



May 14, 2025 Planning Commission Meeting Agenda

May 14, 2025 at 5:30 PM

598 Main Street, Bay St. Louis, MS 39520

Call to Order

Minutes Approval

1. Motion to approve the Minutes of April 16, 2025.

Action Items

2. **GRAFF** - Application for Variance to the Zoning Ordinance Submitted by Adam and Regina Graff. The applicants are wanting to construct a dwelling on the property. The applicants are requesting a variance of 10', resulting in a 15' setback to the front yard. The property is located at 4137 Cardinal Drive, Parcel Number 135N-1-39-233.000, 29 & 30 BLK 615 UN 6 ADD 3 SHORELINE PARK and is zoned R-1A Residential Single Family District.
3. **AJAX LLC** - Application for Special Use District. The applicant is requesting a Special Use District to allow the use of Recreational Vehicle Park located on Chapman Road. Parcel 138H-0-46-028.000, Legal Description PT GADON TOULME CLAIM PT SE 1/4 SEC 27-8S-14W and is zoned R-1A Single Family District.
4. **PHIL SIM PROPERTIES** - Application for Variance to the Zoning Ordinance Submitted by Jason Phillips with Phil Sim Properties. The applicant is wanting to construct a dwelling on the property. The applicant is requesting a variance of 1', resulting in an 11' setback to the side yard on the Alabama Street side of the proposed dwelling. The property is located at 5051 Georgia Street, Parcel Number 139A-0-40-099.001, 1 BLK 314 UN 3 ADD 1 SHORELINE PARK S/D and is zoned R-1A Residential Single Family District.
5. **DYESS** – Application for variance to the zoning ordinance submitted by Larry Dyess Jr. The applicant is requesting to subdivide one parcel into two new parcels of land. Both parcels will not meet the required lot area or lot width. Parcel 1 will need a

variance of 932 sq ft, resulting in 11,068 sq ft. to the minimum lot area and a variance of 26', resulting in a total of 74' to the minimum lot width. Parcel 2 will need a variance of 938 sq ft, resulting in 11,062 sq ft to the minimum lot area and a variance of 26', resulting in a total of 74' to the minimum lot width. The property is located at 118 Engman Avenue. Parcel 136H-1-37-021.000; 5J, REAR FIRST WARD and is zoned R-1 Single-Family District.

- 6. STIEFFEL & LIOKIS** – Application for variance to the zoning ordinance submitted by Stieffel and Liokis. The applicants are wanting to reconfigure two parcels into one new parcel of land. The new parcel will meet the required lot area; however, the applicant is requesting a variance of 21.4' resulting in a total of 78.6' to the minimum lot width. The property is located on the 400 block of Sycamore Street. Parcel 149M-1-30-126.001; 186 & E 18.6 OF 187 3RD WARD BAY ST LOUIS and 137J-0-44-053.000; 100C,108,109B,110,111 3RD WARD BAY ST LOUIS. The property is zoned R-1 Single-Family District.
- 7. STIEFFEL & LIOKIS** – Application for variance to the zoning ordinance submitted by Matt Stieffel and Louie Liokis. The applicants are wanting to reconfigure three parcels into new parcels of land. Parcel A will need a variance of 40' resulting in a total of 60' to the minimum lot width and Parcel B will need a variance of 41', resulting in a total of 59' to the minimum lot width. The property is located on the 500 Block of St Francis Street. Parcel 137J-0-44-057.000; 100D 3RD WARD BAY ST LOUIS and 137J-0-44-053.000; 100C,108,109B,110,111 3RD WARD BAY ST LOUIS and Parcel 137J-0-44-056.000; 109A 3RD WARD BAY ST LOUIS. The property is zoned R-1 Single-Family District.
- 8. BELLAMARE DEVELOPMENT** – Application for minor Site Plan Review for a convenience store and filling station with an attached coffee shop submitted by Bellamare Development. The property in question is located at 1083 HWY 90 and is identified on the Hancock County Land Rolls as Parcel No. 137F-2-26-009.000 (Legal Description: PT 88 J BOUQUIE CL (BUS) HWY 90) and Parcel No. 137F-2-26-010.000 (Legal Description: PT 89 J BOUQUIE CL 26-8-14). The property is zoned C-3 Highway Commercial District.
- 9. *WITHDRAWN* WILLIAMS** - A public hearing for a minor site plan review of a commercial structure that will be used for a dog daycare, boarding facility, and apartment. The property in question is located at 297A HWY 90, Bay St Louis and is

identified on the Hancock County Land Rolls as Parcel 149D-1-21-011.003 and described as PT 233 & 236 1ST WARD BAY ST LOUIS. The property in question is zoned C-1, Central Business District.

- 10.** Motion to recommend the Bay St Louis City Council approve and implement Tax Increment Financing Redevelopment Plan, Bay St. Louis, Mississippi, 2025 conforms with the comprehensive plan of the City of Bay St. Louis adopted on July 02, 2024.

Adjourn

- 11.** Motion to adjourn the meeting of May 14, 2025.



Planning and Zoning Meeting Minutes

April 16, 2025 at 5:30 PM

598 Main Street, Bay St. Louis, MS 39520

Call to Order

PRESENT

- Chairman Amy Doescher
- Commissioner John Romano
- Commissioner Dean Agee
- Commissioner Mikayla Brown
- Commissioner MJ Krankey

ABSENT

- Commissioner Clark Breland
- Commissioner Chet LeBlanc

Minutes Approval

1. Motion to approve the minutes of March 12, 2025.

Motion made by Commissioner Krankey, Seconded by Commissioner Romano.
 Voting Yea: Chairman Doescher, Commissioner Romano, Commissioner Agee,
 Commissioner Brown, Commissioner Krankey

APPROVED

Action Items

2. SELLERS - Application for Variance to the Zoning Ordinance Submitted by Rick Sellers. The applicant wants to construct a carport in front of the dwelling. The applicant requests a front yard variance of 11' resulting in a 14' setback to the front yard. The property is located at 11080 New York Street, Parcel Number 135N-2-39-093.000; 54 BLK 337 UN 3 ADD 1 SHORELINE PARK. The property is zoned R-1A Residential Single Family.

Rick Sellers spoke representing the application.

Motion made by Commissioner Brown, Seconded by Commissioner Romano.
 Voting Yea: Chairman Doescher, Commissioner Romano, Commissioner Brown,
 Commissioner Krankey

Voting Nay: Commissioner Agee

APPROVED

3. **YOUNG** - Application for Variance to the Zoning Ordinance Submitted by Randy Young. The application is adding an addition to the rear of the existing dwelling. The application requests a variance of 9'9", resulting in a 10'3" setback to the rear yard. The property is located at 135 DeMontluzin Avenue, Parcel Number 149F-0-29-209.000; 17 & 18 Beach Front Subdivision. The property is zoned R-2 Residential Two-Family.

Randy Young spoke representing the application.

Motion made by Commissioner Krankey, Seconded by Commissioner Agee.

Voting Yea: Chairman Doescher, Commissioner Romano, Commissioner Agee, Commissioner Brown, Commissioner Krankey

APPROVED

4. **CORBITTNICAUD LLC** - Application for minor Site Plan Review for a Boutique Hotel submitted by Merritt Nicaud. The property in question is located at 105 North Beach Blvd is identified on the Hancock County Land Rolls as Parcel No. 149L-0-29-035.000 legal description as 510 1ST WARD BAY ST LOUIS and parcel NO 149L-0-29-036.000 Legal District PT 512 1ST WARD BAY ST LOUIS. The property is zoned C-1, Central Business District.

TABLED

5. **BELLAMARE DEVELOPMENT** – Application for minor Site Plan Review for a convenience store and filling station with an attached coffee shop submitted by Bellamare Development. The property in question is located at 1083 HWY 90 and is identified on the Hancock County Land Rolls as Parcel No. 137F-2-26-009.000 (Legal Description: PT 88 J BOUQUIE CL (BUS) HWY 90) and Parcel No. 137F-2-26-010.000 (Legal Description: PT 89 J BOUQUIE CL 26-8-14). The property is zoned C-3 Highway Commercial District.

TABLED

Adjourn

6. Motion to adjourn the meeting of April 16, 2025.

Motion made by Commissioner Agee, Seconded by Commissioner Romano.

Voting Yea: Chairman Doescher, Commissioner Romano, Commissioner Agee, Commissioner Brown, Commissioner Krankey

APPROVED

Amy Doescher, Chairman

Date

Caitlin Bourgeois, Secretary

Date

TO: Planning and Zoning Commission
City of Bay St. Louis

RE: 4137 Cardinal Street
135N-1-39-233.000
29 & 30 BLK 615 UN 6 ADD 3 SHORELINE PARK

HEARING DATE: May 14, 2025

I reviewed Adam and Regina Graff's application for a Variance to the Zoning Ordinance. The property is 4137 Cardinal Street, in the R-1A Single Family District. The district requires a 25-foot front yard setback, an 8-foot side yard setback, and a 10-foot rear yard setback since the property is on a canal.

The applicant requests a front yard to construct a dwelling.

Front Yard Setback:

Required: 25'

Proposed Distance of rear yard: 15'

Variance Request: 10'

The administration recommends approving of the variance.

- The parcel is at the dead end of Cardinal so that it will be the last house on Cardinal Street
- The house will be 25' from the road edge, so just passing by, the house will appear to have a 25' setback.
- The rear property line is in the canal, so a 10-foot rear yard setback will nearly put the piles in the canal.
- The zoning department received a letter of support from 8 neighbors in the public notice area.

If there is anything else I can help with in this matter, please feel free to call my office at 228-466-5516.

APPLICATION FOR VARIANCE TO THE ZONING ORDINANCE

Please complete this form in its entirety; failure to do so may cause a delay in the submittal of your application to the Planning and Zoning Commission.

The following information is required before this application will be submitted to the Planning and Zoning Commission for consideration.

OWNER: Adam + Regina Graff

ADDRESS: 1400 Wisteria Drive
Metairie, LA 70005

PHONE: (504) 905-6799

ADDRESS OF PROPERTY IN QUESTION IF DIFFERENT FROM ADDRESS STATED ABOVE

4137 Cardinal Dr., Bay St. Louis (This is the last property on the block, a dead end, with no possibility of any development across the street.)

1. Legal description of property to be considered for variance as described in the Hancock County tax rolls:

29N30BLK 615UN6add3

2. Parcel number(s) as described in the Hancock County tax rolls:

135N-1-39-233.000 Lot 29

3. Present Zoning: B1-A

4. Present use of building/property: Single family residential home

5. Application fee of \$100 (Residential): \$250 #00533735

Application fee of \$200 (Commercial): N/A

Article XIII
1303 APPEALS, HEARING AND NOTICE

Every appeal or application shall refer to the specific provision of the ordinance involved and shall set forth the interpretation that is claimed, the use for which a special exception is sought, or the details of the variance that is applied for and the case may be, and accompanied by a plat or plan, drawn to scale, showing the actual dimensions of the parcel of land to be built upon and used, the size of any building to be erected, and the location of the building upon the lot, the materials to be used and other such information as may be deemed necessary to provide full information regarding intended use. The Municipal Clerk shall forthwith transmit the appeal or application to the Commission together with all papers constituting the record upon which the action appealed from was taken.

Article XIII
1305.3 VARIANCES

To recommend such variances from the terms of this Ordinance as will not be contrary to the public interest where, owing to special conditions, literal enforcement of the provisions of this Ordinance will in an individual case result in unnecessary hardship, so that the spirit of the Ordinance shall be observed, public safety and welfare secured, and substantial justice be done. Such variance may be granted in such case of unnecessary hardship upon a finding by the City Council that all of the following conditions exist:

- A. There are extraordinary and exceptional conditions which pertain to the particular piece of property in question because of its size, shape, or topography that are not applicable to other lands or structures in the same district.
- B. The literal interpretation of the provisions of this Ordinance would deprive the applicant of rights commonly enjoyed by other residents of the district in which the property is located.
- C. Granting the variance requested will not confer upon the applicant any special privileges that are denied to other residents of the district in which the property is located.
- D. The requested variance will be in harmony with the purpose and intent of this Ordinance and will not be injurious to the neighborhood or to the general welfare.
- E. The special circumstances are not the result of the actions of the applicant.
- F. The existence of a nonconforming use of neighboring land, buildings, or structures in the same district or of permitted or nonconforming uses in other districts shall not constitute a reason for the requested variance.
- G. The variance requested is the minimum variance that will make possible the legal use of the land, building, or structure.
- H. The variance is not a request to permit a use of land, building, or structure which is not permitted by right or by special exception in the district involved.

This is a dead end street/location with no possibility of build on opposite side of the street.

- I. Notice of public hearing shall be given as in Section 1305.2(A).
- J. The variance can't be transferred to a subsequent owner of the property, if the variance is unused.
- K. The grant of a variance shall expire if the variance has not been activated within six (6) months of final approval. "Activation" shall mean obtaining a building permit for the required or necessary construction. In addition, the activation shall not be effective unless the construction is completed within six (6) months of obtaining the building permit. On good cause shown, the Bay St. Louis City Council may extend the above stated time limits for up to a maximum of six (6) months.

Please submit the following documentation with your application:

- 1. What is the specific provision of the ordinance involved *Residential Zoning - Development Standards - set Back distance from street vs. property line*
- 2. The use for which a variance is sought *request is to build the house 25 feet from the street, rather than 25 feet from the property line behind the street*
- 3. If request is for a setback variance, please answer the following:

25 feet Front yard setback requirement *25 feet from property line*
15 feet Proposed distance remaining to the property line *15 feet from property line, which is 25 feet from the street*
10 feet Total front yard setback variance needed

15 feet Side yard setback requirement
 Proposed distance remaining to the property line
N/A Total side yard setback variance needed

10 feet Rear yard setback requirement *10 feet from water*
 Proposed distance remaining to the property line
N/A Total rear yard setback variance needed

- 4. If request is for a variance other than setback, please answer the following:

N/A Required total square footage of lot
 Proposed square footage of lot
 Total square footage needed to lot

Required minimum width of lot
 Proposed minimum width of lot
 Total variance to minimum lot width needed

Required fence height
 Proposed fence height
 Total fence height variance needed

5. Other type(s) of variance needed:

None

6. A plat or plan, drawn to scale, showing the actual dimensions of the parcel of land to be built upon.

7. Size of any building to be erected, and the location of the building upon the lot.

2,926 square foot of single family home

8. Materials to be used (Exterior Finish) and other such information as may be deemed necessary to provide full information regarding intended use.

9. Is the property in question in a sub-division? No

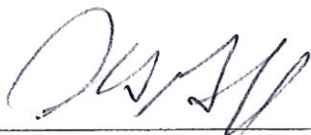
10. If the property in question is within a sub-division, is there an existing covenant running with the land? N/A

11. If the answer to question 9 is yes, please state the book and page numbers where the stated restrictive covenants are filed in the Chancery Clerks Office of Hancock County. N/A

Book Number _____

Page Number _____

It is warranted in good faith by the owners whose name is signed hereto that all of the above facts are true and correct.


Applicant's Signature

3/31/25
Date

FOR OFFICE USE ONLY

Date of Application received: _____

~~200~~ F-25' Rear - 10' water & Right & Left 20' No water

SURVEY DESCRIPTION

PARCEL OF LAND LOCATED IN THE EAST PARK SUBDIVISION UNIT 6 ADDITION 3 HANCOCK COUNTY, MISSISSIPPI

LOTS 27-30

COMMENCING AT A IRON ROD AT THE NORTHEAST CORNER OF LOT 26 IN THE EAST PARK SUBDIVISION UNIT 6 ADDITION 3 HANCOCK COUNTY MISSISSIPPI
 THENCE: N54°16'45"W FOR 50.00 FEET ALONG THE NORTHERLY MARGIN OF CARDINAL STREET TO A IRON ROD
 THENCE: N54°16'45"W FOR 25.00 FEET ALONG SAID MARGIN TO A IRON ROD
 THENCE: N54°16'45"W FOR 73.45 FEET TO A IRON ROD
 THENCE: N54°16'45"W FOR 94.02 FEET TO A IRON ROD
 THENCE: S88°47'59"E FOR 164.04 FEET TO A IRON ROD WITNESS CORNER
 THENCE: S88°47'59"E FOR 27.98 FEET TO A POINT
 THENCE: S56°27'40"E FOR 253.00 FEET TO A IRON ROD
 THENCE: S56°27'40"E FOR 74.27 FEET TO A IRON ROD
 THENCE: S56°27'40"E FOR 25.00 FEET TO A IRON ROD
 THENCE: N34°54'26"E FOR 98.70 FEET TO THE POINT OF BEGINNING SAID PARCEL CONTAINS 27750 SQUARE FEET MORE OR LESS.

REFERENCES

1. DEED BOOK 2020 PAGE 10609
2. DEED BOOK 2016 PAGE 5507
3. DEED BOOK 2014 PAGE 10365
4. PLAT OF RECORD ON FILE AT HANCOCK COUNTY CLERKS OFFICE
5. HANCOCK COUNTY TAX MAP 135N

LEGEND

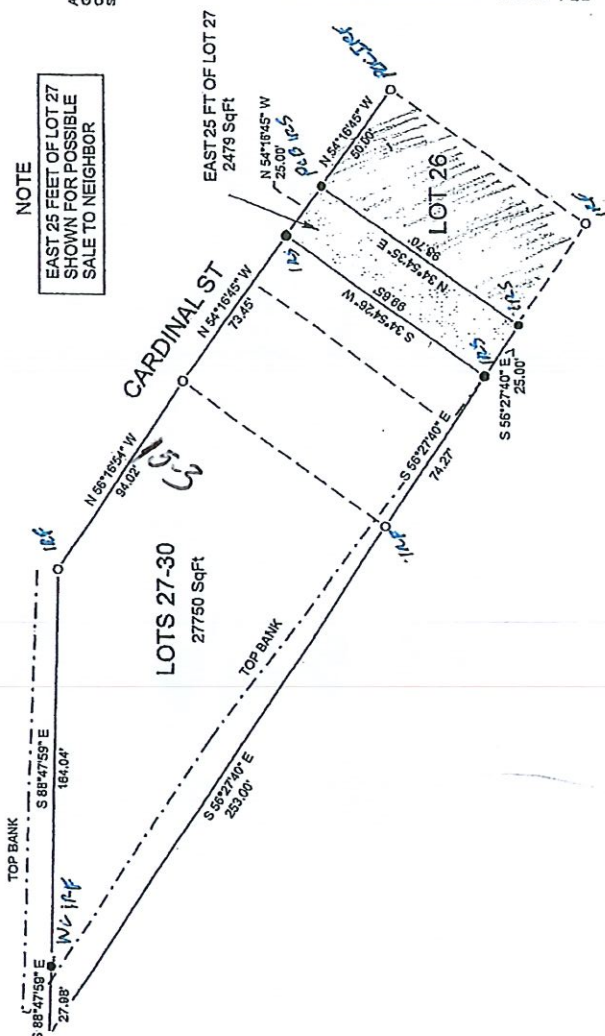
●	IRS = 1/2 INCH IRON ROD SET W/ORANGE CAP MARKED RIED ASSOC PLS 3037
●	WC IRS = 1/2 INCH IRON ROD WITNESS CORNER SET W/ ORANGE CAP MARKED RIED ASSOC PLS 3037
○	IRF = 1/2 IRON ROD FOUND
(M)	MEASURED RECORD
(R)	RECORD
---	LOT LINES
---	TOP BANK

EAST 25 FT. OF LOT 27
 COMMENCING AT A IRON ROD AT THE NORTHEAST CORNER OF LOT 26 IN THE EAST PARK SUBDIVISION UNIT 6 ADDITION 3 HANCOCK COUNTY MISSISSIPPI
 THENCE: N54°16'45"W FOR 50.00 FEET ALONG THE NORTHERLY MARGIN OF CARDINAL STREET TO A IRON ROD
 THENCE: N54°16'45"W FOR 25.00 FEET ALONG SAID MARGIN TO A IRON ROD
 THENCE: S34°54'26"W FOR 99.65 FEET TO A IRON ROD
 THENCE: S56°27'40"E FOR 25.00 FEET TO A IRON ROD
 THENCE: N34°54'35"E FOR 98.70 FEET TO THE POINT OF BEGINNING SAID PARCEL CONTAINS 2749 SQUARE FEET MORE OR LESS.



ALL BEARINGS SHOWN ARE GRID
 GEOID 128US
 CONVERGENCE ANGLE -0°17'19"
 STATE PLANE ZONE 18S EAST

NOTE
 EAST 25 FEET OF LOT 27 SHOWN FOR POSSIBLE SALE TO NEIGHBOR



THIS IS BASED ON INFORMATION PROVIDED BY THE CLIENT. SURVEYOR HAS MADE NO INVESTIGATION OR INDEPENDENT SEARCH TO DETERMINE THE EXISTENCE OF RESTRICTIVE COVENANTS, OWNERSHIP OF TITLE EVIDENCE, OR ANY OTHER FACTS THAT COULD AFFECT THE SURVEY. THE SURVEYOR WILL ONLY IF PLAT HAS ORIGINAL SEAL AND SIGNATURE OF THE SURVEYOR
 NO ATTEMPT HAS BEEN MADE AS PART OF THIS SURVEY TO LOCATE OR IDENTIFY ANY SHOWN OR CONCEALED UTILITIES. THE SURVEYOR MAKES NO WARRANTY AS TO THE SIZE, DEPTH, CONDITION OR LOCATION OF ANY UTILITY OF PUBLIC SERVICE UTILITY IN CONSIDERATION OF A FEE PAID TO DECLARE THAT THIS SURVEY WAS DONE BY ME OR UNDER MY SUPERVISION AND TO THE BEST OF MY KNOWLEDGE AND BELIEF.
 THIS SURVEY MEETS THE MISSISSIPPI MINIMUM STANDARDS FOR A CLASS B SURVEY

RIED & ASSOCIATES LLC
 9526 BENEHEEWAH TRAIL
 PASSCHRISTIAN, MS 39571
 PHONE 228 205-4007

SURVEY OF LOTS 27-30 LOCATED IN THE EAST PARK SUBDIVISION UNIT 6 ADDITION 3 HANCOCK COUNTY MISSISSIPPI

SCALE	50 FT/IN	SURVEY DATE	09/29/23	DRAWN BY	DER
JOB	23-122	DRAWING DATE	10/03/23	CHECKED BY	MLSR





x,y

CLA HN

150

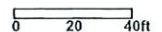
75

SMITH THOMAS L

Parcels
135N-1-39-
234.000

Parcel Number: 135N-1-39-234.000
 Owner Name: GRAFF ADAM H IV ETAL
 Owner Address: 1400 WISTERIA DR
 Owner City, State ZIP: METAIRIE, LA
 70005
 Physical Address: 4137 CARDINAL ST
 Improvement Type:
 Year Built: 0
 Base Area: 0
 Adjusted Area: 0
 Actual Total Value: 20000
 Taxable Total Value: 0
 Estimated Tax: 359.1
 Homestead Exemption: No
 Deed Book: 2023
 Deed Page: 10743
 Legal Description 1: 27 & 28 BLK 615 UN

Close Export



Letter of Support for Zoning Variance

To Whom It May Concern,

We, the undersigned, are neighbors of Adam and Regina Graff, who are building a new home at 4137 Cardinal Street, Bay St. Louis, MS. We understand that they are applying for a variance from the city of Bay St. Louis Zoning Board for the following request:

A 10 foot variance for the home to be built 15 feet behind the street, rather than the code requirement of 15 feet behind the property line.

We have reviewed the proposed change and have no objections. In fact, we support this request and believe it will not negatively impact the neighborhood or our property. We are confident that the proposed changes will be completed in a manner that is consistent with the character and appearance of our community.

We respectfully request that the Zoning Board take our support into consideration when reviewing this application.

Sincerely,

Name: Robert Doyle Robert Doyle

Address: 4129 Cardinal St, Bay St Louis MS 39520

Signature: Robert Doyle

Date: 4/20/25

Letter of Support for Zoning Variance

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We, the undersigned, are neighbors of Adam and Regina Graff, who are building a new home at 4137 Cardinal Street, Bay St. Louis, MS. We understand that they are applying for a variance from the city of Bay St. Louis Zoning Board for the following request:

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We respectfully request that the Zoning Board take our support into consideration when reviewing this application.

Sincerely,

Name: Andy Williams

Address: 4056 Robin St. Bay St. Louis, MS

Signature: Andy Williams

Date: 4-19-25

Letter of Support for Zoning Variance


To Whom It May Concern,

We, the undersigned, are neighbors of Adam and Regina Graff, who are building a new home at 4137 Cardinal Street, Bay St. Louis, MS. We understand that they are applying for a variance from the city of Bay St. Louis Zoning Board for the following request:

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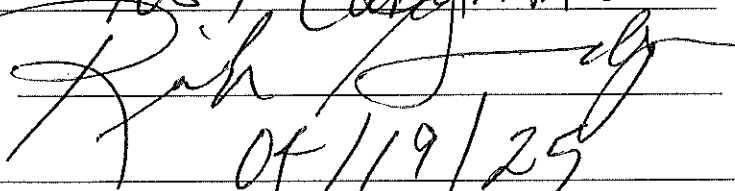
We have reviewed the proposed change and have no objections. In fact, we support this request and believe it will not negatively impact the neighborhood or our property. We are confident that the proposed changes will be completed in a manner that is consistent with the character and appearance of our community.

We respectfully request that the Zoning Board take our support into consideration when reviewing this application.

Sincerely, 

Name: Rick Brady

Address: 4084 Cardinal St

Signature: 

Date: 04/19/25

Letter of Support for Zoning Variance

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We respectfully request that the Zoning Board take our support into consideration when reviewing this application.

Sincerely,

Name: J HOWARD PERTUIT

Address: 4107 ROBIN ST.

Signature: J Howard Pertuit

Date: 19 APRIL 2025

Letter of Support for Zoning Variance

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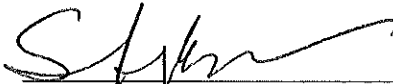
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We respectfully request that the Zoning Board take our support into consideration when reviewing this application.

Sincerely,

Name: STEVE BROWN

Address: 4091 CARDINAL ST

Signature: 

Date: 4-19-25

Letter of Support for Zoning Variance

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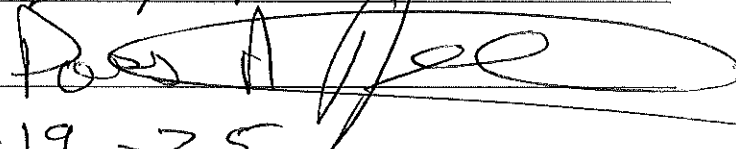
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We respectfully request that the Zoning Board take our support into consideration when reviewing this application.

Sincerely,

Name: 

Address: ~~1612~~ 4059 Robinson

Signature: 

Date: 4-19-25

Letter of Support for Zoning Variance

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We, the undersigned, are neighbors of Adam and Regina Graff, who are building a new home at 4137 Cardinal Street, Bay St. Louis, MS. We understand that they are applying for a variance from the city of Bay St. Louis Zoning Board for the following request:

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We respectfully request that the Zoning Board take our support into consideration when reviewing this application.

Sincerely,
Name: Eddie Laviolette

Address: 4097 CARDINAL STREET, Bay St. Louis, MS.

Signature: EDDIE LAVIOLETTE

Date: April 19, 2025

Letter of Support for Zoning Variance

To Whom It May Concern,

We, the undersigned, are neighbors of Adam and Regina Graff, who are building a new home at 4137 Cardinal Street, Bay St. Louis, MS. We understand that they are applying for a variance from the city of Bay St. Louis Zoning Board for the following request:

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We respectfully request that the Zoning Board take our support into consideration when reviewing this application.

Sincerely,

Name: GEORGE DELPIDIO III

Address: 4144 Robin Bay St. Louis MS

Signature: 

Date: 4-21-25

TO: Planning and Zoning Board
City of Bay St. Louis

RE: 10160 Chapman Road
138H-0-46-028.000
Legal Description: PT GADON TOULME CLAIM PT SE 1/4 SEC
27-8S-14W

HEARING DATE: May 14, 2025

I reviewed Rust Enterprises' application for a Special Use District. The property is 10160 Chapman Road, located in the R-1A Single Family District.

The applicant is requesting approval for a Special Use District to allow the use of a Recreational Vehicle Park.

It is important to note that this property previously had a Special Use District overlay approved for a Recreational Vehicle Park under a former owner. However, the previous owner did not develop the park, and the overlay has since expired.

It is important to note that this property previously had a Special Use District overlay approved for an RV Park under a former owner. However, the park was never developed, and the overlay has since expired.

The proposed RV Park is defined under the Zoning Ordinance as follows:

- Section 302.124 – Recreational Vehicle Park: A recreational vehicle park primarily occupied by recreational vehicles, including seasonal tourist facilities.
- Section 302.123 – Recreational Vehicle: A vehicular type unit primarily designed as temporary living quarters for recreation, camping, or travel use, which either has its own motor power or is mounted on or drawn by another vehicle. This includes travel trailers, camping trailers, and motor homes. Recreational vehicles shall not be used as permanent dwellings and must be located in a designated RV park unless being stored on property that includes a primary dwelling.

According to the application, the proposed RV Park will include paved streets and lighting, as well as amenities like a swimming pool, lazy river, pickleball courts, and open space.

The developer has indicated that the investment to develop this RV Park will be approximately \$5,000,000. Engineering has begun. Following approval of the Special

Use District, the developer will return for Major Site Plan Review, with an anticipated opening for the project by Labor Day 2026. The project financing is in place.

In accordance with Section 1008 of the Bay St. Louis Zoning Ordinance, the applicant will be required to return for a Major Site Plan Review. At that time, a detailed site plan must be submitted demonstrating compliance with the following standards for RV Parks:

- RV Parks must remain distinct from Manufactured Home Parks.
- Each RV site must be at least 1,500 square feet in size, with no more than 15 sites per acre.
- Each site must be at least 15 feet wide and provide a 12 ft. x 30 ft. hard-surfaced or graveled parking pad.
- A minimum of 1.5 parking spaces must be provided per site, with at least one on-site space.
- A 12-foot-wide graveled path must connect the road to each pad.
- The park must include a 6-foot opaque screen along all adjoining uses and lower hedge landscaping along any street-facing sides.
- Utilities must include electric, water, sewer, and solid waste service. Common restroom and shower facilities are required for sites that allow RVs without internal facilities.
- The maximum occupancy for any RV site is 180 days.
- No permanent structures are to be installed at RV sites, aside from accessory structures for park management or recreational use.
- Any additional conditions deemed necessary by the City Council to ensure compatibility with the neighborhood may also be applied.

(SECTION 1008: RECREATIONAL VEHICLE PARKS BSL ZONING ORDINANCE)

- A resort RV Park is highest and best use of the property versus single family housing. The low elevation of this ground makes it prohibitive to typical single-family residential use.

- The proposed RV Park will enhance recreational use for the City of Bay St. Louis, mainly due to its proximity to the BAYS soccer fields and a 9-acre property of Hancock County, both of which are public and recreational in nature.
- The project will increase property tax revenues in the area, converting the property from its current annual tax base of \$1,583 to a much higher taxable value upon completion.
- Longfellow Road will provide traffic access, located within a commercial district and will not impact residential neighborhoods, keeping the character of neighboring residential neighborhoods intact.

If I can be of any further assistance in this matter, please feel free to call my office at 228-466-5516.

- Jeremy Burke

APPLICATION FOR SPECIAL USE DISTRICT

Please complete this form in its entirety; failure to do so may cause a delay in the submittal of your application to the Planning and Zoning Commission.

The following information is required before this application will be submitted to the Planning and Zoning Commission for consideration.

OWNER: AJAX LLC

ADDRESS: 10160 Chapman Road Bay St. Louis, MS 39520

PHONE: 8018752430

ADDRESS OF PROPERTY IN QUESTION IF DIFFERENT FROM ADDRESS STATED ABOVE

1. Legal description of property to be considered for variance (as described in the Hancock County tax rolls):

Legal Description 1: PT GADON TOULME CLAIM

Legal Description 2: PT SE 1/4 SEC 27-8S-14W

2. Parcel number(s) as described in the Hancock County tax rolls):

138H-0-46-028.000

3. Present Zoning: R1A

4. Present use of Building/Property Vacant


5. Application fee of ~~8000~~³²⁵⁰.00 00533770

6. The use for which a Special Use District is sought:

200 Space Luxury RV Resort. All paved roads and spaces. Landscaping and lighting throughout. Pickleball, pool, green areas.

- A. Plans showing location and design of structures, delivery points, loading and storage areas, walls, fences, screening, landscaping, signs lighting devices, and pedestrian walks.
- B. Plans illustrating adequate off-street parking according to standards established for parking.
- C. Plans showing entrance and exits to the area and the traffic routing system so designed as to minimize nuisance effects due to the generation of traffic to and from the area.
- D. Any other information the Planning and Zoning Commission may need to adequately consider the effect the proposed uses may have upon the cost of providing adequate services to the area.
- E. The City Council after recommendation by the Planning and Zoning Commission may attach reasonable special conditions to the approval of such district or amendments to insure there will be no departure from the intent of the Zoning Ordinance.
- F. All proposed Special Use Districts shall follow the procedures for subdivision approval even though the ownership of land may not be divided, and follow the procedures for Site Plan Review.
 - 1. Development shall follow the Site Plan Review Process.
 - 2. A preliminary and final plat, both approved by the City Council, upon recommendation from the Planning and Zoning Commission, shall be required for each Special Use District.
 - 3. The district shall be developed according to the approval of the final plat.
 - 4. Building permits and certificate of occupancy shall be required for each building according to the existing building codes and regulations.

It is warranted in good faith by the owner whose name is signed hereto all of the above facts are true and correct.



Applicant's Signature

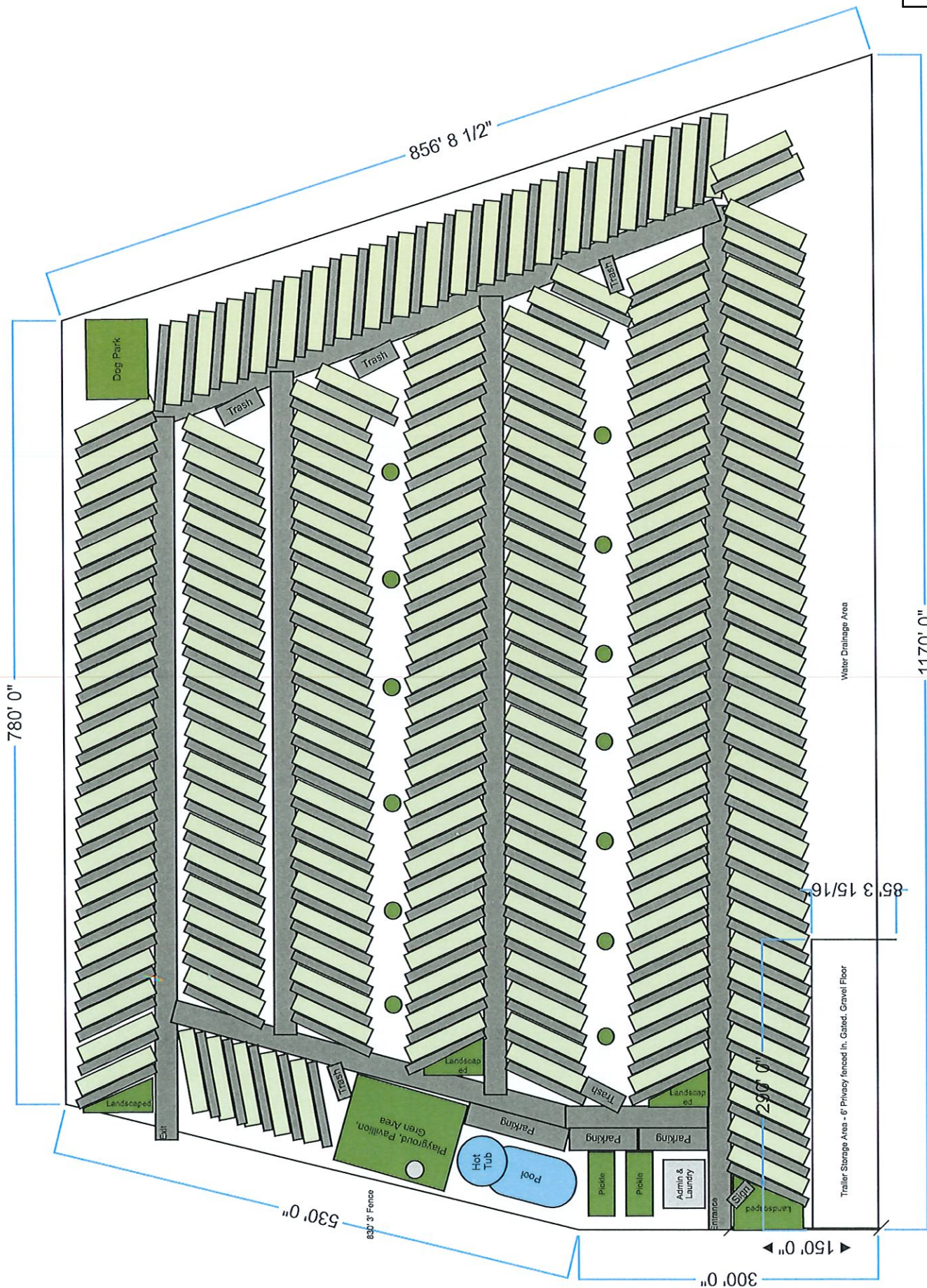
3/31/25

Date

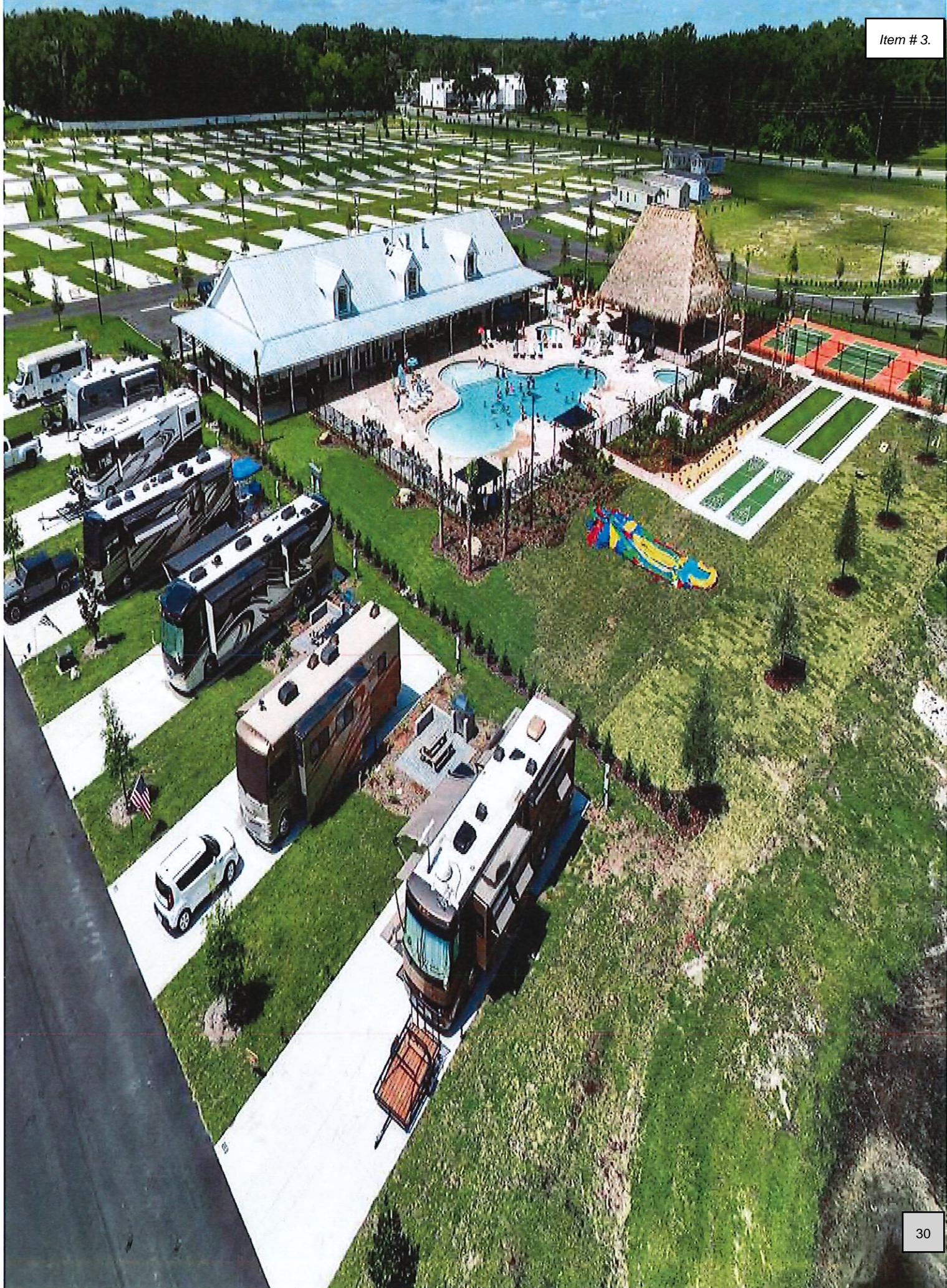
Chandler Rust (Rust Enterprises) is under contract on the above property and is acting on behalf of the current ownership.

FOR OFFICE USE ONLY

Date of Application: _____







BAY ST. LOUIS RV RESORT

Proposed Development Overview

Location Overview

10160 Chapman Road
Bay St. Louis, MS

Planned Opening:
Labor Day 2026

Ground Breaking:
Summer 2025

Due to elevation and street width, the only feasible use of this parcel is a recreational vehicle area. Our intent is to construct a Class A high end luxury RV resort. This would be the highest quality use of the parcel pictured.



Concept Overview

Sites: 200

Lot Sizes:

70x25 Back In (180)

100x25 Pull Through (20)

Paved Portions:

70x12 Back In (180)

100x12 Pull Through (20)

Trailer Portion:

45x12 Back In (180)

75x12 Pull Through (20)

Parking Portion:

25x12 (200)

Misc Parking Spaces



HIGHLIGHTS

Planned Investment: \$5,000,000

This RV **Resort** will feature 200 full hookup sites, picnic tables, eastern red cedar trees throughout, a swimming pool with lazy river, beach, and hot tub, two pickle-ball courts, playground, dog park, green areas and more!

Rules:

Quiet Hours between 9pm-9am

No Large Dog Breeds

No campers in disrepair

No debris or personal items in camping area

Recreational use only

No Manufactured Housing

Renderings:



COMMUNITY IMPACT

We already have plans to work closely with Bay Area Youth Soccer to provide discounted site rentals for traveling soccer teams as well as day pass use for our amenities to youth soccer teams to allow the cities youth league to begin hosting their own soccer tournaments.

Also planned are local food truck roundups and our participation in the Bay St. Louis parades and festivals.



MY EXPERIENCE



Chandler Rust
Owner/Operator/Developer



<https://www.linkedin.com/in/chandler-rust-343172145/>

Other Parks:

Valley View - Duncan, Arizona

Camp Bravo - Okeechobee, Florida

Park Lake - Okeechobee, Florida

Big Mesquite - Three Rivers, Texas

Pine Meadows - Blytheville, Arkansas

Shearin - Blytheville, Arkansas

River Hill - Brownsville, Tennessee

CLOSEST COMPARABLE



Previous Special Use Permit was approved for this parcel for the same use.

g) **ROBERT LAMB** – Application for Special Use District. The applicant is asking for approval of a Special Use District to be allowed to conduct a Recreational Vehicle Park which will be known as “Coast Village RV Resort”. The tract of land will consist of 200 sites. The property in question is located on Chapman Road; Parcel #138H-0-46-028.000, Proposed Green Space Coastal Gables Townhomes, Phase 1. Parcel #138H-0-46-043.000, 15 Coastal Gables Townhomes, Phase 1. Parcel #138H-0-46-057.000, 29 Coastal Gables Townhomes, Phase 1. Parcel #138H-0-46-069.000, 101 Coastal Gables Townhomes, Phase 1. Parcel #138H-0-46-132.000, 134 Coastal Gables Townhomes, Phase 1. Parcel #138H-0-46-297.000, Coastal Gables Townhomes Green Space/Drainage. The property is zoned R-1, Single Family District. These parcels of land are by and at large North, South, East, and West of the external boundaries of Parcels 138H-0-46-298.000 thru 138H-0-46-297.000, respectively. Recommend approval 7/0

Council Member Reed asked if anyone wanted to speak for or against the application. No one came forward.

Motion to accept the recommendation of the Bay Saint Louis Planning and Zoning Commission and approve the application for property located on Chapman Road for a Special Use District

Council Member Knoblock moved, seconded by Council Member DeSalvo, to accept the recommendation of the Bay Saint Louis Planning and Zoning Commission and approve the application for a Special Use District as requested by Robert Lamb, parcel #138H-0-46-028.000, parcel #138H-0-46-043.000, parcel #138H-0-46-057.000, parcel #138H-0-46-069.000, parcel #138H-0-46-132.000 and parcel #138H-0-46-297.000. These parcels of land are by and at large North, South, East, and West of the external boundaries of Parcels 138H-0-46-298.000 thru 138H-0-46-297.000, respectively.

A vote was called for with the following response:

VOTING YEA: DeSalvo, Zimmerman, Smith, Reed, Seal and Knoblock

VOTING NAY: None

ABSENT: Hoffman

TO: Planning and Zoning Commission
City of Bay St. Louis

RE: 5051 Georgia Street
139A-0-40-099.001
1 BLK 314 UN 3 ADD 1 SHORELINE PARK S/D

HEARING DATE: May 14, 2025

I reviewed Phil Sim Properties' application for a Variance to the Zoning Ordinance. The property is at 5051 Georgia Street, in the R-1A Single-Family District. The R-1A district requires a 25-foot front yard setback, an 8-foot side yard setback, and a 20-foot rear yard setback. The property is on the corner lot. Therefore, the side yard setback on the Alabama Street side of the property is 12 feet.

The applicant requests a side yard setback on the Alabama Street side of the house for the dwelling.

Side Yard Setback:

Required: 12' (corner lot)

Proposed Distance of side yard: 11'

Variance Request: 1'

The administration recommends approval of the variance.

- The subcontractor has already put the piling in the ground, and the contractor believes the footer was put 11'5" from the side yard property line.
- The property owner is requesting that in case the contractor accidentally placed the footer in the wrong location.
- The location of the house will not obstruct sight lines at the stop sign turning onto Alabama Street.

If I can be of any further assistance, please call my office at 228-466-5516.

Jeremy L Burke
Zoning Administrator

APPLICATION FOR VARIANCE TO THE ZONING ORDINANCE

Please complete this form in its entirety; failure to do so may cause a delay in the submittal of your application to the Planning and Zoning Commission.

The following information is required before this application will be submitted to the Planning and Zoning Commission for consideration.

OWNER: Phil-Sim properties Jason Phillips
 ADDRESS: 427 West Lake Rd
Hattiesburg, MS 39402
 PHONE: 601-596-8185

ADDRESS OF PROPERTY IN QUESTION IF DIFFERENT FROM ADDRESS STATED ABOVE

5051 Georgia St

1. Legal description of property to be considered for variance as described in the Hancock County tax rolls:

1 BLK 314 UN 3 ADD 1 SHORELINE

2. Parcel number(s) as described in the Hancock County tax rolls:

139A-0-40-099.001

3. Present Zoning: R-1A

4. Present use of building/property: vacant / under construction

5. Application fee of \$²⁵⁰~~100~~ (Residential): 00536182

Application fee of \$²⁵⁰~~200~~ (Commercial): _____

Article XIII
1303 APPEALS, HEARING AND NOTICE

Every appeal or application shall refer to the specific provision of the ordinance involved and shall set forth the interpretation that is claimed, the use for which a special exception is sought, or the details of the variance that is applied for and the case may be, and accompanied by a plat or plan, drawn to scale, showing the actual dimensions of the parcel of land to be built upon and used, the size of any building to be erected, and the location of the building upon the lot, the materials to be used and other such information as may be deemed necessary to provide full information regarding intended use. The Municipal Clerk shall forthwith transmit the appeal or application to the Commission together with all papers constituting the record upon which the action appealed from was taken.

Article XIII
1305.3 VARIANCES

To recommend such variances from the terms of this Ordinance as will not be contrary to the public interest where, owing to special conditions, literal enforcement of the provisions of this Ordinance will in an individual case result in unnecessary hardship, so that the spirit of the Ordinance shall be observed, public safety and welfare secured, and substantial justice be done. Such variance may be granted in such case of unnecessary hardship upon a finding by the City Council that all of the following conditions exist:

- A. There are extraordinary and exceptional conditions which pertain to the particular piece of property in question because of its size, shape, or topography that are not applicable to other lands or structures in the same district.
- B. The literal interpretation of the provisions of this Ordinance would deprive the applicant of rights commonly enjoyed by other residents of the district in which the property is located.
- C. Granting the variance requested will not confer upon the applicant any special privileges that are denied to other residents of the district in which the property is located.
- D. The requested variance will be in harmony with the purpose and intent of this Ordinance and will not be injurious to the neighborhood or to the general welfare.
- E. The special circumstances are not the result of the actions of the applicant.
- F. The existence of a nonconforming use of neighboring land, buildings, or structures in the same district or of permitted or nonconforming uses in other districts shall not constitute a reason for the requested variance.
- G. The variance requested is the minimum variance that will make possible the legal use of the land, building, or structure.
- H. The variance is not a request to permit a use of land, building, or structure which is not permitted by right or by special exception in the district involved.

- I. Notice of public hearing shall be given as in Section 1305.2(A).
- J. The variance can't be transferred to a subsequent owner of the property, if the variance is unused.
- K. The grant of a variance shall expire if the variance has not been activated within six (6) months of final approval. "Activation" shall mean obtaining a building permit for the required or necessary construction. In addition, the activation shall not be effective unless the construction is completed within six (6) months of obtaining the building permit. On good cause shown, the Bay St. Louis City Council may extend the above stated time limits for up to a maximum of six (6) months.

Please submit the following documentation with your application:

- 1. What is the specific provision of the ordinance involved _____
- 2. The use for which a variance is sought _____

3. If request is for a setback variance, please answer the following:

_____ Front yard setback requirement
 _____ Proposed distance remaining to the property line
 _____ Total front yard setback variance needed

(corner) 12' Side yard setback requirement
11' Proposed distance remaining to the property line
1' Total side yard setback variance needed

_____ Rear yard setback requirement
 _____ Proposed distance remaining to the property line
 _____ Total rear yard setback variance needed

4. If request is for a variance other than setback, please answer the following:

_____ Required total square footage of lot
 _____ Proposed square footage of lot
 _____ Total square footage needed to lot

_____ Required minimum width of lot
 _____ Proposed minimum width of lot
 _____ Total variance to minimum lot width needed

_____ Required fence height
 _____ Proposed fence height
 _____ Total fence height variance needed

5. Other type(s) of variance needed:

6. A plat or plan, drawn to scale, showing the actual dimensions of the parcel of land to be built upon.

7. Size of any building to be erected, and the location of the building upon the lot.

8. Materials to be used (Exterior Finish) and other such information as may be deemed necessary to provide full information regarding intended use.

9. Is the property in question in a sub-division? _____

10. If the property in question is within a sub-division, is there an existing covenant running with the land? _____

11. If the answer to question 9 is yes, please state the book and page numbers where the stated restrictive covenants are filed in the Chancery Clerks Office of Hancock County.

Book Number _____

Page Number _____

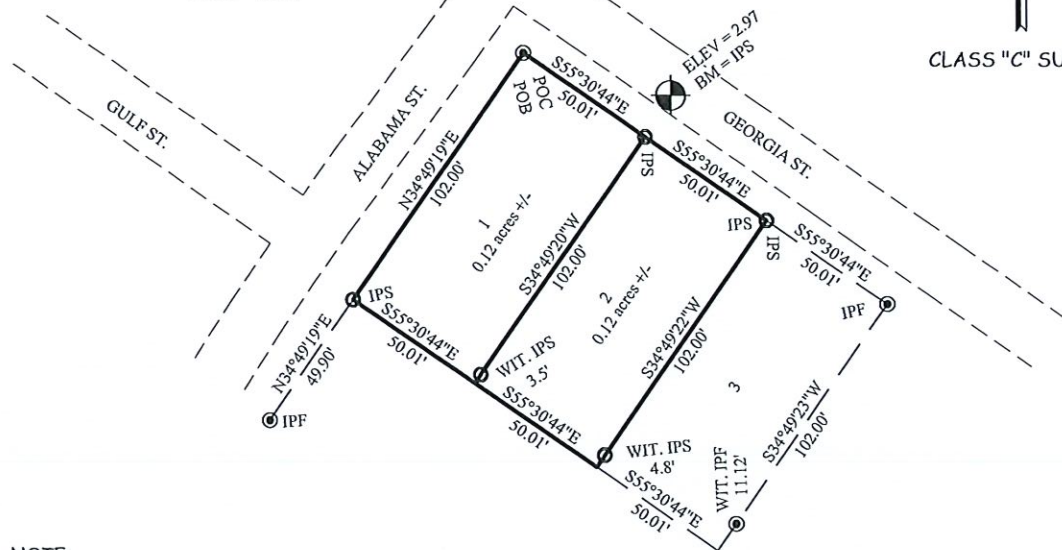
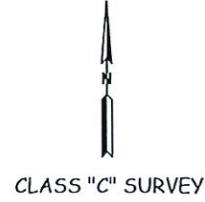
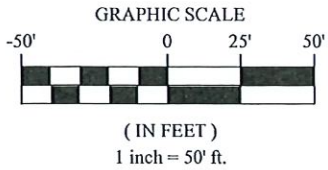
It is warranted in good faith by the owners whose name is signed hereto that all of the above facts are true and correct.


Applicant's Signature

4-17-25
Date

FOR OFFICE USE ONLY

Date of Application received: _____



NOTE:
 ALL BEARINGS SHOWN ON THIS PLAT ARE MISSISSIPPI STATE PLANE EAST ZONE, NAD 83, GEOID CONTINENTAL US NGS 2012.

NO RECORD SEARCH FOR EASEMENTS WAS CONDUCTED.

REFERENCE MATERIAL:
 RECORD PLAT FOR SHORE LINE PARK BLK 314, UNIT 3, ADDITION 1
 PROJECT NO. 14205001
 DB. 2023, PG. 7766
 RECORD GLO FIELD NOTES

LEGEND

—————	PROPERTY LINE
- - - - -	DEED LINE
⊙	MONUMENT FOUND
○	MONUMENT SET
IPF	IRON PIN FOUND
IPS	IRON PIN SET (1/2"x18")
POB	POINT OF BEGINNING
POC	POINT OF COMMENCEMENT
MPF	METAL PIPE FOUND

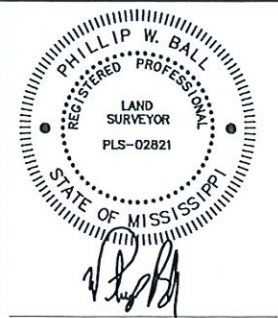
BOUNDARY LINE



LAND SURVEYING

93 Enon Road
 Columbia, MS 39429
 phillipwball@gmail.com
 (T) 601-441-8459
 (T) 601-736-0631

I certify that the information on this plat is thorough and accurate to the best of my knowledge and belief.



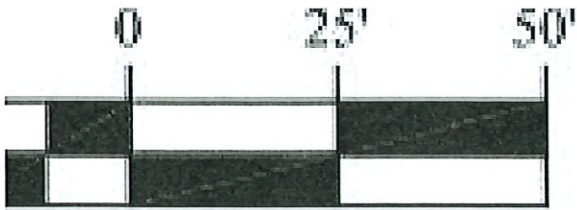
PHILLIP W. BALL, PLS

SURVEY FOR:
CARLOS WILLIAMSON
LOTS 1&2 OF SHORELINE PARK
BLK 314, UNIT 3, ADDITION 1
S 10, T-4-N, R-18-W
HANCOCK COUNTY, MS
 * * * * *

project no.: 2164G032	drawn by: <i>P.W.B.</i>
survey date: 12/17/24	checked by: <i>P.W.B.</i>
stamped date: 12/18/24	sworn chairman: PHILLIP BALL

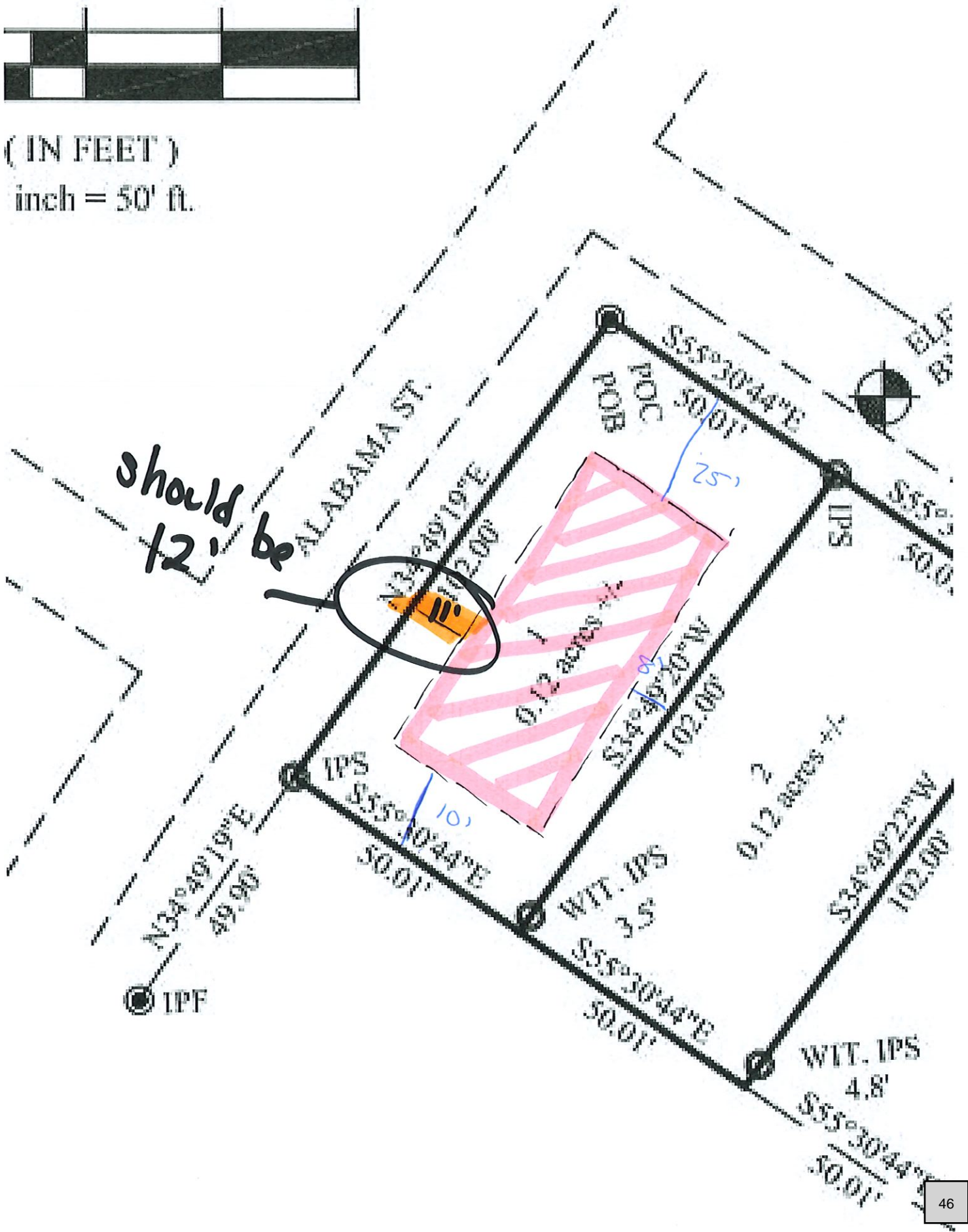
NO ABSTRACT OF TITLE, TITLE COMMITMENT, OR RESULTS OF SUCH WERE FURNISHED TO THE SURVEYOR, NOR WAS ANY SUCH MADE BY THE SURVEYOR, ALL RECORDS USED ARE LISTED HEREON.

APHIC SCALE



(IN FEET)

inch = 50' ft.





TO:
 Planning and Zoning Commission
 City of Bay St. Louis

RE:118 Engman Avenue
 Parcel 136H-1-37-021.000
 Legal Description: 5J, Rear First Ward, Bay St. Louis

HEARING DATE: May 14, 2025

I have reviewed the variance application submitted by Larry Dyess Jr. The applicant will be requesting to subdivide one existing parcel into two new parcels of land. The property lies within the R-1 Residential Single-Family zoning district, which requires a minimum lot area of 12,000 sq ft and a minimum lot width of 100 feet.

The variance requests for the properties that will be requested to be subdivided are as follows:

Parcel 1:

Minimum Lot Area: 12,000 sq ft

Proposed Lot Area: 11,068 sq ft

Variance Needed: 932 sq ft

Minimum Lot Width: 100 ft

Proposed Lot Width: 74 ft (can't be changed)

Variance Needed: 26 ft

Parcel 2:

Minimum Lot Area: 12,000 sq ft

Proposed Lot Area: 11,062 sq ft

Variance Needed: 938 sq ft

Minimum Lot Width: 100 ft

Proposed Lot Width: 74 ft (can't be changed)

Variance Needed: 26 ft

The administration recommends denying the variance.

- The new parcels will be similar to neighboring properties and almost the same size as 116 Engman and 120 Pogo (the adjacent parcels)
- The new parcels are large enough for a dwelling to be easily constructed in the future, which wouldn't require variances for front, rear, or side setbacks. But no house plans for what would be constructed on the property exist

- The width of the parcel can't be changed

Please feel free to contact my office if I can further assist in this matter.

Jeremy L Burke
Zoning Administrator

APPLICATION FOR VARIANCE TO THE ZONING ORDINANCE

Please complete this form in its entirety; failure to do so may cause a delay in the submittal of your application to the Planning and Zoning Commission.

The following information is required before this application will be submitted to the Planning and Zoning Commission for consideration.

OWNER: Larry Dyess Jr

ADDRESS: 10032 EDEN STREET
WAVELAND, MS 39520

PHONE: 504 415 3072

ADDRESS OF PROPERTY IN QUESTION IF DIFFERENT FROM ADDRESS STATED ABOVE

118 ENGMAN AVE, BAY ST LOUIS, MS, 39520

1. Legal description of property to be considered for variance as described in the Hancock County tax rolls:

5 J, REAR FIRST WARD

2. Parcel number(s) as described in the Hancock County tax rolls:

136 H - 1 - 37 - 021.000

3. Present Zoning: _____

4. Present use of building/property: VACANT

5. Application fee of \$100 (Residential): \$ 250.00 #00534313

Application fee of \$200 (Commercial): _____

Article XIII
1303 APPEALS, HEARING AND NOTICE

Every appeal or application shall refer to the specific provision of the ordinance involved and shall set forth the interpretation that is claimed, the use for which a special exception is sought, or the details of the variance that is applied for and the case may be, and accompanied by a plat or plan, drawn to scale, showing the actual dimensions of the parcel of land to be built upon and used, the size of any building to be erected, and the location of the building upon the lot, the materials to be used and other such information as may be deemed necessary to provide full information regarding intended use. The Municipal Clerk shall forthwith transmit the appeal or application to the Commission together with all papers constituting the record upon which the action appealed from was taken.

Article XIII
1305.3 VARIANCES

To recommend such variances from the terms of this Ordinance as will not be contrary to the public interest where, owing to special conditions, literal enforcement of the provisions of this Ordinance will in an individual case result in unnecessary hardship, so that the spirit of the Ordinance shall be observed, public safety and welfare secured, and substantial justice be done. Such variance may be granted in such case of unnecessary hardship upon a finding by the City Council that all of the following conditions exist:

- A. There are extraordinary and exceptional conditions which pertain to the particular piece of property in question because of its size, shape, or topography that are not applicable to other lands or structures in the same district.
- B. The literal interpretation of the provisions of this Ordinance would deprive the applicant of rights commonly enjoyed by other residents of the district in which the property is located.
- C. Granting the variance requested will not confer upon the applicant any special privileges that are denied to other residents of the district in which the property is located.
- D. The requested variance will be in harmony with the purpose and intent of this Ordinance and will not be injurious to the neighborhood or to the general welfare.
- E. The special circumstances are not the result of the actions of the applicant.
- F. The existence of a nonconforming use of neighboring land, buildings, or structures in the same district or of permitted or nonconforming uses in other districts shall not constitute a reason for the requested variance.
- G. The variance requested is the minimum variance that will make possible the legal use of the land, building, or structure.
- H. The variance is not a request to permit a use of land, building, or structure which is not permitted by right or by special exception in the district involved.

I. Notice of public hearing shall be given as in Section 1305.2(A).

J. The variance can't be transferred to a subsequent owner of the property, if the variance is unused.

K. The grant of a variance shall expire if the variance has not been activated within six (6) months of final approval. "Activation" shall mean obtaining a building permit for the required or necessary construction. In addition, the activation shall not be effective unless the construction is completed within six (6) months of obtaining the building permit. On good cause shown, the Bay St. Louis City Council may extend the above stated time limits for up to a maximum of six (6) months.

Please submit the following documentation with your application:

- 1. What is the specific provision of the ordinance involved _____
- 2. The use for which a variance is sought RECONFIGURE 1 PARCEL INTO 2 PARCELS BUT NEED VARIANCE TO PARCEL SIZE

3. If request is for a setback variance, please answer the following:

25' Front yard setback requirement
 _____ Proposed distance remaining to the property line
 _____ Total front yard setback variance needed

8' Side yard setback requirement
 _____ Proposed distance remaining to the property line
 _____ Total side yard setback variance needed

20' Rear yard setback requirement
 _____ Proposed distance remaining to the property line
 _____ Total rear yard setback variance needed

4. If request is for a variance other than setback, please answer the following:

<u>PARCEL 1</u>	<u>PARCEL 2</u>	
<u>12,000</u>	<u>12,000</u>	Required total square footage of lot
<u>11,068</u>	<u>11,062</u>	Proposed square footage of lot
<u>932</u>	<u>938</u>	Total square footage needed to lot
<u>100</u>	<u>100</u>	Required minimum width of lot
<u>74</u>	<u>74</u>	Proposed minimum width of lot
<u>26</u>	<u>26</u>	Total variance to minimum lot width needed
		Required fence height
		Proposed fence height
		Total fence height variance needed

5. Other type(s) of variance needed:

6. A plat or plan, drawn to scale, showing the actual dimensions of the parcel of land to be built upon.

7. Size of any building to be erected, and the location of the building upon the lot.

8. Materials to be used (Exterior Finish) and other such information as may be deemed necessary to provide full information regarding intended use.

9. Is the property in question in a sub-division? _____

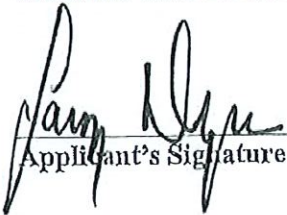
10. If the property in question is within a sub-division, is there an existing covenant running with the land? _____

11. If the answer to question 9 is yes, please state the book and page numbers where the stated restrictive covenants are filed in the Chancery Clerks Office of Hancock County.

Book Number _____

Page Number _____

It is warranted in good faith by the owners whose name is signed hereto that all of the above facts are true and correct.


Applicant's Signature

4/2/25
Date

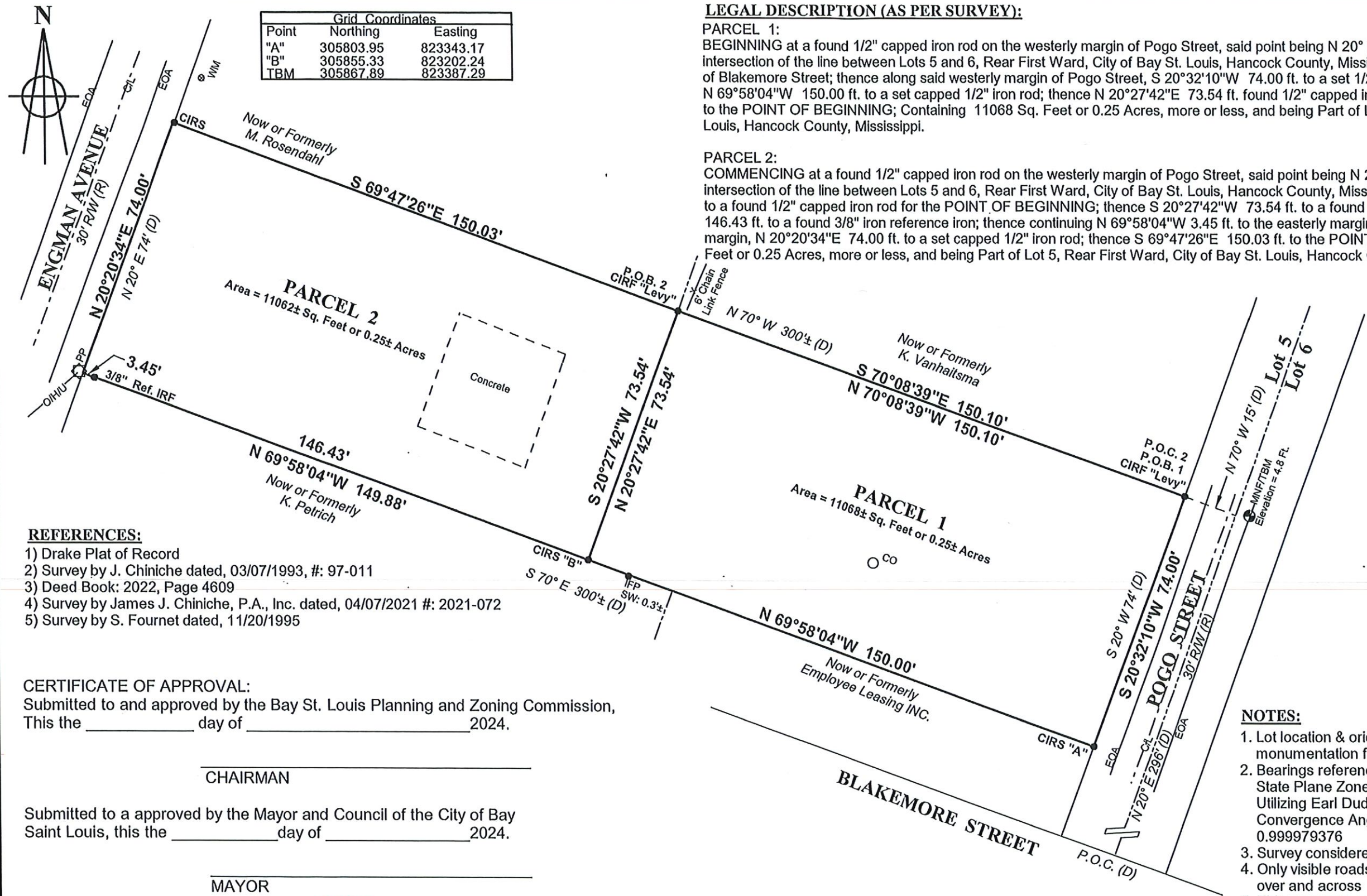
FOR OFFICE USE ONLY

Date of Application received: _____

LEGAL DESCRIPTION (AS PER SURVEY):

PARCEL 1:
 BEGINNING at a found 1/2" capped iron rod on the westerly margin of Pogo Street, said point being N 20° E 296 ft. and N 70° W 15 ft. of the intersection of the line between Lots 5 and 6, Rear First Ward, City of Bay St. Louis, Hancock County, Mississippi with the Northerly margin of Blakemore Street; thence along said westerly margin of Pogo Street, S 20°32'10"W 74.00 ft. to a set 1/2" capped iron rod; thence N 69°58'04"W 150.00 ft. to a set capped 1/2" iron rod; thence N 20°27'42"E 73.54 ft. found 1/2" capped iron rod; thence S 70°08'39"E 150.10 ft. to the POINT OF BEGINNING; Containing 11068 Sq. Feet or 0.25 Acres, more or less, and being Part of Lot 5, Rear First Ward, City of Bay St. Louis, Hancock County, Mississippi.

PARCEL 2:
 COMMENCING at a found 1/2" capped iron rod on the westerly margin of Pogo Street, said point being N 20° E 296 ft. and N 70° W 15 ft. of the intersection of the line between Lots 5 and 6, Rear First Ward, City of Bay St. Louis, Hancock County, Mississippi; thence N 70°08'39"W 150.10 ft. to a found 1/2" capped iron rod for the POINT OF BEGINNING; thence S 20°27'42"W 73.54 ft. to a found 1/2" capped iron rod; thence N 69°58'04"W 146.43 ft. to a found 3/8" iron reference iron; thence continuing N 69°58'04"W 3.45 ft. to the easterly margin of Engman Avenue; thence along said margin, N 20°20'34"E 74.00 ft. to a set capped 1/2" iron rod; thence S 69°47'26"E 150.03 ft. to the POINT OF BEGINNING; Containing 11062 Sq. Feet or 0.25 Acres, more or less, and being Part of Lot 5, Rear First Ward, City of Bay St. Louis, Hancock County, Mississippi.



Grid Coordinates		
Point	Northing	Easting
"A"	305803.95	823343.17
"B"	305855.33	823202.24
TBM	305867.89	823387.29

REFERENCES:

- 1) Drake Plat of Record
- 2) Survey by J. Chiniche dated, 03/07/1993, #: 97-011
- 3) Deed Book: 2022, Page 4609
- 4) Survey by James J. Chiniche, P.A., Inc. dated, 04/07/2021 #: 2021-072
- 5) Survey by S. Fournet dated, 11/20/1995

CERTIFICATE OF APPROVAL:

Submitted to and approved by the Bay St. Louis Planning and Zoning Commission, This the _____ day of _____ 2024.

CHAIRMAN

Submitted to a approved by the Mayor and Council of the City of Bay Saint Louis, this the _____ day of _____ 2024.

MAYOR

LEGEND:

- IRF = 1/2" Iron Rod Found
- IPF = Iron Pipe Found
- IRS = 1/2" Iron Rod Set
- IFP = Iron Fence Post
- TPF = "T" Post Found
- C = Capped
- WFP = Wood Fence Post
- MNS = Magnetic Nail Set
- Ref. = Reference
- R/W = Right-Of-Way
- O/H/U = Overhead Utilities
- PP = Power Pole
- LP = Light Pole
- P.O.B. = Point of Beginning
- P.O.C. = Point of Commencement
- TBM = Temporary Bench Mark
- FH = Fire Hydrant
- WV = Water Valve
- SMH = Sewer Man Hole
- C/L = Centerline
- EOA = Edge of Asphalt
- (P) = Plat of Record
- (D) = Deed of Record
- (R) = Record

NOTES:

1. Lot location & orientation are based on recorded data and monumentation found available.
2. Bearings reference: Grid, Geoid G-2018 U7 NAD '83 State Plane Zone Mississippi East by GPS Observation. Utilizing Earl Dudley's Virtual Reference Network, INET. Convergence Angle = -0° 15' 27" & Combination Factor = 0.999979376
3. Survey considered a Class "B" survey.
4. Only visible roads, lanes, driveways, drains, utilities, etc., over and across said premises are shown hereon.
5. This survey was prepared without a current title report nor were County records researched for easements by this surveyor.

SURVEYOR'S CERTIFICATION:

This is to certify that I have surveyed the property herein described and delineated, and that all measurements and other data are correct to the best of my knowledge and belief.

Michael Raymond McGinnis
 Michael Raymond McGinnis
 Professional Surveyor
 P.L.S.- #02827
 State of Mississippi
 Date 5/1/2024



Rev. # 3 Date:
Rev. # 2 Date:
Rev. # 1 Date:
Date: 04/10/2024
Scale: 1" = 30'
Drawn by: dmr
Dwg. #: 053-24P



OFFICE - (228) 467-6755
 EMAIL - admin@chiniche.com
 WEBSITE - www.chiniche.com
 407 Hwy. 90,
 BAY ST. LOUIS, MS 39520

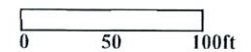
PLAT OF SURVEY OF
 Part of Lot 5, Rear First Ward,
 City of Bay St. Louis,
 Hancock County, Mississippi.

Geoportal Map

Item # 5.



DISCLAIMER: Any user of this map product accepts its faults and assumes all responsibility for the use thereof, and further agrees to hold Hancock County harmless from and against any damage, loss or liability arising from any use of the map product. Users are cautioned to consider carefully the provisional nature of the maps and data before using it for decisions that concern personal or public safety or the conduct of business that involves monetary or operational consequences. Conclusions drawn from, or actions undertaken, on the basis of such maps and data, are the sole responsibility of the user.



Zoning ordinance variance application.

2 messages

Harold Bragg <bragghe@hotmail.com>

Sun, May 11, 2025 at 6:17 PM

To: "cthompson@baystlouis-ms.gov" <cthompson@baystlouis-ms.gov>, Jeremy Burke <jburke@baystlouis-ms.gov>

I am writing to say I am against the variance for the property located at [118 Engman Ave.](#) (parcel 136H-1-37-021.000;5J). I will not be able to attend the public hearing. I hope you can take my objection into account. I have no problem with variances of a few feet to allow someone to add a on to an existing home. This is much different. This will take one lot that meets the minimum size and make two that do not meet it. If this is done, I see no reason to have a minimum lot size in the ordinance.

Thanks,
Harold Bragg
[328 Julia St.](#)
Bay St. Louis

Jeremy Burke <jburke@baystlouis-ms.gov>

Mon, May 12, 2025 at 8:14 AM

To: Harold Bragg <bragghe@hotmail.com>

Cc: "cthompson@baystlouis-ms.gov" <cthompson@baystlouis-ms.gov>

Received

Thank you

[Quoted text hidden]

Re: Request for Variance for 118 Engman Avenue

2 messages

christie andrasadvertising.com <christie@andrasadvertising.com>

Mon, May 12, 2025 at 6:44 PM

To: "jburke@baystlouis-ms.gov" <jburke@baystlouis-ms.gov>, "cthompson@baystlouis-ms.gov" <cthompson@baystlouis-ms.gov>

May 12, 2025

Dear Mr. Burke & Mrs. Thompson,

We received a public hearing notice from Bay Saint Louis Planning & Zoning Commission in regards to 118 Engman Avenue. The applicant would like to divide one parcel into two new parcels.

We respectfully request that the application for variance be denied. I like, most homeowners have made a significant investment based on the city zoning as well as other factors in the purchase / investment selection of our homes. We count on the city government to uphold the zoning when we make a purchase. Our expectation is that the city zoning is stable... and not changed at the whim of neither an individual homeowner nor a larger entity that makes a purchase.

If we create a density nightmare how does that benefit those who live here. I am certainly not opposed to growth, but I AM opposed to irresponsible growth. Before the commission approves variances, they should consider the roads have not changed or expanded. A review of the architect's renderings or developers plans for the land must be taken into account for the size of the land and the variance requested.

The most fundamental consideration for approving a developers request is that it will be of benefit for the current citizens - the people impacted by the change in the surrounding area(s) and the city. The current request will not benefit the current residents, like ourselves. Our concern is the approval could have a snowball effect for additional like requests, doubling the homes in our neighborhood.

Thank you for your consideration in advance. If you would like to discuss further, I can be reached at 504-722-8960.

Christie & Beau Royster

118 Pogo Road & 114 Engman Avenue
Bay Saint Louis, MS

Item # 5.

The information contained in this email (including attachments), is covered by the Electronic Communications Privacy Act, 18 U.S.C. 2510-2521, is confidential and is intended only for the use of the individual(s) named above or "copied" below. If you are not an intended recipient, you are hereby notified that any retention, dissemination, distribution or copying of this document is strictly prohibited. If you have received this document in error, please immediately notify the sender by telephone (504) 722-8960 and destroy the received email document in its entirety.

Jeremy Burke <jburke@baystlouis-ms.gov>
To: "christie andrasadvertising.com" <christie@andrasadvertising.com>
Cc: "cthompson@baystlouis-ms.gov" <cthompson@baystlouis-ms.gov>

Tue, May 13, 2025 at 8:05 AM

Received

[Quoted text hidden]

--

Jeremy Burke

Zoning Administrator/Historic Preservation Commission Coordinator/Community Affairs
City of Bay St Louis
228-466-5516



Caitlin Bourgeois <cthompson@baystlouis-ms.gov>

Variance Change Request for 118 Engman Avenue

1 message

Tracy Oldfather <jto.beachhouse@gmail.com>

Tue, May 13, 2025 at 4:36 PM

To: "cthompson@baystlouis-ms.gov" <cthompson@baystlouis-ms.gov>

Dear Ms Thompson,

RE: Request for variance change of 118 Engman Avenue

We received a public hearing notice concerning a request for variance changes of [118 Engman Avenue](#) from the Bay St. Louis Planning & Zoning Commission. The landowner is requesting to subdivide one parcel into two new parcels; which in turn will result in neither parcel meeting the lot area or lot width. The applicant was aware of the variances when the property was purchased. We are opposed to this request for variance changes and respectfully ask that the application for variance change and subdividing would be denied. This request would not serve our neighborhood's best interest.

We purchased our property in this specific area because of the generous lot and home sizes as well as the zoning factors. This was/is a considerable investment for us. Subdividing and changing the variances to create much smaller parcels will then produce smaller builds which will affect the value of our and other residences' properties. In addition, this desire to subdivide could spark others to do the same, resulting in a congested neighborhood. These smaller homes could be changed to possible vacation rental type homes. We did not purchase property here for such a neighborhood.

Are the treasured properties of our quiet neighborhood being bought up by those who will not live and contribute to our community? We hope the Bay St. Louis Planning & Zoning Commission will protect the existing zones and variances for the benefits of the voting citizens. Thank you for your attention and consideration,

John & Tracy Oldfather
501 Blakemore St.



Caitlin Bourgeois <cthompson@baystlouis-ms.gov>

Ordinance for 118 Engman

1 message

Kyla Van Haitsma <kylavanhaitsma@gmail.com>

Wed, May 14, 2025 at 2:26 PM

To: "cthompson@baystlouis-ms.gov" <cthompson@baystlouis-ms.gov>, "jburke@baystlouis-ms.gov" <jburke@baystlouis-ms.gov>

Kyla VanHaitsma
120 Pogo Rd
Bay St Louis, MS 39520
228-547-2233
Neighboring lot to purposed lot

May 14th 2025

City of Bay St Louis
Planning and Zoning Department

RE: Opposition to Variance Request for Adjacent Property

Dear Planning and Zoning Board,

As a resident and concerned neighbor of the property located adjacent to 118 Engman Dr. Parcel number # 136H-1-37-021.00. I am writing to formally express my opposition to the requested variance currently under review.

According to the application, the proposed variance seeks approval for a lot size of only 938 square feet. The minimum requirement for lot size in our area is 11,062 square feet. This means the proposed lot is approximately 92% smaller than the required minimum—a significant and substantial deviation from zoning standards.

I believe granting this variance would negatively impact the integrity and character of our neighborhood. Approving such an extreme exception could set a precedent that may lead to further encroachments on zoning requirements, potentially affecting property values, increasing density beyond what our infrastructure supports, and altering the established residential nature of our community.

While I understand that variances are sometimes necessary to accommodate unique circumstances, I do not believe this request represents a minor or reasonable adjustment. Instead, it constitutes an excessive deviation that undermines the purpose of our zoning regulations, for solely monetary gains for Mr. Larry Dyess Jr.

I respectfully urge the board to deny this variance request and respect the integrity of the current law.

Thank you for your time and attention to this matter.

Please read this letter in full to the board as I am regretfully unable to attend this evenings board meeting. Thank you.

Sincerely,
Kyla L VanHaitsma
120 Pogo Rd, Bay St Louis, MS 39520

TO: Planning and Zoning Commission
City of Bay St. Louis

RE: 400 Block of Sycamore Street

Parcel 149M-1-30-126.001
186 & E 18.6 OF 187 3RD WARD BAY ST LOUIS

Parcel 137J-0-44-053.000
100C, 108, 109B, 110, 111 3RD WARD BAY ST LOUIS

HEARING DATE: May 14, 2025

I have reviewed the variance application submitted by Matt Stieffel and Louie Liokis. The applicants are proposing to reconfigure two existing parcels into one new parcel. The resulting parcel will comply with the required minimum lot area; however, the applicants request a lot width variance.

The property is located in the R-1 Residential Single-Family zoning district, which requires a minimum lot width of 100 feet. The proposed reconfiguration will result in a lot width of 78.6 feet, necessitating a variance of 21.4 feet.

This proposed configuration requires the following variance:

- Minimum Lot Width: 100'
- Proposed Lot Width: 78.6'
- Variance Needed: 21.4'

It is the recommendation of the administration to approve the variance request submitted by Matt Stieffel and Louie Liokis based upon the following

- This will still have to go before the sketch and final plat approval.
- The lot width currently exists, it can't be made any larger
- This will allow a house to be constructed on the property without any setback variances.
- The variance approval will help the property become more conforming. However, it is important to note that this property can never be completely conforming, as the property owners cannot increase the lot width.

Jeremy L Burke
Zoning Administrator

APPLICATION FOR VARIANCE TO THE ZONING ORDINANCE

Please complete this form in its entirety; failure to do so may cause a delay in the submittal of your application to the Planning and Zoning Commission.

The following information is required before this application will be submitted to the Planning and Zoning Commission for consideration.

OWNER: Steffe / Lickis

ADDRESS: 507 Citizen St
Bay St Louis, MS 39520

PHONE: 228-224-5530 / 504-719-3130

ADDRESS OF PROPERTY IN QUESTION IF DIFFERENT FROM ADDRESS STATED ABOVE

- Legal description of property to be considered for variance as described in the Hancock County tax rolls:
 - 1 186 & E 18.6 OF 187 3RD WARD BSL
 - 2 100C, 108, 109 B, ¹¹⁰ ~~100~~, 111 3RD WARD BSL
- Parcel number(s) as described in the Hancock County tax rolls:
 - 1 149M-130-126.001
 - 2 137J-0-44-053.000
- Present Zoning: R-1
- Present use of building/property: Vacant
- Application fee of \$⁷⁵⁰~~100~~ (Residential): # 00533154
- Application fee of \$²⁵⁰~~200~~ (Commercial): _____

Article XIII
1303 APPEALS, HEARING AND NOTICE

Every appeal or application shall refer to the specific provision of the ordinance involved and shall set forth the interpretation that is claimed, the use for which a special exception is sought, or the details of the variance that is applied for and the case may be, and accompanied by a plat or plan, drawn to scale, showing the actual dimensions of the parcel of land to be built upon and used, the size of any building to be erected, and the location of the building upon the lot, the materials to be used and other such information as may be deemed necessary to provide full information regarding intended use. The Municipal Clerk shall forthwith transmit the appeal or application to the Commission together with all papers constituting the record upon which the action appealed from was taken.

Article XIII
1305.3 VARIANCES

To recommend such variances from the terms of this Ordinance as will not be contrary to the public interest where, owing to special conditions, literal enforcement of the provisions of this Ordinance will in an individual case result in unnecessary hardship, so that the spirit of the Ordinance shall be observed, public safety and welfare secured, and substantial justice be done. Such variance may be granted in such case of unnecessary hardship upon a finding by the City Council that all of the following conditions exist:

- A. There are extraordinary and exceptional conditions which pertain to the particular piece of property in question because of its size, shape, or topography that are not applicable to other lands or structures in the same district.
- B. The literal interpretation of the provisions of this Ordinance would deprive the applicant of rights commonly enjoyed by other residents of the district in which the property is located.
- C. Granting the variance requested will not confer upon the applicant any special privileges that are denied to other residents of the district in which the property is located.
- D. The requested variance will be in harmony with the purpose and intent of this Ordinance and will not be injurious to the neighborhood or to the general welfare.
- E. The special circumstances are not the result of the actions of the applicant.
- F. The existence of a nonconforming use of neighboring land, buildings, or structures in the same district or of permitted or nonconforming uses in other districts shall not constitute a reason for the requested variance.
- G. The variance requested is the minimum variance that will make possible the legal use of the land, building, or structure.
- H. The variance is not a request to permit a use of land, building, or structure which is not permitted by right or by special exception in the district involved.

I. Notice of public hearing shall be given as in Section 1305.2(A).

J. The variance can't be transferred to a subsequent owner of the property, if the variance is unused.

K. The grant of a variance shall expire if the variance has not been activated within six (6) months of final approval. "Activation" shall mean obtaining a building permit for the required or necessary construction. In addition, the activation shall not be effective unless the construction is completed within six (6) months of obtaining the building permit. On good cause shown, the Bay St. Louis City Council may extend the above stated time limits for up to a maximum of six (6) months.

Please submit the following documentation with your application:

- 1. What is the specific provision of the ordinance involved _____
- 2. The use for which a variance is sought _____

3. If request is for a setback variance, please answer the following:

- _____ Front yard setback requirement
- _____ Proposed distance remaining to the property line
- _____ Total front yard setback variance needed

- _____ Side yard setback requirement
- _____ Proposed distance remaining to the property line
- _____ Total side yard setback variance needed

- _____ Rear yard setback requirement
- _____ Proposed distance remaining to the property line
- _____ Total rear yard setback variance needed

4. If request is for a variance other than setback, please answer the following:

- _____ Required total square footage of lot
- _____ Proposed square footage of lot
- _____ Total square footage needed to lot

- 100' Required minimum width of lot
- 78.6' Proposed minimum width of lot
- 21.4' Total variance to minimum lot width needed

- _____ Required fence height
- _____ Proposed fence height
- _____ Total fence height variance needed

5. Other type(s) of variance needed:

6. A plat or plan, drawn to scale, showing the actual dimensions of the parcel of land to be built upon.

7. Size of any building to be erected, and the location of the building upon the lot.

8. Materials to be used (Exterior Finish) and other such information as may be deemed necessary to provide full information regarding intended use.

9. Is the property in question in a sub-division? _____


10. If the property in question is within a sub-division, is there an existing covenant running with the land? _____

11. If the answer to question 9 is yes, please state the book and page numbers where the stated restrictive covenants are filed in the Chancery Clerks Office of Hancock County.

Book Number _____

Page Number _____

It is warranted in good faith by the owners whose name is signed hereto that all of the above facts are true and correct.


Applicant's Signature

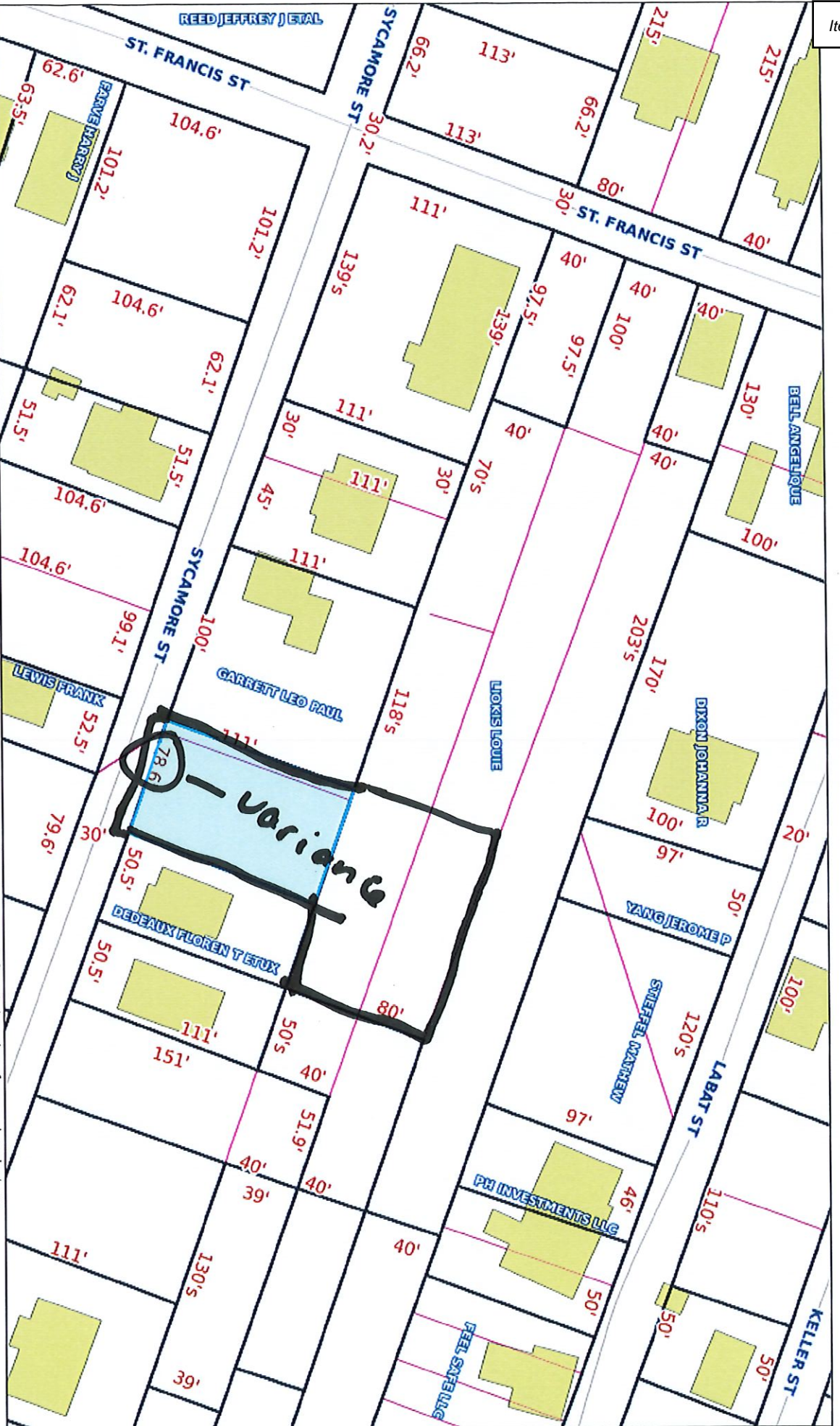
3/18/25
Date

FOR OFFICE USE ONLY

Date of Application received: _____

Geoportal Map

Item # 6.



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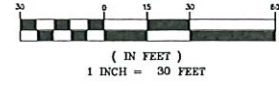




GRID COORDINATES	
POINT NORTHING	EASTING
"A"	294505.69 831728.87
"B"	294318.09 834231.43

SURVEY DATE: 02/22/2022
 BEARINGS REFERENCE: GRID, GEOID
 G2018U7 NAD '83 STATE PLANE ZONE
 MISSISSIPPI EAST BY GPS OBSERVATION

GRAPHIC SCALE



LEGEND:

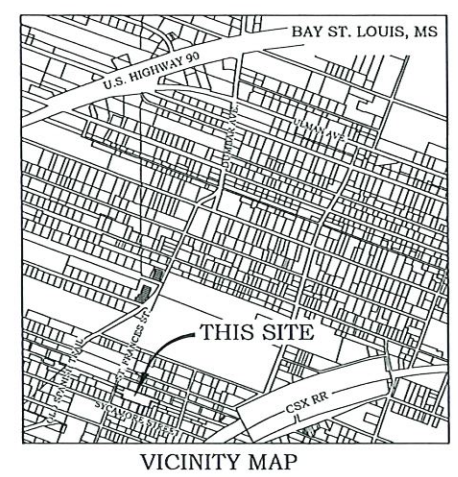
- IRF = 1/2" IRON ROD FOUND
- IPF = IRON PIPE FOUND
- CIRF = CAPPED 1/2" IRON ROD FOUND
- CIRS = CAPPED 1/2" IRON ROD SET
- IFP = IRON FENCE POST
- TPF = "T" POST FOUND
- IBF = IRON BAR FOUND
- AIF = ANGLE IRON FOUND
- WFP = WOOD FENCE POST
- MNF = MAGNETIC NAIL FOUND
- MNS = MAGNETIC NAIL SET
- REF. = REFERENCE
- O/S = OFFSET
- R/W = RIGHT-OF-WAY
- O/H/U = OVERHEAD UTILITIES
- PP = POWER POLE
- LP = LIGHT POLE
- TP = TELEPHONE PEDESTAL
- POB = POINT OF BEGINNING
- POC = POINT OF COMMENCEMENT
- TBM = TEMPORARY BENCH MARK
- FFE = FINISH FLOOR ELEVATION
- TOB = TOP OF BANK
- RCP = REINFORCED CONCRETE PIPE
- CMP = CORRUGATED METAL PIPE
- HDPE = HIGH DENSITY POLYTHENE PIPE
- FH = FIRE HYDRANT
- SMH = SEWER MAN HOLE
- C/L = CENTERLINE
- EOA = EDGE OF ASPHALT
- EOG = EDGE OF GRAVEL
- (P) = PLAT OF RECORD
- (D) = DEED OF RECORD
- (R) = RECORD
- = PROPOSED FIRE HYDRANT
- x 0.00 = GROUND ELEVATION
- = UNDERGROUND WATER MAIN
- = UNDERGROUND SEWER
- = UNDERGROUND GAS LINE
- = OVERHEAD POWER

REFERENCES:

1. HANCOCK COUNTY TAX MAP 137J
2. DRAKE PLAT OF RECORD
3. DEED BOOK 2021, PAGE 7029
4. DEED BOOK 2021, PAGE 6618
5. DEED BOOK 2017, PAGE 11793
6. DEED BOOK 2019, PAGE 9340
7. DEED BOOK 2016, PAGE 1882
8. DEED BOOK 2016, PAGE 4396
9. DEED BOOK 2021, PAGE 13413
10. DEED BOOK 2019, PAGE 14645
11. DEED BOOK 2008, PAGE 10218
12. DEED BOOK 2005, PAGE 10201
13. DEED BOOK BB-259, PAGE 853
14. DEED BOOK 2008, PAGE 27082
15. DEED BOOK 2007, PAGE 19323
16. DEED BOOK 2007, PAGE 16175
17. DEED BOOK 2014, PAGE 11233
18. DEED BOOK BB-290, PAGE 691
19. SURVEY S. OSTEEN, PROJECT #213-22, DATED: 04/06/2021.

NOTES:

1. ALL CORNERS ARE CAPPED 1/2" IRON ROD SET, UNLESS OTHERWISE NOTED.
2. LOT LOCATION & ORIENTATION ARE BASED ON RECORDED DATA AND MONUMENTATION FOUND AVAILABLE.
3. BEARINGS REFERENCE: GRID, GEOID G2018U7 NAD '83 STATE PLANE ZONE MISSISSIPPI EAST BY GPS OBSERVATION. OBSERVATION, UTILIZING EARL DUDLEY'S VIRTUAL REFERENCE NETWORK, INET. HAVING A CONVERGENCE ANGLE OF -0°15'24" AND A COMBINATION FACTOR OF 0.999979529.
4. SURVEY CONSIDERED A CLASS 'B' SURVEY.
5. ONLY VISIBLE ROADS, LANES, DRIVEWAYS, DRAINS, UTILITIES, ETC., OVER AND ACROSS SAID PREMISES ARE SHOWN HEREON.
6. THIS SURVEY WAS PREPARED WITHOUT A CURRENT TITLE REPORT NOR WERE COUNTY RECORDS RESEARCHED FOR EASEMENTS BY THIS SURVEYOR.
7. PROPERTY OWNER/CONTRACTOR IS REQUIRED TO COORDINATE SETBACKS AND VARIANCES WITH PROPER ENTITY.
8. PROPERTY FALLS IN A R-1 DISTRICT BASED ON BAY ST. LOUIS ZONING ORDINANCE - 2017.
9. OWNER IS RESPONSIBLE FOR CERTIFICATION OF TREE SPECIES AND SIZES IN COORDINATION WITH LOCAL ENTITY FOR TREES DESIGNATED AS PROTECTED.
10. SURVEY DATE: 02.22.2022.
11. SOIL CONDITIONS CONSIST OF GENERALLY SANDY MATERIAL.
12. ELEVATIONS ARE TIED TO A U.S.C.G.S. TIDAL BENCH MARK, EL = 4.96'.



VICINITY MAP

**LOTS 108, 110, 111 AND 186 AND PART OF
 LOTS 100, 109 AND 187, THIRD WARD,
 CITY OF BAY ST. LOUIS,
 HANCOCK COUNTY, MISSISSIPPI**



SURVEY DESCRIPTION:

BEGINNING AT A CAPPED 1/2" IRON ROD FOUND AT THE NORTHWEST CORNER OF LOT 100, THIRD WARD, CITY OF BAY ST. LOUIS, HANCOCK COUNTY, MISSISSIPPI, SAID POINT BEING ON THE EASTERLY MARGIN OF ST. FRANCES STREET AND RUN S70°27'44"E ALONG THE NORTHERLY LINE OF LOT 100 FOR 546.00 FEET TO A CAPPED 1/2" IRON ROD FOUND; THENCE RUN S19°24'56"W FOR 40.17 FEET TO A 1/2" IRON ROD FOUND AT THE NORTHEASTERLY CORNER OF LOT 112; THENCE RUN N69°47'39"W ALONG THE NORTHERLY LINE OF LOT 112 FOR 100.38 FEET TO A CAPPED 1/2" IRON ROD FOUND AT THE NORTHEASTERLY CORNER OF LOT 110; THENCE RUN S19°34'32"W ALONG THE EASTERLY LINES OF LOT 110 AND 111 FOR 81.86 FEET TO A CAPPED 1/2" IRON ROD SET AT THE SOUTHEASTERLY CORNER OF LOT 111; THENCE RUN N69°27'44"W ALONG THE SOUTHERLY LINE OF LOT 111 FOR 47.52 FEET TO A CAPPED 1/2" IRON ROD FOUND AT THE NORTHEASTERLY CORNER OF LOT 186; THENCE RUN S21°00'34"W ALONG THE EASTERLY LINE OF LOT 186 FOR 111.68 FEET TO A CAPPED 1/2" IRON ROD FOUND ON THE NORTHERLY MARGIN OF SYCAMORE STREET; THENCE RUN N69°06'32"W ALONG SAID MARGIN LINE FOR 78.80 FEET TO A CAPPED 1/2" IRON ROD FOUND; THENCE LEAVING SAID MARGIN LINE RUN N21°09'21"E FOR 111.21 FEET TO A 1/2" IRON ROD FOUND ON THE SOUTHERLY LINE OF LOT 111; THENCE RUN N70°04'42"W ALONG THE SOUTHERLY LINES OF LOT 111 AND 109 FOR 177.82 FEET TO A 1/2" IRON ROD FOUND; THENCE RUN N71°57'41"W ALONG THE SOUTHERLY LINE OF LOT 109 FOR 42.57 FEET TO A 1/2" IRON ROD FOUND; THENCE RUN N19°46'28"E FOR 40.00 FEET TO A CAPPED 1/2" IRON ROD SET AT THE SOUTHEASTERLY CORNER OF LOT 108; THENCE RUN N71°05'10"W ALONG THE SOUTHERLY LINE OF LOT 108 FOR 100.00 FEET TO A CAPPED 1/2" IRON ROD FOUND ON THE EASTERLY MARGIN LINE OF ST. FRANCES STREET; THENCE RUN N19°56'13"E ALONG SAID MARGIN LINE FOR 79.67 FEET BACK TO THE POINT OF BEGINNING. SAID PARCEL OF LAND IS A PART OF LOTS 100, 108, 109, 110, 111, 186 AND 187, THIRD WARD, CITY OF BAY ST. LOUIS, HANCOCK COUNTY, MISSISSIPPI AND CONTAINING 1.42 ACRES, MORE OR LESS.

CERTIFICATE OF APPROVAL:

SUBMITTED TO AND APPROVED BY THE BAY ST. LOUIS PLANNING AND ZONING COMMISSION, THIS THE ____ DAY OF _____, 2022.

ZONING ADMINISTRATOR

SUBMITTED TO AND APPROVED BY THE MAYOR AND COUNCIL OF THE CITY OF BAY ST. LOUIS, THIS THE ____ DAY OF _____, 2022.

MAYOR

CERTIFICATE:

THIS IS TO CERTIFY THAT I HAVE CAUSED A SURVEY TO BE MADE ON THE PROPERTY LINES SHOWN ON THIS PLAT, AND THAT SAID SURVEY IS TRUE AND CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF.

DATE: 02/24/2022

TERRY MORAN, P.E., P.L.S.
 P.O. Box 4075 Ph. (228) 896-9783
 Bay St. Louis, MS 39555 Fax. (228) 896-9788



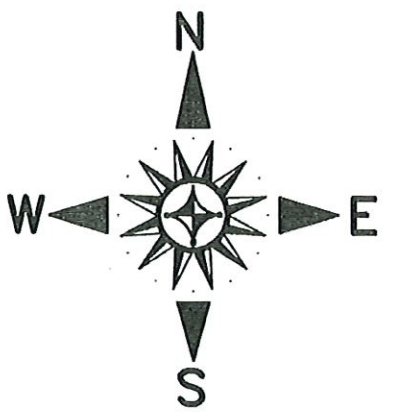
2022-026
 CITY OF BAY ST. LOUIS, HANCOCK COUNTY, MS

PRELIMINARY PLAT OF SURVEY OF,
 LOTS 108, 110, 111, AND 186 AND PART OF
 LOTS 100, 109 AND 187, THIRD WARD,
 CITY OF BAY ST. LOUIS, HANCOCK COUNTY, MS.

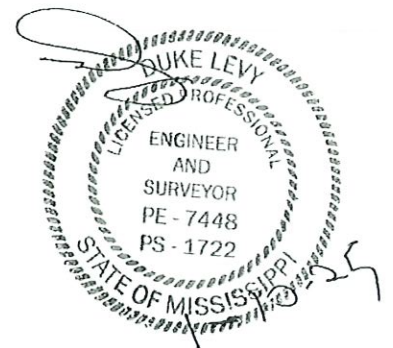
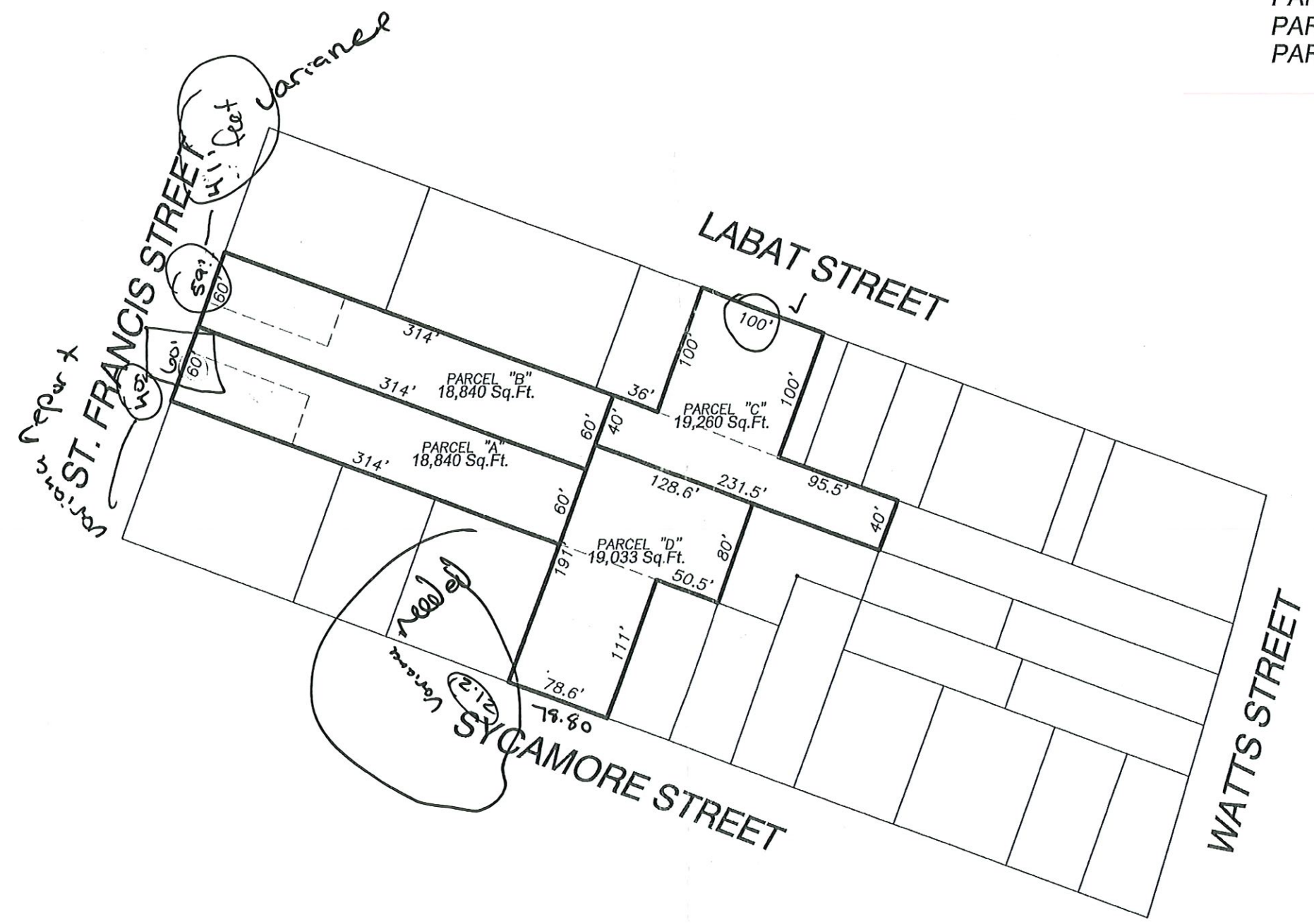
Date: 02-22-2022
 Drawn By: T.M.M.
 Check By: T.M.M.
 REV: _____

Sheet 1

SKETCH PLAT PROPOSED RE-SUBDIVISION



PARCEL "A" = 18,840 Sq.Ft.
PARCEL "B" = 18,840 Sq.Ft.
PARCEL "C" = 19,260 Sq.Ft.
PARCEL "D" = 19,033 Sq.Ft.



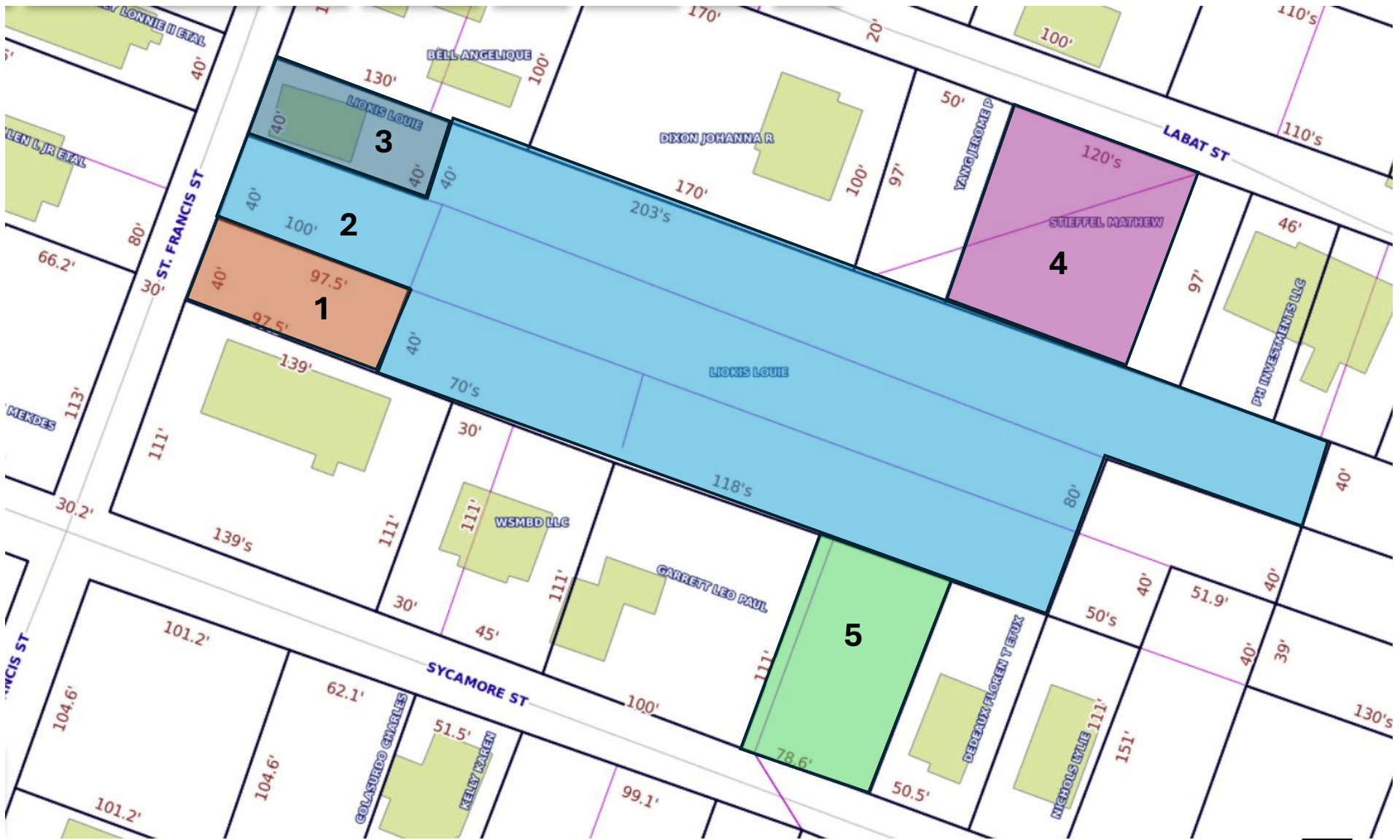
DUKE LEVY & ASSOCIATES, P.A.

DLA 4412 LEISURE TIME DRIVE
DIAMONDHEAD, MS 39525
(228) 343-9691 PHONE

SCALE: 1" = 100'	DATE: 01-10-2025
J.L.C. DRAWING: WO# 2023-204	CLIENT: MATT STIEFFEL

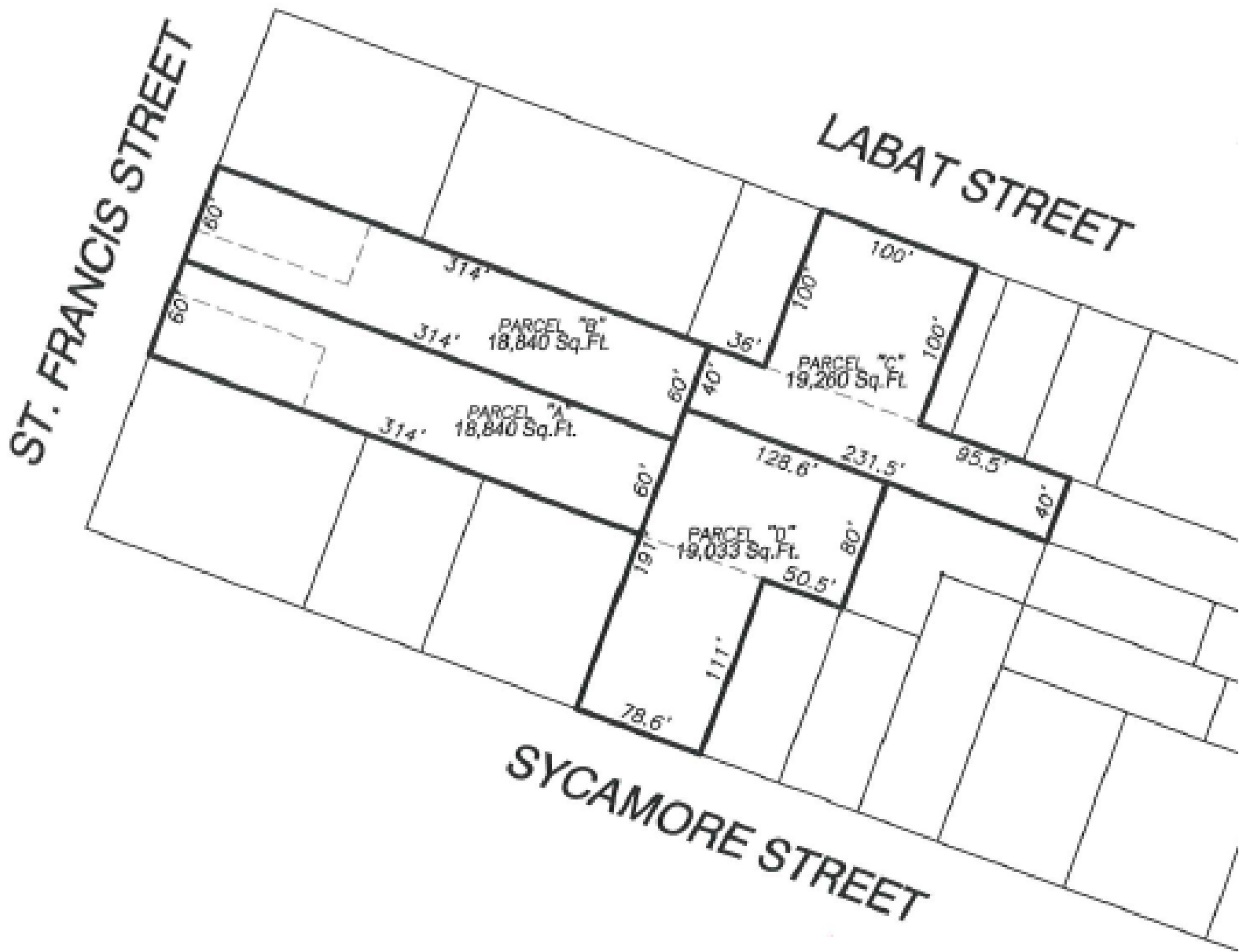
Current Lot / Parcel Configuration: 5 Single Family Lots

Item # 6.



Proposed Re-subdivision:
4 Single Family Lots

Item # 6.



TO: Planning and Zoning Commission
City of Bay St. Louis

RE: 500 Block of St. Francis Street
Parcel 137J-0-44-057.000
100D 3RD WARD BAY ST LOUIS
Parcel 137J-0-44-053.000
100C, 108, 109B, 110, 111 3RD WARD BAY ST LOUIS
Parcel 137J-0-44-056.000
109A 3RD WARD BAY ST LOUIS

HEARING DATE: May 14, 2025

I have reviewed the variance application submitted by Matt Stieffel and Louie Liokis. The applicants are proposing to reconfigure three existing parcels into new parcels of land. The property is located in the R-1 Residential Single-Family zoning district, which requires a minimum lot width of 100 feet. The proposed reconfiguration will result in two parcels (Parcel A and Parcel B) that do not meet the required minimum lot width and therefore require variances.

This proposed configuration requires the following variances:

Parcel A

Minimum Lot Width: 100'
Proposed Lot Width: 60'
Variance Needed: 40'

Parcel B

Minimum Lot Width: 100'
Proposed Lot Width: 59'
Variance Needed: 41'

It is the recommendation of the administration to approval the variance request submitted by Matt Stieffel and Louie Liokis based upon the following

- This will allow a house to be constructed without any setback variances.
- The lot width currently exists; it can't be made any larger
- This will still have to go before the sketch and final plat approval.
- Matt Stieffel and Louie have five parcels that they will reconfigure into four parcels. The only structures that can be constructed on these new configured properties are single-family houses (no multi-family or duplexes).

Jeremy Burke

APPLICATION FOR VARIANCE TO THE ZONING ORDINANCE

Please complete this form in its entirety; failure to do so may cause a delay in the submittal of your application to the Planning and Zoning Commission.

The following information is required before this application will be submitted to the Planning and Zoning Commission for consideration.

OWNER: Stieffel / Liskis

ADDRESS: 507 Citizen St
Bay St Louis MS 39520

PHONE: 228/224/5530 504-719-3130

ADDRESS OF PROPERTY IN QUESTION IF DIFFERENT FROM ADDRESS STATED ABOVE

516 St Francis Street

1. Legal description of property to be considered for variance as described in the Hancock County tax rolls:

- ① 1000 3RD WARD BSL
- ② 1000, 108, 109B, 110, 111 3RD WARD BSL
137J-0-44-053.000
- ③ 109A 3RD WARD BSL

2. Parcel number(s) as described in the Hancock County tax rolls:

- ① 137J-0-44-057.000
- ② 1000, 108, 109B, 110, 111 3RD WARD BSL
137J-0-44-053.000
- ③ 137J-0-44-056.000

3. Present Zoning: R-1

4. Present use of building/property: vacant

5. Application fee of ^{\$250}~~\$100~~ (Residential): ✓ 00533154

Application fee of ^{\$250}~~\$200~~ (Commercial): _____

Article XIII
1303 APPEALS, HEARING AND NOTICE

Every appeal or application shall refer to the specific provision of the ordinance involved and shall set forth the interpretation that is claimed, the use for which a special exception is sought, or the details of the variance that is applied for and the case may be, and accompanied by a plat or plan, drawn to scale, showing the actual dimensions of the parcel of land to be built upon and used, the size of any building to be erected, and the location of the building upon the lot, the materials to be used and other such information as may be deemed necessary to provide full information regarding intended use. The Municipal Clerk shall forthwith transmit the appeal or application to the Commission together with all papers constituting the record upon which the action appealed from was taken.

Article XIII
1305.3 VARIANCES

To recommend such variances from the terms of this Ordinance as will not be contrary to the public interest where, owing to special conditions, literal enforcement of the provisions of this Ordinance will in an individual case result in unnecessary hardship, so that the spirit of the Ordinance shall be observed, public safety and welfare secured, and substantial justice be done. Such variance may be granted in such case of unnecessary hardship upon a finding by the City Council that all of the following conditions exist:

- A. There are extraordinary and exceptional conditions which pertain to the particular piece of property in question because of its size, shape, or topography that are not applicable to other lands or structures in the same district.
- B. The literal interpretation of the provisions of this Ordinance would deprive the applicant of rights commonly enjoyed by other residents of the district in which the property is located.
- C. Granting the variance requested will not confer upon the applicant any special privileges that are denied to other residents of the district in which the property is located.
- D. The requested variance will be in harmony with the purpose and intent of this Ordinance and will not be injurious to the neighborhood or to the general welfare.
- E. The special circumstances are not the result of the actions of the applicant.
- F. The existence of a nonconforming use of neighboring land, buildings, or structures in the same district or of permitted or nonconforming uses in other districts shall not constitute a reason for the requested variance.
- G. The variance requested is the minimum variance that will make possible the legal use of the land, building, or structure.
- H. The variance is not a request to permit a use of land, building, or structure which is not permitted by right or by special exception in the district involved.

- I. Notice of public hearing shall be given as in Section 1305.2(A).
- J. The variance can't be transferred to a subsequent owner of the property, if the variance is unused.
- K. The grant of a variance shall expire if the variance has not been activated within six (6) months of final approval. "Activation" shall mean obtaining a building permit for the required or necessary construction. In addition, the activation shall not be effective unless the construction is completed within six (6) months of obtaining the building permit. On good cause shown, the Bay St. Louis City Council may extend the above stated time limits for up to a maximum of six (6) months.

Please submit the following documentation with your application:

- 1. What is the specific provision of the ordinance involved _____
- 2. The use for which a variance is sought _____

3. If request is for a setback variance, please answer the following:

- _____ Front yard setback requirement
- _____ Proposed distance remaining to the property line
- _____ Total front yard setback variance needed

- _____ Side yard setback requirement
- _____ Proposed distance remaining to the property line
- _____ Total side yard setback variance needed

- _____ Rear yard setback requirement
- _____ Proposed distance remaining to the property line
- _____ Total rear yard setback variance needed

4. If request is for a variance other than setback, please answer the following:

- _____ Required total square footage of lot
- _____ Proposed square footage of lot
- _____ Total square footage needed to lot

- _____ Required minimum width of lot
- _____ Proposed minimum width of lot
- _____ Total variance to minimum lot width needed

- _____ Required fence height
- _____ Proposed fence height
- _____ Total fence height variance needed

Parcel A	Parcel B
100'	100'
60'	59'
40'	41'

5. Other type(s) of variance needed:

6. A plat or plan, drawn to scale, showing the actual dimensions of the parcel of land to be built upon.

7. Size of any building to be erected, and the location of the building upon the lot.

8. Materials to be used (Exterior Finish) and other such information as may be deemed necessary to provide full information regarding intended use.

9. Is the property in question in a sub-division? _____

10. If the property in question is within a sub-division, is there an existing covenant running with the land? _____

11. If the answer to question 9 is yes, please state the book and page numbers where the stated restrictive covenants are filed in the Chancery Clerks Office of Hancock County.

Book Number _____

Page Number _____

It is warranted in good faith by the owners whose name is signed hereto that all of the above facts are true and correct.

Maath J. Leary
Applicant's Signature

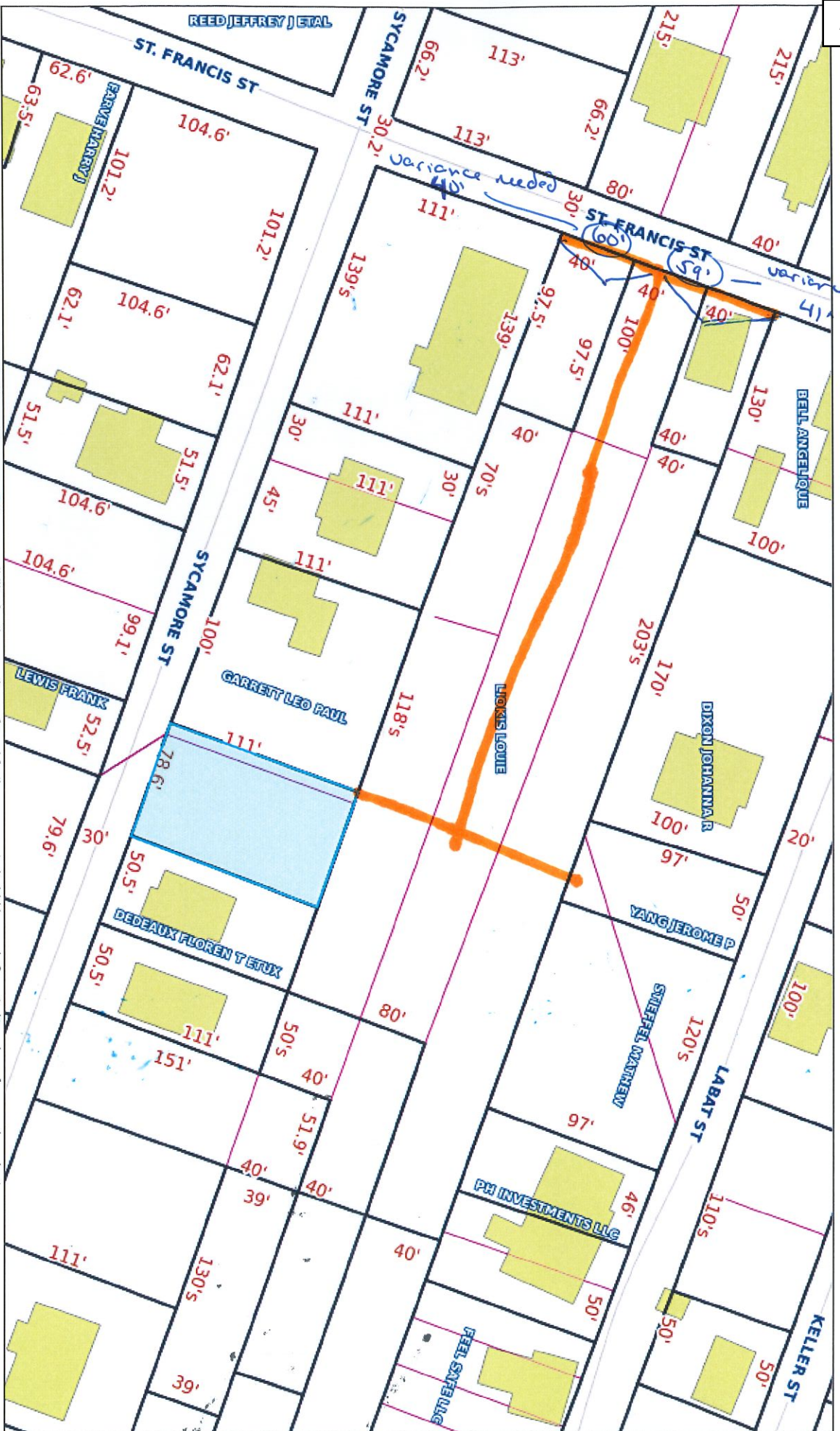
3/18/25
Date

FOR OFFICE USE ONLY

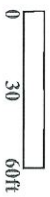
Date of Application received: _____

Geoportal Map

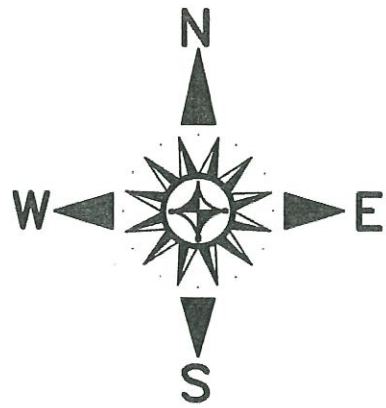
Item # 7.



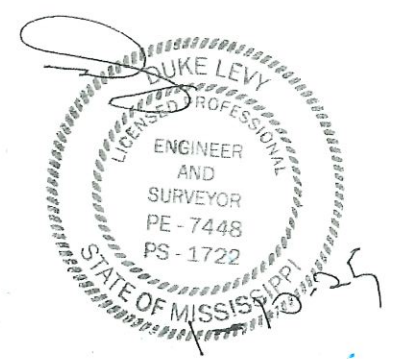
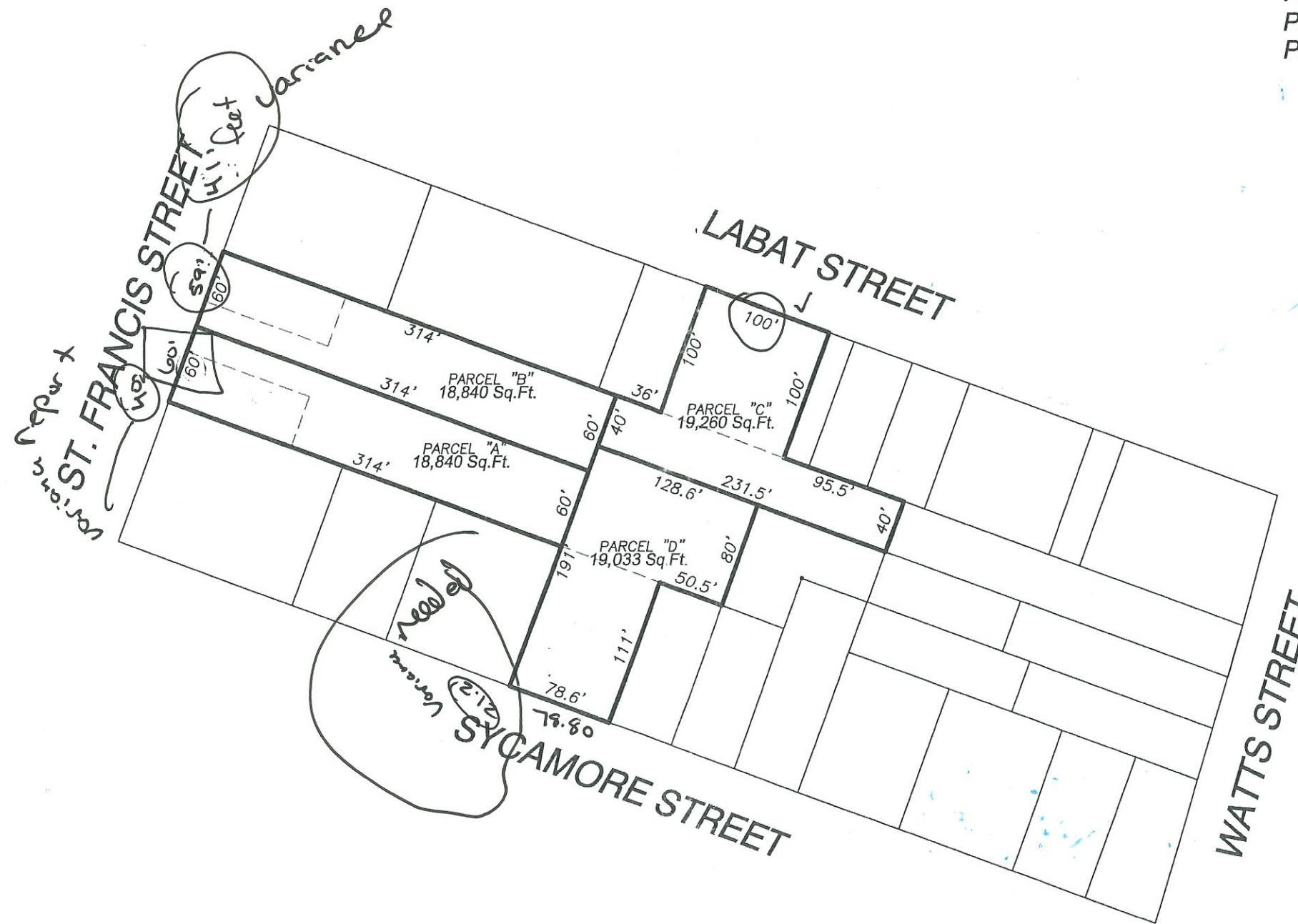
DISCLAIMER: Any user of this map product accepts its faults and assumes all responsibility for the use thereof, and further agrees to hold Hancock County harmless from and against any damage, loss or liability arising from any use of the map product. Users are cautioned to consider carefully the provisional nature of the maps and data before using it for decisions that concern personal or public safety or the conduct of business that involves monetary or operational consequences. Conclusions drawn from, or actions undertaken, on the basis of such maps and data, are the sole responsibility of the user.



SKETCH PLAT PROPOSED RE-SUBDIVISION



PARCEL "A" = 18,840 Sq.Ft.
PARCEL "B" = 18,840 Sq.Ft.
PARCEL "C" = 19,260 Sq.Ft.
PARCEL "D" = 19,033 Sq.Ft.



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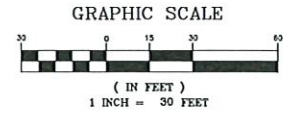
SCALE: 1" = 100'	DATE: 01-10-2025
DRAWING: WO# 2023-204	CLIENT: MATT STIEFFEL

J.L.C.



GRID COORDINATES	
POINT NORTHING	EASTING
"A" 294500.69	823508.87
"B" 294318.09	824223.43

SURVEY DATE: 02/22/2022
 BEARINGS REFERENCE: GRID, GEOID
 G2018U7 NAD 83 STATE PLANE ZONE
 MISSISSIPPI EAST BY GPS OBSERVATION



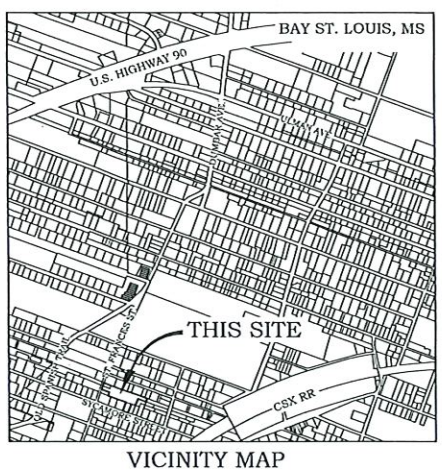
- LEGEND:**
- IRF = 1/2" IRON ROD FOUND
 - IPF = IRON PIPE FOUND
 - CIRF = CAPPED 1/2" IRON ROD FOUND
 - CIRS = CAPPED 1/2" IRON ROD SET
 - IFP = IRON FENCE POST
 - TFP = "T" POST FOUND
 - IBF = IRON BAR FOUND
 - AIF = ANGLE IRON FOUND
 - WFP = WOOD FENCE POST
 - MNF = MAGNETIC NAIL FOUND
 - MNS = MAGNETIC NAIL SET
 - REF. = REFERENCE
 - O/S = OFFSET
 - R/W = RIGHT-OF-WAY
 - O/H/U = OVERHEAD UTILITIES
 - PP = POWER POLE
 - LP = LIGHT POLE
 - TP = TELEPHONE PEDESTAL
 - POB = POINT OF BEGINNING
 - POC = POINT OF COMMENCEMENT
 - TBM = TEMPORARY BENCH MARK
 - FPE = FINISH FLOOR ELEVATION
 - TOB = TOP OF BANK
 - RCP = REINFORCED CONCRETE PIPE
 - CMP = CORRUGATED METAL PIPE
 - HDPE = HIGH DENSITY POLYTHENE PIPE
 - FH = FIRE HYDRANT
 - SMH = SEWER MAN HOLE
 - C/L = CENTERLINE
 - EOA = EDGE OF ASPHALT
 - EOG = EDGE OF GRAVEL
 - (P) = PLAT OF RECORD
 - (D) = DEED OF RECORD
 - (R) = RECORD
 - ⊗ = PROPOSED FIRE HYDRANT
 - x 0.00 = GROUND ELEVATION
 - = UNDERGROUND WATER MAIN
 - = UNDERGROUND SEWER
 - = UNDERGROUND GAS LINE
 - = OVERHEAD POWER

REFERENCES:

1. HANCOCK COUNTY TAX MAP 137J
2. DRAKE PLAT OF RECORD
3. DEED BOOK 2021, PAGE 7029
4. DEED BOOK 2021, PAGE 6618
5. DEED BOOK 2017, PAGE 11793
6. DEED BOOK 2019, PAGE 9340
7. DEED BOOK 2016, PAGE 1882
8. DEED BOOK 2016, PAGE 4396
9. DEED BOOK 2021, PAGE 13413
10. DEED BOOK 2019, PAGE 14645
11. DEED BOOK 2008, PAGE 10218
12. DEED BOOK 2005, PAGE 10201
13. DEED BOOK BB-259, PAGE 853
14. DEED BOOK 2008, PAGE 27082
15. DEED BOOK 2007, PAGE 19323
16. DEED BOOK 2007, PAGE 16175
17. DEED BOOK 2014, PAGE 11233
18. DEED BOOK BB-290, PAGE 691
19. SURVEY S. OSTEEN, PROJECT #213-22, DATED: 04/06/2021.

NOTES:

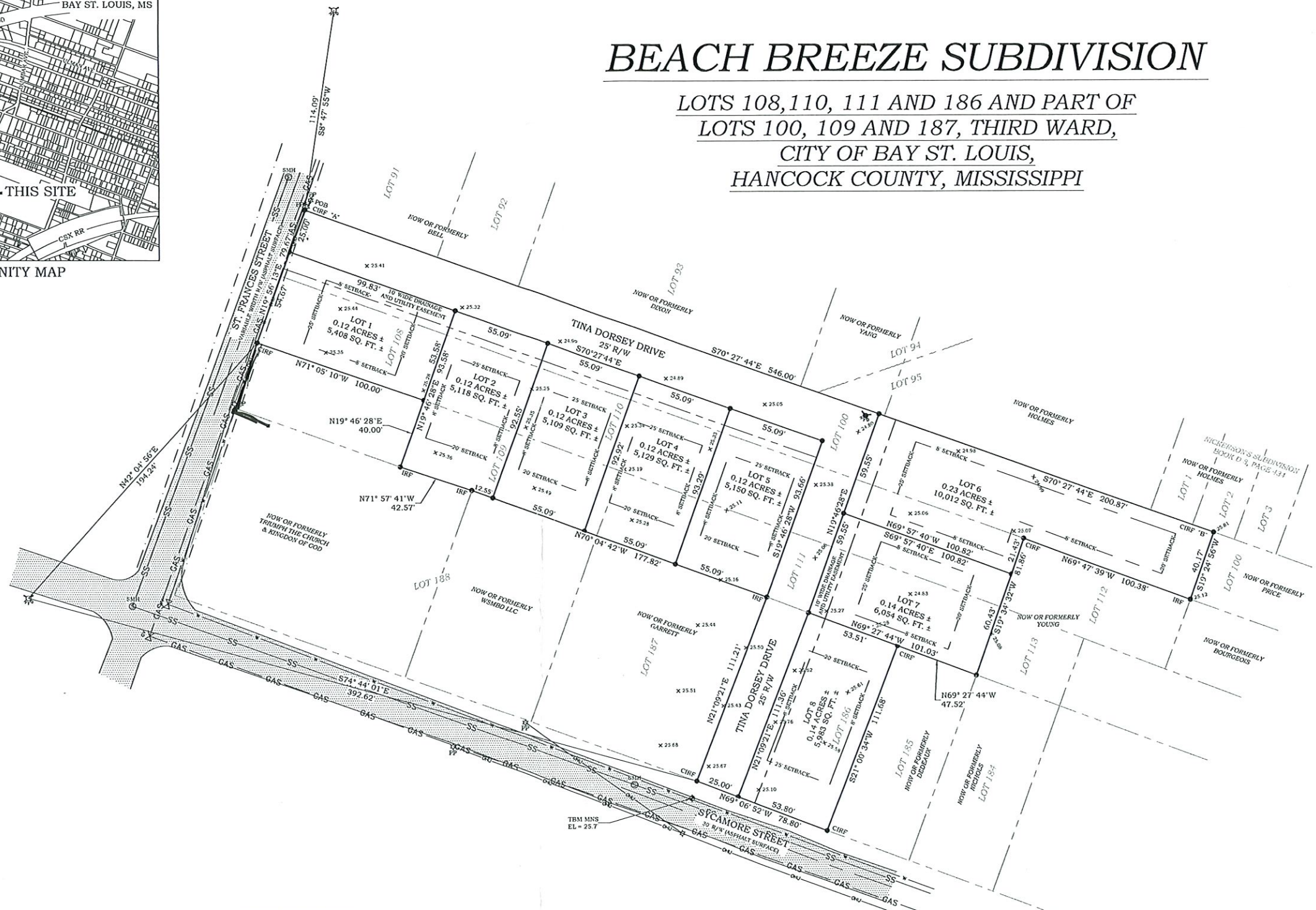
1. ALL CORNERS ARE CAPPED 1/2" IRON ROD SET, UNLESS OTHERWISE NOTED.
2. LOT LOCATION & ORIENTATION ARE BASED ON RECORDED DATA AND MONUMENTATION FOUND AVAILABLE.
3. BEARINGS REFERENCE: GRID, GEOID G2018U7 NAD 83 STATE PLANE ZONE MISSISSIPPI EAST BY GPS OBSERVATION. UTILIZING EARL DUDLEY'S VIRTUAL REFERENCE NETWORK, INET. HAVING A CONVERGENCE ANGLE OF -0°15'24" AND A COMBINATION FACTOR OF 0.999979529.
4. SURVEY CONSIDERED A CLASS "B" SURVEY.
5. ONLY VISIBLE ROADS, LANES, DRIVEWAYS, DRAINS, UTILITIES, ETC., OVER AND ACROSS SAID PREMISES ARE SHOWN HEREON.
6. THIS SURVEY WAS PREPARED WITHOUT A CURRENT TITLE REPORT NOR WERE COUNTY RECORDS RESEARCHED FOR EASEMENTS BY THIS SURVEYOR.
7. PROPERTY OWNER/CONTRACTOR IS REQUIRED TO COORDINATE SETBACKS AND VARIANCES WITH PROPER ENTITY.
8. PROPERTY FALLS IN A R-1 DISTRICT BASED ON BAY ST. LOUIS ZONING ORDINANCE - 2017.
9. OWNER IS RESPONSIBLE FOR CERTIFICATION OF TREE SPECIES AND SIZES IN COORDINATION WITH LOCAL ENTITY FOR TREES DESIGNATED AS PROTECTED.
10. SURVEY DATE: 02/22/2022.
11. SOIL CONDITIONS CONSIST OF GENERALLY SANDY MATERIAL.
12. ELEVATIONS ARE TIED TO A U.S.C.G.S. TIDAL BENCH MARK, EL = 4.96.



VICINITY MAP

BEACH BREEZE SUBDIVISION

LOTS 108, 110, 111 AND 186 AND PART OF
 LOTS 100, 109 AND 187, THIRD WARD,
 CITY OF BAY ST. LOUIS,
 HANCOCK COUNTY, MISSISSIPPI



SURVEY DESCRIPTION:

BEGINNING AT A CAPPED 1/2" IRON ROD FOUND AT THE NORTHWEST CORNER OF LOT 100, THIRD WARD, CITY OF BAY ST. LOUIS, HANCOCK COUNTY, MISSISSIPPI, SAID POINT BEING ON THE EASTERLY MARGIN OF ST. FRANCES STREET AND RUN S70°27'44"E ALONG THE NORTHERLY LINE OF LOT 100 FOR 546.00 FEET TO A CAPPED 1/2" IRON ROD FOUND; THENCE RUN S19°24'56"W FOR 40.17 FEET TO A 1/2" IRON ROD FOUND AT THE NORTHEASTERLY CORNER OF LOT 112; THENCE RUN N69°47'39"W ALONG THE NORTHERLY LINE OF LOT 112 FOR 100.38 FEET TO A CAPPED 1/2" IRON ROD FOUND AT THE NORTHEASTERLY CORNER OF LOT 110; THENCE RUN S19°34'32"W ALONG THE EASTERLY LINES OF LOT 110 AND 111 FOR 81.86 FEET TO A CAPPED 1/2" IRON ROD SET AT THE SOUTHEASTERLY CORNER OF LOT 111; THENCE RUN N69°27'44"W ALONG THE SOUTHERLY LINE OF LOT 111 FOR 47.52 FEET TO A CAPPED 1/2" IRON ROD FOUND AT THE NORTHEASTERLY CORNER OF LOT 186; THENCE RUN S21°00'34"W ALONG THE EASTERLY LINE OF LOT 186 FOR 111.68 FEET TO A CAPPED 1/2" IRON ROD FOUND ON THE NORTHERLY MARGIN OF SYCAMORE STREET; THENCE RUN N69°06'52"W ALONG SAID MARGIN LINE FOR 78.80 FEET TO A CAPPED 1/2" IRON ROD FOUND; THENCE LEAVING SAID MARGIN LINE RUN N21°09'21"E FOR 111.21 FEET TO A 1/2" IRON ROD FOUND ON THE SOUTHERLY LINE OF LOT 111; THENCE RUN N70°04'42"W ALONG THE SOUTHERLY LINES OF LOT 111 AND 109 FOR 177.82 FEET TO A 1/2" IRON ROD FOUND; THENCE RUN N71°57'41"W ALONG THE SOUTHERLY LINE OF LOT 109 FOR 42.57 FEET TO A 1/2" IRON ROD FOUND; THENCE RUN N19°46'28"E FOR 40.00 FEET TO A CAPPED 1/2" IRON ROD SET AT THE SOUTHEASTERLY CORNER OF LOT 108; THENCE RUN N71°05'10"W ALONG THE SOUTHERLY LINE OF LOT 108 FOR 100.00 FEET TO A CAPPED 1/2" IRON ROD FOUND ON THE EASTERLY MARGIN LINE OF ST. FRANCES STREET; THENCE RUN N19°56'13"E ALONG SAID MARGIN LINE FOR 79.67 FEET BACK TO THE POINT OF BEGINNING. SAID PARCEL OF LAND IS A PART OF LOTS 100, 108, 109, 110, 111, 186 AND 187, THIRD WARD, CITY OF BAY ST. LOUIS, HANCOCK COUNTY, MISSISSIPPI AND CONTAINING 1.42 ACRES, MORE OR LESS.

CERTIFICATE OF APPROVAL:
 SUBMITTED TO AND APPROVED BY THE BAY ST. LOUIS PLANNING AND ZONING COMMISSION, THIS THE ____ DAY OF ____ 2022.
 ZONING ADMINISTRATOR _____
 SUBMITTED TO AND APPROVED BY THE MAYOR AND COUNCIL OF THE CITY OF BAY ST. LOUIS, THIS THE ____ DAY OF ____ 2022.
 MAYOR _____

CERTIFICATE:
 THIS IS TO CERTIFY THAT I HAVE CAUSED A SURVEY TO BE MADE ON THE PROPERTY LINES SHOWN ON THIS PLAT, AND THAT SAID SURVEY IS TRUE AND CORRECT TO THE BEST OF MY KNOWLEDGE AND BELIEF.
 DATE: 02/24/2022



TERRY MORAN, P.E., P.L.S.
 P.O. Box 4075 Ph. (228) 896-4733
 Biloxi, MS 39535 Fax. (228) 896-6708

TME
 TERRY MORAN
 ENGINEERING

2022-026 FOR CORR
 CITY OF BAY ST. LOUIS, HANCOCK COUNTY, MS

PRELIMINARY PLAT OF SURVEY OF
 BEACH BREEZE SUBDIVISION,
 LOTS 108, 110, 111, AND 186 AND PART OF
 LOTS 100, 109 AND 187, THIRD WARD,
 CITY OF BAY ST. LOUIS, HANCOCK COUNTY, MS.

Date: 02-22-2022
 Drawn By: T.M.
 Check By: T.M.
 REV: _____

Sheet **1**

Survey Plat for Clare Zimmer

Lots 108, 110, 111, & parts of Lots 100 & 109,
3rd Ward, Bay St. Louis
St. Francis Street, Bay St. Louis, MS 39520

Date of survey: 4-3-21 by Warden & Enriquez •
Drawn by O'Steen 4-6-21
Bearings based on GPS, True North NAD 83
Class B Survey

Reference Material

- 1) Hancock Co. Tax Map
- 2) Aerial Photograph
- 3) Deeds as noted on Plat
- 4) Official Plat of 3rd Ward
City of Bay St. Louis

- = set 1/2" diameter rebar with a plastic cap bearing the name & license number of surveyor
- ⊙ = Fd 1/2" capped rebar set by PLS #3037

N/F = Now or formerly owned by
Fd = Found

POC = Point of Commencement
POB = Point of Beginning

(S) = Measurement as determined by survey

(R) = Measurement according to Record

—E— = Powerline
—x— = Fenceline

↗ = Land hook - same owner on both sides

— = Line graphically shortened to fit (not to scale)



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(601) 928-9866

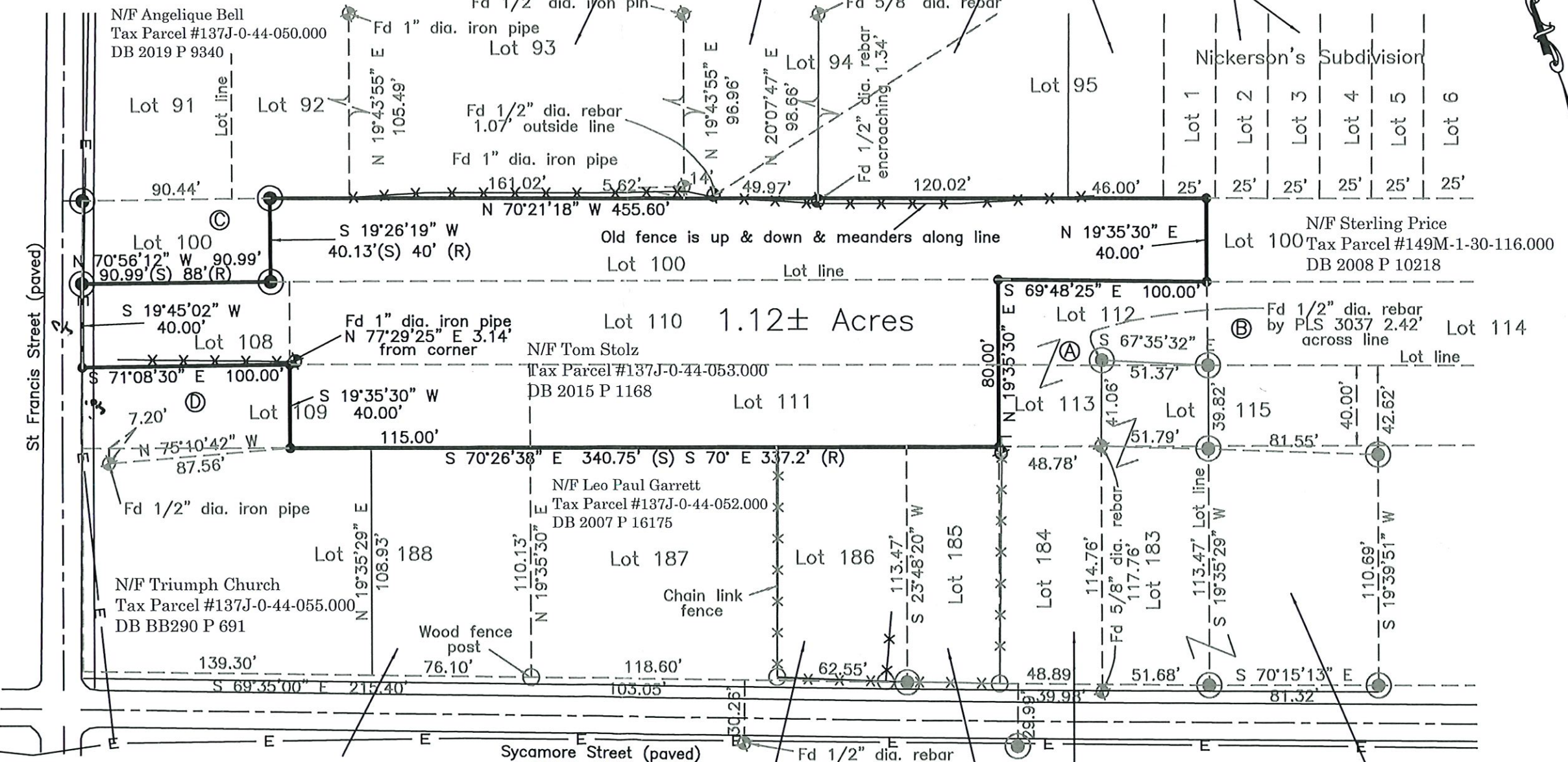
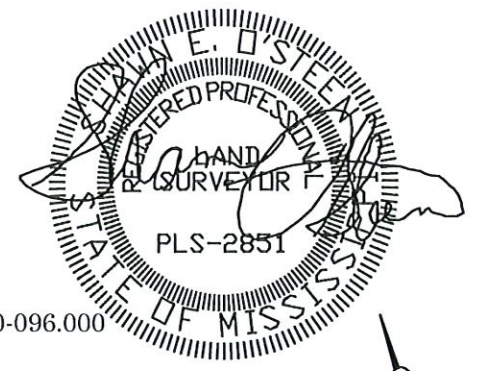
akssurveying.net

- Ⓐ N/F Florence Young
Tax Parcel #149M-1-30-119.000
DB BB259 P 853
- Ⓑ N/F Glen Bourgeois
Tax Parcel #149M-1-30-118.000
DB 2005 P 10201
- Ⓒ N/F Eric Nichols et al
Tax Parcel #137J-0-44-057.000
DB 2015 P 1168

- Ⓓ N/F James Sullivan
Tax Parcel #137J-0-44-056.000
DB 2009 P 2981
- N/F Johanna Dixon
Tax Parcel #137J-0-44-051.000
DB 2016 P 1882

- N/F Phalba Holmes
Tax Parcel #149M-1-30-095.000
DB 2016 P 1330 &
Tax Parcel #149M-1-30-094.000
DB 2016 P 14765

- N/F Nancy Peebles
Tax Parcel #149M-1-30-096.000
DB AA83 P 193



- N/F WSMBD, LLC
Tax Parcel #137J-0-44-054.000
DB 2014 P 11233

- N/F Matthew Stieffel
Tax Parcel #149M-1-30-126.001
DB 2017 P 11793

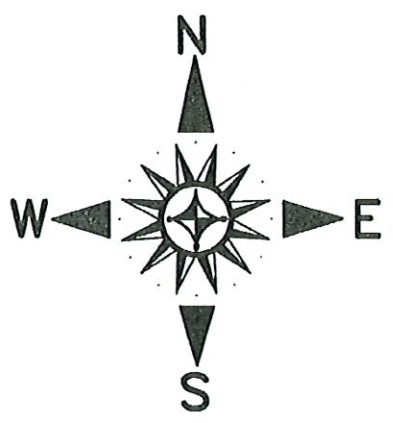
- N/F Lylie Nichols
Tax Parcel #149M-1-30-125.000
DB 2008 P 27082

- N/F Nathaniel Jones
Tax Parcel #149M-1-30-124.000
& 149M-1-30-124.001
DB 2019 P 5422

- N/F Floren Dedeaux
Tax Parcel #149M-1-30-126.000
DB 2007 P 19323

Shawn E. O'Steen, PLS 2851
78 KP Bolling Rd - Perkinston, MS 39573

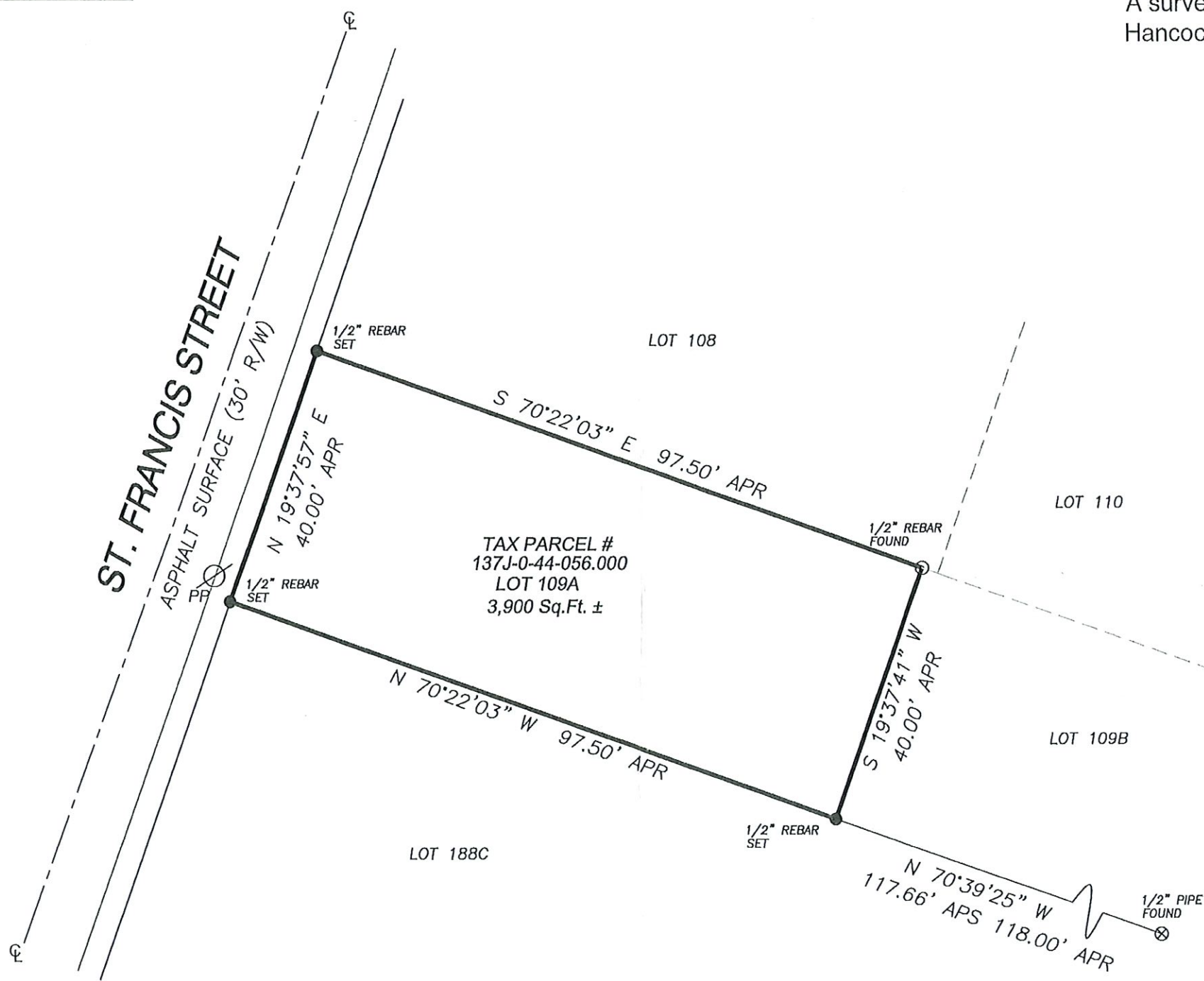
Proj.No. 213-22
Sheet 1 of 1



BOUNDARY SURVEY

LEGAL DESCRIPTION:

A survey of Lot 109A, 3rd Ward, Town of Waveland, Hancock County, Mississippi.



LEGEND:

- ⊕ CENTERLINE
- IRON ROD FOUND
- IRON ROD SET
- ⊗ IRON PIPE FOUND
- ⊙ POWER POLE
- APS AS PER SURVEY
- APR AS PER RECORD

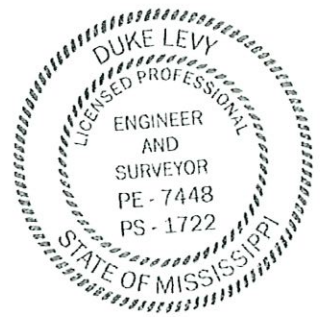
NOTES:

Surveyor has made no investigation or independent search for easements of record, encumbrances, restrictive covenants, ownership title evidence, or any other facts that an accurate and current title search may disclose. No attempt has been made as a part of this boundary survey to obtain or show data concerning existence, size, depth, condition, capacity, or location of any utility or municipal/public service facility. This survey meets Mississippi minimum requirements for a class "B" survey. Survey is valid only if print has original seal and signature of surveyor. No flood zone determination was performed as a part of this survey. An accurate determination can be made by ordering a FEMA Elevation Certificate.

REFERENCES:

- 1) DEED BOOK 2009; PAGE 2981
- 2) DEED BOOK 2022; PAGE 18129
- 3) TOWN OF WAVELAND WARD MAP
- 4) HANCOCK COUNTY TAX MAP

BEARINGS REFERENCED TO GEODETIC BY GPS OBSERVATIONS



In consideration of the fee paid, I declare that this survey made by me or under my immediate supervision is true and correct to the best of my professional knowledge, information, and belief.

Duke Levy
Duke Levy, RLS #1722

DUKE LEVY & ASSOCIATES, P.A.

DLA 4412 LEISURE TIME DRIVE
DIAMONDHEAD, MS 39525
(228) 343-9691 PHONE

SCALE: 1" = 20'	DATE: 02-08-2025
DRAWING: # 2025-026	CLIENT: MATT STIEFFEL

DRAWN BY: JLC

TO:

Planning and Zoning Commission
City of Bay St. Louis

RE:

1083 Hwy 90
Parcel No. 137F-2-26-009.000
Legal Description: PT 88 J BOUQUIE CL (BUS) HWY 90
Parcel No. 137F-2-26-010.000
Legal Description: PT 89 J BOUQUIE CL 26-8-14

Hearing Date: March 12, 2025

Application for a Minor Site Plan Review submitted by Bellamare Development. The application site plan is for a new convenience store and gas station with an attached coffee shop at the property located at 1083 Hwy 90. The property is zoned C-3 Highway Commercial District.

The administration recommends TABLE this minor site plan review.

City Engineering is still reviewing drainage calculations.

If I can be of any further assistance in this matter, please feel free to contact my office at 228-466-5516.

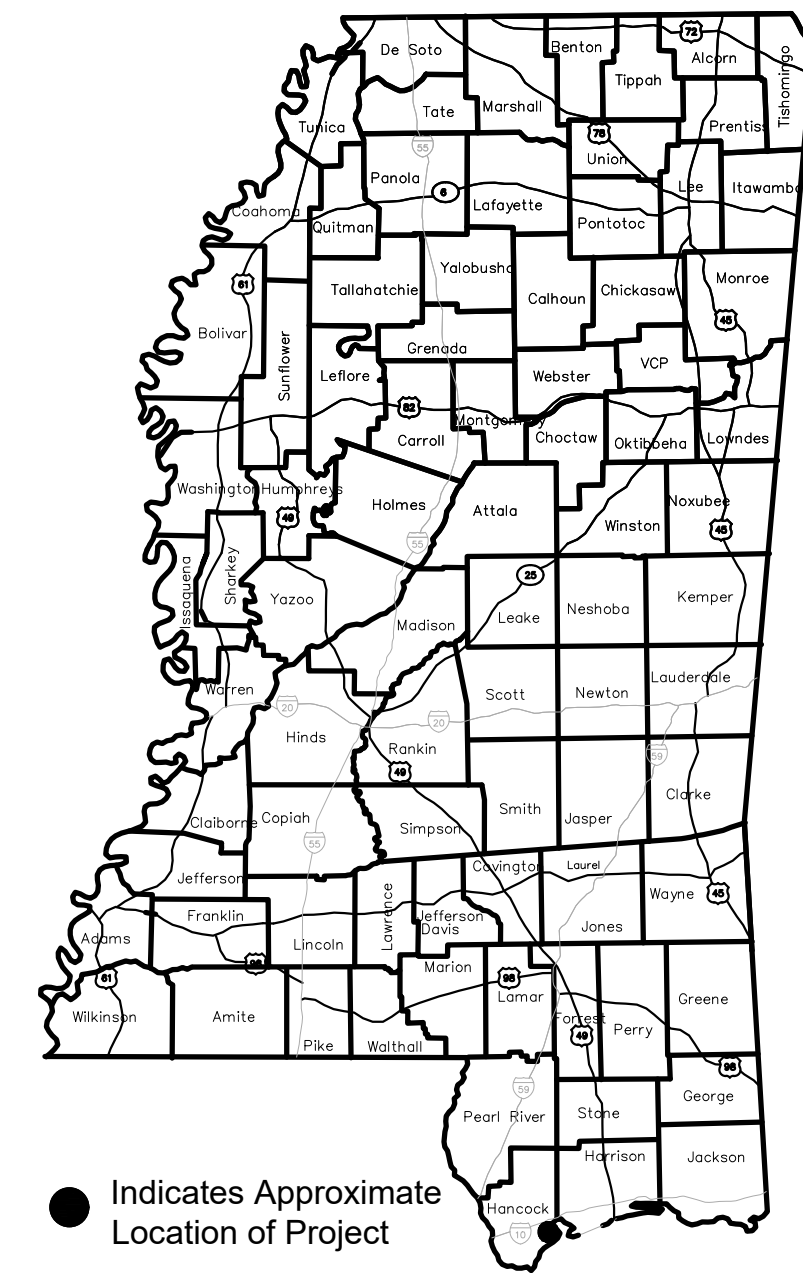
Jeremy L. Burke
Zoning Administrator

BAY ST. LOUIS CONVENIENCE STORE

1083 HIGHWAY 90, BAY SAINT LOUIS

HANCOCK COUNTY, MISSISSIPPI

(CIVIL PERMIT SET 276-1-2024)

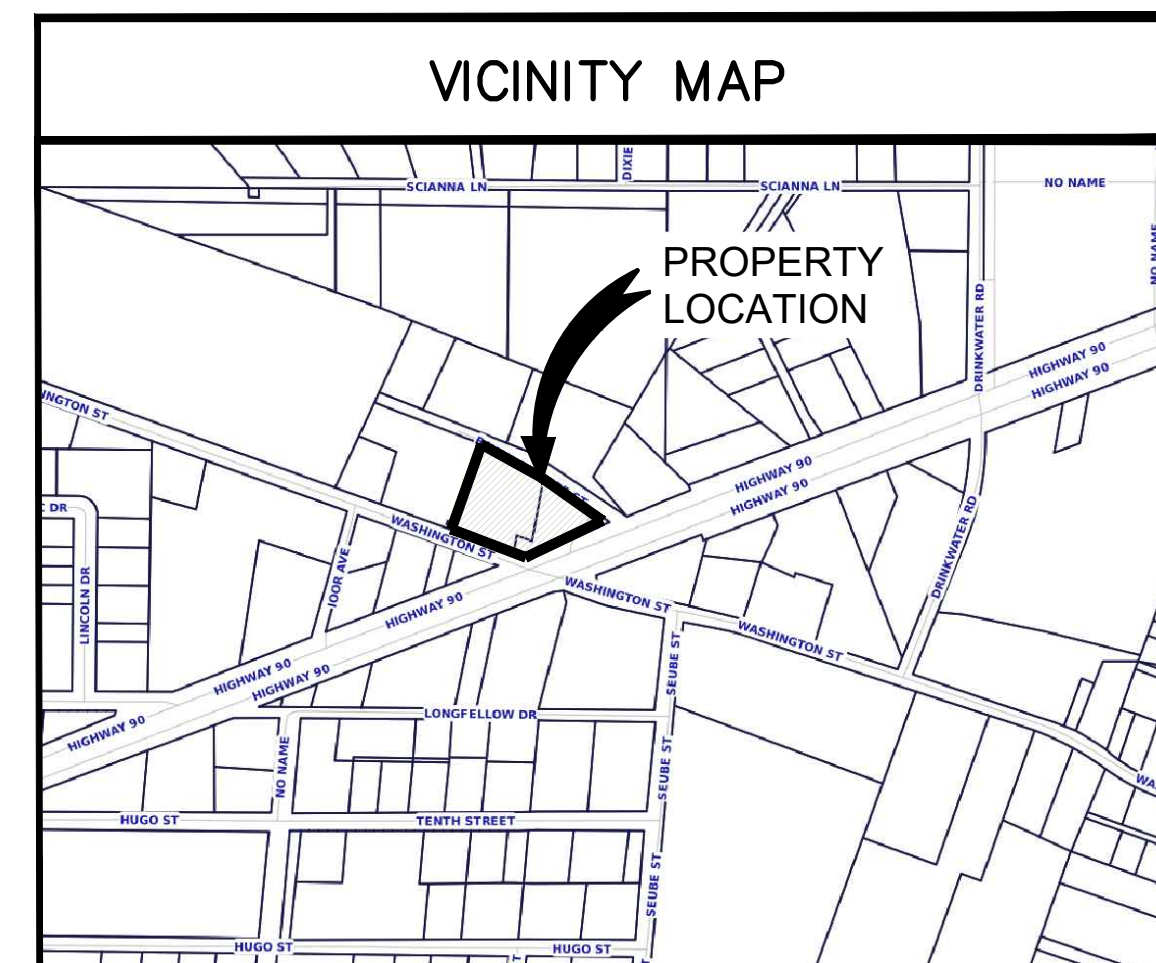


LOCATION MAP

BAY ST. LOUIS CONVENIENCE STORE
1083 HIGHWAY 90, BAY SAINT LOUIS
HANCOCK COUNTY, MISSISSIPPI
CIVIL PERMIT SET

INDEX TO DRAWINGS

SHEET NO.	DRAWING NO.	DESCRIPTION
1	T1.0	COVER SHEET
2	C001	EXISTING CONDITIONS
3	C110	EROSION, SEDIMENT AND POLLUTION CONTROL PLAN
4	C150	EROSION, SEDIMENT AND POLLUTION CONTROL PLAN DETAILS
5	C200	SITE PLAN
6	C250	SITE DETAILS I
7	C251	SITE DETAILS II
8	C300	GRADING PLAN
9	C310	DRAINAGE PLAN
10	C320	DRAINAGE PROFILES
11	C350	DRAINAGE DETAILS
12	C400	UTILITY PLAN
13	C450	UTILITY DETAILS



CIVIL ENGINEERING SERVICES



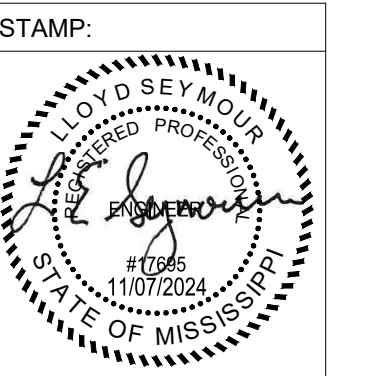
LIVE OAK ENGINEERING

2509 7TH AVE S. 955 HOWARD AVENUE
BIRMINGHAM, AL 35233 BILOXI, MS 39530
LIVEOAKENGINEERING.COM 205.637.3115

LOE JOB # -XXX-XX

SHEET REVISIONS:

#	DATE/REFERENCE

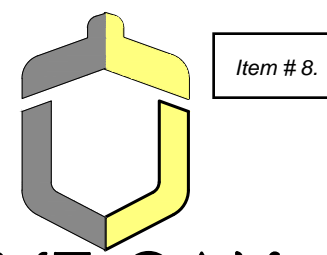


SHEET TITLE:

COVER SHEET

DATE: 11-7-2024
SHEET NUMBER: 1 OF 13

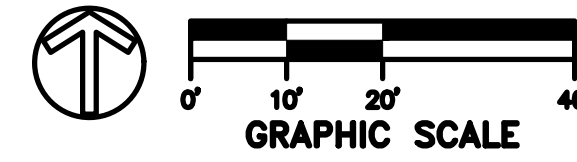
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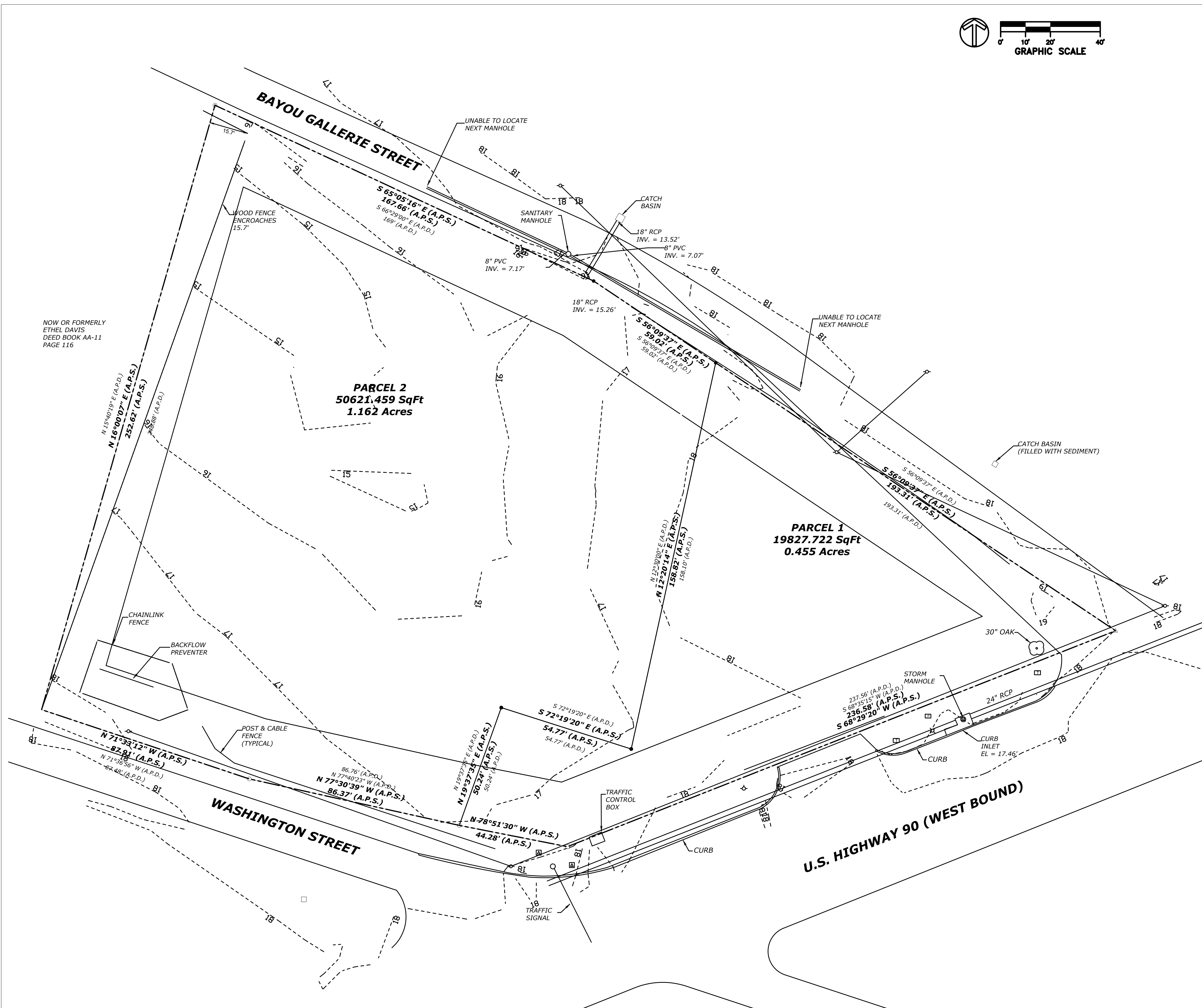
LIVE OAK ENGINEERING

2509 7TH AVE S.
BIRMINGHAM, AL 35233
955B HOWARD AVENUE
BILOXI, MS 39530
LIVEOAKENGINEERING.COM
LOE JOB# 276-1

BAY ST. LOUIS CONVENIENCE STORE
1083 HIGHWAY 90, BAY SAINT LOUIS
HANCOCK COUNTY, MISSISSIPPI
CIVIL PERMIT SET



ELEV =



NOW OR FORMERLY
ETHEL DAVIS
DEED BOOK AA-11
PAGE 116

PARCEL 2
50621.459 SqFt
1.162 Acres

PARCEL 1
19827.722 SqFt
0.455 Acres

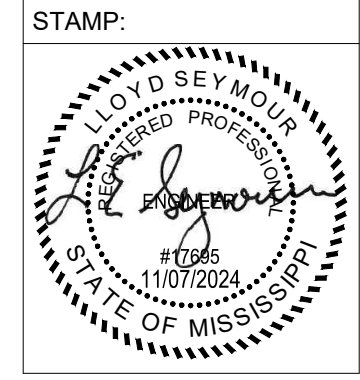
UNDERGROUND UTILITY NOTES

1. CONTRACTOR SHALL VERIFY LOCATIONS OF ALL PROJECT RELATED UTILITIES, BURIED AND ABOVE GROUND, REGARDLESS OF INCLUSION ON THESE PLANS. THE LOCATIONS OF ANY EXISTING UTILITIES SHOWN ARE APPROXIMATE ONLY.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK.
3. ALL CONTRACTOR DAMAGED UTILITIES SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.

THIS SURVEY IS PROVIDED FOR INFORMATIONAL PURPOSES ONLY!
CONTRACTOR SHALL CONSULT ORIGINAL SURVEY FOR ANY PROPERTY INFORMATION AND EXISTING CONDITIONS

SHEET REVISIONS:

#	DATE/REFERENCE

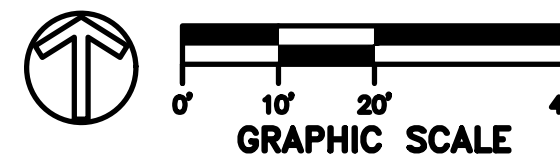


SHEET TITLE:
EXISTING CONDITIONS

DATE: 11-7-2024
SHEET NUMBER: 2 OF 13

C001

1 EXISTING CONDITIONS
C001 SCALE: 1" = 20'



ELEV =

SWPPP LEGEND

- 100.00 --- DENOTES EXISTING GRADE CONTOUR
- 100 — DENOTES FINISHED GRADE CONTOUR
- ↘ ↘ ↘ DENOTES STORMWATER FLOW DIRECTION
- □ — DENOTES PERIMETER CONTROLS
- DENOTES INLET PROTECTION
- DENOTES TREE PROTECTION

SWPPP NOTES

- THE CONTRACTOR SHALL MAINTAIN EROSION AND SEDIMENT CONTROLS DURING THE ENTIRE COURSE OF WORK TO PREVENT ANY SEDIMENT FROM LEAVING THE CONSTRUCTION SITE AND ENTERING ROADWAYS, STORM DRAINS SYSTEMS, DITCHES, SWALES, DETENTION BASINS, LOCAL WATER BODIES, AND/OR ADJACENT PROPERTIES.
- EROSION AND SEDIMENT CONTROL MEASURES SHALL BE INSTALLED PRIOR TO EXPOSING ANY SOIL.
- CONTRACTOR SHALL STAGE, TIME AND SEQUENCE CONSTRUCTION TO MINIMIZE THE SIZE OF EXPOSED SOIL AREAS AND THE TIME BETWEEN EXPOSING THE SOIL AREA AND FINISHING THE SOIL AREA.
- PERIMETER CONTROLS SHALL BE CONSTRUCTED OF SILT FENCE AND/OR APPROVED BMP'S AT LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ARCHITECT.
- ALL INLETS WITHIN THE CONTRACTOR'S LIMITS OF WORK SHALL BE PROTECTED WITH APPROVED EROSION AND SEDIMENT CONTROL MEASURES.
- CONTRACTOR SHALL PROVIDE VEGETATION FOR AREAS WHERE SOILS HAVE BEEN DISTURBED.
- THE CONTRACTOR IS RESPONSIBLE FOR THE REMOVAL OF ANY SEDIMENT THAT MIGRATES INTO THE STORM DRAIN SYSTEM. ANY SEDIMENT THAT HAS MIGRATED OFF OF THE PROJECT SITE SHALL BE REMOVED IMMEDIATELY UPON DISCOVERY.
- TEMPORARY EROSION AND SEDIMENT CONTROLS SHALL REMAIN FUNCTIONAL UNTIL PERMANENT EROSION AND SEDIMENT CONTROLS (SUCH AS PERMANENT GRASSING, PAVEMENT, ETC.) HAVE BEEN ESTABLISHED.
- ALL EROSION CONTROL DEVICES SHALL BE PROPERLY MAINTAINED DURING ALL PHASES OF CONSTRUCTION ACTIVITIES AND ALL DISTURBED AREAS HAVE BEEN STABILIZED. ADDITIONAL CONTROL DEVICES MAY BE REQUIRED DURING CONSTRUCTION IN ORDER TO CONTROL EROSION AND/OR OFFSITE SEDIMENTATION. ALL TEMPORARY CONTROL DEVICES SHALL BE REMOVED ONCE CONSTRUCTION IS COMPLETE AND THE SITE IS STABILIZED. ANY ADDITIONAL TEMPORARY CONTROL DEVICES THAT MAY BE REQUIRED SHALL BE PROVIDED AS PART OF THIS PROJECT AT NO ADDITIONAL COST TO THE OWNER.

CONSTRUCTION PHASE SEQUENCE

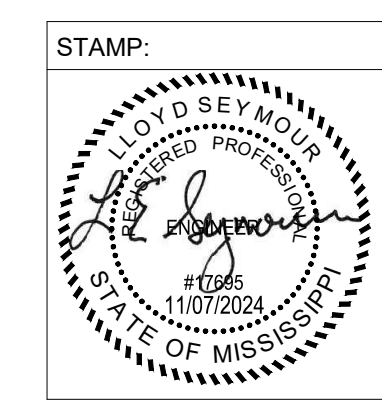
- PHASE ONE**
- INSTALL CONSTRUCTION ENTRANCE/EXIT
 - CLEAR AREA REQUIRED FOR SILT FENCE PLACEMENT
 - INSTALL SILT FENCE
- PHASE TWO**
- DEMOLITION AND REMOVAL OF DEMO DEBRIS
- PHASE THREE**
- CLEAR/GRUB REMAINING SITE AREAS
- PHASE FOUR**
- GRADE SITE TO ROUGH GRADES
 - INSTALL STORM DRAINAGE MEASURES
- PHASE FIVE**
- CONSTRUCT UTILITIES (WATER, SEPTIC)
 - BUILDING CONSTRUCTION BEGINS
- PHASE SIX**
- CONSTRUCT ROADS (PAVING, CURB AND GUTTER, SIDEWALKS)
 - BUILDING CONSTRUCTION CONTINUES
- PHASE SEVEN**
- BUILDING CONSTRUCTION COMPLETED
 - STABILIZE DISTURBED AREAS WITH SOLID SOD/SEED AND MULCH
 - INSTALL PERMANENT LANDSCAPING
- PHASE EIGHT**
- REMOVE BMP MEASURES
- PHASE NINE**
- REMOVE SILT FENCE
 - PROJECT COMPLETE, FINAL INSPECTION

LIVE OAK ENGINEERING
 2509 7TH AVE S.
 BIRMINGHAM, AL 35233
 955B HOWARD AVENUE
 BILOXI, MS 39530
 LIVEOAKENGINEERING.COM
 LOE JOB# 276-1

BAY ST. LOUIS CONVENIENCE STORE
1083 HIGHWAY 90, BAY SAINT LOUIS
HANCOCK COUNTY, MISSISSIPPI
CIVIL PERMIT SET

SHEET REVISIONS:

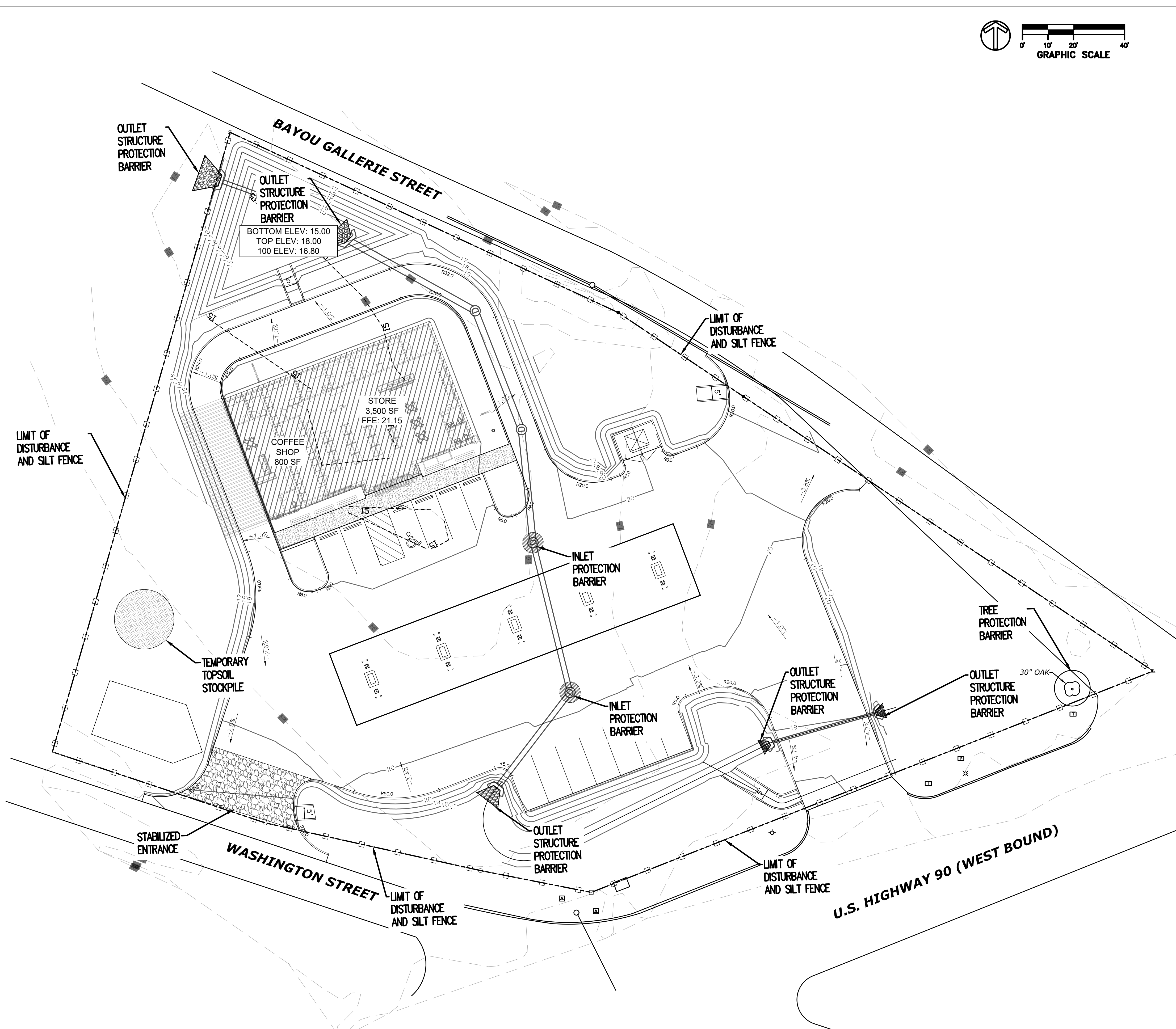
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SHEET TITLE:
EROSION AND SEDIMENT CONTROL PLAN

DATE: 11-7-2024
SHEET NUMBER: 3 OF 13

C110

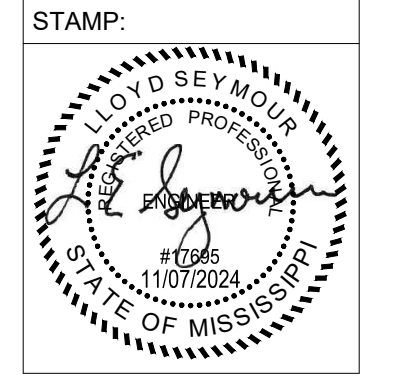


1 EROSION AND SEDIMENT CONTROL PLAN
SCALE: 1" = 20'

BAY ST. LOUIS CONVENIENCE STORE
 1083 HIGHWAY 90, BAY SAINT LOUIS
 HANCOCK COUNTY, MISSISSIPPI
 CIVIL PERMIT SET

SHEET REVISIONS:

#	DATE/REFERENCE



SHEET TITLE:

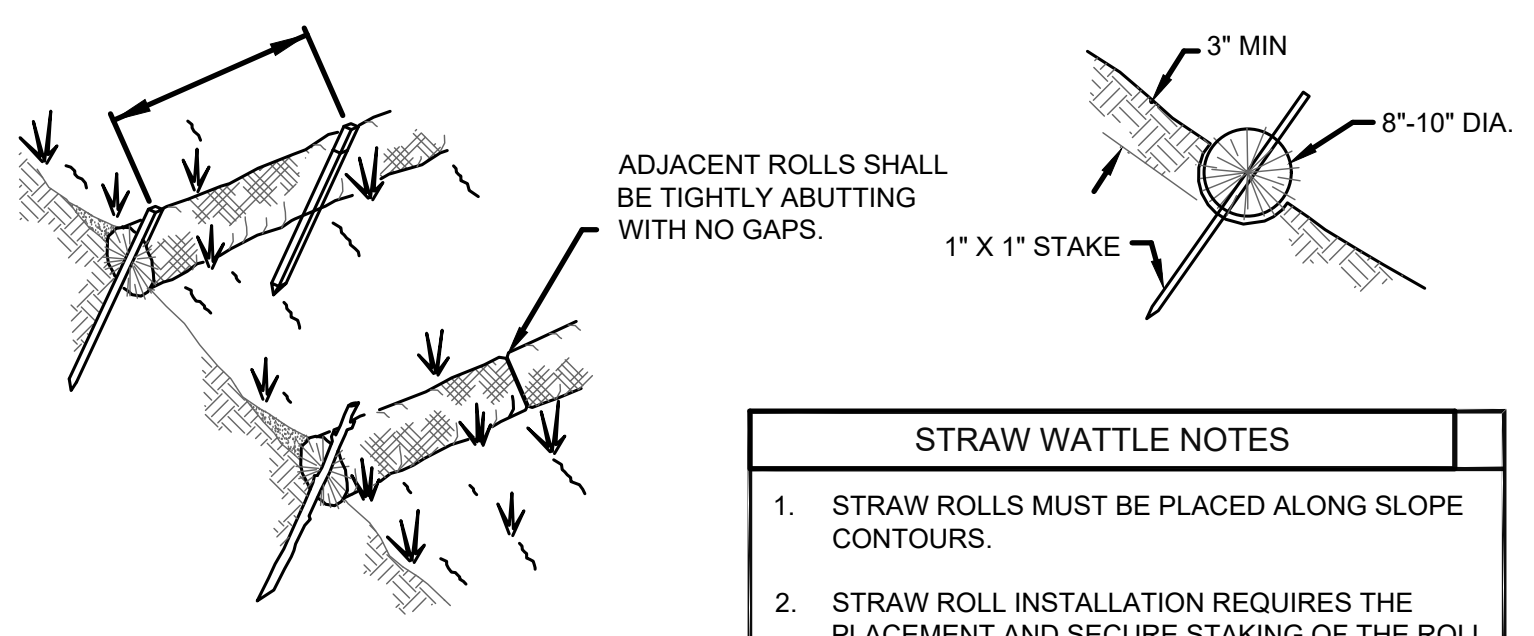
EROSION AND SEDIMENT CONTROL DETAILS

DATE: 11-7-2024
 SHEET NUMBER: 4 OF 13

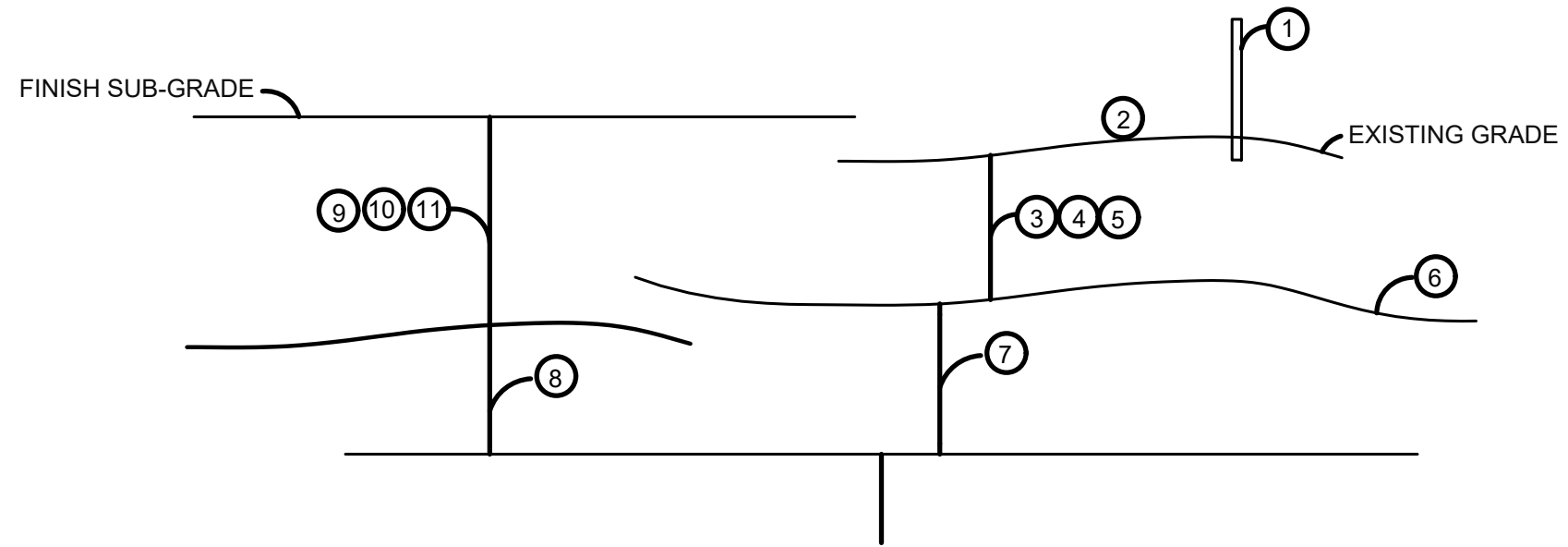
C150

STRAW WATTLE NOTES

1. STRAW ROLLS MUST BE PLACED ALONG SLOPE CONTOURS.
2. STRAW ROLL INSTALLATION REQUIRES THE PLACEMENT AND SECURE STAKING OF THE ROLL IN A 3" MINIMUM TRENCH DUG ON CONTOUR.
3. IF STAKES CAN NOT BE USED SECURE WITH SAND BAGS SPACED 4' APART.
4. RUNOFF MUST NOT BE ALLOWED TO RUN UNDER OR AROUND ROLL.



2 TYPICAL PERIMETER CONTROL DETAILS
 SCALE: NTS



EXCAVATION NOTES

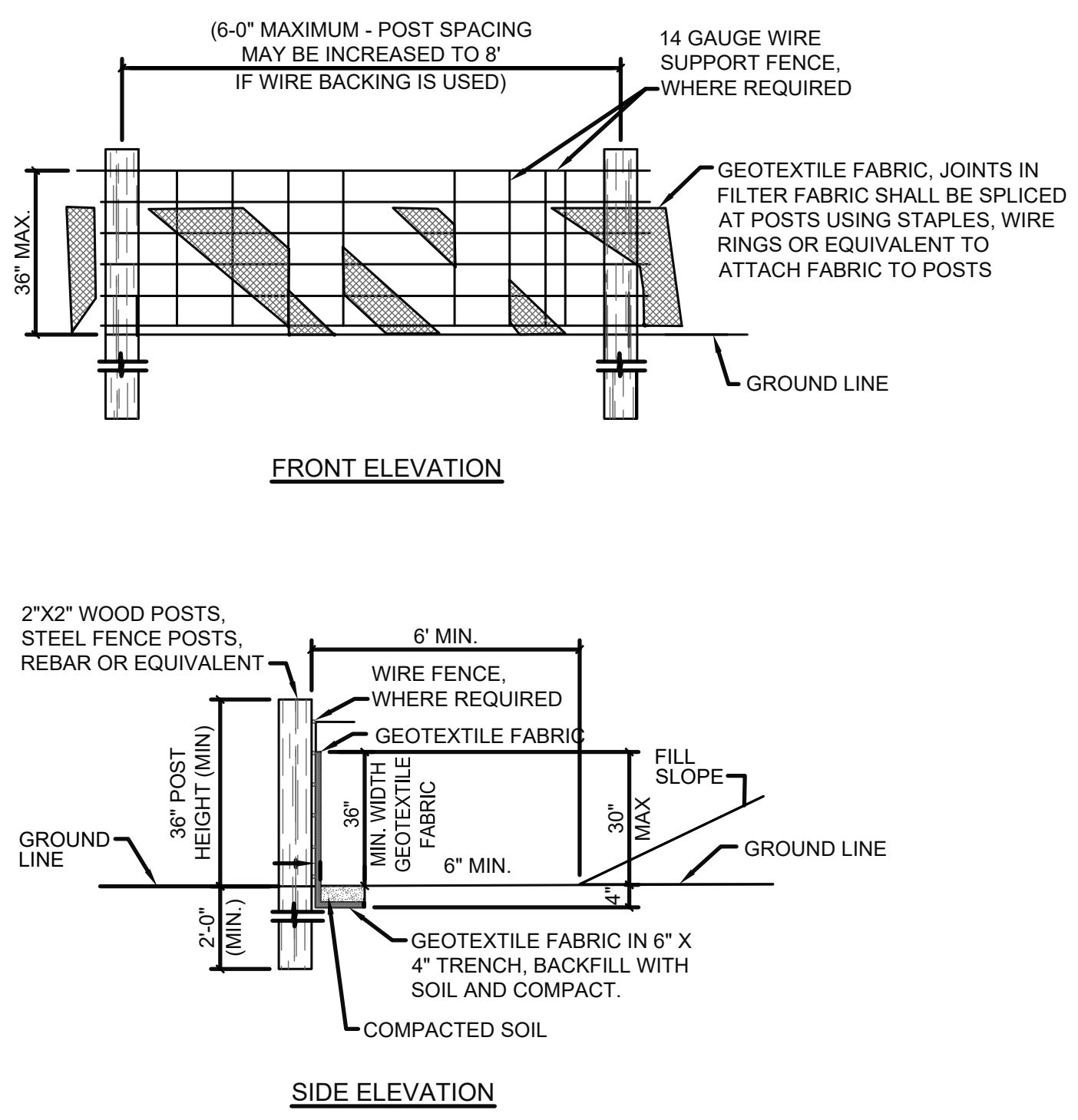
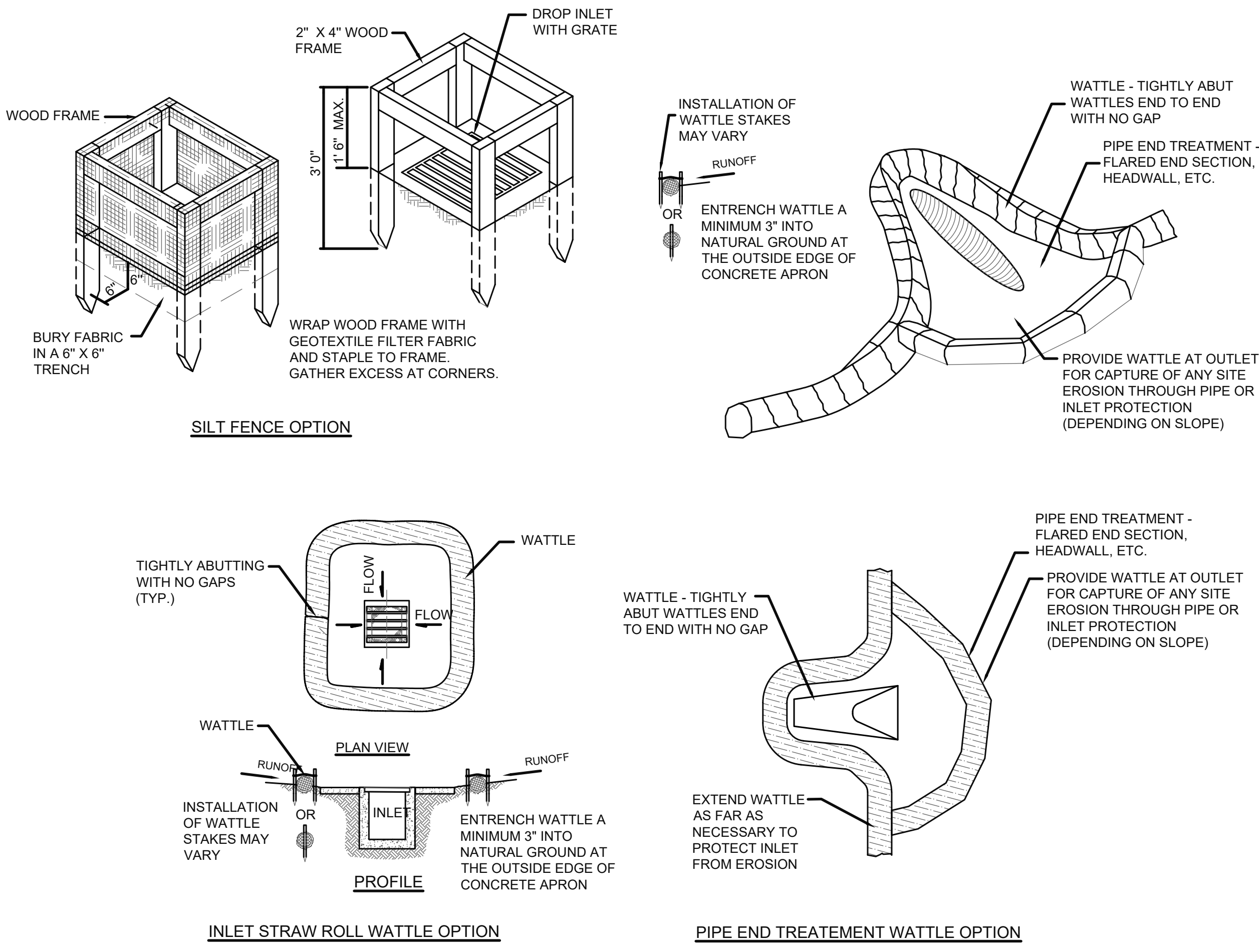
1. **EROSION CONTROL MEASURES:** THE INITIAL STEP FOR SITE PREPARATION SHALL BE TO ESTABLISH EROSION AND SEDIMENT CONTROL MEASURES.
2. **DRAINAGE:** EFFECTIVE DRAINAGE, INCLUDING DITCHING AND/OR POSITIVE GRADING, SHOULD BE ESTABLISHED AT THE BEGINNING OF SITE DEVELOPMENT AND MODIFIED AS NECESSARY DURING CONSTRUCTION. SEE SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS.
3. **CLEARING:** UPON COMPLETION OF DEMOLITION WORK THE CONTRACTOR SHALL VERIFY THAT ALL EXISTING PAVEMENTS, SLABS, FOUNDATIONS, SIDEWALKS, ABANDONED UTILITIES, AND OTHER MISCELLANEOUS DEBRIS HAVE BEEN COMPLETELY REMOVED TO AT LEAST A MINIMUM OF 5 FEET BEYOND THE PROPOSED BUILDING FOOTPRINTS AND NEW PAVEMENT AREAS.
4. **STRIPPING:** ONCE ALL PAVEMENTS, FOUNDATION AND DEBRIS HAVE BEEN REMOVED, STRIPPING EXCAVATIONS SHOULD BE CONTINUED TO APPROXIMATELY 6" BELOW EXISTING GRADE. STRIPPING EXCAVATION SHALL BE CARRIED OUT TO AT LEAST A MINIMUM OF 5 FEET BEYOND THE PROPOSED BUILDING FOOTPRINTS AND NEW PAVEMENT AREAS.
5. **TOPSOIL:** CONTRACTOR SHALL STOCKPILE TOPSOIL AND OTHER SUITABLE FILL MATERIAL TO BE REUSED ON SITE. ALL UNSUITABLE SOILS SHALL BE REMOVED FROM THE SITE.
6. **SUB-GRADE PREPARATION:** ONCE ALL TOPSOIL, ORGANIC MATERIALS, AND/OR OTHER UNSUITABLE SOILS HAVE BEEN REMOVED, THE FILL AREAS SHOULD BE LEVELED AND SEATED USING A STATIC ROLLER AND THEN PROOF-ROLLED USING A LOADED TANDEM AXLE DUMP TRUCK WEIGHING AT LEAST 20 TONS TO IDENTIFY AREAS OF WEAK SOIL.
7. **MUCK:** WHEN EXCAVATIONS ENCOUNTER UNSUITABLE MATERIALS BELOW THE BOTTOM OF THE STRIPPING AND UNDERCUT EXCAVATIONS, THE CONTRACTOR WILL BE REQUIRED TO REMOVE THE MATERIAL AND BACKFILL WITH APPROPRIATE FILL MATERIAL AS APPROVED BY THE ENGINEER. THE DEPTH AND WIDTH OF MUCK EXCAVATION WILL BE AS DIRECTED OR APPROVED BY THE ENGINEER. THE CONTRACTOR WILL NOT BE COMPENSATED FOR EXCAVATION BEYOND THE DIMENSIONS AND ELEVATIONS AS SHOWN ON THE PLANS OR EXCAVATION THAT HAS NOT BEEN DIRECTED OR APPROVED BY THE ENGINEER. ALL MUCK AND FILL FORMATIONS BELOW THE BOTTOM OF THE STRIPPING/UNDERCUT EXCAVATIONS SHALL BE MEASURED AS UNIT PRICE PAY ITEMS PER THE UNSUITABLE SOILS ALLOWANCE.
8. **ON-SITE SOILS:** ON-SITE SOILS ARE SUITABLE TO USE AS STRUCTURAL FILL, BUT WILL LIKELY REQUIRE MOISTURE CONDITIONING TO MEET THE REQUIREMENTS OF STRUCTURAL FILL. IF CONSTRUCTION IS PERFORMED DURING THE WET SEASON THE NEAR SURFACE SOILS MAY BECOME UNSTABLE UNDER CONSTRUCTION TRAFFIC AND REQUIRE ADDITIONAL UNDERCUT.
9. **STRUCTURAL FILL:** IF REQUIRED, STRUCTURAL FILL MATERIAL SHOULD BE SILTY SAND, CLAYEY SAND, OR LEAN CLAY (UNIFIED CLASSIFICATION SM, SC, OR CL) TYPE SOIL. THE PLASTICITY INDEX OF FILL SHOULD BE A MAXIMUM OF 20 AND HAVE A MAXIMUM LIQUID LIMIT OF 40.
10. **COMPACTION:** MATERIALS SHOULD BE COMPACTED TO A MINIMUM OF 95% OF MAXIMUM DRY DENSITY PER STANDARD PROCTOR (ASTM D 698). COMPACTION SHOULD BE ACHIEVED PRIOR TO PLACING SUBSEQUENT LIFTS. FILL SOILS SHOULD BE PLACED IN MAXIMUM LOOSE LIFTS OF 8" AT A MOISTURE CONTENT COMPARABLE (±3%) TO THE OPTIMUM MOISTURE CONTENT ESTABLISHED IN THE LABORATORY.
11. **TESTING:** IN PLACE DENSITY TESTS SHOULD BE MADE PER 2,500 SQUARE FEET PER LIFT WITHIN THE BUILDING FOOTPRINT AND 5,000 SQUARE FEET PER LIFT UNDER PAVEMENT.

5 EXCAVATION DETAIL AND NOTES
 SCALE: NTS

PERIMETER CONTROL NOTES

1. FILTER FABRIC FENCES SHALL BE INSTALLED ALONG CONTOUR WHENEVER POSSIBLE.
1. POSTS SHALL BE SPACED A MAXIMUM OF 6' O.C. SPACING MAY BE INCREASED TO 8' IF WIRE BACKING IS USED.
2. POSTS SHALL BE A MINIMUM OF 5'-0" IN LENGTH. IN ADDITION POSTS SHALL BE EITHER 2" x 2" N.D. WOOD POST OR HEAVY DUTY STEEL T-POSTS WITH PROJECTIONS FOR WIRE FASTENING.
3. WIRE SUPPORT FENCE SHALL BE A MINIMUM OF 36" IN HEIGHT. SHALL NOT EXTEND MORE THAN 36" ABOVE THE GROUND, AND SHALL EXTEND 2" INTO THE TRENCH.
4. WIRE FENCE SHALL BE A MINIMUM OF 14 GAUGE AND SHALL HAVE A MAXIMUM MESH SPACING OF 6". WIRE SHALL BE SECURELY FASTENED TO THE UPSLOPE, PROJECT SIDE OF POSTS USING HEAVY DUTY STAPLES (AT LEAST 1" LONG), TIE WIRES OR HOG RINGS.
5. GEOTEXTILE FABRIC SHALL BE A MINIMUM OF 36" IN HEIGHT, AND SHALL NOT EXTEND MORE THAN 36" ABOVE THE ORIGINAL GROUND SURFACE.
6. FABRIC SHALL BE STAPLED OR WIRED TO THE FENCE AT THE TOP, MIDDLE, AND BOTTOM OF EACH POST. IN ADDITION THE FABRIC SHALL BE STAPLED OR WIRED TO THE WIRE FENCE APPROXIMATELY ONE HALF (1/2) THE DISTANCE BETWEEN THE POSTS AT THE TOP, MIDDLE AND BOTTOM OF THE WIRE FENCE.
7. GEOTEXTILE FABRIC SHALL BE SPliced TOGETHER ONLY AT A SUPPORT POST, WITH A MINIMUM OF 3" OVERLAP. EACH FREE END OF THE FABRIC SHALL BE SECURELY TIED TO THE WIRE FENCE AT 6" O.C. VERTICALLY.
8. SILT FENCES SHALL BE MAINTAINED FOR THE DURATION OF THE PROJECT. THE ENTIRE LENGTH OF FENCE SHALL BE CHECKED FOR ANY DAMAGES ON A DAILY BASIS AND BEFORE AND AFTER ANY RAINFALL EVENT, FOR ANY DAMAGES FOUND SHALL BE REMEDIATED BEFORE THE DAY'S END AT NO ADDITIONAL COST TO THE OWNER.
9. SILT FENCES SHALL BE MAINTAINED TO PREVENT ANY MATERIAL FROM MIGRATING FROM THE UPSLOPE SIDE OF THE FENCE. ANY REQUIRED MAINTENANCE OF THE SILT FENCE SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
10. SEDIMENT DEPOSITS SHALL BE REMOVED AFTER EACH RAINFALL EVENT AND WHEN DEPOSITS REACH APPROXIMATELY ONE-HALF (1/2) THE HEIGHT OF THE FENCE.
11. IN ORDER TO PREVENT SEDIMENT LADEN STORM WATER FROM BY-PASSING THE FENCE, IN AREAS WHERE SILT FENCES ARE NOT UTILIZED ON ALL SIDES OF A DISTURBED AREA, THE FENCE SHALL EXTEND BEYOND THE DISTURBED AREA IN J-HOOK SHAPE ON EACH END AS SHOWN IN THE ISOLATED SILT FENCE INSTALLATION PLAN VIEW.

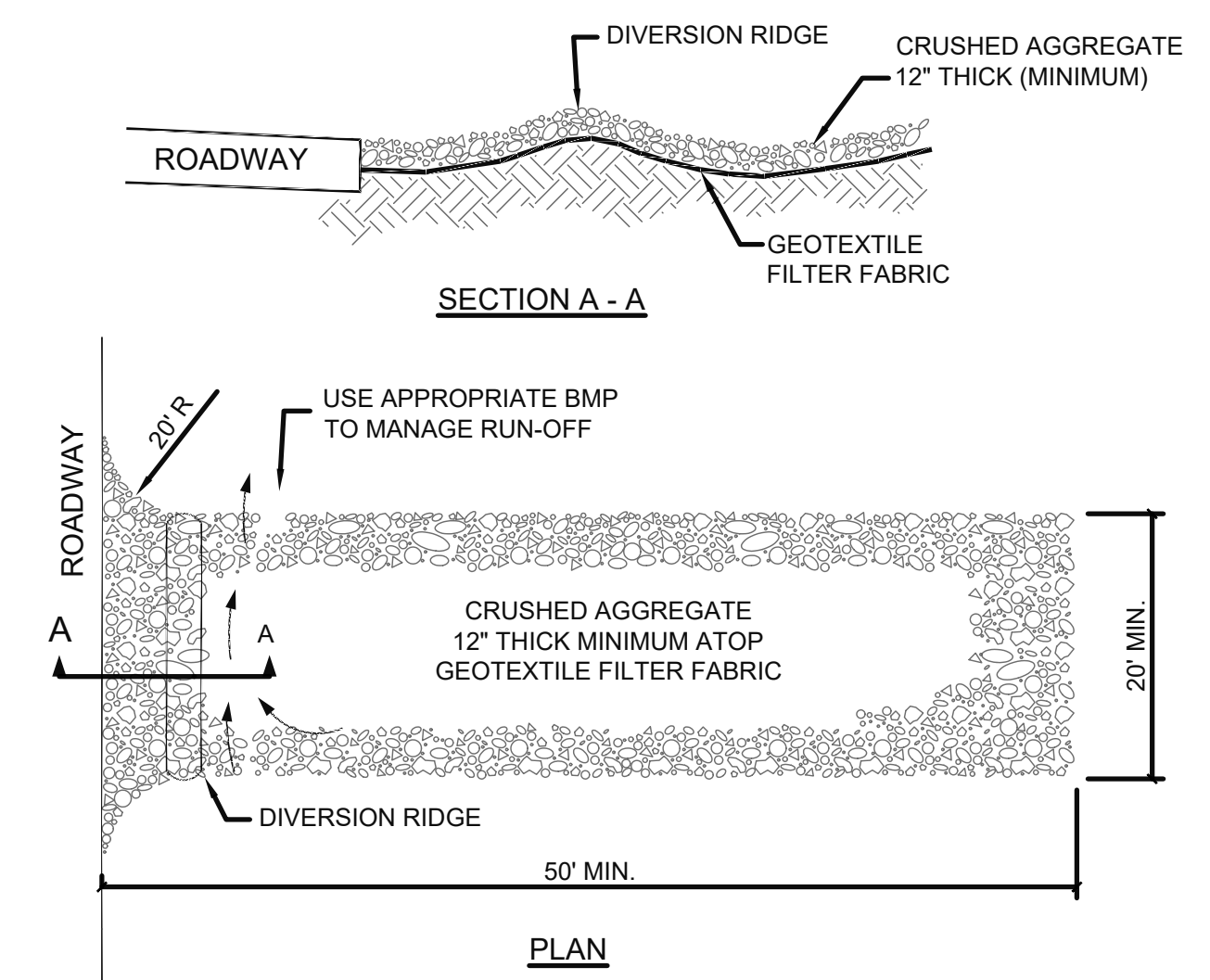
4 TYPICAL INLET/OUTLET PROTECTION DETAIL
 SCALE: NTS



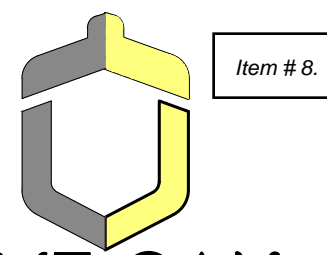
1 TYPICAL PERIMETER CONTROL DETAILS
 SCALE: N.T.S.

CONSTRUCTION ENTRANCE/EXIT NOTE

1. CRUSHED AGGREGATE SHALL BE COMPRISED OF STONE RANGING FROM 3" MINIMUM TO 6" MAXIMUM IN SIZE.
2. THE ENTRANCE SHALL BE PROPERLY GRADED TO PREVENT THE FLOW OF SEDIMENT ONTO PUBLIC ROADWAY. ALL MATERIALS SPILLED, DROPPED, WASHED OR TRACKED FROM VEHICLES ONTO ROADWAYS OR INTO STORM DRAINS SHALL BE REMOVED IMMEDIATELY.
3. THE ENTRANCE SHALL BE PROPERLY MAINTAINED FOR THE DURATION OF THE PROJECT TO PREVENT THE TRACKING OF SEDIMENT ONTO PUBLIC ROADWAY. ALL MAINTENANCE AND REPAIRS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
4. THE ENTRANCE SHALL BE CHECKED ON A DAILY BASIS AND BEFORE AND AFTER ANY RAINFALL EVENT FOR ANY DAMAGES. ANY DAMAGES FOUND SHALL BE REMEDIATED BEFORE THE DAYS END.
5. MEASURES SHALL BE TAKEN TO PREVENT VEHICULAR TRAFFIC FROM BYPASSING THE CONSTRUCTION ENTRANCE DURING INGRESS AND EGRESS.



3 TYPICAL CONSTRUCTION ENTRANCE/EXIT DETAIL
 SCALE: NTS

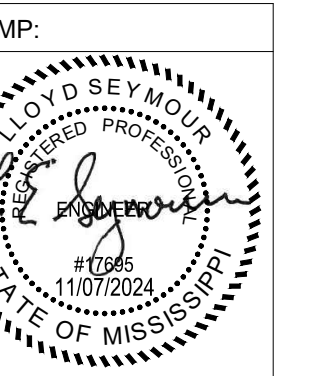


LIVE OAK ENGINEERING

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LOE JOB# 276-1

BAY ST. LOUIS CONVENIENCE STORE
1083 HIGHWAY 90, BAY SAINT LOUIS
HANCOCK COUNTY, MISSISSIPPI
CIVIL PERMIT SET

SHEET REVISIONS:	
#	DATE/REFERENCE

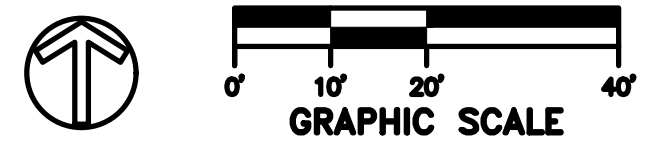


SHEET TITLE:
SITE PLAN

DATE: 11-7-2024
SHEET NUMBER: 5 OF 13

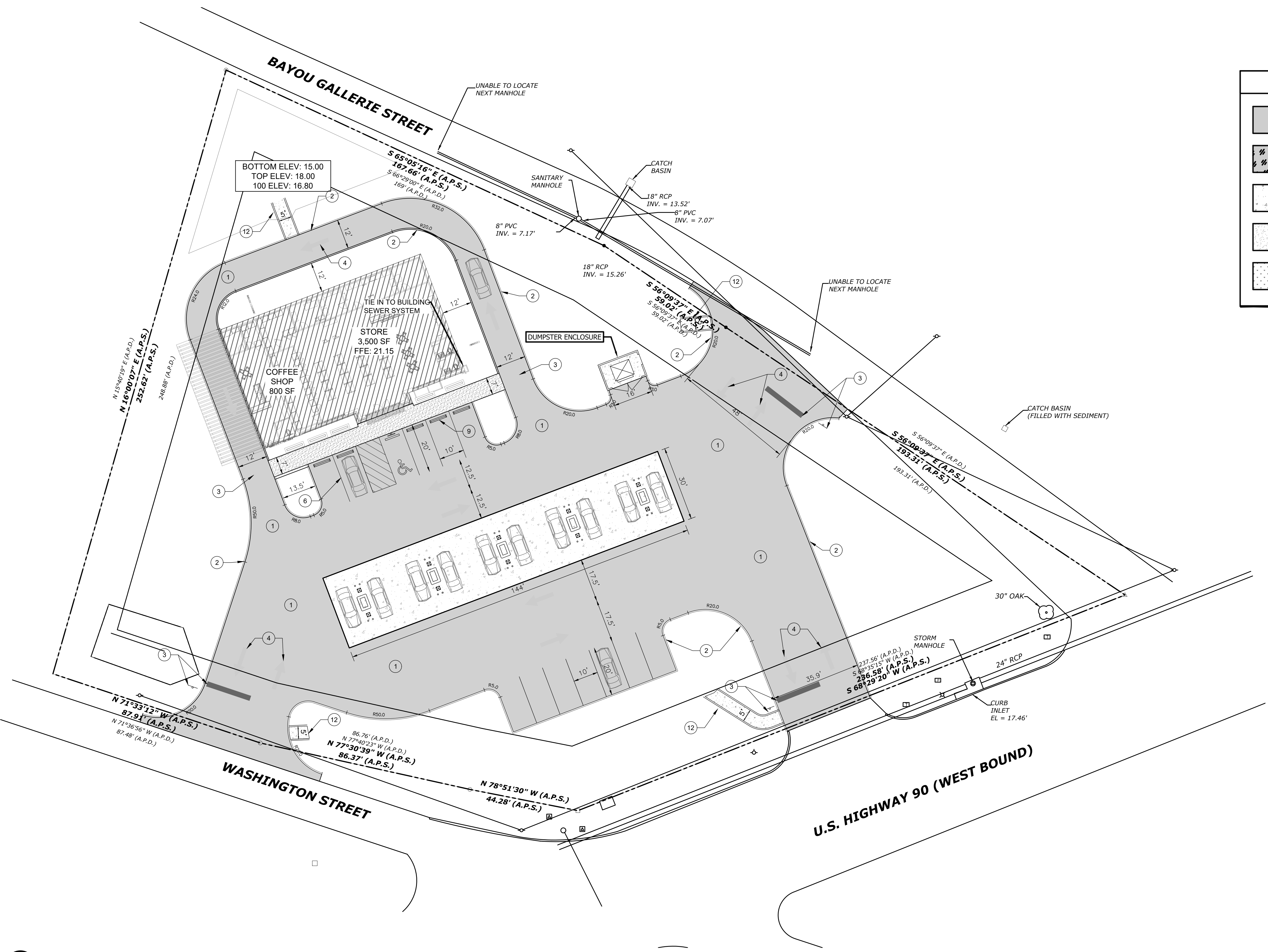
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ELEV =

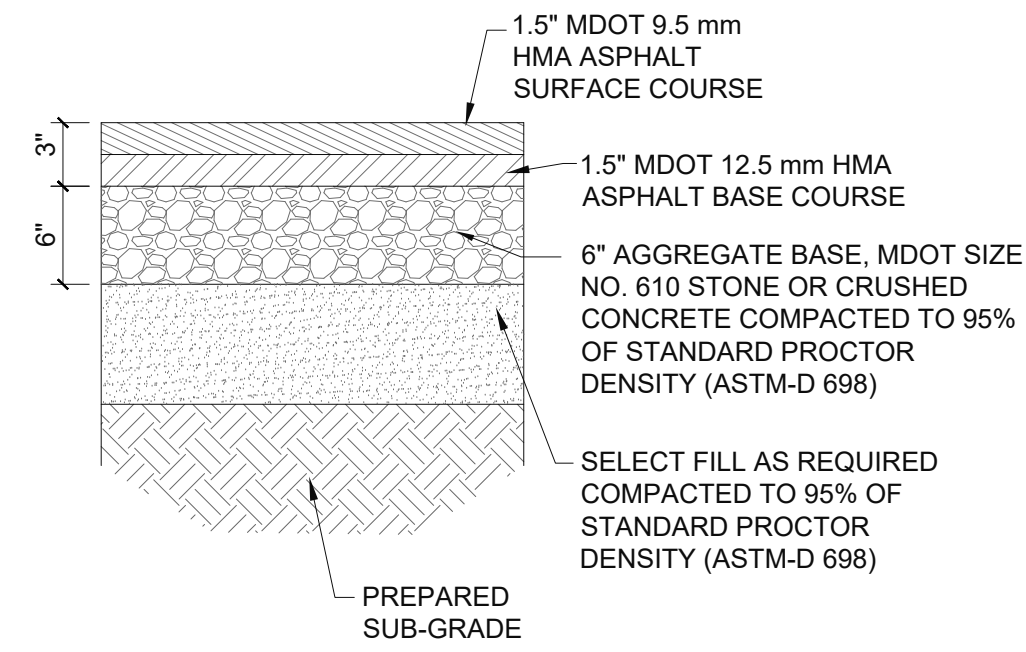


CIVIL SITE LEGEND	
	DENOTES LIGHT DUTY ASPHALT PAVEMENT (SEE PAVEMENT DETAILS)
	DENOTES HEAVY DUTY ASPHALT PAVEMENT (SEE PAVEMENT DETAILS)
	DENOTES 6" HEAVY DUTY CONCRETE PAVEMENT (SEE PAVEMENT DETAILS)
	DENOTES 4" CONCRETE SIDEWALK PAVEMENT (SEE PAVEMENT DETAILS)
	DENOTES GRASS PAVERS WITH PAVER EDGE (SEE PAVEMENT DETAILS)

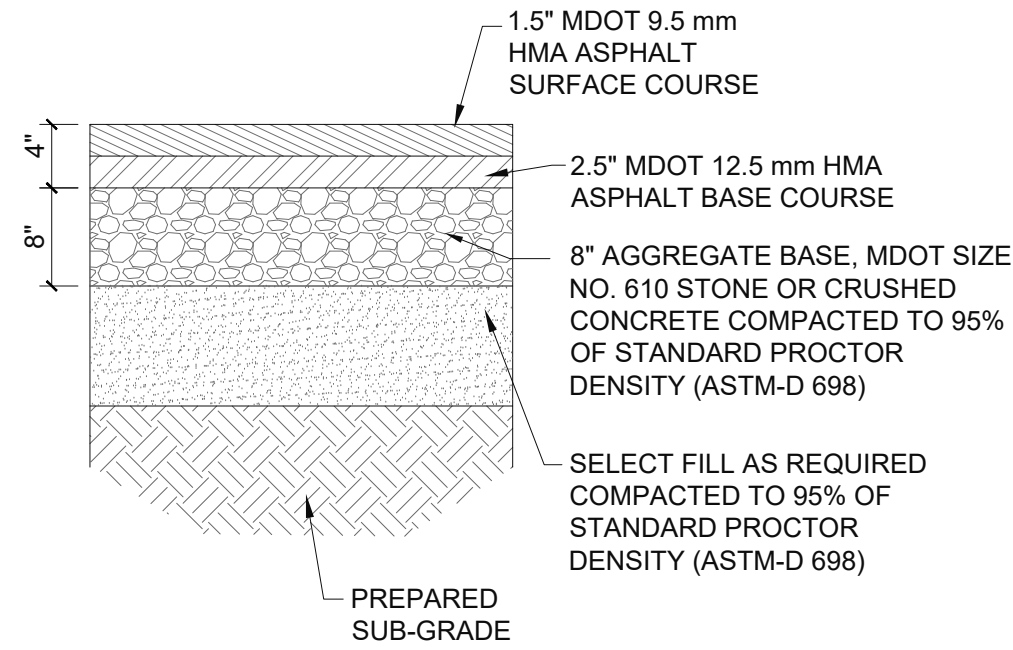
SITE WORK KEYNOTES	
①	HEAVY DUTY ASPHALT PAVEMENT AND GRANULAR BASE (DTL 2 & PAGE 250).
②	6" WIDE CONCRETE CURB/GUTTER (DTL 5 PAGE 250).
③	STOP BAR AND STOP SIGN
④	HORIZONTAL DEMARICATION PAINTING
⑤	GREEN SPACE TO RECEIVE TOPSOIL AND SOD. TOPSOIL MUST CONTAIN NO STONES ROOTS, TRASH, ETC. AND MUST BE UNIFORMLY DISTRIBUTED TO RECEIVE SOD PROVIDE AND INSTALL IRRIGATION SYSTEM AS REQUIRED FOR LOCAL CLIMATE CONDITIONS.
⑥	4" WIDE PAINTED TRAFFIC STRIPES WHITE.
⑦	HEAVY DUTY CONCRETE PAVEMENT
⑧	GAS DISPENSER
⑨	CAR STOP
⑩	CONCRETE STOP
⑪	PICK UP WINDOW
⑫	CONCRETE FLUME



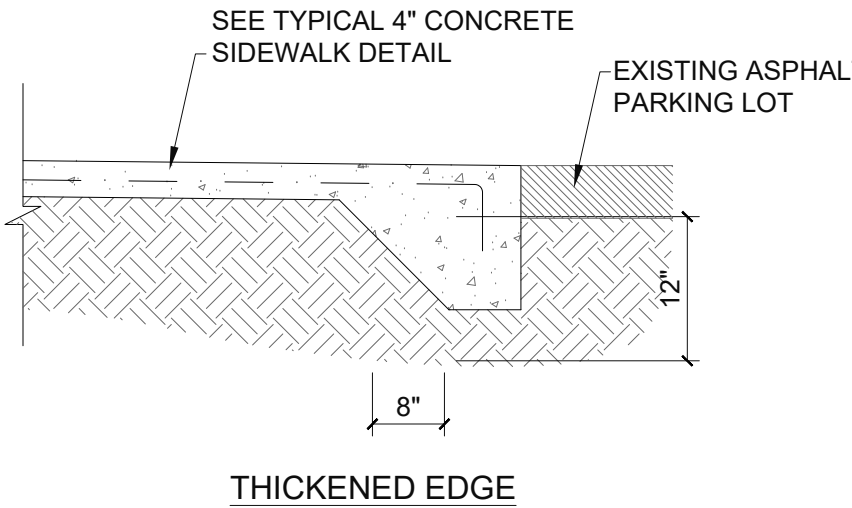
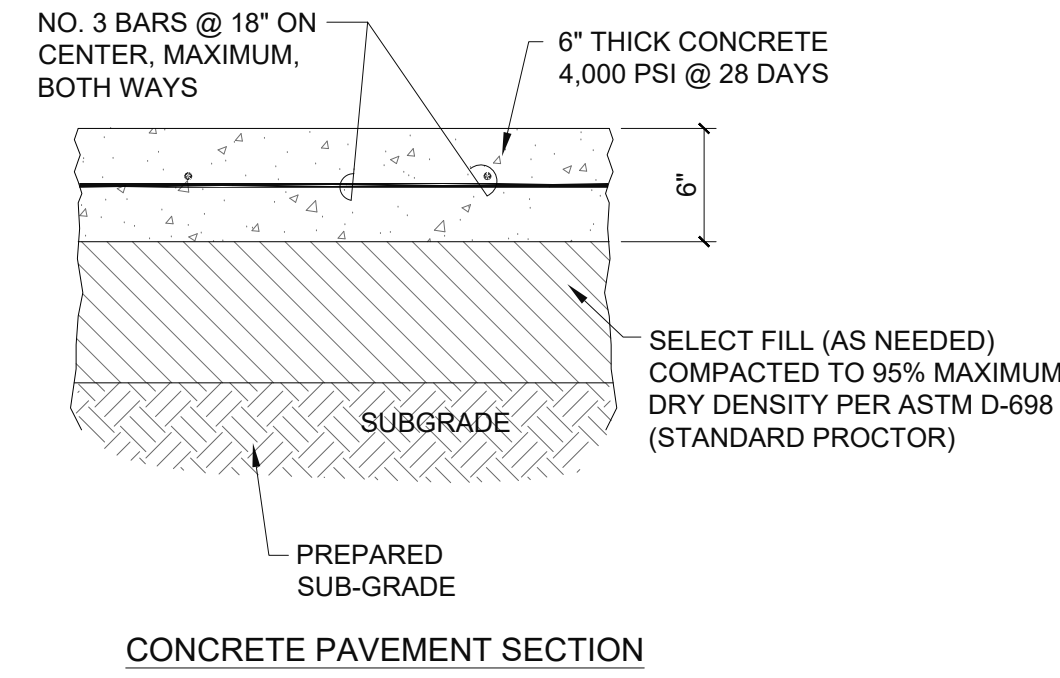
1 CIVIL SITE PLAN
SCALE: 1" = 20'



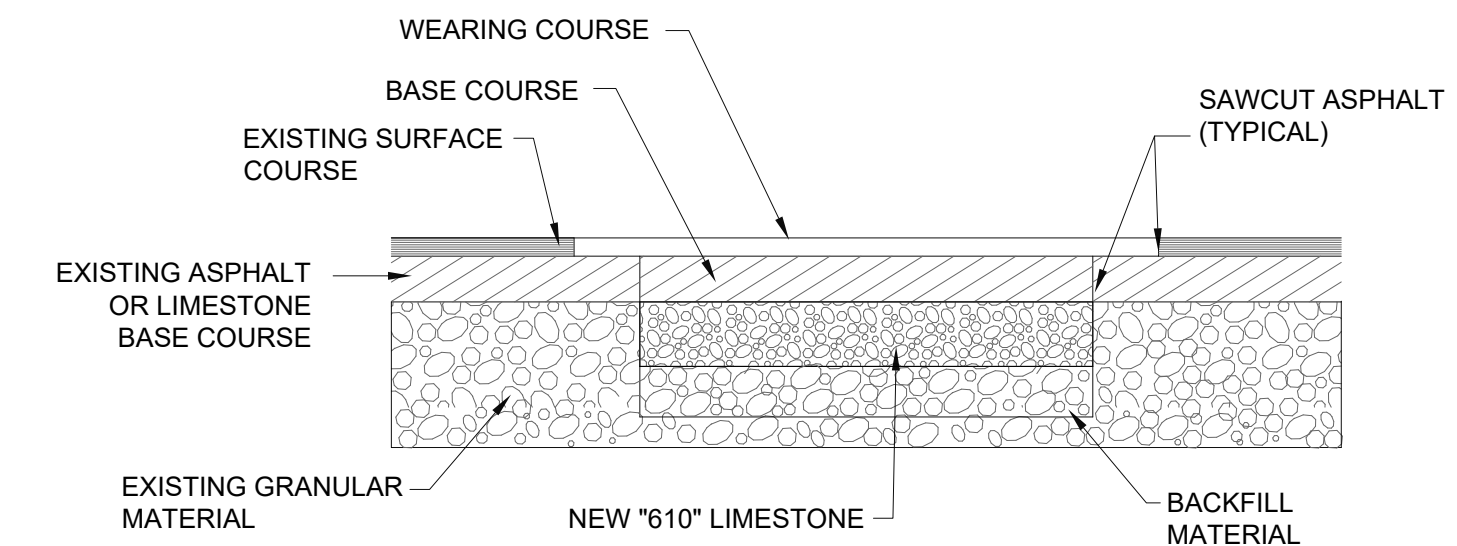
1 TYPICAL LIGHT DUTY ASPHALT PAVEMENT SECTION
SCALE: NTS



2 TYPICAL HEAVY DUTY ASPHALT PAVEMENT SECTION
SCALE: NTS



3 TYPICAL 6" CONCRETE PAVEMENT DETAILS
SCALE: NTS



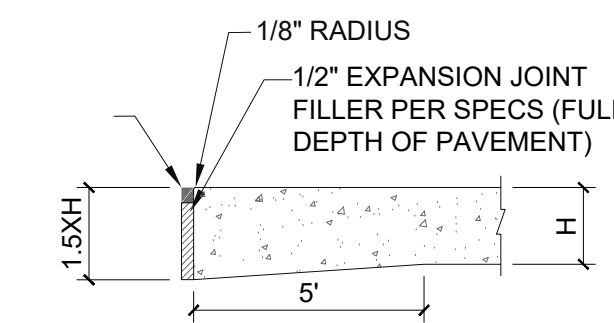
ASPHALT STREET REPAIR NOTES

- ALL MATERIALS AND CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE STATE OF MISSISSIPPI DEPARTMENT OF TRANSPORTATION STANDARDS.
- MIX DESIGNS: BASE COURSE: HMA, TYPE ST, 19 mm
WEARING COURSE: HMA, TYPE ST, 9.5 mm
- EXISTING GRANULAR MATERIAL, NEW BACKFILL MATERIAL AND NEW "610" LIMESTONE SHALL BE COMPACTED TO 95% PROCTOR PER ASTM D1557.
- TACK COAT SHALL BE APPLIED WHERE NEW ASPHALT MEETS EXISTING ASPHALT.
- REFER TO OTHER DETAILS FOR UTILITY PLACEMENT AND PAVEMENT MARKINGS AS APPLICABLE.

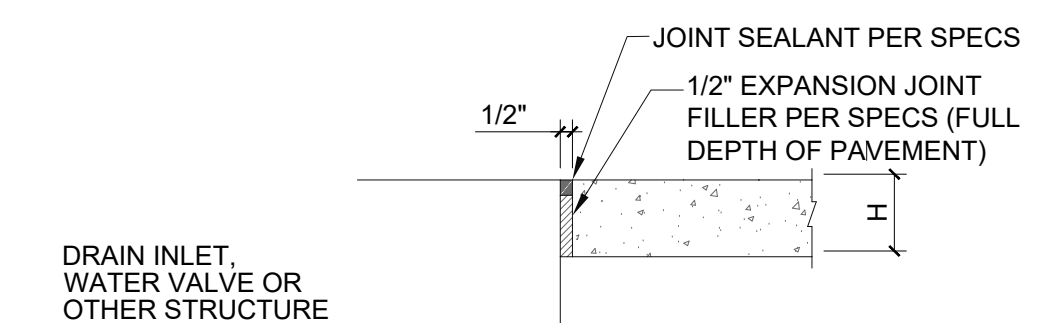
4 TYPICAL ASPHALT STREET PAVEMENT REPAIR SECTION
SCALE: NTS

4" CONCRETE PAVEMENT NOTES

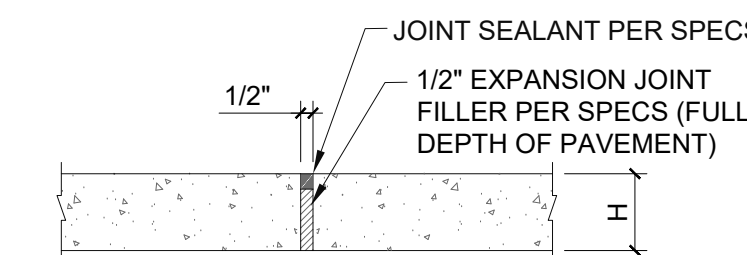
- ALL JOINTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH ACI 330R-08 OR SECTION 3.07 IN THE NRMCA GUIDE SPECIFICATIONS.
- EXPANSION JOINTS SHALL BE LOCATED WHERE SIDEWALK ABUTS CONCRETE DRIVEWAYS, CURB OR OTHER ADJACENT STRUCTURES.
- CONTRACTION JOINTS SHALL BE PLACED AT INTERVALS OF APPROXIMATELY 5 FEET OR AT A SPACING THAT MATCHES THE ADJACENT CURB.
- JOINTS SHALL BE PROVIDED WITHIN TWELVE (12) HOURS OF FINISHING CONCRETE.
- CONTRACTION JOINT CUTS SHALL BE 1/4 OF THE PAVEMENT THICKNESS DEEP. THE WIDTH OF THE CUT SHOULD BE APPROXIMATELY 1/8 INCH FOR UNSEALED JOINTS AND 1/4 INCH FOR SEALED JOINTS.
- FORMED CONTRACTION JOINTS SHALL BE FINISHED WITH A TOOL HAVING A 1/4" RADIUS.
- SCORED JOINTS SHALL BE 1/4" DEEP AND PLACED AT THE SPACING INDICATED FOR THE WIDTH OF SIDEWALK OR MATCH SCORED JOINTS OF ADJACENT CURB.
- CONCRETE SHALL BE FINISHED BY MEANS OF A FLOAT, STEEL TROWELLED AND BROOMED WITH A FINE BRUSH IN A TRANSVERSE DIRECTION.
- CONTRACTOR SHALL INSTALL CONTRACTION JOINTS AT THE END OF ONE PLACEMENT AND THE BEGINNING OF A SECOND PLACEMENT.
- CONTRACTOR SHALL CONTINUE CONTRACTION JOINTS THROUGH CURB AND GUTTER TO HELP ELIMINATE SYMPATHY CRACKS.



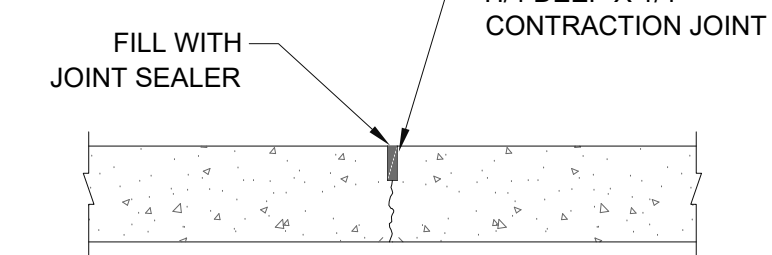
THICKENED EDGE



ISOLATION JOINT

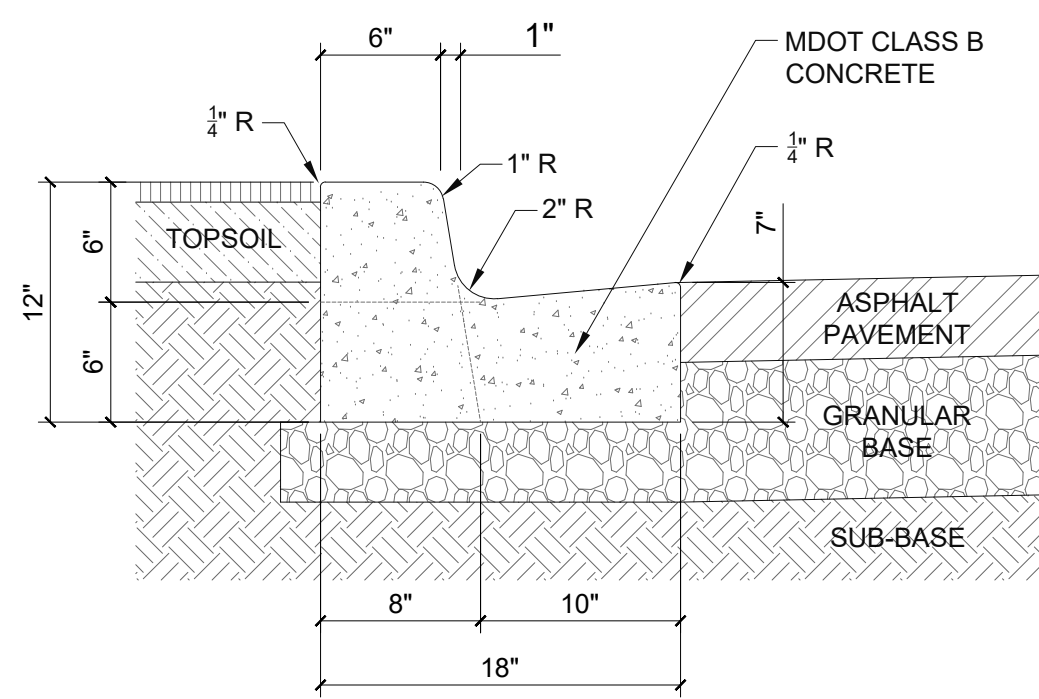


EXPANSION JOINT



CONTRACTION JOINT

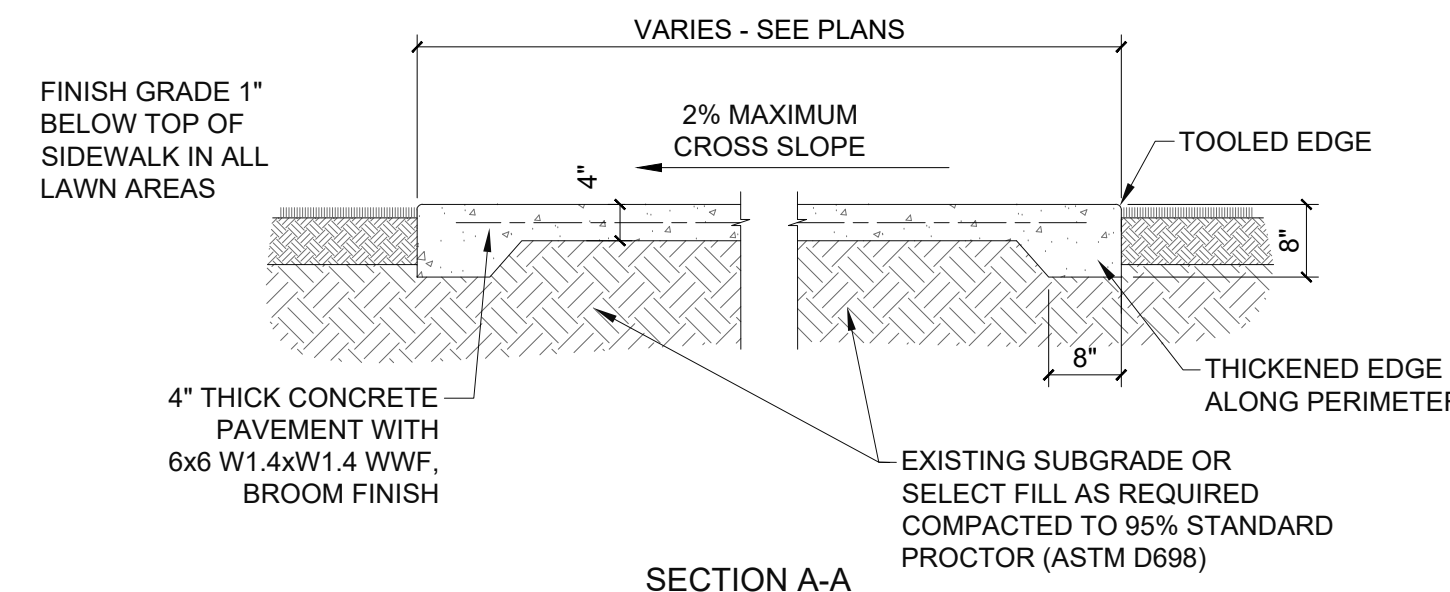
7 TYPICAL CONCRETE JOINT DETAILS
SCALE: NTS



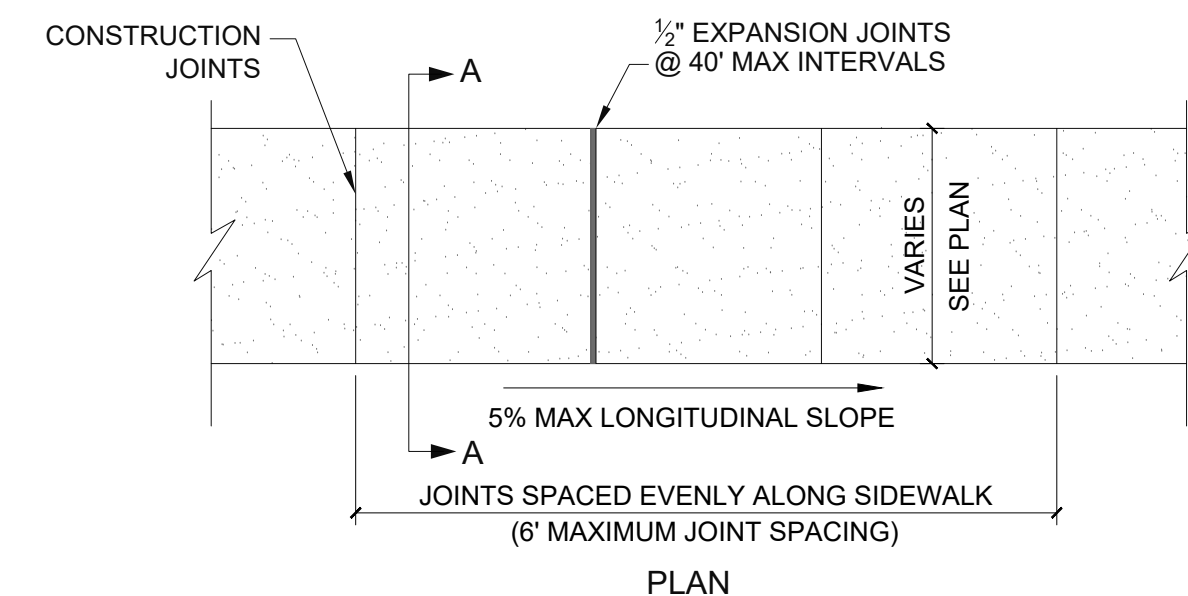
CURB SECTION

CURB & GUTTER NOTES

- CONSTRUCTION SHALL BE IN ACCORDANCE WITH LATEST ADDITION OF ACI 330R.
- EXPANSION JOINTS SHALL BE SPACED AT A MAXIMUM DISTANCE OF 40' APART AND AT ALL RADIUS POINTS, PT'S, AND PC'S.
- EXPANSION JOINTS SHALL BE LOCATED WHERE CURB ABUTS CONCRETE DRIVEWAYS, SIDEWALKS OR OTHER ADJACENT STRUCTURES.
- CONTRACTION JOINTS SHALL BE SPACED AT A MAXIMUM DISTANCE OF 10' APART.
- JOINTS SHALL BE PROVIDED WITHIN TWELVE (12) HOURS OF FINISHING CONCRETE.
- 1/2 INCH BITUMINOUS JOINT FILLER SHALL BE INSTALLED AT EXPANSION JOINT LOCATIONS AND SHALL EXTEND THE FULL DEPTH OF THE CONCRETE.
- CONTRACTION JOINT SHALL BE TOOLED AND BE 1/4 OF THE PAVEMENT THICKNESS DEEP. THE WIDTH OF THE TOOL SHOULD BE APPROXIMATELY 1/8 INCH FOR UNSEALED JOINTS AND 1/4 INCH FOR SEALED JOINTS.
- FORMED CONTRACTION JOINTS SHALL BE FINISHED WITH A TOOL HAVING A 1/4" RADIUS.

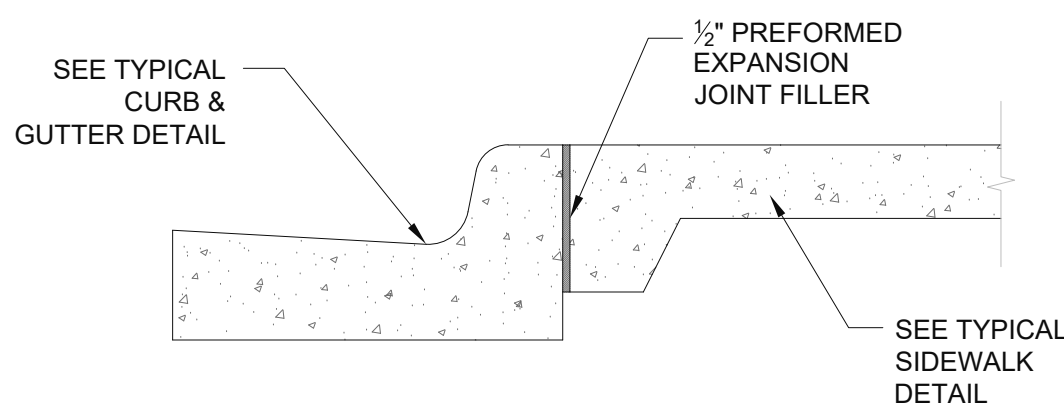


SECTION A-A

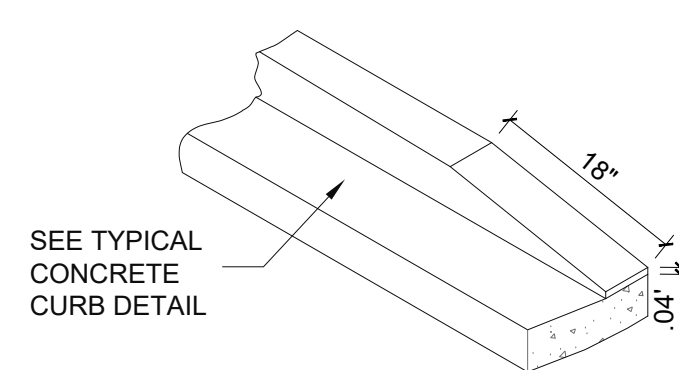


PLAN

6 TYPICAL 4" CONCRETE SIDEWALK DETAILS
SCALE: NTS



CURB AT SIDEWALK DETAIL

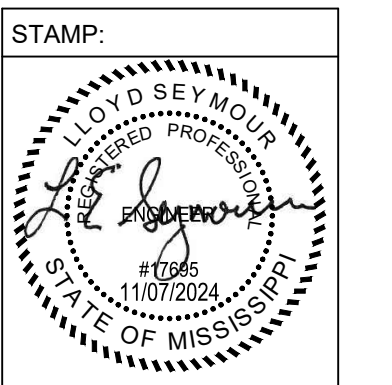


CURB TAPPER

5 TYPICAL CONCRETE CURB DETAIL
SCALE: NTS

SHEET REVISIONS:

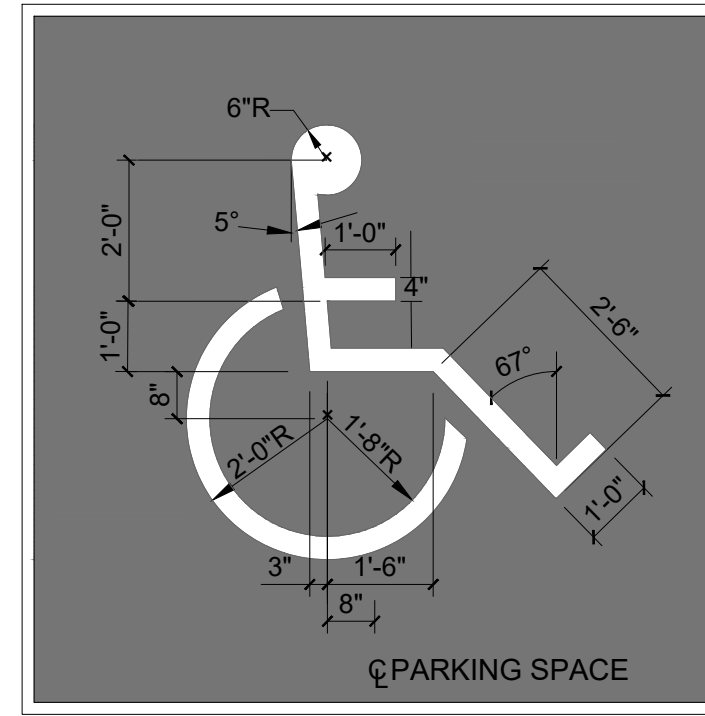
#	DATE/REFERENCE



SHEET TITLE:
SITE DETAILS

DATE: 11-7-2024
SHEET NUMBER: 6 OF 13

C250

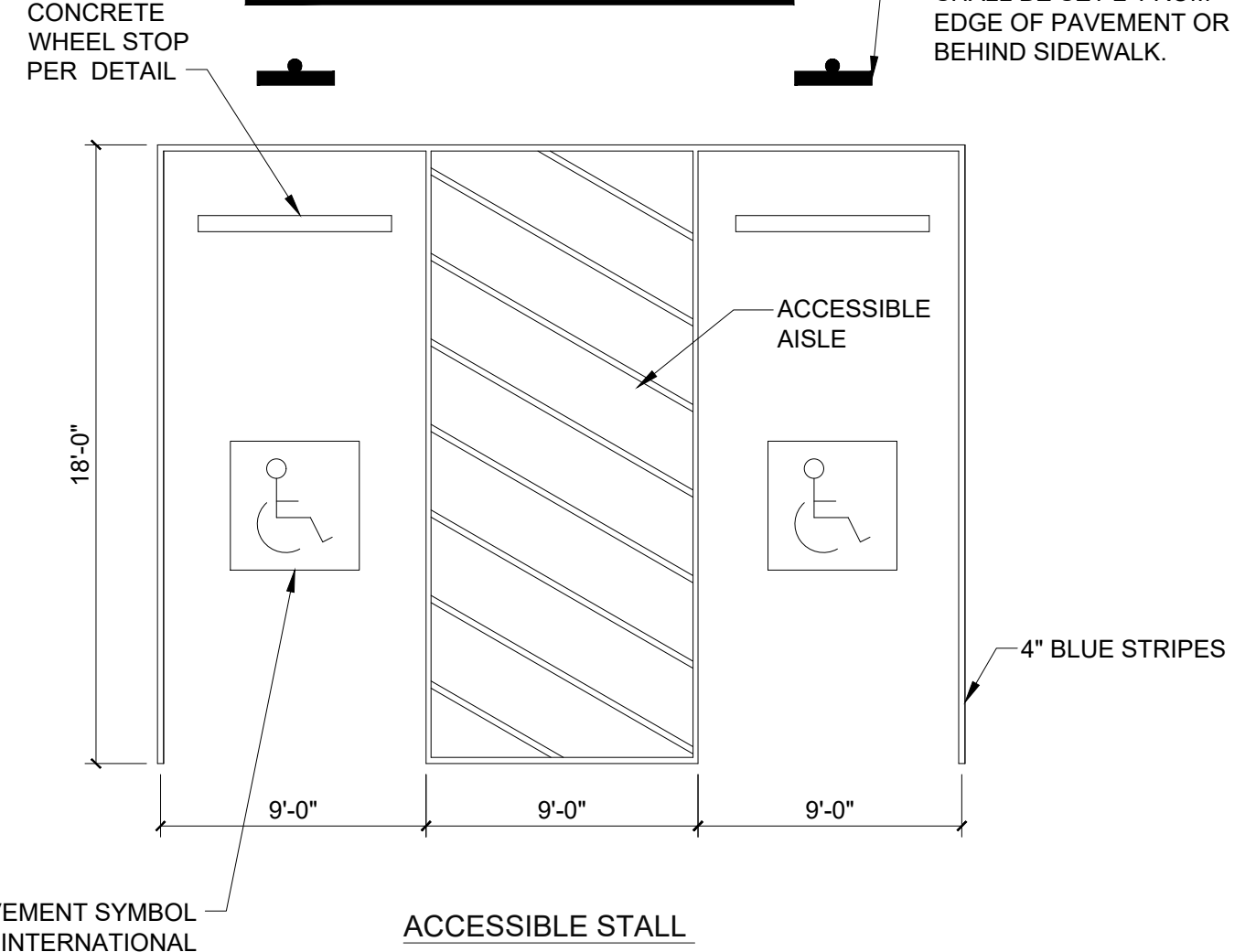


ACCESSIBLE SYMBOL

HANDICAP SYMBOL DETAIL NOTES

1. PAVEMENT SYMBOL SHALL BE PAINTED WHITE ON A BLUE BACKGROUND.
2. BLUE COLOR SHALL MATCH NO. 15090 IN THE FEDERAL STANDARD 595B AS SPECIFIED IN SECTION 522(B)2.

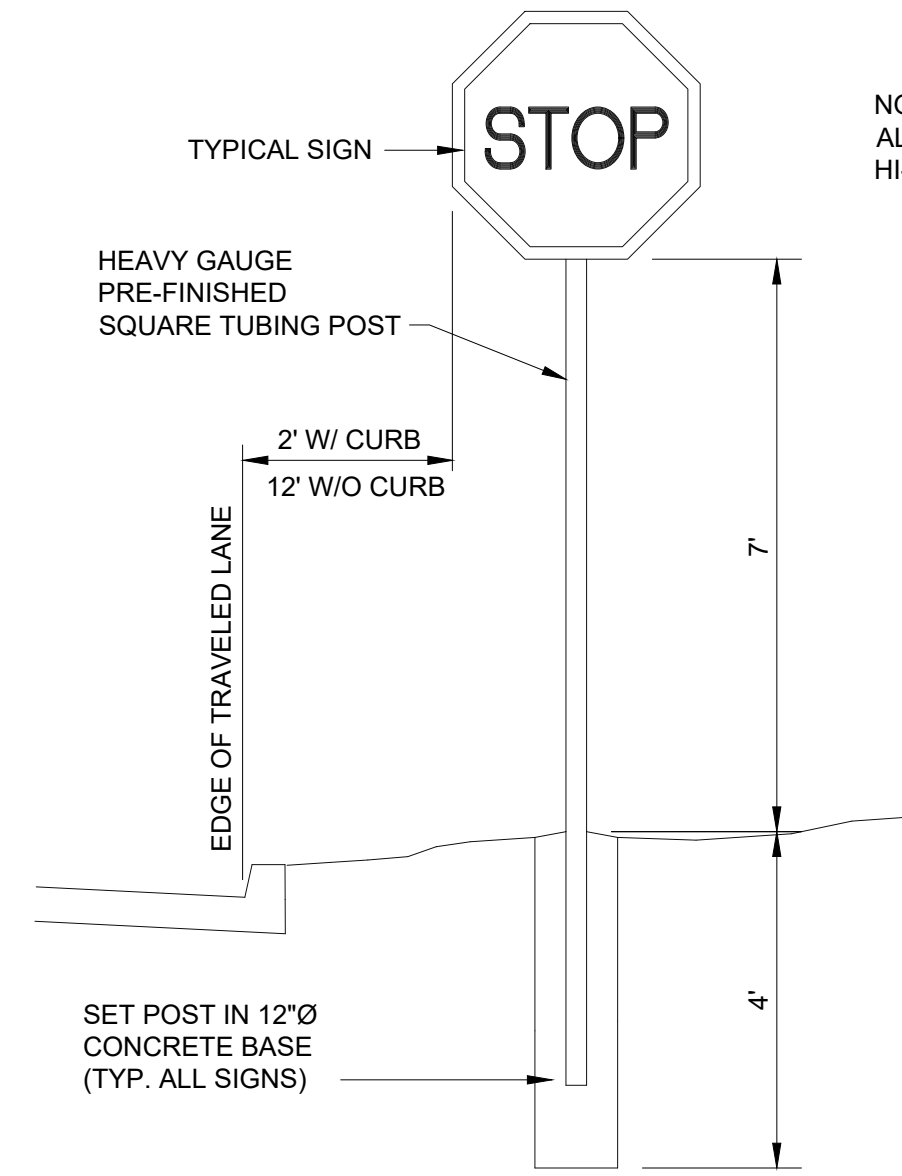
- ACCESSIBLE STALL NOTES**
1. ALL STRIPING CONTIGUOUS TO HANDICAPPED PARKING SPACES SHALL BE BLUE.
 2. ALL ACCESSIBLE STALL MARKINGS SHALL BE THERMOPLASTIC.
 3. ACCESSIBLE PARKING GRADES SHALL NOT EXCEED 1:50 IN ANY DIRECTION.



TYPICAL PAVEMENT SYMBOL PER INTERNATIONAL HANDICAP SYMBOL DETAIL

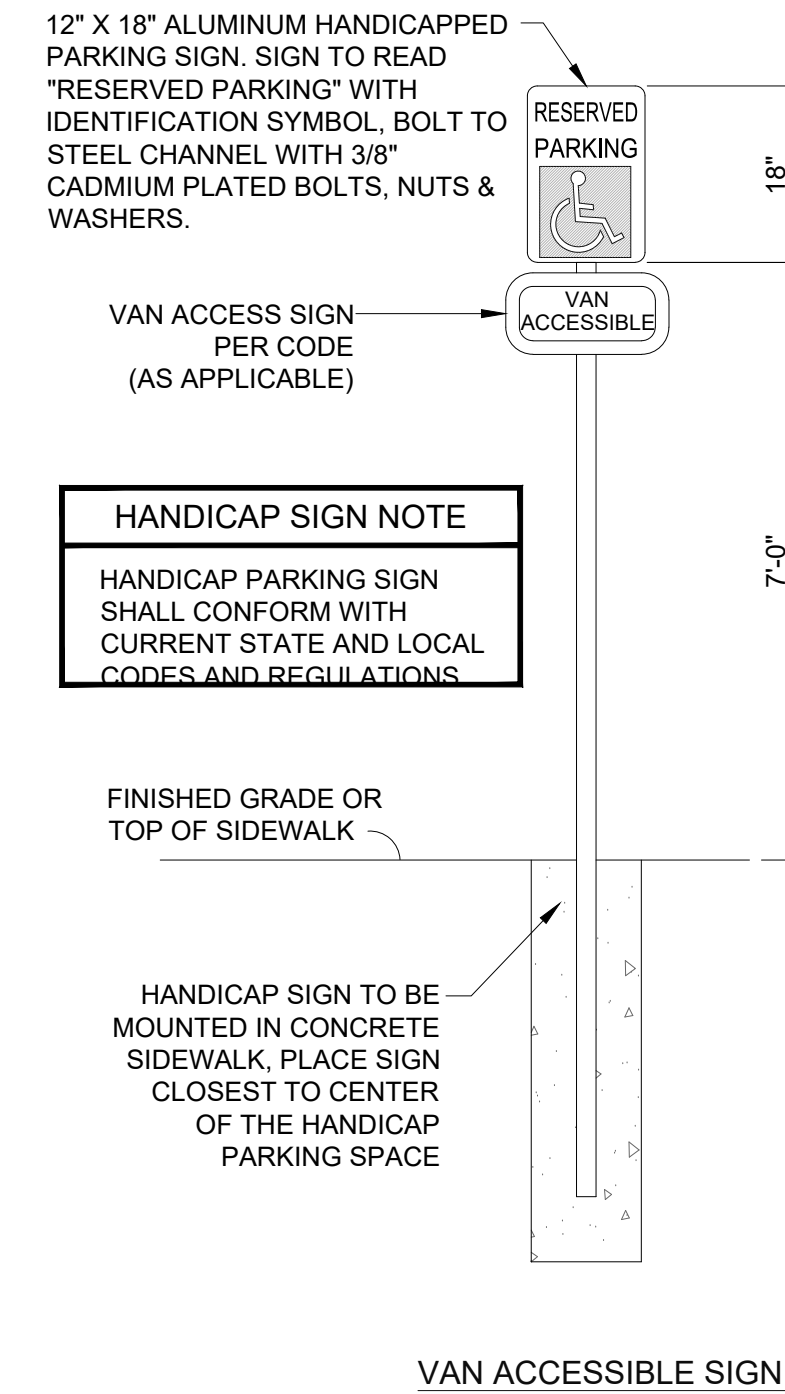
ACCESSIBLE STALL

HANDICAP PARKING SIGNS SHALL BE SET 2' FROM EDGE OF PAVEMENT OR BEHIND SIDEWALK.



NOTE: ALL SIGNS SHALL BE HI-INTENSITY SHEETING.

2 TYPICAL SIGN INSTALLATION DETAIL
C251 SCALE: NTS



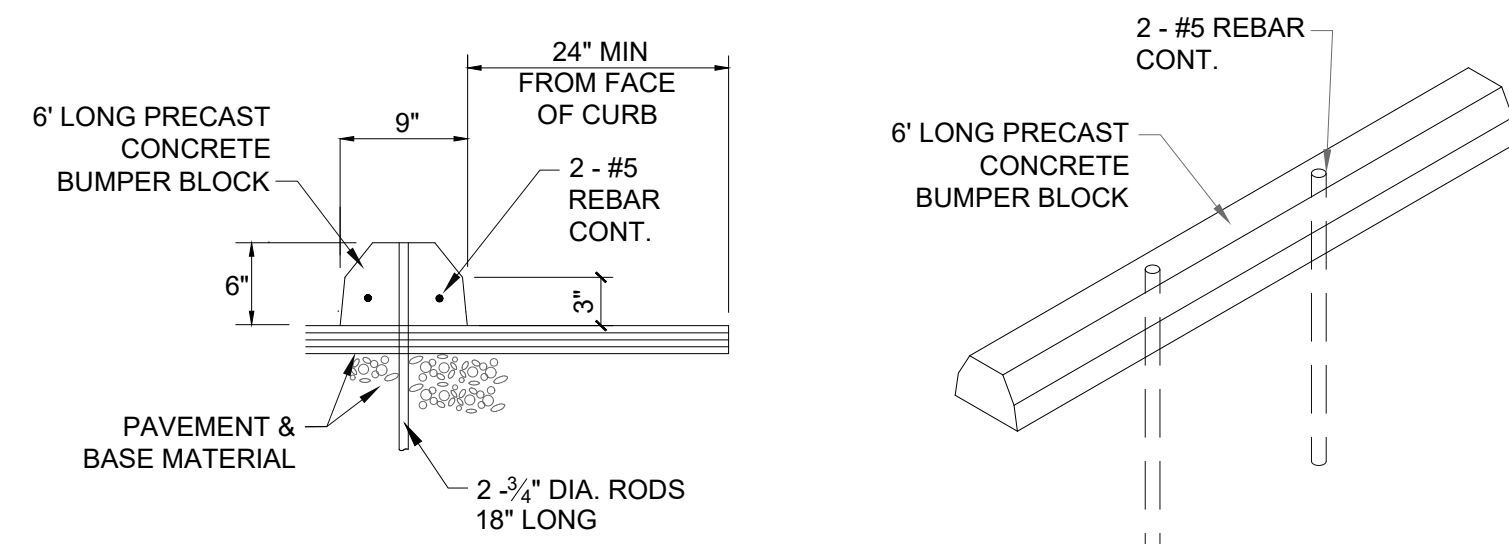
- HANDICAP SIGN NOTE**
- HANDICAP PARKING SIGN SHALL CONFORM WITH CURRENT STATE AND LOCAL CODES AND REGULATIONS.

FINISHED GRADE OR TOP OF SIDEWALK

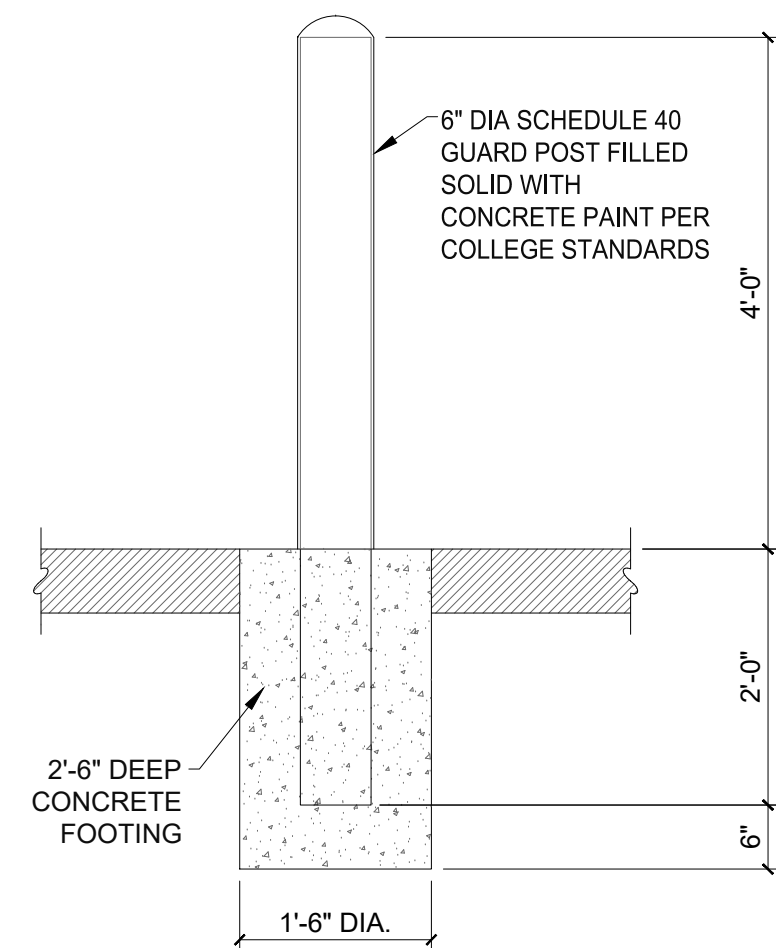
HANDICAP SIGN TO BE MOUNTED IN CONCRETE SIDEWALK, PLACE SIGN CLOSEST TO CENTER OF THE HANDICAP PARKING SPACE

3 TYPICAL ACCESSIBLE SIGN DETAIL
C251 SCALE: NTS

1 TYPICAL ACCESSIBLE STALL DETAIL
C251 SCALE: NTS



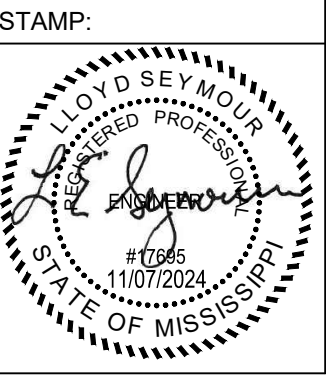
4 TYPICAL CONCRETE WHEEL STOP DETAIL
C251 SCALE: NTS



5 TYPICAL 6" BOLLARD DETAIL
C251 SCALE: NTS

SHEET REVISIONS:

#	DATE/REFERENCE

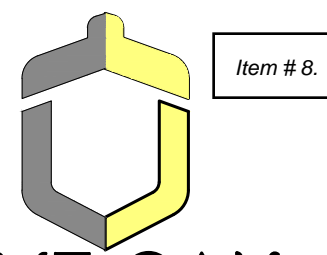


SHEET TITLE:

SITE DETAILS

DATE: 11-7-2024
SHEET NUMBER: 7 OF 13

C251

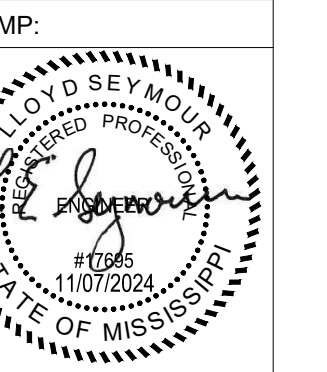


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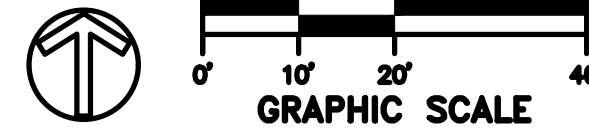
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#	DATE/REFERENCE



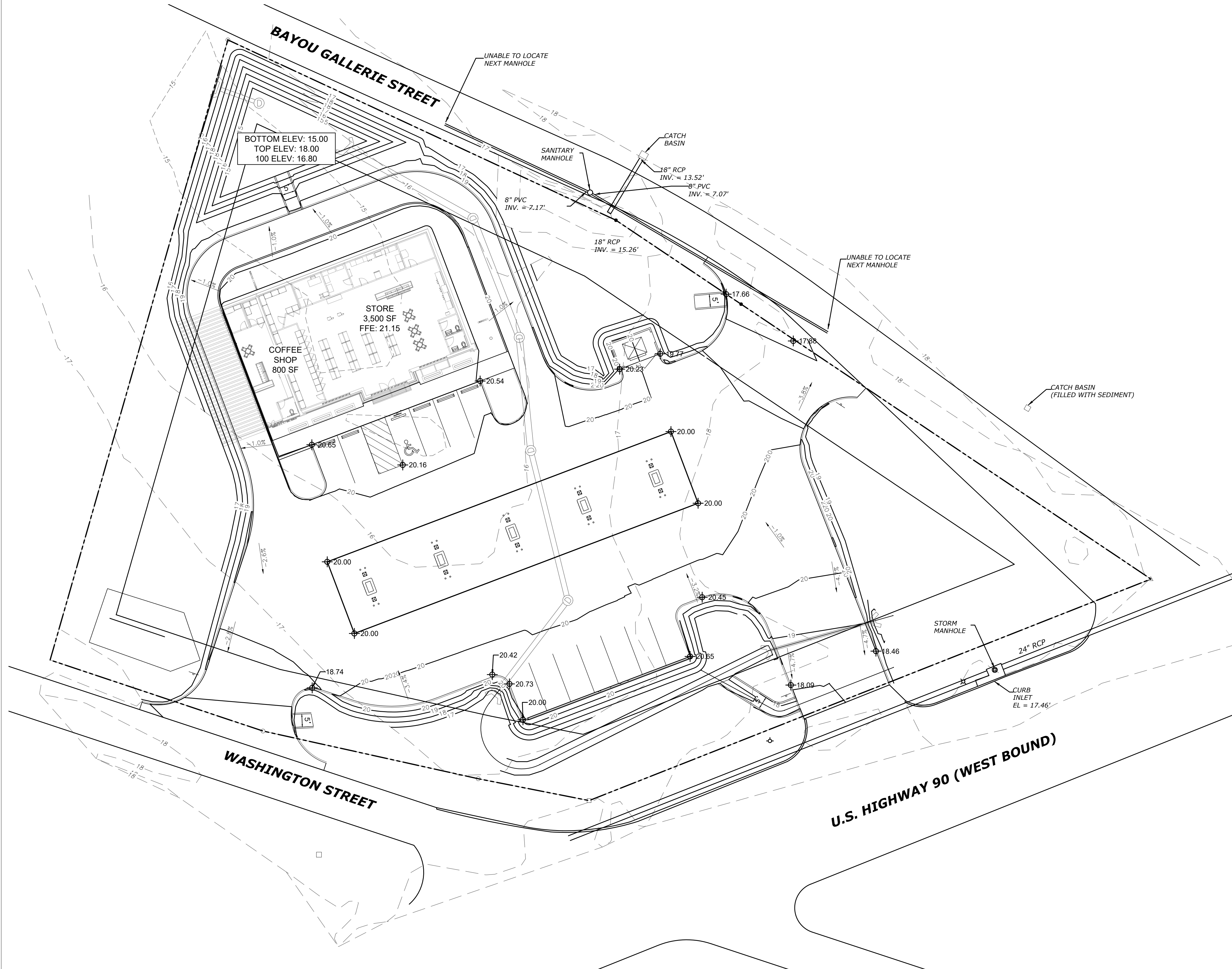
SHEET TITLE:
SITE GRADING AND DRAINAGE PLAN

DATE: 11-7-2024
SHEET NUMBER: 8 OF 13

C300



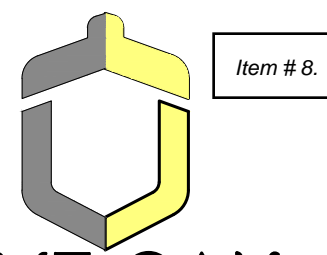
ELEV =



- ### SITE GRADING AND DRAINAGE NOTES
- CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING HORIZONTAL AND VERTICAL LOCATIONS OF ALL EXISTING TIE-IN POINTS, STRUCTURES, PIPES, ETC., PRIOR TO CONSTRUCTION.
 - NO SIDEWALK CROSS SLOPE SHALL EXCEED 2%. CONTRACTOR SHALL NOTIFY ENGINEER PRIOR TO CONSTRUCTION IF CONDITIONS CANNOT BE MET ON SITE.
 - NO ACCESSIBLE PARKING STALLS OR ADJACENT ACCESS AISLES SHALL EXCEED 2% SLOPE IN ANY DIRECTIONS. CONTRACTOR SHALL NOTIFY ENGINEER PRIOR TO CONSTRUCTION IF CONDITIONS CANNOT BE MET ON SITE.
 - NO SIDEWALK CROSS SLOPE SHALL EXCEED 2% AND NO SIDEWALK LONGITUDINAL SLOPE SHALL EXCEED 5%. CONTRACTOR SHALL NOTIFY ENGINEER PRIOR TO CONSTRUCTION IF CONDITIONS CANNOT BE MET ON SITE.
 - FINISH SURFACES TO BE SMOOTH AND EVEN WITH NO ABRUPT OR AWKWARD CHANGES IN GRADE. IF SPECIFIC GRADES AND SLOPES ARE NOT SHOWN FOR WORK IN ANY AREA, THE CONTRACTOR SHALL ENSURE POSITIVE DRAINAGE IS ACHIEVED AWAY FROM BUILDINGS AND STRUCTURES AND TIE INTO EXISTING CONDITIONS.
 - DRAIN BASIN AND INLINE DRAIN TOP ELEVATIONS SHALL BE ADJUSTED AS REQUIRED TO ENSURE THAT POSITIVE DRAINAGE IS MAINTAINED TOWARD THE DRAIN STRUCTURES.
 - ALL ROOF AND CANOPY DRAINAGE SHALL BE CONNECTED TO THE SUBSURFACE DRAINAGE SYSTEM. CONTRACTOR SHALL CONSULT AND VERIFY ROOF AND CANOPY DRAINAGE SIZE AND LOCATION WITH ARCHITECTURAL/PLUMBING DRAWINGS.

- ### LEGEND
- 10--- DENOTES EXISTING GRADE CONTOUR
 - 10— DENOTES PROPOSED GRADE CONTOUR
 - 8.00 DENOTES PROPOSED SPOT ELEVATION
 - ↘ ↙ ↖ DENOTES WATERSHED FLOW DIRECTION
 - DS - DRAIN STRUCTURE
 - RCP - REINFORCED CONCRETE PIPE
 - RCAP - REINFORCED CONCRETE ARCH PIPE
 - HP - HIGH PERFORMANCE POLYPROPYLENE STORM PIPE

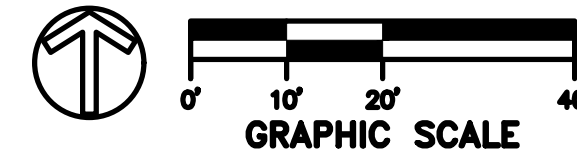
1 GRADING PLAN
C300 SCALE: 1" = 20'



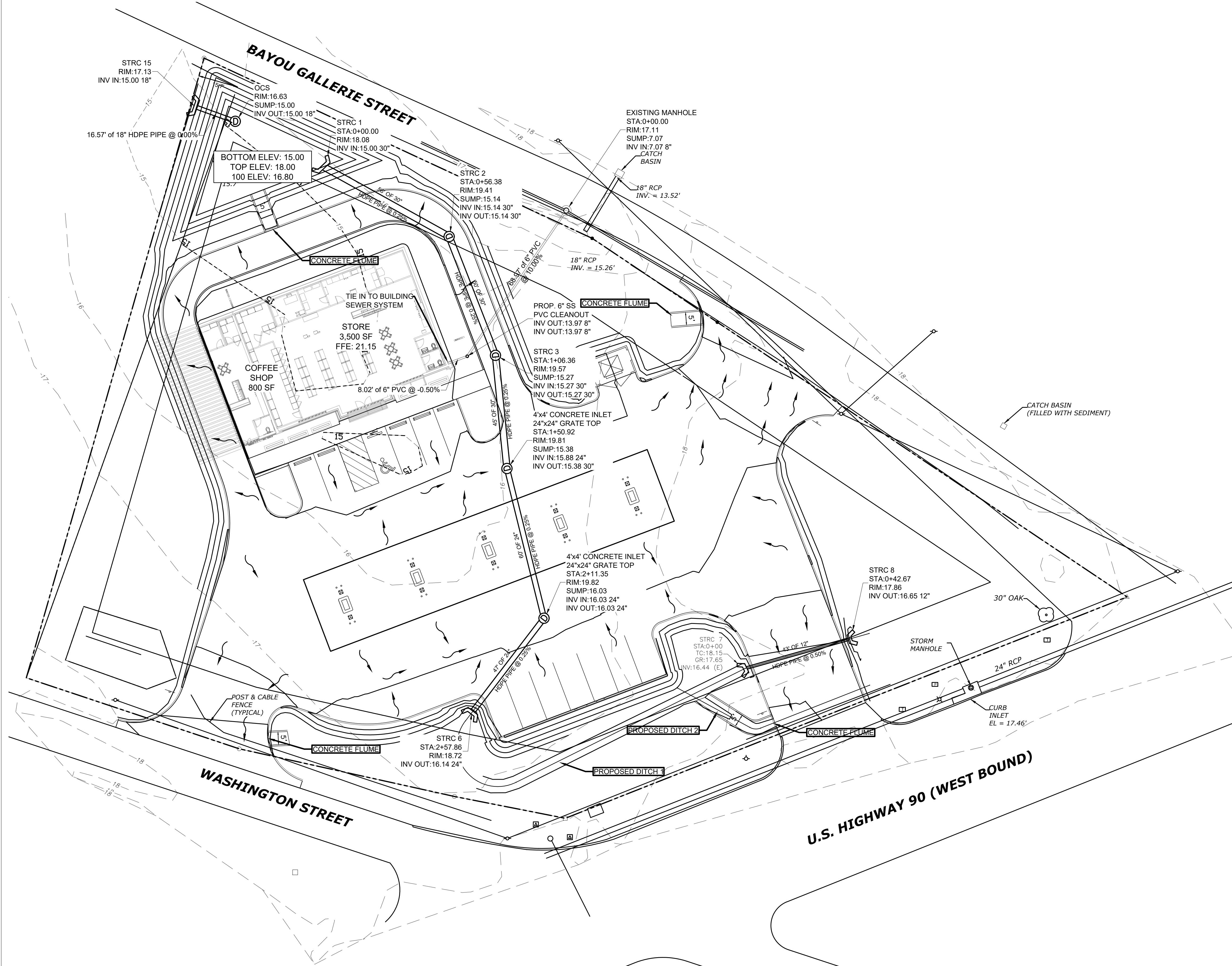
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ELEV =



DRAINAGE FLOW SUMMARY		
DRAINAGE AREA	Pre Peak Run-Off	Post Peak Run-Off
Total Area (Pre)	6.350	
South Area (Post)*		1.680
North Area (Post)		4.560
TOTAL	6.350	6.240

* ROUTED THROUGH POND

- ### STORM WATER MANAGEMENT NOTES
- DRAINAGE SYSTEM SHOWN WILL BE PRIVATELY OWNED AND MAINTAINED.
 - IN THE EVENT OF CONFLICT BETWEEN CITY OF OXFORD STORM WATER REQUIREMENTS AND THE PLANS, THE CITY OF OXFORD LATEST STORM WATER MANAGEMENT ORDINANCE SHALL GOVERN.
 - TRASH RACKS ARE REQUIRED TO PROTECT DISCHARGE STRUCTURE ORIFICES AND CONTAIN GARBAGE FROM EXISTING THE PROPERTY.
 - ALL FLUMES AND OUTLET STRUCTURES ARE REQUIRED TO BE PROTECTED BY RIP-RAP.

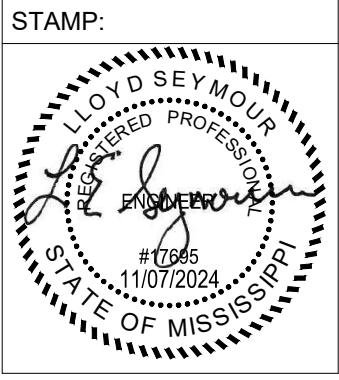
LEGEND

	DENOTES EXISTING GRADE CONTOUR
	DENOTES PROPOSED GRADE CONTOUR
	DENOTES PROPOSED SPOT ELEVATION
	DENOTES WATERSHED FLOW DIRECTION

DS - DRAIN STRUCTURE
RCP - REINFORCED CONCRETE PIPE
RCAP - REINFORCED CONCRETE ARCH PIPE
HP - HIGH PERFORMANCE POLYPROPYLENE STORM PIPE

SHEET REVISIONS:

#	DATE/REFERENCE



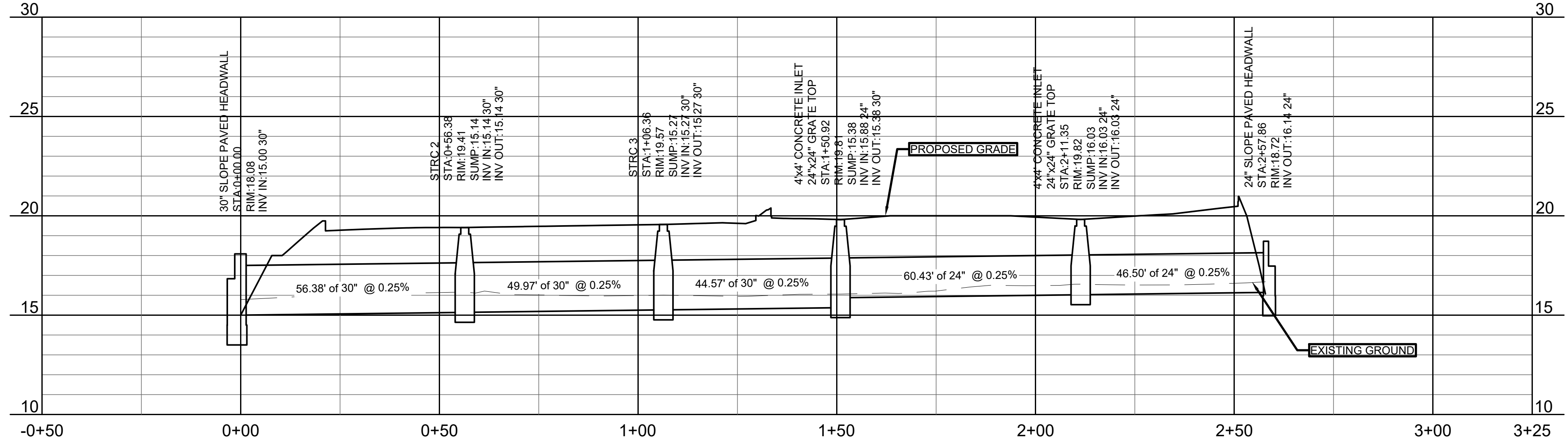
SHEET TITLE:
DRAINAGE PLAN

DATE: 11-7-2024
SHEET NUMBER: 9 OF 13

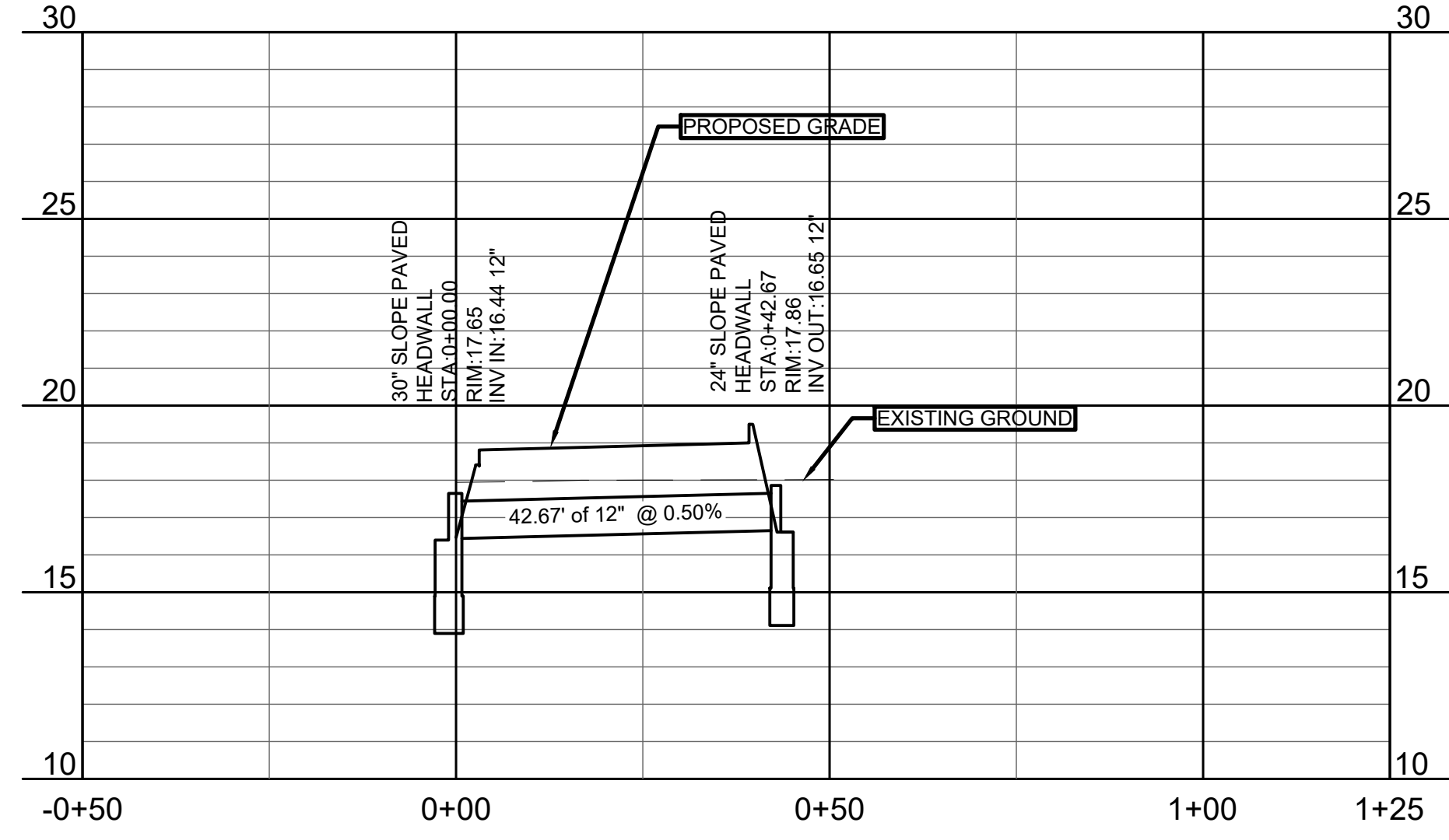
C310

1 DRAINAGE PLAN
SCALE: 1" = 20'

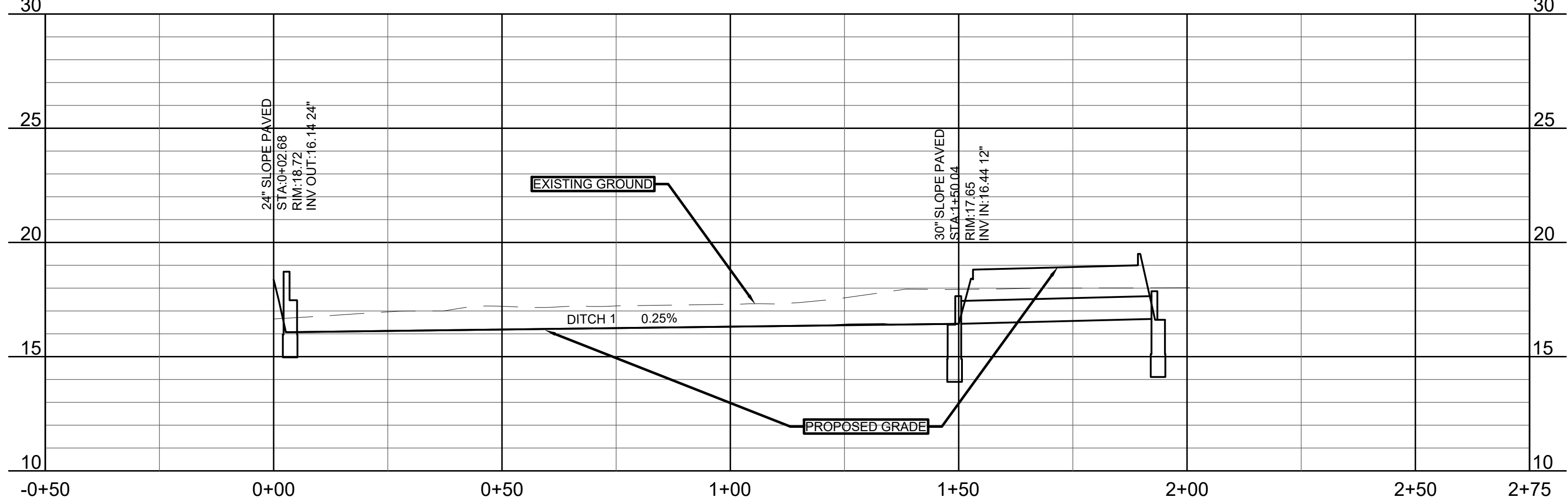
STORM LINE 1 PROFILE



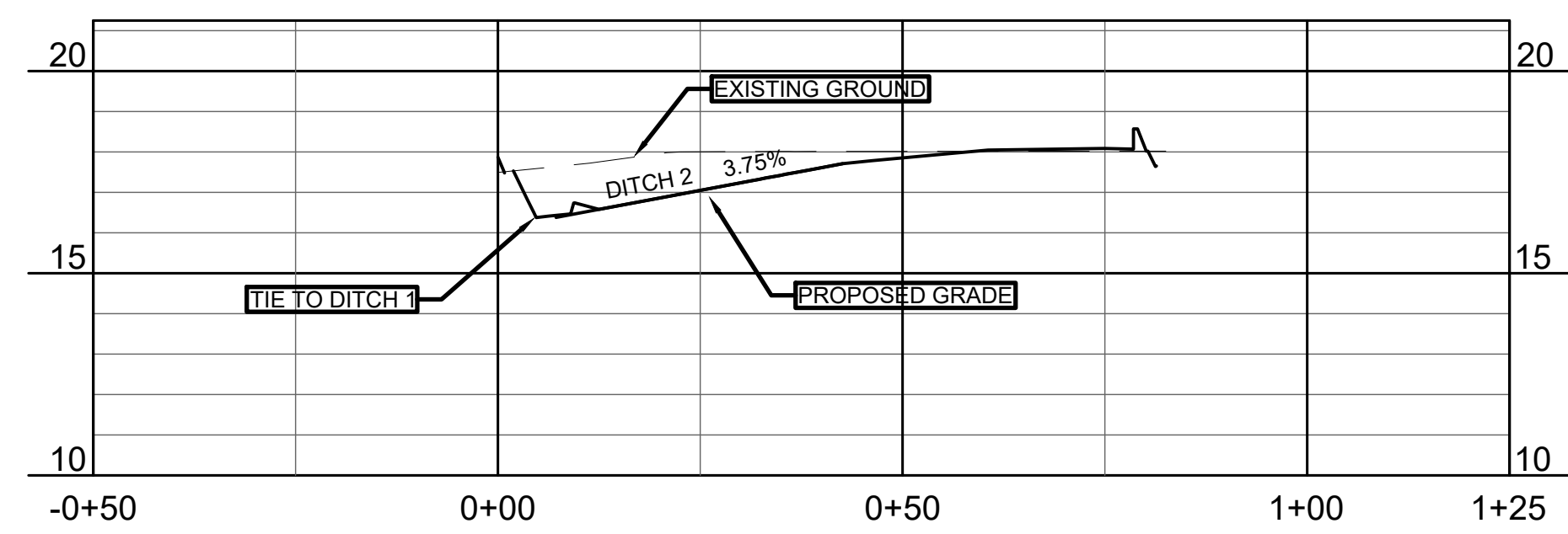
STORM LINE 2 PROFILE



DITCH 1 PROFILE



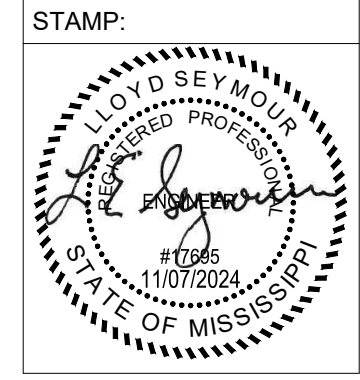
DITCH 2 PROFILE



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#	DATE/REFERENCE



SHEET TITLE:

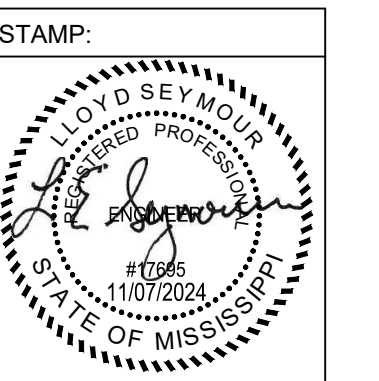
DRAINAGE PROFILES

DATE: 11-7-2024
SHEET NUMBER: 10 OF 13

C320

SHEET REVISIONS:

#	DATE/REFERENCE

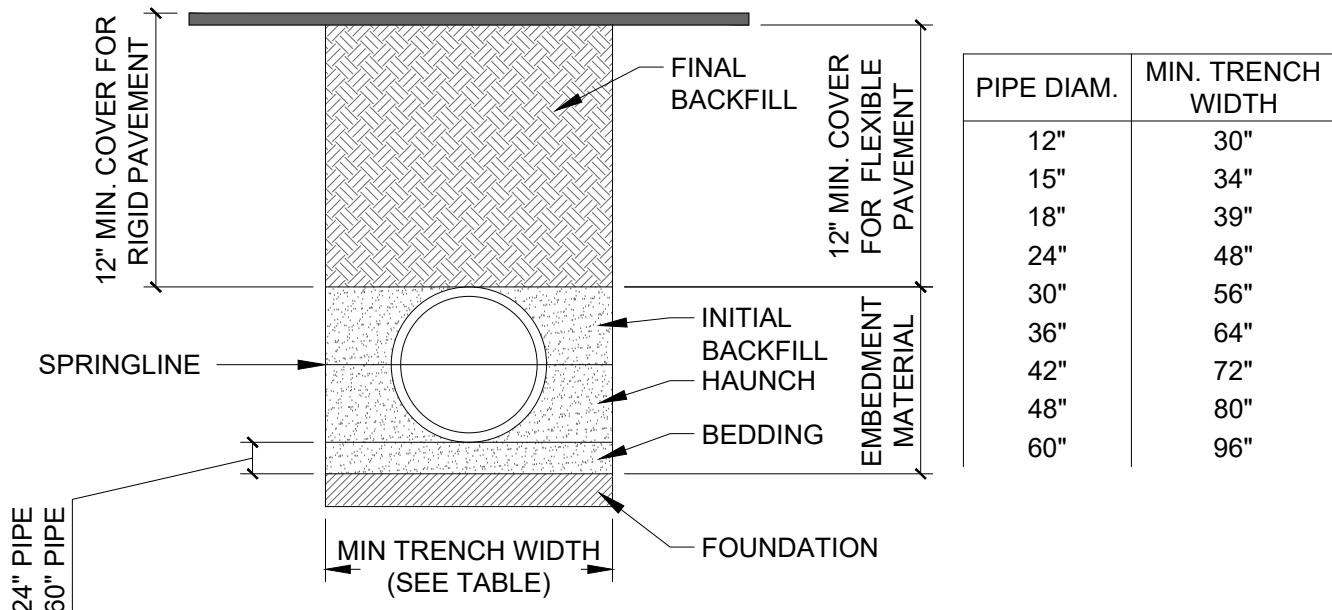


SHEET TITLE:

DRAINAGE DETAILS

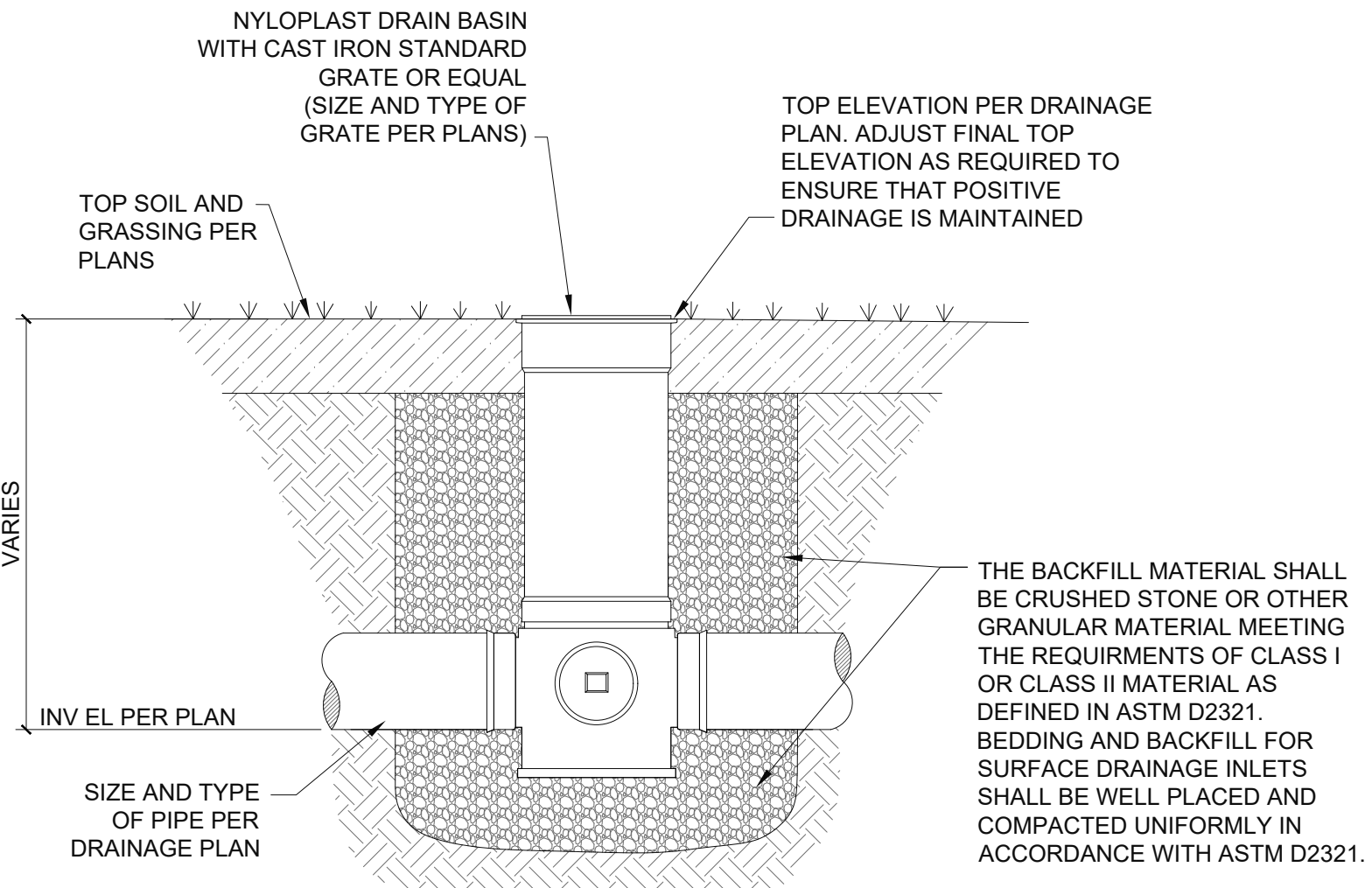
DATE: 11-7-2024
SHEET NUMBER: 11 OF 13

C350

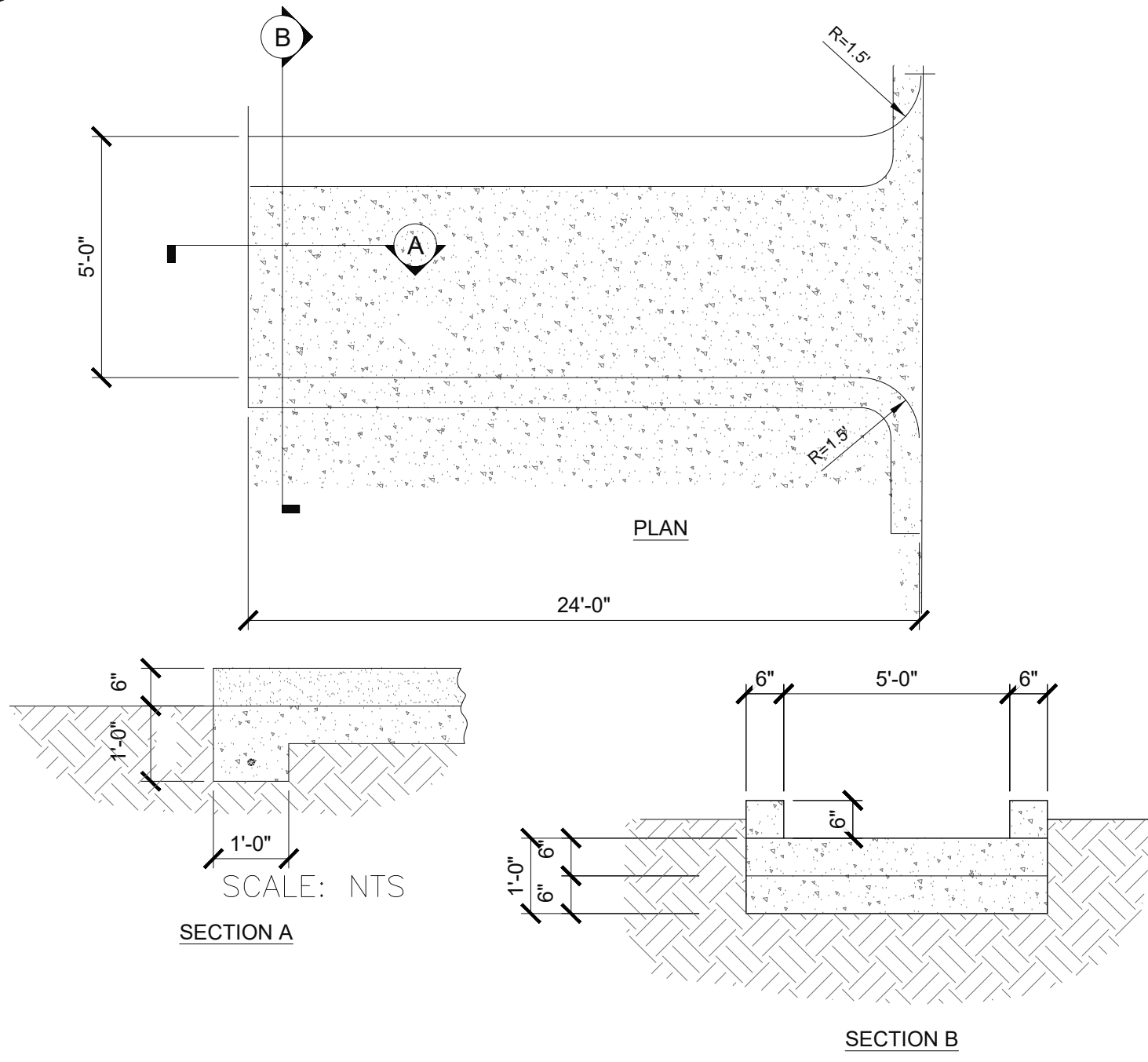


- HP STORM PIPE INSTALLATION NOTES**
- ALL HP STORM PIPE SYSTEMS SHALL BE INSTALLED IN ACCORDANCE WITH LATEST VERSION OF ASTM D2321, "STANDARD PRACTICE FOR UNDERGROUND INSTALLATION OF THERMOPLASTIC PIPE FOR SEWERS AND OTHER GRAVITY FLOW APPLICATIONS".
 - SUITABLE EMBEDMENT MATERIALS, EITHER ON-SITE OR IMPORTED, SHALL MEET THE REQUIREMENTS FOR CLASS I, II, OR III PER THE LATEST VERSION OF ASTM D2321. THE CONTRACTOR SHALL PROVIDE DOCUMENTATION TO THE ENGINEER FOR EMBEDMENT MATERIAL TO BE USED FOR PIPE INSTALLATION. SEE THE EMBEDMENT MATERIAL TABLE FOR COMPACTION AND LIFT PLACEMENT REQUIREMENTS.
 - FOUNDATION: WHERE THE TRENCH BOTTOM IS UNSTABLE, THE CONTRACTOR SHALL EXCAVATE TO A DEPTH REQUIRED BY THE ENGINEER AND REPLACE WITH SUITABLE MATERIAL AS SPECIFIED BY THE ENGINEER.
 - BEDDING: BEDDING IS REQUIRED TO ESTABLISH LINE AND GRADE AND TO PROVIDE FIRM PIPE SUPPORT. MINIMUM BEDDING THICKNESS SHALL BE 4" FOR UP TO 24" DIAMETER PIPE AND 6" FOR 30"-60" DIAMETER PIPE. THE MIDDLE 1/3 BENEATH THE PIPE INVERT SHALL BE LOOSELY PLACED, WHILE THE REMAINDER SHALL BE THOROUGHLY COMPACTED.
 - HAUNCHING: THE HAUNCHING MATERIAL SHALL BE INSTALLED UNIFORMLY IN LIFTS ON EACH SIDE OF THE PIPE AND SHOVELED UNDER THE PIPE ENSURING TO FILL VOIDS. THE MATERIAL SHALL BE THOROUGHLY COMPACTED TO THE SPRING LINE OF THE PIPE EXTENDING TO THE SIDE WALLS OF THE TRENCH ENSURING THAT THE PIPE ALIGNMENT IS NOT DISTURBED.
 - INITIAL BACKFILL: THE INITIAL BACKFILL SHALL PROCEED TO THE TOP OF THE PIPE. THE MATERIAL SHALL BE THOROUGHLY COMPACTED INSTALLED IN UNIFORMED LIFTS ON EACH SIDE OF THE PIPE EXTENDING TO THE SIDE WALLS OF THE TRENCH.
 - FINAL BACKFILL (NON-TRAFFIC): SUITABLE MATERIAL IN NON-TRAFFIC APPLICATIONS (GRASS OR LANDSCAPE AREAS) SHALL BE GENERAL FILL MATERIAL. BACKFILL SHALL PROCEED TO FINISHED GRADE IN 12 INCH LIFTS COMPACTED TO ELIMINATE AIR VOIDS.
 - FINAL BACKFILL (TRAFFIC): SUITABLE MATERIAL IN TRAFFIC APPLICATIONS SHALL BE SELECT FILL COMPACTED IN 8 INCH LOOSE LIFTS TO NOT LESS THAN 95 PERCENT STANDARD PROCTOR.
 - MINIMUM COVER (NON-TRAFFIC): FOR NON-TRAFFIC APPLICATIONS (GRASS OR LANDSCAPE AREAS), MINIMUM COVER IS 12" FROM THE TOP OF PIPE TO GROUND SURFACE.
 - MINIMUM COVER (TRAFFIC): FOR TRAFFIC APPLICATIONS THE MINIMUM COVER IS 12" FOR UP TO 48" DIAMETER PIPE AND 24" OF COVER FOR 60" DIAMETER PIPE, MEASURED FROM TOP OF PIPE TO BOTTOM OF FLEXIBLE PAVEMENT OR TO TOP OF RIGID PAVEMENT.
 - CONTRACTOR SHALL MAINTAIN TRENCH BACKFILL AT GROUND SURFACE UNTIL FINAL ACCEPTANCE OF THE WORK. ALL SURPLUS MATERIALS NOT USED IN BACKFILLING SHALL BE REMOVED AND DISPOSED OF BY CONTRACTOR AT HIS OWN EXPENSE.

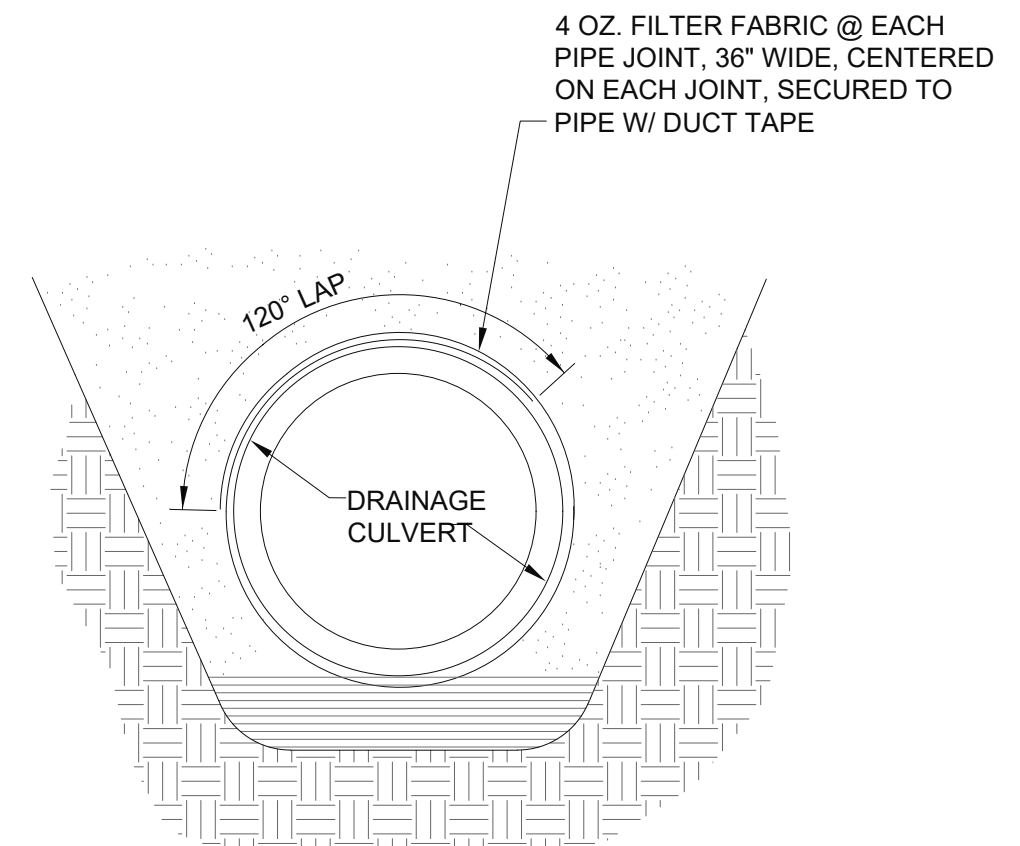
5 HP STORM PIPE TRENCH INSTALLATION DETAIL
SCALE: NTS



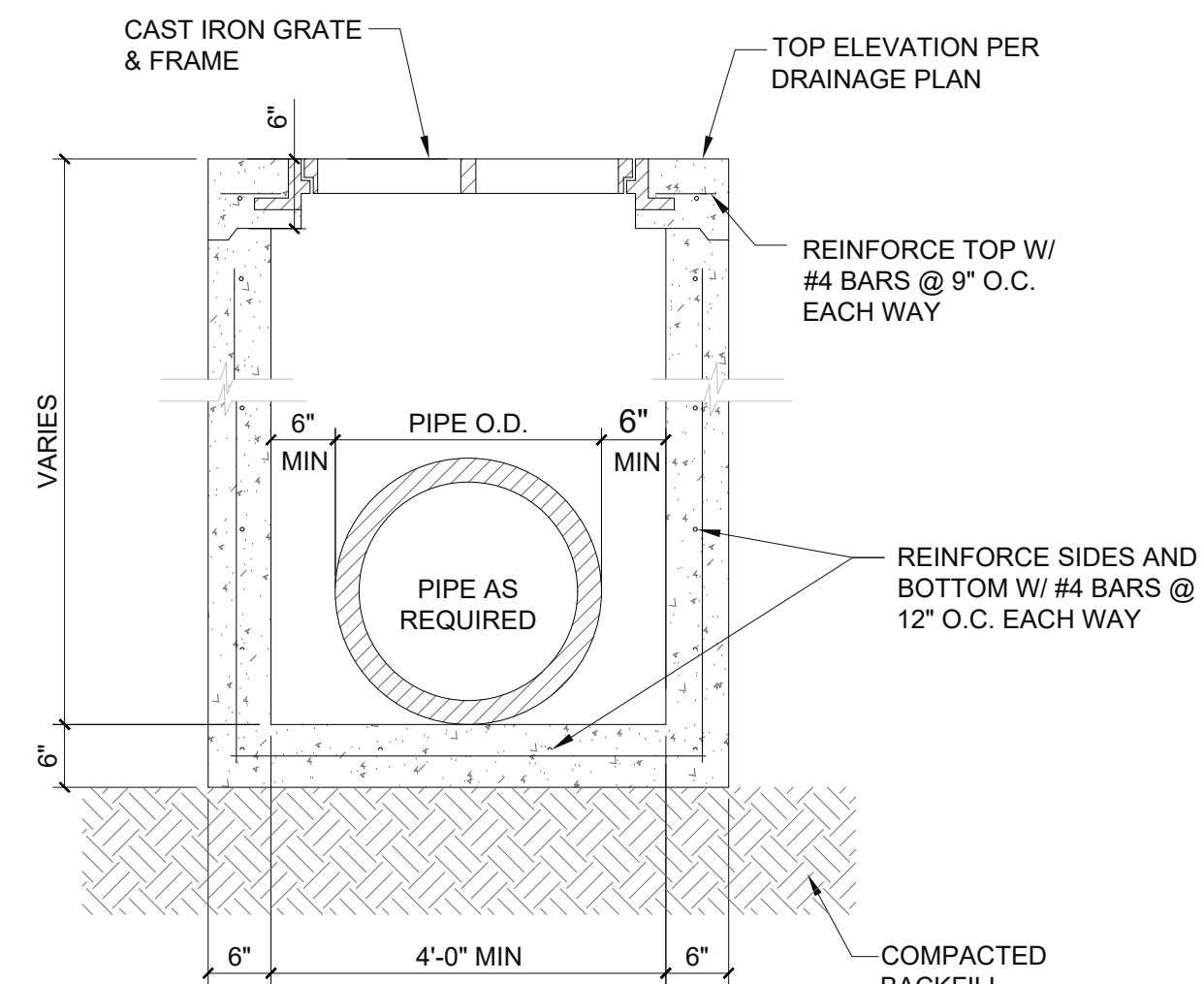
2 TYPICAL DRAIN BASIN DETAIL
SCALE: NTS



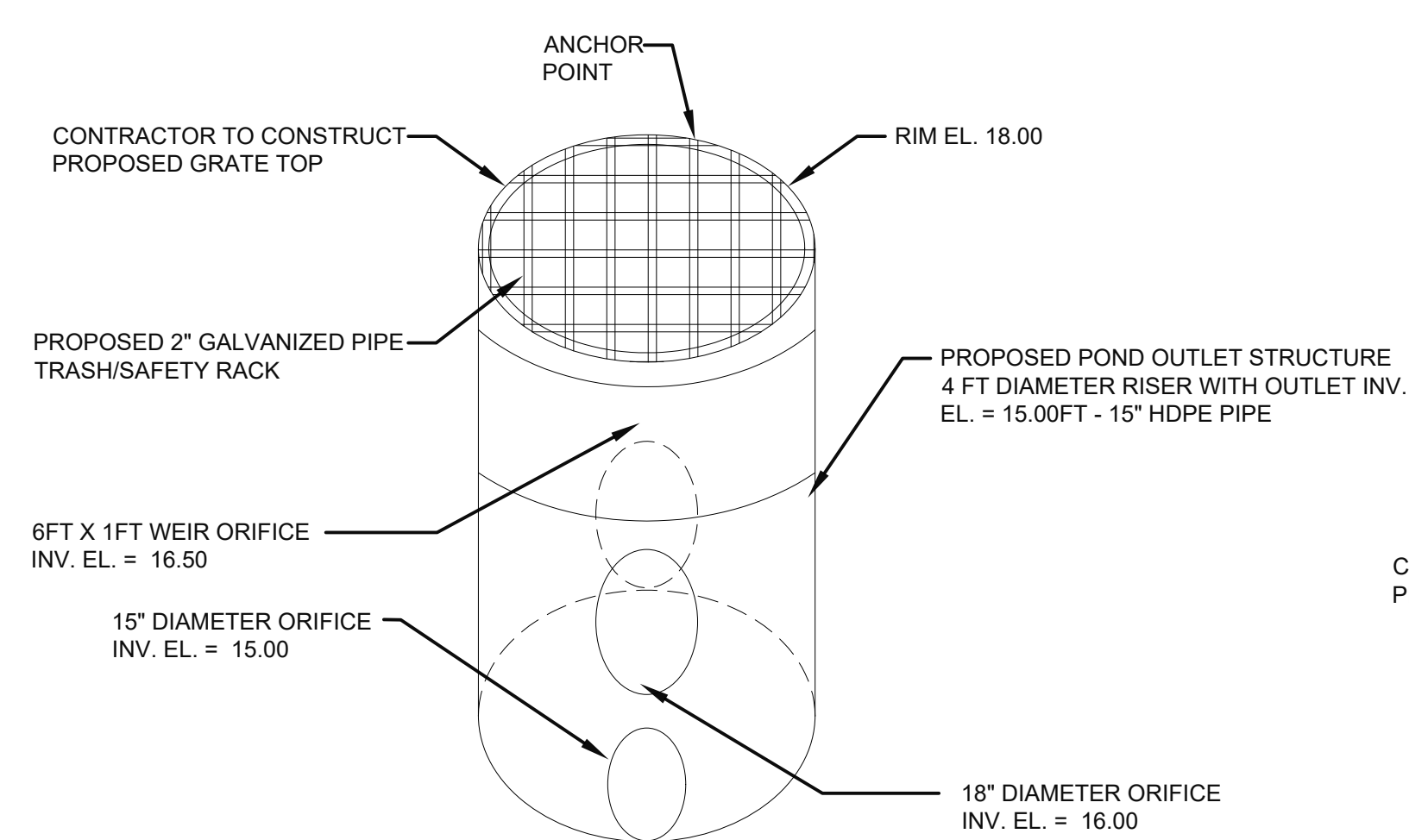
4 TYPICAL CONCRETE FLUME DETAIL
SCALE: NTS



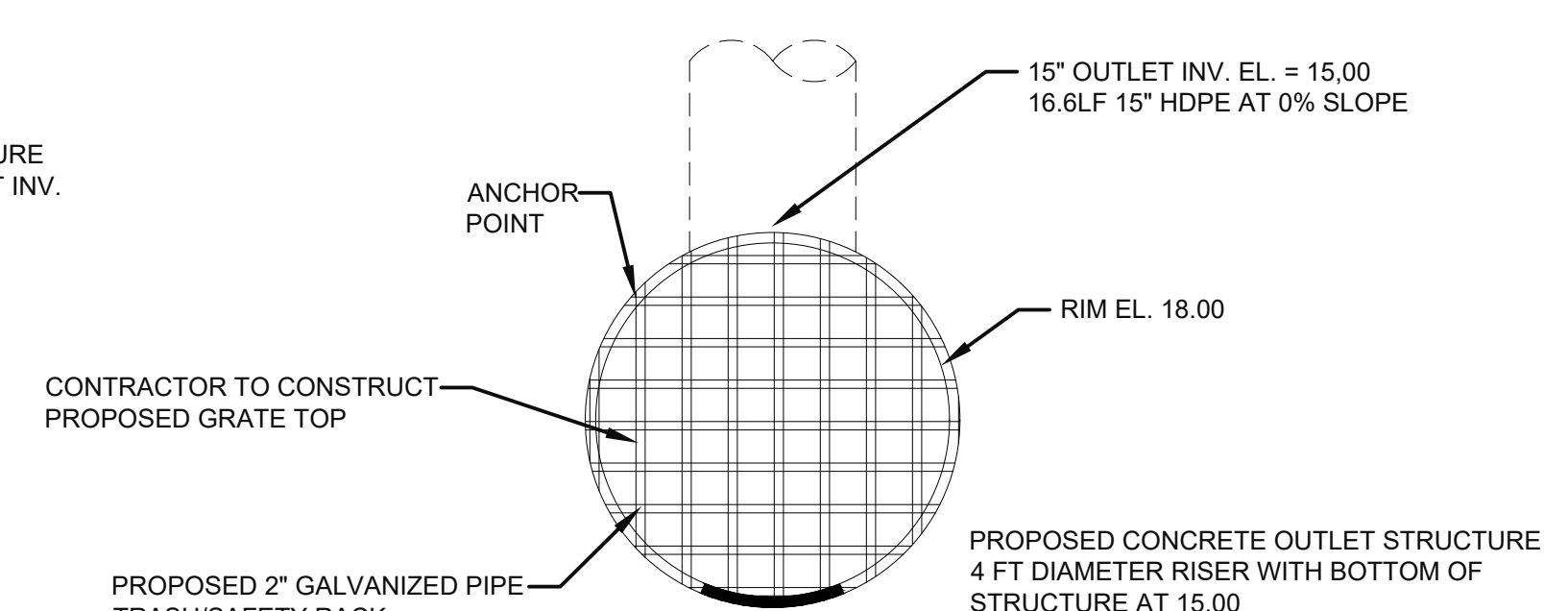
1 TYPICAL STORM DRAIN PIPE WRAP DETAIL
SCALE: NTS



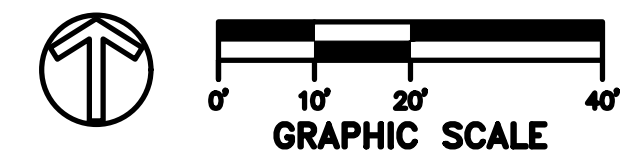
3 TYPICAL DRAIN INLET DETAIL
SCALE: NTS



6 BASIN OUTLET DETAIL - ISOMETRIC VIEW
SCALE: NTS

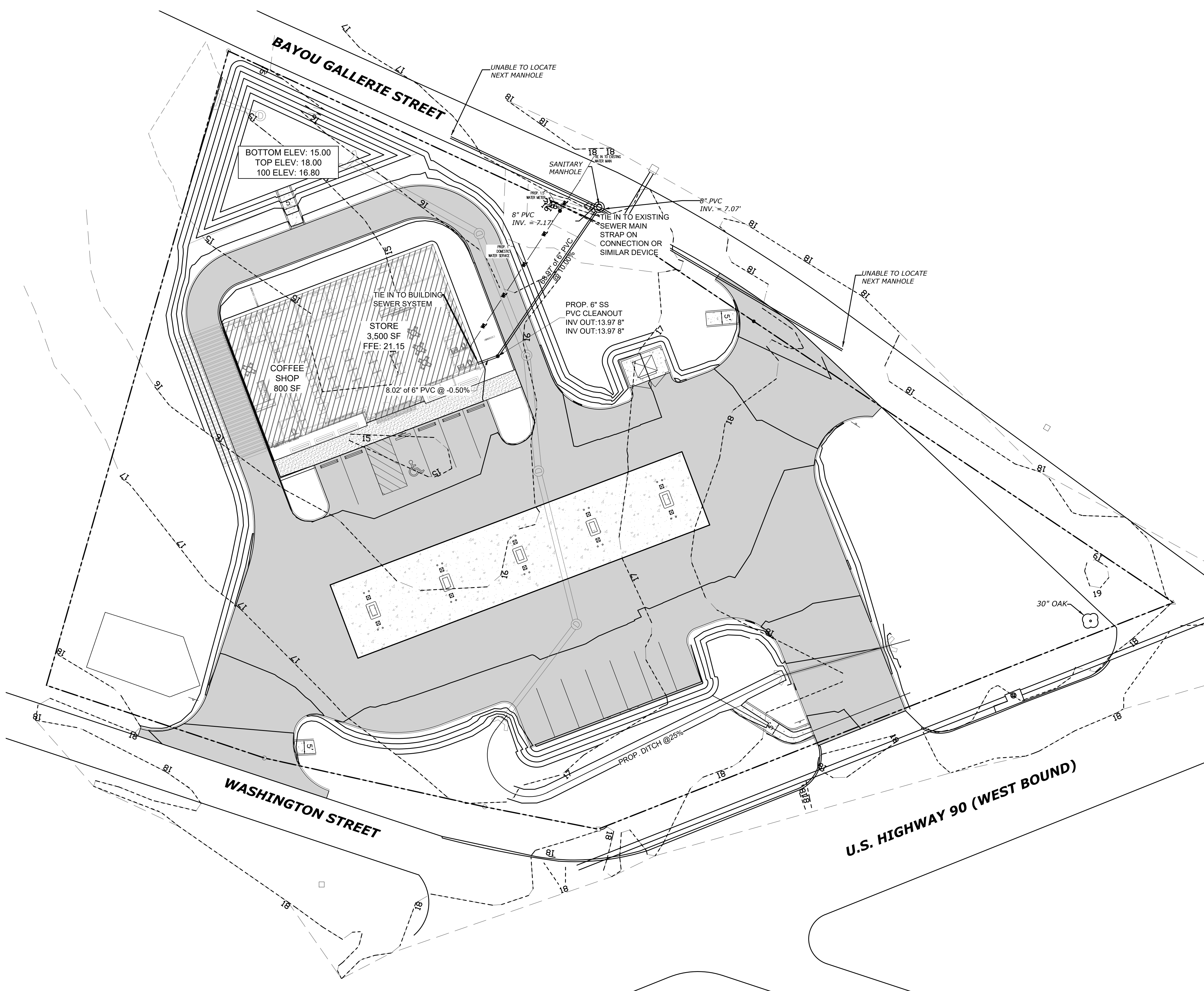


4 TYPICAL CONCRETE FLUME DETAIL
SCALE: NTS



ELEV =

LIVE OAK ENGINEERING
 2509 7TH AVE S.
 BIRMINGHAM, AL 35233
 955B HOWARD AVENUE
 BILOXI, MS 39530
 LIVEOAKENGINEERING.COM
 LOE JOB# 276-1



UNDERGROUND UTILITY NOTES

1. CONTRACTOR SHALL VERIFY LOCATIONS OF ALL PROJECT RELATED UTILITIES, BURIED AND ABOVE GROUND, REGARDLESS OF INCLUSION ON THESE PLANS. THE LOCATIONS OF ANY EXISTING UTILITIES SHOWN ARE APPROXIMATE ONLY.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK.
3. ALL CONTRACTOR DAMAGED UTILITIES SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.

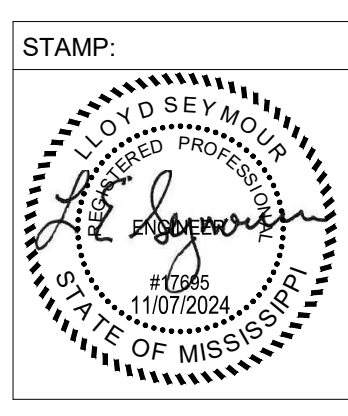
SITE UTILITY NOTES

1. CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING LOCATIONS OF ALL PROJECT RELATED UTILITIES, BURIED AND ABOVE GROUND, REGARDLESS OF INCLUSION ON THESE PLANS. THE LOCATIONS OF ANY EXISTING UTILITIES SHOWN ARE APPROXIMATE ONLY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THE EXACT LOCATIONS OF ALL EXISTING UTILITIES BEFORE COMMENCING WORK. ALL CONTRACTOR DAMAGED UTILITIES SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
2. CONTRACTOR IS RESPONSIBLE FOR CONNECTION TO EXISTING WATER MAIN.
3. CONTRACTOR IS RESPONSIBLE FOR PERFORMING GAS LINE TAP, PROVIDING GAS VALVE, AND PROVIDING GAS SERVICE TO THE BUILDING.
4. CONTRACTOR SHALL FURNISH AND INSTALL 750 LINEAR FEET OF 4" SDR-21 CL200 PVC AND ASSOCIATED CAPS FOR USE AS IRRIGATION SLEEVES. LOCATIONS OF IRRIGATION SLEEVES SHALL BE COORDINATED WITH THE OWNER.
5. CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING WITH OTHER TRADES FOR TIE IN LOCATION AND SIZE REQUIREMENTS FOR UTILITY TIE IN POINTS.
6. CONTRACTOR SHALL NOTIFY THE APPROPRIATE UTILITY AT LEAST 48 HOURS PRIOR TO COMMENCING CONSTRUCTION HAVING POTENTIAL IMPACT TO THE UTILITY'S STRUCTURE.
7. CONTRACTOR SHALL INSTALL FITTINGS, THRUST BLOCKS, AND OTHER REQUIRED COMPONENTS TO ESTABLISH THE PROPER ALIGNMENT OF UTILITY MAINS AND SERVICES AS SHOWN ON THE PLANS.
8. CONTRACTOR SHALL INSTALL ALL WATER MAINS AND SERVICES AT AN ELEVATION WHICH WILL AVOID ALL CONFLICTS WITH SEWER, DRAINAGE, AND OTHER UNDERGROUND UTILITIES. WATER MAINS SHALL BE LAID WITH A MINIMUM OF 30" COVER UNLESS APPROVED BY THE ENGINEER TO AVOID A CONFLICT.
9. ALL VALVES BOXES, CLEANOUTS, SEWER MANHOLE TOPS, AND OTHER UTILITY STRUCTURE TOPS SHALL BE ADJUSTED BY THE CONTRACTOR TO MATCH FINAL GRADES IN ALL AREAS.

BAY ST. LOUIS CONVENIENCE STORE
 1083 HIGHWAY 90, BAY SAINT LOUIS
 HANCOCK COUNTY, MISSISSIPPI
 CIVIL PERMIT SET

SHEET REVISIONS:

#	DATE/REFERENCE



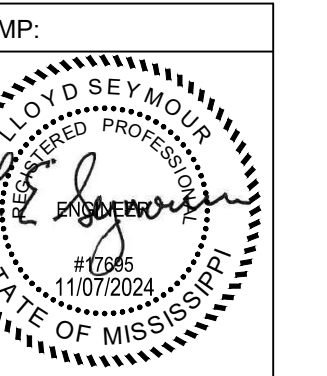
SHEET TITLE:
UTILITY PLAN

DATE: 11-7-2024
 SHEET NUMBER: 12 OF 13

C400

1
 C400 **UTILITY PLAN**
 SCALE: 1" = 20'

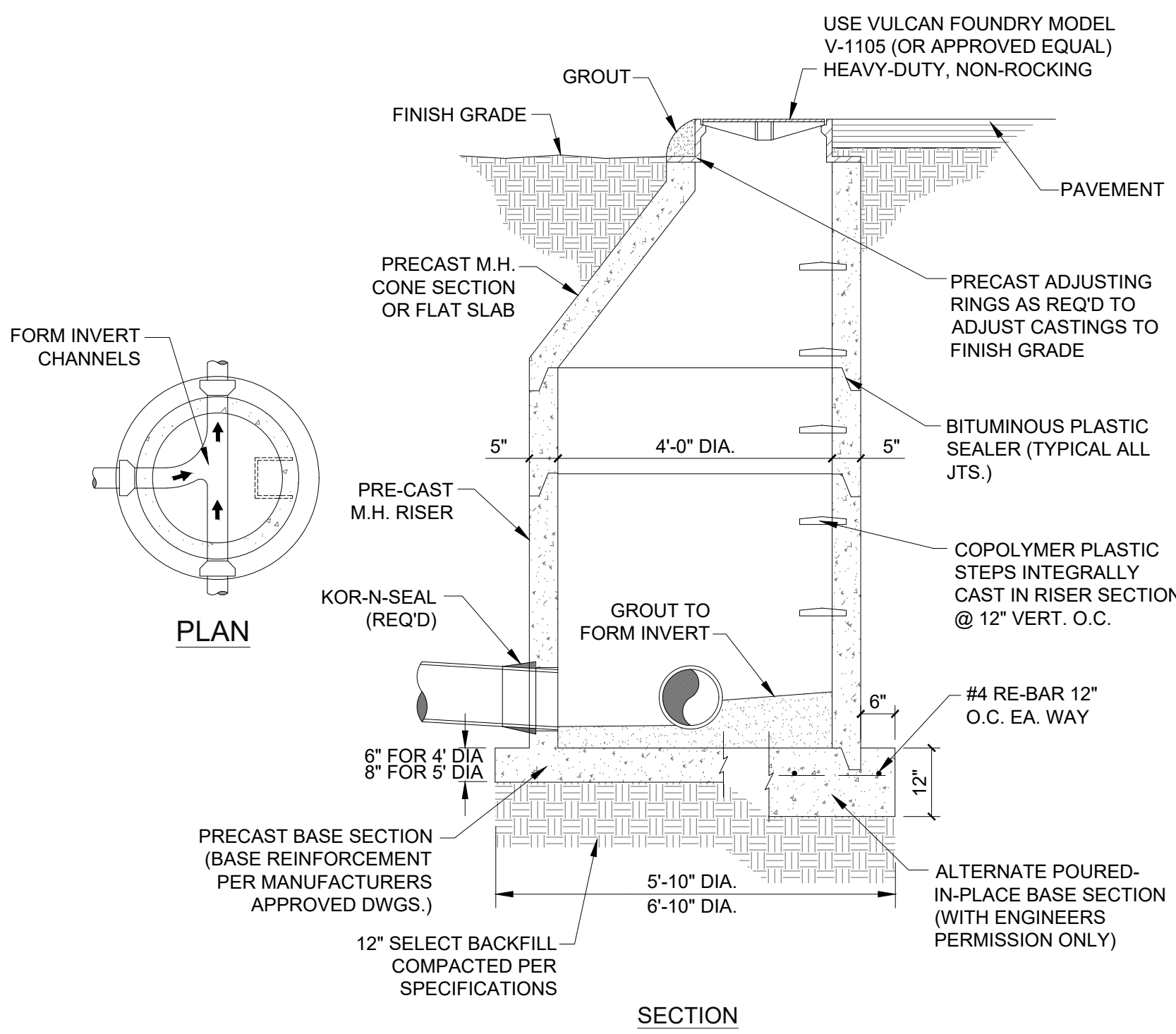
SHEET REVISIONS:	
#	DATE/REFERENCE



SHEET TITLE:
UTILITY DETAILS

DATE: 11-7-2024
SHEET NUMBER: 13 OF 13

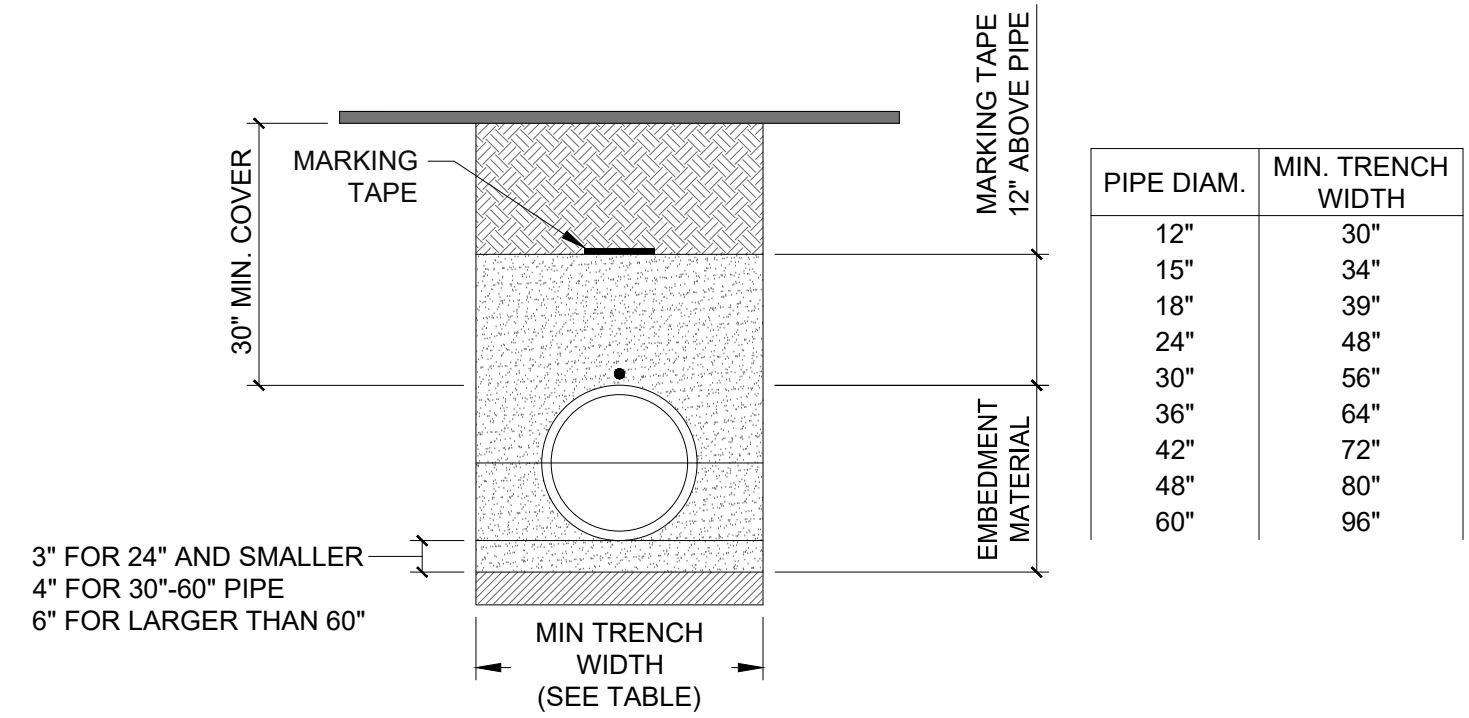
C450



1 TYPICAL SEWER MANHOLE DETAIL
C450 SCALE: NTS

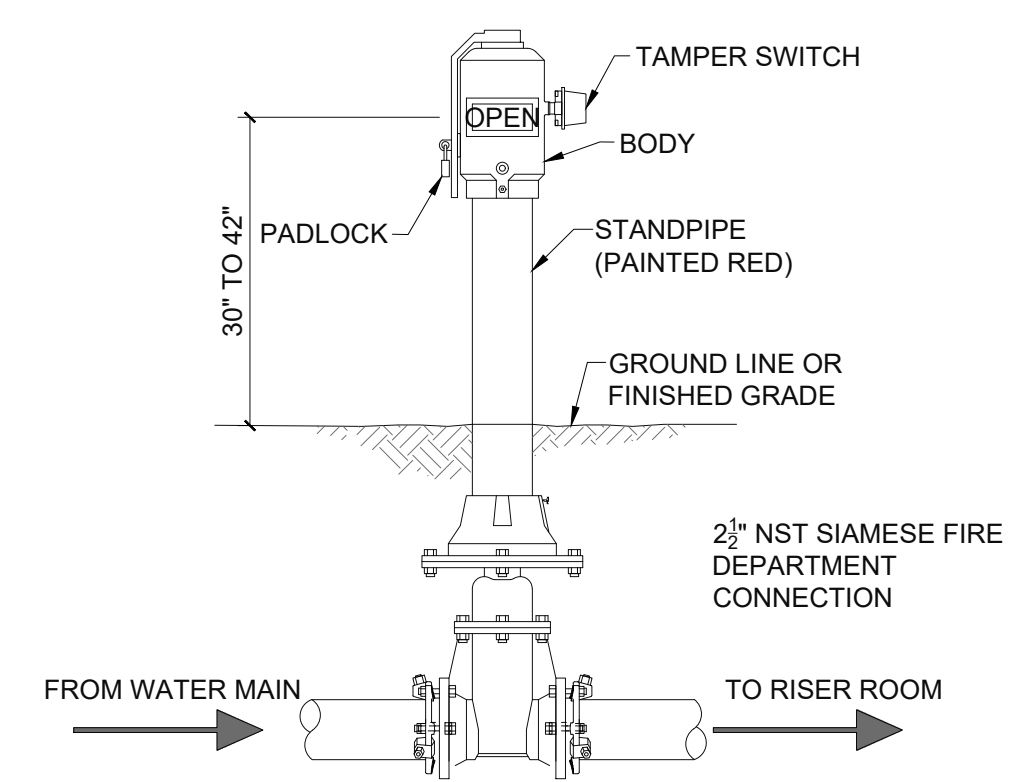
ASTM D2321 CLASS DESCRIPTION		ASTM D2487 DESCRIPTION NOTATION		MINIMUM STANDARD PROCTOR DENSITY (%)	MAXIMUM LIFT PLACEMENT DEPTH
I	CRUSHED ROCK, ANGULAR	N/A	ANGULAR CRUSHED STONE OR ROCK, CRUSHED GRAVEL, CRUSHED SLAG; LARGE VOIDS WITH LITTLE OR NO FINES	DUMPED	18"
II	CLEAN, COARSE-GRAINED SOILS	GW	WELL-GRADED GRAVEL, GRAVEL-SAND MIXTURES; LITTLE OR NO FINES	85%	12"
		GP	POORLY-GRADED GRAVELS, GRAVEL-SAND MIXTURES; LITTLE OR NO FINES		
		SW	WELL-GRADED SANDS, GRAVELLY SANDS; LITTLE OR NO FINES		
		SP	POORLY-GRADED SANDS, GRAVELLY SAND; LITTLE OR NO FINES		
III	COARSE-GRAINED SOILS, BORDERLINE CLEAN TO W/FINES	GW-GC, SP-SM	SANDS AND GRAVELS WHICH ARE BORDERLINE BETWEEN CLEAN AND WITH FINES	90%	9"
		GM	SILTY GRAVELS, GRAVEL-SAND-SILT MIXTURES		
		GC	CLAYEY GRAVELS, GRAVEL-SAND-CLAY MIXTURES		
		SM	SILTY SANDS, SAND-CLAY MIXTURES		
		SC	CLAYEY SANDS, SAND-CLAY MIXTURES		
III	INORGANIC FINE-GRAINED SOILS	ML	INORGANIC SILTS AND VERY FINE SANDS, ROCK FLOUR, SILTY OR CLAYEY FINE SANDS, SILTS WITH SLIGHT PLASTICITY	90%	9"
		CL	INORGANIC CLAYS OF LOW TO MEDIUM PLASTICITY; GRAVELLY, SANDY, OR SILTY CLAYS; LEAN CLAYS		

2 EMBEDMENT MATERIAL TABLE
C450 SCALE: NTS



- WATER AND SEWER PIPE INSTALLATION NOTES**
- SUITABLE EMBEDMENT MATERIALS, EITHER ON-SITE OR IMPORTED, SHALL MEET THE REQUIREMENTS FOR CLASS I, II, OR III PER THE LATEST VERSION OF ASTM D2321. THE CONTRACTOR SHALL PROVIDE DOCUMENTATION TO THE ENGINEER FOR EMBEDMENT MATERIAL TO BE USED FOR PIPE INSTALLATION. SEE THE EMBEDMENT MATERIAL TABLE FOR COMPACTION AND LIFT PLACEMENT REQUIREMENTS.
 - FOUNDATION: WHERE THE TRENCH BOTTOM IS UNSTABLE, THE CONTRACTOR SHALL EXCAVATE TO A DEPTH REQUIRED BY THE ENGINEER AND REPLACE WITH SUITABLE MATERIAL AS SPECIFIED BY THE ENGINEER.
 - BEDDING: BEDDING IS REQUIRED TO ESTABLISH LINE AND GRADE AND TO PROVIDE FIRM PIPE SUPPORT. MINIMUM BEDDING THICKNESS SHALL BE 4" FOR UP TO 24" DIAMETER PIPE AND 6" FOR 30"-60" DIAMETER PIPE. THE MIDDLE 1/3 BENEATH THE PIPE INVERT SHALL BE LOOSELY PLACED, WHILE THE REMAINDER SHALL BE THOROUGHLY COMPACTED.
 - HAUNCHING: THE HAUNCHING MATERIAL SHALL BE INSTALLED UNIFORMLY IN LIFTS ON EACH SIDE OF THE PIPE AND SHOVELED UNDER THE PIPE ENSURING TO FILL VOIDS. THE MATERIAL SHALL BE THOROUGHLY COMPACTED TO THE SPRING LINE OF THE PIPE EXTENDING TO THE SIDE WALLS OF THE TRENCH ENSURING THAT THE PIPE ALIGNMENT IS NOT DISTURBED.
 - INITIAL BACKFILL: THE INITIAL BACKFILL SHALL PROCEED TO THE TOP OF THE PIPE. THE MATERIAL SHALL BE THOROUGHLY COMPACTED INSTALLED IN UNIFORMED LIFTS ON EACH SIDE OF THE PIPE EXTENDING TO THE SIDE WALLS OF THE TRENCH.
 - FINAL BACKFILL (NON-TRAFFIC): SUITABLE MATERIAL IN NON-TRAFFIC APPLICATIONS (GRASS OR LANDSCAPE AREAS) SHALL BE GENERAL FILL MATERIAL. BACKFILL SHALL PROCEED TO FINISHED GRADE IN 12 INCH LIFTS COMPACTED TO ELIMINATE AIR VOIDS.
 - FINAL BACKFILL (TRAFFIC): SUITABLE MATERIAL IN TRAFFIC APPLICATIONS SHALL BE SELECT FILL COMPACTED IN 8 INCH LOOSE LIFTS TO NOT LESS THAN 95 PERCENT STANDARD PROCTOR.
 - MINIMUM COVER (NON-TRAFFIC): FOR NON-TRAFFIC APPLICATIONS (GRASS OR LANDSCAPE AREAS), MINIMUM COVER IS 12" FROM THE TOP OF PIPE TO GROUND SURFACE.
 - MINIMUM COVER (TRAFFIC): FOR TRAFFIC APPLICATIONS THE MINIMUM COVER IS 12" FOR UP TO 48" DIAMETER PIPE AND 24" OF COVER FOR 60" DIAMETER PIPE, MEASURED FROM TOP OF PIPE TO BOTTOM OF FLEXIBLE PAVEMENT OR TO TOP OF RIGID PAVEMENT.
 - CONTRACTOR SHALL MAINTAIN TRENCH BACKFILL AT GROUND SURFACE UNTIL FINAL ACCEPTANCE OF THE WORK. ALL SURPLUS MATERIALS NOT USED IN BACKFILLING SHALL BE REMOVED AND DISPOSED OF BY CONTRACTOR AT HIS OWN EXPENSE.

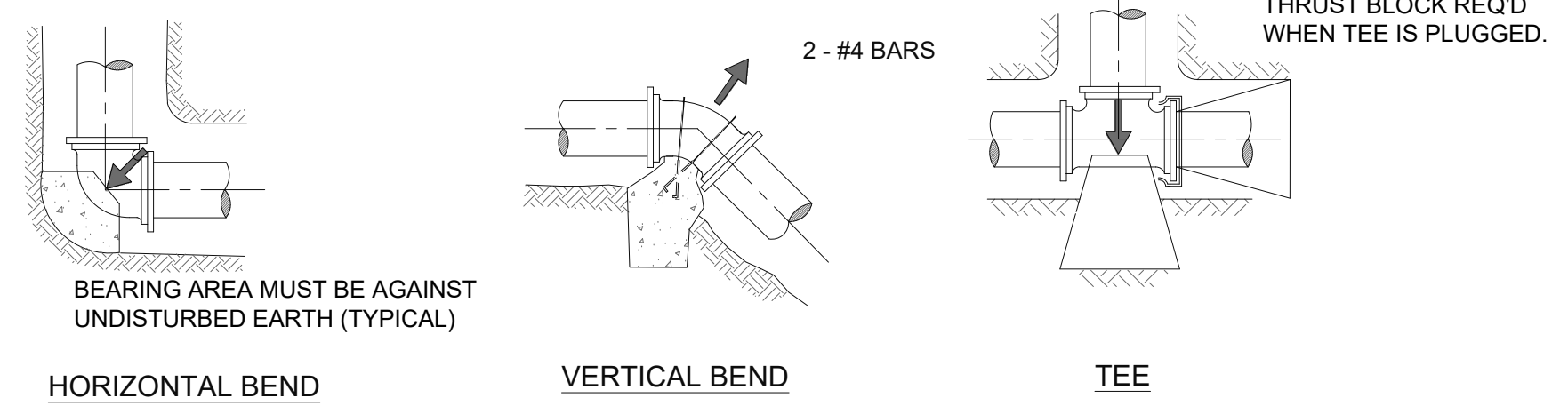
3 WATER AND SEWER PIPE INSTALLATION DETAIL
C450 SCALE: NTS



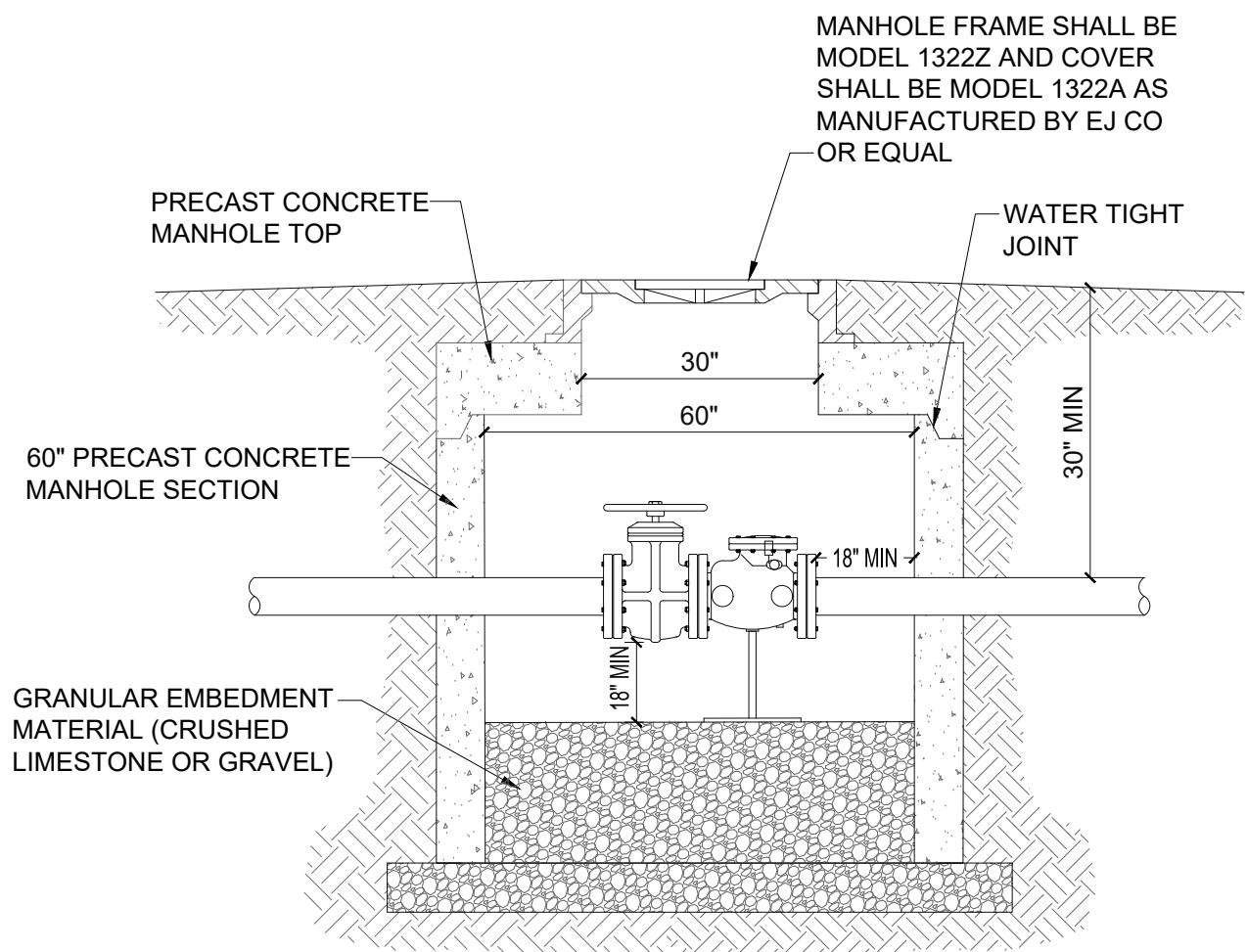
6 TYPICAL POST INDICATOR VALVE DETAIL
C450 SCALE: NTS

BEARING AREAS FOR THRUST BLOCKING IN SQUARE FEET						
FITTING:	4" DIA.	6" DIA.	8" DIA.	10" DIA.	12" DIA.	14" DIA.
TEES	2.0	2.5	4.7	5.0	7.0	9.0
90°	2.0	2.7	6.7	7.2	10.4	12.7
45°	1.0	1.5	3.6	3.9	5.6	6.9
22 1/2°	1.0	1.0	1.8	2.0	2.9	3.5

NOTE: ALL FITTINGS SHALL BE COVERED WITH A GEOTEXTILE FABRIC PRIOR TO POURING CONCRETE THRUST BLOCKING



4 TYPICAL WATER MAIN THRUST BLOCK DETAIL
C450 SCALE: NTS



5 TYPICAL CHECK VALVE DETAIL
C450 SCALE: NTS

Tax Increment Financing Redevelopment Plan Bay St. Louis, Mississippi 2025

Prepared By:
Laurence E. Leyens
Urban Development Toolbox, LLC
7600 Joe Fountain Road
Ocean Springs, MS 39564
(601) 301-8500



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Introduction and Purpose

The following Tax Increment Financing Redevelopment Plan Bay St. Louis, Mississippi 2025 has been developed as an addendum to various other plans, studies, and data collections that exist and are referenced herein on behalf of the City of Bay St. Louis, Mississippi. Its purpose is to:

- illustrate the need for new development and redevelopment activities in Bay St. Louis;
 - consolidate, restate, and expand the redevelopment themes of the Bay St. Louis various community plans, relating specifically to the *Bay St. Louis, Mississippi 25 Year Comprehensive Plan* adopted in July 2024, additional studies, updated goals, and objectives referenced herein;
 - establish additional redevelopment strategies for the City of Bay St. Louis;
 - provide the basis for authorizing specific Tax increment Financing mechanisms to facilitate new development projects and redevelopment projects; and,
 - guide new developments and redevelopment initiatives for all areas of the City of Bay St. Louis, Mississippi referred herein as the (“City”).
-



Section 1 – Redevelopment Planning Method

This Redevelopment Plan was developed in the four steps described as follows:

1. **Review and assessment** - a review of various existing city plans, interviews, and Hancock County plans, the (“County”) including specifically the guiding over-all strategic plan referenced herein as the (“**Bay St. Louis Comprehensive Plan**”) referring to a 25 year plan adopted by the City in July 2024, and other plans, codes, zoning, statutes, and ordinances referenced herein as the (“**General Development Plan**”), as provided by the City of Bay St. Louis, the Hancock County Supervisors, as well as additional collected data, economic impact trends, US Census data, demographics, quality of life factors, and related studies that were conducted in the past few years to identify previously determined findings for the need for economic growth, new developments and redevelopment
2. **Goal identification** - a review of previously identified redevelopment goals and objectives was conducted to identify those that supported redevelopment activities
3. **Redevelopment area designation** - a determination was made of the appropriate redevelopment areas for the City of Bay St. Louis
4. **Implementation** - recommended implementation strategies for accomplishing specific development and redevelopment efforts for Bay St. Louis located in the southwest section of Hancock County, Mississippi.

The primary documents, plans, and studies reviewed in developing this Tax Increment Financing Redevelopment Plan are:

1. City of Bay St. Louis, Mississippi 25 Year Comprehensive Plan Adopted in July 2024
2. Zoning ordinances and survey maps: adopted by the City of Bay St. Louis
3. United States Census Data Collection as of 2020 (with forecasted estimates for 2024)
4. ACS Data USA (reflecting census data, state sourced data, and algorithms established to estimate current values and trends)
5. Census Tract 303 Opportunity Zone
6. United States Census Bureau. P2 Hispanic or Latino, and Not Hispanic or Latino by Race. 2020 Census State Redistricting Data (Public Law 94-171) Summary File. August 2021.
7. United States Census Bureau. Annual Estimates of the Resident Population: April 1, 2020 to July 1, 2023. Population Division. May 2024.
8. United States Census Bureau. 2023 American Community Survey 5-Year Estimates. December 2024.
9. Cubit Planning 2025 Population Projections. December 2024.
10. Various other economic, statistical data, and resources from internet-based providers, opinions, images, and interpretations
11. Various City of Bay St. Louis, Hancock County resources, the State of Mississippi Vital Statistics web sites, and references of various authors, creators, and publishers

Section 2 – Need for Redevelopment in Bay St. Louis Established

The City of Bay St. Louis 25 Year Comprehensive Plan adopted in 2024 states in the following excerpts that provide for the need for new development and redevelopment using good planning techniques to manage land use:

BAY ST. LOUIS DEVELOPMENT PATTERNS AND BUILDOUT

Analysis of development patterns provides the basis for conclusions for future growth and development opportunities and growth constraints. Development patterns are the result of a combination of market forces, development policy, and cumulative community decisions for what development should occur, the form it should take, and where it should be located. Development patterns illustrate both a quantitative and qualitative view of Bay St. Louis currently. The relationship and interaction among shopping areas, neighborhoods, and the natural environment, coupled with the identification of developable land for the future, are included in the development pattern analysis. These factors are mapped, measured, and interpreted in this section.

Existing Land Use

Existing land use and development was inventoried, mapped, and analyzed in order to illustrate community development patterns and trends for Bay Saint Louis 2045. Data sources for existing land use included the 2009 Comprehensive Plan, current aerial photography, information from the Hancock County Geographic Information System and visual surveys of selected areas. The categories used to classify development patterns and their meanings are listed as follows:

Residential Land Uses

- ◆ **Single Family.** A single residential living unit of conventional (on-site) construction, designed to house only one family. The density of units per acre may range from 1 to 8.

Above: BSL Home in an Old Town Neighborhood

Right: Bay St. Louis Existing Land Use

- ◆ **Duplex.** A residential living unit of conventional construction and containing two attached dwelling units.
- ◆ **Multiple-family.** Structures designed with more than two attached living units such as apartments with densities typically greater than 8 units per acre.
- ◆ **Townhomes.** Residential townhome development typically occurs at densities of 4 to 12 units to the acre in structures that are typically attached, but are owner occupied.
- ◆ **Manufactured Home or Park.** Manufactured homes, either on a single lot or within manufactured home communities.

Commercial Land Uses

- ◆ **Corridor Commercial.** Stores as fixed point-of-sale locations designed to attract high volumes of customers. Their

Land Use Category	Acres	% of Total
Residential	1,795.10	5.39%
Commercial-Office	442.09	5.39%
Healthcare	30.05	0.37%
Industrial - Warehouse	3.31	0.04%
Institutional	226.36	2.76%
Public Open Space - Recreation	141.24	1.72%
Private Open Space - Recreation	49.63	0.61%
Vacant	5,310.30	64.80%
Other	196.40	2.40%
Total	8,194.48	100 %

environments auto dominated typically with large parking lots between streets and buildings. Such areas are considered suburban in character.

- ◆ **Old Town and Depot District.** This category represents the historic heart of Bay St. Louis' community life. Its environment is scaled to pedestrians. Buildings are typically sited in close proximity to the street. Parking areas are typically off-site or to the side and rear of buildings.
- ◆ **Service Commercial.** Commercial activity oriented to providing repair, outdoor storage, contracting or machinery and equipment sales, including automobiles. These establishments require outdoor work and storage spaces that often do not blend well with the other land uses.
- ◆ **Recreation Vehicle Park/Campground.** This category refers to facilities designed to accommodate recreational vehicles on a temporary basis.
- ◆ **Lodging .** This category accommodates lodging facilities such as hotels and motels.
- ◆ **Commercial - Other.** These commercial uses fall outside the previous commercial categories.
- ◆ **Office.** This category accommodates places for professional, health care, or administrative activities.
- ◆ **Neighborhood Commercial.** This category of land use activity describes commercial activity that is oriented to nearby neighborhoods, providing light retail goods and services to meet the ordinary requirements of daily life. Buildings are smaller in scale.

Industrial

- ◆ **Industrial.** These areas accommodate manufacturing, warehousing, storage, or distribution of products or goods. These uses may include uses that may generate substantial amounts of noise, odor, light, traffic or other nuisances associated with industrial uses.

Health Care

- ◆ **Medical - Dental.** The category indicates medical uses such as clinics and hospitals.

Institutional

- ◆ **Municipal.** Municipal uses are those which are exclusively used for the administration of local municipal government and include police, fire, and other municipal services.
- ◆ **Government, County and State.** These uses are those which accommodate the operations of Hancock County and the State of Mississippi.
- ◆ **Assembly.** These uses are devoted to various assembly operations such as places of worship, funeral homes, theaters, and other venues in which large numbers of people gather for entertainment, sporting, or other events.
- ◆ **School, Private .** The category indicates land used for private educational purposes.
- ◆ **School, Public.** The category indicates land used for public educational purposes.

Parks, Open Spaces, Natural Areas

- ◆ **Parks and Open Space, Public.** Land that is used as active park space or otherwise reserved as open space and available to the public in general.
- ◆ **Parks and Open Space, Private.** Land that is used as active park space or otherwise reserved as open space and available with private restricted access.
- ◆ **Natural and Environmental Lands.** These lands are generally open lands that serve important ecological functions.

Vacant, Infrastructure, and Miscellaneous

- ◆ **Vacant.** Vacant lands lie unoccupied and may generally be considered a buildable lot or parcel.
- ◆ **Utility/Infrastructure.** These lands include uses devoted to community maintenance functions such as shops and storage yards for maintenance facilities and materials, rights of way streets and other mobility

infrastructure and utility easements for community utility infrastructure such as water, sewer and power facilities.

- ◆ **Miscellaneous Land Uses.** The land uses include independent parking lots, cemeteries, and beachfronts.

Build-out Analysis

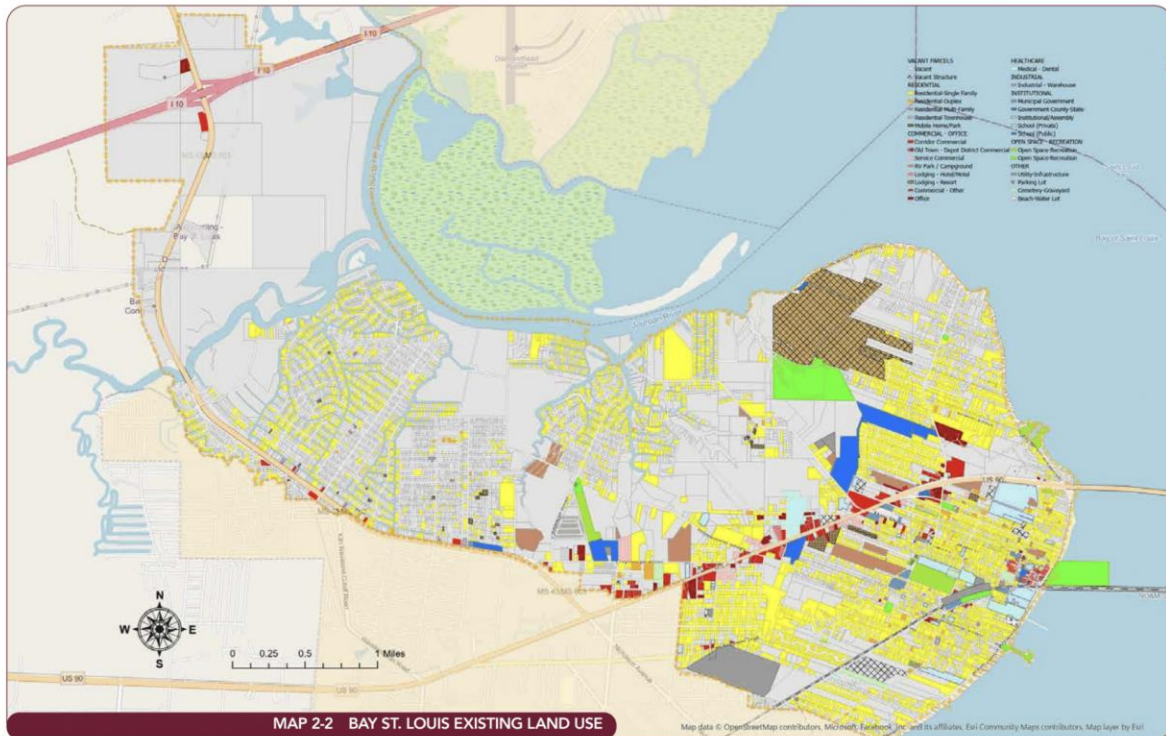
A crucial part of the development patterns analysis is determining Bay St. Louis’ development capacity and build-out. Projecting future growth and its impacts is necessary to properly align growth with community vision. Community build-out analysis is used to provide essential growth information.

A build-out analysis examines development carrying capacity of a given geography. Carrying capacity refers to an area’s maximum potential level of residential and non-residential development. Carrying capacity is developed from variables such as number of dwelling units and floor area ratios permitted per acre by applicable zoning rules, environmental constraints, infrastructure capacity, and other factors that impact development density and intensity. Build-out analysis helps to answer critical planning questions such as:

- ◆ **What are the likely community impacts if growth occurs to the maximum extent permitted?**
- ◆ **Is the community prepared to accommodate growth at the scale permitted?**
- ◆ **Are the growth patterns represented by permitted development desirable, and do they align with community vision?**
- ◆ **Does the amount of planned development reflect current market realities?**

Findings of a build-out analysis are used to assess the impacts of potential growth and to help determine whether current development patterns, plans, and codes align with a community’s vision and market realities.

A build-out analysis is conducted at varying scales and can be performed at any point along a spectrum that ranges from the general or macro-level to more precise, micro-level calculations. Bay St. Louis’ build-out analysis was conducted at the macro-scale using current zoning and environmental constraints. The analysis assumes that future growth under current zoning will occur to the maximum extent permitted. Environmentally constrained development is assumed to occur at 50 and 75 percent of maximum capacity.



Bay St. Louis' build-out analysis identified vacant land and environmentally constrained land from the existing land use survey and the environmental inventory. The results are illustrated on Vacant and Environmentally Constrained Lands Map on the previous page. The zoning classification of each parcel of vacant land was then determined. Vacant areas were summed by zone and permitted development intensity was applied to calculate carrying capacity. This was expressed in units per acre for residential zones and square feet of building per acre in nonresidential zones. Environmental constraint factors were applied to land impacted by wetlands, trees and flood zones at a rate of 25 and 50 percent.

For example, if the maximum permitted dwelling unit density of one hundred vacant acres of land zoned R-1 were two dwelling units per acre, the carrying capacity of the land would be 200 dwelling units (100 acres x 2 units per acre). Dwelling units can then be converted into the estimated population by multiplying by the average household size in Bay St. Louis. In 2021 average household size was 2. Therefore, two hundred dwelling units multiplied by 2 persons per unit represents a population of 400 for the 100 acres.

Commercial building square footage is calculated by multiplying available acreage by either 11,000 or 21,780 square feet per acre. These figures represent an assumed building footprint area per acre of 25 percent in suburban areas or 50 percent in downtown or mixed-use areas, respectively. These ratios are common rules of thumb for suburban commercial and urban commercial development intensity.

Bay St. Louis' build-out analysis reveals that the city can accommodate between 4200 and 9300 persons in the future. Commercial floor space can be accommodated approximately between 1.1 and 2.5 million square feet of building space. Detailed build-out calculations are included in the appendix and summarized in Tables 2.2 and 2.3.

Table 2.2 - Summary of Vacant Land by Zone and Constraint

Current Zoning of Vacant Land	Vacant (Acres)	Constrained Vacant/Flood (ac) ¹	Unconstrained Vacant ²	Total Constrained & Unconstrained Vacant	Wetland Severely Constrained (ac) ³	Square Feet
Residential	2,918	1,461	246	1,707	1,166	-
Commercial	1,051	323	62	385	662	16,752,218
Industrial	3	0	0	0	3	8,146
Casino/ Resort ⁴	-	-	-	-	-	-
Waterfront ⁵	1,354	185	0	185	1,168	8,040,174
Special Use ⁶	16	12	1	13	3	579,348
Totals	5,365	2,004	309	2,313	3,001	25,379,886

NOTES:
¹Land within a FEMA-designated Flood Hazard Area (excluding floodway). These properties are buildable, but subject to specific requirements.
²Land unconstrained by natural hazards including, FEMA flood hazard areas, tidal areas, and wetlands. These properties are buildable, but subject to zoning regulations.
³These properties are located within tidal areas or wetlands. Development is largely precluded due to regulation and/or cost.
⁴Variance between raw vacant land totals and buildout calculations is a result of parcels lying within multiple zoning districts, presence of waterways and unopened public rights-of-way. Overall deviation is less than 1.0%.
⁵Excluded from modeling given the districts' uncertain application.
 Source: Consultant Analysis

Table 2.3 - Potential Build-out Summary

Current Zoning of Vacant Land	Development Density / Intensity	50% Constraint		75% Constraint		2021 HH Size	50% Constraint	75% Constraint
		Devel-opable (ac)	Po-tential Units	Devel-opable (ac)	Po-tential Units		Floor Area/ Population	Floor Area/ Population
Residential	5	854	4,268	427	2,134	2	9,389	4,268
Commercial	0.4	192	-	96	-	-	2,151,265 sf	1,075,633 sf
Industrial	-	0	-	0	-	-	1,426 sf	713 sf
Casino/Resort	-	-	-	-	-	-	-	-
Waterfront	-	-	-	-	-	-	-	-
Special Use	-	7	-	3	-	2	Varies	Varies

Source: Consultant Analysis



The information from this plan as well as other resources referenced herein has been reexamined for implications related to the Tax Increment Financing Redevelopment Plan's (the "**TIF Redevelopment Plan**") conclusions regarding new development and redevelopment opportunities. It further establishes the need for economic development initiatives, economic inducements, and outreach efforts to encourage private sector investments to encourage private sector growth in the City.

Specifically, related statistics provide a detailed picture of the development conditions in Bay St. Louis and Hancock County from 2010 to 2024. Selected statistics have been updated through US Census year 2020 and estimated for some data up to 2024. The data has been reexamined for implications related to the TIF Redevelopment Plan's conclusions regarding new development and redevelopment opportunities.

Specifically, statistics related to demographics, economics, and housing have been selected for discussion to provide a data-driven basis approach to establishing the need for new development and redevelopment activity in Bay St. Louis.

City of Bay St. Louis Historical Summary:

(Copied from The City of Bay St. Louis 25 Year Comprehensive Plan adopted in 2024)

First Nations and Cultures

Bay Saint Louis is located on land that has been occupied by humans for thousands of years. Elevated topography adjacent to the diverse ecosystem formed by the Pearl River, Jourdan River and the Mississippi Gulf Coast with their riverine, wetlands, salt water marshes, open gulf and piney woods, formed the environment for the earliest human occupation. Major prehistoric periods for the area are divided by anthropologists in four distinct periods.

- **THE PALEO-INDIAN CULTURE PERIOD (~12,000-8,000 B.C.)** This first culture, spanned a period from the last glacial age around 12,000 B.C. to about 8000 B.C. Temperatures averaged five to ten degrees cooler, the climate was drier and sea level had not reached its modern height, likely 30 to 100 feet below current levels extending to the edge of the continental shelf. The colder climate resulted in more open grasslands and the southern extension of temperate types of trees like spruce and fir. People were organized in small bands and moved frequently to follow the large game.
- **THE ARCHAIC CULTURE PERIOD (~8,000-1,000 B.C.)** Over the course of time, populations settled in specific localities for longer periods of time. The warming climate encouraged more exploitation of woodlands and river valleys, while along the coast fishing and shellfish harvesting became increasingly common economic pursuits.
- **THE WOODLAND CULTURE PERIOD (~1,000 B.C. - A.D. 700)** Among the perceived changes that are hallmarks of the Woodland is the increasing importance of corn agriculture, the advent of tribal organization replacing multi-family or clan organizations, larger, more permanent villages, an increased use of conical burial mounds.
- **THE PLAQUEMINE AND MISSISSIPPIAN CULTURE PERIODS (A.D. 1000- A.D. 1500)** This period of prehistory is characterized by the construction of large temple mounds that served as platforms for the houses of chiefs and for temples. The European invasion contributed to the depopulation of the region through the spread of disease and violence. Still Choctaws and affiliated groups like the Bayougoulas, Acolapissa, Pascagoula and the Siouan-speaking Biloxi Indians continued to live in the area,.

European Encounter and Settlement

In 1698, Louis XVI, King of France claimed Mississippi River and the territory it claimed for France. The expedition of Bienville explored the claim and camped on the banks of the Bay of Saint Louis in 1699, naming it for Louis IX, the King of France. In December Bienville established the first colony at Bay Saint Louis, consisting of a sergeant and fifteen soldiers, thus creating the third colony located on the Gulf of Mexico. It was another eighteen years before he gave up on his efforts to find deep-water access to the area’s French colonies and built a fort at present day New Orleans.

England later defeated France in the Seven Years War and all of Louisiana east of the Mississippi River except the Ile d’Orleans, was awarded to England. Louisiana west of the Mississippi and the Ile d’Orleans went to Spain. Spain declared war with Britain in June 1779 and as a result of the ensuing treaty, British West Florida became Spanish West Florida.

In the period from 1780 to 1810 Spain moved quickly to recognize the property rights of persons who occupied their holdings in West Florida and who would swear allegiance to the Spanish crown and the Catholic Church.

In 1803, Napoleon Bonaparte sold the vast Louisiana Territory to the United States. Shortly thereafter in 1810, the people living between the Perdido and Pearl rivers declared their independence as the republic of West Florida. This area was annexed by the United States shortly thereafter.



Statehood and The Creation of Hancock County

The Congress of the United States created the Mississippi Territory on April 7, 1798, which included all that land presently composing the states of Mississippi and Alabama. Settlers generally moved into the coastal areas, overwhelming the French and Spanish Catholic population until they reached the eastern shore of the Bay of Saint Louis, where the migration generally ceased.

Hancock County was created in 1812 by the Mississippi Legislature after statehood that same year. The area slowly developed over the ensuing decades with continued settlement and timber harvesting and wood processing dominating the economic activity.

After the Louisiana Purchase in 1803, the territory was opened to homesteaders and in a three

year period thirty-three hundred people moved into the area, mostly from Virginia, Tennessee, Georgia, and the Carolinas. These Anglo-Saxon Protestants settled across the Coast from Mobile to the Bay of Saint Louis, but there was no bridge across the bay for another 110 years after statehood in 1817.

Bay Saint Louis was incorporated by the first act of the first legislature of the State of Mississippi on January 4, 1818. (The city was called Shieldsboro from 1802 until the name Bay Saint Louis was restored by the legislature in 1875.) However, the colony remained staunchly French, relatively isolated from the Americans but kept its close connections with its French New Orleans cousins. Subsequently, Bay Saint Louis became the summer home of wealthy New Orleanians, thus re-enforcing the French culture of the city which it retained into the early 20th Century.

The British were so certain of victory that they brought civil servants to assume governing the areas they expected to conquer with them, as well as wives and children who were waiting on the Mississippi Coast islands. However, the great victory for the Americans was rendered inconsequential because the peace treaty had already been signed and word had not reached the Coast.

Mississippi became a state in December, 1817 and the first act of the Mississippi legislature was to incorporate the city of Bay Saint Louis to become the capital of the state. The incorporation was completed at the morning session but at the afternoon session, the representative from Rankin County changed his vote and Natchez was designated capital instead. It remained the capital for two years before the capital was moved to Jackson where it remains.

The Civil War

Mississippi was the second state to sign an act of secession following South Carolina. The Civil War brought difficult and hard times to the coast although there were few actual battles fought here. Abraham Lincoln sent the iron-clad ship *Massachusetts* to the channel at the west end of Ship Island thus effectively blocking the entire Mississippi Sound to traffic entering or leaving, there being no other deep water channel into the sound. Flat bottom packet boats continued to sail between New Orleans and Mobile, but connections to the eastern seaboard and with Europe were completely severed.

The Resort Era

At the end of the Civil War, real property in the state of Mississippi was said to be ninety percent destroyed. Rebuilding was a long and painful process. However, families made every effort to resume as normal a life style as possible. Carpetbaggers in governments for a period caused many properties to be forfeited, but gradually commerce and social activities resumed.

Northerners began flocking to the warm climates, and resorts sprang up everywhere. Also, the rail between Mobile and New Orleans was completed in 1870 bringing New Orleans to within one hour's travel time. This allowed families to come to Bay Saint Louis in early Spring to avoid the sickness and mosquitoes prevalent in the city. Commuter trains ran regular schedules allowing men to board their "club car" in Bay Saint Louis at 8 a.m., have a leisurely breakfast en-route, and debark at the Canal Street Station one hour later. In the afternoon, they could board the train at 5 p.m. and be picked up at the station after an hour-long cocktail period on the train.

At one time Bay Saint Louis had as many as 10 or 12 hotels. Some were, in fact, quite large like the Crescent, later called the Pickwick, the Bay Saint Louis, and the Clifton. After 1900 came the Liberty, the Klock, and the Weston which became the Reed. There were many boarding houses. Every Friday during the summer, hoards of New Orleanians came over to "The Bay" for swimming parties on the beach. Hotels had "bachelor quarters" for the single men, and every Saturday night there were dances at the various hotels which began after the heat of the day had begun to cool. These were relatively grand affairs with prominent bands, and were always accompanied by veritable feasts of food and drink.

Sunday mornings found everyone at church. Then, after a grand lunch at one of the commercial establishments, they gathered at the Woodmen of the World Hall on Second Street next to the cemetery and were entertained with musical recitals, poetry readings or the presentation of contemporary plays. At 5 p.m. they boarded their train for the return trip to the city.

The Bay had deep artesian wells with water that had a strong sulfur content. Since it smelled so bad, it was assumed that it therefore had to be healthy, so a strong reputation as a health spa developed, contributing to the summer activities.

Until June 1930, Hancock County had the largest sawmill in the world. Timber was brought from miles away to the Weston, Poitevent, Eads mill at Logtown. Subsequently, nearly all the houses at The Bay were built of heart pine. They were thereby especially susceptible to fire. Being built on land divided into arpents, (the French equivalent of an acre) houses that caught fire were unlikely to be saved. In fact, several very large fires devastated The Bay, such as the ones in 1903, 1907 and 1927. About thirty homes and businesses were destroyed in 1907, including Our Lady of the Gulf Catholic Church, the Bay

Saint Louis Opera House, and the Planchet home and stores.

Beachfront homes nearly always had either a pier, with or without pier house, or a gazebo for the enjoyment of the evening breezes. After five in the afternoon, families gathered in these open buildings to socialize, both with their families and with their friends and neighbors. After about 1900, lemonade became the favored evening beverage and it was the most refreshing. This tradition continued to the middle of the century when, after World War II, families had been so scattered, serving in the military or working in defense-related companies, that few large family groups remained to gather. Also, there was the phenomenon, air conditioning. Today few piers remain, and even fewer gazebos, and a great tradition has been lost.

Into the 20th Century

Many changes came to the Bay after 1900. An automobile bridge was built across the Bay of Saint Louis in 1928. However, it burned with such regularity that it was hardly considered reliable. There are many stories of suitors bringing their girls from the Pass over to the Bay to one of the fine motion picture houses only to find when the reel was finished that the bridge had burned and the trip back to The Pass would take much too long to fit within the curfew set by the girls' parents.

In 1963, NASA and the Stennis Space Center brought new prosperity to the Gulf Coast in the form of employment. The impact of the arrival of gambling casinos in 1994 caused the prices of homes and property to soar. New construction was present on nearly every vacant lot on the shore and inland. Unemployment has been limited to those who choose to be unemployed.

Gambling

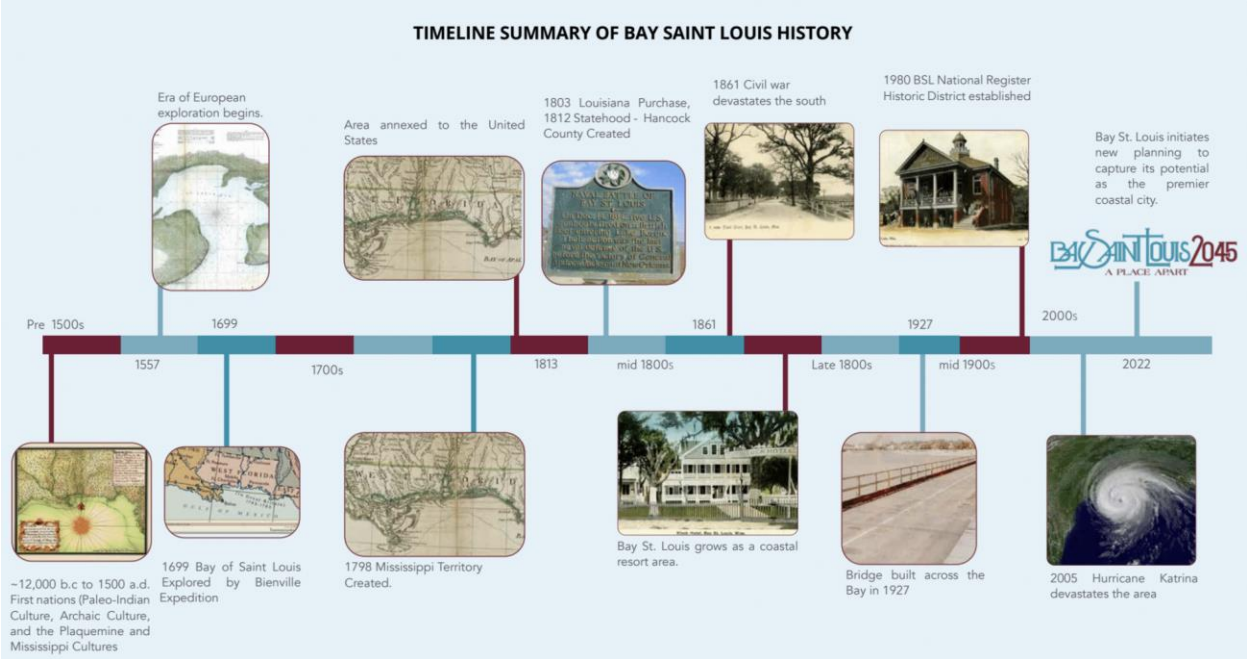
In 1990 the Mississippi State Legislature passed a bill allowing gaming in boats anchored on navigable waters and quickly brought a new and undreamed prosperity to the Mississippi Coast. New revenue flooded into the coast cities though many at great cost to the natural ocean-front landscape. Bay Saint Louis, however, confined its single casino, presently called "Hollywood Casino" to the north end of the Bay of Saint Louis. There were no roads running by the casino and one road was constructed leading to it. Those who objected to were not forced to live with it, and it quickly gained acceptance in the community. People joked that bi-weekly the casino sent an unmarked truck down to City Hall and dumped loads of money over the fence. In reality, the city was rapidly improving its infrastructure: sub-service drainage, street surfacing, lighting and many public facilities.

The Great Disasters

The Mississippi Gulf Coast had finally recovered from massive Hurricane Camille which roared ashore in 1969 killing hundreds and obliterating thousands of homes and businesses. It was the benchmark from which all modern hurricanes were compared. "We survived Camille, we can survive anything." Wrong. On August 29, 2005 Hurricane Katrina made Camille look like a summer storm. Thirty-eight foot storm surge crested the bluff at Bay Saint Louis, sweeping almost everything away. Houses that dated from 1787 were reduced to rubble. The beachfront mansions crumbled under the power of the wind-driven water.

In 1977 the Mississippi Department of Archives and History surveyed the houses in Bay Saint Louis and in 1980 assigned 728 of them to the National Register of Historic Places. There were 560 properties in the Beach Boulevard District alone, and there were three other, smaller districts. Compatibility and charm were the defining attributes of the little city. Hurricane Katrina in 2005 completely destroyed all but sixteen of the mansions on Beach Boulevard. (Please view Preservation/National Register/Beach Boulevard on this site to view those homes before the hurricane.) Recovery of the beach front during the following decade was slow and meticulous. The new and improved seawall of impressive proportions provided an area for a wider boulevard and an extensive area of grass and trees between the two.

In 2014 the new harbor completed the waterfront landscape giving an added charm that the city had not traditionally enjoyed. With its easy access to the downtown complex, it quickly became a strong attraction. Waterfront restaurants and bars sprang up along the beach providing access to the cool evening breezes and views of the picturesque Bay of St. Louis.



Population Trends:

The City of Bay St. Louis 25 Year Comprehensive Plan (adopted in 2024) and U.S. Census data examines the population growth trends in the City and the County since its founding.

Population

Bay St. Louis has seen moderate and sustained growth since at least 1980. The population jumped 12.8% between 2000 and 2010, but slowed significantly to the current estimated population of 9,353.

Between 2010 and 2020, Bay St. Louis grew by just 0.3% while the Gulfport Metro saw 7.1% growth. It's important to note that the population within a 15-minute drive of Bay St. Louis grew by over 10% during the same time period. This geography represents the city's primary local trade area and continues to see healthy population growth.

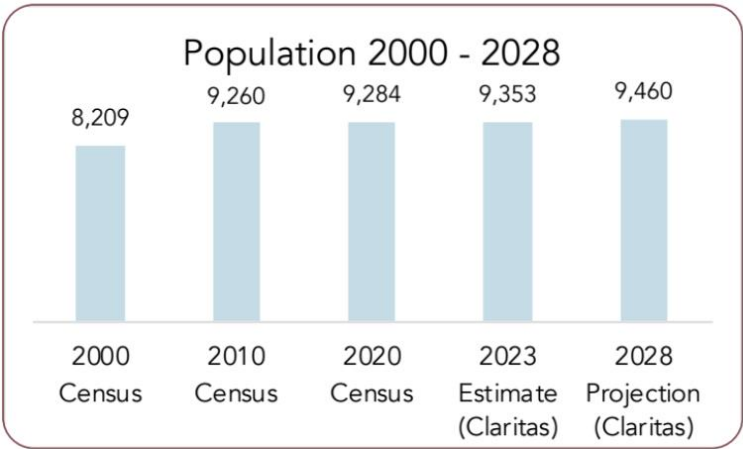
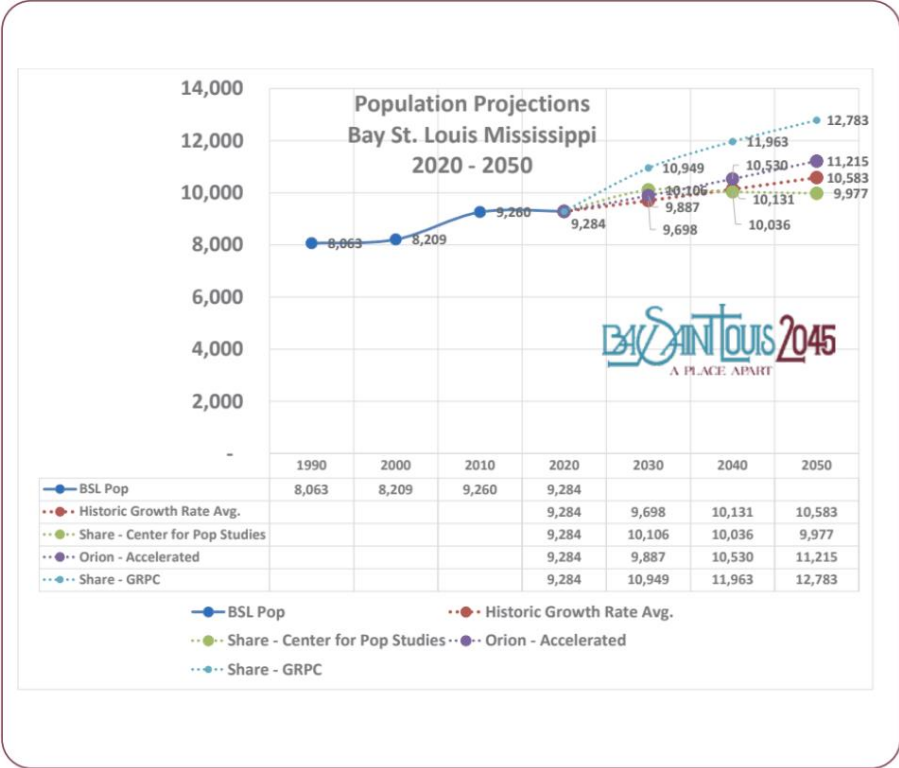


Figure 2:: Population Change in Bay St. Louis. Source: US Census and Claritas.



Income

Bay St. Louis' median household income is \$48,645. While this is among one of the lower income levels within the larger region, it represents a significant increase of the \$40,515 in 2010. It is estimated that 20% of families in the city are below poverty, compared to 12.4% for Hancock County and 14.9% for Mississippi.

Age

The 2021 median age for Bay St. Louis is 48.3. This is slightly higher than that of Hancock County (45.6) and Mississippi (38.5). 31% of Bay St. Louis' population are "Baby Boomers" between the ages of 55 and 74. Bay St. Louis' median age is up 3.2 years since 2010.

Race & Ethnicity

The city is 75% White and 15% Black. 4% of the population is Hispanic or Latino.

Educational Attainment

With 33.3% of its population having a bachelor's degree or higher, Bay St. Louis has higher education levels than Hancock County (26%), Gulfport metro (23%) and the state (22%).

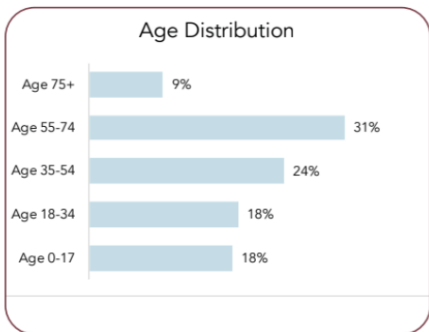


Figure 3: Bay Saint Louis Age Distribution 2023. Source: Claritas.

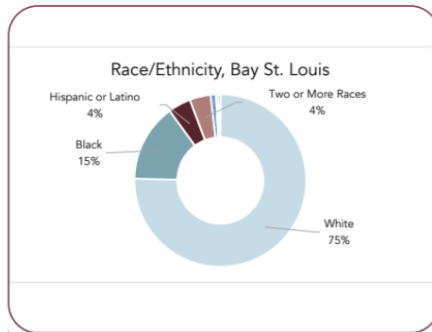





Figure 4: Bay Saint Louis Racial Makeup 2020. Source: US Census

 \$48,645 Median HH Income	 33.3% % Bachelor's Degree+	 21.1% % Families Below Poverty
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Hancock County’s Population Trends:

Hancock County’s recent population increases are examined more closely in Table I, **Migration and Natural Increase** illustrated below. As reflected in this table, the County’s US Census data, and data from the Mississippi Department of Natural Vital Statistics through 2010 population estimates demonstrate that the County has grown by approximately 2,124 persons over the decade from 2010 to 2020. The source of this 2,124-person increase is the natural increase of the population, or the net gain of births over deaths, and the migration of new residents representing a net increase residing in Hancock County.

TABLE I MIGRATION AND NATURAL INCREASE	
2010 Population	43,929
2020 Population	46,053
Change in Population	2,124
Source: US Bureau of the Census, v2020. Mississippi State Department of Health, Vital Statistics, 1990-2012.	



State of Mississippi Population Trends:

Year	Mississippi	% Change
2020	2,961,278	0.2%
2010	2,967,297	4.3%
2000	2,844,658	10.5%
1990	2,573,216	2.1%
1980	2,520,770	13.7%
1970	2,216,994	1.8%
1960	2,178,141	0.0%
1950	2,178,914	-0.2%
1940	2,183,796	8.7%
1930	2,009,821	12.2%
1920	1,790,613	-0.4%
1910	1,797,114	-

- Note: The State of Mississippi has only completed compiling statistical data collected through 2012; with a population estimate of 2,984,926 residents for 2012.

Economic Trends:

(US Census collected data, v2020)

Economy	
📌 In civilian labor force, total, percent of population age 16 years+, 2019-2023	56.6%
📌 In civilian labor force, female, percent of population age 16 years+, 2019-2023	53.7%
📌 Total accommodation and food services sales, 2017 (\$1,000) (c)	8,181,285
📌 Total health care and social assistance receipts/revenue, 2017 (\$1,000) (c)	18,752,296
📌 Total transportation and warehousing receipts/revenue, 2017 (\$1,000) (c)	5,564,028
📌 Total retail sales, 2017 (\$1,000) (c)	36,920,630
📌 Total retail sales per capita, 2017 (c)	\$12,345
Income & Poverty	
📌 Median households income (in 2023 dollars), 2019-2023	\$54,915
📌 Per capita income in past 12 months (in 2023 dollars), 2019-2023	\$30,529
📌 Persons in poverty, percent	⚠️ 18.0%


Businesses	
• Total employer establishments, 2022	60,469
• Total employment, 2022	944,580
• Total annual payroll, 2022 (\$1,000)	43,003,532
• Total employment, percent change, 2021-2022	1.5%
• Total nonemployer establishments, 2022	251,172
• All employer firms, Reference year 2017	41,265
• Men-owned employer firms, Reference year 2017	25,349
• Women-owned employer firms, Reference year 2017	6,653
• Minority-owned employer firms, Reference year 2017	4,503
• Nonminority-owned employer firms, Reference year 2017	32,683
• Veteran-owned employer firms, Reference year 2017	2,914
• Nonveteran-owned employer firms, Reference year 2017	33,214
Housing	
• Housing Units, July 1, 2023, (V2023)	1,350,642
• Owner-occupied housing unit rate, 2019-2023	69.5%
• Median value of owner-occupied housing units, 2019-2023	\$161,400
• Median selected monthly owner costs - with a mortgage, 2019-2023	\$1,353
• Median selected monthly owner costs - without a mortgage, 2019-2023	\$418
• Median gross rent, 2019-2023	\$923
• Building Permits, 2023	7,700
Families & Living Arrangements	
• Households, 2019-2023	1,131,760
• Persons per household, 2019-2023	2.52
• Living in the same house 1 year ago, percent of persons age 1 year+ , 2019-2023	88.8%
• Language other than English spoken at home, percent of persons age 5 years+, 2019-2023	4.2%

The City of Bay St. Louis 25 Year Comprehensive Plan adopted in 2024 emphasizes the importance of coordination with Hancock County as well as other neighboring communities relating to growth and the development of new infrastructure to insure that future development and redevelopment of these targeted areas are constructed in a manner that will sustain long-term growth, providing for future impact demand, and quality of life.



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Bay St. Louis ■ Diamondhead
Kiln ■ Stennis ■ Waveland

Economic Development Goals established in the City’s Comprehensive Plan

EXISTING MOBILITY NETWORK ASSESSMENT

Mobility is more than just driving from point A to point B in Bay St. Louis. It is also about kids walking to school, residents riding golf carts to Old Town, the balance between parking and urban land use, traffic calming on pedestrian oriented streets, the aesthetic quality of the public realm, accessing the beautiful marine resources, riding bikes for recreation and transportation, access to transit, means to economic development, and overall safety no matter what mode of transportation. To gauge how mobility needs are currently being met in the city, the existing transportation system was evaluated according to the following themes:

- ◆ **Connectivity**
- ◆ **Safety**
- ◆ **Walkability**
- ◆ **Policy**

Connectivity

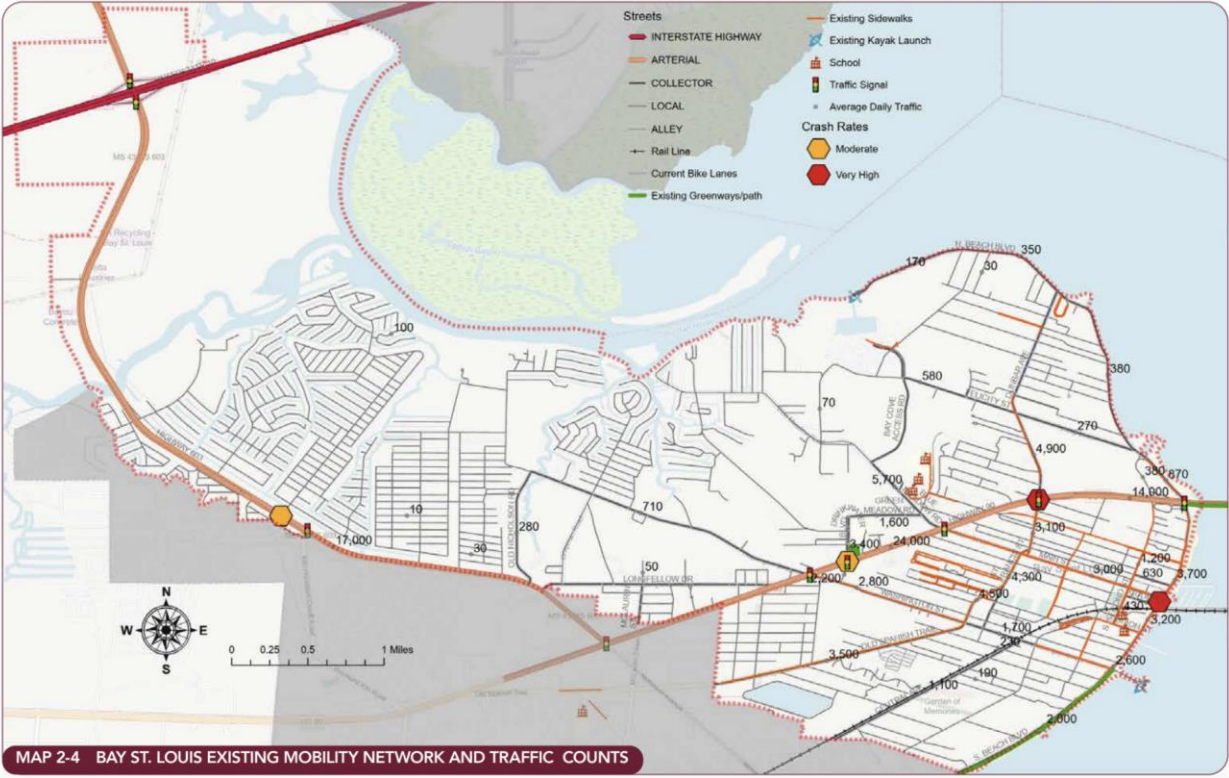
Connectivity is the measure of how well the network of city streets facilitates movement within neighborhoods and between districts. Benefits to connectivity include:

- ◆ **Better walkability and bikeability by making connections more direct, providing multiple routes, and reducing the need for multiple lane heavily trafficked corridors**
- ◆ **Traffic dispersion by enabling multiple routes to destinations instead of funneling traffic into pinch points and corridors**
- ◆ **Emergency access improvement for fire and police responders**
- ◆ **More efficient facilitation of incremental land development**

Historic neighborhoods south of Highway 90 are relatively well connected with intersection densities ranging from 200-350 intersections per square mile with the exception of the area south of Central Avenue and west of Washington Street. Neighborhoods north of Highway 90 have much lower densities of 45-200 intersections per square mile, and are completely disconnected from each other due to barriers such as boat canals and stream/wetland corridors.

The multi-lane Highway 90 corridor serves as a barrier for walking, biking and golf carts with its high vehicle speeds and traffic levels, and the lack of pedestrian signals and facilities at intersections. Recommendations are made in further chapters to facilitate better mobility connections within the city.





MAP 2-4 BAY ST. LOUIS EXISTING MOBILITY NETWORK AND TRAFFIC COUNTS

Traffic and Safety

Due to the well-connected street system in the southern half of town and the capacity of Highway 90 and Highway 603, traffic congestion levels are relatively low compared to other communities along the Gulf Coast. Based on average daily traffic counts and general facility capacity, the following primary streets register at the following volume capacity ratios, with 100% indicating full capacity:

Highway 90: 5%	Highway 603: 39%	South Beach Blvd.: 34%	Washington St: 25%
Main St.: 27%	Dunbar Ave.: 45%	Old Spanish Trail: 41%	Blue Meadow Rd.: 52%

Source: Mississippi DOT

All major and minor streets in the city operate at a relatively high level of service with these low vehicle capacity levels. However, this doesn't preclude traffic pressures at key intersections during peak flow times.

The Mississippi Gulf Coast Vision Zero Action Plan identified four locations in Bay St. Louis with high crash rates:

- ◆ Highway 90 and Dunbar Ave. with very high levels of vehicular crashes
- ◆ Highway 90 and Drinkwater Blvd. with moderate levels of vehicular crashes
- ◆ Highway 603 and Central Ave. with moderate levels of vehicular crashes
- ◆ South Beach Blvd. with very high levels of vehicle/pedestrian crashes

These are the primary crash locations in the city, but other intersections and corridors also register many vehicle to vehicle and vehicle to pedestrian/bike crashes every year. High speeds on primary corridors like Highway 90 are a primary cause of these crashes, along with lack of path and right-of-way clarity at key intersections such as Dunbar and Highway 90.



Pedestrian crashes in the heart of Old Town are a great concern and need to be addressed with specific strategies to reduce vehicle speeds and prioritize pedestrian movements and crossings. Vehicle speeding in neighborhoods was a frequent topic from public engagement activities.

Walkability

The Walk Score website gives Bay St. Louis a walkability score of 46 (out of 100) and a bikeability score of 44, with the highest values in Old Town and surrounding neighborhoods and along Highway 90. The score is not measured by the presence of sidewalks but by street connectivity, housing density, and the nearest of important destinations to walk to like grocery stores and other retail, restaurants and cafes, drug stores, and schools. Bike Score is measured by presence of bike facilities including well-connected low-traffic streets.

Even in its most walkable areas, there are issues regarding true walkability. The Old Town area has good street connectivity accessing restaurant and entertainment venues, but a lack of daily need destinations such as grocery and drug store provisions. The Highway 90 corridor provides a plethora of destinations, but lacks even basic pedestrian infrastructure along the road or at intersections. The remainder of the city suffers from the lack of both pedestrian facilities, good street connectivity, and destinations within walking distance.

The Strava heat map on the next page indicates popular walking, biking and running routes in the city and can help prioritize the planned locations for pedestrian and bicycle facilities for the future. The highest traveled routes in the city include South and North Beach Blvd., Dunbar Ave, N. 2nd St., Carroll Ave., Felicity St., Central Ave., Blaize Ave., and Third St.

Below Left: Relationship of safety and speed
Below: Bay St. Louis' Walkscore®



Policy

Initial review of city policies and standards related to mobility include the following observations:

- ◆ **Complete Streets.** The city does not have a complete street policy/resolution/ ordinance that prioritizes facilities for all users of the roadway for all modes of transportation.
- ◆ **Traffic Calming.** The city has installed traffic calming infrastructure, but the city lacks an official policy and process for residents and business owners to pursue traffic calming measures.
- ◆ **Mobility Design Standards.** City design standards for site and neighborhood development have limited requirements for infrastructure that will promote walking and biking in the city.

The Local Economy



Bay St. Louis experienced sustained growth for the last century. The median age of the population is 48.3, higher than Hancock County and the State. However, it has aged 3.2 years since 2010 as the community has solidified itself as a retiree and second home destination. These dynamics will impact future needs in terms of housing, commercial, and recreational services. New residents have brought affluence and higher home values, but Bay St. Louis continues to have a diversity of incomes. Its \$48,645 median household income is one of the lowest income levels the region. Conversely, Bay St. Louis shows the highest median home value in the region at \$273,512. The city has enjoyed continued residential development with new homes, yet also sees a disparity in affordable residential units. 43% of renters and 18% of homeowners are cost burdened.

There has been a 186% increase in STR units in just the past four years. These vacation rentals not only solidify the city as a vacation destination, but also generate significant revenue for property owners. They also impact the overall affordability of residential by taking units out of the larger housing stock, while increasing the overall cost of long-term housing.

Building permits have steadily increased over the past decade. Most have been single-family detached homes, but recently the city has seen the development of and the adaptive reuse of buildings for higher end condominiums of moderate density. Residents engaged in the planning process also stated the need for rental housing, senior housing, and multi-family.

Bay Saint Louis has a vibrant and walkable downtown with a variety of dining establishments, boutique shops and lodging. Downtown and the Depot District have seen most of the recent commercial improvements and investment. Conversely, while it is the primary commercial corridor, the Highway 90 area has a poor variety of businesses, lack of design quality, and limited vacancy. Overall, there is \$18 million in demand in retail sales in Bay St. Louis proper and \$81 million in the local trade area. Conservative demand studies show potential for as much as 168,000 square feet of new commercial space in the community.



Retail:

(Copied from The City of Bay St. Louis 25 Year Comprehensive

RETAIL

The retail analysis provides insight into the retail patterns in Bay St. Louis and the surrounding area including retail sales, consumer expenditures, opportunity gap, and projected demand growth. The retail analysis helps to identify key opportunities to capture any existing retail leakage and leverage projected growth in the market.

Retail Leakage Analysis

“Retail Leakage” refers to the difference between the retail expenditures by residents living in a particular area and the retail sales produced by the stores located in the same area. If desired products are not available within that area, consumers will travel to other places or use different methods to obtain those products. Consequently, residents are purchasing more than the stores are selling, and the dollars spent outside of the area are said to be “leaking.” This study compared leakage for Bay St. Louis, 15 and 30-minute drive times, Hancock County, and the Gulfport-Biloxi-Pascagoula Metro area. If there is leakage in a study area, it translates directly to demand.

Over the past year, stores located in Bay St. Louis recorded \$211 million in retail sales. During the same time period, residents living in the city spent \$219 million, resulting in a retail leakage of \$18 million over the year. While this amount of leakage is not a significant amount compared to other communities, the fact that local retail sales reflect the expenditures of visitors as well as locals, it suggests local demand may in fact be higher. When we expand this study to the 15-minute drive time primary trade area, there is an even greater leakage of \$81 million. Within the 30-minute regional trade area, there is in fact a retail gain, as this geography reaches to Gulfport and its retail offerings.



Figure 12: Demand by individual retail category in Bay St. Louis' 15-minute drive local trade area. Source: Arnett Muldrow and Claritas.

e Plan adopted in 2024)

Retail Capture

The 15-minute drive time represents Bay St. Louis' primary local trade area and is used to determine opportunities for growth. Bay St. Louis' local trade area experienced leakage (demand) in most categories, but key categories that show greater than \$1 million in annual demand are shown below.

Of course, Bay St. Louis cannot reasonably expect to recapture 100% of the sales leaking from its trade areas. However, it can recapture a percentage of leaking sales through strategic recruitment, economic development and marketing.

The table below illustrates the new or expanded retail space that could be supported in Bay St. Louis by capturing 15%, 25% or 33% of the leaking sales in primary trade area. The retail capture scenarios are described below:

- ◆ **15% Scenario: Capture \$8 million in leakage 31,674 square feet of retail**
- ◆ **25% Scenario: Capture \$14 million in leakage with 52,789 square feet of retail**
- ◆ **33% Scenario: Capture \$18 million in leakage with 69,682 square feet of retail**

Retail Demand Growth

The table above shows CURRENT demand, but the increasing population in Bay St. Louis and the surrounding region will generate additional retail opportunity. As part of this study, it was determined that an additional 4,000 people are expected to reside in Bay St. Louis over the next 20 years. Assuming a rate of 8-12 square feet of retail per person, this additional population and projected commercial growth will generate demand for up to 168,118 square feet of retail in Bay St. Louis.

Sales Tax Revenue

Current and future retail sales represent a major revenue stream for the City of Bay St. Louis. In the 2022 fiscal year alone, the City saw \$171.5 million in gross sales (retail, food/accommodations) resulting in \$2.24 million in taxes diverted back to Bay St. Louis. Both sales and tax generated have

increased significantly over the past decade.

There is the potential, if desired, for the City to create additional revenue related to local sales taxes. Other communities have instituted an additional Tourism & Economic Development Tax (T&ED) from 1% to 3% of sales tax. These resources could fund business support, park development and maintenance, marketing, events, a Main Street Association, etc. Such a policy would require both special legislation by the Mississippi Legislature and local adoption.



Figure 13: Gross Sales in Bay St. Louis 2014 -2022. Source: City of Bay Saint Louis.

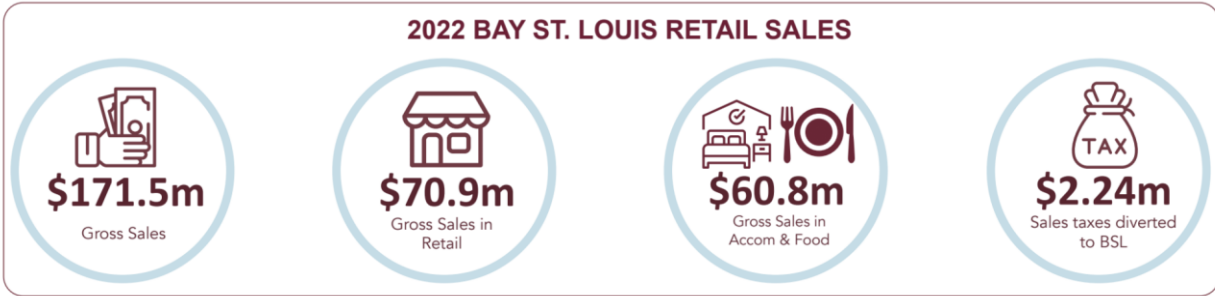


Table 2.4 - Consumer Expenditures, Retail Sales, and Retail Leakage/Gain

	Bay St Louis	15-Min Drivetime	Hancock County	30-Min Drivetime	Gulfport-Biloxi-Pas Metro
Consumer Expenditures	\$211 M	\$467 M	\$561M	\$1.8 B	\$8.1 B
Retail Sales	\$219 M	\$548 M	\$980 M	\$1.77 B	\$7.6 B
Retail Leakage/ (Gain)	\$18 M	\$81 M	\$419 M	(\$32 M)	(\$533 M)

Source: Arnett-Muldrow; Claritas

Table 2.5 - Retail Capture Scenarios

Capture Scenario	Low Scenario (8 sf/ person)	Medium Scenario (10 sf/ person)	High Scenario (12 sf/ person)
Capture Existing Leakage	31,674	52,789	69,682
Capture 5-year Demand Growth	22,925	38,209	50,436
Additional 4,000 Population	32,000	40,000	48,000
Total	86,599	130,998	168,118

Source: Arnett-Muldrow

Table 2.6 - Retail Capture Allocations by Retailer

Select Retail Categories	Calculated Capture (sf) 15% Scenario 31,674 (sf)	Calculated Capture (sf) 25% Scenario 52,789 (sf)	Calculated Capture (sf) 33% Scenario 69,682 (sf)
Furniture Stores	3,303	5,505	7,267
Home Furnishing Stores	2,090	3,483	4,598
Household Appliances Stores	1,000	1,666	2,199
Electronics Stores	1,327	2,212	2,919
Grocery Stores	9,712	16,187	21,367
Clothing Stores	2,554	4,256	5,618
Jewelry Stores	522	869	1,147
Luggage & Leather Goods Stores	1,011	1,685	2,224
Sporting Goods Stores	1,376	2,294	3,028
Hobby, Toys and Games Stores	504	840	1,109
Book Stores	509	848	1,119
Florists	255	424	560
Gift, Novelty, Souvenir Stores	172	287	379
Food Service & Drinking Places	7,340	12,233	16,147

Source: Arnett-Muldrow; Claritas

Employment:

(Copied from The City of Bay St. Louis 25 Year Comprehensive Plan adopted in 2024)

EMPLOYMENT

In 2018, there were approximately 3,780 jobs in the City of Bay St. Louis. The largest sectors included Accommodations and Food (30%), Health Care/ Social Assistance (21%) and Public Administration (11%).

Commuting patterns indicate that Bay St. Louis is a net importer of jobs, meaning more workers are commuting into the city for work rather than residents who commute out. Most of these in-commuters reside in the remainder of Hancock County at 53%, and Harrison County at 28%.

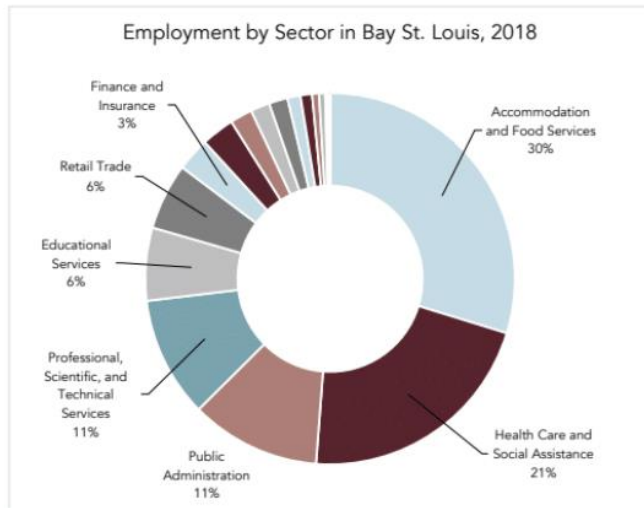


Figure 14: Employment by Sector in Bay St. Louis 2018. Source: US Census On the Map.



Figure 16: Commuting patterns. Source; US Census On the Map.

Income:

(Copied from The City of Bay St. Louis 25 Year Comprehensive Plan adopted in 2024)

Income

Bay St. Louis' median household income is \$48,645. While this is among one of the lower income levels within the larger region, it represents a significant increase of the \$40,515 in 2010. It is estimated that 20% of families in the city are below poverty, compared to 12.4% for Hancock County and 14.9% for Mississippi.

Housing:

(Copied from The City of Bay St. Louis 25 Year Comprehensive Plan adopted in 2024)

HOUSING**Housing Type and Occupancy**

In 2023, it is estimated that 78 percent of Bay St. Louis' housing stock is made up of single-family dwellings with the remaining 22 percent being multifamily or manufactured homes. Bay St. Louis' housing units are 58 percent owner-occupied and 24 percent renter occupied. 28 percent of units are limited use or vacant. Bay St. Louis has a younger housing stock with 42 percent of all units being built since 2000, likely due to the devastation brought by Hurricane Katrina in 2005.

Demand for housing is high, and Bay St. Louis is experiencing an opportunity for increased densities in appropriate locations. An example of this is the new Farragut Lofts redevelopment units currently under construction, as well as adaptively reused 315 Main Street property. Even with significant demand of this type, the majority of new housing will likely continue to be single-family detached units.

Housing Value and Average Rent

With its newer housing stock and increased demand, home values in Bay St. Louis are higher than Hancock County and much of the Gulf Coast region with median value estimated to be \$273,512 in 2023. The real estate service company Zillow is an indicator of the current real estate market. It shows a similar value of \$223,108. This represents an 8 percent year-over-year increase in home value from the year 2021. Of Bay St. Louis' housing units, 34 percent are valued between \$200,000 and \$300,000. Average rents in Bay St. Louis are \$960 for a 1-bedroom unit and \$1,130 for a 2-bedroom unit.

Housing Affordability

Bay St. Louis is seeing increased demand and higher cost of housing as compared to the region. If a person or family spends more than 30% of their income on housing, they are considered to be cost-burdened. In Bay St. Louis, approximately 43 percent of renters and 18 percent of homeowners meet this threshold and can be considered cost burdened.

Short Term Rentals in Bay St. Louis

Bay St. Louis is also seeing an increased usage of its housing as short-term rental property (STR). Renting homes on platforms such as Airbnb and VRBO has been increasing significantly over the past several years. These STR properties provide non-traditional lodging that meets demand not currently met in the lodging market, while also generating income for local property owners. STRs have the potential to impact the community through perceptions of safety, increased need for parking, and garbage collection. In Bay St. Louis, STRs also have a direct impact on both housing availability and affordability.

The number of STR listings, their daily revenue, and occupancy vary over the course of the year with July typically being the annual peak and February being the slowest month. In June of 2023, STR units generated over \$2 million in market revenue for that month. Within the past twelve months, short term rentals in Bay St. Louis had an average daily rate of \$223 and a median monthly revenue of \$2,688. The highest performing property in terms of revenue generated \$135,800 in 2022. While this property may be considered an exception, (waterfront, three-bedroom home, and amenities) and overall income varies by tier, STR units generate an average of \$32,256 per year in Bay St. Louis.

The median home value in Bay St. Louis in 2017 was \$273,512 in 2023. This is well above the Hancock County median of \$47,543, and while housing in the County is less expensive than in the City, there is a relative lack of affordable housing along the Gulf Coast.

Bay St. Louis’s housing trends follow many of the trends that have occurred nationally. The fast pace of residential construction has fueled concerns about “sprawl.” In the midst of prosperity, the home ownership gap between whites and minorities has not narrowed. Very low-income households still lack adequate, affordable housing at a time when losses of subsidized units are rising. National figures indicate that record numbers of very low-income households are devoting more than half their incomes for housing. Renting remains the only option for many who are either unable to qualify for a mortgage loan or to cover the costs associated with buying a home. For many others, though, renting is an attractive lifestyle as well as a prudent financial choice. It is an especially appealing option for people who expect to move again within a few years because they can avoid the steep transaction costs associated with buying and selling a home. It also may be the first independent step for younger persons in making a long-term commitment to a community; as trends have shown, initially they may rent, but as they become settled, they seek to purchase property in the community where they have been living.



Home sites located within Hancock County

RIVERFRONT FOCUS AREA DESIGN

The Riverfront Focus area was chosen to illustrate best development practice for this environmentally sensitive area adjacent to the Jourdan River frontage. The selected site, as seen in the aerial image to the right, consists of 180 acres and is severely impacted by the flood zone. The conceptual design employs the principles of conservation development to preserve over 75 percent of the site.

The site features a mixture of housing types and includes the amenities of a marina, pedestrian pathways, and retail and restaurant space, a center green space, all in the context of a walkable neighborhood.

Right: Diagrammatic concept of Riverfront Trail neighborhood
 Below: Riverfront Trail model
 Opposite Page: Overall concept Riverfront Trails



Best Practices for Conservation Neighborhood Design

- 1 Minimize environmental footprint by prioritizing mixed-use and clustering of dwellings.
- 2 Integrate green infrastructure, such as parks and tree-lined streets, to enhance biodiversity and provide ecosystem services, while implementing low impact stormwater management.
- 3 Encourage energy-efficient building design and construction, utilizing materials with high thermal performance, and promote the use of renewable energy sources like solar panels and wind turbines.
- 4 Foster walkability and bike-friendly design through well-connected sidewalks and bike paths, promoting alternative transportation methods to reduce dependence on cars.
- 5 Include sustainable infrastructure, including energy-efficient street lighting and waste management systems, and explore smart technologies to optimize resource use.
- 6 Identify and protect the natural features of wetlands and wildlife habitats, incorporating them into the neighborhood design to enhance biodiversity.
- 7 Implement education programs to raise awareness about conservation practices within the community, providing resources and incentives for sustainable living practices.
- 8 Establish water access and marine storage facilities as an integral component of the community.



Riverfront Trails Development Data	
Acres	180
Acres Developed	40
Residential Single Family	140
Townhomes	38
Retail/Restaurant	6k to 8k
Density	1 dwelling/acre

RIVERFRONT TRAILS IMPLEMENTATION NOTES

Implementation for the Riverfront Placetype of the character and design concept illustrated in the Riverfront Trails neighborhood requires a well crafted zoning district design to achieve the best practices for conservation development listed on the previous page.

Such a district will allow for the significant clustering of dwelling and a mix of lot and housing types. Included will be requirements for the intensive protection of sensitive environmental features harmonized with access to the Jourdan River and natural land areas. The illustrations at right provide precedent concepts for the intended character of this development type.

*Lower Right and Above: Conceptual riverfront development
Below: Conceptual riverfront development*



*Above and Below: Precedent image of riverfront development
Upper Right and Lower right: Riverfront development concept*



SUBURBAN NEIGHBORHOODS

Estate neighborhoods frequently serve as a transition from the least dense agricultural and rural environments to a more dense urban setting. These neighborhoods typically consist of relatively uniform lot patterns of similar concentrations of lot sizes. The best designs for these neighborhoods strategically incorporate natural features into their layouts and provide for formalized open spaces and neighborhood parks as amenities. Ideally, existing landscapes are preserved and supplemented.

The development pattern of suburban neighborhoods is commonly defined by two basic design configurations. One features curvilinear streets, cul-de-sacs and common areas. A more traditional pattern features gridded street patterns with a high intersection density. Suburban neighborhoods are predominantly residential in nature, but may incorporate civic and institutional uses. Typically landscapes and yards, rather than buildings, are used to frame these neighborhoods.

A range of lot sizes may be present creating opportunities for diverse housing choices. The character of these residential areas is best promoted by preserving or adding to existing landscapes and maintaining a balance between buildings and open space.

*Lower Left and Above: Estate Neighborhood home examples
Lower Right: Aerial image of an Estate Neighborhood
Opposite Page: Suburban Placetype Map*





Planned Characteristics

<i>Primary Land Uses</i>	Single-family detached residential
<i>Secondary Uses</i>	Institutional Assembly Civic and Institutional
<i>Intensity Range</i>	2 to 5 dwelling units/acre
<i>Development Policies</i>	Tree canopy preservation Stormwater managed at neighborhood/project scale and in combination with low impact design Underground utilities

Essential Building Character

<i>Building Placement</i>	Building facades set back from the street Accessory buildings in the rear yard Recessed garages
<i>Frontage and Height</i>	Dwellings ideally have porches At least one entrance faces the primary street, 3 stories max

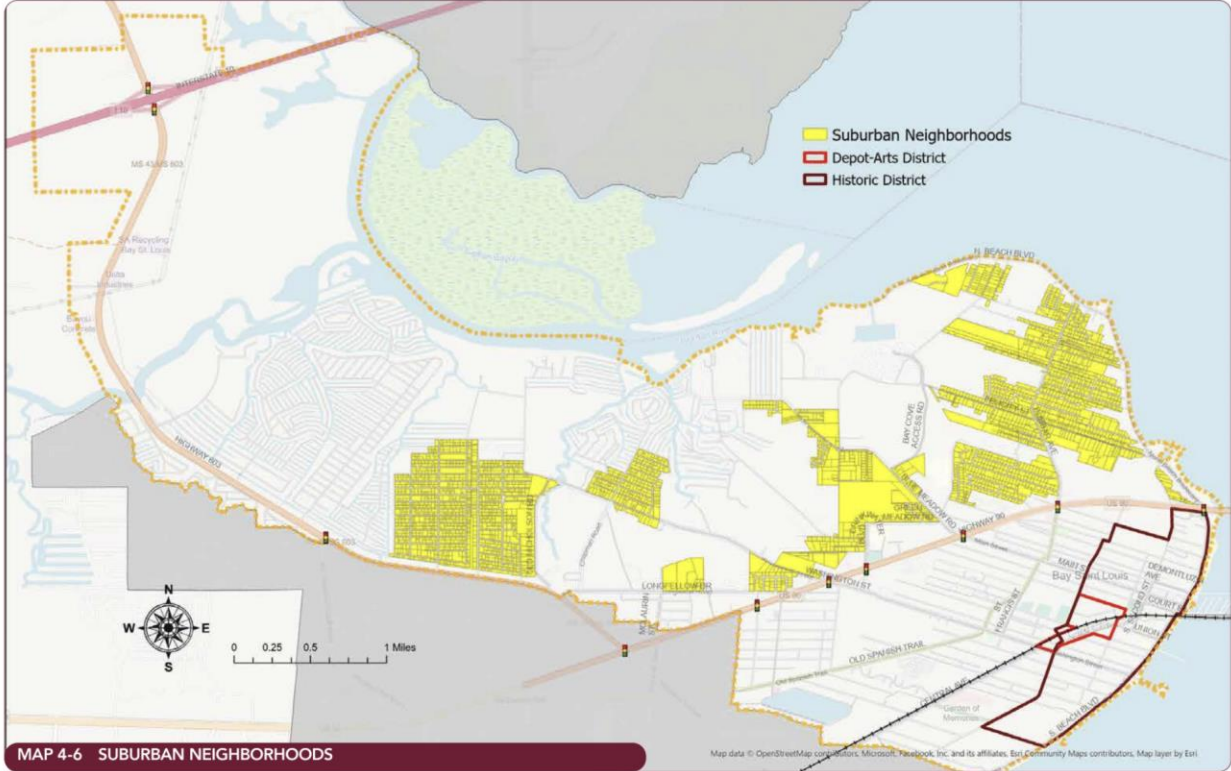
Essential Site Character

<i>Parking and Access</i>	Garages recessed or placed to the rear of the lot, individual drives
<i>Landscape</i>	Street trees, natural or constructed separation from nearby commercial areas

Planned Characteristics

<i>Amenities</i>	10 percent open space. Recreation areas must be provided unless within 1/2 mile of a public park. Bike/ped connectivity to parks and activity centers.
Mobility (see Future Mobility Network)	
<i>Street Types</i>	Local, Collector
<i>Bike/Pedestrian</i>	Bikeways, greenways, trails





MAP 4-6 SUBURBAN NEIGHBORHOODS

OLD TOWN NEIGHBORHOODS

Old Town Neighborhoods form the historic residential heart of Bay St. Louis and surround Bay St. Louis' Old Town Core. The neighborhoods host Bay St. Louis' iconic residential architecture being distinctively coastal in character and forming a significant part of Bay Saint Louis' community identity. These neighborhoods are oriented to predominately single family dwellings and are characterized by mature landscapes and the community's original streets dating to Bay St. Louis' original settlement.

Old Town Neighborhoods are the traditional heart of Bay Saint Louis and embody the community symbolically, culturally, and historically. The neighborhoods are pedestrian oriented and within easy walking distance of the Old Town Core. Vehicular traffic is secondary to the fundamental pedestrian and human scale of the area.

*Lower Right, Above and Below: Old Town Neighborhood Home
Opposite Page: Old Town Neighborhoods Placetype Map*





Planned Characteristics

<i>Primary Land Uses</i>	Single-unit residential Neighborhood commercial at limited corners Single-unit attached residential at limited locations
<i>Secondary Uses</i>	Institutional
<i>Intensity Range</i>	Residential densities of 2 to 6 units per acre Tree canopy mitigation Stormwater management with low impact design (bio-retention, permeable surfaces, infiltration areas) Site plan and design review Historic preservation
<i>Development Policies</i>	

Essential Building Character

<i>Building Placement</i>	Residential buildings have a variety of setbacks
<i>Frontage and Height</i>	Primary entrances face the street 3 stories

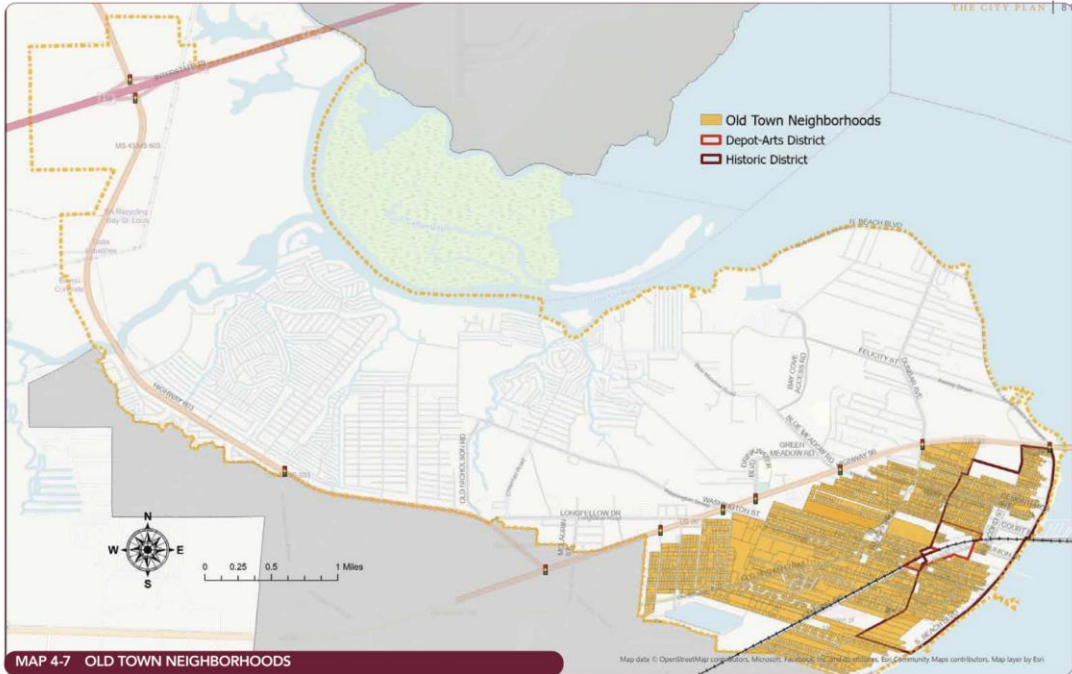
Essential Site Character

<i>Parking and Access</i>	Typical single family parking configurations, recessed parking preferred, Alley access on smaller lots
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Planned Characteristics

<i>Landscape</i>	Street trees where feasible and may include green infrastructure stormwater features
<i>Amenities</i>	Public parks and recreations
Mobility (see Future Mobility Network)	
<i>Street Types</i>	Local, Collector, Arterial Bikeways, greenways, trails Bike/ped connectivity to parks and adjacent places
<i>Bike/Pedestrian</i>	





OLD TOWN NEIGHBORHOODS FOCUS AREA DESIGN (PRESERVATION AND INFILL)

The Old Town focus area neighborhood was chosen to illustrate best development practices for infill development for the historic heart of the City. Design is intended to illustrate mixed-use infill development on a scale compatible with its context.

Infill development in historic neighborhoods should reinforce desirable character where it exists and create it where it does not. Infill best practices balance growth and preservation. Selected best practices are shown in the table at right.

Lower Right and Above: Neighborhood infill development concept
Below: Old Town neighborhood infill from above
Opposite Page: Development concepts





Above: Example of mixed-use infill design
Below: Street perspective of Old Town mixed-use infill



Best Practices for Old Town Neighborhood Preservation and Infill	
1	Preservation of Architectural Character: Ensure that new developments respect and complement the existing architectural style and character harmonizing with the scale, materials, and detailing of the area.
2	Adaptive Reuse: Prioritize the adaptive reuse of existing structures rather than demolition.
3	Scale and Massing: Alignment, but not necessarily duplication, of building and lot scale and development intensity, avoiding overly large or out-of-place structures and reserving the greatest intensity for corners.
4	Pedestrian-Friendly Infrastructure: Enhance the pedestrian experience by incorporating features like crosswalks, street furniture, street lighting, and green spaces.
5	Sustainability Practices: Integrate energy-efficient technologies and green building techniques while respecting neighborhood context. Use eco-friendly landscaping, stormwater management, and energy-efficient systems to minimize the environmental impact.
6	Infill Design Guidelines: Establish infill design guidelines to achieve infill design goals.
7	Parking Management Strategies: Ensure parking is design to side and rear of buildings and featuring building architecture prominently.



Top: Attached mixed-use building example
Middle: Stand alone mixed-use building example
Bottom: Cottage Court housing as potential infill



MIXED RESIDENTIAL AREAS

Mixed Residential Areas are similar in design characteristics to suburban neighborhoods. Locationally, however, these areas tend to be positioned in the interior of the community in established areas, rather than at the community edge.

These areas offer far greater housing type and diversity, ranging from small lot cottages to townhouses to low intensity multiple-family units. Unit density is substantially increased from the Suburban Neighborhood type. Established mixed residential areas in Bay St. Louis are commonly undergoing neighborhood transition or redevelopment.

Secondary uses of institutions and assembly occur frequently in mixed residential areas. Some limited neighborhood scale commercial activity may be present and the development form may be mixed use in character and function. The need for open spaces and parks in mixed residential areas to serve higher densities is pronounced and is to be prioritized. Mobility options include a variety of street patterns, but grid patterns typically dominate to allow for efficiency in lot configurations.

Upper Right, Lower Right and Below:
Mixed residential examples
Opposite Page: Mixed Residential
Placetype Map





Planned Characteristics

Primary Land Uses	Single-unit detached residential Single-unit attached residential Multiple-family residential
Secondary Uses	Mixed-use at limited locations Institutional Assembly
Intensity Range	5 to 8 dwelling units/acre
Development Policies	Slope and canopy preservation Stormwater managed at neighborhood/project scale with low impact design Underground utilities Site plan review

Essential Building Character

Building Placement	Buildings placed nearer to the street
Frontage and Height	Ideally dwellings have porches or balconies 3 stories max

Essential Site Character

Parking and Access	Alley access is desirable
Landscape	Street trees, natural or constructed separation from nearby commercial areas, landscape parking

Planned Characteristics

Amenities	10 to 20% open space and recreation areas if not within 1/4 mile of a city park.
Mobility (see Future Mobility Network)	
Street Types	Local, Collector
Bike/Pedestrian	Bikeways, greenways, trails, connectivity to parks and centers.



Section 3 – Specific Redevelopment Plan Findings, Goals and Objectives

The adopted goals and strategies below are taken from the City of Bay St. Louis’s General Development Plan, including statements adopted in the 2024 Comprehensive Plan as a result of various studies, interviews, and findings. They identify the need for new development and redevelopment in many instances, and these areas have been highlighted to illustrate the policy and direction of the community related to future growth and quality of life for its citizens. The Findings Goals and Objectives are abridged for clarity and those that address the need for redevelopment are emphasized.

Findings, Goals, and Objectives

Findings

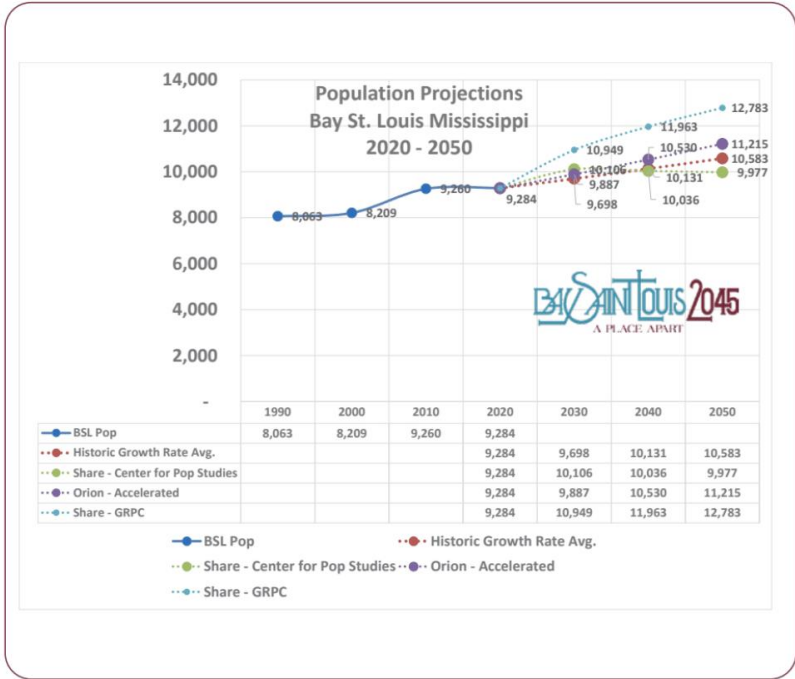
In order to guide the planning process and assure its validity and productivity for the City of Bay St. Louis, the Mayor and City Council reviewed various background studies and their conclusions, including supporting data and analysis. From the data and analysis, and through a series of meetings and discussions with residents of the community, the Bay St. Louis Mayor and City Council, along with the support of the City of Bay St. Louis Planning Commission, have made findings of fact with regard to the community’s developmental condition and set forth development-related goals and objectives.

These Findings and Goals have been developed for the specific purpose of providing direction to the final plan product. These findings and goals follow, with the **Findings** set forth first and **Goals and Objectives** following.

The City of Bay St. Louis Mayor and City Council finds as follows:

Findings Regarding Population

1. The Mayor and City Council finds that Bay St. Louis has seen moderate and sustained growth since at least 1980. The population jumped 12.8% between 2000 and 2010, but slowed significantly to the current estimated population of 9,353. Between 2010 and 2020, Bay St. Louis grew by just 0.3% while the Gulfport Metro saw 7.1% growth. It’s important to note that the population within a 15-minute drive of Bay St. Louis grew by over 10% during the same time period. This geography represents the city’s primary local trade area and continues to see healthy population growth.
2. The Mayor and City Council finds that population projections show probable growth based on historic growth rate and other factors.
3. The Mayor and City Council finds that larger population projections are directly correlated to the degree that economic activity is stimulated by strategic development.



Findings Regarding Economic Analysis

1. Medical, education, utilities, retail trade, and manufacturing jobs, trade, and services provide the bulk of the economic activity for the residents living in the City of Bay St. Louis and Hancock County.
2. The Mayor and City Council finds that there are ample medical facilities and that this component of the general Mississippi economy is thriving.
3. The Mayor and City Council finds that local businesses recorded \$211 million in retail sales in 2023.
4. The Mayor and City Council finds that Bay St. Louis’ median household income is \$48,645. While this is among one of the lower income levels within the larger region, it represents a significant increase of the \$40,515 in 2010. It is estimated that 20% of families in the city are below poverty, compared to 12.4% for Hancock County and 14.9% for Mississippi.
5. The county serves as a "sub-regional" center for primary industrial related to NASA operations, commercial and residential construction, commercial fishing, and has a very narrow spectrum of types of active industry and production, with the exception of NASA and its related service providers.

Findings Regarding Land Use

1. The Mayor and City Council finds that manufactured housing is scattered outside of the City throughout the county and is not adequately regulated for location and aesthetic impact.
2. The Mayor and City Council finds that the commercial retail areas contribute to part of the major economic hub for Bay St. Louis and that the area is active, with many festivals, promotions, and entertainment and that some efforts have been made to improve the viability and attractiveness of this area.

3. The Mayor and City Council finds that there is vacant land in all areas of the community, but planned expansion would offer a greater variety of growth opportunities.
4. The Mayor and City Council finds that current land use development tools are in need of further refinement to accommodate developer's ability to develop mix-use developments, manage future growth relative to current building practices, primarily in terms of general planning, buffering, zoning, land-use, landscaping, expansion of City utilities, and a variety of other supporting development categories.
5. The Mayor and City Council finds that the City and County is well served by its parks, recreation, and proposed greenway systems that will enhance the recreational land uses.

Findings Regarding Transportation

1. The Mayor and City Council finds that the local transportation network is currently at capacity in many areas, particularly at the main entrance off of the I-10 exit and on other corridors struggling to meet current demands, and will likely NOT meet future demands based on current growth trends without additional resources.
2. The Mayor and City Council finds that I-10 has impacted, and will continue to impact development patterns, with migration of development moving towards this corridor to meet market and traffic patterns.
3. The Mayor and City Council finds that transportation options are primarily limited to automobile, and golf carts in the City of Bay St. Louis.
4. The Mayor and City Council finds that pedestrian facilities are lacking and should be provided through any new developments particularly in the community commercial centers. The walkability score is 46 out of 100 and the bike score is 44 out of 100.
5. The Mayor and City Council finds a strong compelling need to have the entrances from I-10, into the economic centers in the community to be improved, including: defined street patterns, improved zoning standards, setback requirements, signage, lighting, and landscaping.

Findings Regarding Housing

1. The Mayor and City Council finds that in 2023, it is estimated that 78 percent of Bay St. Louis' housing stock is made up of single-family dwellings with the remaining 22 percent being multifamily or manufactured homes. Bay St. Louis' housing units are 58 percent owner-occupied and 24 percent renter occupied. 28 percent of units are limited use or vacant.
2. The Mayor and City Council finds that Bay St. Louis has a younger housing stock with 42 percent of all units being built since 2000, likely due to the devastation brought by Hurricane Katrina in 2005. Demand for housing is high, and Bay St. Louis is experiencing an opportunity for increased densities in appropriate locations.
3. The Mayor and City Council finds that Bay St. Louis is seeing increased demand and higher cost of housing as compared to the region. If a person or family spends more than 30% of their income on housing, they are considered to be cost-burdened. In Bay St. Louis, approximately 43 percent of renters and 18 percent of homeowners meet this threshold and can be considered cost burdened.
4. The Mayor and City Council finds that in June of 2023, there were 472 active STR listings within the Bay St. Louis zip code. This is a 43% increase since June of 2022 and a 186% increase since June of 2019, illustrating that the local supply of STR is increasing significantly. Annually, STR listings are occupied 55% of the time. More importantly, over half (56%) are available for rent full-time, meaning they aren't in the local housing stock. In order to more directly manage these properties, the City adopted

in August 2024 a STR ordinance related to licensing, zoning standards and other policies, supporting increased tax revenues.

Findings Regarding Capital Facilities

1. The Mayor and City Council finds that sewer and water services, provided by the Bay St. Louis water and sewer department need to be expanded to accommodate the various areas targeted for development for future growth opportunities.
 - a. Bay St. Louis' water is supplied from the Gulf Coast aquifer system. Water is accessed and distributed by a system consisting of four ground wells and two elevated tanks. The system serves about 4,000 residential accounts and 350 commercial accounts. This system is nearing capacity and upgrades are being planned currently. The Harry Street well was installed in 1982. The city is planning a new well to be added and a new storage tank.
 - b. Bay St. Louis maintains the sewer system and its 44 pump stations. There are no current capacity issues with this system. However, the system is aging.
 - c. Bay St. Louis supplies natural gas to 1,100 customers through its system of an underground distribution system. Interest was expressed in expanding the system and upgrading metering technology. The city is currently upgrading the gas meter reading system to cellular.
2. The Mayor and City Council finds that the existing electrical and communication utility facilities that currently provide services are generally adequate to support desired growth.

Findings Regarding Community Appearance and Design

1. The Mayor and City Council finds that the underlying form of the community is attractive by reason of mature landscapes, its relationship to the Mississippi Sound, the recreational facilities and notable buildings, and that these areas are worthy of protection.
2. The Mayor and City Council finds that the commercial and residential areas are in sound condition but special development efforts such as restrictive zoning ordinances, protection of the rights of way, treatment of alley areas, placing utilities underground, parking and traffic control, and enhancing its attractiveness are still needed.
3. The Mayor and City Council finds that sign control, lighting and landscaping would enhance the appearance of the various commercial corridors particularly at the entrance to the community and the commercial corridors that feed from the entrance, which all act as main transportation corridors for the community.

Goals and Objectives

The Mayor and City Council, after examining the background analysis, making findings (as reflected above) of fact regarding current developments in the City of Bay St. Louis, and being duly charged by the State of Mississippi; and through the action of its resident electorate as the governing body of the City of Bay St. Louis, set forth the following planning goals and principles.

The goals and principles are employed in developing the specific provisions of the City of Bay St. Louis's General Development Plan, as well as the City's Comprehensive Plan. These goals and principles should be used in the day-to-day development decisions of the community. These goals also reflect those of various Hancock County Strategic Plans, and as coordinated by the Mississippi Development Authority in its examination of the

City's assets and through the various grants offered and administered by the State of Mississippi.

Goals Related to General Features

1. The natural beauty and sensitive environmental features of the City of Bay St. Louis and its community will be protected.

Goals Related to Population

1. The City of Bay St. Louis will pursue policies that are designed to produce and encourage moderate population increases by:
 - a. Providing employment opportunities for working age people and recreational, cultural, and medical amenities for retirees

Goals Related to Economy and Economic Development

1. The City will facilitate the development of vibrant and thriving economically diversified developments by making the community a destination of choice for visitors, retirees, and families.
2. The Mayor and City Council will continue to work to make the City of Bay St. Louis a destination for tourists by promoting its location near the Mississippi coastal area, and through its geographic advantages, low cost of living, and natural recreational facilities and opportunities.
 - a. Develop signage, and attractive well engineered' rights of way to direct traffic off of Interstate 10, into the community and commercial centers and continue to promote development of other commercial areas, based on market demand.
 - b. Identify existing architecturally significant buildings and sites in the community and seek to preserve a historic community atmosphere.
 - c. Promote expansion of resort related developments, accommodation's, tourist attractions, RV facilities, water related recreation, and multi-family commercial housing developments that support tourism.
 - d. Promote private sector development at Exit 13 off of I-10 (Highway 603) to attract more transient traffic, commerce and tourism into the city's economic center.
 - e. Promote and further enhance the Highway 90 corridor coming from both the east and west into Bay St. Louis as another opportunity to increase transient traffic, tourism, and commerce into the city's economic center to increase retail sales and support local business.
3. To make the City of Bay St. Louis a quality desired location for young people to live who are working in local industries and those residents that commute into other nearby trade areas for work.
4. Provide adequate amounts of industrial development land.
 - a. Promote the expansion of existing local manufacturing industries.
 - b. Promote the development of retail and other economic activities of the commercial areas to support desired quality of life, businesses, residential developments, and quality of life activities.
 - c. Support, promote, and improve facilities for the local school district to attract young families to migrate into the local community.

- d. Encourage the development of affordable, quality housing that fosters long-term growth.
- e. Promote private development of mixed-use housing/commercial developments, and the development of commercial housing units with multiple tenants, specialty housing targeting short-term rentals, as well as long-term efficient housing stock.

Goals Related to Land Use

The City will

1. promote the construction of a variety of home sizes and types that are affordable in the local economy;
2. provide for all legitimate uses of land through good established planning practices;
3. implement ordinance measures that will preserve residential areas through the appropriate treatment of manufactured housing (dedicated developments allowed under the local ordinances and zoning regulations) and multiple family housing;
4. implement measures that will ensure the appropriate location for industrial uses;
5. encourage commercial developments and preservation of architecturally significant structures;
6. promote the development and maintenance of parks and public spaces;
7. provide for the growth and promote expansion of existing industry; and,
8. establish clear and compelling gateways especially along the Highway 90 corridor where appropriate to encourage additional growth into the current retail trade areas.

Goals Related to Transportation

The City will

1. seek to provide a well-maintained and efficient transportation system characterized by ease of access and circulation along with safe engineered designs;
2. assure appropriate maintenance of local transportation routes;
3. ensure new development is appropriately related to transportation networks;
4. ensure adequate carrying capacities of existing and proposed transportation facilities; and
5. promote the widening of the entrance corridor from the Interstate 10 exits ramps, landscaping, lighting, and modern traffic signalization, and improved frontage roads entering the community corridors.

Goals Related to Housing

The City will

1. promote the construction of a variety of home sizes and types that are affordable in the local economy;
2. review the need for assisted living areas throughout the community;
3. pursue the development of assisted living areas if needed and desired;
4. ensure that a suitable housing stock exists for all income groups; and,
5. continue to update the City's construction codes, encourage the community to adopt, and continue to update practices under current international building and maintenance codes, standards, zoning ordinances, continue supporting community planning efforts, and sound land-use practices.

Goals Related to Community Appearance and Design

The City will

1. promote the development of visually pleasing commercial areas through the adoption of signage controls, lighting, landscaping, additional zoning ordinances, land-use planning and establish well defined historic districts with supporting ordinances to ensure that these properties are protected for future generations;
2. create simple design guidelines such as limited exposed metal surfaces, limited manufactured housing uses, as well as requiring masonry facades, setbacks and landscaping; and,
3. provide and enforce landscape ordinances with buffering provisions for new development.

Goals Related to Capital Facilities

The City will

1. continue to provide adequate police and fire protection for growing population needs;
2. provide or make available utilities, and public services to a growing community to support long-term planning needs;
3. work towards the construction of a new water well and associated water tower to meet current and future consumer demand; and,
4. Expand, enhance and update existing sewer, water, and gas infrastructure to meet current demand and provide for the anticipated growth of the population.



Section 4 – Additional Redevelopment Plan Goals and Objectives

The following goals and objectives of the Redevelopment Plan have been developed by the City of Bay St. Louis's Mayor and City Council to expand upon the General Development Plan goals related to new developments, and redevelopment projects to emphasize, with high priority economic development activity in the City of Bay St. Louis.

- The City of Bay St. Louis will work to create and partner with economic development projects that create good jobs and strengthen the tax base of the community.
- The City will coordinate efforts with other economic development entities and with the local community to assist in financing of public infrastructure improvements that will result in private capital investment for economic development projects by developing vacant land.
- The City will encourage development and redevelopment projects that will strengthen and increase retail sales activity within the local community.
- The City will encourage development and redevelopment projects that will increase jobs in the City of Bay St. Louis.
- The City of Bay St. Louis will ensure that projects undertaken within the community are undertaken in accordance with codes, ordinances, and development plans that are in effect, or that may be adopted in the future.

Section 5 – Redevelopment Planning Policies

The adopted goals and strategies below are taken from the City of Bay St. Louis’s Comprehensive Plan adopted in 2024 and are further supported by the General Development Plan as described herein. They identify redevelopment in many instances and these areas have been highlighted to illustrate the policy direction of the local community related to new developments and redevelopment. The Findings Goals and Objectives are abridged for clarity and those that address the need for redevelopment are highlighted, made bold and enlarged.

The City of Bay St. Louis General Development Plan

Transitional residential areas are typically characterized by older 1960’s -1970’s era housing stock, deferred maintenance, conversions from single-family uses to more intensive uses, and the introduction of incompatible uses. Public and private efforts should focus on upgrading the condition of those residential areas that are in transition, particularly in the outlying areas of the community. Specific efforts including area residents in these planning efforts should be made.

- (1) Transitional residential areas will be protected from disruptive uses such as encroaching industrial and commercial uses.
- (2) Improvement of property through reconstruction and/or an extensive maintenance program by individual owners is encouraged. When necessary, the City shall utilize strict Codes enforcement to protect and revitalize transitional residential areas.
- (3) Vacant land adjoining transitional areas or occupied land to be redeveloped should be utilized for residential, public, or semi-public development unless specific revitalization plans adopted by the local community dictate otherwise. Revitalization plans may consist of a neighborhood plan, historic overlay districts, or an economic redevelopment plan that clearly defines its intent.

Old Town and the Train Depot Districts

Bay St. Louis possesses something that many other competing suburban growth-oriented cities lack, namely a resort driven history that has had very effective zoning and planning through an active governing body made up by the residents. The community has recently (July 2024) developed new goals and objectives focused on growth, quality of life, and expansion.

The general objectives of the policies included for this category is to manage these important areas so that future redevelopment and growth can be permitted, and the transition from residential to non-residential uses (or vice versa) can be accommodated with the least amount of conflict between obsolete and expanding uses. The following policies are recommended for these historic districts land use category.

Appropriate Uses: The principal uses intended for the areas include retail and commercial businesses along the major roadways including personal, professional services, retail, food services and government offices in the commercial trade area and at its fringes and low and medium density residential uses (single-family detached, attached, and townhouses/condominiums) just off the main roadway corridors.

The commercial area of Bay St. Louis serves as an important asset that the community must preserve and enhance. This Plan supports the development and redevelopment of the area in a historically appropriate way.

General Development Policies for Industrial and Employment Areas

The City of Bay St. Louis's prospects for industrial economic development in the next 20 years will, in many cases, evolve from the existing strengths of the local community. New non-agricultural employment for the county is projected to increase over the planning period, and two land use categories should be created to accommodate new industrial development. The Industrial Category is a broad land use category envisioned for areas where the primary types of existing or planned development are industrial in character, such as manufacturing, bulk storage (indoors and outdoors), and heavy distribution activities like truck terminals. Overall floor area ratio recommended in this land use classification is 1.00 (one square foot of building floor area to every one square foot of site area).

Industrial activities generally place a heavy demand on local resources, have the greatest impact on the surrounding environment, and characteristically have less flexibility in locational choice than do other types of economic activities. Therefore, land use policies that promote the efficient utilization of industrial land resources are essential if optimum industrial development is to be realized. The following policies are recommended for this land use category.

(1) **Appropriate Uses:** The Industrial land use classification applies to areas that are used for primarily fabricating, processing, storage, warehousing, wholesaling, and transportation services.

(2) **Density Range:** Generally, unused or underutilized land is common in most industrial areas, and this land is suitable for expansion of existing facilities. The maximum density recommended for these areas is a floor area-ratio of 2.0 (2 square feet of building floor-

area for every one square foot of site area) on individual sites, and in the designated land use areas as a whole.

(3) Pattern of Industrial Development: As illustrated on the Detailed Land Use Map, continued centralization of industrial activities is encouraged. The orderly expansion of essential services to areas of planned industrial development is important both to ensure growth and minimize pressure for land use change in areas not recommended for industrial use.

(4) Development Flexibility: The pattern of industrial development should be sufficiently flexible to accommodate variable market demand. This approach reflects the past pattern of development where, for example, industrial areas planned as manufacturing centers developed with wholesaling and warehousing activities in addition to manufacturing. Exceptions should be at locations where adopted design plans recommend specific mix and/or patterns of industrial land use.

Buffer zones should be established between production activities and less-offensive surrounding land uses.

(5) Industrial Expansion and Redevelopment: Existing manufacturing, storage, and wholesaling activities, including many of the earliest industrial areas in Hancock County, frequently pose complex land use problems. While deterioration, obsolescence, poor location, and inadequate expansion space are problems plaguing some of the businesses in these areas, these marginal economic activities do provide employment opportunities for many persons lacking the necessary training or other types of employment. The following policies are recommended as guidelines for the difficult decisions, which must be made regarding marginal industrial activities.

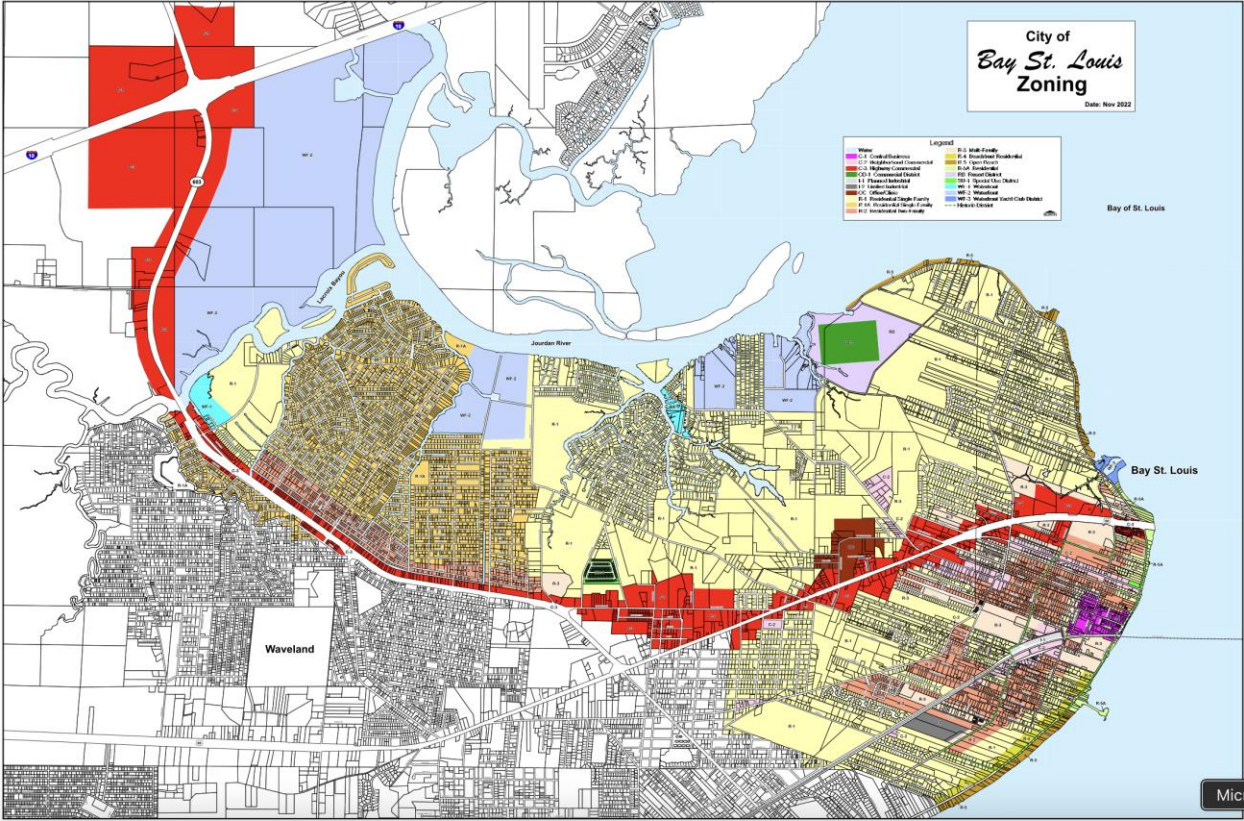


Obsolete and marginal industrial activities should be removed from areas where they are incompatible with surrounding uses, are a nuisance, and are a blighting influence on desirable non-industrial development. The redevelopment of blighted manufacturing and storage areas adjoining residential conservation areas should include residential and other uses that are compatible with the existing neighborhoods. The redevelopment should be based on a detailed, design analysis and a publicly adopted plan.

As seen in the foregoing redevelopment policies currently adopted ordinances, and represented by current practices in place by the City of Bay St. Louis that apply to residential, commercial, and industrial lands within the community’s urban areas are

essentially, the policies acknowledge that redevelopment activity is appropriate in the entire City of Bay St. Louis corporate limits.

The following section establishes and declares the entire City of Bay St. Louis boundary limits as a redevelopment area:



Section 6 – Establishing the Redevelopment Area for the Entire City of Bay St. Louis

In light of the previously stated findings and goals, the redevelopment areas for City of Bay St. Louis is established as follows:

- A. **Redevelopment Area Boundary Description:** The redevelopment area designated by this Plan will consist of all real and personal property located within the official boundaries of City of Bay St. Louis as recorded by the Mississippi Secretary of State and reflected in the State Charter; as of the date of adoption of this plan.
- B. **Redevelopment Area Boundary Map:** A boundary map of the redevelopment area encompassing all real property located within the boundaries of the City of Bay St. Louis is shown above on Page #53 and attached as **Exhibit VI** representing the legal metes and bounds of the entire City as recorded in the Mississippi Secretary of State’s office as reflected in the State Charter.

Section 7 – Redevelopment Implementation Strategies

After consideration and consultation with economic development professionals, the following redevelopment strategies have been identified as potentially appropriate means of achieving redevelopment in the City of Bay St. Louis:

1. enhanced Code Enforcement of the entire City and particularly in the commercial areas where population densities are higher;
2. promote the use of Urban Renewal and tax abatement under the statutes of the State of Mississippi;
3. promote the use of Tax Increment Financing as authorized under the Statutes of the State of Mississippi;
4. pursue and encourage developers using housing programs such as offered by the Mississippi Home Corporation and,
5. pursue other economic inducements, and tax incentives offered by the Mississippi Development Authority or other entities of State of Mississippi.

The Mayor and City Council has already begun implementation of Strategies 1, 2, and 3. The City and also the Hancock County Supervisors offer certain tax incentives for new development in the form of property tax abatements.

Strategy 1, enhanced code enforcement, as referenced above, should be implemented in a more aggressive manner. Aggressive code enforcement under the state statute of 21-19-11, as amended, will encourage redevelopment and improvements in housing inventory, quality of life, and market values.

Strategy 2 Urban Renewal, as referenced above, requires further study.

Strategy 3, Tax Increment Financing, as referenced above is proposed for authorization under this redevelopment plan, ***“The Tax Increment Financing Redevelopment Plan Bay St. Louis, Mississippi 2025.”*** Specific implementation considerations for Tax Increment Financing are set out in Section 8 below.

Strategy 4, other economic and tax incentives, as referenced above, has not been implemented through funds for low-income housing credits, first time buyers program funds, and Section 8 housing, but much more can be done for housing development to support desired population growth.

Strategy 5, has been pursued with mixed results during the past few years since incorporating the City. Further efforts should be made to pursue economic advantages, grants, and incentives through the Mississippi Development Authority as funded and made available.

Section 8 – Redevelopment Method Specifically for Tax Increment Financing

Mississippi’s Tax Increment Financing Act enables local municipalities and counties upon adoption of this Redevelopment Plan and authorizes the City of Bay St. Louis’s Mayor and City Council specifically, the use of Tax Increment Financing as an economic development inducement in new development, and redevelopment projects. The projects may involve reconstruction, renovation, or new construction for residential, commercial, or other uses that the Mayor and City Council, the Governmental Body of the City determined to be in the best public interest of the community. Tax Increment Financing under Mississippi Law may be applied to a “Redevelopment Project.”

Defining and Redevelopment Project

Section 21-45-3 of the Mississippi Code Annotated defines a “redevelopment project” and project areas as follows:

Redevelopment Project (See Exhibit I, Mississippi Code Annotated at 21-45-3 (b), (i), (ii), and (iii))

- (a) A “redevelopment project” may include any work or undertaking by a municipality:
- (i) to acquire project areas or portions thereof, including lands, structures, or improvements the acquisition of which is necessary or incidental to the proper clearance, development or redevelopment of such blighted areas or to the prevention of the spread or recurrence of slum conditions or conditions of blight;
 - (ii) to clear any such areas by demolition or removal of existing buildings, structures, streets, utilities, or other improvements thereon and to install, construct or reconstruct streets, utilities, bulkheads, boat docks, and site improvements essential to the preparation of sites for uses in accordance with the redevelopment plan and public improvements to encourage private redevelopment in accordance with the redevelopment plan; or,
 - (iii) to sell or lease property acquired by a municipality as part of a redevelopment project for not less than its fair value for uses in accordance with such redevelopment plan to retain property or public improvements for public use in accordance with the redevelopment plan.

“Redevelopment project” may also include the preparation of a redevelopment plan, the planning, survey, and other work incident to a redevelopment project and the preparation of all plans and arrangements for carrying out a redevelopment project, relocation of businesses and families required under applicable law, and upon a determination, by resolution of the governing body of the municipality in which such land is located, that the acquisition and development of additional real property not within a project area is essential to the proper clearance or redevelopment of a blighted area or a necessary part of the general slum clearance program of the municipality, the acquisition, planning, preparation for development or disposal of such land shall constitute a redevelopment project.

The Tax Increment Financing Act allows the use of Tax Increment financing for an individual project that lies within a “Redevelopment Project Area”. For the purposes of the Redevelopment Plan, project areas are defined as follows (see attached Exhibit I, Mississippi Code Annotated 21-45-3 (a), (i), (ii), (iii), and (iv)):

- (a) “Project area” includes:
 - (i) areas in which there is a significant amount of buildings or improvements, which by reason of dilapidation, deterioration, age, obsolescence, inadequate provision for ventilation, light, air, sanitation, or open spaces, high density of population and overcrowding or the existence of conditions that endanger life or property by fire and other causes, or any combination of such factors, are conducive to ill health, transmission of disease, infant mortality, juvenile delinquency or crime and are detrimental to the public health, safety, morals, or welfare;
 - (ii) areas which are of important value for purposes of historical preservation, as designated by the Department of Archives and History;
 - (iii) areas which by reason of a significant amount of defective or inadequate street layout, faulty lot layout in relation to size, adequacy, accessibility or usefulness, unsanitary or unsafe conditions, deterioration of site improvements, diversity of ownership, tax delinquency, defective or unusual conditions of title, improper subdivision or obsolete platting or the existence of conditions, which endanger life or property by fire or other causes, or any combination of such factors, substantially impair or arrest the sound growth of the community, retard the provision of housing accommodations or constitute an economic or social liability and are a menace to the public health, safety, morals, or welfare in their present condition and use; or,
 - (iv) areas in which the construction, renovation, repair, or rehabilitation of property for residential, commercial, or other uses are in the public interest.

Establishing a TIF Redevelopment Project Plan

This Redevelopment Plan shall be implemented primarily by the City, exercising the Redevelopment Project powers conferred upon the City of Bay St. Louis; by the Tax Increment Financing Act.

The techniques to be used to meet the goals and objectives of the Redevelopment Plan include rehabilitation and renovation, expansion of existing buildings, acquisition of real property, demolition and removal of buildings, construction of improvements, new development and redevelopment, consolidation of title, elimination of environmental deficiencies and blighting influences, construction of off-street public and private parking facilities, and encouragement of rehabilitation, renovation, new development and redevelopment by private enterprise. For projects located within the legal boundary limits of the City of Bay St. Louis Mississippi (the “City”), the codes, standards, and ordinances of the City shall apply.

A brief summary description of each component or technique of the Plan follows:

1. **Rehabilitation and Renovation**

This Redevelopment Plan provides for the utilization of the Tax Increment Financing powers of the City to rehabilitate and renovate structures, which are:

- a. Substandard structures on which rehabilitation or renovation is practical in the judgment of the owners thereof.
- b. Structures in which, in the judgment of the owner thereof, rehabilitation will result in a higher and better use for the property on which such structure is located.

A substandard structure is a structure that does not conform to the building and related codes of the City as are enforced and are in effect from time to time, whichever may apply.

2. **Acquisition of Real Property**

Real property shall be acquired as provided in Article IX of the Redevelopment Plan.

3. **Demolition and Removal of Structures**

This Redevelopment Plan shall be implemented in some instances through the demolition and removal of structures. The Plan provides for the demolition and removal of the following structures, if necessary and appropriate:

- a. substandard structures, which in the judgment of the owner thereof, rehabilitation is not practical;
- b. structures, which in the judgment of the owner thereof, are economically obsolete;

- c. structures, which in the judgment of the owner thereof, prevent maximum utilization of the property on which such structures are located;
- d. structures that have been acquired by the City and that the Governing Body of the City determines to be economically obsolete; and,
- e. structures that have been acquired by the City and that the Governing Body determines prevent the maximum utilization of the property on which such structures are located.

4. **Construction of Improvements**

This Redevelopment Plan provides for the construction of new improvements for uses on vacant or cleared land within the redevelopment area (any lands lying within the boundaries of the City). Such construction shall be in accord with the City of Bay St. Louis Comprehensive Plan adopted in 2024 or as amended, Official Zoning Ordinance, Subdivision Regulations, building and related codes, and other applicable codes and ordinances of the City, whichever may apply.

5. **Development and Redevelopment**

Under the provisions of this Redevelopment Plan, vacant or cleared land held by the City may be disposed of by private parties for new development or redevelopment. Such disposition of land by the City shall be in accordance with the Redevelopment Plan and applicable law.

This Redevelopment Plan encourages the new development or redevelopment of vacant or cleared land, which is acquired by private enterprises.

6. **Consolidation of Title**

This Redevelopment Plan provides for the elimination of diverse patterns of ownership, which prevent or restrict development of large tracts of land.

In the event that the City determines that optimum utilization of a particular tract of land cannot be attained because of diversity of ownership, and that action by the City is likely to enhance the possibility of development of such tracts, the City's Mayor and City Council may take such actions as it determines to be necessary to consolidate title to such tracts of land.

7. **Elimination of Environmental Deficiencies and Blighting Influences**

This Redevelopment Plan provides for the City to undertake to determine any and all factors, which prevent or restrict new development or redevelopment within the redevelopment area. The Governing Body of the City shall take such action, as it deems necessary to reduce or eliminate such factors or the effects thereof.

Any environmental deficiencies and blighting influence's the City may deem necessary to reduce or eliminate include, but are not necessarily limited to, physical conditions, ownership patterns, non-conforming land uses, improper development, and as allowed under the state statutes.

8. **Construction of Off-Street Parking Facilities**

This Redevelopment Plan provides for the construction of public and private off-street parking facilities to serve the demand within the redevelopment area (all lands lying within the boundaries of City of Bay St. Louis).

Off-street public and private parking facilities shall be constructed in accord with the City of Bay St. Louis Comprehensive Plan, the Official Zoning Ordinances, and other applicable codes and ordinances of the City, whichever may apply.

9. **Encouragement of Rehabilitation, Renovation, New Development and Redevelopment by Private Enterprise**

This Redevelopment Plan provides that the City shall, to the greatest extent feasible, afford maximum opportunity to encourage the rehabilitation, renovation, new development and redevelopment of the redevelopment area (all lands lying within the boundaries of City of Bay St. Louis) by private enterprise.

In order to utilize appropriate private and/or public resources to eliminate and prevent the development, or spread of slums and urban blight, to encourage needed urban rehabilitation and renovation, to provide for the redevelopment of slum and blighted areas, to provide for and encourage the new development or redevelopment of vacant or cleared lands, the City shall encourage rehabilitation, renovation, clearance, new development or redevelopment within the redevelopment area by:

- a. Carrying out a program of voluntary repair, renovation, and rehabilitation of buildings or other improvements in accordance with this Redevelopment Plan, including making loans to defray all or part of the costs (including costs of acquiring real estate) of repairing and rehabilitating buildings or other improvements in accordance with this Redevelopment Plan.

- b. Making loans to defray all or part of the costs of acquiring real property, demolishing and removing buildings and improvements and constructing improvements (including buildings) in the redevelopment area in accordance with this Redevelopment Plan.

Such loans shall be made only from the proceeds of revenue bonds, notes, or similar debt instruments of the City secured solely by the City's rights in connection with such loans; shall be made upon such terms and conditions as the City of Bay St. Louis's Mayor and City Council shall determine, and shall be subject to such additional requirements as the City shall impose.

Section 9 – Conformity with the City’s Development Codes Required

1. Land Uses/Zoning Ordinance

To the extent that it is practical, and authorized, this Redevelopment Plan will encourage development and redevelopment of projects that are compatible with adjoining, and nearby land uses and conform to City zoning controls.

For projects located within the boundaries of the City, and within its zoning jurisdiction, land-use in the redevelopment area shall be regulated by the Official Zoning Ordinance of the City.

The Official Zoning Map delineating defined zoning areas is on file in the office of the City of Bay St. Louis’s City Clerk.

2. Maximum Densities/Zoning Ordinance

For Tax Increment Financing projects located within the corporate limits of the City, the Zoning Ordinances of the local community as applicable shall regulate maximum development densities in the redevelopment area.

For these projects lying in portions of the City, outside the corporate limits of any municipality, this Redevelopment Plan calls for adherence to the goals and objectives of this Redevelopment Plan.

3. Land Development/Subdivision Regulations

This Redevelopment Plan shall require that any development assisted with Tax Increment Financing be developed in conformity with regulations and standards of the community, or municipality governing the subdivision of land, namely the Subdivision Regulations of the municipality.

4. Building Requirements/Building and Related Codes

This Redevelopment Plan requires that any new construction and any rehabilitation shall meet the requirements of the building and related codes of this City and any applicable requirements of the community as applicable, as may be in effect from time to time, whichever is appropriate.

5. **Circulation and Traffic Control**

For the purposes of Tax Increment Financing projects located in the incorporated areas of the City, this Redevelopment Plan shall be implemented in conformity with the standards and policies governing transportation routes in the City. The Subdivision Regulations, street design, and construction standards of the City of Bay St. Louis govern street widths, ingress, and egress patterns, designated turning lanes, turn restrictions, and vehicle limitations established by weight. These regulations, along with realignment, widening, and overlaying of existing streets, will achieve improved traffic circulation and flow for the municipality as applicable, or county and pass-through traffic. Traffic control and circulation in the redevelopment area will utilize electronic signalization and will provide optimum access to existing and proposed off-street parking. This Redevelopment Plan will be implemented so as to efficiently integrate traffic circulation flow into the general circulation patterns of the community.

6. **Off Street Parking and Loading**

For the purposes of Tax Increment Financing projects in the City of Bay St. Louis, this Redevelopment Plan conforms to the City's Official Zoning Ordinance, other codes, and State and Federal ordinances wherein off-street parking, loading and unloading space requirements shall be regulated and controlled.

7. **Minimum Requirements**

For the purposes of this Redevelopment Plan, regulations referred to shall be interpreted and applied as minimum requirements. Whenever the requirements of these regulations are at variance with the requirements of any other lawfully adopted rules, regulations, ordinances, deed restrictions or covenants, the most restrictive or that imposing the higher standards shall govern.

8. **Waiver**

Any requirement or condition contained in this Redevelopment Plan may, upon request; and for good cause shown, be waived or modified by the City to the extent permissible under law.

Section 10 – The Acquisition of Real Property

1. Procedures

This Redevelopment Plan requires that all property acquisitions by the City of Bay St. Louis Mississippi shall be made in accordance with Title 21, Chapter 45, of the Mississippi Code of 1972. Discrimination of any kind shall be prohibited. Race, color, creed, national origin, age, and sex shall in no way affect any aspect of the land acquisition process.

2. Property to Be Acquired

Under the provisions of this Redevelopment Plan, the City of Bay St. Louis's Mayor and City Council may, at its discretion, acquire real property (as defined in the act) for needed public facilities within the redevelopment area.

Where necessary to eliminate unhealthy, unsanitary, or unsafe conditions, lessen density, eliminate obsolete, or other uses detrimental to the public welfare, or otherwise to remove or prevent the spread of blight or deterioration; the City may, from time to time in its discretion, acquire mortgages made to finance costs of (1) acquiring real property, (2) demolishing and removing buildings and improvements, and (3) constructing improvements in the redevelopment area in accordance with this Redevelopment Plan.

Section 11 – General Intent

As related to land uses, improved mobility, public transportation, public utilities, recreational and community facilities, and other public improvements, the objectives and purposes of the Tax Increment Financing Redevelopment Plan is interrelated with the overall planning policies of the City of Bay St. Louis.

The Redevelopment Plan seeks to encourage redevelopment for capital investment that will increase the tax-base, increase retail sales tax rebates, and increase job opportunities in the community.

The Redevelopment Plan seeks to encourage revitalization of the redevelopment area by inducing new developments and redevelopment. Public facilities and services will be improved and properly maintained as a result of implementation of this Redevelopment Plan.

Section 12 – Redevelopment Plan Amendments

This Redevelopment Plan may be modified by the Governing Body of the City of Bay St. Louis in accordance with the provisions of Title 21, Chapter 45, Mississippi; Code of 1972, as amended.

Exhibit I

Tax Increment Financing Plan

A. The Tax Increment Financing Plan is a financial tool for the implementation of the Redevelopment Plan objectives and purposes. The Tax Increment Financing Plan for a redevelopment project requires the following:

I. **Redevelopment Project Description**

- a. Location, legal description, environmental characteristics and zoning
- b. Building type and proposed use(s)

II. **Developer(s) Information**

- a. Name(s) and/or entity name
- b. Address: Local and out-of-state
- c. Tax identification number
- d. Local contractors or agents

III. **Economic Development Impact Description**

- a. Job creation – permanent and temporary
- b. Financial benefit to the community

IV. **A Statement of the Objectives for the Tax Increment Financing Plan**

- a. *Construction of Improvements:*
New improvements shall be constructed for uses in accordance with the Redevelopment Plan on vacant or cleared property within the redevelopment area. Such construction shall be in accordance with the requirements of the City's building and related codes as in force and effect from time to time.
- b. *Redevelopment:*
Property held by the City of Bay St. Louis may be disposed of in favor of private parties for development or redevelopment in accordance with the provisions of this Redevelopment Plan and applicable law.
- c. *Consolidation of Title:*
In order to eliminate diverse patterns of ownership that prevent or restrict development of large tracts of land, in the event that the City determines that optimum utilization of a particular tract of land cannot be attained because of diversity of ownership and that action by the City is likely to enhance the possibility of development of such tract, the Governing Body of the City, shall take such actions as it determines to be desirable to consolidate title to such tract of land.

V. A statement indicating the need and proposed use of the Tax Increment Financing Plan in relationship to the Redevelopment Plan.

- VI. A statement containing the cost estimates of the redevelopment project and the projected sources of revenue to be used to meet the costs including estimates of tax increments and the total amount of indebtedness to be incurred.
- VII. A list of all real property to be included in the Tax Increment Financing Plan.
- VIII. The duration of the Tax Increment Financing Plan's existence.
- IX. A statement of estimated impact of the Tax Increment Financing Plan upon the revenues of all taxing jurisdictions in which a redevelopment project is located.
- X. The Governing Body shall by resolution, from time to time, determine (i) the division of ad valorem tax receipts, if any, that may be used to pay for the cost of all or any part of a redevelopment project, (ii) the duration of time in which such taxes may be used for such purposes, (iii) if the City's Mayor and City Council shall issue bonds for such redevelopment project, and (iv) such other restrictions, rules and regulations as in the sole discretion of the Governing Body of the City shall be necessary in order to promote and protect the public interest.

Exhibit II

The Tax Increment Financing Act

- Tax Increment Financing is a legal, constitutionally accepted financing mechanism to accomplish **DEVELOPMENT** and **REDEVELOPMENT** of municipalities and counties throughout the State of Mississippi. The Tax Increment Financing Act was adopted by the Mississippi Legislature in 1986 and can be found at Sections 21-45-1 et seq., in the Mississippi Code of 1972, as amended. (Attached as Exhibit I)
- Over thirty (30) states have adopted Tax Increment Financing Laws ("TIF"), the first of which was adopted in 1969.
- **TIF** is a tool that can be used to induce or encourage major economic development in Mississippi cities and counties. A municipality and county can undertake joint TIF projects, under the Regional Economic Development Act (REDA) as amended. Attached as **Exhibit V** herein.
- Cities and counties everywhere have used the Economic Development Administration, Community Development Block Grants, Special Assessments, and Industrial Revenue Bond programs to encourage economic development. **TIF** is one more method that should be used.
- The City of Bay St. Louis will use **TIF** to expand and complement aggressive economic development policies. The program is a local program that can be accessed, and used through the City's own initiative. Federal and state governmental approvals are not required.
- Tax Increment Bonds, notes, or other similar debt obligations are issued by the City to construct improvements that will induce development activity, and the City pledges all or part of the increase in ad valorem taxes generated on assessment of the real and/or personal property constituting the project to retire the bonds. The municipality may elect to pledge increases in retail sales tax rebates generated by the project and received from the State to service TIF bonds.
- In most cases, Tax Increment Financing is used to construct public infrastructure improvements (i.e., roads, utilities, drainage, parking, etc.) even though it can be used for other purposes.
- Bonds are issued by the City to construct the improvements, and increased ad valorem taxes generated on assessment of the real and/or personal property constituting the project can be used to retire the bonds. The municipalities may elect to pledge sales tax rebate increases generated by the project and received by the State to service the TIF debt.
- School taxes **will not** be used to retire the bonds. The schools can use those funds in their normal operations.
- There is **NO** financial obligation on the part of the county or the municipality if Tax Increment Financing Revenue Bonds, notes or other similar debt instruments are issued, other than the pledge of the increase in ad valorem tax revenues on real and personal property and/or sales tax rebates received from the State if applicable. The law emphatically states that the TIF bonds are secured solely by increases in ad valorem tax revenues, and/or rebated sales taxes received from the State, and are **NOT** general obligations secured by the full faith, credit, and taxing power of the City, or the County.

Exhibit III

The Tax Increment Financing Process

Tax Increment Financing Redevelopment Plan

- Preparation of a Tax Increment Financing Redevelopment Plan designating all, or part of the City "a redevelopment project area"
- Adoption of a Resolution by the City of Bay St. Louis's Mayor and City Council (the "Governing Body") determining the necessity for Tax Increment Financing and setting a public hearing in connection with the Redevelopment Plan
- Publication of a Notice of Public Hearing
- Conduct a Public Hearing at which time the Redevelopment Plan and related matters are presented to the public, and citizens are given an opportunity to comment
- Passage of a Resolution adopting and authorizing the Tax Increment Financing Redevelopment Plan

Tax Increment Financing Plan

- Preparation of a Tax Increment Financing Plan for a specific economic development project
- Adopting a resolution acknowledging the availability of the Plan, identifying the specific project, the proposed use of the funds from Tax Increment Financing, and other matters. The Resolution sets a Public Hearing
- The City's Mayor and City Council conducts a Public Hearing at which time the Tax Increment Financing Plan is presented to the public and citizens are given an opportunity to comment
- The Governing Body of the City of Bay St. Louis vote in favor of the passage of a resolution to adopt and authorize the Tax Increment Financing Plan
- Bond Counsel (Attorney) proceeds with the bond issue process

Exhibit IV

**Summary Reference of the Tax increment Financing - Mississippi code,
21 - 45 as amended**

Chapter 45 - Tax Increment Financing

§ 21-45-21. Assessment of value of real property described in tax increment financing plan; retention and distribution of captured assessed value; approval of redevelopment plan; certification of amount of sales tax collected

Universal Citation: [MS Code § 21-45-21 \(2016\)](#)

(1) After adoption of a redevelopment plan containing a tax increment financing plan the clerk shall certify the assessed value of the real property, including personal property located thereon, described in the tax increment financing plan. Property taxable at the time of the certification shall be included in the assessed value at its most recently determined valuation.

Property exempt from taxation at the time of the request shall be included at zero unless it was taxable when the tax increment financing plan was approved, in which case its most recently determined assessed valuation before it became exempt shall be included. These assessed values shall be, and will be referred to as, the "original assessed value."

(2) Each year thereafter, the clerk and the State Tax Commission, if applicable, shall certify the amount by which the assessed value of real property, including personal property located thereon, described in the tax increment financing plan has increased or decreased from the original assessed value. These assessed values shall be, and will be referred to as, the "current assessed value."

(3) Any amount by which the current assessed value of the real property, including personal property located thereon, described in the redevelopment plan exceeds the original assessed value shall be referred to as the "captured assessed value." The clerk shall certify the amount of the captured assessed value to the municipality each year for the duration of the tax increment financing plan. A municipality may choose to retain all or a portion of the captured assessed value for purposes of tax increment financing if the plan provides that all or a portion of the captured assessed value is necessary to finance the redevelopment project, including the cost of establishing necessary reserves to insure payment of revenue bonds.

If the tax increment financing plan provides that only a portion of the captured assessed value is necessary to finance the redevelopment program, only that portion shall be set aside and the remainder shall be apportioned to the various municipal tax levy funds and the various county tax levy funds.

The amount of captured assessed value that a municipality intends to use for purposes of tax increment financing must be clearly stated in the tax increment financing plan.

(4) After adoption of a redevelopment plan containing a tax increment financing plan which includes a portion of the municipality sales tax diversion, the State Tax Commission shall certify the amount of sales tax collected by the state within the boundaries of the redevelopment area and diverted to the municipality in the twelve-month period ending on the last day of the month before the effective date of approval of the plan. Any increase in the amount collected within the boundaries shall be set aside by the municipality in the fund created by the tax increment financing plan.

Each redevelopment plan shall be approved in the same manner and at the same times provided in Section 43-35-13 for the approval of urban renewal plans. Any tax increment financing plan shall become effective on the same date as the redevelopment plan is approved.

Exhibit V

Summary Reference of the Regional Economic Development Act (REDA) Mississippi code 57 - 64 as amended

2013 Mississippi Code

Title 57 - PLANNING, RESEARCH AND DEVELOPMENT

Chapter 64 - REGIONAL ECONOMIC DEVELOPMENT

§ 57-64-29 - Authority of member of regional economic development alliance to negotiate purchase option for real property; preliminary engineering, environmental and related studies; costs

Universal Citation: MS Code § 57-64-29 (2013)

A county that is a member of a regional economic development alliance created under the Regional Economic Development Act is authorized to negotiate a purchase option for real property to be used for the purposes of the alliance. A county may pay all costs incurred for the acquisition of such an option regardless of whether the county exercises the option at a later date. As a part of any such option, a county may negotiate the right to enter upon the real property before the purchase for the purpose of conducting any preliminary engineering, environmental and related surveys or studies necessary to effectuate the option. A county may pay all costs incurred for such surveys or studies regardless of whether the county exercises the option at a later date.

REGIONAL ECONOMIC DEVELOPMENT ACT

MISSISSIPPI CODE OF 1972

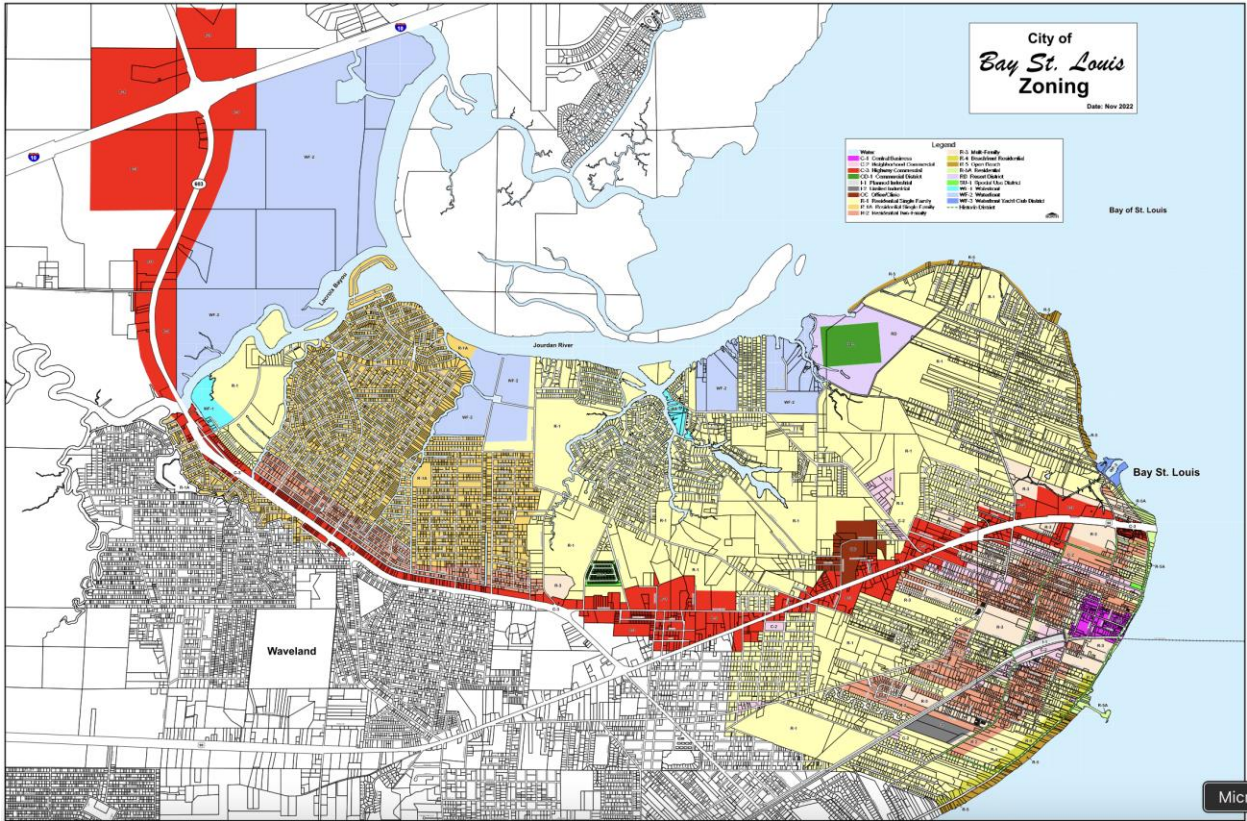
As Amended

Index for Chapter 064 of Title 57

- [57-64-1.](#) Short title.
 - [57-64-3.](#) Declaration of public policy.
 - [57-64-5.](#) Purpose.
 - [57-64-7.](#) Definitions.
 - [57-64-9.](#) Certificate of public convenience and necessity.
 - [57-64-11.](#) Issuance of bonds.
 - [57-64-13.](#) Joint exercise of power and authority by local governments.
 - [57-64-15.](#) Powers with regard to issuance of bonds.
 - [57-64-17.](#) Promulgation of rules and regulations.
 - [57-64-19.](#) Intergovernmental cooperation and coordination
 - [57-64-21.](#) Agreements under this chapter to include certain provisions.
 - [57-64-23.](#) Agreements under this chapter to be approved by certain officers; agree
 - [57-64-25.](#) Applicability of existing laws.
 - [57-64-27.](#) Powers and authorities granted by this chapter additional and suppleme
proceed with a project utilizing methods not included in this chapter.
 - [57-64-29.](#) Authority of member of regional economic development alliance to neg
 - [57-64-31.](#) Eminent domain.
 - [57-64-33.](#) Repealed.
-

Exhibit VI

Legal Description, and Boundary Map of City of Bay St. Louis, Mississippi



As required by Section 21-45-21 of the Mississippi Code of 1972, as amended, the Planning Commission was directed by the City Council to consider the manner in which the *Tax Increment Financing Redevelopment Plan, Bay St. Louis, Mississippi, 2025* conforms with the comprehensive plan of the City of Bay St. Louis adopted on July 02, 2024.

This letter is to advise you that the City of Bay St. Louis Planning Commission has met; and reviewed the *Tax Increment Financing Redevelopment Plan, City of Bay St. Louis, Mississippi, 2025*. As required, the Planning Commission has determined that the Redevelopment Plan is in accord with the goals, and objectives of the general plan for redevelopment for the City of Bay St. Louis.

We recognize Tax Increment Financing as a valuable economic development tool, which will strengthen our community's tax base and provide job opportunities for our City.

Therefore, it is **recommend approve** and implementation of the *Tax Increment Financing Redevelopment Plan, Bay St. Louis, Mississippi, 2025*.

- Jeremy Burke