



TOWN OF ASHLAND CITY

Planning Commission Meeting

April 04, 2022 5:30 PM

Agenda

Chairman: Steven Stratton

Committee Members: Steve Allen, Vivian Foston, Gerald Greer, Mike Stuart, Mike Smith

CALL TO ORDER

ROLL CALL

APPROVAL OF AGENDA

APPROVAL OF MINUTES

1. [March 07, 2022 Planning Meeting Minutes](#)

PUBLIC FORUM

OLD BUSINESS

2. Zoning and Future Growth Discussion

NEW BUSINESS

3. Preliminary Site Plan Approval: Parker Towing
4. Dissolve Regional Planning Commission

OTHER

ADJOURNMENT

Those with disabilities who require certain accommodations in order to allow them to observe and/or participate in this meeting, or who have questions regarding the accessibility of the meeting, should contact the ADA Coordinator at 615-792-7553, M-F 8:00 AM – 4:00 PM. The town will make reasonable accommodations for those persons.



TOWN OF ASHLAND CITY
Planning Commission Meeting
March 07, 2022 5:30 PM
Minutes

CALL TO ORDER

Chairman Stratton called the meeting to order at 5:33 p.m.

ROLL CALL

PRESENT

Chairman Steven Stratton
Committee Member Vivian Foston
Committee Member Gerald Greer
Committee Member Michael Smith
Committee Member Mike Stuart

ABSENT

Committee Member Steve Allen

APPROVAL OF AGENDA

A motion was made by Committee Member Stuart, seconded by Committee Member Greer, to approve the agenda. All approved by voice vote.

APPROVAL OF MINUTES

1. February 07, 2022 Planning Commission Meeting Minutes
A motion was made by Committee Member Foston, seconded by Committee Member Smith, to approve the February 07, 2022 Planning Commission Meeting Minutes. All approved by voice vote.

CALL TO ORDER

A motion was made by Committee Member Greer, seconded by Committee Member Stuart, to defer this until the next meeting due to a vacancy on the board. All approved by voice vote.

PUBLIC FORUM

None.

OLD BUSINESS

None.

NEW BUSINESS

1. Zoning and Future Growth Discussion
Committee Member Greer stated that he was concerned about how we could influence quality growth and how we could sell this town to get quality builders. He stated that the Town of Ashland City was mentioned in a recent article in the Nashville Business Journal and read a paragraph from the article to the board. Committee Member Smith stated that we could ask someone from the Nashville Business Journal to come out to tour Ashland City and run an article on just Ashland City. Committee Member Greer asked if we could get some type of zoning requirements that would prevent unwanted developments like triplexes and duplexes. He stated that we have to get control of what is being built and the quality of the builds. Mr. Gregory stated that we could, but we cannot control the unintended consequences that may come with it. He stated that he does not think our problem is zoning, but the requirements of the zoning. Mr. Gregory stated that it would be a policy change where when someone comes for a rezone request, they should expect it to be a PUD.
2. Plat Amendment Parcel 064-011.01

Mr. Hussaen Ismael stated that he was requesting a plat amendment for a 15,000 sq foot tract on this property that would allow for a single-family home. Committee Member Greer asked if this was in the flood zone. Mr. Ismael stated that the flood zone is scattered on the property and it is not an issue at the moment. Committee Member Greer asked what he planned to do with the rest of the property. Mr. Ismael stated that it was a townhome development zoned R4 PUD and it is in the works. Mr. Gregory stated that this is a two-part request. He stated that the first request is to divide the property and the second request is to rezone for the smaller lot to R3. Mr. Gregory stated that the rezone request will need to go before Council for approval. He stated that the plat references the 2010 flood map and that map has been updated. Committee Member Smith asked if we were voting to separate the tracts. Mr. Gregory stated that was correct and to approve the rezone to R3. Committee Member Smith asked if that only allows for single-family housing. Mr. Gregory stated that it permits for single-family, prefabricated, and duplexes, but there are thirteen (13) special exceptions it will allow for. After much discussion, a motion was made by Committee Member Stuart, seconded by Committee Member Greer, to defer the request. All approved by voice vote.

3. Minor Subdivision Application

Mr. Rick Johnson stated that he is trying to straighten out the lot lines. He stated that he has owned his property for years and is trying to divide the property. Committee Member Stuart asked if it was three existing properties. Mr. Johnson stated that it was and they meet all the setbacks. Mr. Gregory stated that the notes do not have anything about previous recordings, but he is sure the surveyor has a copy of the note. A motion was made by Committee Member Greer, seconded by Committee Member Foston, to approve the application with the note to be added. All approved by voice vote. Voting Yea: Chairman Stratton, Committee Member Foston, Committee Member Greer, Committee Member Smith, Committee Member Stuart.

4. Site Plan Approval 2437 Bell Street

Mr. John Rankin stated he was representing the owner and this was to get approval on a final site plan of six (6) additional units on Bell Street. He stated that these would be different than the existing new builds, but they would be the same style as the three (3) closest to Forrest Street. Chairman Stratton asked if this was part of the existing line of homes. Committee Member Foston asked if this was on the right of Poole Street. Mr. Rankin stated that it is on the corner of Poole and Bell. He stated that they are building a separate road next to Bell Street. Mr. Gregory stated that there is not much separation between Poole Street and that entrance and it does not look like a safe connection. He asked for that to be reconsidered. Mr. Rankin stated that they would be happy to delete that entrance. Committee Member Stratton asked they would accommodate emergency vehicles. Mr. Rankin stated that they would have to make a hammerhead at the end where they can make that turn. He stated that it had not been thought of prior to this moment and they would be happy to delete that entrance. Chairman Stratton stated that he was concerned with the lack of landscaping in this development including the other twenty-one (21) new builds. Mr. Rankin stated that they can make that a condition for all 27 units. A motion was made by Committee Member Stuart, seconded by Chairman Stratton, to approve the site plan with the condition that the entrance and exit at Poole Street are not allowed and all site plan requirements are part of the approval including the drainage issues, stormwater retention, and landscaping for all 27 units. Voting Yea: Chairman Stratton, Committee Member Foston, Committee Member Greer, Committee Member Smith, Committee Member Stuart.

5. Site Plan Approval 1209 Hwy 12S

Mr. Josh Lyon stated he was there on behalf of Klobner Engineering for a site plan approval and they are working through the engineering comments. He stated that they developed this site plan as a private development that will be managed by an HOA and there will be nothing public on it except the utilities such as the sewer and water that will cross through the site. Mr. Lyon stated that there are requests in the engineering comments to design the roadways to City

standards. He stated that because of site constraints they put the drainage down the middle and designed it as a private site plan like a parking lot. Mr. Lyon stated that the request came from the consulting engineer to construct the private drive as a roadway and they are here to request the roadways be allowed to be designed as a private entity that will be controlled by the HOA and never turned over to the City. Mr. Gregory asked if he could explain the difference between the two. Mr. Lyon stated that the City would require certain asphalt and aggregate thickness to be heavy duty because typically they would be turned over to the City and maintained by the developer for a year. He stated that City streets are crowned which means they are higher in the middle and slope to the curb. Mr. Lyon further stated that with their development they are asking for typical standards like you would have in an apartment complex parking lot. He stated that the roadways would slope to the middle to keep drainage away from the units and eliminates conflict of water and sewer. Mr. Gregory asked what the difference was in the paved thickness. Mr. Lyon stated that they would have an inch and one half of topping (1.5in), two inches of binder asphalt (2in), and six inches of aggregate compacted base. He stated that a city street would have a ten-inch stone base (10in), three inches of binder (3in), and two inches of topper (2in) which is much thicker than what is required of any typical parking lot. Mr. Lyon stated that it is a relief in thickness, but better for the infrastructure. Committee Member Smith asked who was requesting it to be held to the City standards. Mr. Lyon stated that the City's engineering consultant is requesting it. Ms. Amanda Bell stated that it is not the product they are wanting to provide. Committee Member Stuart declared a conflict on this particular agenda item. Mr. Gregory asked if there was room for compromise to blend the two. Mr. Lyon stated that drainage down the middle is of high importance. After much discussion, a motion was made by Committee Member Smith, seconded by Committee Member Greer, to approve the site plan with the City's required thickness and drainage down the middle. Voting Yea: Chairman Stratton, Committee Member Foston, Committee Member Greer, Committee Member Smith. Voting Abstaining: Committee Member Stuart.

6. Amend By-Laws and Rules of Procedure

Mr. Gregory stated that we need to amend our By-Laws because it currently reads that the body appoints members but should read that the Mayor appoints. A motion was made by Committee Member Stuart, seconded by Committee Member Greer, to approve the amendment of the By-Laws. Voting Yea: Chairman Stratton, Committee Member Foston, Committee Member Greer, Committee Member Smith, Committee Member Stuart.

OTHER

None.

ADJOURNMENT

A motion was made by Committee Member Stuart, seconded by Chairman Stratton, to adjourn the meeting. All approved by voice vote and the meeting adjourned at 6:47 p.m.

CHAIRMAN STEVEN STRATTON

CITY RECORDER ALICIA MARTIN, CMFO



Ashland City Fire, Building & Life Safety Department

101 Court Street
Ashland City TN 37015

Fire & Life Safety: (615) 792-4531 – Building Codes (615) 792-6455

APPLICATION FOR SITE PLAN APPROVAL

Date Received: March 17, 2022

Property Address: Trinity Road, Parcel Number

011 065 03601 000 2018

Map # 065 Parcel # 36.01 Acreage: 22.77

Property Owner(s): Parker Towing Company, Inc.

Phone: 615-948-4513 (Neel-Schaffer, Inc. – Engineer)

205-331-3298 (Parker Towing Company, Inc.)

Description of project being reviewed: Proposed barge terminal facility, including 50k sq. ft. warehouse, 3,000 sq. ft. office with scales, barge offloading operations, and onsite material storage areas.

Having submitted plans for review by the Ashland City Planning Commission, I understand that I am responsible for all review fees incurred by the Town of Ashland City. I understand that the fee paid at the time of submittal is not applicable for the fees incurred through review. With my signature, I verify that I fully understand that I am responsible for said fees, and that I have received a copy of Ordinance #165.

ASHLAND CITY PLANNING COMMISSION SITE PLAN REVIEW FEE: \$100.00

NEXT SCHEDULED MEETING: April 4, 2022

Michael R. Phillips

Applicant's Signature

March 17, 2022

Date

March 17, 2022

Mr. Allen Nicholson, Building and Codes Director
Town of Ashland City
233 Tennessee Waltz Parkway, Suite 103
Ashland City, TN 37015

**Re: PARKER TOWING CUMBERLAND RIVER TERMINAL
MAP 065, PARCEL 011 065 03601 000 2018
SITE PLAN APPLICATION**

Dear Mr. Nicholson,

Neel-Schaffer, Inc. (NSI) is pleased to submit following site plans to the Planning Commission for review and consideration. Within this package are the Site Plan documents for the development of the above referenced property located along the Cumberland River at the end of Trinity Road. Below are additional details for consideration as you review this package:


- This project will be constructed in phases with the phase 1 buildings, site utilities, and asphalt storage/driveways constructed during this phase. This initial submittal is intended to gain overall site plan approval for access and grading work to commence. Future submittals for consideration will include the marine dock, riverfront stabilization, building permits, and potential railroad spur. These items are shown as approximate with details to be refined in the future. A timeline has not been established for these future phases as of this submission.
- This facility will be an industrial facility, consisting of a barge offloading operation and onsite material storage.
- The sign for the location will be added beneath the existing ARCOSA sign currently located along Highway 12 at Trinity Road. We have included details associated with the existing sign and the proposed sign addition within this package.
- The USACE and ARAP permits have been received for this location and the Zero Rise certification and Cumberland River flood study analysis are included in this package for your review. We have included the permit documentation within this package.
 - As noted in the permit documents, the northern portion of the site has been authorized to fill the wetlands, and the will remain as wetlands as well as the stream along Trinity Road will remain minimally altered.
 - The permit application for the Tennessee Construction General Notice of Intent and Storm Water Pollution Prevention Plan (SWPPP) will be submitted separately to the State of Tennessee and a courtesy copy will be provided to the City. Any approvals received during the City's review process will be forwarded for inclusion within this package.
- We have been in close contact with the public utility companies, including Cheatham County Wastewater Authority, Cumberland Electric Membership Corporation, and Ashland City, in order

to coordinate requirements for utility services to the site. The utility drawings in this package represent preliminary plans based on the following factors:

- The sewer force main is approximately shown on the plans based on conversational details from multiple CCWWA and Ashland City staff members. Spot excavations to physically locate the main have proven to be unsuccessful; however, the City has indicated that assistance will be provided to locate the force main. Once located, a plan to reroute the sewer force main will be further developed.
- Pressure tests of the existing supply waterline along Trinity Road indicate sufficient volume and pressure for future service at the site.
- CEMC has committed to installing sufficient powerlines and transformers to provide service from Trinity Road to the site with enough height to cross the future railroad spur. Parker Towing will provide the required 11-ft gravel access road for the powerline to facilitate the work and will ensure that a 40-ft clear zone is provided to maintain the power lines.
- The office building that is shown was originally attached to the side of the warehouse as shown within the included building plans. The full design for this structure has not been finalized but the location change was made to accommodate the scale ticketing system. A future submittal will be made to include the finalized building plans for both the warehouse and the office space as part of a building permit application.

We appreciate the opportunity to work with the Planning Commission on this project. Should you have any questions or if I may be of further service to you in any way, please don't hesitate to call me at 615-383-8420 or 615-948-4513 (cell).

Sincerely,
NEEL-SCHAFFER, INC.



Mike Phillips., P.E., CFM
Senior Project Manager

Property Survey

(Arc Surveying)

Site Plans

(Neel-Schaffer, Inc)

Building Plans

(Nucor, Inc)

Sign Details

Permit Documents



Ashland City Fire, Building & Life Safety Department

101 Court Street
Ashland City TN 37015

Fire & Life Safety: (615) 792-4531 – Building Codes (615) 792-6455

PLANNING COMMISSION SITE PLAN CHECKLIST

NAME OF SITE Parker Towing – Cumberland River Terminal

LOCATION Trinity Rd, Map and Parcel 065 03601 000 2021 ZONING DISTRICT I1 (Light Industrial)

OWNER Parker Towing, Inc.

ENGINEER Neel-Schaffer, Inc.

1. Three (3) copies of the site plan. Please indicate at time of application if you would like any of the remaining copies after your case is heard and voted on. **(Provided electronically – PDF).**
2. Three (3) copies and an electronic PDF of revised site plans made available to the Fire, Building and Life Safety Department – according to planner/engineer comments. Also written response to all comments to match what was changed on revised site plans. **(Provided electronically – PDF).**
3. Location map of the site at a scale of not less than 1"=2000' (USGS map is acceptable). Map must show the following **(Provided in enclosed Supporting Documents, Figure 1 and enclosed construction plans)**:
 - a. Approximate site boundary
 - **Detail in survey plan sheets (G0.03, G0.04), as well as item 2 in Supporting Documents folder.**
 - b. Public streets in the vicinity
 - **Nearest public road – TN Highway 12. Detail on the survey plan sheets (G0.03, G0.04).**
 - c. Types of development of surrounding parcels
 - **Surrounding property found in I1 – Light Industrial**
 - d. Public water and sewer lines serving the site
 - **Best known information provided on plan sheet C4.00. See item #18 for additional details.**
 - e. Map # and Parcel # of site location
 - **Map: 065 Parcel – 036.01**
4. Site boundary, stamped and signed by a registered surveyor.
 - **Shown on survey plan sheets (G0.03, G0.04 – ARC Surveying and Mapping, Inc.) and the separate property survey (item 2 in Supporting Documents folder).**
5. The shape, size and location of all existing buildings on the lot.
 - **No existing buildings are located on site.**

6. The existing and intended use of the lot and of structures on it. If residential, give the number of dwelling units per building.
 - **Existing property is undeveloped. Intended use is to provide a barge loading/unloading and material storage facility for large steel wire coils.**
7. Topographic survey of the site with contour intervals at no greater than 5' intervals, stamped and signed by a registered surveyor.
 - **Shown on survey plan sheets (G0.03, G0.04).**
8. Location of all driveways and entrances with dimensions from the centerline of the drive to the nearest property corner and to the nearest intersection (if the intersection is closer than 200 feet).
 - **Detail on survey plan sheets (G0.03, G0.04) and sheet C2.00.**
9. Dimensioned layout and location of all parking spaces including handicapped spaces.
 - **Parking spaces are 18-ft long by 10-ft wide. 13 regular spaces with 2 handicap spaces at the warehouse facility and 8 regular spaces with 2 handicap spaces at the office building – Shown on plan sheet C1.00.**
10. Dimensioned layout and location of off-street loading bays and docks.
 - **Final design for marine dock is not finalized at this time. Separate submittal will be provided to the City in the future for consideration and review. Approximate location provided on plan sheet C1.00.**
11. Location and area of open space.
 - **Open space includes wetlands in the northern portion of the site to remain undisturbed – Shown on plan sheet C1.00.**
12. A table showing the ground coverage, total floor area and building heights.
 - **Ground coverage – Disturbed area is 15.62AC. Total floor area – 53,000 s.f. (50,000 s.f. warehouse plus 3,000 s.f. office). Warehouse building height 36-ft and 39-ft. Detail on plan sheet C1.00 and in the separate Nucor, Inc preliminary building plans provided. Future building permit submittal to be provided for consideration and review once building design is finalized.**
13. Location, dimension and heights of all fences and walls with materials specified.
 - **A gate will be installed at the driveway along Trinity Rd. Natural vegetation will remain along the north, east, and southern boundaries. No fence is expected to be installed.**
14. Location, type and amount of landscaping.
 - **Trees and shrubbery to be included near warehouse and office buildings – Preliminary layout shown on plan sheet L1.00.**
15. Proposed means of surface drainage, including locations and sizes of all culverts, ditches and detention structures, storm-water system to be designed as per the requirements of the Ashland City Planning Commission.
 - **Site grading is to provide sheet flow of water towards natural wetlands to remain on the north side of the site. Shot rock material is to be used for site fill and facilitates**



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surface drainage. Riprap splash pads will be used to disperse sheet flow to prevent erosion – Shown on plan sheet C2.00.

- **Driveway culvert details shown on S1.00 (Sized to exceed the 100-year storm discharge)**

16. Dimensioned location of all easements and right-of-ways.

- **Shown on attached ALTA survey by Denham Land Surveyors, LLC.**
- **Also shown on plan sheet C1.00**

17. Location of all portions of the site that are within the floodway and the 100-year floodplain. A note will be included which gives the FEMA map number from which this information was developed. In addition, if portions of the site are in the 100-year floodplain and/or the floodway, the 100-year flood elevation(s) at the site will be listed on the plan.

- **Shown on the survey plan sheets (old FEMA data that was in effect at time of survey), as well as the plan sheet C1.00, which shows the updated FEMA data.**
- **A hydraulic report and No-Rise Certification, along with supporting technical data, are included with this submittal for the City's review.**

18. Location, size and distance to all public utilities serving the site including all fire hydrants.

- **6" Public water main is located along Trinity Road. This line was tested for flow and pressure by a third-party contractor. Sufficient supply available for project. Details on plan sheet C4.00. Flow testing data follows:**
 - Public hydrant @ Trinity Rd & Hwy 12 on 3/9/2022 at 12:15 pm
 - Static – 98 psi
 - Residual – 64 psi
 - Pitot 70 psi
 - GPM – 1,405 gpm
 - Public hydrant @ end of Trinity Rd on 3/9/2022 at 12:40 pm
 - Static – 106 psi
 - Residual 58 psi
 - Pitot – 52 psi
 - Flow – 1,230 gpm
- **Approximate location of the public sewer provided on plan sheet C4.00. Spot excavations to physically locate the force main have proven to be unsuccessful. Additional coordination with Ashland Public Works is ongoing. Preliminary proposed sewer force main rerouting is detailed on plan sheet C4.00.**
- **Electrical service to the facility will be provided by CEMC from overhead line along Trinity Road. Details are shown on sheet E1.00.**

19. Location, by type and size of all proposed signs, (Please note that signs larger than 40 sq. ft. are not permitted per the sign ordinance for the Town of Ashland City.

- **The sign for the location will be added beneath the existing ARCOSA sign currently located along Hwy 12 at the end of Trinity Road. We have included details associated with the existing sign and the proposed sign addition within this package.**

20. Vegetation, show at minimum the following:

- a. Existing tree masses and hedgerows
- b. General description of the tree types and sizes within the tree masses
- c. Location and identification of trees 15" in caliper (measured 4' above the ground) or larger
- d. Description of landscaping requirements for the site based upon surrounding land uses (see Zoning Ordinance Section 3, 140)
 - **Facility has previously been cleared. Areas with existing vegetation along the northern and eastern boundaries are to remain. Tree survey has been conducted by Denham Surveying but is not able to be provided at this time. Additional submittal to be forthcoming.**

21. Identification of slopes greater than 15% and identification of those soils (SCS soil mapping is acceptable) on those slopes.

- **No slopes greater than 15% on site.**

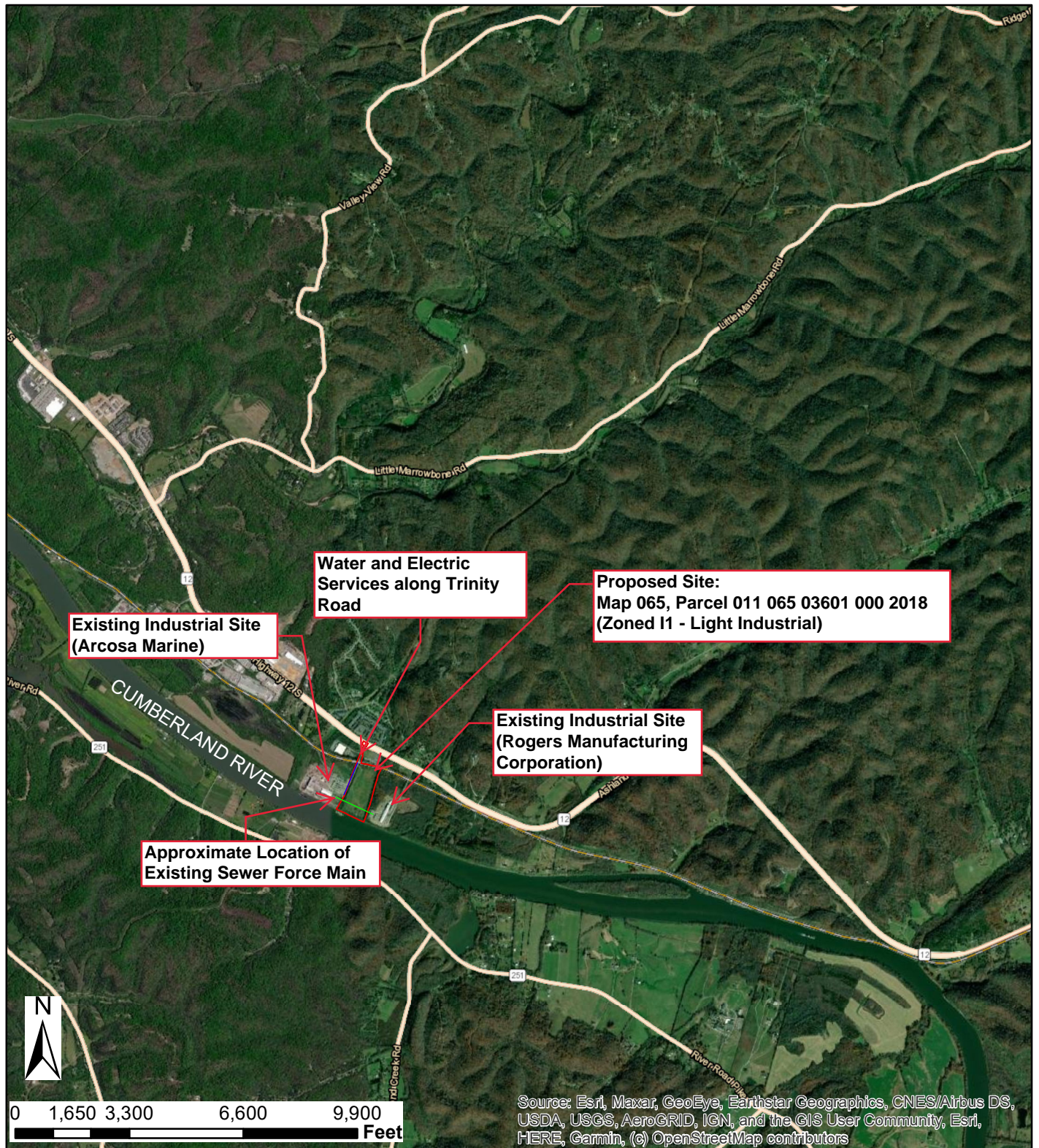


FIGURE 1 - SITE LOCATION MAP

**Storm Water Pollution Prevention Plan
Cumberland River Terminal
Parker Towing Company**

Prepared By:



Date: 03/15/2022



VICINITY MAP -- NOT TO SCALE



GRAPHIC SCALE
1" = 50'

Survey Datum:

1. The horizontal datum is based on the Tennessee State Plane Coordinate System (NAD1983)
2. The vertical datum is based on the NAVD88 vertical datum
3. All distances, coordinates, and bearings are based on grid values



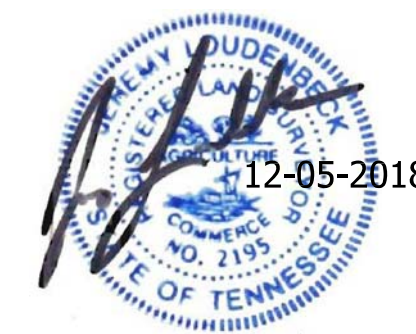
Symbol Legend			
☆ LP	Light Pole	○	Property Corner
■	curb drain inlet	□	sewer clean out
⊙	Sewer Man Hole	—	Fence line
⊕	Drainage Man Hole	—	Under Ground Electric
⊙	Gas Meter	—	Drain line
⊙	Electric Meter	—	Major Contours
⊙	Water Meter	—	Minor Contours
⊙	Power Pole	—	Guy Anchor
⊙	PIV valve	■	Building
⊙	water valve	■	Concrete
⊙	fire hydrant	■	Grass
PMT	Pad-Mount Trans.	---	Delineated Wetland
A/C	Air Conditioner		

Survey Notes:

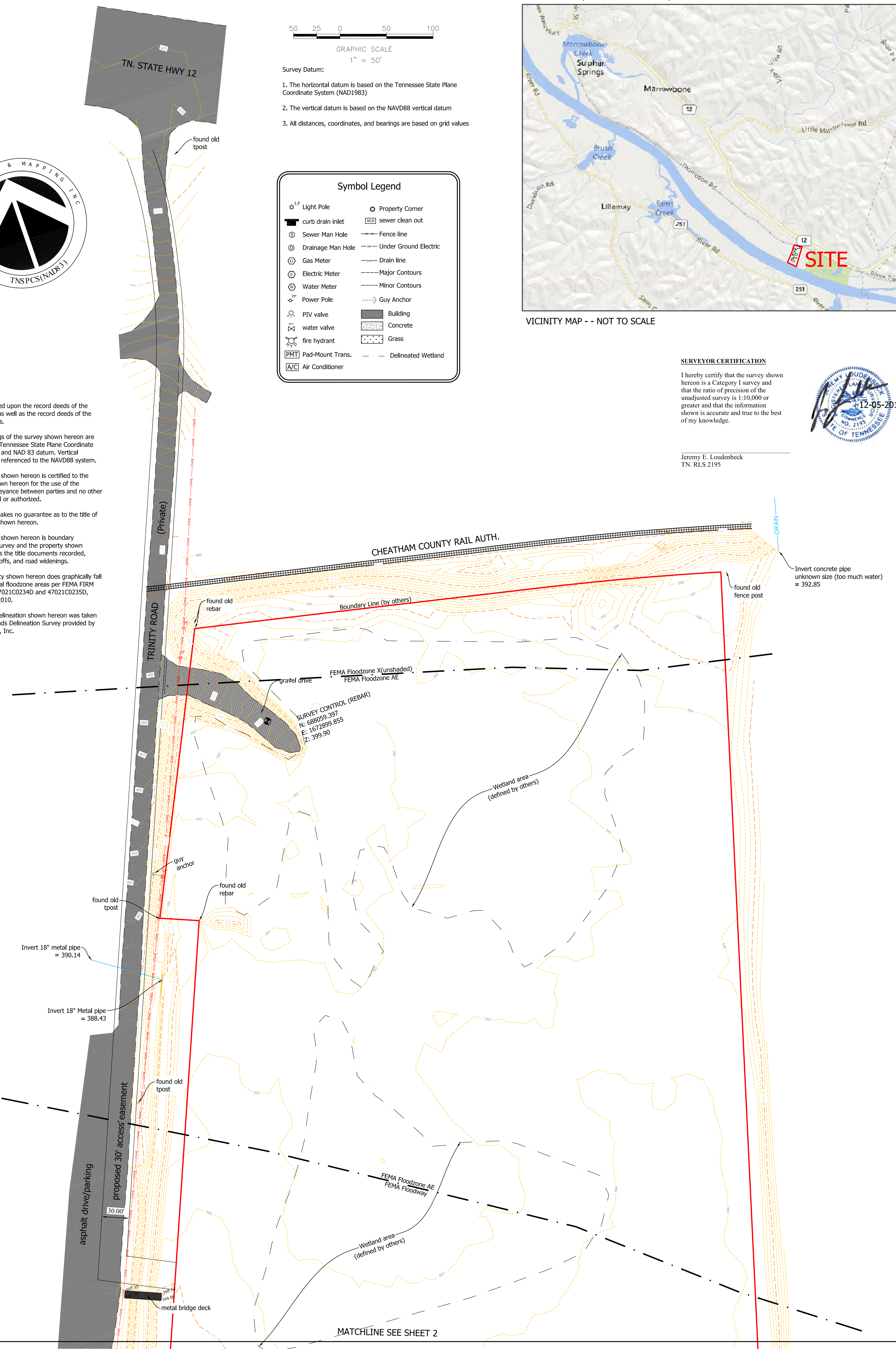
1. Survey based upon the record deeds of the subject tract as well as the record deeds of the adjacent tracts.
2. The bearings of the survey shown hereon are based on the Tennessee State Plane Coordinate System (GPS) and NAD 83 datum. Vertical information is referenced to the NAVD88 system.
3. The survey shown hereon is certified to the assignees shown hereon for the use of the property conveyance between parties and no other use is certified or authorized.
4. This firm makes no guarantee as to the title of the property shown hereon.
5. The survey shown hereon is boundary retracement survey and the property shown hereon reflects the title documents recorded, including sell-offs, and road widenings.
6. The property shown hereon does graphically fall within a special floodzone areas per FEMA FIRM MAP Panels 47021C0234D and 47021C0235D, dated 09/27/2010.
7. Wetlands delineation shown hereon was taken from a Wetlands Delineation Survey provided by Lord & Winter, Inc.

SURVEYOR CERTIFICATION

I hereby certify that the survey shown hereon is a Category I survey and that the ratio of precision of the unadjusted survey is 1:10,000 or greater and that the information shown is accurate and true to the best of my knowledge.



Jeremy E. Loudonbeck
TN. RLS 2195



NOTE:
Boundary and Easements shown performed by others. ARC performed hydrographic survey and topographic information only.

SHEET: 1 OF: 2
ARC Project No.: 18-11-08M
File Name: Shase
File Date: 05/02/2018
Plot Date: 05/02/2018
Scale: 1" = 50'
Drawn By: SBU



Arc Surveying & Mapping, Inc.
Professional Hydrographers • Surveyors • Mappers
5567 Commander Drive, Suite 101
Arlington, TN. 38002
901.867.5333

Prepared For:
Neel-Schafer Inc.
210 25th Avenue North, Suite 800
Nashville, TN. 37203

Project Title:
Topographic Survey
Parker Towing site
Ashland City, TN. Cumberland River

DATE	BY	REVISION

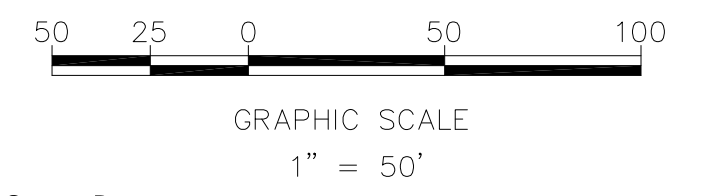
Survey Notes:

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Jeremy E. Loudonbeck
TN. RLS 2195



Survey Datum:

1. The horizontal datum is based on the Tennessee State Plane Coordinate System (NAD1983)
2. The vertical datum is based on the NAVD88 vertical datum
3. All distances, coordinates, and bearings are based on grid values

MATCHLINE SEE SHEET 1

Symbol Legend	
⊙ LP	Light Pole
⊙	Property Corner
⊠	curb drain inlet
⊠	sewer clean out
⊙	Sewer Man Hole
⊙	Fence line
⊙	Drainage Man Hole
⊙	Under Ground Electric
⊙	Gas Meter
⊙	Drain line
⊙	Electric Meter
⊙	Major Contours
⊙	Water Meter
⊙	Minor Contours
⊙	Power Pole
⊙	Guy Anchor
⊙	PIV valve
⊙	Building
⊙	water valve
⊙	Concrete
⊙	Grass
⊙	fire hydrant
⊙	Delineated Wetland
PMT	Pad-Mount Trans.
A/C	Air Conditioner

Invert 18" metal pipe = 389.48

Concrete endwall
Invert 18" metal pipe = 387.37



origin of pvc sewer line is unknown

Exist. Sewer Manhole
Rim = 395.21
invert 6" pvc = 390.00

Exist. Sewer Clean Out
invert 6" pvc = 390.33

water surface = 385.18

SURVEY CONTROL (REBAR)
N: 686652.868
E: 1673983.951
Z: 401.01

CUMBERLAND RIVER
Flow

NOTE:

Boundary and Easements shown performed by others. ARC performed hydrographic survey and topographic information only.

SHEET: 2 OF: 2	ARC Project No.: 18-11-08M	File Name: Share	Scale: 1" = 50'	Drawn By: SBU
	Fid Bk / Pk: xxxxxx	Plot Date: 05/02/2018		



Arc Surveying & Mapping, Inc.
Professional Hydrographers • Surveyors • Mappers
5567 Commander Drive, Suite 101
Arlington, TN. 38002
901.867.5333

Prepared For:
Neel-Schafer Inc.
210 25th Avenue North, Suite 800
Nashville, TN. 37203

Project Title:
Topographic Survey
Parker Towing site
Ashland City, TN. Cumberland River

DATE	BY	REVISION



PROJECT NUMBER: S1F-19023 R5
 PROJECT NAME: Parker Towing
 PROJECT LOCATION: Ashland City, TN COUNTY: Cheatham
 CUSTOMER: Carter Group, LLC Nashville, TN



PROJECT LOADS

DESIGN CODE: IBC 2018 BUILDING END USE: 2B
 ROOF LIVE LOAD: 20 PSF MBMA OCC. CLASS: II - Standard Buildings
 GROUND SNOW LOAD: 10 PSF SNOW EXP. FACTOR, Ce: 0.9
 SNOW IMPORTANCE FACTOR, Is: 1
 WIND: 115 WIND IMPORTANCE FACTOR, Iw: 1
 EXPOSURE: C WITHIN HURRICANE COASTLINE YES NO
 UL 90 YES NO RAIN INTENSITY (in/hr) 6

GENERAL NOTES

1. MATERIALS	ASTM DESCRIPTION	MATERIALS	ASTM DESCRIPTION
STRUCTURAL STEEL PLATE	A529 / A572 / A1011	ROOF AND WALL SHEETING	A653 / A792
HOT ROLLED MILL SHAPES	A36 / A529 / A572 / A500	BOLTS	A307 / A325 / A490
HSS ROUND	A500	CABLE	A475
HSS RECTANGULAR	A500	RODS	A529 / A572
COLD FORM SHAPES	A653 / A1011		

2. **STRUCTURAL PRIMER NOTES:**
 SHOP COAT PRIMER IS INTENDED TO PROTECT THE STEEL FRAMING FOR A SHORT PERIOD OF TIME. STORAGE IN EXTREME COLD TEMPERATURES OR WINTER SNOW CONDITIONS, INCLUDING TRANSPORTATION ON SALTED OR CHEMICALLY TREATED ROADS WILL ADVERSELY AFFECT THE DURABILITY AND LONGEVITY OF THE PRIMER. THE COAT OF SHOP PRIMER DOES NOT PROVIDE THE UNIFORMITY OF APPEARANCE, OR THE DURABILITY AND CORROSION RESISTANCE OF A FIELD APPLIED FINISH COAT OF PAINT OVER A SHOP PRIMER. MINOR ABRASIONS TO THE SHOP COAT PRIMER CAUSED BY HANDLING, LOADING, SHIPPING, UNLOADING AND ERECTION ARE UNAVOIDABLE AND ARE NOT THE RESPONSIBILITY OF THE METAL BUILDING MANUFACTURER. METAL BUILDING MANUFACTURER IS NOT RESPONSIBLE FOR THE DETERIORATION OF THE PRIMER OR CORROSION THAT MAY RESULT FROM ATMOSPHERIC AND ENVIRONMENTAL CONDITIONS NOR THE COMPATIBILITY OF THE PRIMER TO ANY FIELD APPLIED COATING.

3. **BUILDING ERECTION NOTES:**
 THE GENERAL CONTRACTOR AND/OR ERECTOR IS RESPONSIBLE TO SAFELY AND PROPERLY ERECT THE METAL BUILDING SYSTEM IN CONFORMANCE WITH THESE DRAWINGS, OSHA REQUIREMENTS AND EITHER MBMA OR CSA S16 STANDARDS PERTAINING TO PROPER ERECTION. TEMPORARY SUPPORTS SUCH AS GUYS, BRACES, FALSEWORK, CRIBBING OR OTHER ELEMENTS FOR ERECTION ARE TO BE DETERMINED, FURNISHED AND INSTALLED BY THE ERECTOR. THESE SUPPORTS MUST SECURE THE STEEL FRAMING, OR PARTLY ASSEMBLED STEEL FRAMING, AGAINST LOADS COMPARABLE IN INTENSITY TO THOSE FOR WHICH THE STRUCTURE WAS DESIGNED IN ADDITION TO LOADS RESULTING FROM THE ERECTION OPERATION. SECONDARY WALL AND ROOF FRAMING (PURLINS, GIRTS AND/OR JOIST) ARE NOT DESIGNED TO FUNCTION AS A WORKING PLATFORM OR TO PROVIDE AS AN ANCHORAGE POINT FOR A FALL ARREST /SAFETY TIE OFF.\P

4. **A325 & A490 BOLT TIGHTENING REQUIREMENTS:**
 IT IS THE RESPONSIBILITY OF THE ERECTOR TO ENSURE PROPER BOLT TIGHTNESS IN ACCORDANCE WITH APPLICABLE REGULATIONS. FOR PROJECTS IN THE UNITED STATES SEE THE RCSC SPECIFICATION FOR STRUCTURAL JOINTS USING A325 OR A490 BOLTS OR FOR PROJECTS IN CANADA, SEE THE CAN/CSA S16 LIMIT STATES DESIGN OF STEEL STRUCTURES FOR MORE INFORMATION.
 THE FOLLOWING CRITERIA MAY BE USED TO DETERMINE THE BOLT TIGHTNESS (I.E., "SNUG-TIGHT" OR "FULLY-PRETENSIONED"), UNLESS REQUIRED OTHERWISE BY LOCAL JURISDICTION OR CONTRACT REQUIREMENTS:
 A) ALL A490 BOLTS SHALL BE "FULLY-PRETENSIONED".
 B) ALL A325 BOLTS IN PRIMARY FRAMING (RIGID FRAMES AND BRACING) MAY BE "SNUG-TIGHT", EXCEPT AS FOLLOWS: "FULLY-PRETENSION" A325 BOLTS IF:
 a) BUILDING SUPPORTS A CRANE SYSTEM WITH A CAPACITY GREATER THAN 5 TONS.
 b) BUILDING SUPPORTS MACHINERY THAT CREATES VIBRATION, IMPACT OR STRESS-REVERSALS ON THE CONNECTIONS. THE ENGINEER-OF-RECORD FOR THE PROJECT SHOULD BE CONSULTED TO EVALUATE FOR THIS CONDITION.
 c) THE PROJECT SITE IS LOCATED IN A HIGH SEISMIC AREA. FOR IBC-BASED CODES, "HIGH SEISMIC AREA" IS DEFINED AS "SEISMIC DESIGN CATEGORY" OF "D", "E", OR "F". SEE THE "BUILDING LOADS" SECTION OF THIS PAGE FOR THE DEFINED SEISMIC DESIGN CATEGORY FOR THIS PROJECT.
 d) ANY CONNECTION DESIGNATED IN THESE DRAWINGS AS "A325-SC". "SLIP-CRITICAL (SC)" CONNECTIONS MUST BE FREE OF PAINT, OIL, OR OTHER MATERIALS THAT REDUCE FRICTION AT CONTACT SURFACES. GALVANIZED OR LIGHTLY RUSTED SURFACES ARE ACCEPTABLE.
 C) IN CANADA, ALL A325 AND A490 BOLTS SHALL BE "FULLY PRE-TENSIONED", EXCEPT FOR SECONDARY MEMBERS (PURLINS, GIRTS, OPENING FRAMING, ETC.) AND FLANGE BRACES.
 SECONDARY MEMBERS (PURLINS, GIRTS, OPENING FRAMING, ETC.) AND FLANGE BRACE CONNECTIONS MAY ALWAYS BE "SNUG-TIGHT", UNLESS INDICATED OTHERWISE IN THESE DRAWINGS.

5. **GENERAL DESIGN NOTES:**
 1) ALL STRUCTURAL STEEL SECTIONS AND WELDED PLATE MEMBERS ARE DESIGNED IN ACCORDANCE WITH ANSI/AISC 360 "SPECIFICATIONS FOR STRUCTURAL STEEL BUILDINGS" OR THE CAN/CSA S16 "LIMIT STATES DESIGN OF STEEL STRUCTURES", AS REQUIRED BY THE SPECIFIED BUILDING CODE.
 2) ALL WELDING OF STRUCTURAL STEEL IS BASED ON EITHER AWS D1.1 "STRUCTURAL WELDING CODE - STEEL" OR CAN/CSA W59 "WELDED STEEL CONSTRUCTION (METAL ARC WELDING)", AS REQUIRED BY THE SPECIFIED BUILDING CODE.
 3) ALL COLD FORMED MEMBERS ARE DESIGNED IN ACCORDANCE WITH ANSI/AISI 100 OR THE CAN/CSA S136 "SPECIFICATIONS FOR THE DESIGN OF COLD FORMED STEEL STRUCTURAL MEMBERS", AS REQUIRED BY THE SPECIFIED BUILDING CODE.
 4) ALL WELDING OF COLD FORMED STEEL IS BASED ON AWS D1.3 "STRUCTURAL WELDING CODE - SHEET STEEL" OR CAN/CSA W59 "WELDED STEEL CONSTRUCTION (METAL ARC WELDING)", AS REQUIRED BY THE SPECIFIED BUILDING CODE.
 5) THIS MANUFACTURING FACILITY IS IAS AC-472 ACCREDITED AND CAN/CSA A660 AND W47.1 CERTIFIED (IF APPLICABLE) FOR THE DESIGN AND MANUFACTURING OF METAL BUILDING SYSTEMS.
 6) IF JOISTS ARE INCLUDED WITH THIS PROJECT, THEY ARE SUPPLIED AS A PART OF THE SYSTEMS ENGINEERED METAL BUILDING AND ARE FABRICATED IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 1926.758 OF OSHA SAFETY STANDARDS FOR STEEL ERECTION, DATED JANUARY 18, 2001.

6. **GLOSSARY OF ABBREVIATIONS:**

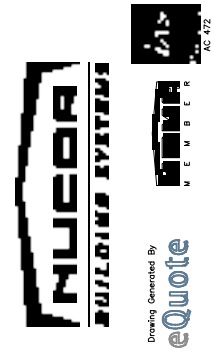
A.B. = ANCHOR BOLTS	Max = MAXIMUM	Req'd = REQUIRED
BS = BOTH SIDES	M.B. = MACHINE BOLTS	Rev. = REVISION
B.U. = BUILT-UP	MBS = METAL BUILDING SUPPLIER	SIM = SIMILAR
Dia = DIAMETER	Min = MINIMUM	SL = STEEL LINE
Flg = FLANGE	N/A = NOT APPLICABLE	SLV = SHORT LEG VERTICAL
F.S. = FAR SIDE	NIC = NOT IN CONTRACT	TBD = TO BE DETERMINED
Ga. = GAUGE	N.S. = NEAR SIDE	Typ = TYPICAL
H.S.B. = HIGH STRENGTH BOLTS	O.A.L. = OVERALL LENGTH	U.N.O. = UNLESS NOTED OTHERWISE
Ht. = HEIGHT	O.C. = ON CENTER	
LLV = LONG LEG VERTICAL	BS = BOTH SIDES	

?? = PART MARK TO BE DETERMINED AND WILL BE UPDATED ON FOR CONSTRUCTION DRAWINGS

SEISMIC INFORMATION Ss:0.41, S1:0.149
 Design Sds/Sd1: _____ Site Class: E
 Seismic Imp. Factor Ie: 1 Seismic Design Category: _____
Analysis Procedure: Equivalent Lateral Force Method
Basic SFRS:

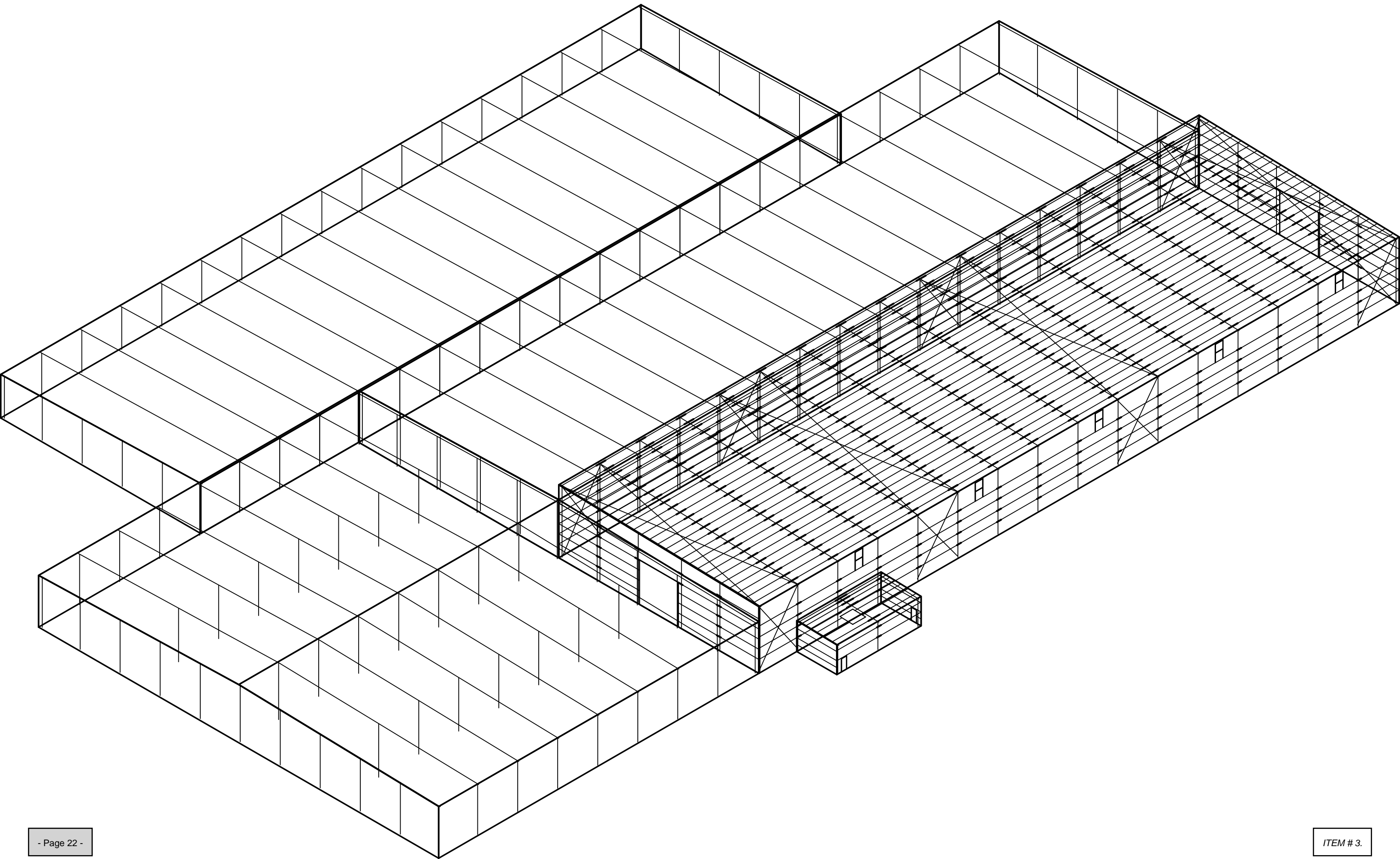
NOTES:
 1) COLLATERAL DEAD LOADS, UNLESS OTHERWISE NOTED, ARE ASSUMED TO BE UNIFORMLY DISTRIBUTED. WHEN SUSPENDED SPRINKLER SYSTEMS, LIGHTING, HVAC EQUIPMENT, CEILINGS, ETC., ARE SUSPENDED FROM ROOF MEMBERS, CONSULT THE M.B.S. IF THESE CONCENTRATED LOADS EXCEED 200 POUNDS, OR IF INDIVIDUAL MEMBERS ARE LOADED SIGNIFICANTLY MORE THAN OTHERS.
 2) THE DESIGN OF STRUCTURAL MEMBERS SUPPORTING GRAVITY LOADS IS CONTROLLED BY THE MORE CRITICAL EFFECT OF ROOF LIVE LOAD OR ROOF SNOW LOAD, AS DETERMINED BY THE APPLICABLE CODE.

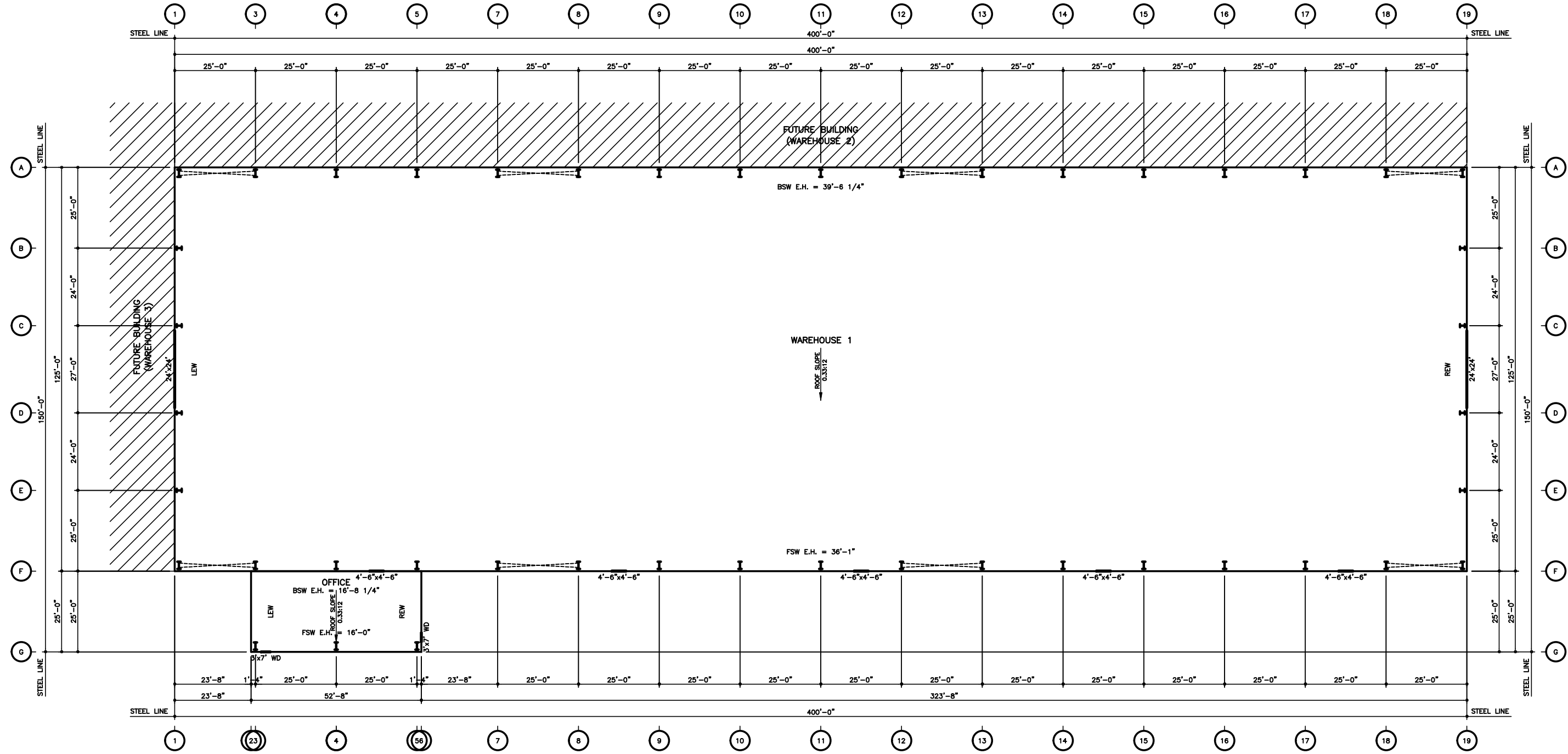
	BUILDING Warehouse	BUILDING Warehouse	BUILDING Warehouse	BUILDING Warehouse	BUILDING Office
ROOF DEAD (PSF):	2.4				3.4
PRI. COL. (PSF):	5	5	5	5	10
SEC. COL. (PSF):	5	5	5	5	10
SNOW Ct:	1.2	1.2	1.2	1.2	1
SNOW Cs:					
ROOF SNOW (PSF):	10	10	10	10	10
WIND ENCLOSURE:	Enclosed	Enclosed	Enclosed	Enclosed	Enclosed
GCpi:					
SEISMIC R:					
SEISMIC Cs:					
BASE SHEAR (KIPS):					



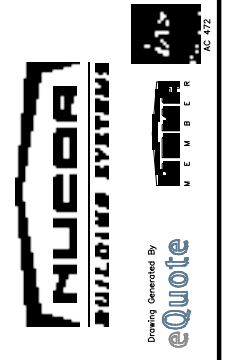
PROJECT NAME: **PARKER TOWING**
 ASHLAND CITY, TN
 CUSTOMER NAME: **CARTER GROUP, LLC**
 NASHVILLE, TN

DO NOT USE FOR FINAL CONSTRUCTION
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 NARY COVERSHEET DRAWING
 QUOTE NUMBER: **S1F-19023 R5**
 SHEET PRE SHEET
 ITEM # 3.
 C1



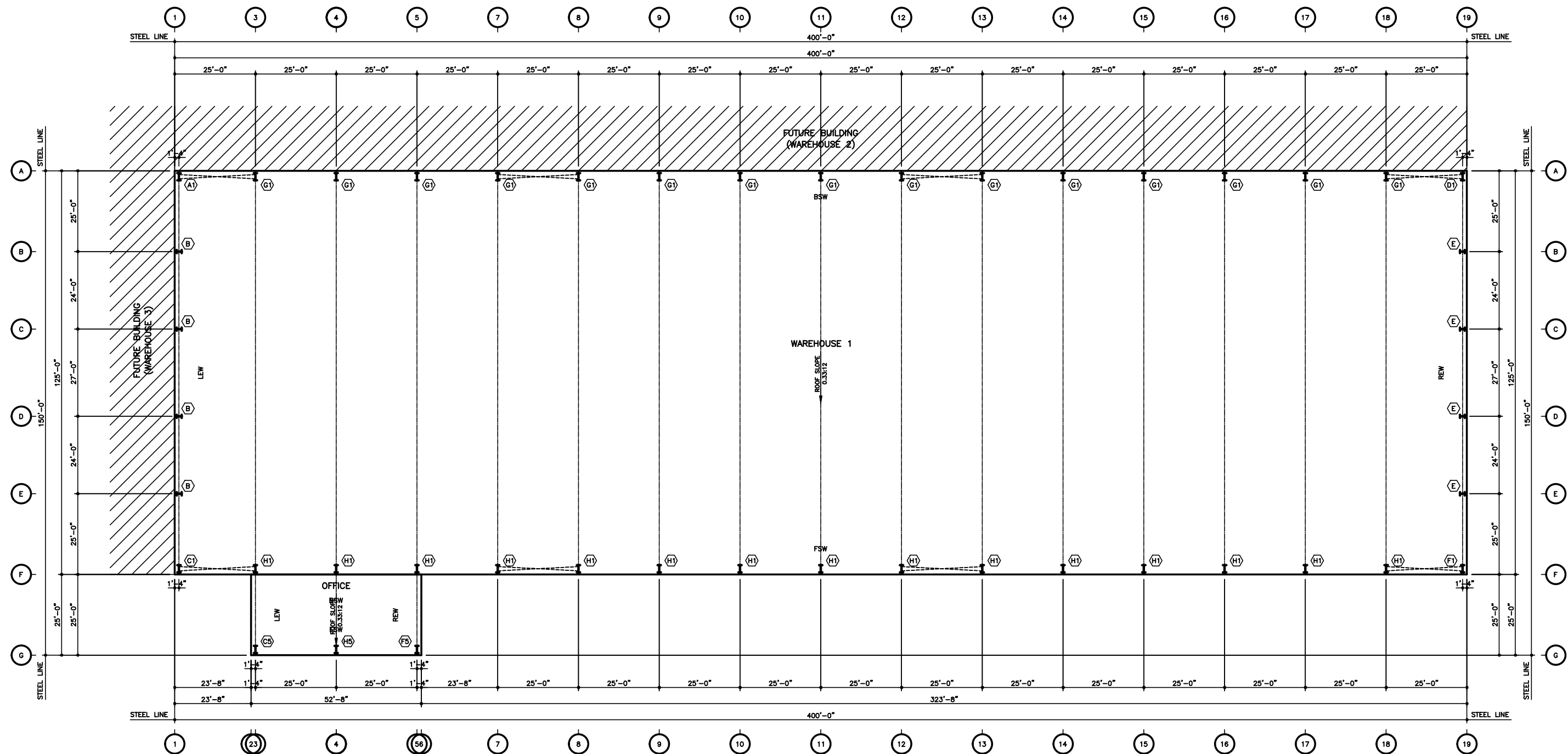


Column locations marked with "*" are designated as Most Economical (Pipe, Tube, or I-Shape) and will be determined at final design.

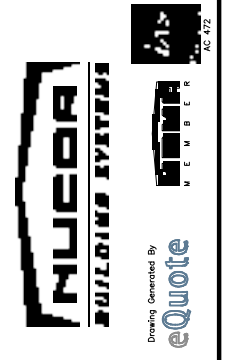


PROJECT NAME:
PARKER TOWING
 ASHLAND CITY, TN
 CUSTOMER NAME:
CARTER GROUP, LLC
 NASHVILLE, TN

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PRINARY FLOOR PLAN
 QUOTE NUMBER:
S1F-19023 R5
 FP1
ITEM # 3.



Finish floor elevation assumed to be 100'-0" unless noted otherwise.
 Column locations marked with * are designated as Most Economical (Pipe, Tube, or I-Shape) and will be determined at final design.



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 CUSTOMER NAME:
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 NASHVILLE, TN

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 1/10/2022 1:18 PM
 NARY ANCHOR BOLT PLAN
 QUOTE NUMBER:
 S1F-19023 R5
 AB1
 ITEM # 3.

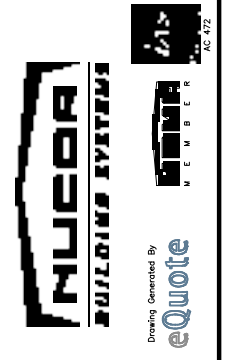
ITEM # 3.

ANCHOR BOLT PLAN

GENERAL NOTES

1. THE SPECIFIED ANCHOR ROD DIAMETER ASSUMES F1554 GRADE 36 UNLESS NOTED OTHERWISE. ANCHOR ROD MATERIAL OF EQUAL DIAMETER MEETING OR EXCEEDING THE STRENGTH REQUIREMENTS SET FORTH ON THESE DRAWINGS MAY BE UTILIZED AT THE DISCRETION OF THE FOUNDATION DESIGN ENGINEER. ANCHOR ROD EMBEDMENT LENGTH SHALL BE DETERMINED BY THE FOUNDATION DESIGN ENGINEER.
2. NUCOR BUILDING SYSTEMS IS NOT RESPONSIBLE FOR PROJECT FOUNDATION DESIGN. THE FOUNDATION DESIGN IS THE RESPONSIBILITY OF A REGISTERED PROFESSIONAL ENGINEER, FAMILIAR WITH LOCAL SITE CONDITIONS.
3. ALL ANCHOR RODS, FLAT WASHERS FOR ANCHOR RODS, EXPANSION BOLTS, AS WELL AS ALL CONCRETE/MASONRY EMBED PLATES ARE NOT BY NUCOR BUILDING SYSTEMS.
4. THIS DRAWING IS NOT TO SCALE.
5. FINISHED FLOOR ELEVATION = 100'-0" UNLESS NOTED OTHERWISE.
6. "SINGLE" CEE COLUMNS SHALL BE ORIENTED WITH THE "TOES" TOWARD THE LOW EAVE UNLESS NOTED OTHERWISE.

BERTNOTE



PROJECT NAME:
PARKER TOWING
ASHLAND CITY, TN

CUSTOMER NAME:
CARTER GROUP, LLC
NASHVILLE, TN

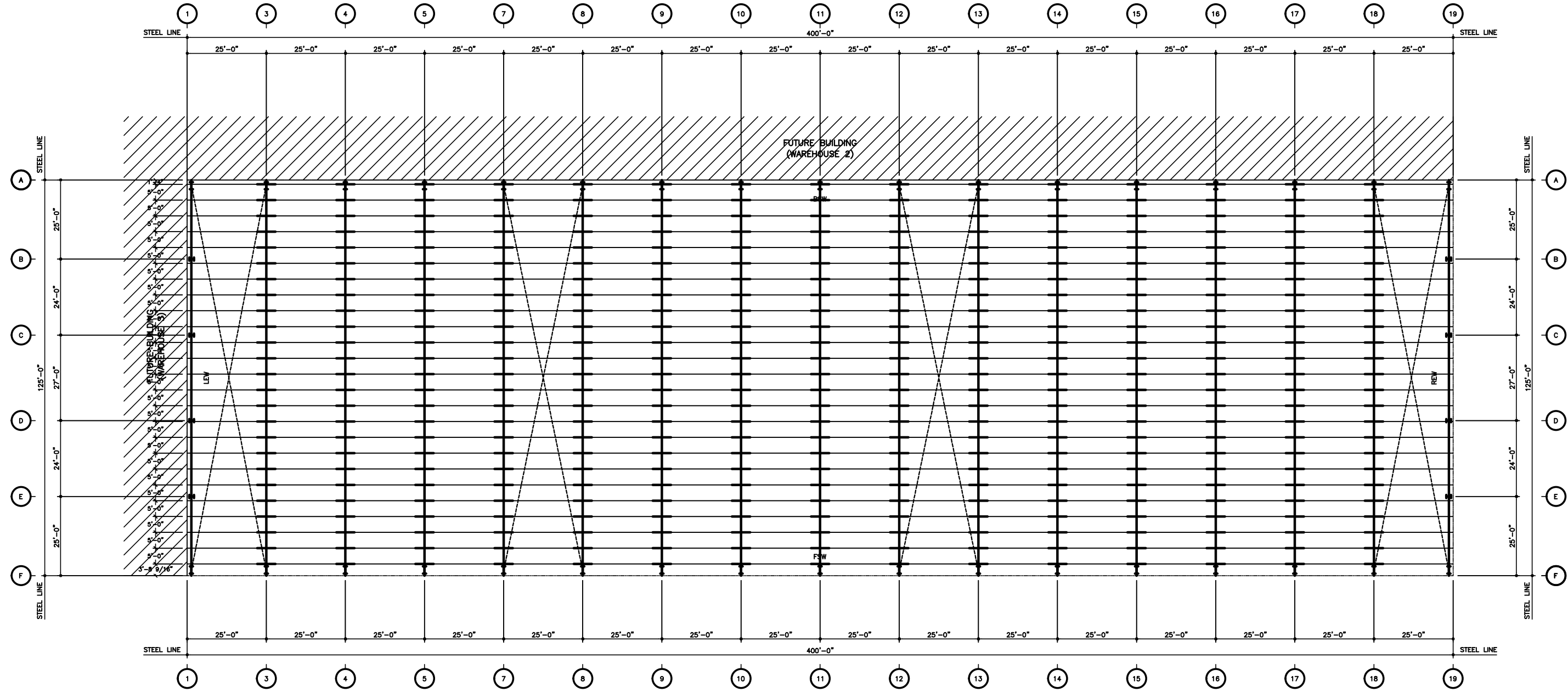
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PRIMARY ANCHOR BOLT DETAILS

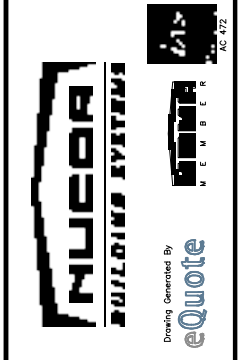
QUOTE NUMBER:
S1F-19023 R5

ITEM # 3

AB1 OF 1



WAREHOUSE 1 ROOF FRAMING PLAN



PROJECT NAME:
PARKER TOWING
 ASHLAND CITY, TN

CUSTOMER NAME:
CARTER GROUP, LLC
 NASHVILLE, TN

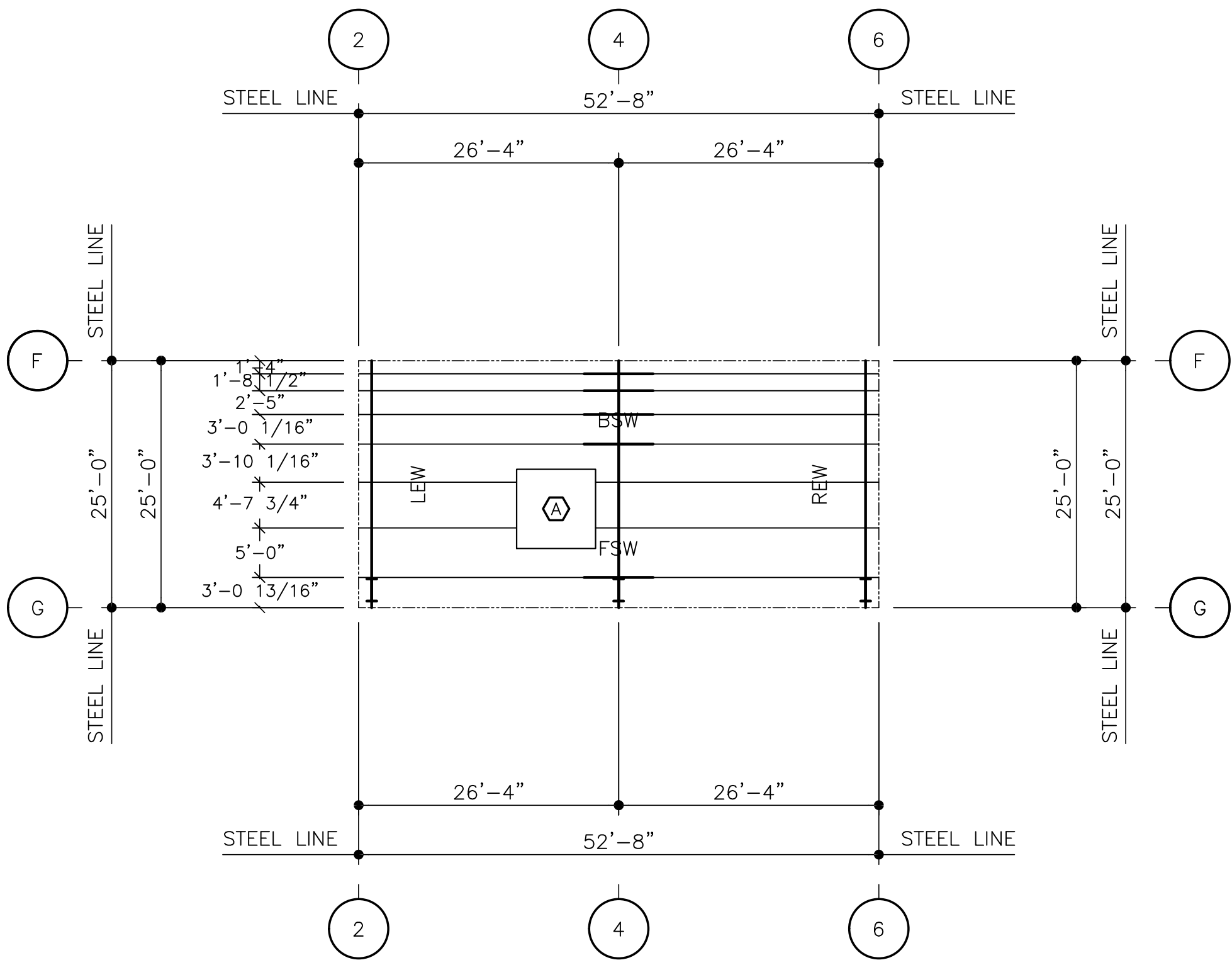
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SHEET PREPARED: 1/10/2022 1:18 PM
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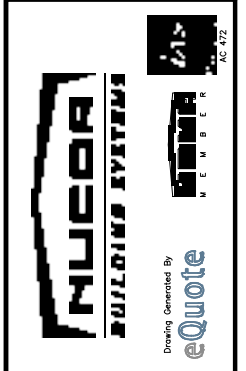
ITEM # 3

QUOTE NUMBER: **S1F-19023 R5**
 SHEET NUMBER: **RF1**

FRAMED OPENING SCHEDULE							
(D)	QTY	WIDTH	LENGTH	WEIGHT (lbs)	LOADS ONLY	LOC. FROM LEW	LOC. FROM FSW
A	1	8'-0"	8'-0"	2500	NO	20'-0"	10'-0"



OFFICE ROOF FRAMING PLAN



PROJECT NAME:
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 NASHVILLE, TN

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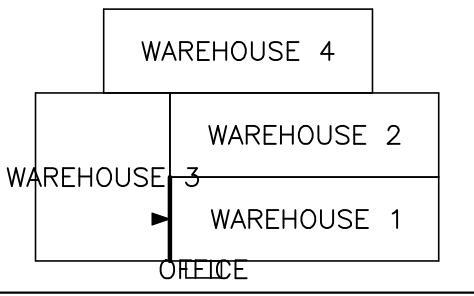
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PRIMARY ROOF FRAMING PLAN

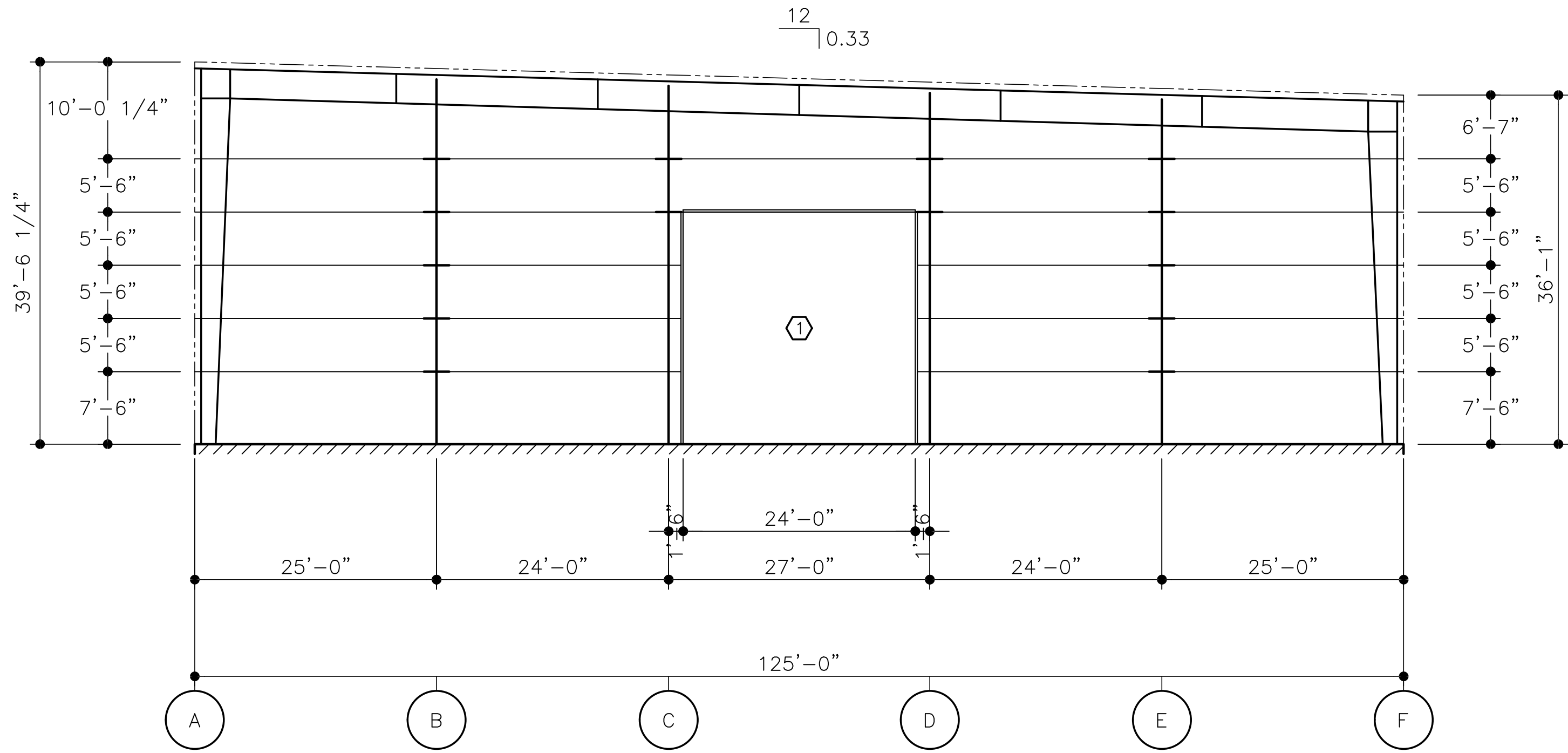
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S1F-19023 R5

RF5

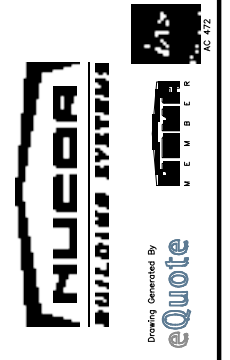
KEY PLAN



FRAMED OPENING SCHEDULE					
ID	QTY	WIDTH	HEIGHT	SILL HEIGHT	LOCATED
1	1	24'-0"	24'-0"	0'-0"	FACTORY



ELEVATION AT LINE 1



PROJECT NAME:
PARKER TOWING
ASHLAND CITY, TN
CUSTOMER NAME:
CARTER GROUP, LLC
NASHVILLE, TN

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1/10/2022 1:18 PM

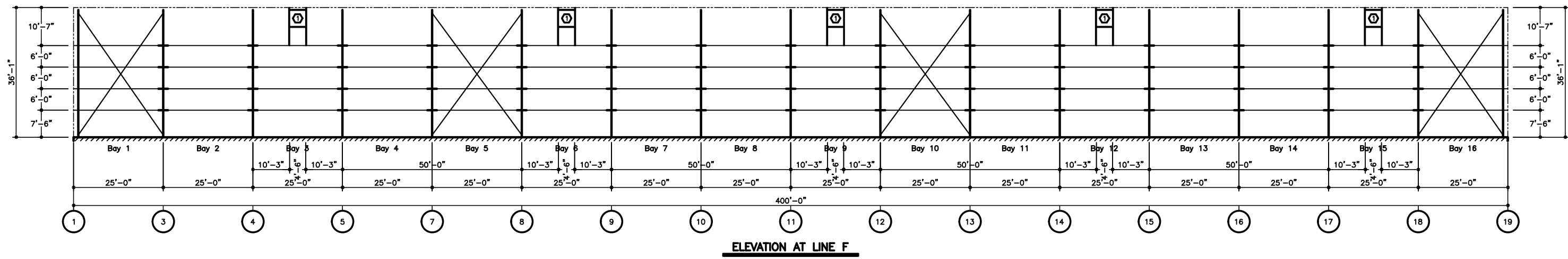
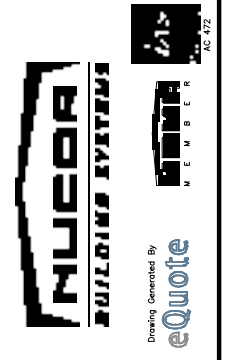
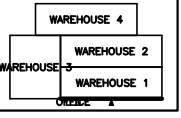
PRELIMINARY STRUCTURAL ELEVATIONS

QUOTE NUMBER:
S1F-19023 R5

ITEM # 3

ST5

FRAMED OPENING SCHEDULE					
NO.	QTY	WIDTH	HEIGHT	SILL HEIGHT	LOCATED
1	5	4'-6"	4'-6"	30'-10"	FACTORY



PROJECT NAME:
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CUSTOMER NAME:
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 NASHVILLE, TN

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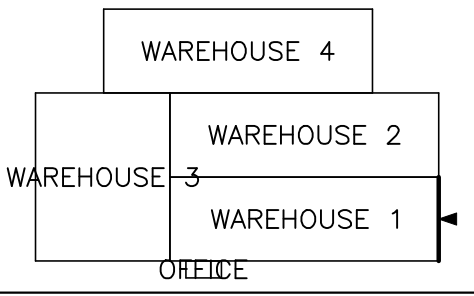
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PRIMARY STRUCTURAL ELEVATIONS

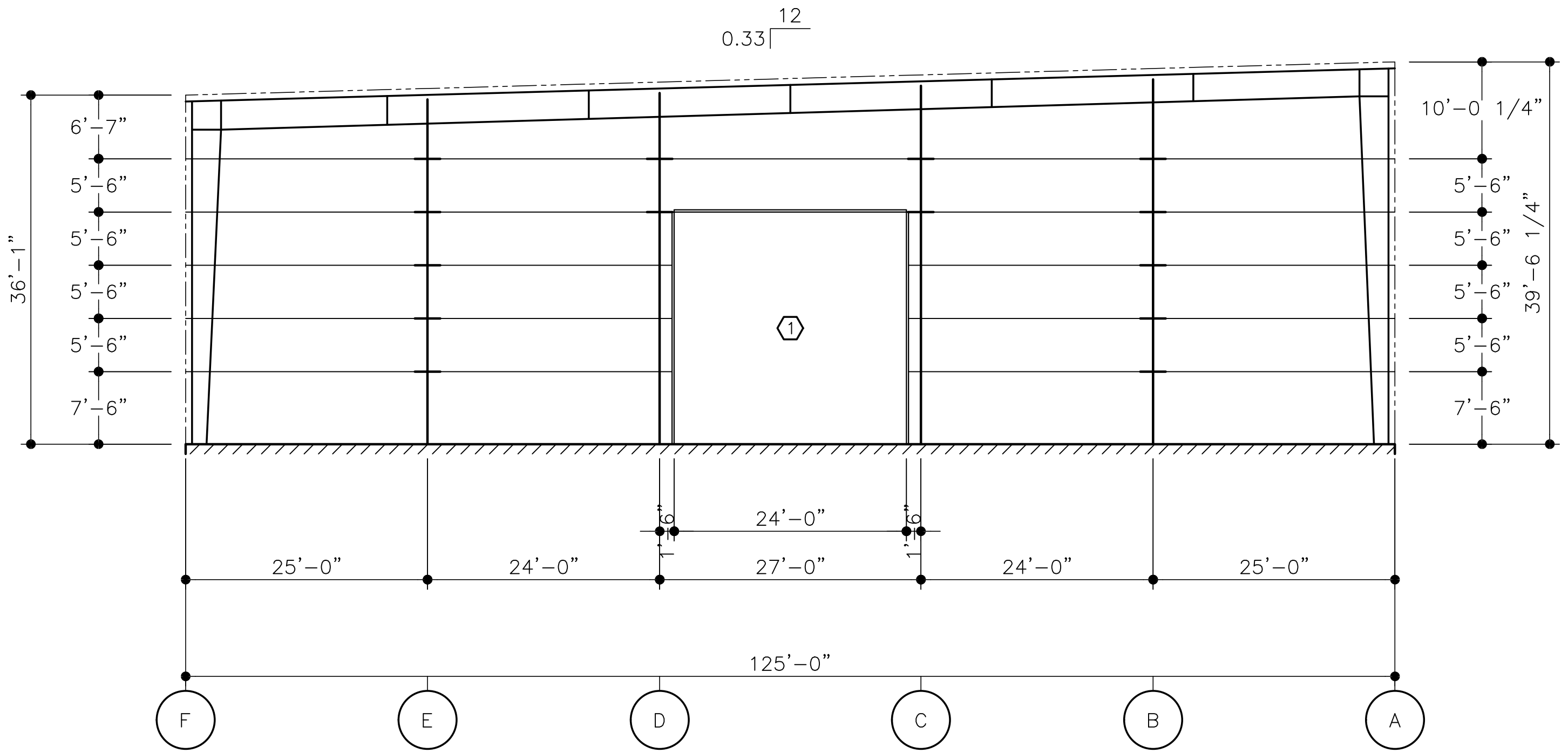
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ITEM # 3

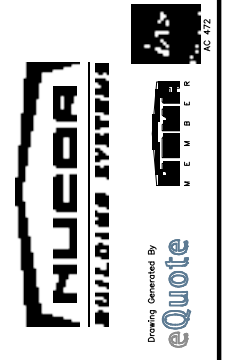
KEY PLAN



FRAMED OPENING SCHEDULE					
ID	QTY	WIDTH	HEIGHT	SILL HEIGHT	LOCATED
1	1	24'-0"	24'-0"	0'-0"	FACTORY



ELEVATION AT LINE 19



PROJECT NAME:
PARKER TOWING
ASHLAND CITY, TN

CUSTOMER NAME:
CARTER GROUP, LLC
NASHVILLE, TN

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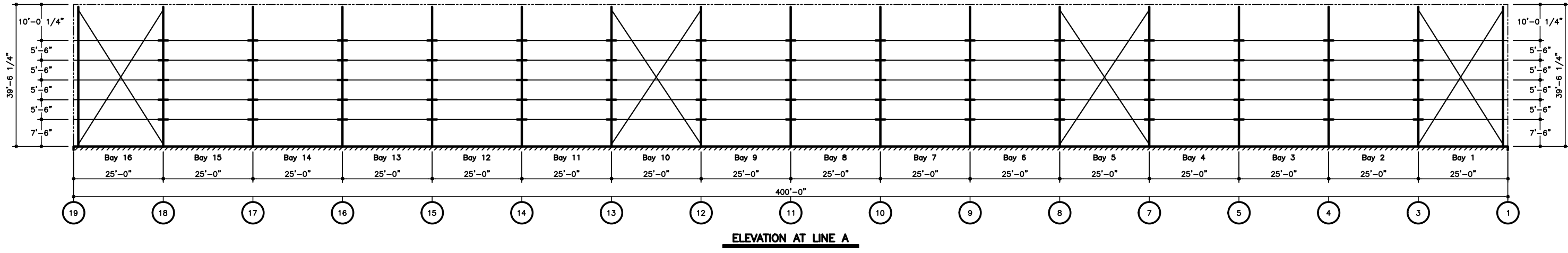
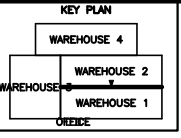
1/10/2022 1:18 PM

PREPARED BY:
NARY STRUCTURAL ELEVATIONS

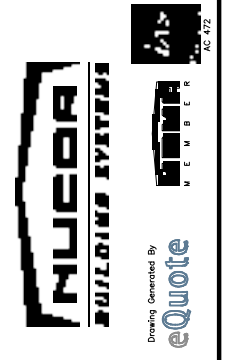
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S1F-19023 R5

SHEET NUMBER:
ST7

ITEM # 3



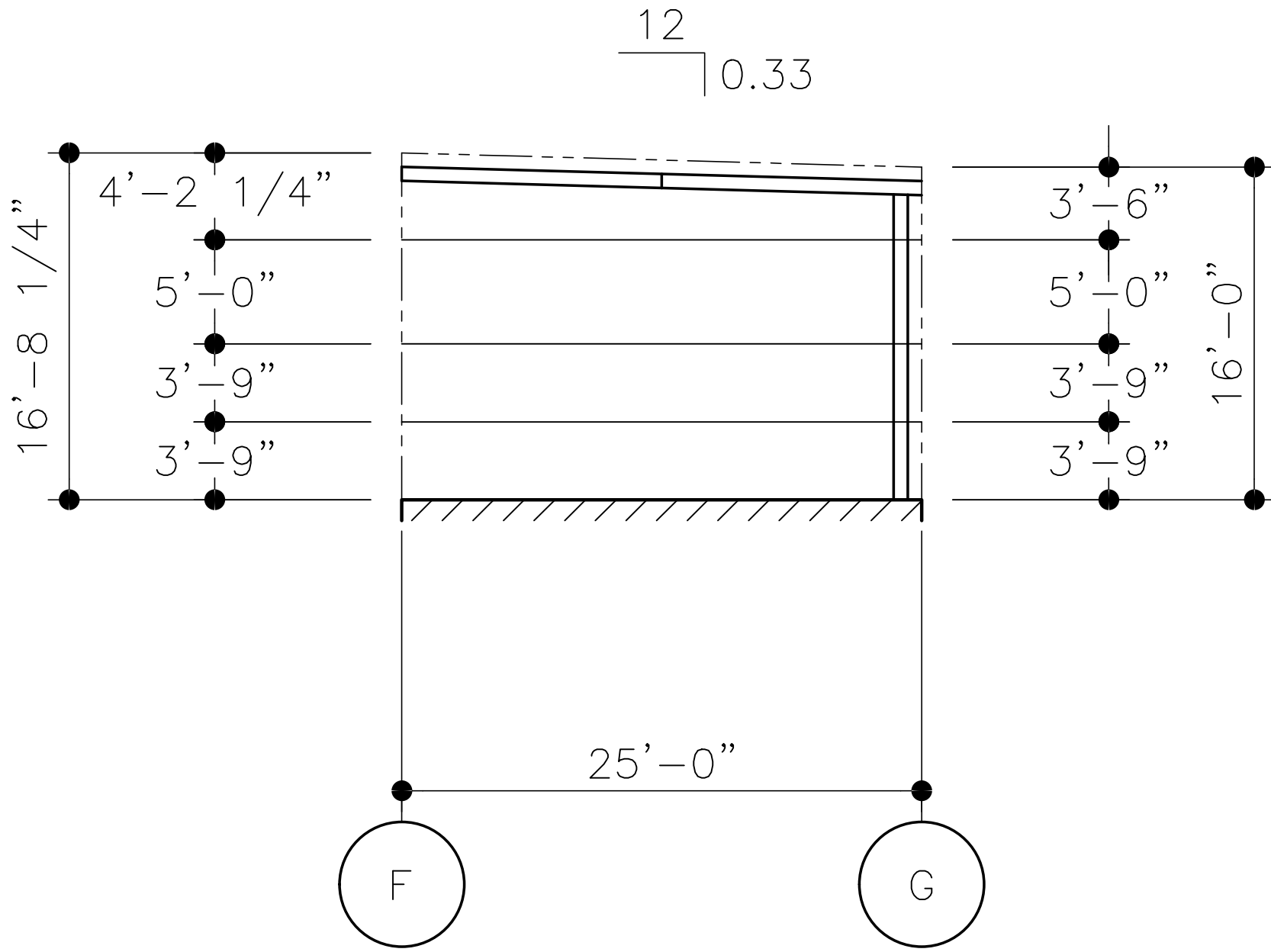
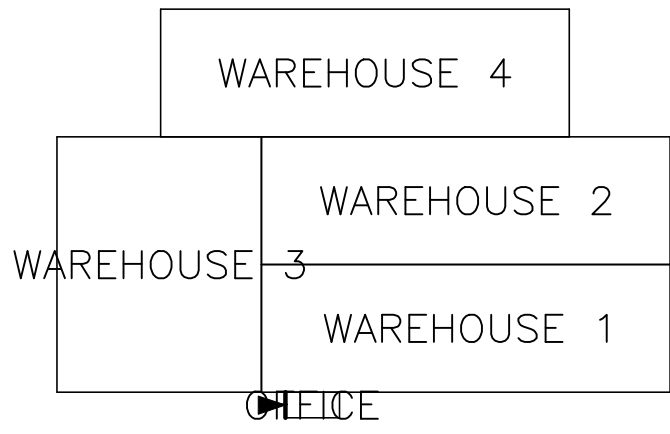
ELEVATION AT LINE A



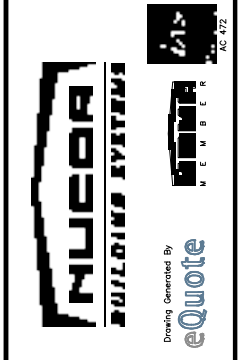
PROJECT NAME:
PARKER TOWING
 ASHLAND CITY, TN
 CUSTOMER NAME:
CARTER GROUP, LLC
 NASHVILLE, TN

DO NOT USE FOR FINAL CONSTRUCTION
 1/10/2022 1:18 PM
PRELIMINARY STRUCTURAL ELEVATIONS
 SHEET NUMBER:
ST8
S1F-19023 R5
ITEM # 3.

KEY PLAN



ELEVATION AT LINE 2



PROJECT NAME:
 PARKER TOWING
 ASHLAND CITY, TN

CUSTOMER NAME:
 CARTER GROUP, LLC
 NASHVILLE, TN

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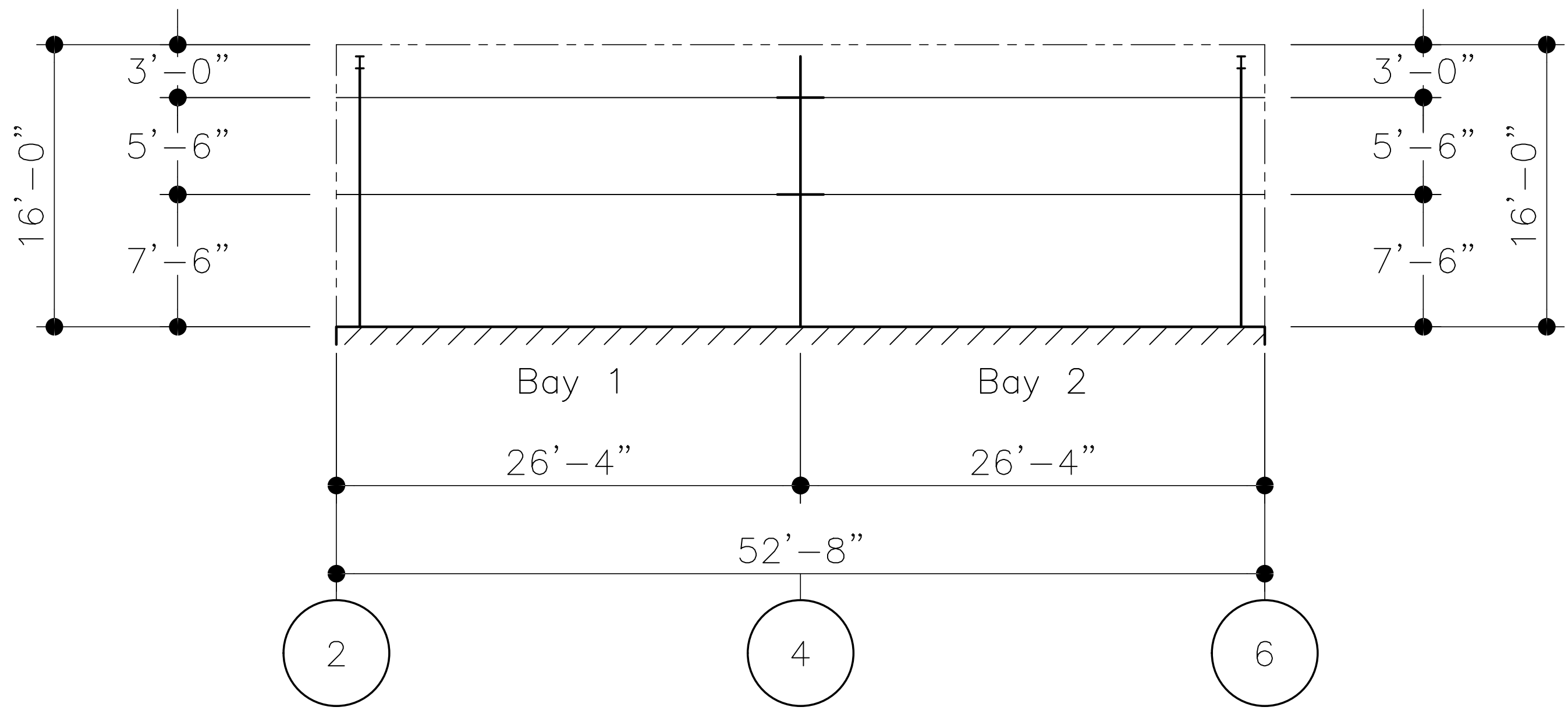
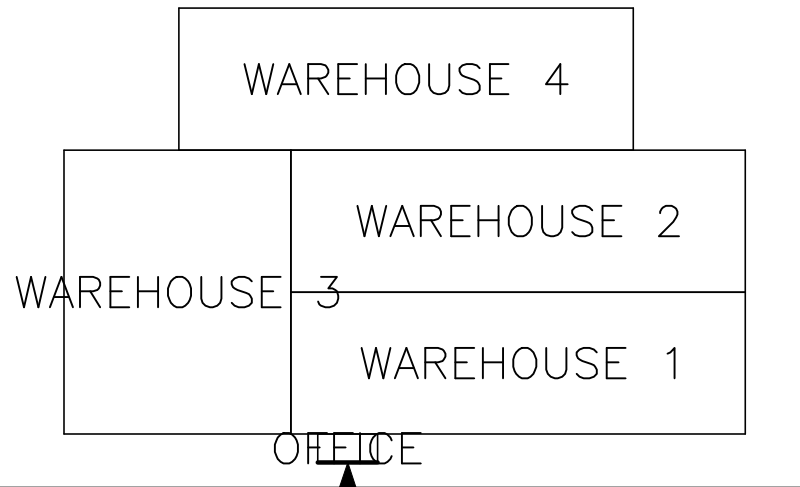
NARY STRUCTURAL ELEVATIONS

QUOTE NUMBER:
 S1F-19023 R5

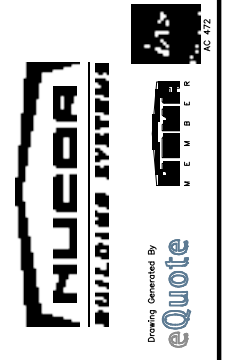
ITEM # 3

ST21

KEY PLAN



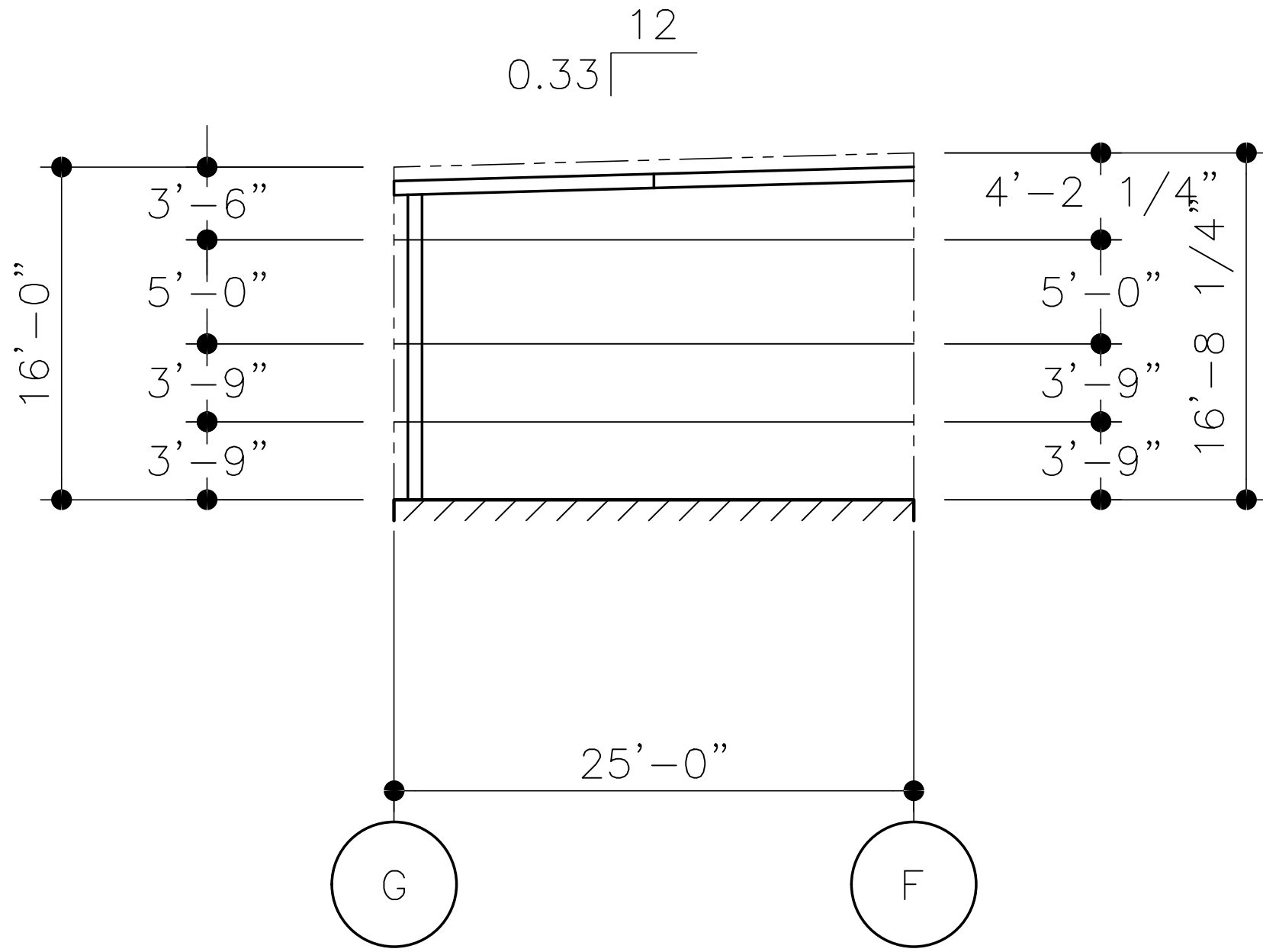
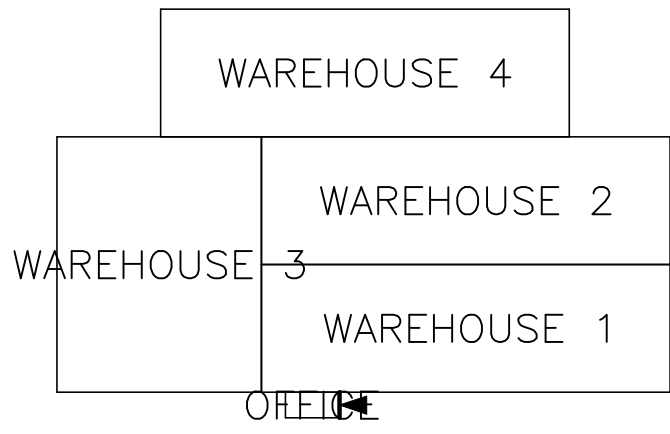
ELEVATION AT LINE G



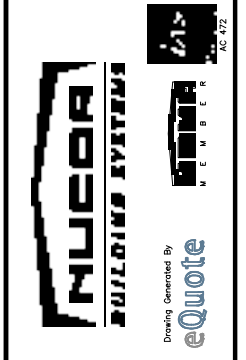
PROJECT NAME:
 PARKER TOWING
 ASHLAND CITY, TN
 CUSTOMER NAME:
 CARTER GROUP, LLC
 NASHVILLE, TN

DO NOT USE FOR FINAL CONSTRUCTION
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 SHEET NUMBER:
 S1F-19023 R5
 ST22
 ITEM # 3.

KEY PLAN



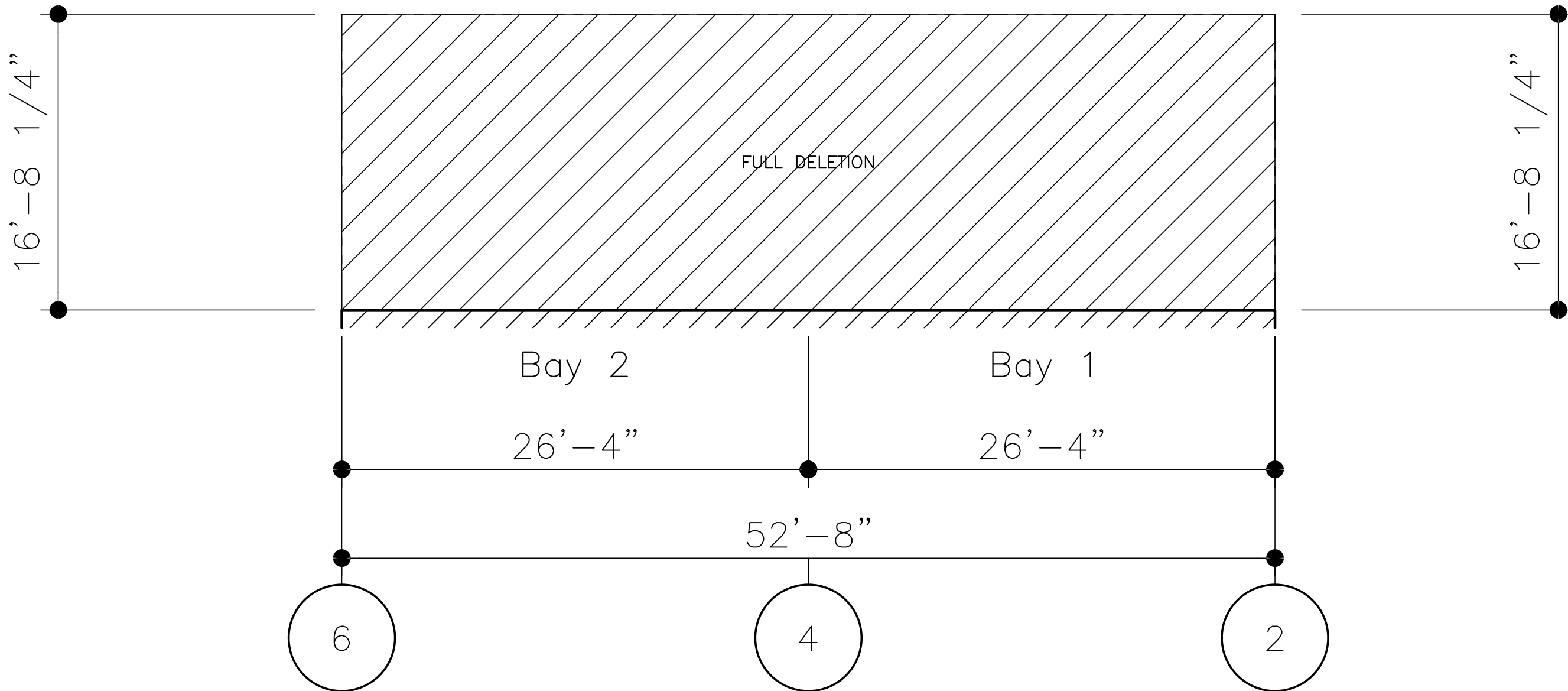
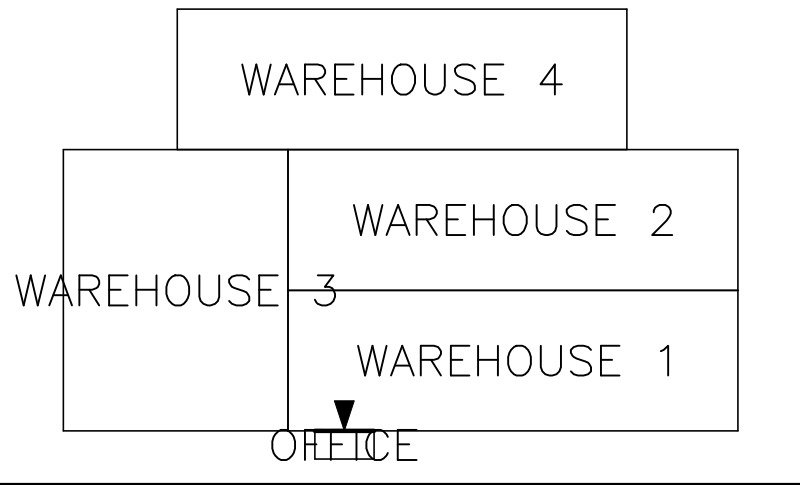
ELEVATION AT LINE 6



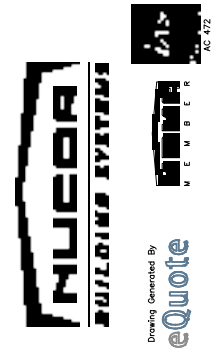
PROJECT NAME:
 PARKER TOWING
 ASHLAND CITY, TN
 CUSTOMER NAME:
 CARTER GROUP, LLC
 NASHVILLE, TN

DO NOT USE FOR FINAL CONSTRUCTION
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 SHEET PRE SHEET
 QUOTE NUMBER:
 S1F-19023 R5
 ITEM # 3
 ST23

KEY PLAN

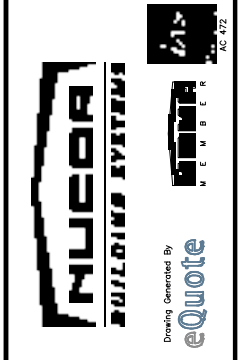
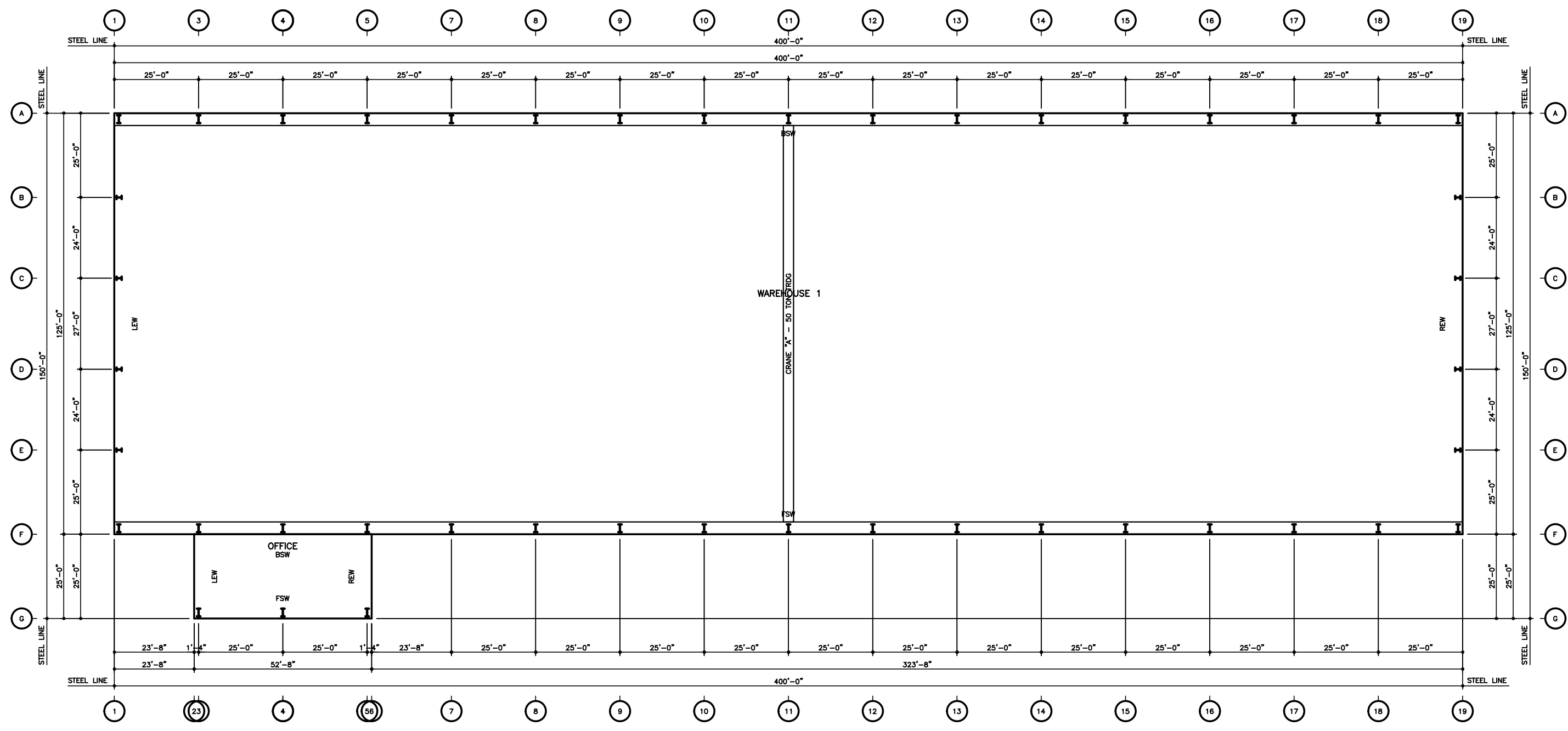


ELEVATION AT LINE F



PROJECT NAME:
PARKER TOWING
ASHLAND CITY, TN
CUSTOMER NAME:
CARTER GROUP, LLC
NASHVILLE, TN

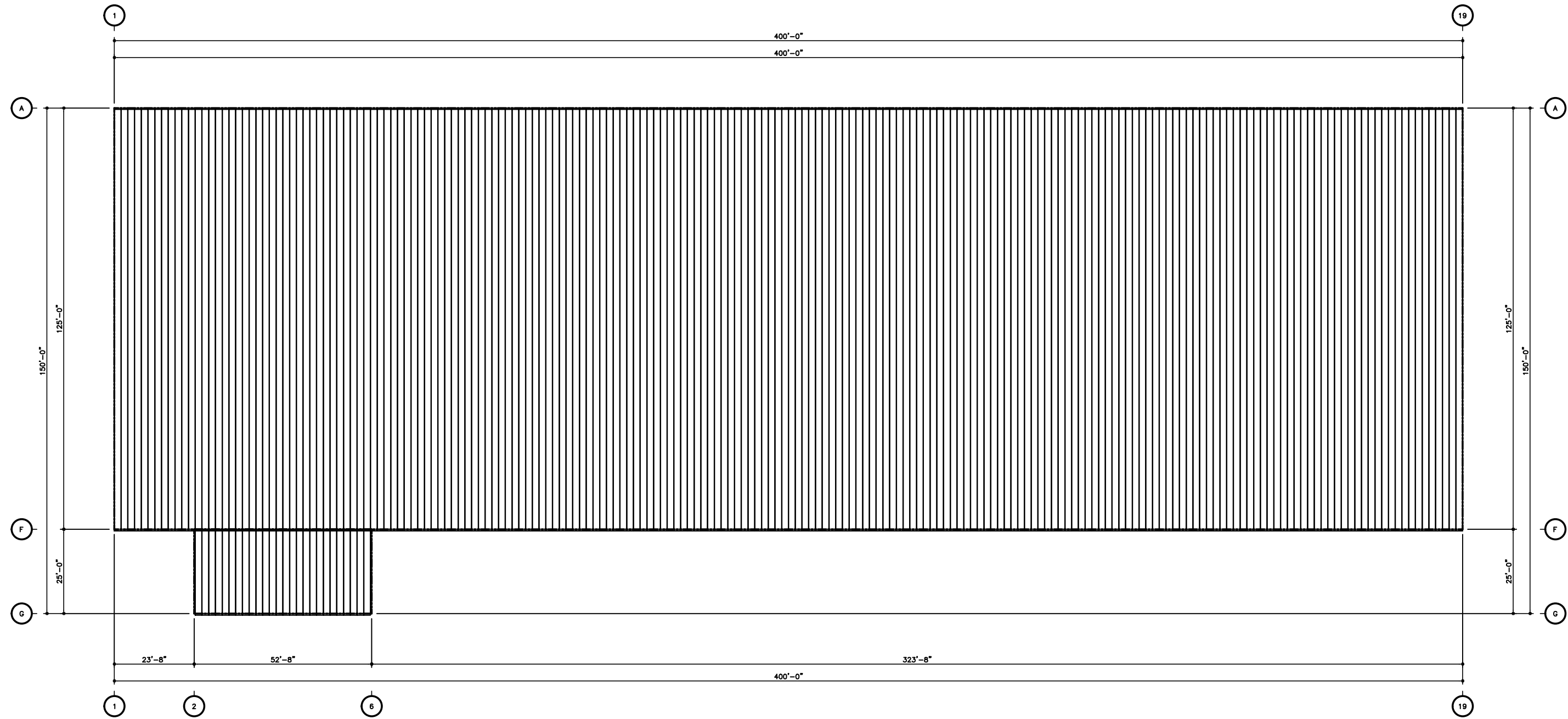
DO NOT USE FOR FINAL CONSTRUCTION
1/10/2022 1:19 PM
NARY STRUCTURAL ELEVATIONS
SHEET NUMBER:
S1F-19023 R5
ITEM # 3
ST24



PROJECT NAME:
PARKER TOWING
 ASHLAND CITY, TN
 CUSTOMER NAME:
CARTER GROUP, LLC
 NASHVILLE, TN

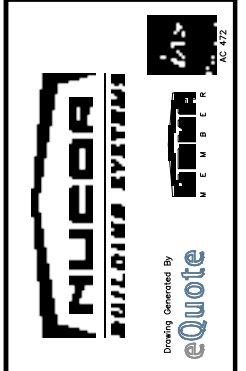
DO NOT USE FOR FINAL CONSTRUCTION
 1/10/2022 1:19 PM
 NARY CRANE PLAN
 QUOTE NUMBER:
 S1F-19023 R5
 CP1
 ITEM # 3.

Column locations marked with "*" are designated as Most Economical (Pipe, Tube, or I-Shape) and will be determined at final design.



ROOF SHEETING PLAN

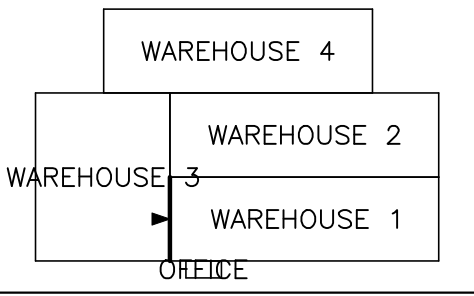
- WAREHOUSE 1 - PANELS: 24 GA. CFR - GALVALUME PLUS
- WAREHOUSE 2 - PANELS: 24 GA. CFR - GALVALUME PLUS
- WAREHOUSE 3 - PANELS: 24 GA. CFR - GALVALUME PLUS
- WAREHOUSE 4 - PANELS: 24 GA. CFR - GALVALUME PLUS
- OFFICE - PANELS: 24 GA. CFR - GALVALUME PLUS



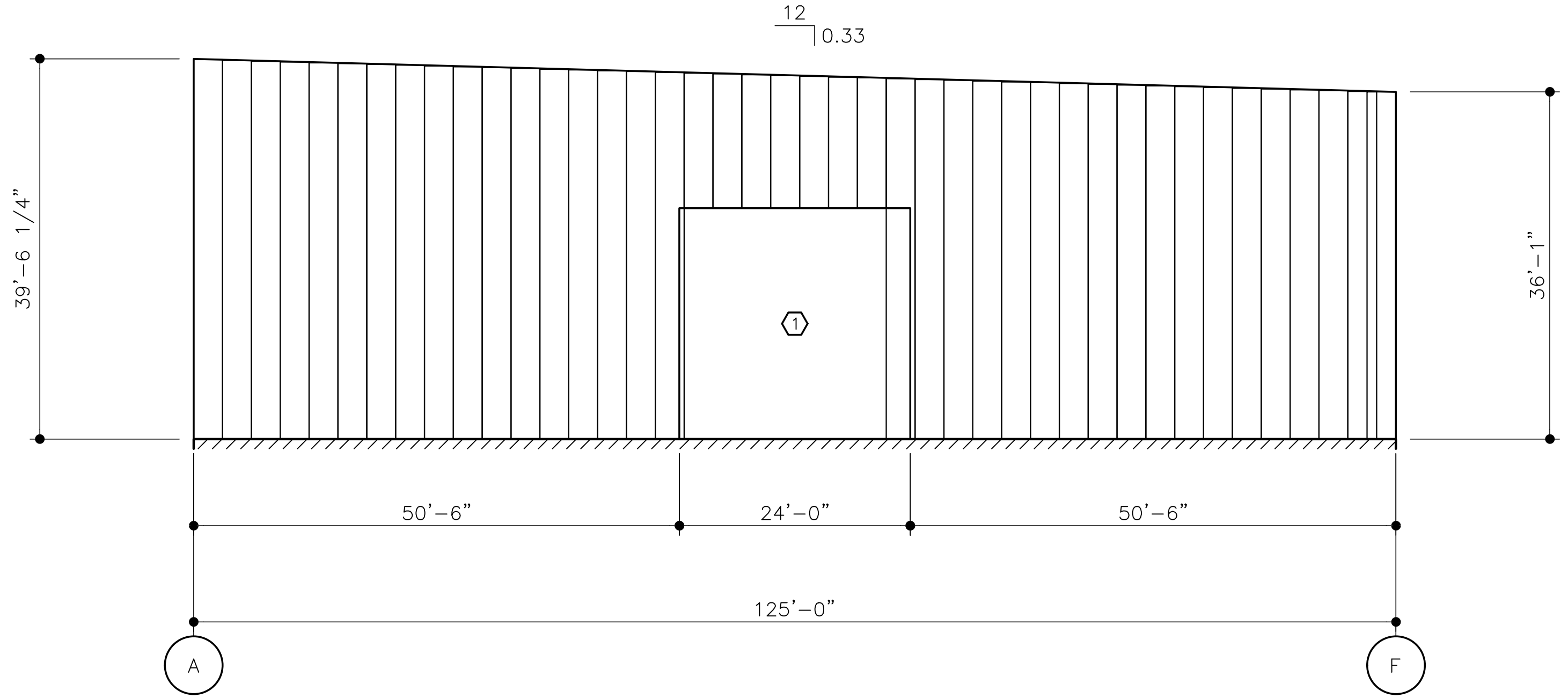
PROJECT NAME:
PARKER TOWING
ASHLAND CITY, TN
CUSTOMER NAME:
CARTER GROUP, LLC
NASHVILLE, TN

DO NOT USE FOR FINAL CONSTRUCTION
1/10/2022 1:19 PM
PRIMARY ROOF SHEETING PLAN
QUOTE NUMBER:
S1F-19023 R5
RS1
ITEM # 3.

KEY PLAN

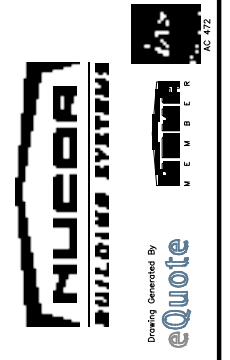


FRAMED OPENING SCHEDULE					
ID	QTY	WIDTH	HEIGHT	SILL HEIGHT	LOCATED
1	1	24'-0"	24'-0"	0'-0"	FACTORY



WALL SHEETING ELEVATION AT LINE 1

PANELS: 26 GA. CLASSIC WALL - SANDSTONE (SP)



PROJECT NAME:
PARKER TOWING
ASHLAND CITY, TN
CUSTOMER NAME:
CARTER GROUP, LLC
NASHVILLE, TN

DO NOT USE FOR FINAL CONSTRUCTION

1/10/2022 1:19 PM

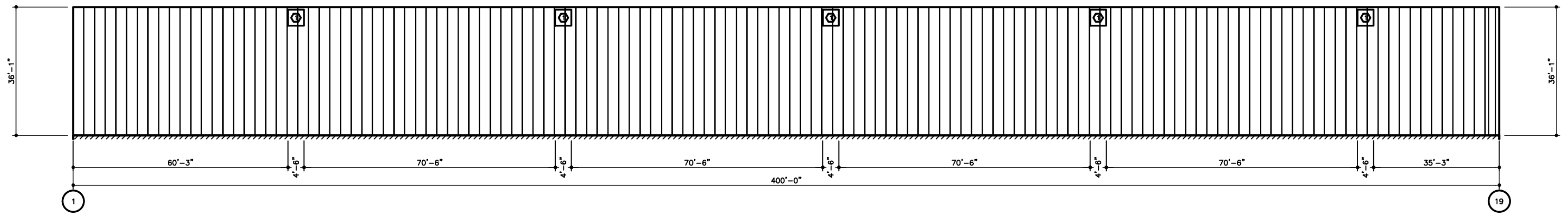
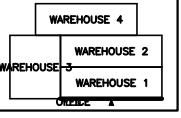
PRELIMINARY SHEETING ELEVATIONS

QUOTE NUMBER:
S1F-19023 R5

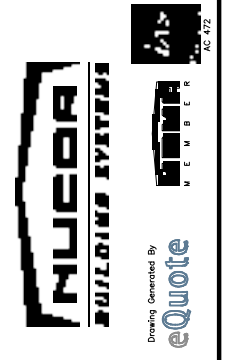
ITEM # 3

WS5

FRAMED OPENING SCHEDULE				
QTY	WIDTH	HEIGHT	SILL HEIGHT	LOCATED
1	5	4'-6"	4'-6"	30'-10" FACTORY



WALL SHEETING ELEVATION AT LINE F
 PANELS: 26 GA. CLASSIC WALL - SANDSTONE (SP)



PROJECT NAME:
 PARKER TOWING
 ASHLAND CITY, TN

CUSTOMER NAME:
 CARTER GROUP, LLC
 NASHVILLE, TN

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1/10/2022 1:19 PM

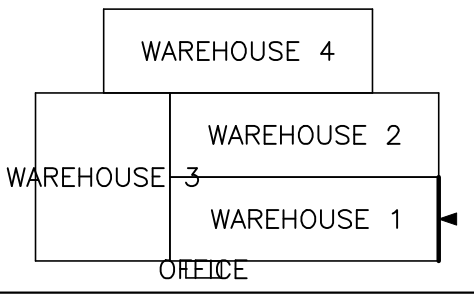
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QUOTE NUMBER: S1F-19023 R5

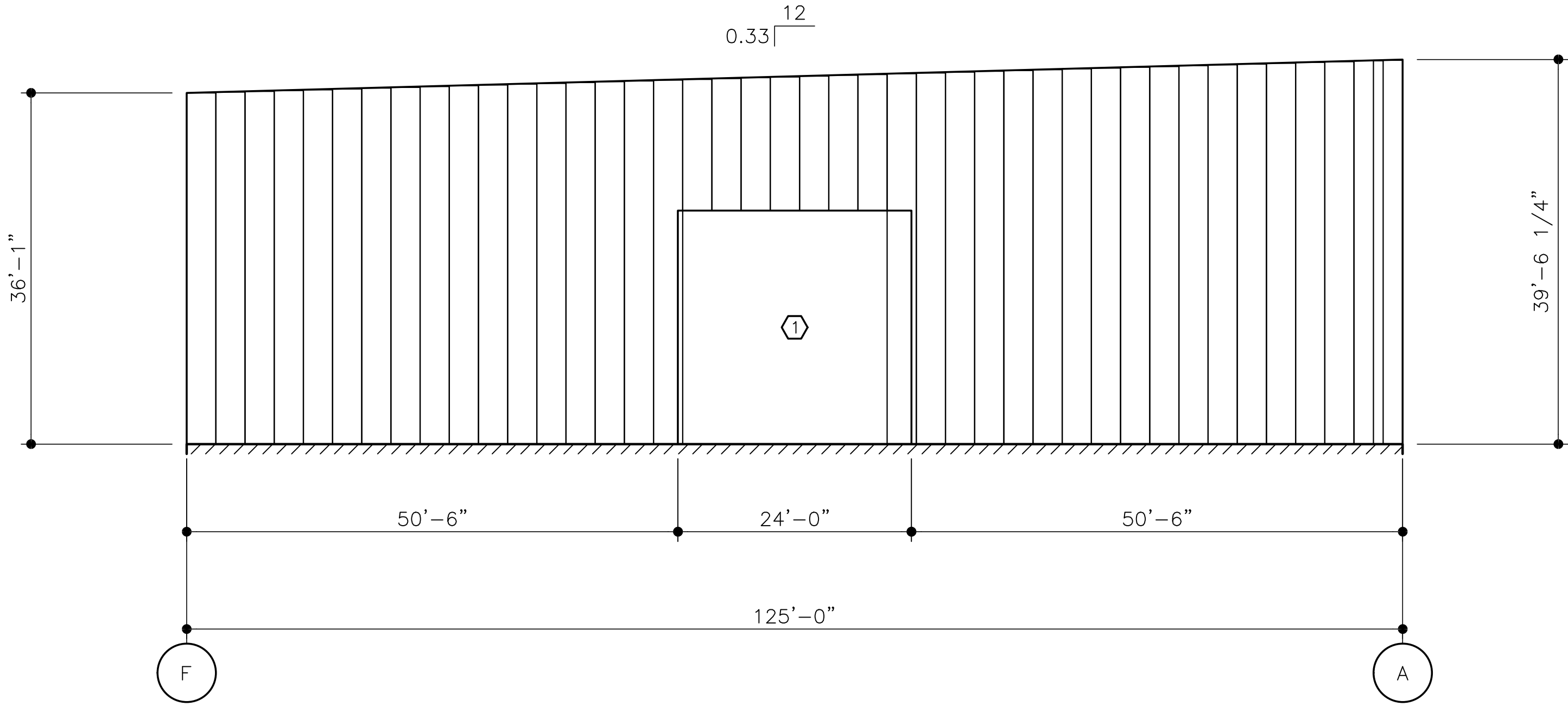
ITEM # 3.

WS6

KEY PLAN

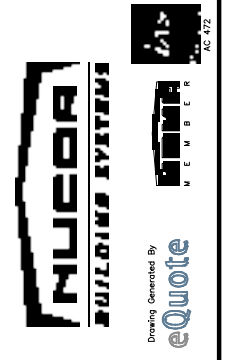


FRAMED OPENING SCHEDULE					
ID	QTY	WIDTH	HEIGHT	SILL HEIGHT	LOCATED
1	1	24'-0"	24'-0"	0'-0"	FACTORY



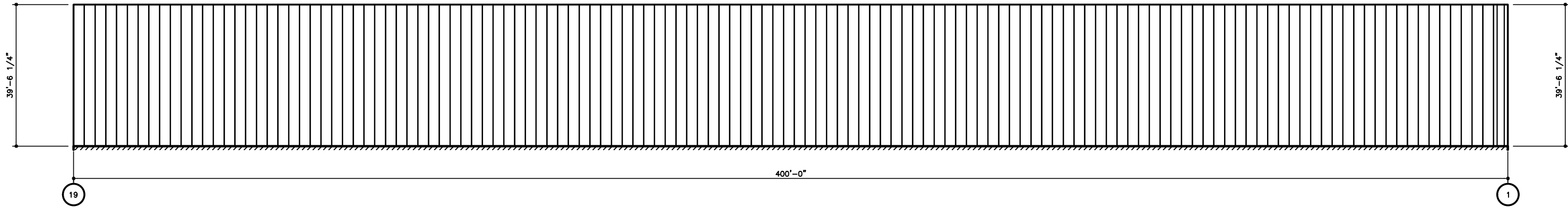
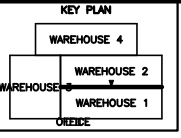
WALL SHEETING ELEVATION AT LINE 19

PANELS: 26 GA. CLASSIC WALL - SANDSTONE (SP)



PROJECT NAME:
PARKER TOWING
ASHLAND CITY, TN
CUSTOMER NAME:
CARTER GROUP, LLC
NASHVILLE, TN

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PRIMARY SHEETING ELEVATIONS
QUOTE NUMBER:
S1F-19023 R5
WS7
ITEM # 3



WALL SHEETING ELEVATION AT LINE A
 PANELS: 26 GA. CLASSIC WALL - SANDSTONE (SP)

DO NOT USE FOR FINAL CONSTRUCTION

SHEET: PRE SHEET

DATE: 1/10/2022 1:19 PM

DESCRIPTION: WALL SHEETING ELEVATIONS

QUOTE NUMBER: WS8 S1F-19023 R5

ITEM # 3.

PROJECT NAME:
 PARKER TOWING
 ASHLAND CITY, TN

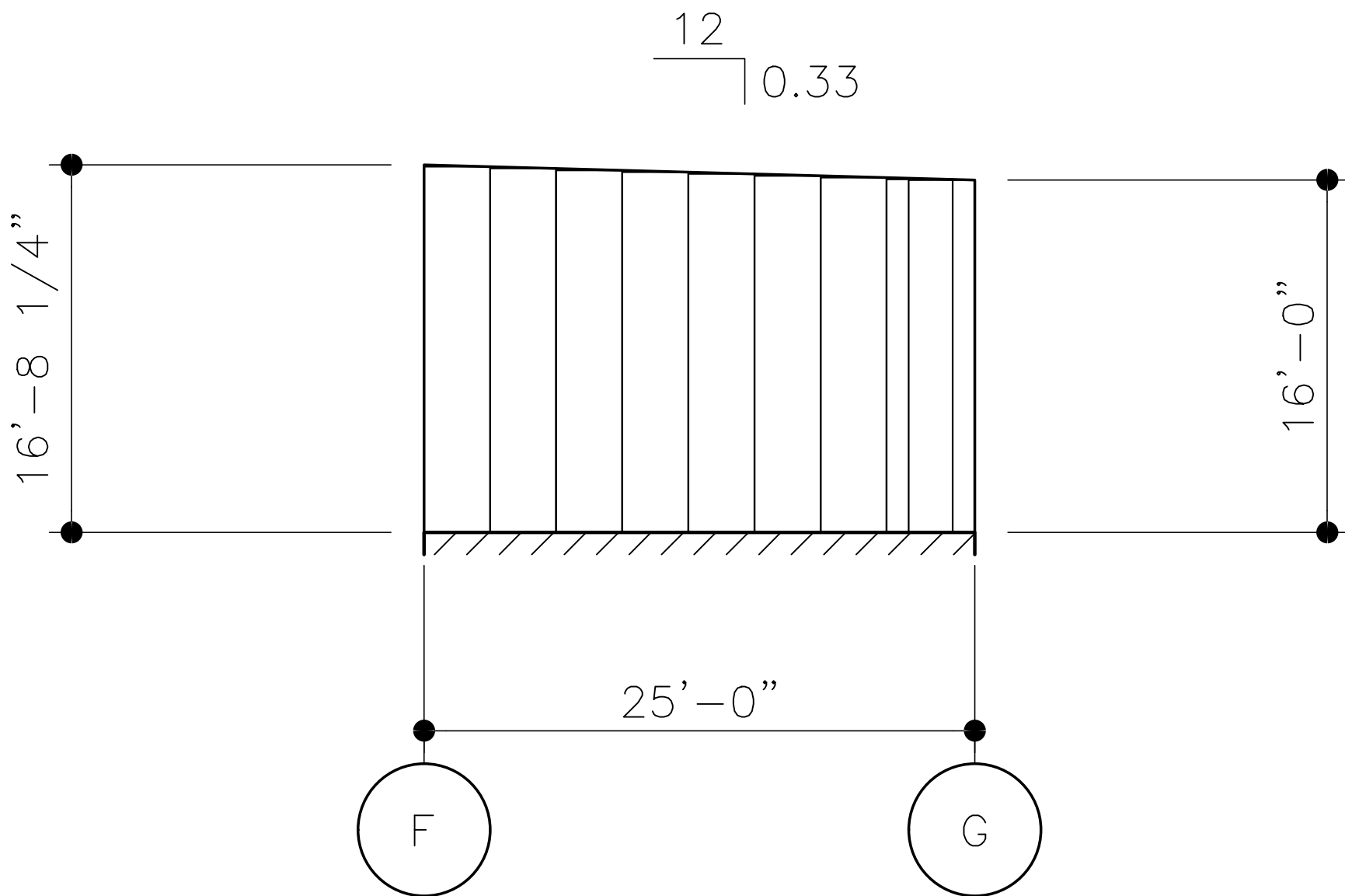
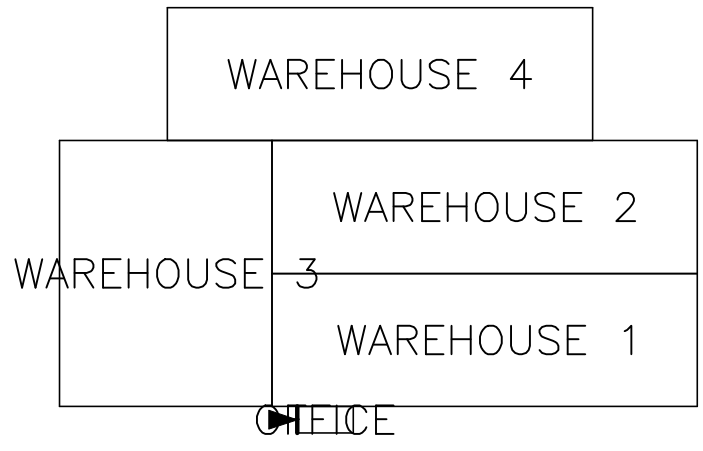
CUSTOMER NAME:
 CARTER GROUP, LLC
 NASHVILLE, TN

MUEBA BUILDING SYSTEMS

Drawing Generated By
eQuote

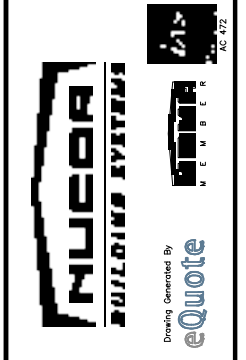
AC 672

KEY PLAN



WALL SHEETING ELEVATION AT LINE 2

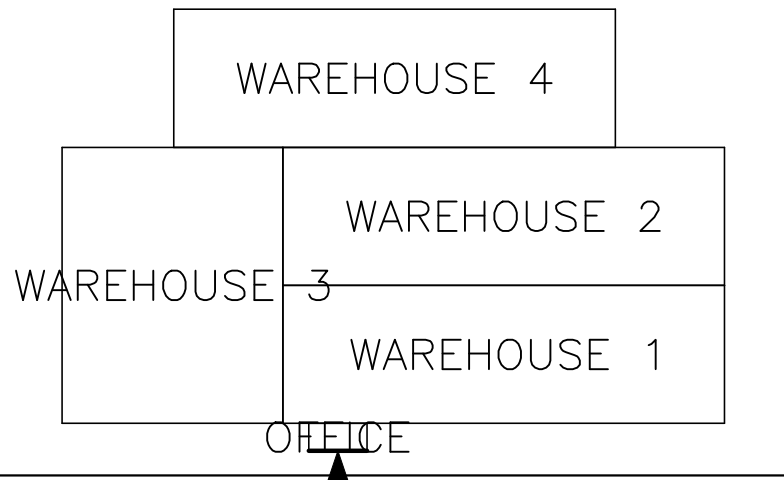
PANELS: 26 GA. CLASSIC WALL – SANDSTONE (SP)



PROJECT NAME:
 PARKER TOWING
 ASHLAND CITY, TN
 CUSTOMER NAME:
 CARTER GROUP, LLC
 NASHVILLE, TN

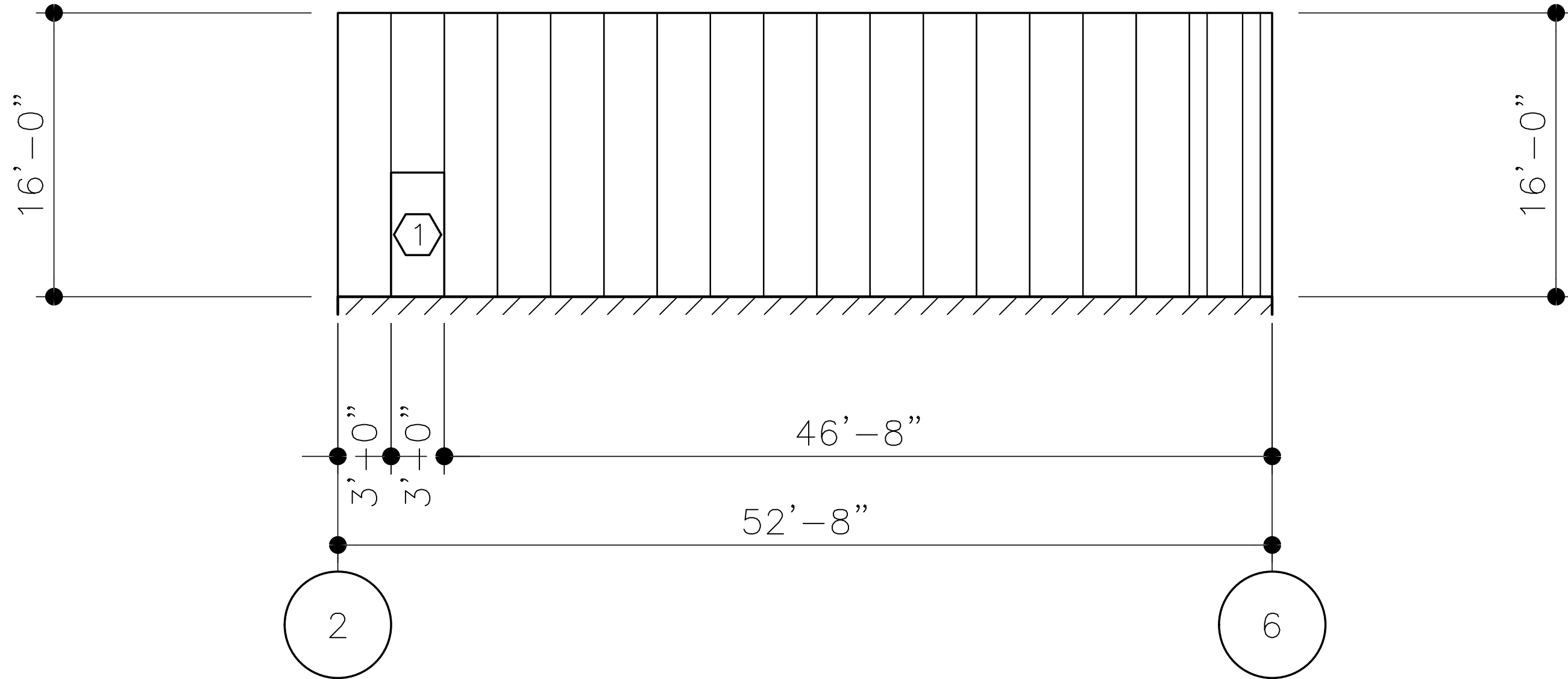
DO NOT USE FOR FINAL CONSTRUCTION
 1/10/2022 1:19 PM
 NARY SHEETING ELEVATIONS
 QUOTE NUMBER:
 S1F-19023 R5
 WS21
 ITEM # 3

KEY PLAN



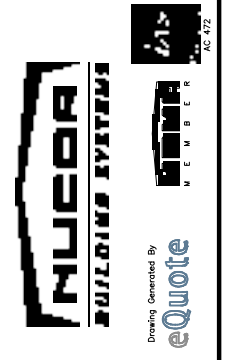
FRAMED OPENING SCHEDULE

ID	QTY	WIDTH	HEIGHT	SILL HEIGHT	LOCATED
1	1	3'-0"	7'-0"	0'-0"	FACTORY



WALL SHEETING ELEVATION AT LINE G

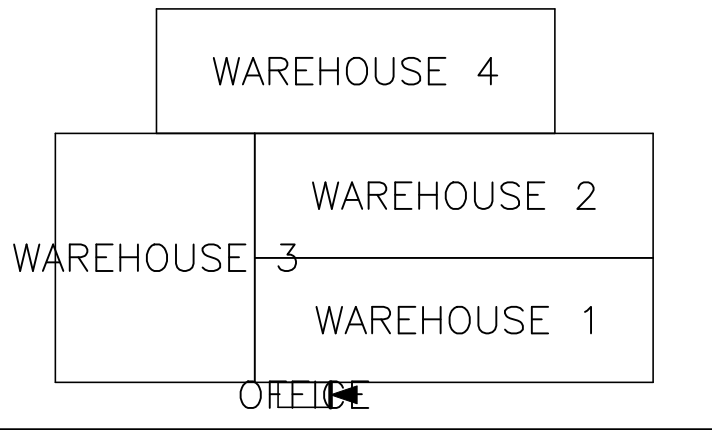
PANELS: 26 GA. CLASSIC WALL - SANDSTONE (SP)



PROJECT NAME:
PARKER TOWING
ASHLAND CITY, TN
CUSTOMER NAME:
CARTER GROUP, LLC
NASHVILLE, TN

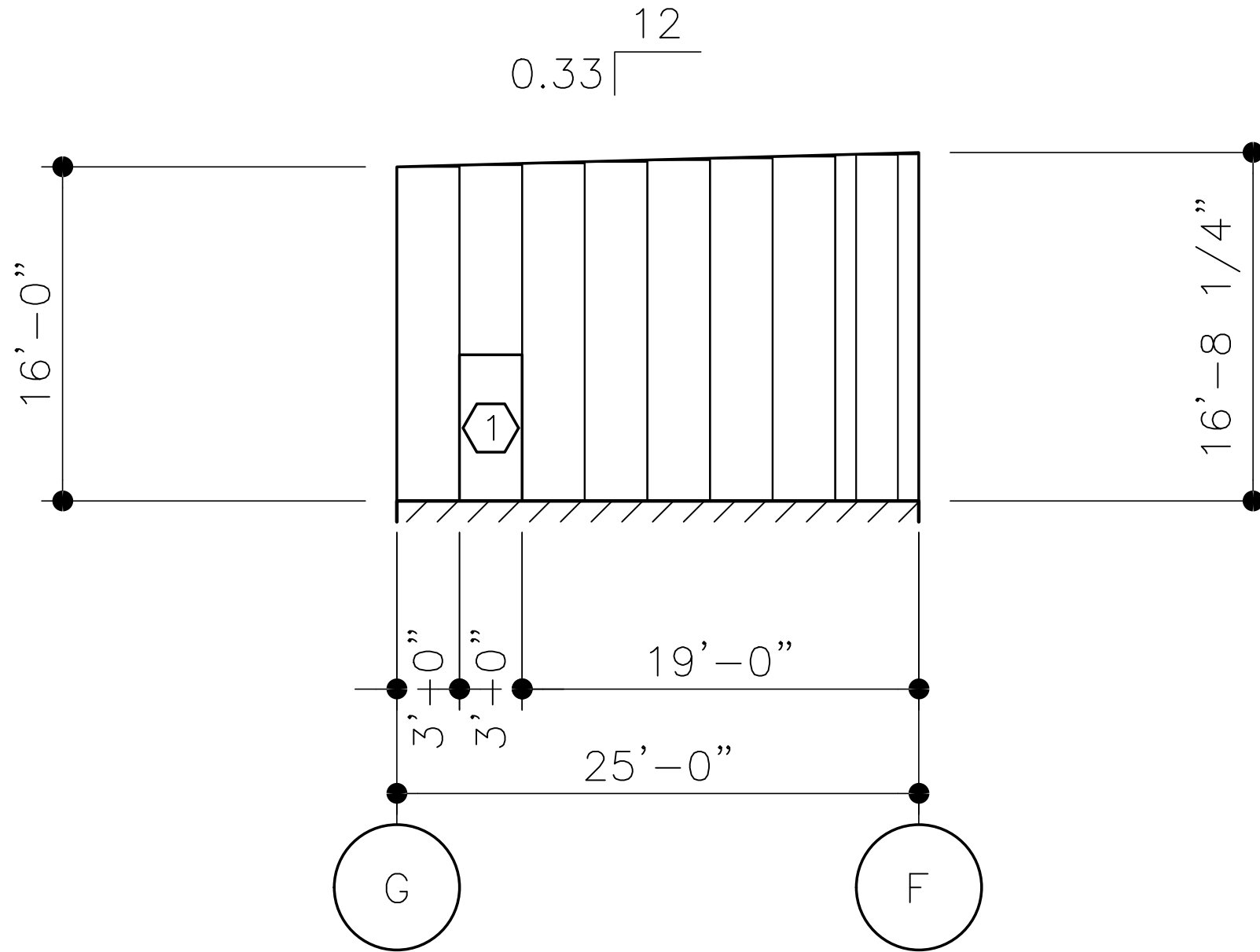
DO NOT USE FOR FINAL CONSTRUCTION
1/10/2022 1:19 PM
NARY SHEETING ELEVATIONS
SHEET NUMBER:
S1F-19023 R5
ITEM # 3.
WS22

KEY PLAN



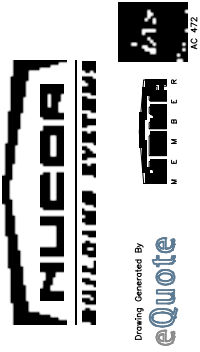
FRAMED OPENING SCHEDULE

(D)	QTY	WIDTH	HEIGHT	SILL HEIGHT	LOCATED
1	1	3'-0"	7'-0"	0'-0"	FACTORY



WALL SHEETING ELEVATION AT LINE 6

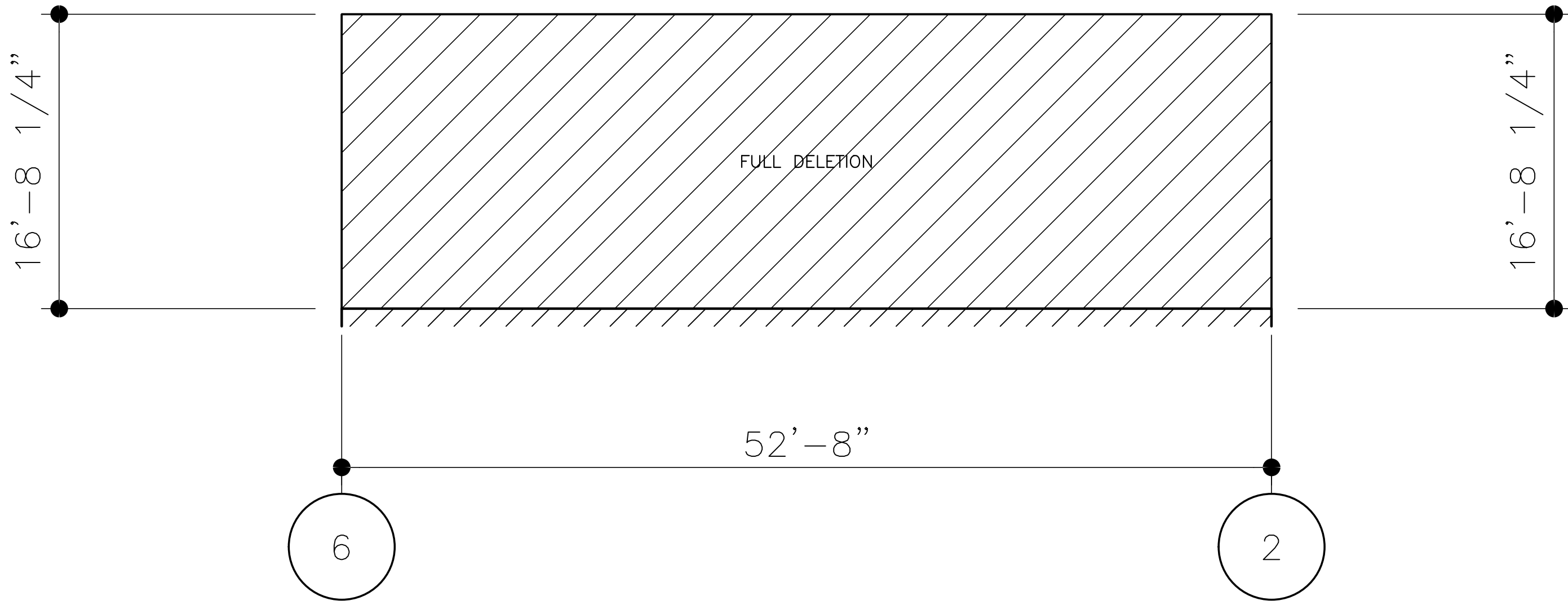
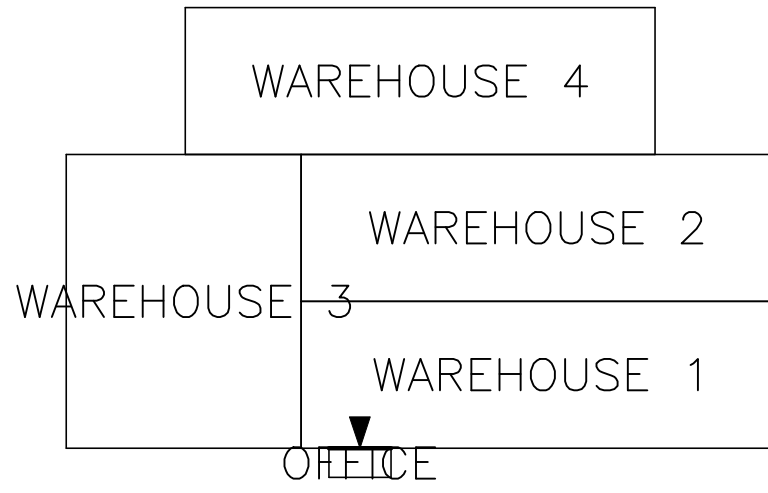
PANELS: 26 GA. CLASSIC WALL - SANDSTONE (SP)



PROJECT NAME:
PARKER TOWING
ASHLAND CITY, TN
CUSTOMER NAME:
CARTER GROUP, LLC
NASHVILLE, TN

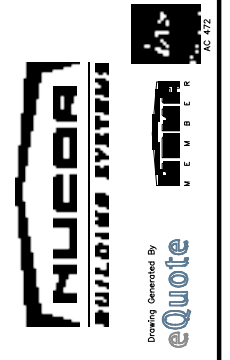
DO NOT USE FOR FINAL CONSTRUCTION
1/10/2022 1:19 PM
NARY SHEETING ELEVATIONS
QUOTE NUMBER:
S1F-19023 R5
ITEM # 3
WS23

KEY PLAN



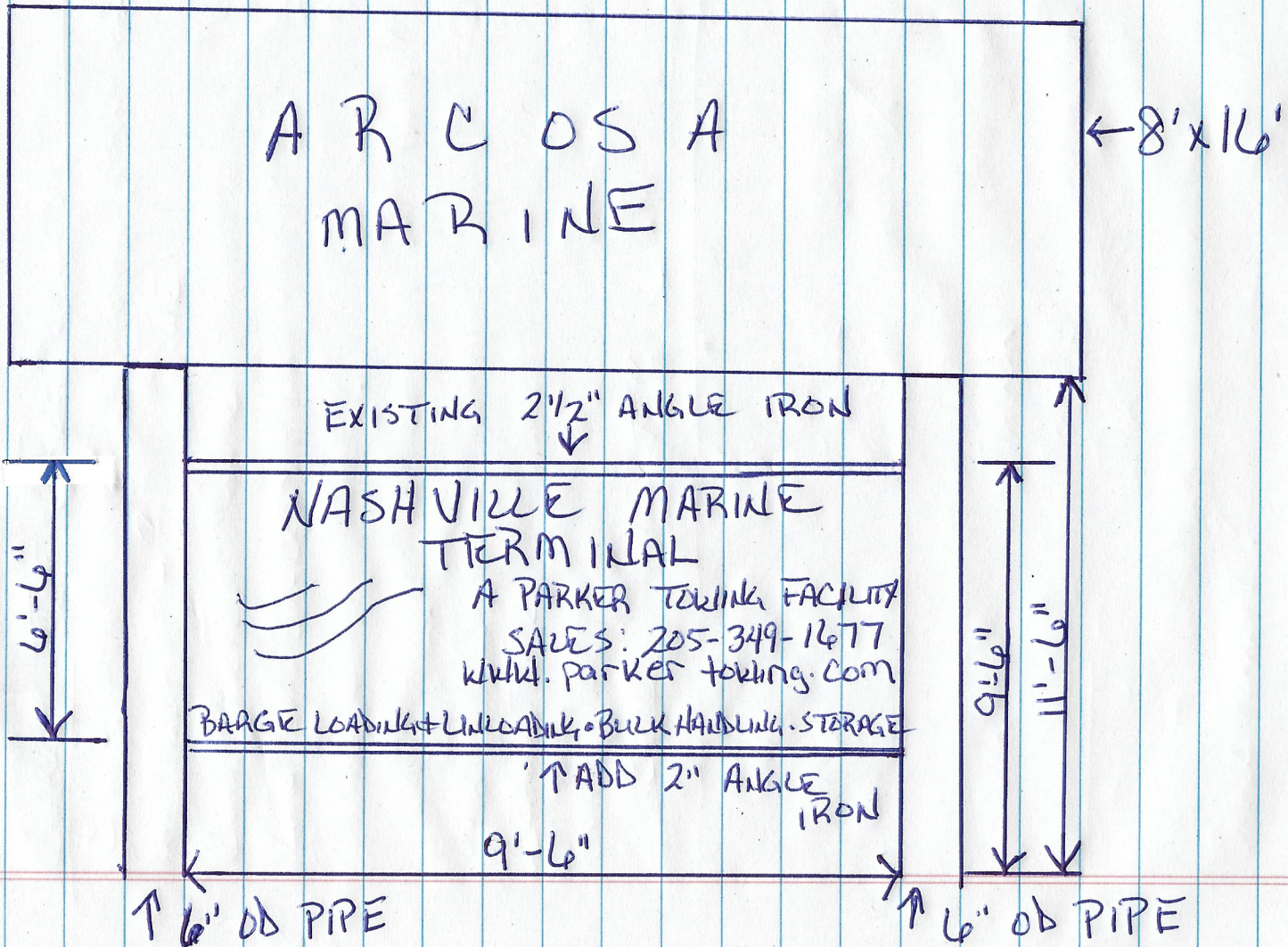
WALL SHEETING ELEVATION AT LINE F

PANELS: 26 GA. CLASSIC WALL - SANDSTONE (SP)



PROJECT NAME:
 PARKER TOWING
 ASHLAND CITY, TN
 CUSTOMER NAME:
 CARTER GROUP, LLC
 NASHVILLE, TN

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 NARY SHEETING ELEVATIONS
 QUOTE NUMBER:
 S1F-19023 R5
 ITEM # 3
 WS24



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Ashland City Municipal Regional Planning Commission
Resolution

WHEREAS, Section 13-3-102, Tennessee Code Annotated, provides for the establishment of planning regions adjacent to and outside of municipalities having active municipal planning commissions; and

WHEREAS, the Ashland City Municipal Regional Planning Commission was organized to exercise planning and development controls inside the corporate limits; and

WHEREAS, planning and control of development beyond the corporate limits has not been shown to be advantageous to the city.

NOW, THEREFORE, BE IT RESOLVED by the Ashland City Municipal Regional Planning Commission that the Local Government Planning Advisory Committee be requested in accordance with Tennessee Code Annotated, Section 13-3-102, to dissolve the Ashland City Planning Region; and

BE IT FURTHER RESOLVED that the entire area previously identified as the Ashland City Planning Region revert to Cheatham County as part of the Cheatham County planning region.

Date

Chair
Ashland City Planning Commission