, poorly lit and very close to the road, with little buffer from moving traffic. Some attendees indicated concerns that Nela Avenue was being used as a cut-through route for regional traffic, contributing to higher traffic volumes and speeds.

Daetwyler Drive, McCoy Road, Judge Road, and Gondola Drive were also mentioned. They suffer from similar issues with traffic, speeding, and subpar sidewalks.



Figure 14 Residents Offering Comments on Cycling in Belle Isle

## STAKEHOLDER INTERVIEWS

A selection of key stakeholders for the area was identified by the Project Team and City staff. These individuals were interviewed in small groups to gain valuable input on the area's needs. In total, 13 stakeholders divided into groups of residents impacted by identified target areas in the City provided input over the course of three meetings held November 12-13 2018. These target areas focused discussions on the Hoffner Avenue corridor, Nela Avenue/Seminole Drive corridor, and the Daetwyler Drive/Judge Road corridor in order to gain resident feedback on potential solutions under consideration by the project team.

## 4 GAP ANALYSIS

In order to provide recommendations that will effectively support a multimodal transportation network to accommodate growth and development in Belle Isle, it is critical to understand the existing conditions of the transportation network as discussed in Section 2. Doing so allows the project team to gain an understanding of the mobility options community members have at their disposal, and helps with the identification of network gaps that may exist.

Below is a review of specific gaps in the roadway, pedestrian, and bicycle networks in Belle Isle, as well as a summary of potential safety concerns that will be instrumental in the development of recommendations.

### ROADWAY NETWORK

The vehicular network facilitates acceptable traffic flows throughout most of the study City. However, vehicular constraints exist in some locations as follows:

- **Hoffner Avenue** – As an arterial that accommodates traffic traversing the City from Conway Road and points east to Orange Avenue/Hansel Avenue that is constrained by neighborhoods, Hoffner Avenue experience peak hour congestion and is expected to continue to experience congestion in the future. Future plans for the road do not contemplate significant widening within the city limits due to the constrained nature of the corridor. However, the slower speeds associated with congestion along the corridor will improve the safety of pedestrians and cyclists.
- **Judge Road** – Particular peak hour congestion and queuing is experienced on the eastbound approach to the Judge Road/Conway Road intersection that is primarily due to limited eastbound left turn queuing space and traffic signal timing.
- **Perkins Road/Gondola Drive** – Peak hour vehicular cut-through traffic presents increased northbound left and eastbound right turning vehicles at Perkins Road/Gondola Drive intersection. Since only the Gondola Drive approaches to the intersection are stop controlled and wide curb radii exist on all corners of the intersection, vehicular turning speeds present safety concerns.

Vehicular capacity constraints are present on FDOT roadways that cross or border the city limits such as McCoy Road, Orange Avenue, and Conway Road that are due to more regional commuting patterns. Overall, possibility of significant traffic improvements are hindered by existing constraints and city limit boundaries.

As the future for development and multimodal travel in and surrounding the City is envisioned, special consideration should be made for block size. This will impact the density and efficiency of the roadway network, particularly in areas where annexation



may be contemplated. Smaller blocks create opportunities to distribute traffic, and lessen bottlenecks resulting in congestion.

Finally, allowing low speed vehicles (such as golf carts) on city streets has been previously considered by the Belle Isle City Council and could allow for the addition of another mode of travel that would have less environmental impact than single occupant vehicles.

## WALKING

As noted in Section 2, most streets appear to have sidewalks on at least one side of the street. While the presence of sidewalks on one side of the street, or even the absence of sidewalks, is acceptable for many neighborhood streets, higher traveled roadways such as Hoffner Avenue and McCoy Road present areas where pedestrian accessibility is difficult. For instance, along Hoffner Avenue, restricted right-of-way in areas where sidewalk is present only along one side of the street presents challenges to providing a complete sidewalk infrastructure. There are other locations, such as those along McCoy Road east of Via Flora and at Daetwyler Drive where sidewalk infrastructure is incomplete.



**Figure 15 Hoffner Avenue Unsignalized Crossing**

Many corridors in Belle Isle include an inviting environment for walking, particularly along streets such as Nela Avenue, Seminole Drive, and many of the neighborhood streets. However crosswalk markings are limited along some streets. Unsignalized crossings, for example, allow the pedestrian network to be more useful as they limit travel distances and support desire lines (paths that pedestrians would like to travel were there not barriers like busy streets). Midblock crossing are already legal in

the state of Florida, whether or not a crossing is marked; marked crossing increase safety for those already making these legal crossings and require motor vehicles to yield where gaps in traffic flow may be sparse. Such treatments would be particularly helpful on long corridors with few signalized intersections such as Hoffner Avenue.

Similarly, wide curb radii at some intersections provide for longer crossing distances for pedestrians while encouraging faster turning speeds for through vehicles. Locations such as those at the eastern Nela Avenue/Homewood Drive intersection and along Hoffner Avenue at the Conway Isle Circle and Lake Conway Shores Drive intersections, as well as others, present locations where wide curb radii encourage faster turns by vehicles that could be detrimental to pedestrians crossing in those locations.

Street trees and landscaping add visual interest, provide shade, and soften environments dominated by hardscapes. Belle Isle has a beautiful native tree canopy and many of Belle Isle's residential neighborhoods take advantage of this creating inviting and calming streetscapes. Commercial areas along McCoy Road and Orange Avenue/Hansel Avenue, however, have relatively few trees and tend to be dominated by parking in lots or on street. This exacerbates the impact of the region's hot summers and tends to hurry people through outdoor spaces pushing them indoors and should be contemplated in commercial areas currently within the city limits as well as those that may be considered for annexation.

## BIKING

Belle Isle has a limited existing bicycle network but does offer some streets that provide low-stress alternatives to riding on higher traveled nearby regional streets. Where available, this can make for a comfortable riding experience for bicyclists of all ages and ability levels and is a tremendous asset for the community as it forms the basis for a much broader City and county-wide bicycle network.

However, connectivity in Belle Isle is hindered by the barriers presented by its lakes, which are otherwise an incredible asset for its residents. While neighborhood streets and corridors such as Nela Avenue and Seminole Drive provide relatively comfortable biking conditions for cyclists, north-south connectivity on the east side of the City is limited to bicycle lanes along Conway Road, which can provide a somewhat daunting environment for novice cyclists given the high vehicular speeds on the roadway, and on-street or on sidewalks along Judge Road and Daetwyler Drive. East-west bicycle connectivity in the northern third of the city is also limited along Hoffner Avenue which is a more highly traveled vehicular route with narrow right of way in some areas that limits the ability for additional connectivity.

While bicycles are allowed to share streets, the lack of dedicated facilities means that few are willing to do so. In addition to a less-than-friendly street layout in some streets, the many curb cuts and undulating curves of some streets create more points of conflict and reduced visibility between motorists and bicyclists. Defining part of the right-of-way for bicyclists would create safe space for bicyclists and encourage more to



Figure 16 Cyclist Using Nela Avenue Sidewalk

consider this mode of travel.

Finally, the lack of physically designated bicycle routes contributes to a lack of clarity among users as to what routes cyclists should use to travel through the City. While some residents have developed regular cycling routes to circumnavigate Belle Isle, most cyclists are left to determine for themselves which routes are acceptable to cycle around the City.

## TRANSIT NETWORK

The transit network serving locations within the city limits of Belle Isle are limited to a minimal number of LYNX bus stops near the periphery of the City along McCoy Road, Orange Avenue, Hansel Avenue, and Conway Road. LYNX routes serving these corridors provide those within walking distance access to the wider Orlando area, but the number and location of stops limits the accessibility of residents in the City.

The Sand Lake Road SunRail station provides commuter rail access just beyond the city limits, however, limited porosity between the southwest quadrant of the City and Orange Avenue requires circuitous pedestrian and bicycle routing if City residents try to access the station. Adequate bicycle and vehicular parking for existing conditions is provided at the Sand Lake Road station for residents wishing to park and use SunRail services.



Figure 17 Sand Lake Road Sun Rail Station



## 5 RECOMMENDATIONS

Citywide transportation recommendations take a “complete network” approach that recognizes that each corridor cannot be all things to all people. While every street in the network should accommodate as many modes as feasible, some streets will provide certain enhancements for one or two modes. When taken together, this provides an overall network that provides quality connections and mobility for all users.

### RECOMMENDATIONS

This plan proposes a complete, connected network for pedestrians, low stress/“family-friendly” bicycle corridors, commuter bicycle networks, transit, and vehicles. Together, these networks provide a composite multimodal network that supports the goals of the plan:

- *Transportation is safe for all residents and visitors whether driving, walking, or bicycling*
- *Residents can drive between destinations with minimal traffic congestion*
- *Residents can comfortably walk and bicycle to parks, schools, transit, and shopping areas.*
- *Belle Isle’s streets are attractive and contribute to the beauty of the community.*

The following sections describe recommendations for Belle Isle’s transportation network in support of these goals.

### Provide Complete Streets

Complete streets work for everyone in the community, regardless of their travel mode. A complete street network comprises a variety of street types that account for all users and create multiple safe and convenient ways for people to get around in a comfortable and integrated environment.

Complete streets support safety and community cohesion. They respect diversity and choice and make multiple transportation options viable. Complete streets enable those that do not drive to remain active and connected members of the community, whether they are too young or old, whether they have a disability, cannot afford a car, or simply choose not to drive. Complete streets also support the objectives of improved human and environmental health.

Belle Isle has made efforts to improve pedestrian and bicycle facilities, but the overall network favors the mobility of automobiles at the expense of other users. As the City

repaves and rebuilds its roadways, Belle Isle should take the opportunity to create or retrofit facilities to adequately accommodate pedestrians, bicyclists, and transit users.

- Adopt a complete street policy for Belle Isle.
  - Belle Isle should approach all planned transportation improvements and all planned development projects within the right-of-way as an opportunity to advance the value and objective of safety and complete streets.
  - Every street should safely accommodate all users.
  - Any street subject to major maintenance, rehabilitation, or reconstruction should provide safe accommodation for all users, of all abilities.
  - The means of accommodation should be appropriate to the street context and developed in consultation with local community stakeholders.
  - Capacity of transportation infrastructure (and potential impacts from proposed developments) should be judged from a multimodal perspective and not from a purely vehicular level of service standpoint.
  - The City should actively pursue regional, state, and federal funding opportunities to support complete streets improvements.
  - City agencies and departments should collaborate and coordinate with one another and adjacent jurisdictions to apply complete street principles and provide continuous networks.
  - Progress on complete streets should be measured in concert with the adopted measures of the Belle Isle Transportation Master Plan.

## **Provide a Safe and Accessible Pedestrian Network**

Every street must provide for safe accommodation of pedestrian travel. On busy streets, the best practice is to provide buffered sidewalks on both sides, while streets with lower traffic volumes may only need a sidewalk on one side.

- Prioritize walking connections to transit stops, schools, and parks. Implement first-last mile walking connection to transit and prioritize access to transit stops.
- Support projects that improve pedestrian connectivity.
- Require new development or redevelopment to provide appropriate pedestrian connectivity within the site and to adjacent properties with existing development or future development potential.
  - On commercial sites, on-site sidewalks should connect the public sidewalk to building entrances along a legible and direct route.
- Improve pedestrian access to destination areas in the City.
- Improve pedestrian routes that connect students to schools.
- Maintain a sidewalk inventory.
- Establish a methodology for project prioritization and performance evaluation.

- Improve pedestrian access across major roadways that create barriers to connecting the network. Comply with all state and federal regulations including the Americans with Disabilities Act (ADA).
- Adopt a formal sidewalk policy requiring sidewalks on all new, reconstructed, or substantially rehabilitated streets that respond to local needs and community context.
  - All new streets should provide sidewalks on both sides of the street irrespective of anticipated traffic volumes unless explicitly designed as a shared street.
  - Sidewalks should be considered with every major maintenance, restoration or street reconstruction project. Sidewalks may be constructed independent of other street projects.
  - Recommended guidance for sidewalks is that streets with moderate to high vehicle volumes (5,000 or more vehicles per day) should have sidewalks on both sides of the street. Moderate volume streets should have a continuous sidewalk at least along one side; local streets (less than 5,000 vehicles per day) should have a sidewalk on at least one side of the street, unless specifically redesigned or actively managed as a shared street.
  - Sidewalks should be a minimum of 5 feet wide.
  - The sidewalk network should be continuous and connected. Curb ramps must be provided at street crossings.



Figure 18 Existing Daetwyler Drive Sidewalk



- Provide a buffer/parkway/amenity zone between sidewalks and curb or edge of pavement, with street trees planted and old growth trees retained wherever possible.
- Plant or retain canopy trees near the sidewalk to provide shade for people walking in the hot Florida climate.

## **Provide a Robust Network for Bicycle Travel**

Despite having a year-round warm climate and relatively flat topography, bicycle mode share in Belle Isle is quite low compared to other modes and well below what has been demonstrated to be possible in many comparable communities throughout the United States. There is an active and engaged bicycle constituency in Belle Isle who have expressed a desire for greater accommodation and more support to increase the bicycle mode share. Increasing the bicycle mode share has the potential to reduce the growth of vehicle traffic and congestion, improve public health, and advance environmental goals.

A number of different factors can support bicycling as a viable mode choice. These include adequate provision of bicycle parking for both short term and long term users; bike share services; enhanced bicycle facilities; and building amenities for bicyclists such as bicycle storage, changing rooms, and showers.

Bicycle supportive policies and services should be integrated into the community's existing transportation policies to strengthen provisions for bicycle facilities and amenities. Policies should be flexible enough to allow for revisions over time as bicycle infrastructure and programming develops. Policies should be in coordination with a Complete Streets policy and proactively look toward the future for opportunities in technology and bicycle programming or systems.

Adoption of the following policies have become widespread:

- Create conditions that make bicycling more attractive than driving for trips of three miles or less.
- Coordinate with regional bicycle planning efforts.
- Fully integrate bicycles into ongoing local planning efforts.
- Further integrate support for bicycling into existing City policies.
- Adopt a plan for a system of bicycle facilities linking major destinations.
- Create, distribute, and regularly update a map of existing bicycle facilities and low-stress routes.
- Adopt bicycle facility design standards to ensure best practices in design and delivery of facilities.
- Adopt bicycle-supportive policies, including the provision of short- and long-term bicycle parking, showers, and changing facilities in non-residential developments.
  - This will be increasingly recommended as possible annexations and redevelopment opportunities arise.

- Improve and expand bicycle safety and education programs targeted at both drivers and cyclists.

## Provide Effective Transit Connectivity

Transit service is most concentrated on the Orange Avenue, McCoy Road, and Conway Road corridors. Multiple LYNX routes converge around the Sand Lake Road SunRail station, just beyond the city limits. Given the potential for transit oriented development surrounding the Sand Lake Road SunRail station, planning to accommodate effective transit service will be critical as the City contemplates annexations and redevelopment opportunities. The potential for water taxi service was reviewed. However, given the privatized nature of the lakeshores within the City, public water transportation along the lakes would be difficult or inefficient from a cost perspective.

- Promote transit-friendly design features in new development and redevelopment projects.
- Expand ADA-accessible sidewalks and crosswalks serving bus stops.
- Encourage employers located near the Sand Lake Road SunRail station to provide transit subsidies or other transit incentives.

## Provide for the Safe and Efficient Flow of Private Vehicles

Motor vehicle travel is and will continue to be the leading form of travel in Belle Isle. For many, commuting by private vehicle is the most convenient, if not the only viable mode. Prioritizing motor vehicle travel on some corridors recognizes the need to accommodate local residents and visitors and to provide efficient connections to regional networks, while acknowledging the limitations imposed by constrained rights-of-way and natural barriers within the City. Recommended improvements are not intended to invite or accommodate additional regional traffic through the City.

- Promote and support regional efforts to enhance vehicle performance of Orange Avenue.
- Promote safe speeds with traffic calming and design queues for slower speeds in more neighborhood settings.
- Improve safety for all travelers by improving congestion where possible in the City, including Judge Road and Daetwyler Drive.
- Coordinate with FDOT to optimize signal timings along McCoy Road to improve congestion.
- Consider allowing low speed vehicles (such as golf carts) on roadways within the city limits that are not within the jurisdiction of Orange County or FDOT<sup>7</sup>.

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<sup>7</sup> As of the publication of this plan, low speed vehicles (such as golf carts) are not allowed on Orange County streets, but are being considered by the Orange County Commission. Further consideration of this recommendation may be warranted given the decision made by the Orange County Commission.

## IMPROVEMENTS AND IMPLEMENTATION

A set of physical improvements for implementation have been recommended to support the goals determined at the onset of the plan and the overall planning recommendations described above. These are summarized on Figure 19 and Figure 20.

### Roadway Network

#### Widen Eastbound Judge Road Approach to Conway Road

Peak period queuing along eastbound Judge Road at Conway Road has impacted drivers with the demand for storage of eastbound left turning vehicles outpacing the capacity of the existing left turn lane. It is recommended that Judge Road be widened to allow for a three 11 foot lanes between Conway Road and Franconia Drive. This will allow for an additional 900 feet of storage for left turning vehicles at the Judge Road/Conway Road intersection.

#### New All-Way Stop at Gondola Drive and Perkins Road

The Gondola Drive/Perkins Road intersection currently operates under two-way stop control on the northbound and southbound Gondola Drive approaches. With this configuration eastbound and westbound traffic on Perkins Road does not stop. Congestion at the nearby Orange Avenue/McCoy Road/Sand Lake Road intersection combined with the presence of a traffic signal at the Gondola Drive/McCoy Road intersection has caused the Gondola Drive/Perkins Road intersection to see increased cut-through traffic. Vehicular volumes have been noted to be particularly increased on the eastbound Perkins Road approach to the intersection that can, at peak times, reach approximately 400 vehicles or more on this two-lane neighborhood street<sup>8</sup>. As such, it is recommended that all-way stop control be investigated for this intersection in an effort to improve safety conditions for drivers, pedestrians, and cyclists, as well as to reduce cut-through traffic along these neighborhood streets. This recommendation would need to be coordinated with Orange County given the city limit boundaries extend along the eastern edge of pavement on Gondola Drive and northern edge of pavement of Perkins Road, leaving the intersection itself primarily within Orange County's jurisdiction.

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<sup>8</sup> Traffic volumes based on Orange County data collected May 4, 2018 which note 392-443 vehicles per hour on the eastbound Perkins Road approach to the intersection from 3-6pm.



Figure 19 Recommended Improvements (North)

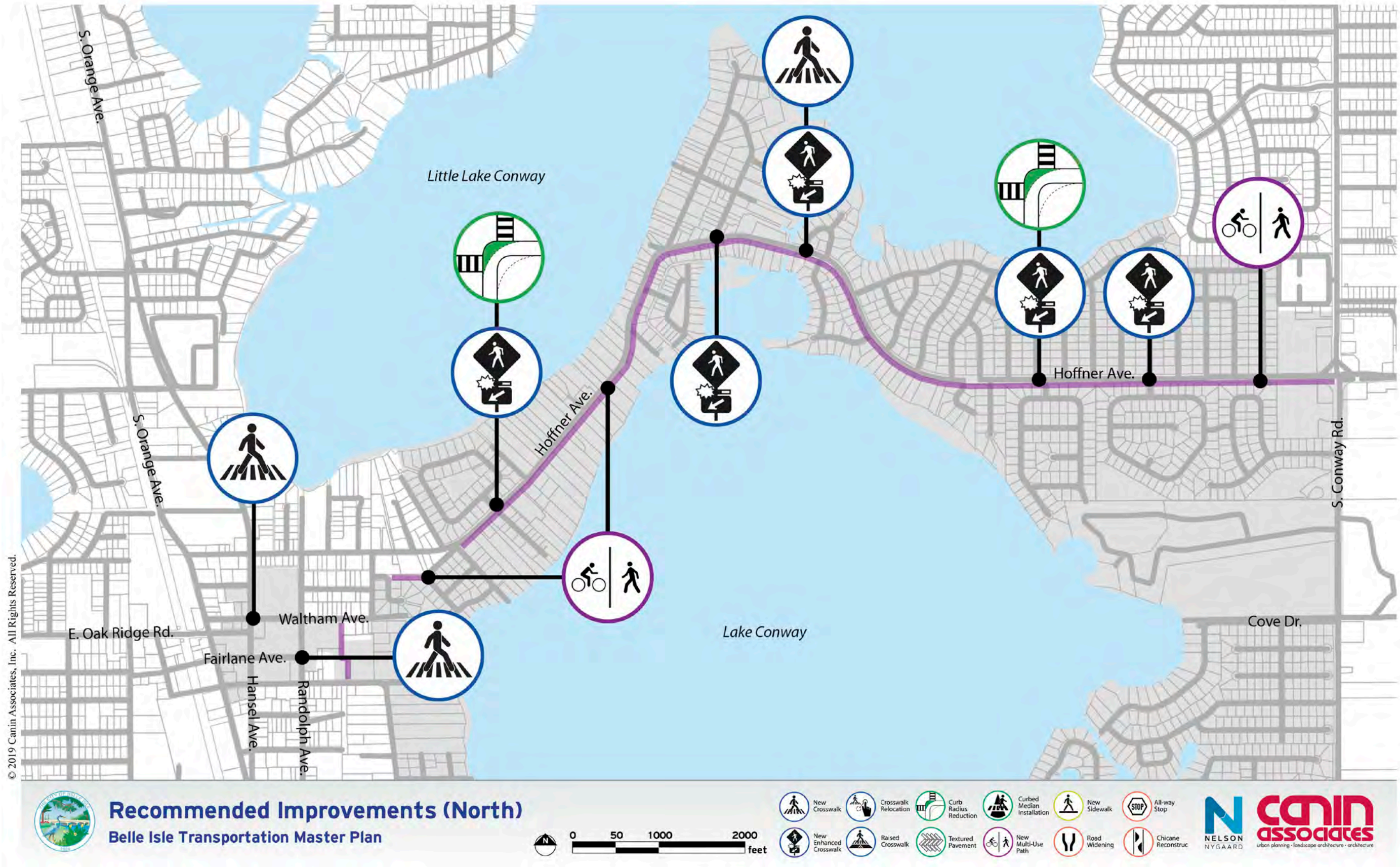
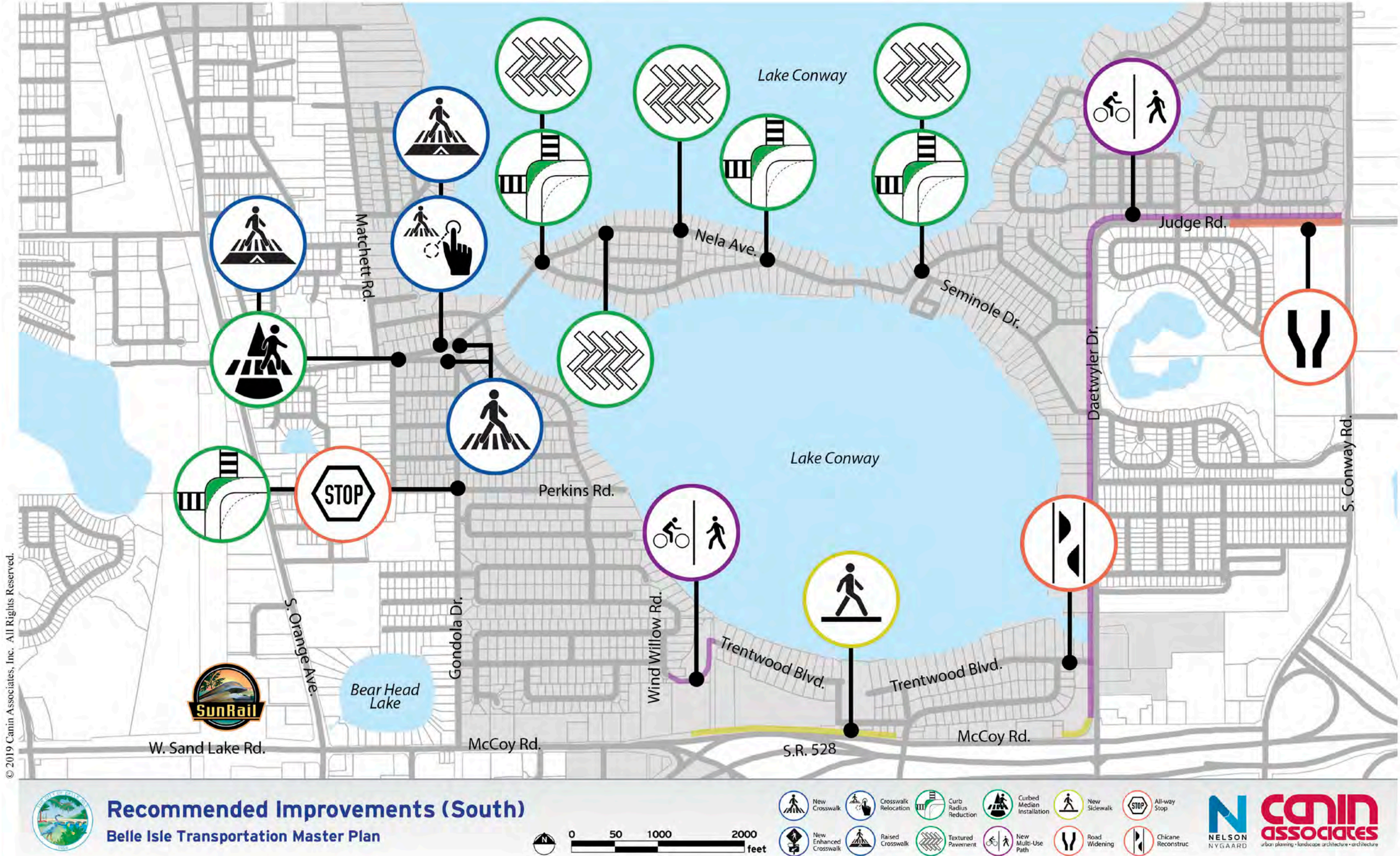




Figure 20 Recommended Improvements (South)





## Traffic Calming

Traffic is calmed by adding “texture” to the right of way. This disrupts a driver’s environmental certainty, forces them to pay attention, and to slow down and drive more cautiously. Traffic calming measures are not new to Belle Isle, having been implemented in several locations within the City in the form of speed humps, chicanes, raised intersection diverters, and a hybrid roundabout. However, improvements to some of these treatments would result in more effective implementation of traffic calming. These are described as follows:

### Nela Avenue Intersections Textured Pavement

Textured pavement treatments are recommended along Nela Avenue at the existing stop-controlled intersections at Island Street, Easter Street, Homewood Drive (west), and Seminole Drive. This will increase driver awareness of speeds along Nela Avenue to complement existing traffic control measures along Nela Avenue.

### Trentwood Boulevard Chicane

The existing chicane on Trentwood Boulevard between Daetwyler Drive and Flowertree Road is in poor condition and is recommended for redesign and reconstruction. Currently requiring oncoming vehicular traffic to yield to one another, the existing chicane has been designed too narrow such that many vehicles mount the chicane’s curbs, causing damage to the chicane.

### Reduced Curb Radii

Wide curb radii at the corners of intersections can invite drivers to turn through intersections at higher speeds, presenting potential hazards for pedestrians, cyclists, and other drivers. While Belle Isle contains many locations where curb radii wide, encouraging higher speed turns, recent NACTO design standards recommend curb radii of 10-15 feet. Given this, some specific locations are recommended for reductions in curb radii as follows:

- Hoffner Avenue/Conway Isle Circle/Peninsula Drive
- Hoffner Avenue/Lake Conway Shores Drive/Mortier Avenue
- Gondola Drive/Perkins Road
- Nela Avenue/Homewood Drive (west)
- Nela Avenue/Homewood Drive (east)
- Nela Avenue/Seminole Drive

## Pedestrian Network

### Sidewalk Needs

Sidewalks function as integral components of a pedestrian-friendly transportation network where pedestrians can experience safety, comfort, access, and efficient mobility. While Belle Isle generally provides an overall acceptable sidewalk network for its neighborhood context, some gaps exist along McCoy Road within the City that are



recommended. These include connecting the gap missing in the northwest quadrant of the Daetwyler Drive/McCoy Road intersection as well as missing sidewalks along the north side of McCoy Road between Via Flora and Boggy Creek Road. It is also recommended that these sidewalk links be designed to be adapted as shared-use paths for pedestrians and cyclists should the sidewalk infrastructure along the north side of McCoy Road be upgraded in the future given the difficult existing environment posed for cyclists in the bicycle lanes provided on McCoy Road.



**Figure 21 Existing City Hall/Police Department Pedestrian Connectivity**

In addition, while there are sidewalks within the area of City Hall, there are no sidewalks providing direct connections to City Hall. Currently, pedestrians are required to use a crosswalk that is marked within the vehicular path of opposite driveways for City Hall and the Police Department. It is recommended that sidewalks be constructed to along the south side of Nela Avenue (east of the City Hall driveway), the west side of Conway Circle (between Nela Avenue and Gondola Drive/Nela Avenue), and along the north side of south Nela Avenue to connect to a sidewalk that would bisect City Hall's property.

## Crosswalk Improvements

Where crosswalks are marked, high-visibility crosswalk styles should be used. NACTO guidance suggests that high visibility markings be used at any locations where greater motorist warning is considered beneficial, or where pedestrians may not be expected to cross (such as mid-block locations), or where there are substantially higher pedestrian crossing volumes. High-visibility crosswalk styles typically fall into one of three general categories:

- Transverse (Solid, Standard, Dashed)
- Longitudinal (Continental, Ladder)
- Diagonal (Zebra)

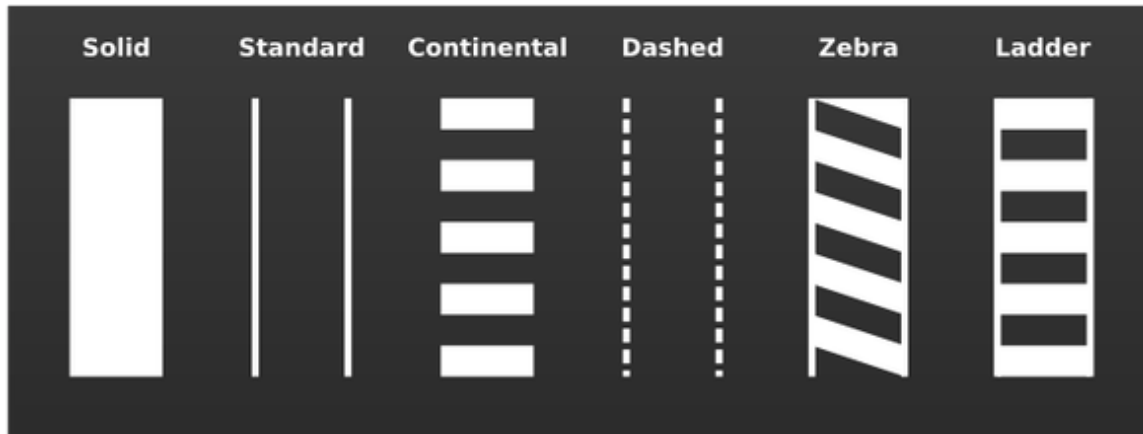
NACTO guidance suggests that ladder, zebra, and continental crosswalk markings are more visible to approaching vehicles and have been shown to improve yielding behavior, and therefore are preferable to standard or dashed markings.<sup>9</sup>

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<sup>9</sup> <https://nacto.org/publication/urban-street-design-guide/intersection-design-elements/crosswalks-and-crossings/conventional-crosswalks/>

Ladder and Continental crosswalks are most ubiquitous in Belle Isle and Orange County, providing a preferred high-visibility crosswalk style. It is recommended that crosswalks be at least 8 feet wide, or at least the width of the approaching sidewalk if the sidewalk exceeds 8 feet. Approaching sidewalks should also be free of obstruction to allow pedestrians to move freely across the intersection and ramps or paved landings (when no curb is present) should be provided on either side of the crosswalk.

**Figure 22 High-Visibility Crosswalk Styles<sup>10</sup>**



New or improved crosswalks are recommended at numerous locations throughout the City to improve pedestrian accessibility. Some additional locations have been identified for raised crosswalks to further act as traffic calming in addition to pedestrian safety measures. Overall, crosswalks have been specifically identified to improve pedestrian connectivity in three areas:

- Hoffner Avenue at:
  - St. Germain Avenue/Wandsworth Avenue
  - Lake Conway Shores Drive/Mortier Avenue
  - St Denis Court (west)
  - Pleasure Island Drive (improved, should this crosswalk remain)
  - Peninsula Drive/Conway Isle Circle
- Surrounding Cornerstone Charter Academy at:
  - Randolph Avenue/Fairlane Avenue (eastbound approach)
  - Hansel Avenue/Waltham Avenue (westbound approach)
- Surrounding City Hall at:
  - Gondola Drive/Conway Circle/Nela Avenue (eastbound approach)
  - Conway Circle/Nela Avenue (northbound approach)
  - Nela Avenue/City Hall/Police Department (relocate and install raised crosswalk)
  - Nela Avenue/Matchett Road (construct median and raised crosswalk)

<sup>10</sup> [Source: sfbetterstreets](#)

## Rectangular Rapid Flashing Beacon (RRFB) Crosswalks

Crosswalks enhanced with Rectangular Rapid Flashing Beacons provide on-demand flashing signal lights alerting drivers to a pedestrian crossing. Stop bars are provided where cars are to stop as if at a traditional traffic signal. RRFB signals are recommended at all midblock crossings along Hoffner Avenue to allow safe passage.

Currently the only signalized pedestrian crossings along Hoffner Avenue are at Conway Road and Hansel Avenue, approximately 2.75 miles apart from one another. Improving pedestrian connectivity in this area is necessary given the significant amount of residential development and lack of traffic control along Hoffner Avenue. RRFBs are recommended in conjunction with the crosswalks noted previously along Hoffner Avenue at the following:



Figure 23 RRFB Example

- St. Germain Avenue/ Wandsworth Avenue
- Lake Conway Shores Drive/Mortier Avenue
- St Denis Court (west)
- Pleasure Island Drive (improved, should this crosswalk remain)
- Peninsula Drive/ Conway Isle Circle

## Bicycle Network

### Shared-Use Paths

Shared-use paths are pathways within a public right-of-way or easement that are designed to accommodate two-way non-motorized users, but are physically separated from motor vehicle traffic by an open space or a barrier. Users of shared-use paths typically include people riding bicycles and walking, as well as joggers, skaters and skateboarders, and scooter riders, to name a few.

The lack of east/west connectivity combined with resident input on the comfort level of biking on higher traveled corridors within the City such as Hoffner Avenue and Daetwyler Drive/Judge Road suggest these as corridors that should have upgraded facilities. As such, shared-use path are recommended as described below:



### Hoffner Avenue Path

Hoffner Avenue is a key east/west connector along the north side of the City that is highly traveled by vehicles, resulting in uncomfortable conditions for cyclists on the corridor. While Hoffner Avenue has a wide right of way for most of the eastern portion of the corridor, the right of way narrows considerably between Conway Isles Circle and Oak Island Road along the western portion of the corridor, making the incorporation of bicycle lanes difficult.

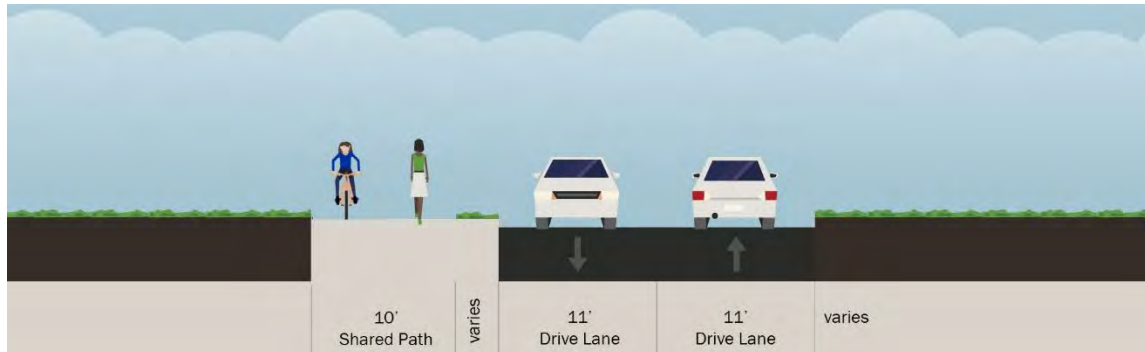


Figure 25 Hoffner Avenue (western section looking west) Cross-Section

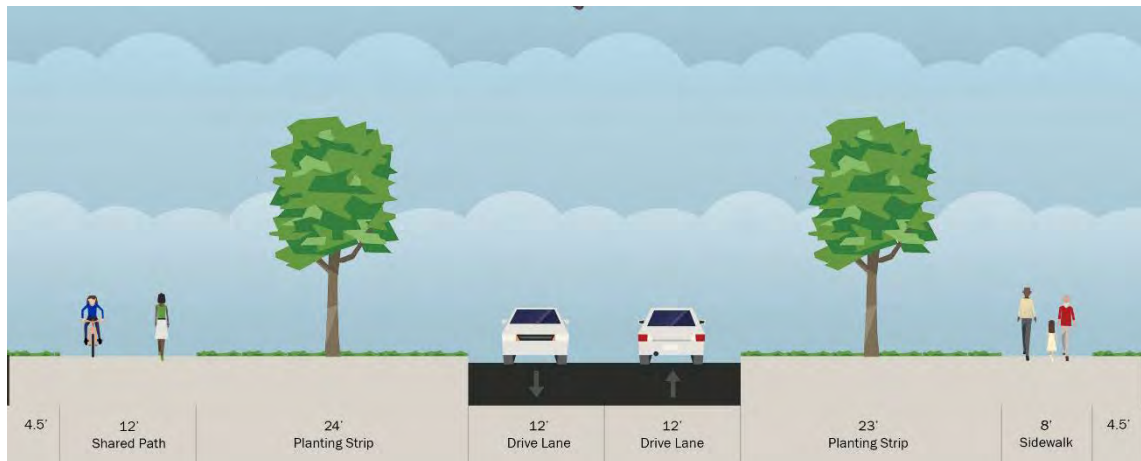


Figure 24 Hoffner Avenue (eastern section looking west) Cross-Section

As a result, it is recommended that a 10-foot wide shared-use path be constructed along the south side of Hoffner Avenue from Wilks Avenue to St. Denis Court as an upgrade the south side sidewalk. A dismount zone for cyclists is recommended as the path crosses the bridge over the canal linking Lake Conway with Little Lake Conway given the narrow nature of the existing pedestrian path alongside the bridge. This dismount zone could be reconsidered should the existing bridge be redesigned and reconstructed in the future. Beyond St. Denis Court to Conway Road, it is recommended that the shared-use path be constructed as a wider 12-foot upgrade to the south side sidewalk to take advantage of the wider right-of-way that is present on the eastern portion of the corridor. Further study of right-of-way impacts and the impact of potential utility and stormwater relocations will be necessary should this recommendation be designated for implementation.

#### Wilks Avenue/Windmill Court Connector

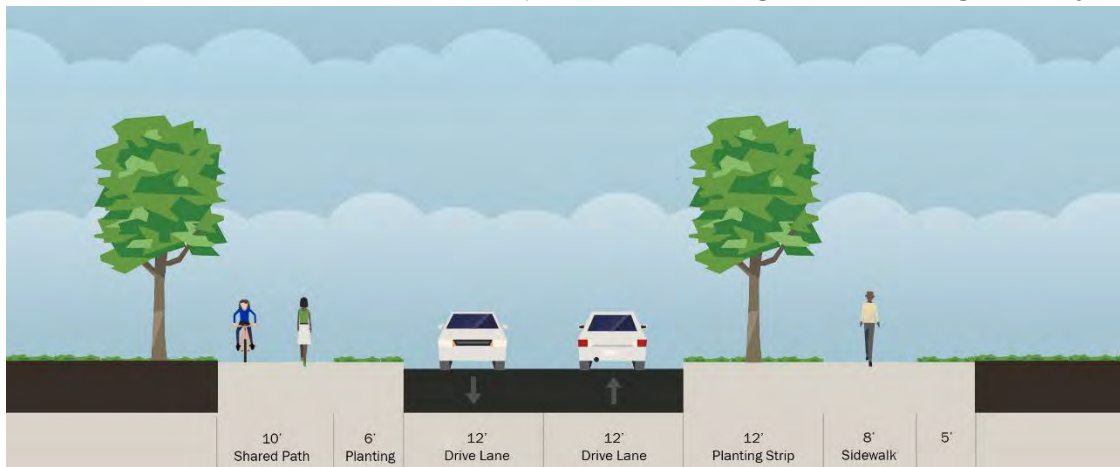
Approximately 400 feet of abandoned roadway right of way exists between Wilks Avenue and Windmill Court that is recommended to be repurposed as a 12-foot wide shared-use path to continue the east/west connectivity provided by the longer Hoffner Avenue shared-use path. Connecting lower traveled Wilks Avenue and Windmill Court (and nearby Marinell Drive), this connector would provide a vital link within the City of Belle Isle<sup>11</sup>. Coordination would be required with the residents of Windmill Terrace for the reconstruction of the brick neighborhood signage built across the right of way.

#### Waltham Avenue/Wallace Street Connector

It is recommended that a shared-use path be constructed to connect Waltham Avenue and Wallace Street, bisecting the City-owned parkland partially occupied by ballfields serving Cornerstone Charter Academy. This shared-use path would use the existing 10-foot wide sidewalk meeting Wallace Street adjacent to the Orange County Fire Rescue Station 70, extending it north between the existing softball field parking lot to connect to Waltham Avenue. Coordination may be required with Cornerstone Charter Academy to reconfigure fencing securing the school's portions of the property to allow access to the shared-use path.

#### Daetwyler Drive/Judge Road Path

Vehicular volumes along the Daetwyler Drive/Judge Road corridor make cycling uncomfortable on the roadway itself and while sidewalks exist along much of the corridor, they can be narrow for cycling purposes and are often placed close to fences and walls that can pose hazards for cyclists (such as along the north side of Judge Road). As a result, a 10-foot shared-use path is recommended along the north side of Judge Road and the west side of Daetwyler Drive to provide a vital link in the southeast quadrant of the City connecting existing bicycle lanes on McCoy Road and Conway Road as well as linking Belle Isle neighborhood streets with the larger regional bicycle network. Connection of the shared use path to the existing sidewalk along Conway



**Figure 26 Daetwyler Drive (looking north) Cross-Section**

<sup>11</sup> Alternatively the Hoffner Avenue shared-use path could be extended to Marinell Drive with coordination with Orange County, as this segment of Hoffner Avenue falls outside of the City of Belle Isle.

Road should be shifted away from the subdivision wall that the existing sidewalk currently follows. Finally, further study of utility and stormwater impacts as well as spacing of the shared use path at pinch points such as adjacent to the Waters Edge Drive and Franconia Drive intersection may need be necessary should this recommendation be designated for implementation.

#### Trimble Park Connector

An approximately 850 foot sidewalk exists within Trimble Park connecting Wind Willows Road and Trentwood Boulevard that is recommended to be repurposed as a 12-foot wide shared-use path to continue the east/west connectivity provided by neighborhood streets in the southern half of the City. Connecting lower traveled Wind Willows Road and Trentwood Boulevard, this connector would provide a vital link along the southern edge of the City of Belle Isle.

#### Potential Other Connectivity (Nela Avenue/Seminole Drive)

Additional shared-use facilities could be considered in the long-term for the Nela Avenue/Seminole Drive corridor between the Nela Avenue bridge and Daetwyler Drive. While right-of-way appears to exist to widen the existing sidewalk to accommodate a 10-12 foot wide shared-use path, the low speed and low traffic volume nature of Nela Avenue and Seminole Drive lends itself better to use as a bicycle boulevard with signing and striping for shared use of the roadway with bicycles in the near-term.

### **Bicycle Route and Wayfinding**

A designated bicycle route within the City combined with wayfinding (directional signage) can play an important role in ensuring that bicycle riders can navigate the network options that may be available to them. New riders, who may not have adequate knowledge of the network, may have little tolerance for wayfinding errors when trying to figure out where to go. The development of a bicycle network map (as recommended on Figure 27) that is enhanced with wayfinding signs placed at or near important destinations, at bicycle network intersections, or other key decision making points will improve conditions for cyclists in the City. It is recommended that roadways along these bicycle routes that are designated for shared on-street bicycle usage be signed and striped for those roadways' shared nature.



Figure 27 Recommended Bicycle Network



## 6 GETTING IT DONE

The Belle Isle Transportation Master Plan seeks to create policies and projects to support future investments in the area by guiding the development of a multimodal transportation network that facilitates walking, bicycling, transit-use, and effectively manages vehicular demands. This section includes specific recommendations to implement and fund the solutions identified as a part of this plan.

### Cost Estimates

The recommended major capital facility improvements by category and potential costs are shown in Figure 28.

**Figure 28 Cost Estimates of Major Capital Facility Improvements<sup>12 13</sup>**

Type	Treatment	Estimated Cost
Road Widening	Judge Road widen from two to three lanes (900' between Franconia Drive and Conway Road)	\$310,000
Sidewalks	McCoy Road/Daetwyler Drive Northwest Corner	\$24,000
	McCoy Road from Via Flora to Boggy Creek Road	\$121,000
	City Hall	\$12,000
RRFB	Rectangular Rapid Flashing Beacons (per location) <sup>14</sup>	\$25,000
Multi-Use Paths	Hoffner Avenue Path from Wilks Avenue to Conway Road	\$650,000
	Wilks Avenue/Windmill Court Connector Path	\$22,000
	Waltham Avenue/Wallace Street Connector Path	\$15,000
	Daetwyler Drive/Judge Road Path from McCoy Road to Conway Road	\$415,000
	Trimble Park Connector Path	\$46,000

<sup>12</sup> Cost estimates are generally approximated for planning purposes based on FDOT Long Range Estimate models (except where noted):

<https://www.fdot.gov/programmanagement/estimates/lre/costpermilemodels/cpmsummary.shtm>

<sup>13</sup> Cost estimates may fluctuate based on right-of-way acquisition, realignment of utilities, or other factors that are not included in these estimates.

<sup>14</sup> Adjusted estimate based on FHWA Pedestrian and Bicycle Information Center Costs:

[http://www.pedbikeinfo.org/cms/downloads/Countermeasure\\_Costs\\_Summary\\_Oct2013.pdf](http://www.pedbikeinfo.org/cms/downloads/Countermeasure_Costs_Summary_Oct2013.pdf)

## Regional Coordination

Given Belle Isle's position within the Greater Orlando Metro area, coordination with adjoining jurisdiction and regional planning organizations is critical to the success of any transportation projects that may seek to improve the lives of City residents. This becomes increasingly critical as regional roadways serving Belle Isle (such as Orange Avenue, McCoy Road, and Conway Road) become increasingly congested and alternate forms of transportation such as SunRail, LYNX, and regional bicycle infrastructure is expanded. The location of Belle Isle's city limits along many roadways as well as ownership of many roadway facilities by other entities (such as Hoffner Avenue (Orange County) or Orange Avenue (FDOT)) makes coordination required in order to provide transportation improvements in certain areas of the City. Regular participation and cooperation with MetroPlan Orlando, Orange County, FDOT, the Cities of Orlando and Edgewater, and other nearby governmental entities can further the visibility of Belle Isle's priorities.

## Funding

There are many funding sources that can be used to support the Belle Isle Transportation Master Plan's implementation, including leveraging existing resources; local, regional, state, and federal grant funding opportunities; private funding; and partnership opportunities. While many of these funding sources are competitive—particularly the public grant sources—many cities have been very successful at competing for grant funds. By matching projects to the funding sources for which they are best suited (and for which they can be most competitive), the City can continue to use a variety of funding mechanisms to build projects and implement new programs.

This section is organized into public funding sources and private funding sources. The public sources are further categorized into local, regional, state, and federal programs.

### Public Funding Sources

Public funding sources include local, regional, state, and federal funds and grant opportunities. The regional, state, and federal sources are distributed through regular funding competitions, and the amount available in a given year depends on a wide range of factors. It is expected that some of the projects identified will be competitive for public funding given the benefits they provide to specific communities and their focus on improving comfort and safety. Examples of these types of sources include Federal Highway Administration (FHWA) Safe Routes to School funding, Federal Transit Administration (FTA) funding through programs like the National Aging and Disability Transportation Center (NADTC), and FDOT funding for safety programs (also including Safe Routes to School).

### Private Funding Sources

Private funding sources are increasingly used to supplement public funds, particularly in areas that are experiencing a great deal of growth and development. While private funding is most often the "last dollar in" for a project—rather than the seed money for an improved pedestrian crossing, for example—leveraging private investment is a powerful



way for cities to implement more projects and build stronger partnerships with community members.

Particularly as annexation and new development are contemplated, partnerships with local businesses can generate support and funding for projects in specific places or as a part of larger neighborhood initiatives. Projects funded through public-private partnerships may include green streets and pedestrian plazas, pedestrian tunnels, bike share programs, and multi-use trails. Working proactively with corporate stakeholders, which often occurs as a part of large redevelopment projects or within the scope of a specific community benefits agreement—can also lead to a partnership for funding projects.

Finally, a number of national foundations have begun to play important roles in supporting pedestrian infrastructure improvements and programming. National foundations that have funded urban health and active transportation investments in the recent past include the following:

- Bloomberg Philanthropies' Sustainable Cities and Initiative for Global Road Safety, respectively, grants aim to tackle climate change at the city and local level and reduce traffic deaths and injuries.
- The Kresge Foundation has supported planning (not construction) for bicycle and pedestrian facilities.
- Outside the Box is a grant program funded by Redbox and managed by the Online Computer Library Center (OCLC) in partnership with the Project for Public Spaces to support libraries and their communities in carrying out free, fun events in the public right-of-way to activate spaces.
- The Robert Wood Johnson Foundation funds projects and research related to the health impacts of active transportation and the built environment.
- Southwest Airlines' Heart of the Community Program grants provide financial and technical assistance to local community partners who seek to bring new life to public spaces and transform them into vibrant places that connect people and strengthen communities.
- The Surdna Foundation's Sustainable Transportation Networks and Equitable Development Patterns Grant supports efforts to boost sustainable transportation networks.

## **Development Fees**

Some jurisdictions have implemented impact fees that can be used to fund various types of infrastructure. For example, a fee may be adopted for each peak hour vehicle trip that is generated by a new residential project. In most cases, this funding is combined with funds from other projects to establish a pool of money to construct the improvements that are on an adopted project list which can include projects that serve many travel modes. As part of approval for new projects, the City could require developers to fund or build infrastructure in right-of-way adjacent to their project.

## **Business Improvement Districts and Community Benefit Districts**

Infrastructure can be funded as part of a local benefit assessment district, which is based on the concept that those who benefit from a service should help to fund it. One common example is the Business Improvement District (BID), where business owners pay directly into a common fund to provide improved infrastructure, support operations to maintain clean and safe streets, and enhance wayfinding and placemaking elements in the district. These districts may fund bike improvements along with ongoing maintenance, placemaking, and landscaping projects. This approach may become more applicable as the City contemplates annexation and/or with further development surrounding the Sand Lake SunRail station.



**CITY OF BELLE ISLE, FLORIDA  
CITY COUNCIL AGENDA ITEM COVER SHEET**

**Meeting Date:** August 6, 2019

**To:** Honorable Mayor and City Council Members

**From:** B. Francis, City Manager

**Subject:** Proposed Agreement with Regal Boats to Use Venetian Ramp

**Background:** Regal Boats is requesting to use Venetian Ramp to launch its boats. Regal launches its boats twice a year for customer appreciation events. The Boats are usually larger and longer than Venetian Ramp allows. The current restriction on Venetian Ramp is 24 feet. Regal boats are in excess of 30 feet and 12,000 pounds. Regal cannot launch their boats at Randolph Ramp because the location for their events is on the center lake and located at 2110 Hoffner Avenue and their boats cannot get under the Hoffner Bridge if launched at Randolph Ramp. 2110 Hoffner Avenue is approximately 450 feet from Venetian Ramp.

Regal is willing to enter in to an agreement with the City to pay for any damages done at the ramp and for any police escorts that might be needed to get their boats from the factory to the ramp.

**Staff Recommendation:** Approve the agreement. If approved, the City Attorney will draft the final agreement for signatures.

**Suggested Motion:** I move that we approve the agreement with Regal boats to allow Regal to Launch their boats at Venetian Ramp.

**Alternatives:** Do not approve the agreement and prohibit Regal from launching their large boats at the ramp.

**Fiscal Impact:** TBD

**Attachments:** Draft Agreement



Use Agreement between City of Belle Isle and Regal Marine

This Agreement is made this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_ by and between the City of Belle Isle, Florida (hereinafter "City") having an address of 1600 Nela Avenue, Belle Isle, FL 32809 and marketing Business Associates LTD, dba Regal Marine Industries (hereinafter "Regal") having an address of 2260 Jetport Drive, Belle Isle, FL 32809

RECITALS

- A. City maintains and controls the Venetian Boat Ramp in Belle Isle, FL.
- B. The Venetian Boat Ramp currently does not allow any boats over 24 feet in length to be launched from the ramp
- C. Regal manufactures boats that are over 24 feet in length and over 12,000 pounds (6 tons) which are considered to be oversized boats.
- D. Due to the size of the Regal boats, they cannot navigate under the Hoffner Bridge thereby not being able to use the Randolph Ramp to launch boats for their events.
- E. Hoffner Avenue has a truck restriction of 6 tons, except for local deliveries and Regal uses oversized vehicles to transport the oversized boats to the Venetian Boat Ramp
- F. Regal has 2 events per year (2 days each) where they invite customers to 2110 Hoffner Road to display Regal boats
- G. City desires to permit Regal to utilize the Venetian Boat Ramp for the sole purpose of launching and retrieving Regal boats before and after the events and to set forth in writing the terms and conditions of Regal's use of same.

WITNESSETH

City, for good and valuable consideration, permits to Regal the right to utilize the Venetian Boat Ramp (hereinafter "ramp") on the following terms and conditions:

1. PERMIT

- 1.1. City hereby grants to Regal a revocable permit to utilize the ramp for the sole purpose of launching and retrieving oversized boats at the Venetian Boat Ramp and Regal hereby accepts such permit.
- 1.2. In consideration for such grant, Regal agrees to assume and to perform all of Regal's duties contained in this agreement.
- 1.3. Regal acknowledges and agrees that any right to use the facilities pursuant to any other agreement or understanding with City, or any other person or entity, is superseded by this Agreement.

2. TERM

The rights and obligations of the parties under this agreement shall be effective as of the date of execution of this agreement and shall immediately expire upon City notifying Regal of City's intent to terminate this Agreement.

3. IMPROVEMENT/ DUTY TO NOTIFY

- 3.1. Regal acknowledges and agrees that Regal shall not construct any improvements in or on the facilities, nor shall Regal expand, modify, alter or otherwise change any improvements currently existing at the Venetian Boat Ramp, unless agreed to, in writing by the City. Any improvements, agreed to by the City in writing, will be paid for solely by Regal.
- 3.2. If Regal notices a condition of the facilities that may pose a risk to the Regal or other users of the facilities, Regal agrees to notify City of the condition and to immediately cease utilizing the facilities until the condition has been addressed by the City. Regal will repair, at Regal's own expense, any damages done to the Venetian Boat Ramp, including the driveway, Pasadena Drive, and Venetian Drive.

#### 4. COMPLIANCE WITH LAWS

Regal shall comply with all laws, orders, ordinances, rules, regulations or requirements of any governmental authority having jurisdiction over the streets and facilities.

#### 5. USE/ASSIGNMENT

Regal agrees to not allow any individual, or organization, to use the facilities and agrees to immediately report all violators to the City. Regal further covenants and agrees that Regal shall not assign any of Regal's rights under this agreement, to any other person or entity, wither expressly or by operation of law, without the express written consent of City, which consent may be withheld in the sole and absolute discretion of City.

#### 6. INDEMNITY

Regal shall and will save, defend, hold and keep harmless and indemnify City, its agents, servants, owners, directors and employees from and against any and all expenses, sums paid, liability and claims, of any kind or nature, including personal injuries, arising in any manner or under any circumstances through the exercise by Regal of any right granted or conferred hereby, or the performance or non-performance of an of Regal's obligations hereunder, or any other act or omission of Regal or City. Regal hereby assumes all risks and dangers and all responsibility for a losses and/or damages, whether caused in whole or in part by the negligence or other conduct of the City of Belle Isle, its officers, owners, agents, servants, contractors and employees.

Regal voluntarily agrees to release, waive, discharge, hold harmless, defend and indemnify the City of Belle Isle, its officers, owners, agents, servants and employees from any and all claims, actions or losses for bodily injury, property damage, wrongful death, loss of use or otherwise which may arise out of Regal's use of the Venetian Boat Ramp.

#### 7. EVENT OF DEFAULT

- 7.1. Upon any default by Regal under any of Regal's obligations under this Agreement, City may, at its option terminate this Agreement; or exercise any other remedy available to City at law or in equity.
- 7.2. The rights and remedies of City under this Agreement shall not be mutually exclusive. The exercise by City of one or more of the rights and remedies under this Agreement shall not preclude the exercise of any other right or remedy. Damages at law may not be an adequate remedy for a breach or threatened breach of this Agreement and in the event of a breach or threatened breach of any provision hereunder the respective rights and obligations hereunder shall be enforceable by specific performance, injunction or other equitable remedy.

#### 8. WAIVER

Any waiver of any covenant or condition of this Agreement shall be in writing signed by the party waiving the right, shall extend to the particular case only, and only in the manner specified, and shall not be construed as applying to or in any way waiving any further or other rights hereunder. The exercise of any of the options aforesaid shall not be construed as a waiver of City's right to recover actual damages for any breach in an action at law, or to restrain any breach or threatened breach in equity or otherwise.

#### 9. MISCELLANEOUS

- 9.1. This Agreement constitutes the entire agreement between the parties in respect of the facilities, and there are no oral agreements between the parties in connection herewith.
- 9.2. This Agreement shall be governed by the law of the state of Florida.
- 9.3. In the event that any provision of this Agreement is held to be invalid or unenforceable, such invalidity or unenforceability shall not affect the remainder of the Agreement which shall remain in full force and effect and shall be construed as though they had not contained the invalid or unenforceable provision.

- 9.4. The covenants herein shall be binding upon, and the rights hereunder shall inure to the benefit of the parties, their personal representatives, successors and assigns, except that Regal's rights shall inure only to those assigns for which Regal has obtained City's prior written consent in accordance with Article 5 of this Agreement.
- 9.5. The provisions of the Agreement shall not be construed to create a joint venture or partnership between City and Regal.
- 9.6. City and Regal agree that neither this Agreement nor any memorandum thereof shall be recorded.
- 9.7. Regal will notify the City at least thirty (30) days prior to any event of how many boats are to be launched, their size and weight, and information on the size and weight of the vehicles transporting the boats
- 9.8. Regal will request a police escort for all oversized vehicles as determined by the Belle Isle Police Department.
- 9.9. Regal will pay for all police escorts prior to, and after, the events.
- 9.10. The City and Regal will perform a joint inspection of Venetian Boat Ramp prior to launching and after retrieving boats to determine if any damage was done by Regal to the ramp and its facilities. Any damage done by Regal will be repaired by Regal within fourteen (14) days of the damage.
- 9.11. Regal will be solely responsible to pay for any and all damages done by Regal, its employees, or contractors

IN WITNESS WHEREOF, the parties hereto have set forth their hands and seals below.





# special events committee

JUN 17 '19 AM 10:16

b.

## CITY OF BELLE ISLE SPECIAL EVENTS COMMITTEE MEMBERSHIP APPLICATION

The Special Events Committee's purpose is to organize, plan and prepare for "special events" that the City can either host or attend. If you are interested in becoming part of this committee, please email Yolanda Quiceno, City Clerk a completed application at [yquiceno@belleislefl.gov](mailto:yquiceno@belleislefl.gov).

Name: Betty Lynn DeJarnette  
Home Address: 6513 Matchett Rd Belle Isle, FL 32809  
Home Phone: 407-855-7416 Cell Phone: 813-638-2058  
Email: delsinsmom@gmail.com Fax: \_\_\_\_\_

1. Will you have time to fulfill the duties of this committee? ☒ Yes ☐ No
2. Are you able to attend the necessary meetings? ☒ Yes ☐ No
3. Describe your community involvement experience and or any special expertise you have which would be applicable to this committee.

10 yrs of working with the Boy Scouts of America as Committee Chair for local Packs & Troops. Also, worked with the council to put on programs like summer day camp. Also, work for several years (over)

4. Describe why you are interested in serving on the Special Events Committee:

I want to be involved in the community and I think I have the skills that would be very useful to this committee

By signing below, you are affirming to the best of your knowledge that the information you have provided on this form is true and complete.

Signature: Betty L DeJarnette

Date: 6/14/19

3. as Rental Chairman for the Pine Castle Women's Club. For the Women's Club I had to develop rental policies, meet club expectations, report to the club's board, and bring special requests from potential renters to the board. I have experience in planning events and getting donations from the community for those events.